1-15-10 Letting, Item 154

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	1
	ILLINOIS	CONTRACT NO. 62930		

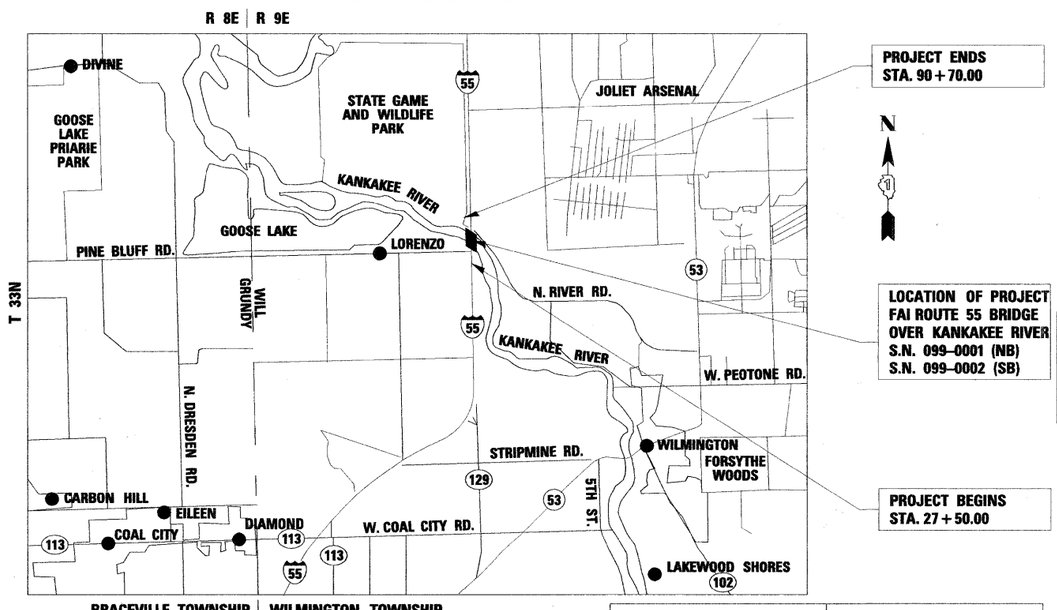
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PROPOSED HIGHWAY PLANS

FAI ROUTE 55 (I–55) OVER THE KANKAKEE RIVER
SECTION 88(B&B–1)BR
BRIDGE REHABILITATION
WILL COUNTY
PROJECT: ESP-055-6(238)241
C–91–166–05

FOR INDEX OF SHEETS, SEE SHEET NO. 2

IMPROVEMENTS LOCATED WITHIN
THE CITY OF WILMINGTON, IL

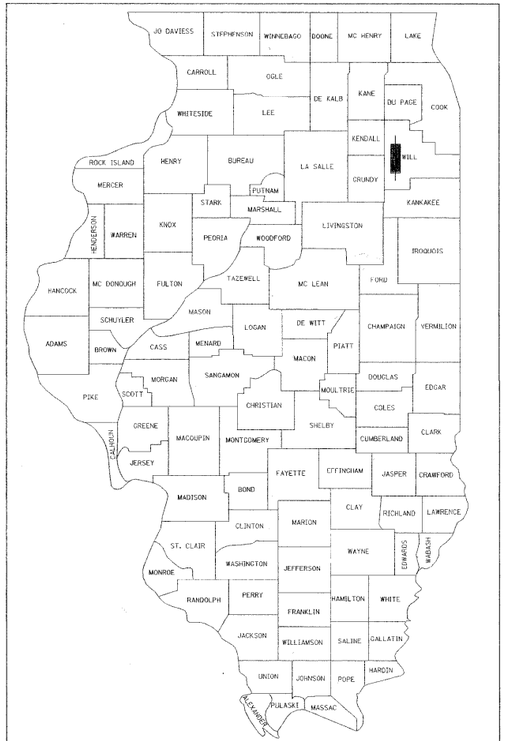


LOCATION MAP
(NTS)

LOCATION OF PROJECT
FAI ROUTE 55 BRIDGE
OVER KANKAKEE RIVER
S.N. 099–0001 (NB)
S.N. 099–0002 (SB)

GROSS LENGTH = NET LENGTH = 6320 FEET = 1.20 MILES

D-91-166-05

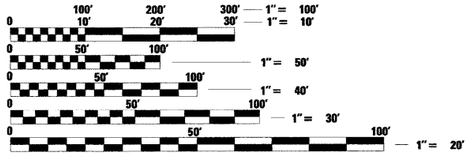


LOCATION OF SECTION INDICATED THUS: –

rjngroup
Excellence through Ownership
200 West Front Street
Wheaton, Il 60187
License # 184-000813

TRAFFIC DATA

FUNCTIONAL CLASSIFICATION – INTERSTATE, CLASS I TRUCK ROUTE
AT NEW RIVER RD. 2005 ADT = 55,000
AT LORENZO RD. 2005 ADT = 40,900
INTERSTATE 55 POSTED SPEED LIMIT = 65 MPH



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1–800–892–0123
OR 811

PROJECT ENGINEER: MICHELLE AQUINO (847) 705–4606
PROJECT MANAGER: RAJENDRA SHAH (847) 705–4555

CONTRACT NO. 62930

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED June 24 20 09
Dino O'Hare PE
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER
December 4, 2009
Charles J. Ingersoll PE
ENGINEER OF DESIGN AND ENVIRONMENT
December 4, 20 09
Christine M. Reed PE
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

LICENSED STRUCTURAL ENGINEER
MARY COOMBE BLOXDORF
4859
STATE OF ILLINOIS
6-23-09

MARY COOMBE BLOXDORF
DISCIPLINE: STRUCTURAL ENGINEER
LICENSE NO. 081–004859
LICENSE EXPIRES 11/30/2010

CHAD M. JANSON
062-053980
REGISTERED PROFESSIONAL ENGINEER
OF
6-22-09
STATE OF ILLINOIS

CHAD M. JANSON
DISCIPLINE – CIVIL ENGINEER
LICENSE NO. 062–053980
LICENSE EXPIRES 11/30/2009

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

FINAL PLANS

INDEX OF SHEETS

HIGHWAY STANDARDS

STANDARD NO.	STANDARD TITLE
000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-04	TEMPORARY EROSION CONTROL SYSTEMS
482001-02	HMA SHOULDER ADJACENT TO FLEXIBLE PAVEMENT
482011-03	HMA SHOULDER STRIPS/SHOULDERS WITH RESURFACING
515001-03	NAME PLATE FOR BRIDGES
601101-01	CONCRETE HEADWALL FOR PIPE DRAIN
606401-01	PAVED DITCH
630001-08	STEEL PLATE BEAM GUARDRAIL
630301-05	TRAFFIC BARRIER TERMINAL, TYPE 5 & 5A
635006-03	REFLECTOR AND TERMINAL MARKER PLACEMENT
635011-02	REFLECTOR MARKER AND MOUNTING DETAILS
701101-02	OFF-ROAD OPERATIONS, MULTILANE
701321-10	LANE CLOSURE, MULTI-LANE DIVIDED WITH CROSS OVER AND BARRIER
701400-03	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701401-05	LANE CLOSURE, FREEWAY/EXPRESSWAY
701411-05	LANE CLOSURE, MULTI-LANE AT ENTRANCE OR EXIT RAMP, FOR SPEEDS >= 45 MPH
701416-06	LANE CLOSURE, FREEWAY/EXPRESSWAY, WITH CROSSOVER AND BARRIER
701901-01	TRAFFIC CONTROL DEVICES
704001-05	TEMPORARY CONCRETE BARRIER
720001-01	SIGN PANEL MOUNTING DETAILS
720006-02	SIGN PANEL ERECTION DETAILS
780001-02	TYPICAL PAVEMENT MARKINGS
781001-03	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS

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Excellence through Ownership
200 West Front Street
Wheaton, Il 60187

DESIGNED - KSD	REVISED -	
DRAWN - KSD	REVISED -	
CHECKED - CMJ	REVISED -	
DATE - 03-04-09	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS & STANDARDS

SCALE: NTS | SHEET NO. 1 OF 1 SHEETS | STA. ---- | TO STA. ----

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	2
		CONTRACT NO. 62930		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

GENERAL NOTES – ROADWAY

1. SAW CUTTING OF PAVEMENTS, SHOULDERS, ETC. SHALL BE FULL DEPTH AND SHALL RESULT IN A CLEAN, STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEM REMOVED.

2. USE NO. 8 EPOXY-COATED TIE BARS (OR DOWEL BARS) CONFORMING TO ARTICLE 1003.10(B)(2) OF THE STANDARD SPECIFICATIONS FOR LONGITUDINAL CONSTRUCTION JOINT GROUTED-IN-PLACE TIE BAR AS SHOWN ON STATE STANDARD 420001 AND FOR TYING PORTLAND CEMENT CONCRETE PAVEMENT TO EXISTING CONCRETE PAVEMENT AS SHOWN ON THE PLANS. THE TIE BARS WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PAVEMENT ITEMS BEING CONSTRUCTED.

3. THE THICKNESS OF BITUMINOUS MIXTURES SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE BASE ON WHICH THE BITUMINOUS MIXTURE IS PLACED.

4. EXCEPT AS NOTED ON THE PLANS, PAVEMENT GRADES SHOWN ARE AT THE TOP OF PAVEMENT SURFACES.

5. THE ENGINEER SHALL BE THE SOLE JUDGE CONCERNING CURING TIME FOR THE VARIOUS PAVEMENT LIFTS.

GENERAL NOTES – TRAFFIC CONTROL & PROTECTION

1. STAGING PROCEDURES PRESENTED ARE THE SUGGESTED SEQUENCE OF OPERATIONS. AT HIS OPTION, THE CONTRACTOR MAY SUBMIT AN ALTERNATIVE STAGING PROPOSAL TO THE ENGINEER FOR HIS APPROVAL.

2. TRAFFIC CONDITIONS, ACCIDENTS AND OTHER UNFORESEEN EMERGENCY CONDITIONS MAY REQUIRE THE ENGINEER TO RESTRICT, MODIFY OR REMOVE LANE CLOSURES OF CHANNELIZATION SHOWN IN THE PLANS. THE CONTRACTOR SHALL MAKE THE NECESSARY ADJUSTMENTS AS DIRECTED BY THE ENGINEER WITHOUT DELAY. THE CONTRACTOR SHALL RESPOND TO ANY REQUEST MADE BY THE ENGINEER FOR CORRECTION WITHIN TWO HOURS FROM THE TIME OF NOTIFICATION.

3. ALL TEMPORARY PAVEMENT MARKINGS PROPOSED WITHIN THE WORK AREA SHALL BE COMPLETED PRIOR TO THE CONSTRUCTION PHASE CHANGE.

4. BARRICADES: THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SAND BAGS ON EACH TYPE III BARRICADE USED - ONE (1) WEIGHTED SAND BAG ACROSS EACH BOTTOM RAIL.

5. THE RESIDENT ENGINEER SHALL CONTACT MS. CORA MATHIS, AREA TRAFFIC ENGINEER AT (815) 485-6475, A MINIMUM OF TWO WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

6. THE MEDIAN CROSSOVERS SHALL BE CONSTRUCTED WITH DAILY LANE CLOSURES PER DISTRICT 1 STANDARDS.

7. WHEN WORKERS ARE PRESENT, CONSTRUCTION SPEED LIMIT AND FLAGGER SIGNS SHALL BE PLACED PER DISTRICT 1 STANDARDS.

8. VARIABLE MESSAGE SIGNS SHALL BE INSTALLED AS DIRECTED BY THE ENGINEER.

9. TEMPORARY MOVEABLE CONCRETE BARRIER WALL SHALL BE LEASED BY IDOT FROM BARRIER SYSTEMS INC.

10. THE CONTRACTOR SHALL REPLACE PRISMATIC BARRIER REFLECTORS ON THE NORTHBOUND I-55 BRIDGE STRUCTURE AS INDICATED ON THE TRAFFIC CONTROL PLAN. THIS WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR MONODIRECTIONAL PRISMATIC BARRIER REFLECTOR.

11. REMOVAL OF EXISTING PAVEMENT MARKINGS SHALL BE PAID FOR AS PAVEMENT MARKING REMOVAL. REMOVAL OF TEMPORARY PAVEMENT MARKING TAPE SHALL BE PAID FO AS WORK ZONE PAVEMENT MARKING REMOVAL.

GENERAL NOTES – DRAINAGE & UTILITIES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL J.U.L.I.E. AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED UTILITIES. (48 HOUR NOTIFICATION REQUIRED).

2. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE DEPARTMENT OF TRANSPORTATION

GENERAL NOTES – LIGHTING

1. CONTRACTOR SHALL RELAMP, CLEAN, AND SERVICE THE EXISTING LUMINAIRES ON THE LIGHT TOWERS NORTH OF WILMINGTON ROAD. NEW SPLICES, FUSES, FUSE HOLDERS, AND SURGE PROTECTORS SHALL ALSO BE PROVIDED FOR THESE LIGHT TOWERS. THIS WORK SHALL BE PAID FOR UNDER •CLEAN AND RELAMP EXISTING LUMINAIRE• AND COMPLETED TO THE SATISFACTION OF THE ENGINEER.

GENERAL NOTES – MISCELLANEOUS

1. ALL ELEVATIONS SHOWN ON THESE PLANS ARE ON THE U.S.G.S DATUM

2. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION FROM THE DEPARTMENT

3. ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUBNUMBER LISTED ON THE INDEX OF SHEETS OR THE COPY OF THE STANDARD INCLUDED IN THESE PLANS.

4. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL •J.U.L.I.E.• AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. 48 HOUR NOTIFICATION IS REQUIRED.

5. RESERVED.

6. RESERVED.

7. RESERVED.

8. THE REMOVAL OF GUARDRAIL TERMINAL SECTIONS SHALL BE INCLUDED IN THE UNIT PRICE PER FOOT FOR •GUARDRAIL REMOVAL.•

9. WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS. .

10. RESERVED.

11. USE #8 EPOXY-COATED TIE BARS, CONFORMING TO ART. 1006.10 OF THE STANDARD SPECIFICATIONS, FOR ALL TIE BARS. USE THE •LONGITUDINAL CONSTRUCTION JOINT (TIE BAR GROUTED IN PLACE)• DETAIL SHOWN ON HIGHWAY STANDARD 420001 FOR ALL LONGITUDINAL JOINTS AND FOR TYING PCC PAVEMENT WIDENING TO EXISTING CONCRETE PAVEMENT AS SHOWN ON THE PLANS.

12. USE #8 EPOXY-COATED TIE BARS, CONFORMING TO ART. 1006.10 OF THE STANDARD SPECIFICATIONS, FOR ALL TIE BARS. USE THE •LONGITUDINAL CONSTRUCTION JOINT (TIE BAR GROUTED IN PLACE)• DETAIL SHOWN ON HIGHWAY STANDARD 420001 FOR ALL LONGITUDINAL JOINTS.

13. ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.

14. ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.

15. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

16. ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

17. RESERVED.

18. DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

19. IT SHALL BE THE CONTRACTOR•S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

20. FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.

21. RESERVED.

22. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847)705-4151 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

23. THE ENGINEER SHALL CONTACT MS. CORA MATHIS, TRAFFIC FIELD ENGINEER, AT (815) 485-6475 A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

24. WHERE SECTION OR SUB-SECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKERS AND MONUMENTS UNTIL THE OWNER, AN AUTHORIZED SURVEYOR OR AGENT HAS WITNESSED THEIR LOCATION.

25. THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM THE BUREAU OF MAINTENANCE (or CONSTRUCTION or BRIDGE INSPECTORS).

26. RESERVED.

27.RESERVED.

28. THE PROPOSED HOT-MIX ASPHALT RESURFACING SHALL BE TAPERED OUT AT A RATE OF TWENTY (20) FEET PER INCH OF THICKNESS AT MAINLINE AND MAJOR SIDE STREET LOCATIONS, EXCEPT WHERE BUTT JOINTS ARE INDICATED. AT MINOR STREETS AND ENTRANCES, TAPER OUT IN TEN (10) FEET EXCEPT WHERE BUTT JOINTS ARE INDICATED. AT SEAL COATED STREETS AND ENTRANCES, TAPER OUT IN THREE (3) FEET UNLESS OTHERWISE SHOWN ON THE PLANS.

29. RESERVED.

30. RESERVED.

31. THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF PLATED STRUCTURES BY STATION AND OFFSET LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT.

32. THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

33. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

34. FOR FRAMES AND LIDS ADJUSTMENT WITHOUT MILLING, REUSE EXISTING FRAME AND LID UNLESS OTHERWISE SPECIFIED IN THE PLANS.

35. ALL CLASS B PAVEMENT PATCHING WHICH REQUIRES FRAMES AND GRATES TO BE ADJUSTED SHALL BE CONSTRUCTED UTILIZING •CAST IN PLACE• ALTERNATE ACCORDING TO STATE HIGHWAY STANDARD 420111 FOR •PCC PAVEMENT ROUNDOUTS•.

36. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE NOTIFICATION OF ALL EMERGENCY SERVICES, SCHOOL DISTRICTS, I.D.O.T.•S COMMUNICATIONS CENTER, SPRINGFIELD TRUCK PERMIT SECTION AND OTHER AGENCIES AFFECTED BY THE CLOSURE. THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR POSTING SIGNS THAT WILL INDICATE THE DATES THE CLOSURE WILL BE IN PLACE.

37. THE ENGINEER SHALL OBTAIN APPROVAL FROM THE DISTRICT ONE BUREAU CHIEF OF MAINTENANCE FOR ANY PROPOSED REVISIONS IN THE SCHEDULED PAVEMENT PATCHING LOCATIONS.

38. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

39. RESERVED.

37.PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III AND ITS REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING.

38. THE •ADVANCED WARNING SIGN DETAIL FOR ARTERIAL TRAFFIC• LOCATED IN THE SPECIAL PROVISION FOR •TEMPORARY INFORMATION SIGNING FOR LANE CLOSURES• (AND THE SPECIAL PROVISION FOR •PUBLIC CONVENIENCE AND SAFETY•) IS/ARE APPLICABLE ONLY TO ARTERIAL ROADS AND SHALL NOT BE APPLIED TO EXPRESSWAYS/TOLLWAYS.

GENERAL NOTES – EROSION CONTROL & LANDSCPING

1. ALL EROSION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND OF THE APPLICABLE STATE STANDARDS FOR THE ENTIRE DURATION OF THE CONTRACT, OR UNTIL SUCH TIME AS DIRECTED BY THE ENGINEER.

2. ALL AREAS DISTURBED AND RESTORED WITH SEEDING SHALL BE COVERED WITH AN EROSION CONTROL BLANKET IN ACCORDANCE WITH SECTION 251 OF THE STANDARD SPECIFICATIONS.

3. SEEDING WILL NOT BE PERMITTED AT ANY TIME WHEN THE GROUND IS FROZEN, WET, OR IN AN UNTILLABLE CONDITION. LOCATIONS TO BE SEEDED ARE AS OUTLINED IN THESE PLANS. ANY ADDITIONAL AREAS SHALL BE DETERMINED BY THE ENGINEER.

3. BEFORE STARTING CONSTRUCTION ON STAGE 2 OF THE PROJECT, EROSION CONTROL BLANKET AND SEEDING, CLASS 3 SHALL BE PLACED AFTER FINAL GRADING HAS BEEN COMPLETED ON THE FIRST PHASE OF CONSTRUCTION.

4. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS FOR THE PROTECTION OF EXISTING PLANT MATERIAL FOR WHICH THE CONTRACT DOES NOT PROVIDE REMOVAL. THE PROTECTION OF EXISTING PLANT MATERIAL AND THE REPAIR OR REPLACEMENT OF EXISTING PLANT MATERIAL DAMAGED BY THE CONTRACTOR SHALL BE DONE IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 201 OF THE STANDARD SPECIFICATIONS.

5. LANDSCAPE CONTRACTOR SHALL STAKE THE LOCATION OF ALL SEEDING AND HAVE ALL PLANTING LAYOUTS APPROVED BY THE ENGINEER PRIOR TO INSTALLATION.

6. APPROVED WATERING EQUIPMENT SHALL BE AT THE SITE OF THE WORK AND IN OPERATING CONDITION PRIOR TO STARTING THE SEEDING OPERATION AND DURING ALL SEEDING OPERATIONS OR SEEDING WILL NOT BE ALLOWED.

7. THE CONTRACTOR SHALL REPAIR IN KIND ANY AREAS DAMAGED AS A RESULT OF LANDSCAPING OPERATIONS

8. THE FINISHED EARTHWORK SHALL HAVE A VEGETATION SUSTAINING SOIL COVERING THE TOP FOUR (4) INCHES IN AREAS TO BE SEEDED OR SODDED.

PLOT DATE = 6/25/2009
FILE NAME = K:\IIB2518\ST LOUIS\I55-18-879\L0 Current\L2 Sheets\182518_Gen.dgn
PLOT SCALE = PLT_SCALE
USER NAME = Administrator
MODEL = Default

rjngroup
Excellence through Ownership

200 West Front Street
Wheaton, Il 60187

DESIGNED - KSD		REVISED -
DRAWN - KSD		REVISED -
CHECKED - CMJ		REVISED -
DATE - 03-04-09		REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-55 OVER THE KANKAKEE RIVER
GENERAL NOTES

SCALE: NTS | SHEET NO. 1 OF 1 SHEETS | STA. ---- | TO STA. ----

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	3
		CONTRACT NO. 62930		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

SUMMARY OF QUANTITIES

	CODE NO.	ITEM DESCRIPTION	UNIT	URBAN TOTAL QUANTITY	CONSTRUCTION TYPE CODE: ROADWAY 1000-2A QUANTITY	BRIDGE: SN 099-0001 X071-2A QUANTITY	BRIDGE: SN 099-0002 X071-2A QUANTITY
	20200100	EARTH EXCAVATION	CU YD	3,610	3,610		
	~~20201550~~	~~SUB-BASE GRANULAR MATERIAL, TYPE B~~	~~CU YD~~	~~783~~	~~783~~		
	20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	500	500		
	20400800	FURNISHED EXCAVATION	CU YD	200	200		
	20800150	TRENCH BACKFILL	CU YD	120	120		
	25000210	SEEDING, CLASS 2A	ACRE	0.56	0.56		
	25000400	NITROGEN FERTILIZER NUTRIENT	POUND	50	50		
	25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	50	50		
	25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	50	50		
	25100630	EROSION CONTROL BLANKET	SQ YD	2,713	2,713		
	28000500	INLET AND PIPE PROTECTION	EACH	4	4		
	40600895	CONSTRUCTING TEST STRIP	EACH	1	1		
	40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	898	898		
	40603595	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	TON	72	72		
	40800020	BITUMINOUS MATERIALS (PRIME COAT)	TON	5	5		
	42001300	PROTECTIVE COAT	SQ YD	8,802		4,353	4,449
	44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	10,680	10,680		
	44000156	HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"	SQ YD	2,250	2,250		
	44004250	PAVED SHOULDER REMOVAL	SQ YD	2,670	2,670		
	48203003	HOT-MIX ASPHALT SHOULDERS, 1 1/2''	SQ YD	10,680	10,680		
	50102400	CONCRETE REMOVAL	CU YD	93		85	8
	50104400	CONCRETE HEADWALL REMOVAL	EACH	10	10		
*	50200100	STRUCTURE EXCAVATION	CU YD	10		10	
	50300225	CONCRETE STRUCTURES	CU YD	46.8		17.3	29.5
	50300255	CONCRETE SUPERSTRUCTURE	CU YD	90.8		90.8	
	50300260	BRIDGE DECK GROOVING	SQ YD	8,306		4,087	4,219
	50500505	STUD SHEAR CONNECTORS	EACH	19		19	
NP	55039700	STORM SEWERS TO BE CLEANED	FOOT	850	850		
	50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	18,740		15,650	3,090
	52000110	PREFORMED JOINT STRIP SEAL	FOOT	92		92	
	52000325	NEOPRENE EXPANSION JOINT, 2 1/2"	FOOT	97			97
	52000340	NEOPRENE EXPANSION JOINT, 4"	FOOT	191		94	97
	542A0217	PIPE CULVERTS, CLASS A, TYPE 1 12"	FOOT	339	339		
	542A0220	PIPE CULVERTS, CLASS A, TYPE 1 15"	FOOT	475	475		
	542A0223	PIPE CULVERTS, CLASS A, TYPE 1 18"	FOOT	30	30		
	54213657	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12"	EACH	2	2		
	54213660	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 15"	EACH	1	1		
	54247090	GRATING FOR CONCRETE FLARED END SECTION 12"	EACH	2	2		
	54247100	GRATING FOR CONCRETE FLARED END SECTION 15"	EACH	1	1		
	59000200	EPOXY CRACK INJECTION	FOOT	902		627	275
	60100060	CONCRETE HEADWALL FOR PIPE DRAINS	EACH	4	4		
	60107600	PIPE UNDERDRAINS 4"	FOOT	4,000	4,000		
	60236200	INLETS, TYPE A, TYPE 8 GRATE	EACH	3	3		
	60615400	PAVED DITCH, TYPE A-15	FOOT	517	517		
	63801200	MODULAR GLARE SCREEN SYSTEM	FOOT	4,300	4,300		
	67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	12	12		
	67100100	MOBILIZATION	L SUM	1	1		
	~~70100405~~	~~TRAFFIC CONTROL AND PROTECTION, STANDARD 701321~~	~~EACH~~	~~1~~	~~1~~		
	70106800	CHANGEABLE MESSAGE SIGN	CAL MO	12	12		

	CODE NO.	ITEM DESCRIPTION	UNIT	URBAN TOTAL QUANTITY	CONSTRUCTION TYPE CODE: ROADWAY 1000-2A QUANTITY	BRIDGE: SN 099-0001 X071-2A QUANTITY	BRIDGE: SN 099-0002 X071-2A QUANTITY
	70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	12,250	12,250		
	70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	12,370	12,370		
	70400100	TEMPORARY CONCRETE BARRIER	FOOT	6,125	6,125		
	70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	6,125	6,125		
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	20,080	20,080		
*	78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	2,690	2,690		
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	660	660		
*	78003120	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 5"	FOOT	4,450	4,450		
*	78005110	EPOXY PAVEMENT MARKING-LINE 4"	FOOT	5,220	5,220		
*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	350	350		
	78300100	PAVEMENT MARKING REMOVAL	SQ FT	6,430	6,430		
	78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	350	350		
	X0320887	POLYMER CONCRETE	CU FT	0.5			0.5
	X0322185	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 1/4 INCHES	SQ YD	8,611		4,162	4,449
	X0323017	TEMPORARY INFORMATIONAL SIGNS	EACH	55	55		
	X0324744	REMOVAL OF EXISTING PRECAST CONCRETE UNITS	SQ FT	180		180	
	X0325085	TEMPORARY PAVEMENT (INTERSTATE)	SQ YD	7,044	7,044		
	X0325303	STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)	SQ FT	17		17	
	X0325305	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	621		531	90
	X0325349	TEMPORARY CONCRETE BARRIER (TO REMAIN PERMANENTLY)	FOOT	1,038	1,038		
	X0325416	TRAFFIC CONTROL AND PROTECTION FOR DETOUR ROUTE	L SUM	1	1		
	X0325426	PORTLAND CEMENT CONCRETE SURFACE REMOVAL 1 3/4"	SQ YD	850	850		
*	X0325590	HIGH TENSION CABLE MEDIAN BARRIER TERMINALS	EACH	4	4		
	X0325775	WET REFLECTIVE TEMPORARY TAPE, TYPE III, 4 INCH	FOOT	37,110	37,110		
	X4067107	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	36	36		
	X5121800	PERMANENT STEEL SHEET PILING	SQ FT	468		468	
	X7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	L SUM	1	1		
	X7013820	TRAFFIC CONTROL SURVEILLANCE, EXPRESSWAYS	CAL DA	~~80~~	~~80~~		
	X0003686	REMOVE EXISTING CONCRETE END SECTION	EACH	1	1		
*	X0007164	CLEAN AND RELAMP EXISTING LUMINAIRE	EACH	57	57		
	~~Z0002820~~	~~BARRICADES, TYPE III~~	~~WEEK~~	~~60~~	~~60~~		
	Z0006201	BRIDGE DECK HYDRO-SCARIFICATION 1"	SQ YD	8,611		4,162	4,449
	Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1		
NP	Z0014700	CULVERT TO BE CLEANED	EACH	14	14		
	Z0016001	DECK SLAB REPAIR (FULL DEPTH, TYPE I)	SQ YD	6		5	1
	Z0016002	DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	238		238	
NP	Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	14	14		
	Z0030150	IMPACT ATTENUATORS (NON-REDIRECTIVE), TEST LEVEL 3	EACH	4	4		
	Z0030250	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2	2		
	Z0030350	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3	EACH	2	2		
	Z0040530	PIPE UNDERDRAIN REMOVAL	FOOT	4,000	4,000		
	Z0043800	PRECAST PRESTRESSED CONCRETE I-BEAM REPAIR	SQ FT	11.5		11.5	
	Z0065740	SLOTTED DRAIN 12" WITH VARIABLE SLOT	FOOT	466	466		
⊙	Z0076600	TRAINEES	HOUR	1000	1000		
	Z0076870	UNDERDRAIN CONNECTION TO STRUCTURE	EACH	4	4		
*	X0326676	REPLACE SURFACE SENSOR AND TEMPERATURE PROBE	L SUM	1		1	
*	X0326677	REMOVE HIGH TENSION CABLE MEDIAN BARRIER	FOOT	2,390	2,390		
	Z0001050	AGGREGATE SUBGRADE, 12"	SQ YD	7,044	7,044		
	Z0065760	SLOTTED DRAIN 15" WITH VARIABLE SLOT	FOOT	573	573		

* SPECIALTY ITEM

NP = NON-PARTICIPATING

⊙ = Y080

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DRAWN	KSD	REVISED	-
CHECKED	CMJ	REVISED	-
DATE	03-04-09	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-55 OVER THE KANKAKEE RIVER
SUMMARY OF QUANTITIES

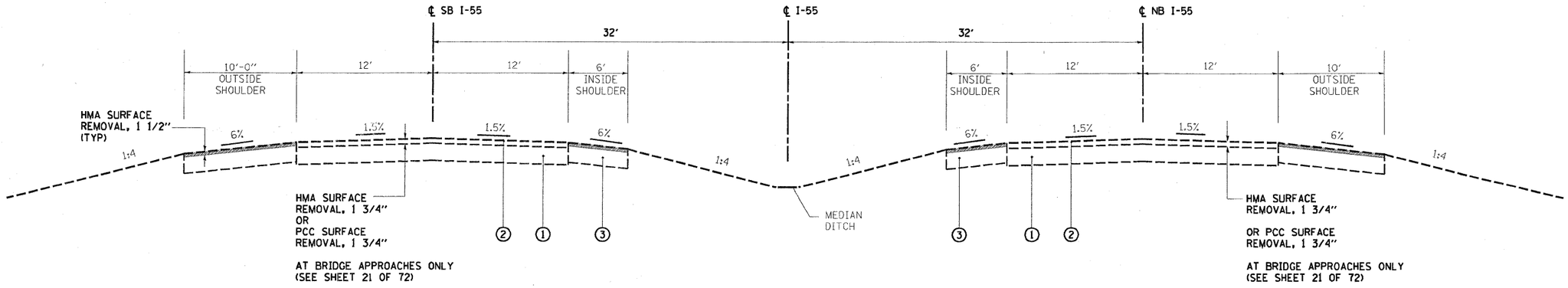
SCALE: NTS | SHEET NO. 1 OF 1 SHEETS | STA. ---- | TO STA. ----

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	4

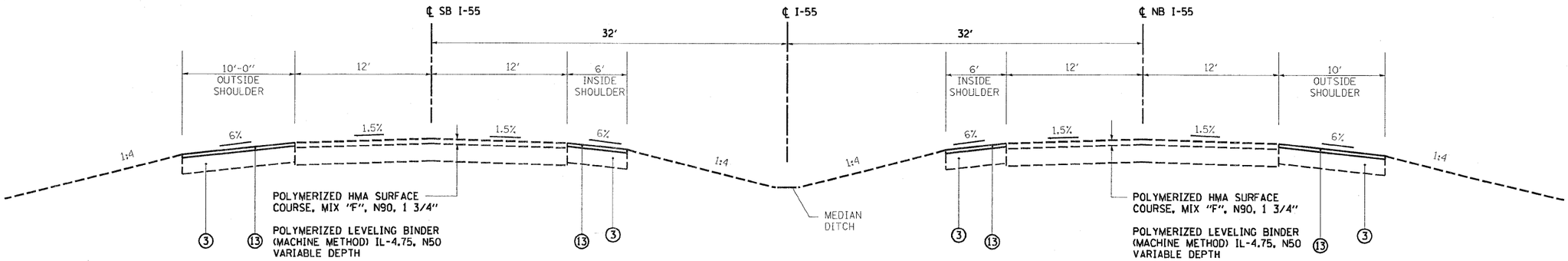
CONTRACT NO. 62930

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT

Rev



EXISTING ROADWAY
TYPICAL SECTION



PROPOSED ROADWAY
TYPICAL SECTION

INSIDE SHOULDER MILLING & RESURFACING:
STA 42+50.00 TO 59+97.50
STA 73+00.00 TO 76+50.00

OUTSIDE SHOULDER MILLING & RESURFACING:
STA 39+00.00 TO 59+97.50
STA 73+00.00 TO 87+50.00

LEGEND
1. EXISTING PCC PAVEMENT, 10"
2. EXISTING HOT-MIX ASPHALT SURFACE, VARIES
3. EXISTING HOT-MIX ASPHALT SHOULDER, 10"
13. PROPOSED HOT-MIX ASPHALT SHOULDER, 1 1/2"

HOT-MIX ASPHALT MIXTURE

MIXTURE TYPE	AC TYPE	AIR VOIDS	MAX RAP %
HMA SURFACE COURSE, MIX "D", N70 (IL 9.5 mm)	PG 64-22	4% @ 70 Gyr.	15
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	SBS/SBR PG 76-28/-22	4% @ 50 Gyr.	15
HMA BINDER, 13" (HMA BINDER IL-19mm)	PG 64-22/58-22*	2% @ 30 Gyr.	50
HMA SHOULDERS, 10" (HMA BINDER IL-19mm)	PG 64-22/58-22*	2% @ 30 Gyr.	50
POLYMERIZED HMA SURFACE COURSE, MIX "F", N90	SBS/SBR PG 70-22	4% @ 90 Gyr.	10
STABILIZED SUBBASE (HMA IL-19mm)	PG 64-22/58-22*	2% @ 30 Gyr.	50

THE UNIT WEIGHT TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112LBS/SY/IN
*WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22.

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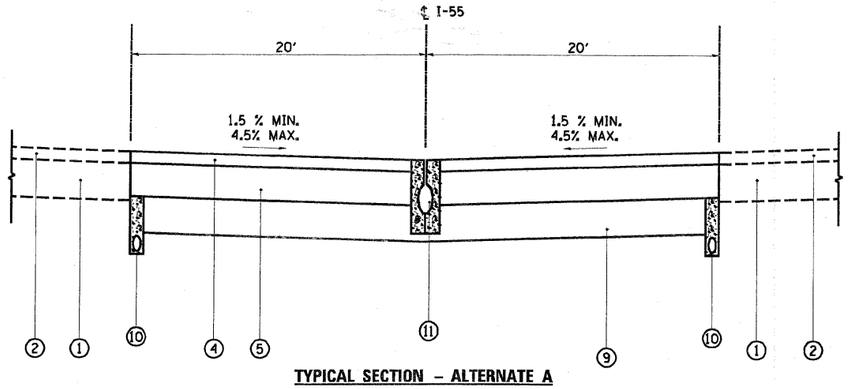
DATE - 03-04-09	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-55 OVER THE KANKAKEE RIVER
TYPICAL SECTIONS

SCALE: NTS | SHEET NO. 1 OF 2 SHEETS | STA. ---- | TO STA. ----

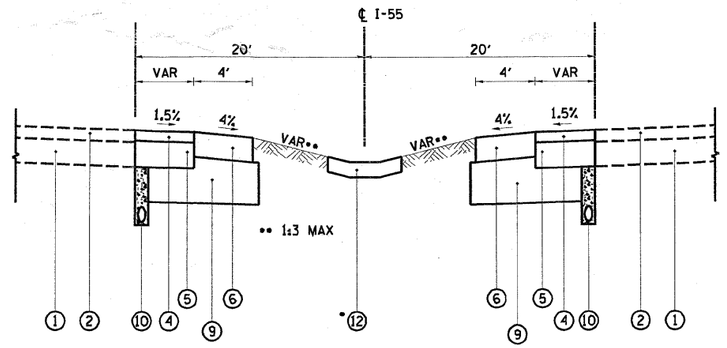
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	5
		CONTRACT NO. 62930		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



TYPICAL SECTION – ALTERNATE A

STA 35+97.64 TO 40+63.34
STA 78+93.80 TO 84+65.56

ALTERNATE A
HMA CROSSOVERS



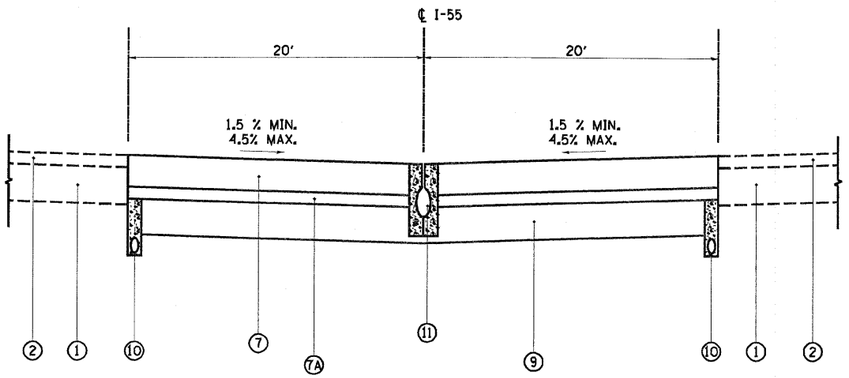
TYPICAL SECTION – ALTERNATE A

STA 33+50.00 TO 35+97.84
STA. 40+63.34 TO 42+50.00
STA 76+50.00 TO 78+93.80
STA 84+65.56 TO 87+50.00

• SEE PLAN AND PROFILE FOR LOCATION OF STORM SEWER AND PAVED DITCH

CONTRACTOR SHALL BID ACCORDING TO EITHER ALTERNATE A OR ALTERNATE B. PAVEMENT MATERIAL IN EITHER ALTERNATE WILL BE MEASURED AND PAID FOR PER SQUARE YARD AS TEMPORARY PAVEMENT (INTERSTATE). SEE SPECIAL PROVISIONS.

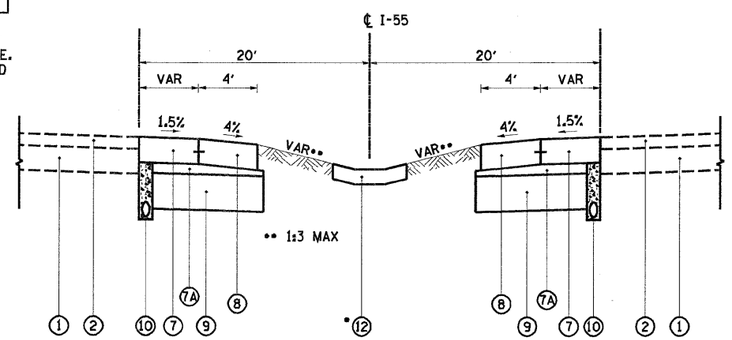
CROSS-SECTIONS (SHEETS 57-64) MAY NOT MATCH THE TYPICAL SECTIONS SHOWN HERE. PLAN QUANTITIES WERE CALCULATED BASED ON TYPICAL SECTIONS SHOWN HERE.



TYPICAL SECTION – ALTERNATE B

STA 35+97.64 TO 40+63.34
STA 78+93.80 TO 84+65.56

ALTERNATE B
PCC CROSSOVERS



TYPICAL SECTION – ALTERNATE B

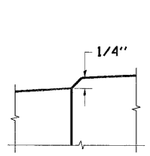
STA 33+50.00 TO 35+97.84
STA. 40+63.34 TO 42+50.00
STA 76+50.00 TO 78+93.80
STA 84+65.56 TO 87+50.00

LEGEND

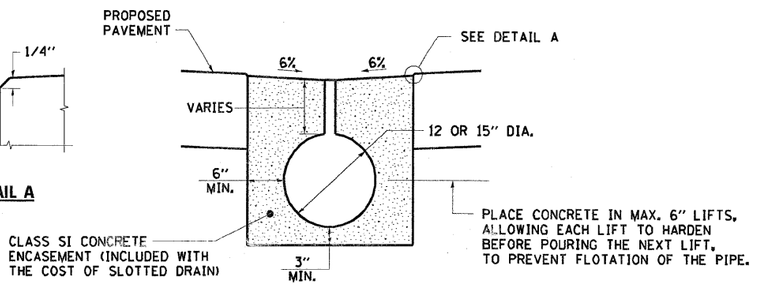
	①	EXISTING PCC PAVEMENT, 10"
	②	EXISTING BITUMINOUS CONCRETE SURFACE, VARIES
	③	EXISTING BITUMINOUS SHOULDER, 10"
ALT. A	④	PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
ALT. A	⑤	PROPOSED HOT-MIX ASPHALT BINDER COURSE, 13"
ALT. A	⑥	PROPOSED HOT-MIX ASPHALT SHOULDER, 10"
ALT. B	⑦	PROPOSED JPCC PAVEMENT, 10"
ALT. B	⑦A	PROPOSED STABILIZED SUBBASE, 4 1/2"
ALT. B	⑧	PROPOSED PCC SHOULDER, 10" (HINGED)
	⑨	PROPOSED AGGREGATE SUBGRADE, 12"
	⑩	PIPE UNDERDRAINS, 4" (PERFORATED PVC PIPE BACKFILLED WITH FA-1 OR FA-2)
	⑪	SLOTTED DRAIN
	⑫	PAVED DITCH TYPE A-15

NOTE

~~PCC SHOULDERS IN ALTERNATE B SHALL BE ASSEMBLED ACCORDING TO STD. 483001. WHEN SUBBASE GRANULAR MATERIAL TYPE B IS SUBSTITUTED FOR TYPE A, ALL CONCRETE PAVEMENT SHALL BE CLASS PV.~~



DETAIL A



SLOTTED DRAIN DETAIL

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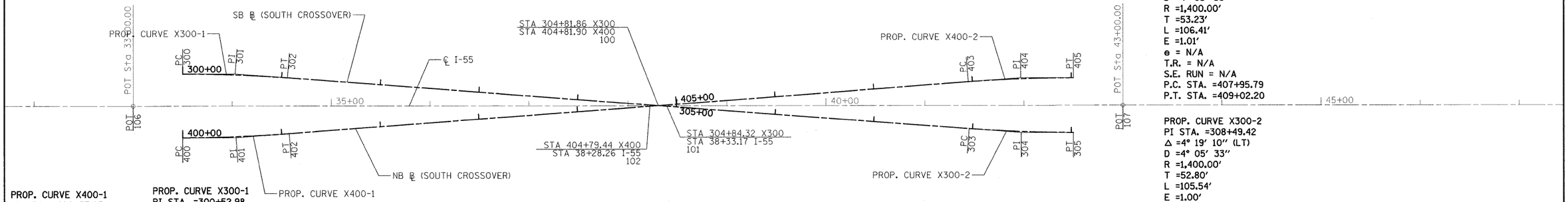
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-55 OVER THE KANKAKEE RIVER
CROSSOVER TYPICAL SECTIONS

SCALE: NTS | SHEET NO. 2 OF 2 SHEETS | STA. ---- | TO STA. ----

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	6
		CONTRACT NO. 62930		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

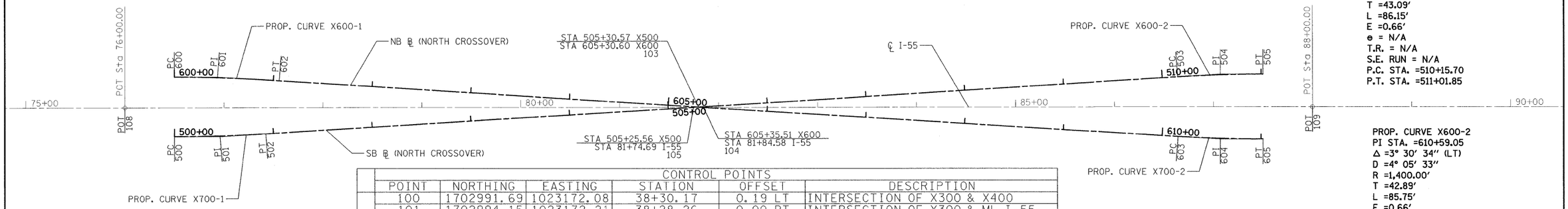


PROP. CURVE X400-1
PI STA. =400+53.83
Δ =4° 21′ 38″ (LT)
D =4° 03′ 07″
R =1,414.00′
T =53.83′
L =107.61′
E =1.02′
e = N/A
T.R. = N/A
S.E. RUN = N/A
P.C. STA. =400+00.00
P.T. STA. =401+07.61

PROP. CURVE X300-1
PI STA. =300+52.98
Δ =4° 20′ 04″ (RT)
D =4° 05′ 33″
R =1,400.00′
T =52.98′
L =105.91′
E =1.00′
e = N/A
T.R. = N/A
S.E. RUN = N/A
P.C. STA. =300+00.00
P.T. STA. =301+05.91

PROP. CURVE X400-2
PI STA. =408+49.02
Δ =4° 21′ 18″ (RT)
D =4° 05′ 33″
R =1,400.00′
T =53.23′
L =106.41′
E =1.01′
e = N/A
T.R. = N/A
S.E. RUN = N/A
P.C. STA. =407+95.79
P.T. STA. =409+02.20

PROP. CURVE X300-2
PI STA. =308+49.42
Δ =4° 19′ 10″ (LT)
D =4° 05′ 33″
R =1,400.00′
T =52.80′
L =105.54′
E =1.00′
e = N/A
T.R. = N/A
S.E. RUN = N/A
P.C. STA. =307+96.62
P.T. STA. =309+02.16



PROP. CURVE X600-1
PI STA. =600+43.48
Δ =3° 33′ 29″ (RT)
D =4° 05′ 33″
R =1,400.00′
T =43.48′
L =86.94′
E =0.68′
e = N/A
T.R. = N/A
S.E. RUN = N/A
P.C. STA. =600+00.00
P.T. STA. =600+86.94

PROP. CURVE X500-1
PI STA. =500+46.16
Δ =3° 46′ 36″ (LT)
D =4° 05′ 33″
R =1,400.00′
T =46.16′
L =92.28′
E =0.76′
e = N/A
T.R. = N/A
S.E. RUN = N/A
P.C. STA. =500+00.00
P.T. STA. =500+92.28

PROP. CURVE X500-2
PI STA. =510+58.79
Δ =3° 31′ 33″ (RT)
D =4° 05′ 33″
R =1,400.00′
T =43.09′
L =86.15′
E =0.66′
e = N/A
T.R. = N/A
S.E. RUN = N/A
P.C. STA. =510+15.70
P.T. STA. =511+01.85

PROP. CURVE X600-2
PI STA. =610+59.05
Δ =3° 30′ 34″ (LT)
D =4° 05′ 33″
R =1,400.00′
T =42.89′
L =85.75′
E =0.66′
e = N/A
T.R. = N/A
S.E. RUN = N/A
P.C. STA. =610+16.16
P.T. STA. =611+01.91

CONTROL POINTS

	POINT	NORTHING	EASTING	STATION	OFFSET	DESCRIPTION
MISC	100	1702991.69	1023172.08	38+30.17	0.19 LT	INTERSECTION OF X300 & X400
	101	1702994.15	1023172.21	38+28.26	0.00 RT	INTERSECTION OF X300 & ML I-55
	102	1702989.25	1023172.31	38+33.17	0.00 RT	INTERSECTION OF X400 & ML I-55
	103	1707339.72	1023081.78	81+79.68	0.31 LT	INTERSECTION OF X500 & X600
	104	1707344.62	1023081.98	81+74.69	0.00 RT	INTERSECTION OF X600 & ML I-55
	105	1707334.74	1023082.19	81+35.51	0.00 RT	INTERSECTION OF X500 & ML I-55
	106	1702461.10	1023183.40	33+00.00	0.00 RT	P.O.T. ML I-55
	107	1703460.88	1023162.41	43+00.00	0.00 RT	P.O.T. ML I-55
	108	1706760.18	1023094.26	76+00.00	0.00 RT	P.O.T. ML I-55
	109	1707959.91	1023069.05	88+00.00	0.00 RT	P.O.T. ML I-55
X 300	300	1702510.43	1023149.71	300+00.00	0.00 RT	P.C. X300
	301	1702563.40	1023148.61	300+52.98	1.00 LT	P.I. X300
	302	1702616.30	1023151.51	301+05.91	0.00 RT	P.T. X300
	303	1703305.97	1023189.30	307+96.62	0.00 RT	P.C. X300
	304	1703358.69	1023192.19	308+49.42	1.00 RT	P.I. X300
	305	1703411.48	1023191.10	309+02.16	0.00 RT	P.T. X300
X 400	400	1702511.76	1023214.60	400+00.00	0.00 RT	P.C. X400
	401	1702565.58	1023213.49	400+53.83	1.02 RT	P.I. X400
	402	1702619.16	1023208.28	401+07.61	0.00 RT	P.T. X400
	403	1703304.10	1023141.71	407+95.79	0.00 RT	P.C. X400
	404	1703357.09	1023136.57	408+49.02	1.01 LT	P.I. X400
	405	1703410.31	1023135.46	409+02.20	0.00 RT	P.T. X400
X 500	500	1706810.77	1023122.37	500+00.00	0.00 RT	P.C. X500
	501	1706856.92	1023121.61	500+46.16	0.76 RT	P.I. X500
	502	1706902.93	1023117.81	500+92.28	0.00 RT	P.T. X500
	503	1707823.22	1023041.89	510+15.70	0.00 RT	P.C. X500
	504	1707866.16	1023038.35	510+58.79	0.66 LT	P.I. X500
	505	1707909.24	1023037.45	511+01.85	0.00 RT	P.T. X500
X 600	600	1706809.52	1023062.45	600+00.00	0.00 RT	P.C. X600
	601	1706853.00	1023061.55	600+43.48	0.68 LT	P.I. X600
	602	1706896.44	1023063.36	600+84.94	0.00 RT	P.T. X600
	603	1707824.87	1023101.94	610+16.16	0.00 RT	P.C. X600
	604	1707867.72	1023103.72	610+59.05	0.66 RT	P.I. X600
	605	1707910.60	1023102.87	611+01.91	0.00 RT	P.T. X600

BENCHMARKS:

1. CHISELED "X" ON TOP OF CONCRETE PARAPET,
   SWW CORNER OF SB BRIDGE
   ELEVATION = 533.22

2. TOP OF LARGE CONCRETE LIGHT POLE BASE
   FOUNDATION IN MEDIAN ± STA. 9+85
   ELEVATION = 528.43

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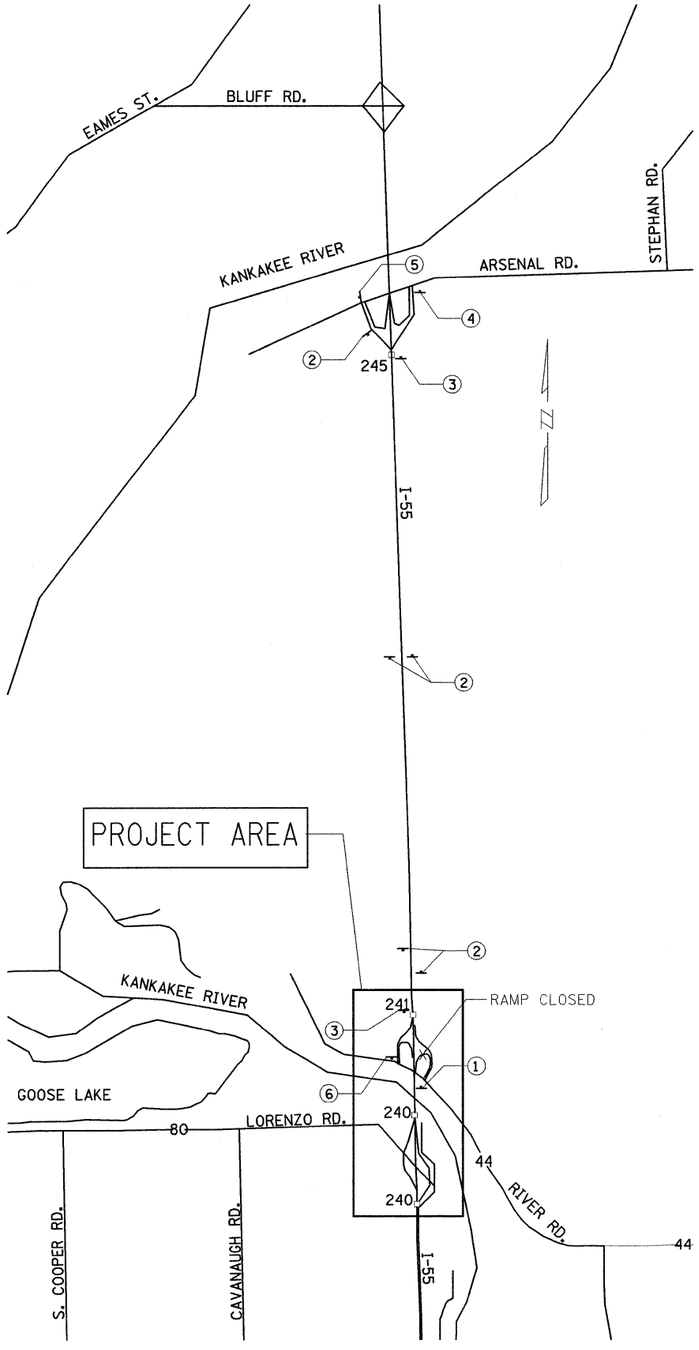
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-55 OVER THE KANKAKEE RIVER
ALIGNMENTS & TIES

SCALE: 1″ = 50′ | SHEET NO. 1 OF 1 SHEETS | STA. ---- | TO STA. ----

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	7
		CONTRACT NO. 62930		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



① DETOUR AHEAD
W20-2(0)-48
W/TYPE 2 FLASHING LIGHT

② RIVER ROAD / DETOUR — M4-8(0) 3015; M6-3(0) 3018

③ RIVER ROAD / DETOUR — M4-8(0) 3015; M6-2R(0) 3018

④ RIVER ROAD / DETOUR — M4-8(0) 3015; M5-1L(0) 3018

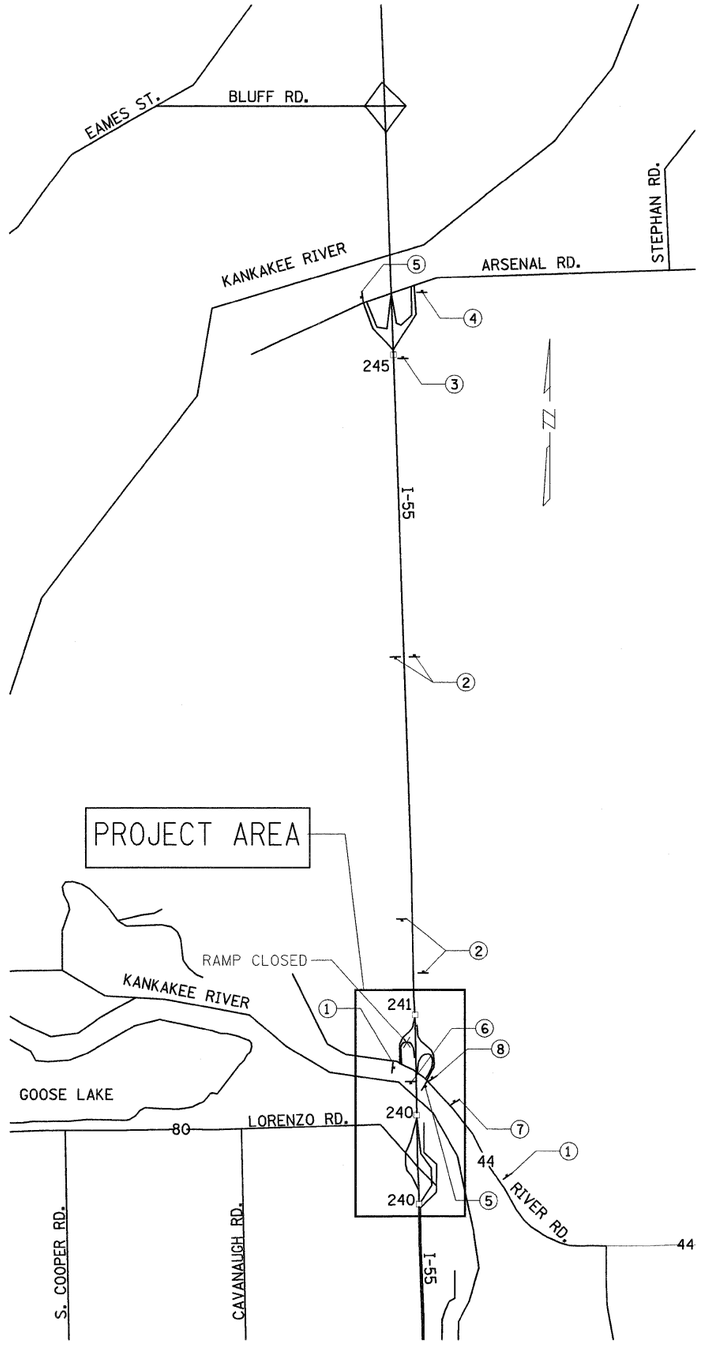
⑤ RIVER ROAD / DETOUR — M4-8(0) 3015; M6-1(0) 3018

⑥ END / DETOUR — M4-6(0) 3015; M4-8(0) 3015

SIGN

STAGE II DETOUR SIGNING

NOTE: REFER TO DISTRICT 1 DETAIL "FREEWAY ENTRANCE AND EXIT RAMP CLOSURE DETAILS" FOR ADDITIONAL SIGNAGE.



① DETOUR AHEAD
W20-2(0)-48
W/TYPE 2 FLASHING LIGHT

② SOUTH 55 M3-3; M1-1 / DETOUR — M4-8(0) 3015; M6-3(0) 3018

③ SOUTH 55 M3-3; M1-1 / DETOUR — M4-8(0) 3015; M6-2R(0) 3018

④ SOUTH 55 M3-3; M1-1 / DETOUR — M4-8(0) 3015; M5-1L(0) 3018

⑤ SOUTH 55 M3-3; M1-1 / DETOUR — M4-8(0) 3015; M6-1(0) 3018

⑥ END / DETOUR — M4-6(0) 3015; M4-8(0) 3015

⑦ SOUTH 55 M3-3; M1-1 / DETOUR — M4-8(0) 3015; M5-1R(0) 3018

⑧ SOUTH 55 M3-3; M1-1 / DETOUR — M4-8(0) 3015; M6-1(0) 3018

STAGE III DETOUR SIGNING

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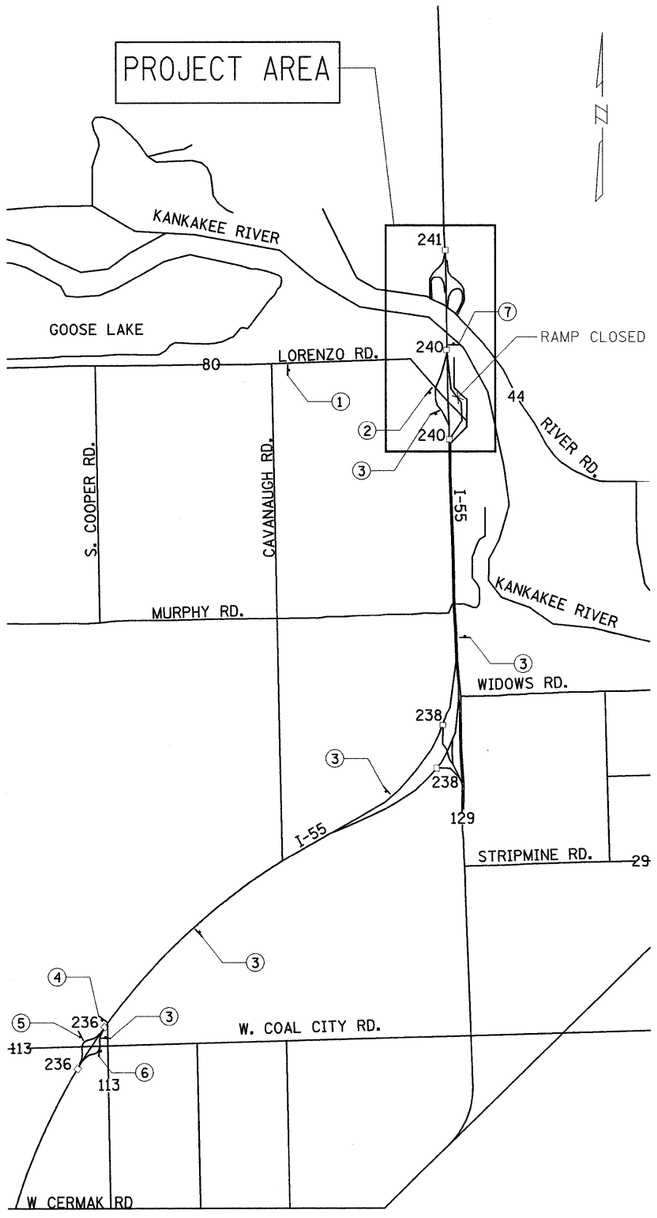
DATE - 03-04-09	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-55 OVER THE KANKAKEE RIVER
RIVER ROAD DETOUR SIGNING

SCALE: NTS | SHEET NO. 1 OF 2 SHEETS | STA. ---- | TO STA. ----

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	8
		CONTRACT NO. 62930		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



① DETOUR AHEAD
W20-2(O)-48
W/TYPE 2 FLASHING LIGHT

② M3-1, M1-1, DETOUR M4-8(O) 3015, M6-1(O) 3018

③ M3-1, M1-1, DETOUR M4-8(O) 3015, M6-3(O) 3018

④ M3-1, M1-1, DETOUR M4-8(O) 3015, M6-2R(O) 3018

⑤ M3-1, M1-1, DETOUR M4-8(O) 3015, M5-1L(O) 3018

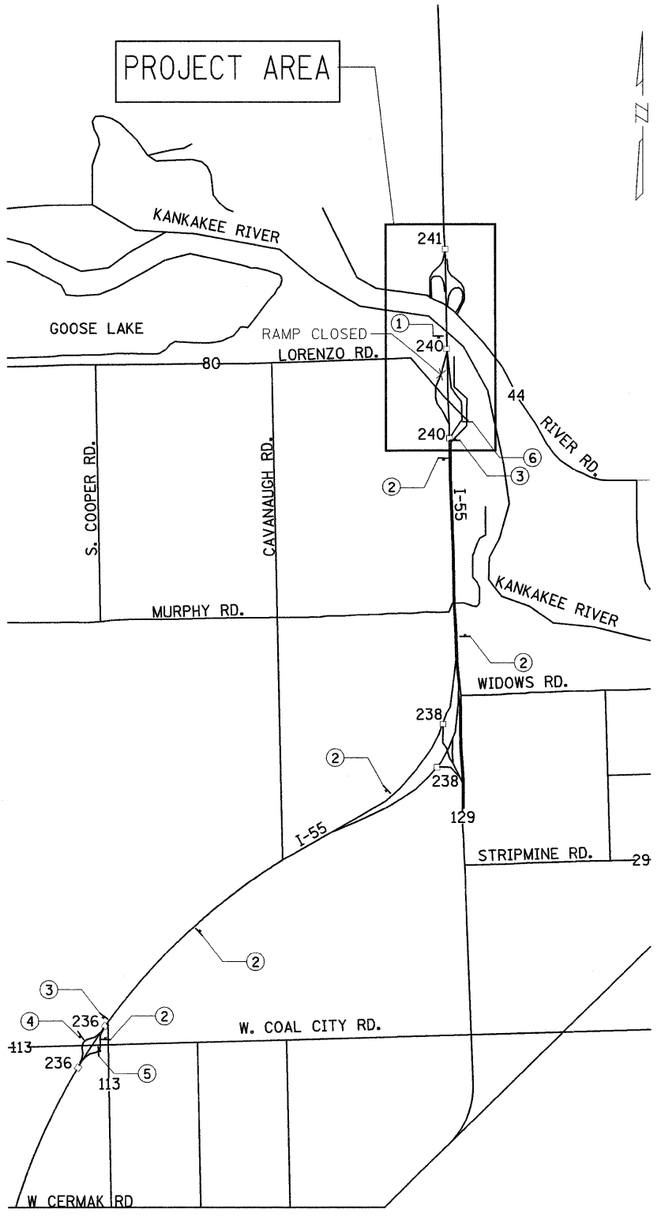
⑥ M3-1, M1-1, DETOUR M4-8(O) 3015, M6-1(O) 3018

⑦ END DETOUR M4-6(O) 3015, M4-8(O) 3015

SIGN

STAGE II DETOUR SIGNING

NOTE: REFER TO DISTRICT 1 DETAIL "FREEWAY ENTRANCE AND EXIT RAMP CLOSURE DETAILS" FOR ADDITIONAL SIGNAGE.



① DETOUR AHEAD
W20-2(O)-48
W/TYPE 2 FLASHING LIGHT

② LORENZO ROAD, DETOUR M4-8(O) 3015, M6-3(O) 3018

③ LORENZO ROAD, DETOUR M4-8(O) 3015, M6-2R(O) 3018

④ LORENZO ROAD, DETOUR M4-8(O) 3015, M5-1L(O) 3018

⑤ LORENZO ROAD, DETOUR M4-8(O) 3015, M6-1(O) 3018

⑥ END DETOUR M4-6(O) 3015, M4-8(O) 3015

STAGE III DETOUR SIGNING

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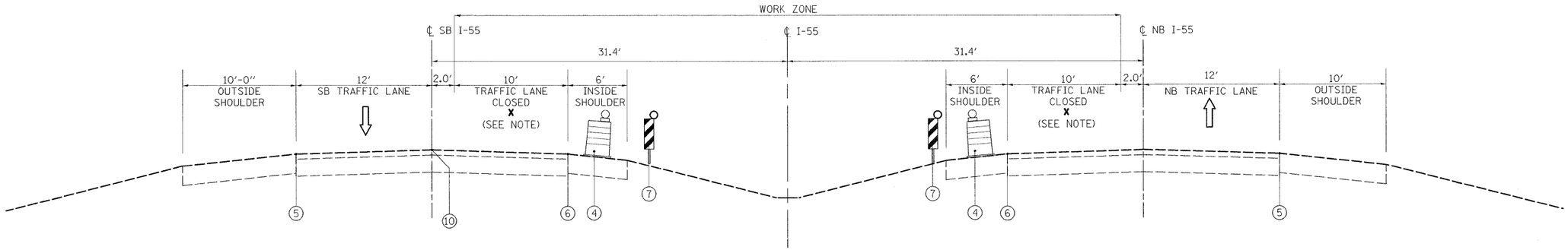
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-55 OVER THE KANKAKEE RIVER
LORENZO ROAD DETOUR SIGNING

SCALE: NTS | SHEET NO. 2 OF 2 SHEETS | STA. ---- | TO STA. ----

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	9
		CONTRACT NO. 62930		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



TYPICAL SECTION – STAGE I B CROSSOVER CONSTRUCTION

(SECTION LOOKING NORTH)

STA 33+50.00 TO 59+97.50

STA 73+00.00 TO 87+50.00

BRIDGE OMMISION

STA 59+97.50 TO 73+00.00

NOTE: INSIDE LANE CLOSURES FOR STAGE I PERMITTED DURING ALLOWABLE HOURS PER TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).

STAGE I (WORK IN MEDIAN)

1. CLOSE INSIDE LANES FOR NB AND SB TRAFFIC USING FREEWAY STANDARD 701401-05: LANE CLOSURE, FREEWAY/EXPRESSWAY.
2. PERFORM EXCAVATION AND CONSTRUCT EMBANKMENT.
3. INSTALL CULVERTS AND DRAINAGE STRUCTURES.
5. CONSTRUCT CROSSOVER PAVEMENT AND SHOULDER IMPROVEMENTS.
6. CLOSE CROSSOVER TO TRAFFIC USING TEMPORARY MOVEABLE BARRIER WALL.

STAGE I (WORK ON SHOULDERS)

1. CLOSE OUTSIDE LANES FOR NB & SB TRAFFIC USING DISTRICT ONE FREEWAY STANDARD 701101: OFF ROAD OPERATIONS, MULTILANE.
2. REPAIR NB AND SB OUTSIDE SHOULDER.
3. TEMPORARY STRIPE PAVEMENT.

LEGEND

(1) WET REFLECTIVE TEMPORARY TAPE TYPE III, 4″ WHITE EDGE LINE
(2) WET REFLECTIVE TEMPORARY TAPE TYPE III, 4″ YELLOW EDGE LINE
(3) PAVEMENT MARKING REMOVAL
(4) DRUMS WITH STEADY BURNING MONODIRECTIONAL LIGHT
(5) EXISTING PAVEMENT MARKING, WHITE EDGE LINE
(6) EXISTING PAVEMENT MARKING, YELLOW EDGE LINE
(7) VERTICAL PANELS (BACK TO BACK)
(8) TEMPORARY CONCRETE BARRIER
(9) WET REFLECTIVE TEMPORARY TAPE TYPE III, 4″ WHITE SOLID CENTER LINE
(10) EXISTING PAVEMENT MARKING, WHITE LANE LINE (10′ LINE, 30′ SKIP)

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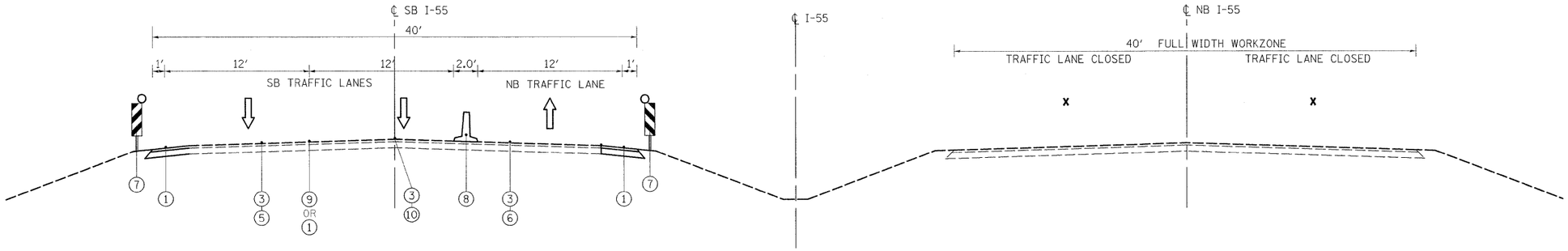
TRAFFIC CONTROL CONSTRUCTION STAGING NOTES & DETAILS (STAGE I)

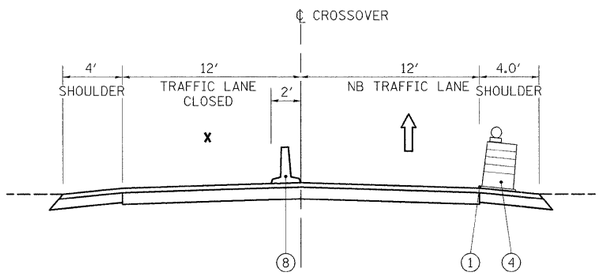
SCALE: NTS | SHEET NO. 1 OF 3 SHEETS | STA. ---- | TO STA. ----

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	10
		CONTRACT NO. 62930		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



TYPICAL SECTION – STAGE II
SOUTH BOUND
(SECTION LOOKING NORTH)
STA. 42+50 TO STA. 76+50

TYPICAL SECTION – STAGE II
(SECTION LOOKING NORTH)
STA. 42+50 TO STA. 76+50



CROSSOVER TYPICAL SECTION – STAGE II

SOUTH BOUND PEAK
(SECTION LOOKING NORTH)

STA. 33+50 TO STA. 42+50
STA. 76+50 TO STA. 87+50

STAGE II (FOR WORK AT NB I–55 BRIDGE)

1. USING DAY LANE CLOSURE, PATCH AND REPAIR ANY NEEDED AREAS OF SOUTHBOUND BRIDGE DECK OR AS DIRECTED BY THE ENGINEER. THIS WORK WILL BE PAID FOR AS BITUMINOUS CONCRETE REMOVAL (DECK) (1 1/2 INCH), BITUMINOUS MATERIALS (PRIME COAT) AND INCIDENTAL BITUMINOUS SURFACING.
2. SHIFT TRAFFIC ONTO SOUTHBOUND PAVEMENT IN ACCORDANCE WITH TRAFFIC PLAN AND DISTRICT ONE STANDARD TC-09 "FREEWAY SINGLE AND MULTILANE WEAVE".
3. PERFORM STRUCTURAL REHABILITATION ON NB 1-55 BRIDGE.
4. LANDSCAPE RESTORATION.
5. TEMPORARY STRIPE PAVEMENT.

LEGEND

(1) WET REFLECTIVE TEMPORARY TAPE TYPE III, 4" WHITE EDGE LINE
(2) WET REFLECTIVE TEMPORARY TAPE TYPE III, 4" YELLOW EDGE LINE
(3) PAVEMENT MARKING REMOVAL
(4) DRUMS WITH STEADY BURNING MONODIRECTIONAL LIGHT
(5) EXISTING PAVEMENT MARKING, WHITE EDGE LINE
(6) EXISTING PAVEMENT MARKING, YELLOW EDGE LINE
(7) VERTICAL PANELS (BACK TO BACK)
(8) TEMPORARY CONCRETE BARRIER
(9) WET REFLECTIVE TEMPORARY TAPE TYPE III, 4" WHITE (10' LINE, 30' SPACE) LINE
(10) EXISTING PAVEMENT MARKING, WHITE LANE LINE (10' LINE, 30' SKIP)

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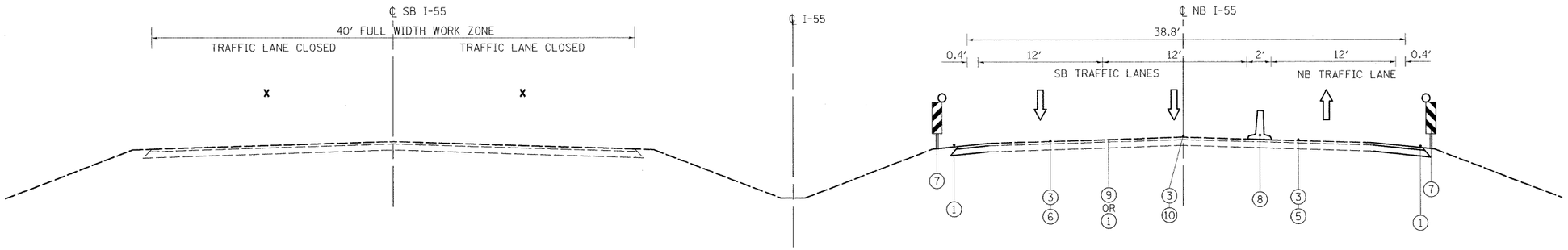
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I–55 OVER THE KANKAKEE RIVER
TRAFFIC CONTROL STAGING NOTES & DETAILS STAGE II

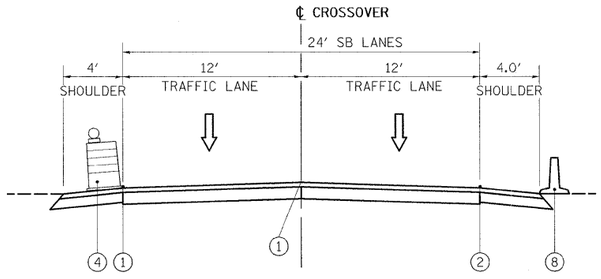
SCALE: NTS | SHEET NO. 2 OF 3 SHEETS | STA. ---- | TO STA. ----

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	11
		CONTRACT NO. 62930		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



TYPICAL SECTION – STAGE III REHABILITATION
(SECTION LOOKING NORTH)
STA. 42+50 TO STA. 76+50

TYPICAL SECTION – STAGE III REHABILITATION
(SECTION LOOKING NORTH)
STA. 42+50 TO STA. 76+50



CROSSOVER TYPICAL SECTION – STAGE III REHABILITATION
(SECTION LOOKING NORTH)
STA. 33+50 TO STA. 43+50
STA. 76+50 TO STA. 90+70

STAGE III (FOR WORK AT SB I–55 BRIDGE)

1. SHIFT SB TRAFFIC ONTO NB PAVEMENT IN ACCORDANCE WITH TRAFFIC PLAN AND DISTRICT ONE STANDARD TC-09 "FREEWAY SINGLE AND MULTILANE WEAVE".
2. PERFORM STRUCTURAL REHABILITATION WORK ON SB I-55 BRIDGE.
3. AFTER COMPLETION OF BRIDGE WORK, REMOVE TRAFFIC CONTROL DEVICES, RESTORE PAVEMENT MARKINGS, LANDSCAPING AND BARRIER REFLECTORS AND SHIFT SB TRAFFIC TO SB PAVEMENT, MAINTAINING TWO (2) LANES IN EACH DIRECTION NB & SB.
4. CLOSE INSIDE NB LANE PER STAGE 1 AND RESTORE PAVEMENT MARKINGS.

LEGEND

(1) WET REFLECTIVE TEMPORARY TAPE TYPE III, 4" WHITE EDGE LINE
(2) WET REFLECTIVE TEMPORARY TAPE TYPE III, 4" YELLOW EDGE LINE
(3) PAVEMENT MARKING REMOVAL
(4) DRUMS WITH STEADY BURNING MONODIRECTIONAL LIGHT
(5) EXISTING PAVEMENT MARKING, WHITE EDGE LINE
(6) EXISTING PAVEMENT MARKING, YELLOW EDGE LINE
(7) VERTICAL PANELS (BACK TO BACK)
(8) TEMPORARY CONCRETE BARRIER
(9) WET REFLECTIVE TEMPORARY TAPE TYPE III, 4" WHITE (10' LINE, 30' SPACE) LINE
(10) EXISTING PAVEMENT MARKING, WHITE LANE LINE (10' LINE, 30' SKIP)

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
NOTES & DETAILS STAGE III

SCALE: NTS | SHEET NO. 3 OF 3 SHEETS | STA. ---- | TO STA. ----

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	12
		CONTRACT NO. 62930		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

N

LEGEND

① WET REFLECTIVE TEMPORARY TAPE TYPE III, 4" WHITE EDGE LINE
② WET REFLECTIVE TEMPORARY TAPE TYPE III, 4" YELLOW EDGE LINE
③ NOT USED
④ DRUMS WITH STEADY BURNING MONODIRECTIONAL LIGHT
⑤ NOT USED
⑥ NOT USED
⑦ VERTICAL PANELS (BACK TO BACK)
⑧ TEMPORARY CONCRETE BARRIER
⑨ WET REFLECTIVE TEMPORARY TAPE TYPE III, 4" WHITE (10' LINE, 30' SPACE) LINE

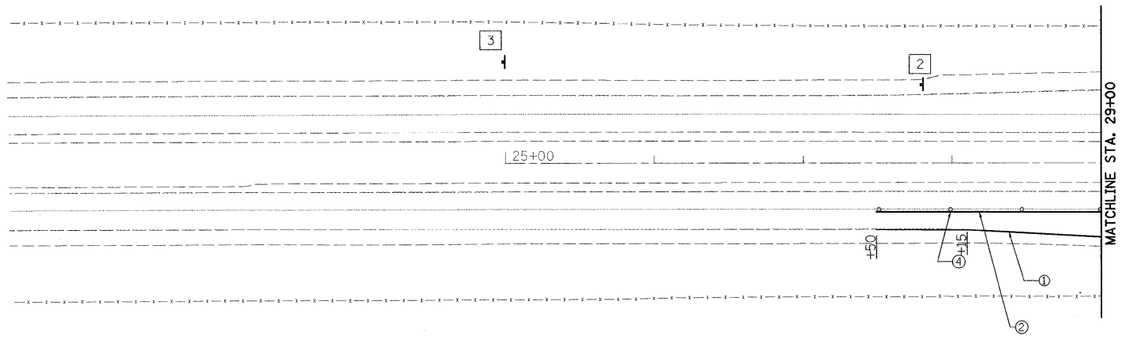
TEMPORARY CONCRETE BARRIER

TEMPORARY CONCRETE BARRIER WITH MODULAR GLARE SCREEN

1 DO NOT PASS R4-1

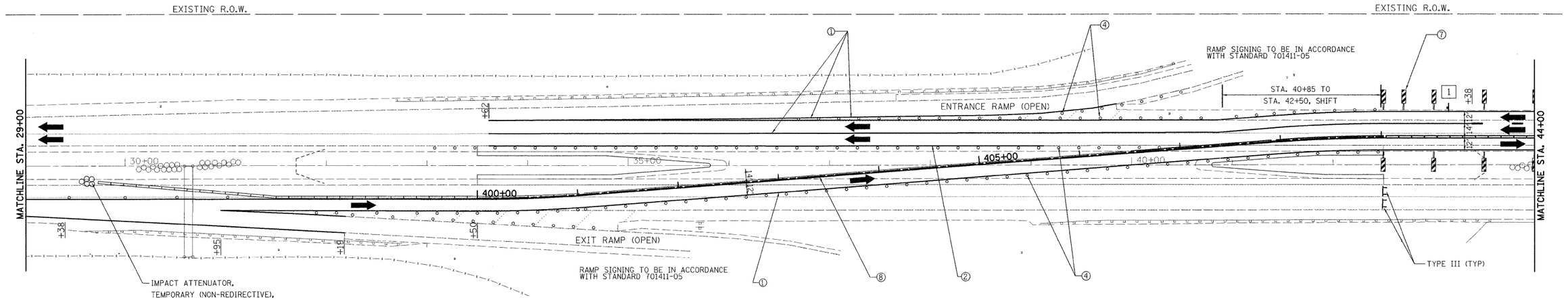
2 PASS WITH CARE R4-2

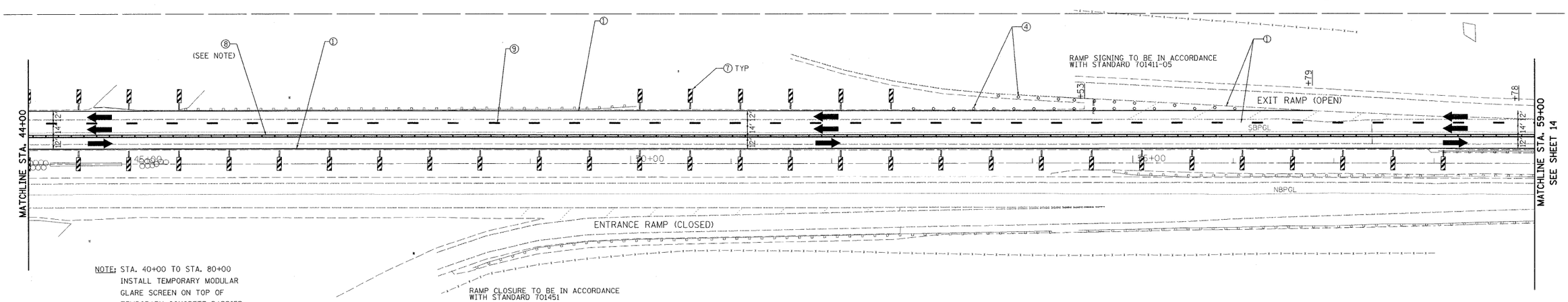
3 END CONSTRUCTION G20-2



ADVANCED LANE CLOSURE SIGNING TO BE IN ACCORDANCE WITH STANDARD 701400-03

LANE CLOSURE TRAFFIC CONTROL (INCLUDING SIGNING) TO BE IN ACCORDANCE WITH STANDARD 701416-06





NOTE: STA. 40+00 TO STA. 80+00 INSTALL TEMPORARY MODULAR GLARE SCREEN ON TOP OF TEMPORARY CONCRETE BARRIER.

ENTRANCE RAMP CLOSED
SEE SHEET 9 FOR SIGNING

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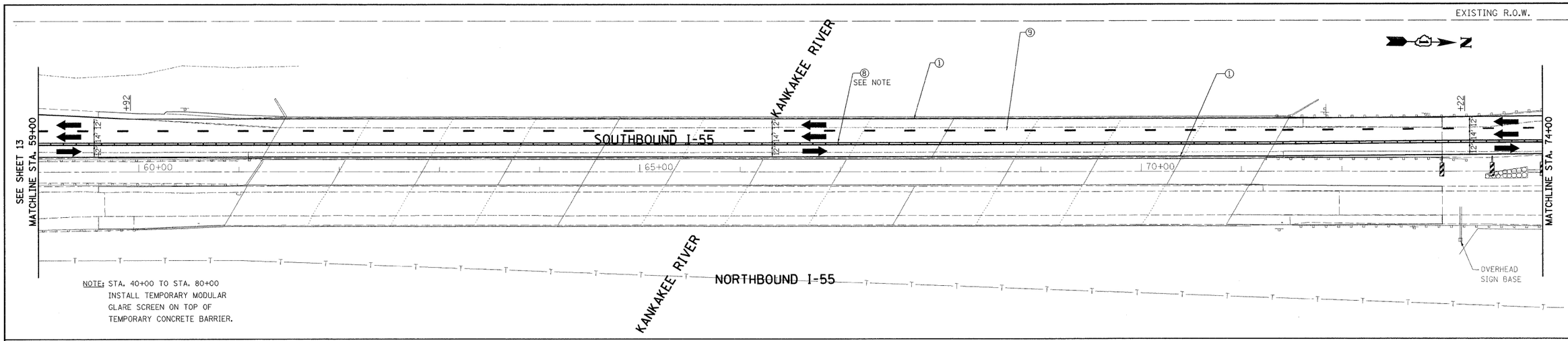
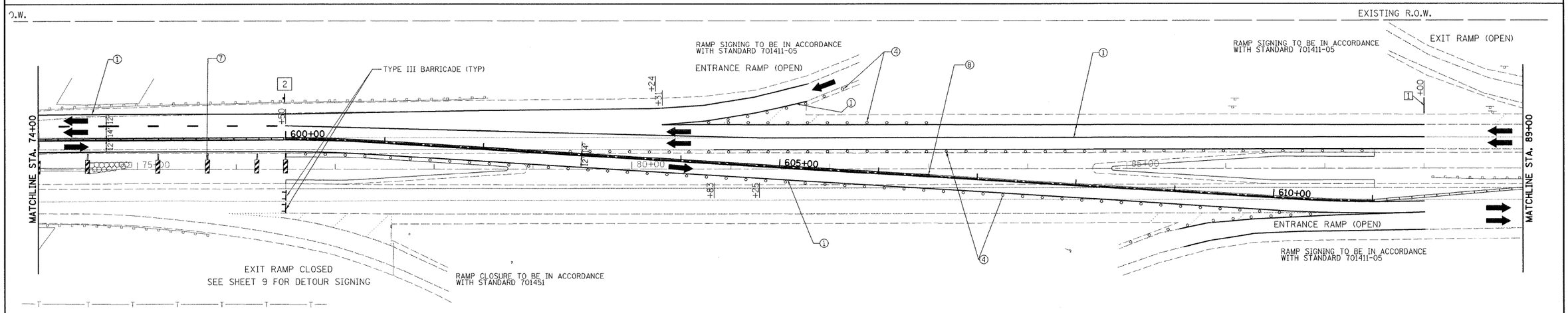
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

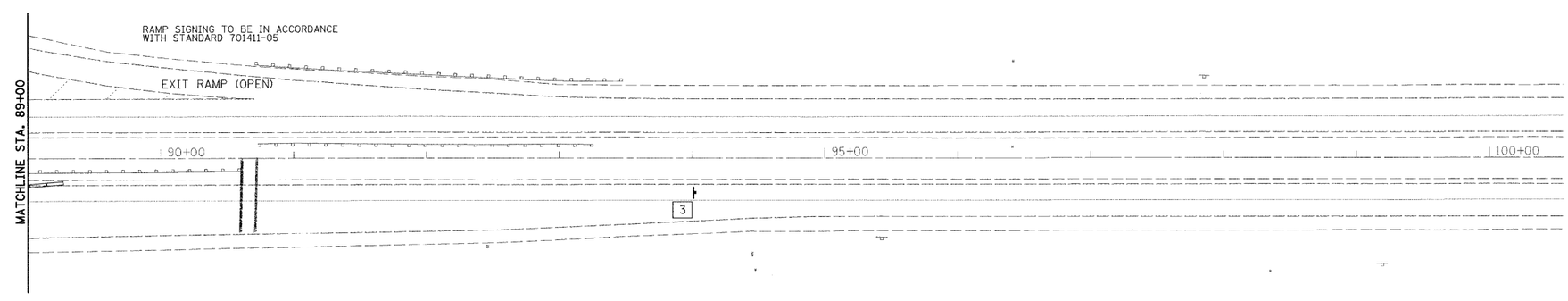
IL-55 OVER THE KANKAKEE RIVER
TRAFFIC CONTROL PLAN STAGE II

SCALE: 1" = 50' | SHEET NO. 1 OF 2 SHEETS | STA. 29+00.00 TO STA. 59+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	13
		CONTRACT NO. 62930		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		







1 DO NOT PASS R4-1

2 PASS WITH CARE R4-2

3 END CONSTRUCTION G20-2

TEMPORARY CONCRETE BARRIER

TEMPORARY CONCRETE BARRIER WITH MODULAR GLARE SCREEN

LEGEND

① WET REFLECTIVE TEMPORARY TAPE TYPE III, 4" WHITE EDGE LINE
② WET REFLECTIVE TEMPORARY TAPE TYPE III, 4" YELLOW EDGE LINE
③ NOT USED
④ DRUMS WITH STEADY BURNING MONODIRECTIONAL LIGHT
⑤ NOT USED
⑥ NOT USED
⑦ VERTICAL PANELS (BACK TO BACK)
⑧ TEMPORARY CONCRETE BARRIER
⑨ WET REFLECTIVE TEMPORARY TAPE TYPE III, 4" WHITE (10' LINE, 30' SPACE) LINE

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-55 OVER THE KANKAKEE RIVER
TRAFFIC CONTROL PLAN STAGE II

SCALE: 1" = 50' | SHEET NO. 2 OF 2 SHEETS | STA. 59+00.00 TO STA. 89+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	14
		CONTRACT NO. 62930		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

LEGEND

① WET REFLECTIVE TEMPORARY TAPE TYPE III, 4" WHITE EDGE LINE
② WET REFLECTIVE TEMPORARY TAPE TYPE III, 4" YELLOW EDGE LINE
③ NOT USED
④ DRUMS WITH STEADY BURNING MONODIRECTIONAL LIGHT
⑤ NOT USED
⑥ NOT USED
⑦ VERTICAL PANELS (BACK TO BACK)
⑧ TEMPORARY CONCRETE BARRIER
⑨ WET REFLECTIVE TEMPORARY TAPE TYPE III, 4" WHITE (10' LINE, 30' SPACE) LINE

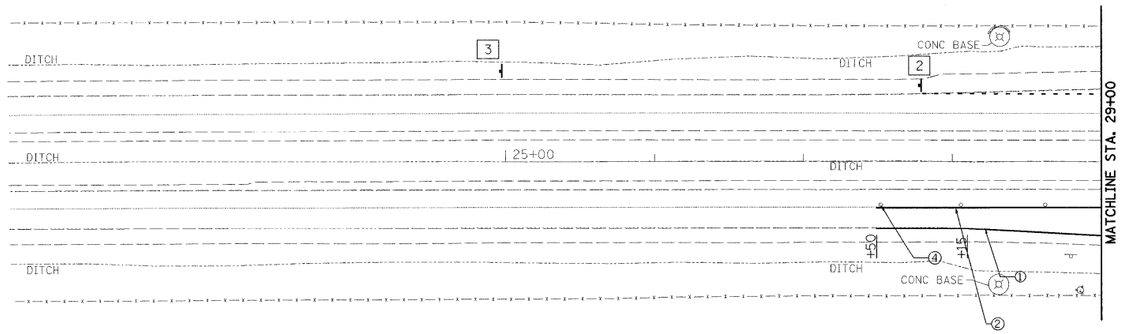
TEMPORARY CONCRETE BARRIER

TEMPORARY CONCRETE BARRIER WITH MODULAR GLARE SCREEN

1 DO NOT PASS R4-1

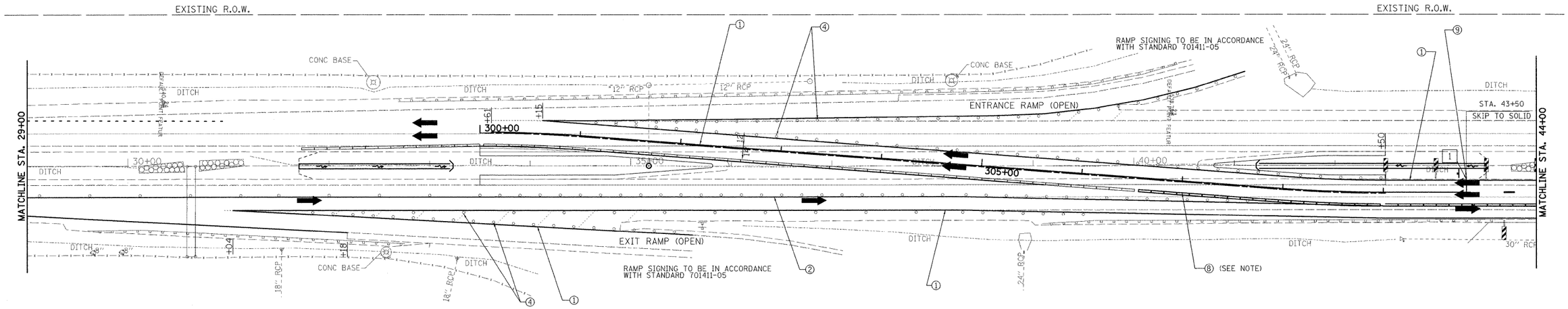
2 PASS WITH CARE R4-2

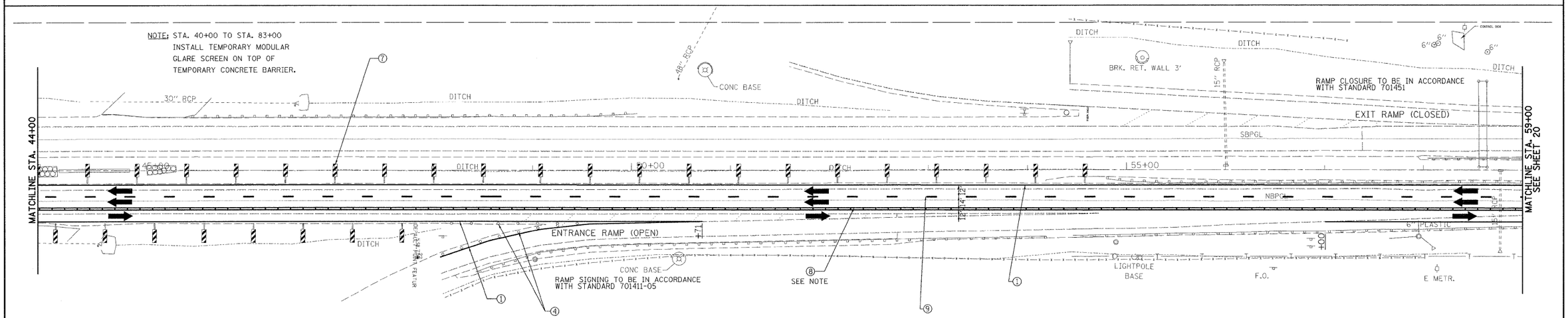
3 END CONSTRUCTION G20-2



ADVANCED LANE CLOSURE SIGNING TO BE IN ACCORDANCE WITH STANDARD 701400-03

LANE CLOSURE TRAFFIC CONTROL (INCLUDING SIGNING) TO BE IN ACCORDANCE WITH STANDARD 701416-06





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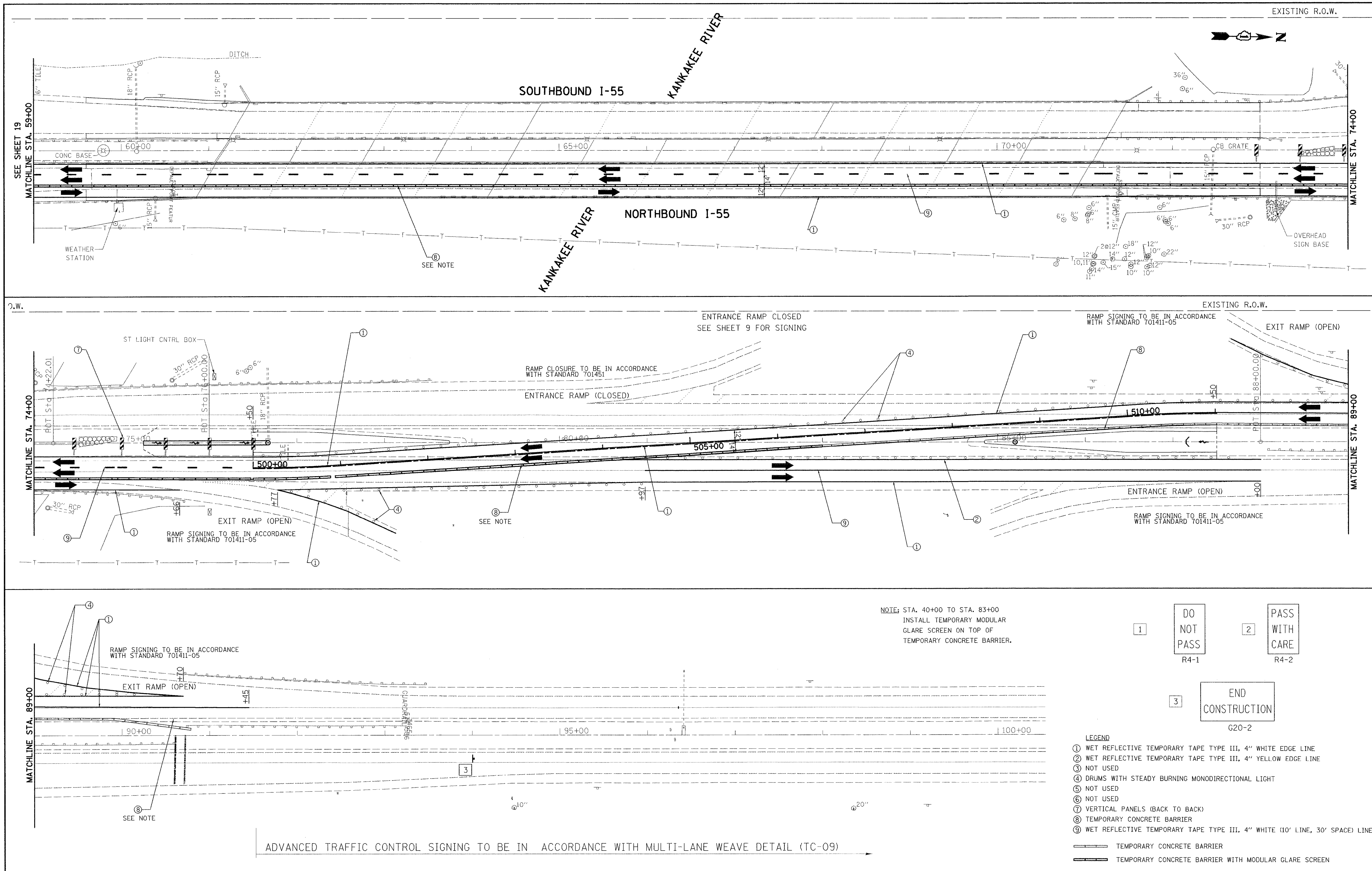
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-55 OVER THE KANKAKEE RIVER
TRAFFIC CONTROL PLAN STAGE III

SCALE: 1" = 50' | SHEET NO. 1 OF 2 SHEETS | STA. 29+00.00 TO STA. 59+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	15
		CONTRACT NO. 62930		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



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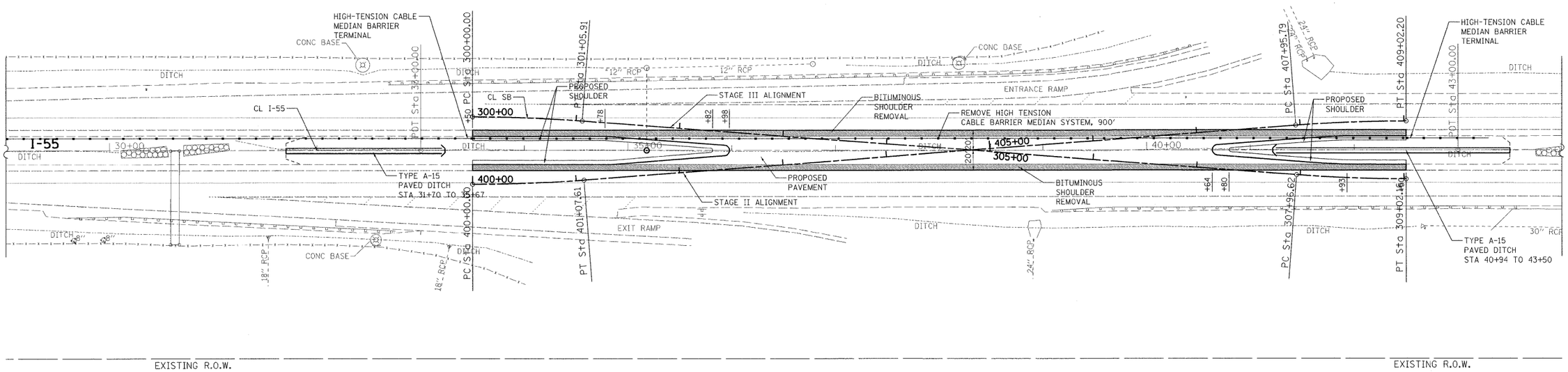
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-55 OVER THE KANKAKEE RIVER
TRAFFIC CONTROL PLAN STAGE III

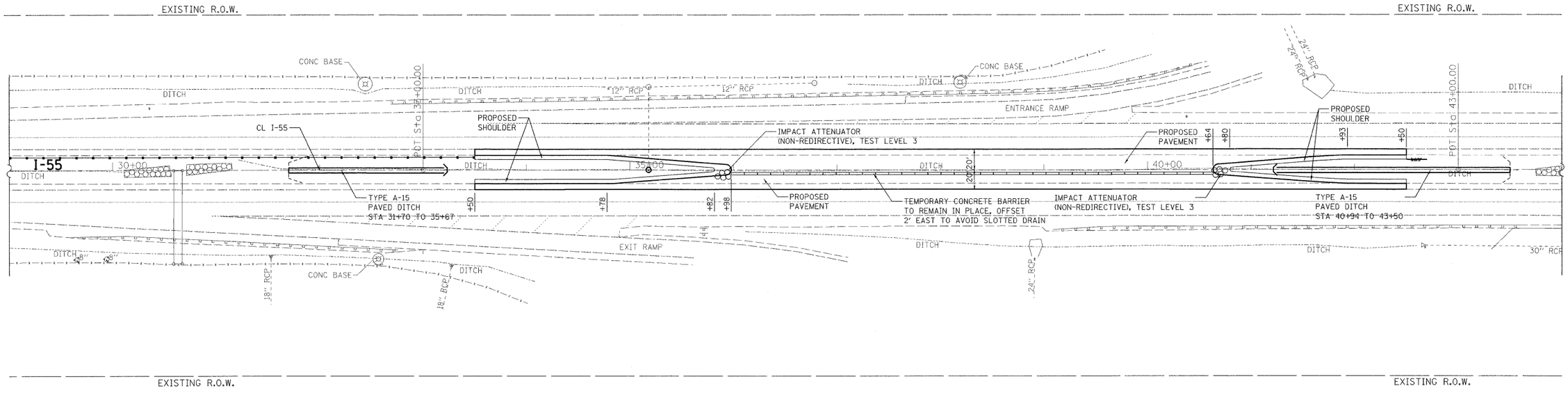
SCALE: 1" = 50' | SHEET NO. 2 OF 2 SHEETS | STA. 59+00.00 TO STA. 89+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	16
		CONTRACT NO. 62930		

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT



DURING CONSTRUCTION



PERMANENT CONDITION

NOTE: STATION CALLOUTS ARE RELATIVE TO EXISTING BASELINE

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PLOT SCALE = 1.0000
PLOT SCALE = 1.0000
USER NAME = bbuzzell
MODEL = Default

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DATE - 03-04-09		REVISED -

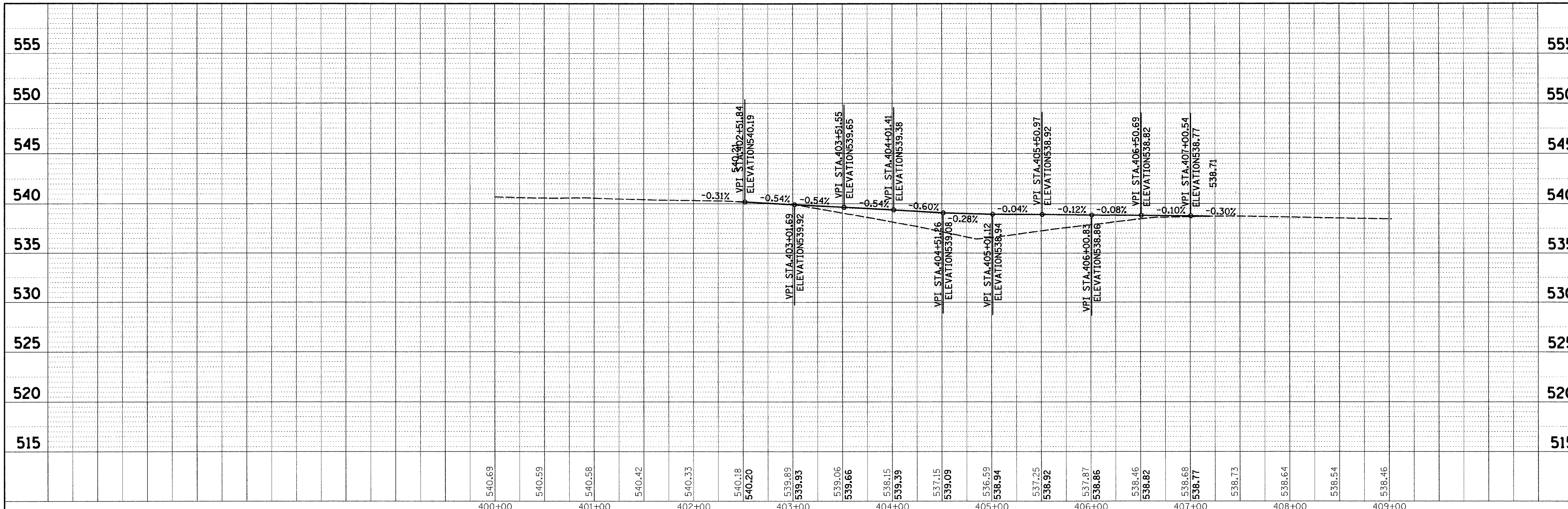
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOUTH CROSSOVER
PLAN

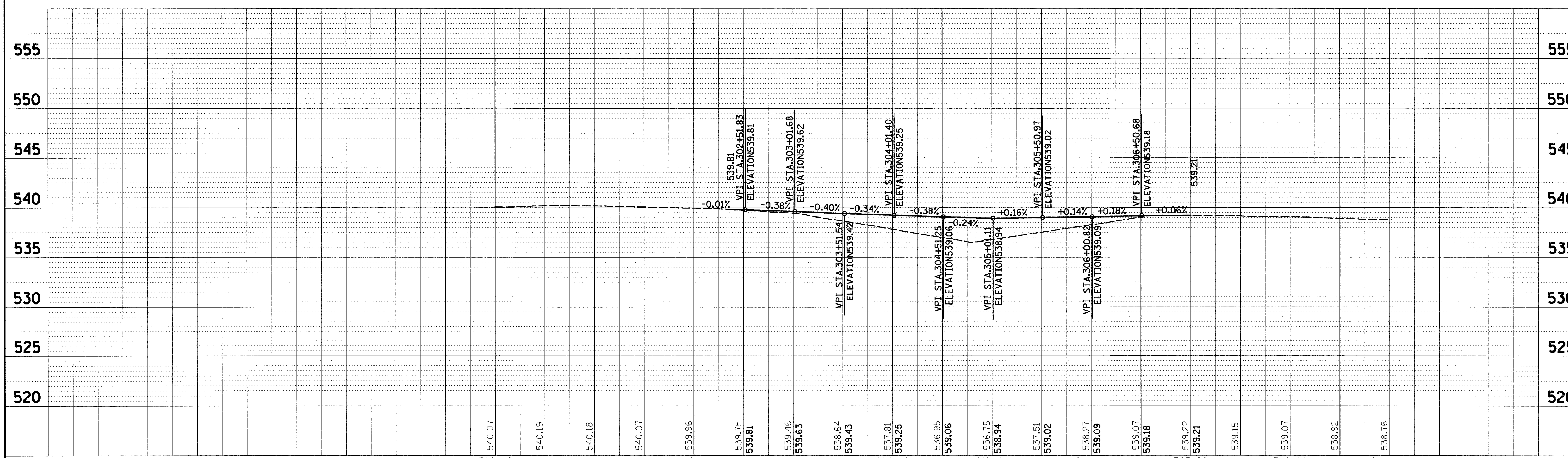
SCALE: 1" = 50' | SHEET NO. 1 OF 4 SHEETS | STA. 29+00.00 TO STA. 44+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	17
		CONTRACT NO. 62930		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

PLAN | NOTE BOOK | NO. | SURVEYED | PLOTTED | ALIGNMENT CHECKED | RT. OF WAY CHECKED | CADD FILE NAME | BY | DATE



PROFILE | NOTE BOOK | NO. | SURVEYED | PLOTTED | GRADES CHECKED | B.M. NOTED | STRUCTURE NOTATNS CHK'D | BY | DATE



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Excellence through Ownership

200 West Front Street
Wheaton, Il 60187

DESIGNED -	REVISED -
DRAWN -	REVISED -
CHECKED -	REVISED -
DATE	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

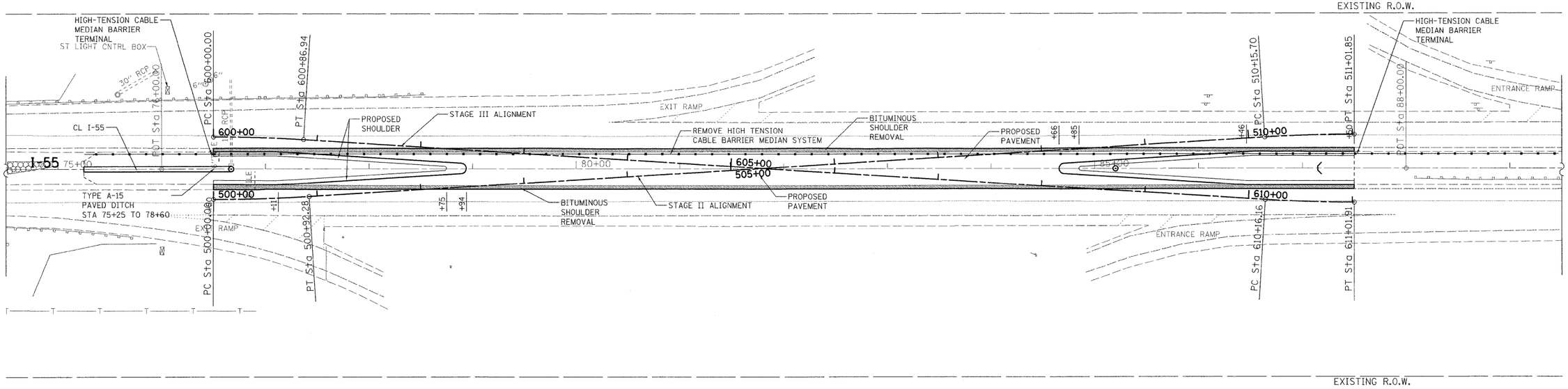
SOUTH CROSSOVER
PROFILE

SCALE: 1" = 50' | SHEET NO. 3 OF 4 SHEETS | STA. 29+00.00 TO STA. 44+00.00

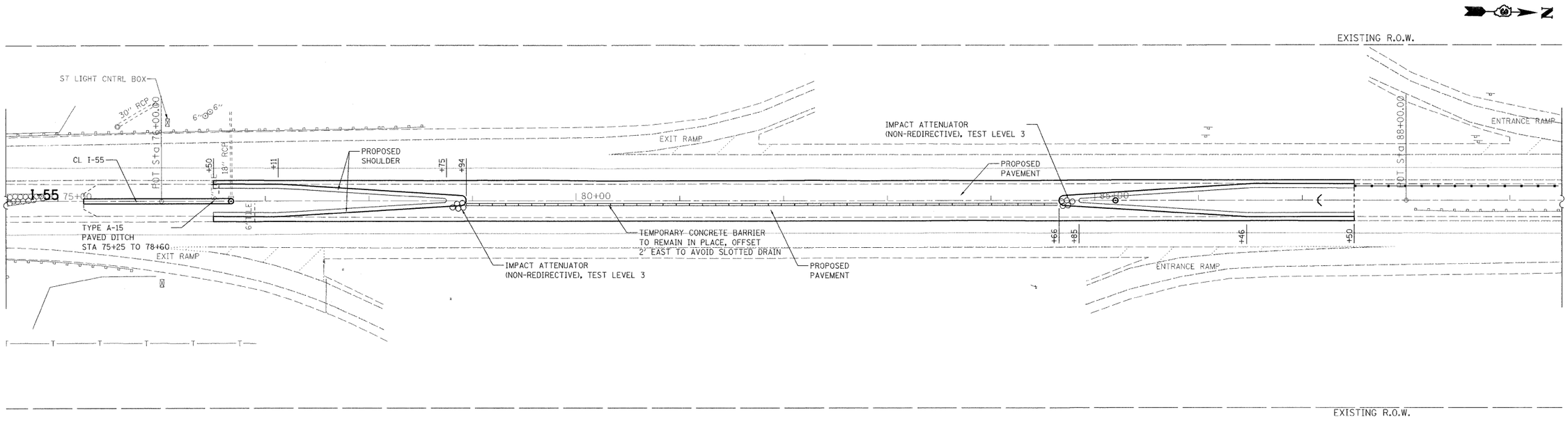
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	BB (B&B-1) BR	WILL	72	18

CONTRACT NO. 62930

FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT



DURING CONSTRUCTION



PERMANENT CONDITION

NOTE: STATION CALLOUTS ARE RELATIVE TO EXISTING BASELINE

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200 West Front Street
Wheaton, Il 60187

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DRAWN - KSD	REVISED -	
CHECKED - CMJ	REVISED -	
DATE - 03-04-09	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

NORTH CROSSOVER PLAN

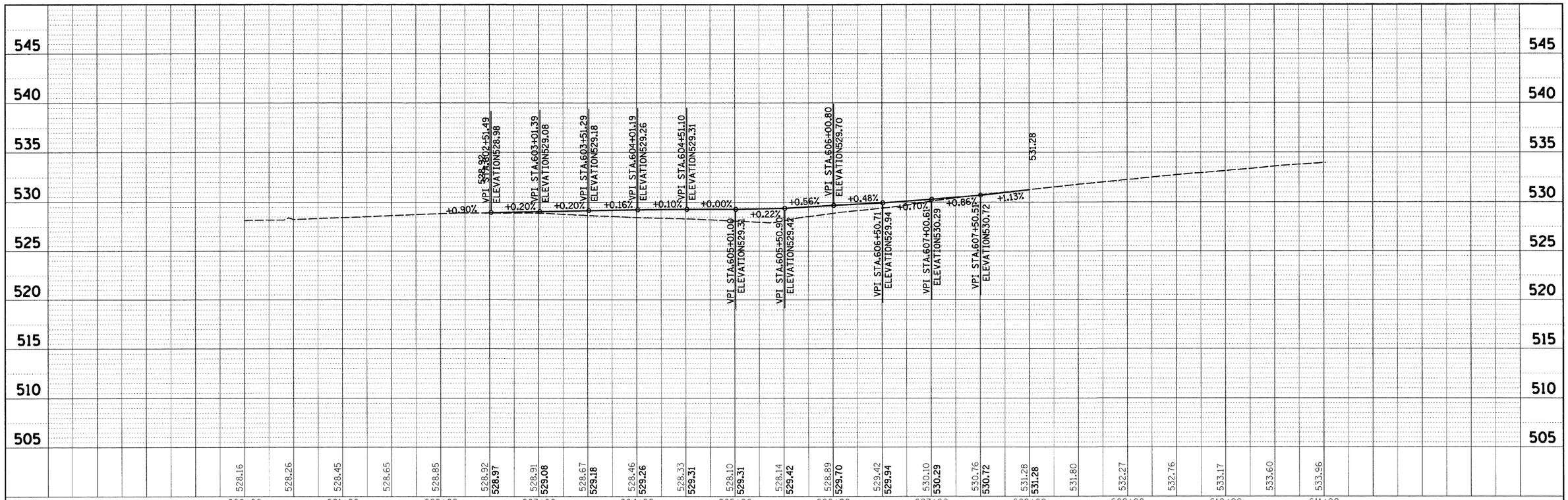
SCALE: 1" = 50' | SHEET NO. 2 OF 4 SHEETS | STA. 29+00.00 TO STA. 44+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	19
		CONTRACT NO. 62930		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

PLAN

SURVEYED	
PLOTTED	
ALIGNMENT CHECKED	
GRADES CHECKED	
S.M. NOTED	
STRUCTURE NOTATNS CHKD	
CADD FILE NAME	
NOTE BOOK	
NO.	

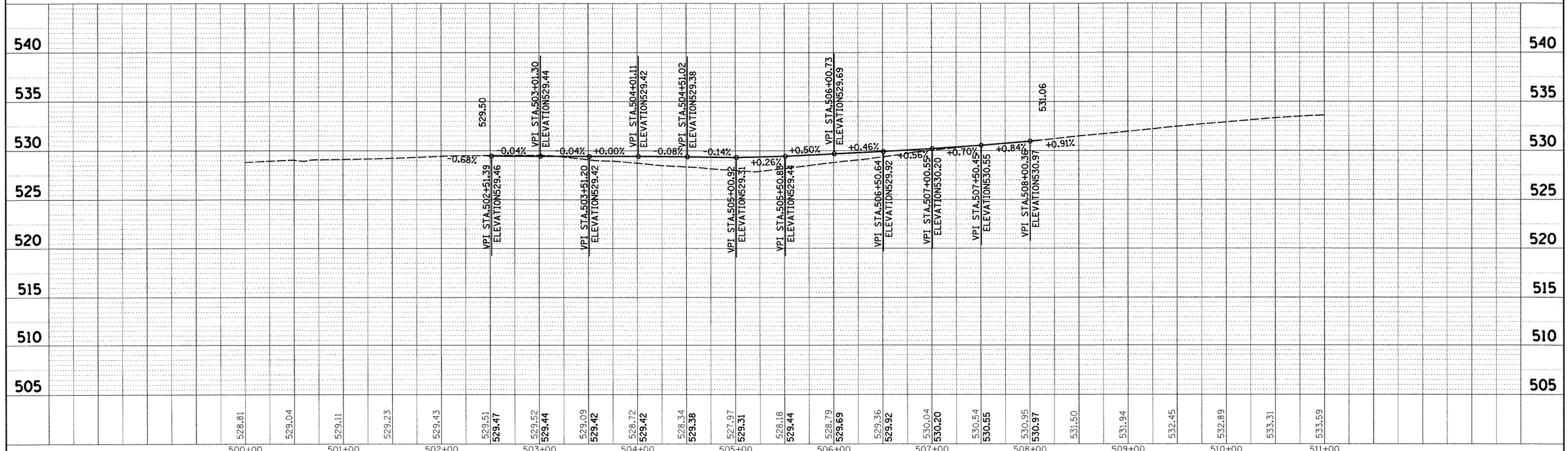
BY | DATE



PROFILE

SURVEYED	
PLOTTED	
GRADES CHECKED	
S.M. NOTED	
STRUCTURE NOTATNS CHKD	
NOTE BOOK	
NO.	

BY | DATE



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200 West Front Street
Wheaton, Il 60187

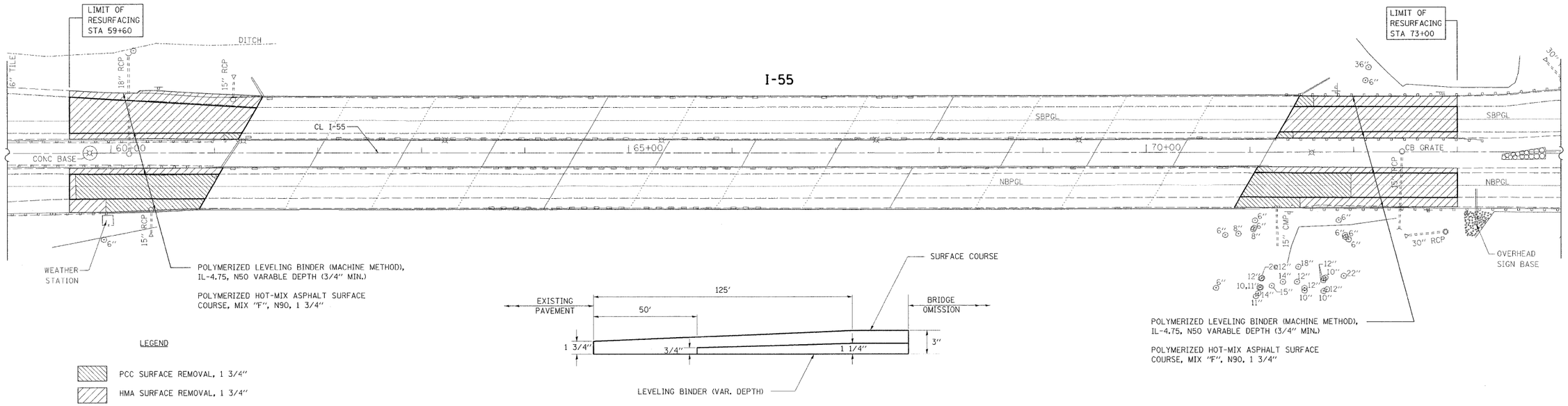
DESIGNED	-	REVISED	-
DRAWN	-	REVISED	-
CHECKED	-	REVISED	-
DATE	-	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

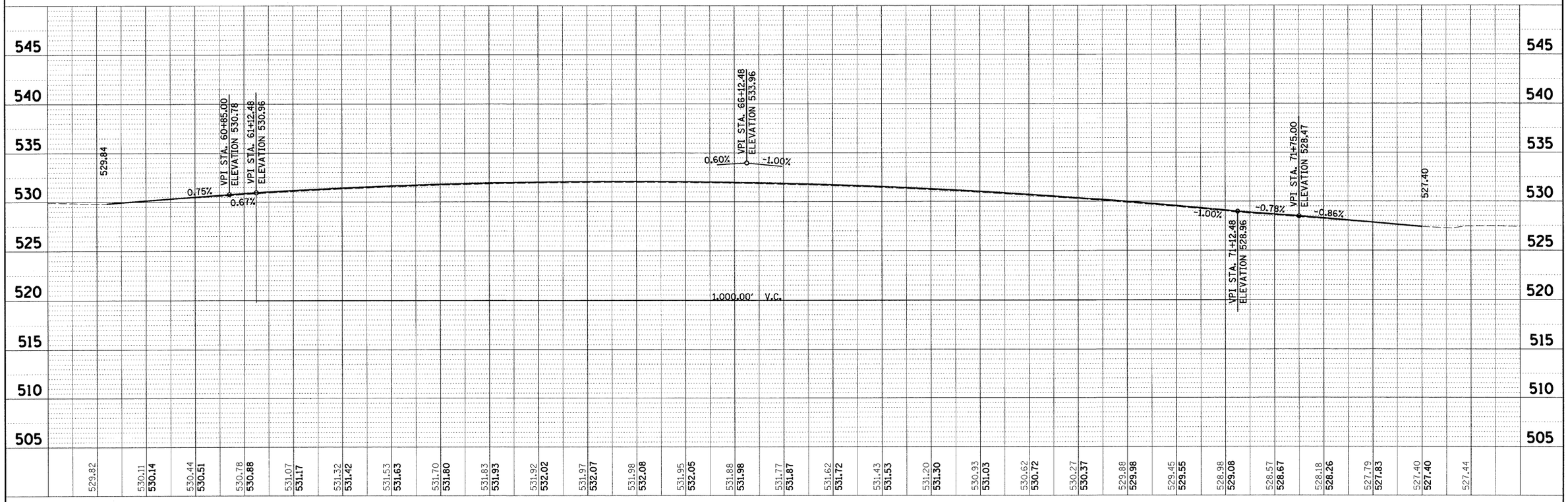
NORTH CROSSOVER
PROFILE

SCALE: 1" = 50' | SHEET NO. 4 OF 4 SHEETS | STA. 29+00.00 TO STA. 44+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	BB (B&B-1) BR	WILL	72	20
		CONTRACT NO. 62930		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



HMA PAVEMENT BUTT JOINT DETAIL



rjngroup Excellence through Ownership 200 West Front Street Wheaton, Il 60187	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
	DRAWN -	REVISED -	
	CHECKED -	REVISED -	
	DATE -	REVISED -	

I-55 OVER THE KANKAKEE RIVER
BRIDGE PLAN & PROFILE

SCALE: 1" = 50' | SHEET NO. X OF 5 SHEETS | STA. 59+00.00 TO STA. 74+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	BB (B&B-1) BR	WILL	72	21
		CONTRACT NO. 62930		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

PLAN: SURVEYED, PLOTTED, ALIGNMENT CHECKED, GRADES CHECKED, CADD FILE NAME, NOTE BOOK, NO., BY, DATE

PROFILE: SURVEYED, PLOTTED, GRADES CHECKED, CADD FILE NAME, NOTE BOOK, NO., BY, DATE, STRUCTURE NO/FATNS CHKD

EXISTING R.O.W.

EXISTING R.O.W.

CONC BASE

DITCH

DITCH

12" RCP

12" RCP

PROP. INLET, TYPE A
W/ TYPE 8 FRAME & GRATE
STA. 35+18
RIM=538.6
INV.=536.7 (W)*
INV.=536.7 (N)
INV.=536.7 (S)

CONC BASE

DITCH

24" RCP

24" RCP

DITCH

PROP. 12" RCP

I-55

30+00

DITCH

35+00

DITCH

PROP. 12" SLOTTED DRAIN, 465'

40+00

PROP. 12" RCP

DITCH

DITCH

18" RCP

BEGIN TYPE A
PAVED DITCH
STA 31+70

18" RCP

CONC BASE

DITCH

PROP. FES
STA. 33+25
INV.=537.19

DITCH

24" RCP

DITCH

PROP. FES
STA. 41+20
INV.=537.3

END TYPE A
PAVED DITCH
STA 43+50

30" RCP

* VERIFY LOCATION AND INVERT OF EXISTING STORM SEWER PRIOR TO ORDERING MATERIALS

EXISTING R.O.W.

EXISTING R.O.W.

PLAN
SURVEYED BY
DRAWN BY
ALIGNMENT CHECKED
PRT. OF WAY CHECKED
NOTE BOOK
NO.
DATE
BY

PROFILE
SURVEYED BY
DRAWN BY
GRADES CHECKED
ELEV. NOTED
STRUCTURE NO.
NOTE BOOK
NO.
DATE
BY

555
550
545
540
535
530
525
520
515

STA. 33+25
INV.=537.19

UPPER DITCH
(SLOPE TOWARD INLET)

STA. 35+18
RIM=538.6
INV.=536.7 (W)*
INV.=536.7 (N)
INV.=536.7 (S)

DESIGN PROFILE GRADE

STA. 41+20
INV.=537.3

EXISTING PROFILE GRADE

PAVED DITCH @ 0.25%

12" RCP STM. SEWER @ 0.25%, 195 LF

12" RCP STM.
@ 0.10%, 80 LF

12" SLOTTED DRAIN @ 0.10%, 465 LF

12" RCP STM.
@ 0.10%, 58 LF

PAVED DITCH @ 0.10%

541.50 541.38 541.25 541.16 541.04 540.93 540.81 540.70 540.59 540.48 540.54 540.42 540.33 540.25 540.15 540.04 539.95 539.82 539.72 539.63 539.53 539.46 539.32 539.13 539.08 538.94 538.78 538.57 538.45

30+00 31+00 32+00 33+00 34+00 35+00 36+00 37+00 38+00 39+00 40+00 41+00 42+00 43+00

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Excellence through Ownership

200 West Front Street
Wheaton, Il 60187

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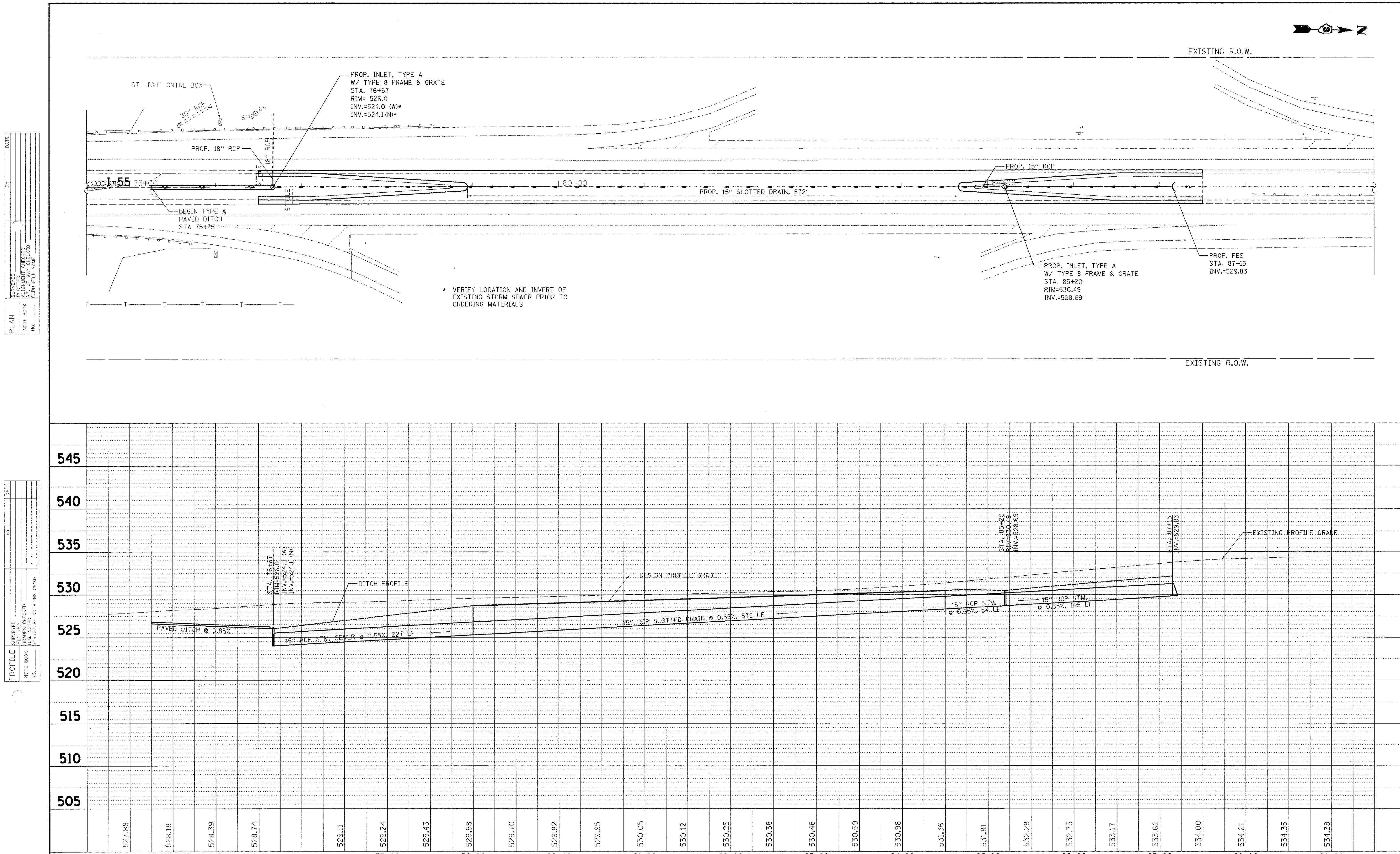
CHECKED -	REVISED -
DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOUTH CROSSOVER
DRAINAGE PLAN & PROFILE

SCALE: 1" = 50' | SHEET NO. 1 OF 2 SHEETS | STA. 29+00.00 TO STA. 44+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	BB (B&B-1) BR	WILL	72	22
		CONTRACT NO. 62930		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



rjngroup Excellence through Ownership 200 West Front Street Wheaton, Il 60187	DESIGNED - DRAWN - CHECKED - DATE -	REVISED - REVISED - REVISED - REVISED -	**STATE OF ILLINOIS** **DEPARTMENT OF TRANSPORTATION**

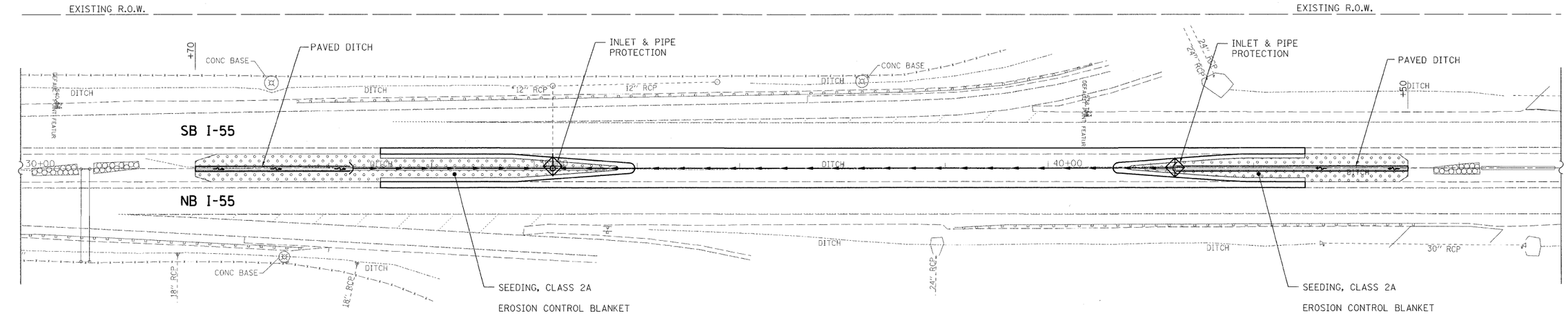
NORTH CROSSOVER
DRAINAGE PLAN & PROFILE

SCALE: 1" = 50' | SHEET NO. 2 OF 4 SHEETS | STA. 29+00.00 TO STA. 44+00.00

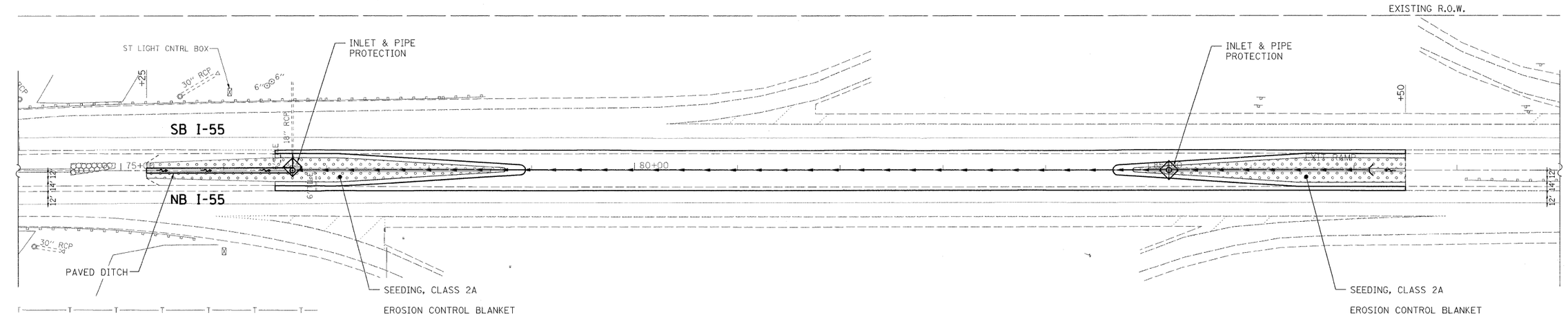
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	BB (B&B-1) BR	WILL	72	23

CONTRACT NO. 62930

FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT



SOUTH CROSSOVER



NORTH CROSSOVER

rjngroup
Excellence through Ownership
200 West Front Street
Wheaton, Il 60187

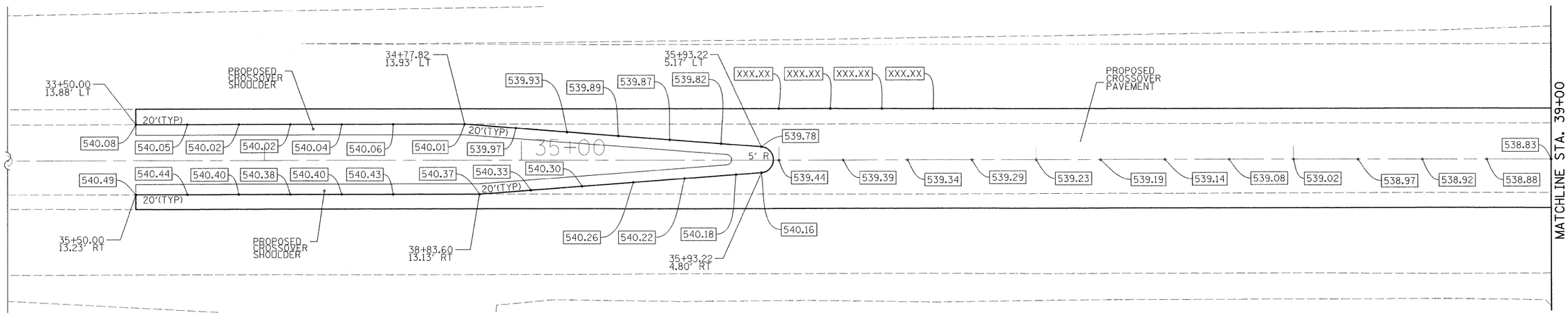
DESIGNED	KSD	REVISED	-
DRAWN	KSD	REVISED	-
CHECKED	CMJ	REVISED	-
DATE	03-04-09	REVISED	-

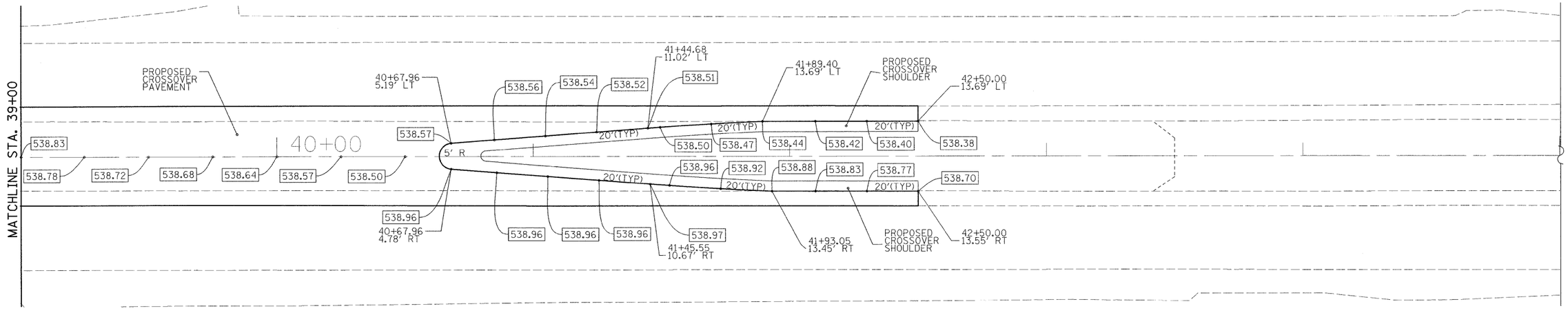
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL-55 OVER THE KANKAKEE RIVER
EROSION CONTROL PLAN

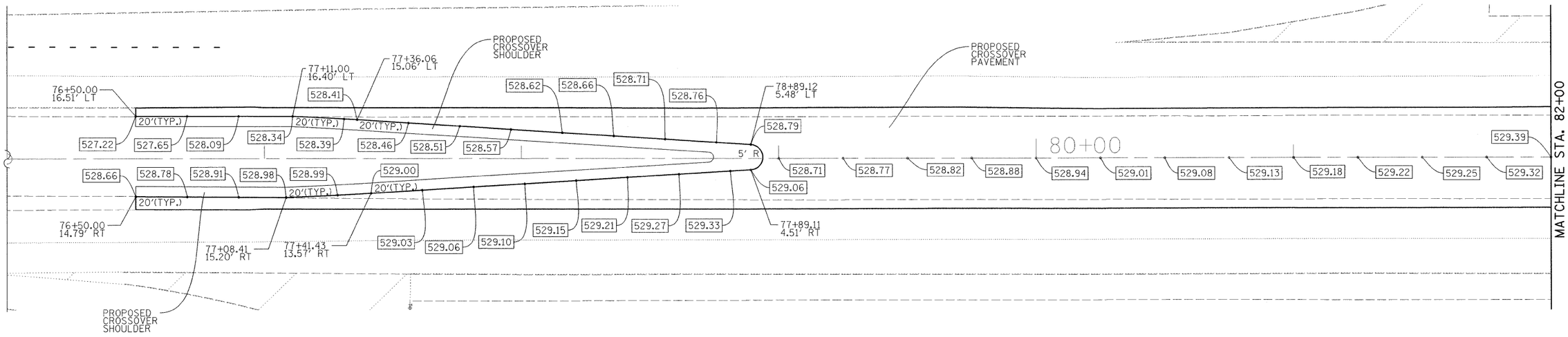
SCALE: 1" = 50' | SHEET NO. 1 OF 1 SHEETS | STA. 30+00.00 TO STA. 84+00.00

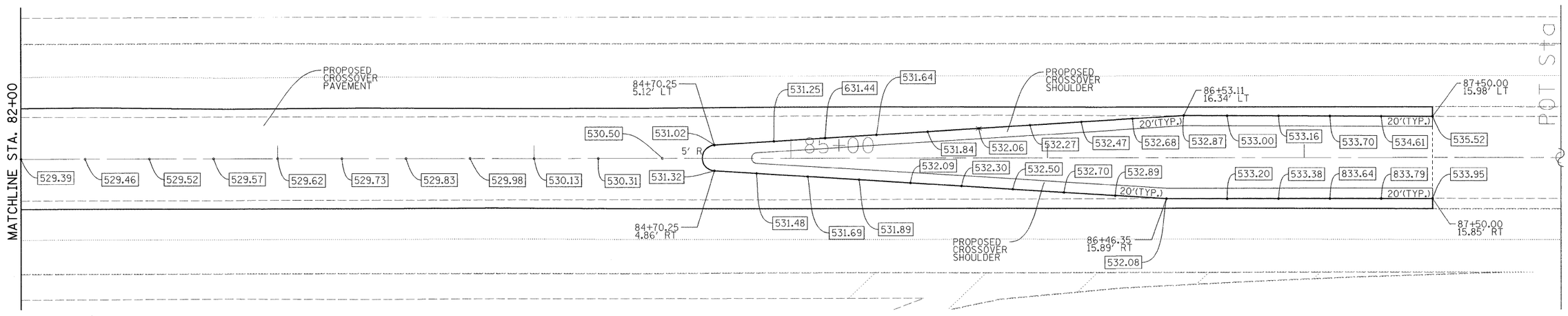
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	24
		CONTRACT NO. 62930		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		





rjngroup Excellence through Ownership	200 West Front Street Wheaton, Il 60187	DESIGNED - KSD DRAWN - KSD CHECKED - CMJ DATE - 03-04-09	REVISED - REVISED - REVISED - REVISED -	**STATE OF ILLINOIS** **DEPARTMENT OF TRANSPORTATION**	**I-55 OVER THE KANKAKEE RIVER** **PAVEMENT ELEVATION DETAILS – SOUTH CROSSOVER** SCALE: 1" = 20' | SHEET NO. 1 OF 2 SHEETS | STA. 33+00.00 TO STA. 45+00.00	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
						55	88 (B&B-1) BR	WILL	72	25
								CONTRACT NO. 62930		
						FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		





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Excellence through Ownership

200 West Front Street
Wheaton, Il 60187

DESIGNED - KSD	REVISED -	
DRAWN - KSD	REVISED -	
CHECKED - CMJ	REVISED -	
DATE - 03-04-09	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-55 OVER THE KANKAKEE RIVER
PAVEMENT ELEVATION DETAILS - NORTH CROSSOVER

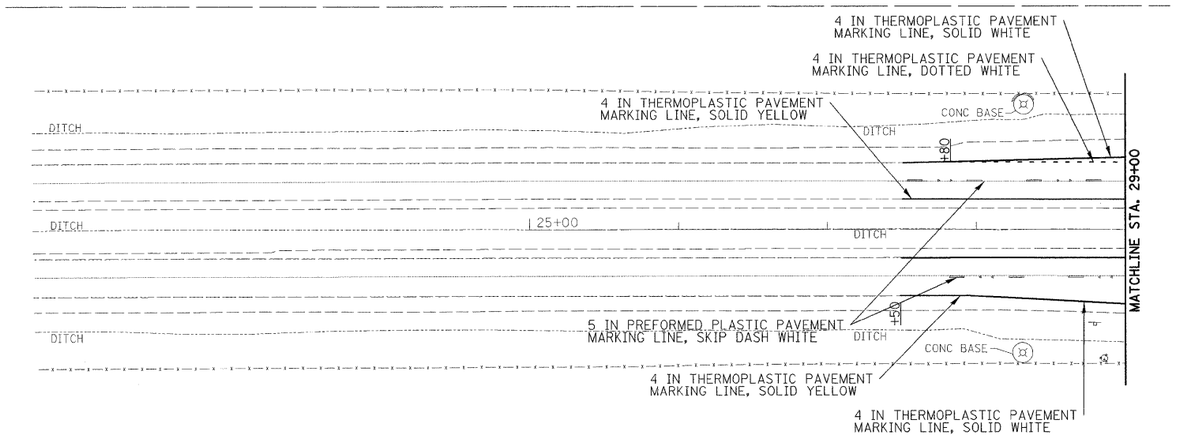
SCALE: 1" = 20' | SHEET NO. 2 OF 2 SHEETS | STA. 76+00.00 TO STA. 88+00.00

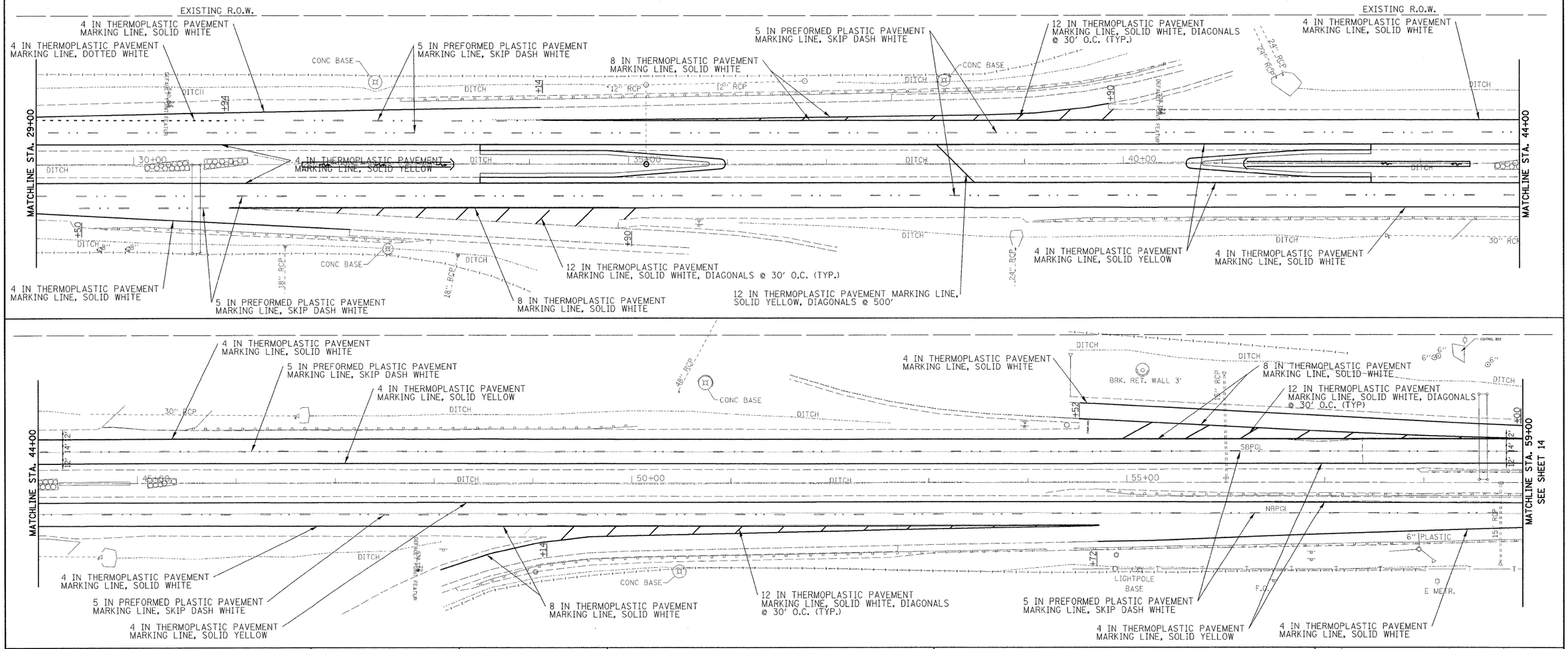
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	26
		CONTRACT NO. 62930		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

N

LEGEND

◁ ONE-WAY CRYSTAL RAISED REFLECTIVE PAVEMENT MARKER





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Excellence through Ownership
200 West Front Street
Wheaton, Il 60187

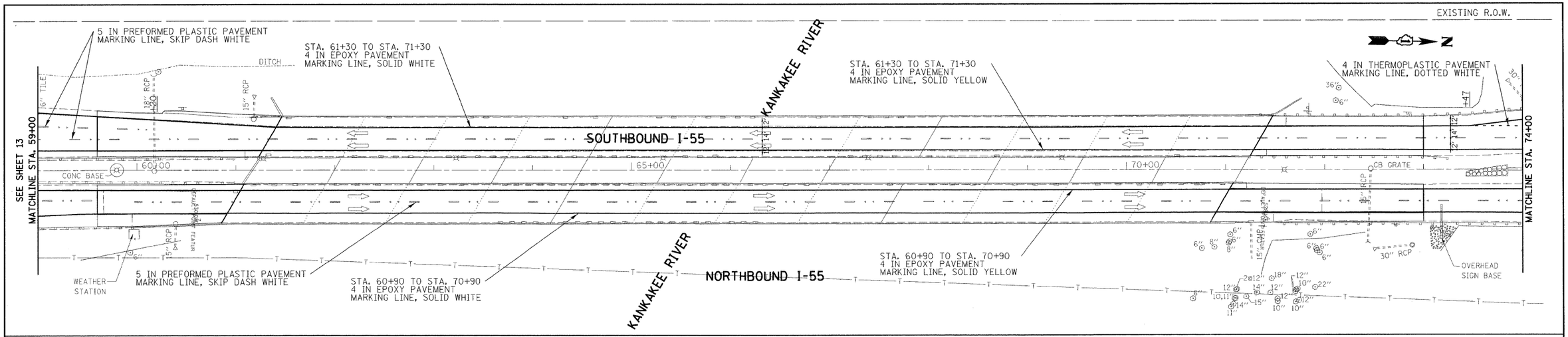
DESIGNED - KSD	REVISED -	
DRAWN - KSD	REVISED -	
CHECKED - CMJ	REVISED -	
DATE - 03-04-09	REVISED -	

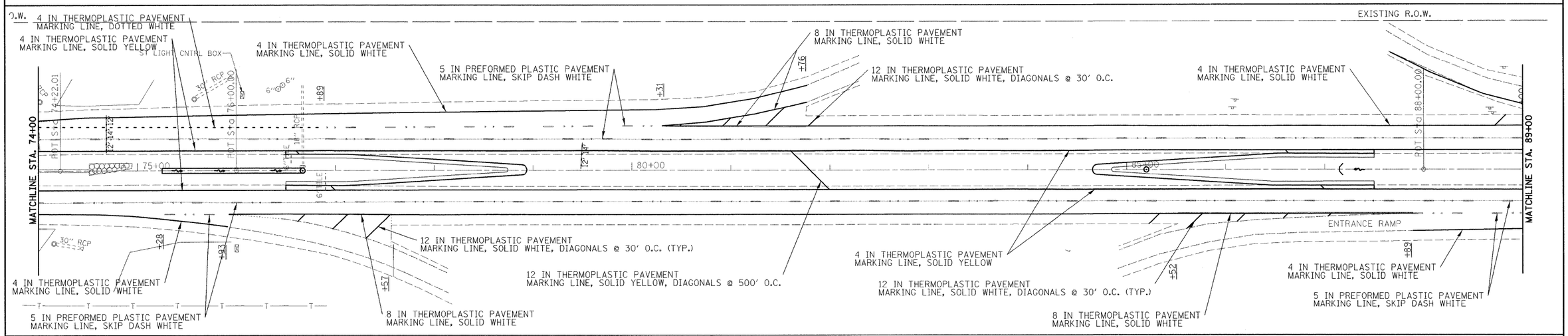
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

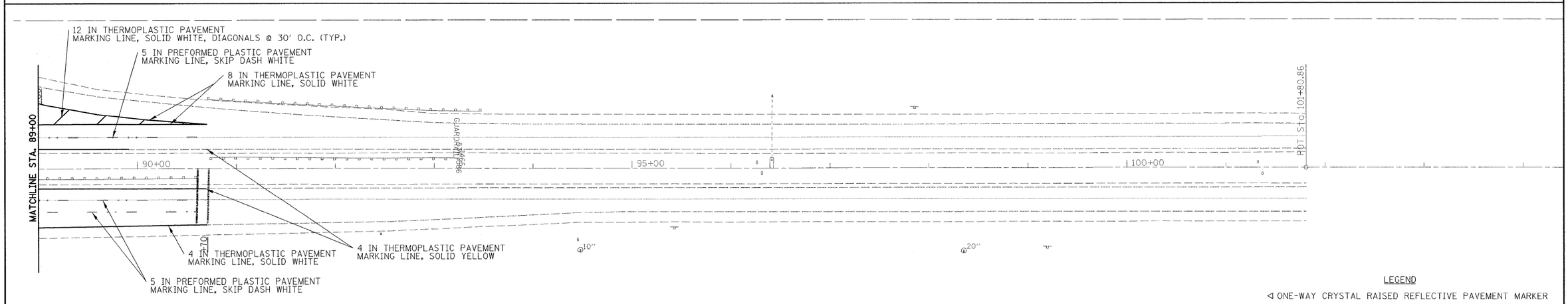
IL-55 OVER THE KANKAKEE RIVER
PAVEMENT MARKING PLAN

SCALE: 1" = 50' | SHEET NO. 1 OF 2 SHEETS | STA. 29+00.00 TO STA. 59+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	27
		CONTRACT NO. 62930		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		







LEGEND

◁ ONE-WAY CRYSTAL RAISED REFLECTIVE PAVEMENT MARKER

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200 West Front Street
Wheaton, Il 60187

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DRAWN	KSD	REVISED	-
CHECKED	CMJ	REVISED	-
DATE	03-04-09	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-55 OVER THE KANKAKEE RIVER
PAVEMENT MARKING PLAN

SCALE: 1" = 50' | SHEET NO. 2 OF 2 SHEETS | STA. 59+00.00 TO STA. 89+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	28
		CONTRACT NO. 62930		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

Benchmark: Chiseled "X" on top of west parapet, south end of SN 099-0002, Elev. 533.22

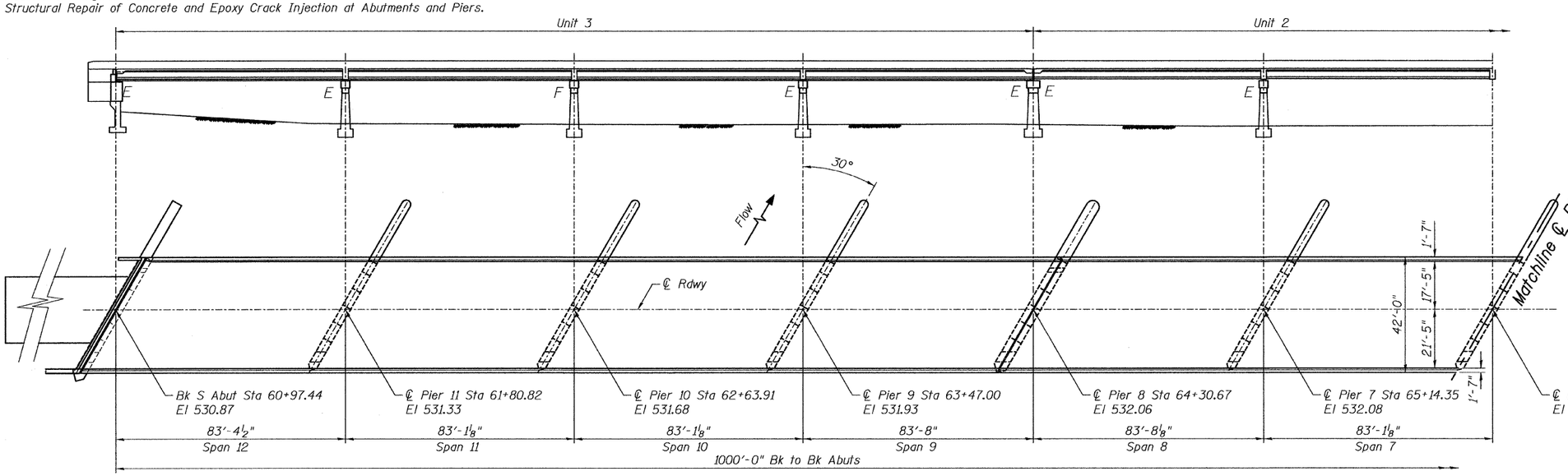
Existing Structure: 12 Span PPC I-Beam Superstructure Consisting of Three (4 Span) Continuos Units on Solid Concrete Piers and Closed Concrete Abutments. The Abutments and Piers Were Built in 1935 and New Caps and Superstructure Were Added in 1977.

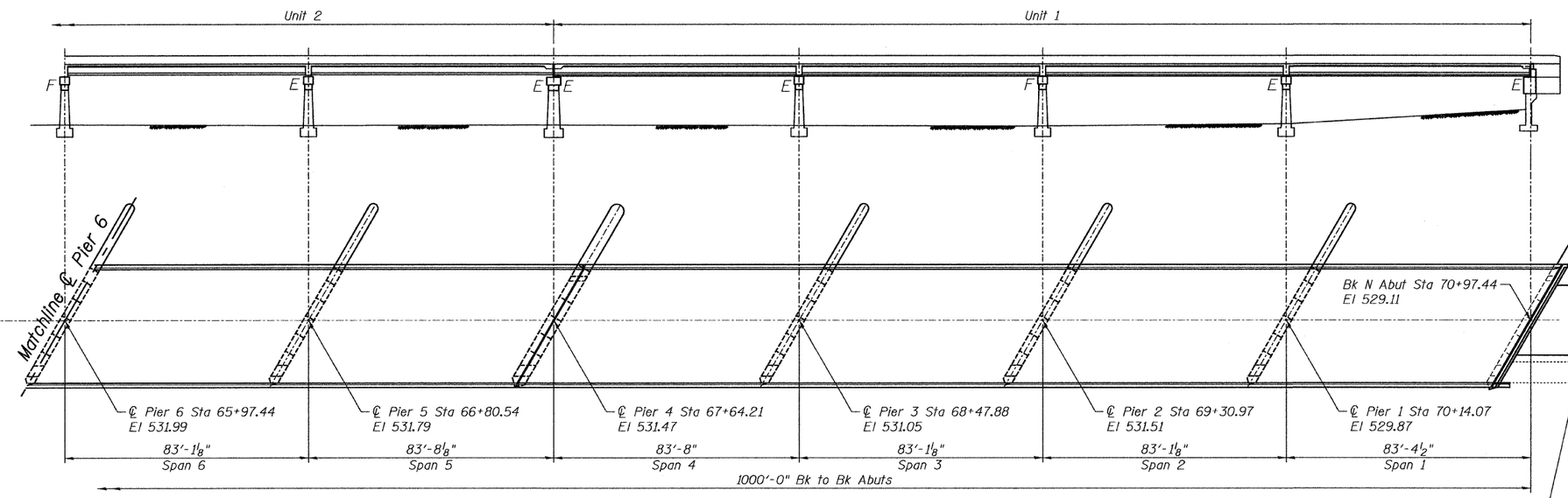
Proposed Improvement: Deck Repair. Scarify Deck 1" and Install Bridge Deck Latex Concrete Overlay $2\frac{1}{4}$".
Neoprene Joint Replacement, P.P.C. Beam Repair.
Addition of Wingwall at the North Abutment.
Structural Repair of Concrete and Epoxy Crack Injection at Abutments and Piers.

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	Will	72	29
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-		

SHEET NO. 1
19 SHEETS

Contract #62930 * 88(B&B-1)BR





PLAN & ELEVATION

Existing Approach Beams to be removed. See sheet 14 of 19.

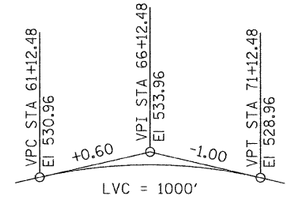
DESIGN SPECIFICATIONS

2002 AASHTO

DESIGN STRESSES

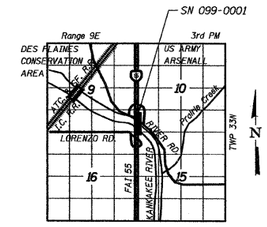
FIELD UNITS

f'_c = 3,500 psi
f_y = 60,000 psi (reinforcement)



PROFILE GRADE

N.B. Structure



LOCATION SKETCH

LICENSED STRUCTURAL ENGINEER
MARY COOMBE BLOXDORF
4859
SPRINGFIELD
STATE OF ILLINOIS

ILLINOIS STRUCTURAL NO. 4859
EXPIRES: 11/30/10
DATE: 11/23/09

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE: PLAN AND ELEVATION

PROJECT: I-55 NB OVER KANKAKEE RIVER
FAI ROUTE 55, SECTION 88(B&B-1)BR
WILL COUNTY
SN 099-0001

PROJECT NO. 03095-16
SCALE
DATE 9/23/09
DRAWN BY CFC
CHECKED BY MCB/KPS
DRAWING NO. 1

COOMBE-BLOXDORF P.C.
Engineers / Land Surveyors
Springfield, Illinois
Design Firm License No. 184-002703

OF 19 SHTS

PLOT DATE = 11/23/2009
FILE NAME = ...\sn-099-0001-sht-01-plan-el.dgn
PLOT SCALE = 214.0000 '/ IN.
USER NAME = CFC

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	Will	72	30
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-	

SHEET NO. 2
19 SHEETS

Contract #62930 * 88(B&B-1)BR

INDEX OF SHEETS

GENERAL NOTES

PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF THE WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.

THE CONTRACTOR SHALL ENSURE THAT NO CONCRETE WILL BE ALLOWED TO DROP INTO THE RIVER. CONCRETE SHALL BE CAUGHT ON A FLOATING PLATFORM OR OTHER MEANS APPROVED BY THE ENGINEER. COST INCLUDED WITH DECK SLAB REPAIR (FULL DEPTH, TYPE II).

PRIOR TO BEGINNING ANY BEAM REPAIR WORK, THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING A PRELOADING SYSTEM ON THE BRIDGE DECK OVER THE EXISTING DAMAGED BEAM AT THE SPECIFIED LOCATIONS. THE PRELOADING SYSTEM SHOULD PRODUCE A TOTAL MAXIMUM SERVICE LOAD MOMENT AS SHOWN AT THE CENTERLINE OF THE DAMAGED AREA.

PRELOADING SHALL BE KEPT IN PLACE FOR AT LEAST THREE (3) DAYS AFTER COMPLETION OF CONCRETE REPAIR OR UNTIL THE CONCRETE HAS REACHED AN ULTIMATE STRENGTH OF 5,000 psi. THE CONTRACTOR'S PROPOSED PRELOADING SYSTEM, WITH COMPUTATIONS, SEALED AND SIGNED BY AN ILLINOIS STRUCTURAL ENGINEER SHALL BE SUBMITTED TO THE BUREAU OF BRIDGES AND STRUCTURES FOR APPROVAL. THE PRELOADING SYSTEM SHALL BE PLACED SHORTLY AFTER BRIDGE CLOSURE FOR REPAIRS.

THE PRELOADING SYSTEM SHALL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE UNIT PRICE OF PPC-I BEAM REPAIRS.

REINFORCEMENT BARS SHALL CONFORM TO THE REQUIREMENTS OF ASTM A 760 Gr 60. SEE SPECIAL PROVISIONS.

REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED.

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Bridge Deck Latex Concrete Overlay 2¼"	Sq Yd	4162		4162
Bridge Deck Hydro Scarification 1"	Sq Yd	4162		4162
Deck Slab Repair (Full Depth, Type I)	Sq Yd	5		5
Deck Slab Repair (Full Depth, Type II)	Sq Yd	238		238
Preformed Joint Strip Seal	Foot	92		92
Neoprene Expansion Joint 4"	Foot	94		94
Precast Prestressed Concrete I-Beam Repair	Sq Ft	11.5		11.5
Epoxy Crack Injection	Foot		627	627
Structural Repair of Concrete (Depth equal to or less than 5")	Sq Ft		531	531
Structural Repair of Concrete (Depth greater than 5")	Sq Ft		17	17
Removal of Existing Precast Concrete Units	Sq Ft		180	180
Concrete Structures	Cu Yd		17.3	17.3
Reinforcement Bars, Epoxy Coated	Pound	11,780	3870	15,650
Structure Excavation	Cu Yd		10	10
Permanent Steel Sheet Piling	Sq Ft		468	468
Stud Shear Connectors	Each		19	19
Protective Coat	Sq Yd	4341	12	4353
Concrete Removal	Cu Yd	78.7	6.3	85.0
Concrete Superstructure	Cu Yd	90.8		90.8
Bridge Deck Grooving	Sq Yd	4087		4087
Replace Surface Sensor and Temperature Probe	L. Sum	1		1

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE
GENERAL NOTES AND TOTAL BILL OF MATERIAL

PROJECT
I-55 NB OVER KANKAKEE RIVER
FAI ROUTE 55, SECTION 88(B&B-1)BR
WILL COUNTY
SN 099-0001

PROJECT NO. 03095-16
SCALE
DATE 6/25/09
DRAWN BY TFG/CFC
CHECKED BY MCB
DRAWING NO. 2
OF 19 SHTS

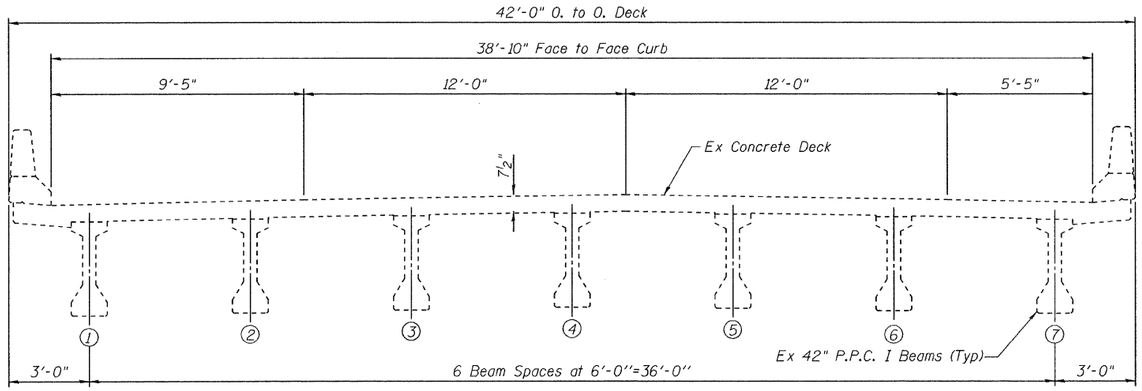
COOMBE-BLOXDORF P.C.
Engineers / Land Surveyors
Springfield, Illinois
Design Firm License No. 184-002703

PLOT DATE = 6/25/2009
FILE NAME = ...88sn-099-0001-sht-2-general-notes.dgn
PLOT SCALE = 1011.7647 '/' / IN.
USER NAME = JML

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	WILL	72	31
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-	

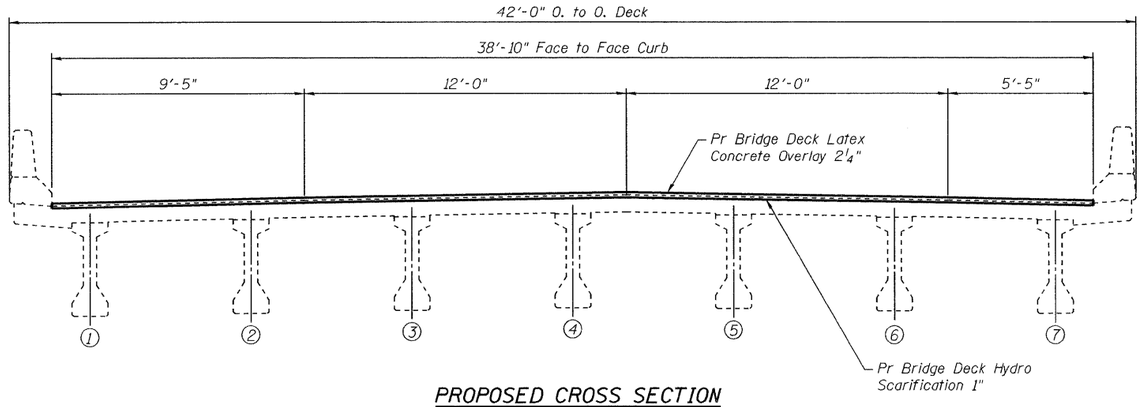
SHEET NO. 3
19 SHEETS

Contract #62930 * 88(B&B-1)BR



EXISTING CROSS SECTION

(Looking South)



PROPOSED CROSS SECTION

(Looking South)

NOTE:

Bridge Deck Hydro Scarification 1" and Bridge Deck Latex Concrete Overlay 2¼" extends between Concrete Removal of Deck in Each Unit.

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Bridge Deck Latex Concrete Overlay 2¼"	Sq Yd	4162
Bridge Deck Hydro Scarification 1"	Sq Yd	4162

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE: CROSS SECTION

PROJECT: I-55 NB OVER KANKAKEE RIVER
FAI ROUTE 55, SECTION 88(B&B-1)BR
WILL COUNTY
SN 099-0001

PROJECT NO. 03095-16
SCALE
DATE 06/25/09
DRAWN BY TFG/CFC
CHECKED BY MCB
DRAWING NO. 3
OF 19 SHTS

COOMBE-BLOXDORF P.C.
Engineers / Land Surveyors
Springfield, Illinois
Design Firm License No. 184-002703

PLOT DATE = 6/25/2009
FILE NAME = ...&sn-099-0001-snt-3-bridge-xs.dgn
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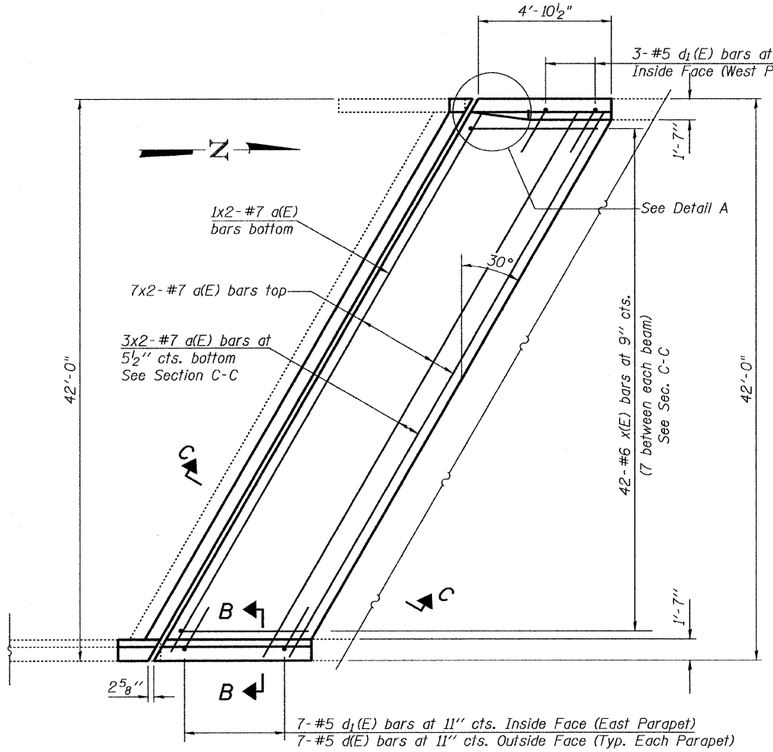
USER NAME = JML

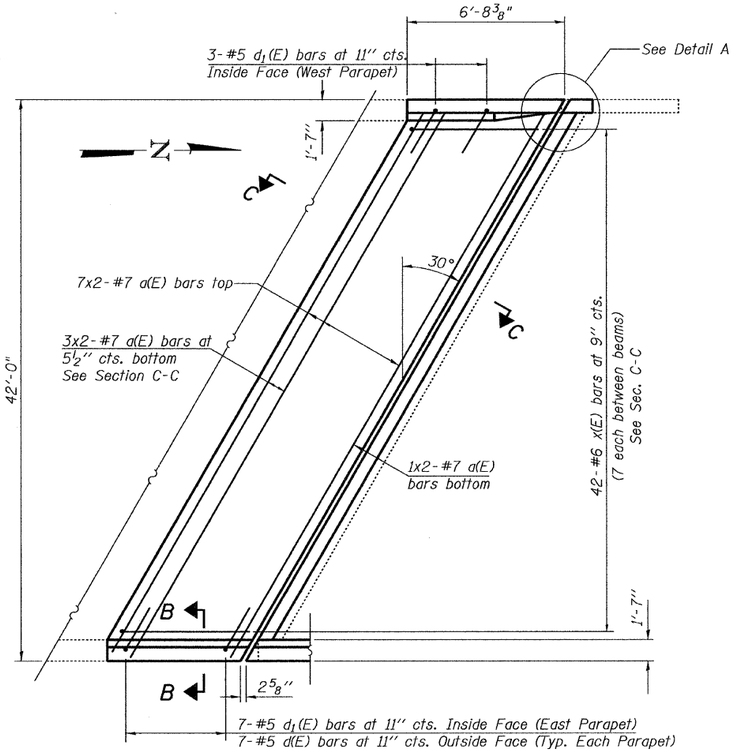
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	Will	72	32
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-		

SHEET NO. 4
19 SHEETS

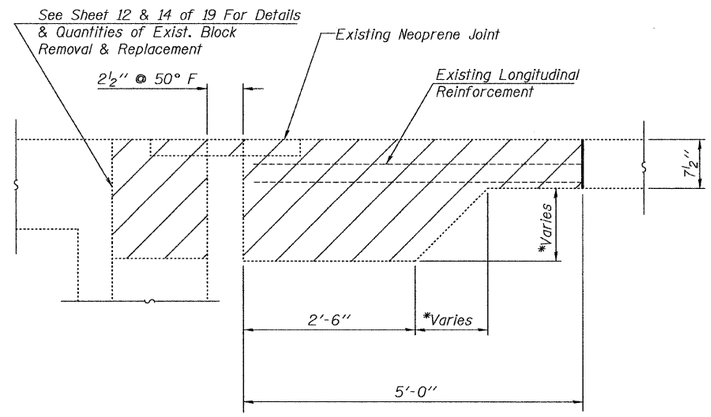
Contract #62930 * 88(B&B-1)BR



PLAN AT SOUTH ABUTMENT



PLAN AT NORTH ABUTMENT



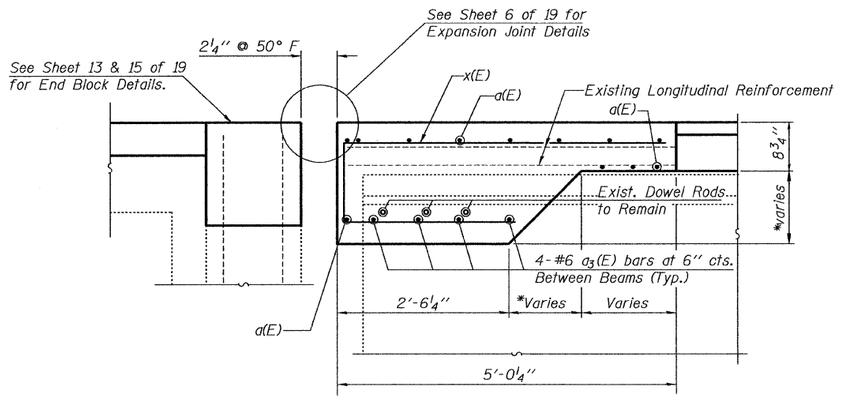
CONCRETE REMOVAL AT ABUTMENTS

(dimensions at right L's)

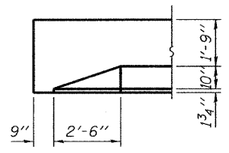
*1'-1" min. @ low beam to 1'-5⅛" max. at high beam

MIN. BAR LAP

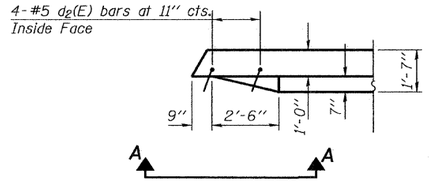
#7 bars = 2'-9"



SECTION C-C



VIEW A-A



DETAIL A

(North Abutment Opposite)

NOTES:

Existing longitudinal reinforcement and dowel rods extending into the removed areas shall be cleaned, straightened and incorporated into the new construction. Cost included with "Concrete Removal".

Hatched areas indicate deck to be removed as "Concrete Removal". Removal of existing neoprene joint is included with "Concrete Removal".

The parapet within the limits of the deck to be removed is also to be removed as "Concrete Removal".

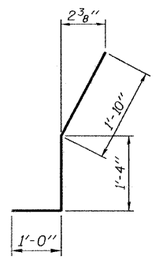
See Sheet 5 of 19 for Section B-B and d(E) and $d_1(E)$ bar details.

Any longitudinal reinforcement bars extending into the new construction that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

Existing transverse reinforcement extending into removed area shall be cut.

BILL OF MATERIAL
2 ABUTMENT JOINTS

Bar	No.	Size	Length	Shape
a(E)	44	#7	25'-3"	
$a_3(E)$	48	#6	4'-3"	
d(E)	28	#5	5'-0"	
$d_1(E)$	20	#5	3'-1"	
$d_2(E)$	8	#5	4'-2"	
x(E)	84	#6	9'-5"	
Concrete Removal			Cu. Yd.	26.0
Concrete Superstructure			Cu. Yd.	34.5
Reinforcement Bars, Epoxy Coated			Pound	4010



BAR $d_2(E)$

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE: EXPANSION JOINT DETAILS ABUTMENTS

PROJECT: I-55 NB OVER KANKAKEE RIVER
FAI ROUTE 55, SECTION 88(B&B-1)BR
WILL COUNTY
SN 099-0001

PROJECT NO. 03095-16
SCALE
DATE 9/23/09
DRAWN BY TFG
CHECKED BY MCB
DRAWING NO. 4

COOMBE-BLOXDORF P.C.
Engineers / Land Surveyors
Springfield, Illinois
Design Firm License No. 184-002703

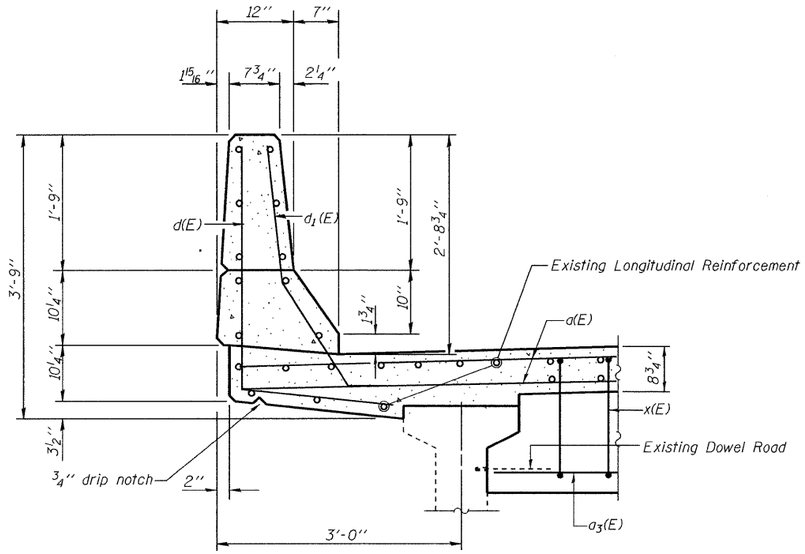
OF 19 SHTS

PLOT DATE = 11/23/2009
FILE NAME = I:\dgn-099-0001-sht-04-expansion-joint-details-abuts.dgn
PLOT SCALE = 64.0000 '/ IN.
USER NAME = CFC

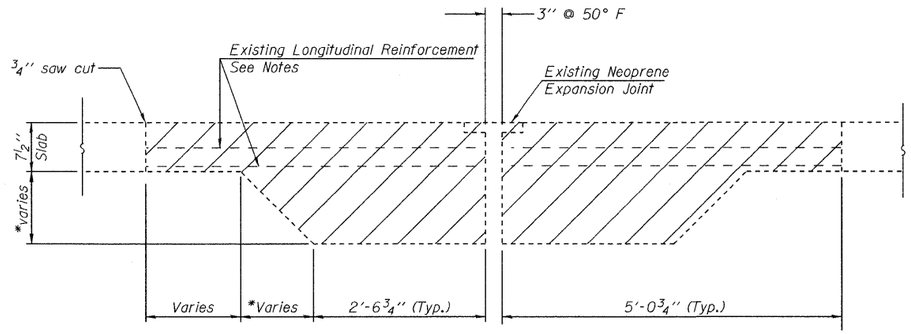
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	Will	72	33
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-	

SHEET NO. 5
19 SHEETS

Contract #62930 * 88(B&B-1)BR



SECTION B-B



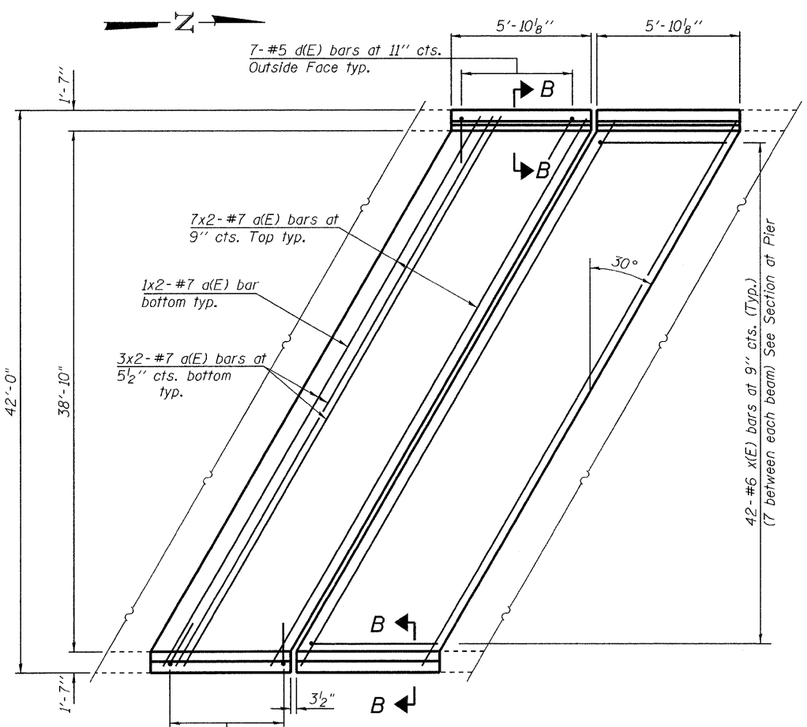
CONCRETE REMOVAL AT PIERS 4 & 8

(dimensions at right L's)

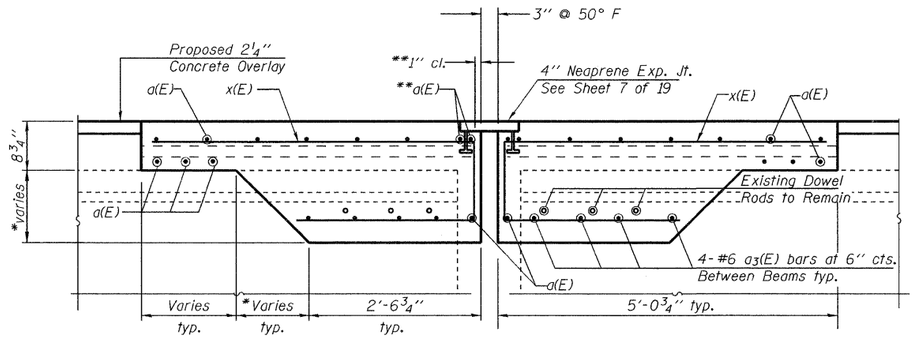
*1'-1'' min. at low beam to
1'-5 1/8'' max. at high beam

BILL OF MATERIAL
2 PIERS

Bar	No.	Size	Length	Shape
a(E)	88	#7	25'-3''	
a3(E)	96	#6	4'-3''	
d(E)	28	#5	5'-0''	
d1(E)	28	#5	3'-1''	
x(E)	168	#6	9'-5''	
Concrete Removal			Cu. Yd.	52.7
Concrete Superstructures			Cu. Yd.	56.3
Reinforcement Bars, Epoxy Coated			Pound	7770

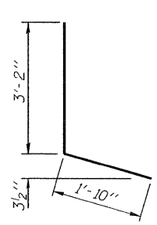


PLAN AT PIERS 4 & 8

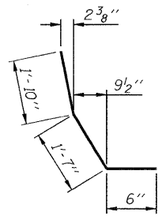


SECTION AT PIERS 4 & 8

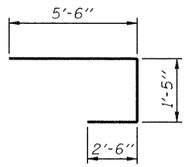
(dimensions at right L's)



BAR d(E)



BAR d1(E)



BAR x(E)

MIN. BAR LAP

#7 bars = 2'-9''

NOTES:

Existing longitudinal reinforcement and dowel rods extending into the removed areas shall be cleaned, straightened and incorporated into the new construction. Cost included with Concrete Removal.

Hatched areas indicate deck to be removed as Concrete Removal. Removal of existing neoprene joint is included with Concrete Removal.

The parapet within the limits of the deck to be removed is also to be removed as Concrete Removal.

Any longitudinal reinforcement bars extending into the New Construction that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

**Place a(E) bars in back of anchor bolt as shown if required to maintain 1'' cl. (±0-1 /8''). Anchor bolts should be tied to a(E) bars, typical each side of joint.

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE
EXPANSION JOINT DETAILS PIERS 4 & 8

PROJECT
I-55 NB OVER KANKAKEE RIVER
FAI ROUTE 55, SECTION 88(B&B-1)BR
WILL COUNTY
SN 099-0001

PROJECT NO. 03095-16
SCALE
DATE 6/25/09
DRAWN BY TFG/CFC
CHECKED BY MCB
DRAWING NO. 5
OF 19 SHTS

COOMBE-BLOXDORF P.C.
Engineers / Land Surveyors
Springfield, Illinois
Design Firm License No. 184-002703

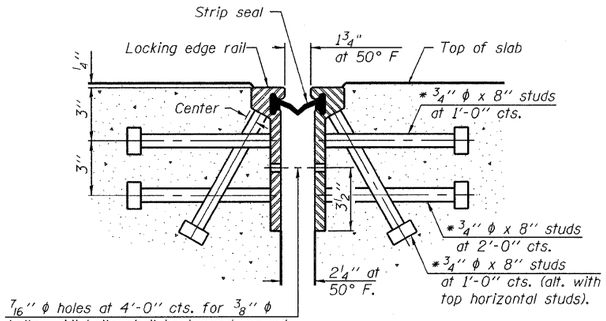
PLOT DATE = 6/25/2009
FILE NAME = ...\sn-099-0001-sht-15-exp-joint-det-PIERS-4-8.dgn
PLOT SCALE = 10.1.7647 '/' / IN.
USER NAME = JML

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	Will	72	34
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-	

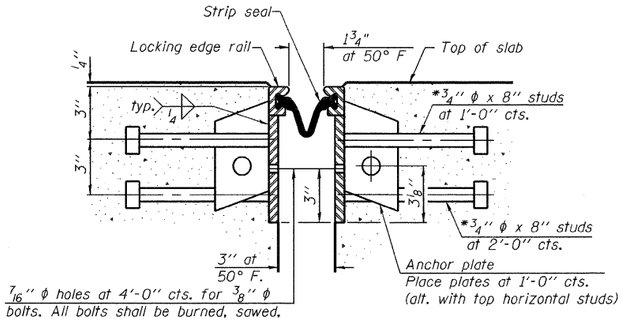
SHEET NO. 6
19 SHEETS

Contract #62930 * 88(B&B-1)BR

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



SECTION THRU ROLLED RAIL JOINT



SECTION THRU WELDED RAIL JOINT

Notes:

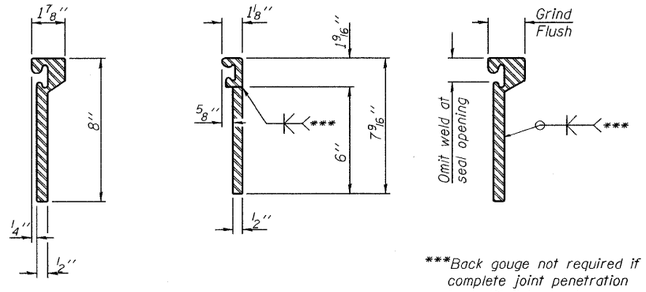
The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The height and thickness of the Locking Edge Rails shown are minimum dimensions. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities and stage construction joints.

The manufacturer's recommended installation methods shall be followed.

The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.



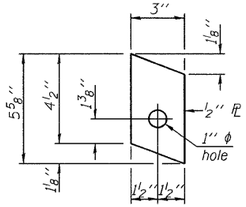
***Back gouge not required if complete joint penetration is verified by mock-up.

ROLLED EXTRUDED RAIL

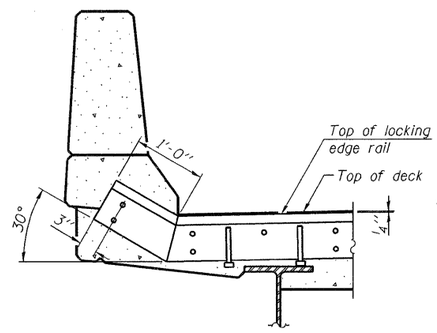
WELDED RAIL

LOCKING EDGE RAIL SPLICE

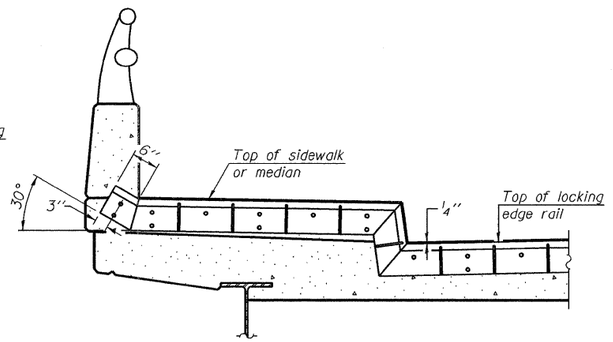
The inside of the locking edge rail groove shall be free of weld residue.



ANCHOR PL (for welded rail)



AT PARAPET

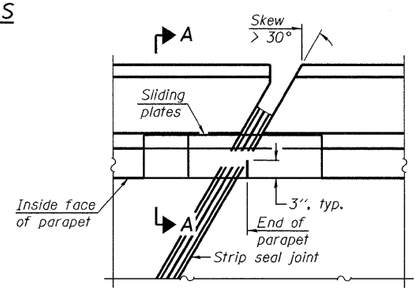


AT SIDEWALK OR MEDIAN

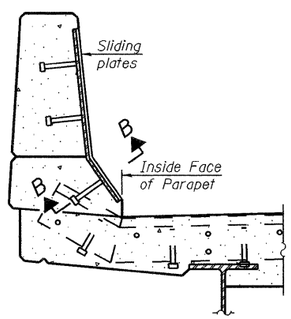
Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.

LOCKING EDGE RAILS

TYPICAL END TREATMENTS

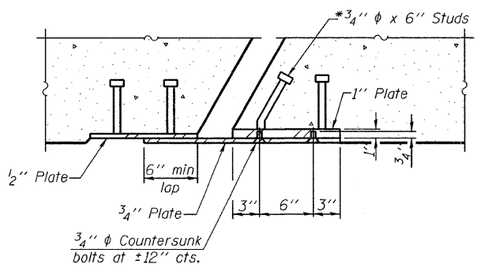


PLAN



SECTION A-A

POINT BLOCK DETAILS
(for skews > 30°)



SECTION B-B

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	92

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE: PREFORMED JOINT STRIP SEAL

PROJECT: I-55 NB OVER KANKAKEE RIVER
FAI ROUTE 55, SECTION 88(B&B-1)BR
WILL COUNTY
SN 099-0001

PROJECT NO. 03095-16
SCALE
DATE 6/25/09
DRAWN BY TFG
CHECKED BY MCB
DRAWING NO. 6

COOMBE-BLOXDORF P.C.
Engineers / Land Surveyors
Springfield, Illinois
Design Firm License No. 184-002703

OF 19 SHTS

PLOT DATE = 6/25/2009
FILE NAME = ...\sn-099-0001-sht-i-5-preformed-joint-strip-seal.dgn
PLOT SCALE = 0.0588 '' / IN.
USER NAME = JML

EJ-SSJ 10-1-08

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	Will	72	35
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-	

SHEET NO. 7
19 SHEETS

Contract #62930 * 88(B&B-1)BR

Joint Size	"C" at 50°F	"D" at 50°F
2"	2"	1½" Min.
2½"	2½"	1¾" Min.
4"	3"	2½" Min.

INSTALLATION NOTES

(1) Install continuous seal in roadway, parapet, curb, and sidewalk.

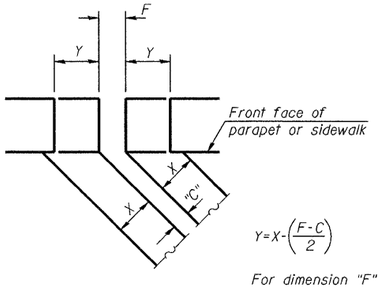
(2) Install anchor blocks as indicated.

Note A:
Maximum spacing of anchor bolts shall be 12" centers.

SKEW LIMITATIONS

The details of the anchor blocks and the elastomeric membrane in the parapet, as shown, are for up to 50° skews.

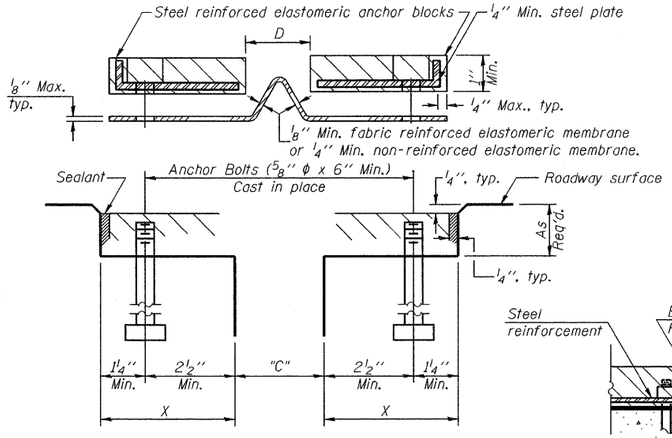
For skews greater than 50°, the anchor blocks and the elastomeric membrane, installed according to dimension "D", might require modifications to insure a minimum clearance of 1½" from centerline of anchor studs to edge of parapet opening. The anchor blocks and the elastomeric membrane shall also be installed to the top of the parapet with the anchor studs spaced at ±12" cts.



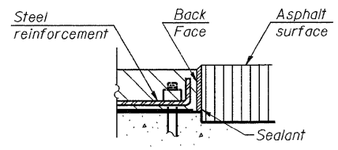
$Y = X - \left(\frac{F-C}{2}\right)$

For dimension "F" see sheet 5 of 19

FORMING BLOCKOUT SKETCH



CROSS SECTION



ANCHOR BLOCK WITH ASPHALT SURFACE

GENERAL NOTES

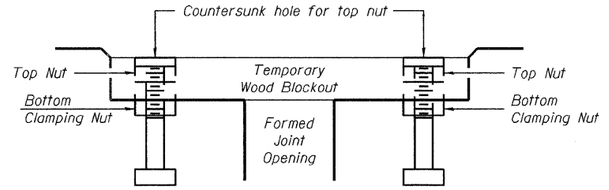
Continuous Seal Neoprene Expansion Joint shall consist of molded anchor blocks of elastomer and steel, field assembled over continuous lengths of elastomeric membrane.

The elastomeric membrane shall be premolded with a single or a double upward convolution that will have a "memory" to return to its molded position upon joint closure.

The convolution length shall be such that the extended length will not be greater than the manufactured length when the joint is fully expanded in its design range and will not protrude above the anchor blocks when the joint is fully compressed.

Joint openings shall be adjusted according to Article 503.10(c) of the Standard Specifications when the deck is poured at an ambient temperature other than 50° F.

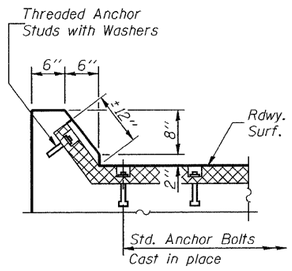
The parapet and roadway membrane shall be made continuous by an approved vulcanizing process. Lapping will not be permitted.



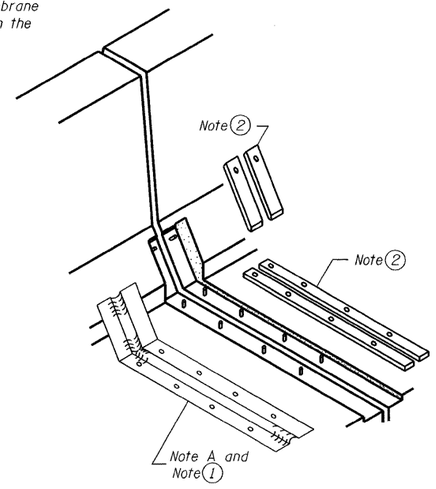
Note:
Stud needs to be threaded lower to allow for use of clamping nut.

Anchor studs should be stainless

RECOMMENDED BLOCKOUT DETAIL



AT CURB



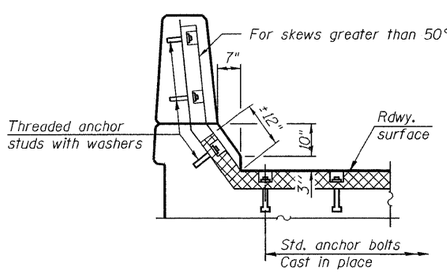
AT PARAPET

AT SIDEWALK OR MEDIAN

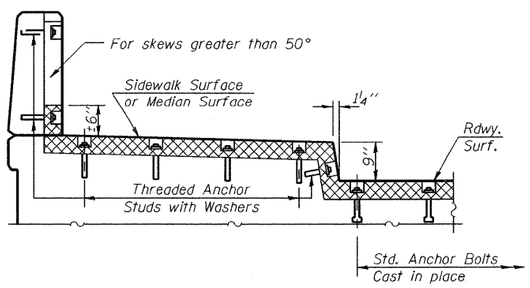
AT WALL

BILL OF MATERIAL

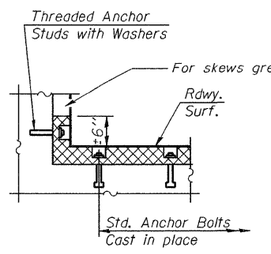
Item	Unit	Total
Neoprene Expansion Joint 4"	foot	94



AT PARAPET



AT SIDEWALK OR MEDIAN



AT WALL

TYPICAL END TREATMENTS

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE
CONTINUOUS SEAL TYPE
NEOPRENE EXPANSION JOINTS

PROJECT
I-55 NB OVER KANKAKEE RIVER
FAI ROUTE 55, SECTION 88(B&B-1)BR
WILL COUNTY
SN 099-0001

PROJECT NO. 03095-16
SCALE
DATE 06/25/09
DRAWN BY CFC
CHECKED BY MCB
DRAWING NO. 7
OF 19 SHTS

COOMBE-BLOXDORF P.C.
Engineers / Land Surveyors
Springfield, Illinois
Design Firm License No. 184-002703

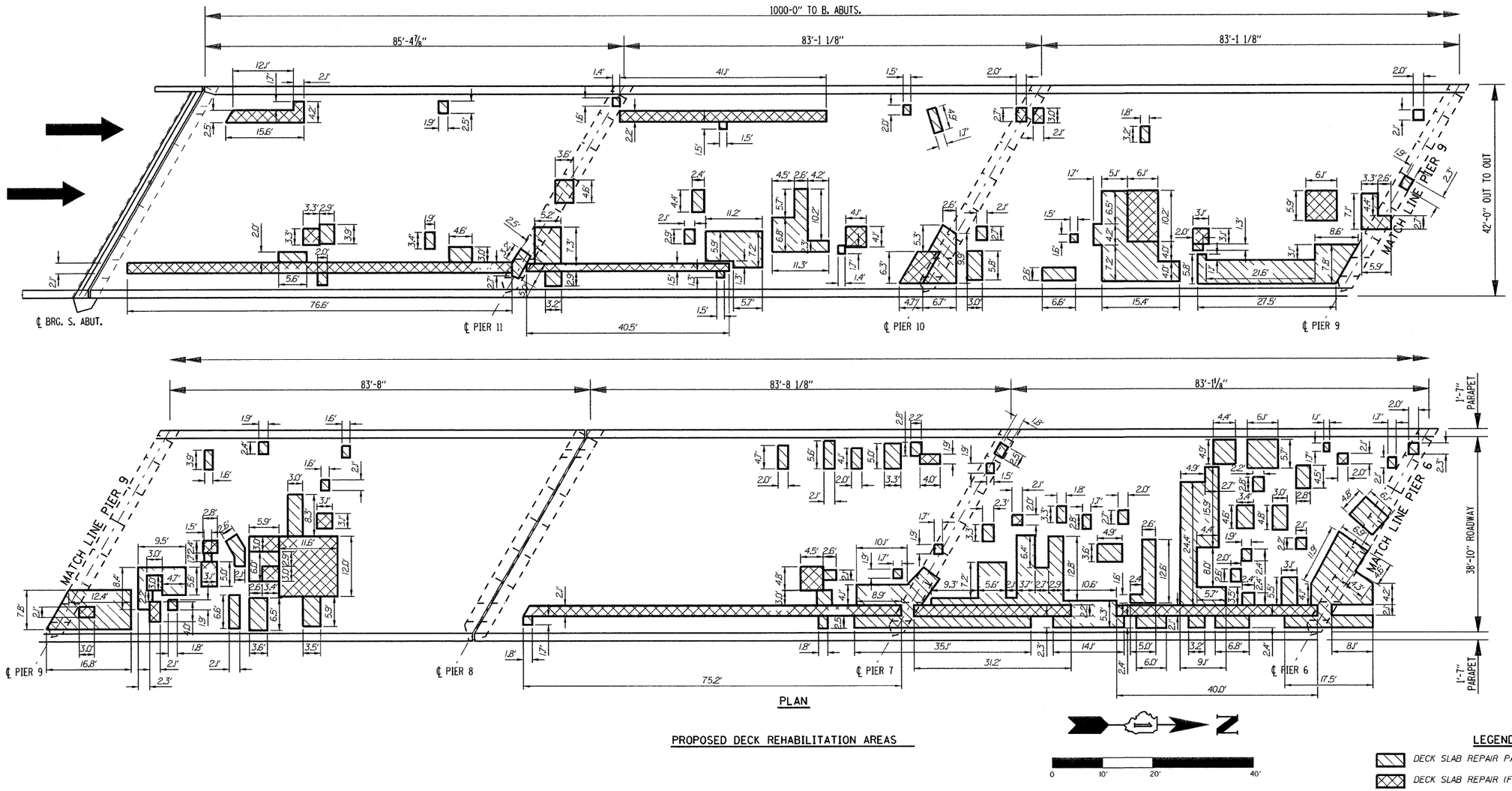
EJ-CS 10-22-04

PLOT DATE = 6/25/2009
FILE NAME = u:\pn-099-0001\snt-7-continous-seal-neo-expan-joints.dgn
PLOT SCALE = 1.0000 '/ IN.
USER NAME = JNL

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	Will	72	36
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-	

SHEET NO. 8
19 SHEETS

Contract #62930 * 88(B&B-1)BR



PLAN

PROPOSED DECK REHABILITATION AREAS

0 10' 20' 40'

LEGEND

- DECK SLAB REPAIR PARTIAL
- DECK SLAB REPAIR (FULL DEPTH, TYPE II)
- DECK SLAB REPAIR (FULL DEPTH, TYPE I)

NOTES:

Deck Drains that are Located In Areas of Full Depth Repair Shall be Replaced as approved by the Engineer. Cost Included with Deck Slab Repair (Full Depth, Type I) or Deck Slab Repair (Full Depth, Type II) as Applicable.

The Areas of Deck Slab Repair are Based on Results of an Infrared Deck Survey Conducted on 04/16/09.

Areas of Deck Slab Repair (Partial) are shown for Information only to assist the Contractor in bidding.

Areas of repair that are located within 10 feet of the parapet face should be repaired in maximum lengths of 10' at a time to ensure stability at the parapet.

BILL OF MATERIAL

ITEM	UNIT	TOTAL
DECK SLAB REPAIR (FULL DEPTH, TYPE I)	Sq Yd	1
DECK SLAB REPAIR (FULL DEPTH, TYPE II)	Sq Yd	124

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE: DECK SLAB REPAIR

PROJECT: I-55 NB OVER KANKAKEE RIVER
FAI ROUTE 55, SECTION 88(B&B-1)BR
WILL COUNTY
SN 099-0001

PROJECT NO. 03095-16
SCALE
DATE 6/25/09
DRAWN BY CFC
CHECKED BY MCB
DRAWING NO. 8

COOMBE-BLOXDORF P.C.
Engineers / Land Surveyors
Springfield, Illinois
Design Firm License No. 184-002703

OF 19 SHTS

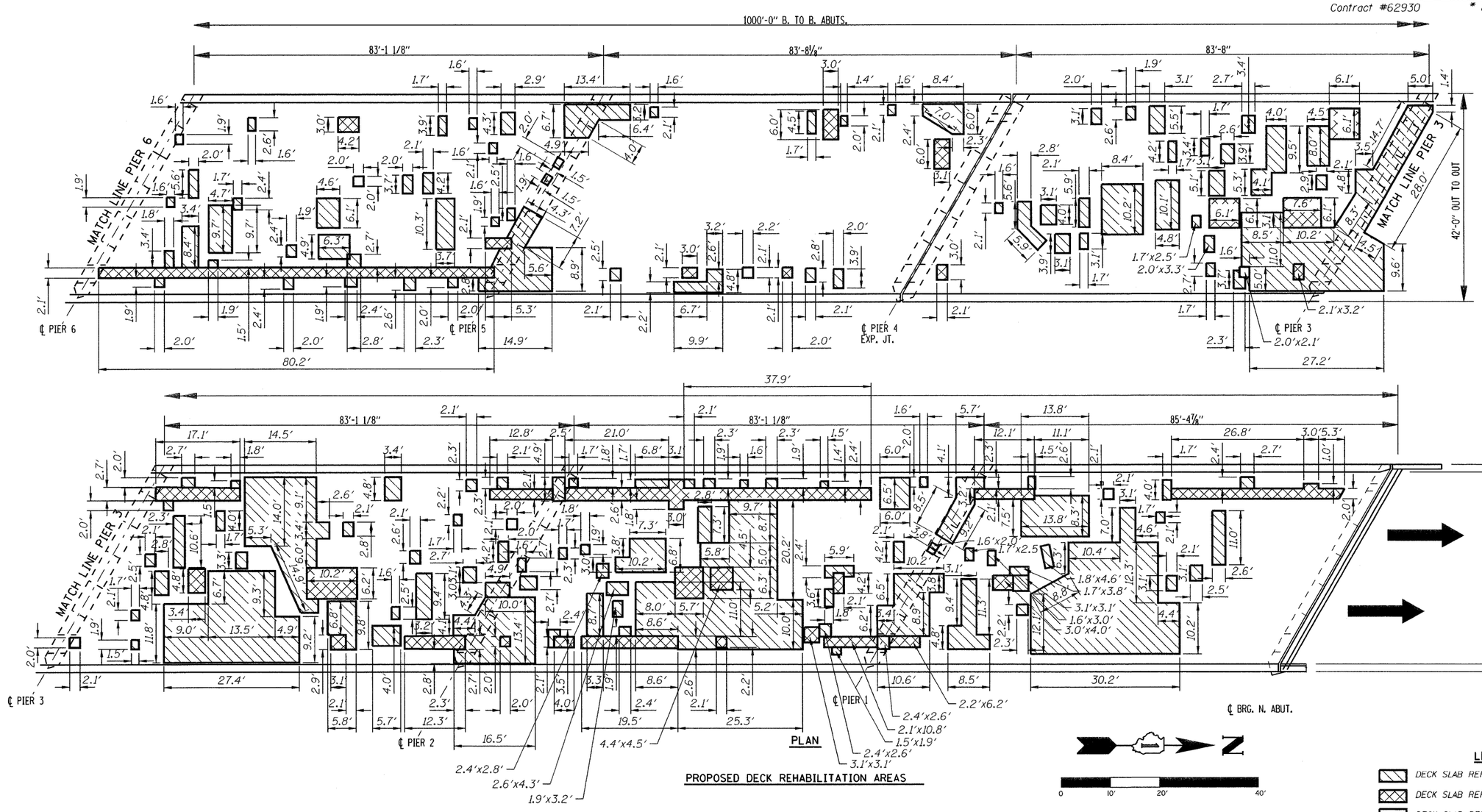
PLOT DATE = 6/25/2009
FILE NAME = ...\Sn-099-0001-snt-8-deck-slab-repair.dgn
PLOT SCALE = 21ill.5294 '/' IN.
USER NAME = JNL

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	Will	72	37
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-	

SHEET NO. 9
19 SHEETS

Contract #62930

* 88(B&B-1)BR



PLAN

PROPOSED DECK REHABILITATION AREAS

LEGEND

- DECK SLAB REPAIR PARTIAL
- DECK SLAB REPAIR (FULL DEPTH, TYPE II)
- DECK SLAB REPAIR (FULL DEPTH, TYPE I)

NOTES:

Deck Drains that are Located In Areas of Full Depth Repair Shall be Replaced as approved by the Engineer. Cost Included with Deck Slab Repair (Full Depth, Type I) or Deck Slab Repair (Full Depth, Type II) as Applicable.

The Areas of Deck Slab Repair are Based on Results of an Infrared Deck Survey Conducted on 04/16/09.

Areas of Deck Slab Repair (Partial) are shown for Information only to assist the Contractor in bidding.

Areas of repair that are located within 10 feet of the parapet face should be repaired in maximum lengths of 10' at a time to ensure stability at the parapet.

BILL OF MATERIAL

ITEM	UNIT	TOTAL
DECK SLAB REPAIR (FULL DEPTH, TYPE I)	Sq Yd	4
DECK SLAB REPAIR (FULL DEPTH, TYPE II)	Sq Yd	114

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE: DECK SLAB REPAIR

PROJECT: I-55 NB OVER KANKAKEE RIVER
FAI ROUTE 55, SECTION 88(B&B-1)BR
WILL COUNTY
SN 099-0001

PROJECT NO. 03095-16
SCALE
DATE 6/25/09
DRAWN BY CFC
CHECKED BY MCB
DRAWING NO. 9
OF 19 SHTS

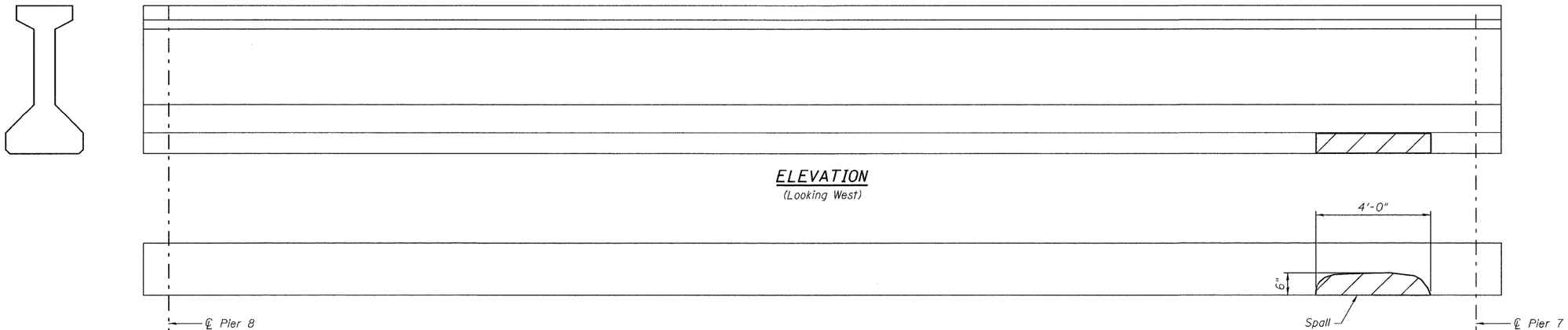
COOMBE-BLOXDORF P.C.
Engineers / Land Surveyors
Springfield, Illinois
Design Firm License No. 184-002703

PLOT DATE = 6/25/2009
FILE NAME = ...\sn-099-0001-snt-9-deck-slab-repair.dgn
PLOT SCALE = 211.5294 '/ IN.
USER NAME = JML

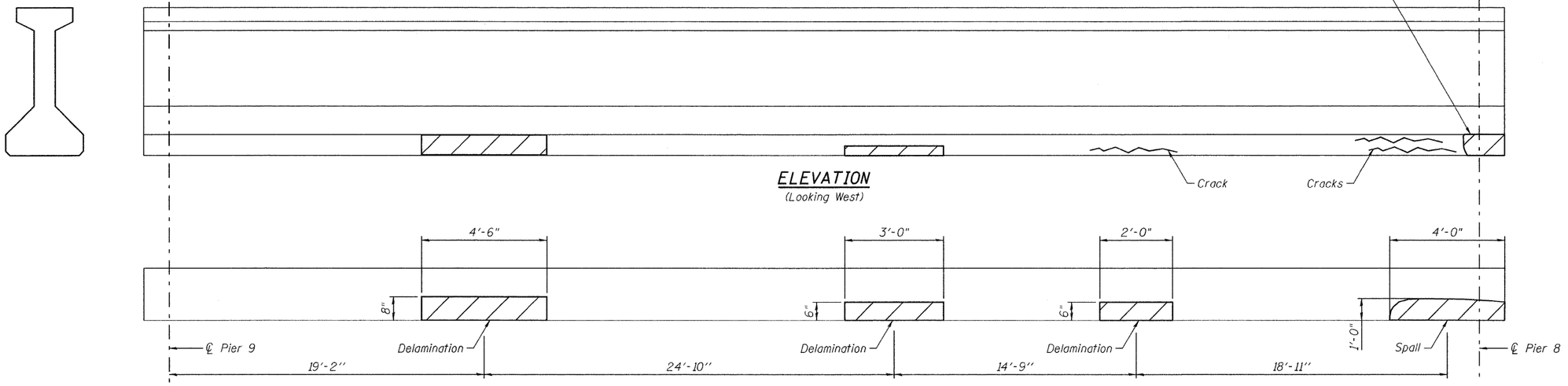
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	Will	72	38
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-	

SHEET NO. 10
19 SHEETS

Contract #62930 * 88(B&B-1)BR



BOTTOM FLANGE
PLAN VIEW
BEAM 1, SPAN 8



BOTTOM FLANGE
PLAN VIEW
BEAM 1, SPAN 9

Note:
See Sheet 11 of 19 for preloading requirements.

LEGEND

Precast Prestressed Concrete I-Beam Repair

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Precast Prestressed Concrete I-Beam Repair	Sq Ft	11.5

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE
BEAM REPAIR DETAILS

PROJECT
I-55 NB OVER KANKAKEE RIVER
FAI ROUTE 55, SECTION 88(B&B-1)BR
WILL COUNTY
SN 099-0001

PROJECT NO. 03095-16
SCALE
DATE 06/25/09
DRAWN BY CFC
CHECKED BY MCB/KPS
DRAWING NO. 10
OF 19 SHTS

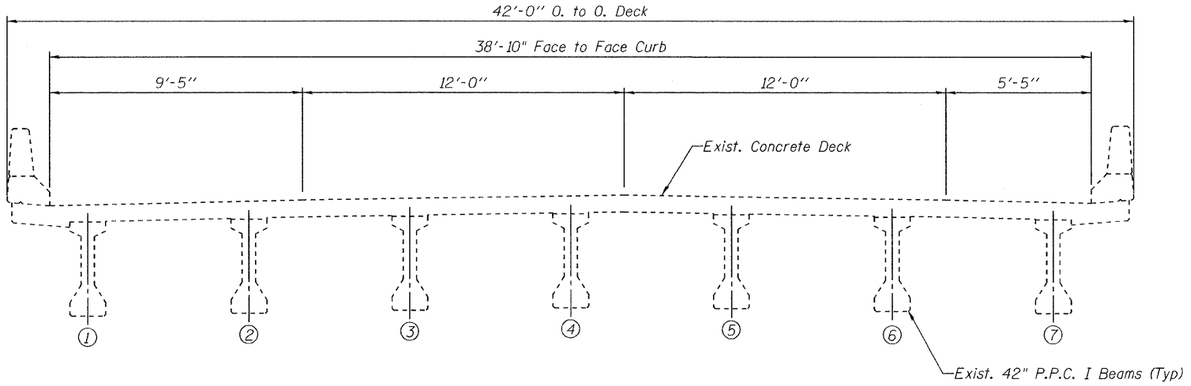
COOMBE-BLOXDORF P.C.
Engineers / Land Surveyors
Springfield, Illinois
Design Firm License No. 184-002703

PLOT DATE = 6/25/2009
FILE NAME = ...\sn-099-0001-sht-10-beam-repair-det.dgn
PLOT SCALE = 56.8824 '/' IN.
USER NAME = JML

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	Will	72	39
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-	

SHEET NO. 11
19 SHEETS

Contract #62930 * 88(B&B-1)BR



EXISTING CROSS SECTION
(Looking South)

PRELOADING FOR PPC I-BEAM REPAIRS
(Service Moment)

BEAM 1

Span	Location		Moment (kip-ft)
	From	Distance*	
9	℄ Pier 9	19'-2"	320
9	℄ Pier 9	44'-0"	649
9	℄ Pier 8	21'-0"	547

*To center of repair area. If actual distance varies by more than 2' from distance shown notify the Engineer to determine Moment required to preload.

REPAIR PROCEDURES FOR BEAM 1 (SPANS 8 AND 9)

1. Beam repairs shall be done before addition of the Bridge Deck Overlay.

2. The damaged area of the beam shall be cleaned of all loose and spalled concrete and sealant. All loose material shall be removed to sound concrete until coarse aggregate will break under chipping rather than dislodging. Hand tools shall be used for the removal of concrete adjacent to the prestressing strands. While a 15 pound chipping hammer may be used away from prestressing strands, extreme care shall be taken not to damage the exposed prestressing strands.

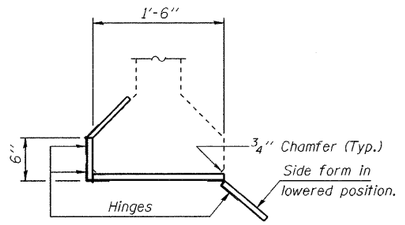
3. Using the same tools, remove the existing concrete to sound concrete, as described above, along the edges of the damaged area to a depth of 1" min. to 1½" max. The edges shall be saw cut ¾" deep. The entire area of existing concrete against which new concrete will be placed and any exposed portions of the prestressing strands shall be sandblasted. The concrete shall be sandblasted to expose clean, well bonded aggregate.

4. Power driven pins as shown in Detail A shall be placed at 9" alternate centers along damaged length of beam at locations shown in Detail A. Place 1" x 1" x 18 gauge welded wire fabric in repair areas and attach it to the pins or strands with wire ties. The clearance between the finished surface of the new concrete and the welded wire fabric shall be 1" minimum. All beams involved in this work shall be rebuilt to their original dimensions.

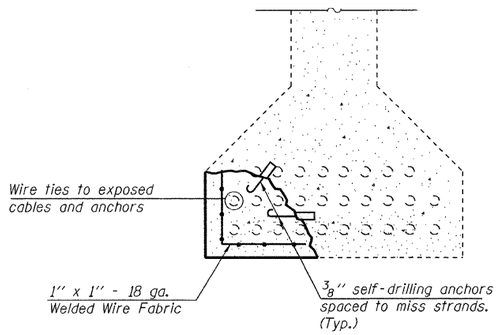
5. The surface of the existing concrete against which new concrete will be placed shall be prepared as a bonded construction joint according to Article 503.09(b) of the Standard Specifications. Other minor mortar repair, crack sealing or surface sealing of gouges on the beam shall be performed as directed by the Engineer.

6. The repair shall be made using a material from the "Approved List of Non-Shrink Grouts" maintained by the Bureau of Materials and Physical Research. The repair material chosen shall be appropriate for the thickness of repair to be made. Coarse aggregate with maximum size of ⅜" shall be added with the amount as specified by the manufacturer. Place the lower form on the bottom of the beam and compact by vibrating (or other approved methods) the mix into the voids.

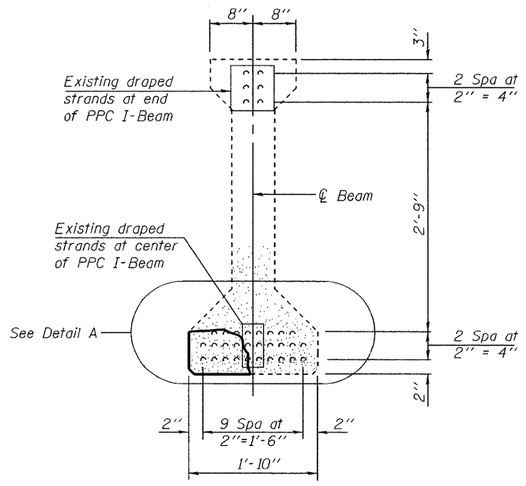
7. Preloading, if specified, and forms shall be kept in place until the repair material has reached an ultimate strength of 5,000 psi. Timing of form removal shall be modified as necessary to meet curing requirements as specified by the manufacturer.



SUGGESTED FORM DETAIL



DETAIL A



TYPICAL PATCHING DETAIL
Beam 1, Spans 8 and 9

NOTE

The cost of concrete removal, Class PS Concrete, power Driven pins, wire ties, wire mesh, epoxy bonding agent, Epoxy Crack Injection and all other work required to perform repairs on Beam 1 in Spans 8 and 9 shall be included in the unit cost per sq. ft. for Precast Prestressed Concrete I-beam repair

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE: BEAM PRELOADING DETAILS

PROJECT: I-55 NB OVER KANKAKEE RIVER
FAI ROUTE 55, SECTION 88(B&B-1)BR
WILL COUNTY
SN 099-0001

PROJECT NO. 03095-16
SCALE
DATE 6/25/09
DRAWN BY TFG
CHECKED BY MCB
DRAWING NO. 11

COOMBE-BLOXDORF P.C.
Engineers / Land Surveyors
Springfield, Illinois
Design Firm License No. 184-002703

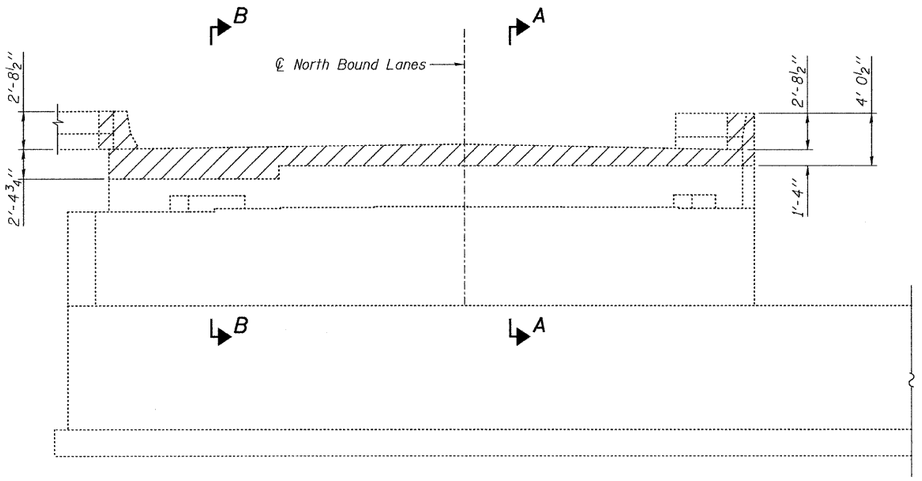
OF 19 SHTS

PLOT DATE = 6/25/2009
FILE NAME = ...\sn-099-0001-snt-11-beam-preloading-details.dgn
PLOT SCALE = 62.0588 '/' IN.
USER NAME = JML

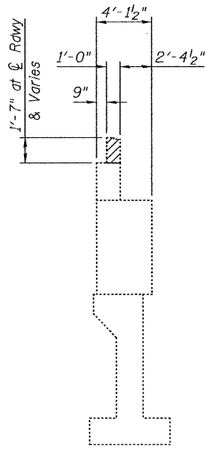
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	Will	72	40
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-		

SHEET NO. 12
19 SHEETS

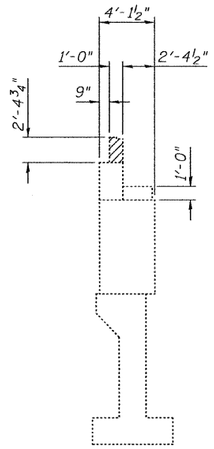
Contract #62930 * 88(B&B-1)BR



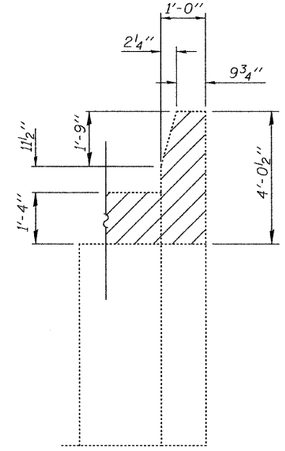
ELEVATION



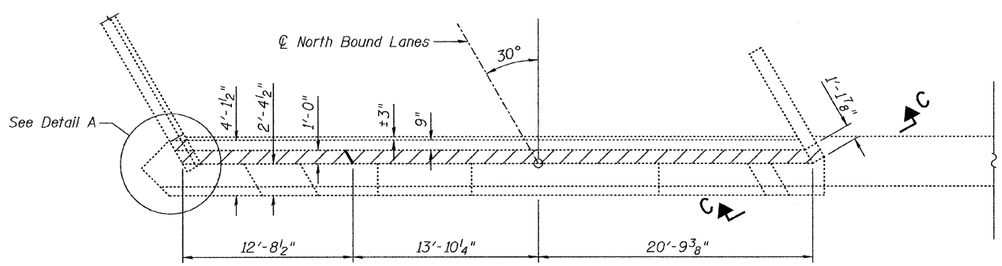
SECTION A-A



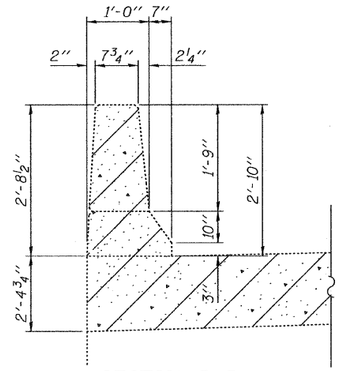
SECTION B-B



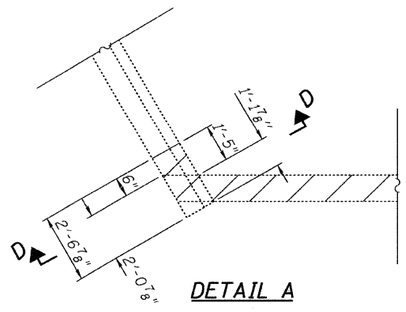
SECTION C-C



PLAN



SECTION D-D



DETAIL A

BILL OF MATERIAL

Item	Unit	Total
Concrete Removal	Cu Yd	3.4

NOTES:

Hatched areas indicate Concrete Removal.

Existing vertical reinforcement extending into the New Construction shall be cleaned, straightened and incorporated into the new construction and existing horizontal reinforcement at the wingwalls extending into the new construction shall be cleaned, straightened and incorporated into the new construction. See Sheet 13 of 19. Cost included with Concrete Removal.

Any reinforcement bars extending into the new construction that are damaged during Concrete Removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

Existing reinforcement not extending into areas of new construction shall be cut at the removal line and removed. Exposed portion will be cleaned and coated with a layer of epoxy. Cost included with Concrete Removal.

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE
SOUTH ABUTMENT CONCRETE REMOVAL

PROJECT
I-55 NB OVER KANKAKEE RIVER
FAI ROUTE 55, SECTION 88(B&B-1)BR
WILL COUNTY
SN 099-0001

PROJECT NO. 03095-16
SCALE
DATE 9/23/09
DRAWN BY TFG/CFC
CHECKED BY MCB
DRAWING NO. 12

COOMBE-BLOXDORF P.C.
Engineers / Land Surveyors
Springfield, Illinois
Design Firm License No. 184-002703

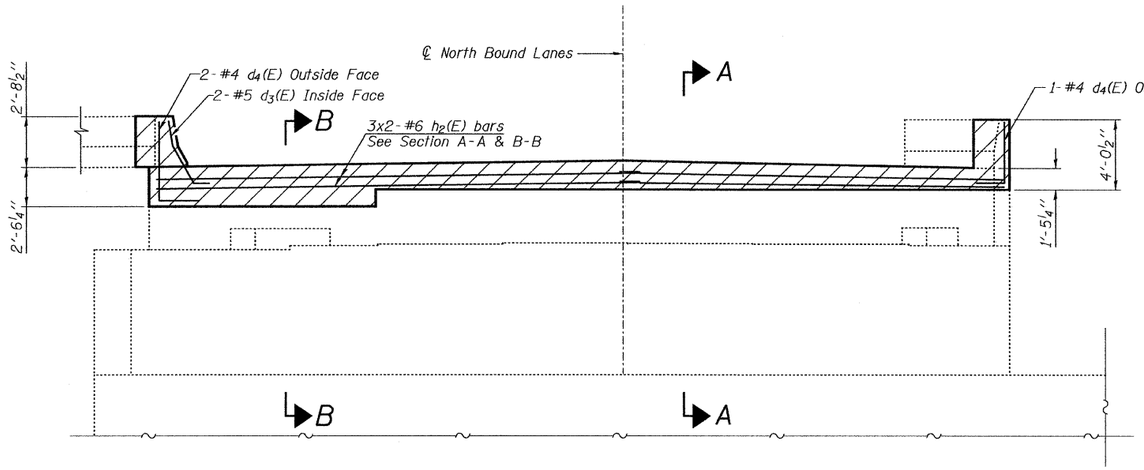
OF 19 SHTS

PLOT DATE = 11/23/2009
FILE NAME = s:\gn-099-0001-sht-12-south-abutment-concrete-removal.dgn
PLOT SCALE = 4.0000 '/ IN.
USER NAME = CFC.

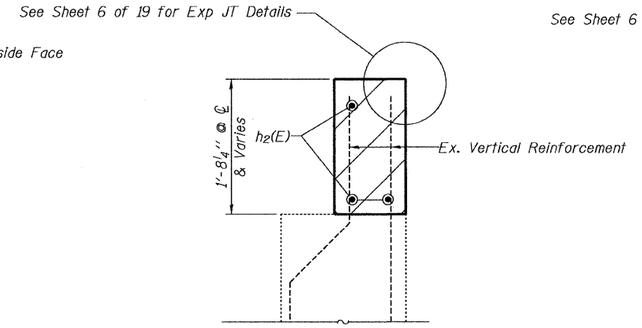
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	WILL	72	41
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-	

SHEET NO. 13
19 SHEETS

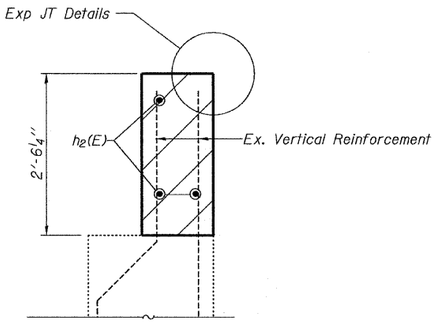
Contract #62930 *BB(B&B-1)BR



ELEVATION



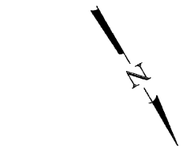
SECTION A-A

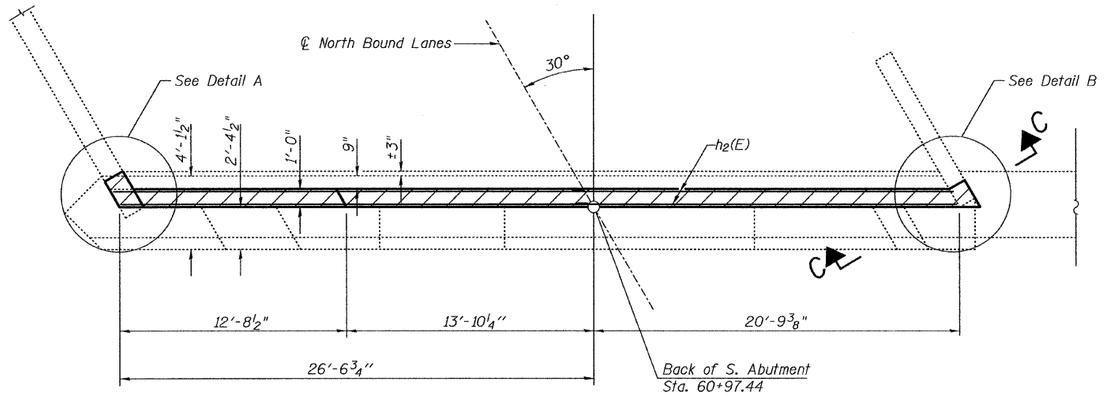


SECTION B-B

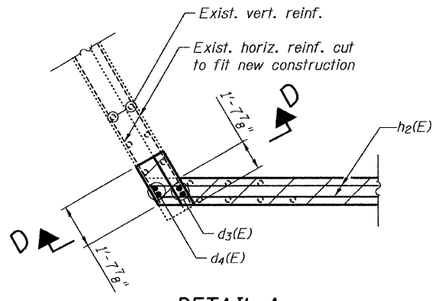
MIN. BAR LAP

#6 bar = 2'-7"

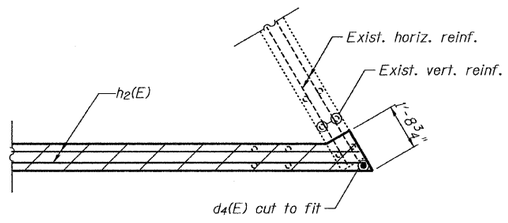




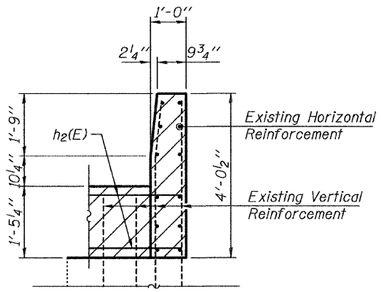
PLAN



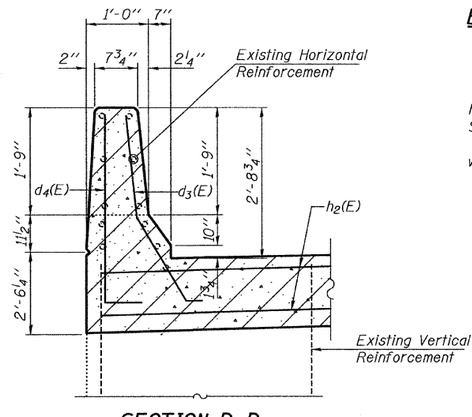
DETAIL A



DETAIL B



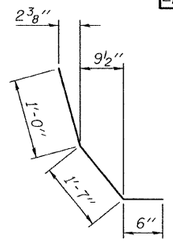
SECTION C-C



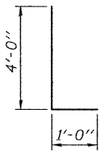
SECTION D-D

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h_2(E)	6	#6	25'-5"	
d_3(E)	2	#5	3'-1"	
d_4(E)	3	#4	5'-0"	
Reinforcement Bars, Epoxy Coated			Pound	240



BAR d_3(E)



BAR d_4(E)

NOTES:

Pour hatched area after proposed deck is in place. Match top of hatched area to top of deck. Quantity included with Concrete Superstructure Sheet 4 of 19.

Bars indicated thus 3x2-#6 h_2(E) indicates 3 lines of bars with 2 lengths per line.

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE: SOUTH ABUTMENT DETAILS

PROJECT: I-55 NB OVER KANKAKEE RIVER
FAI ROUTE 55, SECTION BB(B&B-1)BR
WILL COUNTY
SN 099-0001

PROJECT NO. 03095-16
SCALE
DATE 9/23/09
DRAWN BY TFG
CHECKED BY MCB
DRAWING NO. 13
OF 19 SHTS

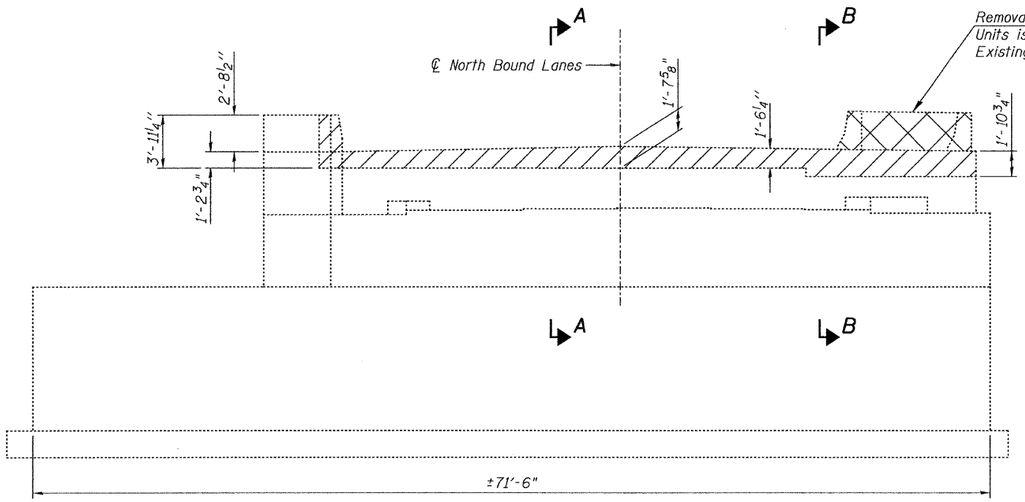
COOMBE-BLOXDORF P.C.
Engineers / Land Surveyors
Springfield, Illinois
Design Firm License No. 184-002703

PLOT DATE = 11/23/2009
FILE NAME = ...\sn-099-0001-snt-13-south-abut-details.dgn
PLOT SCALE = 40.0000 '/ IN.
USER NAME = CFC_

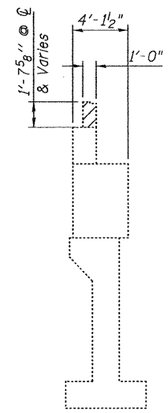
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	WILL	72	42
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-		

SHEET NO. 14
19 SHEETS

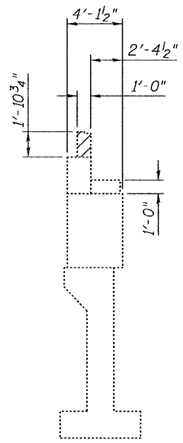
Contract #62930 *BB(B&B-1)BR



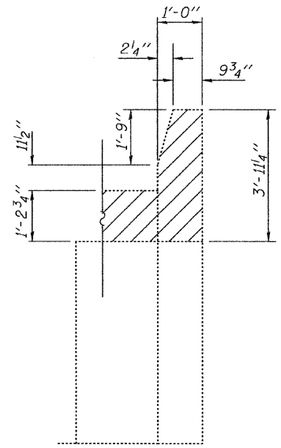
ELEVATION



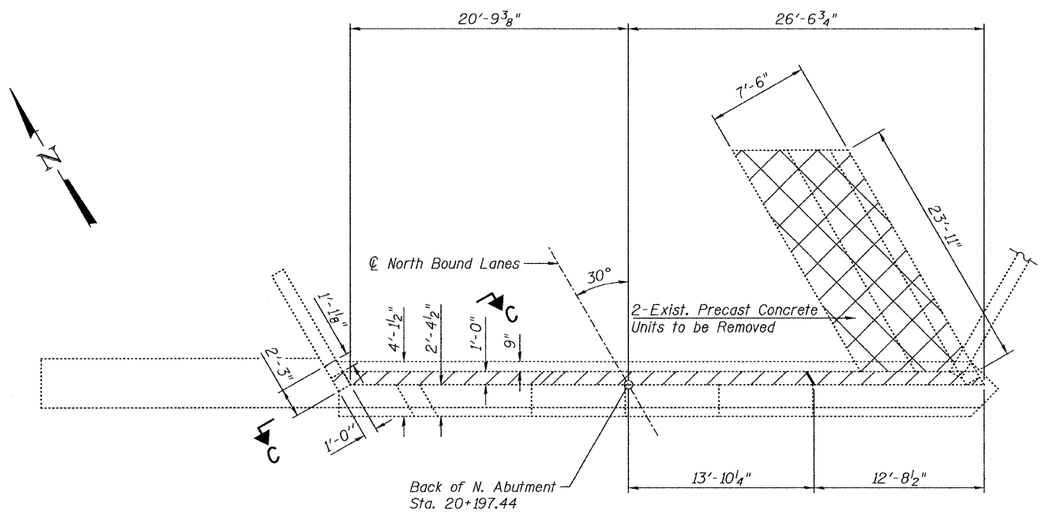
SECTION A-A



SECTION B-B



SECTION C-C



PLAN

BILL OF MATERIAL

Item	Unit	Total
Concrete Removal	Cu Yd	2.9
Removal of Existing Precast Concrete Units	Sq Ft	180

NOTES:

Hatched areas indicate Concrete Removal.
Cross Hatched areas indicate Removal of Existing Precast Concrete Units.
Existing vertical reinforcement extending into the New Construction shall be cleaned, straightened and incorporated into the new construction and existing horizontal reinforcement at the west wingwall extending into the new construction shall be cleaned, straightened and incorporated into the new construction. See Sheet 15 of 19. Cost included with Concrete Removal.
Any reinforcement bars extending into the new construction that are damaged during Concrete Removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
Existing reinforcement not extending into areas of new construction shall be cut at the removal line and removed. Exposed portion will be cleaned and coated with a layer of epoxy. Cost included with Concrete Removal.

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE
NORTH ABUTMENT CONCRETE REMOVAL

PROJECT
I-55 SB OVER KANKAKEE RIVER
FAI ROUTE 55, SECTION 88(B&B-1)BR
WILL COUNTY
SN 099-0001

PROJECT NO. 03095-16
SCALE
DATE 9/23/09
DRAWN BY TFG/CFC
CHECKED BY MCB
DRAWING NO. 14

COOMBE-BLOXDORF P.C.
Engineers / Land Surveyors
Springfield, Illinois
Design Firm License No. 184-002703

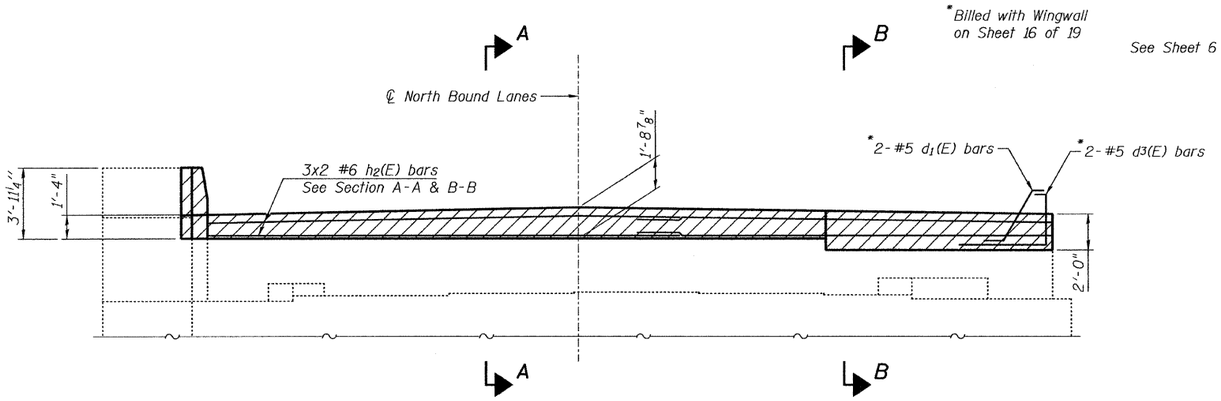
OF 19 SHTS

PLOT DATE = 11/23/2009
FILE NAME = ...\Sn-099-0001-sht-14-north-dbut-concrete-removal.dgn
PLOT SCALE = 1.0000 '/ IN.
USER NAME = CFC.

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	WILL	72	43
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-		

SHEET NO. 15
19 SHEETS

Contract #62930 *88(B&B-1)BR



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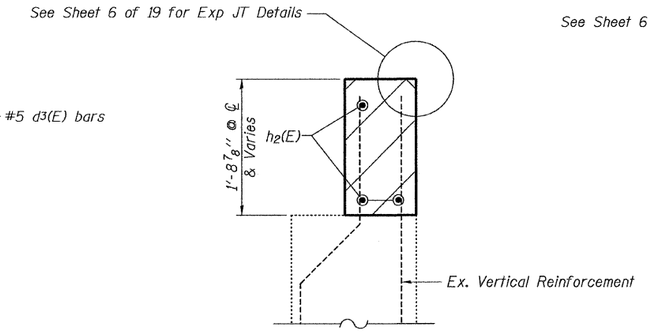
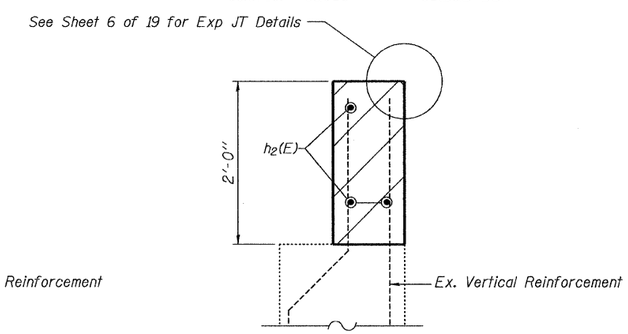


SECTION A-A



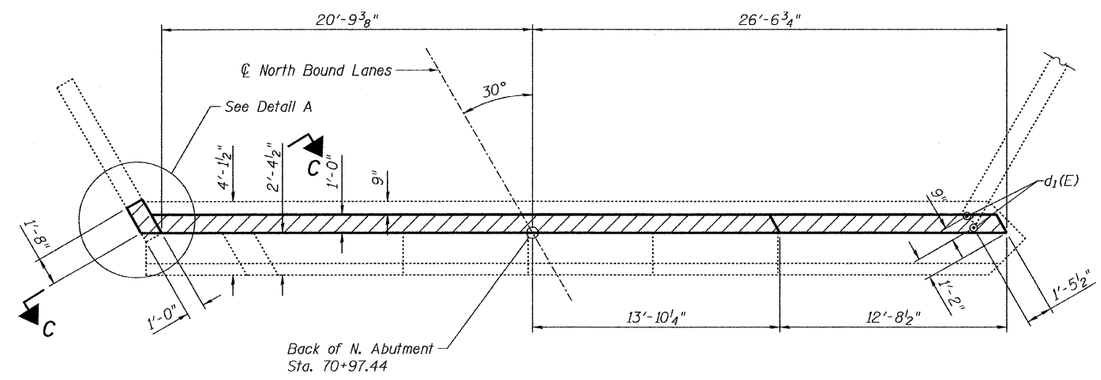
SECTION B-B

MIN. BAR LAP

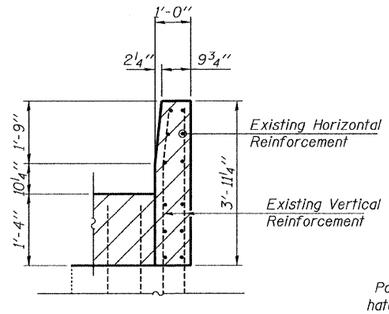
#6 bar = 2'-7"

BILL OF MATERIAL

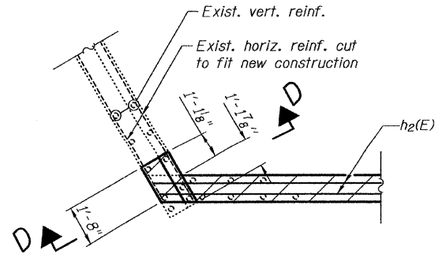
Bar	No.	Size	Length	Shape
h2(E)	6	#6	25'-5"	———
Reinforcement Bars, Epoxy Coated		Pound	230	



PLAN



SECTION C-C



DETAIL A

NOTES:

Pour hatched area after proposed deck is in place. Match top of hatched area to top of deck. Quantity included with Concrete Superstructure Sheet 4 of 19.

Bars indicated thus 3x2-#6 h2(E) indicates 3 lines of bars with 2 lengths per line.

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE: NORTH ABUTMENT DETAILS

PROJECT: I-55 NB OVER KANKAKEE RIVER
FAI ROUTE 55, SECTION BB(B&B-1)BR
WILL COUNTY
SN 099-0001

PROJECT NO. 03095-16
SCALE
DATE 9/23/09
DRAWN BY TFG/CFC
CHECKED BY MCB
DRAWING NO. 15
OF 19 SHTS

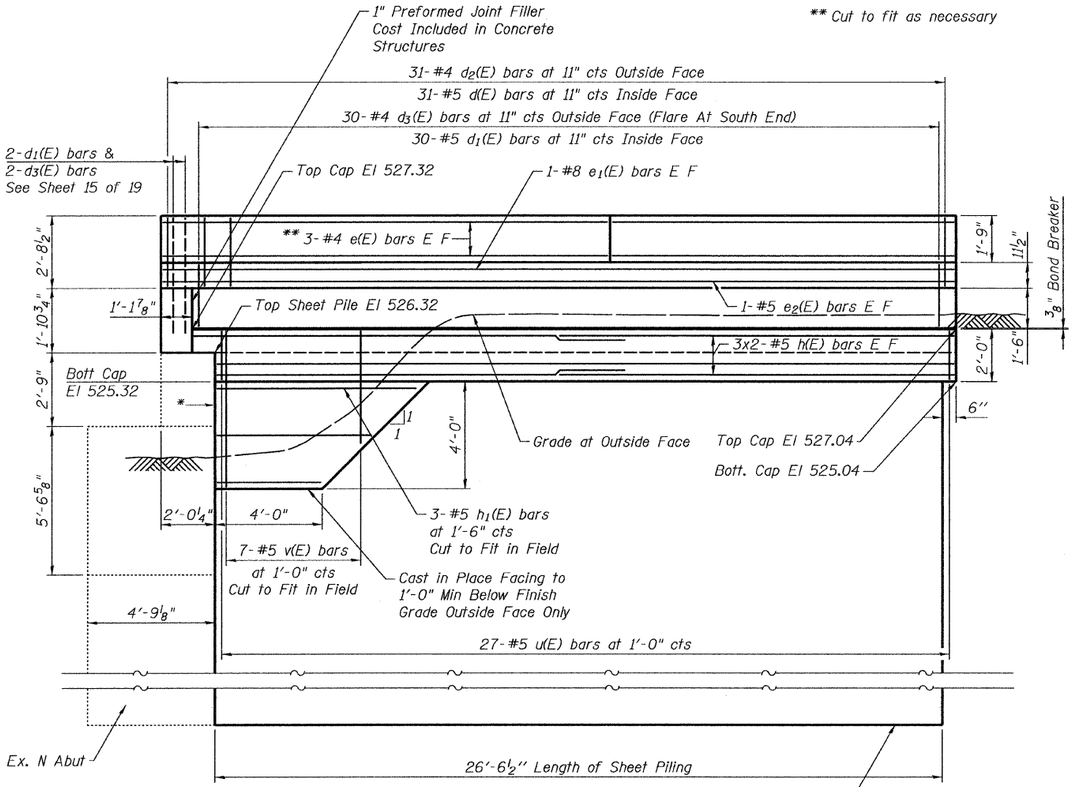
COOMBE-BLOXDORF P.C.
Engineers / Land Surveyors
Springfield, Illinois
Design Firm License No. 184-002703

PLOT DATE = 11/23/2009
FILE NAME = i:\gm-099-0001-ant-15-nor-th-abut-details.dgn
PLOT SCALE = 40.0000 '/ IN.
USER NAME = CFC.

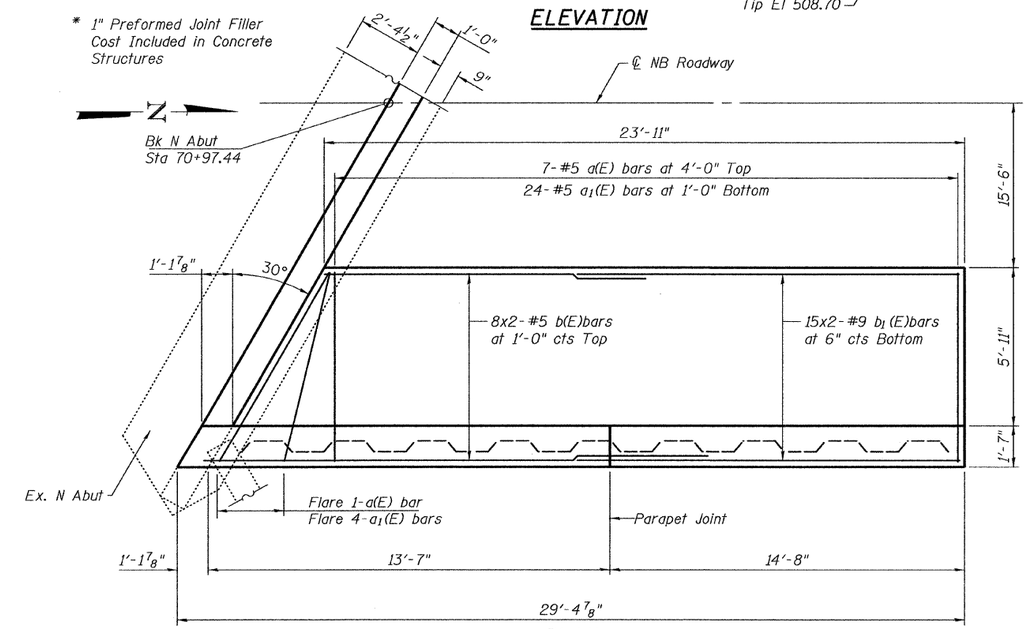
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	Will	72	44
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-		

SHEET NO. 16
19 SHEETS

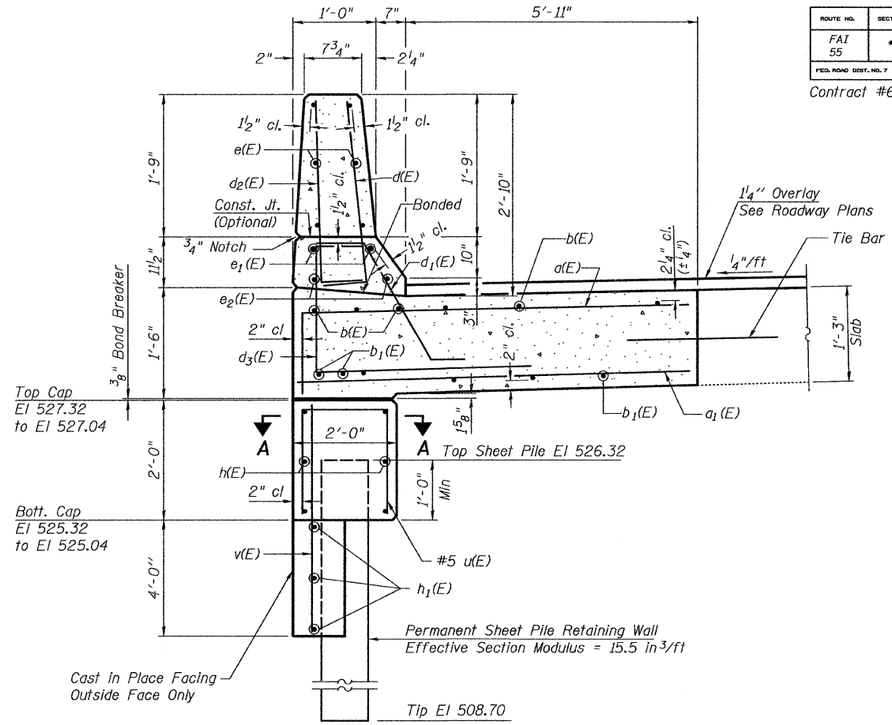
Contract #62930 * 88(B&B-1)BR



ELEVATION



PLAN

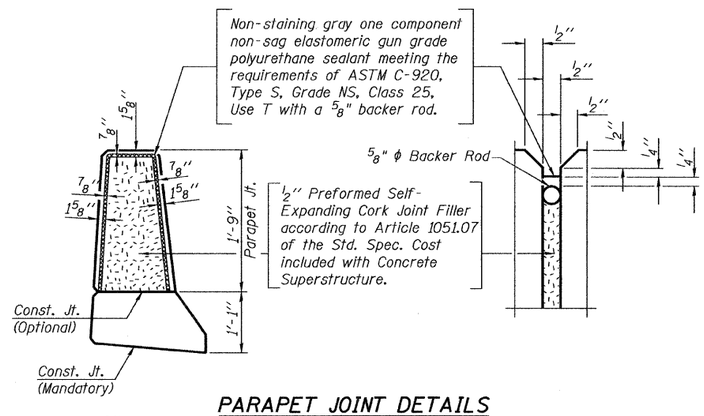


SECTION THRU PARAPET ABOVE STEEL SHEET PILE WALL

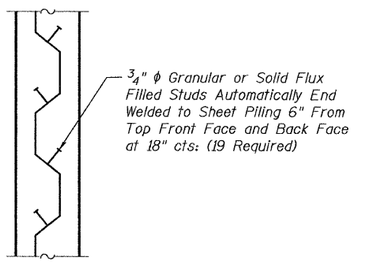
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	8	#5	8'-1"	
a_1(E)	28	#5	7'-2"	
b(E)	16	#5	16'-4"	
b_1(E)	30	#9	17'-6"	
d(E)	31	#5	3'-0"	
d_1(E)	32	#5	2'-8"	
d_2(E)	31	#4	3'-0"	
d_3(E)	32	#4	5'-0"	
e(E)	12	#4	14'-4"	
e_1(E)	2	#8	28'-0"	
e_2(E)	2	#5	28'-0"	
h(E)	12	#5	15'-3"	
h_1(E)	3	#5	7'-8"	
u(E)	27	#5	5'-0"	
v(E)	7	#5	5'-8"	
Concrete Structures			Cu. Yd.	17.3
Reinforcement Bars, Epoxy Coated			Pound	3400
Structure Excavation			Cu. Yd.	10
Permanent Steel Sheet Piling			Sq. Ft.	468
Stud Shear Connectors			Each	19
Protective Coat			Sq. Yd.	12

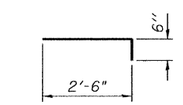
Bars Indicated Thus 3x2-#5 etc Indicates 3 Lines of Bars with 2 Lengths Per Line.



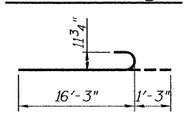
PARAPET JOINT DETAILS



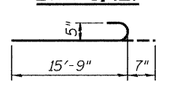
SECTION A-A



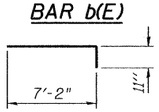
BAR d(E) & d_2(E)



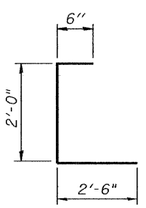
BAR b_1(E)



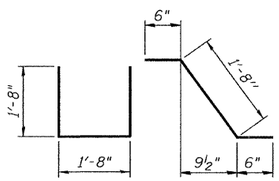
BAR b(E)



BAR a(E)



BAR d_3(E)



BAR u(E)

BAR d_1(E)

MIN BAR LAPS

#4 bars 1'-4"
#5 bars 2'-5"
#9 bars 4'-7"

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE
NORTH ABUTMENT WINGWALL DETAILS

PROJECT
I-55 NB OVER KANKAKEE RIVER
FAI ROUTE 55, SECTION 88(B&B-1)BR
WILL COUNTY
SN 099-0001

PROJECT NO. 03095-16
SCALE
DATE 9/23/09
DRAWN BY TFG/CFC
CHECKED BY MCB
DRAWING NO. 16 OF 19 SHTS

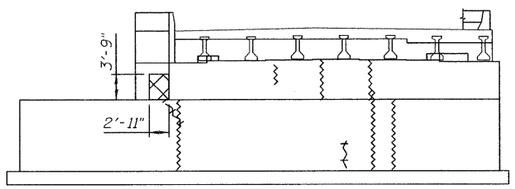
COOMBE-BLOXDORF P.C.
Engineers / Land Surveyors
Springfield, Illinois
Design Firm License No. 184-002703

PLOT DATE = 11/23/2009
FILE NAME = ...\bri-099-0001-sht-16-north-abut-wingwall-det.dgn
PLOT SCALE = 2.0000 '/ IN.
USER NAME = CFC.

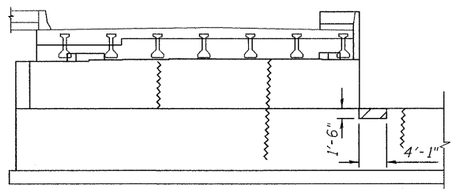
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	Will	72	45
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-	

SHEET NO. 17
19 SHEETS

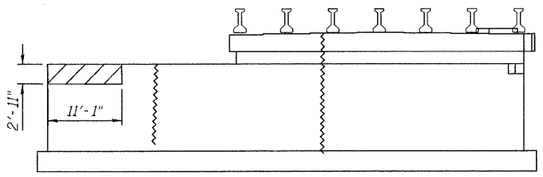
Contract #62930 * 88(B&B-1)BR



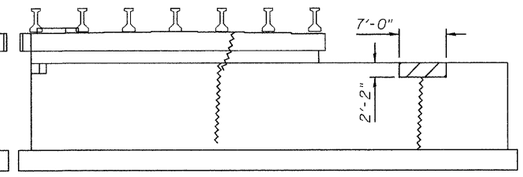
NORTH ABUTMENT
(LOOKING NORTH)



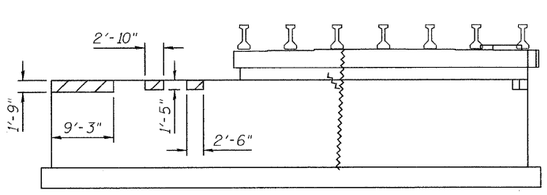
SOUTH ABUTMENT
(LOOKING SOUTH)



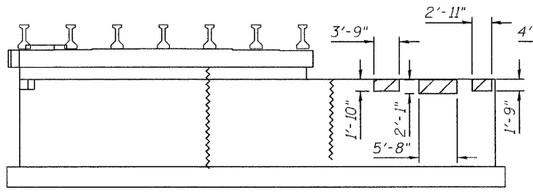
PIER 1
ELEVATION, SOUTH FACE
(LOOKING NORTH)



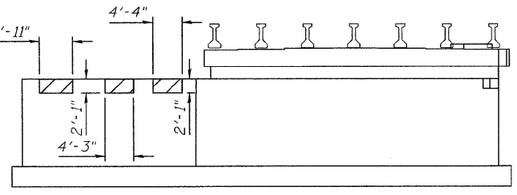
PIER 1
ELEVATION, NORTH FACE
(LOOKING SOUTH)



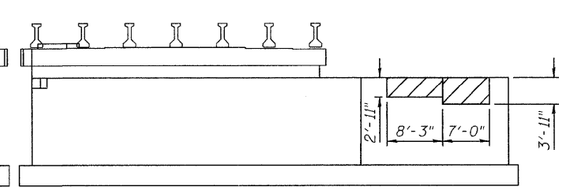
PIER 2
ELEVATION, SOUTH FACE
(LOOKING NORTH)



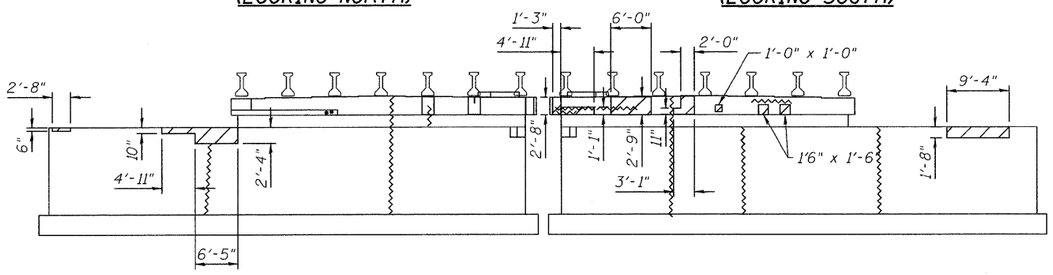
PIER 2
ELEVATION, NORTH FACE
(LOOKING SOUTH)



PIER 3
ELEVATION, SOUTH FACE
(LOOKING NORTH)

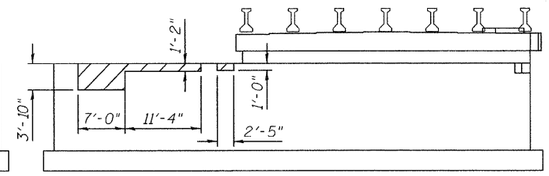


PIER 3
ELEVATION, NORTH FACE
(LOOKING SOUTH)

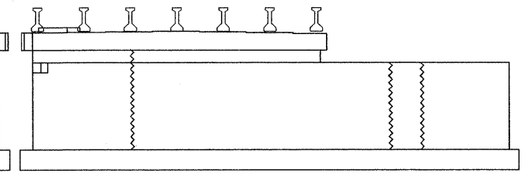


PIER 4
ELEVATION, SOUTH FACE
(LOOKING NORTH)

PIER 4
ELEVATION, NORTH FACE
(LOOKING SOUTH)



PIER 5
ELEVATION, SOUTH FACE
(LOOKING NORTH)



PIER 5
ELEVATION, NORTH FACE
(LOOKING SOUTH)

LEGEND

Epoxy Crack Injection

Structural Repair of Concrete
(Depth Equal to or Less Than 5")

Structural Repair of Concrete
(Depth greater Than 5")

Note:
Quantities are Included in Bill of Material on Sheet 18 of 19

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE
ABUTMENT & PIER REPAIR DETAILS

PROJECT
I-55 NB OVER KANKAKEE RIVER
FAI ROUTE 55, SECTION 88(B&B-1)BR
WILL COUNTY
SN 099-0001

PROJECT NO. 03095-16
SCALE
DATE 06/25/09
DRAWN BY CFC
CHECKED BY MCB/KPS
DRAWING NO. 17
OF 19 SHTS

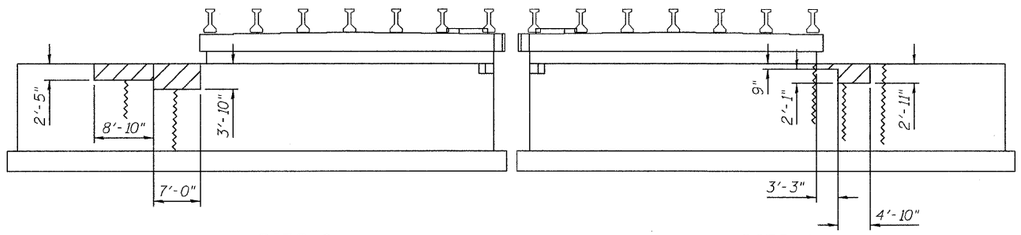
COOMBE-BLOXDORF P.C.
Engineers / Land Surveyors
Springfield, Illinois
Design Firm License No. 184-002703

PLOT DATE = 6/25/2009
FILE NAME = ...\dgn-099-0001-sht-17-abut-pier-repair-det.dgn
PLOT SCALE = 21in1.5294 '=' / IN.
USER NAME = JWL

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	Will	72	46
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-		

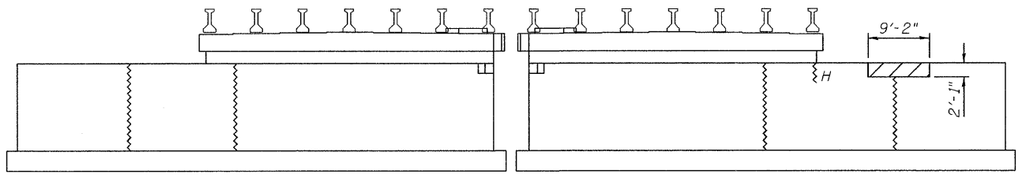
SHEET NO. 18
19 SHEETS

Contract #62930 * 88(B&B-1)BR



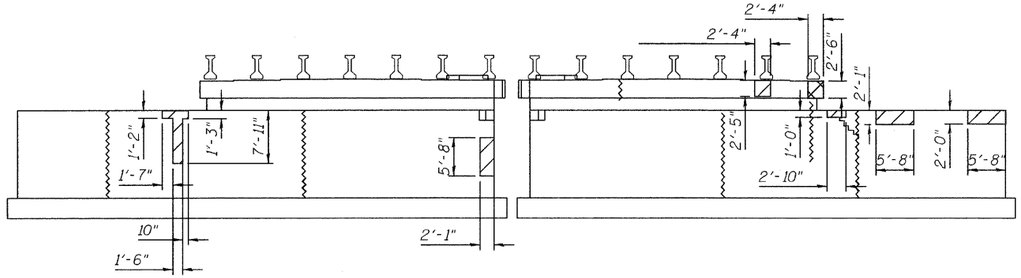
PIER 6
ELEVATION, SOUTH FACE
(LOOKING NORTH)

PIER 6
ELEVATION, NORTH FACE
(LOOKING SOUTH)



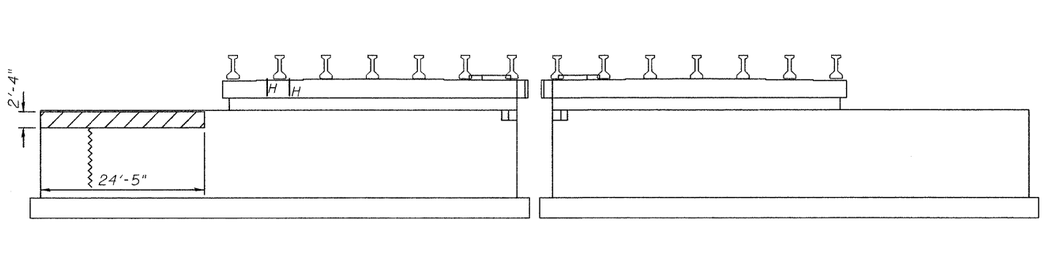
PIER 7
ELEVATION, SOUTH FACE
(LOOKING NORTH)

PIER 7
ELEVATION, NORTH FACE
(LOOKING SOUTH)



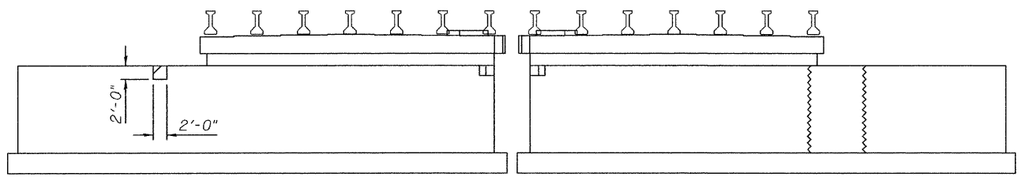
PIER 8
ELEVATION, SOUTH FACE
(LOOKING NORTH)

PIER 8
ELEVATION, NORTH FACE
(LOOKING SOUTH)



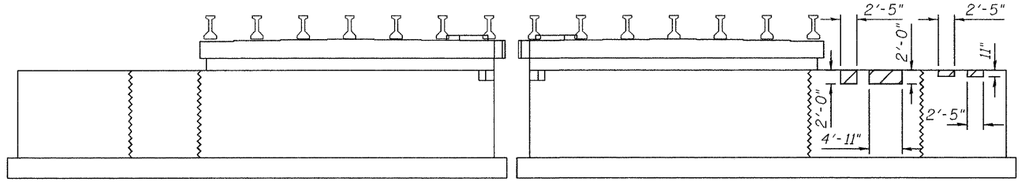
PIER 9
ELEVATION, SOUTH FACE
(LOOKING NORTH)

PIER 9
ELEVATION, NORTH FACE
(LOOKING SOUTH)



PIER 10
ELEVATION, SOUTH FACE
(LOOKING NORTH)

PIER 10
ELEVATION, NORTH FACE
(LOOKING SOUTH)



PIER 11
ELEVATION, SOUTH FACE
(LOOKING NORTH)

PIER 11
ELEVATION, NORTH FACE
(LOOKING SOUTH)

LEGEND

- Epoxy Crack Injection
- Structural Repair of Concrete (Depth Equal to or Less Than 5")
- Structural Repair of Concrete (Depth greater Than 5")

BILL OF MATERIAL

Item	Unit	Total
Epoxy Crack Injection	Foot	627
Structural Repair of Concrete (Depth Equal to or Less Than 5")	Sq Ft	531
Structural Repair of Concrete (Depth Greater Than 5")	Sq Ft	17

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE
PIER REPAIR DETAILS

PROJECT
I-55 NB OVER KANKAKEE RIVER
FAI ROUTE 55, SECTION 88(B&B-1)BR
WILL COUNTY
SN 099-0001

PROJECT NO. 03095-16
SCALE
DATE 06/25/09
DRAWN BY CFC
CHECKED BY MCB/KPS
DRAWING NO. 18

COOMBE-BLOXDORF P.C.
Engineers / Land Surveyors
Springfield, Illinois
Design Firm License No. 184-002703

OF 19 SHTS

PLOT DATE = 6/25/2009
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USER NAME = JML

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	Will	72	47
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-	

SHEET NO. 19
19 SHEETS

Contract #62930 * 88(B&B-1)BR

Illinois Department of Transportation
Division of Highways
ILLINOIS DEPARTMENT OF TRANSPORTATION

SOIL BORING LOG

Page 1 of 1

Date 7/6/05

ROUTE FAI 55 DESCRIPTION NB I-55 over Kankakee River LOGGED BY J. Stewart

SECTION 88(B & B1)BR LOCATION SW, SEC. 10, TWP. 33, RNG. 9, 3rd PM

COUNTY WILL DRILLING METHOD CME 750, 3.25 ID H.S.A. HAMMER TYPE CME Automatic

STRUCT. NO. 099-0001
Station

BORING NO. B-1
Station 21+03
Offset 5.00ft Rt NB I-55 Edge Lane Line
Ground Surface Elev. 528.70 ft

Surface Water Elev. 507.86 ft
Stream Bed Elev. 508.78 ft

Groundwater Elev.:
First Encounter 505.7 ft ▼
Upon Completion ft
After Hrs. ft

DEPTH (ft)	BLOWS (/6")	UCS Qu (tsf)	MOIST (%)	DEPTH (ft)	BLOWS (/6")	UCS Qu (tsf)	MOIST (%)
8" Reinforced Concrete 528.03				Medium Stiff to Stiff mottled Gray/Black SILTY CLAY LOAM *(continued)*	7		14
Augured through fine GRAVEL (Fill) 528.70			13	Grades with Weathered LIMESTONE Fragments	47 10		
Very Stiff Gray SILTY CLAYw/ trace fine gravel 525.70	2	2.5 P	9	505.70 ▼	3		25
Loose to Medium Dense fine SAND and GRAVEL (Fill)	4 5		7	Probable LIMESTONE Bedrock (rotary wash drill with tricone roller bit.)	100/4.5"		
-5	4			-25			
	7 6		8				
	5			500.70			
	5 3		6	Note: Stationing changed from Previous Construction End of Boring			
518.20 -10				-30			
Soft Gray SILTY CLAY LOAM	2 2 2		14				
516.70 Soft Brown SILTY CLAY LOAM with occasional shale fragments	1 1 9		17				
-15				35			
Grades w/ trace GRAVEL	0 1 5		10				
Limestone Cobble @ 17.5 feet 513.70	17						
Medium Stiff to Stiff mottled Gray/Black SILTY CLAY LOAM	18 B	1.0 P	15				
-20				-40			

C:\PROGRAM FILES\GINT\PROJECTS\I-55 NB @ KANKAKEE RIVER.GPJ - 7/7/2005

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, from 137 (Rev. 8-99)

PLOT DATE	= 6/25/2009
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USER NAME	= JML_

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE: BORING LOG

PROJECT: I-55 NB OVER KANKAKEE RIVER
FAI ROUTE 55, SECTION 88(B&B-1)BR
WILL COUNTY
SN 099-0001

PROJECT NO. 03095-16
SCALE
DATE 06/25/09
DRAWN BY CFC
CHECKED BY MCB/KPS
DRAWING NO. 19

COOMBE-BLOXDORF P.C.
Engineers / Land Surveyors
Springfield, Illinois
Design Firm License No. 184-002703

OF 19 SHTS

Benchmark: Chiseled "X" on top of west parapet, south end of SN 099-0002, Elev. 533.22

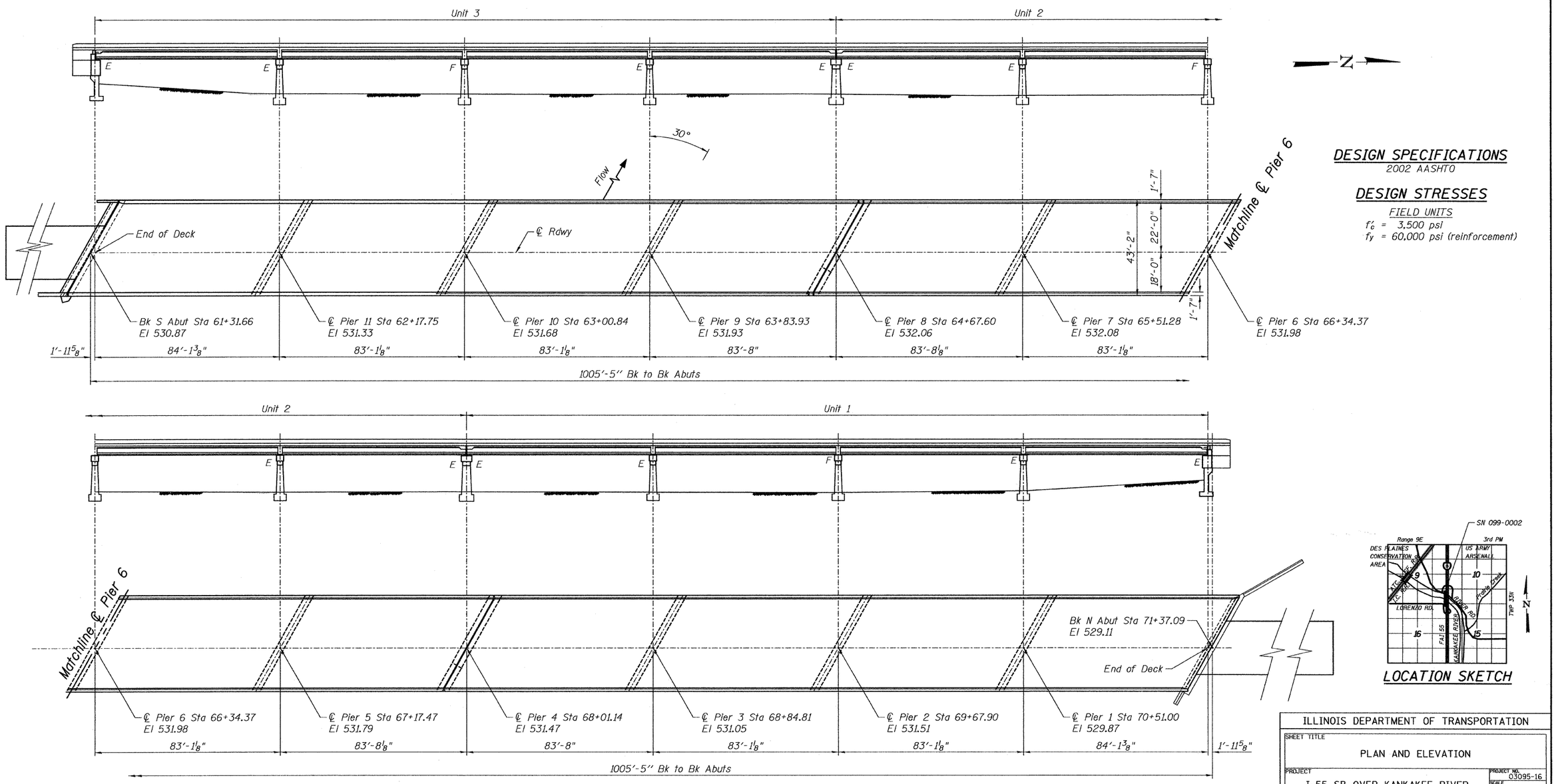
Existing Structure: 12 Span PPC I-Beam Superstructure Consisting of Three (4 Span) Continuos Units on Solid Concrete Piers and Closed Concrete Abutments. The Abutments and Piers Were Built in 1955 and New Caps and Superstructure Were Added in 1986.

Proposed Improvement: Deck Repair. Scarify Deck 1" and Install Bridge Deck Latex concrete Overlay $2\frac{1}{4}$".
Replace Neoprene Expansion Joints. Install Concrete Brackets Under Cantilever Portion of Pier Caps at Piers 1 Thru 11.
Structural Repair of Concrete and Epoxy Crack Injection at Abutments and Piers.

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	Will	72	48
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-	

SHEET NO. 1
9 SHEETS

Contract #62930 * 88(B&B-1)BR



PLAN & ELEVATION

DESIGN SPECIFICATIONS

2002 AASHTO

DESIGN STRESSES

FIELD UNITS

f'_c = 3,500 psi
f_y = 60,000 psi (reinforcement)

LOCATION SKETCH

STATE OF ILLINOIS
LICENSED STRUCTURAL ENGINEER
MARY COOMBE BLOXDORF
4859
SPRINGFIELD

ILLINOIS STRUCTURAL NO. 4859
EXPIRES: 11/30/10
DATE: 6-25-09

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE: PLAN AND ELEVATION

PROJECT:
I-55 SB OVER KANKAKEE RIVER
FAI ROUTE 55, SECTION 88(B&B-1)BR
WILL COUNTY
SN 099-0002

PROJECT NO. 03095-16
SCALE
DATE 6/25/09
DRAWN BY CFC
CHECKED BY MCB/KPS
DRAWING NO. 1
OF 9 SHTS

COOMBE-BLOXDORF P.C.
Engineers / Land Surveyors
Springfield, Illinois
Design Firm License No. 184-002703

PLOT DATE = 12/01/2009
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ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	Will	72	49
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-	

SHEET NO. 2
9 SHEETS

Contract #62930 * 88(B&B-1)BR

INDEX OF SHEETS

GENERAL NOTES

PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF THE WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.

THE CONTRACTOR SHALL ENSURE THAT NO CONCRETE WILL BE ALLOWED TO DROP INTO THE RIVER. CONCRETE SHALL BE CAUGHT ON A FLOATING PLATFORM OR OTHER MEANS APPROVED BY THE ENGINEER. COST INCLUDED WITH DECK SLAB REPAIR (FULL DEPTH, TYPE II).

REINFORCEMENT BARS SHALL CONFORM TO THE REQUIREMENTS OF ASTM A 760 GR 60. SEE SPECIAL PROVISIONS.

REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED.

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Bridge Deck Latex Concrete Overlay 2¼"	Sq Yd	4449		4449
Bridge Deck Hydro Scarification 1"	Sq Yd	4449		4449
Neoprene Expansion Joint 2½"	Foot	97		97
Neoprene Expansion Joint 4"	Foot	97		97
Epoxy Crack Injection	Foot		275	275
Structural Repair of Concrete (Depth equal to or less than 5")	Sq Ft		90	90
Concrete Removal	Cu Yd		8.0	8.0
Concrete Structures	Cu Yd		29.5	29.5
Reinforcement Bars, Epoxy Coated	Pound		3,090	3,090
Polymer Concrete	Cu Ft	0.5		0.5
Deck Slab Repair (Full Depth, Type I)	Sq Yd	1		1
Bridge Deck Grooving	Sq Yd	4219		4219
Protective Coat	Sq Yd	4449		4449

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ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE
GENERAL NOTES AND TOTAL BILL OF MATERIAL

PROJECT
I-55 SB OVER KANKAKEE RIVER
FAI ROUTE 55, SECTION 88(B&B-1)BR
WILL COUNTY
SN 099-0002

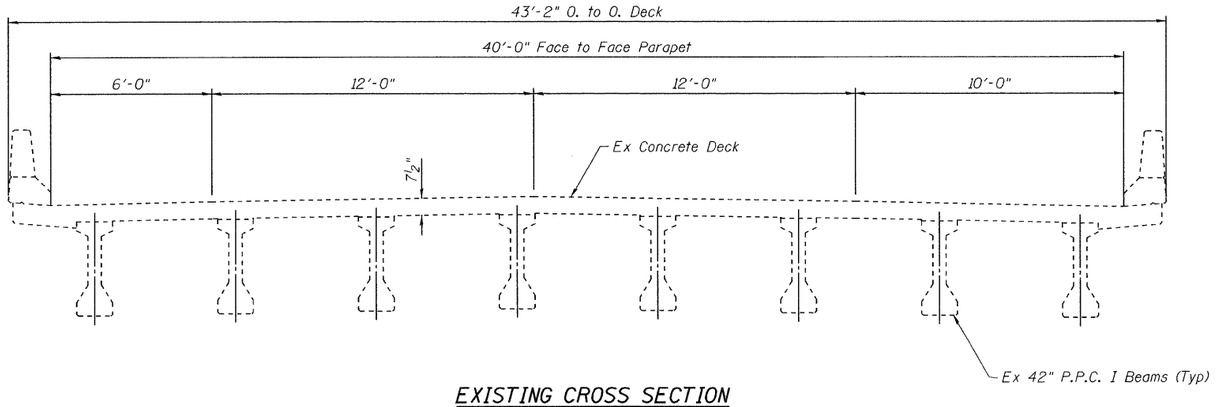
PROJECT NO. 03095-16
SCALE
DATE 6/25/09
DRAWN BY TFG/CFC
CHECKED BY MCB/KPS
DRAWING NO. 2
OF 9 SHTS

COOMBE-BLOXDORF P.C.
Engineers / Land Surveyors
Springfield, Illinois
Design Firm License No. 184-002703

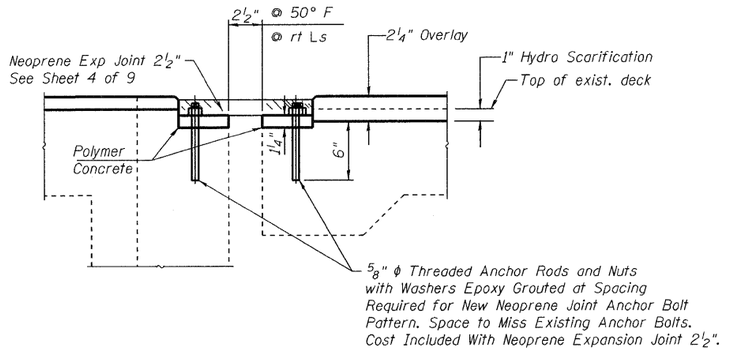
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	Will	72	50
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-	

SHEET NO. 3
9 SHEETS

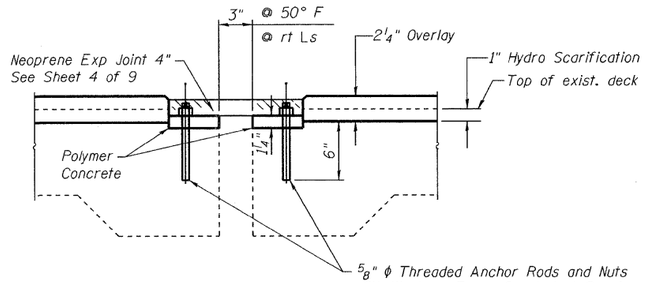
Contract #62930 * 88(B&B-1)BR



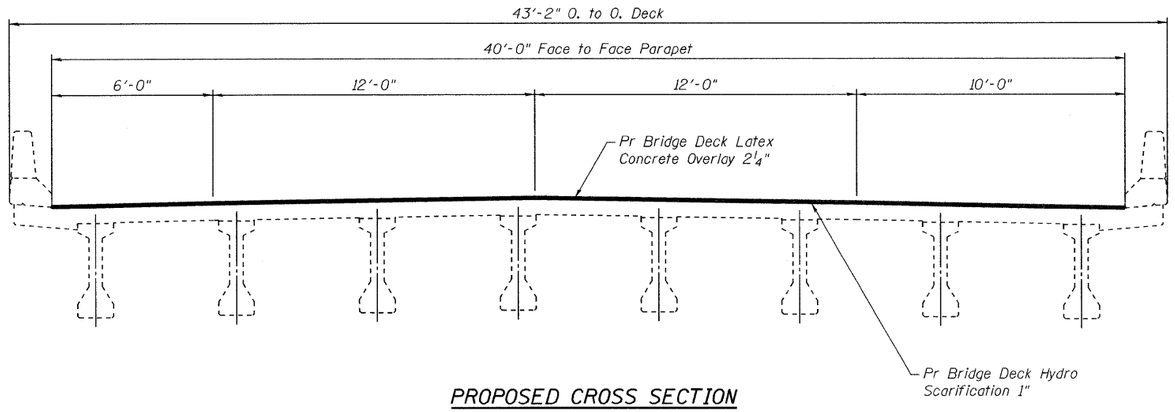
EXISTING CROSS SECTION
(Looking South)



SECTION AT ABUTMENTS
(Showing New Neoprene Joint)



SECTION AT PIERS 4 & 8
(Showing New Neoprene Joint)



PROPOSED CROSS SECTION
(Looking South)

NOTES:

Limits of Concrete Bridge Deck Latex Concrete Overlay 2¼" and Bridge Deck Hydro Scarification 1" Extend from End of Deck at S Abutment to End of Deck at N Abutment 333'-9⅞" Units 1 & 3, ±333'-3" Unit 2.

Remove Existing Neoprene Joints. Cut Existing Anchor Bolts Flush With Top of Concrete, Grind Smooth and Seal With Epoxy. Cost Included With Neoprene Joints 2½" and 4" as Applicable.

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Bridge Deck Latex Concrete Overlay 2¼"	Sq Yd	4449
Bridge Deck Hydro Scarification 1"	Sq Yd	4449
Neoprene Expansion Joint 2½"	Foot	97
Neoprene Expansion Joint 4"	Foot	97

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE: CROSS SECTION

PROJECT: I-55 SB OVER KANKAKEE RIVER
FAI ROUTE 55, SECTION 88(B&B-1)BR
WILL COUNTY
SN 099-0002

PROJECT NO. 03095-16
SCALE
DATE 6/25/09
DRAWN BY CFC
CHECKED BY MCB/KPS
DRAWING NO. 3 OF 9 SHTS

COOMBE-BLOXDORF P.C.
Engineers / Land Surveyors
Springfield, Illinois
Design Firm License No. 184-002703

PLOT DATE = 6/25/2009
FILE NAME = ...\sn-099-0002-sheet-3-br1dgx-xs.dgn
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USER NAME = JML

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	Will	72	51
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-	

SHEET NO. 4
9 SHEETS

Contract #62930 * 88(B&B-1)BR

Joint Size	"C" at 50°F	"D" at 50°F
2"	2"	1½" Min.
2½"	2½"	1¾" Min.
4"	3"	2½" Min.

INSTALLATION NOTES

(1) Install continuous seal in roadway, parapet, curb, and sidewalk.

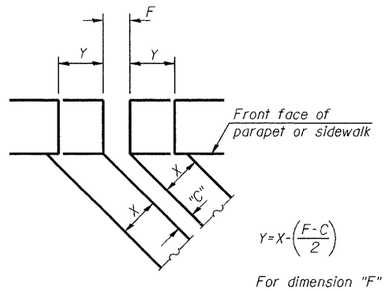
(2) Install anchor blocks as indicated.

Note A:
Maximum spacing of anchor bolts shall be 12" centers.

SKEW LIMITATIONS

The details of the anchor blocks and the elastomeric membrane in the parapet, as shown, are for up to 50° skews.

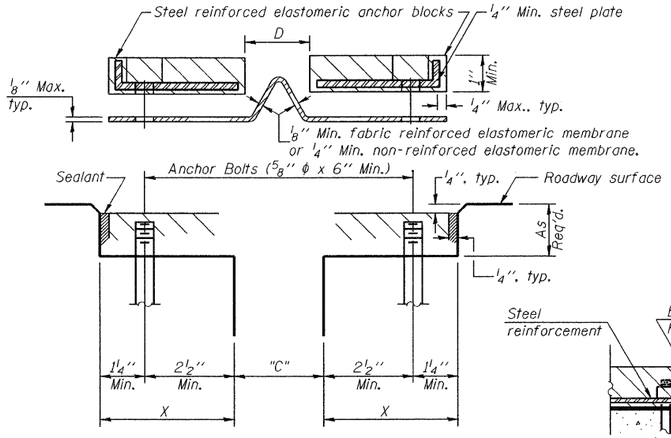
For skews greater than 50°, the anchor blocks and the elastomeric membrane, installed according to dimension "D", might require modifications to insure a minimum clearance of 1½" from centerline of anchor studs to edge of parapet opening. The anchor blocks and the elastomeric membrane shall also be installed to the top of the parapet with the anchor studs spaced at ±12" cts.



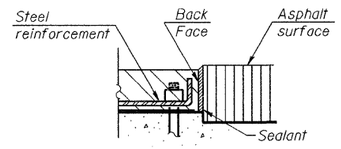
$Y = X - \left(\frac{F-C}{2}\right)$

For dimension "F" see sheet 3 of 9

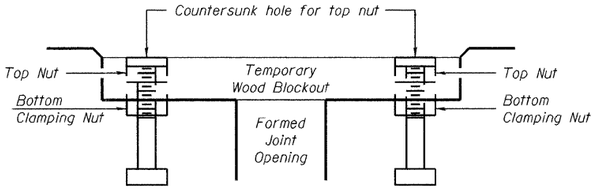
FORMING BLOCKOUT SKETCH



CROSS SECTION



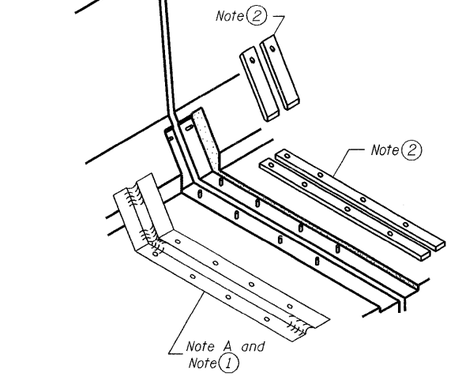
ANCHOR BLOCK WITH ASPHALT SURFACE



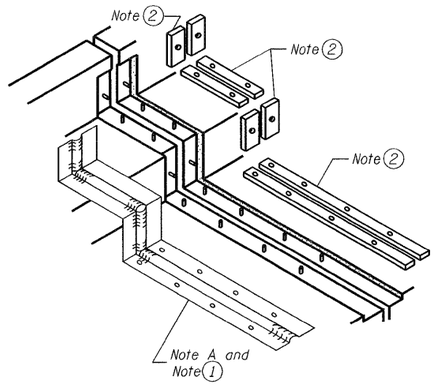
Note:
Stud needs to be threaded lower to allow for use of clamping nut.

Anchor studs should be stainless

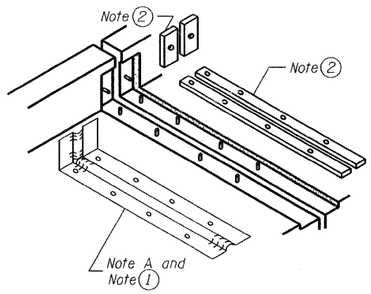
RECOMMENDED BLOCKOUT DETAIL



AT PARAPET



AT SIDEWALK OR MEDIAN



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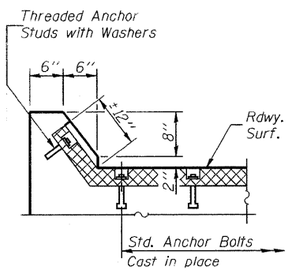

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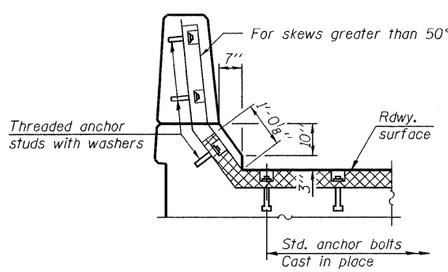
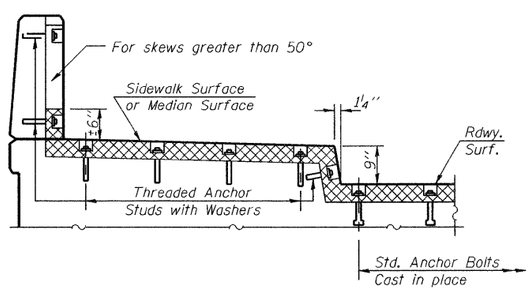


AT PARAPET



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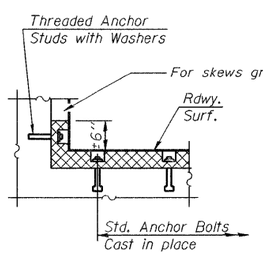

AT WALL

TYPICAL END TREATMENTS

GENERAL NOTES

Continuous Seal Neoprene Expansion Joint shall consist of molded anchor blocks of elastomer and steel, field assembled over continuous lengths of elastomeric membrane.

The elastomeric membrane shall be premolded with a single or a double upward convolution that will have a "memory" to return to its molded position upon joint closure.

The convolution length shall be such that the extended length will not be greater than the manufactured length when the joint is fully expanded in its design range and will not protrude above the anchor blocks when the joint is fully compressed.

Joint openings shall be adjusted according to Article 503.10(c) of the Standard Specifications when the deck is poured at an ambient temperature other than 50° F.

The parapet and roadway membrane shall be made continuous by an approved vulcanizing process. Lapping will not be permitted.

BILL OF MATERIAL

Item	Unit	Total
Neoprene Expansion Joint 2½"	foot	97
Neoprene Expansion Joint 4"	foot	97

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE
CONTINUOUS SEAL TYPE NEOPRENE EXPANSION JOINTS

PROJECT
I-55 SB OVER KANKAKEE RIVER
FAI ROUTE 55, SECTION 88(B&B-1)BR
WILL COUNTY
SN 099-0002

PROJECT NO. 03095-16
SCALE
DATE 06/25/09
DRAWN BY CFC
CHECKED BY MCB/KPS
DRAWING NO. 4
OF 9 SHTS

COOMBE-BLOXDORF P.C.
Engineers / Land Surveyors
Springfield, Illinois
Design Firm License No. 184-002703

EJ-CS 10-22-04

PLOT DATE = 6/25/2009
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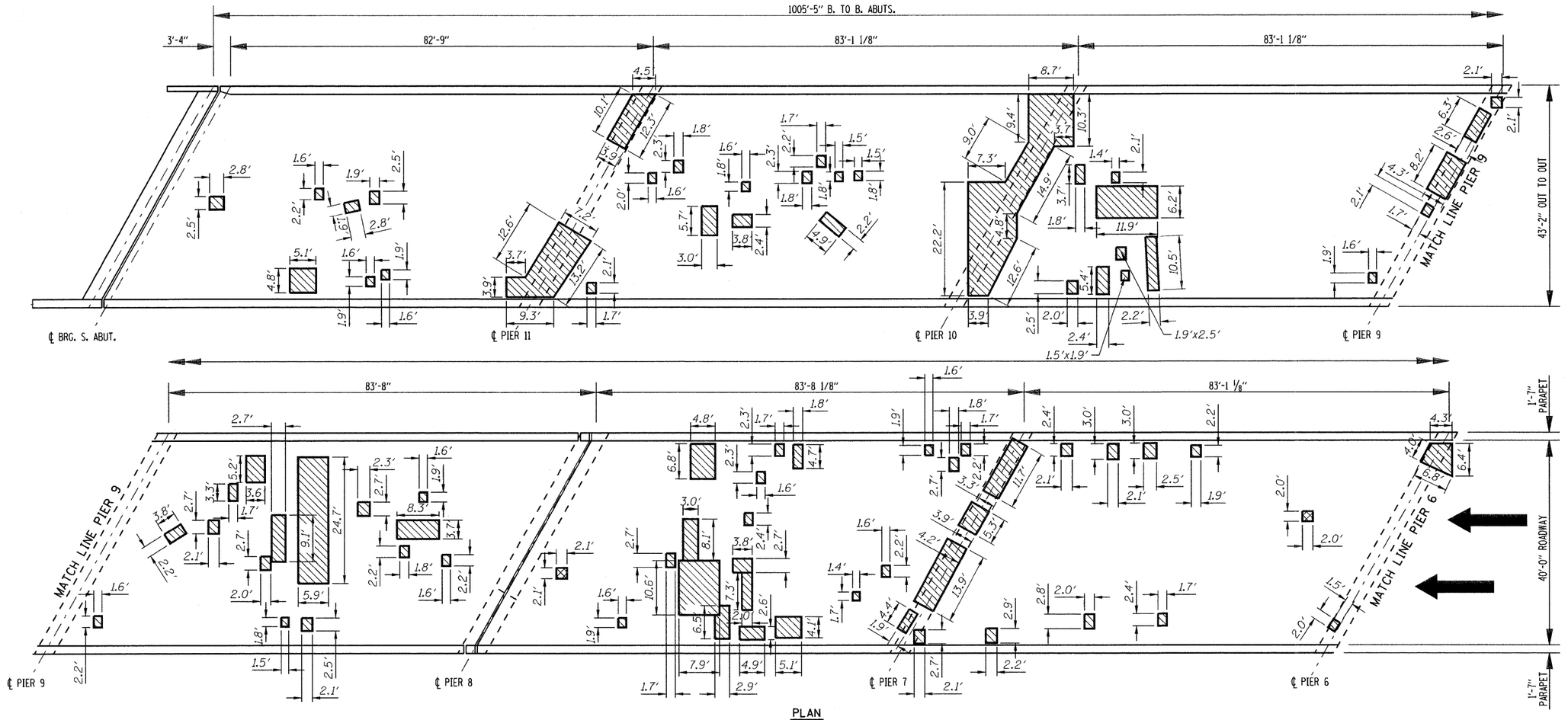
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	Will	72	52
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-		

SHEET NO. 5
9 SHEETS

Contract #62930 * 88(B&B-1)BR



PLAN

PROPOSED DECK REHABILITATION AREAS

LEGEND

- DECK SLAB REPAIR PARTIAL
- DECK SLAB REPAIR (FULL DEPTH, TYPE I)

NOTES:

Deck Drains that are Located in Areas of Full Depth Repair Shall be Replaced as approved by the Engineer. Cost included with Deck Slab Repair (Full Depth, Type I) or Deck Slab Repair (Full Depth, Type II) as Applicable.

The Areas of Deck Slab Repair are Based on Results of an Infrared Deck Survey Conducted on 04/16/09.

Areas of Deck Slab Repair (Partial) shown for information only to assist the Contractor in bidding.

Areas of repair that are located within 10 feet of the parapet face should be repaired in maximum lengths of 10' at a time to ensure stability at the parapet.

BILL OF MATERIAL

	UNIT	TOTAL
DECK SLAB REPAIR (FULL DEPTH, TYPE I)	Sq Yd	1

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE: DECK SLAB REPAIR

PROJECT: I-55 SB OVER KANKAKEE RIVER
FAI ROUTE 55, SECTION 88(B&B-1)BR
WILL COUNTY
SN 099-0002

PROJECT NO. 03095-16
SCALE
DATE 6/25/09
DRAWN BY CFC
CHECKED BY MCB/KPS
DRAWING NO. 5 OF 9 SHTS

COOMBE-BLOXDORF P.C.
Engineers / Land Surveyors
Springfield, Illinois
Design Firm License No. 184-002703

PLOT DATE = 6/25/2009
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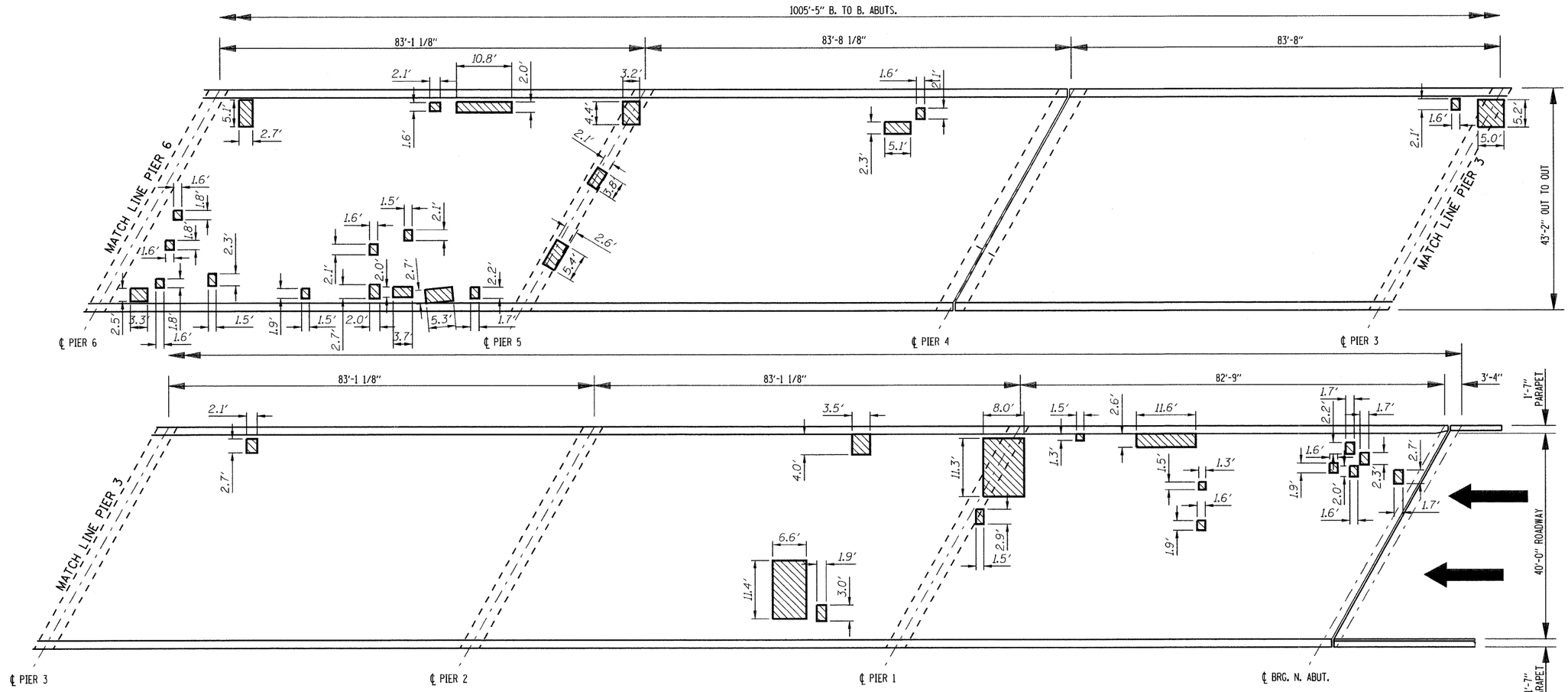
USER NAME = JML

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	Will	72	53
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-	

SHEET NO. 6
9 SHEETS

Contract #62930 * 88(B&B-1)BR



PLAN

PROPOSED DECK REHABILITATION AREAS

0 10' 20' 40'

LEGEND

DECK SLAB REPAIR PARTIAL

NOTES:

Deck Drains that are Located in Areas of Full Depth Repair Shall be Replaced as approved by the Engineer. Cost Included with Deck Slab Repair (Full Depth,Type I) or Deck Slab Repair (Full Depth,Type II) as Applicable.

The Areas of Deck Slab Repair are Based on Results of an Infrared Deck Survey Conducted on 04/16/09.

Areas of Deck Slab Repair (Partial) shown for information only to assist the Contractor in bidding.

Areas of repair that are located within 10 feet of the parapet face should be repaired in maximum lengths of 10' at a time to ensure stability at the parapet.

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE: DECK SLAB REPAIR

PROJECT: I-55 SB OVER KANKAKEE RIVER
FAI ROUTE 55, SECTION 88(B&B-1)BR
WILL COUNTY
SN 099-0002

PROJECT NO. 03095-16
SCALE
DATE 6/25/09
DRAWN BY CFC
CHECKED BY MCB/KPS
DRAWING NO. 6 OF 9 SHTS

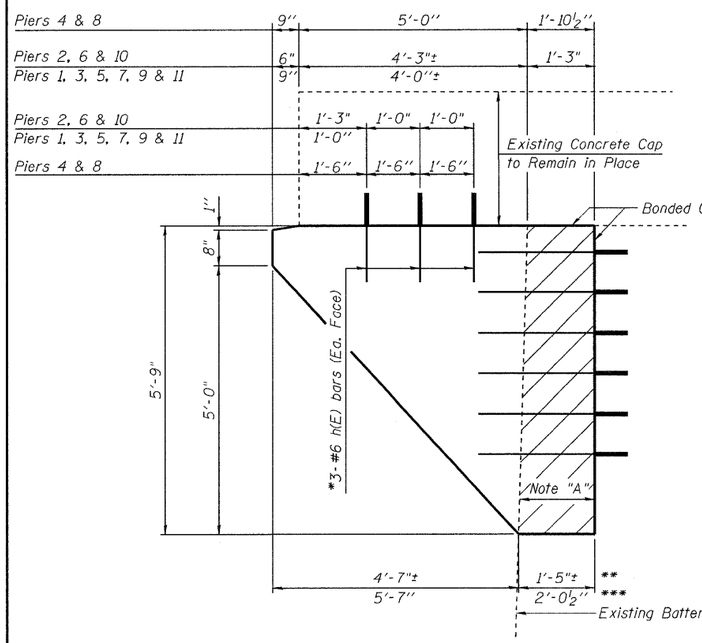
COOMBE-BLOXDORF P.C.
Engineers / Land Surveyors
Springfield, Illinois
Design Firm License No. 184-002703

PLOT DATE = 6/25/2009
FILE NAME = ...\sn-099-0002-sheet-6-deck-slab-repair.dgn
PLOT SCALE = 21d1.5294 '/' IN.
USER NAME = JML

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	Will	72	54
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-	

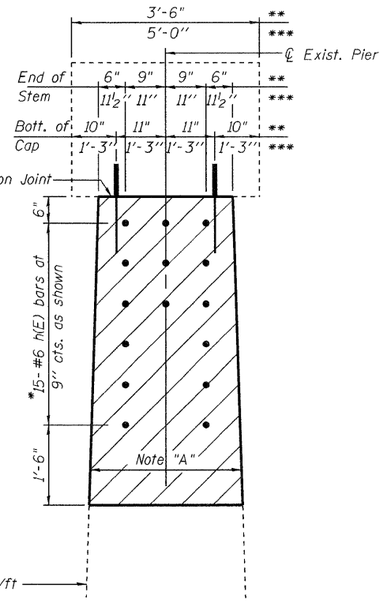
SHEET NO. 7
9 SHEETS

Contract #62930 * 88(B&B-1)BR



ELEVATION

Showing Anchor Bars and Dimensions
(Looking North)



END VIEW

Showing Anchor Bars and Dimensions

**Piers 1, 2, 3, 5, 6, 7, 9, 10 & 11
***Piers 4 & 8

*Epoxy grout h(E) bars into 9" drilled holes. See Section 584 of the Standard Specifications.

Note "A":
Hatched area indicates concrete removal of a portion of the west nose of the existing pier stem. Existing reinforcement in the nose shall be cleaned, straightened and incorporated into the new construction. Cost Included With Concrete Removal.

NOTE:

The Contractor shall take the necessary means to ensure that the concrete between the bottom of the existing cap and the top of the new cantilever support is properly consolidated. The method of consolidation shall be approved by the Engineer.

The Contractor shall take precautions that no live loads be allowed on the outside beam during the concrete removal from the west pier nose and until the proposed concrete has cured.

NOTES:

Existing reinforcement extending into the new construction shall be cleaned and incorporated into the new construction. Cost included with Concrete Removal.

Any existing reinforcement bars which are intended to be incorporated into the new construction that are damaged during concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system. Cost included with Concrete Removal.

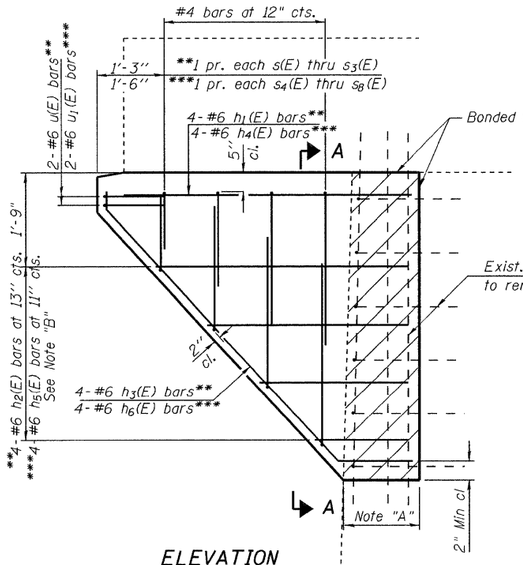
All exposed edges shall have 2" chamfers.

BILL OF MATERIAL
ONE SUPPORT BRACKET
PIERS 4 & 8

Bar	No.	Size	Length	Shape
h(E)	21	#6	3'-0"	
h_4(E)	4	#6	7'-3"	
h_5(E)	4	#6	9'-0"	
h_6(E)	4	#6	9'-6"	
s_4(E)	2	#4	6'-1"	
s_5(E)	2	#4	7'-1"	
s_6(E)	2	#4	7'-11"	
s_7(E)	2	#4	8'-11"	
s_8(E)	2	#4	9'-9"	
u_1(E)	2	#6	7'-4"	
Concrete Removal			Cu. Yd.	1.3
Concrete Structures			Cu. Yd.	4.4
Reinforcement Bars. Epoxy Coated			Lb.	330

BILL OF MATERIAL
ONE SUPPORT BRACKET
PIERS 1, 2, 3, 5, 6, 7, 9, 10 & 11

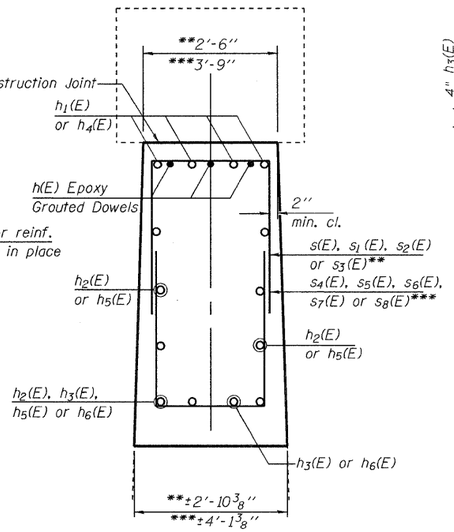
Bar	No.	Size	Length	Shape
h(E)	21	#6	3'-0"	
h_1(E)	4	#6	5'-8"	
h_2(E)	4	#6	6'-10"	
h_3(E)	4	#6	8'-2"	
s(E)	2	#4	4'-10"	
s_1(E)	2	#4	5'-10"	
s_2(E)	2	#4	7'-0"	
s_3(E)	2	#4	8'-0"	
u(E)	2	#6	6'-1"	
Concrete Removal			Cu. Yd.	0.6
Concrete Structures			Cu. Yd.	2.3
Reinforcement Bars. Epoxy Coated			Lb.	270



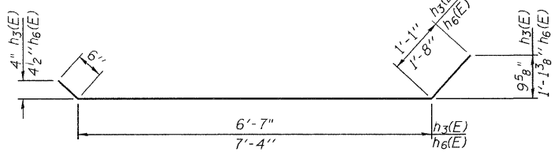
ELEVATION

Showing Reinforcement
(Looking North)

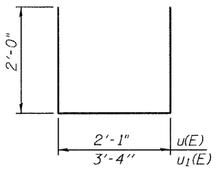
Note "B":
Order bars full length. Cut to fit and use remainder in opposite face.



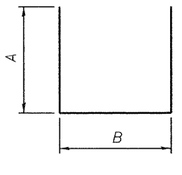
SECTION A-A



BAR h_3(E) or h_6(E)



BAR u(E) & u_1(E)



BARS s(E), s_1(E), s_2(E), s_3(E), s_4(E), s_5(E), s_6(E) s_7(E) & s_8(E)

A & B DIMENSIONS

BAR	A	B
s(E)	1'-4"	2'-2"
s_1(E)	1'-10"	2'-2"
s_2(E)	2'-5"	2'-2"
s_3(E)	2'-11"	2'-2"
s_4(E)	1'-4"	3'-5"
s_5(E)	1'-10"	3'-5"
s_6(E)	2'-3"	3'-5"
s_7(E)	2'-9"	3'-5"
s_8(E)	3'-2"	3'-5"

MIN BAR LAP

#4 bars 1'-4"

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE: CONCRETE BRACKETS AT PIERS

PROJECT: I-55 SB OVER KANKAKEE RIVER
FAI ROUTE 55, SECTION 88(B&B-1)BR
WILL COUNTY
SN 099-0002

PROJECT NO. 03095-16
SCALE
DATE 6/25/09
DRAWN BY TFG
CHECKED BY KPS/MCB
DRAWING NO. 7

COOMBE-BLOXDORF P.C.
Engineers / Land Surveyors
Springfield, Illinois
Design Firm License No. 184-002703

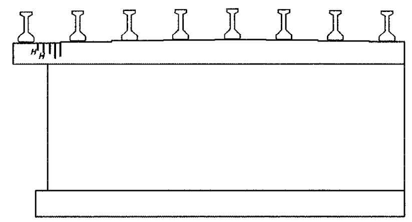
OF 9 SHTS

PLOT DATE = 6/25/2009
FILE NAME = ...ilgsn-099-0002-sheet-1-concrete-brackets-at-piers.sdgn
PLOT SCALE = 28.3412 '/ IN.
USER NAME = JML

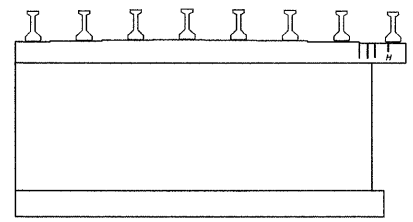
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	Will	72	55
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-		

SHEET NO. 8
9 SHEETS

Contract #62930
* 88(B&B-1)BR



PIER 1
ELEVATION, SOUTH FACE
(LOOKING NORTH)



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ELEVATION, NORTH FACE
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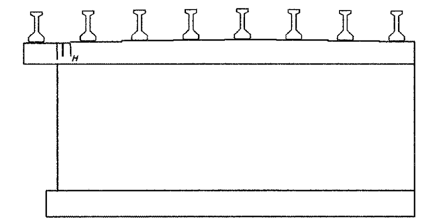

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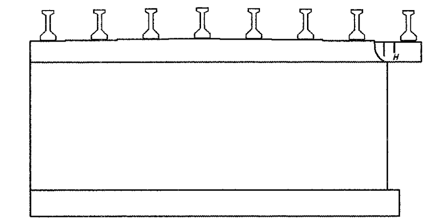

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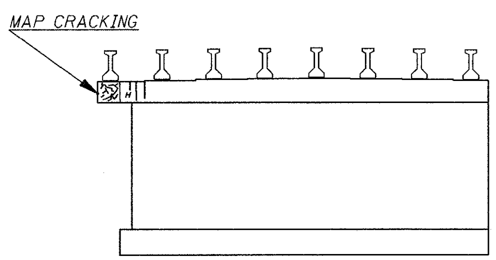

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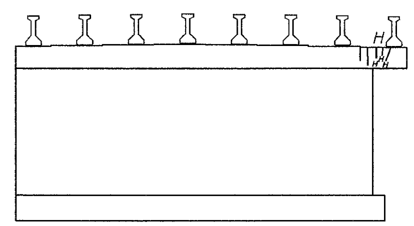

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ELEVATION, NORTH FACE
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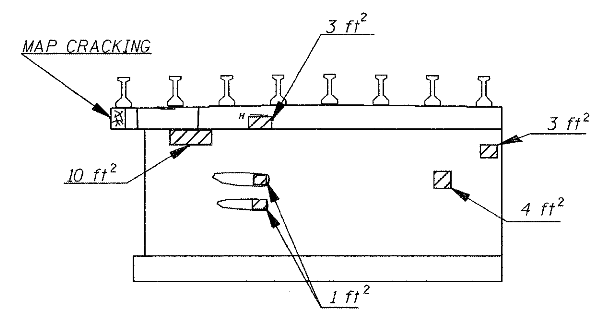

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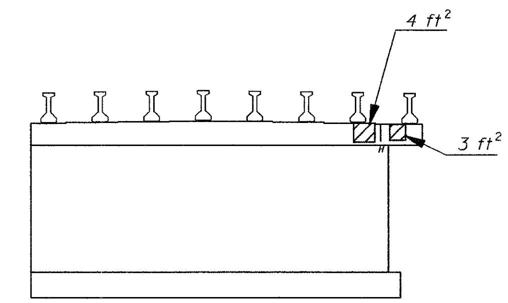
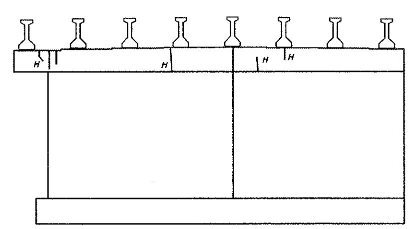


PIER 4
ELEVATION, NORTH FACE
(LOOKING SOUTH)



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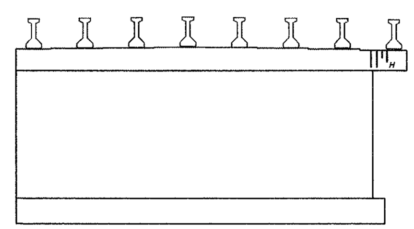

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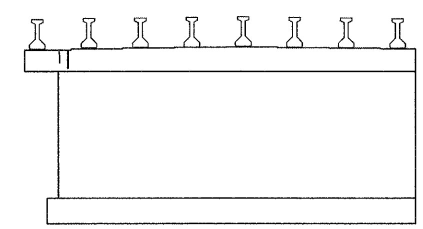

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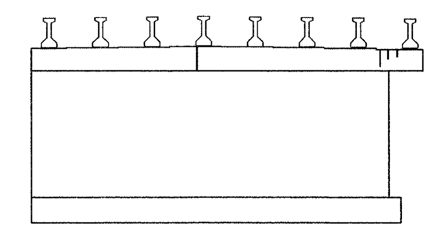


PIER 6
ELEVATION, NORTH FACE
(LOOKING SOUTH)

LEGEND

Epoxy Crack Injection

Structural Repair of Concrete
(Depth Equal to or Less Than 5")

Note:
Quantities Included in Bill of Material on Sheet 9 of 9

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE
PIER REPAIRS

PROJECT
I-55 SB OVER KANKAKEE RIVER
FAI ROUTE 55, SECTION 88(B&B-1)BR
WILL COUNTY
SN 099-0002

PROJECT NO. 03095-16
SCALE
DATE 06/25/09
DRAWN BY CFC
CHECKED BY MCB/KPS
DRAWING NO. 8
OF 9 SHTS

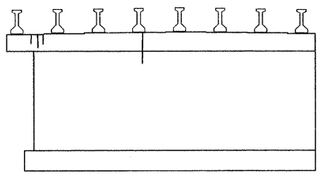
COOMBE-BLOXDORF P.C.
Engineers / Land Surveyors
Springfield, Illinois
Design Firm License No. 184-002703

PLOT DATE = 6/25/2009
FILE NAME = ...Bsn-099-0002-sheet-8-pier-repairs.dgn
PLOT SCALE = 21.11.5294 '‚" / IN.
USER NAME = JML.

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	Will	72	56
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-	

SHEET NO. 9
9 SHEETS

Contract #62930 * 88(B&B-1)BR



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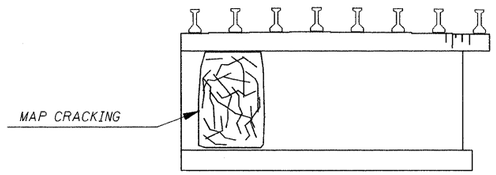

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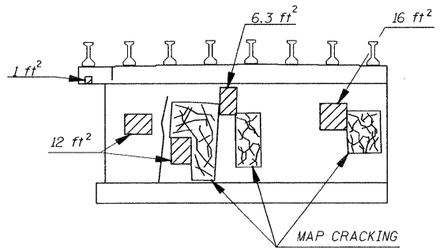

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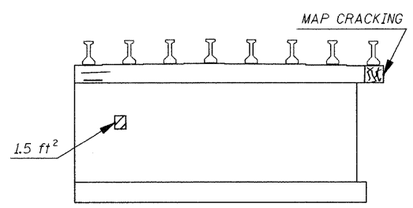

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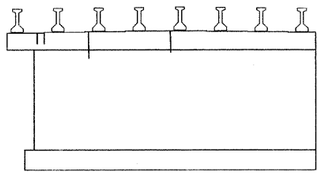

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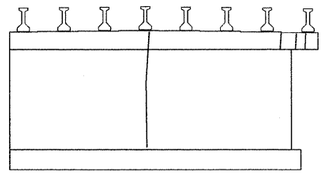

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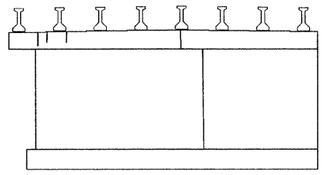

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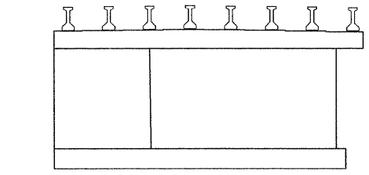
ELEVATION, SOUTH FACE
(LOOKING NORTH)



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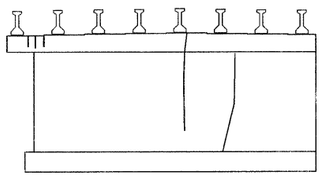
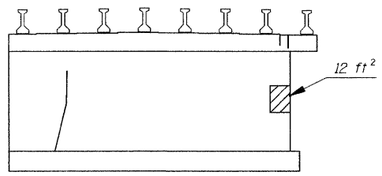
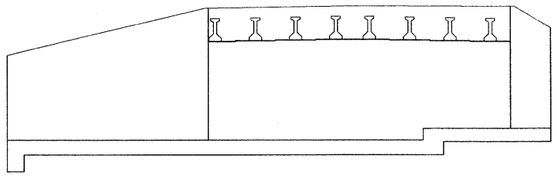


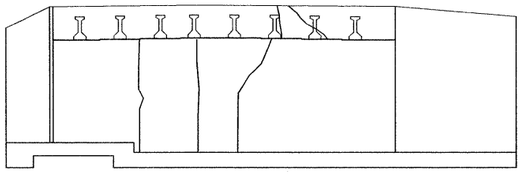
PIER 11
ELEVATION, SOUTH FACE
(LOOKING NORTH)



PIER 11
ELEVATION, NORTH FACE
(LOOKING SOUTH)



NORTH ABUTMENT
(LOOKING NORTH)



SOUTH ABUTMENT
(LOOKING SOUTH)

LEGEND

Epoxy Crack Injection

Structural Repair of Concrete
(Depth Equal to or Less Than 5")

BILL OF MATERIAL

Item	Unit	Total
Epoxy Crack Injection	Foot	275
Structural Repair of Concrete (Depth Equal to or Less Than 5")	Sq Ft	90

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE
ABUTMENT AND PIER REPAIRS

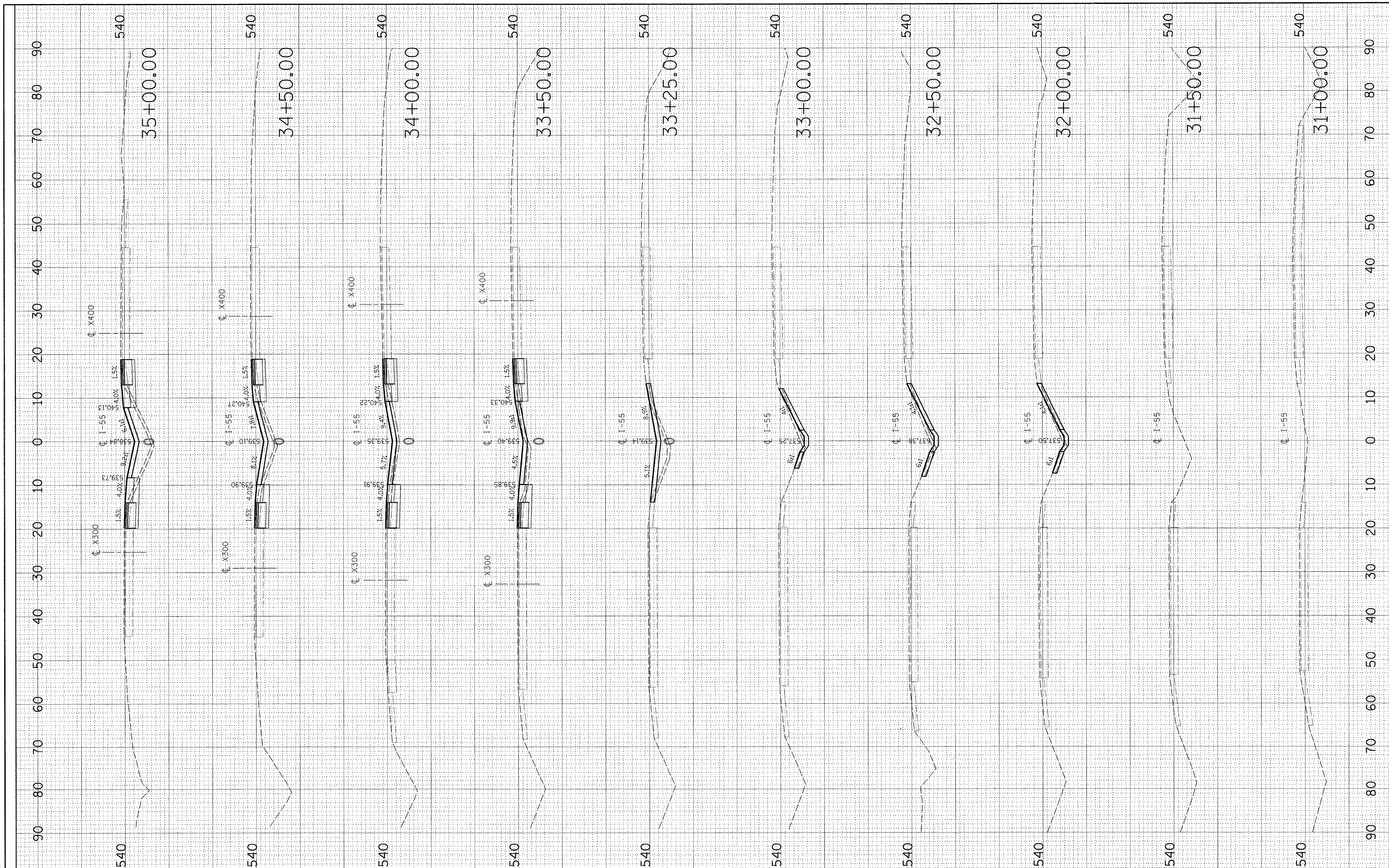
PROJECT
I-55 SB OVER KANKAKEE RIVER
FAI ROUTE 55, SECTION 88(B&B-1)BR
WILL COUNTY
SN 099-0002

PROJECT NO. 03095-16
SCALE
DATE 06/25/09
DRAWN BY CFC
CHECKED BY MCB/KPS
DRAWING NO. 9

COOMBE-BLOXDORF P.C.
Engineers / Land Surveyors
Springfield, Illinois
Design Firm License No. 184-002703

OF 9 SHTS

PLOT DATE = 6/25/2009
FILE NAME = ..\fas-099-0002-sheet-9-abutment-pier-repairs.dgn
PLOT SCALE = 2111.5294 '/' / IN.
USER NAME = JML_



rjngroup
Excellence through Ownership
200 West Front Street
Wheaton, Il 60187

DESIGNED -	REVISED -
DRAWN -	REVISED -
CHECKED -	REVISED -
DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-55 OVER KANKAKEE RIVER
BRIDGE REHABILITATION

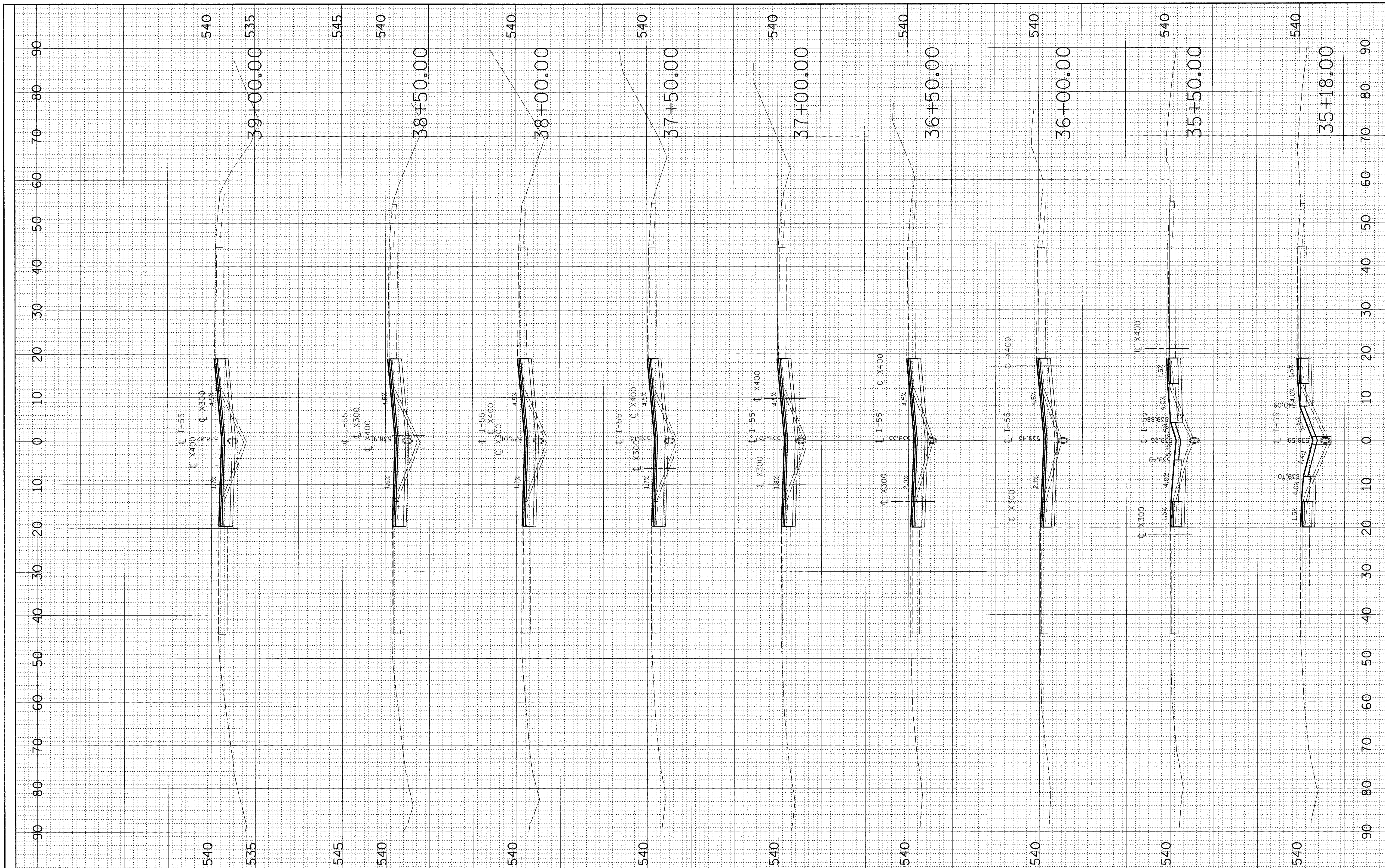
I-55
CROSS SECTIONS

SCALE: ________ SHEET NO. __ OF ___ SHEETS STA. 31+00.00 TO STA. 35+00.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	57

CONTRACT NO. 62930

FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT



rjngroup

Excellence through Ownership

200 West Front Street
Wheaton, Il 60187

DESIGNED - ___	REVISED - ___		
DRAWN - ___	REVISED - ___		
CHECKED - ___	REVISED - ___		
DATE - ________	REVISED - ___		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-55 OVER KANKAKEE RIVER
BRIDGE REHABILITATION

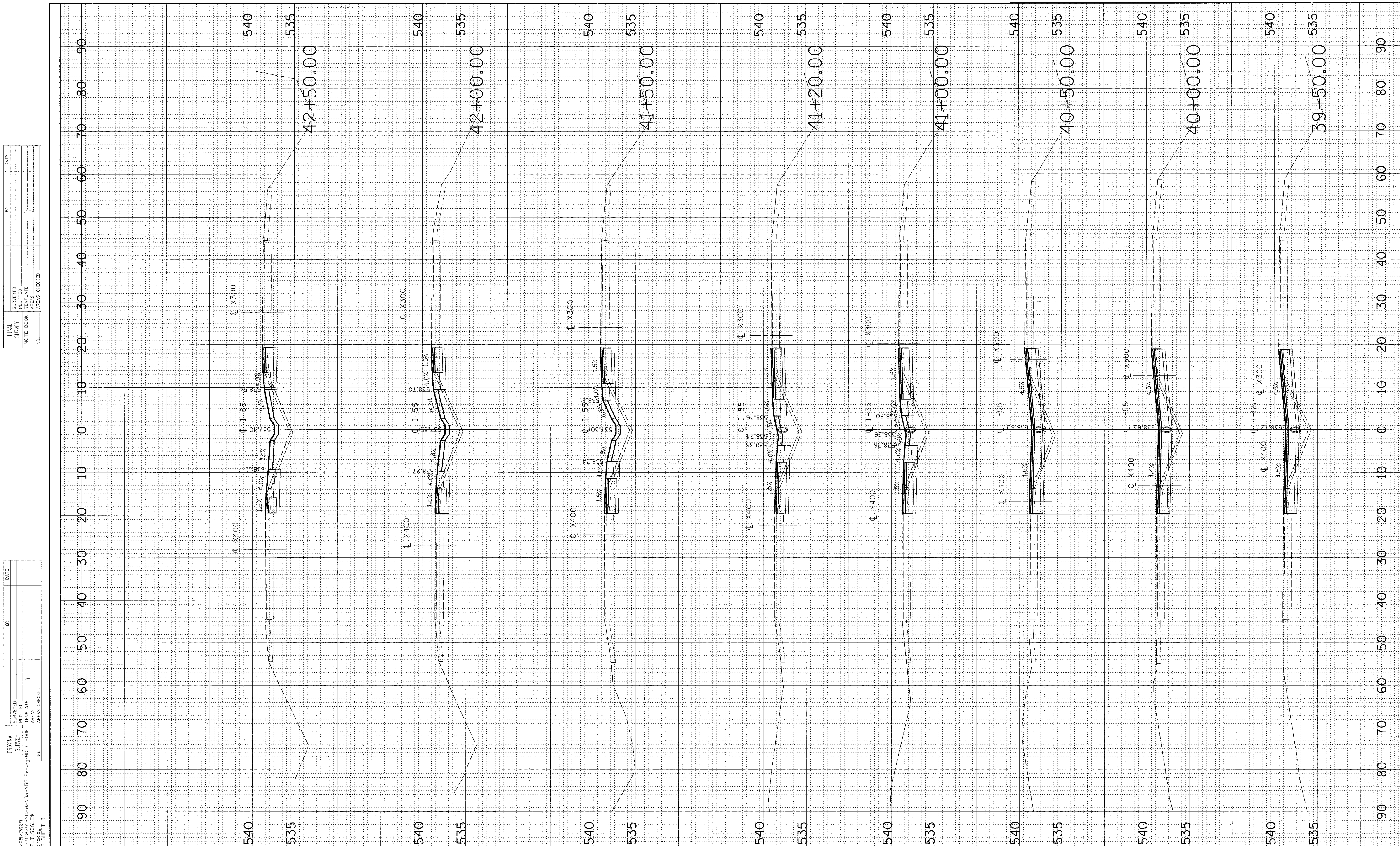
I-55
CROSS SECTIONS

SCALE: ________ SHEET NO. __ OF ___ SHEETS STA. 35+18.00 TO STA. 39+00.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	58

CONTRACT NO. 62930

FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT



rjngroup
Excellence through Ownership

200 West Front Street
Wheaton, Il 60187

DESIGNED -		REVISED -	
DRAWN -		REVISED -	
CHECKED -		REVISED -	
DATE -		REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

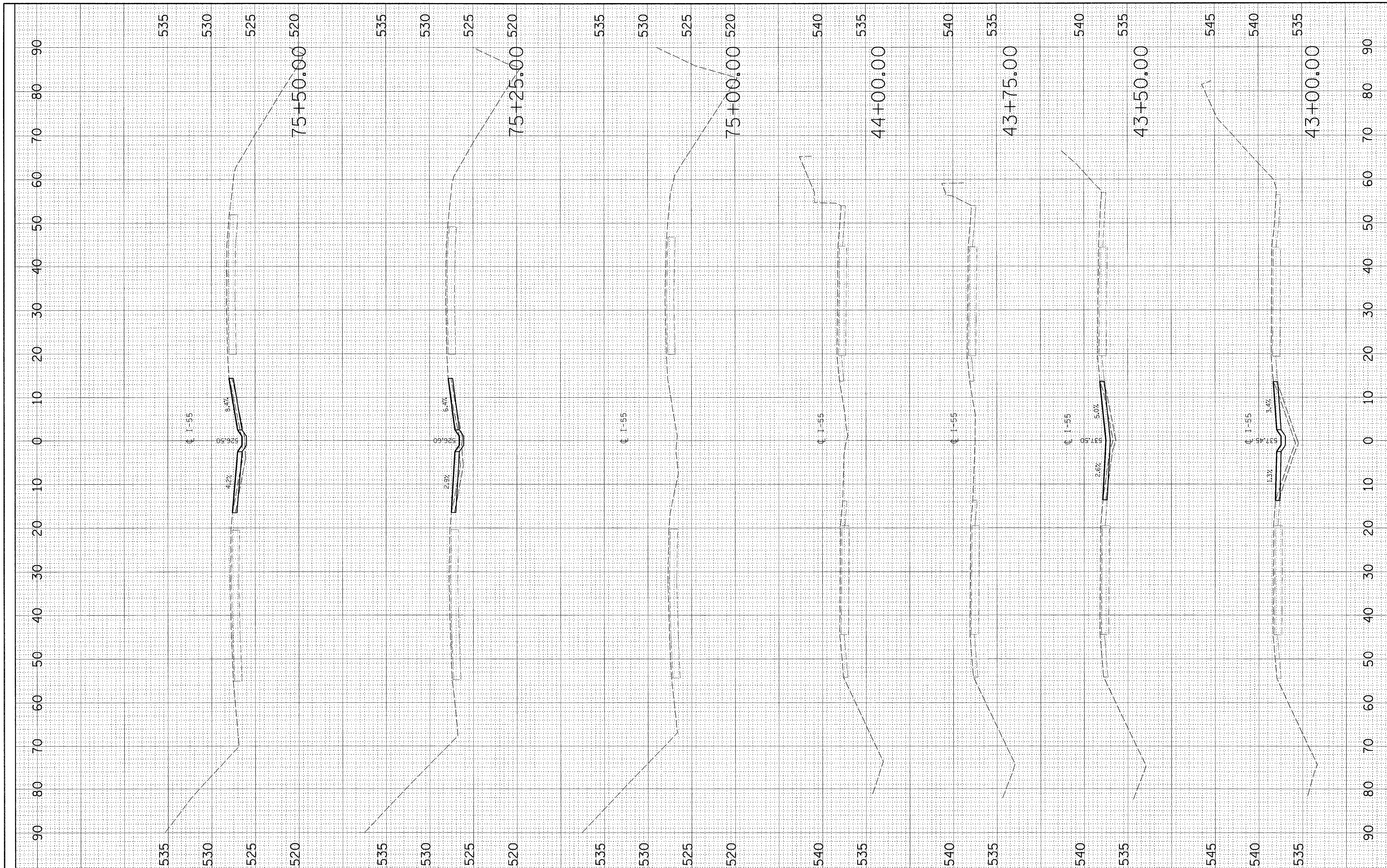
I-55 OVER KANKAKEE RIVER BRIDGE REHABILITATION

I-55 CROSS SECTIONS

SCALE: ________ | SHEET NO. __ OF ___ SHEETS | STA. 39+50.00 TO STA. 42+50.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	59
		CONTRACT NO. 62930		

FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT



ORIGINAL SURVEY | NOTE BOOK | NO. | SURVEYED | PLOTTED | TEMPLATE | AREAS | AREAS CHECKED | BY | DATE

FINAL SURVEY | NOTE BOOK | NO. | SURVEYED | PLOTTED | TEMPLATE | AREAS | AREAS CHECKED | BY | DATE

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PLOT SCALE = $PLT.SCALE$
USER NAME = ...
MODEL = XS-SHEET_4

rjngroup
Excellence through Ownership

200 West Front Street
Wheaton, Il 60187

DESIGNED - ___	REVISED - ___
DRAWN - ___	REVISED - ___
CHECKED - ___	REVISED - ___
DATE - ________	REVISED - ___

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-55 OVER KANKAKEE RIVER
BRIDGE REHABILITATION

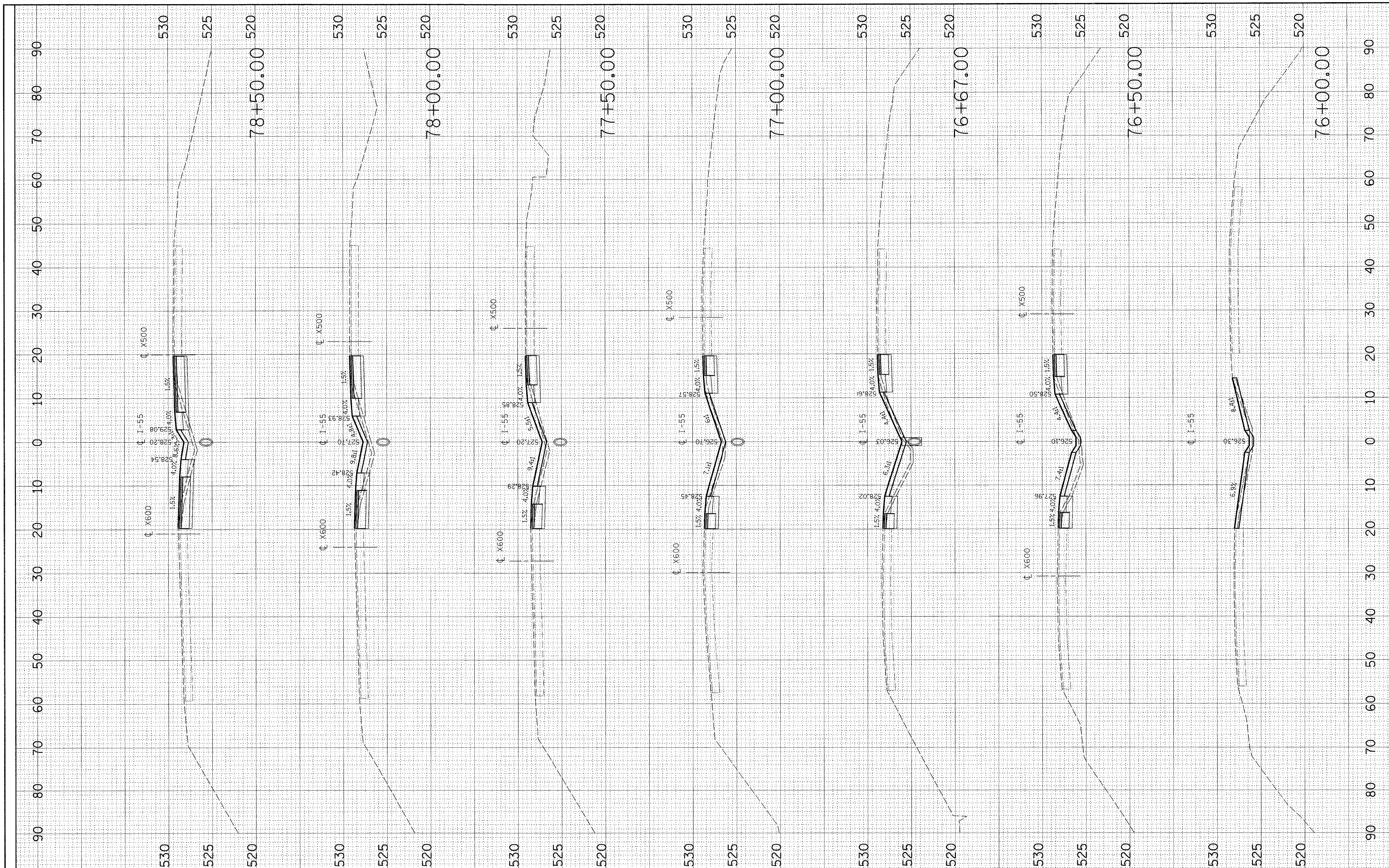
SCALE: ________ SHEET NO. __ OF ___ SHEETS

I-55
CROSS SECTIONS

STA. 43+00.00 TO STA. 75+50.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	60
		CONTRACT NO. 62930		

FED. ROAD DIST. NO. _ | ILLINOIS | FED. AID PROJECT



rjngroup
Excellence through Ownership

200 West Front Street
Wheaton, Il 60187

DESIGNED - ___	REVISED - ___		
DRAWN - ___	REVISED - ___		
CHECKED - ___	REVISED - ___		
DATE - ________	REVISED - ___		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

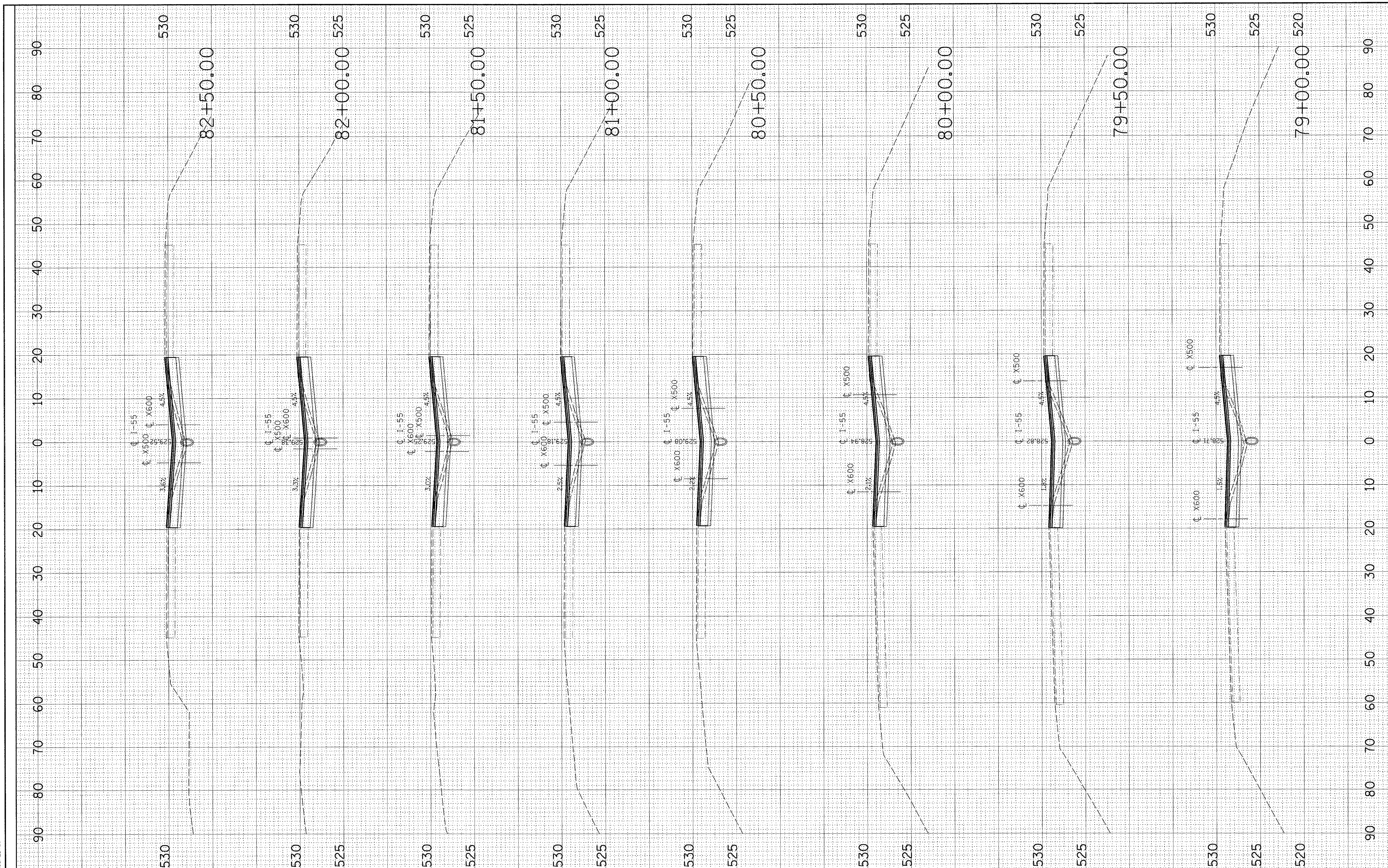
I-55 OVER KANKAKEE RIVER
BRIDGE REHABILITATION

I-55
CROSS SECTIONS

SCALE: ________ | SHEET NO. __ OF ___ SHEETS | STA. 76+00.00 TO STA. 78+50.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	61
		CONTRACT NO. 62930		

FED. ROAD DIST. NO. _ | ILLINOIS | FED. AID PROJECT



rjngroup
Excellence through Ownership
200 West Front Street
Wheaton, Il 60187

DESIGNED -		REVISED -	
DRAWN -		REVISED -	
CHECKED -		REVISED -	
DATE -		REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-55 OVER KANKAKEE RIVER
BRIDGE REHABILITATION

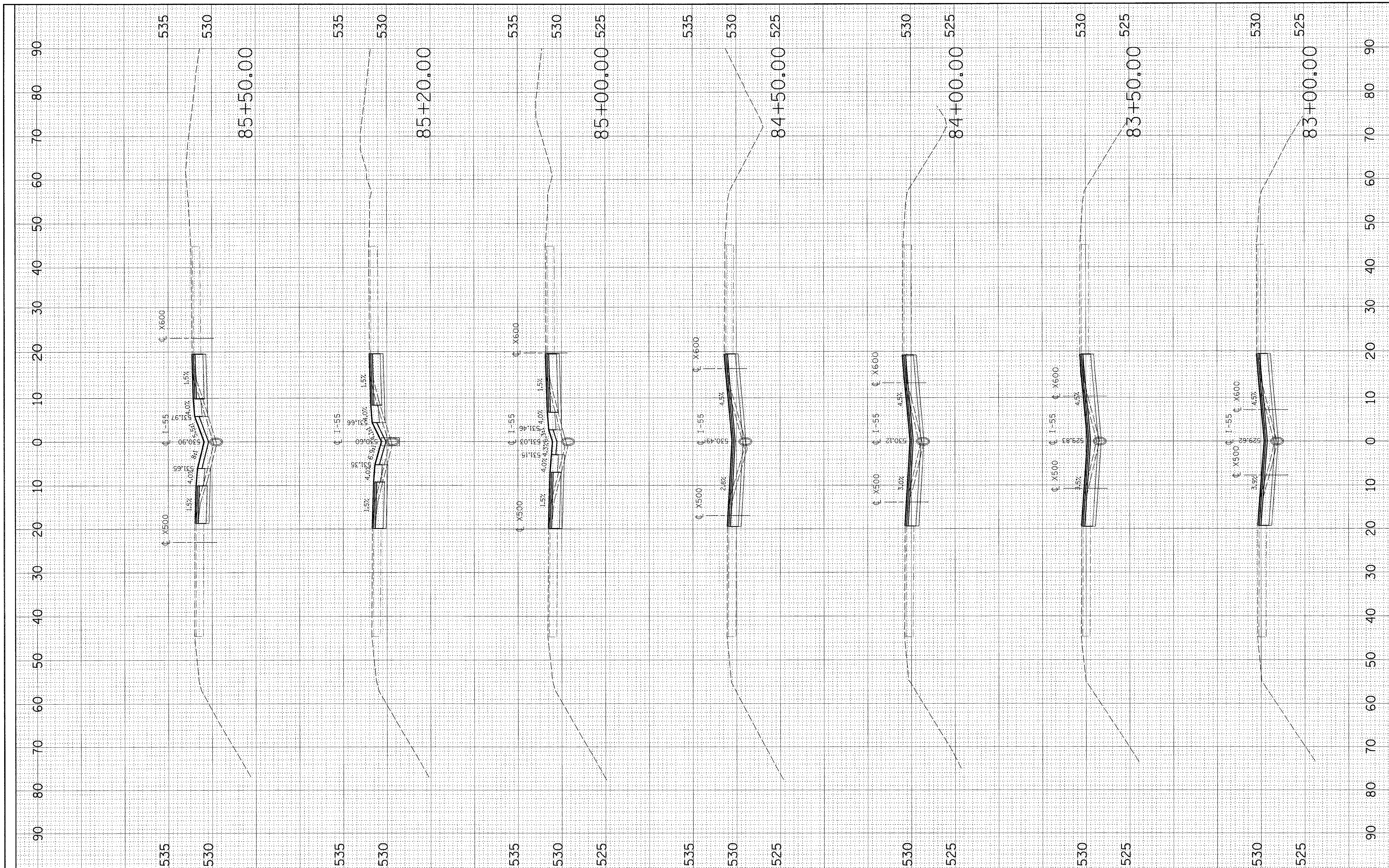
I-55
CROSS SECTIONS

SCALE: ________ SHEET NO. __ OF ___ SHEETS STA. 79+00.00 TO STA. 82+50.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	62

CONTRACT NO. 62930

FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT



rjngroup
Excellence through Ownership
200 West Front Street
Wheaton, Il 60187

DESIGNED - ___	REVISED - ___
DRAWN - ___	REVISED - ___
CHECKED - ___	REVISED - ___
DATE - ________	REVISED - ___

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

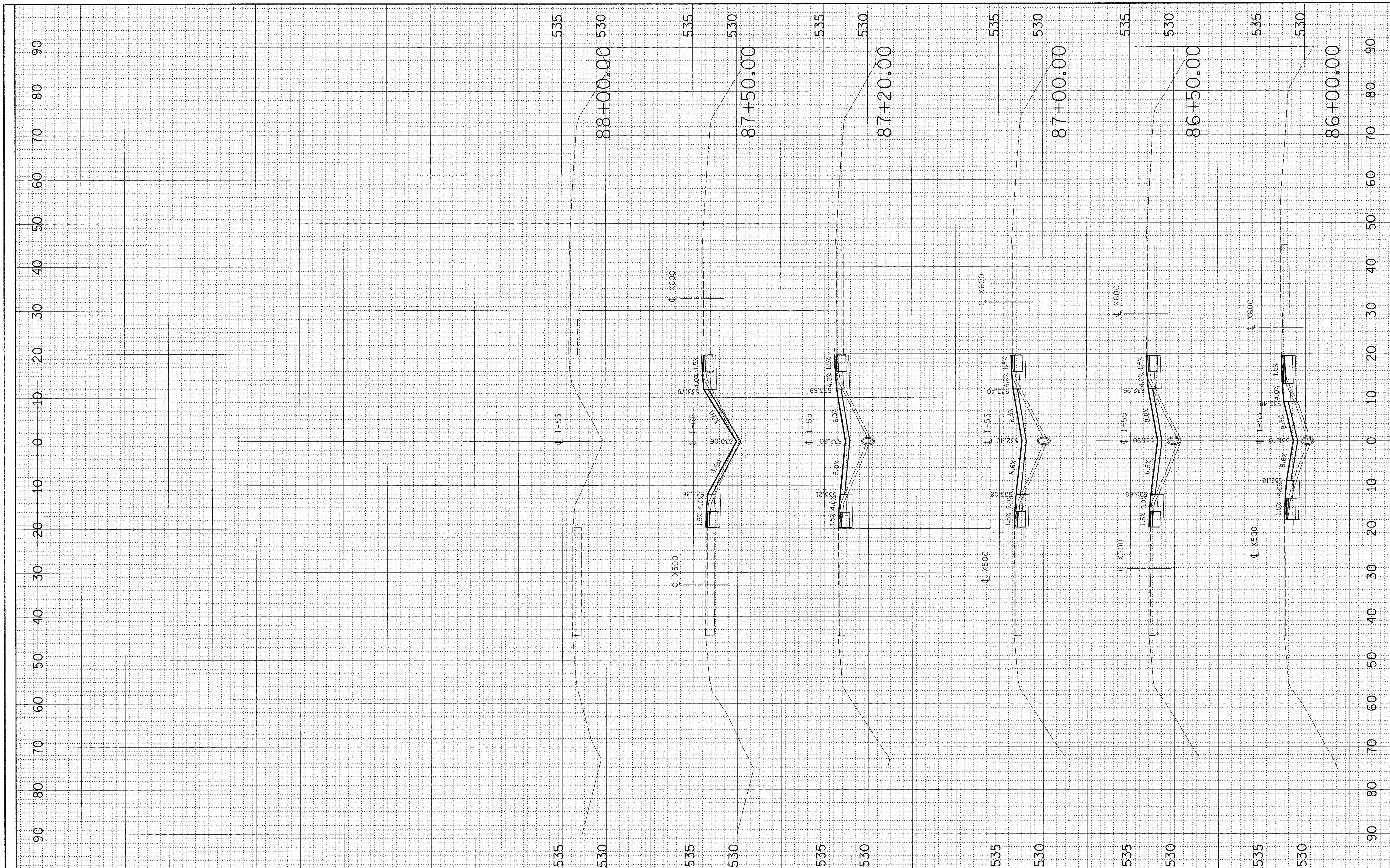
I–55 OVER KANKAKEE RIVER BRIDGE REHABILITATION

I–55 CROSS SECTIONS

SCALE: ________ | SHEET NO. __ OF ___ SHEETS | STA. 83+00.00 TO STA. 85+50.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	63
		CONTRACT NO. 62930		

FED. ROAD DIST. NO. _ | ILLINOIS | FED. AID PROJECT



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PLOT SCALE = $PLT.SCALE$
USER NAME = Jtracey
MODEL = XS.SHEET.8

rjngroup
Excellence through Ownership
200 West Front Street
Wheaton, Il 60187

DESIGNED	- ___	REVISED	- ___
DRAWN	- ___	REVISED	- ___
CHECKED	- ___	REVISED	- ___
DATE	- ________	REVISED	- ___

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-55 OVER KANKAKEE RIVER BRIDGE REHABILITATION

I-55 CROSS SECTIONS

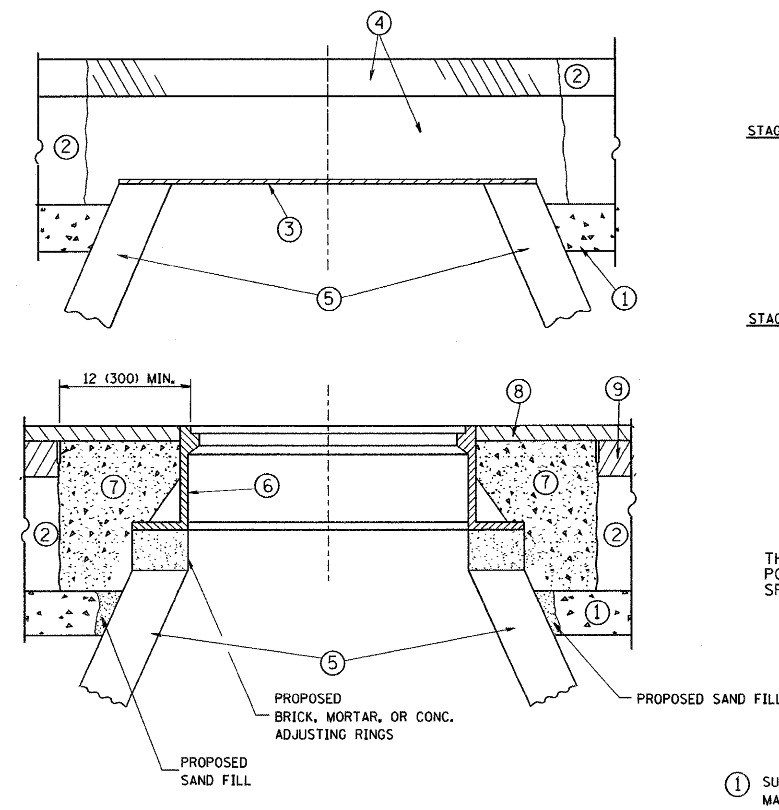
SCALE: ________ SHEET NO. __ OF ___ SHEETS STA. 86+00.00 TO STA. 88+00.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	64
		CONTRACT NO. 62930		

FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT

CONTRACT NO. 62930

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	65
STA.		TO STA.		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.

B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.

C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.

D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.

B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.

C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

① SUB-BASE GRANULAR MATERIAL

② EXISTING PAVEMENT

③ 36 (900) DIAMETER METAL PLATE

④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX

⑤ EXISTING STRUCTURE

⑥ FRAME AND LID (SEE NOTES)

⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE

⑧ PROPOSED HMA SURFACE COURSE

⑨ PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

REVISIONS NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/30/95
R. SHAH	03/10/95
A. ABBAS	03/21/97
R. WIEDEMAN	05/14/04
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: VERT. HORIZ. NONE

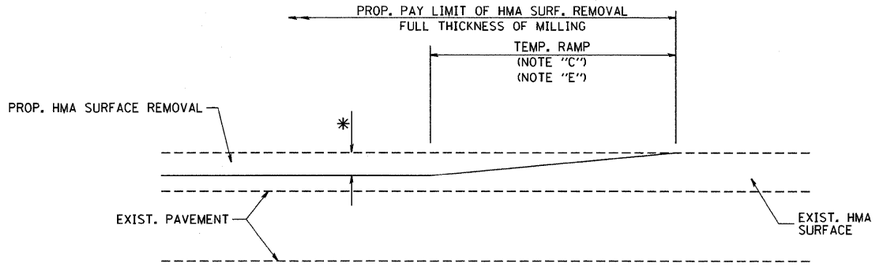
DRAWN BY

CHECKED BY

BD600-03 (BD-8)

CONTRACT NO. 62930

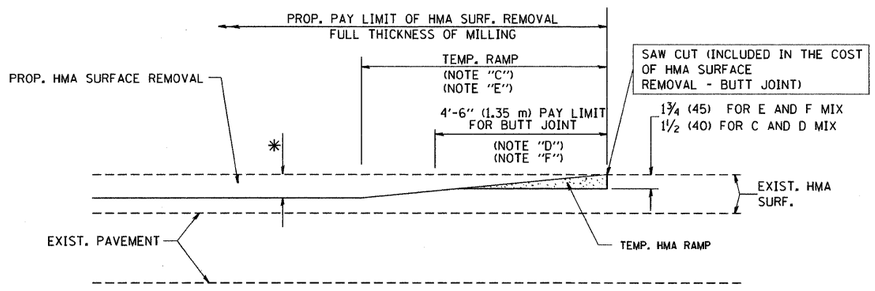
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	66
STA.		TO STA.		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

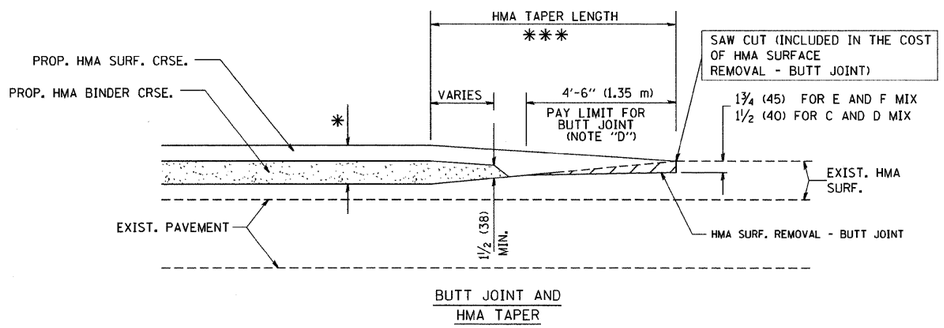


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

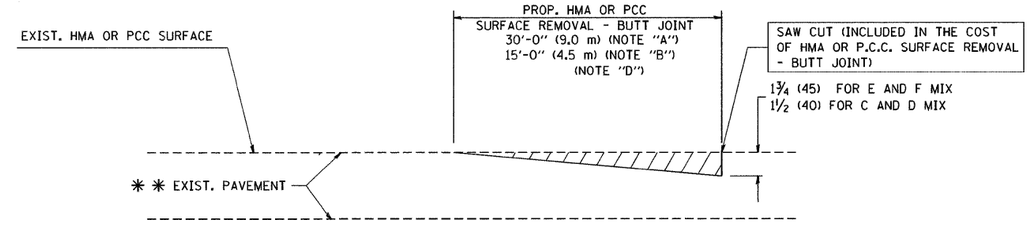
OPTION 2

TYPICAL TEMPORARY RAMP

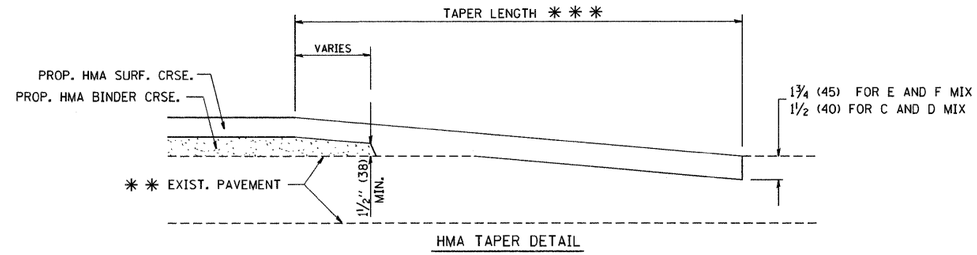


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.

B: MINOR SIDE ROADS.

C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.

F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

SCALE: VERT. NONE HORIZ.

DRAWN BY

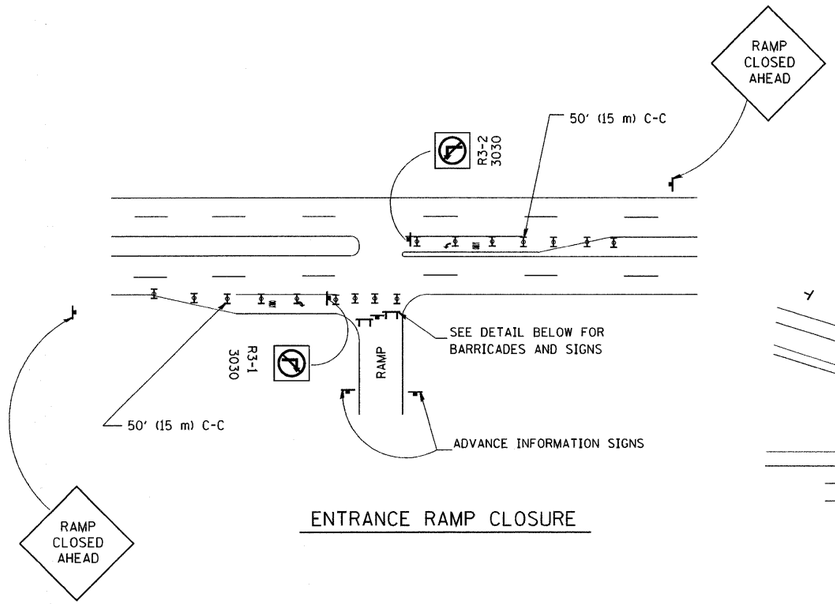
CHECKED BY

BD400-05 (VI=BD32)

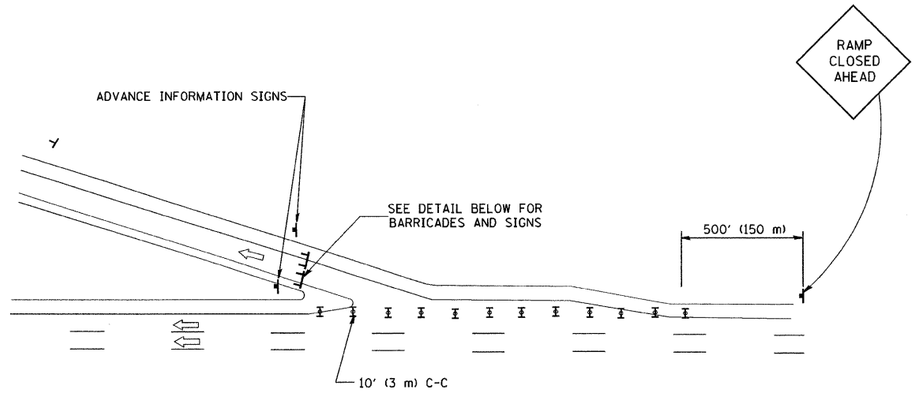
PLOT DATE = 3/9/2007
FILE NAME = c:\projects\vi=bd32.dgn
PLOT SCALE = 80.0000 ' / IN.
USER NAME = bauerdl

CONTRACT NO. 62930

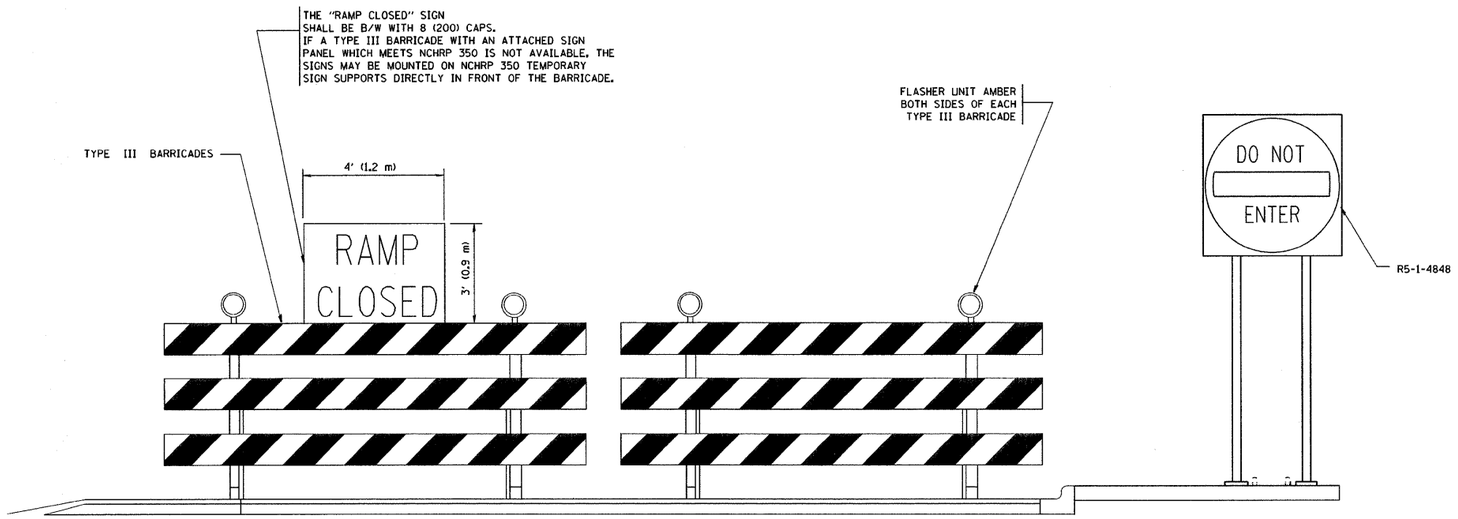
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	67
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



ENTRANCE RAMP CLOSURE



EXIT RAMP CLOSURE

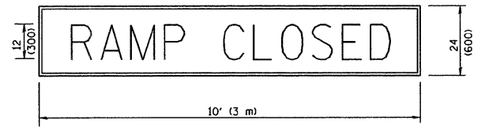


DETAIL FOR REQUIRED BARRICADES & SIGNS

SYMBOLS

- TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- TYPE III BARRICADE WITH FLASHING LIGHT

RAMP CLOSURE ADVANCE WARNING SIGN

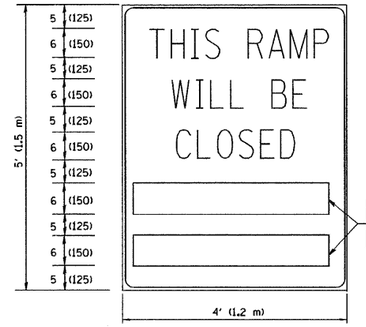


BLACK LEGEND ON ORANGE REFLECTORIZED BACKGROUND

I (25) BORDER

THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR THE CLOSED EXIT RAMPS.

RAMP CLOSURE ADVANCE INFORMATION SIGN



BLACK LEGEND ON WHITE REFLECTORIZED BACKGROUND

½ (12) BORDER

THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

GENERAL NOTES:

1. CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
2. STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
3. A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES.
4. ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED.
5. THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
7. THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY- FOUR 24 HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED TWENTY FOUR 24 HOURS IN LENGTH.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
DWS	2-83
DWS	1/90
DWS	9/94
DWS	12/94
DWS/JAF	12/02
JAF	2/06
SPB	1/07
Revise devices to meet NCHRP 350	4/03

ILLINOIS DEPARTMENT OF TRANSPORTATION

FREEWAY
ENTRANCE AND EXIT RAMP
CLOSURE DETAILS

SCALE: NONE

DRAWN BY

CHECKED BY

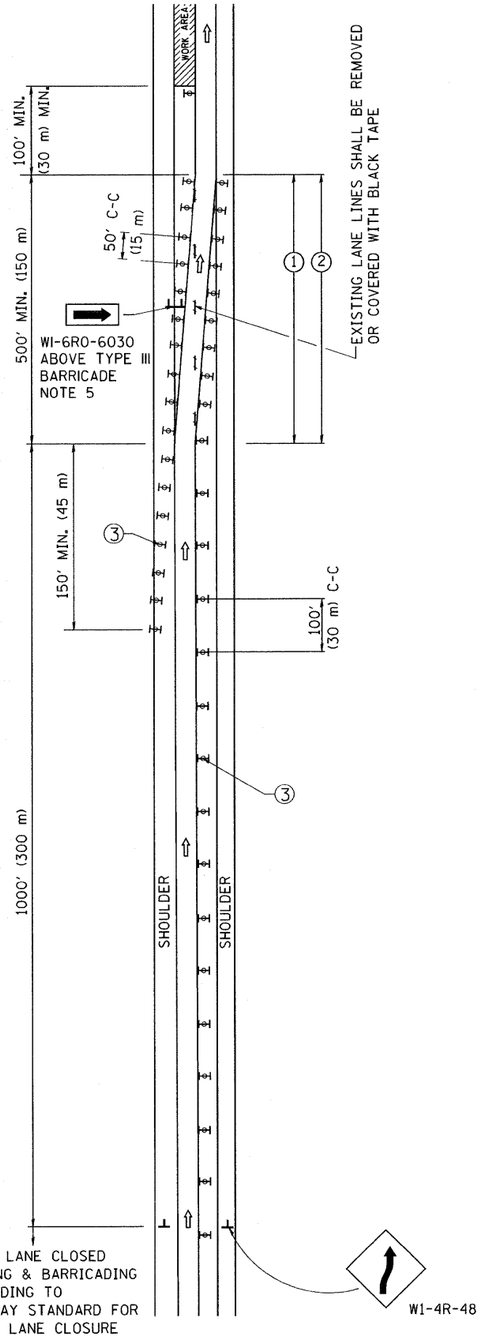
TC-8

PLOT DATE = 3/6/2007
FILE NAME = K:\dsn\tc08.dgn
PLOT SCALE = 50.000 ' / IN.
USER NAME = bauerdl

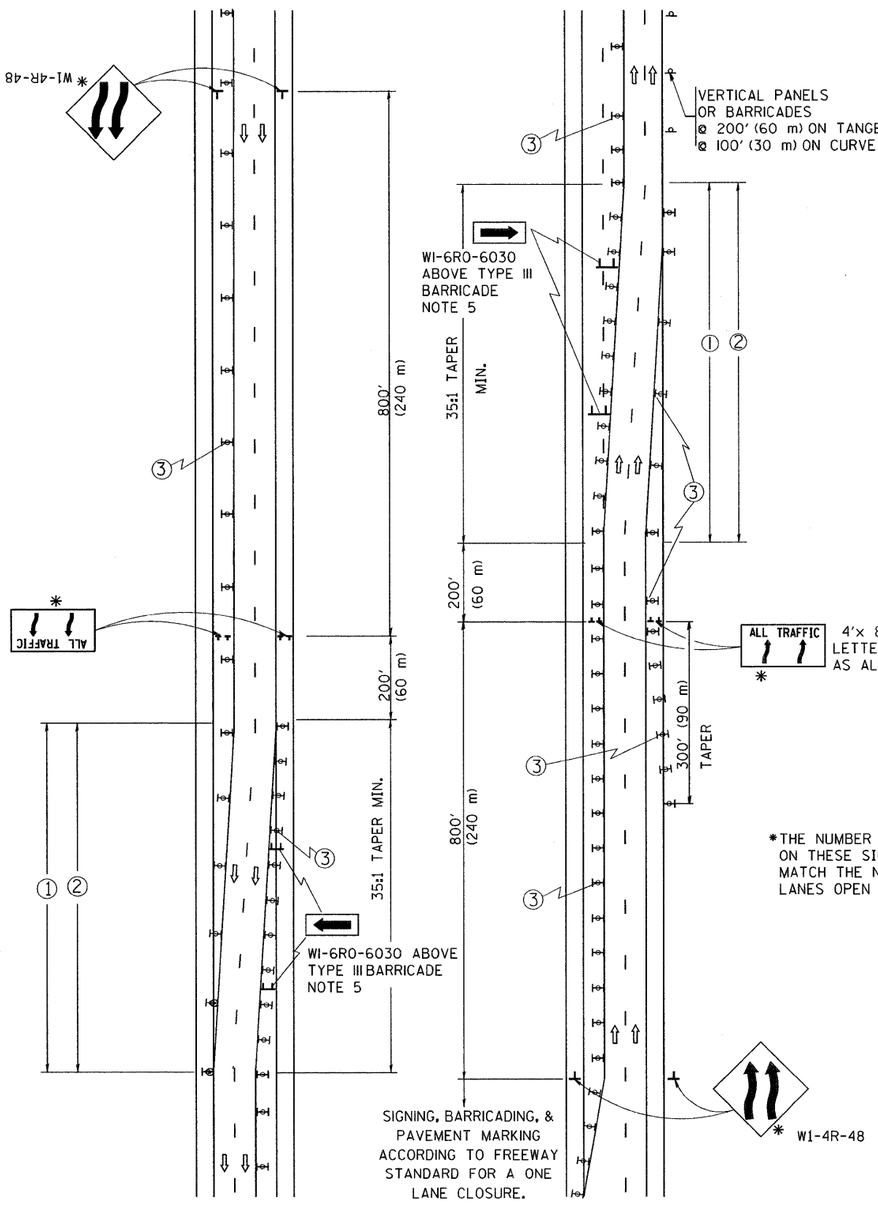
CONTRACT NO. 62930

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	68
STA.		TO STA.		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

SINGLE LANE WEAVE



MULTI-LANE WEAVE



*THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC

ALL TRAFFIC * 4'x 8' (1.2 m x 2.4 m); 1 (25) BORDER; 10 (250) CAPITAL LETTERS BACKGROUND SHEETING SHALL BE THE SAME AS ALL DIAMOND SHAPED CONSTRUCTION SIGNS.

GENERAL NOTES

1. EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED OR COVERED WITH BLACK TAPE. PAVEMENT MARKING REMOVAL OR BLACK TAPE SHALL NOT BE REQUIRED FOR LANE CLOSURES UNDER 24 HOURS IN DURATION.
2. CONTINOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVE LANE LINES SHALL BE 10'-30' (3 m-9 m) SKIP DASH, WHITE.
3. PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
4. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
5. IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 IS NOT AVAILABLE, THE SIGNS MAY BE MOUNTED ON NCHRP 350 TEMPORARY SIGN SUPPORTS DIRECTLY IN FRONT OF THE BARRICADE.
6. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.

SYMBOLS

- DIRECTION OF TRAFFIC
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH MONO-DIRECTIONAL STEADY BURNING LIGHT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

REVISIONS NAME	DATE
DWS	2/87
DWS	1/90
DWS	12/27/94
DWS	11/96
JAF	4/03
JAF	2/06
SPB	1/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE & MULTI-LANE WEAVE

SCALE: NONE

DRAWN BY R.H.

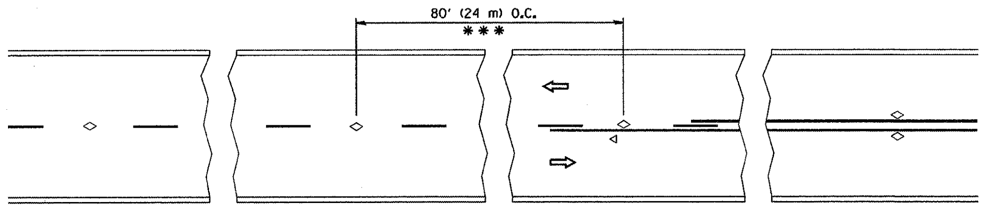
CHECKED BY

TC-9

PLOT DATE = 3/6/2007
FILE NAME = c:\projects\tc09.dgn
PLOT SCALE = 50.208 ' / IN.
USER NAME = bauerdl

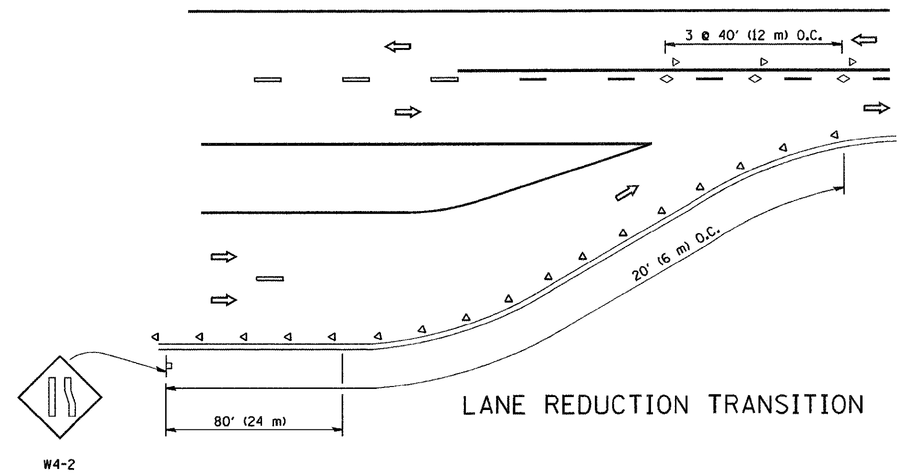
CONTRACT NO. 62930

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	69
STA.		TO STA.		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

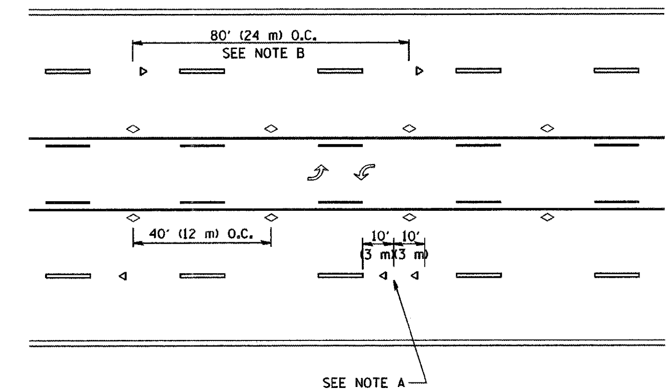


* * * REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

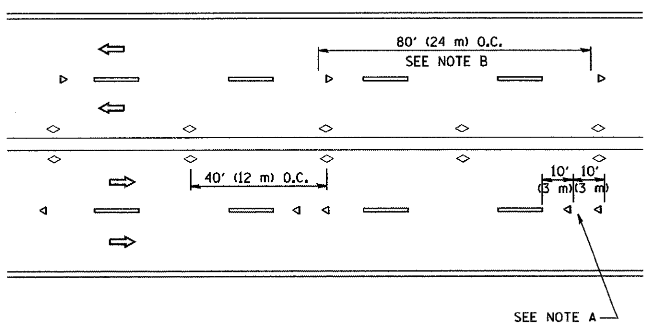
TWO-LANE/TWO-WAY



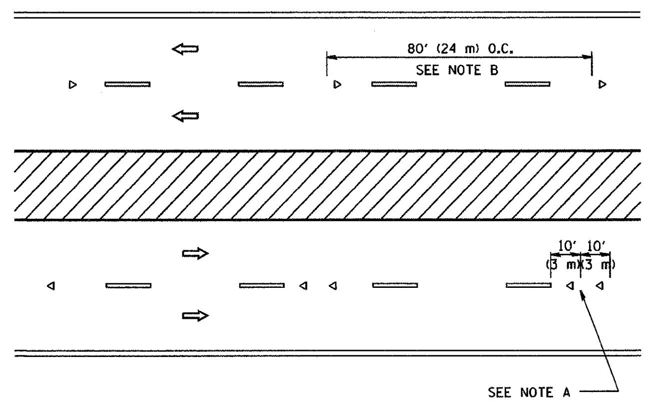
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

LANE MARKER NOTES

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

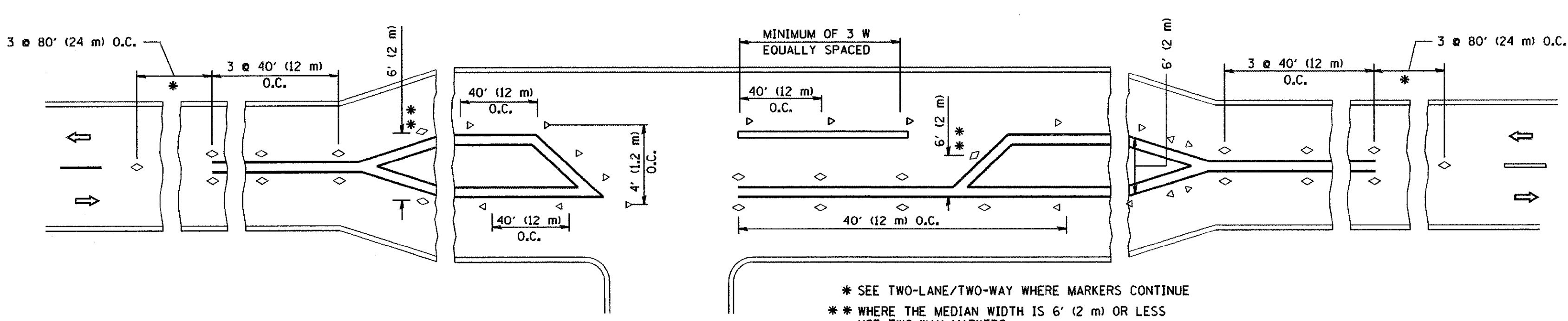
A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE

* * WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

REVISIONS	
NAME	DATE
T. RAMMACHER	09-19-94
T. RAMMACHER	03-12-99
T. RAMMACHER	01-06-00

ILLINOIS DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE

DRAWN BY CADD

CHECKED BY

TC-11

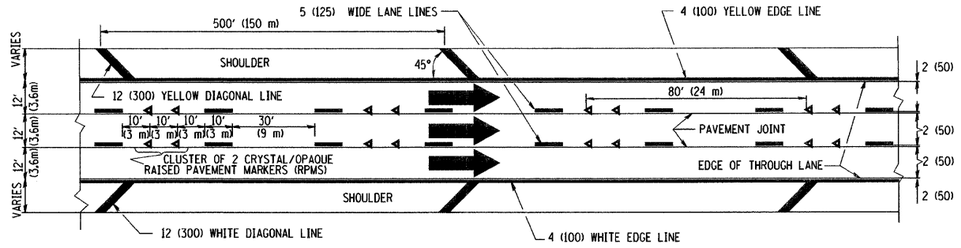
PLOT DATE = 3/6/2007
FILE NAME = K:\diststd\tc11.dgn
PLOT SCALE = 50.000 ' / IN.
USER NAME = bauerdl

CONTRACT NO. 62930

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	70
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH

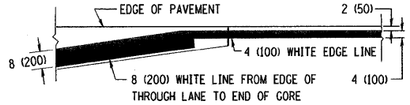
THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH



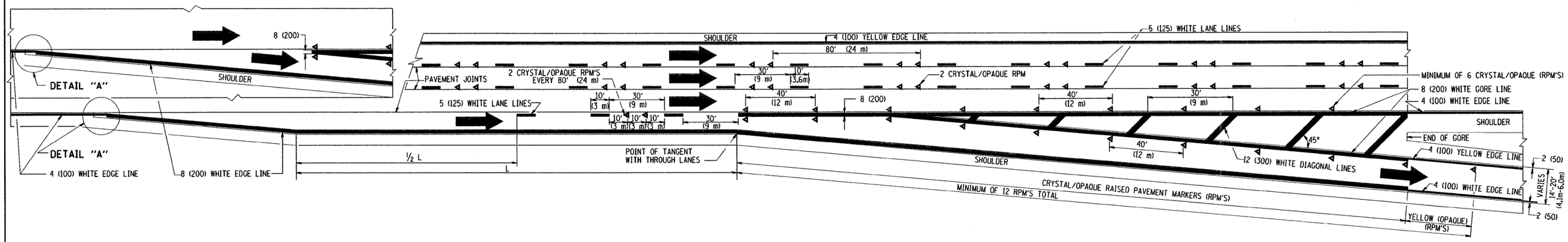
TYPICAL EDGE LINES & LANE LINES

NOTES:

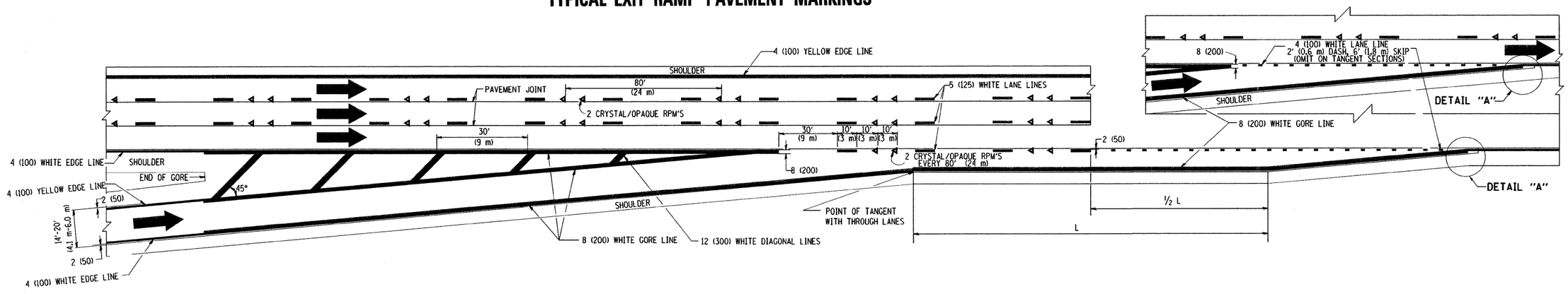
1. THERMO PLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR THE EDGE LINES, GORE LINES, AND DIAGONAL LINES ON BITUMINOUS PAVEMENT ONLY.
2. PREFORMED PLASTIC TYPE B PAVEMENT MARKING LINE SHALL BE USED FOR ALL LANE LINES ON BITUMINOUS PAVEMENT
3. POLYUREA PAVEMENT MARKING SHALL BE USED FOR ALL MARKINGS ON PCC



DETAIL "A"



TYPICAL EXIT RAMP PAVEMENT MARKINGS



TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS

REVISIONS	
NAME	DATE
DWS	1/90
DWS	5/91
AH	3/96
DWS	7/96
JAF	2/06
SPB	1/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS

SCALE: NONE

DRAWN BY C.A.D.D.

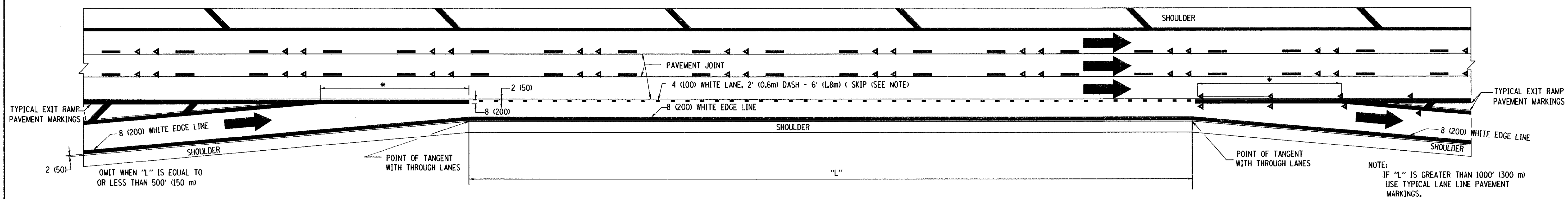
CHECKED BY

TC12 SHEET 1 OF 2

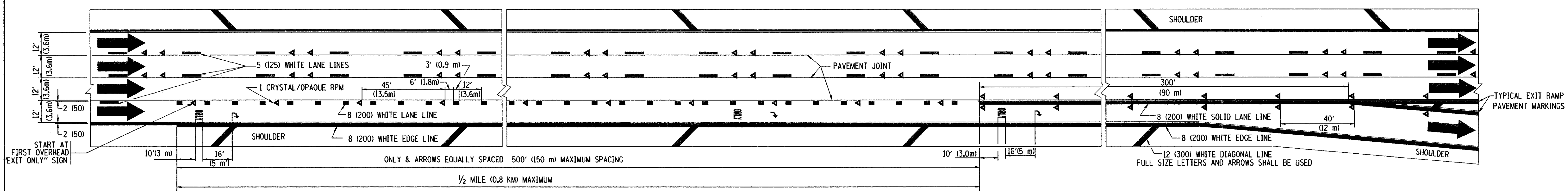
PLOT DATE = 3/6/2007
FILE NAME = s:\standards\tc12.dgn
PLOT SCALE = 50.000 '/ IN.
USER NAME = bauer-dl

CONTRACT NO. 62930

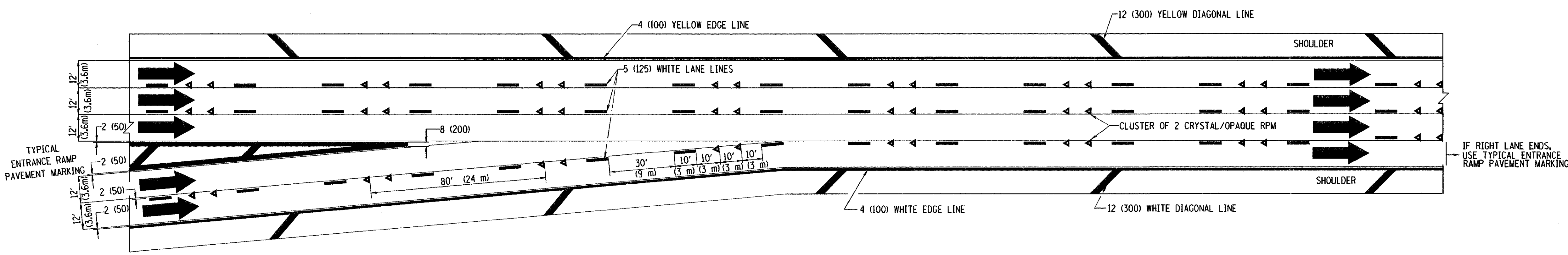
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	71
STA.		TO STA.		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



TYPICAL ENTRANCE/EXIT RAMP COMBINATION PAVEMENT MARKINGS



TYPICAL EXIT ONLY LANE PAVEMENT MARKINGS



TYPICAL TWO LANE ENTRANCE RAMP PAVEMENT MARKINGS

REVISIONS NAME	DATE
DWS	1/90
DWS	5/91
SPB	1/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS

SCALE: NONE

DRAWN BY C.A.D.D.

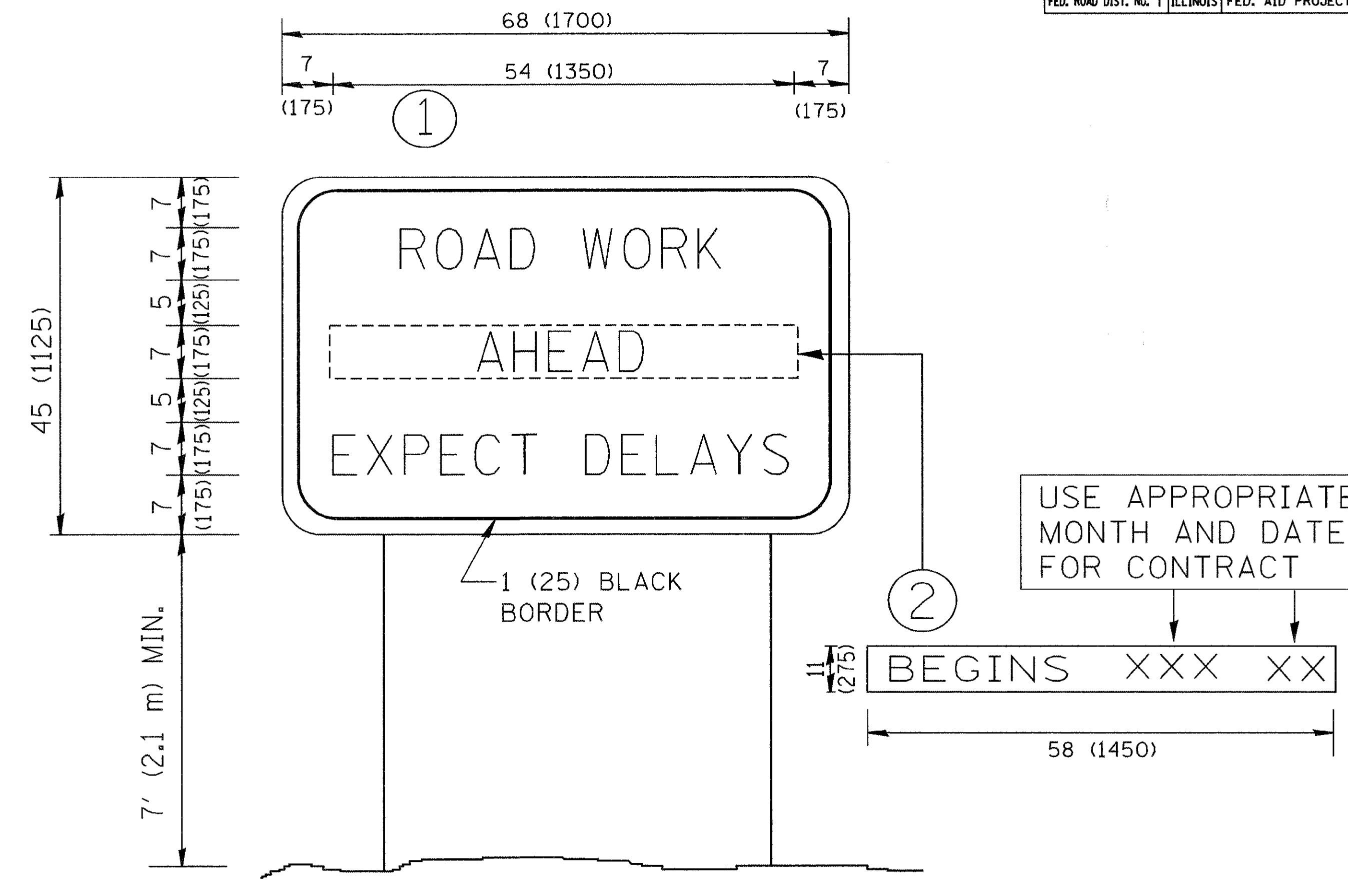
CHECKED BY

TC12 SHEET 2 OF 2

PLOT DATE = 3/6/2007 FILE NAME = K:\dists\tc12.dgn PLOT SCALE = 50.000 '/ IN. USER NAME = bauerdl

CONTRACT NO. 62930

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	88 (B&B-1) BR	WILL	72	72
STA.		TO STA.		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
R. MIRS	9-15-97
R. MIRS	12-11-97
T. RAMMACHER	2-2-99
C. JUCIUS	1-31-07

ILLINOIS DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD
INFORMATION SIGN

SCALE: NONE

DRAWN BY DESIGN
CHECKED BY
TC22

PLOT DATE = 3/8/2007
FILE NAME = K:\diststd\tc22.dgn
PLOT SCALE = 50.000 ' / IN.
USER NAME = bauerdl