

|                    |        |              |           |
|--------------------|--------|--------------|-----------|
| SECTION            | COUNTY | TOTAL SHEETS | SHEET NO. |
| 91-3)B-2           | UNION  | 251          | 1         |
| CONTRACT NO. 78504 |        |              |           |

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FOR SUMMARY OF QUANTITIES, SEE SHEETS 4-12

**TRAFFIC DATA**

**SN 091-0005 (NB)**

FUNCTIONAL CLASSIFICATION: FEDERAL-AID INTERSTATE  
 MAIN ROUTE  
 ADT: 6577 (2019)  
 PV: 57.7%  
 TRUCKS: 42.3%  
 DESIGN SPEED: 70 MPH  
 POSTED SPEED: 70 MPH

**TRAFFIC DATA**

**SN 091-0006 (SB)**

FUNCTIONAL CLASSIFICATION: FEDERAL-AID INTERSTATE  
 MAIN ROUTE  
 ADT: 6577 (2019)  
 PV: 57.7%  
 TRUCKS: 42.3%  
 DESIGN SPEED: 70 MPH  
 POSTED SPEED: 70 MPH

**TOWNSHIP**

UNION COUNTY UNIT ROAD DISTRICT

DESIGN DESIGNATION : N.A.

COORDINATE SYSTEM : IDOT CONTROL (HORIZONTAL)  
IDOT CONTROL (VERTICAL)

POSTED SPEED : 70 MPH



ESCA PROJECT NO. 1259 11

JULIE.  
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
 1-800-892-0123  
 OR 811

PROJECT ENGINEER: DAVID PICHE  
 PROJECT DESIGNER: ESCA CONSULTANTS, INC.

CONTRACT NO. 78504

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

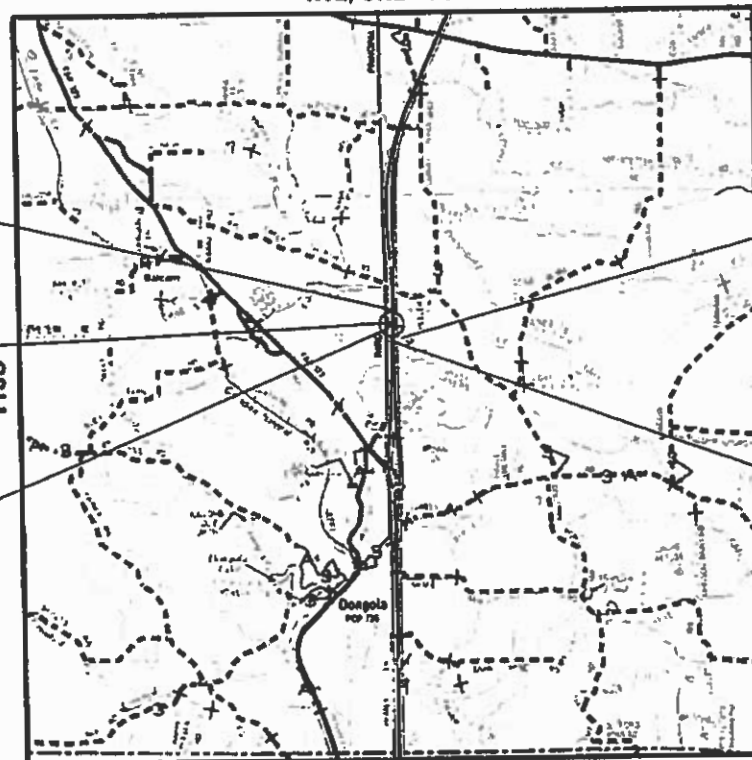
# PROPOSED HIGHWAY PLANS

FAI ROUTE 57 (I-57)  
 SECTION (91-3)B-2  
 PROJECT NHPP-U2AS(458)  
 BRIDGE REPLACEMENT  
 OVER BIG CREEK  
 UNION COUNTY

C-99-003-16  
R1E, 3RD PM



IMPROVEMENTS BEGIN  
STA 2397+30



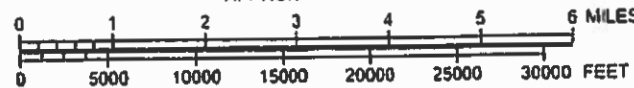
BIG CREEK

IMPROVEMENTS END  
STA 2899+50

EXISTING STRUCTURE NO. 091-0005  
 STATION 2895+35.00  
 PROPOSED STRUCTURE NO. 091-0077  
 STATION 2395+15.00  
 OVER BIG CREEK  
 THREE SPAN 1933 STEEL BEAMS  
 207'-3" BK TO BK ABUTMENTS  
 SKEWED 45° LEFT FORWARD

EXISTING STRUCTURE NO. 091-0006  
 STATION 2394+90.07  
 PROPOSED STRUCTURE NO. 091-0078  
 STATION 2394+90.07  
 OVER BIG CREEK  
 THREE SPAN 1933 STEEL BEAMS  
 179'-4" BK TO BK ABUTMENTS  
 SKEWED 45° LEFT FORWARD

LOCATION MAP  
APPROXIMATE SCALE



GROSS LENGTH = 770.00 FT. = 0.146 MILE  
 NET LENGTH = 770.00 FT. = 0.146 MILE

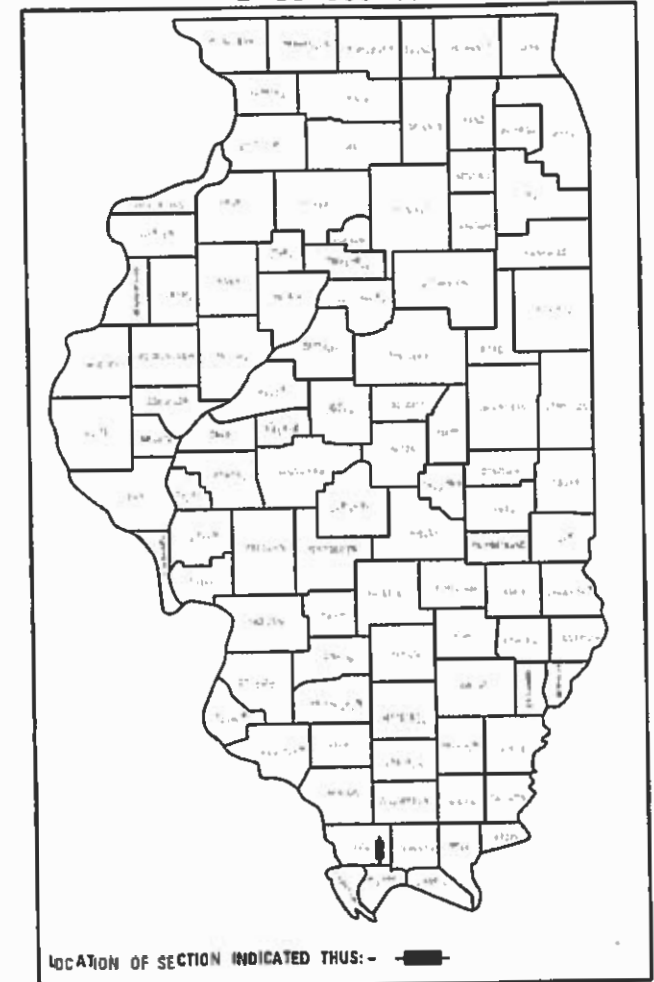


E<PIR ES 11-30-21

Signature of Eric L. Henkel

10-07-19  
DATE

D-99-003-16



LOCATION OF SECTION INDICATED THUS: - [Symbol]

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SUBMITTED 10-12-2019  
 [Signature]  
 REGION FIVE ENGINEER

Dec 6 2019  
 [Signature]  
 ENGINEER OF DESIGN AND ENVIRONMENT

Dec 6 2019  
 [Signature]  
 DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY  
 OF THE STATE OF ILLINOIS







| SUMMARY OF QUANTITIES |                                   |       |                   | CONSTRUCTION CODE                   |                                       |                                       |
|-----------------------|-----------------------------------|-------|-------------------|-------------------------------------|---------------------------------------|---------------------------------------|
|                       |                                   |       |                   | 90% FEDERAL<br>10% STATE<br>ROADWAY | 90% FEDERAL<br>10% STATE<br>STRUCTURE | 90% FEDERAL<br>10% STATE<br>STRUCTURE |
| CODE<br>NO.           | ITEM                              | UNIT  | TOTAL<br>QUANTITY | 0004                                | 0010                                  | 0010                                  |
|                       |                                   |       |                   | ROADWAY                             | S.N. 091-0077                         | S.N. 091-0078                         |
| 20100500              | TREE REMOVAL, ACRES               | ACRE  | 0.25              | 0.25                                |                                       |                                       |
| 20200100              | EARTH EXCAVATION                  | CU YD | 8295              | 8295                                |                                       |                                       |
| 20800150              | TRENCH BACKFILL                   | CU YD | 99                | 99                                  |                                       |                                       |
| 25000210              | SEEDING, CLASS 2A                 | ACRE  | 4.5               | 4.5                                 |                                       |                                       |
| 25000350              | SEEDING, CLASS 7                  | ACRE  | 4.5               | 4.5                                 |                                       |                                       |
| 25000400              | NITROGEN FERTILIZER NUTRIENT      | POUND | 405               | 405                                 |                                       |                                       |
| 25000500              | PHOSPHORUS FERTILIZER NUTRIENT    | POUND | 405               | 405                                 |                                       |                                       |
| 25000600              | POTASSIUM FERTILIZER NUTRIENT     | POUND | 405               | 405                                 |                                       |                                       |
| 25000700              | AGRICULTURAL GROUND LIMESTONE     | TON   | 9.0               | 9.0                                 |                                       |                                       |
| 25100115              | MULCH, METHOD 2                   | ACRE  | 9.00              | 9.00                                |                                       |                                       |
| 25100630              | EROSION CONTROL BLANKET           | SQ YD | 463               | 463                                 |                                       |                                       |
| 28000250              | TEMPORARY EROSION CONTROL SEEDING | POUND | 900               | 900                                 |                                       |                                       |
| 28000305              | TEMPORARY DITCH CHECKS            | FOOT  | 1015              | 1015                                |                                       |                                       |
| 28000400              | PERIMETER EROSION BARRIER         | FOOT  | 478               | 478                                 |                                       |                                       |

MODEL: PLOT01  
FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\DOT78504-eth-scp01.dgn



USER NAME = jmk  
ESCA PROJECT NO. 1259.11  
PLOT SCALE = 0.1667' / in.  
PLOT DATE = 10/15/2019

DESIGNED - SKM  
DRAWN - SKM  
CHECKED - ELH  
DATE - 10/19

REVISED -  
REVISED -  
REVISED -  
REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SCALE: NONE      SHEET NO. 1 OF 9      SHEETS      STA.      TO STA.

| F.A.I.<br>RTE.     | SECTION   | COUNTY | TOTAL<br>SHEETS           | SHEET<br>NO. |
|--------------------|-----------|--------|---------------------------|--------------|
| 57                 | (91-3)B-2 | UNION  | 161                       | 4            |
| CONTRACT NO. 78504 |           |        | ILLINOIS FED. AID PROJECT |              |

| SUMMARY OF QUANTITIES |   |       |                   | CONSTRUCTION CODE                   |                                       |                                       |
|-----------------------|---|-------|-------------------|-------------------------------------|---------------------------------------|---------------------------------------|
|                       |   |       |                   | 90% FEDERAL<br>10% STATE<br>ROADWAY | 90% FEDERAL<br>10% STATE<br>STRUCTURE | 90% FEDERAL<br>10% STATE<br>STRUCTURE |
| CODE<br>NO.           | ITEM  | UNIT  | TOTAL<br>QUANTITY | 0004                                | 0010                                  | 0010                                  |
|                       |   |       |                   | ROADWAY                             | S.N. 091-0077                         | S.N. 091-0078                         |
| 28000500              | INLET AND PIPE PROTECTION                           | EACH  | 9                 | 9                                   |                                       |                                       |
| 28100109              | STONE RIPRAP, CLASS A5                              | SQ YD | 5353              |                                     |                                       | 5353                                  |
| 28200200              | FILTER FABRIC                                       | SQ YD | 5387              | 34                                  |                                       | 5353                                  |
| 31100100              | SUBBASE GRANULAR MATERIAL, TYPE A                   | TON   | 70                | 70                                  |                                       |                                       |
| 31100300              | SUBBASE GRANULAR MATERIAL, TYPE A 4"                | SQ YD | 370               | 370                                 |                                       |                                       |
| 31100700              | SUBBASE GRANULAR MATERIAL, TYPE A 8"                | SQ YD | 3655              | 3655                                |                                       |                                       |
| 31100910              | SUBBASE GRANULAR MATERIAL, TYPE A 12"               | SQ YD | 1803              | 1803                                |                                       |                                       |
| 31200100              | STABILIZED SUBBASE 4"                               | SQ YD | 1803              | 1803                                |                                       |                                       |
| 40600290              | BITUMINOUS MATERIALS (TACK COAT)                    | POUND | 1008              | 1008                                |                                       |                                       |
| 42000080              | PAVEMENT CONNECTOR (PCC) FOR BRIDGE APPROACH SLAB   | SQ YD | 694               | 694                                 |                                       |                                       |
| 42000516              | PORTLAND CEMENT CONCRETE PAVEMENT 10 3/4" (JOINTED) | SQ YD | 842               | 842                                 |                                       |                                       |
| 42001300              | PROTECTIVE COAT                                     | SQ YD | 2503              | 2503                                |                                       |                                       |
| 44000100              | PAVEMENT REMOVAL                                    | SQ YD | 7087              | 7087                                |                                       |                                       |
| 44004250              | PAVED SHOULDER REMOVAL                              | SQ YD | 1452              | 1452                                |                                       |                                       |

MODEL: PLOT02  
 FILE NAME: C:\Users\skm\Desktop\1259\_11\PEH\Highway\CADD\_Sheets\0978504-ht-ss01.dgn



|                            |
|----------------------------|
| USER NAME = skm            |
| ESCA PROJECT NO. 1259.11   |
| PLOT SCALE = 0.1667' / in. |
| PLOT DATE = 10/11/2019     |

|                |
|----------------|
| DESIGNED - SKM |
| DRAWN - SKM    |
| CHECKED - ELH  |
| DATE - 10/19   |

|           |
|-----------|
| REVISED - |
| REVISED - |
| REVISED - |
| REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SCALE: NONE    SHEET NO. 2 OF 9 SHEETS    STA.    TO STA.

| F.A.I.<br>RTE.            | SECTION   | COUNTY | TOTAL<br>SHEETS | SHEET<br>NO. |
|---------------------------|-----------|--------|-----------------|--------------|
| 57                        | (91-3)B-2 | UNION  | 161             | 5            |
| CONTRACT NO. 78504        |           |        |                 |              |
| ILLINOIS FED. AID PROJECT |           |        |                 |              |



| SUMMARY OF QUANTITIES |  |       |                   | CONSTRUCTION CODE                   |                                       |                                       |
|-----------------------|--|-------|-------------------|-------------------------------------|---------------------------------------|---------------------------------------|
|                       |  |       |                   | 90% FEDERAL<br>10% STATE<br>ROADWAY | 90% FEDERAL<br>10% STATE<br>STRUCTURE | 90% FEDERAL<br>10% STATE<br>STRUCTURE |
| CODE<br>NO.           | ITEM                                       | UNIT  | TOTAL<br>QUANTITY | 0004                                | 0010                                  | 0010                                  |
|                       |  |       |                   | ROADWAY                             | S.N. 091-0077                         | S.N. 091-0078                         |
| 44200050              | WELDED WIRE REINFORCEMENT                  | SQ YD | 110               | 110                                 |                                       |                                       |
| 44200978              | CLASS B PATCHES, TYPE I, 11 INCH           | SQ YD | 10                | 10                                  |                                       |                                       |
| 44200982              | CLASS B PATCHES, TYPE II, 11 INCH          | SQ YD | 10                | 10                                  |                                       |                                       |
| 44200986              | CLASS B PATCHES, TYPE III, 11 INCH         | SQ YD | 60                | 60                                  |                                       |                                       |
| 44200988              | CLASS B PATCHES, TYPE IV, 11 INCH          | SQ YD | 50                | 50                                  |                                       |                                       |
| 44201299              | DOWEL BARS 1 1/2"                          | EACH  | 20                | 20                                  |                                       |                                       |
| 44213200              | SAW CUTS                                   | FOOT  | 150               | 150                                 |                                       |                                       |
| 44213204              | TIE BARS 3/4"                              | EACH  | 30                | 30                                  |                                       |                                       |
| 48101500              | AGGREGATE SHOULDERS, TYPE B 6"             | SQ YD | 1965              | 1965                                |                                       |                                       |
| 48203029              | HOT-MIX ASPHALT SHOULDERS, 8"              | SQ YD | 374               | 374                                 |                                       |                                       |
| 48300515              | PORTLAND CEMENT CONCRETE SHOULDERS 10 3/4" | SQ YD | 961               | 961                                 |                                       |                                       |
| 50100300              | REMOVAL OF EXISTING STRUCTURES NO. 1       | EACH  | 1                 |                                     | 1                                     |                                       |
| 50100400              | REMOVAL OF EXISTING STRUCTURES NO. 2       | EACH  | 1                 |                                     |                                       | 1                                     |
| 50104400              | CONCRETE HEADWALL REMOVAL                  | EACH  | 5                 | 5                                   |                                       |                                       |

MODEL: PLOT03  
 FILE NAME: C:\Users\skm\Desktop\1259\_11\SEHighway\CADD\_Sheets\0978504-ht-soq01.dgn



USER NAME = skm  
 ESCA PROJECT NO. 1259.11  
 PLOT SCALE = 0.1667' / in.  
 PLOT DATE = 10/11/2019

DESIGNED - SKM  
 DRAWN - SKM  
 CHECKED - ELH  
 DATE - 10/19

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SCALE: NONE    SHEET NO. 3 OF 9    SHEETS    STA.    TO STA.

| F.A.I.<br>RTE.     | SECTION   | COUNTY           | TOTAL<br>SHEETS | SHEET<br>NO. |
|--------------------|-----------|------------------|-----------------|--------------|
| 57                 | (91-3)B-2 | UNION            | 161             | 6            |
| CONTRACT NO. 78504 |           |                  |                 |              |
| ILLINOIS           |           | FED. AID PROJECT |                 |              |

| SUMMARY OF QUANTITIES |  |       |                   | CONSTRUCTION CODE                   |                                       |                                       |
|-----------------------|--|-------|-------------------|-------------------------------------|---------------------------------------|---------------------------------------|
|                       |  |       |                   | 90% FEDERAL<br>10% STATE<br>ROADWAY | 90% FEDERAL<br>10% STATE<br>STRUCTURE | 90% FEDERAL<br>10% STATE<br>STRUCTURE |
| CODE<br>NO.           | ITEM                                     | UNIT  | TOTAL<br>QUANTITY | 0004                                | 0010                                  | 0010                                  |
|                       |  |       |                   | ROADWAY                             | S.N. 091-0077                         | S.N. 091-0078                         |
| 50105220              | PIPE CULVERT REMOVAL                     | FOOT  | 1006              | 1006                                |                                       |                                       |
| 50200100              | STRUCTURE EXCAVATION                     | CU YD | 894               |                                     | 427                                   | 467                                   |
| 50300100              | FLOOR DRAINS                             | EACH  | 22                |                                     | 18                                    | 4                                     |
| 50300225              | CONCRETE STRUCTURES                      | CU YD | 605.4             |                                     | 308.8                                 | 296.6                                 |
| 50300255              | CONCRETE SUPERSTRUCTURE                  | CU YD | 678.3             |                                     | 361.0                                 | 317.3                                 |
| 50300300              | PROTECTIVE COAT                          | SQ YD | 2783              |                                     | 1470                                  | 1313                                  |
| 50301350              | CONCRETE SUPERSTRUCTURE (APPROACH SLAB)  | CU YD | 249               |                                     | 124.5                                 | 124.5                                 |
| 50500105              | FURNISHING AND ERECTING STRUCTURAL STEEL | L SUM | 1                 |                                     | 0.6                                   | 0.4                                   |
| 50500505              | STUD SHEAR CONNECTORS                    | EACH  | 14094             |                                     | 7560                                  | 6534                                  |
| 50800105              | REINFORCEMENT BARS                       | POUND | 59610             |                                     | 32010                                 | 27600                                 |
| 50800205              | REINFORCEMENT BARS, EPOXY COATED         | POUND | 396050            |                                     | 204850                                | 191200                                |
| 50800515              | BAR SPLICERS                             | EACH  | 444               |                                     | 216                                   | 228                                   |
| 50800530              | MECHANICAL SPLICERS                      | EACH  | 128               |                                     | 128                                   | 0                                     |
| 51202100              | FURNISHING STEEL PILES HP14X117          | FOOT  | 1885              |                                     | 1014                                  | 871                                   |

MODEL: PLOT04  
FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\0978504-eth-ssq01.dgn



USER NAME = jmk  
ESCA PROJECT NO. 1259.11  
PLOT SCALE = 0.1667' / in.  
PLOT DATE = 10/15/2019

DESIGNED - SKM  
DRAWN - SKM  
CHECKED - ELH  
DATE - 10/19

REVISED -  
REVISED -  
REVISED -  
REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SCALE: NONE SHEET NO. 4 OF 9 SHEETS STA. TO STA.

|                    |           |        |                           |              |
|--------------------|-----------|--------|---------------------------|--------------|
| F.A.I.<br>RTE.     | SECTION   | COUNTY | TOTAL<br>SHEETS           | SHEET<br>NO. |
| 57                 | (91-3)B-2 | UNION  | 161                       | 7            |
| CONTRACT NO. 78504 |           |        | ILLINOIS FED. AID PROJECT |              |

| SUMMARY OF QUANTITIES |   |       |                   | CONSTRUCTION CODE                   |                                       |                                       |
|-----------------------|---|-------|-------------------|-------------------------------------|---------------------------------------|---------------------------------------|
|                       |   |       |                   | 90% FEDERAL<br>10% STATE<br>ROADWAY | 90% FEDERAL<br>10% STATE<br>STRUCTURE | 90% FEDERAL<br>10% STATE<br>STRUCTURE |
| CODE<br>NO.           | ITEM  | UNIT  | TOTAL<br>QUANTITY | 0004                                | 0010                                  | 0010                                  |
|                       |   |       |                   | ROADWAY                             | S.N. 091-0077                         | S.N. 091-0078                         |
| 51202305              | DRIVING PILES                                       | FOOT  | 1885              |                                     | 1014                                  | 871                                   |
| 51204650              | PILE SHOES  | EACH  | 52                |                                     | 26                                    | 26                                    |
| 51500100              | NAME PLATES   | EACH  | 2                 |                                     | 1                                     | 1                                     |
| * 51603000            | DRILLED SHAFT IN SOIL                               | CU YD | 312.5             |                                     | 170.7                                 | 141.8                                 |
| * 51604000            | DRILLED SHAFT IN ROCK                               | CU YD | 130.5             |                                     | 65.7                                  | 64.8                                  |
| 52100520              | ANCHOR BOLTS, 1"                                    | EACH  | 96                |                                     | 48                                    | 48                                    |
| 5421D015              | PIPE CULVERTS, CLASS D, TYPE 1 15" (TEMPORARY)      | FOOT  | 334               | 334                                 |                                       |                                       |
| 5421D018              | PIPE CULVERTS, CLASS D, TYPE 1 18" (TEMPORARY)      | FOOT  | 176               | 176                                 |                                       |                                       |
| 54213663              | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 18" | EACH  | 2                 | 2                                   |                                       |                                       |
| 54244405              | FLUSH INLET BOX FOR MEDIAN, STANDARD 542546         | EACH  | 2                 | 2                                   |                                       |                                       |
| 54248510              | CONCRETE COLLAR                                     | CU YD | 0.3               | 0.3                                 |                                       |                                       |
| 54262712              | METAL FLARED END SECTIONS 12"                       | EACH  | 4                 | 4                                   |                                       |                                       |
| 550A0090              | STORM SEWERS, CLASS A, TYPE 1 18"                   | FOOT  | 155               | 155                                 |                                       |                                       |
| 58600101              | GRANULAR BACKFILL FOR STRUCTURES                    | CU YD | 432               |                                     | 211                                   | 221                                   |

\* SPECIALTY ITEM

MODEL\_PLOTS  
FILE NAME: C:\Users\skm\Desktop\1259\_11\PEH\Highway\CADD\_Sheets\0978504-ht-ss01.dgn



USER NAME = skm  
ESCA PROJECT NO. 1259.11  
PLOT SCALE = 0.1667' / in.  
PLOT DATE = 10/11/2019

DESIGNED - SKM  
DRAWN - SKM  
CHECKED - ELH  
DATE - 10/19

REVISED -  
REVISED -  
REVISED -  
REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NONE SHEET NO. 5 OF 9 SHEETS STA. TO STA.

| F.A.I.<br>RTE.     | SECTION   | COUNTY           | TOTAL<br>SHEETS | SHEET<br>NO. |
|--------------------|-----------|------------------|-----------------|--------------|
| 57                 | (91-3)B-2 | UNION            | 161             | 8            |
| CONTRACT NO. 78504 |           |                  |                 |              |
| ILLINOIS           |           | FED. AID PROJECT |                 |              |



| SUMMARY OF QUANTITIES |   |        |                   | CONSTRUCTION CODE                   |                                       |                                       |
|-----------------------|---|--------|-------------------|-------------------------------------|---------------------------------------|---------------------------------------|
|                       |   |        |                   | 90% FEDERAL<br>10% STATE<br>ROADWAY | 90% FEDERAL<br>10% STATE<br>STRUCTURE | 90% FEDERAL<br>10% STATE<br>STRUCTURE |
| CODE<br>NO.           | ITEM  | UNIT   | TOTAL<br>QUANTITY | 0004<br>ROADWAY                     | 0010<br>S.N. 091-0077                 | 0010<br>S.N. 091-0078                 |
| 59100100              | GEOCOMPOSITE WALL DRAIN                               | SQ YD  | 226               |                                     | 112                                   | 114                                   |
| 60100060              | CONCRETE HEADWALLS FOR PIPE DRAINS                    | EACH   | 5                 | 5                                   |                                       |                                       |
| 60100945              | PIPE DRAINS 12"                                       | FOOT   | 143               | 143                                 |                                       |                                       |
| 60108100              | PIPE UNDERDRAINS 4" (SPECIAL)                         | FOOT   | 122               | 122                                 |                                       |                                       |
| 60260100              | INLETS TO BE ADJUSTED                                 | EACH   | 1                 | 1                                   |                                       |                                       |
| 61000050              | CONCRETE THRUST BLOCKS                                | EACH   | 2                 | 2                                   |                                       |                                       |
| 61000225              | TYPE F INLET BOX, STANDARD 610001                     | EACH   | 2                 | 2                                   |                                       |                                       |
| 61000335              | TYPE G INLET BOX, STANDARD 610001                     | EACH   | 2                 | 2                                   |                                       |                                       |
| * 63100085            | TRAFFIC BARRIER TERMINAL, TYPE 6                      | EACH   | 4                 | 4                                   |                                       |                                       |
| 63200310              | GUARDRAIL REMOVAL                                     | FOOT   | 312               | 312                                 |                                       |                                       |
| * 63301210            | REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL, TYPE A | FOOT   | 1025              | 1025                                |                                       |                                       |
| 63500105              | DELINEATORS   | EACH   | 8                 | 8                                   |                                       |                                       |
| 64200116              | SHOULDER RUMBLE STRIPS, 16 INCH                       | FOOT   | 1233              | 1233                                |                                       |                                       |
| 67000400              | ENGINEER'S FIELD OFFICE, TYPE A                       | CAL MO | 29                | 29                                  |                                       |                                       |

\* SPECIALTY ITEM

MODEL: PLOT06  
FILE NAME: Y:\0011259-11\78504\CADD\Highway\CADD\_Sheets\0978504-shr\_sqr01.dgn



|                            |                |           |
|----------------------------|----------------|-----------|
| USER NAME = jmk            | DESIGNED - SKM | REVISED - |
| ESCA PROJECT NO. 1259.11   | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.1667' / in. | CHECKED - ELH  | REVISED - |
| PLOT DATE = 10/15/2019     | DATE - 10/19   | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NONE SHEET NO. 6 OF 9 SHEETS STA. TO STA.

| F.A.I.<br>RTE.            | SECTION   | COUNTY | TOTAL<br>SHEETS    | SHEET<br>NO. |
|---------------------------|-----------|--------|--------------------|--------------|
| 57                        | (91-3)B-2 | UNION  | 161                | 9            |
|                           |           |        | CONTRACT NO. 78504 |              |
| ILLINOIS FED. AID PROJECT |           |        |                    |              |

| SUMMARY OF QUANTITIES |   |        |                   | CONSTRUCTION CODE                   |                                       |                                       |
|-----------------------|---|--------|-------------------|-------------------------------------|---------------------------------------|---------------------------------------|
|                       |   |        |                   | 90% FEDERAL<br>10% STATE<br>ROADWAY | 90% FEDERAL<br>10% STATE<br>STRUCTURE | 90% FEDERAL<br>10% STATE<br>STRUCTURE |
| CODE<br>NO.           | ITEM  | UNIT   | TOTAL<br>QUANTITY | 0004                                | 0010                                  | 0010                                  |
|                       |   |        |                   | ROADWAY                             | S.N. 091-0077                         | S.N. 091-0078                         |
| 67100100              | MOBILIZATION                                    | L SUM  | 1                 | 1                                   |                                       |                                       |
| 70100420              | TRAFFIC CONTROL AND PROTECTION, STANDARD 701411 | EACH   | 1                 | 1                                   |                                       |                                       |
| 70100800              | TRAFFIC CONTROL AND PROTECTION, STANDARD 701401 | L SUM  | 1                 | 1                                   |                                       |                                       |
| 70103815              | TRAFFIC CONTROL SURVEILLANCE                    | CAL DA | 25                | 25                                  |                                       |                                       |
| 70107025              | CHANGEABLE MESSAGE SIGN                         | CAL DA | 112               | 112                                 |                                       |                                       |
| 70300100              | SHORT TERM PAVEMENT MARKING                     | FOOT   | 3084              | 3084                                |                                       |                                       |
| 70300150              | SHORT TERM PAVEMENT MARKING REMOVAL             | SQ FT  | 1028              | 1028                                |                                       |                                       |
| 70300220              | TEMPORARY PAVEMENT MARKING - LINE 4"            | FOOT   | 41615             | 41615                               |                                       |                                       |
| 70400100              | TEMPORARY CONCRETE BARRIER                      | FOOT   | 19675             | 19675                               |                                       |                                       |
| 70400500              | TEMPORARY CONCRETE BARRIER, STATE OWNED         | FOOT   | 14600             | 14600                               |                                       |                                       |
| 70500665              | TEMPORARY TRAFFIC BARRIER TERMINAL, TYPE 6      | EACH   | 2                 | 2                                   |                                       |                                       |
| * 72400600            | RELOCATE SIGN PANEL ASSEMBLY - TYPE B           | EACH   | 1                 | 1                                   |                                       |                                       |
| * 72501000            | TERMINAL MARKER - DIRECT APPLIED                | EACH   | 5                 | 5                                   |                                       |                                       |
| * 78009004            | MODIFIED URETHANE PAVEMENT MARKING - LINE 4"    | FOOT   | 39960             | 39960                               |                                       |                                       |

\* SPECIALTY ITEM

MODEL: PLOT07  
FILE NAME: C:\Users\skm\Desktop\1259\_11\PEH\Highway\CADD\_Sheets\0978504-ht-so01.dgn



|                            |
|----------------------------|
| USER NAME = skm            |
| ESCA PROJECT NO. 1259.11   |
| PLOT SCALE = 0.1667' / in. |
| PLOT DATE = 10/11/2019     |

|                |
|----------------|
| DESIGNED - SKM |
| DRAWN - SKM    |
| CHECKED - ELH  |
| DATE - 10/19   |

|           |
|-----------|
| REVISED - |
| REVISED - |
| REVISED - |
| REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SCALE: NONE    SHEET NO. 7 OF 9 SHEETS    STA.    TO STA.

|                    |           |        |                           |              |
|--------------------|-----------|--------|---------------------------|--------------|
| F.A.I.<br>RTE.     | SECTION   | COUNTY | TOTAL<br>SHEETS           | SHEET<br>NO. |
| 57                 | (91-3)B-2 | UNION  | 161                       | 10           |
| CONTRACT NO. 78504 |           |        | ILLINOIS FED. AID PROJECT |              |

| SUMMARY OF QUANTITIES |   |       |                   | CONSTRUCTION CODE                   |                                       |                                       |
|-----------------------|---|-------|-------------------|-------------------------------------|---------------------------------------|---------------------------------------|
|                       |   |       |                   | 90% FEDERAL<br>10% STATE<br>ROADWAY | 90% FEDERAL<br>10% STATE<br>STRUCTURE | 90% FEDERAL<br>10% STATE<br>STRUCTURE |
| CODE<br>NO.           | ITEM  | UNIT  | TOTAL<br>QUANTITY | 0004<br>ROADWAY                     | 0010<br>S.N. 091-0077                 | 0010<br>S.N. 091-0078                 |
| * 78100100            | RAISED REFLECTIVE PAVEMENT MARKER                     | EACH  | 13                | 13                                  |                                       |                                       |
| 78100200              | TEMPORARY RAISED REFLECTIVE PAVEMENT MARKER           | EACH  | 1198              | 1198                                |                                       |                                       |
| 78100300              | REPLACEMENT REFLECTOR                                 | EACH  | 151               | 151                                 |                                       |                                       |
| * 78200005            | GUARDRAIL REFLECTORS, TYPE A                          | EACH  | 14                | 14                                  |                                       |                                       |
| * 78200006            | GUARDRAIL REFLECTORS, TYPE B                          | EACH  | 4132              | 4132                                |                                       |                                       |
| X0301993              | REMOVE AND REINSTALL CONCRETE HEADWALL FOR PIPE DRAIN | EACH  | 3                 | 3                                   |                                       |                                       |
| X0320051              | CROSSHOLE SONIC LOGGING ACCESS DUCTS                  | FOOT  | 480               |                                     | 248                                   | 232                                   |
| X0320052              | CROSSHOLE SONIC LOGGING TESTING                       | EACH  | 16                |                                     | 8                                     | 8                                     |
| X0323265              | REMOVE EXISTING RIPRAP                                | SQ YD | 54                | 54                                  |                                       |                                       |
| X0325299              | INLET MODIFICATION                                    | EACH  | 1                 | 1                                   |                                       |                                       |
| X0327980              | PAVEMENT MARKING REMOVAL - WATER BLASTING             | SQ FT | 13320             | 13320                               |                                       |                                       |
| X0484300              | MEDIAN INLET BOX REMOVAL                              | EACH  | 2                 | 2                                   |                                       |                                       |
| *** X2503100          | MOWING  | UNIT  | 16                | 16                                  |                                       |                                       |
| X2800500              | INLET PROTECTION, SPECIAL                             | EACH  | 1                 | 1                                   |                                       |                                       |

\* SPECIALTY ITEM      \*\*\* 100% STATE

REV. - MS

MODEL PLOT08  
FILE NAME: Y:\PLOT011259-11\_78504\CADD\Highway\CADD\_Sheets\0978504-eth-scp01.dgn



|                            |                |           |
|----------------------------|----------------|-----------|
| USER NAME = kah            | DESIGNED - SKM | REVISED - |
| ESCA PROJECT NO. 1259.11   | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.1667' / in. | CHECKED - ELH  | REVISED - |
| PLOT DATE = 10/17/2019     | DATE - 10/19   | REVISED - |

|                |           |
|----------------|-----------|
| DESIGNED - SKM | REVISED - |
| DRAWN - SKM    | REVISED - |
| CHECKED - ELH  | REVISED - |
| DATE - 10/19   | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NONE      SHEET NO. 8 OF 9 SHEETS      STA.      TO STA.

|                    |           |        |                           |              |
|--------------------|-----------|--------|---------------------------|--------------|
| F.A.I.<br>RTE.     | SECTION   | COUNTY | TOTAL<br>SHEETS           | SHEET<br>NO. |
| 57                 | (91-3)B-2 | UNION  | 161                       | 11           |
| CONTRACT NO. 78504 |           |        | ILLINOIS FED. AID PROJECT |              |



| SUMMARY OF QUANTITIES |   |       |                   | CONSTRUCTION CODE                   |                                       |                                       |
|-----------------------|---|-------|-------------------|-------------------------------------|---------------------------------------|---------------------------------------|
|                       |   |       |                   | 90% FEDERAL<br>10% STATE<br>ROADWAY | 90% FEDERAL<br>10% STATE<br>STRUCTURE | 90% FEDERAL<br>10% STATE<br>STRUCTURE |
| CODE<br>NO.           | ITEM  | UNIT  | TOTAL<br>QUANTITY | 0004<br>ROADWAY                     | 0010<br>S.N. 091-0077                 | 0010<br>S.N. 091-0078                 |
| X5030250              | BRIDGE DECK GROOVING (LONGITUDINAL)                                   | SQ YD | 1998              |                                     | 1053                                  | 945                                   |
| X5420350              | END SECTIONS 18" SPECIAL  | EACH  | 2                 | 2                                   |                                       |                                       |
| X5421550              | METAL END SECTIONS 15", SPECIAL                                       | EACH  | 1                 | 1                                   |                                       |                                       |
| X6020073              | INLETS, TYPE A, TYPE 8 GRATE, TEMPORARY                               | EACH  | 3                 | 3                                   |                                       |                                       |
| X6024875              | TEMPORARY INLET   | EACH  | 1                 | 1                                   |                                       |                                       |
| * X6330103            | REMOVE AND RE-ERECT TRAFFIC BARRIER TERMINAL, TYPE 1 SPECIAL, TANGENT | EACH  | 5                 | 5                                   |                                       |                                       |
| X7010216              | TRAFFIC CONTROL AND PROTECTION, (SPECIAL)                             | L SUM | 1                 | 1                                   |                                       |                                       |
| X7030005              | TEMPORARY PAVEMENT MARKING REMOVAL                                    | SQ FT | 13872             | 13872                               |                                       |                                       |
| X7050169              | TEMPORARY TRAFFIC BARRIER TERMINAL, TYPE 1, SPECIAL (FLARED)          | EACH  | 2                 | 2                                   |                                       |                                       |
| X7830050              | RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL                  | EACH  | 151               | 151                                 |                                       |                                       |
| Z0016001              | DECK SLAB REPAIR (FULL DEPTH, TYPE I)                                 | SQ YD | 10                |                                     | 8                                     | 2                                     |
| Z0016002              | DECK SLAB REPAIR (FULL DEPTH, TYPE II)                                | SQ YD | 40                |                                     | 24                                    | 16                                    |
| Z0029090              | DIAMOND GRINDING (BRIDGE SECTION)                                     | SQ YD | 2732              |                                     | 1425                                  | 1307                                  |
| ∅ Z0076600            | TRAINEES  | HOUR  | 1000              | 1000                                |                                       |                                       |
| Z0046304              | PIPE UNDERDRAINS FOR STRUCTURES 4"                                    | FOOT  | 348               |                                     | 174                                   | 174                                   |
| ∅ Z0076604            | TRAINEES TRAINING PROGRAM GRADUATE                                    | HOUR  | 1000              | 1000                                |                                       |                                       |
| Z0062456              | TEMPORARY PAVEMENT  | SQ YD | 3081              | 3081                                |                                       |                                       |

\* SPECIALTY ITEM ∅ 0042

REV. - MS

MODEL: PLOT09  
FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\DP978504-eth-ssq01.dgn



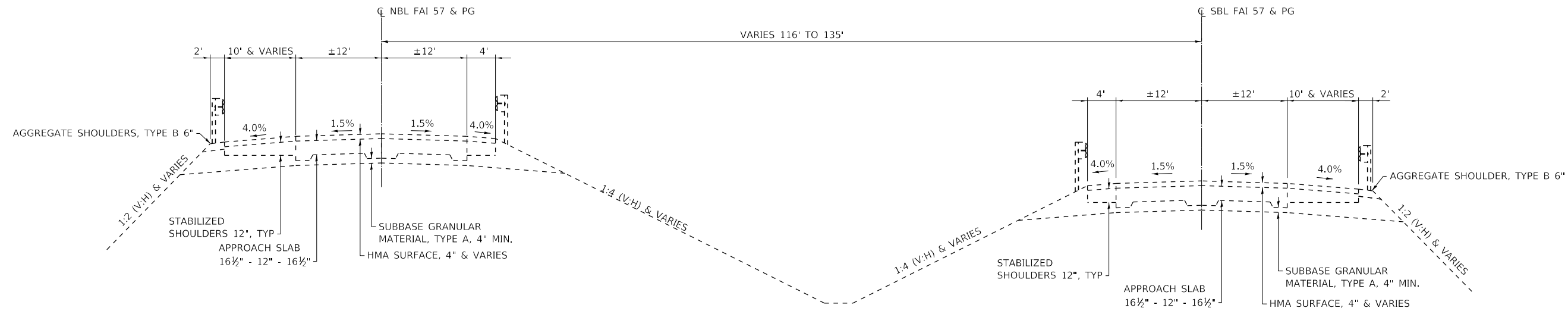
|                            |                |           |
|----------------------------|----------------|-----------|
| USER NAME = kah            | DESIGNED - SKM | REVISED - |
| ESCA PROJECT NO. 1259.11   | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.1667' / in. | CHECKED - ELH  | REVISED - |
| PLOT DATE = 10/17/2019     | DATE - 10/19   | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

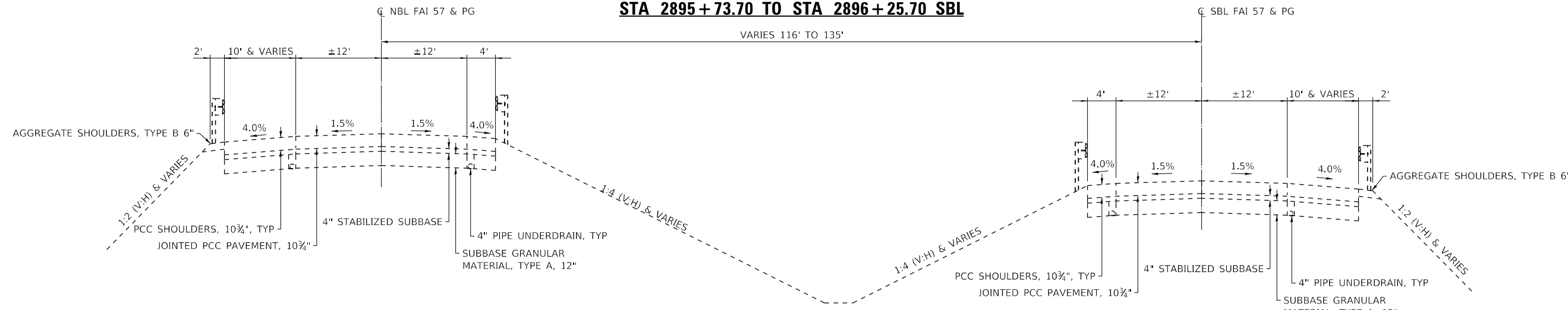
SUMMARY OF QUANTITIES

SCALE: NONE SHEET NO. 9 OF 9 SHEETS STA. TO STA.

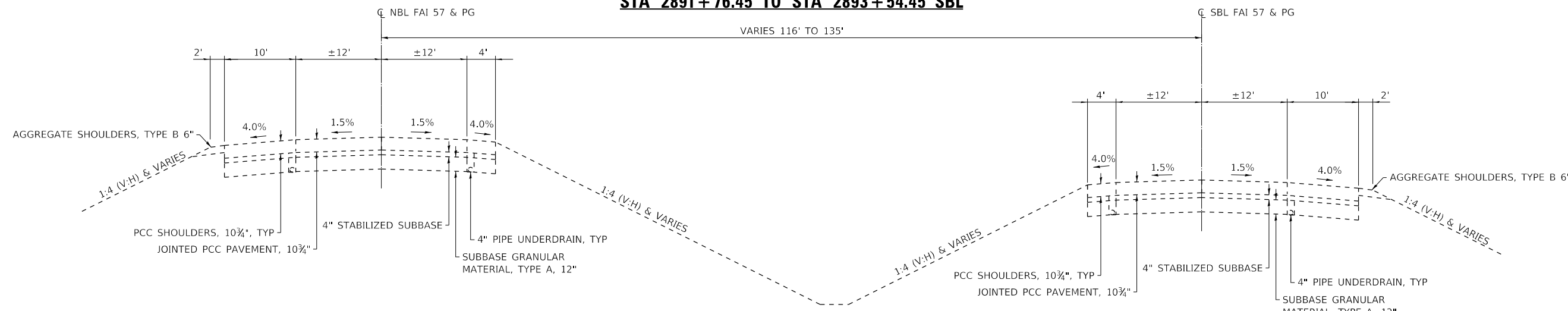
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|----------------|-----------|----------|--------------------|--------------|
| F.A.I.<br>RTE. | SECTION   | COUNTY   | TOTAL<br>SHEETS    | SHEET<br>NO. |
| 57             | (91-3)B-2 | UNION    | 161                | 12           |
|                |           |          | CONTRACT NO. 78504 |              |
|                |           | ILLINOIS | FED. AID PROJECT   |              |



**EXISTING TYPICAL SECTION**  
**STA 2894+65.50 TO STA 2895+17.50 NBL**  
**STA 2893+54.45 TO STA 2894+06.45 SBL**  
**STA 2897+12.50 TO STA 2897+64.50 NBL**  
**STA 2895+73.70 TO STA 2896+25.70 SBL**



**EXISTING TYPICAL SECTION**  
**STA 2897+64.50 TO STA 2899+42.50 NBL**  
**STA 2891+76.45 TO STA 2893+54.45 SBL**



**EXISTING TYPICAL SECTION**  
**STA 2892+87.50 TO STA 2894+65.50 NBL**  
**STA 2896+25.70 TO STA 2898+03.70 SBL**

MODEL\_PLOT FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\978504-Hwy-1259-11.dgn



USER NAME = jmk  
 ESCA PROJECT NO. 1259.11  
 PLOT SCALE = 0.1667' / in.  
 PLOT DATE = 10/7/2019

DESIGNED - SKM  
 DRAWN - SKM  
 CHECKED - ELH  
 DATE - 04/19

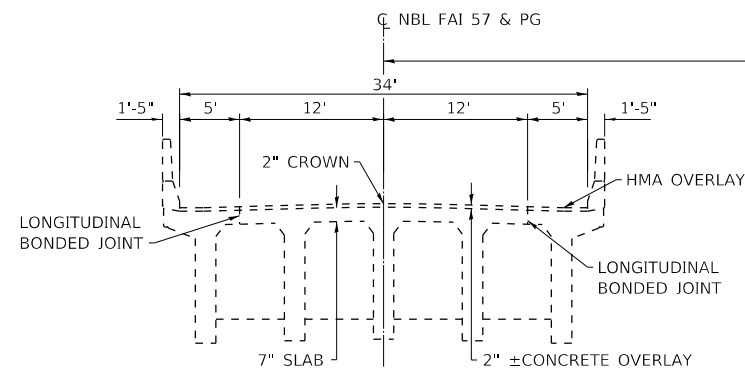
REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

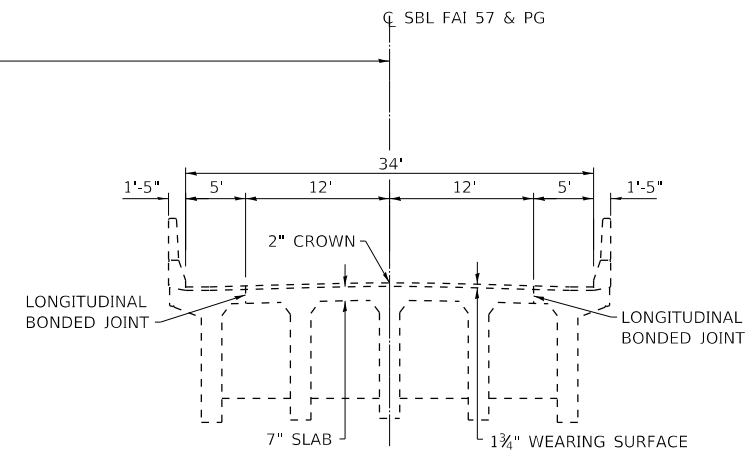
**I-57 TYPICAL SECTIONS**

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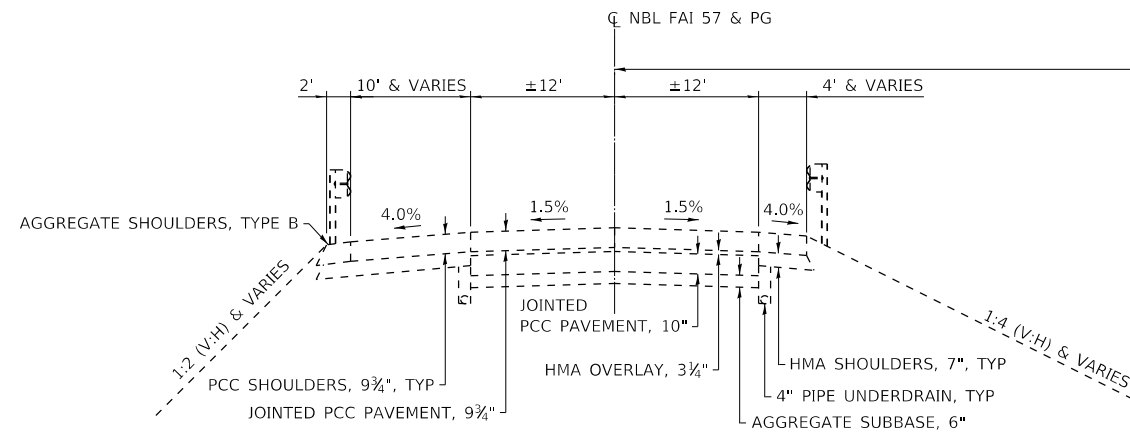
| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 13        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |



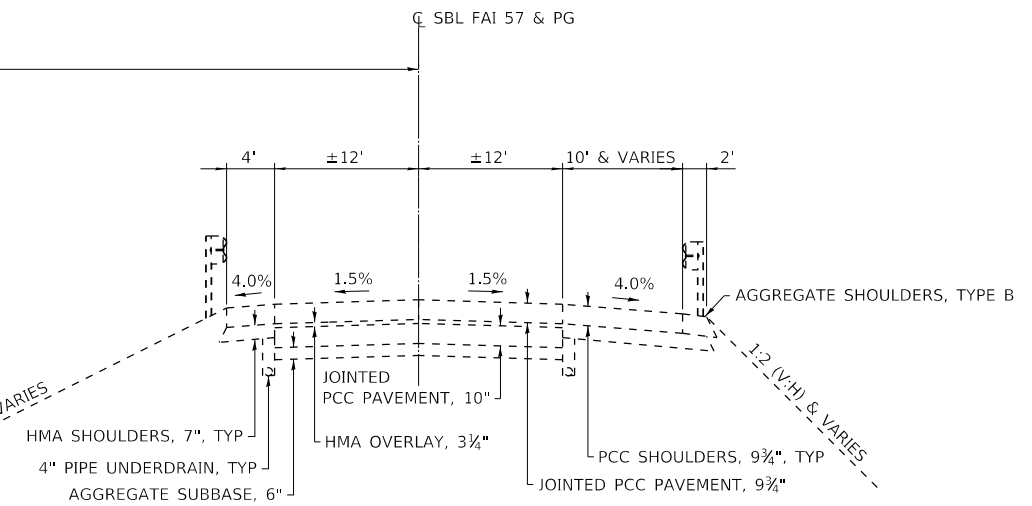
VARIES 122' TO 128'



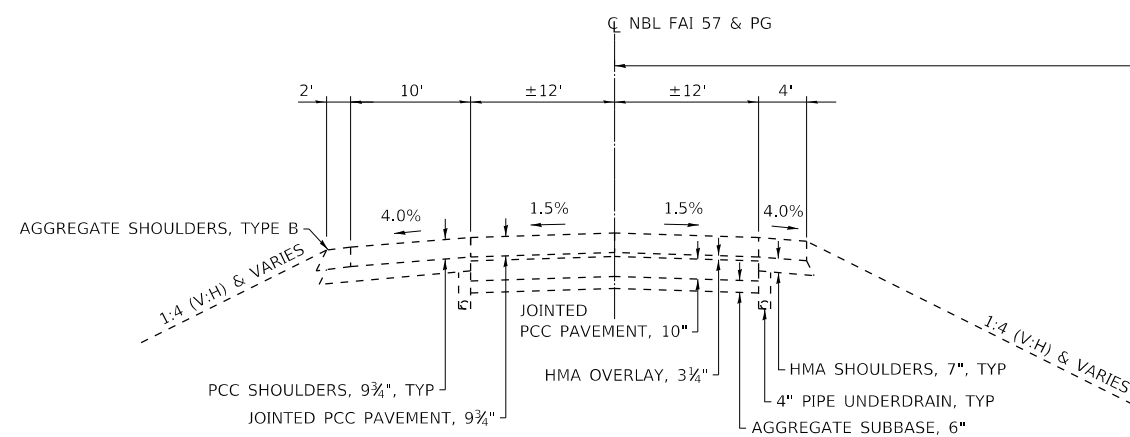
**EXISTING TYPICAL SECTION**  
**STA 2895+17.50 TO STA 2897+12.50 NBL**  
**STA 2894+06.45 TO STA 2895+73.70 SBL**



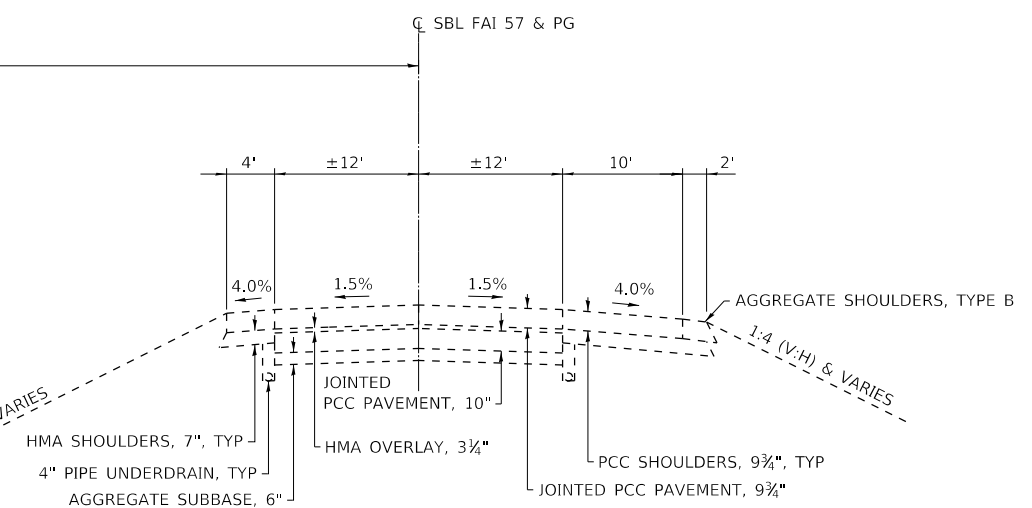
VARIES 116' TO 135'



**EXISTING TYPICAL SECTION**  
**STA 2899+42.50 TO STA 2903+00.00 NBL**  
**STA 2891+00.00 TO STA 2891+76.45 SBL**



VARIES 116' TO 135'



**EXISTING TYPICAL SECTION**  
**STA 2891+00.00 TO STA 2892+87.50 NBL**  
**STA 2898+03.70 TO STA 2900+00.00 SBL**

MODEL PLOT  
 FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\0978504-11-Highway\02.dgn



|                            |                |           |
|----------------------------|----------------|-----------|
| USER NAME = jmk            | DESIGNED - SKM | REVISED - |
| ESCA PROJECT NO. 1259.11   | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.1667' / in. | CHECKED - ELH  | REVISED - |
| PLOT DATE = 10/7/2019      | DATE - 08/19   | REVISED - |

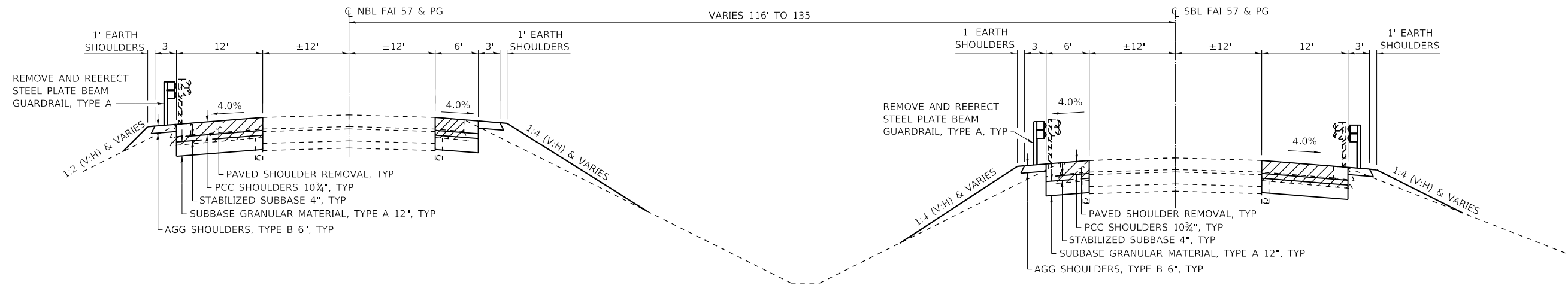
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

I-57 TYPICAL SECTIONS

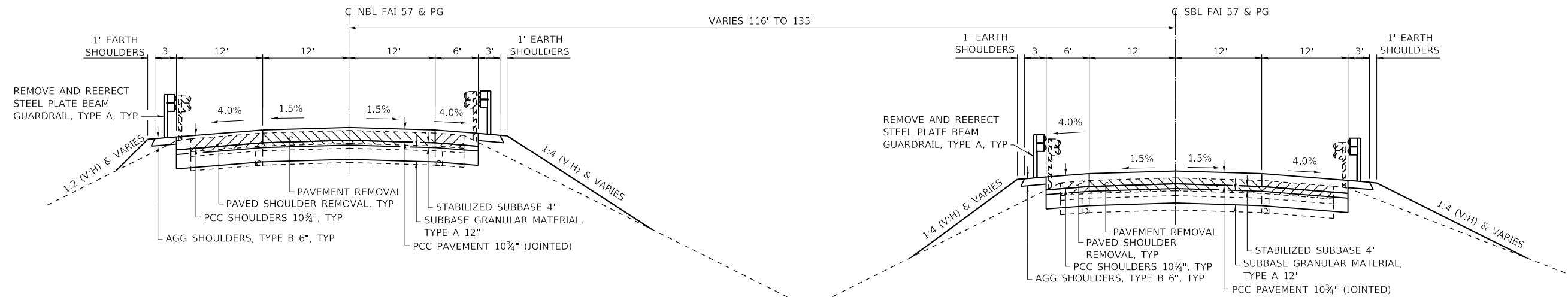
SCALE: NONE SHEET NO. 2 OF 4 SHEETS STA. TO STA.

| F.A.I. RTE.        | SECTION   | COUNTY | TOTAL SHEETS              | SHEET NO. |
|--------------------|-----------|--------|---------------------------|-----------|
| 57                 | (91-3)B-2 | UNION  | 161                       | 14        |
| CONTRACT NO. 78504 |           |        | ILLINOIS FED. AID PROJECT |           |





**PROPOSED TYPICAL SECTION**  
**STA 2899+50.00 TO STA 2900+90.00 NBL**  
**STA 2891+14.00 TO STA 2891+80.00 SBL**



**PROPOSED TYPICAL SECTION**  
**STA 2897+86.72 TO STA 2899+50.00 NBL**  
**STA 2891+80.00 TO STA 2893+32.31 SBL**

MODEL\_PLOT  
 FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\978504-11-Highway\1259-11-78504.dwg



USER NAME = jmk  
 ESCA PROJECT NO. 1259.11  
 PLOT SCALE = 0.1667' / in.  
 PLOT DATE = 10/7/2019

DESIGNED - SKM  
 DRAWN - SKM  
 CHECKED - ELH  
 DATE - 10/19

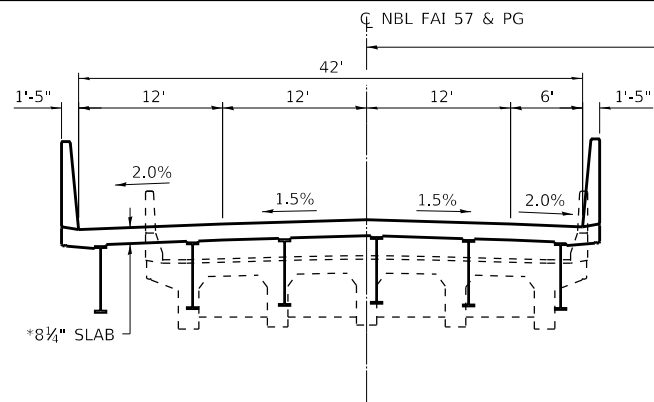
REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

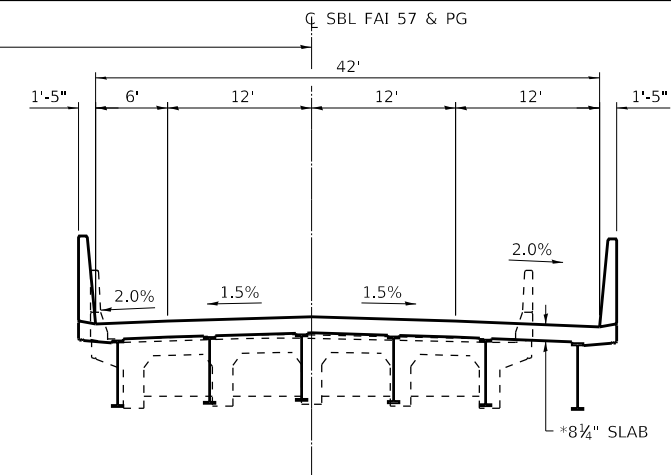
**I-57 TYPICAL SECTIONS**

SCALE: NONE SHEET NO. 3 OF 4 SHEETS STA. TO STA.

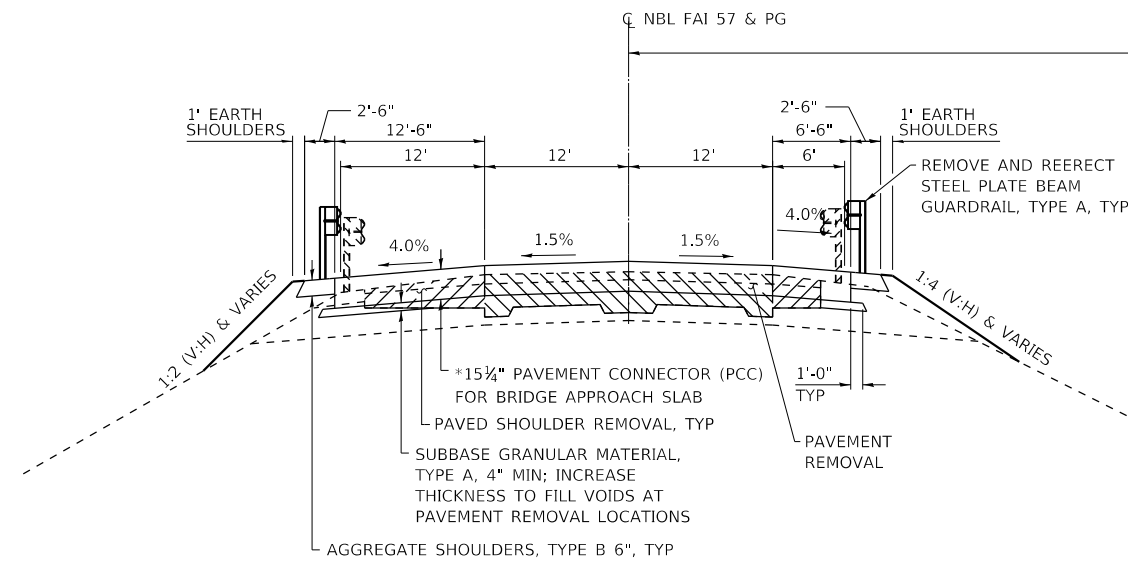
| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 15        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |



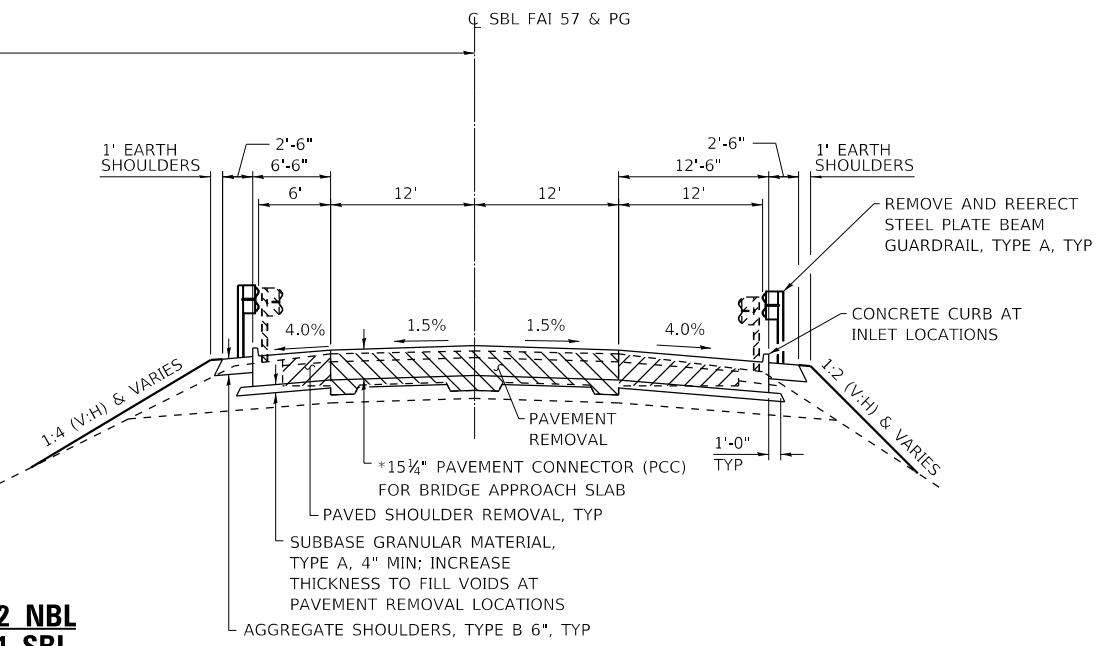
VARIES 122' TO 128'



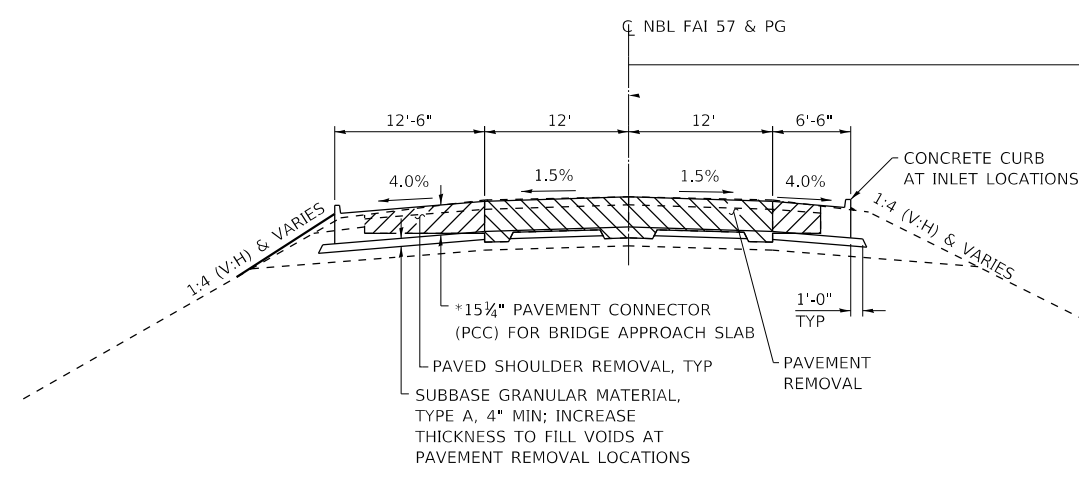
**PROPOSED TYPICAL SECTION**  
**STA 2895+11.38 TO STA 2897+18.63 NBL**  
**STA 2894+00.40 TO STA 2895+79.74 SBL**



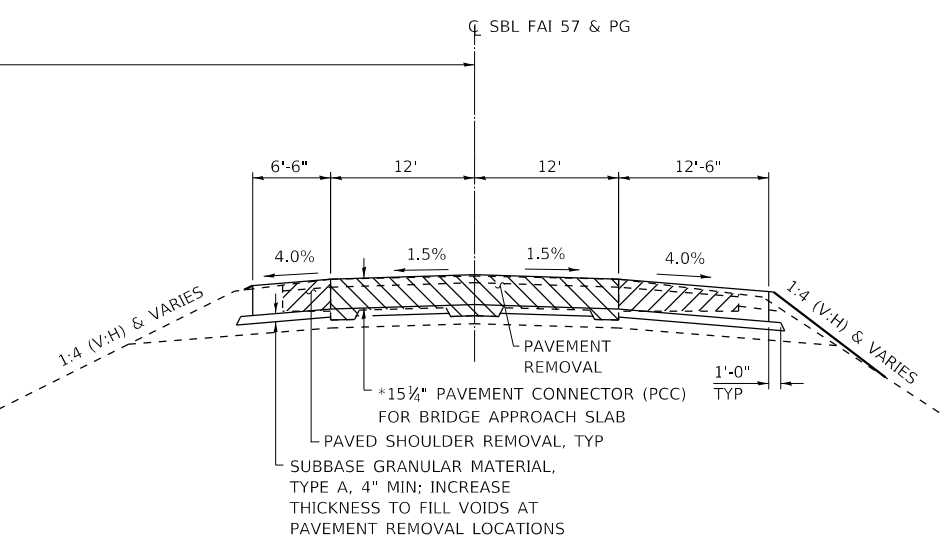
VARIES 116' TO 135'



**PROPOSED TYPICAL SECTION**  
**STA 2897+47.22 TO STA 2897+86.72 NBL**  
**STA 2893+32.31 TO STA 2893+71.81 SBL**



VARIES 116' TO 135'



**PROPOSED TYPICAL SECTION**  
**STA 2894+49.29 TO STA 2894+82.79 NBL**  
**STA 2896+08.32 TO STA 2896+42.00 SBL**

\*PRIOR TO GRINDING

MODEL: I-57  
 FILE: I-57.DWG  
 USER: jmk  
 DATE: 10/7/2019



|                            |                |           |
|----------------------------|----------------|-----------|
| USER NAME = jmk            | DESIGNED - SKM | REVISED - |
| ESCA PROJECT NO. 1259.11   | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.1667' / in. | CHECKED - ELH  | REVISED - |
| PLOT DATE = 10/7/2019      | DATE - 08/19   | REVISED - |

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

I-57 TYPICAL SECTIONS

SCALE: NONE SHEET NO. 4 OF 4 SHEETS STA. TO STA.

|                           |           |        |              |           |
|---------------------------|-----------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
| 57                        | (91-3)B-2 | UNION  | 161          | 16        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |

| EARTHWORK SCHEDULE            |                  |  |            |   |
|-------------------------------|------------------|--|------------|---|
| LOCATION                      | EARTH EXCAVATION | EXCAVATION TO BE USED IN EMBANKMENT (ADJUSTED FOR SHRINKAGE) | EMBANKMENT | EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) |
|                               | CU YD            | CU YD  | CU YD      | CU YD                                       |
| SOUTH CROSSOVERS CONSTRUCTION | 2530             | 1898   | 1750       | +148  |
| I-57 NB (LT)                  | 2245             | 1684   | 480        | +1204                                       |
| I-57 SB (RT)                  | 1930             | 1448   | 1040       | +408  |
| NORTH CROSSOVERS REMOVAL      | 1590             | 1193   | 1210       | -17   |
|                               |                  |  |            |   |
|                               |                  |  |            |   |
|                               |                  |  |            |   |
| TOTALS                        | 8295             | 6223   | 4480       | +1743                                       |

| TREE REMOVAL SCHEDULE |                     |
|-----------------------|---------------------|
| LOCATION              | TREE REMOVAL, ACRES |
|                       | ACRE                |
| STA 2893+50, RT       | 0.05                |
| STA 2895+30, RT       | 0.10                |
| STA 2896+00, LT       | 0.05                |
| STA 2897+50, LT       | 0.05                |
|                       |                     |
|                       |                     |
|                       |                     |
| TOTALS                | 0.25                |

EXCAVATION TO BE USED IN EMBANKMENT = EARTH EXCAVATION \* 0.75

① CONTRACTOR SHALL EXCAVATE FROM SOUTH CROSSOVERS OR AS DIRECTED BY THE ENGINEER TO PROVIDE FOR EMBANKMENT NECESSARY TO RETURN NORTH CROSSOVERS TO ORIGINAL CONDITION.

| STORM SEWER SCHEDULE |  |                               |                                   |   |                 |                        |                                   |                                   |                          |
|----------------------|--|-------------------------------|-----------------------------------|---|-----------------|------------------------|-----------------------------------|-----------------------------------|--------------------------|
| LOCATION             | FLUSH INLET BOX FOR MEDIAN STANDARD 542546 | METAL FLARED END SECTIONS 12" | STORM SEWERS, CLASS A, TYPE 1 18" | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 18" | PIPE DRAINS 12" | CONCRETE THRUST BLOCKS | TYPE F INLET BOX, STANDARD 610001 | TYPE G INLET BOX, STANDARD 610001 | MEDIAN INLET BOX REMOVAL |
|                      | EACH                                       | EACH                          | FOOT                              | EACH  | FOOT            | EACH                   | EACH                              | EACH                              | EACH                     |
| STA 2893+40          |  | 1                             |                                   |   | 27              | 1                      | 1                                 |                                   |                          |
| STA 2893+41          |  | 1                             |                                   |   | 38              |                        |                                   | 1                                 |                          |
| STA 2894+23          | 1  |                               |                                   |   |                 |                        |                                   |                                   | 1                        |
| STA 2894+50          |  |                               | 77                                |   |                 |                        |                                   |                                   |                          |
| STA 2894+59          |  | 2                             |                                   |   | 78              | 1                      | 1                                 | 1                                 |                          |
| STA 2896+50          |  |                               | 78                                | 1   |                 |                        |                                   |                                   |                          |
| STA 2896+80          | 1  |                               |                                   | 1   |                 |                        |                                   |                                   | 1                        |
|                      |  |                               |                                   |   |                 |                        |                                   |                                   |                          |
|                      |  |                               |                                   |   |                 |                        |                                   |                                   |                          |
| TOTALS               | 2  | 4                             | 155                               | 2   | 143             | 2                      | 2                                 | 2                                 | 2                        |

| MEDIAN CROSSOVERS SCHEDULE |                 |                                      |                                |                    |  |  |                            |   |                                |                 |                    |                           |                  |                 |          |            |                           |          |
|----------------------------|-----------------|--------------------------------------|--------------------------------|--------------------|--|--|----------------------------|---|--------------------------------|-----------------|--------------------|---------------------------|------------------|-----------------|----------|------------|---------------------------|----------|
| LOCATION                   | TRENCH BACKFILL | SUBBASE GRANULAR MATERIAL, TYPE A 8" | AGGREGATE SHOULDERS, TYPE B 6" | TEMPORARY PAVEMENT | PIPE CULVERTS, CLASS D, TYPE 1 15" (TEMPORARY) | PIPE CULVERTS, CLASS D, TYPE 1 18" (TEMPORARY) | END SECTIONS 18" (SPECIAL) | INLETS, TYPE A, TYPE 8 GRATE, TEMPORARY | METAL END SECTIONS 15" SPECIAL | TEMPORARY INLET | INLET MODIFICATION | INLET PROTECTION, SPECIAL | PAVEMENT REMOVAL | CLASS B PATCHES | TIE BARS | DOWEL BARS | WELDED WIRE REINFORCEMENT | SAW CUTS |
|                            | CU YD           | SQ YD                                | SQ YD                          | SQ YD              | FOOT   | FOOT   | EACH                       | EACH                                    | EACH                           | EACH            | EACH               | EACH                      | SQ YD            | SQ YD           | EACH     | EACH       | SQ YD                     | FOOT     |
| NORTH CROSSOVERS           |                 |                                      |                                |                    | 13   |  |                            |   | 1                              |                 |                    |                           |                  | 130             | 30       | 20         | 110                       | 150      |
| SOUTH CROSSOVERS           | 99              | 3655                                 | 676                            | 3081               | 321  | 176  | 2                          | 3                                       |                                | 1               | 1                  | 1                         | 3081             |                 |          |            |                           |          |
|                            |                 |                                      |                                |                    |  |  |                            |   |                                |                 |                    |                           |                  |                 |          |            |                           |          |
| TOTALS                     | 99              | 3655                                 | 676                            | 3081               | 334  | 176  | 2                          | 3                                       | 1                              | 1               | 1                  | 1                         | 5458             | 130             | 30       | 20         | 110                       | 150      |

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| USER NAME = jmk            | DESIGNED - SKM | REVISED - |
| ESCA PROJECT NO. 1259.11   | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.1667' / in. | CHECKED - ELH  | REVISED - |
| PLOT DATE = 10/15/2019     | DATE - 10/19   | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SCHEDULES OF QUANTITIES

SCALE: NONE SHEET NO. 1 OF 4 SHEETS STA. TO STA.

|                    |           |                  |              |           |
|--------------------|-----------|------------------|--------------|-----------|
| F.A.I. RTE.        | SECTION   | COUNTY           | TOTAL SHEETS | SHEET NO. |
| 57                 | (91-3)B-2 | UNION            | 161          | 17        |
| CONTRACT NO. 78504 |           |                  |              |           |
| ILLINOIS           |           | FED. AID PROJECT |              |           |

| PCC PAVING SCHEDULE        |   |                 |                                       |                       |                  |
|----------------------------|---|-----------------|---------------------------------------|-----------------------|------------------|
| LOCATION                   | PORTLAND CEMENT CONCRETE PAVEMENT 10 3/4" (JOINTED) | PROTECTIVE COAT | SUBBASE GRANULAR MATERIAL, TYPE A 12" | STABILIZED SUBBASE 4" | PAVEMENT REMOVAL |
|                            | SQ YD   | SQ YD           | SQ YD                                 | SQ YD                 | SQ YD            |
| NBL STA 2897+87 TO 2899+50 | 436   | 436             | 436                                   | 436                   | 437              |
| SBL STA 2891+80 TO 2893+32 | 406   | 406             | 406                                   | 406                   | 410              |
|                            |   |                 |                                       |                       |                  |
|                            |   |                 |                                       |                       |                  |
| TOTALS                     | 842   | 842             | 842                                   | 842                   | 847              |

| SEEDING SCHEDULE   |                   |                  |                              |                                |                               |                               |                 |
|--------------------|-------------------|------------------|------------------------------|--------------------------------|-------------------------------|-------------------------------|-----------------|
| LOCATION           | SEEDING, CLASS 2A | SEEDING, CLASS 7 | NITROGEN FERTILIZER NUTRIENT | PHOSPHORUS FERTILIZER NUTRIENT | POTASSIUM FERTILIZER NUTRIENT | AGRICULTURAL GROUND LIMESTONE | MULCH, METHOD 2 |
|                    | ACRE              | ACRE             | POUND                        | POUND                          | POUND                         | TON                           | ACRE            |
| NORTHEAST QUADRANT | 0.04              | 0.04             | 4                            | 4                              | 4                             | 0.08                          | 0.08            |
| NORTHWEST QUADRANT | 0.07              | 0.07             | 6                            | 6                              | 6                             | 0.14                          | 0.14            |
| SOUTHEAST QUADRANT | 0.10              | 0.10             | 9                            | 9                              | 9                             | 0.20                          | 0.20            |
| SOUTHWEST QUADRANT | 0.03              | 0.03             | 3                            | 3                              | 3                             | 0.06                          | 0.06            |
| NORTH MEDIAN       | 0.12              | 0.12             | 11                           | 11                             | 11                            | 0.24                          | 0.24            |
| SOUTH MEDIAN       | 0.10              | 0.10             | 9                            | 9                              | 9                             | 0.20                          | 0.20            |
| NORTH CROSSOVERS   | 2.04              | 2.04             | 183                          | 183                            | 183                           | 4.08                          | 4.08            |
| SOUTH CROSSOVERS   | 2.00              | 2.00             | 180                          | 180                            | 180                           | 4.00                          | 4.00            |
|                    |                   |                  |                              |                                |                               |                               |                 |
| TOTALS             | 4.50              | 4.50             | 405                          | 405                            | 405                           | 9.00                          | 9.00            |

| BRIDGE APPROACH SCHEDULE   |                                     |   |                 |                  |
|----------------------------|-------------------------------------|---|-----------------|------------------|
| LOCATION                   | SUBBASE GRANULAR MATERIAL, TYPE A ① | PAVEMENT CONNECTOR (PCC) FOR BRIDGE APPROACH SLAB | PROTECTIVE COAT | PAVEMENT REMOVAL |
|                            | TON                                 | SQ YD   | SQ YD           | SQ YD            |
| SN 091-0077 NORTH APPROACH | 14                                  | 172   | 173             | 186              |
| SN 091-0077 SOUTH APPROACH | 26                                  | 175   | 177             | 203              |
| SN 091-0078 NORTH APPROACH | 19                                  | 172   | 173             | 200              |
| SN 091-0078 SOUTH APPROACH | 11                                  | 175   | 177             | 193              |
|                            |                                     |   |                 |                  |
| TOTALS                     | 70                                  | 694   | 700             | 782              |

① 4" SUBBASE GRANULAR MATERIAL, TYPE A COST INCLUDED IN PAVEMENT CONNECTOR (PCC) FOR BRIDGE APPROACH SLAB PER HIGHWAY STANDARD 420401. QUANTITY SHOWN IS ADDITIONAL SUBBASE MATERIAL, TYPE A NECESSARY TO FILL VOID FROM PAVEMENT REMOVAL AND TO FACILITATE GRADE RAISE, SEE PAVEMENT TRANSITION SHEET FOR MORE DETAIL.

| SHOULDER SCHEDULE                |                                  |                                |                                      |                              |                       |                 |                       |                                       |                                 |                        |
|----------------------------------|----------------------------------|--------------------------------|--------------------------------------|------------------------------|-----------------------|-----------------|-----------------------|---------------------------------------|---------------------------------|------------------------|
| LOCATION                         | BITUMINOUS MATERIALS (TACK COAT) | AGGREGATE SHOULDERS, TYPE B 6" | SUBBASE GRANULAR MATERIAL, TYPE A 4" | 8" HOT-MIX ASPHALT SHOULDERS | PCC SHOULDERS 10 3/4" | PROTECTIVE COAT | STABILIZED SUBBASE 4" | SUBBASE GRANULAR MATERIAL, TYPE A 12" | SHOULDER RUMBLE STRIPS, 16 INCH | PAVED SHOULDER REMOVAL |
|                                  | POUND                            | SQ YD                          | SQ YD                                | SQ YD                        | SQ YD                 | SQ YD           | SQ YD                 | SQ YD                                 | FOOT                            | SQ YD                  |
| SN 091-0077 NORTHEAST CORNER     |                                  |                                |                                      |                              |                       |                 |                       |                                       | 47                              | 83                     |
| SN 091-0077 NORTHWEST CORNER     |                                  |                                |                                      |                              |                       |                 |                       |                                       | 21                              | 27                     |
| SN 091-0077 SOUTHEAST CORNER     |                                  | 106                            |                                      |                              | 405                   | 405             | 405                   | 405                                   | 330                             | 326                    |
| SN 091-0077 SOUTHWEST CORNER     |                                  | 98                             |                                      |                              | 145                   | 145             | 145                   | 145                                   | 271                             | 145                    |
| SN 091-0078 NORTHEAST CORNER     |                                  | 96                             |                                      |                              | 146                   | 146             | 146                   | 146                                   | 271                             | 145                    |
| SN 091-0078 NORTHWEST CORNER     |                                  | 74                             |                                      |                              | 265                   | 265             | 265                   | 265                                   | 225                             | 246                    |
| SN 091-0078 SOUTHEAST CORNER     |                                  |                                |                                      |                              |                       |                 |                       |                                       | 21                              | 32                     |
| SN 091-0078 SOUTHWEST CORNER     |                                  |                                |                                      |                              |                       |                 |                       |                                       | 47                              | 90                     |
| MEDIAN CROSSOVER                 | 1008                             |                                | 370                                  | 374                          |                       |                 |                       |                                       |                                 | 358                    |
| RT STA 2721+50 TO RT STA 2726+65 |                                  | 115                            |                                      |                              |                       |                 |                       |                                       |                                 |                        |
| LT STA 2718+19 TO LT STA 2730+66 |                                  | 278                            |                                      |                              |                       |                 |                       |                                       |                                 |                        |
| RT STA 2903+00 TO RT STA 2914+50 |                                  | 255                            |                                      |                              |                       |                 |                       |                                       |                                 |                        |
| LT STA 2902+50 TO RT STA 2914+50 |                                  | 267                            |                                      |                              |                       |                 |                       |                                       |                                 |                        |
|                                  |                                  |                                |                                      |                              |                       |                 |                       |                                       |                                 |                        |
| TOTALS                           | 1008                             | 1289                           | 370                                  | 374                          | 961                   | 961             | 961                   | 961                                   | 1233                            | 1452                   |

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USER NAME = skm  
ESCA PROJECT NO. 1259.11  
PLOT SCALE = 0.1667' / in.  
PLOT DATE = 10/11/2019

DESIGNED - SKM  
DRAWN - SKM  
CHECKED - ELH  
DATE - 10/19

REVISED -  
REVISED -  
REVISED -  
REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SCHEDULES OF QUANTITIES

SCALE: NONE SHEET NO. 2 OF 4 SHEETS STA. TO STA.

| F.A.I. RTE.        | SECTION   | COUNTY | TOTAL SHEETS              | SHEET NO. |
|--------------------|-----------|--------|---------------------------|-----------|
| 57                 | (91-3)B-2 | UNION  | 161                       | 18        |
| CONTRACT NO. 78504 |           |        | ILLINOIS FED. AID PROJECT |           |

| PIPE UNDERDRAIN SCHEDULE |                           |                                    |                               |   |
|--------------------------|---------------------------|------------------------------------|-------------------------------|---|
| LOCATION                 | CONCRETE HEADWALL REMOVAL | CONCRETE HEADWALLS FOR PIPE DRAINS | PIPE UNDERDRAINS 4" (SPECIAL) | REMOVE AND REINSTALL CONCRETE HEADWALL FOR PIPE DRAIN |
|                          | EACH                      | EACH                               | FOOT                          | EACH  |
| STA 2893+51              | 1                         | 1                                  | 10                            |   |
| STA 2897+67              | 1                         | 1                                  | 10                            |   |
| STA 2904+96              | 1                         | 1                                  | 30                            | 1   |
| STA 2910+13              | 1                         | 1                                  | 45                            | 1   |
| STA 2911+15              | 1                         | 1                                  | 27                            | 1   |
| TOTALS                   | 5                         | 5                                  | 122                           | 3   |

| EROSION CONTROL SCHEDULE |                         |                                   |                           |                        |                           |               |
|--------------------------|-------------------------|-----------------------------------|---------------------------|------------------------|---------------------------|---------------|
| LOCATION                 | EROSION CONTROL BLANKET | TEMPORARY EROSION CONTROL SEEDING | PERIMETER EROSION BARRIER | TEMPORARY DITCH CHECKS | INLET AND PIPE PROTECTION | FILTER FABRIC |
|                          | SQ YD                   | POUND                             | FOOT                      | FOOT                   | EACH                      | SQ YD         |
| NORTHEAST QUADRANT       |                         | 8                                 |                           |                        |                           |               |
| NORTHWEST QUADRANT       |                         | 14                                |                           |                        |                           |               |
| SOUTHEAST QUADRANT       | 463                     | 20                                | 478                       |                        |                           |               |
| SOUTHWEST QUADRANT       |                         | 6                                 |                           |                        |                           |               |
| NORTH MEDIAN             |                         | 24                                |                           |                        | 1                         |               |
| SOUTH MEDIAN             |                         | 20                                |                           | 670                    | 2                         |               |
| NORTH CROSSOVERS         |                         | 408                               |                           | 160                    | 1                         |               |
| SOUTH CROSSOVERS         |                         | 400                               |                           | 185                    | 5                         | 34            |
| TOTALS                   | 463                     | 900                               | 478                       | 1015                   | 9                         | 34            |

| PAVEMENT MARKING SCHEDULE               |                             |        |                                     |  |        |                                   |                       |   |   |  |
|---|-----------------------------|--------|-------------------------------------|--|--------|-----------------------------------|-----------------------|---|---|--|
| LOCATION                                | SHORT TERM PAVEMENT MARKING |        | SHORT TERM PAVEMENT MARKING REMOVAL | MODIFIED URETHANE PAVEMENT MARKING - LINE 4" |        | RAISED REFLECTIVE PAVEMENT MARKER | REPLACEMENT REFLECTOR | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL | PAVEMENT MARKING REMOVAL - WATER BLASTING | RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL |
|   | FOOT                        |        | SQ FT                               | FOOT   |        | EACH                              | EACH                  | EACH                                      | SQ FT                                     | EACH   |
|   | WHITE                       | YELLOW |                                     | WHITE  | YELLOW |                                   |                       |   |   |  |
| NB I-57                                 |                             |        |                                     |  |        |                                   |                       |   |   |  |
| STA 2717+25 TO STA 2721+00              |                             |        |                                     | 400  | 375    |                                   |                       |   | 258                                       |  |
| STA 2727+00 TO STA 2905+50              |                             |        |                                     |  | 16617  |                                   |                       |   | 5539                                      |  |
| STA 2748+00 TO STA 2762+00              |                             |        |                                     | 360  |        |                                   | 18                    |   | 120                                       | 18   |
| STA 2894+49.29 TO STA 2899+50.00        | 76                          | 24     | 33                                  | 501  |        | 7                                 |                       | 7   | 167                                       |  |
| STA 2894.49.29 TO STA 2946+00           |                             |        |                                     | 1290   |        |                                   |                       |   | 430                                       |  |
| STA 2899+50 TO STA 2946+00 FOR STAGE II | 448                         | 668    | 372                                 |  |        |                                   | 59                    |   |   | 59   |
| FOR STAGE III                           | 236                         | 32     | 89                                  |  |        |                                   |                       |   |   |  |
| SB I-57                                 |                             |        |                                     |  |        |                                   |                       |   |   |  |
| STA 2679+00 TO STA 2727+00              |                             |        |                                     | 1210   |        |                                   | 61                    |   | 403                                       | 61   |
| STA 2719+00 TO STA 2720+00              |                             |        |                                     | 200  |        |                                   |                       |   | 68  |  |
| STA 2720+00 TO STA 2906+00              |                             |        |                                     |  | 17367  |                                   |                       |   | 5789                                      |  |
| STA 2882+00 TO STA 2891+80              |                             |        |                                     |  |        |                                   | 13                    |   |   | 13   |
| STA 2882+00 TO STA 2896+50              |                             |        |                                     | 370  |        |                                   |                       |   | 123                                       |  |
| STA 2891+80.00 TO STA 2896+42.00        | 68                          | 20     | 30                                  | 470  |        | 6                                 |                       | 6   | 157                                       |  |
| STA 2910+00 TO STA 2914+50              |                             |        |                                     |  | 800    |                                   |                       |   | 266                                       |  |
| FOR STAGE II                            | 276                         | 44     | 107                                 |  |        |                                   |                       |   |   |  |
| FOR STAGE III                           | 512                         | 680    | 397                                 |  |        |                                   |                       |   |   |  |
| SUBTOTALS                               | 1616                        | 1468   | 1028                                | 4801   | 35159  | 13                                | 151                   | 13  | 13320                                     | 151  |
| TOTALS                                  | 3084                        |        | 1028                                | 39960  |        | 13                                | 151                   | 13  | 13320                                     | 151  |

| GUARDRAIL SCHEDULE           |                   |             |   |                       |                                  |                              |  |   |
|------------------------------|-------------------|-------------|---|-----------------------|----------------------------------|------------------------------|--|---|
| LOCATION                     | GUARDRAIL REMOVAL | TBT, TYPE 6 | REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL, TYPE A | TEMPORARY TBT, TYPE 6 | TERMINAL MARKER - DIRECT APPLIED | GUARDRAIL REFLECTORS, TYPE A | REMOVE AND REERECT TRAFFIC BARRIER TERMINAL, TYPE 1 SPECIAL, TANGENT | TEMPORARY TBT, TYPE 1 SPECIAL, (FLARED) |
|                              | FOOT              | EACH        | FOOT  | EACH                  | EACH                             | EACH                         | EACH   | EACH                                    |
| SN 091-0077 SOUTHWEST CORNER | 43                | 1           | 187.5   |                       | 1                                | 3                            | 1  |   |
| SN 091-0077 SOUTHEAST CORNER | 58                | 1           | 300.0   |                       |                                  | 5                            |  |   |
| SN 091-0078 NORTHWEST CORNER | 43                | 1           | 125.0   |                       | 1                                | 3                            | 1  |   |
| SN 091-0078 NORTHEAST CORNER | 43                | 1           | 187.5   |                       | 1                                | 3                            | 1  |   |
| SN 091-0078 SOUTHEAST CORNER |                   |             |   | 1                     | 1                                |                              |  | 1                                       |
| NORTH CROSSOVERS             | 125               |             | 225.0   |                       |                                  |                              | 2  |   |
| SN 091-0005 NORTHWEST CORNER |                   |             |   | 1                     | 1                                |                              |  | 1                                       |
| TOTALS                       | 312               | 4           | 1025.0  | 2                     | 5                                | 14                           | 5  | 2                                       |

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| USER NAME = skm            | DESIGNED - SKM | REVISED - |
| ESCA PROJECT NO. 1259.11   | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.1667' / in. | CHECKED - ELH  | REVISED - |
| PLOT DATE = 10/11/2019     | DATE - 10/19   | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SCHEDULES OF QUANTITIES**

SCALE: NONE    SHEET NO. 3 OF 4    SHEETS    STA.    TO STA.

|                    |           |        |                           |           |
|--------------------|-----------|--------|---------------------------|-----------|
| F.A.I. RTE.        | SECTION   | COUNTY | TOTAL SHEETS              | SHEET NO. |
| 57                 | (91-3)B-2 | UNION  | 161                       | 19        |
| CONTRACT NO. 78504 |           |        | ILLINOIS FED. AID PROJECT |           |

| TEMPORARY PAVEMENT MARKING SCHEDULE |                                      |        |   |
|-------------------------------------|--------------------------------------|--------|---|
| LOCATION                            | TEMPORARY PAVEMENT MARKING - LINE 4" |        | TEMPORARY PAVEMENT MARKING REMOVAL<br>SQ FT |
|                                     | FOOT                                 |        |   |
|                                     | WHITE                                | YELLOW |   |
| AFTER STAGE II                      |                                      |        |   |
| NB I-57                             |                                      |        |   |
| STA 2727+00 TO STA 2905+50          |                                      | 16617  | 5539  |
| STA 2901+50 TO STA 2946+00          | 1120                                 |        | 373   |
| SB I-57                             |                                      |        |   |
| STA 2701+00 TO STA 2724+00          | 580                                  |        | 193   |
| STA 2719+00 TO STA 2720+00          | 200                                  |        | 67  |
| STA 2720+00 TO STA 2722+50          | 70                                   |        | 23  |
| STA 2720+00 TO STA 2725+00          |                                      | 500    | 167   |
| STA 2891+80 TO STA 2896+42          | 582                                  | 462    | 348   |
| STA 2910+00 TO STA 2914+50          |                                      | 450    | 150   |
| AFTER STAGE III                     |                                      |        |   |
| NB I-57                             |                                      |        |   |
| STA 2717+25 TO STA 2719+25          | 400                                  |        | 133   |
| STA 2717+25 TO STA 2721+00          |                                      | 375    | 125   |
| STA 2894+49 TO STA 2899+50          | 631                                  | 501    | 377   |
| STA 2911+00 TO STA 2914+50          |                                      | 350    | 117   |
| STA 2914+00 TO STA 2935+00          | 530                                  |        | 177   |
| SB I-57                             |                                      |        |   |
| STA 2679+00 TO STA 2727+00          | 1210                                 |        | 403   |
| STA 2720+00 TO STA 2722+50          | 70                                   |        | 24  |
| STA 2724+00 TO STA 2906+00          |                                      | 16967  | 5656  |
|                                     | SUBTOTALS                            | 5393   | 36222                                       |
|                                     | TOTALS                               | 41615  | 13872                                       |

| TEMPORARY CONCRETE BARRIER SCHEDULE |                            |   |                              |
|-------------------------------------|----------------------------|---|------------------------------|
| LOCATION                            | TEMPORARY CONCRETE BARRIER | TEMPORARY CONCRETE BARRIER, STATE OWNED | GUARDRAIL REFLECTORS, TYPE B |
|                                     | FOOT                       | FOOT                                    | EACH                         |
| STAGE II TRAFFIC CONTROL            |                            |   |                              |
| STA 2726+55 TO STA 2908+32          | 9650                       | 7300                                    | 2037                         |
| STAGE III TRAFFIC CONTROL           |                            |   |                              |
| STA 2722+59 TO STA 2907+17          | 10025                      | 7300                                    | 2082                         |
|                                     | SUBTOTALS                  | 19675                                   | 14600                        |
|                                     | TOTALS                     | 34275                                   | 4119                         |

| TEMPORARY RAISED REFLECTIVE PAVEMENT MARKER SCHEDULE |   |        |
|--|---|--------|
| LOCATION   | TEMPORARY RAISED REFLECTIVE PAVEMENT MARKER |        |
|  | EACH  |        |
|  | WHITE                                       | YELLOW |
| STAGE I  |   |        |
| NORTH TAPER, NORTH CROSSOVERS                        |   | 66     |
| SOUTH TAPER, NORTH CROSSOVERS                        |   | 66     |
| NORTH TAPER, SOUTH CROSSOVERS                        |   | 66     |
| SOUTH TAPER, SOUTH CROSSOVERS                        |   | 66     |
| STAGE II   |   |        |
| NORTH TAPER  | 51  |        |
| SOUTH TAPER  |   | 51     |
| NORTH CROSSOVERS                                     | 49  | 58     |
| SOUTH CROSSOVERS                                     | 67  | 67     |
| STAGE III  |   |        |
| NORTH TAPER  |   | 51     |
| SOUTH TAPER  | 51  |        |
| NORTH CROSSOVERS                                     | 44  | 55     |
| SOUTH CROSSOVERS                                     | 63  | 63     |
| STAGE IV   |   |        |
| NORTH TAPER, NORTH CROSSOVERS                        |   | 66     |
| SOUTH TAPER, NORTH CROSSOVERS                        |   | 66     |
| NORTH TAPER, SOUTH CROSSOVERS                        |   | 66     |
| SOUTH TAPER, SOUTH CROSSOVERS                        |   | 66     |
|  | SUBTOTALS                                   | 325    |
|  | TOTALS                                      | 1198   |

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USER NAME = skm  
 ESCA PROJECT NO. 1259.11  
 PLOT SCALE = 0.1667' / in.  
 PLOT DATE = 10/11/2019

DESIGNED - SKM  
 DRAWN - SKM  
 CHECKED - ELH  
 DATE - 10/19

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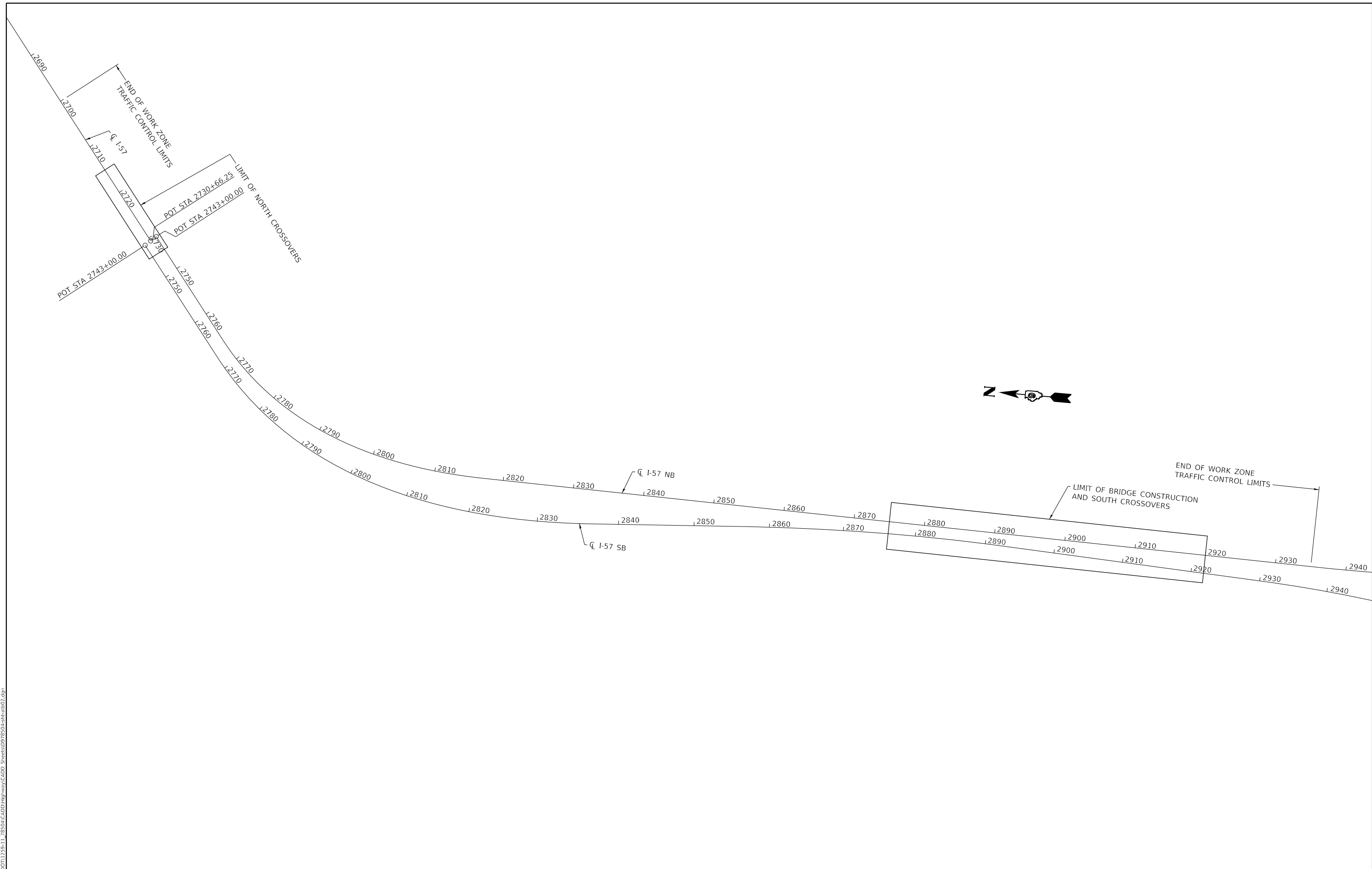
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SCHEDULES OF QUANTITIES

SCALE: NONE    SHEET NO. 4 OF 4    SHEETS    STA.    TO STA.

| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 20        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |

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| USER NAME = jmk            |
| ESCA PROJECT NO. 1259.11   |
| PLOT SCALE = 0.1667' / in. |
| PLOT DATE = 10/7/2019      |

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| DESIGNED - SKM |
| DRAWN - JPC    |
| CHECKED - ELH  |
| DATE - 06/19   |

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| REVISED - |
| REVISED - |

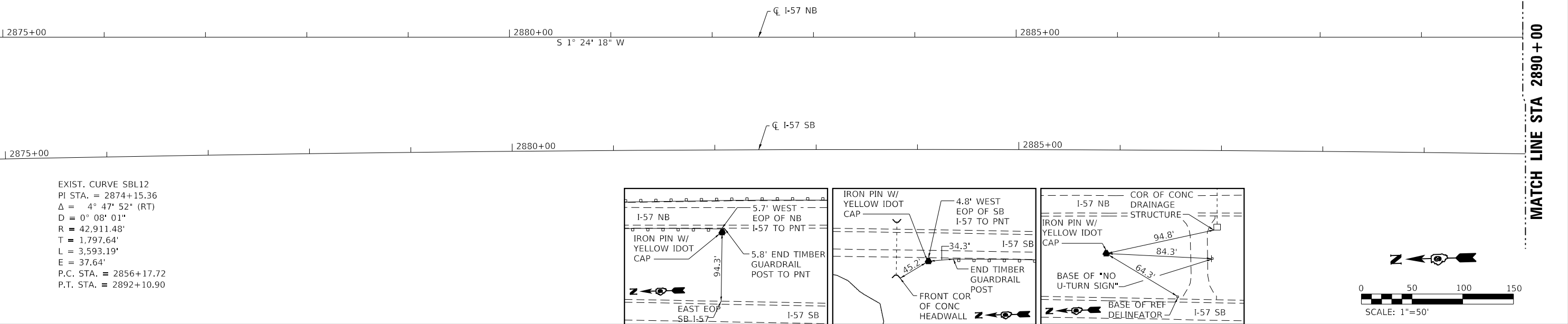
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ALIGNMENT, TIES, AND BENCHMARKS**

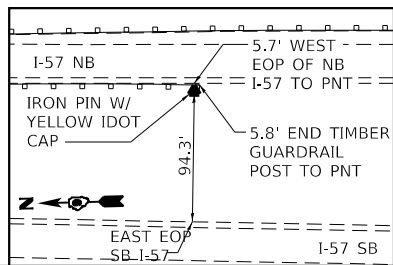
SCALE: NO SCALE    SHEET NO. 1 OF 3 SHEETS    STA. 2690+00.00 TO STA. 2940+00.00

|                |                   |              |                  |              |
|----------------|-------------------|--------------|------------------|--------------|
| F.A.I. RTE. 57 | SECTION (91-3)B-2 | COUNTY UNION | TOTAL SHEETS 161 | SHEET NO. 21 |
| ILLINOIS       |                   |              | FED. AID PROJECT |              |

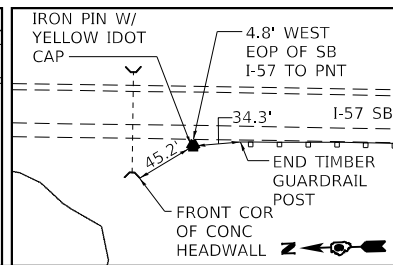
CONTRACT NO. 78504



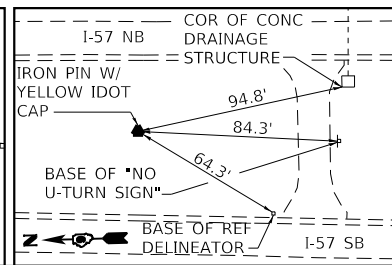
EXIST. CURVE SBL12  
 PI STA. = 2874+15.36  
 $\Delta = 4^\circ 47' 52''$  (RT)  
 $D = 0^\circ 08' 01''$   
 $R = 42,911.48'$   
 $T = 1,797.64'$   
 $L = 3,593.19'$   
 $E = 37.64'$   
 P.C. STA. = 2856+17.72  
 P.T. STA. = 2892+10.90



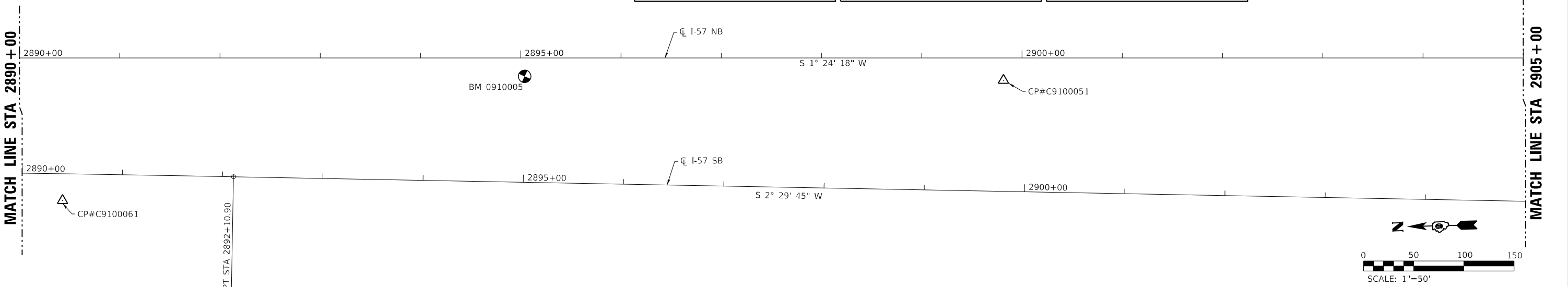
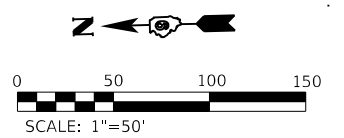
**CP#C9100051**  
 STA 2899+81.49, 22.34' RT  $\angle$  I-57 NB  
 N = 268416.8257 E = 745634.8013



**CP#C9100061**  
 STA 2890+42.95, 142.28' RT  $\angle$  I-57 NB  
 N = 269358.0282 E = 745537.9061

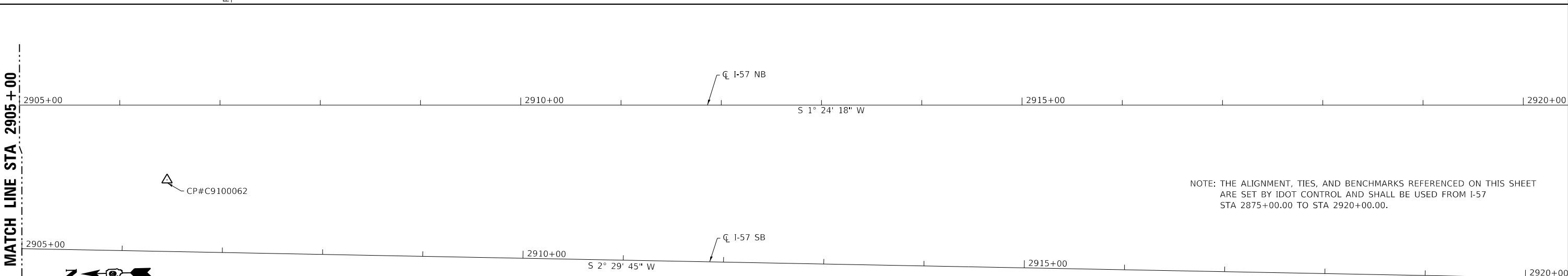
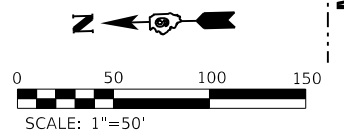


**CP#C9100062**  
 STA 2906+47.18, 74.57' RT  $\angle$  I-57 NB  
 N = 267752.6161 E = 745566.2681



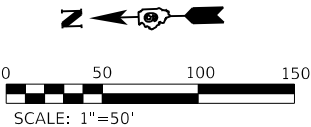
MATCH LINE STA 2890+00

MATCH LINE STA 2905+00



MATCH LINE STA 2905+00

MATCH LINE STA 2920+00



NOTE: THE ALIGNMENT, TIES, AND BENCHMARKS REFERENCED ON THIS SHEET ARE SET BY IDOT CONTROL AND SHALL BE USED FROM I-57 STA 2875+00.00 TO STA 2920+00.00.

**BENCHMARK** IDOT CONTROL  
 BM 0910005 - ALUMINUM DISK IN PARAPET WALL, NW CORNER OF  
 SN 091-0005, 18' RT  $\angle$  NBL I-57, STA 2895+03.89, EL 412.49

MODEL: PLOT  
 FILE NAME: Y:\IDOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\0978504-18-1801.dgn



|                            |                |           |
|----------------------------|----------------|-----------|
| USER NAME = jmk            | DESIGNED - SKM | REVISED - |
| ESCA PROJECT NO. 1259.11   | DRAWN - JPC    | REVISED - |
| PLOT SCALE = 0.1667' / in. | CHECKED - ELH  | REVISED - |
| PLOT DATE = 10/7/2019      | DATE - 08/19   | REVISED - |

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

|                                 |  |
|---------------------------------|--|
| ALIGNMENT, TIES, AND BENCHMARKS |  |
| SCALE: 1"=50'                   | SHEET NO. 2 OF 3 SHEETS STA. 2875+00.00 TO STA. 2920+00.00 |

| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 22        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |

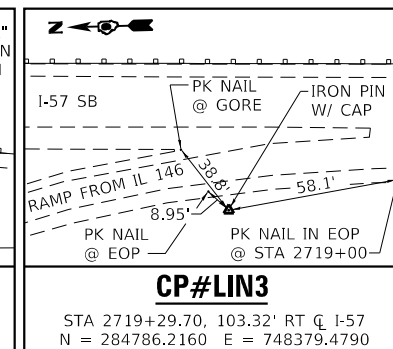
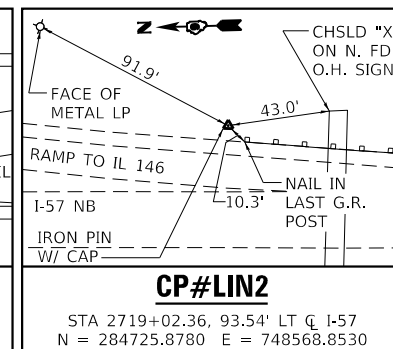
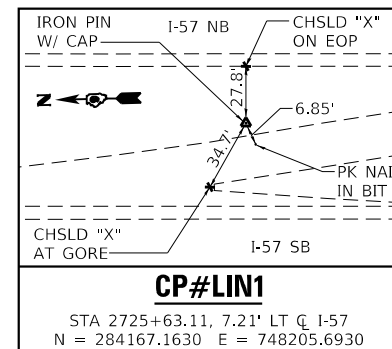
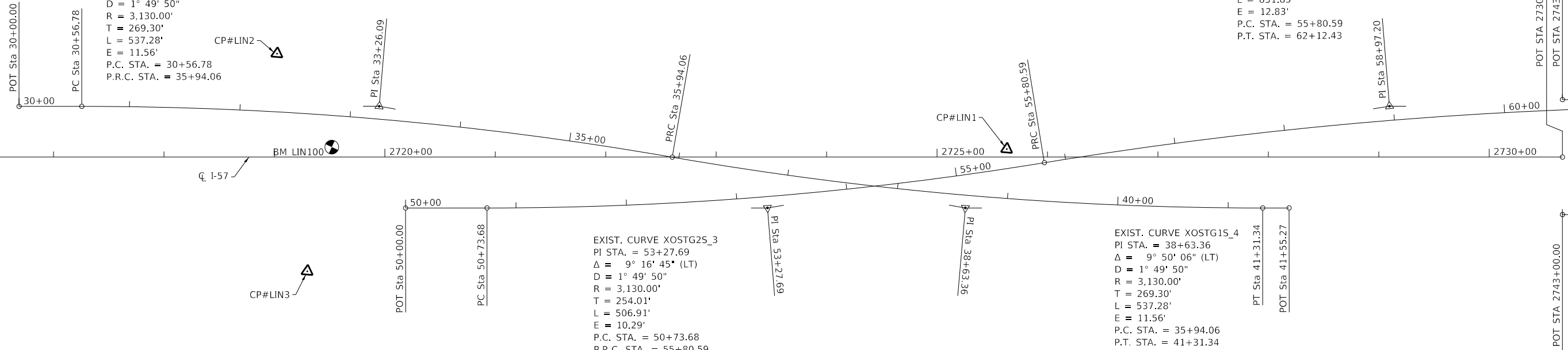


EXIST. CURVE XOSTG15\_3  
 PI STA. = 33+26.09  
 $\Delta = 9^\circ 50' 06''$  (RT)  
 $D = 1^\circ 49' 50''$   
 $R = 3,130.00'$   
 $T = 269.30'$   
 $L = 537.28'$   
 $E = 11.56'$   
 P.C. STA. = 30+56.78  
 P.R.C. STA. = 35+94.06

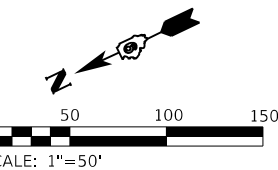
EXIST. CURVE XOSTG2S\_4  
 PI STA. = 58+97.20  
 $\Delta = 9^\circ 16' 57''$  (RT)  
 $D = 1^\circ 28' 09''$   
 $R = 3,899.99'$   
 $T = 316.62'$   
 $L = 631.85'$   
 $E = 12.83'$   
 P.C. STA. = 55+80.59  
 P.T. STA. = 62+12.43

EXIST. CURVE XOSTG2S\_3  
 PI STA. = 53+27.69  
 $\Delta = 9^\circ 16' 45''$  (LT)  
 $D = 1^\circ 49' 50''$   
 $R = 3,130.00'$   
 $T = 254.01'$   
 $L = 506.91'$   
 $E = 10.29'$   
 P.C. STA. = 50+73.68  
 P.R.C. STA. = 55+80.59

EXIST. CURVE XOSTG15\_4  
 PI STA. = 38+63.36  
 $\Delta = 9^\circ 50' 06''$  (LT)  
 $D = 1^\circ 49' 50''$   
 $R = 3,130.00'$   
 $T = 269.30'$   
 $L = 537.28'$   
 $E = 11.56'$   
 P.C. STA. = 35+94.06  
 P.T. STA. = 41+31.34



NOTE: THE ALIGNMENT, TIES, AND BENCHMARKS REFERENCED ON THIS SHEET ARE SET BY GPS IN STATE PLANE NAD 83 (EAST ZONE) AND NAVD88 AND SHALL BE USED FROM I-57 STA 2717+00.00 TO STA 2744+00.00.



|                            |                |           |
|----------------------------|----------------|-----------|
| USER NAME = jmk            | DESIGNED - SKM | REVISED - |
| ESCA PROJECT NO. 1259.11   | DRAWN - JPC    | REVISED - |
| PLOT SCALE = 0.1667' / in. | CHECKED - ELH  | REVISED - |
| PLOT DATE = 10/7/2019      | DATE - 08/19   | REVISED - |

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

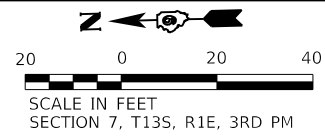
ALIGNMENT, TIES, AND BENCHMARKS

SCALE: 1"=50' SHEET NO. 3 OF 3 SHEETS STA. 2717+00.00 TO STA. 2744+00.00

**BENCHMARK** NAVD 88  
 BM LIN100 - CHISLED SQUARE IN SOUTH OH SIGN FOUNDATION COLUMN WEST SIDE OF NB I-57, 9' LT  $\angle$  I-57, STA 2719+53.29, EL 511.25

| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 23        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |

MODEL\_PLOT  
 FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\0978504-11-1803.dgn



MODEL: D:\p1\1259.11\_78504\CADD\Highway\CADD\_Sheets\0978504-ht-cd-ban01.dgn

**LEGEND**

AGG SHOULDERS, TYPE B 6"



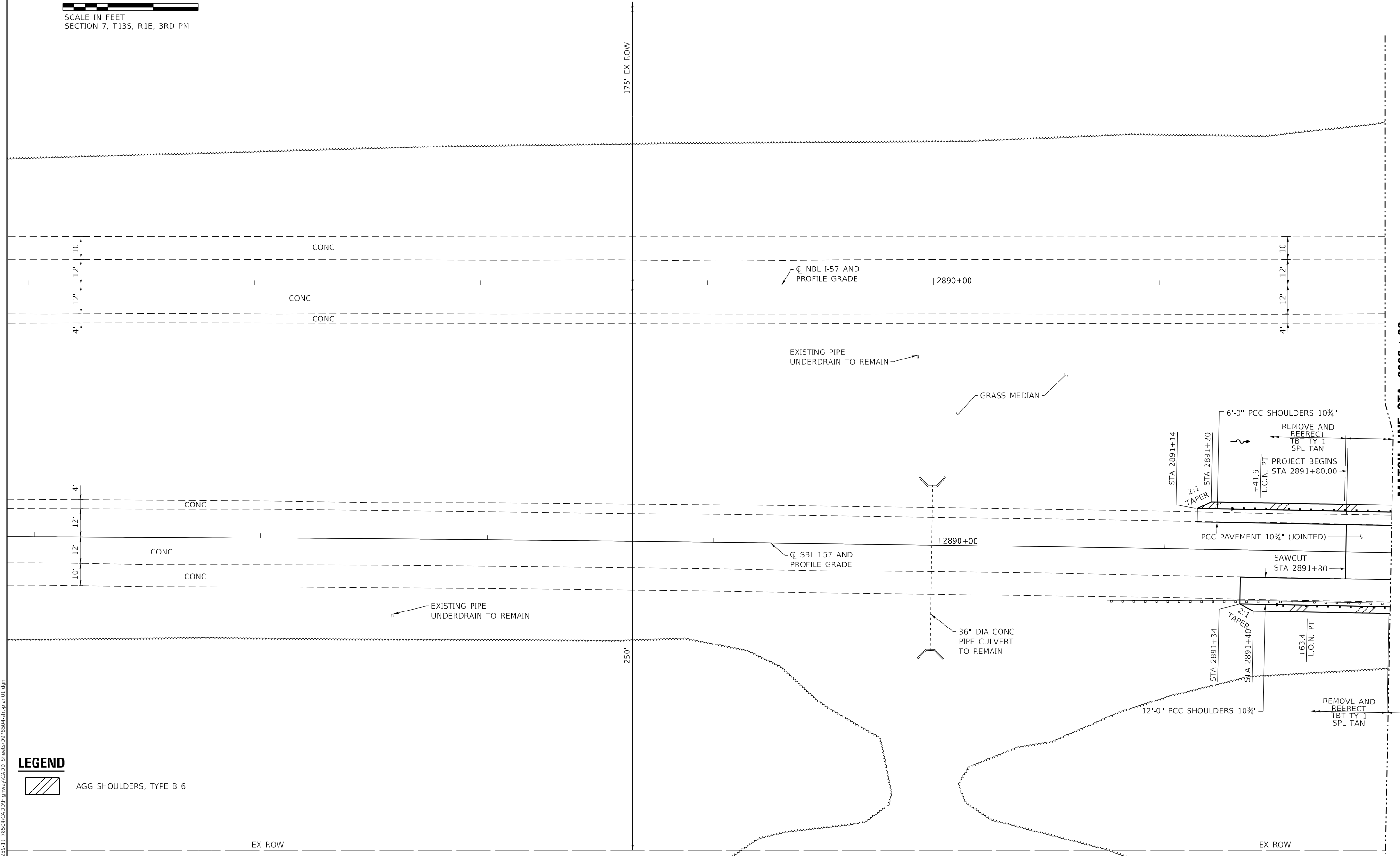
|                              |                |           |
|------------------------------|----------------|-----------|
| USER NAME = jmk              | DESIGNED - SKM | REVISED - |
| ESCA PROJECT NO. 1259.11     | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 40.0000 ' / in. | CHECKED - ELH  | REVISED - |
| PLOT DATE = 10/7/2019        | DATE - 10/19   | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

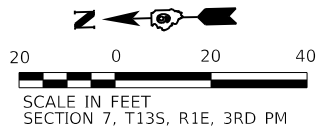
**I-57 PLAN**

SCALE: 1"=20'      SHEET NO. 1 OF 3 SHEETS      STA. 2886+00 TO STA. 2892+00

|                           |           |        |              |           |
|---------------------------|-----------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
| 57                        | (91-3)B-2 | UNION  | 161          | 24        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |



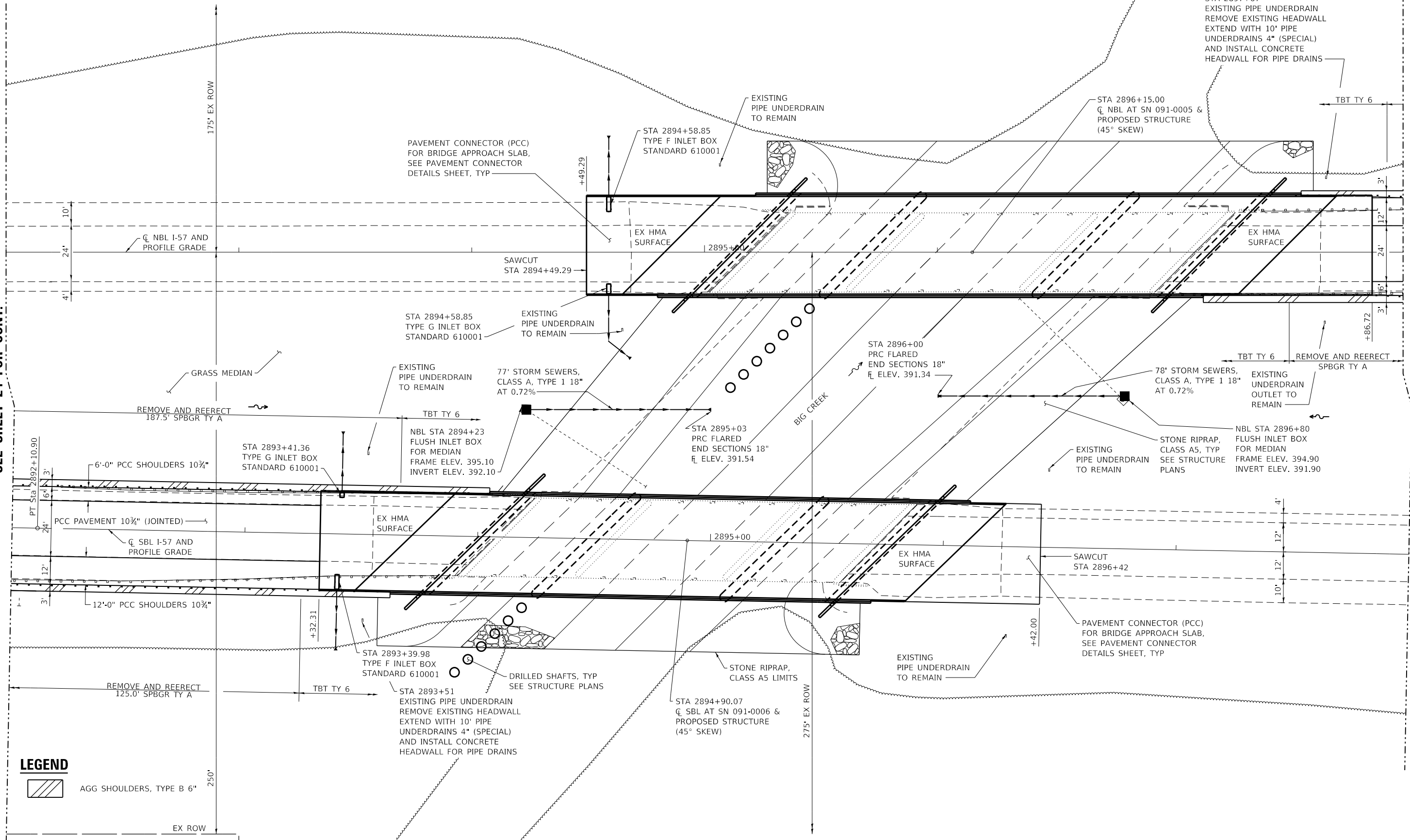
**MATCH LINE STA 2892 + 00  
SEE SHEET 25 FOR CONT.**



B.M. 0910005 ELEV. 412.49  
 ALUMINUM DISK, IDOT BM, SET IN TOP  
 OF NW PARAPET WALL OF SN 091-0005,  
 STA 2894+90, 18' RT OF C NB LANES OF I-57

MATCH LINE STA 2892+00  
SEE SHEET 24 FOR CONT.

MATCH LINE STA 2898+00  
SEE SHEET 26 FOR CONT.



**LEGEND**  
 AGG SHOULDERS, TYPE B 6"

MODEL: D:\efw\it...  
 FILE NAME: C:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\0978504-rt-ct-bd-01.dgn

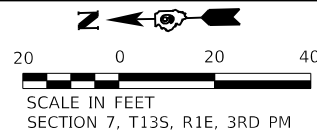


|                             |                |           |
|-----------------------------|----------------|-----------|
| USER NAME = jmk             | DESIGNED - SKM | REVISED - |
| ESCA PROJECT NO. 1259.11    | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 40.0000' / in. | CHECKED - ELH  | REVISED - |
| PLOT DATE = 10/7/2019       | DATE - 10/19   | REVISED - |

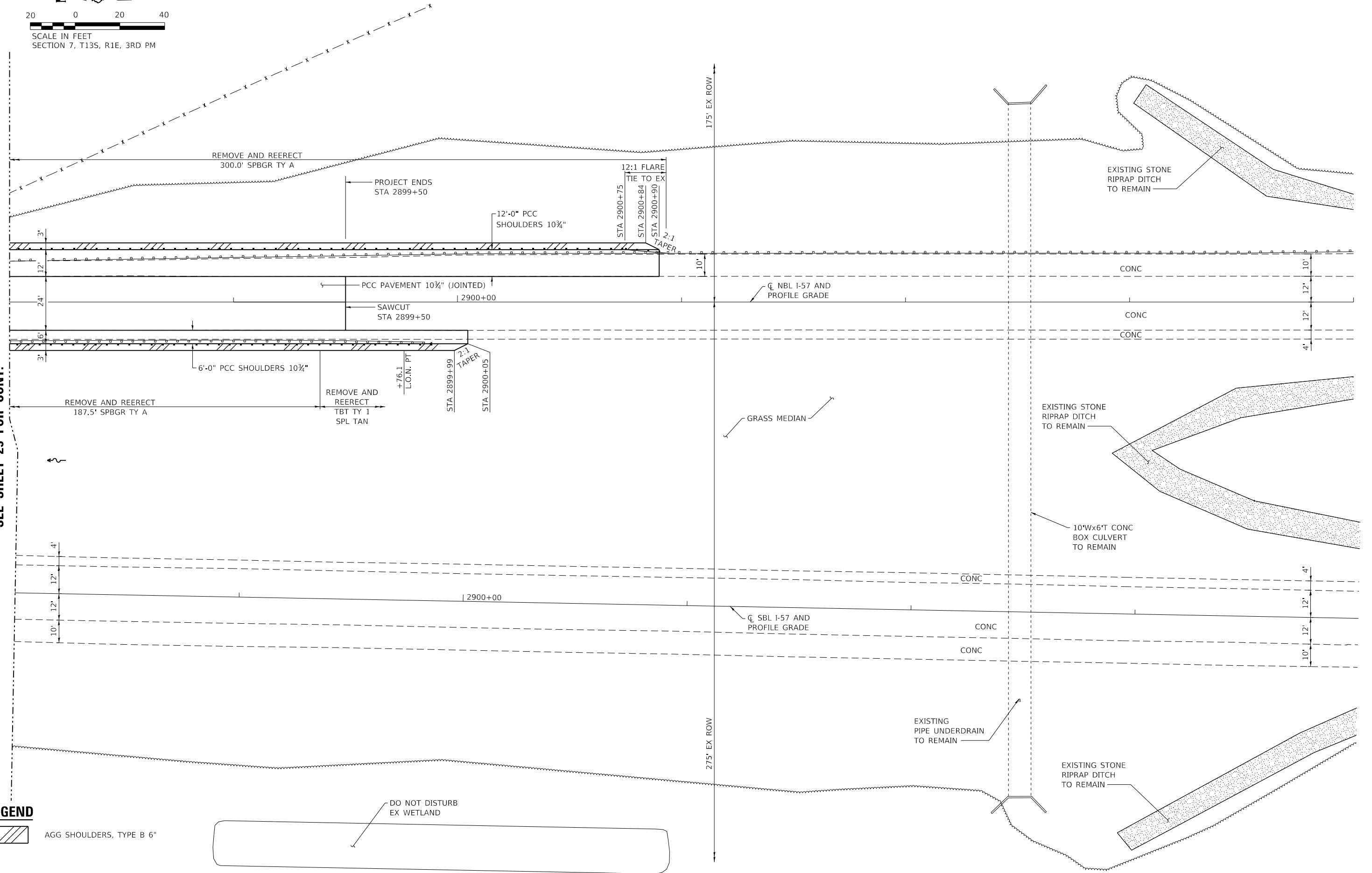
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**I-57 PLAN**  
 SCALE: 1"=20'  
 SHEET NO. 2 OF 3 SHEETS  
 STA. 2892+00 TO STA. 2898+00

| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 25        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |

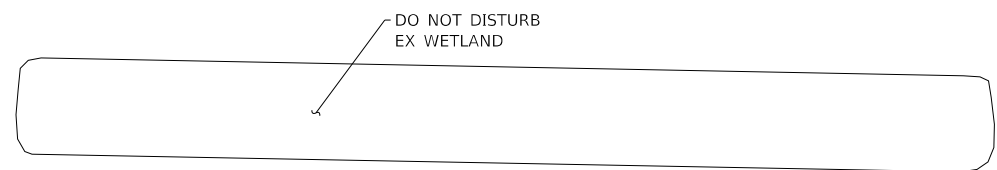


MATCH LINE STA 2898 +00  
SEE SHEET 25 FOR CONT.



**LEGEND**

AGG SHOULDERS, TYPE B 6"



MODEL: D:\m\11 ESCA\11259-11\_78504\CADD\Highway\CADD\_Sheets\0978504-rt-cplan01.dgn

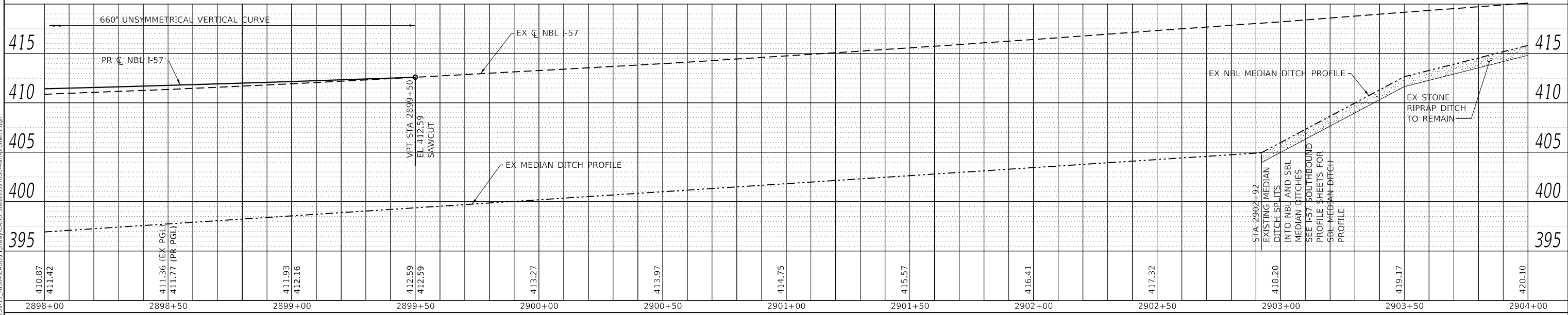
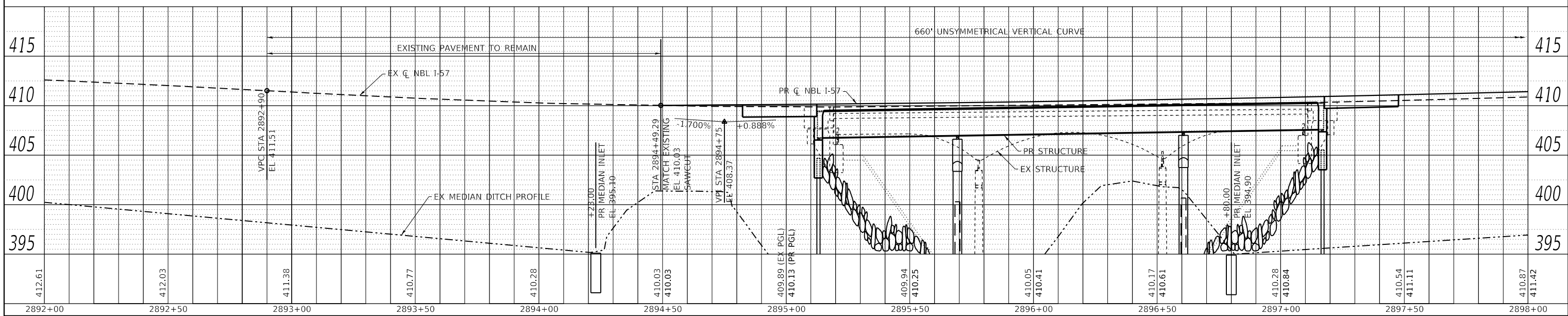
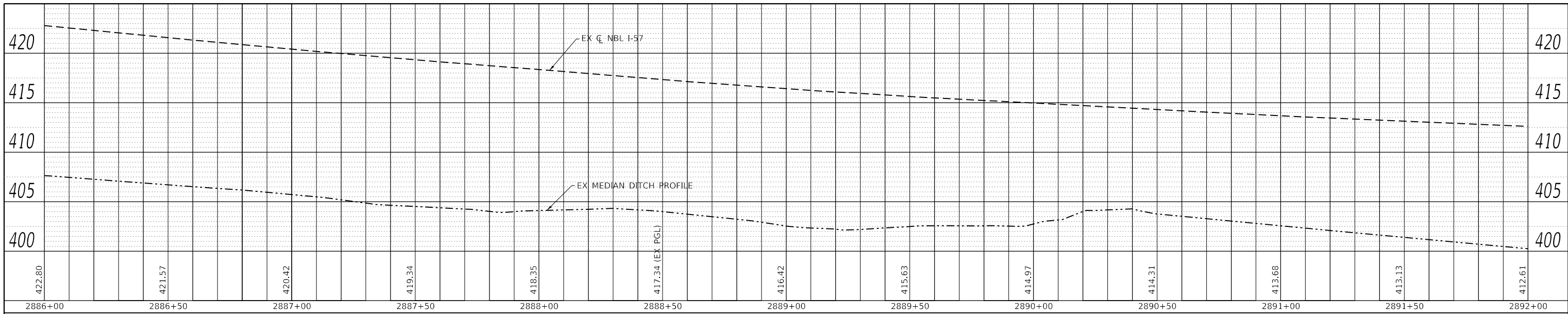


|                              |                |           |
|------------------------------|----------------|-----------|
| USER NAME = jmk              | DESIGNED - SKM | REVISED - |
| ESCA PROJECT NO. 1259.11     | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 40.0000 ' / in. | CHECKED - ELH  | REVISED - |
| PLOT DATE = 10/7/2019        | DATE - 10/19   | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**I-57 PLAN**  
SCALE: 1"=20' SHEET NO. 3 OF 3 SHEETS STA. 2898+00 TO STA. 2904+00

|                           |           |        |              |           |
|---------------------------|-----------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
| 57                        | (91-3)B-2 | UNION  | 161          | 26        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |



|           |                             |      |
|-----------|-----------------------------|------|
| PROFILE   | REVISION                    | DATE |
| NOTE BOOK | GRADES CHECKED              |      |
| NO.       | STRUCTURE NOTATIONS CHECKED |      |

MODEL: Default  
FILE NAME: Y:\01071259-1\78504\CADD\Highway\CADD\_Sheets\978504-sh-profile1.dgn



USER NAME = jmk  
ESCA PROJECT NO. 1259.04  
PLOT SCALE = 40,0000' / in.  
PLOT DATE = 10/7/2019

DESIGNED - SKM  
DRAWN - SKM  
CHECKED - ELH  
DATE - 08/19

REVISED -  
REVISED -  
REVISED -  
REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

I-57 NORTHBOUND PROFILE

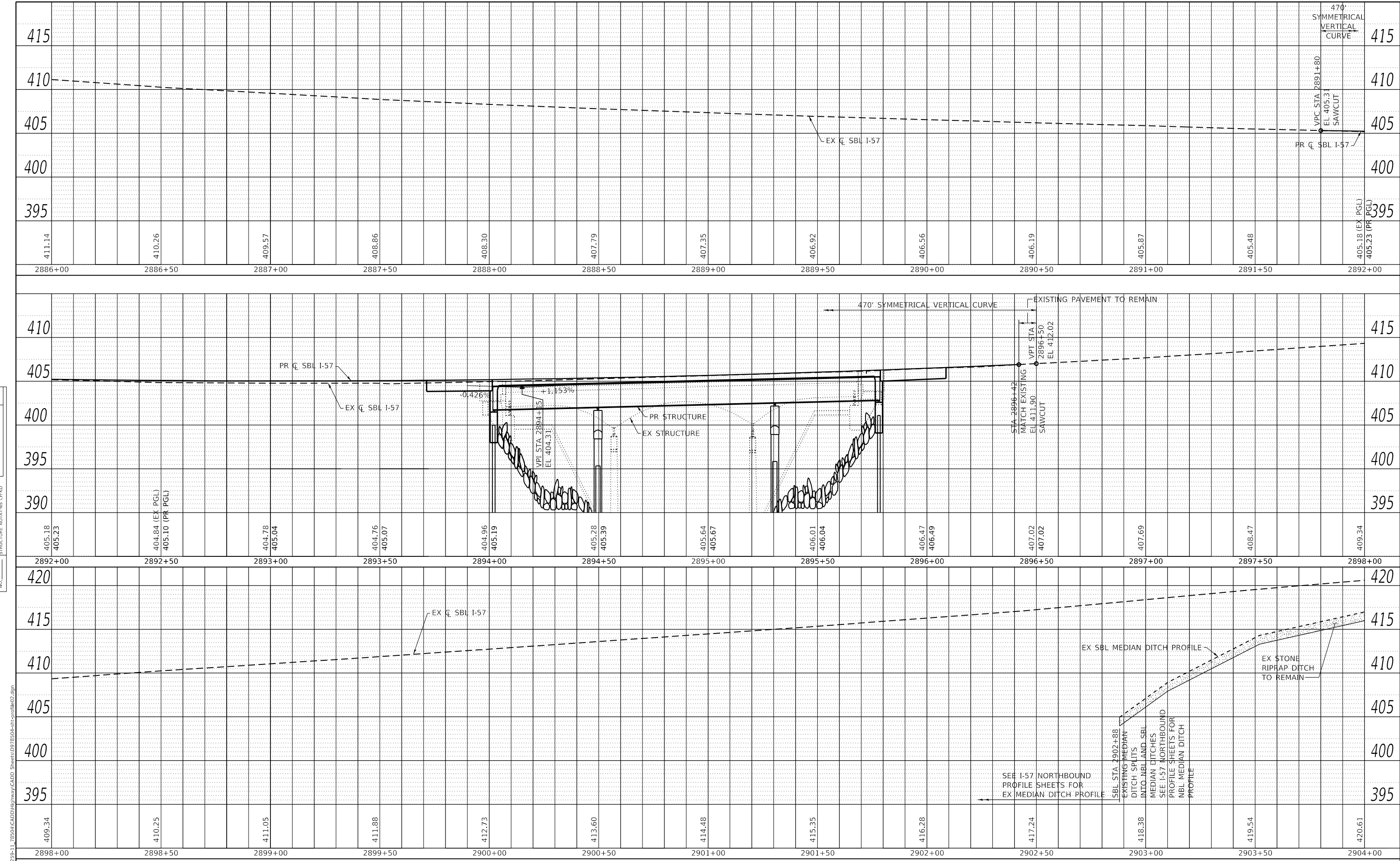
SCALE: AS SHOWN SHEET NO. 1 OF 2 SHEETS STA. 2886+00 TO STA. 2904+00

|                           |           |        |              |           |
|---------------------------|-----------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
| 57                        | (91-3)B-2 | UNION  | 161          | 27        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |



|          |  |
|----------|--|
| DATE     |  |
| BY       |  |
| DESIGNED |  |
| DRAWN    |  |
| CHECKED  |  |
| DATE     |  |

|           |  |
|-----------|--|
| PROFILE   |  |
| NOTE BOOK |  |
| NO.       |  |



MODEL: Default  
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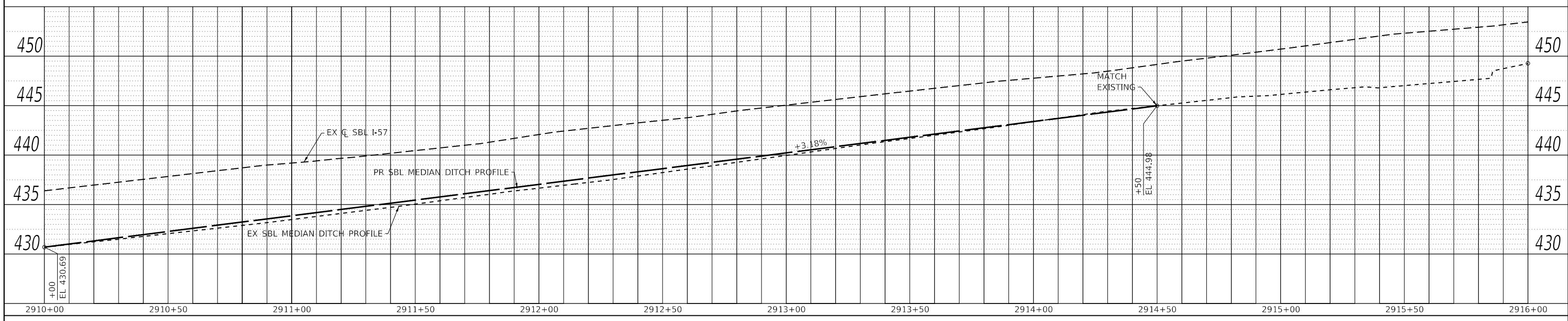
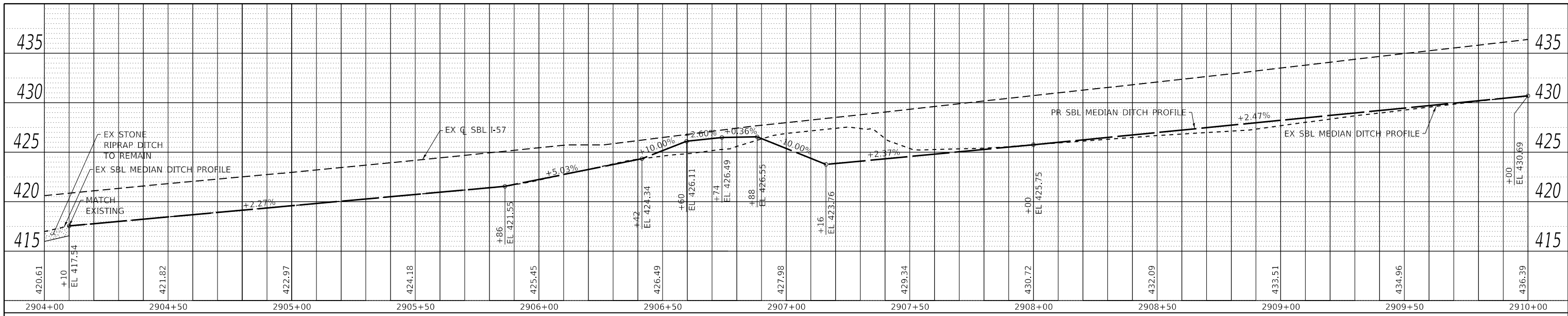


|                             |                    |           |
|-----------------------------|--------------------|-----------|
| USER NAME = jmk             | DESIGNED - JMK/SKM | REVISED - |
| ESCA PROJECT NO. 1259.11    | DRAWN - JMK/SKM    | REVISED - |
| PLOT SCALE = 40.0000' / in. | CHECKED - ELH      | REVISED - |
| PLOT DATE = 10/7/2019       | DATE - 10/19       | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

|                                |                         |
|--------------------------------|-------------------------|
| <b>I-57 SOUTHBOUND PROFILE</b> |                         |
| SCALE: AS SHOWN                | SHEET NO. 1 OF 2 SHEETS |
| STA. 2904+00                   | TO STA. 2886+00         |

|                           |           |        |              |           |
|---------------------------|-----------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
| 57                        | (91-3)B-2 | UNION  | 161          | 29        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |



|          |      |
|----------|------|
| PROFILE  | DATE |
| DESIGNED | BY   |
| DRAWN    |      |
| CHECKED  |      |
| DATE     |      |
| NO.      |      |

MODEL: Default  
FILE NAME: Y:\DOT\1259-1-78504\CADD\Highway\CADD Sheets\DP78504-sh-profile02.dgn



USER NAME = jmk  
ESCA PROJECT NO. 1259.11  
PLOT SCALE = 40.0000' / in.  
PLOT DATE = 10/7/2019

DESIGNED - JMK  
DRAWN - JMK  
CHECKED - SKM  
DATE - 06/19

REVISED -  
REVISED -  
REVISED -  
REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

SCALE: AS SHOWN SHEET NO. 2 OF 2 SHEETS STA. 2904+00 TO STA. 2886+00

**I-57 SOUTHBOUND PROFILE**

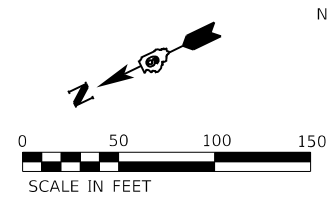
|                           |           |        |              |           |
|---------------------------|-----------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
| 57                        | (91-3)B-2 | UNION  | 161          | 30        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |



|           |  |
|-----------|--|
| DATE      |  |
| BY        |  |
| PLAN      |  |
| SURVEYED  |  |
| PLOTTED   |  |
| ALIGNED   |  |
| CHECKED   |  |
| NO. _____ |  |
| NOTE BOOK |  |
| NO. _____ |  |
| FILE NAME |  |

|           |  |
|-----------|--|
| DATE      |  |
| BY        |  |
| PROFILE   |  |
| SURVEYED  |  |
| PLOTTED   |  |
| GRADES    |  |
| CHECKED   |  |
| NO. _____ |  |
| NOTE BOOK |  |
| NO. _____ |  |
| FILE NAME |  |

MODEL: Default  
FILE NAME: Y:\01011259-11\_78504-CADD\Highway\CADD Sheets\DP78504-sh-cplp03.dgn



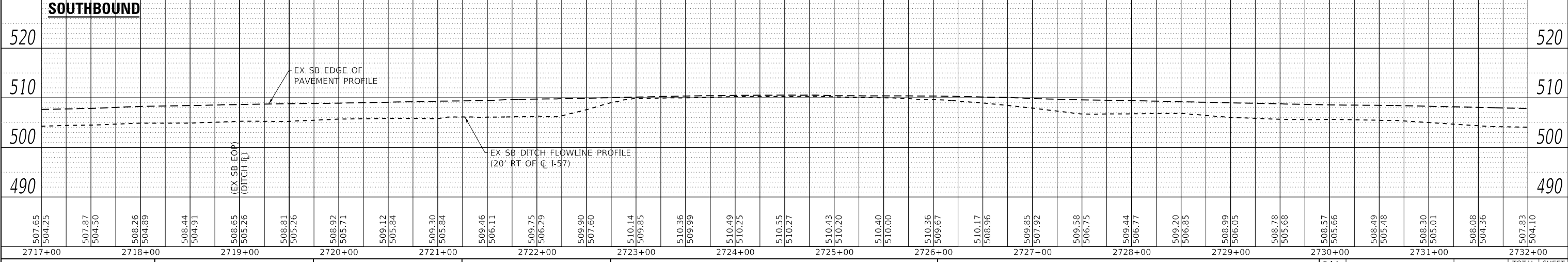
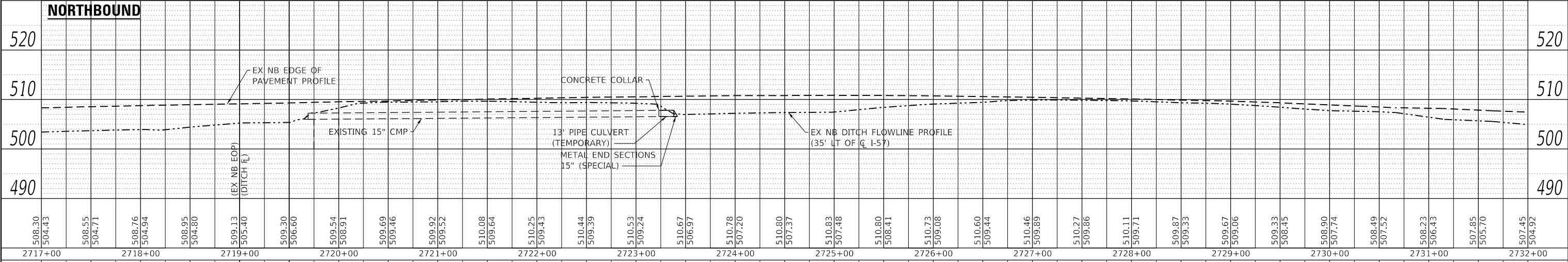
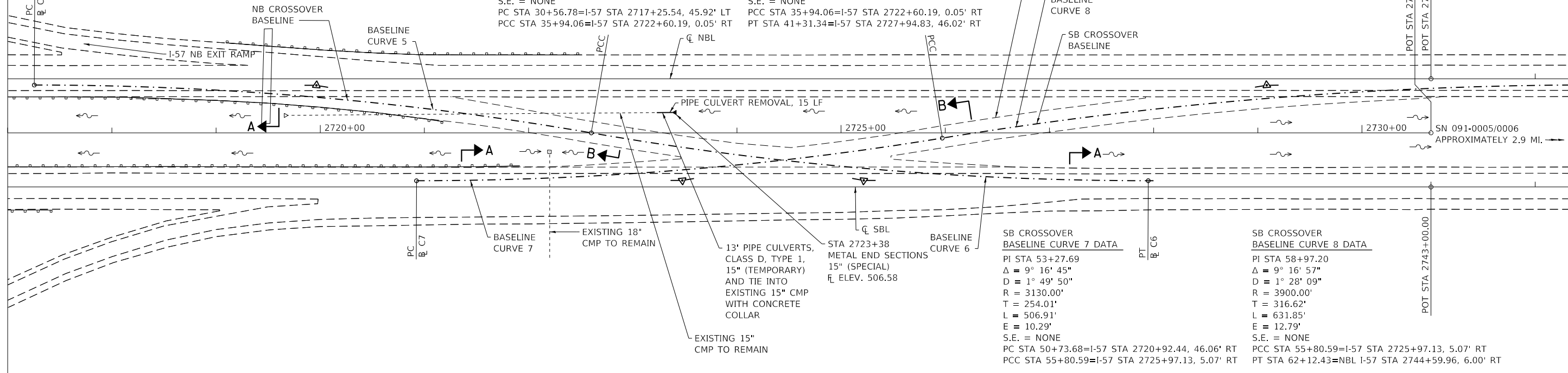
NOTE: NORTH CROSSOVERS ARE EXISTING SOUTH CROSSOVERS FROM IDOT CONTRACT NO. 78349. THESE CROSSOVERS WERE ORIGINALLY DESIGNED/CONSTRUCTED FOR IDOT CONTRACT NO. 78286 TO KEEP BOTH RAMPS OPEN DURING CONSTRUCTION.

**NB CROSSOVER BASELINE CURVE 5 DATA**  
 PI STA 33+26.09  
 $\Delta = 9^\circ 50' 06''$   
 $D = 1^\circ 49' 50''$   
 $R = 3130.00'$   
 $T = 269.30'$   
 $L = 537.28'$   
 $E = 11.56'$   
 S.E. = NONE  
 PC STA 30+56.78=I-57 STA 2717+25.54, 45.92' LT  
 PCC STA 35+94.06=I-57 STA 2722+60.19, 0.05' RT

**NB CROSSOVER BASELINE CURVE 6 DATA**  
 PI STA 38+63.36  
 $\Delta = 9^\circ 50' 06''$   
 $D = 1^\circ 49' 50''$   
 $R = 3130.00'$   
 $T = 269.30'$   
 $L = 537.28'$   
 $E = 11.56'$   
 S.E. = NONE  
 PCC STA 35+94.06=I-57 STA 2722+60.19, 0.05' RT  
 PT STA 41+31.34=I-57 STA 2727+94.83, 46.02' RT

**SB CROSSOVER BASELINE CURVE 7 DATA**  
 PI STA 53+27.69  
 $\Delta = 9^\circ 16' 45''$   
 $D = 1^\circ 49' 50''$   
 $R = 3130.00'$   
 $T = 254.01'$   
 $L = 506.91'$   
 $E = 10.29'$   
 S.E. = NONE  
 PC STA 50+73.68=I-57 STA 2720+92.44, 46.06' RT  
 PCC STA 55+80.59=I-57 STA 2725+97.13, 5.07' RT

**SB CROSSOVER BASELINE CURVE 8 DATA**  
 PI STA 58+97.20  
 $\Delta = 9^\circ 16' 57''$   
 $D = 1^\circ 28' 09''$   
 $R = 3900.00'$   
 $T = 316.62'$   
 $L = 631.85'$   
 $E = 12.79'$   
 S.E. = NONE  
 PCC STA 55+80.59=I-57 STA 2725+97.13, 5.07' RT  
 PT STA 62+12.43=NBL I-57 STA 2744+59.96, 6.00' RT



USER NAME = jmk  
 ESCA PROJECT NO. 1259.11  
 PLOT SCALE = 100.0000' / in.  
 PLOT DATE = 10/7/2019

DESIGNED - SKM  
 CHECKED - SKM  
 DRAWN - ELH  
 CHECKED - 10/19

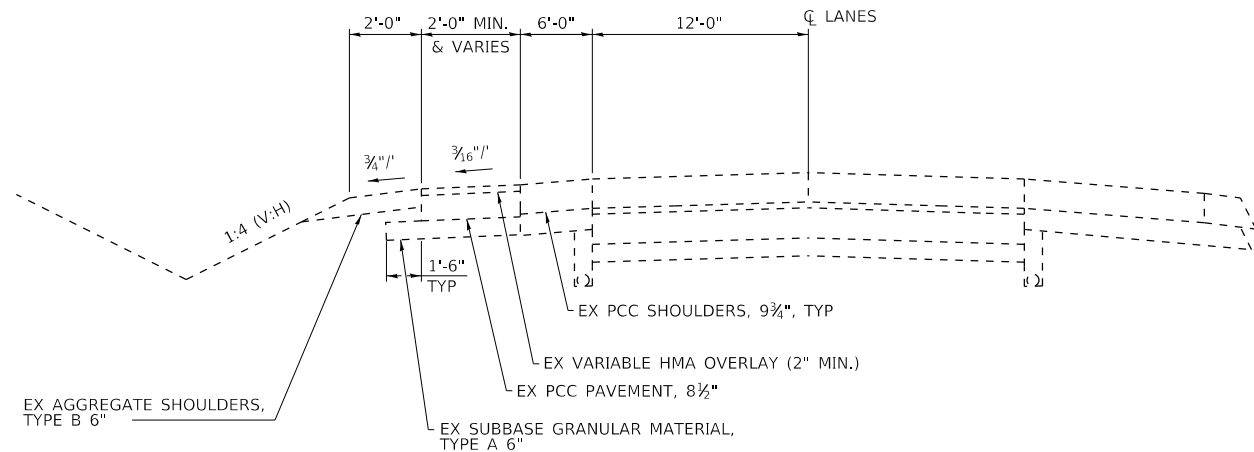
REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

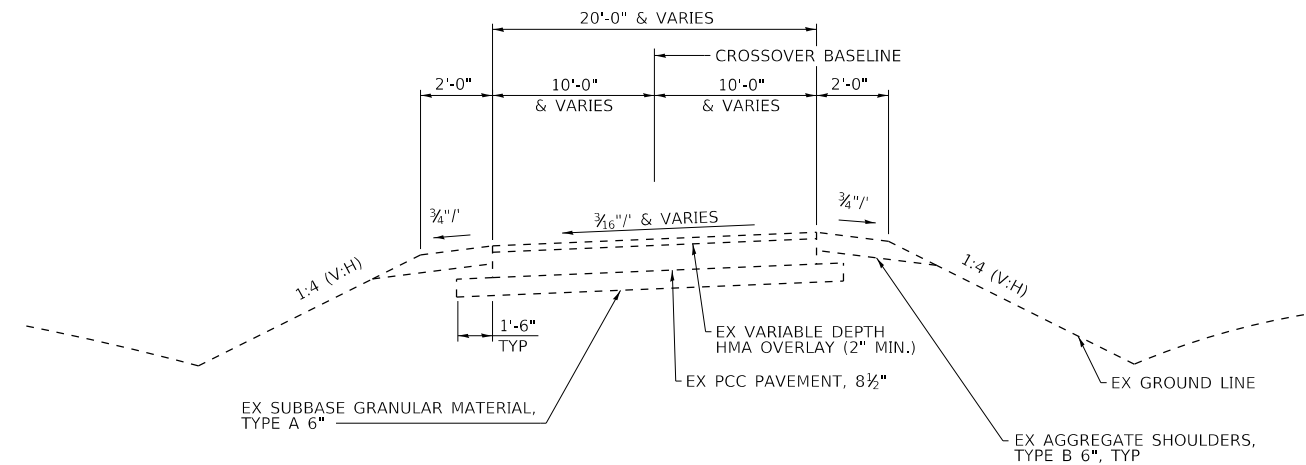
**NORTH CROSSOVERS PLAN AND PROFILE**

SCALE: AS SHOWN SHEET NO. 1 OF 1 SHEETS STA. 2717+00 TO STA. 2745+00

|                           |           |        |              |           |
|---------------------------|-----------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
| 57                        | (91-3)B-2 | UNION  | 161          | 31        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |



**NORTH CROSSOVERS SECTION A**



**NORTH CROSSOVERS SECTION B**

MODEL: PLOT  
FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\0978504-11-Highway\1259-11\_78504.dgn



|                            |
|----------------------------|
| USER NAME = jmk            |
| ESCA PROJECT NO. 1259.11   |
| PLOT SCALE = 0.1667' / in. |
| PLOT DATE = 10/7/2019      |

|                |
|----------------|
| DESIGNED - SKM |
| DRAWN - SKM    |
| CHECKED - ELH  |
| DATE - 04/19   |

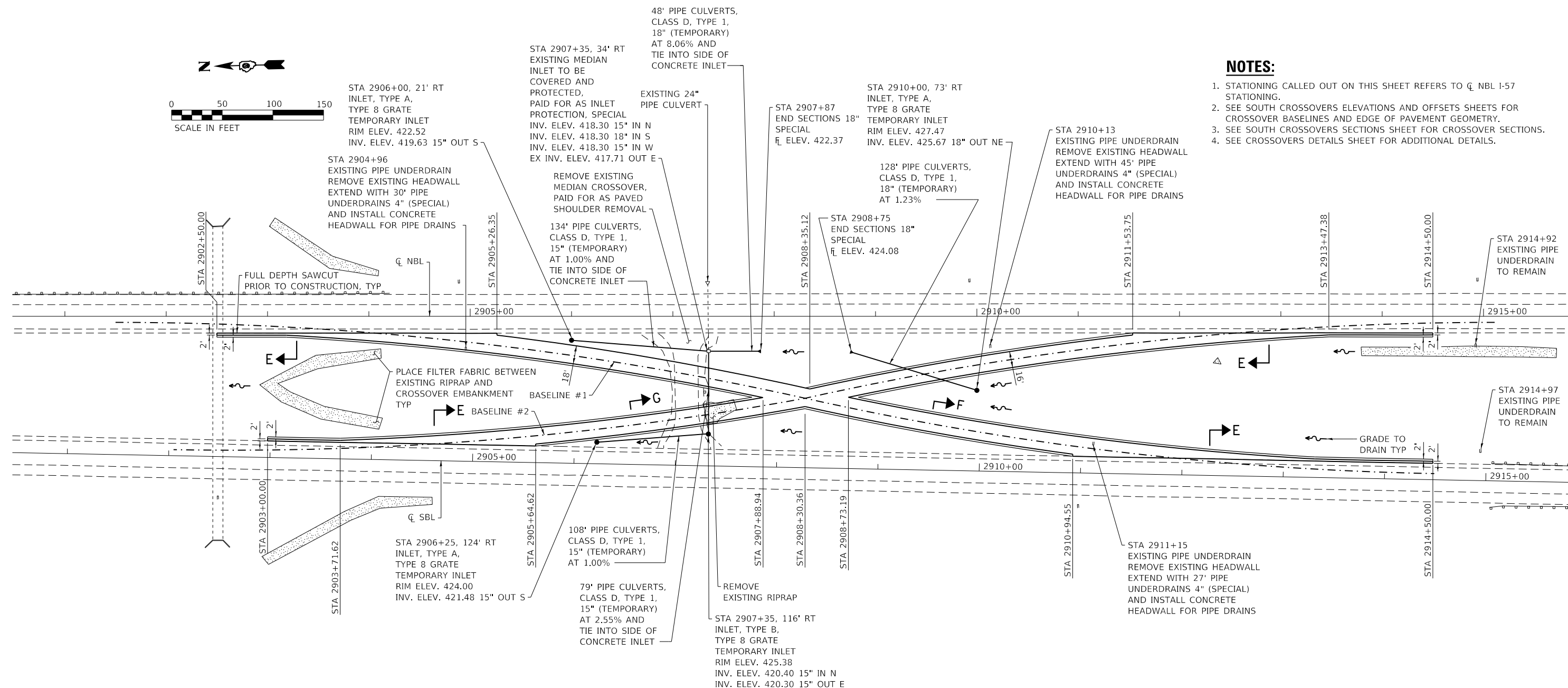
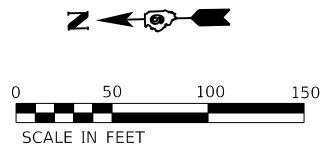
|           |
|-----------|
| REVISED - |
| REVISED - |
| REVISED - |
| REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**NORTH CROSSOVERS SECTIONS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| F.A.I. RTE.        | SECTION   | COUNTY | TOTAL SHEETS              | SHEET NO. |
|--------------------|-----------|--------|---------------------------|-----------|
| 57                 | (91-3)B-2 | UNION  | 161                       | 32        |
| CONTRACT NO. 78504 |           |        | ILLINOIS FED. AID PROJECT |           |



- NOTES:**
1. STATIONING CALLED OUT ON THIS SHEET REFERS TO  $\bar{C}$  NBL I-57 STATIONING.
  2. SEE SOUTH CROSSOVERS ELEVATIONS AND OFFSETS SHEETS FOR CROSSOVER BASELINES AND EDGE OF PAVEMENT GEOMETRY.
  3. SEE SOUTH CROSSOVERS SECTIONS SHEET FOR CROSSOVER SECTIONS.
  4. SEE CROSSOVERS DETAILS SHEET FOR ADDITIONAL DETAILS.

MODEL\_PLOT  
 FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\0978504-1-Plan02.dgn



|                            |                |           |
|----------------------------|----------------|-----------|
| USER NAME = jmk            | DESIGNED - SKM | REVISED - |
| ESCA PROJECT NO. 1259.11   | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.1667' / in. | CHECKED - ELH  | REVISED - |
| PLOT DATE = 10/14/2019     | DATE - 10/19   | REVISED - |

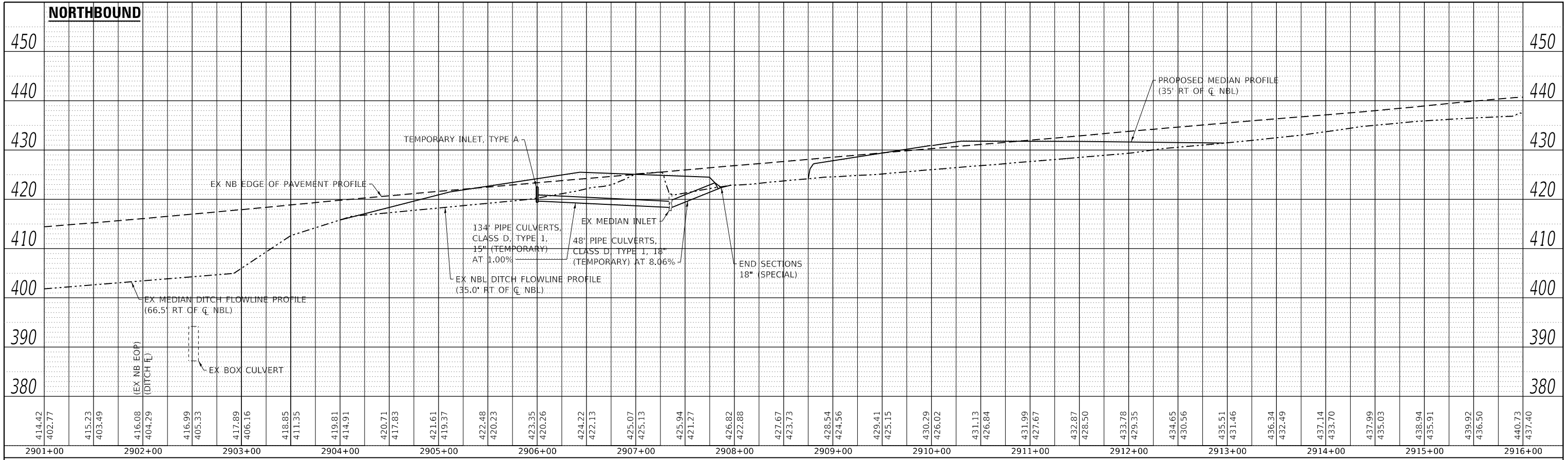
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**SOUTH CROSSOVERS PLAN**

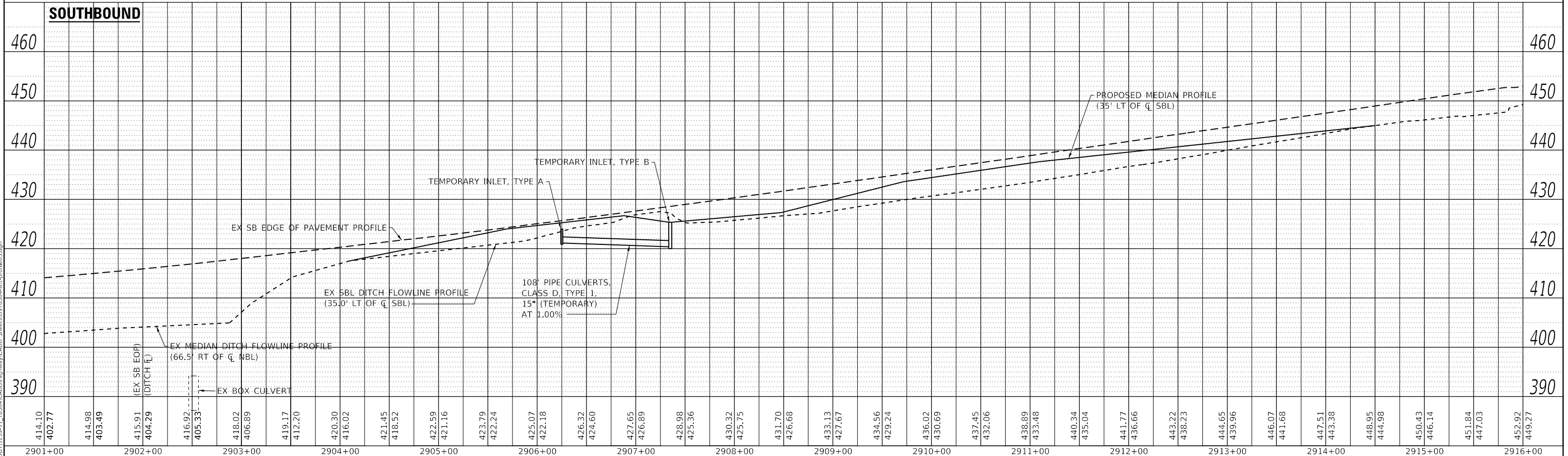
SCALE: 1"=50'      SHEET NO. 1 OF 1 SHEETS      STA. 2900+50 TO STA. 2916+00

|                    |           |        |                           |           |
|--------------------|-----------|--------|---------------------------|-----------|
| F.A.I. RTE.        | SECTION   | COUNTY | TOTAL SHEETS              | SHEET NO. |
| 57                 | (91-3)B-2 | UNION  | 161                       | 33        |
| CONTRACT NO. 78504 |           |        | ILLINOIS FED. AID PROJECT |           |

|               |           |         |
|---------------|-----------|---------|
| PLAN          | SURVEYED  | DATE    |
|               | PLOTTED   |         |
| NOTE BOOK NO. | ALIGNED   | CHECKED |
|               | FILE NAME |         |



|               |           |          |
|---------------|-----------|----------|
| PROFILE       | SURVEYED  | DATE     |
|               | PLOTTED   |          |
| NOTE BOOK NO. | GRADES    | CHECKED  |
|               | STRUCTURE | NOTATION |



MODEL: DefRail  
FILE NAME: Y:\01011259-11-78504-CADD\Highway\CADD Sheets\DP78504-sh-cp-profiled3.dgn



|                              |                |           |
|------------------------------|----------------|-----------|
| USER NAME = jmk              | DESIGNED - SKM | REVISED - |
| ESCA PROJECT NO. 1259.11     | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 100.0000' / in. | CHECKED - ELH  | REVISED - |
| PLOT DATE = 10/7/2019        | DATE - 05/19   | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SOUTH CROSSOVERS PROFILE**

SCALE: AS SHOWN SHEET NO. 1 OF 1 SHEETS STA. 2916+00 TO STA. 2901+00

|                           |           |        |              |           |
|---------------------------|-----------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
| 57                        | (91-3)B-2 | UNION  | 161          | 34        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |





**NB CROSSOVER  
BASELINE CURVE 3 DATA**

PI STA 73+14.24  
 $\Delta = 11^\circ 27' 59''$   
 $D = 1^\circ 49' 50''$   
 $R = 3130.00'$   
 $T = 314.24'$   
 $L = 626.39'$   
 $E = 15.74'$   
 S.E. = NONE  
 PC STA 70+00.00=NBL I-57 STA 2902+07.22, 131.48' RT  
 PCC STA 76+26.39=NBL I-57 STA 2908+30.51, 80.87' RT

**NB CROSSOVER  
BASELINE CURVE 4 DATA**

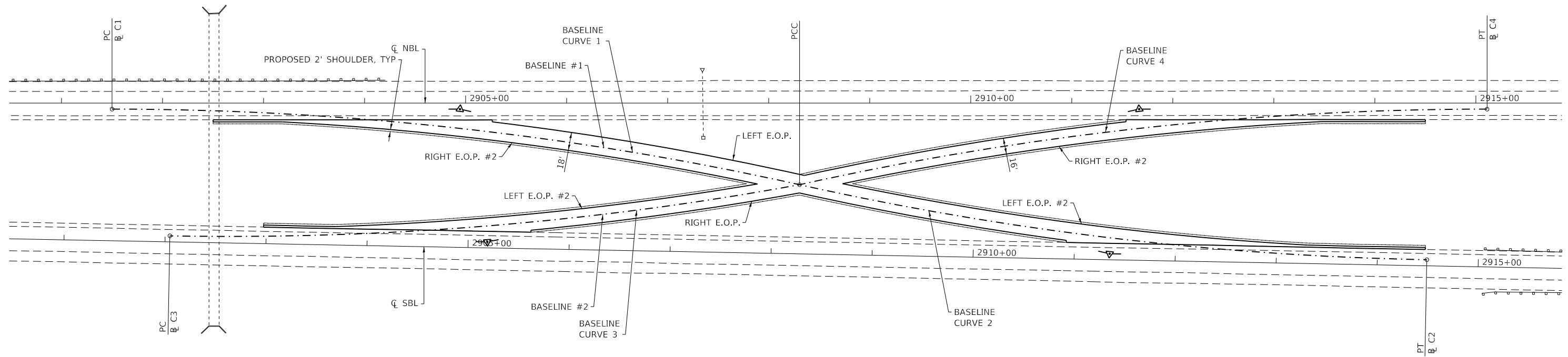
PI STA 79+70.76  
 $\Delta = 12^\circ 33' 26''$   
 $D = 1^\circ 49' 50''$   
 $R = 3130.00'$   
 $T = 344.37'$   
 $L = 685.99'$   
 $E = 18.89'$   
 S.E. = NONE  
 PCC STA 76+26.39=NBL I-57 STA 2908+30.51, 80.87' RT  
 PT STA 83+12.38=NBL I-57 STA 15+11.02, 6.00' RT

**SB CROSSOVER  
BASELINE CURVE 1 DATA**

PI STA 53+44.37  
 $\Delta = 12^\circ 33' 26''$   
 $D = 1^\circ 49' 50''$   
 $R = 3130.00'$   
 $T = 344.37'$   
 $L = 685.99'$   
 $E = 18.89'$   
 S.E. = NONE  
 PC STA 50+00.00=NBL I-57 STA 2901+50.00, 6.00' RT  
 PCC STA 56+85.99=NBL I-57 STA 2908+30.51, 80.87' RT

**SB CROSSOVER  
BASELINE CURVE 2 DATA**

PI STA 60+00.23  
 $\Delta = 11^\circ 27' 59''$   
 $D = 1^\circ 49' 50''$   
 $R = 3130.00'$   
 $T = 314.24'$   
 $L = 626.39'$   
 $E = 15.74'$   
 S.E. = NONE  
 PCC STA 56+85.99=NBL I-57 STA 2908+30.51, 80.87' RT  
 PT STA 63+12.38=NBL I-57 STA 14+51.42, 155.18' RT



MODEL\_PLOT  
FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\0978504-11-14-19-03.dwg



|                            |                |           |
|----------------------------|----------------|-----------|
| USER NAME = jmk            | DESIGNED - SKM | REVISED - |
| ESCA PROJECT NO. 1259.11   | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.1667' / in. | CHECKED - ELH  | REVISED - |
| PLOT DATE = 10/7/2019      | DATE - 04/19   | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SOUTH CROSSOVERS ELEVATIONS AND OFFSETS**

SCALE: 1"=50' SHEET NO. 1 OF 2 SHEETS STA. 2900+50 TO STA. 2916+00

| F.A.I. RTE.        | SECTION   | COUNTY | TOTAL SHEETS              | SHEET NO. |
|--------------------|-----------|--------|---------------------------|-----------|
| 57                 | (91-3)B-2 | UNION  | 161                       | 36        |
| CONTRACT NO. 78504 |           |        | ILLINOIS FED. AID PROJECT |           |

**ELEVATION AND OFFSET DATA**

| NBL I-57 STATION | BASELINE #1 |    | BASELINE #2 |    | LEFT E.O.P. |           | RIGHT E.O.P. #2 |       | BREAK POINT |           | LEFT E.O.P. #2 |           | RIGHT E.O.P. |           |
|------------------|-------------|----|-------------|----|-------------|-----------|-----------------|-------|-------------|-----------|----------------|-----------|--------------|-----------|
|                  | OFFSET (FT) |    | OFFSET (FT) |    | OFFSET (FT) | ELEVATION |                 |       | OFFSET (FT) | ELEVATION | OFFSET (FT)    | ELEVATION | OFFSET (FT)  | ELEVATION |
| 2901+50.00       | 6.00        | RT | -           | -  | -           | -         | -               | -     | -           | -         | -              | -         | -            | -         |
| 2901+75.00       | 6.10        | RT | -           | -  | -           | -         | -               | -     | -           | -         | -              | -         | -            | -         |
| 2902+00.00       | 6.40        | RT | -           | -  | -           | -         | -               | -     | -           | -         | -              | -         | -            | -         |
| 2902+07.22       | 6.52        | RT | 131.48      | RT | -           | -         | -               | -     | -           | -         | -              | -         | -            | -         |
| 2902+25.00       | 6.90        | RT | 131.77      | RT | -           | -         | -               | -     | -           | -         | -              | -         | -            | -         |
| 2902+50.00       | 7.60        | RT | 132.00      | RT | 16.55       | RT        | 416.97          | 18.55 | RT          | 416.94    | -              | -         | -            | -         |
| 2902+75.00       | 8.50        | RT | 132.04      | RT | 16.56       | RT        | 417.42          | 18.56 | RT          | 417.39    | -              | -         | -            | -         |
| 2903+00.00       | 9.60        | RT | 131.87      | RT | 16.56       | RT        | 417.87          | 18.56 | RT          | 417.84    | -              | -         | -            | -         |
| 2903+18.52       | 10.54       | RT | 131.62      | RT | 16.55       | RT        | 418.23          | 18.55 | RT          | 418.20    | -              | -         | -            | -         |
| 2903+25.00       | 10.90       | RT | 131.51      | RT | 16.55       | RT        | 418.35          | 18.91 | RT          | 418.31    | -              | -         | -            | -         |
| 2903+50.00       | 12.40       | RT | 130.94      | RT | 16.53       | RT        | 418.83          | 20.41 | RT          | 418.77    | -              | -         | -            | -         |
| 2903+71.62       | 13.86       | RT | 130.29      | RT | 16.51       | RT        | 419.25          | 21.88 | RT          | 419.16    | -              | -         | -            | -         |
| 2904+00.00       | 16.00       | RT | 129.21      | RT | 16.48       | RT        | 419.79          | 24.03 | RT          | 419.67    | -              | -         | -            | -         |
| 2904+06.03       | 16.49       | RT | 128.95      | RT | 16.49       | RT        | 419.90          | 24.52 | RT          | 419.77    | -              | -         | -            | -         |
| 2904+25.00       | 18.10       | RT | 128.05      | RT | 16.52       | RT        | 420.24          | 26.14 | RT          | 420.09    | -              | -         | -            | -         |
| 2904+50.00       | 20.41       | RT | 126.68      | RT | 16.57       | RT        | 420.69          | 28.45 | RT          | 420.50    | -              | -         | -            | -         |
| 2904+63.37       | 21.73       | RT | 125.87      | RT | 16.62       | RT        | 420.93          | 29.77 | RT          | 420.72    | -              | -         | -            | -         |
| 2904+75.00       | 22.92       | RT | 125.12      | RT | 16.66       | RT        | 421.14          | 30.96 | RT          | 420.91    | -              | -         | -            | -         |
| 2905+00.00       | 25.63       | RT | 123.35      | RT | 16.75       | RT        | 421.58          | 33.68 | RT          | 421.32    | -              | -         | -            | -         |
| 2905+25.00       | 28.55       | RT | 121.38      | RT | 16.64       | RT        | 422.02          | 36.60 | RT          | 421.71    | -              | -         | -            | -         |
| 2905+26.35       | 28.71       | RT | 121.27      | RT | 16.64       | RT        | 422.05          | 36.77 | RT          | 421.73    | -              | -         | -            | -         |
| 2905+26.35       | 28.71       | RT | 121.27      | RT | 18.64       | RT        | 422.02          | 36.77 | RT          | 421.73    | -              | -         | -            | -         |
| 2905+50.00       | 31.66       | RT | 119.21      | RT | 21.58       | RT        | 422.62          | 39.73 | RT          | 422.36    | -              | -         | -            | -         |
| 2905+64.62       | 33.58       | RT | 117.85      | RT | 23.50       | RT        | 422.99          | 41.66 | RT          | 422.75    | -              | -         | -            | -         |
| 2905+64.62       | 33.58       | RT | 117.85      | RT | 23.50       | RT        | 422.99          | 41.66 | RT          | 422.75    | -              | -         | -            | -         |
| 2905+75.00       | 34.99       | RT | 116.84      | RT | 24.89       | RT        | 423.25          | 43.06 | RT          | 423.02    | -              | -         | -            | -         |
| 2906+00.00       | 38.52       | RT | 114.27      | RT | 28.41       | RT        | 423.89          | 46.60 | RT          | 423.68    | -              | -         | -            | -         |
| 2906+25.00       | 42.25       | RT | 111.49      | RT | 32.14       | RT        | 424.52          | 50.35 | RT          | 424.34    | -              | -         | -            | -         |
| 2906+50.00       | 46.19       | RT | 108.51      | RT | 36.06       | RT        | 425.16          | 54.30 | RT          | 425.00    | -              | -         | -            | -         |
| 2906+75.00       | 50.34       | RT | 105.32      | RT | 40.20       | RT        | 425.79          | 58.46 | RT          | 425.67    | -              | -         | -            | -         |
| 2907+00.00       | 54.70       | RT | 101.93      | RT | 44.54       | RT        | 426.42          | 62.83 | RT          | 426.33    | -              | -         | -            | -         |
| 2907+25.00       | 59.27       | RT | 98.33       | RT | 49.10       | RT        | 427.06          | 67.41 | RT          | 426.99    | -              | -         | -            | -         |
| 2907+50.00       | 64.05       | RT | 94.53       | RT | 53.86       | RT        | 427.69          | 72.20 | RT          | 427.65    | -              | -         | -            | -         |
| 2907+75.00       | 69.03       | RT | 90.52       | RT | 58.83       | RT        | 428.33          | 77.20 | RT          | 428.31    | -              | -         | -            | -         |
| 2907+88.94       | 71.91       | RT | 88.19       | RT | 61.69       | RT        | 428.68          | -     | -           | 80.08     | RT             | 428.68    | -            | -         |
| 2908+00.00       | 74.24       | RT | 86.30       | RT | 64.01       | RT        | 428.92          | -     | -           | 80.29     | RT             | 428.92    | -            | -         |
| 2908+25.00       | 79.65       | RT | 81.88       | RT | 69.41       | RT        | 429.48          | -     | -           | 80.77     | RT             | 429.48    | -            | -         |
| 2908+30.36       | 80.84       | RT | 80.90       | RT | 70.59       | RT        | 429.59          | -     | -           | 80.87     | RT             | 429.59    | -            | -         |
| 2908+30.51       | 80.87       | RT | 80.87       | RT | 70.63       | RT        | 429.60          | -     | -           | 80.87     | RT             | 429.60    | -            | -         |
| 2908+35.12       | 81.90       | RT | 79.85       | RT | 71.66       | RT        | 429.70          | -     | -           | 80.76     | RT             | 429.70    | -            | -         |
| 2908+50.00       | 85.15       | RT | 76.60       | RT | 68.41       | RT        | 430.03          | -     | -           | 80.41     | RT             | 430.03    | -            | -         |
| 2908+73.19       | 90.07       | RT | 71.68       | RT | 63.51       | RT        | 430.54          | -     | -           | 79.85     | RT             | 430.54    | -            | -         |
| 2909+00.00       | 95.53       | RT | 66.22       | RT | 58.06       | RT        | 430.76          | 74.38 | RT          | 430.66    | -              | -         | -            | -         |
| 2909+25.00       | 100.40      | RT | 61.35       | RT | 53.21       | RT        | 430.96          | 69.49 | RT          | 430.77    | -              | -         | -            | -         |
| 2909+50.00       | 105.06      | RT | 56.69       | RT | 48.56       | RT        | 431.16          | 64.82 | RT          | 430.89    | -              | -         | -            | -         |
| 2909+75.00       | 109.51      | RT | 52.24       | RT | 44.12       | RT        | 431.37          | 60.36 | RT          | 431.00    | -              | -         | -            | -         |
| 2910+00.00       | 113.75      | RT | 48.00       | RT | 39.89       | RT        | 431.57          | 56.11 | RT          | 431.11    | -              | -         | -            | -         |
| 2910+25.00       | 117.78      | RT | 43.96       | RT | 35.87       | RT        | 431.77          | 52.06 | RT          | 431.22    | -              | -         | -            | -         |
| 2910+50.00       | 121.61      | RT | 40.14       | RT | 32.05       | RT        | 431.97          | 48.23 | RT          | 431.34    | -              | -         | -            | -         |
| 2910+75.00       | 125.23      | RT | 36.52       | RT | 28.44       | RT        | 432.18          | 44.60 | RT          | 431.45    | -              | -         | -            | -         |
| 2910+94.55       | 127.91      | RT | 33.83       | RT | 25.76       | RT        | 432.33          | 41.90 | RT          | 431.54    | -              | -         | -            | -         |
| 2910+94.55       | 127.91      | RT | 33.83       | RT | 25.76       | RT        | 432.33          | 41.90 | RT          | 431.54    | -              | -         | -            | -         |
| 2911+00.00       | 128.64      | RT | 33.10       | RT | 25.03       | RT        | 432.38          | 41.17 | RT          | 431.56    | -              | -         | -            | -         |
| 2911+25.00       | 131.85      | RT | 29.90       | RT | 21.83       | RT        | 432.58          | 37.96 | RT          | 431.68    | -              | -         | -            | -         |
| 2911+50.00       | 134.85      | RT | 26.89       | RT | 18.84       | RT        | 432.78          | 34.94 | RT          | 431.79    | -              | -         | -            | -         |
| 2911+53.75       | 135.29      | RT | 26.46       | RT | 18.40       | RT        | 432.81          | 34.51 | RT          | 431.81    | -              | -         | -            | -         |
| 2911+53.75       | 135.29      | RT | 26.46       | RT | 16.40       | RT        | 432.94          | 34.51 | RT          | 431.81    | -              | -         | -            | -         |
| 2911+75.00       | 137.66      | RT | 24.09       | RT | 16.42       | RT        | 433.32          | 32.14 | RT          | 432.34    | -              | -         | -            | -         |
| 2911+96.68       | 139.92      | RT | 21.82       | RT | 16.45       | RT        | 433.71          | 29.87 | RT          | 432.88    | -              | -         | -            | -         |
| 2912+25.00       | 142.65      | RT | 19.10       | RT | 16.43       | RT        | 434.21          | 27.13 | RT          | 433.54    | -              | -         | -            | -         |
| 2912+50.00       | 144.84      | RT | 16.90       | RT | 16.45       | RT        | 434.64          | 24.93 | RT          | 434.11    | -              | -         | -            | -         |
| 2912+55.34       | 145.28      | RT | 16.46       | RT | 16.46       | RT        | 434.73          | 24.49 | RT          | 434.23    | -              | -         | -            | -         |
| 2912+75.00       | 146.83      | RT | 14.91       | RT | 16.50       | RT        | 435.07          | 22.93 | RT          | 434.66    | -              | -         | -            | -         |
| 2913+00.00       | 148.62      | RT | 13.12       | RT | 16.46       | RT        | 435.50          | 21.14 | RT          | 435.20    | -              | -         | -            | -         |
| 2913+25.00       | 150.21      | RT | 11.53       | RT | 16.31       | RT        | 435.94          | 19.55 | RT          | 435.73    | -              | -         | -            | -         |
| 2913+33.60       | 150.71      | RT | 11.03       | RT | 16.27       | RT        | 436.09          | 19.05 | RT          | 435.91    | -              | -         | -            | -         |
| 2913+47.38       | 151.46      | RT | 10.28       | RT | 16.29       | RT        | 436.30          | 18.29 | RT          | 436.18    | -              | -         | -            | -         |
| 2913+50.00       | 151.60      | RT | 10.14       | RT | 16.30       | RT        | 436.34          | 18.30 | RT          | 436.22    | -              | -         | -            | -         |
| 2913+75.00       | 152.79      | RT | 8.96        | RT | 16.34       | RT        | 436.73          | 18.34 | RT          | 436.61    | -              | -         | -            | -         |
| 2914+00.00       | 153.78      | RT | 7.97        | RT | 16.37       | RT        | 437.13          | 18.37 | RT          | 437.01    | -              | -         | -            | -         |
| 2914+25.00       | 154.56      | RT | 7.18        | RT | 16.39       | RT        | 437.54          | 18.39 | RT          | 437.42    | -              | -         | -            | -         |
| 2914+50.00       | 155.15      | RT | 6.59        | RT | 16.37       | RT        | 437.98          | 18.37 | RT          | 437.86    | -              | -         | -            | -         |
| 2914+51.42       | 155.18      | RT | 6.57        | RT | -           | -         | -               | -     | -           | -         | -              | -         | -            | -         |
| 2914+75.00       | -           | -  | 6.21        | RT | -           | -         | -               | -     | -           | -         | -              | -         | -            | -         |
| 2915+00.00       | -           | -  | 6.02        | RT | -           | -         | -               | -     | -           | -         | -              | -         | -            | -         |
| 2915+11.02       | -           | -  | 6.00        | RT | -           | -         | -               | -     | -           | -         | -              | -         | -            | -         |

**NOTE:**

THE CONTRACTOR SHALL CONSTRUCT THIS MEDIAN CROSSOVER USING THE ELEVATION AND OFFSET DATA TABLE FOUND ON THIS SHEET. VALUES SHOWN ARE BASED ON THE ORIGINAL ROADWAY PLANS AND FIELD SURVEY. CONTRACTOR MAY MAKE MINOR ADJUSTMENTS IN THE FIELD AS APPROVED BY THE ENGINEER.

MODEL\_PLOT  
FILE NAME: Y:\JDOT1259-11\_78504\CADD\Highway\CADD\_Sheets\0978504-rt-the-bc-04.dgn



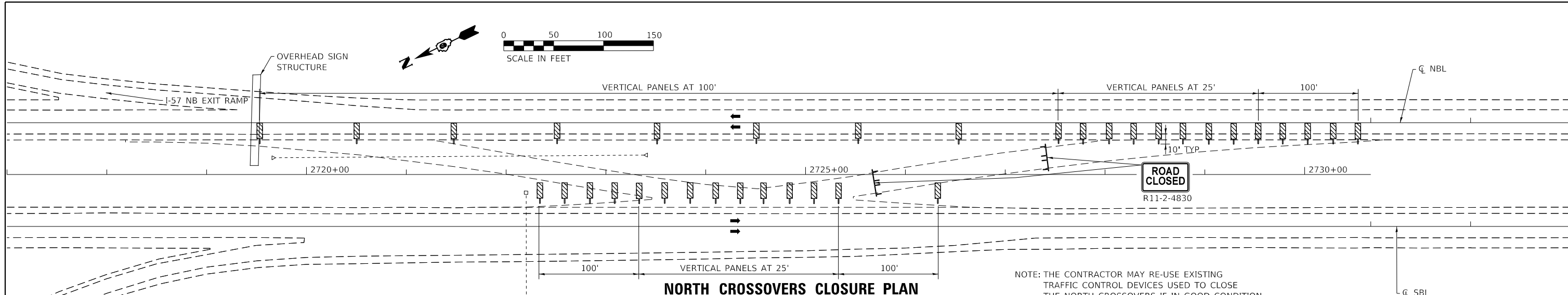
|                            |                |           |
|----------------------------|----------------|-----------|
| USER NAME = jmk            | DESIGNED - SKM | REVISED - |
| ESCA PROJECT NO. 1259.11   | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.1667' / in. | CHECKED - ELH  | REVISED - |
| PLOT DATE = 10/7/2019      | DATE - 04/19   | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SOUTH CROSSOVERS ELEVATIONS AND OFFSETS**

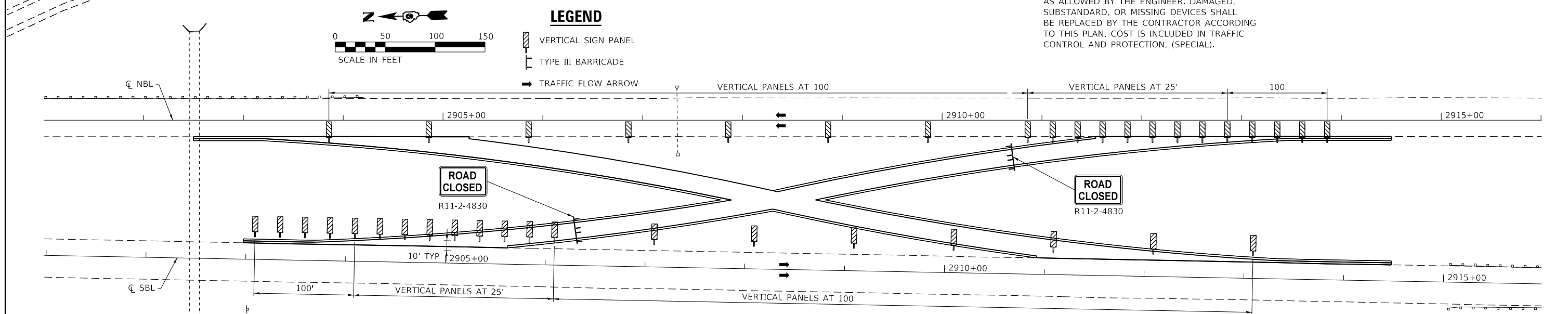
SCALE: NONE SHEET NO. 2 OF 2 SHEETS STA. TO STA.

|                           |           |        |              |           |
|---------------------------|-----------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
| 57                        | (91-3)B-2 | UNION  | 161          | 37        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |



**NORTH CROSSOVERS CLOSURE PLAN**

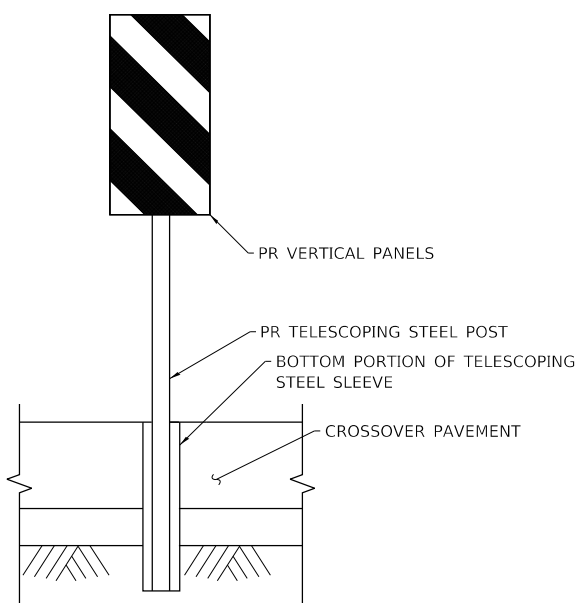
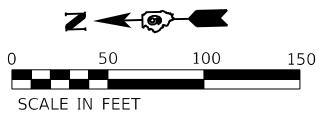
NOTE: THE CONTRACTOR MAY RE-USE EXISTING TRAFFIC CONTROL DEVICES USED TO CLOSE THE NORTH CROSSOVERS IF IN GOOD CONDITION AS ALLOWED BY THE ENGINEER. DAMAGED, SUBSTANDARD, OR MISSING DEVICES SHALL BE REPLACED BY THE CONTRACTOR ACCORDING TO THIS PLAN. COST IS INCLUDED IN TRAFFIC CONTROL AND PROTECTION, (SPECIAL).



**SOUTH CROSSOVERS CLOSURE PLAN**

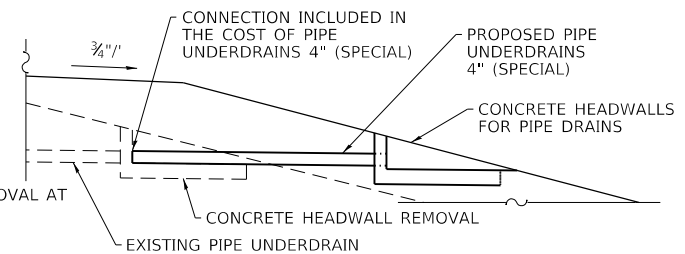
**LEGEND**

- VERTICAL SIGN PANEL
- TYPE III BARRICADE
- TRAFFIC FLOW ARROW



NOTE: FOR INSTALLATION OF VERTICAL PANELS IN AREAS OF CROSSOVER PAVEMENT, 3" DIA. HOLES WILL BE CORED THROUGH PAVEMENT FOR PLACEMENT. TELESCOPING STEEL POLES WILL BE PLACED 1" BELOW SURFACE, AND WORK SHALL BE DONE TO THE SATISFACTION OF THE ENGINEER. WHEN OPENING CROSSOVER, PANELS WILL BE REMOVED AND HOLES WILL BE LEFT IN PLACE. COST OF PANELS, POSTS, SLEEVES, TYPE III BARRICADES, SIGNS, AND CORING SHALL BE INCLUDED IN TRAFFIC CONTROL AND PROTECTION, (SPECIAL).

| CROSSOVER CLOSURE SCHEDULE |                      |                                 |                     |                    |
|----------------------------|----------------------|---------------------------------|---------------------|--------------------|
| LOCATION                   | VERTICAL SIGN PANELS | TELESCOPING STEEL SIGN SUPPORTS | TYPE III BARRICADES | SIGN PANEL, TYPE 2 |
|                            | EACH                 | EACH                            | EACH                | SQ FT              |
| NORTH CROSSOVERS           | 35                   | 35                              | 4                   | 20                 |
| SOUTH CROSSOVERS           | 40                   | 40                              | 4                   | 20                 |
| TOTALS                     | 75                   | 75                              | 8                   | 40                 |



NOTE: DURING CROSSOVERS REMOVAL AT UNDERDRAIN EXTENSIONS, THE PROPOSED PIPE UNDERDRAIN 4" (SPECIAL) EXTENSION SHALL BE REMOVED. THE CONCRETE HEADWALL FOR PIPE DRAINS SHALL BE REINSTALLED AT THE ORIGINAL END OF THE UNDERDRAIN. THIS WORK WILL BE PAID FOR AS REMOVE AND REINSTALL CONCRETE HEADWALL FOR PIPE DRAIN.

**UNDERDRAIN EXTENSION AT CROSSOVERS**

MODEL: PLOT FILE: NAME: Y:\DOT11259-11\_78504\CADD\Highway\CADD\_Sheets\DOT11259-11\_78504.dgn



USER NAME = jmk  
 ESCA PROJECT NO. 1259.11  
 PLOT SCALE = 0.1667' / in.  
 PLOT DATE = 10/7/2019

DESIGNED - SKM  
 DRAWN - SKM  
 CHECKED - ELH  
 DATE - 10/19

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

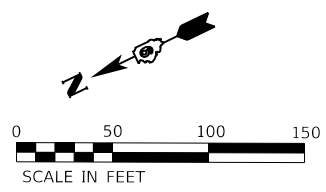
CROSSOVERS DETAILS

SCALE: 1"=50' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 38        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |



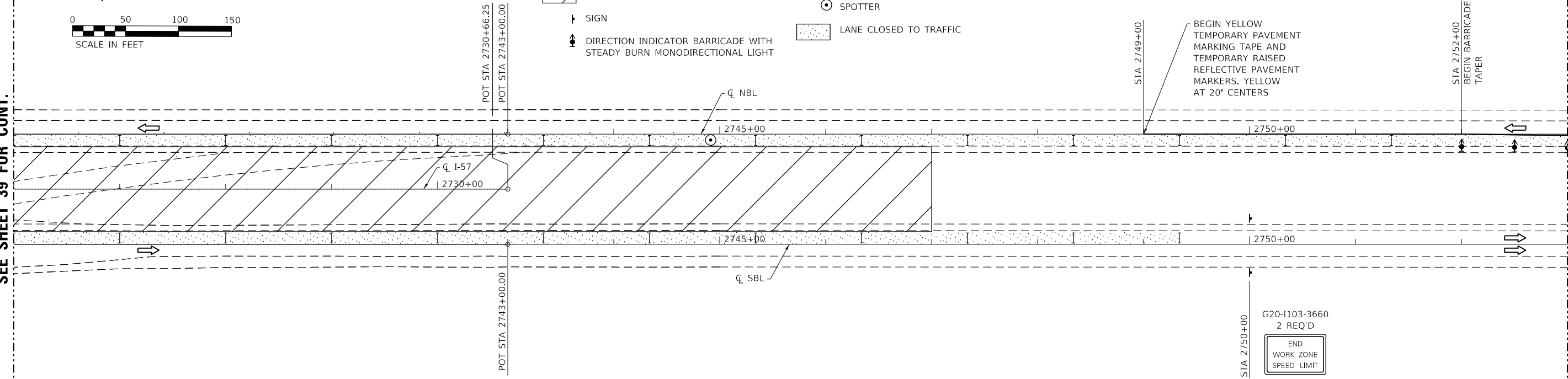




**LEGEND**

- ARROW BOARD
- WORK AREA
- SIGN
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE
- SPOTTER
- LANE CLOSED TO TRAFFIC

MATCH LINE STA 2726+00 I-57  
SEE SHEET 39 FOR CONT.



NOTE:  
SEE STANDARD 701401 AND SPECIAL PROVISIONS FOR FURTHER INFORMATION

BEGIN YELLOW TEMPORARY PAVEMENT MARKING TAPE AND TEMPORARY RAISED REFLECTIVE PAVEMENT MARKERS, YELLOW AT 20' CENTERS

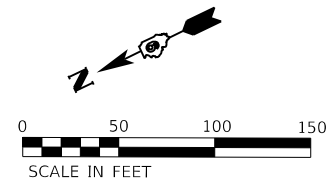
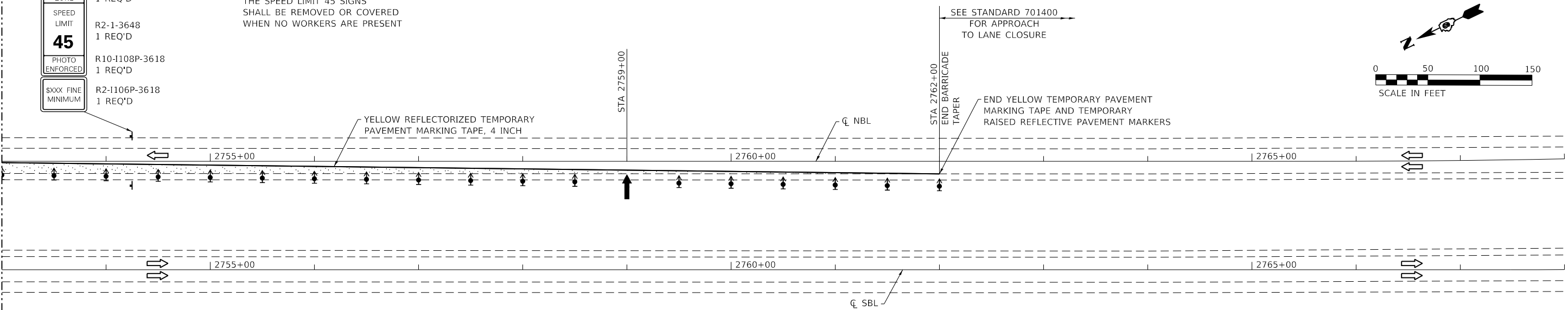
G20-1103-3660  
2 REQ'D  
END WORK ZONE SPEED LIMIT

MATCH LINE STA 2753+00 I-57

|                   |                 |
|-------------------|-----------------|
| WORK ZONE         | W2-III5(0)-3618 |
| SPEED LIMIT       | R2-1-3648       |
| 45                | 1 REQ'D         |
| PHOTO ENFORCED    | R10-1108P-3618  |
|                   | 1 REQ'D         |
| SXXX FINE MINIMUM | R2-1106P-3618   |
|                   | 1 REQ'D         |

NOTE:  
THE SPEED LIMIT 45 SIGNS SHALL BE REMOVED OR COVERED WHEN NO WORKERS ARE PRESENT

MATCH LINE STA 2753+00 I-57



MODEL\_PLOT FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\0978504-sh-stdm02.dgn



|                  |                  |
|------------------|------------------|
| USER NAME        | = jmk            |
| ESCA PROJECT NO. | 1259.11          |
| PLOT SCALE       | = 0.1667 ' / in. |
| PLOT DATE        | = 10/7/2019      |

|          |           |
|----------|-----------|
| DESIGNED | - SKM     |
| DRAWN    | - SKM/JMK |
| CHECKED  | - ELH     |
| DATE     | - 10/19   |

|         |   |
|---------|---|
| REVISED | - |
| REVISED | - |
| REVISED | - |
| REVISED | - |

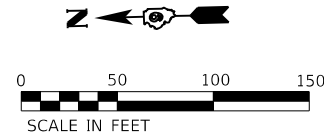
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STAGES I AND IV TRAFFIC CONTROL**

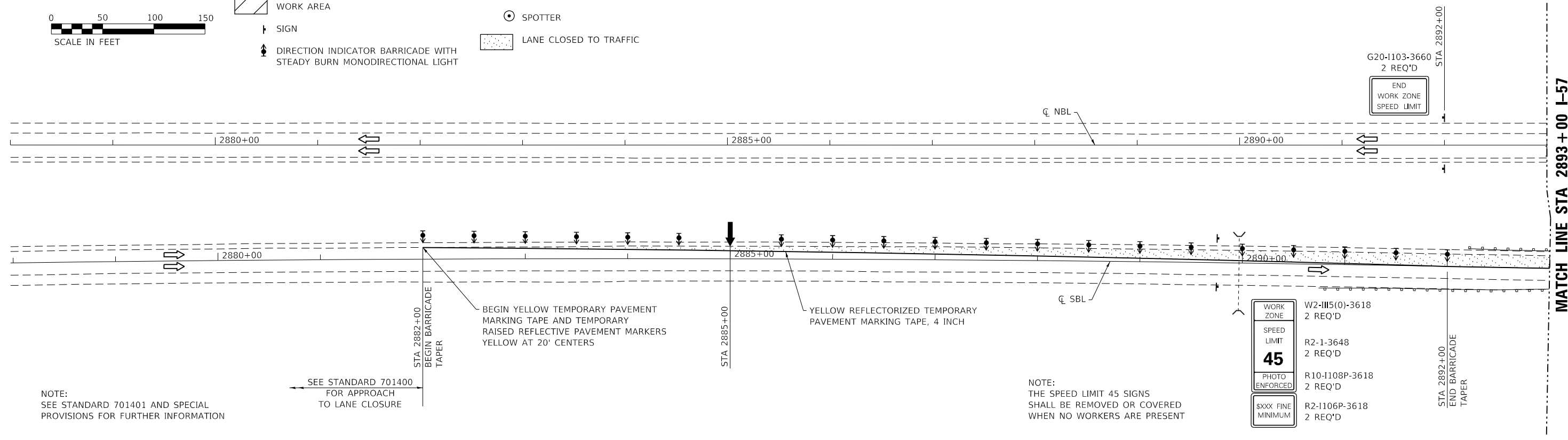
SCALE: 1"=50' SHEET NO. 2 OF 4 SHEETS STA. 2726+00 TO STA. 2768+00

|                    |           |        |                           |           |
|--------------------|-----------|--------|---------------------------|-----------|
| F.A.I. RTE.        | SECTION   | COUNTY | TOTAL SHEETS              | SHEET NO. |
| 57                 | (91-3)B-2 | UNION  | 161                       | 40        |
| CONTRACT NO. 78504 |           |        | ILLINOIS FED. AID PROJECT |           |

**LEGEND**



- ↑ ARROW BOARD
- ▨ WORK AREA
- ↑ SIGN
- ⇄ DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- ⌈ TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE
- ⊙ SPOTTER
- ▨ LANE CLOSED TO TRAFFIC

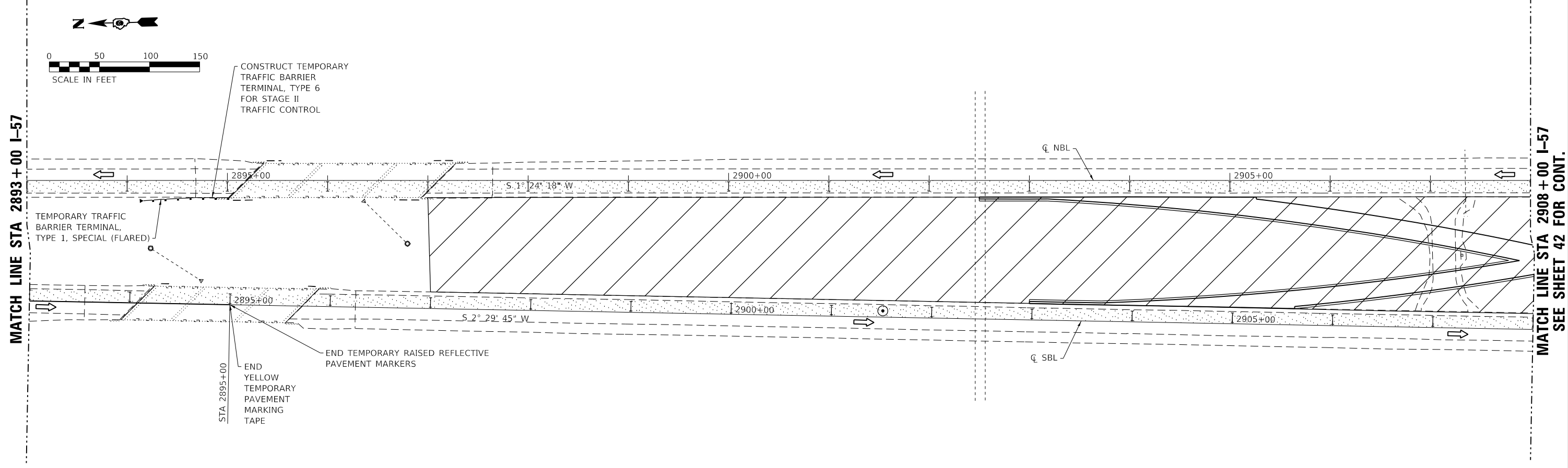


NOTE:  
SEE STANDARD 701401 AND SPECIAL PROVISIONS FOR FURTHER INFORMATION

SEE STANDARD 701400 FOR APPROACH TO LANE CLOSURE

NOTE:  
THE SPEED LIMIT 45 SIGNS SHALL BE REMOVED OR COVERED WHEN NO WORKERS ARE PRESENT

MATCH LINE STA 2893 + 00 I-57



MATCH LINE STA 2908 + 00 I-57  
SEE SHEET 42 FOR CONT.

MODEL\_PLOT FILE NAME: Y:\DOT11259-11\_78504\CADD\Highway\CADD\_Sheets\DOT11259-11\_78504-11-2893-00-2908-00.dwg



|                            |                 |           |
|----------------------------|-----------------|-----------|
| USER NAME = jmk            | DESIGNED - SKM  | REVISED - |
| ESCA PROJECT NO. 1259.11   | DRAWN - SKM/JMK | REVISED - |
| PLOT SCALE = 0.1667' / in. | CHECKED - ELH   | REVISED - |
| PLOT DATE = 10/7/2019      | DATE - 10/19    | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

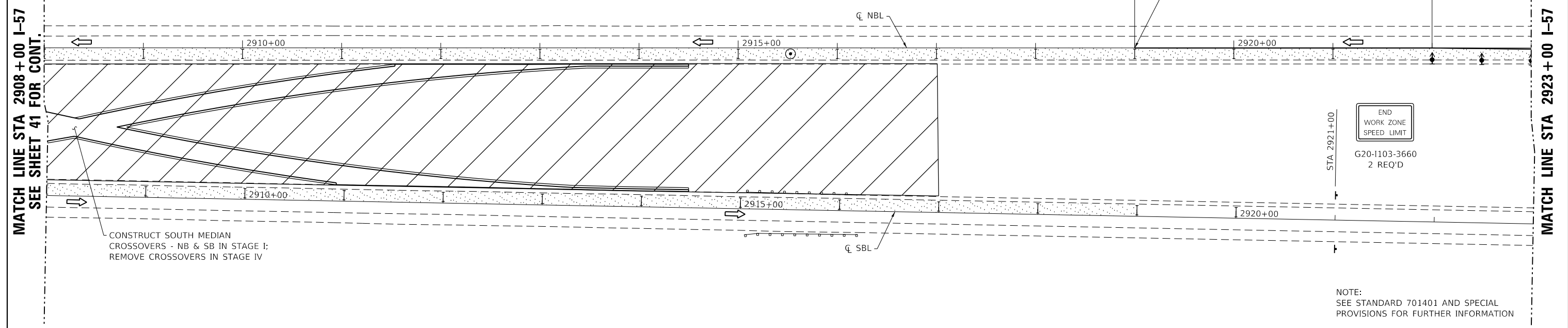
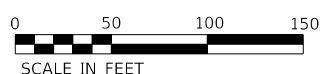
**STAGES I AND IV TRAFFIC CONTROL**

SCALE: 1"=50' SHEET NO. 3 OF 4 SHEETS STA. 2878+00 TO STA. 2908+00

|                    |           |        |                           |           |
|--------------------|-----------|--------|---------------------------|-----------|
| F.A.I. RTE.        | SECTION   | COUNTY | TOTAL SHEETS              | SHEET NO. |
| 57                 | (91-3)B-2 | UNION  | 161                       | 41        |
| CONTRACT NO. 78504 |           |        | ILLINOIS FED. AID PROJECT |           |

**LEGEND**

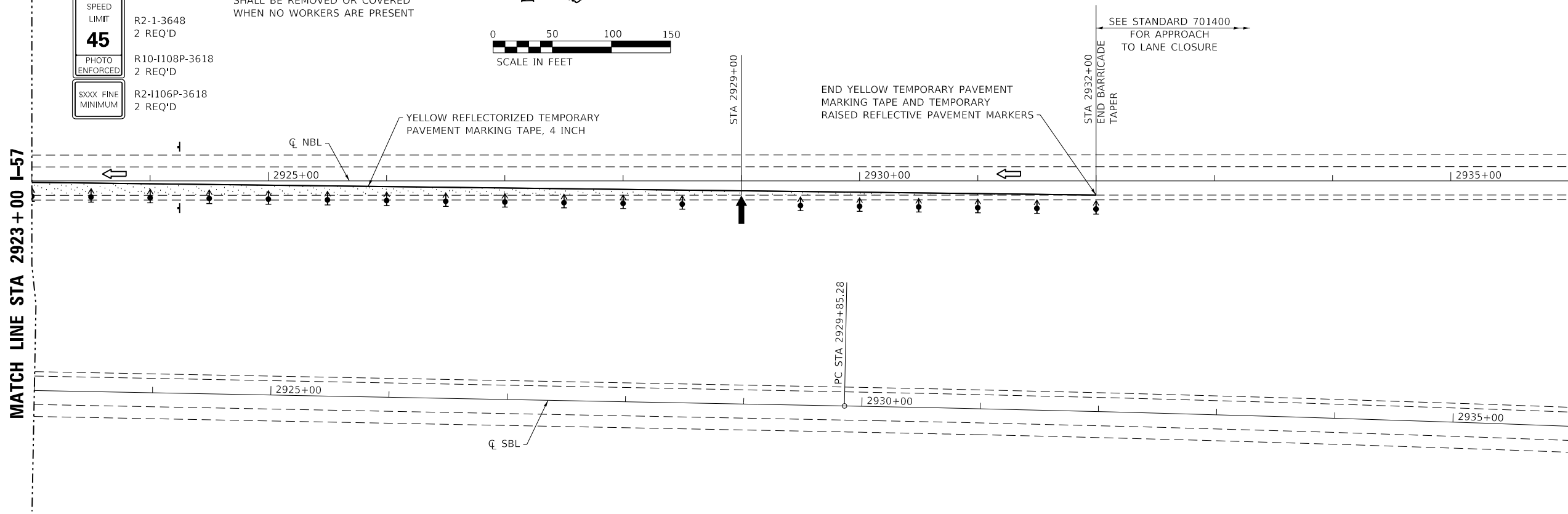
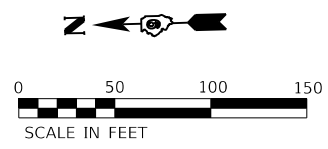
- ARROW BOARD
- WORK AREA
- SIGN
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE
- SPOTTER
- LANE CLOSED TO TRAFFIC



NOTE:  
SEE STANDARD 701401 AND SPECIAL PROVISIONS FOR FURTHER INFORMATION

WORK ZONE: W2-III(0)-3618 2 REQ'D  
 SPEED LIMIT: R2-1-3648 2 REQ'D  
**45**  
 PHOTO ENFORCED: R10-1108P-3618 2 REQ'D  
 SXXX FINE MINIMUM: R2-1106P-3618 2 REQ'D

NOTE:  
THE SPEED LIMIT 45 SIGNS SHALL BE REMOVED OR COVERED WHEN NO WORKERS ARE PRESENT



SEE STANDARD 701400 FOR APPROACH TO LANE CLOSURE

MODEL\_PLOT FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\0978504-11-std-19.dgn



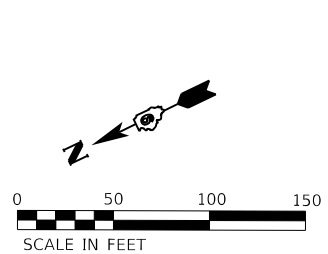
|                            |                 |           |
|----------------------------|-----------------|-----------|
| USER NAME = jmk            | DESIGNED - SKM  | REVISED - |
| ESCA PROJECT NO. 1259.11   | DRAWN - SKM/JMK | REVISED - |
| PLOT SCALE = 0.1667' / in. | CHECKED - ELH   | REVISED - |
| PLOT DATE = 10/7/2019      | DATE - 04/19    | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STAGES I AND IV TRAFFIC CONTROL**

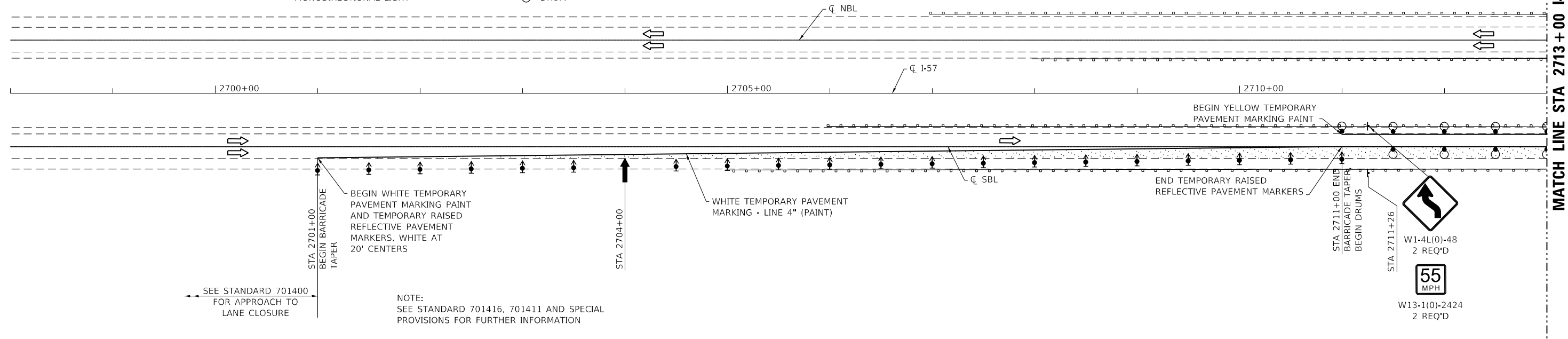
SCALE: 1"=50' SHEET NO. 4 OF 4 SHEETS STA. 2908+00 TO STA. 2936+00

| F.A.I. RTE.        | SECTION   | COUNTY | TOTAL SHEETS              | SHEET NO. |
|--------------------|-----------|--------|---------------------------|-----------|
| 57                 | (91-3)B-2 | UNION  | 161                       | 42        |
| CONTRACT NO. 78504 |           |        | ILLINOIS FED. AID PROJECT |           |



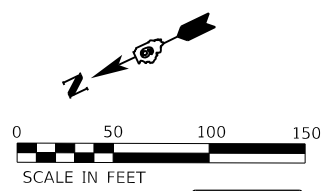
**LEGEND**

- ↑ ARROW BOARD
- ▨ WORK AREA
- ↑ SIGN
- ↑ DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- ↑ TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- ▨ VERTICAL PANEL (BACK TO BACK)
- ↑ TYPE III BARRICADE WITH FLASHING LIGHTS
- TEMPORARY CONCRETE BARRIER
- ▨ LANE CLOSED TO TRAFFIC
- DRUM WITH STEADY BURN MONODIRECTIONAL LIGHT
- DRUM

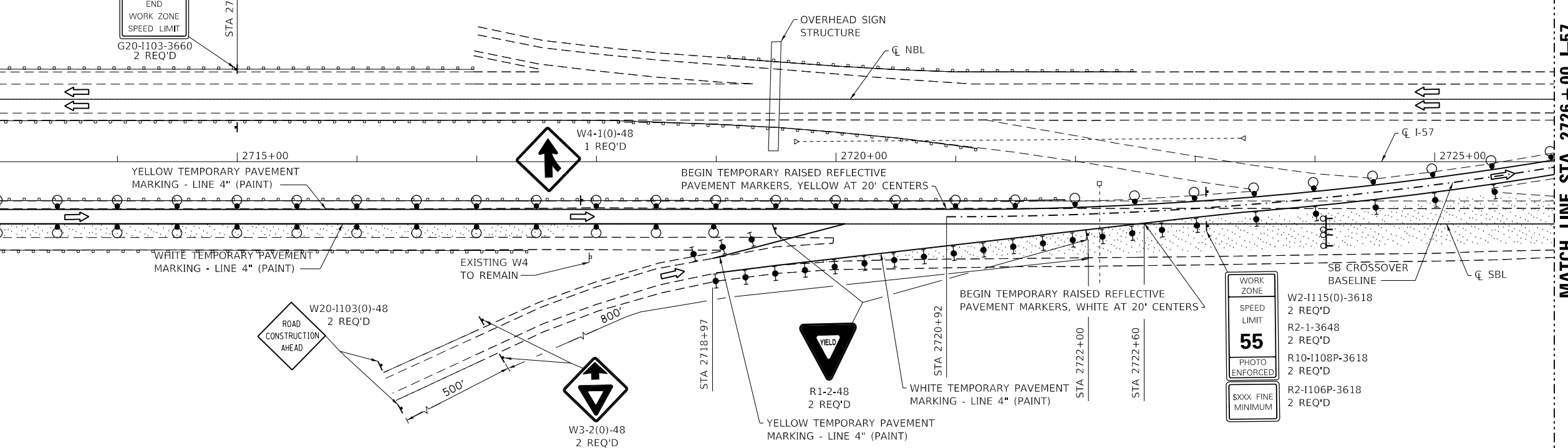


SEE STANDARD 701400 FOR APPROACH TO LANE CLOSURE

NOTE: SEE STANDARD 701416, 701411 AND SPECIAL PROVISIONS FOR FURTHER INFORMATION



MATCH LINE STA 2713 + 00 I-57



YELLOW TEMPORARY PAVEMENT MARKING - LINE 4" (PAINT)

WHITE TEMPORARY PAVEMENT MARKING - LINE 4" (PAINT)

EXISTING W4 TO REMAIN

W4-1(0)-48  
1 REQ'D

W20-1103(0)-48  
2 REQ'D

W3-2(0)-48  
2 REQ'D

R1-2-48  
2 REQ'D

YELLOW TEMPORARY PAVEMENT MARKING - LINE 4" (PAINT)

BEGIN TEMPORARY RAISED REFLECTIVE PAVEMENT MARKERS, WHITE AT 20' CENTERS

WHITE TEMPORARY PAVEMENT MARKING - LINE 4" (PAINT)

WORK ZONE  
SPEED LIMIT  
**55**  
PHOTO ENFORCED  
\$XXX FINE MINIMUM

- W2-1115(0)-3618  
2 REQ'D
- R2-1-3648  
2 REQ'D
- R10-1108P-3618  
2 REQ'D
- R2-1106P-3618  
2 REQ'D

MATCH LINE STA 2726 + 00 I-57  
SEE SHEET 44 FOR CONT.

MODEL\_PLOT FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\0978504-11-27-13\1259-11-27-13.dwg



|                            |                 |           |
|----------------------------|-----------------|-----------|
| USER NAME = jmk            | DESIGNED - SKM  | REVISED - |
| ESCA PROJECT NO. 1259.11   | DRAWN - SKM/JMK | REVISED - |
| PLOT SCALE = 0.1667' / in. | CHECKED - ELH   | REVISED - |
| PLOT DATE = 10/7/2019      | DATE - 04/19    | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

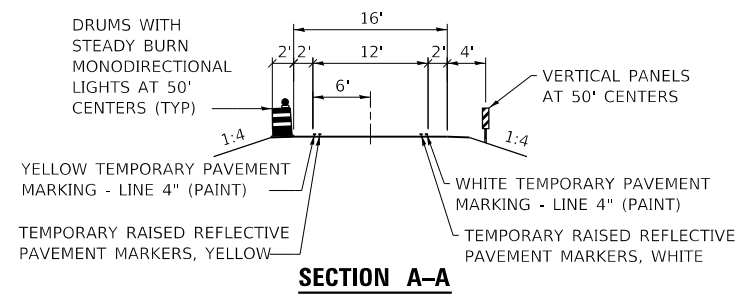
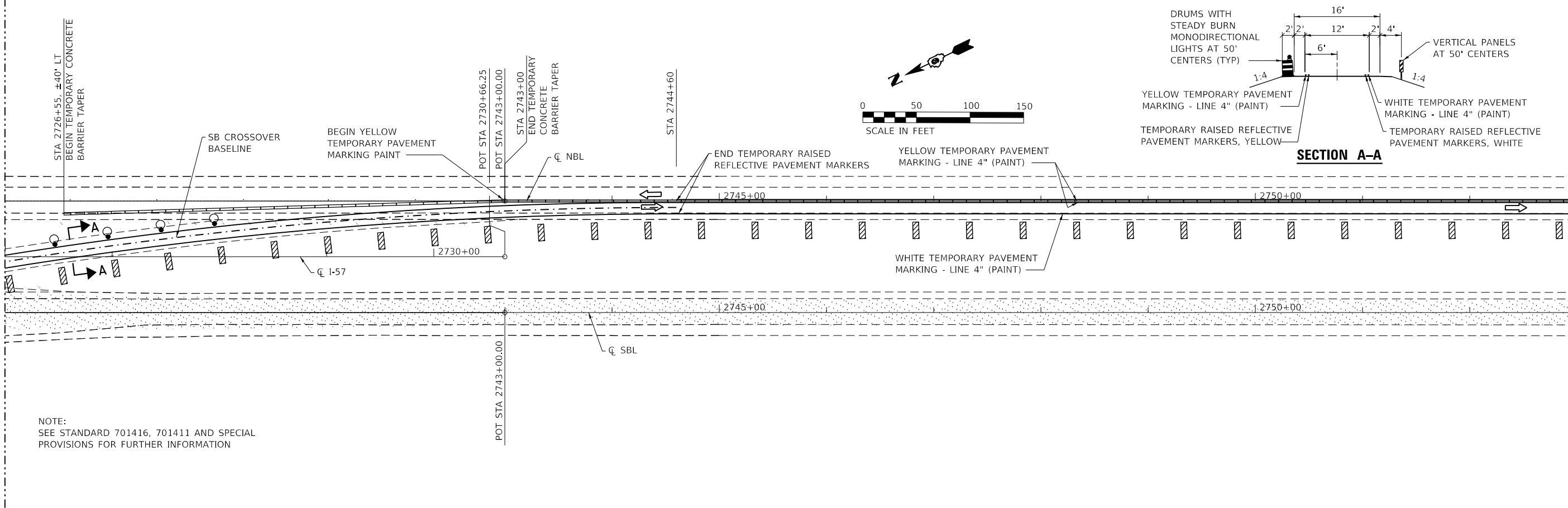
**STAGE II TRAFFIC CONTROL**

SCALE: 1"=50' SHEET NO. 1 OF 4 SHEETS STA. 2698+00 TO STA. 2726+00

|                           |           |        |              |           |
|---------------------------|-----------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
| 57                        | (91-3)B-2 | UNION  | 161          | 43        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |

MATCH LINE STA 2726+00 I-57  
SEE SHEET 43 FOR CONT.

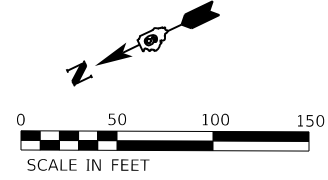
MATCH LINE STA 2753+00 I-57



NOTE:  
SEE STANDARD 701416, 701411 AND SPECIAL  
PROVISIONS FOR FURTHER INFORMATION

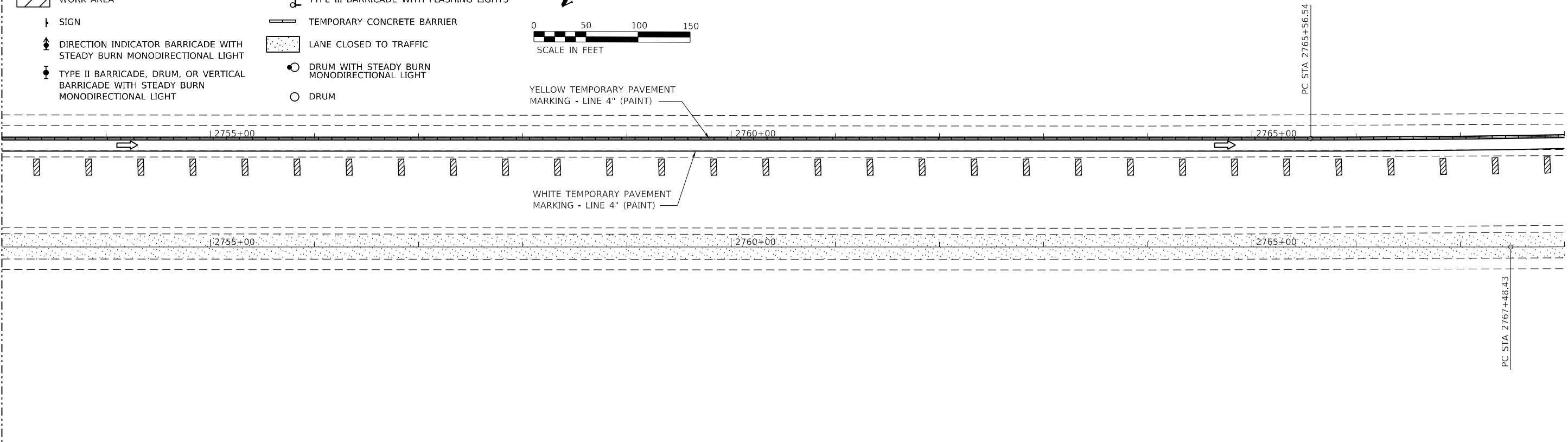
**LEGEND**

- ARROW BOARD
- WORK AREA
- SIGN
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- VERTICAL PANEL (BACK TO BACK)
- TYPE III BARRICADE WITH FLASHING LIGHTS
- TEMPORARY CONCRETE BARRIER
- LANE CLOSED TO TRAFFIC
- DRUM WITH STEADY BURN MONODIRECTIONAL LIGHT
- DRUM



NOTE:  
CONTINUE TRAFFIC CONTROL  
FROM STA 2768+00 TO  
STA 2887+00

MATCH LINE STA 2753+00 I-57



PC STA 2767+48.43

MODEL\_PLOT FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\0978504-sh-stdtrf06.dgn



|                            |                 |           |
|----------------------------|-----------------|-----------|
| USER NAME = jmk            | DESIGNED - SKM  | REVISED - |
| ESCA PROJECT NO. 1259.11   | DRAWN - SKM/JMK | REVISED - |
| PLOT SCALE = 0.1667' / in. | CHECKED - ELH   | REVISED - |
| PLOT DATE = 10/7/2019      | DATE - 04/19    | REVISED - |












**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

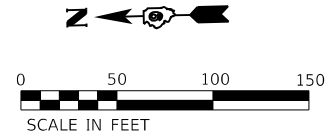
**STAGE II TRAFFIC CONTROL**

SCALE: 1"=50' SHEET NO. 2 OF 4 SHEETS STA. 2726+00 TO STA. 2768+00

| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 44        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |

**LEGEND**

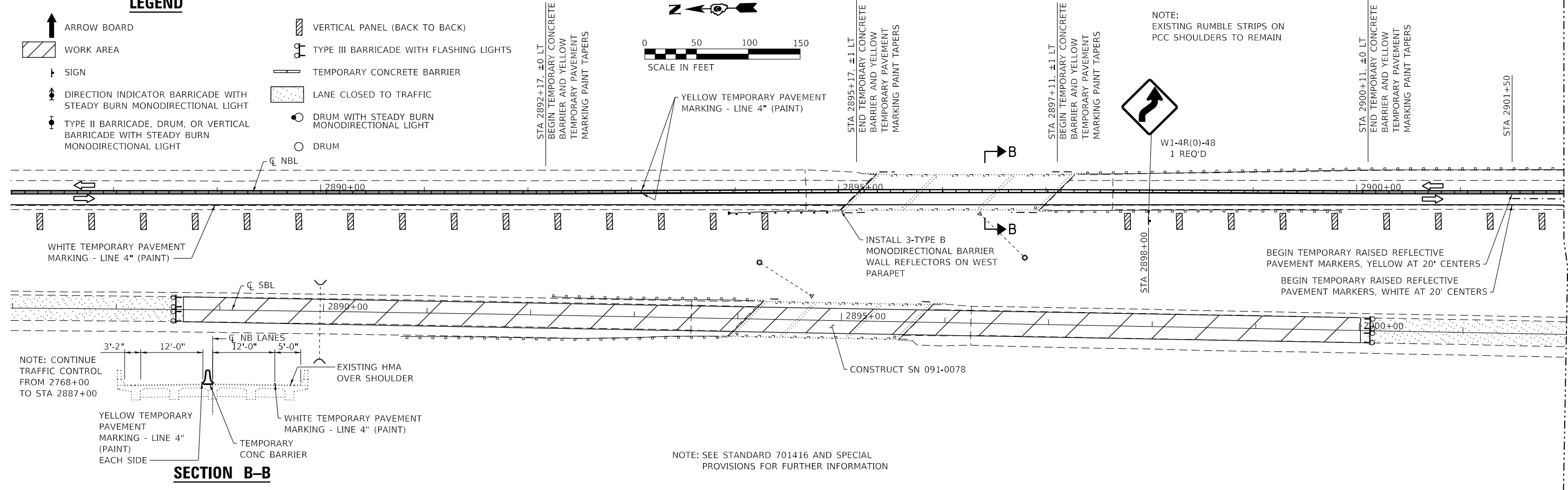
-  ARROW BOARD
-  WORK AREA
-  SIGN
-  DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
-  TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
-  VERTICAL PANEL (BACK TO BACK)
-  TYPE III BARRICADE WITH FLASHING LIGHTS
-  TEMPORARY CONCRETE BARRIER
-  LANE CLOSED TO TRAFFIC
-  DRUM WITH STEADY BURN MONODIRECTIONAL LIGHT
-  DRUM



NOTE:  
EXISTING RUMBLE STRIPS ON  
PCC SHOULDERS TO REMAIN



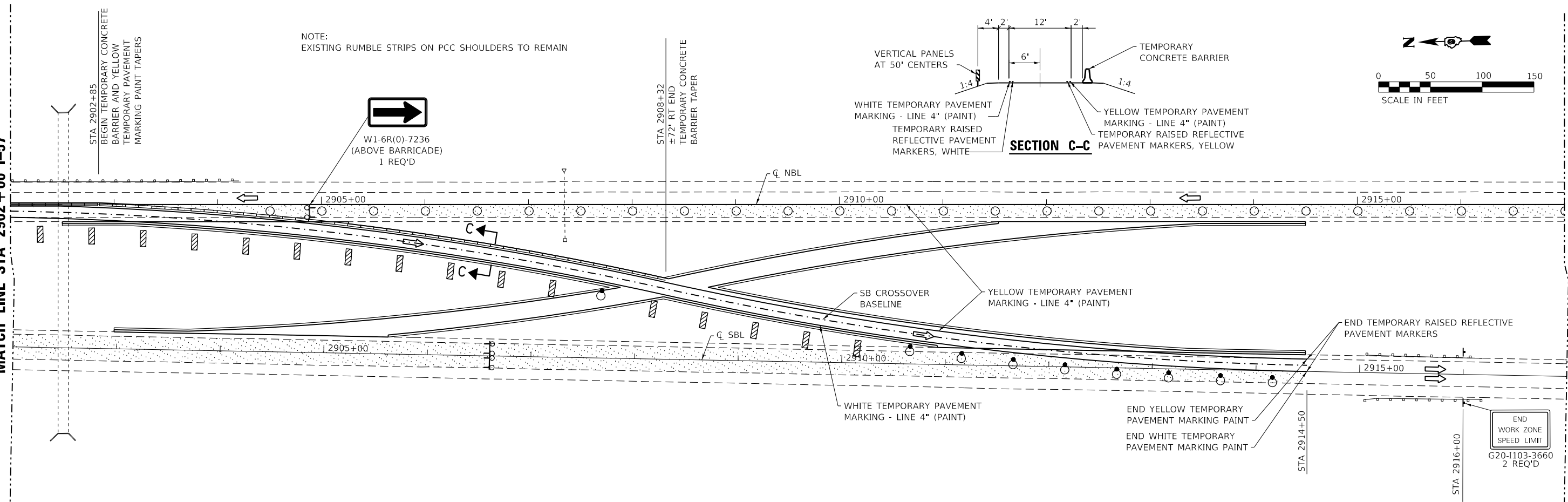
W1-4R(0)-48  
1 REQ'D



**SECTION B-B**

NOTE: SEE STANDARD 701416 AND SPECIAL PROVISIONS FOR FURTHER INFORMATION

MATCH LINE STA 2902 + 00 I-57



**SECTION C-C**

MATCH LINE STA 2917 + 00 I-57  
SEE SHEET 46 FOR CONT.

MODEL\_PLOT  
FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\0978504-ht-stdrpt07.dgn



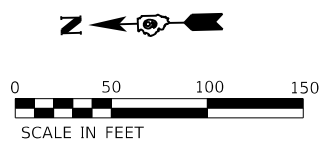
|                            |                 |           |
|----------------------------|-----------------|-----------|
| USER NAME = jmk            | DESIGNED - SKM  | REVISED - |
| ESCA PROJECT NO. 1259.11   | DRAWN - SKM/JMK | REVISED - |
| PLOT SCALE = 0.1667' / in. | CHECKED - ELH   | REVISED - |
| PLOT DATE = 10/7/2019      | DATE - 08/19    | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STAGE II TRAFFIC CONTROL**

SCALE: 1"=50' SHEET NO. 3 OF 4 SHEETS STA. 2887+00 TO STA. 2917+00

| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 45        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |

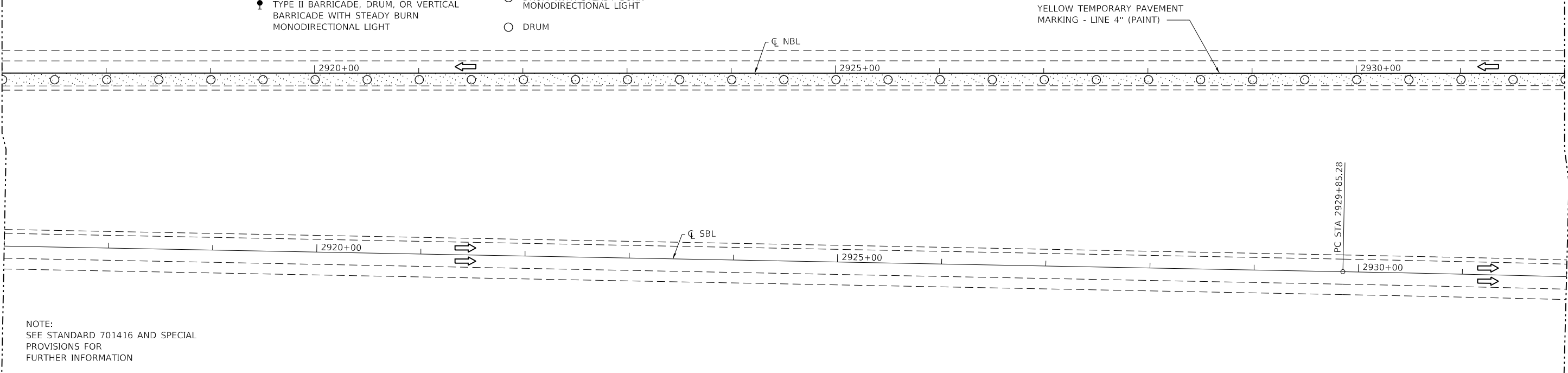


**LEGEND**

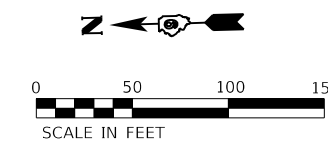
- ARROW BOARD
- WORK AREA
- SIGN
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- VERTICAL PANEL (BACK TO BACK)
- TYPE III BARRICADE WITH FLASHING LIGHTS
- TEMPORARY CONCRETE BARRIER
- LANE CLOSED TO TRAFFIC
- DRUM WITH STEADY BURN MONODIRECTIONAL LIGHT
- DRUM

MATCH LINE STA 2917+00 I-57  
SEE SHEET 45 FOR CONT.

MATCH LINE STA 2932+00 I-57

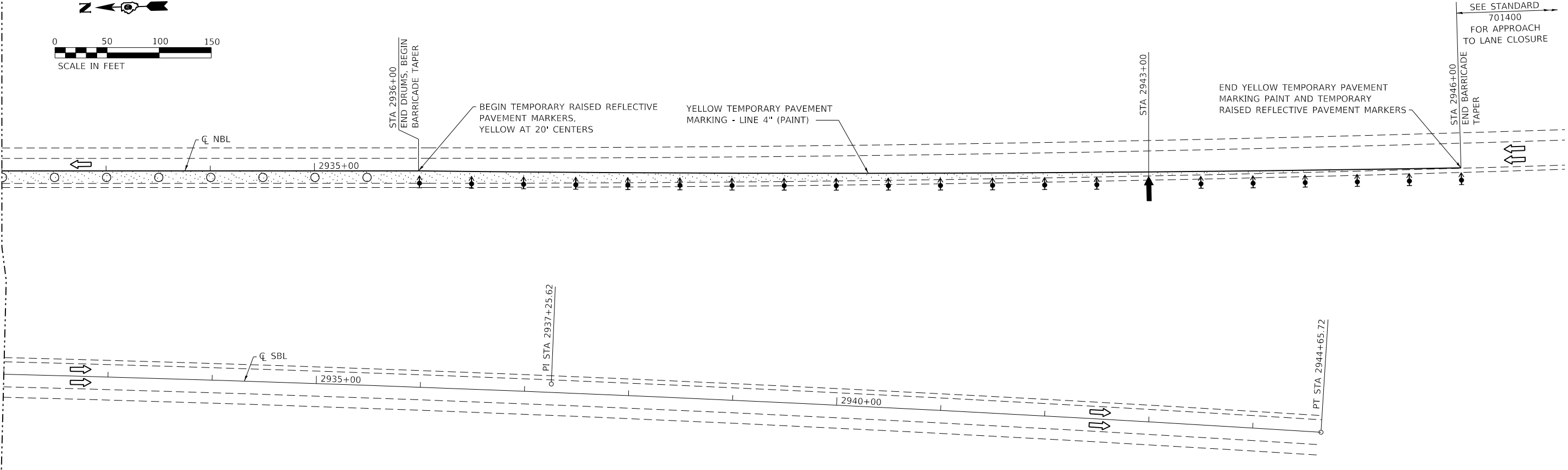


NOTE:  
SEE STANDARD 701416 AND SPECIAL PROVISIONS FOR FURTHER INFORMATION



MATCH LINE STA 2932+00 I-57

SEE STANDARD 701400 FOR APPROACH TO LANE CLOSURE



MODEL\_PLOT  
FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\0978504-ht-stage08.dgn



|                            |                 |           |
|----------------------------|-----------------|-----------|
| USER NAME = jmk            | DESIGNED - SKM  | REVISED - |
| ESCA PROJECT NO. 1259.11   | DRAWN - SKM/JMK | REVISED - |
| PLOT SCALE = 0.1667' / in. | CHECKED - ELH   | REVISED - |
| PLOT DATE = 10/7/2019      | DATE - 04/19    | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**





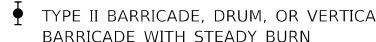


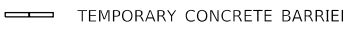
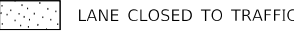


**STAGE II TRAFFIC CONTROL**

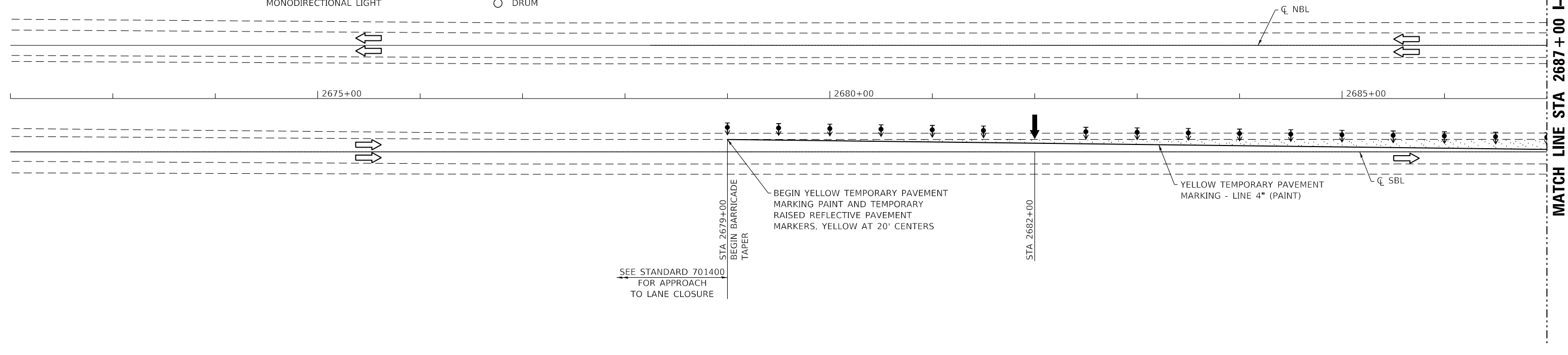
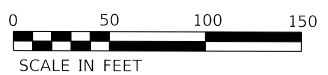
SCALE: 1"=50'    SHEET NO. 4 OF 4 SHEETS    STA. 2917+00 TO STA. 2947+00

|                    |           |                  |              |           |
|--------------------|-----------|------------------|--------------|-----------|
| F.A.I. RTE.        | SECTION   | COUNTY           | TOTAL SHEETS | SHEET NO. |
| 57                 | (91-3)B-2 | UNION            | 161          | 46        |
| CONTRACT NO. 78504 |           |                  |              |           |
| ILLINOIS           |           | FED. AID PROJECT |              |           |

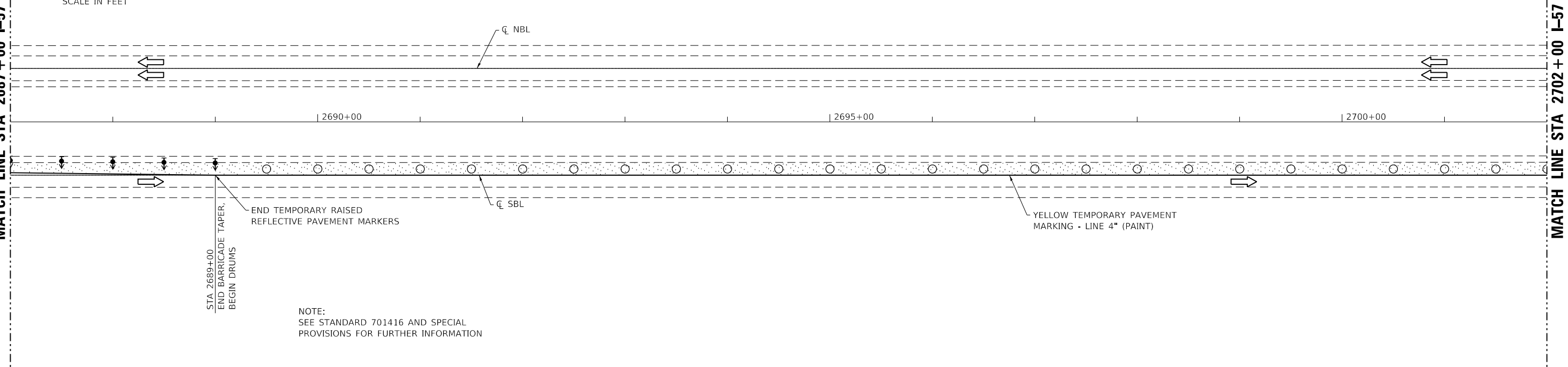
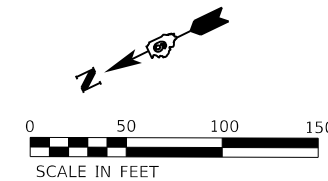


**LEGEND**

-  ARROW BOARD
-  WORK AREA
-  SIGN
-  DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
-  TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
-  VERTICAL PANEL (BACK TO BACK)
-  TYPE III BARRICADE WITH FLASHING LIGHTS
-  TEMPORARY CONCRETE BARRIER
-  LANE CLOSED TO TRAFFIC
-  DRUM WITH STEADY BURN MONODIRECTIONAL LIGHT
-  DRUM



MATCH LINE STA 2687 + 00 I-57



MATCH LINE STA 2702 + 00 I-57  
SEE SHEET 48 FOR CONT.

MODEL\_PLOT  
FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\0978504-sh-traffic09.dgn



|                            |
|----------------------------|
| USER NAME = jmk            |
| ESCA PROJECT NO. 1259.11   |
| PLOT SCALE = 0.1667' / in. |
| PLOT DATE = 10/7/2019      |

|                 |           |
|-----------------|-----------|
| DESIGNED - SKM  | REVISED - |
| DRAWN - SKM/JMK | REVISED - |
| CHECKED - ELH   | REVISED - |
| DATE - 04/19    | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STAGE III TRAFFIC CONTROL**

SCALE: 1"=50' SHEET NO. 1 OF 4 SHEETS STA. 2672+00 TO STA. 2702+00

| F.A.I. RTE.        | SECTION   | COUNTY | TOTAL SHEETS              | SHEET NO. |
|--------------------|-----------|--------|---------------------------|-----------|
| 57                 | (91-3)B-2 | UNION  | 161                       | 47        |
| CONTRACT NO. 78504 |           |        | ILLINOIS FED. AID PROJECT |           |

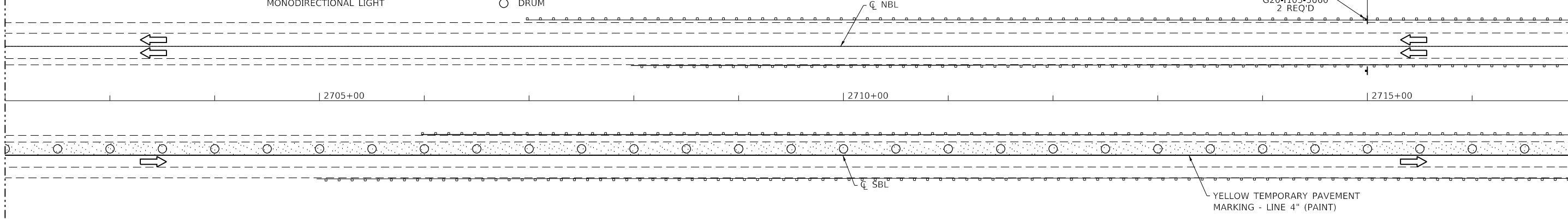
**LEGEND**

- ARROW BOARD
- WORK AREA
- SIGN
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- VERTICAL PANEL (BACK TO BACK)
- TYPE III BARRICADE WITH FLASHING LIGHTS
- TEMPORARY CONCRETE BARRIER
- LANE CLOSED TO TRAFFIC
- DRUM WITH STEADY BURN MONODIRECTIONAL LIGHT
- DRUM



MATCH LINE STA 2702+00 I-57  
SEE SHEET 47 FOR CONT.

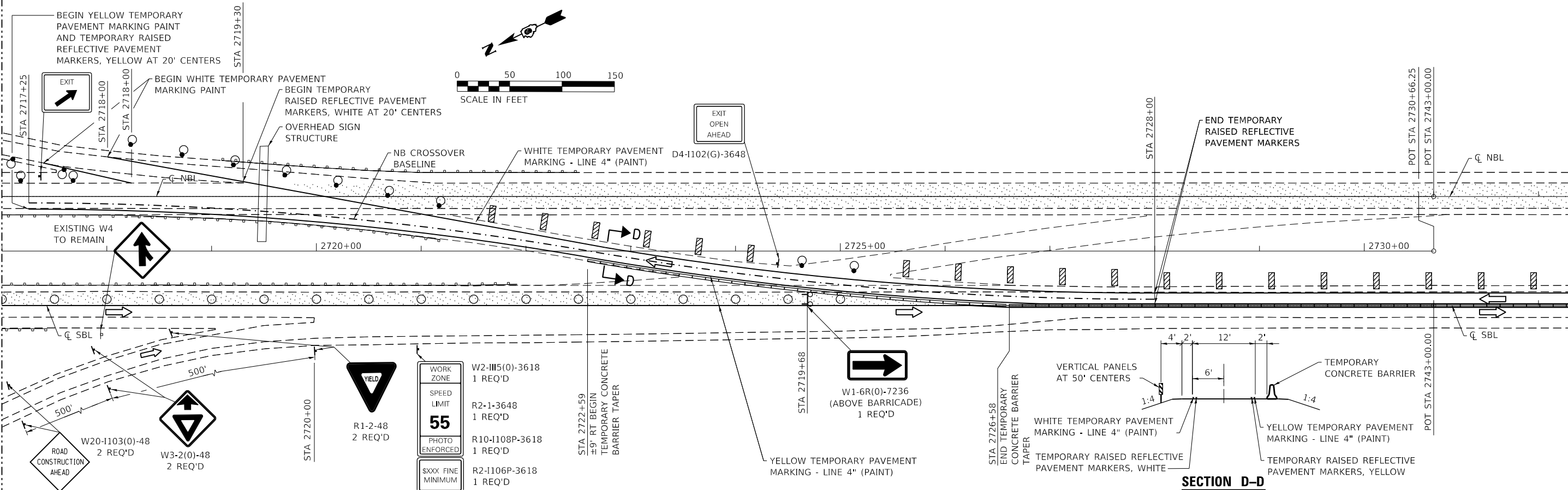
MATCH LINE STA 2717+00 I-57



NOTE:  
SEE STANDARD 701416 AND SPECIAL PROVISIONS FOR FURTHER INFORMATION

MATCH LINE STA 2717+00 I-57

MATCH LINE STA 2743+00 I-57



**SECTION D-D**

|                   |                 |         |
|-------------------|-----------------|---------|
| WORK ZONE         | W2-III5(0)-3618 | 1 REQ'D |
| SPEED LIMIT       | R2-1-3648       | 1 REQ'D |
| PHOTO ENFORCED    | R10-1108P-3618  | 1 REQ'D |
| XXXX FINE MINIMUM | R2-1106P-3618   | 1 REQ'D |

MODEL: PLOT FILE: NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\0978504-8th-stage\10.dgn



USER NAME = jmk  
ESCA PROJECT NO. 1259.11  
PLOT SCALE = 0.1667' / in.  
PLOT DATE = 10/7/2019

DESIGNED - SKM  
DRAWN - SKM/JMK  
CHECKED - ELH  
DATE - 10/19

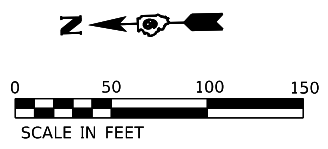
REVISED -  
REVISED -  
REVISED -  
REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STAGE III TRAFFIC CONTROL**

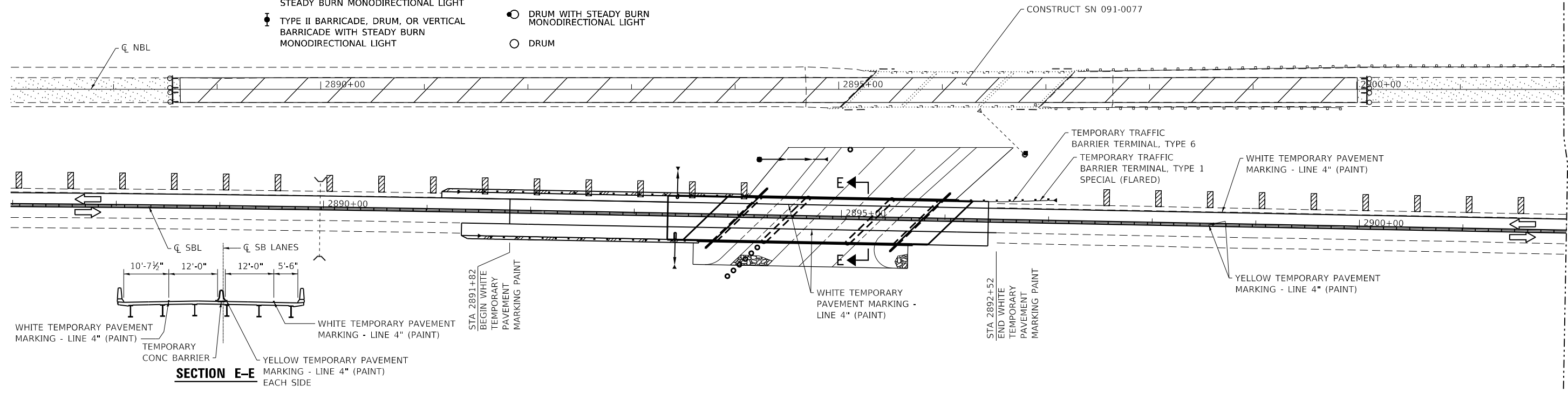
SCALE: 1"=50' SHEET NO. 2 OF 4 SHEETS STA. 2702+00 TO STA. 2744+00

|                    |           |        |                           |           |
|--------------------|-----------|--------|---------------------------|-----------|
| F.A.I. RTE.        | SECTION   | COUNTY | TOTAL SHEETS              | SHEET NO. |
| 57                 | (91-3)B-2 | UNION  | 161                       | 48        |
| CONTRACT NO. 78504 |           |        | ILLINOIS FED. AID PROJECT |           |



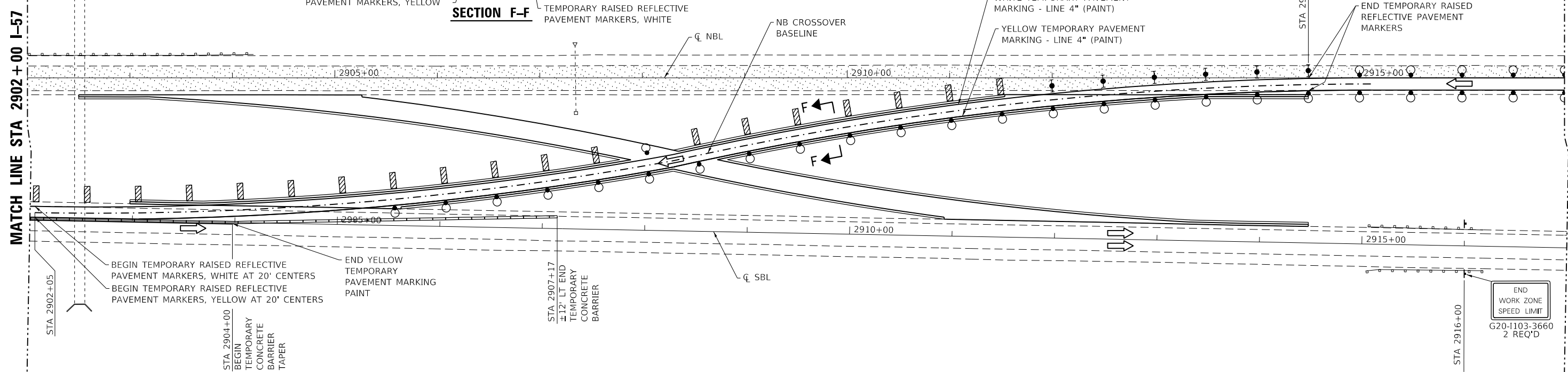
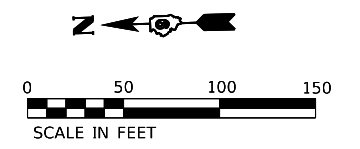
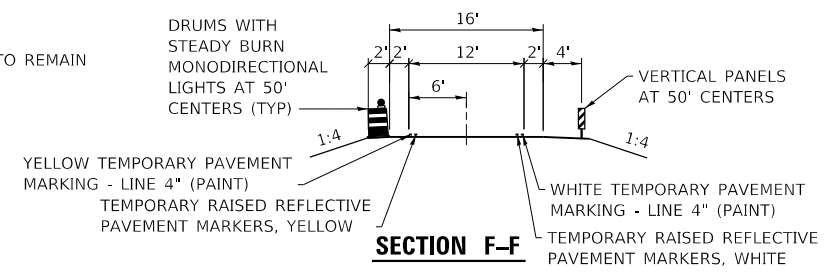
**LEGEND**

- ↑ ARROW BOARD
- ▨ WORK AREA
- ↑ SIGN
- ↔ DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- ⊕ TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- ▨ VERTICAL PANEL (BACK TO BACK)
- ⊕ TYPE III BARRICADE WITH FLASHING LIGHTS
- ▬ TEMPORARY CONCRETE BARRIER
- ▨ LANE CLOSED TO TRAFFIC
- ⊕ DRUM WITH STEADY BURN MONODIRECTIONAL LIGHT
- DRUM



MATCH LINE STA 2902+00 I-57

NOTE:  
EXISTING RUMBLE STRIPS ON PCC SHOULDER TO REMAIN



MATCH LINE STA 2917+00 I-57  
SEE SHEET 50 FOR CONT.

MODEL: PLOT  
FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\0978504-11-11-11.dgn



USER NAME = jmk  
ESCA PROJECT NO. 1259.11  
PLOT SCALE = 0.1667' / in.  
PLOT DATE = 10/7/2019

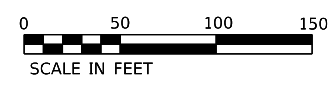
|                 |           |
|-----------------|-----------|
| DESIGNED - SKM  | REVISED - |
| DRAWN - SKM/JMK | REVISED - |
| CHECKED - ELH   | REVISED - |
| DATE - 04/19    | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STAGE III TRAFFIC CONTROL**

SCALE: 1"=50' SHEET NO. 3 OF 4 SHEETS STA. 2887+00 TO STA. 2917+00

| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 49        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |



**LEGEND**

- ↑ ARROW BOARD
- ▨ WORK AREA
- ⊥ SIGN
- ↕ DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- ⊥ TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- ▨ VERTICAL PANEL (BACK TO BACK)
- ⊥ TYPE III BARRICADE WITH FLASHING LIGHTS
- TEMPORARY CONCRETE BARRIER
- ▨ LANE CLOSED TO TRAFFIC
- DRUM WITH STEADY BURN MONODIRECTIONAL LIGHT
- DRUM



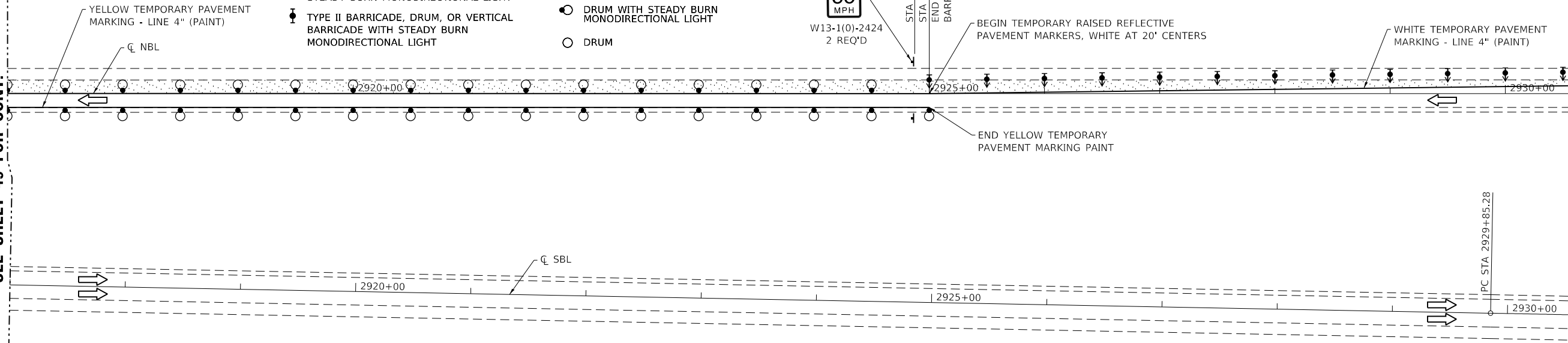
W1-4L(0)-48  
2 REQ'D



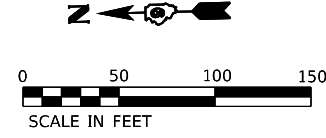
W13-1(0)-2424  
2 REQ'D

MATCH LINE STA 2917+00 I-57  
SEE SHEET 49 FOR CONT.

MATCH LINE STA 2931+00 I-57



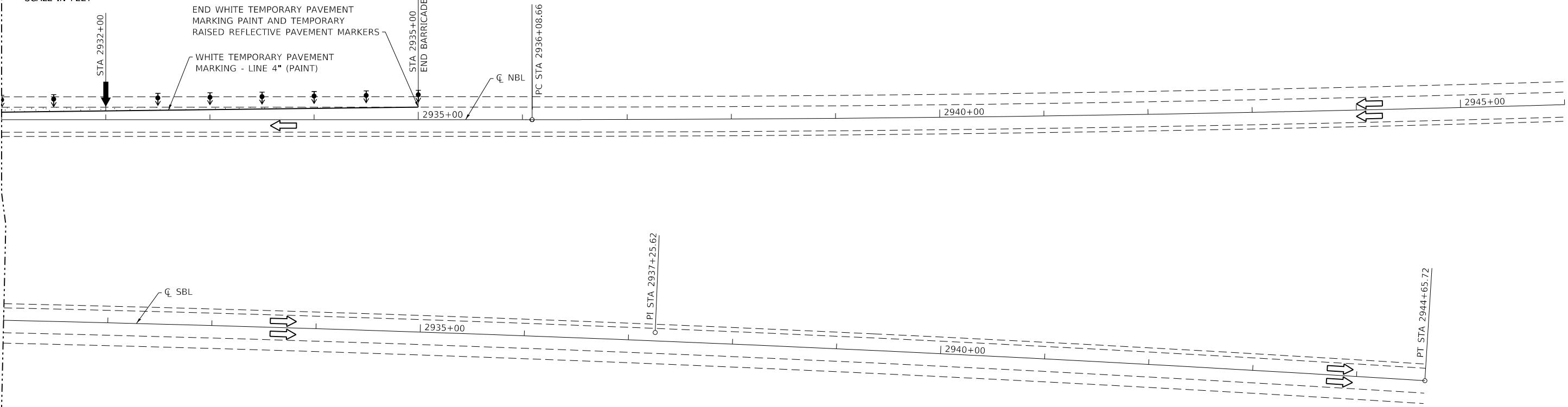
NOTE:  
SEE STANDARD 701416 AND SPECIAL PROVISIONS FOR FURTHER INFORMATION



SEE STANDARD 701400 FOR APPROACH TO LANE CLOSURE

MATCH LINE STA 2931+00 I-57

2945+00



MODEL: PLOT  
FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\0978504-11-11-12.dgn



USER NAME = jmk  
ESCA PROJECT NO. 1259.11  
PLOT SCALE = 0.1667' / in.  
PLOT DATE = 10/7/2019

DESIGNED - SKM  
DRAWN - SKM/JMK  
CHECKED - ELH  
DATE - 04/19

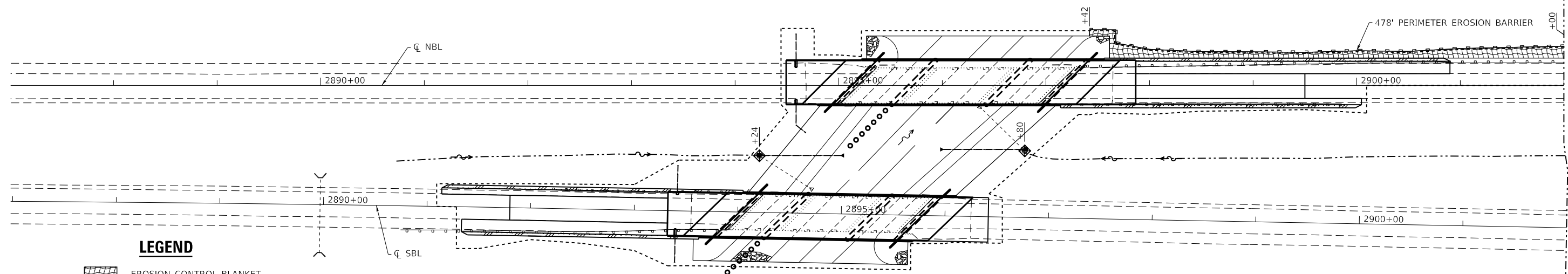
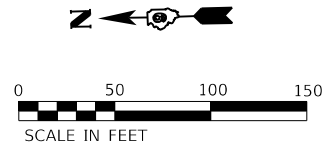
REVISED -  
REVISED -  
REVISED -  
REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**







**STAGE III TRAFFIC CONTROL**

SCALE: 1"=50' SHEET NO. 4 OF 4 SHEETS STA. 2917+00 TO STA. 2946+00

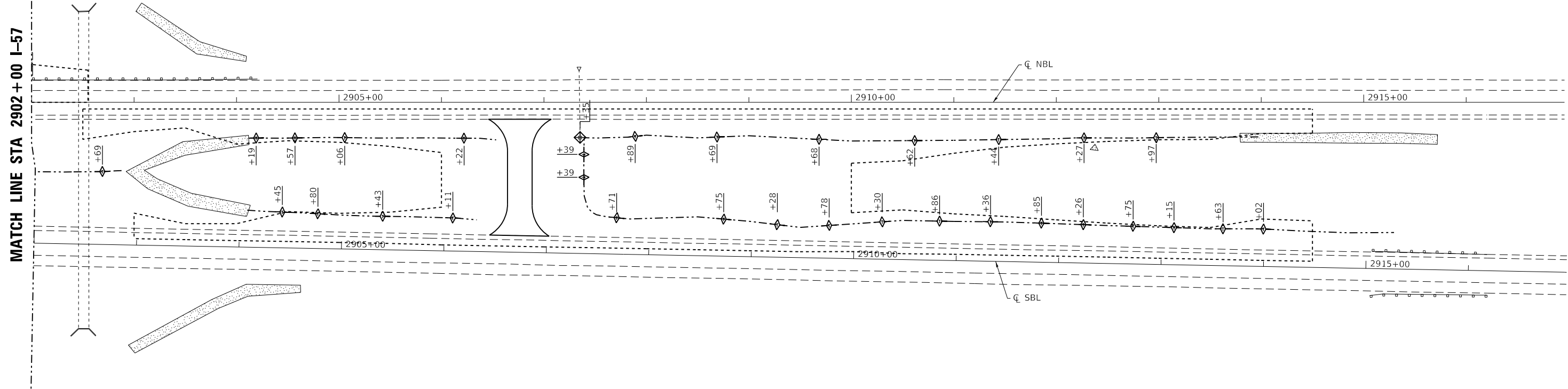
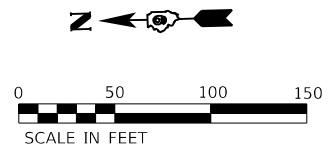
| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 50        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |



**LEGEND**

-  EROSION CONTROL BLANKET
-  PERIMETER EROSION BARRIER
-  INLET AND PIPE PROTECTION
-  TEMPORARY DITCH CHECK
-  CONSTRUCTION LIMITS
-  DITCH FLOWLINE

MATCH LINE STA 2902+00 I-57



MATCH LINE STA 2902+00 I-57

MODEL\_PLOT  
FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\0978504-eroc01.dgn



USER NAME = jmk  
ESCA PROJECT NO. 1259.11  
PLOT SCALE = 0.1667' / in.  
PLOT DATE = 10/7/2019

DESIGNED - JMK  
DRAWN - JMK  
CHECKED - SKM  
DATE - 06/19

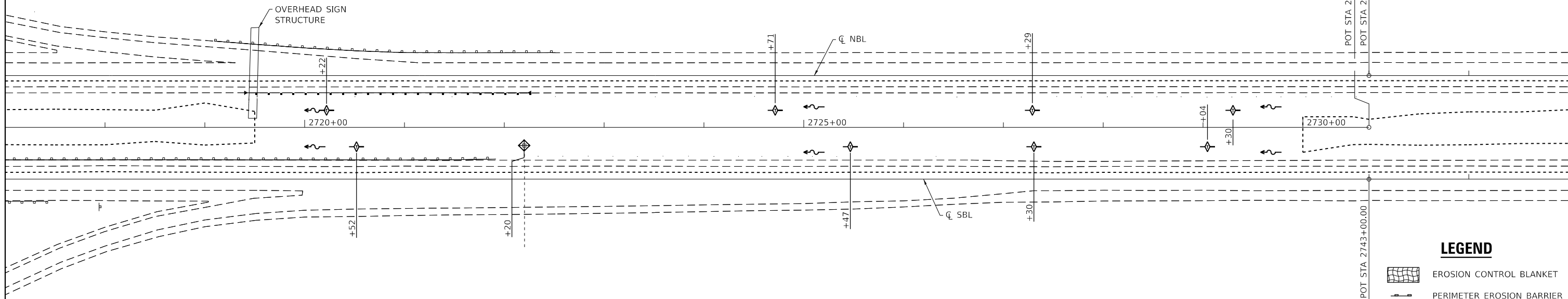
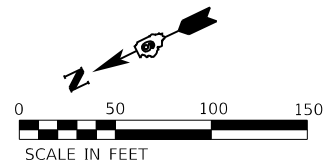
REVISED -  
REVISED -  
REVISED -  
REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**EROSION CONTROL PLANS**

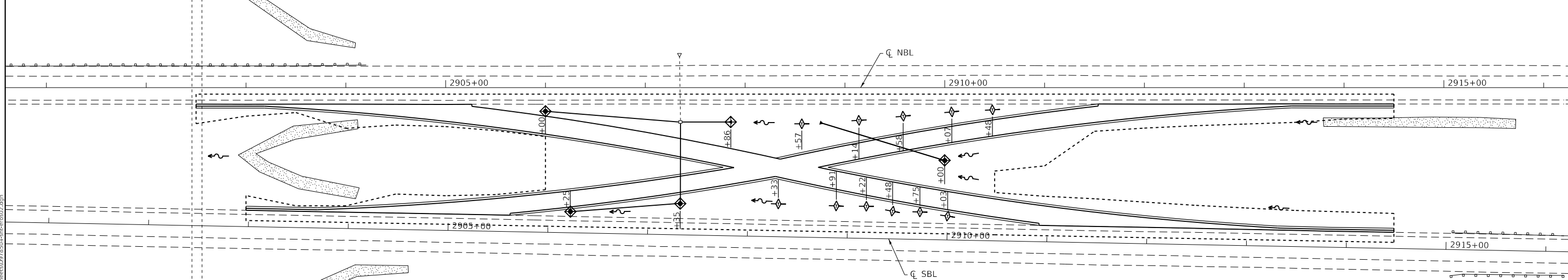
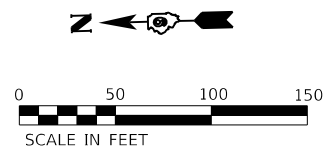
SCALE: 1"=50'    SHEET NO. 1 OF 2 SHEETS    STA. 2887+00 TO STA. 2916+00

|                    |           |        |                           |           |
|--------------------|-----------|--------|---------------------------|-----------|
| F.A.I. RTE.        | SECTION   | COUNTY | TOTAL SHEETS              | SHEET NO. |
| 57                 | (91-3)B-2 | UNION  | 159                       | 51        |
| CONTRACT NO. 78504 |           |        | ILLINOIS FED. AID PROJECT |           |



**NORTH CROSSOVERS REMOVAL  
EROSION CONTROL PLAN**

- LEGEND**
- EROSION CONTROL BLANKET
  - PERIMETER EROSION BARRIER
  - INLET AND PIPE PROTECTION
  - TEMPORARY DITCH CHECK
  - CONSTRUCTION LIMITS



**SOUTH CROSSOVERS CONSTRUCTION  
EROSION CONTROL PLAN**

MODEL\_PLOT  
FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\0978504-ht-eros02.dgn



|                            |                |           |
|----------------------------|----------------|-----------|
| USER NAME = jmk            | DESIGNED - JMK | REVISED - |
| ESCA PROJECT NO. 1259.11   | DRAWN - JMK    | REVISED - |
| PLOT SCALE = 0.1667' / in. | CHECKED - SKM  | REVISED - |
| PLOT DATE = 10/7/2019      | DATE - 06/19   | REVISED - |




**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

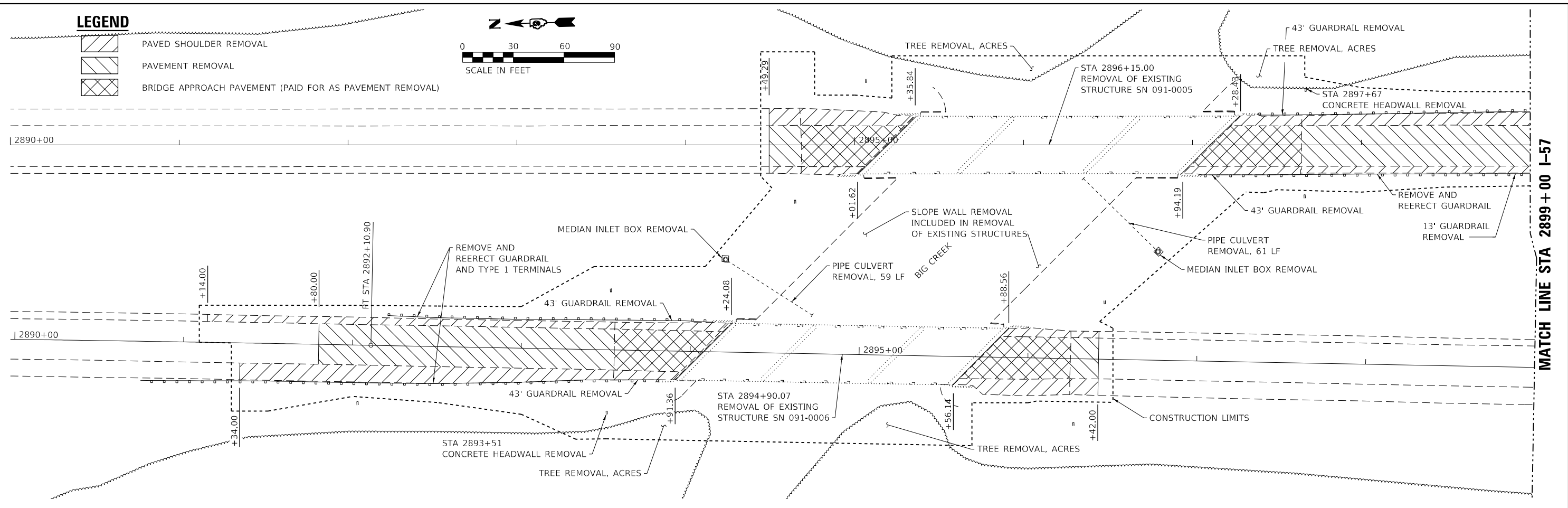
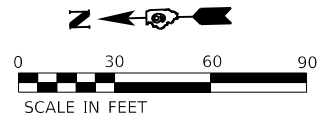
**EROSION CONTROL PLANS**

SCALE: 1"=50'    SHEET NO. 2 OF 2    SHEETS    STA. 2887+00    TO STA. 2916+00

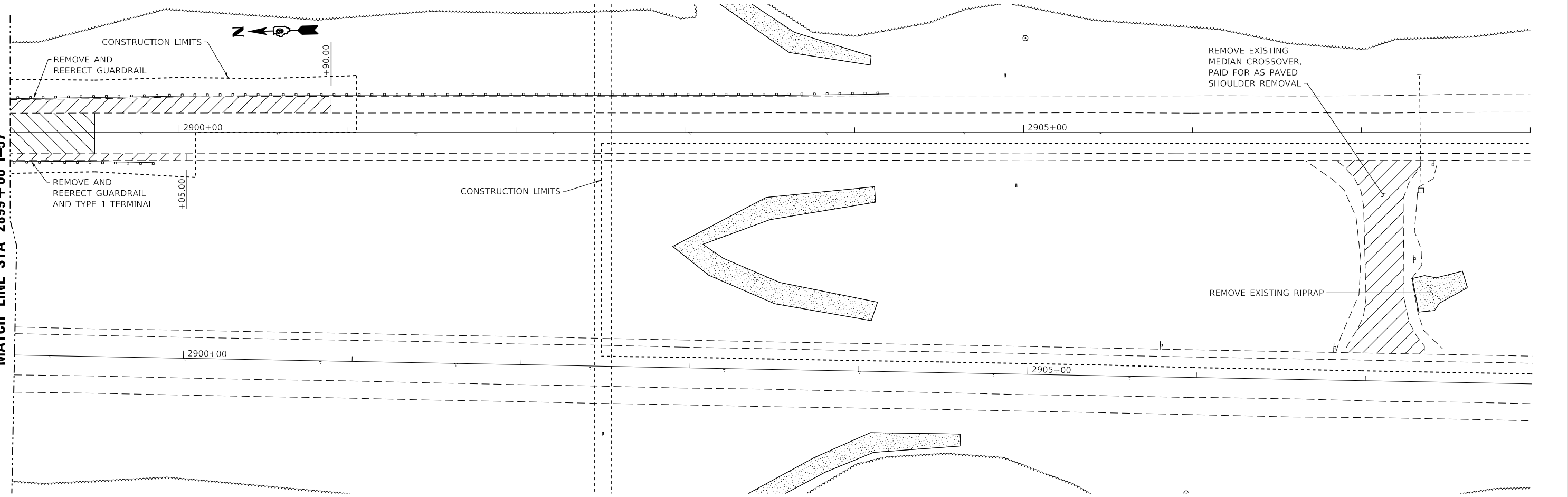
| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 52        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |

**LEGEND**

-  PAVED SHOULDER REMOVAL
-  PAVEMENT REMOVAL
-  BRIDGE APPROACH PAVEMENT (PAID FOR AS PAVEMENT REMOVAL)



MATCH LINE STA 2899 + 00 I-57



MATCH LINE STA 2899 + 00 I-57

MODEL\_PLOT  
FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\0978504-57-rem-01.dgn



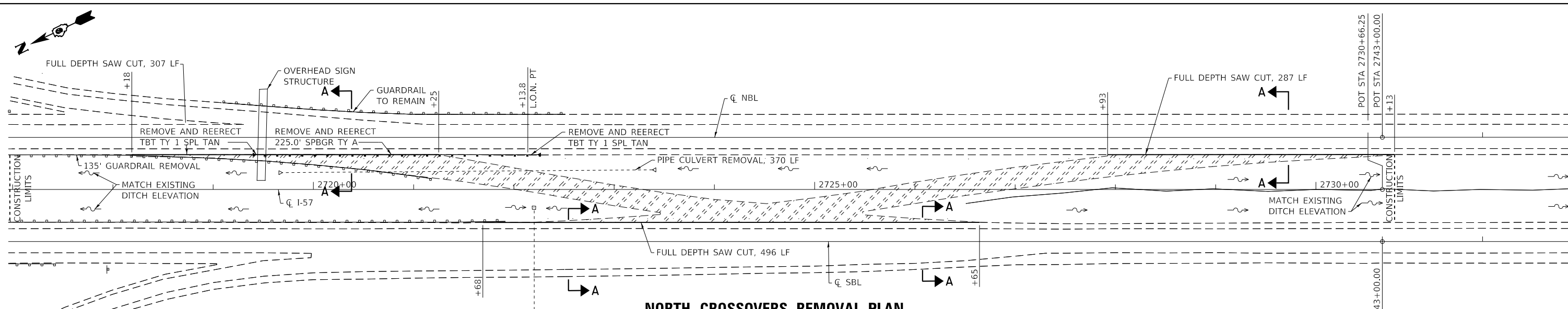
|                            |                |           |
|----------------------------|----------------|-----------|
| USER NAME = jmk            | DESIGNED - SKM | REVISED - |
| ESCA PROJECT NO. 1259.11   | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.1667' / in. | CHECKED - ELH  | REVISED - |
| PLOT DATE = 10/7/2019      | DATE - 10/19   | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

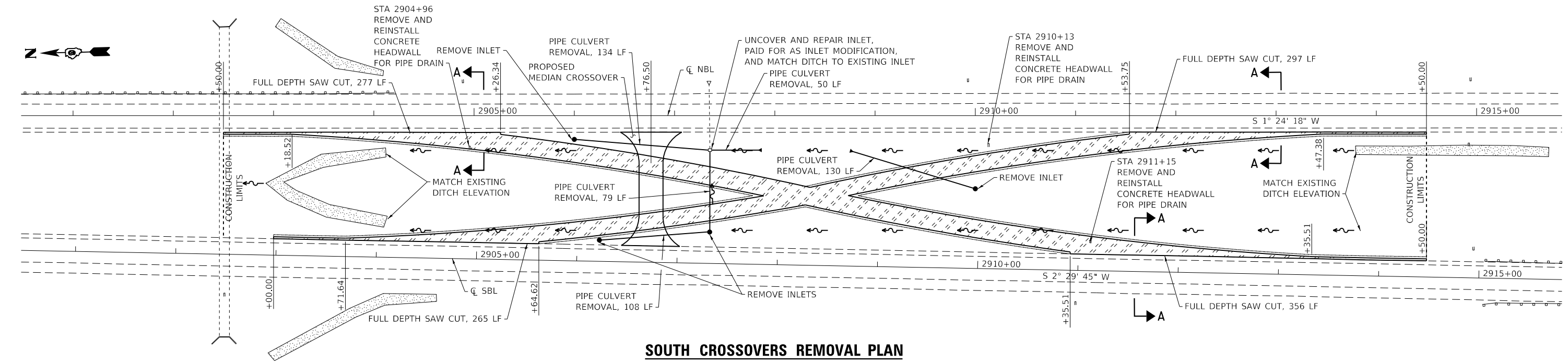
**REMOVAL PLAN**

SCALE: 1"=30' SHEET NO. 1 OF 1 SHEETS STA. 2878+00 TO STA. 2908+00

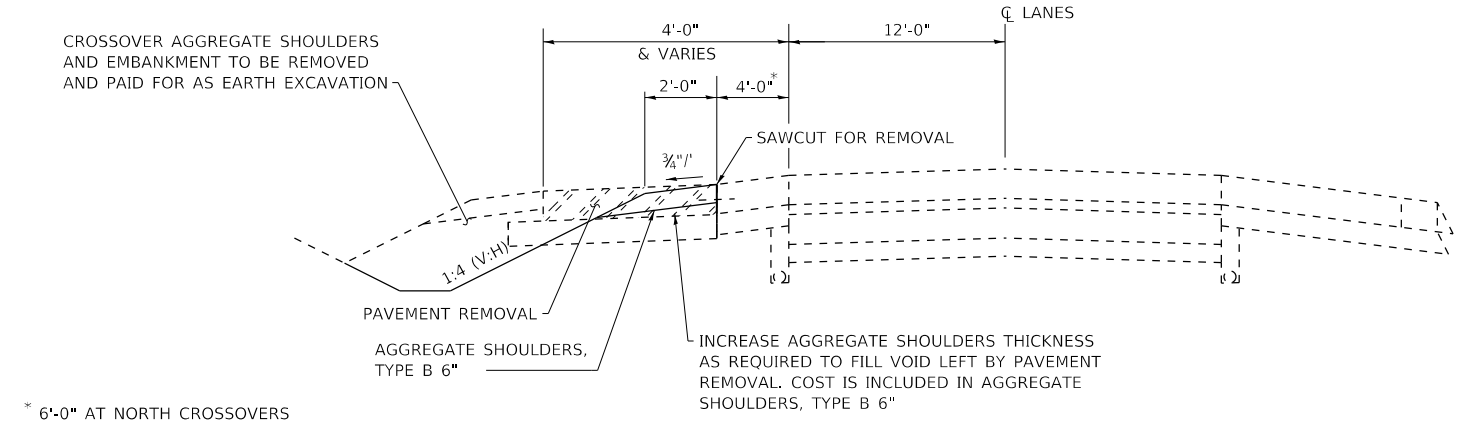
|                    |           |        |                           |           |
|--------------------|-----------|--------|---------------------------|-----------|
| F.A.I. RTE.        | SECTION   | COUNTY | TOTAL SHEETS              | SHEET NO. |
| 57                 | (91-3)B-2 | UNION  | 161                       | 53        |
| CONTRACT NO. 78504 |           |        | ILLINOIS FED. AID PROJECT |           |



**NORTH CROSSOVERS REMOVAL PLAN**



**SOUTH CROSSOVERS REMOVAL PLAN**



**SECTION A-A**

MODEL: PLOT FILE: NAME: Y:\DOT11259-11\_78504\CADD\Highway\CADD\_Sheets\DOT11259-11\_rte-rem-02.dgn



|                            |                |           |
|----------------------------|----------------|-----------|
| USER NAME = jmk            | DESIGNED - SKM | REVISED - |
| ESCA PROJECT NO. 1259.11   | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.1667' / in. | CHECKED - ELH  | REVISED - |
| PLOT DATE = 10/7/2019      | DATE - 06/19   | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CROSSOVERS REMOVAL PLANS**

SCALE: 1"=50' SHEET NO. 1 OF 1 SHEETS STA. 2717+00 TO STA. 2916+00

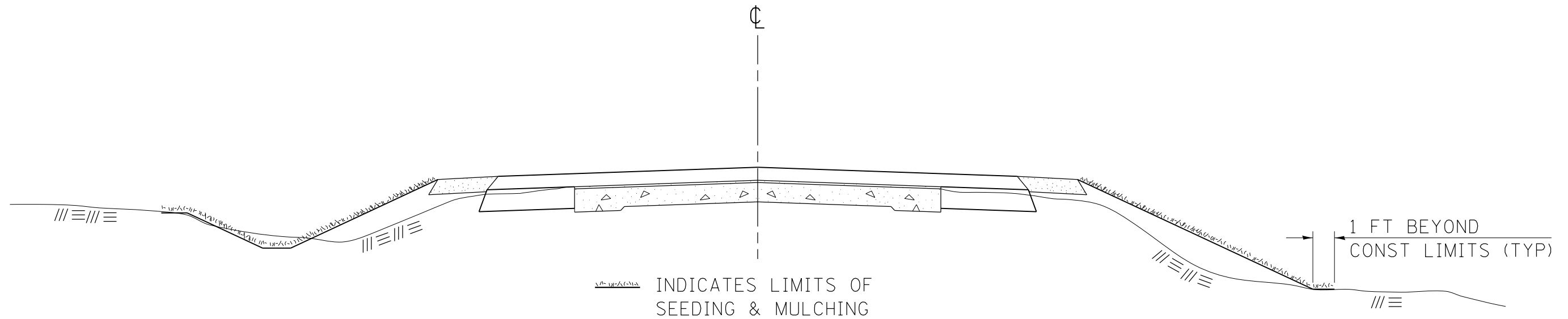
| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 54        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |







# SEEDING & MULCHING



## GENERAL NOTES

IN GENERAL, ALL EARTH SURFACES DISTURBED DURING CONSTRUCTION OPERATIONS SHALL BE SEEDED AND MULCHED UPON COMPLETION OF ALL GRADING OPERATIONS.

FERTILIZER NUTRIENTS AND LIMESTONE SHALL BE APPLIED TO ALL SEEDED AREAS.

SECTIONS 250 AND 251 OF THE STANDARD SPECIFICATIONS SHALL GOVERN THIS WORK EXCEPT AS SPECIFIED HEREIN OR AS NOTED IN THE SPECIAL PROVISIONS.

### REVISIONS

|         |         |
|---------|---------|
| REDRAWN | 2-15-89 |
| REVISED | 8-15-94 |
| REVISED | 6-3-99  |
| REVISED | 3-27-08 |

STD. 9-12

MODEL PLOT  
FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\DOT78504-11-Details\03.dgn



|                            |                |           |
|----------------------------|----------------|-----------|
| USER NAME = jmk            | DESIGNED - JMK | REVISED - |
| ESCA PROJECT NO. 1259.11   | DRAWN - JMK    | REVISED - |
| PLOT SCALE = 0.1667' / in. | CHECKED - SKM  | REVISED - |
| PLOT DATE = 10/7/2019      | DATE - 04/19   | REVISED - |

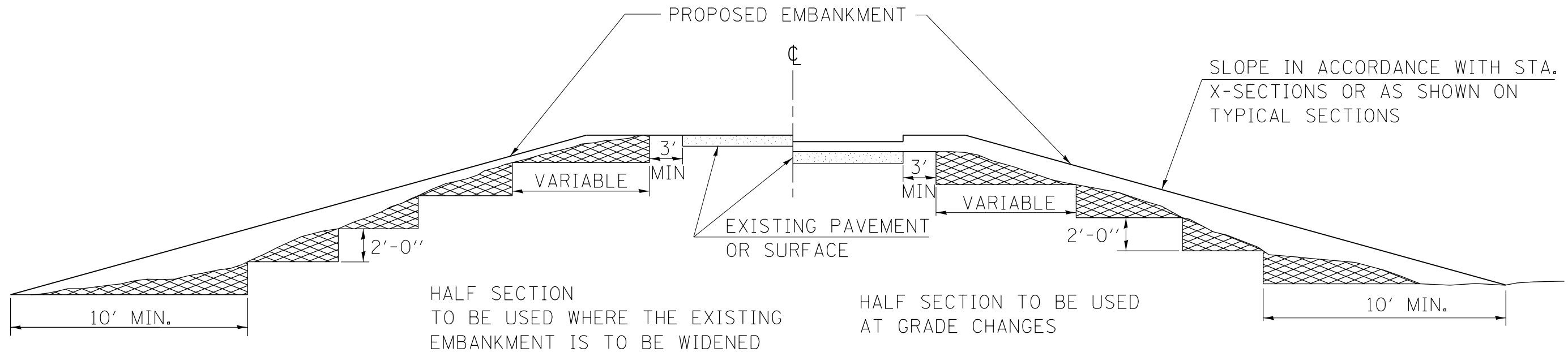
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SEEDING AND MULCHING DETAILS**

SCALE: NONE    SHEET NO. 1 OF 1    SHEETS    STA.    TO STA.

| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 57        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |

# TYPICAL CROSS SECTION SHOWING STEP CONSTRUCTION ON EXISTING FILL



MATERIAL TO BE REMOVED AND REPLACED IN THE EMBANKMENT IN ACCORDANCE WITH ART. 205.04 OF THE STANDARD SPECIFICATION. COST TO BE INCLUDED IN THE VARIOUS ITEMS OF EXCAVATION AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED BECAUSE OF THIS WORK.

| REVISIONS |         |
|-----------|---------|
| REDRAWN   | 2-15-89 |
| REVISED   | 8-15-94 |
| CHECKED   | 6-3-99  |
| RESIZED   | 5-7-08  |

STD. 9-16

MODEL\_PLOT  
FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\0978504-std-detail04.dgn



|                            |                |           |
|----------------------------|----------------|-----------|
| USER NAME = jmk            | DESIGNED - JMK | REVISED - |
| ESCA PROJECT NO. 1259.11   | DRAWN - JMK    | REVISED - |
| PLOT SCALE = 0.1667' / in. | CHECKED - SKM  | REVISED - |
| PLOT DATE = 10/7/2019      | DATE - 04/19   | REVISED - |

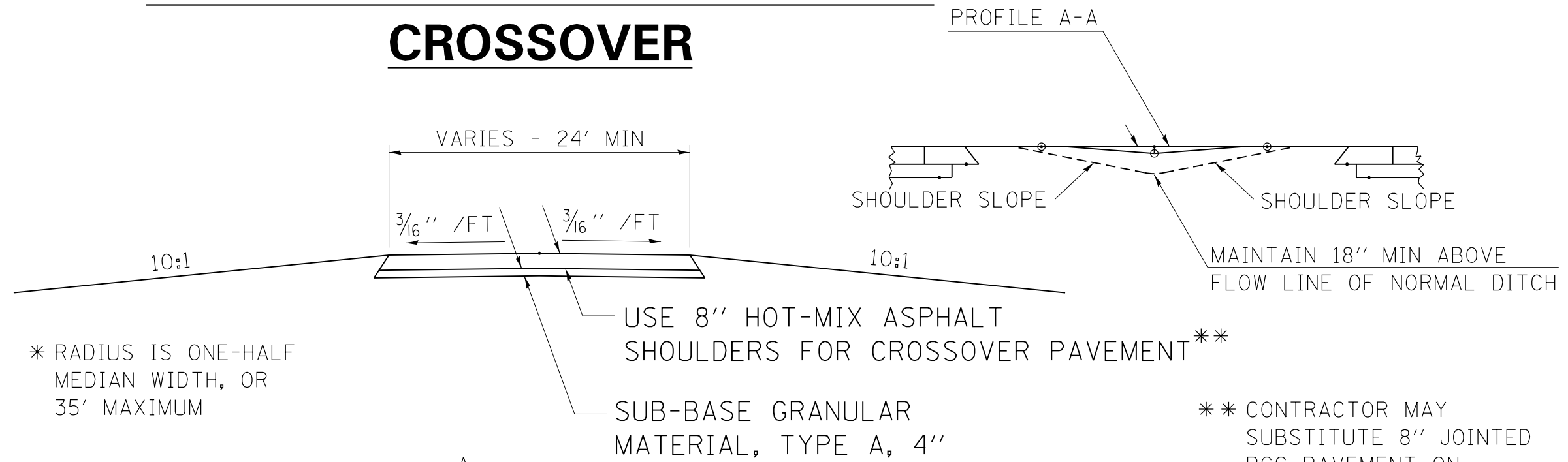
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STEP CONSTRUCTION ON EXISTING FILL DETAILS**

SCALE: NONE    SHEET NO. 1 OF 1 SHEETS    STA.    TO STA.

| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 58        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |

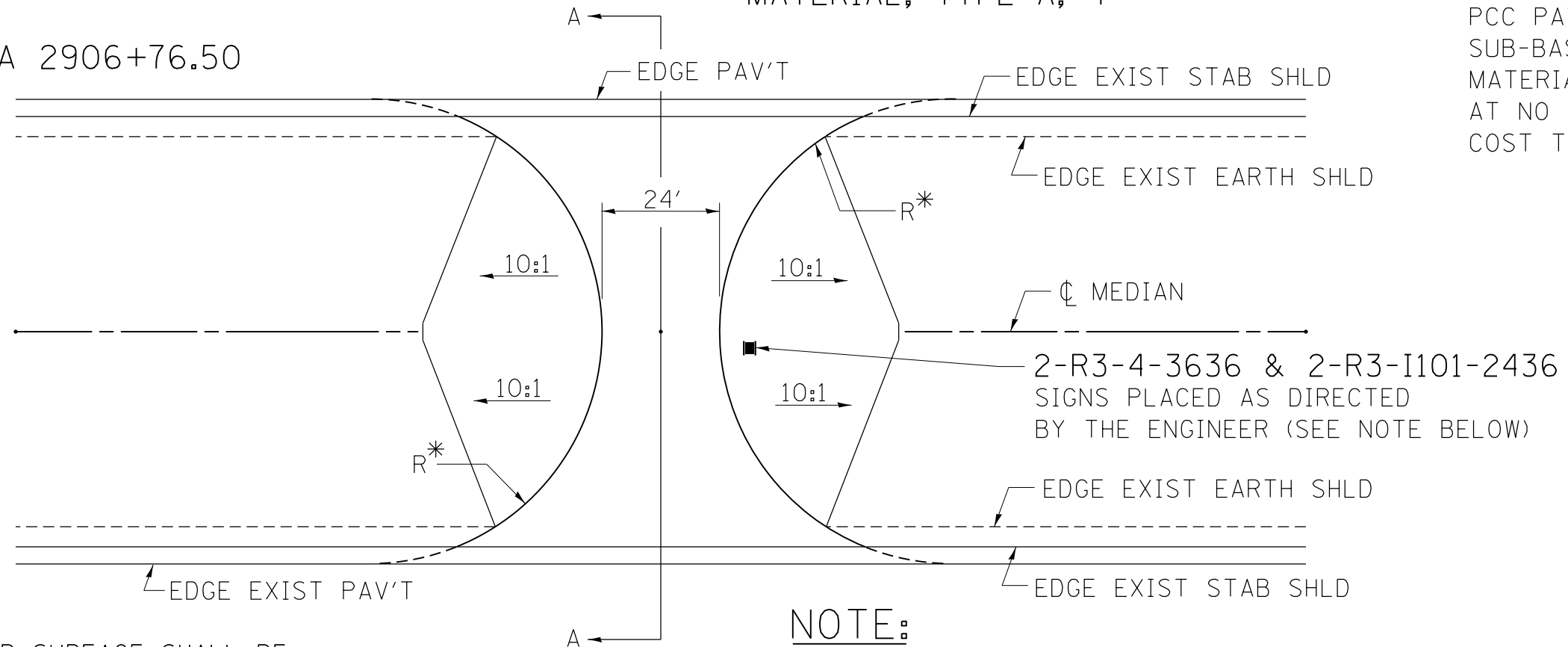
# DETAIL OF PROPOSED MEDIAN CROSSOVER



\* RADIUS IS ONE-HALF MEDIAN WIDTH, OR 35' MAXIMUM

\*\* CONTRACTOR MAY SUBSTITUTE 8" JOINTED PCC PAVEMENT ON SUB-BASE GRANULAR MATERIAL, TYPE A, 4" AT NO ADDITIONAL COST TO THE DEPARTMENT

TO BE USED: NBL STA 2906+76.50



**NOTE:**

THE PROPOSED CROSSOVER SURFACE SHALL BE BUTTED TO THE EDGE OF THE EXISTING STABILIZED SHOULDER. SEE SCHEDULE OF QUANTITIES SHEETS FOR EARTHWORK SCHEDULE. EARTHWORK SHALL BE COMPACTED TO THE SATISFACTION OF THE ENGINEER.

**NOTE:**

THE SIGN ASSEMBLY FOR EACH LOCATION SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "RELOCATE SIGN PANEL ASSEMBLY-TYPE B" EXISTING SIGN SUPPORTS SHALL BE USED FOR NEW INSTALLATION.

| REVISIONS |          |
|-----------|----------|
| DRAWN     | 8-30-89  |
| REVISED   | 01-10-07 |
| REVISED   | 3-26-08  |
| REVISED   |          |

STD. 9-55

MODEL\_PLOT FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\0978504-1-1-Details\05.dgn



|                            |                |           |
|----------------------------|----------------|-----------|
| USER NAME = jmk            | DESIGNED - JMK | REVISED - |
| ESCA PROJECT NO. 1259.11   | DRAWN - JMK    | REVISED - |
| PLOT SCALE = 0.1667' / in. | CHECKED - SKM  | REVISED - |
| PLOT DATE = 10/7/2019      | DATE - 04/19   | REVISED - |

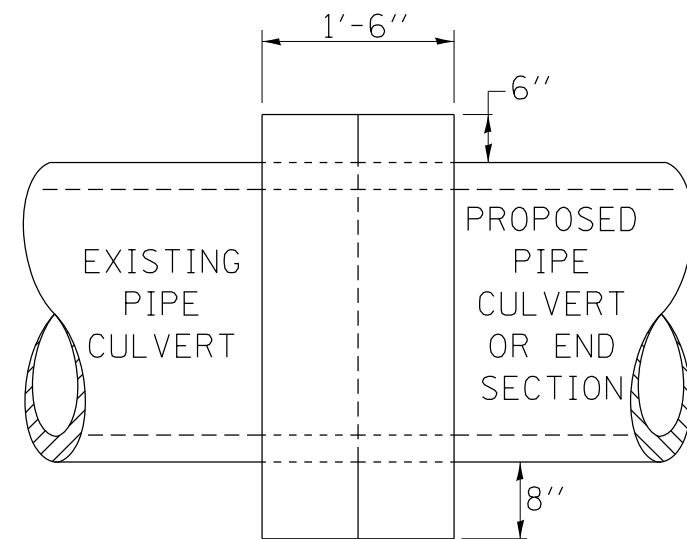
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|  |                                      |
|--|--------------------------------------|
| <b>PROPOSED MEDIAN CROSSOVER DETAILS</b> |                                      |
| SCALE: NONE                              | SHEET NO. 1 OF 1 SHEETS STA. TO STA. |

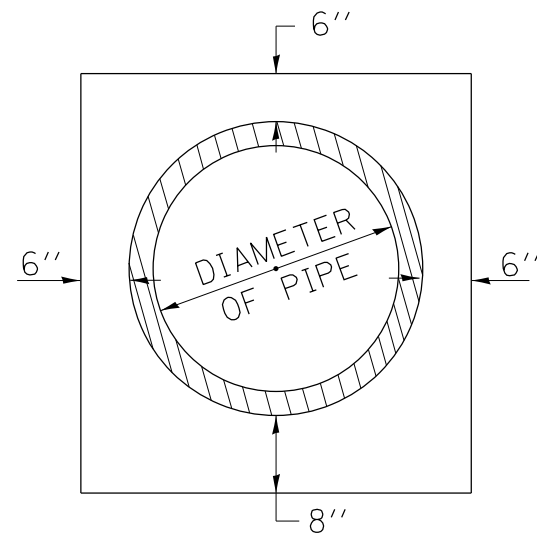
| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 59        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |

# CONCRETE COLLAR

PIPE TO PIPE



SIDE VIEW



END VIEW

## TABULATION

| DIAMETER OF PIPE | CL SI CONC CU YDS EST |
|------------------|-----------------------|
| 12"              | 0.24                  |
| 15"              | 0.29                  |
| 18"              | 0.32                  |
| 24"              | 0.44                  |
| 30"              | 0.56                  |
| 36"              | 0.66                  |
| 42"              | 0.80                  |
| 48"              | 0.93                  |
| 54"              | 1.07                  |
| 60"              | 1.22                  |
| 72"              | 1.55                  |

THE CONCRETE COLLAR SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC YARD FOR CONCRETE COLLAR, AS SHOWN ON THE PLANS, WHICH PRICE SHALL INCLUDE THE REMOVAL OF SUCH PORTIONS THE EXISTING HEADWALLS AS MAY BE REQUIRED.

CLASS SI CONCRETE SHALL BE USED THROUGHOUT.

### REVISIONS

|         |         |
|---------|---------|
| DRAWN   | 7-13-90 |
| REVISED | 8-22-94 |
| REVISED | 3-26-08 |
| REVISED | 5-17-13 |

STD. 9-79

MODEL\_PLOT  
FILE NAME: Y:\PROJECTS\1259-11\_78504\CADD\Highway\CADD\_Sheets\0978504-std-detail02.dgn



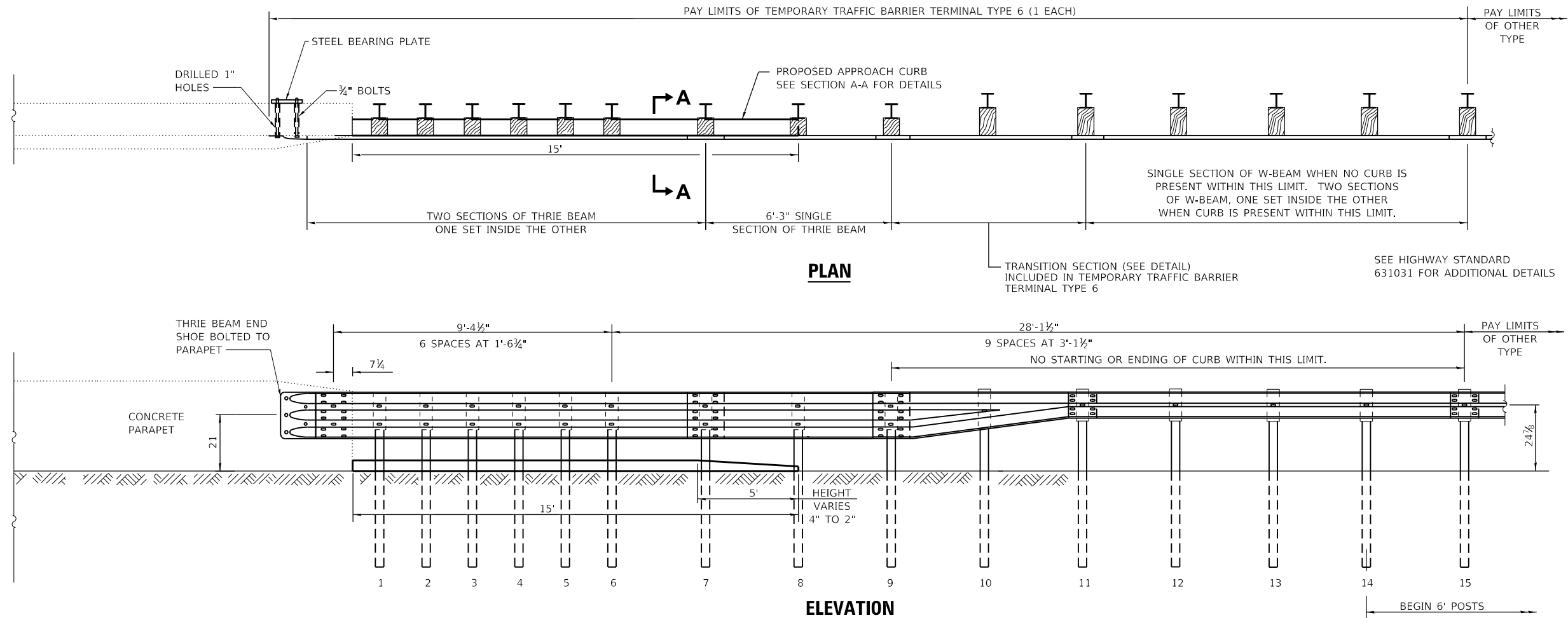
|                            |                |           |
|----------------------------|----------------|-----------|
| USER NAME = jmk            | DESIGNED - JMK | REVISED - |
| ESCA PROJECT NO. 1259.11   | DRAWN - JMK    | REVISED - |
| PLOT SCALE = 0.1667' / in. | CHECKED - SKM  | REVISED - |
| PLOT DATE = 10/7/2019      | DATE - 04/19   | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

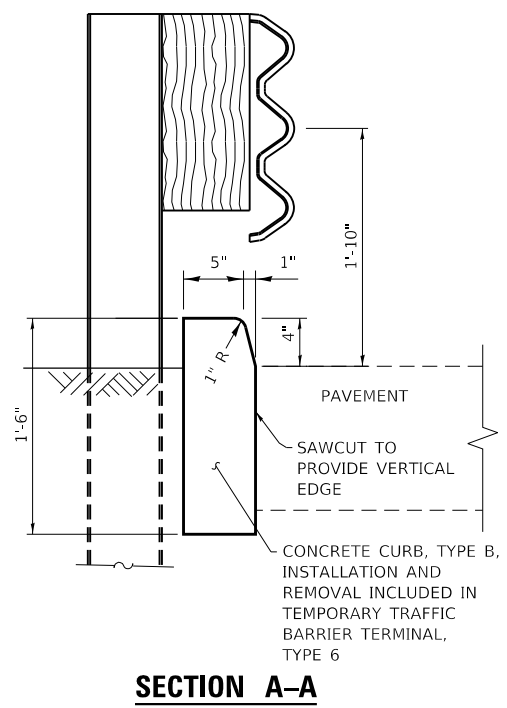
CONCRETE COLLAR DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. \_\_\_\_ TO STA. \_\_\_\_

| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS       | SHEET NO. |
|---------------------------|-----------|--------|--------------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161                | 60        |
|                           |           |        | CONTRACT NO. 78504 |           |
| ILLINOIS FED. AID PROJECT |           |        |                    |           |



**TEMPORARY TRAFFIC BARRIER TERMINAL, TYPE 6**



**SECTION A-A**

MODEL PLOT  
 FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\DOT1259-11\_78504-11-1.dgn

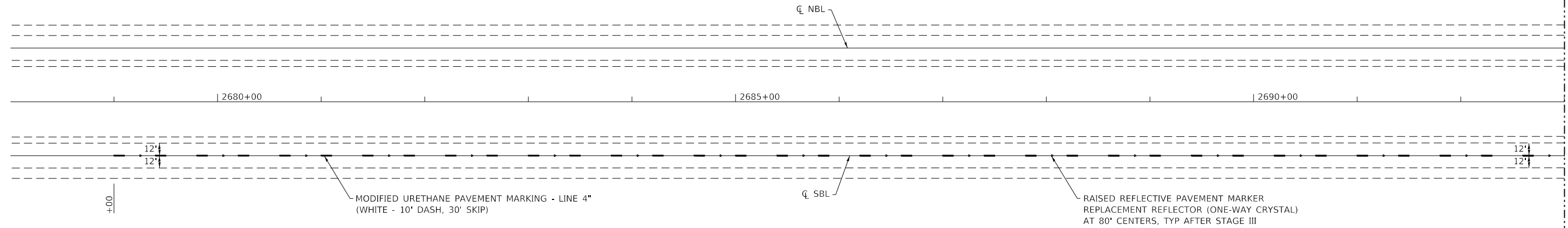
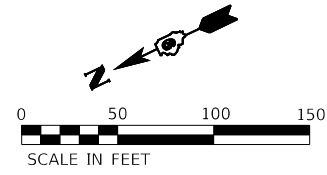


|                            |                |           |
|----------------------------|----------------|-----------|
| USER NAME = jmk            | DESIGNED - SKM | REVISED - |
| ESCA PROJECT NO. 1259.11   | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.1667' / in. | CHECKED - ELH  | REVISED - |
| PLOT DATE = 10/7/2019      | DATE - 08/19   | REVISED - |

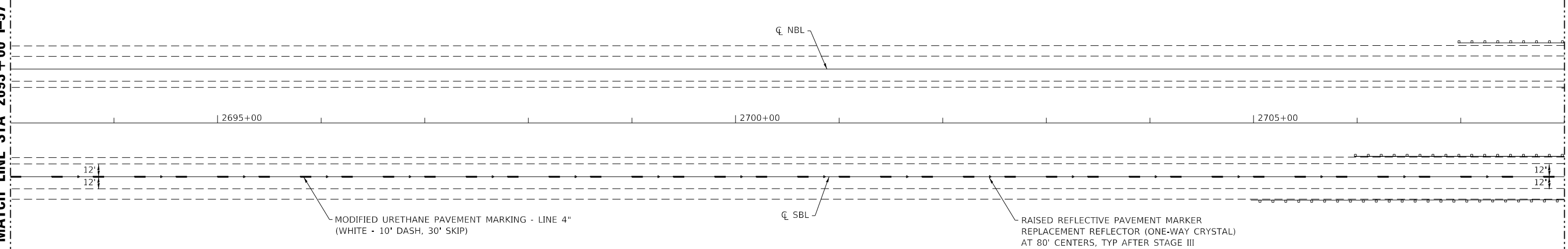
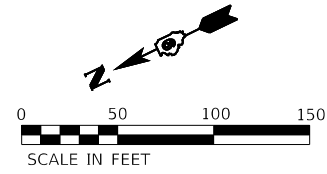
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                        |                         |
|------------------------|-------------------------|
| <b>ROADWAY DETAILS</b> |                         |
| SCALE: AS SHOWN        | SHEET NO. 1 OF 1 SHEETS |
| STA. _____             | TO STA. _____           |

|                           |           |        |              |           |
|---------------------------|-----------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
| 57                        | (91-3)B-2 | UNION  | 161          | 61        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |



MATCH LINE STA 2693 + 00 I-57



MATCH LINE STA 2693 + 00 I-57

MATCH LINE STA 2708 + 00 I-57  
SEE SHEET 63 FOR CONT.

MODEL\_PLOT  
FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\978504-shp.mxd



|                            |                |           |
|----------------------------|----------------|-----------|
| USER NAME = jmk            | DESIGNED - SKM | REVISED - |
| ESCA PROJECT NO. 1259.11   | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.1667' / in. | CHECKED - ELH  | REVISED - |
| PLOT DATE = 10/7/2019      | DATE - 10/19   | REVISED - |

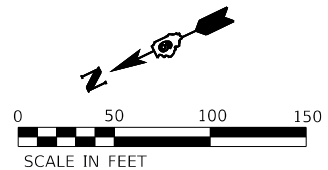
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING PLANS**

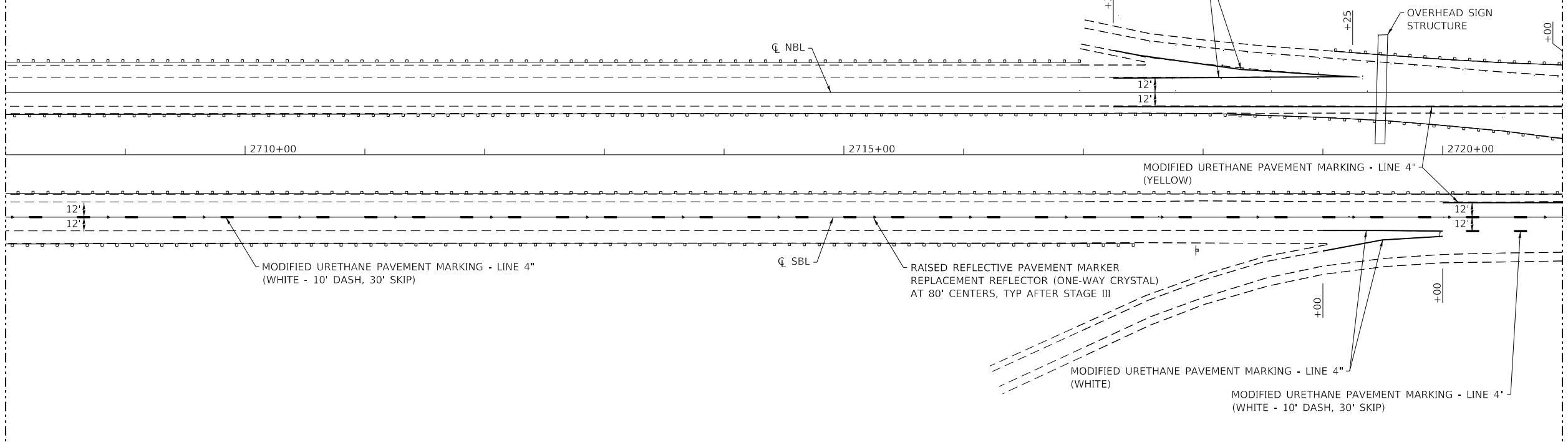
SCALE: 1"=50'    SHEET NO. 1 OF 5 SHEETS    STA. 2678+00 TO STA. 2708+00

| F.A.I. RTE.        | SECTION   | COUNTY | TOTAL SHEETS              | SHEET NO. |
|--------------------|-----------|--------|---------------------------|-----------|
| 57                 | (91-3)B-2 | UNION  | 161                       | 62        |
| CONTRACT NO. 78504 |           |        | ILLINOIS FED. AID PROJECT |           |

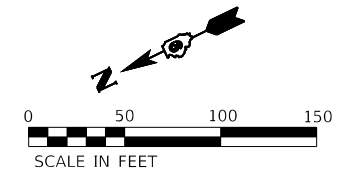




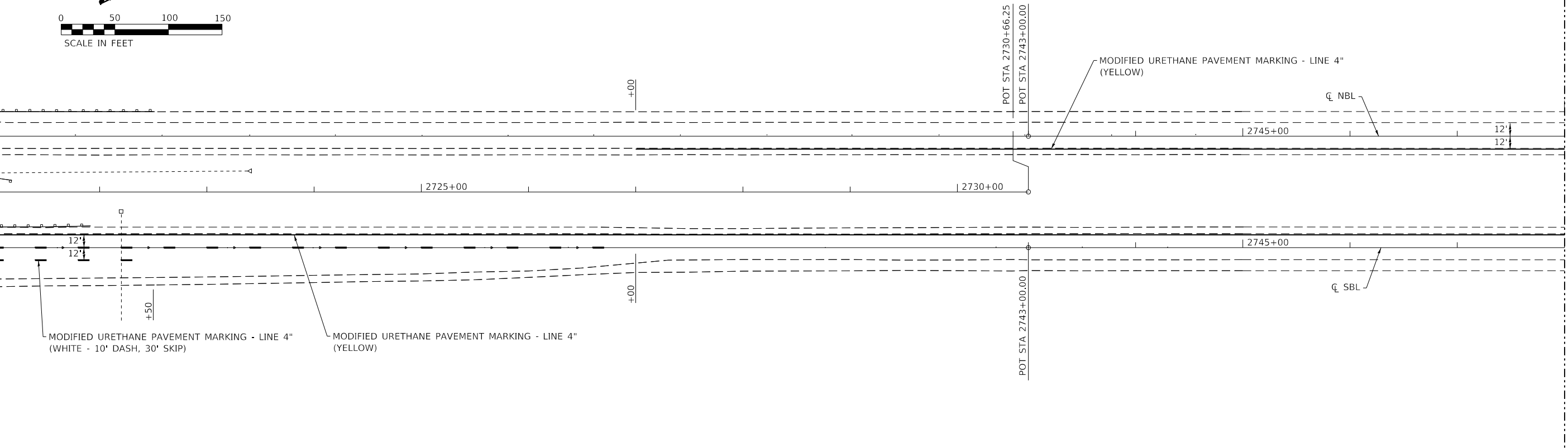
MATCH LINE STA 2708+00 I-57  
SEE SHEET 62 FOR CONT.



MATCH LINE STA 2721+00 I-57



MATCH LINE STA 2721+00 I-57



MATCH LINE STA 2748+00 I-57  
SEE SHEET 64 FOR CONT.

MODEL\_PLOT  
FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\0978504-eth-pmk02.dgn



USER NAME = jmk  
ESCA PROJECT NO. 1259.11  
PLOT SCALE = 0.1667' / in.  
PLOT DATE = 10/7/2019

DESIGNED - SKM  
DRAWN - SKM  
CHECKED - ELH  
DATE - 04/19

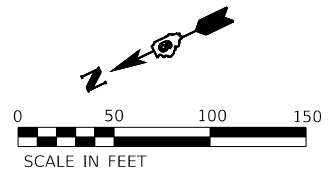
REVISED -  
REVISED -  
REVISED -  
REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

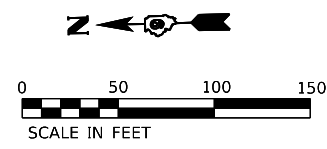
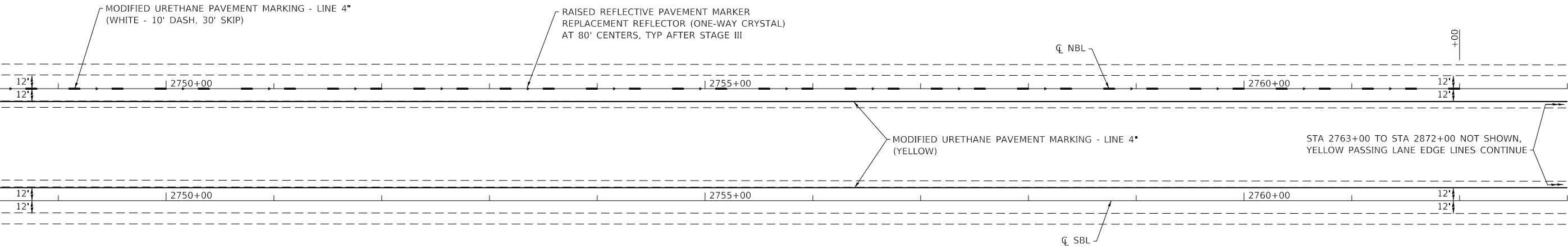
PAVEMENT MARKING PLANS

SCALE: 1"=50' SHEET NO. 2 OF 5 SHEETS STA. 2708+00 TO STA. 2748+00

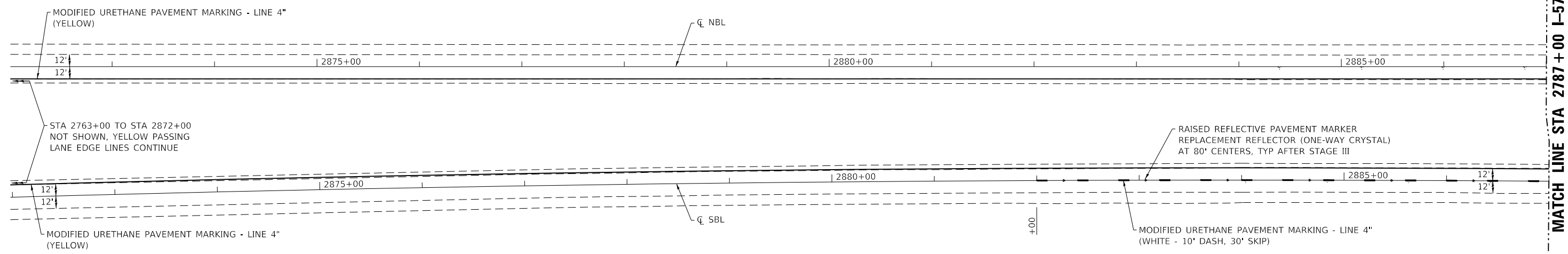
| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 63        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |



MATCH LINE STA 2748+00 I-57  
SEE SHEET 63 FOR CONT.



MATCH LINE STA 2787+00 I-57  
SEE SHEET 65 FOR CONT.



MODEL: PLOT  
FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\978504-tpm\03.dgn



USER NAME = jmk  
ESCA PROJECT NO. 1259.11  
PLOT SCALE = 0.1667' / in.  
PLOT DATE = 10/7/2019

DESIGNED - SKM  
DRAWN - SKM  
CHECKED - ELH  
DATE - 04/19

REVISED -  
REVISED -  
REVISED -  
REVISED -

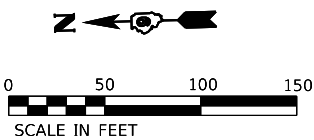
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING PLANS**

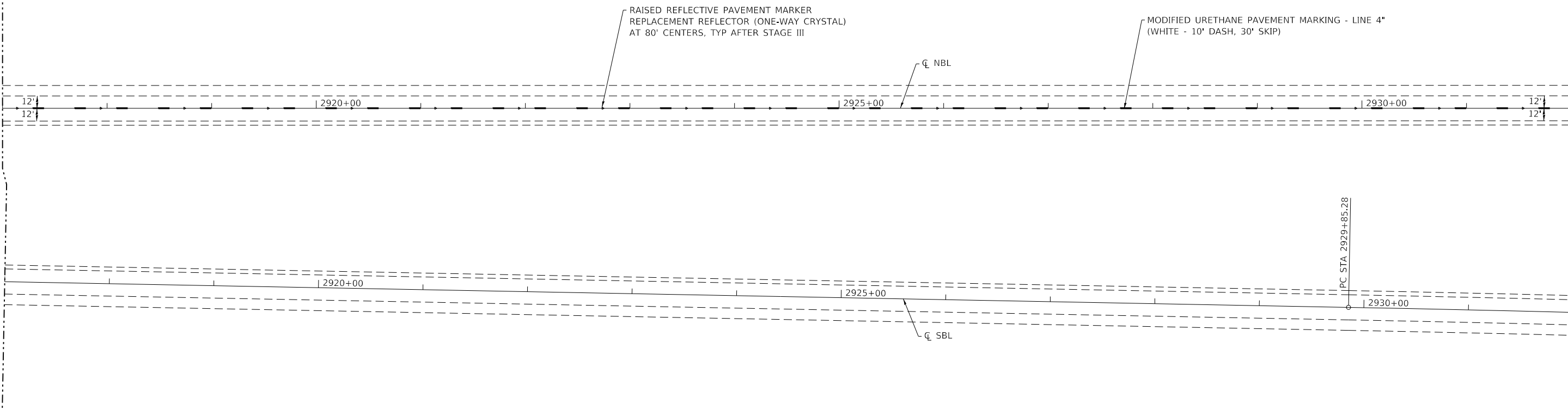
SCALE: 1"=50'    SHEET NO. 3 OF 5 SHEETS    STA. 2748+00 TO STA. 2787+00

| F.A.I. RTE.        | SECTION   | COUNTY           | TOTAL SHEETS | SHEET NO. |
|--------------------|-----------|------------------|--------------|-----------|
| 57                 | (91-3)B-2 | UNION            | 161          | 64        |
| CONTRACT NO. 78504 |           |                  |              |           |
| ILLINOIS           |           | FED. AID PROJECT |              |           |

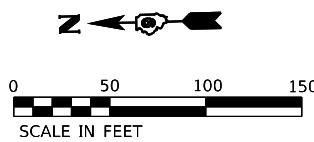




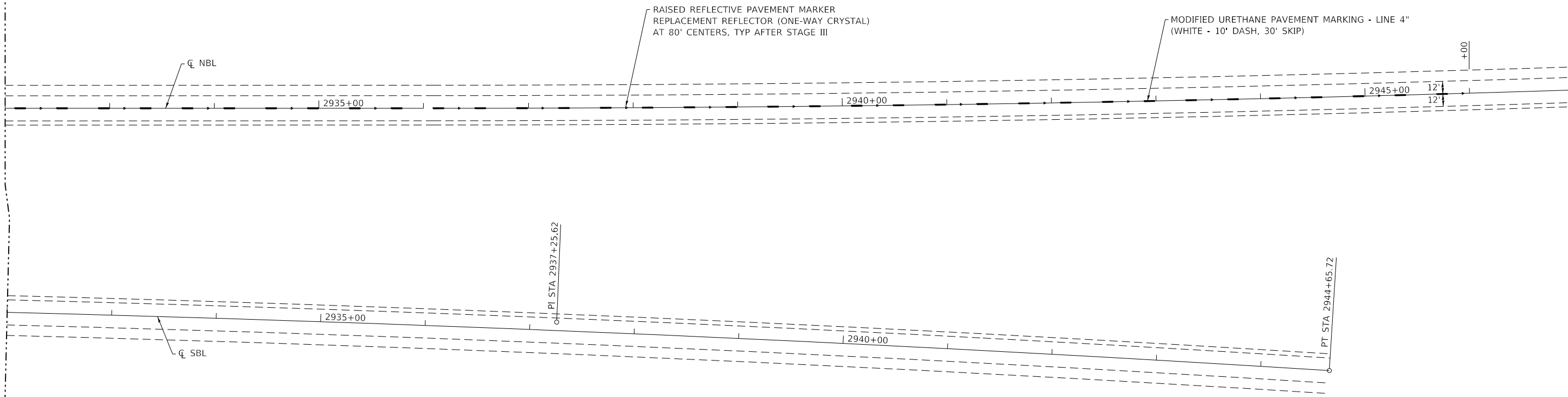
MATCH LINE STA 2917+00 I-57  
SEE SHEET 65 FOR CONT.



MATCH LINE STA 2932+00 I-57



MATCH LINE STA 2932+00 I-57



MODEL: PLOT  
FILE NAME: Y:\DOT\1259-11\_78504\CADD\Highway\CADD\_Sheets\0978504-5th-rpm\05.dgn



|                            |
|----------------------------|
| USER NAME = jmk            |
| ESCA PROJECT NO. 1259.11   |
| PLOT SCALE = 0.1667' / in. |
| PLOT DATE = 10/7/2019      |

|                |            |
|----------------|------------|
| DESIGNED - SKM | REVISIED - |
| DRAWN - SKM    | REVISIED - |
| CHECKED - ELH  | REVISIED - |
| DATE - 04/19   | REVISIED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING PLANS**

SCALE: 1"=50'      SHEET NO. 5 OF 5 SHEETS      STA. 2917+00 TO STA. 2947+00

| F.A.I. RTE.        | SECTION   | COUNTY | TOTAL SHEETS              | SHEET NO. |
|--------------------|-----------|--------|---------------------------|-----------|
| 57                 | (91-3)B-2 | UNION  | 161                       | 66        |
| CONTRACT NO. 78504 |           |        | ILLINOIS FED. AID PROJECT |           |

BENCHMARK: 0910005 - Aluminum disk, IDOT BM, set in top of NW Parapet wall of SN 091-0005, Sta. 2894+90, 18' RT. of  $\bar{C}$  NB Lanes of I-57. Elev. 412.49

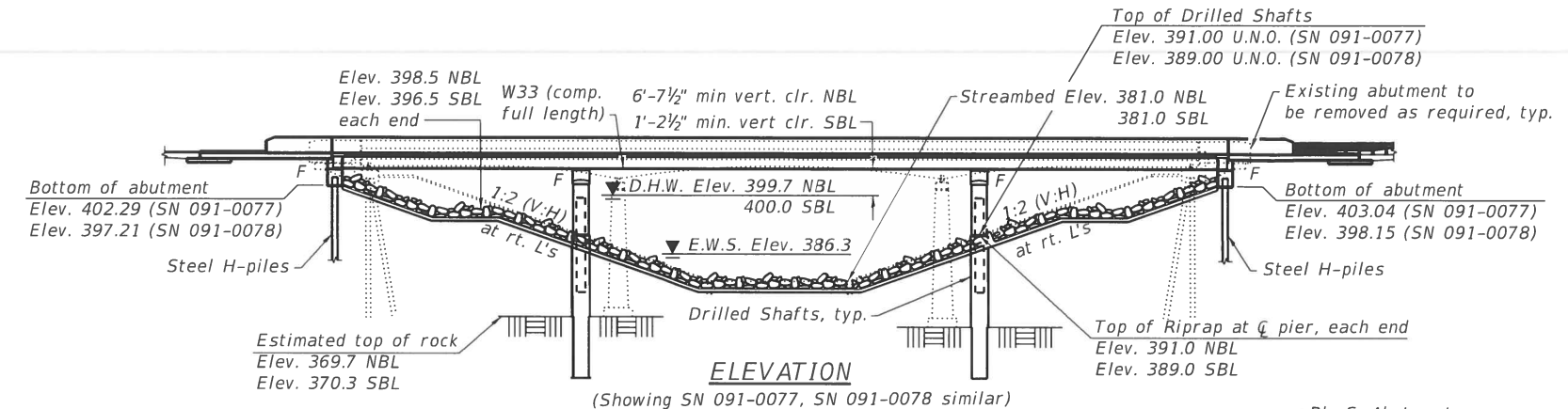
**EXISTING STRUCTURES:**

SN 091-0005 and SN 091-0006 were originally constructed in 1958 as F.A.I. 57, Section 91-3B-1. The three-span structures consist of concrete decks supported by five haunched concrete T-beams. The concrete spans are supported by concrete wall piers and open abutments. The abutments are supported by steel piles and the piers are supported on spread footings. Back to back abutments is 195'-0" NB Lanes and 167'-3" SB Lanes. The superstructure width is 34'-0" between parapets. The skew is 45° NB Lanes and 43°-53'-00" SB Lanes. Interstate traffic will be maintained utilizing crossovers.

No salvage.

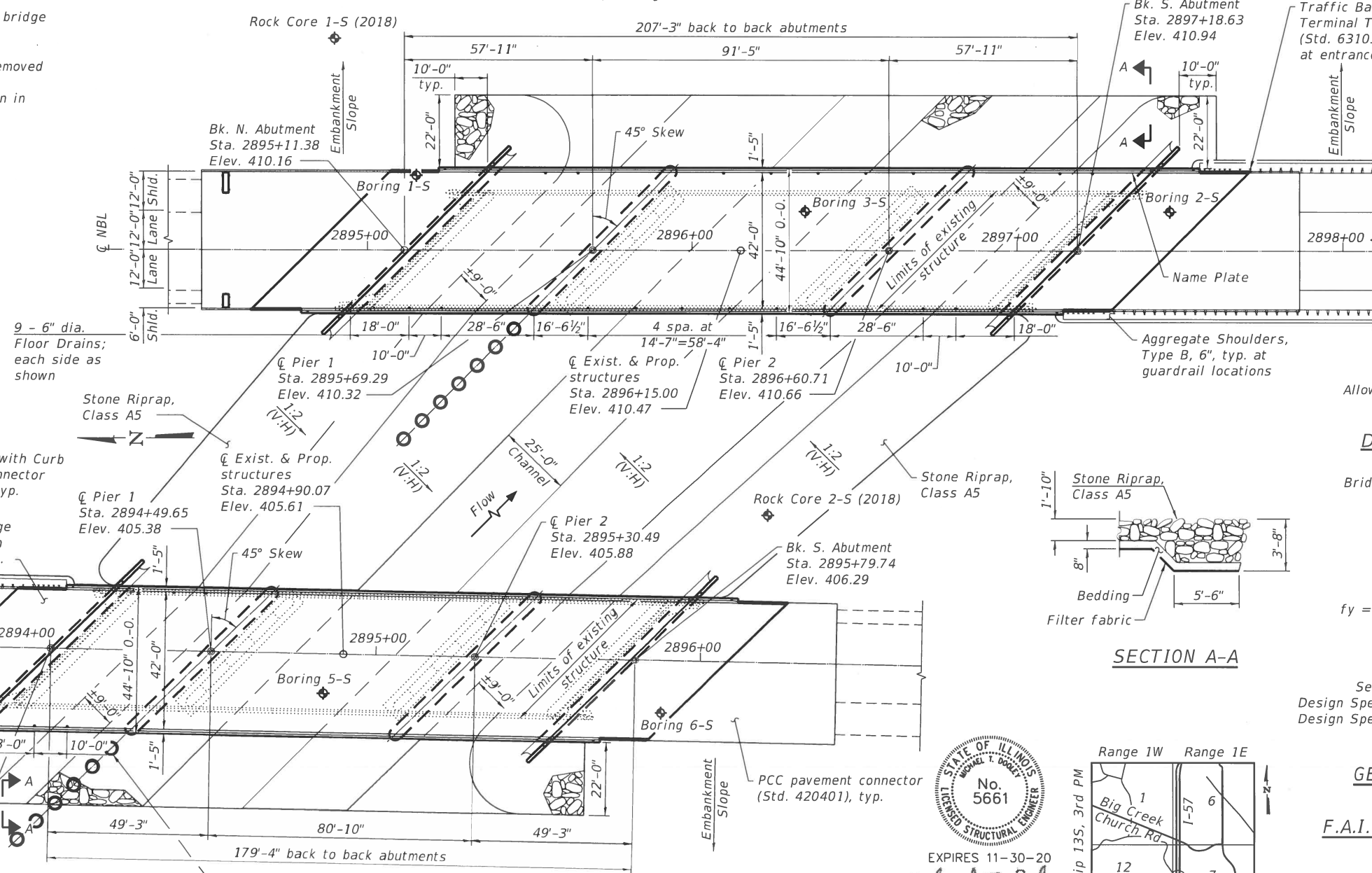
Up to 1/4" may be ground off the bridge deck, the bridge approach slabs, and the pavement connectors.

The existing piers and abutment piles shall be removed to one foot below the proposed bottom of riprap bedding. Below this level these items shall remain in place to improve slope stability.

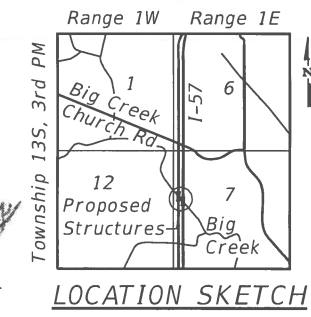


**ELEVATION**  
(Showing SN 091-0077, SN 091-0078 similar)

**APPROVED**  
For Structural Adequacy Only  
*Dr. Carl Ruyter*  
Engineer of Bridges & Structures



**PLAN**



**LOCATION SKETCH**

**LOADING HL-93**  
Allow 50 psf for future wearing surface

**DESIGN SPECIFICATIONS**  
2017 AASHTO LRFD  
Bridge Design Specifications 8th Edition

**DESIGN STRESSES FIELD UNITS**  
f'c = 3,500 psi (substructure)  
f'c = 4,000 psi (superstructure)  
fy = 50,000 psi (AASHTO M270 Grade 50)  
fy = 60,000 psi (reinforcement)

**SEISMIC DATA**  
Seismic Performance Zone (SPZ) = 3  
Design Spectral Acceleration at 1.0 sec. (S<sub>D1</sub>) = 0.460  
Design Spectral Acceleration at 0.2 sec. (S<sub>D5</sub>) = 1.213  
Site Soil Class = C

**GENERAL PLAN & ELEVATION**  
**I-57 OVER BIG CREEK**  
F.A.I. RTE. 57 - SECTION (91-3)B-2  
UNION COUNTY  
STATION 2896+15.00 (NB)  
STATION 2894+90.07 (SB)  
STRUCTURE NO. 091-0077 (NB)  
STRUCTURE NO. 091-0078 (SB)

STATE OF ILLINOIS  
MICHAEL T. DALRY  
LICENSED STRUCTURAL ENGINEER  
No. 5661  
EXPIRES 11-30-20  
*Michael T. Dalry*  
SIGNATURE  
10/15/2019  
DATE

MODEL: PLOT  
FILE NAME: Y:\IDOT\1259-11\_78504\CADD\SP SN 091-0077-0078\0910077-78504-01-GenPlan.dgn  
10/15/2019 1:47:58 PM



|                              |                         |           |
|------------------------------|-------------------------|-----------|
| USER NAME = kah              | DESIGNED - KJA 07/19    | REVISED - |
| ESCA PROJECT NO. 1259.11     | CHECKED - RTM/SHL 07/19 | REVISED - |
| PLOT SCALE = 0.2" = 1' / in. | DRAWN - KAH 07/19       | REVISED - |
| PLOT DATE = 10/15/2019       | CHECKED - MTD/SHL 08/19 | REVISED - |

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

|                           |                   |              |                  |              |
|---------------------------|-------------------|--------------|------------------|--------------|
| F.A.I. RTE. 57            | SECTION (91-3)B-2 | COUNTY UNION | TOTAL SHEETS 161 | SHEET NO. 67 |
| CONTRACT NO. 78504        |                   |              |                  |              |
| ILLINOIS FED. AID PROJECT |                   |              |                  |              |

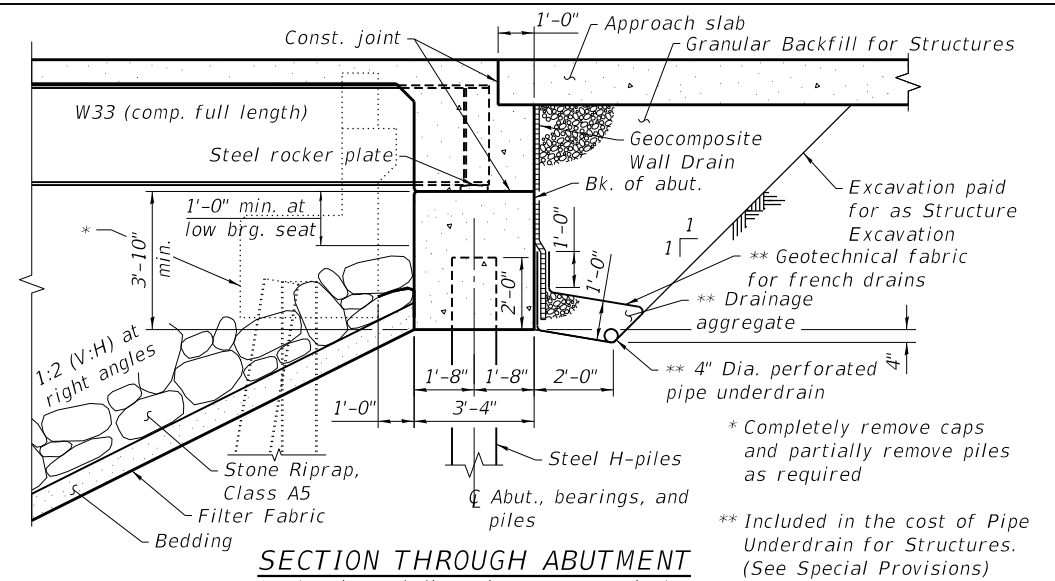
SHEET 1 OF 38 SHEETS

**GENERAL NOTES**

- Fasteners shall be ASTM F3125 Grade 325 Type 1, mechanically galvanized bolts in painted areas. Bolts 7/8" Ø, holes 1 1/16" Ø, unless otherwise noted.
- Calculated weight of Structural Steel = 450,330 lbs. (Grade 50)  
58,500 lbs. (Grade 36)
- No field welding is permitted except as specified in the contract documents.
- Reinforcement bars designated (E) shall be epoxy coated.
- Bearing seat surfaces shall be constructed or adjusted to the designated elevations within a tolerance of 1/8 in. (0.01 ft.). Adjustment shall be made either by grinding the surface or by shimming the bearings.
- The Inorganic Zinc Rich Primer / Acrylic / Acrylic Paint System shall be used for shop and field painting of new structural steel except where otherwise noted. The color of the final finish coat for all interior steel surfaces shall be gray, Munsell No. 5B 7/1. The color of the final finish coat for the exterior and bottom flange of the fascia beams shall be Interstate Green, Munsell No. 7.5G 4/8.
- Slipforming of the parapets is not allowed.
- Removal of SN 091-0005 (NB) will be paid for as Removal of Existing Structures No. 1 and removal of SN 091-0006 (SB) will be paid for as Removal of Existing Structures No. 2
- The concrete for bridge decks finished according to Article 503.16(a) of the Standard Specifications shall be placed and compacted parallel to the skew in uniform increments along centerline of bridge. The machine used for finishing shall be set parallel to the skew for striking off and screeding the concrete.
- Quantities of Deck Slab Repair are included to repair SN 091-0005 prior to allowing two-way traffic over the structure. Quantities of Deck Slab Repair are included to repair SN 091-0006 as necessary. The quantities used shall be determined by the Engineer.
- Layout of the slope protection system may be varied to suit ground conditions in the field as directed by the Engineer.

**STRUCTURE INDEX OF SHEETS**

|   |                       |
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| General Plan & Elevation                            | Sheet No. 1 of 38     |
| General Data  | Sheet No. 2 of 38     |
| Substructure Layout                                 | Sheet No. 3 of 38     |
| Top of Slab Elevations                              | Sheet No. 4 of 38     |
| Top of Slab Elevations (NB)                         | Sheet No. 5 of 38     |
| Top of Slab Elevations (SB)                         | Sheet No. 6 of 38     |
| Top of Approach Slab Elevations (NB)                | Sheet No. 7 of 38     |
| Top of Approach Slab Elevations (SB)                | Sheet No. 8 of 38     |
| Superstructure (NB)                                 | Sheet No. 9 of 38     |
| Superstructure Details (NB)                         | Sheet No. 10 of 38    |
| Superstructure (SB)                                 | Sheet No. 11 of 38    |
| Superstructure Details (SB)                         | Sheet No. 12 of 38    |
| Diaphragm Details                                   | Sheet No. 13 of 38    |
| Bridge Approach Slab Details                        | Sheet No. 14-15 of 38 |
| Steel Framing Plan & Details (NB)                   | Sheet No. 16 of 38    |
| Steel Framing Details (NB)                          | Sheet No. 17 of 38    |
| Steel Framing Plan & Details (SB)                   | Sheet No. 18 of 38    |
| Steel Framing Details (SB)                          | Sheet No. 19 of 38    |
| Bearing Details                                     | Sheet No. 20 of 38    |
| North Abutment (NB)                                 | Sheet No. 21 of 38    |
| South Abutment (NB)                                 | Sheet No. 22 of 38    |
| North Abutment (SB)                                 | Sheet No. 23 of 38    |
| South Abutment (SB)                                 | Sheet No. 24 of 38    |
| Pier 1 (NB)   | Sheet No. 25 of 38    |
| Pier 2 (NB)   | Sheet No. 26 of 38    |
| Pier Details (NB)                                   | Sheet No. 27 of 38    |
| Pier 1 (SB)   | Sheet No. 28 of 38    |
| Pier 2 (SB)   | Sheet No. 29 of 38    |
| Pier Details (SB)                                   | Sheet No. 30 of 38    |
| HP Pile Details                                     | Sheet No. 31 of 38    |
| Bar Splicer Assembly and Mechanical Splicer Details | Sheet No. 32 of 38    |
| Boring Logs   | Sheet No. 33-38 of 38 |



Note: All drainage system components shall extend to 2'-0" from the end of each wingwall except an outlet pipe shall extend until intersecting with side slopes. The pipes shall drain into concrete headwalls. (See Article 601.05 of the Standard Specifications and Highway Standard 601101).

**SN 091-0077**

**DESIGN SCOUR ELEVATION TABLE**

| Event / Limit State | Design Scour Elevations (ft.) |        |        |          |          | Item 113 |
|---------------------|-------------------------------|--------|--------|----------|----------|----------|
|                     | N. Abut.                      | Pier 1 | Pier 2 | S. Abut. | Item 113 |          |
| Q100                | 402.3                         | 369.7  | 369.7  | 403.0    |          | 5        |
| Q200                | 402.3                         | 369.7  | 369.7  | 403.0    |          |          |
| Design              | 402.3                         | 369.7  | 369.7  | 403.0    |          |          |
| Check               | 402.3                         | 369.7  | 369.7  | 403.0    |          |          |

**SN 091-0078**

**DESIGN SCOUR ELEVATION TABLE**

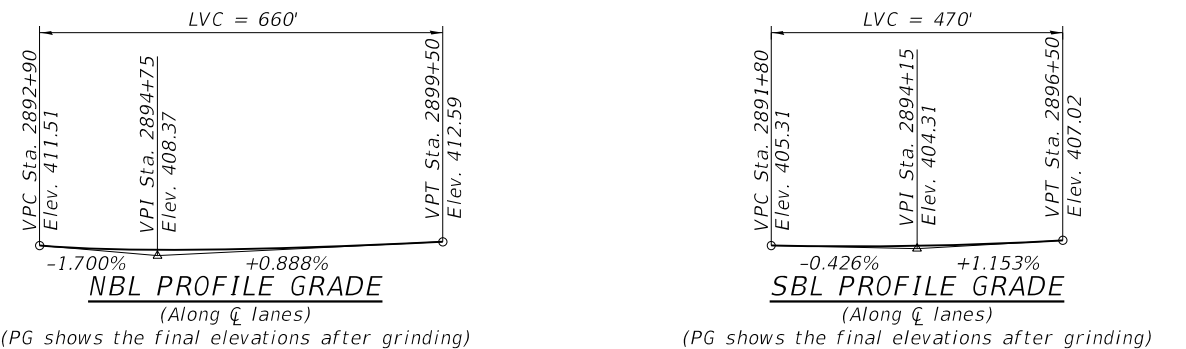
| Event / Limit State | Design Scour Elevations (ft.) |        |        |          |          | Item 113 |
|---------------------|-------------------------------|--------|--------|----------|----------|----------|
|                     | N. Abut.                      | Pier 1 | Pier 2 | S. Abut. | Item 113 |          |
| Q100                | 397.2                         | 370.3  | 370.3  | 398.2    |          | 5        |
| Q200                | 397.2                         | 370.3  | 370.3  | 398.2    |          |          |
| Design              | 397.2                         | 370.3  | 370.3  | 398.2    |          |          |
| Check               | 397.2                         | 370.3  | 370.3  | 398.2    |          |          |

**STATION 2896+15.00**  
**BUILT 20\_\_ BY**  
**STATE OF ILLINOIS**  
**F.A.I. RT. 57 SEC. (91-3)B-2**  
**LOADING HL-93**  
**STR. NO. 091-0077**

**NORTHBOUND NAME PLATE**  
See Std. 515001

**STATION 2894+90.07**  
**BUILT 20\_\_ BY**  
**STATE OF ILLINOIS**  
**F.A.I. RT. 57 SEC. (91-3)B-2**  
**LOADING HL-93**  
**STR. NO. 091-0078**

**SOUTHBOUND NAME PLATE**  
See Std. 515001



(PG shows the final elevations after grinding) **SN 091-0077 WATERWAY INFORMATION**

| Flood              | Freq. Yr. | Discharge (cfs) | Waterway Opening (sq. ft.) |       |        | Head (ft.) |       | Headwater Elev. (ft.) |       |
|--------------------|-----------|-----------------|----------------------------|-------|--------|------------|-------|-----------------------|-------|
|                    |           |                 | Exist.                     | Prop. | H.W.E. | Exist.     | Prop. | Exist.                | Prop. |
| Design             | 10        | 4150            | 887                        | 898   | 398.0  | 1.0        | 1.0   | 399.0                 | 399.0 |
| Base               | 50        | 6270            | 1044                       | 1077  | 399.7  | 1.5        | 1.4   | 401.2                 | 401.1 |
| Scour Design Check | 100       | 7180            | 1102                       | 1147  | 400.3  | 1.7        | 1.7   | 402.0                 | 402.0 |
| Overtop Existing   | 200       | 8197            | 1161                       | 1218  | 400.9  | 2.0        | 2.0   | 402.9                 | 402.9 |
| Overtop Proposed   | -         | -               | -                          | -     | -      | -          | -     | -                     | -     |
| Max. Calc.         | 500       | 9560            | 1222                       | 1291  | 401.5  | 2.4        | 2.4   | 403.9                 | 403.9 |

10 Year Outlet Velocity through Existing Structure: 4.6 ft/s  
 10 Year Outlet Velocity through Proposed Structure: 4.5 ft/s

| Flood              | Freq. Yr. | Discharge (cfs) | Waterway Opening (sq. ft.) |       |        | Head (ft.) |       | Headwater Elev. (ft.) |       |
|--------------------|-----------|-----------------|----------------------------|-------|--------|------------|-------|-----------------------|-------|
|                    |           |                 | Exist.                     | Prop. | H.W.E. | Exist.     | Prop. | Exist.                | Prop. |
| Design             | 10        | 4150            | 856                        | 890   | 398.1  | 1.0        | 1.0   | 399.1                 | 399.1 |
| Base               | 50        | 6270            | 1054                       | 1098  | 400.0  | 1.5        | 1.4   | 401.5                 | 401.4 |
| Scour Design Check | 100       | 7180            | 1103                       | 1177  | 400.7  | 1.7        | 1.7   | 402.4                 | 402.4 |
| Overtop Existing   | 200       | 8197            | 1160                       | 1244  | 401.3  | 2.0        | 2.0   | 403.3                 | 403.3 |
| Overtop Proposed   | -         | -               | -                          | -     | -      | -          | -     | -                     | -     |
| Max. Calc.         | 500       | 9560            | 1222                       | 1290  | 402.1  | 2.4        | 2.4   | 404.5                 | 404.5 |

10 Year Outlet Velocity through Existing Structure: 4.6 ft/s  
 10 Year Outlet Velocity through Proposed Structure: 4.3 ft/s

**TOTAL BILL OF MATERIAL**

| ITEM                                     | UNIT    | SUPER   | SUB     | TOTAL   |
|--|---------|---------|---------|---------|
| Stone Riprap, Class A5                   | Sq. Yd. |         | 5,353   | 5,353   |
| Filter Fabric                            | Sq. Yd. |         | 5,353   | 5,353   |
| Removal of Existing Structures No. 1     | Each    |         |         | 1       |
| Removal of Existing Structures No. 2     | Each    |         |         | 1       |
| Structure Excavation                     | Cu. Yd. |         | 894     | 894     |
| Floor Drains                             | Each    | 22      |         | 22      |
| Concrete Structures                      | Cu. Yd. |         | 605.4   | 605.4   |
| Concrete Superstructure                  | Cu. Yd. | 678.3   |         | 678.3   |
| Protective Coat                          | Sq. Yd. | 2,783   |         | 2,783   |
| Concrete Superstructure (Approach Slab)  | Cu. Yd. | 249.0   |         | 249.0   |
| Furnishing and Erecting Structural Steel | L Sum   | 1       |         | 1       |
| Stud Shear Connectors                    | Each    | 14,094  |         | 14,094  |
| Reinforcement Bars                       | Pound   |         | 59,610  | 59,610  |
| Reinforcement Bars, Epoxy Coated         | Pound   | 258,400 | 137,650 | 396,050 |
| Bar Splicers                             | Each    |         | 444     | 444     |
| Mechanical Splicers                      | Each    |         | 128     | 128     |
| Furnishing Steel Piles HP14x117          | Foot    |         | 1,885   | 1,885   |
| Driving Piles                            | Foot    |         | 1,885   | 1,885   |
| Pile Shoes                               | Each    |         | 52      | 52      |
| Name Plates                              | Each    | 2       |         | 2       |
| Drilled Shaft in Soil                    | Cu. Yd. |         | 312.5   | 312.5   |
| Drilled Shaft in Rock                    | Cu. Yd. |         | 130.5   | 130.5   |
| Anchor Bolts, 1"                         | Each    | 96      |         | 96      |
| Geocomposite Wall Drain                  | Sq. Yd. |         | 226     | 226     |
| Crosshole Sonic Logging Access Ducts     | Foot    |         | 480     | 480     |
| Crosshole Sonic Logging Testing          | Each    |         | 16      | 16      |
| Bridge Deck Grooving (Longitudinal)      | Sq. Yd. | 1,998   |         | 1,998   |
| Granular Backfill for Structures         | Cu. Yd. |         | 432     | 432     |
| Diamond Grinding (Bridge Section)        | Sq. Yd. | 2,732   |         | 2,732   |
| Pipe Underdrains for Structures 4"       | Foot    |         | 348     | 348     |
| Deck Slab Repair (Full Depth, Type I)    | Sq. Yd. |         | 10      | 10      |
| Deck Slab Repair (Full Depth, Type II)   | Sq. Yd. |         | 40      | 40      |

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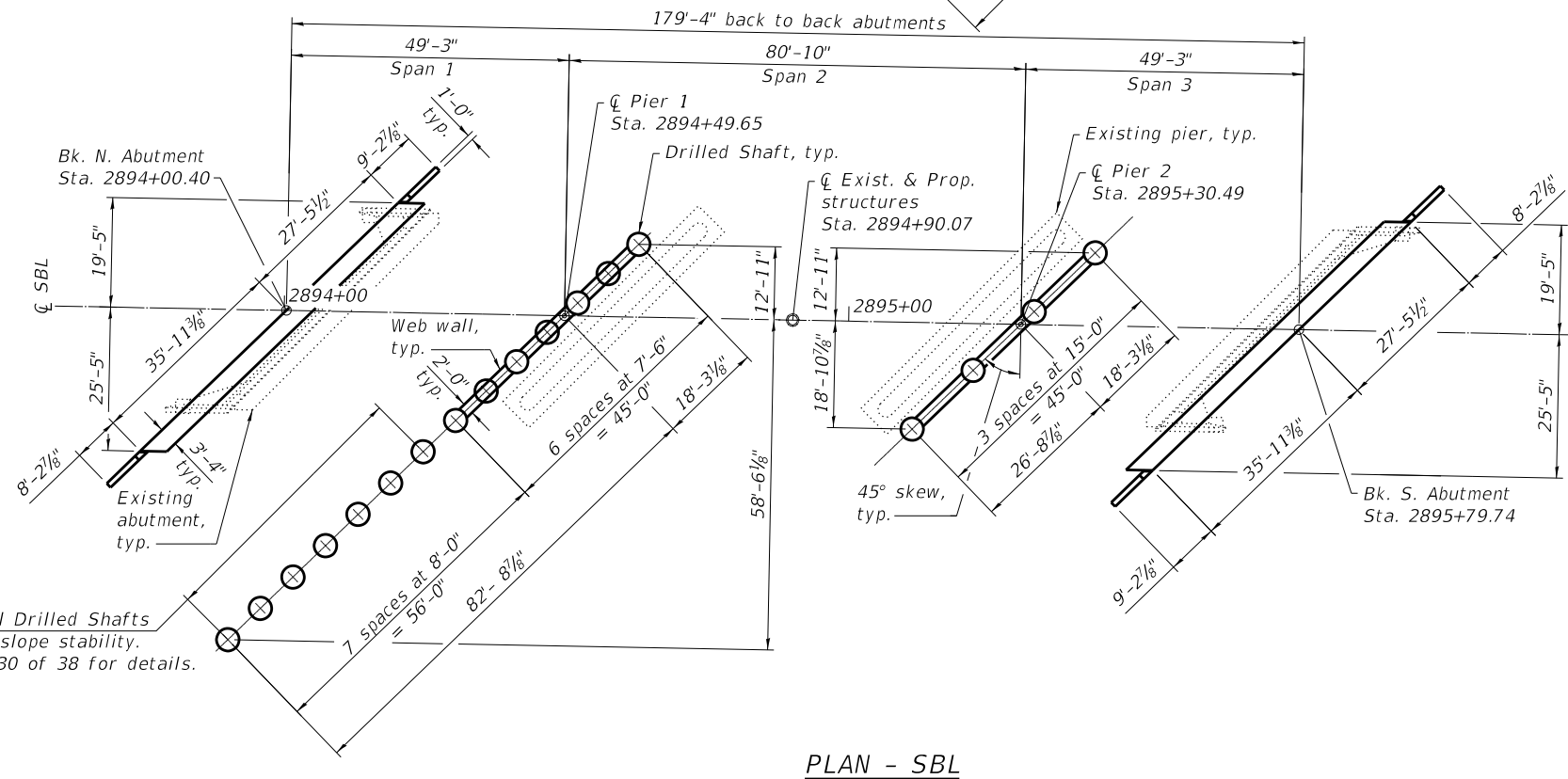
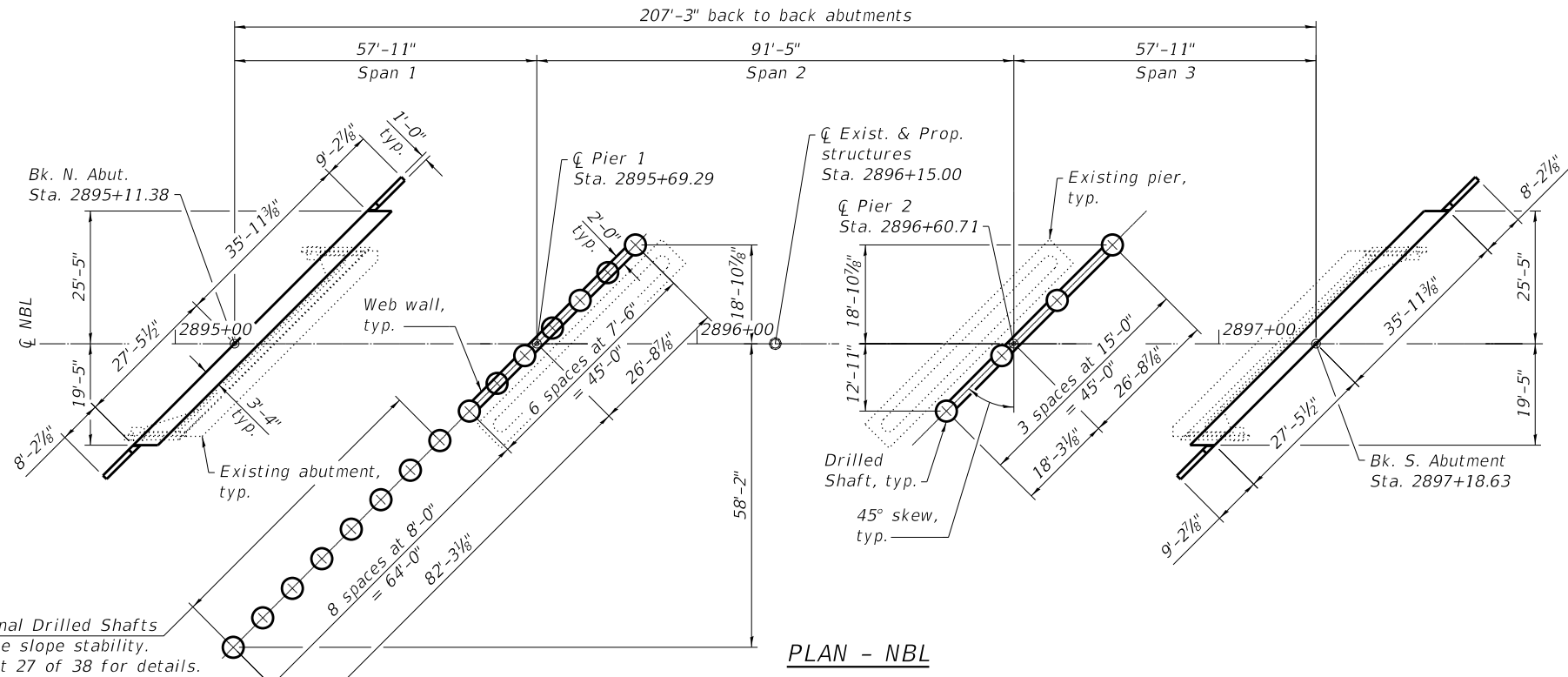
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| USER NAME = kah              | DESIGNED - KJA 07/19    | REVISED - |
| ESCA PROJECT NO. 1259.11     | CHECKED - RTM/SHL 07/19 | REVISED - |
| PLOT SCALE = 0:2" = 1' / in. | DRAWN - KAH 07/19       | REVISED - |
| PLOT DATE = 11/4/2019        | CHECKED - MTD/SHL 08/19 | REVISED - |

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**GENERAL DATA**  
**STRUCTURE NO. 091-0077 (NB) & 091-0078 (SB)**

SHEET 2 OF 38 SHEETS

| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 68        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |



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USER NAME = kah  
 ESCA PROJECT NO. 1259.11  
 PLOT SCALE = 0:2" = 1' / in.  
 PLOT DATE = 10/15/2019

DESIGNED - KJA 07/19  
 CHECKED - RTM/SHL 07/19  
 DRAWN - KAH 07/19  
 CHECKED - MTD/SHL 08/19

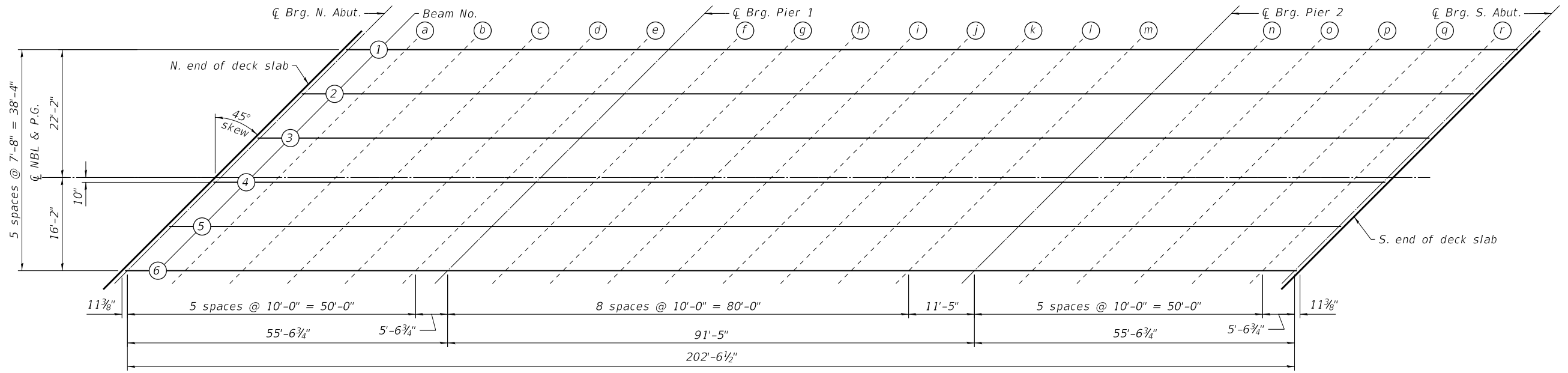
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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

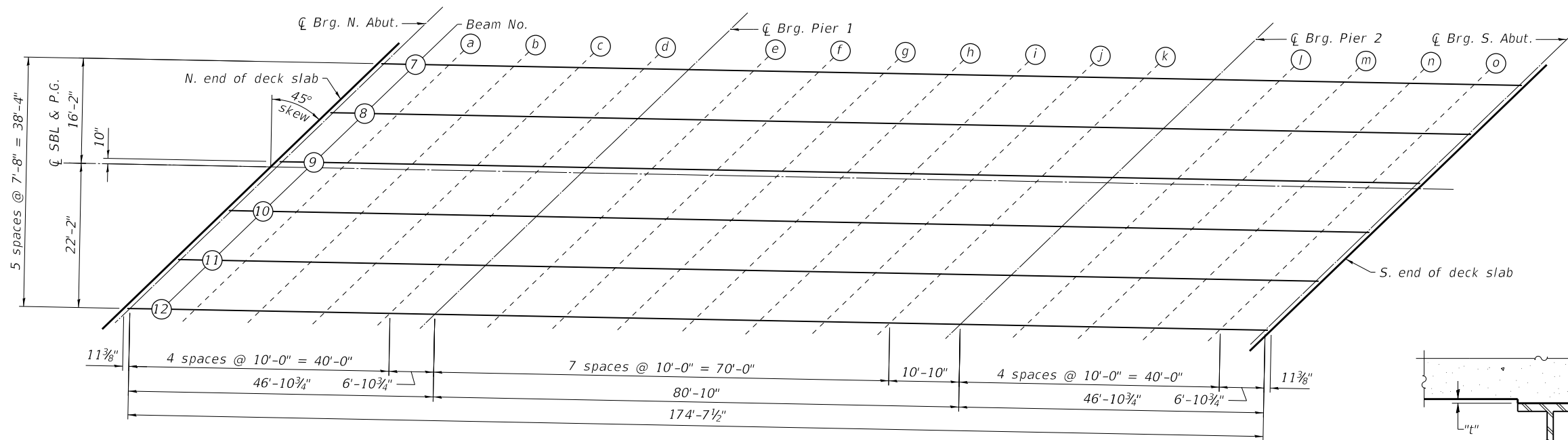
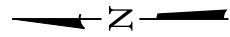
**SUBSTRUCTURE LAYOUT**  
**STRUCTURE NO. 091-0077 (NB) & 091-0078 (SB)**

SHEET 3 OF 38 SHEETS

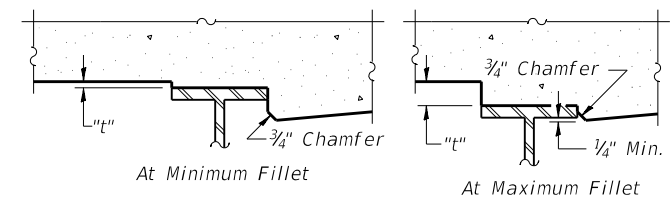
| F.A.I. RTE.        | SECTION   | COUNTY   | TOTAL SHEETS     | SHEET NO. |
|--------------------|-----------|----------|------------------|-----------|
| 57                 | (91-3)B-2 | UNION    | 161              | 69        |
| CONTRACT NO. 78504 |           |          |                  |           |
|                    |           | ILLINOIS | FED. AID PROJECT |           |



PLAN - NBL



PLAN - SBL



To determine "t": After all structural steel has been erected, elevations of the top flanges of the beams shall be taken at intervals shown on this sheet. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection and Grinding" shown on Sheets 5 and 6 of 38, minus the initial slab thickness prior to grinding, equals the fillet heights "t" above top flange of beams.

The slab is to be ground after curing to achieve smoothness, but the slab is not to be ground to elevations below the "Theoretical Grade Elevations" shown on Sheets 5 and 6 of 38. For grinding the deck, see Special Provisions.

**FILLET HEIGHTS**

MODEL: PLOT  
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USER NAME = kah  
ESCA PROJECT NO. 1259.11  
PLOT SCALE = 0:2 " = 1' / in.  
PLOT DATE = 10/15/2019

DESIGNED - KJA 07/19  
CHECKED - RTM/SHL 07/19  
DRAWN - KAH 07/19  
CHECKED - MTD/SHL 08/19

REVISED -  
REVISED -  
REVISED -  
REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS  
STRUCTURE NO. 091-0077 (NB) & 091-0078 (SB)

SHEET 4 OF 38 SHEETS

| F.A.I. RTE.        | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|-----------|--------|--------------|-----------|
| 57                 | (91-3)B-2 | UNION  | 161          | 70        |
| CONTRACT NO. 78504 |           |        |              |           |

ILLINOIS FED. AID PROJECT



**BEAM 1**

| Location        | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Dead Load Deflection and Grinding |
|-----------------|------------|--------------|------------------------------|---|
| N. End of Slab  | 2895+34.96 | -22.17       | 409.84                       | 409.86  |
| ☐ Brg. N. Abut. | 2895+35.90 | -22.17       | 409.84                       | 409.86  |
| a               | 2895+45.90 | -22.17       | 409.86                       | 409.89  |
| b               | 2895+55.90 | -22.17       | 409.89                       | 409.92  |
| c               | 2895+65.90 | -22.17       | 409.92                       | 409.95  |
| d               | 2895+75.90 | -22.17       | 409.95                       | 409.97  |
| e               | 2895+85.90 | -22.17       | 409.99                       | 410.01  |
| ☐ Brg. Pier 1   | 2895+91.46 | -22.17       | 410.00                       | 410.02  |
| f               | 2896+01.46 | -22.17       | 410.04                       | 410.09  |
| g               | 2896+11.46 | -22.17       | 410.08                       | 410.16  |
| h               | 2896+21.46 | -22.17       | 410.11                       | 410.22  |
| i               | 2896+31.46 | -22.17       | 410.15                       | 410.28  |
| j               | 2896+41.46 | -22.17       | 410.19                       | 410.32  |
| k               | 2896+51.46 | -22.17       | 410.24                       | 410.35  |
| l               | 2896+61.46 | -22.17       | 410.28                       | 410.37  |
| m               | 2896+71.46 | -22.17       | 410.33                       | 410.38  |
| ☐ Brg. Pier 2   | 2896+82.88 | -22.17       | 410.38                       | 410.40  |
| n               | 2896+92.88 | -22.17       | 410.43                       | 410.44  |
| o               | 2897+02.88 | -22.17       | 410.48                       | 410.50  |
| p               | 2897+12.88 | -22.17       | 410.53                       | 410.56  |
| q               | 2897+22.88 | -22.17       | 410.58                       | 410.61  |
| r               | 2897+32.88 | -22.17       | 410.64                       | 410.66  |
| ☐ Brg. S. Abut. | 2897+38.44 | -22.17       | 410.67                       | 410.69  |
| S. End of Slab  | 2897+39.38 | -22.17       | 410.68                       | 410.70  |

**BEAM 2**

| Location        | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Dead Load Deflection and Grinding |
|-----------------|------------|--------------|------------------------------|---|
| N. End of Slab  | 2895+27.29 | -14.50       | 409.97                       | 409.99  |
| ☐ Brg. N. Abut. | 2895+28.24 | -14.50       | 409.97                       | 409.99  |
| a               | 2895+38.24 | -14.50       | 410.00                       | 410.02  |
| b               | 2895+48.24 | -14.50       | 410.02                       | 410.05  |
| c               | 2895+58.24 | -14.50       | 410.05                       | 410.08  |
| d               | 2895+68.24 | -14.50       | 410.08                       | 410.10  |
| e               | 2895+78.24 | -14.50       | 410.11                       | 410.13  |
| ☐ Brg. Pier 1   | 2895+83.79 | -14.50       | 410.13                       | 410.15  |
| f               | 2895+93.79 | -14.50       | 410.17                       | 410.21  |
| g               | 2896+03.79 | -14.50       | 410.20                       | 410.28  |
| h               | 2896+13.79 | -14.50       | 410.24                       | 410.35  |
| i               | 2896+23.79 | -14.50       | 410.28                       | 410.40  |
| j               | 2896+33.79 | -14.50       | 410.32                       | 410.44  |
| k               | 2896+43.79 | -14.50       | 410.36                       | 410.47  |
| l               | 2896+53.79 | -14.50       | 410.40                       | 410.48  |
| m               | 2896+63.79 | -14.50       | 410.44                       | 410.49  |
| ☐ Brg. Pier 2   | 2896+75.21 | -14.50       | 410.50                       | 410.52  |
| n               | 2896+85.21 | -14.50       | 410.54                       | 410.56  |
| o               | 2896+95.21 | -14.50       | 410.59                       | 410.61  |
| p               | 2897+05.21 | -14.50       | 410.64                       | 410.67  |
| q               | 2897+15.21 | -14.50       | 410.70                       | 410.72  |
| r               | 2897+25.21 | -14.50       | 410.75                       | 410.77  |
| ☐ Brg. S. Abut. | 2897+30.77 | -14.50       | 410.78                       | 410.80  |
| S. End of Slab  | 2897+31.72 | -14.50       | 410.79                       | 410.81  |

**BEAM 3**

| Location        | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Dead Load Deflection and Grinding |
|-----------------|------------|--------------|------------------------------|---|
| N. End of Slab  | 2895+19.63 | -6.83        | 410.08                       | 410.10  |
| ☐ Brg. N. Abut. | 2895+20.57 | -6.83        | 410.08                       | 410.10  |
| a               | 2895+30.57 | -6.83        | 410.11                       | 410.13  |
| b               | 2895+40.57 | -6.83        | 410.13                       | 410.16  |
| c               | 2895+50.57 | -6.83        | 410.16                       | 410.18  |
| d               | 2895+60.57 | -6.83        | 410.19                       | 410.20  |
| e               | 2895+70.57 | -6.83        | 410.22                       | 410.24  |
| ☐ Brg. Pier 1   | 2895+76.12 | -6.83        | 410.23                       | 410.25  |
| f               | 2895+86.12 | -6.83        | 410.27                       | 410.31  |
| g               | 2895+96.12 | -6.83        | 410.30                       | 410.38  |
| h               | 2896+06.12 | -6.83        | 410.34                       | 410.44  |
| i               | 2896+16.12 | -6.83        | 410.37                       | 410.50  |
| j               | 2896+26.12 | -6.83        | 410.41                       | 410.54  |
| k               | 2896+36.12 | -6.83        | 410.45                       | 410.56  |
| l               | 2896+46.12 | -6.83        | 410.49                       | 410.58  |
| m               | 2896+56.12 | -6.83        | 410.54                       | 410.59  |
| ☐ Brg. Pier 2   | 2896+67.54 | -6.83        | 410.59                       | 410.61  |
| n               | 2896+77.54 | -6.83        | 410.63                       | 410.65  |
| o               | 2896+87.54 | -6.83        | 410.68                       | 410.70  |
| p               | 2896+97.54 | -6.83        | 410.73                       | 410.76  |
| q               | 2897+07.54 | -6.83        | 410.78                       | 410.81  |
| r               | 2897+17.54 | -6.83        | 410.84                       | 410.86  |
| ☐ Brg. S. Abut. | 2897+23.11 | -6.83        | 410.87                       | 410.89  |
| S. End of Slab  | 2897+24.05 | -6.83        | 410.87                       | 410.89  |

**☐ NBL & P.G.**

| Location        | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Dead Load Deflection and Grinding |
|-----------------|------------|--------------|------------------------------|---|
| N. End of Slab  | 2895+12.79 | 0.00         | 410.17                       | 410.19  |
| ☐ Brg. N. Abut. | 2895+13.74 | 0.00         | 410.17                       | 410.19  |
| a               | 2895+23.74 | 0.00         | 410.19                       | 410.22  |
| b               | 2895+33.74 | 0.00         | 410.22                       | 410.24  |
| c               | 2895+43.74 | 0.00         | 410.24                       | 410.27  |
| d               | 2895+53.74 | 0.00         | 410.27                       | 410.29  |
| e               | 2895+63.74 | 0.00         | 410.30                       | 410.32  |
| ☐ Brg. Pier 1   | 2895+69.29 | 0.00         | 410.32                       | 410.34  |
| f               | 2895+79.29 | 0.00         | 410.35                       | 410.39  |
| g               | 2895+89.29 | 0.00         | 410.38                       | 410.46  |
| h               | 2895+99.29 | 0.00         | 410.41                       | 410.52  |
| i               | 2896+09.29 | 0.00         | 410.45                       | 410.58  |
| j               | 2896+19.29 | 0.00         | 410.49                       | 410.61  |
| k               | 2896+29.29 | 0.00         | 410.53                       | 410.64  |
| l               | 2896+39.29 | 0.00         | 410.57                       | 410.65  |
| m               | 2896+49.29 | 0.00         | 410.61                       | 410.66  |
| ☐ Brg. Pier 2   | 2896+60.71 | 0.00         | 410.66                       | 410.68  |
| n               | 2896+70.71 | 0.00         | 410.71                       | 410.72  |
| o               | 2896+80.71 | 0.00         | 410.75                       | 410.77  |
| p               | 2896+90.71 | 0.00         | 410.80                       | 410.83  |
| q               | 2897+00.71 | 0.00         | 410.85                       | 410.88  |
| r               | 2897+10.71 | 0.00         | 410.90                       | 410.92  |
| ☐ Brg. S. Abut. | 2897+16.27 | 0.00         | 410.93                       | 410.95  |
| S. End of Slab  | 2897+17.22 | 0.00         | 410.94                       | 410.96  |

**BEAM 4**

| Location        | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Dead Load Deflection and Grinding |
|-----------------|------------|--------------|------------------------------|---|
| N. End of Slab  | 2895+11.96 | 0.83         | 410.15                       | 410.17  |
| ☐ Brg. N. Abut. | 2895+12.90 | 0.83         | 410.15                       | 410.18  |
| a               | 2895+22.90 | 0.83         | 410.18                       | 410.20  |
| b               | 2895+32.90 | 0.83         | 410.20                       | 410.23  |
| c               | 2895+42.90 | 0.83         | 410.23                       | 410.25  |
| d               | 2895+52.90 | 0.83         | 410.25                       | 410.27  |
| e               | 2895+62.90 | 0.83         | 410.28                       | 410.30  |
| ☐ Brg. Pier 1   | 2895+68.46 | 0.83         | 410.30                       | 410.32  |
| f               | 2895+78.46 | 0.83         | 410.33                       | 410.38  |
| g               | 2895+88.46 | 0.83         | 410.36                       | 410.44  |
| h               | 2895+98.46 | 0.83         | 410.40                       | 410.51  |
| i               | 2896+08.46 | 0.83         | 410.43                       | 410.56  |
| j               | 2896+18.46 | 0.83         | 410.47                       | 410.60  |
| k               | 2896+28.46 | 0.83         | 410.51                       | 410.62  |
| l               | 2896+38.46 | 0.83         | 410.55                       | 410.64  |
| m               | 2896+48.46 | 0.83         | 410.59                       | 410.64  |
| ☐ Brg. Pier 2   | 2896+59.88 | 0.83         | 410.64                       | 410.66  |
| n               | 2896+69.88 | 0.83         | 410.69                       | 410.70  |
| o               | 2896+79.88 | 0.83         | 410.74                       | 410.76  |
| p               | 2896+89.88 | 0.83         | 410.78                       | 410.81  |
| q               | 2896+99.88 | 0.83         | 410.83                       | 410.86  |
| r               | 2897+09.88 | 0.83         | 410.89                       | 410.91  |
| ☐ Brg. S. Abut. | 2897+15.44 | 0.83         | 410.91                       | 410.93  |
| S. End of Slab  | 2897+16.38 | 0.83         | 410.92                       | 410.94  |

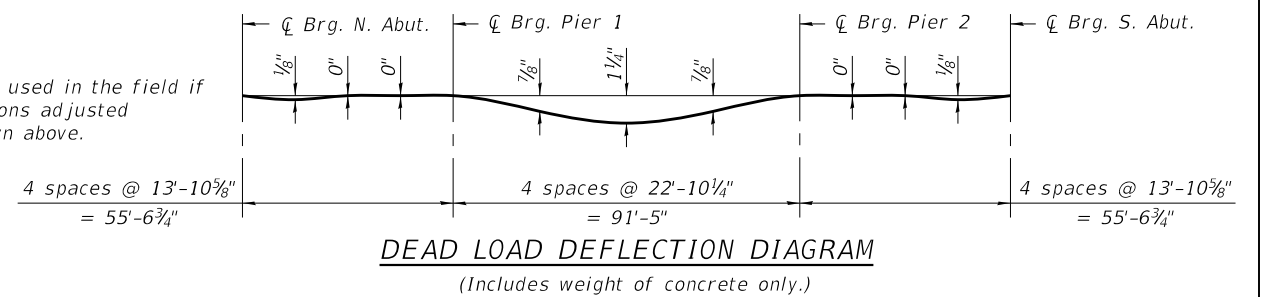
**BEAM 5**

| Location        | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Dead Load Deflection and Grinding |
|-----------------|------------|--------------|------------------------------|---|
| N. End of Slab  | 2895+04.29 | 8.50         | 410.02                       | 410.04  |
| ☐ Brg. N. Abut. | 2895+05.24 | 8.50         | 410.02                       | 410.04  |
| a               | 2895+15.24 | 8.50         | 410.04                       | 410.07  |
| b               | 2895+25.24 | 8.50         | 410.07                       | 410.09  |
| c               | 2895+35.24 | 8.50         | 410.09                       | 410.12  |
| d               | 2895+45.24 | 8.50         | 410.12                       | 410.14  |
| e               | 2895+55.24 | 8.50         | 410.15                       | 410.17  |
| ☐ Brg. Pier 1   | 2895+60.79 | 8.50         | 410.16                       | 410.18  |
| f               | 2895+70.79 | 8.50         | 410.19                       | 410.24  |
| g               | 2895+80.79 | 8.50         | 410.22                       | 410.30  |
| h               | 2895+90.79 | 8.50         | 410.26                       | 410.37  |
| i               | 2896+00.79 | 8.50         | 410.29                       | 410.42  |
| j               | 2896+10.79 | 8.50         | 410.33                       | 410.46  |
| k               | 2896+20.79 | 8.50         | 410.37                       | 410.48  |
| l               | 2896+30.79 | 8.50         | 410.41                       | 410.49  |
| m               | 2896+40.79 | 8.50         | 410.45                       | 410.50  |
| ☐ Brg. Pier 2   | 2896+52.21 | 8.50         | 410.49                       | 410.51  |
| n               | 2896+62.21 | 8.50         | 410.54                       | 410.55  |
| o               | 2896+72.21 | 8.50         | 410.58                       | 410.60  |
| p               | 2896+82.21 | 8.50         | 410.63                       | 410.66  |
| q               | 2896+92.21 | 8.50         | 410.68                       | 410.71  |
| r               | 2897+02.21 | 8.50         | 410.73                       | 410.75  |
| ☐ Brg. S. Abut. | 2897+07.77 | 8.50         | 410.76                       | 410.78  |
| S. End of Slab  | 2897+08.72 | 8.50         | 410.76                       | 410.78  |

**BEAM 6**

| Location        | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Dead Load Deflection and Grinding |
|-----------------|------------|--------------|------------------------------|---|
| N. End of Slab  | 2894+96.63 | 16.17        | 409.87                       | 409.89  |
| ☐ Brg. N. Abut. | 2894+97.57 | 16.17        | 409.87                       | 409.89  |
| a               | 2895+07.57 | 16.17        | 409.89                       | 409.92  |
| b               | 2895+17.57 | 16.17        | 409.91                       | 409.94  |
| c               | 2895+27.57 | 16.17        | 409.94                       | 409.96  |
| d               | 2895+37.57 | 16.17        | 409.96                       | 409.98  |
| e               | 2895+47.57 | 16.17        | 409.99                       | 410.01  |
| ☐ Brg. Pier 1   | 2895+53.12 | 16.17        | 410.00                       | 410.02  |
| f               | 2895+63.12 | 16.17        | 410.03                       | 410.08  |
| g               | 2895+73.12 | 16.17        | 410.06                       | 410.15  |
| h               | 2895+83.12 | 16.17        | 410.10                       | 410.21  |
| i               | 2895+93.12 | 16.17        | 410.13                       | 410.26  |
| j               | 2896+03.12 | 16.17        | 410.16                       | 410.29  |
| k               | 2896+13.12 | 16.17        | 410.20                       | 410.32  |
| l               | 2896+23.12 | 16.17        | 410.24                       | 410.33  |
| m               | 2896+33.12 | 16.17        | 410.28                       | 410.33  |
| ☐ Brg. Pier 2   | 2896+44.54 | 16.17        | 410.33                       | 410.35  |
| n               | 2896+54.54 | 16.17        | 410.37                       | 410.38  |
| o               | 2896+64.54 | 16.17        | 410.41                       | 410.43  |
| p               | 2896+74.54 | 16.17        | 410.46                       | 410.49  |
| q               | 2896+84.54 | 16.17        | 410.51                       | 410.54  |
| r               | 2896+94.54 | 16.17        | 410.56                       | 410.58  |
| ☐ Brg. S. Abut. | 2897+00.11 | 16.17        | 410.58                       | 410.60  |
| S. End of Slab  | 2897+01.05 | 16.17        | 410.59                       | 410.61  |

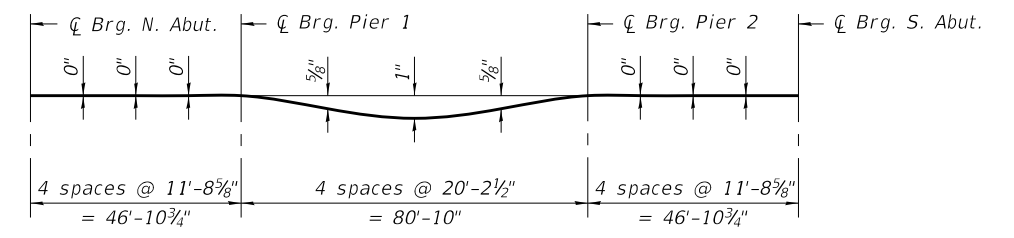
Note:  
The deflections in this diagram are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections and grinding as shown above.



| BEAM 7          |            |              |                              |   | BEAM 8          |            |              |                              |   | BEAM 9          |            |              |                              |   | ☐ SBL & P.G.    |            |              |                              |   |
|-----------------|------------|--------------|------------------------------|---|-----------------|------------|--------------|------------------------------|---|-----------------|------------|--------------|------------------------------|---|-----------------|------------|--------------|------------------------------|---|
| Location        | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Dead Load Deflection and Grinding | Location        | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Dead Load Deflection and Grinding | Location        | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Dead Load Deflection and Grinding | Location        | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Dead Load Deflection and Grinding |
| N. End of Slab  | 2894+17.98 | -16.17       | 404.99                       | 405.01  | N. End of Slab  | 2894+10.31 | -8.50        | 405.09                       | 405.11  | N. End of Slab  | 2894+02.65 | -0.83        | 405.18                       | 405.20  | N. End of Slab  | 2894+01.81 | 0.00         | 405.19                       | 405.21  |
| ☐ Brg. N. Abut. | 2894+18.92 | -16.17       | 404.99                       | 405.01  | ☐ Brg. N. Abut. | 2894+11.26 | -8.50        | 405.10                       | 405.12  | ☐ Brg. N. Abut. | 2894+03.59 | -0.83        | 405.19                       | 405.21  | ☐ Brg. N. Abut. | 2894+02.76 | 0.00         | 405.20                       | 405.22  |
| a               | 2894+28.92 | -16.17       | 405.03                       | 405.05  | a               | 2894+21.26 | -8.50        | 405.13                       | 405.16  | a               | 2894+13.59 | -0.83        | 405.22                       | 405.24  | a               | 2894+12.76 | 0.00         | 405.23                       | 405.25  |
| b               | 2894+38.92 | -16.17       | 405.07                       | 405.09  | b               | 2894+31.26 | -8.50        | 405.17                       | 405.20  | b               | 2894+23.59 | -0.83        | 405.26                       | 405.28  | b               | 2894+22.76 | 0.00         | 405.27                       | 405.29  |
| c               | 2894+48.92 | -16.17       | 405.12                       | 405.14  | c               | 2894+41.26 | -8.50        | 405.22                       | 405.24  | c               | 2894+33.59 | -0.83        | 405.30                       | 405.32  | c               | 2894+32.76 | 0.00         | 405.31                       | 405.33  |
| d               | 2894+58.92 | -16.17       | 405.17                       | 405.18  | d               | 2894+51.26 | -8.50        | 405.26                       | 405.28  | d               | 2894+43.59 | -0.83        | 405.34                       | 405.36  | d               | 2894+42.76 | 0.00         | 405.35                       | 405.37  |
| ☐ Brg. Pier 1   | 2894+65.82 | -16.17       | 405.20                       | 405.22  | ☐ Brg. Pier 1   | 2894+58.15 | -8.50        | 405.30                       | 405.32  | ☐ Brg. Pier 1   | 2894+50.48 | -0.83        | 405.38                       | 405.40  | ☐ Brg. Pier 1   | 2894+49.65 | 0.00         | 405.38                       | 405.40  |
| e               | 2894+75.82 | -16.17       | 405.26                       | 405.30  | e               | 2894+68.15 | -8.50        | 405.35                       | 405.40  | e               | 2894+60.48 | -0.83        | 405.43                       | 405.47  | e               | 2894+59.65 | 0.00         | 405.43                       | 405.48  |
| f               | 2894+85.82 | -16.17       | 405.32                       | 405.39  | f               | 2894+78.15 | -8.50        | 405.41                       | 405.48  | f               | 2894+70.48 | -0.83        | 405.48                       | 405.55  | f               | 2894+69.65 | 0.00         | 405.49                       | 405.56  |
| g               | 2894+95.82 | -16.17       | 405.38                       | 405.48  | g               | 2894+88.15 | -8.50        | 405.47                       | 405.56  | g               | 2894+80.48 | -0.83        | 405.54                       | 405.63  | g               | 2894+79.65 | 0.00         | 405.54                       | 405.64  |
| h               | 2895+05.82 | -16.17       | 405.44                       | 405.55  | h               | 2894+98.15 | -8.50        | 405.53                       | 405.63  | h               | 2894+90.48 | -0.83        | 405.60                       | 405.70  | h               | 2894+89.65 | 0.00         | 405.60                       | 405.71  |
| i               | 2895+15.82 | -16.17       | 405.51                       | 405.61  | i               | 2895+08.15 | -8.50        | 405.59                       | 405.69  | i               | 2895+00.48 | -0.83        | 405.66                       | 405.76  | i               | 2894+99.65 | 0.00         | 405.67                       | 405.76  |
| j               | 2895+25.82 | -16.17       | 405.58                       | 405.66  | j               | 2895+18.15 | -8.50        | 405.66                       | 405.74  | j               | 2895+10.48 | -0.83        | 405.73                       | 405.80  | j               | 2895+09.65 | 0.00         | 405.73                       | 405.81  |
| k               | 2895+35.82 | -16.17       | 405.66                       | 405.71  | k               | 2895+28.15 | -8.50        | 405.74                       | 405.78  | k               | 2895+20.48 | -0.83        | 405.80                       | 405.84  | k               | 2895+19.65 | 0.00         | 405.80                       | 405.85  |
| ☐ Brg. Pier 2   | 2895+46.66 | -16.17       | 405.74                       | 405.76  | ☐ Brg. Pier 2   | 2895+38.99 | -8.50        | 405.82                       | 405.84  | ☐ Brg. Pier 2   | 2895+31.32 | -0.83        | 405.88                       | 405.90  | ☐ Brg. Pier 2   | 2895+30.49 | 0.00         | 405.88                       | 405.90  |
| l               | 2895+56.66 | -16.17       | 405.83                       | 405.84  | l               | 2895+48.99 | -8.50        | 405.90                       | 405.91  | l               | 2895+41.32 | -0.83        | 405.95                       | 405.97  | l               | 2895+40.49 | 0.00         | 405.96                       | 405.97  |
| m               | 2895+66.66 | -16.17       | 405.91                       | 405.93  | m               | 2895+58.99 | -8.50        | 405.98                       | 406.00  | m               | 2895+51.32 | -0.83        | 406.03                       | 406.05  | m               | 2895+50.49 | 0.00         | 406.04                       | 406.06  |
| n               | 2895+76.66 | -16.17       | 406.00                       | 406.02  | n               | 2895+68.99 | -8.50        | 406.07                       | 406.09  | n               | 2895+61.32 | -0.83        | 406.12                       | 406.14  | n               | 2895+60.49 | 0.00         | 406.12                       | 406.15  |
| o               | 2895+86.66 | -16.17       | 406.09                       | 406.12  | o               | 2895+78.99 | -8.50        | 406.16                       | 406.18  | o               | 2895+71.32 | -0.83        | 406.20                       | 406.23  | o               | 2895+70.49 | 0.00         | 406.21                       | 406.23  |
| ☐ Brg. S. Abut. | 2895+93.55 | -16.17       | 406.16                       | 406.18  | ☐ Brg. S. Abut. | 2895+85.88 | -8.50        | 406.22                       | 406.24  | ☐ Brg. S. Abut. | 2895+78.22 | -0.83        | 406.27                       | 406.29  | ☐ Brg. S. Abut. | 2895+77.38 | 0.00         | 406.27                       | 406.29  |
| S. End of Slab  | 2895+94.49 | -16.17       | 406.17                       | 406.19  | S. End of Slab  | 2895+86.83 | -8.50        | 406.23                       | 406.25  | S. End of Slab  | 2895+79.16 | -0.83        | 406.27                       | 406.29  | S. End of Slab  | 2895+78.33 | 0.00         | 406.28                       | 406.30  |

| BEAM 10         |            |              |                              |   | BEAM 11         |            |              |                              |   | BEAM 12         |            |              |                              |   |
|-----------------|------------|--------------|------------------------------|---|-----------------|------------|--------------|------------------------------|---|-----------------|------------|--------------|------------------------------|---|
| Location        | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Dead Load Deflection and Grinding | Location        | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Dead Load Deflection and Grinding | Location        | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Dead Load Deflection and Grinding |
| N. End of Slab  | 2893+94.98 | 6.83         | 405.07                       | 405.09  | N. End of Slab  | 2893+87.31 | 14.50        | 404.92                       | 404.94  | N. End of Slab  | 2893+79.65 | 22.17        | 404.75                       | 404.77  |
| ☐ Brg. N. Abut. | 2893+95.92 | 6.83         | 405.07                       | 405.09  | ☐ Brg. N. Abut. | 2893+88.26 | 14.50        | 404.92                       | 404.94  | ☐ Brg. N. Abut. | 2893+80.59 | 22.17        | 404.75                       | 404.77  |
| a               | 2894+05.92 | 6.83         | 405.10                       | 405.13  | a               | 2893+98.26 | 14.50        | 404.95                       | 404.97  | a               | 2893+90.59 | 22.17        | 404.78                       | 404.80  |
| b               | 2894+15.92 | 6.83         | 405.14                       | 405.16  | b               | 2894+08.26 | 14.50        | 404.98                       | 405.01  | b               | 2894+00.59 | 22.17        | 404.81                       | 404.83  |
| c               | 2894+25.92 | 6.83         | 405.18                       | 405.19  | c               | 2894+18.26 | 14.50        | 405.02                       | 405.04  | c               | 2894+10.59 | 22.17        | 404.84                       | 404.86  |
| d               | 2894+35.92 | 6.83         | 405.22                       | 405.23  | d               | 2894+28.26 | 14.50        | 405.06                       | 405.07  | d               | 2894+20.59 | 22.17        | 404.88                       | 404.89  |
| ☐ Brg. Pier 1   | 2894+42.82 | 6.83         | 405.25                       | 405.27  | ☐ Brg. Pier 1   | 2894+35.15 | 14.50        | 405.09                       | 405.11  | ☐ Brg. Pier 1   | 2894+27.48 | 22.17        | 404.90                       | 404.92  |
| e               | 2894+52.82 | 6.83         | 405.30                       | 405.34  | e               | 2894+45.15 | 14.50        | 405.13                       | 405.18  | e               | 2894+37.48 | 22.17        | 404.94                       | 404.99  |
| f               | 2894+62.82 | 6.83         | 405.35                       | 405.42  | f               | 2894+55.15 | 14.50        | 405.18                       | 405.25  | f               | 2894+47.48 | 22.17        | 404.99                       | 405.06  |
| g               | 2894+72.82 | 6.83         | 405.40                       | 405.50  | g               | 2894+65.15 | 14.50        | 405.23                       | 405.33  | g               | 2894+57.48 | 22.17        | 405.04                       | 405.14  |
| h               | 2894+82.82 | 6.83         | 405.46                       | 405.56  | h               | 2894+75.15 | 14.50        | 405.29                       | 405.39  | h               | 2894+67.48 | 22.17        | 405.09                       | 405.20  |
| i               | 2894+92.82 | 6.83         | 405.52                       | 405.62  | i               | 2894+85.15 | 14.50        | 405.35                       | 405.44  | i               | 2894+77.48 | 22.17        | 405.15                       | 405.25  |
| j               | 2895+02.82 | 6.83         | 405.58                       | 405.66  | j               | 2894+95.15 | 14.50        | 405.41                       | 405.48  | j               | 2894+87.48 | 22.17        | 405.21                       | 405.28  |
| k               | 2895+12.82 | 6.83         | 405.65                       | 405.70  | k               | 2895+05.15 | 14.50        | 405.47                       | 405.52  | k               | 2894+97.48 | 22.17        | 405.27                       | 405.32  |
| ☐ Brg. Pier 2   | 2895+23.66 | 6.83         | 405.73                       | 405.75  | ☐ Brg. Pier 2   | 2895+15.99 | 14.50        | 405.55                       | 405.57  | ☐ Brg. Pier 2   | 2895+08.32 | 22.17        | 405.34                       | 405.36  |
| l               | 2895+33.66 | 6.83         | 405.80                       | 405.82  | l               | 2895+25.99 | 14.50        | 405.62                       | 405.63  | l               | 2895+18.32 | 22.17        | 405.41                       | 405.43  |
| m               | 2895+43.66 | 6.83         | 405.88                       | 405.90  | m               | 2895+35.99 | 14.50        | 405.69                       | 405.71  | m               | 2895+28.32 | 22.17        | 405.48                       | 405.50  |
| n               | 2895+53.66 | 6.83         | 405.96                       | 405.99  | n               | 2895+45.99 | 14.50        | 405.77                       | 405.80  | n               | 2895+38.32 | 22.17        | 405.56                       | 405.58  |
| o               | 2895+63.66 | 6.83         | 406.05                       | 406.07  | o               | 2895+55.99 | 14.50        | 405.85                       | 405.88  | o               | 2895+48.32 | 22.17        | 405.64                       | 405.66  |
| ☐ Brg. S. Abut. | 2895+70.55 | 6.83         | 406.11                       | 406.13  | ☐ Brg. S. Abut. | 2895+62.88 | 14.50        | 405.91                       | 405.93  | ☐ Brg. S. Abut. | 2895+55.22 | 22.17        | 405.69                       | 405.71  |
| S. End of Slab  | 2895+71.49 | 6.83         | 406.12                       | 406.14  | S. End of Slab  | 2895+63.83 | 14.50        | 405.92                       | 405.94  | S. End of Slab  | 2895+56.16 | 22.17        | 405.70                       | 405.72  |

Note:  
The deflections in this diagram are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections and grinding as shown above.



MODEL: PLOT FILE NAME: Y:\IDOT\1259-11\_78504\CADD\SP\_SN\_091-0077-0078\0910077-78504-06-TS\labElev5B.dgn



|                               |                         |           |
|-------------------------------|-------------------------|-----------|
| USER NAME = kah               | DESIGNED - KJA 07/19    | REVISED - |
| ESCA PROJECT NO. 1259.11      | CHECKED - RTM/SHL 07/19 | REVISED - |
| PLOT SCALE = 0:2 " = 1" / in. | DRAWN - KAH 07/19       | REVISED - |
| PLOT DATE = 10/15/2019        | CHECKED - MTD/SHL 08/19 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TOP OF SLAB ELEVATIONS (SB)  
STRUCTURE NO. 091-0078 (SB)**

|                           |           |        |              |           |
|---------------------------|-----------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
| 57                        | (91-3)B-2 | UNION  | 161          | 72        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |

**NORTH APPROACH SLAB (NB)**

**EAST EDGE OF SHOULDER**

| Location           | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Grinding |
|--------------------|------------|--------------|------------------------------|--|
| N. end of N. Appr. | 2895+06.79 | -24.00       | 409.73                       | 409.75   |
| A1                 | 2895+16.79 | -24.00       | 409.76                       | 409.78   |
| A2                 | 2895+26.79 | -24.00       | 409.78                       | 409.80   |
| S. End of N. Appr. | 2895+36.79 | -24.00       | 409.80                       | 409.82   |

**EAST EDGE OF ROADWAY**

| Location           | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Grinding |
|--------------------|------------|--------------|------------------------------|--|
| N. end of N. Appr. | 2894+94.79 | -12.00       | 409.95                       | 409.97   |
| A1                 | 2895+04.79 | -12.00       | 409.97                       | 409.99   |
| A2                 | 2895+14.79 | -12.00       | 409.99                       | 410.01   |
| S. End of N. Appr. | 2895+24.79 | -12.00       | 410.01                       | 410.03   |

**☐ NBL & P.G.**

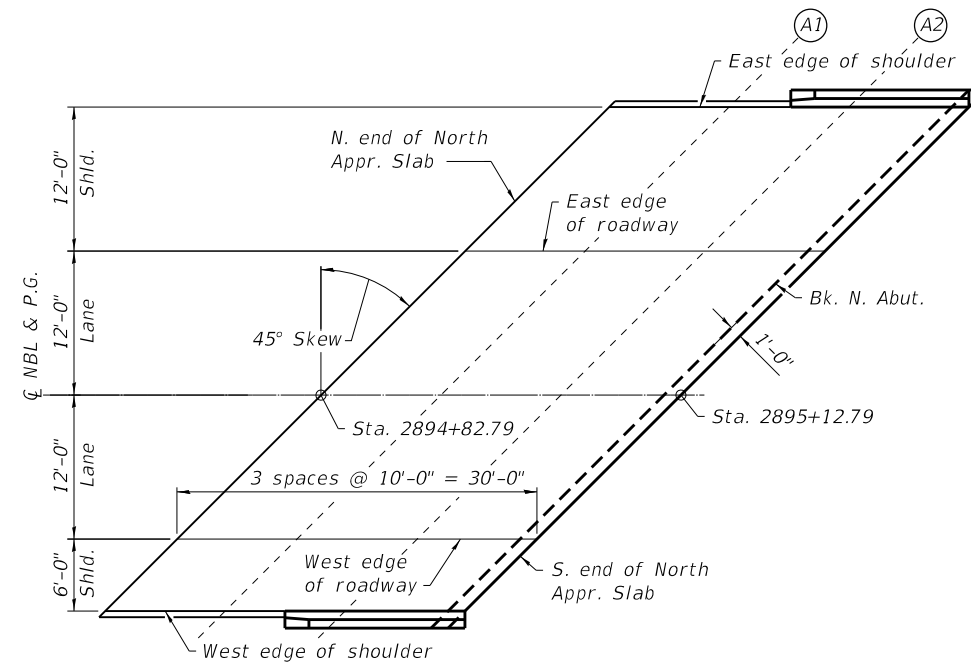
| Location           | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Grinding |
|--------------------|------------|--------------|------------------------------|--|
| N. end of N. Appr. | 2894+82.79 | 0.00         | 410.11                       | 410.13   |
| A1                 | 2894+92.79 | 0.00         | 410.13                       | 410.15   |
| A2                 | 2895+02.79 | 0.00         | 410.15                       | 410.17   |
| S. End of N. Appr. | 2895+12.79 | 0.00         | 410.17                       | 410.19   |

**WEST EDGE OF ROADWAY**

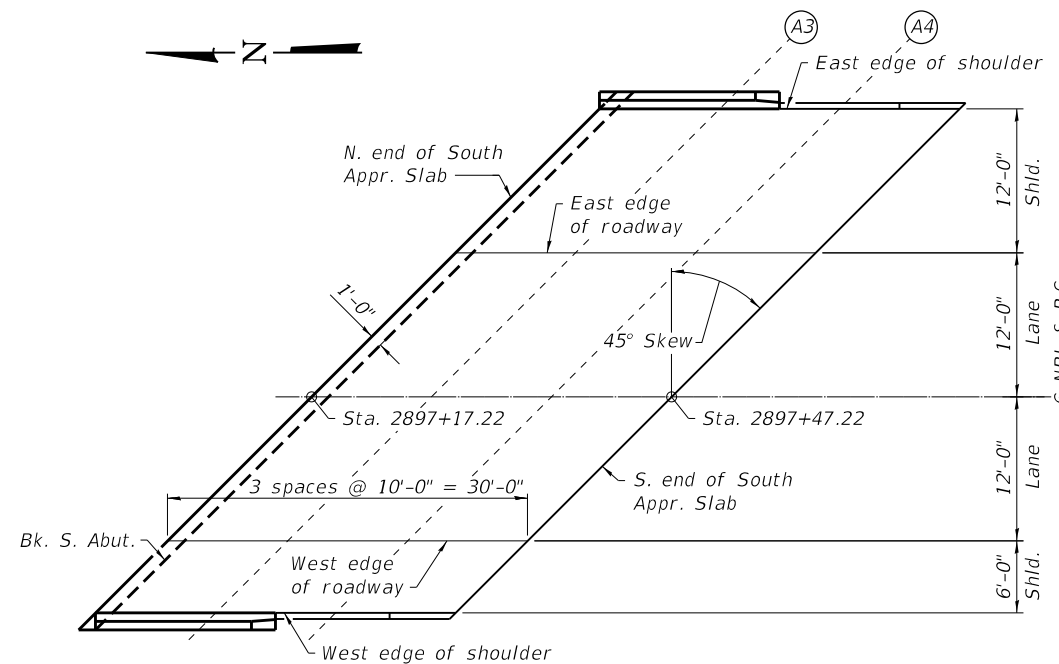
| Location           | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Grinding |
|--------------------|------------|--------------|------------------------------|--|
| N. end of N. Appr. | 2894+70.79 | 12.00        | 409.91                       | 409.93   |
| A1                 | 2894+80.79 | 12.00        | 409.92                       | 409.94   |
| A2                 | 2894+90.79 | 12.00        | 409.94                       | 409.96   |
| S. End of N. Appr. | 2895+00.79 | 12.00        | 409.96                       | 409.98   |

**WEST EDGE OF SHOULDER**

| Location           | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Grinding |
|--------------------|------------|--------------|------------------------------|--|
| N. end of N. Appr. | 2894+64.79 | 18.00        | 409.78                       | 409.80   |
| A1                 | 2894+74.79 | 18.00        | 409.79                       | 409.81   |
| A2                 | 2894+84+79 | 18.00        | 409.81                       | 409.83   |
| S. End of N. Appr. | 2894+94.79 | 18.00        | 409.83                       | 409.85   |



**N. APPROACH SLAB PLAN**



**S. APPROACH SLAB PLAN**

**SOUTH APPROACH SLAB (NB)**

**EAST EDGE OF SHOULDER**

| Location           | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Grinding |
|--------------------|------------|--------------|------------------------------|--|
| N. end of S. Appr. | 2897+41.22 | -24.00       | 410.65                       | 410.67   |
| A3                 | 2897+51.22 | -24.00       | 410.71                       | 410.73   |
| A4                 | 2897+61.22 | -24.00       | 410.77                       | 410.79   |
| S. End of S. Appr. | 2897+71.22 | -24.00       | 410.83                       | 410.85   |

**EAST EDGE OF ROADWAY**

| Location           | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Grinding |
|--------------------|------------|--------------|------------------------------|--|
| N. end of S. Appr. | 2897+29.22 | -12.00       | 410.82                       | 410.84   |
| A3                 | 2897+39.22 | -12.00       | 410.88                       | 410.90   |
| A4                 | 2897+49.22 | -12.00       | 410.94                       | 410.96   |
| S. End of S. Appr. | 2897+59.22 | -12.00       | 410.99                       | 411.01   |

**☐ NBL & P.G.**

| Location           | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Grinding |
|--------------------|------------|--------------|------------------------------|--|
| N. end of S. Appr. | 2897+17.22 | 0.00         | 410.94                       | 410.96   |
| A3                 | 2897+27.22 | 0.00         | 410.99                       | 411.01   |
| A4                 | 2897+37.22 | 0.00         | 411.05                       | 411.07   |
| S. End of S. Appr. | 2897+47.22 | 0.00         | 411.10                       | 411.12   |

**WEST EDGE OF ROADWAY**

| Location           | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Grinding |
|--------------------|------------|--------------|------------------------------|--|
| N. end of S. Appr. | 2897+05.22 | 12.00        | 410.69                       | 410.71   |
| A3                 | 2897+15.22 | 12.00        | 410.75                       | 410.77   |
| A4                 | 2897+25.22 | 12.00        | 410.80                       | 410.82   |
| S. End of S. Appr. | 2897+35.22 | 12.00        | 410.86                       | 410.88   |

**WEST EDGE OF SHOULDER**

| Location           | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Grinding |
|--------------------|------------|--------------|------------------------------|--|
| N. end of S. Appr. | 2896+99.22 | 18.00        | 410.54                       | 410.56   |
| A3                 | 2897+09.22 | 18.00        | 410.59                       | 410.61   |
| A4                 | 2897+19.22 | 18.00        | 410.65                       | 410.67   |
| S. End of S. Appr. | 2897+29.22 | 18.00        | 410.70                       | 410.72   |

MODEL: PLOT FILE NAME: Y:\IDOT\1259-11\_78504\CADD\SP\_SN\_091-0077-0078\0910077-78504-07-TAppSlabElevNB.dgn



USER NAME = kah  
 ESCA PROJECT NO. 1259.11  
 PLOT SCALE = 0.2" = 1' / in.  
 PLOT DATE = 10/15/2019

DESIGNED - KJA 07/19  
 CHECKED - RTM/SHL 07/19  
 DRAWN - KAH 07/19  
 CHECKED - MTD/SHL 08/19

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**TOP OF APPROACH SLAB ELEVATIONS (NB)  
 STRUCTURE NO. 091-0077 (NB)**

SHEET 7 OF 38 SHEETS

| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 73        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |

**NORTH APPROACH SLAB (SB)**

**EAST EDGE OF SHOULDER**

| Location           | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Grinding |
|--------------------|------------|--------------|------------------------------|--|
| N. end of N. Appr. | 2893+89.81 | -18.00       | 404.86                       | 404.88   |
| A1                 | 2893+99.81 | -18.00       | 404.89                       | 404.91   |
| A2                 | 2894+09.81 | -18.00       | 404.92                       | 404.94   |
| S. End of N. Appr. | 2894+19.81 | -18.00       | 404.96                       | 404.98   |

**EAST EDGE OF ROADWAY**

| Location           | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Grinding |
|--------------------|------------|--------------|------------------------------|--|
| N. end of N. Appr. | 2893+83.81 | -12.00       | 404.96                       | 404.98   |
| A1                 | 2893+93.81 | -12.00       | 404.99                       | 405.01   |
| A2                 | 2894+03.81 | -12.00       | 405.02                       | 405.04   |
| S. End of N. Appr. | 2894+13.81 | -12.00       | 405.05                       | 405.07   |

**☉ SBL & P.G.**

| Location           | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Grinding |
|--------------------|------------|--------------|------------------------------|--|
| N. end of N. Appr. | 2893+71.81 | 0.00         | 405.11                       | 405.13   |
| A1                 | 2893+81.81 | 0.00         | 405.14                       | 405.16   |
| A2                 | 2893+91.81 | 0.00         | 405.16                       | 405.18   |
| S. End of N. Appr. | 2894+01.81 | 0.00         | 405.19                       | 405.21   |

**WEST EDGE OF ROADWAY**

| Location           | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Grinding |
|--------------------|------------|--------------|------------------------------|--|
| N. end of N. Appr. | 2893+59.81 | 12.00        | 404.91                       | 404.93   |
| A1                 | 2893+69.81 | 12.00        | 404.93                       | 404.95   |
| A2                 | 2893+79.81 | 12.00        | 404.95                       | 404.97   |
| S. End of N. Appr. | 2893+89.81 | 12.00        | 404.98                       | 405.00   |

**WEST EDGE OF SHOULDER**

| Location           | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Grinding |
|--------------------|------------|--------------|------------------------------|--|
| N. end of N. Appr. | 2893+47.81 | 24.00        | 404.65                       | 404.67   |
| A1                 | 2893+57.81 | 24.00        | 404.66                       | 404.68   |
| A2                 | 2893+67.81 | 24.00        | 404.68                       | 404.70   |
| S. End of N. Appr. | 2893+77.81 | 24.00        | 404.71                       | 404.73   |

**SOUTH APPROACH SLAB (SB)**

**EAST EDGE OF SHOULDER**

| Location           | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Grinding |
|--------------------|------------|--------------|------------------------------|--|
| N. end of S. Appr. | 2895+96.33 | -18.00       | 406.15                       | 406.17   |
| A3                 | 2896+06.33 | -18.00       | 406.25                       | 406.27   |
| A4                 | 2896+16.33 | -18.00       | 406.35                       | 406.37   |
| S. End of S. Appr. | 2896+26.33 | -18.00       | 406.46                       | 406.48   |

**EAST EDGE OF ROADWAY**

| Location           | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Grinding |
|--------------------|------------|--------------|------------------------------|--|
| N. end of S. Appr. | 2895+90.33 | -12.00       | 406.21                       | 406.23   |
| A3                 | 2896+00.33 | -12.00       | 406.31                       | 406.33   |
| A4                 | 2896+10.33 | -12.00       | 406.41                       | 406.43   |
| S. End of S. Appr. | 2896+20.33 | -12.00       | 406.51                       | 406.53   |

**☉ SBL & P.G.**

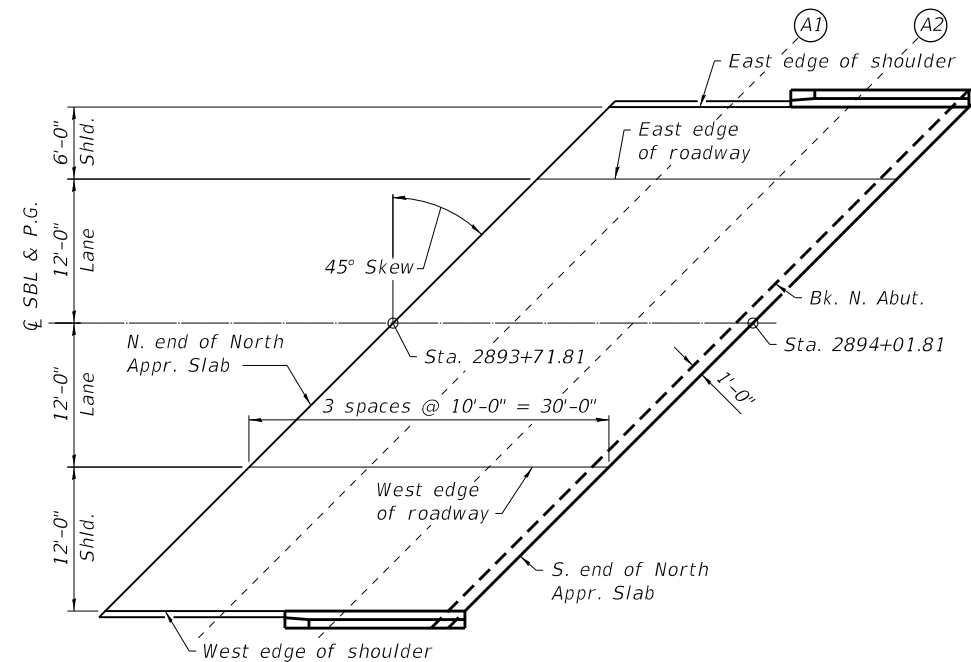
| Location           | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Grinding |
|--------------------|------------|--------------|------------------------------|--|
| N. end of S. Appr. | 2895+78.33 | 0.00         | 406.28                       | 406.30   |
| A3                 | 2895+88.33 | 0.00         | 406.37                       | 406.39   |
| A4                 | 2895+98.33 | 0.00         | 406.47                       | 406.49   |
| S. End of S. Appr. | 2896+08.33 | 0.00         | 406.57                       | 406.59   |

**WEST EDGE OF ROADWAY**

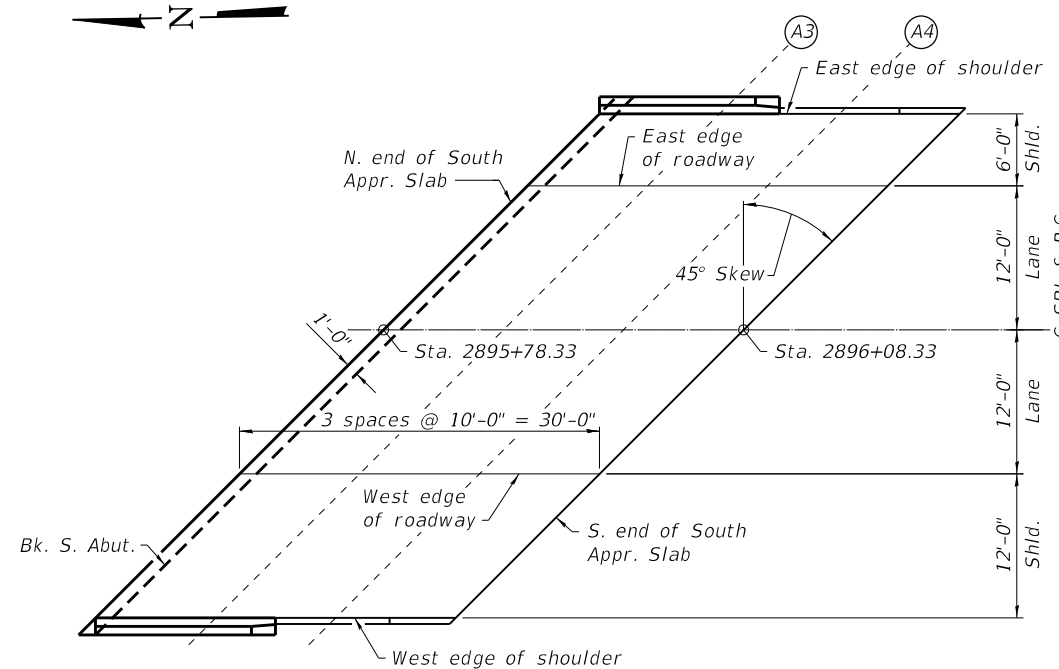
| Location           | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Grinding |
|--------------------|------------|--------------|------------------------------|--|
| N. end of S. Appr. | 2895+66.33 | 12.00        | 405.99                       | 406.01   |
| A3                 | 2895+76.33 | 12.00        | 406.08                       | 406.10   |
| A4                 | 2895+86.33 | 12.00        | 406.17                       | 406.19   |
| S. End of S. Appr. | 2895+96.33 | 12.00        | 406.27                       | 406.29   |

**WEST EDGE OF SHOULDER**

| Location           | Station    | Offset (ft.) | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted for Grinding |
|--------------------|------------|--------------|------------------------------|--|
| N. end of S. Appr. | 2895+54.33 | 24.00        | 405.65                       | 405.67   |
| A3                 | 2895+64.33 | 24.00        | 405.74                       | 405.76   |
| A4                 | 2895+74.33 | 24.00        | 405.82                       | 405.84   |
| S. End of S. Appr. | 2895+84.33 | 24.00        | 405.92                       | 405.94   |



**N. APPROACH SLAB PLAN**



**S. APPROACH SLAB PLAN**

MODEL: PLOT  
FILE NAME: Y:\IDOT\1259-11\_78504\CADD\SP\_SN\_091-0077-0078\0910077-78504-08-TAppSlabElevSB.dgn



USER NAME = kah  
ESCA PROJECT NO. 1259.11  
PLOT SCALE = 0.1667' / in.  
PLOT DATE = 10/15/2019

DESIGNED - KJA 07/19  
CHECKED - RTM/SHL 07/19  
DRAWN - KAH 07/19  
CHECKED - MTD/SHL 08/19

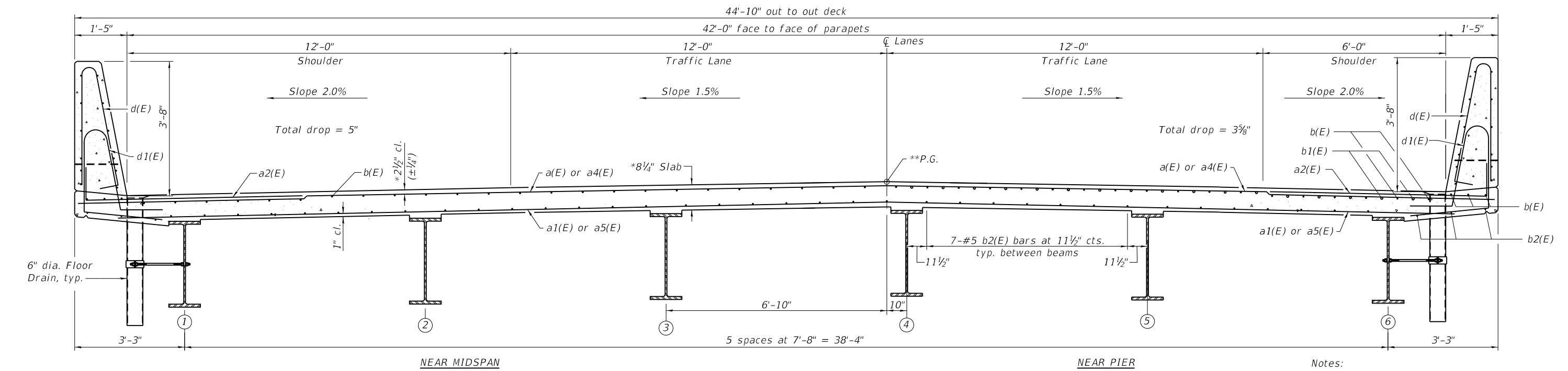
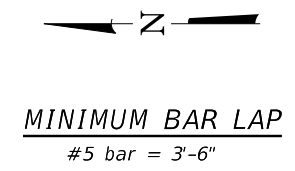
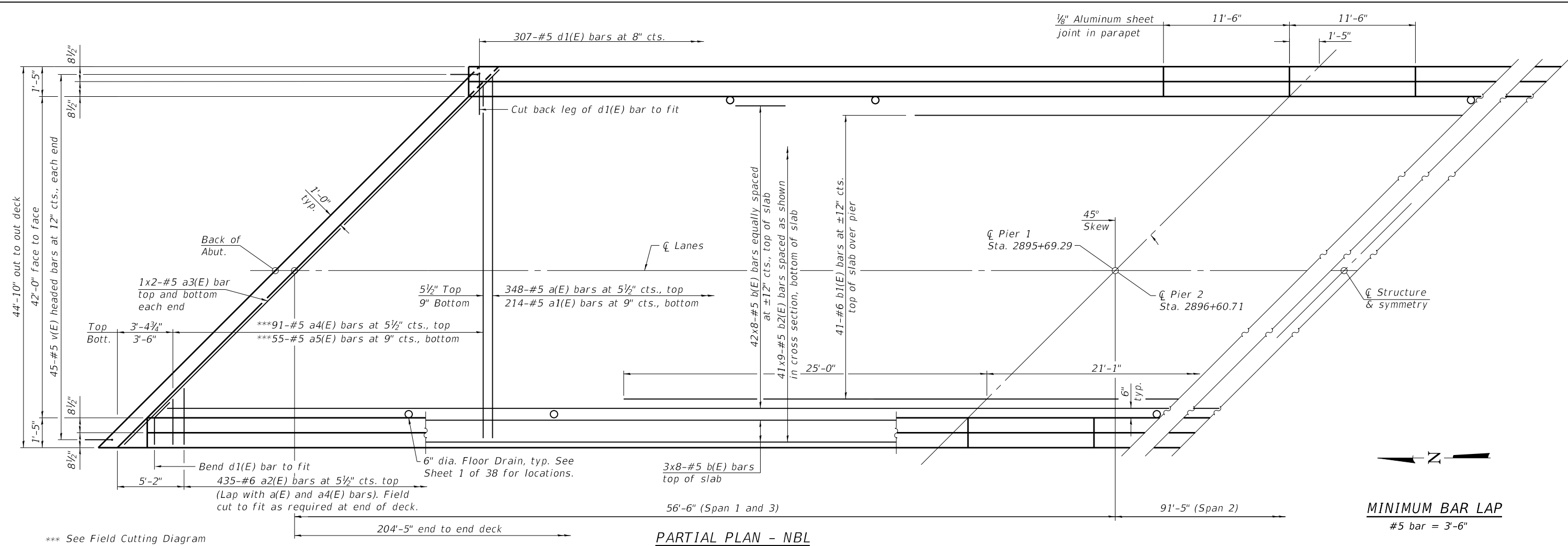
REVISED -  
REVISED -  
REVISED -  
REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TOP OF APPROACH SLAB ELEVATIONS (SB)  
STRUCTURE NO. 091-0078 (SB)**

SHEET 8 OF 38 SHEETS

| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 74        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |



\* Prior to grinding  
\*\* After grinding

Notes:  
See Sheet 10 of 38 for superstructure details and Bill of Material.  
Bars indicated thus 20 x 3-#5 etc. indicates 20 lines of bars with 3 lengths per line.

MODEL: PLOT  
FILE NAME: Y:\IDOT\1259-11\_78504\CADD\SP\_SN\_091-0077-0078\0910077-78504-09-SprNB.dgn



|                            |                         |           |
|----------------------------|-------------------------|-----------|
| USER NAME = kah            | DESIGNED - KJA 07/19    | REVISED - |
| ESCA PROJECT NO. 1259.11   | CHECKED - RTM/SHL 07/19 | REVISED - |
| PLOT SCALE = 0.1667' / in. | DRAWN - KAH 07/19       | REVISED - |
| PLOT DATE = 10/15/2019     | CHECKED - MTD/SHL 08/19 | REVISED - |

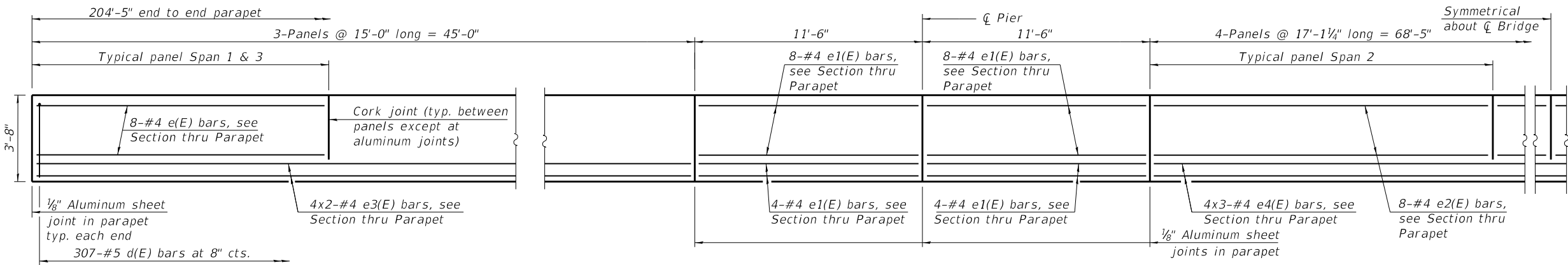
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**SUPERSTRUCTURE (NB)**  
**STRUCTURE NO. 091-0077 (NB)**

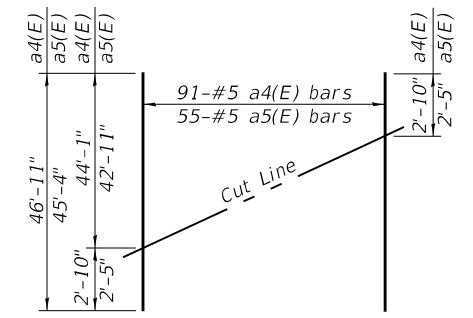
| F.A.I. RTE.        | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|-----------|--------|--------------|-----------|
| 57                 | (91-3)B-2 | UNION  | 161          | 75        |
| CONTRACT NO. 78504 |           |        |              |           |

SHEET 9 OF 38 SHEETS

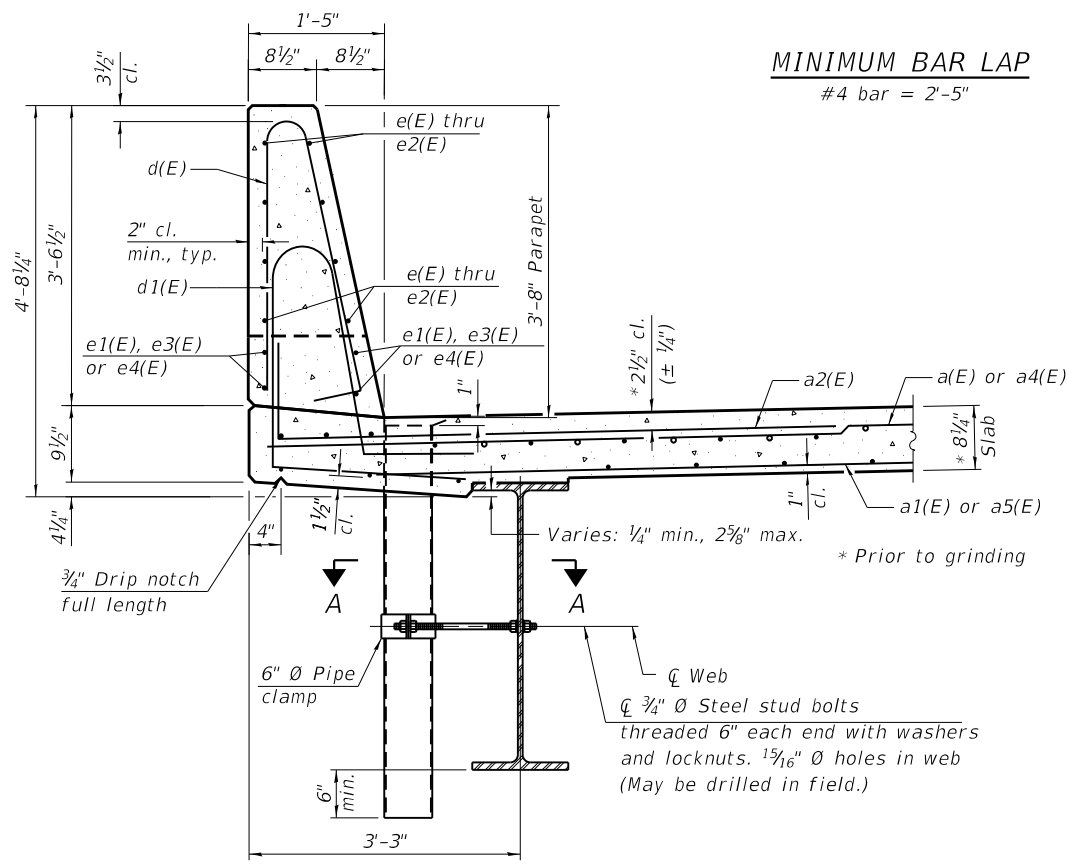
ILLINOIS FED. AID PROJECT



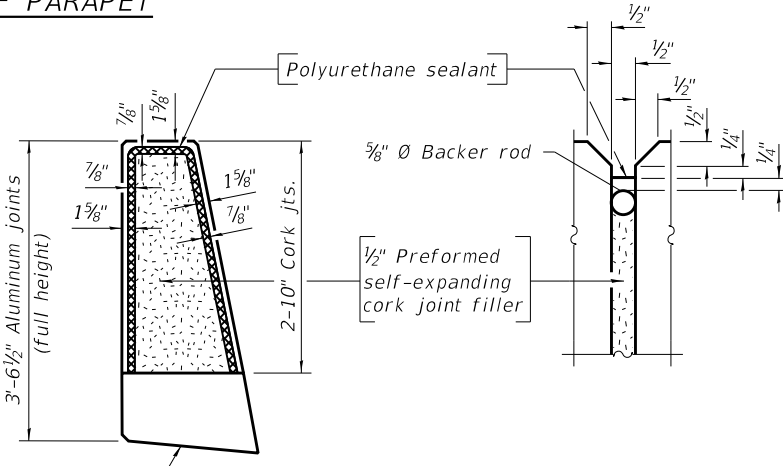
**PARTIAL INSIDE ELEVATION OF PARAPET**



**FIELD CUTTING DIAGRAM**  
Order a4(E) and a5(E) bars full length. Cut as shown and use remainder of bars in opposite end of deck.

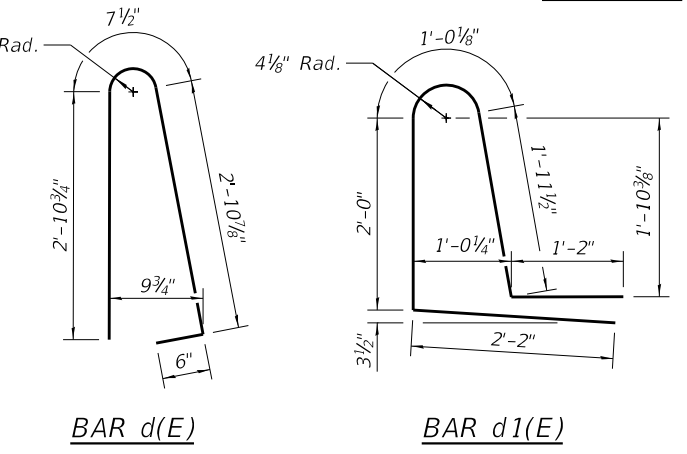
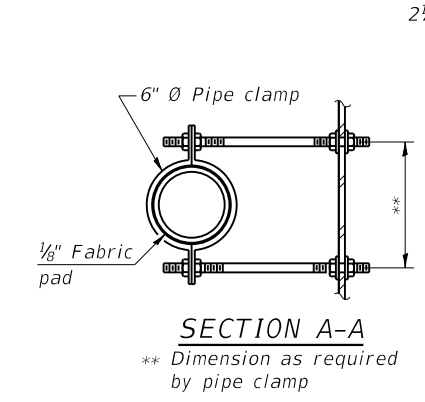
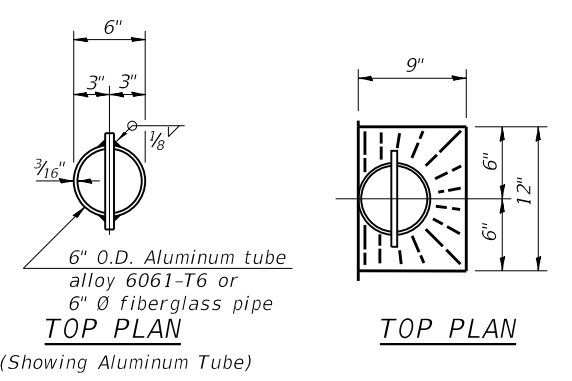
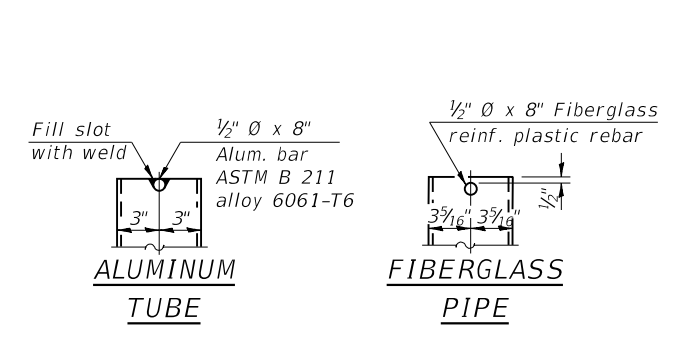


**SECTION THRU PARAPET**



**PARAPET JOINT DETAILS**

**Notes:**  
Fiberglass pipe shall conform to ASTM D2996, with short-time rupture strength hoop tensile stress of 30,000 p.s.i. minimum.  
The exterior surfaces of the floor drains shall be painted according to Article 506 with the finish coat as specified. The exterior surfaces of the drains shall be cleaned according to the Society of Protective Coatings Spec. SSPC-SP1 prior to painting.  
The top portion of aluminum floor drains shall be coated to minimize reaction with wet concrete.  
The clamping device shall be galvanized according to AASHTO M 232. Cost of clamping device included with Floor Drains.  
The 1/8" aluminum sheet shall be ASTM B 209 alloy 3003-H14 and coated to minimize reaction with wet concrete. Cost included with Concrete Superstructure.  
The polyurethane sealant shall be according to Article 1050.04 of the Std. Spec. and the color shall be gray.  
Headed bars shall conform to ASTM A970 with threaded attachment; Class HA; and reinforcement bars conforming to ASTM A706. Cost included with Reinforcement Bars, Epoxy Coated.



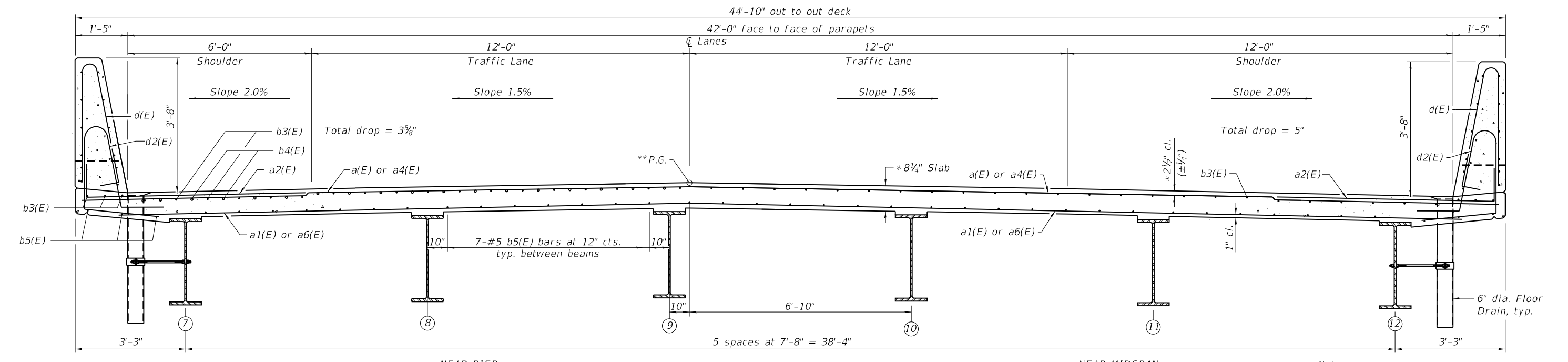
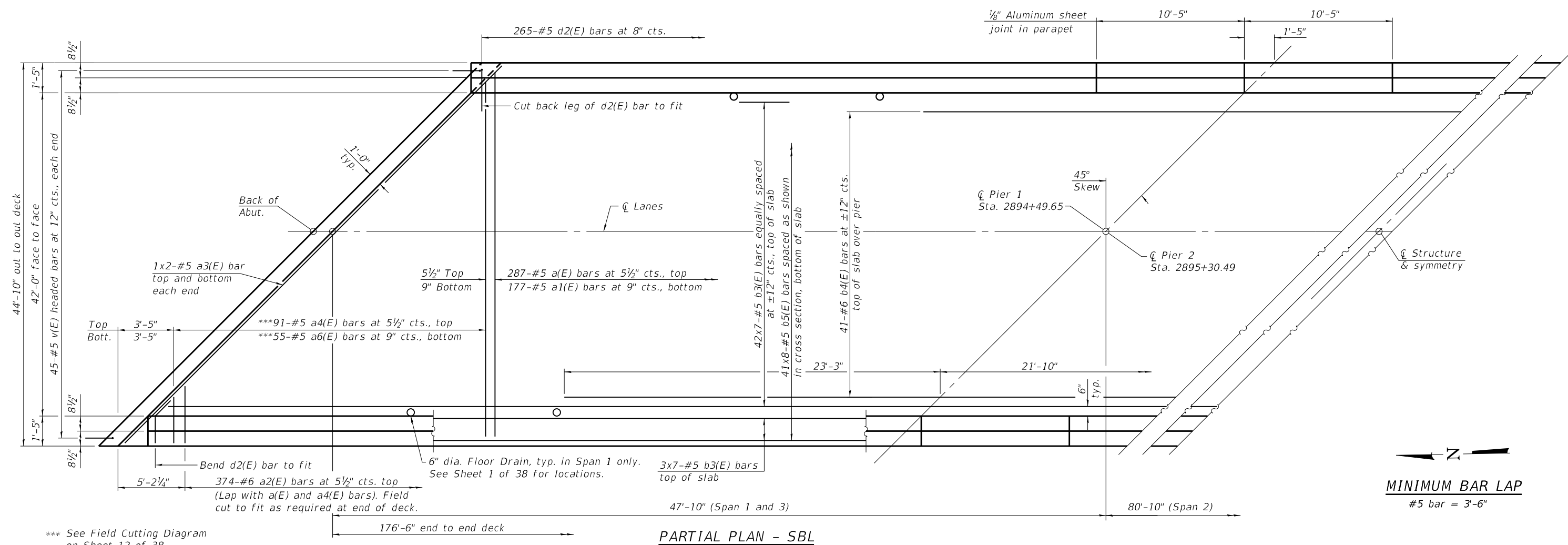
|  |                            |                   |       |           |
|--|----------------------------|-------------------|-------|-----------|
| MODEL: PLOT  | USER NAME = kah            | DESIGNED - KJA    | 07/19 | REVISED - |
| FILE NAME: Y:\IDOT\1259-11_78504\CADD\SP_SN_091-00777-007809100777-78504-1-10-SprDlts\NB.dgn | ESCA PROJECT NO. 1259.11   | CHECKED - RTM/SHL | 07/19 | REVISED - |
|  | PLOT SCALE = 0.1667' / in. | DRAWN - KAH       | 07/19 | REVISED - |
|  | PLOT DATE = 10/15/2019     | CHECKED - MTD/SHL | 08/19 | REVISED - |

|  |   |                             |           |        |              |           |
|--|---|-----------------------------|-----------|--------|--------------|-----------|
| ESCA CONSULTANTS, INC.                                     | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> |                             |           |        |              |           |
| SUPERSTRUCTURE DETAILS (NB)<br>STRUCTURE NO. 091-0077 (NB) |   | F.A.I. RTE.                 | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|  |   | 57                          | (91-3)B-2 | UNION  | 161          | 76        |
|  |   | CONTRACT NO. 78504          |           |        |              |           |
|  |   | ILLINOIS / FED. AID PROJECT |           |        |              |           |

**SUPERSTRUCTURE BILL OF MATERIAL**

| Bar                              | No. | Size | Length  | Shape    |        |
|----------------------------------|-----|------|---------|----------|--------|
| a(E)                             | 348 | #5   | 44'-6"  | —        |        |
| a1(E)                            | 214 | #5   | 43'-6"  | —        |        |
| a2(E)                            | 870 | #6   | 8'-4"   | —        |        |
| a3(E)                            | 8   | #5   | 33'-4"  | —        |        |
| a4(E)                            | 91  | #5   | 46'-11" | —        |        |
| a5(E)                            | 55  | #5   | 45'-4"  | —        |        |
| b(E)                             | 384 | #5   | 28'-7"  | —        |        |
| b1(E)                            | 82  | #6   | 46'-1"  | —        |        |
| b2(E)                            | 369 | #5   | 25'-10" | —        |        |
| d(E)                             | 614 | #5   | 7'-0"   | —        |        |
| d1(E)                            | 614 | #5   | 8'-4"   | —        |        |
| e(E)                             | 96  | #4   | 14'-8"  | —        |        |
| e1(E)                            | 96  | #4   | 11'-2"  | —        |        |
| e2(E)                            | 64  | #4   | 16'-9"  | —        |        |
| e3(E)                            | 32  | #4   | 23'-7"  | —        |        |
| e4(E)                            | 24  | #4   | 24'-4"  | —        |        |
| m(E)                             | 16  | #6   | 33'-6"  | —        |        |
| m1(E)                            | 30  | #6   | 10'-2"  | —        |        |
| m2(E)                            | 12  | #6   | 4'-0"   | —        |        |
| s(E)                             | 82  | #5   | 7'-0"   | —        |        |
| s1(E)                            | 82  | #5   | 10'-4"  | —        |        |
| v(E)                             | 90  | #5   | 3'-1"   | —        |        |
| Reinforcement Bars, Epoxy Coated |     |      |         | Lbs.     | 87,340 |
| Concrete Superstructure          |     |      |         | Cu. Yds. | 352.6  |

Bars indicated thus 1 x 2-#4 etc. indicates 1 line of bars with 2 lengths per line.



Notes:  
 See Sheet 12 of 38 for superstructure details and Bill of Material.  
 Bars indicated thus 20 x 3-#5 etc. indicates 20 lines of bars with 3 lengths per line.

MODEL: PLOT  
 FILE NAME: Y:\IDOT\1259-11\_78504\CADD\SP\_SN\_091-0077-0078\0910077-78504-11-5spr5B.dgn



|                            |                         |           |
|----------------------------|-------------------------|-----------|
| USER NAME = kah            | DESIGNED - KJA 07/19    | REVISED - |
| ESCA PROJECT NO. 1259.11   | CHECKED - RTM/SHL 07/19 | REVISED - |
| PLOT SCALE = 0.1667' / in. | DRAWN - KAH 07/19       | REVISED - |
| PLOT DATE = 10/15/2019     | CHECKED - MTD/SHL 08/19 | REVISED - |

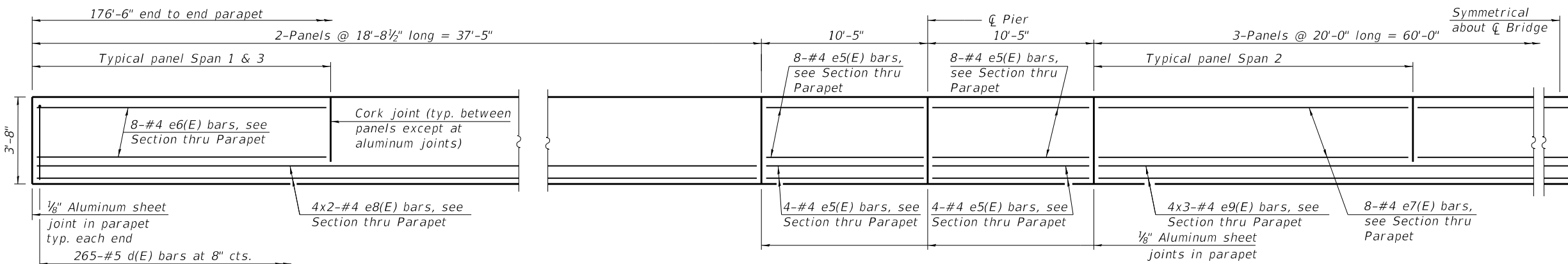
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**SUPERSTRUCTURE (SB)**  
**STRUCTURE NO. 091-0078 (SB)**

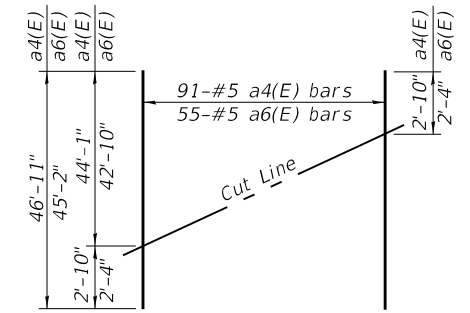
| F.A.I. RTE.        | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|-----------|--------|--------------|-----------|
| 57                 | (91-3)B-2 | UNION  | 161          | 77        |
| CONTRACT NO. 78504 |           |        |              |           |

SHEET 11 OF 38 SHEETS

ILLINOIS FED. AID PROJECT

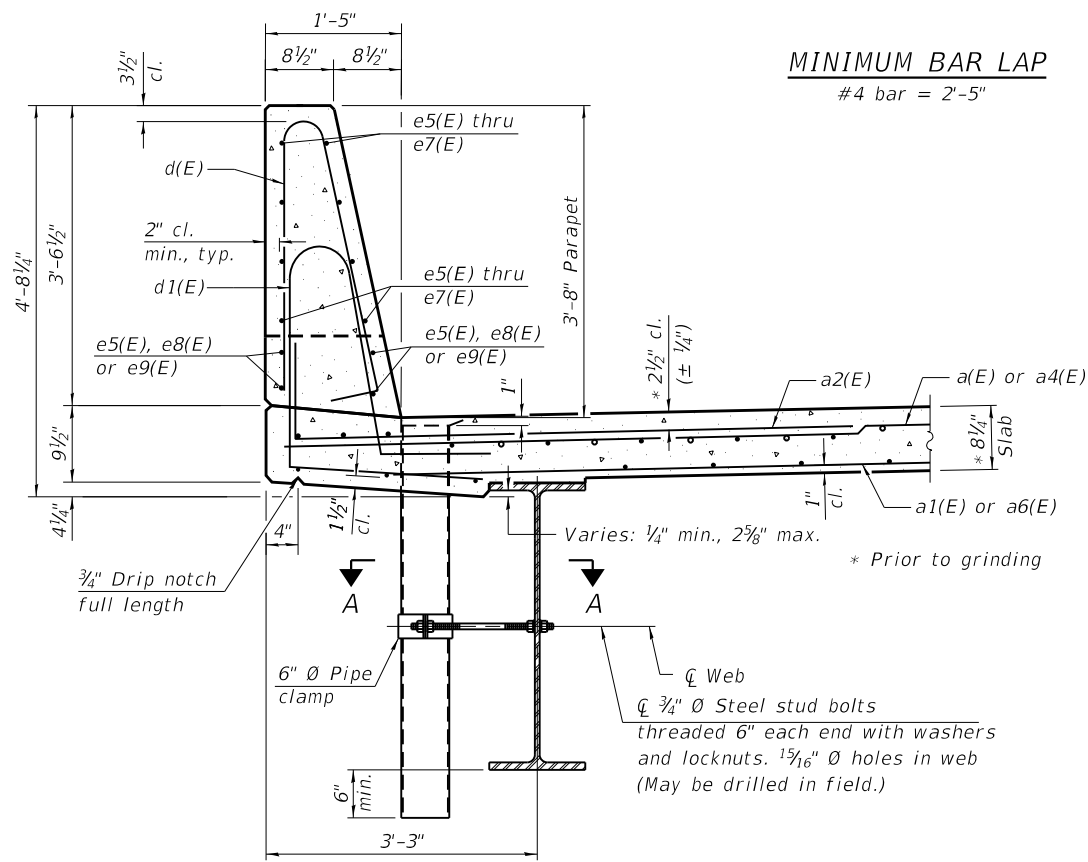


PARTIAL INSIDE ELEVATION OF PARAPET

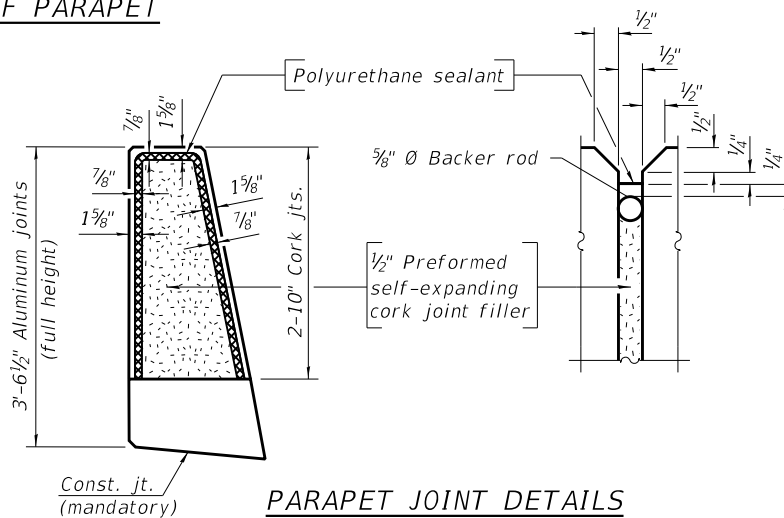


FIELD CUTTING DIAGRAM

Order a4(E) and a6(E) bars full length. Cut as shown and use remainder of bars in opposite end of deck.



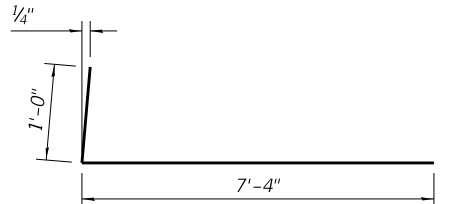
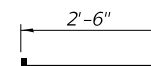
SECTION THRU PARAPET



PARAPET JOINT DETAILS

Notes:

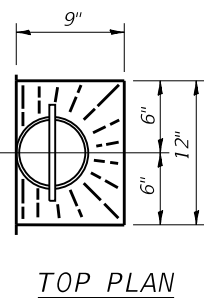
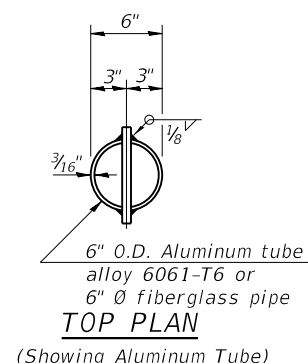
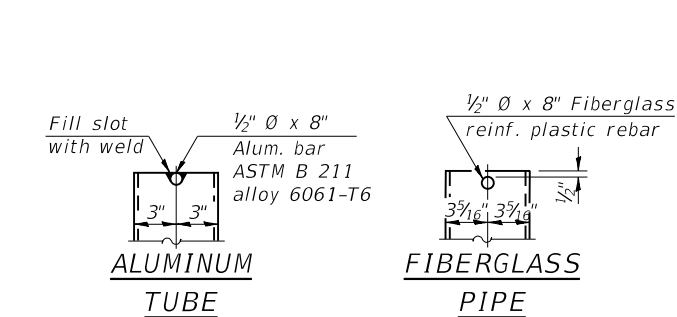
- Fiberglass pipe shall conform to ASTM D2996, with short-time rupture strength hoop tensile stress of 30,000 p.s.i. minimum.
- The exterior surfaces of the floor drains shall be painted according to Article 506 with the finish coat as specified. The exterior surfaces of the drains shall be cleaned according to the Society of Protective Coatings Spec. SSPC-SP1 prior to painting.
- The top portion of aluminum floor drains shall be coated to minimize reaction with wet concrete.
- The clamping device shall be galvanized according to AASHTO M 232. Cost of clamping device included with Floor Drains.
- The 1/8" aluminum sheet shall be ASTM B 209 alloy 3003-H14 and coated to minimize reaction with wet concrete. Cost included with Concrete Superstructure.
- The polyurethane sealant shall be according to Article 1050.04 of the Std. Spec. and the color shall be gray.
- Headed bars shall conform to ASTM A970 with threaded attachment; Class HA; and reinforcement bars conforming to ASTM A706. Cost included with Reinforcement Bars, Epoxy Coated.



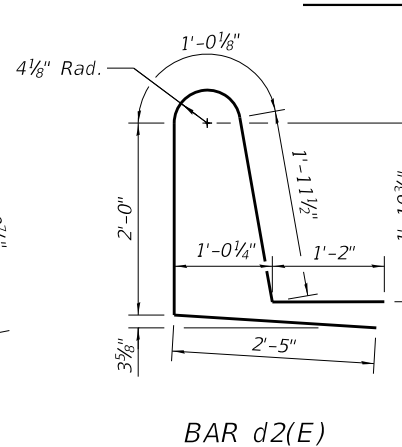
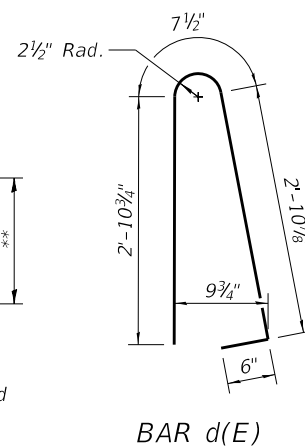
SUPERSTRUCTURE BILL OF MATERIAL

| Bar                              | No. | Size     | Length  | Shape  |
|----------------------------------|-----|----------|---------|--------|
| a(E)                             | 287 | #5       | 44'-6"  | —      |
| a1(E)                            | 177 | #5       | 43'-6"  | —      |
| a2(E)                            | 748 | #6       | 8'-4"   | —      |
| a3(E)                            | 8   | #5       | 33'-4"  | —      |
| a4(E)                            | 91  | #5       | 46'-11" | —      |
| a6(E)                            | 55  | #5       | 45'-2"  | —      |
| b3(E)                            | 336 | #5       | 28'-2"  | —      |
| b4(E)                            | 82  | #6       | 45'-1"  | —      |
| b5(E)                            | 328 | #5       | 25'-1"  | —      |
| d(E)                             | 530 | #5       | 7'-0"   | —      |
| d2(E)                            | 530 | #5       | 8'-7"   | —      |
| e5(E)                            | 96  | #4       | 10'-1"  | —      |
| e6(E)                            | 64  | #4       | 18'-4"  | —      |
| e7(E)                            | 48  | #4       | 19'-8"  | —      |
| e8(E)                            | 32  | #4       | 19'-9"  | —      |
| e9(E)                            | 24  | #4       | 21'-6"  | —      |
| m(E)                             | 16  | #6       | 33'-6"  | —      |
| m1(E)                            | 30  | #6       | 10'-2"  | —      |
| m2(E)                            | 12  | #6       | 4'-0"   | —      |
| s(E)                             | 82  | #5       | 7'-0"   | —      |
| s1(E)                            | 82  | #5       | 10'-4"  | —      |
| v(E)                             | 90  | #5       | 3'-1"   | —      |
| Reinforcement Bars, Epoxy Coated |     | Lbs.     |         | 76,590 |
| Concrete Superstructure          |     | Cu. Yds. |         | 308.9  |

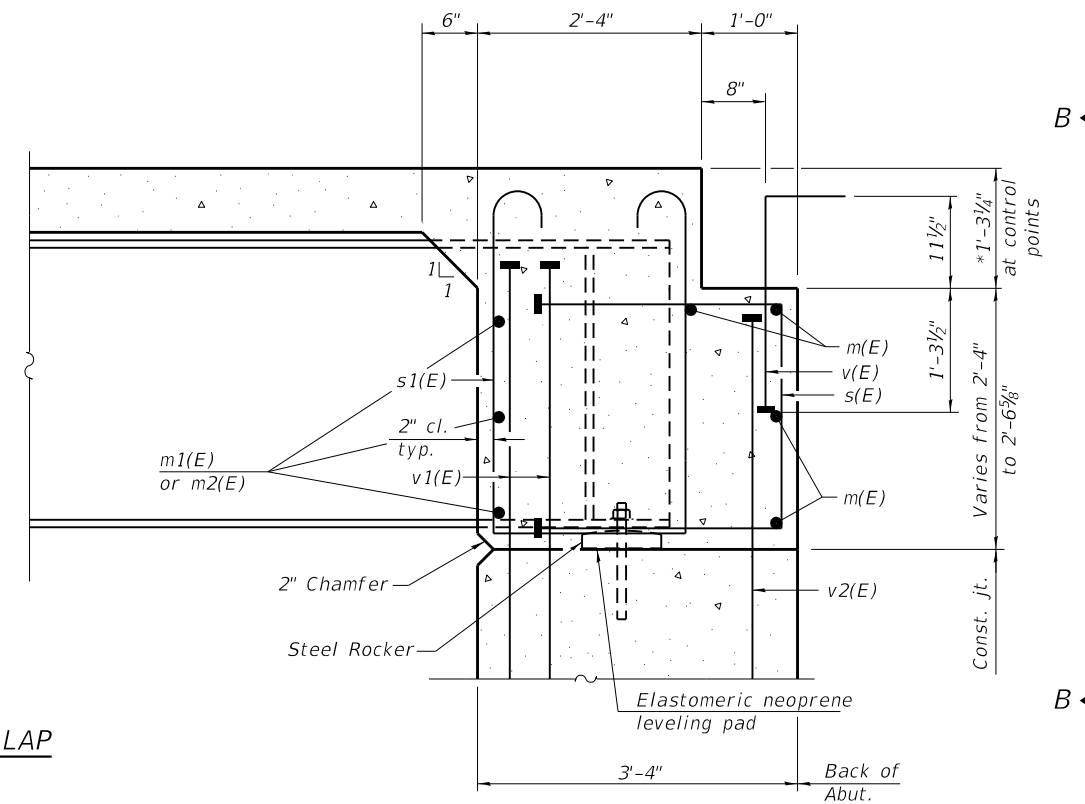
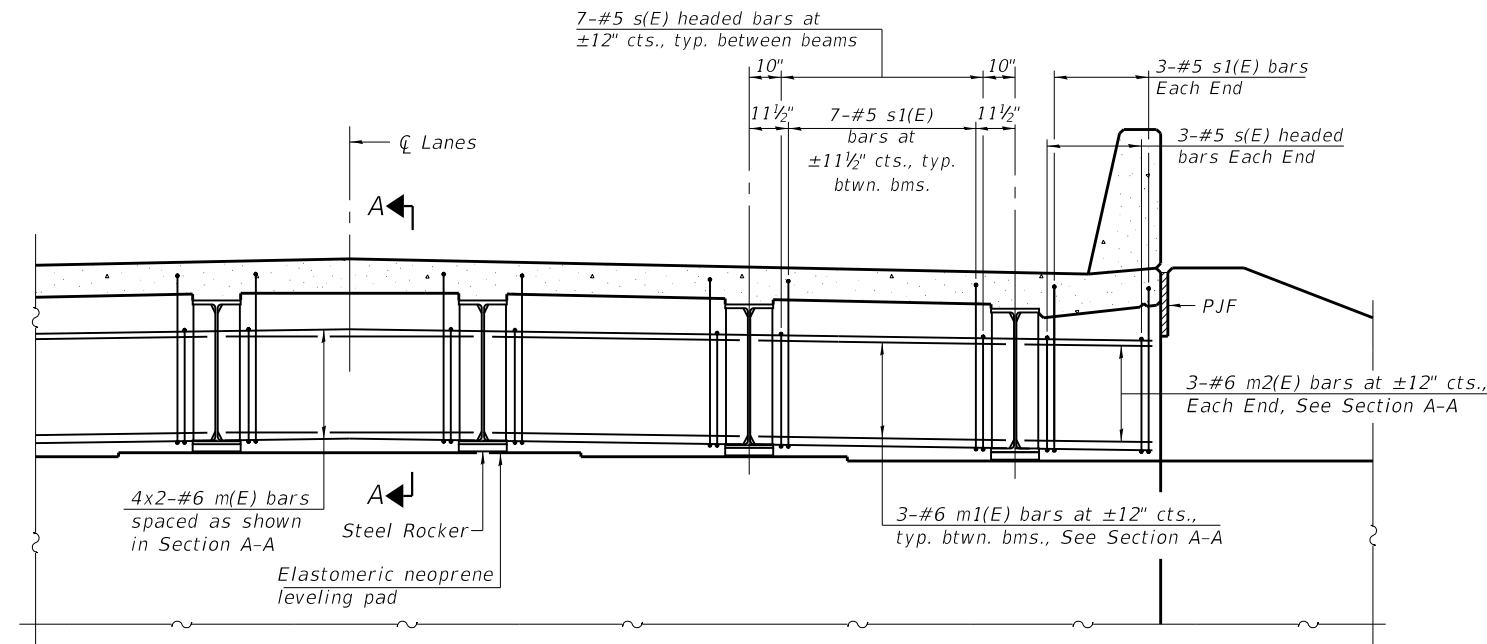
Bars indicated thus 1 x 2-#4 etc. indicates 1 line of bars with 2 lengths per line.



SECTION A-A  
\*\* Dimension as required by pipe clamp



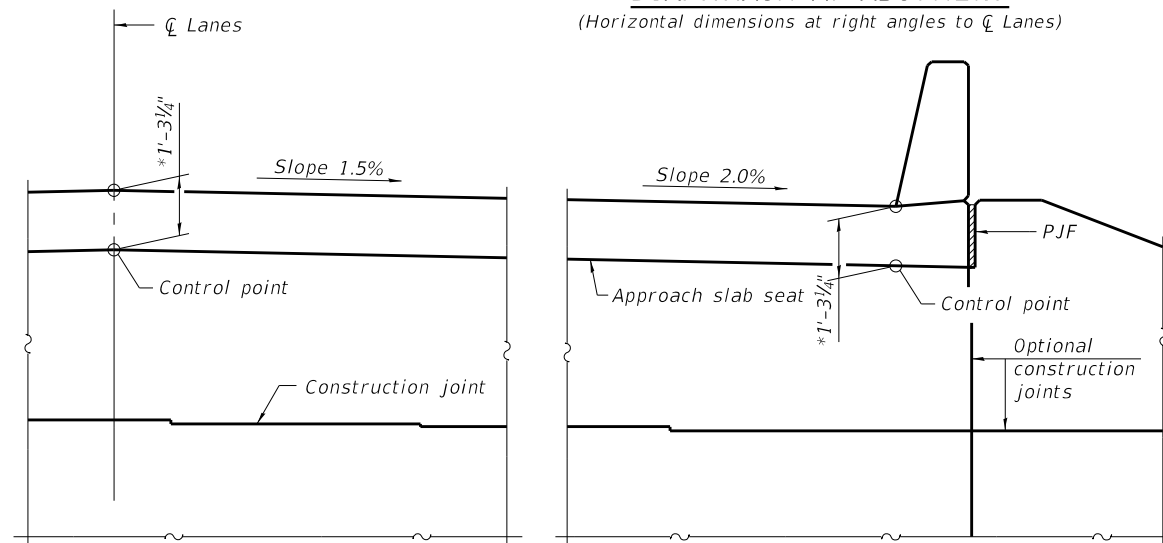




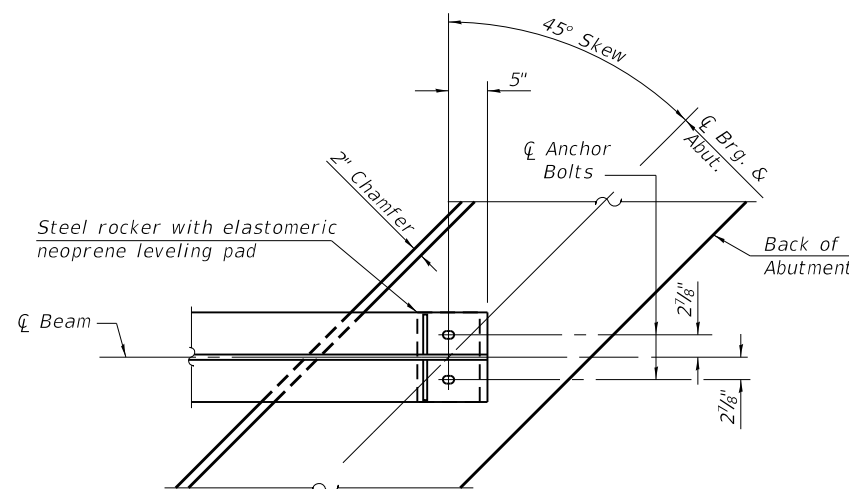
**DIAPHRAGM AT ABUTMENT**  
(Horizontal dimensions at right angles to  $\bar{C}$  Lanes)

**MINIMUM BAR LAP**  
#6 bar = 4'-0"

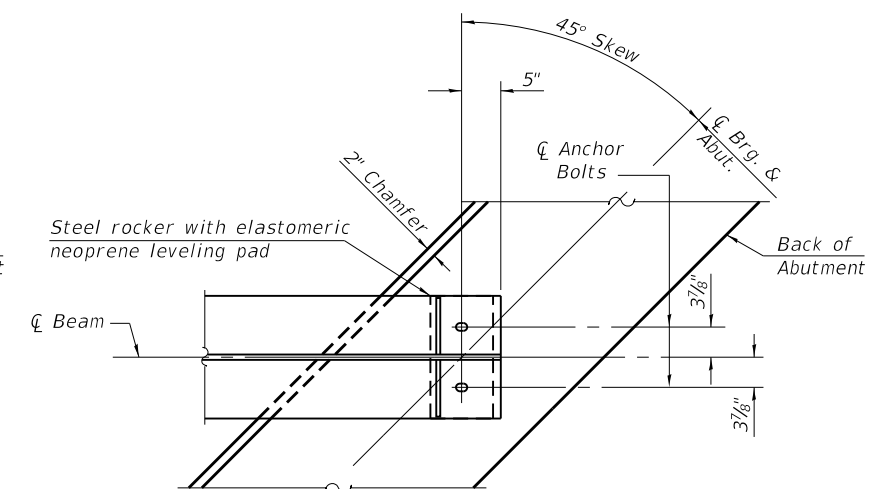
**SECTION A-A**  
(at Rt. L's)



**VIEW B-B**



**PLAN AT SOUTHBOUND ABUTMENTS**  
(Showing bottom flange of beam)



**PLAN AT NORTHBOUND ABUTMENTS**  
(Showing bottom flange of beam)

Notes:  
 Reinforcement bars in diaphragm are billed with superstructure on Sheets 10 and 12 of 38.  
 Concrete in diaphragm is included with Concrete Superstructure on Sheets 10 and 12 of 38.  
 For details of bars s(E), s1(E), and v(E) see Sheets 10 and 12 of 38.  
 The s(E) and s1(E) bars shall be placed parallel to the beams.  
 Spacing for these bars shall be at right angles to the beams.  
 The approach slab seat shall have a constant slope determined from the control points shown.  
 For bearing details see Sheet 20 of 38.  
 Beams shall be braced for stability during erection and remain braced until deck is poured and cured.  
 The v1(E) and v2(E) bars are included with the abutment drawings on Sheets 21 thru 24 of 38.  
 See Sheet 14 of 38 for P.J.F. details.

MODEL: PLOT  
 FILE NAME: Y:\IDOT\1259-11\_78504\CADD\SP\_SN\_091-0077-0078\0910077-78504-13-Diaphragm.dwg



USER NAME = kah  
 ESCA PROJECT NO. 1259.11  
 PLOT SCALE = 0:2 " = 1" / in.  
 PLOT DATE = 10/15/2019

DESIGNED - KJA 07/19  
 CHECKED - RTM/SHL 07/19  
 DRAWN - KAH 07/19  
 CHECKED - MTD/SHL 08/19

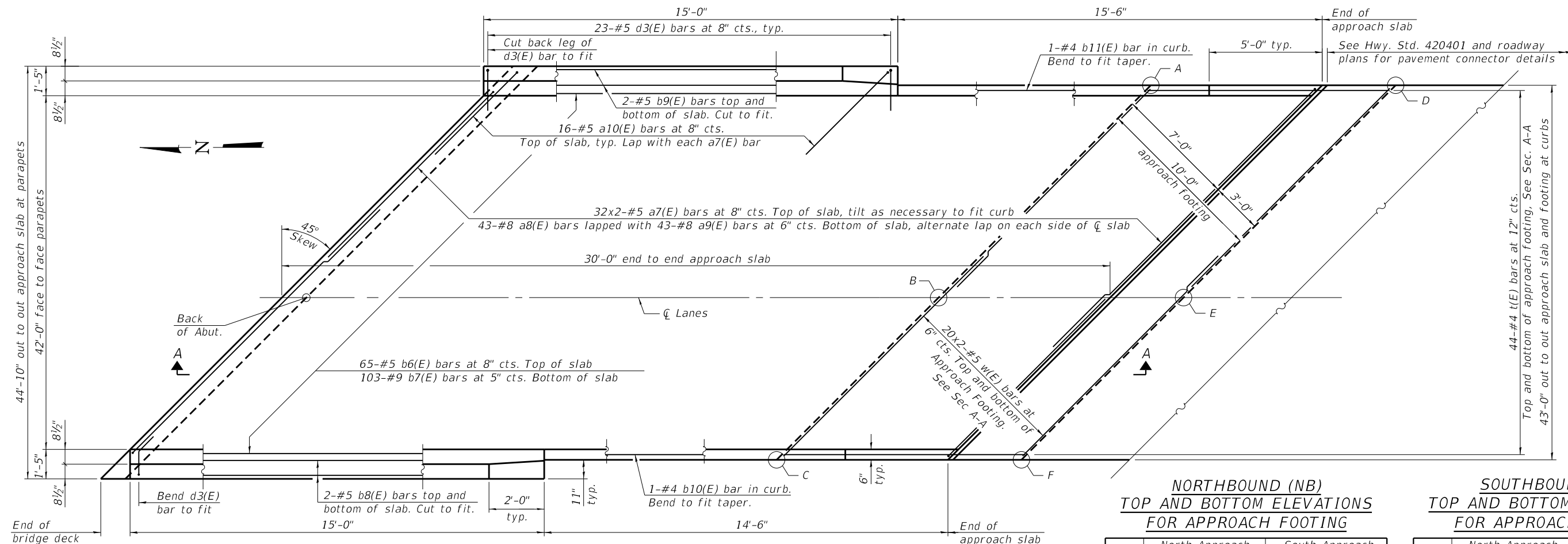
REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**DIAPHRAGM DETAILS**  
**STRUCTURE NO. 091-0077 (NB) & 091-0078 (SB)**

SHEET 13 OF 38 SHEETS

| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 79        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |



Notes:  
 See Sheet 15 of 38 for Section A-A, Bill of Material and additional details.  
 Bars indicated thus 20 x 2-#5 etc. indicates 20 lines of bars with 2 lengths per line.

**PLAN**  
 (Showing NB South Approach; other approaches similar)

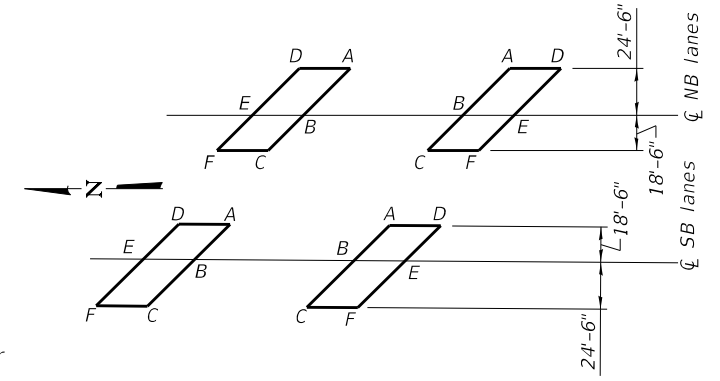
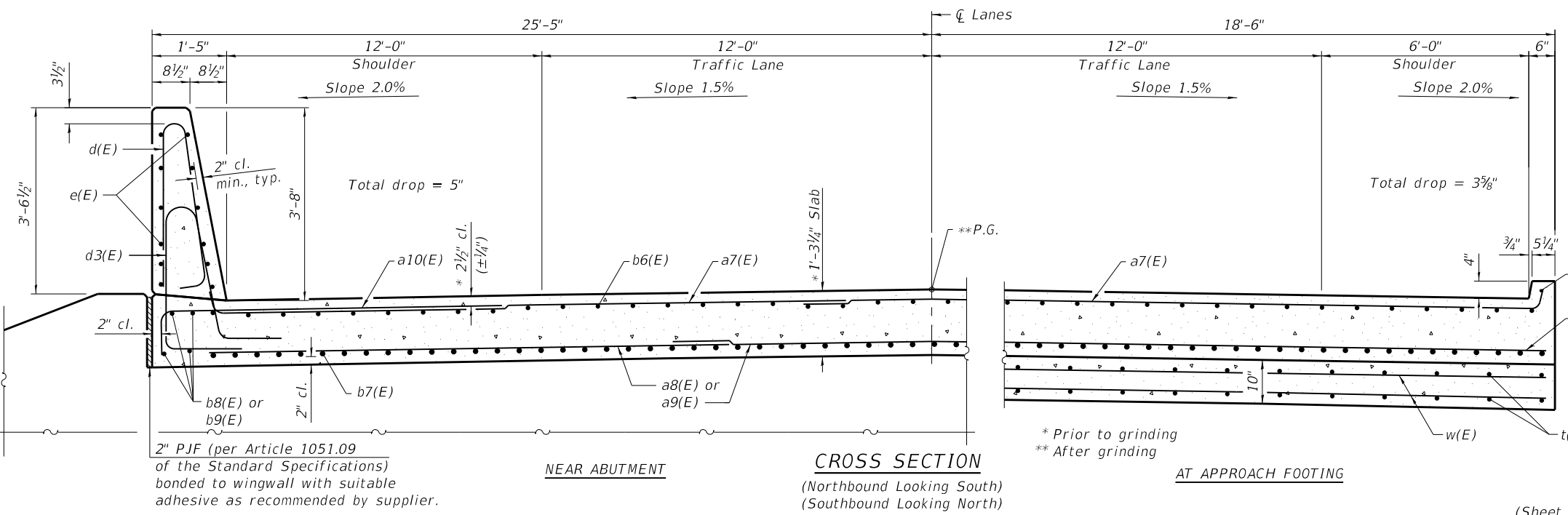
**MINIMUM BAR LAP**  
 #5 bars = 3'-6"  
 #8 bars = 6'-9"

**NORTHBOUND (NB)  
 TOP AND BOTTOM ELEVATIONS  
 FOR APPROACH FOOTING**

| Point | North Approach |        | South Approach |        |
|-------|----------------|--------|----------------|--------|
|       | Top            | Bottom | Top            | Bottom |
| A     | 408.50         | 407.67 | 409.51         | 408.68 |
| B     | 408.87         | 408.04 | 409.80         | 408.97 |
| C     | 408.53         | 407.70 | 409.39         | 408.56 |
| D     | 408.47         | 407.64 | 409.60         | 408.77 |
| E     | 408.85         | 408.02 | 409.88         | 409.05 |
| F     | 408.51         | 407.68 | 409.46         | 408.63 |

**SOUTHBOUND (SB)  
 TOP AND BOTTOM ELEVATIONS  
 FOR APPROACH FOOTING**

| Point | North Approach |        | South Approach |        |
|-------|----------------|--------|----------------|--------|
|       | Top            | Bottom | Top            | Bottom |
| A     | 403.63         | 402.80 | 405.10         | 404.27 |
| B     | 403.89         | 403.06 | 405.22         | 404.39 |
| C     | 403.40         | 402.57 | 404.56         | 403.73 |
| D     | 403.59         | 402.76 | 405.25         | 404.42 |
| E     | 403.85         | 403.02 | 405.36         | 404.53 |
| F     | 403.38         | 402.55 | 404.69         | 403.86 |



**NORTH APPROACH FOOTINGS**  
**SOUTH APPROACH FOOTINGS**  
**LEGEND FOR APPROACH FOOTING ELEVATIONS**

(Sheet 1 of 2)

MODEL: PLOT  
 FILE NAME: Y:\IDOT\1259-11\_78504\CADD\SP\_SN\_091-0077-0078\0910077-78504-1-4-Apr\Dis.dgn



|                            |                         |           |
|----------------------------|-------------------------|-----------|
| USER NAME = kah            | DESIGNED - KJA 07/19    | REVISED - |
| ESCA PROJECT NO. 1259.11   | CHECKED - RTM/SHL 07/19 | REVISED - |
| PLOT SCALE = 0.1667' / in. | DRAWN - KAH 07/19       | REVISED - |
| PLOT DATE = 10/15/2019     | CHECKED - MTD/SHL 08/19 | REVISED - |

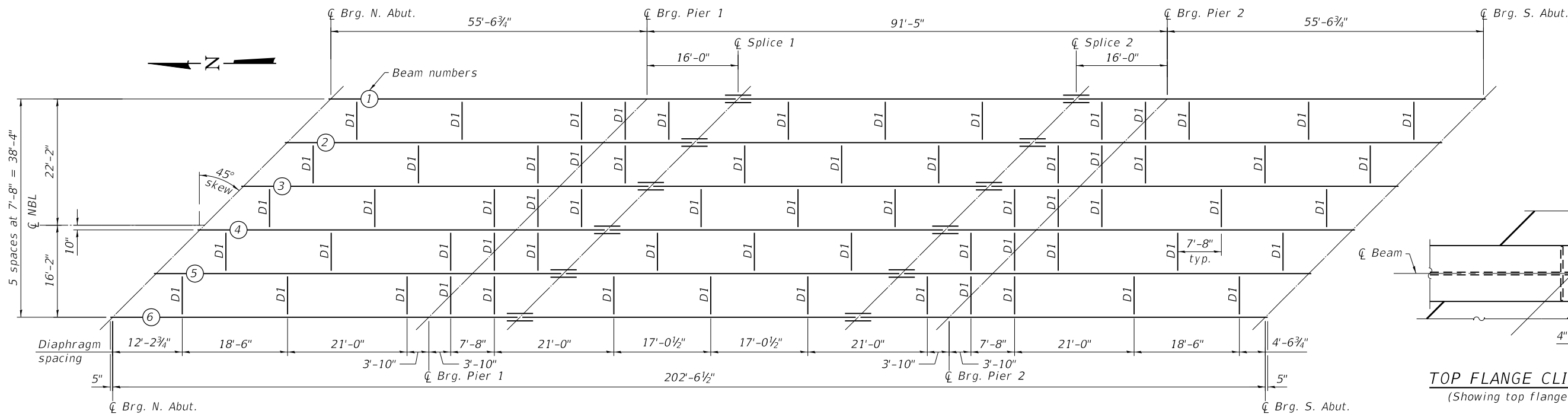
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**BRIDGE APPROACH SLAB DETAILS  
 STRUCTURE NO. 091-0077 (NB) & 091-0078 (SB)**

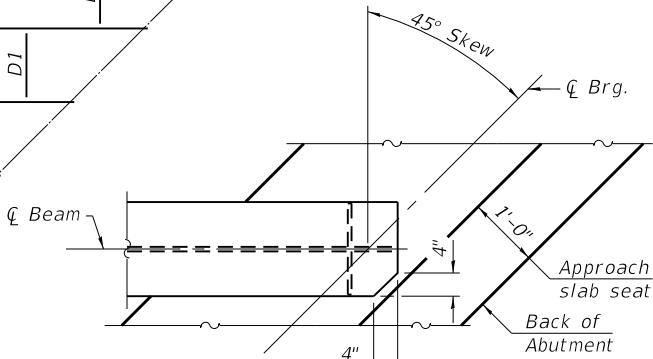
|                           |           |        |              |           |
|---------------------------|-----------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
| 57                        | (91-3)B-2 | UNION  | 161          | 80        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |

SHEET 14 OF 38 SHEETS

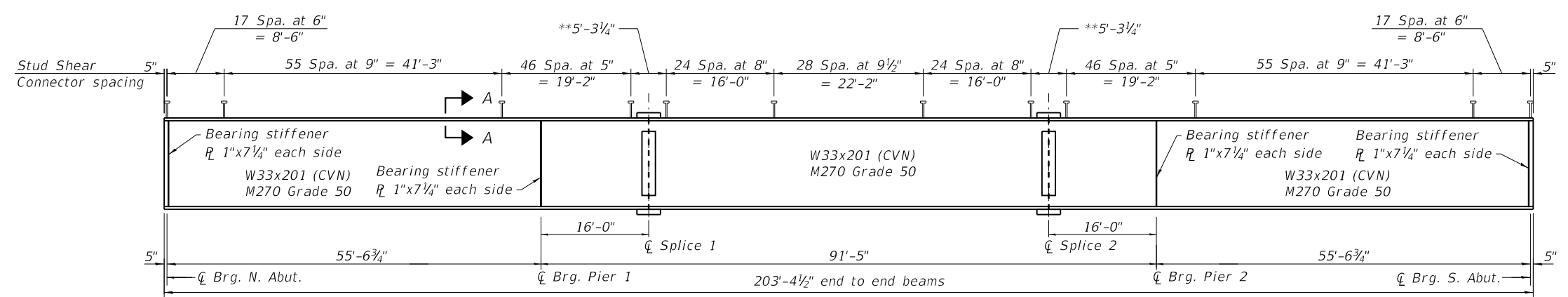




STEEL FRAMING PLAN - NBL

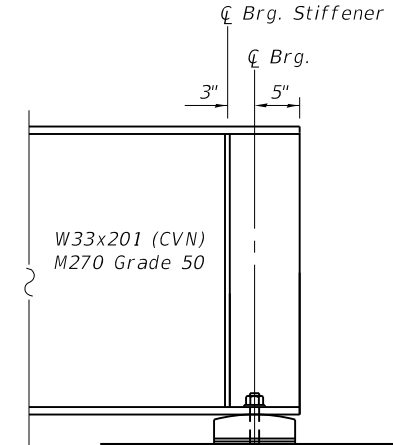


TOP FLANGE CLIP DETAIL  
(Showing top flange of beam)



BEAM ELEVATION

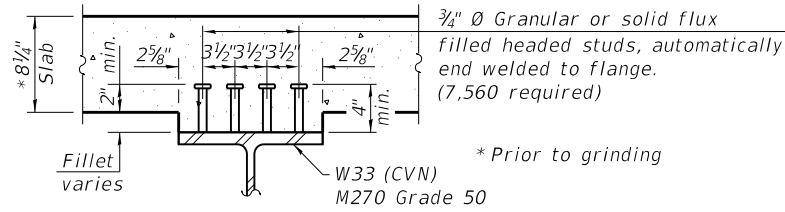
\*\* Omit shear connectors over splices



END OF BEAM DETAIL  
(Showing bearing stiffener location)

TOP OF BEAM ELEVATIONS  
(For fabrication only)

| Beam | ¢ Brg. N. Abut. | ¢ Brg. Pier 1 | ¢ Splice 1 | ¢ Splice 2 | ¢ Brg. Pier 2 | ¢ Brg. S. Abut. |
|------|-----------------|---------------|------------|------------|---------------|-----------------|
| 1    | 409.11          | 409.24        | 409.28     | 409.52     | 409.61        | 409.94          |
| 2    | 409.25          | 409.37        | 409.40     | 409.64     | 409.73        | 410.05          |
| 3    | 409.36          | 409.47        | 409.50     | 409.73     | 409.82        | 410.14          |
| 4    | 409.43          | 409.54        | 409.57     | 409.79     | 409.88        | 410.19          |
| 5    | 409.30          | 409.40        | 409.43     | 409.64     | 409.73        | 410.03          |
| 6    | 409.15          | 409.24        | 409.27     | 409.48     | 409.56        | 409.86          |



SECTION A-A

Notes:  
See Sheet 17 of 38 for additional steel details.  
All diaphragms shall be installed as steel is erected and secured with erection pins and bolts except as otherwise noted. Individual diaphragms at supports may be temporarily disconnected to install bearing anchors rods.  
Load carrying components designated "CVN" denotes Charpy-V-Notch Impact Energy Requirements, Zone 2.

MODEL: PLOT FILE NAME: Y:\IDOT\1259-11\_78504\CADD\SP\_SN\_091-0077-0078\0910077-78504-16-SIF\mPlnDlslsNB.dgn



USER NAME = kah  
ESCA PROJECT NO. 1259.11  
PLOT SCALE = 0:2" = 1' / in.  
PLOT DATE = 10/15/2019

DESIGNED - KJA 07/19  
CHECKED - RTM/SHL 07/19  
DRAWN - KAH 07/19  
CHECKED - MTD/SHL 08/19

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

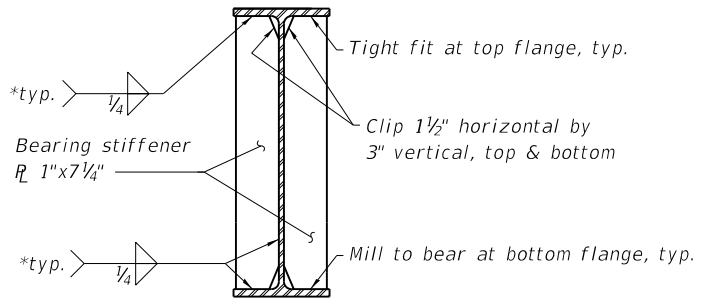
STEEL FRAMING PLAN & DETAILS (NB)  
STRUCTURE NO. 091-0077 (NB)

SHEET 16 OF 38 SHEETS

| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 82        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |

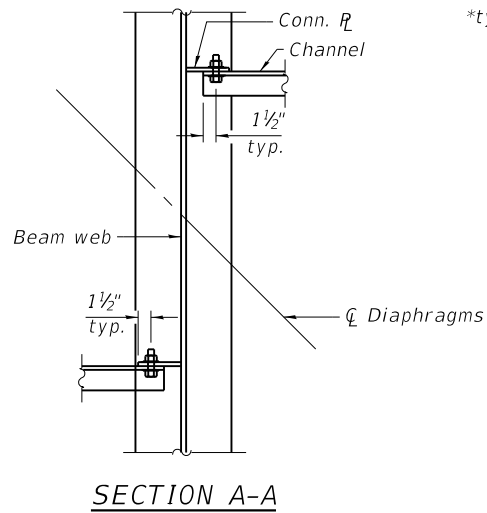
|                    | BEAM REACTION TABLE |          |          |          |
|--------------------|---------------------|----------|----------|----------|
|                    | Abutment            |          | Pier     |          |
|                    | Interior            | Exterior | Interior | Exterior |
| LLDF               | 0.7911              | 0.7043   | 0.7911   | 0.7043   |
| OCF                | -                   | 1.2      | -        | -        |
| R <sub>DC1</sub>   | (k) 17.9            | 17.6     | 92.0     | 91.0     |
| R <sub>DC2</sub>   | (k) 3.2             | 3.2      | 16.0     | 16.0     |
| R <sub>DW</sub>    | (k) 6.5             | 4.8      | 32.4     | 23.9     |
| R <sub>ℓ</sub>     | (k) 59.3            | 63.4     | 103.2    | 91.3     |
| R <sub>IM</sub>    | (k) 15.3            | 16.3     | 21.0     | 18.5     |
| R <sub>Total</sub> | (k) 102.2           | 105.3    | 264.6    | 240.7    |

|   | INTERIOR BEAM MOMENT TABLE |                          |            |
|---|----------------------------|--------------------------|------------|
|   | 0.4 Sp. 1 or 0.6 Sp. 3     | Pier                     | 0.5 Span 2 |
|   | Is                         | (in <sup>4</sup> ) 11600 | 11600      |
| Ic(n)   | (in <sup>4</sup> ) 28493   | 28493                    | 28493      |
| Ic(3n)  | (in <sup>4</sup> ) 20967   | 20967                    | 20967      |
| Ic(cr)  | (in <sup>4</sup> ) -       | 14752                    | -          |
| Ss  | (in <sup>3</sup> ) 686.0   | 686.0                    | 686.0      |
| Sc(n)   | (in <sup>3</sup> ) 954.6   | 954.6                    | 954.6      |
| Sc(3n)  | (in <sup>3</sup> ) 868.4   | 868.4                    | 868.4      |
| Sc(cr)  | (in <sup>3</sup> ) -       | 762.1                    | -          |
| DC1   | (k/ft) 1.087               | 1.087                    | 1.087      |
| MDC1  | (k) 132.5                  | 660.6                    | 476.3      |
| DC2   | (k/ft) 0.190               | 0.190                    | 0.190      |
| MDC2  | (k) 24.3                   | 115.2                    | 83.3       |
| DW  | (k/ft) 0.384               | 0.384                    | 0.384      |
| MDW   | (k) 49.1                   | 232.8                    | 168.3      |
| LLDF  | 0.6785                     | 0.6457                   | 0.6213     |
| M <sub>ℓ + IM</sub>                                     | (k) 692.7                  | 880.1                    | 900.3      |
| f <sub>ℓ</sub> (Strength I)                             | (ksi) 0                    | 0                        | 0          |
| M <sub>u</sub> + 1/2 f <sub>ℓ</sub> S <sub>xc</sub>     | (k) 1482.0                 | 2859.1                   | 2527.4     |
| ∅f Mn   | (k) 4800.4                 | -                        | 4800.4     |
| f <sub>s</sub> DC1                                      | (ksi) 2.32                 | 11.56                    | 8.33       |
| f <sub>s</sub> DC2                                      | (ksi) 0.34                 | 1.81                     | 1.15       |
| f <sub>s</sub> DW                                       | (ksi) 0.68                 | 3.67                     | 2.33       |
| f <sub>s</sub> (ℓ+IM)                                   | (ksi) 8.71                 | 13.86                    | 11.32      |
| f <sub>ℓ</sub> (Service II)                             | (ksi) 0                    | 0                        | 0          |
| f <sub>s</sub> + 1/2 f <sub>ℓ</sub> (Service II)        | (ksi) 14.66                | 35.06                    | 26.53      |
| 0.95R <sub>h</sub> F <sub>yf</sub>                      | (ksi) 47.50                | 47.50                    | 47.50      |
| f <sub>s</sub> + 1/2 f <sub>ℓ</sub> (Total)(Strength I) | (ksi) -                    | 46.46                    | -          |
| ∅f F <sub>n</sub>                                       | (ksi) -                    | 50.00                    | -          |
| Vf  | (k) 62.8                   | 69.6                     | 57.5       |

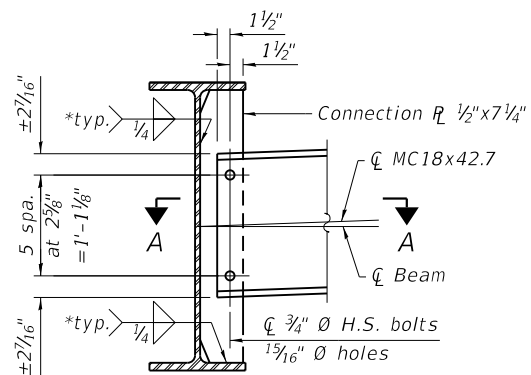


**BEARING STIFFENER DETAIL**  
(24 Required)

\* Stop welds 1/4" (±1/8") from edges of plate.

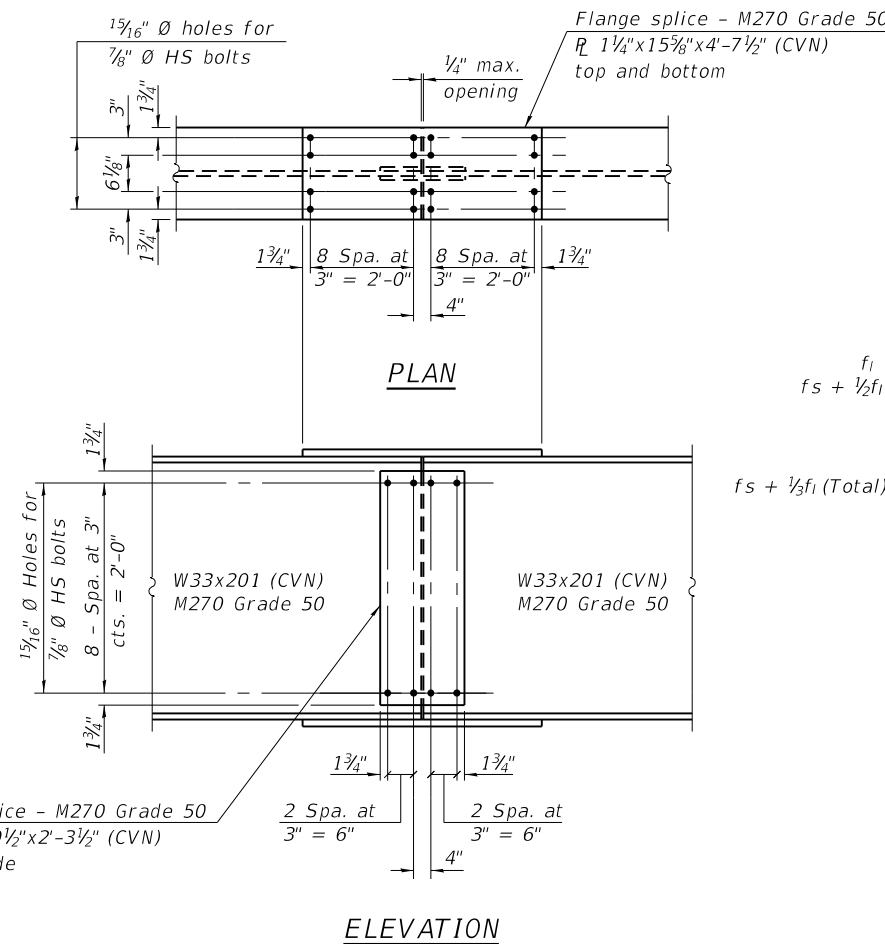


**SECTION A-A**



**INTERIOR DIAPHRAGM (D1)**  
(65 Required)

Notes:  
Two hardened washers required for each set of oversized holes.  
Alternate channels of equal depth and larger weight are permitted to facilitate material acquisition. Alternate channels, if utilized, shall be provided at no additional cost to the Department.



**ELEVATION**

**SPlice DETAIL**  
(12 Required)

Is, Ss: Non-composite moment of inertia and section modulus of the steel section used for computing fs(Total-Strength I, and Service II) due to non-composite dead loads (in.<sup>4</sup> and in.<sup>3</sup>).

Ic(n), Sc(n): Composite moment of inertia and section modulus of the steel and deck based upon the modular ratio, "n", used for computing fs(Total-Strength I, and Service II) in uncracked sections due to short-term composite live loads (in.<sup>4</sup> and in.<sup>3</sup>).

Ic(3n), Sc(3n): Composite moment of inertia and section modulus of the steel and deck based upon 3 times the modular ratio, "3n", used for computing fs(Total-Strength I, and Service II) in uncracked sections, due to long-term composite (superimposed) dead loads (in.<sup>4</sup> and in.<sup>3</sup>).

Ic(cr), Sc(cr): Composite moment of inertia and section modulus of the steel and longitudinal deck reinforcement, used for computing fs (Total-Strength I, and Service II) in cracked sections, due to both short-term composite live loads and long-term composite (superimposed) dead loads (in.<sup>4</sup> and in.<sup>3</sup>).

DC1: Un-factored non-composite dead load (kips/ft.).  
MDC1: Un-factored moment due to non-composite dead load (kip-ft.).  
DC2: Un-factored long-term composite (superimposed excluding future wearing surface) dead load (kips/ft.).  
MDC2: Un-factored moment due to long-term composite (superimposed excluding future wearing surface) dead load (kip-ft.).  
DW: Un-factored long-term composite (superimposed future wearing surface only) dead load (kips/ft.).  
MDW: Un-factored moment due to long-term composite (superimposed future wearing surface only) dead load (kip-ft.).  
M<sub>ℓ + IM</sub>: Un-factored live load moment plus dynamic load allowance (kip-ft.).

f<sub>ℓ</sub> (Strength I): Factored lateral flange stress (ksi).  
M<sub>u</sub> + 1/2 f<sub>ℓ</sub> S<sub>xc</sub>: Factored design moment (kip-ft.).  
1.25 (MDC1 + MDC2) + 1.5 MDW + 1.75 M<sub>ℓ + IM</sub> + 1/2 f<sub>ℓ</sub> (Strength I) S<sub>xc</sub>

∅f Mn: Compact composite positive moment capacity computed according to Article 6.10.7.1 or non-slender negative moment capacity according to Article A6.1.1 or A6.1.2 (kip-ft.).

f<sub>s</sub> DC1: Un-factored stress at edge of flange for controlling steel flange due to vertical non-composite dead loads as calculated below (ksi).  
MDC1/ Ss

f<sub>s</sub> DC2: Un-factored stress at edge of flange for controlling steel flange due to vertical composite dead loads as calculated below (ksi).  
MDC2/ Sc(3n) or MDC2/ Sc(cr) as applicable.

f<sub>s</sub> DW: Un-factored stress at edge of flange for controlling steel flange due to vertical composite future wearing surface loads as calculated below (ksi).  
MDW/ Sc(3n) or MDW/ Sc(cr) as applicable.

f<sub>s</sub> (ℓ+IM): Un-factored stress at edge of flange for controlling steel flange due to vertical composite live load plus impact loads as calculated below (ksi).  
M<sub>ℓ + IM</sub> / Sc(n) or M<sub>ℓ + IM</sub> / Sc(cr) as applicable.

f<sub>ℓ</sub> (Service II): Un-factored lateral flange stress (ksi).  
f<sub>s</sub> + 1/2 f<sub>ℓ</sub> (Service II): Sum of stresses as computed below (ksi).  
f<sub>s</sub> DC1 + f<sub>s</sub> DC2 + f<sub>s</sub> DW + 1.3 f<sub>s</sub> (ℓ + IM) + 1/2 f<sub>ℓ</sub> (Service II)

0.95R<sub>h</sub>F<sub>yf</sub>: Composite stress capacity for Service II loading according to Article 6.10.4.2 (ksi).

f<sub>s</sub> + 1/2 f<sub>ℓ</sub> (Total)(Strength I): Sum of stresses as computed below on non-compact section (ksi).  
1.25 (f<sub>s</sub> DC1 + f<sub>s</sub> DC2) + 1.5 f<sub>s</sub> DW + 1.75 f<sub>s</sub> (ℓ + IM) + 1/2 f<sub>ℓ</sub> (Strength I)

∅f F<sub>n</sub>: Non-compact composite positive or negative stress capacity for Strength I loading according to Article 6.10.7 or 6.10.8 (ksi).

Vf: Maximum factored shear range in span computed according to Article 6.10.10.

LLDF: Live Load Distribution Factor computed according to Table 4.6.2.2.2b-1, Table 4.6.2.2.2d-1, Table 4.6.2.2.3a-1 and Table 4.6.2.2.3b-1.

OCF: Obtuse Correction Factor computed according to Table 4.6.2.2.3c-1 or as simplified in Section 3.3.1 of the Bridge Manual.

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USER NAME = kah  
ESCA PROJECT NO. 1259.11  
PLOT SCALE = 0.1667' / in.  
PLOT DATE = 11/4/2019

DESIGNED - KJA 07/19  
CHECKED - RTM/SHL 07/19  
DRAWN - KAH 07/19  
CHECKED - MTD/SHL 08/19

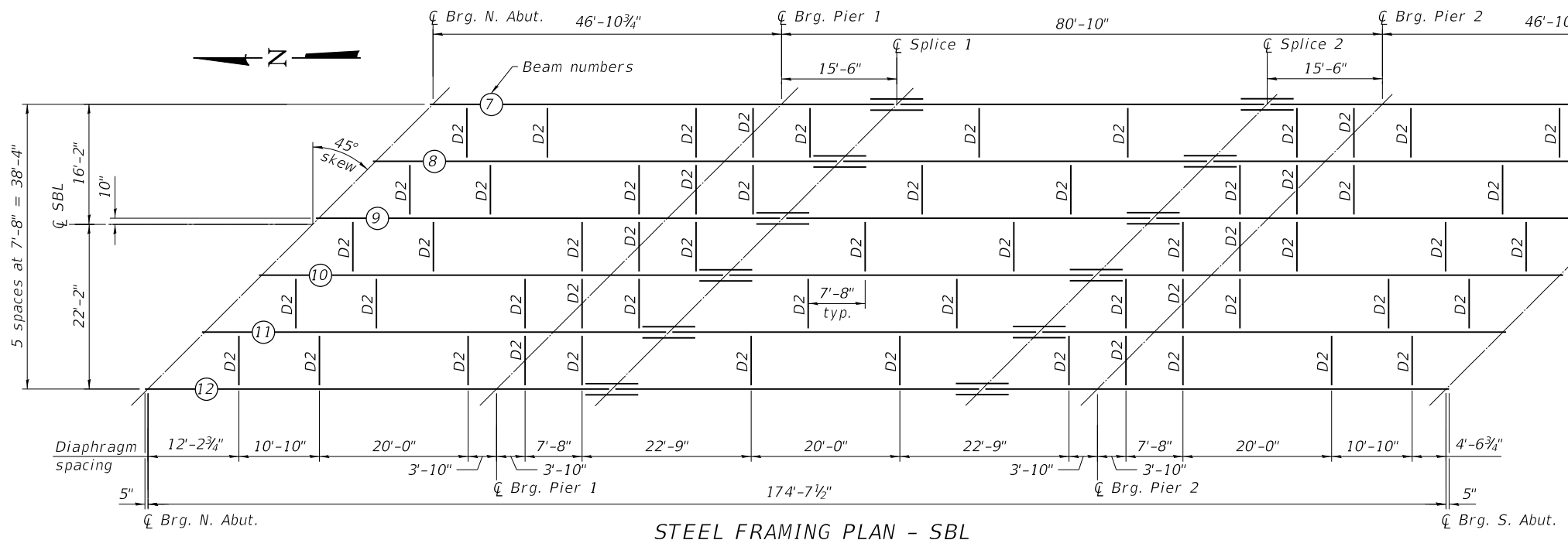
REVISED -  
REVISED -  
REVISED -  
REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

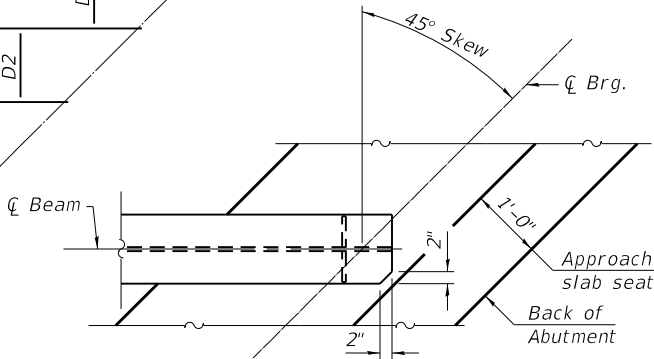
STEEL FRAMING DETAILS (NB)  
STRUCTURE NO. 091-0077 (NB)

SHEET 17 OF 38 SHEETS

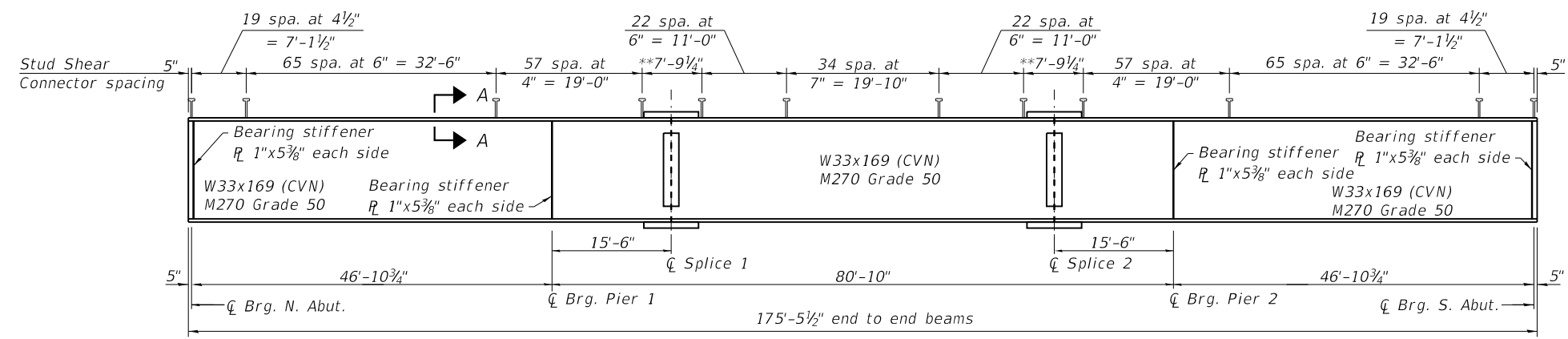
| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 83        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |



**STEEL FRAMING PLAN - SBL**

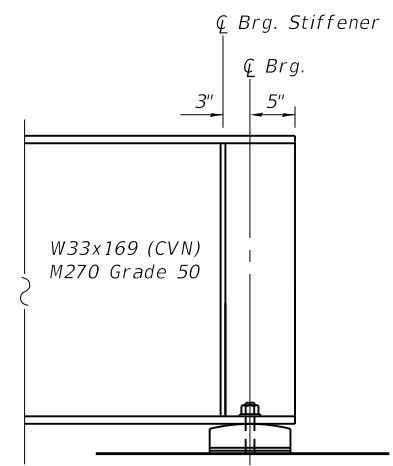


**TOP FLANGE CLIP DETAIL**  
(Showing top flange of beam)



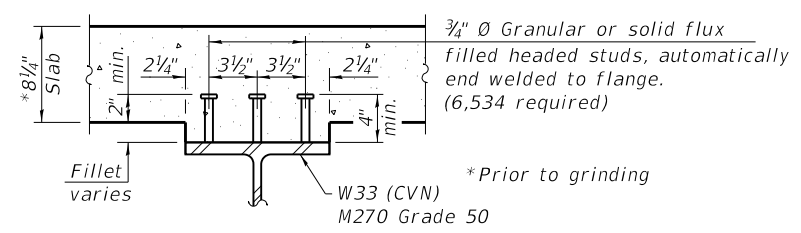
**BEAM ELEVATION**

\*\* Omit shear connectors over splices



**END OF BEAM DETAIL**  
(Showing bearing stiffener location)

Notes:  
See Sheet 19 of 38 for additional steel details.  
All diaphragms shall be installed as steel is erected and secured with erection pins and bolts except as otherwise noted. Individual diaphragms at supports may be temporarily disconnected to install bearing anchors rods.  
Load carrying components designated "CVN" denotes Charpy-V-Notch Impact Energy Requirements, Zone 2.



**SECTION A-A**

**TOP OF BEAM ELEVATIONS**  
(For fabrication only)

| Beam | ☐ Brg. N. Abut. | ☐ Brg. Pier 1 | ☐ Splice 1 | ☐ Splice 2 | ☐ Brg. Pier 2 | ☐ Brg. S. Abut. |
|------|-----------------|---------------|------------|------------|---------------|-----------------|
| 7    | 404.27          | 404.43        | 404.48     | 404.81     | 404.97        | 405.44          |
| 8    | 404.37          | 404.52        | 404.57     | 404.89     | 405.04        | 405.50          |
| 9    | 404.46          | 404.60        | 404.64     | 404.95     | 405.10        | 405.54          |
| 10   | 404.35          | 404.47        | 404.51     | 404.81     | 404.95        | 405.38          |
| 11   | 404.20          | 404.31        | 404.35     | 404.63     | 404.77        | 405.19          |
| 12   | 404.03          | 404.13        | 404.16     | 404.43     | 404.56        | 404.97          |

MODEL: PLOT FILE NAME: Y:\IDOT\1259-11\_78504\CADD\SP\_091-0077-0078\0910077-78504-18-SIF\mPinDlssSB.dgn



|                               |                         |           |
|-------------------------------|-------------------------|-----------|
| USER NAME = kah               | DESIGNED - KJA 07/19    | REVISED - |
| ESCA PROJECT NO. 1259.11      | CHECKED - RTM/SHL 07/19 | REVISED - |
| PLOT SCALE = 0:2,000 ** / in. | DRAWN - KAH 07/19       | REVISED - |
| PLOT DATE = 10/15/2019        | CHECKED - MTD/SHL 08/19 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

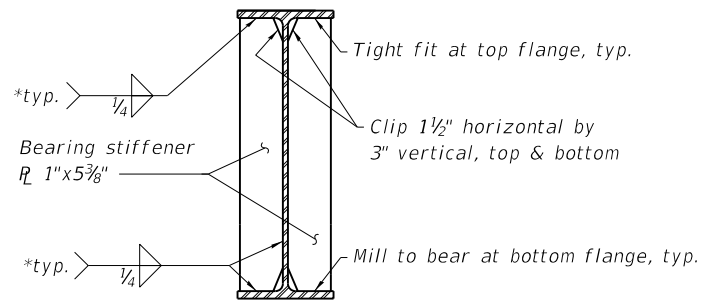
**STEEL FRAMING PLAN & DETAILS (SB)  
STRUCTURE NO. 091-0078 (SB)**

SHEET 18 OF 38 SHEETS

|                           |           |        |              |           |
|---------------------------|-----------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
| 57                        | (91-3)B-2 | UNION  | 161          | 84        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |

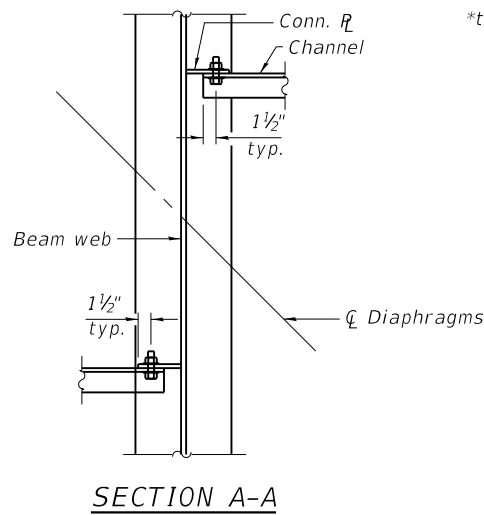
|             | BEAM REACTION TABLE |          |          |          |
|-------------|---------------------|----------|----------|----------|
|             | Abutment            |          | Pier     |          |
|             | Interior            | Exterior | Interior | Exterior |
| LLDF        | 0.7911              | 0.7043   | 0.7911   | 0.7043   |
| OCF         | -                   | 1.2      | -        | -        |
| $R_{DC1}$   | (k) 13.6            | 13.5     | 77.1     | 77.0     |
| $R_{DC2}$   | (k) 2.6             | 2.6      | 14.0     | 14.0     |
| $R_{DW}$    | (k) 5.2             | 3.8      | 28.4     | 21.0     |
| $R_{\ell}$  | (k) 55.4            | 59.2     | 95.2     | 84.8     |
| $R_{IM}$    | (k) 14.6            | 15.6     | 18.7     | 16.7     |
| $R_{Total}$ | (k) 91.4            | 94.7     | 233.4    | 213.5    |

|   | INTERIOR BEAM MOMENT TABLE |                         |            |
|---|----------------------------|-------------------------|------------|
|   | 0.4 Sp. 1 or 0.6 Sp. 3     | Pier                    | 0.5 Span 2 |
|   | $I_s$                      | (in <sup>4</sup> ) 9290 | 9290       |
| $I_c(n)$                                    | (in <sup>4</sup> ) 24520   | 24520                   | 24520      |
| $I_c(3n)$                                   | (in <sup>4</sup> ) 18125   | 18125                   | 18125      |
| $I_c(cr)$                                   | (in <sup>4</sup> ) -       | 12386                   | -          |
| $S_s$                                       | (in <sup>3</sup> ) 549.0   | 549.0                   | 549.0      |
| $S_c(n)$                                    | (in <sup>3</sup> ) 795.7   | 795.7                   | 795.7      |
| $S_c(3n)$                                   | (in <sup>3</sup> ) 722.5   | 722.5                   | 722.5      |
| $S_c(cr)$                                   | (in <sup>3</sup> ) -       | 624.5                   | -          |
| DC1   | (k/ft) 1.042               | 1.042                   | 1.042      |
| MDC1  | (k) 74.9                   | 489.4                   | 360.7      |
| DC2   | (k/ft) 0.190               | 0.190                   | 0.190      |
| MDC2  | (k) 14.5                   | 89.1                    | 66.1       |
| DW  | (k/ft) 0.384               | 0.384                   | 0.384      |
| MDW   | (k) 29.3                   | 180.0                   | 133.6      |
| LLDF  | 0.6994                     | 0.6620                  | 0.6349     |
| $M_{\ell+IM}$                               | (k) 552.7                  | 691.8                   | 761.2      |
| $f_l$ (Strength I)                          | (ksi) 0                    | 0                       | 0          |
| $M_u + \frac{1}{2} f_l S_{xc}$              | (k) 1123.0                 | 2203.8                  | 2066.1     |
| $\phi f Mn$                                 | (k) 4248.3                 | -                       | 4011.1     |
| $f_s DC1$                                   | (ksi) 1.64                 | 10.70                   | 7.88       |
| $f_s DC2$                                   | (ksi) 0.24                 | 1.71                    | 1.10       |
| $f_s DW$                                    | (ksi) 0.49                 | 3.46                    | 2.22       |
| $f_s (\ell+IM)$                             | (ksi) 8.34                 | 13.29                   | 11.48      |
| $f_l$ (Service II)                          | (ksi) 0                    | 0                       | 0          |
| $f_s + \frac{1}{2} f_l$ (Service II)        | (ksi) 13.21                | 33.15                   | 26.12      |
| $0.95R_h F_{yf}$                            | (ksi) 47.50                | 47.50                   | 47.50      |
| $f_s + \frac{1}{2} f_l$ (Total)(Strength I) | (ksi) -                    | 43.96                   | -          |
| $\phi F_n$                                  | (ksi) -                    | 50.00                   | -          |
| Vf  | (k) 58.5                   | 67.3                    | 55.1       |

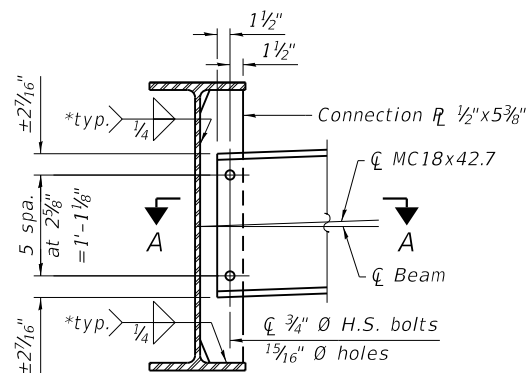


**BEARING STIFFENER DETAIL**  
(24 Required)

\* Stop welds 1/4" ( $\pm 1/8$ ") from edges of plate.

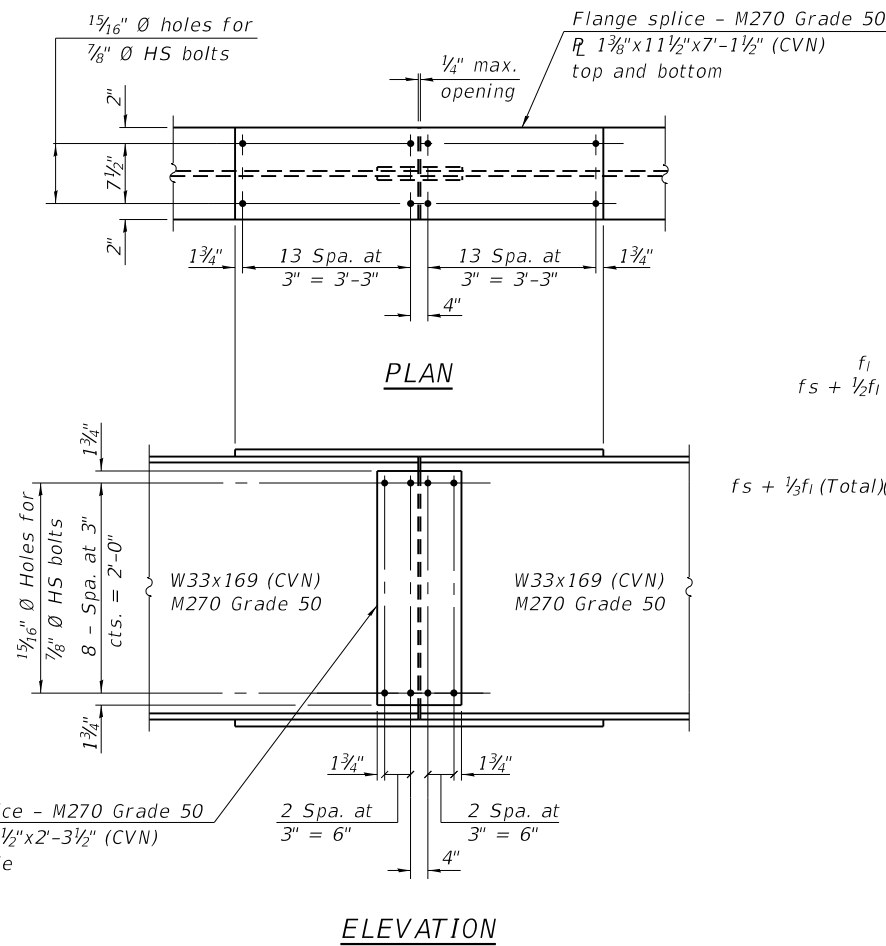


**SECTION A-A**



**INTERIOR DIAPHRAGM (D2)**  
(60 Required)

Notes:  
Two hardened washers required for each set of oversized holes.  
Alternate channels of equal depth and larger weight are permitted to facilitate material acquisition. Alternate channels, if utilized, shall be provided at no additional cost to the Department.



**ELEVATION**  
**SPLICE DETAIL**  
(12 Required)

$I_s, S_s$ : Non-composite moment of inertia and section modulus of the steel section used for computing  $f_s$ (Total-Strength I, and Service II) due to non-composite dead loads (in.<sup>4</sup> and in.<sup>3</sup>).

$I_c(n), S_c(n)$ : Composite moment of inertia and section modulus of the steel and deck based upon the modular ratio, "n", used for computing  $f_s$ (Total-Strength I, and Service II) in uncracked sections due to short-term composite live loads (in.<sup>4</sup> and in.<sup>3</sup>).

$I_c(3n), S_c(3n)$ : Composite moment of inertia and section modulus of the steel and deck based upon 3 times the modular ratio, "3n", used for computing  $f_s$ (Total-Strength I, and Service II) in uncracked sections, due to long-term composite (superimposed) dead loads (in.<sup>4</sup> and in.<sup>3</sup>).

$I_c(cr), S_c(cr)$ : Composite moment of inertia and section modulus of the steel and longitudinal deck reinforcement, used for computing  $f_s$  (Total-Strength I, and Service II) in cracked sections, due to both short-term composite live loads and long-term composite (superimposed) dead loads (in.<sup>4</sup> and in.<sup>3</sup>).

DC1: Un-factored non-composite dead load (kips/ft.).  
MDC1: Un-factored moment due to non-composite dead load (kip-ft.).  
DC2: Un-factored long-term composite (superimposed excluding future wearing surface) dead load (kips/ft.).  
MDC2: Un-factored moment due to long-term composite (superimposed excluding future wearing surface) dead load (kip-ft.).  
DW: Un-factored long-term composite (superimposed future wearing surface only) dead load (kips/ft.).  
MDW: Un-factored moment due to long-term composite (superimposed future wearing surface only) dead load (kip-ft.).  
 $M_{\ell+IM}$ : Un-factored live load moment plus dynamic load allowance (impact) (kip-ft.).  
 $f_l$  (Strength I): Factored lateral flange stress (ksi).  
 $M_u + \frac{1}{2} f_l S_{xc}$ : Factored design moment (kip-ft.).  
1.25 (MDC1 + MDC2) + 1.5 MDW + 1.75  $M_{\ell+IM} + \frac{1}{2} f_l$  (Strength I)  $S_{xc}$   
 $\phi f Mn$ : Compact composite positive moment capacity computed according to Article 6.10.7.1 or non-slender negative moment capacity according to Article A6.1.1 or A6.1.2 (kip-ft.).  
 $f_s DC1$ : Un-factored stress at edge of flange for controlling steel flange due to vertical non-composite dead loads as calculated below (ksi).  
MDC1/  $S_s$   
 $f_s DC2$ : Un-factored stress at edge of flange for controlling steel flange due to vertical composite dead loads as calculated below (ksi).  
MDC2/  $S_c(3n)$  or MDC2/  $S_c(cr)$  as applicable.  
 $f_s DW$ : Un-factored stress at edge of flange for controlling steel flange due to vertical composite future wearing surface loads as calculated below (ksi).  
MDW/  $S_c(3n)$  or MDW/  $S_c(cr)$  as applicable.  
 $f_s (\ell+IM)$ : Un-factored stress at edge of flange for controlling steel flange due to vertical composite live load plus impact loads as calculated below (ksi).  
 $M_{\ell+IM} / S_c(n)$  or  $M_{\ell+IM} / S_c(cr)$  as applicable.  
 $f_l$  (Service II): Un-factored lateral flange stress (ksi).  
 $f_s + \frac{1}{2} f_l$  (Service II): Sum of stresses as computed below (ksi).  
 $f_s DC1 + f_s DC2 + f_s DW + 1.3 f_s (\ell+IM) + \frac{1}{2} f_l$  (Service II)  
 $0.95R_h F_{yf}$ : Composite stress capacity for Service II loading according to Article 6.10.4.2 (ksi).  
 $f_s + \frac{1}{2} f_l$  (Total)(Strength I): Sum of stresses as computed below on non-compact section (ksi).  
1.25 ( $f_s DC1 + f_s DC2$ ) + 1.5  $f_s DW + 1.75 f_s (\ell+IM) + \frac{1}{2} f_l$  (Strength I)  
 $\phi F_n$ : Non-compact composite positive or negative stress capacity for Strength I loading according to Article 6.10.7 or 6.10.8 (ksi).  
Vf: Maximum factored shear range in span computed according to Article 6.10.10.  
LLDF: Live Load Distribution Factor computed according to Table 4.6.2.2.2b-1, Table 4.6.2.2.2d-1, Table 4.6.2.2.3a-1 and Table 4.6.2.2.3b-1.  
OCF: Obtuse Correction Factor computed according to Table 4.6.2.2.3c-1 or as simplified in Section 3.3.1 of the Bridge Manual.

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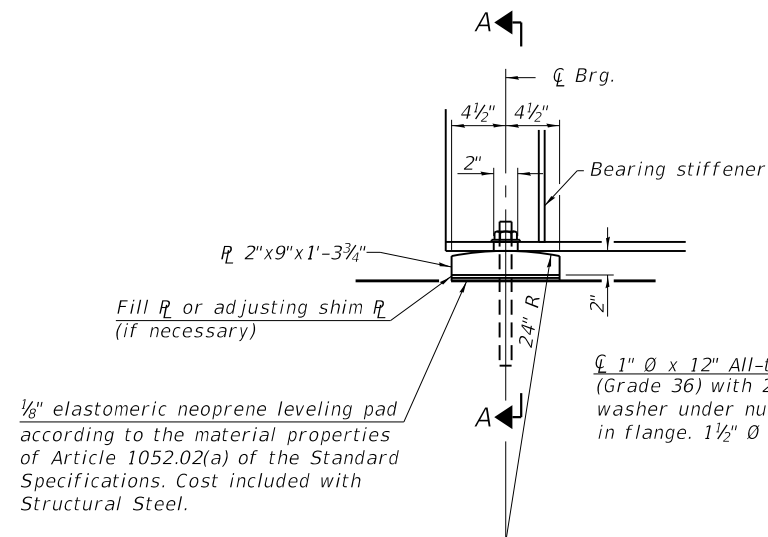
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| ESCA PROJECT NO. 1259.11   | CHECKED - RTM/SHL 07/19 | REVISED - |
| PLOT SCALE = 0.1667' / in. | DRAWN - KAH 07/19       | REVISED - |
| PLOT DATE = 11/4/2019      | CHECKED - MTD/SHL 08/19 | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

STEEL FRAMING DETAILS (SB)  
STRUCTURE NO. 091-0078 (SB)

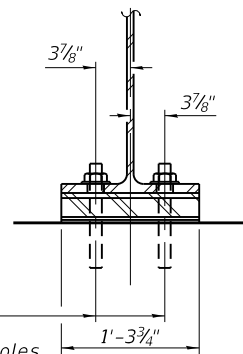
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|---------------------------|-----------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
| 57                        | (91-3)B-2 | UNION  | 161          | 85        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |

SHEET 19 OF 38 SHEETS



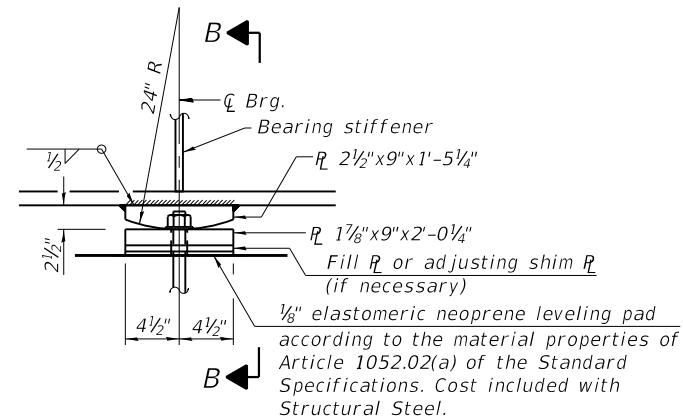
1/8" elastomeric neoprene leveling pad according to the material properties of Article 1052.02(a) of the Standard Specifications. Cost included with Structural Steel.

ELEVATION AT ABUTMENT



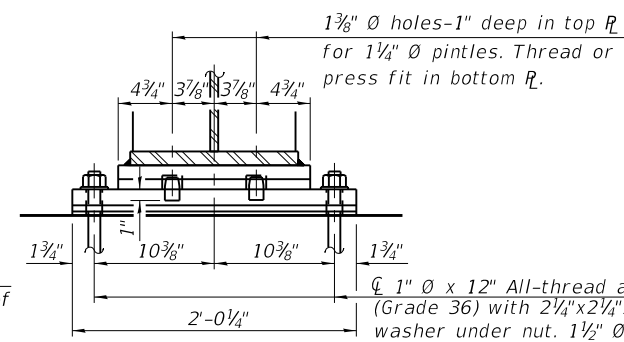
SECTION A-A

FIXED BEARING AT ABUTMENTS - NBL



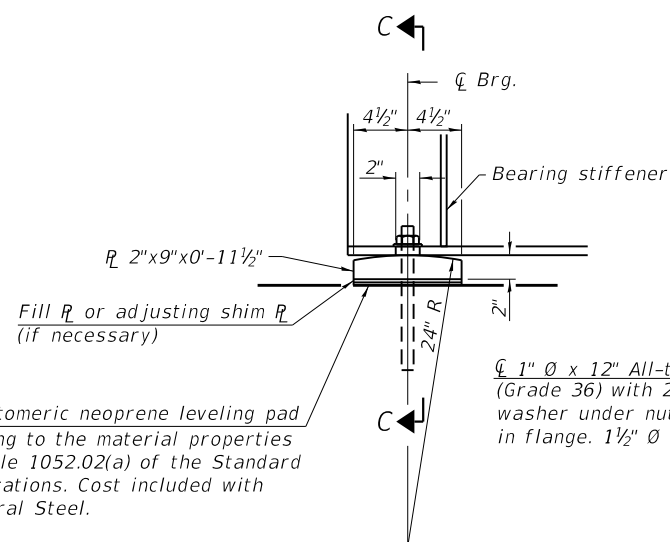
1/8" elastomeric neoprene leveling pad according to the material properties of Article 1052.02(a) of the Standard Specifications. Cost included with Structural Steel.

ELEVATION AT PIER



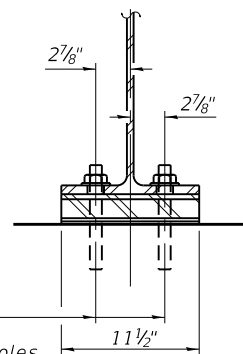
SECTION B-B

FIXED BEARING AT PIERS - NBL



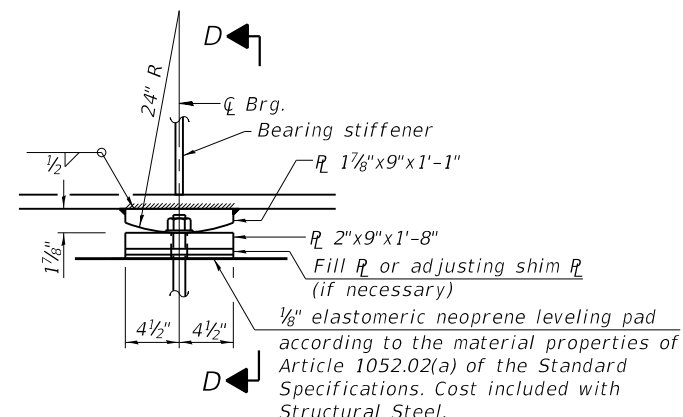
1/8" elastomeric neoprene leveling pad according to the material properties of Article 1052.02(a) of the Standard Specifications. Cost included with Structural Steel.

ELEVATION AT ABUTMENT



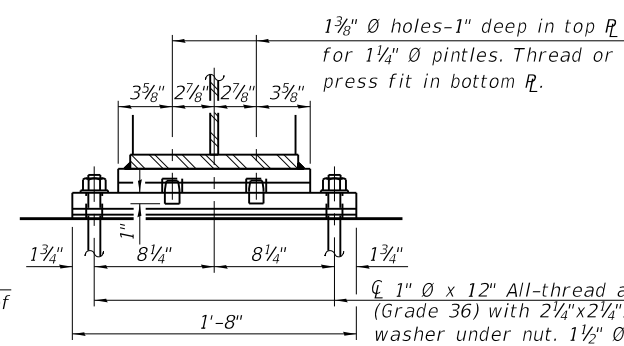
SECTION C-C

FIXED BEARING AT ABUTMENTS - SBL



1/8" elastomeric neoprene leveling pad according to the material properties of Article 1052.02(a) of the Standard Specifications. Cost included with Structural Steel.

ELEVATION AT PIER



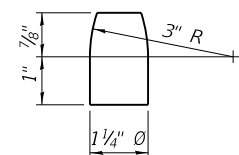
SECTION D-D

FIXED BEARING AT PIERS - SBL

BEARING FILL PLATES

| Location                  | Thickness |
|---------------------------|-----------|
| NBL South Abutment Beam 4 | 5/8"      |
| SBL South Abutment Beam 9 | 1/2"      |

Fill plates are required at the locations shown in this table and shall be placed as shown on bearing details.



PINTLE

Notes:

- Anchor bolts shall be according to Article 521.06 of the Standard Specifications.
- Beams shall be braced for stability during erection and remain braced until deck is poured and cured.
- Anchor bolts at all supports shall be installed as each member is erected unless an equivalent temporary means of lateral restraint is used.
- Two 1/8 in. adjusting shims shall be provided for each bearing in addition to all other plates or shims and placed as shown on bearing details.
- The anchor bolt sizes and grades shown in the Fixed Bearing at Piers details constitute a calculated seismic structural fuse. Substitution of higher diameter and/or grade anchor bolts will not be allowed.

TWO BRIDGES  
BILL OF MATERIAL

| Item             | Unit | Total |
|------------------|------|-------|
| Anchor Bolts, 1" | Each | 96    |

MODEL: PLOT  
FILE NAME: Y:\IDOT\1259-11\_78504\CADD\SP\_SN\_091-0077-0078\0910077-78504-20-BrgDtlis.dgn



USER NAME = kah  
ESCA PROJECT NO. 1259.11  
PLOT SCALE = 0.1667 ' / in.  
PLOT DATE = 10/15/2019

DESIGNED - KJA 07/19  
CHECKED - RTM/SHL 07/19  
DRAWN - KAH 07/19  
CHECKED - MTD/SHL 08/19

REVISED -  
REVISED -  
REVISED -  
REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

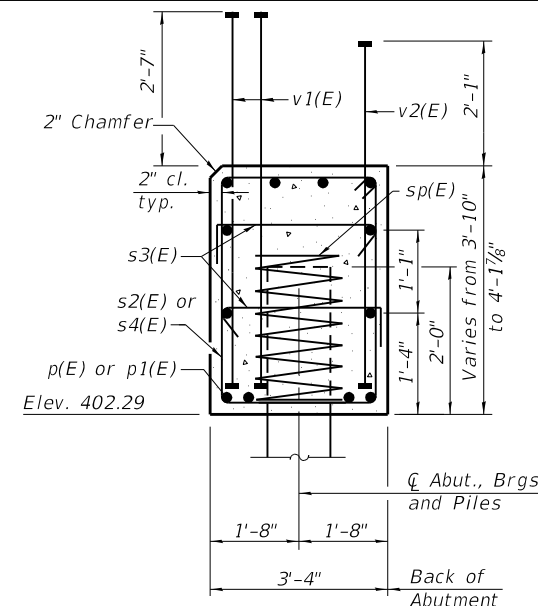
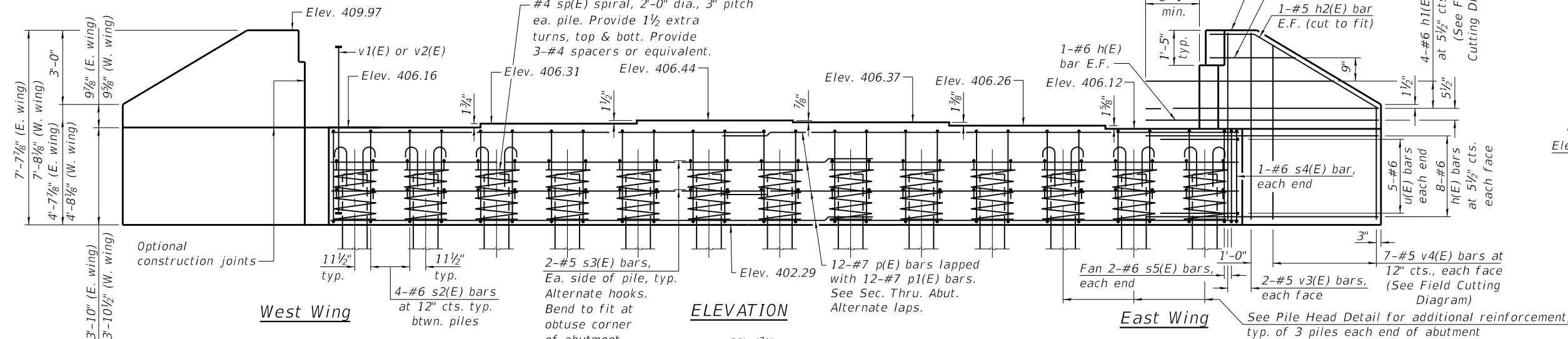
BEARING DETAILS  
STRUCTURE NO. 091-0077 (NB) & 091-0078 (SB)

SHEET 20 OF 38 SHEETS

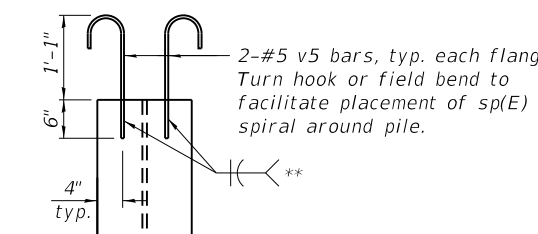
| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 86        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |



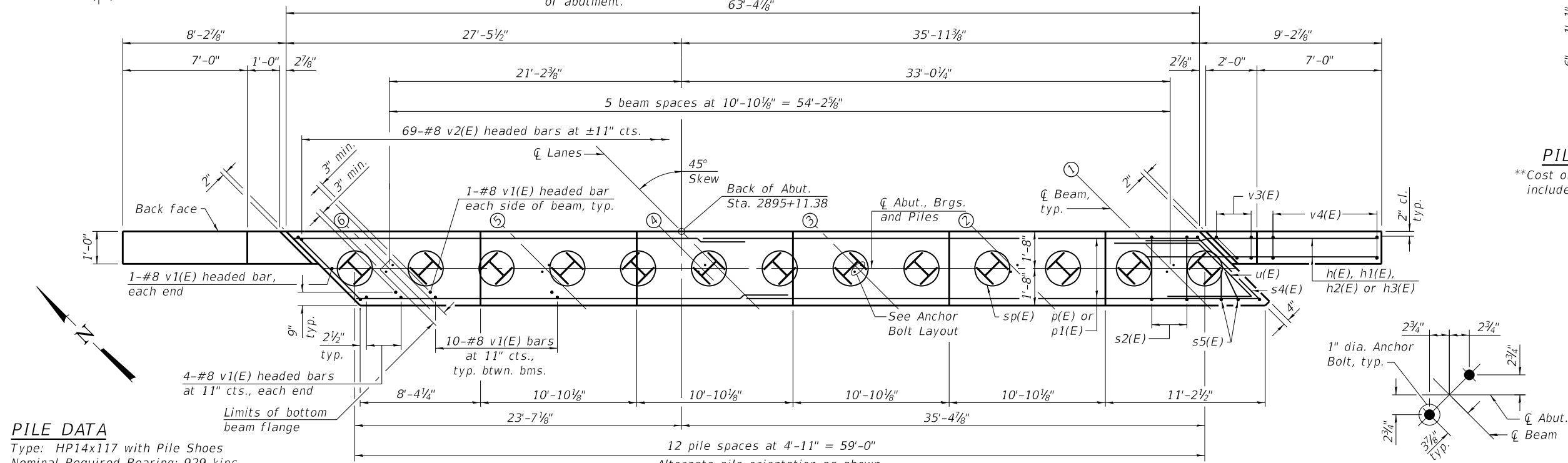
Notes:  
 Pour steps monolithically with cap.  
 For details of piles see Sheet 31 of 38.  
 Space reinforcement in cap to miss anchor bolts.  
 Headed bars shall conform to ASTM A970 with Threaded attachment; Class HA; and reinforcement bars conforming to ASTM A706. Cost included with Reinforcement Bars, Epoxy Coated.



**SEC. THRU ABUT.**  
 Dimensions at right angles to abutment.



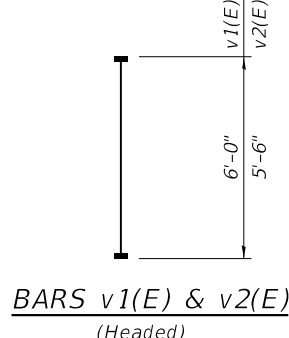
**PILE HEAD DETAIL**  
 \*\*Cost of welding v5 bars to piles included with Furnishing Piles.



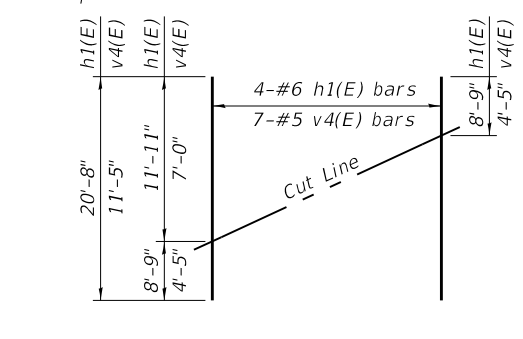
**PLAN**

**PILE DATA**

Type: HP14x117 with Pile Shoes  
 Nominal Required Bearing: 929 kips  
 Factored Resistance Available: 511 kips  
 Est. Length: 39 ft.  
 No. Production Piles: 13  
 No. Test Piles: 0

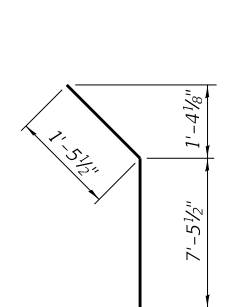


**BARS v1(E) & v2(E)**  
 (Headed)

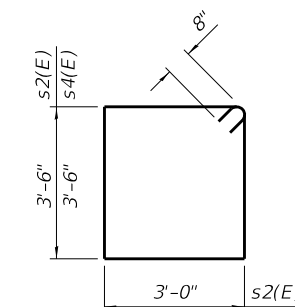


**FIELD CUTTING DIAGRAM**

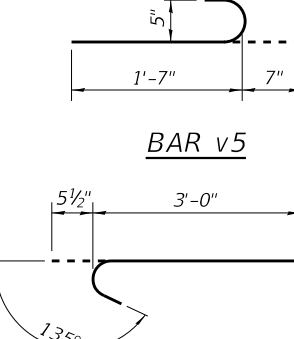
Order h1(E) and v4(E) full length. Cut as shown and use remainder of bars in opposite face.



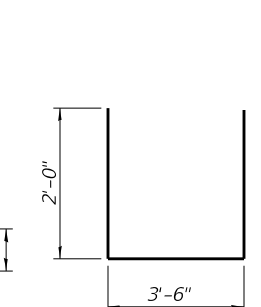
**BAR h2(E)**



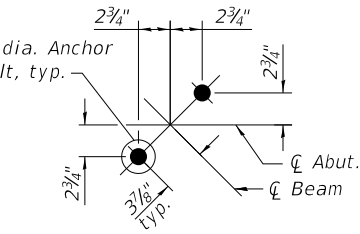
**BARS s2(E) & s4(E)**



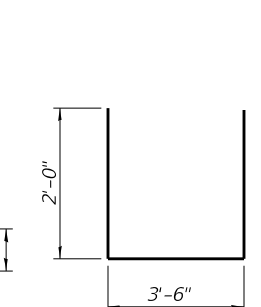
**BAR s3(E)**



**BAR s5(E)**



**ANCHOR BOLT LAYOUT**



**BAR u(E)**

**BILL OF MATERIAL**

| Bar                               | No. | Size    | Length | Shape |
|-----------------------------------|-----|---------|--------|-------|
| h(E)                              | 36  | #6      | 12'-2" | —     |
| h1(E)                             | 8   | #6      | 20'-8" | —     |
| h2(E)                             | 4   | #5      | 8'-11" | —     |
| h3(E)                             | 4   | #5      | 3'-1"  | —     |
| p(E)                              | 12  | #7      | 37'-0" | —     |
| p1(E)                             | 12  | #7      | 31'-0" | —     |
| s2(E)                             | 48  | #6      | 14'-4" | U     |
| s3(E)                             | 52  | #5      | 4'-0"  | U     |
| s4(E)                             | 2   | #6      | 16'-9" | U     |
| s5(E)                             | 4   | #6      | 7'-6"  | U     |
| sp(E)                             | 13  | #4      | 2'-0"  | WWW   |
| u(E)                              | 10  | #6      | 12'-9" | U     |
| v1(E)                             | 72  | #8      | 6'-0"  | I     |
| v2(E)                             | 69  | #8      | 5'-6"  | I     |
| v3(E)                             | 8   | #5      | 7'-3"  | I     |
| v4(E)                             | 14  | #5      | 11'-5" | I     |
| v5                                | 24  | #5      | 2'-2"  | U     |
| Structure Excavation              |     | Cu. Yd. | 182    |       |
| Concrete Structures               |     | Cu. Yd. | 35.4   |       |
| Reinforcement Bars                |     | Pound   | 55     |       |
| Reinforcement Bars, Epoxy Coated  |     | Pound   | 7,160  |       |
| Furnishing Steel Piles, HP 14x117 |     | Foot    | 507    |       |
| Driving Piles                     |     | Foot    | 507    |       |
| Pile Shoes                        |     | Each    | 13     |       |

\* Length is height of spiral

MODEL: PLOT  
 FILE NAME: Y:\D011259-11\_78504\CADD\SP\_SN\_091-0077-0078\0910077-78504-21-NAButNB.dgn  
 10/15/2019 12:33:25 PM



USER NAME = kah  
 ESCA PROJECT NO. 1259.11  
 PLOT SCALE = 0:2" = 1' / in.  
 PLOT DATE = 10/15/2019

DESIGNED - KJA 07/19  
 CHECKED - RTM/SHL 07/19  
 DRAWN - KAH 07/19  
 CHECKED - MTD/SHL 08/19

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

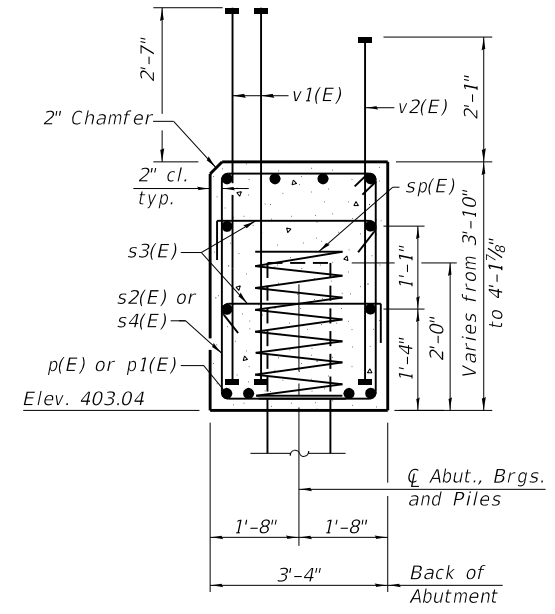
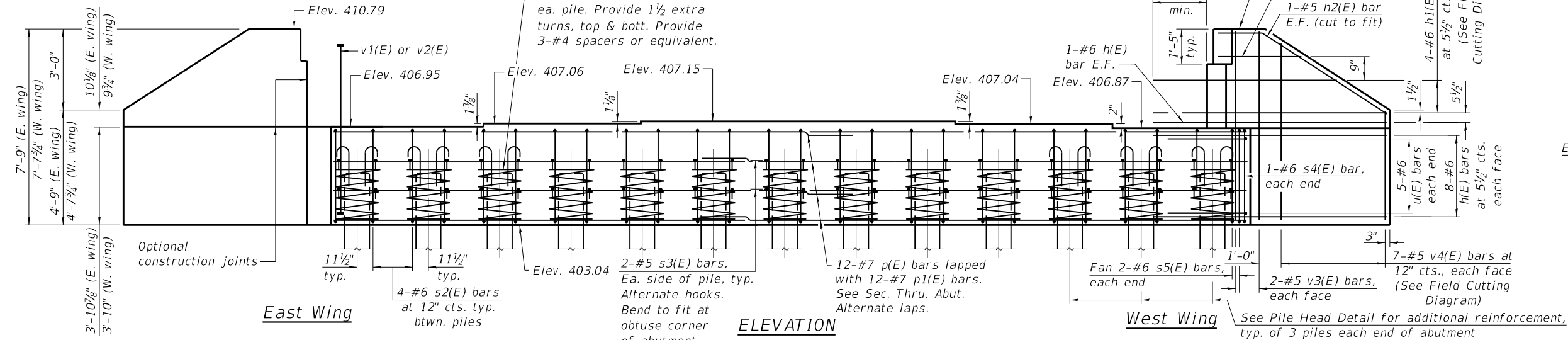
**NORTH ABUTMENT (NB)  
 STRUCTURE NO. 091-0077 (NB)**

SHEET 21 OF 38 SHEETS

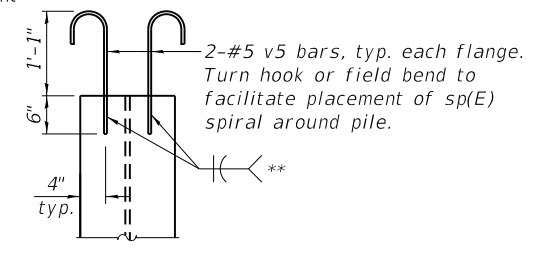
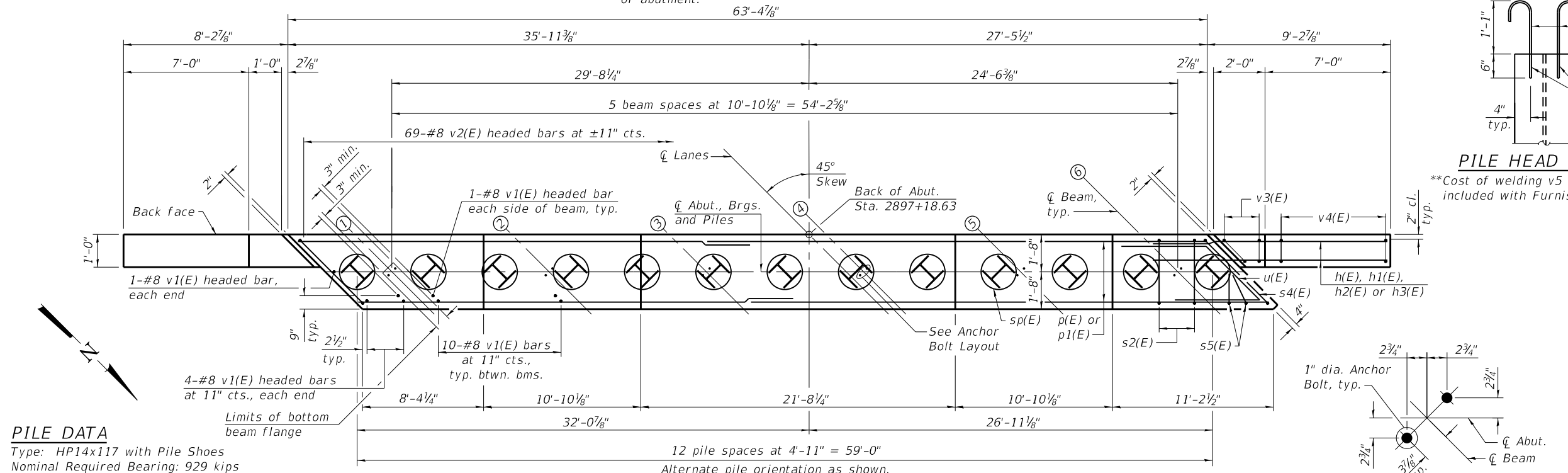
| F.A.I. RTE. | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|-----------|--------|--------------|-----------|
| 57          | (91-3)B-2 | UNION  | 161          | 87        |

CONTRACT NO. 78504  
 ILLINOIS FED. AID PROJECT

Notes:  
 Pour steps monolithically with cap.  
 For details of piles see Sheet 31 of 38.  
 Space reinforcement in cap to miss anchor bolts.  
 Headed bars shall conform to ASTM A970 with Threaded attachment; Class HA; and reinforcement bars conforming to ASTM A706. Cost included with Reinforcement Bars, Epoxy Coated.



**SEC. THRU ABUT.**  
 Dimensions at right angles to abutment.



**PILE HEAD DETAIL**  
 \*\*Cost of welding v5 bars to piles included with Furnishing Piles.

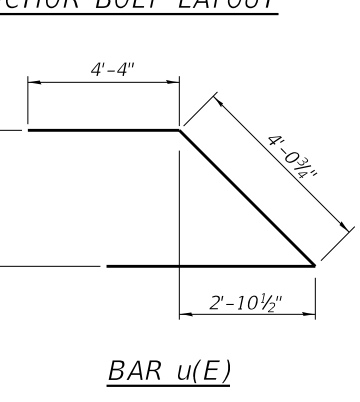
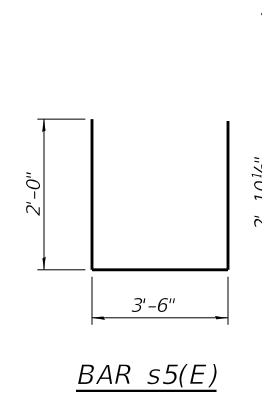
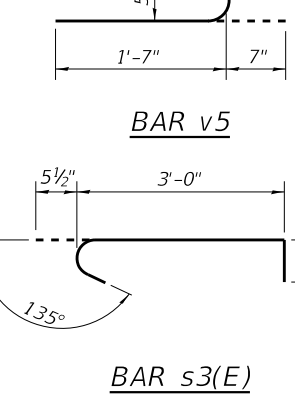
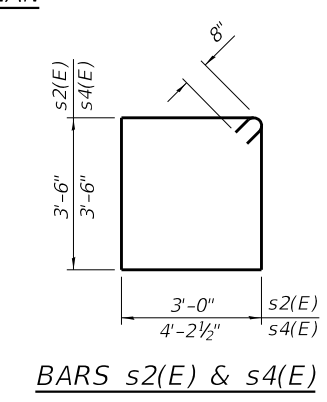
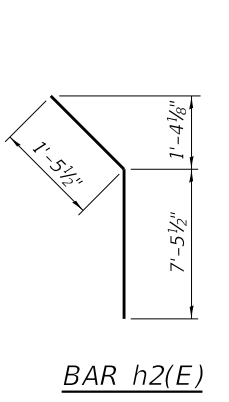
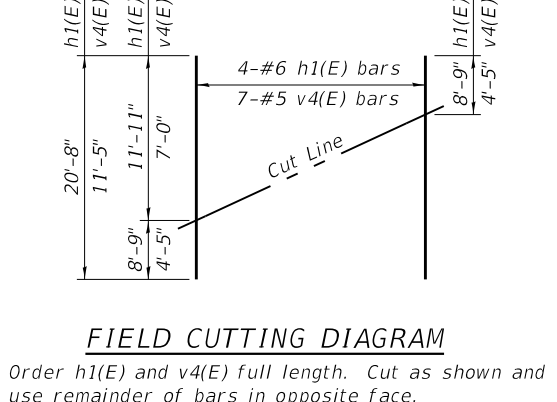
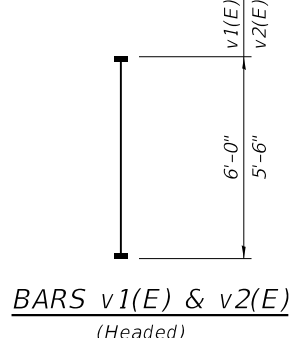
**BILL OF MATERIAL**

| Bar                               | No. | Size    | Length | Shape |
|-----------------------------------|-----|---------|--------|-------|
| h(E)                              | 36  | #6      | 12'-2" | —     |
| h1(E)                             | 8   | #6      | 20'-8" | —     |
| h2(E)                             | 4   | #5      | 8'-11" | —     |
| h3(E)                             | 4   | #5      | 3'-1"  | —     |
| p(E)                              | 12  | #7      | 37'-0" | —     |
| p1(E)                             | 12  | #7      | 31'-0" | —     |
| s2(E)                             | 48  | #6      | 14'-4" | □     |
| s3(E)                             | 52  | #5      | 4'-0"  | U     |
| s4(E)                             | 2   | #6      | 16'-9" | □     |
| s5(E)                             | 4   | #6      | 7'-6"  | U     |
| * sp(E)                           | 13  | #4      | 2'-0"  | W     |
| u(E)                              | 10  | #6      | 12'-9" | U     |
| v1(E)                             | 72  | #8      | 6'-0"  | —     |
| v2(E)                             | 69  | #8      | 5'-6"  | —     |
| v3(E)                             | 8   | #5      | 7'-3"  | —     |
| v4(E)                             | 14  | #5      | 11'-5" | —     |
| v5                                | 24  | #5      | 2'-2"  | —     |
| Structure Excavation              |     | Cu. Yd. | 173    |       |
| Concrete Structures               |     | Cu. Yd. | 35.5   |       |
| Reinforcement Bars                |     | Pound   | 55     |       |
| Reinforcement Bars, Epoxy Coated  |     | Pound   | 7,160  |       |
| Furnishing Steel Piles, HP 14x117 |     | Foot    | 507    |       |
| Driving Piles                     |     | Foot    | 507    |       |
| Pile Shoes                        |     | Each    | 13     |       |

\* Length is height of spiral

**PILE DATA**

Type: HP14x117 with Pile Shoes  
 Nominal Required Bearing: 929 kips  
 Factored Resistance Available: 511 kips  
 Est. Length: 39 ft.  
 No. Production Piles: 13  
 No. Test Piles: 0



|                              |                   |       |           |
|------------------------------|-------------------|-------|-----------|
| USER NAME = kah              | DESIGNED - KJA    | 07/19 | REVISED - |
| ESCA PROJECT NO. 1259.11     | CHECKED - RTM/SHL | 07/19 | REVISED - |
| PLOT SCALE = 0:2" = 1' / in. | DRAWN - KAH       | 07/19 | REVISED - |
| PLOT DATE = 10/15/2019       | CHECKED - MTD/SHL | 08/19 | REVISED - |

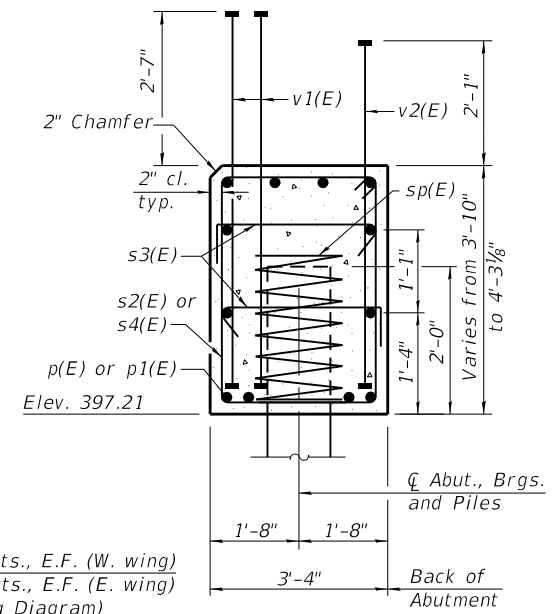
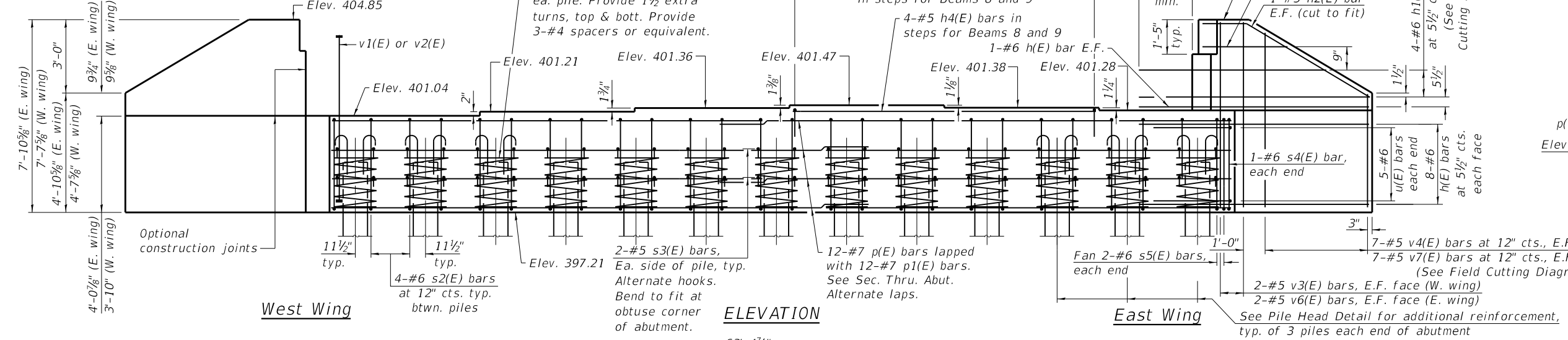
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SOUTH ABUTMENT (NB)  
 STRUCTURE NO. 091-0077 (NB)**

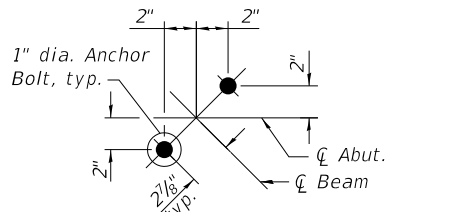
| F.A.I. RTE.        | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|-----------|--------|--------------|-----------|
| 57                 | (91-3)B-2 | UNION  | 161          | 88        |
| CONTRACT NO. 78504 |           |        |              |           |

**Notes:**

Pour steps monolithically with cap.  
 For details of piles see Sheet 31 of 38.  
 Space reinforcement in cap to miss anchor bolts.  
 Headed bars shall conform to ASTM A970 with Threaded attachment; Class HA; and reinforcement bars conforming to ASTM A706. Cost included with Reinforcement Bars, Epoxy Coated.



**SEC. THRU ABUT.**  
 Dimensions at right angles to abutment.



**ANCHOR BOLT LAYOUT**

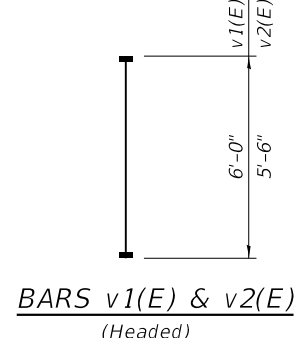
**BILL OF MATERIAL**

| Bar                               | No. | Size    | Length  | Shape |
|-----------------------------------|-----|---------|---------|-------|
| h(E)                              | 36  | #6      | 12'-2"  | —     |
| h1(E)                             | 8   | #6      | 20'-8"  | —     |
| h2(E)                             | 4   | #5      | 8'-11"  | —     |
| h3(E)                             | 4   | #5      | 3'-1"   | —     |
| h4(E)                             | 4   | #5      | 21'-4"  | —     |
| p(E)                              | 12  | #7      | 37'-0"  | —     |
| p1(E)                             | 12  | #7      | 31'-0"  | —     |
| s2(E)                             | 48  | #6      | 14'-4"  | U     |
| s3(E)                             | 52  | #5      | 4'-0"   | U     |
| s4(E)                             | 2   | #6      | 16'-9"  | U     |
| s5(E)                             | 4   | #6      | 7'-6"   | U     |
| s6(E)                             | 22  | #5      | 6'-0"   | U     |
| sp(E)                             | 13  | #4      | 2'-0"   | W     |
| u(E)                              | 10  | #6      | 12'-9"  | U     |
| v1(E)                             | 72  | #8      | 6'-0"   | I     |
| v2(E)                             | 69  | #8      | 5'-6"   | I     |
| v3(E)                             | 4   | #5      | 7'-3"   | I     |
| v4(E)                             | 7   | #5      | 11'-5"  | I     |
| v5                                | 24  | #5      | 2'-2"   | I     |
| v6(E)                             | 4   | #5      | 7'-6"   | I     |
| v7(E)                             | 7   | #5      | 11'-10" | I     |
| Structure Excavation              |     | Cu. Yd. | 186     |       |
| Concrete Structures               |     | Cu. Yd. | 36.2    |       |
| Reinforcement Bars                |     | Pound   | 55      |       |
| Reinforcement Bars, Epoxy Coated  |     | Pound   | 7,390   |       |
| Furnishing Steel Piles, HP 14x117 |     | Foot    | 429     |       |
| Driving Piles                     |     | Foot    | 429     |       |
| Pile Shoes                        |     | Each    | 13      |       |

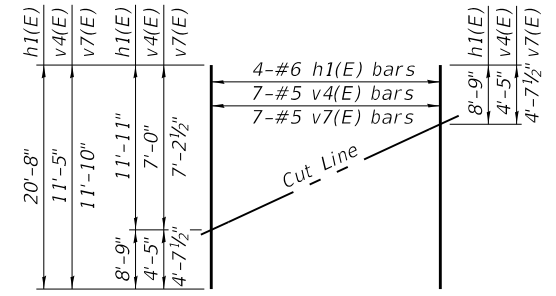
\* Length is height of spiral

**PILE DATA**

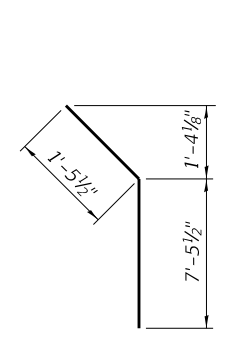
Type: HP14x117 with Pile Shoes  
 Nominal Required Bearing: 929 kips  
 Factored Resistance Available: 511 kips  
 Est. Length: 33 ft.  
 No. Production Piles: 13  
 No. Test Piles: 0



**BARS v1(E) & v2(E)**  
 (Headed)

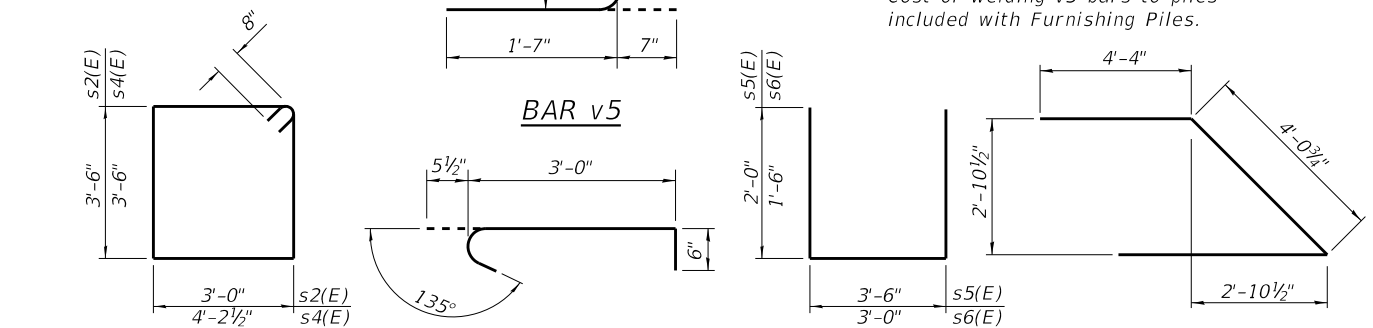


**FIELD CUTTING DIAGRAM**  
 Order h1(E), v4(E) and v7(E) full length. Cut as shown and use remainder of bars in opposite face.



**BAR h2(E)**

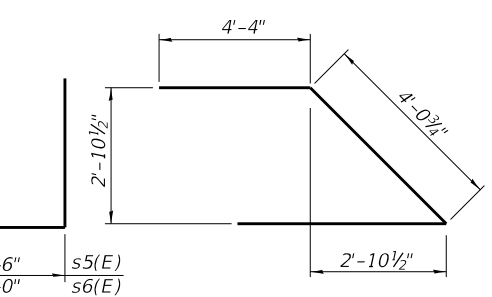
**PLAN**



**BARS s2(E) & s4(E)**      **BAR s3(E)**      **BARS s5(E) & s6(E)**      **BAR u(E)**

**PILE HEAD DETAIL**

\*\*Cost of welding v5 bars to piles included with Furnishing Piles.



MODEL: PLOT  
 FILE NAME: Y:\DOT11259-11\_78504\CADD\SP\_SN\_091-00777-007809100771-78504-23-NAbutSB.dgn  
 10/15/2019 12:33:27 PM



|                              |                   |       |           |
|------------------------------|-------------------|-------|-----------|
| USER NAME = kah              | DESIGNED - KJA    | 07/19 | REVISED - |
| ESCA PROJECT NO. 1259.11     | CHECKED - RTM/SHL | 07/19 | REVISED - |
| PLOT SCALE = 0.2" = 1' / in. | DRAWN - KAH       | 07/19 | REVISED - |
| PLOT DATE = 10/15/2019       | CHECKED - MTD/SHL | 08/19 | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**NORTH ABUTMENT (SB)  
 STRUCTURE NO. 091-0078 (SB)**

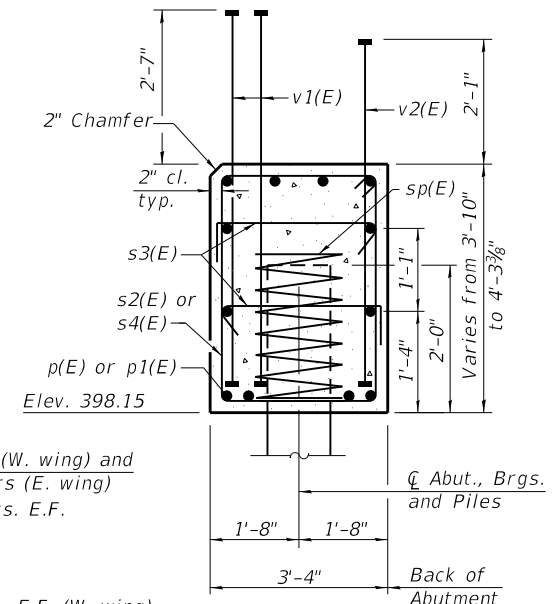
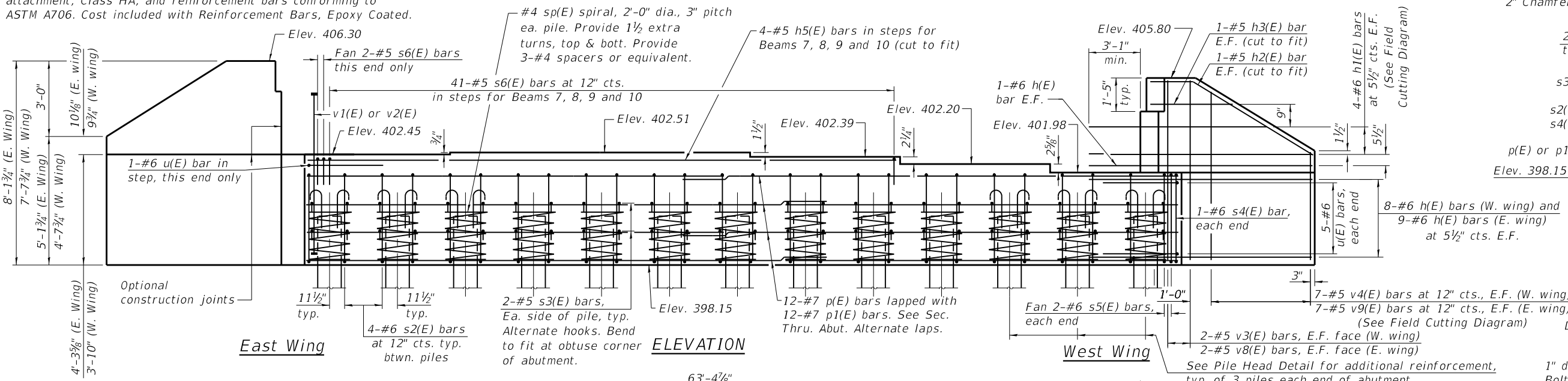
SHEET 23 OF 38 SHEETS

| F.A.I. RTE.        | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|-----------|--------|--------------|-----------|
| 57                 | (91-3)B-2 | UNION  | 161          | 89        |
| CONTRACT NO. 78504 |           |        |              |           |

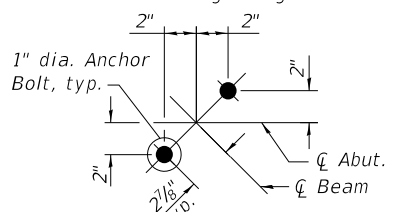
ILLINOIS FED. AID PROJECT

**Notes:**

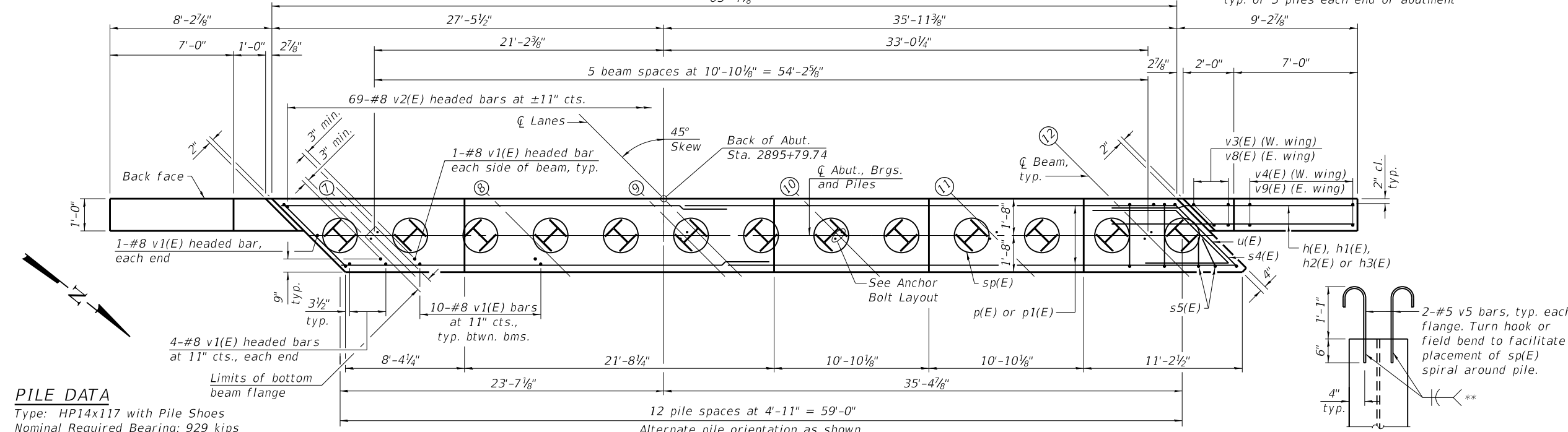
Pour steps monolithically with cap.  
 For details of piles see Sheet 31 of 38.  
 Space reinforcement in cap to miss anchor bolts.  
 Headed bars shall conform to ASTM A970 with Threaded attachment; Class HA; and reinforcement bars conforming to ASTM A706. Cost included with Reinforcement Bars, Epoxy Coated.



**SEC. THRU ABUT.**  
 Dimensions at right angles to abutment.



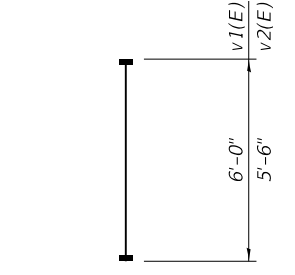
**ANCHOR BOLT LAYOUT**



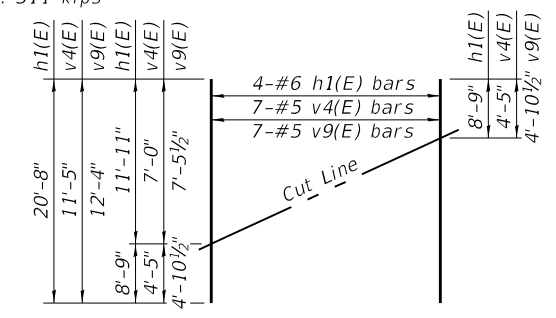
**PLAN**

**PILE DATA**

Type: HP14x117 with Pile Shoes  
 Nominal Required Bearing: 929 kips  
 Factored Resistance Available: 511 kips  
 Est. Length: 34 ft.  
 No. Production Piles: 13  
 No. Test Piles: 0

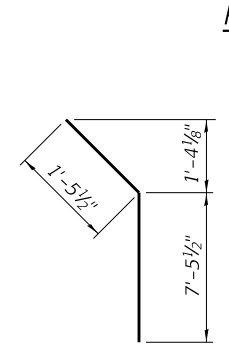


**BARS v1(E) & v2(E)**  
 (Headed)

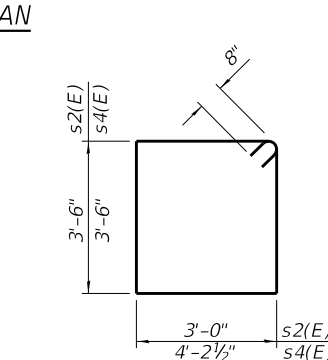


**FIELD CUTTING DIAGRAM**

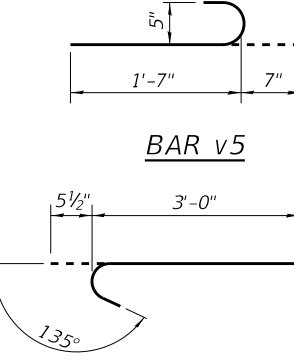
Order h1(E), v4(E) and v9(E) full length. Cut as shown and use remainder of bars in opposite face.



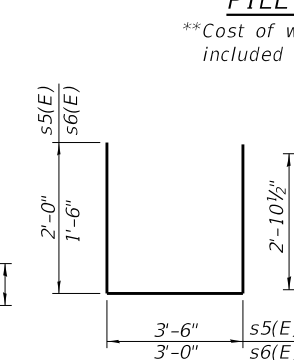
**BAR h2(E)**



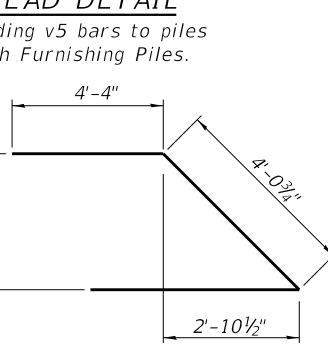
**BARS s2(E) & s4(E)**



**BAR s3(E)**



**BARS s5(E) & s6(E)**



**BAR u(E)**



**PILE HEAD DETAIL**  
 \*\*Cost of welding v5 bars to piles included with Furnishing Piles.

**BILL OF MATERIAL**

| Bar   | No. | Size | Length | Shape |
|-------|-----|------|--------|-------|
| h(E)  | 38  | #6   | 12'-2" | —     |
| h1(E) | 8   | #6   | 20'-8" | —     |
| h2(E) | 4   | #5   | 8'-11" | —     |
| h3(E) | 4   | #5   | 3'-1"  | —     |
| h5(E) | 4   | #5   | 43'-6" | —     |
| p(E)  | 12  | #7   | 37'-0" | —     |
| p1(E) | 12  | #7   | 31'-0" | —     |
| s2(E) | 48  | #6   | 14'-4" | U     |
| s3(E) | 52  | #5   | 4'-0"  | U     |
| s4(E) | 2   | #6   | 16'-9" | U     |
| s5(E) | 4   | #6   | 7'-6"  | U     |
| s6(E) | 43  | #5   | 6'-0"  | U     |
| sp(E) | 13  | #4   | 2'-0"  | WWW   |
| u(E)  | 11  | #6   | 12'-9" | U     |
| v1(E) | 72  | #8   | 6'-0"  | I     |
| v2(E) | 69  | #8   | 5'-6"  | I     |
| v3(E) | 4   | #5   | 7'-3"  | I     |
| v4(E) | 7   | #5   | 11'-5" | I     |
| v5    | 24  | #5   | 2'-2"  | I     |
| v8(E) | 4   | #5   | 7'-9"  | I     |
| v9(E) | 7   | #5   | 12'-4" | I     |

|                                   |         |       |
|-----------------------------------|---------|-------|
| Structure Excavation              | Cu. Yd. | 209   |
| Concrete Structures               | Cu. Yd. | 37.1  |
| Reinforcement Bars                | Pound   | 55    |
| Reinforcement Bars, Epoxy Coated  | Pound   | 7,670 |
| Furnishing Steel Piles, HP 14x117 | Foot    | 442   |
| Driving Piles                     | Foot    | 442   |
| Pile Shoes                        | Each    | 13    |

\* Length is height of spiral

MODEL: PLOT  
 FILE NAME: Y:\DOT1259-11\_78504\CADD\SP\_SN\_091-0077-0078\0910077-78504-24-5Abut5B.dgn



|                              |                   |       |           |
|------------------------------|-------------------|-------|-----------|
| USER NAME = kah              | DESIGNED - KJA    | 07/19 | REVISED - |
| ESCA PROJECT NO. 1259.11     | CHECKED - RTM/SHL | 07/19 | REVISED - |
| PLOT SCALE = 0:2" = 1' / in. | DRAWN - KAH       | 07/19 | REVISED - |
| PLOT DATE = 10/15/2019       | CHECKED - MTD/SHL | 08/19 | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SOUTH ABUTMENT (SB)  
 STRUCTURE NO. 091-0078 (SB)**

SHEET 24 OF 38 SHEETS

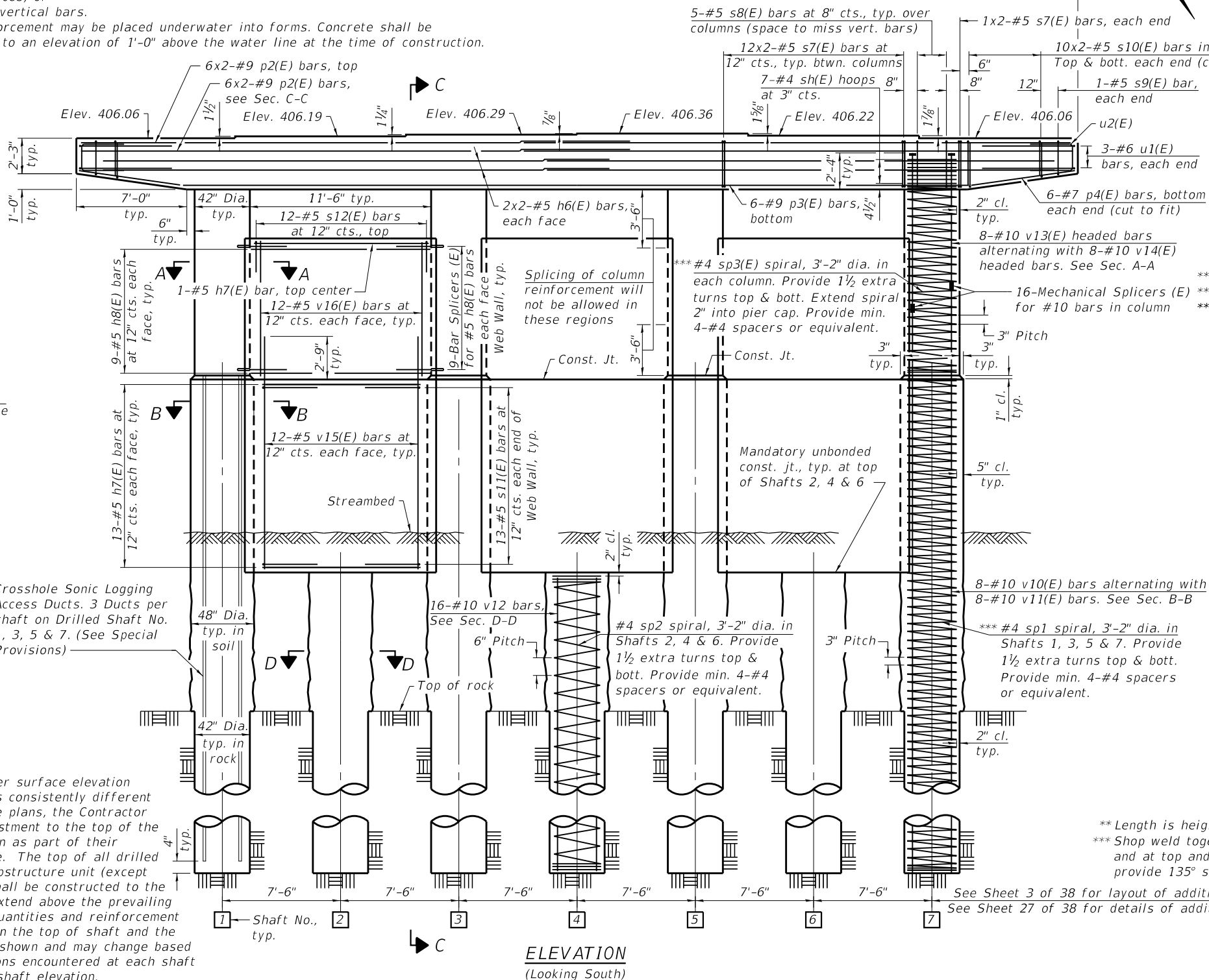
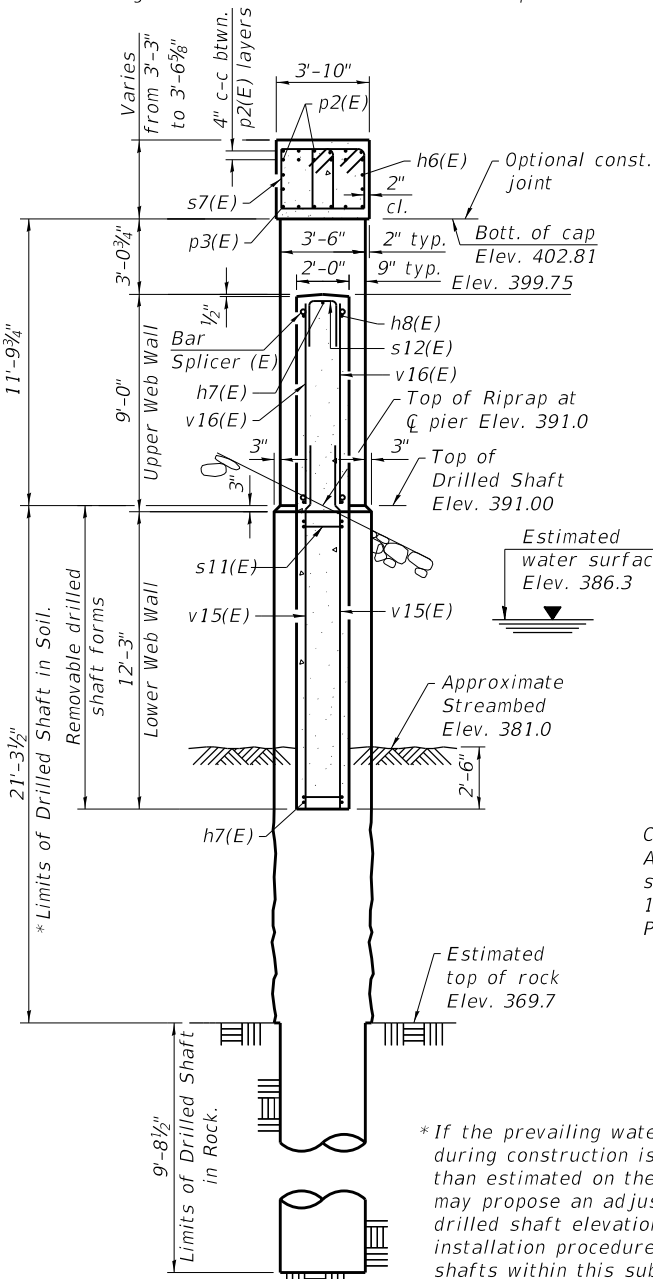
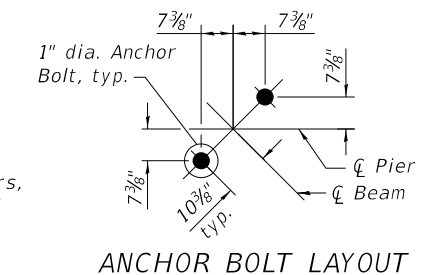
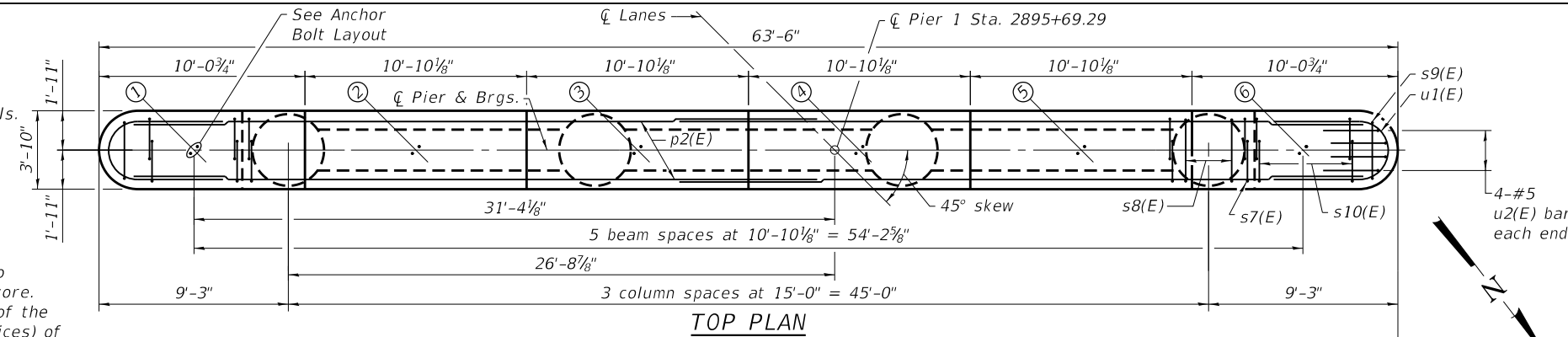
| F.A.I. RTE. | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|-----------|--------|--------------|-----------|
| 57          | (91-3)B-2 | UNION  | 161          | 90        |

CONTRACT NO. 78504

ILLINOIS FED. AID PROJECT

**Notes:**

Pour steps monolithically with cap.  
 Space reinforcement in cap to miss anchor bolts.  
 See Sheet 27 of 38 for Section A-A, Section B-B, Section D-D, Bar Bending Details and Construction Sequence for Web Walls.  
 See Sheet 32 of 38 for Bar Splicer and Mechanical Splicer details.  
 Headed bars shall conform to ASTM A970 with Threaded attachment; Class HA; and reinforcement bars conforming to ASTM A706. Cost included with Reinforcement Bars, Epoxy Coated.  
 Bars indicated thus 2x2-#5 etc. indicates 2 line of bars with 2 lengths per line.  
 When splicing of spiral reinforcement is necessary, the spirals shall be provided with 11#2 extra turns at the ends to be spliced. These additional turns shall either be welded together according to AWS D1.4, or shall both terminate with a 135° standard hook into core.  
 Seismic hoops (sh(E) bars) shall be placed in the cap at the top of the column reinforcement as detailed. The 135° hooks (or alternate splices) of the seismic hoops shall be staggered around the perimeter of the vertical bars.  
 If a portion of the drilled shaft web walls is under water, reinforcement may be placed underwater into forms. Concrete shall be tremied according to Article 503.08 of the Standard Specifications to an elevation of 1'-0" above the water line at the time of construction.



**BILL OF MATERIAL**

| Bar                                  | No. | Size    | Length  | Shape |
|--------------------------------------|-----|---------|---------|-------|
| h6(E)                                | 8   | #5      | 32'-1"  | —     |
| h7(E)                                | 81  | #5      | 10'-8"  | —     |
| h8(E)                                | 54  | #5      | 11'-6"  | —     |
| p2(E)                                | 24  | #9      | 34'-9"  | —     |
| p3(E)                                | 6   | #9      | 49'-6"  | —     |
| p4(E)                                | 12  | #7      | 6'-8"   | —     |
| sh(E)                                | 28  | #4      | 15'-8"  | ○     |
| sp1                                  | 4   | #4      | 30'-9"  | ⋈     |
| sp2                                  | 3   | #4      | 18'-2"  | ⋈     |
| sp3(E)                               | 4   | #4      | 11'-10" | ⋈     |
| s7(E)                                | 76  | #5      | 11'-3"  | □     |
| s8(E)                                | 20  | #5      | 13'-9"  | □     |
| s9(E)                                | 2   | #5      | 11'-7"  | □     |
| s10(E)                               | 80  | #5      | 7'-9"   | □     |
| s11(E)                               | 78  | #5      | 6'-8"   | □     |
| s12(E)                               | 36  | #5      | 5'-7"   | □     |
| u1(E)                                | 6   | #6      | 14'-0"  | U     |
| u2(E)                                | 8   | #5      | 7'-5"   | J     |
| v10(E)                               | 32  | #10     | 35'-0"  | —     |
| v11(E)                               | 32  | #10     | 37'-0"  | —     |
| v12                                  | 48  | #10     | 18'-4"  | —     |
| v13(E)                               | 32  | #10     | 10'-2"  | —     |
| v14(E)                               | 32  | #10     | 8'-2"   | —     |
| v15(E)                               | 72  | #5      | 14'-10" | —     |
| v16(E)                               | 72  | #5      | 8'-7"   | —     |
| Concrete Structures                  |     | Cu. Yd. | 99.8    |       |
| Reinforcement Bars                   |     | Pound   | 7,920   |       |
| Reinforcement Bars, Epoxy Coated     |     | Pound   | 24,440  |       |
| Structure Excavation                 |     | Cu. Yd. | 36      |       |
| Bar Splicers                         |     | Each    | 108     |       |
| Mechanical Splicers                  |     | Each    | 64      |       |
| Drilled Shaft in Soil                |     | Cu. Yd. | 51.9    |       |
| Drilled Shaft in Rock                |     | Cu. Yd. | 24.2    |       |
| Crosshole Sonic Logging Access Ducts |     | Foot    | 124     |       |
| Crosshole Sonic Logging Testing      |     | Each    | 4       |       |

\*If the prevailing water surface elevation during construction is consistently different than estimated on the plans, the Contractor may propose an adjustment to the top of the drilled shaft elevation as part of their installation procedure. The top of all drilled shafts within this substructure unit (except Shafts 2, 4 and 6) shall be constructed to the same elevation and extend above the prevailing water surface. The quantities and reinforcement detailing are based on the top of shaft and the estimated elevations shown and may change based on the actual elevations encountered at each shaft and the final top of shaft elevation.

\*\* Length is height of spiral.  
 \*\*\* Shop weld together extra spiral turns at top for sp1 and at top and bottom for sp3(E) per AWS D1.4, or provide 135° standard hooks into core at ends of spiral.

See Sheet 3 of 38 for layout of additional Drilled Shafts for slope stability.  
 See Sheet 27 of 38 for details of additional Drilled Shafts for slope stability.

MODEL: PLOT FILE NAME: Y:\DOT\1259-11\_78504\CADD\SP\_SN\_091-0077-0078\0910077-78504+25-Pier1NB.dgn



|                          |                         |           |
|--------------------------|-------------------------|-----------|
| USER NAME = kah          | DESIGNED - KJA 07/19    | REVISED - |
| ESCA PROJECT NO. 1259.11 | CHECKED - RTM/SHL 07/19 | REVISED - |
| PLOT SCALE = 0.2" / 1"   | DRAWN - KAH 07/19       | REVISED - |
| PLOT DATE = 11/4/2019    | CHECKED - MTD/SHL 08/19 | REVISED - |

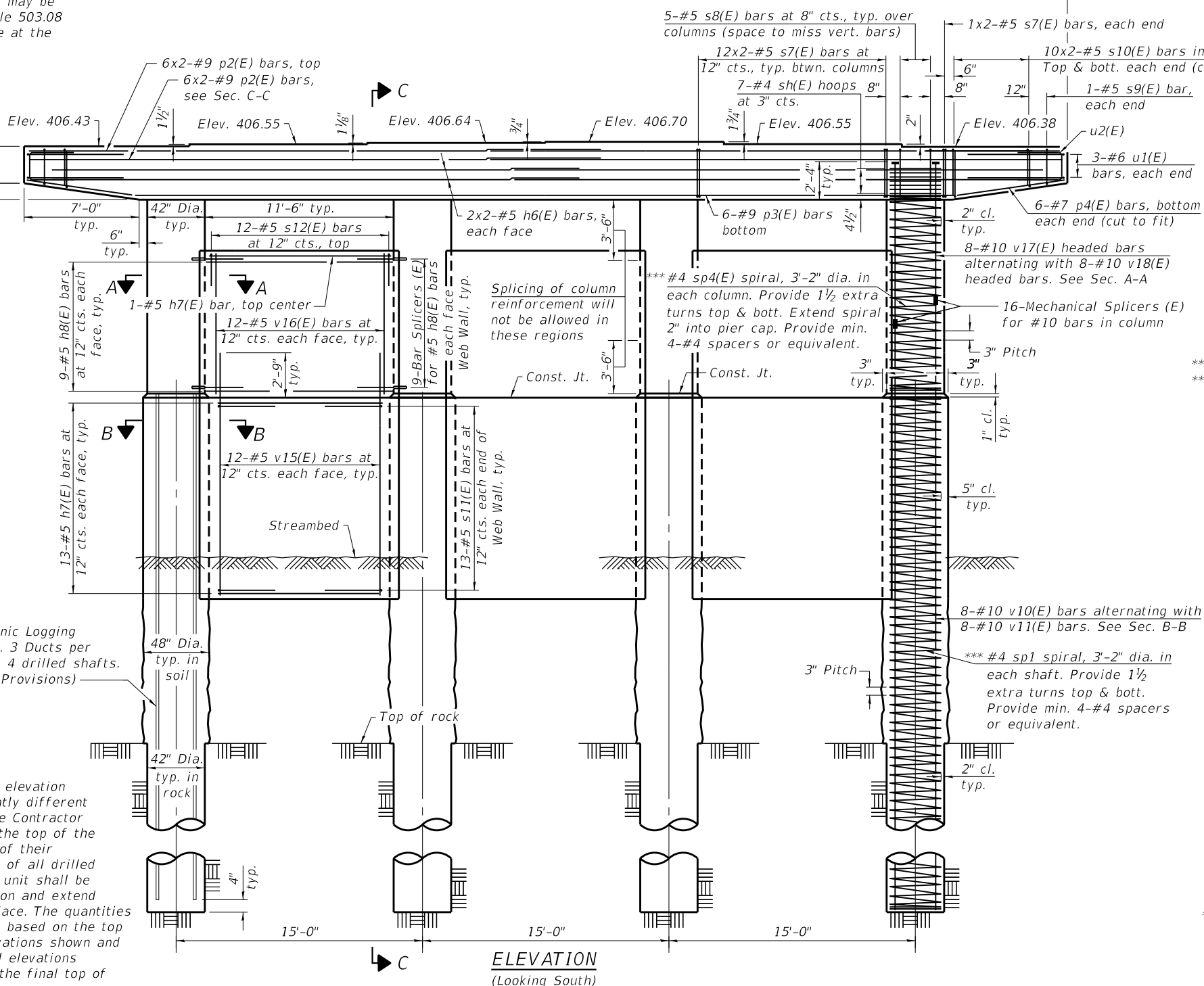
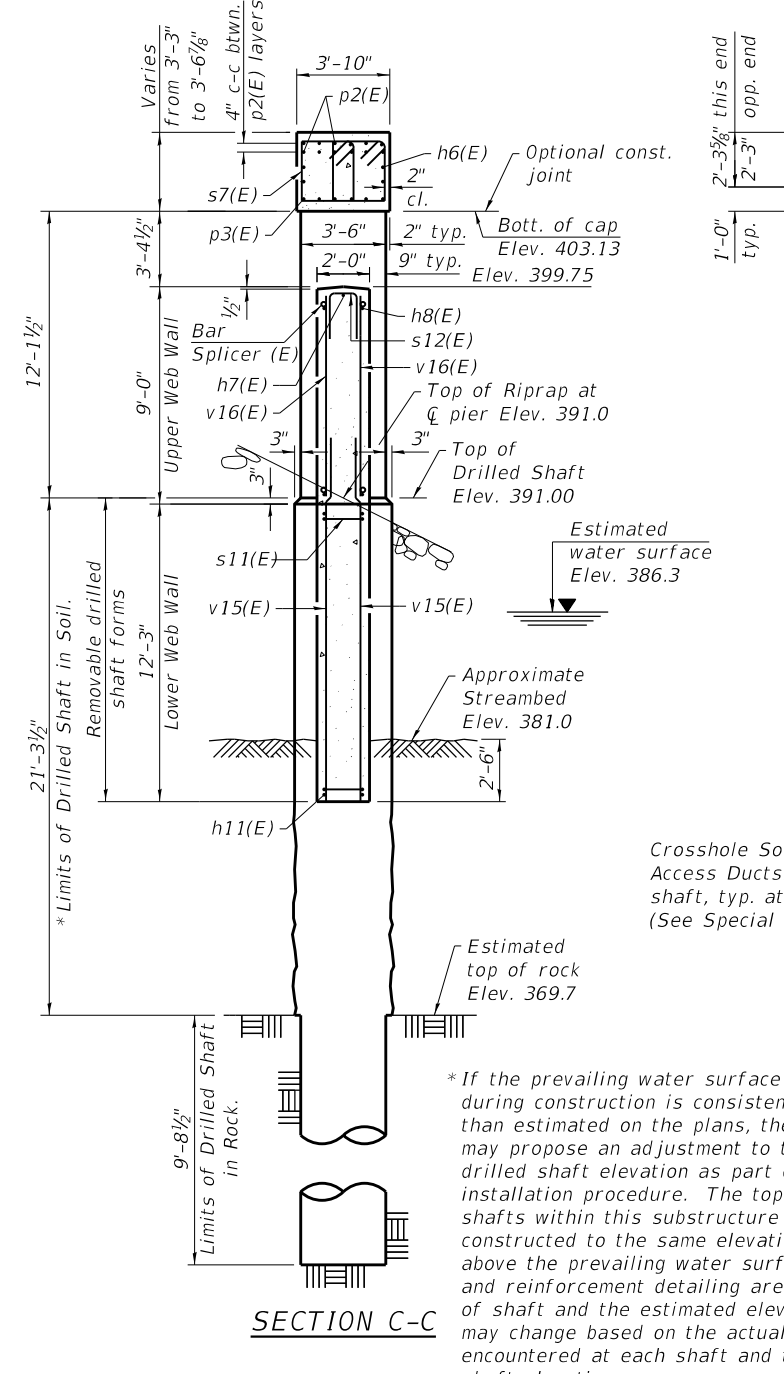
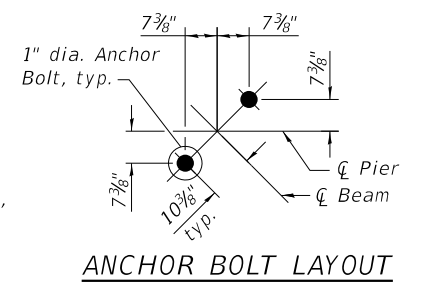
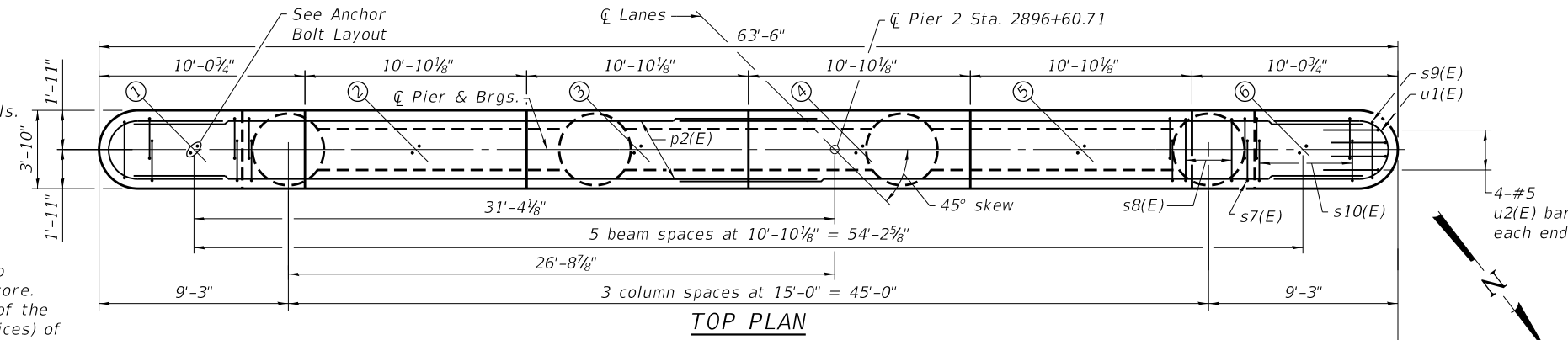
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**PIER 1 (NB)  
 STRUCTURE NO. 091-0077 (NB)**

| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 91        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |

**Notes:**

Pour steps monolithically with cap.  
 Space reinforcement in cap to miss anchor bolts.  
 See Sheet 27 of 38 for Section A-A, Section B-B, Bar Bending Details and Construction Sequence for Web Walls.  
 See Sheet 32 of 38 for Bar Splicer and Mechanical Splicer details.  
 Headed bars shall conform to ASTM A970 with Threaded attachment; Class HA; and reinforcement bars conforming to ASTM A706. Cost included with Reinforcement Bars, Epoxy Coated.  
 Bars indicated thus 2x2-#5 etc. indicates 2 line of bars with 2 lengths per line.  
 When splicing of spiral reinforcement is necessary, the spirals shall be provided with 1 1/2 extra turns at the ends to be spliced.  
 These additional turns shall either be welded together according to AWS D1.4, or shall both terminate with a 135° standard hook into core.  
 Seismic hoops (sh(E) bars) shall be placed in the cap at the top of the column reinforcement as detailed. The 135° hooks (or alternate splices) of the seismic hoops shall be staggered around the perimeter of the vertical bars.  
 If a portion of the drilled shaft web walls is under water, reinforcement may be placed underwater into forms. Concrete shall be tremied according to Article 503.08 of the Standard Specifications to an elevation of 1'-0" above the water line at the time of construction.



**BILL OF MATERIAL**

| Bar                                  | No.     | Size   | Length  | Shape |
|--------------------------------------|---------|--------|---------|-------|
| h6(E)                                | 8       | #5     | 32'-1"  | —     |
| h7(E)                                | 81      | #5     | 10'-8"  | —     |
| h8(E)                                | 54      | #5     | 11'-6"  | —     |
| p2(E)                                | 24      | #9     | 34'-9"  | —     |
| p3(E)                                | 6       | #9     | 49'-6"  | —     |
| p4(E)                                | 12      | #7     | 6'-8"   | —     |
| sh(E)                                | 28      | #4     | 15'-8"  | ⊙     |
| sp1                                  | 4       | #4     | 30'-9"  | ⋈     |
| sp4(E)                               | 4       | #4     | 12'-2"  | ⋈     |
| s7(E)                                | 76      | #5     | 11'-3"  | ⊠     |
| s8(E)                                | 20      | #5     | 13'-9"  | ⊠     |
| s9(E)                                | 2       | #5     | 11'-7"  | ⊠     |
| s10(E)                               | 80      | #5     | 7'-9"   | ⊠     |
| s11(E)                               | 78      | #5     | 6'-8"   | ⊠     |
| s12(E)                               | 36      | #5     | 5'-7"   | ⊠     |
| u1(E)                                | 6       | #6     | 14'-0"  | U     |
| u2(E)                                | 8       | #5     | 7'-5"   | J     |
| v10(E)                               | 32      | #10    | 35'-0"  | —     |
| v11(E)                               | 32      | #10    | 37'-0"  | —     |
| v15(E)                               | 72      | #5     | 14'-10" | —     |
| v16(E)                               | 72      | #5     | 8'-7"   | —     |
| v17(E)                               | 32      | #10    | 10'-6"  | —     |
| v18(E)                               | 32      | #10    | 8'-6"   | —     |
| Concrete Structures                  | Cu. Yd. | 100.5  |         |       |
| Reinforcement Bars                   | Pound   | 3,350  |         |       |
| Reinforcement Bars, Epoxy Coated     | Pound   | 24,570 |         |       |
| Structure Excavation                 | Cu. Yd. | 36     |         |       |
| Bar Splicers                         | Each    | 108    |         |       |
| Mechanical Splicers                  | Each    | 64     |         |       |
| Drilled Shaft in Soil                | Cu. Yd. | 39.6   |         |       |
| Drilled Shaft in Rock                | Cu. Yd. | 13.8   |         |       |
| Crosshole Sonic Logging Access Ducts | Foot    | 124    |         |       |
| Crosshole Sonic Logging Testing      | Each    | 4      |         |       |

\*\* Length is height of spiral.  
 \*\*\* Shop weld together extra spiral turns at top for sp1 and at top and bottom for sp4(E) per AWS D1.4, or provide 135° standard hooks into core at ends of spiral.

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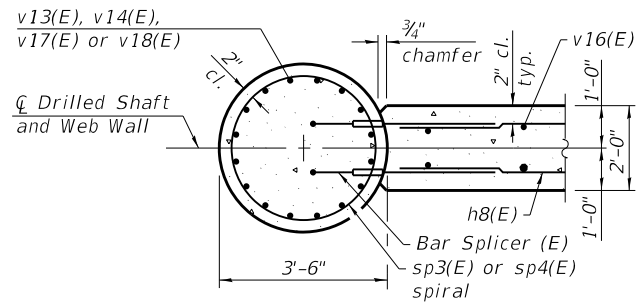
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| USER NAME = kah            | DESIGNED - KJA    | 07/19 | REVISED - |
| ESCA PROJECT NO. 1259.11   | CHECKED - RTM/SHL | 07/19 | REVISED - |
| PLOT SCALE = 0.1667' / in. | DRAWN - KAH       | 07/19 | REVISED - |
| PLOT DATE = 11/4/2019      | CHECKED - MTD/SHL | 08/19 | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

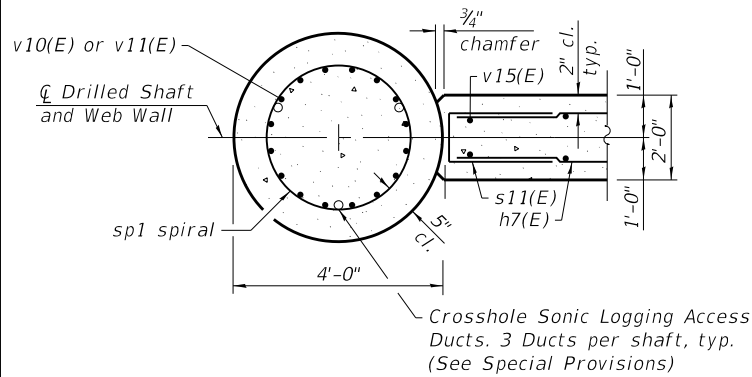
**PIER 2 (NB)  
 STRUCTURE NO. 091-0077 (NB)**

SHEET 26 OF 38 SHEETS

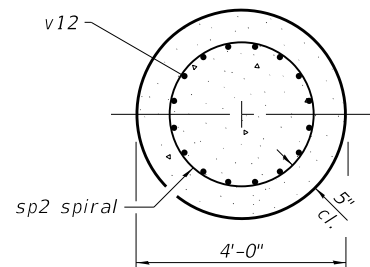
| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 92        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |



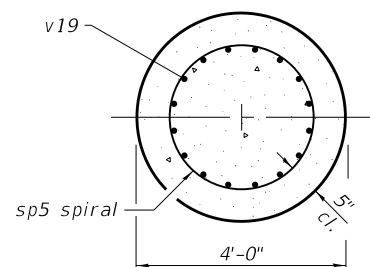
SECTION A-A



SECTION B-B



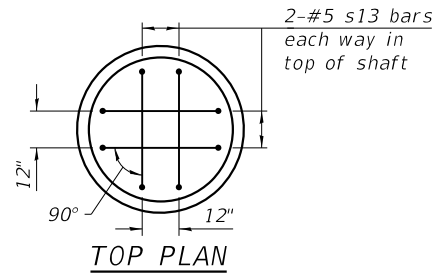
SECTION D-D



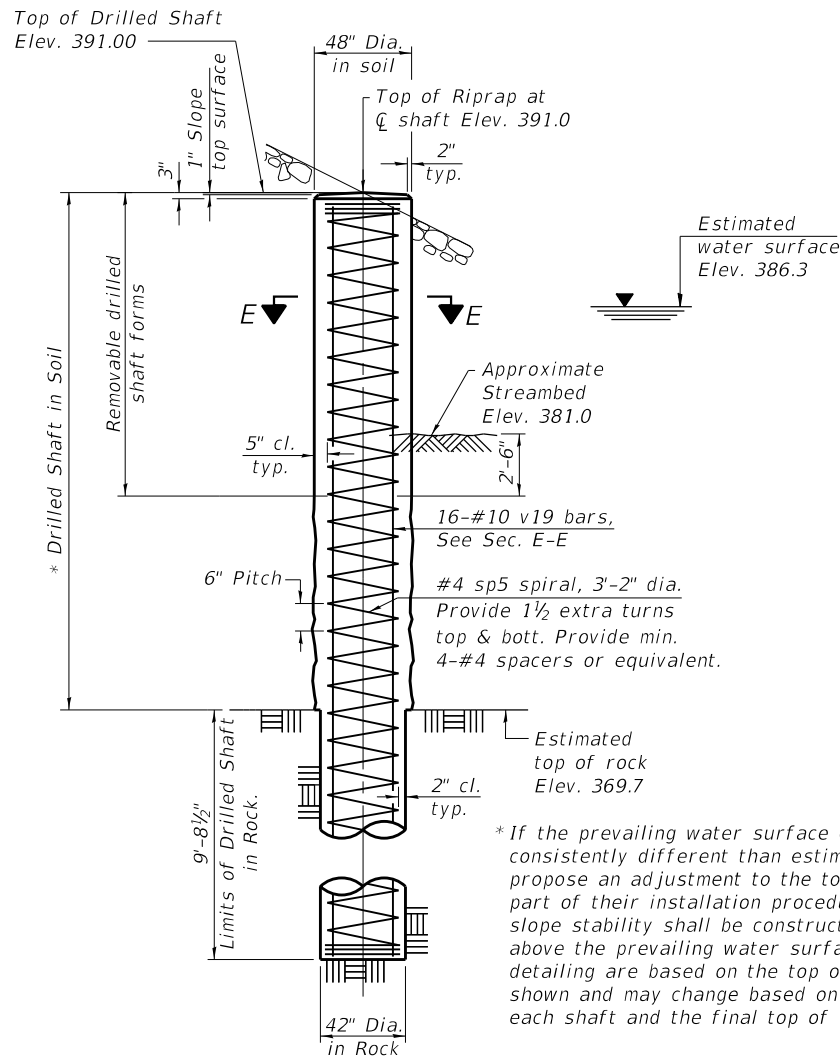
SECTION E-E

**CONSTRUCTION SEQUENCE FOR WEB WALL**

1. Excavate between shafts to elevation of web wall base and set lower web wall forms through water to bear on the circular edge of drilled shafts. Secure in place with fill, struts or tie forms together as required.
2. Place the lower web wall reinforcement cage into the forms using spacers to maintain proper clearances.
3. If the forms can be sealed against the shafts and streambed to allow dewatering, the reinforcement and the concrete placement may be completed in the dry. Alternatively, the rebar cage can be lowered into position through water and the concrete discharged at the base of the excavation through a tremie pipe or pump hose, displacing water, sediment, and tainted concrete out the top of the forms.
4. Construct columns.
5. Construct upper web walls.



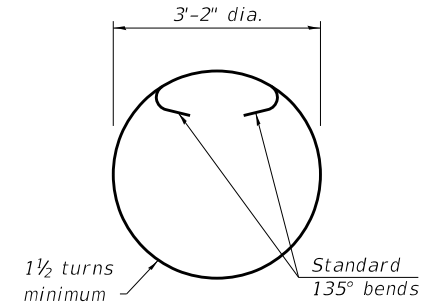
TOP PLAN



TYPICAL DRILLED SHAFT FOR SLOPE STABILITY

See Sheet 3 of 38 for layout of Drilled Shafts for slope stability.

\* If the prevailing water surface elevation during construction is consistently different than estimated on the plans, the Contractor may propose an adjustment to the top of the drilled shaft elevation as part of their installation procedure. The top of all drilled shafts for slope stability shall be constructed to the same elevation and extend above the prevailing water surface. The quantities and reinforcement detailing are based on the top of shaft and the estimated elevations shown and may change based on the actual elevations encountered at each shaft and the final top of shaft elevation.



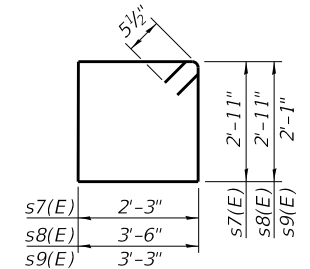
BAR sh(E)  
(Seismic Hoop)

Mechanically spliced or shop welded (per AWS D1.4) seismic hoop alternates will be permitted at no additional cost to the Department.

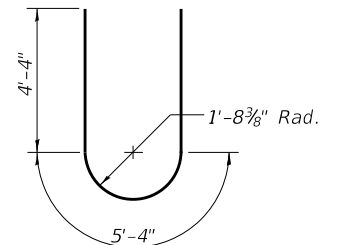
**A & B DIMENSIONS**

| Bar    | A         | B     |
|--------|-----------|-------|
| s10(E) | 2'-3"     | 2'-9" |
| s11(E) | 1'-8"     | 2'-6" |
| s12(E) | 1'-6 3/4" | 2'-0" |
| s13    | 2'-11"    | 2'-0" |
| u2(E)  | 1'-11"    | 2'-9" |

BARS s10(E) THRU s13 & u2(E)



BARS s7(E), s8(E) & s9(E)



BAR u1(E)

**L DIMENSIONS**

| Bar    | L      |
|--------|--------|
| v13(E) | 10'-2" |
| v14(E) | 8'-2"  |
| v17(E) | 10'-6" |
| v18(E) | 8'-6"  |

BARS v13(E), v14(E), v17(E) & v18(E)  
(Headed)

Headed bars shall conform to ASTM A970 with Threaded attachment; Class HA; and reinforcement bars conforming to ASTM A706. Cost included with Reinforcement Bars, Epoxy Coated.

**EIGHT DRILLED SHAFTS FOR SLOPE STABILITY BILL OF MATERIAL**

| Bar                   | No. | Size | Length  | Shape  |
|-----------------------|-----|------|---------|--------|
| ** sp5                | 8   | #4   | 30'-8"  | ⋈      |
| s13                   | 32  | #5   | 6'-11"  | □      |
| v19                   | 128 | #10  | 30'-10" | —      |
| Reinforcement Bars    |     |      | Pound   | 20,630 |
| Drilled Shaft in Soil |     |      | Cu. Yd. | 79.2   |
| Drilled Shaft in Rock |     |      | Cu. Yd. | 27.7   |

\*\* Length is height of spiral.

MODEL: PLOT  
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USER NAME = kah  
ESCA PROJECT NO. 1259.11  
PLOT SCALE = 0.1667' / in.  
PLOT DATE = 11/4/2019

DESIGNED - KJA 07/19  
CHECKED - RTM/SHL 07/19  
DRAWN - KAH 07/19  
CHECKED - MTD/SHL 08/19

REVISED -  
REVISED -  
REVISED -  
REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

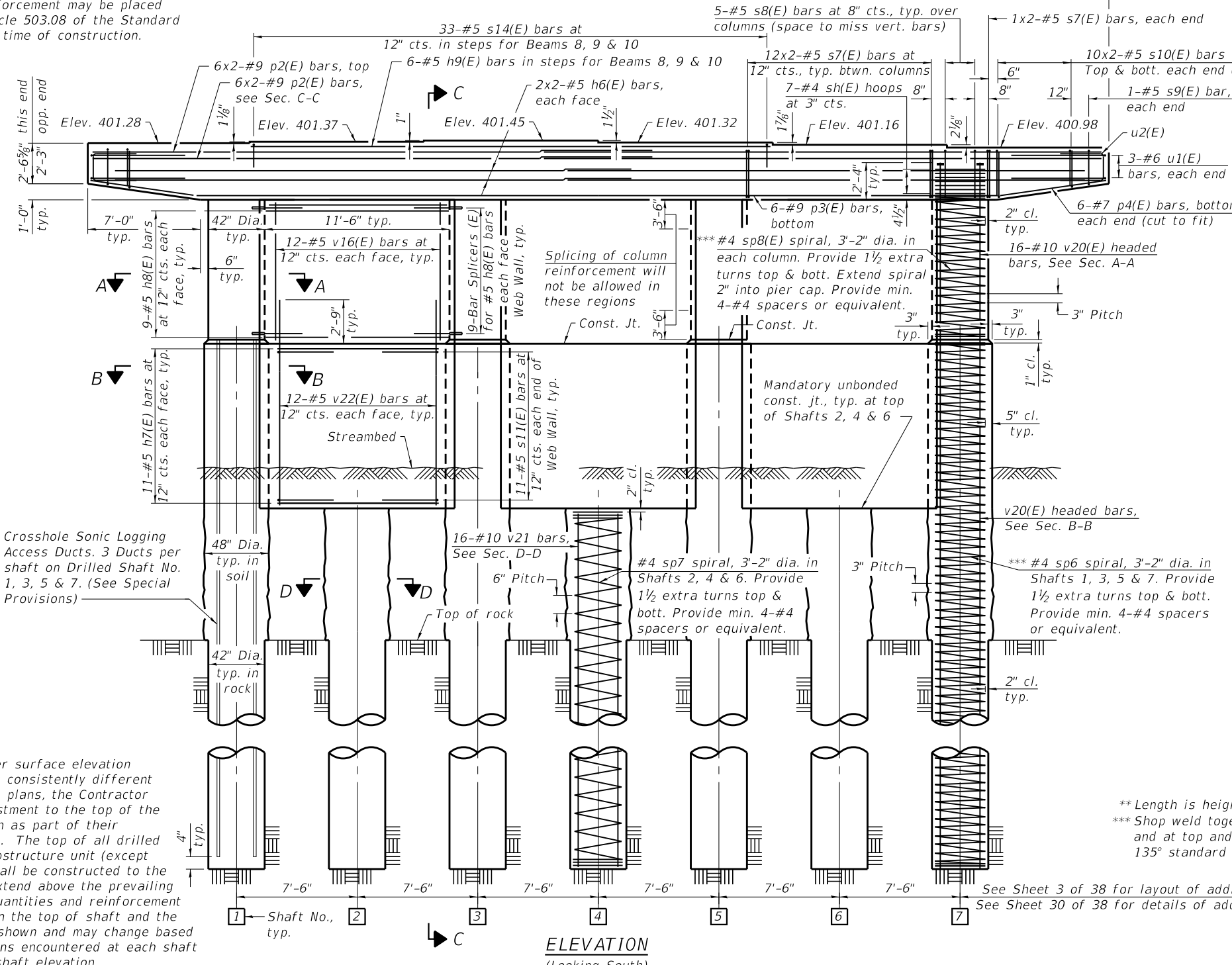
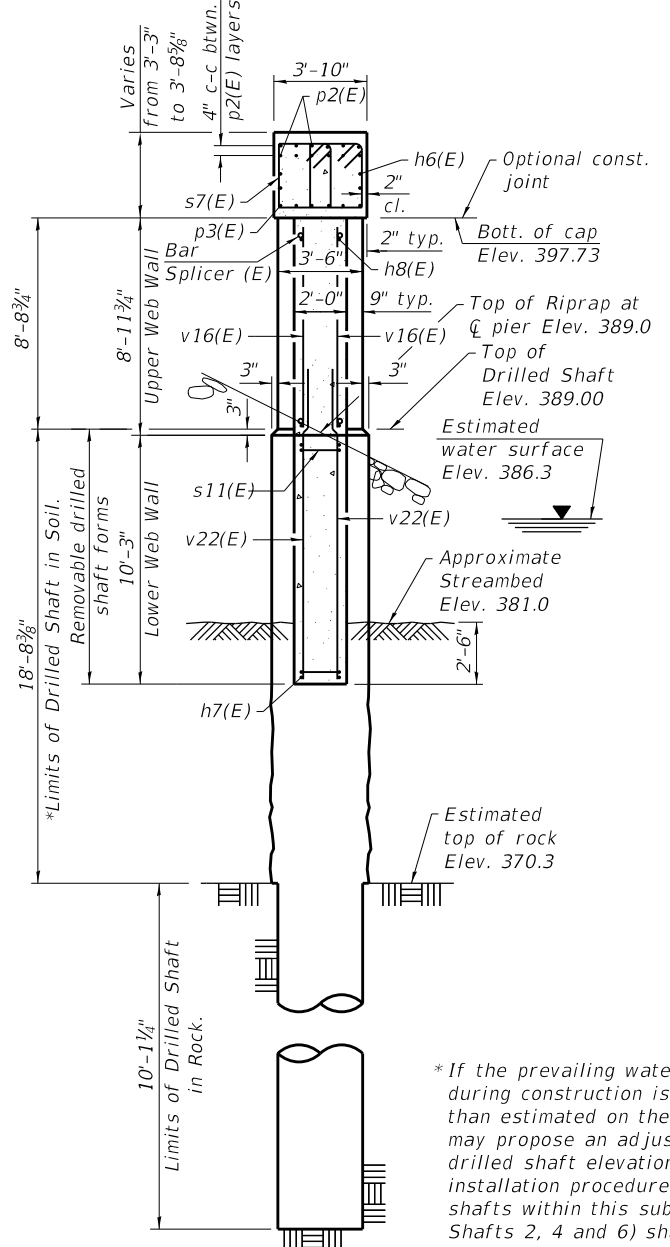
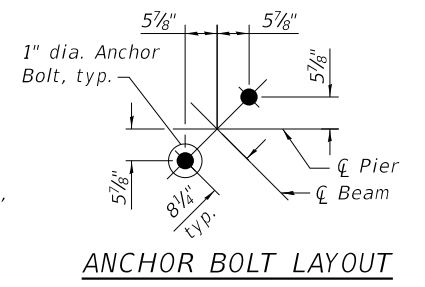
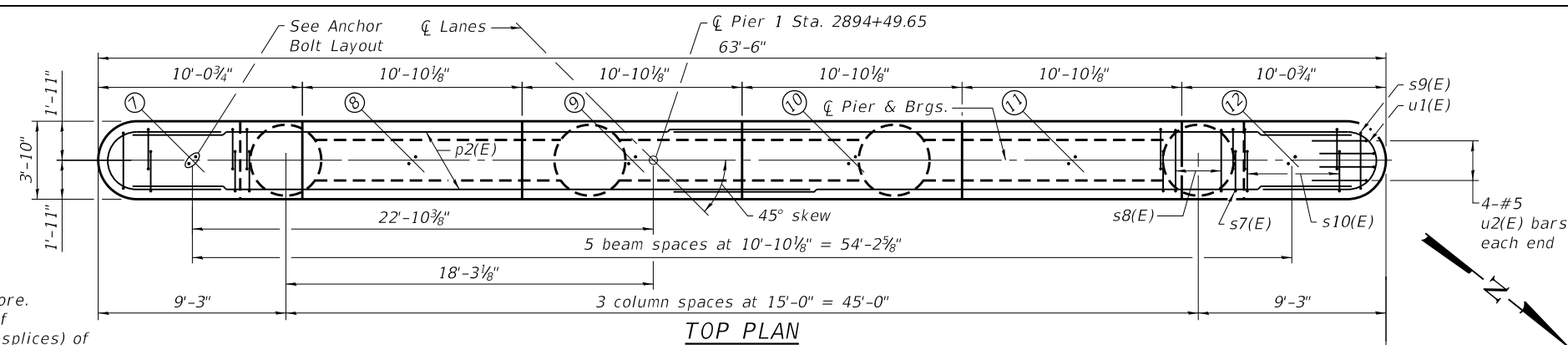
PIER DETAILS (NB)  
STRUCTURE NO. 091-0077 (NB)

SHEET 27 OF 38 SHEETS

| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 93        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |



Notes:  
 Pour steps monolithically with cap.  
 Space reinforcement in cap to miss anchor bolts.  
 See Sheet 30 of 38 for Section A-A, Section B-B, Section D-D,  
 Bar Bending Details and Construction Sequence for Web Walls.  
 See Sheet 32 of 38 for Bar Splicer details.  
 Headed bars shall conform to ASTM A970 with Threaded  
 attachment; Class HA; and reinforcement bars conforming to  
 ASTM A706. Cost included with Reinforcement Bars, Epoxy Coated.  
 Bars indicated thus 2x2-#5 etc. indicates 2 line of bars with 2  
 lengths per line.  
 When splicing of spiral reinforcement is necessary, the spirals  
 shall be provided with 1½ extra turns at the ends to be spliced.  
 These additional turns shall either be welded together according to  
 AWS D1.4, or shall both terminate with a 135° standard hook into core.  
 Seismic hoops (sh(E) bars) shall be placed in the cap at the top of  
 the column reinforcement as detailed. The 135° hooks (or alternate splices) of  
 the seismic hoops shall be staggered around the perimeter of the vertical bars.  
 If a portion of the drilled shaft web walls is under water, reinforcement may be placed  
 underwater into forms. Concrete shall be tremied according to Article 503.08 of the Standard  
 Specifications to an elevation of 1'-0" above the water line at the time of construction.



| BILL OF MATERIAL                     |     |         |         |       |
|--------------------------------------|-----|---------|---------|-------|
| Bar                                  | No. | Size    | Length  | Shape |
| h6(E)                                | 8   | #5      | 32'-1"  | —     |
| h7(E)                                | 66  | #5      | 10'-8"  | —     |
| h8(E)                                | 54  | #5      | 11'-6"  | —     |
| h9(E)                                | 6   | #5      | 32'-2"  | —     |
| p2(E)                                | 24  | #9      | 34'-9"  | —     |
| p3(E)                                | 6   | #9      | 49'-6"  | —     |
| p4(E)                                | 12  | #7      | 6'-8"   | —     |
| sh(E)                                | 28  | #4      | 15'-8"  | ○     |
| sp6                                  | 4   | #4      | 28'-6"  | ⋈     |
| sp7                                  | 3   | #4      | 17'-11" | ⋈     |
| sp8(E)                               | 4   | #4      | 8'-9"   | ⋈     |
| s7(E)                                | 76  | #5      | 11'-3"  | □     |
| s8(E)                                | 20  | #5      | 13'-9"  | □     |
| s9(E)                                | 2   | #5      | 11'-7"  | □     |
| s10(E)                               | 80  | #5      | 7'-9"   | □     |
| s11(E)                               | 66  | #5      | 6'-8"   | □     |
| s14(E)                               | 33  | #5      | 6'-6"   | □     |
| u1(E)                                | 6   | #6      | 14'-0"  | U     |
| u2(E)                                | 8   | #5      | 7'-5"   | U     |
| v16(E)                               | 72  | #5      | 8'-7"   | —     |
| v20(E)                               | 64  | #10     | 39'-11" | —     |
| v21                                  | 48  | #10     | 18'-1"  | —     |
| v22(E)                               | 72  | #5      | 12'-10" | —     |
| Concrete Structures                  |     | Cu. Yd. | 91.7    |       |
| Reinforcement Bars                   |     | Pound   | 7,620   |       |
| Reinforcement Bars, Epoxy Coated     |     | Pound   | 22,480  |       |
| Structure Excavation                 |     | Cu. Yd. | 36      |       |
| Bar Splicers                         |     | Each    | 108     |       |
| Drilled Shaft in Soil                |     | Cu. Yd. | 46.2    |       |
| Drilled Shaft in Rock                |     | Cu. Yd. | 25.2    |       |
| Crosshole Sonic Logging Access Ducts |     | Foot    | 116     |       |
| Crosshole Sonic Logging Testing      |     | Each    | 4       |       |

\*If the prevailing water surface elevation during construction is consistently different than estimated on the plans, the Contractor may propose an adjustment to the top of the drilled shaft elevation as part of their installation procedure. The top of all drilled shafts within this substructure unit (except Shafts 2, 4 and 6) shall be constructed to the same elevation and extend above the prevailing water surface. The quantities and reinforcement detailing are based on the top of shaft and the estimated elevations shown and may change based on the actual elevations encountered at each shaft and the final top of shaft elevation.

\*\* Length is height of spiral.  
 \*\*\* Shop weld together extra spiral turns at top for sp6 and at top and bottom for sp8(E) per AWS D1.4, or provide 135° standard hooks into core at ends of spiral.

See Sheet 3 of 38 for layout of additional Drilled Shafts for slope stability.  
 See Sheet 30 of 38 for details of additional Drilled Shafts for slope stability.

MODEL: PLOT FILE NAME: Y:\IDOT\1259-11\_78504\CADD\SP\_SN\_091-00177-0078\0910077-78504-28-Pier1SB.dgn



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 ESCA PROJECT NO. 1259.11  
 PLOT SCALE = 0.1667' / in.  
 PLOT DATE = 11/4/2019

DESIGNED - KJA 07/19  
 CHECKED - RTM/SHL 07/19  
 DRAWN - KAH 07/19  
 CHECKED - MTD/SHL 08/19

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

PIER 1 (SB)  
 STRUCTURE NO. 091-0078 (SB)

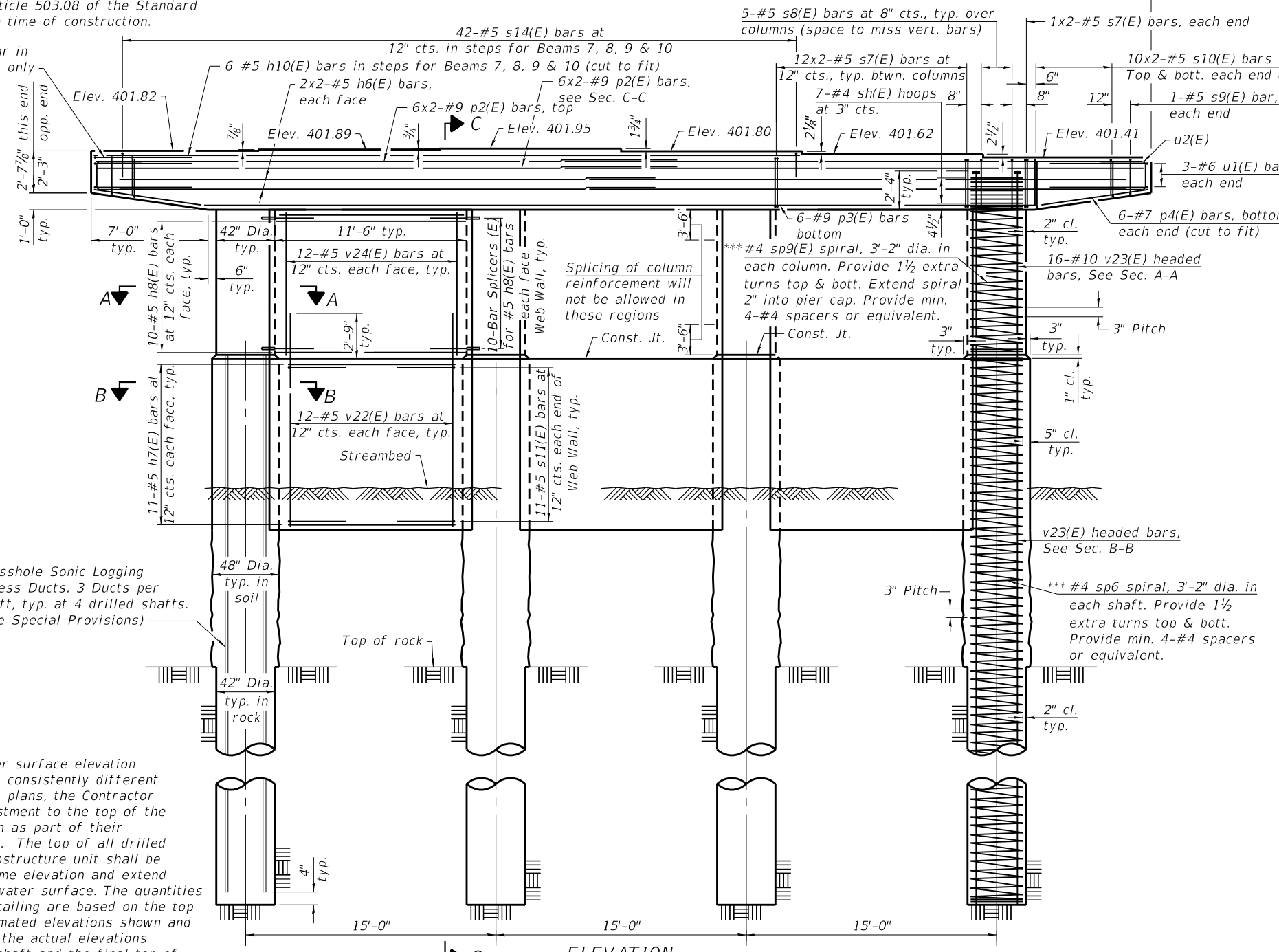
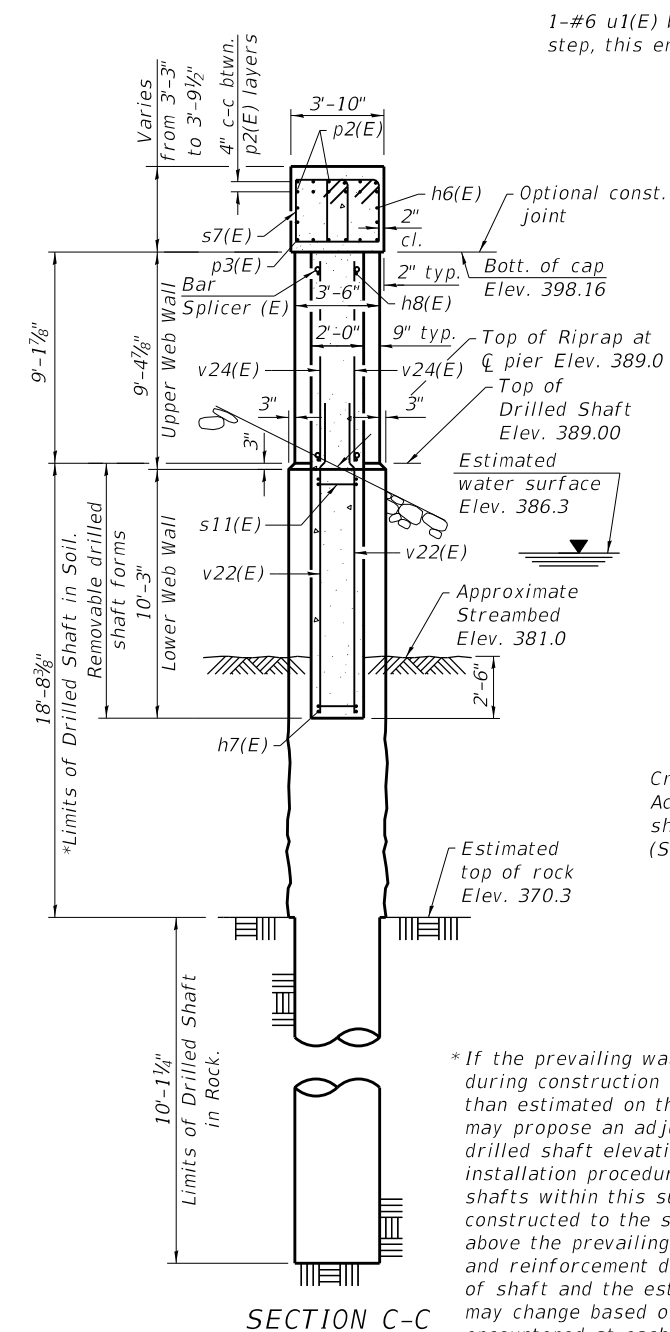
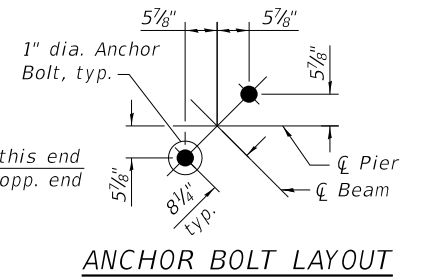
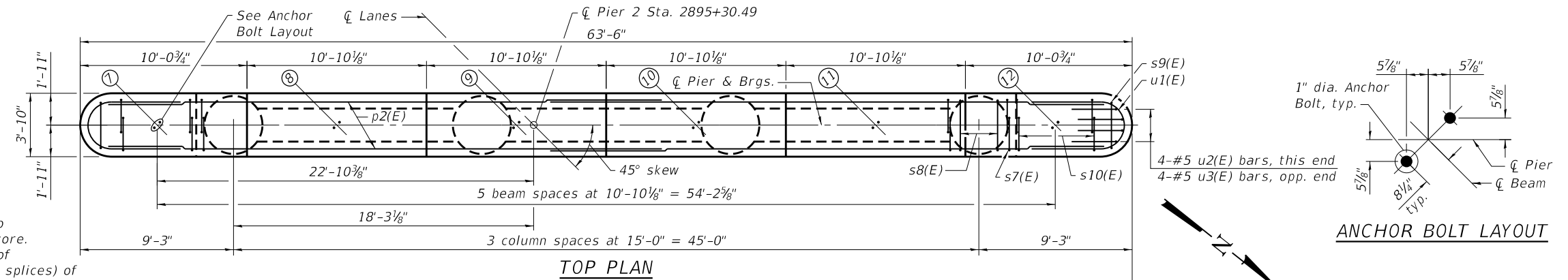
SHEET 28 OF 38 SHEETS

| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 94        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |



**Notes:**

Pour steps monolithically with cap.  
 Space reinforcement in cap to miss anchor bolts.  
 See Sheet 30 of 38 for Section A-A, Section B-B, Bar Bending Details and Construction Sequence for Web Walls.  
 See Sheet 32 of 38 for Bar Splicer details.  
 Headed bars shall conform to ASTM A970 with Threaded attachment; Class HA; and reinforcement bars conforming to ASTM A706. Cost included with Reinforcement Bars, Epoxy Coated.  
 Bars indicated thus 2x2-#5 etc. indicates 2 line of bars with 2 lengths per line.  
 When splicing of spiral reinforcement is necessary, the spirals shall be provided with 1½ extra turns at the ends to be spliced.  
 These additional turns shall either be welded together according to AWS D1.4, or shall both terminate with a 135° standard hook into core.  
 Seismic hoops (sh(E) bars) shall be placed in the cap at the top of the column reinforcement as detailed. The 135° hooks (or alternate splices) of the seismic hoops shall be staggered around the perimeter of the vertical bars.  
 If a portion of the drilled shaft web walls is under water, reinforcement may be placed underwater into forms. Concrete shall be tremied according to Article 503.08 of the Standard Specifications to an elevation of 1'-0" above the water line at the time of construction.



**BILL OF MATERIAL**

| Bar                                  | No.     | Size   | Length  | Shape |
|--------------------------------------|---------|--------|---------|-------|
| h6(E)                                | 8       | #5     | 32'-1"  | —     |
| h7(E)                                | 66      | #5     | 10'-8"  | —     |
| h8(E)                                | 60      | #5     | 11'-6"  | —     |
| h10(E)                               | 6       | #5     | 42'-2"  | —     |
| p2(E)                                | 24      | #9     | 34'-9"  | —     |
| p3(E)                                | 6       | #9     | 49'-6"  | —     |
| p4(E)                                | 12      | #7     | 6'-8"   | —     |
| sh(E)                                | 28      | #4     | 15'-8"  | ○     |
| sp6                                  | 4       | #4     | 28'-6"  | ⋈     |
| sp9(E)                               | 4       | #4     | 9'-2"   | ⋈     |
| s7(E)                                | 76      | #5     | 11'-3"  | □     |
| s8(E)                                | 20      | #5     | 13'-9"  | □     |
| s9(E)                                | 2       | #5     | 11'-7"  | □     |
| s10(E)                               | 80      | #5     | 7'-9"   | □     |
| s11(E)                               | 66      | #5     | 6'-8"   | □     |
| s14(E)                               | 42      | #5     | 6'-6"   | □     |
| u1(E)                                | 7       | #6     | 14'-0"  | U     |
| u2(E)                                | 4       | #5     | 7'-5"   | U     |
| u3(E)                                | 4       | #5     | 7'-9"   | U     |
| v22(E)                               | 72      | #5     | 12'-10" | —     |
| v23(E)                               | 64      | #10    | 40'-4"  | —     |
| v24(E)                               | 72      | #5     | 9'-0"   | —     |
| Concrete Structures                  | Cu. Yd. | 94.0   |         |       |
| Reinforcement Bars                   | Pound   | 3,110  |         |       |
| Reinforcement Bars, Epoxy Coated     | Pound   | 22,890 |         |       |
| Structure Excavation                 | Cu. Yd. | 36     |         |       |
| Bar Splicers                         | Each    | 120    |         |       |
| Drilled Shaft in Soil                | Cu. Yd. | 34.8   |         |       |
| Drilled Shaft in Rock                | Cu. Yd. | 14.4   |         |       |
| Crosshole Sonic Logging Access Ducts | Foot    | 116    |         |       |
| Crosshole Sonic Logging Testing      | Each    | 4      |         |       |

\*If the prevailing water surface elevation during construction is consistently different than estimated on the plans, the Contractor may propose an adjustment to the top of the drilled shaft elevation as part of their installation procedure. The top of all drilled shafts within this substructure unit shall be constructed to the same elevation and extend above the prevailing water surface. The quantities and reinforcement detailing are based on the top of shaft and the estimated elevations shown and may change based on the actual elevations encountered at each shaft and the final top of shaft elevation.

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 CHECKED - RTM/SHL 07/19  
 DRAWN - KAH 07/19  
 CHECKED - MTD/SHL 08/19

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

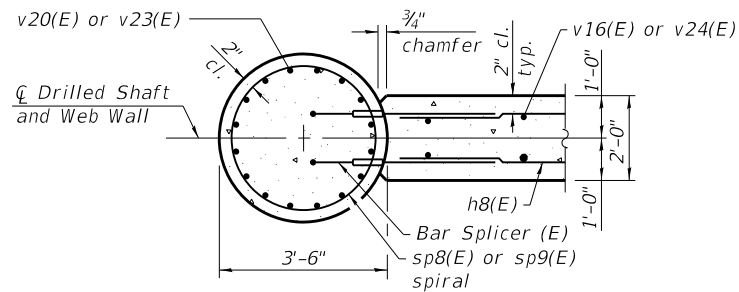
**PIER 2 (SB)  
 STRUCTURE NO. 091-0078 (SB)**

SHEET 29 OF 38 SHEETS

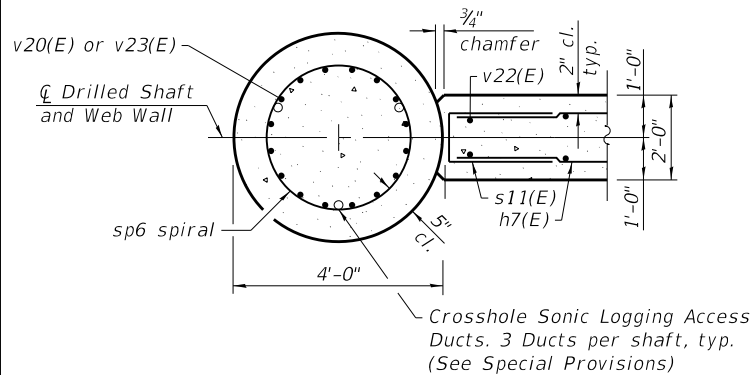
| F.A.I. RTE. | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|-----------|--------|--------------|-----------|
| 57          | (91-3)B-2 | UNION  | 161          | 95        |

CONTRACT NO. 78504

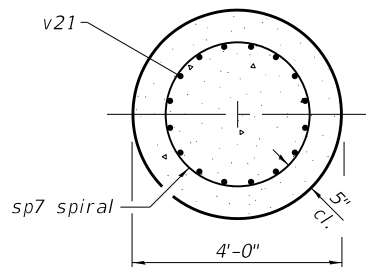
ILLINOIS FED. AID PROJECT



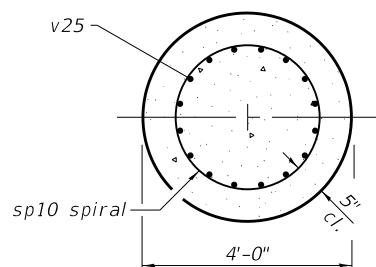
SECTION A-A



SECTION B-B



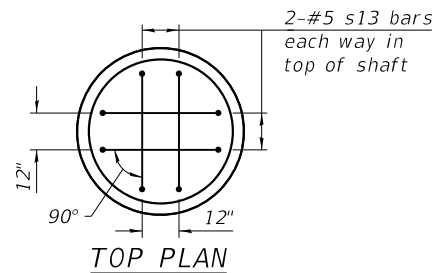
SECTION D-D



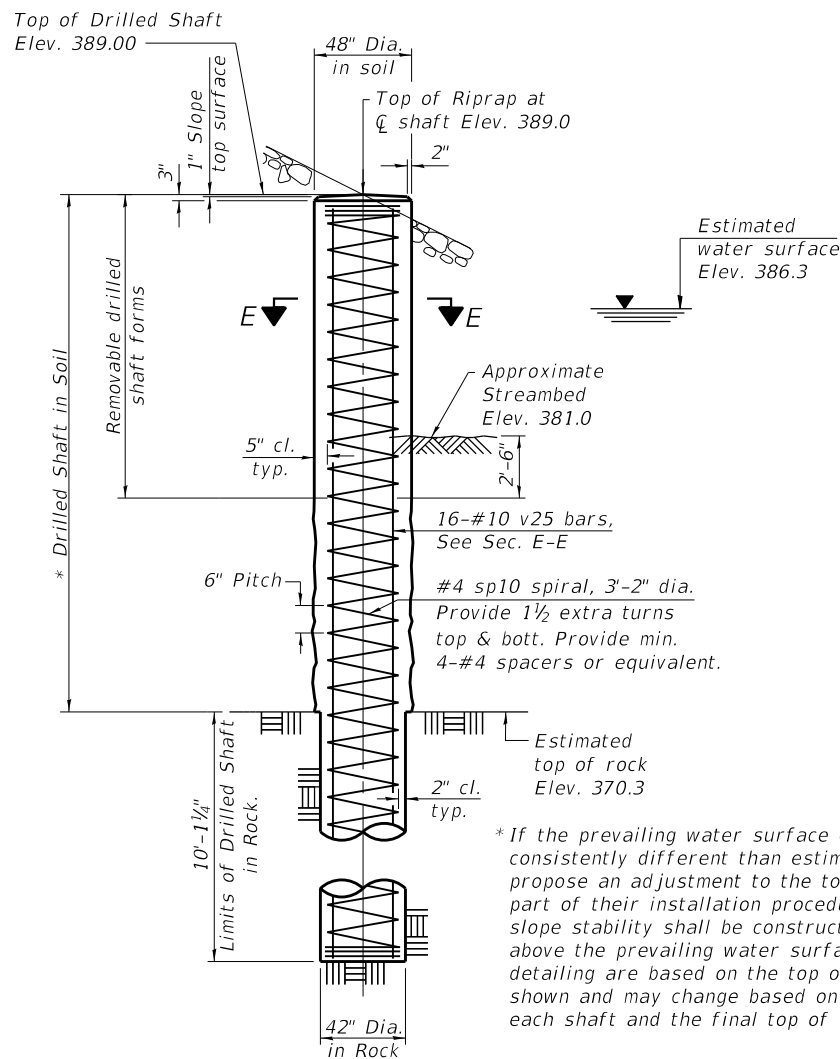
SECTION E-E

**CONSTRUCTION SEQUENCE FOR WEB WALL**

1. Excavate between shafts to elevation of web wall base and set lower web wall forms through water to bear on the circular edge of drilled shafts. Secure in place with fill, struts or tie forms together as required.
2. Place the lower web wall reinforcement cage into the forms using spacers to maintain proper clearances.
3. If the forms can be sealed against the shafts and streambed to allow dewatering, the reinforcement and the concrete placement may be completed in the dry. Alternatively, the rebar cage can be lowered into position through water and the concrete discharged at the base of the excavation through a tremie pipe or pump hose, displacing water, sediment, and tainted concrete out the top of the forms.
4. Construct columns.
5. Construct upper web walls.

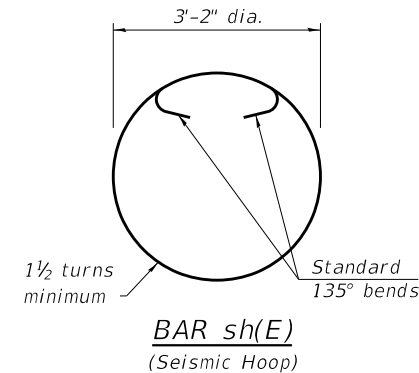


TOP PLAN



TYPICAL DRILLED SHAFT FOR SLOPE STABILITY

See Sheet 3 of 38 for layout of Drilled Shafts for slope stability.

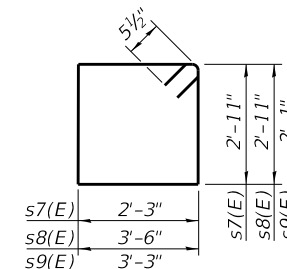


Mechanically spliced or shop welded (per AWS D1.4) seismic hoop alternates will be permitted at no additional cost to the Department.

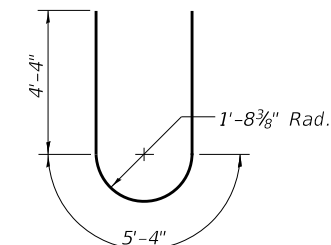
**A & B DIMENSIONS**

| Bar    | A      | B     |
|--------|--------|-------|
| s10(E) | 2'-3"  | 2'-9" |
| s11(E) | 1'-8"  | 2'-6" |
| s13    | 2'-11" | 2'-0" |
| s14(E) | 3'-6"  | 1'-6" |
| u2(E)  | 1'-11" | 2'-9" |
| u3(E)  | 2'-3"  | 2'-9" |

BARS s10(E), s11(E), s13, s14(E), u2(E) & u3(E)



BARS s7(E), s8(E) & s9(E)



BAR u1(E)

**L DIMENSIONS**

| Bar    | L       |
|--------|---------|
| v20(E) | 39'-11" |
| v23(E) | 40'-4"  |

BARS v20(E) & v23(E) (Headed)

Headed bars shall conform to ASTM A970 with Threaded attachment; Class HA; and reinforcement bars conforming to ASTM A706. Cost included with Reinforcement Bars, Epoxy Coated.

**SEVEN DRILLED SHAFTS FOR SLOPE STABILITY BILL OF MATERIAL**

| Bar                   | No. | Size    | Length | Shape |
|-----------------------|-----|---------|--------|-------|
| ** sp10               | 7   | #4      | 28'-5" | ⌘     |
| s13                   | 28  | #5      | 6'-11" | □     |
| v25                   | 112 | #10     | 28'-7" | —     |
| Reinforcement Bars    |     | Pound   | 16,760 |       |
| Drilled Shaft in Soil |     | Cu. Yd. | 60.8   |       |
| Drilled Shaft in Rock |     | Cu. Yd. | 25.2   |       |

\*\* Length is height of spiral.

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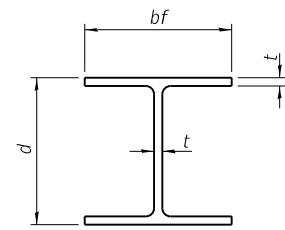
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CHECKED - RTM/SHL 07/19  
DRAWN - KAH 07/19  
CHECKED - MTD/SHL 08/19

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PIER DETAILS (SB)  
STRUCTURE NO. 091-0078 (SB)

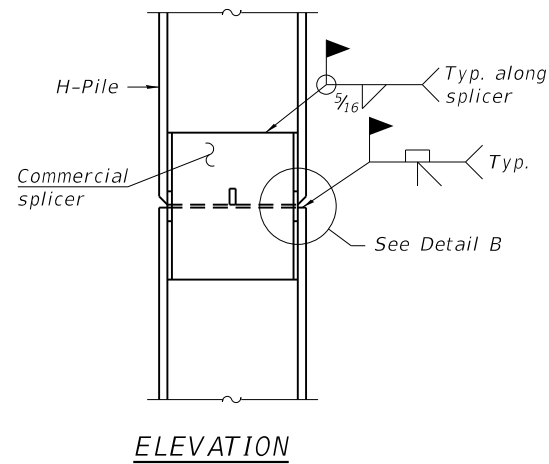
SHEET 30 OF 38 SHEETS

| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 96        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |

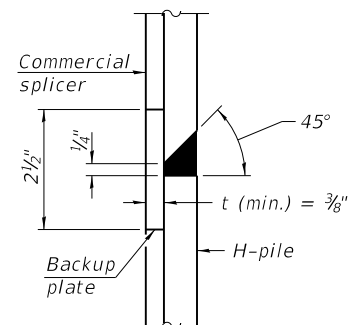


**STEEL PILE TABLE**

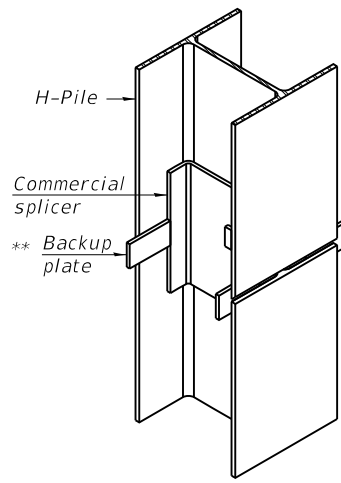
| Designation | Depth d | Flange width bf | Web and Flange thickness t | Encasement diameter A |
|-------------|---------|-----------------|----------------------------|-----------------------|
| HP 14x117   | 14 1/4" | 14 7/8"         | 1 3/16"                    | 30"                   |
| x102        | 14"     | 14 3/4"         | 1 1/16"                    | 30"                   |
| x89         | 13 7/8" | 14 3/4"         | 5/8"                       | 30"                   |
| x73         | 13 3/8" | 14 3/8"         | 1/2"                       | 30"                   |
| HP 12x84    | 12 1/4" | 12 1/4"         | 1 1/16"                    | 24"                   |
| x74         | 12 1/8" | 12 1/4"         | 5/8"                       | 24"                   |
| x63         | 12"     | 12 1/8"         | 1/2"                       | 24"                   |
| x53         | 11 3/4" | 12"             | 7/16"                      | 24"                   |
| HP 10x57    | 10"     | 10 1/4"         | 9/16"                      | 24"                   |
| x42         | 9 3/4"  | 10 1/8"         | 7/16"                      | 24"                   |
| HP 8x36     | 8"      | 8 1/8"          | 7/16"                      | 18"                   |



**ELEVATION**

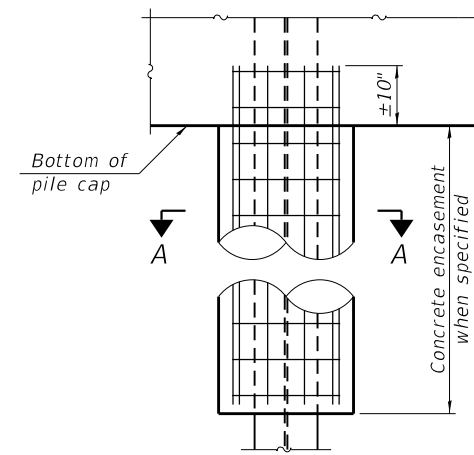


**DETAIL "B"**

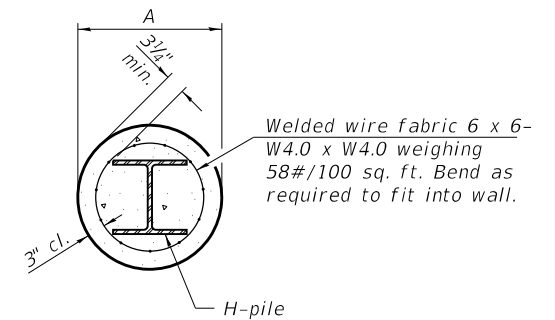


**ISOMETRIC VIEW**

**WELDED COMMERCIAL SPLICE**

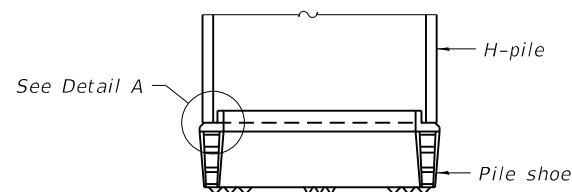


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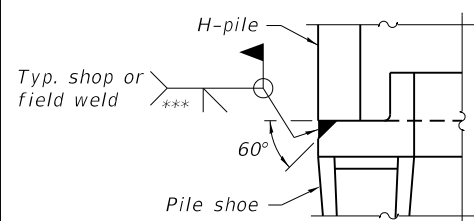


**SECTION A-A**

**INDIVIDUAL PILE CONCRETE ENCASUREMENT**  
(Forms for encasement may be omitted when soil conditions permit).

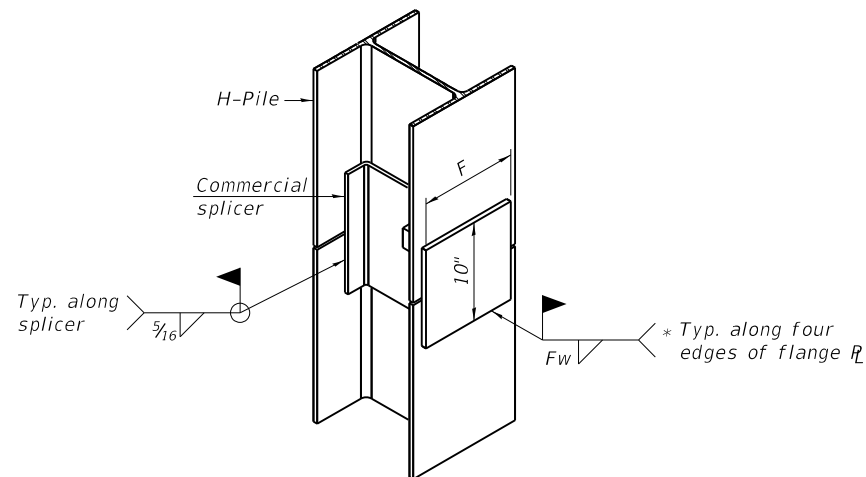


**ELEVATION**



**DETAIL A**

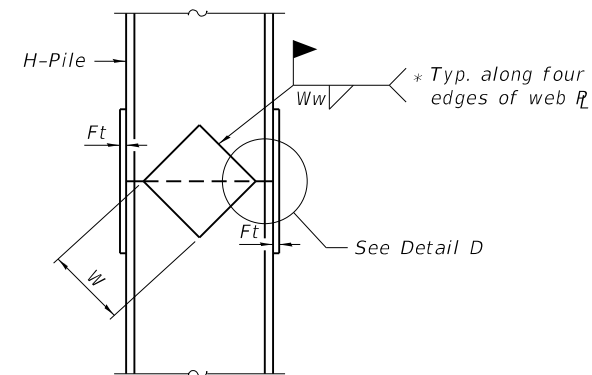
**SHOE ATTACHMENT**



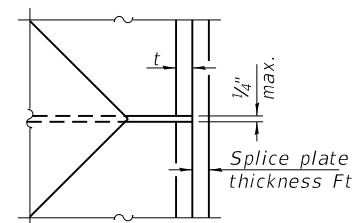
**ISOMETRIC VIEW**

**WELDED COMMERCIAL SPLICE ALTERNATE**

- \* Interrupt welds 1/4" from end of web and/or each flange.
- \*\* Remove portions of backup plates that extend outside the flanges.
- \*\*\* Weld size per pile shoe manufacturer (5/16" min.).

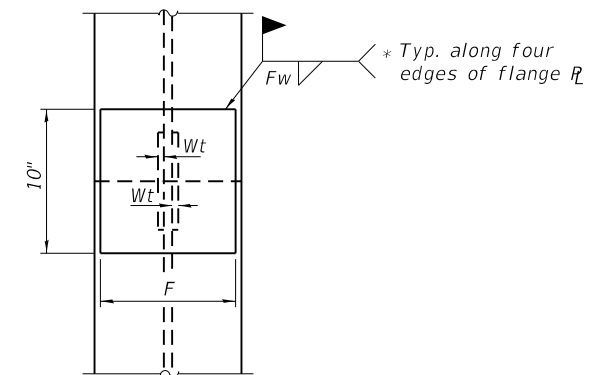


**ELEVATION**



**DETAIL D**

**WELDED PLATE FIELD SPLICE**



**END VIEW**

| Designation | F       | Ft   | Fw      | W      | Wt   | Ww   |
|-------------|---------|------|---------|--------|------|------|
| HP 14x117   | 12 1/2" | 1"   | 7/8"    | 7 3/4" | 5/8" | 1/2" |
| x102        | 12 1/2" | 7/8" | 3/4"    | 7 3/4" | 5/8" | 1/2" |
| x89         | 12 1/2" | 3/4" | 1 1/16" | 7 3/4" | 5/8" | 1/2" |
| x73         | 12 1/2" | 5/8" | 9/16"   | 7 3/4" | 5/8" | 1/2" |
| HP 12x84    | 10"     | 7/8" | 1 1/16" | 6 1/2" | 5/8" | 1/2" |
| x74         | 10"     | 7/8" | 1 1/16" | 6 1/2" | 5/8" | 1/2" |
| x63         | 10"     | 5/8" | 1/2"    | 6 1/2" | 1/2" | 3/8" |
| x53         | 10"     | 5/8" | 1/2"    | 6 1/2" | 1/2" | 3/8" |
| HP 10x57    | 8"      | 3/4" | 9/16"   | 5 1/4" | 1/2" | 3/8" |
| x42         | 8"      | 5/8" | 9/16"   | 5 1/4" | 1/2" | 3/8" |
| HP 8x36     | 7"      | 5/8" | 7/16"   | 4 1/4" | 1/2" | 3/8" |

Note:  
The steel H-piles shall be according to AASHTO M270 Grade 50.

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F-HP 8-11-2017



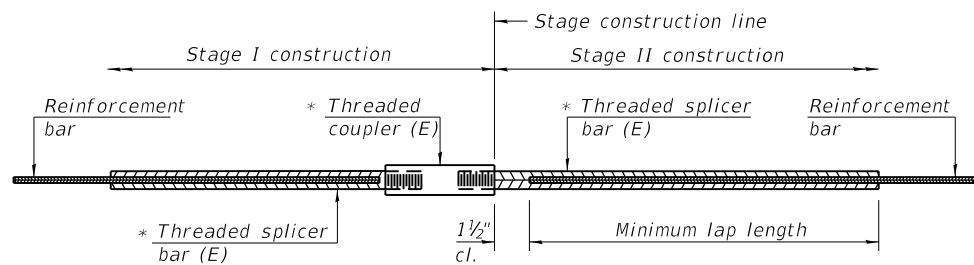
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| PLOT SCALE = 0.1667' / in. | DRAWN - KAH 07/19       | REVISED - |
| PLOT DATE = 10/15/2019     | CHECKED - MTD/SHL 08/19 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**HP PILE DETAILS  
STRUCTURE NO. 091-0077 (NB) & 091-0078 (SB)**

SHEET 31 OF 38 SHEETS

| F.A.I. RTE.               | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 57                        | (91-3)B-2 | UNION  | 161          | 97        |
| CONTRACT NO. 78504        |           |        |              |           |
| ILLINOIS FED. AID PROJECT |           |        |              |           |

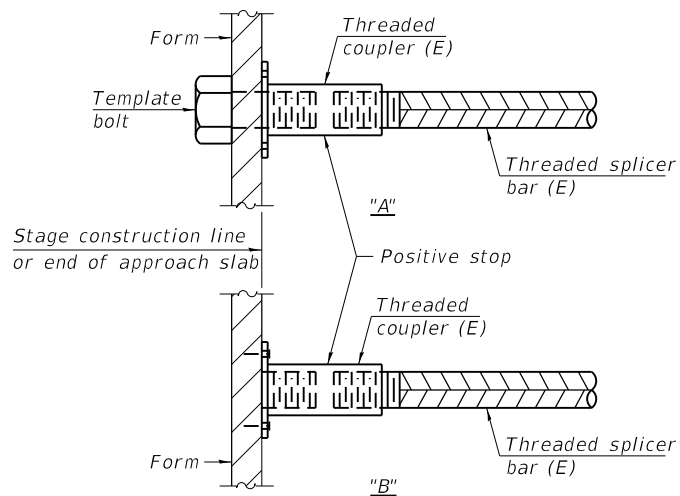


**STANDARD BAR SPLICER ASSEMBLY**

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

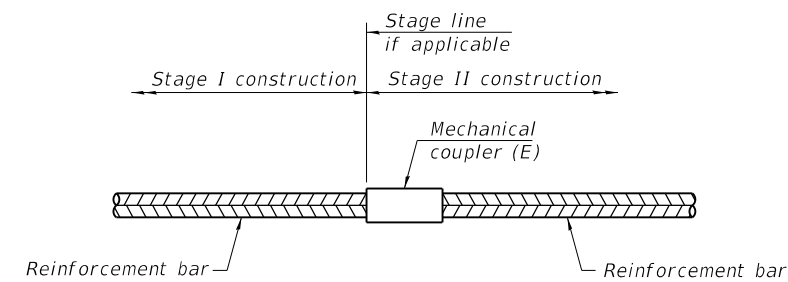
\* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

| Location | Bar size | No. assemblies required | Minimum lap length |
|----------|----------|-------------------------|--------------------|
| N/A      |          |                         |                    |
|          |          |                         |                    |
|          |          |                         |                    |



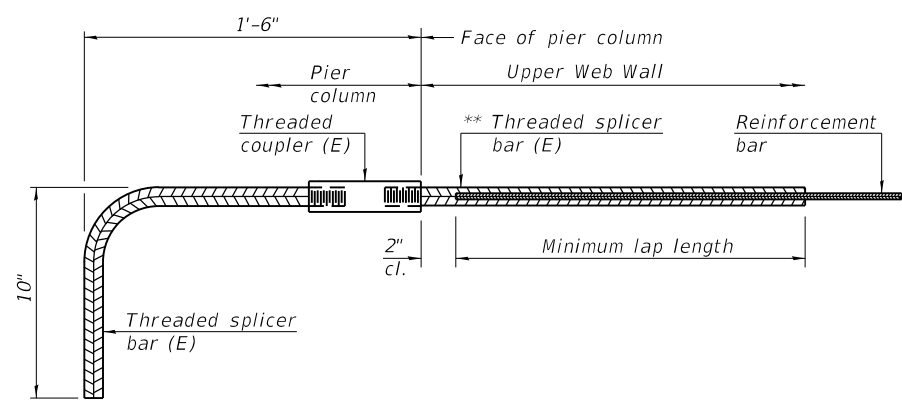
**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.  
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.  
 (E) : Indicates epoxy coating.



**STANDARD MECHANICAL SPLICER**

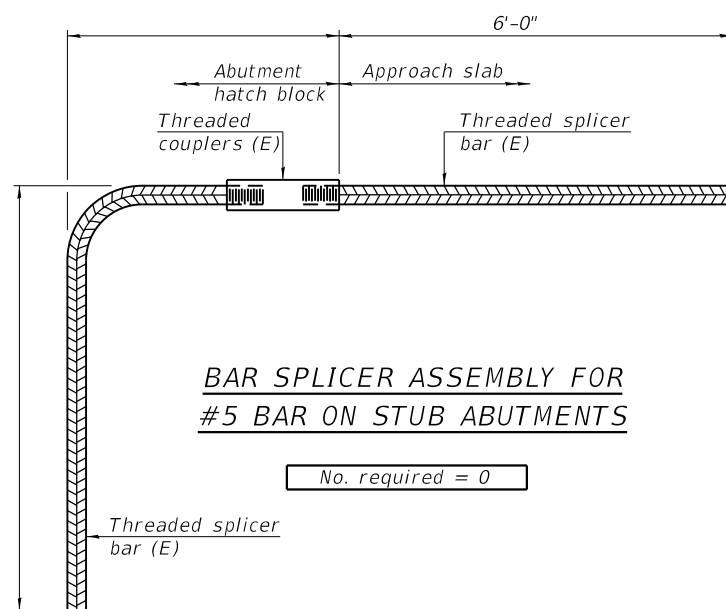
| Location           | Bar size | No. assemblies required |
|--------------------|----------|-------------------------|
| NBL Pier 1 Columns | #10      | 64                      |
| NBL Pier 2 Columns | #10      | 64                      |
|                    |          |                         |
|                    |          |                         |



**BAR SPLICER ASSEMBLY FOR #5 BAR IN UPPER WEB WALLS**

\*\* Threaded splicer bar length = min. lap length + 2" + thread length

| Location             | Bar size | No. assemblies required | Minimum lap length |
|----------------------|----------|-------------------------|--------------------|
| NBL Pier 1 Web Walls | #5       | 108                     | 3'-4"              |
| NBL Pier 2 Web Walls | #5       | 108                     | 3'-4"              |
| SBL Pier 1 Web Walls | #5       | 108                     | 3'-4"              |
| SBL Pier 2 Web Walls | #5       | 120                     | 3'-4"              |



**BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS**

No. required = 0

**NOTES**

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.  
 All reinforcement shall be lapped and tied to the splicer bars.  
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.  
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

MODEL: PLOT FILE NAME: Y:\IDOT\1259-11\_78504\CADD\SP\_SN\_091-0077-0078\0910077-78504-32-BarSplicers.dgn



|                            |                         |           |
|----------------------------|-------------------------|-----------|
| USER NAME = kah            | DESIGNED - KJA 07/19    | REVISED - |
| ESCA PROJECT NO. 1259.11   | CHECKED - RTM/SHL 07/19 | REVISED - |
| PLOT SCALE = 0.1667' / in. | DRAWN - KAH 07/19       | REVISED - |
| PLOT DATE = 10/15/2019     | CHECKED - MTD/SHL 08/19 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS  
STRUCTURE NO. 091-0077 (NB) & 091-0078 (SB)**

SHEET 32 OF 38 SHEETS

|                    |           |                  |              |           |
|--------------------|-----------|------------------|--------------|-----------|
| F.A.I. RTE.        | SECTION   | COUNTY           | TOTAL SHEETS | SHEET NO. |
| 57                 | (91-3)B-2 | UNION            | 161          | 98        |
| CONTRACT NO. 78504 |           |                  |              |           |
| ILLINOIS           |           | FED. AID PROJECT |              |           |



