

STAGE 1

- PERFORM DITCH GRADING AND SHAPING AT STA. 106+00, RT. THE VILLAGE WILL VIDEO TAPE THE EXISTING STORM ONCE THE SEWERS ARE ABLE TO DISCHARGE TO THE DITCH. THE VILLAGE SHALL THEN DETERMINE THE EXTENT OF STORM SEWER MAINLINE REPLACEMENT IS NECESSARY BETWEEN IRVING PARK ROAD AND ALBION AVENUE. THE CONTRACTOR SHALL NOT ORDER ANY MATERIAL FOR THIS MAINLINE REPLACEMENT UNTIL THE VILLAGE DETERMINES THE CONDITION OF THE EXISTING PIPES.
- IMPLEMENT DETOUR ROUTE FOR SOUTHBOUND TRAFFIC AS SHOWN ON SHEET NO. 33.
- INSTALL AND ACTIVATE TEMPORARY TRAFFIC SIGNALS AT THE INTERSECTION OF WRIGHT BOULEVARD AND IRVING PARK ROAD AND AT THE INTERSECTION OF WRIGHT BOULEVARD AND WISE ROAD. (NOTE: THE EXISTING SIGNALS SHALL BE TURNED OFF AT THE SAME TIME THE TEMPORARY SIGNALS ARE ACTIVATED.)
- REMOVE EXISTING SIGNAL POLES AND EQUIPMENT DESIGNATED TO BE REMOVED.
- PLACE ALL CONSTRUCTION SIGNS, TEMPORARY PAVEMENT MARKINGS AND BARRICADES.
- CLOSE EXISTING SIDEWALK ON EAST SIDE OF WRIGHT BOULEVARD IN ACCORDANCE WITH IDOT STANDARD 701801.
- CONSTRUCT STORM SEWER LATERALS ACROSS WRIGHT BOULEVARD. DAILY LANE CLOSURES IN ACCORDANCE WITH IDOT STANDARD 701501 SHALL BE ALLOWED BETWEEN THE HOURS OF 9:00 AM AND 3:00 PM. THE EXISTING PAVEMENT WITHIN THE WORK ZONE SHALL REMAIN EXCEPT FOR THE REQUIRED TRENCHES. REMOVAL OF THE PAVEMENT OVER THE TRENCHES SHALL BE PAID FOR AS "PAVEMENT REMOVAL". TRENCHES OUTSIDE THE STAGE 1 WORK ZONE SHALL NOT BE LEFT IN STONE OVERNIGHT. THE CONTRACTOR SHALL CONSTRUCT, REPAIR AND MAINTAIN THE TRENCHES WITH CLASS D PATCHES, 6 INCH. THE CONTRACTOR SHALL ENSURE POSITIVE DRAINAGE IS PROVIDED TO THE NEW STORM SEWER STRUCTURES ON BOTH SIDES OF WRIGHT BOULEVARD.
- REMOVE EXISTING PAVEMENT, CURB AND GUTTER, AND DRIVEWAYS ON THE EAST SIDE OF WRIGHT BOULEVARD.
- CONSTRUCT ALL CURB AND GUTTER, PAVEMENT (EXCEPT SURFACE COURSE), SIDEWALKS, AND DRIVEWAYS ON THE EAST SIDE OF WRIGHT BOULEVARD.
- WHEN WORKING ADJACENT TO WISE ROAD, DAILY LANE CLOSURES IN ACCORDANCE WITH IDOT STANDARD 701601 SHALL BE ALLOWED BETWEEN THE HOURS OF 9:00 AM AND 3:00 PM.
- PLACE TOPSOIL AND SOD ON THE EAST SIDE.

STAGE 2

- PLACE ALL CONSTRUCTION SIGNS, TEMPORARY PAVEMENT MARKINGS, AND BARRICADES. ADJUST TEMPORARY TRAFFIC SIGNALS AND SHIFT TRAFFIC AS INDICATED.
- CLOSE EXISTING BIKE PATH ON WEST SIDE OF WRIGHT BOULEVARD IN ACCORDANCE WITH IDOT STANDARD 701801.
- REMOVE EXISTING PAVEMENT, CURB AND GUTTER, AND DRIVEWAYS ON THE WEST SIDE OF WRIGHT BOULEVARD.
- CONSTRUCT REMAINING STORM SEWERS AND DRAINAGE STRUCTURES ALONG THE WEST SIDE OF WRIGHT BOULEVARD.
- CONSTRUCT ALL CURB AND GUTTER, PAVEMENT (EXCEPT SURFACE COURSE), SIDEWALKS, BIKE PATHS, AND DRIVEWAYS ON THE WEST SIDE OF WRIGHT BOULEVARD.
- WHEN WORKING ADJACENT TO WISE ROAD, DAILY LANE CLOSURES IN ACCORDANCE WITH IDOT STANDARD 701601 SHALL BE ALLOWED BETWEEN THE HOURS OF 9:00 AM AND 3:00 PM.

STAGE 3 (NOT SHOWN)

- CONSTRUCTION SIGNS, TEMPORARY PAVEMENT MARKINGS, AND BARRICADES SHALL REMAIN AS SHOWN FOR STAGE 2.
- CONSTRUCT MEDIAN PAVEMENT.
- PLACE TOPSOIL AND SOD.
- MILL RESURFACING AREA BENEATH ELGIN-O'HARE EXPRESSWAY.
- PLACE SURFACE COURSE AT SOUTH END OF WRIGHT BOULEVARD AND IN THE RESURFACING SECTION.
- PLACE PERMANENT PAVEMENT MARKINGS ALONG ENTIRE LENGTH OF PROJECT.
- INSTALL AND ACTIVATE PERMANENT SIGNALS AT ALL LOCATIONS.
- END USE OF SOUTHBOUND DETOUR.

ACCESS MAINTENANCE NOTES

MAINTAINING ACCESS TO DRIVEWAYS ALONG WRIGHT BOULEVARD AND THE CROSS STREETS IS OF THE UTMOST IMPORTANCE TO THE VILLAGE. ACCESS TO ALL PROPERTIES SHALL BE MAINTAINED AT ALL TIMES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUB-STAGING THE CONSTRUCTION OF DRIVEWAYS. THE CONTRACTOR SHALL FOLLOW THESE PROCEDURES TO ENSURE PROPER DRIVEWAY ACCESS:

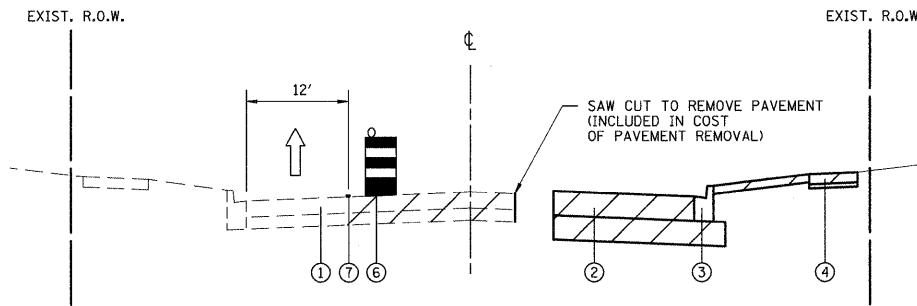
- THE CONTRACTOR SHALL WORK WITH ADJACENT BUSINESS OWNERS TO DETERMINE DRIVEWAY RECONSTRUCTION SCHEDULING. ALL DRIVEWAY CLOSURES SHALL BE APPROVED BY THE ENGINEER.
- TEMPORARY DRIVES SHALL BE CONSTRUCTED USING 100% RECYCLED ASPHALT PAVEMENT. THE WIDTH OF THE DRIVE SHALL BE DETERMINED BY THE ENGINEER. THE CONTRACTOR SHALL NOTE THAT THE TEMPORARY DRIVEWAY WIDTH MAY EXCEED THE WIDTH OF THE EXISTING DRIVEWAY IN ORDER TO ALLOW FOR SEMI-TRAILERS TO BACK INTO THE LOADING DOCKS.

THE COST OF PLACING, MAINTAINING AND REMOVING TEMPORARY DRIVES SHALL BE INCLUDED IN THE COST OF "TEMPORARY ACCESS (COMMERCIAL ENTRANCE)".

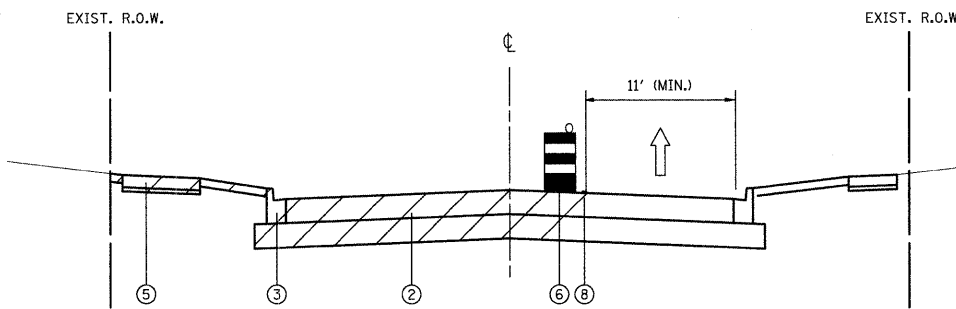
IT MAY BE NECESSARY TO CONSTRUCT TEMPORARY PADS IN THE CONSTRUCTION ZONE FOR DRIVEWAYS ON THE OPPOSITE SIDE OF THE ROAD. THESE PADS MAY BE NECESSARY FOR SEMI-TRAILERS TO BACK INTO THE LOADING DOCKS. THE ENGINEER SHALL DIRECT THE CONTRACTOR AS TO THE LOCATIONS AND SIZES OF THESE PADS. THE COST OF PLACING THESE TEMPORARY PADS SHALL BE PAID FOR AS "TEMPORARY ACCESS (COMMERCIAL ENTRANCE)".
- A 4" PVC DRAIN SHALL BE PLACED UNDER TEMPORARY DRIVES TO PROVIDE POSITIVE DRAINAGE WHEN THE ROADWAY IS EXCAVATED. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF "DRIVEWAY PAVEMENT REMOVAL".
- QUANTITIES FOR HIGH-EARLY-STRENGTH PORTLAND CEMENT CONCRETE PAVEMENT 10" JOINTED AND COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (SPECIAL) HAVE BEEN INCLUDED FOR USE IN FRONT OF THE DRIVEWAYS IN ORDER TO EXPEDITE THE COMPLETION OF THIS WORK. THESE ITEMS SHALL ONLY BE USED WHEN APPROVED BY THE ENGINEER.
- ALL BARRICADES REQUIRED TO SUB-STAGE DRIVEWAY CONSTRUCTION AND MAINTAIN ACCESS TO DRIVEWAYS SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION".

LEGEND

- EXISTING PAVEMENT
 - PROPOSED PAVEMENT
 - PROPOSED COMB. CONCRETE CURB & GUTTER
 - PROPOSED SIDEWALK
 - PROPOSED BIKEPATH
 - TYPE II BARRICADES OR DRUMS WITH MONO DIRECTIONAL STEADY BURN LIGHT
 - TEMPORARY PAINT PAVEMENT MARKING - LINE 4"
 - PAVEMENT MARKING TAPE, TYPE III - LINE 4"
 - CONSTRUCTION WORK ZONE
- ↑ TRAFFIC FLOW



STAGE 1 TYPICAL SECTION



STAGE 2 TYPICAL SECTION

NOTE:
THE NORTHBOUND LANE MAY BE REDUCED TO A 10' WIDTH DURING CONSTRUCTION AND CURING OF THE MEDIAN PAVEMENT.

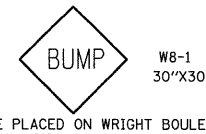
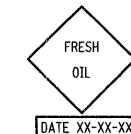
CROSS STREET RECONSTRUCTION

THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUB-STAGING INTERSECTION CONSTRUCTION.

- THE CROSS STREETS MAY BE CLOSED TO ONE-WAY TRAFFIC DURING THE WORK DAY FROM 9:00 A.M. TO 3:00 P.M. THE CLOSURE SHALL CONFORM TO IDOT STANDARD 701501. AT ALL OTHER TIMES, THE CROSS STREETS MUST REMAIN OPEN TO TWO-WAY TRAFFIC. A MINIMUM 12' LANE SHALL BE PROVIDED IN EACH DIRECTION. THREE FLAGGERS WILL BE REQUIRED FOR THE CLOSURES (ONE FOR NORTHBOUND WRIGHT AND ONE FOR EACH DIRECTION OF TRAFFIC ON THE CROSS STREET). THE COST OF THE FLAGGERS SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION".
- TRAFFIC ON AND ACCESS TO THE CROSS STREETS SHALL BE MAINTAINED THROUGH THE USE OF THE PAY ITEM "TEMPORARY ACCESS (ROAD)". A QUANTITY OF 2 EACH HAS BEEN INCLUDED FOR EACH INTERSECTION (ONE FOR EACH STAGE THE INTERSECTION IS UNDER CONSTRUCTION). THE CONTRACTOR SHALL MAINTAIN THE TEMPORARY AGGREGATE SUCH THAT THE MAXIMUM ELEVATION DIFFERENCE BETWEEN THE TEMPORARY AGGREGATE AND THE EXISTING OR PROPOSED PAVEMENT IN THE EAST-WEST DIRECTION IS LESS THAN ONE INCH. THE WORK INCLUDED IN THIS PAY ITEM SHALL ALSO CONSIST OF PLACING ADDITIONAL TEMPORARY AGGREGATE AT THE CORNERS OF EACH INTERSECTION IN ORDER TO ENLARGE THE CORNER RADII TO ALLOW FOR TRUCK TURNING MOVEMENTS.
- THE CROSS STREETS (EXCEPT FOR ESTES COURT) MAY BE CLOSED TO TRAFFIC FOR A MAXIMUM OF 2 TIMES TO CONSTRUCT THE PROPOSED CONCRETE PAVEMENT - ONCE FOR THE CONSTRUCTION OF THE NORTHBOUND THROUGH LANE AND SIDE STREET APPROACH AND ONCE FOR THE REMAINDER OF THE INTERSECTION CONSTRUCTION. THE SIDE STREET MUST BE RE-OPENED TO TWO-WAY TRAFFIC WITHIN 14 CALENDAR DAYS OF THE CLOSURE. IF THE SIDE STREET IS NOT RE-OPENED WITHIN THIS TIME, THE CONTRACTOR WILL BE CHARGED LIQUIDATED DAMAGES, IN ACCORDANCE WITH THE BDE SPECIAL PROVISION. THE CONTRACTOR SHALL PLACE CHANGEABLE MESSAGE SIGNS ON WRIGHT BOULEVARD AND THE SIDESTREETS ONE WEEK IN ADVANCE OF THE CLOSURES. THE ENGINEER SHALL APPROVE THE MESSAGE TO BE DISPLAYED. THIS WORK WILL BE PAID FOR AS "CHANGEABLE MESSAGE SIGN".

MAINTENANCE OF TRAFFIC GENERAL NOTES

- TRAFFIC CONTROL DEPICTED IN THESE PLANS AND THE APPLICABLE IDOT DETAILS AND STANDARDS ARE THE MINIMUM REQUIREMENTS. OTHER WORK OR SIGNING MAY BE REQUIRED BY THE ENGINEER. TRAFFIC CONTROL AND PROTECTION SHALL BE PERFORMED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS, DIVISION 700; APPLICABLE GUIDELINES IN THE ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS; AND APPLICABLE HIGHWAY STANDARDS FOR TRAFFIC CONTROL, UNLESS HEREIN REVISED.
- THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND TRAFFIC CONTROL DEVICES SHALL FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.
- ALL CONSTRUCTION SIGNS SHALL HAVE FLUORESCENT ORANGE BACKGROUNDS.
- ALL SIGNS SHALL BE MOUNTED ON METAL POSTS, 7 FEET ABOVE THE EXISTING GROUND AND DRIVEN A MINIMUM OF 3 FEET INTO THE GROUND. A J.U.L.I.E. LOCATE SHALL BE PERFORMED PRIOR TO THE INSTALLATION OF THE POSTS.
- BARRICADES WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS WILL BE REQUIRED ADJACENT TO PAVEMENT EDGES WHERE WIDENING, CURB AND GUTTER OR OVERLAYING WORK IS BEING DONE, AS SPECIFIED IN SECTION 701 OF THE STANDARD SPECIFICATIONS. SPACING SHALL BE AS SHOWN ON THE CONSTRUCTION STAGING PLANS UNLESS OTHERWISE DIRECTED BY THE ENGINEER. BARRICADES THAT MUST BE PLACED IN EXCAVATED AREAS SHALL HAVE LEG EXTENSIONS INSTALLED SUCH THAT THE TOPS OF THE BARRICADES ARE IN COMPLIANCE WITH THE HEIGHT REQUIREMENTS OF STANDARD 701901.
- ALL BARRICADES OR DRUMS AT LANE DIVERSIONS WITHIN TAPER SECTIONS SHALL HAVE DIRECTION INDICATOR PANELS.
- BARRICADES OR DRUMS EQUIPPED WITH ONE-WAY FLASHING LIGHTS WILL BE REQUIRED AT ALL OPEN TRENCHES, EXCAVATIONS, OPEN OR EXPOSED SEWER STRUCTURES, AND AT ANY OTHER LOCATIONS DESIGNATED BY THE ENGINEER OR LAW ENFORCEMENT AGENCIES. BARRICADES SHALL BE PLACED AT 50' CENTERS ALONG TANGENTS, 25' ALONG TAPERS AND 10' AROUND RADII.
- DRUMS SHALL HAVE ALTERNATING REFLECTORIZED TYPE AA OR TYPE AP FLUORESCENT ORANGE AND REFLECTORIZED WHITE HORIZONTAL, CIRCUMFERENTIAL STRIPES.
- DRUMS AND BARRICADES SHALL MEET THE REQUIREMENTS OF THE NATIONAL COOPERATIVE HIGHWAY RESEARCH PROGRAM (NCHRP) REPORT 350 AND THE SUPPLEMENTAL SPECIAL PROVISION "WORK ZONE TRAFFIC CONTROL DEVICES".
- THE CONTRACTOR SHALL INFORM THE ENGINEER OF ANY STAGE CHANGE AT LEAST TWO WEEKS IN ADVANCE OF THE CHANGE.
- EXISTING TRAFFIC CONTROL SIGNS AND DEVICES SHALL BE REMOVED OR RELOCATED BY THE CONTRACTOR AFTER THE TRAFFIC CONTROL REQUIREMENTS ARE MET OR AS AUTHORIZED BY THE ENGINEER. ANY SIGNS OR DEVICES LEFT IN PLACE ARE TO BE PROTECTED FROM DAMAGE AND MAINTAINED. ANY DAMAGE CAUSED BY HIS WORK SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER AT THE EXPENSE OF THE CONTRACTOR.
- THE FIRST WARNING SIGNS IN EACH DIRECTION OF TRAVEL SHALL BE EQUIPPED WITH MONO-DIRECTIONAL AMBER FLASHING LIGHTS DURING HOURS OF DARKNESS. FLAGS ARE OPTIONAL.
- TEMPORARY LANE CLOSURES WILL BE ALLOWED ONLY BETWEEN THE HOURS OF 9:00 A.M. AND 3:00 P.M., WITH TRAFFIC MAINTAINED IN ACCORDANCE WITH THE APPLICABLE IDOT STANDARD UNLESS OTHERWISE DIRECTED BY THE ENGINEER. FULL CLOSURE OF WRIGHT BOULEVARD WILL NOT BE ALLOWED.
- "WORKERS" SIGNS SHALL ONLY BE ERECTED WHEN WORKERS ARE PRESENT. SIGN MUST BE COVERED OR REMOVED WHEN NO WORKERS ARE PRESENT.
- A QUANTITY OF "RAISED REFLECTIVE PAVEMENT MARKER REMOVAL" HAS BEEN INCLUDED TO REMOVE RRPMS WITHIN THE WORK ZONE THAT ARE IN CONFLICT WITH THE PROPOSED STAGING OF TRAFFIC, AS DETERMINED BY THE ENGINEER. ONLY THOSE RRPMS DESIGNATED FOR REMOVAL BY THE ENGINEER SHALL BE PAID FOR. ALL OTHER RRPMS REMOVED WITHIN THE WORKZONE SHALL BE CONSIDERED INCLUDED IN THE PAY ITEM "PAVEMENT REMOVAL".
- "FRESH OIL" SIGNS (W21-2-4848) WITH DATE SIGNS SHALL BE ERECTED 48 HOURS PRIOR TO PRIMING ALONG WRIGHT BOULEVARD. THE COST OF THESE SIGNS SHALL BE INCLUDED IN THE PAY ITEM "TRAFFIC CONTROL AND PROTECTION".
- THE CONTRACTOR SHALL ERECT TEMPORARY STREET NAME SIGNS ON METAL POSTS THROUGHOUT CONSTRUCTION TO THE SATISFACTION OF THE ENGINEER. THE COST OF THESE SIGNS SHALL BE INCLUDED IN THE PAY ITEM "TRAFFIC CONTROL AND PROTECTION".
- TEMPORARY PAVEMENT MARKING TAPE SHALL BE USED ON ALL SURFACES OUTSIDE OF THE PROJECT LIMITS AND ON THE FINAL PAVEMENT SURFACE. THIS WORK SHALL BE PAID FOR AS "PAVEMENT MARKING TAPE, TYPE III" OF THE SIZE SPECIFIED.
- ARROW BOARDS WILL BE REQUIRED WHEN IMPLEMENTING ALL LANE CLOSURES, AND SHALL BE INCLUDED IN THE PAY ITEM "TRAFFIC CONTROL AND PROTECTION".
- THE COST OF SUPPLYING, ERECTING, AND MAINTAINING BARRICADES, DRUMS, WARNING LIGHTS, AND SIGNS SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION". QUANTITIES FOR SHORT-TERM PAVEMENT MARKINGS, TEMPORARY PAVEMENT MARKINGS, AND WORK ZONE PAVEMENT MARKING REMOVAL ARE NOT INCLUDED IN THE ITEM "TRAFFIC CONTROL AND PROTECTION" AND SHALL BE MEASURED SEPARATELY FOR PAYMENT.
- A TOTAL QUANTITY OF FOUR EACH FOR "TEMPORARY ACCESS (ROAD)" HAS BEEN INCLUDED FOR USE IN PROVIDING PEDESTRIAN ACCESS ACROSS THE RECONSTRUCTION AT THE INTERSECTION OF WRIGHT BOULEVARD AND IRVING PARK ROAD AND THE INTERSECTION OF WRIGHT BOULEVARD AND WISE ROAD.
- THE "ROAD CLOSED" (R11-2) SIGNS SHALL BE MOUNTED ON THE TYPE III BARRICADES. ALL TYPE III BARRICADES SHALL HAVE 2 AMBER TYPE A-LOW INTENSITY FLASHING LIGHTS SPACED NEAR THE CENTERLINES OF THE SUPPORTS.



TO BE PLACED ON WRIGHT BOULEVARD AND ALL SIDE STREETS AS DIRECTED BY THE ENGINEER

DATE	
BY	
PLAN	
NOTED	
NO.	

DATE	
BY	
PROFILE	
NOTED	
NO.	