

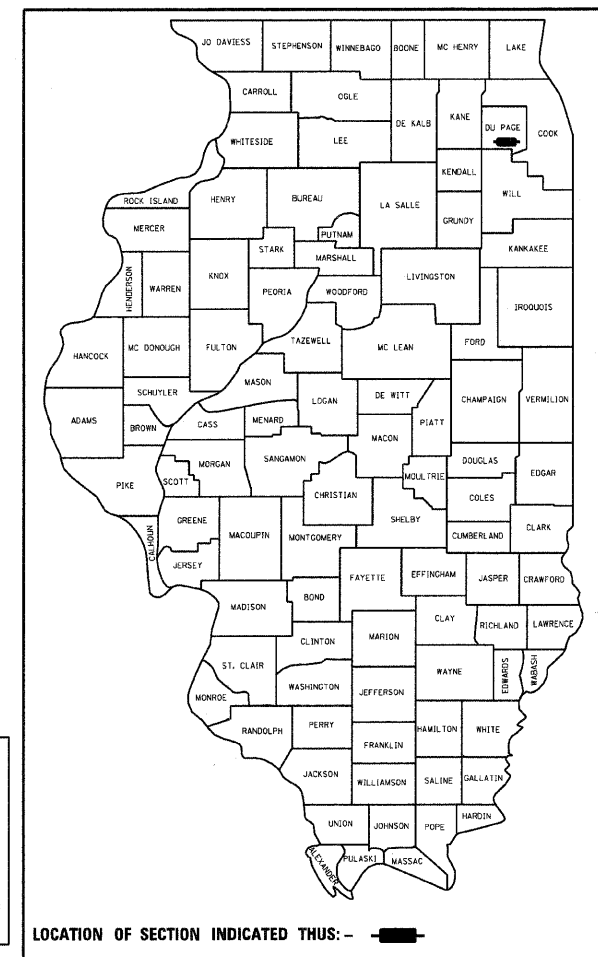
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
**DIVISION OF HIGHWAYS**  
**PLANS FOR PROPOSED**  
**FEDERAL AID HIGHWAY**

**VARIOUS ROUTES IN DUPAGE COUNTY**  
**SECTION 09-00232-06-RS**  
**PROJECT: ARA-9003(490)**  
**JOB NO C-91-077-10**  
**DUPAGE COUNTY**

**INDEX OF SHEETS**

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FOR INDEX OF STANDARDS, SEE SHEET 3

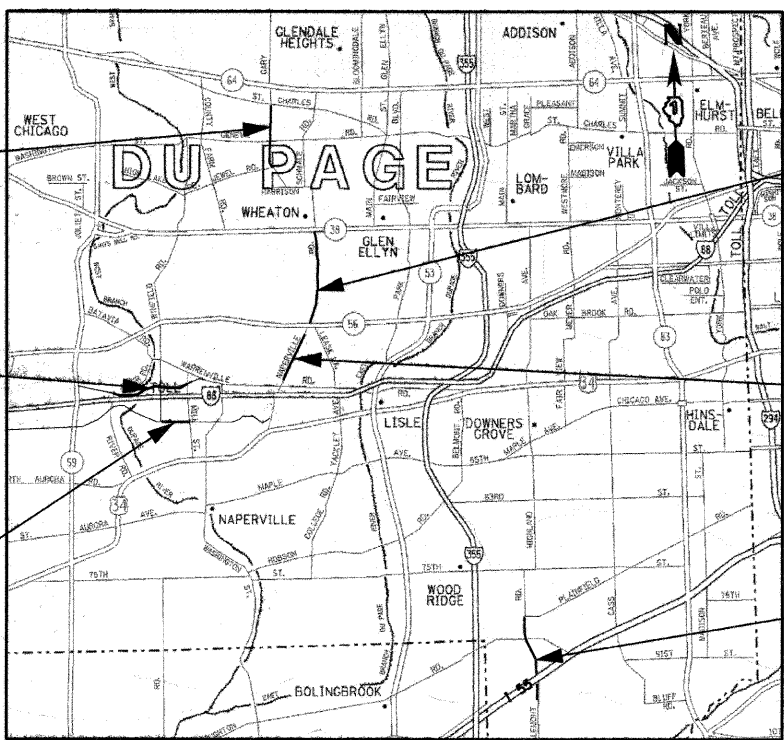


FIELD ENGINEER: MARILIN SOLOMON 847-705-4407

**GARY AVENUE (FAU 2561)**  
**JEWELL ROAD TO ST. CHARLES ROAD**  
 (STA. 127+32 TO STA. 199+47)  
 GROSS LENGTH OF PROJECT  
 7215 FEET = 1.37 MILES  
 NET LENGTH OF PROJECT  
 6268 FEET = 1.19  
 ADT (2001) = 15,400  
 POSTED SPEED LIMIT = 35 MPH TO 45 MPH  
 DESIGN SPEED LIMIT = 45 MPH

**FERRY ROAD (FAU 1481)**  
**(RIVER ROAD TO MILL STREET)**  
 STA. 99+06 TO STA. 166+99  
 GROSS LENGTH OF PROJECT  
 6793 FEET = 1.29 MILES  
 NET LENGTH OF PROJECT  
 6521 FEET = 1.24 MILES  
 ADT (2005) = 17,000  
 POSTED SPEED LIMIT = 40 MPH  
 DESIGN SPEED LIMIT = 45 MPH

**DIEHL ROAD (FAU 1485)**  
**(WINFIELD ROAD TO MILL STREET)**  
 STA. 88+66 TO STA. 118+95  
 GROSS LENGTH OF PROJECT  
 3432 FEET = 0.65 MILES  
 NET LENGTH OF PROJECT  
 3432 FEET = 0.65 MILES  
 ADT (2005) = 27,000  
 POSTED SPEED LIMIT = 40 MPH  
 DESIGN SPEED LIMIT = 45 MPH



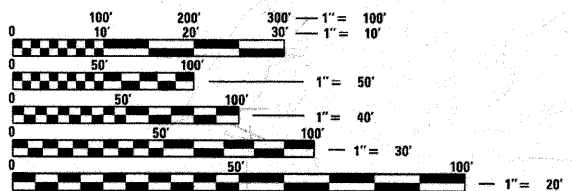
**NAPERVILLE ROAD (FAP 0856)**  
**(IL 38 TO IL 56)**  
 STA. 58+12 TO STA. 163+44  
 GROSS LENGTH OF PROJECT  
 10,532 FEET = 1.99 MILES  
 NET LENGTH OF PROJECT  
 10,532 FEET = 1.99 MILES  
 ADT (2005) = 36,600  
 POSTED SPEED LIMIT = 35 MPH TO 40 MPH  
 DESIGN SPEED LIMIT = 45 MPH

**NAPERVILLE ROAD (FAP 0856)**  
**(IL 56 TO LUCENT LANE)**  
 STA. 59+63 TO STA. 124+24  
 GROSS LENGTH OF PROJECT  
 5237 FEET = 0.99 MILES  
 NET LENGTH OF PROJECT  
 5237 FEET = 0.99 MILES  
 ADT (2003) = 35,800  
 POSTED SPEED LIMIT = 45 MPH TO 50 MPH  
 DESIGN SPEED LIMIT = 45 MPH TO 55 MPH

**LEMONT ROAD (FAU 2612)**  
**(83RD STREET TO 97TH STREET)**  
 STA. 213+19 TO STA. 310+76  
 GROSS LENGTH OF PROJECT  
 9757 FEET = 1.85 MILES  
 NET LENGTH OF PROJECT  
 9757 FEET = 1.85 MILES  
 ADT (2006) = 34,900  
 POSTED SPEED LIMIT = 40 MPH TO 50 MPH  
 DESIGN SPEED LIMIT = 45 MPH TO 55 MPH

**LOCATION MAP**  
**NOT TO SCALE**

S 1,2,3,5,8,21,28,33 T 39N, R 10E  
 S 30,31 T 38N R 11E



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.



J.U.L.I.E.  
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
 1-800-892-0123  
 OR 811

PROJECT MANAGER: MARK A. REZNICEK, P.E.

CONTRACT NO. 63335

**ESI CONSULTANTS, LTD**  
 1979 N. MILL STREET, SUITE 100  
 NAPERVILLE, IL 60563  
 (630) 420-1700  
 WWW.ESICONSULTANTS.LTD.COM  
 ILLINOIS DEPARTMENT OF PROFESSIONAL REGULATION REGISTRATION #184-003685



DUPAGE COUNTY DOT  
 JACK T. KNUEPFER ADMIN BLDG.  
 421 N. COUNTY FARM ROAD  
 WHEATON, IL 60187

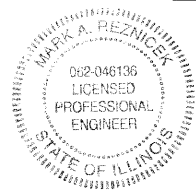
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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
 DIVISION OF HIGHWAYS

APPROVED October 18, 2009  
*Charles S. Fikowski*  
 DUPAGE COUNTY DIVISION OF TRANSPORTATION, COUNTY ENGINEER

PASSED OCTOBER 26, 2009  
*C. H. St. Charles*  
 DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID  
 BASED ON LIMITED  
 REVIEW OCTOBER 26, 2009  
*Diana M. O'Keefe*  
 DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER



**PROFESSIONAL ENGINEER'S CERTIFICATION**  
 I, MARK A. REZNICEK, A LICENSED PROFESSIONAL ENGINEER OF ILLINOIS, HEREBY CERTIFY THAT THIS SUBMISSION WAS PREPARED ON BEHALF OF THE COUNTY OF DUPAGE BY ESI CONSULTANTS, LTD. UNDER MY PERSONAL DIRECTION. THIS TECHNICAL SUBMISSION IS INTENDED TO BE USED AS AN INTEGRAL PART OF AND IN CONJUNCTION WITH THE PROJECT SPECIFICATIONS AND CONTRACT DOCUMENTS.

DATED THIS 19th DAY OF October, A.D., 2009.

*Mark A. Reznick*

ENGINEER  
 ILLINOIS REG. PROF. ENGINEER NO. 062-046136 EXPIRATION DATE 11/30/2011



1. LOCATION OF UTILITIES SHOWN ON PLANS ARE APPROXIMATE ONLY, AND ARE NOT NECESSARILY COMPLETE. CONTRACTOR SHALL MAKE HIS OWN INVESTIGATIONS AS TO LOCATION OF ALL EXISTING UNDERGROUND STRUCTURES, CABLES, AND PIPELINES.
2. ALL ROAD SIGNS, STREET SIGNS, AND TRAFFIC SIGNS WHICH NEED TO BE RELOCATED OR MOVED DUE TO CONSTRUCTION SHALL BE TAKEN DOWN AND STORED BY THE CONTRACTOR EXCEPT THOSE THAT ARE NECESSARY FOR PROPER TRAFFIC CONTROL WHICH SHALL BE TEMPORARILY RESET UNTIL COMPLETION OF CONSTRUCTION OPERATIONS. AFTER COMPLETION OF THE WORK, THE CONTRACTOR SHALL RESET ALL SAID SIGNS. THE WORK SHALL BE INCLUDED IN THE TRAFFIC CONTROL PAY ITEMS, AND SHALL NOT BE PAID FOR SEPARATELY.
3. SPECIAL ATTENTION IS DRAWN TO THE FACT THAT ARTICLE 105.06 OF THE STANDARD SPECIFICATIONS REQUIRES THE CONTRACTOR TO HAVE A COMPETENT SUPERINTENDENT ON THE PROJECT SITE AT ALL TIMES, IRRESPECTIVE OF THE AMOUNT OF WORK SUBLET. THE SUPERINTENDENT SHALL BE CAPABLE OF READING AND UNDERSTANDING THE PLANS AND SPECIFICATIONS, SHALL HAVE FULL AUTHORITY TO EXECUTE ORDERS TO EXPEDITE THE PROJECT, SHALL BE RESPONSIBLE FOR SCHEDULING AND HAVE CONTROL OF ALL WORK AS THE AGENT OF THE CONTRACTOR. FAILURE TO COMPLY WITH THIS PROVISION WILL RESULT IN A SUSPENSION OF WORK AS PROVIDED IN ARTICLE 108.08.
4. ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST STANDARD OF THE DEPARTMENT AS SHOWN ON THE PLANS.
5. THE UNIT PRICE FOR ALL REMOVAL PAY ITEMS SHALL INCLUDE ALL REQUIRED SAW CUTS.
6. ALL WORK PERFORMED RELATIVE TO THIS IMPROVEMENT SHALL COMPLY WITH ALL APPLICABLE RULES AND REGULATIONS OF O.S.H.A.
7. CONTRACTOR SHALL PROVIDE AND INSTALL TWO WEIGHTED SAND BAGS ON EACH TYPE II OR TYPE II BARRICADE USED. (ONE WEIGHTED SAND BAG ACROSS EACH BOTTOM RAIL.) THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT THE COST SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT UNIT PRICE FOR THE CONSTRUCTION ITEMS INVOLVED AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
8. MIXTURE FOR CRACKS, JOINTS AND FLANGWAYS: AN ESTIMATED QUANTITY BASED ON FIELD OBSERVATION HAS BEEN INCLUDED IN THE CONTRACT. THE LOCATIONS AND LIMITS OF ALL JOINT OR CRACK FILLING SHALL BE DETERMINED BY THE ENGINEER AT THE TIME OF CONSTRUCTION. IF THE MIXTURE FOR CRACKS, JOINTS AND FLANGWAYS IS NOT REQUIRED, THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.
9. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)
10. 10 FEET TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB & GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS & GUTTERS AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
11. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND ANY CITY, VILLAGE, AND/OR TOWNSHIP AGENCIES.
12. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 2 INCHES (50 MM). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3-1/2 INCHES (85 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
13. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE THE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE IDOT DISTRICT 1 DETAIL FOR "BUTT JOINT AND HMA TAPER DETAILS" ON LEMONT ROAD AND THE DUPAGE COUNTY DETAIL FOR "BUTT JOINT DETAIL" FOR NAPERVILLE ROAD, GARY AVENUE, FERRY ROAD AND DIEHL ROAD.
14. WHENEVER, DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY AT THE CONCLUSION OF CONSTRUCTION OPERATIONS. ALL DRAINAGE STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT PAY ITEMS.
15. EXISTING PUBLIC AND PRIVATE UTILITIES ARE SHOWN ON THE PLANS ACCORDING TO INFORMATION OBTAINED FROM UTILITY COMPANIES, MUNICIPALITIES, AND SURVEYS. THE CONTRACTOR SHALL FAMILIARIZE HIMSELF WITH THE LOCATION OF ALL UTILITIES AND STRUCTURES THAT MAY BE FOUND IN THE VICINITY OF THE CONSTRUCTION. HE WILL ALSO ASSUME RESPONSIBILITY FOR ALL UTILITIES WHETHER SHOWN OR NOT AND MUST REALIZE THAT THE ACTUAL LOCATIONS AND/OR ELEVATIONS OF THE UTILITIES MAY BE DIFFERENT THAN INDICATED.
16. SHOULD ANY DAMAGES OCCUR DUE TO THE CONTRACTOR'S NEGLIGENCE, THE CONTRACTOR, AT HIS OWN EXPENSE, SHALL MAKE REPAIRS IN A MANNER ACCEPTABLE TO THE ENGINEER. THE CONTRACTOR SHALL NOTIFY ALL UTILITY COMPANIES OF HIS CONSTRUCTION SCHEDULE AND COORDINATE CONSTRUCTION OPERATIONS WITH THE UTILITY COMPANIES SO THAT RELOCATION OF UTILITY LINES AND STRUCTURES MAY PROCEED IN AN ORDERLY MANNER.
17. GARY AVENUE, FERRY ROAD, DIEHL ROAD, NAPERVILLE ROAD, AND LEMONT ROAD SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES. WHEN NECESSARY TO CLOSE ONE LANE DUE TO CONSTRUCTION, THE CONTRACTOR SHALL MAINTAIN A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION DURING CONSTRUCTION HOURS WITH THE USE OF SIGNS AND FLAGMEN AS SHOWN ON THE TRAFFIC CONTROL STANDARDS. ACCESS TO ALL ADJOINING PROPERTIES SHALL BE MAINTAINED AT ALL TIMES. LANE CLOSURES SHALL NOT BEGIN EARLIER THAN 8 AM. EASTBOUND CLOSURES SHALL BE BETWEEN 9AM AND DUSK. WESTBOUND CLOSURES SHALL BE BETWEEN 8AM AND 3PM.
18. NO WORK SHALL COMMENCE UNTIL TRAFFIC CONTROL REQUIREMENTS ARE MET.
19. ALL UTILITIES, SCHOOL DISTRICTS, LOCAL POLICE, AND FIRE DEPARTMENTS SHALL BE NOTIFIED BY THE CONTRACTOR PRIOR TO THE START OF CONSTRUCTION.

20. UNLESS AUTHORIZED BY THE ENGINEER, ALL EXISTING ACCESS POINTS SHALL BE MAINTAINED AT ALL TIMES BY THE CONTRACTOR.
21. DURING THE CONSTRUCTION, THE CONTRACTOR WILL BE REQUIRED, AT HIS EXPENSE, TO HAVE AVAILABLE A WATER TRUCK OR SIMILAR EQUIPMENT TO CONTROL DUST. IF NECESSARY, THE CONTRACTOR SHALL BE REQUIRED TO CONTROL DUST DURING NONWORKING HOURS. THIS WORK IS INCLUDED IN THE CONTRACT UNIT PRICE FOR PAVEMENT REMOVAL.
22. ALL EXCESS MATERIAL (BROKEN CONCRETE, CULVERT PIPE, WASTE ROADWAY EXCAVATION, SURPLUS MATERIAL FROM SEWER TRENCHES, ETC.) SHALL BE LEGALLY DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT OF WAY. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO SELECT DUMP SITES AND OBTAIN PERMISSION AND ALL NECESSARY PERMITS TO USE SUCH DUMP SITES. PAYMENT FOR THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR PAVEMENT REMOVAL.
23. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RECESSED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
24. ALL PAVEMENT PATCHING LOCATIONS, COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT LOCATIONS, AND SIDEWALK REMOVAL AND REPLACEMENT LOCATIONS, SHALL BE DETERMINED BY THE ENGINEER.
25. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
26. THE CONTRACTOR SHALL MILL BEFORE PATCHING.

DENOTES INCIDENTAL ITEM OR WORK

**HIGHWAY STANDARDS**

|           |   |
|-----------|---|
| 000001-05 | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS                                |
| 001006    | DECIMAL OF AN INCH AND OF A FOOT  |
| 420001-07 | PAVEMENT JOINTS   |
| 606001-04 | CONCRETE CURB & COMBINATION CURB & GUTTER                                   |
| 701411-06 | LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS ≥ 45 MPH      |
| 701456    | PARTIAL EXIT RAMP CLOSURE FREEWAY / EXPRESSWAY                              |
| 701501-05 | URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED                                       |
| 701601-06 | URBAN LANE CLOSURE, MULTI LANE, 1-WAY OR 2-WAY, WITH NON TRAVERSABLE MEDIAN |
| 701606-06 | URBAN LANE CLOSURE, MULTI LANE, 2-WAY MOUNTABLE MEDIAN                      |
| 701701-06 | LANE CLOSURE, MULTI LANE, INTERSECTION, FOR SPEEDS (45 MPH)                 |
| 701901-01 | TRAFFIC CONTROL DEVICES   |
| 780001-02 | TYPICAL PAVEMENT MARKINGS   |
| 886001-01 | DETECTOR LOOP INSTALLATIONS   |
| 886006-01 | TYPICAL LAYOUTS FOR DETECTION LOOPS   |

**IDOT DISTRICT ONE STANDARDS**

|       |   |
|-------|---|
| BD-8  | DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING                         |
| BD-22 | PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT                                 |
| BD-24 | CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT                             |
| BD-32 | BUTT JOINT AND HMA TAPER DETAILS  |
| TC-10 | TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS |
| TC-13 | DISTRICT ONE TYPICAL PAVEMENT MARKINGS                                      |
| TC-14 | TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)     |
| TS-05 | DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS                         |
| TS-07 | DISTRICT ONE DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING     |

| HOT-MIX ASPHALT MIX REQUIREMENTS  |                 |
|---|-----------------|
| MIXTURE TYPE  | AIR VOIDS @Ndes |
| PAVEMENT RESURFACING - DIEHL ROAD, NAPERVILLE ROAD (IL 38-56 AND LUCENT TO IL 56) |                 |
| POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL 9.5 mm)              | 4% @ 90 GYR.    |
| PAVEMENT RESURFACING - GARY AVENUE, FERRY ROAD                                    |                 |
| HOT-MIX ASPHALT SURFACE COURSE MIX "D", N70 (IL 9.5 mm)                           | 4% @ 70 GYR.    |
| PAVEMENT RESURFACING - LEMONT ROAD  |                 |
| POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL 9.5 mm)              | 4% @ 90 GYR.    |
| POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL 4.75, N50                        | 4% @ 50 GYR.    |
| LEVELING BINDER (MACHINE METHOD), N70 (IL 9.5 mm)                                 | 4% @ 70 GYR.    |
| SHOULDERS - GARY AVENUE, LEMONT ROAD  |                 |
| HOT-MIX ASPHALT SURFACE COURSE MIX "D", N70 (IL 9.5 mm)                           | 4% @ 70 GYR.    |
| SHOULDERS - NAPERVILLE ROAD (LUCENT TO IL 56)                                     |                 |
| POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL 9.5 mm)              | 4% @ 90 GYR.    |
| PATCHING  |                 |
| CLASS D PATCHES (HMA BINDER IL-19 mm)   | 4% @ 70 GYR.    |

- NOTES:**
1. THE UNIT WEIGHT USED TO CALC. ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
  2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

DRAWN DATE: 04/15/09

**ESI CONSULTANTS, LTD**  
 1979 N. MILL STREET, SUITE 100  
 NAPERVILLE, IL 60563  
 (630) 420-1700  
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 ILLINOIS DEPARTMENT OF PROFESSIONAL REGULATION REGISTRATION #184-003665



DUPAGE COUNTY DOT  
 JACK T. KNUEPFER ADMIN BLDG.  
 421 N. COUNTY FARM ROAD  
 WHEATON, IL 60187

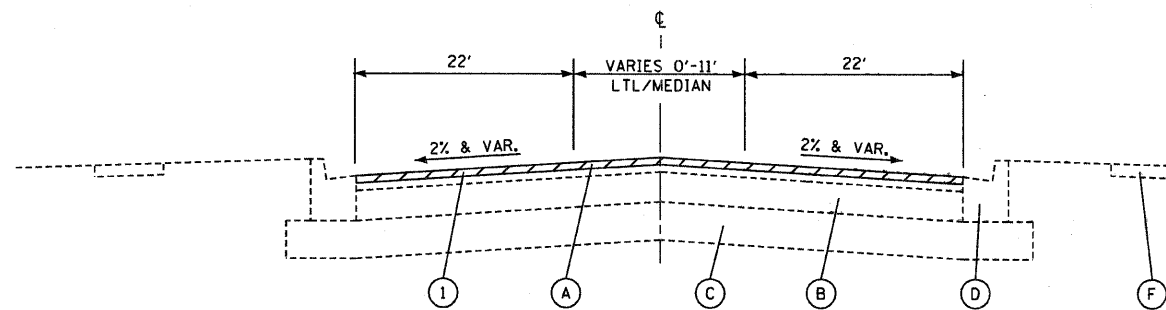
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ESI PROJECT NO. 09-280  
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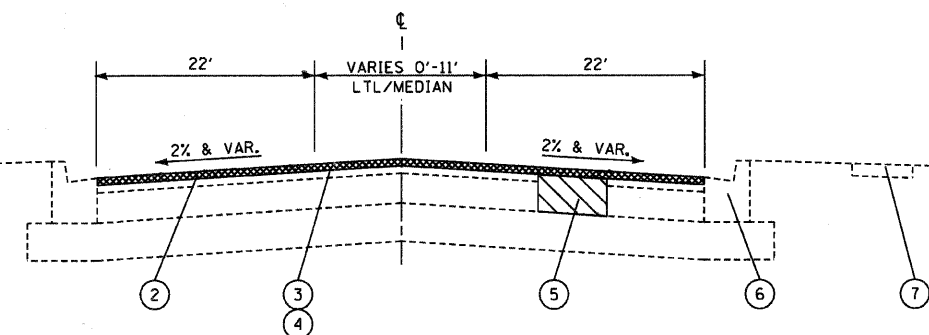
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 TECH: DN  
 QAQC: JS

**DUPAGE LAPP PROJECT  
 CONTRACT NO. 63335**

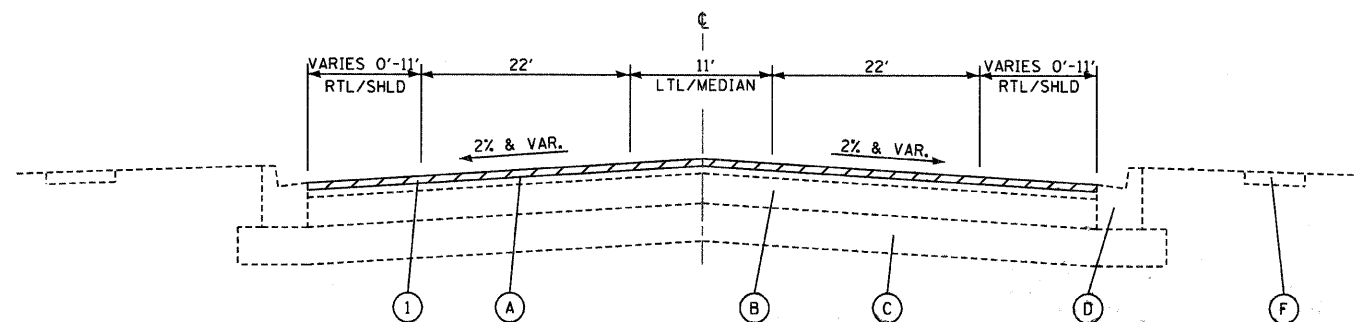
**GENERAL NOTES**



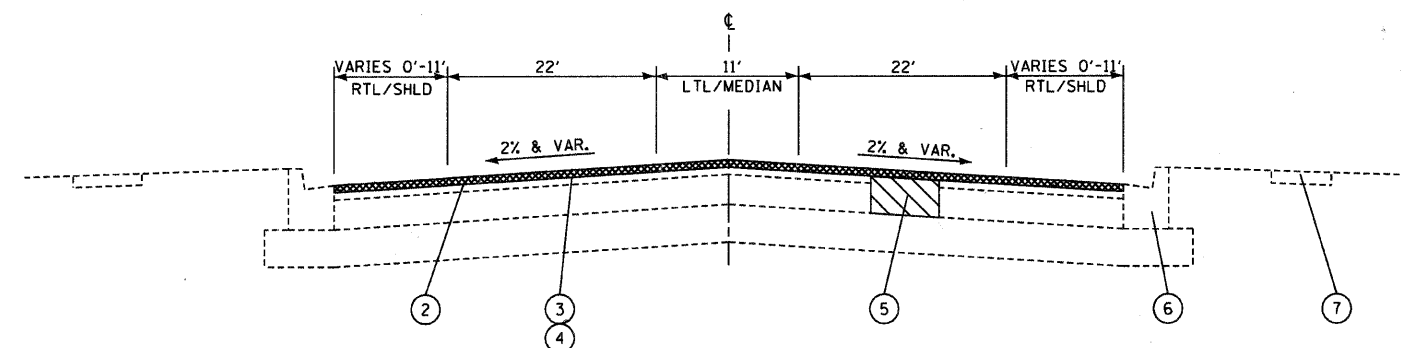
CH 23 NAPERVILLE ROAD  
IL ROUTE 38 TO BLANCHARD STREET  
STA. 58+12 TO STA. 138+85  
EXISTING TYPICAL SECTION



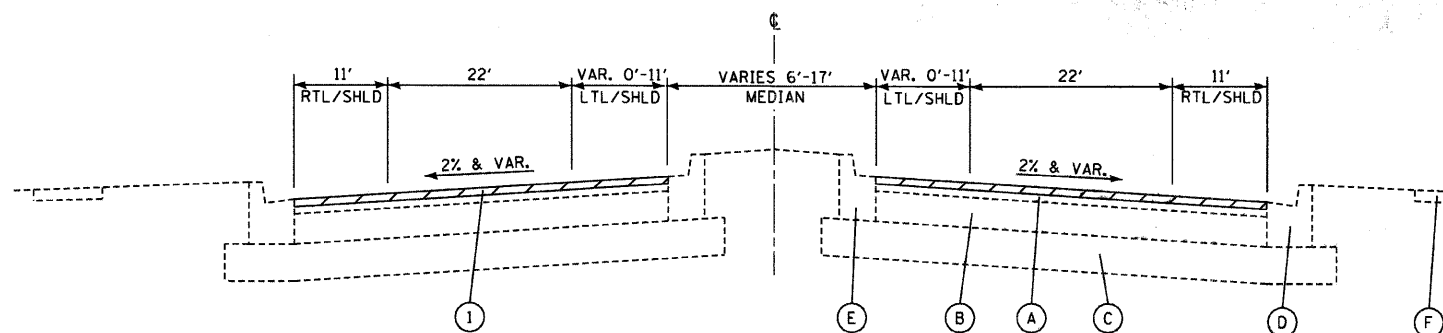
CH 23 NAPERVILLE ROAD  
IL ROUTE 38 TO BLANCHARD STREET  
STA. 58+12 TO STA. 138+85  
PROPOSED TYPICAL SECTION



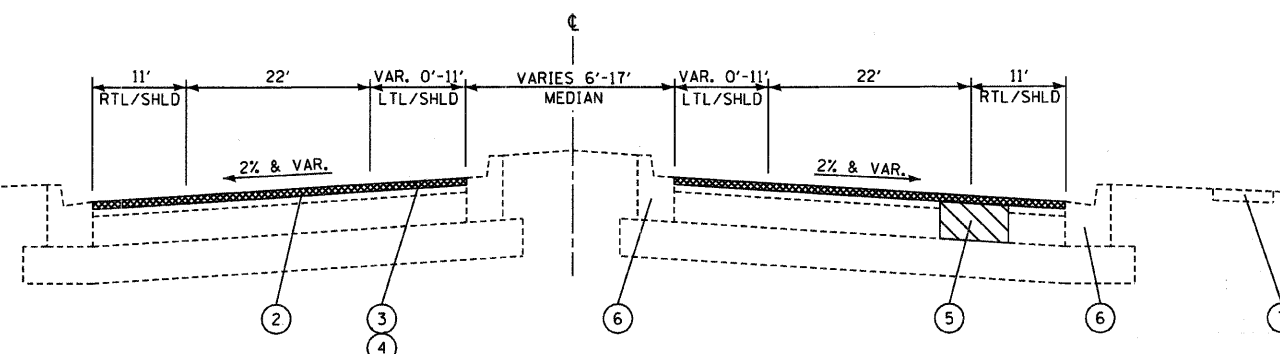
CH 23 NAPERVILLE ROAD  
BLANCHARD STREET TO LOOP ROAD  
STA. 138+85 TO STA. 152+33  
EXISTING TYPICAL SECTION



CH 23 NAPERVILLE ROAD  
BLANCHARD STREET TO LOOP ROAD  
STA. 138+85 TO STA. 152+33  
PROPOSED TYPICAL SECTION



CH 23 NAPERVILLE ROAD  
LOOP ROAD TO NORTH OF IL ROUTE 56  
STA. 152+33 TO 163+44  
EXISTING TYPICAL SECTION



CH 23 NAPERVILLE ROAD  
LOOP ROAD TO NORTH OF IL ROUTE 56  
STA. 152+33 TO STA. 163+44  
PROPOSED TYPICAL SECTION

LEGEND

- |   |   |
|---|---|
| (A) EXISTING HMA BINDER & SURFACE (VARIES 3-1/2" TO 4-1/2") | (1) HMA SURFACE REMOVAL, 1-3/4"   |
| (B) EXISTING HMA OR P.C.C. BASE COURSE                      | (2) POLY. HMA SURFACE COURSE, MIX "F", N90, 1-3/4"                                |
| (C) EXISTING AGGREGATE SUBGRADE                             | (3) BITUMINOUS MATERIALS (PRIME COAT)   |
| (D) EXISTING COMB. CONCRETE CURB & GUTTER, TYPE B-6.18      | (4) AGGREGATE (PRIME COAT)  |
| (E) EXISTING COMB. CONCRETE CURB & GUTTER, TYPE B-6.12      | (5) PATCH (AS DIRECTED BY THE ENGINEER), 13"                                      |
| (F) EXISTING SIDEWALK- VARIOUS LOCATIONS                    | (6) COMB. CONC. CURB & GUTTER REMOVAL & REPLACEMENT (AS DIRECTED BY THE ENGINEER) |
|   | (7) SIDEWALK REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)                |

NOTE:

LEVELING BINDER (MIN. 3/4"), IF REQUIRED, SHALL BE LEVELING BINDER (MACHINE METHOD), N70, IL 9.5

PRINTED DATE: 10/16/2009



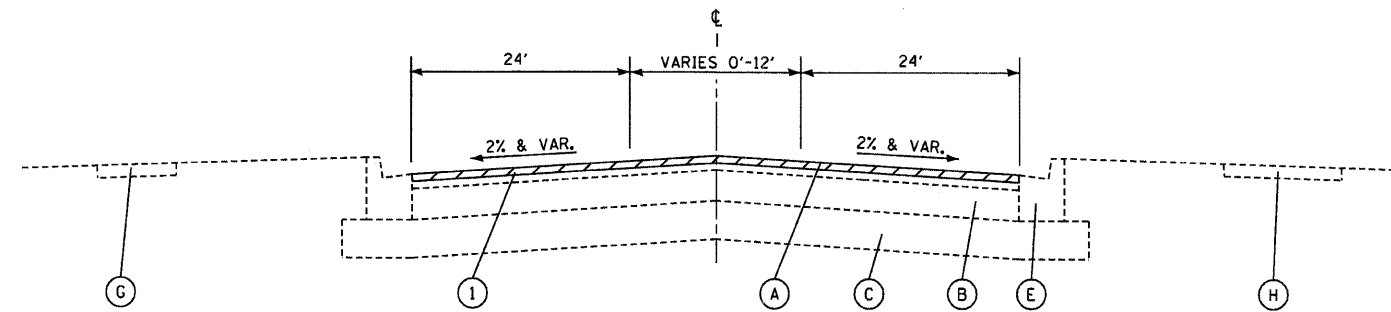
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| ESI PROJECT NO. 09-280       | ENGR. MR  |
| FILE: 04 Typical Sections 01 | TECH: DN  |
| DATE: 06-25-09               | QA/QC: JS |

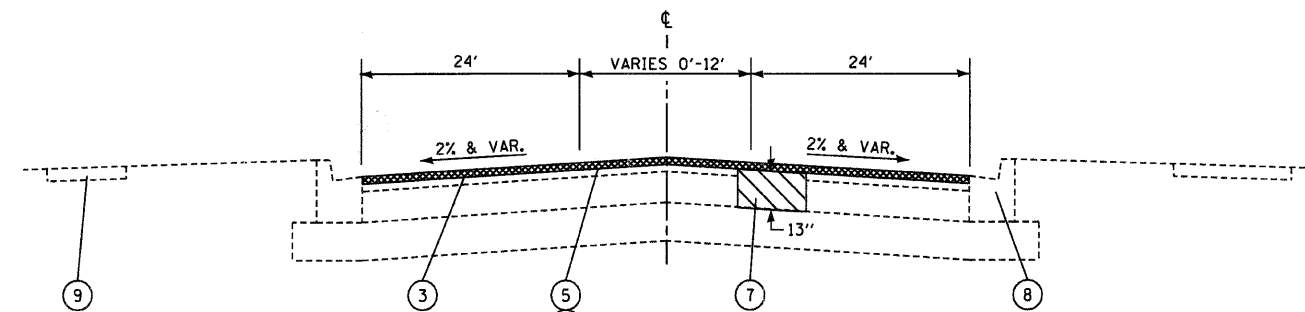




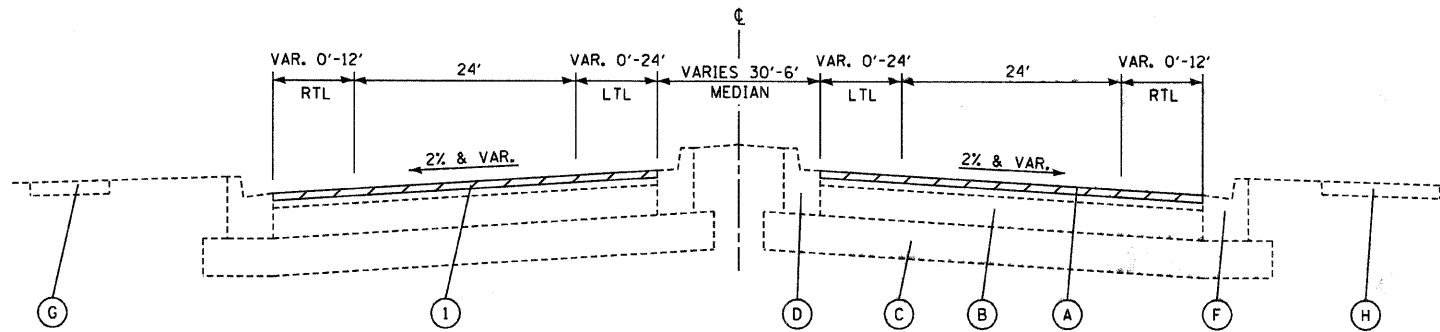




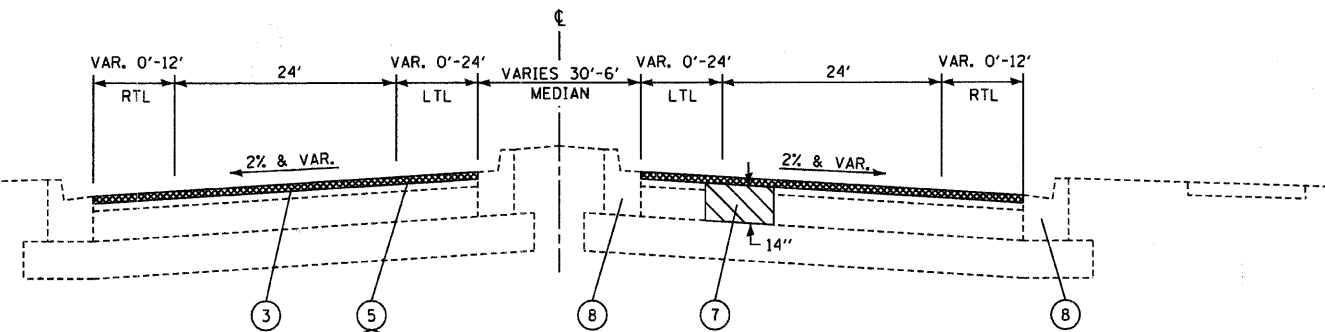
CH 3 FERRY ROAD  
RIVER ROAD TO WEST BRANCH - DUPAGE RIVER  
STA. 99+06 TO STA. 116+16  
EXISTING TYPICAL SECTION



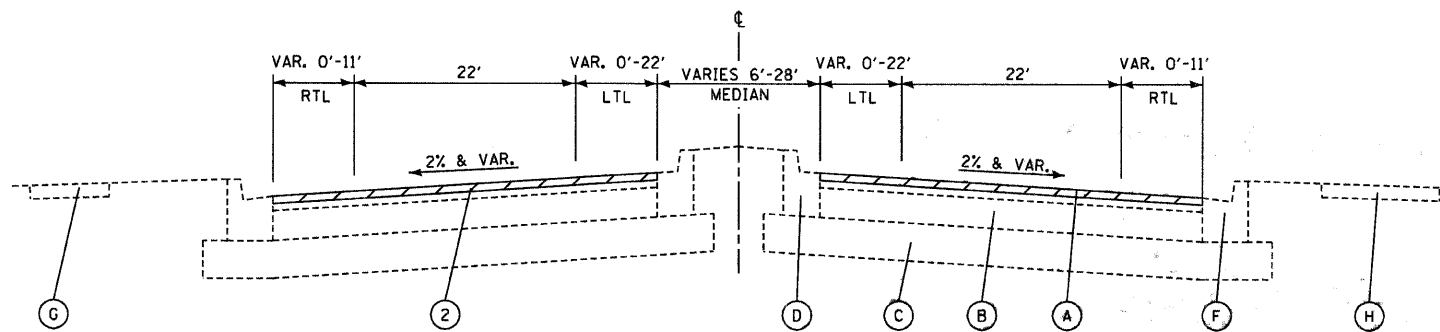
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RIVER ROAD TO WEST BRANCH - DUPAGE RIVER  
STA. 99+06 TO STA. 116+16  
PROPOSED TYPICAL SECTION



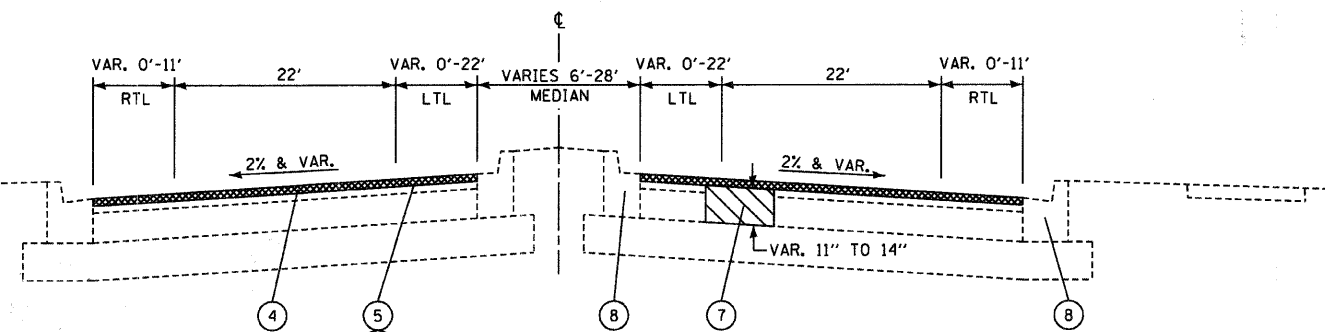
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WEST BRANCH - DUPAGE RIVER TO MILL STREET  
STA. 116+16 TO STA. 166+98  
EXISTING TYPICAL SECTION



CH 3 FERRY ROAD  
WEST BRANCH - DUPAGE RIVER TO MILL STREET  
STA. 116+16 TO STA. 166+98  
PROPOSED TYPICAL SECTION



CH 53 DIEHL ROAD  
WINFIELD ROAD TO MILL STREET  
STA. 88+66 TO STA. 118+95  
EXISTING TYPICAL SECTION



CH 53 DIEHL ROAD  
WINFIELD ROAD TO MILL STREET  
STA. 88+66 TO STA. 118+95  
PROPOSED TYPICAL SECTION

LEGEND

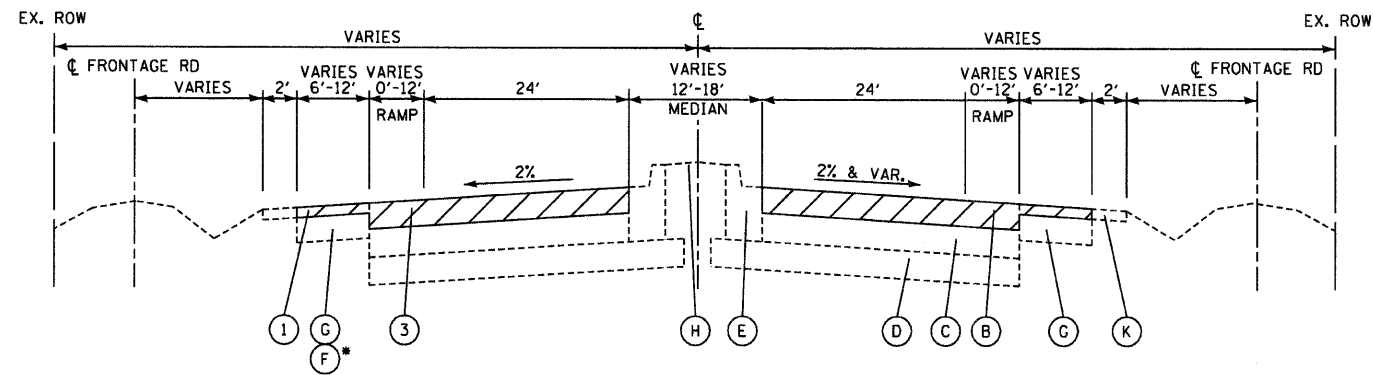
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|--|---|
| (A) EXISTING HMA BINDER & SURFACE (VARIES 3" TO 4")    | (1) HMA SURFACE REMOVAL, 1-1/2"   |
| (B) EXISTING HMA OR P.C.C. BASE COURSE                 | (2) HMA SURFACE REMOVAL, 1-3/4"   |
| (C) EXISTING AGGREGATE SUBGRADE                        | (3) HMA SURFACE COURSE, MIX "D", N70, 1-1/2"                                      |
| (D) EXISTING COMB. CONCRETE CURB & GUTTER, TYPE B-6.12 | (4) POLY. HMA SURFACE COURSE, MIX "F", N90, 1-3/4"                                |
| (E) EXISTING COMB. CONCRETE CURB & GUTTER, TYPE B-6.18 | (5) BITUMINOUS MATERIALS (PRIME COAT)   |
| (F) EXISTING COMB. CONCRETE CURB & GUTTER, TYPE B-6.24 | (6) AGGREGATE (PRIME COAT)  |
| (G) EXISTING SIDEWALK                                  | (7) PATCH (AS DIRECTED BY THE ENGINEER)   |
| (H) EXISTING BITUMINOUS PATH                           | (8) COMB. CONC. CURB & GUTTER REMOVAL & REPLACEMENT (AS DIRECTED BY THE ENGINEER) |
|  | (9) SIDEWALK REMOVAL & REPLACEMENT (AS DIRECTED BY THE ENGINEER)                  |

NOTE:

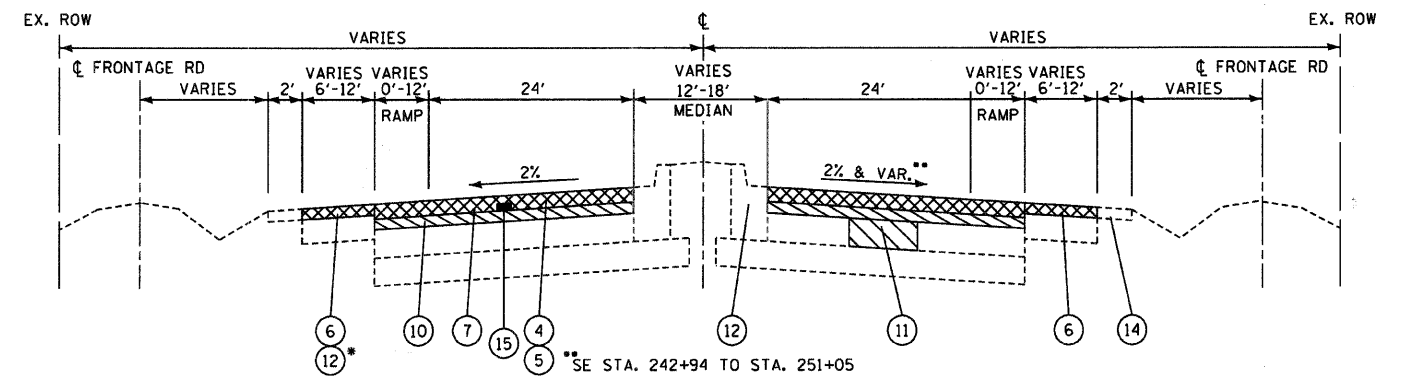
LEVELING BINDER (MIN. 3/4"), IF REQUIRED, SHALL BE LEVELING BINDER (MACHINE METHOD), N70, IL 9.5

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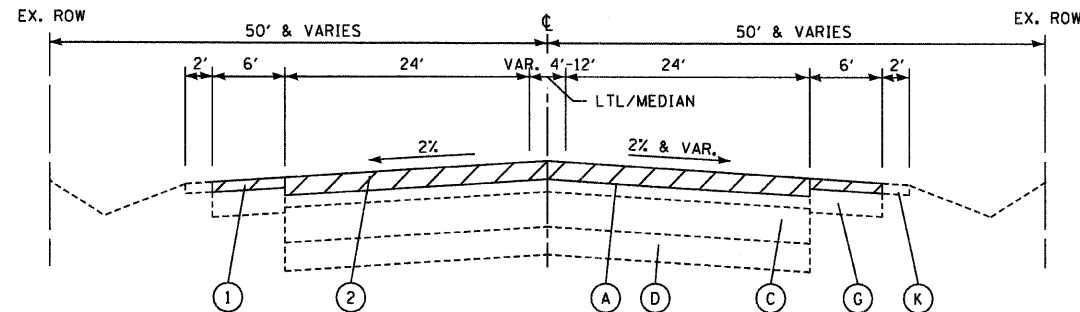
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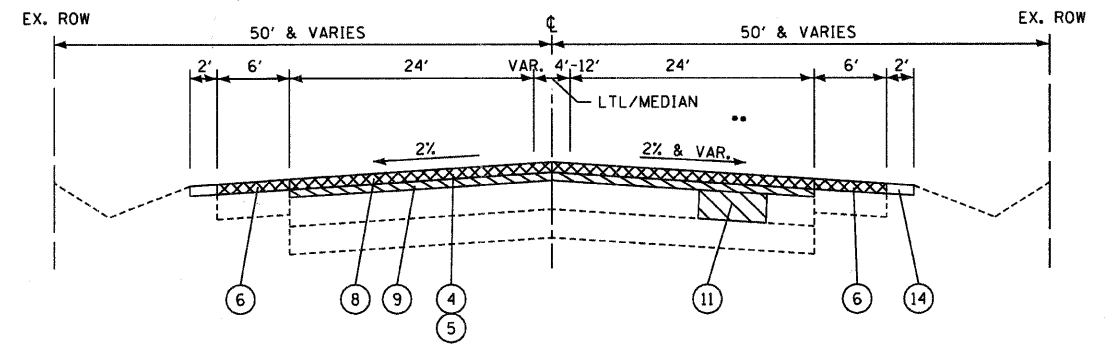
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 97TH STREET TO SOUTH OF N. FRONTAGE ROAD (CHEESE ROAD)  
 STA. 213+19 TO STA. 251+00  
 EXISTING TYPICAL SECTION  
 \* SECTION VARIES FROM BITUMINOUS SHOULDER TO CURB AND GUTTER TYPE B-6.18



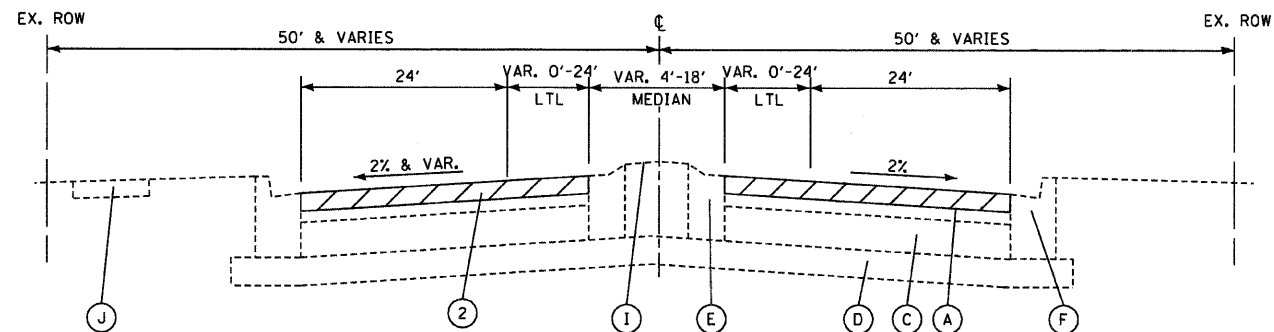
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 97TH STREET TO SOUTH OF N. FRONTAGE ROAD (CHEESE ROAD)  
 STA. 213+19 TO STA. 251+05  
 PROPOSED TYPICAL SECTION  
 \* SECTION VARIES FROM BITUMINOUS SHOULDER TO CURB AND GUTTER TYPE B-6.18



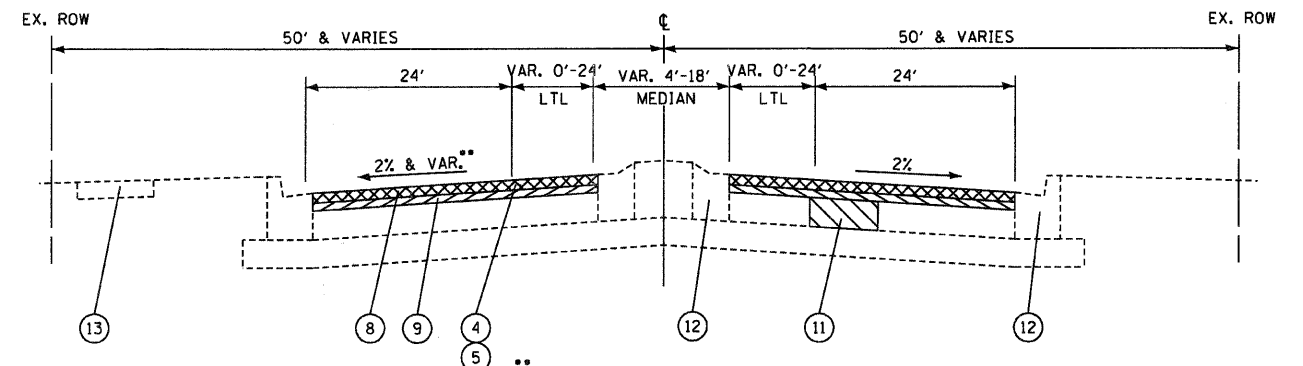
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 SOUTH OF N. FRONTAGE ROAD (CHEESE ROAD) TO SOUTH OF 87TH STREET  
 STA. 251+00 TO STA. 272+25  
 EXISTING TYPICAL SECTION



CH 9 LEMONT ROAD  
 SOUTH OF N. FRONTAGE ROAD (CHEESE ROAD) TO SOUTH OF 87TH STREET  
 STA. 251+05 TO STA. 256+83  
 PROPOSED TYPICAL SECTION



CH 9 LEMONT ROAD  
 SOUTH OF 87TH STREET TO 83RD STREET  
 STA. 272+25 TO STA. 310+76  
 EXISTING TYPICAL SECTION



CH 9 LEMONT ROAD  
 SOUTH OF 87TH STREET TO 83RD STREET  
 STA. 272+25 TO STA. 310+76  
 PROPOSED TYPICAL SECTION

LEGEND

- |  |                                     |  |   |
|--|-------------------------------------|--|---|
| (A) EXISTING HMA BINDER & SURFACE 3-3/4" TO 5"±        | (I) EXISTING MOUNTABLE PAVED MEDIAN | (1) HMA SURFACE REMOVAL, 1-1/2"                                      | (10) LEVELING BINDER (MACHINE METHOD), N70, 1-1/2"                                  |
| (B) EXISTING HMA BINDER & SURFACE 3-1/2"±              | (J) EXISTING SIDEWALK               | (2) HMA SURFACE REMOVAL, 2-1/2"                                      | (11) PATCH (AS DIRECTED BY THE ENGINEER), 9"-11"                                    |
| (C) EXISTING P.C.C. BASE COURSE 9"±                    | (K) EXISTING AGGREGATE SHOULDER     | (3) HMA SURFACE REMOVAL, 3-1/2"                                      | (12) COMB. CONC. CURB & GUTTER REMOVAL & REPLACEMENT (AS DIRECTED BY THE ENGINEER)  |
| (D) EXISTING AGGREGATE SUBGRADE                        |                                     | (4) BITUMINOUS MATERIALS (PRIME COAT)                                | (13) SIDEWALK REMOVAL & REPLACEMENT (AS DIRECTED BY THE ENGINEER)                   |
| (E) EXISTING COMB. CONCRETE CURB & GUTTER, TYPE B-6.12 |                                     | (5) AGGREGATE (PRIME COAT)   | (14) AGGREGATE SHOULDER (WEDGE) REMOVAL & REPLACEMENT (AS DIRECTED BY THE ENGINEER) |
| (F) EXISTING COMB. CONCRETE CURB & GUTTER, TYPE B-6.18 |                                     | (6) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1.5"               | (15) STRIP REFLECTIVE CRACK CONTROL TREATMENT                                       |
| (G) EXISTING HMA SHOULDER                              |                                     | (7) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 2"     |   |
| (H) EXISTING GRASS MEDIAN                              |                                     | (8) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1-3/4" |   |
|  |                                     | (9) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4" |   |

PRINTED DATE: 10/16/2009  
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 Plotter: HP DesignJet 5000

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DUPAGE COUNTY DOT  
 JACK T. KNUEPFER ADMIN BLDG.  
 421 N. COUNTY FARM ROAD  
 WHEATON, IL 60187

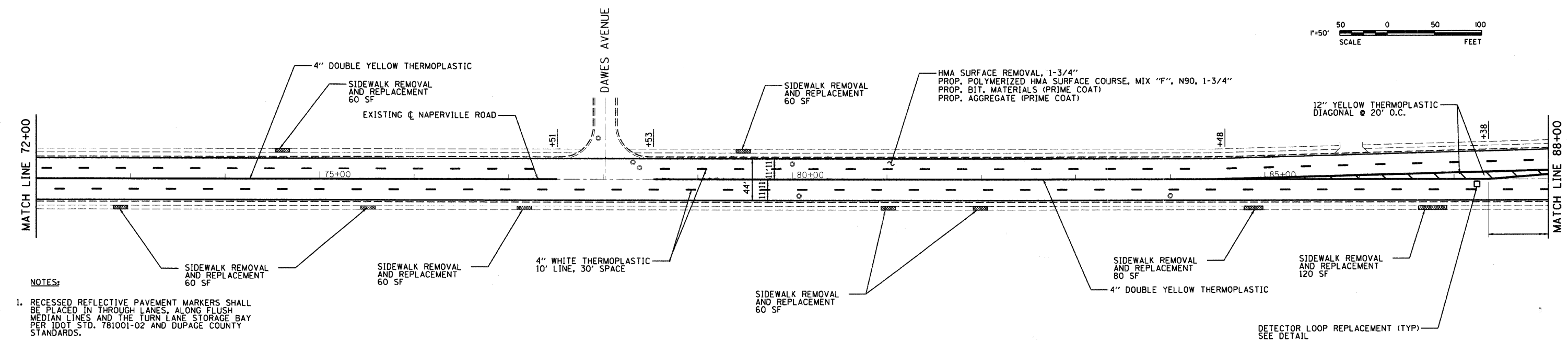
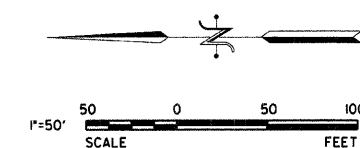
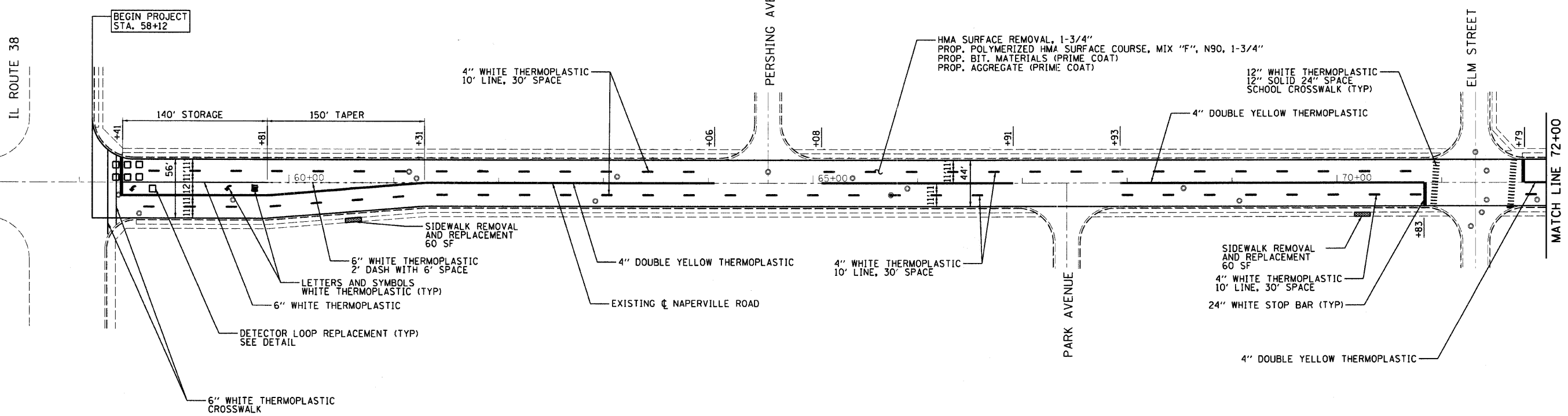
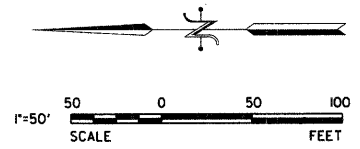
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| ESI PROJECT NO. 09-280       | ENGR. MR  |
| FILE: 08 Typical Sections 05 | TECH. DN  |
| DATE: 06-25-09               | QA/QC. JS |

**DUPAGE LAPP PROJECT**  
**CONTRACT NO. 63335**

**TYPICAL SECTIONS**  
**LEMONT ROAD- 97TH STREET TO 83TH STREET**





- NOTES:**
1. RECESSED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN THROUGH LANES, ALONG FLUSH MEDIAN LINES AND THE TURN LANE STORAGE BAY PER IDOT STD. 781001-02 AND DUPAGE COUNTY STANDARDS.
  2. RE-ESTABLISHING EXISTING PAINTED AREAS, SUCH AS MEDIAN NOSES, SHALL BE PAID FOR AS THERMOPLASTIC PAVEMENT MARKINGS, LETTERS AND SYMBOLS AND PERFORMED AS DIRECTED BY THE ENGINEER.
  3. BARRICADES OR TEMPORARY RAMPS TO BE USED BY THE CONTRACTOR AROUND UTILITY ADJUSTMENTS AS DIRECTED BY THE ENGINEER. COST INCIDENTAL TO HMA SURFACE REMOVAL.
  4. THE CONTRACTOR SHALL REPLACE DETECTOR LOOPS AS DIRECTED BY THE ENGINEER.

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DUPAGE COUNTY DOT  
 JACK T. KNUEPFER ADMIN BLDG.  
 421 N. COUNTY FARM ROAD  
 WHEATON, IL 60187

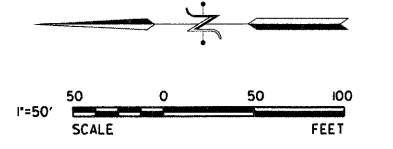
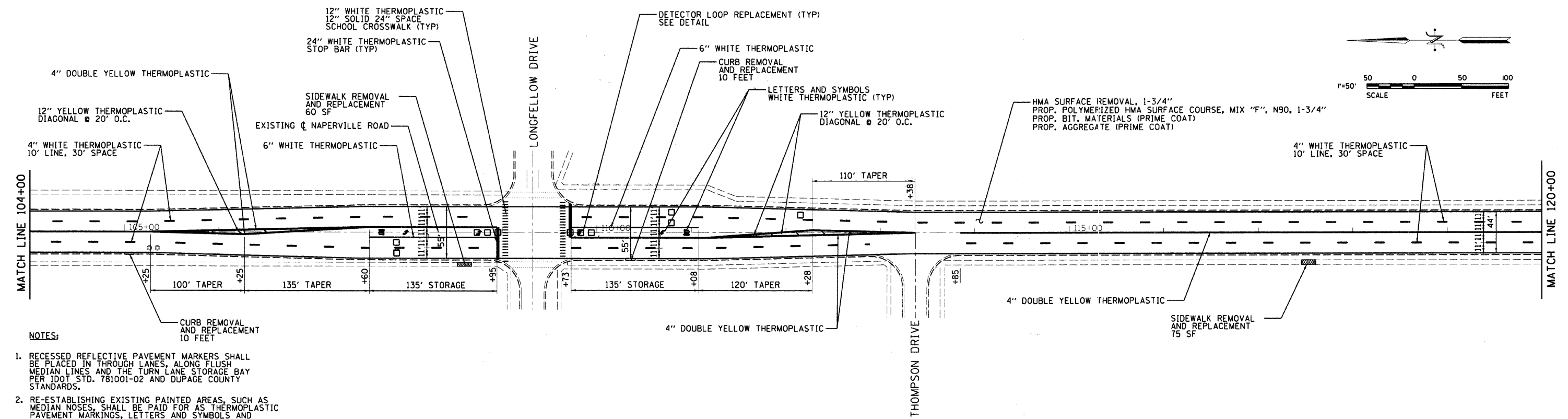
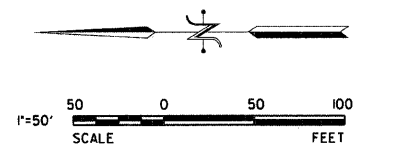
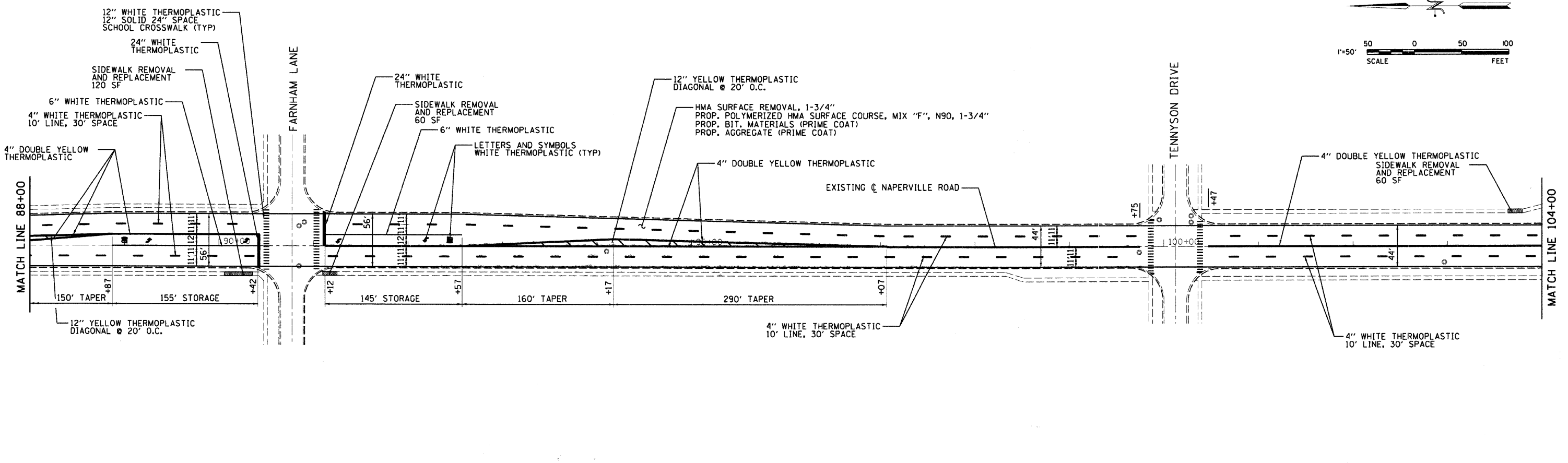
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ESI PROJECT NO. 09-280  
 FILE: 09 Naper North 01  
 DATE: 06-25-09  
 ENGR: MR  
 TECH: DN  
 QA/QC: JS

**DUPAGE LAPP PROJECT  
 CONTRACT NO. 63335**

**NAPERVILLE ROAD  
 IL 38 TO IL 56**

DRAWING NO.  
**9**



- NOTES:**
1. RECESSED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN THROUGH LANES, ALONG FLUSH MEDIAN LINES AND THE TURN LANE STORAGE BAY PER IDOT STD. 781001-02 AND DUPAGE COUNTY STANDARDS.
  2. RE-ESTABLISHING EXISTING PAINTED AREAS, SUCH AS MEDIAN NOSES, SHALL BE PAID FOR AS THERMOPLASTIC PAVEMENT MARKINGS, LETTERS AND SYMBOLS AND PERFORMED AS DIRECTED BY THE ENGINEER.
  3. BARRICADES OR TEMPORARY RAMPS TO BE USED BY THE CONTRACTOR AROUND UTILITY ADJUSTMENTS AS DIRECTED BY THE ENGINEER. COST INCIDENTAL TO HMA SURFACE REMOVAL.
  4. THE CONTRACTOR SHALL REPLACE DETECTOR LOOPS AS DIRECTED BY THE ENGINEER.

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 scale: 1/8"=1'-0"  
 sheet: 10 of 10

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 ILLINOIS DEPARTMENT OF PROFESSIONAL REGULATION REGISTRATION #184-003885

DUPAGE COUNTY DOT  
 JACK T. KNUEPFER ADMIN BLDG.  
 421 N. COUNTY FARM ROAD  
 WHEATON, IL 60187

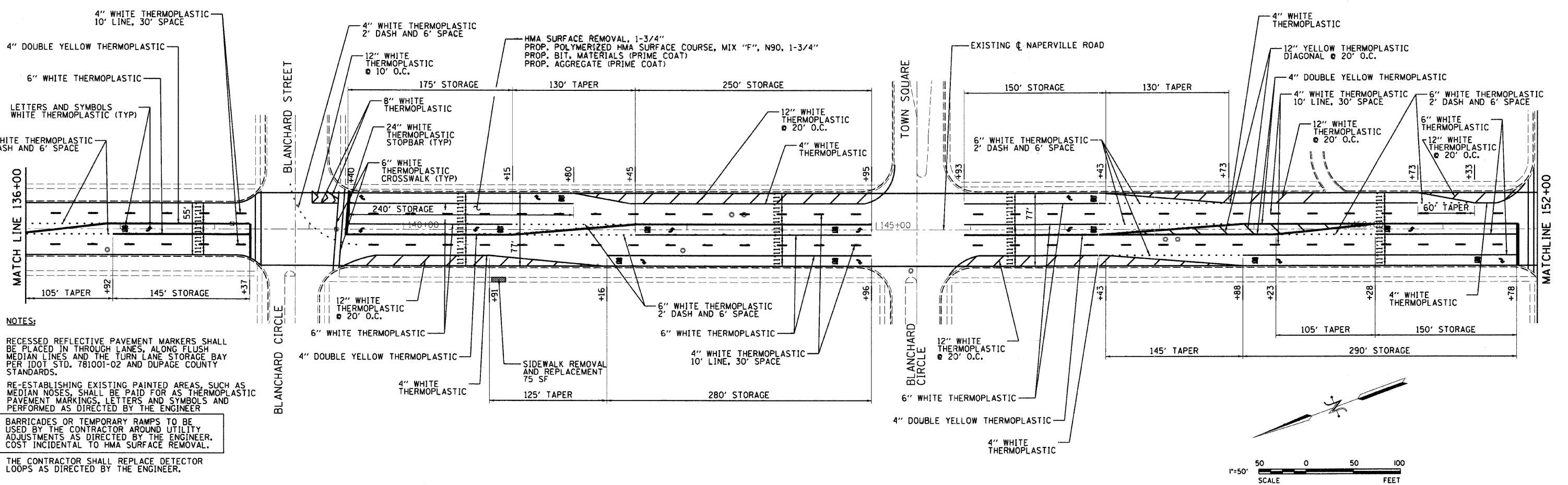
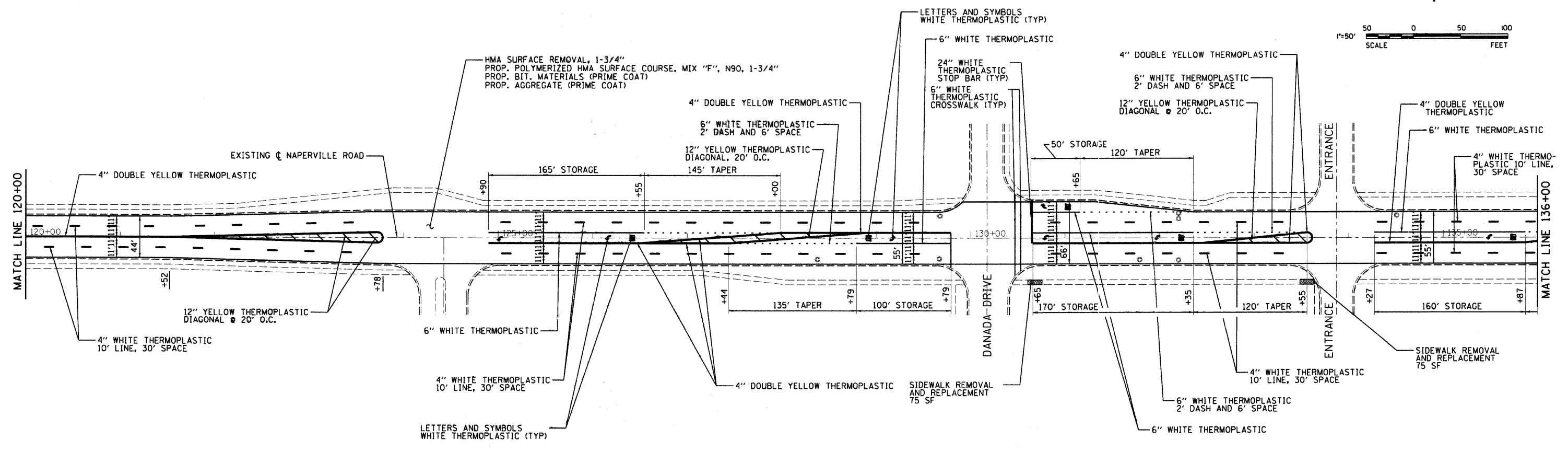
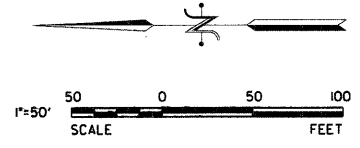
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| ESI PROJECT NO. 09-280       | ENGR. MR |
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| DATE: 06-25-09               | TECH: JJ |
|                              | QAQC: JS |

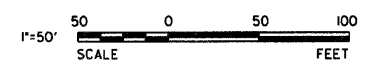
**DUPAGE LAPP PROJECT**  
**CONTRACT NO. 63335**

**NAPERVILLE ROAD**  
 IL 38 TO IL 56

DRAWING NO.  
**10**



- NOTES:**
1. RECESSED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN THROUGH LANES, ALONG FLUSH MEDIAN LINES AND THE TURN LANE STORAGE BAY PER IDOT STD. 781001-02 AND DUPAGE COUNTY STANDARDS.
  2. RE-ESTABLISHING EXISTING PAINTED AREAS, SUCH AS MEDIAN NOSES, SHALL BE PAID FOR AS THERMOPLASTIC PAVEMENT MARKINGS, LETTERS AND SYMBOLS AND PERFORMED AS DIRECTED BY THE ENGINEER.
  3. BARRICADES OR TEMPORARY RAMPS TO BE USED BY THE CONTRACTOR AROUND UTILITY ADJUSTMENTS AS DIRECTED BY THE ENGINEER. COST INCIDENTAL TO HMA SURFACE REMOVAL.
  4. THE CONTRACTOR SHALL REPLACE DETECTOR LOOPS AS DIRECTED BY THE ENGINEER.



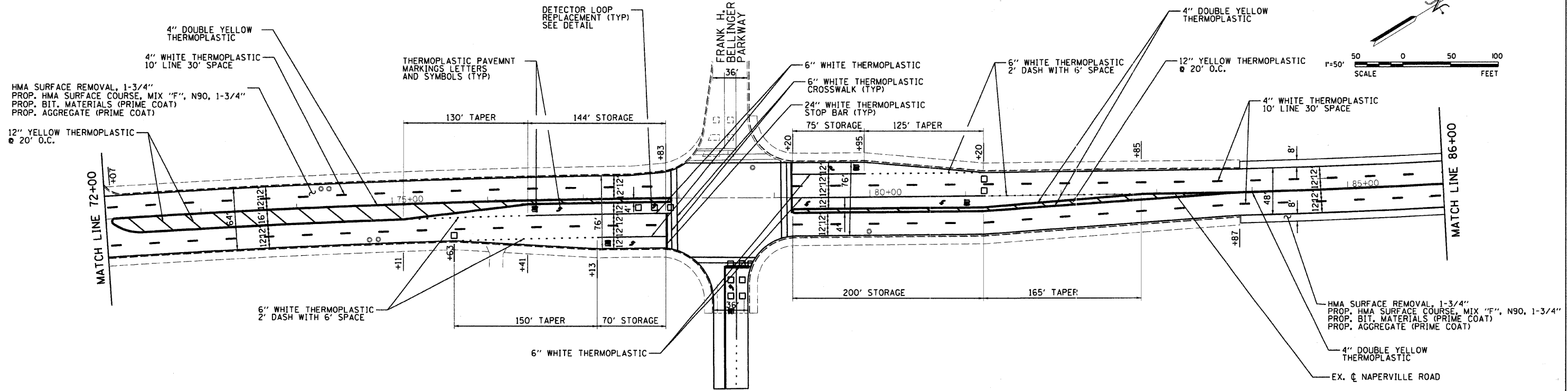
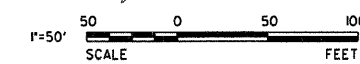
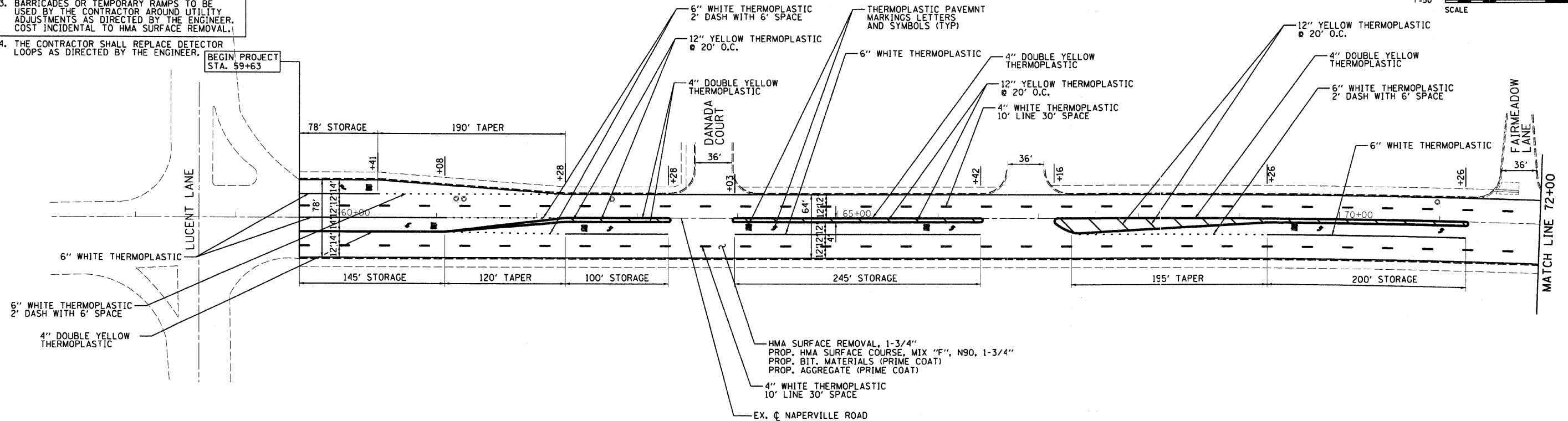
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**NOTES:**

1. RECESSED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN THROUGH LANES, ALONG FLUSH MEDIAN LINES AND THE TURN LANE STORAGE BAY PER IDOT STD. 781001-02 AND DUPAGE COUNTY STANDARDS.
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4. THE CONTRACTOR SHALL REPLACE DETECTOR LOOPS AS DIRECTED BY THE ENGINEER.



PRINTED DATE: 07/29/2009  
 PROJECT NO: 09-280  
 DRAWING NO: 13  
 CONTRACT NO: 63335  
 COUNTY: DUPAGE  
 SHEET: 13 OF 13  
 SCALE: AS SHOWN  
 DATE: 06-25-09  
 DESIGNED BY: JN  
 CHECKED BY: JJ  
 IN CHARGE: JS  
 PROJECT: DUPAGE LAPP PROJECT  
 CONTRACT NO. 63335  
 SHEET: 13 OF 13  
 DATE: 06-25-09

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 NAPERVILLE, IL 60563  
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 ILLINOIS DEPARTMENT OF PROFESSIONAL REGULATION REGISTRATION #194-003886

DUPAGE COUNTY DOT  
 JACK T. KNUEPFER ADMIN BLDG.  
 421 N. COUNTY FARM ROAD  
 WHEATON, IL 60187

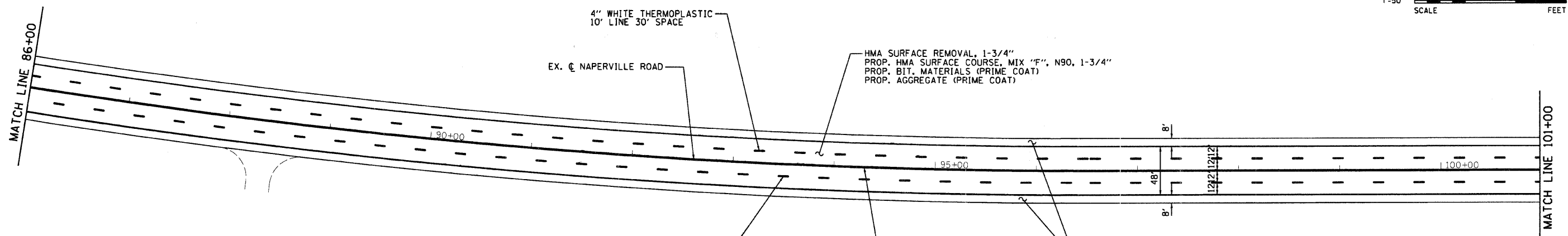
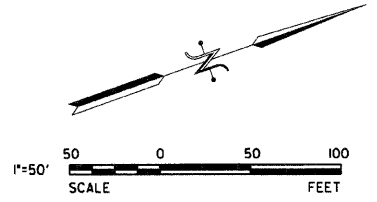
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ESI PROJECT NO. 09-280  
 FILE: 13 Naper South 01  
 DATE: 06-25-09  
 ENGR: MR  
 TECH: DN  
 TECH: JJ  
 QA/QC: JS

**DUPAGE LAPP PROJECT**  
**CONTRACT NO. 63335**

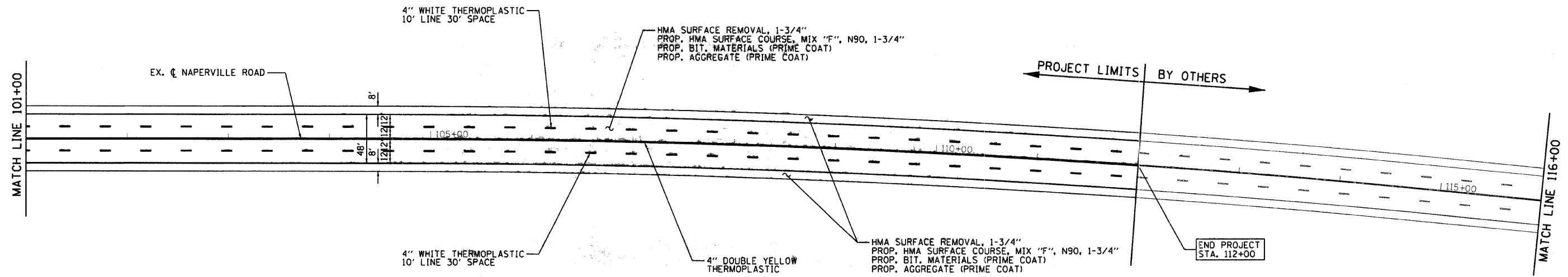
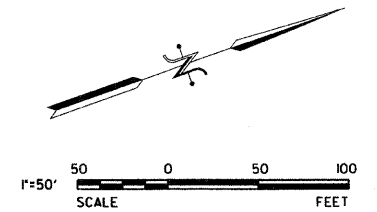
**NAPERVILLE ROAD**  
 LUCENT LANE TO IL 56

DRAWING NO.  
**13**



**NOTES:**

1. RECESSED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN THROUGH LANES, ALONG FLUSH MEDIAN LINES AND THE TURN LANE STORAGE BAY PER IDOT STD. 781001-02 AND DUPAGE COUNTY STANDARDS.
2. RE-ESTABLISHING EXISTING PAINTED AREAS, SUCH AS MEDIAN NOSES, SHALL BE PAID FOR AS THERMOPLASTIC PAVEMENT MARKINGS, LETTERS AND SYMBOLS AND PERFORMED AS DIRECTED BY THE ENGINEER.
3. BARRICADES OR TEMPORARY RAMPS TO BE USED BY THE CONTRACTOR AROUND UTILITY ADJUSTMENTS AS DIRECTED BY THE ENGINEER. COST INCIDENTAL TO HMA SURFACE REMOVAL.
4. THE CONTRACTOR SHALL REPLACE DETECTOR LOOPS AS DIRECTED BY THE ENGINEER.



PRINTED DATE: 07/16/2009  
 PROJECT: naperville\_south\_02.dwg  
 PLOTTER: HP DesignJet 5000

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 (630) 420-1700  
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DUPAGE COUNTY DOT  
 JACK T. KNUEPFER ADMIN BLDG.  
 421 N. COUNTY FARM ROAD  
 WHEATON, IL 60187

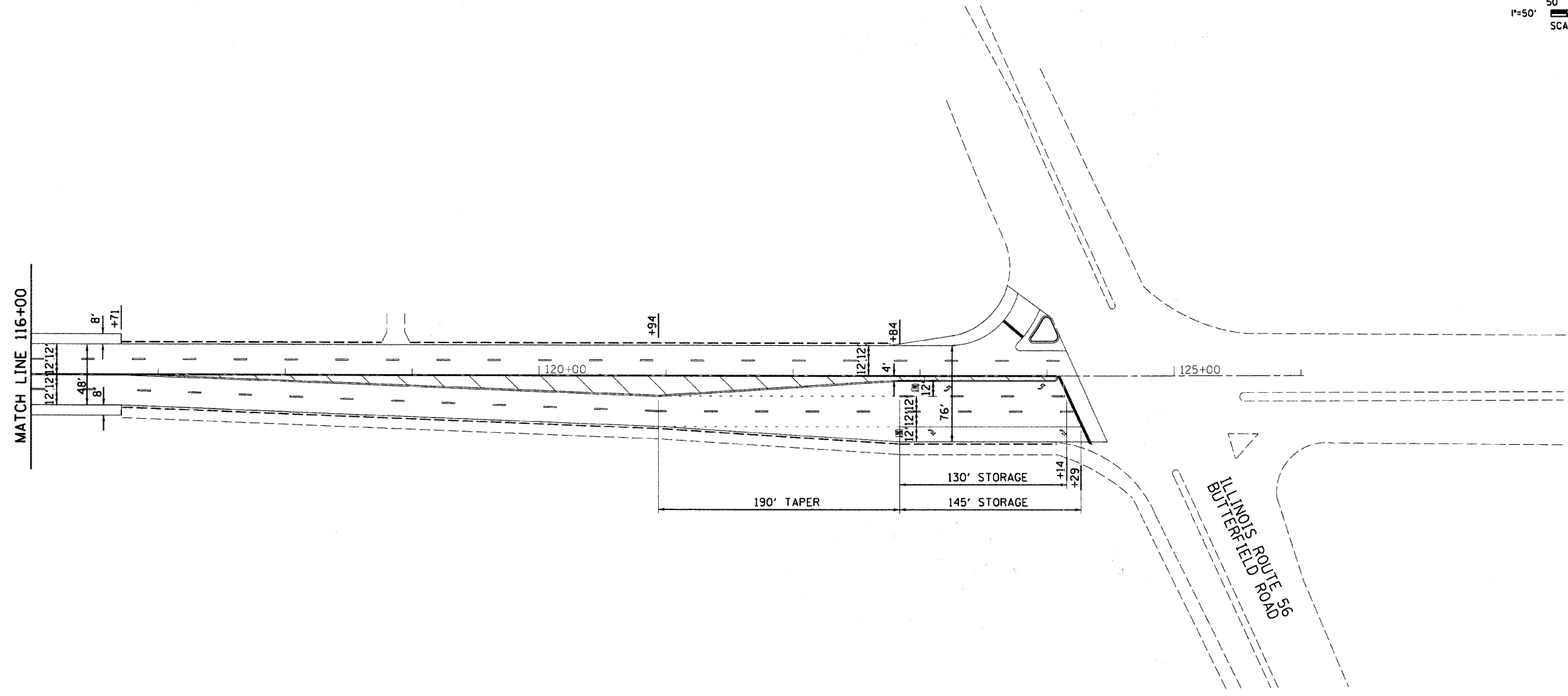
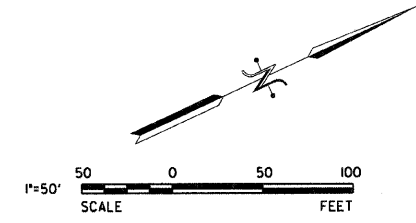
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| ESI PROJECT NO. 09-280       | ENGR: MR |
| FILE: 14 Naperville South 02 | TECH: DN |
| DATE: 06-25-09               | TECH: JJ |
|                              | QAQC: JS |

**DUPAGE LAPP PROJECT  
 CONTRACT NO. 63335**

**NAPERVILLE ROAD  
 LUCENT LANE TO IL 56**

DRAWING NO.  
**14**



**NOTES:**

1. RECESSED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN THROUGH LANES, ALONG FLUSH MEDIAN LINES AND THE TURN LANE STORAGE BAY PER IDOT STD. 781001-02 AND DUPAGE COUNTY STANDARDS.
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4. THE CONTRACTOR SHALL REPLACE DETECTOR LOOPS AS DIRECTED BY THE ENGINEER.

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 User: jknuepfer

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 1379 N. MILL STREET, SUITE 100  
 NAPERVILLE, IL 60563  
 (630) 420-1700  
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 ILLINOIS DEPARTMENT OF PROFESSIONAL REGULATION REGISTRATION #184-003685

**DUPAGE COUNTY DOT**  
 JACK T. KNUEPFER ADMIN BLDG.  
 421 N. COUNTY FARM ROAD  
 WHEATON, IL 60187

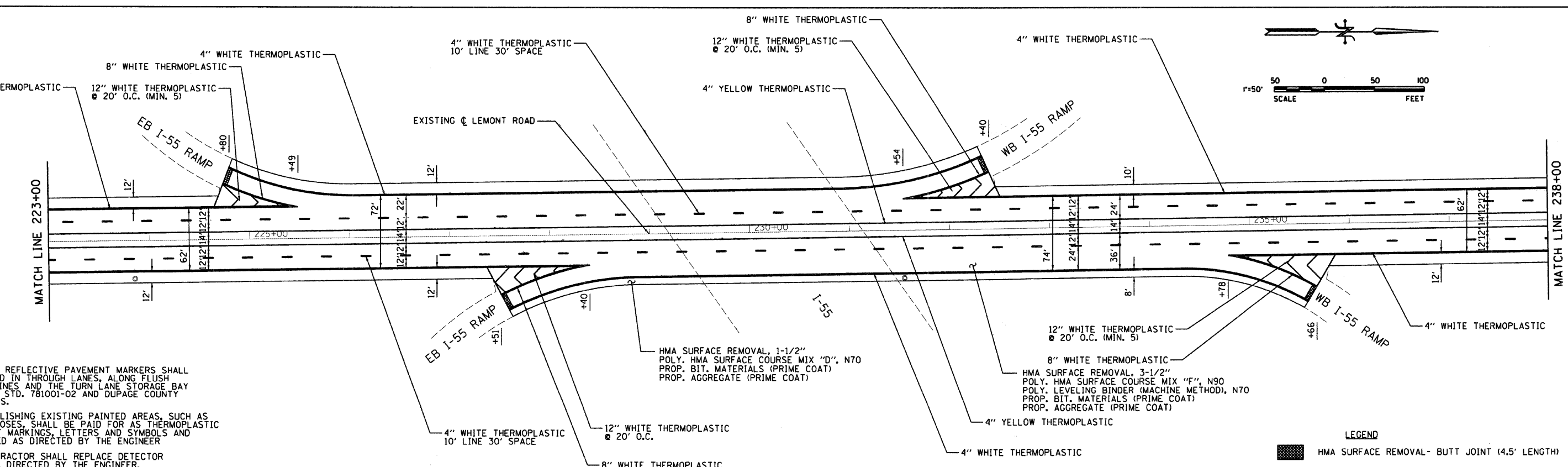
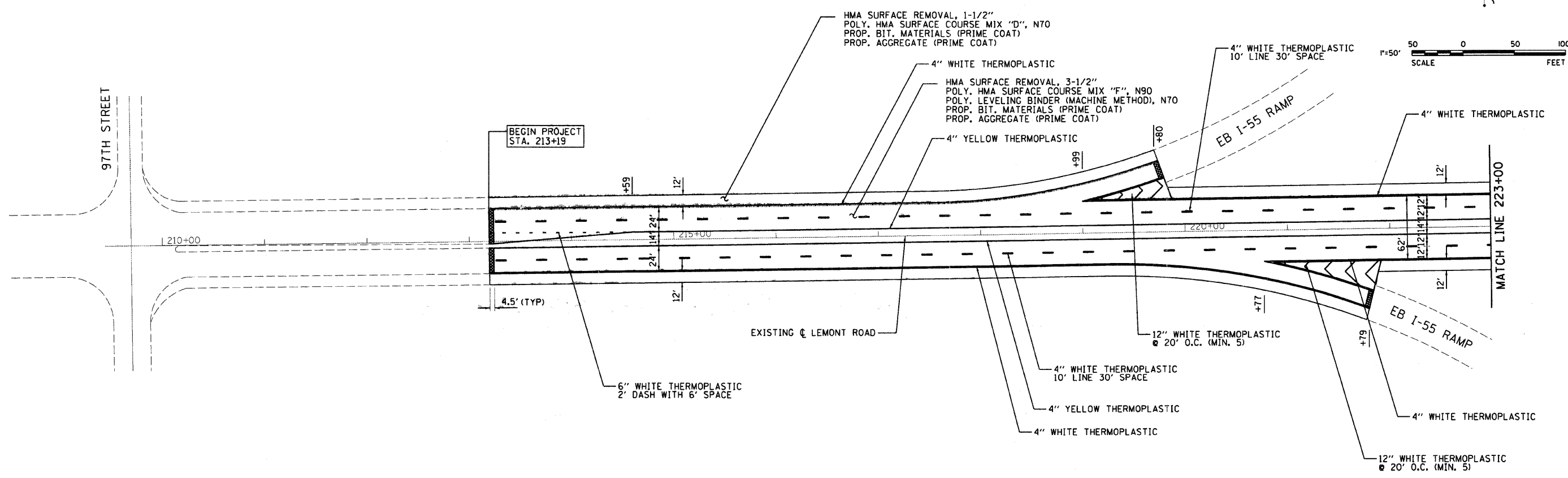
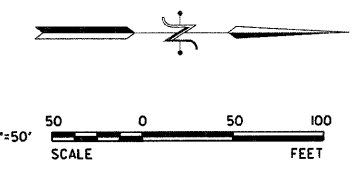
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| ESI PROJECT NO. 09-280       | ENGR: MR             |
| FILE: 15 Naperville South 03 | TECH: DN<br>TECH: JJ |
| DATE: 06-25-09               | QA/QC: JS            |

**DUPAGE LAPP PROJECT**  
**CONTRACT NO. 63335**

**NAPERVILLE ROAD**  
**LUCENT LANE TO IL 56**

DRAWING NO.  
**15**



- NOTES:**
1. RECESSED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN THROUGH LANES, ALONG FLUSH MEDIAN LINES AND THE TURN LANE STORAGE BAY PER IDOT STD. 781001-02 AND DUPAGE COUNTY STANDARDS.
  2. RE-ESTABLISHING EXISTING PAINTED AREAS, SUCH AS MEDIAN NOSES, SHALL BE PAID FOR AS THERMOPLASTIC PAVEMENT MARKINGS, LETTERS AND SYMBOLS AND PERFORMED AS DIRECTED BY THE ENGINEER.
  3. THE CONTRACTOR SHALL REPLACE DETECTOR LOOPS AS DIRECTED BY THE ENGINEER.

PRINTED DATE: 10/16/2009  
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 User: jkn  
 Plot: 16\_lemont\_road.dwg  
 Plot Date: 10/16/2009 10:16:29 AM  
 Plot Scale: 1/8" = 1'-0"  
 Plot Size: 11.00 x 17.00  
 Plot Orientation: Landscape  
 Plot Color: Black  
 Plot Lineweight: 0.20  
 Plot Linetype: Solid  
 Plot Font: Arial, 10  
 Plot Title: 16\_lemont\_road.dwg  
 Plot User: jkn

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 1970 N. MILL STREET, SUITE 100  
 NAPERVILLE, IL 60563  
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DUPAGE COUNTY DOT  
 JACK T. KNUEPFER ADMIN BLDG  
 421 N. COUNTY FARM ROAD  
 WHEATON, IL 60187

| NO. | DATE | DESCRIPTION OF REVISIONS | NO. | DATE | DESCRIPTION OF REVISIONS |
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| ESI PROJECT NO. 09-280  | ENGR: MR  |
| FILE: 16 Lemont Road 01 | TECH: DN  |
| DATE: 06-25-09          | QA/QC: JS |

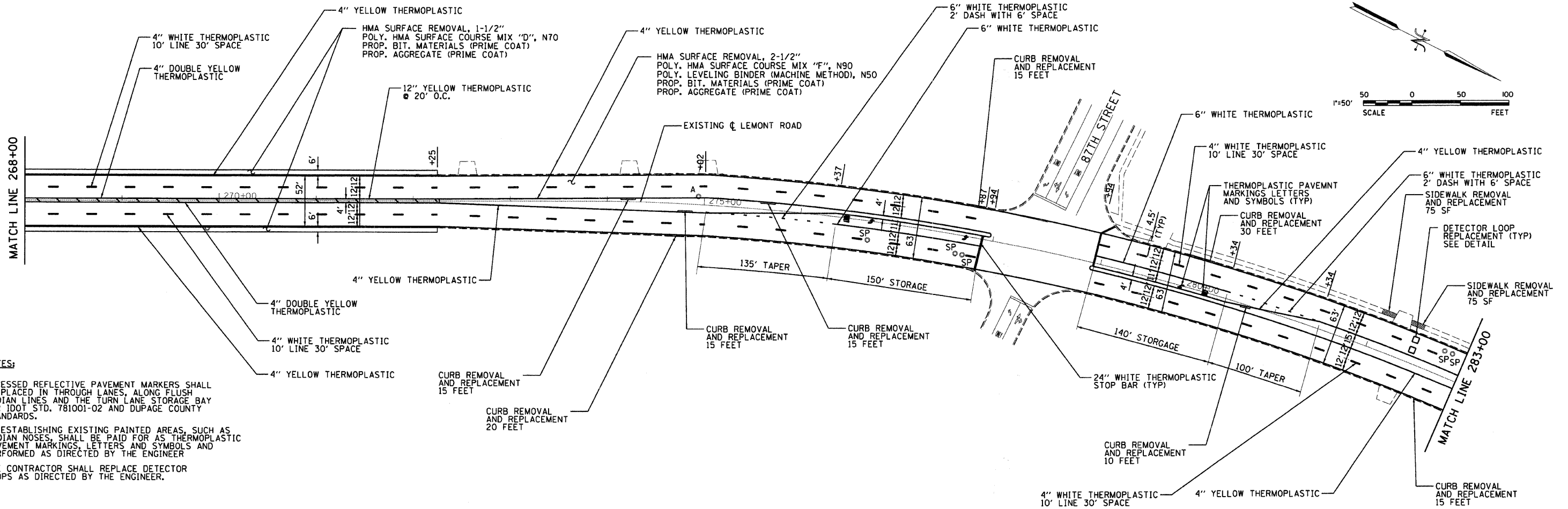
**DUPAGE LAPP PROJECT**  
**CONTRACT NO. 63335**

**LEMONT ROAD**  
 83RD STREET TO 97TH STREET

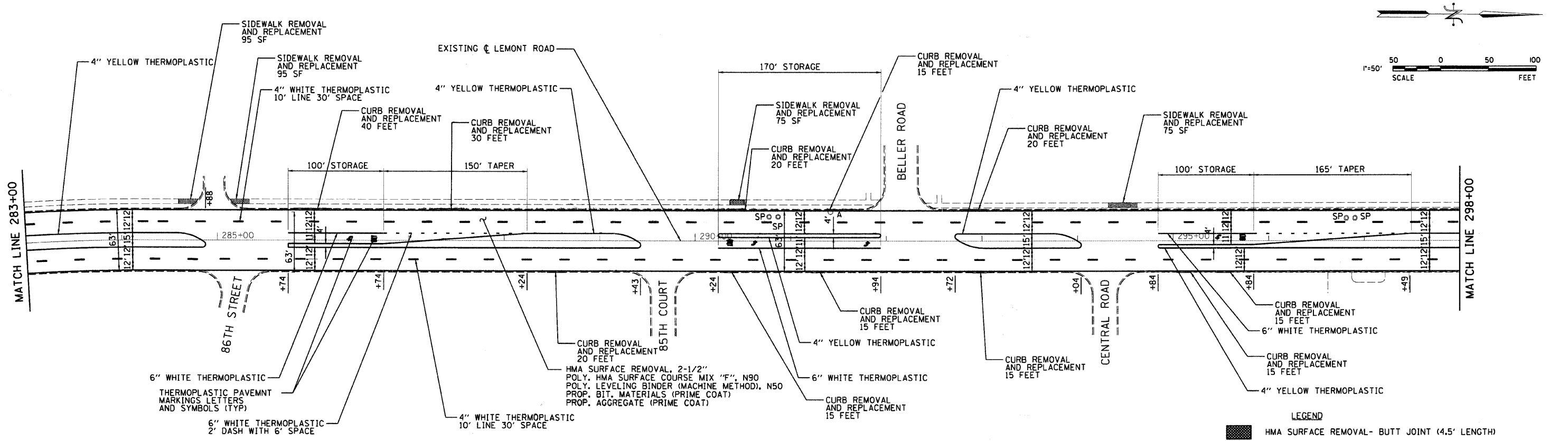
DRAWING NO.  
**16**







- NOTES:**
1. RECESSED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN THROUGH LANES, ALONG FLUSH MEDIAN LINES AND THE TURN LANE STORAGE BAY PER IDOT STD. 781001-02 AND DUPAGE COUNTY STANDARDS.
  2. RE-ESTABLISHING EXISTING PAINTED AREAS, SUCH AS MEDIAN NOSES, SHALL BE PAID FOR AS THERMOPLASTIC PAVEMENT MARKINGS, LETTERS AND SYMBOLS AND PERFORMED AS DIRECTED BY THE ENGINEER
  3. THE CONTRACTOR SHALL REPLACE DETECTOR LOOPS AS DIRECTED BY THE ENGINEER.



**LEGEND**  
 HMA SURFACE REMOVAL- BUTT JOINT (4.5' LENGTH)

PRINTED DATE: 10/16/2009  
 I:\Projects\63335\Drawings\ Lemont Road 03.dwg  
 User: jknuepfer  
 Plot: Lemont Road 03.dwg  
 Country: USA  
 State: IL  
 City: Wheaton  
 Project: 63335  
 Drawing: 18

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 ILLINOIS DEPARTMENT OF PROFESSIONAL REGULATION REGISTRATION #194-003688

DUPAGE COUNTY DOT  
 JACK T. KNUEPFER ADMIN BLDG.  
 421 N. COUNTY FARM ROAD  
 WHEATON, IL 60187

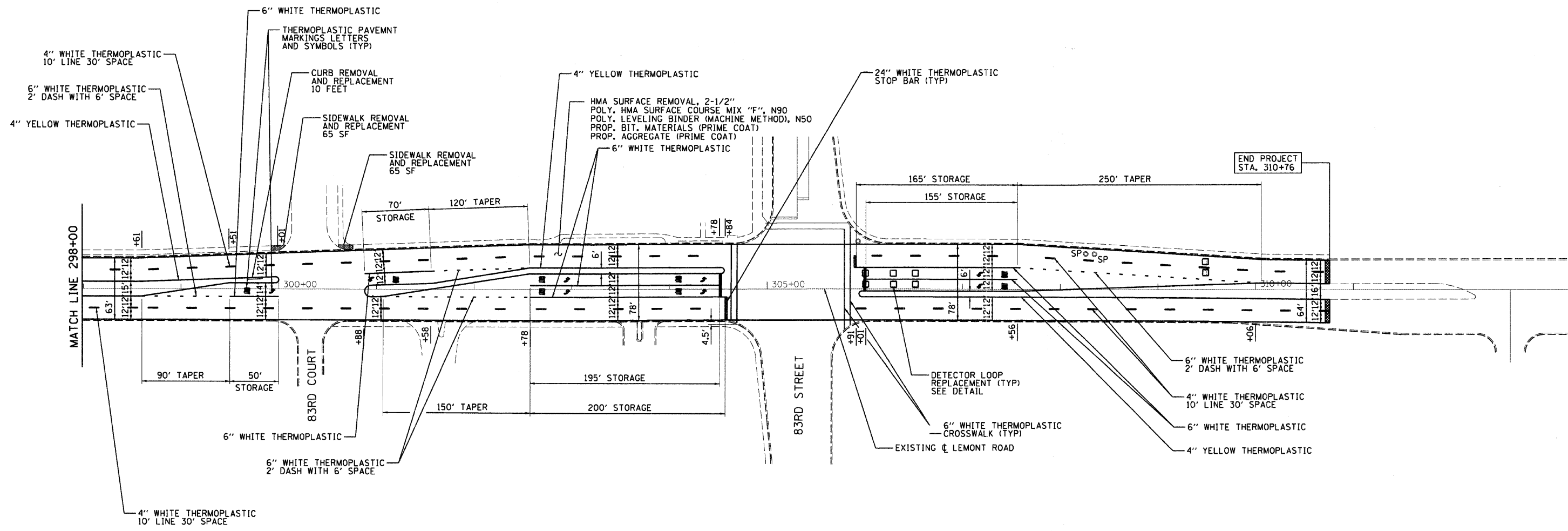
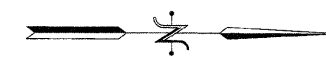
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ESI PROJECT NO. 09-280  
 ENGR. MR  
 FILE: 18 Lemont Road 03  
 TECH: DN  
 DATE: 06-25-09  
 QAQC: JS

**DUPAGE LAPP PROJECT**  
**CONTRACT NO. 63335**

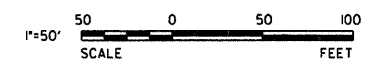
**LEMONT ROAD**  
**83RD STREET TO 97TH STREET**

DRAWING NO. **18**



**NOTES:**

1. RECESSED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN THROUGH LANES, ALONG FLUSH MEDIAN LINES AND THE TURN LANE STORAGE BAY PER IDOT STD. 781001-02 AND DUPAGE COUNTY STANDARDS.
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3. THE CONTRACTOR SHALL REPLACE DETECTOR LOOPS AS DIRECTED BY THE ENGINEER.



PRINTED DATE: 06/16/2009  
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 User: jknuepfer  
 Plot: Lemont Road.dwg  
 Plot Date: 06/16/2009 10:56:53 AM  
 Plot Scale: 1"=50'  
 Plot Size: 11.00 x 17.00  
 Plot Orientation: Landscape  
 Plot Color: Black  
 Plot Lineweight: 0.20  
 Plot Font: Arial, 10  
 Plot Style: Lemont Road.ctb  
 Plot Device: HP DesignJet 5000 Series

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 1979 N. MILL STREET, SUITE 100  
 BAPERVILLE, IL 60563  
 (630) 426-1700  
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 ILLINOIS DEPARTMENT OF PROFESSIONAL REGULATION REGISTRATION #194-003685

DUPAGE COUNTY DOT  
 JACK T. KNUEPFER ADMIN BLDG  
 421 N. COUNTY FARM ROAD  
 WHEATON, IL 60187

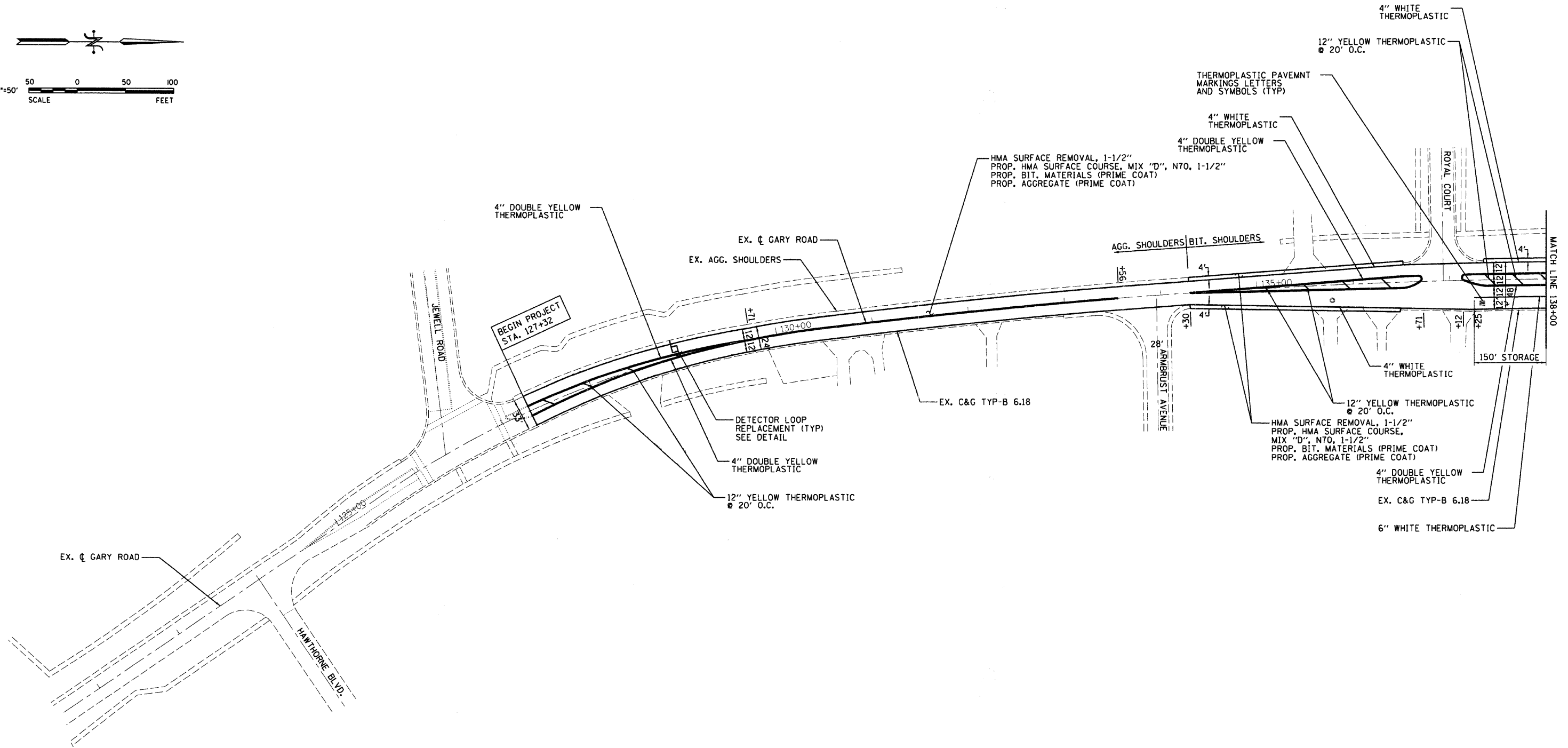
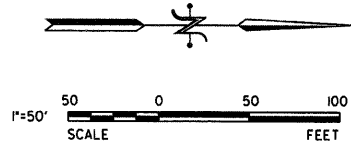
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| ESI PROJECT NO. 09-280  | ENGR: MR  |
| FILE: 19 Lemont Road 04 | TECH: DN  |
| DATE: 06-25-09          | QA/QC: JS |

**DUPAGE LAPP PROJECT  
 CONTRACT NO. 63335**

**LEMONT ROAD  
 83RD STREET TO 97TH STREET**

DRAWING NO.  
**19**



**NOTES:**

1. RECESSED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN THROUGH LANES, ALONG FLUSH MEDIAN LINES AND THE TURN LANE STORAGE BAY PER IDOT STD. 781001-02 AND DUPAGE COUNTY STANDARDS.
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3. BARRICADES OR TEMPORARY RAMPS TO BE USED BY THE CONTRACTOR AROUND UTILITY ADJUSTMENTS AS DIRECTED BY THE ENGINEER. COST INCIDENTAL TO HMA SURFACE REMOVAL.
4. THE CONTRACTOR SHALL REPLACE DETECTOR LOOPS AS DIRECTED BY THE ENGINEER.

PRINTED DATE: 10/16/2009  
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 Plot Orientation: Landscape  
 Plot Color: Black  
 Plot Lineweight: 0.20  
 Plot Linetype: Solid  
 Plot Font: Arial, 10  
 Plot Title: 20 Gary Road 01

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DUPAGE COUNTY DOT  
 JACK T. KNUEPFER ADMIN BLDG.  
 421 N. COUNTY FARM ROAD  
 WHEATON, IL 60187

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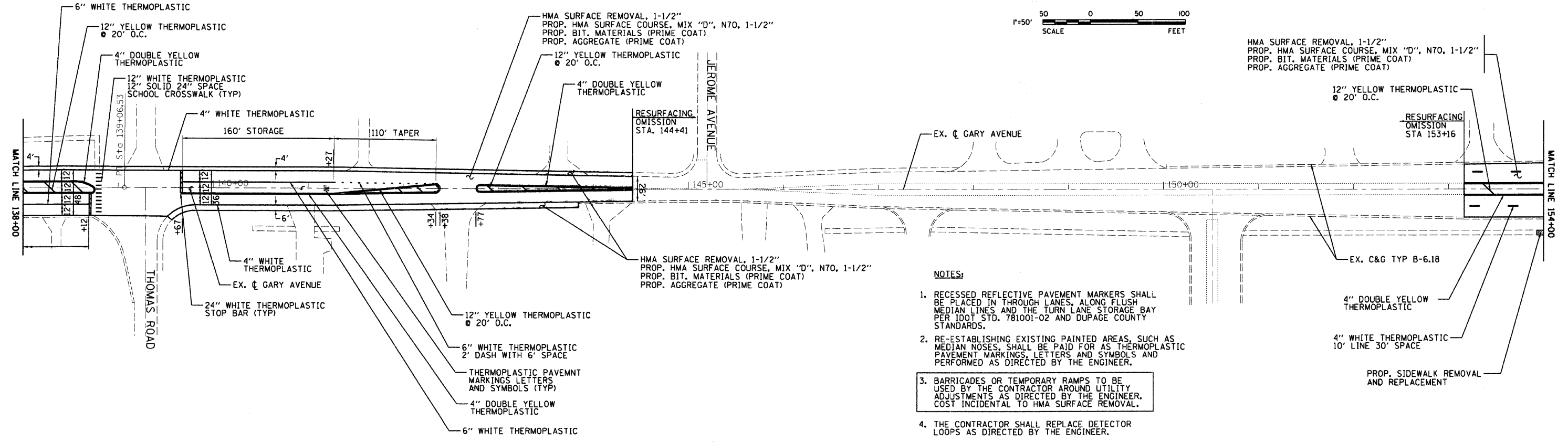
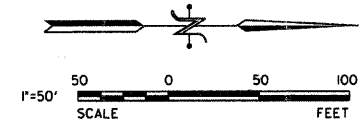
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| ESI PROJECT NO. 09-280 | ENGR: MR             |
| FILE: 20 Gary Road 01  | TECH: DN<br>TECH: JJ |
| DATE: 06-25-09         | QAQC: JS             |

**DUPAGE LAPP PROJECT  
 CONTRACT NO. 63335**

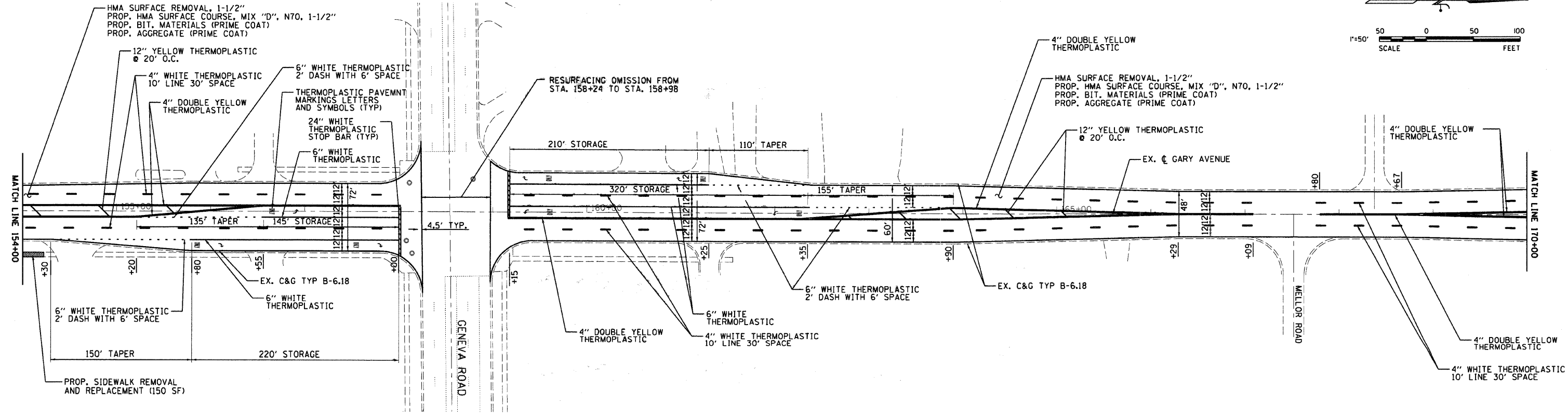
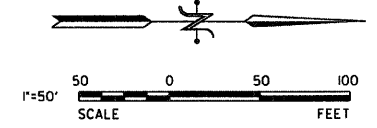
**GARY AVENUE- ROADWAY PLANS  
 JEWELL ROAD TO ST. CHARLES ROAD**

DRAWING NO.  
**20**



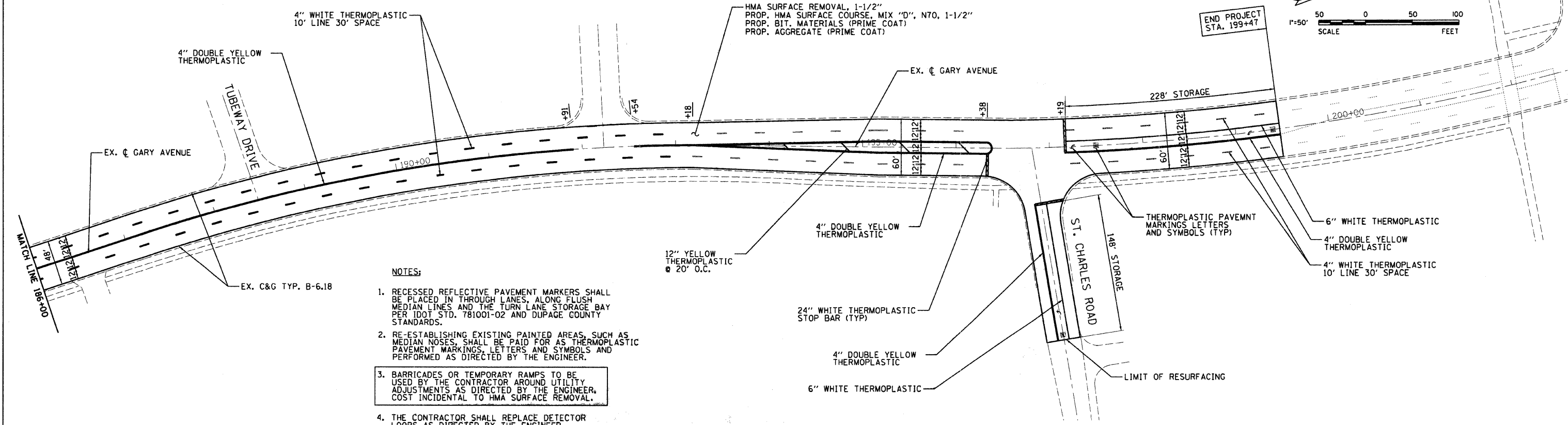
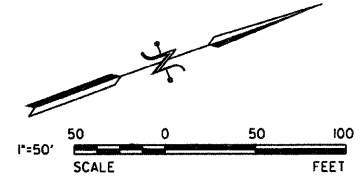
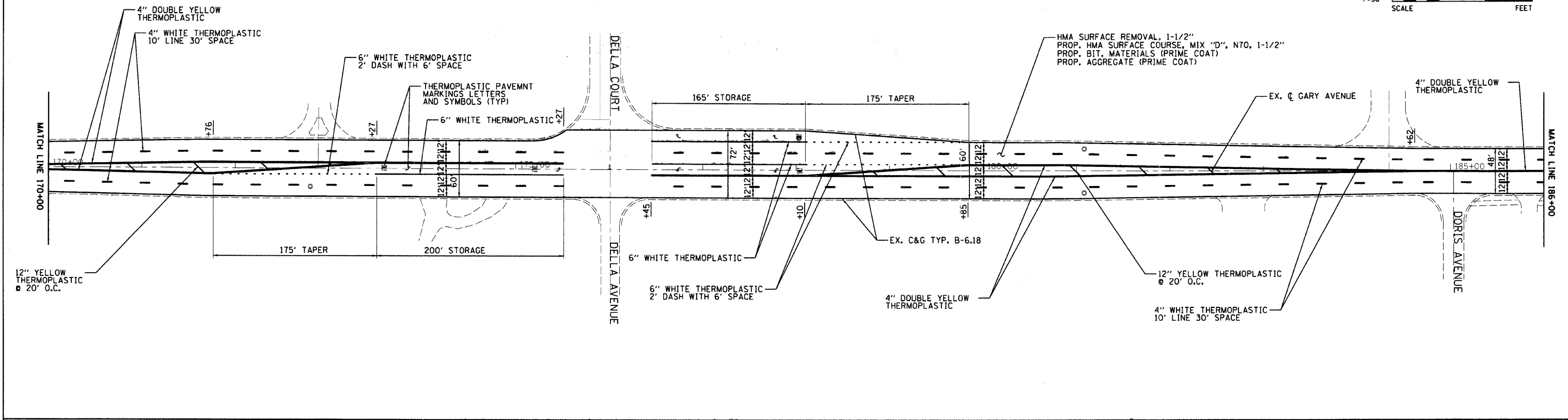
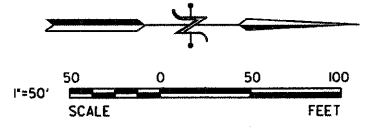


- NOTES:**
- RECESSED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN THROUGH LANES, ALONG FLUSH MEDIAN LINES AND THE TURN LANE STORAGE BAY PER IDOT STD. 781001-02 AND DUPAGE COUNTY STANDARDS.
  - RE-ESTABLISHING EXISTING PAINTED AREAS, SUCH AS MEDIAN NOSES, SHALL BE PAID FOR AS THERMOPLASTIC PAVEMENT MARKINGS, LETTERS AND SYMBOLS AND PERFORMED AS DIRECTED BY THE ENGINEER.
  - BARRICADES OR TEMPORARY RAMPS TO BE USED BY THE CONTRACTOR AROUND UTILITY ADJUSTMENTS AS DIRECTED BY THE ENGINEER. COST INCIDENTAL TO HMA SURFACE REMOVAL.
  - THE CONTRACTOR SHALL REPLACE DETECTOR LOOPS AS DIRECTED BY THE ENGINEER.



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PRINTED DATE: 10/16/2009  
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- NOTES:**
1. RECESSED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN THROUGH LANES, ALONG FLUSH MEDIAN LINES AND THE TURN LANE STORAGE BAY PER IDOT STD. 781001-02 AND DUPAGE COUNTY STANDARDS.
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  4. THE CONTRACTOR SHALL REPLACE DETECTOR LOOPS AS DIRECTED BY THE ENGINEER.

PRINTED DATE: 10/16/2009  
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 Title: Gary Road 03.dwg  
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 Plot Date: 10/16/2009 10:16:29 AM  
 Plot Scale: 1/50  
 Plot Device: HP DesignJet 5000

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 NAPERVILLE, IL 60563  
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 ILLINOIS DEPARTMENT OF PROFESSIONAL REGULATION REGISTRATION #184-003885

DUPAGE COUNTY DOT  
 JACK T. KNUEPFER ADMIN BLDG.  
 421 N. COUNTY FARM ROAD  
 WHEATON, IL 60187

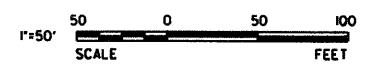
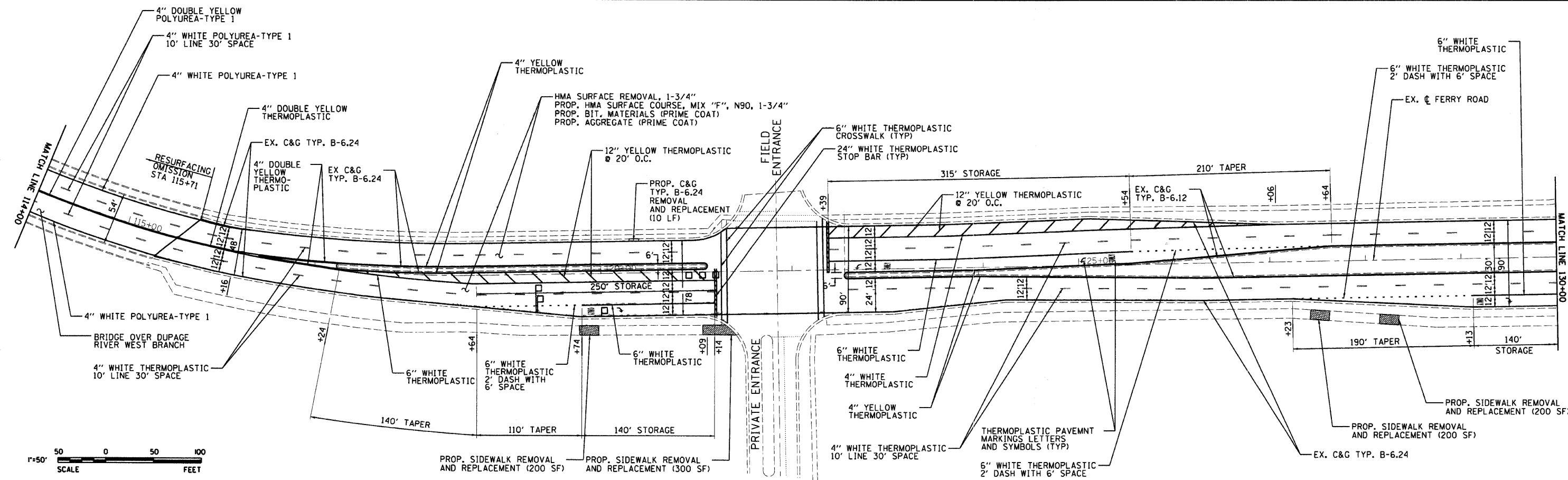
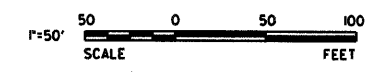
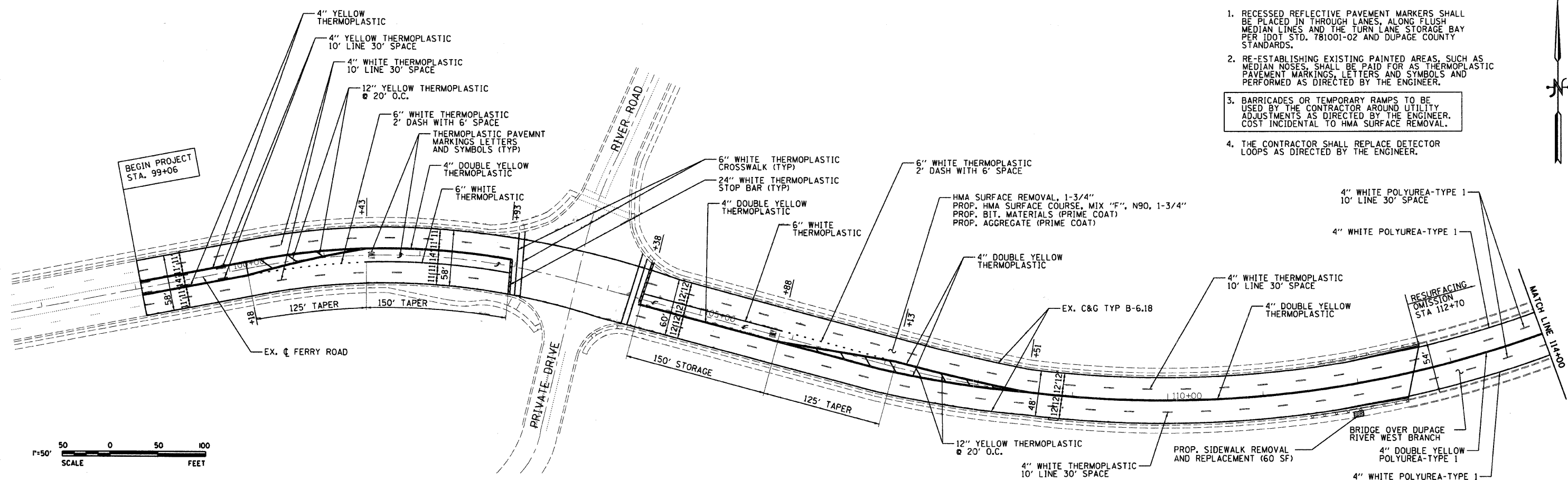
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| ESI PROJECT NO. 09-280 | ENGR: MR       |
| FILE: 22 Gary Road 03  | TECH: DN<br>JJ |
| DATE: 06-25-09         | QA/QC: JS      |

**DUPAGE LAPP PROJECT  
 CONTRACT NO. 63335**

**GARY AVENUE- ROADWAY PLANS  
 JEWELL ROAD TO ST. CHARLES ROAD**

- NOTES:**
1. RECESSED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN THROUGH LANES, ALONG FLUSH MEDIAN LINES AND THE TURN LANE STORAGE BAY PER IDOT STD. 781001-02 AND DUPAGE COUNTY STANDARDS.
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  3. BARRICADES OR TEMPORARY RAMPS TO BE USED BY THE CONTRACTOR AROUND UTILITY ADJUSTMENTS AS DIRECTED BY THE ENGINEER. COST INCIDENTAL TO HMA SURFACE REMOVAL.
  4. THE CONTRACTOR SHALL REPLACE DETECTOR LOOPS AS DIRECTED BY THE ENGINEER.



PRINTED DATE: 07/28/2009  
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 Location: lous\work\63335  
 County: 001  
 Date: 07/28/2009  
 Author: lous  
 Title: 63335.dwg  
 Description: FERRY PLAN

**ESI CONSULTANTS, LTD**  
 1375 N. MILL STREET, SUITE 100  
 NAPERVILLE, IL 60563  
 (630) 426-1700  
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 ILLINOIS DEPARTMENT OF PROFESSIONAL REGULATION REGISTRATION #164-003685

DUPAGE COUNTY DOT  
 JACK T. KNUEPFER ADMIN BLDG  
 421 N. COUNTY FARM ROAD  
 WHEATON, IL 60187

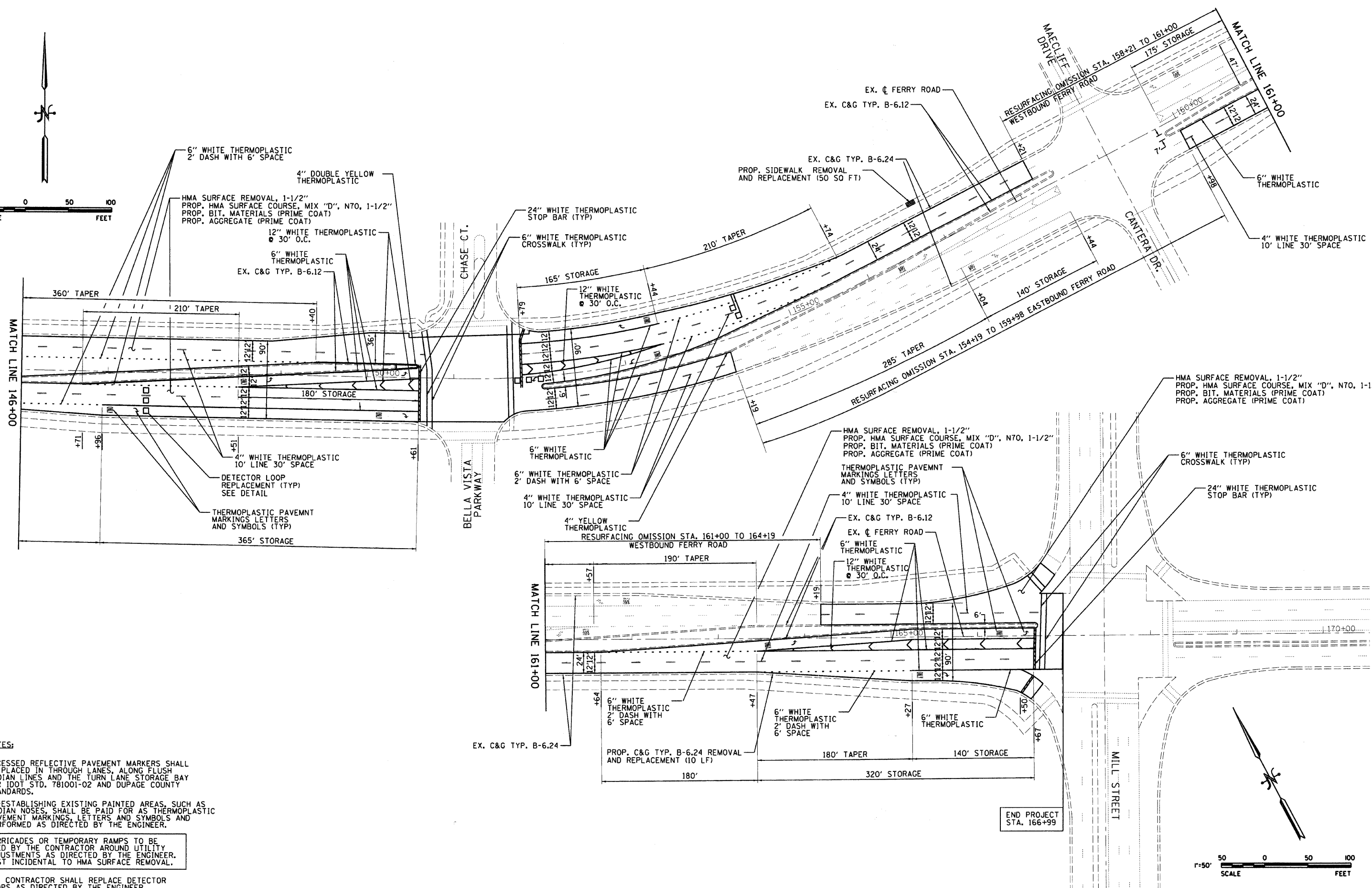
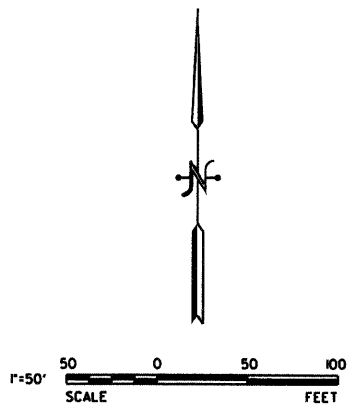
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ESI PROJECT NO. 09-280  
 FILE: 23 Ferry Plan 1  
 DATE: 06-25-09  
 ENGR: MR  
 TECH: DN  
 QA/QC: JS

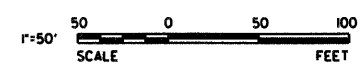
**DUPAGE LAPP PROJECT  
 CONTRACT NO. 63335**

**FERRY ROAD- ROADWAY PLANS  
 RIVER ROAD TO MILL STREET**





- NOTES:**
1. RECESSED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN THROUGH LANES, ALONG FLUSH MEDIAN NOSES, SHALL BE PAID FOR AS THERMOPLASTIC PAVEMENT MARKINGS, LETTERS AND SYMBOLS AND PERFORMED AS DIRECTED BY THE ENGINEER.
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  4. THE CONTRACTOR SHALL REPLACE DETECTOR LOOPS AS DIRECTED BY THE ENGINEER.



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 ILLINOIS DEPARTMENT OF PROFESSIONAL REGULATION REGISTRATION #184-003685

DUPAGE COUNTY DOT  
 JACK T. KNUEPFER ADMIN BLDG.  
 421 N. COUNTY FARM ROAD  
 WHEATON, IL 60187

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ESI PROJECT NO. 09-280  
 FILE: 25 Ferry Plan 3  
 DATE: 06-25-09  
 ENGR: MR  
 TECH: JJ  
 QAQC: JS

**DUPAGE LAPP PROJECT  
 CONTRACT NO. 63335**

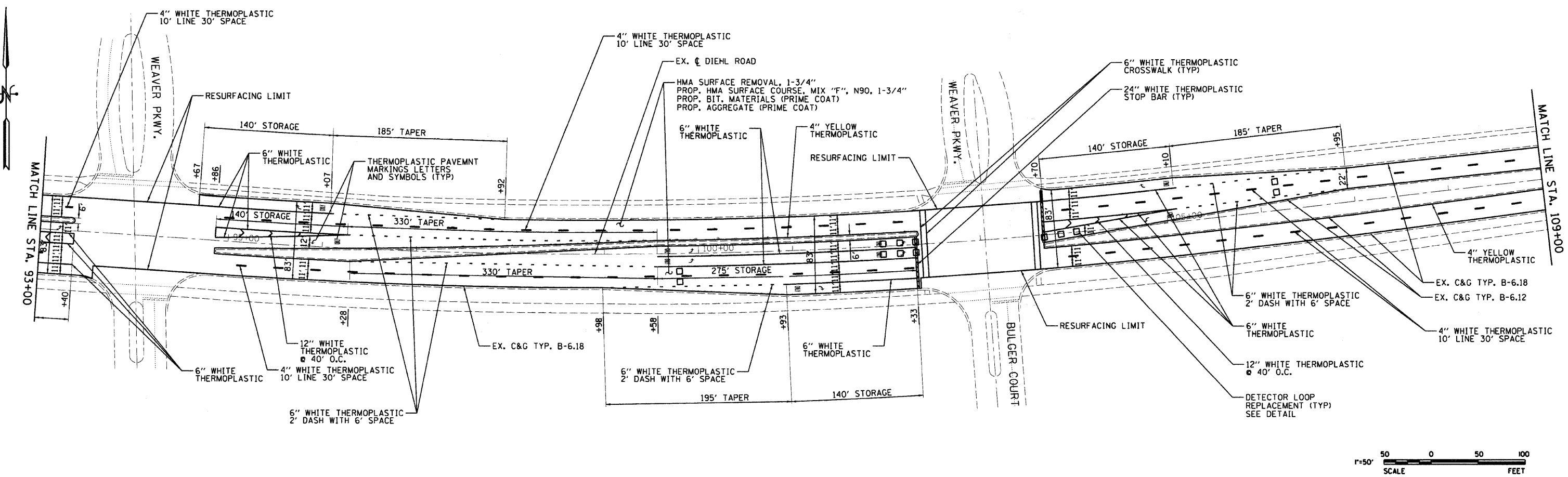
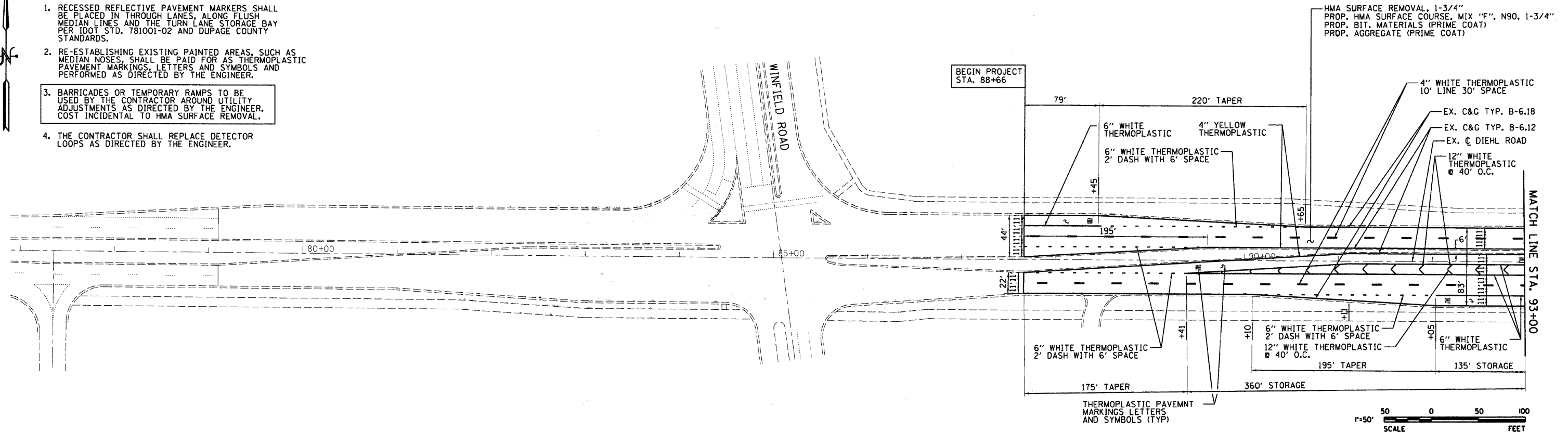
**FERRY ROAD- ROADWAY PLANS  
 RIVER ROAD TO MILL STREET**

DRAWING NO.  
**25**



**NOTES:**

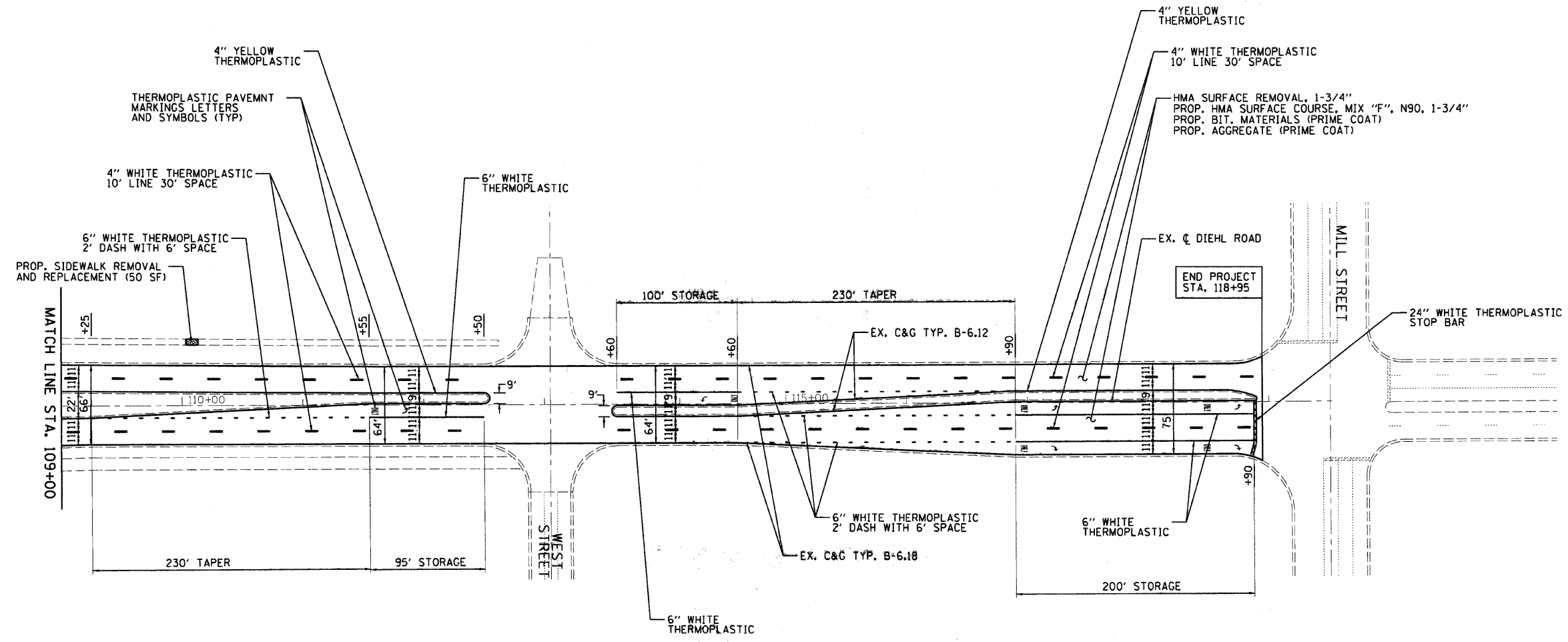
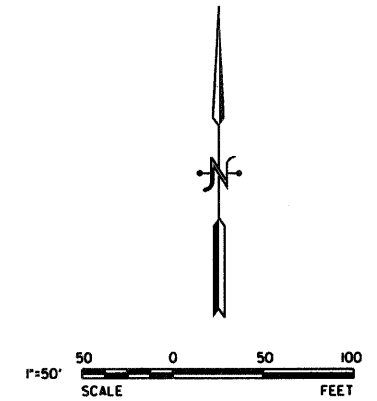
1. RECESSED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN THROUGH LANES, ALONG FLUSH MEDIAN LINES AND THE TURN LANE STORAGE BAY PER IDOT STD. 781001-02 AND DUPAGE COUNTY STANDARDS.
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4. THE CONTRACTOR SHALL REPLACE DETECTOR LOOPS AS DIRECTED BY THE ENGINEER.



PRINTED DATE: 10/16/2009  
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 Scale: 1/8"=1'-0"  
 Plot Size: 11.00 x 17.00  
 Plot Device: HP DesignJet 500

| NO. | DATE | DESCRIPTION OF REVISIONS | NO. | DATE | DESCRIPTION OF REVISIONS |
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**NOTES:**

1. RECESSED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN THROUGH LANES, ALONG FLUSH MEDIAN LINES AND THE TURN LANE STORAGE BAY PER IDOT STD. 781001-02 AND DUPAGE COUNTY STANDARDS.
2. RE-ESTABLISHING EXISTING PAINTED AREAS, SUCH AS MEDIAN NOSES, SHALL BE PAID FOR AS THERMOPLASTIC PAVEMENT MARKINGS, LETTERS AND SYMBOLS AND PERFORMED AS DIRECTED BY THE ENGINEER.
3. BARRICADES OR TEMPORARY RAMPS TO BE USED BY THE CONTRACTOR AROUND UTILITY ADJUSTMENTS AS DIRECTED BY THE ENGINEER. COST INCIDENTAL TO HMA SURFACE REMOVAL.
4. THE CONTRACTOR SHALL REPLACE DETECTOR LOOPS AS DIRECTED BY THE ENGINEER.

PRINTED DATE: 10/16/2009  
 X:\Users\jshelton\Documents\dupage\esi\09-280\02 Diehl Road 02.dwg  
 User: jshelton  
 Date: 10/16/2009 10:16:29 AM  
 Plot: 10/16/2009 10:16:29 AM  
 Scale: 1/8"=1'-0"  
 Plot Size: 11.00 x 17.00  
 Plot Orientation: Portrait  
 Plot Device: HP DesignJet 5000

**ESI CONSULTANTS, LTD**  
 1979 N. MILL STREET, SUITE 100  
 NAPERVILLE, IL 60563  
 (630) 420-1700  
 WWW.ESICONSULTANTS.LTD.COM  
 ILLINOIS DEPARTMENT OF PROFESSIONAL REGULATION REGISTRATION #194-003685

DUPAGE COUNTY DOT  
 JACK T. KNUEPFER ADMIN BLDG.  
 421 N. COUNTY FARM ROAD  
 WHEATON, IL 60187

| NO. | DATE | DESCRIPTION OF REVISIONS | NO. | DATE | DESCRIPTION OF REVISIONS |
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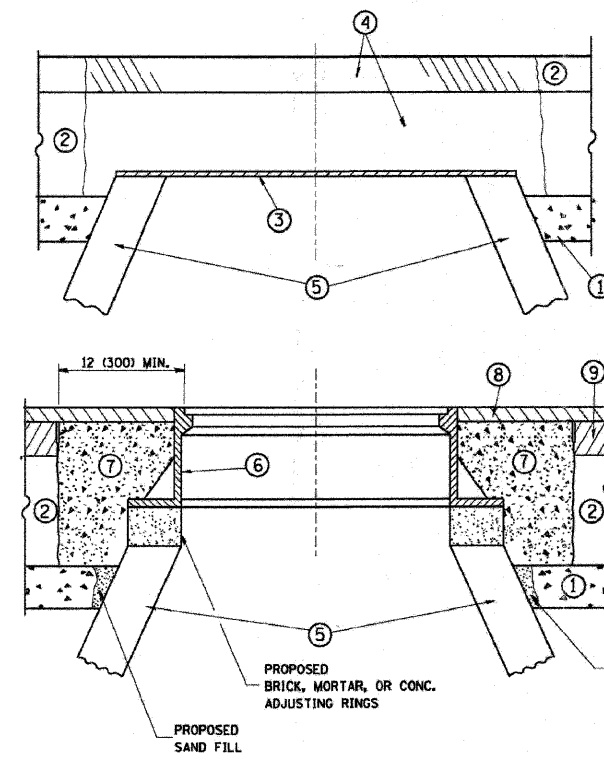
ESI PROJECT NO. 09-280  
 FILE: 27 Diehl Road 02  
 DATE: 06-25-09  
 ENGR: MR  
 TECH: DN  
 TECH: JJ  
 QAQC: JS

**DUPAGE LAPP PROJECT**  
**CONTRACT NO. 63335**

**DIEHL ROAD- ROADWAY PLANS**  
**WINFIELD ROAD TO MILL STREET**

DRAWING NO.  
**27**

|   |         |        |              |           |
|---|---------|--------|--------------|-----------|
| CONTRACT NO.                                    |         |        |              |           |
| F.A. RTE.                                       | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| STA.  | TO STA. |        |              |           |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |         |        |              |           |



**CONSTRUCTION PROCEDURES**

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
  - B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
  - C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
  - D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.
- STAGE 2 (AFTER PAVEMENT MILLING)**
- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
  - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
  - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS S1 CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS S1 CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:** THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

| REVISIONS   |          |
|-------------|----------|
| NAME        | DATE     |
| R. SHAH     | 10/25/94 |
| R. SHAH     | 01/30/95 |
| R. SHAH     | 03/10/95 |
| A. ABBAS    | 03/21/97 |
| R. WIEDEMAN | 05/14/04 |
| R. BORO     | 01/01/07 |

ILLINOIS DEPARTMENT OF TRANSPORTATION

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: VERT. NONE  
HORIZ.

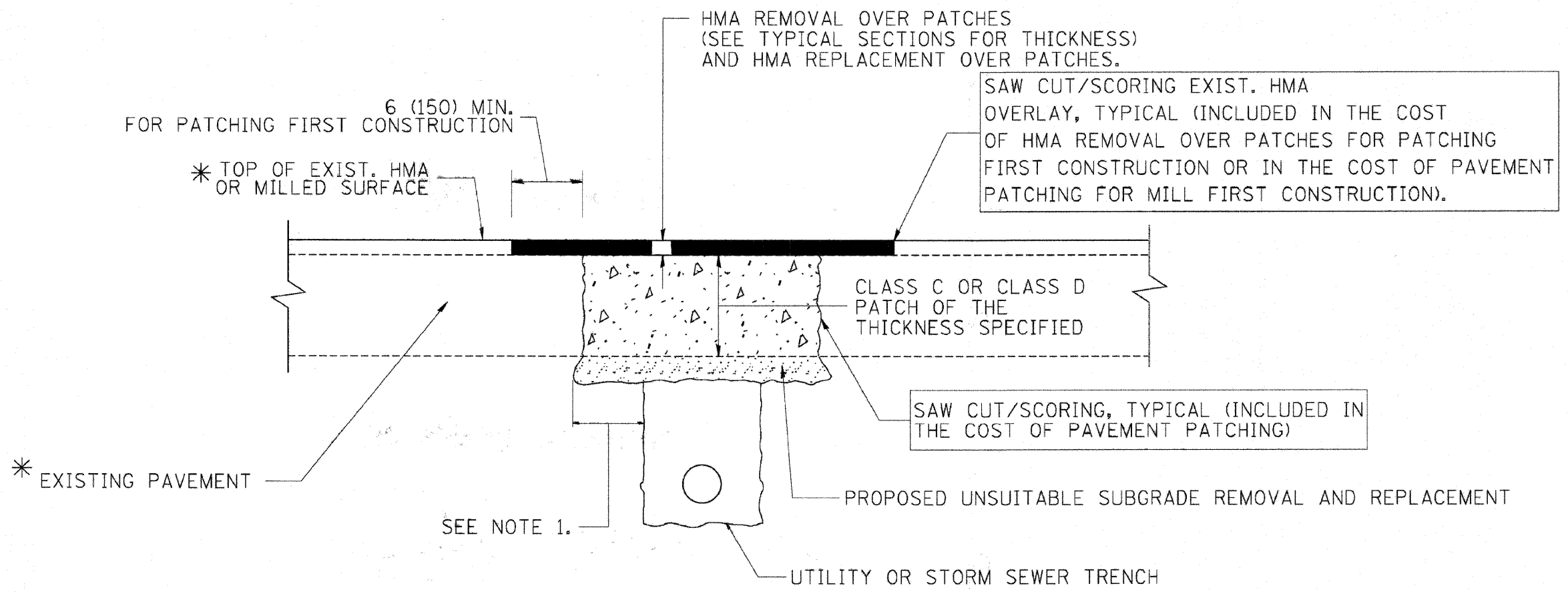
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CHECKED BY  
BD600-03 (BD-8)

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PLOT SCALE = 3/8" = 1'-0"  
USER NAME = bbornd

PRINTED DATE: 10/16/2009 11:22:52 AM C:\Users\bornd\Documents\esi\dupage\dupage\dupage.dwg

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ESI PROJECT NO. 09-280 ENGR: MR  
FILE: 28 Details 01 TECH: DN  
DATE: 06-25-09 QAQC: JS



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

**SEQUENCE OF CONSTRUCTION (PATCHING FIRST)**

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

**SEQUENCE OF CONSTRUCTION (MILLING FIRST)**

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| REVISIONS |          |
|-----------|----------|
| NAME      | DATE     |
| R. SHAH   | 01/14/95 |
| R. SHAH   | 03/23/95 |
| R. SHAH   | 04/24/95 |
| A. HOUSEH | 03/15/96 |
| A. ABBAS  | 03/21/97 |
| A. ABBAS  | 01/20/98 |
| ART ABBAS | 04/27/98 |
| R. BORO   | 01/01/07 |
| R. BORO   | 09/04/07 |
| K. ENG    | 10/27/08 |

ILLINOIS DEPARTMENT OF TRANSPORTATION

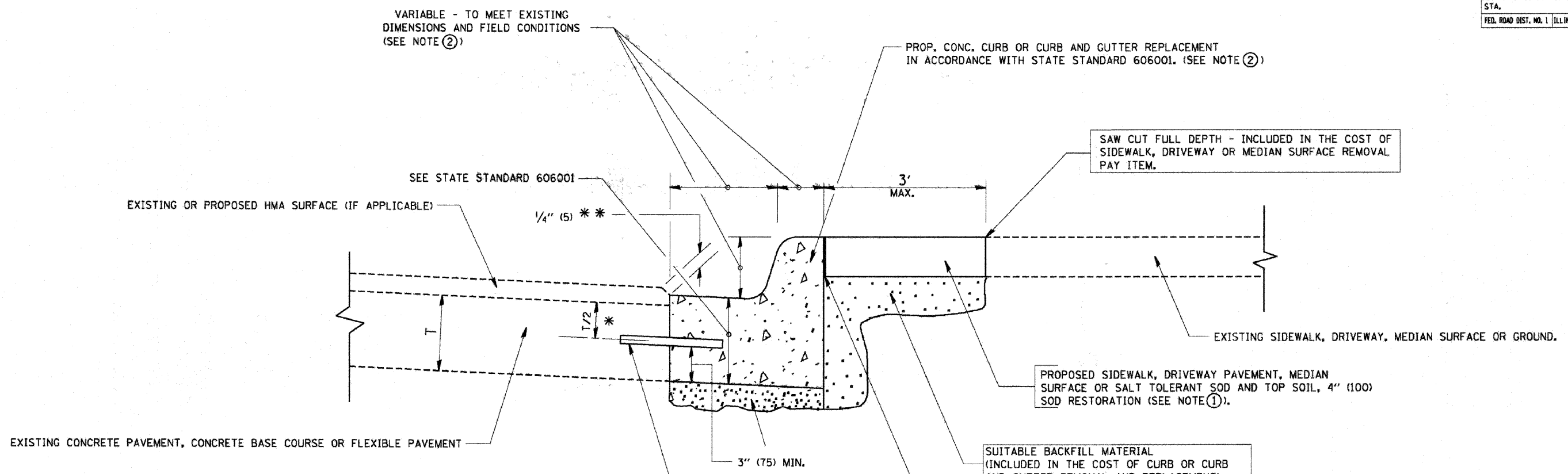
**PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT**

SCALE: VERT. NONE  
HORIZ.

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BD400-04 (80-22)

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 PLOTTER: HP DesignJet 5000

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- \* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- \* \* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

- ② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

**BASIS OF PAYMENT:**  
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

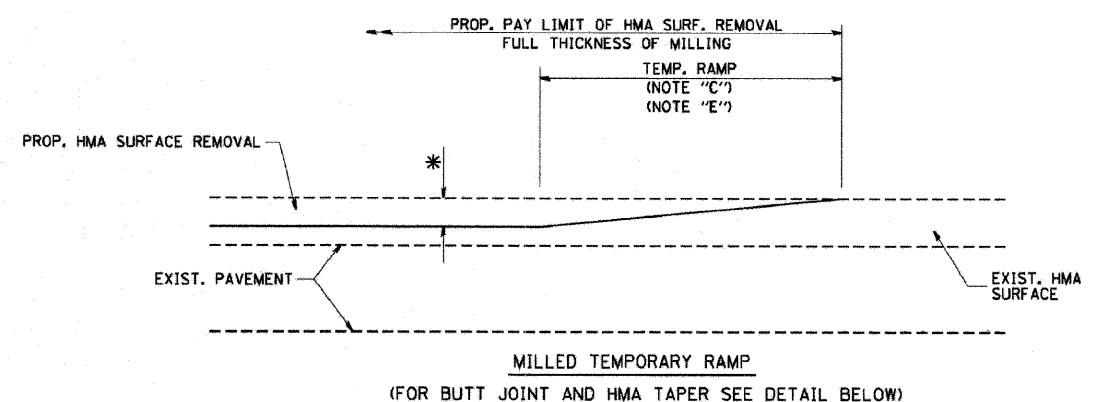
| REVISIONS |          |
|-----------|----------|
| NAME      | DATE     |
| A. HOUSEH | 03/11/94 |
| R. SHAH   | 02/24/95 |
| R. SHAH   | 03/02/95 |
| R. SHAH   | 08/19/96 |
| R. SHAH   | 09/12/96 |
| R. SHAH   | 09/19/96 |
| R. SHAH   | 10/03/96 |
| A. ABBAS  | 03/21/97 |
| M. GOMEZ  | 01/22/01 |
| R. BORO   | 01/01/07 |

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT**

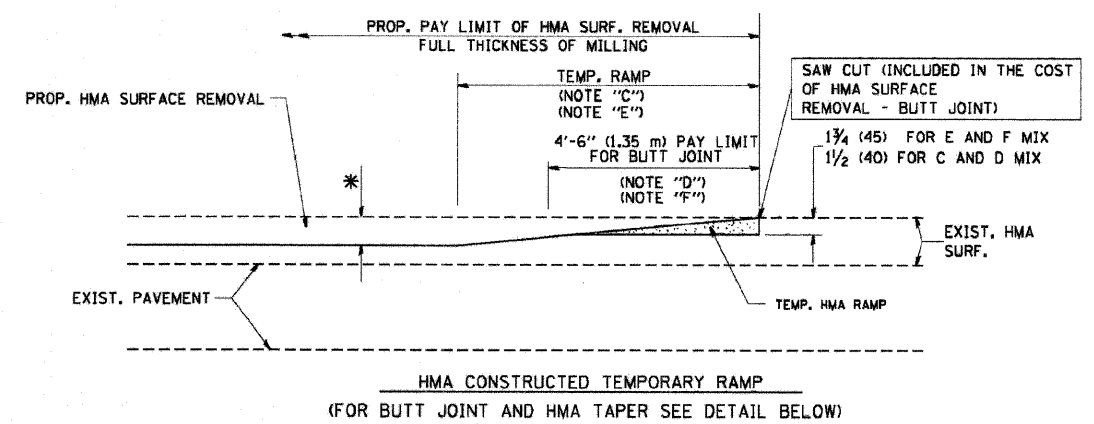
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HORIZ.  
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CHECKED BY  
BD600-06 (BD-24)

# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

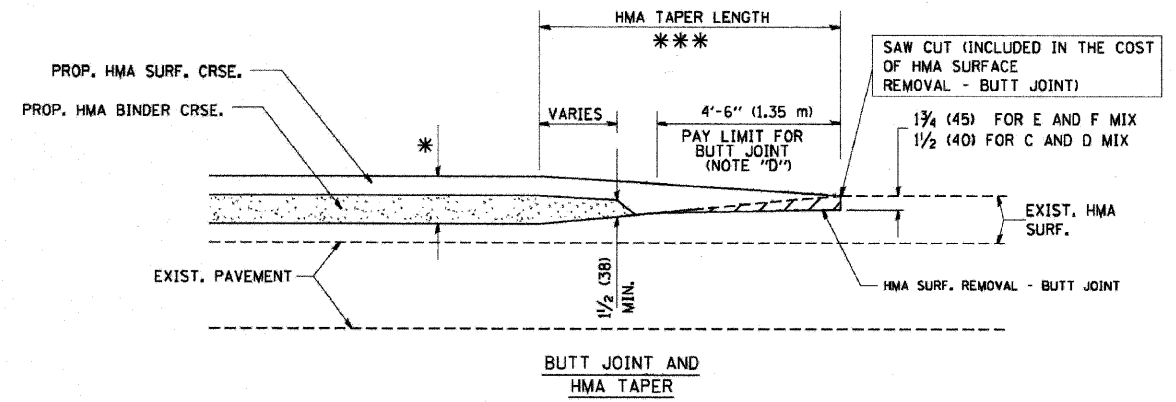
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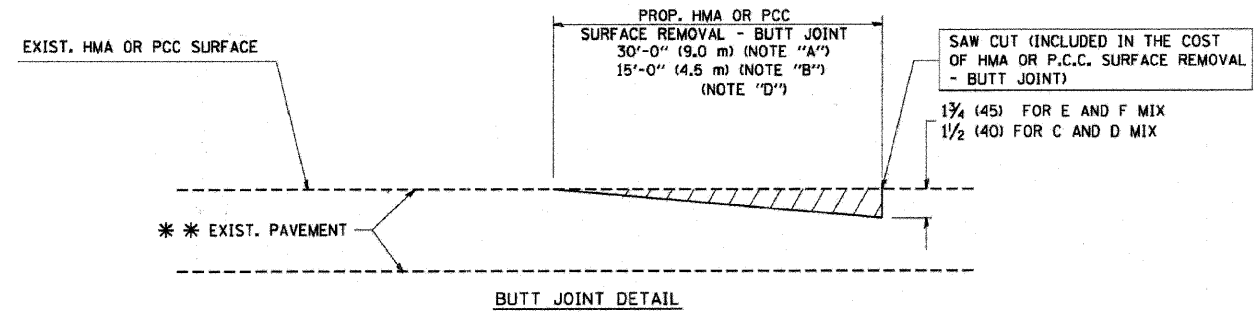
**OPTION 1**



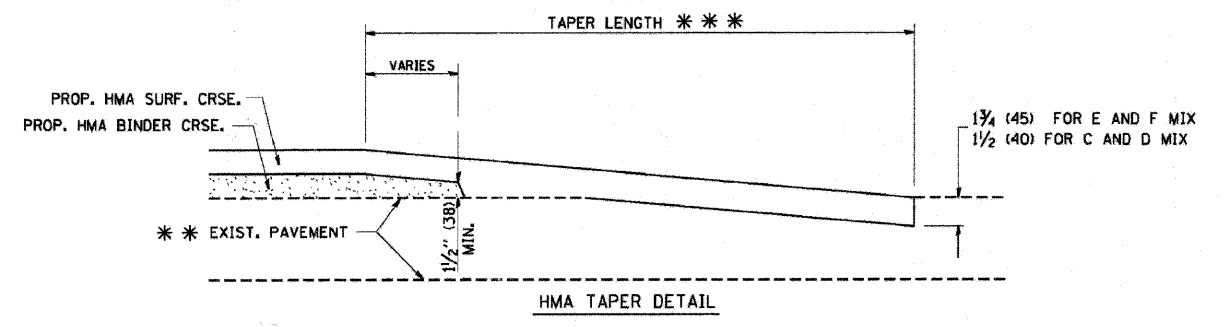
**OPTION 2  
TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**



**BUTT JOINT DETAIL**



**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

| REVISIONS  |          |
|------------|----------|
| NAME       | DATE     |
| M. DE YONG | 6-13-90  |
| M. DE YONG | 7-3-90   |
| M. DE YONG | 3-21-92  |
| R. SHAH    | 09/09/94 |
| R. SHAH    | 10/25/94 |
| A. ABBAS   | 03/21/97 |
| M. GOMEZ   | 04/06/01 |
| R. BORO    | 01/01/07 |

ILLINOIS DEPARTMENT OF TRANSPORTATION

**BUTT JOINT AND HMA TAPER DETAILS**

SCALE: VERT. NONE  
HORIZ. NONE  
DRAWN BY  
CHECKED BY  
BD400-05 (V1=BD32)

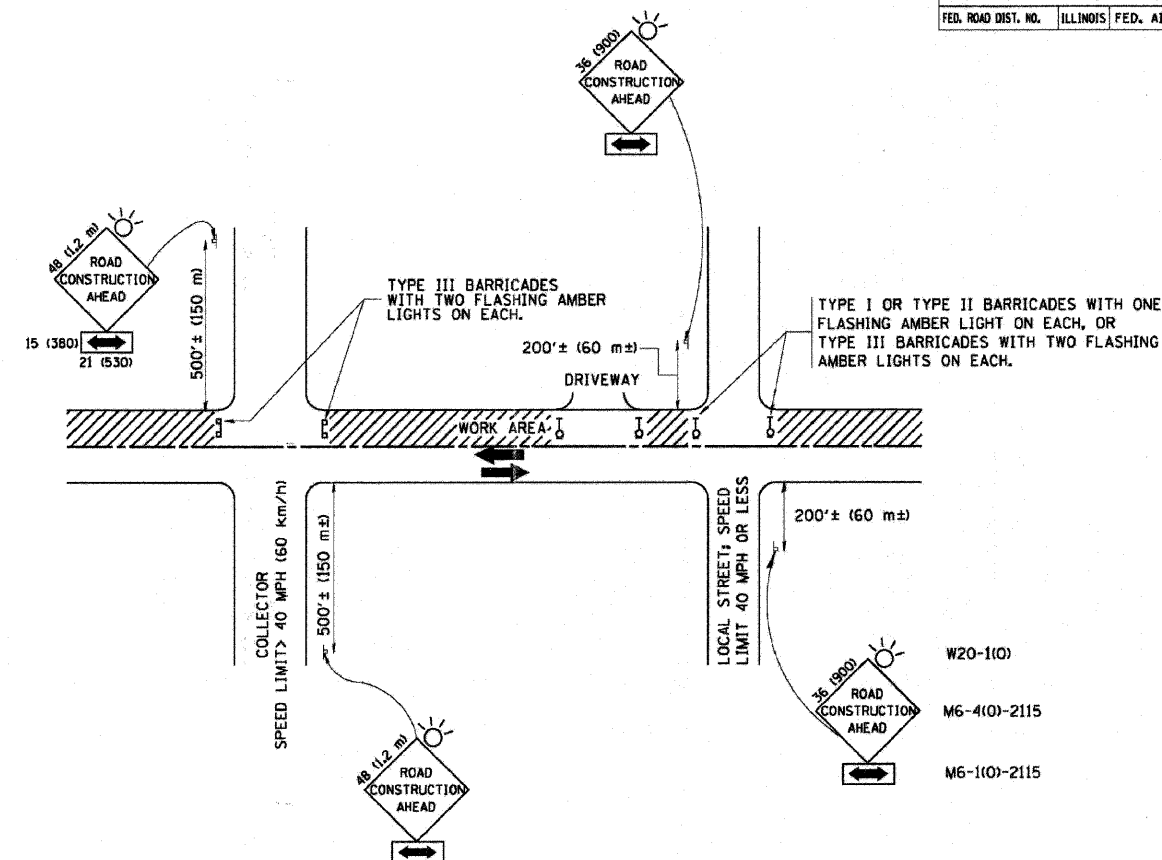
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| ESI PROJECT NO. 09-280 | ENGR. MR  |
| FILE: 31 Details 04    | TECH. DN  |
| DATE: 06-25-09         | QA/QC. JS |

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|---------------------|----------|------------------|-----------------|
| CONTRACT NO.        |          |                  |                 |
| F.A. RTE.           | SECTION  | COUNTY           | TOTAL SHEET NO. |
| STA.                | TO STA.  |                  |                 |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT |                 |



### TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

#### NOTES:

##### A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

##### B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

| REVISIONS    |          |
|--------------|----------|
| NAME         | DATE     |
| LHA          | 6/789    |
| T. RAMMACHER | 09/08/94 |
| J. OBERLE    | 10/18/95 |
| A. HOUSEH    | 03/06/96 |
| A. HOUSEH    | 10/15/96 |
| T. RAMMACHER | 01/06/00 |
|              |          |
|              |          |

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 TRAFFIC CONTROL AND PROTECTION  
 FOR  
 SIDE ROADS, INTERSECTIONS, AND  
 DRIVEWAYS

SCALE: NONE

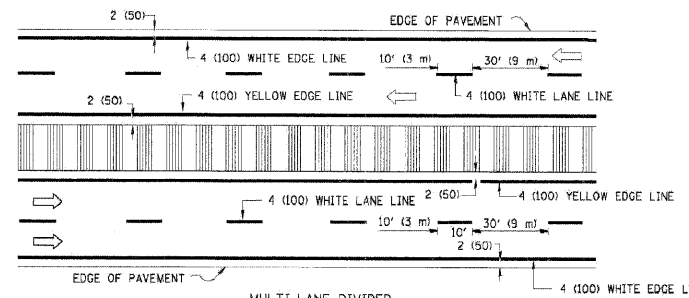
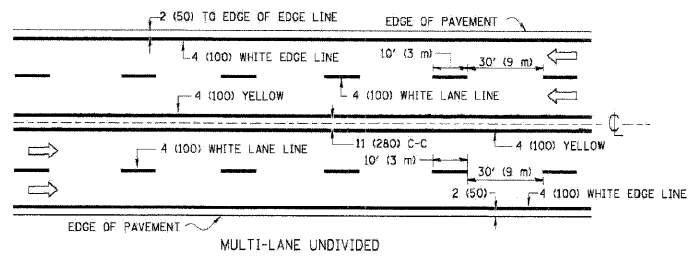
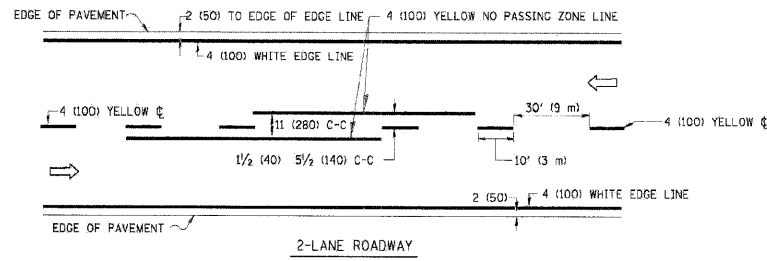
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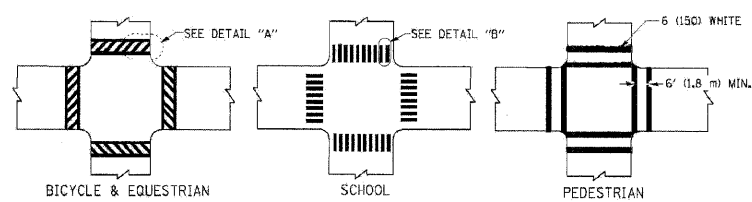
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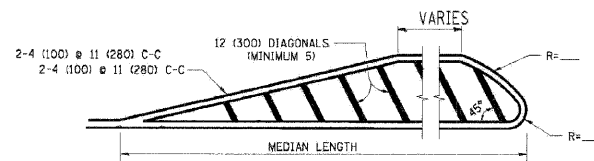
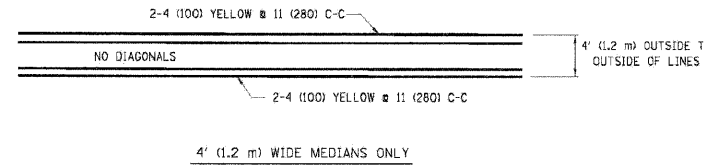


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

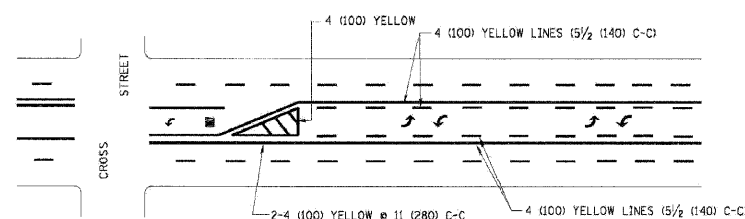
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

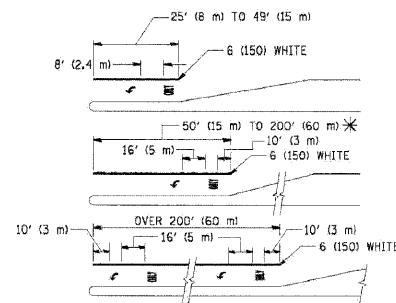


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.  
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
 75' (25 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h))  
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))  
 20' C-C PER DUPAGE STANDARD



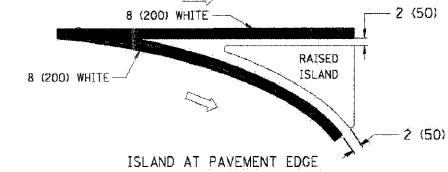
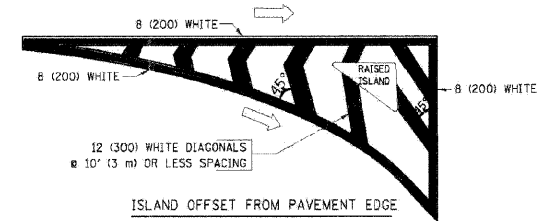
A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
 AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.6 SQ. FT. (1.9 m<sup>2</sup>)  
 \* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE



TYPICAL ISLAND MARKING

| TYPE OF MARKING   | WIDTH OF LINE  | PATTERN                         | COLOR   | SPACING / REMARKS  |
|---|--|---------------------------------|---|--|
| CENTERLINE ON 2 LANE PAVEMENT   | 4 (100)  | SKIP-DASH                       | YELLOW  | 10' (3 m) LINE WITH 30' (9 m) SPACE  |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT   | 2 # 4 (100)  | SOLID                           | YELLOW  | 11 (280) C-C   |
| NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS                                      | 4 (100)<br>2 # 4 (100)   | SOLID<br>SOLID                  | YELLOW<br>YELLOW  | 5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE<br>11 (280) C-C<br>OMIT SKIP-DASH CENTERLINE BETWEEN   |
| LANE LINES  | 4 (100)<br>5 (125) ON FREEWAYS   | SKIP-DASH<br>SKIP-DASH          | WHITE<br>WHITE  | 10' (3 m) LINE WITH 30' (9 m) SPACE  |
| DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)                                   | SAME AS LINE BEING EXTENDED  | SKIP-DASH                       | SAME AS LINE BEING EXTENDED                             | 2' (600) LINE WITH 6' (1.8 m) SPACE  |
| EDGE LINES  | 4 (100)  | SOLID                           | YELLOW-LEFT<br>WHITE-RIGHT                              | OUTLINE MOUNTABLE MEDIANS IN YELLOW. EDGE LINES ARE NOT USED NEXT TO BARRIER CURB  |
| TURN LANE MARKINGS  | 6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))                                      | SOLID                           | WHITE   | SEE TYPICAL TURN LANE MARKING DETAIL   |
| TWO WAY LEFT TURN MARKING   | 2 # 4 (100) EACH DIRECTION<br>8' (2.4m) LEFT ARROW   | SKIP-DASH AND SOLID<br>IN PAIRS | YELLOW<br>WHITE   | 10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH<br>5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE<br>SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL         |
| CROSSWALK LINES (PEDESTRIAN)<br>A. DIAGONALS (BIKE & EQUESTRIAN)<br>B. LONGITUDINAL BARS (SCHOOL) | 2 # 4 (150)<br>12 (300) @ 45°<br>12 (300) @ 90°  | SOLID<br>SOLID<br>SOLID         | WHITE<br>WHITE<br>WHITE                                 | NOT LESS THAN 6' (1.8 m) APART<br>2' (600) APART<br>2' (600) APART<br>SEE TYPICAL CROSSWALK MARKING DETAILS.   |
| STOP LINES  | 24 (600)   | SOLID                           | WHITE   | PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE |
| PAINTED MEDIANS   | 2 # 4 (100) WITH 12 (300) DIAGONALS @ 45°<br>NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS | SOLID                           | YELLOW;<br>TWO WAY TRAFFIC<br>WHITE;<br>ONE WAY TRAFFIC | 11 (280) C-C FOR THE DOUBLE LINE<br>SEE TYPICAL PAINTED MEDIAN MARKING.  |
| GORE MARKING AND CHANNELIZING LINES   | 8 (200) WITH 12 (300) DIAGONALS @ 45°  | SOLID                           | WHITE   | DIAGONALS:<br>15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h))<br>20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h))<br>30' (9 m) C-C (OVER 45MPH (70 km/h))            |
| RAILROAD CROSSING   | 24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"               | SOLID                           | WHITE   | SEE STATE STANDARD 780001 AREA OF:<br>"R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH<br>"X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )  |
| SHOULDER DIAGONALS  | 12 (300) @ 45°   | SOLID                           | WHITE - RIGHT<br>YELLOW - LEFT                          | 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))<br>75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h))<br>150' (45 m) C-C (OVER 45MPH (70 km/h))                       |

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

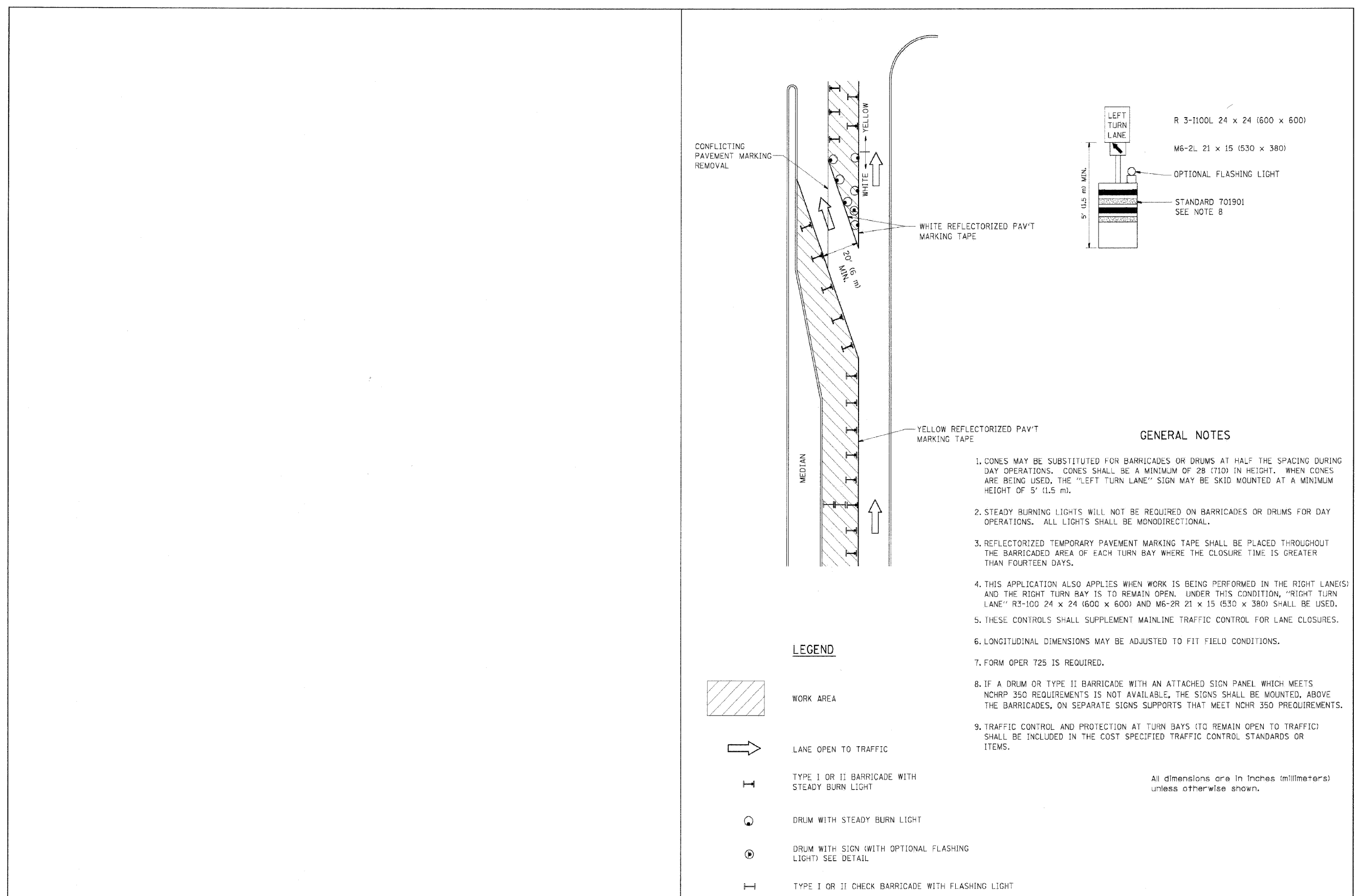
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| PLOT SCALE = 1/8" = 1' |                       |                  |                                 |
| PLOT DATE = 7/9/2009   |                       |                  |                                 |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

|                           |                         |           |                           |        |              |
|---------------------------|-------------------------|-----------|---------------------------|--------|--------------|
| DISTRICT ONE              |                         | F.A. RTL. | SECTION                   | COUNTY | TOTAL SHEETS |
| TYPICAL PAVEMENT MARKINGS |                         |           | TC-13                     |        | NO.          |
| SCALE: NONE               | SHEET NO. 1 OF 1 SHEETS | STA.      | CONTRACT NO.              |        |              |
|                           |                         | TO STA.   | ILLINOIS FED. AID PROJECT |        |              |

| NO. | DATE | DESCRIPTION OF REVISIONS | NO. | DATE | DESCRIPTION OF REVISIONS |
|-----|------|--------------------------|-----|------|--------------------------|
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PRINTED DATE: 07/29/2009 11:02:53 AM  
 FILE: C:\WORK\63333\63333.dwg  
 USER: JTS  
 PLOT: JTS  
 PLOT DATE: 7/14/2009



**GENERAL NOTES**

- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
- STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
- REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
- THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
- THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
- LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
- FORM OPER 725 IS REQUIRED.
- IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.
- TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

- LEGEND**
- WORK AREA
  - LANE OPEN TO TRAFFIC
  - TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
  - DRUM WITH STEADY BURN LIGHT
  - DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
  - TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

|  |             |                                 |                            |   |  |                         |         |        |              |           |
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| FILE NAME =                                    | USER NAME = | REVISED - T. RAMMACHER 09-08-94 | REVISED - R. BORO 09-14-09 | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>TRAFFIC CONTROL AND PROTECTION AT TURN BAYS<br/>(TO REMAIN OPEN TO TRAFFIC)</b> | F.A. RTE.               | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
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| PLOT SCALE = 49.9999 1/1 IN.                   |             | REVISED - A. HOUSEH 10-12-96    | REVISED -                  |   |  |                         |         |        |              |           |
| PLOT DATE = 7/14/2009                          |             | REVISED - T. RAMMACHER 01-06-00 | REVISED -                  |   |  |                         |         |        |              |           |
|  |             |                                 |                            |   | SCALE: NONE  | SHEET NO. 1 OF 1 SHEETS |         | STA.   | TO STA.      |           |
|  |             |                                 |                            |   |  |                         | TC-14   |        | CONTRACT NO. |           |
|  |             |                                 |                            |   | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT                                    |                         |         |        |              |           |

| NO. | DATE | DESCRIPTION OF REVISIONS | NO. | DATE | DESCRIPTION OF REVISIONS |
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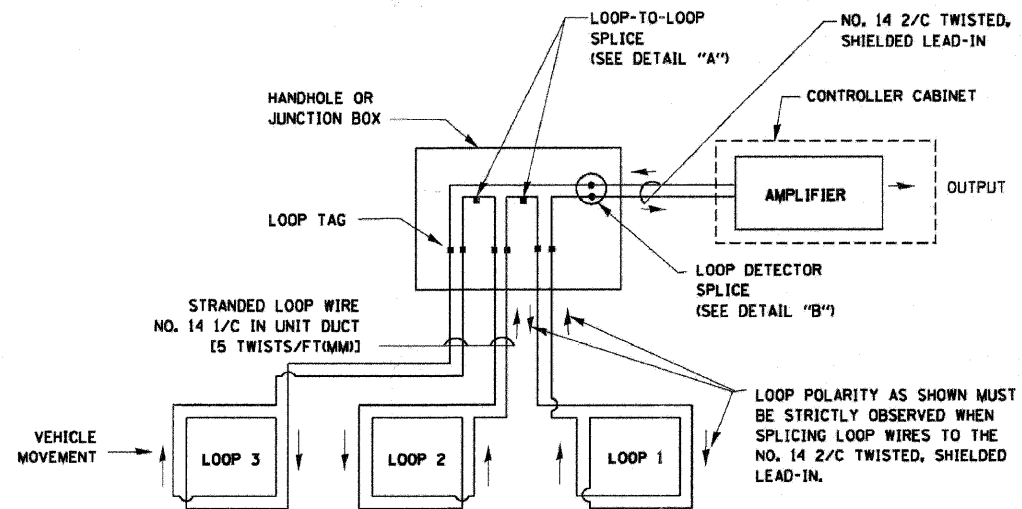
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|                        | QAQC: JS |



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| F.A. RTE.           | SECTION  | COUNTY           | TOTAL SHEETS | SHEET NO. |
| STA.                | TO STA.  |                  |              |           |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT |              |           |

**LOOP DETECTOR NOTES**

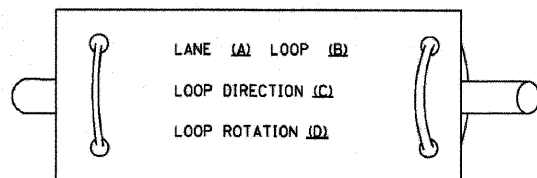
1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE UNIT DUCT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). UNIT DUCT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.



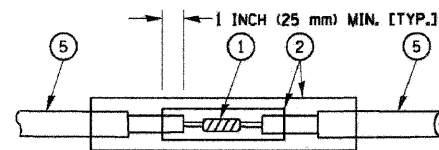
**DETECTOR LOOP WIRING SCHEMATIC**

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm), IF IN CONCRETE. THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

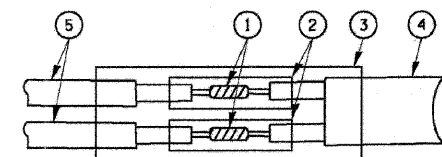
**LOOP LEAD-IN CABLE TAG**



- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.



**DETAIL "A"  
LOOP-TO-LOOP SPLICE**



**DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE**

**LOOP DETECTOR SPLICE**

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

| REVISIONS         |          |
|-------------------|----------|
| NAME              | DATE     |
| CADD              | 5/30/00  |
| ADD NOTE NO. 8    | 11/12/01 |
| BUREAU OF TRAFFIC | 1-01-02  |
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ILLINOIS DEPARTMENT OF TRANSPORTATION  
**DISTRICT ONE  
STANDARD TRAFFIC SIGNAL  
DESIGN DETAILS**

SCALE: NONE

DRAWN BY: RWP  
DESIGNED BY: DAD  
CHECKED BY: DAZ  
SHEET 1 OF 4

TS05

PLOT DATE = 3/7/2007  
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USER NAME = bbarndt

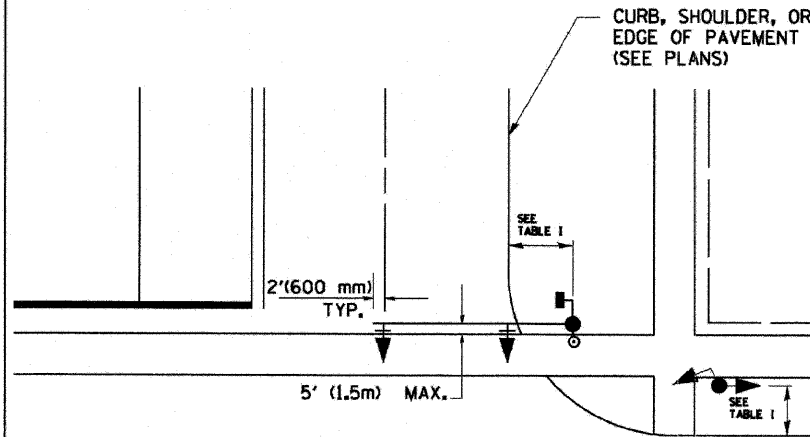
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| FILE: 36 Details 09    | TECH: DN |
| DATE: 06-25-09         | QAQC: JS |

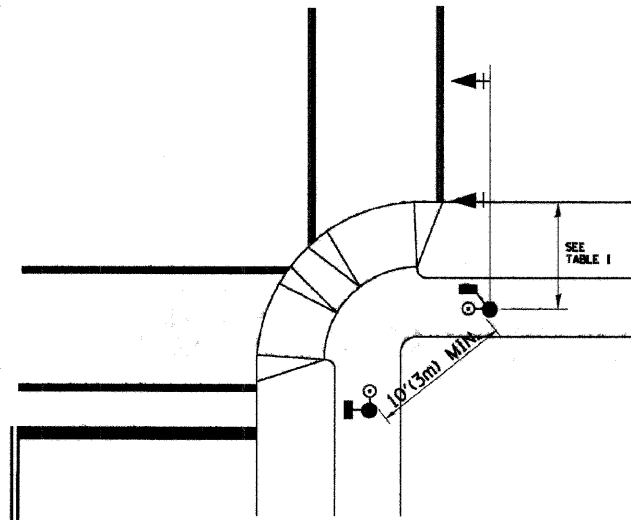
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| STA.                |         | TO STA.                   |              |           |
| FED. ROAD DIST. NO. |         | ILLINOIS FED. AID PROJECT |              |           |

**TRAFFIC SIGNAL MAST ARM AND POST**

MAST ARM MOUNTED SIGNAL IN PROPOSED & FUTURE SIDEWALK AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNAL AND PUSHBUTTON DETECTOR



**PEDESTRIAN SIGNAL PUSHBUTTON**



RECOMMENDED PUSHBUTTON LOCATIONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHALL BE IN ACCORDANCE WITH THE CURRENT MUTCD (SEE NOTE 1). TO MEET MUTCD REQUIREMENTS, PEDESTRIAN SIGNAL PUSHBUTTONS MAY HAVE TO BE MOUNTED ON A SEPARATE POST.

**NOTES:**

- AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS WITH PEDESTRIAN ACTUATION, EACH PUSHBUTTON SHALL ACTIVATE BOTH THE WALK INTERVAL AND THE ACCESSIBLE PEDESTRIAN SIGNALS.  
  
AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS, PUSHBUTTONS SHOULD CLEARLY INDICATE WHICH CROSSWALK SIGNAL IS ACTUATED BY EACH PUSHBUTTON. PUSHBUTTONS AND TACTILE ARROWS SHOULD HAVE HIGH VISUAL CONTRAST (SEE THE DEPARTMENT OF JUSTICE'S AMERICANS WITH DISABILITIES ACT STANDARDS FOR ACCESSIBLE DESIGN, 1991). TACTILE ARROWS SHOULD POINT IN THE SAME DIRECTION AS THE ASSOCIATED CROSSWALK. AT CORNERS OF SIGNALIZED LOCATIONS WITH ACCESSIBLE PEDESTRIAN SIGNALS WHERE PEDESTRIAN PUSHBUTTONS ARE PROVIDED, THE PUSHBUTTONS SHOULD BE SEPARATED BY THE DISTANCE OF AT LEAST 10 FT (3m). THIS ENABLES PEDESTRIANS WHO HAVE VISUAL DISABILITIES TO DISTINGUISH AND LOCATE THE APPROPRIATE PUSHBUTTON.  
  
PUSHBUTTONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHOULD BE LOCATED AS FOLLOWS:  
 A: ADJACENT TO A LEVEL ALL-WEATHER SURFACE TO PROVIDE ACCESS FROM A WHEELCHAIR, AND WHERE THERE IS AN ALL WEATHER SURFACE, WHEELCHAIR ACCESSIBLE ROUTE TO THE RAMP.  
 B: WITHIN 5 FT (1.5m) OF THE CROSSWALK EXTENDED.  
 C: WITHIN 10 FT (3m) OF THE EDGE OF CURB, SHOULDER, OR PAVEMENT.  
 D: PARALLEL TO THE CROSSWALK TO BE USED (SEE MUTCD FIGURE 4E-2).  
 E: NORMAL PEDESTRIAN PUSHBUTTON MOUNTING HEIGHT SHOULD BE 3.5 FT (1.05m) ABOVE ADJACENT SIDEWALK
- PEDESTRIAN SIGNAL FACES SHALL BE MOUNTED WITH THE BOTTOM OF THE HOUSING NOT LESS THAN 8 FT (2.4m) NOR MORE THAN 10 FT (3.0m) ABOVE THE SIDEWALK LEVEL AND SO THERE IS A PEDESTRIAN INDICATION IN THE LINE OF PEDESTRIANS' VISION WHICH PERTAINS TO THE CROSSWALK BEING USED.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, NOT MOUNTED OVER A ROADWAY, SHALL BE AT LEAST 10 FT (3.0m) BUT NOT MORE THAN 15 FT (4.5m) ABOVE THE SIDEWALK OR, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE HIGHWAY IF NO SIDEWALKS EXIST.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, MOUNTED OVER A ROADWAY, SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001 AND 877006. (16 FT (5m) MIN., 18 FT (5.5m) MAX., FROM HIGHEST POINT OF PAVEMENT)

**PEDESTRIAN SIGNAL POST**

PEDESTRIAN SIGNAL HEAD AND PEDESTRIAN PUSHBUTTON DETECTOR LOCATION

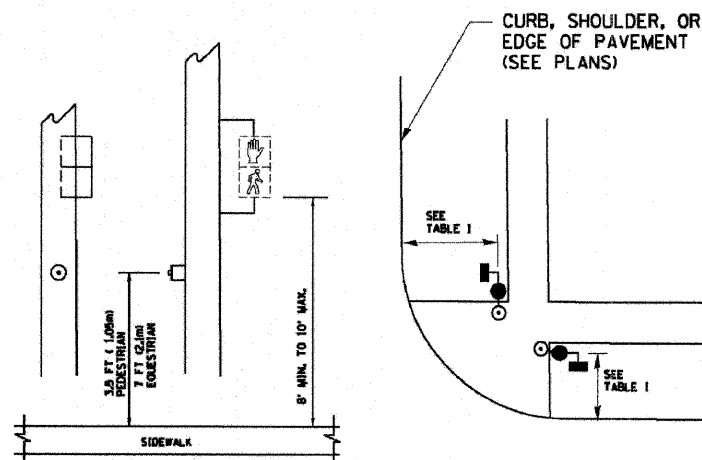


TABLE I

| TRAFFIC SIGNAL EQUIPMENT     | COMBINATION CONCRETE CURB AND GUTTER (MIN. DIST. FROM BACK OF CURB) | SHOULDER/NON-CURBED AREA (MIN. DIST. FROM EDGE OF PAVEMENT) |
|------------------------------|---|---|
| TRAFFIC SIGNAL MAST ARM POLE | 6 FT (1.8m)   | SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)              |
| TRAFFIC SIGNAL POST          | 4 FT (1.2m)   | SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)              |
| PEDESTRIAN SIGNAL POST       | 4 FT (1.2m)   | SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)              |
| PEDESTRIAN PUSHBUTTON        | SEE NOTE 1  | SEE NOTE 1  |

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USER NAME = dss\p01

| REVISIONS         |         |
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| NAME              | DATE    |
| BUREAU OF TRAFFIC | 1/01/02 |
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ILLINOIS DEPARTMENT OF TRANSPORTATION  
**DISTRICT 1**  
STANDARD TRAFFIC SIGNAL  
DESIGN DETAILS

SCALE: NONE

DRAWN BY: RWP  
DESIGNED BY: DAD  
CHECKED BY: DAZ  
SHEET 2 OF 4

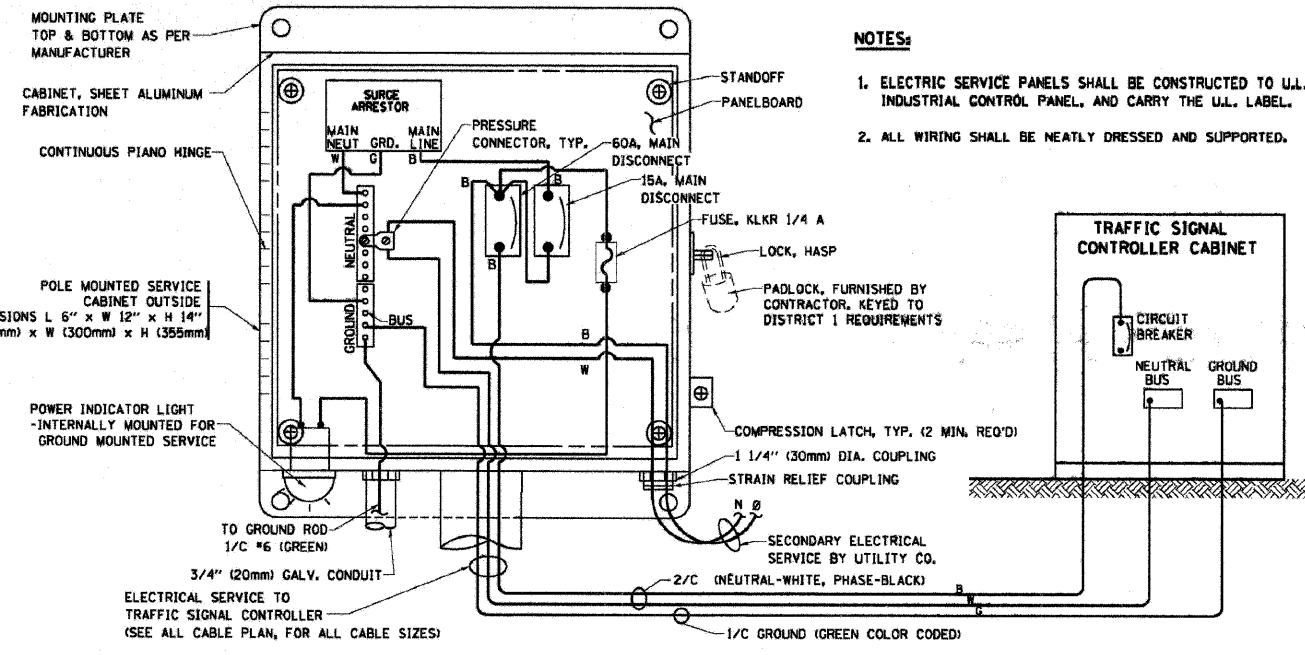
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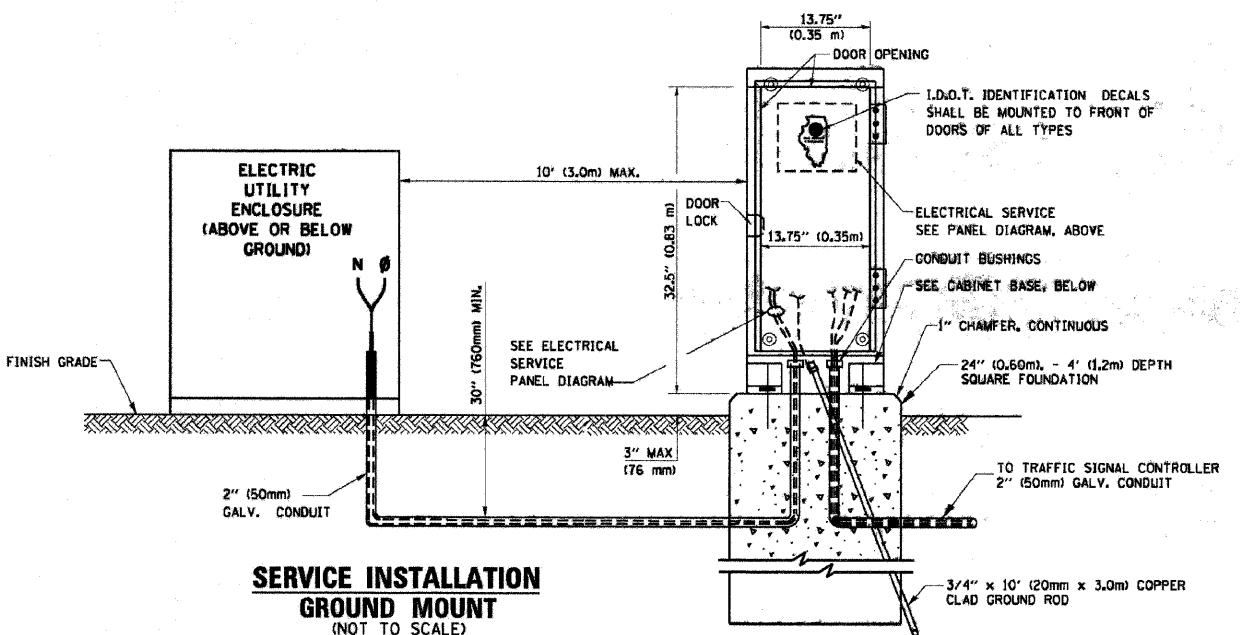
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| DATE: 06-25-09         | QAQC: JS             |



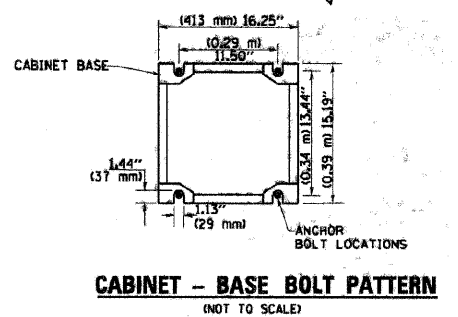
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|---------------------|---------|---------------------------|--------------|-----------|
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| STA.                |         | TO STA.                   |              |           |
| FED. ROAD DIST. NO. |         | ILLINOIS FED. AID PROJECT |              |           |



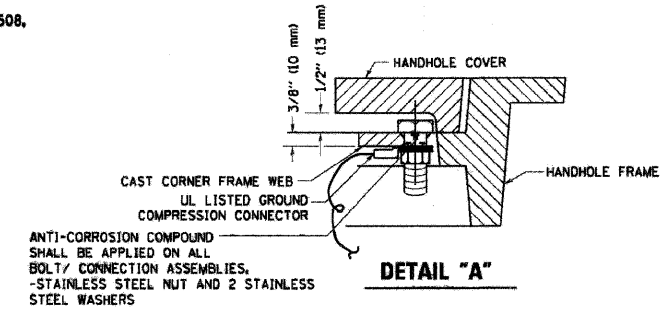
**ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)**  
**SERVICE INSTALLATION POLE MOUNT (SHOWN)**  
 (NOT TO SCALE)



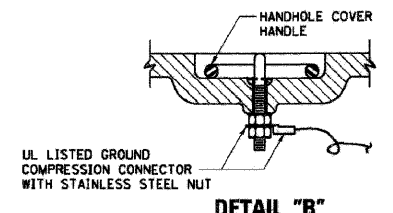
**SERVICE INSTALLATION GROUND MOUNT**  
 (NOT TO SCALE)



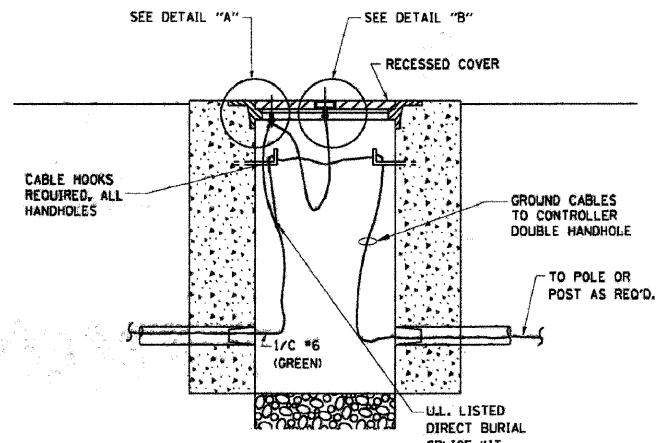
**CABINET - BASE BOLT PATTERN**  
 (NOT TO SCALE)



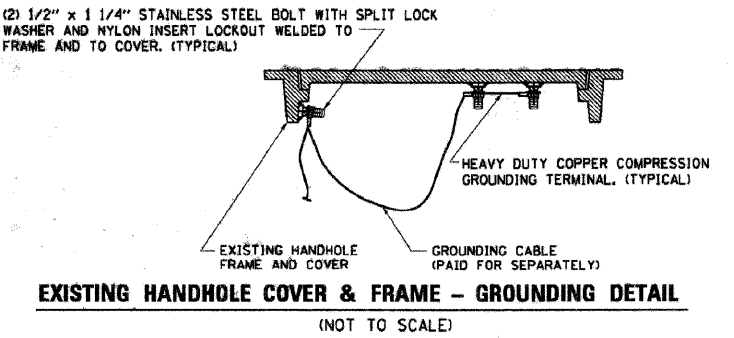
**DETAIL "A"**



**DETAIL "B"**



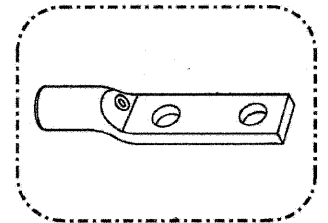
**HANDHOLE COVER & FRAME - GROUNDING DETAIL**  
 (NOT TO SCALE)



**EXISTING HANDHOLE COVER & FRAME - GROUNDING DETAIL**  
 (NOT TO SCALE)

**GROUNDING SYSTEM**

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



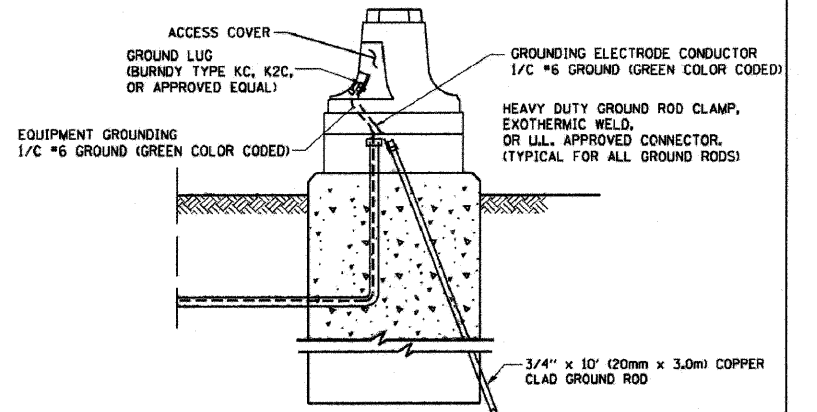
HEAVY-DUTY COMPRESSION TERMINAL (BURDYN TYPE YCHA OR APPROVED EQUAL)



3/4" (20mm) HEAVY-DUTY GROUND ROD CLAMP (BURDYN TYPE GRC OR APPROVED EQUAL)

**NOTES:**

- \* ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- \* GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES. 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES. 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



**MAST ARM POLE / POST-GROUNDING DETAIL**  
 (NOT TO SCALE)

| REVISIONS         |         |
|-------------------|---------|
| NAME              | DATE    |
| CADD              | 5/30/00 |
| CADD              | 3/15/01 |
| BUREAU OF TRAFFIC | 1/01/02 |
|                   |         |
|                   |         |
|                   |         |

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**DISTRICT ONE**  
**STANDARD TRAFFIC SIGNAL**  
**DESIGN DETAILS**

SCALE: NONE

DRAWN BY: RWP  
 DESIGNED BY: DAD  
 CHECKED BY: DAZ  
 SHEET 3 OF 4  
 TS05

PRINTED DATE: 10/16/2009  
 FILE NAME: 63333-03.dwg  
 PLOT DATE: 3/7/2007  
 PLOT TIME: 11:50:00  
 USER: jkuepfer

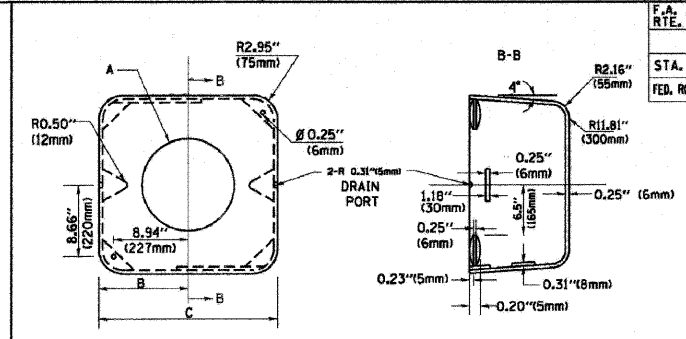
| NO. | DATE | DESCRIPTION OF REVISIONS | NO. | DATE | DESCRIPTION OF REVISIONS |
|-----|------|--------------------------|-----|------|--------------------------|
|     |      |                          |     |      |                          |
|     |      |                          |     |      |                          |

|                        |          |
|------------------------|----------|
| ESI PROJECT NO. 09-280 | ENGR: MR |
| FILE: 38 Details 11    | TECH: DN |
| DATE: 06-25-08         | TECH: JJ |
|                        | QAQC: JS |



|                     |                           |        |              |           |
|---------------------|---------------------------|--------|--------------|-----------|
| F.A. RTE.           | SECTION                   | COUNTY | TOTAL SHEETS | SHEET NO. |
| STA.                | TO STA.                   |        |              |           |
| FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT |        |              |           |

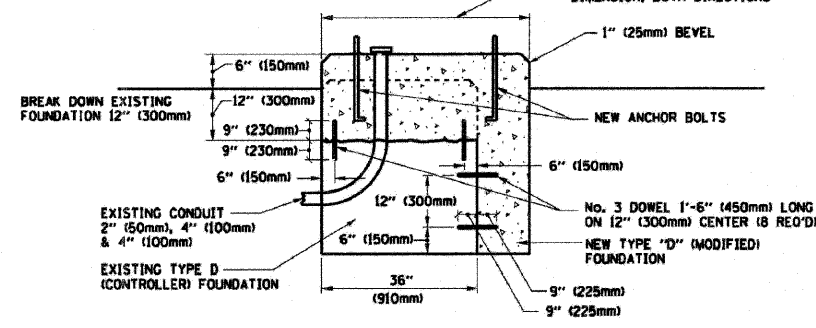
MATERIAL:  
 - ASTM A48 CLASS 30 GREY IRON  
 - ASTM A123 HOT DIPPED GALVANIZED



| TYPE | A                 | B              | C             | HEIGHT      | WEIGHT |
|------|-------------------|----------------|---------------|-------------|--------|
| I    | Ø 10.125" (257mm) | 9.5" (241mm)   | 19" (483mm)   | 12" (300mm) | 24kg   |
| II   | Ø 11.125" (283mm) | 10.75" (273mm) | 21.5" (546mm) | 12" (300mm) | 26kg   |

**SHROUD DETAIL**

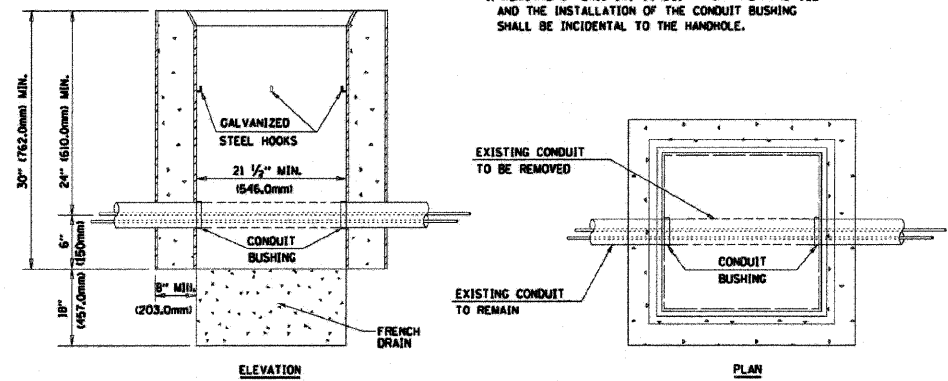
NOTE:  
 SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



**MODIFY EXISTING TYPE "D" FOUNDATION**

(NOT TO SCALE)

NOTES:  
 1. REMOVAL OF EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHING SHALL BE INCIDENTAL TO THE HANDHOLE.

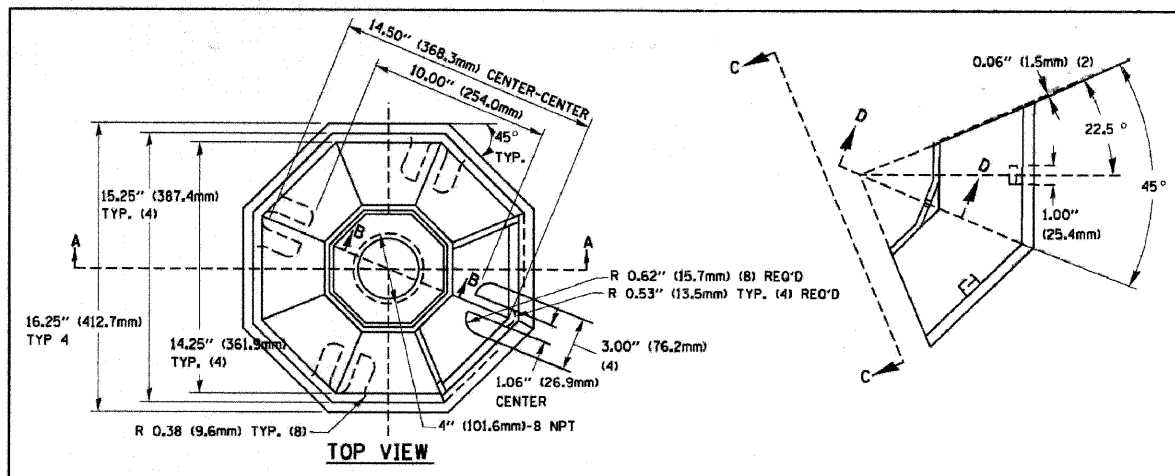


**DETAIL HANDHOLE TO INTERCEPT EXISTING CONDUIT**

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 DISTRICT ONE  
 STANDARD TRAFFIC SIGNAL  
 DESIGN DETAILS

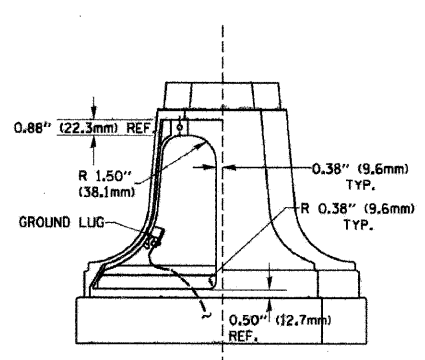
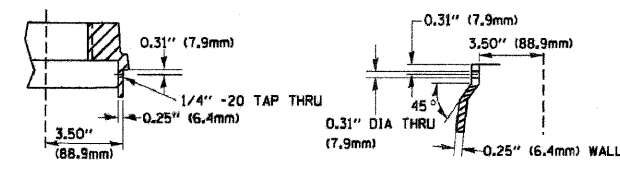
| REVISIONS         | DATE     |
|-------------------|----------|
| BUREAU OF TRAFFIC | 5/30/00  |
| BUREAU OF TRAFFIC | 3/15/01  |
| BUREAU OF TRAFFIC | 11/12/01 |
| BUREAU OF TRAFFIC | 1-01-02  |

SCALE: NONE  
 DRAWN BY: RWP  
 DESIGNED BY: DAZ  
 CHECKED BY: DAZ  
 SHEET 4 OF 4  
 TS05



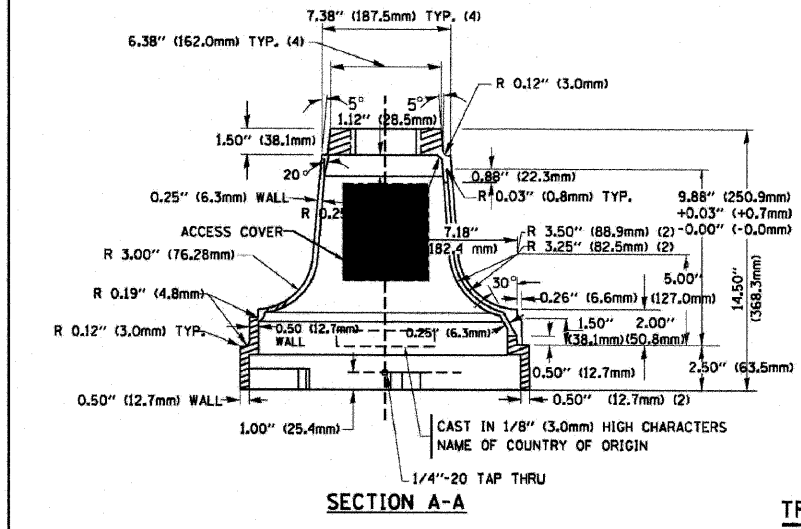
**SECTION B-B**

**SECTION D-D**



**VIEW C-C**

**TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A**



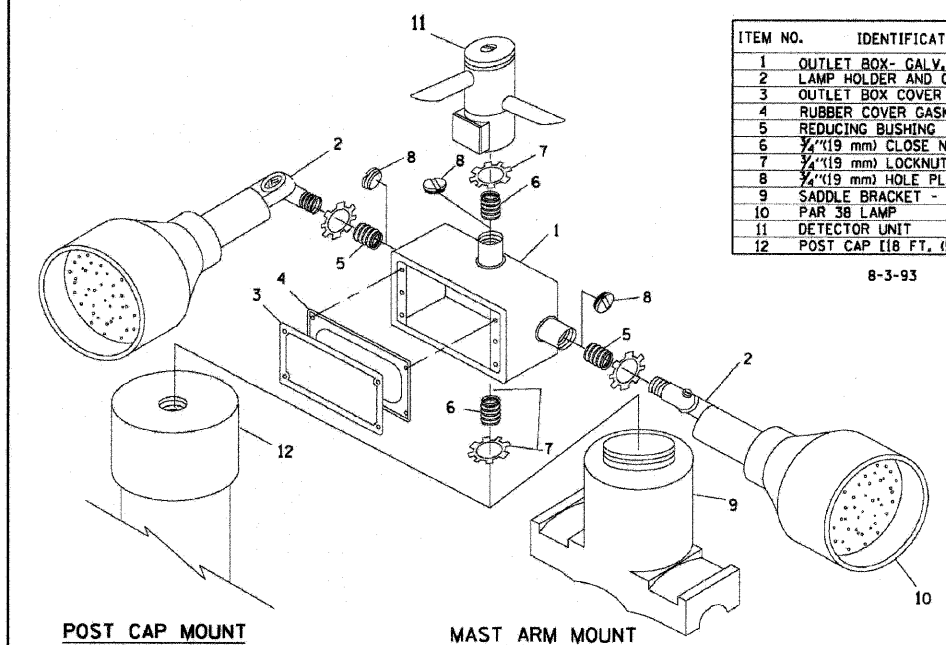
**SECTION A-A**

**NOTES:**

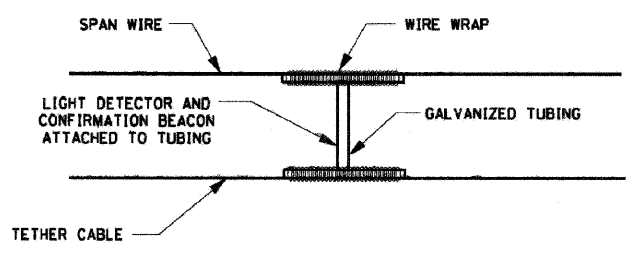
- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
- ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT  
 ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT  
 ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
- WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.

| ITEM NO. | IDENTIFICATION                               |
|----------|--|
| 1        | OUTLET BOX - GALV. 21 CU.IN. (0.000344 CU-M) |
| 2        | LAMP HOLDER AND COVER                        |
| 3        | OUTLET BOX COVER                             |
| 4        | RUBBER COVER GASKET                          |
| 5        | REDUCING BUSHING                             |
| 6        | 3/4" (19 mm) CLOSE NIPPLE                    |
| 7        | 3/4" (19 mm) LOCKNUT                         |
| 8        | 3/4" (19 mm) HOLE PLUG                       |
| 9        | SADDLE BRACKET - GALV.                       |
| 10       | PAR 38 LAMP                                  |
| 11       | DETECTOR UNIT                                |
| 12       | POST CAP [18 FT. (5.4 m) POST MIN.]          |

8-3-93



**EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL**



**LIGHT DETECTOR AND CONFIRMATION BEACON MOUNTING FOR TEMPORARY TRAFFIC SIGNALS**

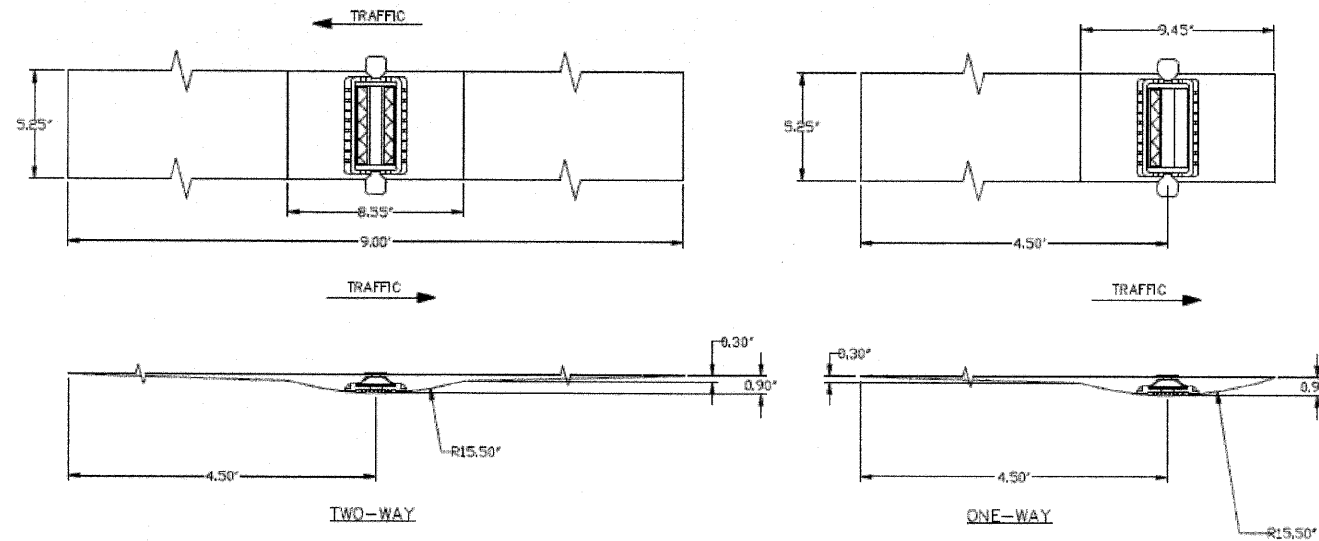
(NOT TO SCALE)

PRINTED DATE: 10/16/2009  
 PLOT DATE: 10/16/2009  
 PLOT SCALE: 1/8" = 1'-0"  
 USER NAME: bhpawd

| NO. | DATE | DESCRIPTION OF REVISIONS | NO. | DATE | DESCRIPTION OF REVISIONS |
|-----|------|--------------------------|-----|------|--------------------------|
|     |      |                          |     |      |                          |

|                        |           |
|------------------------|-----------|
| ESI PROJECT NO. 09-280 | ENGR. MR  |
| FILE: 39 Details 12    | TECH: DN  |
| DATE: 09-25-09         | QA/QC: JS |

## RECESSED REFLECTIVE PAVEMENT MARKERS



## PAVEMENT MARKINGS, SIGNING

RECESSED REFLECTIVE PAVEMENT MARKERS SHALL BE INSTALLED AS FOLLOWS:

- TWO (2) TWO-WAY AMBER RECESSED PAVEMENT MARKERS SHALL BE PLACED AT FORTY FEET (40') C-C ALONG DOUBLE YELLOW CENTERLINES AT INTERSECTIONS AND CURVES. IN TANGENT SECTIONS THE RECESSED PAVEMENT MARKERS WILL BE AT EIGHTY FEET (80') CENTERS.
- TWO (2) ONE-WAY CRYSTAL RECESSED MARKERS SHALL BE PLACED AT (10') APART AT (80') C-C ALONG LANE LINES BETWEEN WHITE DASHES AT INTERSECTIONS AND CURVES. IN TANGENT SECTIONS, ONLY ONE (1) CRYSTAL RECESSED PAVEMENT MARKERS SHALL BE PLACED AT (80') C-C.
- TURN BAY LINES SHALL HAVE ONE-WAY CRYSTAL MARKERS PLACED AT FORTY FEET (40') C-C.
- TWO-WAY AMBER MARKERS SHALL BE USED WHEN THE PAINTED MEDIAN IS LESS THAN OR EQUAL TO FOUR FEET (4') IN WIDTH; ONE-WAY AMBER MARKERS SHALL BE USED WHEN THE PAINTED MEDIAN IS GREATER THAN FOUR FEET (4') IN WIDTH.
- CRYSTAL/RED MARKERS SHALL BE PLACED AT LANE LINES AND TURN BAYS ON DIVIDED HIGHWAYS AND HIGHWAYS WITH RAISED MEDIANS.

TURNING LANES 150 TO 199 FEET SHALL HAVE AN ADDITIONAL ARROW PLACED PRIOR TO THE END OF THE TURN LANE.

TURNING LANES 200 FEET AND LONGER SHALL HAVE AN ADDITIONAL ARROW AND ONLY PLACED PRIOR TO THE END OF THE LANE.

SKIP DASHED AND YELLOW CENTERLINES SHALL BE GAPPED AT SIDE ROAD INTERSECTIONS. THE GAP SHALL BEGIN AND END AT A FORTY FOOT TO FORTY-FIVE FOOT RADIUS POINT FROM THE CENTERLINE OF THE SIDE ROAD.

ALL FOUR INCH (4") AND SIX INCH (6") LONGITUDINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC.

CROSS WALK PAVEMENT MARKINGS SHALL BE INSTALLED AS FOLLOWS:

- TWO (2) SIX INCH (6") PARALLEL LINES SIX FOOT (6') APART SHALL BE USED ON SIDE ROADS AND AT SIGNALIZED INTERSECTIONS WITHOUT SCHOOL CROSSINGS.
- NEAR ALL SCHOOL AND NON-SIGNALIZED CROSS WALKS ACROSS COUNTY HIGHWAYS, A SERIES OF TWELVE INCH (12") WIDE, SIX FEET (6') LONG LINES AT TWENTY-FOUR INCH (24") C-C SPACING SHALL BE USED.
- ALL BICYCLE AND EQUESTRIAN PATH CROSSINGS SHALL INCLUDE TWO (2) TWELVE INCH (12") PARALLEL LINES AND A SERIES OF TWELVE INCH (12") WIDE LINES AT 45° AT THIRTY-SIX INCH (36") C-C SPACING.

TEMPORARY PAVEMENT MARKING DELINEATION SHALL BE AS FOLLOWS:

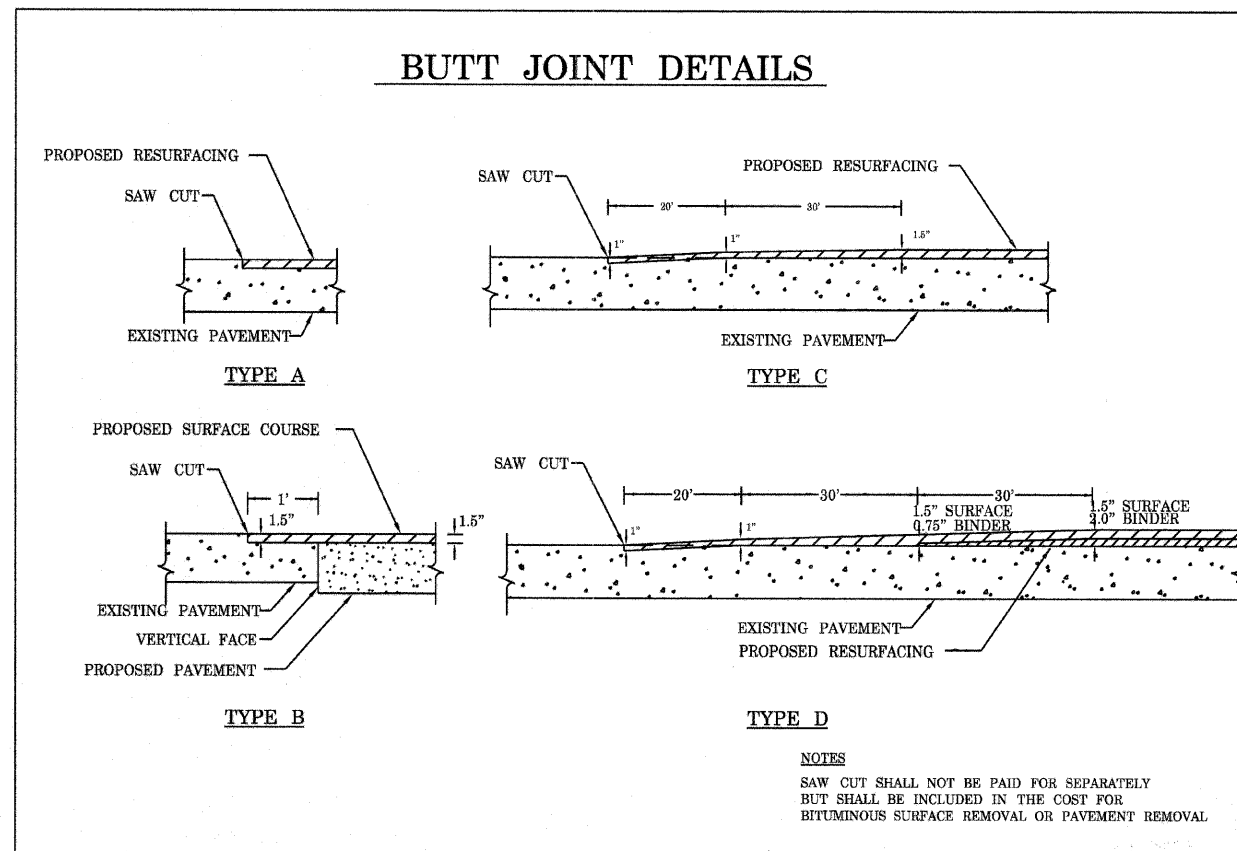
FOUR INCH (4") CENTERLINES, LANE LINES, TURN BAY LINES, PAINTED MEDIANS AND TWENTY-FOUR INCH (24") STOP BARS. THE MARKINGS SHALL BE PAINTED AND PLACED THE SAME DAY AS BITUMINOUS SURFACE REMOVAL.

TEMPORARY PAVEMENT MARKINGS SHALL USE A CONTINUOUS DOUBLE YELLOW LINE FOR THE CENTERLINE FOLLOWING BITUMINOUS SURFACE REMOVAL.

ALL SHORT TERM PAVEMENT MARKINGS WILL BE PLACED AS DIRECTED BY THE ENGINEER.

THE CONTRACTOR SHALL COORDINATE WITH THE DU PAGE COUNTY DOT FOR THE RELOCATION OF PERMANENT ROADWAY SIGNS DURING CONSTRUCTION.

## BUTT JOINT DETAILS



### NOTES

SAW CUT SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST FOR BITUMINOUS SURFACE REMOVAL OR PAVEMENT REMOVAL.

PRINTED DATE: 10/16/2009 10:00 AM PROJECT: 40 Details 13