

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
158	09-00026-00-RS	LAKE	8	1
		ILLINOIS	CONTRACT NO. 63364	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

FAU 0158 (DEERPATH DRIVE)
ILLINOIS ROUTE 132 (GRAND AVE) TO SAND LAKE RD
LAPP RESURFACING
SECTION 09-00026-00-RS
PROJECT NO. ARA-9003 (458)
LAKE COUNTY
C-91-892-09

INDEX OF SHEETS

- 1 COVER SHEET
- 2 GENERAL NOTES, HIGHWAY STANDARDS AND SUMMARY OF QUANTITIES
- 3 TYPICAL SECTION
- 4 PLAN - STA. 100+00 TO STA. 106+00
- 5 PLAN - STA. 106+00 TO STA. 115+00
- 6 PLAN - STA. 115+00 TO STA. 120+00
- 7 PLAN - STA. 120+00 TO STA. 128+71
- 8 STANDARD DETAILS

TRAFFIC DATA

ADT: 175 VPD(2007)

POSTED SPEED

20 MPH (EXISTING)
20 MPH (PROPOSED)

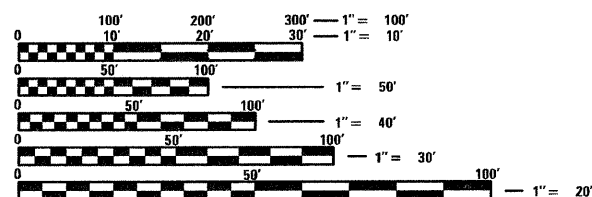
DESIGN SPEED

25 MPH (EXISTING)
25 MPH (PROPOSED)

DESIGN DESIGNATION: LOCAL

HIGHWAY STANDARDS

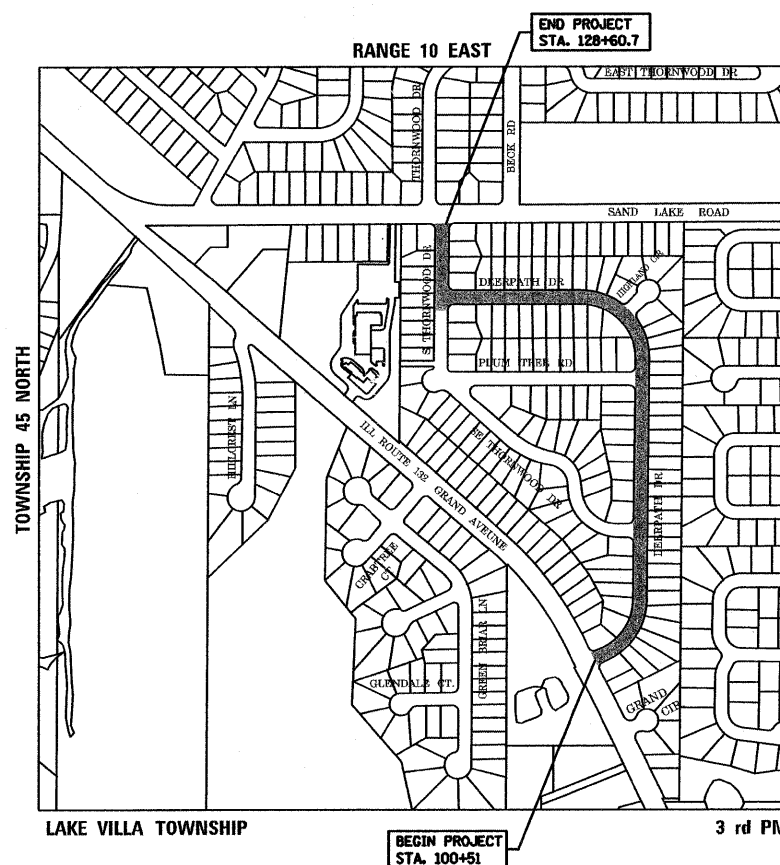
- 000001-05 STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
- 442201-03 CLASS C AND D PATCHES
- 542401-01 METAL END SECTION FOR PIPE CULVERTS
- 701301-03 LANE CLOSURE, 2L 2W, SHORT TIME OPERATIONS
- 701311-03 LANE CLOSURE, 2L 2W, MOVING OPERATIONS-DAY ONLY
- 701501-05 URBAN LANE CLOSURE, 2L 2W, UNDIVIDED
- 701901-01 TRAFFIC CONTROL DEVICES



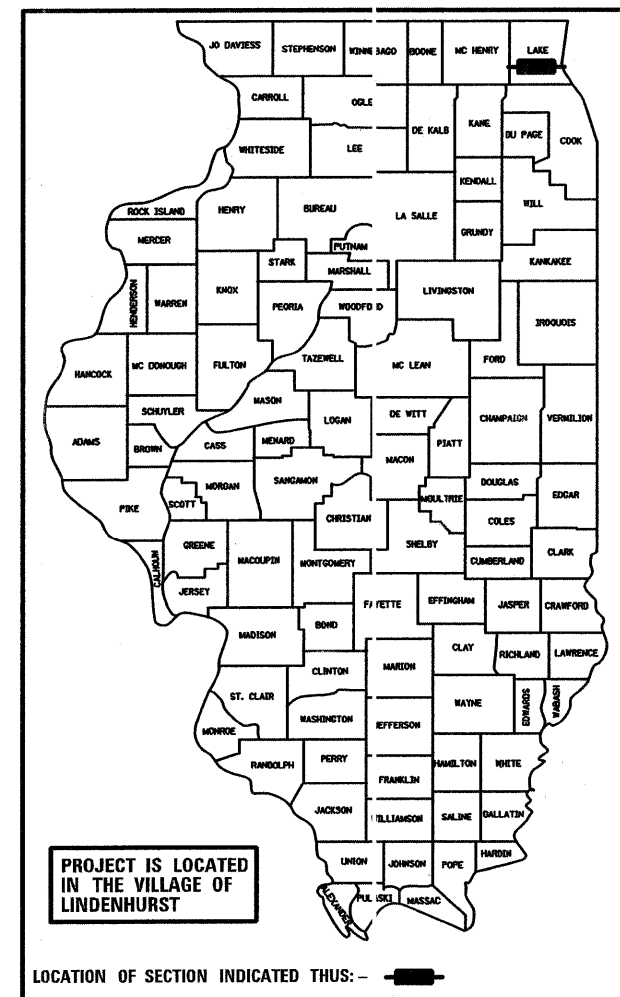
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER : JOHN BRIGGS, P.E.
PROJECT MANAGER : SCOTT REISINGER
PROJECT NUMBER : ARA-9003(458)
CONTRACT NUMBER : 63364



GROSS LENGTH = 2809.7 FT. = 0.532 MILE
NET LENGTH = 2809.7 FT. = 0.532 MILE



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED October 15, 2009
Wayne D. Walsh
DIRECTOR OF PUBLIC WORKS, VILLAGE OF LINDENHURST

PASSED OCTOBER 26, 2009
John R. Briggs
DISTRICT ONE ENGINEER OF LOCAL ROADS AND STREETS
RELEASING FOR BID
BASED ON LIMITED REVIEW OCTOBER 29, 2009
Diana M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER

PROFESSIONAL ENGINEER'S SIGN & SEAL
John R. Briggs 10/15/09
JOHN R. BRIGGS, P.E.
EXPIRES: 11-30-11
SEAL

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

GENERAL NOTES

- ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE STATE OF ILLINOIS "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION," LATEST ADDITION, THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", LATEST ADDITION, DETAILS IN THESE PLANS, AND THE CONTRACT DOCUMENTS.
- THE CONTRACTOR IS TO NOTIFY ALL UTILITY COMPANIES AT LEAST 48 HOURS PRIOR TO ANY CONSTRUCTION. THE JULIE TELEPHONE NUMBER FOR THIS AREA IS (800)892-0123. THE CONTRACTOR WILL NOTIFY THE OWNER AND ENGINEER AT LEAST 2 WORKING DAYS IN ADVANCE OF STARTING.
- THE CONTRACTOR SHALL INDEMNIFY VILLAGE OF LINDENHURST, BONESTROO, THEIR AGENTS, ETC. FROM ALL LIABILITY INVOLVED WITH THE CONSTRUCTION, INSTALLATION AND TESTING OF WORK ON THIS PROJECT. THE CONTRACTOR SHALL TAKE OUT AND MAINTAIN DURING THE LIFE OF THIS CONTRACT, COMPREHENSIVE BODILY INJURY LIABILITY AND PROPERTY DAMAGE INSURANCE AS STIPULATED IN FORM LR107-4. VILLAGE OF LINDENHURST, AND BONESTROO SHALL BE NAMED AS ADDITIONAL INSUREDS ON THE CONTRACTOR'S COMPREHENSIVE LIABILITY POLICY.
- THE CONTRACTOR SHALL PLAN THEIR WORK SO THAT THE STREET AND DRIVENWAYS ARE COMPLETELY BACKFILLED AND ACCESSIBLE TO TRAFFIC AT THE END OF EACH WORKDAY. PARTIAL CROSSINGS WILL NOT BE PERMITTED.
- ALL EXCAVATED MATERIAL SHALL BE REMOVED FROM THE PROJECT SITE AT THE END OF EACH WORKING DAY. EXCAVATED MATERIAL SHALL NOT BE STOCKPILED ON PROJECT SITE DURING NON-WORKING HOURS.
- THE CONTRACTOR SHALL SUBMIT, IN WRITING FOR APPROVAL BY THE ENGINEER, A LIST OF ALL MATERIALS AND MATERIAL CERTIFICATIONS PROPOSED TO BE USED PRIOR TO ORDERING OR DELIVERY.
- THE ENGINEER MAY ORDER TESTS OF ANY MATERIALS USED IN THE CONSTRUCTION. THE MATERIALS TO BE TESTED MAY BE TAKEN FROM THE SITE.
- IN ACCORDANCE WITH VILLAGE NPDES STORM WATER ORDINANCE, THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ADEQUATE EROSION PROTECTION DURING CONSTRUCTION AS WELL AS PROVIDING ADEQUATE PROTECTION TO ADJOINING STREETS FROM MUD AND POLLUTED RUNOFF AS WELL AS KEEPING EXISTING PAVEMENT CLEAN OF MUD AND DEBRIS. ALL EROSION CONTROL MEASURES TO BE APPROVED BY THE ENGINEER.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR INTERPRETING ALL SUBSURFACE INFORMATION REQUIRED FOR THIS PROJECT. COPIES OF PAVEMENT CORES ARE AVAILABLE FROM THE ENGINEER.
- DUST CONTROL WATERING SHALL BE REQUIRED AT THE DIRECTION OF THE ENGINEER. USE OF VILLAGE WATER WILL BE AVAILABLE FROM THE VILLAGE OF LINDENHURST AND CHARGED AT THE CURRENT RATE OF BILLINGS. THE CONTRACTOR SHALL NOTIFY THE PUBLIC WORKS DEPARTMENT 48 HOURS PRIOR TO THE NEED FOR WATER WITHDRAWAL FROM THE SYSTEM. AT NO TIME WILL THE CONTRACTOR BE ALLOWED TO OPERATE VALVES, HYDRANTS OR ANY OTHER PART OF THE VILLAGE WATER SYSTEM. THE USE OF TANKER TRUCKS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. ALL EQUIPMENT, LABOR, MATERIALS, ETC., REQUIRED SHALL BE PAID FOR AT THE UNIT PRICE FOR DUST CONTROL WATERING OR SUPPLEMENTAL WATERING, AS MEASURED IN UNITS OF 1,000 GALLONS EACH.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND MAINTENANCE OF ADEQUATE SIGNS, TRAFFIC CONTROL DEVICES, AND WARNING DEVICES TO INFORM AND PROTECT THE PUBLIC DURING ALL PHASES OF CONSTRUCTION. BARRICADES AND WARNING SIGNS SHALL BE PROVIDED IN ACCORDANCE WITH THE FHWA "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES". ADEQUATE LIGHTING SHALL BE MAINTAINED FROM DUSK TO DAWN AT ALL LOCATIONS WHERE CONSTRUCTION OPERATIONS WARRANT, OR AS DESIGNATED BY THE VILLAGE. ALL TRAFFIC CONTROL WORK SHALL BE DONE IN ACCORDANCE WITH THE I.D.O.T. "HIGHWAY STANDARDS" AS SHOWN ON THE PLANS.
- ALL TRENCHES CAUSED BY THIS CONSTRUCTION AROUND, UNDER AND/OR NEAR ANY PAVED SURFACE, STREETS, SIDEWALKS, DRIVEWAY, ETC. SHALL BE BACKFILLED WITH DESIGNATED MATERIAL, AND SHALL BE MECHANICALLY COMPACTED IN PLACE TO NINETY-FIVE PERCENT (95%) OF MAXIMUM DENSITY AT OPTIMUM MOISTURE AS DETERMINED BY THE STANDARD PROCTOR TEST. PAVEMENT, SIDE WALK, CURB AND GUTTERS, OR ANY OTHER RIGID SURFACE, SHALL BE BACKFILLED WITH DESIGNATED MATERIAL. WHERE THE EDGE OF THE TRENCH IS MORE THAN 2 FEET FROM THE EDGE OF AN EXISTING OR PROPOSED PAVED SURFACE TRENCH BACKFILL SHALL BE USED BELOW THE INTERSECTION OF ONE TO ONE (1:1) SLOPE LINE DRAWN FROM THE EDGE OF PAVEMENT TO THE NEAREST VERTICAL EDGE OF THE TRENCH.
- BEFORE PLACEMENT OF ANY BASE COURSE MATERIALS, THE CONTRACTOR SHALL TREAT THE SUBGRADE AS PER ARTICLE 301.03 OF THE STANDARD SPECIFICATIONS.
- AGGREGATE BASE COURSE, AGGREGATE FOR DRIVES OR TEMPORARY ACCESS SHALL BE CA-6, GRADE 8, CRUSHED LIMESTONE.
- THE CONTRACTOR SHALL REMOVE EXISTING BRICK PAVERS IN DRIVEWAY APRONS ONLY AS REQUIRED, RE-GRADE THE DRIVEWAY PROFILE AND REPLACE THE BRICK PAVERS USING THE SAME PATTERN THAT IS EXISTING. SUBGRADE PREPARATION AND MATERIALS SHALL BE GRANULAR, CONSISTENT WITH THE EXISTING MATERIALS. PAYMENT SHALL BE AT THE UNIT PRICE PER SQUARE FOOT FOR BRICK DRIVEWAY REMOVAL AND REPLACEMENT.
- CONTRACTOR SHALL COMPACT/ HAND TAMP ALL BITUMINOUS DRIVEWAY EDGES TO PROVIDE A UNIFORM, FINISHED EDGE AS PART OF DRIVEWAY REPLACEMENT.
- ALL MANHOLE STRUCTURES, VALVES, INLETS, ETC. SHALL BE MAINTAINED IN AN ACCESSABLE CONDITION AT ALL TIMES. NO PLATING OF MANHOLES WILL BE ALLOWED DURING CONSTRUCTION OPERATIONS.

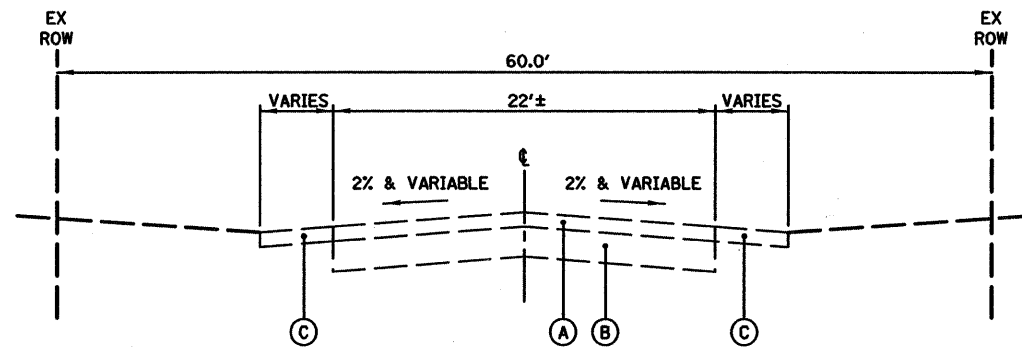
GENERAL NOTES (ITEMS NOT PAID FOR)

- THE CONTRACTOR WILL BE REQUIRED TO REPLACE, IN KIND, ALL SURFACE FEATURES DAMAGED OR REMOVED INCLUDING, BUT NOT LIMITED TO: STREET PAVING, SIDEWALKS, DRIVES, LAWNS, TREES, FIRE HYDRANTS, PROPERTY PINS, DRAIN TILES, ETC. THE WORK SHALL BE INCLUDED IN THE COST OF CONSTRUCTION AND NO EXTRA COMPENSATION WILL BE PERMITTED, UNLESS SPECIFIC PAY ITEMS ARE ESTABLISHED.
- DURING THE COURSE OF THE WORK, THE CONTRACTOR SHALL INSURE THAT AT THE CONCLUSION OF EACH DAY'S OPERATION, POSITIVE DRAINAGE IS ACHIEVED. SITE DRAINAGE MAY BE ACHIEVED BY DITCHING, GRADING OR ANY OTHER METHOD AGREEABLE TO THE ENGINEER. THE CONTRACTOR'S FAILURE TO PROVIDE ADEQUATE DRAINAGE SHALL CONSTITUTE NO SUBSEQUENT CLAIM FOR ADDED COMPENSATION DUE TO DELAYS OR UNSUITABLE MATERIALS RESULTING THEREOF.
- UPON COMPLETION OF THE WORK, INCLUDING ANY RE-WORK, AND BEFORE ACCEPTANCE THEREOF, THE CONTRACTOR SHALL THOROUGHLY CLEAN THE AREAS OF HIS OPERATIONS AND REMOVE ALL SURPLUS CONSTRUCTION MATERIAL AND DEBRIS RESULTING FROM HIS WORK. CLEAN-UP SHALL BE INCLUDED IN THE COST OF THE CONTRACT.
- ALL STRUCTURES (MANHOLES, VALVE VAULTS, AUXILIARY VALVE BOXES, ETC) SHALL BE CLEANED OUT AT THE END OF THE PROJECT AND PRIOR TO ACCEPTANCE. STORM SEWER MAINS SHALL BE JET CLEANED IF EVIDENCE OF DEBRIS BUILD-UP IS PRESENT AT THE TIME OF ACCEPTANCE. WORK TO BE INCLUDED IN THE COST OF UTILITY CONSTRUCTION.
- THE RIM ELEVATIONS SHOWN ON THE PLANS ARE APPROXIMATE ONLY AND THE CONTRACTOR WILL ADJUST ALL STRUCTURES TO GRADE WHEN FINAL GRADING IS COMPLETED. PRECAST ADJUSTMENT RINGS SHALL BE LIMITED TO NO MORE THAN 8 INCHES. PRECAST BARREL SECTIONS WILL BE ADDED TO THE MANHOLES WHEN THE ADJUSTMENT REQUIRED EXCEEDS 8 INCHES. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THIS ADJUSTMENT.
- PAVEMENT SWEEPING SHALL BE PERFORMED A MINIMUM OF ONCE A WEEK, OR AT THE DIRECTION OF THE ENGINEER. ADDITIONAL DUST CONTROL MEASURES, SUCH AS CALCIUM CHLORIDE SHALL BE PROVIDED AS NEEDED. WORK SHALL BE INCLUDED IN THE COST OF CONSTRUCTION OPERATIONS.
- EXISTING SUMP PUMPS, FIELD TILES, ETC. DISCHARGING INTO EXISTING STORM SEWERS SHALL BE PROTECTED AND MODIFIED (IF NECESSARY) WHEN INSTALLING NEW STORM SEWERS AND SHALL BE CONNECTED TO NEW STRUCTURES. COST OF PROTECTION/MODIFICATION SHALL BE INCLUDED THE UNIT PRICE FOR STORM SEWERS UNLESS OTHERWISE SPECIFIED.
- ALL DAMAGE CAUSED TO VALVES, VALVE BOXES AND B-BOXES SHALL BE REPAIRED BY CONTRACTOR AT THEIR EXPENSE. DAMAGE TO WATER SERVICES BY CONTRACTOR SHALL BE REPAIRED IMMEDIATELY UPON DAMAGE BEING INCURRED.
- ALL DRAIN TILE AND/OR CULVERT REMOVAL SHALL INCLUDE REPLACEMENT WITH TRENCH BACKFILL, AS DESIGNATED IN THE SPECIFICATIONS, PAVEMENT RESTORATION, ETC. DRAIN TILE/CULVERT REMOVAL AND REPLACEMENT SHALL BE INCLUDED IN THE COST OF THE CONTRACT PRICE UNLESS IT IS OTHERWISE DESIGNATED IN THE PLANS AND IN THE SCHEDULE OF PRICES.

SUMMARY OF QUANTITIES

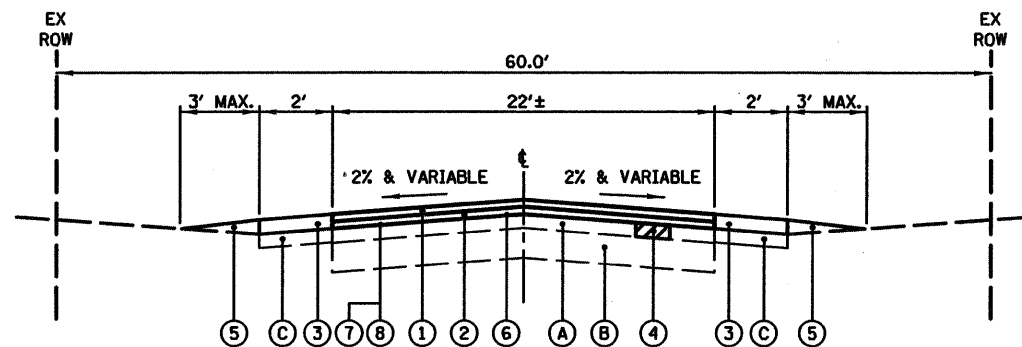
Pay Code	Item Description	Total	Units	2000 Participating	2007 Non-Participating
20800150	TRENCH BACKFILL	4	CU YD	4	
21101615	TOPSOIL FURNISH AND PLACE, 4"	1450	SQ YD	1450	
SP ** 21400100	GRADING AND SHAPING DITCHES	100	FOOT		100
25000100	SEEDING, CLASS I	0.30	ACRE	0.30	
25000400	NITROGEN FERTILIZER NUTRIENT	27	POUND	27	
25000500	PHOSPHORUS FERTILIZER NUTRIENT	27	POUND	27	
25000600	POTASSIUM FERTILIZER NUTRIENT	27	POUND	27	
25100630	EROSION CONTROL BLANKET	1450	SQ YD	1450	
* 25200200	SUPPLEMENTAL WATERING	5	UNIT	5	
40201000	AGGREGATE FOR TEMPORARY ACCESS	60	TON	60	
40600100	BITUMINOUS MATERIALS (PRIME COAT)	1050	GALLON	1050	
40600300	AGGREGATE (PRIME COAT)	9	TON	9	
40600625	LEVELING BINDER (MACHINE METHOD), N50	425	TON	425	
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	150	SQ YD	150	
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	825	TON	825	
44000200	DRIVEWAY PAVEMENT REMOVAL	1500	SQ YD	1500	
44201717	CLASS D PATCHES, TYPE II, 6 INCH	70	SQ YD	70	
44201721	CLASS D PATCHES, TYPE III, 6 INCH	825	SQ YD	825	
44213200	SAW CUTS	1250	FOOT	1250	
SP ** 44300300	AREA REFLECTIVE CRACK CONTROL TREATMENT, SYSTEM A	6600	SQ YD	6600	
48101200	AGGREGATE SHOULDERS, TYPE B	190	TON	190	
SP ** 50105210	REMOVE EXISTING CULVERTS	100	FOOT		100
SP ** 54215547	METAL END SECTIONS 12"	8	EACH		8
542C0217	PIPE CULVERTS, CLASS C, TYPE 1 12"	80	FOOT		80
SP ** 56500600	DOMESTIC WATER SERVICE BOXES TO BE ADJUSTED	5	EACH	5	
60255500	MANHOLES TO BE ADJUSTED	4	EACH	4	
60266600	VALVE BOXES TO BE ADJUSTED	3	EACH	3	
67100100	MOBILIZATION	1	L SUM	1	
* 70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	1	L SUM	1	
SP ** X0321556	SANITARY MANHOLES TO BE ADJUSTED	10	EACH	10	
SP ** XX000406	BRICK PAVER REMOVAL AND REPLACEMENT	135	SQ FT	135	
SP ** XX000610	RELOCATE EXISTING MAILBOX	70	EACH		70
SP ** XX006806	HOT-MIX ASPHALT DRIVEWAY PAVEMENT	1500	SQ YD	1500	
SP ** Z0019600	DUST CONTROL WATERING	5	UNIT	5	

△ SPECIALTY ITEMS



- EXISTING LEGEND**
- (A) ASPHALT PAVEMENT, 5"-6"±
 - (B) AGGREGATE BASE
 - (C) AGGREGATE SHOULDER

- PROPOSED LEGEND**
- (1) HOT MIX ASPHALT SURFACE COURSE, MIX C, N50, 2" THICKNESS
 - (2) LEVELING BINDER COURSE, (MACHINE METHOD), N50, 1" THICKNESS
 - (3) AGGREGATE SHOULDER, CA-6, TYPE B (GRADE B LIMESTONE)
 - (4) HOT MIX ASPHALT PAVEMENT PATCHING, 6" (AS DIRECTED BY ENGINEER)
 - (5) LANDSCAPE RESTORATION, TOPSOIL, SEED AND EROSION BLANKET
 - (6) AREA REFLECTIVE CRACK CONTROL TREATMENT, TYPE A
 - (7) BITUMINOUS MATERIALS (PRIME COAT)
 - (8) AGGREGATE (PRIME COAT)



HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
	MIXTURE TYPE	AIR VOIDS @ Ndes
PAVEMENT OVERLAY	HOT MIX ASPHALT SURFACE COURSE, MIX "C", N50 (IL 9.5mm), 2"	4% @50 GYR
	LEVELING BINDER (MACHINE METHOD), N50, 1"	4% @50 GYR
DRIVEWAYS	HOT MIX ASPHALT SURFACE COURSE, MIX "C", N50 (IL 9.5mm), 2"	4% @50 GYR
PATCHING	HOT MIX ASPHALT PAVEMENT PATCHING, 6"	4% @50 GYR

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/ SQ. YD./IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SB5/ SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

•• WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG58-22.

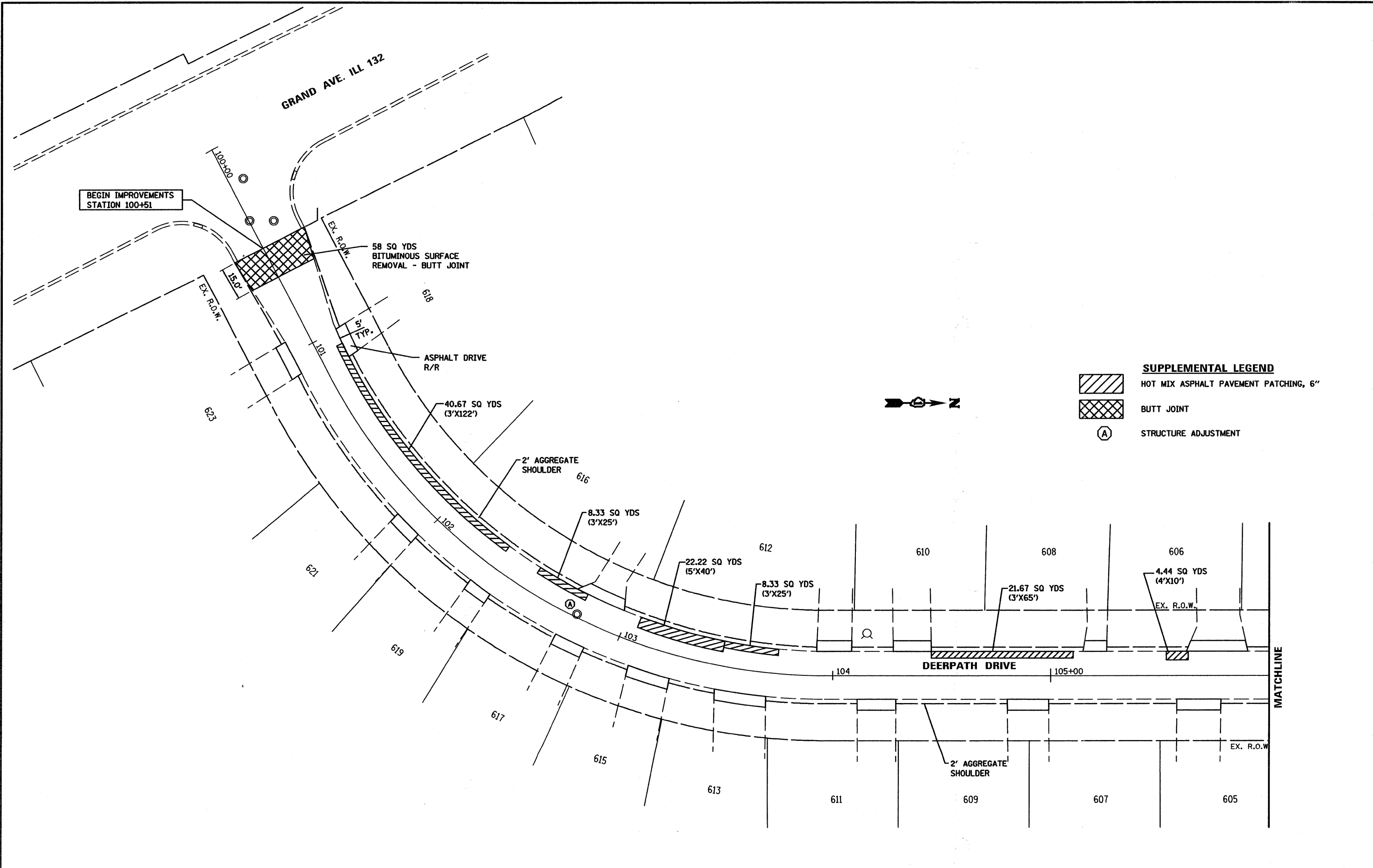
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION




TYPICAL SECTIONS

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
158	09-00026-00-RS	LAKE	8	3
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 63364	

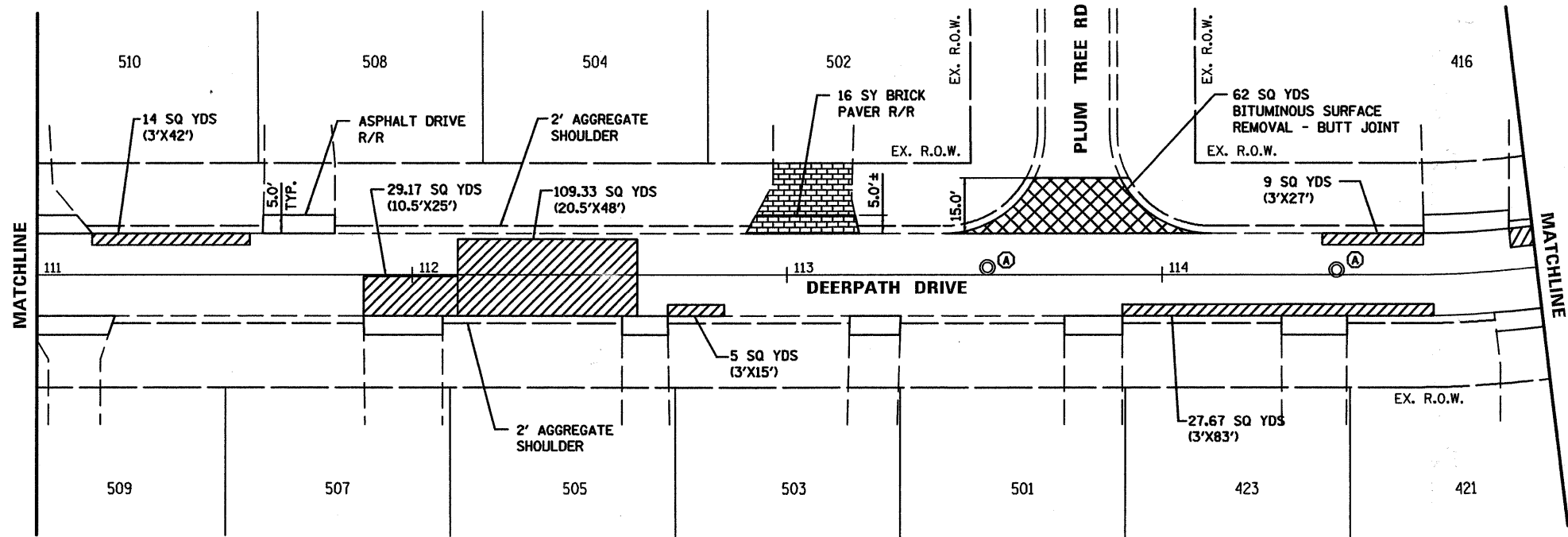
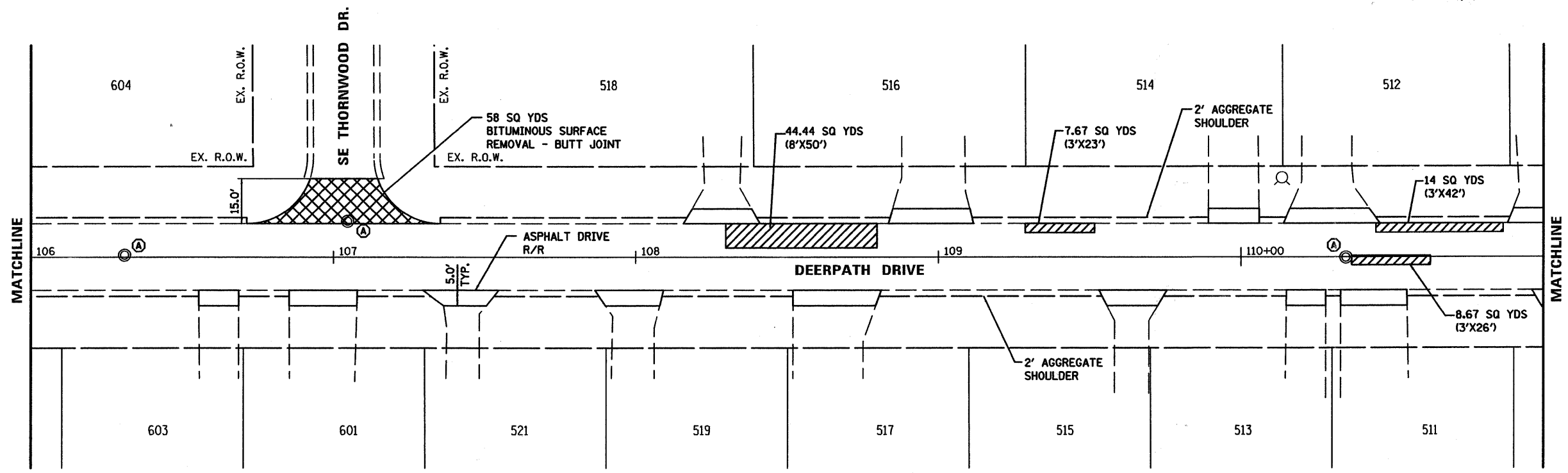





SUPPLEMENTAL LEGEND

	HOT MIX ASPHALT PAVEMENT PATCHING, 6"
	BUTT JOINT
	STRUCTURE ADJUSTMENT



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PLOT SCALE = 48.0000 / in.	CHECKED - JBR	REVISED -	REVISED -		SCALE: 1"=20'	SHEET NO. 4 OF	SHEETS	STA. 100+00	TO STA. 106+00	CONTRACT NO. 63364		
PLOT DATE = 10/15/2009	DATE -	REVISED -	REVISED -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT							



- SUPPLEMENTAL LEGEND**
-  HOT MIX ASPHALT PAVEMENT PATCHING, 6"
 -  BUTT JOINT
 -  STRUCTURE ADJUSTMENT

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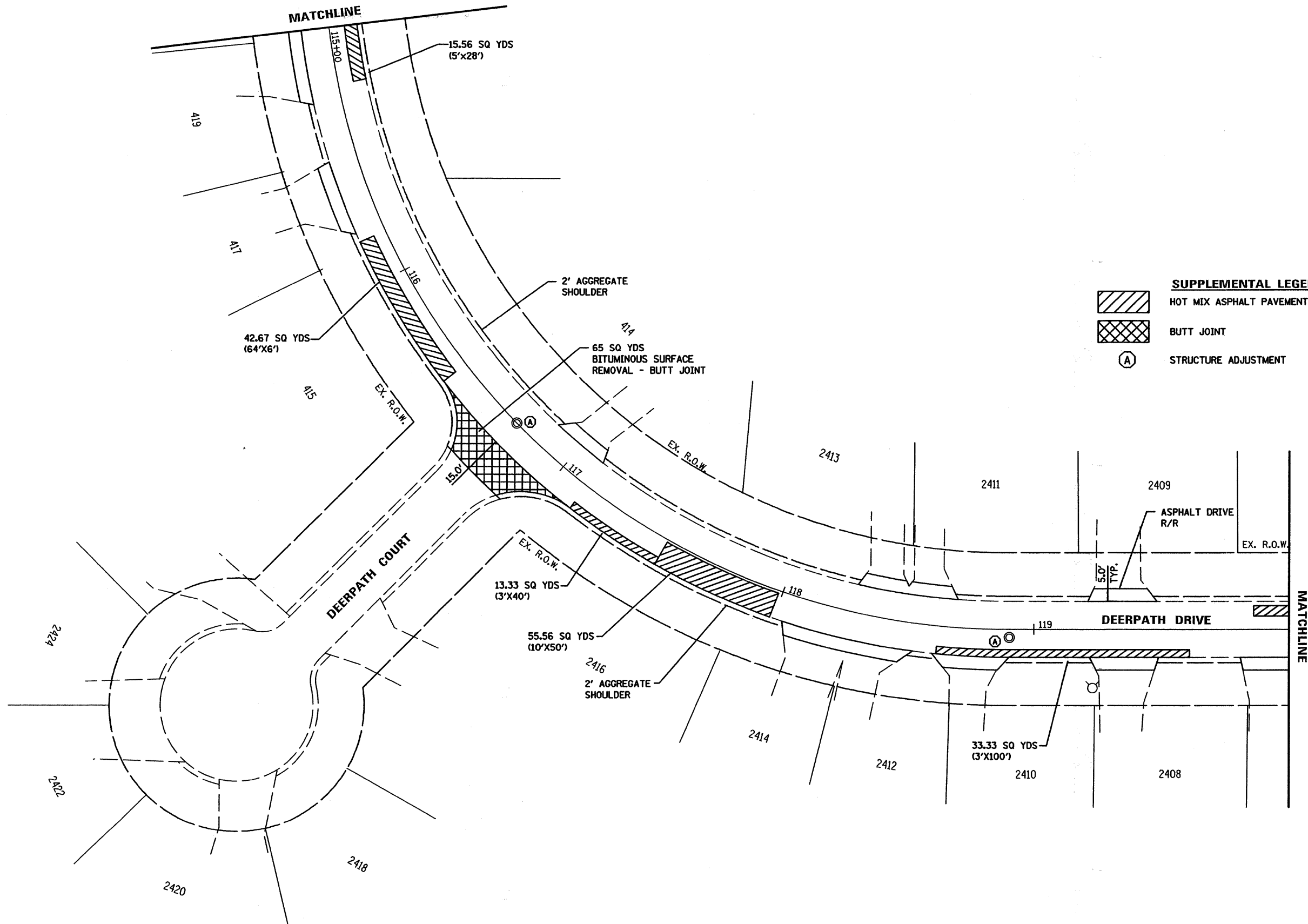
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**




**PROPOSED PLAN
 DEERPATH DRIVE RESURFACING**

SCALE: 1"=20' SHEET NO. 5 OF SHEETS STA. TO STA.

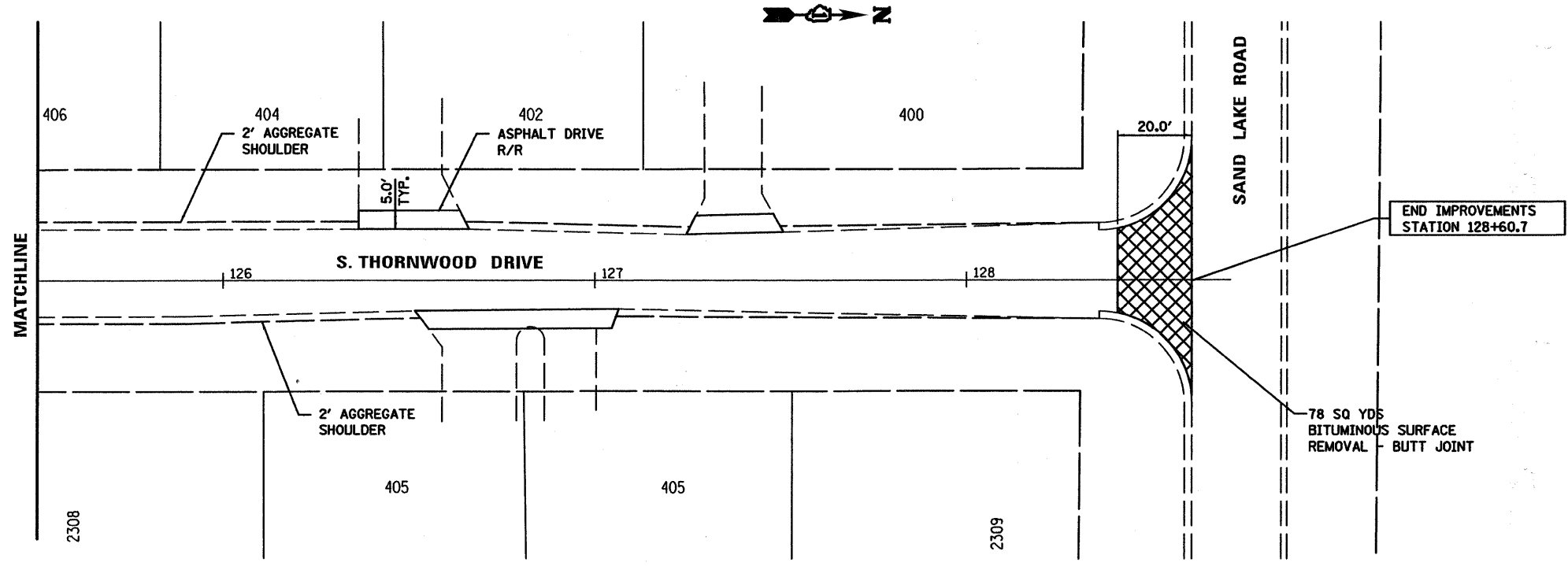
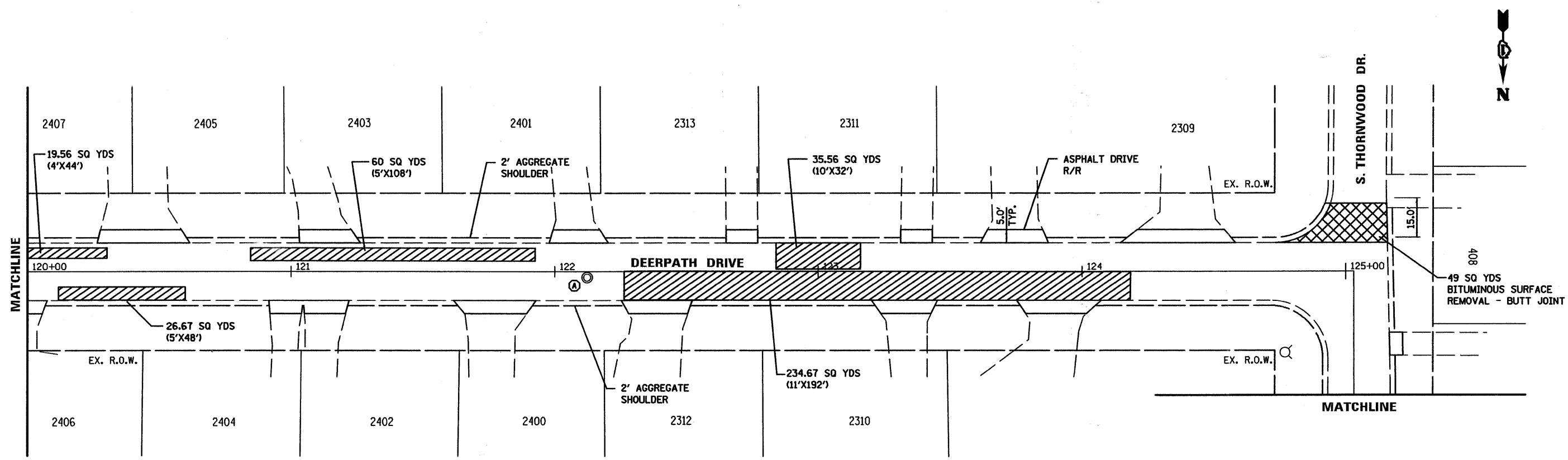
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
158	09-00026-00-RS	LAKE	8	5
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
		CONTRACT NO. 63364		






SUPPLEMENTAL LEGEND

	HOT MIX ASPHALT PAVEMENT PATCHING, 6"
	BUTT JOINT
	STRUCTURE ADJUSTMENT

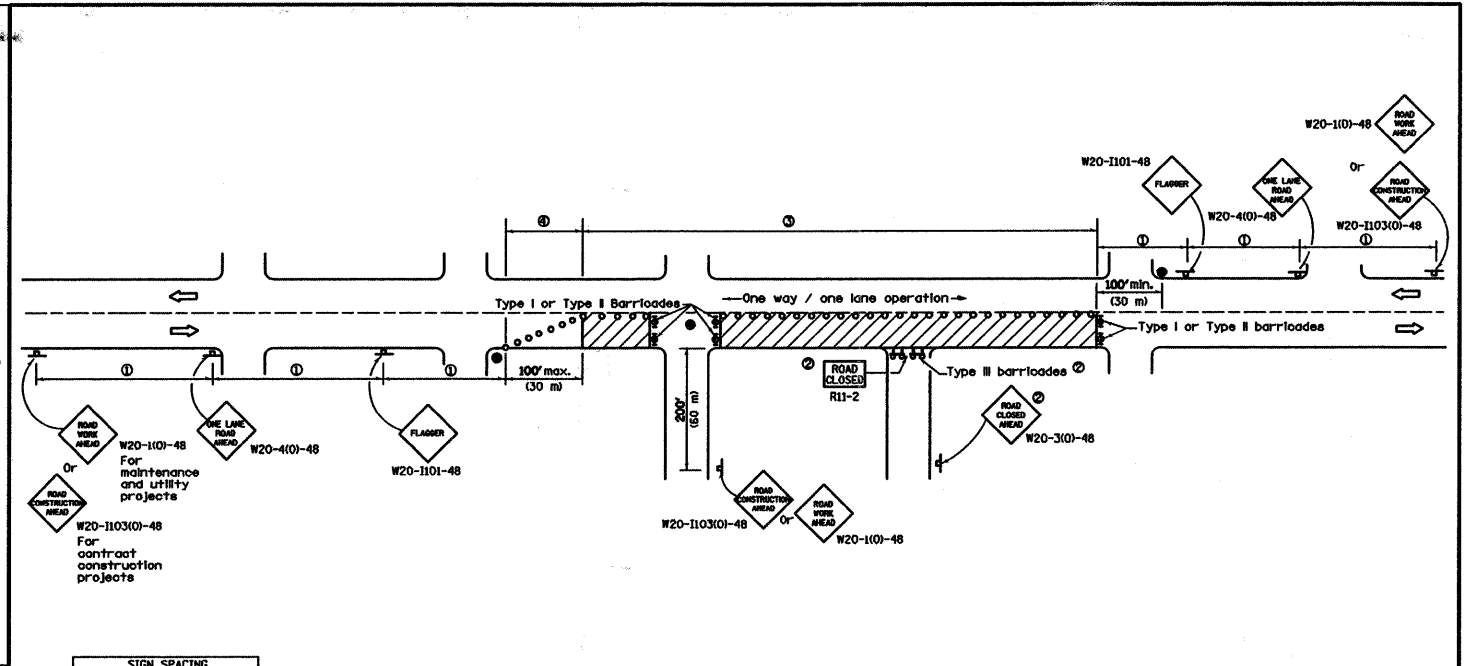
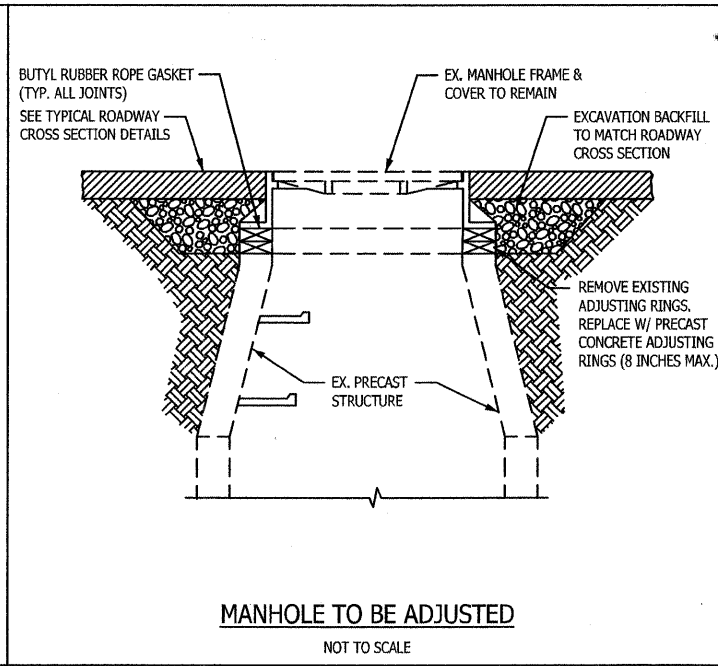
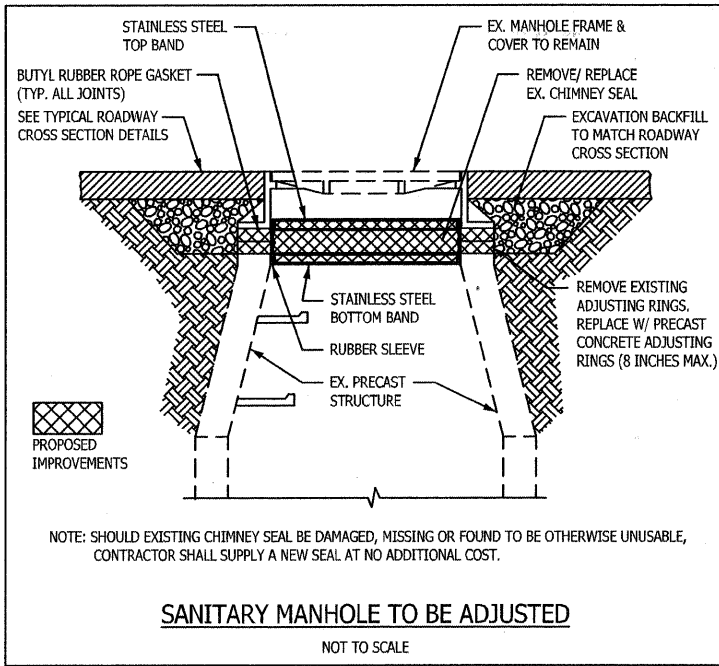
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	PLOT SCALE = 40.0000' / in.	DRAWN - JWJ	REVISED -		SCALE: 1"=20'	SHEET NO. 6 OF	SHEETS	STA. 115+00	TO STA. 120+00	CONTRACT NO. 63364			
	PLOT DATE = 10/15/2009	CHECKED - JRB	REVISED -		SHEET NO. 6 OF	SHEETS	STA. 115+00	TO STA. 120+00					
	DATE -	REVISED -	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT										



SUPPLEMENTAL LEGEND

	HOT MIX ASPHALT PAVEMENT PATCHING, 6"
	BUTT JOINT
	STRUCTURE ADJUSTMENT

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		CHECKED - JRB	REVISED -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT							
		DATE -	REVISED -									

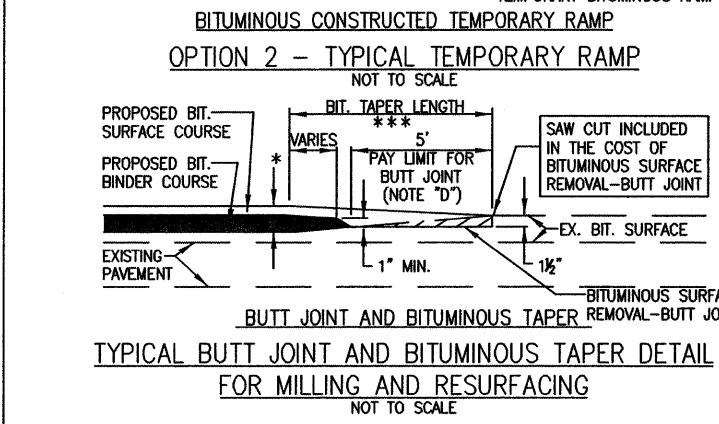
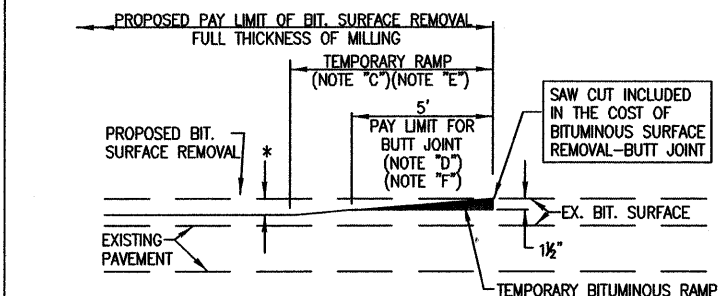
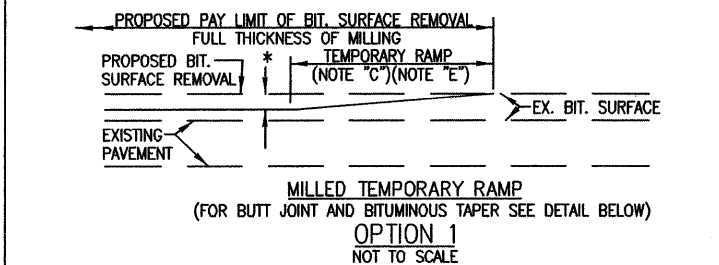


SIGN SPACING	
Posted Speed	Sign Spacing
55	500' (150 m)
50-45	350' (100 m)
<45	200' (60 m)

- SYMBOLS**
- Work area
 - Cone, drum or barricade (not required for moving operations)
 - Sign on portable or permanent support
 - Flagger with traffic control sign
 - Barricade or drum with flashing light
 - Type III barricade with flashing lights

- GENERAL NOTES**
- 1 Refer to SIGN SPACING TABLE for distances.
 - 2 For approved alderoad closures.
 - 3 Cones at 25' (8 m) centers for 250' (75 m). Additional cones may be placed at 50' (15 m) centers. When drums or Type I or Type II barricades are used, the interval between devices may be doubled.
 - 4 Cones, drums or barricades at 20' (6 m) centers.

**URBAN LANE CLOSURE,
2L, 2W, UNDIVIDED**
STANDARD 701501-05



- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS
 - ** PC CONCRETE, BITUMINOUS OR BITUMINOUS RESURFACED PAVEMENT
 - *** 20 FT PER 1 INCH RESURFACING (NOTE "A")
10 FT PER 1 INCH RESURFACING (NOTE "B")
- A. MAINLINE ROADWAYS AND MAJOR SIDE ROADS
 - B. MINOR SIDE ROADS
 - C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING BITUMINOUS SURFACE.
 - D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED BITUMINOUS COURSES.
 - E. TAPER THE TEMP. RAMP AT A RATE OF 1 FT PER INCH OF MILLING THICKNESS
 - F. INSTALLATION AND REMOVAL OF THE 5 FT TEMP. BIT. RAMP WILL BE PAID AS "BITUMINOUS SURFACE REMOVAL - BUTT JOINT"
 - G. SEE ARTICLE 406.11 AND 406.24 OF THE STANDARD SPECIFICATIONS FOR "BITUMINOUS AND P.C.C. SURFACE REMOVAL, BUTT JOINT".

