

007

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

F.A.I. DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2019-056-BR	COOK	31	1
			CONTRACT NO. 62J33	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

# PROPOSED HIGHWAY PLANS

F.A.I. ROUTE 57: I-57  
 AT IL ROUTE 1 (HALSTED ST.)  
 SECTION: 2019-056-BR  
 PROJECT: STP-0652(581)  
 BRIDGE & JOINT REPAIR, BRIDGE DECK OVERLAY  
 COOK COUNTY  
 C-91-417-19

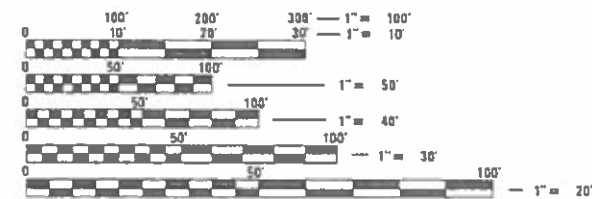
**TRAFFIC DATA**

IL RTE 1:  
 EXISTING ADT = 26600 (2018)  
 POSTED SPEED LIMIT = 30 MPH

I-57:  
 EXISTING ADT = 128800 (2018)  
 POSTED SPEED LIMIT = 55 MPH

THE IMPROVEMENT IS LOCATED IN  
THE CITY OF CHICAGO

LOCATION OF  
IMPROVEMENT:  
SN 016-2036

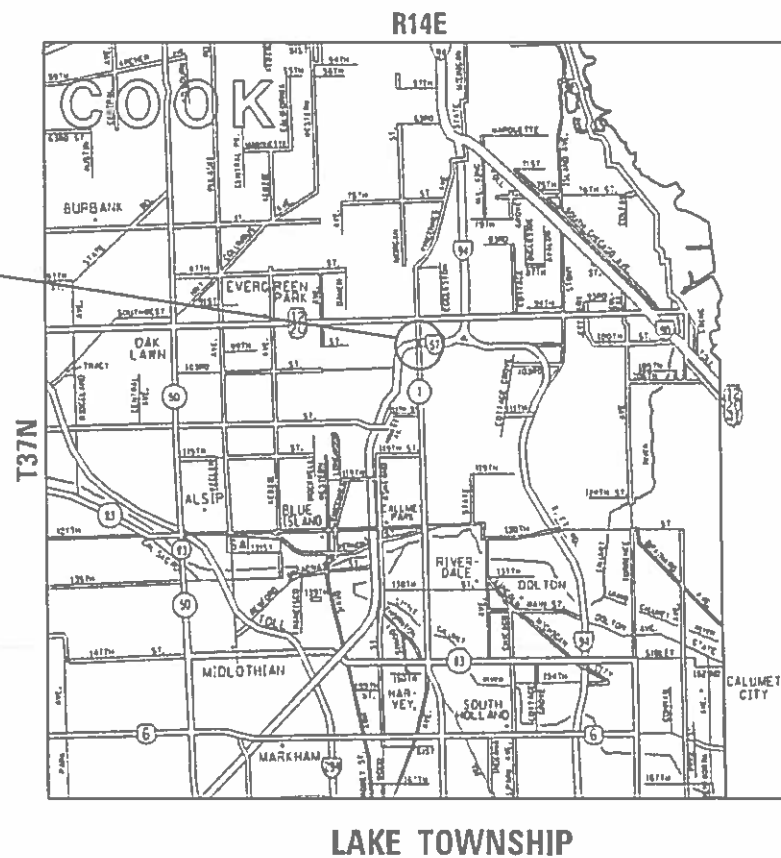


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD  
 ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT  
 CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS  
 ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

C.U.A.N.  
 CHICAGO UTILITY ALERT NETWORK  
 1-312-744-7000

PROJECT ENGINEER: ALAIN MIDY (847) 221-3056  
 PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247

CONTRACT NO. 62J33



GROSS LENGTH = 250 FT. = 0.0473 MILE  
 NET LENGTH = 250 FT. = 0.0473 MILE

D-91-183-19



LOCATION OF SECTION INDICATED THIS: —

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SUBMITTED October 15, 2019  
*Anthony J. Rungler* / ABS REGIONAL ENGINEER  
*Doc De...* 2019  
 ENGINEER OF DESIGN AND ENVIRONMENT  
*Doc De...* 2019  
 DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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OF THE STATE OF ILLINOIS

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**STATE STANDARDS**

701101-05 OFF-RD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE  
 701427-05 LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS < 40 MPH  
 701601-09 URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN  
 701701-10 URBAN LANE CLOSURE, MULTILANE INTERSECTION  
 701801-06 SIDEWALK, CORNER OR CROSSWALK CLOSURE  
 701901-08 TRAFFIC CONTROL DEVICES

**GENERAL NOTES – LIGHTING**

- MEADE ELECTRIC CO. DISTRICT ONE ELECTRICAL MAINTENANCE CONTRACTOR LOCATES IDOT ELECTRICAL EQUIPMENT AND UNDERGROUND CABLES 773-287-7672

**GENERAL NOTES**

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL C.U.A.N. AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. (48 HOUR NOTIFICATION IS REQUIRED).
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF CHICAGO.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
- IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV FOR ARTERIALS AND (847) 705-4151 FOR EXPRESSWAYS A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE CONTRACTOR SHALL REQUEST AND GAIN APPROVAL FROM THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S EXPRESSWAY TRAFFIC OPERATIONS ENGINEER AT WWW.IDOTLCS.COM AT LEAST 24 HOURS IN ADVANCE OF ALL DAILY LANE, RAMP, AND SHOULDER CLOSURES AND 7 DAYS IN ADVANCE OF ALL PERMANENT AND WEEKEND CLOSURES ON ALL FREEWAYS AND/OR EXPRESSWAYS. THIS ADVANCE NOTIFICATION IS CALCULATED BASED ON WORKWEEK OF MONDAY THROUGH FRIDAY AND SHALL NOT INCLUDE WEEKENDS AND HOLIDAYS.
- THE ENGINEER SHALL EMAIL PATRICE HARRIS, AREA TRAFFIC FIELD TECHNICIAN, AT PATRICE.HARRIS@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM THE BUREAU OF MAINTENANCE.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- THE "ARTERIAL ROAD INFORMATION SIGN (TC-22)" IS APPLICABLE ONLY TO ARTERIAL ROADS AND SHALL NOT BE APPLIED TO EXPRESSWAYS/TOLLWAYS.

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USER NAME = rothjp	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>I-57 AT IL ROUTE 1 (HALSTED ST.) INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES</b>		F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
DRAWN -	REVISED -	57				2019-056-BR	COOK	31	2	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -		CONTRACT NO. 62J33						
PLOT DATE = 10/16/2019	DATE -	REVISED -		SCALE:	SHEET 1 OF 1 SHEETS	STA. TO STA.	ILLINOIS	FED. AID PROJECT		

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE							
CODE NO	ITEM	UNIT		0013 20% STATE 80% FED.							0013 20% STATE 80% FED.						
X0900053	PREFORMED JOINT SEAL 2 1/4"	FOOT	108.5	108.5					* 78009006	MODIFIED URETHANE PAVEMENT MARKING - LINE 6"	FOOT	294	294				
64300450	IMPACT ATTENUATORS (NON-REDIRECTIVE), TEST LEVEL 3	EACH	1	1													
52000040	PREFORMED JOINT SEAL 3 1/2"	FOOT	108.5	108.5					* 78200011	BARRIER WALL REFLECTORS, TYPE C	EACH	8	8				
									* X0327577	PROTECT AND MAINTAIN EXISTING UNDERPASS LUMINAIRE	L SUM	1	1				
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6													
67100100	MOBILIZATION	L SUM	1	1					X0327980	PAVEMENT MARKING REMOVAL - WATER BLASTING	SO FT	1065	1065				
70200100	NIGHTTIME WORK ZONE LIGHTING	L SUM	1	1													
70300900	PAVEMENT MARKING TAPE, TYPE IV - LETTERS AND SYMBOLS	SO FT	301.6	301.6													
70300904	PAVEMENT MARKING TAPE, TYPE IV 4"	FOOT	7432	7432													
70400100	TEMPORARY CONCRETE BARRIER	FOOT	250	250					X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1				
70300906	PAVEMENT MARKING TAPE, TYPE IV 6"	FOOT	469	469													
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	652	652					X7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	L SUM	1	1				
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	567	567					X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SO FT	2478	2478				
									Z0012102	CONCRETE BRIDGE DECK SCARIFICATION 3/8 INCH	SO YD	1701	1701				
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	87	87					Z0012193	BRIDGE DECK THIN POLYMER OVERLAY 3/8"	SO YD	1701	1701				
									Z0012754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SO FT	1218	1218				
* 78009000	MODIFIED URETHANE PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	72.8	72.8					Z0012755	STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)	SO FT	12	12				
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	1250	1250					Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	102.8	102.8				

FILE NAME =	USER NAME = rdhjp	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>I-57 AT IL ROUTE 1 (HALSTED ST.) SUMMARY OF QUANTITIES</b>	F.A.I. RTE. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\planroom\dot\illinois\gov\PI\DOT\Documents\DOT\Offices\District\Projects\DI\8319\CADD\Design\DI\8319-sh-50\DRAWN	CHECKED -	REVISED -	57			2019-056-BR	COOK	31	3	
PLOT SCALE = 100,0000' / 1in.	DATE -	REVISED -	CONTRACT NO. 62J33							
PLOT DATE = 10/18/2019			SCALE:			SHEET NO. 1 OF 2 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT		

\*= SPECIALTY ITEM

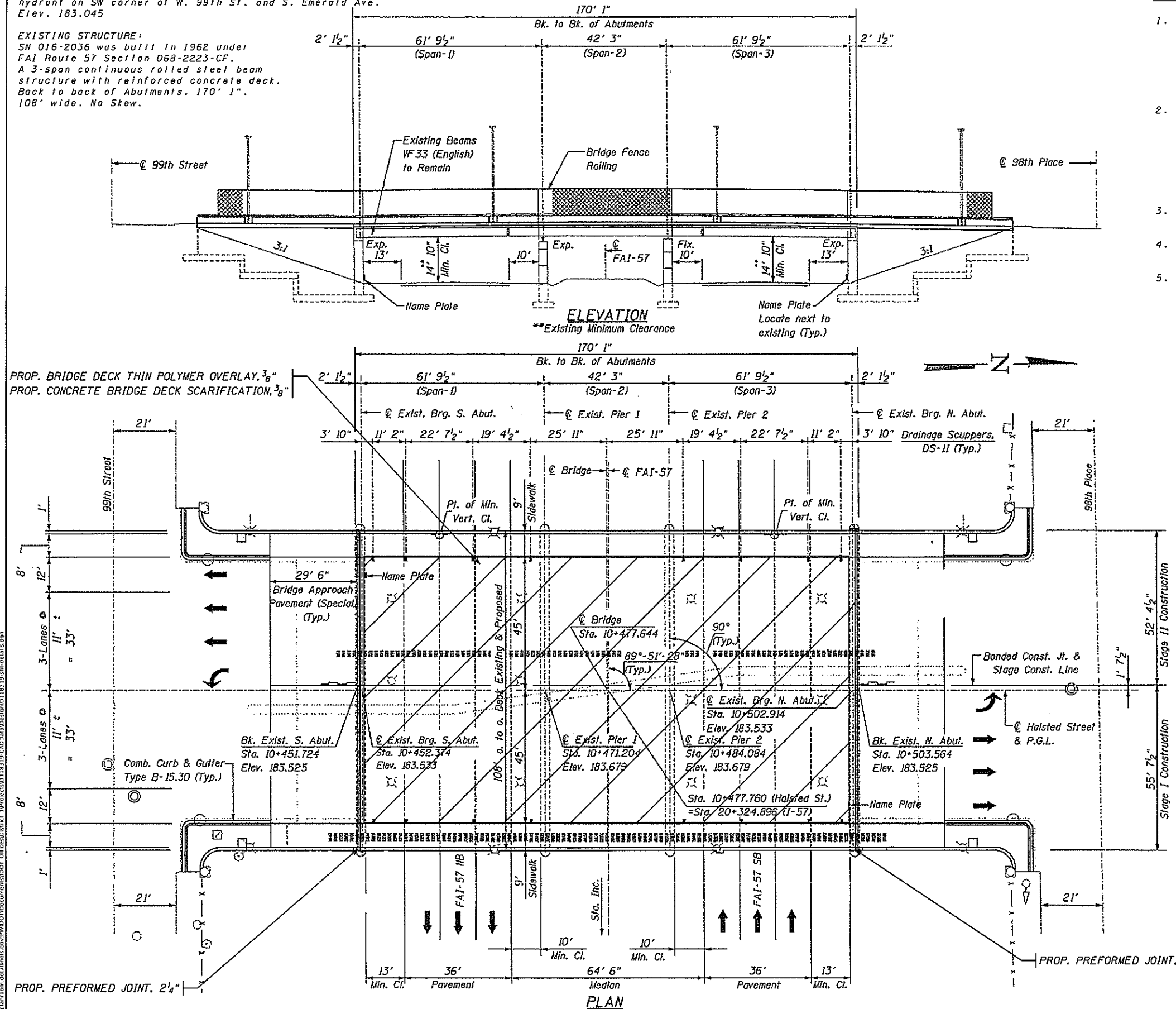
\*= SPECIALTY ITEMS

REV. 12/10/19 REV. 12/04/19 REV. 11/27/19



BENCH MARK: "•" top of NW flange bolt of fire hydrant on SW corner of W. 99th St. and S. Emerald Ave. Elev. 183.045

EXISTING STRUCTURE:  
 SN 016-2036 was built in 1962 under  
 FAI Route 57 Section 068-2223-CF.  
 A 3-span continuous rolled steel beam  
 structure with reinforced concrete deck.  
 Back to back of Abutments, 170' 1".  
 108' wide. No Skew.

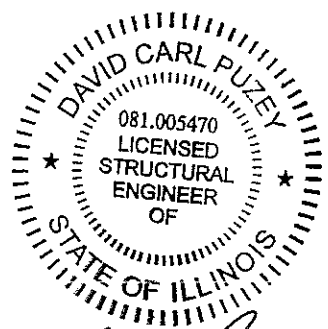


**GENERAL NOTES:**

1. PRIOR TO THE POURING OF NEW CONCRETE, ALL HEAVY OR LOOSE RUST, LOOSE MILL SCALE, AND OTHER LOOSE OR POTENTIALLY DETRIMENTAL FOREIGN MATERIAL SHALL BE REMOVED FROM THE SURFACES IN CONTACT WITH CONCRETE. TIGHTLY ADHERED PAINT MAY REMAIN UNLESS OTHERWISE NOTED. REMOVAL SHALL BE ACCOMPLISHED BY METHODS THAT WILL NOT DAMAGE THE STEEL AND THE COST WILL BE INCLUDED IN THE PAY ITEM COVERING REMOVAL OF THE EXISTING CONCRETE.
2. PLAN DIMENSIONS AND DETAIL RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIABLES. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF THE WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.
3. CONCRETE SEALER TO BE APPLIED ON TOP OF SIDEWALKS AND TOP AND FRONT FACE OF PARAPETS
4. REPAIR AREAS ARE ESTIMATED. THE ENGINEER SHALL DETERMINE ACTUAL REPAIR LOCATIONS AND RECORD THEM ON THE AS-BUILT PLANS.
5. JOINT OPENINGS SHALL BE ADJUSTED ACCORDING TO ARTICLE 520.04 OF THE STANDARD SPECIFICATIONS WHEN THE DECK IS POURED AT AN AMBIENT TEMPERATURE OTHER THAN 50° F

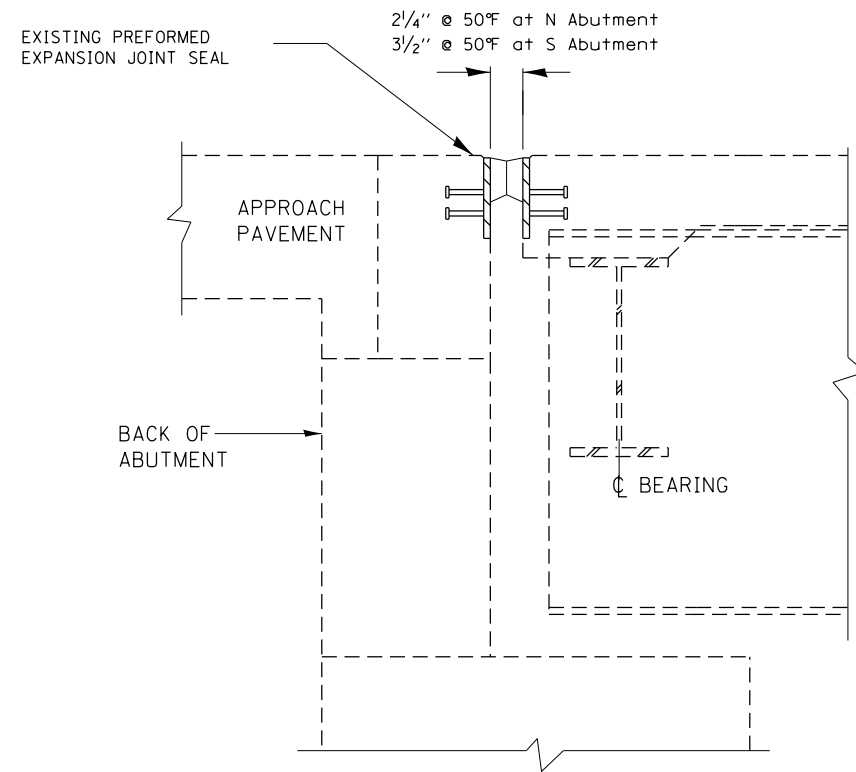
TOTAL BILL OF MATERIALS:		
PREFORMED JOINT SEAL 2 1/4"	FOOT	108.5
PREFORMED JOINT SEAL 3 1/2"	FOOT	108.5
BRIDGE DECK CONCRETE SEALER	SO FT	3740
CONCRETE BRIDGE DECK SCARIFICATION 3/8" INCH	SO YD	1701
BRIDGE DECK THIN POLYMER OVERLAY 3/8"	SO YD	1701
STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SO FT	1218
STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)	SO FT	12
TEMPORARY SHORING AND CRIBBING	EACH	4

INTERIOR GIRDER REACTION TABLE		
	ABUTMENTS	PIERS
RDL (Kip)	29.5	64.1
RLL (Kip)	33.3	38.4
Imp (Kip)	9.0	8.3
R (Total) (Kip)	71.7	110.8

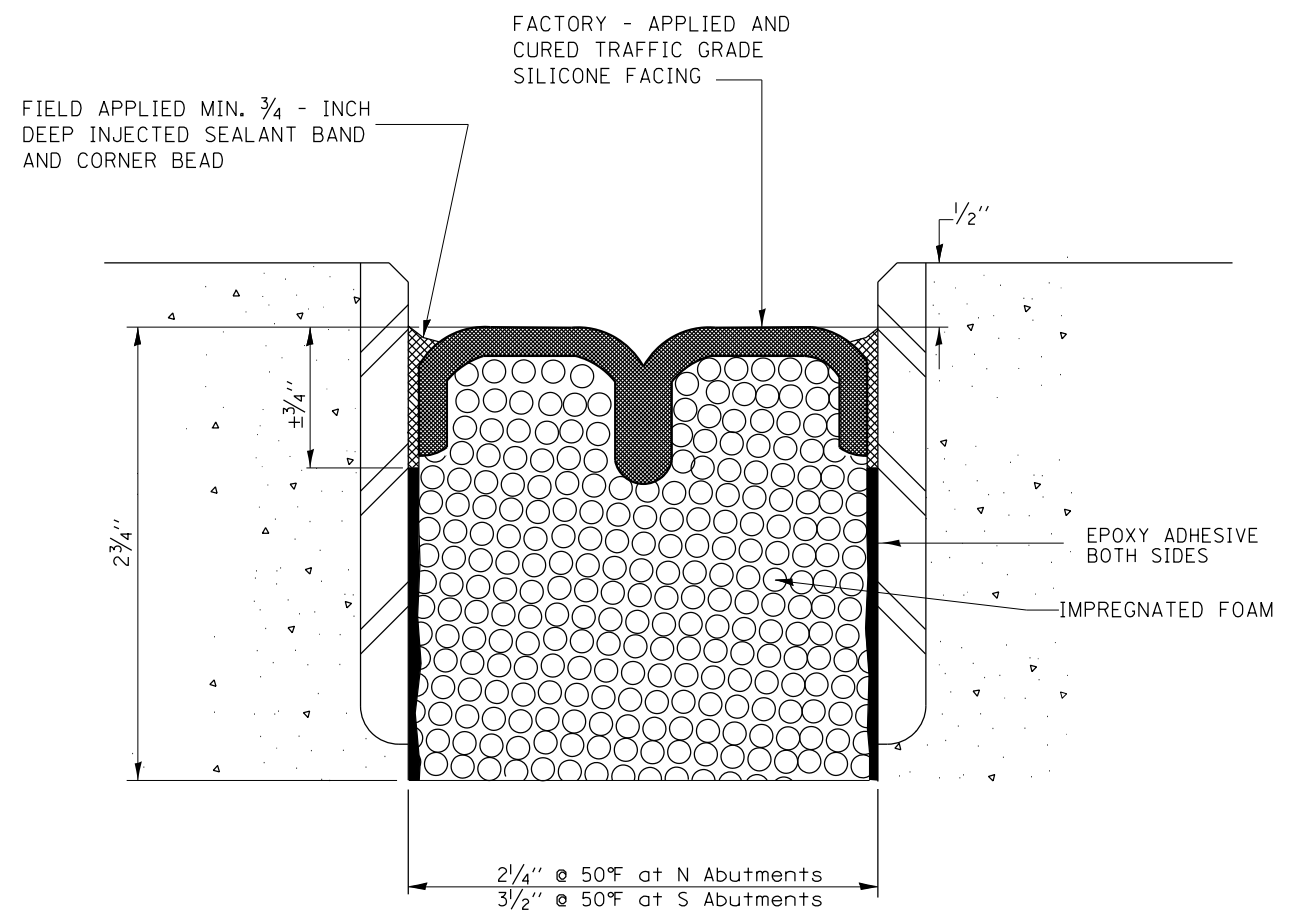


*Dr. Carl Puzey* 12/4/19  
 Expires 11/30/20

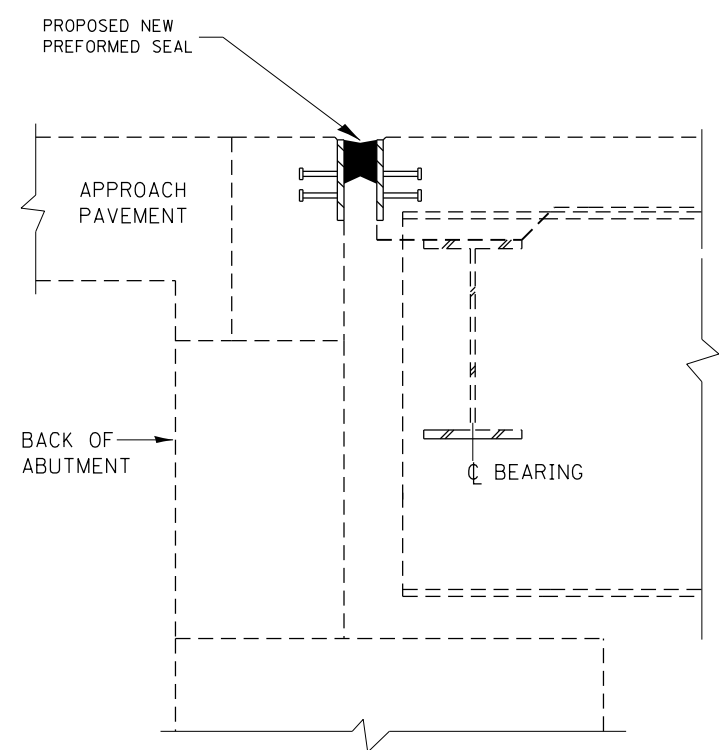
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 DATE: 12/3/2019



**EXISTING PREFORMED JOINT  
AT NORTH AND SOUTH ABUTMENTS**



**PREFORMED JOINT SEAL  
AT NORTH AND SOUTH ABUTMENTS**



**PROPOSED NEW PREFORMED JOINT SEAL  
AT NORTH AND SOUTH ABUTMENTS**

*BILL OF MATERIALS*

	Item	Unit	Quantity
N	Preformed Joint Seal 2 1/4"	Foot	108.5
S	Preformed Joint Seal 3 1/2"	Foot	108.5

**NOTES:**

1. JOINT SEALS SHALL BE PREFORMED PRE-COMPRESSED, SILICONE COATED SELF-EXPANDING SEALANT SYSTEM.
2. THE REMOVAL OF THE EXISTING JOINT IS INCLUDED IN THE COST OF THE PREFORMED JOINT SEAL

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	DATE -	REVISED -

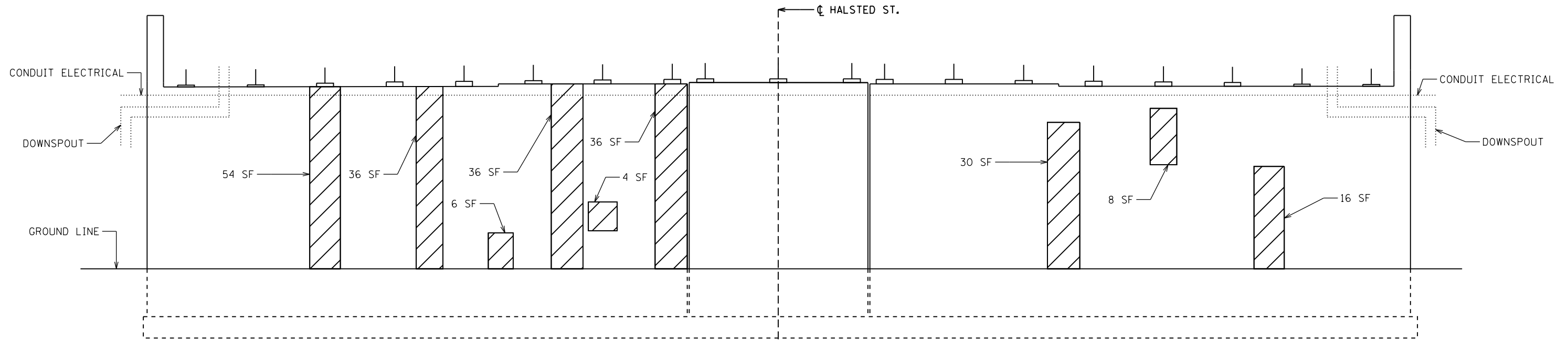
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>BRIDGE REPAIR DETAILS I-57 AT IL ROUTE 1 (HALSTED ST.)</b>		F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		57	2019-056-BR	COOK	31	6
SCALE:		SHEET 2 OF 5 SHEETS		STA.	TO STA.	

CONTRACT NO. 62J33	
ILLINOIS	FED. AID PROJECT

NOTE:  
WATCH FOR CONFLICTS WITH ELECTRICAL  
CONDUITS AND DOWNSPOUTS

**NORTH ABUTMENT CONCRETE REPAIR LOCATIONS**

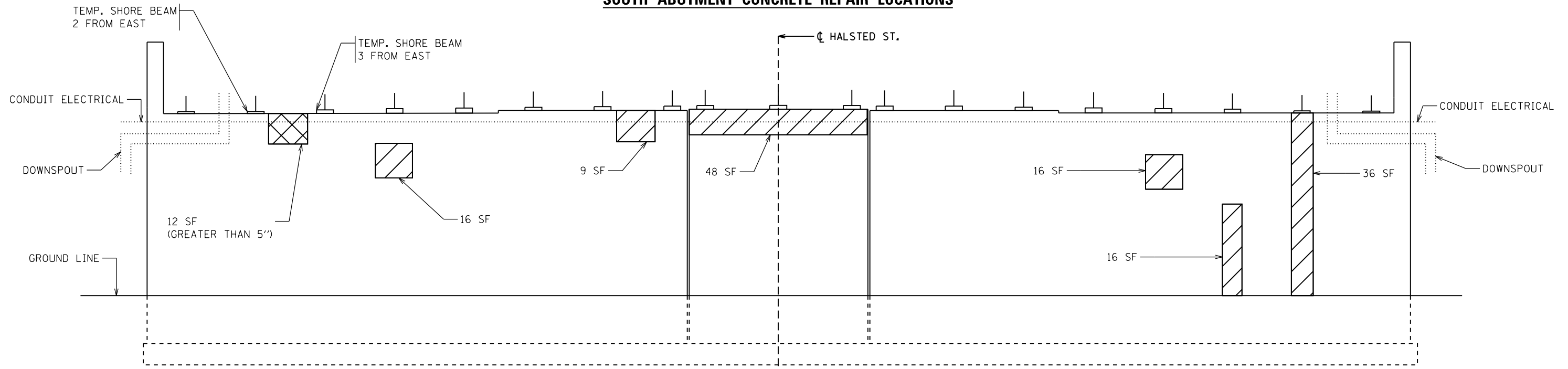


**NORTH ABUTMENT ELEVATION**  
(LOOKING NORTH)

**LEGEND**

- STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)
- STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)

**SOUTH ABUTMENT CONCRETE REPAIR LOCATIONS**



**SOUTH ABUTMENT ELEVATION**  
(LOOKING SOUTH)

**BILL OF MATERIALS**

Item	Unit	Quantity
Structural Repair of Concrete (Depth Equal to or Less than 5")	SQ. FT.	367
Structural Repair of Concrete (Depth Greater than 5")	SQ. FT.	12
Temporary Shoring and Cribbing	EACH	2

FOR REACTIONS SEE SHEET 1 OF 5

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PLOT DATE = 12/3/2019	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**ABUTMENT CONCRETE REPAIR LOCATIONS**  
**I-57 AT IL ROUTE 1 (HALSTED ST.)**

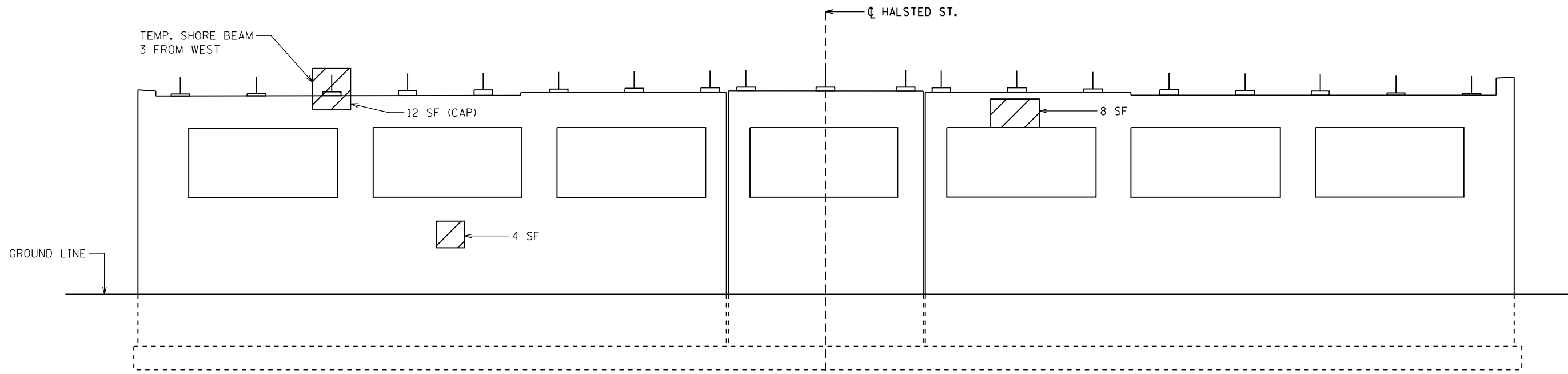
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2019-056-BR	COOK	31	7
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62J33	





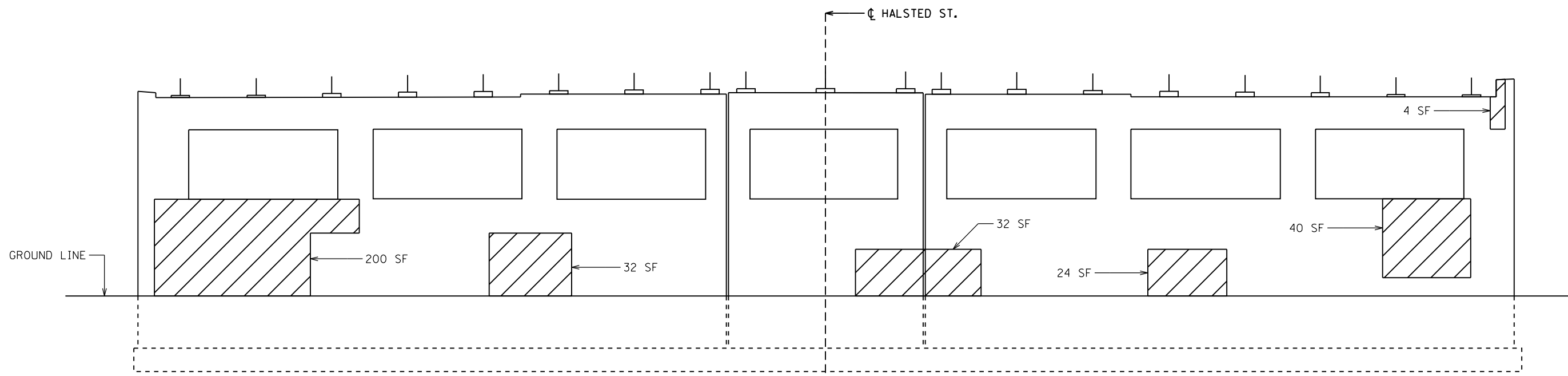
**PIER 2 CONCRETE REPAIRS**



**SOUTH ELEVATION**  
(LOOKING NORTH)

**LEGEND**

STRUCTURAL REPAIR OF CONCRETE  
(DEPTH EQUAL TO OR LESS THAN 5 INCHES)



**NORTH ELEVATION**  
(LOOKING SOUTH)

**BILL OF MATERIALS**

Item	Unit	Quantity
Structural Repair of Concrete (Depth Equal to or Less than 5")	SQ. FT.	356
Temporary Shoring and Cribbing	EACH	1

FOR REACTIONS SEE SHEET 1 OF 5

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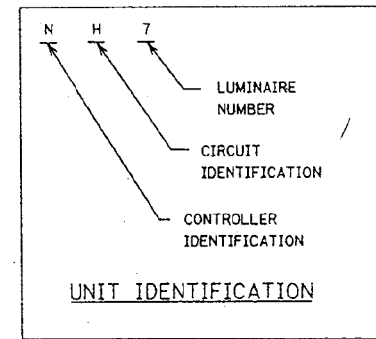
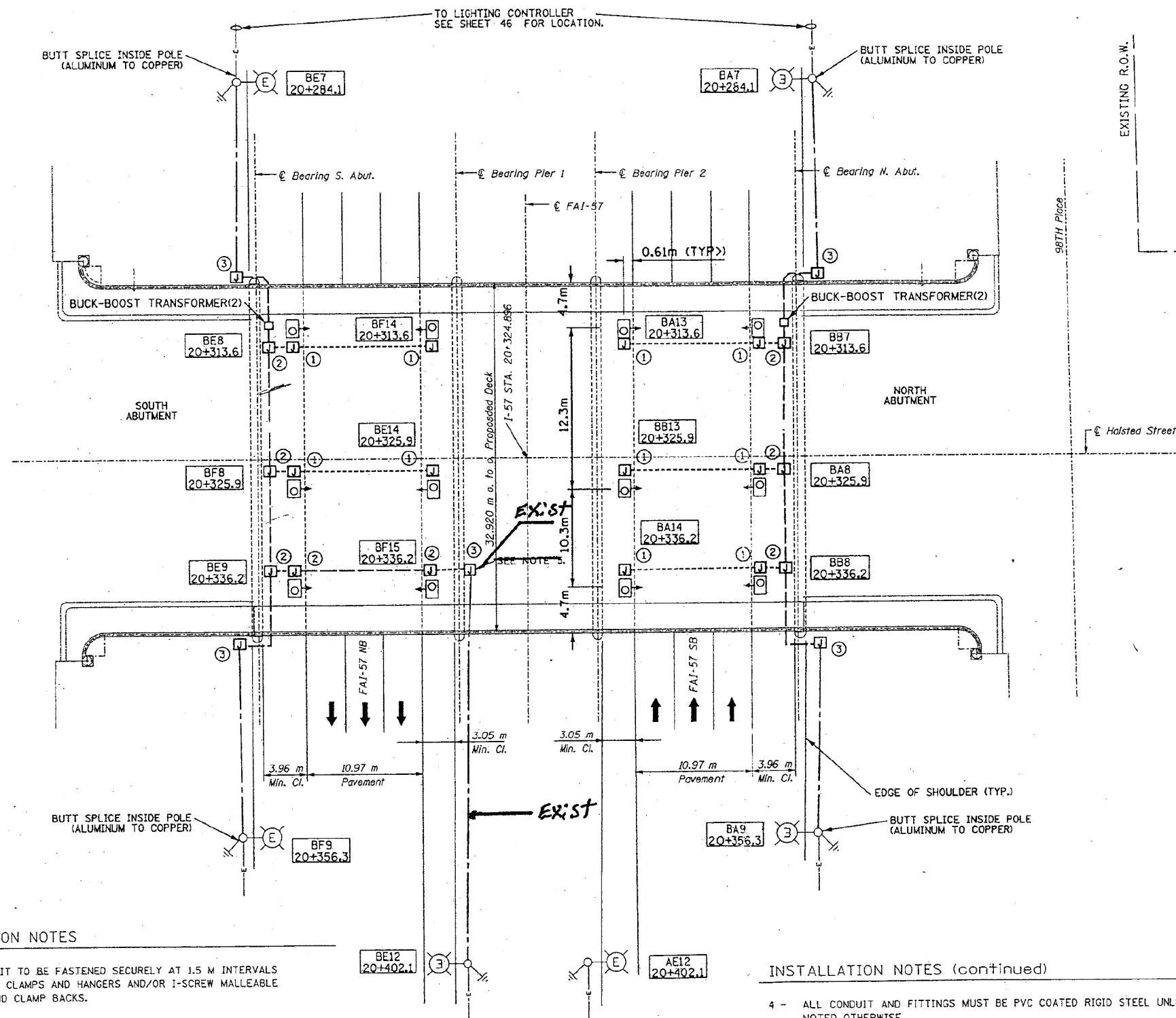
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**PIER CONCRETE REPAIR LOCATIONS**  
**I-57 AT IL ROUTE 1 (HALSTED ST.)**

SCALE: SHEET 5 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2019-056-BR	COOK	31	9
CONTRACT NO. 62J33				
ILLINOIS FED. AID PROJECT				

# FOR INFORMATION ONLY



### LEGEND:

- |  |          |  |          |
|--|----------|--|----------|
|  | EXISTING |  | PROPOSED |
|  | EXISTING |  | PROPOSED |
|  | EXISTING |  | PROPOSED |
|  | EXISTING |  | PROPOSED |
|  | EXISTING |  | PROPOSED |
|  | EXISTING |  | PROPOSED |
|  | EXISTING |  | PROPOSED |

### INSTALLATION NOTES

- ALL CONDUIT TO BE FASTENED SECURELY AT 1.5 M INTERVALS WITH BEAM CLAMPS AND HANGERS AND/OR I-SCREW MALLEABLE CLAMPS AND CLAMP BACKS.
- ONE 1/C #12 AWG COPPER CONDUCTOR WITH GREEN INSULATION SHALL BE CONNECTED TO EACH FIXTURE HOUSING FOR GROUNDING PURPOSES.
- CONNECT NEW UNIT DUCT THROUGH EXISTING ELBOW IN BASE OF POLE. SEE SHEET 47 FOR POLE WIRING DETAIL. THIS WORK IS INCIDENTAL TO TRENCH AND BACKFILL.

### INSTALLATION NOTES (continued)

- ALL CONDUIT AND FITTINGS MUST BE PVC COATED RIGID STEEL UNLESS NOTED OTHERWISE.
- ~~INSTALL 2" CONDUIT ON THE NON TRAFFIC SIDE OF THE PIER FROM JUNCTION BOX TO 30" BELOW GRADE. INSTALL NEW UNIT DUCT TO POLE BE12.~~
- UNDERGROUND SPLICES ARE NOT ALLOWED.

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION F.A.I. RTE. 57 AT IL. RTE. 1 (HALSTED ST.)
NAME	DATE	
		<b>UNDERPASS LIGHTING PLAN</b>  SCALE: 1:200 DATE: DEC 2003 DRAWN BY: JMV CHECKED BY: BLL

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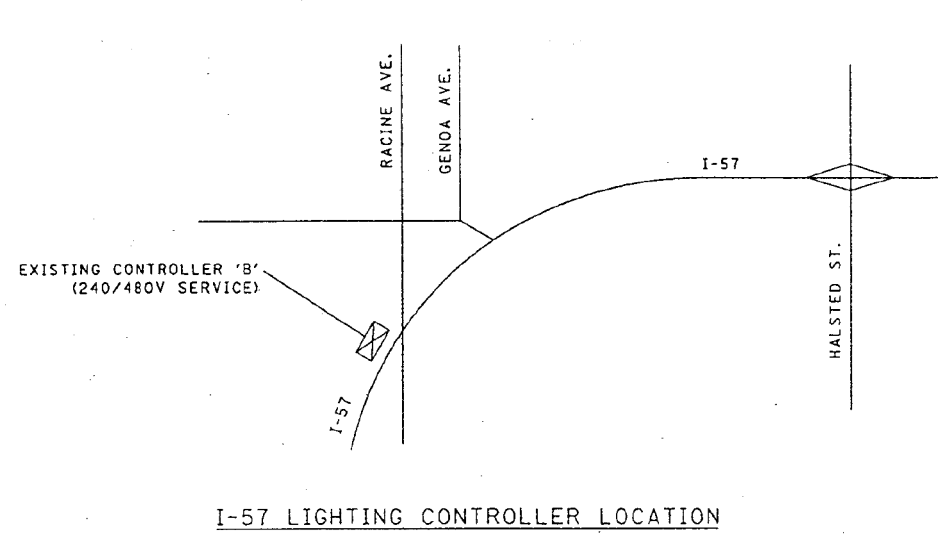
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PLOT SCALE = 100,0000' / in.	CHECKED - A. MIDY	REVISED -
PLOT DATE = 10/16/2019	DATE - 9/18/2019	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

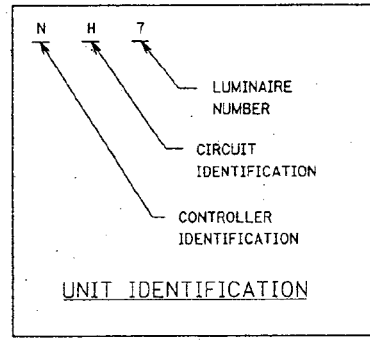
**BRIDGE LIGHTING DETAILS - FOR INFORMATION ONLY**  
**I-57 AT IL ROUTE 1 (HALSTED ST.)**  
 SCALE: SHEET 1 OF 4 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2019-056-BR	COOK	31	10
CONTRACT NO. 62J33				
ILLINOIS FED. AID PROJECT				

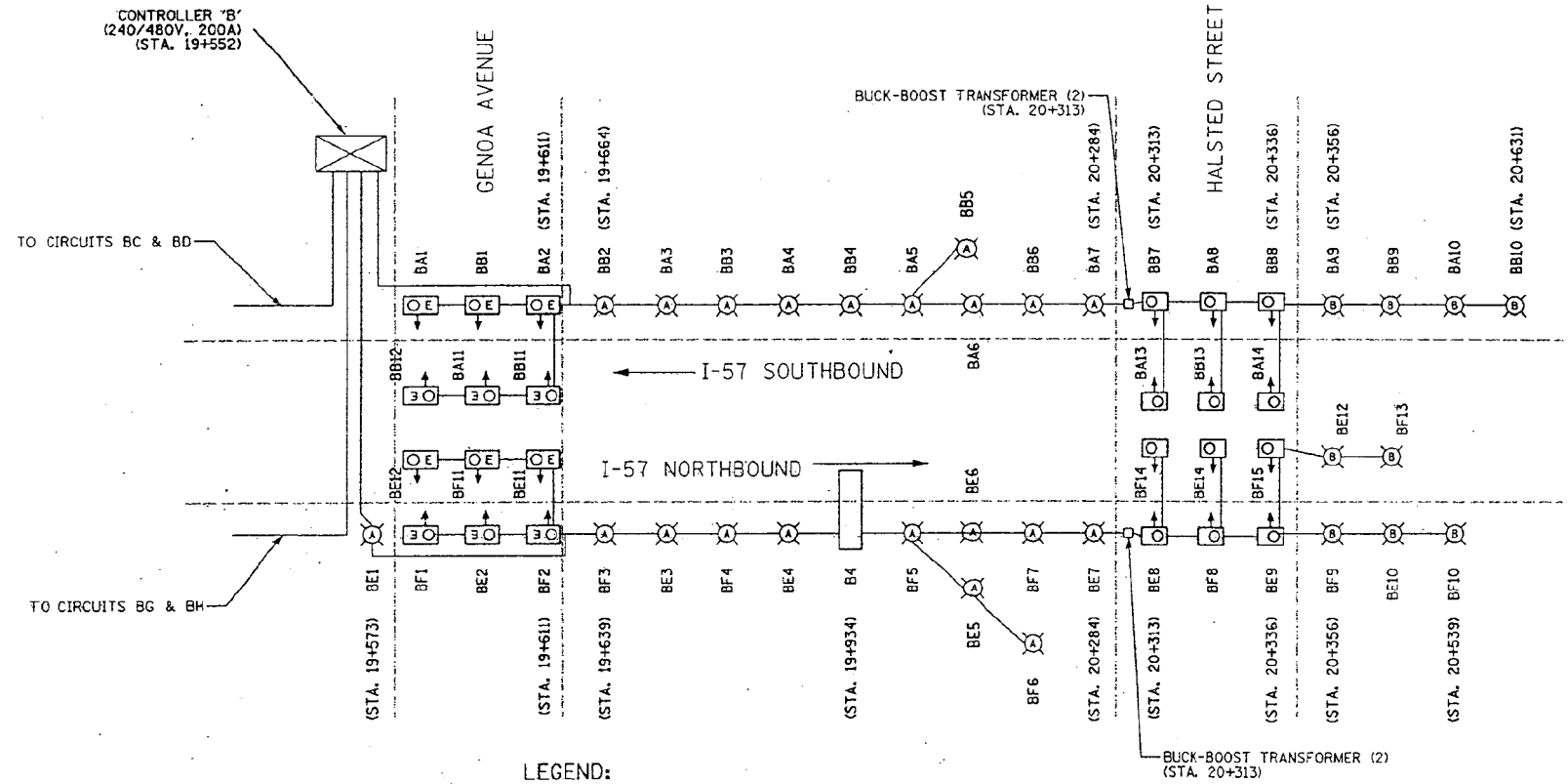
# FOR INFORMATION ONLY



I-57 LIGHTING CONTROLLER LOCATION



Ckt #	Description	Phase A (Red) (amps)	Phase B (Black) (amps)
A	5-400W, 2-200W, 3-55W, 3-70W	14.4	
B	5-400W, 2-200W, 3-55W, 3-70W		14.4
C	8-400W, 3-55W	17.2	
D	8-400W, 3-55W		17.2
E	6-400W, 2-200W, 3-55W, 4-85W, 3-70W	18.4	
F	5-400W, 3-200W, 3-55W, 2-85W, 3-70W		16.4
G	7-400W, 3-55W, 2-85W	16.2	
H	8-400W, 3-55W, 2-85W		18.2
Totals:		66.2	66.2



LEGEND:

- 400W HPS LUMINAIRE ON 47.5' POLE (TYPE A)
- 200W HPS LUMINAIRE ON 35' POLE (TYPE B)
- 55W LPS UNDERPASS LUMINAIRE
- 70W HPS UNDERPASS LUMINAIRE
- LIGHTED SIGN STRUCTURE -  
B2 = 2 FIXTURES (4 - 85W FLUORESCENT)  
B4 = 3 FIXTURES (6 - 85W FLUORESCENT)

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION F.A.I. RTE. 57 AT IL. RTE. 1 (HALSTED ST.)
NAME	DATE	
		<b>UNDERPASS LIGHTING ONE LINE DIAGRAM</b>  SCALE: NONE                      DRAWN BY: JMV DATE: DEC 2003                      CHECKED BY: BLL

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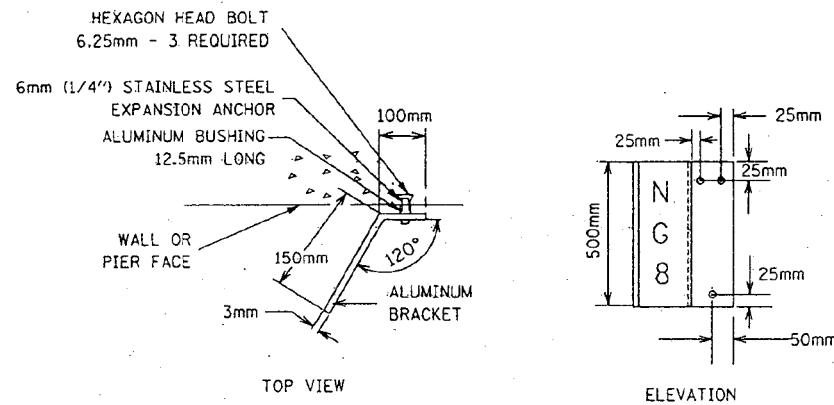
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PLOT DATE = 10/16/2019	CHECKED - A. MIDY	REVISED -
	DATE - 9/18/2019	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

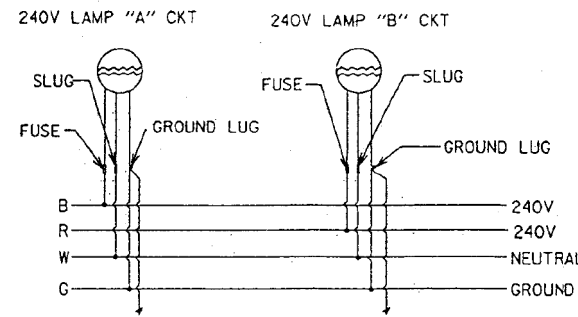
**BRIDGE LIGHTING DETAILS - FOR INFORMATION ONLY**  
**I-57 AT IL ROUTE 1 (HALSTED ST.)**

F.A.I. RTE. 57	SECTION 2019-056-BR	COUNTY COOK	TOTAL SHEETS 31	SHEET NO. 11
CONTRACT NO. 62J33				
SCALE: SHEET 2 OF 4 SHEETS		ILLINOIS FED. AID PROJECT		

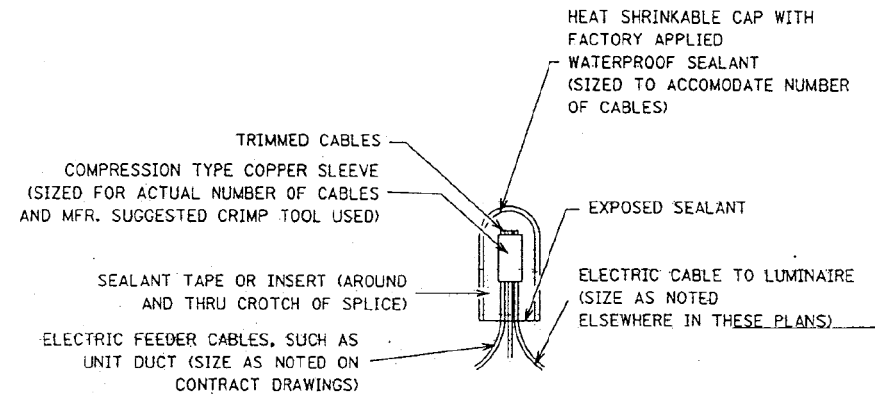
# FOR INFORMATION ONLY



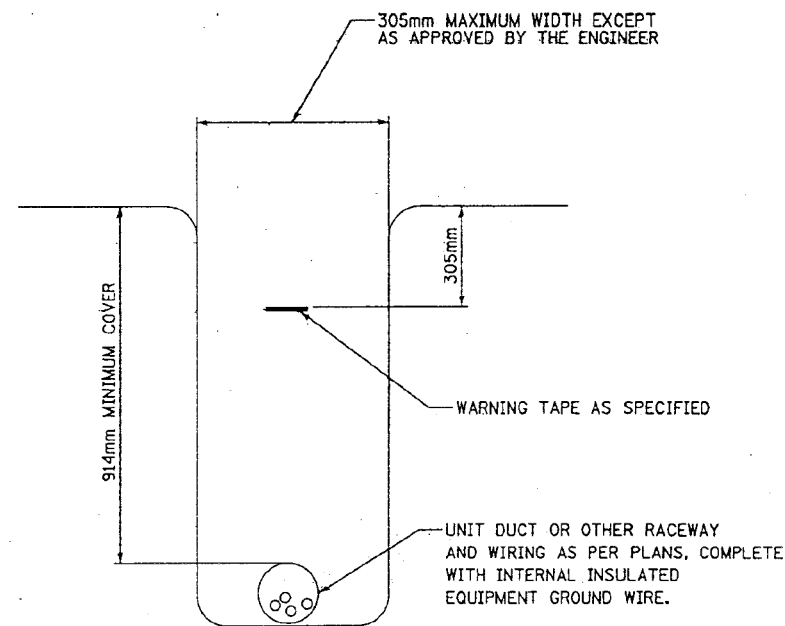
LUMINAIRE NUMBERING  
DECAL BRACKET-TYPICAL



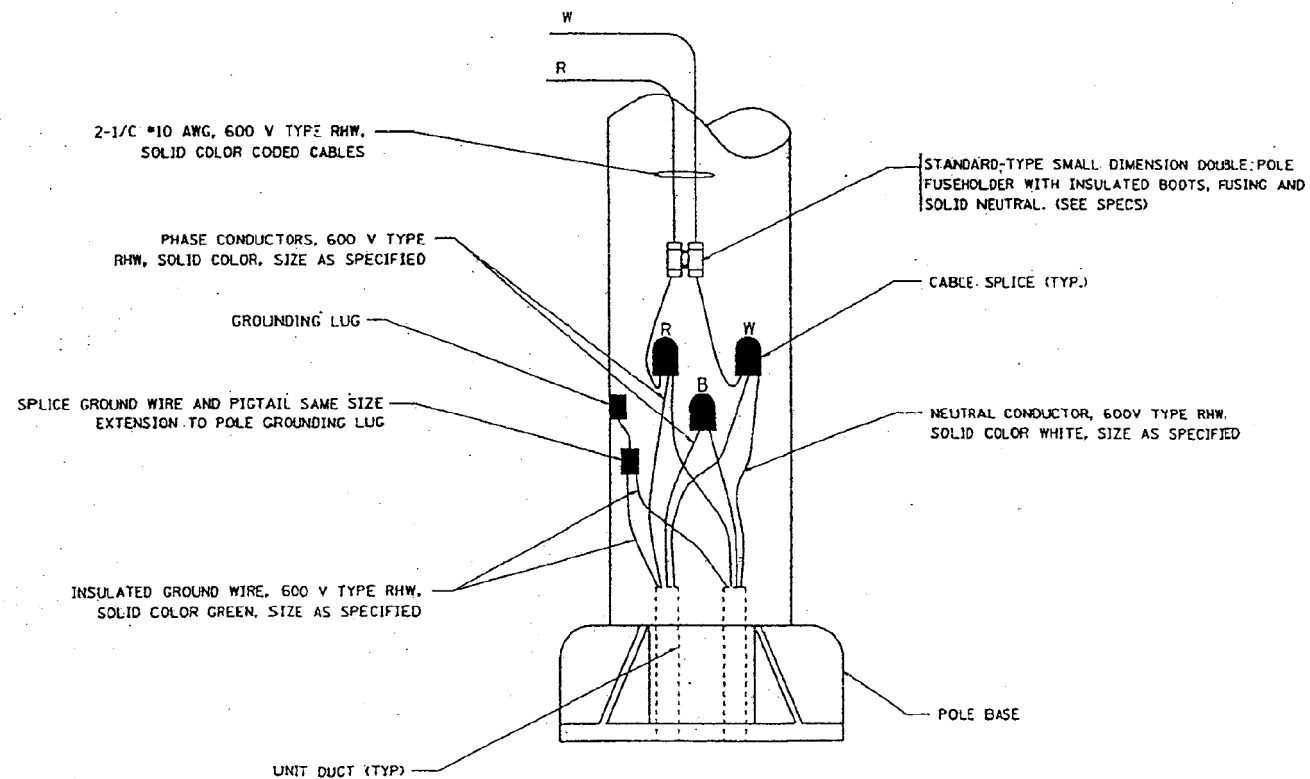
TYPICAL WIRING DIAGRAM



ELECTRIC CABLE SPLICE DETAIL



TYPICAL WIRING IN TRENCH DETAIL  
N.T.S.



POLE WIRING DETAIL  
N.T.S.

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION F.A.I. RTE. 57 AT IL. RTE. 1 (HALSTED ST.)
NAME	DATE	
		UNDERPASS LIGHTING DETAILS
SCALE: NONE		DRAWN BY: JMV
DATE: DEC 2003		CHECKED BY: BLL

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	DATE - 9/18/2019	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

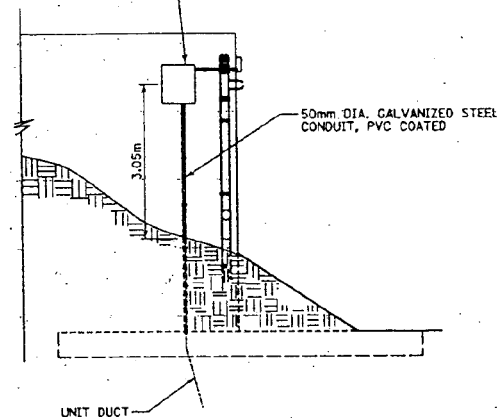
BRIDGE LIGHTING DETAILS - FOR INFORMATION ONLY  
I-57 AT IL ROUTE 1 (HALSTED ST.)

SCALE: SHEET 3 OF 4 SHEETS STA. TO STA.

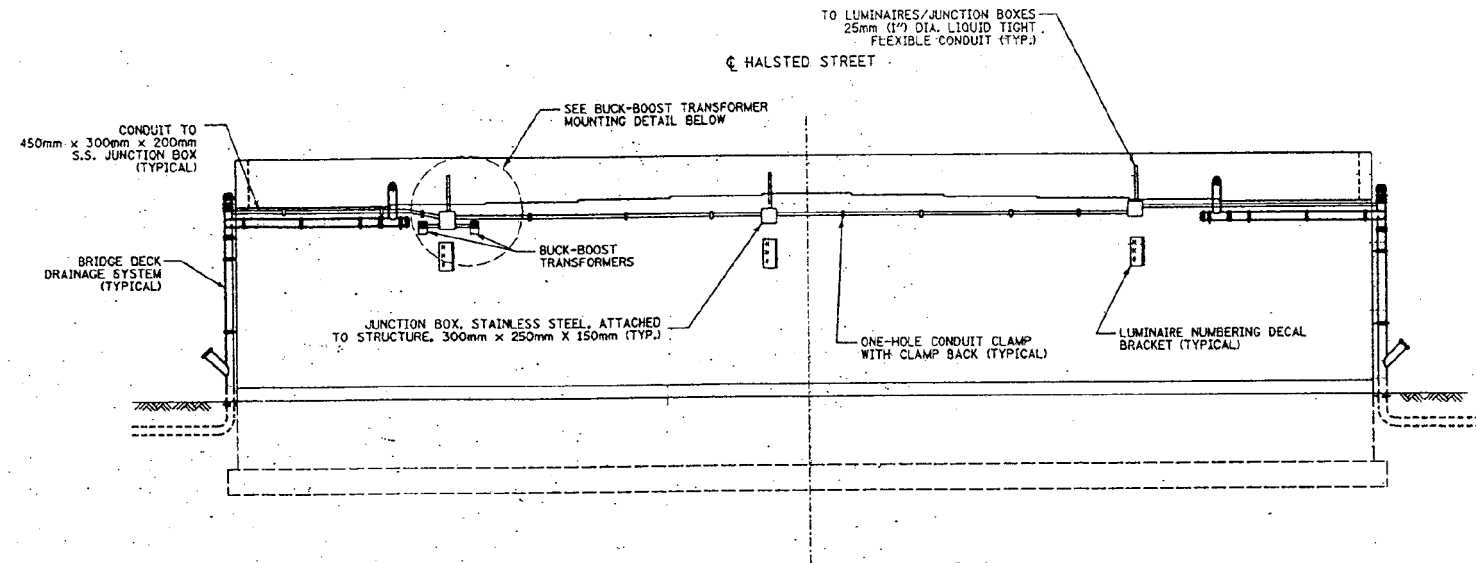
F.A.I. RTE. 57	SECTION 2019-056-BR	COUNTY COOK	TOTAL SHEETS 31	SHEET NO. 12
CONTRACT NO. 62J33				
ILLINOIS FED. AID PROJECT				

# FOR INFORMATION ONLY

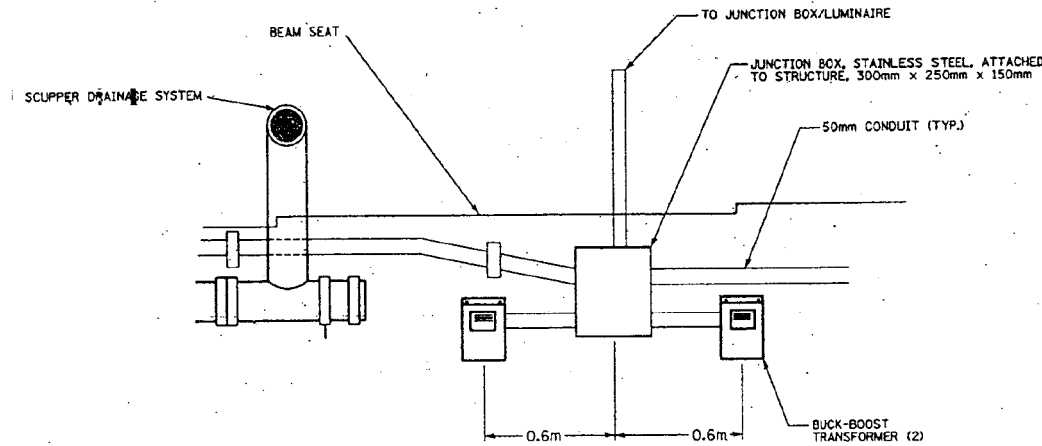
450mm x 300mm x 200mm S.S. JUNCTION BOX ATTACHED TO STRUCTURE. PROVIDE DOUBLE POLE 30 AMP FUSE FOR PHASE CONDUCTORS & SOLID LUG FOR NEUTRAL CONDUCTOR.



**SIDE VIEW OF ABUTMENT**  
(NORTH ABUTMENT, LOOKING EAST)  
(TYPICAL ALL QUADRANTS)



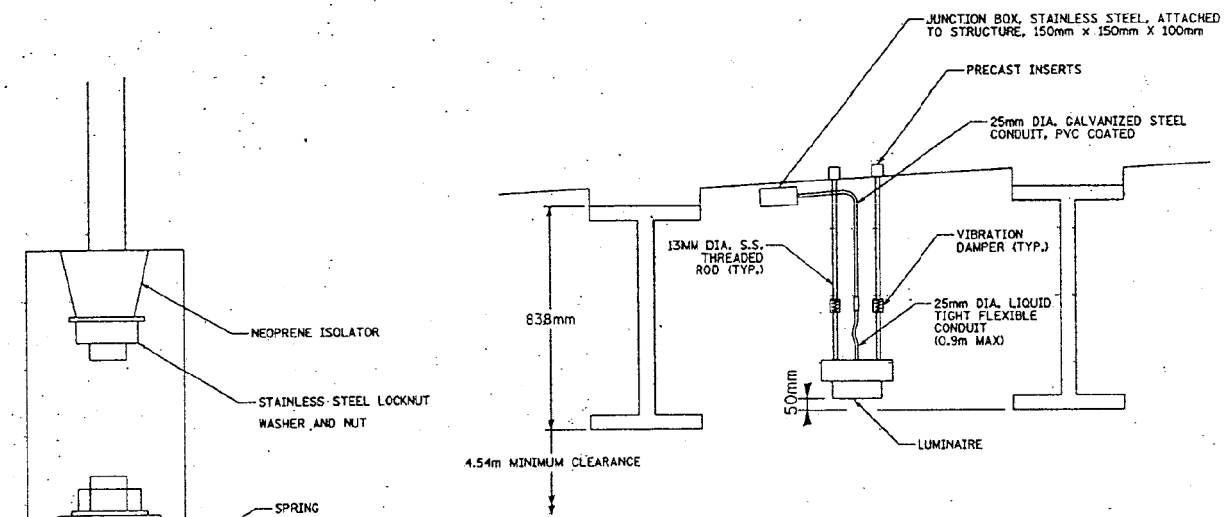
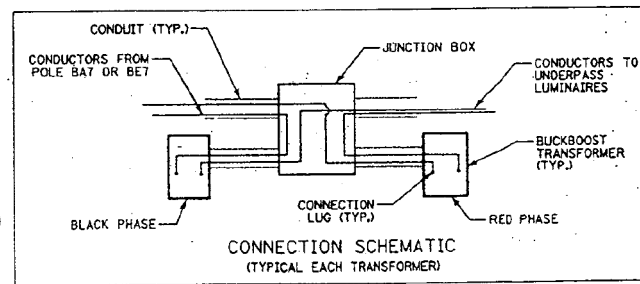
**ABUTMENT WALL CONDUIT ROUTING DETAIL**  
(NORTH ABUTMENT, LOOKING NORTH)  
(TYPICAL BOTH ABUTMENTS)



**BUCK-BOOST TRANSFORMER MOUNTING DETAIL**

**NOTES:**

- BUCK-BOOST TRANSFORMER SHALL BE MOUNTED TO THE ABUTMENT WALL USING DRILLED CONCRETE ANCHORS AND STAINLESS STEEL HARDWARE PER THE MANUFACTURER'S RECOMMENDATION.
- CONDUCTORS FROM POLE BAT (OR BET) SHALL BE CONNECTED TO THE BUCK-BOOST TRANSFORMER BEFORE BEING CONNECTED TO THE UNDERPASS LUMINAIRES. (SEE CONNECTION SCHEMATIC)



**INSTALLATION NOTES:**

- CONTRACTOR SHALL COORDINATE ELECTRICAL INSTALLATION WITH BRIDGE CONTRACTOR.
- LUMINAIRE SHALL BE CENTERED BETWEEN BEAMS.
- LUMINAIRE SHALL BE INSTALLED 0.61m FROM EDGE OF PAVEMENT.
- INSERTS INSTALLED AS PART OF CONCRETE SUPERSTRUCTURE.

**UNDERPASS LUMINAIRE MOUNTING DETAIL**

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. RTE. 57 AT IL. RTE. 1 (HALSTED ST.)

**UNDERPASS LIGHTING DETAILS**

SCALE: NONE                          DRAWN BY: JMY  
DATE: DEC 2003                      CHECKED BY: BLL

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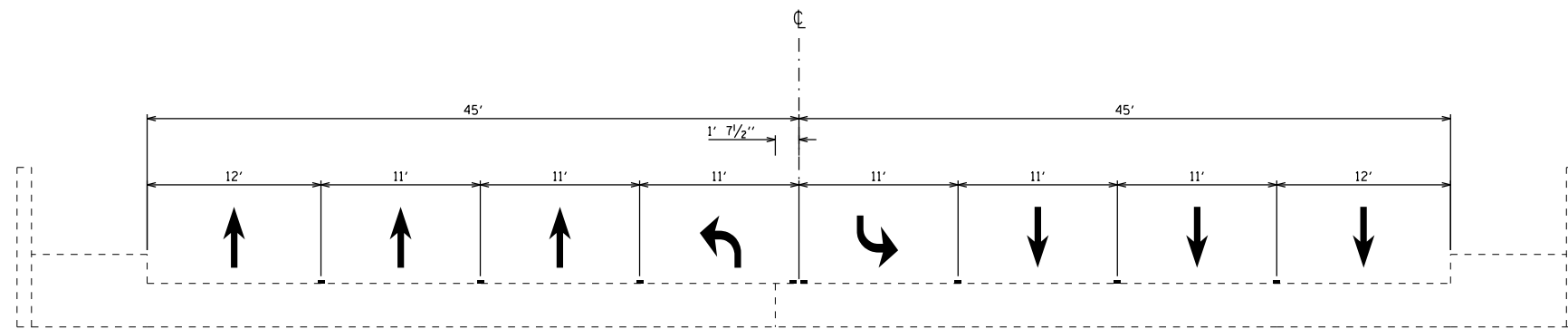
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PLOT DATE = 10/16/2019	DATE - 9/18/2019	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

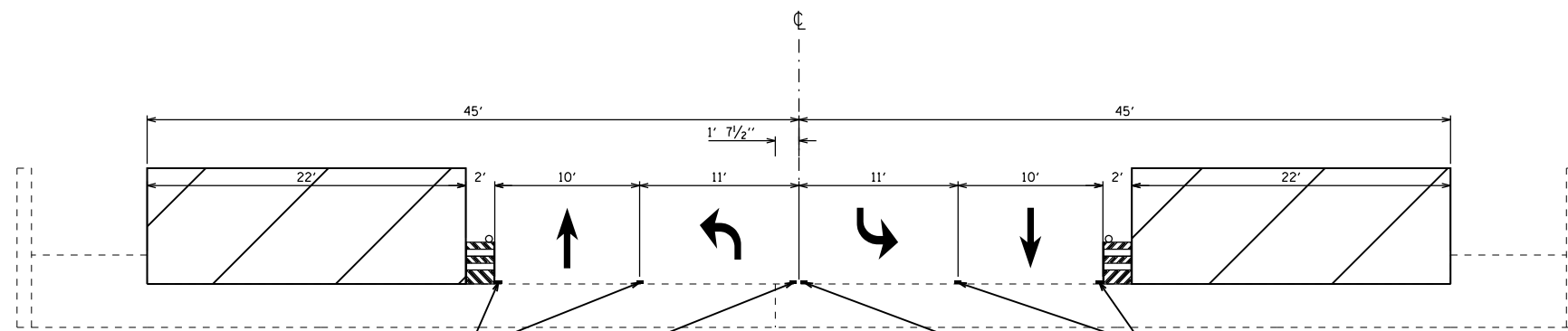
BRIDGE LIGHTING DETAILS - FOR INFORMATION ONLY  
I-57 AT IL ROUTE 1 (HALSTED ST.)

SCALE:	SHEET 4 OF 4 SHEETS	STA. TO STA.
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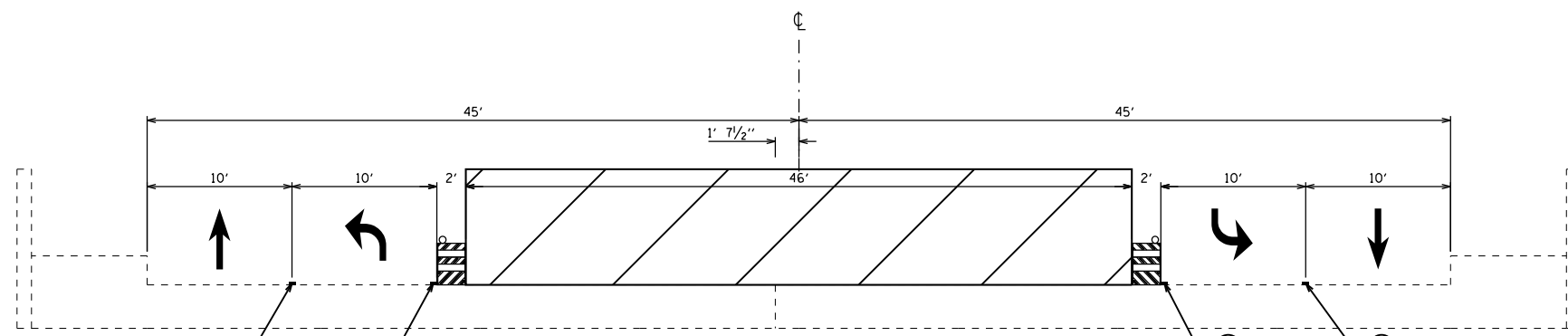
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2019-056-BR	COOK	31	13
CONTRACT NO. 62J33				
ILLINOIS FED. AID PROJECT				



**TYPICAL STRUCTURE SECTION**  
(LOOKING NORTH)



**STAGE 1 CONSTRUCTION**  
(LOOKING NORTH)



**STAGE 2 CONSTRUCTION**  
(LOOKING NORTH)

- ① TEMP. PAVEMENT MARKING TAPE, TYPE IV (WHITE)
- ② TEMP. PAVEMENT MARKING TAPE, TYPE IV (YELLOW)

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**







**MAINTENANCE OF TRAFFIC TYPICAL CROSS SECTIONS (IL RTE. 1)  
I-57 AT IL ROUTE 1 (HALSTED ST.)**

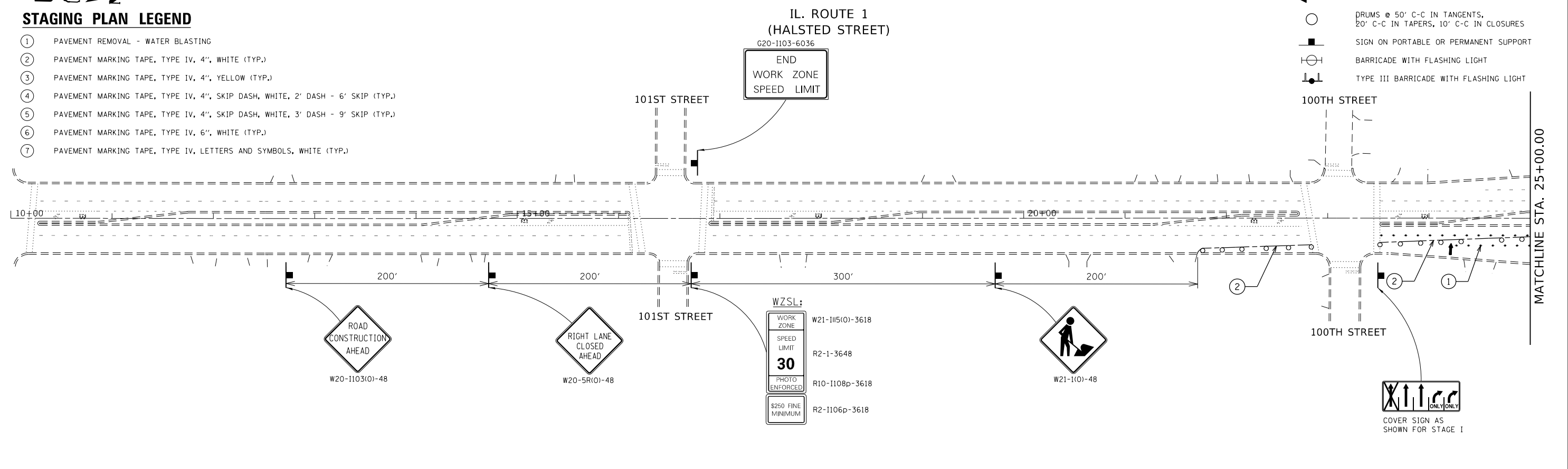
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2019-056-BR	COOK	31	14
CONTRACT NO. 62J33				
ILLINOIS FED. AID PROJECT				

### STAGING PLAN LEGEND







- ① PAVEMENT REMOVAL - WATER BLASTING
- ② PAVEMENT MARKING TAPE, TYPE IV, 4", WHITE (TYP.)
- ③ PAVEMENT MARKING TAPE, TYPE IV, 4", YELLOW (TYP.)
- ④ PAVEMENT MARKING TAPE, TYPE IV, 4", SKIP DASH, WHITE, 2' DASH - 6' SKIP (TYP.)
- ⑤ PAVEMENT MARKING TAPE, TYPE IV, 4", SKIP DASH, WHITE, 3' DASH - 9' SKIP (TYP.)
- ⑥ PAVEMENT MARKING TAPE, TYPE IV, 6", WHITE (TYP.)
- ⑦ PAVEMENT MARKING TAPE, TYPE IV, LETTERS AND SYMBOLS, WHITE (TYP.)

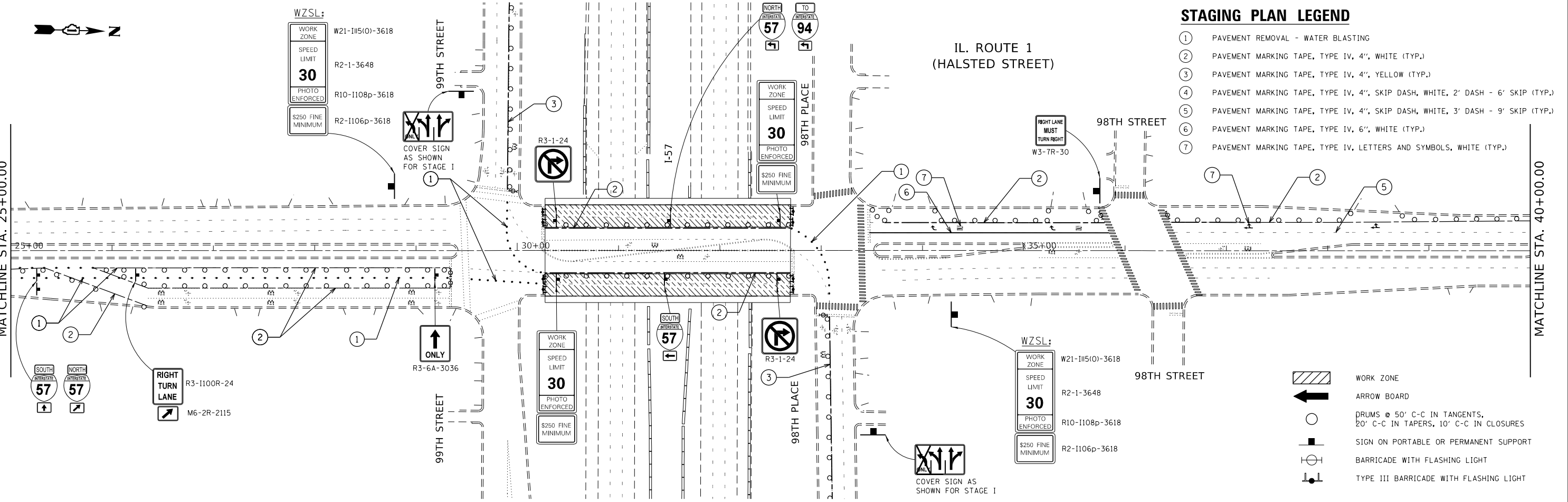
-  WORK ZONE
-  ARROW BOARD
-  DRUMS @ 50' C-C IN TANGENTS, 20' C-C IN TAPERS, 10' C-C IN CLOSURES
-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  BARRICADE WITH FLASHING LIGHT
-  TYPE III BARRICADE WITH FLASHING LIGHT



### STAGING PLAN LEGEND

- ① PAVEMENT REMOVAL - WATER BLASTING
- ② PAVEMENT MARKING TAPE, TYPE IV, 4", WHITE (TYP.)
- ③ PAVEMENT MARKING TAPE, TYPE IV, 4", YELLOW (TYP.)
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- ⑤ PAVEMENT MARKING TAPE, TYPE IV, 4", SKIP DASH, WHITE, 3' DASH - 9' SKIP (TYP.)
- ⑥ PAVEMENT MARKING TAPE, TYPE IV, 6", WHITE (TYP.)
- ⑦ PAVEMENT MARKING TAPE, TYPE IV, LETTERS AND SYMBOLS, WHITE (TYP.)

-  WORK ZONE
-  ARROW BOARD
-  DRUMS @ 50' C-C IN TANGENTS, 20' C-C IN TAPERS, 10' C-C IN CLOSURES
-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  BARRICADE WITH FLASHING LIGHT
-  TYPE III BARRICADE WITH FLASHING LIGHT



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PLOT DATE = 10/25/2019	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>MAINTENANCE OF TRAFFIC (STAGE 1) I-57 AT IL ROUTE 1 (HALSTED ST.)</b>	
SCALE: 1" = 50'	STATION: STA. TO STA.
SHEET 1 OF 2 SHEETS	

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2019-056-BR	COOK	31	15
CONTRACT NO. 62J33				
ILLINOIS FED. AID PROJECT				



IL. ROUTE 1  
(HALSTED STREET)

WZSL:

WORK ZONE	W21-1115(0)-3618
SPEED LIMIT	R2-1-3648
PHOTO ENFORCED	R10-1108p-3618
\$250 FINE MINIMUM	R2-1106p-3618

96TH STREET

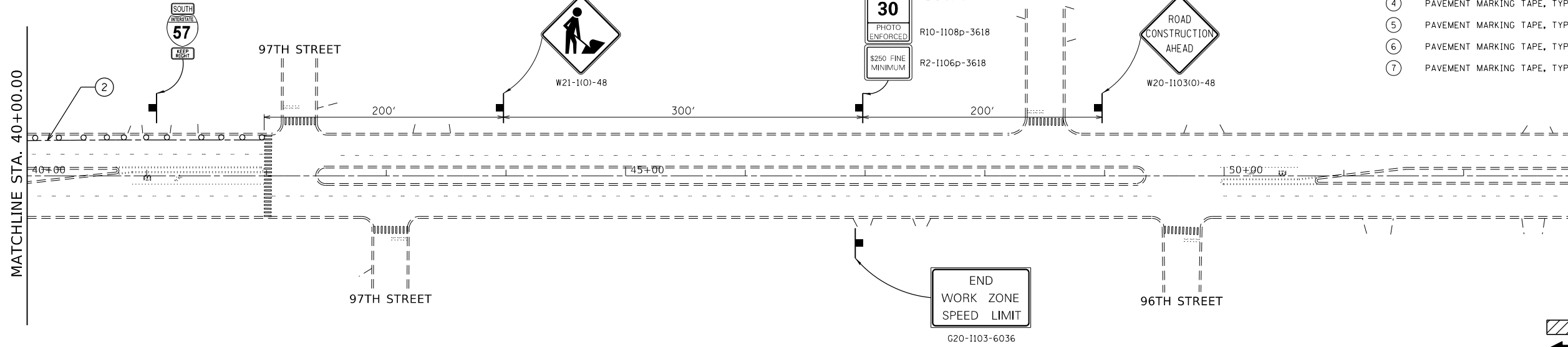


W20-1103(0)-48

**STAGING PLAN LEGEND**

- ① PAVEMENT REMOVAL - WATER BLASTING
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- ④ PAVEMENT MARKING TAPE, TYPE IV, 4", SKIP DASH, WHITE, 2' DASH - 6' SKIP (TYP.)
- ⑤ PAVEMENT MARKING TAPE, TYPE IV, 4", SKIP DASH, WHITE, 3' DASH - 9' SKIP (TYP.)
- ⑥ PAVEMENT MARKING TAPE, TYPE IV, 6", WHITE (TYP.)
- ⑦ PAVEMENT MARKING TAPE, TYPE IV, LETTERS AND SYMBOLS, WHITE (TYP.)

MATCHLINE STA. 40+00.00



- WORK ZONE
- ARROW BOARD
- DRUMS @ 50' C-C IN TANGENTS, 20' C-C IN TAPERS, 10' C-C IN CLOSURES
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- BARRICADE WITH FLASHING LIGHT
- TYPE III BARRICADE WITH FLASHING LIGHT

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PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/25/2019	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC (STAGE 1)  
I-57 AT IL ROUTE 1 (HALSTED ST.)

SCALE: 1" = 50' SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2019-056-BR	COOK	31	16
CONTRACT NO. 62J33				
ILLINOIS FED. AID PROJECT				



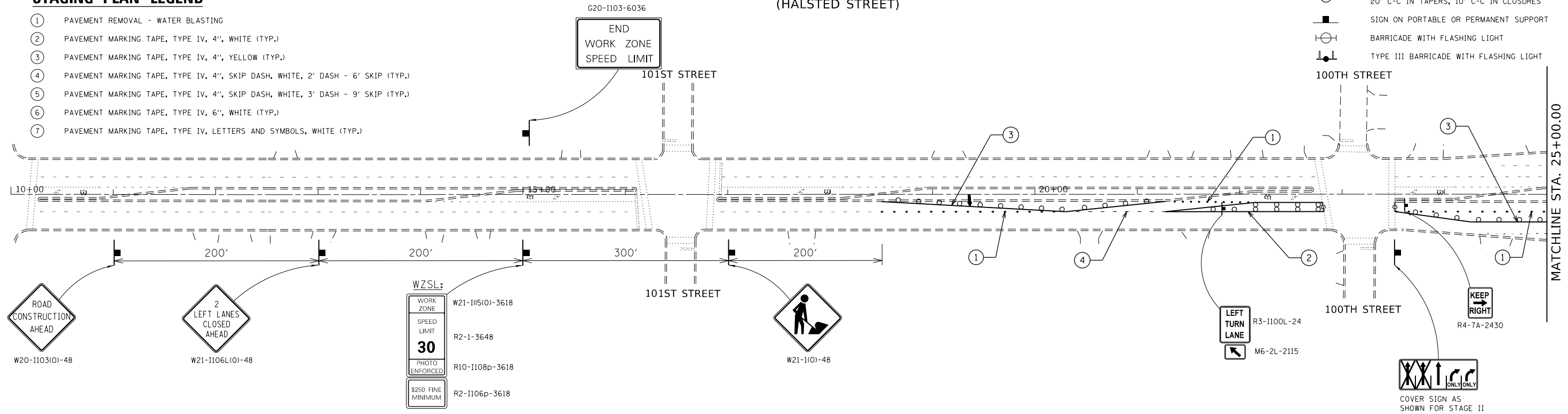


### STAGING PLAN LEGEND

- ① PAVEMENT REMOVAL - WATER BLASTING
- ② PAVEMENT MARKING TAPE, TYPE IV, 4", WHITE (TYP.)
- ③ PAVEMENT MARKING TAPE, TYPE IV, 4", YELLOW (TYP.)
- ④ PAVEMENT MARKING TAPE, TYPE IV, 4", SKIP DASH, WHITE, 2' DASH - 6' SKIP (TYP.)
- ⑤ PAVEMENT MARKING TAPE, TYPE IV, 4", SKIP DASH, WHITE, 3' DASH - 9' SKIP (TYP.)
- ⑥ PAVEMENT MARKING TAPE, TYPE IV, 6", WHITE (TYP.)
- ⑦ PAVEMENT MARKING TAPE, TYPE IV, LETTERS AND SYMBOLS, WHITE (TYP.)

- WORK ZONE
- ARROW BOARD
- DRUMS @ 50' C-C IN TANGENTS, 20' C-C IN TAPERS, 10' C-C IN CLOSURES
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- BARRICADE WITH FLASHING LIGHT
- TYPE III BARRICADE WITH FLASHING LIGHT

### IL. ROUTE 1 (HALSTED STREET)

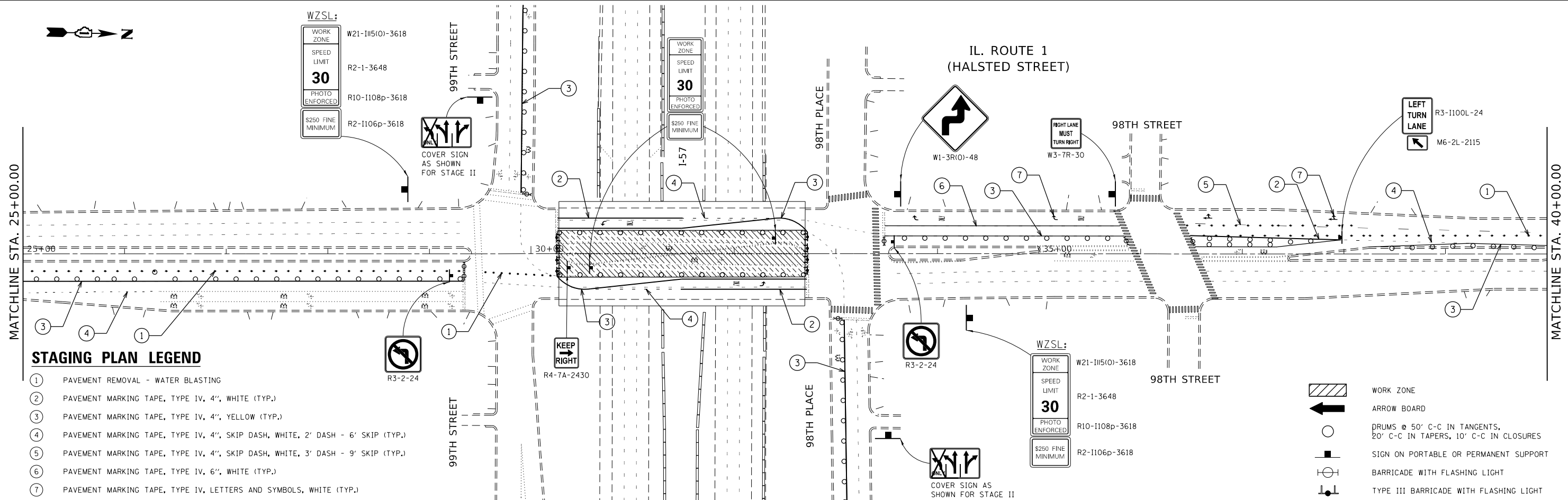


### STAGING PLAN LEGEND

- ① PAVEMENT REMOVAL - WATER BLASTING
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- ⑤ PAVEMENT MARKING TAPE, TYPE IV, 4", SKIP DASH, WHITE, 3' DASH - 9' SKIP (TYP.)
- ⑥ PAVEMENT MARKING TAPE, TYPE IV, 6", WHITE (TYP.)
- ⑦ PAVEMENT MARKING TAPE, TYPE IV, LETTERS AND SYMBOLS, WHITE (TYP.)

- WORK ZONE
- ARROW BOARD
- DRUMS @ 50' C-C IN TANGENTS, 20' C-C IN TAPERS, 10' C-C IN CLOSURES
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- BARRICADE WITH FLASHING LIGHT
- TYPE III BARRICADE WITH FLASHING LIGHT

### IL. ROUTE 1 (HALSTED STREET)



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PLOT DATE = 10/25/2019	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

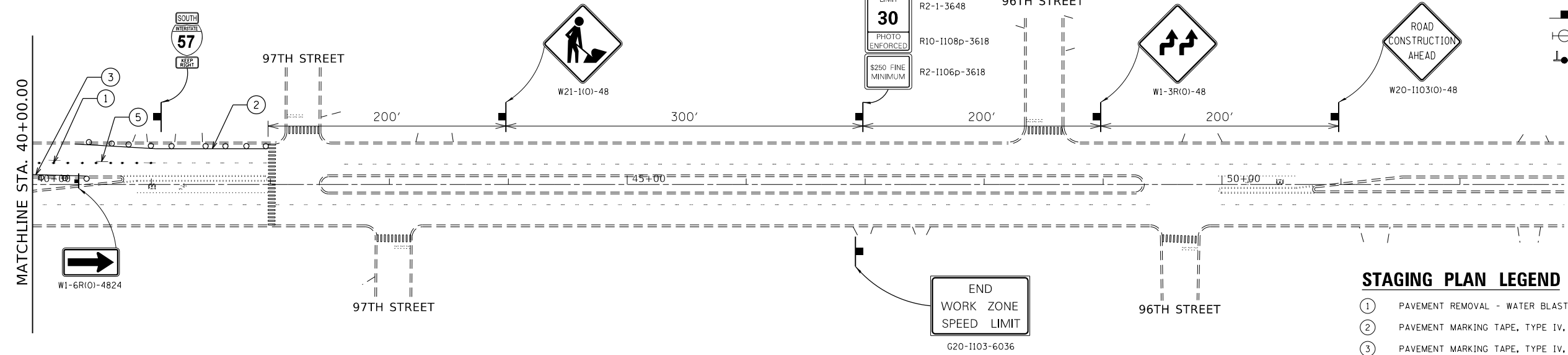
MAINTENANCE OF TRAFFIC (STAGE 2)  
I-57 AT IL ROUTE 1 (HALSTED ST.)

SCALE: 1"= 50' SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2019-056-BR	COOK	31	17
			CONTRACT NO. 62J33	
ILLINOIS FED. AID PROJECT				



IL. ROUTE 1  
(HALSTED STREET)



- WORK ZONE
- ARROW BOARD
- DRUMS @ 50' C-C IN TANGENTS, 20' C-C IN TAPERS, 10' C-C IN CLOSURES
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- BARRICADE WITH FLASHING LIGHT
- TYPE III BARRICADE WITH FLASHING LIGHT

**STAGING PLAN LEGEND**

- ① PAVEMENT REMOVAL - WATER BLASTING
- ② PAVEMENT MARKING TAPE, TYPE IV, 4", WHITE (TYP.)
- ③ PAVEMENT MARKING TAPE, TYPE IV, 4", YELLOW (TYP.)
- ④ PAVEMENT MARKING TAPE, TYPE IV, 4", SKIP DASH, WHITE, 2' DASH - 6' SKIP (TYP.)
- ⑤ PAVEMENT MARKING TAPE, TYPE IV, 4", SKIP DASH, WHITE, 3' DASH - 9' SKIP (TYP.)
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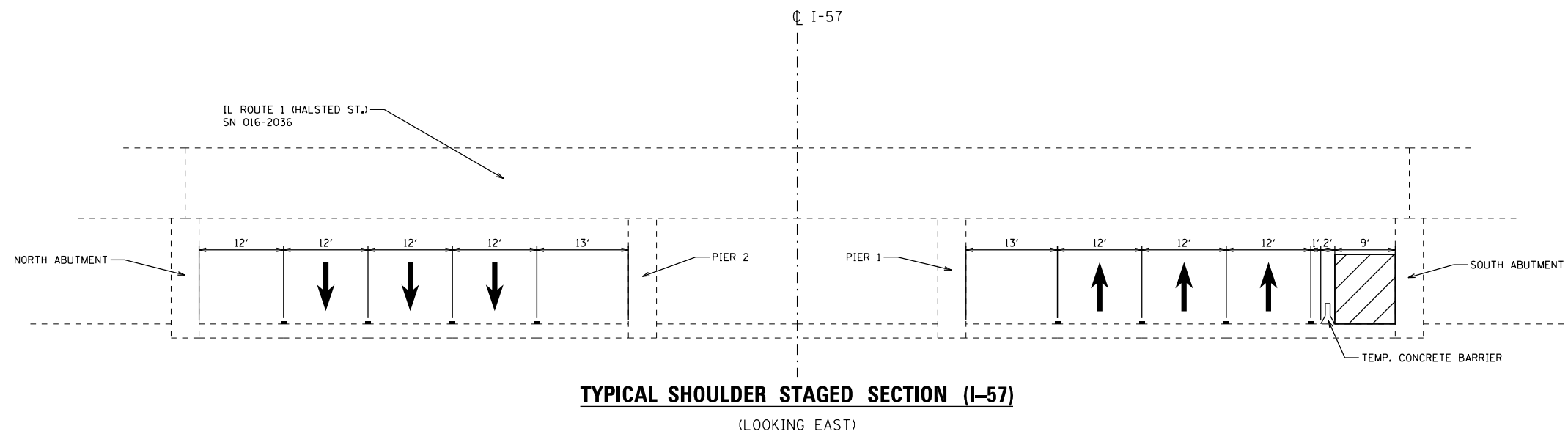
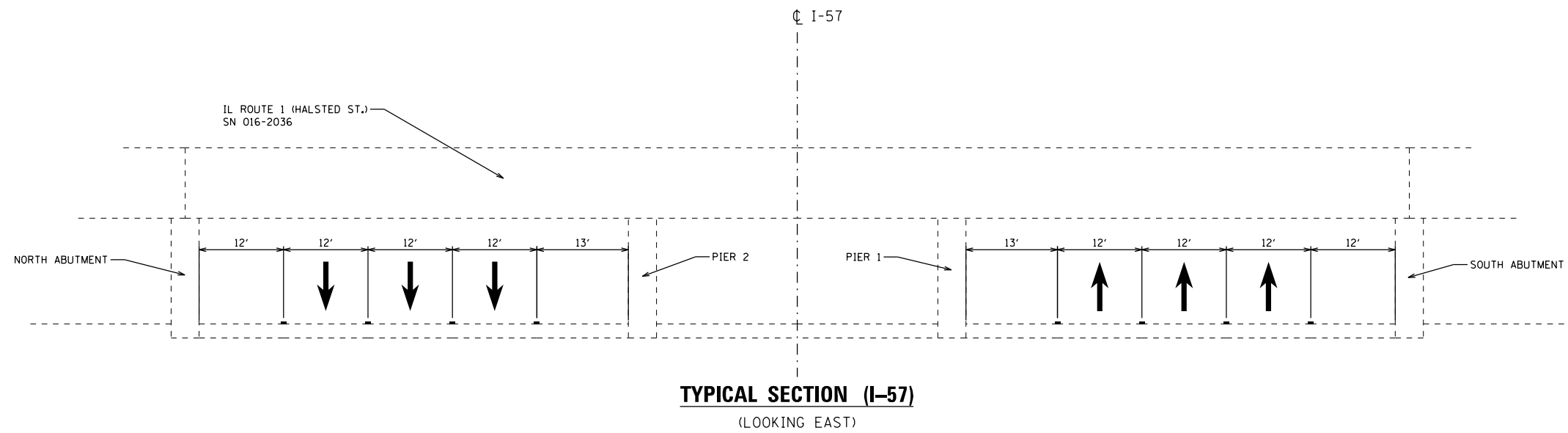
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PLOT DATE = 10/25/2019	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

<b>MAINTENANCE OF TRAFFIC (STAGE 2)</b> <b>I-57 AT IL ROUTE 1 (HALSTED ST.)</b>	
SCALE: 1" = 50'	SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2019-056-BR	COOK	31	18
CONTRACT NO. 62J33			ILLINOIS FED. AID PROJECT	



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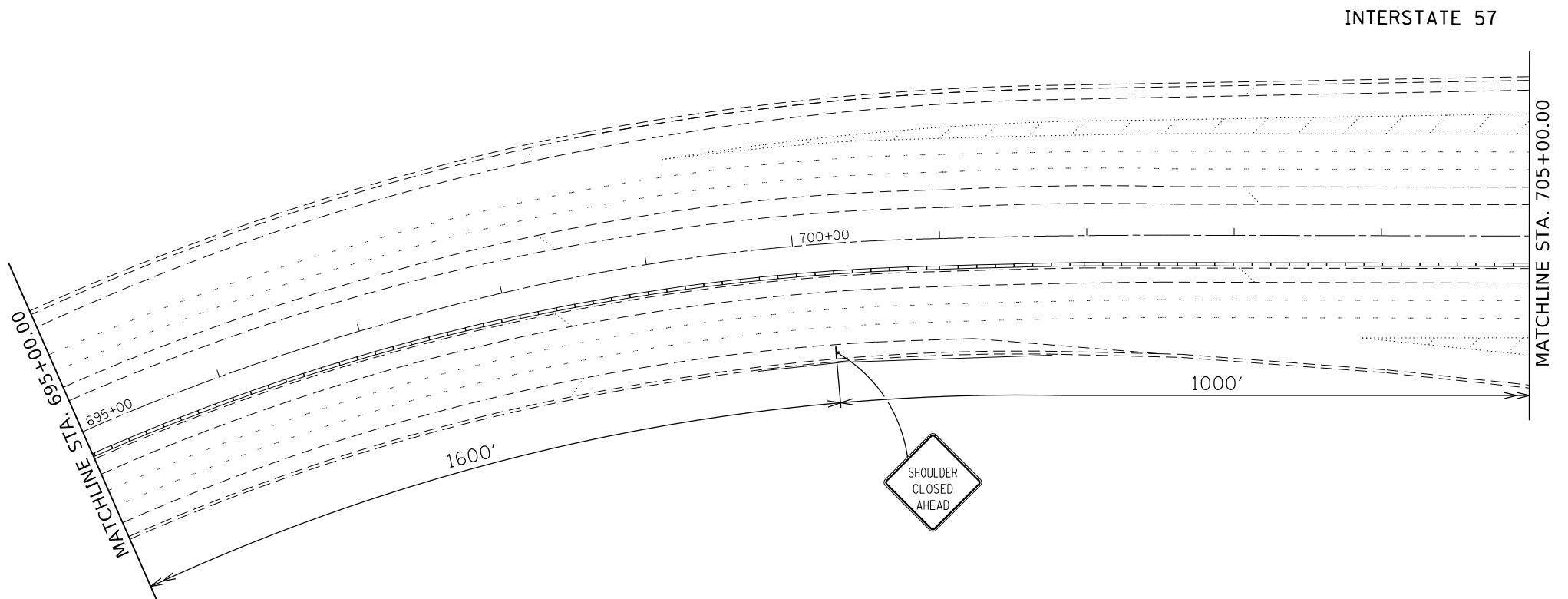
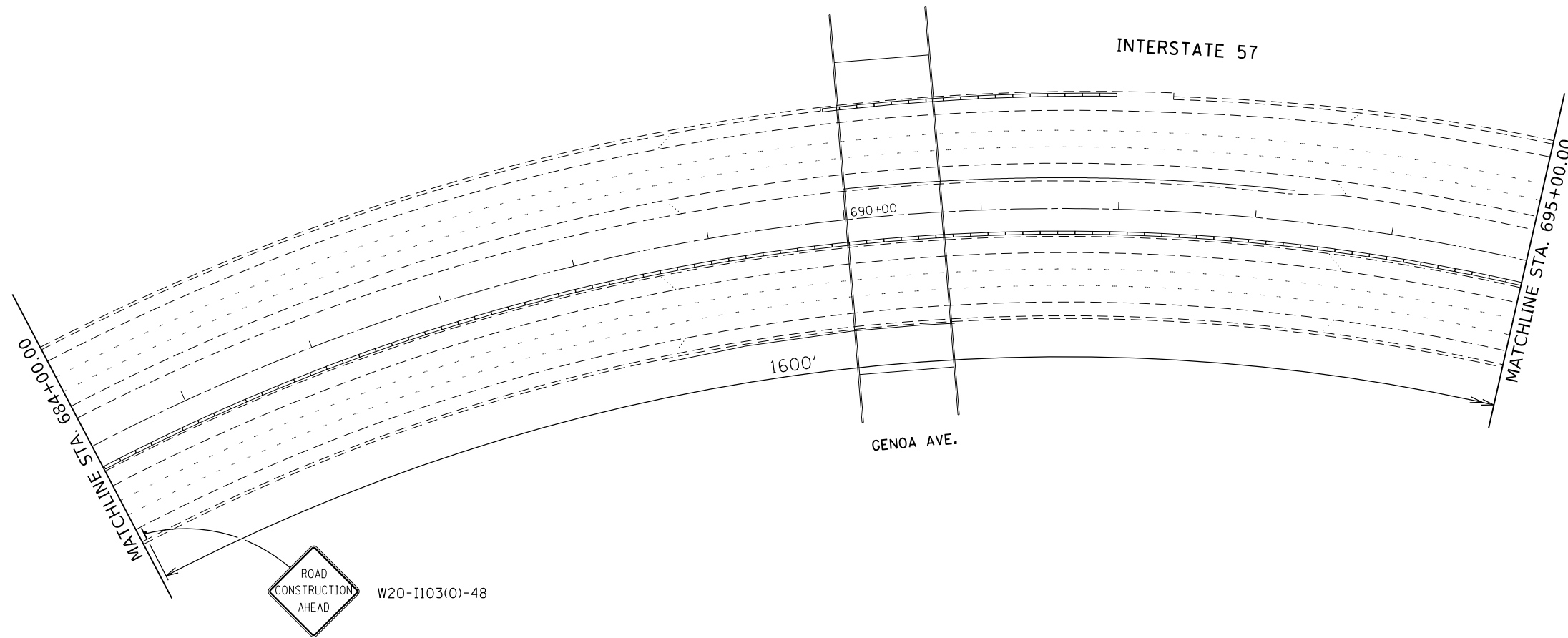
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PLOT DATE = 10/16/2019	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC TYPICAL CROSS SECTIONS (I-57)**  
**I-57 AT IL ROUTE 1 (HALSTED ST.)**

SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2019-056-BR	COOK	31	19
CONTRACT NO. 62J33				
ILLINOIS FED. AID PROJECT				



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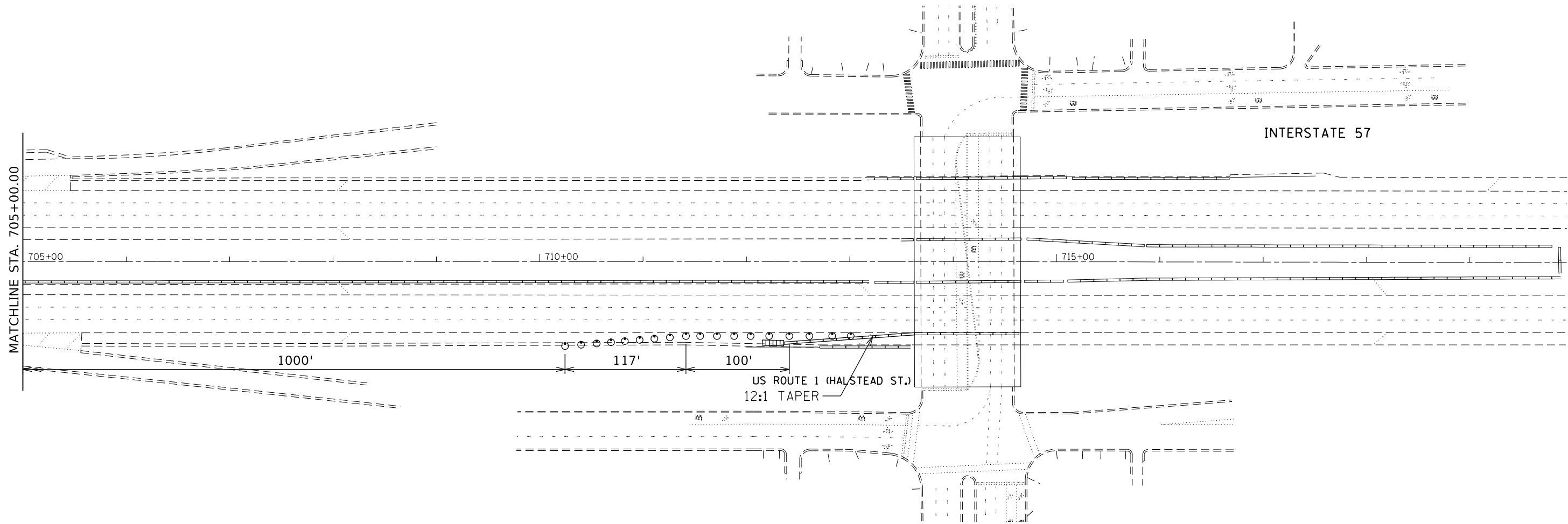
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC (SHOULDER CLOSURE)  
I-57 AT IL ROUTE 1 (HALSTED ST.)**

SCALE: SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2019-056-BR	COOK	31	20
CONTRACT NO. 62J33				
ILLINOIS FED. AID PROJECT				

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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

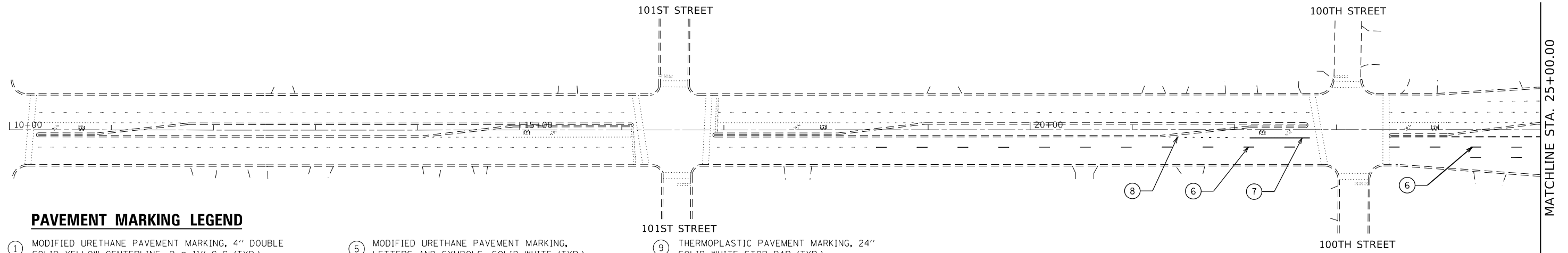
**MAINTENANCE OF TRAFFIC (SHOULDER CLOSURE)  
 I-57 AT IL ROUTE 1 (HALSTED ST.)**

SCALE: SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2019-056-BR	COOK	31	21
CONTRACT NO. 62J33				
ILLINOIS FED. AID PROJECT				

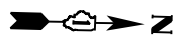


IL. ROUTE 1  
(HALSTED STREET)

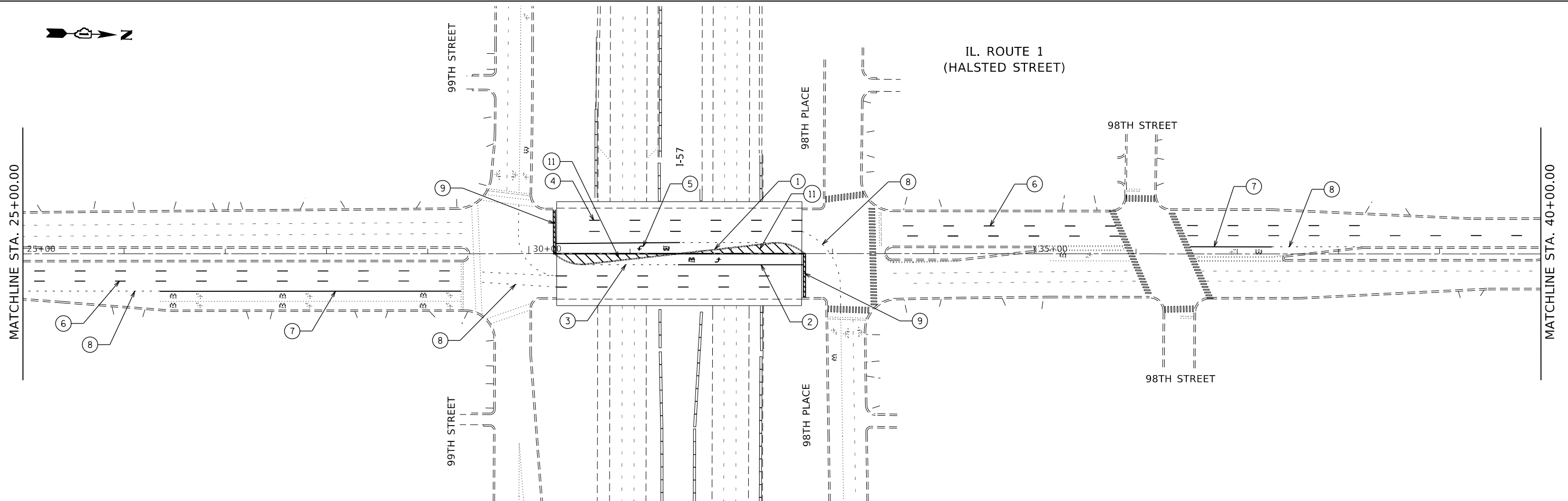


**PAVEMENT MARKING LEGEND**

- |  |   |  |
|--|---|--|
| ① MODIFIED URETHANE PAVEMENT MARKING, 4" DOUBLE SOLID YELLOW CENTERLINE, 2 @ 11" C-C (TYP.)      | ⑤ MODIFIED URETHANE PAVEMENT MARKING, LETTERS AND SYMBOLS, SOLID WHITE (TYP.)             | ⑨ THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE STOP BAR (TYP.)                      |
| ② MODIFIED URETHANE PAVEMENT MARKING, 6" SOLID WHITE TURN LANE (TYP.)                            | ⑥ THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH WHITE LANE LINE, 6' DASH - 18' SKIP (TYP.) | ⑩ THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS, SOLID WHITE (TYP.)              |
| ③ MODIFIED URETHANE PAVEMENT MARKING, 6" SKIP DASH WHITE TURN LANE, 2' LINE WITH 6' SPACE (TYP.) | ⑦ THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE TURN LANE (TYP.)                         | ⑪ MODIFIED URETHANE PAVEMENT MARKING, 12" SOLID YELLOW DIAGONALS @ 45°, 10' C-C (TYP.) |
| ④ MODIFIED URETHANE PAVEMENT MARKING, 4" SKIP DASH WHITE LANE LINE, 6' DASH - 18' SKIP (TYP.)    | ⑧ THERMOPLASTIC PAVEMENT MARKING, 6" SKIP DASH WHITE TURN LANE, 2' DASH - 6' SKIP (TYP.)  |  |



IL. ROUTE 1  
(HALSTED STREET)



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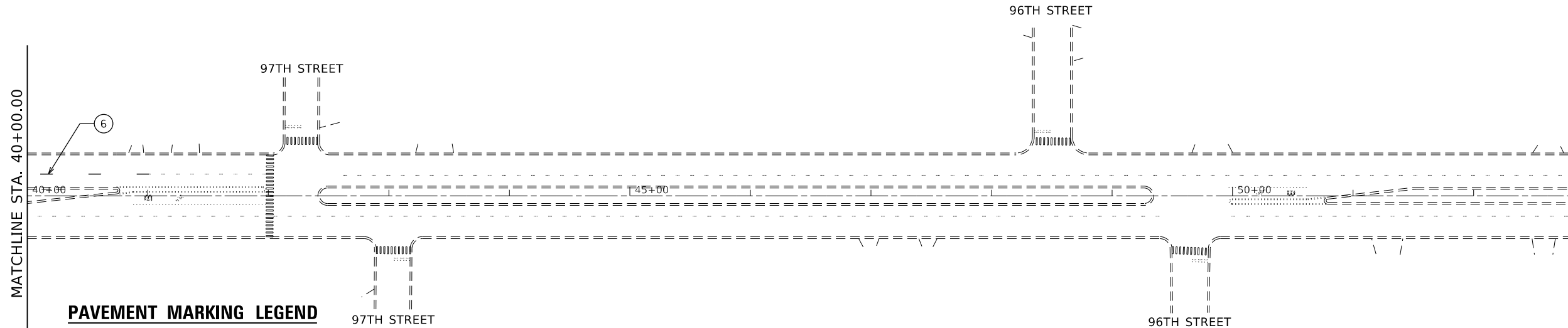
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PROPOSED PAVEMENT MARKING PLAN  
I-57 AT IL ROUTE 1 (HALSTED ST.)  
SCALE: 1" = 50'  
SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2019-056-BR	COOK	31	22
CONTRACT NO. 62J33				
ILLINOIS FED. AID PROJECT				



IL. ROUTE 1  
(HALSTED STREET)



**PAVEMENT MARKING LEGEND**

- |  |   |  |
|--|---|--|
| ① MODIFIED URETHANE PAVEMENT MARKING, 4" DOUBLE SOLID YELLOW CENTERLINE, 2 @ 11" C-C (TYP.)      | ⑤ MODIFIED URETHANE PAVEMENT MARKING, LETTERS AND SYMBOLS, SOLID WHITE (TYP.)             | ⑨ THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE STOP BAR (TYP.)                      |
| ② MODIFIED URETHANE PAVEMENT MARKING, 6" SOLID WHITE TURN LANE (TYP.)                            | ⑥ THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH WHITE LANE LINE, 6' DASH - 18' SKIP (TYP.) | ⑩ THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS, SOLID WHITE (TYP.)              |
| ③ MODIFIED URETHANE PAVEMENT MARKING, 6" SKIP DASH WHITE TURN LANE, 2' LINE WITH 6' SPACE (TYP.) | ⑦ THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE TURN LANE (TYP.)                         | ⑪ MODIFIED URETHANE PAVEMENT MARKING, 12" SOLID YELLOW DIAGONALS @ 45°, 10' C-C (TYP.) |
| ④ MODIFIED URETHANE PAVEMENT MARKING, 4" SKIP DASH WHITE LANE LINE, 6' DASH - 18' SKIP (TYP.)    | ⑧ THERMOPLASTIC PAVEMENT MARKING, 6" SKIP DASH WHITE TURN LANE, 2' DASH - 6' SKIP (TYP.)  |  |

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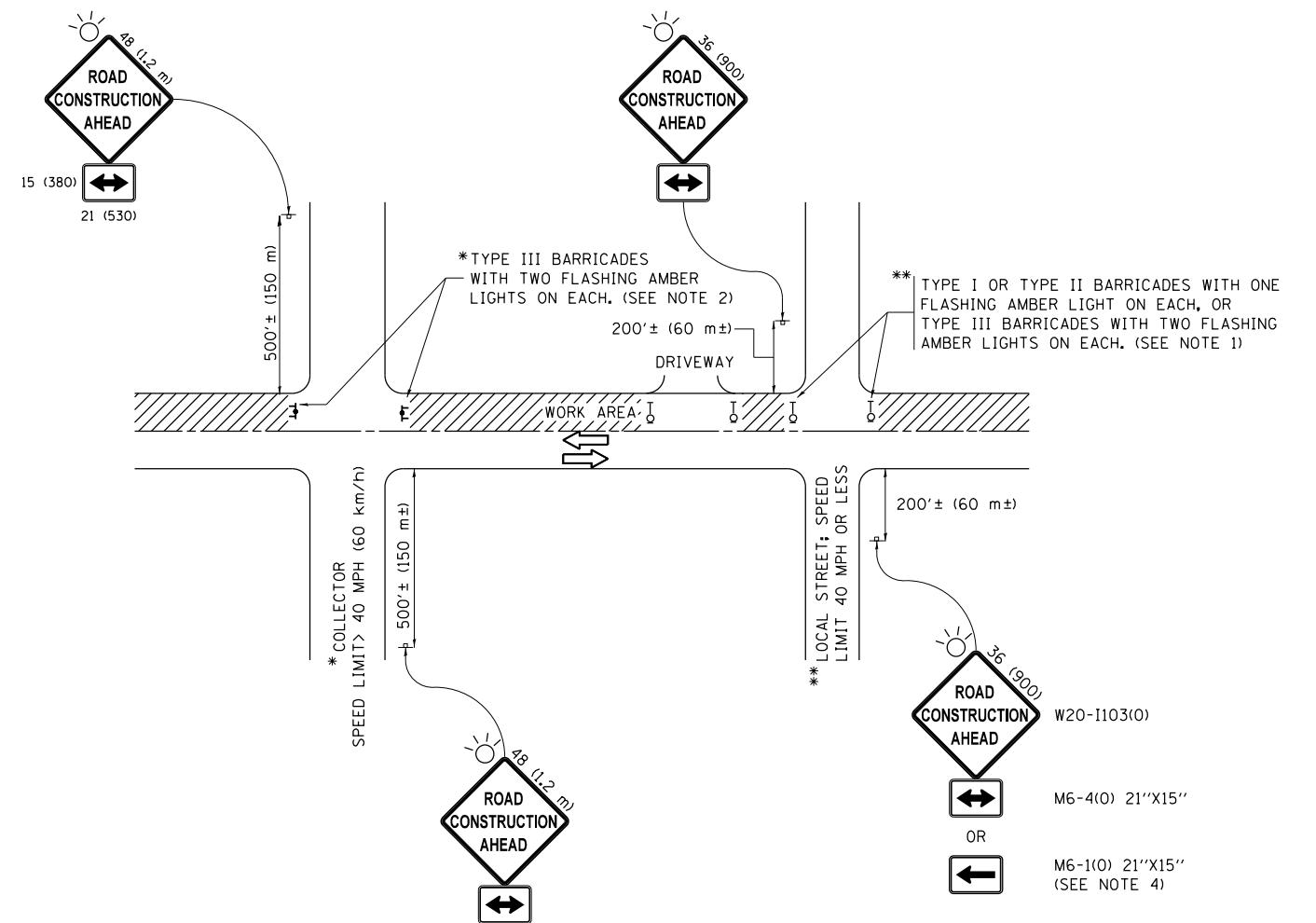
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	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PROPOSED PAVEMENT MARKING PLAN  
I-57 AT IL ROUTE 1 (HALSTED ST.)**

SCALE: 1" = 50'    SHEET 2 OF 2 SHEETS    STA.    TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2019-056-BR	COOK	31	23
CONTRACT NO. 62J33				
ILLINOIS FED. AID PROJECT				



**NOTES:**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = rothjp	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

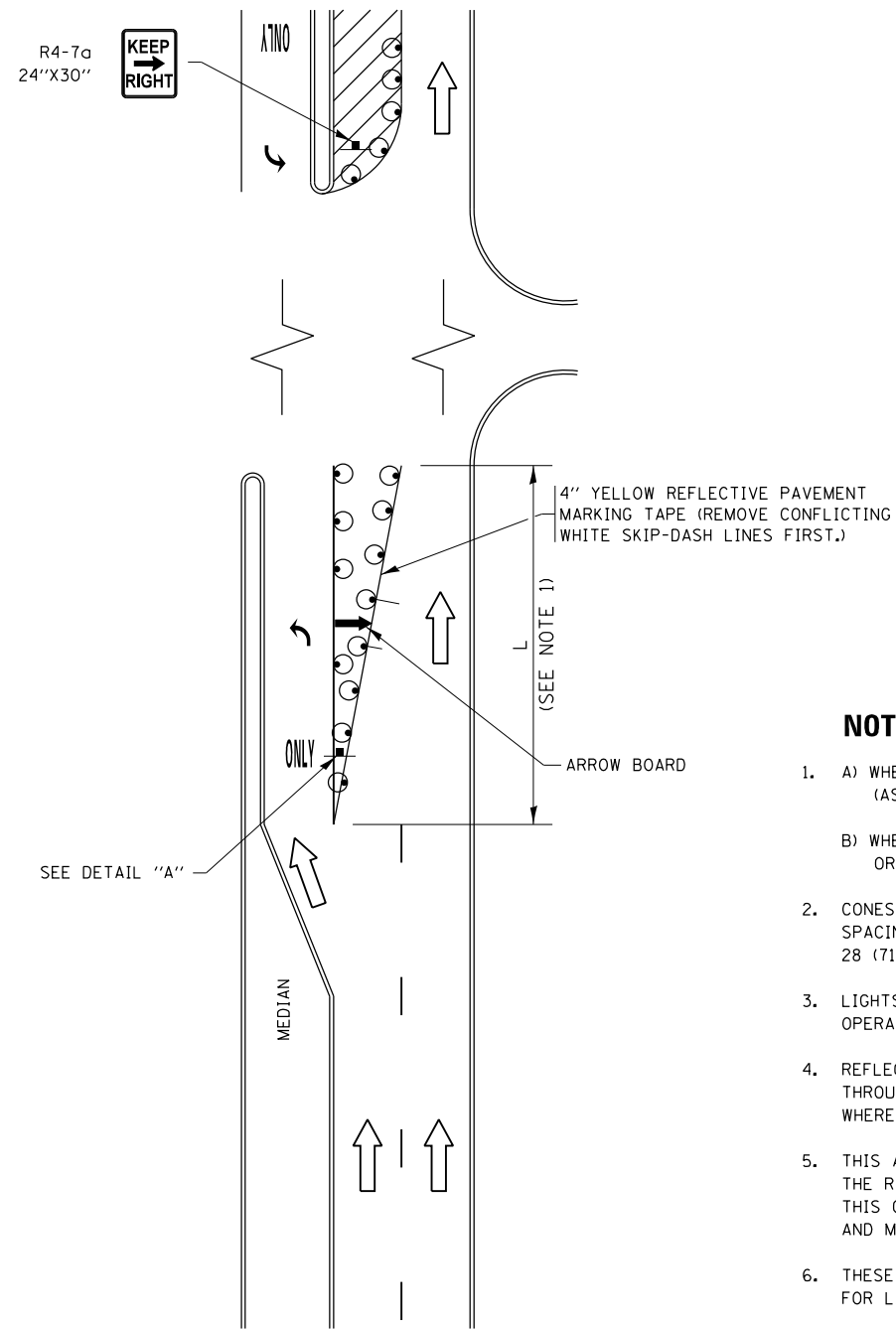
**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2019-056-BR	COOK	31	24
<b>TC-10</b>			<b>CONTRACT NO. 62J33</b>	
ILLINOIS FED. AID PROJECT				

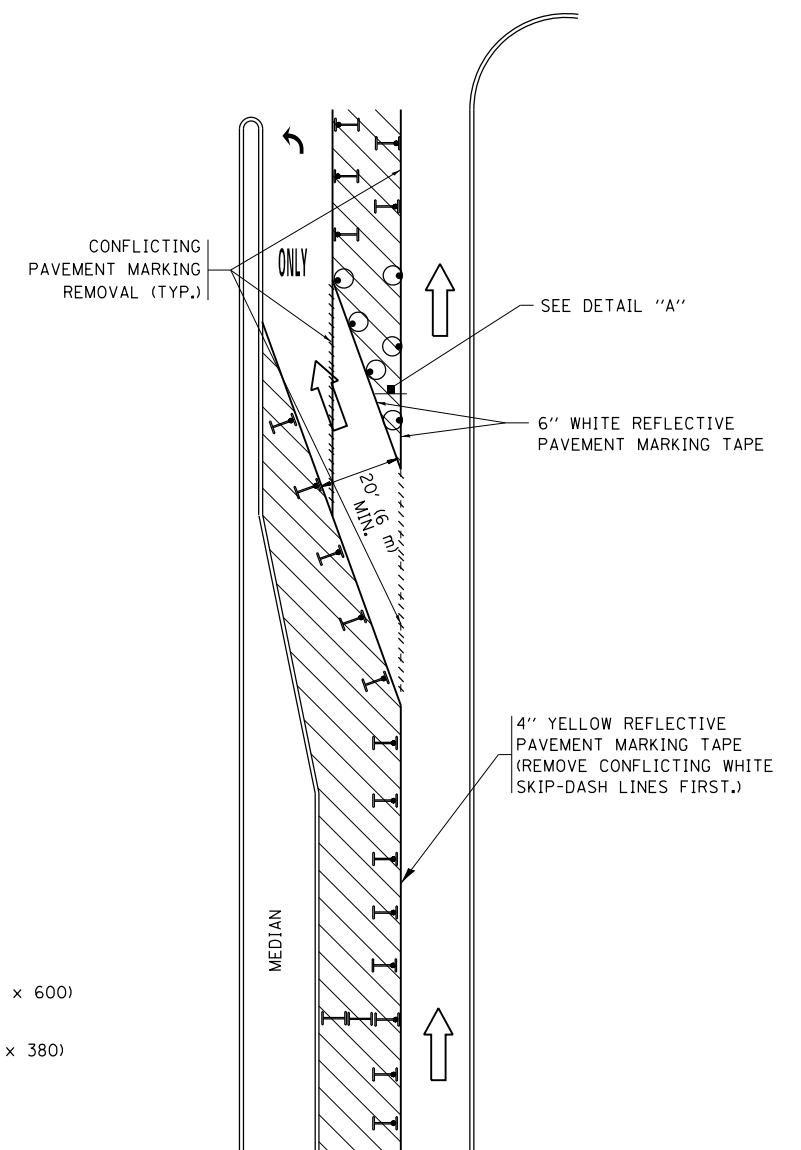


# TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER



**FIGURE 1**

# TURN BAY ENTRANCE WITHIN A LANE CLOSURE



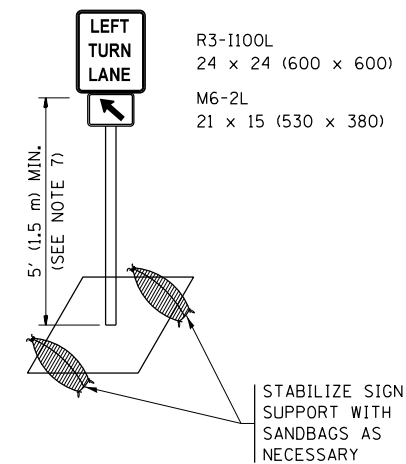
**FIGURE 2**

## LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

## NOTES:

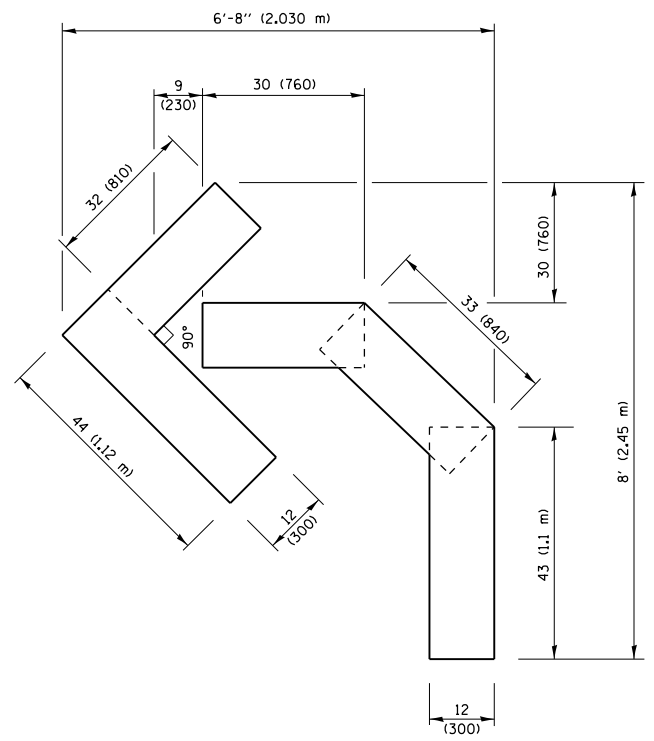
1. A) WHEN "L" IS  $\leq$  THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.  
B) WHEN "L" IS  $>$  THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



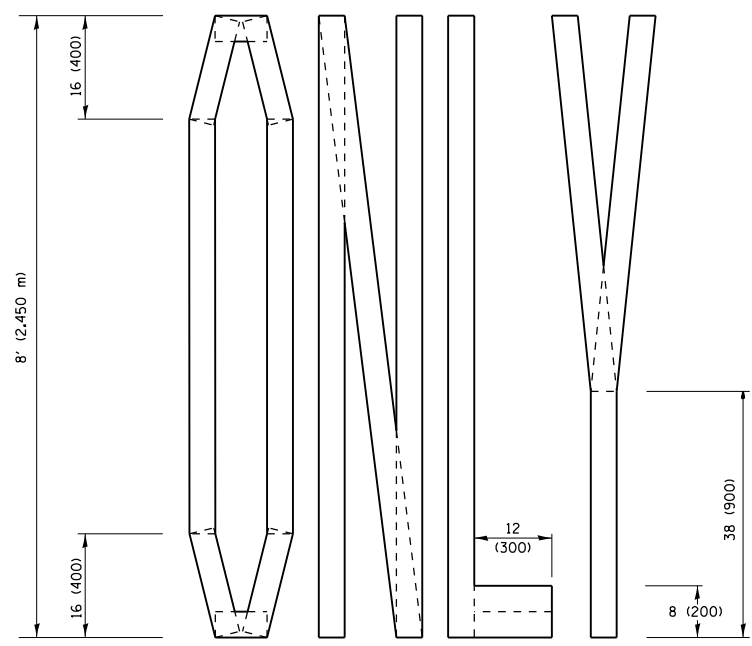
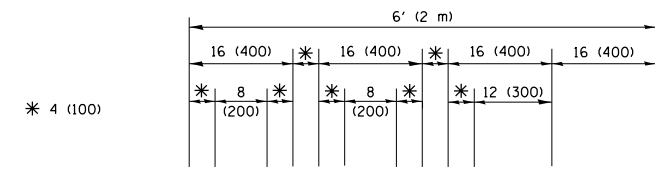
**DETAIL A**

All dimensions are in inches (millimeters) unless otherwise shown.

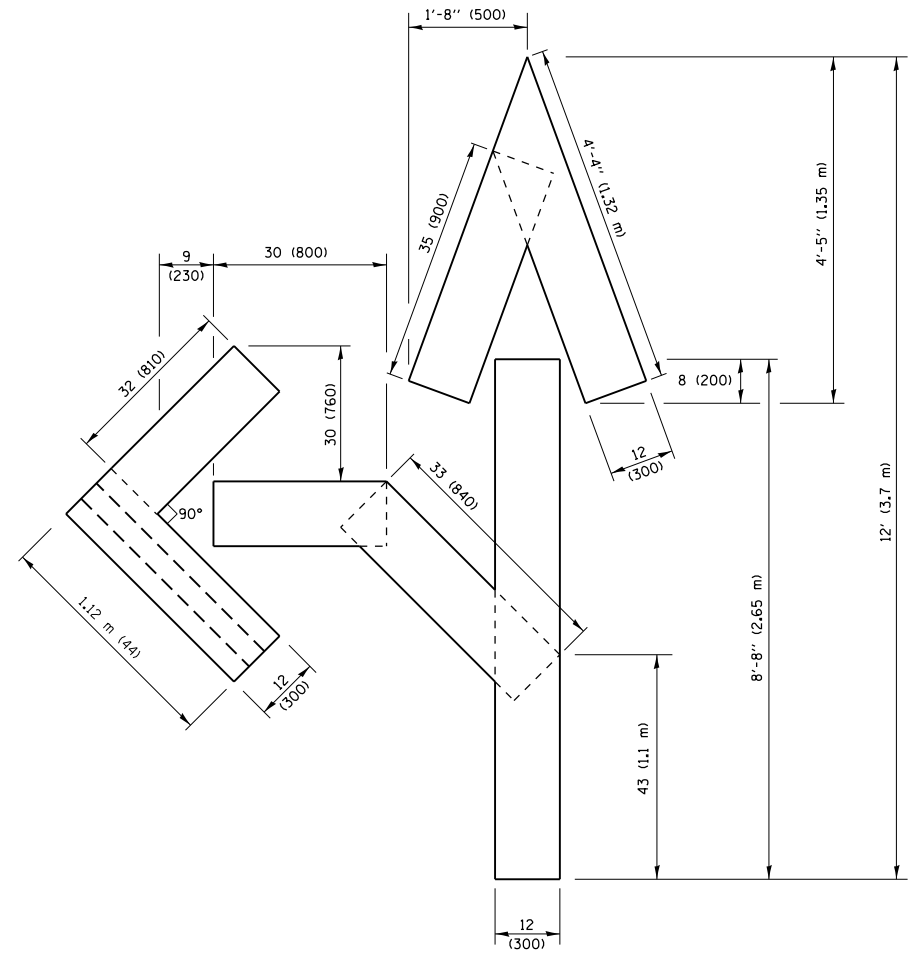
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	PLOT DATE = 10/16/2019	REVISED - T. RAMMACHER 01-06-00	REVISED -			SCALE: NONE		SHEET 1 OF 1 SHEETS		STA. TO STA.	



**QUANTITY**  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.41 sq. m)

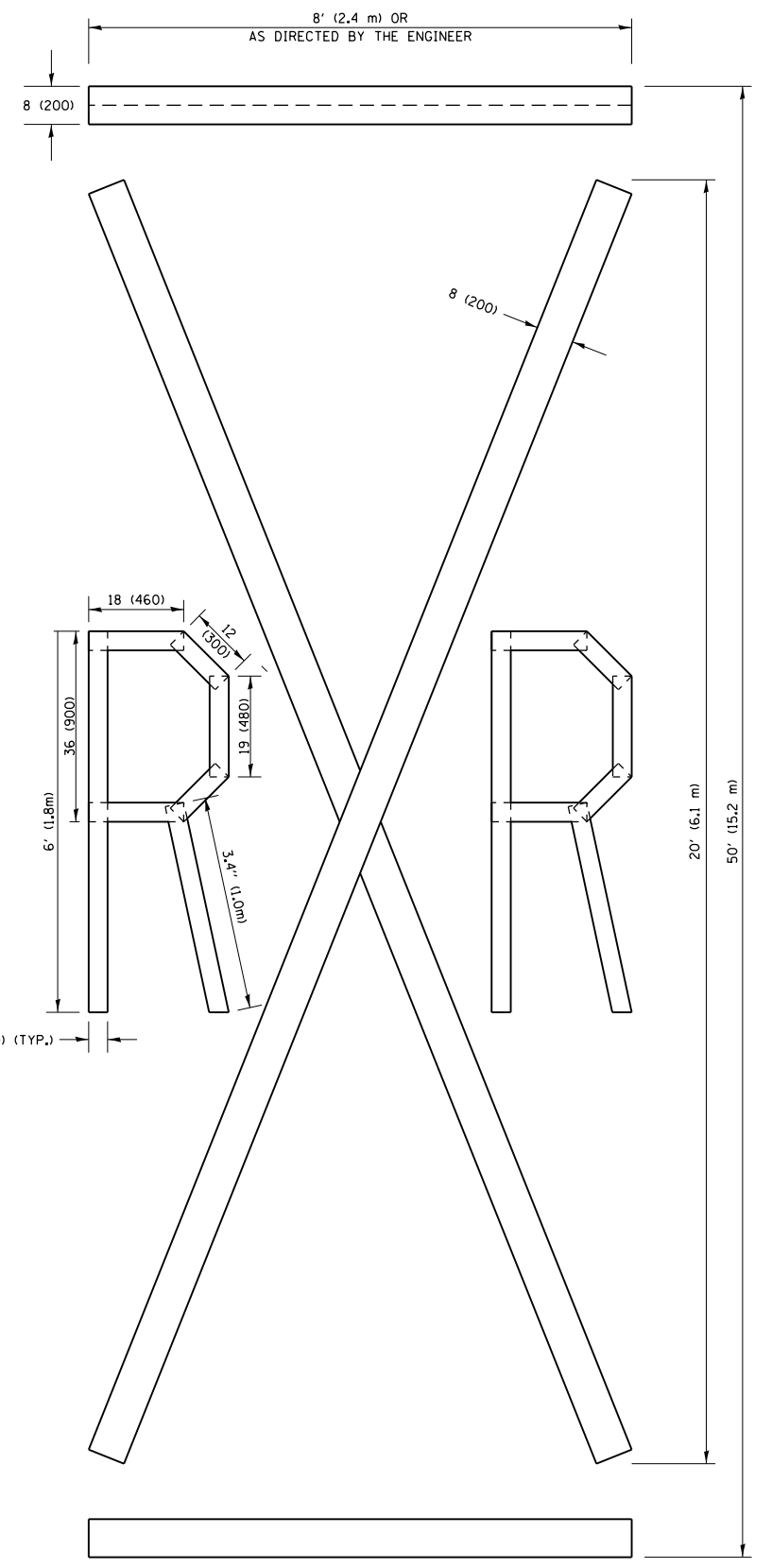


**QUANTITY**  
 4 (100) LINE = 64.1 ft. (19.5 m)  
 21.4 sq. ft. (1.99 sq. m)



**QUANTITY**  
 4 (100) LINE = 82.5 ft. (25.1 m)  
 27.5 sq. ft. (2.53 sq. m)

**NOTE:**  
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**  
 4 (100) LINE = 225.9 ft. (68.9 m)  
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = rothjp	DESIGNED -	REVISED -T. RAMMACHER 03-02-98
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			REVISED -A. SCHUETZE 09-15-16

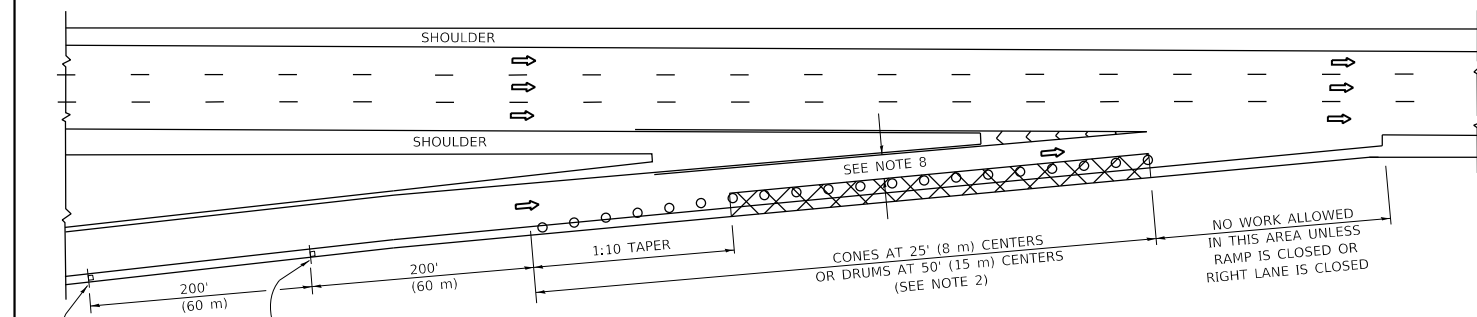
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS</b>			
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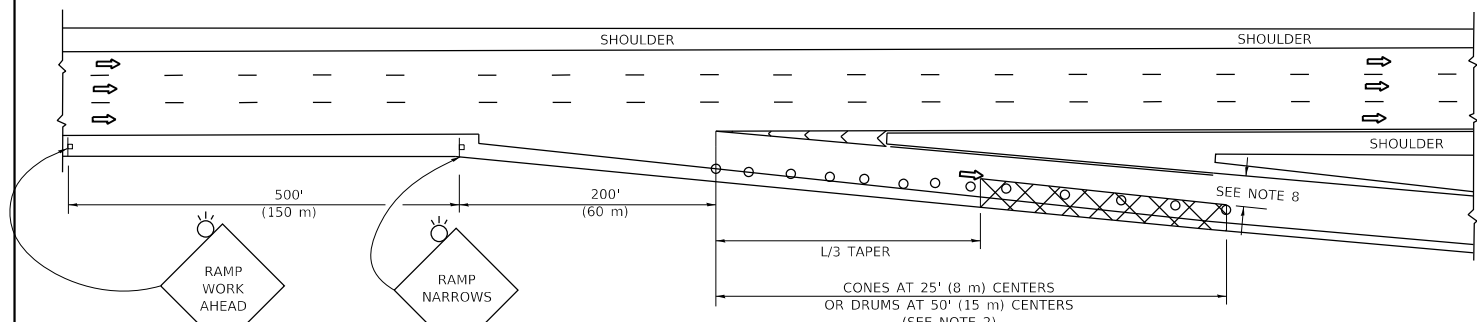
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<b>TC-16</b>			<b>CONTRACT NO. 62J33</b>	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

**PARTIAL RAMP CLOSURE DETAILS**

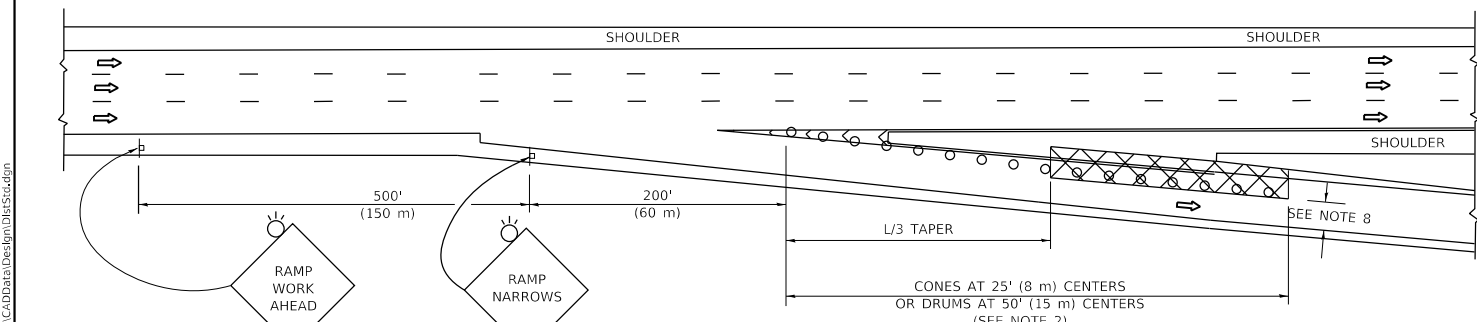
**SHOULDER CLOSURE DETAILS**



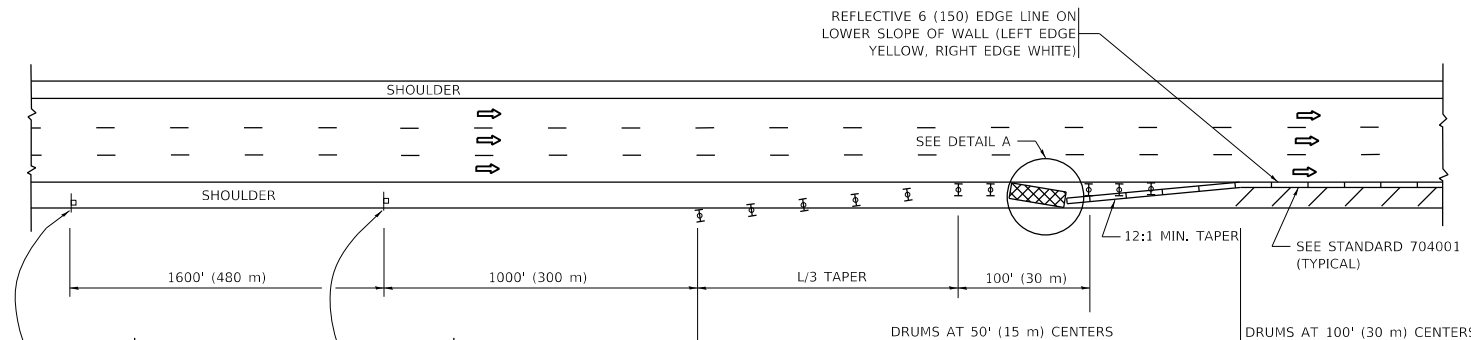
**TYPICAL ENTRANCE RAMP**



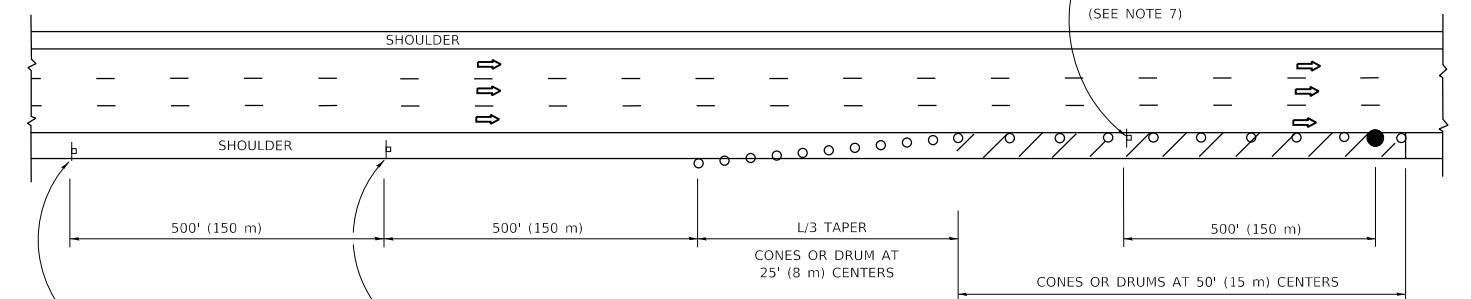
**TYPICAL EXIT RAMP**



**TYPICAL EXIT RAMP**

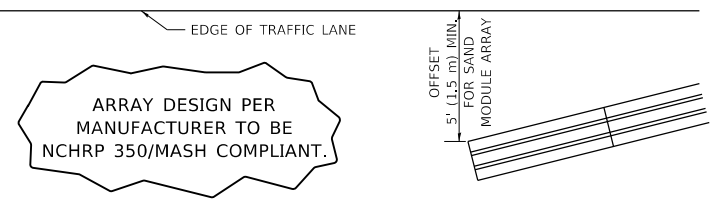


**PERMANENT SHOULDER CLOSURE**



**DAYTIME SHOULDER CLOSURE**

THIS DETAIL IS USED WHERE:  
 1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCR OACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



**DETAIL "A"  
 IMPACT ATTENUATOR, TEMPORARY  
 (SEE NOTE 5)**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

**SYMBOLS**

- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

**GENERAL NOTES:**

1. THE "L" DISTANCE EQUALS:  

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC ENGLISH L=0.65(WXS) L=(W)XS
W = WIDTH OF OFFSET IN FEET (METERS)	
S = NORMAL POSTED SPEED MPH (KM/H)	
2. TYPE II BARRICADES OR DRUMS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES. TYPE II BARRICADES OR DRUMS WITH MONODIRECTIONAL STEADY BURN LIGHTS ARE REQUIRED FOR DELINEATING OBSTACLES, EXCAVATIONS, OR HAZARDS EXCEEDING 100 FT (30m) IN LENGTH AT NIGHT.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
  - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
  - b. THE WORK AVTTIVITY REQUIRES FREQUENT ENCR OACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
8. 12' MIN. WIDTH TANGENT SECTION  
 16' MIN. WIDTH CURVE SECTION.

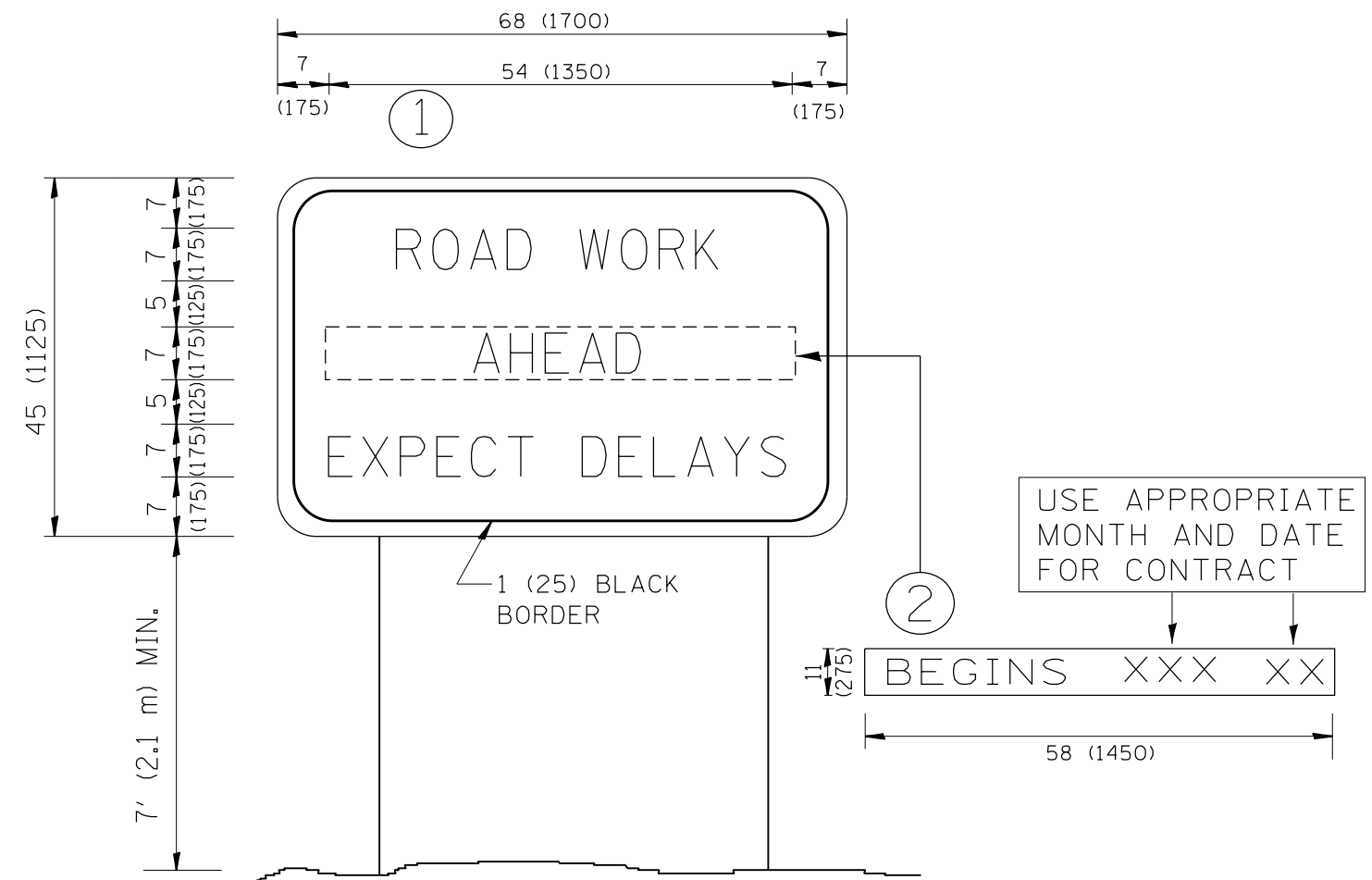
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES			
SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. TO STA.	

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2019-056-BR	COOK	31	27
<b>TC-17</b>		CONTRACT NO. 62J33		
ILLINOIS FED. AID PROJECT				



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

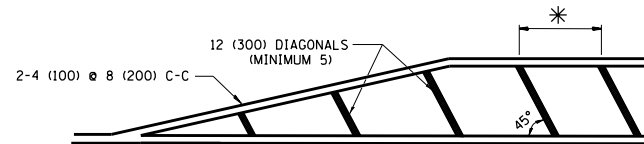
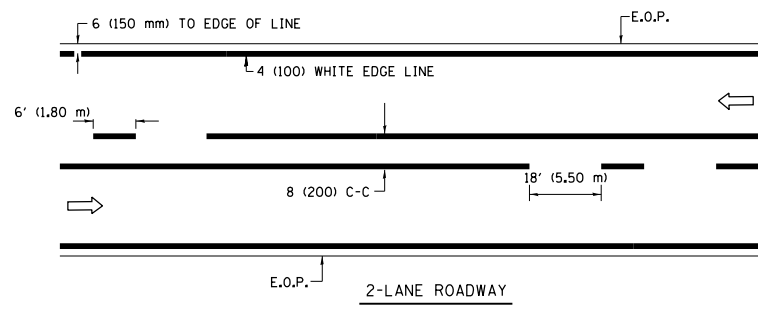
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	PLOT DATE = 10/16/2019	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

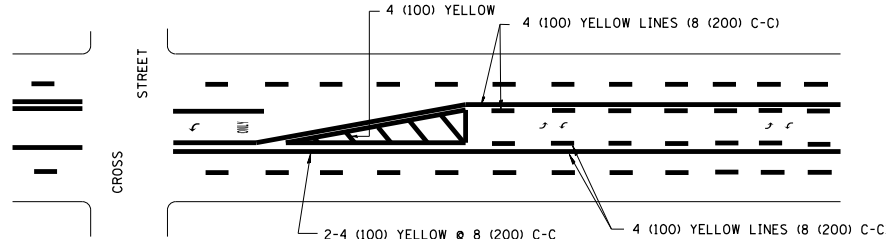
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2019-056-BR	COOK	31	28
TC-22			CONTRACT NO. 62J33	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



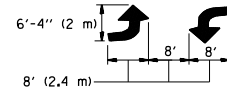
\* FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

\* DIAGONAL LINE SPACING: 20' (6.1 m) C-C

**PAINTED MEDIANS**

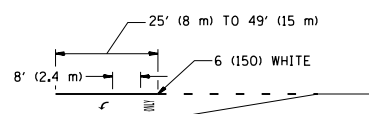
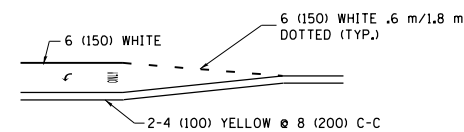


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



**MEDIAN WITH TWO-WAY LEFT TURN LANE**

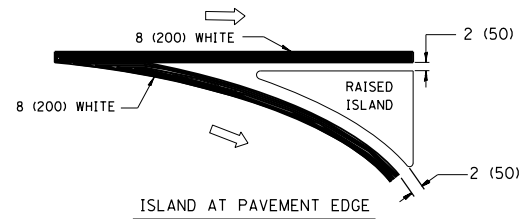
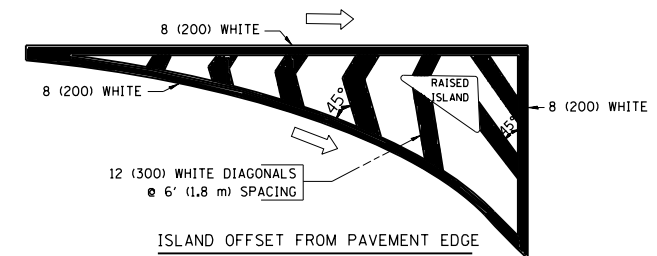
**TYPICAL PAINTED MEDIAN MARKING**



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
 AREA = 15.8 SQ. FT. (1.47 m<sup>2</sup>) ONLY AREA = 22.9 SQ. FT. (2.13 m<sup>2</sup>)  
 \* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**

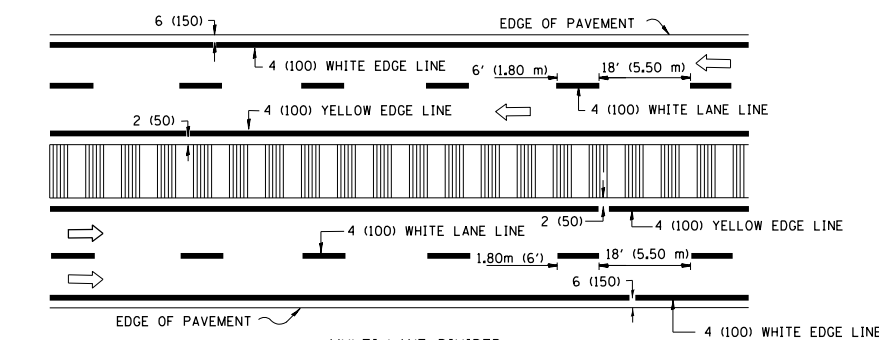
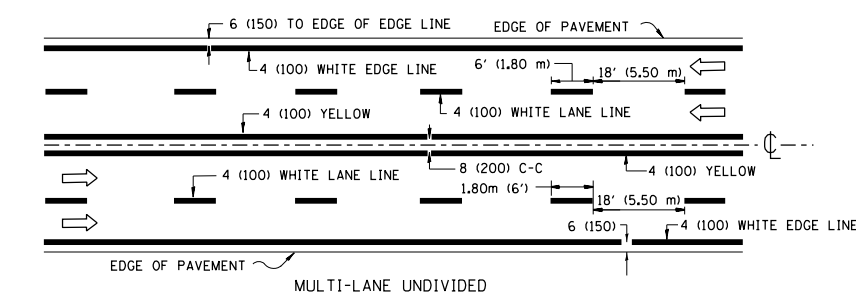


**TYPICAL ISLAND MARKING**

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	8 (200) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	8 (200) C-C
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	6' (1.8 m) LINE WITH 18' (5.50 m) SPACE FOR SKIP-DASH; 8 (200) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL & PEDESTRIAN)	12 (300) @ 45° 24 (600) @ 90°	SOLID SOLID	WHITE WHITE	2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	8 (200) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 20' (6.1 m) (LESS THAN 30 MPH (50 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )

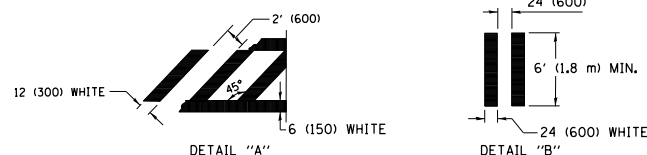
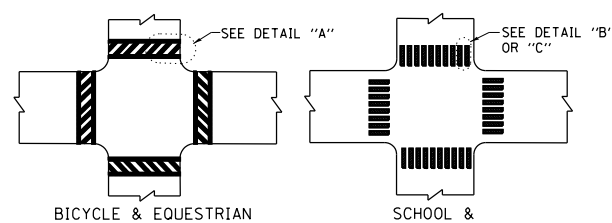
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STREET MARKING STANDARDS, PRINTED BY CITY OF CHICAGO, DEPARTMENT OF TRANSPORTATION, BUREAU OF TRAFFIC.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

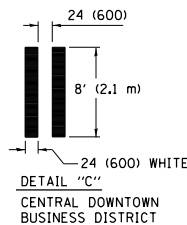


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

**TYPICAL LANE AND EDGE LINE MARKING**



**TYPICAL CROSSWALK MARKING**



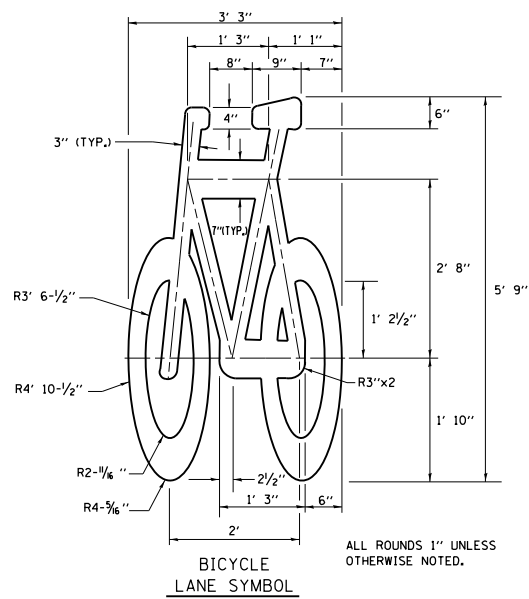
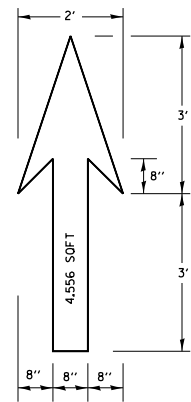
**DETAIL "C"  
CENTRAL DOWNTOWN BUSINESS DISTRICT**

FILE NAME =	USER NAME = rothjp	DESIGNED -	REVISED - T. RAMMACHER 12-07-00
pw:\planning\dot.illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\011831\DRAWING\Design\DistStd.dgn		CHECKED -	REVISED - K. ENG 02-28-12
		DATE -	REVISED -
			REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

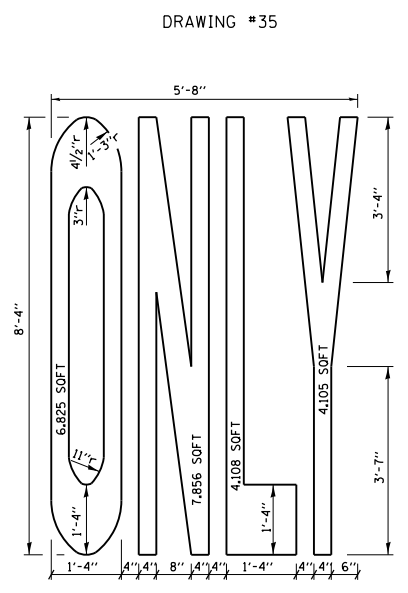
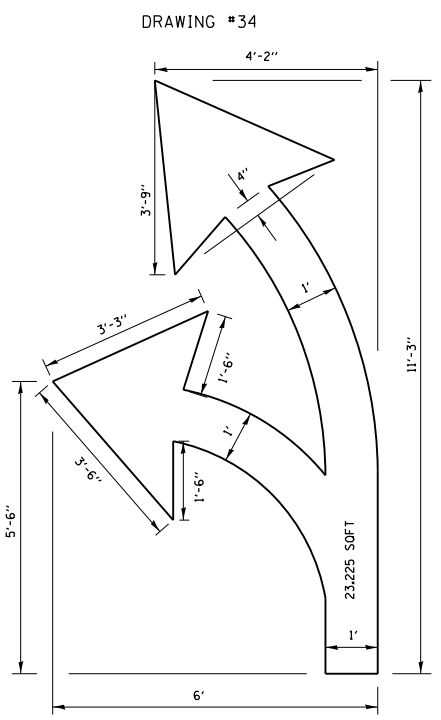
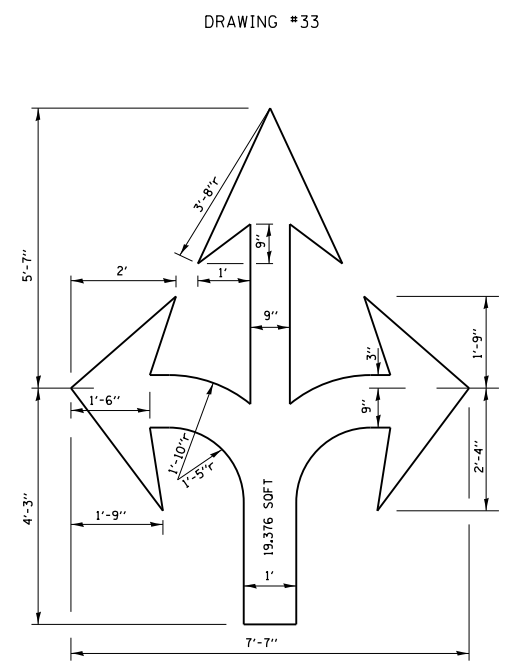
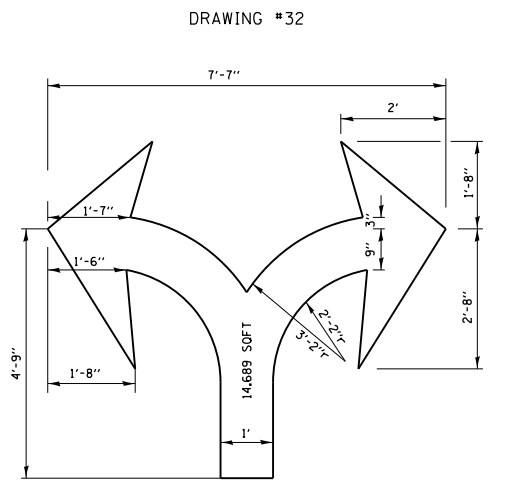
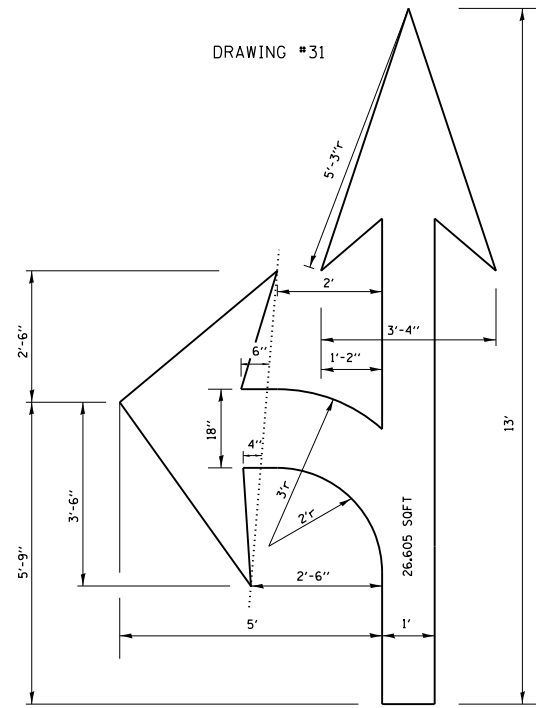
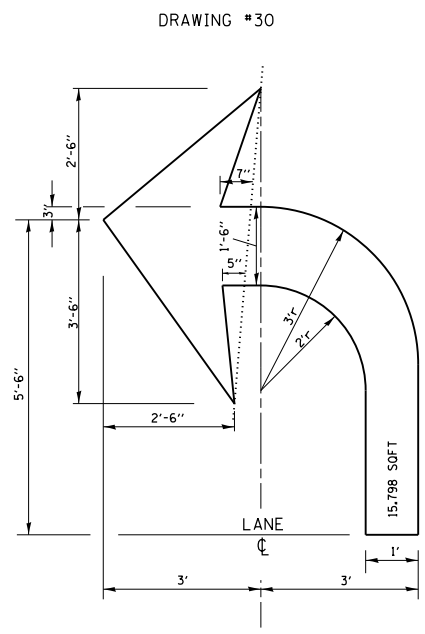
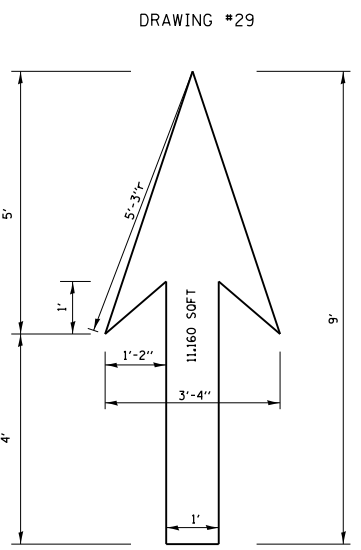
<b>CITY OF CHICAGO</b>			
<b>TYPICAL PAVEMENT MARKINGS</b>			
SCALE: NONE	SHEET NO. 1 OF 3 SHEETS	STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2019-056-BR	COOK	31	29
<b>TC-24</b>		<b>CONTRACT NO. 62J33</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTE:**  
 1.) FOR BIKE LANE SYMBOLS ONLY, USE PRE-FORMED THERMOPLASTIC WITH A MINIMUM THICKNESS OF 90 MILS, MINIMUM SKID RESISTANCE VALUE OF 60 BPN, & A MINIMUM INDEX OF REFRACTION OF 1.50.  
 2.) THE RESIDENT ENGINEER SHALL CONTACT MR. BEN GOMBERG AT 312-744-8093 AT LEAST ONE CALENDAR WEEK PRIOR TO INSTALLING BIKE LANE SYMBOLS.

**TYPICAL BIKE LANE SYMBOLS**  
DRAWING #28



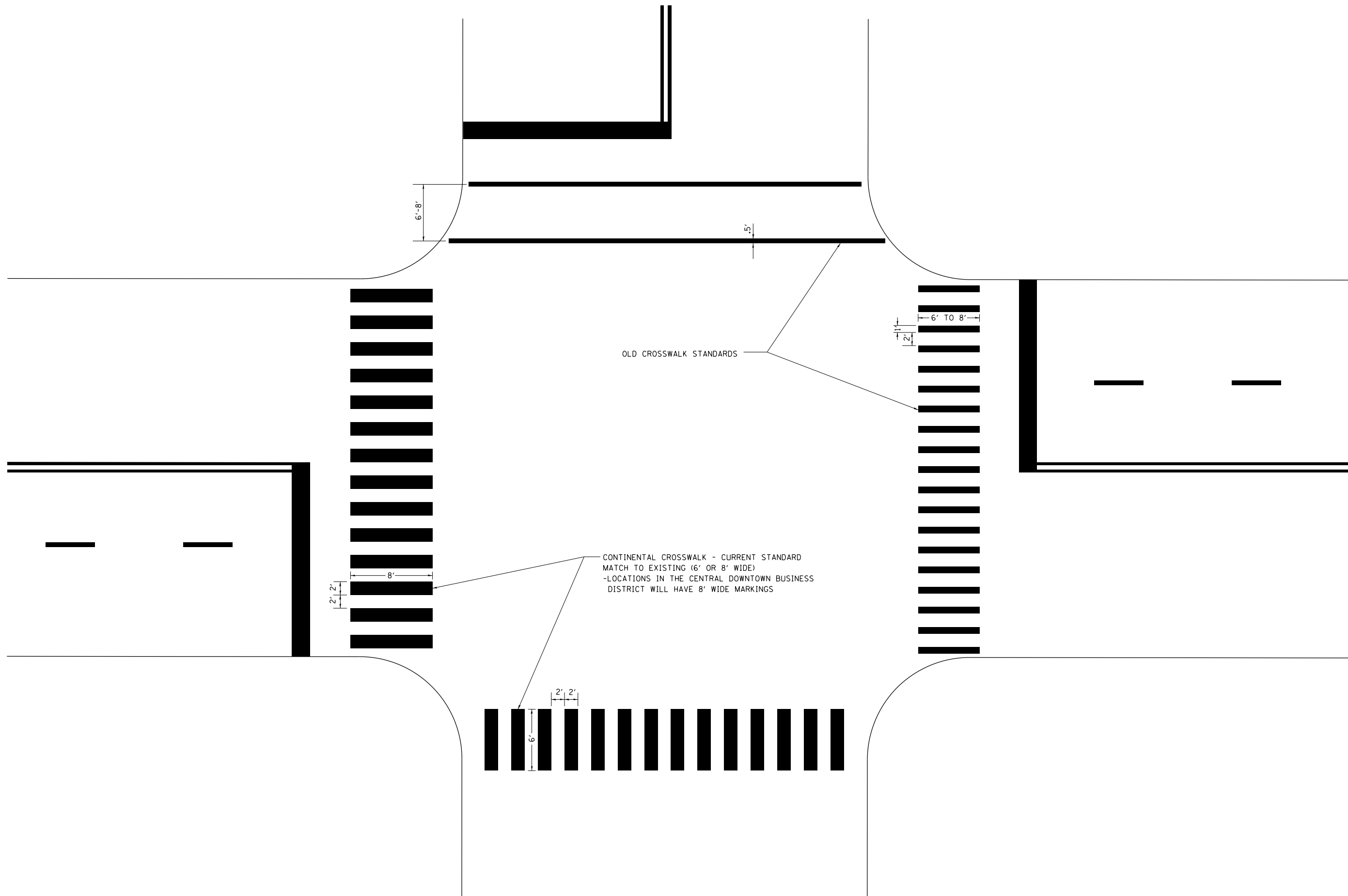
**NOTE:**  
ALL MARKINGS SHALL BE SOLID WHITE UNLESS OTHERWISE NOTED IN THE PLANS

FILE NAME =	USER NAME = rothjp	DESIGNED -	REVISED - T. RAMMACHER 12-07-00
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PLOT SCALE = 100.0000' / in.		DATE -	REVISED -
PLOT DATE = 10/16/2019			

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

<b>CITY OF CHICAGO</b>			
<b>TYPICAL PAVEMENT MARKINGS</b>			
SCALE: NONE	SHEET NO. 2 OF 3 SHEETS	STA.	TO STA.

F.A.I. RE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2019-056-BR	COOK	31	30
<b>TC-24</b>		<b>CONTRACT NO. 62J33</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 10/16/2019	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>CITY OF CHICAGO</b>			
<b>TYPICAL PAVEMENT MARKINGS</b>			
SCALE: NONE	SHEET NO. 3 OF 3 SHEETS	STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2019-056-BR	COOK	31	31
<b>TC-24</b>			<b>CONTRACT NO. 62J33</b>	
FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT				