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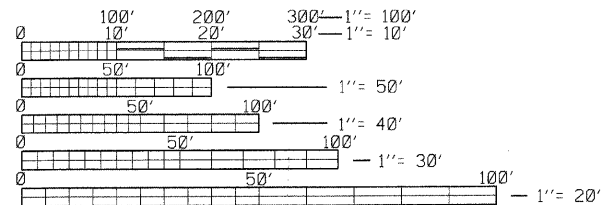
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**THIS IMPROVEMENT IS LOCATED IN WAUKEGAN**

**TRAFFIC DATA**

ADT COUNT (YR)  
 JACKSON ST. 10,700 (2000)

POSTED SPEED DESIGN SPEED  
 30 MPH 30 MPH



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

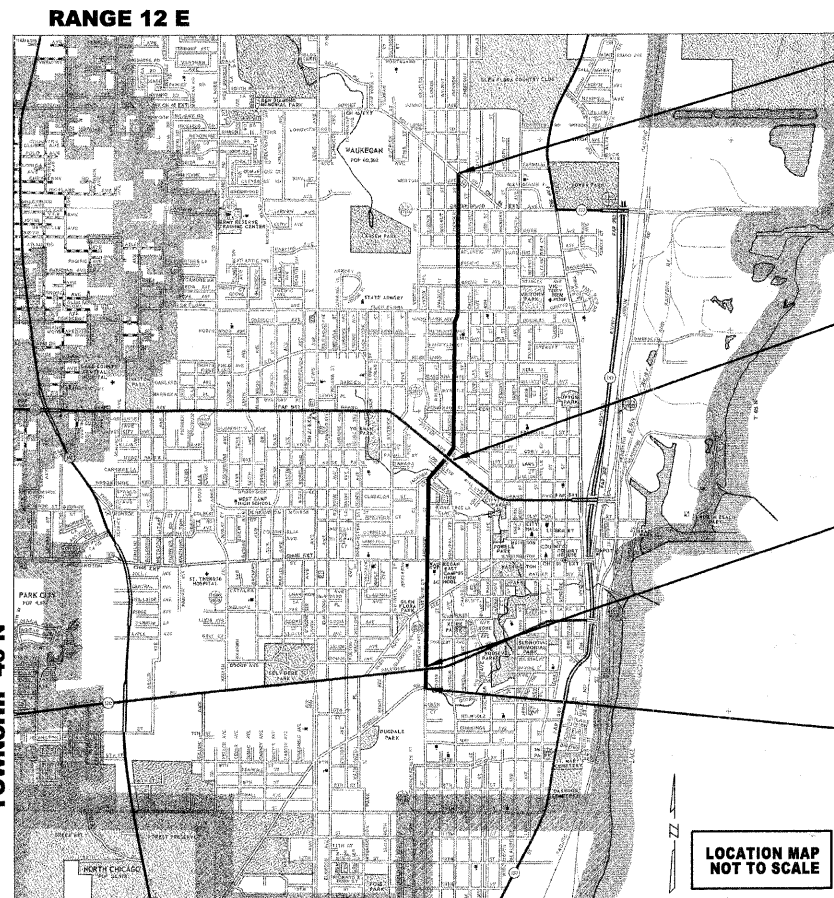
**J.U.L.I.E.**  
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
 CALL 811

Know what's below.  
 Call before you dig.

**CITY OF WAUKEGAN ENGINEERING DEPARTMENT**  
 100 N. MARTIN LUTHER KING JR AVE  
 WAUKEGAN, IL 60085  
 (847) 625-6858

**PROJECT ENGINEER: RON LAUBACH**  
**PROJECT MANAGER: RON LAUBACH**  
**CONTRACT NO. 63342**

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
**DIVISION OF HIGHWAYS**  
**PROPOSED**  
**HIGHWAY PLANS**  
**FAU ROUTE 2738 (JACKSON STREET)**  
**SECTION 09-00235-00-RS**  
**PROJECT NO. ARA-9003(462)**  
**JACKSON ST - DUGDALE RD to GOLF RD**  
**LOCAL AGENCY PAVEMENT PROJECT**  
**LAKE COUNTY**  
**JOB NO. C-91-896-09 3rd P.M.**



**PROJECT ENDS STATION 139+50.5**

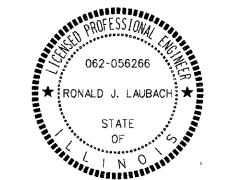
**OMISSION STATION 64+77.4 TO 65+66.2**

**OMISSION STATION 7+25.3 TO 8+77.2**

**PROJECT START STATION 2+66.4**

LOCATION MAP NOT TO SCALE

TOWNSHIP 45 N



SIGNATURE: *R. Laubach*  
 DATE: OCTOBER 15, 2009  
 LICENSE EXP. NOVEMBER 30, 2009

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2738	09-00235-00-RS	LAKE	16	1
		ILLINOIS	CONTRACT NO. 63342	



**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
**DIVISION OF HIGHWAYS**

APPROVED *October 15, 2009*  
*R. Laubach*  
 CITY OF WAUKEGAN, CITY ENGINEER

PASSED *OCTOBER 26, 2009*  
*Cheryl Chasnovsky*  
 DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID BASED ON LIMITED REVIEW  
*October 26, 2009*  
*Diane M. O'Keefe*  
 DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

**PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS**

ASSOCIATE FIELD ENGINEER : KEVIN STALLWORTH (847) 705-4169

GROSS LENGTH OF IMPROVEMENT = 13684.10 FT. (2.592 MILE)  
 OMISSION (RT 120 AND RT 132) = 240.7 FT.  
 NET LENGTH OF IMPROVEMENT = 13443.40 FT. (2.546 MILE)

GENERAL NOTES

- 1) BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED UTILITIES. (48 HOUR NOTIFICATION REQUIRED)
- 2) THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF WAUKEGAN.
- 3) BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS UNLESS OTHERWISE SPECIFIED.
- 4) WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H)
- 5) BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
- 6) DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- 7) IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- 8) THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- 9) SAW CUTTING PRIOR TO ANY REMOVAL ITEMS NOTED ON THE PLANS OR DIRECTED BY THE ENGINEER SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEMS BEING REMOVED.
- 10) THE CONTRACTOR SHALL USE CARE IN REMOVING OR EXCAVATING NEAR ALL EXISTING ITEMS WHICH WILL REMAIN. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- 11) MATCH EXISTING PAVEMENT MARKINGS AT THE PROJECT AND OMISSION LIMITS.
- 12) ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
  - A) THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.
  - B) THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR. THIS WORK SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
  - C) THE CONTRACTOR WILL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MIGHT NOT BE SHOWN ON THE PLANS. ANY UTILITY PROPERTY DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE OWNER AT THE CONTRACTOR'S EXPENSE.
  - D) ALL UTILITY COMPANIES SHALL BE NOTIFIED AT LEAST 3 DAYS PRIOR TO THE START OF CONSTRUCTION.
- 13) QUANTITIES FOR PATCHING SHALL NOT EXCEED THOSE PROVIDED IN THE SUMMARY OF QUANTITIES UNLESS APPROVED BY THE ENGINEER. THE ENGINEER WILL IDENTIFY FINAL PATCH LOCATIONS IN THE FIELD. FULL DEPTH PATCHING SHALL TAKE PLACE AFTER THE MILLING OPERATION.
- 14) LIMITS OF RESURFACING ON ALL SIDE STREETS ARE TO BE AT THE RADIUS RETURN OR AS DIRECTED BY THE ENGINEER.
- 15) ALL PAVEMENT MARKING SHALL BE PLACED THROUGHOUT THE PROJECT ACCORDING TO DISTRICT 1 TYPICAL PAVEMENT MARKING.
- 16) ALL HMA PAVEMENT PATCHING SHALL BE CLASS D.

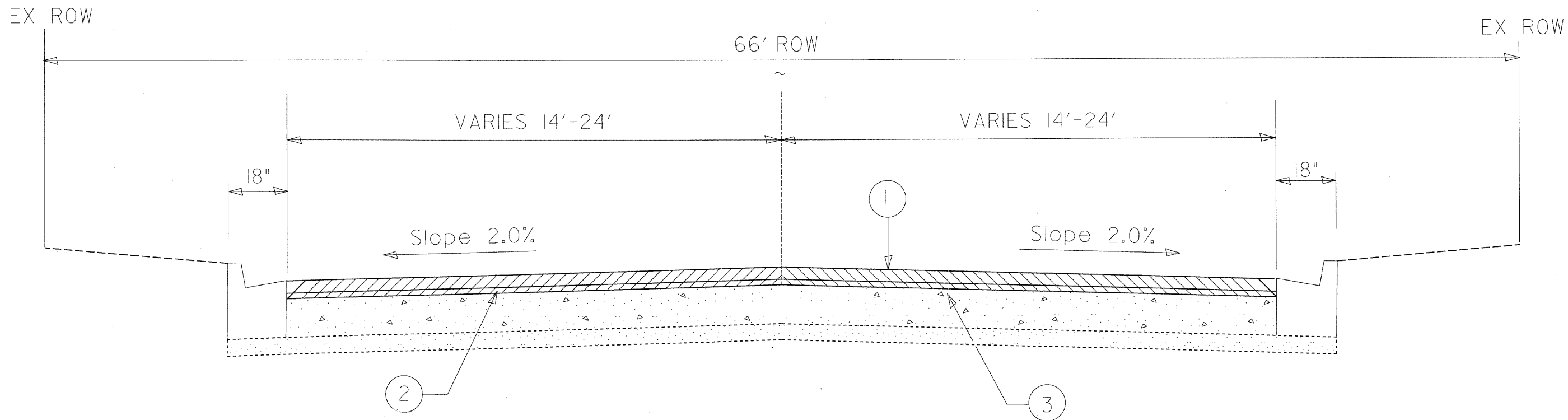
SUMMARY OF QUANTITIES

ITEM No	ITEM DESCRIPTION	UNIT	Quantity
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	5,760
40600300	AGGREGATE (PRIME COAT)	TON	115
40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	3,226
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	710
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	4,838
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	57,600
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	300
44201741	CLASS D PATCHES, TYPE II, 8 INCH	SQ YD	59
44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQ YD	16
44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	1,009
60266610	VALVE BOXES TO BE ADJUSTED (SPECIAL)	EACH	30
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	231
67100100	MOBILIZATION	L SUM	1
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	1,900
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	800
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	12,000
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	750
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	100
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	100
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	400
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	1,220
Z0017700	DRAINAGE & UTILITY STRUCTURES TO BE RECONSTRUCTED	EACH	23

\* SPECIALTY ITEM

HIGHWAY STANDARDS

000001-05	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
442201-03	CLASS C AND D PATCHES
701301-03	LANE CLOSURE, 2L 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L 2W, MOVING OPERATIONS-DAY ONLY
701501-05	URBAN LANE CLOSURE, 2L 2W, UNDIVIDED
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUTS FOR DETECTION LOOPS



**PROPOSED TYPICAL SECTION**  
 STA 2+66.4 TO STA 139+50.5, JACKSON STREET

CONTRACTOR SHALL MILL BEFORE PATCHING

DESCRIPTION	MIX TYPE	PERCENT AIR VOID @ Ndes
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	IL 9.5MM	4% @ 70 Gyr.
LEVELING BINDER (MACHINE METHOD), N70	IL 9.5MM	4% @ 70 Gyr.
CLASS D PATCHES (HMA BINDER IL-19MM)	IL 19MM	4% @ 70 Gyr.

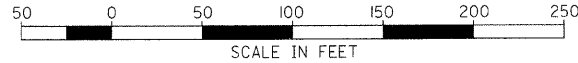
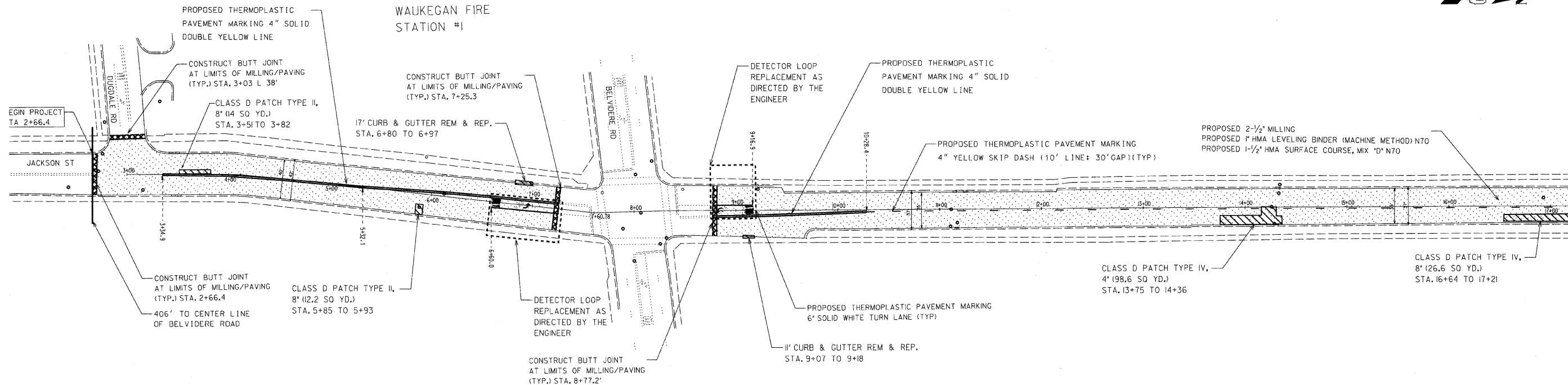
THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQYD/IN

FOR "PERCENT RAP" SEE DISTRICT ONE SPECIAL PROVISIONS

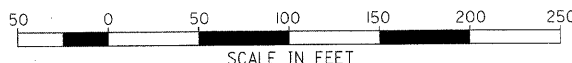
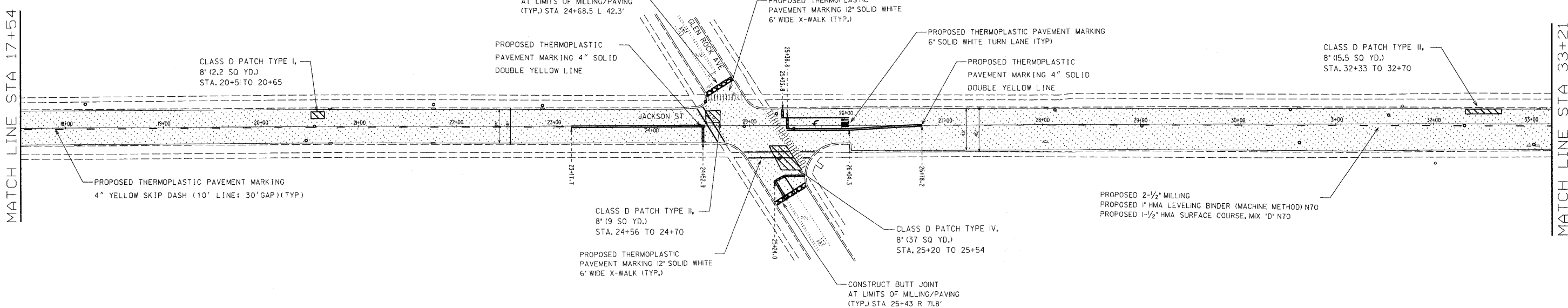
THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

**PROPOSED LEGEND**

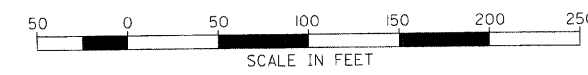
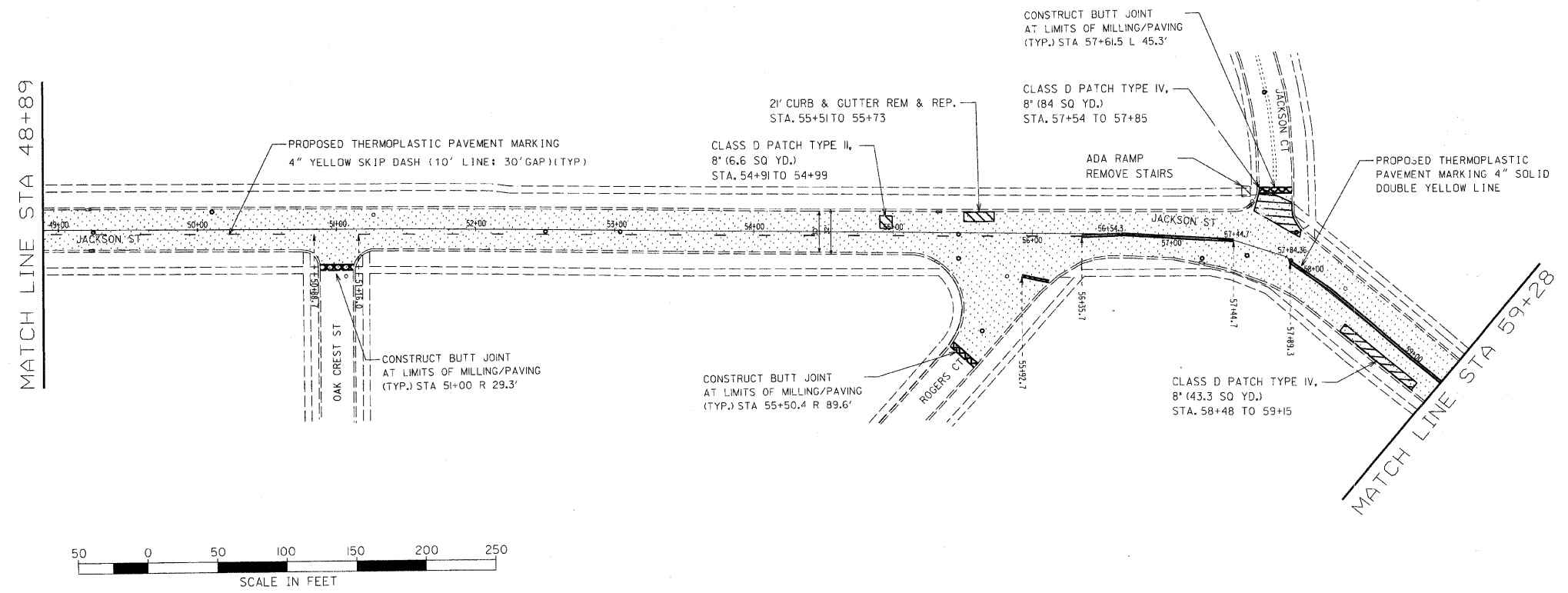
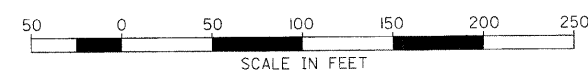
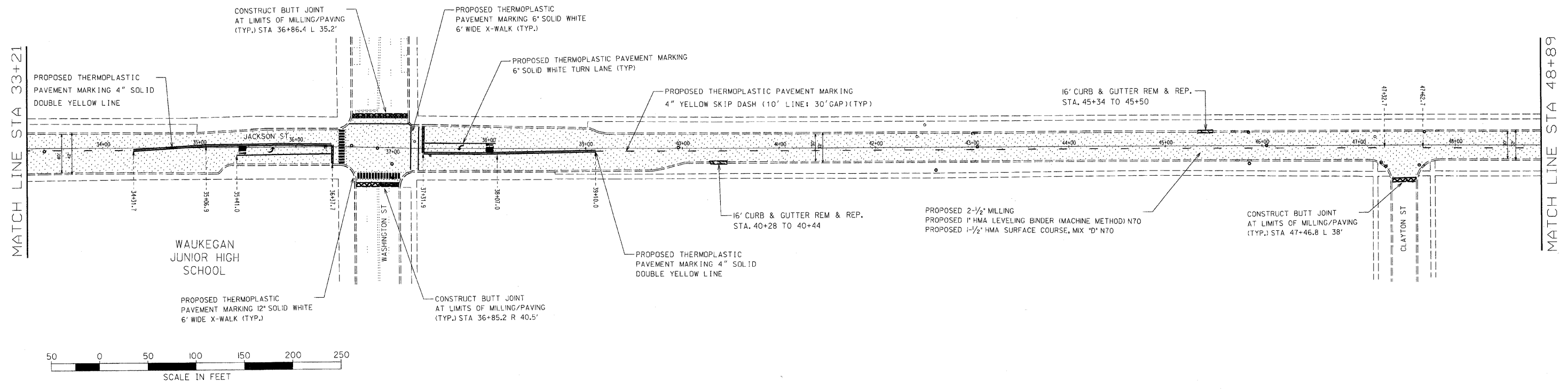
- ① HOT-MIX ASPHALT SURFACE COURSE, MIX D, N70, 1-1/2"
- ② LEVELING BINDER (MACHINE METHOD), N70, 1"
- ③ BITUMINOUS MATERIALS (PRIME COAT)
- ④ INDICATES HMA SURFACE REMOVAL (MILLING) 2-1/2"



NOTE:  
 1. DETECTOR LOOP PLANS IN BACK SEE SHEET 13  
 2. EXIST. PAVT MARKINGS TO BE REPLACED IN-KIND



DESIGNED - RHL DRAWN - PTO CHECKED - DATE - 08/21/09	REVISED - REVISED - REVISED - REVISED -	<b>STATE OF ILLINOIS          DEPARTMENT OF TRANSPORTATION</b>	<b>JACKSON STREET LAPP PROJECT          DUGDALE ROAD TO GOLF ROAD</b>		F.A.U. RTE. 2738	SECTION 09-00235-00-RS	COUNTY LAKE	TOTAL SHEETS 16	SHEET NO. 4			
			SCALE: 1:50		SHEET NO. 3 OF 10 SHEETS	STA. 3+00 TO STA. 33+21	CONTRACT NO. 63342					
			ILLINOIS FED. AID PROJECT									



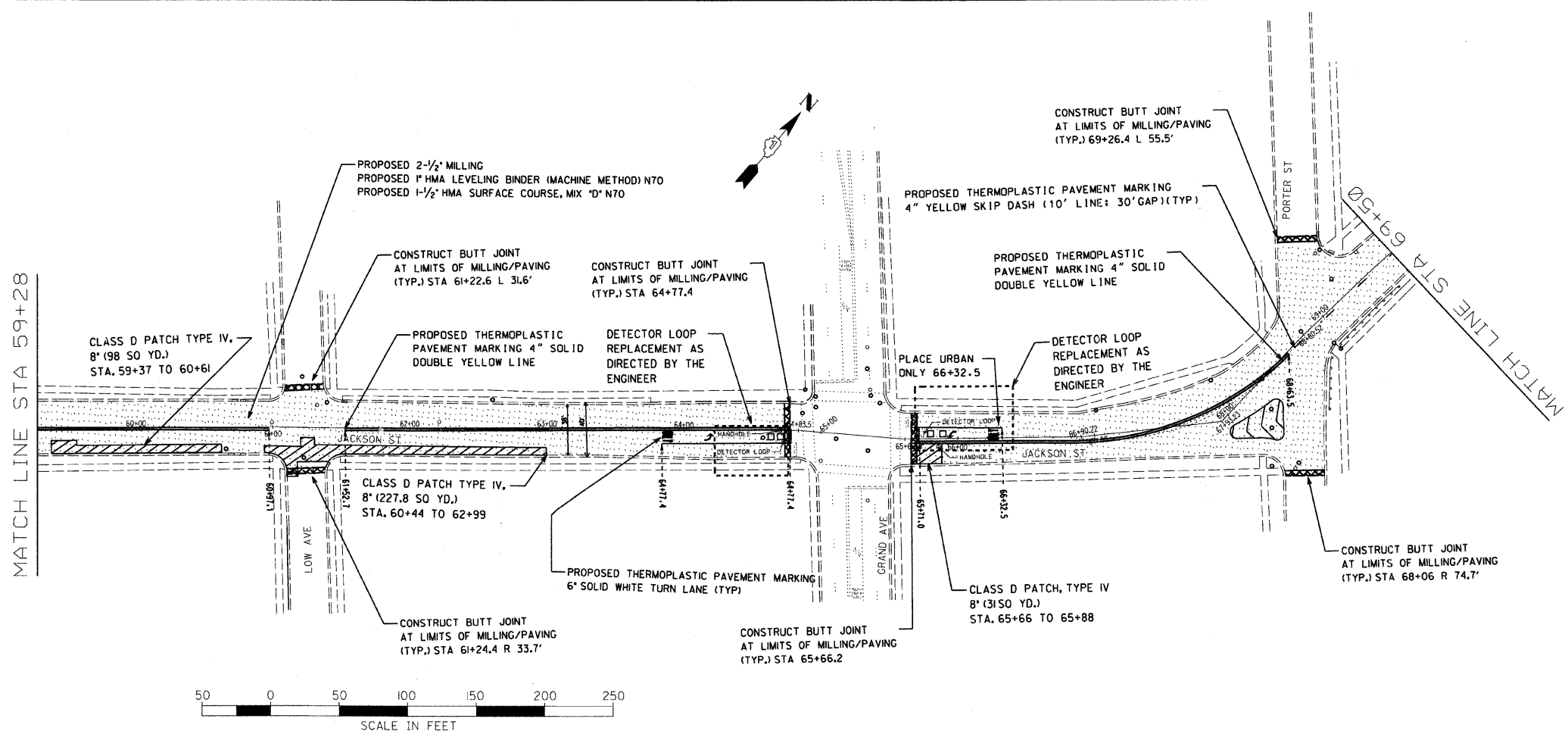
DESIGNED	-	RHL	REVISED	-	_____
DRAWN	-	PTO	REVISED	-	_____
CHECKED	-		REVISED	-	_____
DATE	-	08/21/09	REVISED	-	_____

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

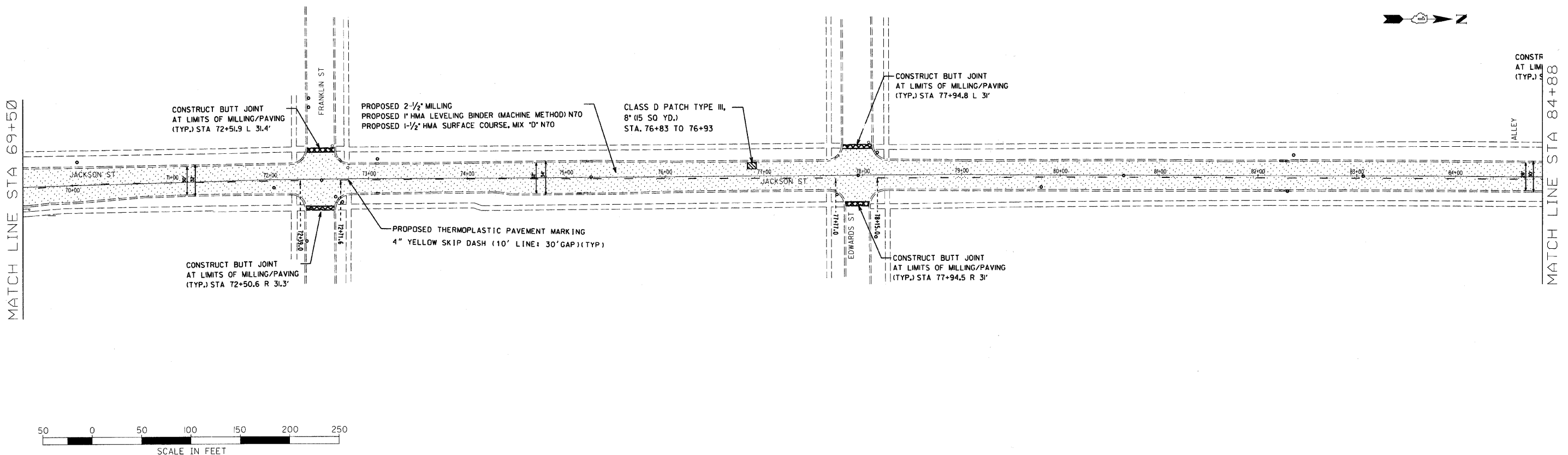
**JACKSON STREET LAPP PROJECT  
DUGDALE ROAD TO GOLF ROAD**

SCALE: 1:50      SHEET NO. 4 OF 10 SHEETS      STA. 33+21 TO STA. 59+23

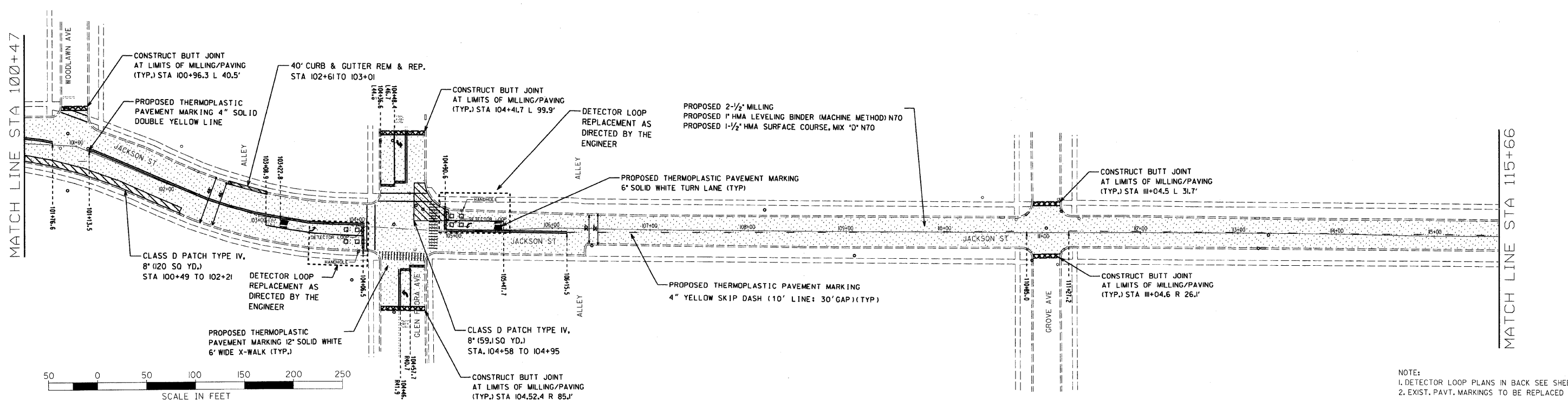
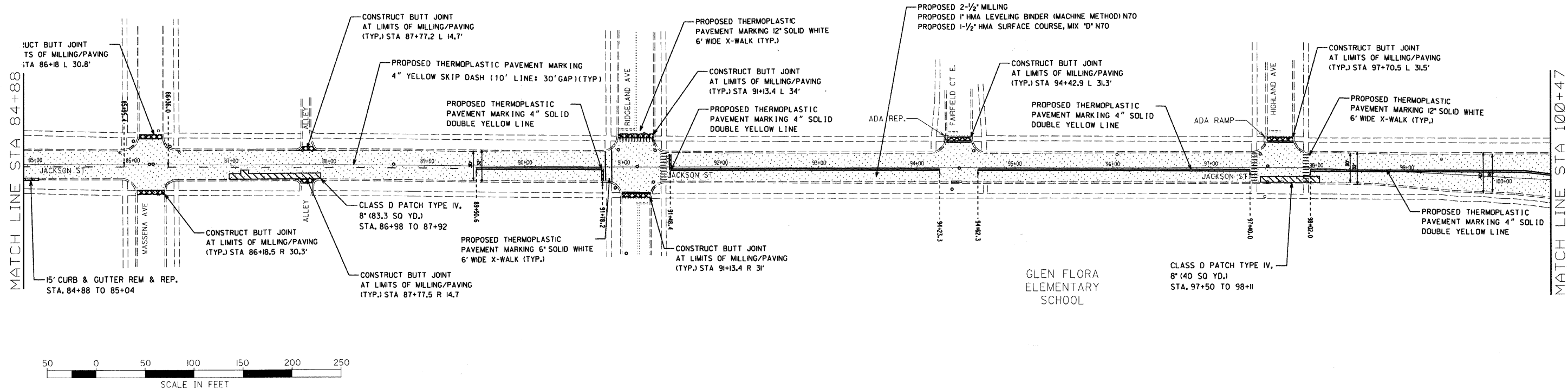
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
273B	09-00235-00-RS	LAKE	16	5
CONTRACT NO. 63342				
ILLINOIS FED. AID PROJECT				



NOTE:  
 1. DETECTOR LOOP REPLACEMENT AS DIRECTED BY ENGINEER  
 2. EXIST. PAVT. MARKINGS TO BE REPLACED IN-KIND  
 3. PROPOSED THERMOPLASTIC PAVEMENT MARKING.  
 TRAFFIC CHANNELIZING ISLAND, 8" SOLID WHITE BORDER WITH 12" SOLID WHITE DIAGONAL STRIPING.



DESIGNED - RHL	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>JACKSON STREET LAPP PROJECT DUGDALE ROAD TO GOLF ROAD</b>		F.A.U. RTE. 2738	SECTION 09-00235-00-RS	COUNTY LAKE	TOTAL SHEETS 16	SHEET NO. 6	
DRAWN - PTO	REVISED -		SCALE: 1:50	SHEET NO. 5 OF 10 SHEETS	STA. 59+23 TO STA. 84+25	CONTRACT NO. 63342				
CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT							
DATE - 08/21/09	REVISED -									



NOTE:  
 1. DETECTOR LOOP PLANS IN BACK SEE SHEET 14  
 2. EXIST. PAVT. MARKINGS TO BE REPLACED IN-KIND

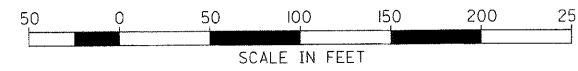
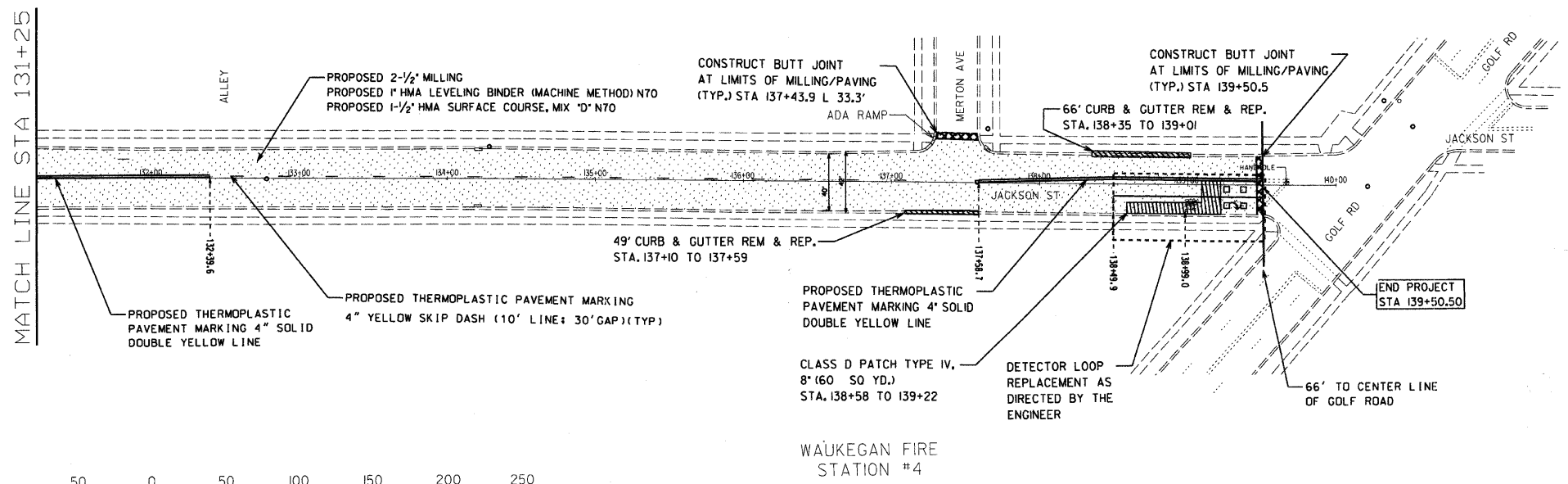
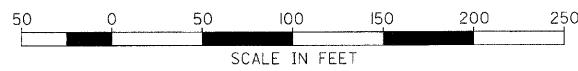
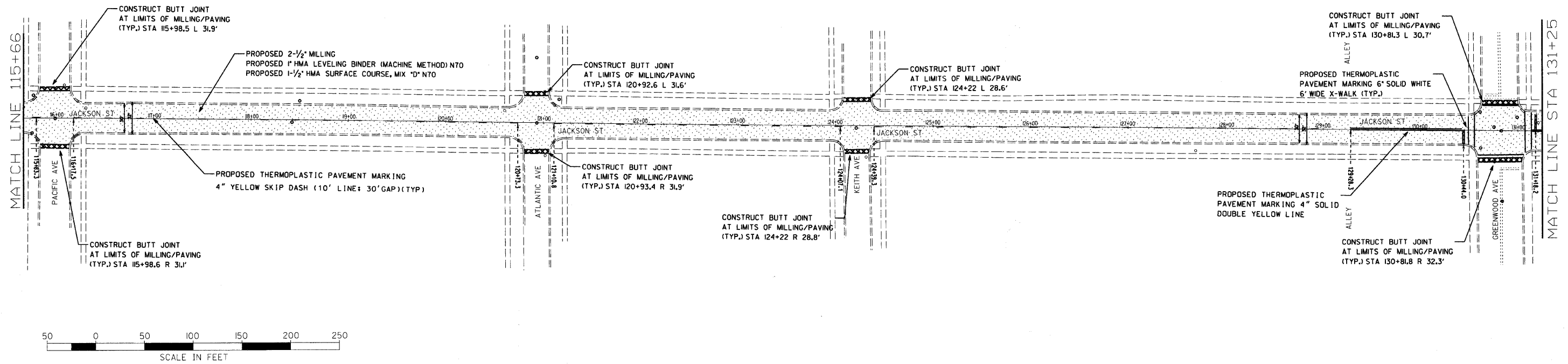
DESIGNED	RHL	REVISED	
DRAWN	PTO	REVISED	
CHECKED		REVISED	
DATE	08/21/09	REVISED	

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**JACKSON STREET LAPP PROJECT  
 DUGDALE ROAD TO GOLF ROAD**

SCALE: 1:50      SHEET NO. 6 OF 10 SHEETS      STA. 84+25 TO STA. 115+66

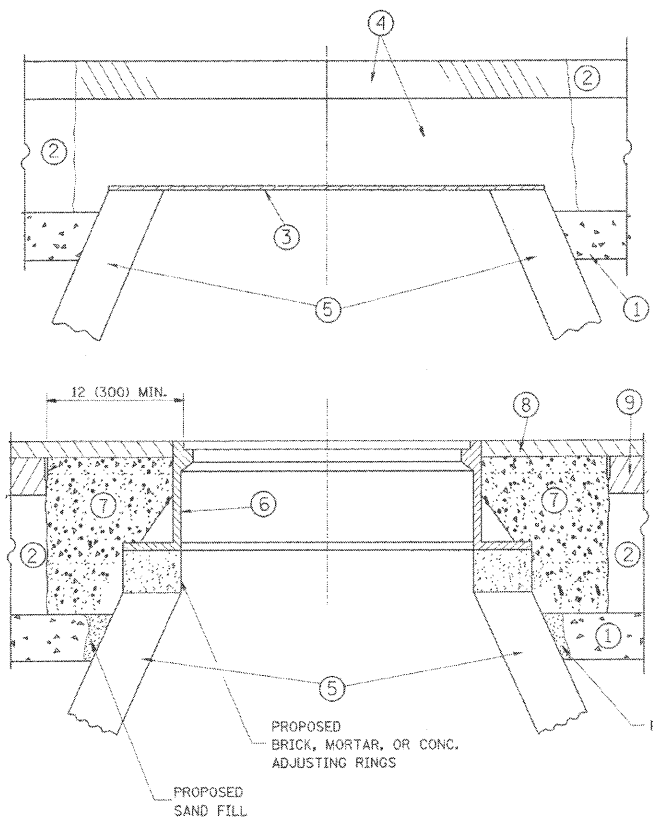
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2738	09-00235-00-RS	LAKE	16	7
CONTRACT NO. 63342			ILLINOIS FED. AID PROJECT	



NOTE:  
 1. DETECTOR LOOP PLANS IN BACK SEE SHEET 15  
 2. EXIST. PAVT. MARKINGS TO BE REPLACED IN-KIND

DESIGNED - RHL		REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>JACKSON STREET LAPP PROJECT DUGDALE ROAD TO GOLF ROAD</b>		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
DRAWN - PTO		REVISED -				2738	09-00235-00-RS	LAKE	16	8
CHECKED -		REVISED -				CONTRACT NO. 63342				
DATE - 08/21/09		REVISED -				ILLINOIS FED. AID PROJECT				
			SCALE: 1:50	SHEET NO. 7 OF 10 SHEETS	STA. 115+66 TO STA. 139+50					





**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:** THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL" NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

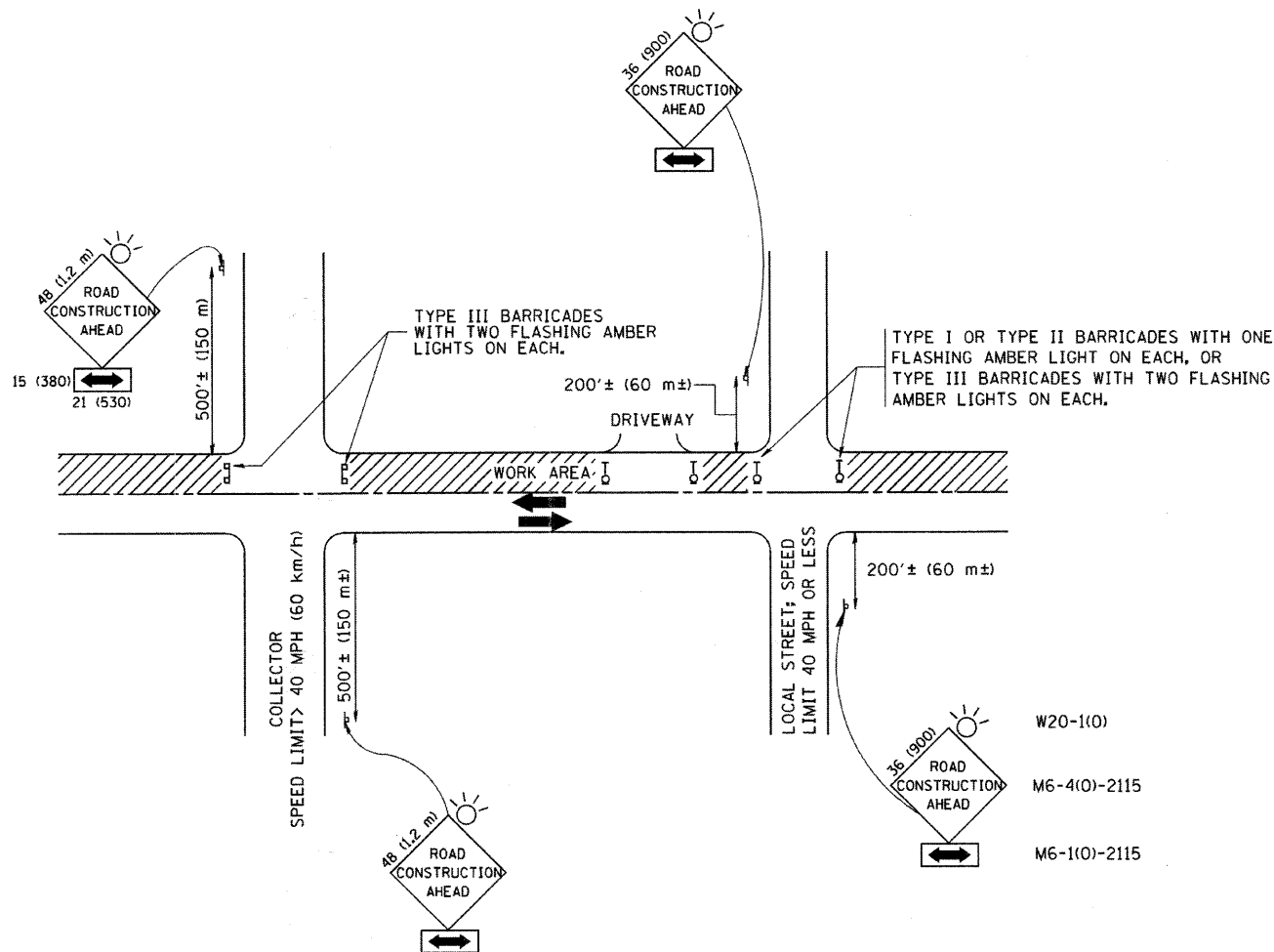
IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**



**TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

**NOTES:**

**A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS**

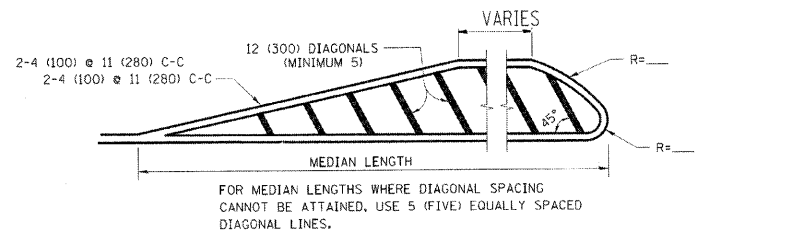
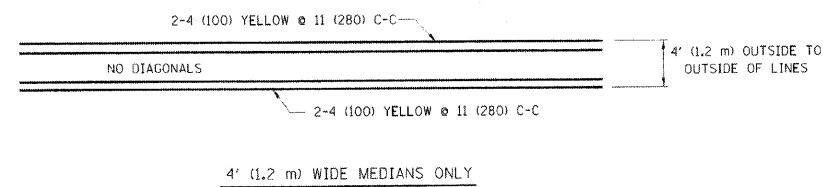
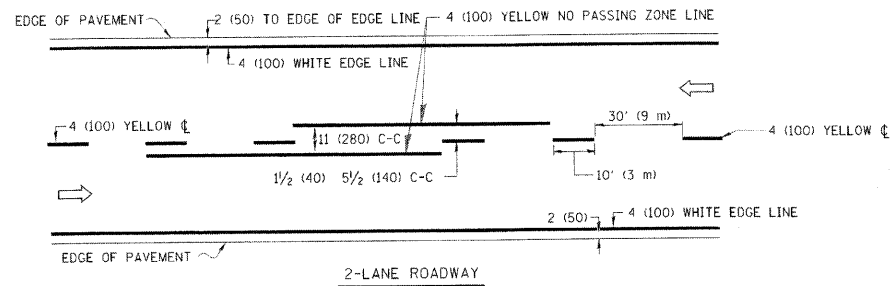
1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

**B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:**

- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

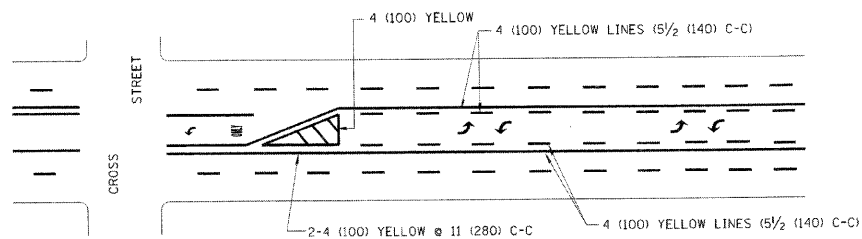
All dimensions are in millimeters (inches) unless otherwise shown.

DESIGNED - RHL	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>JACKSON STREET LAPP PROJECT DUGDALE ROAD TO GOLF ROAD</b>		F.A.U. RTE. 2738	SECTION 09-00235-00-RS	COUNTY LAKE	TOTAL SHEETS 16	SHEET NO. 9	
DRAWN - PTO	REVISED -		SCALE: N/A	SHEET NO. 8 OF 10 SHEETS	CONTRACT NO. 63342					
CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT							
DATE - 08/21/09	REVISED -									

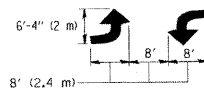


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

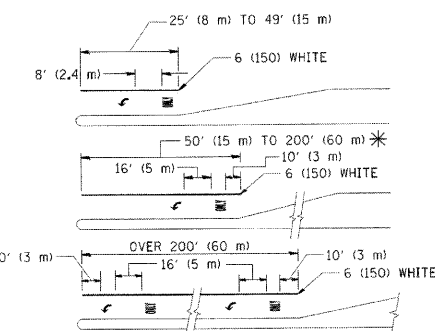


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

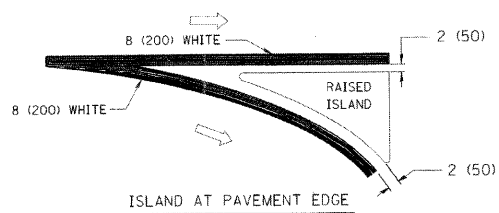
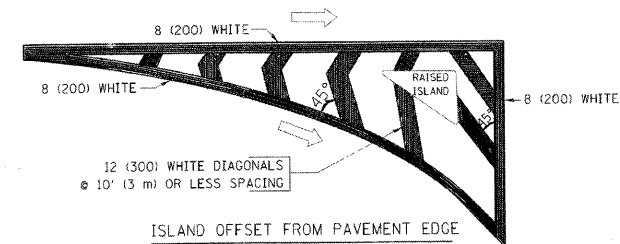
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
 \* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

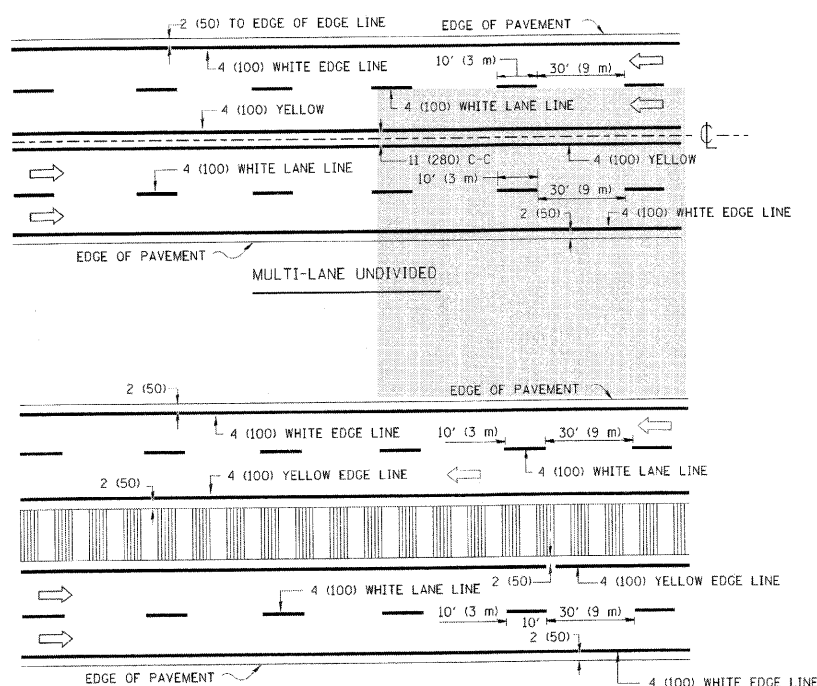


TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4 m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

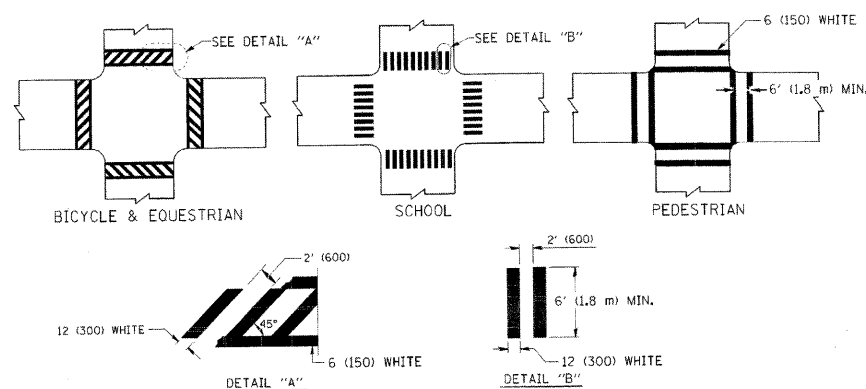
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.



NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

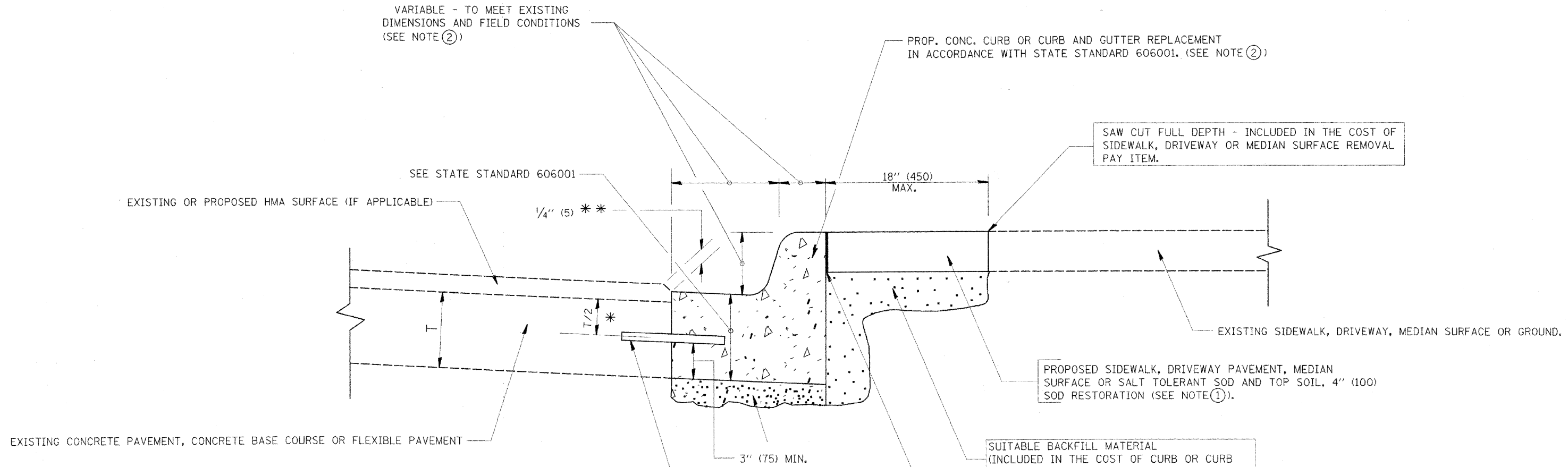
DESIGNED - RHL	REVISED -
DRAWN - PTO	REVISED -
CHECKED -	REVISED -
DATE - 08/21/09	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

JACKSON STREET LAPP PROJECT  
 DUGDALE ROAD TO GOLF ROAD

SCALE: N/A SHEET NO. 9 OF 10 SHEETS

F.A.U. RTE. 2738	SECTION 09-00235-00-RS	COUNTY LAKE	TOTAL SHEETS 16	SHEET NO. 10
CONTRACT NO. 63342				
ILLINOIS FED. AID PROJECT				



\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

\*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

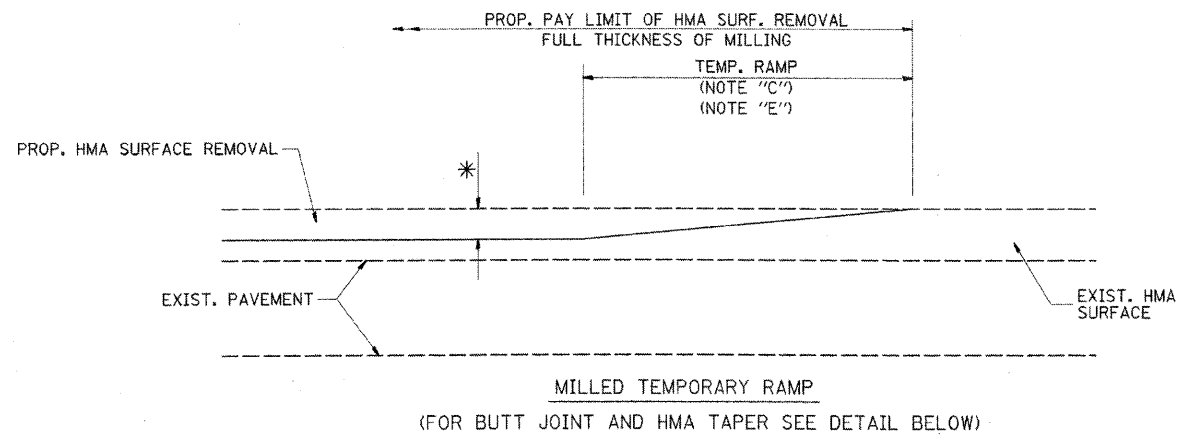
**BASIS OF PAYMENT:**

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

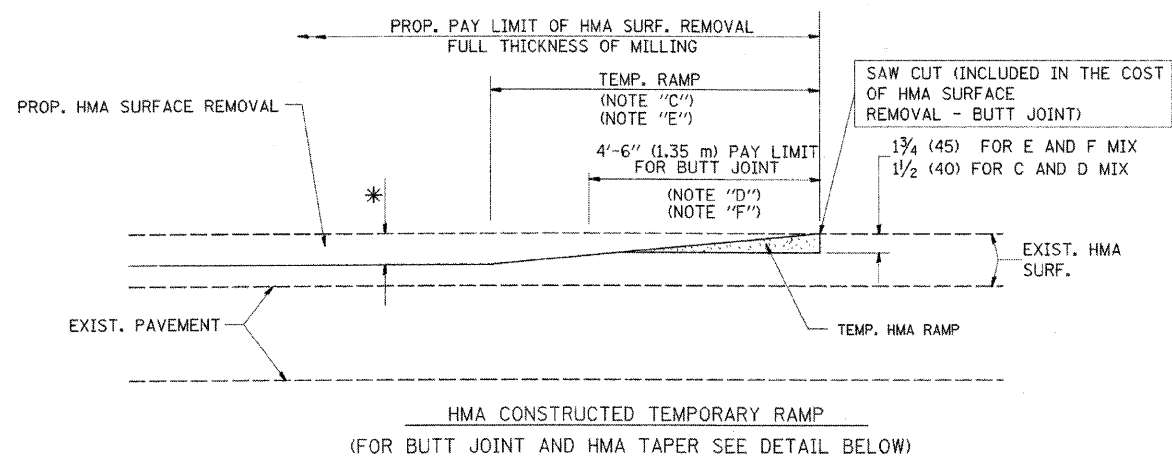
# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

DESIGNED - RHL	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>JACKSON STREET LAPP PROJECT DUGDALE ROAD TO GOLF ROAD</b>		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
DRAWN - PTO	REVISED -				2738	09-00235-00-RS	LAKE	16	11
CHECKED -	REVISED -				CONTRACT NO. 63342				
DATE - 08/21/09	REVISED -				ILLINOIS FED. AID PROJECT				
			SCALE: N/A	SHEET NO. 8 OF 10 SHEETS					

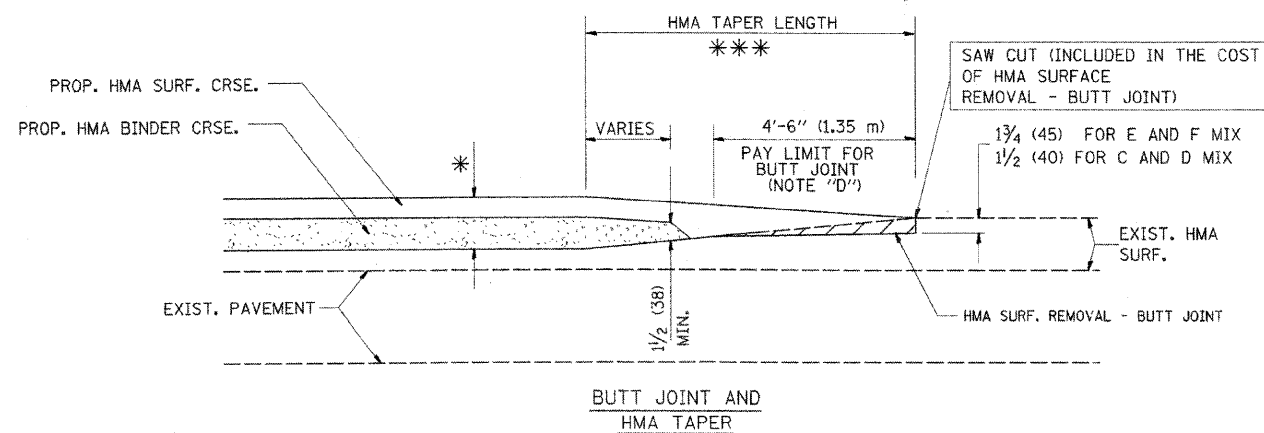


**OPTION 1**



**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".



ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

		DESIGNED - RHL	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>JACKSON STREET LAPP PROJECT DUGDALE ROAD TO GOLF ROAD</b>		F.A.U. RTE. -	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - PTO	REVISED -		SCALE: N/A	SHEET NO. 9 OF 10 SHEETS	2738	09-00235-00-RS	LAKE	16	12
		CHECKED -	REVISED -								CONTRACT NO. 63342
		DATE - 08/21/09	REVISED -								ILLINOIS FED. AID PROJECT

F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1225	03-00225-00-1L	LAKE	57	30
ILLINOIS				

63730

**LEGEND**

-  PAVEMENT MILLING & BITUMINOUS OVERLAY
-  SIDEWALK REMOVAL AND REPLACEMENT

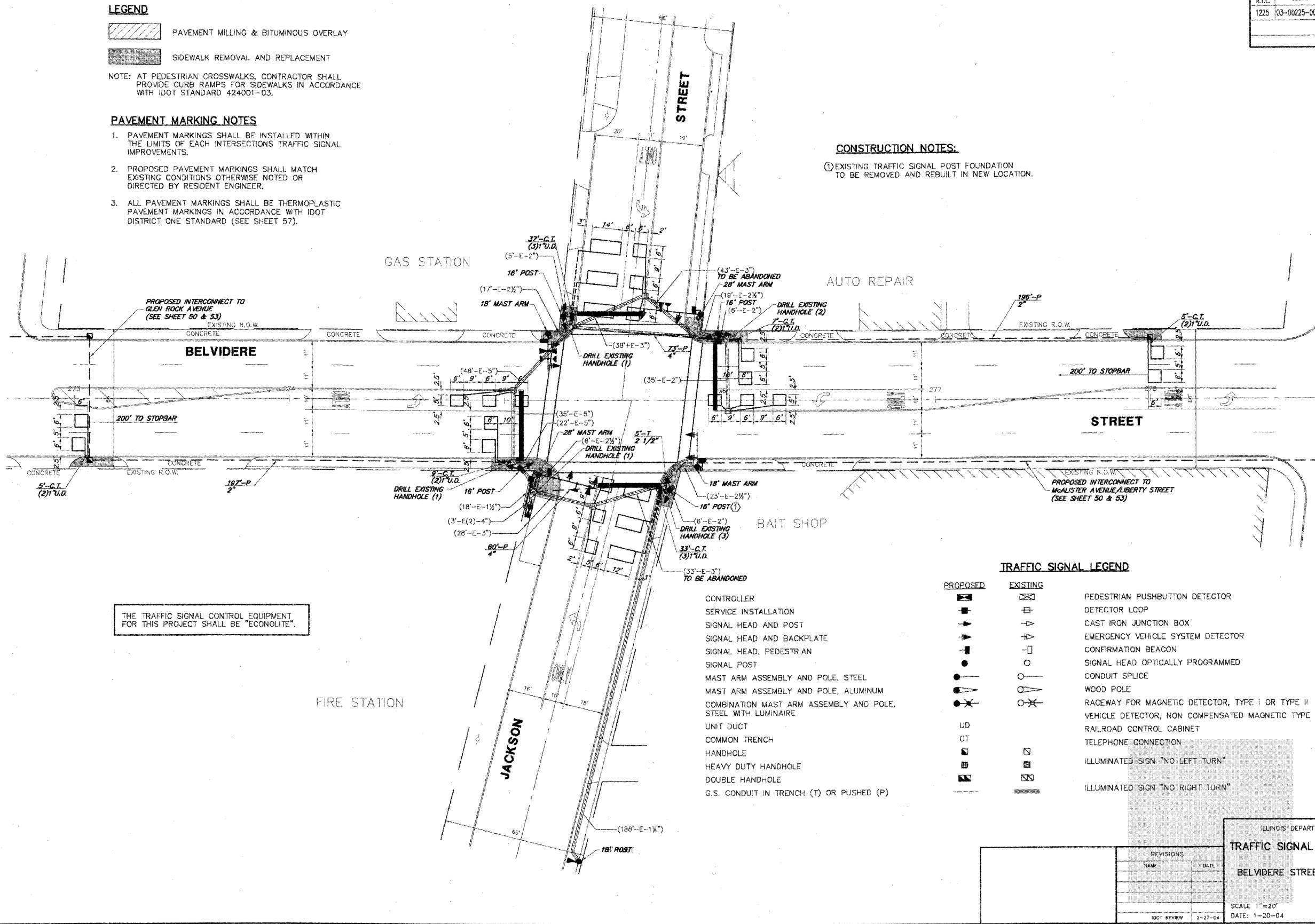
NOTE: AT PEDESTRIAN CROSSWALKS, CONTRACTOR SHALL PROVIDE CURB RAMP FOR SIDEWALKS IN ACCORDANCE WITH IDOT STANDARD 424001-03.

**PAVEMENT MARKING NOTES**

1. PAVEMENT MARKINGS SHALL BE INSTALLED WITHIN THE LIMITS OF EACH INTERSECTIONS TRAFFIC SIGNAL IMPROVEMENTS.
2. PROPOSED PAVEMENT MARKINGS SHALL MATCH EXISTING CONDITIONS OTHERWISE NOTED OR DIRECTED BY RESIDENT ENGINEER.
3. ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC PAVEMENT MARKINGS IN ACCORDANCE WITH IDOT DISTRICT ONE STANDARD (SEE SHEET 57).





























**CONSTRUCTION NOTES:**

- ① EXISTING TRAFFIC SIGNAL POST FOUNDATION TO BE REMOVED AND REBUILT IN NEW LOCATION.



THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE".

**TRAFFIC SIGNAL LEGEND**

PROPOSED	EXISTING	DESCRIPTION
		PEDESTRIAN PUSHBUTTON DETECTOR
		DETECTOR LOOP
		CAST IRON JUNCTION BOX
		EMERGENCY VEHICLE SYSTEM DETECTOR
		CONFIRMATION BEACON
		SIGNAL HEAD OPTICALLY PROGRAMMED
		CONDUIT SPLICE
		WOOD POLE
		RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II
		VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE
		RAILROAD CONTROL CABINET
		TELEPHONE CONNECTION
		ILLUMINATED SIGN "NO LEFT TURN"
		ILLUMINATED SIGN "NO RIGHT TURN"

ILLINOIS DEPARTMENT OF TRANSPORTATION

**TRAFFIC SIGNAL MODERNIZATION PLAN**

**BELVIDERE STREET AT JACKSON STREET**

REVISIONS	
NAME	DATE

SCALE 1"=20'  
DATE: 1-20-04

DRAWN BY: LJD  
DESIGNED BY: JRD  
CHECKED BY: BLS

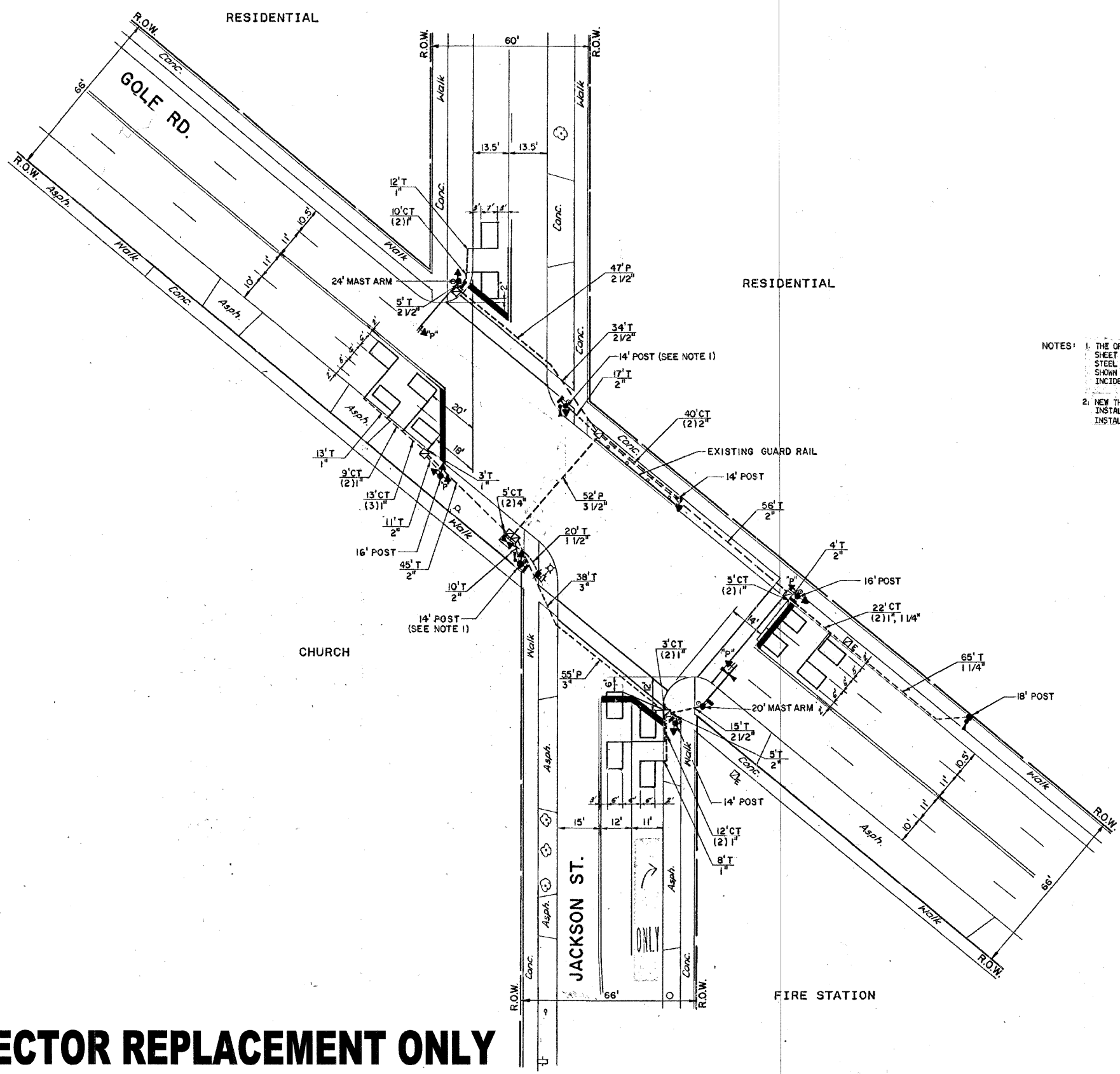
**FOR LOOP DETECTOR REPLACEMENT ONLY**

	DESIGNED - RHL	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>JACKSON STREET LAPP PROJECT DUGDALE ROAD TO GOLF ROAD</b>	F.A.U. R.T.E. 2738	SECTION 09-00235-00-RS	COUNTY LAKE	TOTAL SHEETS 16	SHEET NO. 13
	DRAWN - PTO	REVISED -	SCALE: N/A		SHEET NO. 10 OF 10 SHEETS		CONTRACT NO. 63342		
	CHECKED -	REVISED -	ILLINOIS FED. AID PROJECT						
	DATE - 08/21/09	REVISED -							



F.A.U. ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*		LAKE	38	20
STA.	TO STA.		FEDERAL AID PROJECT NO.	
			M-5003812	

\* STATE: 1981-064TS  
CITY: 83-00164-01-TL



NOTES: 1. THE OPTICOM DETECTOR UNIT SHALL BE INSTALLED AS SHOWN IN THE DETAIL ON SHEET 3. THE CONTRACTOR SHALL INSTALL A POST EXTENSION OF 1" GALVANIZED STEEL CONDUIT PAINTED YELLOW OF SUFFICIENT LENGTH TO OBTAIN THE HEIGHT SHOWN AND ALL MATERIAL AND LABOR REQUIRED TO DO THIS WORK SHALL BE INCIDENTAL TO THE ITEM "EMERGENCY VEHICLE PRIORITY SYSTEM DETECTOR UNIT".  
2. NEW THERMOPLASTIC PAVEMENT MARKING STOP AND CROSSWALK LINES ARE TO BE INSTALLED ON ALL LEGS. NEW CENTER AND LANE LINES ARE ALSO TO BE INSTALLED.

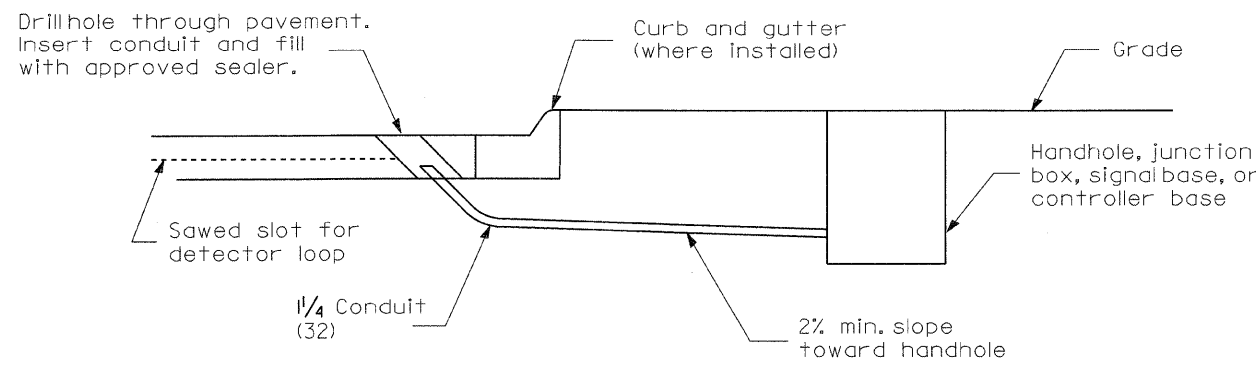
**FOR LOOP DETECTOR REPLACEMENT ONLY**



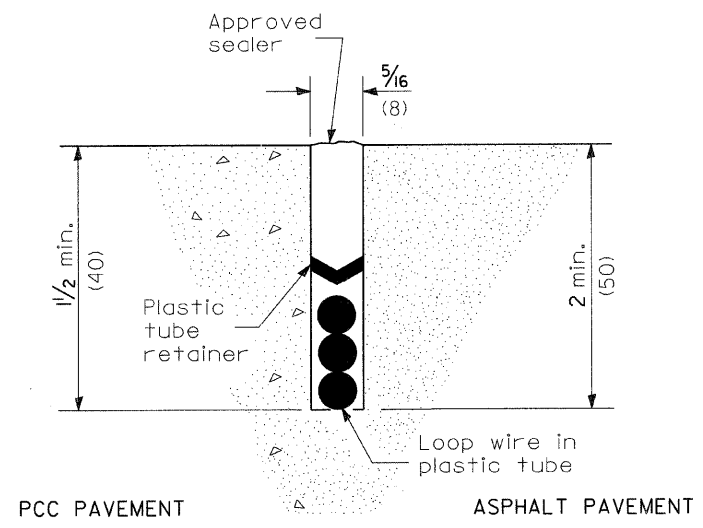
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
CITY OF WAUKEGAN  
JACKSON ST FAU 2738 &  
GOLF RD FAU 1215  
PROPOSED  
TRAFFIC SIGNAL MODERNIZATION  
SCALE 1" = 20'  
DATE APRIL 1983  
DRAWN BY W.D.  
CHECKED BY G.P.

JOB NO. 2629

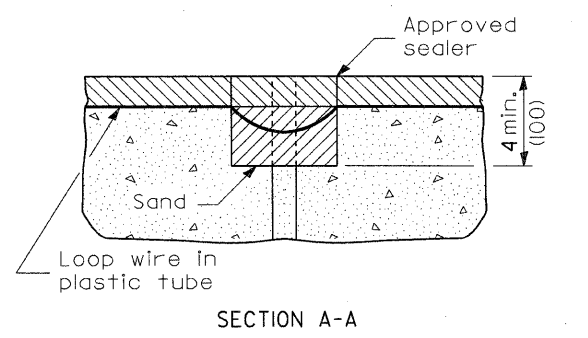
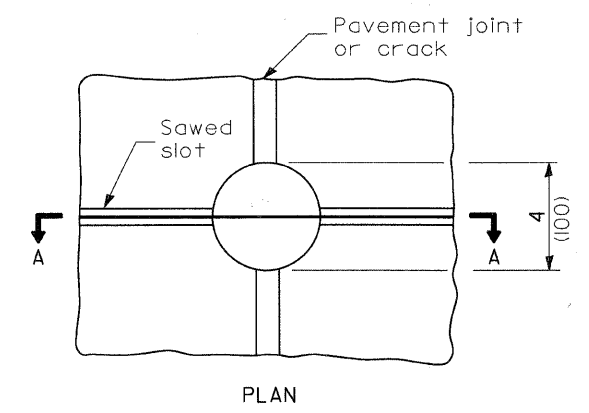
DESIGNED - RHL	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>JACKSON STREET LAPP PROJECT DUGDALE ROAD TO GOLF ROAD</b>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
DRAWN - PTO	REVISED -			2738	09-00235-00-RS	LAKE	16	15
CHECKED -	REVISED -			CONTRACT NO. 63342				
DATE - 08/21/09	REVISED -			ILLINOIS FED. AID PROJECT				



**DETECTOR LOOP LEAD-IN**

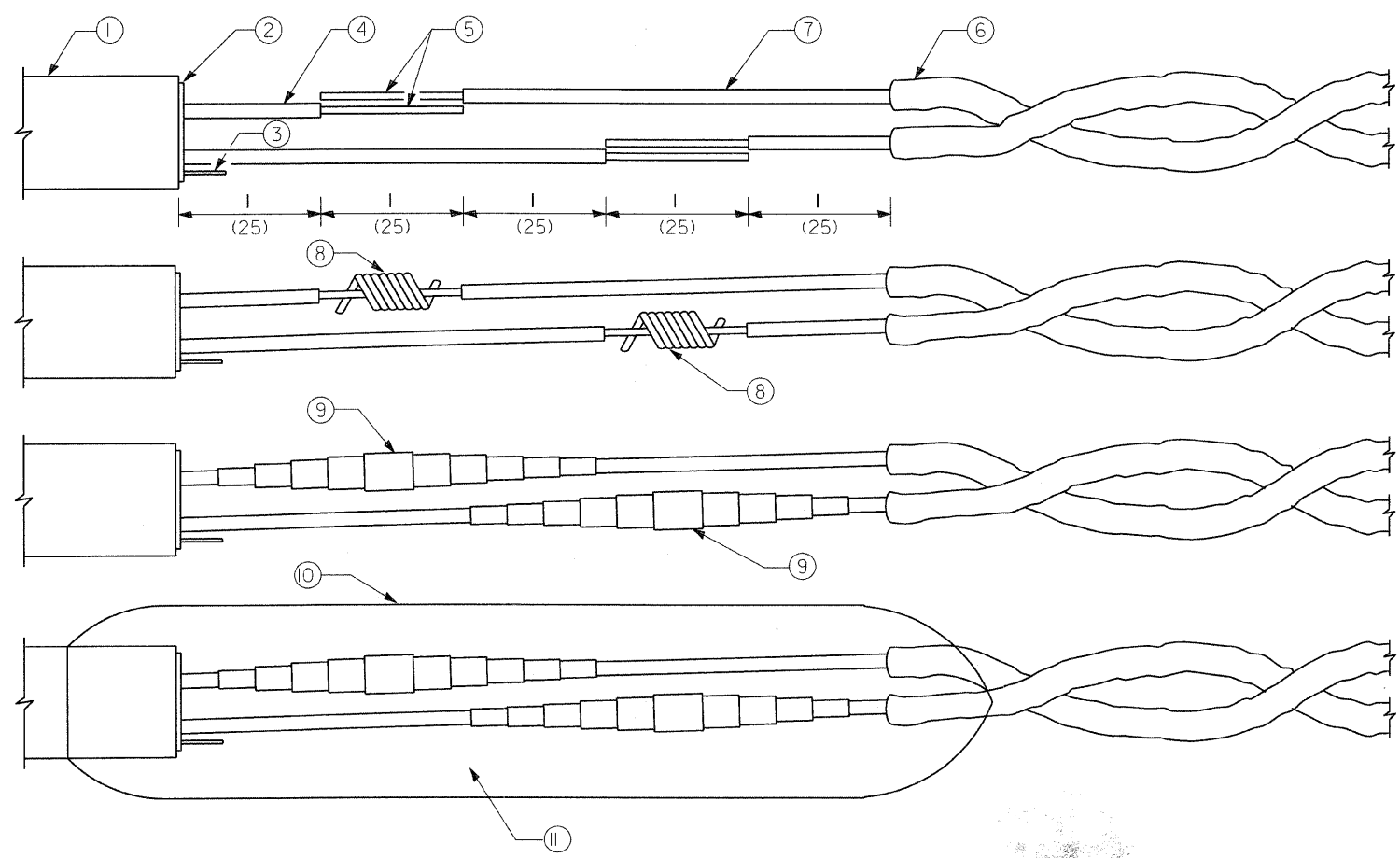


**DETECTOR LOOP INSTALLATION**



**NOTE**  
Loop wire shall follow saw cut to bottom, forming slack section at joint.

**DETECTOR LOOP AT PAVEMENT JOINT OR PAVEMENT CRACK**



**LOOP WIRE AND LEAD-IN CABLE SPLICE**

- ① = Lead-in cable (single pair or multipair)
- ② = Lead-in cable shield
- ③ = Lead-in cable shield drain-wire
- ④ = Lead-in cable insulated conductor
- ⑤ = Bare conductor
- ⑥ = Loop wire in tube
- ⑦ = Loop wire insulated conductor
- ⑧ = Twisted and resin soldered conductor
- ⑨ = Electrical tape insulated splice
- ⑩ = Rigid mold
- ⑪ = Waterproof and dielectric resin