

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
**PLANS FOR PROPOSED
FEDERAL AID HIGHWAY**

F. A. P. REC.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	05-00047-00-CH	WILL	036	001
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

CONTRACT #63033

INDEX OF SHEETS
SEE SHEET NO. 2

STATE STANDARDS
SEE SHEET NO. 2

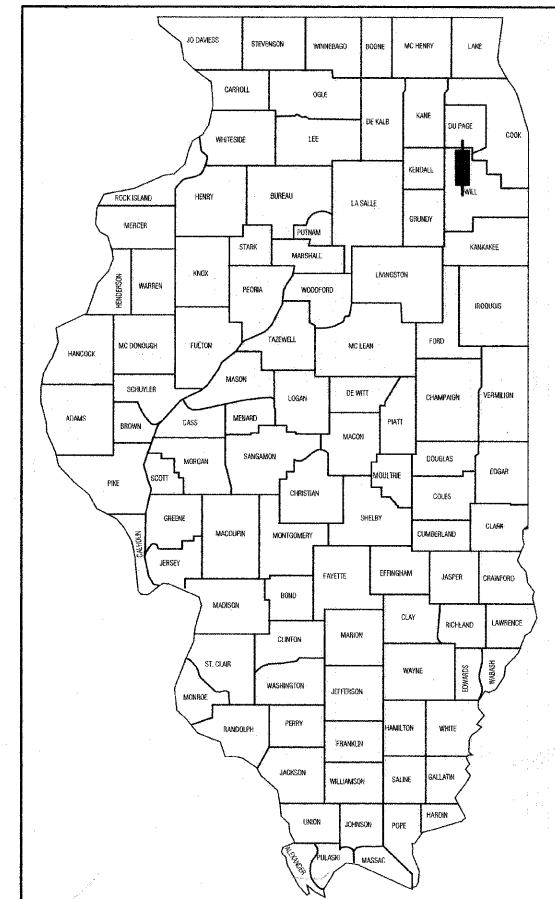
DESIGN DESIGNATION - IL RTE 53 ADT 29,000 (2030) - PRINCIPAL ARTERIAL
PV=25,810 SU=2,030 MU=1,160

2007 ADT - 26,600
2030 ADT - 29,000

EXISTING SPEED LIMIT - 45 MPH
PROPOSED SPEED LIMIT - 45 MPH

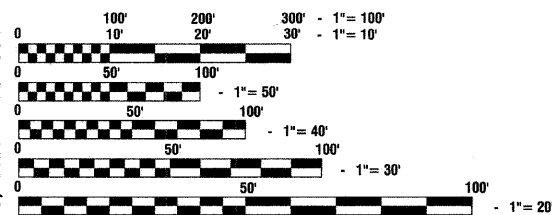
DESIGN PERIOD - 20 YEARS
DESIGN SPEED LIMIT - 50 MPH
STREET CLASSIFICATION - CLASS 1 ROAD

F.A.P. ROUTE 846
IL RTE 53 AT UNIVERSITY PARKWAY
INTERSECTION IMPROVEMENT
CMAQ PROJECT NO.: CMF-0846(016)
SECTION NO.: 05-00047-00-CH
VILLAGE OF ROMEOVILLE
WILL COUNTY
C-91-038-06



LOCATION OF SECTION INDICATED THIS: - [Symbol]

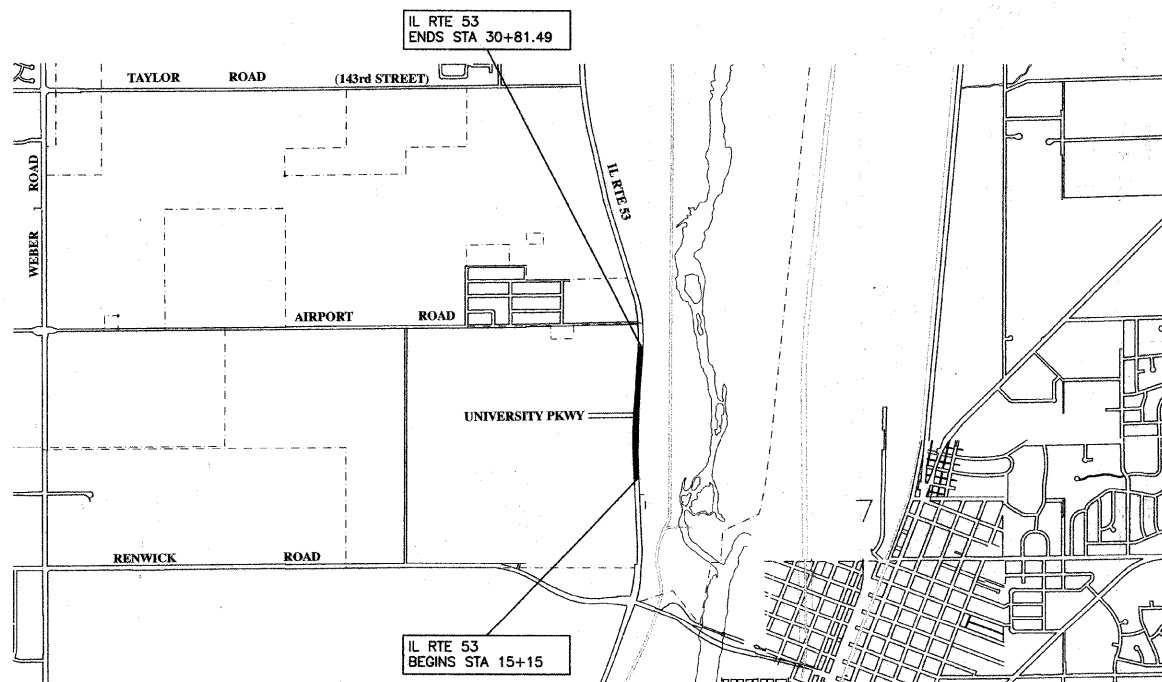
SCALES
PLAN - 1"=50'
PROFILE HORIZ. - 1"=50'
PROFILE VERT. - 1"=5'
CROSS SECTIONS HORIZ. - 1"=10'
CROSS SECTIONS VERT. - 1"=5'



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J. U. L. I. E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123 or 811

CONTRACT NO. 63033



TOWNSHIP 36 RANGE 10
LOCATION MAP - N.T.S.

GROSS LENGTH= 1566.49 FEET= 0.297 MILES
NET LENGTH= 1566.49 FEET= 0.297 MILES



STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

Approved: *D. J. Bell*
Village of Romeoville, Director of Public Works
Local Agency Position

Passed: *OCTOBER 20, 2008*
C. Holt CHRISTOPHER HOLT
District 1 Engineer of Local Roads & Streets

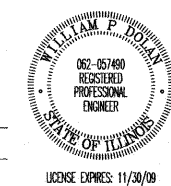
Released for Bid Based on Limited Review: *OCTOBER 21, 2008*
Diana M. O'Keefe
Deputy Director of Highways, Region 1 Engineer

DESCRIPTION OF IMPROVEMENT

THIS IMPROVEMENT CONSISTS OF EARTH EXCAVATION, PAVEMENT REMOVAL, WIDENING AND OVERLAY, CULVERT CONSTRUCTION, TRAFFIC SIGNALS, AND ALL INCIDENTAL WORK AS NECESSARY TO COMPLETE THE IMPROVEMENT SHOWN HEREIN AND AS DESCRIBED IN THE SPECIFICATIONS.

PREPARED BY OR UNDER THE DIRECT SUPERVISION OF:

Walter J. [Signature]
8/19/08



PRINTED BY THE AUTHORITY OF
THE STATE OF ILLINOIS

LICENSE EXPIRES: 11/30/08

FIELD ENGINEER: MELCHOR MANGOBA 847-705-4408
CONSULTANTS: ROBINSON ENGINEERING, LTD. 708-331-6700

F. A. P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	05-00047-00-CH	WILL	036	002
STA.		TO STA.		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

CONTRACT #63033

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- 22. PROPOSED CABLE PLAN
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- 280001-04 TEMPORARY EROSION CONTROL SYSTEMS
- 424001-05 CURB RAMPS FOR SIDEWALKS
- 542001-02 REINFORCED CONCRETE END SECTIONS WITH PARALLEL WINGWALLS
FOR PIPE CULVERTS 12" THRU 40" DIA AT RIGHT ANGLES WITH ROADWAY
- 542301-02 PRECAST REINFORCED CONCRETE FLARED END SECTION
- 542311-01 GRATING FOR CONCRETE FLARED END SECTION (24" THRU 54" PIPE)
- 602001-01 CATCH BASIN, TYPE A
- 602011-01 CATCH BASIN, TYPE C
- 602401-02 MANHOLE, TYPE A
- 602701-02 CAST IRON STEPS
- 604001-03 FRAME AND LIDS, TYPE 1
- 604036-02 GRATE, TYPE 8
- 604091-02 FRAME AND GRATE, TYPE 24
- 606001-04 CONCRETE CURB TYPE B & COMBINATION CONCRETE CURB & GUTTER
- 630001-08 STEEL PLATE BEAM GUARDRAIL
- 630201-06 PCC/HMA STABILIZATION AT STEEL PLATE BEAM GUARDRAIL
- 630301-05 SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS
- 631011-05 TRAFFIC BARRIER TERMINAL, TYPE 2
- 635006-03 REFLECTOR AND TERMINAL MARKER PLACEMENT
- 701101-02 OFF-RD MOVING OPERATIONS 2L, 2W, DAY ONLY
- 701421-02 LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY, FOR SPEEDS ≥ 45 MPH TO 55 MPH
- 701426-03 LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS, FOR SPEEDS ≥ 45 MPH
- 701701-06 URBAN LANE CLOSURE, MULTILANE INTERSECTION
- 701901-01 TRAFFIC CONTROL DEVICES
- 720001-01 SIGN PANEL MOUNTING DETAILS
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- 814006-02 DOUBLE HANDHOLES
- 857001-01 STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
- 862001-01 UNINTERRUPTABLE POWER SUPPLY (UPS)
- 873001-02 TRAFFIC SIGNAL GROUNDING & BONDING
- 877001-04 STEEL MAST ARM ASSEMBLY AND POLE 16' THRU 55'
- 878001-07 CONCRETE FOUNDATION DETAILS
- 880001-01 SPAN WIRE MOUNTED SIGNALS AND FLASHING BEACON INSTALLATION
- 880006-01 TRAFFIC SIGNAL MOUNTING DETAILS
- 886001-01 DETECTOR LOOP INSTALLATIONS

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**IL RTE 53 AT UNIVERSITY PARKWAY
INTERSECTION IMPROVEMENT
INDEX OF SHEETS
AND STATE STANDARDS**

SCALE: VERT. NA
HORIZ. NA
DATE 8-29-08

DRAWN BY RG
CHECKED BY WD

F. A. P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	05-00047-00-CH	WILL	036	003
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CONTRACT #63033

SUMMARY OF QUANTITIES					PAVEMENT	MARKINGS	SIGNS	LANDSCAPE	LIGHTS	SIGNALS	TRAINING
S.I.	CODE NO.	PAY ITEM	UNIT	QUAN	CONSTRUCTION TYPE CODE						
					1000	SFTY-1D	Y002-1C	Y003	Y030-1E	Y031-1F	Y080
	20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	30	30						
	20100500	TREE REMOVAL, ACRES	ACRE	0.42	0.42						
	20101200	TREE ROOT PRUNING	EACH	7	7						
	20101300	TREE PRUNING (1 TO 10 INCH DIAMETER)	EACH	3	3						
	20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	4	4						
	20200100	EARTH EXCAVATION	CU YD	3420	3420						
	20200200	ROCK EXCAVATION	CU YD	100	100						
	20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	330	330						
	20700420	POROUS GRANULAR EMBANKMENT, SUBGRADE	CU YD	330	330						
	20800150	TRENCH BACKFILL	CU YD	99	99						
	21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	4280	4280						
	25000210	SEEDING, CLASS 2A	ACRE	0.88	0.88						
	25000400	NITROGEN FERTILIZER NUTRIENT	POUND	79	79						
	25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	79	79						
	25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	79	79						
	25100630	EROSION CONTROL BLANKET	SQ YD	4280	4280						
	28000300	TEMPORARY DITCH CHECKS	EACH	1	1						
	28000400	PERIMETER EROSION BARRIER	FOOT	3000	3000						
	28000500	INLET AND PIPE PROTECTION	EACH	9	9						
	28000510	INLET FILTERS	EACH	4	4						
	28100101	STONE RIPRAP, CLASS A1	SQ YD	305	305						
	28100107	STONE RIPRAP, CLASS A4	SQ YD	305	305						
	28200200	FILTER FABRIC	SQ YD	305	305						
	35501327	HOT-MIX ASPHALT BASE COURSE, 10 3/4"	SQ YD	2879	2879						
	40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	1154	1154						
	40600300	AGGREGATE (PRIME COAT)	TON	12	12						
	40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGWAYS	TON	1	1						
	40600895	CONSTRUCTING TEST STRIP	EACH	2	2						
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL -- BUTT JOINT	SQ YD	272	272						
	40603595	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	TON	1292	1292						
	44000160	HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"	SQ YD	8860	8860						
	44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	77	77						
	44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	290	290						
	44201794	CLASS D PATCHES, TYPE III, 12 INCH	SQ YD	40	40						
	48101500	AGGREGATE SHOULDERS, TYPE B 6"	SQ YD	1023	1023						
	54213471	END SECTIONS 36"	EACH	2	2						
	54213669	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 24"	EACH	1	1						
	54247130	GRATING FOR CONCRETE FLARED END SECTION 24"	EACH	1	1						
	550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	7	7						
	550A0120	STORM SEWERS, CLASS A, TYPE 1 24"	FOOT	667	667						
	55100700	STORM SEWER REMOVAL 15"	FOOT	555	555						
	552A1300	STORM SEWERS JACKED IN PLACE, CLASS A 36"	FOOT	70	70						
	60107600	PIPE UNDERDRAINS 4"	FOOT	435	435						
	60204505	CATCH BASINS, TYPE A, 5'-DIAMETER, TYPE 8 GRATE	EACH	1	1						
	60208240	CATCH BASINS, TYPE C, TYPE 24 FRAME AND GRATE	EACH	1	1						
	60221100	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	2	2						
	60500050	REMOVING CATCH BASINS	EACH	3	3						
	60602400	CONCRETE GUTTER (SPECIAL)	FOOT	26	26						
	60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	161	161						

* - INDICATES SPECIALTY ITEMS

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

IL RTE 53 AT UNIVERSITY PARKWAY INTERSECTION IMPROVEMENT SUMMARY OF QUANTITIES

SCALE: VERT. NA
HORIZ. NA
DATE: 8-29-08

DRAWN BY: RG
CHECKED BY: WD

ROBINSON ENGINEERING, LTD.
17000 SOUTH PARK AVENUE SOUTH HOLLAND, ILLINOIS 60473
(708) 591-6700 FAX (708) 591-9828
ILLINOIS DESIGN FIRM REGISTRATION NO. 154401208 04161_02 QUAN-01

F. A. P. REC.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	05-00047-00-CH	WILL	036	004
STA.		TO STA.		
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CONTRACT #63033

SUMMARY OF QUANTITIES					PAVEMENT	MARKINGS	SIGNS	LANDSCAPE	LIGHTS	SIGNALS	TRAINING
S.I.	CODE NO.	PAY ITEM	UNIT	QUAN	CONSTRUCTION TYPE CODE						
					I000	SFTY-1D	Y002-1C	Y003	Y030-1E	Y031-1F	Y080
	60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	354	354						
*	63000000	STEEL PLATE BEAM GUARD RAIL, TYPE A	FOOT	137.5	137.5						
*	63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	2	2						
*	63100167	TRAFFIC BARRIER TERMINAL TYPE 1, SPECIAL (TANGENT)	EACH	2	2						
	63200310	GUARDRAIL REMOVAL	FOOT	287.5	287.5						
	67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6						
	67100100	MOBILIZATION	L SUM	1	1						
	70100310	TRAFFIC CONTROL AND PROTECTION, STANDARD 701421	L SUM	1	1						
	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1						
	70300100	SHORT-TERM PAVEMENT MARKING	FOOT	427	427						
	70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	3915	3915						
	70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	69	69						
	70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	1585	1585						
	72000100	SIGN PANEL - TYPE 1	SQ FT	7.5	7.5						
	72000200	SIGN PANEL - TYPE 2	SQ FT	25	25						
	72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	9	9						
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	109.2	109.2						
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	8579	8579						
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	487	487						
*	78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	92	92						
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	162	162						
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	122	122						
*	78001100	PAINT PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	72.8	72.8						
*	78001130	PAINT PAVEMENT MARKING - LINE 6"	FOOT	44	44						
*	78001140	PAINT PAVEMENT MARKING - LINE 8"	FOOT	128	128						
*	78001150	PAINT PAVEMENT MARKING - LINE 12"	FOOT	28	28						
*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	168	168						
*	78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	2	2						
*	81000600	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL	FOOT	461						461	
*	81000700	CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL	FOOT	70						70	
*	81000800	CONDUIT IN TRENCH, 3" DIA., GALVANIZED STEEL	FOOT	5						5	
*	81001000	CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL	FOOT	132						132	
*	81018500	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL	FOOT	154						154	
*	81018900	CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL	FOOT	230						230	
*	81400100	HANDHOLE	EACH	2						2	
*	81400200	HEAVY-DUTY HANDHOLE	EACH	4						4	
*	81400300	DOUBLE HANDHOLE	EACH	1						1	
*	81900200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	668						668	
*	85700200	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET	EACH	1						1	
*	87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO.14 3C	FOOT	505						505	
*	87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO.14 5C	FOOT	1886						1886	
*	87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO.14 7C	FOOT	283						283	
*	87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO.14 1 PAIR	FOOT	958						958	
*	87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	120						120	
*	87700160	STEEL MAST ARM ASSEMBLY AND POLE, 24 FT.	EACH	1						1	
*	87700180	STEEL MAST ARM ASSEMBLY AND POLE, 28 FT.	EACH	1						1	
*	87700220	STEEL MAST ARM ASSEMBLY AND POLE, 36 FT.	EACH	1						1	
*	87702930	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 40 FT.	EACH	1						1	
*	87800150	CONCRETE FOUNDATION, TYPE C	FOOT	4						4	

* - INDICATES SPECIALTY ITEMS

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ILLINOIS DEPARTMENT OF TRANSPORTATION

**IL RTE 53 AT UNIVERSITY PARKWAY
INTERSECTION IMPROVEMENT
SUMMARY OF QUANTITIES**

SCALE: VERT. NA
HORIZ. NA
DATE: 8-29-08

DRAWN BY: RG
CHECKED BY: WD

ROBINSON ENGINEERING, LTD.
1700 SOUTH PARK AVENUE SOUTH HOLLAND, ILLINOIS 60473
(708) 321-6700 FAX (708) 321-3828
ILLINOIS DESIGN FIRM REGISTRATION NO. 184001028 04161_02-QUAN-01

F. A. P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
B46	05-00047-00-CH	WILL	036	005
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CONTRACT #63033

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S.I.	CODE NO.	PAY ITEM	UNIT	QUAN	CONSTRUCTION TYPE CODE						
					1000	SFTY-1D	Y002-1C	Y003	Y030-1E	Y031-F	Y080
*	87800400	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	45							45
*	87800415	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	15							15
*	88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	5							5
*	88030050	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	3							3
*	88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	2							2
*	88200210	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	7							7
*	88500100	INDUCTIVE LOOP DETECTOR	EACH	3							3
*	88600100	DETECTOR LOOP, TYPE I	FOOT	292							292
*	88700200	LIGHT DETECTOR	EACH	2							2
*	88700300	LIGHT DETECTOR AMPLIFIER	EACH	1							1
*	89000100	TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1							1
*	89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1							1
*	89502380	REMOVE EXISTING HANDHOLE	EACH	3							3
*	89502385	REMOVE EXISTING CONCRETE FOUNDATION	EACH	7							7
	Z0001050	AGGREGATE SUBGRADE 12"	SQ YD	3854	3854						
	Z0076600	TRAINEES	HOOR	500							500
*	XX003661	ELECTRIC CABLE IN CONDUIT, COAXIAL	FOOT	307							307
	XX005656	INLET FILTER CLEANING	EACH	8	8						
*	XX003070	UNINTERRUPTABLE POWER SUPPLY	EACH	1							1
	X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	51.4	51.4						
	X0322584	REVTMENT MAT REMOVAL	SQ YD	212	212						
*	X0323153	ELECTRIC CABLE IN CONDUIT, GROUND, NO. 6 1C (GREEN)	FOOT	619							619
*	X0323371	VIDEO VEHICLE DETECTION, 1 CAMERA	EACH	1							1
*	X0325737	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1							1
	X0540000	BRICK PAVERS	SQ FT	360	360						
	X4067107	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	454	454						
*	X8050015	SERVICE INSTALLATION - POLE MOUNTED	EACH	1							1
*	X8730250	ELECTRIC CABLE IN CONDUIT, NO. 20 3/C, TWISTED, SHIELDED	FOOT	505							505
*	XX007252	ELECTRIC CABLE IN CONDUIT, NO. 16 3/C	FOOT	307							307
	Z0005215	HOT-MIX ASPHALT STABILIZATION 6" AT STEEL PLATE BEAM GUARD RAIL	SQ YD	106	106						

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846	05-00047-00-CH	WILL	036	006
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CONTRACT #63033

GENERAL NOTES

- 1 ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED TO BE THE LATEST STANDARDS OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION.
- 2 ITEMS OF WORK LISTED IN THE SUMMARY OF QUANTITIES WHICH ARE NOT SPECIFICALLY INDICATED IN THE PLANS SHALL BE PERFORMED AT LOCATIONS AS DIRECTED BY THE ENGINEER.
- 3 DRAINAGE STRUCTURE ELEVATIONS: GRADES OF SEWER LINES WERE DETERMINED FROM AVAILABLE PLANS AND SURVEYS. ACCORDINGLY, AS DIRECTED BY THE ENGINEER, THE INVERTS OF THE PROPOSED DRAINAGE WILL BE REVISED TO MEET EXISTING FIELD CONDITIONS.
- 4 THE TOP OF ALL STRUCTURES SHALL BE FLUSH WITH THE ADJACENT SURFACE OR AT THE INDICATED ELEVATIONS SHOWN ON THE PLANS.
- 5 FRAME ELEVATIONS ARE GIVEN ONLY TO ASSIST IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF THE STRUCTURE. FRAMES ON ALL NEW STRUCTURES WILL BE ADJUSTED TO THE FINAL ELEVATION OF THE AREA IN WHICH THEY ARE LOCATED AS PART OF THE STRUCTURE COST.
- 6 WHEN, IN THE CONSTRUCTION OPERATION, ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DITCHES, GUTTERS OR OTHER DRAINAGE STRUCTURES SO THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH DAY BY THE CONTRACTOR AT HIS EXPENSE. AT THE CONCLUSION OF THE CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES SHALL BE FREE FROM ALL DIRT AND DEBRIS. THIS WORK SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- 7 WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE OR PUBLIC DRAINS, SEWERS OR CATCH BASINS. HE SHALL PROVIDE FACILITIES TO TAKE IN ALL STORM WATER WHICH WILL BE RECEIVED BY THESE DRAINS & SEWERS AND DISCHARGE SAME. HE SHALL PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT, IF NECESSARY, AND A TEMPORARY OUTLET, AND BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER RECEIVED FROM THESE TEMPORARY SEWER CONNECTIONS UNTIL SUCH TIME AS THE PERMANENT CONNECTIONS WITH SEWERS ARE BUILT AND IN SERVICE. THIS WORK SHALL NOT BE PAID FOR DIRECTLY, BUT WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- 8 THE APPROXIMATE LOCATION OF KNOWN PUBLIC UTILITIES ARE SHOWN ON THE PLANS. HOWEVER, THE DEPARTMENT DOES NOT GUARANTEE ITS ACCURACY. PRIOR TO COMMENCING OPERATIONS ON THE PROJECT WHICH MAY IN ANY WAY CREATE THE POSSIBILITY OF INVOLVEMENT WITH EXISTING UTILITIES, THE CONTRACTOR SHALL CONTACT THE UTILITY COMPANY INVOLVED. ADJUSTMENT OF ALL PUBLIC UTILITIES WITHIN THE LIMITS OF THIS IMPROVEMENT WILL BE DONE BY THE RESPECTIVE OWNERS. NO ADDITIONAL COMPENSATION WILL BE ALLOWED DUE TO DELAYS OR INCONVENIENCE CAUSED BY THESE ADJUSTMENTS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY LOCATIONS OF UNDERGROUND INSTALLATION BEFORE STARTING CONSTRUCTION OPERATIONS. PLANS FOR THESE ADJUSTMENTS WILL BE ON FILE IN THE OFFICE OF THE COOK COUNTY HIGHWAY DEPARTMENT, BUREAU OF CONSTRUCTION FOR INFORMATION AND USE.
- 9 ALL TRENCHES PLACED IN STATE OR LOCAL RIGHT-OF-WAY SHALL BE BACKFILLED WITHIN 2 FEET OF PROPOSED PAVEMENT, DRIVEWAYS, AND SIDEWALKS WITH TRENCH BACKFILL ONLY ACCORDING TO SECTION 208 OF THE STANDARD SPECIFICATIONS.
- 10 ALL PAVEMENT STUBS SHALL BE ONE (1') FOOT UNLESS OTHERWISE NOTED.
- 11 THE CONTRACTOR SHALL PROTECT ALL TREES WITHIN AND ADJACENT TO THE CONSTRUCTION SITE DURING THE CLEARING AND SUBSEQUENT CONSTRUCTION OPERATIONS IN ACCORDANCE WITH SECTION 201 OF THE STANDARD SPECIFICATIONS. THOSE TREES TO BE REMOVED AS SHOWN IN THE PLANS SHALL BE DONE IN ACCORDANCE WITH SECTION 201 AND 202 OF THE STANDARD SPECIFICATIONS.
- 12 BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED).
- 13 THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES.
- 14 WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.
- 15 ALL STORM SEWERS FOR THIS PROJECT SHALL BE FURNISHED AND INSTALLED WITH RUBBER GASKETS IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR STORM SEWERS AS SPECIFIED.
- 16 ALL PIPE CONNECTIONS BETWEEN THE LATERAL SEWER LINES SHALL BE MADE BY MEANS OF A PRECAST WYE OR TEE SECTION. THIS SHALL NOT BE PAID FOR INDEPENDENTLY BUT SHALL BE CONSIDERED INCIDENTAL TO THE MAIN DRAIN STORM SEWER CONSTRUCTION.
- 17 THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK. THE CONTRACTOR SHALL ALSO CONTACT ROBINSON ENGINEERING (708) 331-6700, THE VILLAGE OF ROMEOVILLE (815) 886-1870, AND LEWIS UNIVERSITY (815) 836-5235 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- 18 ALL HOT-MIX ASPHALT PAVING SHALL FOLLOW DESIGNATED DRIVING LANES AS SHOWN IN STRIPING DETAILS. NO LONGITUDINAL PAVING JOINT OR SEAMS ARE ALLOWED WITHIN THE DRIVING LANES. ALL LONGITUDINAL PAVING JOINTS OR SEAMS WILL BE BETWEEN THE DRIVING LANES.
- 19 POROUS GRANULAR EMBANKMENT (PGE) SUBGRADE HAS BEEN PROVIDED FOR LOCATIONS WHERE SOILS TEND TO BE UNSTABLE WHEN WET. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH PGE WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE ENGINEER (BY USE OF A CONE PENETROMETER IN CONJUNCTION WITH THE IDOT SUBGRADE STABILITY MANUAL). IF UNSTABLE AND/OR UNSUITABLE MATERIALS ARE NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.
- 20 ALL PAVEMENT, CURB AND SIDEWALK REMOVALS SHALL BE MADE BY MEANS OF STRAIGHT SAW CUT JOINT. THE COST FOR SAW CUTTING SHALL BE INCIDENTAL TO THE CONTRACT.
- 21 CONTRACTOR SHALL BE RESPONSIBLE AT ALL TIMES FOR TRAFFIC CONTROL AND PROTECTION IN ACCORDANCE WITH THE IDOT STANDARD SPECIFICATIONS ADOPTED JANUARY 1, 2007, THE LATEST EDITION OF THE ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, AND THE STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS.
- 22 10' TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER TO EXISTING CURB AND GUTTER OR TO TAPER FROM 6" TO 0", UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEM OF WORK SPECIFIED.
- 23 ALL STORM SEWERS, UNLESS OTHERWISE NOTED, SHALL CONFORM TO THE STATE SPECIFICATIONS FOR REINFORCED CONCRETE CULVERT, STORM DRAIN AND SEWER PIPE A.A.S.H.T.O. DESIGNATION M170 (A.S.T.M. DESIGNATION C76), WITH A MINIMUM OF CLASS III.
- 24 CHERT AGGREGATE SHALL NOT BE ALLOWED IN THE MANUFACTURE OF STORM SEWERS, END SECTIONS, OR PRECAST DRAINAGE STRUCTURES.
- 25 IN COMPLIANCE WITH THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY (I.E.P.A.) FOR CONSTRUCTION PROJECT WHICH DISTURBS ONE ACRE OR MORE TOTAL LAND AREA, THE RESIDENT ENGINEER AND THE CONTRACTOR SHALL COOPERATIVELY DEVELOP A STORM WATER POLLUTION PREVENTION PLAN AS SPECIFIED IN THE SPECIAL PROVISION FOR NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) CONTAINED IN THE CONTRACT DOCUMENT.
- 26 IT IS ESTIMATED 287.5 LIN. FT. OF EXISTING GUARDRAIL WITH POSTS AND FITTINGS ARE TO BE REMOVED WITHIN THE LIMITS SHOWN ON THE PLAN OR AT THE DIRECTION OF THE ENGINEER. EXISTING RAILS, POSTS AND FITTINGS WILL NOT BE RE-USED WITHIN THE IMPROVEMENT. THE EXISTING GUARDRAIL SHALL BE REMOVED BY THE CONTRACTOR AND DISPOSED OF BY HIM AT HIS EXPENSE AND A SALVAGE VALUE OF THE REMOVED GUARDRAIL, POSTS AND FITTINGS SHALL BE REFLECTED IN THE CONTRACT UNIT BID PRICE PER LIN. FT. FOR GUARDRAIL REMOVAL.
- 27 TWO WEEKS PRIOR TO PLACING PERMANENT PAVEMENT MARKINGS, CONTACT CORA MATHIS, AREA TRAFFIC FIELD ENGINEER AT (815) 485-6475.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

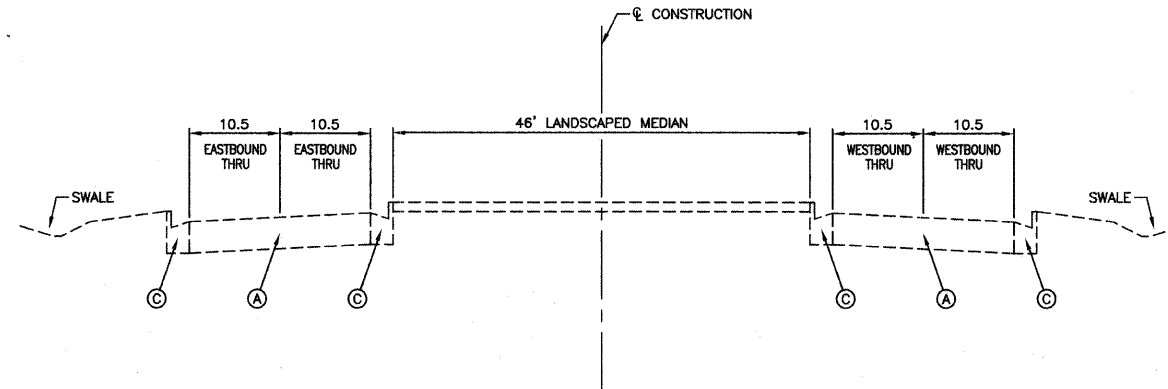
**IL RTE 53 AT UNIVERSITY PARKWAY
INTERSECTION IMPROVEMENT
GENERAL NOTES**

SCALE: VERT. NA
HORIZ. NA
DATE: 8-29-08

DRAWN BY: RG
CHECKED BY: WD

P. A. P. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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STA.		TO STA.		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

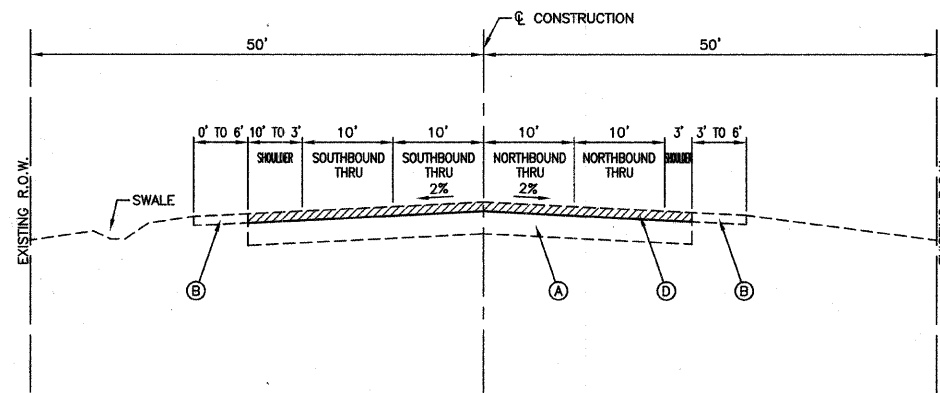
CONTRACT #63033



EXISTING CROSS SECTION
UNIVERSITY PARKWAY

LEGEND

- (A) EXISTING PAVEMENT
- (B) EXISTING AGGREGATE SHOULDER
- (C) EXISTING TYPE B-6.12 CURB AND GUTTER
- (D) HOT-MIX ASPHALT SURFACE REMOVAL - 2 3/4"
- (E) AGGREGATE SUBGRADE - 12"
- (F) HOT-MIX ASPHALT BASE COURSE, 10 3/4"
- (G) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50-3/4"
- (H) POLYMERIZED HOT MIX ASPHALT SURFACE COURSE, MIX "F", N90-2"
- (I) AGGREGATE SHOULDER, TYPE B-6"
- (J) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (K) TOPSOIL FURNISH AND PLACE - 4"



EXISTING CROSS SECTION
IL ROUTE 53
STA 15+15 TO STA 30+81.49

REVISIONS	
NAME	DATE

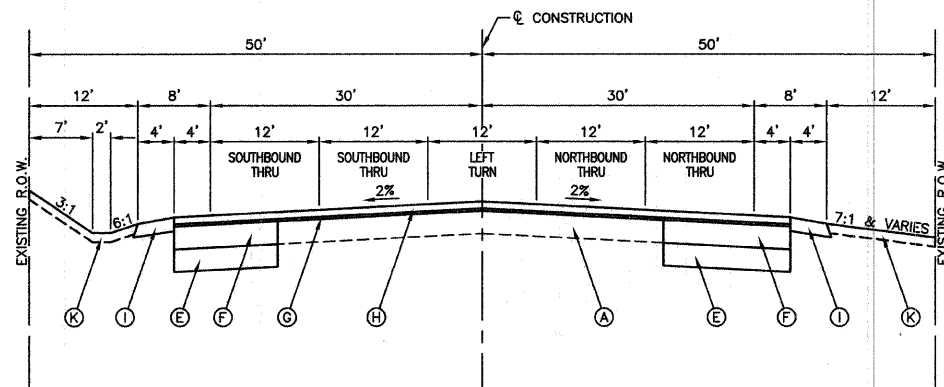
ILLINOIS DEPARTMENT OF TRANSPORTATION

**IL RTE 53 AT UNIVERSITY PARKWAY
INTERSECTION IMPROVEMENT
EXISTING TYPICAL CROSS SECTIONS**

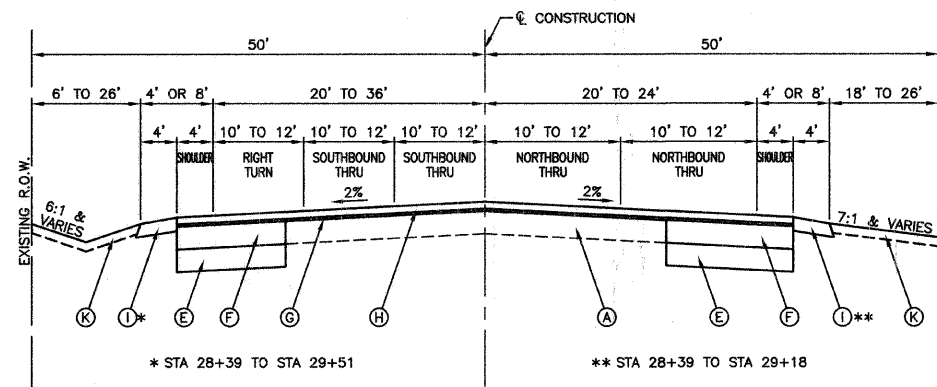
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STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	

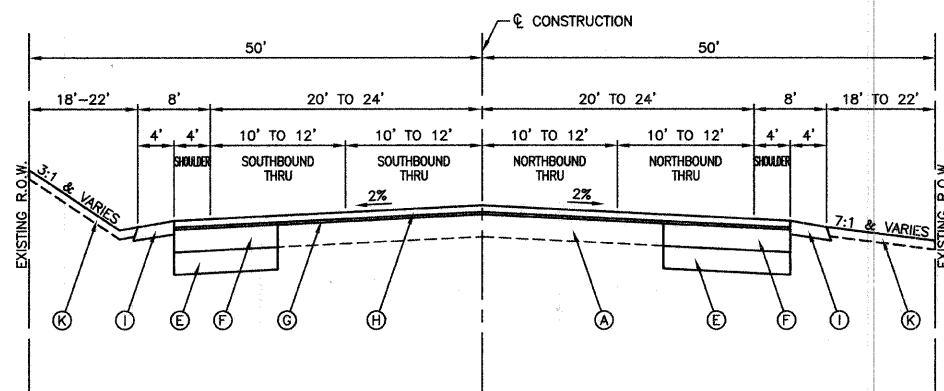
CONTRACT #63033



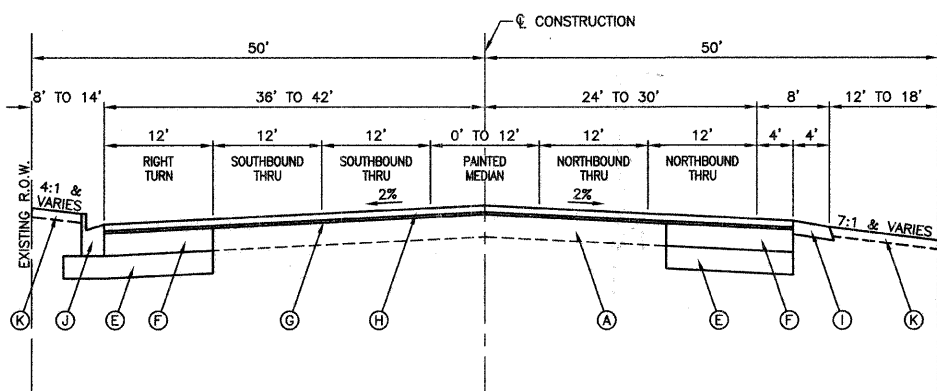
PROPOSED TYPICAL CROSS SECTION
IL ROUTE 53
STA 20+15 TO STA 25+00



PROPOSED TYPICAL CROSS SECTION
IL ROUTE 53
STA 28+39 TO STA 30+81.49



PROPOSED TYPICAL CROSS SECTION
IL ROUTE 53
STA 15+15 TO STA 20+15



PROPOSED TYPICAL CROSS SECTION
IL ROUTE 53
STA 25+00 TO STA 28+39

LEGEND

- (A) EXISTING PAVEMENT
- (B) EXISTING AGGREGATE SHOULDER
- (C) EXISTING TYPE B-6.12 CURB AND GUTTER
- (D) HOT-MIX ASPHALT SURFACE REMOVAL - 2 3/4"
- (E) AGGREGATE SUBGRADE - 12"
- (F) HOT-MIX ASPHALT BASE COURSE, 10 3/4"
- (G) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50-3/4"
- (H) POLYMERIZED HOT MIX ASPHALT SURFACE COURSE, MIX "F", N90-2"
- (I) AGGREGATE SHOULDER, TYPE B-6"
- (J) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (K) TOPSOIL FURNISH AND PLACE - 4"

EARTHWORK QUANTITIES

TOTAL CUT	=	3420	CY
TOTAL TOPSOIL (ESTIMATED)	=	960	CY
TOTAL AVAILABLE CUT TO FILL	=	2460	CY
TOTAL FILL	=	27	CY
CUT TO FILL (15% SHRINKAGE)	=	31	CY

ANTICIPATED UNDERCUT AREAS

LOCATION	RECOMMENDATION*	OFFSET	REASON
18+50 TO 24+00	REMOVE 12-INCHES	RT	HIGH MOISTURE

*REPLACE WITH PGES (POROUS GRANULAR EMBANKMENT, SUBGRADE), AREAS AND DEPTHS TO BE VERIFIED

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIX TYPE	AC TYPE	AIR VOIDS
POLYMERIZED HOT MIX ASPHALT SURFACE COURSE, MIX "F", N90(L-9.5mm), 2"	SBS/SBR PG 70-22	4% AT 90 GYRATIONS
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"	SBS/SBR PG 76-28/22	4% AT 50 GYRATIONS
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19mm), 10 3/4" (IN 3" LIFTS)	PG 64-22*	4% AT 90 GYRATIONS
CLASS D PATCHES (HMA BINDER IL-19mm), 12" (IN 4" LIFTS)	PG 64-22*	4% AT 70 GYRATIONS

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN
* WHEN RIPRAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22

REVISIONS

NAME	DATE

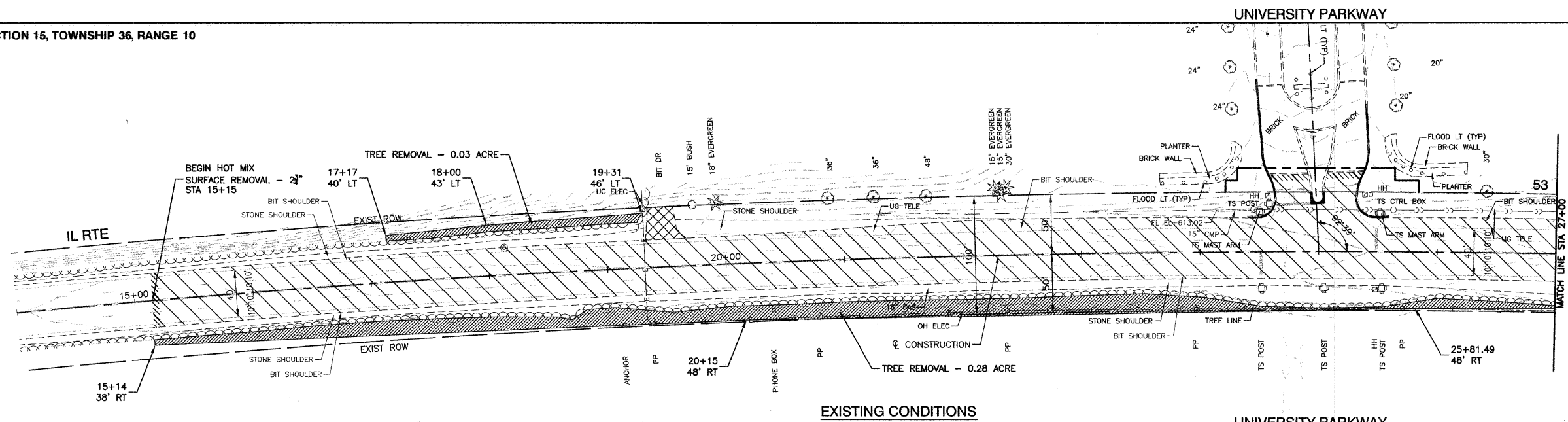
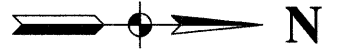
ILLINOIS DEPARTMENT OF TRANSPORTATION

IL RTE 53 AT UNIVERSITY PARKWAY
INTERSECTION IMPROVEMENT
PROPOSED TYPICAL CROSS SECTIONS

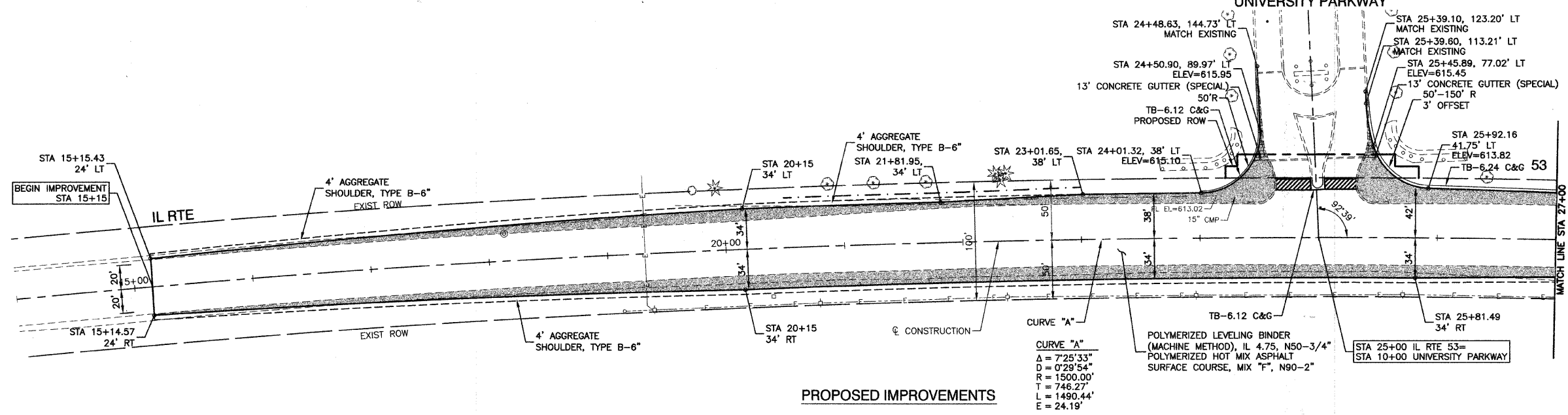
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HORIZ. NA
DATE: 8-29-08
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CHECKED BY: WD

F. A. P. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	05-00047-00-CH	WILL	036	009
STA. 15+15		TO STA. 27+00		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

CONTRACT #63033



EXISTING CONDITIONS



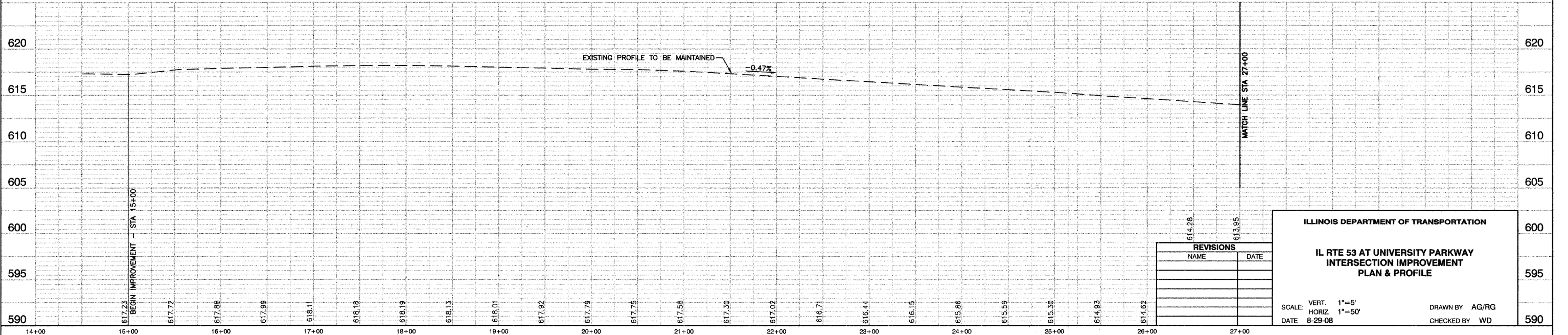
PROPOSED IMPROVEMENTS

LEGEND

- CURB AND GUTTER REMOVAL
- DRIVEWAY PAVEMENT REMOVAL
- BRICK PAVERS
- TREE REMOVAL
- TREE REMOVAL (ACRES)
- HMA SURFACE REMOVAL - BUTT JOINT
- HOT MIX ASPHALT SURFACE REMOVAL, 2-3/4"
- AGGREGATE BASE COURSE, 12"
- HOT MIX ASPHALT, BASE COURSE, 10-3/4"
- PROPOSED SWALE
- CLASS "D" PATCHES, TYPE III, 12"

NOTES

1. TREES SHALL BE REMOVED WITHIN THE ROW/EASEMENT AS NECESSARY TO CONSTRUCT THE PROPOSED CROSS ROAD CULVERT (SEE DRAINAGE PLANS)



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**IL RTE 53 AT UNIVERSITY PARKWAY
INTERSECTION IMPROVEMENT
PLAN & PROFILE**

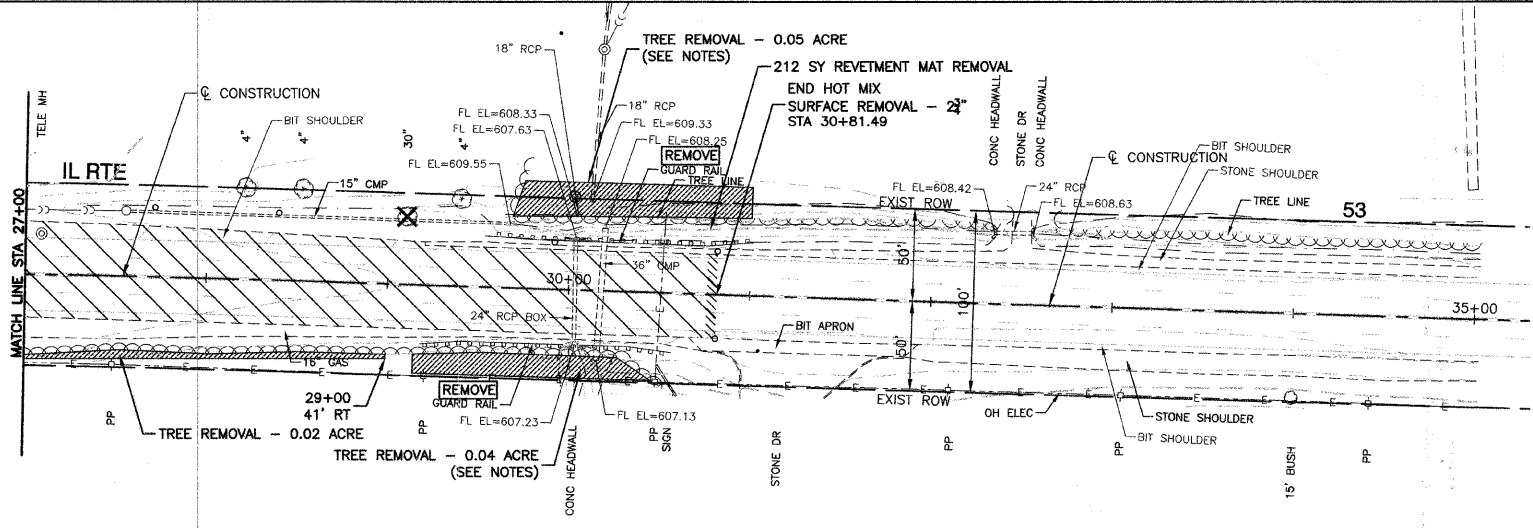
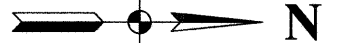
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DATE 8-29-08

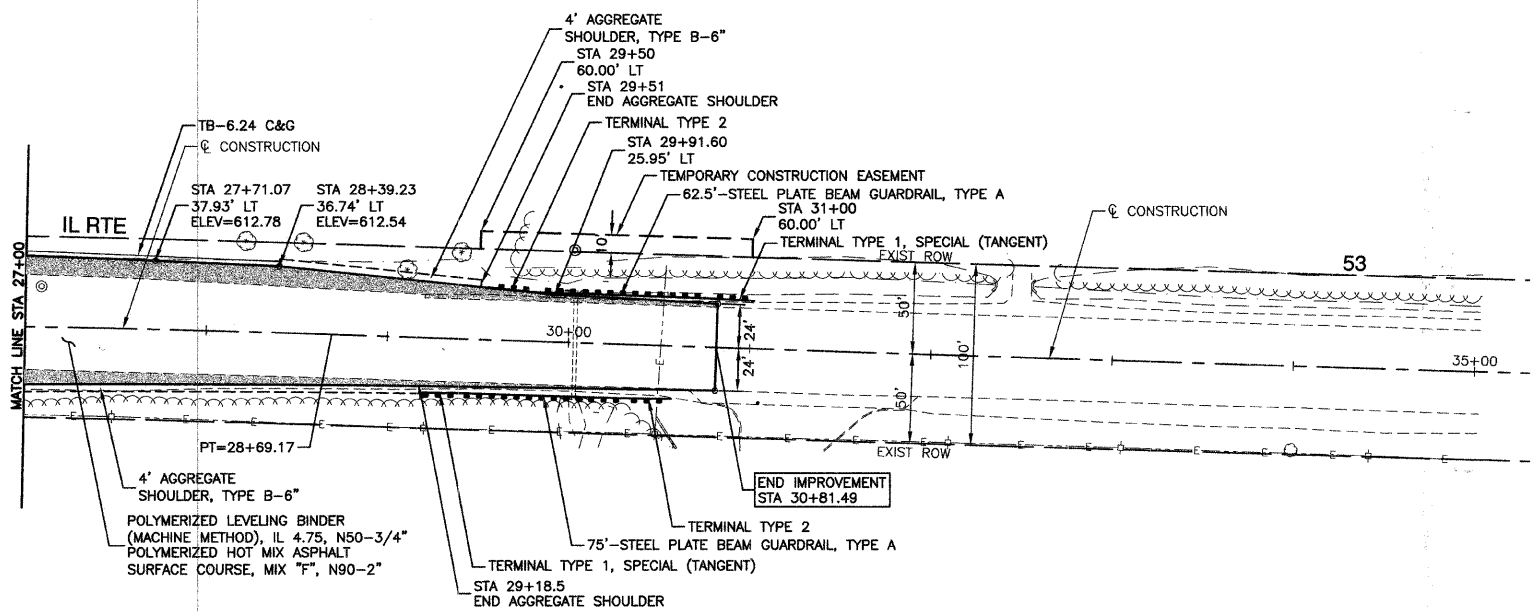
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F. A. P. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	05-00047-00-CH	WILL	036	010
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EXISTING CONDITIONS



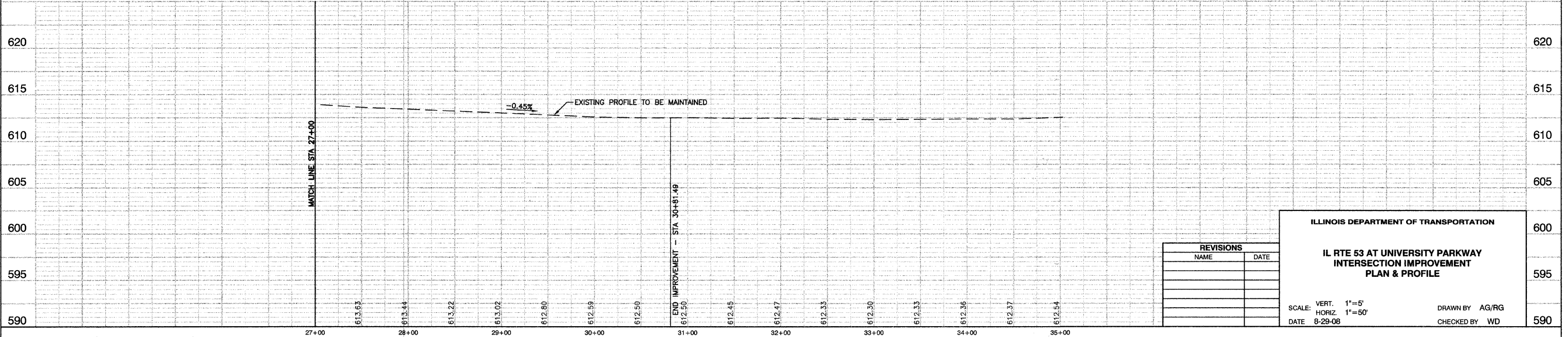
PROPOSED IMPROVEMENTS

LEGEND

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**IL RTE 53 AT UNIVERSITY PARKWAY
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PLAN & PROFILE**

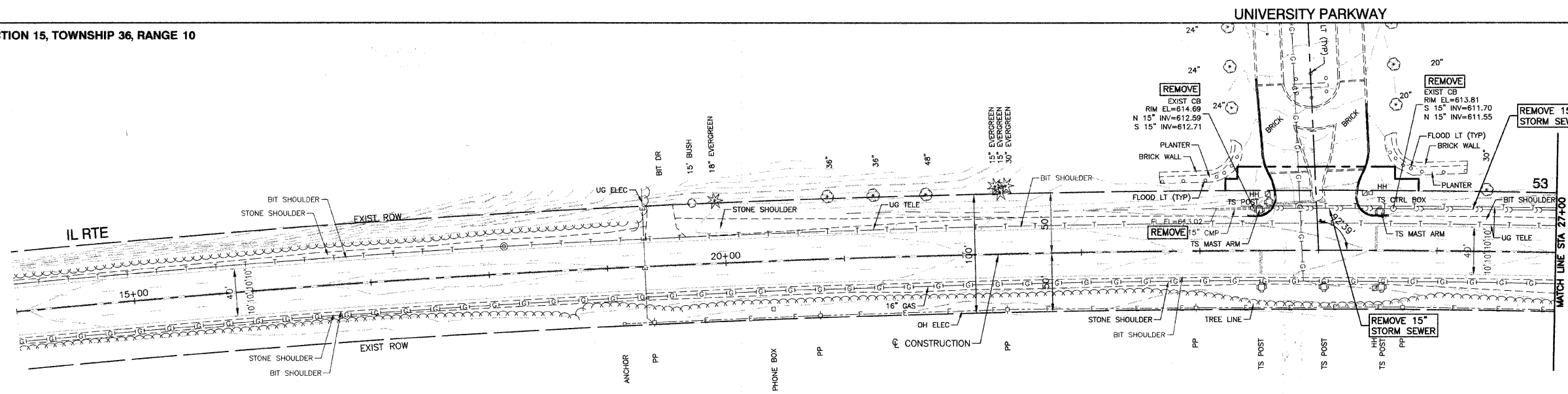
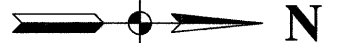
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DATE 8-29-08

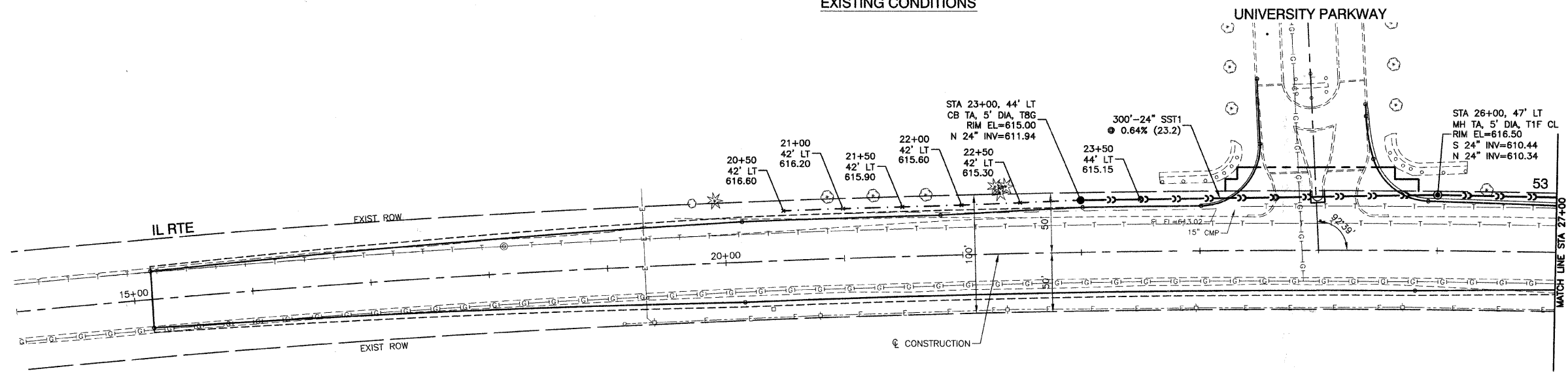
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STA. 15+15		TO STA. 27+00		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

CONTRACT #63033

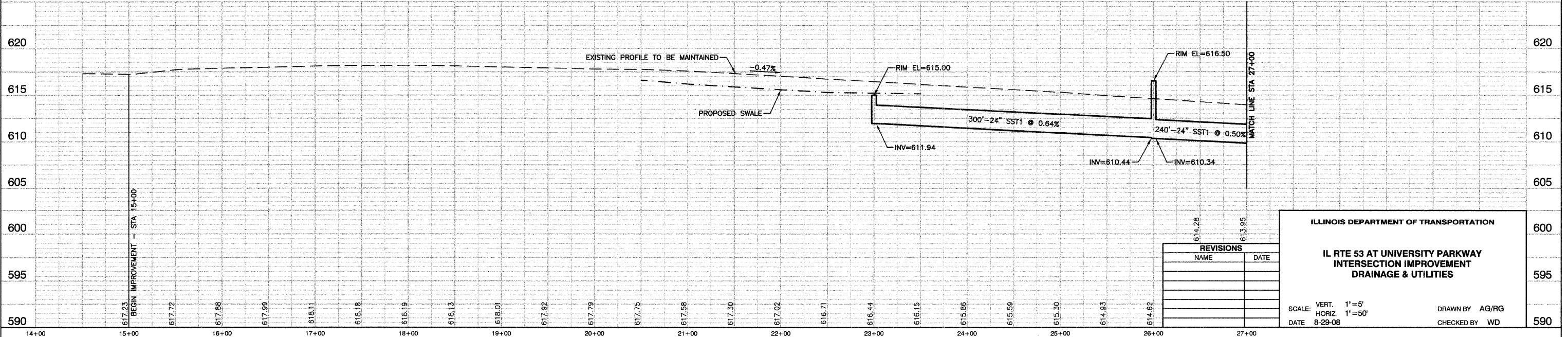


EXISTING CONDITIONS



PROPOSED IMPROVEMENTS

- NOTES**
1. ALL RIM ELEVATIONS OF STRUCTURES IN THE PROPOSED CURB LINE ARE GIVEN TO THE EDGE OF PAVEMENT. ALL OTHER RIM ELEVATIONS ARE GIVEN TO THE CENTER OF STRUCTURES. ALL CALL-OUTS REFERENCE THE CENTERLINE OF CONSTRUCTION.
 2. (XX.X) DENOTES CUBIC YARDS OF TRENCH BACK-FILL



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

IL RTE 53 AT UNIVERSITY PARKWAY INTERSECTION IMPROVEMENT DRAINAGE & UTILITIES

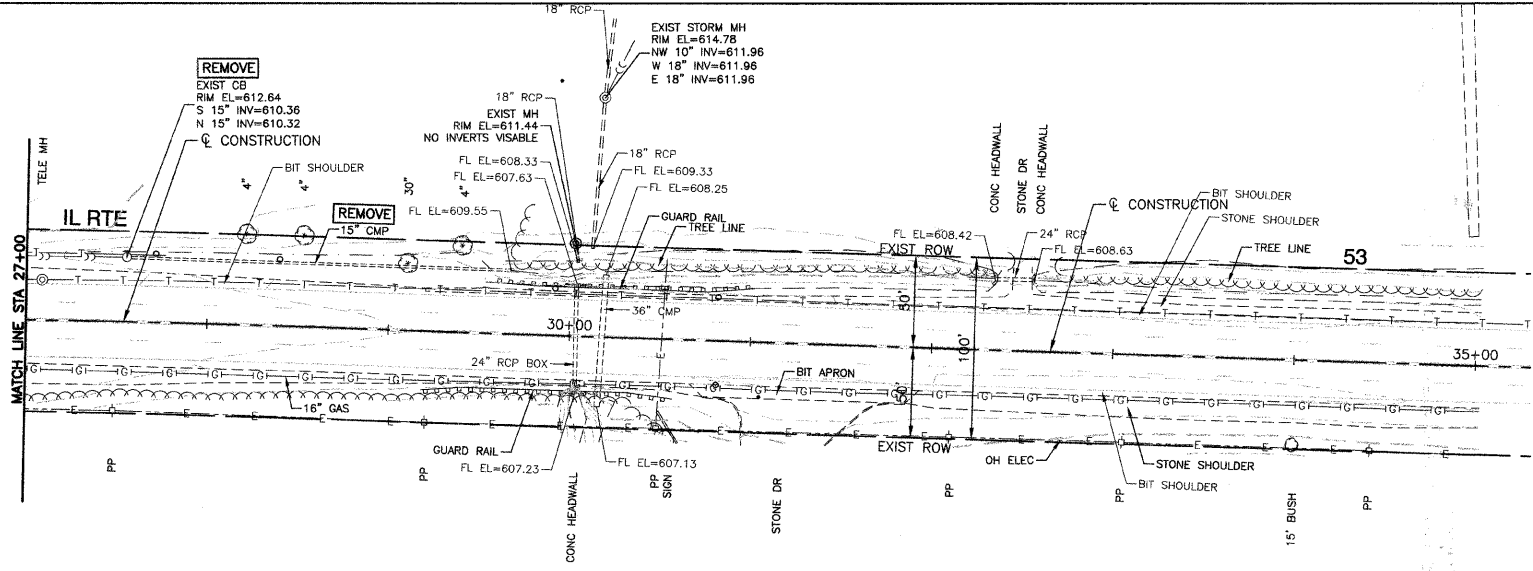
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DATE 8-29-08

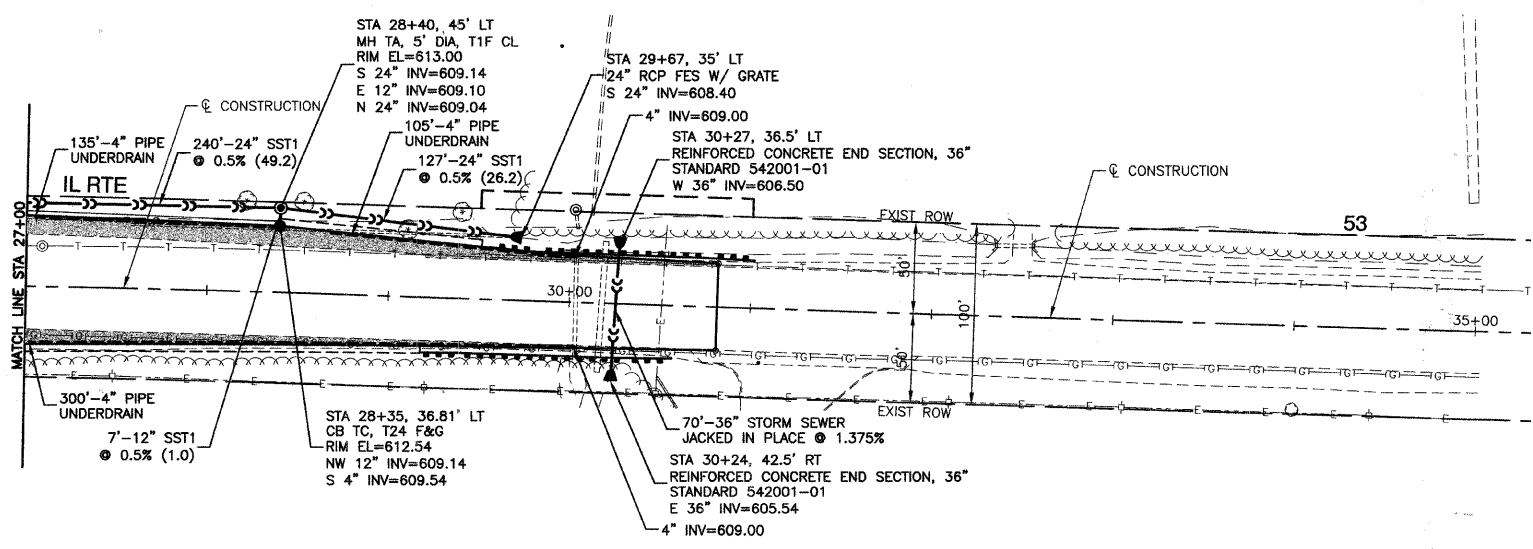
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P. A. P. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	05-00047-00-CH	WILL	036	012
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FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

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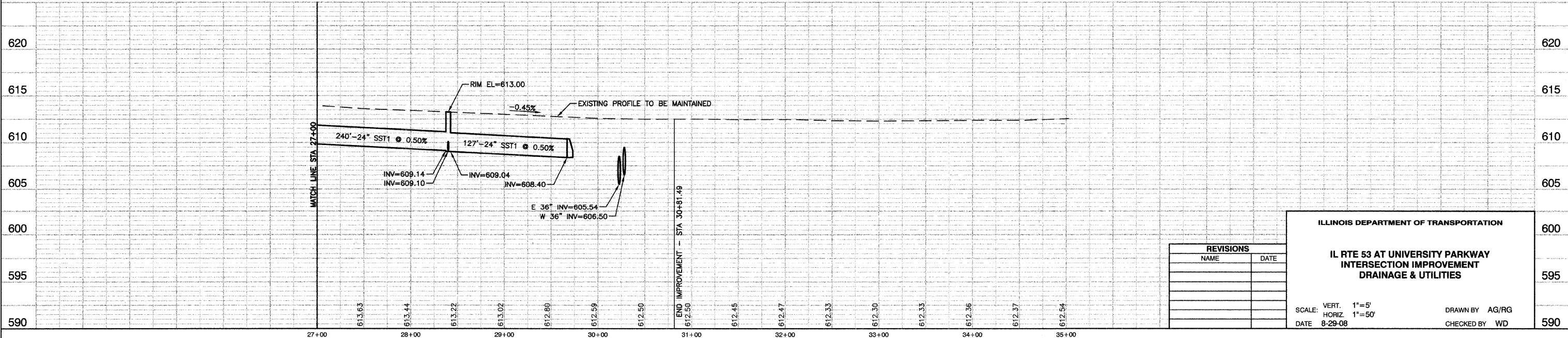
EXISTING CONDITIONS



PROPOSED IMPROVEMENTS

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3. ALL 4\"/>



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**IL RTE 53 AT UNIVERSITY PARKWAY
INTERSECTION IMPROVEMENT
DRAINAGE & UTILITIES**

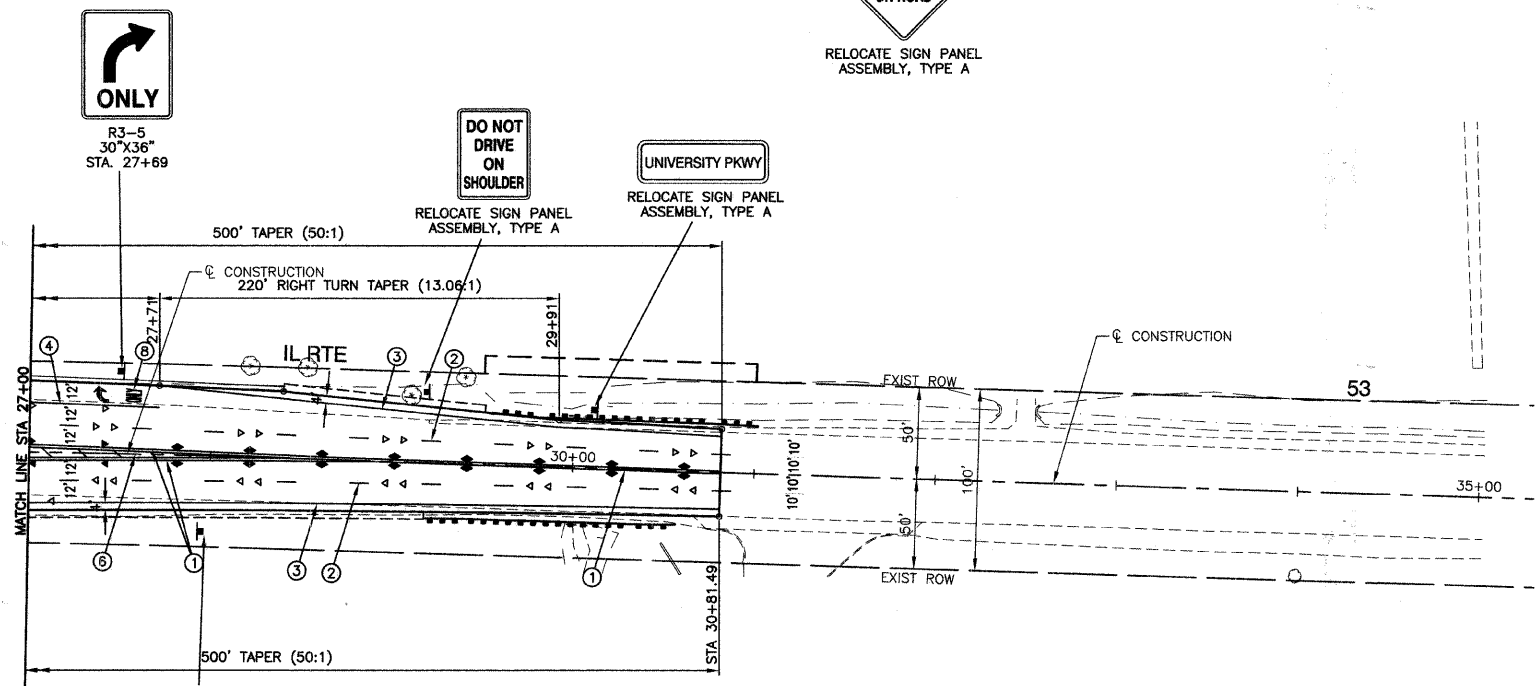
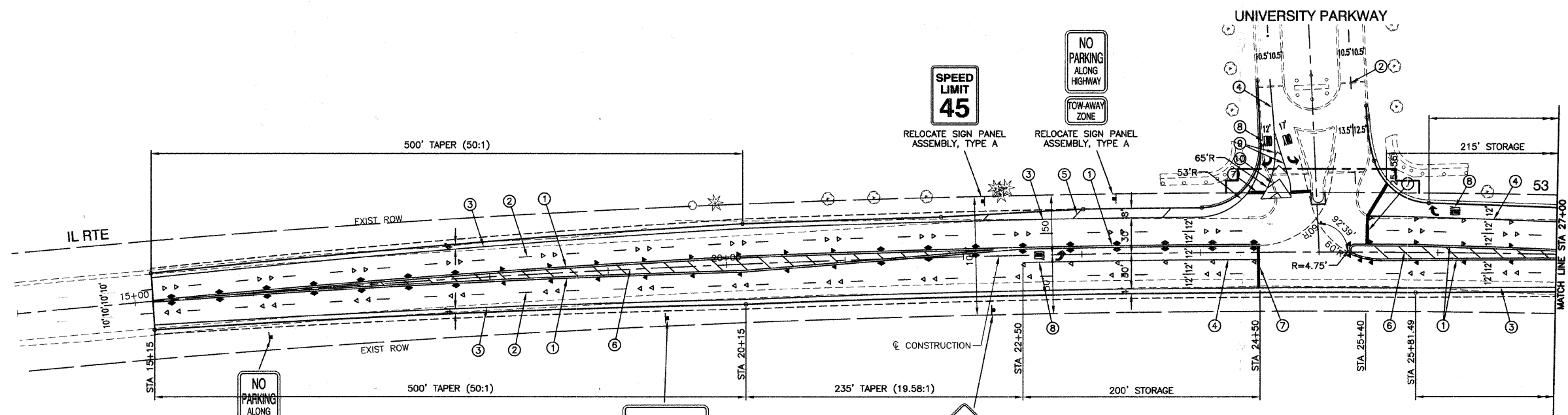
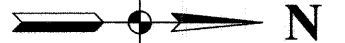
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P. A. P. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT #63033



PAVEMENT MARKING LEGEND

- ① DOUBLE 4" YELLOW CENTERLINE (11" C/C)
- ② 4" WHITE SKIP - DASH LINE (10' LINE - 30' SPACE)
- ③ 4" WHITE EDGE LINE
- ④ 6" WHITE LANE LINE
- ⑤ 12" WHITE DIAGONAL LINE (75' C/C)
- ⑥ 12" YELLOW DIAGONAL LINE (20' C/C)
- ⑦ 24" WHITE STOP BAR
- ⑧ WHITE LETTERS & SYMBOLS - LARGE
- ⑨ 8" WHITE LINE
- ⑩ 12" WHITE DIAGONAL LINE (10' C/C)
- ▲ ONE-WAY AMBER MARKER 40' C/C UNLESS OTHERWISE INDICATED
- ◄ ONE-WAY CRYSTAL MARKER 80' C/C UNLESS OTHERWISE INDICATED
- ◆ TWO-WAY AMBER MARKER 40' C/C UNLESS OTHERWISE INDICATED

NOTES

1. ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED.
2. SPACING OF 12" YELLOW DIAGONALS IN THE MEDIAN NORTH OF UNIVERSITY PARKWAY SHALL BE REDUCED TO ALLOW FOR 5 DIAGONALS.
3. ALL PAVEMENT MARKINGS OVER BRICK PAVERS SHALL BE PAINT

REVISIONS	
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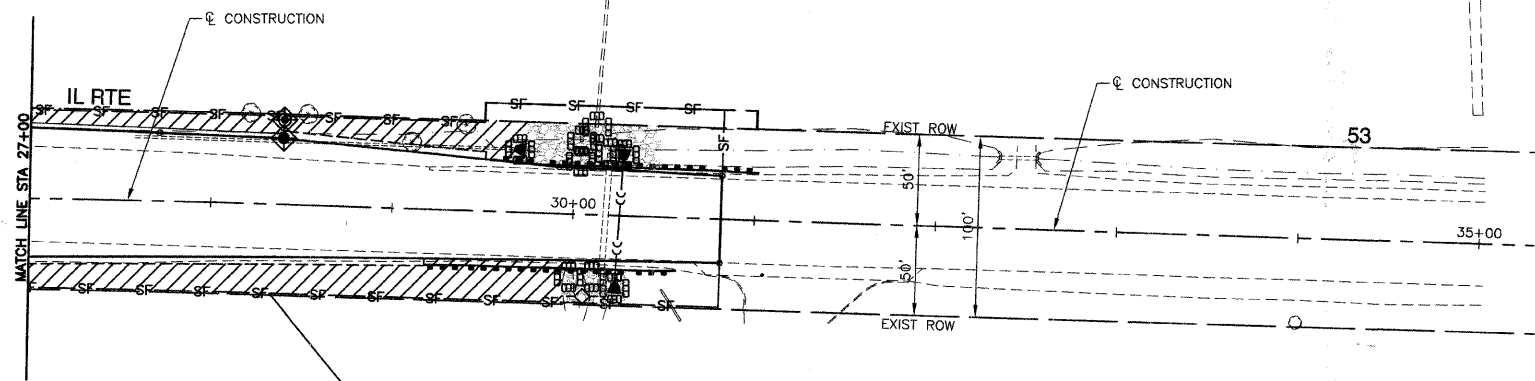
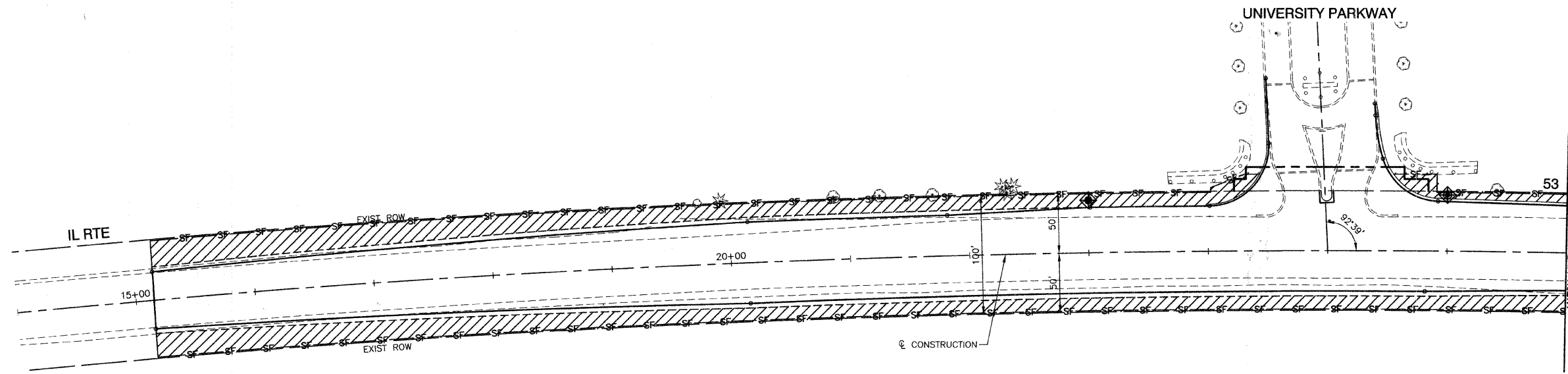
ILLINOIS DEPARTMENT OF TRANSPORTATION

IL RTE 53 AT UNIVERSITY PARKWAY INTERSECTION IMPROVEMENT PAVEMENT MARKING & SIGNING







SCALE: VERT. NA
 HORIZ. 1"=50'
 DATE: 8-29-08
 DRAWN BY: AG/RG
 CHECKED BY: WD

P. A. P. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	05-00047-00-CH	WILL	036	014
STA. 15+15		TO STA. 30+81.49		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

CONTRACT #63033



LEGEND

-  TOPSOIL FURNISH AND PLACE, 4" SEEDING, CLASS 2A WITH EROSION CONTROL BLANKET NITROGEN FERTILIZER NUTRIENT PHOSPHORUS FERTILIZER NUTRIENT POTASSIUM FERTILIZER NUTRIENT
-  FILTER FABRIC
STONE RIPRAP, CLASS A1
STONE RIPRAP, CLASS A4
-  PERIMETER EROSION BARRIER
-  INLET AND PIPE PROTECTION
-  INLET FILTER
-  TEMPORARY DITCH CHECK

REVISIONS	
NAME	DATE

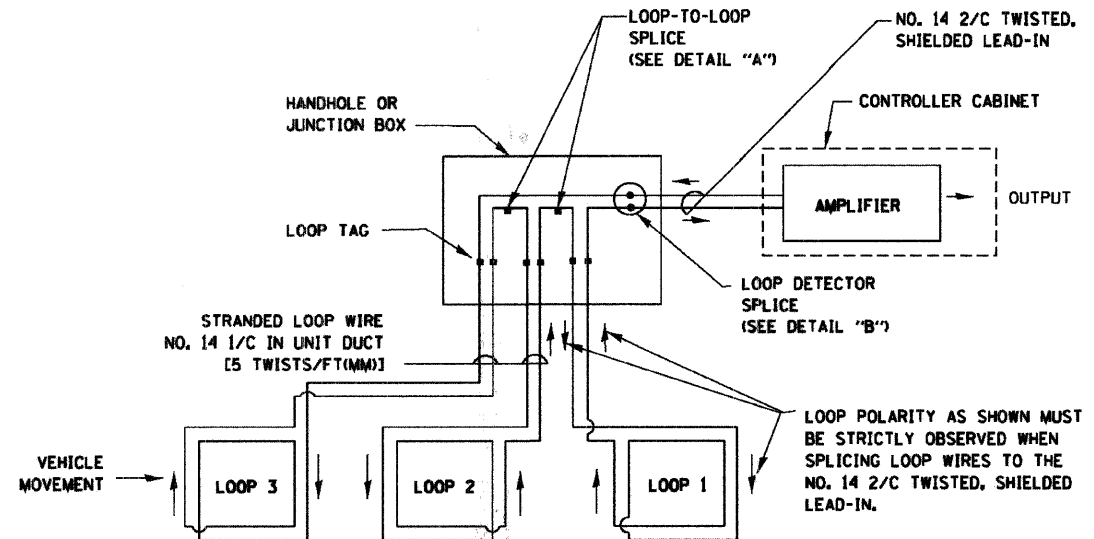
ILLINOIS DEPARTMENT OF TRANSPORTATION

**IL RTE 53 AT UNIVERSITY PARKWAY
INTERSECTION IMPROVEMENT
LANDSCAPING & EROSION CONTROL**

SCALE: VERT. NA
HORIZ. 1"=50'
DATE: 8-29-08
DRAWN BY: AG/RG
CHECKED BY: WD

LOOP DETECTOR NOTES

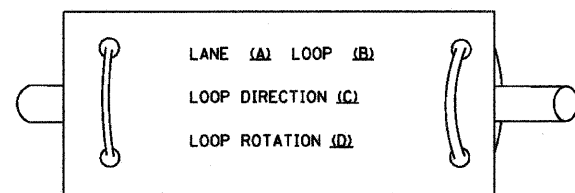
- EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE UNIT DUCT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). UNIT DUCT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVESHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.



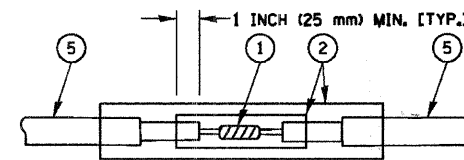
DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm), IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

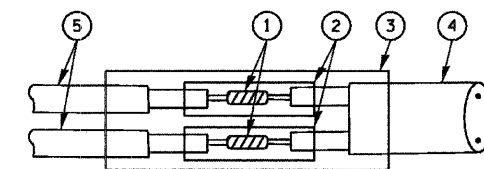
LOOP LEAD-IN CABLE TAG



- LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.



**DETAIL "A"
LOOP-TO-LOOP SPLICE**



**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

LOOP DETECTOR SPLICE

- WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- NO. 14 2/C TWISTED, SHIELDED CABLE.
- LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

REVISIONS	
NAME	DATE
CADD	5/30/00
ADD NOTE NO. 8	11/12/01
BUREAU OF TRAFFIC	1-01-02

ILLINOIS DEPARTMENT OF TRANSPORTATION
**DISTRICT ONE
STANDARD TRAFFIC SIGNAL
DESIGN DETAILS**

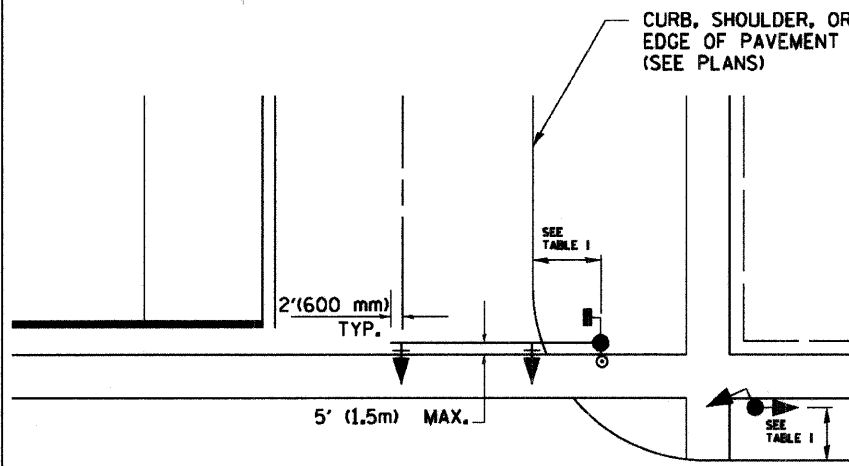
SCALE: NONE

DRAWN BY: RWP
DESIGNED BY: DAD
CHECKED BY: DAZ
SHEET 1 OF 4

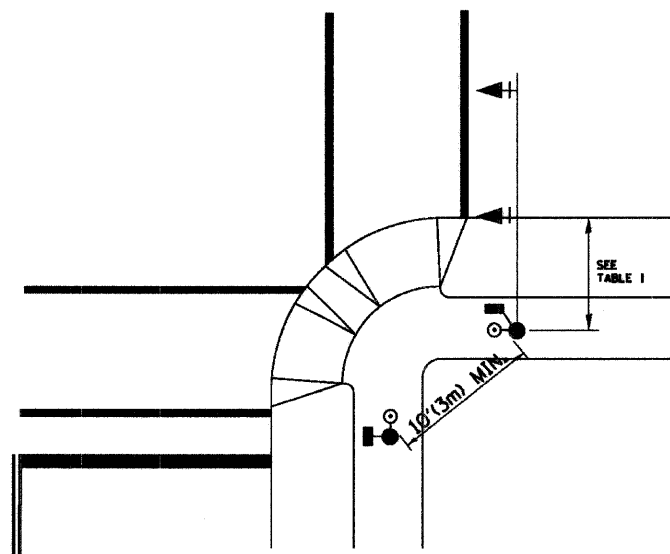
CONTRACT #63033

TRAFFIC SIGNAL MAST ARM AND POST

MAST ARM MOUNTED SIGNAL IN PROPOSED & FUTURE SIDEWALK AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNAL AND PUSHBUTTON DETECTOR



PEDESTRIAN SIGNAL PUSHBUTTON



RECOMMENDED PUSHBUTTON LOCATIONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHALL BE IN ACCORDANCE WITH THE CURRENT MUTCD (SEE NOTE 1). TO MEET MUTCD REQUIREMENTS, PEDESTRIAN SIGNAL PUSHBUTTONS MAY HAVE TO BE MOUNTED ON A SEPARATE POST.

NOTES:

- AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS WITH PEDESTRIAN ACTUATION, EACH PUSHBUTTON SHALL ACTIVATE BOTH THE WALK INTERVAL AND THE ACCESSIBLE PEDESTRIAN SIGNALS.

AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS, PUSHBUTTONS SHOULD CLEARLY INDICATE WHICH CROSSWALK SIGNAL IS ACTUATED BY EACH PUSHBUTTON. PUSHBUTTONS AND TACTILE ARROWS SHOULD HAVE HIGH VISUAL CONTRAST (SEE THE DEPARTMENT OF JUSTICE'S AMERICANS WITH DISABILITIES ACT STANDARDS FOR ACCESSIBLE DESIGN, 1991). TACTILE ARROWS SHOULD POINT IN THE SAME DIRECTION AS THE ASSOCIATED CROSSWALK. AT CORNERS OF SIGNALIZED LOCATIONS WITH ACCESSIBLE PEDESTRIAN SIGNALS WHERE PEDESTRIAN PUSHBUTTONS ARE PROVIDED, THE PUSHBUTTONS SHOULD BE SEPARATED BY THE DISTANCE OF AT LEAST 10 FT (3m). THIS ENABLES PEDESTRIANS WHO HAVE VISUAL DISABILITIES TO DISTINGUISH AND LOCATE THE APPROPRIATE PUSHBUTTON.

PUSHBUTTONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHOULD BE LOCATED AS FOLLOWS:
 - A: ADJACENT TO A LEVEL ALL-WEATHER SURFACE TO PROVIDE ACCESS FROM A WHEELCHAIR, AND WHERE THERE IS AN ALL WEATHER SURFACE, WHEELCHAIR ACCESSIBLE ROUTE TO THE RAMP.
 - B: WITHIN 5 FT (1.5m) OF THE CROSSWALK EXTENDED.
 - C: WITHIN 10 FT (3m) OF THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
 - D: PARALLEL TO THE CROSSWALK TO BE USED (SEE MUTCD FIGURE 4E-2).
 - E: NORMAL PEDESTRIAN PUSHBUTTON MOUNTING HEIGHT SHOULD BE 3.5 FT (1.05m) ABOVE ADJACENT SIDEWALK.
- PEDESTRIAN SIGNAL FACES SHALL BE MOUNTED WITH THE BOTTOM OF THE HOUSING NOT LESS THAN 8 FT (2.4m) NOR MORE THAN 10 FT (3.0m) ABOVE THE SIDEWALK LEVEL AND SO THERE IS A PEDESTRIAN INDICATION IN THE LINE OF PEDESTRIANS' VISION WHICH PERTAINS TO THE CROSSWALK BEING USED.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, NOT MOUNTED OVER A ROADWAY, SHALL BE AT LEAST 10 FT (3.0m) BUT NOT MORE THAN 15 FT (4.5m) ABOVE THE SIDEWALK OR, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE HIGHWAY IF NO SIDEWALKS EXIST.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, MOUNTED OVER A ROADWAY, SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001 AND 877006. (16 FT (5m) MIN., 18 FT (5.5m) MAX., FROM HIGHEST POINT OF PAVEMENT)

PEDESTRIAN SIGNAL POST

PEDESTRIAN SIGNAL HEAD AND PEDESTRIAN PUSHBUTTON DETECTOR LOCATION

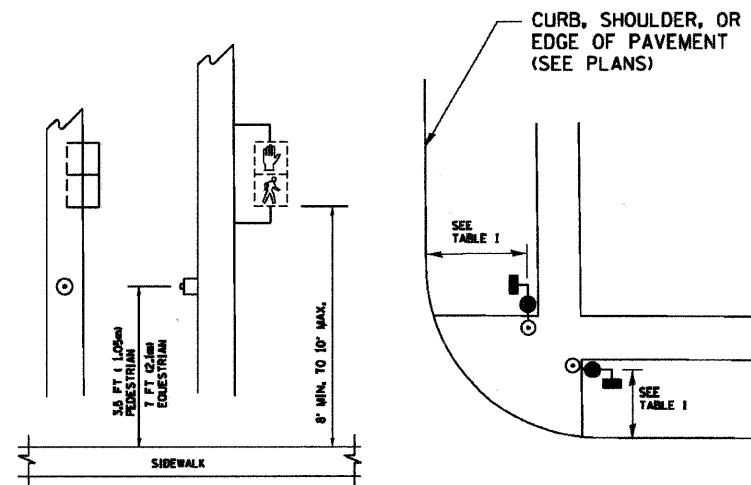


TABLE I

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MIN. DIST. FROM BACK OF CURB)	SHOULDER/NON-CURBED AREA (MIN. DIST. FROM EDGE OF PAVEMENT)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN PUSHBUTTON	SEE NOTE 1	SEE NOTE 1

REVISIONS	
NAME	DATE
BUREAU OF TRAFFIC	1/01/02

ILLINOIS DEPARTMENT OF TRANSPORTATION

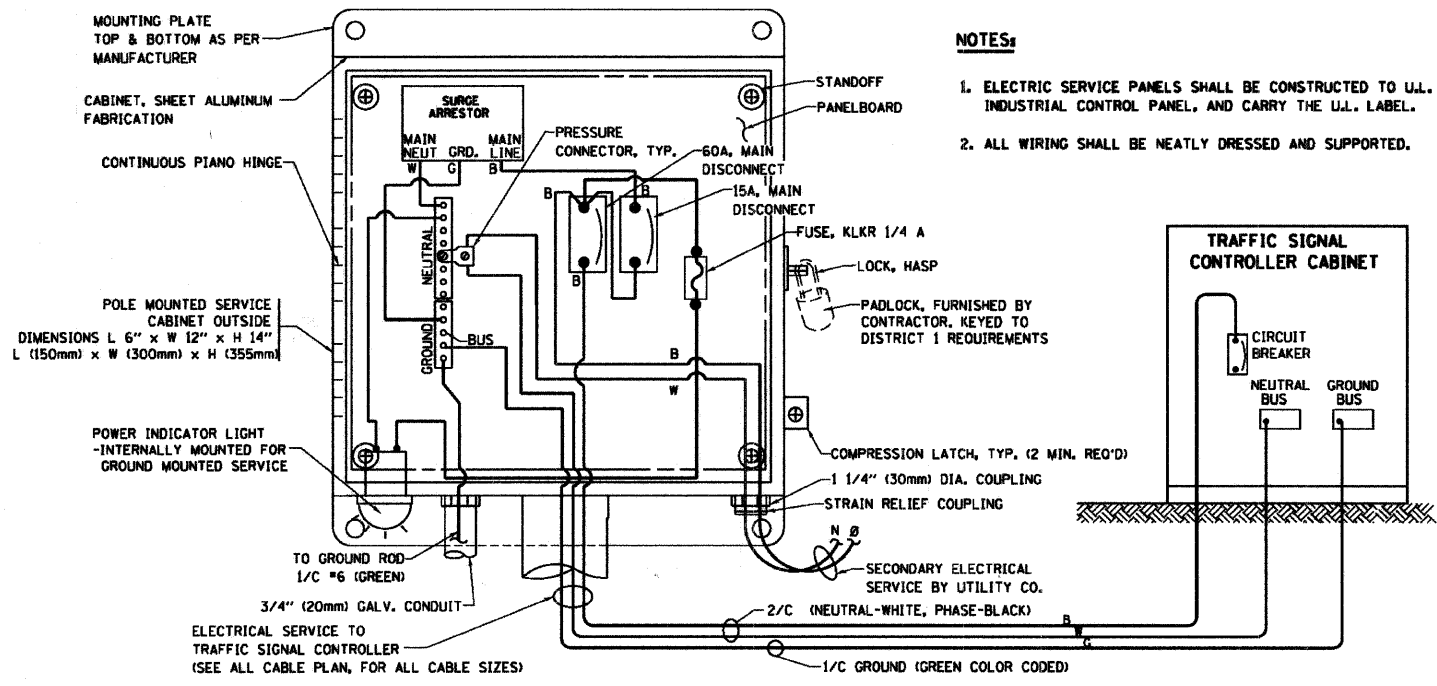
DISTRICT 1
STANDARD TRAFFIC SIGNAL
DESIGN DETAILS

SCALE: NONE

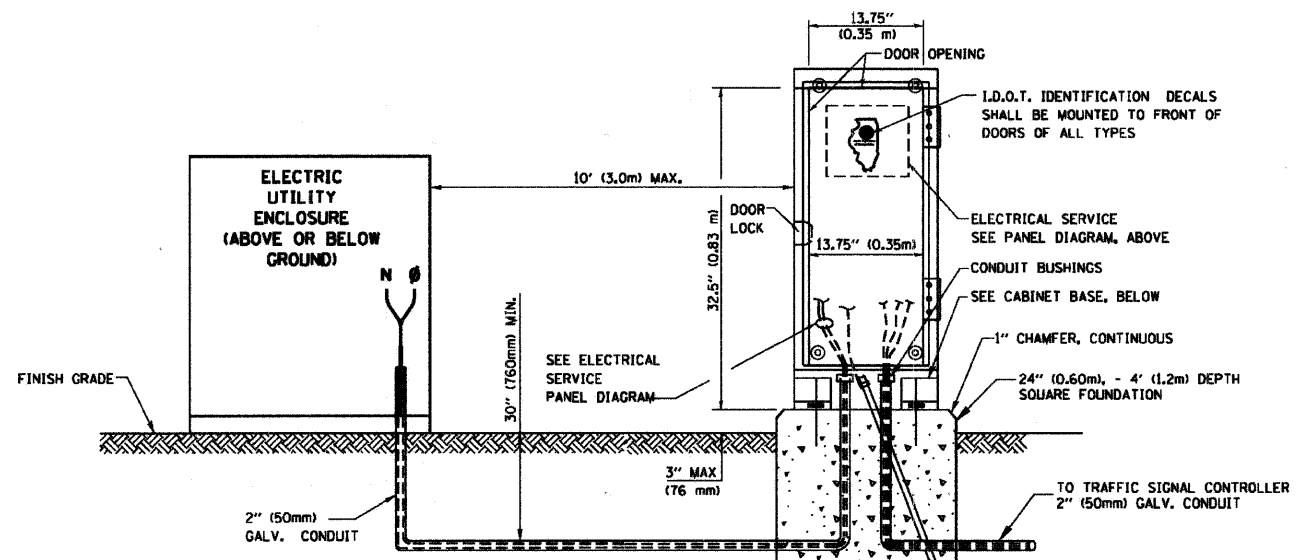
DRAWN BY: RWP
DESIGNED BY: DAD
CHECKED BY: DAZ
SHEET 2 OF 4

F. A. P. RITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

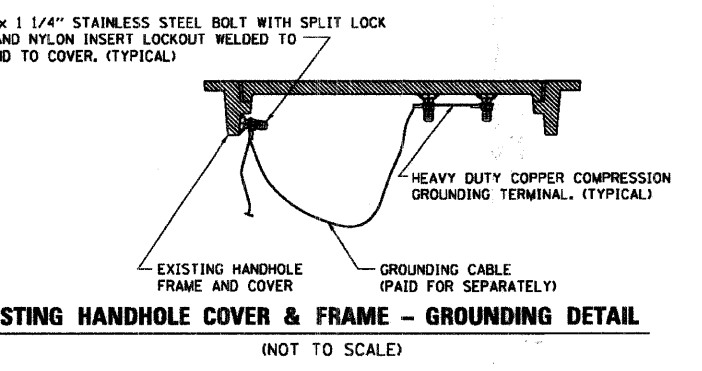
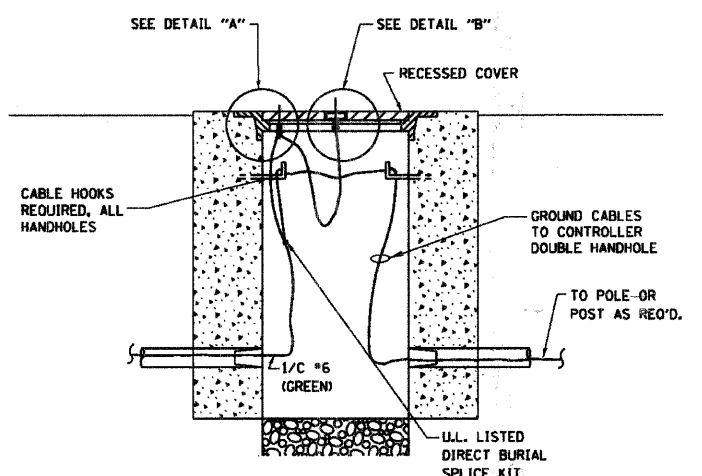
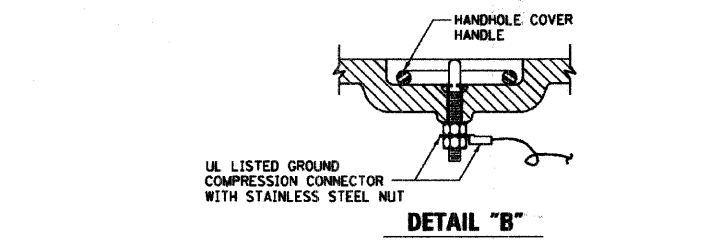
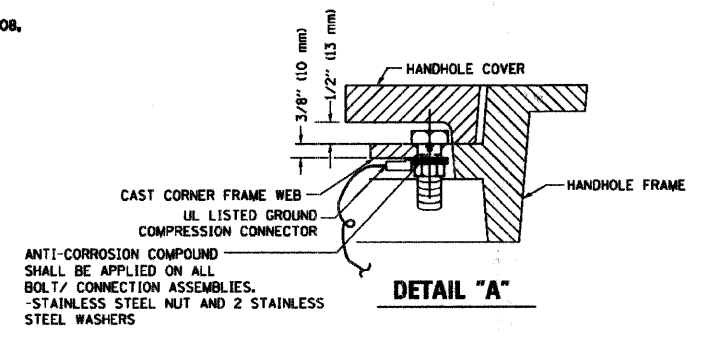
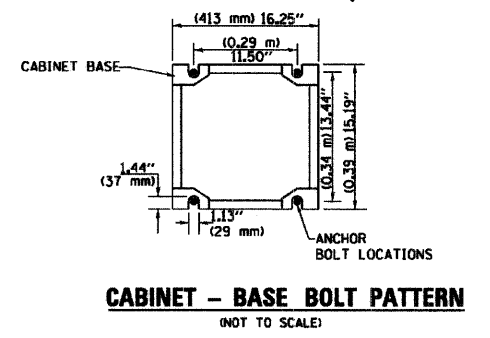
CONTRACT #63033



ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
 (NOT TO SCALE)

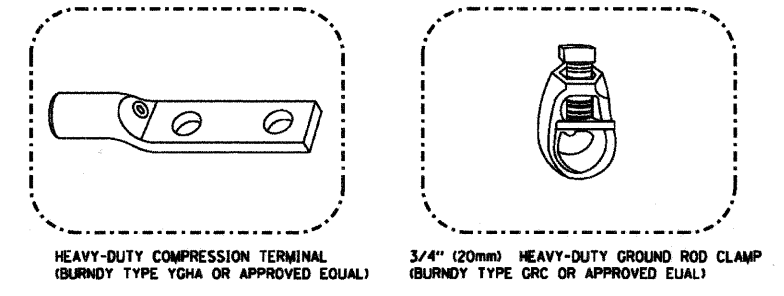


SERVICE INSTALLATION GROUND MOUNT
 (NOT TO SCALE)



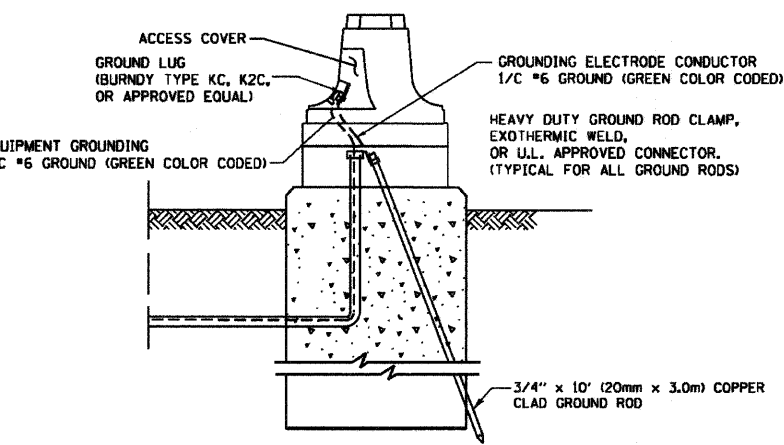
NOTES:

- THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
- THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
- ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
- THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES. 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES. 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



REVISIONS	
NAME	DATE
CADD	5/30/00
CADD	3/15/01
BUREAU OF TRAFFIC	1/01/02

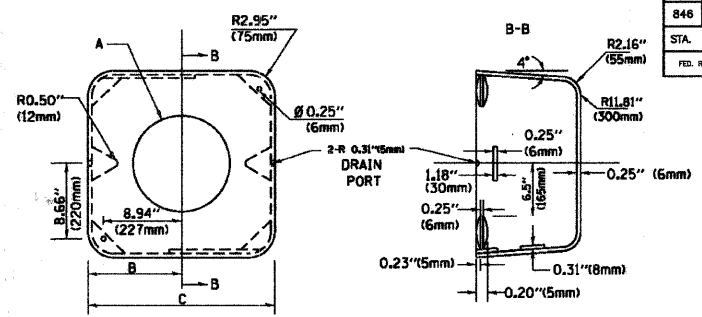
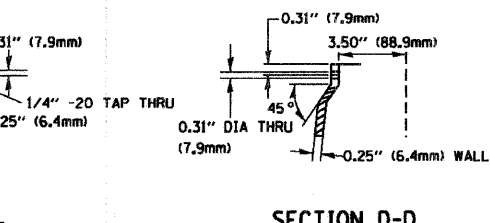
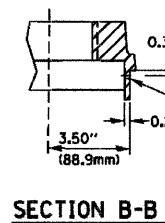
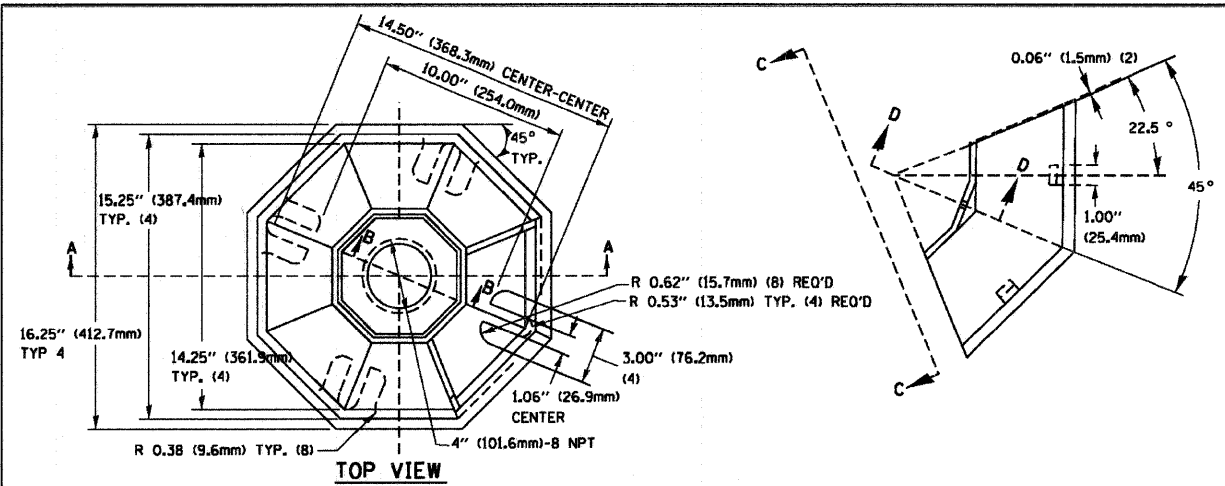
ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT ONE
 STANDARD TRAFFIC SIGNAL
 DESIGN DETAILS

SCALE: NONE
 DRAWN BY: RWP
 DESIGNED BY: DAD
 CHECKED BY: DAZ
 SHEET 3 OF 4
 TS05

F. A. P. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	05-00047-00-CH	WILL	036	018
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

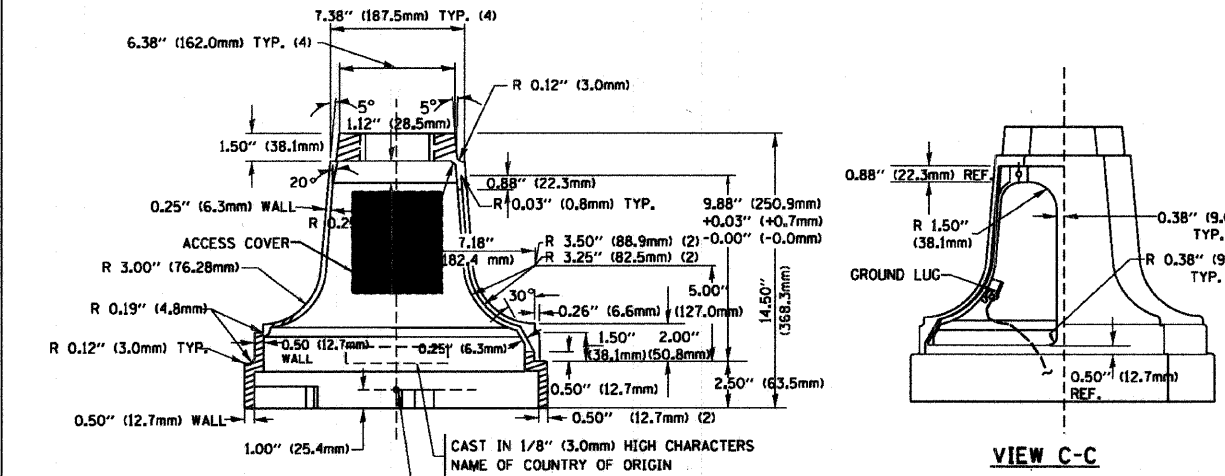
CONTRACT #63033

MATERIAL:
 - ASTM A48 CLASS 30 GREY IRON
 - ASTM A123 HOT DIPPED GALVANIZED

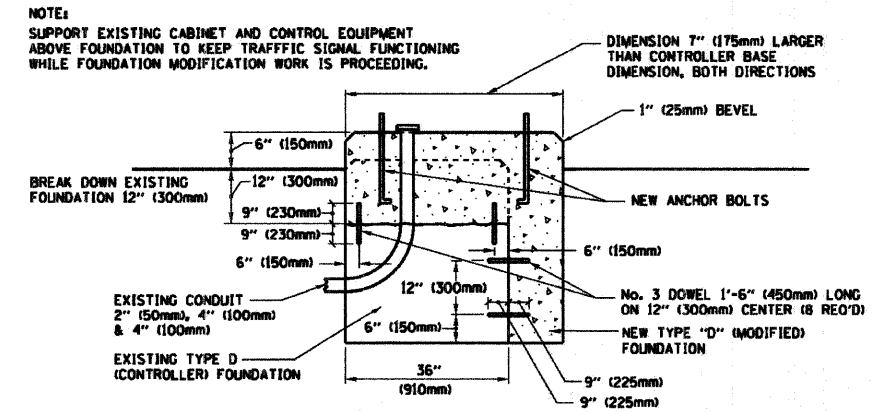


TYPE	A	B	C	HEIGHT	WEIGHT
I	Ø 10.125\"(257mm)	9.5\"(241mm)	19\"(483mm)	12\"(300mm)	24kg
II	Ø 11.125\"(283mm)	10.75\"(273mm)	21.5\"(546mm)	12\"(300mm)	26kg

SHROUD DETAIL



TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A



MODIFY EXISTING TYPE "D" FOUNDATION

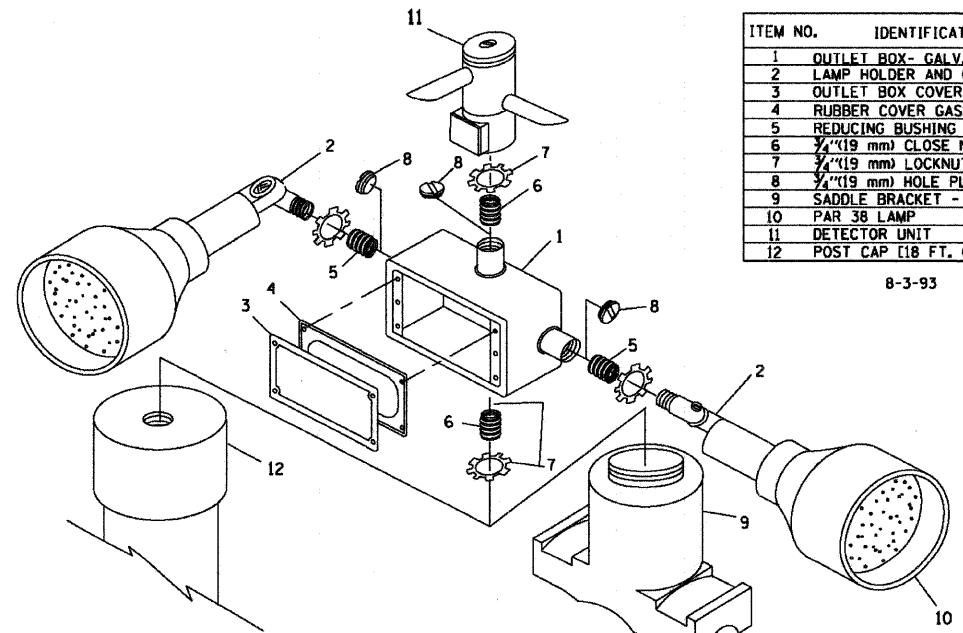
(NOT TO SCALE)

NOTES:

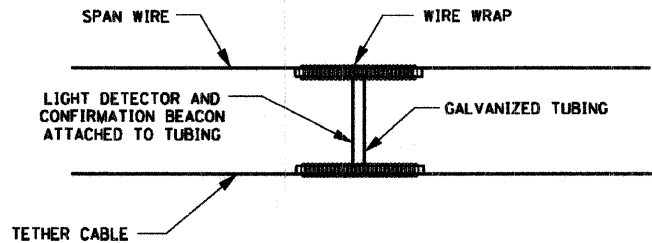
- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
- ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
 ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
 ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
- WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4\"(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.

ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU. IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4\"(19 mm) CLOSE NIPPLE
7	3/4\"(19 mm) LOCKNUT
8	3/4\"(19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	PAR 38 LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

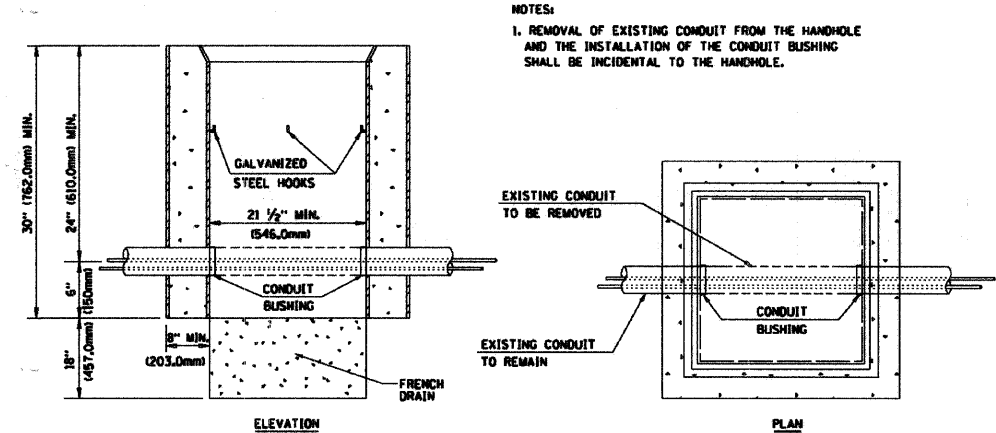
8-3-93



POST CAP MOUNT MAST ARM MOUNT EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL



LIGHT DETECTOR AND CONFIRMATION BEACON MOUNTING FOR TEMPORARY TRAFFIC SIGNALS (NOT TO SCALE)



DETAIL HANDHOLE TO INTERCEPT EXISTING CONDUIT N.T.S.

REVISIONS	
NAME	DATE
BUREAU OF TRAFFIC	5/30/00
BUREAU OF TRAFFIC	3/15/01
BUREAU OF TRAFFIC	11/12/01
BUREAU OF TRAFFIC	1-01-02

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT ONE
 STANDARD TRAFFIC SIGNAL
 DESIGN DETAILS

SCALE: NONE

DRAWN BY: RWP
 DESIGNED BY: DAD
 CHECKED BY: DAZ
 SHEET 4 OF 4

TS05

NOTES FOR TEMPORARY TRAFFIC SIGNALS

1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE STATE APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA TS1 OR TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE 12". HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
4. ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
6. THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL, AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
7. ALL DETECTION ZONES ARE APPROXIMATE. THE EXACT LOCATIONS SHALL BE DETERMINED AND LAID OUT IN THE FIELD BY THE MANUFACTURE'S TECHNICIAN AS APPROVED BY THE ENGINEER.
8. TEMPORARY SERVICE CABLE SHALL RUN FROM TEMPORARY SIGNAL POLE TO TEMPORARY SIGNAL POLE TO TEMPORARY SIGNAL POLE ABOVE THE TRAFFIC SIGNAL SPAN WIRE.

F. A. P. SITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
B46	05-00047-00-CH	WILL	036	019
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

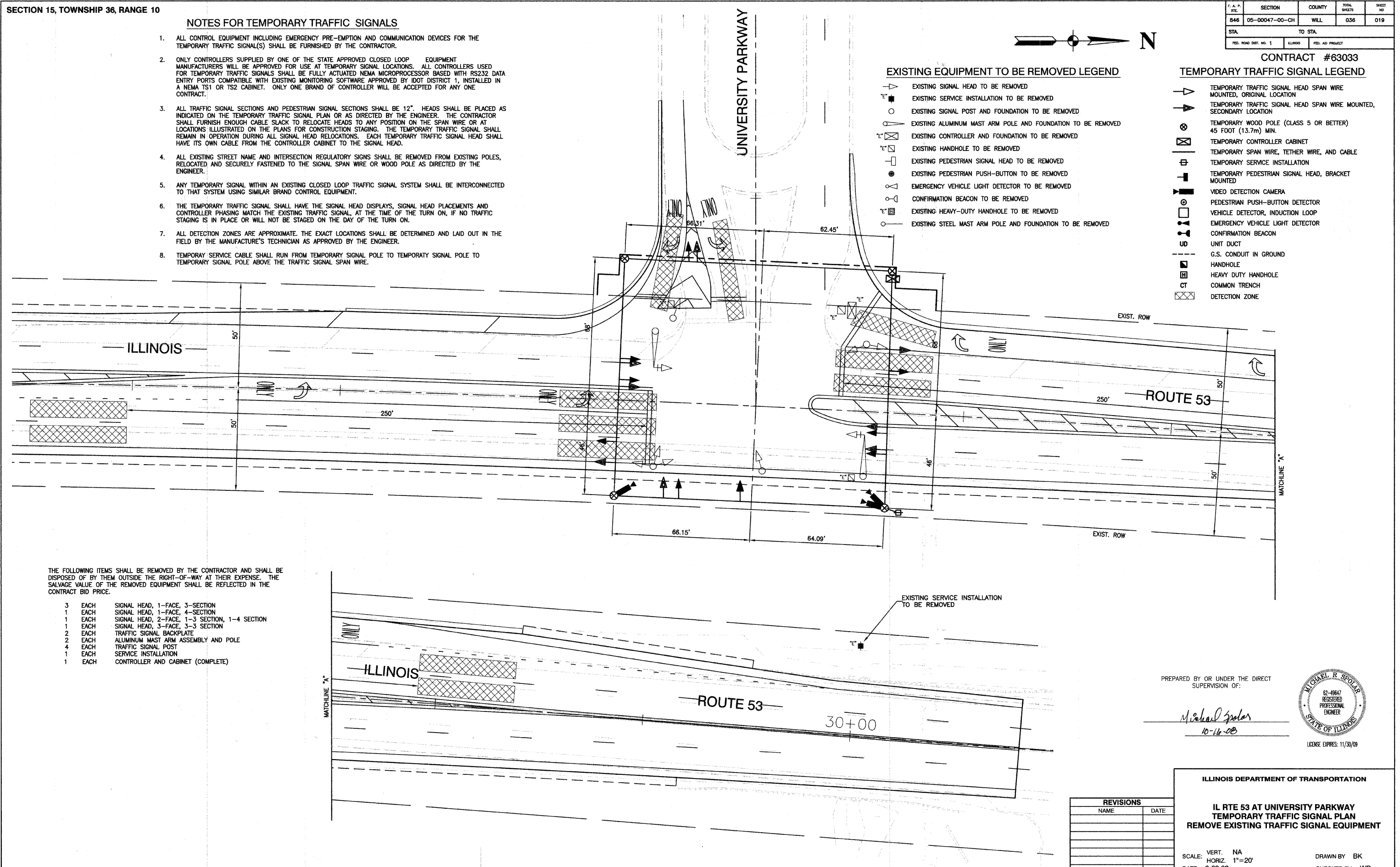
CONTRACT #63033

EXISTING EQUIPMENT TO BE REMOVED LEGEND

- △ EXISTING SIGNAL HEAD TO BE REMOVED
- EXISTING SERVICE INSTALLATION TO BE REMOVED
- EXISTING SIGNAL POST AND FOUNDATION TO BE REMOVED
- ⊖ EXISTING ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED
- ⊗ EXISTING CONTROLLER AND FOUNDATION TO BE REMOVED
- ⊕ EXISTING HANDHOLE TO BE REMOVED
- ⊖ EXISTING PEDESTRIAN SIGNAL HEAD TO BE REMOVED
- EXISTING PEDESTRIAN PUSH-BUTTON TO BE REMOVED
- ⊖ EXISTING EMERGENCY VEHICLE LIGHT DETECTOR TO BE REMOVED
- ⊖ EXISTING CONFIRMATION BEACON TO BE REMOVED
- ⊖ EXISTING HEAVY-DUTY HANDHOLE TO BE REMOVED
- EXISTING STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED

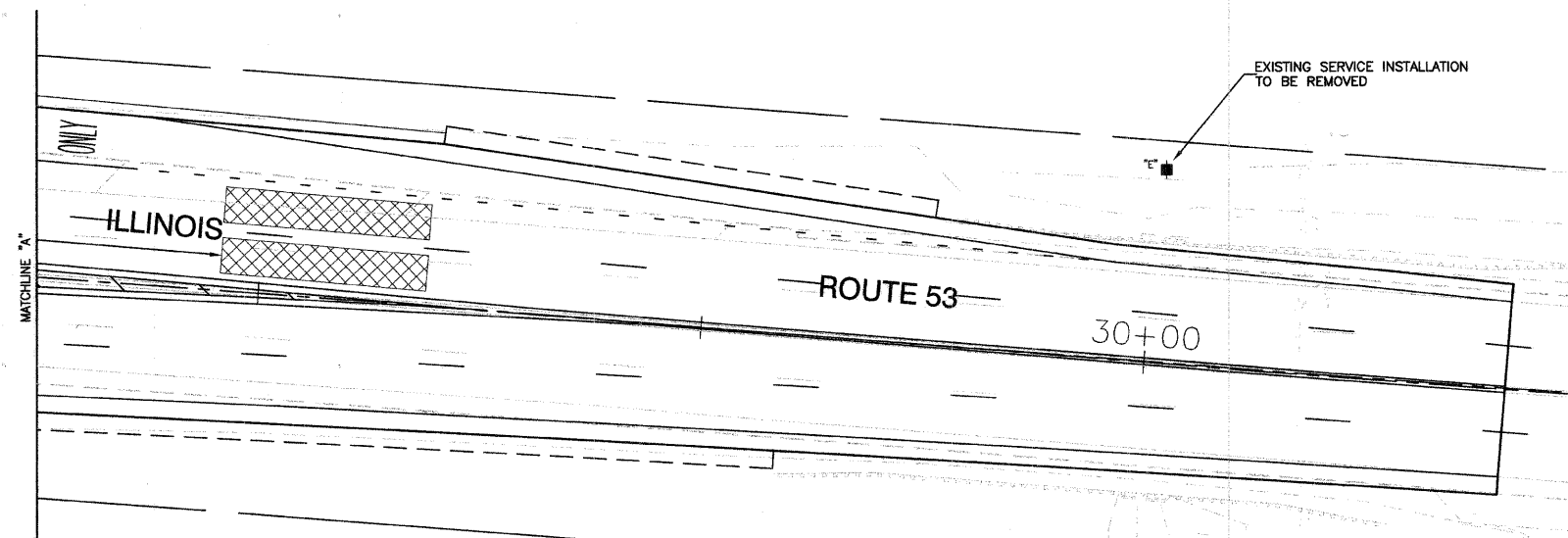
TEMPORARY TRAFFIC SIGNAL LEGEND

- △ TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED, ORIGINAL LOCATION
- △ TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED, SECONDARY LOCATION
- ⊖ TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MIN.
- ⊗ TEMPORARY CONTROLLER CABINET
- ⊖ TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE
- ⊖ TEMPORARY SERVICE INSTALLATION
- ⊖ TEMPORARY PEDESTRIAN SIGNAL HEAD, BRACKET MOUNTED
- ⊖ VIDEO DETECTION CAMERA
- ⊖ PEDESTRIAN PUSH-BUTTON DETECTOR
- ⊖ VEHICLE DETECTOR, INDUCTION LOOP
- ⊖ EMERGENCY VEHICLE LIGHT DETECTOR
- ⊖ CONFIRMATION BEACON
- UD UNIT DUCT
- - - G.S. CONDUIT IN GROUND
- ⊖ HANDHOLE
- ⊖ HEAVY DUTY HANDHOLE
- CT COMMON TRENCH
- ⊖ DETECTION ZONE



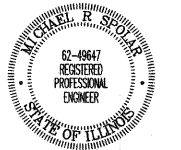
THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 3 EACH SIGNAL HEAD, 1-FACE, 3-SECTION
- 1 EACH SIGNAL HEAD, 1-FACE, 4-SECTION
- 1 EACH SIGNAL HEAD, 2-FACE, 1-3 SECTION, 1-4 SECTION
- 1 EACH SIGNAL HEAD, 3-FACE, 3-3 SECTION
- 2 EACH TRAFFIC SIGNAL BACKPLATE
- 2 EACH ALUMINUM MAST ARM ASSEMBLY AND POLE
- 4 EACH TRAFFIC SIGNAL POST
- 1 EACH SERVICE INSTALLATION
- 1 EACH CONTROLLER AND CABINET (COMPLETE)



PREPARED BY OR UNDER THE DIRECT SUPERVISION OF:

Michael Spolar
10-16-08



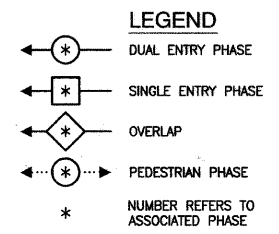
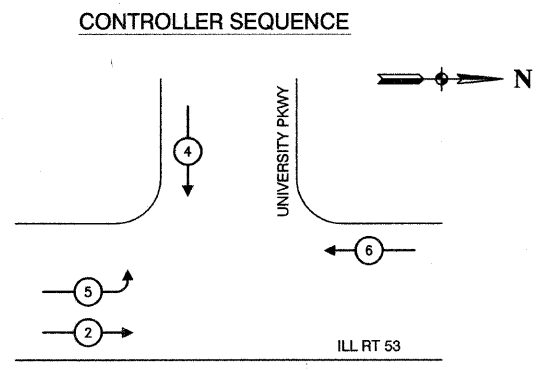
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
IL RTE 53 AT UNIVERSITY PARKWAY
TEMPORARY TRAFFIC SIGNAL PLAN
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT

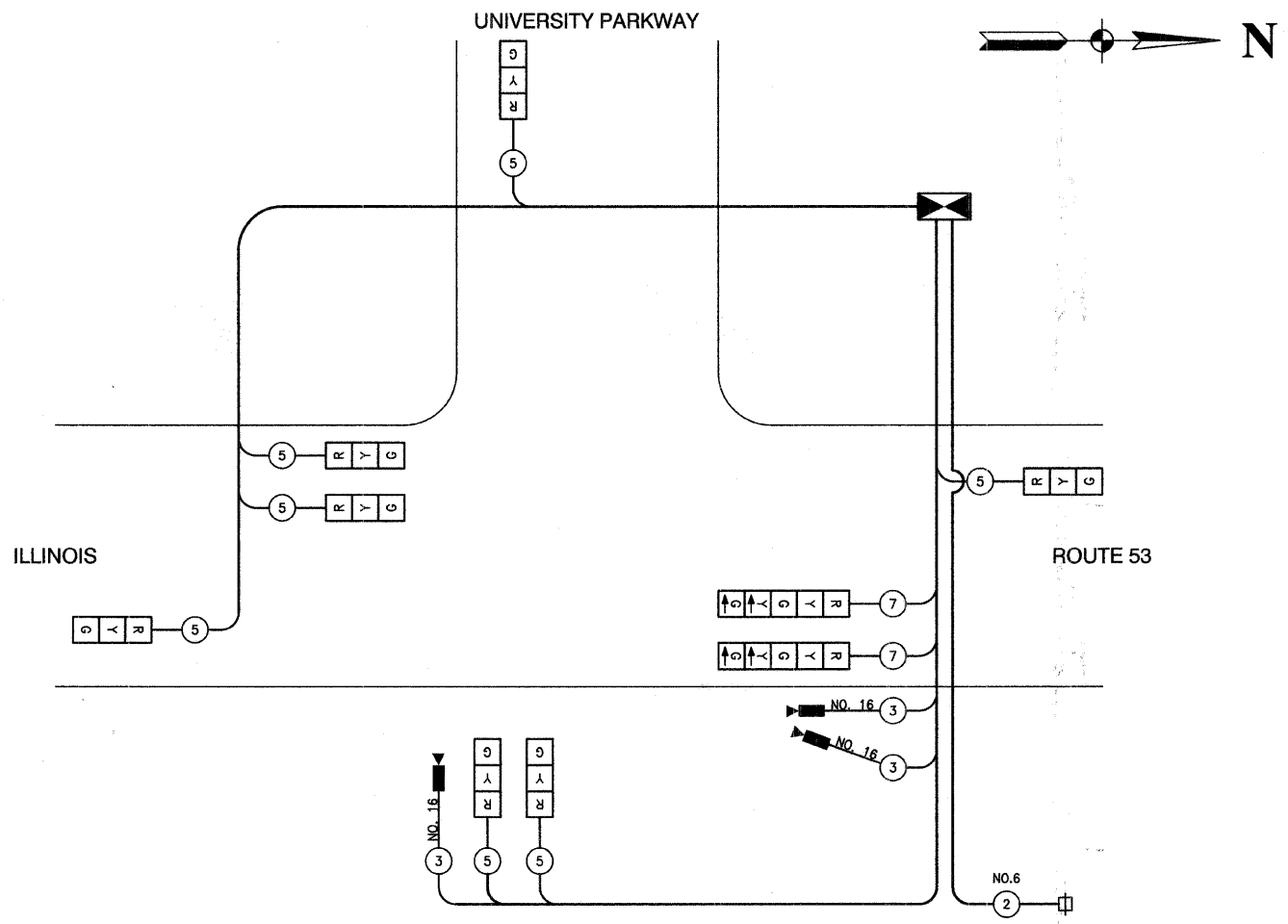
SCALE: VERT. NA
HORIZ. 1"=20'
DATE: 8-29-08
DRAWN BY: BK
CHECKED BY: WD

F. A. P. REC.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	05-00047-00-CH	WILL	036	020
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	

CONTRACT #63033



PHASE DESIGNATION DIAGRAM



CABLE PLAN

TEMPORARY CABLE PLAN LEGEND

- [R] 12" TRAFFIC SIGNAL SECTION
- [X] CONTROLLER CABINET
- [S] SERVICE INSTALLATION
- (2) DENOTES NUMBER OF CONDUCTORS. ALL CABLE NO. 14 EXCEPT AS INDICATED. ALL LOOP DETECTOR CABLE TO BE SHIELDED.
- [EVL] EMERGENCY VEHICLE LIGHT DETECTOR
- [CB] CONFIRMATION BEACON
- [V] VEHICLE DETECTOR, INDUCTION LOOP
- [P] PUSHBUTTON DETECTOR
- [P12] 12" PEDESTRIAN SIGNAL SECTION
- [VDC] VIDEO DETECTION CAMERA

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE		% OPERATION	
		INCAND.	LED		
SIGNAL (RED)	9	135	17	0.50	76.5
(YELLOW)	9	135	25	0.25	56.25
(GREEN)	9	135	15	0.25	33.75
ARROW	4	135	12	0.10	4.8
PED. SIGNAL	-	90	25	1.00	-
CONTROLLER	1	100	100	1.00	100.0
ILLUM. SIGN	-	84	-	0.05	-
FLASHER	-	-	-	0.50	-
ENERGY COSTS TO:				TOTAL =	271.30
LEWIS UNIVERSITY ONE UNIVERSITY PARKWAY ROMEONVILLE, IL 60446					
ENERGY SUPPLY CONTACT: MARK ANDERSON PHONE: (615) 724-5998 COMPANY: COMMONWEALTH EDISON					

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**IL RTE 53 AT UNIVERSITY PARKWAY
TEMPORARY CABLE PLAN
AND PHASE DESIGNATION DIAGRAM**

SCALE: VERT. NA
HORIZ. 1"=20'

DATE: 8-29-08

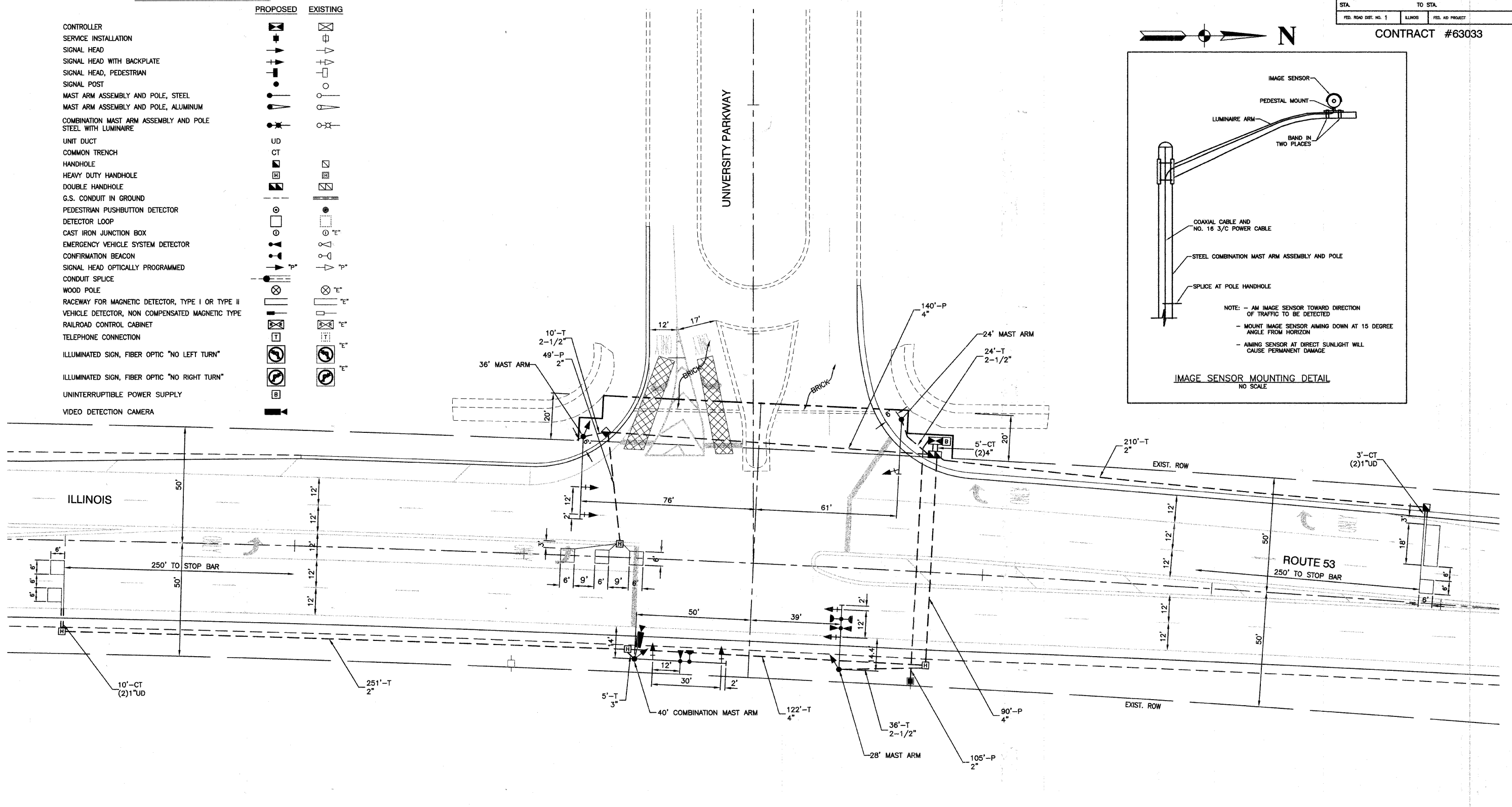
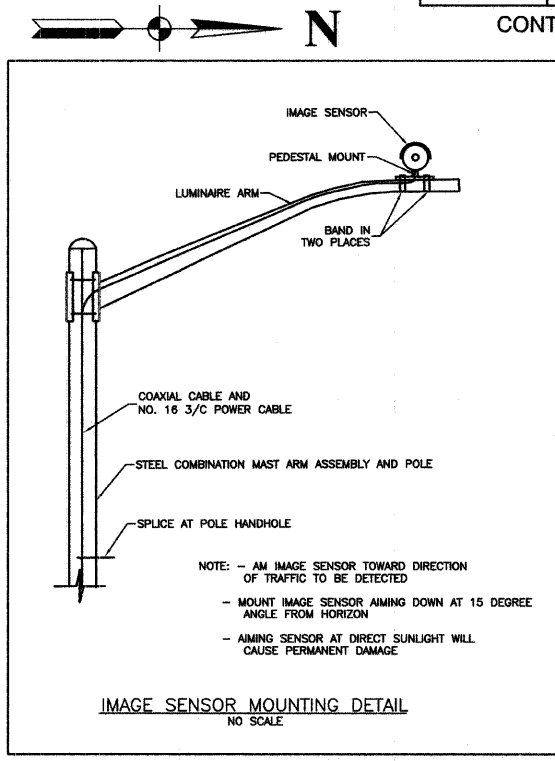
DRAWN BY: BK
CHECKED BY: WD

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
CONTROLLER		
SERVICE INSTALLATION		
SIGNAL HEAD		
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD, PEDESTRIAN		
SIGNAL POST		
MAST ARM ASSEMBLY AND POLE, STEEL		
MAST ARM ASSEMBLY AND POLE, ALUMINUM		
COMBINATION MAST ARM ASSEMBLY AND POLE STEEL WITH LUMINAIRE		
UNIT DUCT	UD	
COMMON TRENCH	CT	
HANDHOLE		
HEAVY DUTY HANDHOLE		
DOUBLE HANDHOLE		
G.S. CONDUIT IN GROUND		
PEDESTRIAN PUSHBUTTON DETECTOR		
DETECTOR LOOP		
CAST IRON JUNCTION BOX		
EMERGENCY VEHICLE SYSTEM DETECTOR		
CONFIRMATION BEACON		
SIGNAL HEAD OPTICALLY PROGRAMMED		
CONDUIT SPLICE		
WOOD POLE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RAILROAD CONTROL CABINET		
TELEPHONE CONNECTION		
ILLUMINATED SIGN, FIBER OPTIC "NO LEFT TURN"		
ILLUMINATED SIGN, FIBER OPTIC "NO RIGHT TURN"		
UNINTERRUPTIBLE POWER SUPPLY		
VIDEO DETECTION CAMERA		

P. A. P. ETC.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	05-00047-00-GH	WILL	036	021
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

CONTRACT #63033



NOTES FOR TRAFFIC SIGNAL

ALL DETECTION ZONES ARE APPROXIMATE. THE EXACT LOCATIONS SHALL BE DETERMINED AND LAID OUT IN THE FIELD BY THE MANUFACTURE'S TECHNICIAN AS APPROVED BY THE ENGINEER.

THE VIDEO DETECTION SYSTEM'S MANUFACTURER'S REPRESENTATIVE SHALL PROVIDE ASSISTANCE IN ALL CABLE TERMINATIONS.

RESTORATION OF WORK AREA

RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEMS SUCH AS FOUNDATION, CONDUIT, HANHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SALT TOLERANT SOD, AND ALL DAMAGE TO UNMOWED FIELD SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

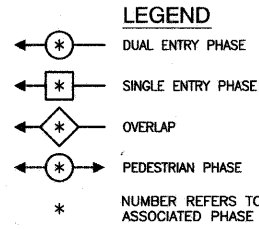
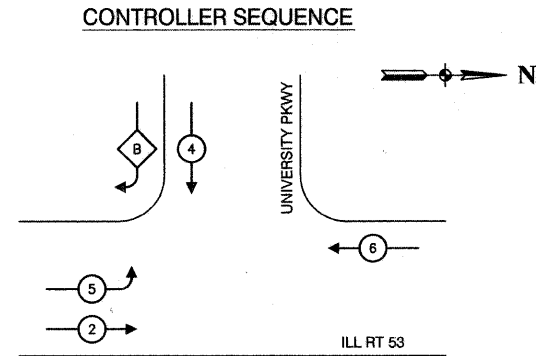
IL RTE 53 AT UNIVERSITY PARKWAY
TRAFFIC SIGNAL INSTALLATION PLAN

SCALE: VERT. NA
HORIZ. 1"=20'
DATE: 8-29-08

DRAWN BY: BK
CHECKED BY: WD

F. A. P. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
B46	05-00047-00-CH	WILL	036	022
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

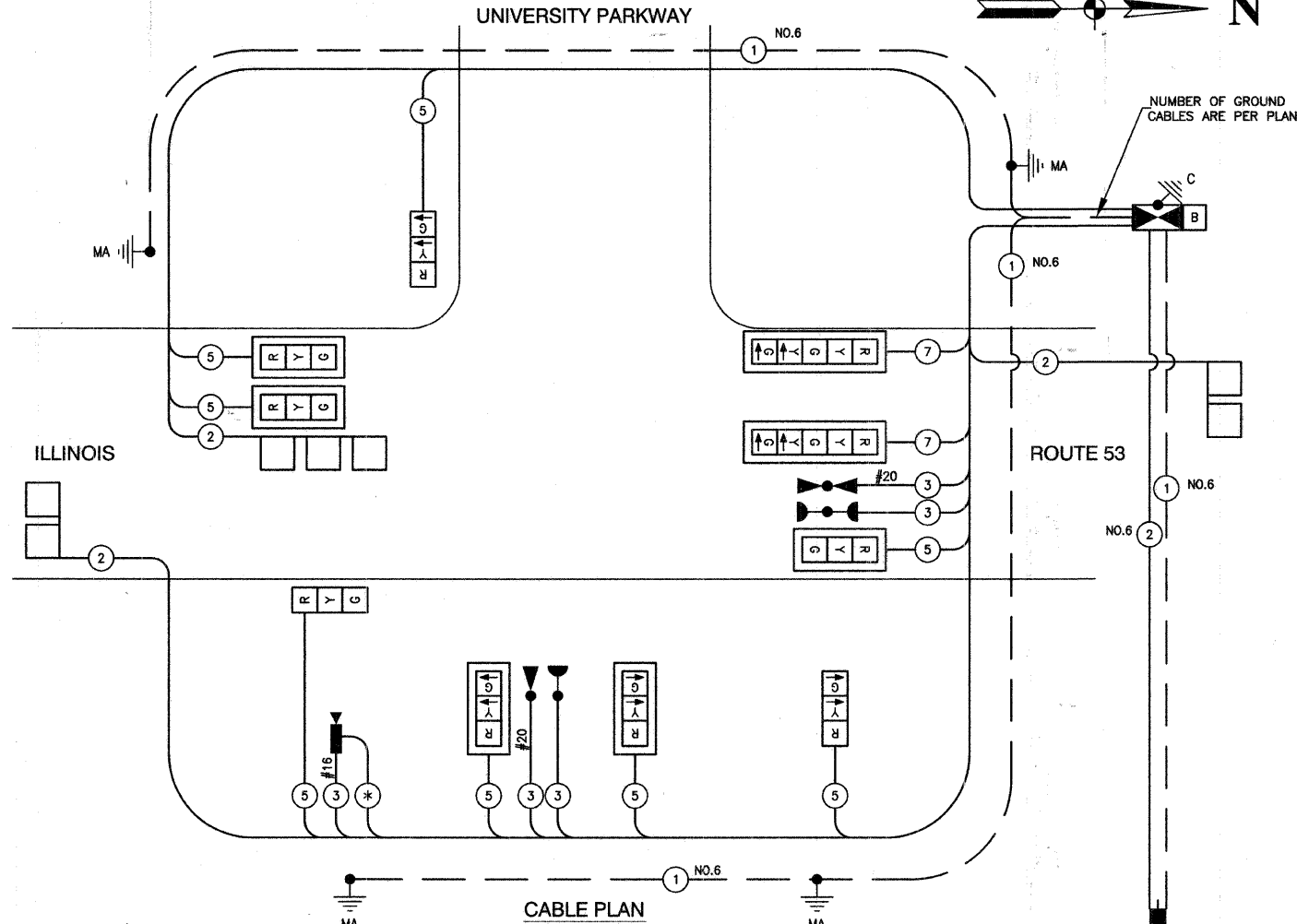
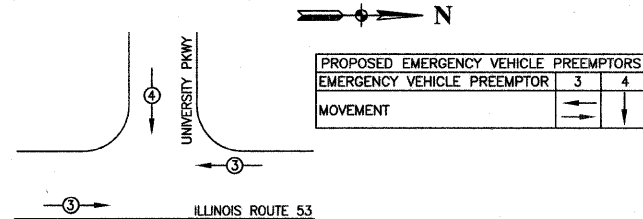
CONTRACT #63033



PHASE DESIGNATION DIAGRAM

OVERLAP LETTER	PERMISSIVE PHASE	PROTECTED PHASE
B	= 4	+ 5

EMERGENCY VEHICLE PREEMPTION SEQUENCE



CABLE PLAN

SCHEDULE OF QUANTITIES

ITEM	UNIT	QUAN
SIGN PANEL, TYPE 1	SQ FT	7.5
SIGN PANEL, TYPE 2	SQ FT	25
CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL	FOOT	461
CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL	FOOT	70
CONDUIT IN TRENCH, 3" DIA., GALVANIZED STEEL	FOOT	5
CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL	FOOT	132
CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL	FOOT	154
CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL	FOOT	230
HANDHOLE	EACH	2
DOUBLE HANDHOLE	EACH	1
HEAVY-DUTY HANDHOLE	EACH	4
TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	668
FULL-ACTUATED CONTROLLER AND TYPE IV CABINET	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	505
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	1886
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	283
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	958
ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C	FOOT	120
STEEL MAST ARM ASSEMBLY AND POLE, 24 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 28 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 36 FT.	EACH	1
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE, 40 FT.	EACH	1
CONCRETE FOUNDATION, TYPE C	FOOT	4
CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	45
CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	15
SIGNAL HEAD, L.E.D., 1-FACE, 3-SECTION, MAST ARM MOUNTED	EACH	5
SIGNAL HEAD, L.E.D., 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	3
SIGNAL HEAD, L.E.D., 1-FACE, 5-SECTION, MAST ARM MOUNTED	EACH	2
TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	7
INDUCTIVE LOOP DETECTOR	EACH	3
DETECTOR LOOP, TYPE I	FOOT	292
LIGHT DETECTOR	EACH	2
LIGHT DETECTOR AMPLIFIER	EACH	1
UNINTERRUPTIBLE POWER SUPPLY	EACH	1
ELECTRIC CABLE IN CONDUIT, GROUND, NO. 6, 1C (GREEN)	FOOT	619
SERVICE INSTALLATION - POLE MOUNTED	EACH	1
ELECTRIC CABLE IN CONDUIT, NO. 20, 3C, TWISTED, SHIELDED	FOOT	505
TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1
TEMPORARY TRAFFIC SIGNAL TIMINGS	EACH	1
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REMOVE EXISTING HANDHOLE	EACH	3
REMOVE EXISTING CONCRETE FOUNDATION	EACH	7
VIDEO VEHICLE DETECTION, 1 CAMERA	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 16 3/C	FOOT	307
ELECTRIC CABLE IN CONDUIT, COAXIAL CABLE	FOOT	307

CABLE PLAN LEGEND

EXISTING	PROPOSED	DESCRIPTION
(C)	(G)	8" TRAFFIC SIGNAL SECTION
(R)	(R)	12" TRAFFIC SIGNAL SECTION
(W)	(W)	12" PEDESTRIAN SIGNAL SECTION
(P)	(P)	12" PEDESTRIAN SIGNAL SECTION
(CAB)	(CAB)	CONTROLLER CABINET
(S)	(S)	SERVICE INSTALLATION
(T)	(T)	TELEPHONE CONNECTION
(M)	(M)	MAGNETIC DETECTOR
(E)	(E)	EMERGENCY VEHICLE LIGHT DETECTOR
(C)	(C)	CONFIRMATION BEACON
(P)	(P)	PUSHBUTTON DETECTOR
(V)	(V)	VEHICLE DETECTOR, INDUCTION LOOP
(2)	(2)	DENOTES NUMBER OF CONDUCTORS. ALL CABLE NO. 14 EXCEPT AS INDICATED. ALL LOOP DETECTOR CABLE TO BE SHIELDED.
(SIGNAL)	(SIGNAL)	SIGNAL FACE WITH BACKPLATE "P" INDICATES PROGRAMMED HEAD
(R)	(R)	RAILROAD CONTROL CABINET
(E)	(E)	ILLUMINATED SIGN, FIBER OPTIC "NO LEFT TURN"
(E)	(E)	ILLUMINATED SIGN, FIBER OPTIC "NO RIGHT TURN"
(H/C)	(H/C)	GROUND ROD AT HANDHOLE, DOUBLE HANDHOLE, OR CONTROLLER
(P/MA)	(P/MA)	GROUND ROD AT POST OR MAST ARM POLE
(S)	(S)	GROUND ROD AT ELECTRIC SERVICE INSTALLATION
(1)	(1)	GROUND CABLE IN CONDUIT NO. 6 (GREEN)
(B)	(B)	INTERRUPTIBLE POWER SUPPLY
(*)	(*)	COAXIAL CABLE

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE INCAND.	LED	% OPERATION	
SIGNAL (RED)	10	135	17	0.50	85.00
(YELLOW)	10	135	25	0.25	62.50
(GREEN)	10	135	15	0.25	37.50
ARROW	4	135	12	0.10	4.80
PED. SIGNAL	-	90	25	1.00	-
CONTROLLER	1	100	100	1.00	100.00
ILLUM. SIGN	-	84	-	0.05	-
FLASHER	-	-	-	0.50	-
TOTAL =					289.80

FOUNDATION (DEPTH)	FT. (m)	CABLE SLACK	FT. (m)	VERTICAL	FT. (m)
TYPE A - POST	4 (1.2)	HANDHOLE	6.5 (2.0)	ALL FOUNDATIONS	3.5 (1.0)
D - CONTROLLER	4 (1.2)	DOUBLE HANDHOLE	13 (4.0)	MAST ARM (L) POLE	20'+L-2=
E - M. ARM POLE	-	SIGNAL POST	2 (1.0)	(8m+L-0.8m)=	-
30" (750mm)	15 (4.6)	CONTROLLER CAB.	1 (0.5)	BRACKET MOUNTED	13 (4.0)
30" (750mm)	15 (4.6)	FIBER OPTIC	13 (4.0)	PED. PUSHBUTTON	4 (1.2)
36" (900mm)	15 (4.6)	ELECTRIC SERVICE	1 (0.5)	ELECTRIC SERVICE	13.5 (4.1)
		GROUND CABLE	1 (0.5)	SERVICE TO GROUND	13.5 (4.1)
				POST MOUNTED	6 (1.8)

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ILLINOIS DEPARTMENT OF TRANSPORTATION

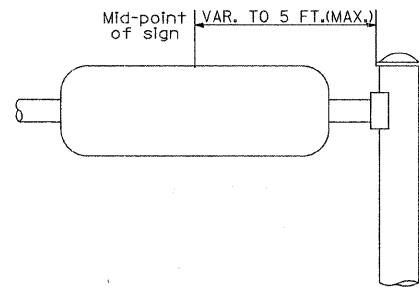
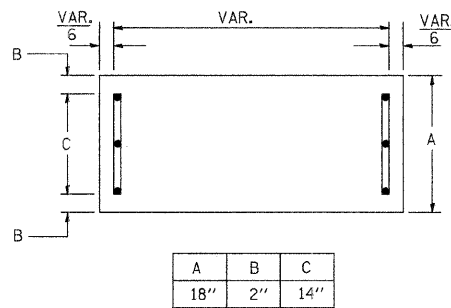
IL RTE 53 AT UNIVERSITY PARKWAY CABLE PLAN AND PHASE DESIGNATION DIAGRAM

SCALE: VERT. NA
HORIZ. 1"=20'

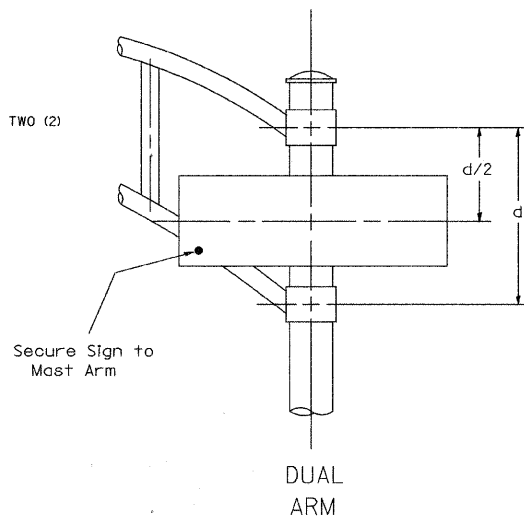
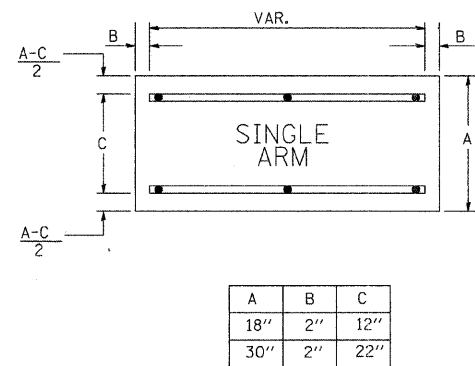
DATE 8-29-08

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CHECKED BY WD

SUPPORTING CHANNELS



SUPPORTING CHANNELS



SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM
Shall be used. See Note #5.

Upper Case to Lower Case
Spacing Chart 8-6 Inch Series "C & D"

FIRST LETTER	SECOND LETTER															
	a c d e		b h i k l				f w		j		s t		v y		x z	
	g o q	m n p r u														
SERIES	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
A W X	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ²	1 ⁴	0 ⁶	1 ⁰	1 ¹	1 ⁴	0 ⁶	1 ⁰	1 ¹	1 ²	1 ²	1 ⁴
B	1 ⁴	1 ⁵	2 ⁰	2 ¹	1 ⁴	1 ⁵	1 ¹	1 ²	1 ⁴	1 ⁵	1 ²	1 ⁴	1 ²	1 ⁴	1 ⁶	1 ⁷
C E G	1 ⁴	1 ⁵	2 ⁰	2 ¹	1 ²	1 ⁴	0 ⁶	1 ⁰	1 ²	1 ⁴	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ⁴	1 ⁵
D O Q R	1 ⁴	1 ⁵	2 ⁰	2 ¹	1 ⁴	1 ⁵	0 ⁶	1 ⁰	1 ²	1 ⁴	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ⁴	1 ⁵
F	0 ⁵	0 ⁶	1 ⁴	1 ⁵	0 ⁶	1 ⁰	0 ⁵	0 ⁶	0 ⁶	1 ⁰	0 ⁶	1 ⁰	0 ⁶	1 ⁰	1 ¹	1 ²
H I M N	2 ⁰	2 ¹	2 ²	2 ⁴	2 ⁰	2 ¹	1 ⁴	1 ⁵	1 ⁶	1 ⁷	1 ⁶	1 ⁷	2 ⁰	2 ¹	2 ⁰	2 ¹
J U	2 ⁰	2 ¹	2 ⁰	2 ¹	1 ⁶	1 ⁷	1 ⁴	1 ⁵	1 ⁶	1 ⁷	1 ⁶	1 ⁷	1 ⁶	1 ⁷	2 ⁰	2 ¹
K L	1 ¹	1 ²	1 ⁶	1 ⁷	1 ¹	1 ²	0 ⁵	0 ⁶	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ²	1 ⁴
P	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ²	1 ⁴	0 ⁵	0 ⁶	1 ¹	1 ²	1 ¹	1 ²	1 ²	1 ⁴	1 ²	1 ⁴
S	1 ²	1 ⁴	1 ⁶	1 ⁷	1 ²	1 ⁴	0 ⁶	1 ⁰	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴
T	1 ¹	1 ²	1 ⁶	1 ⁷	0 ⁶	1 ⁰	0 ⁶	1 ⁰	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ²	1 ⁴
V	0 ⁶	1 ⁰	1 ⁴	1 ⁵	1 ¹	1 ²	0 ⁶	1 ⁰	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴
Y	0 ⁵	0 ⁶	1 ⁴	1 ⁵	0 ⁶	1 ⁰	0 ⁵	0 ⁶	0 ⁵	0 ⁷	0 ⁵	0 ⁶	0 ⁶	1 ⁰	1 ¹	1 ²
Z	1 ⁶	1 ⁷	2 ²	2 ⁴	1 ⁶	1 ⁷	1 ²	1 ⁴	1 ⁶	1 ⁷	1 ⁶	1 ⁷	1 ⁶	1 ⁷	2 ⁰	2 ¹

Lower Case to Lower Case
Spacing Chart 6 Inch Series "C & D"

FIRST LETTER	SECOND LETTER															
	a c d e		b h i k l				f w		j		s t		v y		x z	
	g o q	m n p r u														
SERIES	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
ad h g i j	1 ⁶	1 ⁷	2 ²	2 ⁴	1 ⁶	1 ⁷	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ⁶	1 ⁷	1 ⁶	1 ⁷
l m n q u																
b f k o p s	1 ²	1 ⁴	1 ⁶	1 ⁷	1 ¹	1 ²	0 ⁵	0 ⁶	1 ¹	1 ²	1 ¹	1 ²	1 ²	1 ⁴	1 ²	1 ⁴
c e	1 ²	1 ⁴	1 ⁶	1 ⁷	1 ²	1 ⁴	0 ⁶	1 ⁰	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴
r	0 ⁶	1 ⁰	1 ²	1 ⁴	0 ⁶	1 ⁰	0 ³	0 ³	0 ⁵	0 ⁶	0 ⁵	0 ⁶	0 ⁶	1 ⁰	0 ⁶	1 ⁰
t z	1 ²	1 ⁴	1 ⁶	1 ⁷	1 ²	1 ⁴	0 ⁶	1 ⁰	1 ¹	1 ²	1 ¹	1 ²	1 ²	1 ⁴	1 ²	1 ⁴
v y	1 ¹	1 ²	1 ⁴	1 ⁵	1 ¹	1 ²	0 ⁵	0 ⁶	0 ⁶	1 ⁰	0 ⁶	1 ⁰	1 ¹	1 ²	1 ¹	1 ²
w	1 ¹	1 ²	1 ⁴	1 ⁵	1 ¹	1 ²	0 ⁵	0 ⁶	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ²	1 ⁴
x	1 ²	1 ⁴	1 ⁶	1 ⁷	1 ¹	1 ²	0 ⁵	0 ⁶	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ²	1 ⁴

Number to Number
Spacing Chart 8 Inch Series "C & D"

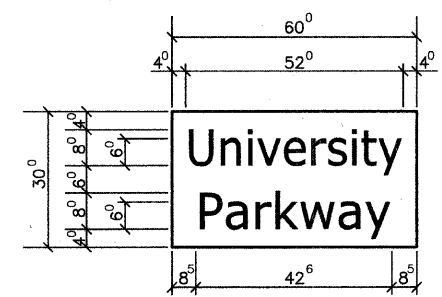
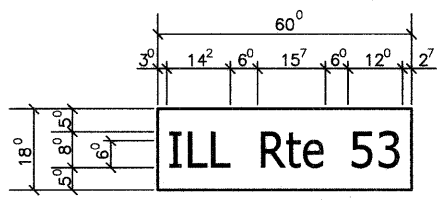
FIRST NUMBER	SECOND NUMBER																			
	0		1		2		3		4		5		6		7		8		9	
	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
0 9	1 ⁶	1 ⁷	1 ⁶	1 ⁷	1 ⁴	1 ⁵	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ⁶	1 ⁷	1 ²	1 ⁴	1 ⁶	1 ⁷	1 ⁶	1 ⁷
1	2 ⁰	2 ¹	2 ⁰	2 ¹	2 ⁰	2 ¹	1 ⁶	1 ⁷	1 ⁴	1 ⁵	2 ⁰	2 ¹	2 ⁰	2 ¹	1 ⁴	1 ⁵	2 ⁰	2 ¹	2 ⁰	2 ¹
2 3 4	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ²	1 ⁴	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ¹	1 ²	1 ⁶	1 ⁷	1 ⁴	1 ⁵
5	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ¹	1 ²	1 ¹	1 ²	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ¹	1 ²	1 ⁴	1 ⁵	1 ⁴	1 ⁵
6	1 ⁶	1 ⁷	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ¹	1 ²	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ⁴	1 ⁵
7	1 ²	1 ⁴	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ²	1 ⁵	0 ⁵	0 ⁶	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ¹	1 ²	1 ⁴	1 ⁵	1 ²	1 ⁴
8	1 ⁶	1 ⁷	1 ⁶	1 ⁷	1 ⁴	1 ⁵	1 ²	1 ⁵	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ⁶	1 ⁷	1 ²	1 ⁴	1 ⁶	1 ⁷	1 ⁴	1 ⁵

EXAMPLE, 2³ DENOTES 3" / 8"

UPPER AND LOWER CASE LETTER WIDTHS
CONTRACT #63033

LETTERS	6 INCH UPPER CASE LETTERS				8 INCH UPPER CASE LETTERS				LETTERS	6 INCH LOWER CASE LETTERS	
	SERIES		SERIES		SERIES		SERIES			SERIES	
	C	D	C	D	C	D	C	D		C	D
A	3 ⁶	5 ⁰	5 ⁰	6 ⁵	a	3 ⁵	4 ²				
B	3 ²	4 ⁰	4 ³	5 ³	b	3 ⁵	4 ²				
C	3 ²	4 ⁰	4 ³	5 ³	c	3 ⁵	4 ¹				
D	3 ²	4 ⁰	4 ³	5 ³	d	3 ⁵	4 ²				
E	3 ⁰	3 ⁵	4 ⁰	4 ⁷	e	3 ⁵	4 ²				
F	3 ⁰	3 ⁵	4 ⁰	4 ⁷	f	2 ³	2 ⁶				
G	3 ²	4 ⁰	4 ³	5 ³	g	3 ⁵	4 ²				
H	3 ²	4 ⁰	4 ³	5 ³	h	3 ⁵	4 ²				
I	0 ⁷	0 ⁷	1 ¹	1 ²	i	1 ¹	1 ¹				
J	3 ⁰	3 ⁶	4 ⁰	5 ⁰	j	2 ⁰	2 ²				
K	3 ²	4 ¹	4 ³	5 ⁴	k	3 ⁵	4 ²				
L	3 ⁰	3 ⁵	4 ⁰	4 ⁷	l	1 ¹	1 ¹				
M	3 ⁷	4 ⁵	5 ¹	6 ¹	m	6 ⁰	7 ⁰				
N	3 ²	4 ⁰	4 ³	5 ³	n	3 ⁵	4 ²				
O	3 ⁴	4 ²	4 ⁵	5 ⁵	o	3 ⁶	4 ³				
P	3 ²	4 ⁰	4 ³	5 ³	p	3 ⁵	4 ²				
Q	3 ⁴	4 ²	4 ⁵	5 ⁵	q	3 ⁵	4 ²				
R	3 ²	4 ⁰	4 ³	5 ³	r	2 ⁶	3 ²				
S	3 ²	4 ⁰	4 ³	5 ³	s	3 ⁶	4 ²				
T	3 ⁰	3 ⁵	4 ⁰	4 ⁷	t	2 ⁷	3 ²				
U	3 ²	4 ⁰	4 ³	5 ³	u	3 ⁵	4 ²				
V	3 ⁵	4 ⁴	4 ⁷	6 ⁰	v	4 ²	4 ⁷				
W	4 ⁴	5 ²	6 ⁰	7 ⁰	w	5 ⁵	6 ⁴				
X	3 ⁴	4 ⁰	4 ⁵	5 ³	x	4 ⁴	5 ¹				
Y	3 ⁶	5 ⁰	5 ⁰	6 ⁶	y	4 ⁶	5 ³				
Z	3 ²	4 ⁰	4 ³	5 ³	z	3 ⁶	4 ³				

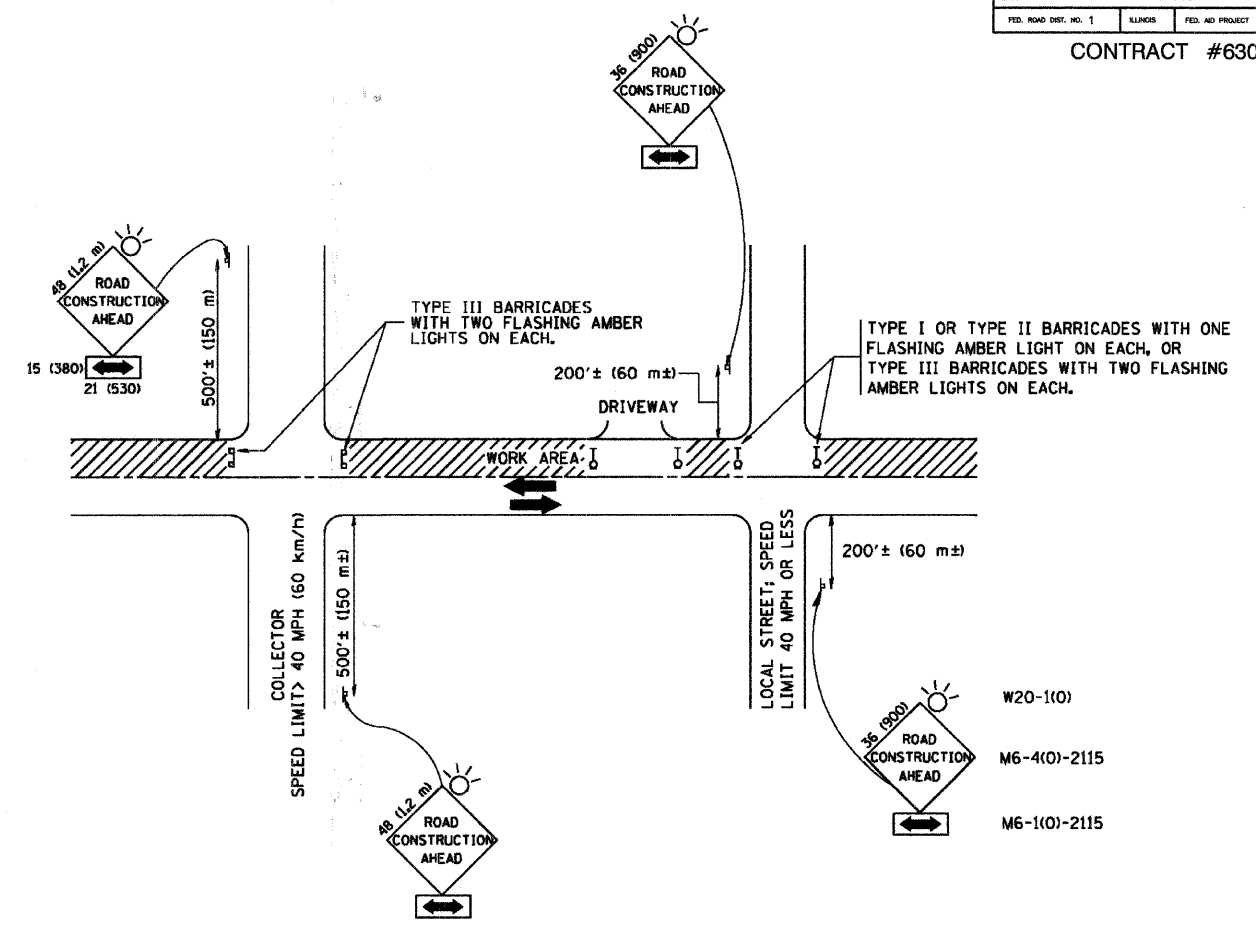
NUMBER	6 INCH SERIES		8 INCH SERIES	
	C	D	C	D
1	1 ²	1 ⁴	1 ⁵	2 ⁰
2	3 ²	4 ⁰	4 ³	5 ³
3	3 ²	4 ⁰	4 ³	5 ³
4	3 ⁵	4 ³	4 ⁷	5 ⁷
5	3 ²	4 ⁰	4 ³	5 ³
6	3 ²	4 ⁰	4 ³	5 ³
7	3 ²	4 ⁰	4 ³	5 ³
8	3 ²	4 ⁰	4 ³	5 ³
9	3 ²	4 ⁰	4 ³	5 ³
0	3 ⁴	4 ²	4 ⁵	5 ⁵



GENERAL NOTES

- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 834001, 834006 AND 834011, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 6'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
- ALL SIGNS SHALL HAVE A WHITE REFLECTORIZED LEGEND AND BORDER ON A GREEN REFLECTORIZED BACKGROUND, TYPE A SHEETING.
- THE SIGN LENGTH SHOULD BE INCREASED IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHOULD NOT EXCEED 6'-0".
- ALL BORDERS SHALL BE 3/4" WIDE AND CORNER RADIUS SHALL BE 2-1/4".
- SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS. LOCAL SUPPL

CONTRACT #63033



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS**
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:**
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.**
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.**

All dimensions are in millimeters (inches) unless otherwise shown.

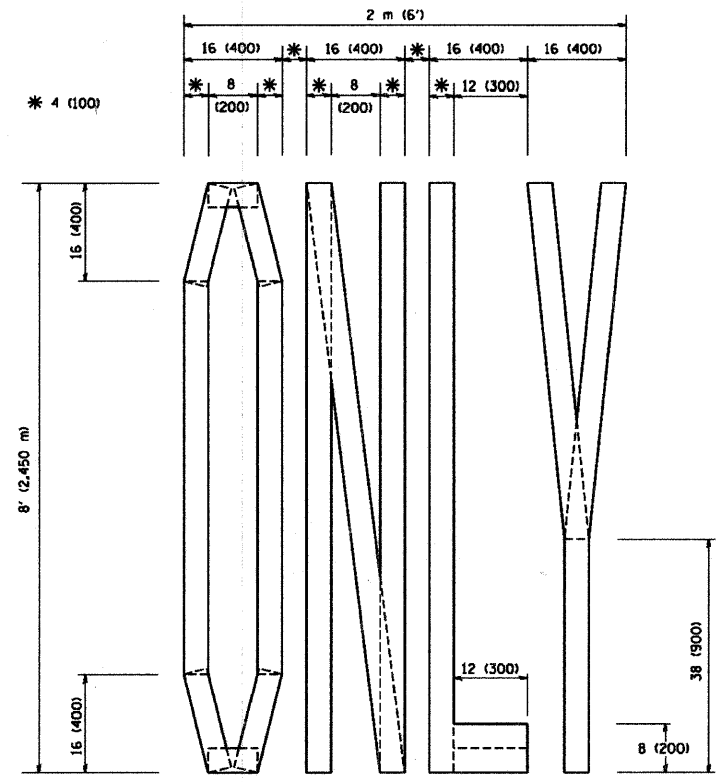
REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TRAFFIC CONTROL AND PROTECTION
 FOR
 SIDE ROADS, INTERSECTIONS, AND
 DRIVEWAYS

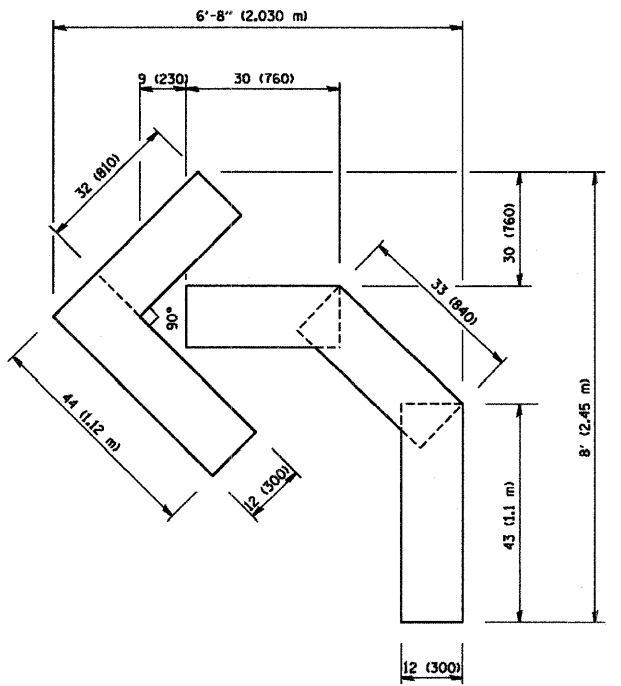
SCALE: NONE
 DRAWN BY
 CHECKED BY
 TC-10

F. A. R. BITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
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STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	

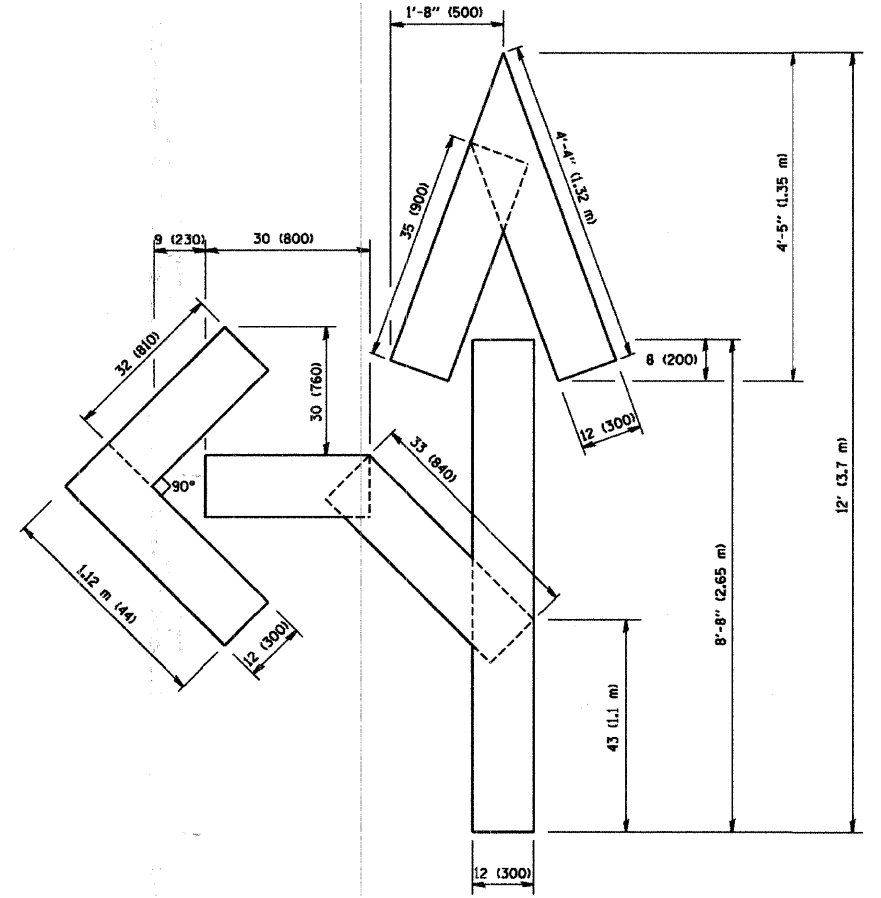
CONTRACT #63033



QUANTITY
4 (100) LINE = 64.1 ft. (19.7 m)
21.1 sq. ft. (1.97 sq. m)



QUANTITY
4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.39 sq. m)



QUANTITY
4 (100) LINE = 82.5 ft. (25.3 m)
27.5 sq. ft. (2.53 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

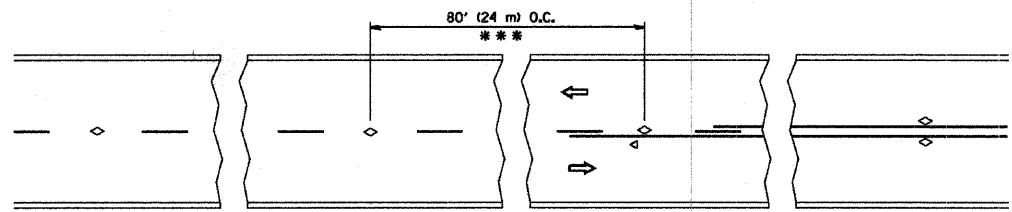
REVISIONS	
NAME	DATE
T. RAMMACHER	09/18/94
J. OBERLE	06/01/96
T. RAMMACHER	06/05/96
T. RAMMACHER	11/04/97
T. RAMMACHER	03/02/98
E. GOMEZ	08/28/00

ILLINOIS DEPARTMENT OF TRANSPORTATION
**PAVEMENT MARKING
LETTERS AND SYMBOLS
FOR TRAFFIC STAGING**

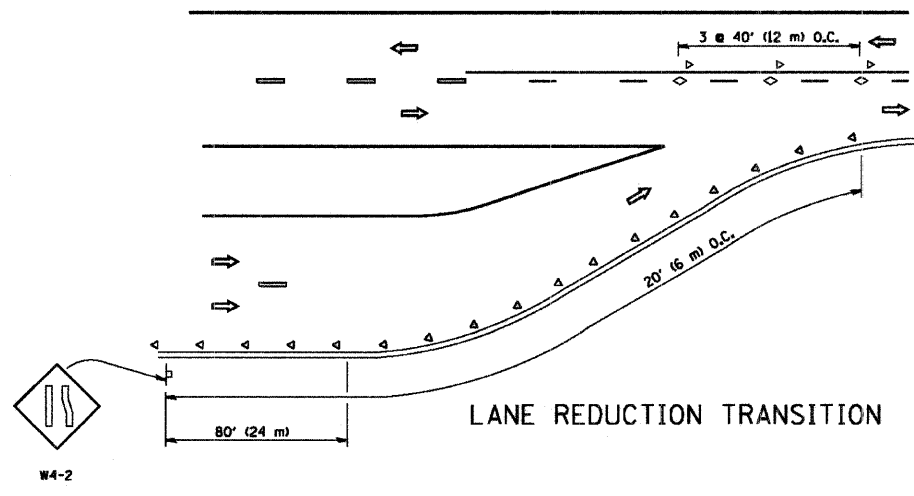
SCALE: NONE

DRAWN BY CADD
CHECKED BY

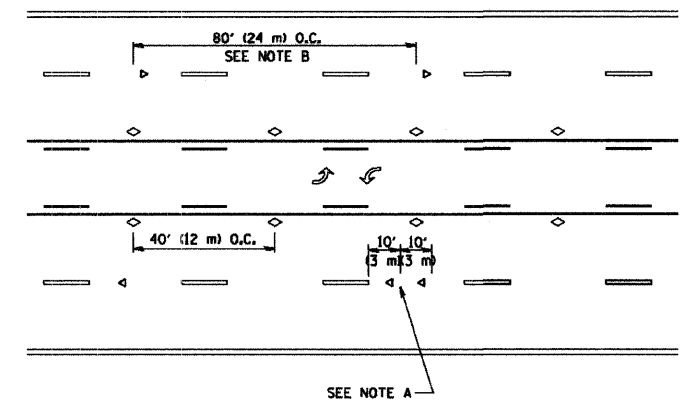
CONTRACT #63033



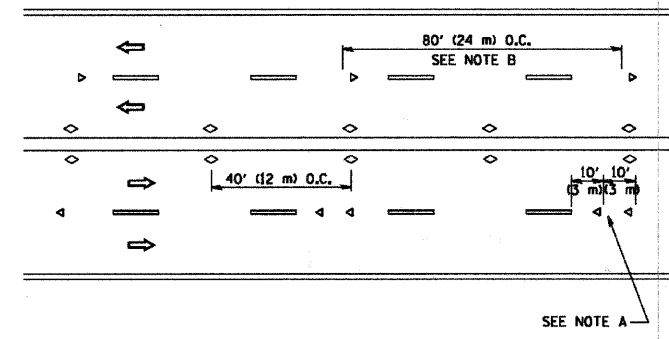
TWO-LANE/TWO-WAY



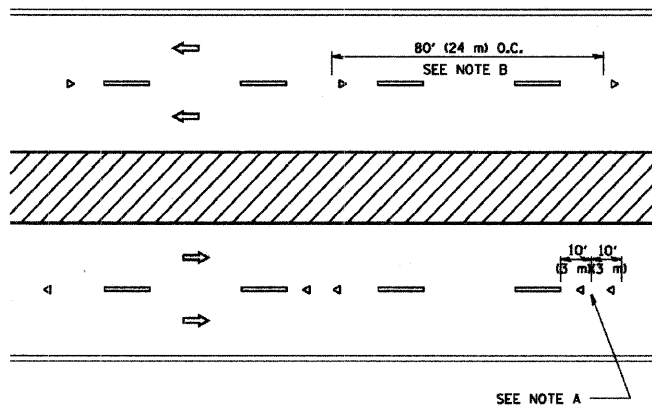
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

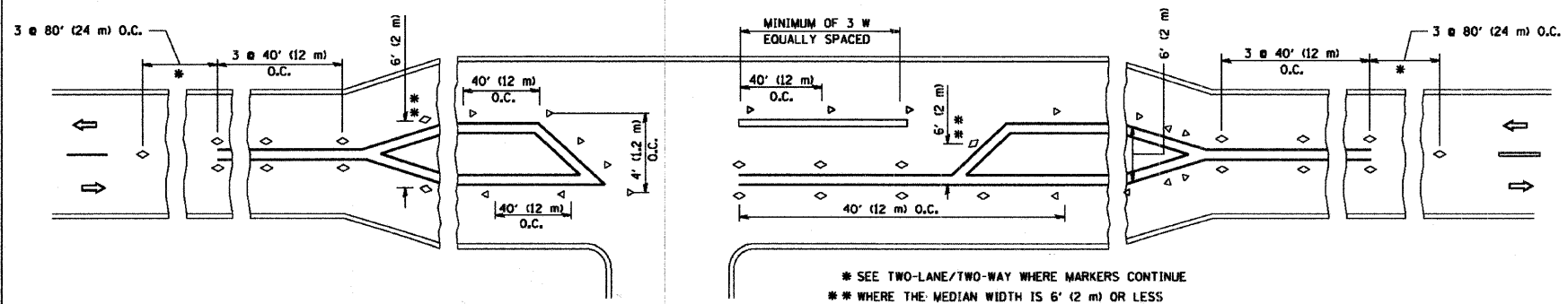
- YELLOW STRIPE
- WHITE STRIPE
- ◁ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◇ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

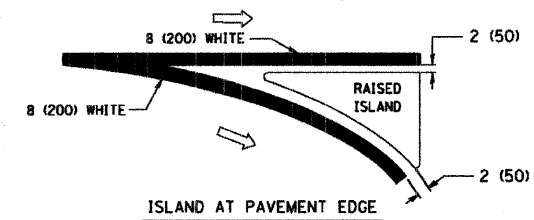
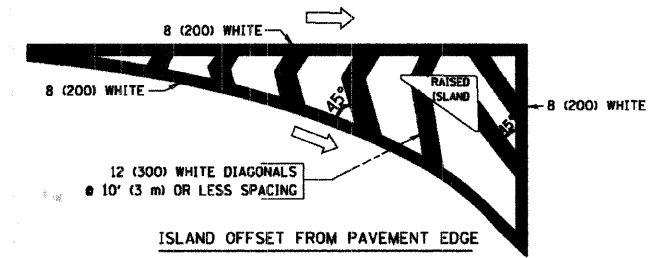
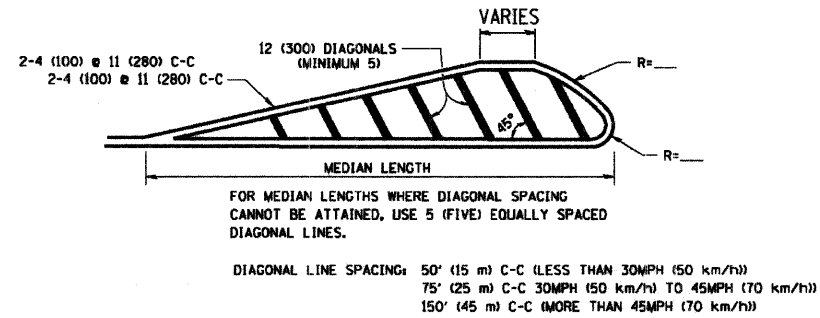
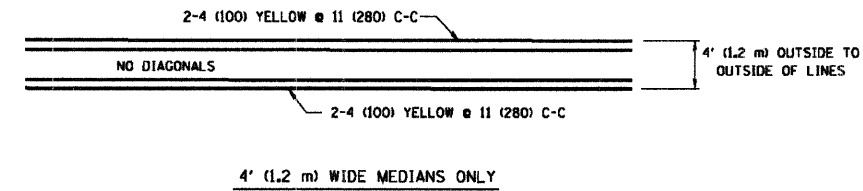
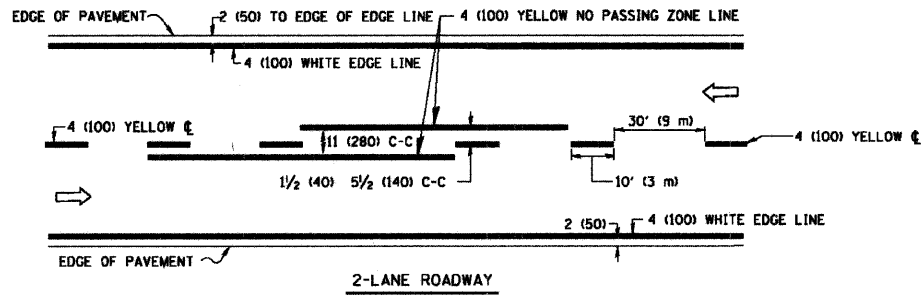
REVISIONS	
NAME	DATE
T. RAMMACHER	09-19-94
T. RAMMACHER	03-12-99
T. RAMMACHER	01-06-00

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT
 MARKERS (SNOW-PLOW RESISTANT)

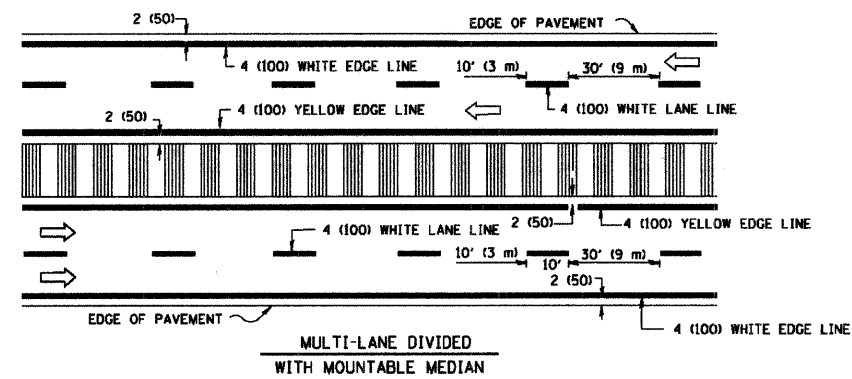
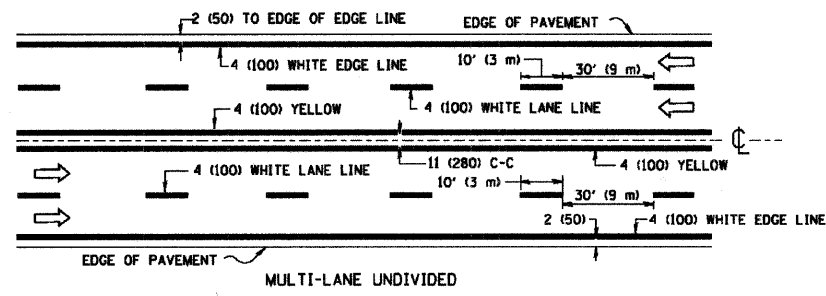
SCALE: NONE

DRAWN BY CADD
 CHECKED BY
 TC-11

CONTRACT #63033

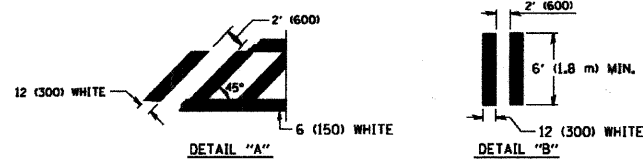
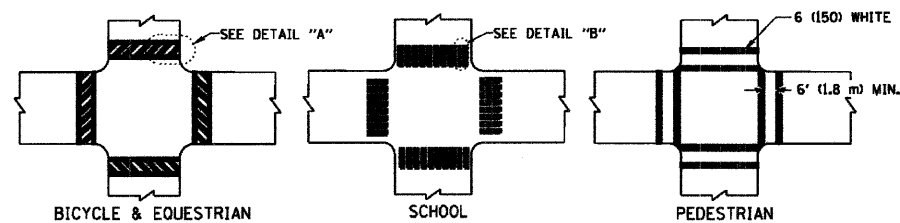


TYPICAL ISLAND MARKING

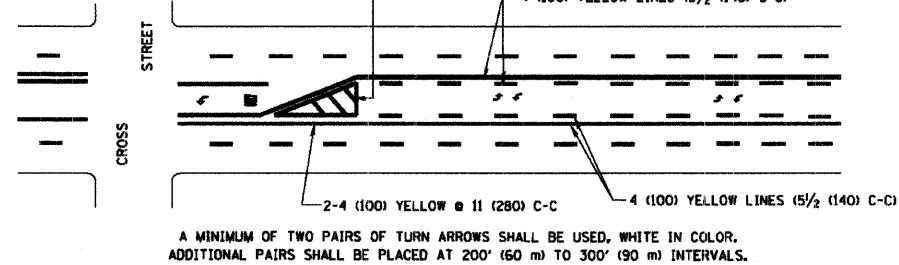


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

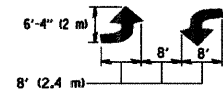
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

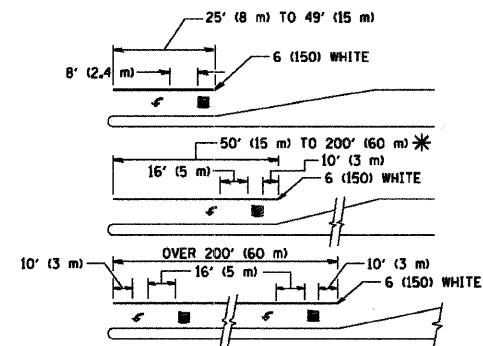


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m²) AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

REVISIONS	
NAME	DATE
EVERS	03-19-90
T. RAMMACHER	10-27-94
ALEX HOUSEH	10-09-96
ALEX HOUSEH	10-17-96
T. RAMMACHER	01-06-00

ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT
MARKINGS

SCALE: NONE

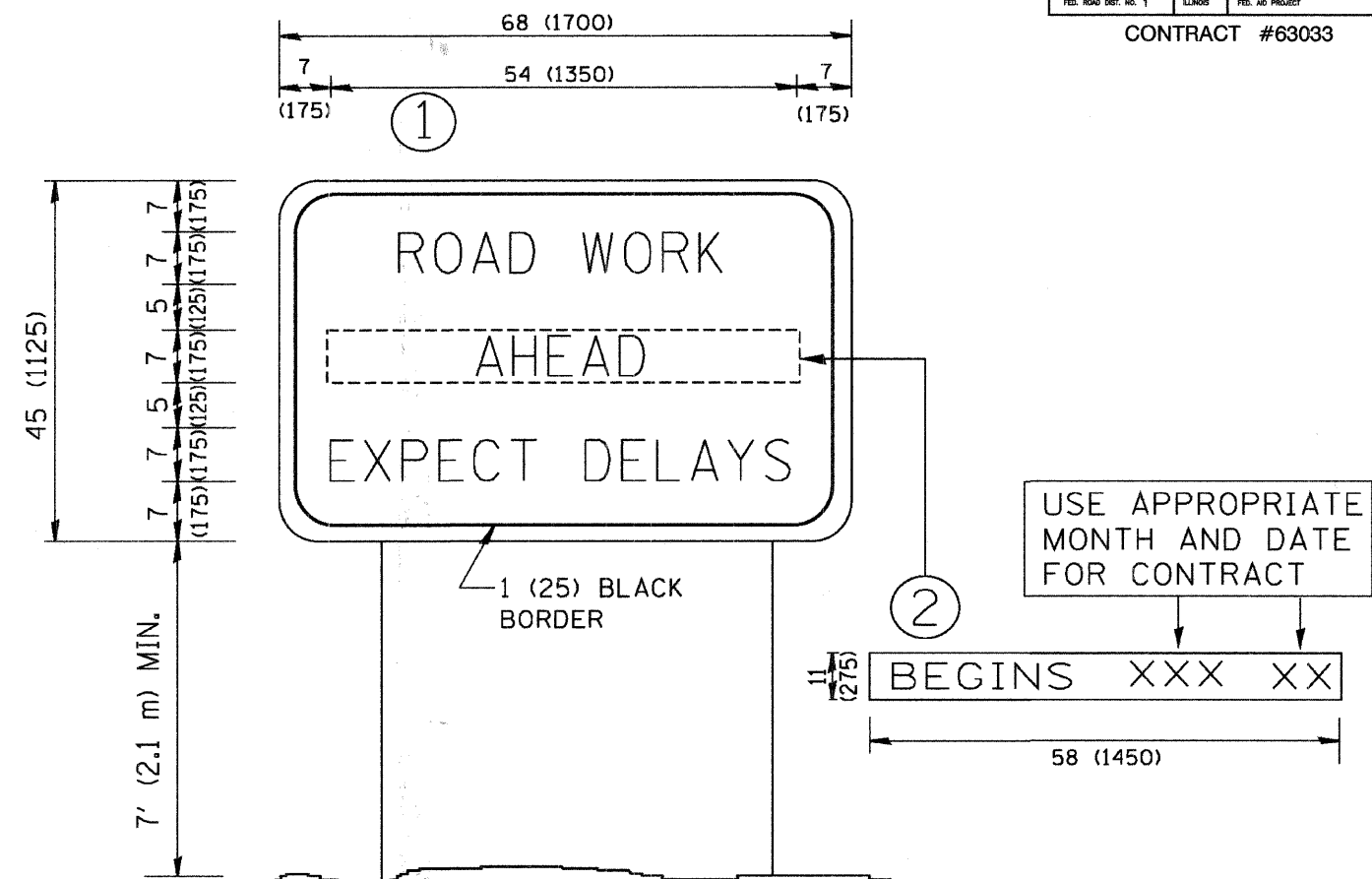
DRAWN BY CADD

CHECKED BY

TC-13

F. A. P. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	05-00047-00-CH	WILL	036	028
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

CONTRACT #63033



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
R. MIRS	9-15-97
R. MIRS	12-11-97
T. RAMMACHER	2-2-99
C. JUCIUS	1-31-07

ILLINOIS DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD INFORMATION SIGN

SCALE: NONE

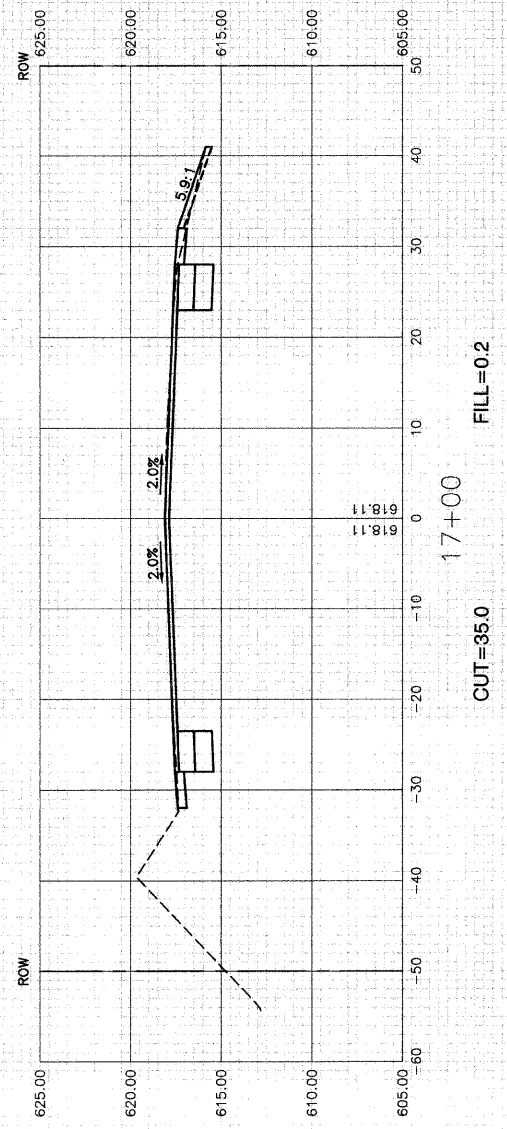
DRAWN BY DESIGN

CHECKED BY

TC22

F. A. P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	05-00047-00-CH	WILL	036	029
STA. 15+50		TO STA. 17+00		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

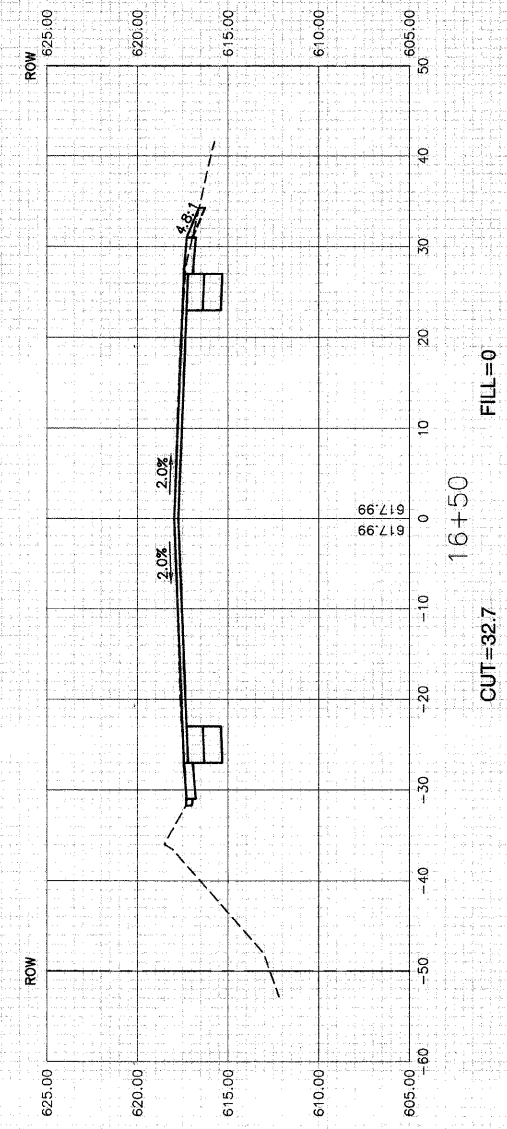
CONTRACT #63033



17+00

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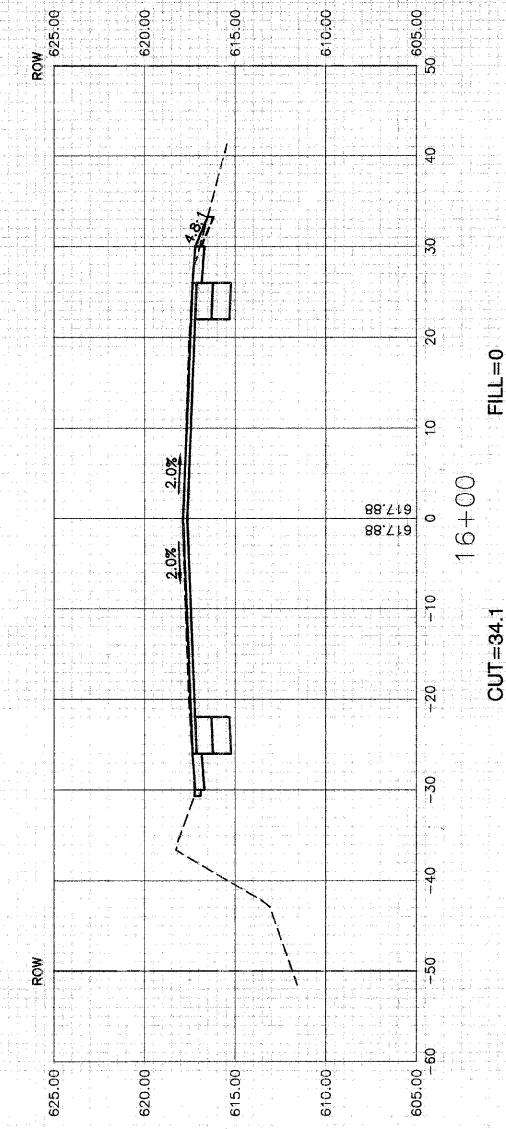
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16+50

CUT=32.7

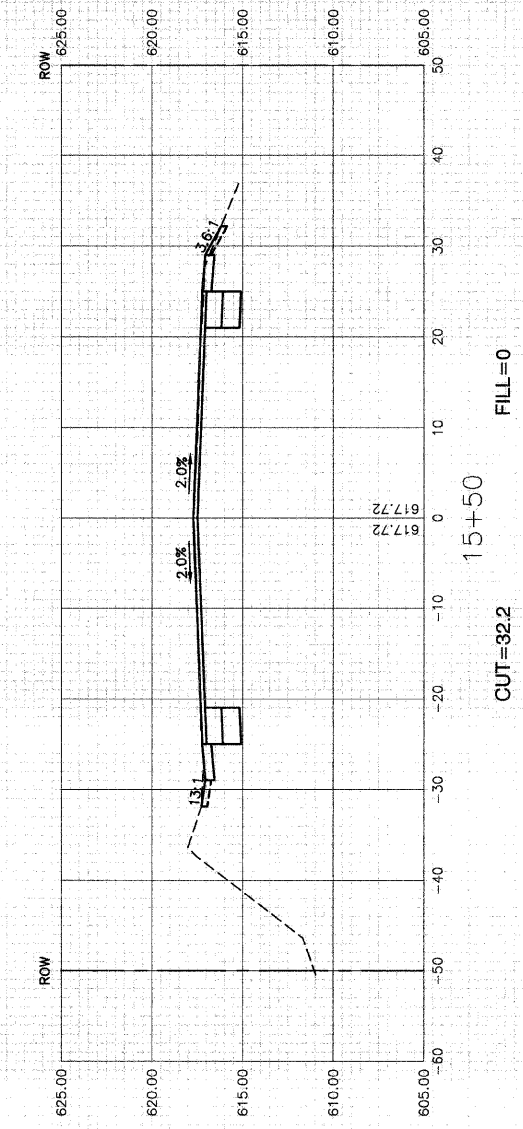
FILL=0



16+00

CUT=34.1

FILL=0



15+50

CUT=32.2

FILL=0

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

IL RTE 53 AT UNIVERSITY PARKWAY
INTERSECTION IMPROVEMENT
CROSS SECTIONS

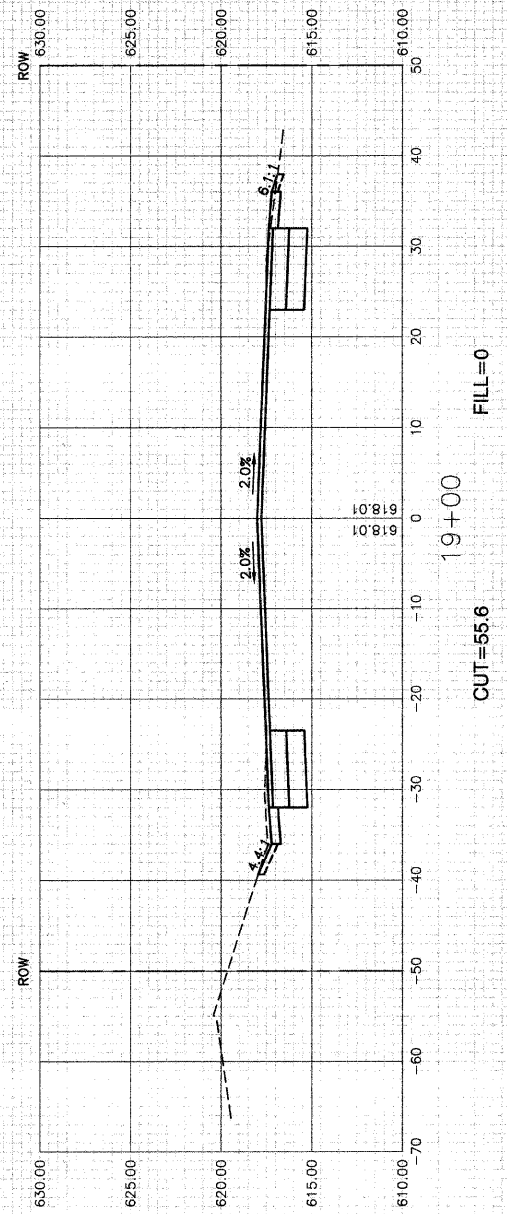
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HORIZ. 1"=10'
DATE 8-29-08

DRAWN BY RG
CHECKED BY WD

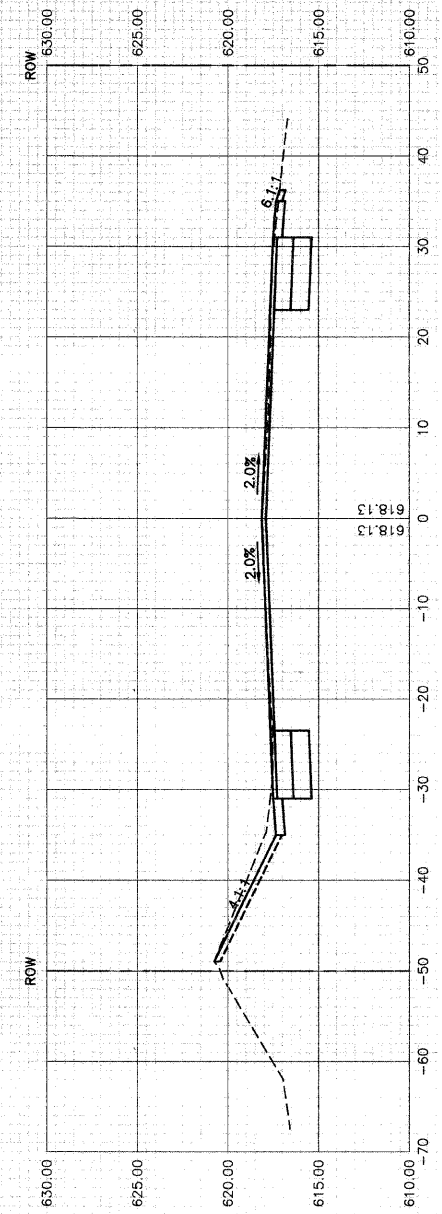
ROBINSON ENGINEERING, LTD.
1700 SOUTH PARK AVENUE SOUTH HOLLAND, ILLINOIS 60473
(708) 381-8200 FAX (708) 381-8628
ILLINOIS DESIGN FIRM REGISTRATION NO. 186001158 04161_02-XSEC-01

P. A. P. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	05-00047-00-CH	WILL	036	030
STA. 15+50		TO STA. 17+00		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

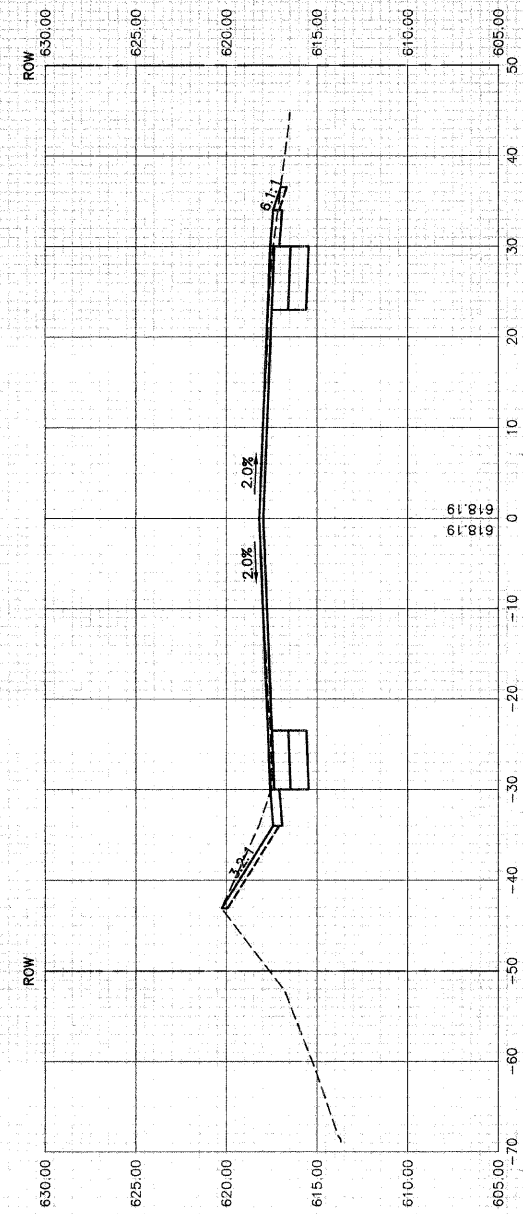
CONTRACT #63033



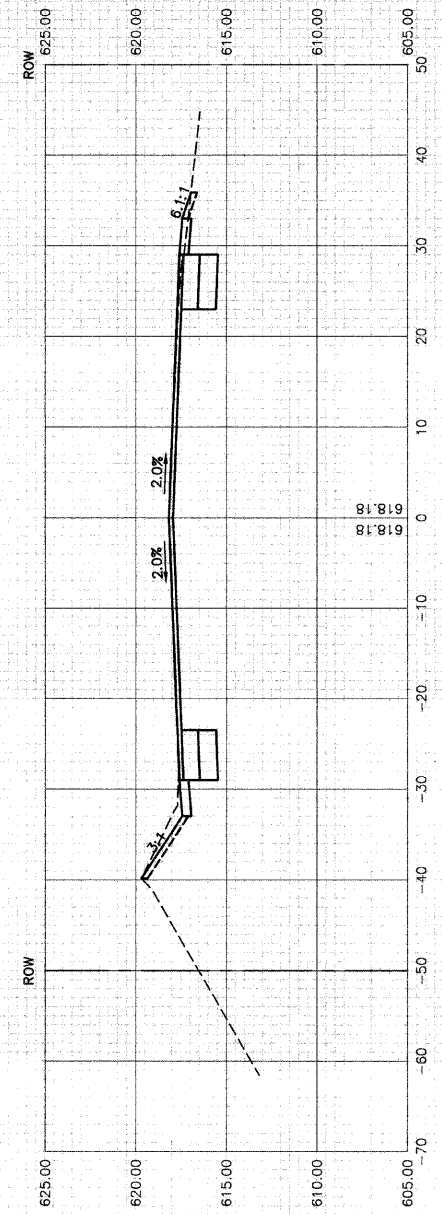
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FILL=0



CUT=54.9
FILL=0



CUT=48.2
FILL=0



CUT=43.0
FILL=0

REVISIONS	
NAME	DATE

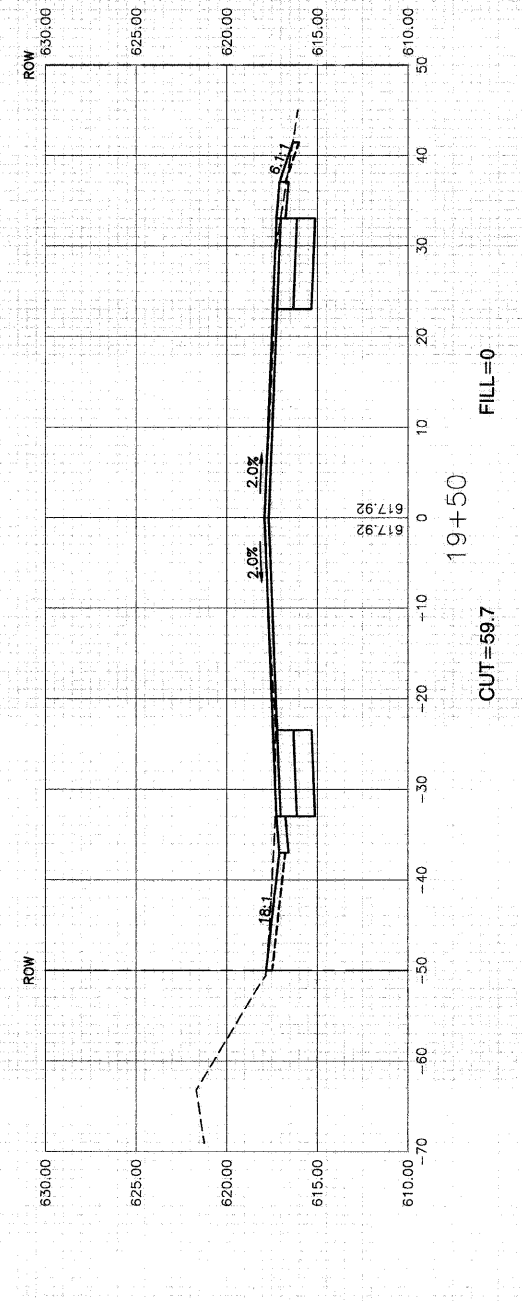
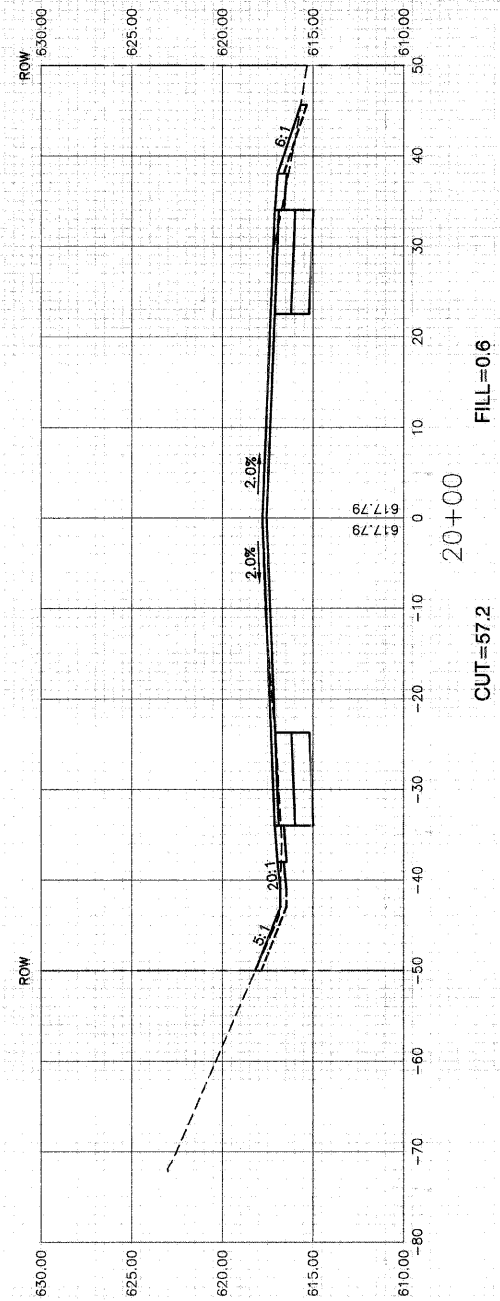
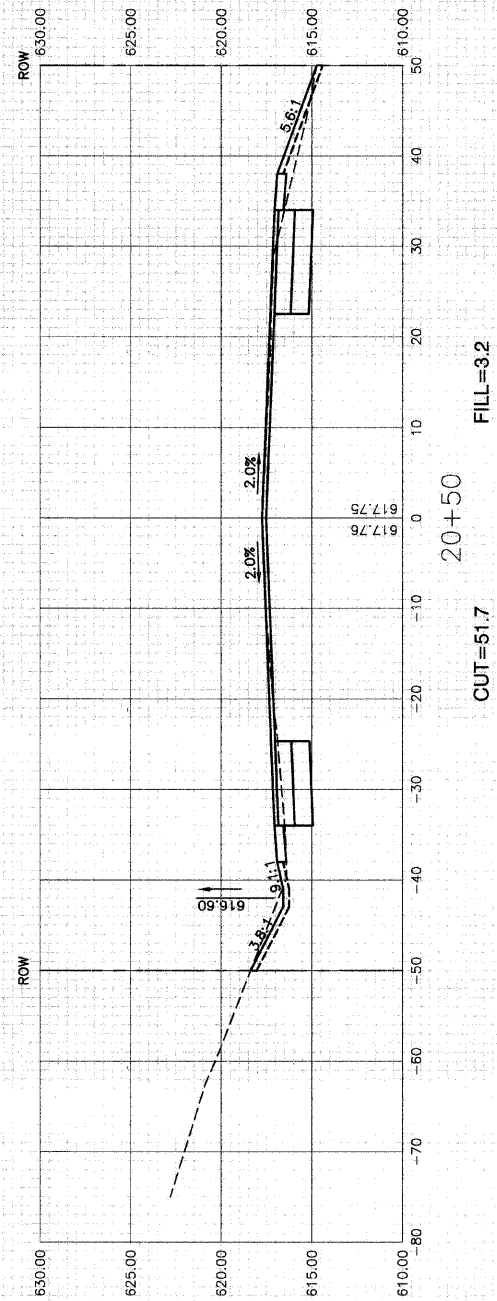
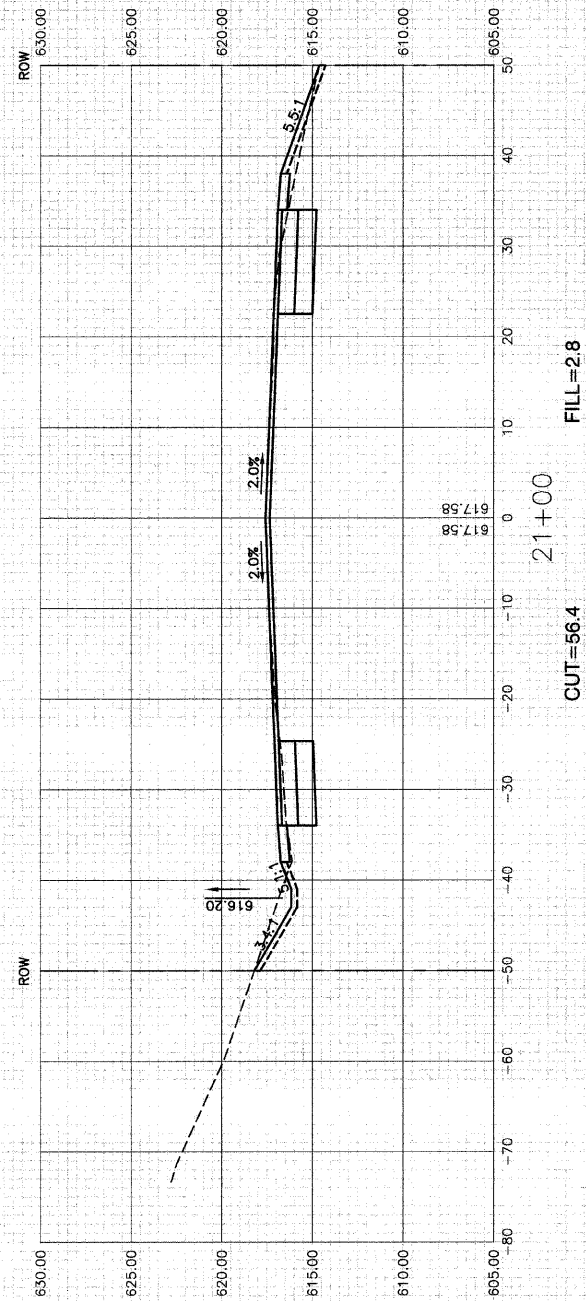
ILLINOIS DEPARTMENT OF TRANSPORTATION

IL RTE 53 AT UNIVERSITY PARKWAY
INTERSECTION IMPROVEMENT
CROSS SECTIONS

SCALE: VERT. 1"=5'
HORIZ. 1"=10'
DATE: 8-29-08
DRAWN BY: RG
CHECKED BY: WD

F. A. P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	05-00047-00-CH	WILL	036	031
STA. 15+50		TO STA. 17+00		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

CONTRACT #63033



REVISIONS	
NAME	DATE

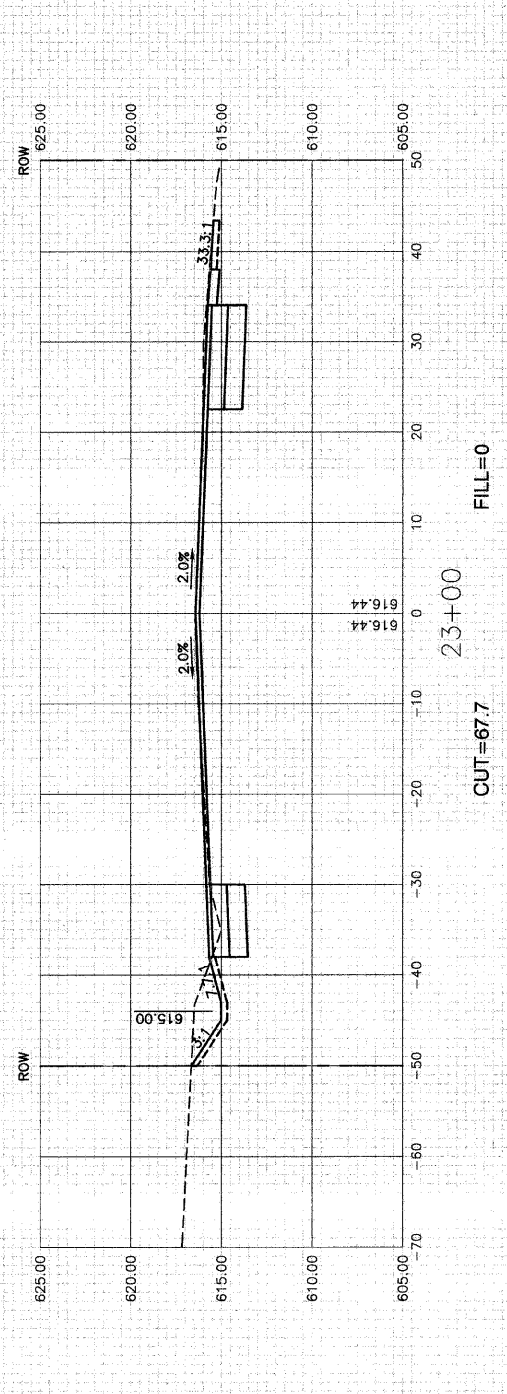
ILLINOIS DEPARTMENT OF TRANSPORTATION

IL RTE 53 AT UNIVERSITY PARKWAY
INTERSECTION IMPROVEMENT
CROSS SECTIONS

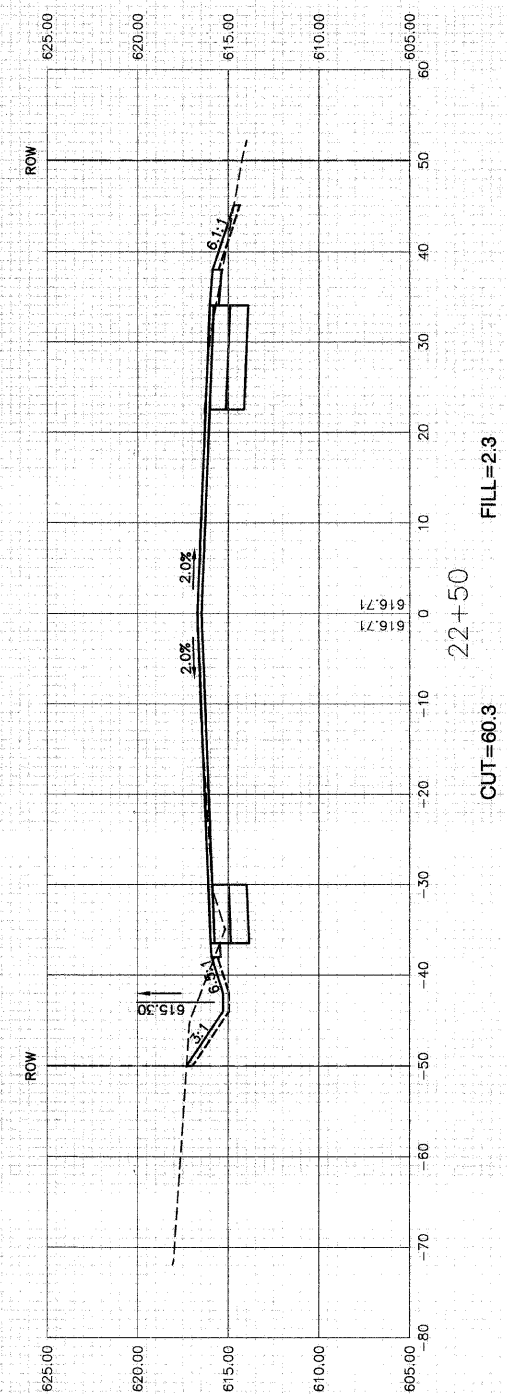
SCALE: VERT. 1"=5'
HORIZ. 1"=10'
DATE 8-29-08
DRAWN BY RG
CHECKED BY WD

F. A. P. ETC.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	05-00047-00-CH	WILL	036	032
STA. 15+50		TO STA. 17+00		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

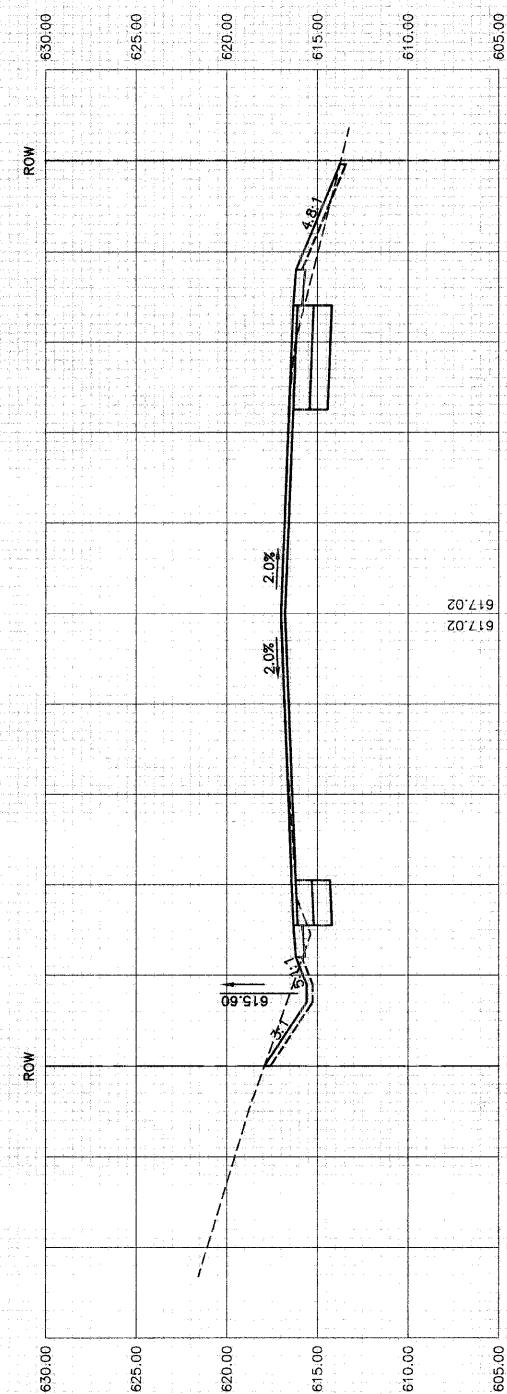
CONTRACT #63033



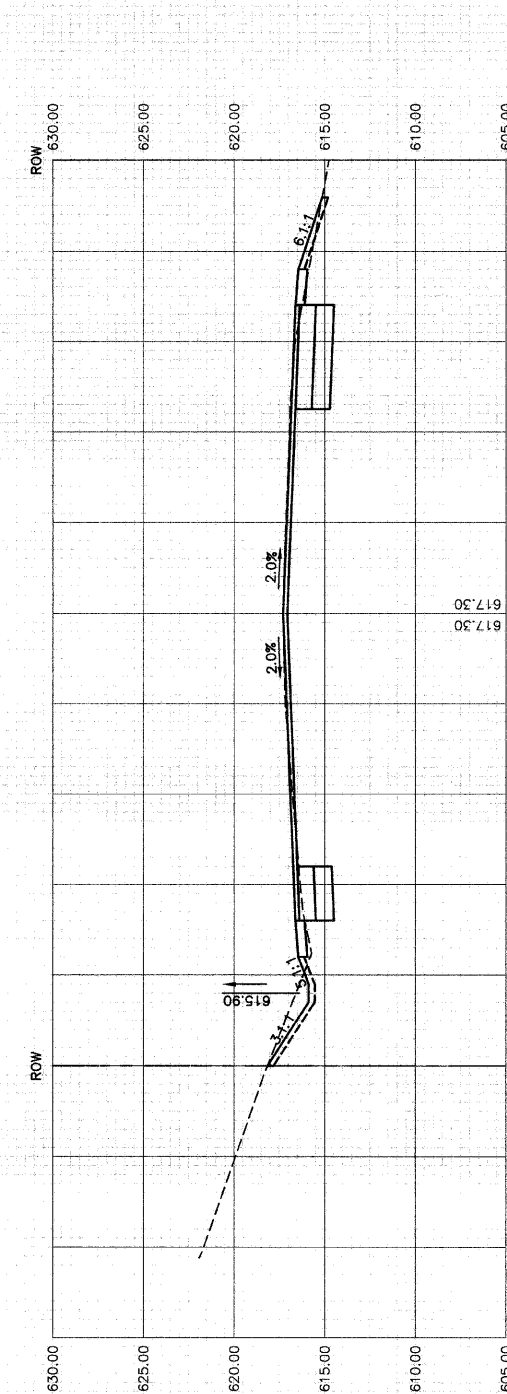
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FILL=0



CUT=60.3
FILL=2.3



CUT=52.9
FILL=4.3



CUT=54.5
FILL=1.2

REVISIONS	
NAME	DATE

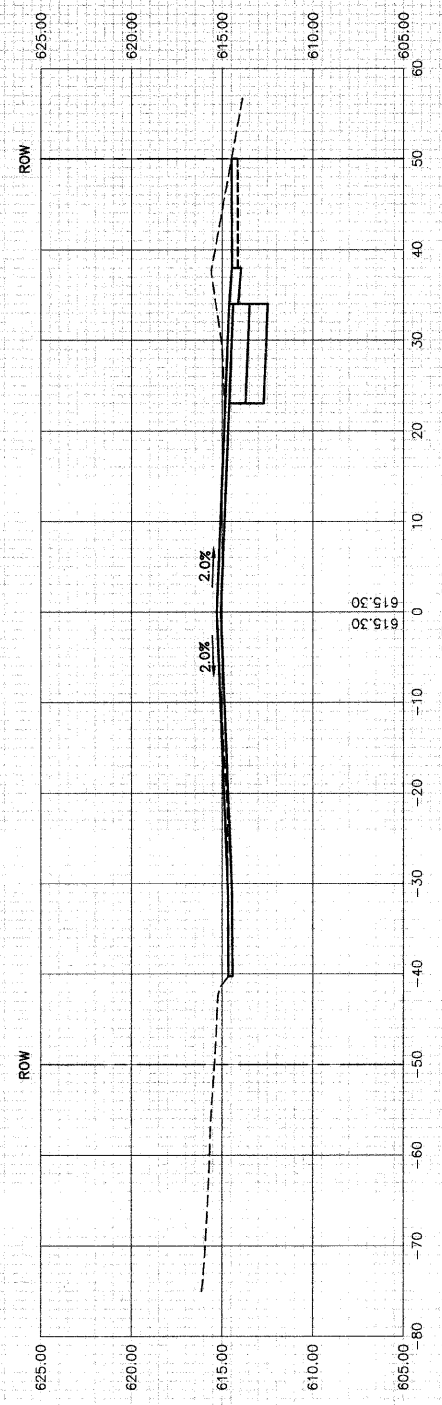
ILLINOIS DEPARTMENT OF TRANSPORTATION

IL RTE 53 AT UNIVERSITY PARKWAY
INTERSECTION IMPROVEMENT
CROSS SECTIONS

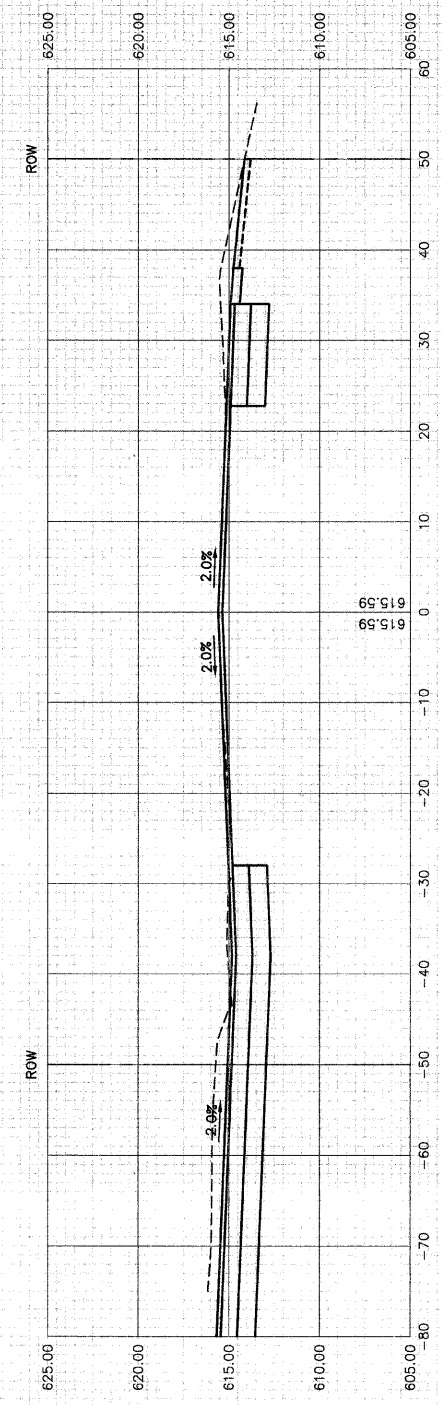
SCALE: VERT. 1"=5'
HORIZ. 1"=10'
DATE: 8-29-08
DRAWN BY: RG
CHECKED BY: WD

P. A. P. SHEET	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	05-00047-00-CH	WILL	036	033
STA. 15+50		TO STA. 17+00		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

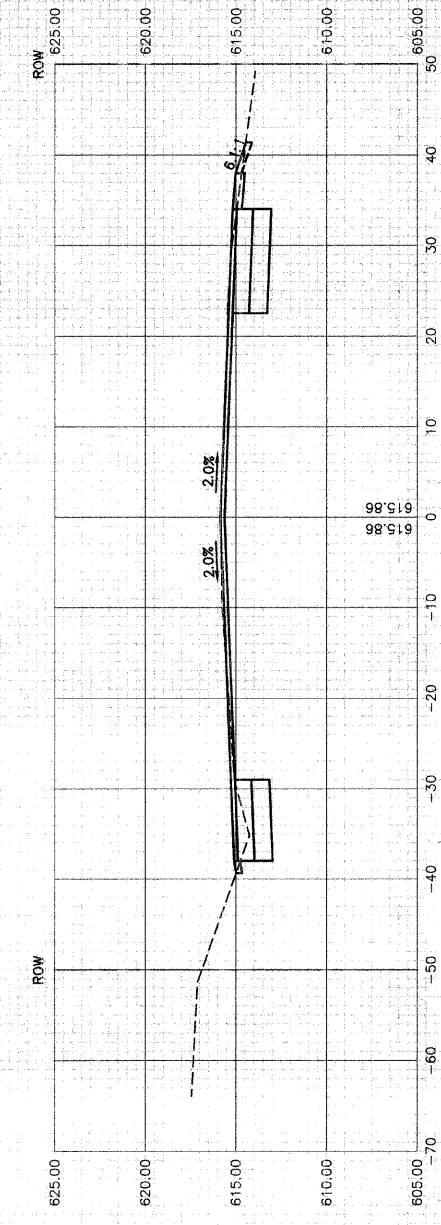
CONTRACT #63033



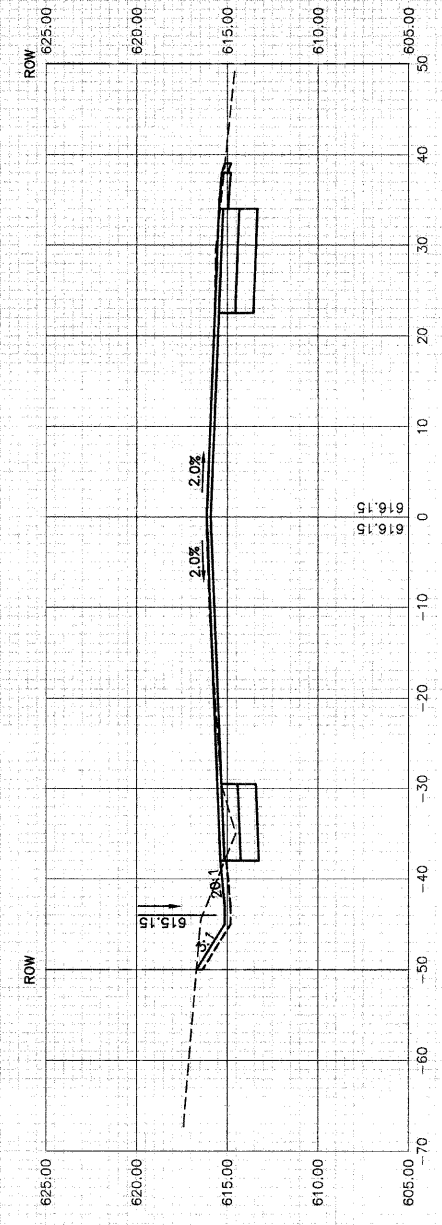
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CUT=182.5 FILL=0



CUT=51.5 FILL=0



CUT=62.3 FILL=0

REVISIONS	
NAME	DATE

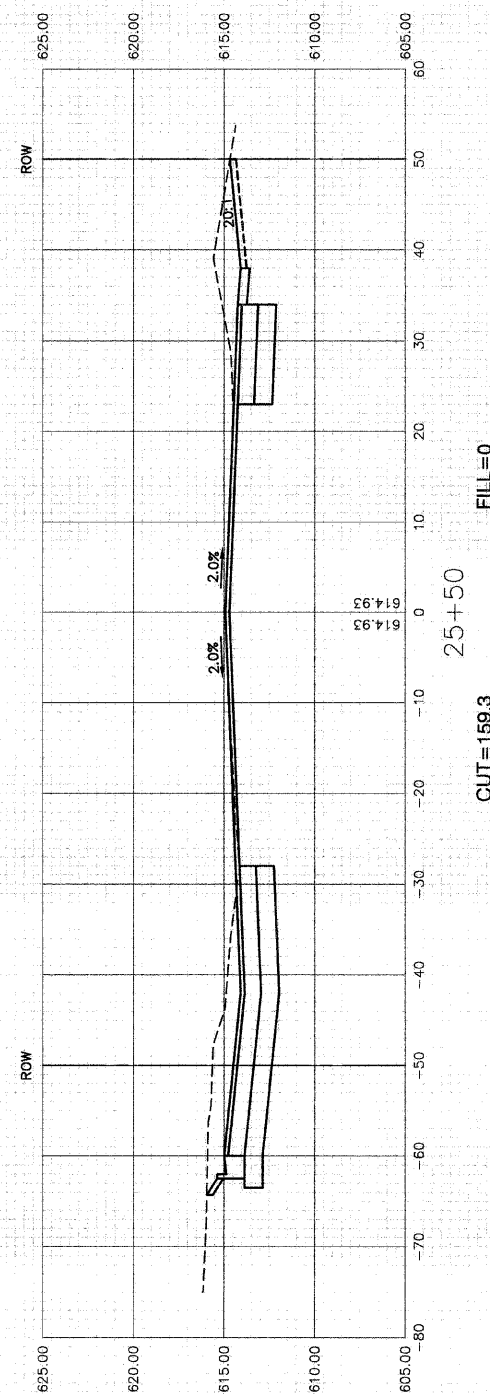
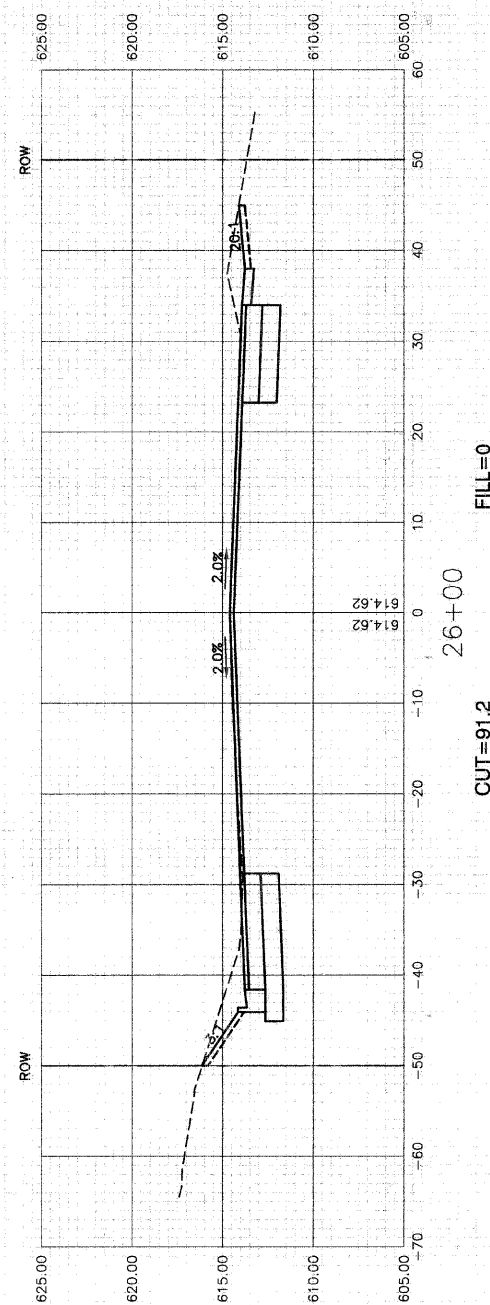
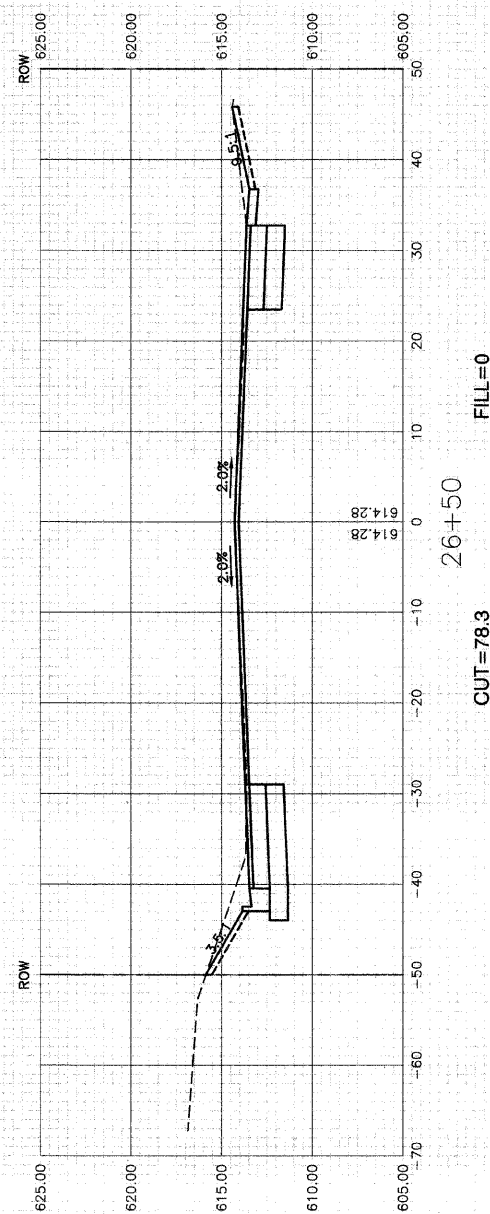
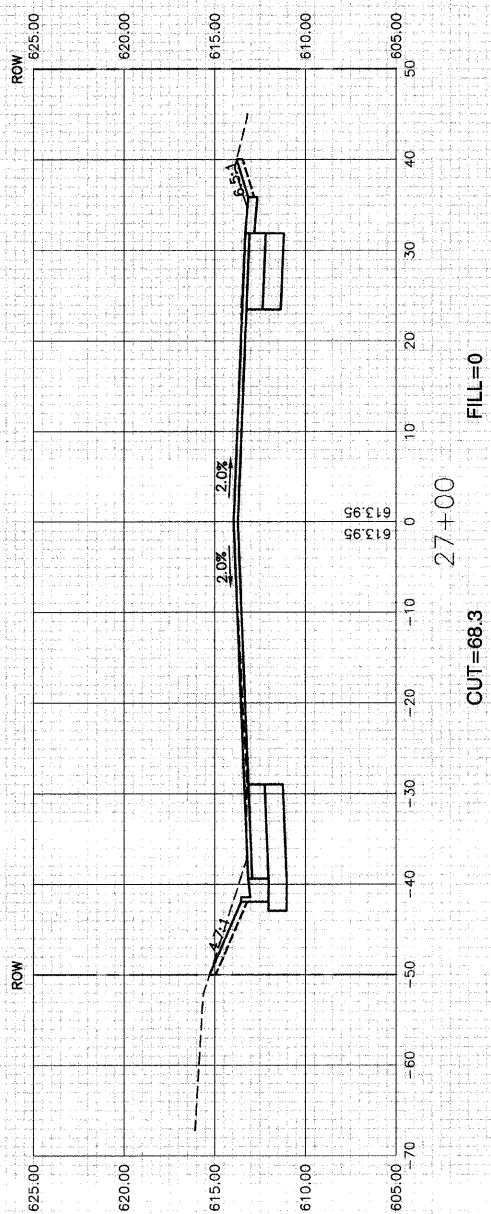
ILLINOIS DEPARTMENT OF TRANSPORTATION

IL RTE 53 AT UNIVERSITY PARKWAY INTERSECTION IMPROVEMENT CROSS SECTIONS

SCALE: VERT. 1"=5'
 HORIZ. 1"=10'
 DATE: 8-29-08
 DRAWN BY: RG
 CHECKED BY: WD

F. A. P. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	05-00047-00-CH	WILL	036	034
STA. 15+50		TO STA. 17+00		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

CONTRACT #63033



REVISIONS	
NAME	DATE

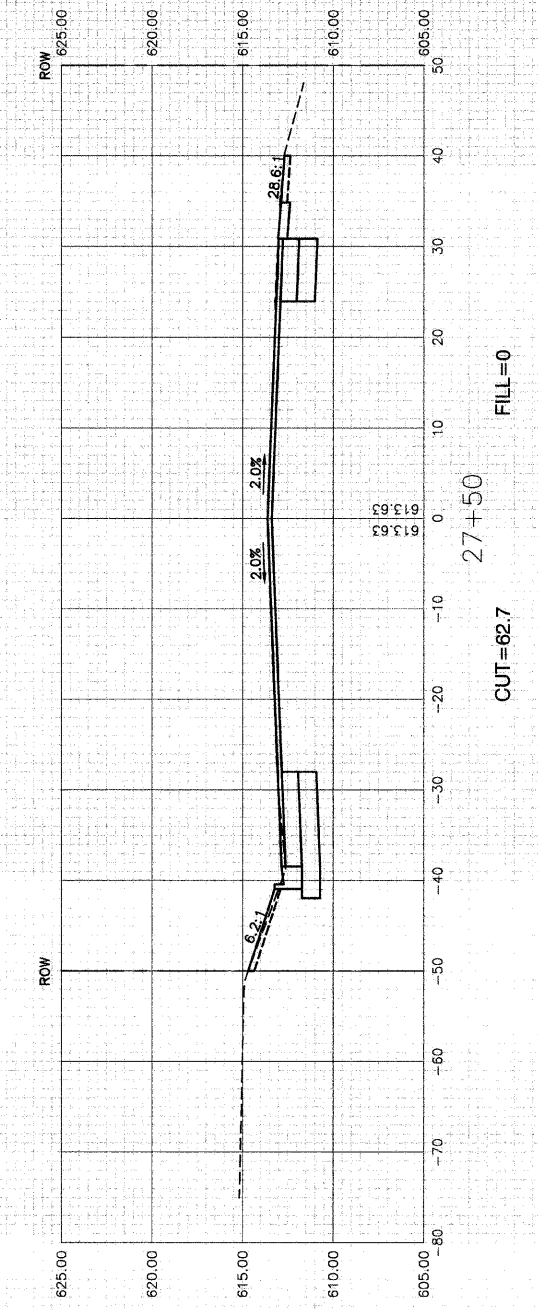
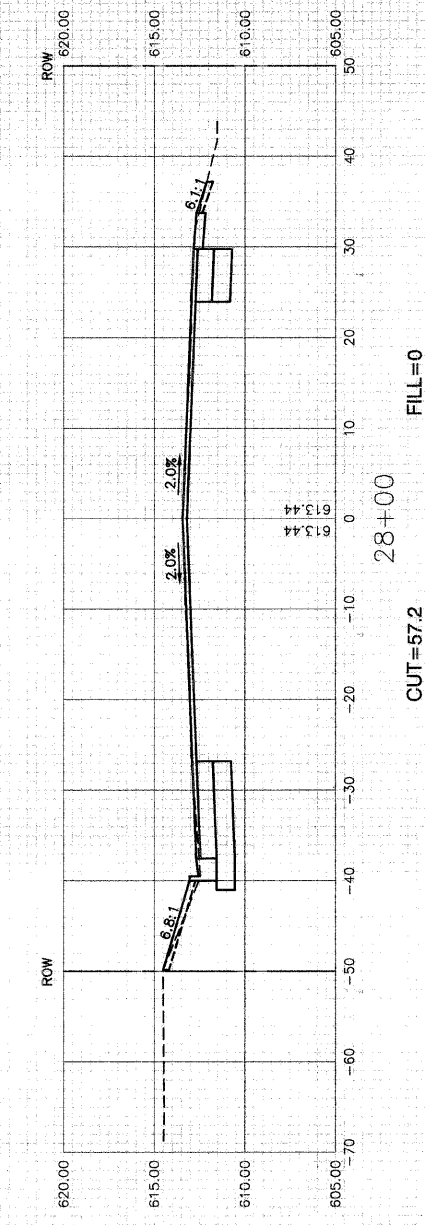
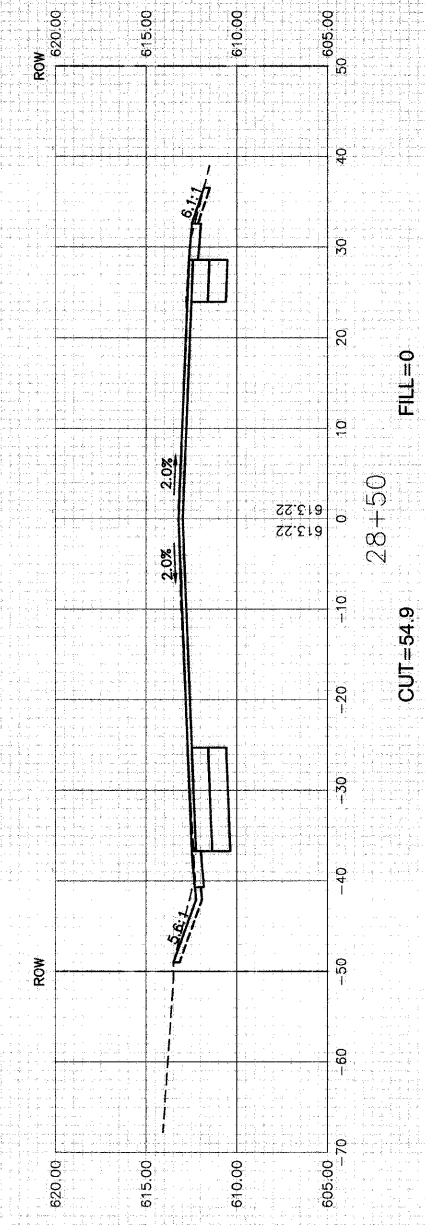
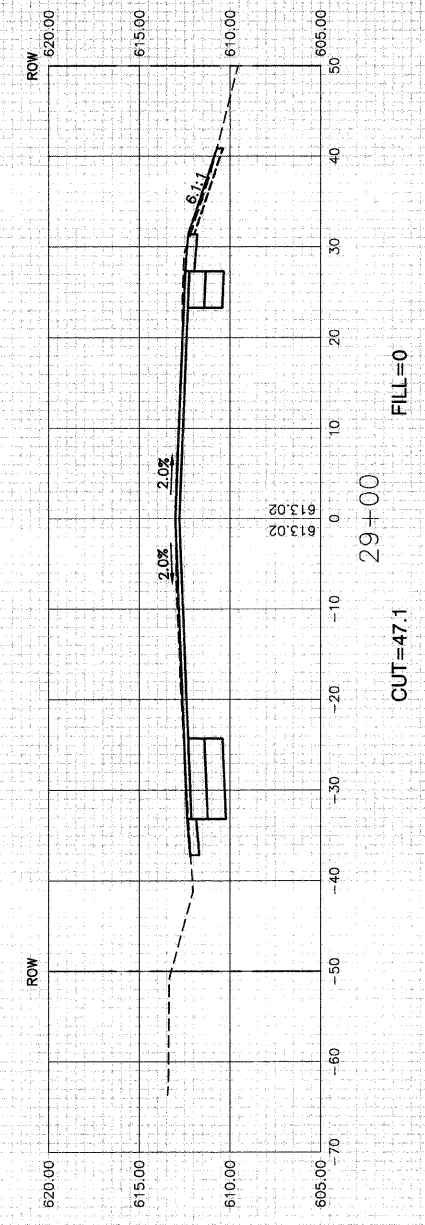
ILLINOIS DEPARTMENT OF TRANSPORTATION

IL RTE 53 AT UNIVERSITY PARKWAY
INTERSECTION IMPROVEMENT
CROSS SECTIONS

SCALE: VERT. 1"=5'
HORIZ. 1"=10'
DATE: 8-29-08
DRAWN BY: RG
CHECKED BY: WD

F. A. P. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	05-00047-00-CH	WILL	036	035
STA. 15+50		TO STA. 17+00		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

CONTRACT #63033



REVISIONS	
NAME	DATE

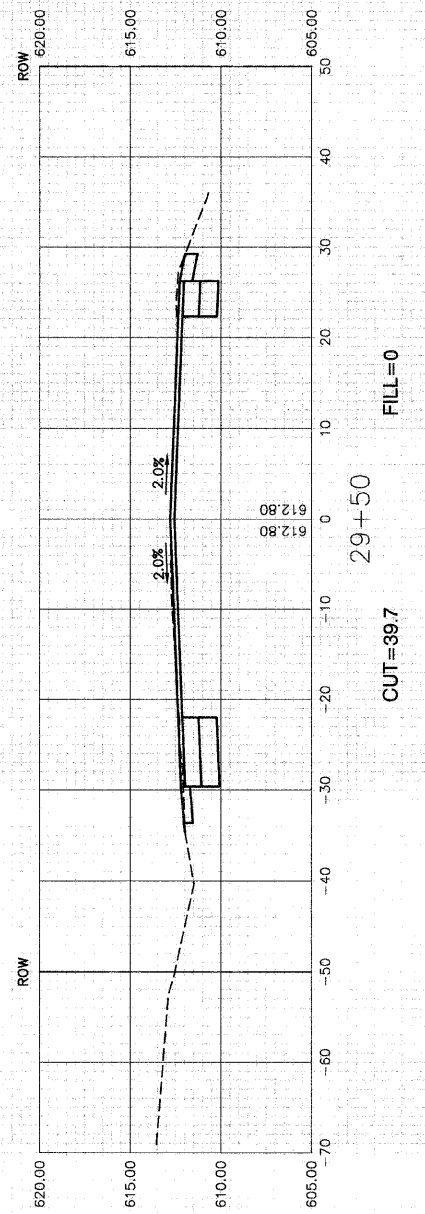
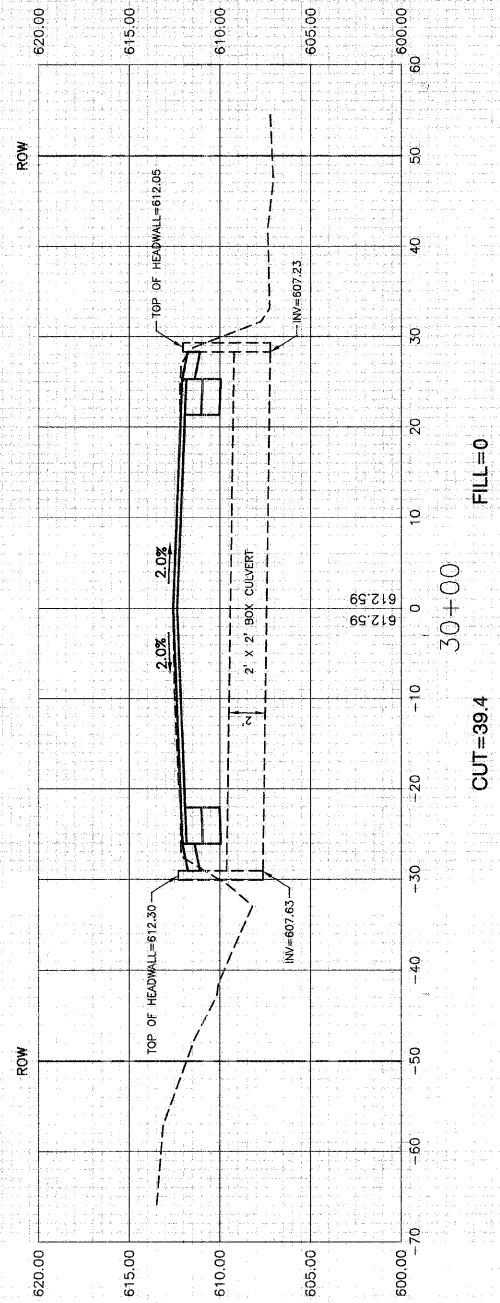
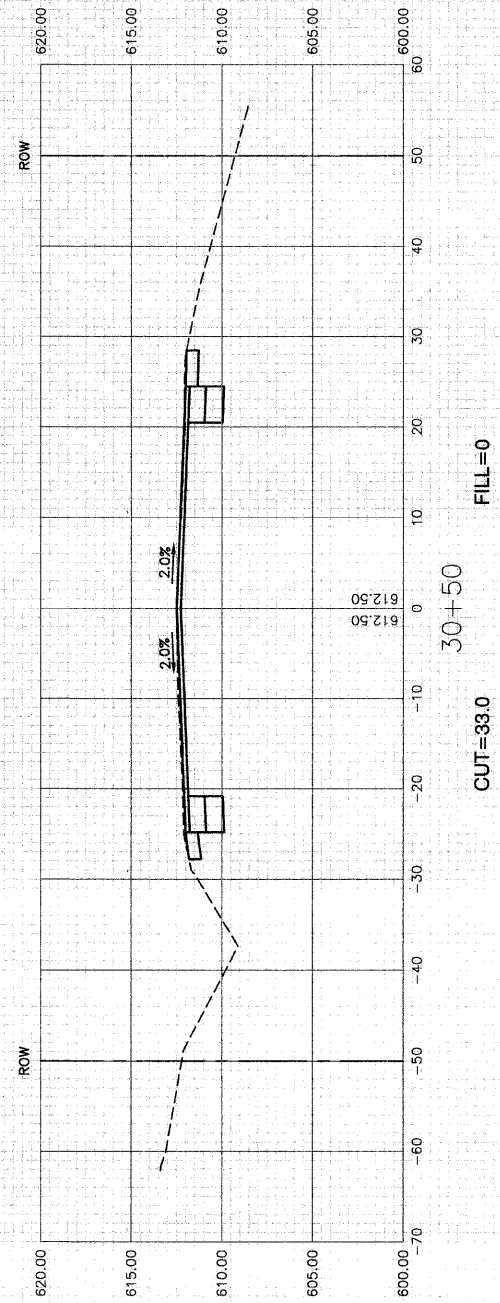
ILLINOIS DEPARTMENT OF TRANSPORTATION

IL RTE 53 AT UNIVERSITY PARKWAY
INTERSECTION IMPROVEMENT
CROSS SECTIONS

SCALE: VERT. 1"=5'
HORIZ. 1"=10'
DATE: 8-29-08
DRAWN BY: RG
CHECKED BY: WD

F. A. P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	05-00047-00-CH	WILL	036	036
STA. 15+50		TO STA. 17+00		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

CONTRACT #63033



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

IL RTE 53 AT UNIVERSITY PARKWAY INTERSECTION IMPROVEMENT CROSS SECTIONS

SCALE: VERT. 1"=5'
 HORIZ. 1"=10'
 DATE: 8-29-08

DRAWN BY: RG
 CHECKED BY: WD