

LETTING ITEN NO: 8A  
LETTING DATE: 01/15/2016

# DIXON MUNICIPAL AIRPORT CHARLES R. WALGREEN FIELD DIXON, ILLINOIS

D1027  
TOTAL SHEETS = 13

## INDEX TO SHEETS

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## CONSTRUCTION PLANS FOR DIXON MUNICIPAL AIRPORT

INSTALL PERIMETER FENCE ON NORTH PROPERTY LINE

ILLINOIS PROJECT: C73-4370  
SBGP PROJECT: 3-17-SBGP-99/105

DECEMBER 1, 2015

SUMMARY OF QUANTITIES				
ITEM	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
<b>BASE BID</b>				
AR108086	1/C #6 XLP - USE	LF	2,650	
AR110212	2" STEEL DUCT, DIRECT BURY	LF	570	
AR110312	2" STEEL DUCT, JACKED	LF	55	
AR150510	ENGINEER'S FIELD OFFICE	LS	1	
AR151455	CLEARING AND GRUBBING	LS	1	
AR162506	CLASS E FENCE 6'	LF	1,500	
AR162530	WALKWAY GATES, CLASS E (5')	EACH	1	
AR162624	CLASS E GATE - 24'	EACH	2	
AR162720	ELECTRIC GATE - 20'	EACH	1	
AR162900	REMOVE CLASS E FENCE	LF	500	
AR162905	REMOVE GATE	EACH	2	
AR162920	REMOVE MANUAL SLIDE GATE	EACH	2	
AR901510	SEEDING	ACRE	1.0	
AR908510	MULCHING	ACRE	1.0	
<b>ADDITIVE ALTERNATE NO 1: T-HANGAR AREA</b>				
AS162506	CLASS E FENCE 6'	LF	620	
AS162624	CLASS E GATE - 24'	EACH	1	
AS162900	REMOVE CLASS E FENCE	LF	650	
AS162905	REMOVE GATE	EACH	1	
AS901510	SEEDING	ACRE	0.4	
AS908510	MULCHING	ACRE	0.4	
<b>ADDITIVE ALTERNATE NO 2: EAST TERMINAL AREA</b>				
AT162506	CLASS E FENCE 6'	LF	230	
AT162530	WALKWAY GATES, CLASS E (5')	EACH	1	
AT162900	REMOVE CLASS E FENCE	LF	230	
AT162905	REMOVE GATE	EACH	1	
AT901510	SEEDING	ACRE	0.2	
AT908510	MULCHING	ACRE	0.2	

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SUBMITTED BY: *[Signature]*  
DANIEL L. PAPE, P.E.

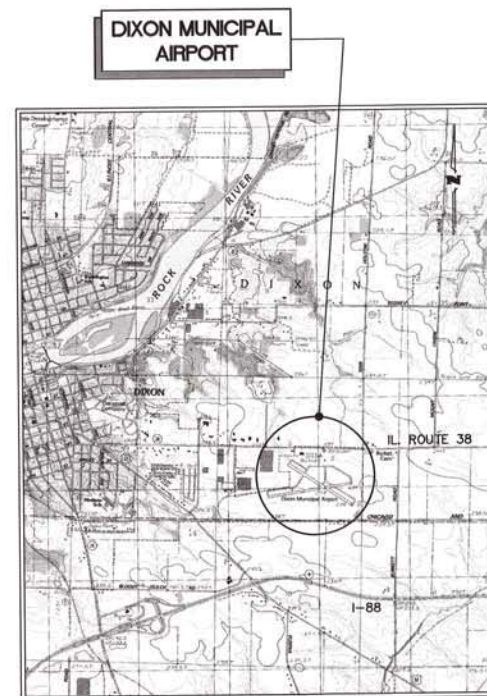
DATE: 12/1/15

**811** Know what's below. Call before you dig. www.illinois1call.com

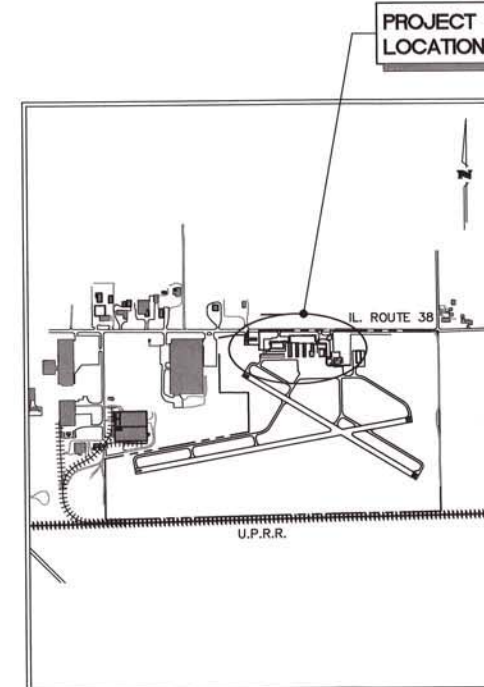
J.U.L.I.E. JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.



LOCATION MAP



SITE PLAN

DESIGN INFORMATION

APPROACH CATEGORY B  
DESIGN GROUP I

DIXON MUNICIPAL AIRPORT

TOWNSHIP: 21 NORTH  
RANGE: 9 EAST  
LEE COUNTY

DIXON TOWNSHIP  
(SECTION: 3)  
OPPOSITE LINCOLN HIGHWAY 38  
(FRANKLIN GROVE ROAD)

UNICOM RADIO FREQUENCY - 123.05



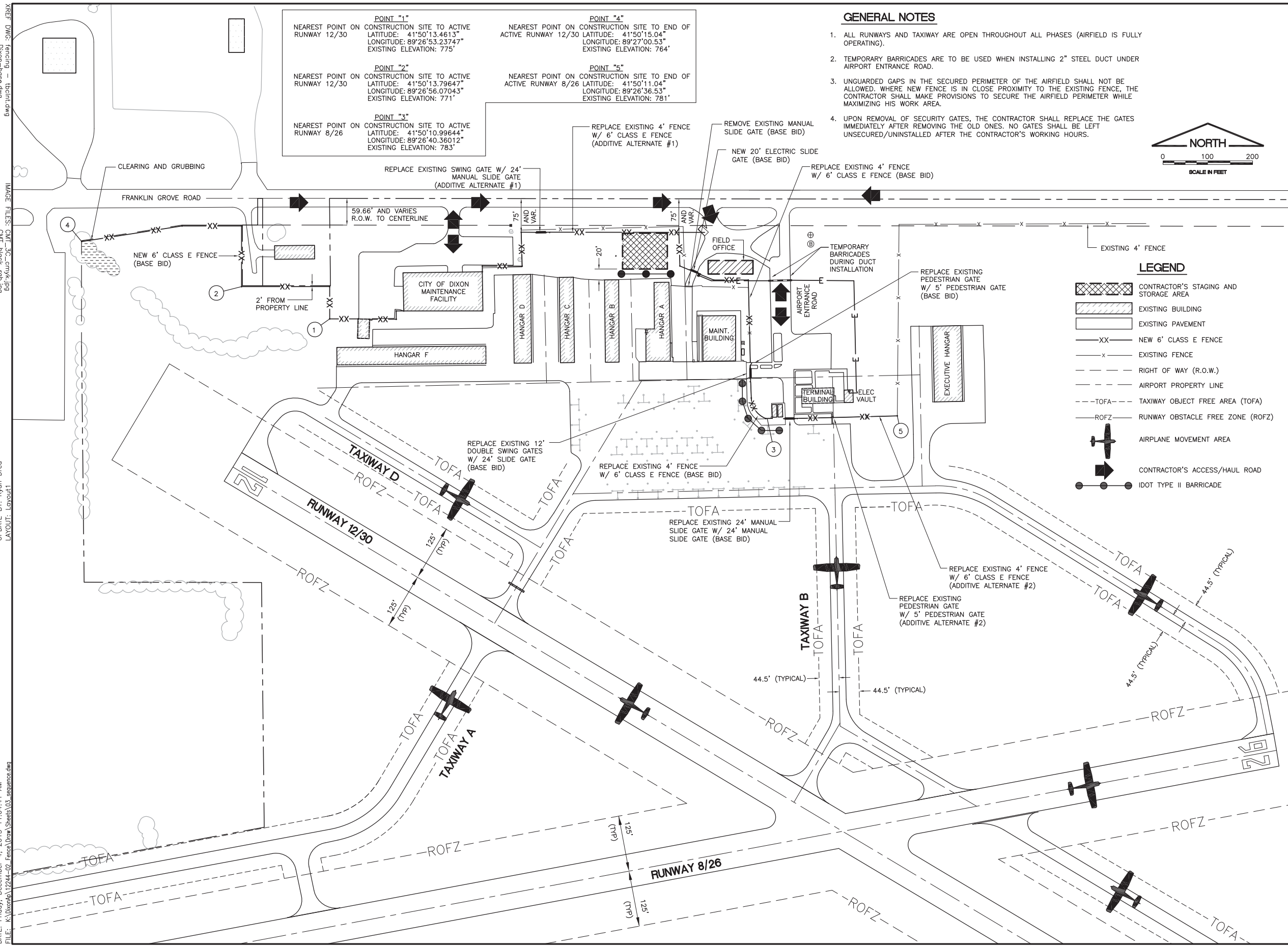


DATE: Friday, December 4, 2015 11:04:41 AM  
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 D:\Dixon\12244-02\_Fence\Draw\Sheets\03\_sequence.dwg  
 CMT\_block\_glb.dwg  
 CMT\_3C\_cmyk.dwg

NEAREST POINT ON RUNWAY 12/30	POINT "1" CONSTRUCTION SITE TO ACTIVE LATITUDE: 41°50'13.4613" LONGITUDE: 89°26'53.23747" EXISTING ELEVATION: 775'	NEAREST POINT ON CONSTRUCTION SITE TO END OF ACTIVE RUNWAY 12/30	POINT "4" CONSTRUCTION SITE TO END OF ACTIVE RUNWAY 12/30 LATITUDE: 41°50'15.04" LONGITUDE: 89°27'00.53" EXISTING ELEVATION: 764'
NEAREST POINT ON RUNWAY 8/26	POINT "2" CONSTRUCTION SITE TO ACTIVE LATITUDE: 41°50'13.79647" LONGITUDE: 89°26'56.07043" EXISTING ELEVATION: 771'	NEAREST POINT ON CONSTRUCTION SITE TO END OF ACTIVE RUNWAY 8/26	POINT "5" CONSTRUCTION SITE TO END OF ACTIVE RUNWAY 8/26 LATITUDE: 41°50'11.04" LONGITUDE: 89°26'36.53" EXISTING ELEVATION: 761'
NEAREST POINT ON RUNWAY 8/26	POINT "3" CONSTRUCTION SITE TO ACTIVE LATITUDE: 41°50'10.99644" LONGITUDE: 89°26'40.36012" EXISTING ELEVATION: 783'		

**GENERAL NOTES**

1. ALL RUNWAYS AND TAXIWAYS ARE OPEN THROUGHOUT ALL PHASES (AIRFIELD IS FULLY OPERATING).
2. TEMPORARY BARRICADES ARE TO BE USED WHEN INSTALLING 2" STEEL DUCT UNDER AIRPORT ENTRANCE ROAD.
3. UNGUARDED GAPS IN THE SECURED PERIMETER OF THE AIRFIELD SHALL NOT BE ALLOWED. WHERE NEW FENCE IS IN CLOSE PROXIMITY TO THE EXISTING FENCE, THE CONTRACTOR SHALL MAKE PROVISIONS TO SECURE THE AIRFIELD PERIMETER WHILE MAXIMIZING HIS WORK AREA.
4. UPON REMOVAL OF SECURITY GATES, THE CONTRACTOR SHALL REPLACE THE GATES IMMEDIATELY AFTER REMOVING THE OLD ONES. NO GATES SHALL BE LEFT UNSECURED/UNINSTALLED AFTER THE CONTRACTOR'S WORKING HOURS.



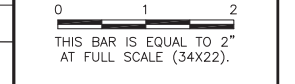
**LEGEND**

- CONTRACTOR'S STAGING AND STORAGE AREA
- EXISTING BUILDING
- EXISTING PAVEMENT
- NEW 6' CLASS E FENCE
- EXISTING FENCE
- RIGHT OF WAY (R.O.W.)
- AIRPORT PROPERTY LINE
- TAXIWAY OBJECT FREE AREA (TOFA)
- RUNWAY OBSTACLE FREE ZONE (ROFZ)
- AIRPLANE MOVEMENT AREA
- CONTRACTOR'S ACCESS/HAUL ROAD
- IDOT TYPE II BARRICADE

IL. CONTRACT: **DI027**  
 IL. LETTING ITEM: **8A**  
 IL. PROJECT: **C73-4370**  
 S.B.G. PROJECT: **3-17-SBGP-99/105**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



**DIXON MUNICIPAL AIRPORT  
 DIXON, ILLINOIS**  
**INSTALL PERIMETER FENCE ON NORTH PROPERTY LINE**  
**SEQUENCE OF CONSTRUCTION  
 PER AC 150/5370-2F (LATEST EDITION)**

DESIGN BY:	RMD
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	12/01/15
JOB No:	12244-02
<b>FINAL</b>	
SHEET	3 OF 13 SHEETS

REFERENCE: DWG: fencing - bchd1.dwg  
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 LAYOUT: LAYOUT1  
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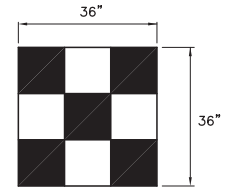
**GENERAL NOTES**

- THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT MANAGER AND RESIDENT ENGINEER AND BE APPROVED BY THE DIVISION OF AERONAUTICS AND FEDERAL AVIATION ADMINISTRATION.
- ALL OPERATIONS SHALL BE IN CONFORMANCE WITH AC 150/5370-2F (LATEST EDITION) OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION.
- CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE AREA WHEN CONSTRUCTION IS NOT IN PROGRESS.
- THE AIRPORT MANAGER IN CONSULTATION WITH THE RESIDENT ENGINEER SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT SAFETY.
- ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STORAGE AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING OR CONSTRUCTING THESE PAVEMENTS AND AREAS SHALL BE INCIDENTAL TO THE CONTRACT. EXISTING AREAS OUTSIDE THE PROJECT LIMITS WHICH ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT MANAGER.
- THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING TAXIWAYS, APRONS AND RUNWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE ENGINEER.
- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY. CONTRACTOR'S WORK HOURS SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES.
- THE CONTRACTOR SHALL PROVIDE PORTABLE FLOOD LIGHTING FOR NIGHTTIME CONSTRUCTION. SUFFICIENT UNITS SHALL BE PROVIDED SO THAT WORK AREAS ARE ILLUMINATED TO A LEVEL OF FIVE HORIZONTAL FOOT CANDLES. THE LIGHTING LEVELS SHALL BE CALCULATED AND MEASURED IN ACCORDANCE WITH THE CURRENT STANDARDS OF THE ILLUMINATION ENGINEERING SOCIETY. LIGHTS SHALL BE POSITIONED SO AS NOT TO INTERFERE WITH AIRPORT OPERATIONS.
- THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. WHEN ACTIVE AIRFIELD PAVEMENTS ARE UTILIZED AS HAUL ROADS BY THE CONTRACTOR, MATERIAL TRACKED ON TO THE PAVEMENT SHALL BE CONTINUALLY REMOVED WITH SAID SWEEPER. THIS SWEEPING SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO AR150520 MOBILIZATION.
- MATERIALS REMOVED FROM THE PROJECT WILL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS NOTED OTHERWISE.
- FOR WORK ON AIRPORT PROPERTY: PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO BARRICADES, SIGNING, RUNWAY CLOSED MARKERS, AIR OPERATIONS AREA (A.O.A.) LATHE AND RIBBON, ETC. SHALL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. BARRICADES AT 10-FOOT CENTERS WITH ONE ORANGE FLAG (24" x 24") BETWEEN EACH SET OF BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. BARRICADES SHALL BE WEIGHTED TO PREVENT BLOWING OVER. BARRICADES SHALL HAVE A FLASHING RED LIGHT AND CONFORM TO IDOT STANDARD 702001, TYPE II. BARRICADE INSTALLATION WILL BE REQUIRED PRIOR TO ACCESS TO THE A.O.A. BY CONTRACTOR'S WORKERS, EQUIPMENT OR MATERIAL. SIGNS SHALL BE PLACED AT EACH TAXIWAY/RUNWAY CLOSURE LOCATION AND SHALL BE ATTACHED TO THE BARRICADES. EACH BARRICADE LOCATION SHALL CONSIST OF ONE "DO NOT ENTER" SIGN AND ONE "AIRCRAFT MOVEMENT AREA" SIGN. SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR SHALL CONTACT THE AIRPORT MANAGER (5) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED DURING NON WORKING HOURS. THE CONTRACTOR SHALL PROVIDE A SIGN AT THE ACCESS GATE SAYING "AUTHORIZED PERSONNEL ONLY". THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE UPON LEAVING THE SITE. THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGES TO THE ACCESS ROAD, ACCESS GATE OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL COST RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACON LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION. SEE FLAG DETAIL, THIS SHEET.
- IN THE CASE OF AN EMERGENCY, CONTRACTOR SHALL NOTIFY AIRPORT MANAGER AND THE ENGINEER IMMEDIATELY.
- DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK.
- THE TALLEST PIECE OF CONSTRUCTION EQUIPMENT IS ANTICIPATED TO BE AN EXCAVATOR TRUCK WHICH HAS A MAXIMUM HEIGHT OF 25 FEET.
- IF RUNWAY NUMERALS ARE PRESENT DURING CONSTRUCTION THEN CONTRACTOR SHALL PLACE CLOSED RUNWAY MARKER OVER NUMERALS AS DETAILED, OTHERWISE PLACE RUNWAY CLOSED MARKER IN TURF AT ENDS OF RUNWAY AS DETAILED.
- DIXON MUNICIPAL AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT. COORDINATION OF WORK WITH THE AIRPORT IS MANDATORY SO AS TO MINIMIZE IMPACTS ON AIRPORT OPERATIONS.
- APPROXIMATE LOCATION OF HAUL ROUTES ON THE AIRPORT SITE ARE SHOWN ON THE GENERAL PROJECT LAYOUT AND THE PHASING PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE ROADS USED AS HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. THE BEFORE AND AFTER CONDITION OF ON-SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED AND DETERMINED BY THE CONTRACTOR AND THE ENGINEER. FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE ENGINEER PRIOR TO THE WORK. ALL ON-SITE ACCESS ROADS TO AIRPORT FACILITIES SHALL REMAIN OPEN AND MAINTAINED AT ALL TIMES.

- MOBILIZATION/EQUIPMENT STORAGE AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE AS SHOWN ON THE PLANS. THIS AREA SHALL BE RESTORED TO THE ORIGINAL CONDITION UPON COMPLETION OF THE PROJECT. THE RESTORATION SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- LOCATION OF KNOWN EXISTING AIRPORT UNDERGROUND CABLES ARE SHOWN ON THE PLANS AND MUST BE VERIFIED BY THE CONTRACTOR. REPAIR OF DAMAGED CABLE MUST BE STARTED IMMEDIATELY AND CONTINUED UNTIL COMPLETED. ALL SUCH REPAIRS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS, OR AS DIRECTED BY THE OWNER OF THE CABLE, AND SHALL BE AT THE CONTRACTOR'S EXPENSE. IF FAA CABLES ARE DAMAGED, REPAIRS SHALL BE DONE FROM POINT TO POINT IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF A FAA REPRESENTATIVE. THE OWNER MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS.
- COORDINATION MEETINGS - THE CONTRACTOR SHALL CONDUCT WEEKLY COORDINATION MEETINGS TO DISCUSS WORK AREAS AND SCHEDULING, ETC. WITH THE ENGINEER, AIRPORT OPERATIONS, FAA, AND OTHER APPROPRIATE OFFICIALS. MINUTES FROM THE WEEKLY MEETINGS SHALL BE PREPARED BY THE CONTRACTOR, FURNISHED TO ALL ATTENDEES PRIOR TO THE SUBSEQUENT MEETING, AND KEPT ON FILE AT THE FIELD OFFICE. THE COORDINATION MEETING COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- THE CONTRACTOR SHALL PROVIDE THE PHONE NUMBERS OF THREE PERSONNEL, INCLUDING THE PROJECT SUPERINTENDENT, WHO MAY BE CONTACTED IN AN EMERGENCY. PERSONNEL SHALL BE ON CALL 24 HOURS PER DAY FOR MAINTAINING AIRPORT HAZARD LIGHTING AND BARRICADES.
- DRAINAGE MODIFICATIONS SHALL BE SEQUENCED TO PROVIDE POSITIVE DRAINAGE AT ALL TIMES AT NO ADDITIONAL COST TO THE CONTRACT.
- VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN 44.5' FROM ACTIVE TAXIWAYS AND 125' FROM ACTIVE RUNWAYS UNLESS OTHERWISE APPROVED BY THE AIRPORT MANAGER.
- CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A MANNER AS NOT TO VIOLATE FEDERAL AVIATION ADMINISTRATION PART 77 SURFACES OR RUNWAY AND TAXIWAY SAFETY AREAS.
- ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER ELECTRICAL CABLES SHALL REMAIN IN SERVICE AT ALL TIMES. ALL EXISTING LIGHTING AND VAULT EQUIPMENT SHALL REMAIN IN SERVICE UNTIL PROPOSED IMPROVEMENTS ARE INSTALLED AND OPERATIONAL, UNLESS OTHERWISE APPROVED BY THE ENGINEER. ANY CABLES DAMAGED BY THE CONTRACTOR SHALL BE IMMEDIATELY REPAIRED AT HIS EXPENSE.
- COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. CONTRACTOR IS REFERRED TO SECTION 50-17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER OR THE DESIGN ENGINEER ASSUME ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED THAT THE LOCATIONS, SIZE AND TYPE MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE RESIDENT ENGINEER AND THE AIRPORT MANAGER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER.
- ALL AIRFIELD LIGHTING AND LIGHTING GUIDANCE SYSTEMS (NAVAIDS) LOCATED WITHIN AND IMMEDIATELY ADJACENT TO THE CONTRACTOR'S WORK ZONE SHALL BE CHECKED FOR OPERATIONAL CONDITION PRIOR TO THE DEPARTURE FROM THE AIRPORT WITH THE AIRPORT MANAGER AND/OR AIRPORT MAINTENANCE. ANY DEFICIENCIES IN THESE SYSTEMS DUE TO THE ACTS OF CONTRACTOR OR HIS SUBCONTRACTORS, SUPPLIERS OR CONSULTANTS SHALL BE REPAIRED IMMEDIATELY.

**DESIGN AIRCRAFT APPROACH CATEGORY: B**  
**DESIGN AIRPORT GROUP: I**  
 RUNWAY 12/30 OBSTACLE FREE ZONE TOTAL WIDTH = 250'  
 RUNWAY 8/26 OBSTACLE FREE ZONE TOTAL WIDTH = 250'  
 TAXIWAY CENTERLINE TO OBJECT SEPARATION = 44.5'  
 TAXIWAY CENTERLINE TO OBJECT SEPARATION = 39.5'

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE SPECIAL PROVISIONS SECTION 30-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS.



**CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL ORANGE AND WHITE CHECKERED FLAG**  
 NOT TO SCALE

IL. CONTRACT: **DI027**  
 IL. LETTING ITEM: **8A**  
 IL. PROJECT: **C73-4370**  
 S.B.G. PROJECT: **3-17-SBGP-99/105**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34x22).

**DIXON MUNICIPAL AIRPORT  
 DIXON, ILLINOIS**  
**INSTALL PERIMETER FENCE ON NORTH PROPERTY LINE**

**SEQUENCE OF CONSTRUCTION  
 GENERAL NOTES AND DETAILS**



DESIGN BY:	RMD
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	12/01/15
JOB No:	12244-02

**FINAL**

NOTE - ALL PHASES  
 ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER. ALL TEMPORARY CABLING AND SPlicing NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.

DATE: Friday, December 4, 2015 11:05:27 AM  
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 LAYOUT: LAYOUT1  
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 CMT\_block\_glb.jpg  
 bwp\_desc-on-fencing  
 bwp\_title-dix-on-fencing

**NOTES**

1. ALL LENGTHS SHOWN FOR FENCE REMOVALS OR NEW FENCE ARE FOR THE SECTIONS SHOWN ON THIS SHEET ONLY.
2. SHOULD THE AIRPORT NOT WANT THE REMOVED MATERIALS, ALL FENCING MATERIAL REMOVED SHALL BE DISPOSED OF OFF AIRPORT PROPERTY.
3. HOLES FROM REMOVED POSTS SHALL BE BACKFILLED, COMPACTED AND SEEDED. RESTORATION COSTS SHALL BE INCLUDED IN THE COSTS TO REMOVE EXISTING FENCE.
4. NEW FENCE SHALL BE INSTALLED NO CLOSER THAN 2' FROM THE PROPERTY LINE UNLESS OTHERWISE SHOWN.

**KEYED POINTS (FENCING PLAN 1)**

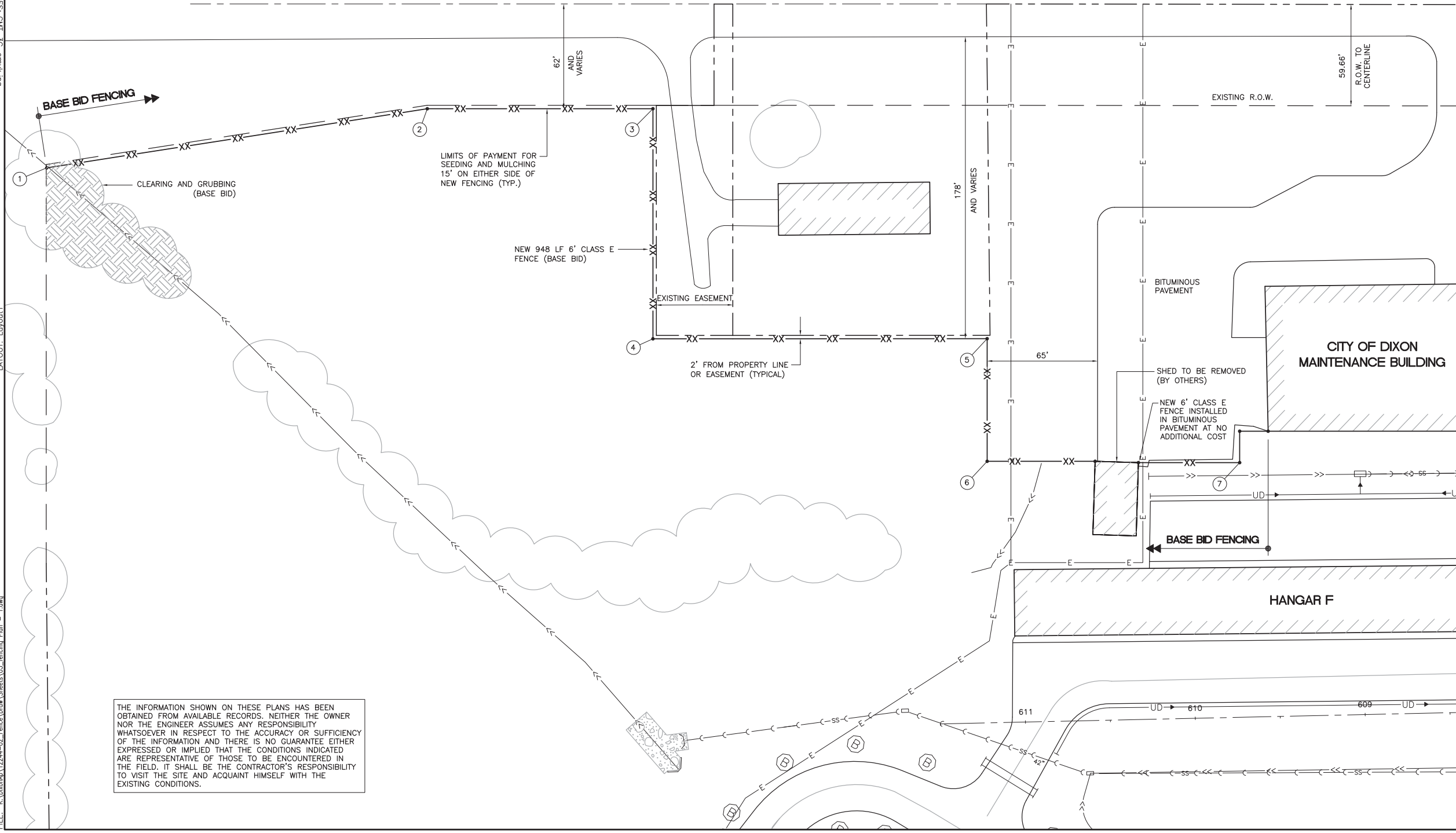
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2	1884164.4927	2492054.8918
3	1884164.4969	2492187.7174
4	1884029.0970	2492187.7174
5	1884029.1703	2492384.4382
6	1883957.0061	2492384.4382
7	1883956.2334	2492533.0786

**LEGEND**

- XX — NEW 6' CLASS E FENCE
- [Symbol] — NEW ELECTRIC/MANUAL VEHICLE GATE TO BE INSTALLED
- [Symbol] — NEW PEDESTRIAN GATE TO BE INSTALLED
- x — EXISTING FENCE
- [Symbol] — EXISTING RIGHT-OF-WAY (R.O.W.)
- [Symbol] — EXISTING AIRPORT PROPERTY LINE
- E — EXISTING BURIED ELECTRICAL
- W — EXISTING WATER MAIN
- [Hatched Box] CLEARING AND GRUBBING
- G — EXISTING GAS MAIN
- SS — EXISTING STORM SEWER
- UD — EXISTING UNDERDRAIN
- T — EXISTING TELEPHONE LINE
- OH/E — EXISTING OVERHEAD ELECTRIC LINE
- [Symbol] — EXISTING LIGHT POLE
- >> — EXISTING DITCH LINE
- (B) — EXISTING RUNWAY REFLECTOR



FRANKLIN GROVE ROAD

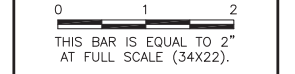


THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

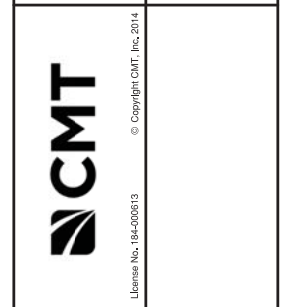
IL CONTRACT: **DI027**  
 IL LETTING ITEM: **8A**  
 IL PROJECT: **C73-4370**  
 S.B.G. PROJECT: **3-17-SBGP-99/105**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



**DIXON MUNICIPAL AIRPORT  
 DIXON, ILLINOIS**  
**INSTALL PERIMETER FENCE ON NORTH PROPERTY LINE  
 FENCING PLAN - 1  
 (BASE BID)**



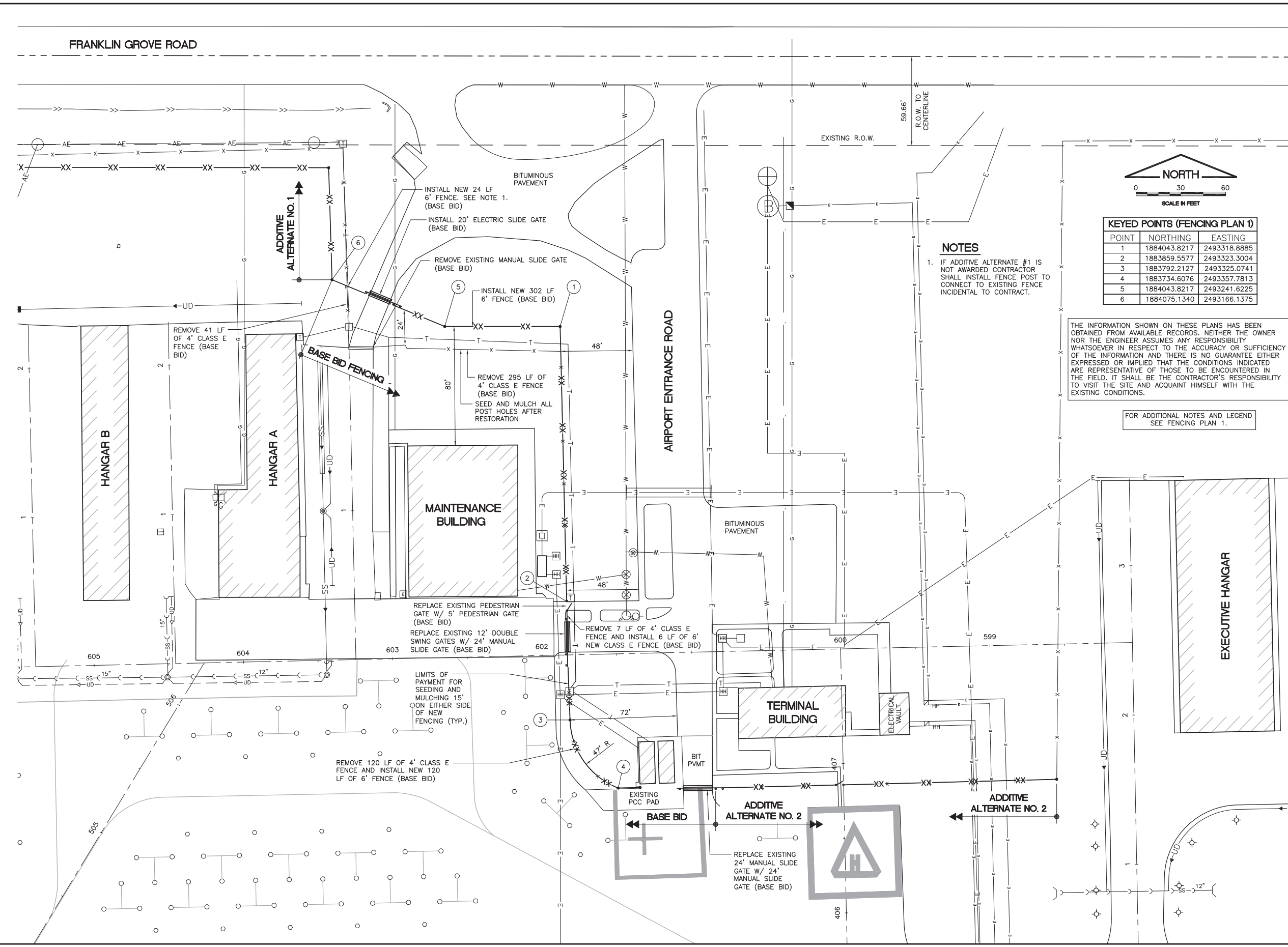
DESIGN BY: RMD  
 DRAWN BY: JRO  
 CHECKED BY: DKP  
 APPROVED BY: DLP  
 DATE: 12/01/15  
 JOB No: 12244-02

**FINAL**

SHEET 5 OF 13 SHEETS



DATE: Friday, December 4, 2015 11:05:51 AM  
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 UPDATE BY: Ryan Drea  
 LAYOUT: Layout1  
 IMAGE FILES: CMT\_3C\_cmyk.jpg  
 CMT\_block\_rgb.jpg  
 bwp\_desc-dwg - bwp\_desc-dwg  
 bwp\_desc-dwg - bwp\_desc-dwg



**KEYED POINTS (FENCING PLAN 1)**

POINT	NORTHING	EASTING
1	1884043.8217	2493318.8885
2	1883859.5577	2493323.3004
3	1883792.2127	2493325.0741
4	1883734.6076	2493357.7813
5	1884043.8217	2493241.6225
6	1884075.1340	2493166.1375

**NOTES**

1. IF ADDITIVE ALTERNATE #1 IS NOT AWARDED CONTRACTOR SHALL INSTALL FENCE POST TO CONNECT TO EXISTING FENCE INCIDENTAL TO CONTRACT.

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

FOR ADDITIONAL NOTES AND LEGEND SEE FENCING PLAN 1.

IL CONTRACT: **DI027**  
 IL LETTING ITEM: **8A**  
 IL PROJECT: **C73-4370**  
 S.B.G. PROJECT: **3-17-SBGP-99/105**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**DIXON MUNICIPAL AIRPORT  
 DIXON, ILLINOIS**

**INSTALL PERIMETER FENCE ON NORTH PROPERTY LINE**

**FENCING PLAN - 2  
 (BASE BID)**

**CMT**  
1010097891/CMT/18C-2014

DESIGN BY:	RMD
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	12/01/15
JOB No:	12244-02

**FINAL**

SHEET 6 OF 13 SHEETS

DATE: Friday, December 4, 2015 11:06:16 AM  
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 UPDATE BY: Ryan Drea  
 LAYOUT: Layout1  
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 CMT\_block\_glb.jpg  
 REF: DWG: Fencing - 1.tbl  
 D:\xon-dwg-base.dwg

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

FOR ADDITIONAL NOTES AND LEGEND SEE FENCING PLAN 1.

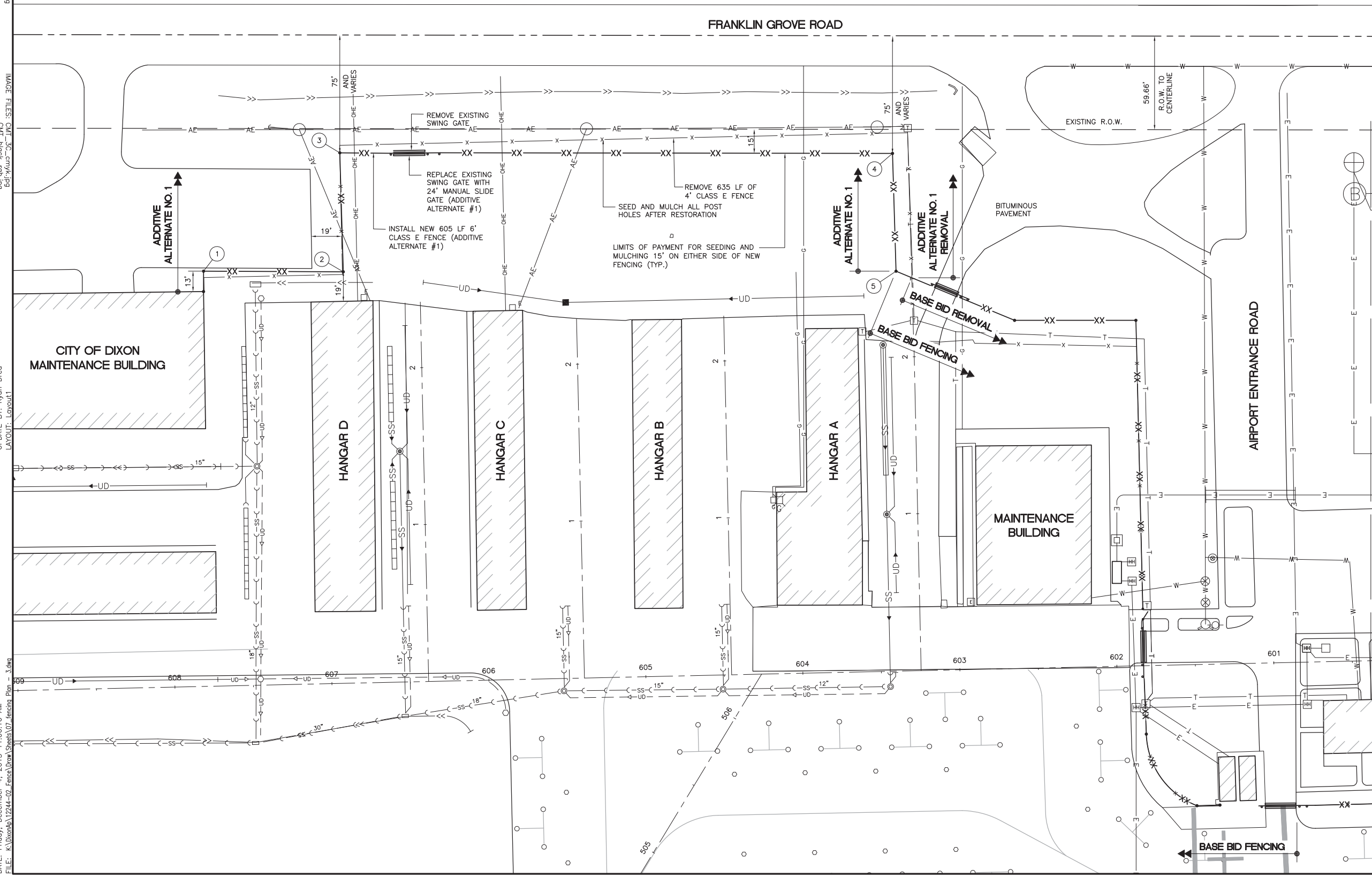
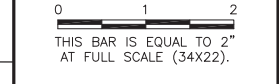
KEYED POINTS (FENCING PLAN 3)		
POINT	NORTHING	EASTING
1	1884075.1464	2492725.1222
2	1884074.7366	2492814.1188
3	1884150.5354	2492811.9105
4	1884150.1715	2493163.8863
5	1884075.1340	2493166.1375



IL. CONTRACT: **DI027**  
 IL. LETTING ITEM: **8A**  
 IL. PROJECT: **C73-4370**  
 S.B.G. PROJECT: **3-17-SBGP-99/105**

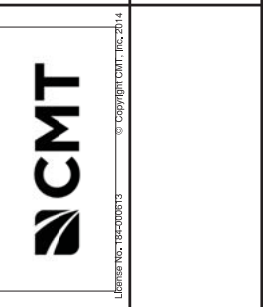
SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



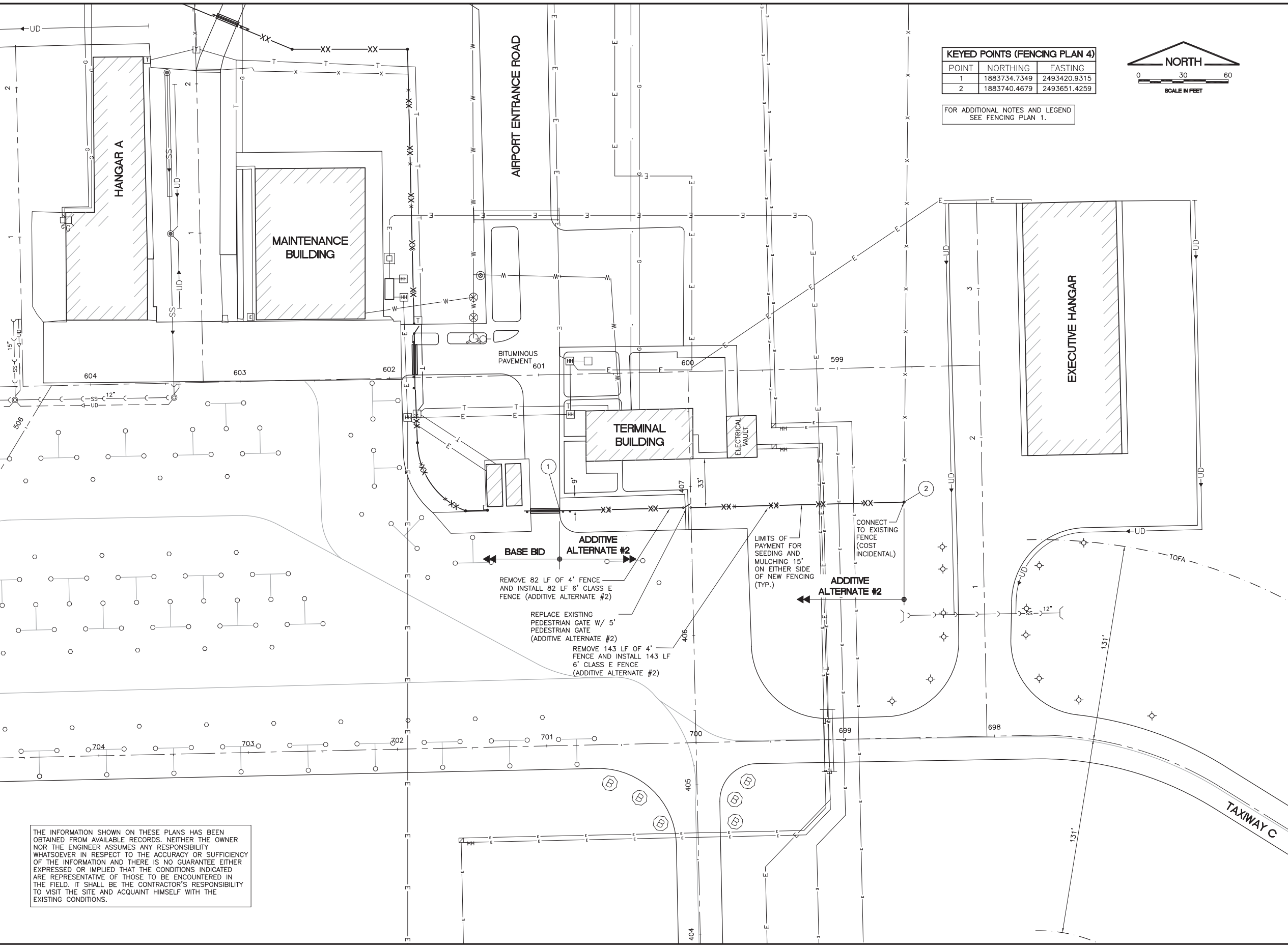
**DIXON MUNICIPAL AIRPORT  
 DIXON, ILLINOIS**

**INSTALL PERIMETER FENCE ON NORTH PROPERTY LINE  
 FENCING PLAN - 3  
 (ADDITIVE ALTERNATE #1)**



DESIGN BY:	RMD
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	12/01/15
JOB No:	12244-02
<b>FINAL</b>	
SHEET 7 OF 13 SHEETS	

DATE: Friday, December 4, 2015 11:06:41 AM  
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 UPDATE BY: Ryan Drea  
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 REF: DWG: Fencing  
 D:\Dixon\12244-02\_Fence\Draw\Sheets\08\_Fencing Plan - 4.dwg



**KEYED POINTS (FENCING PLAN 4)**

POINT	NORTHING	EASTING
1	1883734.7349	2493420.9315
2	1883740.4679	2493651.4259

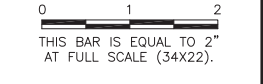
FOR ADDITIONAL NOTES AND LEGEND SEE FENCING PLAN 1.



IL. CONTRACT: **DI027**  
 IL. LETTING ITEM: **8A**  
 IL. PROJECT: **C73-4370**  
 S.B.G. PROJECT: **3-17-SBGP-99/105**

SURVEY BOOK # BOOK #

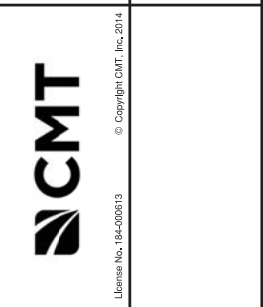
REVISIONS		
NUMBER	BY	DATE



**DIXON MUNICIPAL AIRPORT  
 DIXON, ILLINOIS**

**INSTALL PERIMETER FENCE ON NORTH PROPERTY LINE**

**FENCING PLAN - 4  
 (ADDITIVE ALTERNATE #2)**



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DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	12/01/15
JOB No:	12244-02

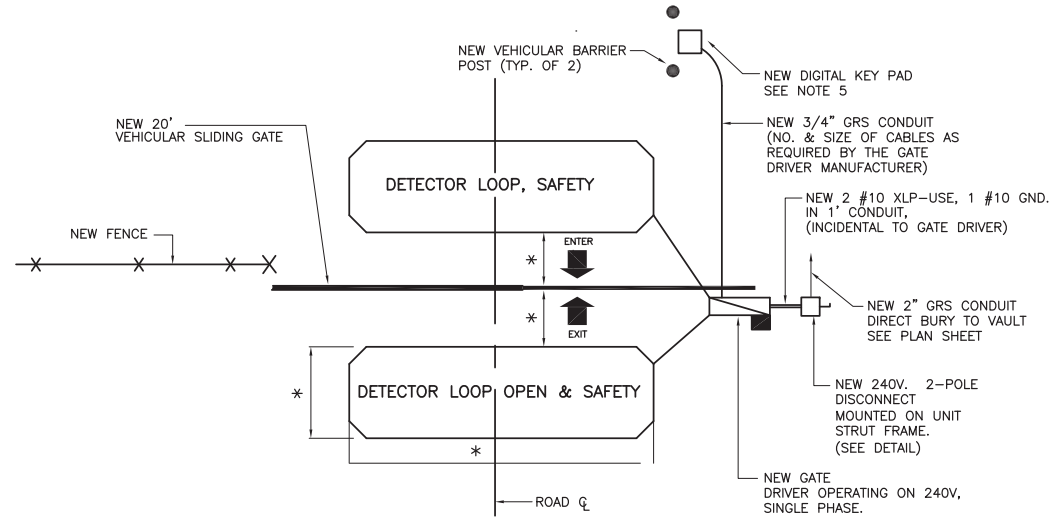
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SHEET 8 OF 13 SHEETS

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REF: DWG: Fencing - Detail.dwg  
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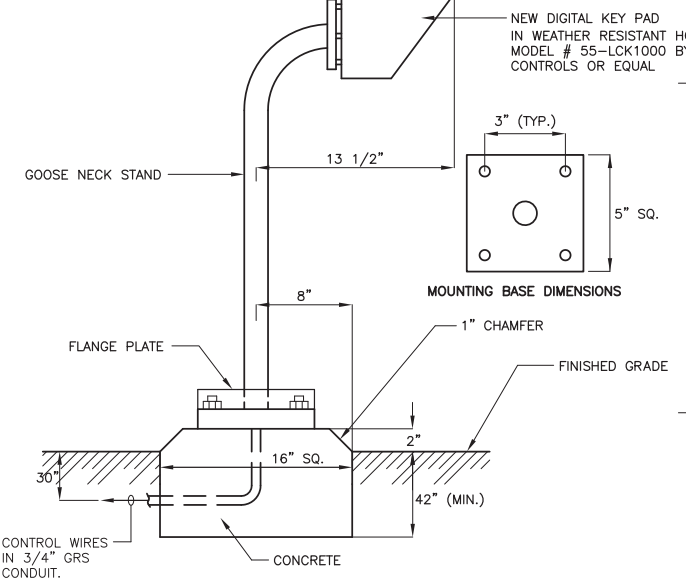


**NEW KEY PAD OPERATED GATE AND DETECTOR LOOP LAYOUT**

NOT TO SCALE  
 \* PER MANUFACTURERS RECOMMENDATION CONTRACTOR SHALL COORDINATE THIS WORK WITH ENGINEER.

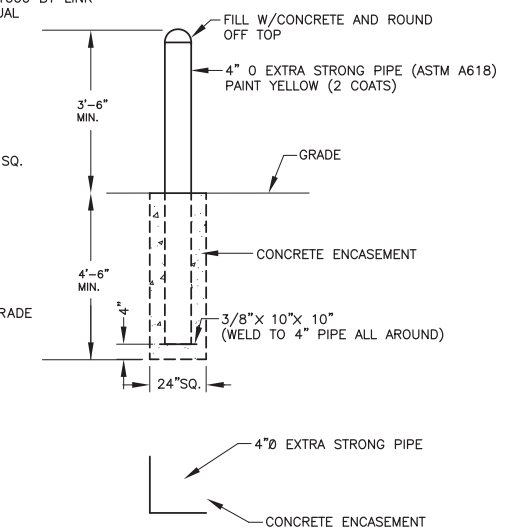
**NOTES:**

1. THE LOCATION OF THE NEW KEY PAD, OPERATED GATE DRIVER, DISCONNECT, KEY PAD, AND DETECTOR LOOPS ARE FOR INFORMATION ONLY AND SHALL BE FIELD ADJUSTED PER THE MANUFACTURER RECOMMENDATION.
2. THE MINIMUM BURIAL DEPTH FOR GRS CONDUIT IS 30" BELOW FINISHED GRADE.
3. NO DIRECT BURIED CABLE WILL BE ALLOWED IN THE INSTALLATION OF THE NEW KEY PAD OPERATED GATE DRIVER.
4. NEW KEY PAD SHALL BE MODEL # 55-LCK1000 BY LINK CONTROLS OR EQUAL.



**DIGITAL KEY PAD MOUNTING DETAIL**

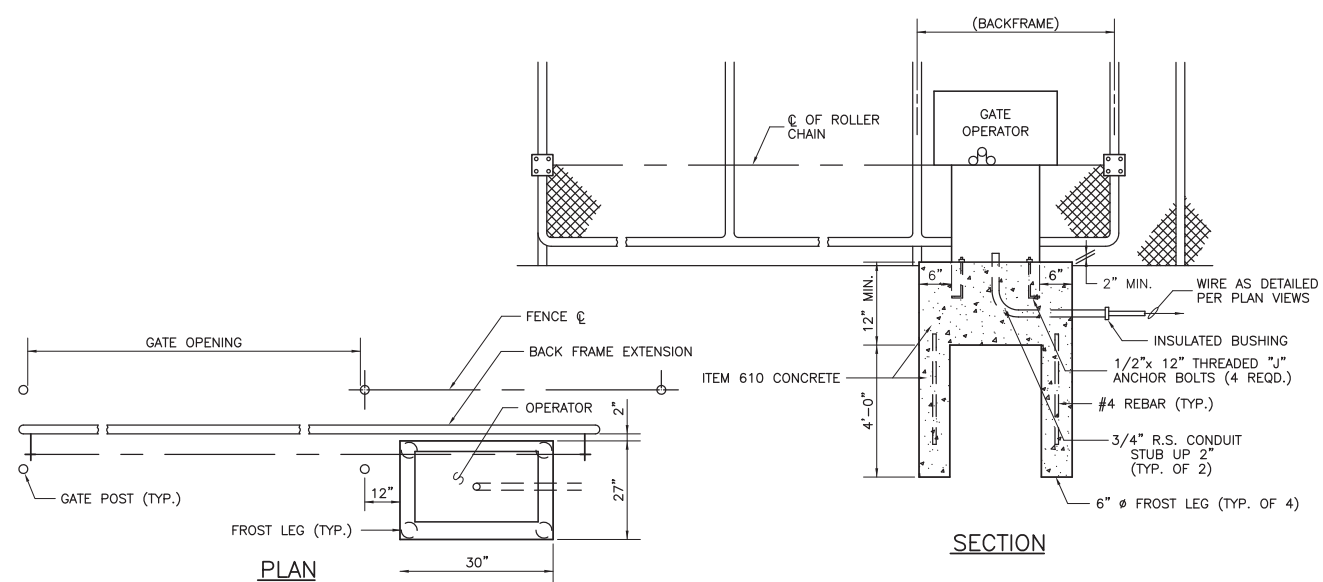
LOCATIONS, DETAILS AND CHARACTER OF EQUIPMENT SHOWN ON THIS SHEET ARE GENERIC. EQUIPMENT LOCATIONS SHALL BE AS RECOMMENDED BY THE EQUIPMENT MANUFACTURER.



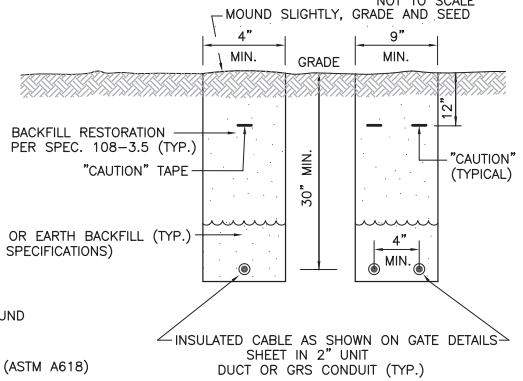
**VEHICULAR BARRIER DETAIL**

**NOTES:**

1. THE VEHICULAR BARRIERS WILL NOT BE MEASURED SEPARATELY FOR PAYMENT BUT SHALL BE CONSIDERED INCIDENTAL TO ELECTRIC GATE.
2. LOCATION OF UNDERGROUND ELECTRICAL ITEMS SHALL BE COORDINATED WITH VEHICULAR BARRIERS TO AVOID ANY CONFLICTS.



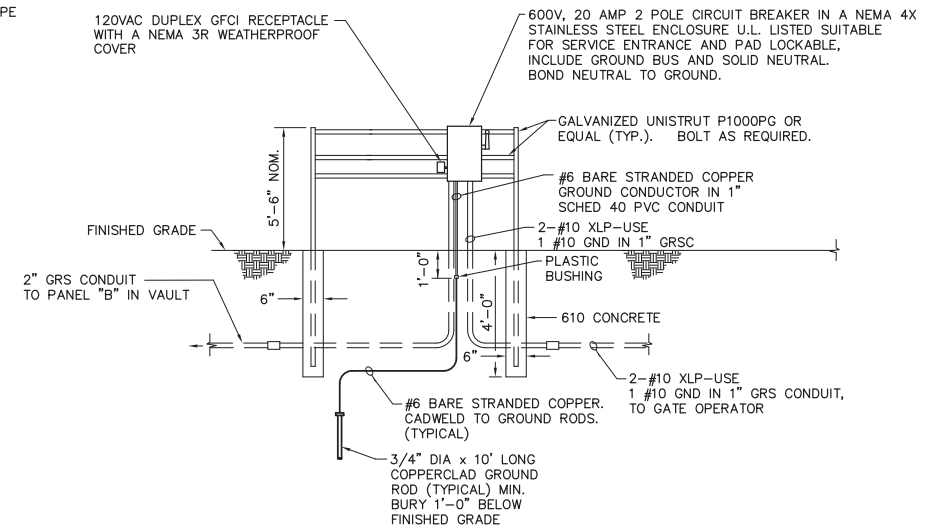
**GATE OPERATOR DETAIL**



**TURF AREA CABLE TRENCH DETAIL**

**NOTES:**

1. TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 4" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
2. DEPTH OF TRENCHES SHALL BE AS SHOWN UNLESS OTHERWISE SPECIFIED ON THE PLANS.
3. SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
4. ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL.



**SERVICE ENTRANCE AND DISCONNECT**

**GENERAL NOTES:**

1. CANTILEVERED GATE SHALL BE SUFFICIENTLY RIGID TO WITHSTAND FLEXING OR BENDING DURING WINDY CONDITIONS. CONTRACTOR SHALL PROVIDE STIFFENERS, STRUCTURAL SHAPES IN EXCESS OF THE MINIMUM SPECIFIED DIMENSIONS OR ADDITIONAL ROLLERS AND POSTS SUFFICIENT TO PREVENT DISPLACEMENT OF THE GATE BY WIND OR BY UNAUTHORIZED PERSONNEL.
2. CONTRACTOR SHALL PROVIDE AND INSTALL GATE AS A COMPLETE WORKING UNIT. THE GATE WORK SHALL INCLUDE, BUT NOT BE LIMITED TO THE GATE, GATE OPERATOR AND FOUNDATION, AND POWER CABLES CONDUIT, CARD, READER, TRENCHING, CIRCUIT BREAKERS, VEHICULAR BARRIERS AND ALL CONNECTIONS, LABOR AND MATERIALS NECESSARY TO COMPLETE OPERATION.
3. LOCATION OF THE GATE OPERATOR SHALL BE AS RECOMMENDED BY THE MANUFACTURER.
4. THE FABRIC TYPE AND FINISH OF THE GATE SHALL MATCH WITH THE PROPOSED FENCE OR AS DIRECTED BY THE ENGINEER.
5. ALL PROPOSED SLIDING VEHICULAR GATES SHALL HAVE ENCLOSED TRUCK ASSEMBLIES, TYMETAL OR EQUAL.

IL. CONTRACT: **DI027**  
 IL. LETTING ITEM: **8A**  
 IL. PROJECT: **C73-4370**  
 S.B.G. PROJECT: **3-17-SBGP-99/105**

REVISIONS		
NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34x22).

**DIXON MUNICIPAL AIRPORT  
 DIXON, ILLINOIS**  
**INSTALL PERIMETER FENCE ON NORTH PROPERTY LINE**

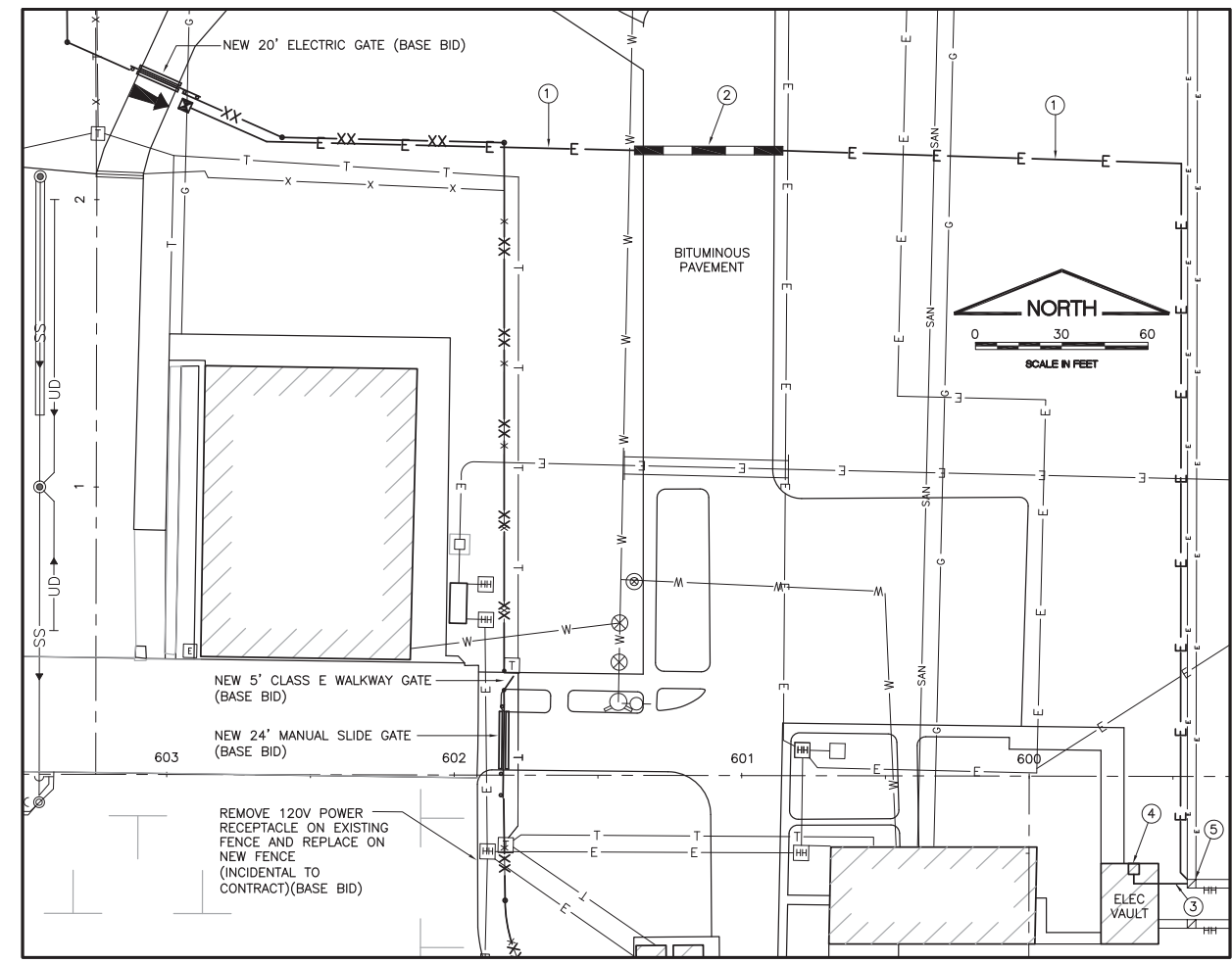
**ELECTRIC GATE DETAILS**



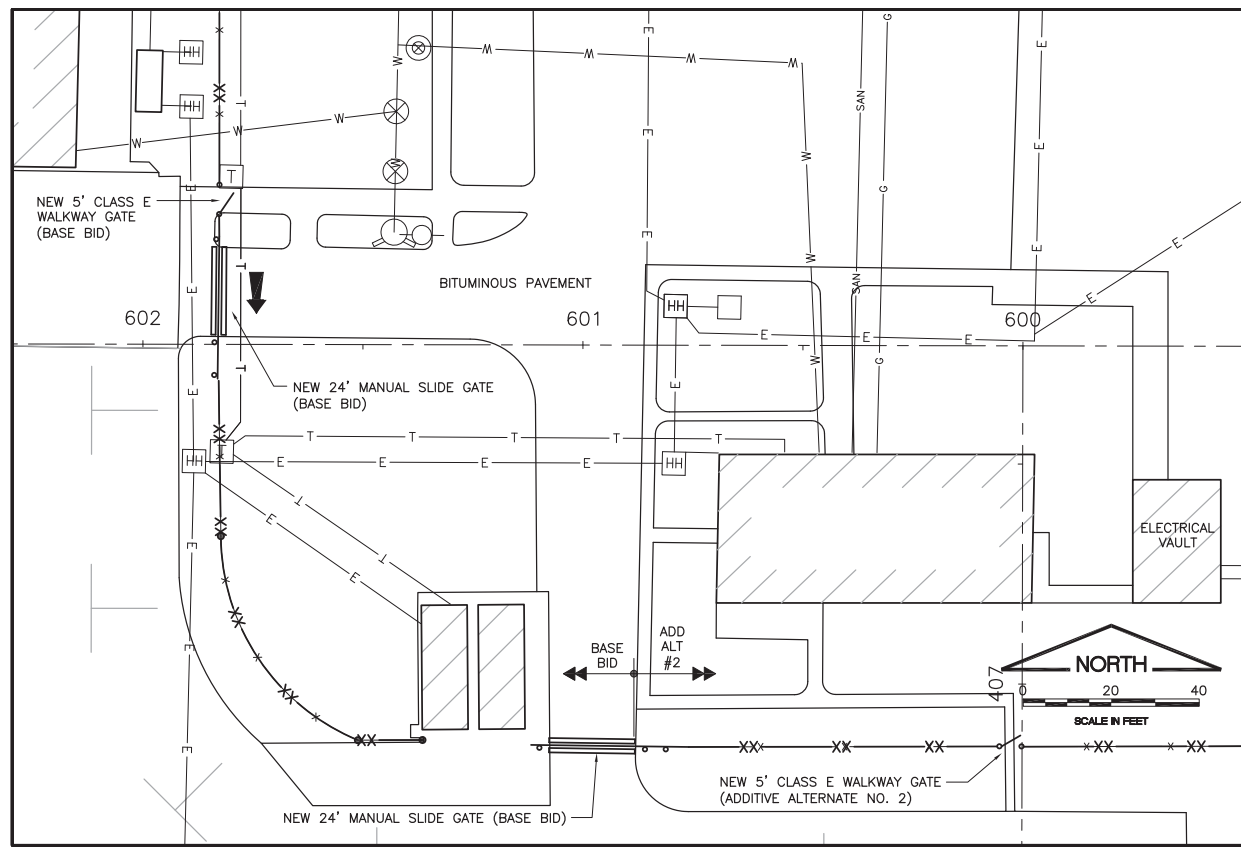
DESIGN BY:	RMD
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	12/01/15
JOB No:	12244-02

**FINAL**

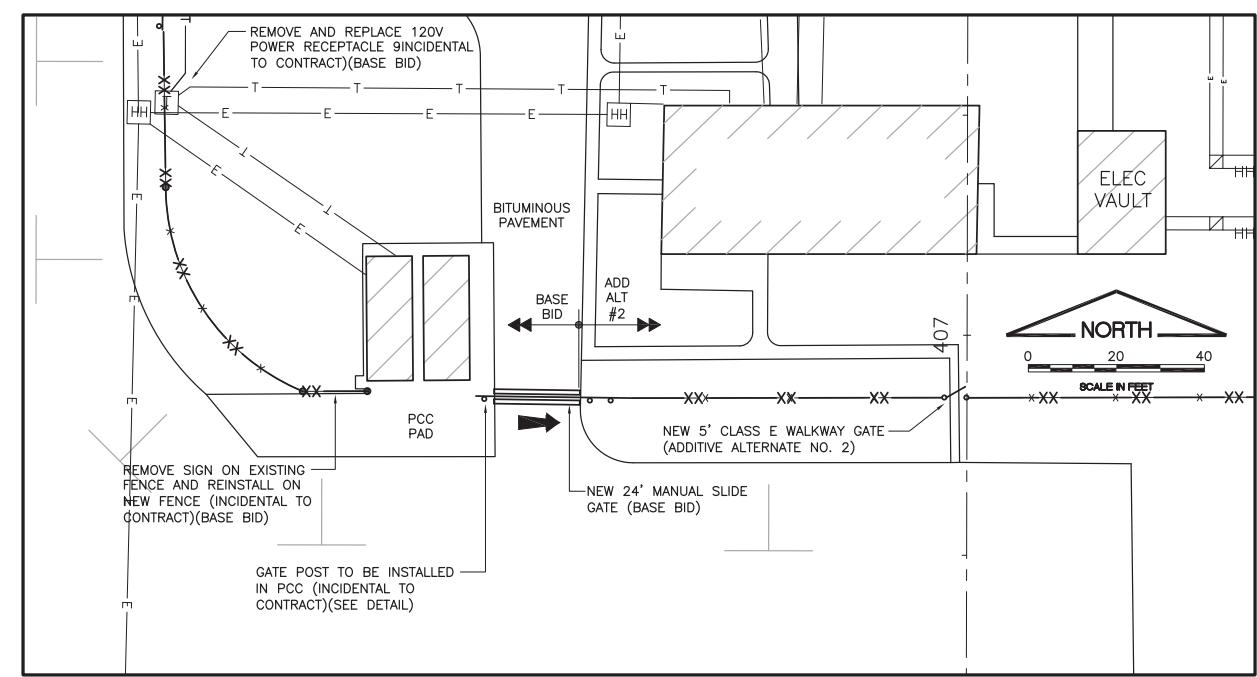
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 UPDATE BY: Ryan Drea  
 LAYOUT: LAYOUT1  
 REF: DWG: Fence - Detail.dwg  
 IMAGE FILES: CMT\_3C\_cmyk.jpg  
 CMT\_block\_glb.jpg  
 bwp-base.dwg



**20' ELECTRIC GATE DETAIL**



**24' MANUAL SLIDE GATE DETAIL**



**24' MANUAL SLIDE GATE DETAIL**

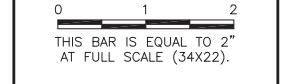
**KEYED NOTES**

- ① NEW 3 #6 XLP-USE, 1 #6 GND. IN 2" GRSC CONDUIT IN TRENCH.
  - ② NEW 3 #6 XLP-USE, 1 #6 GND. IN 2" GRSC CONDUIT, JACKED.
  - ③ NEW 3 #6 XLP-USE, 1 #6 GND. IN EXISTING CONDUIT.
  - ④ EXISTING PANEL "B" INSTALL (1) 20A, 2-POLE CIRCUIT BREAKER AND CONDUIT FROM WIREWAY. (COST INCIDENTAL TO CONTRACT)
  - ⑤ EXISTING LOW VOLTAGE HANDHOLE. CORE HOLE TO CONNECT NEW CONDUIT. COST SHALL BE INCIDENTAL TO CONDUIT
- ➔ DIRECTION OF OPENING SLIDE GATE

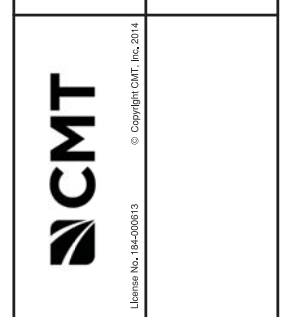
IL. CONTRACT: **DI027**  
 IL. LETTING ITEM: **8A**  
 IL. PROJECT: **C73-4370**  
 S.B.G. PROJECT: **3-17-SBGP-99/105**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



**DIXON MUNICIPAL AIRPORT  
 DIXON, ILLINOIS**  
**INSTALL PERIMETER FENCE ON NORTH PROPERTY LINE**  
**GATE DETAILS**

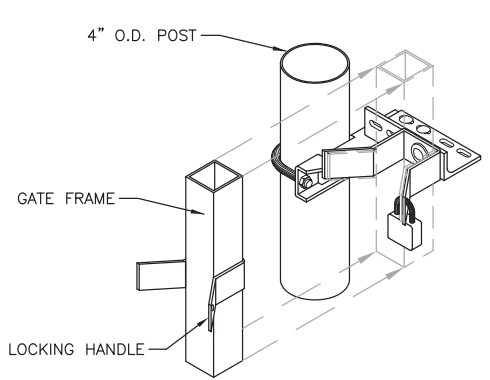


DESIGN BY:	RMD
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	12/01/15
JOB No:	12244-02

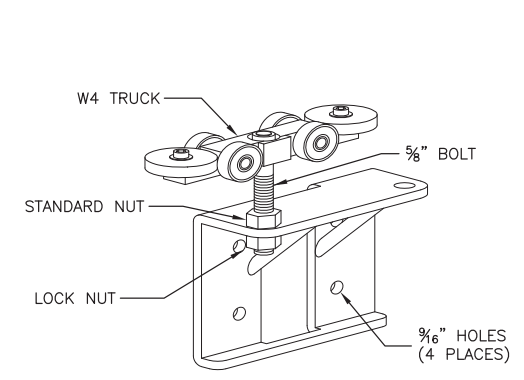
FINAL

SHEET 10 OF 13 SHEETS

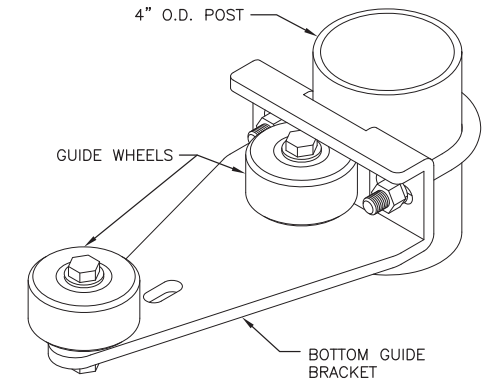
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 CMT\_3C\_cmyk.jpg  
 CMT\_block.jpg



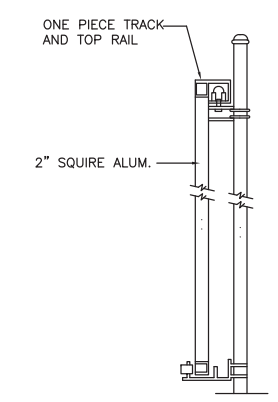
**CATCH ASSEMBLY DETAIL**  
N.T.S.



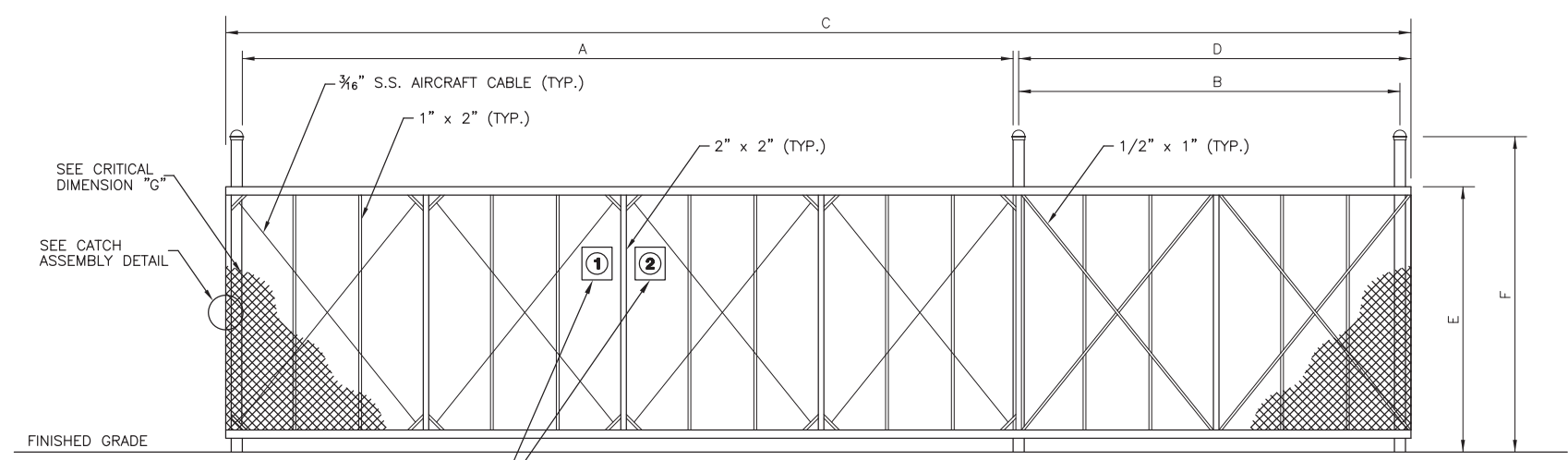
**GATE HANGER ASSEMBLY**  
N.T.S.



**STANDARD BOTTOM GUIDE ASSEMBLY**  
N.T.S.

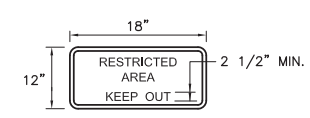


**GATE FRAME SECTION**  
N.T.S.



**ELEVATION**  
**CANTILEVER SLIDE GATE**  
N.T.S.

CRITICAL DIMENSIONS			
A	CLEAR OPENING	20'-0"	24'-0"
B	COUNTER BALANCE POST SPACING, CENTER TO CENTER	10'-1"	11'-1"
C	OVERALL GATE LENGTH	30'-0"	36'-0"
D	COUNTERBALANCE LENGTH	10'-0"	12'-0"
E	NOMINAL GATE HEIGHT	6'-0"	6'-0"
F	POST HEIGHT	7'-6"	7'-6"
G	FABRIC HEIGHT	5'-0"	5'-0"



- NOTES:**
- EACH PEDESTRIAN OR VEHICLE SWING GATE SHALL REQUIRE ONE "RESTRICTED" SIGN. EVERY 100' OF FENCE SHALL REQUIRE ONE "RESTRICTED" SIGN, COST INCIDENTAL TO FENCE.
  - 0.08 GA ALUMINUM ALLOY SHEET. LETTERING SHALL BE RED ON A WHITE BACKGROUND.

**SIGN DETAILS**



**WARNING SIGN DETAIL**

**UL 235 COMPLIANCE NOTES**

GATE INSTALLATION SHALL COMPLY WITH ALL REQUIREMENTS OF UL 235, INCLUDING, BUT NOT LIMITED TO:

- ALL OPENINGS OF THE SLIDE GATE ARE GUARDED OR SCREENED FROM THE BOTTOM OF THE GATE TO A MINIMUM OF 4 FEET ABOVE GROUND TO PREVENT A 2-1/4" DIAMETER SPHERE FROM PASSING THROUGH THE OPENINGS ANYWHERE IN THE GATE, AND IN THAT PORTION OF THE ADJACENT FENCE THAT THE GATE COVERS IN THE OPEN POSITION.
- ALL EXPOSED PINCH POINTS ARE ELIMINATED OR GUARDED AND GUARDING IS SUPPLIED FOR ALL EXPOSED ROLLERS.
- A WARNING SIGN (SEE DETAIL, THIS SHEET) MUST BE AFFIXED TO BOTH SIDES OF THE SLIDE GATE, BOTH MANUAL AND ELECTRIC.
- FOR ADDITIONAL UL 235 REQUIREMENTS FOR THIS GATE INSTALLATION, SEE ELECTRIC GATE DETAILS SHEET.

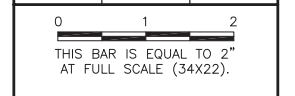
LOCATIONS, DETAILS AND CHARACTER OF EQUIPMENT SHOWN ON THIS SHEET ARE GENERIC. EQUIPMENT LOCATIONS SHALL BE AS RECOMMENDED BY THE EQUIPMENT MANUFACTURER.

SEE SHEET 9 - ELECTRIC GATE DETAILS FOR SLIDE GATE NOTES.

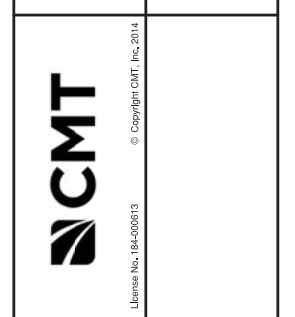
IL. CONTRACT: **DI027**  
 IL. LETTING ITEM: **8A**  
 IL. PROJECT: **C73-4370**  
 S.B.G. PROJECT: **3-17-SBGP-99/105**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



**DIXON MUNICIPAL AIRPORT**  
**DIXON, ILLINOIS**  
**INSTALL PERIMETER FENCE ON NORTH PROPERTY LINE**  
**CANTILEVER GATE DETAILS**

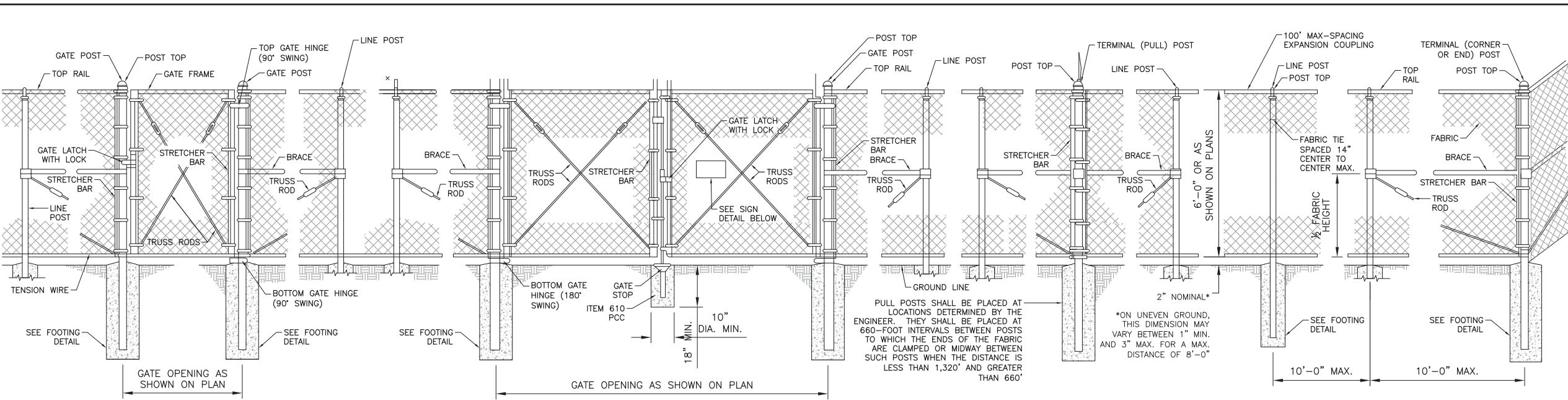


DESIGN BY: RMD  
 DRAWN BY: JRO  
 CHECKED BY: DKP  
 APPROVED BY: DLP  
 DATE: 12/01/15  
 JOB No: 12244-02

**FINAL**  
 SHEET 11 OF 13 SHEETS



REF: DWG: Fencing - Detail.dwg  
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 LAYOUT: Layout1  
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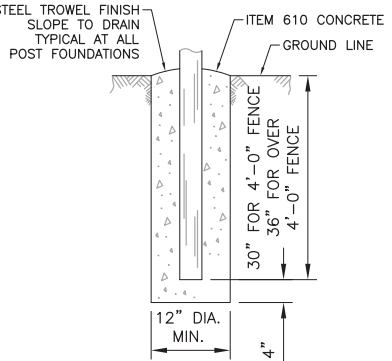
**PEDESTRIAN GATE ARRANGEMENT**

**VEHICLE GATE ARRANGEMENT**

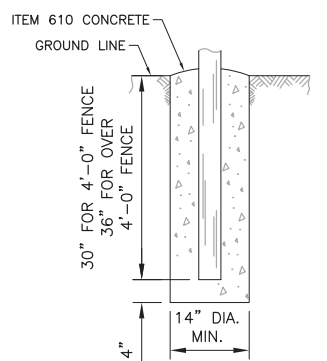
**PULL POST ARRANGEMENT**

**LINE POST ARRANGEMENT**

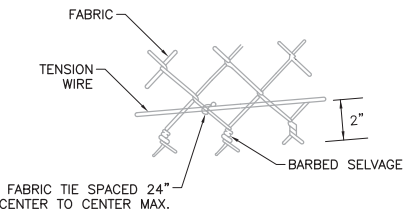
**CORNER OR END POST ARRANGEMENT**



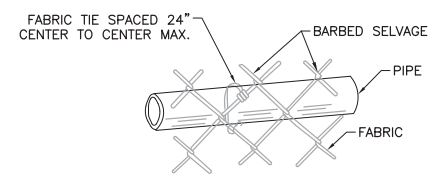
**FOOTING FOR LINE POST**



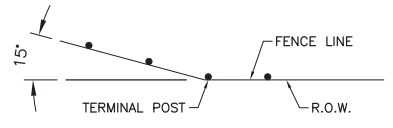
**FOOTING FOR GATE & TERMINAL POST**



**METHOD OF TYING FABRIC TO TENSION WIRE**

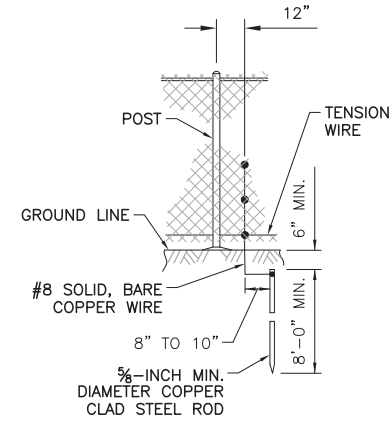


**METHOD OF TYING FABRIC TO PIPE**

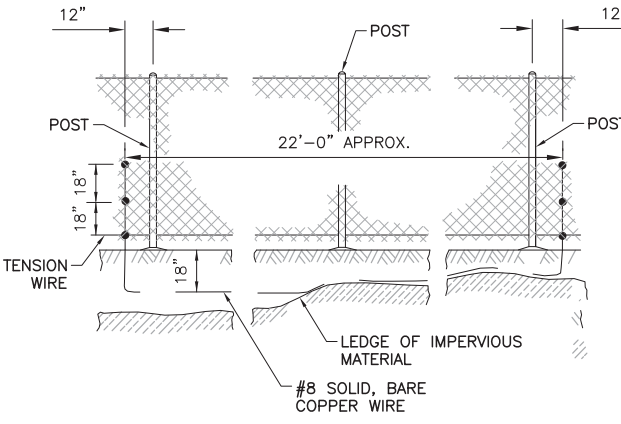


**NOTES:**  
 WHEN THE FENCE LINE HAS A CHANGE IN DIRECTION OF 15° OR MORE, A TERMINAL POST SHALL BE PLACED AS SHOWN ABOVE.  
 WHERE THE ANGLE IS LESS THAN 15° AND EXISTING CONDITIONS REQUIRE A TERMINAL POST, THEY SHALL BE PLACED AS DIRECTED BY THE ENGINEER.

**INSTALLATION AT CORNERS**



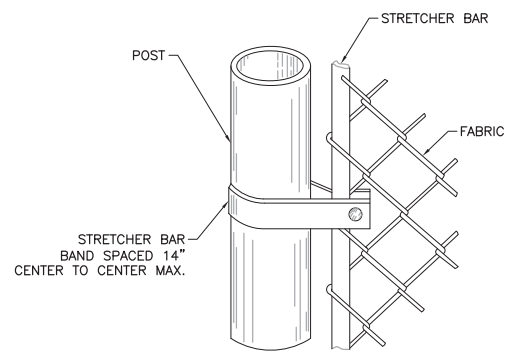
**STANDARD GROUND**



**COUNTERPOISE GROUND (ALTERNATE)**

**PROTECTIVE ELECTRICAL GROUNDS**

- ELECTRICAL GROUNDING NOTES:**
1. CONTINUOUS FENCE SHALL BE GROUNDED AT INTERVALS NOT EXCEEDING 1000' EXCEPT THERE SHALL BE A GROUND NOT EXCEEDING 100 FT. FROM A GATE IN EACH SECTION OF THE FENCE ADJACENT TO THE GATE.
  2. FENCE UNDER POWER LINE SHALL BE GROUNDED BY THREE GROUNDS, ONE DIRECTLY UNDER THE CROSSING AND ONE ON EACH SIDE 25 TO 50 FT. AWAY. A SINGLE GROUND SHALL BE LOCATED DIRECTLY UNDER EACH TELEPHONE WIRE OR CABLE CROSSING.
  3. THE COUNTERPOISE SHALL BE USED ONLY WHERE IT IS IMPOSSIBLE TO DRIVE A GROUND ROD BECAUSE OF AN IMPERVIOUS EARTH STRUCTURES.
  4. THE GROUND WIRE SHALL BE CONNECTED TO THE FABRIC AND THE GROUND ROD BY A MECHANICAL CLAMP OF CAST BRONZE BODY AND BRONZE OR STAINLESS STEEL BOLTS AND WASHERS. WHEN A TENSION WIRE IS REQUIRED, THE BOTTOM CONNECTION OF THE GROUND WIRE SHALL BE MADE TO THE TENSION WIRE.

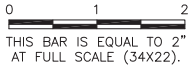


**METHOD OF FASTENING STRETCHER BAR TO POST**

IL. CONTRACT: **DI027**  
 IL. LETTING ITEM: **8A**  
 IL. PROJECT: **C73-4370**  
 S.B.G. PROJECT: **3-17-SBGP-99/105**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



**DIXON MUNICIPAL AIRPORT  
 DIXON, ILLINOIS**  
**INSTALL PERIMETER FENCE ON NORTH PROPERTY LINE**

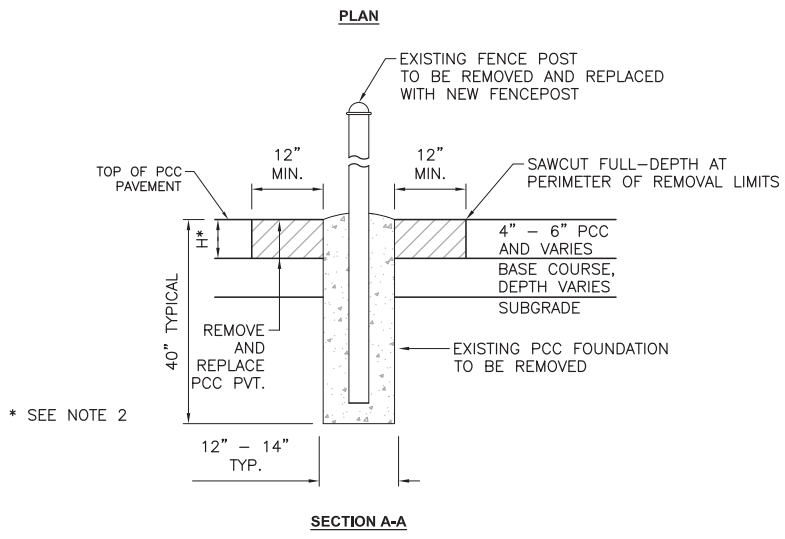
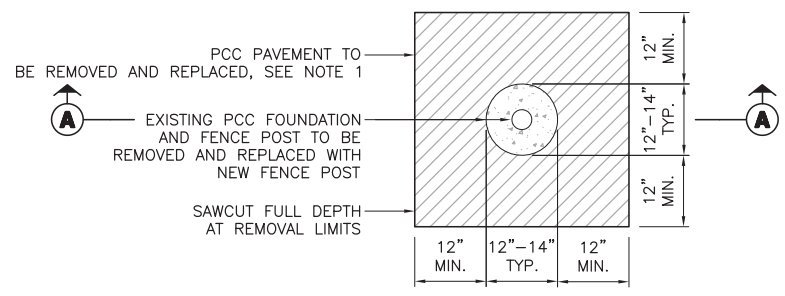
**FENCING DETAILS - 1**



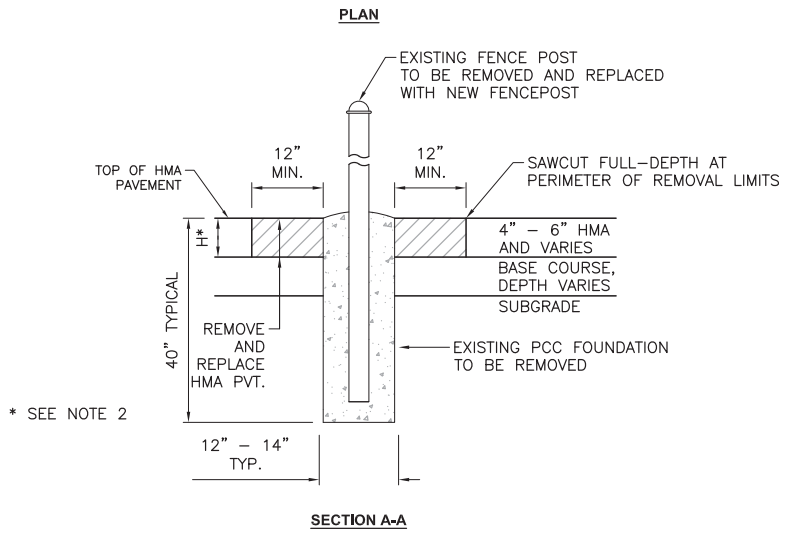
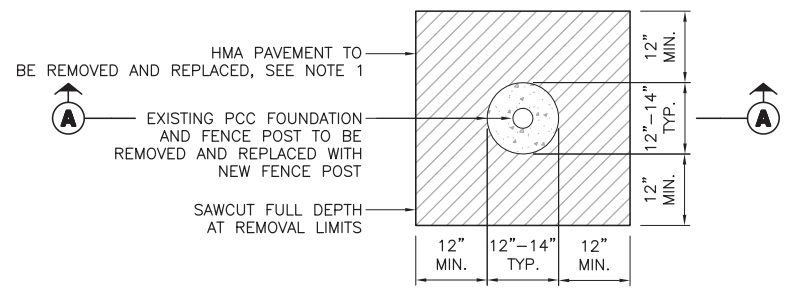
DESIGN BY:	RMD
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	12/01/15
JOB No:	12244-02

**FINAL**

REF: DWG: Fence - Detail - 2.dwg  
 IMAGE FILES: C:\3C\cmyk\jg\cmt\_block\jg\jg  
 UPDATE BY: Ryan Drea  
 LAYOUT: LAYOUT1  
 DATE: Friday, December 4, 2015 11:08:40 AM  
 FILE: k:\Dhona\12244-02\_Fence\Draw\Sheets\13\_Fence\_Details\_2.dwg



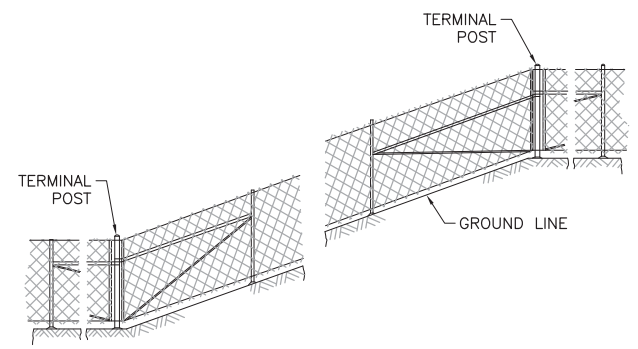
**FENCE POST REMOVAL AND REPLACEMENT IN PCC PAVEMENT**



**FENCE POST REMOVAL AND REPLACEMENT IN HMA PAVEMENT**

- NOTES:**
- PCC PAVEMENT TO BE REMOVED FULL DEPTH
  - BASE COURSE AND/OR SUBGRADE TO BE REMOVED SUCH THAT THE DEPTH "H" IS A MINIMUM OF 6 INCHES.
  - NEW FENCE SHALL REUSE EXISTING POST HOLES AS FIELD CONDITIONS ALLOW. SEE DETAIL FOR NEW FENCE POST FOUNDATION IN EXISTING PCC PAVEMENT.
  - COSTS OF PAVEMENT AND FOUNDATION REMOVAL, NEW PAVEMENT, SAW CUTTING, CLEANUP AND DISPOSAL, SHALL BE CONSIDERED INCIDENTAL TO AR162900, REMOVE CLASS E FENCE.
  - AT THE CONTRACTOR'S OPTION AND AT NO ADDITIONAL COST TO THE CONTRACT, THE CONTRACTOR MAY REMOVE A "STRIP" OF PCC PAVEMENT ALONG THE FENCELINE, TO BE REPLACED IN KIND AFTER FENCELINE REMOVAL AND REPLACEMENT.
  - SAME DETAIL IS TO BE USED IF NEW POST HOLES ARE REQUIRED IN EXISTING PCC PAVEMENT, NOT INCLUDING THE REMOVAL PORTION (COST INCIDENTAL TO CONTRACT)

- NOTES:**
- HMA PAVEMENT TO BE REMOVED FULL DEPTH
  - BASE COURSE AND/OR SUBGRADE TO BE REMOVED SUCH THAT THE DEPTH "H" IS A MINIMUM OF 6 INCHES.
  - NEW FENCE SHALL REUSE EXISTING POST HOLES AS FIELD CONDITIONS ALLOW. SEE DETAIL FOR NEW FENCE POST FOUNDATION IN EXISTING HMA PAVEMENT.
  - COSTS OF PAVEMENT AND FOUNDATION REMOVAL, NEW PAVEMENT, SAW CUTTING, CLEANUP AND DISPOSAL, SHALL BE CONSIDERED INCIDENTAL TO AR162900, REMOVE CLASS E FENCE.
  - AT THE CONTRACTOR'S OPTION AND AT NO ADDITIONAL COST TO THE CONTRACT, THE CONTRACTOR MAY REMOVE A "STRIP" OF ASPHALT PAVEMENT ALONG THE FENCELINE, TO BE REPLACED IN KIND AFTER FENCELINE REMOVAL AND REPLACEMENT.
  - SAME DETAIL IS TO BE USED IF NEW POST HOLES ARE REQUIRED IN EXISTING HMA PAVEMENT, NOT INCLUDING THE REMOVAL PORTION (COST INCIDENTAL TO CONTRACT)



**INSTALLATION ON SLOPES**

IL. CONTRACT: **DI027**  
 IL. LETTING ITEM: **8A**  
 IL. PROJECT: **C73-4370**  
 S.B.G. PROJECT: **3-17-SBGP-99/105**

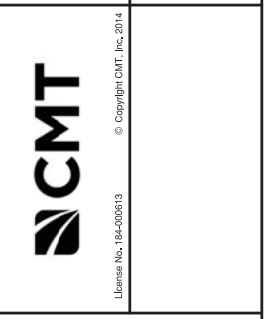
SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2  
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34x22).

**DIXON MUNICIPAL AIRPORT  
 DIXON, ILLINOIS  
 INSTALL PERIMETER FENCE ON NORTH PROPERTY LINE**

**FENCING DETAILS - 2**



DESIGN BY:	RMD
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	12/01/15
JOB No:	12244-02

**FINAL**