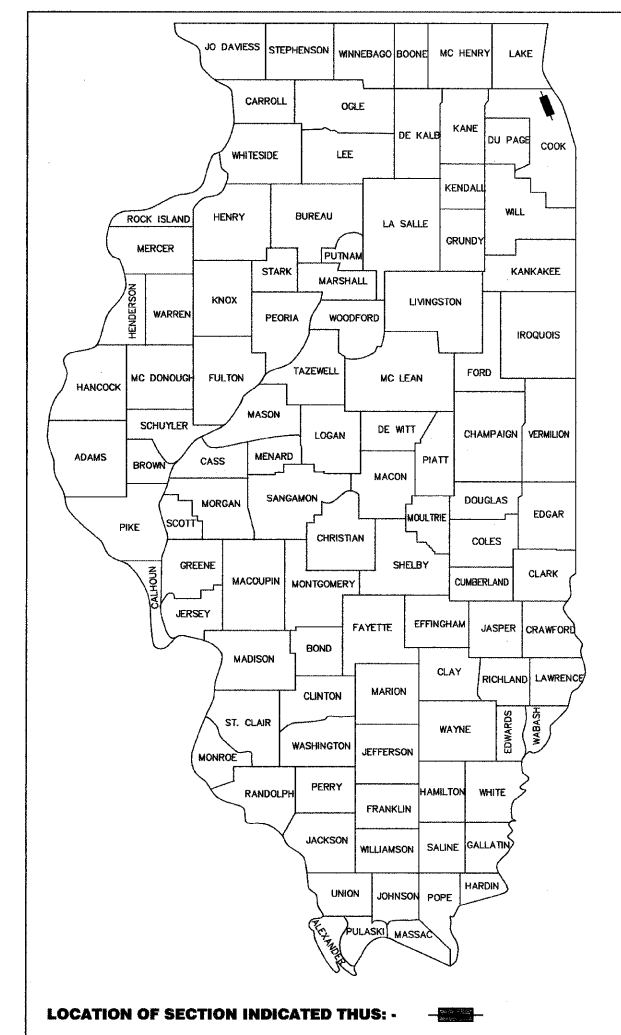


F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2734	09-00150-00-RS	COOK	23	1

CONTRACT: 63362

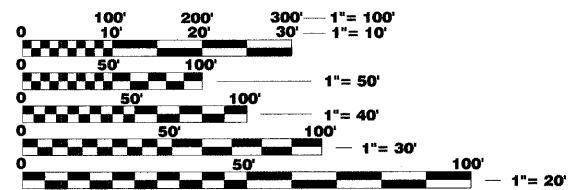
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PLANS FOR PROPOSED
FEDERAL-AID HIGHWAY
F.A.U. ROUTE 2734 (PFINGSTEN ROAD)
WHITE PINE DRIVE TO KEYSTONE ROAD
SECTION 09-00150-00-RS
PROJECT NO. ARA-9003(308)
VILLAGE OF NORTHBROOK
COOK COUNTY
C-91-587-09



- INDEX OF SHEETS**
- 1 COVER SHEET
 - 2 GENERAL NOTES, DETAIL AND SUMMARY OF QUANTITIES
 - 3 TYPICAL SECTIONS
 - 4-7 ROADWAY PLAN SHEETS
 - 8-11 PAVEMENT MARKING PLAN SHEETS
 - 12 DRIVEWAY DETAILS DISTANCE BETWEEN ROW AND CURB OR EDGE < 4.5M (15')
 - 13 DRIVEWAY DETAILS DISTANCE BETWEEN ROW AND FACE OF CURB OR EDGE < 4.5M (15')
 - 14 DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
 - 15 CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
 - 16 BUTT JOINT AND BITUMINOUS TAPER DETAILS
 - 17 TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS
 - 18 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT)
 - 19 DISTRICT ONE TYPICAL PAVEMENT MARKINGS
 - 20 PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
 - 21 TEMPORARY INFORMATION SIGNING
 - 22 DETECTOR LOOP INSTALLATION DETAIL FOR ROADWAY RESURFACING
 - 23 PAVEMENT PATCHING AND HMA SURFACED PAVEMENT
- HIGHWAY STANDARDS**
- 000001-05 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
 - 424001-05 CURB RAMPS FOR SIDEWALKS
 - 442201-03 CLASS C AND D PATCHES
 - 604076-04 FRAMES AND GRATES TYPE 21
 - 606001-04 CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
 - 606301-04 PC CONCRETE ISLANDS AND MEDIANS
 - 701606-06 URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
 - 701701-06 URBAN LANE CLOSURE, MULTILANE INTERSECTION
 - 701801-04 LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
 - 701901-01 TRAFFIC CONTROL DEVICES
 - 780001-02 TYPICAL PAVEMENT MARKINGS
 - 781001-03 TYPICAL APPLICATION RAISED REFLECTIVE PAVEMENT MARKERS
 - 886001-01 DETECTOR LOOP INSTALLATIONS

TRAFFIC DATA:
ADT 15,000 VPD (2006)

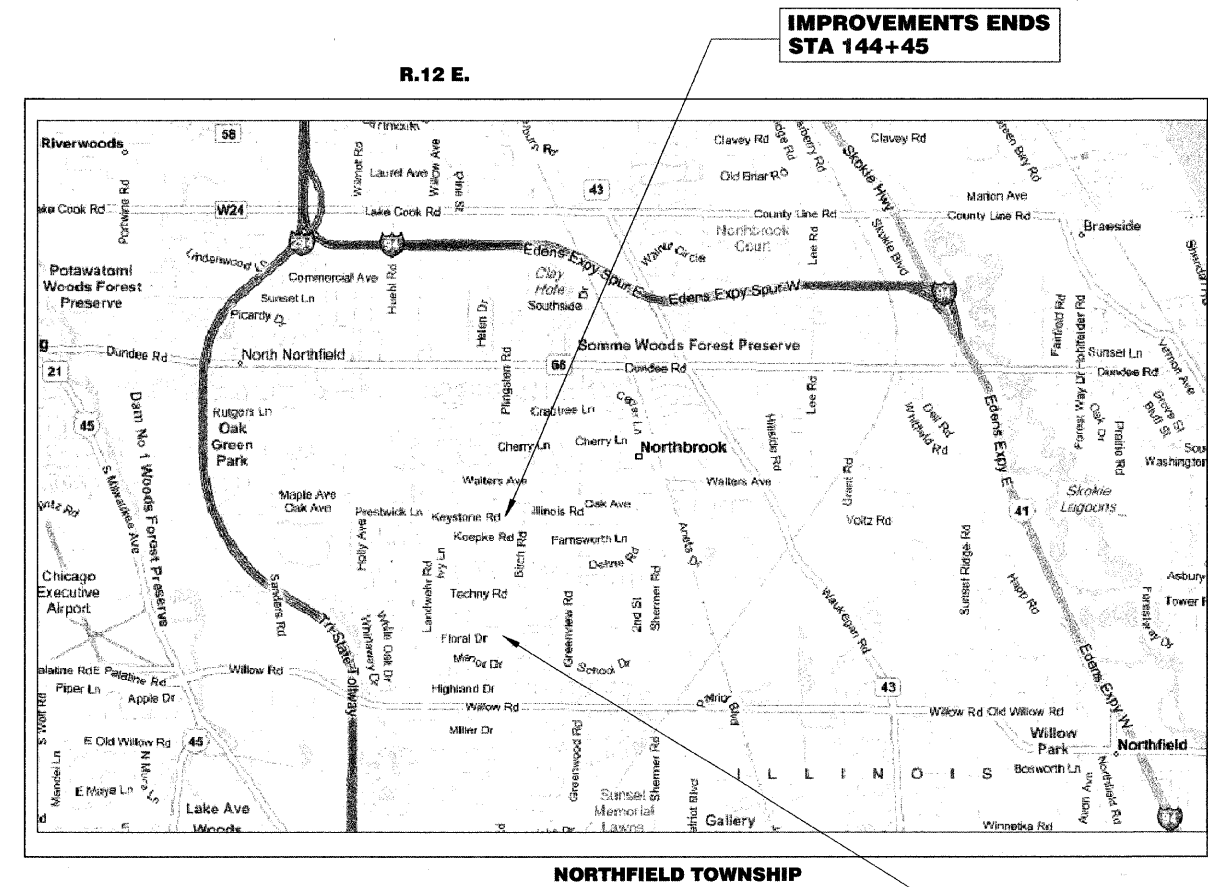
POSTED SPEED LIMIT - 40 MPH
DESIGN SPEED LIMIT - 45 MPH
CLASSIFICATION: ATERIAL



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123

CONTRACT NO. 63362



IMPROVEMENTS ENDS
STA 144+45



NORTHFIELD TOWNSHIP

IMPROVEMENTS BEGINS
STA 106+45

GROSS LENGTH OF PROJECT: 3,800 L.F. = 0.725 MILES
NET LENGTH OF PROJECT: 3,800 L.F. = 0.725 MILES

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED October 22, 2009

Paul K. Kendzior
VILLAGE ENGINEER

PASSED OCTOBER 26 2009

Cheryl Henderson
DISTRICT ONE ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID
 BASED ON LIMITED REVIEW OCTOBER 26, 2009

Diane M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER

PROFESSIONAL ENGINEER'S SIGN & SEAL

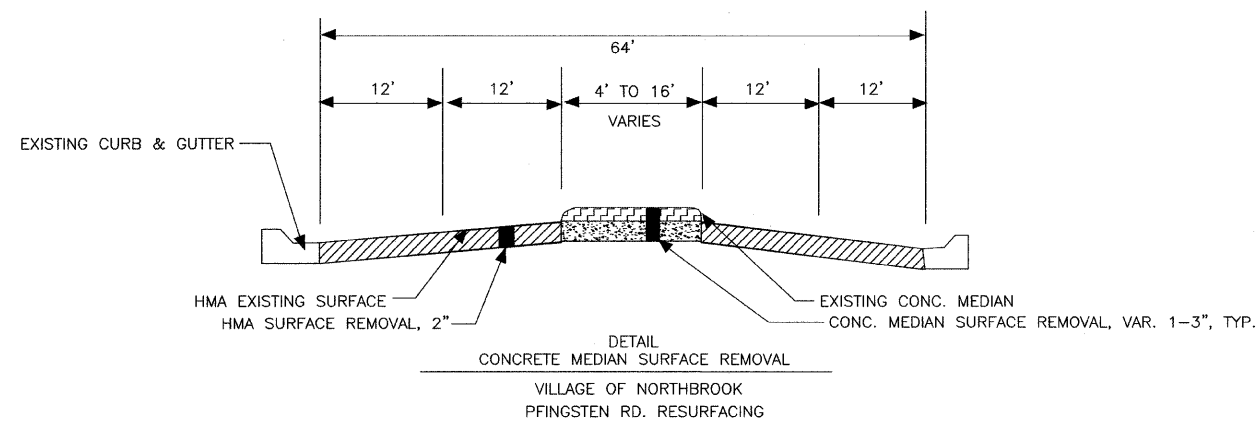
EXPIRES: PR. 1/30/11

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

ASSOCIATE FIELD ENGINEER: KEVIN STALLWORTH 847-705-4169

GENERAL NOTES:

1. THE CONTRACTOR SHALL PERFORM ALL WORK IN THIS CONTRACT IN A MANNER THAT CONFORMS WITH THE NORTHBROOK MUNICIPAL CODE. THE CONTRACTORS ATTENTION IS DRAWN SPECIFICALLY TO PROVISIONS OF THE MUNICIPAL CODE REGARDING CONSTRUCTION EQUIPMENT TRACKING MUD ON PUBLIC STREETS (SEC. 16-1) AND DUST CONTROL (SEC. 16-25).
2. "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" (REFERRED TO HEREAFTER AS "STANDARD SPECIFICATIONS"), AND ITS SUPPLEMENTAL REFERENCES, PUBLISHED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION AND THE "STANDARDS AND SPECIFICATIONS FOR PUBLIC AND PRIVATE IMPROVEMENTS", PUBLISHED BY THE VILLAGE OF NORTHBROOK, SHALL BE INCORPORATED BY REFERENCE INTO THIS CONTRACT. ANY CONTRADICTION BETWEEN THESE REFERENCES UNRESOLVED BY THE SPECIAL PROVISIONS SHALL BE INTERPRETED BY THE ENGINEER.
3. THE ASPHALT SURFACE SHALL BE MILLED IN A MANNER THAT WILL LEAVE THE EXISTING SURFACE 2 INCHES BELOW THE EDGE OF THE GUTTER FLAG WITH A CROSS SLOPE BETWEEN 1.5% AND 2.5%.
4. THE CONTRACTOR SHALL REMOVE AND DISPOSE OF THE SHORT TERM PAVEMENT MARKING FROM THE FINAL SURFACE JUST PRIOR TO OR JUST FOLLOWING THE INSTALLATION OF THE PERMANENT PAVEMENT MARKING. THE COST OF THE REMOVAL SHALL BE INCLUDED IN THE COST OF "SHORT TERM PAVEMENT MARKING".
5. COMBINATION CONCRETE CURB AND GUTTER SHALL BE MEASURED STRAIGHT ACROSS DRIVEWAYS AND TRANSITIONS. NO ADDITIONAL COMPENSATION SHALL BE ALLOWED FOR THE CURB AND GUTTER TRANSITION.
6. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS PRIOR TO BIDDING ON THIS PROJECT. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR FAILURE TO VERIFY EXISTING DIMENSIONS OR CONDITIONS.
7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER OR OWNER. THIS WORK SHALL BE DONE AT THE CONTRACTOR'S EXPENSE.
8. ONLY PRECAST CONCRETE ADJUSTMENT RINGS SHALL BE ALLOWED FOR THE ADJUSTMENT OF CATCH BASINS, MANHOLES, VALVE VAULTS AND INLETS.
9. CRUSHED CONCRETE SHALL NOT BE INSTALLED ON THIS CONTRACT, EXCEPT FOR TEMPORARY PURPOSES WHERE IT WILL BE COMPLETELY REMOVED FROM THE SITE.
10. THE CONTRACTOR SHALL ERECT "FRESH OIL" SIGNS PRIOR TO THE PLACEMENT OF THE PRIME COAT AND COMMENCEMENT OF PAVING OPERATIONS.
11. THE CONTRACTOR SHALL BE REQUIRED TO DISPOSE OF ALL SIDEWALKS, CURB AND GUTTER, PAVEMENT AND ALL OTHER MATERIALS EXCAVATED OR REMOVED DUE TO THE PROPOSED IMPROVEMENTS. ALL EXCESS EXCAVATED SHALL BE REMOVED FROM THE WORKSITE ON THE DAY OF ITS EXCAVATION. NO ADDITIONAL PAYMENT SHALL BE MADE FOR HAULING THESE MATERIALS OUTSIDE THE PROJECT LIMITS.
12. THE ACTUAL QUANTITIES FOR: PCC SW 5"; C&G REM & REP; AND CLASS D PATCHES WILL NOT EXCEED THE QUANTITY LISTED IN THE SUMMARY OF QUANTITIES.
13. IF PAVEMENT PATCHING OPERATIONS PRECEDE ASPHALT MILLING OPERATIONS, THE CONTRACTOR SHALL PROVIDE A SMOOTH, DRIVEABLE SURFACE FREE OF ABRUPT GRADE CHANGES AT HIS/HER OWN EXPENSE AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
14. ALL WORK NECESSARY TO RESTORE DISTURBED AREAS WILL NOT BE MEASURED FOR PAYMENT, BUT THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE ITEM CAUSING THE DISTURBANCE.
15. FRAMES AND GRATES, TYPE 21 SHALL HAVE THE WORDS "DUMP NO WASTE" AND "DRAINS TO WATERWAYS" AND A SIDEVIEW OF A FISH STAMPED OR CAST INTO THE TOP SIDE OF CURB BACK.



SPECIALTY ITEM	ITEM NO.	DESCRIPTION	UNIT	XXXX-XX
	40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	2700
	40600300	AGGREGATE (PRIME COAT)	TON	60
	40600400	MIXTURES FOR CRACKS, JOINTS AND FLANGWAYS	TON	10
	40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	1200
	40600895	CONSTRUCTING TEST STRIP	EACH	1
	40600982	HOT MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	270
	40603340	HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	2350
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK, 5'	SQ FT	1000
	42400800	DETECTABLE WARNINGS	SQ FT	100
	44000157	HOT MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	25000
	44000600	SIDEWALK REMOVAL	SQ FT	1100
	44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	500
	44002020	CONCRETE MEDIAN SURFACE REMOVAL	SQ FT	18000
	44201749	CLASS D PATCHES, TYPE I, 9 INCH	SQ YD	320
	44201753	CLASS D PATCHES, TYPE II, 9 INCH	SQ YD	320
	44201757	CLASS D PATCHES, TYPE III, 9 INCH	SQ YD	320
	44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SQ YD	320
	44300100	AREA REFLECTIVE CRACK CONTROL TREATMENT	SQ YD	27000
	60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	5
	60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	30
	60404920	FRAMES AND GRATES, TYPE 21	EACH	1
	67100100	MOBILIZATION	L SUM	1
	70102625	TRAFFIC CONTROL AND PROTECTION STANDARD - 70106	L SUM	1
	70102635	TRAFFIC CONTROL AND PROTECTION STANDARD - 701701	L SUM	1
	70102640	TRAFFIC CONTROL AND PROTECTION STANDARD - 701801	L SUM	1
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	6000
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	250
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE, 4"	FOOT	14600
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE, 6"	FOOT	3000
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE, 12"	FOOT	300
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE, 24"	FOOT	500
*	78100100	RAISED REFLECTOR PAVEMENT MARKER	EACH	350
*	78300200	RAISED REFLECTOR PAVEMENT MARKER REMOVAL	EACH	140
*	88600600	DETECTOR LOOP REPLACEMENT	FOOT	480
	X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	100
	XX006947	HOT MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT	SQ YD	250

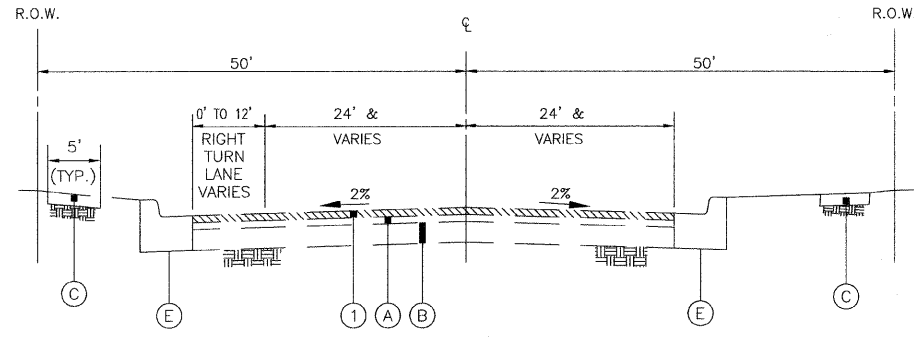
VILLAGE OF NORTHBROOK

PFINGSTEN ROAD RESURFACING
GENERAL NOTES, DETAIL
AND SUMMARY OF QUANTITIES

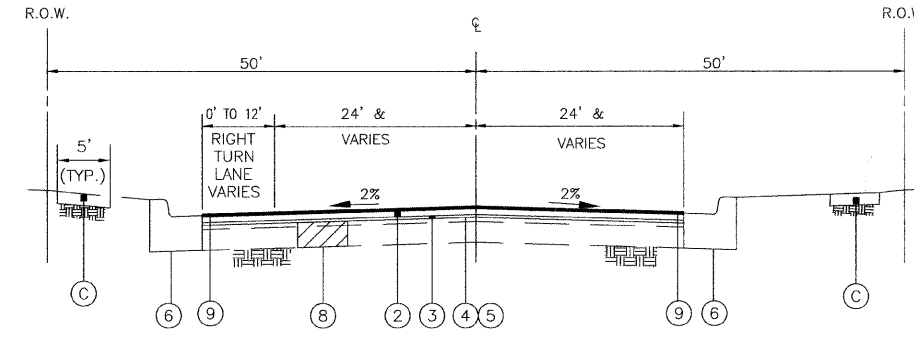
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SCALE: N.T.S.
FILE NO.: N/A

CONTRACT: 63362

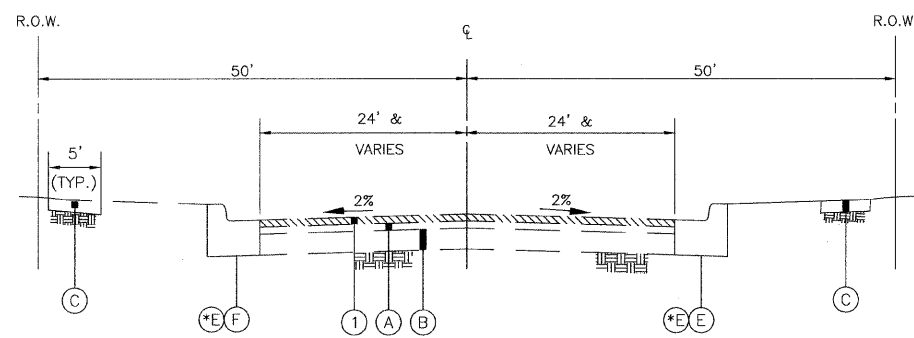


EXISTING TYPICAL SECTION
STA. 106+45 TO 107+00

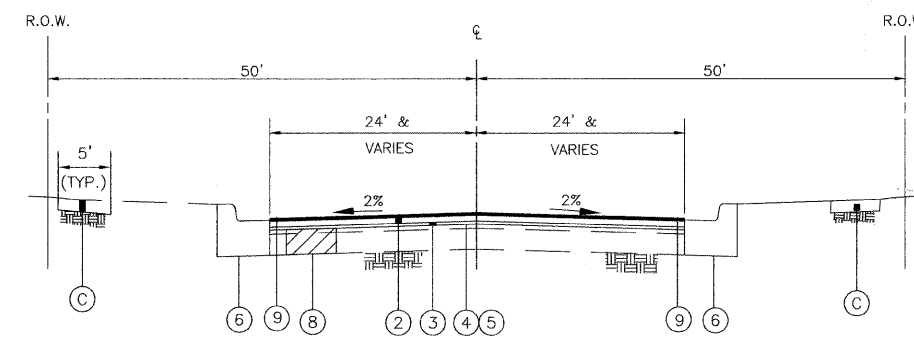


PROPOSED TYPICAL SECTION
STA. 106+45 TO 107+00

- (A) EXISTING ASPHALT COURSE
- (B) EXISTING P.C.C. BASE COURSE
- (C) EXISTING P.C.C. SIDEWALK
- (D) CORRUGATED CONCRETE MEDIAN
- (E) COMB. CONC. CURB & GUTTER, TYPE B-6.12
- (1) HOT MIX ASPHALT SURFACE REMOVAL, 2"
- (2) HOT MIX ASPHALT, MIX D, N70, IL-9.5MM, (1-1/2")
- (3) HOT MIX ASPHALT LEVELING BINDER IL-9.5 (MACHINE METHOD), N70 (3/4")
- (4) BITUMINIOUS MATERIALS (PRIME COAT)
- (5) AGGREGATE (PRIME COAT)
- (6) COMB. CONC. CURB & GUTTER REMOVAL AND REPLACEMENT, AS DIRECTED BY THE ENGINEER
- (7) CONCRETE MEDIAN SURFACE REMOVAL
- (8) CLASS D PATCHES, BINDER IL-19MM, 9", AS SHOWN ON PLANS AND DIRECTED BY THE ENGINEER
- (9) HMA SURFACE COURSE TO BE 1/4" ABOVE GUTTER FLAG, (TYP.)



EXISTING TYPICAL SECTION
*STA. 107+00 TO 110+00
STA. 143+00 TO 144+45

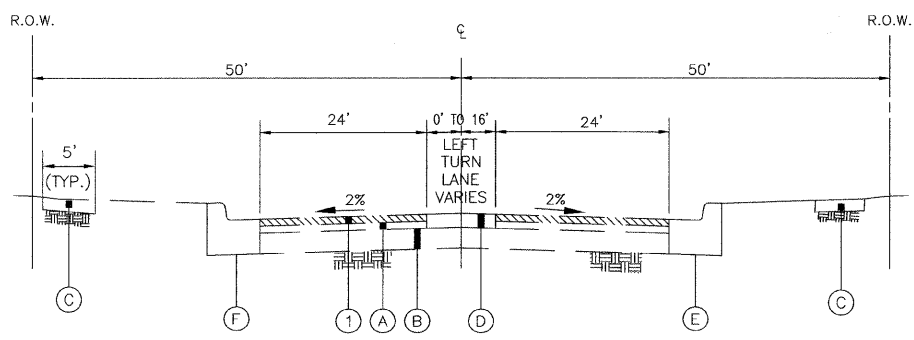


PROPOSED TYPICAL SECTION
*STA. 107+00 TO 110+00
STA. 143+00 TO 144+45

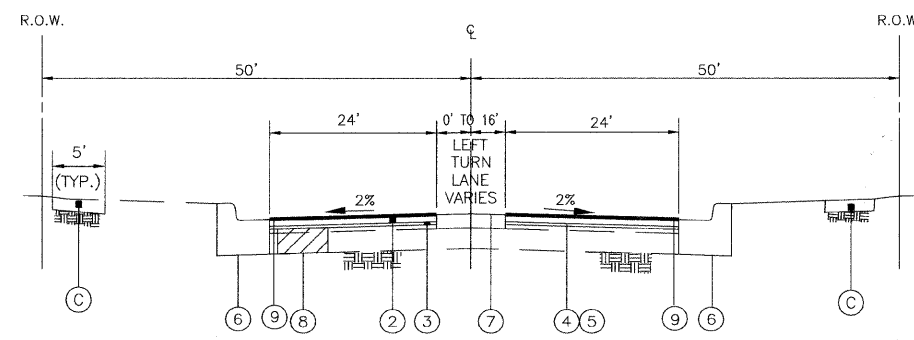
NOTE:
** "CONTRACTOR SHALL MILL BEFORE PATCHING".

MIXTURE USE	DESIGN AIR VOIDS
HOT MIX ASPHALT, MIX D, N70, IL-9.5MM	4% @ 70 GYR.
HMA LEVELING BINDER IL-9.5 (MACHINE METHOD), N70	4% @ 70 GYR.
CLASS D PATCHES, BINDER IL-19MM, TYPE I-IV, 9 IN.	4% @ 70 GYR.
HMA SURF. CSE., MIX C, IL-9.5MM, N50 (DRIVEWAY)	4% @ 50 GYR.

NOTE:
1. THE UNIT WEIGHT USED TO CALCULATE ALL ASPHALT MIXTURES IS 112 LBS./SQ.YD./IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS."
3. "FOR PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS."



EXISTING TYPICAL SECTION
STA. 110+00 TO 143+00



PROPOSED TYPICAL SECTION
STA. 110+00 TO 143+00

J:\TOMICH\SKOKIE LAPP\SKOKIE TYP-SEC.DWG

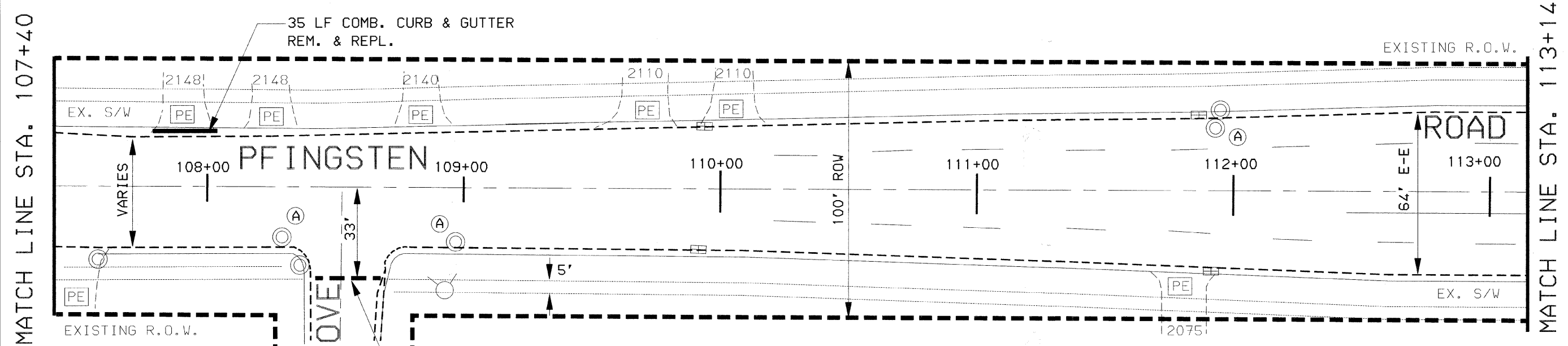
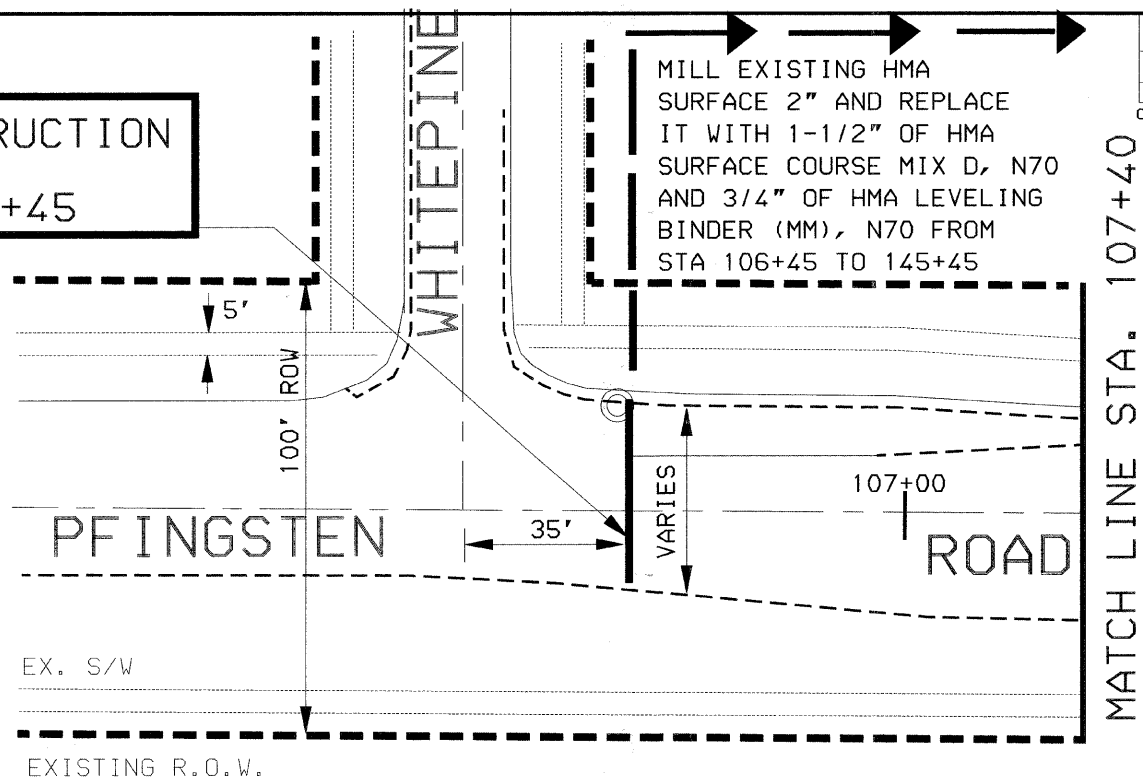
VILLAGE OF NORTHBROOK
PFINGSETN ROAD RESURFACING
TYPICAL SECTION

DESIGN: JWH
DRAWN: JWH
CHECKED: RJD
DATE: 5/9/09
SCALE: N.T.S.
FILE NO.: N/A

F. A. U. ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2734	09-00150-00-RS	COOK	23	4
FHWA REGION NO.		ILLINOIS PROJECT		
CONTRACT: 63362				

**BEGIN CONSTRUCTION
STA. 106+45**

MILL EXISTING HMA SURFACE 2" AND REPLACE IT WITH 1-1/2" OF HMA SURFACE COURSE MIX D, N70 AND 3/4" OF HMA LEVELING BINDER (MM), N70 FROM STA 106+45 TO 145+45



LEGEND

- (A) FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
- REC CATCH BASINS TO BE RECONSTRUCTED
- △ FRAMES AND GRATES, TYPE 21

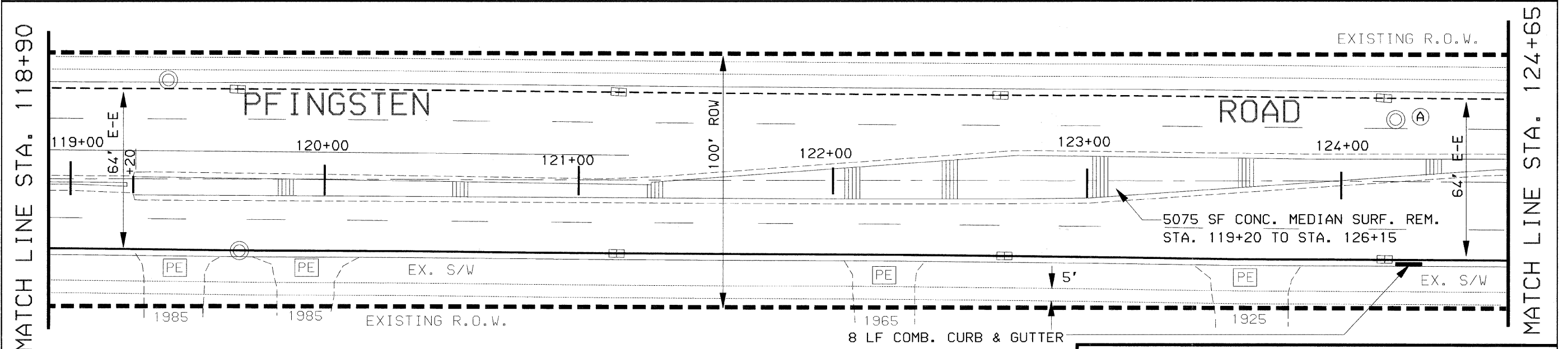
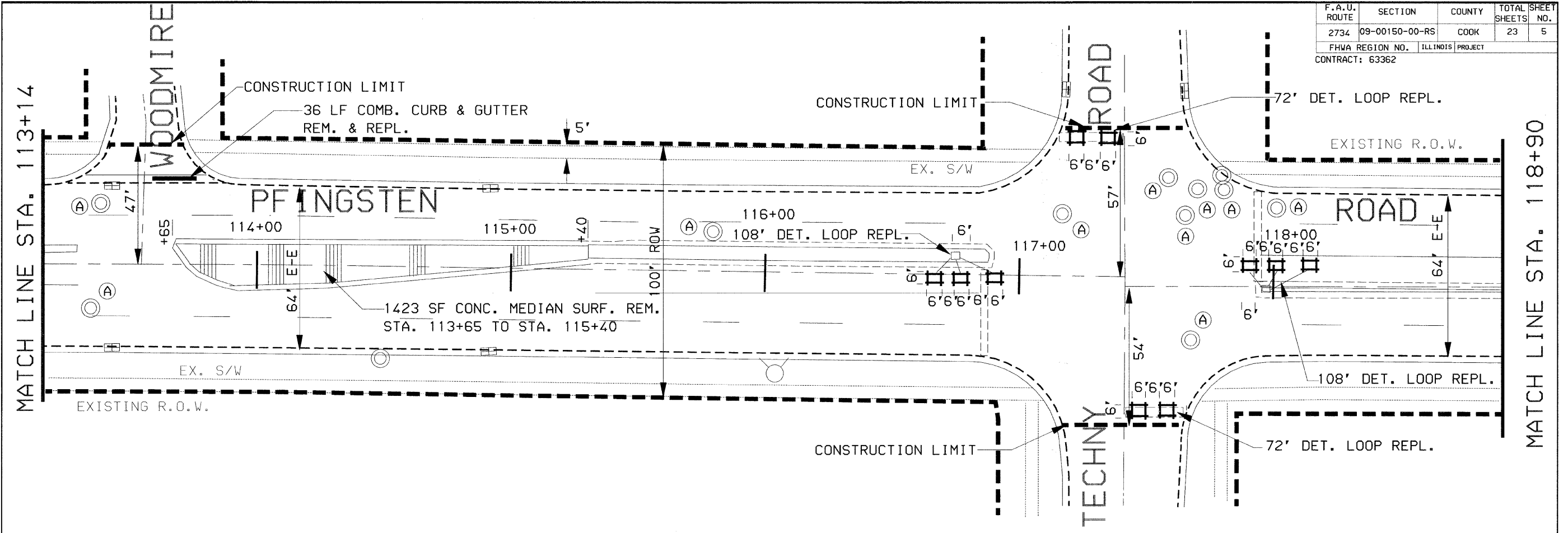
STATION	SIDE	# OF LOOPS	EST. LENGTH
116+70	RT.	3	108'
117+50	RT.	1	72'
117+50	LT.	1	72'
118+00	LT.	3	108
137+30	LT.	1	60'
137+50	LT.	1	60'
TOTAL ESTIMATED LENGTH			480 LF

VILLAGE OF NORTHBROOK

PFLINGSTEN ROAD RESURFACING
EXISTING AND PROPOSED PLAN
STA. 106+45 TO STA. 113+14

DESIGN: JWH	DATE: 4/20/09
DRAWN: JWH	SCALE: 20'
CHECKED: RJD	FILE NO.:

F.A.U. ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2734	09-00150-00-RS	COOK	23	5
FHWA REGION NO.		ILLINOIS PROJECT		
CONTRACT: 63362				



LEGEND

- (A) FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
- REC CATCH BASINS TO BE RECONSTRUCTED
- △ FRAMES AND GRATES, TYPE 21

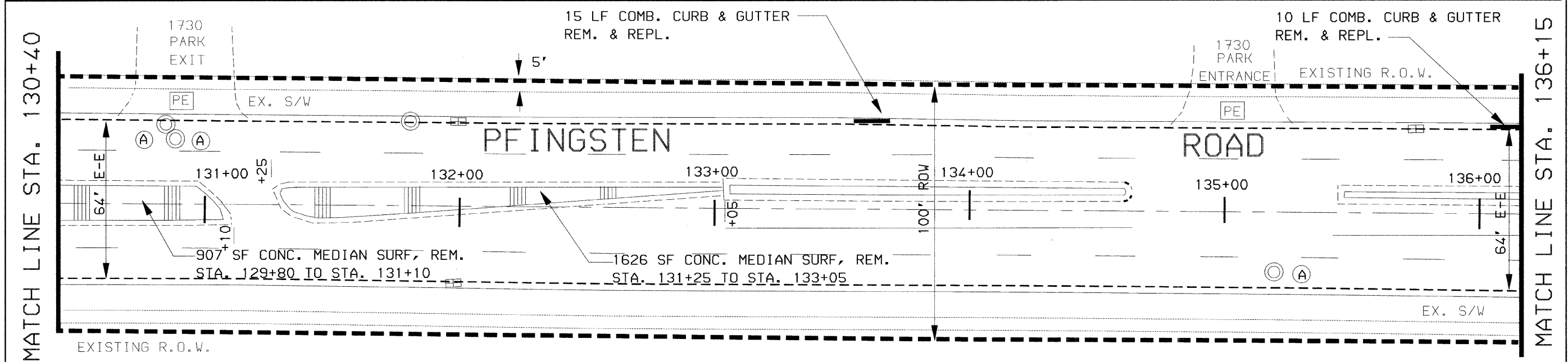
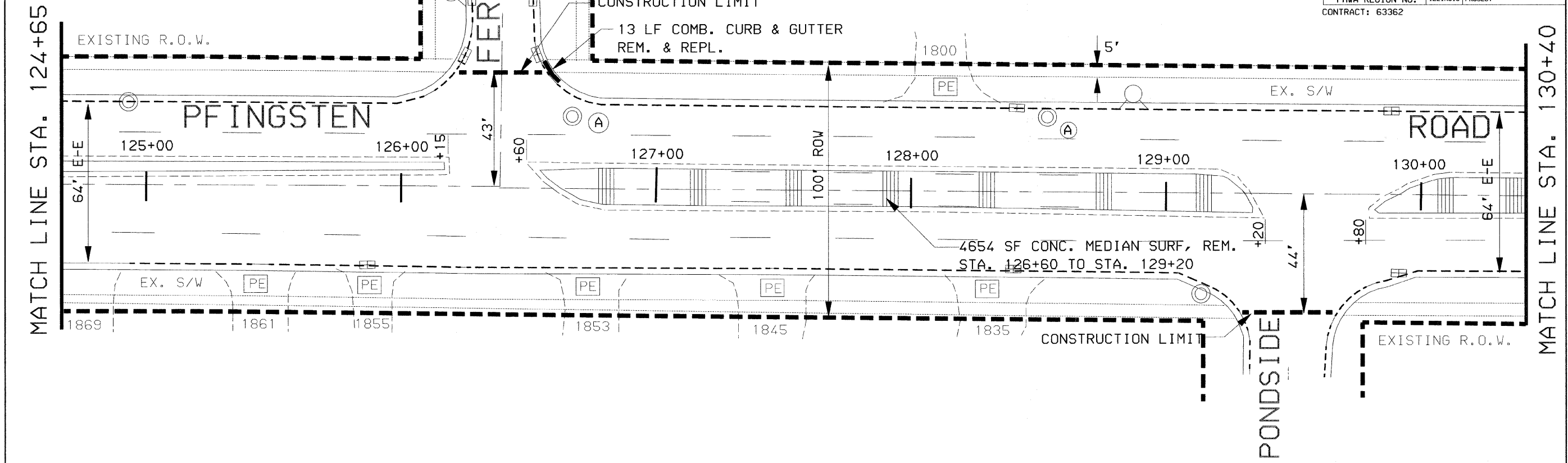


VILLAGE OF NORTHBROOK

PFINGSTEN ROAD RESURFACING
EXISTING AND PROPOSED PLAN
STA. 113+14 TP STA. 124+65

DESIGN: JWH	DATE: 4/20/09
DRAWN: JWH	SCALE: 20'
CHECKED: RJD	FILE NO.:

F.A.U. ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2734	09-00150-00-RS	COOK	23	6
FHWA REGION NO.		ILLINOIS PROJECT		
CONTRACT: 63362				



LEGEND

- (A) FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
- REC CATCH BASINS TO BE RECONSTRUCTED
- △ FRAMES AND GRATES, TYPE 21

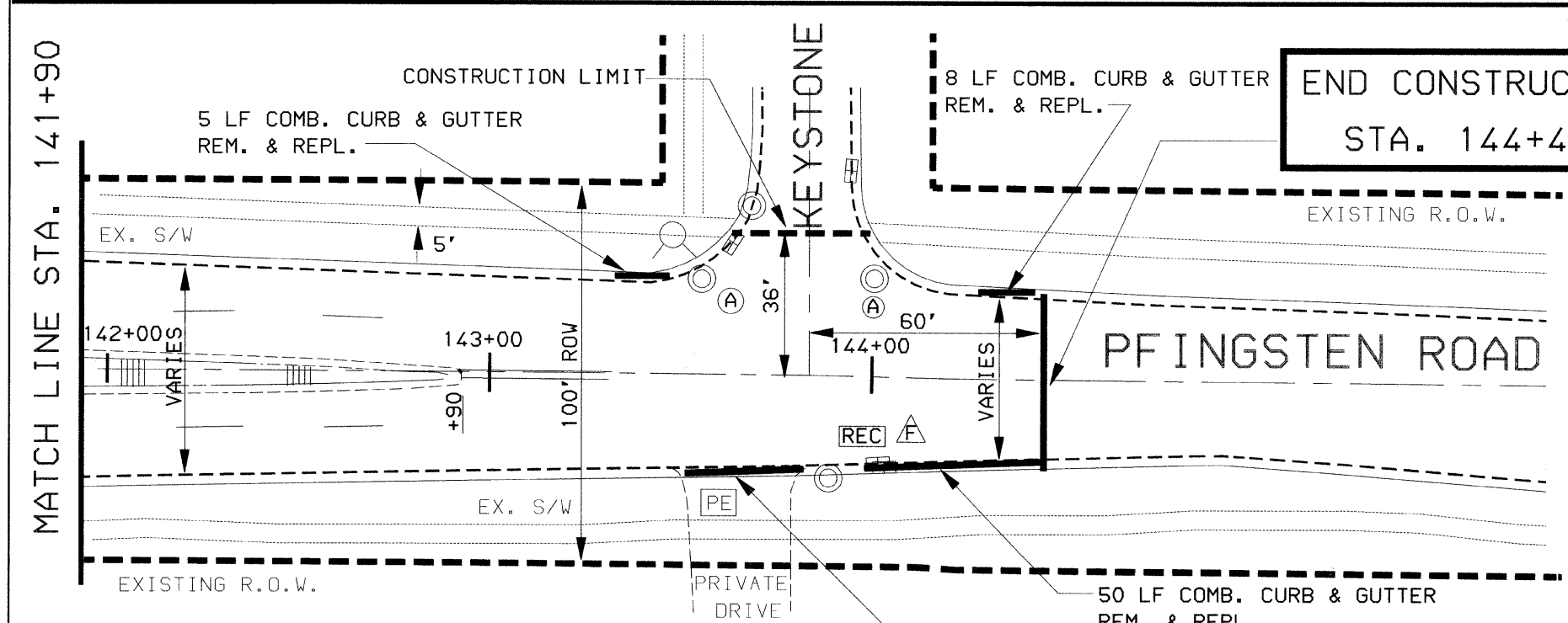
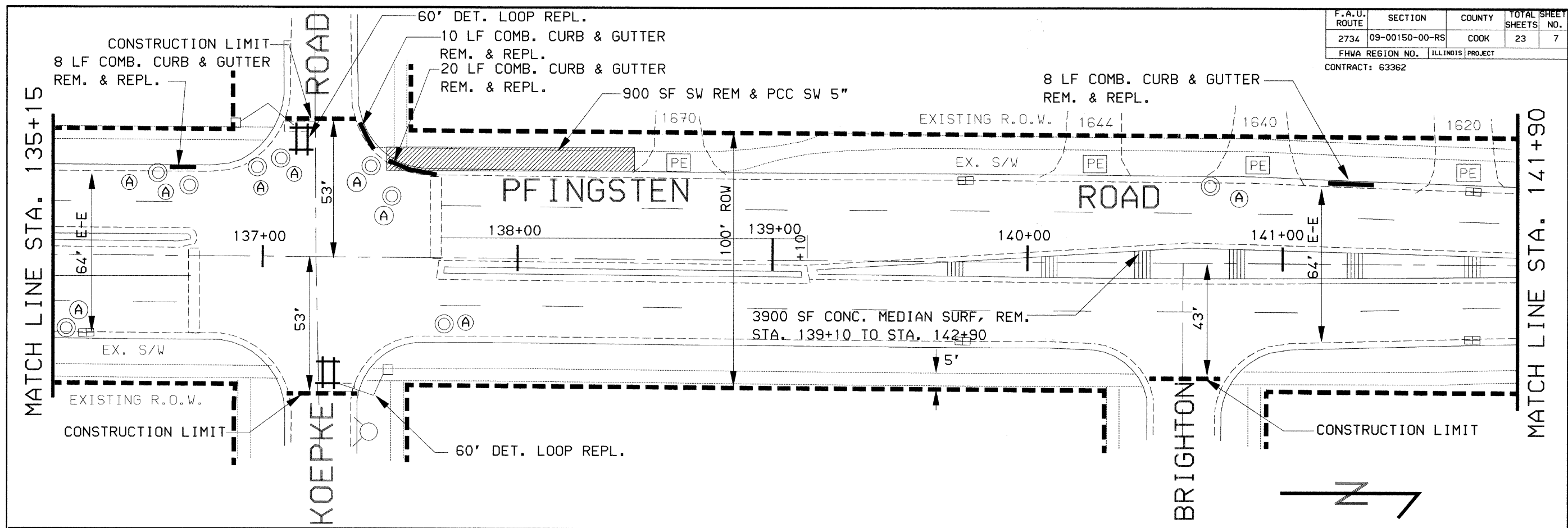


VILLAGE OF NORTHBROOK

PFINGSTEN ROAD RESURFACING
EXISTING AND PROPOSED PLAN
STA. 124+65 TO STA. 136+15

DESIGN: JWH	DATE: 4/20/09
DRAWN: JWH	SCALE: 20'
CHECKED: RJD	FILE NO.:

F.A.U. ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2734	09-00150-00-RS	COOK	23	7
FHWA REGION NO.		ILLINOIS PROJECT		
CONTRACT: 63362				



- LEGEND**
- (A) FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
 - REC CATCH BASINS TO BE RECONSTRUCTED
 - △ FRAMES AND GRATES, TYPE 21

VILLAGE OF NORTHBROOK

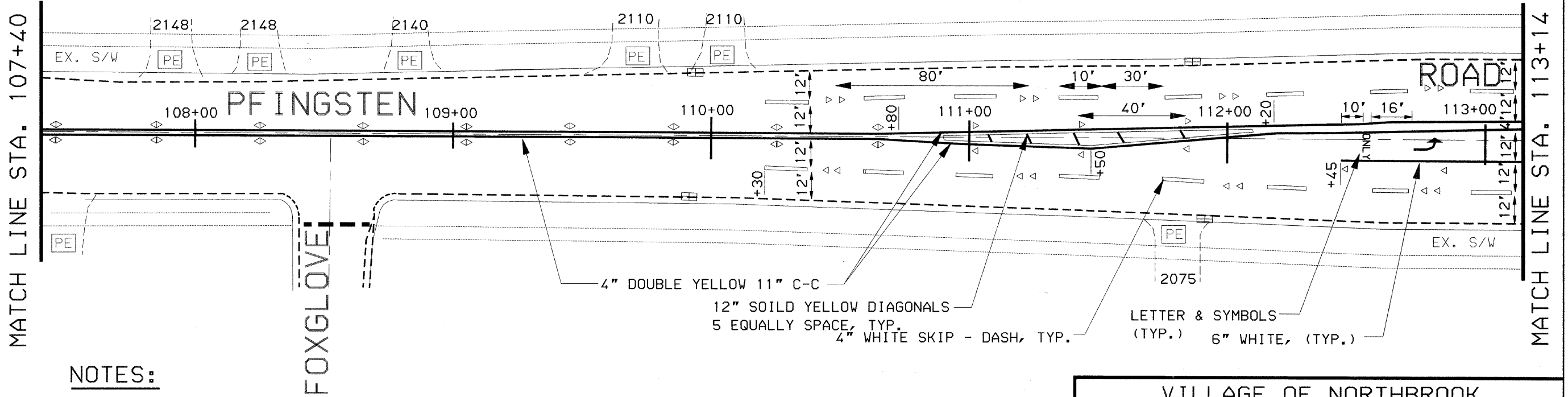
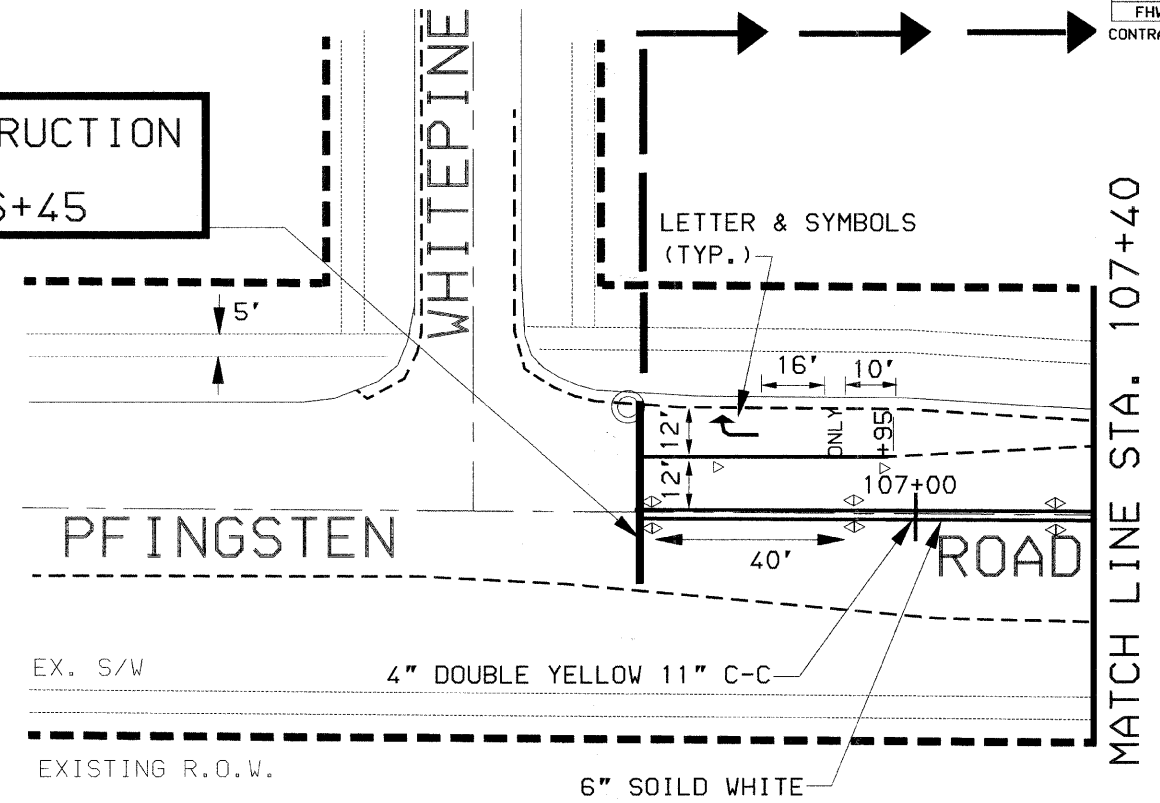
PFINGSTEN ROAD RESURFACING
EXISTING AND PROPOSED PLAN
STA. 136+15 TO STA. 144+45

DESIGN: JWH	DATE: 4/20/09
DRAWN: JWH	SCALE: 20'
CHECKED: RJD	FILE NO.:

F.A.U. ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2734	09-00150-00-RS	COOK	23	8
FHWA REGION NO.		ILLINOIS PROJECT		
CONTRACT: 63362				



BEGIN CONSTRUCTION
STA. 106+45



NOTES:

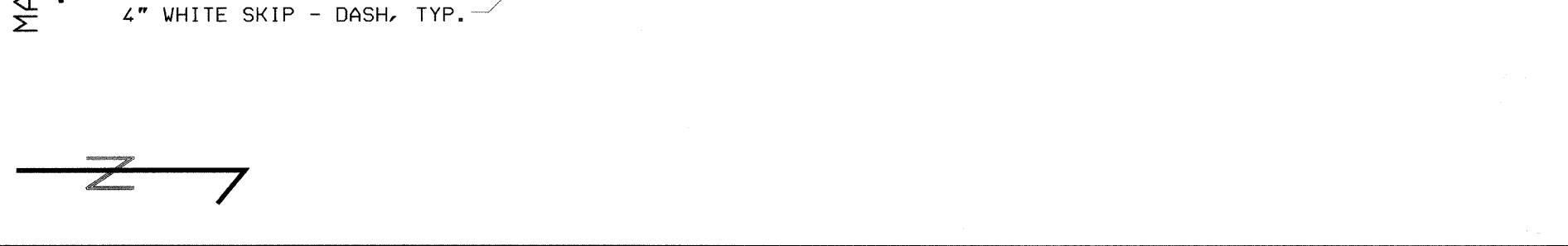
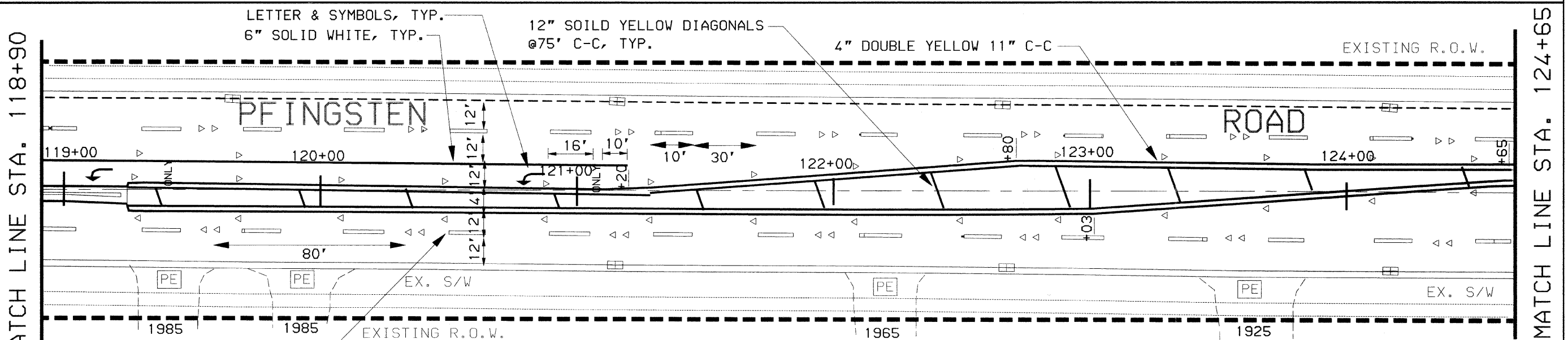
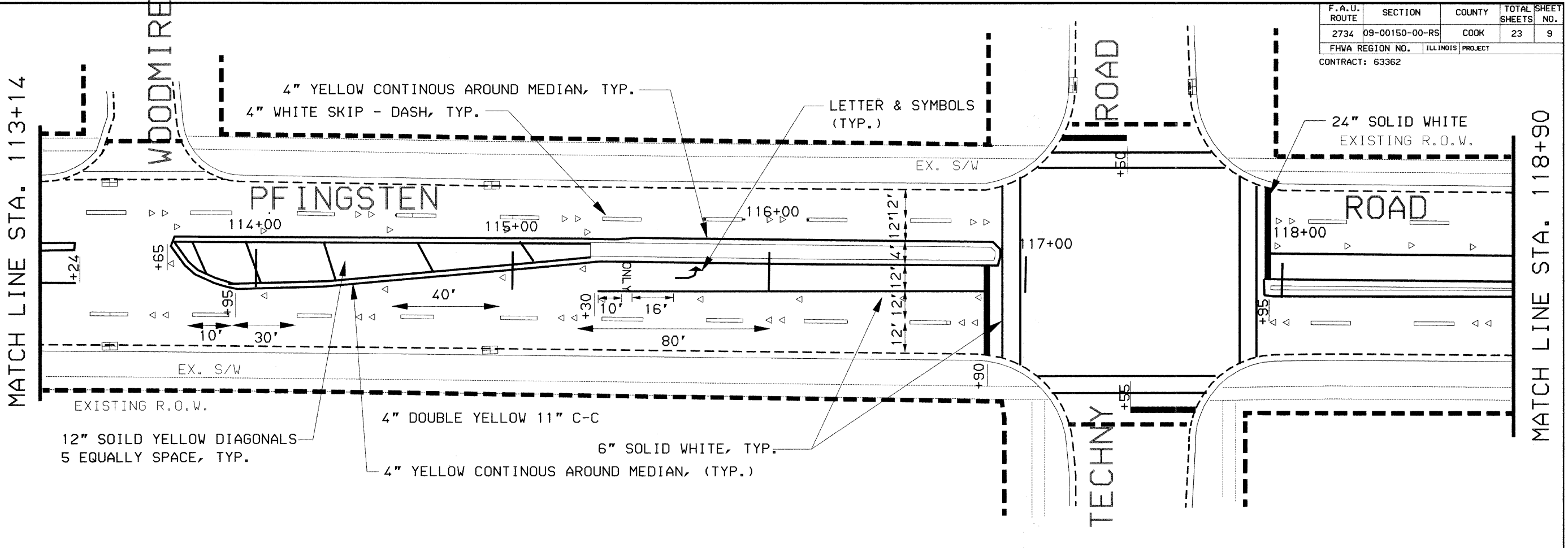
1. REFER TO DISTRICT 1 STANDARD DETAIL TC-11 FOR ADDITIONAL DETAILS REGARDING APPLICATIONS OF RAISED REFLECTIVE PAVEMENT MARKERS
2. REFER TO DISTRICT 1 STANDARD DETAIL TC-13 FOR ADDITIONAL DETAILS REGARDING APPLICATIONS OF RAISED REFLECTIVE PAVEMENT MARKERS
3. ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC

VILLAGE OF NORTHBROOK

PFINGSTEN ROAD RESURFACING
EXISTING AND PROPOSED PAVEMENT MARKINGS
STA. 107+40 TO STA. 113+14

DESIGN: JWH	DATE: 4/23/09
DRAWN: JWH	SCALE: 20'
CHECKED: RJD	FILE NO.:

F.A.U. ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2734	09-00150-00-RS	COOK	23	9
FHWA REGION NO.		ILLINOIS PROJECT		
CONTRACT: 63362				

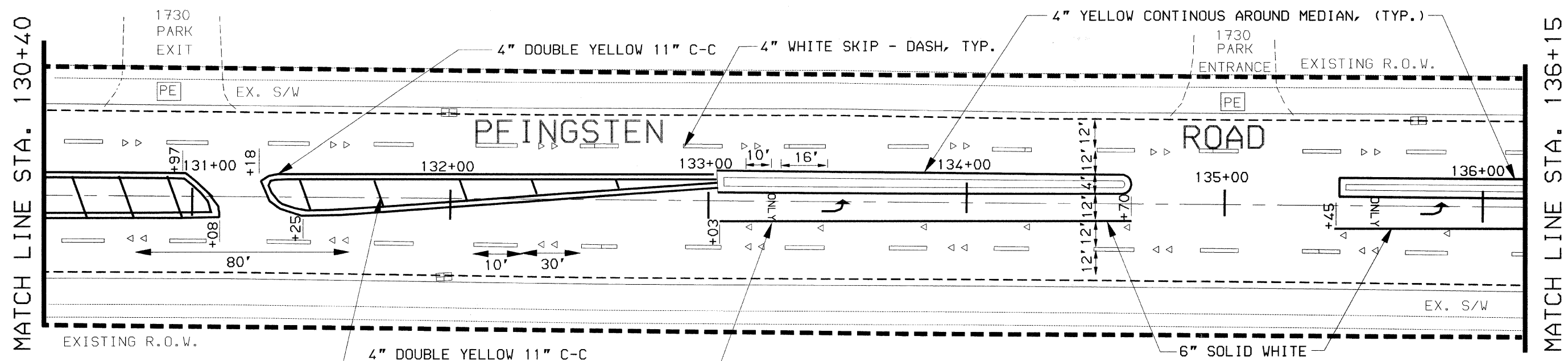
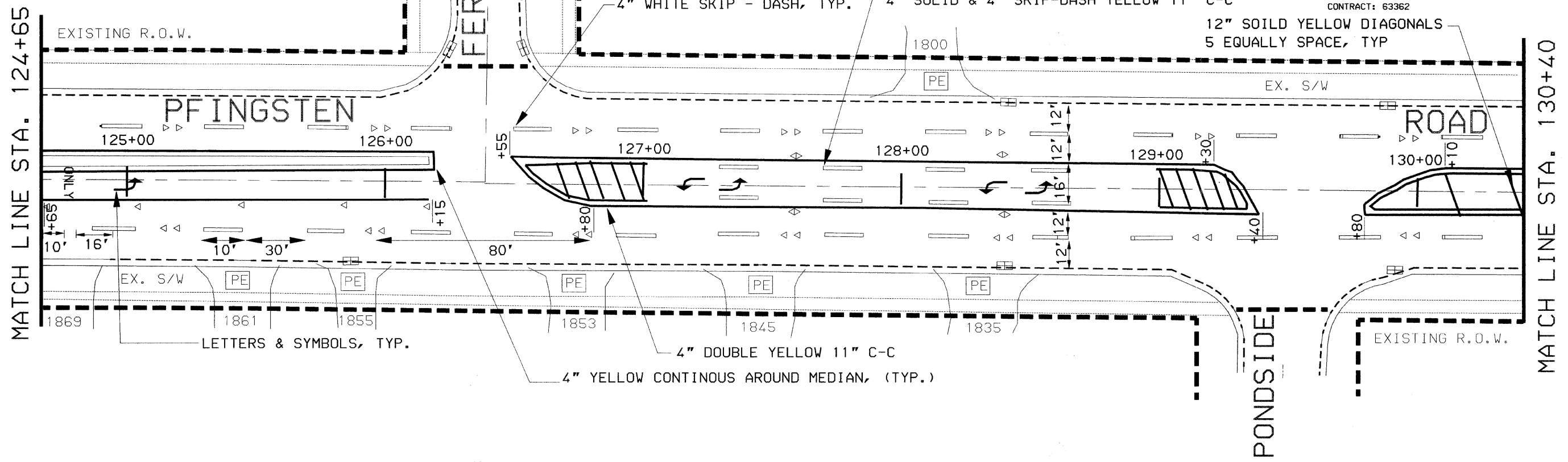


VILLAGE OF NORTHBROOK

PFINGSTEN ROAD RESURFACING
EXISTING AND PROPOSED PAVEMENT MARKINGS
STA. 113+14 TO 124+65

DESIGN: JWH	DATE: 4/23/09
DRAWN: JWH	SCALE: 20'
CHECKED: RJD	FILE NO.:

F.A.U. ROUTE	SECTION	COUNTY	TOTAL SHEET SHEETS	NO.
2734	09-00150-00-RS	COOK	23	10
FHWA REGION NO. ILLINOIS PROJECT		CONTRACT: 63362		

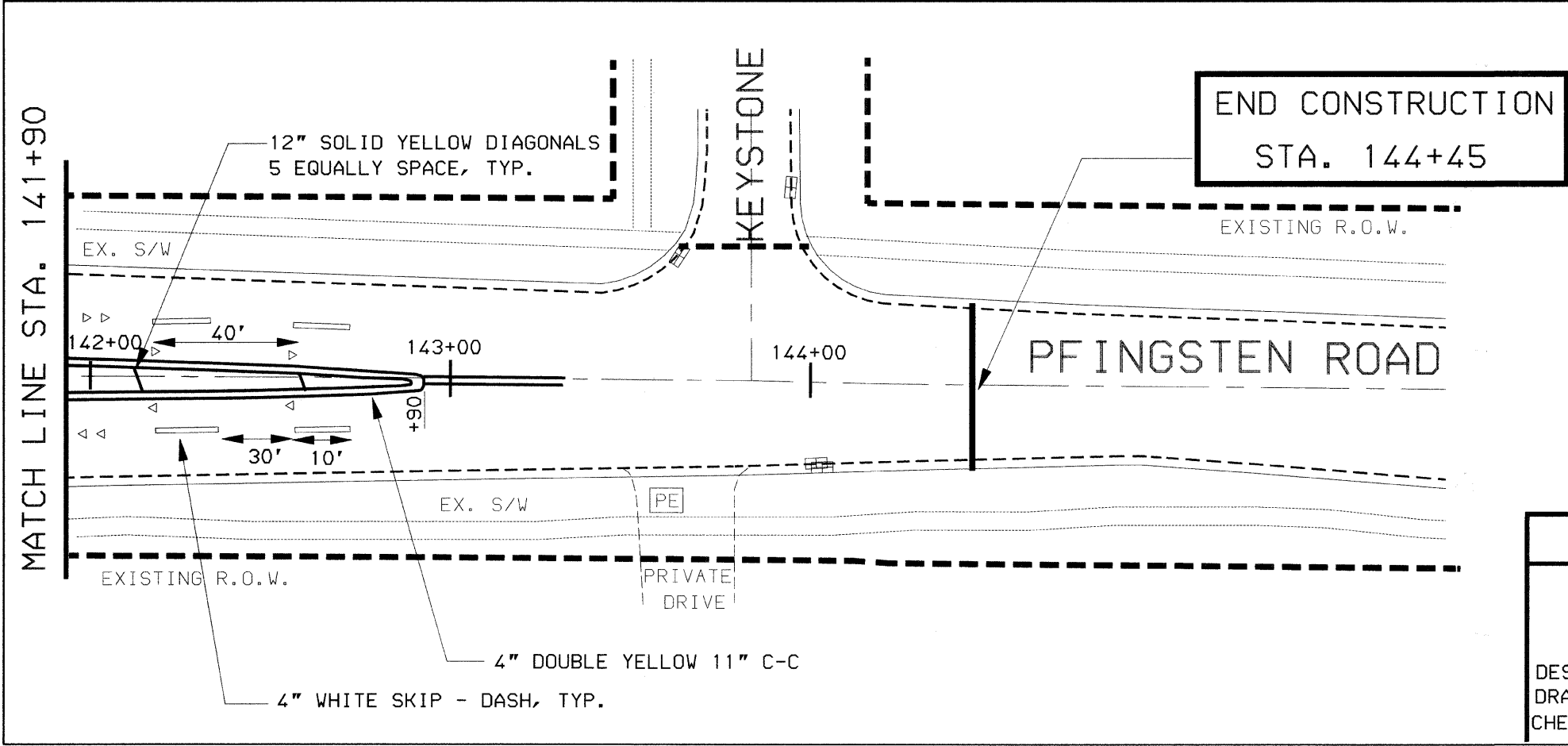
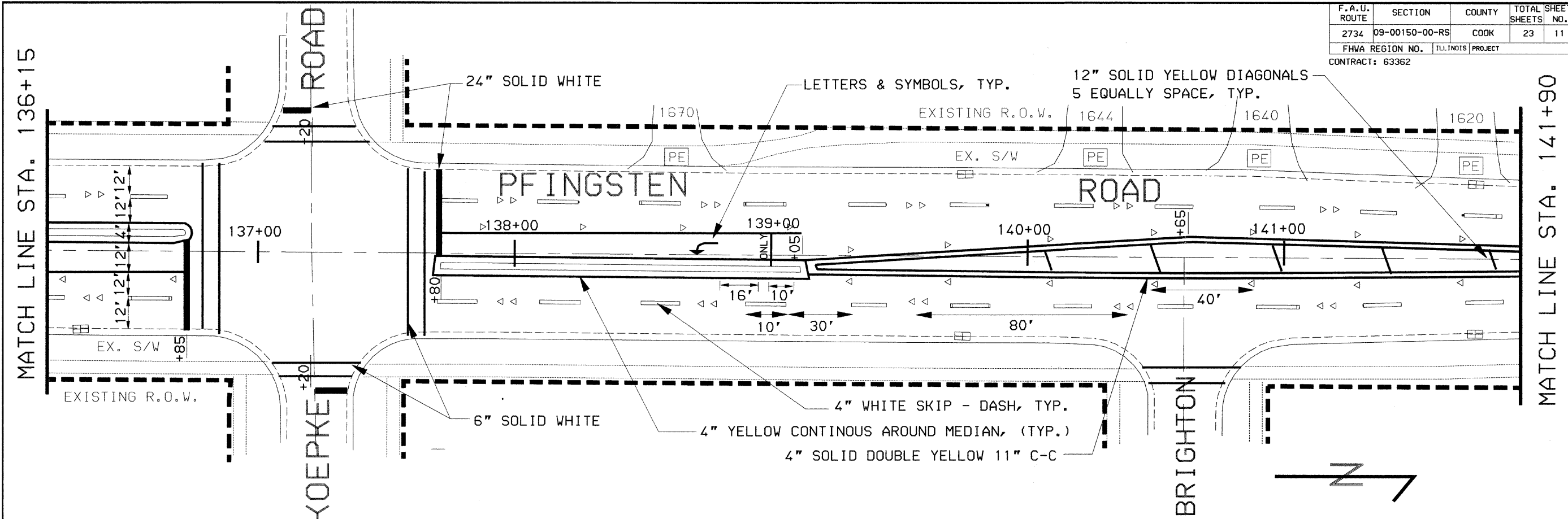


VILLAGE OF NORTHBROOK

PFINGSTEN ROAD RESURFACING
EXISTING AND PROPOSED PAVEMENT MARKINGS
STA. 124+65 TO STA. 135+15

DESIGN: JWH	DATE: 4/23/09
DRAWN: JWH	SCALE: 20'
CHECKED: RJD	FILE NO.:

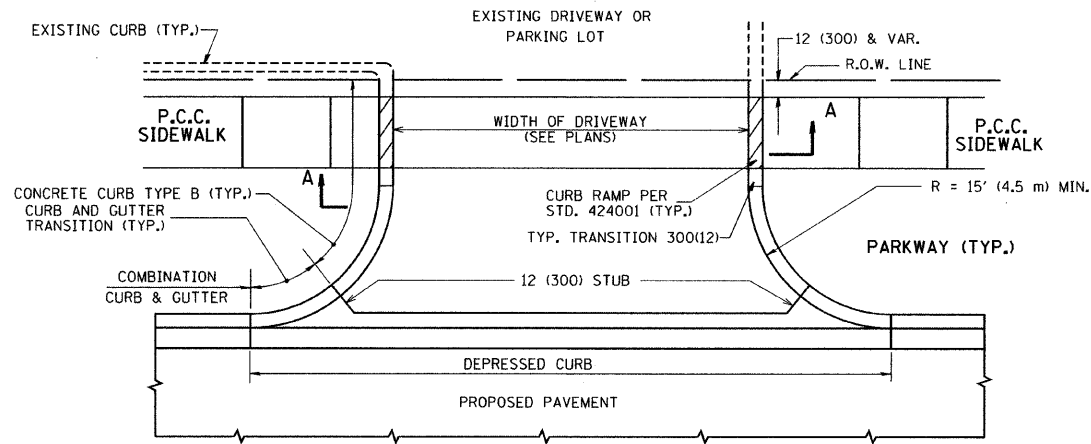
F.A.U. ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2734	09-00150-00-RS	COOK	23	11
FHWA REGION NO.		ILLINOIS PROJECT		
CONTRACT: 63362				



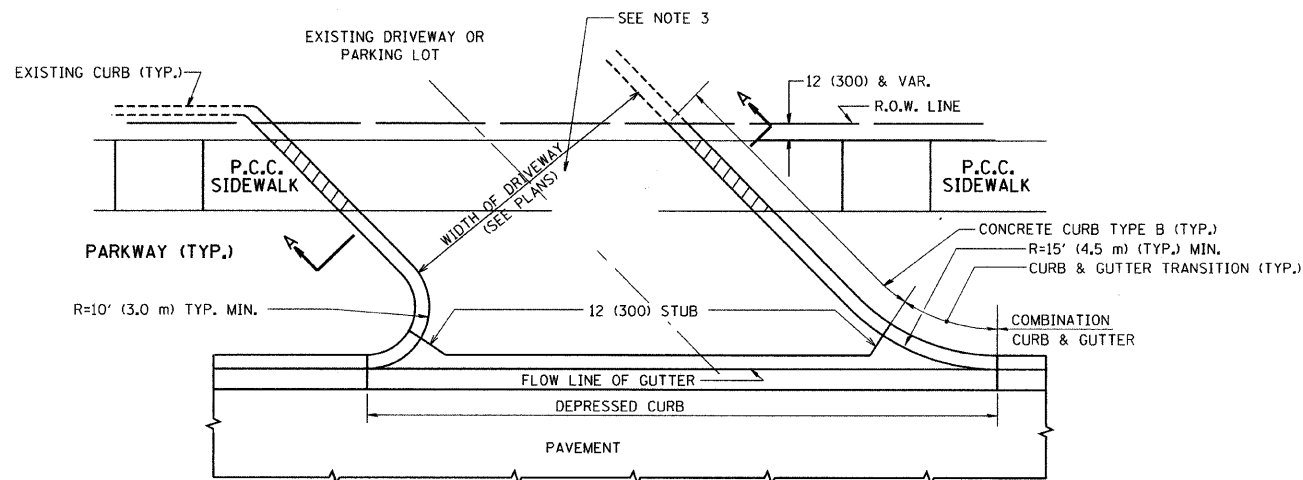
VILLAGE OF NORTHBROOK

PFINGSTEN ROAD RESURFACING
EXISTING AND PROPOSED PAVEMENT MARKINGS
STA. 136+15 TO STA. 144+45

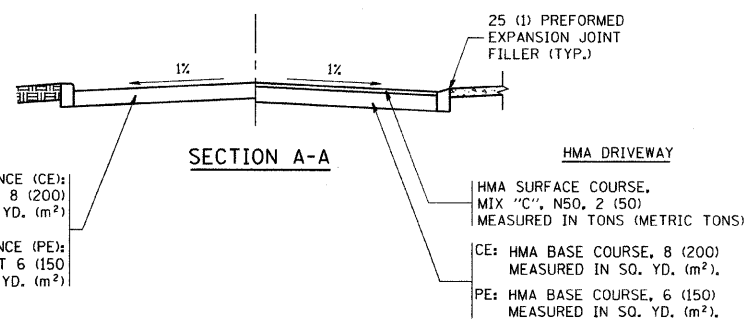
DESIGN: JWH	DATE: 4/23/09
DRAWN: JWH	SCALE: 20'
CHECKED: RJD	FILE NO.:



WITH CONCRETE CURB, TYPE B

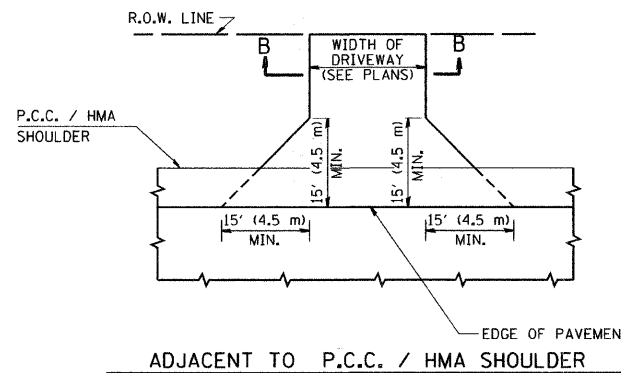


WITH CONCRETE CURB, TYPE B

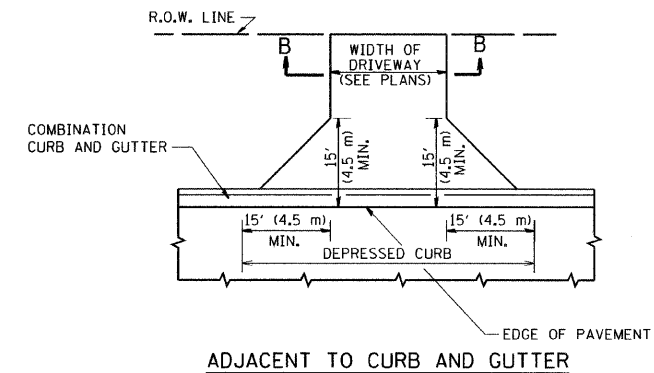


RIGID DRIVEWAY
 COMMERCIAL ENTRANCE (CE):
 P.C.C. DRIVEWAY PAVEMENT 8 (200)
 MEASURED IN SQ. YD. (m²)
 NON-COMMERCIAL ENTRANCE (PE):
 P.C.C. DRIVEWAY PAVEMENT 6 (150)
 MEASURED IN SQ. YD. (m²)

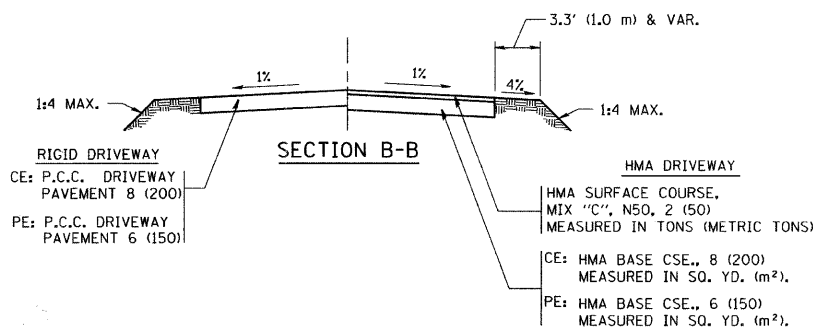
HMA DRIVEWAY
 HMA SURFACE COURSE,
 MIX "C", N50, 2 (50)
 MEASURED IN TONS (METRIC TONS)
 CE: HMA BASE COURSE, 8 (200)
 MEASURED IN SQ. YD. (m²),
 PE: HMA BASE COURSE, 6 (150)
 MEASURED IN SQ. YD. (m²).



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



RURAL FIELD ENTRANCE (FE)
 HMA SURFACE COURSE,
 MIX "C", N50, 2 (50)
 MEASURED IN TONS (METRIC TONS)
 CE: HMA BASE CSE., 8 (200)
 MEASURED IN SQ. YD. (m²),
 PE: HMA BASE CSE., 6 (150)
 MEASURED IN SQ. YD. (m²),
 AGGREGATE BASE CSE., TYPE B, 8 (200)
 MEASURED IN SQ. YD. (m²).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

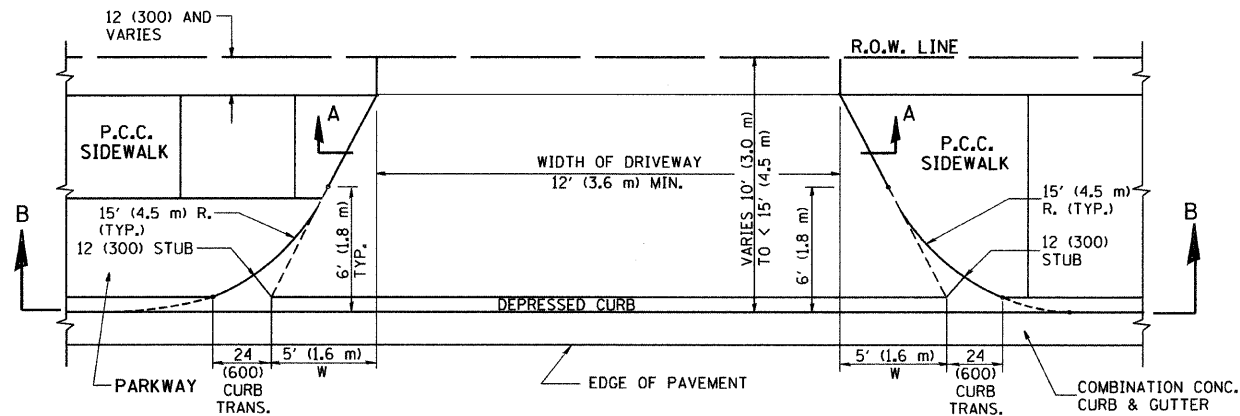
1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

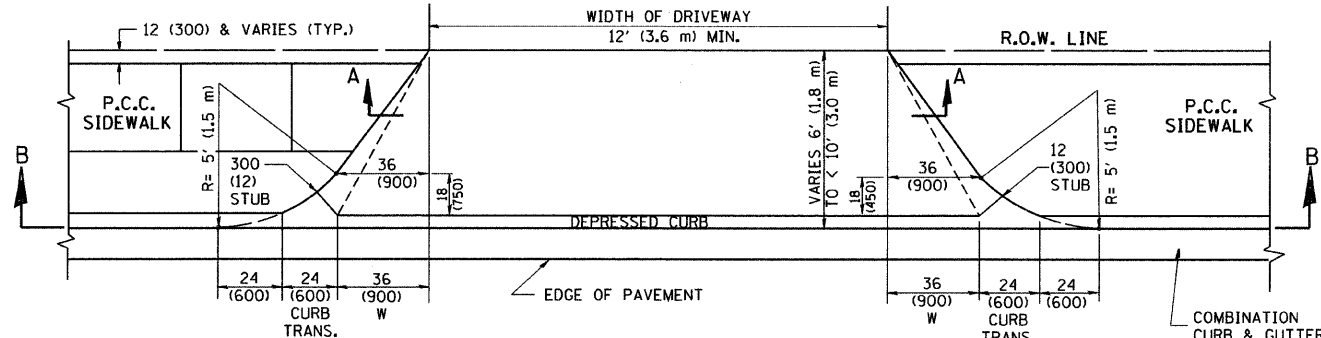
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		DRAWN -	REVISED - P. LGFLUER 04-15-03
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - R. BORO 01-01-07
	PLOT DATE = 6/11/2008	DATE - 11-04-95	REVISED - R. BORO 06-11-08

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

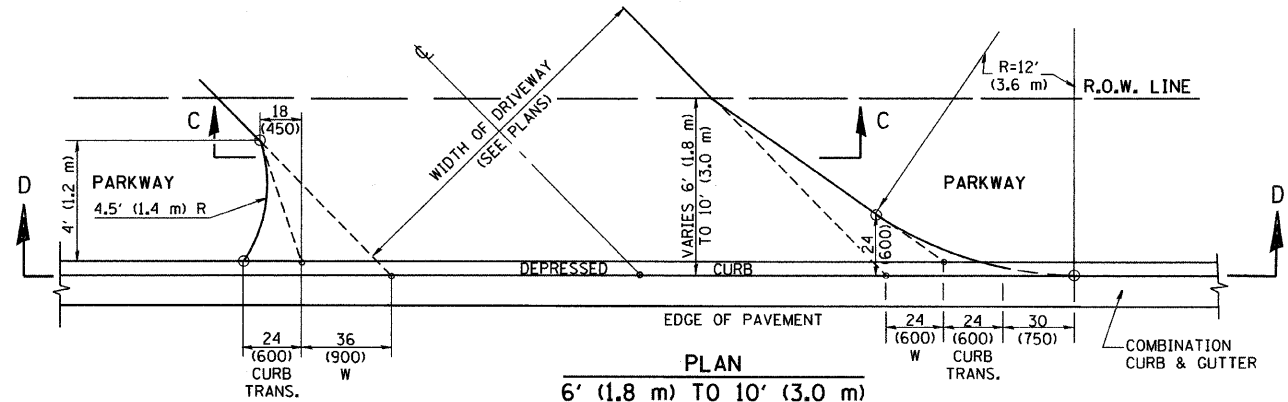
DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NONE		2734	09-00150-00-RS	COOK	23	12
SHEET NO. 1 OF 1 SHEETS		BD0156-07 (BD-01)		CONTRACT NO. 6336Z		
STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



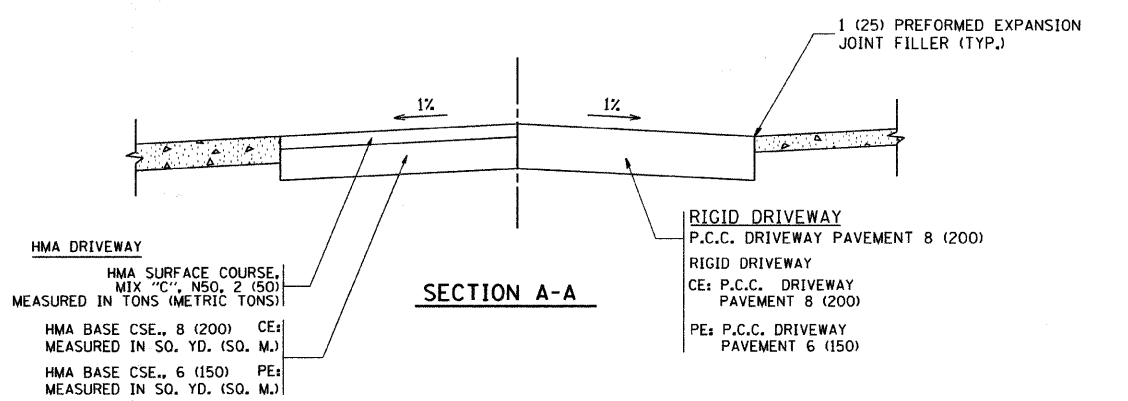
PLAN
10' (3.0 m) TO < 15' (4.5 m)



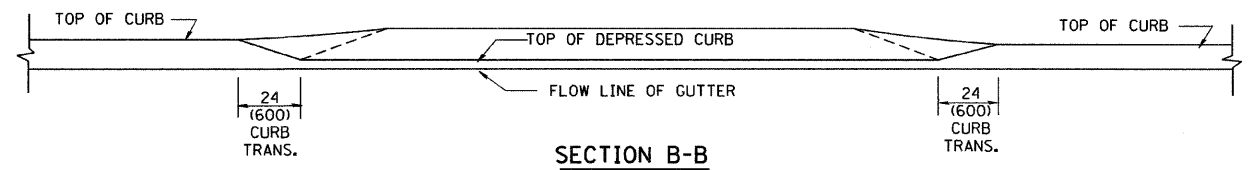
PLAN
6' (1.8 m) TO < 10' (3.0 m)



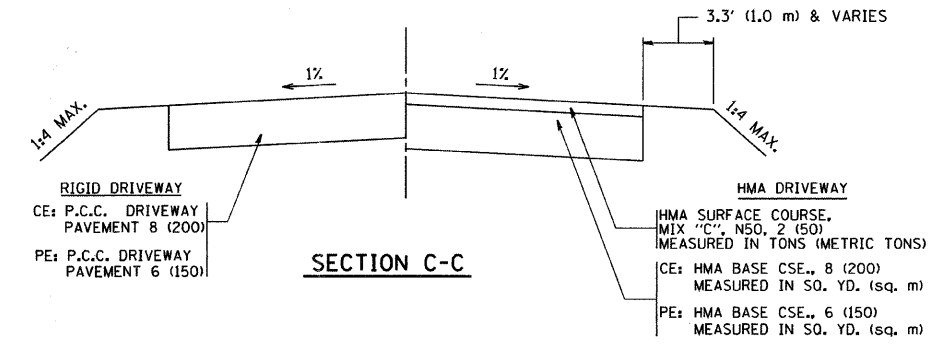
PLAN
6' (1.8 m) TO 10' (3.0 m)



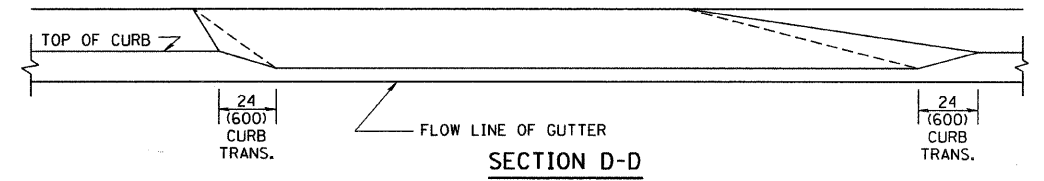
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

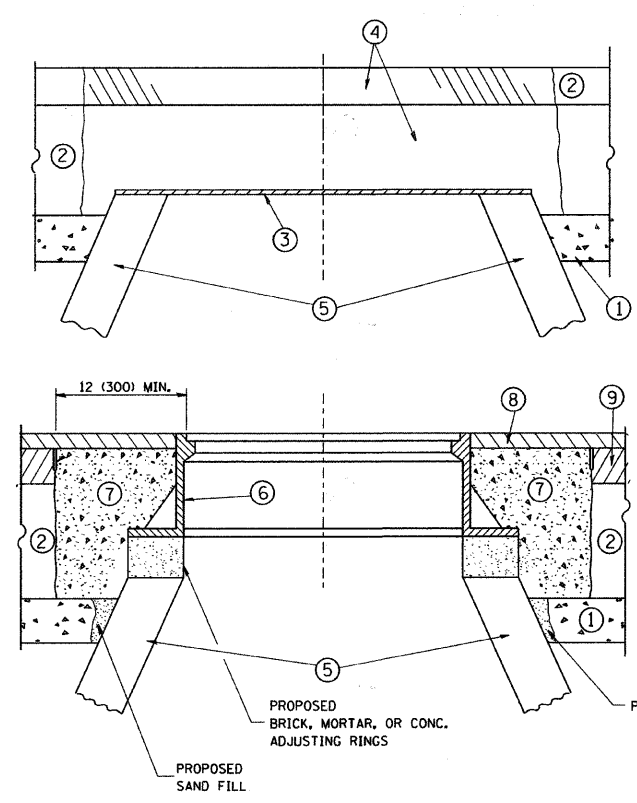
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USER NAME = goglianobt	DESIGNED - R. SHAH	REVISED - T. HOLTZ 04-08-97
PLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED - M. GOMEZ 04-06-01
PLOT DATE = 1/4/2008	CHECKED -	REVISED - P. LoFLEUR 04-15-03
	DATE - 11-06-95	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS	
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2734	09-00150-00-RS	COOK	23	13
BD400-02 (BD-02)			CONTRACT NO. 6362	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
 - B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
 - C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
 - D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.
- STAGE 2 (AFTER PAVEMENT MILLING)**
- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL" NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

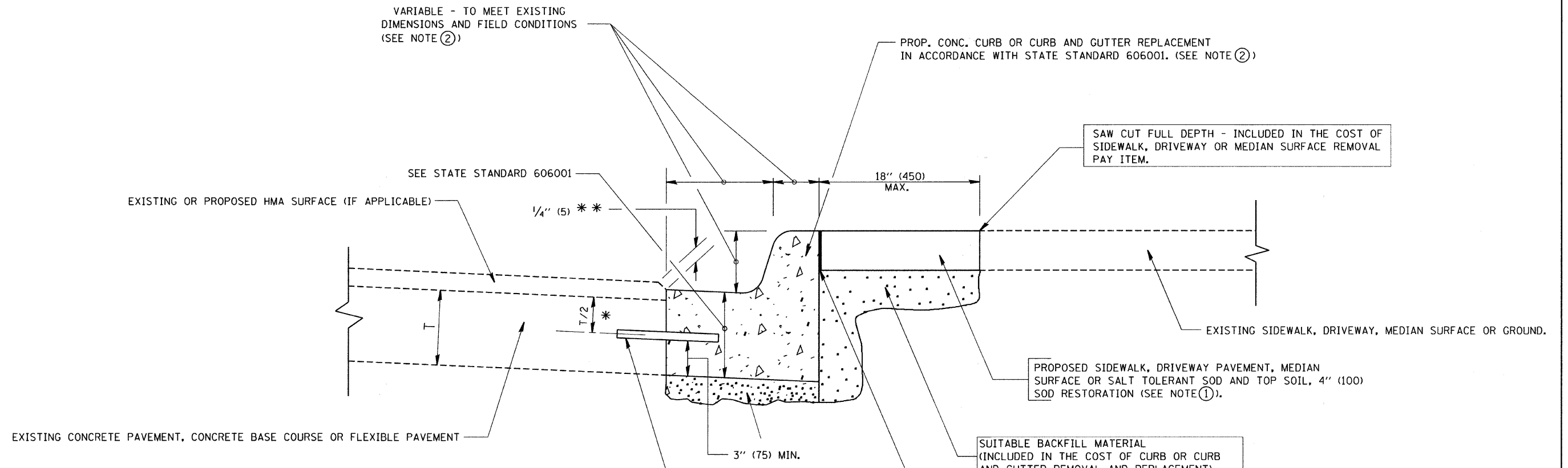
THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME = W:\dststd\22x34\bd88.dgn	USER NAME = gaglianobt	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING		F.A. RTE. = 2734	SECTION = 09-00150-00-RS	COUNTY = COOK	TOTAL SHEETS = 23	SHEET NO. = 14	
		DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	BD600-03 (BD-8)		CONTRACT NO. 6362		
		PLOT SCALE = 50.0000' / IN.	REVISED - R. WIEDEMAN 05-14-04		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
		PLOT DATE = 1/4/2008	REVISED - R. BORO 01-01-07									



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

- ② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

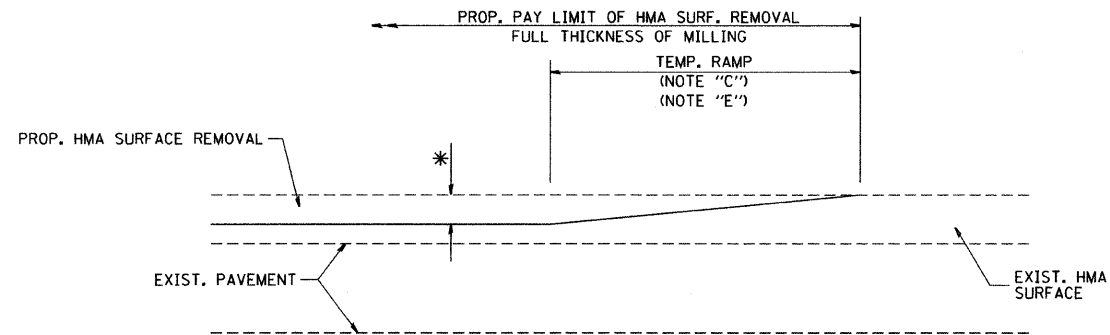
BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

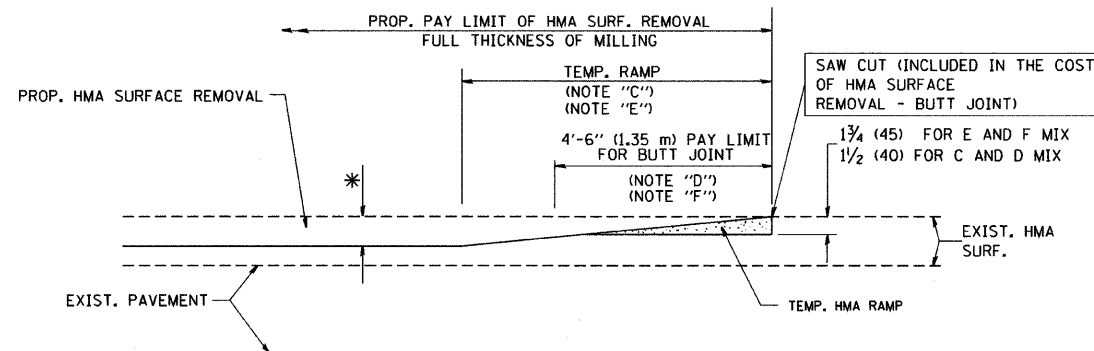
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	2734	09-00150-00-RS	COOK	23	15
		PLOT SCALE = 5/8" = 1' IN.	REVISED - M. GOMEZ 01-22-01					BD600-06 (BD-24)		CONTRACT NO. 63362		
		PLOT DATE = 1/4/2008	REVISED - R. BORO 01-01-07					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

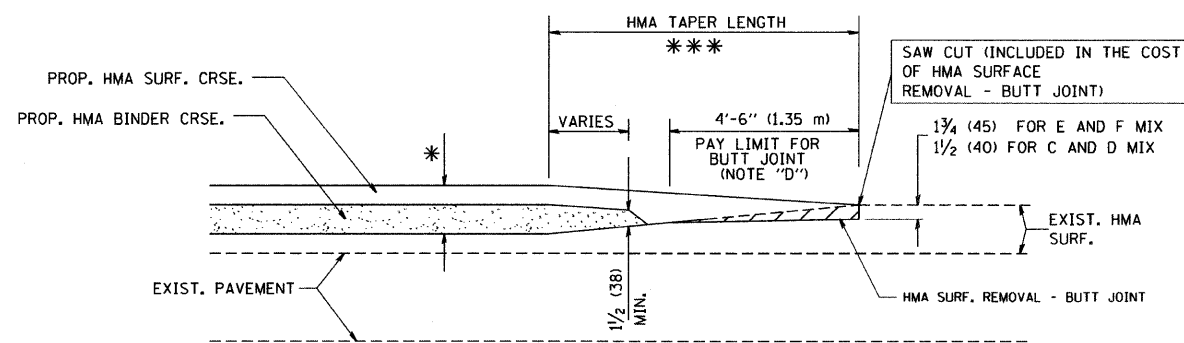
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

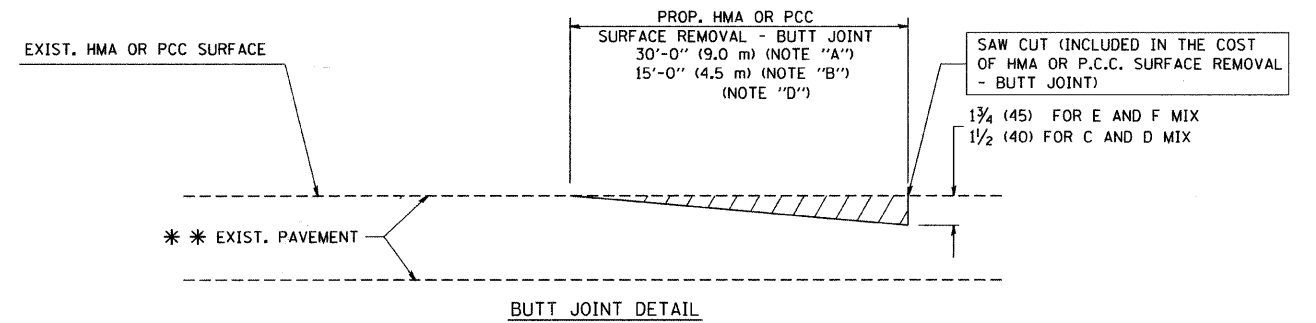
OPTION 2

TYPICAL TEMPORARY RAMP

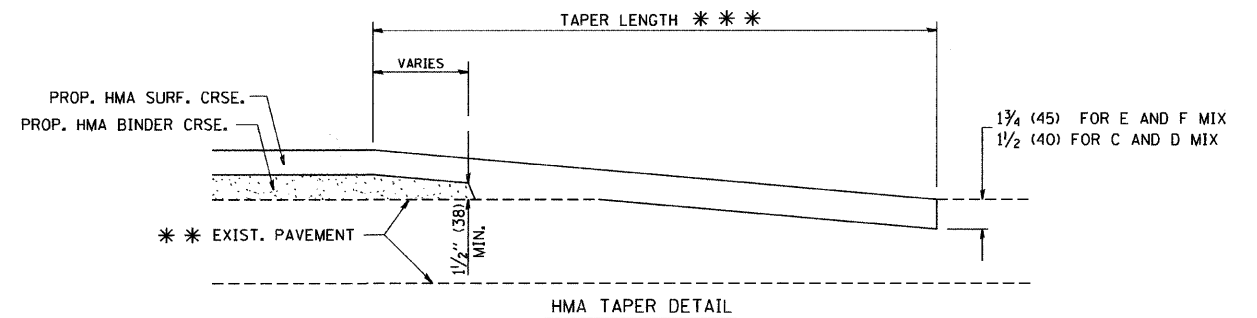


BUTT JOINT AND
HMA TAPER

**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

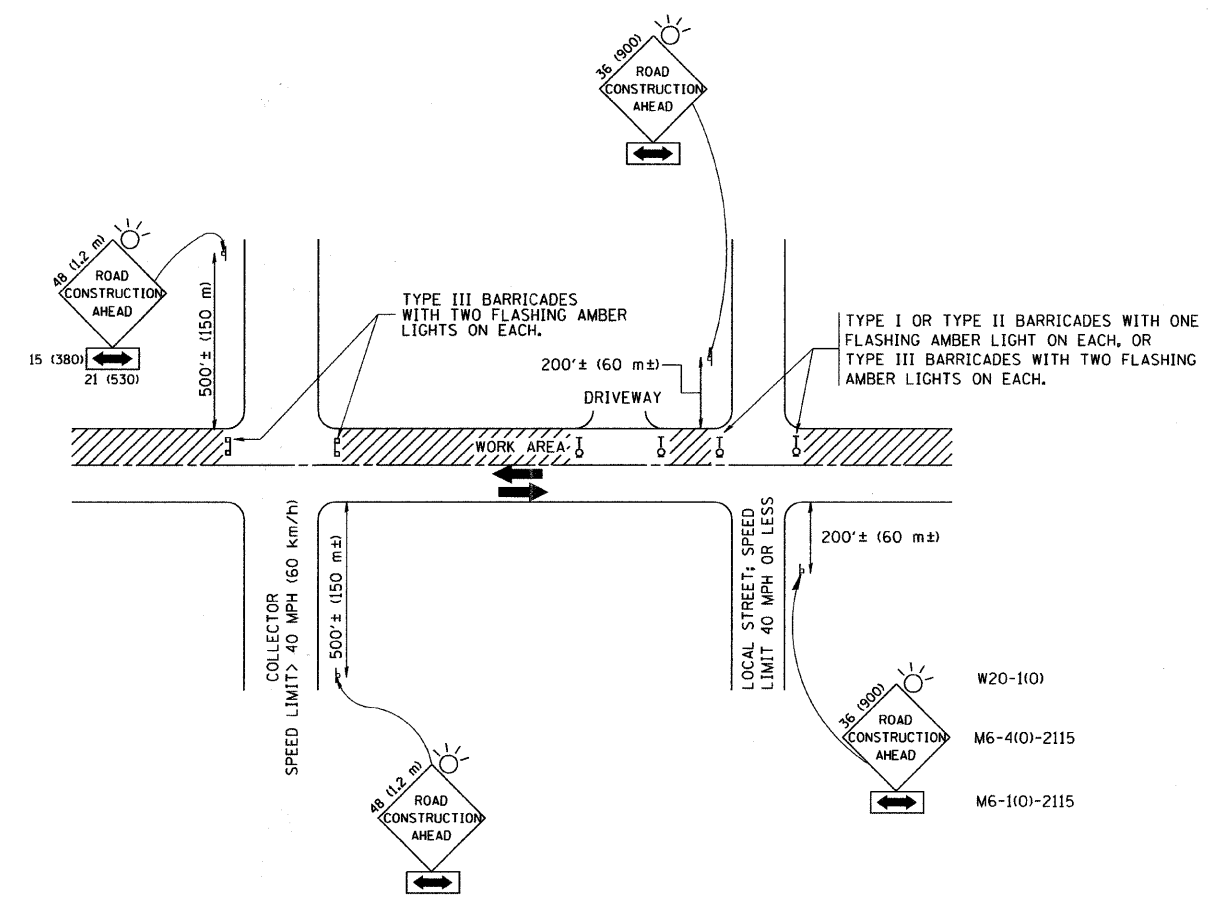
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PLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT DATE = 1/4/2008	CHECKED -	REVISED - M. GOMEZ 04-06-01
	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT AND HMA TAPER DETAILS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE. 2734	SECTION 09-00150-00-RS	COUNTY COOK	TOTAL SHEETS 23	SHEET NO. 16
BD400-05 BD32			CONTRACT NO. 63362	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 70150L, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

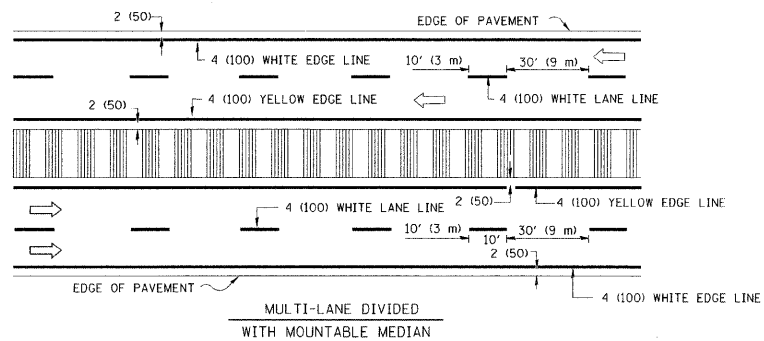
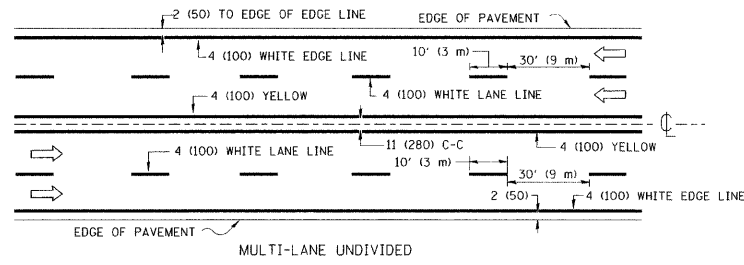
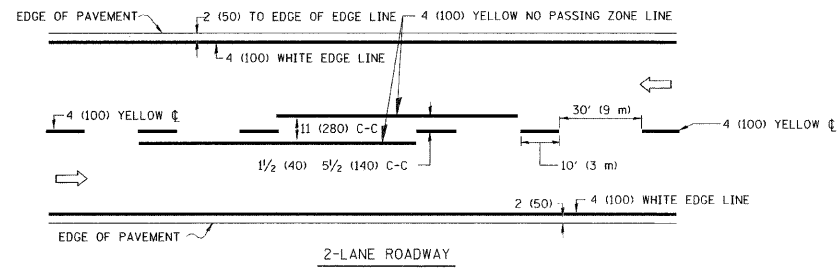
All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME = W:\diststa\22x34\ta10.dgn	USER NAME = gaglianobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
		PLOT SCALE = 50.000 / IN.	REVISED - A. HOUSEH 10-15-96
		PLOT DATE = 1/4/2008	REVISED - T. RAMMACHER 01-06-00
		CHECKED -	
		DATE - 06-89	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

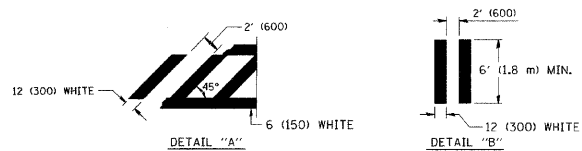
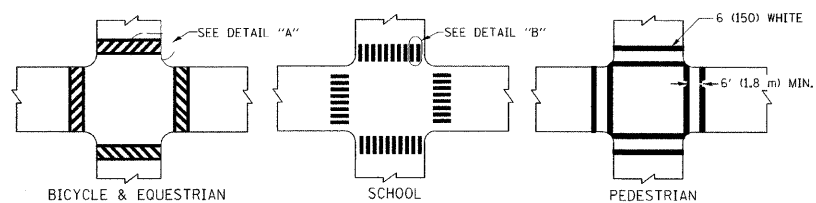
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.:	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2734	09-00150-00-RS	COOK	23	17
TC-10			CONTRACT NO. 63362	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

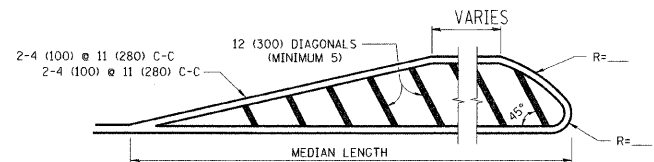
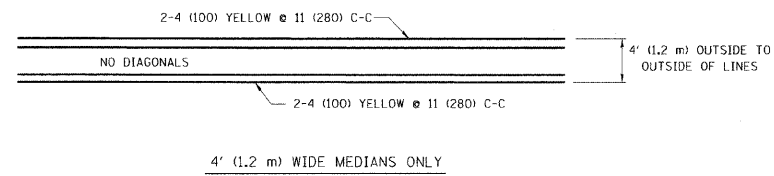


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

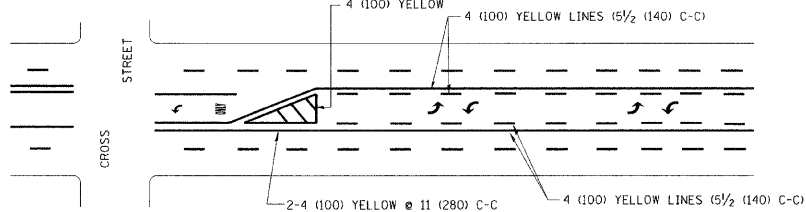


TYPICAL CROSSWALK MARKING

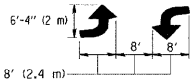


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

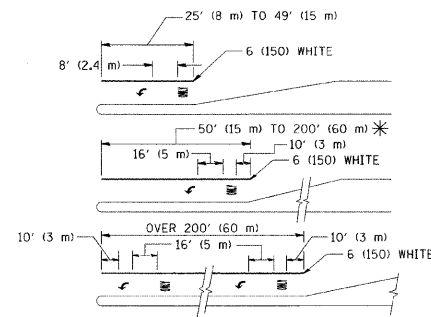


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

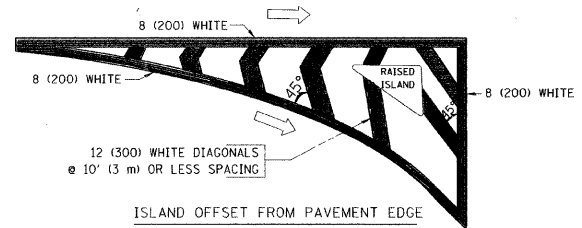


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

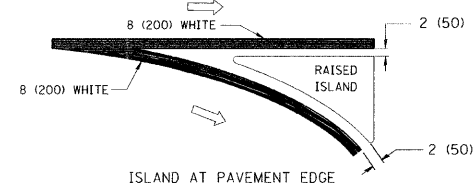
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE
FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100)	SKIP-DASH	WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
ON FREEWAYS	5 (125)	SKIP-DASH	WHITE	
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN)	2 @ 6 (150)	SOLID	WHITE	NOT LESS THAN 6' (1.8 m) APART
A. DIAGONALS (BIKE & EQUESTRIAN)	12 (300) @ 45°	SOLID	WHITE	2' (600) APART
B. LONGITUDINAL BARS (SCHOOL)	12 (300) @ 90°	SOLID	WHITE	2' (600) APART
				SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "X"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

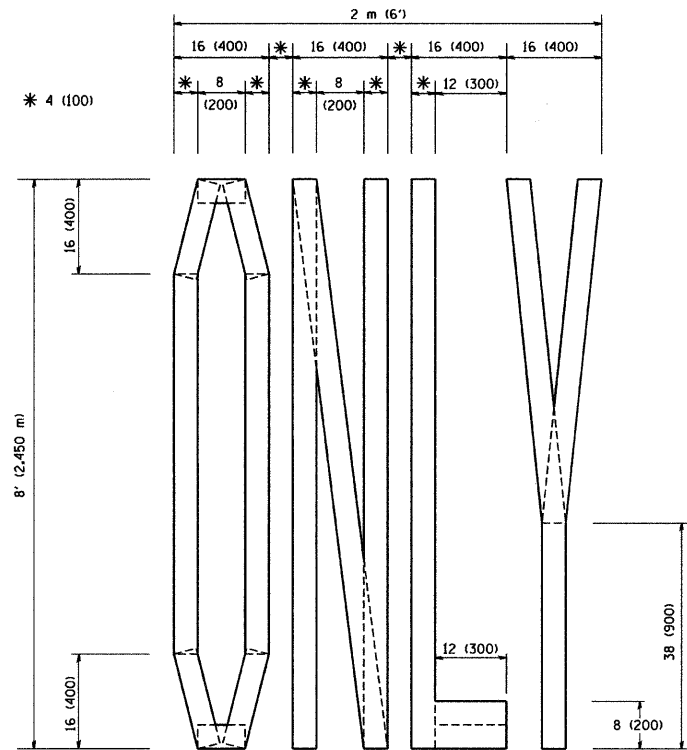
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = drivakosgn	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
ct:\pwork\pwork\drivakosgn\d0188315\3.dgn		DRAWN -	REVISED - C. JUCIUS 09-09-09
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -

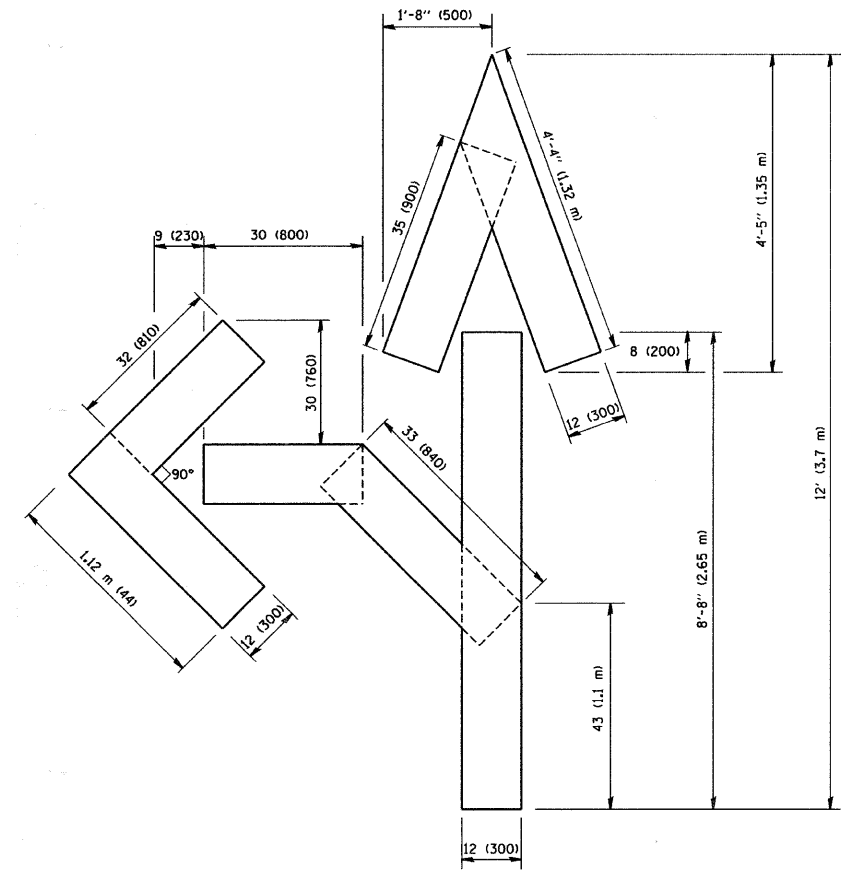
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

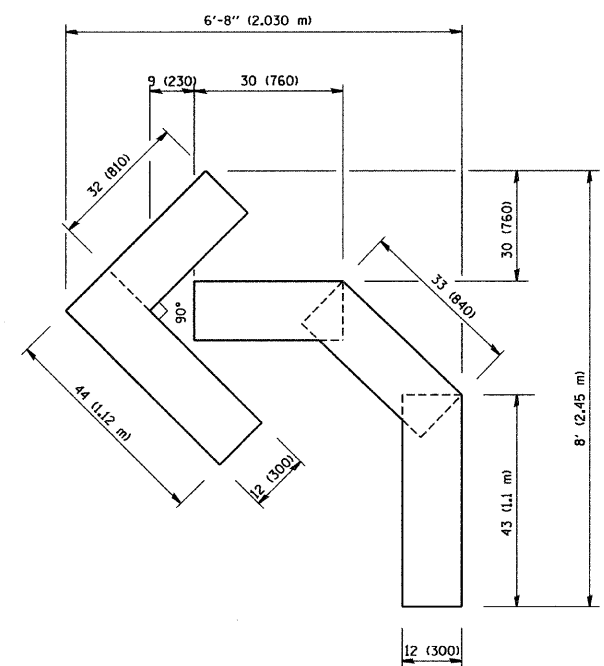
F.A. RTE. 2734	SECTION 09-00150-00-RS	COUNTY COOK	TOTAL SHEETS 23	SHEET NO. 19
TC-13		CONTRACT NO. 63362		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



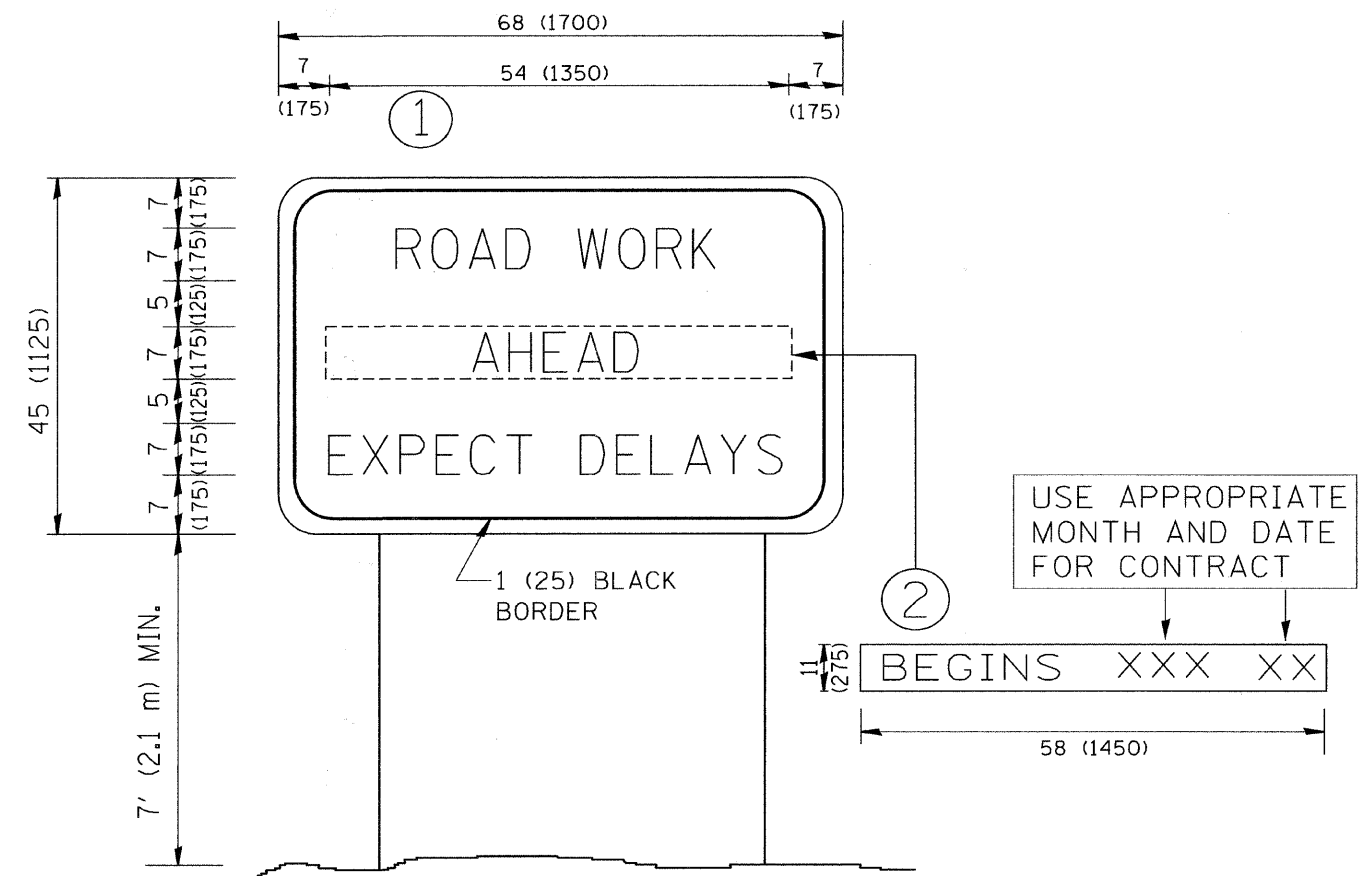
QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\diststd\22x34\to16.dgn	USER NAME = gogliano	DESIGNED -	REVISED - T. RAMMACHER 06-05-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING	F.A. RTE. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50,0000 / IN.	CHECKED -	REVISED - T. RAMMACHER 11-04-97			2734	09-00150-00-RS	COOK	23	20
	PLOT DATE = 1/4/2008	DATE = 09-18-94	REVISED - E. GOMEZ 08-28-00			TC-16		CONTRACT NO. 633/2		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



NOTES:

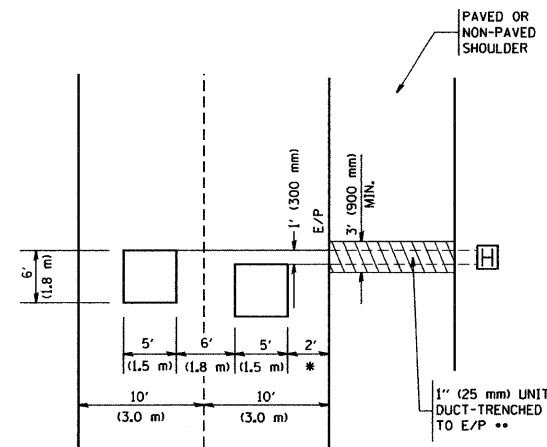
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\dststd\22x34\to22.dgn	USER NAME = gaglianobt	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN	F.A. RTE. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - R. MIRS 12-11-97			2734	09-00150-00-RS	COOK	23	21
		PLOT SCALE = 50.000 / IN.	REVISED - T. RAMMACHER 02-02-99			TC-22		CONTRACT NO. 63362		
		PLOT DATE = 1/4/2008	REVISED - C. JUCIUS 01-31-07			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



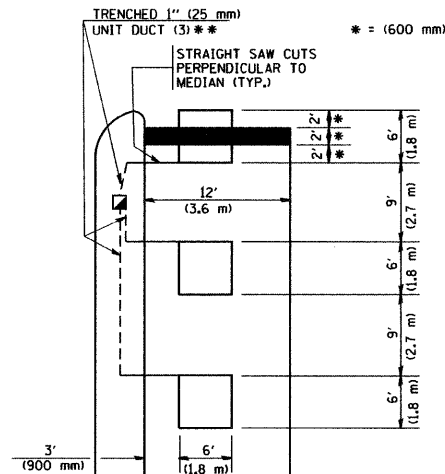
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

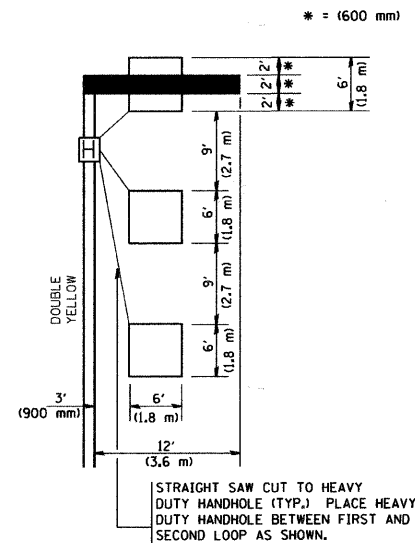


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

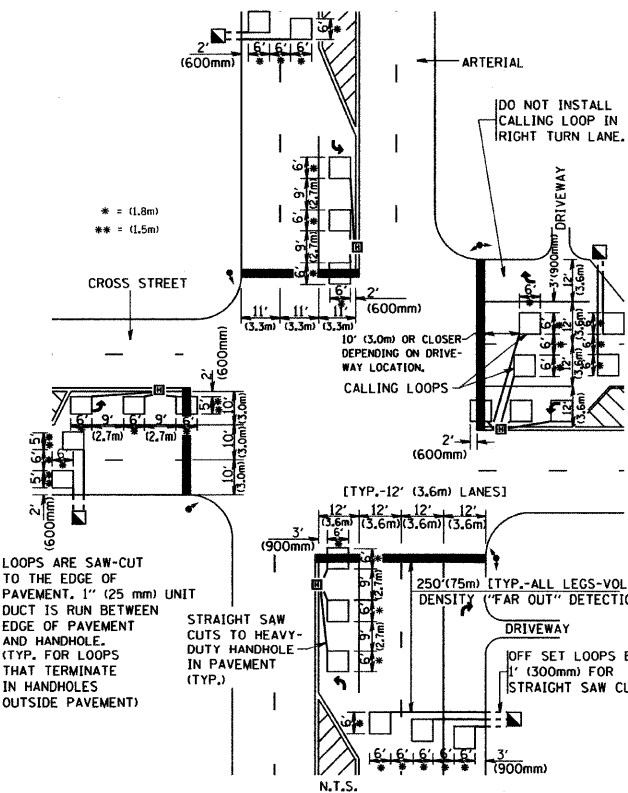
**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)



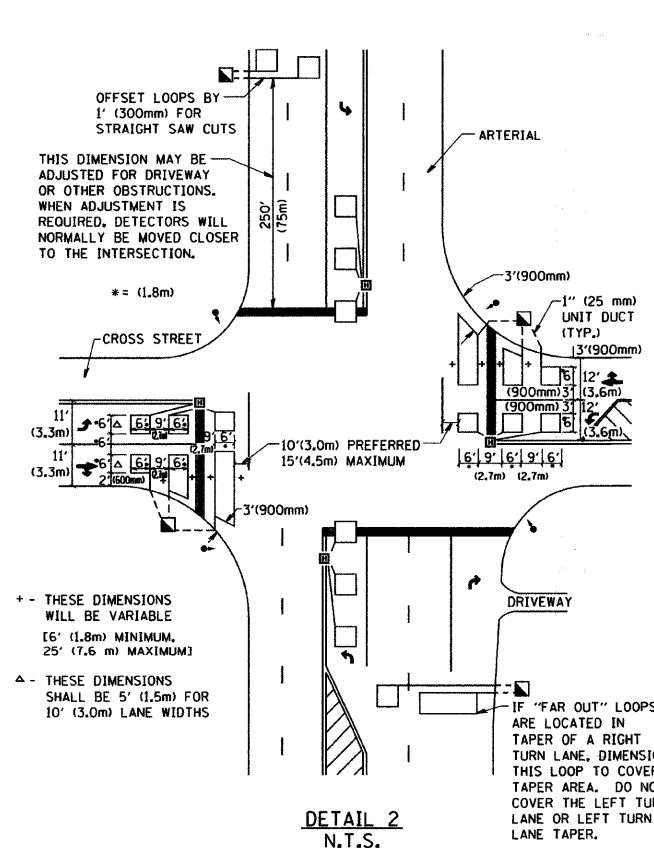
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = W:\dststd\22x34\ts07.dgn

USER NAME = goglianobt
PLOT SCALE = 50,0000 / IN.
PLOT DATE = 1/4/2008

DESIGNED -
DRAWN -
CHECKED - R.K.F.
DATE -

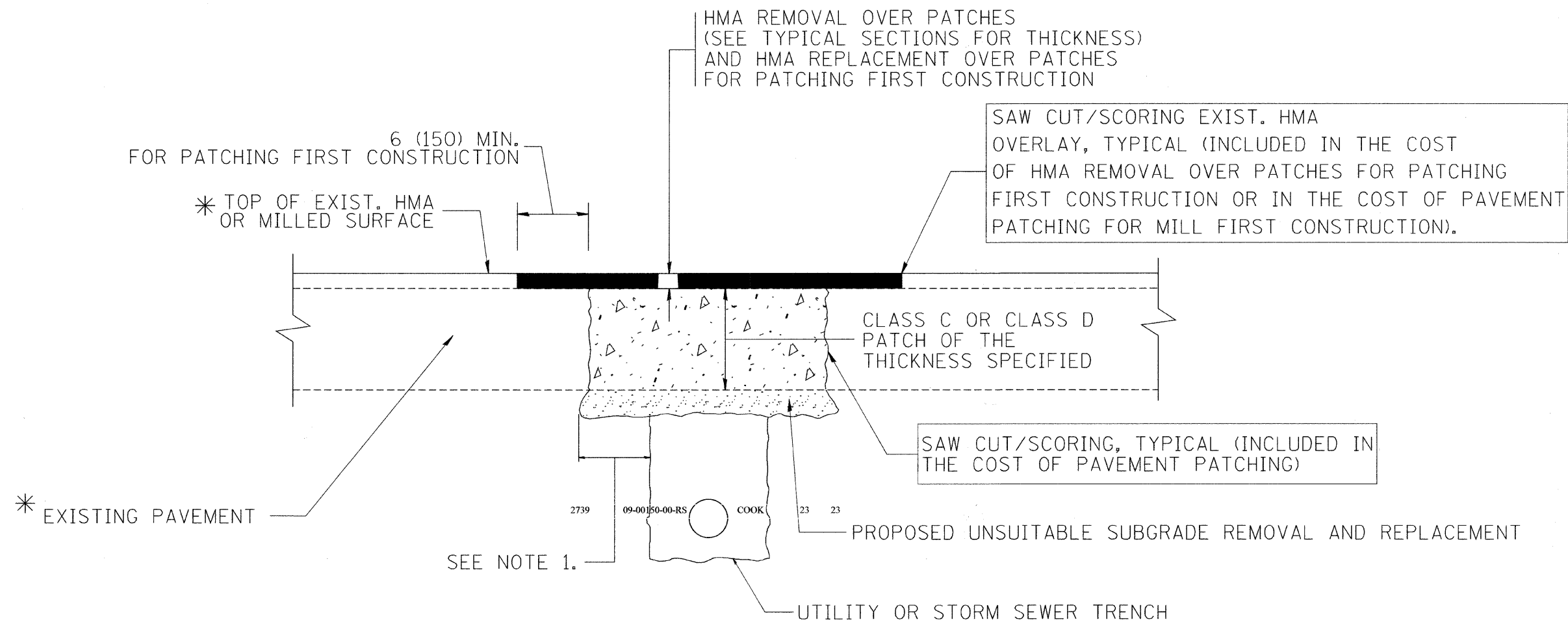
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2734	09-00150-00-RS	COOK	23	22
TS-07			CONTRACT NO. 63362	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\diststd22x34\bd22.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A. RTE. 2734	SECTION 09-00150-00-RS	COUNTY COOK	TOTAL SHEETS 23	SHEET NO. 23
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	BD400-04 (BD-22)		CONTRACT NO. 63362		
	PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - K. ENG 10-27-08		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							