1-15-2 21 LETTING ITEM 13

STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

PROPOSED

HIGHWAY PLANS

J--91--495--20

CARROLL OGLE WHITESIDE LEE ROCK ISLAND HENRY BUREAU LA SALLE GRUNDY KANICAKEE ROCK ISLAND HENRY BUREAU LA SALLE GRUNDY KANICAKEE KANICAKEE KOOX PEORIA WOODFORD ROCUOIS

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT LOCATED IN

THE CITY OF CHICAGO

TRAFFIC DATA

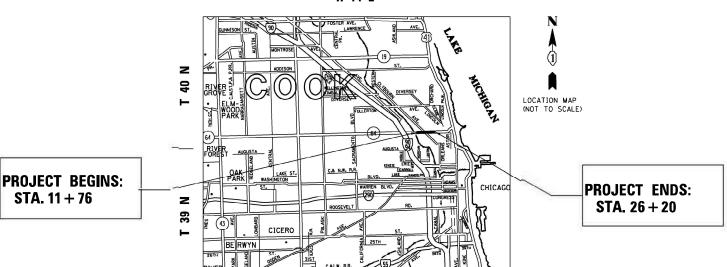
ADT 2019: 27,100 VPD

SPEED LIMIT: 30 MPH

F.A.P. ROUTE 307: IL 64 (NORTH AVENUE) SHEFFIELD AVE. TO HALSTED ST. SECTION 2020–096–RS SMART OVERLAY & ADA IMPROVEMENTS

> COOK COUNTY C-91-293-20

> > R 14 E



GROSS LENGTH = 1.444 FT. = 0.28 MILE

NET LENGTH = 1.444 FT. = 0.28 MILE

0 100' 200' 300' — 1" = 100' 10' 20' 30' — 1" = 10' 0 50' 100' — 1" = 50' 0 50' 100' — 1" = 40' 0 50' 100' — 1" = 30' 0 50' 100' — 1" = 20'

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

C.U.A.N. CHICAGO UTILITY ALERT NETWORK (312)-744-7000

PROJECT ENGINEER: DAN WILGREEN (847) 705–4240 PROJECT MANAGER: FAWAD AQUEEL (847) 705–4247

CONTRACT NO. 62L74

STATE OF ILLINGIS

DEPARTMENT OF TRANSPORTATION

SUBMITTED

CTO BER 6 20 20

AND REGIONAL ENGINEER

December 4, 2020

ENGINEER OF DESIGN AND ENVIRONMENT

December 4, 2020

DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION 13

LOCATION OF SECTION INDICATED THUS: - -

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

REV-SEP

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	INDEX OF SHEETS
3-4	SUMMARY OF QUANTITIES
5	EXISTING AND PROPOSED TYPICAL SECTIONS
6	ROADWAY AND PAVEMENT MARKING PLAN
7-8	SIDEWALK DETAIL PLAN
9	DRIVEWAY DETAIL (BD-02)
10	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-8)
11	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)
12	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)
13	BUTT JOINT AND HMA TAPER DETAILS (BD-32)
14	CITY OF CHICAGO DETECTABLE WARNINGS (BD-58)
15	TRAFFIC CONTROL AND PROTECTION FOR SIDE REOADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)
16	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14)
17	ARTERIAL ROAD INFORMATION SIGN (TC-22)
18-20	CITY OF CHICAGO TYPICAL PAVEMENT MARKINGS (TC-24)
21	DRIVEWAY ENTRANCE SIGNING (TC-26)

STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-08	TYPICAL SYMBOLS, ABBREVIATIONS AND PATTERNS
442201-03	CLASS C AND D PATCHES
604001-05	FRAME AND LIDS, TYPE 1
606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701427-05	LANE CLOSURE, MULTILANE, INTERMITENT OR MOVING OPER., FOR SPEEDS = 40 MPH
701602-10	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER, OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS
781001-04	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
814001-03	HANDHOLES

GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION THE CONTRACTOR SHALL CALL "CUAN" (CHICAGO UTILITY ALERT NETWORK) AT 312-744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, (48 HOUR NOTIFICATION REQUIRED)
- 2. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND THE CITY OF CHICAGO
- 3. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION OF THE DEPARTMENT.
- 4. ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS WILL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- 5. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
- 6. ALL PAVEMENT PATCHING, CURB AND GUTTER REMOVAL AND REPLACEMENT, SIDEWALK REMOVAL, P.C.C. SIDEWALK 5", AND DRAINAGE ADJUSTMENT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- 8. FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT KALPANA, KANNAN-HOSADURGA@ILLINOIS, GOV A MINIMUM OF 72 HOURS OF WORK
- 10. THE RESIDENT ENGINEER SHALL CONTACT EMAD ALHUSSEINI, ARTERIAL TRAFFIC FIELD ENGINEER, AT EMAD.ALHUSSEINI@ILLINOIS.GOV MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- 11. THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.
- 12. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- 13. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- 14. PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
- 15. ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF CURB OR DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER, SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT. ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED AS INCIDENTAL.
- 16. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- 17. ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
- 18. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- 19. PROPOSED SIDEWALK RAMPS SHALL CONFORM TO CURRENT ADA REQUIREMENTS AND APPLICABLE STATE HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER.

307

20. THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

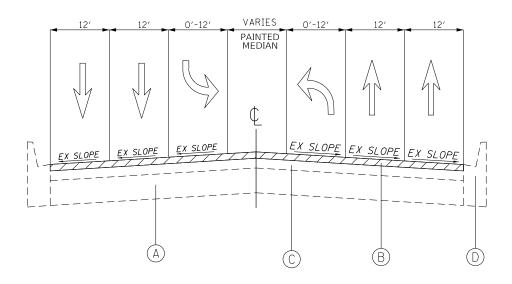
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INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES

| SHEET OF SHEETS STA. TO STA.

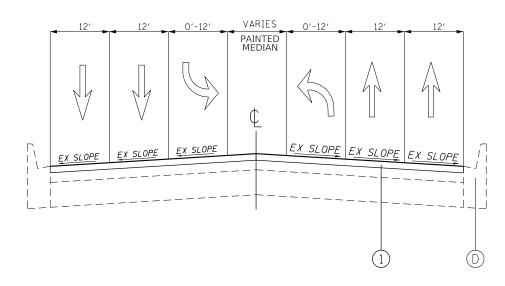
	SUMMARY OF QUANTITIES				CO	NSTRUCTIO	ON TYPE C	ODE			SUMMARY	OF QUANTITIES				COI	NSTRUCTION	TYPE COD	E	
CODE NO	ITEM	UNIT	TOTAL OUANTITIES URBAN	100% STATE 0005						CODE NO	SOMMANT	ITEM	UNIT	TOTAL QUANTITIES URBAN	100% STATE 0005					
20200100	EARTH EXCAVATION	CU YD	8	8						44201771	CLASS D PATCHE	S. TYPE IV. 10 INCH	SO YD	30	30					
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	4188	4188						60237460	INLETS, TYPE A	, TYPE 23 FRAME AND GRATE	EACH	1	1					
							1													
40600400	MIXTURE FOR CRACKS, JOINTS, AND	TON	1	1						60255700	MANHOLES TO BE	ADJUSTED WITH NEW TYPE 1	EACH	3	3					
	FLANGEWAYS										FRAME, OPEN LI	D								
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT	SO YD	220	220						60255800	MANHOLES TO BE	ADJUSTED WITH NEW TYPE 1	EACH	3	3					
	TNIOL	1									FRAME, CLOSED	LID								
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	37	37						66900200	NON-SPECIAL WA	STE DISPOSAL	CU YD	8	8					
40604172	POLYMERIZED HOT-MIX ASPHALT SURFACE	TON	912	912						66900530	SOIL DISPOSAL	ANALYSIS	EACH	2	2					
	COURSE, IL-9.5, MIX "E", N70																			
] ,	66901001	REGULATED SUBS	STANCES PRE-CONSTRUCTION	LSUM	1	1					
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY	SO YD	45	45							PLAN									
	PAVEMENT, 8 INCH																			
									•	£ 66901003	REGULATED SUBS	TANCES FINAL CONSTRUCTION	LSUM	1	1					
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	578	578							REPORT									
44000156	HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"	SO YD	9306	9306					1	66901006	REGULATED SUBS	TANCES MONITORING	CAL DA	3	3					
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	45	45						67000400	FNGINFFR'S FIF	LD OFFICE, TYPE A	CAL MO	6	6					
44000600	SIDEWALK REMOVAL	SO FT	600	600						67100100	MOBILIZATION		L SUM	1	1					
44002216	HOT-MIX ASPHALT REMOVAL OVER PATCHES. 4"	SO YD	161	161						70102625	TRAFFIC CONTRO	L AND PROTECTION,	L SUM	1	1					
											STANDARD 70160	96								
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SO YD	25	25																
										70102632	TRAFFIC CONTRO	L AND PROTECTION,	L SUM	1	1				RE	V-SEP
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SO YD	85	85							STANDARD 70160	2						* SPEC	IALTY	ITEM
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	SUMMARY OF QUANTITIES				CON	STRUCTIO	N TYPE C	CODE			SLIMMAF	RY OF QUANTITIES				СО	NSTRUCTIO	N TYPE CODE	
CODE NO	ITEM	UNIT	TOTAL QUANTITIES URBAN	100% STATE 0005						CODE NO		ITEM	UNIT	TOTAL QUANTITIES URBAN	100% STATE 0005				
70102635	TRAFFIC CONTROL AND PROTECTION,	L SUM	1	1						* 78000600	THERMOPLASTI	C PAVEMENT MARKING - LINE 12"	FOOT	705	705				
	STANDARD 701701																		
										7000050	THE DWOD! AST I	C DAVENENT MARKING . LINE . 24"	FOOT	1076	1076				
		_								* 78000650	THERMOPLASTI	C PAVEMENT MARKING - LINE 24"	F00T	1276	1276				
70102640	TRAFFIC CONTROL AND PROTECTION,	L SUM	1	1															
	STANDARD 701801									* 85000200	MAINTENANCE	OF EXISTING TRAFFIC SIGNAL	EACH	1	1				
											INSTALLATION								
70300100	SHORT TERM PAVEMENT MARKING	F00T	2665	2665															
										89502376	REBUILD EXIS	TING HANDHOLE	EACH	1	1				
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	888	888															
										x0320050	CONSTRUCTION	LAYOUT (SPECIAL)	L SUM	1	1				
70300210	TEMPORARY PAVEMENT MARKING LETTERS AN	D SO FT	339	339															
	SYMBOLS									x4240800	DETECTABLE W	ARNINGS (SPECIAL)	SO FT	160	160				
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	F00T	3578	3578						x4400501	COMBINATION	CURB AND GUTTER REMOVAL AND	F00T	150	150				
											REPLACEMENT	LESS THAN OR EQUAL TO 10 FEET							
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	F00T	1489	1489															
										x6030310	FRAMES AND L	IDS TO BE ADJUSTED	EACH	40	40				
70300260	TEMPORARY PAVEMENT MARKING - LINE 12'	FOOT	705	705							(SPECIAL)								
70300280	TEMPORARY PAVEMENT MARKING - LINE 24'	FOOT	1276	1276						Z0001110	GAS VALVE TO	BE ADJUSTED	EACH	1	1				
													!						
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	1332	1332			1			Z0004562	COMBINATION	CONCRETE CURB AND GUTTER	FOOT	50	50				
	·										REMOVAL AND								
78000100	THERMOPLASTIC PAVEMENT MARKING -	SO FT	339	339					1				1						
	LETTERS AND SYMBOLS	3311	333	333			<u> </u>			Z0018500	DRAINACE CED	UCTURES TO BE CLEANED	EACH	8	8				
	FELIEUS WIND SIMBULS								<u> </u>		DUMINAGE 21K	OCTURES TO BE CLEANED	EACH	•	0				
7800000	THE DIAGRA SETTION DANIELES AND	411 5007	75.70	75.70					<u> </u>	70070050	TEMPODARY	FORMATION CLONING	50.55	E 2	E2				
78000200	THERMOPLASTIC PAVEMENT MARKING - LINE	4" F00T	3578	3578						20030850	I LMPUKARY IN	FORMATION SIGNING	SO FT	52	52				
												JOHNE SEAL ONE							
78000400	THERMOPLASTIC PAVEMENT MARKING - LINE	6" F00T	1489	1489			<u> </u> 		<u> </u>	Z0033700	LUNGITUDINAL	JOINT SEALANT	FOOT	5776	5776				
									<u> </u> 				<u> </u>				NON-PAR	T 100% STATE	
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EXISTING TYPICAL SECTION

STA 11+76 TO STA 26+20



PROPOSED TYPICAL SECTION

STA 11+76 TO STA 26+20

LEGEND - EXISTING:

- (A) CONCRETE PAVEMENT ± 10"
- (B) HOT MIX ASPHALT SURFACE REMOVAL 1 3/4"
- (C) HOT MIX ASPHALT SURFACE 4"±
- (D) COMBINATION CURB & GUTTER, TYPE B-6.12

LEGEND - PROPOSED

1) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", IL-9.5, N70, 13/4"

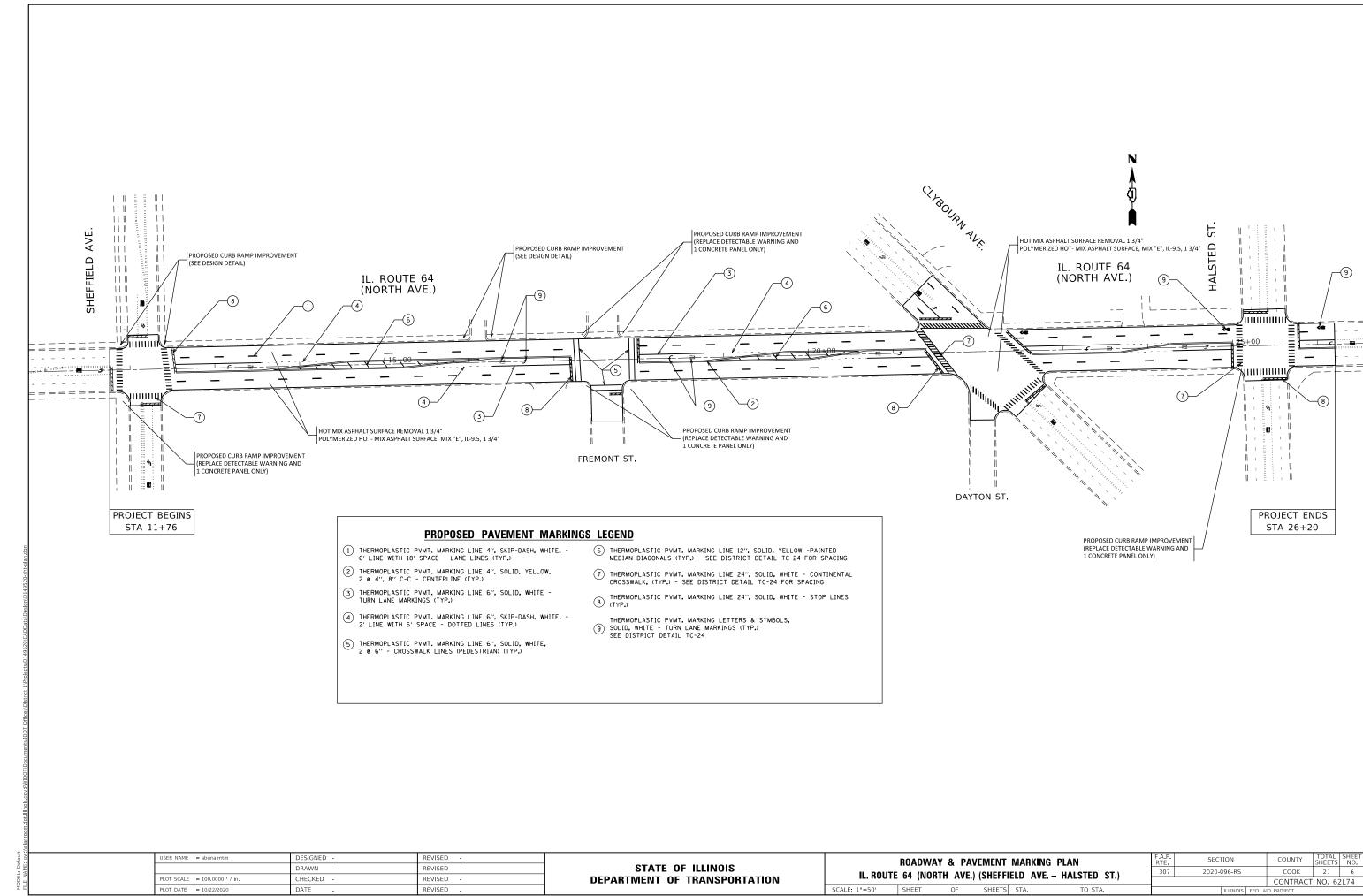
- NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
- NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY SPECIAL PROVISIONS.

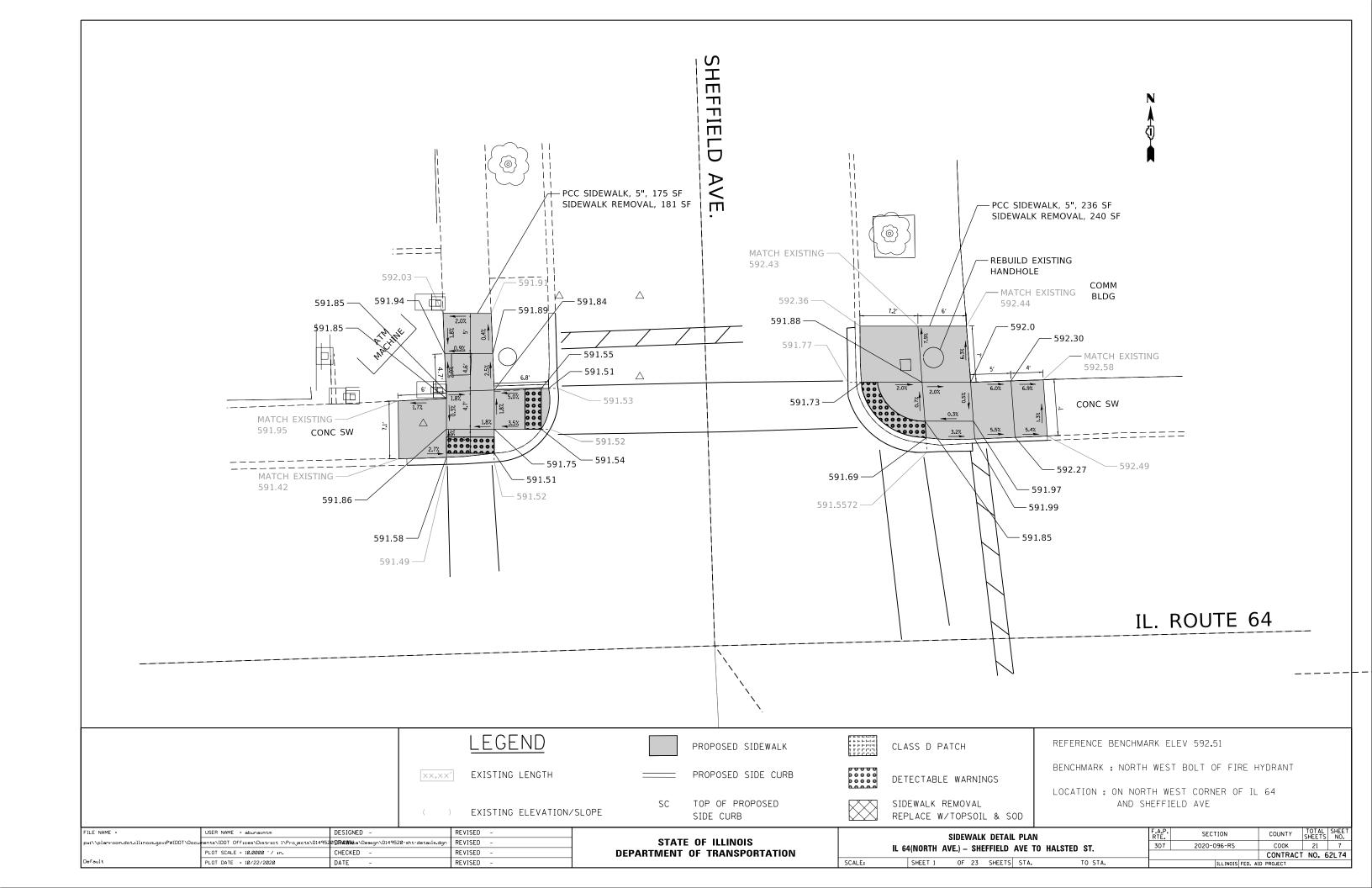
 FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.
- NOTE 3: QUALITY MANAGEMENT PROGRAM (QMP) IDNTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.
- NOTE 4: THE CONTRACTOR SHALL PATCH FIRST.
- NOTE 5: THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE MILLED SURFACE

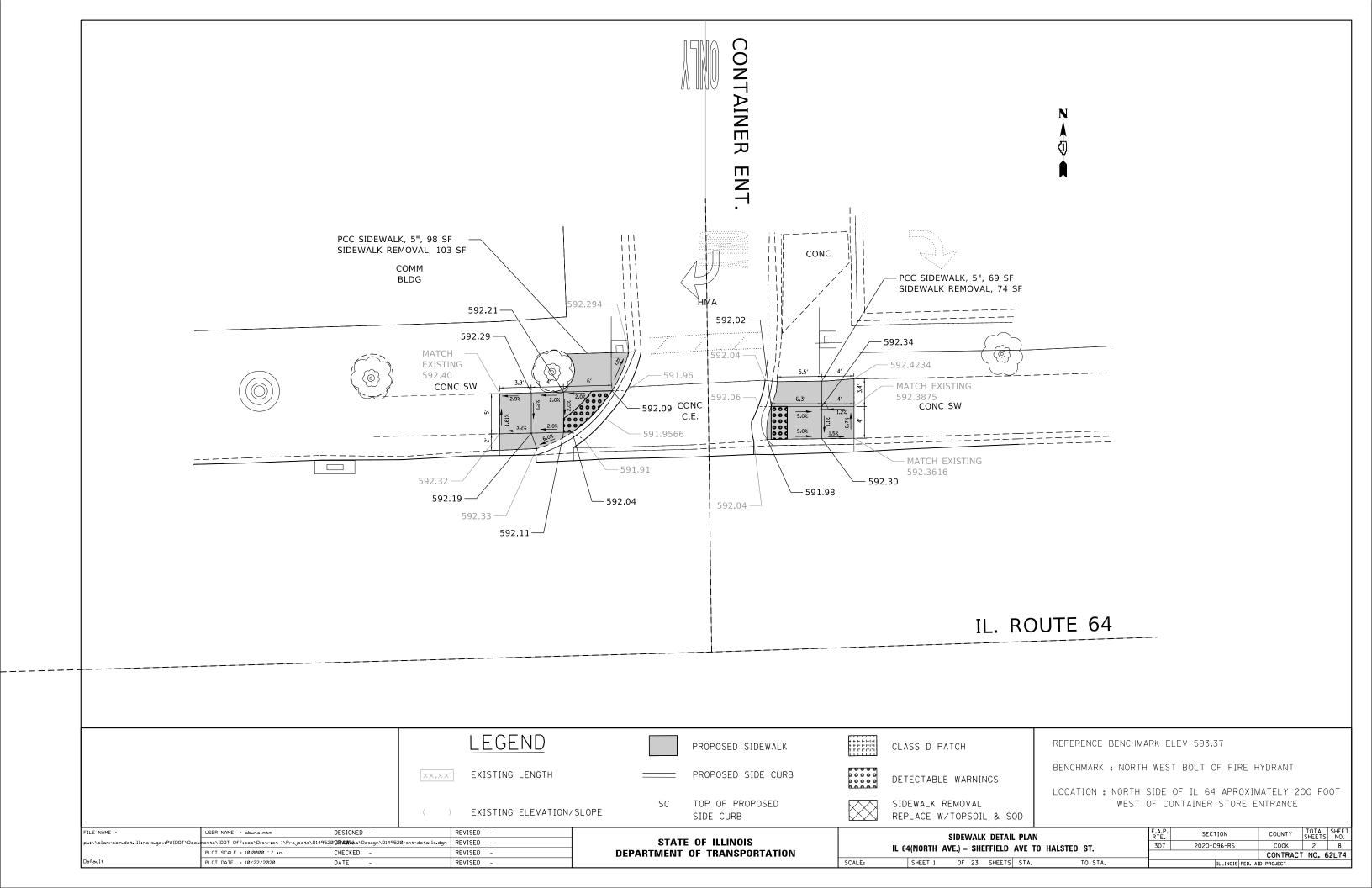
HOT-MIX ASPHALT MIXTURE REQUIREMENTS						
MIXTURE TYPE	AIR VOIDS @ Ndes	PROGRAM (QMP)				
PAVEMENT RESURFACING						
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", IL-9.5, N70, 1 3/4'	4.0% AT 70 GYR	QC/QA				
PATCHING						
CLASS D PATCHES (HMA BINDER IL-19 mm)	4% AT 70 GYR.	QC/QA				
HOT-MIX ASPHALT REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm)	4% AT 70 GYR.	QC/QA				
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONT	TROL FOR PERFORMANCE	E (QCP)				

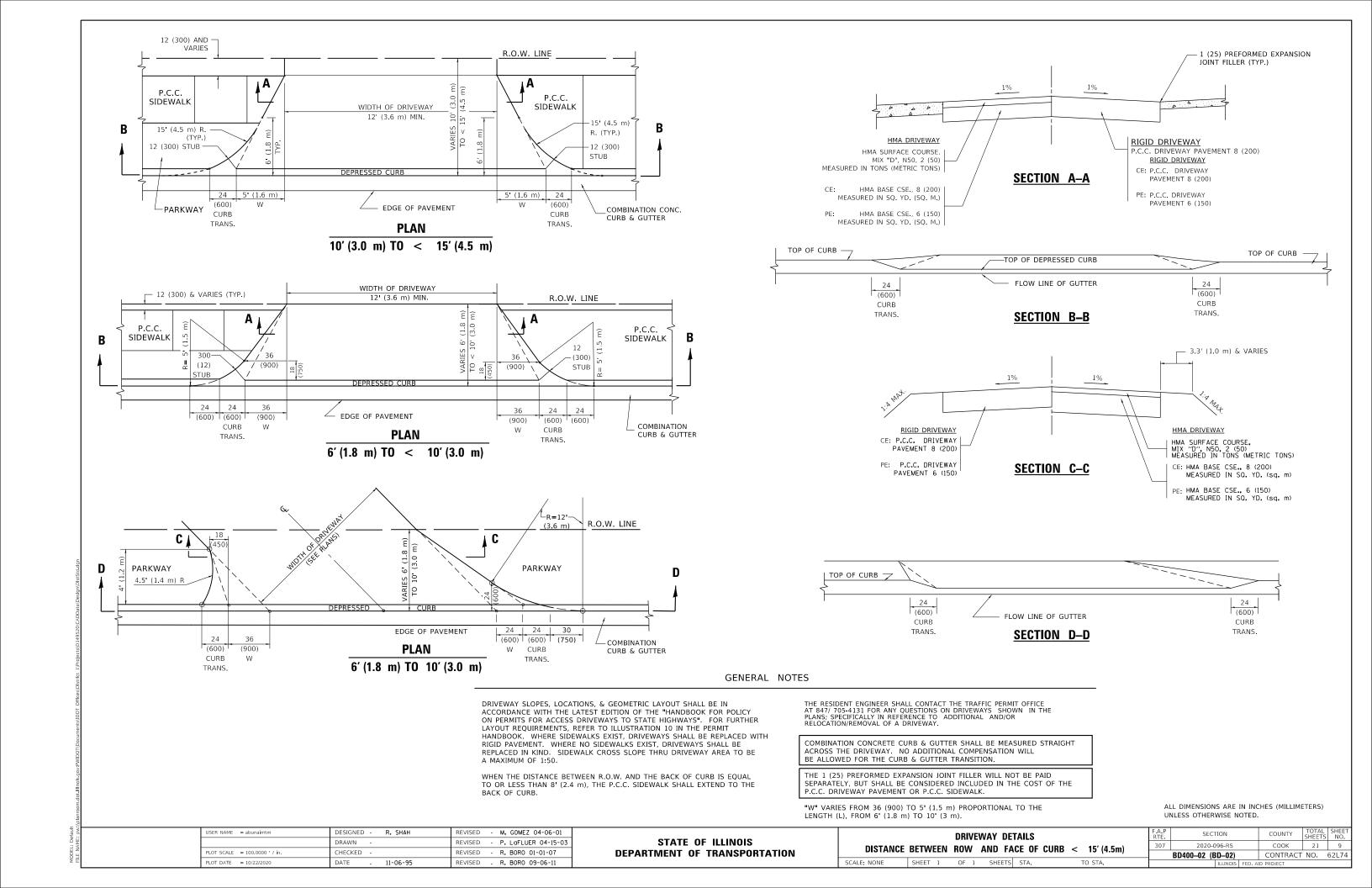
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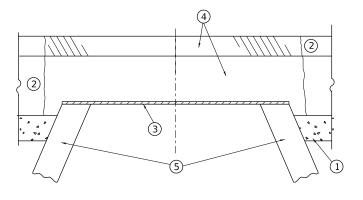
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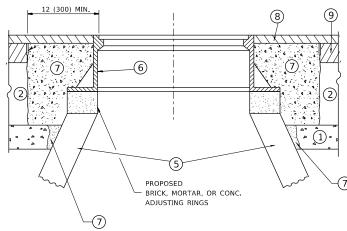












NOTES

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40)
 THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1 *
 CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING
 BASE COURSE OR THE BINDER COURSE.
- f * UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE FINGINFER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- 6 FRAME AND LID (SEE NOTES)
- 2 EXISTING PAVEMENT
- (7) CLASS PP-1 *CONCRETE
- 3 36 (900) DIAMETER METAL PLATE
- 8) PROPOSED HMA SURFACE COURSE
- PROPOSED CRUSHED STONE AND HMA SURFACE MIX

 (5) EXISTING STRUCTURE
- 9 PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

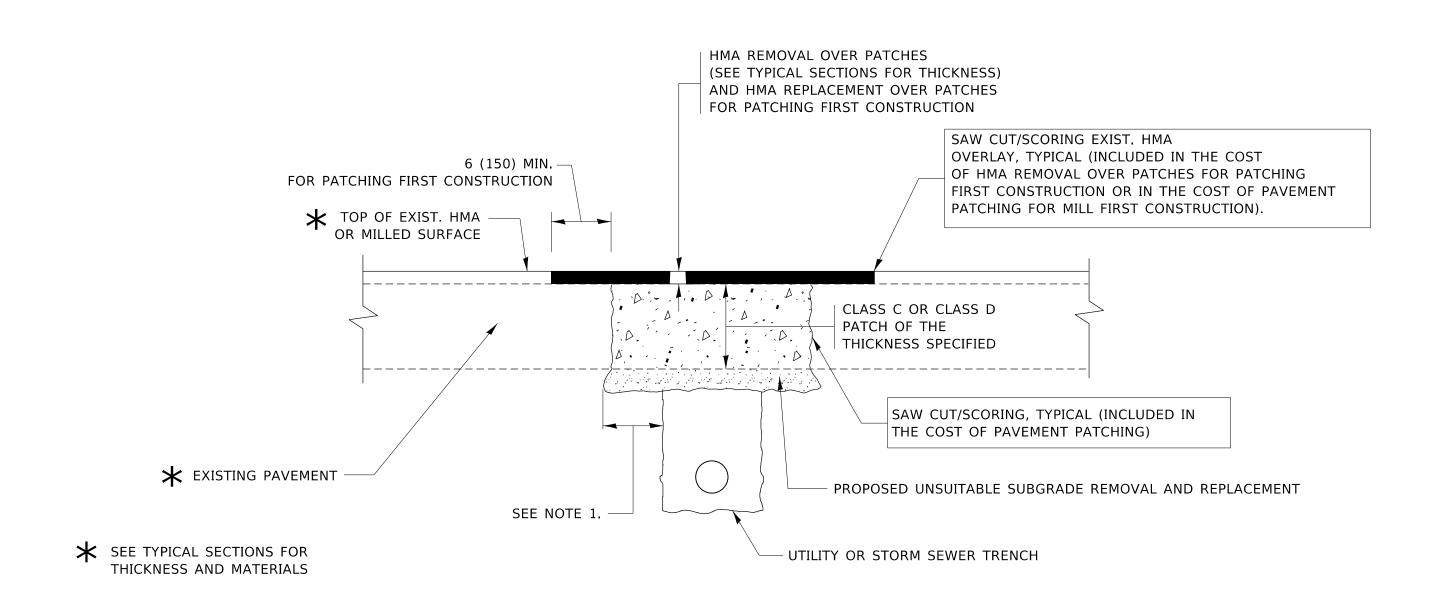
NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING
SHEET 1 OF 1 SHEETS STA. TO STA.



NOTES:

- 1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
- 2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

- 1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
- 2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
- 3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

- 1. MILL HMA FIRST IF THERE IS AT LEAST $4\frac{1}{2}$ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
- 2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

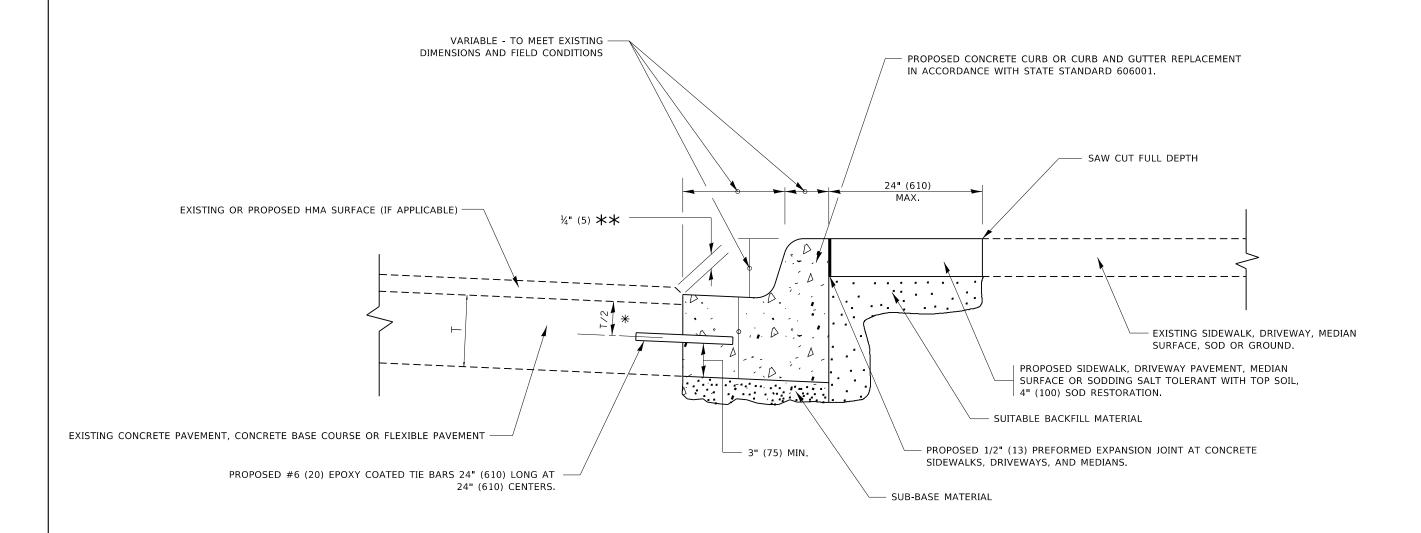
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

OSEK NAME = abunannun	DESIGNED -	N. SHAII	KEVISED	-	A. ADDAS 04-27-90
	DRAWN -		REVISED	-	R. BORO 01-01-07
PLOT SCALE = 100.0000 / in.	CHECKED -		REVISED	-	R. BORO 09-04-07
PLOT DATE = 10/22/2020	DATE -	10-25-94	REVISED	-	K. ENG 10-27-08

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE: NONE

		PAVEMENT PATCHING FOR						F.A.P RTE	SECTION		COUNTY	TOTAL SHEETS	SHEET NO.
HMA SURFACED PAVEMENT					ENT	307	2020-096-RS		соок	21	11		
		1111	ות ט	JIII	AULD I	AVEIVII			BD400-04 (BD-22)		CONTRACT	NO.	62L74
	SHEET	1	OF	1	SHEETS	STA.	TO STA.		ILLINOIS	FED. A	ID PROJECT		



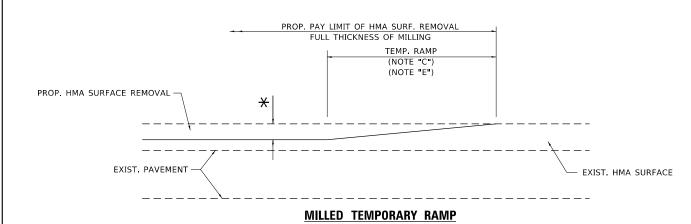
- 💥 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- $\star\star$ IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

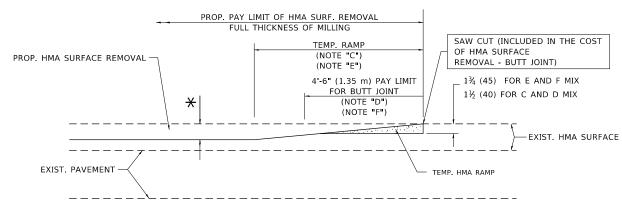
USER NAME = abunaimtm	DESIGNED -	A. HOUSEH	REVISED	-	A. ABBAS 03-21-97
	DRAWN -		REVISED	-	M. GOMEZ 01-22-01
PLOT SCALE = 100.0000 / in.	CHECKED -		REVISED	-	R. BORO 12-15-09
PLOT DATE = 10/22/2020	DATE -	03-11-94	REVISED	-	K. SMITH 07-11-19

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

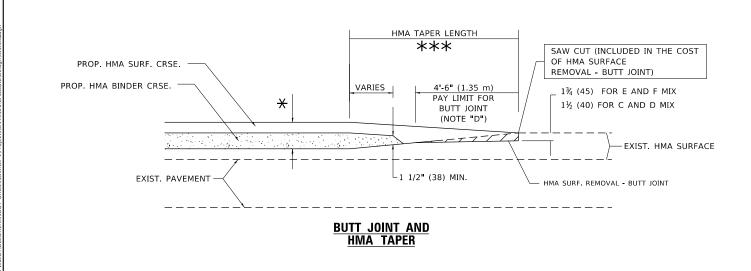


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

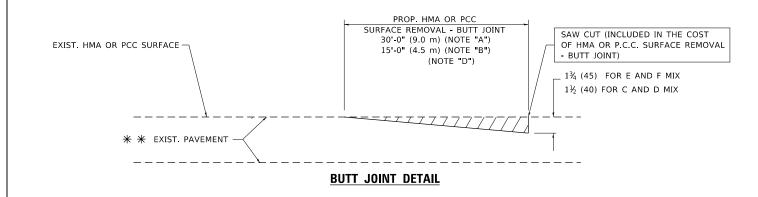
 USER NAME
 = abunaimtm
 DESIGNED
 M. DE YONG
 REVISED
 R. SHAH 10-25-94

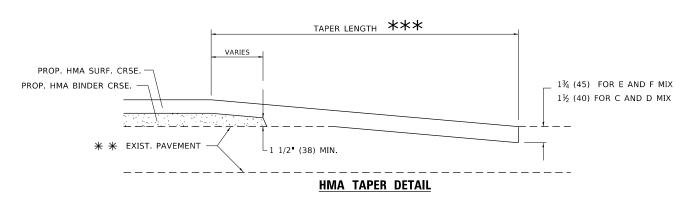
 DRAWN
 REVISED
 A. ABBAS 03-21-97

 PLOT SCALE
 = 100,0000 ' / in.
 CHECKED
 REVISED
 M. GOMEZ 04-06-01

 PLOT DATE
 = 10/22/2020
 DATE
 06-13-90
 REVISED
 R.BORO 01-01-07

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION





TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A. MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F. INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL BUTT JOINT.

 ** SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- G. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A") 10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

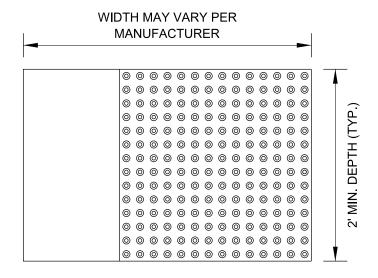
BASIS OF PAYMENT

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER)
FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL-BUTT JOINT".

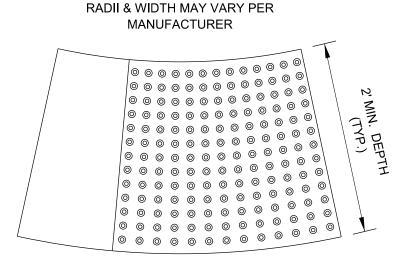
SCALE: NONE

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

STRAIGHT DETECTABLE WARNING UNITS



RADIAL DETECTABLE WARNING UNITS

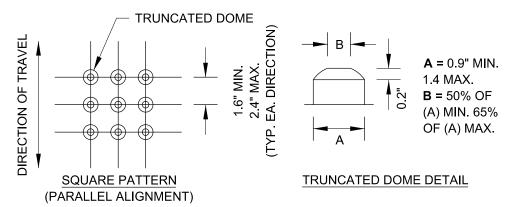


DETECTABLE WARNING UNIT SIZES

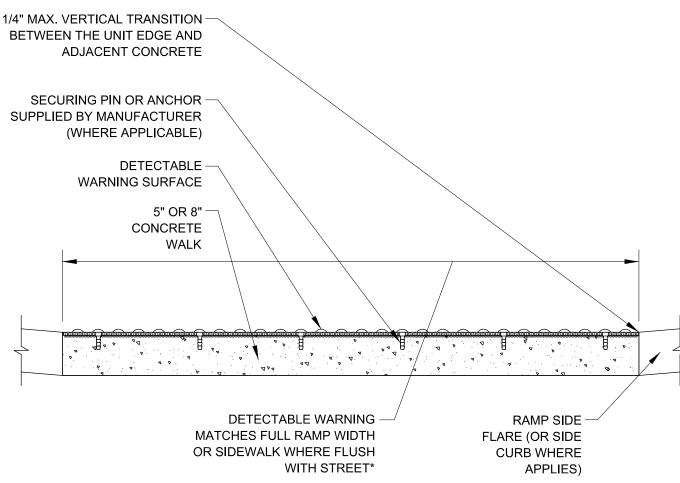
- VERIFY ALL DIMENSIONS WITH THE PRODUCT MANUFACTURER.
- IF USING RADIAL UNITS, VERIFY THAT THE CURB RADIUS MATCHES AVAILABLE UNIT RADII WITH THE PRODUCT MANUFACTURER.

GENERAL NOTE:

THE ROWS OF DOMES IN THE DETECTABLE WARNING MATERIAL MUST BE ALIGNED WITH THE PATH OF WHEELCHAIR TRAVEL WHICH IS REQUIRED TO BE PERPENDICULAR TO THE GRADE BREAK AT THE BOTTOM OF THE RAMP TO PERMIT TRACKING BETWEEN DOME ROWS. ON BLENDED TRANSITIONS OR FLUSH TRANSITIONS, WHERE RADIAL UNITS ARE SITUATED ABOUT THE CURB RADIUS, DOME ORIENTATION IS NOT SIGNIFICANT.



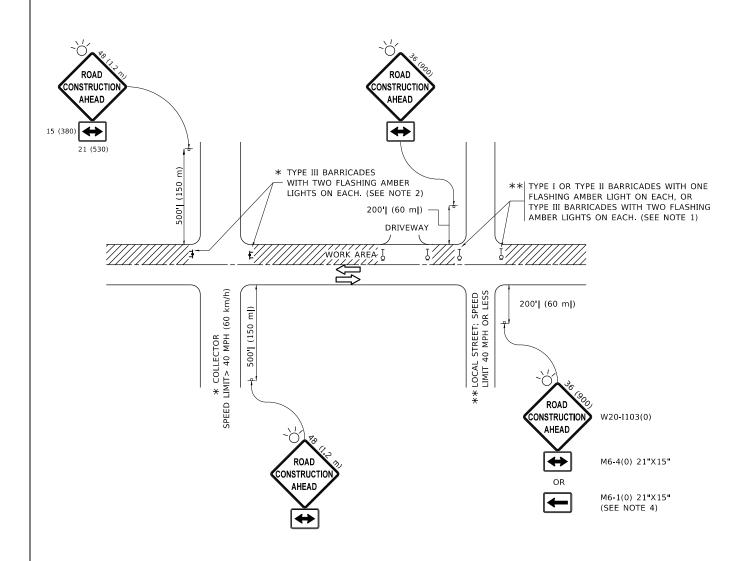
UNIT PATTERN & DOME DETAIL



*A BORDER OF 2 INCHES OR LESS AROUND THE DETECTABLE WARNING SURFACE IS
ACCEPTABLE IF REQUIRED FOR PROPER INSTALLATION OF THE DETECTABLE WARNING SURFACE PRODUCT

DETECTABLE WARNING UNIT SECTION

USER NAME = abunaimtm	DESIGNED -	REVISED -	CITY OF CHICAGO F.A.P SECTION				CITY OF CHICAGO		SECTION	COUNTY	TOTAL	SHEET		
	DRAWN -	REVISED -	STATE OF ILLINOIS								2020-096-RS	соок	21	14
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION	DETECTABLE WARNINGS				BD 58	CONTRACT	No.	52L74			
PLOT DATE = 10/22/2020	DATE - 06-20-2017	REVISED -		SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.		ILLINOIS FED. A	ID PROJECT		



NOTES:

- 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
- THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY
 b) BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION
 OF THE CLOSED PORTION.
- 3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE
 4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL
 BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

- 5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- 6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER
- 7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = abunaimtm	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
	DRAWN -	REVISED - T. RAMMACHER 01-06-00
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
PLOT DATE = 10/22/2020	DATE - 06-89	REVISED A SCHUETZE 09-15-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

s						TION FOR DRIVEWAYS
SCALE: NONE	SHEET	1 OF	1	SHEETS	STA.	TO STA.

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

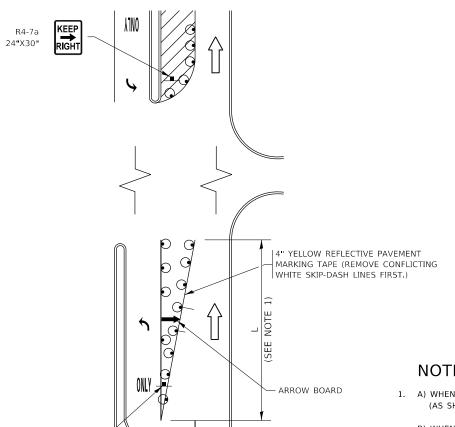


FIGURE 1

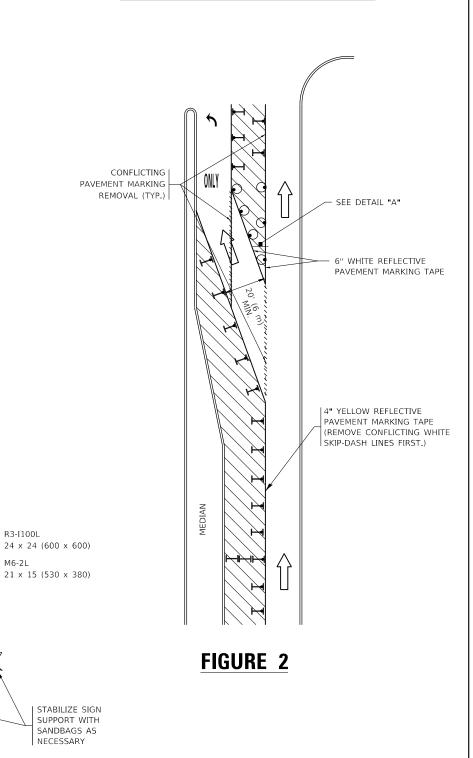
SEE DETAIL "A"

LEGEND WORK AREA LANE OPEN TO TRAFFIC ARROW BOARD TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT DRUM WITH STEADY BURN LIGHT SIGN ASSEMBLY TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

- 1. A) WHEN "L" IS ≤ THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
 - B) WHEN "L" IS > THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
- 2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- 3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
- 4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
- 5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
- 6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
- 7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREOUIREMENTS.
- 8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE WITHIN A LANE CLOSURE



DETAIL A

SCALE: NONE

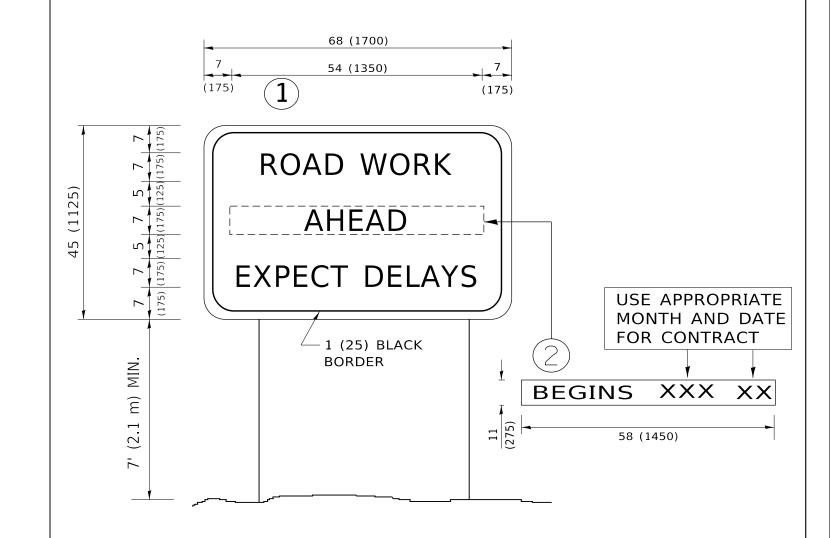
TURN LANE

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = abunaimtm	DESIGNED	- 1.	RAMMACHER 09-08-94	KEVISED	-	R. BORO 09-14-09	
	DRAWN	-	A. HOUSEH 11-07-95	REVISED	- A.	SCHUETZE 07-01-1	3
PLOT SCALE = 100.0000 / in.	CHECKED	-	A. HOUSEH 10-12-96	REVISED	- A.	SCHUETZE 09-15-1	6
PLOT DATE = 10/22/2020	DATE	- T.	RAMMACHER 01-06-00	REVISED	-		

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

TRAFE	IC CONTROL	AND	PROTECTION AT TU	RN BAYS	F.A.P RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	(TO REM	ми	OPEN TO TRAFFIC)		307	2020-096-RS	соок	21	16
	(10 IILIV	MIN	OI LIV TO THAITIO,			TC-14	CONTRACT	NO.	62L74
NE	CHEET 1 O	E 1	CHEETE CTA	TO STA		TI LINOIC SED	HD DOOLEGE		



NOTES:

- 1. USE BLACK LETTERING ON ORANGE BACKGROUND.
- 2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
- 3. ERECT SIGN 1 WITH INSTALLED PANEL 2 ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
- 4. REMOVE PANEL(2)SOON AFTER THE START OF CONSTRUCTION.
- 5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.

SHEET

6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)

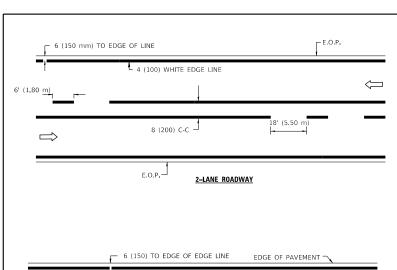
SCALE: NONE

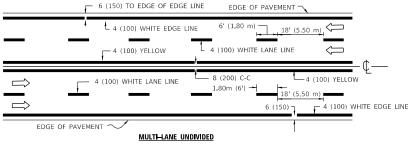
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

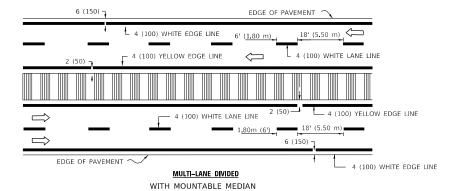
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

USER NAME = abunaimtm	DESIGNED -	REVISED	-	R. MIRS 09-15-97
	DRAWN -	REVISED	-	R. MIRS 12-11-97
PLOT SCALE = 100.0000 / in.	CHECKED -	REVISED	- T.	RAMMACHER 02-02-9
PLOT DATE = 10/22/2020	DATE -	REVISED	-	C. JUCIUS 01-31-07

ARTERIAL ROAD					F.A.P RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
INFORMATION SIGN			307	2020-096-RS	соок	21	17		
INFUNIVIATION SIGN			TC-22	CONTRACT	NO.	62L74			
1	OF 1	SHEETS	STA.	TO STA		TILLINOIS FED A	ID PROJECT		

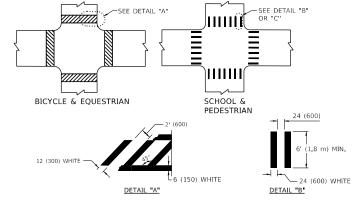




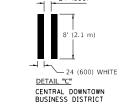


TYPICAL LANE AND EDGE LINE MARKING

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE



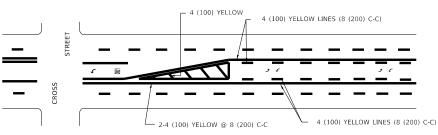
TYPICAL CROSSWALK MARKING



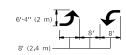
2-4 (100) @ 8 (200) C-C (MINIMUM 5)

- * FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
- * DIAGONAL LINE SPACING: 20' (6.1 m) C-C

PAINTED MEDIANS

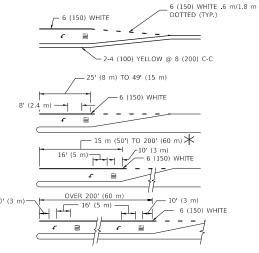


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

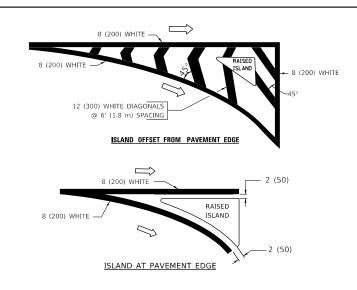


FULL SIZE LETTERS 8 (2.4 m) AND ARROWS SHALL BE USED. \uparrow AREA = 15.8 SQ. FT. (1.47 m²) $\parallel \parallel \parallel \parallel$ AREA = 22.9 SQ. FT. (2.13 m²)

TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	8 (200) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	8 (200) C-C
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	6' (1.8 m) LINE WITH 18' (5.50 m) SPACE FOR SKIP-DASH; 8 (200) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4 m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL & PEDESTRIAN)	12 (300) @ 45° 24 (600) @ 90°	SOLID SOLID	WHITE WHITE	2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	8 (200) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 20' (6.1 m) (LESS THAN 30 MPH (50 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R":3.6 SO. FT. (0.33m ²) EACH "X":54,0 SO. FT. (5.0 m ²)

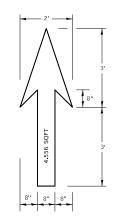
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STREET MARKING STANDARDS, PRINTED BY CITY OF CHICAGO, DEPARTMENT OF TRANSPORTATION, BUREAU OF TRAFFIC.

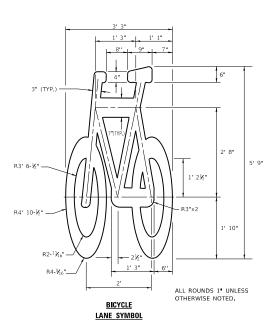
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

USER NAME = abunaimtm	DESIGNED -	REVISED	-T. RAMMACHER 12-07-0
	DRAWN -	REVISED	- K. ENG 02-28-12
PLOT SCALE = 100.0000 / in.	CHECKED -	REVISED	=
PLOT DATE = 10/22/2020	DATE -	REVISED	=

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO								F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TYPICAL PAVEMENT MARKINGS								2020-096-RS	соок	21	18
	1111	UAL	FAV	LIVILIVI	WANKIIVUS			·	TC-24	CONTRACT	NO.	62L74
CALE: NONE SHEET 1 OF 3 SHEETS STA. TO STA.									ILLINOIS FED.	AID PROJECT		

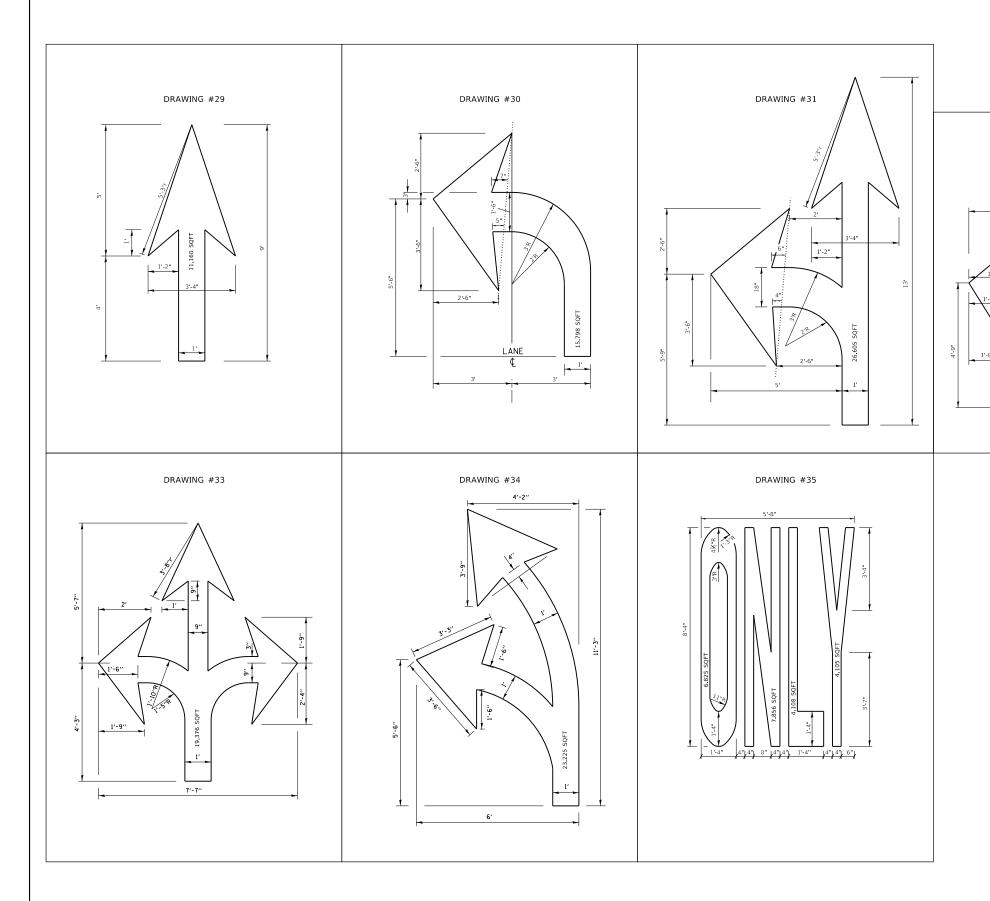




NOTE:

- 1. FOR BIKE LANE SYMBOLS ONLY,
 USE PRE-FORMED THERMOPLASTIC
 WITH A MINIMUM THICKNESS OF 90 MILS,
 MINIMUM SKID RESISTANCE VALUE OF 60 BPN,
 & A MINIMUM INDEX OF REFRACTION OF 1.50.
- 2. THE RESIDENT ENGINEER SHALL CONTACT MR. BEN GOMBERG AT 312-744-8093 AT LEAST ONE CALENDAR WEEK PRIOR TO INSTALLING BIKE LANE SYMBOLS.

TYPICAL BIKE LANE SYMBOLS
DRAWING #28



NOTE:

ALL MARKINGS SHALL BE SOLID WHITE UNLESS OTHERWISE NOTED IN THE PLANS

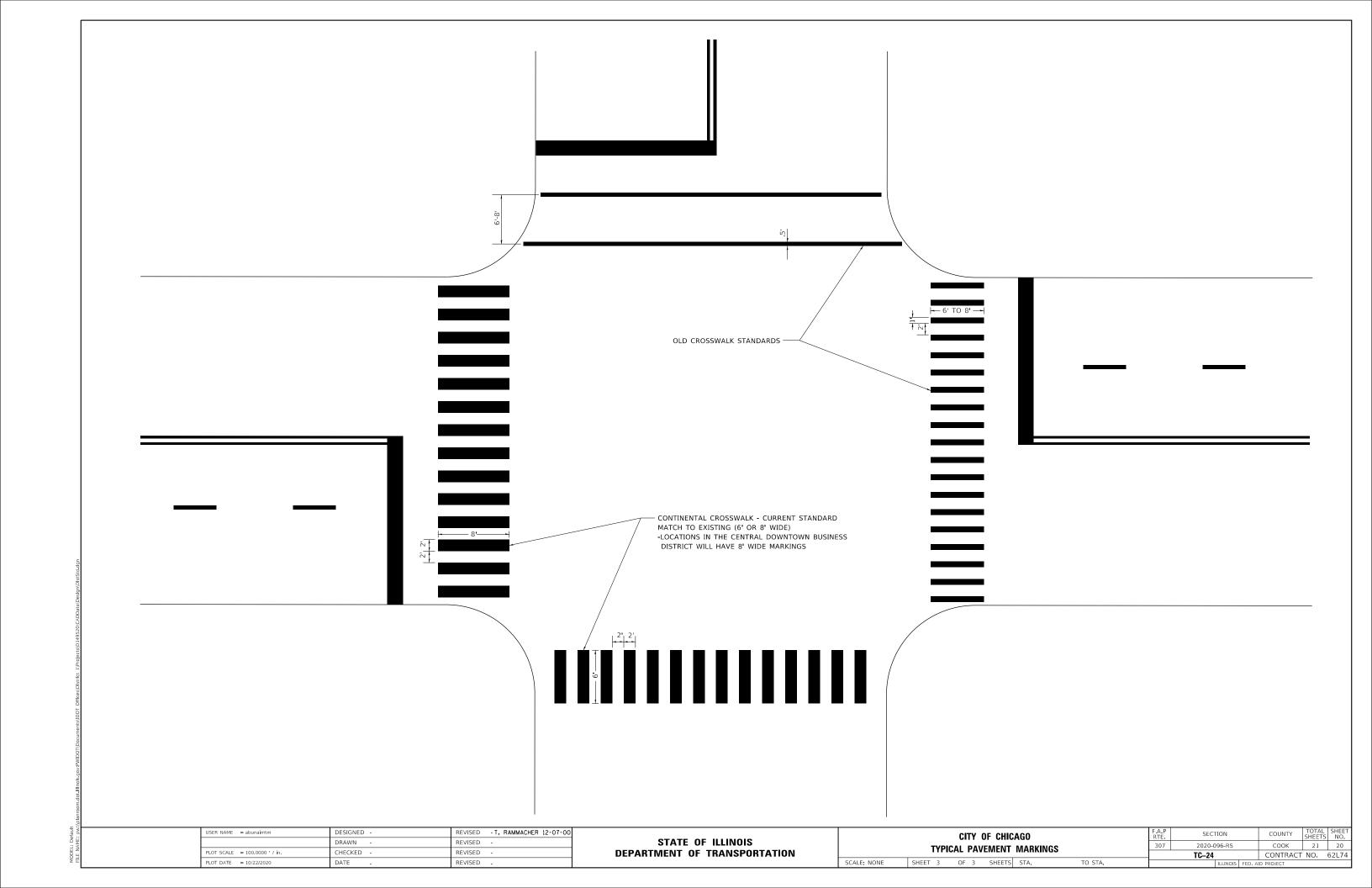
DRAWING #32

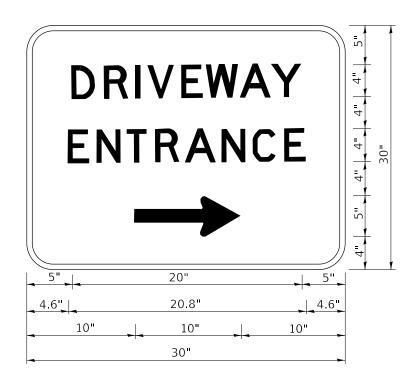
USER NAME = abunaimtm	DESIGNED -	REVISED - I. RAMMACHER 12-07-00
	DRAWN -	REVISED -
PLOT SCALE = 100.0000 / in.	CHECKED -	REVISED -
PLOT DATE = 10/22/2020	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE: NONE

CITY OF CHICAGO	F.A.P RTE.				TOTAL SHEETS	SHEET NO.
TYPICAL PAVEMENT MARKINGS	307	2020-096-RS		COOK	21	19
TITIOAL TAVEINENT MAININGS		TC-24		CONTRACT	NO.	62L74
SHEET 2 OF 3 SHEETS STA. TO STA	٨.	ILLINOIS	FED. AI	D PROJECT		





3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

- 1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
- 2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
- 3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

 USER NAME
 = abunaimtm
 DESIGNED REVISED C. JUCIUS 02-15-07

 DRAWN REVISED

 PLOT SCALE
 = 100,0000 ' / in.
 CHECKED REVISED

 PLOT DATE
 = 10/22/2020
 DATE REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION