

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2715	09-00011-00-RS	LAKE	10	1
CONTRACT NO. 63357				
ILLINOIS FED. AID PROJECT				

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS**

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

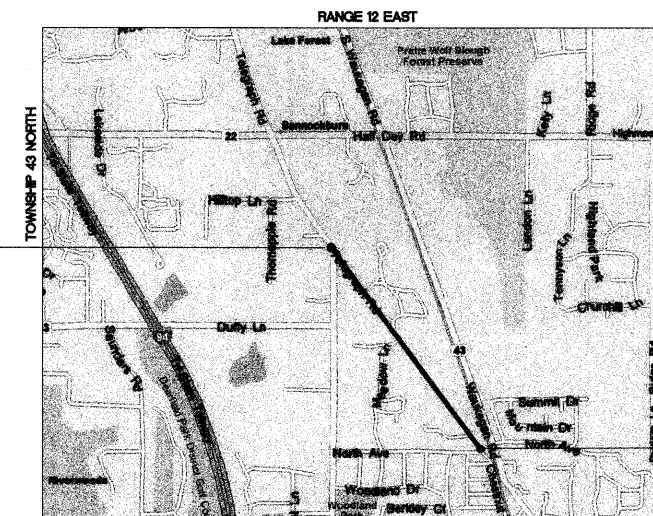
**FAU 2715/ TELEGRAPH ROAD
WILMOT ROAD TO NORTH AVENUE
SECTION: 09-00011-00-RS
JOB NO. C-91-887-09
PROJECT NO. ARA-9003(454)
VILLAGE OF BANNOCKBURN, ILLINOIS
LAKE COUNTY**

LOCATION MAP
(NOT TO SCALE)



LOCATION OF SECTION INDICATED THUS: [Black Rectangle]

- INDEX OF SHEETS**
1. COVER SHEET, INDEX OF SHEETS, LIST OF STATE STANDARDS
 2. GENERAL NOTES & SUMMARY OF QUANTITIES
 3. TYPICAL SECTIONS
 - 4.-5. PROPOSED CONDITIONS - PLAN VIEW
 - 6.-10. CONSTRUCTION DETAILS

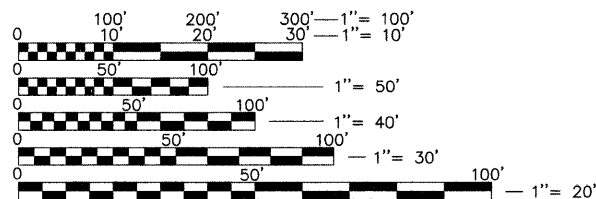


TELEGRAPH ROAD
PROJECT ENDS
STA. 53+65.5

TELEGRAPH ROAD
PROJECT BEGINS
STA. 0+29

PROJECT INFORMATION
LENGTH OF PROJECT = 5337 FT (1.01 MI)
ADT = 1250 VPD (2007)
POSTED SPEED LIMIT = 25 MPH
DESIGN SPEED LIMIT = 30 MPH
ROAD CLASSIFICATION = URBAN COLLECTOR

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE BELOW SCALES MAY BE USED.



J.U.L.I.E.

JOINT
UTILITY
LOCATION
INFORMATION FOR
EXCAVATION
CALL 811



Know what's below.
Call before you dig.

EXISTING UTILITIES: WHEN THE PLANS OR SPECIAL PROVISIONS INCLUDE INFORMATION PERTAINING TO THE LOCATION OF UNDERGROUND UTILITY FACILITIES, SUCH INFORMATION REPRESENTS ONLY THE OPINION OF THE ENGINEER AS TO THE LOCATION OF SUCH UTILITIES AND IS ONLY INCLUDED FOR THE CONVENIENCE OF THE BIDDER. THE ENGINEER AND OWNER ASSUME NO RESPONSIBILITY WHATEVER IN RESPECT TO THE SUFFICIENCY OR ACCURACY OF THE INFORMATION SHOWN ON THE PLANS RELATIVE TO THE LOCATION OF UNDERGROUND UTILITY FACILITIES OR THE MANNER IN WHICH THEY ARE TO BE REMOVED OR ADJUSTED. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES. HE SHALL ALSO OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES, DETAILED INFORMATION RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULES OF THE UTILITY COMPANIES FOR REMOVING OR ADJUSTING THEM.

CONTRACTOR IS RESPONSIBLE FOR CONTACTING J.U.L.I.E. AT 1-800-892-0123 AND MUST ACQUIRE A DIG NUMBER A MINIMUM OF 72 HOURS PRIOR TO ANY WORK BEING DONE.

NOTE:
THE CONTRACTOR IS SOLELY RESPONSIBLE
FOR JOBSITE SAFETY.

CONTRACT NO. 63357

FILE NAME =
8101.000-TELEGRAPH-PR1.dwg

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS	
APPROVED: <u>October 14</u> , 2009	<i>Mona Jansky</i> VILLAGE OF BANNOCKBURN
PASSED: <u>OCTOBER 20</u> , 2009	<i>Chris Heald</i> DISTRICT ONE ENGINEER OF LOCAL ROADS AND STREETS
RELEASING FOR BID BASED ON LIMITED REVIEW: <u>OCTOBER 29</u> , 2009	<i>Diana M. O'Leary</i> DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER

PROFESSIONAL ENGINEER'S SIGNATURE & SEAL

Leo X. Morand

LEO X. MORAND
EXPIRES: 11-30-09

**GHA GEWALT HAMILTON
ASSOCIATES, INC.**

850 Forest Edge Drive ■ Vernon Hills, IL 60061
Phone: 847-478-9700 Fax: 847-478-9701

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OF THE STATE OF ILLINOIS**

ASSOCIATE FIELD ENGINEER: KEVIN STALLWORTH (847) 705-4169

GENERAL NOTES

A-1 THE ILLINOIS DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" LATEST EDITION, THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" LATEST EDITION, PROJECT SPECIFICATIONS, ALL APPLICABLE REQUIREMENTS OF THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, MUNICIPALITY, ORDINANCES OF AUTHORITIES HAVING JURISDICTION AND ALL ADDENDA THERE SHALL GOVERN THIS WORK.

A-2 ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED TO MEAN THE RESIDENT ENGINEER.

A-3 NO CONSTRUCTION PLANS SHALL BE USED FOR CONSTRUCTION UNLESS SPECIFICALLY MARKED 'FOR CONSTRUCTION'. PRIOR TO COMMENCEMENT OF CONSTRUCTION, THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS, AND CONDITIONS AFFECTING THEIR WORK WITH THE ACTUAL CONDITIONS AT THE JOB SITE. IN ADDITION, THE CONTRACTOR MUST VERIFY THE ENGINEERS' LINE AND GRADES. IF THERE ARE ANY DISCREPANCIES FROM WHAT IS SHOWN ON THE CONSTRUCTION PLANS, STANDARD SPECIFICATIONS AND/OR SPECIAL DETAILS, THE CONTRACTOR SHALL SECURE WRITTEN INSTRUCTION FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF THE WORK AFFECTED BY OMISSION OR DISCREPANCIES. FAILING TO SECURE SUCH INSTRUCTION, THE CONTRACTOR WILL BE CONSIDERED TO HAVE PROCEEDED AT HIS OWN RISK AND EXPENSE. IN THE EVENT OF ANY DOUBT OR QUESTION ARISING WITH RESPECT TO THE TRUE MEANING OF THE CONSTRUCTION PLANS OR SPECIFICATIONS, THE DECISION OF THE ENGINEER SHALL BE FINAL AND CONCLUSIVE.

A-4 IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS PRIOR TO BIDDING ON THE PROJECT.

A-5 EASEMENTS FOR THE EXISTING UTILITIES, BOTH PUBLIC AND PRIVATE, AND UTILITIES WITHIN PUBLIC RIGHTS-OF-WAY ARE SHOWN ON THE PLANS ACCORDING TO AVAILABLE RECORDS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE EXACT LOCATION IN THE FIELD OF THESE UTILITY LINES AND THEIR PROTECTION FROM DAMAGE DUE TO CONSTRUCTION OPERATIONS. IF EXISTING UTILITY LINES OF ANY NATURE ARE ENCOUNTERED WHICH CONFLICT IN LOCATION WITH NEW CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE ENGINEER SO THAT THE CONFLICT MAY BE RESOLVED.

A-6 SAW CUTTING OF PAVEMENTS, SIDEWALK, ETC. SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEM REMOVED.

A-7 THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARKERS UNTIL THE OWNER, HIS AGENT, OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

A-8 OFFSET LOCATIONS GIVEN IN THE PLANS FOR STRUCTURES, EDGE OF PAVEMENT, ETC. ARE FROM THE ROADWAY CENTERLINE.

A-9 HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT). IN ACCORDANCE WITH THE BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

A-10 QUANTITIES FOR PATCHING SHALL NOT EXCEED THOSE PROVIDED IN THE SUMMARY OF QUANTITIES UNLESS APPROVED BY THE ENGINEER. THE ENGINEER WILL IDENTIFY FINAL PATCH LOCATIONS IN THE FIELD.

A-11 WHENEVER, DURING CONSTRUCTION OPERATIONS, ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF GUTTERS, DRAINAGE STRUCTURES, DITCHES, ETC. SUCH THAT THE NATURAL FLOW LINE OF WATER IS OBSTRUCTED, THE LOOSE MATERIAL WILL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATION, ALL DRAINAGE STRUCTURES AND FLOW LINES SHALL BE FREE FROM DIRT AND DEBRIS. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR'S FAILURE TO PROVIDE THE ABOVE WILL PRECLUDE ANY POSSIBLE ADDED COMPENSATION REQUESTED DUE TO DELAYS OR UNSUITABLE MATERIALS CREATED AS A RESULT THEREOF.

STORM SEWERS, WATER MAINS, AND UTILITIES

B-1 EXISTING UTILITIES: WHEN THE PLANS OR SPECIAL PROVISIONS INCLUDE INFORMATION PERTAINING TO THE LOCATION OF UNDERGROUND UTILITY FACILITIES, SUCH INFORMATION REPRESENTS ONLY THE OPINION OF THE ENGINEER AS TO THE LOCATION OF SUCH UTILITIES AND IS ONLY INCLUDED FOR THE CONVENIENCE OF THE CONTRACTOR. THE ENGINEER AND OWNER ASSUME NO RESPONSIBILITY WHATSOEVER IN RESPECT TO THE SUFFICIENCY OR ACCURACY OF THE INFORMATION SHOWN ON THE PLANS RELATIVE TO THE LOCATION OF UNDERGROUND UTILITY FACILITIES OR THE MANNER IN WHICH THEY ARE TO BE REMOVED OR ADJUSTED. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES. THIS WORK SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PROCESS OF THE CONTRACT, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. HE SHALL ALSO OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULES OF THE UTILITY COMPANIES FOR REMOVING OR ADJUSTING THEM.

B-2 THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING JULIE AS WELL AS LOCAL CABLE TV COMPANIES AND THE VILLAGE. THE FACILITIES SHALL BE LOCATED PRIOR TO ANY WORK WITHIN ANY EASEMENT, R.O.W, OR SUSPECTED UTILITY LOCATION.

B-3 THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THROUGH THEY MIGHT NOT BE SHOWN ON THE PLANS. ANY UTILITY PROPERTY DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE OWNER AT THE CONTRACTOR'S EXPENSE.

B-4 ALL UTILITY COMPANIES SHALL BE NOTIFIED AT LEAST 3 DAYS PRIOR TO THE START OF CONSTRUCTION.

B-5 THE CONTRACTOR SHALL ENSURE THAT ALL WATER SYSTEM VALVE VAULTS, AND SANITARY SEWER MANHOLES REMAIN READILY ACCESSIBLE TO THE VILLAGE FOR EMERGENCY OPERATIONS. THE LOCATIONS OF ALL WATER AND SANITARY FACILITIES SHALL BE MARKED AND READILY VISABLE AT ALL TIMES.

SIGNING AND STRIPING

C-1 SEE IDOT STANDARD DETAIL 780001, DISTRICT ONE DETAIL TC-13, AND REFERENCE PLAN SHEETS.

TRAFFIC CONTROL

D-1 SEE TRAFFIC CONTROL HIGHWAY STANDARDS CONCERNING TRAFFIC CONTROL AND PROTECTION.

DEBRIS REMOVAL

MATERIALS RESULTING FROM THE REMOVAL OF ASPHALT SURFACES, UTILITY ADJUSTMENTS, RESTORATION WORK, ETC. SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IN THE JUDGEMENT OF THE CITY, SHOULD IT BE NECESSARY TO REMOVE SUCH MATERIALS, THE CITY WILL HAVE THE MATERIAL REMOVED AND THE CONTRACTOR SHALL BE BILLED (CHARGED) ACCORDINGLY.

WATER SUPPLY

THE INDISCRIMINATE USE OF FIRE HYDRANTS, EXISTING STREAMS, CREEKS, WETLANDS OR PONDS IS STRICTLY PROHIBITED. THE CONTRACTOR SHALL PROVIDE A WATER TRUCK AND DRIVER AS REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING WATER FROM AN APPROVED SOURCE OTHER THAN HIS YARD, WRITTEN APPROVAL FROM THE AGENCY HAVING JURISDICTION FOR THE SOURCE. IF THE WATER IS FROM A SOURCE OTHER THAN HIS YARD, WRITTEN APPROVAL FROM THE AGENCY HAVING JURISDICTION FOR THE SOURCE OF THE WATER MUST BE RECEIVED BY THE CONTRACTOR PRIOR TO USE OF THE WATER.

DRIVEWAY CLOSING

IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS AND THE VILLAGE WHEN ACCESS TO THEIR DRIVEWAYS WILL BE TEMPORARILY CLOSED DUE TO SIDEWALK REPLACEMENT, AND/OR DRIVEWAY REPLACEMENT. AT LOCATIONS WHERE THE DRIVEWAY IS SCHEDULED TO BE REMOVED, THE CONTRACTOR SHALL CONTACT THE BUSINESS/HOMEOWNER 24 HOURS PRIOR TO REMOVING THE CURB, SIDEWALK, OR DRIVE APPROACH. EVERY EFFORT SHALL BE MADE TO ACCOMMODATE ACCESS TO THESE PROPERTIES. THE CONTRACTOR SHALL NOT BE ALLOWED TO CLOSE A DRIVEWAY FOR MORE THAN 8 HOURS UNDER ANY CIRCUMSTANCE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE BARRICADES TO PREVENT TRAFFIC FROM USING THE DRIVEWAYS DURING THIS PERIOD.

STREET SWEEPING AND PREPARATION

THE CONTRACTOR SHALL BE RESPONSIBLE FOR SWEEPING AND CLEANING STREETS OF ANY DEBRIS AND MATERIAL THAT HAS ACCUMULATED AS A RESULT OF THE CONSTRUCTION ACTIVITY. A MECHANICAL SWEEPER, MECHANICALLY DRIVEN AIR AND HANDWORK WITH SHOVEL AND BROOM SHALL BE UTILIZED TO PROVIDE A CLEAN STREET FOR THE MOTORING PUBLIC. WITHIN 24 HOURS OF PLACING PRIME COAT AND THE LAYING OF HMA, THE CONTRACTOR SHALL SWEEP THE PAVEMENT AND REMOVE STANDING WATER, EARTH, WEEDS, LEAVES, DIRT, CONSTRUCTION DEBRIS AND ALL LOOSE MATERIAL.

HIGHWAY STANDARDS:

000001-05 STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
 442201-03 CLASS C AND D PATCHES
 701301-03 LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
 701311-03 LANE CLOSURE, 2L, 2W, MOVING OPERATIONS-DAY ONLY
 701501-05 URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
 701901-01 TRAFFIC CONTROL DEVICES
 780001-02 TYPICAL PAVEMENT MARKINGS

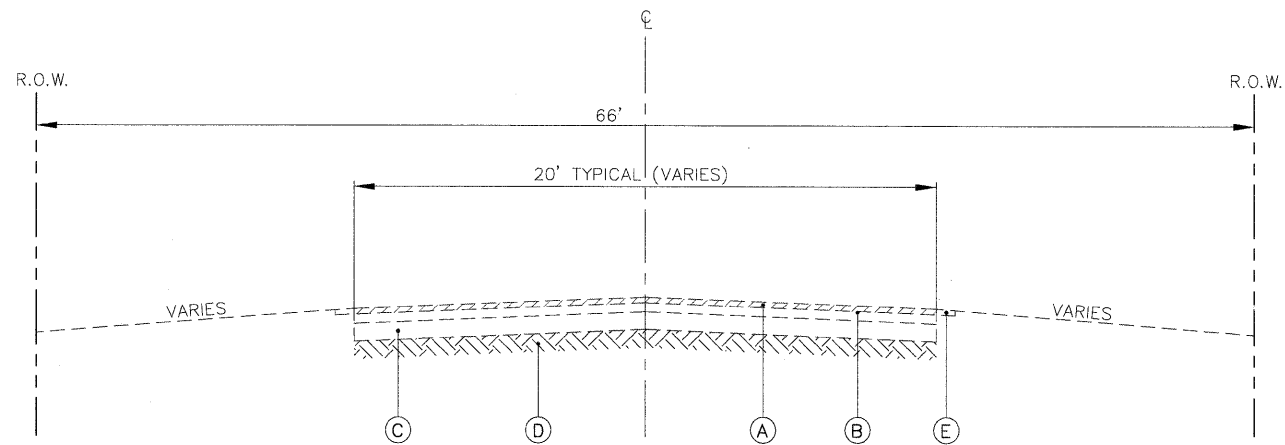
SUMMARY OF QUANTITIES

				ROADWAY	
				100% FEDERAL	
ITEM	PAYCODE	DESCRIPTION	UNIT	1000	
1.	20101700	SUPPLEMENTAL WATERING	UNIT	135	
2.	21101600	TOPSOIL FURNISH AND PLACE, VARIABLE DEPTH	SQ YD	1190	
3.	25000110	SEEDING - CLASS 1A	ACRE	0.25	
4.	25000400	NITROGEN FERTILIZER NUTRIENT	POUND	25	
5.	25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	25	
6.	25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	25	
7.	25100630	EROSION CONTROL BLANKET	SQ YD	1200	
8.	40300100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	1205	
9.	40600300	AGGREGATE (PRIME COAT)	TON	18	
10.	40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	900	
11.	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	700	
12.	40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	1650	
13.	44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2 "	SQ YD	13000	
14.	44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	973	
15.	44201713	CLASS D PATCHES, TYPE I, 6 INCH	SQ YD	143	
16.	44201717	CLASS D PATCHES, TYPE II, 6 INCH	SQ YD	257	
17.	44201721	CLASS D PATCHES, TYPE III, 6 INCH	SQ YD	150	
18.	44201723	CLASS D PATCHES, TYPE IV, 6 INCH	SQ YD	250	
19.	44300100	AREA REFLECTIVE CRACK CONTROL TREATMENT	SQ YD	13000	
20.	48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	250	
21.	60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	6	
22.	67100100	MOBILIZATION	L SUM	1	
23.	70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	
24.	70300100	SHORT-TERM PAVEMENT MARKING	FOOT	2190	
25.	70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	244	
* 26.	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	42	
* 27.	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	10700	
* 28.	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	85	
* 29.	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	14	
* 30.	78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	14	
31.	X0322256	TEMPORARY INFORMATION SIGNAGE	SQ FT	52	
32.	XX004774	BRICK DRIVEWAY REMOVAL AND REPLACEMENT	SQ FT	1282	

* DENOTES SPECIALITY ITEM

FILE NAME = 8101.000-TELEGRAPH-PR1.dwg	USER NAME =	DESIGNED - BVS	REVISED - 10/16/09	TELEGRAPH ROAD RESURFACING PROGRAM	GENERAL NOTES AND SUMMARY OF QUANTITIES	F.A.U. RTE. 2715	SECTION 09-000H-00-RS	COUNTY LAKE	TOTAL SHEETS 10	SHEET NO. 2
	PLOT SCALE = N.T.S.	DRAWN - BVS	REVISED - 10/22/09			CONTRACT NO. 63357				
	PLOT DATE = 8/3/09	CHECKED - DJG	DATE - 8/3/09			ILLINOIS FED. AID PROJECT				

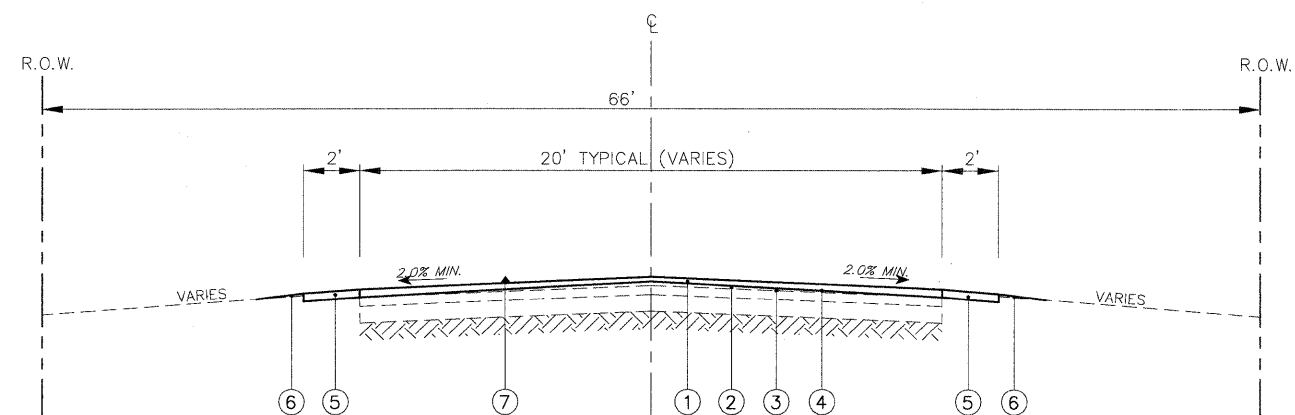
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EXISTING TYPICAL CROSS SECTION
 TELEGRAPH ROAD (FAU ROUTE 2715)
 STA. 0+29 (NORTH AVENUE) TO STA. 53+65.5 (WILMOT ROAD)

EXISTING LEGEND

- (A) HMA SURFACE REMOVAL, 2 1/2"
- (B) EXISTING HMA PAVEMENT, VARIES
- (C) EXISTING AGGREGATE BASE, VARIES
- (D) EXISTING SUBBASE
- (E) EXISTING AGGREGATE SHOULDER



PROPOSED TYPICAL CROSS SECTION
 TELEGRAPH ROAD (FAU ROUTE 2715)
 STA. 0+29 (NORTH AVENUE) TO STA. 53+65.5 (WILMOT ROAD)

PROPOSED LEGEND

- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX 'C', N50, 2"
- (2) LEVELING BINDER (MACHINE METHOD), N50, 1 1/4"
- (3) BITUMINOUS MATERIALS (PRIME COAT) AND AGGREGATE (PRIME COAT)
- (4) AREA REFLECTIVE CRACK CONTROL TREATMENT
- (5) AGGREGATE SHOULDER, 2", TO BE REPAIRED AS NEEDED
- (6) RESTORATION
- (7) BLUE RAISED REFLECTIVE PAVEMENT MARKER

NOTES:

- HOT-MIX ASPHALT SURFACE REMOVAL, BUTT JOINTS AND ROADWAY BUTT JOINTS SHALL CONSIST OF COLD MILLING BUTT JOINTS AS MARKED BY THE ENGINEER.
- DRIVEWAY HMA PAVEMENT TO BE REMOVED AND REPLACED AS MARKED IN THE FIELD BY THE ENGINEER.
- THE INTENTION OF DRIVEWAY REMOVAL AND REPLACEMENT IS TO TRANSITION FROM PROPOSED PAVEMENT ELEVATION TO EXISTING DRIVEWAY PAVEMENT ELEVATION.
- LIMITS OF REMOVAL SHALL NOT EXTEND PAST THE PUBLIC RIGHT-OF-WAY.
- COLD MILL EXISTING HMA SURFACE COURSE AND/OR BINDER COURSE TO A TOTAL DEPTH OF 2" AS MARKED BY THE ENGINEER.
- CLASS D PATCHES, INCLUDES REMOVAL OF SURFACE COURSE AND BINDER OR SUBBASE TO A TOTAL DEPTH OF 6" AS MARKED BY THE ENGINEER AND REPLACED WITH HMA AND PROPERLY PREPARED TO ACCEPT THE FINAL LIFT OF HMA SURFACE COURSE.

NOTE: CONTRACTOR SHALL MILL BEFORE PATCHING

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ N _{DES}
PAVEMENT RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 (IL 9.5 mm)	4% @ 50 Gyr.
LEVELING BINDER (MACHINE METHOD), IL 9.5 mm, N50	4% @ 50 Gyr.
DRIVEWAYS	
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 (IL 9.5 mm); 3"	4% @ 50 Gyr.
PATCHING	
CLASS D PATCHES (HMA BINDER IL-19 mm)	4% @ 70 Gyr.
<small>Note: The unit weight used to calculate all HMA Mixture Quantities is 112Lbs/SqYd/in. Note: The "AC TYPE" for polymerized HMA Mixes shall be "SBS/SBR PG 70-22" and for non-polymerized HMA the "AC TYPE" shall be "PG 64-22" unless modified by District One Special Provisions. For "PERCENT OF RAP" see District One Special Provisions.</small>	

FILE NAME =
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USER NAME =
DESIGNED - BVS
DRAWN - BVS
PLOT SCALE = N.T.S.
PLOT DATE = 8/3/09

REVISED - 10/16/09
REVISED - 10/22/09
CHECKED - DJG
DATE - 8/3/09

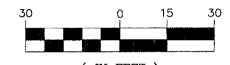
**TELEGRAPH ROAD
RESURFACING PROGRAM**

TYPICAL SECTIONS

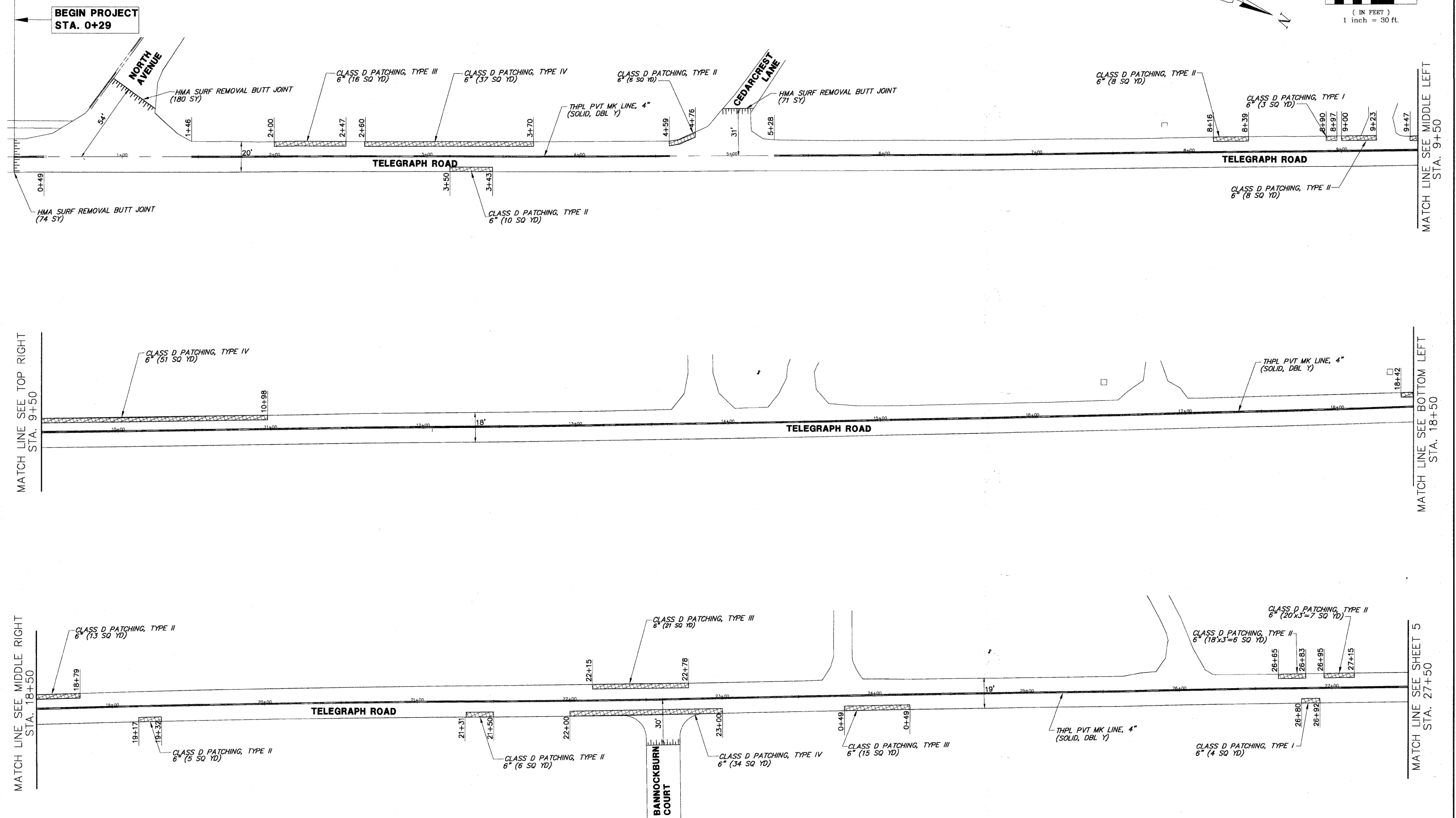
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2715	09-00011-00-RS	LAKE	10	3
				CONTRACT NO. 63357
<small>ILLINOIS FED. AID PROJECT</small>				

GRAPHIC SCALE

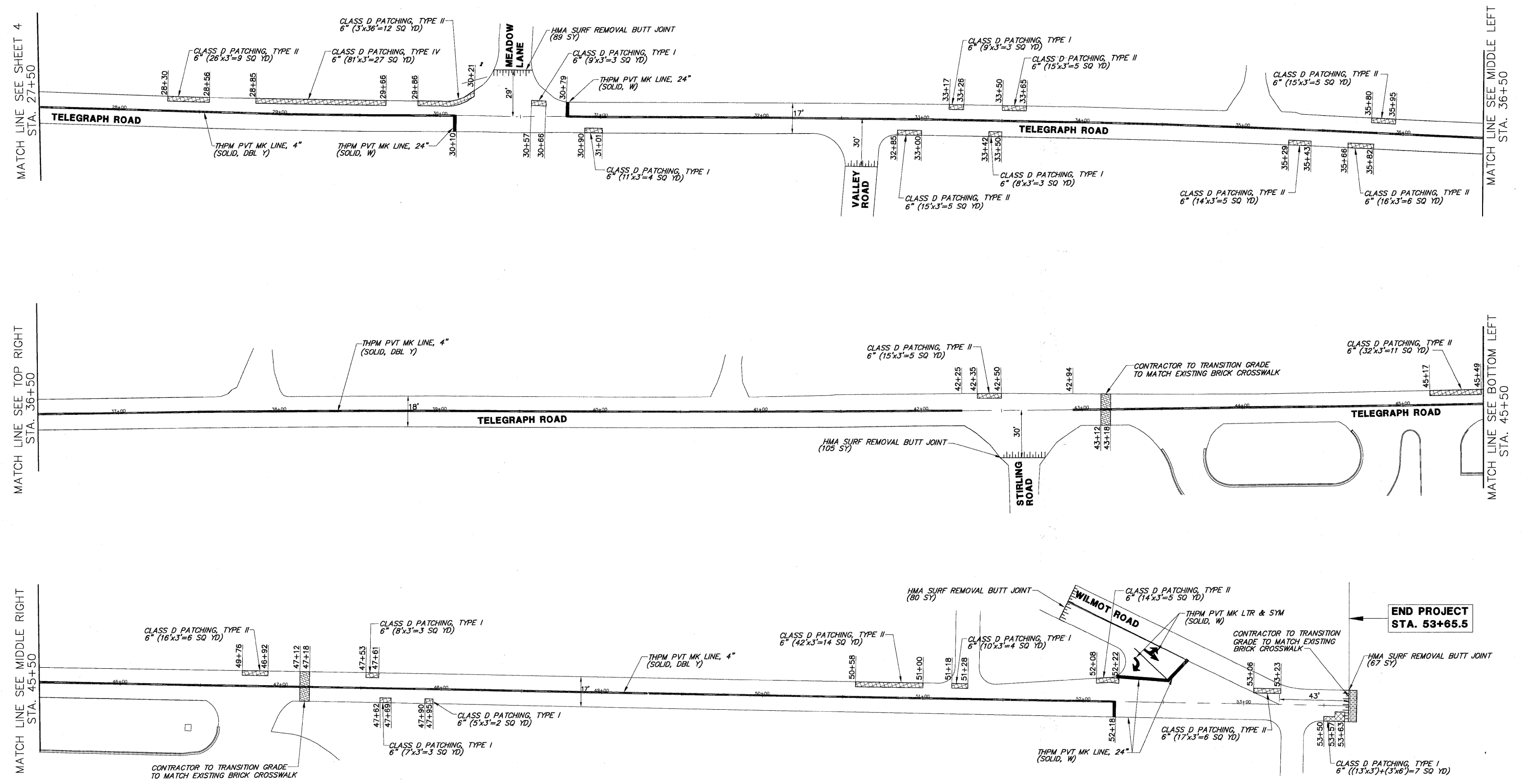
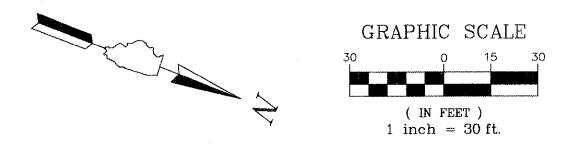


(IN FEET)
1 inch = 30 ft.



*NOTE: FINAL PATCHING LOCATIONS AND DIMENSIONS TO BE DETERMINED BY THE ENGINEER IN THE FIELD.

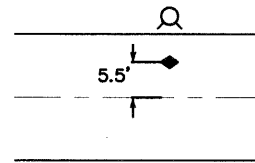
FILE NAME = 8101.000-TELEGRAPH-PR1.dwg	USER NAME =	DESIGNED - BVS	REVISED - 10/16/09	TELEGRAPH ROAD RESURFACING PROGRAM	PROPOSED PLAN	F.A.U. RTE. 2715	SECTION 09-000II-00-RS	COUNTY LAKE	TOTAL SHEETS 10	SHEET NO. 4		
PLOT SCALE = N.T.S.	CHECKED - DJG	SCALE: 1"=30'				SHEET NO. 4 OF 10 SHEETS		STA. 0+29 TO STA. 27+50		CONTRACT NO. 63357		
PLOT DATE = 8/3/09	DATE - 8/3/09									ILLINOIS FED. AID PROJECT		



*NOTE: FINAL PATCHING LOCATIONS AND DIMENSIONS TO BE DETERMINED BY THE ENGINEER IN THE FIELD.

FILE NAME = 8101.000-TELEGRAPH-PR1.dwg	USER NAME =	DESIGNED - BVS	REVISED - 10/16/09	TELEGRAPH ROAD RESURFACING PROGRAM		PROPOSED PLAN		F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = N.T.S.	DRAWN - BVS						2715	09-00011-00-RS	LAKE	10	5
	PLOT DATE = 8/3/09	CHECKED - DJG						CONTRACT NO. 63357				
				SCALE: 1"=30'		SHEET NO. 5 OF 10 SHEETS		STA. 27+50 TO STA. 53+65.5		ILLINOIS FED. AID PROJECT		

TYPICAL APPLICATIONS
RAISED REFLECTIVE MARKERS (SNOW-PLOW RESISTANT)

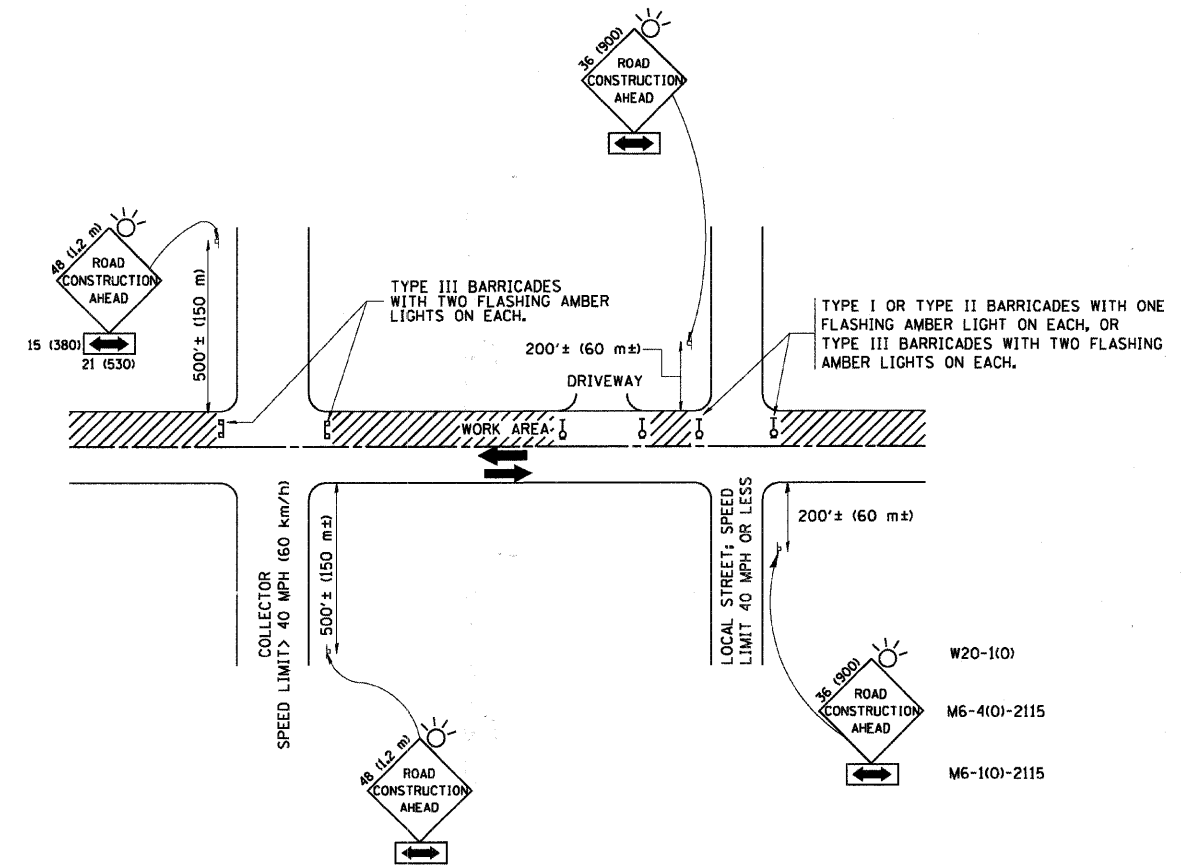


TWO-LANE/TWO-WAY
FIRE HYDRANT LOCATION

SYMBOLS

- CENTER LINE
- ◆ TWO-WAY BLUE MARKER
- ⊙ FIRE HYDRANT

RAISED REFLECTIVE PAVEMENT MARKER DETAIL



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME = W:\d\state\22x34\to18.dgn	USER NAME = geylienob1	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN - -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000 / IN.	CHECKED - -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TC-10		CONTRACT NO.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

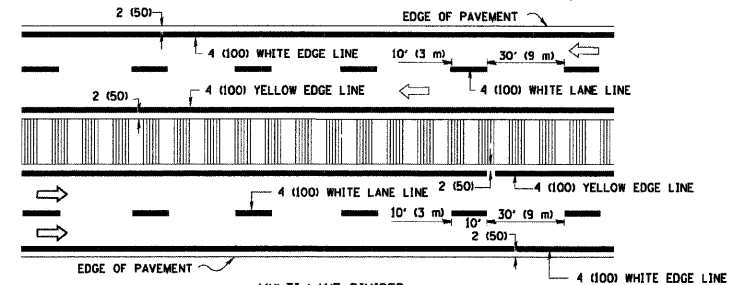
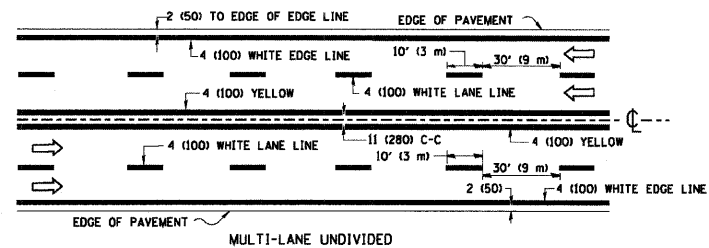
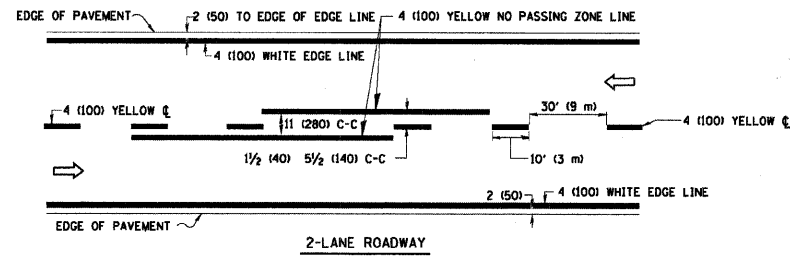
FILE NAME = 8101.000-TELEGRAPH-PR1.dwg	USER NAME =	DESIGNED - BVS	REVISED - 10/16/09
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	PLOT DATE = 8/3/09	DATE - 8/3/09	

TELEGRAPH ROAD
RESURFACING PROGRAM

DETAILS

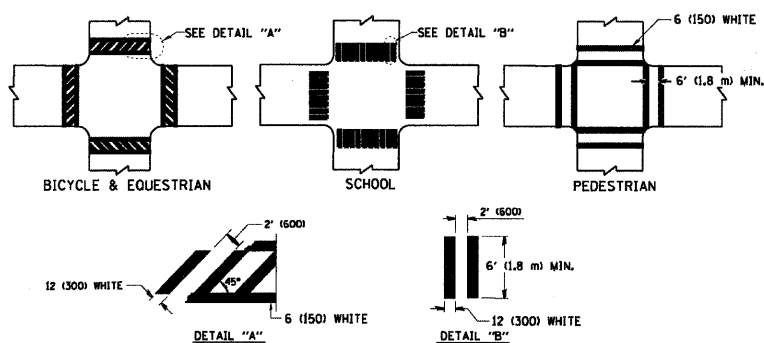
F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2715	09-00011-00-RS	LAKE	10	6
CONTRACT NO. 63357				
ILLINOIS FED. AID PROJECT				

SCALE: NONE SHEET NO. 6 OF 10 SHEETS STA. TO STA.

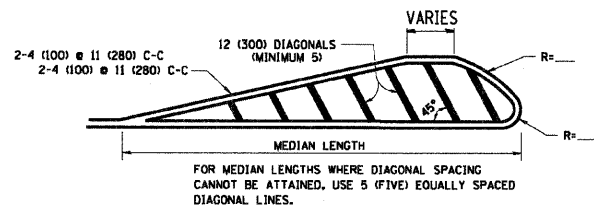
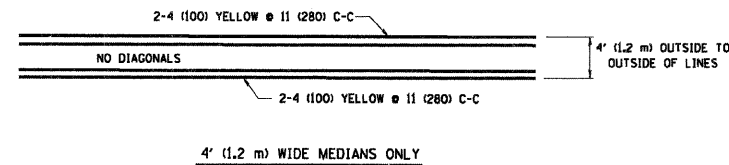


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

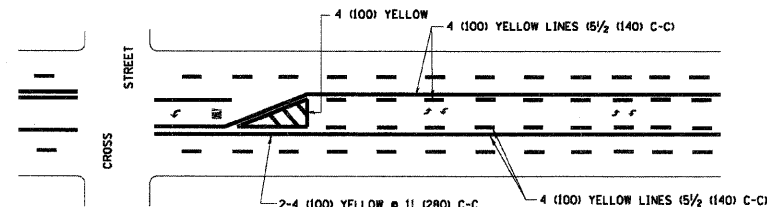


TYPICAL CROSSWALK MARKING

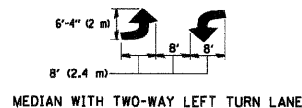


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

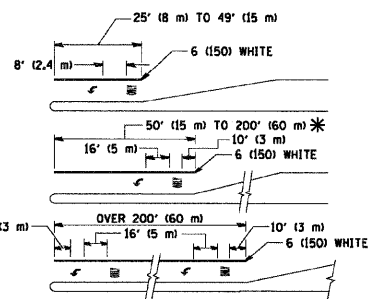
MEDIANS OVER 4' (1.2 m) WIDE



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



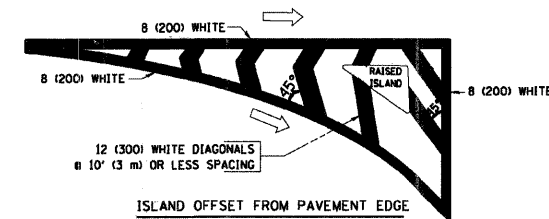
TYPICAL PAINTED MEDIAN MARKING



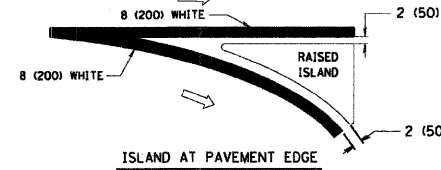
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m²) AREA = 20.8 SQ. FT. (1.9 m²) * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES: FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100)	SKIP-DASH	WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	5 (125)	SKIP-DASH	WHITE	SAME AS LINE BEING EXTENDED
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW. EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
ORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD T80001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD T80001.

All dimensions are in inches (millimeters) unless otherwise shown.

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	PLOT SCALE = 58.080" / IN.	DRAWN - BVS	REVISED - A. HOUSEH 10-09-96
	PLOT DATE = 1/4/2008	CHECKED - DJG	REVISED - A. HOUSEH 10-17-96
		DATE - 03-19-90	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-13			
SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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8101.000-TELEGRAPH-PR1.dwg

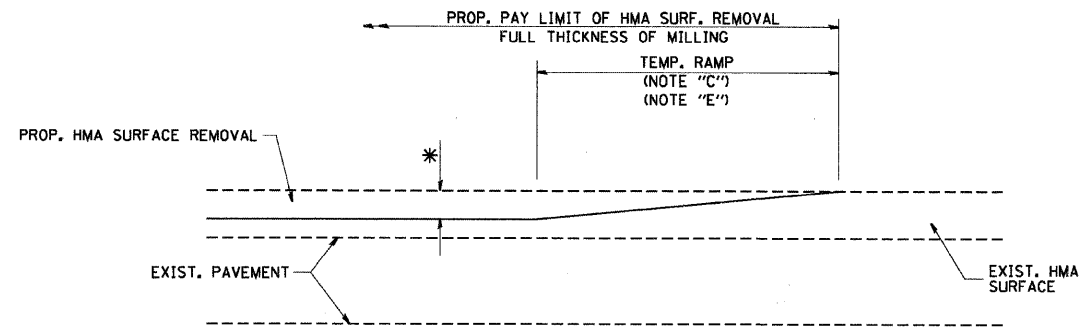
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PLOT SCALE = N.T.S.	DRAWN - BVS	
PLOT DATE = 8/3/09	CHECKED - DJG	
	DATE - 8/3/09	

TELEGRAPH ROAD
RESURFACING PROGRAM

DETAILS

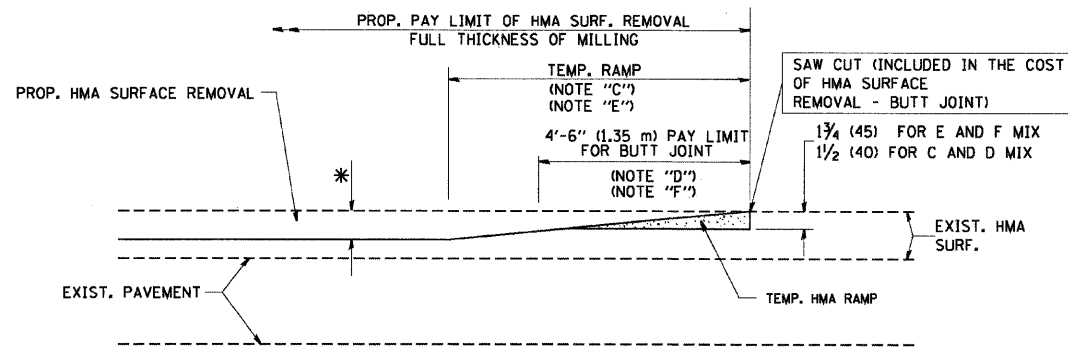
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2715	09-0001-00-RS	LAKE	10	7
CONTRACT NO. 63357				
ILLINOIS FED. AID PROJECT				



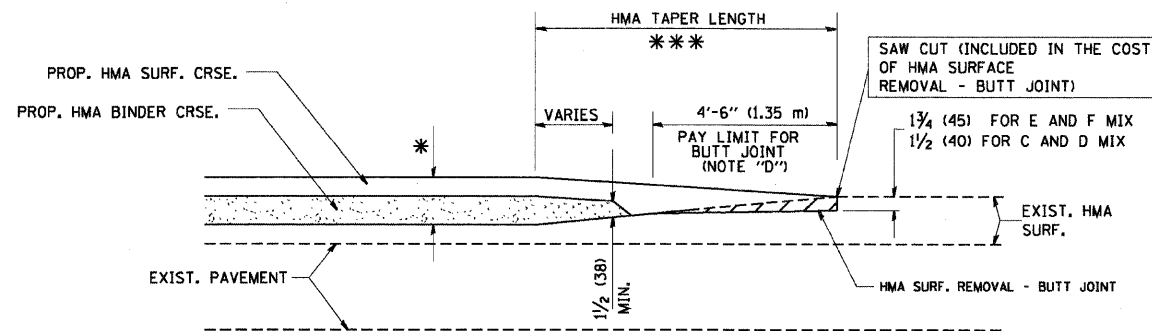
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

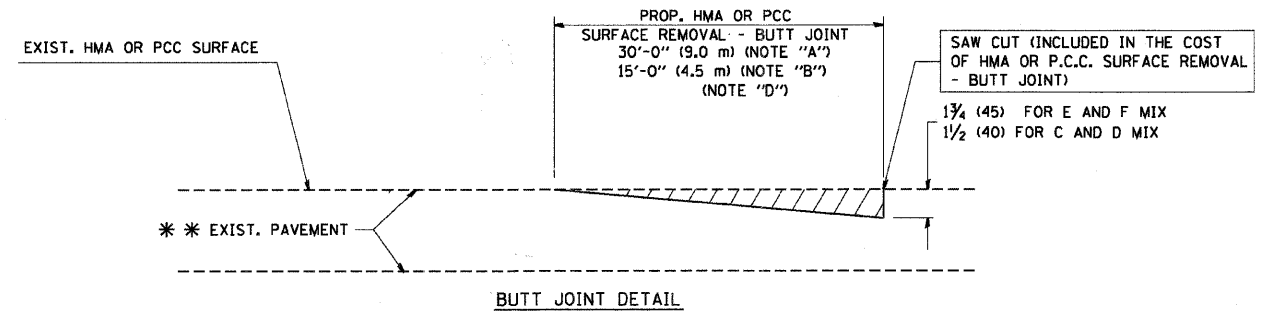


HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

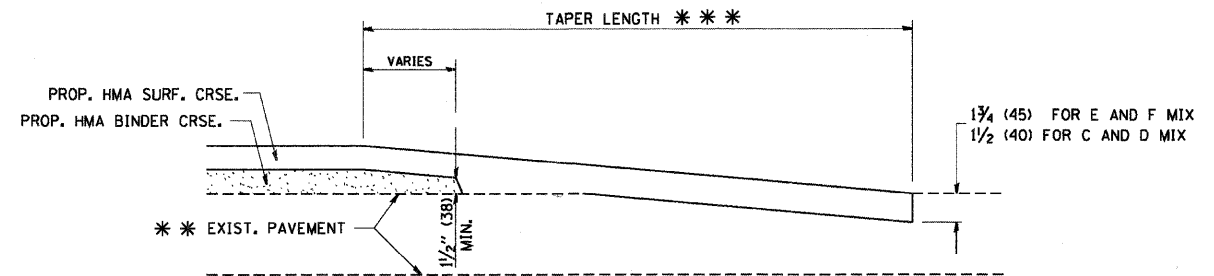
OPTION 2
TYPICAL TEMPORARY RAMP



BUTT JOINT AND HMA TAPER
TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\ndistata\22x34\bd32.dgn	USER NAME = geglianob1	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 5/8"=1'-0"	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	BD400-05 BD32			
CONTRACT NO.				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

FILE NAME = 8101.000-TELEGRAPH-PR1.dwg	USER NAME =	DESIGNED - BVS	REVISED - 10/16/09
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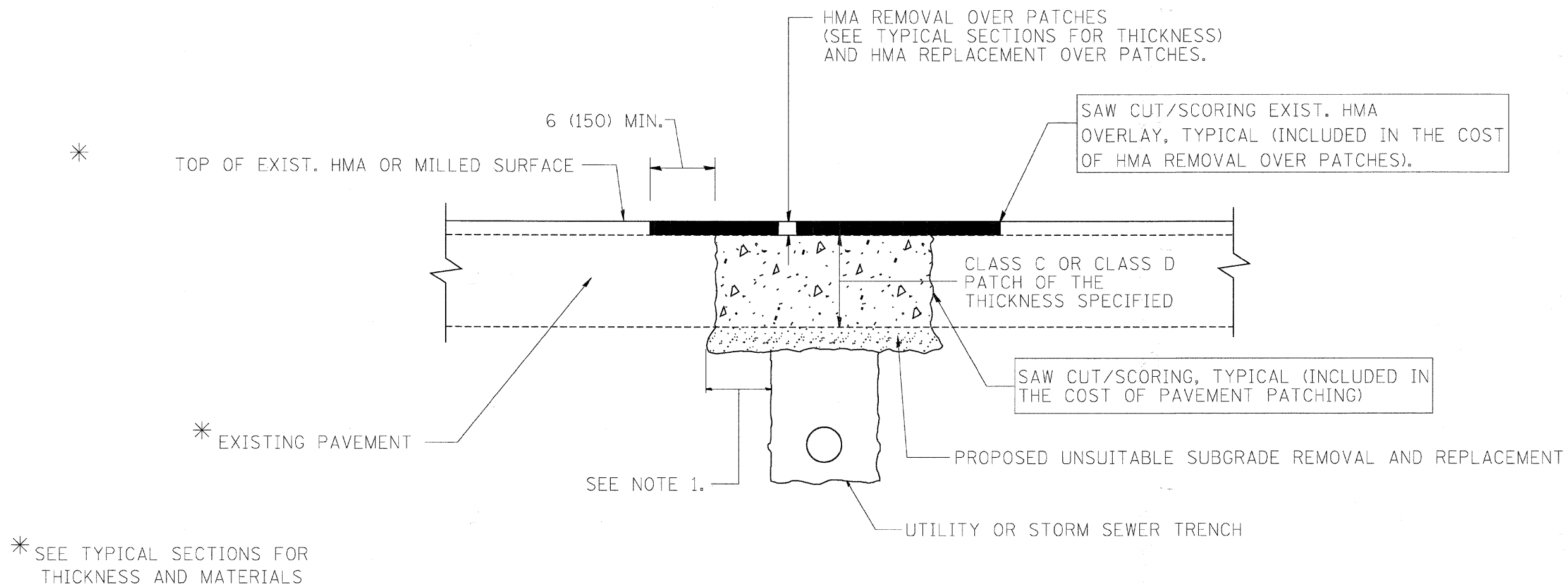
TELEGRAPH ROAD
RESURFACING PROGRAM

DETAILS

SCALE: NONE SHEET NO. 8 OF 10 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2715	09-000II-00-RS	LAKE	10	8
CONTRACT NO.			63357	
ILLINOIS FED. AID PROJECT				

CONTRACT NO.			
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS
			SHEET NO.
STA.		TO STA.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE FULL DEPTH PATCHES
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

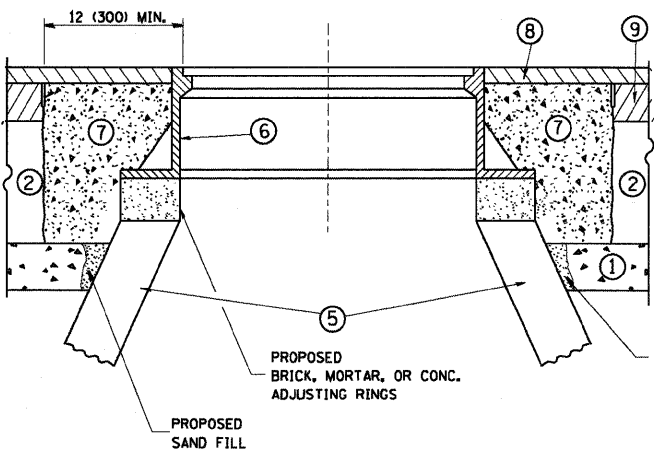
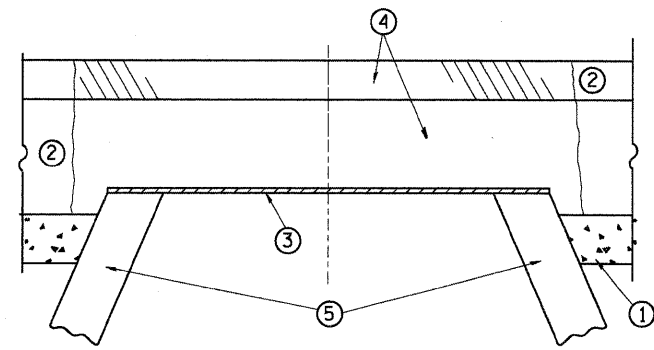
REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION	
NAME	DATE		
R. SHAH	10/25/94		
R. SHAH	01/14/95		
R. SHAH	03/23/95		
R. SHAH	04/24/95		
A. HOUSEH	03/15/96		
A. ABBAS	03/21/97		
A. ABBAS	01/20/98		
ART ABBAS	04/27/98		
R. BORO	01/01/07		

PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT

SCALE: VERT. NONE
 HORIZ. 1" = 12'
 PLOT DATE: 10/31/2006
 DRAWN BY: BD400-04 (BD-22)
 CHECKED BY:
 REVISION DATE: 01/01/07

PLOT DATE = 10/31/2006
 FILE NAME = K:\view\0402.dwg
 USER NAME = jaygo

FILE NAME = 8101.000-TELEGRAPH-PR1.dwg	USER NAME =	DESIGNED - BVS	REVISED - 10/16/09	TELEGRAPH ROAD RESURFACING PROGRAM	DETAILS	F.A.U. RTE. 2715	SECTION 09-0001-00-RS	COUNTY LAKE	TOTAL SHEETS 10	SHEET NO. 9		
PLOT SCALE = N.T.S.	DRAWN - BVS	CHECKED - DJG				SCALE: NONE	SHEET NO. 9 OF 10 SHEETS	STA.	TO STA.	CONTRACT NO. 63357		
PLOT DATE = 8/3/09	DATE - 8/3/09					ILLINOIS FED. AID PROJECT						



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"
NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

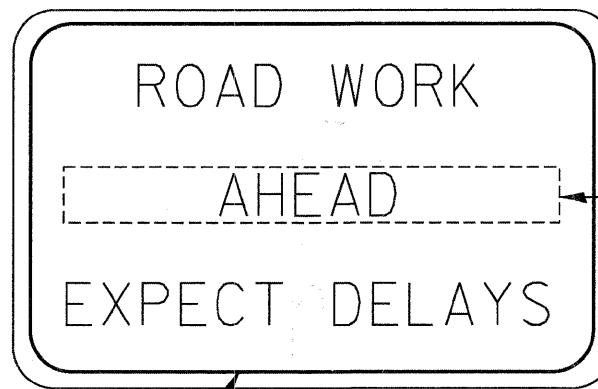
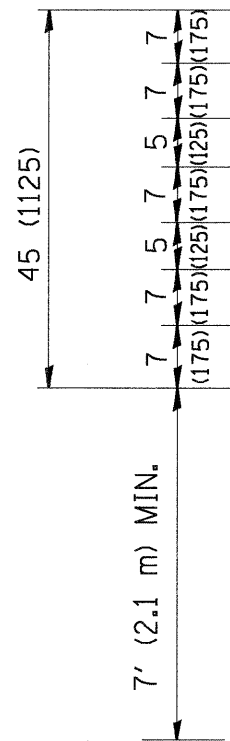
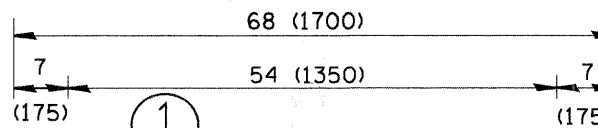
CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

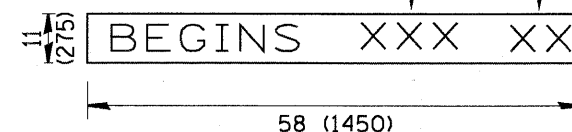
WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN



USE APPROPRIATE MONTH AND DATE FOR CONTRACT



NOTES:

- 1. USE BLACK LETTERING ON ORANGE BACKGROUND.
- 2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
- 3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
- 4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
- 5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
- 6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
- 7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = 8101.000-TELEGRAPH-PR1.dwg	USER NAME =	DESIGNED - BVS	REVISED - 10/16/09	TELEGRAPH ROAD RESURFACING PROGRAM	DETAILS	F.A.U. RTE. 2715	SECTION 09-000II-00-RS	COUNTY LAKE	TOTAL SHEETS 10	SHEET NO. 10		
PLOT SCALE = N.T.S.	DRAWN - BVS	CHECKED - DJG				SCALE: NONE	SHEET NO. 10 OF 10 SHEETS	STA.	TO STA.	CONTRACT NO. 63357		
PLOT DATE = 8/3/09	DATE - 8/3/09					ILLINOIS FED. AID PROJECT						