

DESIGN BY: MICHAEL HUDELSON (309) 671-3477 PROJECT ENGINEER: MAUREEN ADDIS (309) 671-3454

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18 RS, (18HB)	KNOX	70	1
				+1
				71

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS**

**PLANS FOR PROPOSED
FEDERAL AID HIGHWAY**

**FAP ROUTE 313 (US 34)
SECTION 18 RS, (18HB)
KNOX COUNTY
C-94-099-01**

PROJECT ACNHF-BHF-0313(014)
R 1 E

For Index of Sheets On Page 2

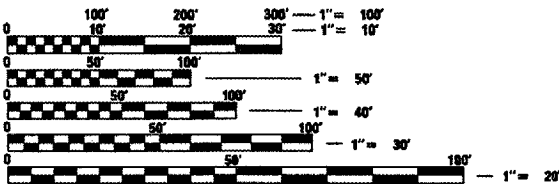
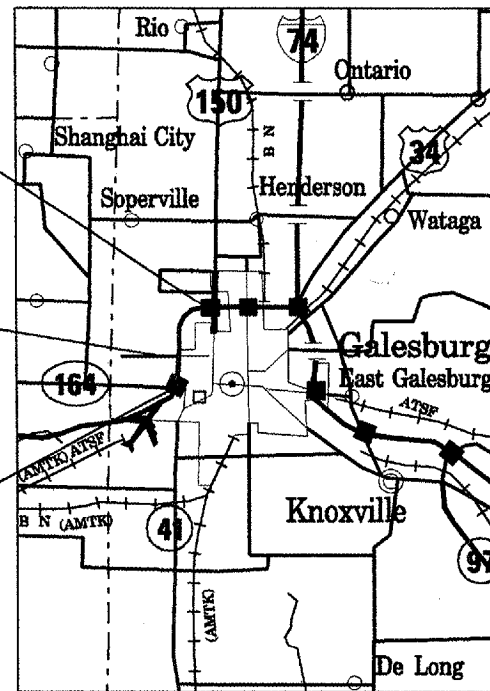
LIST OF STANDARDS

- | | |
|-----------|-----------|
| 420001-07 | 635006-02 |
| 420401-06 | 635011-01 |
| 420601-05 | 642001 |
| 420701-02 | 667101 |
| 442001-04 | 701101-01 |
| 442101-07 | 701400-02 |
| 482011-03 | 701401-04 |
| 515001-02 | 701406-04 |
| 601001-02 | 701411-04 |
| 601101 | 701426-02 |
| 606001-03 | 701602-03 |
| 606101-02 | 701701-05 |
| 630001-07 | 701901 |
| 630301-04 | 780001-01 |
| 631031-06 | 781001-02 |
| | BLR-22-5 |

PROJECT ENDS
US 34 STA 477+00

BRIDGE DECK
REPLACEMENT
FREMONT ST.
OVER US 34
(S.N. 048-0069)

PROJECT BEGINS
US 34 STA 363+00



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

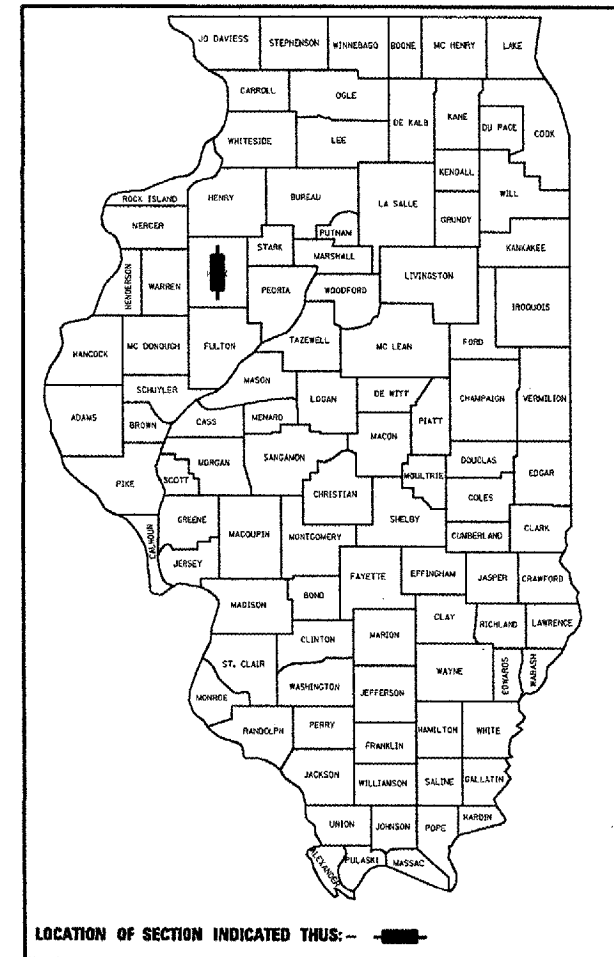
J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123

JOB DESCRIPTION
This project consists of 3 3/4" hot-mix asphalt overlay on the mainline and ramps, 2" hot-mix asphalt surface removal on the shoulder, and 5 3/4" hot-mix asphalt overlay on the shoulders. Improvements consist of pipe underdrains and deck replacement on structure carrying Fremont St. over US 34. (S.N. 048-0069)

GROSS LENGTH OF IMPROVEMENT = 11400 FEET = 2.16 MILES
NET LENGTH OF IMPROVEMENT = 11280 FEET = 2.14 MILES

CONTRACT NO. 68190 CATALOG NO. 032328-01

D-94-078-01



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED 10/17 2007
Joseph E. Conover
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER
Dec 7, 2007
Eric E. Horn
ENGINEER OF DESIGN AND ENVIRONMENT
Dec 7, 2007
Christine M. Reed
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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OF THE STATE OF ILLINOIS**

SUMMARY OF QUANTITIES

P.A.P. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18 RS.(18HB)I	KNOX	70	3
STA.	TO STA.			
FILE NO. AND EXT. NO. 1	DATE	FILE NO. AND EXT. NO.		

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SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE	
CODE NO	ITEM	UNIT		KNOX CO. 80%-20% FED-STA 1000	KNOX CO. 80%-20% FED-STA X271-2A
28100825	STONE DUMPED RIPRAP, CLASS B3	TON	1.8	1.8	
28200200	FILTER FABRIC	SQ YD	5.6	5.6	
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	22.3	22.3	
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	4	4	
40600895	CONSTRUCTING TEST STRIP	EACH	2	2	
40600982	HOT - MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	458.2	458.2	
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SQ YD	828.5	828.5	
40600990	TEMPORARY RAMP	SQ YD	279.9	279.9	
40603085	HOT - MIX ASPHALT BINDER COURSE, IL - 19.0, N70	TON	8808.9	8808.9	
40603335	HOT - MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	13509	13509	
40603340	HOT - MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	5884.9	5884.9	
40800030	AGGREGATE (PRIME COAT)	TON	208.8	208.8	
42000500	PORTLAND CEMENT CONCRETE PAVEMENT 10"	SQ YD	1087.7	1087.7	
42001200	PAVEMENT FABRIC	SQ YD	1522.2	1522.2	
42001400	BRIDGE APPROACH PAVEMENT (SPECIAL)	SQ YD	193		193
42001430	BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE)	SQ YD	40		40
44000100	PAVEMENT REMOVAL	SQ YD	902.6	902.6	
44000157	HOT - MIX ASPHALT SURFACE REMOVAL, 2 "	SQ YD	41525	41525	
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	193	193	
44000920	BITUMINOUS CONCRETE SHOULDER REMOVAL	SQ YD	1863.3	1863.3	
44002500	GUTTER OUTLET REMOVAL	EACH	2	2	
44200529	CLASS A PATCHES, TYPE II, 8 INCH	SQ YD	733	733	
44200533	CLASS A PATCHES, TYPE III, 8 INCH	SQ YD	297	297	
44200535	CLASS A PATCHES, TYPE IV, 8 INCH	SQ YD	601	601	
44200970	CLASS B PATCHES, TYPE II, 10 INCH	SQ YD	147	147	
44200974	CLASS B PATCHES, TYPE III, 10 INCH	SQ YD	21	21	
44200976	CLASS B PATCHES, TYPE IV, 10 INCH	SQ YD	425	425	
44201863	CLASS D PATCHES, TYPE II, 18 INCH	SQ YD	19.6	19.6	
44213000	PATCHING REINFORCEMENT	SQ YD	1631	1631	
44213200	SAW CUTS	FOOT	9794	9794	
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	22800	22800	

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE	
CODE NO	ITEM	UNIT		KNOX CO. 80%-20% FED-STA 1000	KNOX CO. 80%-20% FED-STA X271-2A
48101200	AGGREGATE SHOULDERS, TYPE B	TON	1714	1714	
48203029	HOT - MIX ASPHALT SHOULDERS, 8"	SQ YD	3001.4	3001.4	
50102400	CONCRETE REMOVAL	CU YD	4.5		4.5
50104650	SLOPE WALL REMOVAL	SQ YD	248		248
50104720	REMOVAL OF EXISTING CONCRETE DECK	EACH	1		1
50157300	PROTECTIVE SHIELD	SQ YD	400		400
50300255	CONCRETE SUPER STRUCTURE	CU YD	252.6		252.6
50300260	BRIDGE DECK GROOVING	SQ YD	608		608
50300300	PROTECTIVE COAT	SQ YD	823		823
50500405	FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	4490		4490
50500505	STUD SHEAR CONNECTORS	EACH	990		990
50500715	JACK AND REMOVE EXISTING BEARINGS	EACH	10		10
50501110	STRUCTURAL STEEL REMOVAL	POUND	2760		2760
50600300	CLEANING AND PAINTING STEEL BRIDGE	L SUM	1		1
50606400	CONTAINMENT AND DISPOSAL OF LEAD PAINT & CLEANING RESIDUES	L SUM	1		1
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	56330		56330
50800305	WELDED WIRE FABRIC 6X6	SQ YD	252.5	252.5	
51100100	SLOPE WALL 4 INCH	SQ YD	289		289
51500100	NAME PLATES	EACH	1		1
52100010	ELASTOMERIC BEARING ASSEMBLY, TYPE 1	EACH	10		10
59000200	EPOXY CRACK INJECTION	FOOT	13.5		13.5
60100070	SHOULDER REMOVAL AND REPLACEMENT	FOOT	44585	44585	
60107600	PIPE UNDERDRAINS 4"	FOOT	36576	36576	
60107700	PIPE UNDERDRAINS 6"	FOOT	7235	7235	
60108100	PIPE UNDERDRAINS 4" (SPECIAL)	FOOT	2034.5	2034.5	
60108200	PIPE UNDERDRAINS 6" (SPECIAL)	FOOT	178.7	178.7	
60600095	CLASS SI CONCRETE (OUTLET)	CU YD	5.9	5.9	
60602500	CONCRETE GUTTER, TYPE A	FOOT	95	95	
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	35	35	
63000000	STEEL PLATE BEAM GUARD RAIL, TYPE A	FOOT	575	575	
63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	6	2	4
63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	2	2	
63200310	GUARD RAIL REMOVAL	FOOT	530	330	200

* SPECIALTY ITEM

SUMMARY OF QUANTITIES

F.A.P. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18 RS, (18HB)	KNOX	70	4
STA.	TO STA.			
FED. ROAD DIST. NO. 1	SLURRY	FUEL AIR PROJECT		

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ACNHF BHE

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE	
CODE NO	ITEM	UNIT		KNOX CO. 80%-20% FED-STA 1000	KNOX CO. 80%-20% FED-STA X271-2A
63500110	DELINEATORS (SPECIAL)	EACH	4	4	
64200105	SHOULDER RUMBLE STRIP	FOOT	44100	44100	
67100100	MOBILIZATION	L SUM	1	1	
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	2	2	
70100700	TRAFFIC CONTROL AND PROTECTION, STANDARD 701406	L SUM	1	1	
70100800	TRAFFIC CONTROL AND PROTECTION, STANDARD 701401	L SUM	1	1	
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1	
70102672	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602, SPECIAL	L SUM	1	1	
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	12	12	
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	13532	13532	
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	46.8	46.8	
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	51719	51719	
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	5760	5760	
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	2624	2624	
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	414	414	
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	36	36	
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	4511	4511	
78003130	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 6"	FOOT	5760	5760	
78005100	EPOXY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	46.8	46.8	
78005110	EPOXY PAVEMENT MARKING - LINE 4"	FOOT	51719	51719	
78005140	EPOXY PAVEMENT MARKING - LINE 8"	FOOT	2624	2624	
78005150	EPOXY PAVEMENT MARKING - LINE 12"	FOOT	414	414	
78005180	EPOXY PAVEMENT MARKING - LINE 24"	FOOT	36	36	
78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	316	316	
* 78200410	GUARDRAIL MARKERS, TYPE A	EACH	10	10	
* 78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	2	2	
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	316	316	

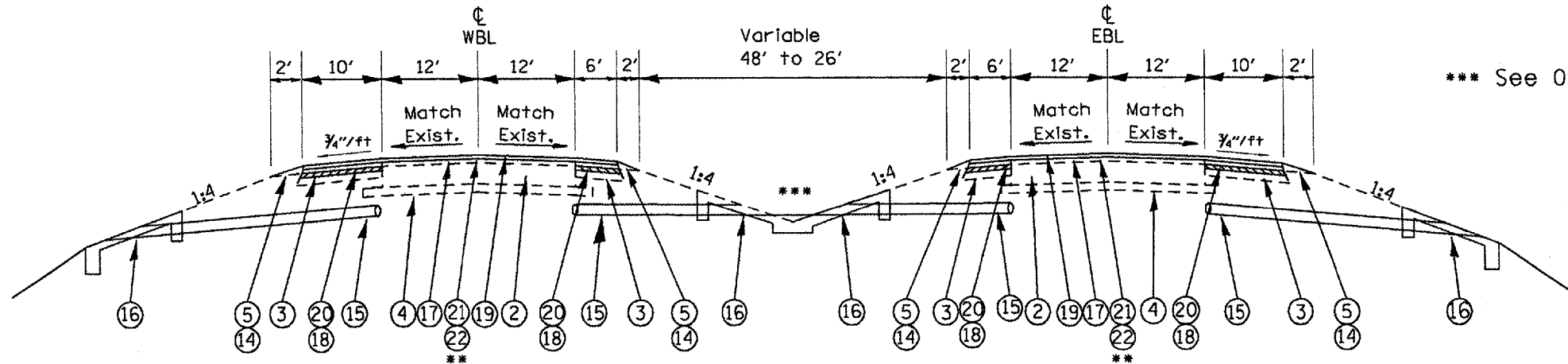
SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE	
CODE NO	ITEM	UNIT		KNOX CO. 80%-20% FED-STA 1000	KNOX CO. 80%-20% FED-STA X271-2A
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1	
Z0017202	DOWEL BARS 1 1/2"	EACH	356	356	
Z0075310	TIE BARS 3/4"	EACH	406	406	
Z0076600	TRAINEES	HOUR	500		500
60600096	CLASS SI CONCRETE (OUTLET PROTECTOR)	CU YD	58.2	58.2	
X0301512	GUARDRAIL AGGREGATE EROSION CONTROL	TON	3.2	3.2	
X0322729	MATERIAL TRANSFER DEVICE	TON	12949	12949	
X0323830	DRAINAGE SCUPPERS, DS-11	EACH	4		4
52000100	PREFORMED JOINT STRIP SEAL	FOOT	62		62
X0325305	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	139		139
X0712400	TEMPORARY PAVEMENT	SQ YD	479	479	
X0919000	TEMPORARY PAVEMENT REMOVAL	SQ YD	479	479	
52100520	ANCHOR BOLTS, 1"	EACH	40		40
X2503100	MOWING	UNIT	228	228	

* SPECIALTY ITEM

▼ - NP 100% STATE

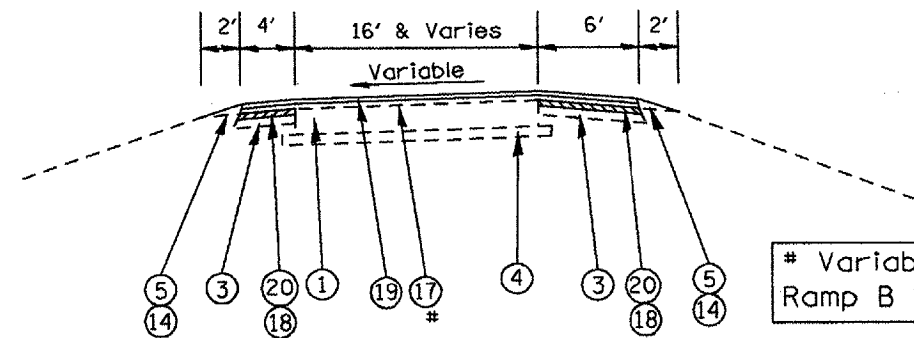
▼ Y080 80% FED / 20% STATE

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18RS,118HD1	KNOX	70	5
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

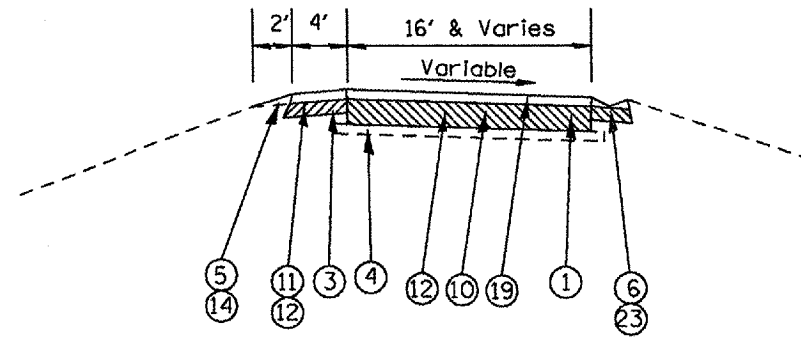


*** See Outlet Protector Detail Page 51-52

US 34
Sta. 363+00 to 477+00

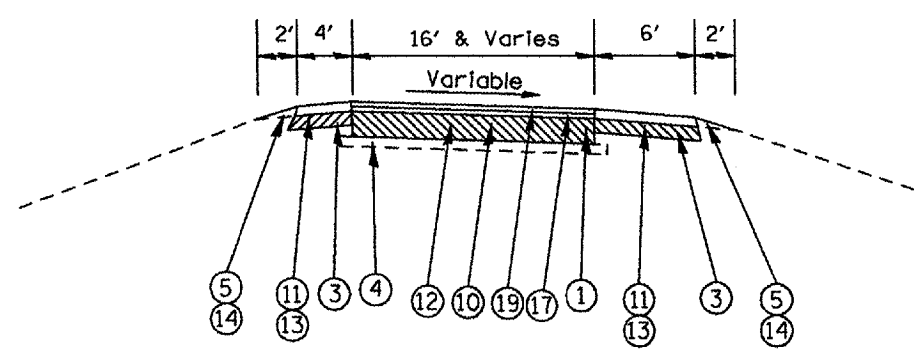


US 34/Main St. Ramp A
Sta. 0+00 to 12+49
US 34/Main St. Ramp B
Sta. 1+30 to 23+32



US 34/Main St. Ramp B
Sta. 0+29 to 1+30

Variable Depth
Ramp B 1+30 to 2+72



US 34/Main St. Ramp A
Sta. 12+49 to 14+89

- ① Existing PCC Pavement 10"
- ② Existing CRC Pavement 8"
- ③ Existing Bituminous Shoulders 8"
- ④ Existing Subbase Granular Material
- ⑤ Existing Aggregate Shoulder
- ⑥ Existing Concrete Curb and Gutter
- ⑩ Proposed Pavement Removal
- ⑪ Proposed Bituminous Shoulder Removal
- ⑫ Proposed PCC Pavement 10"
- ⑬ Proposed Hot-Mix Asphalt Shoulder 8"
- ⑭ Proposed Aggregate Shoulder, Type B
- ⑮ Proposed Pipe Underdrains & Pipe Underdrains (Special)*
- ⑯ Proposed Outlet Protector
- ⑰ Proposed Hot-Mix Asphalt Binder Course, IL-19.0, N70 2.25"
- ⑱ Proposed Hot-Mix Asphalt Shoulder 5.75"
- ⑲ Proposed Hot-Mix Surface Course, N70 1.5"
- ⑳ Proposed Hot-Mix Asphalt Surface Removal 2"
- ㉑ Proposed Strip Reflective Crack Control**
- ㉒ Proposed Mixture for Cracks, Joints, and Flangeways**
- ㉓ Proposed Type A Gutter

* See Plan Sheets for Diameter
** See Detail

Detail

The crack in the CRC mainline Pavement on or near the centerline (offset varies) will be filled with Mixture for Cracks, Joints, and Flangeways. Strip Reflective Crack Control will be placed on first lift of Binder Course and centered over the crack.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**Typical Sections
not to scale**

SCALE: VERT.
HORIZ.
DATE

DRAWN BY
CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18RS.18HBI	KNOX	70	6
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

RESURFACING TABLE

LOCATION	WIDTH	LENGTH	AREA	HOT-MIX ASPHALT SURFACE REMOVAL		PCC SURFACE REMOVAL BUTT JOINT	TEMPORARY RAMP	STRIP REFLECTIVE CRACK CONTROL TREATMENT	BITUMINOUS MATERIALS (PRIME COAT)	AGGREGATE (PRIME COAT)	PORTLAND CEMENT CONCRETE PAVEMENT 10"	HOT-MIX ASPHALT BINDER COURSE IL-19.0, N70		HOT-MIX ASPHALT SURFACE COURSE SUPERPAVE MIX "D", N70	HOT-MIX ASPHALT SHOULDERS		AGGREGATE SHOULDERS TYPE B		HOT-MIX ASPHALT SHOULDERS 8"			
				BUTT JOINT	2"							2.25"	VAR.		1.5"	LEFT SIDE	RIGHT SIDE	LEFT SIDE	RIGHT SIDE	LEFT SIDE	MED.	RIGHT SIDE
WESTBOUND US 34																						
362+40 TO 363+00	24	60	160	106.7		160	44.4		0.05	0.5		10.1		13.5	14.5	8.7	1.4	2				
363+00 TO 370+15	24	715	1906.7		476.7			715	0.61	5.7		242.1		161	153.5		23.8					
370+15 TO 456+75	24	8660	23093.3		15395.6			8660	7.39	69.3		2932.5		1949.9	3098.4	1859	287.9	287.9				
456+75 TO 467+75	24	1100	2933.3		1222.2			1100	0.94	8.8		372.5		247.7	393.6		36.6	35.7			1275	
467+75 TO 477+00	24	925	2466.7		1644.4			925	0.79	7.4		313.2		208.3	330.9	198.6	30.8	30.8				
477+00 TO 477+60	24	60	160	96.7		160	44.4		0.05	0.5		10.1		13.5	14.5	6.5	2	2				
EASTBOUND US 34																						
362+40 TO 363+00	24	60	160	106.7		160	44.4		0.05	0.5		10.1		13.5	8.7	14.5	2	2				
363+00 TO 363+59	24	59	157.3		104.9			59	0.05	0.5		20		13.3	12.7	21.1	2	2				
363+59 TO 375+97	24	1238	3301.3		825.3			1238	1.06	9.9		419.2		278.8	265.8		41.2					
375+97 TO 456+75	24	8078	21541.3		14360.9			8078	6.89	64.6		2735.4		1818.9	1734.1	2890.1	268.6	268.6				
456+75 TO 467+75	24	1100	2933.3		1222.2			1100	0.94	8.8		372.5		247.7	393.6	35.7	36.6	1275				
467+75 TO 477+00	24	925	2466.7		1644.4			925	0.79	7.4		313.2		208.3	330.9	198.6	30.8	30.8				
477+00 TO 477+60	24	60	160	96.7		160	44.4		0.05	0.5		10.1		13.5	6.5	14.5	2	2				
US 34 MEDIAN X-OVER																						
																	0.5	0.5			76.2	
MAIN ST. RAMP A																						
00+00 TO 05+28	1 to 30	528	909.3		363.1				0.29	2.7		116		77		116.9		17.6				
05+28 TO 07+30	16	202	359.1		481				0.11	1.1		45.8		30.4	141.5	43.4		6.7				
07+30 TO 12+49	16	519	922.7		576.7				0.3	2.8		117.6		78.1	74.3	111.4	17.3	17.3				
12+49 TO 14+89	16	240	426.7						0.1	0.9	426.7	54.4		36.1			8	8	160		106.7	
14+89 TO 19+76	16	487	865.8		541.1				0.28	2.6		110.4		73.3	69.7	104.5	16.2	16.2				
19+76 TO 22+04	16 to 28	228	557.3		253.3				0.18	1.7		70.8		47.1	32.6	48.9	7.6	7.6				
22+04 TO 23+24	28	120	373.3		133.3				0.12	1.1		47.4		31.5	17.2	25.8	4	4				
23+24 TO 23+94	VAR	70	309.8		80.6				0.1	0.9		39.2		26.1	10	15	2.3	2.3				
23+94 TO 24+24	VAR	30	188.5	51.4		188.5	73.4		0.06	0.6		11.9		15.9	4.3	15.9	1	2.5				
MAIN ST. RAMP B																						
00+29 TO 01+30	VAR	101	649.5								661						3.2	0.6				
01+30 TO 01+61	16	31	56				28.9		0.02	0.2			7.6	4.7			1	1	13.8		20.7	
01+61 TO 02+72	16	111	197.3		49.3				0.06	0.6			34.8	16.7	15.9		3.7	3.7			74	
02+72 TO 10+91	16	819	1456		910				0.47	4.4		185.6		123.3	117.2	175.8	27.2	27.2				
10+91 TO 12+82	16	191	339.6		465.4				0.11	1		43.3		28.7	108.9	41		6.4				
12+82 TO 13+77	16 to 14	95	158.3		126.7				0.05	0.5		20.2		13.4	20.4			3.2				
13+77 TO 23+32	20 to 1	955	1114.2		647.8				0.36	3.3		142.9		94.7	208.6			31.8				
SUBTOTAL												8766.5	42.4		6822.6	6686.3	833	880.8	1448.8	76.2	1476.4	
TOTAL				458.2	41524.9	828.5	279.9	22800	22.27	208.8	1087.7	8808.9	5884.9	13508.9	1713.8							

SURFACE TYPE	BITUMINOUS PRIME COAT (GAL/SQ YD)	AGGREGATE PRIME COAT (LB/SQ YD)
ON COLD MILLED SURFACE	0.1	4
EXISTING PAVEMENT	0.05	4
FOG COAT ON NEW BINDER	0.03	2

0.004 TONS PER GALLON FOR BITUMINOUS PRIME COAT

HMA SURFACE AND BINDER	112 LB/SQ YD IN
ALL OTHER HMA	112 LB/SQ YD IN
COARSE AGGREGATE	2.05 TONS/CU YD

LOCATION	TON
CENTERLINE CRACK WB & EB	4
TOTAL	4

NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

Schedule of Quantities

SCALE: VERT. DATE
HORIZ. DATE

DRAWN BY
CHECKED BY

DATE = 10/25/2007
 PLOT SCALE = 1/8" = 100'
 USER NAME = hndh123456

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18RS(18HB1)	KNOX	70	7
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

PAVEMENT REMOVAL	
LOCATION	SQ YD
RAMP A 12+49 TO 14+89	426.7
RAMP B 0+29 TO 1+30	475.9
TOTAL	902.6

COMBINATION CURB AND GUTTER REMOVAL	
LOCATION	FOOT
RAMP B 0+29 TO 1+68 RT	193
TOTAL	193

BITUMINOUS CONCRETE SHOULDER REMOVAL	
LOCATION	SQ YD
US 34 WB 456+75 TO 467+75	733.3
US 34 EB 456+75 TO 467+75	733.3
RAMP A LT 12+49 TO 14+89	160
RAMP A RT 12+49 TO 14+89	106.7
RAMP B LT 1+30 TO 1+61	60.7
RAMP B RT 1+68 TO 2+72	69.3
TOTAL	1863.3

GUTTER OUTLET REMOVAL	
LOCATION	EACH
MAIN ST. 53+16 LT	1
RAMP B 1+68 RT	1
TOTAL	2

CLASS SI CONCRETE (OUTLET PROTECTOR) AND WELDED WIRE FABRIC 6 X 6															
WB US 34 LT				WB US 34 RT				EB US 34 LT				EB US 34 RT			
LOCATION	TYPE	CLASS SI CONC. (OUT PROT)	WELDED WIRE FABRIC 6X6	LOCATION	TYPE	CLASS SI CONC. (OUT PROT)	WELDED WIRE FABRIC 6X6	LOCATION	TYPE	CLASS SI CONC. (OUT PROT)	WELDED WIRE FABRIC 6X6	LOCATION	TYPE	CLASS SI CONC. (OUT PROT)	WELDED WIRE FABRIC 6X6
		CU YD	SQ YD			CU YD	SQ YD			CU YD	SQ YD			CU YD	SQ YD
367+50	1	0.61	2.75	367+50	1	0.61	2.75	367+50	1	0.61	2.75	367+50	1	0.62	2.75
371+75	1	0.61	2.75	371+75	1	0.61	2.75	371+75	1	0.61	2.75	371+75	1	0.62	2.75
372+25	1	0.61	2.75	372+25	1	0.61	2.75	372+25	1	0.61	2.75	372+25	1	0.62	2.75
376+00	1	0.61	2.75	376+00	1	0.61	2.75	376+00	1	0.61	2.75	376+00	1	0.62	2.75
380+04	1	0.61	2.75	380+44	1	0.61	2.75	380+44	1	0.61	2.75	380+74	1	0.62	2.75
385+00	1	0.61	2.75	385+00	1	0.61	2.75	385+00	1	0.61	2.75	385+00	1	0.62	2.75
389+75	1	0.61	2.75	389+75	1	0.61	2.75	390+40	1	0.61	2.75	390+40	1	0.62	2.75
393+00	1	0.61	2.75	393+00	1	0.61	2.75	393+00	1	0.61	2.75	393+00	1	0.62	2.75
397+00	1	0.61	2.75	397+00	3	0.99	3.75	397+00	3	INCL. IN WB US 34 RT		397+00	1	0.62	2.75
401+05	1	0.61	2.75	401+05	2	0.86	3.75	410+60	3	INCL. IN WB US 34 RT		401+05	1	0.62	2.75
406+00	1	0.61	2.75	406+00	2	0.68	2.25	436+25	3	INCL. IN WB US 34 RT		406+00	1	0.62	2.75
410+60	1	0.61	2.75	410+60	3	0.99	3.75	445+25	3	INCL. IN WB US 34 RT		411+00	1	0.62	2.75
415+20	1	0.61	2.75	416+00	2	0.80	3.25	454+25	3	INCL. IN WB US 34 RT		417+40	1	0.62	2.75
418+20	1	0.61	2.75	418+20	2	0.68	2.25	475+30	3	INCL. IN WB US 34 RT		421+00	1	0.62	2.75
422+50	1	0.61	2.75	422+50	2	0.77	3.00					426+00	1	0.62	2.75
427+15	1	0.61	2.75	427+15	2	0.79	3.15					431+00	1	0.62	2.75
431+50	1	0.61	2.75	431+00	2	0.68	2.25					436+25	1	0.62	2.75
436+25	1	0.61	2.75	436+25	3	0.99	3.75					440+50	1	0.62	2.75
440+50	1	0.61	2.75	440+50	2	0.68	2.25					445+25	1	0.62	2.75
445+25	1	0.61	2.75	445+25	3	1.17	5.25					449+50	1	0.62	2.75
449+50	1	0.61	2.75	449+50	2	0.83	3.45					454+25	1	0.62	2.75
454+25	1	0.61	2.75	454+25	3	1.20	5.45					458+50	1	0.62	2.75
458+50	1	0.61	2.75	458+50	2	0.68	2.25					463+25	1	0.62	2.75
463+25	1	0.61	2.75	463+25	2	0.79	3.15					467+00	1	0.62	2.75
467+00	1	0.61	2.75	467+00	2	0.68	2.25					472+00	1	0.62	2.75
471+00	1	0.61	2.75	471+00	2	0.68	2.25								
475+30	1	0.61	2.75	475+30	3	1.52	8.05								
SUBTOTAL		16.47	74.25			21.34	87.5			4.88	22			15.5	68.75
TOTAL														58.19	252.5

PATCHES									
CLASS A PATCHES, 8 INCH			CLASS B PATCHES, 10 INCH			PATCHING REINFORCEMENT	SAW CUTS	DOWEL BARS 1 1/2"	TIE BARS 3/4"
TYPE II	TYPE III	TYPE IV	TYPE II	TYPE III	TYPE IV				
SQ YD	SQ YD	SQ YD	SQ YD	SQ YD	SQ YD	SQ YD	FOOT	EACH	EACH
733	297	601	147	21	425	1631	9794	356	406

STONE DUMPED RIP RAP, CLASS B3					FILTER FABRIC
LOCATION	LENGTH	WIDTH	DEPTH	TONS	
	FOOT	FOOT	IN		SQ YD
RAMP B 0+44 45.4' LT	5	5	8	0.9	2.8
RAMP B 1+18 17.1' RT	5	5	8	0.9	2.8
TOTAL				1.8	5.6

TEMPORARY PAVEMENT	
TEMPORARY PAVEMENT REMOVAL	
LOCATION	EACH
RAMP B (STAGE I)	368
RAMP B (STAGE II)	111
TOTAL	479

PIPE UNDERDRAINS 6"			
EASTBOUND US 34 LT			
LOCATION (US TO DS)	SLOPE (%)	LENGTH (FT)	
410+60 TO 401+05	0.2	955	
415+00 TO 410+60	0.2	440	
427+00 TO 418+00	0.2	900	
436+25 TO 427+00	0.2	925	
445+00 TO 436+25	0.2	875	
454+00 TO 445+25	0.2	875	
462+90 TO 454+25	0.2	865	
475+30 TO 463+00	0.2	1230	
477+00 TO 475+30	0.2	170	
TOTAL (FT)		7235	

PIPE UNDERDRAINS 6" (SPECIAL)			
EASTBOUND US 34 LT			
LOCATION	SLOPE (%)	LENGTH (FT)	
401+05	2.0	26.0	
410+60	2.0	18.7	
418+00	2.0	20.0	
427+00	2.0	20.0	
436+25	2.0	20.4	
445+25	2.0	20.0	
454+25	2.0	18.9	
463+00	2.0	18.0	
475+30	2.0	16.7	
TOTAL (FT)		178.7	

COMBINATION CONCRETE CURB AND GUTTER B6.24	
LOCATION	FOOT
RAMP B 0+29 TO 1+13	35
TOTAL	35

RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	
LOCATION	EACH
JOBSITE	316
TOTAL	316

CLASS SI CONCRETE (OUTLET)	
LOCATION	CU YD
RAMP B 0+29 LT	2.7
RAMP B 1+00 RT	3.2
TOTAL	5.9

MATERIAL TRANSFER DEVICE	
LOCATION	TON
MAINLINE SURFACE AND BINDER	12949
TOTAL	12949

CONCRETE GUTTER, TYPE A	
LOCATION	FOOT
RAMP B 0+29 TO 1+13	95
TOTAL	95

PAVEMENT FABRIC	
LOCATION	SQ YD
CLASS B PATCHES TY III & IV	446
RAMP A 12+49 TO 14+89	426.7
RAMP B 0+29 TO 1+30	649.5
TOTAL	1522.2

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

Schedule of Quantities

SCALE: VERT. DATE: HORIZ. DATE: DRAWN BY: CHECKED BY:

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18RS.(18HB)I	KNOX	TO	8
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

PIPE UNDERDRAINS 4"											
WESTBOUND US 34 LT			WESTBOUND US 34 RT			EASTBOUND US 34 LT			EASTBOUND US 34 RT		
LOCATION (US TO DS)	SLOPE (%)	LENGTH (FT)	LOCATION (US TO DS)	SLOPE (%)	LENGTH (FT)	LOCATION (US TO DS)	SLOPE (%)	LENGTH (FT)	LOCATION (US TO DS)	SLOPE (%)	LENGTH (FT)
363+00 TO 367+50		450	363+00 TO 367+50		450	363+00 TO 367+50		450	363+00 TO 367+50		450
367+50 TO 371+75		425	367+50 TO 371+75		425	367+50 TO 371+75		425	367+50 TO 371+75		425
376+00 TO 372+25	0.2	375	376+00 TO 372+25	0.2	375	376+00 TO 372+25	0.2	375	376+00 TO 372+25	0.2	375
379+77 TO 376+00	0.2	377	379+88 TO 376+00	0.2	388	380+18 TO 376+00	0.2	418	380+29 TO 376+00	0.2	429
385+00 TO 380+04	0.2	496	385+00 TO 380+44	0.2	456	385+00 TO 380+44	0.2	456	385+00 TO 380+74	0.2	426
389+75 TO 385+00	0.2	475	389+75 TO 385+00	0.2	475	389+95 TO 385+00	0.2	495	389+95 TO 385+00	0.2	495
393+00 TO 389+75	0.2	325	393+00 TO 389+75	0.2	325	393+00 TO 390+40	0.2	260	393+00 TO 390+40	0.2	260
397+00 TO 393+00	0.2	400	397+00 TO 393+00	0.2	400	397+00 TO 393+00	0.2	400	397+00 TO 393+00	0.2	400
400+75 TO 397+00	0.2	375	400+75 TO 397+00	0.2	375	400+75 TO 397+00	0.2	375	400+75 TO 397+00	0.2	375
406+00 TO 401+05	0.2	495	406+00 TO 401+05	0.2	495				406+00 TO 401+05	0.2	495
410+00 TO 406+00	0.2	400	410+00 TO 406+00	0.2	400				411+00 TO 406+00	0.2	500
414+60 TO 410+60	0.2	400	414+80 TO 410+60	0.2	420				415+75 TO 411+00	0.2	475
417+90 TO 415+20	0.2	270	417+90 TO 416+00	0.2	190				421+00 TO 417+00	0.2	400
422+50 TO 418+25	0.2	425	422+50 TO 418+25	0.2	425				426+00 TO 421+00	0.2	500
426+90 TO 422+50	0.2	440	426+90 TO 422+50	0.2	440				431+00 TO 426+00	0.2	500
431+50 TO 427+15	0.2	435	431+00 TO 427+15	0.2	385				436+20 TO 431+00	0.2	520
435+90 TO 431+50	0.2	440	435+90 TO 431+00	0.2	490				440+50 TO 436+25	0.2	425
440+50 TO 436+25	0.2	425	440+50 TO 436+25	0.2	425				444+90 TO 440+50	0.2	440
445+25 TO 440+50	0.2	475	445+25 TO 440+50	0.2	475				449+50 TO 445+25	0.2	425
449+50 TO 445+25	0.2	425	449+50 TO 445+25	0.2	425				453+90 TO 449+50	0.2	440
454+25 TO 449+50	0.2	475	454+25 TO 449+50	0.2	475				458+50 TO 454+25	0.2	425
458+50 TO 454+25	0.2	425	458+50 TO 454+25	0.2	425				462+90 TO 458+50	0.2	440
462+90 TO 458+50	0.2	440	462+90 TO 458+50	0.2	440				467+00 TO 463+25	0.2	375
467+00 TO 463+25	0.2	375	467+00 TO 463+25	0.2	375				472+00 TO 467+00	0.2	500
471+00 TO 467+00	0.2	400	471+00 TO 467+00	0.2	400				477+00 TO 472+00	0.2	500
474+95 TO 471+00	0.2	395	474+95 TO 471+00	0.2	395						
477+00 TO 475+30	0.2	170	477+00 TO 475+30	0.2	170						
SUBTOTALS (FT)		11008			10919			3654			10995
TOTAL (FT)											36576

GUARDRAIL TABLE					
LOCATION	STEEL PLATE BEAM GUARD RAIL TYPE A	TRAFFIC BARRIER TERMINAL TYPE 1 TANGENT SPECIAL	TRAFFIC BARRIER TERMINAL TYPE 6	TERMINAL MARKER DIRECT APPLIED	GUARDRAIL MARKERS TYPE A
	FOOT	EACH	EACH	EACH	EACH
WESTBOUND US 34					
RT 351+82.93 TO 352+26.68			1		
RT 352+26.68 TO 355+14.18	287.5				5
RT 355+14.18 TO 355+64.18		1		1	
EASTBOUND US 34					
LT 346+34.76 TO 346+84.76		1		1	
LT 346+84.76 TO 349+72.26	287.5				5
LT 349+72.26 TO 350+16.01			1		
FREMONT STREET					
			4		
TOTALS	575	2	6	2	10

SHOULDER RUMBLE STRIP	
LOCATION	TON
MAINLINE EXCEPT RAMP TAPER & GORE	44100
TOTAL	44100

DELINEATORS (SPECIAL)	
LOCATION	EACH
EASTBOUND MEDIAN 454+00	1
EASTBOUND MEDIAN 4623+00	1
WESTBOUND MEDIAN 462+50	1
WESTBOUND MEDIAN 470+50	1
TOTAL	4

GUARDRAIL AGGREGATE EROSION CONTROL	
LOCATION	TON
US 34 EB 346+34.76 TO 350+16.01	1.6
US 34 WB 351+82.93 TO 355+64.18	1.6
TOTAL	3.2

GUARD RAIL REMOVAL	
LOCATION	FOOT
WESTBOUND US 34	
351+83 TO 353+40	165
EASTBOUND US 34	
348+51 TO 350+16	165
FREMONT STREET	200
TOTAL	530

CLASS D PATCHES, TYPE II, 18 INCH	
LOCATION	SQ YD
FREMONT STREET	
WEST APPROACH	9.8
EAST APPROACH	9.8
TOTAL	19.6

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

Schedule of Quantities

SCALE: VERT. _____ HORIZ. _____ DATE _____ DRAWN BY _____ CHECKED BY _____

PLOT DATE = 10/10/2007
 FILE NAME = c:\p\projects\18rs\18rs\18rs.dgn
 PLOT SCALE = 1/8"=1'-0"
 USER NAME = hudson

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18RS,18H01	KNOX	70	9
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

PIPE UNDERDRAIN 4" (SPECIAL)											
WESTBOUND US 34 LT			WESTBOUND US 34 RT			EASTBOUND US 34 LT			EASTBOUND US 34 RT		
LOCATION	SLOPE (%)	LENGTH (FT)	LOCATION	SLOPE (%)	LENGTH (FT)	LOCATION	SLOPE (%)	LENGTH (FT)	LOCATION	SLOPE (%)	LENGTH (FT)
367+50	2.0	23.9	367+50	2.0	19.0	367+50	2.0	19.0	367+50	2.0	23.3
371+75	2.0	28.4	371+75	2.0	19.0	371+75	2.0	20.8	371+75	2.0	30.8
372+25	2.0	24.0	372+25	2.0	20.5	372+25	2.0	19.5	372+25	2.0	32.3
376+00	2.0	21.4	376+00	2.0	18.8	376+00	2.0	20.1	376+00	2.0	26.2
380+04	2.0	18.7	380+44	2.0	17.4	380+44	2.0	19.0	380+74	2.0	36.2
385+00	2.0	20.7	385+00	2.0	19.4	385+00	2.0	21.6	385+00	2.0	27.5
389+75	2.0	21.5	389+75	2.0	19.6	390+40	2.0	19.1	390+40	2.0	31.5
393+00	2.0	19.5	393+00	2.0	19.5	393+00	2.0	20.2	393+00	2.0	26.9
397+00	2.0	19.0	397+00	2.0	22.9	397+00	2.0	20.0	397+00	2.0	29.6
401+05	2.0	23.7	401+05	2.0	17.0				401+05	2.0	24.5
406+00	2.0	27.8	406+00	2.0	19.3				406+00	2.0	24.8
410+60	2.0	63.9	410+60	2.0	20.7				411+00	2.0	26.2
415+20	2.0	24.4	416+00	2.0	17.5				417+40	2.0	21.7
418+20	2.0	25.7	418+20	2.0	17.9				421+00	2.0	22.7
422+50	2.0	24.2	422+50	2.0	17.5				426+00	2.0	24.0
427+15	2.0	24.5	427+15	2.0	17.2				431+00	2.0	22.0
431+50	2.0	25.6	431+00	1.5	18.9				436+25	2.0	22.7
436+25	2.0	30.5	436+25	2.0	19.6				440+50	2.0	22.7
440+50	2.0	25.7	440+50	1.5	19.2				445+25	2.0	29.5
445+25	2.0	26.7	445+25	2.0	19.3				449+50	2.0	22.9
449+50	2.0	26.8	449+50	2.0	17.9				454+25	2.0	28.4
454+25	2.0	25.2	454+25	2.0	18.8				458+50	2.0	23.7
458+50	2.0	27.9	458+50	2.0	19.8				463+25	2.0	29.7
463+25	2.0	24.3	463+25	2.0	17.0				467+00	2.0	20.2
467+00	2.0	23.9	467+00	2.0	18.0				472+00	2.0	21.5
471+00	2.0	24.0	471+00	1.0	18.3						
475+30	2.0	26.3	475+30	2.0	15.5						
SUBTOTALS (FT)		698.2			505.5			179.3			651.5
TOTAL (FT)		2034.5									

SHORT TERM PAVEMENT MARKING					WORK ZONE PAVEMENT MARKING REMOVAL
LOCATION	LENGTH	CL DASH FOOT	SHLDR DIAG FOOT	ALL APPS FOOT	
WESTBOUND US 34					SQ FT
362+40 TO 477+60	11520	1152	922		SHORT TERM PAVEMENT MARKING X 1/3 (WIDTH)
EASTBOUND US 34					
362+40 TO 477+60	11520	1152	922		
SUBTOTAL		4148			
X 3 APPLICATIONS				12444	
STAGE CONSTRUCTION (STAGES II)				328	
STAGE CONSTRUCTION (STAGES III)				760	
TOTAL				13532	4511

SHOULDER REMOVAL AND REPLACEMENT	
LOCATION	FOOT
PIPE UNDERDRAIN PLACEMENT	44585
TOTAL	44585

PERMANENT AND TEMPORARY PAVEMENT MARKINGS											RAISED REFLECTIVE PAVEMENT MARKERS	
LOCATION	LENGTH	EPOXY 4"				PREFORMED PLASTIC 6"	EPOXY 8"		EPOXY 12"	EPOXY 24"		EPOXY LETTERS AND SYMBOLS
		WHITE SOLID		YELLOW SOLID			WHITE SOLID	DASH	WHITE DIAGONAL RAMP	WHITE STOP BAR		
		LT	RT	LT	RT	WHITE DASH	FOOT	FOOT	FOOT	FOOT		FOOT
WESTBOUND US 34												
362+40 TO 477+60	11520	11520			11520	2880						144
EASTBOUND US 34												
362+40 TO 477+60	11520		11520	11520		2880						144
MAIN ST. RAMP A												
0+00 TO 2+85	285							72				
2+85 TO 7+30	445						890		414			
7+30 TO 22+25	1495		1495	1495								
22+25 TO 24+24	199		253	199			302			36	31.2	
MAIN ST. RAMP B												
0+29 TO 1+30	101		155	120			80				15.6	
1+30 TO 10+91	961		961	961								
10+91 TO 16+45	554						1108					28
16+45 TO 23+32	687							172				
SUB TOTALS		11520	14384	14295	11520		2380	244				
TOTALS		51719				5760	2624	414	36	46.8	316	

REVISIONS	
NAME	DATE

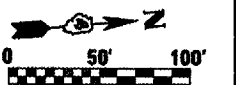
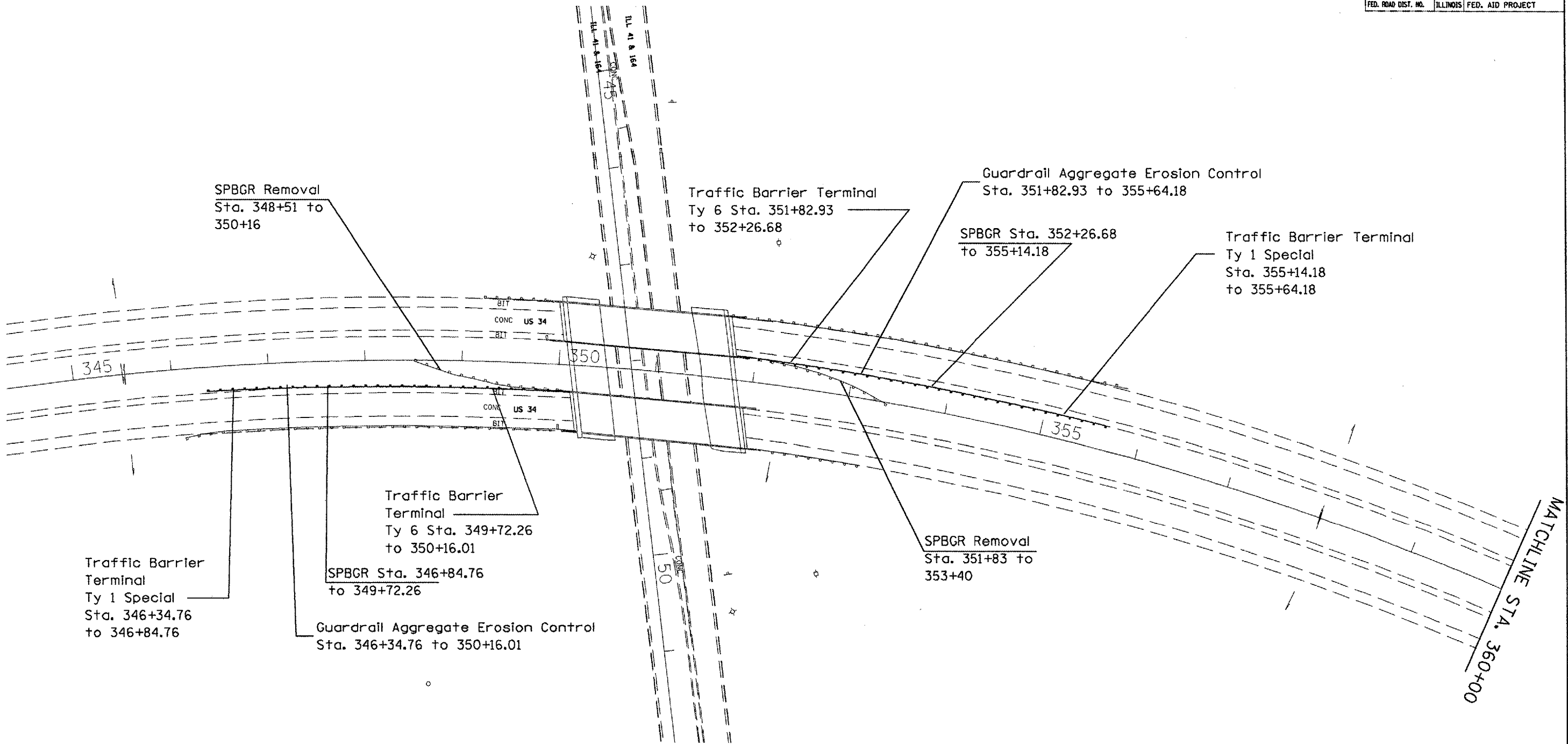
ILLINOIS DEPARTMENT OF TRANSPORTATION

Schedule of Quantities

SCALE: VERT. HORIZ. DATE

DRAWN BY CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18RS,18HB1I	KNOX	70	10
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

Proposed Plan

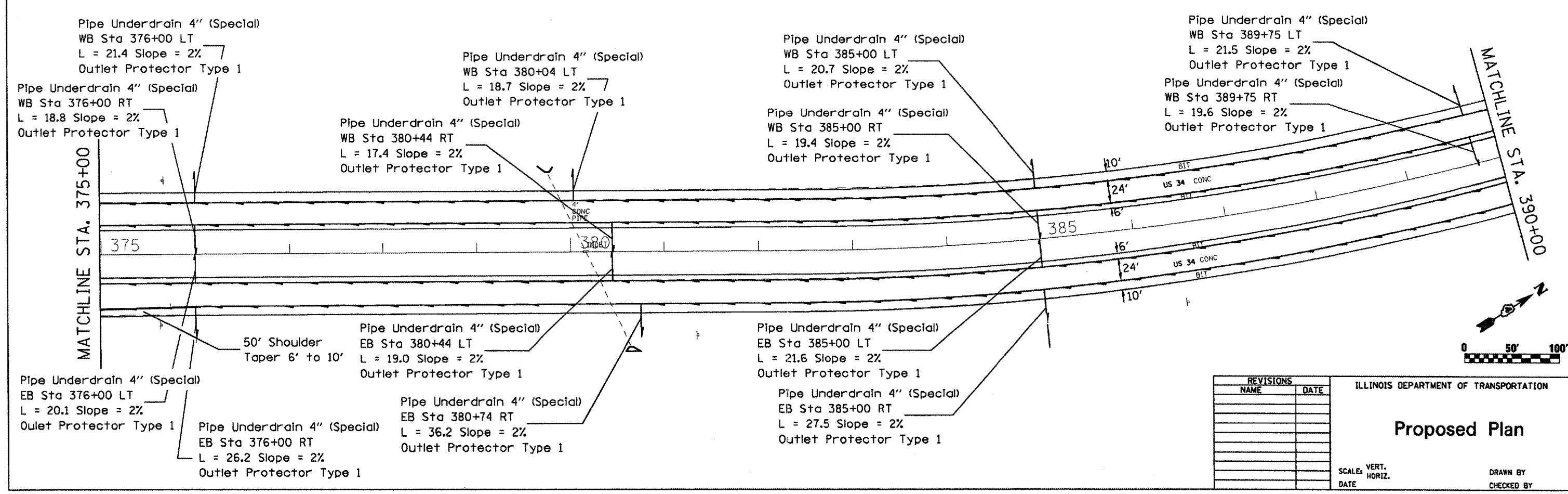
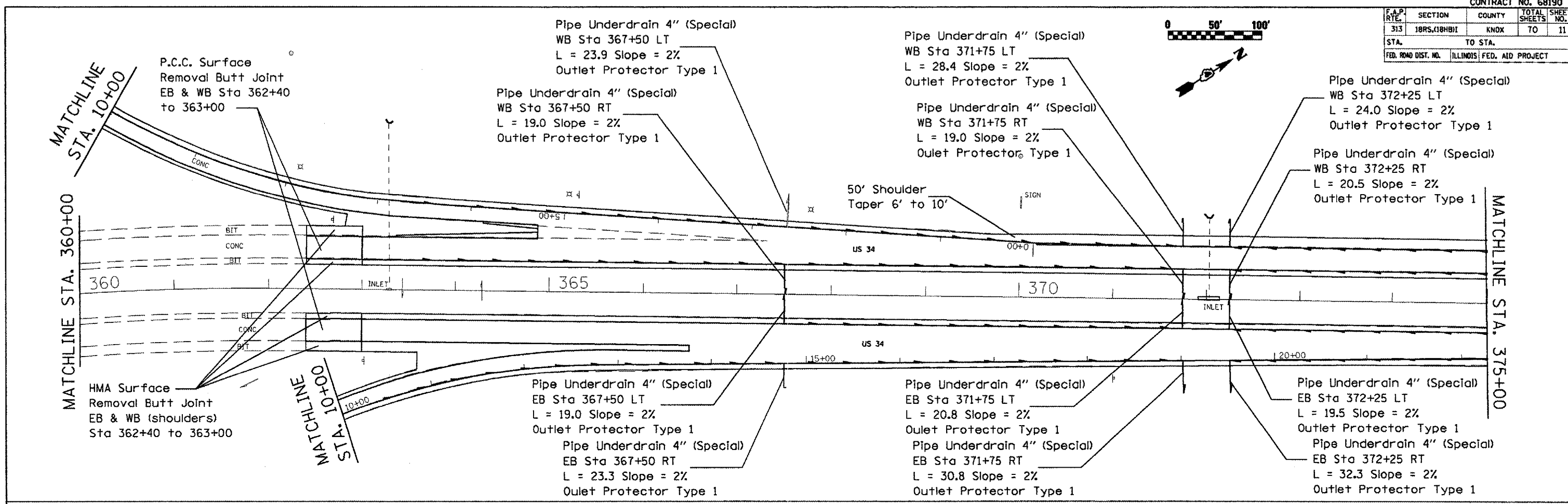
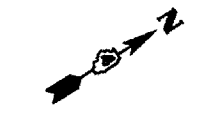
SCALE: VERT. _____
HORIZ. _____

DATE _____

DRAWN BY _____
CHECKED BY _____

PLOT DATE : 08/15/2007
 PLOT SCALE : 1/8"=1'-0"
 USER NAME : hndh100nm

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18RS.18HB1	KNOX	70	11
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

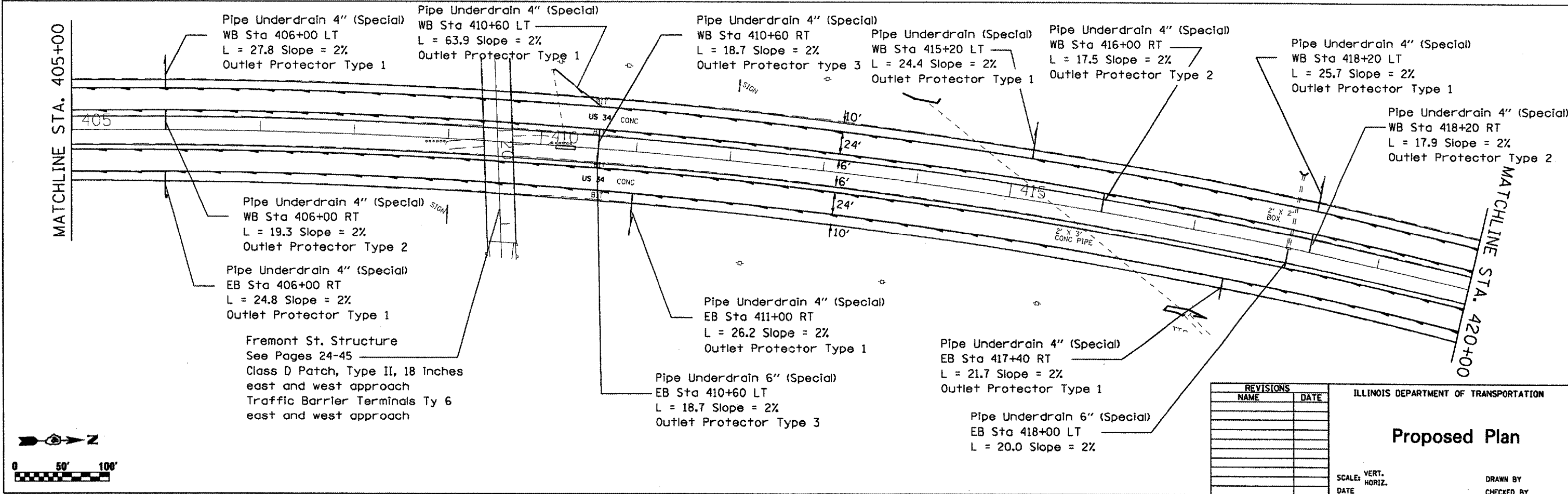
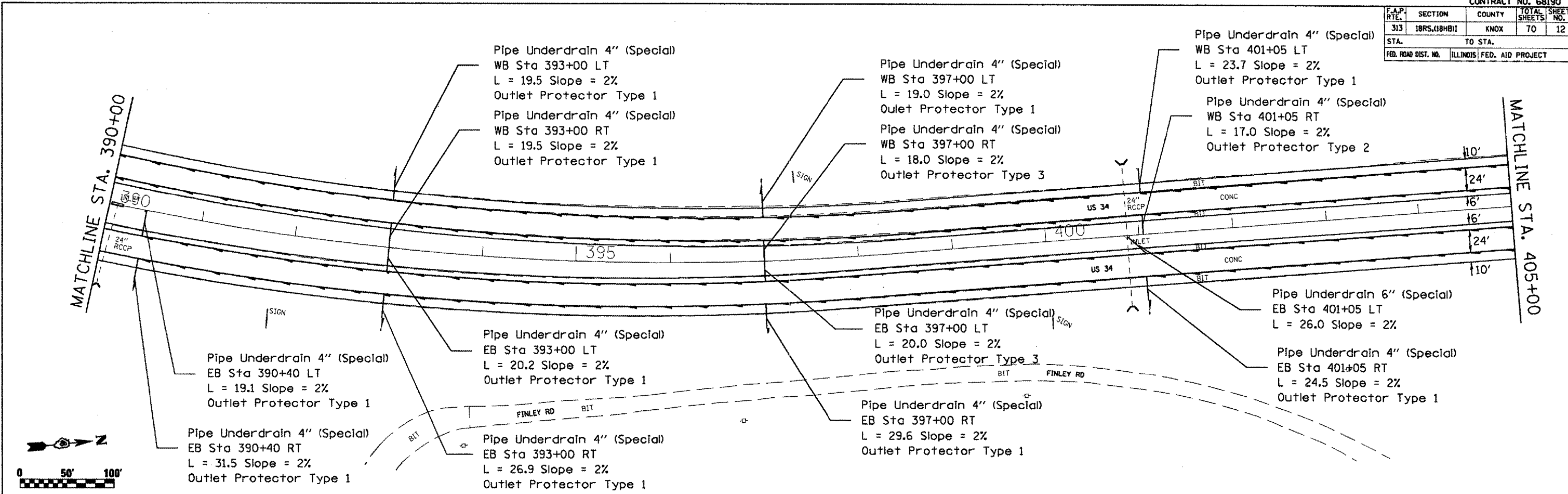
Proposed Plan

SCALE: VERT. HORIZ. DATE

DRAWN BY CHECKED BY

PLOT DATE = 10/18/2007
 FILE NAME = c:\p\projects\18rs18hb1\general2.dgn
 PLOT SCALE = 1/8"=1'-0"
 USER NAME = huckstetter

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18RS,(18HBI)	KNOX	70	12
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



REVISIONS	
NAME	DATE

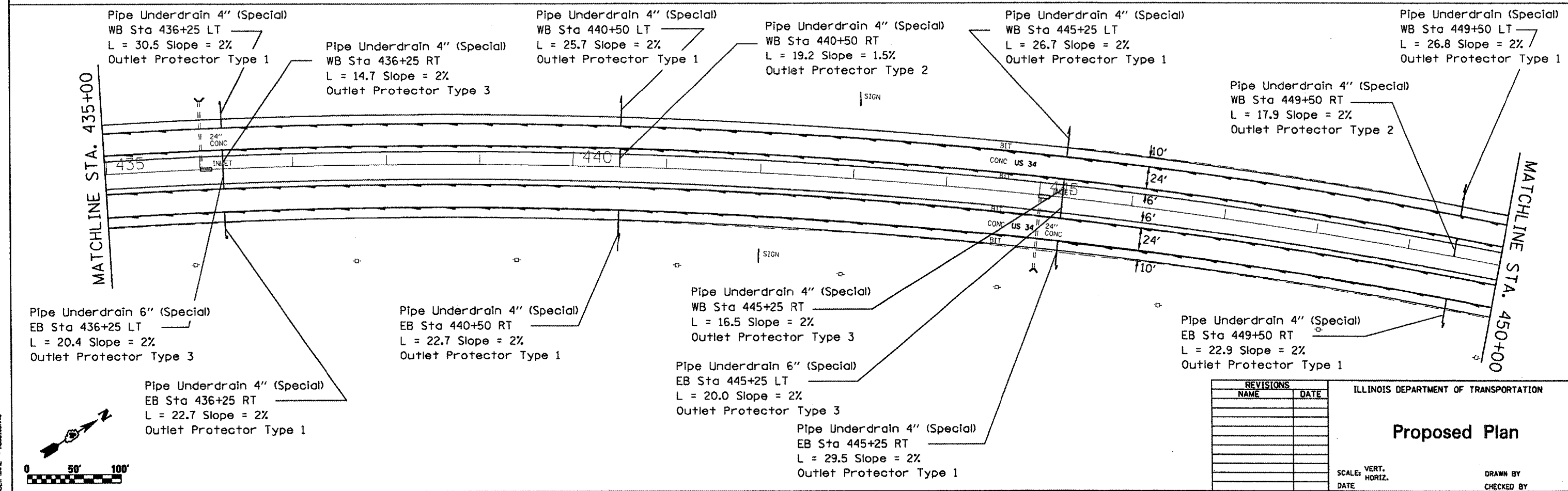
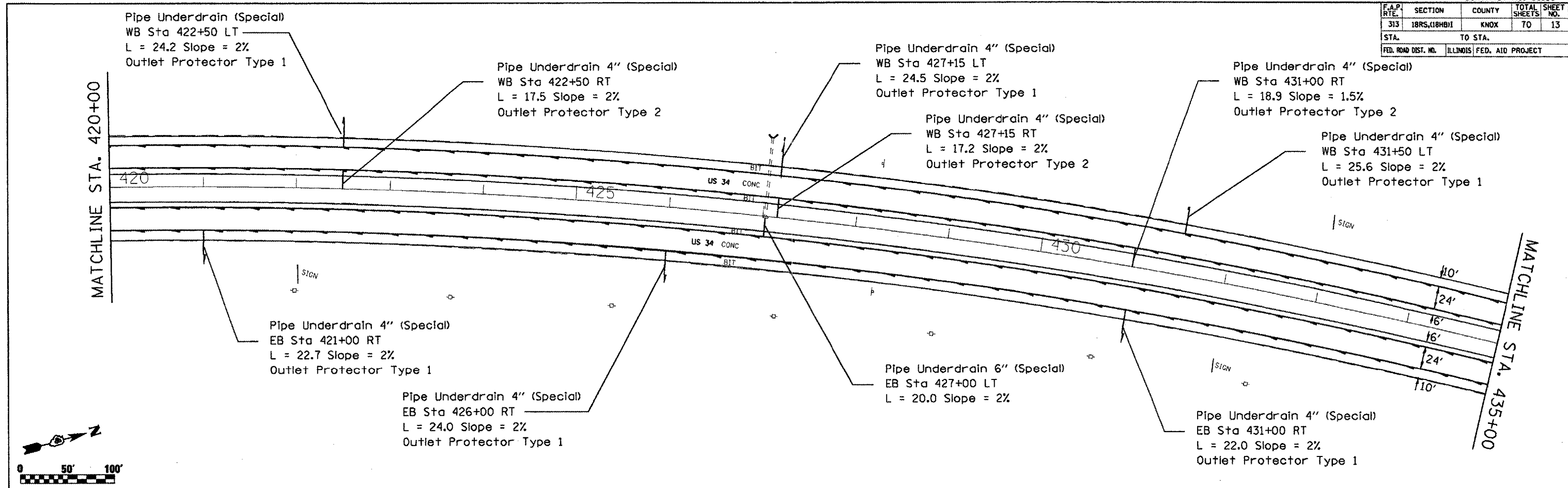
ILLINOIS DEPARTMENT OF TRANSPORTATION

Proposed Plan

VERT. SCALE: _____
 HORIZ. SCALE: _____
 DATE: _____
 DRAWN BY: _____
 CHECKED BY: _____

PLOT DATE = 10/16/2007
 PLOT SCALE = 1/8" = 100'
 USER NAME = huda@iastate.edu

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18RS,18HB1	KNOX	70	13
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



REVISIONS	
NAME	DATE

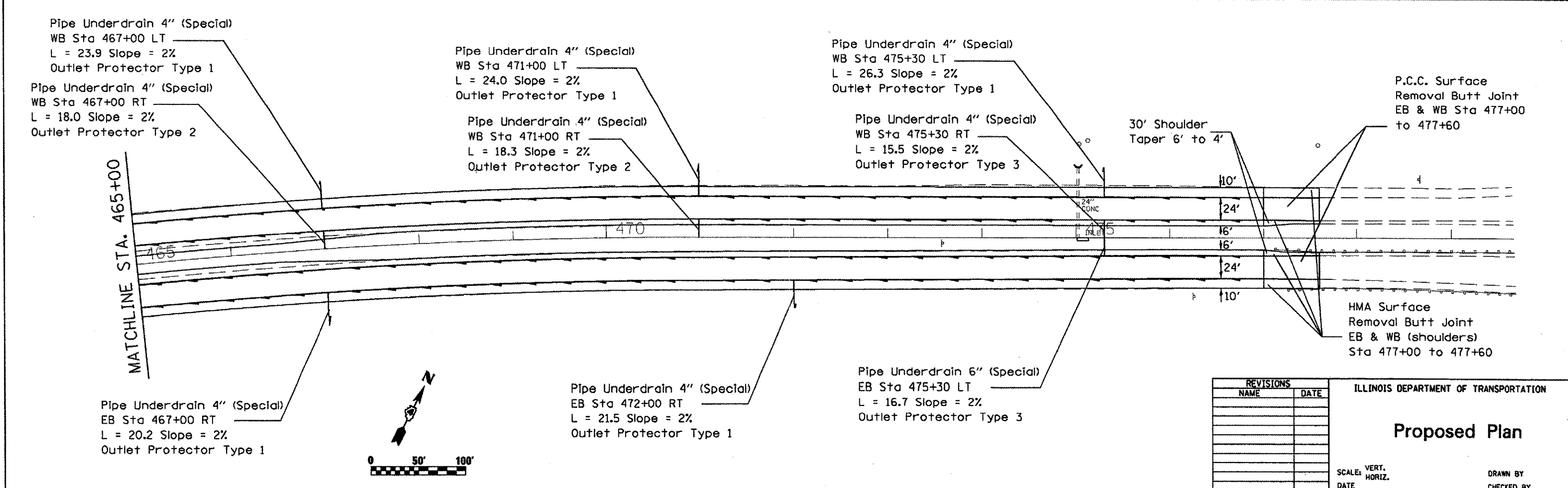
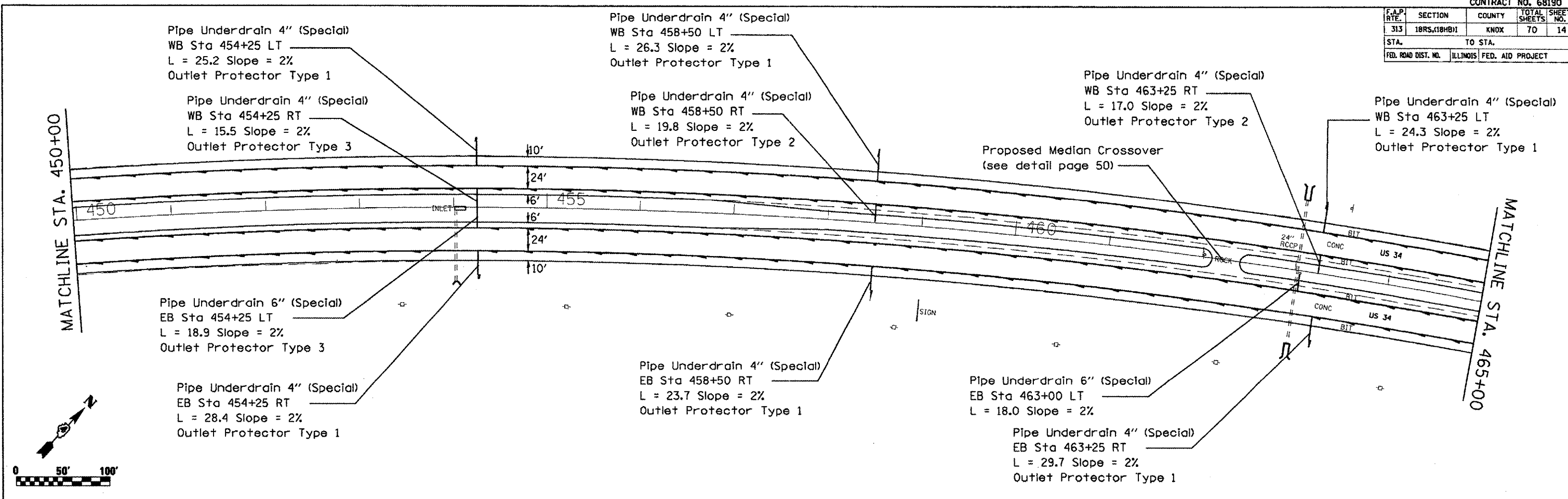
ILLINOIS DEPARTMENT OF TRANSPORTATION

Proposed Plan

SCALE: VERT. _____ HORIZ. _____ DATE _____ DRAWN BY _____ CHECKED BY _____

PLOT DATE = 10/12/2007
 FILE NAME = c:\p00\68190\18rs\18hb1\13\plan\13.dgn
 PLOT SCALE = 1/8" = 1'-0"
 USER NAME = hnd13eom

F.A.P. RYE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18RS,(18HB)	KNOX	70	14
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

Proposed Plan

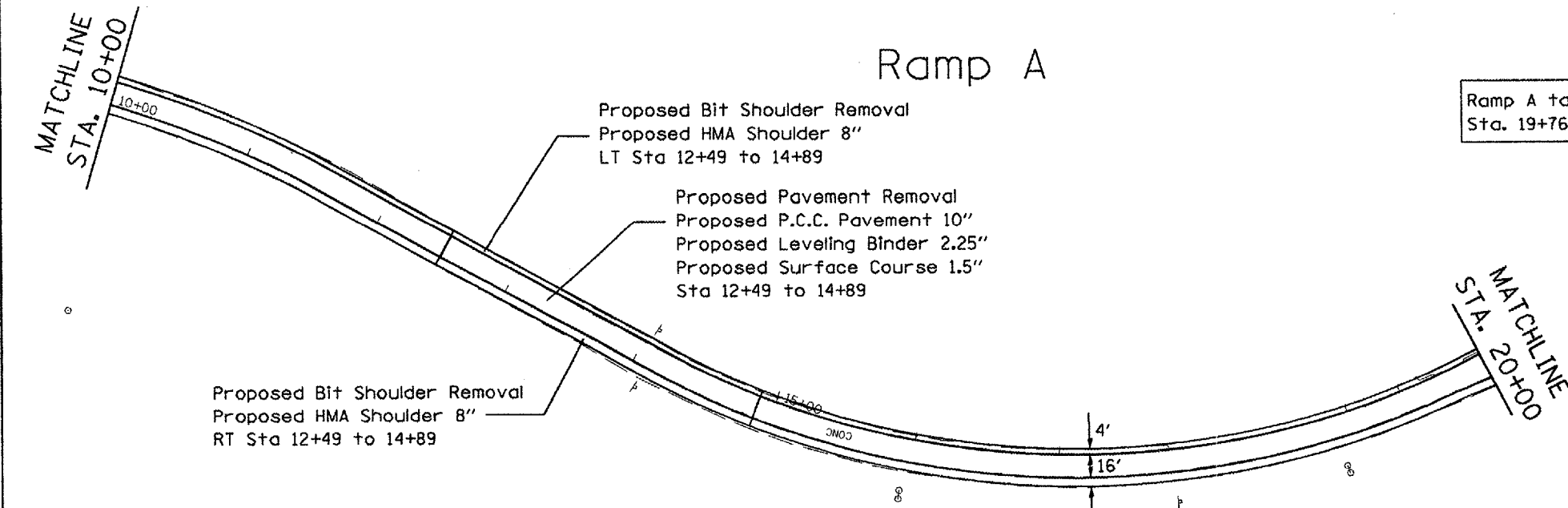
SCALE: VERT. _____
HORIZ. _____

DATE _____

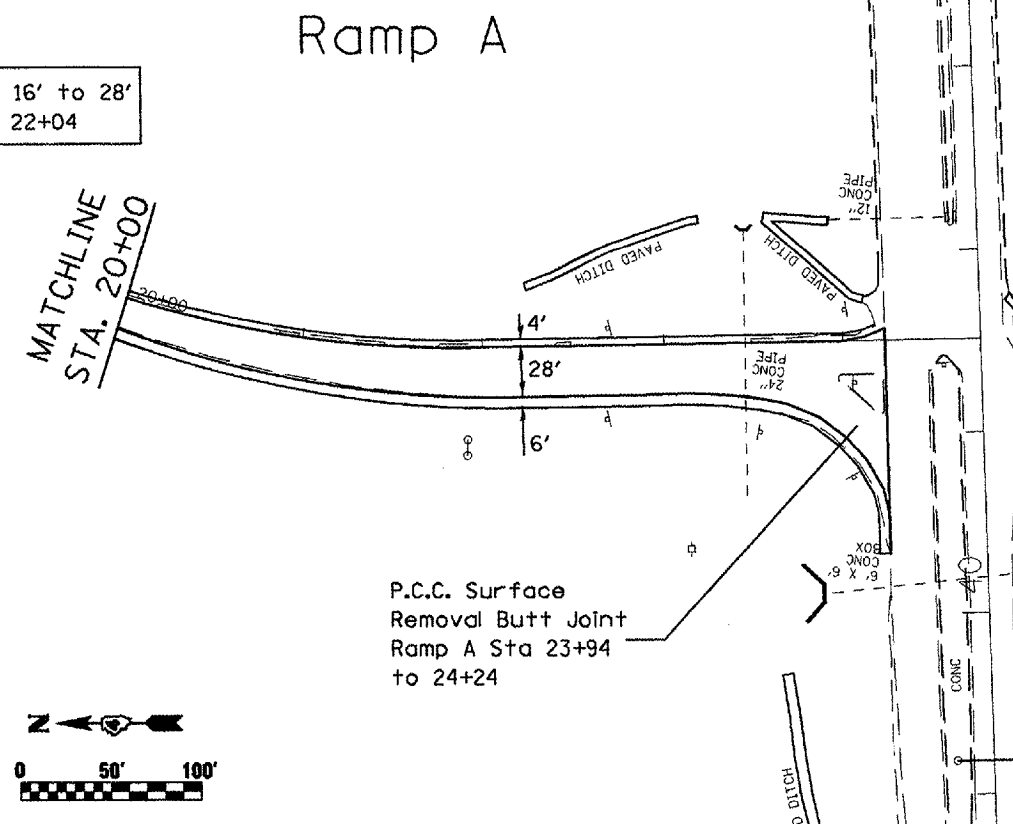
DRAWN BY _____
CHECKED BY _____

PLOT DATE = 10/18/2007
 PLOT SCALE = 1/8"=1'-0"
 USER NAME = hnd1101000

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18RS.18MB1	KNOX	70	15
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

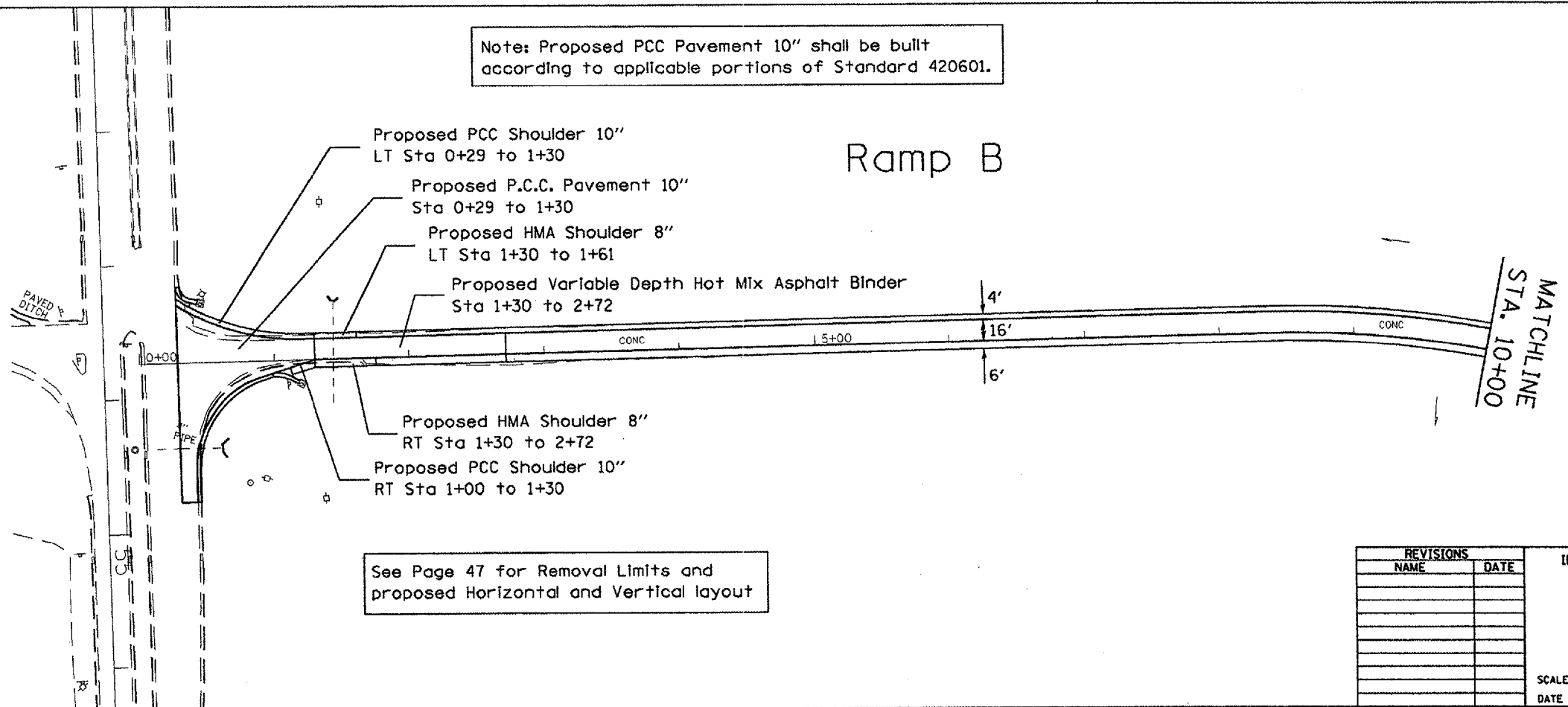
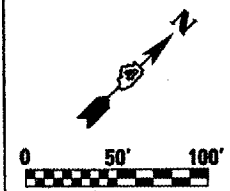


Ramp A taper 16' to 28'
Sta. 19+76 to 22+04



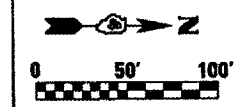
Note: Proposed PCC Pavement 10" shall be built according to applicable portions of Standard 420601.

Note: Proposed PCC Pavement 10" shall be built according to applicable portions of Standard 420601.



Ramp B

See Page 47 for Removal Limits and proposed Horizontal and Vertical layout



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

Proposed Plan

SCALE: VERT. _____
HORIZ. _____

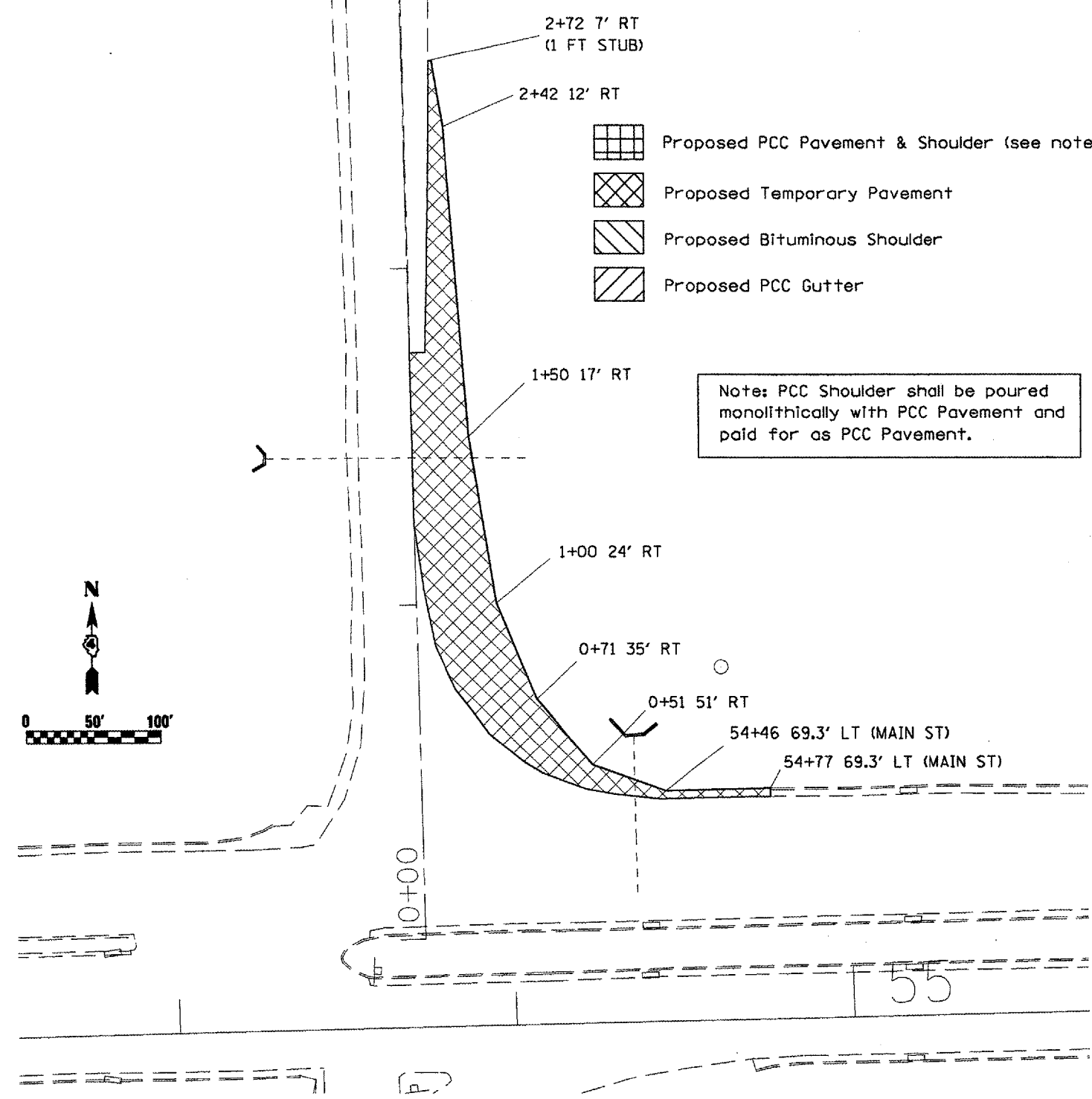
DATE _____ DRAWN BY _____
CHECKED BY _____

PLOT DATE: 10/15/2007
 FILE NAME: c:\projects\10818\10818.dwg
 USER: jk
 PLOT SCALE: 1/8" = 1'-0"
 PLOT NAME: 10818.dwg

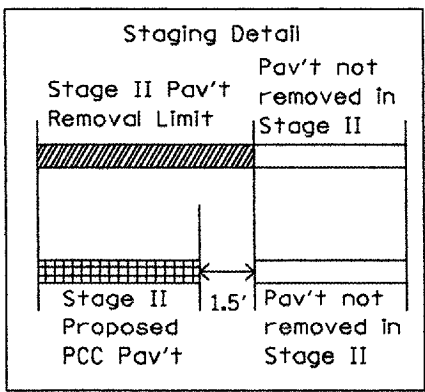
F.A.P. RYE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18RS,18HB1	KNOX	70	16
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

Stage I

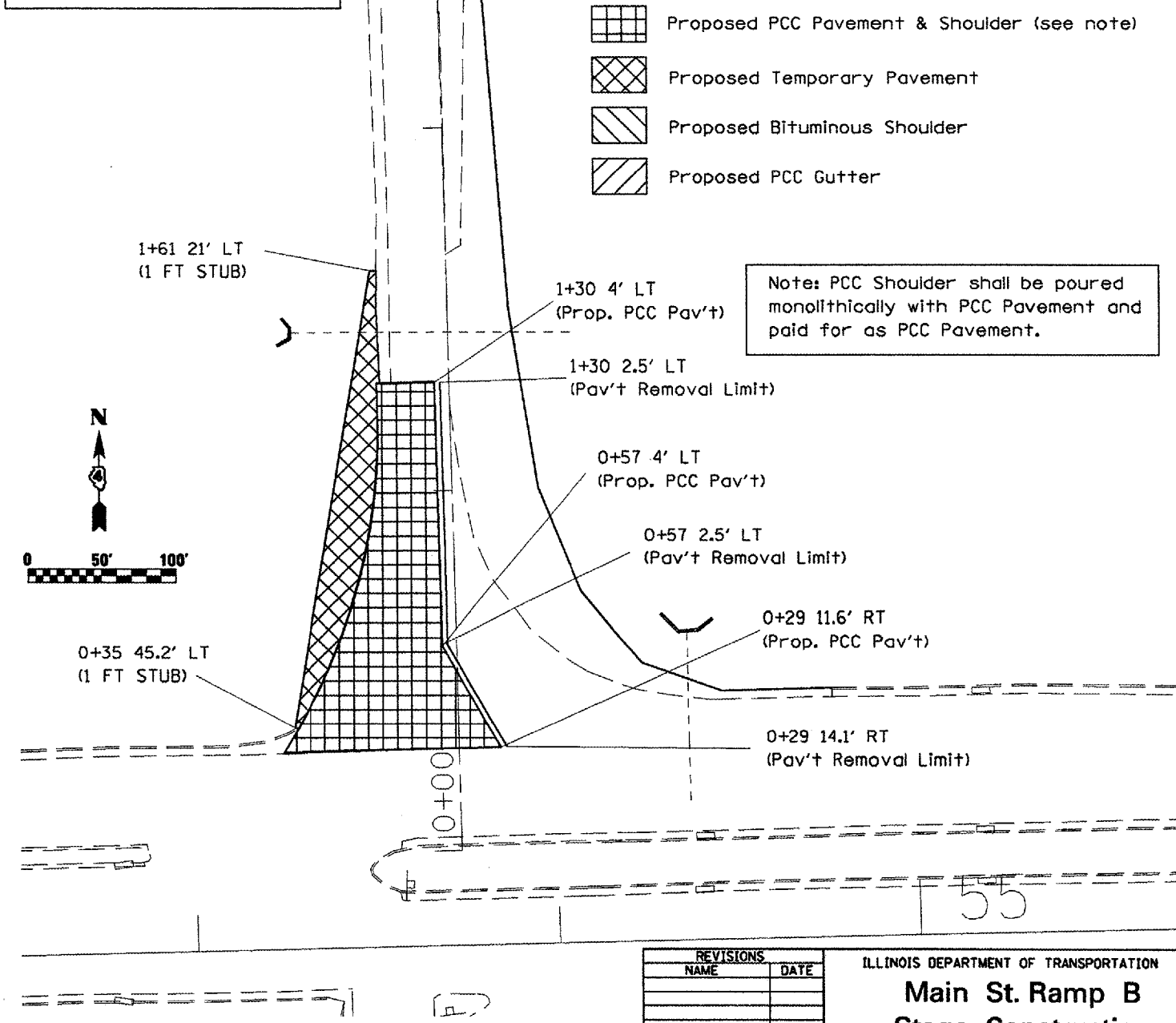
- Stage I Removal**
Remove Combination Concrete Curb and Gutter RT
Remove Gutter Outlet RT
- Stage I Construction**
Construct Temporary Pavement RT
- Stage I Traffic Control**
Standard 701701 and as directed by the Engineer



Stage II



- Stage II Removal**
Remove Pavement LT
Remove Bituminous Shoulder LT
Remove Gutter Outlet LT
- Stage II Construction**
Construct PCC Pavement & Shoulder LT
Construct Temporary Pavement LT
- Stage II Traffic Control**
Standard 701602 (Sp1)
(See Maintenance of Traffic detail page 18)



PLOT DATE = 10/16/2007
 FILE NAME = C:\pwork\18rs\18hb1\general12.dgn
 USER NAME = hndbls1000

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
**Main St. Ramp B
 Stage Construction
 Stages I & II**

SCALE: VERT. _____
 HORIZ. _____
 DATE _____

DRAWN BY _____
 CHECKED BY _____





F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18RS, (18HB)1	KNOX	70	17
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

Stage III

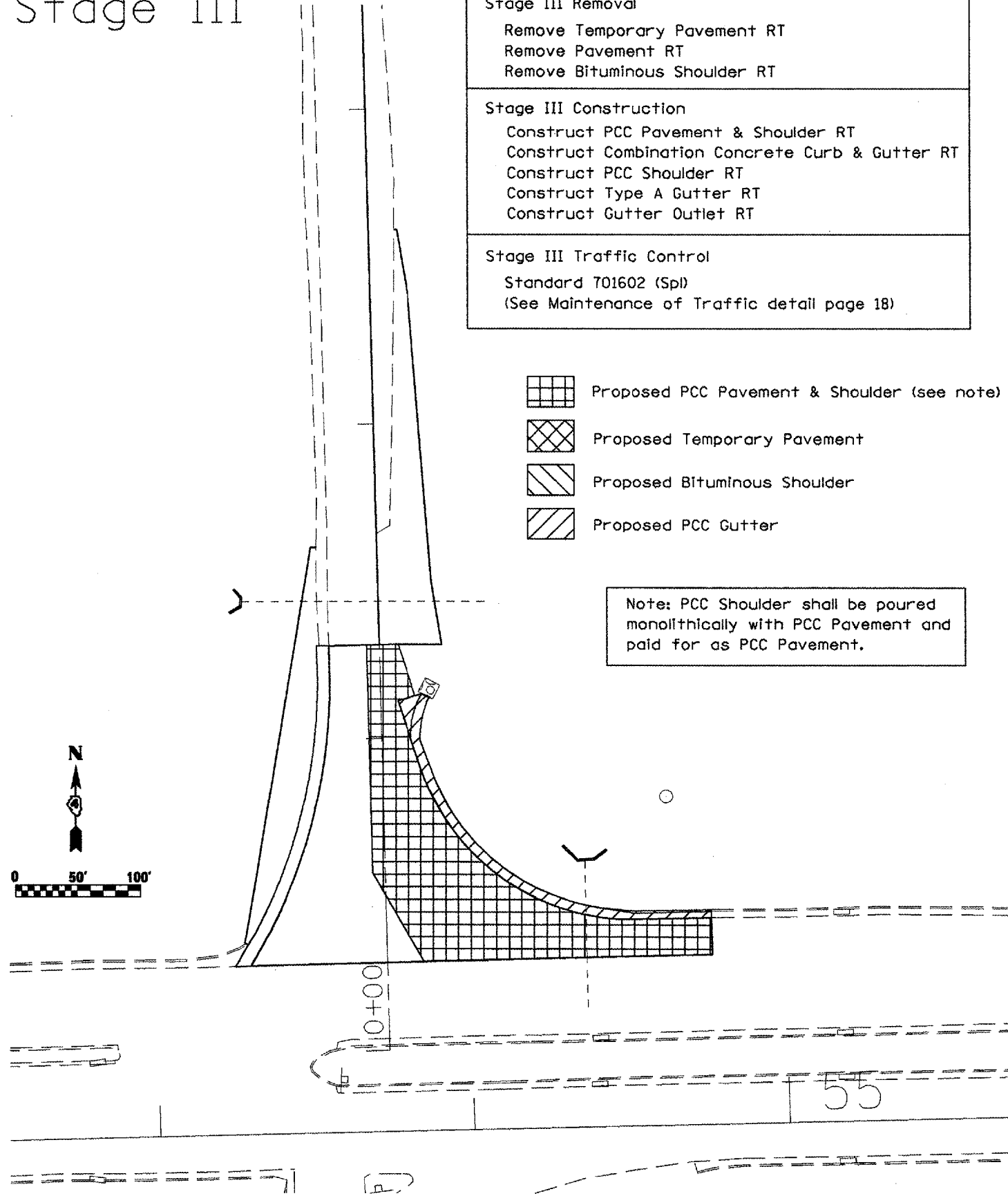
Stage III Removal
 Remove Temporary Pavement RT
 Remove Pavement RT
 Remove Bituminous Shoulder RT

Stage III Construction
 Construct PCC Pavement & Shoulder RT
 Construct Combination Concrete Curb & Gutter RT
 Construct PCC Shoulder RT
 Construct Type A Gutter RT
 Construct Gutter Outlet RT

Stage III Traffic Control
 Standard 701602 (Spl)
 (See Maintenance of Traffic detail page 18)

-  Proposed PCC Pavement & Shoulder (see note)
-  Proposed Temporary Pavement
-  Proposed Bituminous Shoulder
-  Proposed PCC Gutter

Note: PCC Shoulder shall be poured monolithically with PCC Pavement and paid for as PCC Pavement.







Stage IV

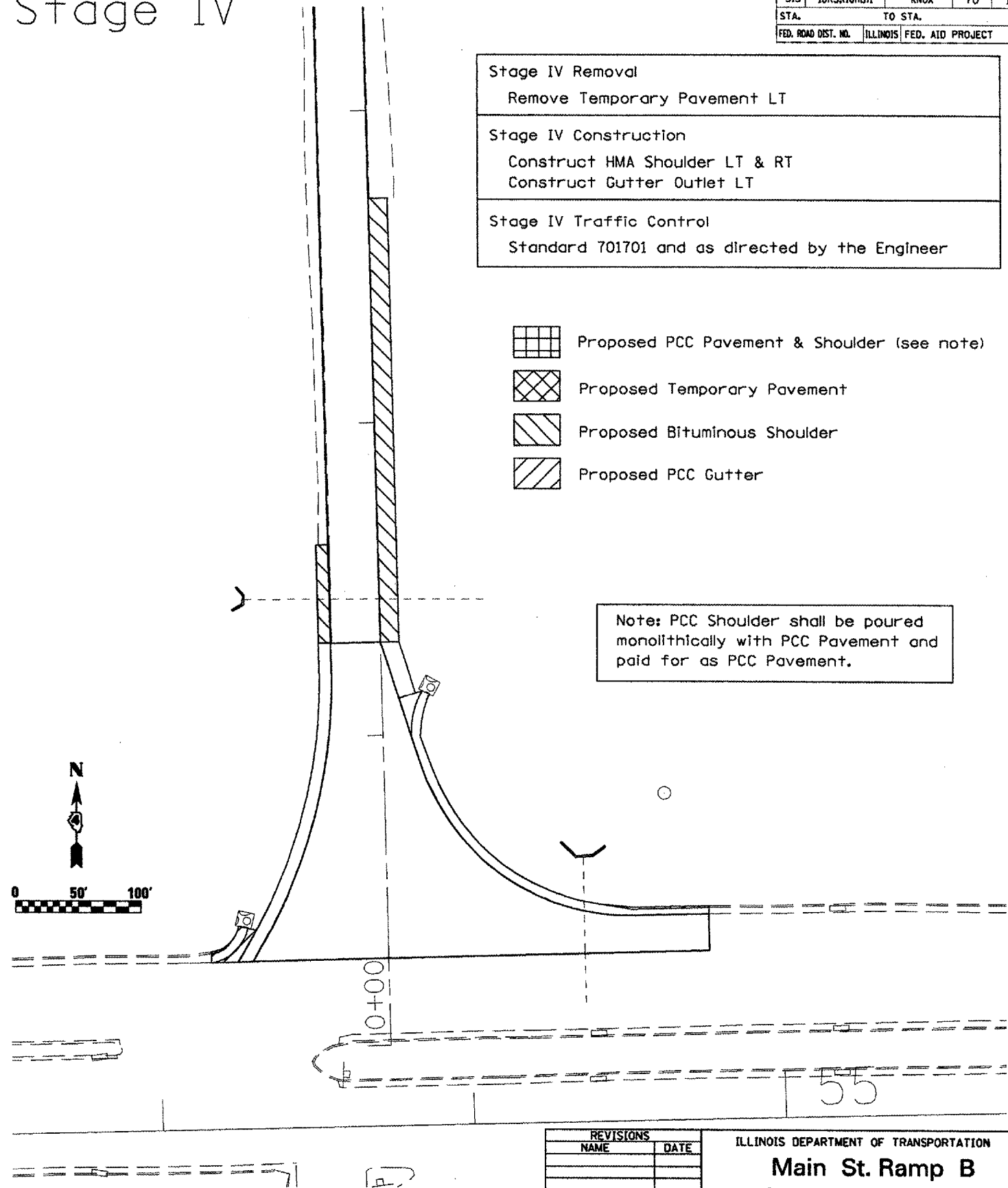
Stage IV Removal
 Remove Temporary Pavement LT

Stage IV Construction
 Construct HMA Shoulder LT & RT
 Construct Gutter Outlet LT

Stage IV Traffic Control
 Standard 701701 and as directed by the Engineer

-  Proposed PCC Pavement & Shoulder (see note)
-  Proposed Temporary Pavement
-  Proposed Bituminous Shoulder
-  Proposed PCC Gutter

Note: PCC Shoulder shall be poured monolithically with PCC Pavement and paid for as PCC Pavement.



PLOT DATE = 10/19/2007
 PLOT SCALE = 1/4" = 10'-0"
 USER NAME = hudson

REVISIONS	
NAME	DATE

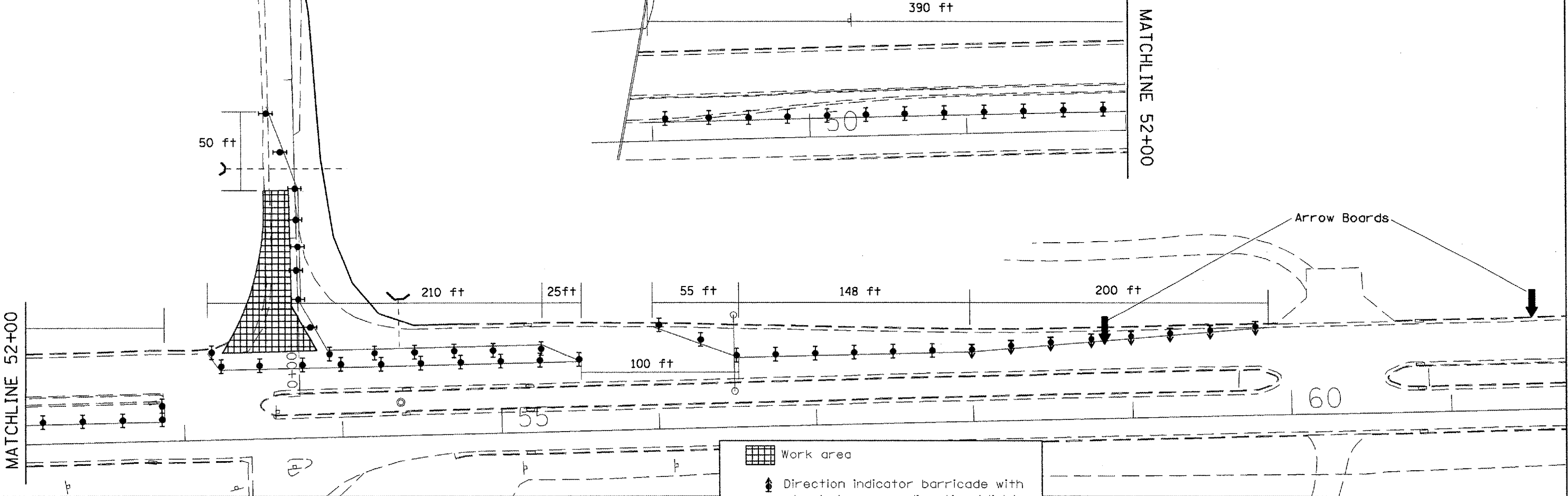
ILLINOIS DEPARTMENT OF TRANSPORTATION
Main St. Ramp B
Stage Construction
Stages III & IV

SCALE: VERT. _____
 HORIZ. _____

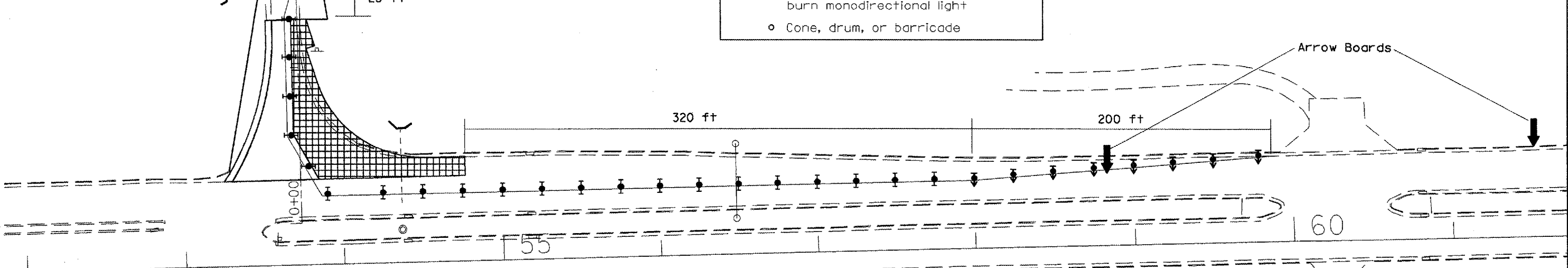
DRAWN BY _____
 CHECKED BY _____

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18RS,18HB1	KNOX	70	18
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

Stage II



Stage III



- Work area
- Direction indicator barricade with steady burn monodirectional light
- Type II barricade, drum, or vertical barricade with steady burn monodirectional light
- Cone, drum, or barricade

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

701602 (Special)

Maintenance of Traffic

not to scale

SCALE: VERT. _____

HORIZ. _____

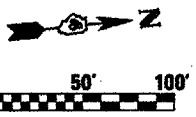
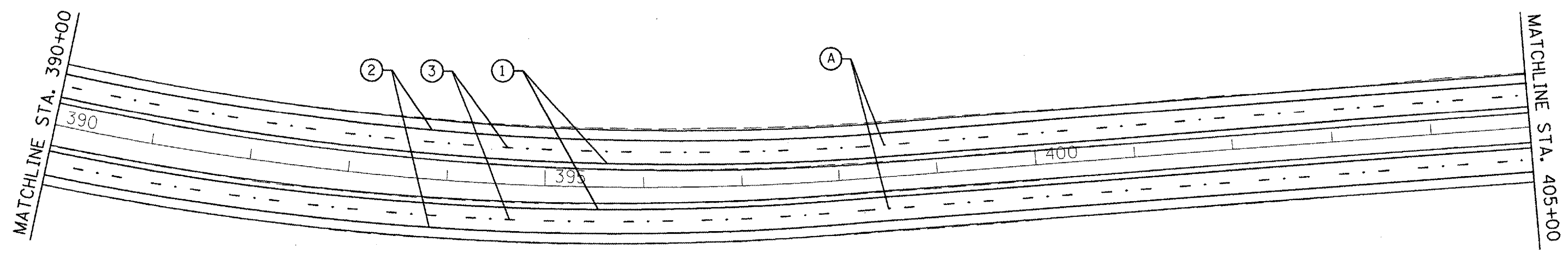
DATE _____

DRAWN BY _____

CHECKED BY _____

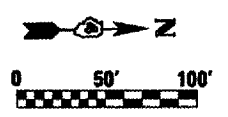
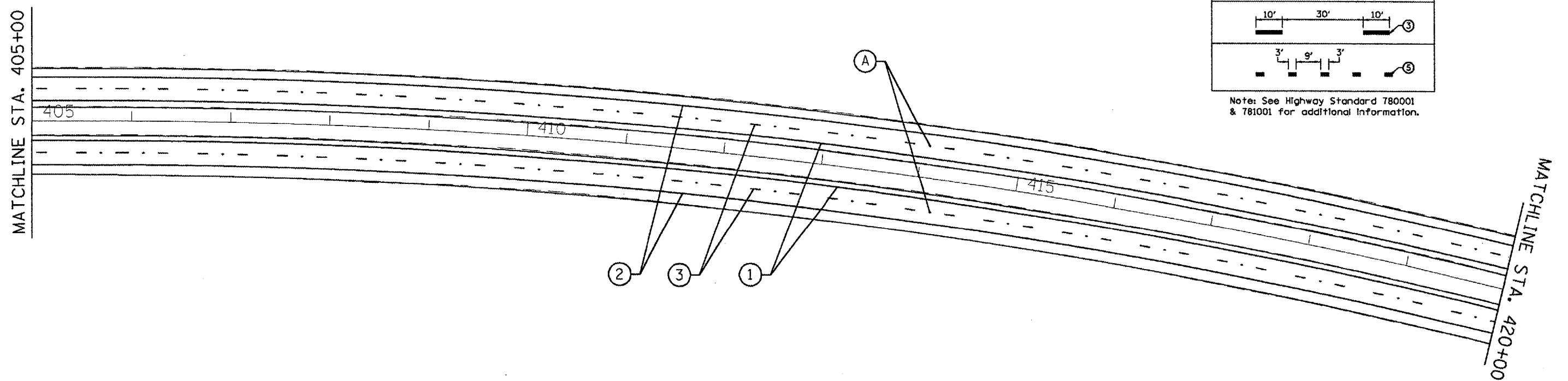
PLOT DATE = 10/15/2007
 FILE NAME = c:\projects\701602\stage2.dgn
 PLOT SCALE = 63.6111 / IN.
 USER NAME = hndh10000

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18RS.18HB11	KNOX	70	20
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



1. 4" Yellow Solid
2. 4" White Solid
3. 6" White Dash
4. 8" White Solid
5. 8" White Dash
6. 12" White Diagonal
7. 24" White Stop Bar
8. Letters & Symbols
A. One-Way Amber @ 40' C-C.

Note: See Highway Standard 780001 & 781001 for additional information.



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

Pavement Markings

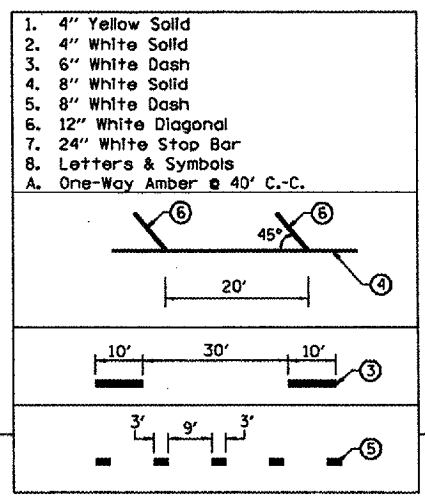
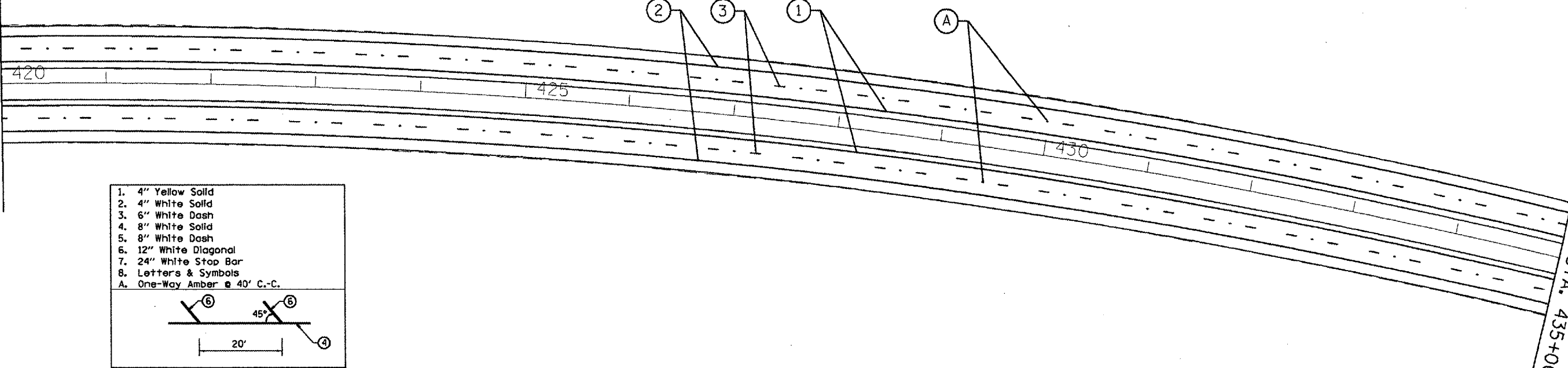
SCALE: VERT. _____
HORIZ. _____

DRAWN BY _____
CHECKED BY _____

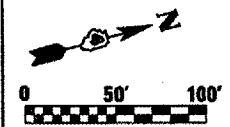
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313	18RS, (18HBI)	KNOX	70	21
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

MATCHLINE STA. 420+00

MATCHLINE STA. 435+00

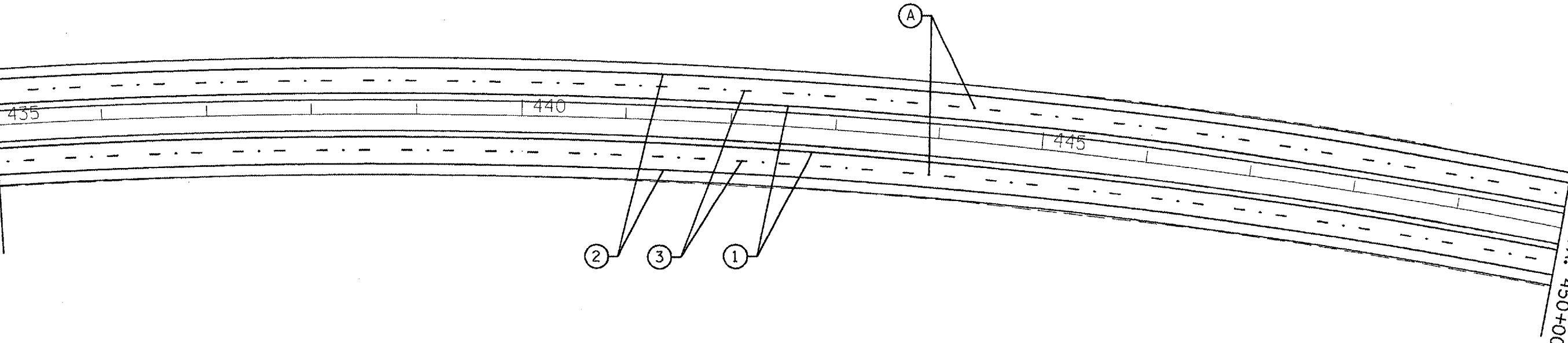


Note: See Highway Standard 780001 & 781001 for additional information.



MATCHLINE STA. 435+00

MATCHLINE STA. 450+00



REVISIONS	
NAME	DATE

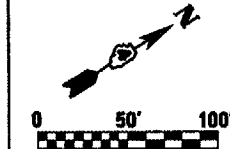
ILLINOIS DEPARTMENT OF TRANSPORTATION

Pavement Markings

SCALE: VERT. _____
HORIZ. _____

DATE _____

DRAWN BY _____
CHECKED BY _____



PLOT DATE = 10/18/2007
 PLOT SCALE = 1/8" = 100'
 USER NAME = hndh10000

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18RS,18HB1	KNOX	70	22
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

MATCHLINE STA. 450+00

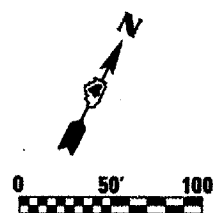
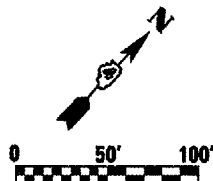
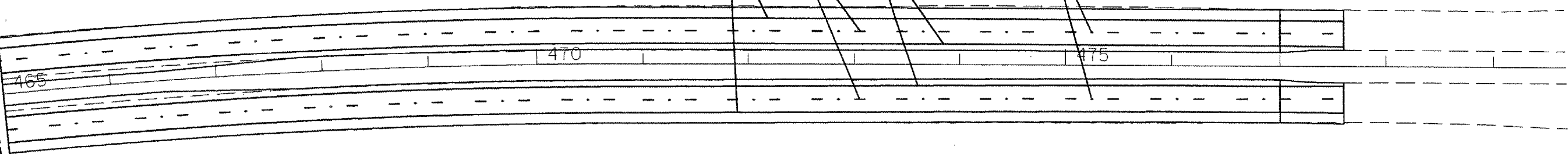
MATCHLINE STA. 465+00



1. 4" Yellow Solid
2. 4" White Solid
3. 6" White Dash
4. 8" White Solid
5. 8" White Dash
6. 12" White Diagonal
7. 24" White Stop Bar
8. Letters & Symbols
A. One-Way Amber @ 40' C.-C.

Note: See Highway Standard 780001 & 781001 for additional information.

MATCHLINE STA. 465+00



PLOT DATE = 06/16/2007
 PLOT SCALE = 1/8" = 10'
 USER NAME = hndk10000

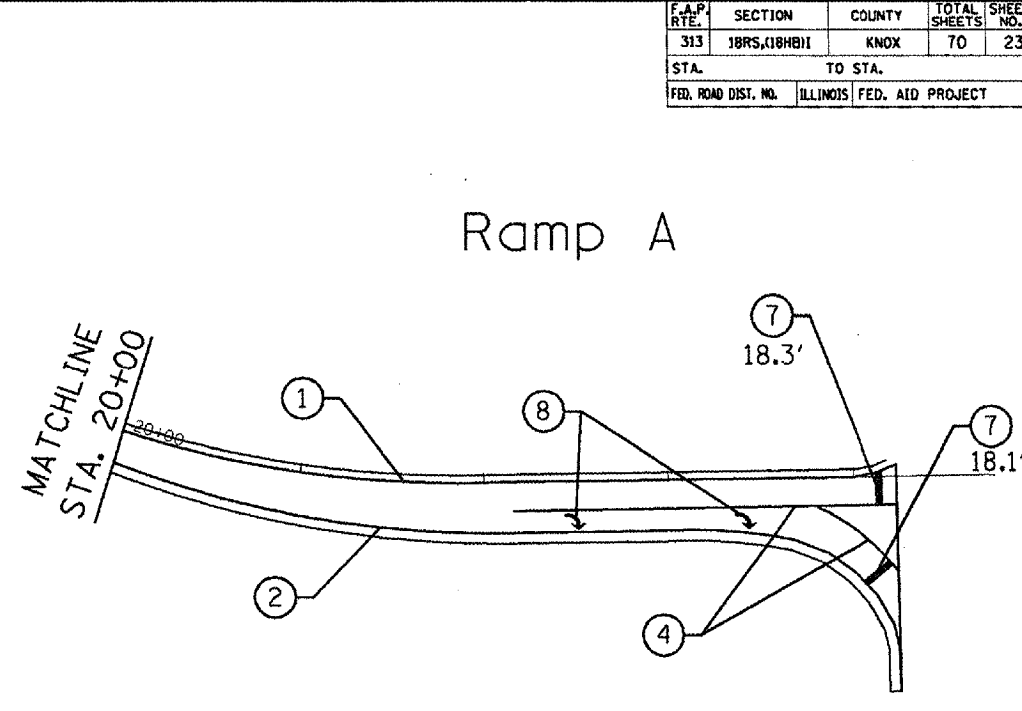
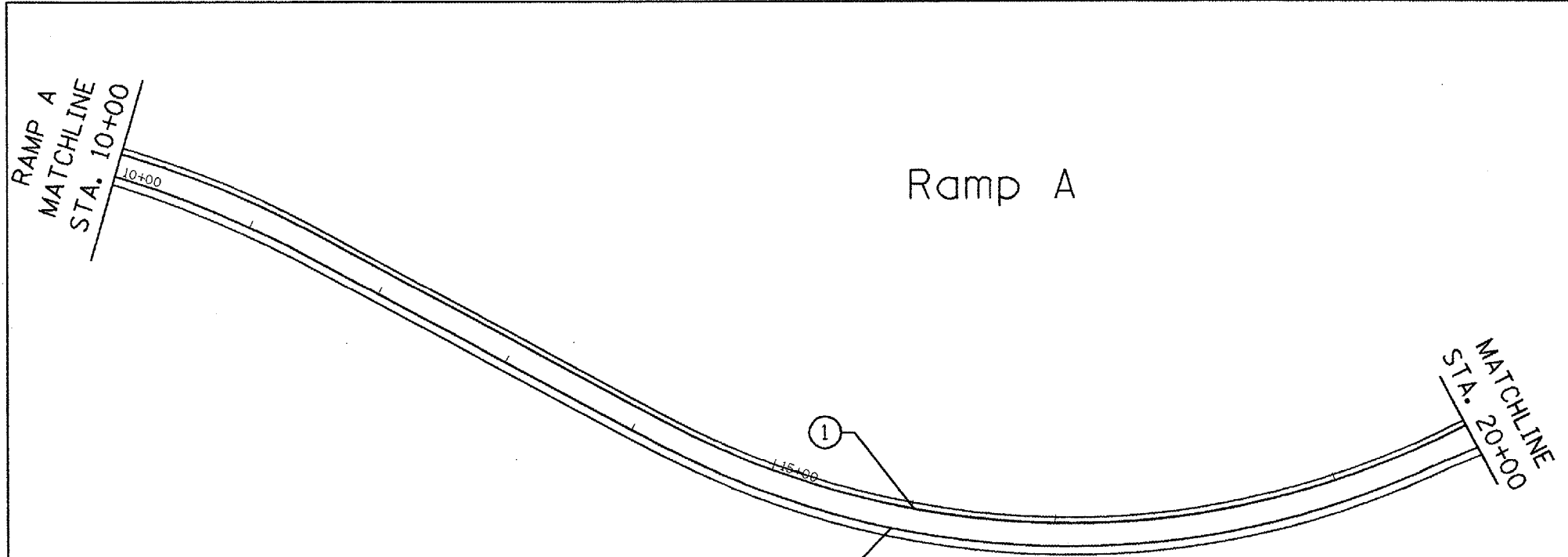
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

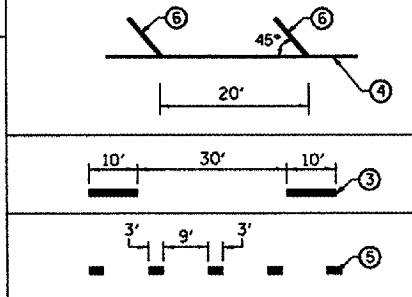
Pavement Markings

SCALE: VERT. HORIZ.
 DATE
 DRAWN BY
 CHECKED BY

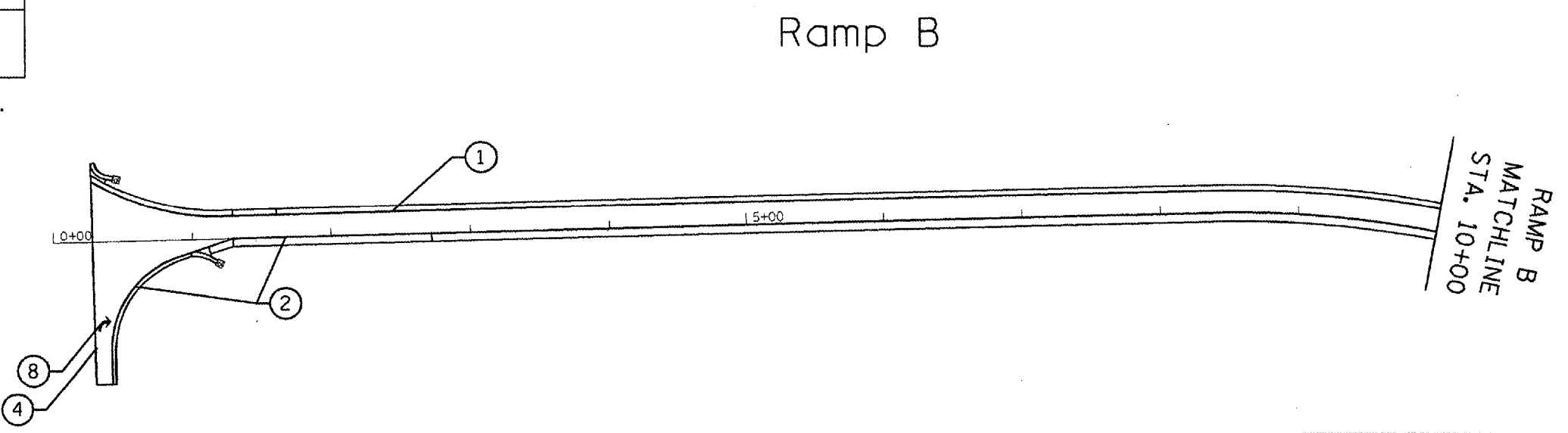
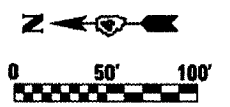
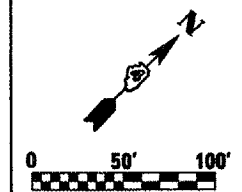
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18RS,(18)B11	KNOX	70	23
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



1. 4" Yellow Solid
2. 4" White Solid
3. 6" White Dash
4. 8" White Solid
5. 8" White Dash
6. 12" White Diagonal
7. 24" White Stop Bar
8. Letters & Symbols
- A. One-Way Amber @ 40' C.-C.



Note: See Highway Standard 780001 & 781001 for additional information.



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

Pavement Markings

SCALE: VERT. _____ HORIZ. _____
 DATE _____ DRAWN BY _____ CHECKED BY _____

PLOT DATE = 10/18/2007
 FILE NAME = 03\p-0404\18rs\18b11\general1.dgn
 PLOT SCALE = 1/8"=1'-0"
 USER NAME = hnd31111111

Bench Mark: TBM MWS - Chiseled square of drop inlet. Station 410+16.
Offset ±35' North. Elev. 777.916

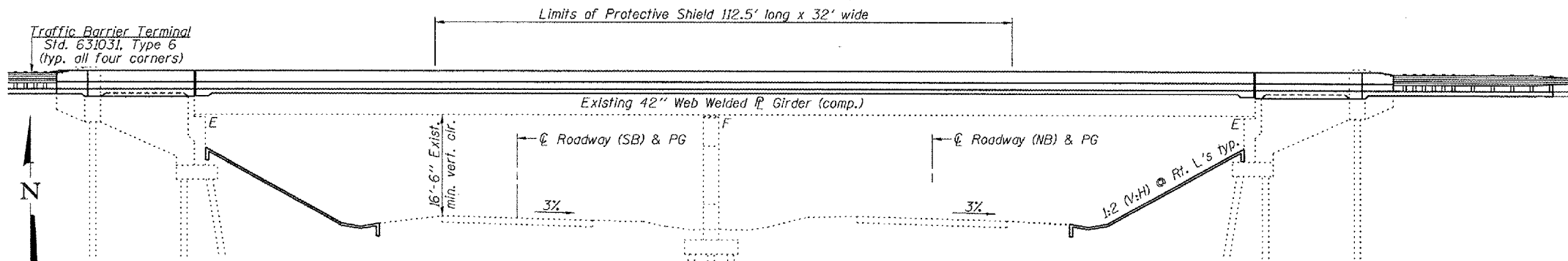
Existing Structure: S.N. 048-0069, originally built in 1971 as T.R. 169, Section 18HB. The existing structure is a two span composite plate girder supported on sand filled vaulted abutments and a trapezoidal pier supported pier. The total structure length is 205'-0" back to back of approach bents with an out to out bridge width of 32'-0". Traffic to be detoured during construction.

No Salvage

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	SHEET	NO.	SHEET NO. 1
F.A.P. 313	18-HB	KNOX	70	24	23 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

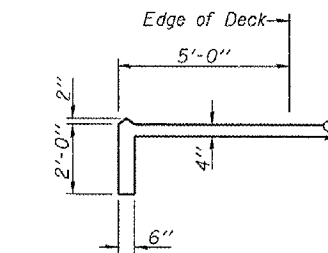
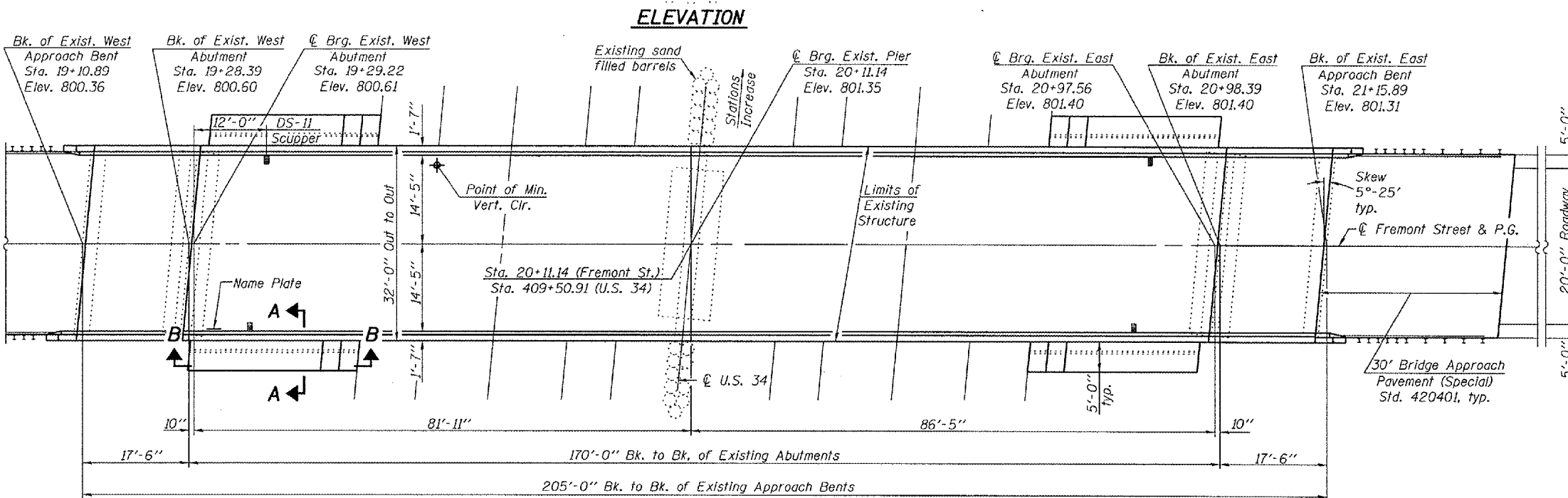
Contract #68190



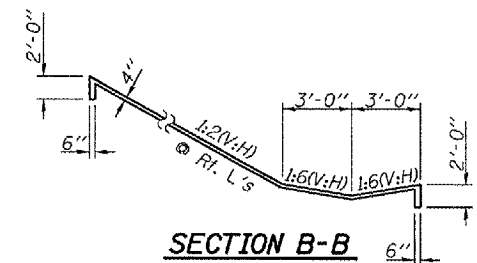
STATION 20+11.14
RE-BUILT 200 BY
STATE OF ILLINOIS
F.A.P. RTE. 313 - SEC. 18-HB
LOADING HS20-44
STR. NO. 048-0069

NAME PLATE

See Std. 515001
Existing name plate shall be cleaned and placed next to the new name plate. Cost included in "Name Plates".

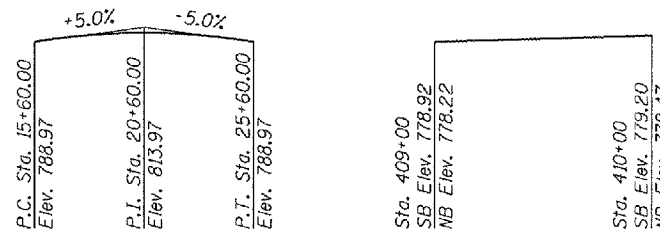


SECTION A-A



SECTION B-B

PLAN

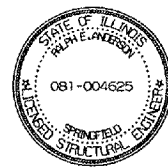


PROFILE GRADE
(Fremont St.)

PROFILE GRADE
(U.S. 34)

DESIGNED	Stephan M. Brown
CHECKED	[Signature]
DRAWN	R. Sommer
CHECKED	SMR SEM

Nov. 20, 2007
EXAMINED [Signature]
PASSED [Signature]

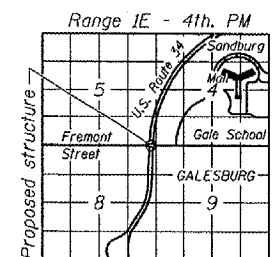


EXPIRES 11-30-2008

LOADING HS-20-44
Allow 25#/sq. ft. for future wearing surface.
DESIGN SPECIFICATIONS
2002 AASHTO
DESIGN STRESSES

FIELD UNITS
New Construction
 $f'_c = 3,500$ psi
 $f_y = 60,000$ psi (reinforcement)
 $f_y = 36,000$ psi (AASHTO M270 Grade 36 Structural Steel)
Existing Construction
 $f'_c = 3,500$ psi
 $f_y = 40,000$ psi (reinforcement)
 $f_y = 36,000$ psi (Structural Steel)

SEISMIC DATA
Seismic Performance Category (SPC) = A
Bedrock Acceleration Coefficient (A) = 0.04g
Site Coefficient (S) = 1.2



LOCATION SKETCH

GENERAL PLAN & ELEVATION
FREMONT STREET OVER U.S. RTE. 34
F.A.P. RT. 313 SECTION 18-HB
KNOX COUNTY
STATION 20+11.14
STRUCTURE NO. 048-0069

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	SHEET NO.	SHEET TOTAL	SHEET NO. 2
F.A.P. 313	18-HB	KNOX	70	25	23 SHEETS
FED. ROAD DIST. NO. 7	ALLIGS	FED. AID PROJECT			

Contract #68190

INDEX OF SHEETS

1. General Plan and Elevation
2. General Data
- 3, 4 & 5. Top of Bridge Slab Elevations
6. Top of Approach Span Slab Elevations
7. Top of West Approach Pavement Slab Elevations
8. Top of East Approach Pavement Slab Elevations
9. Superstructure
10. Superstructure Details
11. Bridge Approach Pavement (Special) Slab Details
12. Approach Span
13. Approach Span Details
14. Drainage Scupper, DS-II
15. Structural Steel
16. Structural Steel Details
17. Preformed Joint Strip Seal
18. Bearing Details
- 19 & 20. Concrete Removal & Repair Details- Abutments
21. Abutments
22. Abutment Details
23. Concrete Parapet Slip Forming Option

SCOPE OF WORK

1. Remove and replace existing bridge deck and vaulted approach spans.
2. Remove and replace bearings at abutments.
3. Remove and replace end diaphragms.
4. Repair substructure.
5. Remove and replace slopewalls.
6. Remove top of existing wing walls.
7. Clean and paint existing Structural Steel.

*Removal of the existing approach span slabs is included in this pay item. Portions of existing rebar are to be reused in new approach span slabs as shown on sheet 19 of 23.

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
*Removal of Existing Concrete Deck	Each			1
Protective Coat	Sq. Yd.	823		823
Concrete Superstructure	Cu. Yd.	252.6		252.6
Reinforcement Bars, Epoxy Coated	Pound	56,220	110	56,330
Name Plates	Each	1		1
Bridge Deck Grooving	Sq. Yd.	608		608
Furnishing & Erecting Structural Steel	Pound	4490		4490
Stud Shear Connectors	Each	990		990
Cleaning and Painting Steel Bridge	L.Sum			1
Slope Wall 4 inch	Sq. Yd.		289	289
Protective Shield	Sq. Yd.	400		400
Concrete Removal	Cu. Yd.	4.5		4.5
Structural Steel Removal	Pound	2820		2820
Structural Repair of Concrete (Depth equal to or less than 5 inches)	Sq. Ft.		139	139
Jack and Remove Existing Bearings	Each		10	10
Elastomeric Bearing Assembly, Type 1	Each		10	10
Preformed Joint Strip Seal	Foot	62		62
Drainage Scuppers, DS-II	Each	4		4
Epoxy Crack Injection	Foot		13.5	13.5
Containment and Disposal of Lead	L.Sum			1
Paint Cleaning Residues	L.Sum			1
Slope Wall Removal	Sq. Yd.		248	248
Anchor Bolts, 1"	Each		40	40

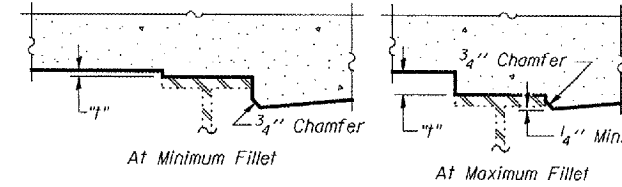
GENERAL NOTES

Fasteners shall be AASHTO M164 Type 1, mechanically galvanized bolts. Bolts $\frac{3}{4}$ " ϕ , holes $\frac{13}{16}$ " ϕ , unless otherwise noted. No field welding is permitted except as specified in the contract documents. Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60 (LL Modified). See Special Provisions. Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete. As directed by the Engineer, existing construction accessories welded to the top flange of girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by an individual acceptable to the Engineer. Any cracks that cannot be removed by grinding $\frac{1}{4}$ inch deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications. Plan dimensions and details relative to existing plans are subject to routine variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished based upon the unit price bid for the work. The SSPC-QP1 and SSPC-QP2 Painting Contractor Certifications will not be required for this bridge. The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project. Slopewall shall be reinforced with welded wire fabric, 6" x 6" - W4.0 x W4.0, weighing 58 lbs. per 100 sq. ft. Cleaning and painting of the existing structural steel shall be as specified in the Special Provision for "Cleaning and Painting Existing Steel Structures". All existing steel shall be cleaned per Near White Blast Cleaning-SSPC-SPI0. All existing steel shall be painted according to the requirements of Paint System 1-OZ/E/U. The color of the final finish coat for all interior steel surfaces shall be Gray, Munsell No 5B 7/L. The color of the final finish coat for the exterior and bottom flange of the fascia beams shall be Blue, Munsell No 10B 3/6. Reinforcement bars designated (E) shall be epoxy coated.

DESIGNED Stephen M. Ryan	Nov. 20 2007
CHECKED Phillip R. Litchfield	EXAMINED <i>Thomas J. Damagala</i>
DRAWN R. Sommer	PASSED <i>Rafael E. Anderson</i>
CHECKED SMR/SEM	

GENERAL DATA
F.A.P. RT. 313 SECTION 18-HB
KNOX COUNTY
STATION 20+11.14
STRUCTURE NO. 048-0069

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

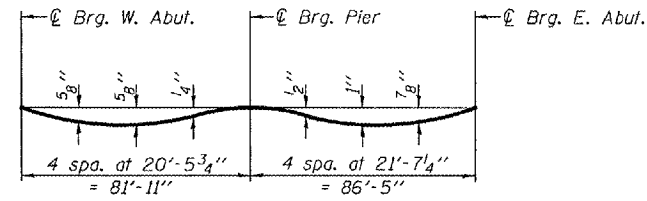


ROUTE NO.	SECTION	COUNTY	SPAN	SHEET	SHEET NO. 3 23 SHEETS
F.A.P. 313	18-HB	KNOX	70	26	
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT		

Contract #68190

To determine "t": After all structural steel has been erected, elevations of the top flanges of the girders shall be taken at intervals shown below. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection" shown on sheets 4 and 5 of 23, minus slab thickness, equals the fillet heights "t" above top flange of girders.

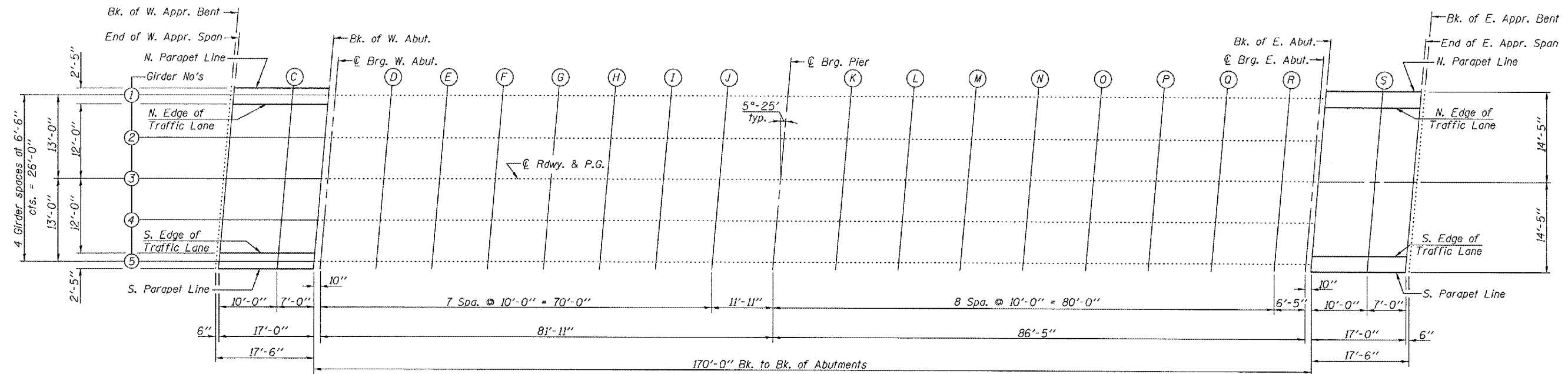
FILLET HEIGHTS



DEAD LOAD DEFLECTION DIAGRAM

(Includes weight of concrete only.)

Note:
The above deflections are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections as shown on sheets 4 and 5 of 23.



PLAN

DESIGNED	Stephen M. Ryan
CHECKED	Phillip R. Litchfield
DRAWN	R. Sommer
CHECKED	SMR/SEM

Nov. 20, 2007
EXAMINED *Thomas J. Donagale*
PASSED *Ralph E. Anderson*
ENGINEER OF BRIDGE DESIGN
ENGINEER OF BRIDGES AND STRUCTURES

TOP OF BRIDGE SLAB ELEVATIONS
F.A.P. RT. 313 SECTION 18-HB
KNOX COUNTY
STATION 20+11.14
STRUCTURE NO. 048-0069

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 4 23 SHEETS
F.A.P. 313	18-HB	KNOX	70	27	
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-			

Contract #68190

GIRDER 1

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk W. Abut.	1929.62	-13.00	800.41	800.41
⊕ Brg. W. Abut.	1930.46	-13.00	800.42	800.42
D	1940.46	-13.00	800.55	800.57
E	1950.46	-13.00	800.66	800.71
F	1960.46	-13.00	800.77	800.82
G	1970.46	-13.00	800.86	800.92
H	1980.46	-13.00	800.95	800.99
I	1990.46	-13.00	801.02	801.04
J	2000.46	-13.00	801.08	801.10
⊕ Brg. Pier	2012.37	-13.00	801.15	801.15
K	2022.37	-13.00	801.19	801.21
L	2032.37	-13.00	801.22	801.26
M	2042.37	-13.00	801.25	801.30
N	2052.37	-13.00	801.26	801.33
O	2062.37	-13.00	801.26	801.34
P	2072.37	-13.00	801.25	801.33
Q	2082.37	-13.00	801.24	801.29
R	2092.37	-13.00	801.21	801.23
⊕ Brg. E. Abut.	2098.79	-13.00	801.19	801.19
Bk E. Abut.	2099.62	-13.00	801.18	801.18

GIRDER 2

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk W. Abut.	1929.01	-6.50	800.51	800.51
⊕ Brg. W. Abut.	1929.84	-6.50	800.52	800.52
D	1939.84	-6.50	800.65	800.67
E	1949.84	-6.50	800.76	800.81
F	1959.84	-6.50	800.87	800.92
G	1969.84	-6.50	800.96	801.02
H	1979.84	-6.50	801.05	801.09
I	1989.84	-6.50	801.12	801.15
J	1999.84	-6.50	801.19	801.20
⊕ Brg. Pier	2011.76	-6.50	801.25	801.25
K	2021.76	-6.50	801.30	801.31
L	2031.76	-6.50	801.33	801.36
M	2041.76	-6.50	801.35	801.41
N	2051.76	-6.50	801.37	801.44
O	2061.76	-6.50	801.37	801.45
P	2071.76	-6.50	801.36	801.43
Q	2081.76	-6.50	801.34	801.40
R	2091.76	-6.50	801.32	801.34
⊕ Brg. E. Abut.	2098.17	-6.50	801.30	801.30
Bk E. Abut.	2099.01	-6.50	801.29	801.29

GIRDER 3, ⊕ RDWY. & P.G.

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk W. Abut.	1928.39	0.00	800.60	800.60
⊕ Brg. W. Abut.	1929.22	0.00	800.61	800.61
D	1939.22	0.00	800.74	800.76
E	1949.22	0.00	800.86	800.90
F	1959.22	0.00	800.96	801.01
G	1969.22	0.00	801.06	801.11
H	1979.22	0.00	801.14	801.18
I	1989.22	0.00	801.22	801.24
J	1999.22	0.00	801.29	801.30
⊕ Brg. Pier	2011.14	0.00	801.35	801.35
K	2021.14	0.00	801.39	801.41
L	2031.14	0.00	801.43	801.46
M	2041.14	0.00	801.45	801.51
N	2051.14	0.00	801.47	801.54
O	2061.14	0.00	801.47	801.55
P	2071.14	0.00	801.46	801.54
Q	2081.14	0.00	801.45	801.50
R	2091.14	0.00	801.42	801.44
⊕ Brg. E. Abut.	2097.56	0.00	801.40	801.40
Bk E. Abut.	2098.39	0.00	801.40	801.40

DESIGNED <i>Stephen M. Ryan</i>	Nov. 20 2007
CHECKED <i>Phillip R. Litchfield</i>	EXAMINED <i>Thomas J. Damagalki</i> ENGINEER OF BRIDGE DESIGN
DRAWN <i>R. Sommer</i>	PASSED <i>Ralph E. Anderson</i> ENGINEER OF BRIDGES AND STRUCTURES
CHECKED <i>SMR/SEM</i>	

TOP OF BRIDGE SLAB ELEVATIONS
F.A.P. RT. 313 SECTION 18-HB
KNOX COUNTY
STATION 20+11.14
STRUCTURE NO. 048-0069

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	SHEET NO.	SHEET	SHEET NO. 5
F.A.P. 313	18-HB	KNOX	70	28	23 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT	Contract #68190		

GIRDER 4

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk W. Abut.	1927.77	6.50	800.49	800.49
⊕ Brg. W. Abut.	1928.61	6.50	800.51	800.51
D	1938.61	6.50	800.63	800.66
E	1948.61	6.50	800.75	800.80
F	1958.61	6.50	800.85	800.91
G	1968.61	6.50	800.95	801.01
H	1978.61	6.50	801.04	801.08
I	1988.61	6.50	801.11	801.14
J	1998.61	6.50	801.18	801.19
⊕ Brg. Pier	2010.52	6.50	801.25	801.25
K	2020.52	6.50	801.29	801.31
L	2030.52	6.50	801.33	801.36
M	2040.52	6.50	801.35	801.41
N	2050.52	6.50	801.36	801.44
O	2060.52	6.50	801.37	801.45
P	2070.52	6.50	801.36	801.44
Q	2080.52	6.50	801.35	801.40
R	2090.52	6.50	801.32	801.34
⊕ Brg. E. Abut.	2096.94	6.50	801.30	801.30
Bk E. Abut.	2097.77	6.50	801.30	801.30

GIRDER 5

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk W. Abut.	1927.16	13.00	800.38	800.38
⊕ Brg. W. Abut.	1927.99	13.00	800.39	800.39
D	1937.99	13.00	800.52	800.54
E	1947.99	13.00	800.63	800.68
F	1957.99	13.00	800.74	800.79
G	1967.99	13.00	800.84	800.89
H	1977.99	13.00	800.93	800.97
I	1987.99	13.00	801.00	801.03
J	1997.99	13.00	801.07	801.08
⊕ Brg. Pier	2009.91	13.00	801.14	801.14
K	2019.91	13.00	801.18	801.20
L	2029.91	13.00	801.22	801.25
M	2039.91	13.00	801.24	801.30
N	2049.91	13.00	801.26	801.33
O	2059.91	13.00	801.26	801.34
P	2069.91	13.00	801.26	801.33
Q	2079.91	13.00	801.24	801.30
R	2089.91	13.00	801.22	801.24
⊕ Brg. E. Abut.	2096.32	13.00	801.20	801.20
Bk E. Abut.	2097.16	13.00	801.19	801.19

DESIGNED Stephen M. Ryan
CHECKED Phillip R. Litchfield
DRAWN R. Sommer
CHECKED SMR/SEM

Nov. 20 2007
EXAMINED Thomas J. Damagala
PASSED Ralph E. Curkum
ENGINEER OF BRIDGE DESIGN
ENGINEER OF BRIDGES AND STRUCTURES

TOP OF BRIDGE SLAB ELEVATIONS
F.A.P. RT. 313 SECTION 18-HB
KNOX COUNTY
STATION 20+11.14
STRUCTURE NO. 048-0069

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	SHEETS	PAGE	SHEET NO. 6
F.A.P. 313	18-HB	KNOX	70	29	23 SHEETS
FED. ROAD DIST. NO. 7	BALANCE	FED. AID PROJECT-			

Contract #68190

NORTH PARAPET LINE

Location	Station	Offset	Theoretical Grade Elevations
Bk. W. Appr. Bent	1912.26	-14.42	800.14
End W. Appr. Span	1912.76	-14.42	800.15
C	1922.76	-14.42	800.29
Bk. W. Abut.	1929.76	-14.42	800.38
Bk. E. Abut.	2099.76	-14.42	801.15
S	2109.76	-14.42	801.11
End E. Appr. Span	2116.76	-14.42	801.07
Bk. E. Appr. Bent	2117.26	-14.42	801.07

SOUTH EDGE OF TRAFFIC LANE

Location	Station	Offset	Theoretical Grade Elevations
Bk. W. Appr. Bent	1909.75	12.00	800.15
End W. Appr. Span	1910.25	12.00	800.16
C	1920.25	12.00	800.31
Bk. W. Abut.	1927.25	12.00	800.40
Bk. E. Abut.	2097.25	12.00	801.21
S	2107.25	12.00	801.17
End E. Appr. Span	2114.25	12.00	801.14
Bk. E. Appr. Bent	2114.75	12.00	801.13

NORTH EDGE OF TRAFFIC LANE

Location	Station	Offset	Theoretical Grade Elevations
Bk. W. Appr. Bent	1912.03	-12.00	800.19
End W. Appr. Span	1912.53	-12.00	800.20
C	1922.53	-12.00	800.34
Bk. W. Abut.	1929.53	-12.00	800.43
Bk. E. Abut.	2099.53	-12.00	801.20
S	2109.53	-12.00	801.16
End E. Appr. Span	2116.53	-12.00	801.12
Bk. E. Appr. Bent	2117.03	-12.00	801.12

SOUTH PARAPET LINE

Location	Station	Offset	Theoretical Grade Elevations
Bk. W. Appr. Bent	1909.52	14.42	800.10
End W. Appr. Span	1910.02	14.42	800.11
C	1920.02	14.42	800.25
Bk. W. Abut.	1927.02	14.42	800.35
Bk. E. Abut.	2097.02	14.42	801.16
S	2107.02	14.42	801.12
End E. Appr. Span	2114.02	14.42	801.09
Bk. E. Appr. Bent	2114.52	14.42	801.08

Ⓢ ROADWAY & P.G.

Location	Station	Offset	Theoretical Grade Elevations
Bk. W. Appr. Bent	1910.89	0.00	800.36
End W. Appr. Span	1911.39	0.00	800.37
C	1921.39	0.00	800.51
Bk. W. Abut.	1928.39	0.00	800.60
Bk. E. Abut.	2098.39	0.00	801.40
S	2108.39	0.00	801.35
End E. Appr. Span	2115.39	0.00	801.32
Bk. E. Appr. Bent	2115.89	0.00	801.31

DESIGNED <i>Stephen M. Ryan</i>
CHECKED <i>Phillip R. Litchfield</i>
DRAWN <i>R. Sommer</i>
CHECKED <i>SMR/SEM</i>

EXAMINED <i>Thomas J. Demagala</i>	Nov. 20 2007
PASSED <i>Ralph E. Anderson</i>	

TOP OF APPROACH SPAN
SLAB ELEVATIONS
F.A.P. RT. 313 SECTION 18-HB
KNOX COUNTY
STATION 20+11.14
STRUCTURE NO. 048-0069

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	SHEET	SHEET NO.
F.A.P. 313	18-HB	KNOX	70	30
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	

Contract #68190

SHEET NO. 7
23 SHEETS

NORTH CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
End W. Appr. Pav. Slab	1882.80	-14.83	799.70
A	1892.80	-14.83	799.87
B	1902.80	-14.83	800.03
* Beginning W. Appr. Pav. Slab	1912.76	-14.42	800.15

SOUTH EDGE OF TRAFFIC LANE

Location	Station	Offset	Theoretical Grade Elevations
End W. Appr. Pav. Slab	1880.25	12.00	799.67
A	1890.25	12.00	799.84
B	1900.25	12.00	800.01
Beginning W. Appr. Pav. Slab	1910.25	12.00	800.16

NORTH EDGE OF TRAFFIC LANE

Location	Station	Offset	Theoretical Grade Elevations
End W. Appr. Pav. Slab	1882.53	-12.00	799.71
A	1892.53	-12.00	799.88
B	1902.53	-12.00	800.04
Beginning W. Appr. Pav. Slab	1912.53	-12.00	800.20

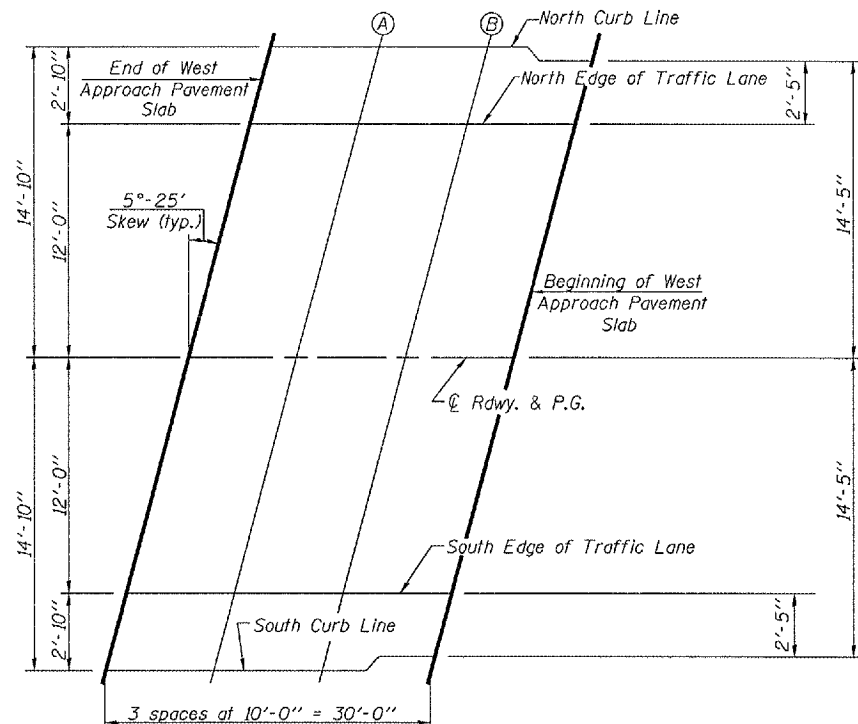
SOUTH CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
End W. Appr. Pav. Slab	1879.98	14.83	799.65
A	1889.98	14.83	799.82
B	1899.98	14.83	799.99
* Beginning W. Appr. Pav. Slab	1910.02	14.42	800.11

☉ ROADWAY & P.G.

Location	Station	Offset	Theoretical Grade Elevations
End W. Appr. Pav. Slab	1881.39	0.00	799.87
A	1891.39	0.00	800.05
B	1901.39	0.00	800.21
Beginning W. Appr. Pav. Slab	1911.39	0.00	800.37

*Data shown is at the inside edge of Parapet at this location (instead of at the curb line).



PLAN
(West Approach)

DESIGNED	Stephen M. Ryan
CHECKED	Phillip R. Litchfield
DRAWN	R. Sommer
CHECKED	SMR/SEM

Nov. 20 2007
EXAMINED *Thomas J. Demagali*
PASSED *Ralph E. Anderson*
ENGINEER OF BRIDGES AND STRUCTURES

TOP OF WEST APPROACH PAVEMENT
SLAB ELEVATIONS
F.A.P. RT. 313 SECTION 18-HB
KNOX COUNTY
STATION 20+11.14
STRUCTURE NO. 048-0069

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	SHEET NO.	SHEET TOTAL	SHEET NO. 8 23 SHEETS
F.A.P. 313	18-HB	KNOX	76	31	
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT		

Contract #68190

NORTH CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
* Beginning E. Appr. Pav. Slab	2116.76	-14.42	801.07
T	2126.80	-14.83	801.05
U	2136.80	-14.83	800.97
End E. Appr. Pav. Slab	2146.80	-14.83	800.89

SOUTH EDGE OF TRAFFIC LANE

Location	Station	Offset	Theoretical Grade Elevations
Beginning E. Appr. Pav. Slab	2114.25	12.00	801.14
T	2124.25	12.00	801.08
U	2134.25	12.00	801.01
End E. Appr. Pav. Slab	2144.25	12.00	800.93

NORTH EDGE OF TRAFFIC LANE

Location	Station	Offset	Theoretical Grade Elevations
Beginning E. Appr. Pav. Slab	2116.53	-12.00	801.12
T	2126.53	-12.00	801.06
U	2136.53	-12.00	800.99
End E. Appr. Pav. Slab	2146.53	-12.00	800.91

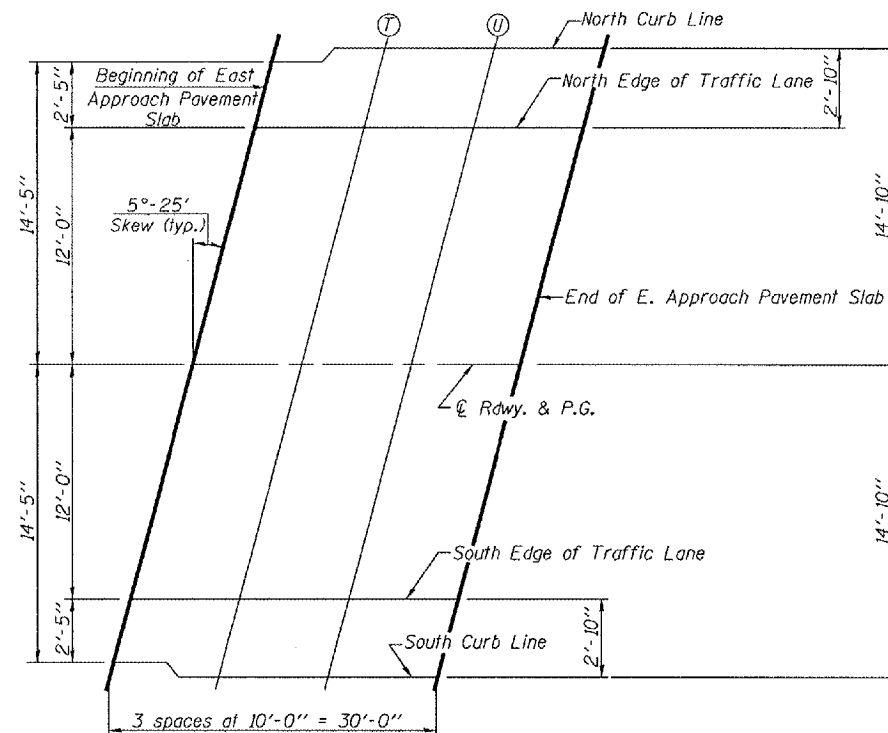
SOUTH CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
* Beginning E. Appr. Pav. Slab	2114.02	14.42	801.09
T	2123.98	14.83	801.06
U	2133.98	14.83	801.00
End E. Appr. Pav. Slab	3143.98	14.83	800.92

☉ ROADWAY & P.G.

Location	Station	Offset	Theoretical Grade Elevations
Beginning E. Appr. Pav. Slab	2115.39	0.00	801.32
T	2125.39	0.00	801.26
U	2135.39	0.00	801.19
End E. Appr. Pav. Slab	2145.39	0.00	801.11

*Data shown is at the inside edge of Parapet at this location (instead of at the curb line).



PLAN
(East Approach)

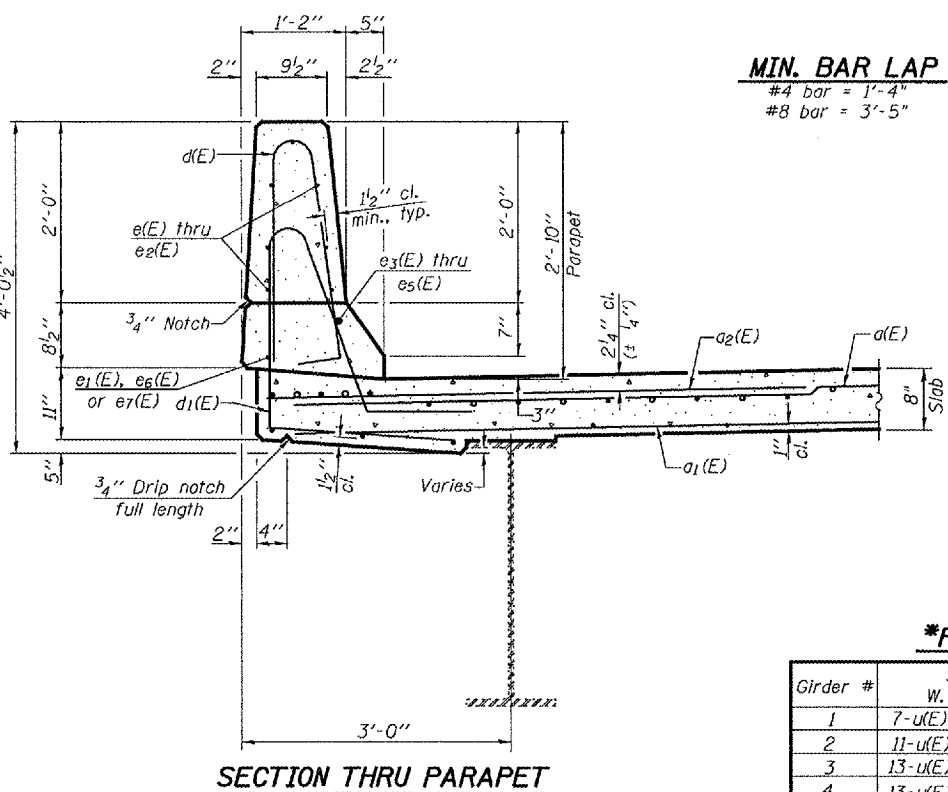
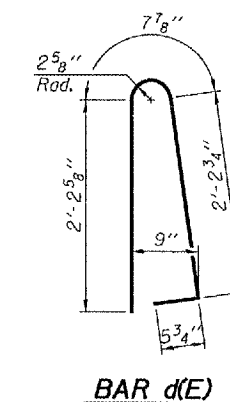
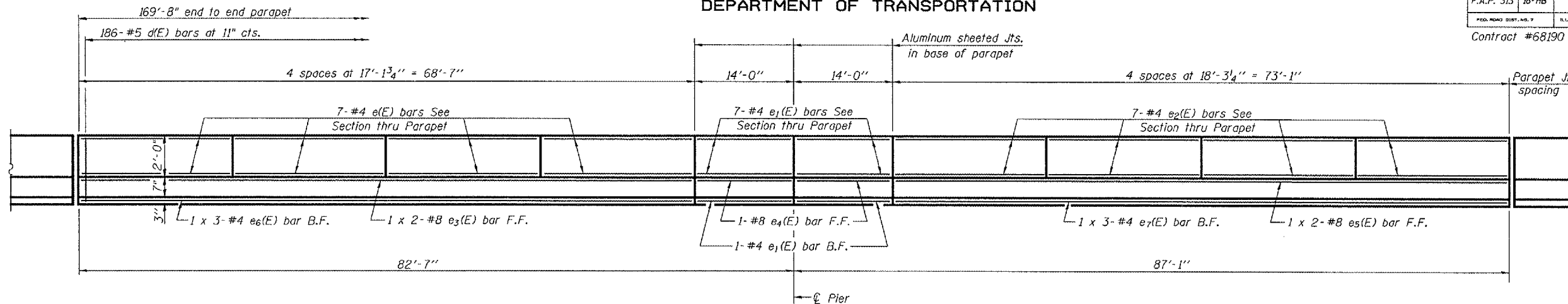
DESIGNED	Stephen M. Ryan
CHECKED	Phillip R. Litchfield
DRAWN	R. Sommer
CHECKED	SMR/SEM

EXAMINED	Thomas J. Nangalaki	Nov. 20 2007
PASSED	Ralph E. Anderson	

**TOP OF EAST APPROACH PAVEMENT
SLAB ELEVATIONS
F.A.P. RT. 313 SECTION 18-HB
KNOX COUNTY
STATION 20+11.14
STRUCTURE NO. 048-0069**

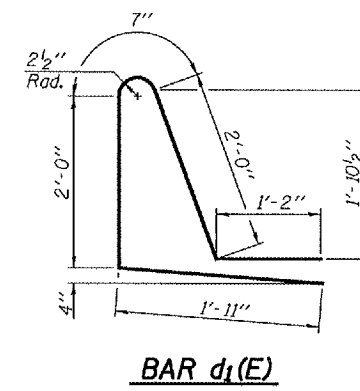
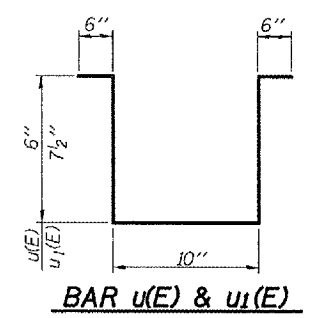
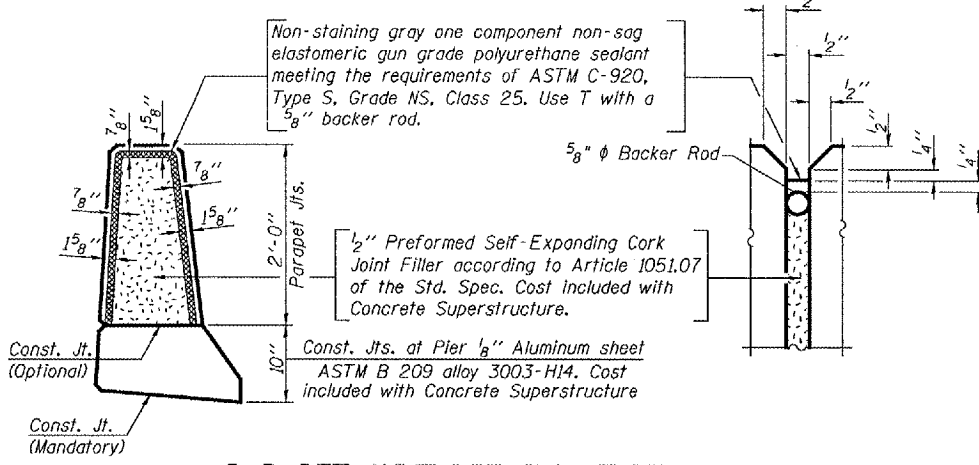
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	SHEET	SHEET	SHEET NO. 10
F.A.P. 313	18-HB	KNOX	70	33	23 SHEETS
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT		
Contract #68190					



MIN. BAR LAP
#4 bar = 1'-4"
#8 bar = 3'-5"

INSIDE ELEVATION OF PARAPET
(North Parapet shown. The South Parapet is similar)



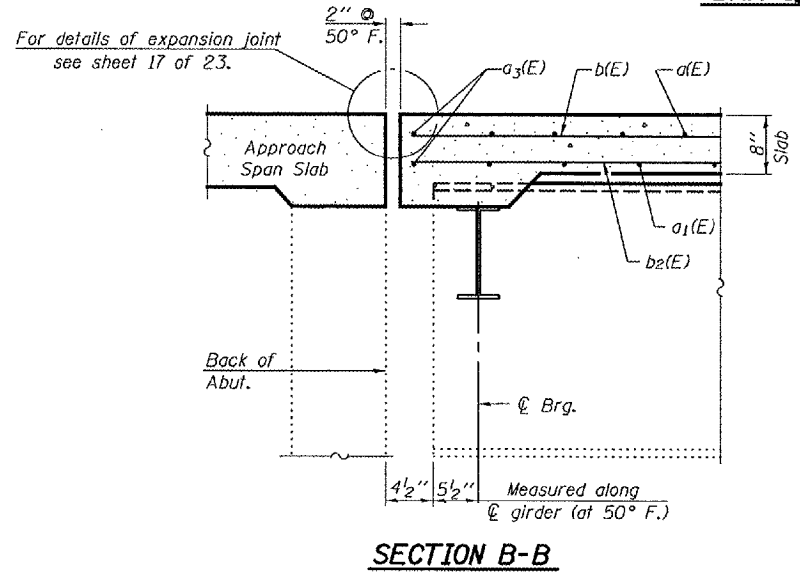
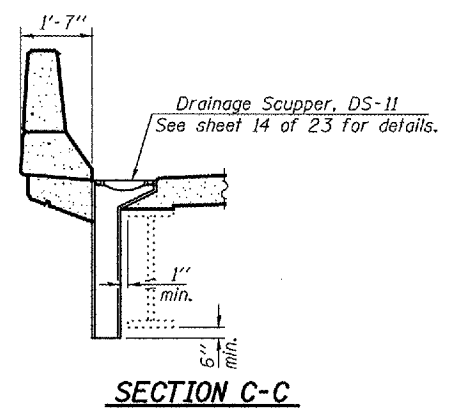
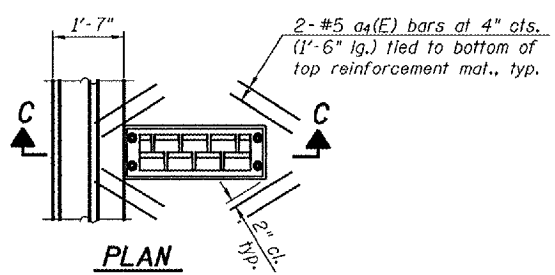
SUPERSTRUCTURE BILL OF MATERIAL

Bar No.	Size	Length	Shape
a(E)	240	#5 31'-4"	—
a1(E)	205	#5 29'-10"	—
a2(E)	478	#6 6'-0"	—
a3(E)	4	#5 30'-0"	—
a4(E)	32	#5 1'-6"	—
b(E)	210	#5 30'-1"	—
b1(E)	96	#6 20'-1"	—
b2(E)	210	#5 26'-1"	—
b3(E)	60	#5 23'-3"	—
d(E)	372	#5 5'-7"	—
d1(E)	372	#5 7'-8"	—
e(E)	56	#4 16'-10"	—
e1(E)	32	#4 13'-9"	—
e2(E)	56	#4 18'-0"	—
e3(E)	4	#8 35'-11"	—
e4(E)	4	#8 13'-9"	—
e5(E)	4	#8 38'-2"	—
e6(E)	6	#4 23'-8"	—
e7(E)	6	#4 25'-2"	—
u(E)	228	#4 2'-10"	L
u1(E)	397	#4 3'-1"	L

***FILLET REINFORCEMENT TABLE**

Girder #	Span 1. @ Brg. W. Abut. to 59'-0"			Span 2. 22'-5" from @ Pier to @ Brg. E. Abut.		
	1	7-u(E)	45-u1(E)	8-u(E)	7-u(E)	44-u1(E)
2	11-u(E)	39-u1(E)	10-u(E)	9-u(E)	39-u1(E)	17-u(E)
3	13-u(E)	37-u1(E)	10-u(E)	10-u(E)	38-u1(E)	17-u(E)
4	13-u(E)	36-u1(E)	11-u(E)	10-u(E)	39-u1(E)	16-u(E)
5	13-u(E)	37-u1(E)	10-u(E)	8-u(E)	43-u1(E)	14-u(E)

*Provide the fillet reinforcement shown in this table in the order shown and at the locations shown. All bars shall be at 12" cts.



Reinforcement Bars, Epoxy Coated Pound 44480
Concrete Superstructure Cu. Yd. 195.6

Bars indicated thus 1 x 2 #8 etc. indicates 1 line of bars with 2 lengths per line.

SUPERSTRUCTURE DETAILS
F.A.P. RT. 313 SECTION 18-HB
KNOX COUNTY
STATION 20+11.14
STRUCTURE NO. 048-0069

DESIGNED Stephen M. Ryan
CHECKED Phillip R. Litchfield
DRAWN R. Sommer
CHECKED SMR/SEM

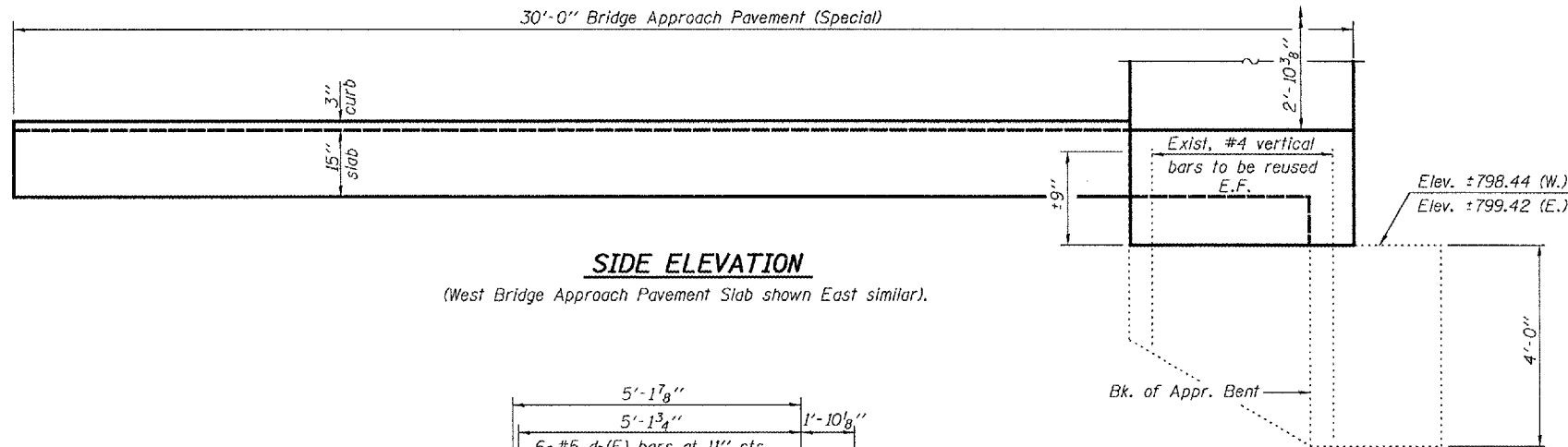
EXAMINED *Thomas J. Demagala*
PASSED *Ralph E. Anderson*

Nov. 20 2007
ENGINEER OF BRIDGES AND STRUCTURES

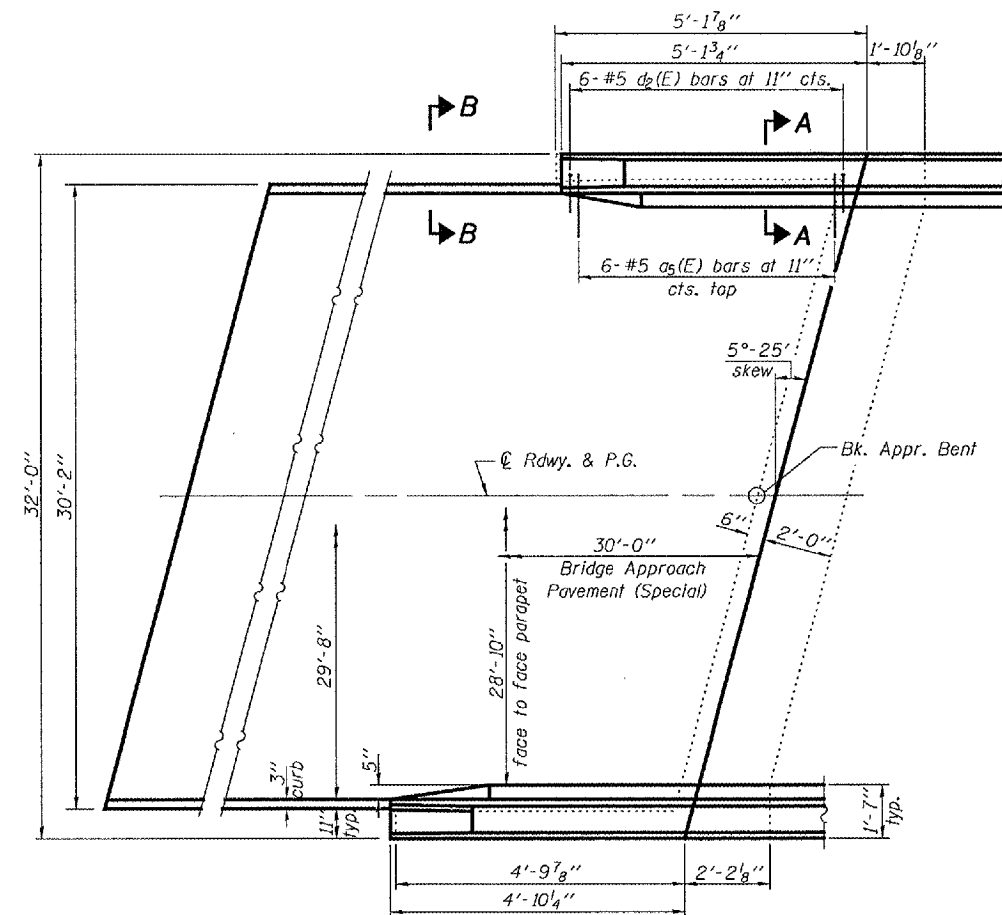
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	SHEET	SHEET NO. 11
F.A.P. 313	18-HB	KNOX	70 34	23 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

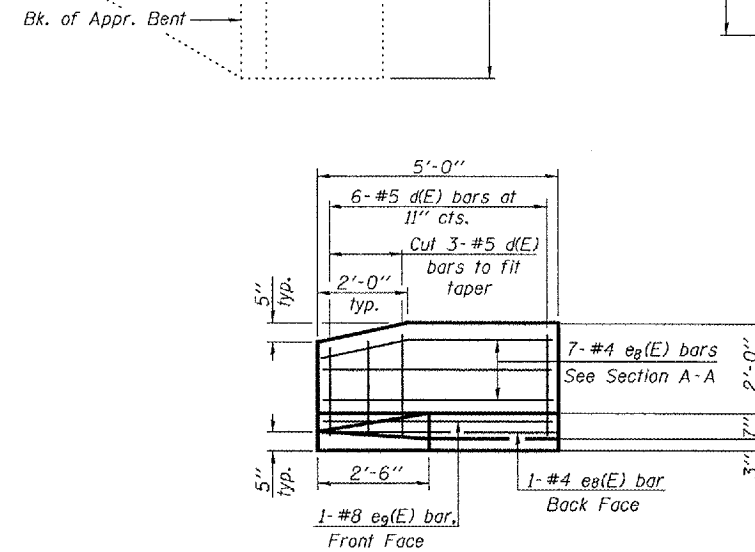
Contract #68190



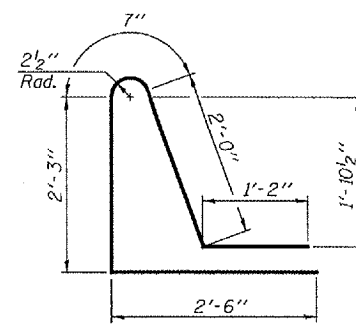
SIDE ELEVATION
(West Bridge Approach Pavement Slab shown East similar).



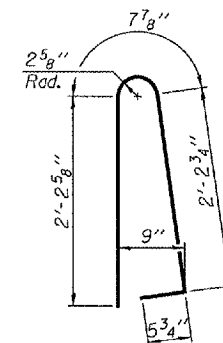
PLAN
(West Bridge Approach Pavement Slab shown East similar).



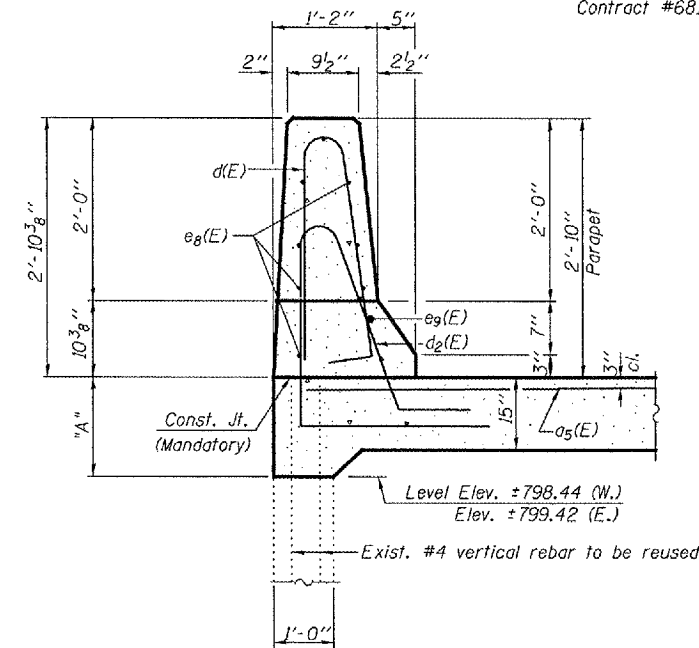
INSIDE ELEVATION OF PARAPET



BAR d2(E)



BAR d(E)

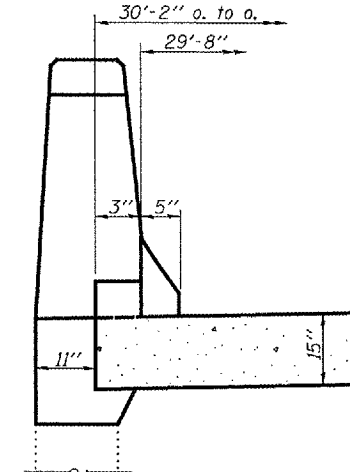


SECTION A-A
See table for dimension "A"

***TWO BRIDGE APPROACH
PAVEMENT SLAB PARAPETS
BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a5(E)	24	#5	6'-0"	—
d(E)	24	#5	5'-7"	U
d2(E)	24	#5	8'-6"	U
e8(E)	32	#4	4'-8"	—
e9(E)	4	#8	4'-8"	—
Reinforcement Bars, Epoxy Coated			Pound	650

Notes: *Quantities are shown for information only. Cost is included with Bridge Approach Pavement (Special). See Roadway Plans. For additional approach pavement details, see Roadway Plans.



VIEW B-B

TABLE FOR DIMENSION "A"

Location	*"A"
West Vault	North Side Varies from ±1'-7 1/4" to ±1'-8 1/8"
	South Side Varies from ±1'-6 5/8" to ±1'-7 1/2"
East Vault	North Side Varies from ±1'-7 1/8" to ±1'-7 1/2"
	South Side Varies from ±1'-7 3/8" to ±1'-7 5/8"

*Dimension shown varies from beginning of parapet on bridge approach pavement (Special) to end of parapet at bent.

DESIGNED Stephen M. Ryan
CHECKED Phillip R. Litchfield
DRAWN R. Sommer
CHECKED SMR/SEM

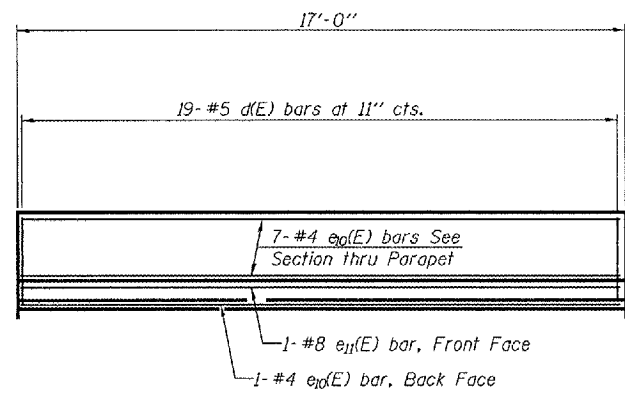
EXAMINED Thomas J. Domagala
PASSED Robert E. Anderson
NOV 20 2007

BRIDGE APPROACH PAVEMENT (SPECIAL) SLAB DETAILS
F.A.P. RT. 313 SECTION 18-HB
KNOX COUNTY
STATION 20+11.14
STRUCTURE NO. 048-0069

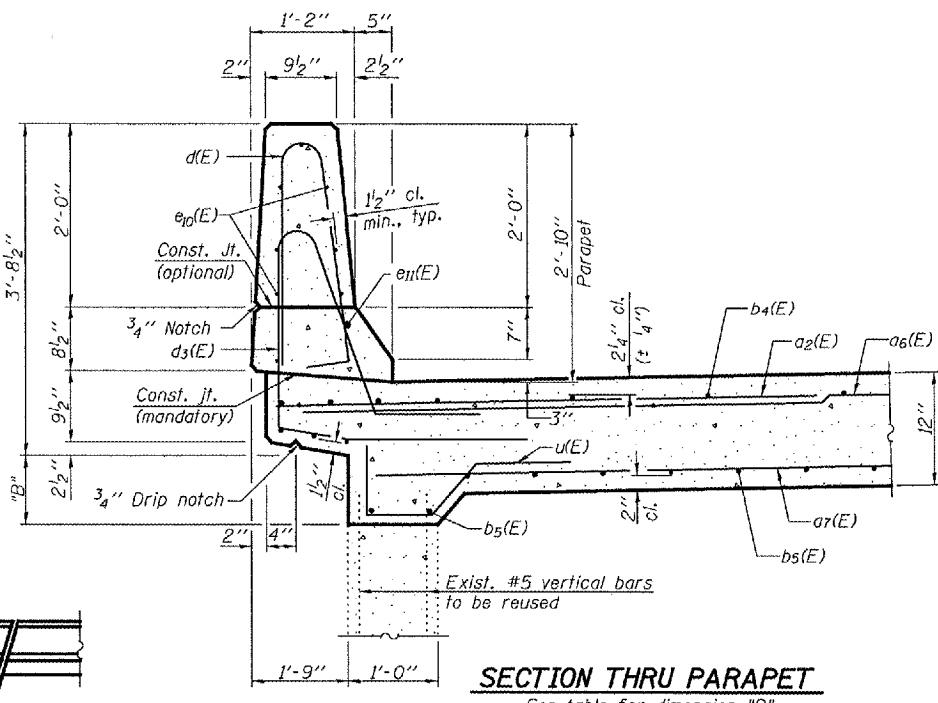
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	SHEET	SHEET	SHEET NO. 12
F.A.P. 313	18-HB	KNOX	70	35	23 SHEETS
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT		
Contract #68190					

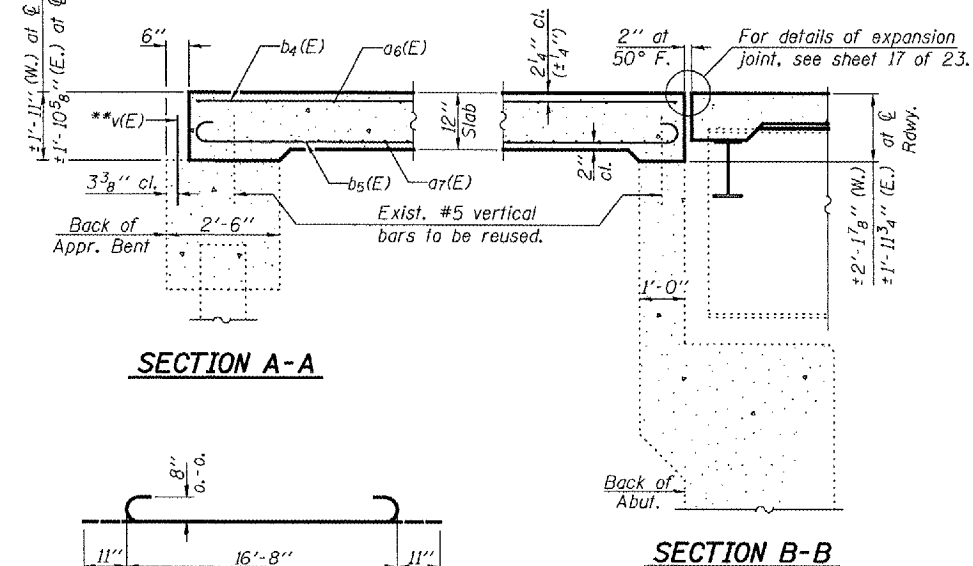
**For v(E) bars see sheet 21 of 23. (v(E) billed with abut.)



INSIDE ELEVATION OF PARAPET



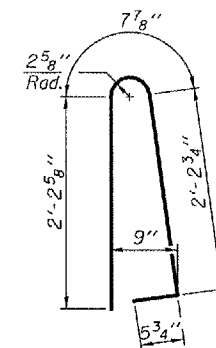
SECTION THRU PARAPET
See table for dimension "B"



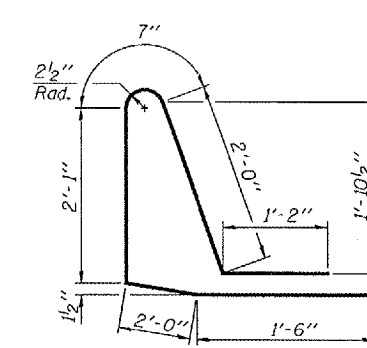
SECTION A-A

SECTION B-B

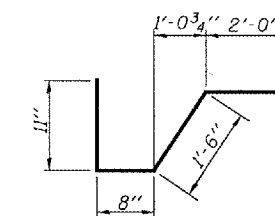
BAR b5(E)



BAR d(E)



BAR d3(E)



BAR u(E)

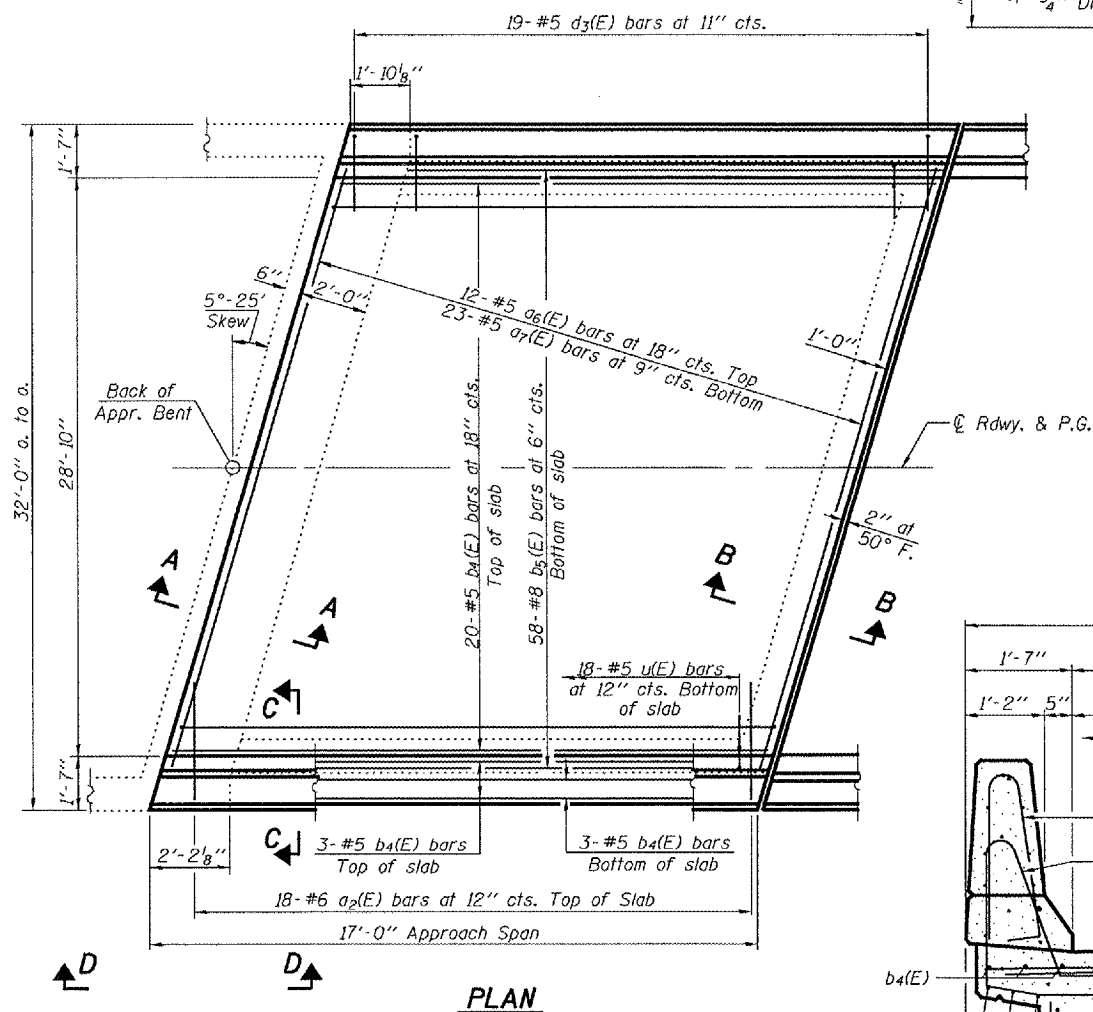
TWO APPROACH SPANS
BILL OF MATERIAL

Bar	No.	Size	Length	Shape	
a2(E)	72	#6	6'-0"	—	
a6(E)	24	#5	31'-5"	—	
a7(E)	46	#5	28'-4"	—	
b4(E)	64	#5	16'-8"	—	
b5(E)	116	#8	18'-6"	U	
d(E)	76	#5	5'-7"	U	
d3(E)	76	#5	9'-4"	U	
ep(E)	32	#4	16'-8"	—	
ep(E)	4	#8	16'-8"	—	
u(E)	72	#5	5'-1"	L	
Reinforcement Bars, Epoxy Coated				Pound	11740
Concrete Superstructure				Cu. Yd.	57.0

TABLE FOR DIMENSION "B"

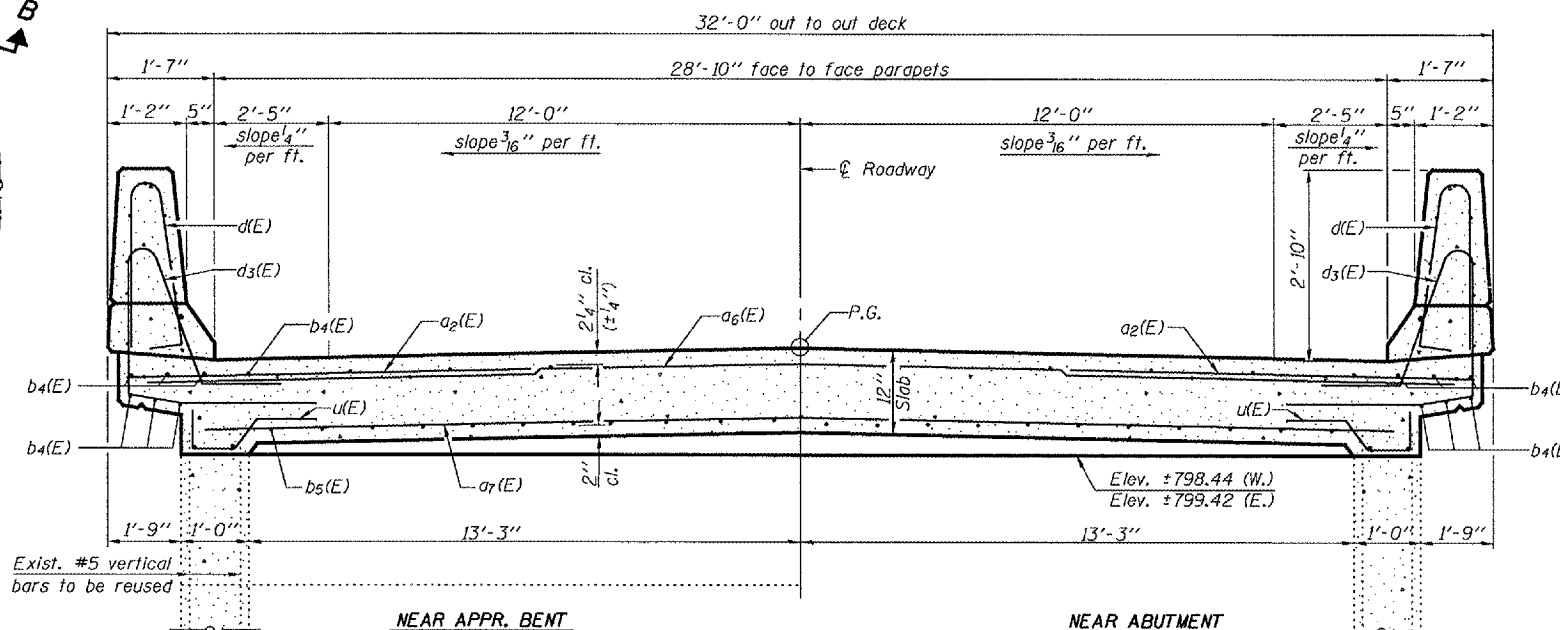
Location	*B*
West Vault	North Side Varies from $\pm 10 \frac{1}{8}$ " to $\pm 1'-0 \frac{1}{8}$ "
South Side	Varies from $\pm 9 \frac{1}{2}$ " to $\pm 1'-0 \frac{3}{8}$ "
East Vault	North Side Varies from $\pm 9 \frac{3}{8}$ " to $\pm 10 \frac{3}{8}$ "
South Side	Varies from $\pm 9 \frac{1}{2}$ " to $\pm 10 \frac{1}{2}$ "

*Dimension shown varies from beginning of approach span at bent to end of approach span at back of abutment.



PLAN

Note: See sheet 13 of 23 for Section C-C and View D-D.



CROSS SECTION

DESIGNED Stephen M. Ryan
CHECKED Phillip R. Litchfield
DRAWN R. Sommer
CHECKED SMR/SEM

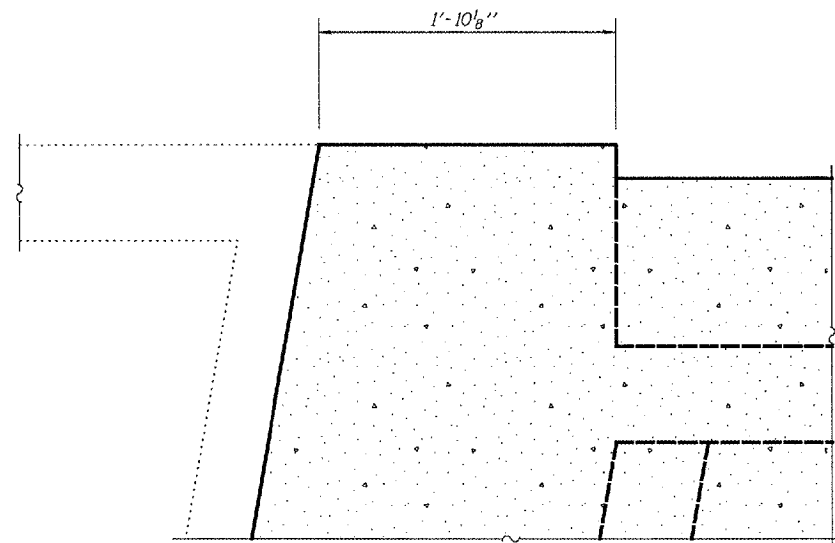
EXAMINED Thomas J. Demagali
PASSED Ralph E. Anderson

APPROACH SPAN
F.A.P. RT. 313 SECTION 18-HB
KNOX COUNTY
STATION 20+11.14
STRUCTURE NO. 048-0069

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

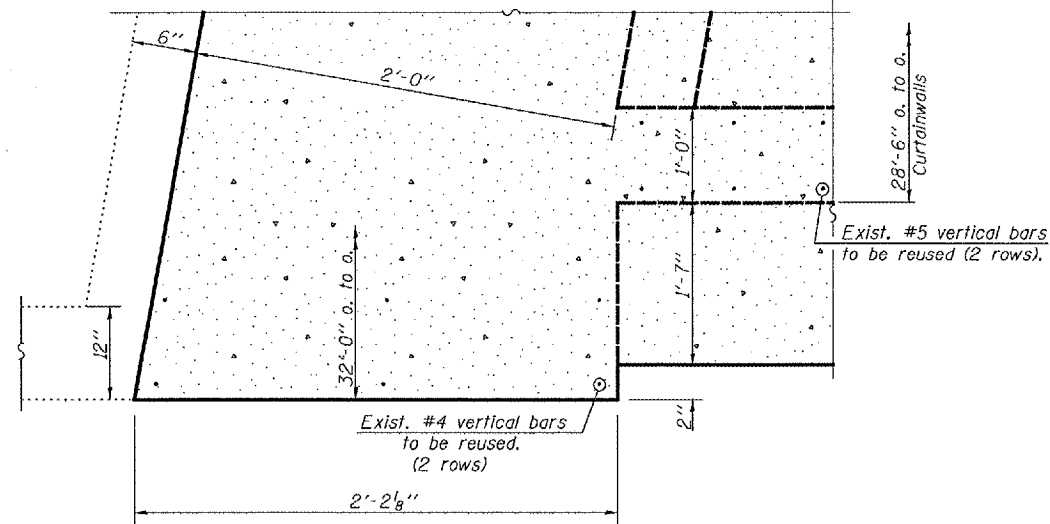
ROUTE NO.	SECTION	QUANTITY	SHEETS	SHEET	SHEET NO.
F.A.P. 313	18-HB	KNOX	76	36	23 SHEETS
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT		

Contract #68190



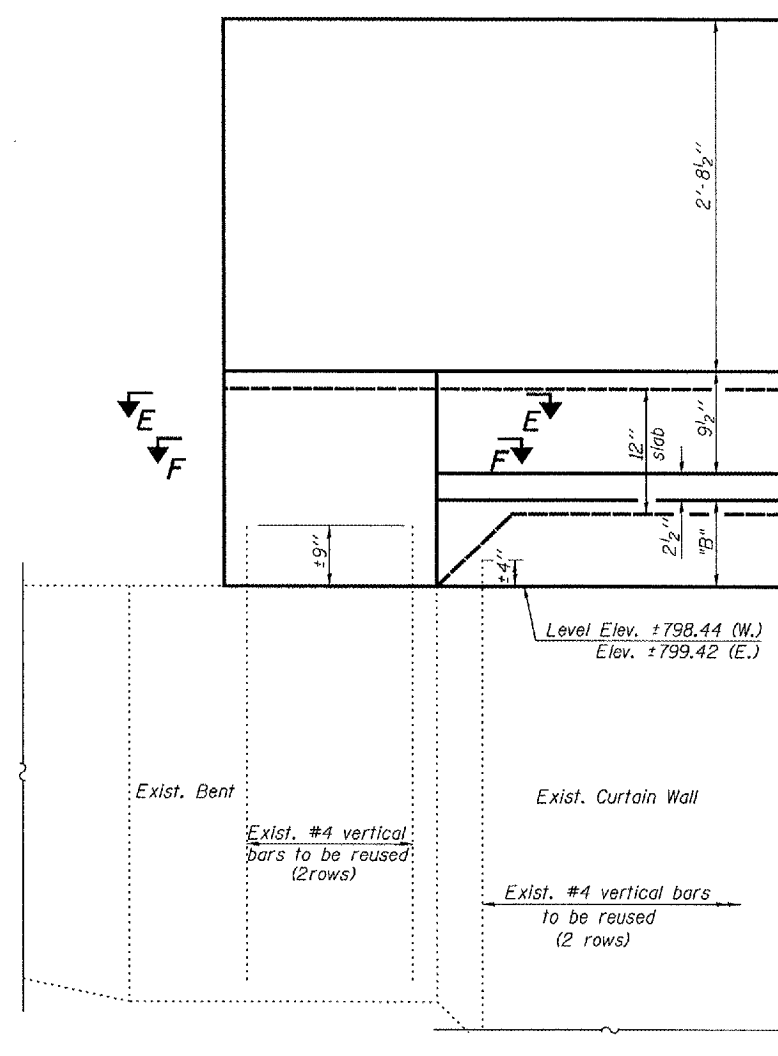
SECTION E-E

North end of West Bent and South end of East Bent.
Dimensions not shown here are the same as shown
in Section F-F.



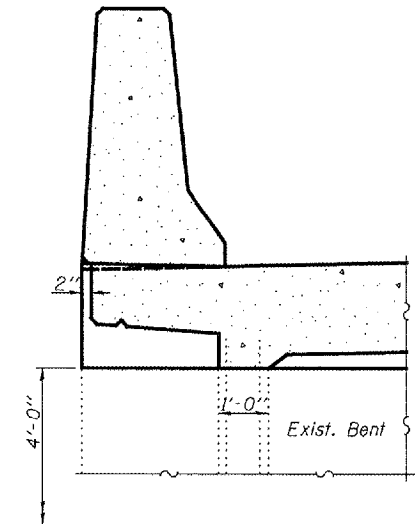
SECTION F-F

South end of West Bent and North end of East Bent.
For other locations, See Section E-E.



VIEW D-D

Details of South end of West Bent and North end of East Bent
are shown here and in Section F-F.
Details of North end of West Bent and South end of East Bent
are similar except as shown in Section E-E.



SECTION C-C

DESIGNED Stephen M. Ryan
CHECKED Phillip R. Litchfield
DRAWN R. Sommer
CHECKED SMR/SEM

2007	
EXAMINED	<i>Thomas J. Damgalaki</i>
PASSED	<i>Ralph E. Anderson</i>

APPROACH SPAN DETAILS
F.A.P. RT. 313 SECTION 18-HB
KNOX COUNTY
STATION 20+11.14
STRUCTURE NO. 048-0069

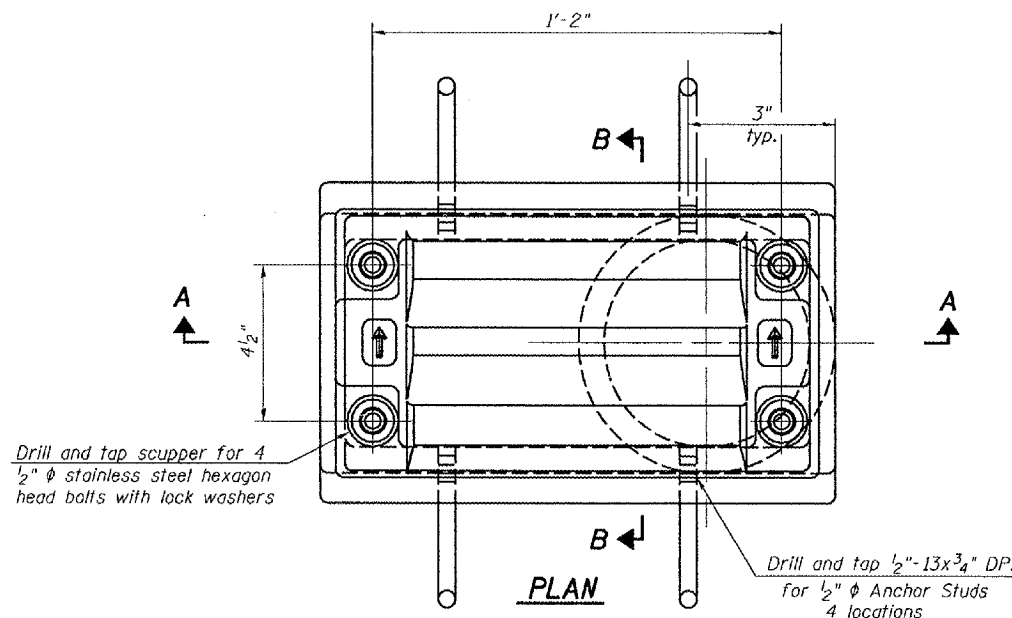
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	SHEET	SHEET	SHEET NO. 14
F.A.P. 313	18-HB	KNOX	70	31	23 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

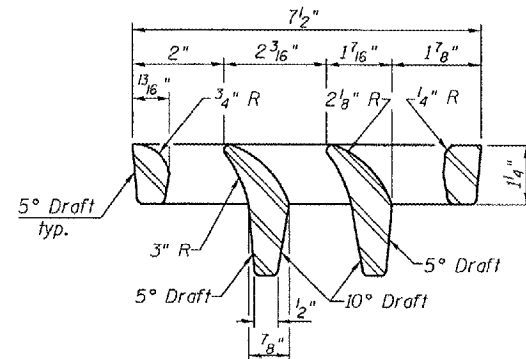
Contract #68190

Notes:

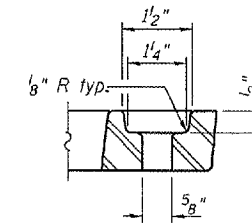
All cast iron parts shall be gray iron conforming to the requirements of AASHTO M 105, Class 35B.
Bolts, anchor studs, washers and nuts shall conform to the requirements of ASTM A 307 and shall be galvanized according to AASHTO M 232.
Downspouts located on the exterior side of a painted steel fascia beam shall be painted with the finish coat specified for the exterior side of the fascia beam.
As an alternate, bolts, anchor studs, washers and nuts may be stainless steel according to Article 1006.29(d) of the Standard Specifications.
Structural steel weldments of equal sections and of the same configuration may be substituted for the cast iron scupper frame. Fillet or full penetration welds shall be used for the weldments. Details shall be submitted to the Engineer for approval. Structural steel weldments shall not be substituted for the cast iron scupper grate. Structural steel frames and downspouts shall be galvanized according to AASHTO M 111.
The Contractor shall take appropriate measures to assure that Protective Coat is not applied to the scupper.
Cost of the Grate, Frame, Downspout, Anchor Studs, Bolts, Washers and Nuts including complete installation of the scupper shall be paid for at the contract unit price each for Drainage Scupper, DS-11.
Alternate fiberglass downspout conforming to ASTM D 2996 with a short-time rupture strength hoop tensile stress of 30,000 psi min. may be used in lieu of the cast iron or steel equivalent.



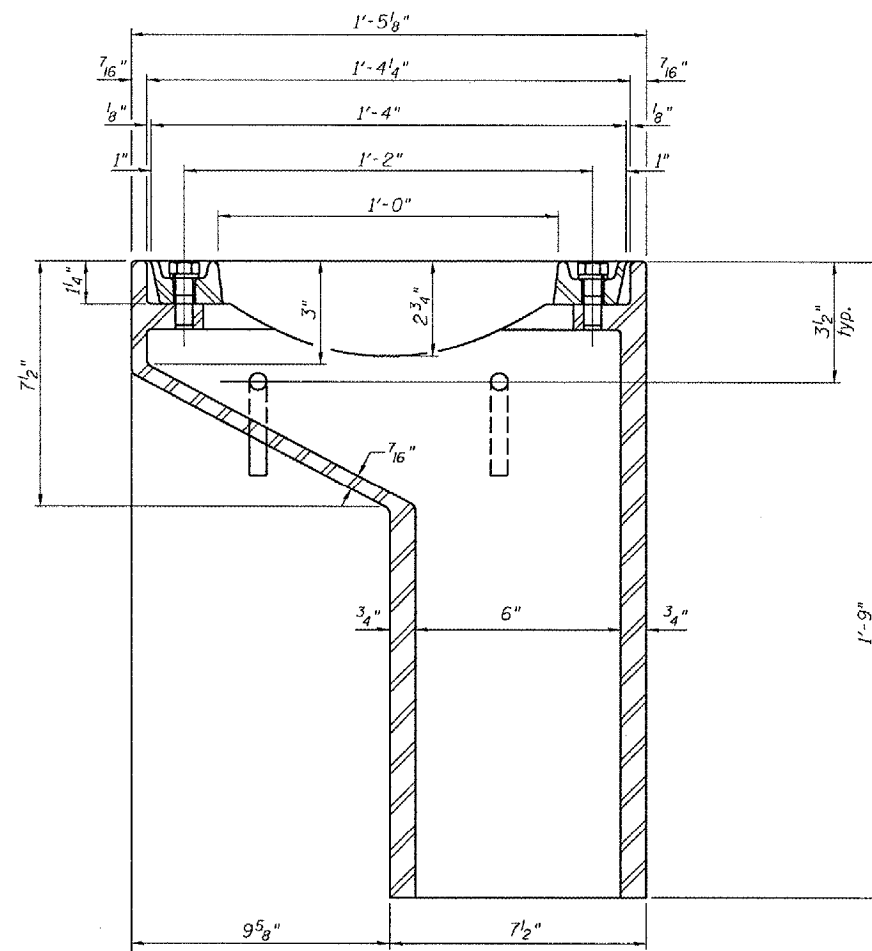
PLAN



VANE GRATE DETAIL

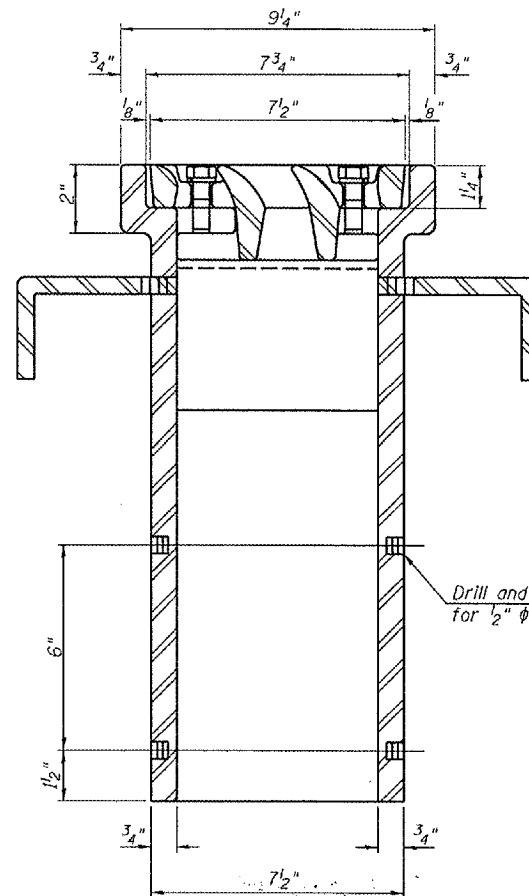


BOLT HOLE DETAIL



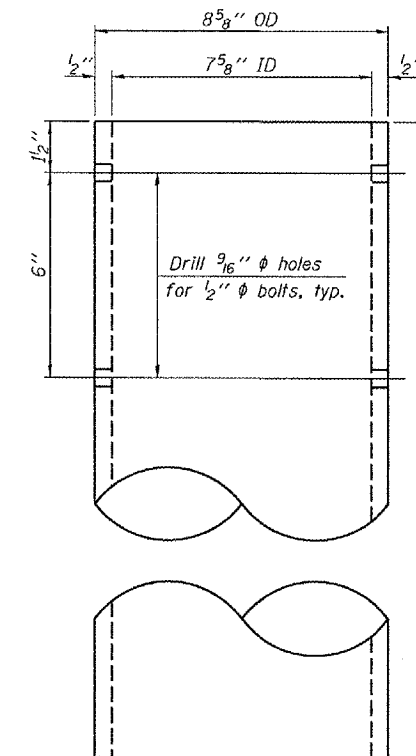
SECTION A-A

See sheet 10 of 23 for scupper location relative to parapet.

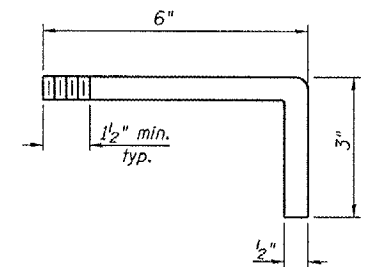


SECTION B-B

Drill and tap 1/2"-13x1/2" DP. for 1/2" φ bolts. (4 locations)



DOWNSPOUT



ANCHOR STUD DETAIL

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Drainage Scupper, DS-11	Each	4

DESIGNED Stephen M. Ryan
CHECKED Phillip R. Litchfield
DRAWN R. Sommer
CHECKED SMR/SEM

EXAMINED Thomas J. Domagalabi	Nov. 20 2007
PASSED Ralph E. Anderson	

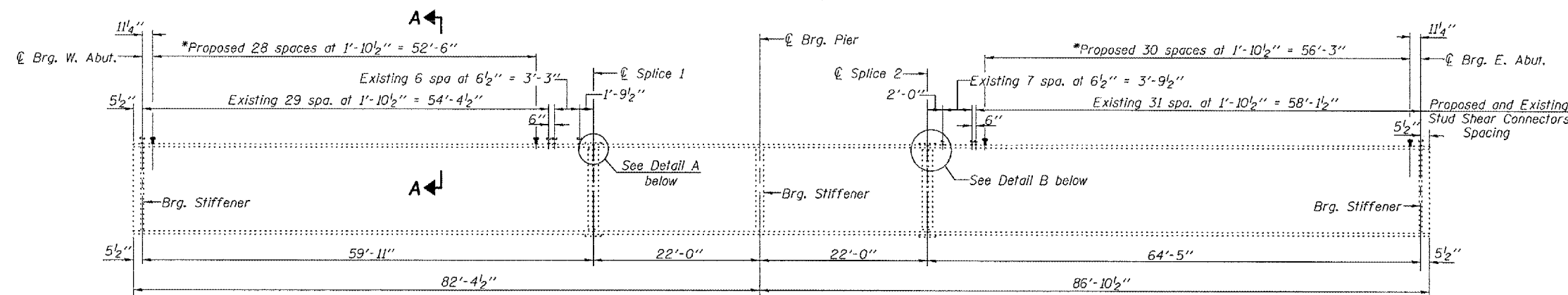
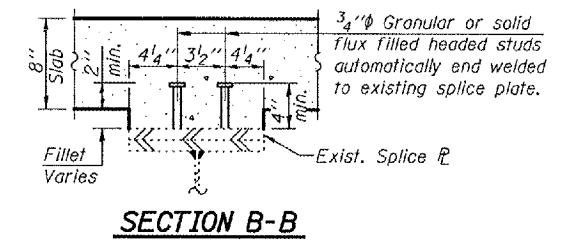
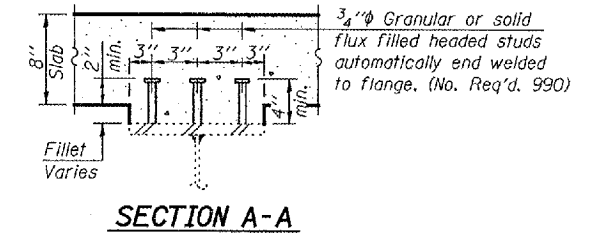
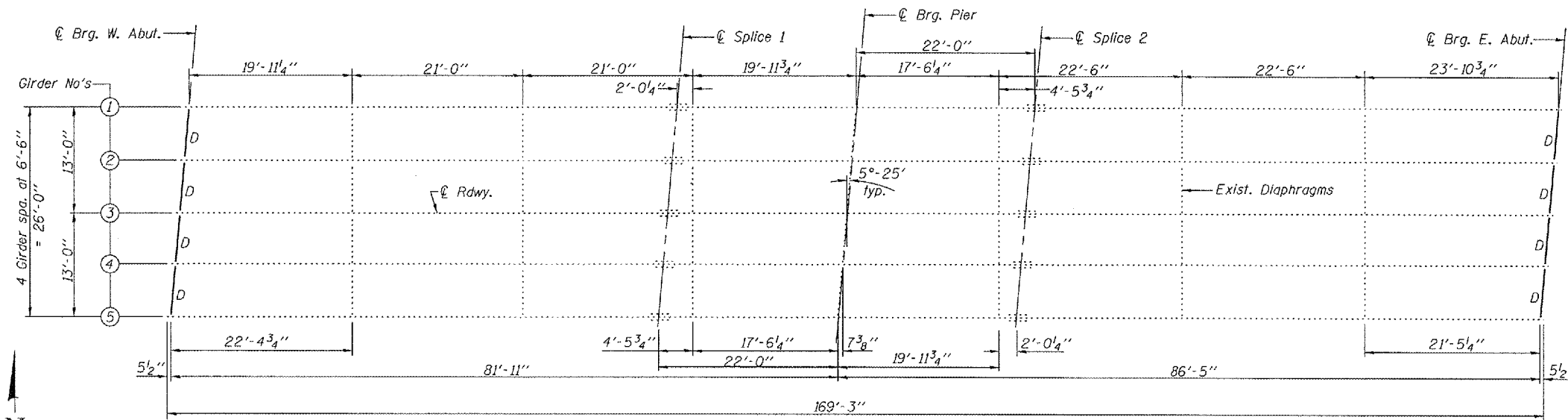
DS-11 9-3-07

DRAINAGE SCUPPER, DS-11
F.A.P. RT. 313 SECTION 18-HB
KNOX COUNTY
STATION 20+11.14
STRUCTURE NO. 048-0069

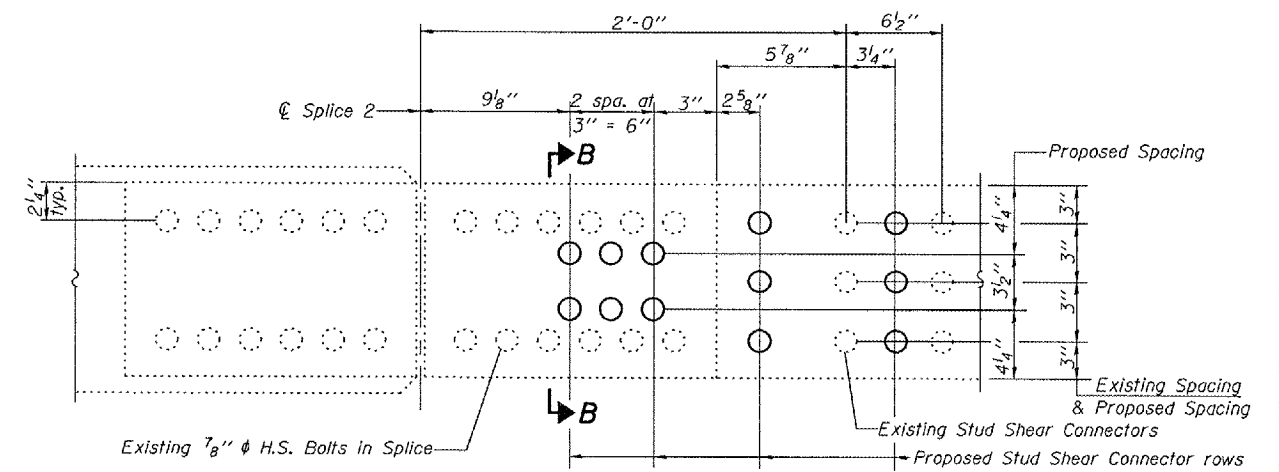
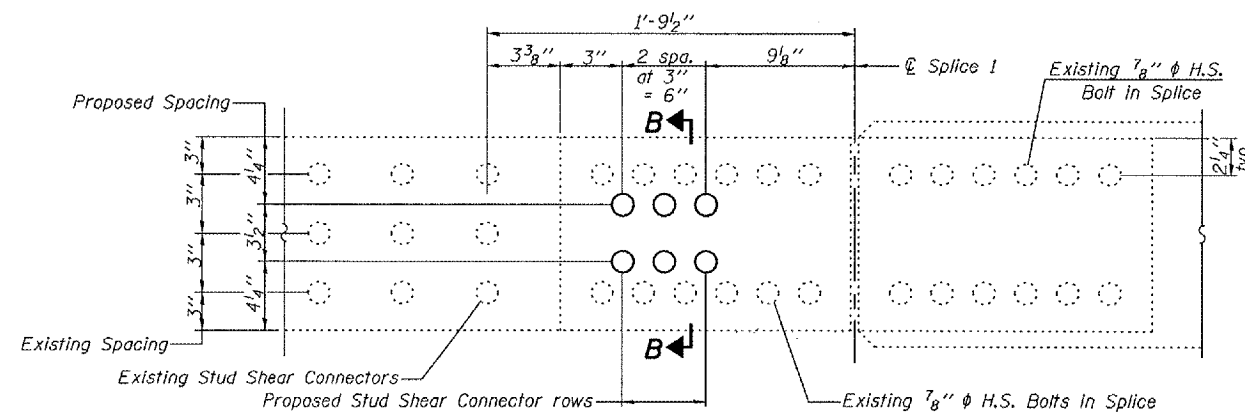
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	SHEETS	NO.	SHEET NO. 15
F.A.P. 313	18-HB	KNOX	70	38	23 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

Contract #68190



*Place proposed stud shear connectors at mid-point between existing stud shear connectors for an alternating proposed and existing stud shear connector spacing of 11 1/4".



DESIGNED	Stephen M. Ryan
CHECKED	Phillip R. Litchfield
DRAWN	R. Sommer
CHECKED	SMR/SEM

Nov. 20 2007

EXAMINED *Thomas J. Demasalebi*
ENGINEER OF BRIDGE DESIGN

PASSED *Ralph E. Anderson*
ENGINEER OF BRIDGES AND STRUCTURES

STRUCTURAL STEEL
F.A.P. RT. 313 SECTION 18-HB
KNOX COUNTY
STATION 20+11.14
STRUCTURE NO. 048-0069

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

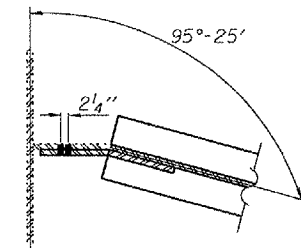
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F.A.P. 313	18-HB	KNOX	10	37
FED. PROJ. DIST. NO. 7	ILLINOIS	FED. AID PROJECT		23 SHEETS

Contract #68190

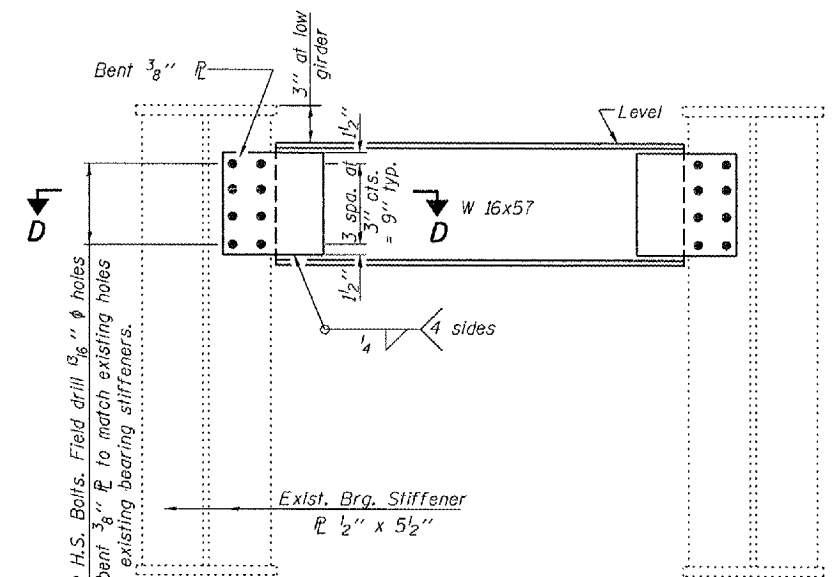
	0.4 Sp. 1	Pier	0.6 Sp. 2
I_s	11972	19829	11972
$I_c(n)$	31416		31416
$I_c(3n)$	23111		23111
S_s	531	896	531
$S_c(n)$	822		822
$S_c(3n)$	747		747
Z	997		
ϕ	0.871	1.299	0.871
$M\phi$	344	1182	422
$s\phi$	0.343		0.343
$M_s\phi$	171		202
M_t	591	438	629
$M(Imp)$	144	105	148
$S_s[M_t + M(Imp)]$	1225	905	1295
M_a	2262	2713	2495
M_u	3359	2991	3359
$f_s\phi$ non-comp	7.8	15.8	9.5
$f_s\phi$ comp	2.7		3.2
$f_s S_s [M_t + M(Imp)] / k.s.i.$	17.9	12.1	18.9
f_s (Overload)	28.4	27.9	31.6
f_s (Total)			
VR	50.1		49.7

	W. Abut.	Pier	E. Abut.
$R\phi$	35.6	133.7	39.1
R_t	36.8	54.1	37.0
Imp.	8.9	12.9	8.7
R (Total)	81.3	200.7	84.8

- I_s, S_s : Non-composite moment of inertia and section modulus of the steel section used for computing f_s (Total and Overload) due to non-composite dead loads (in.⁴ and in.³).
- $I_c(n), S_c(n)$: Composite moment of inertia and section modulus of the steel and deck based upon the modular ratio, "n", used for computing f_s (Total and Overload) due to short-term composite live loads (in.⁴ and in.³).
- $I_c(3n), S_c(3n)$: Composite moment of inertia and section modulus of the steel and deck based upon 3 times the modular ratio, "3n", used for computing f_s (Total and Overload) due to long-term composite (superimposed) dead loads (in.⁴ and in.³).
- Z : Plastic Section Modulus of the steel section in non-composite areas (in.³).
- ϕ : Un-factored non-composite dead load (kips/ft.).
- $M\phi$: Un-factored moment due to non-composite dead load (kip-ft.).
- $s\phi$: Un-factored long-term composite (superimposed) dead load (kips/ft.).
- $M_s\phi$: Un-factored moment due to long-term composite (superimposed) dead load (kip-ft.).
- M_t : Un-factored live load moment (kip-ft.).
- M_{Imp} : Un-factored moment due to impact (kip-ft.).
- M_a : Factored design moment (kip-ft.).
- M_u : Compact composite moment capacity according to AASHTO LFD 10.50.1.1 or compact non-composite moment capacity according to AASHTO LFD 10.48.1 (kip-ft.).
- f_s (Overload): Sum of stresses as computed from the moments below (ksi).
- f_s (Total): Sum of stresses as computed from the moments below on non-compact section (ksi).
- $1.3 [M\phi + M_s\phi + \frac{5}{3} (M_t + M_{Imp})]$
- VR: Maximum t + impact horizontal shear range within the composite portion of the span for stud shear connector design (kips).



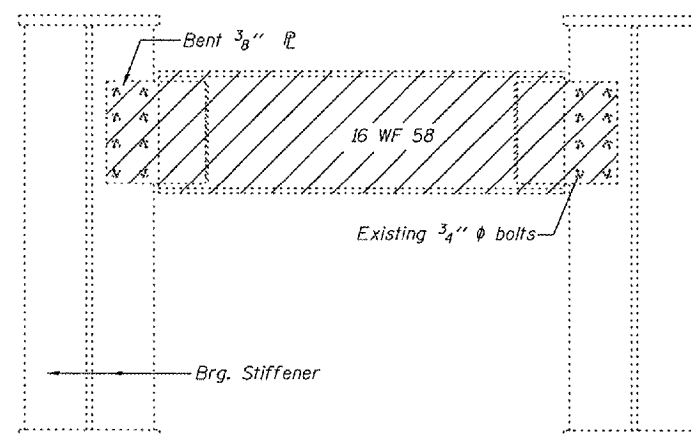
SECTION D-D



END DIAPHRAGM D
(8 required)

Notes:
Field verify all dimensions prior to ordering steel.
Cost of field drilling included with Furnishing and Erecting Structural Steel.
Diaphragms at supports may be temporarily disconnected to install bearing anchor rods.
Care shall be taken not to damage existing steel that is to be reused.

Indicates steel to be removed as Structural Steel Removal



END DIAPHRAGM REMOVAL
(8 locations total, 4 of each abutment)

BILL OF MATERIAL

Item	Unit	Total
Structural Steel Removal	Pound	2820

DESIGNED Stephen M. Ryan
CHECKED Phillip R. Litchfield
DRAWN R. Sommer
CHECKED SMR/SEM

Nov. 20 2007
EXAMINED *Thomas J. Nangalaki*
PASSED *Ralph E. Anderson*

STRUCTURAL STEEL DETAILS
F.A.P. RT. 313 SECTION 18-HB
KNOX COUNTY
STATION 20+11.14
STRUCTURE NO. 048-0069

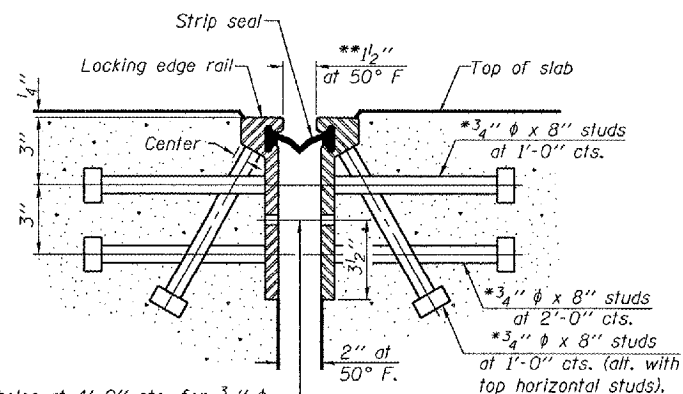
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	SHEETS	SHEET	SHEET NO. 17
F.A.P. 313	18-HB	KNOX	70	40	23 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-			

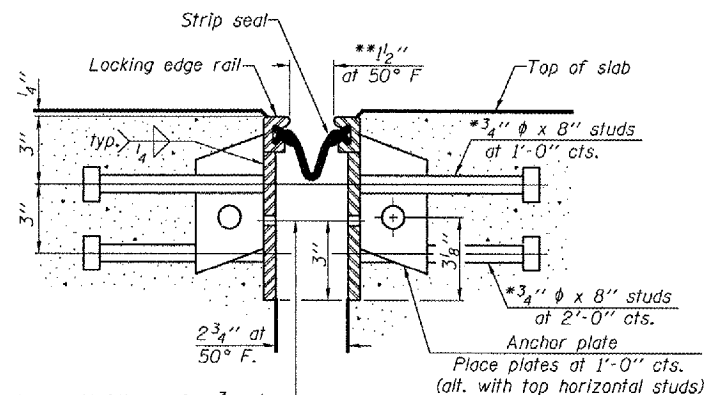
Contract #68190

*Granular or solid Flux filled headed studs conforming to Article 1006.32 of the Std. Specs.. automatically end welded.

**When joint is fixed, dimension is set at 1 1/2".



7/16" φ holes at 4'-0" cts. for 3/8" φ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.



7/16" φ holes at 4'-0" cts. for 3/8" φ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The height and thickness of the Locking Edge Rails shown are minimum dimensions. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities and stage construction joints.

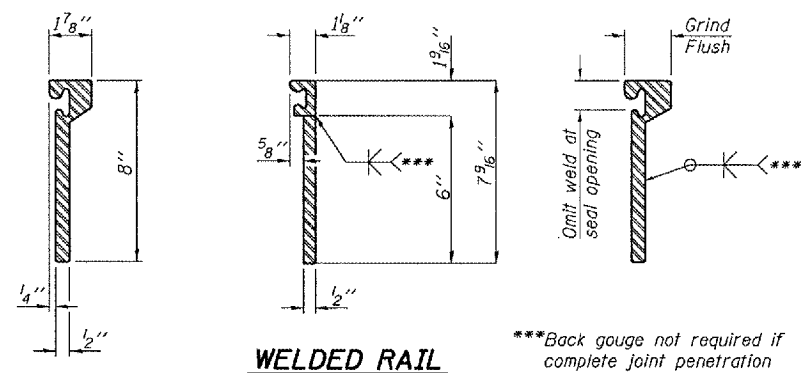
The manufacturer's recommended installation methods shall be followed.

The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

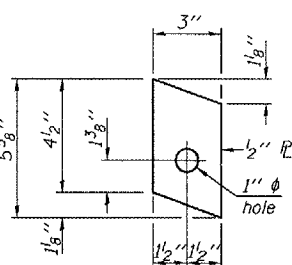
SECTION THRU
ROLLED RAIL JOINT

SECTION THRU
WELDED RAIL JOINT

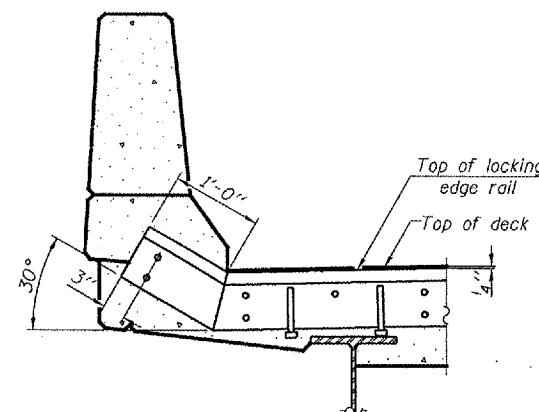


WELDED RAIL

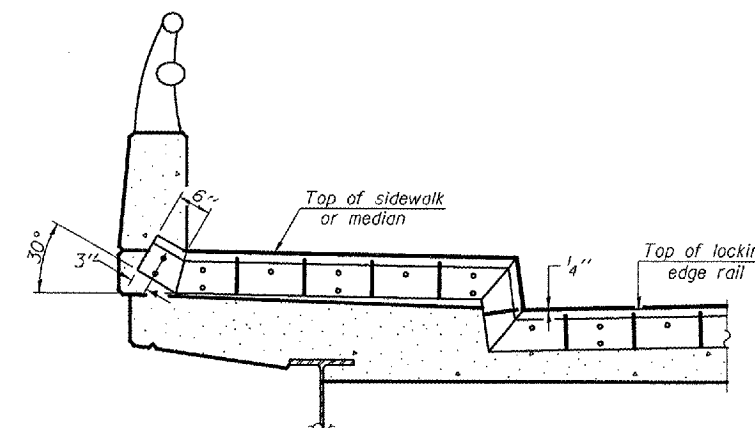
***Back gouge not required if complete joint penetration is verified by mock-up.



ANCHOR PLATE
(For welded rail)



AT PARAPET



AT SIDEWALK OR MEDIAN

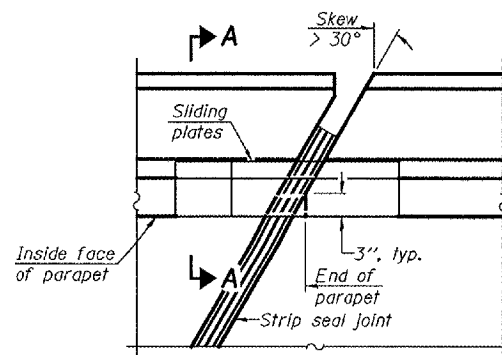
Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.

ROLLED
(EXTRUDED) RAIL

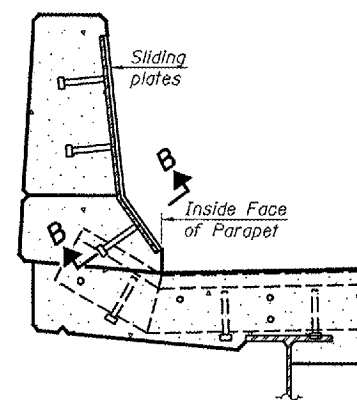
LOCKING EDGE
RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue.

LOCKING EDGE RAILS



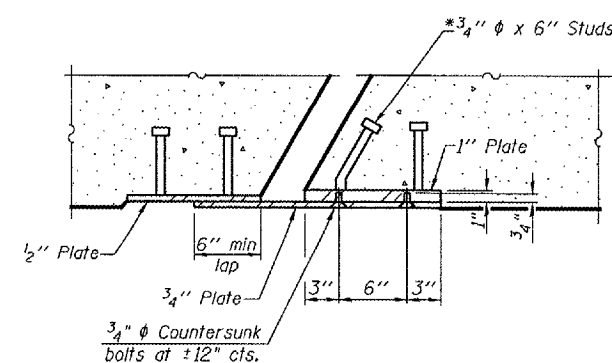
PLAN



SECTION A-A

POINT BLOCK DETAILS
(for skews > 30°)

TYPICAL END TREATMENTS



SECTION B-B

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	62.0

PREFORMED JOINT STRIP SEAL
F.A.P. RT. 313 SECTION 18-HB
KNOX COUNTY
STATION 20+11.14
STRUCTURE NO. 048-0069

DESIGNED	Stephen M. Ryan
CHECKED	Phillip R. Litchfield
DRAWN	R. Sommer
CHECKED	SMR/SEM

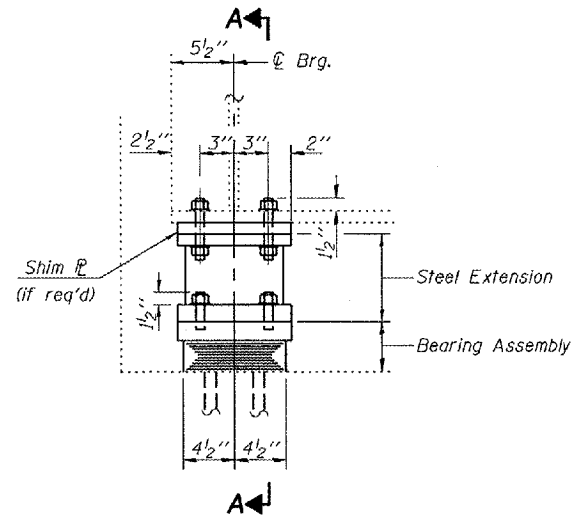
EXAMINED	Thomas J. Demagala	Nov. 20 2007
PASSED	Ralph E. Anderson	

EJ-SSJ 9-3-07

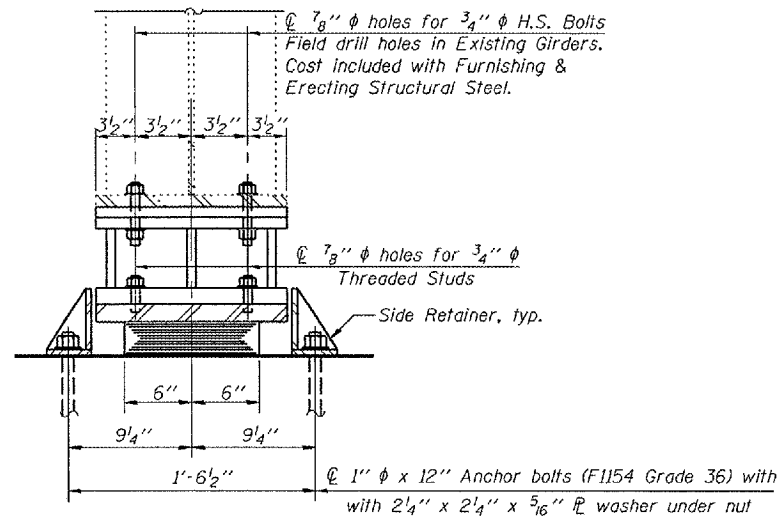
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	SHEET	SHEET NO. 18
F.A.P. 313	18-HB	KNOX	70 41	23 SHEETS
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	

Contract #68190

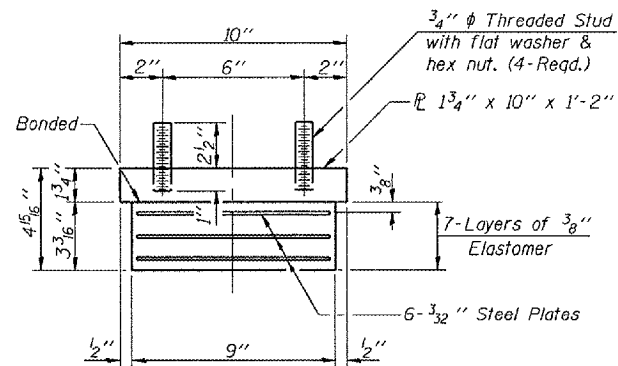


ELEVATION AT ABUT.



SECTION A-A

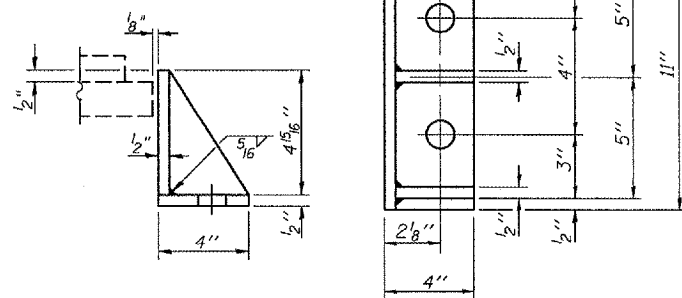
TYPE I ELASTOMERIC EXP. BRG.
(10 Req'd.)



BEARING ASSEMBLY

Note:
Shim plates shall not be placed under Bearing Assembly.

Notes:
Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade and diameter specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi).
Two 1/8 in. adjusting shims shall be provided for each bearing in addition to all other plates or shims and placed as shown on bearing details.
Anchor bolts for side retainers shall be installed in holes drilled before or after members are in place.
Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.
Side retainers and other steel members required for the bearing assembly shall be included in the cost of Elastomeric Bearing Assembly, Type I.

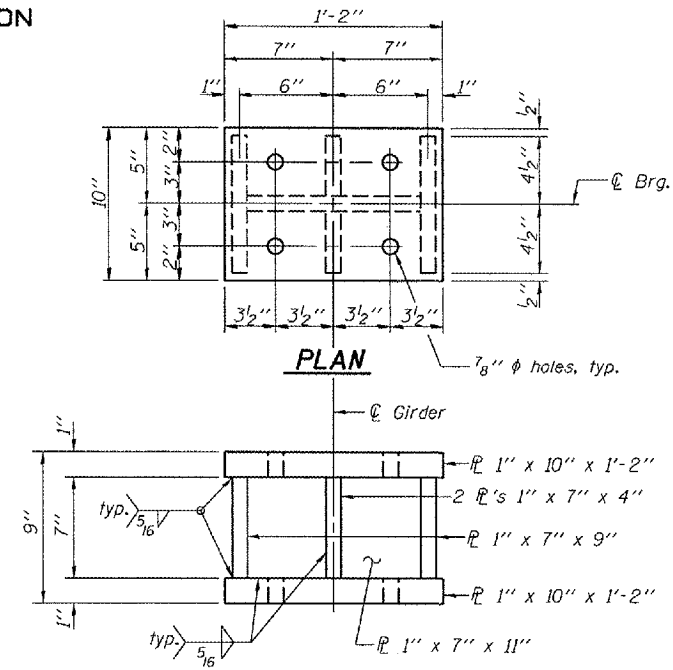


SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

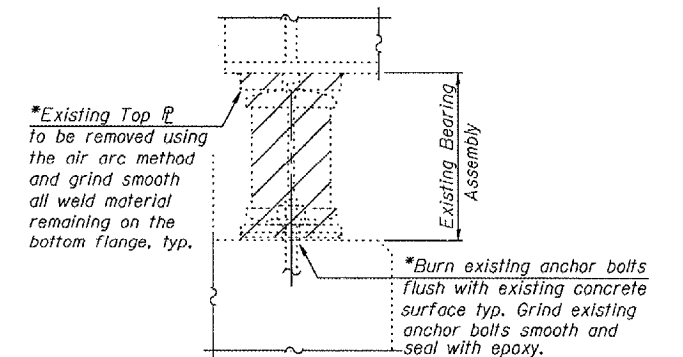
DESIGNED	Stephen M. Ryan
CHECKED	Phillip R. Litchfield
DRAWN	R. Sommer
CHECKED	SMR/PRL

Nov. 20 2007
EXAMINED *Thomas J. Donagallo*
PASSED *Ralph E. Anderson*
ENGINEER OF BRIDGES AND STRUCTURES



ELEVATION

STEEL EXTENSIONS AT ABUTMENT
(10 required)



JACK AND REMOVE EXISTING BEARING

Hatched areas indicate removal of existing bearings.
*Cast included with Jack and Remove Existing Bearings.

**JACK AND REMOVE EXISTING BEARING PROCEDURE
WEST AND EAST ABUTMENTS**

- Jacking and removing existing bearings shall be done after existing deck removal is complete.
- The Contractor shall submit, for approval by the Engineer, plans for jacking existing girders and installing new bearings prior to commencing any related work. This work shall be done after existing concrete deck is removed and prior to pouring of the new concrete deck. The max. dead load reaction per beam (weight of steel only) per bearing at Abutments is 5.0 kips. Minimum jack capacity per beam is 7.5 kips at Abutments.
- Prior to ordering any material, the Contractor shall verify steel extension height required at each bearing so that total height of new bearing and steel extension matches height of existing bearing and shims.
- There shall be at least one jack per bearing and the jack shall be placed close to the bearing. The steel shall be raised a maximum of 1/8".
- The new bearings and steel extensions shall be in place and the jacks shall be lowered before the new concrete deck is poured.

BILL OF MATERIAL

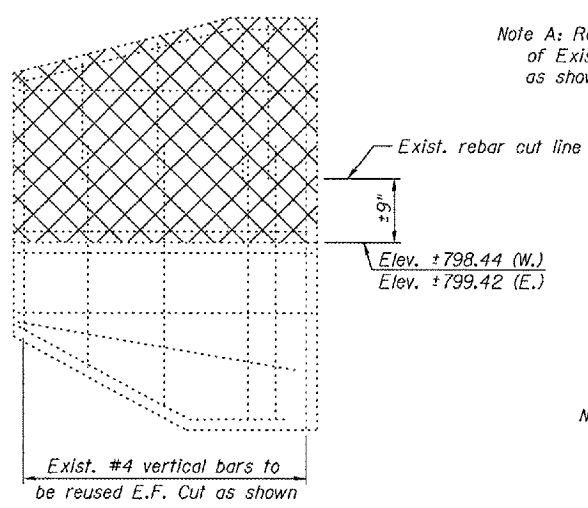
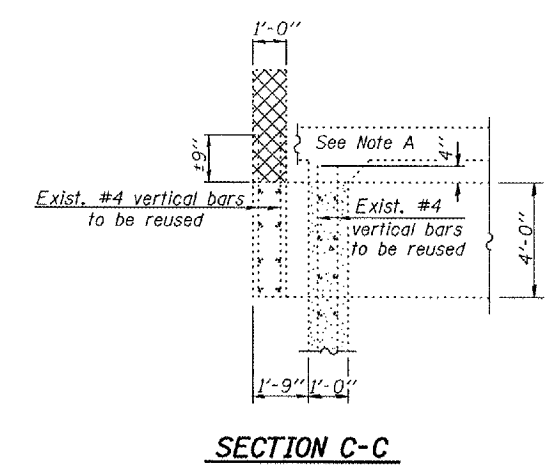
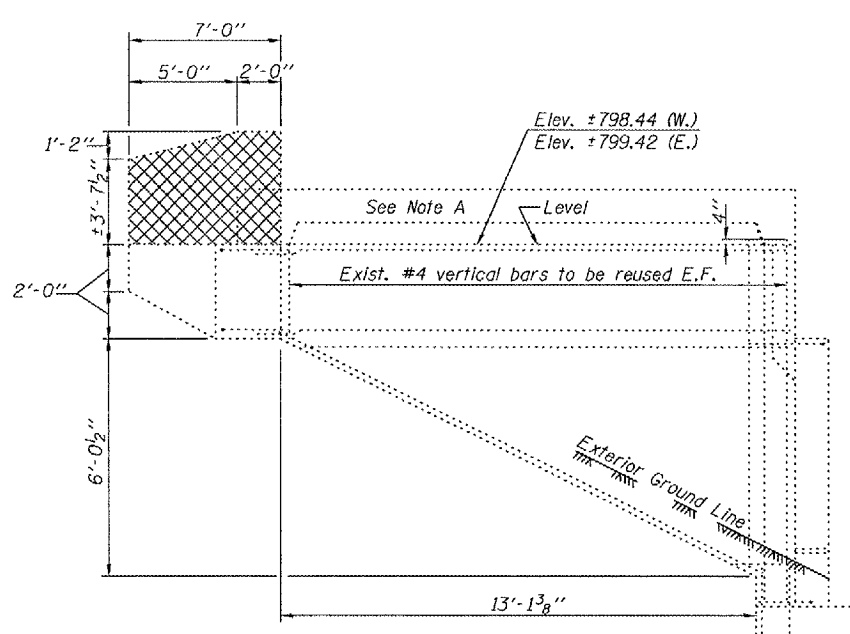
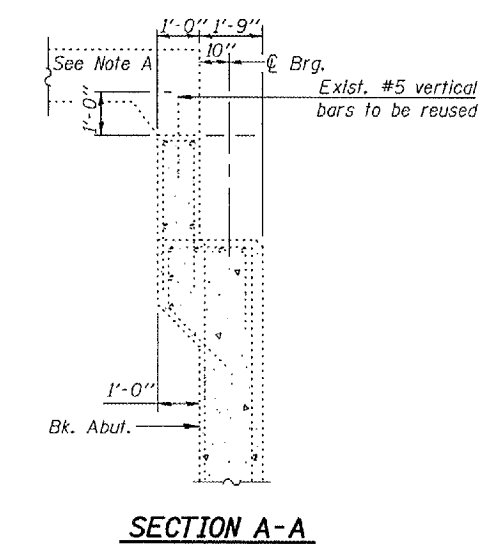
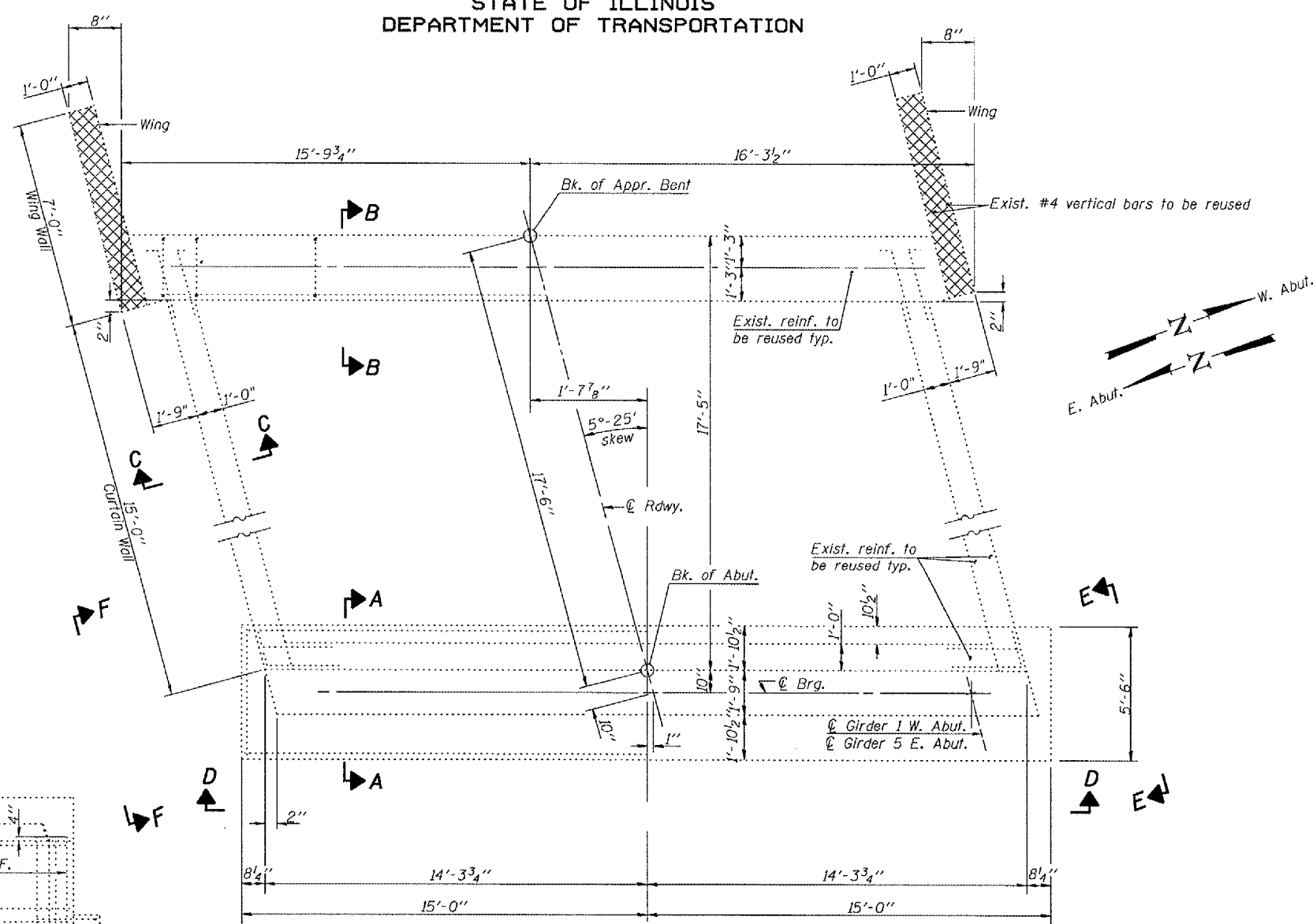
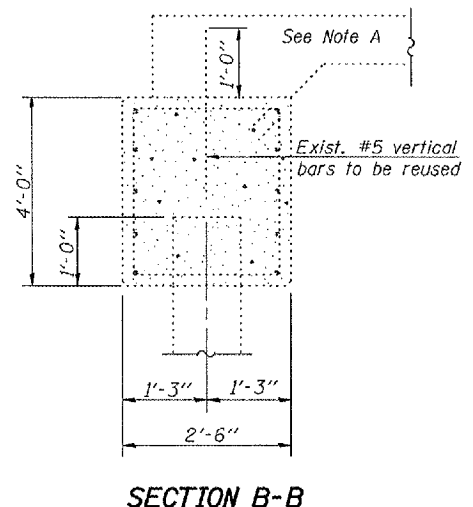
Item	Unit	Total
Elastomeric Bearing Assembly Type I	Each	10
Anchor Bolts 1"	Each	40
Jack and Remove Existing Bearings	Each	10

BEARING DETAILS
F.A.P. RT. 313 SECTION 18-HB
KNOX COUNTY
STATION 20+11.14
STRUCTURE NO. 048-0069

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 19
F.A.P. 313	18-HB	KNOX	70	42	23 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-			

Contract #68190



Note A: Removal of the existing approach span slabs is included in Removal of Existing Concrete Deck. Note that portions of the existing reinforcement as shown are to be reused in the new approach span slabs.

BILL OF MATERIAL

Item	Unit	Total
Concrete Removal	Cu. Yd.	4.5
Structural Repair of Concrete (Depth equal to or less than 5 inches)	Sq. Ft.	139
Epoxy Crack Injection	Foot	13.5

DESIGNED Stephen M. Ryan
CHECKED Phillip R. Litchfield
DRAWN R. Sommer
CHECKED SMR/SEM

EXAMINED *Thomas J. Demagalli*
PASSED *Ronald E. Anderson*
NOV. 20 2007
ENGINEER OF BRIDGE DESIGN
ENGINEER OF BRIDGES AND STRUCTURES

Notes: For Views D-D, E-E and F-F see sheet 20 of 23. Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

CONCRETE REMOVAL & REPAIR DETAILS-ABUTMENTS
F.A.P. RT. 313 SECTION 18-HB
KNOX COUNTY
STATION 20+11.14
STRUCTURE NO. 048-0069

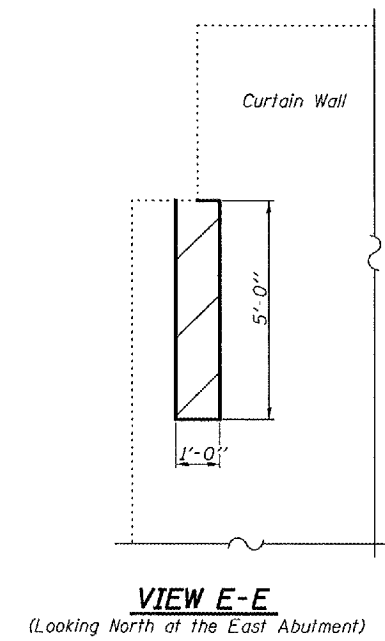
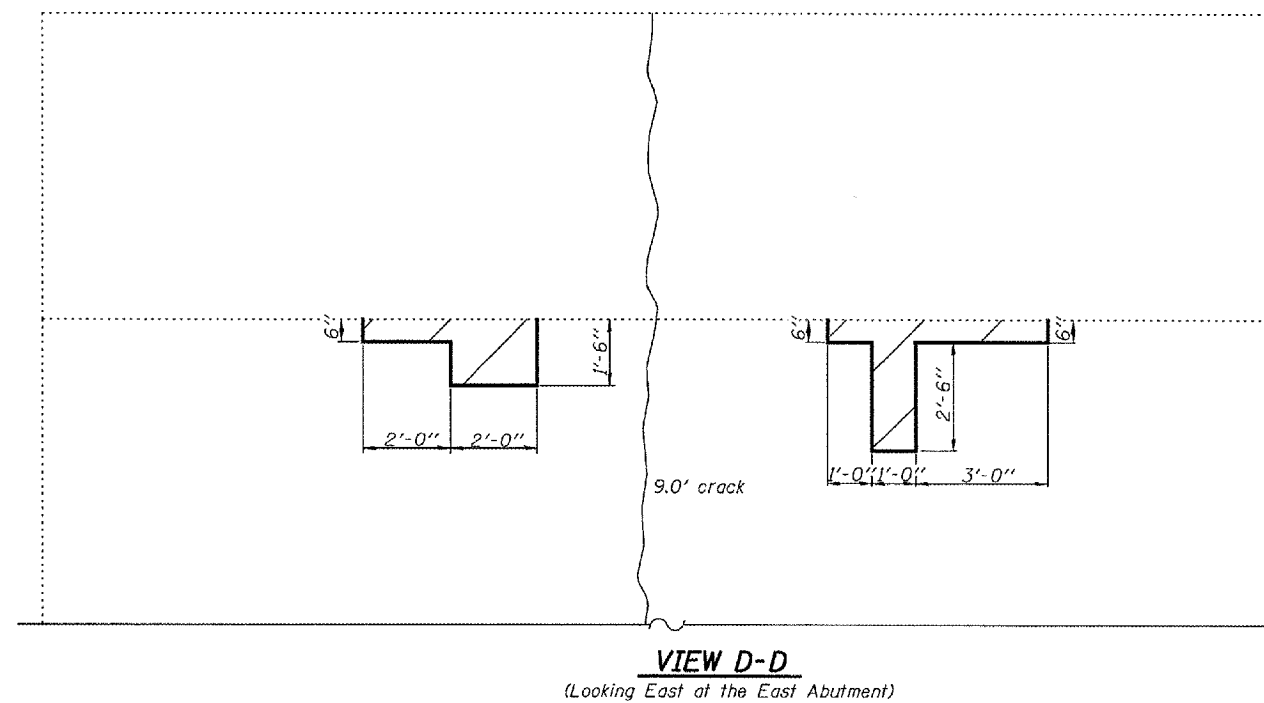
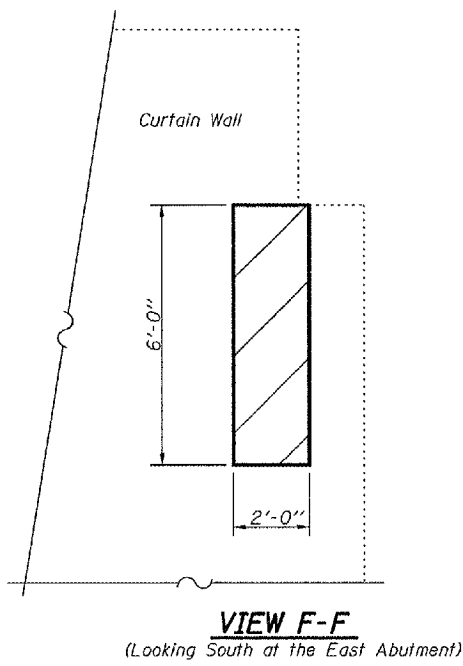
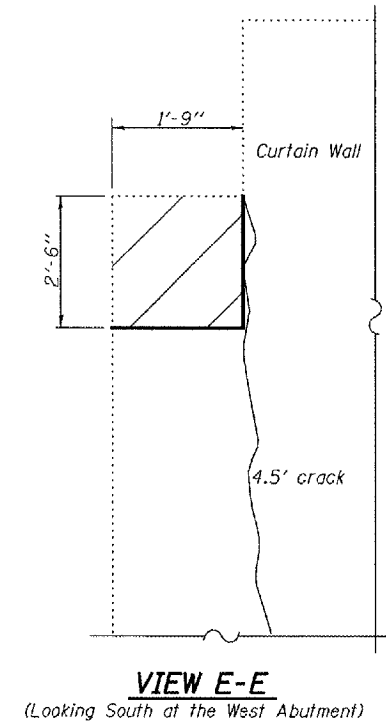
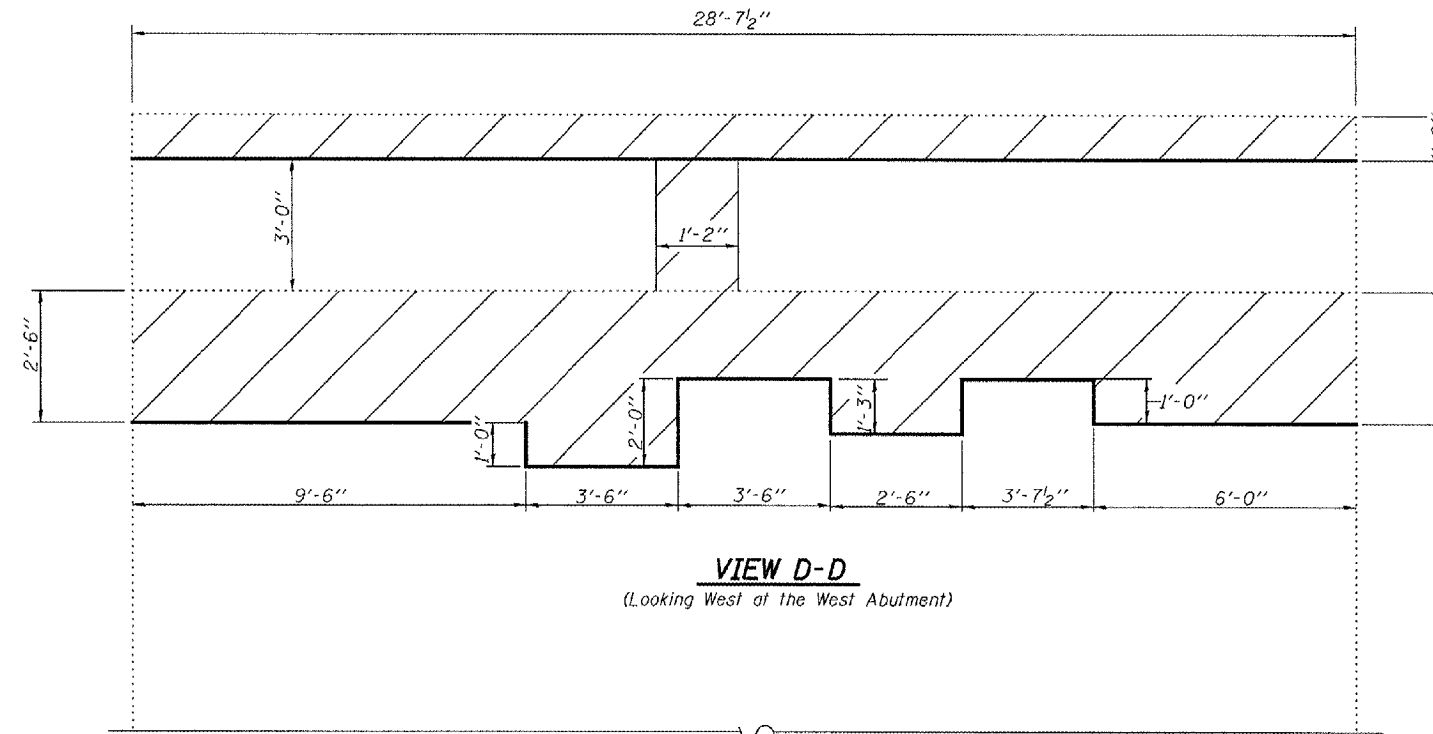
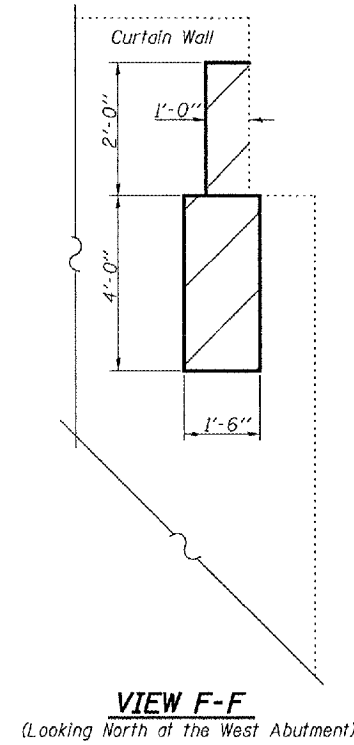
Indicates Concrete Removal

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	SHEET NO.	SHEET NO.
F.A.P. 313	18-HB	KNOX	70	43
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	

Contract #68190

SHEET NO. 20
23 SHEETS



DESIGNED	Stephen M. Ryan
CHECKED	Phillip R. Litchfield
DRAWN	R. Sommer
CHECKED	SMR/SEM

Nov. 20 2007
EXAMINED *Thomas J. Demagallibi*
PASSED *Ralph E. Anderson*
ENGINEER OF BRIDGES AND STRUCTURES

Epoxy Crack Injection

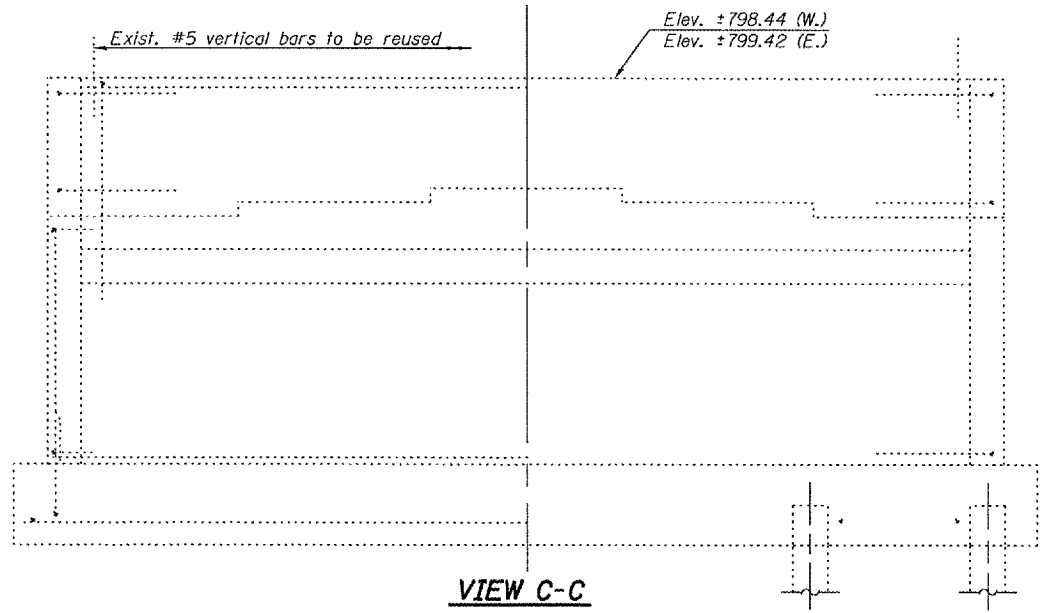
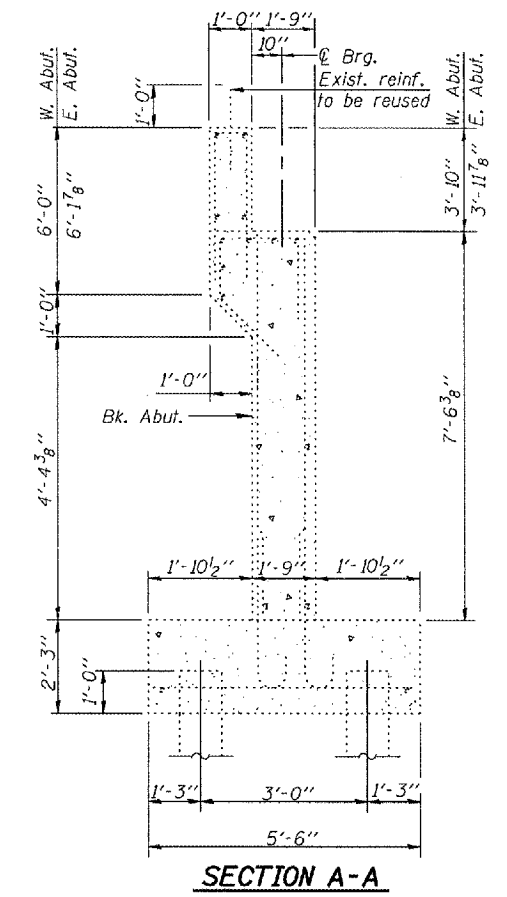
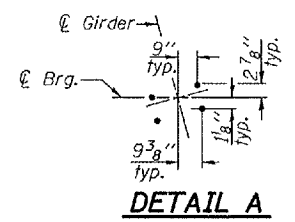
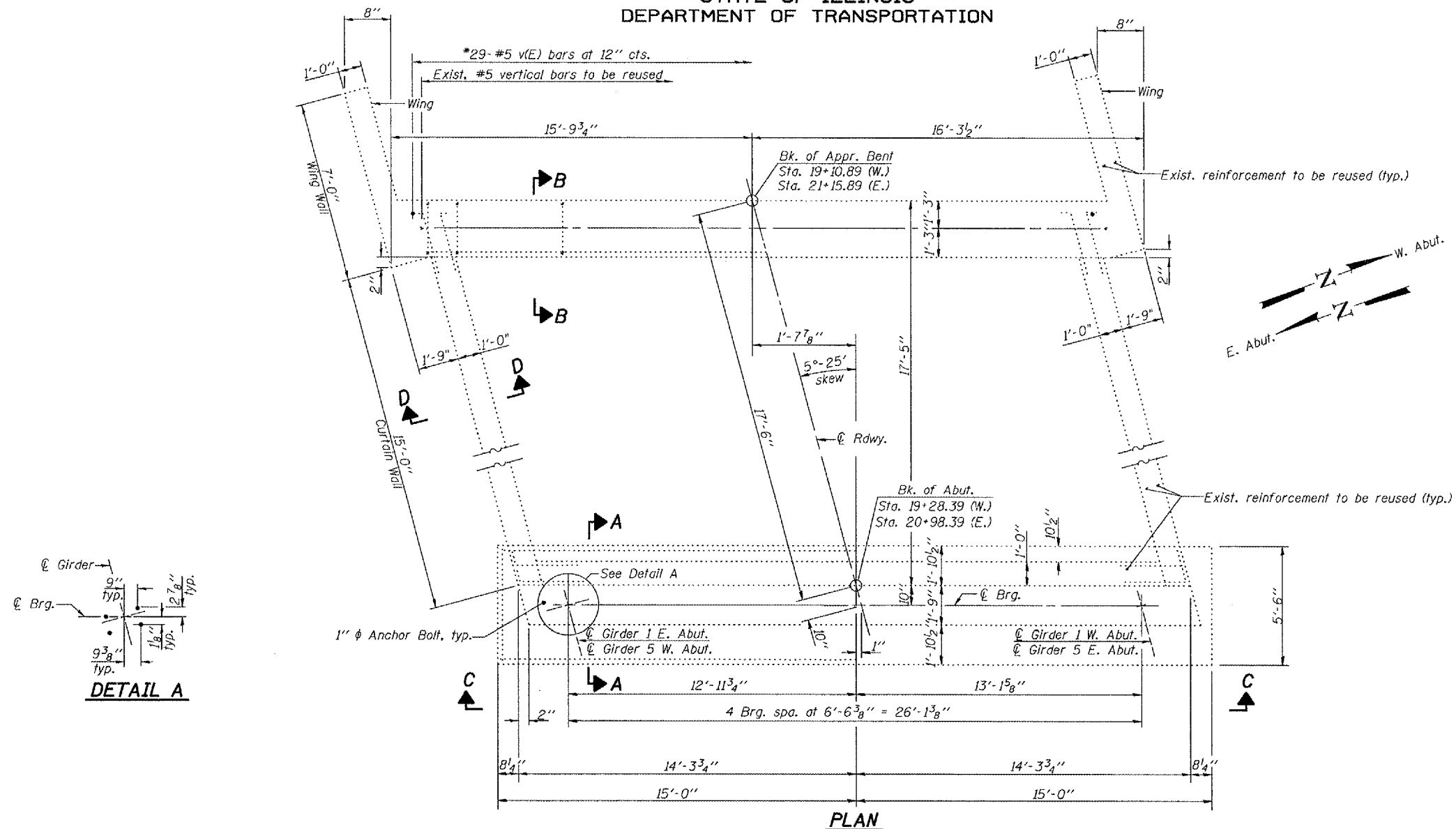
Structural Repair of Concrete
(Depth equal to or less than
5 inches)

CONCRETE REMOVAL & REPAIR DETAILS-ABUTMENTS
F.A.P. RT. 313 SECTION 18-HB
KNOX COUNTY
STATION 20+11.14
STRUCTURE NO. 048-0069

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	DRAWN	DATE	SHEET	SHEET NO. 21
F.A.P. 313	18-HB	KNOX	70	44	23 SHEETS
FED. ROAD DIST. NO. 7	ILL. HIGHWAY	FED. AID PROJECT			

Contract #68190



**TWO ABUTMENTS
BILL OF MATERIAL**

Bar	No.	Size	Length	Shape	
v(E)	58	#5	1'-9"		
Reinforcement Bars, Epoxy Coated				Pound	110

*Epoxy grout v(E) bars in min. 9" drilled holes according to Sec. 584 of the Standard Specifications.

ABUTMENTS
F.A.P. RT. 313 SECTION 18-HB
KNOX COUNTY
STATION 20+11.14
STRUCTURE NO. 048-0069

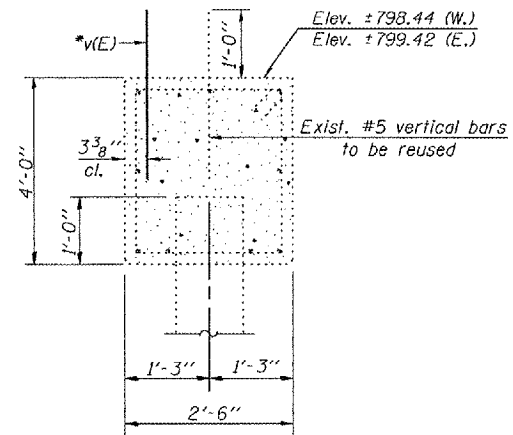
DESIGNED Stephen M. Ryan
CHECKED Phillip R. Litchfield
DRAWN R. Sommer
CHECKED SMR/SEM

EXAMINED *Thomas J. Damgalaki*
PASSED *Ralph E. Anderson*
NOV. 20 2007
ENGINEER OF BRIDGE DESIGN
ENGINEER OF BRIDGES AND STRUCTURES

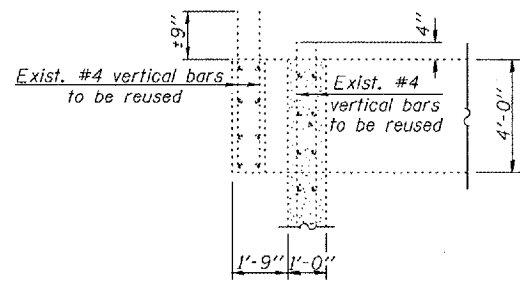
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	JOB SHEETS	SHEET	SHEET NO. 22
F.A.P. 313	18-HB	KNOX	70	45	23 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

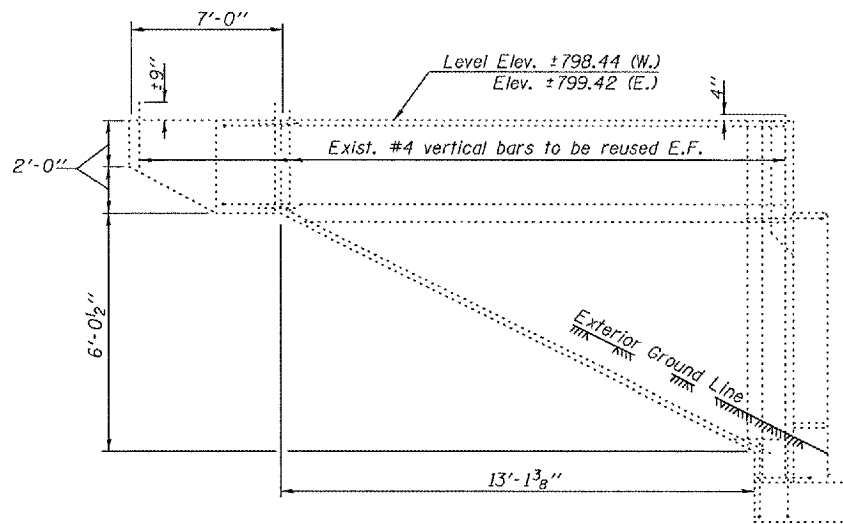
Contract #68190



SECTION B-B



SECTION D-D



SIDE ELEVATION

DESIGNED Stephen M. Ryan
CHECKED Phillip R. Litchfield
DRAWN R. Sommer
CHECKED SMR/SEM

Nov. 20 2007
EXAMINED *Thomas J. Demagala*
PASSED *Ralph E. Anderson*
ENGINEER OF BRIDGES AND STRUCTURES

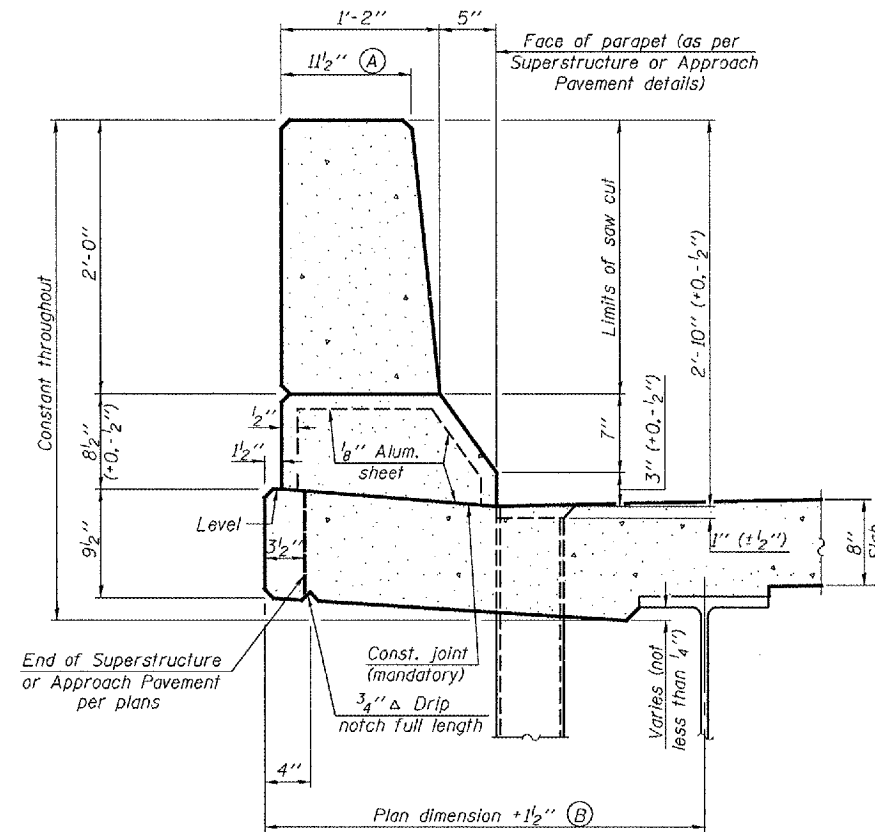
ABUTMENT DETAILS
F.A.P. RT. 313 SECTION 18-HB
KNOX COUNTY
STATION 20+11.14
STRUCTURE NO. 048-0069

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	SHEET NO.	SHEET NO.
F.A.P. 313	18-HB	KNOX	70	46a
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	

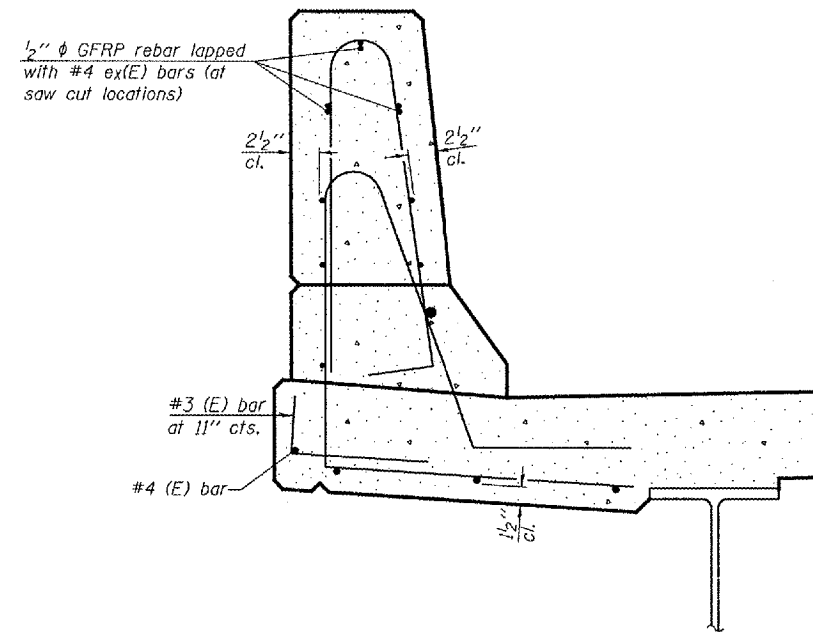
SHEET NO. 23
23 SHEETS

Contract #68190



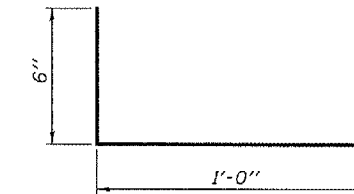
SECTION

(Showing Superstructure dimensions. Approach Pavement dimensions similar when applicable).

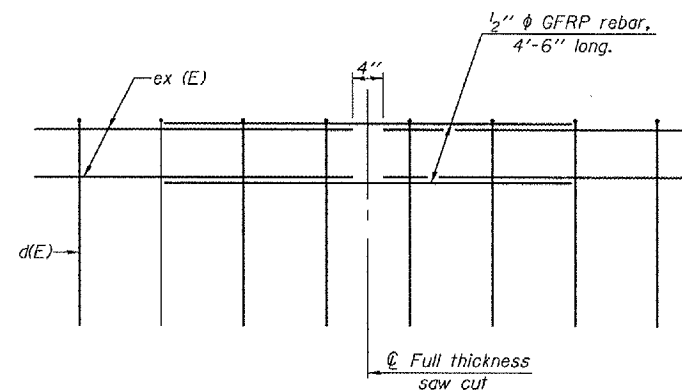


SECTION

(Showing reinforcement clearances for slip forming and additional reinforcement bars. Superstructure shown. Approach Pavement similar).



#3 (E) BAR



GFRP REBAR STIFFENING DETAIL

(Place as shown in parapet section at each parapet joint location.)

GENERAL NOTES

All dimensions shall remain the same as shown on contract plans, except dimensions A and B which are to be revised as shown to provide additional clearance. Additional concrete needed to revise dimension A and B = 0.0165 cu. yds./ft. of parapet. Place aluminum sheet in curb portion at and near piers. Full thickness saw cut at all joint locations in lieu of cork joint filler.

DESIGNED	Stephen M. Ryan
CHECKED	Sheila E. Moynihan
DRAWN	R. Sommer
CHECKED	SMR/SEM

EXAMINED	Thomas J. Demagallibi	Nov. 20 2007
PASSED	Ralph E. Anderson	

SFP-34

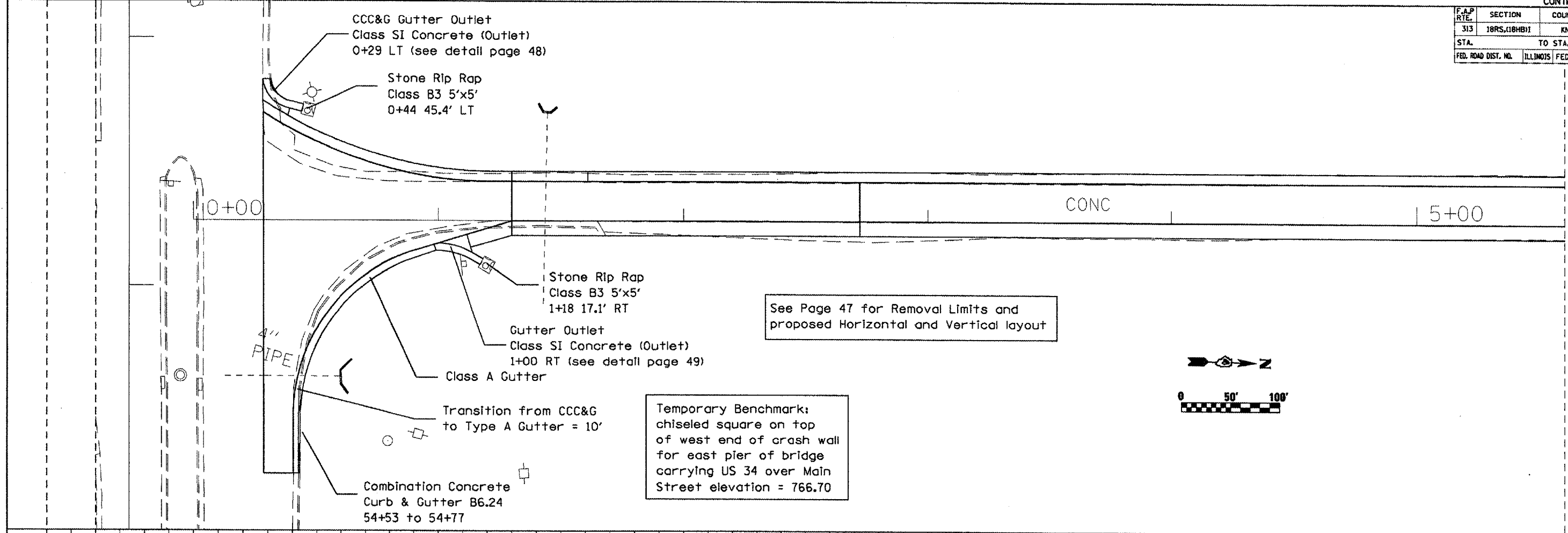
**CONCRETE PARAPET
SLIPFORMING OPTION**
F.A.P. RT. 313 SECTION 18-HB
KNOX COUNTY
STATION 20+11.14
STRUCTURE NO. 048-0069

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18RS, (10)HB1	KNOX	70	46
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

PLAN	DATE
BY	
CHECKED	
DATE	
NO.	
FILE NAME	

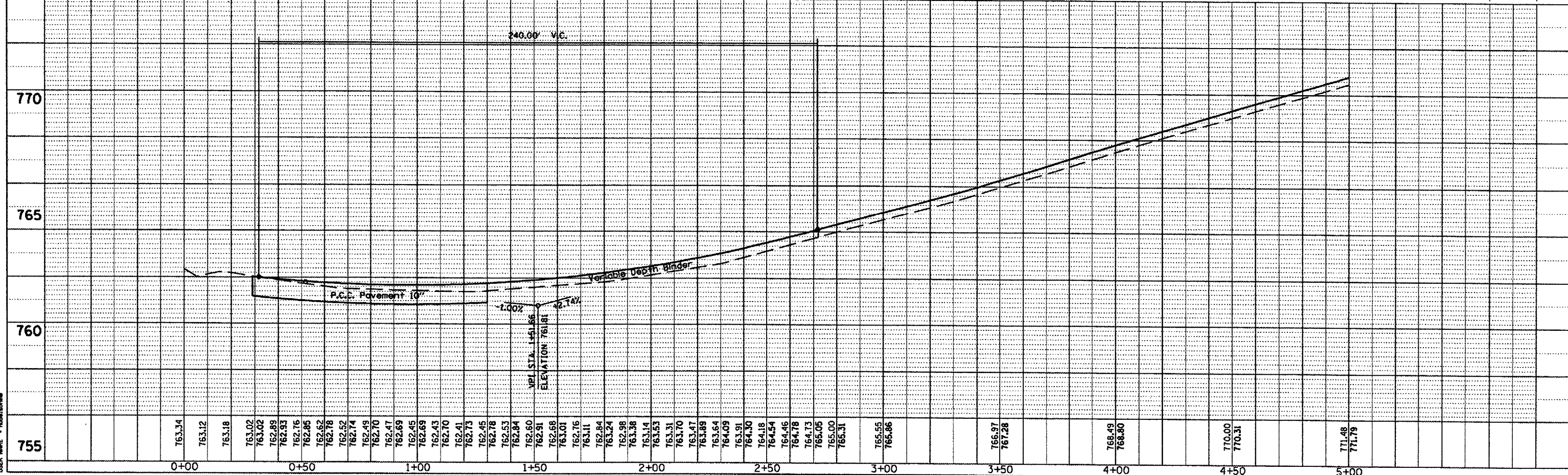
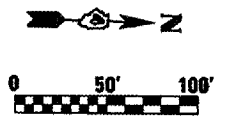
PROFILE	DATE
BY	
CHECKED	
DATE	
NO.	
FILE NAME	

PLOT DATE = 05/18/2007
 PLOT SCALE = 42.8673" / IN.
 USER NAME = hndalac@me





See Page 47 for Removal Limits and proposed Horizontal and Vertical layout

Temporary Benchmark:
 chiseled square on top
 of west end of crash wall
 for east pier of bridge
 carrying US 34 over Main
 Street elevation = 766.70



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18RS,18HB11	KNOX	70	47
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

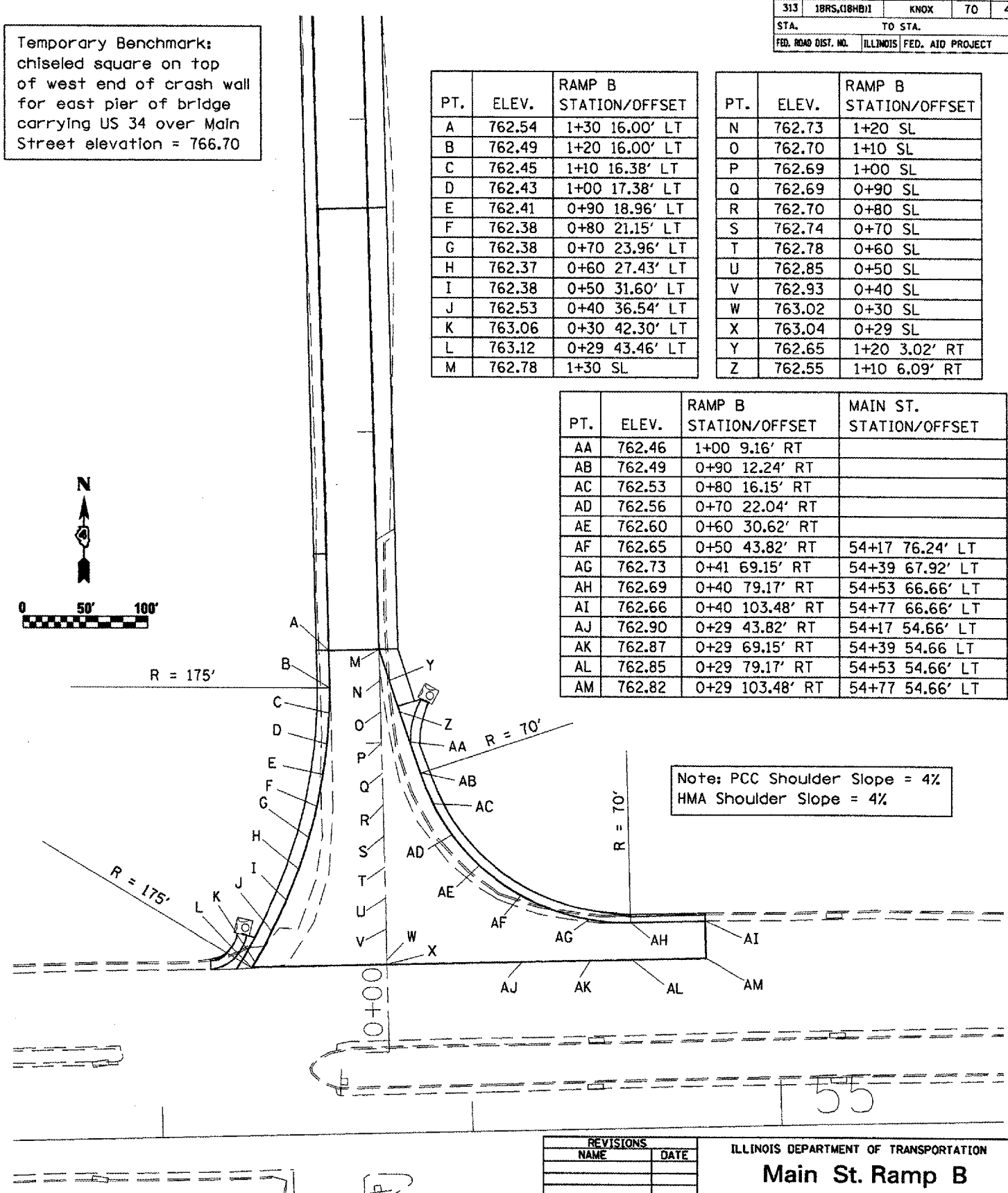
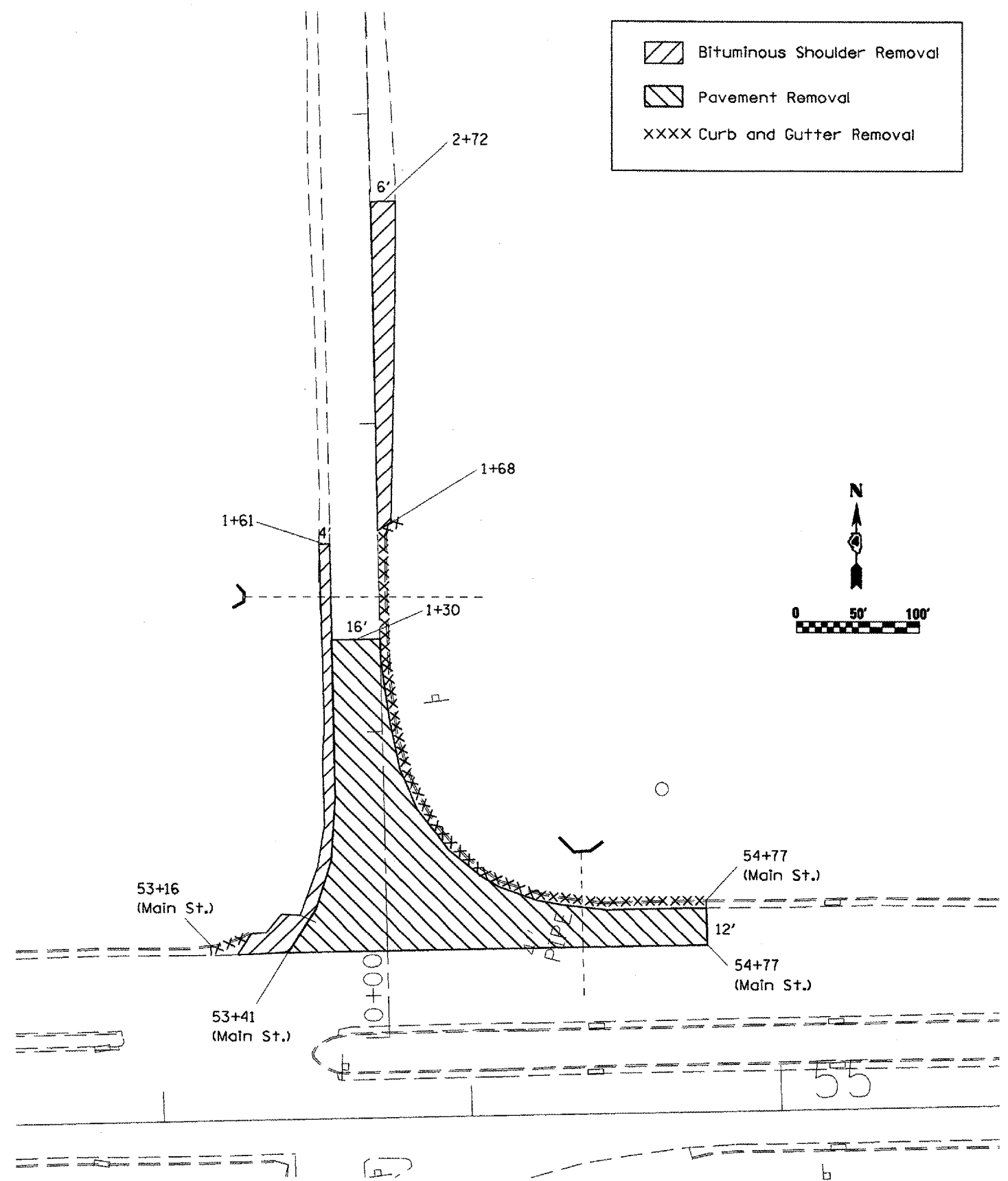
 Bituminous Shoulder Removal
 Pavement Removal
 XXXX Curb and Gutter Removal

Temporary Benchmark: chiseled square on top of west end of crash wall for east pier of bridge carrying US 34 over Main Street elevation = 766.70

PT.	ELEV.	RAMP B STATION/OFFSET
A	762.54	1+30 16.00' LT
B	762.49	1+20 16.00' LT
C	762.45	1+10 16.38' LT
D	762.43	1+00 17.38' LT
E	762.41	0+90 18.96' LT
F	762.38	0+80 21.15' LT
G	762.38	0+70 23.96' LT
H	762.37	0+60 27.43' LT
I	762.38	0+50 31.60' LT
J	762.53	0+40 36.54' LT
K	763.06	0+30 42.30' LT
L	763.12	0+29 43.46' LT
M	762.78	1+30 SL

PT.	ELEV.	RAMP B STATION/OFFSET
N	762.73	1+20 SL
O	762.70	1+10 SL
P	762.69	1+00 SL
Q	762.69	0+90 SL
R	762.70	0+80 SL
S	762.74	0+70 SL
T	762.78	0+60 SL
U	762.85	0+50 SL
V	762.93	0+40 SL
W	763.02	0+30 SL
X	763.04	0+29 SL
Y	762.65	1+20 3.02' RT
Z	762.55	1+10 6.09' RT

PT.	ELEV.	RAMP B STATION/OFFSET	MAIN ST. STATION/OFFSET
AA	762.46	1+00 9.16' RT	
AB	762.49	0+90 12.24' RT	
AC	762.53	0+80 16.15' RT	
AD	762.56	0+70 22.04' RT	
AE	762.60	0+60 30.62' RT	
AF	762.65	0+50 43.82' RT	54+17 76.24' LT
AG	762.73	0+41 69.15' RT	54+39 67.92' LT
AH	762.69	0+40 79.17' RT	54+53 66.66' LT
AI	762.66	0+40 103.48' RT	54+77 66.66' LT
AJ	762.90	0+29 43.82' RT	54+17 54.66' LT
AK	762.87	0+29 69.15' RT	54+39 54.66' LT
AL	762.85	0+29 79.17' RT	54+53 54.66' LT
AM	762.82	0+29 103.48' RT	54+77 54.66' LT



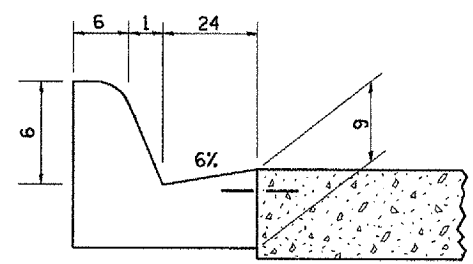
Note: PCC Shoulder Slope = 4%
HMA Shoulder Slope = 4%

REVISIONS	
NAME	DATE

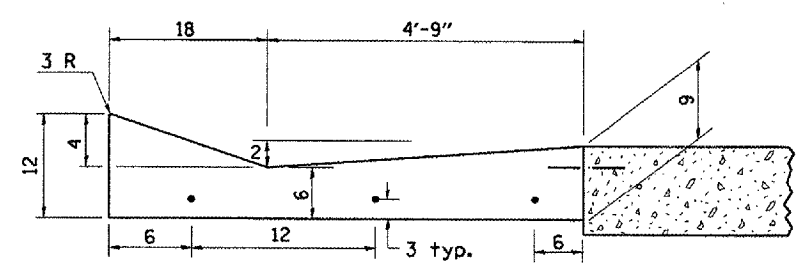
ILLINOIS DEPARTMENT OF TRANSPORTATION
**Main St. Ramp B
 Removal Limits &
 Horiz./Vert. Layout**
 SCALE: VERT. _____
 HORIZ. _____
 DATE _____ DRAWN BY _____
 CHECKED BY _____

PLOT DATE = 10/18/2007
 PLOT SCALE = 1" = 40'
 USER NAME = hnd313rme

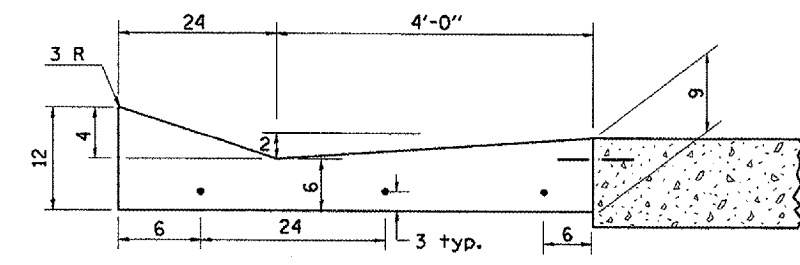
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18RS,18HBJ1	KNOX	70	48
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



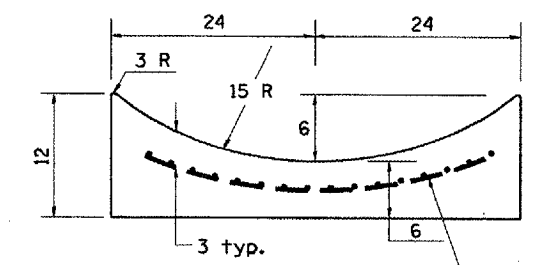
SECTION A-A



SECTION B-B

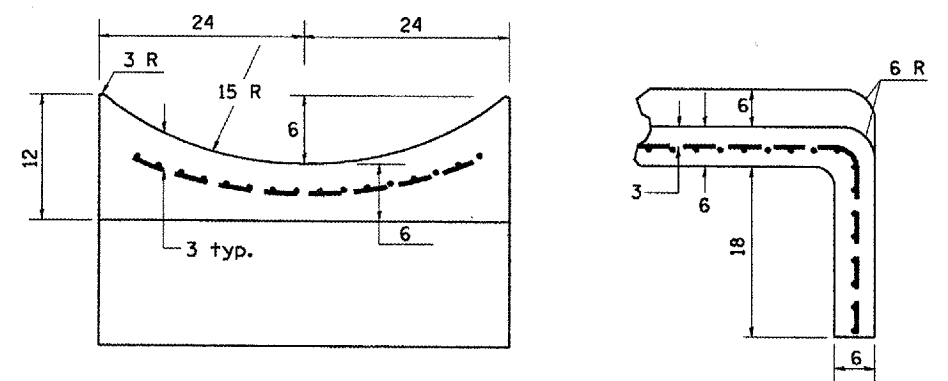
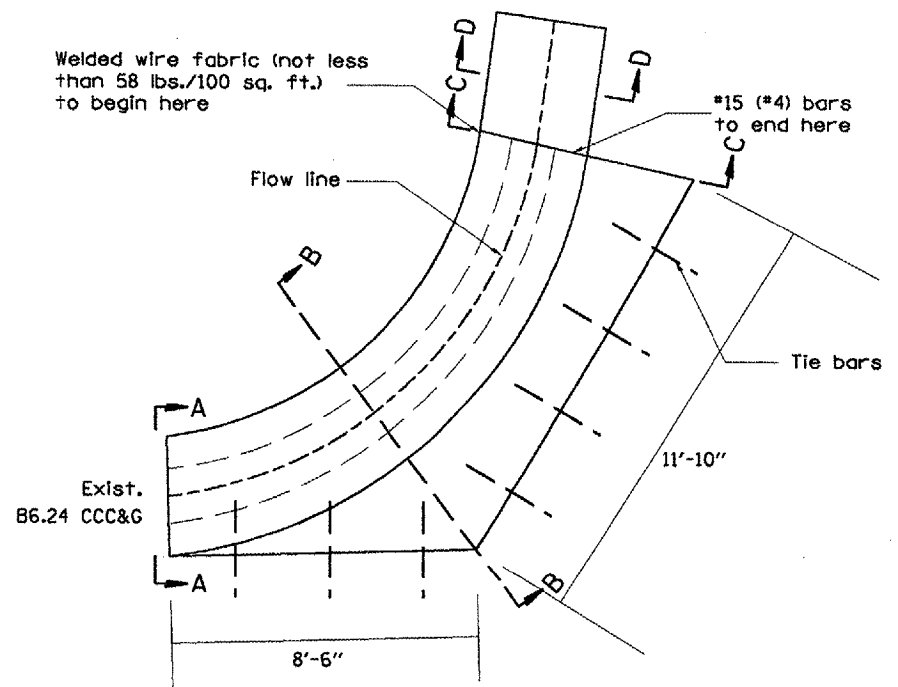


SECTION C-C



SECTION D-D

Welded wire fabric (typ.)



SECTIONS AT END OF OUTLET (CURTAIN WALL)

All dimensions are in inches unless otherwise noted.

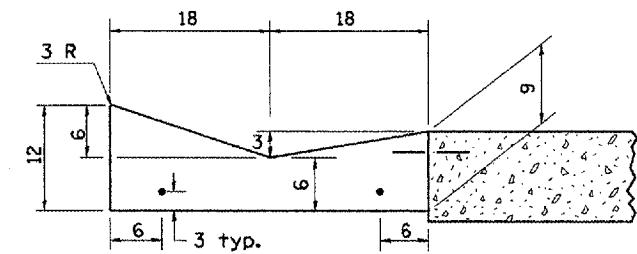
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
CCC&G Outlet Detail
Ramp B 0+29 LT
 (not to scale)

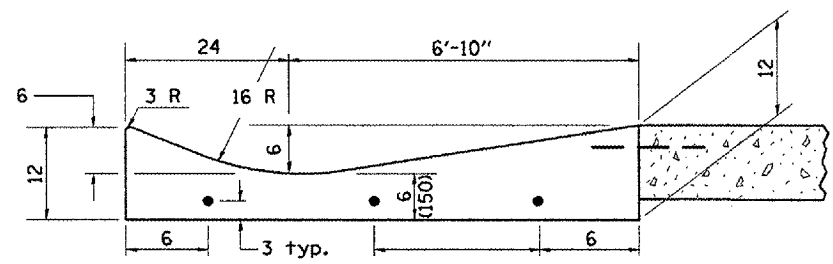
SCALE: VERT. _____
 HORIZ. _____
 DATE _____ DRAWN BY _____
 CHECKED BY _____

PLOT DATE = 10/18/2007
 FILE NAME = c:\pca\pca\work\68190\general\2.dgn
 PLOT SCALE = 1/8"=1'-0"
 USER NAME = hush123456

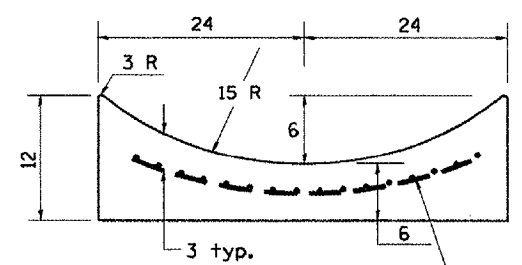
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18RS,18HB1	KNOX	70	49
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



SECTION A-A

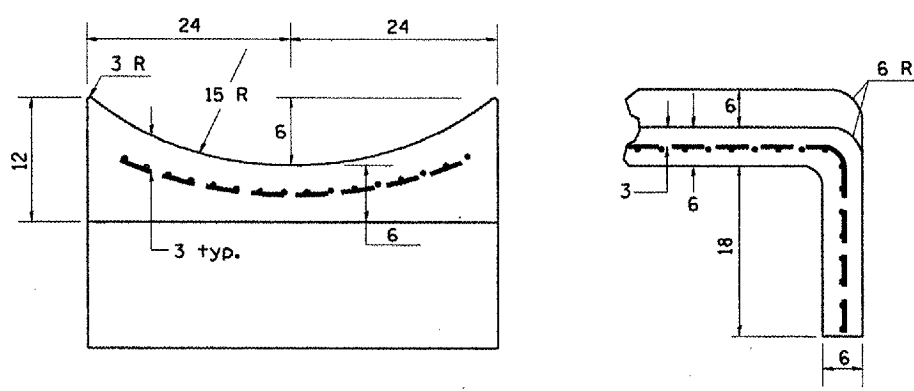
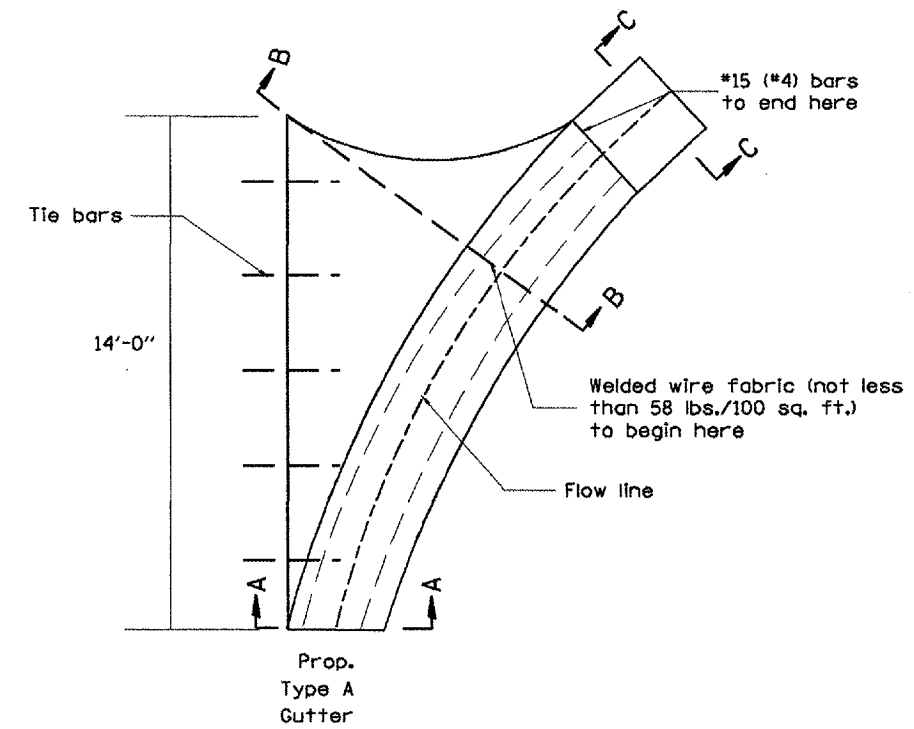


SECTION B-B



SECTION C-C

Welded wire fabric (typ.)



SECTIONS AT END OF OUTLET
(CURTAIN WALL)

All dimensions are in inches unless otherwise noted.

REVISIONS	
NAME	DATE

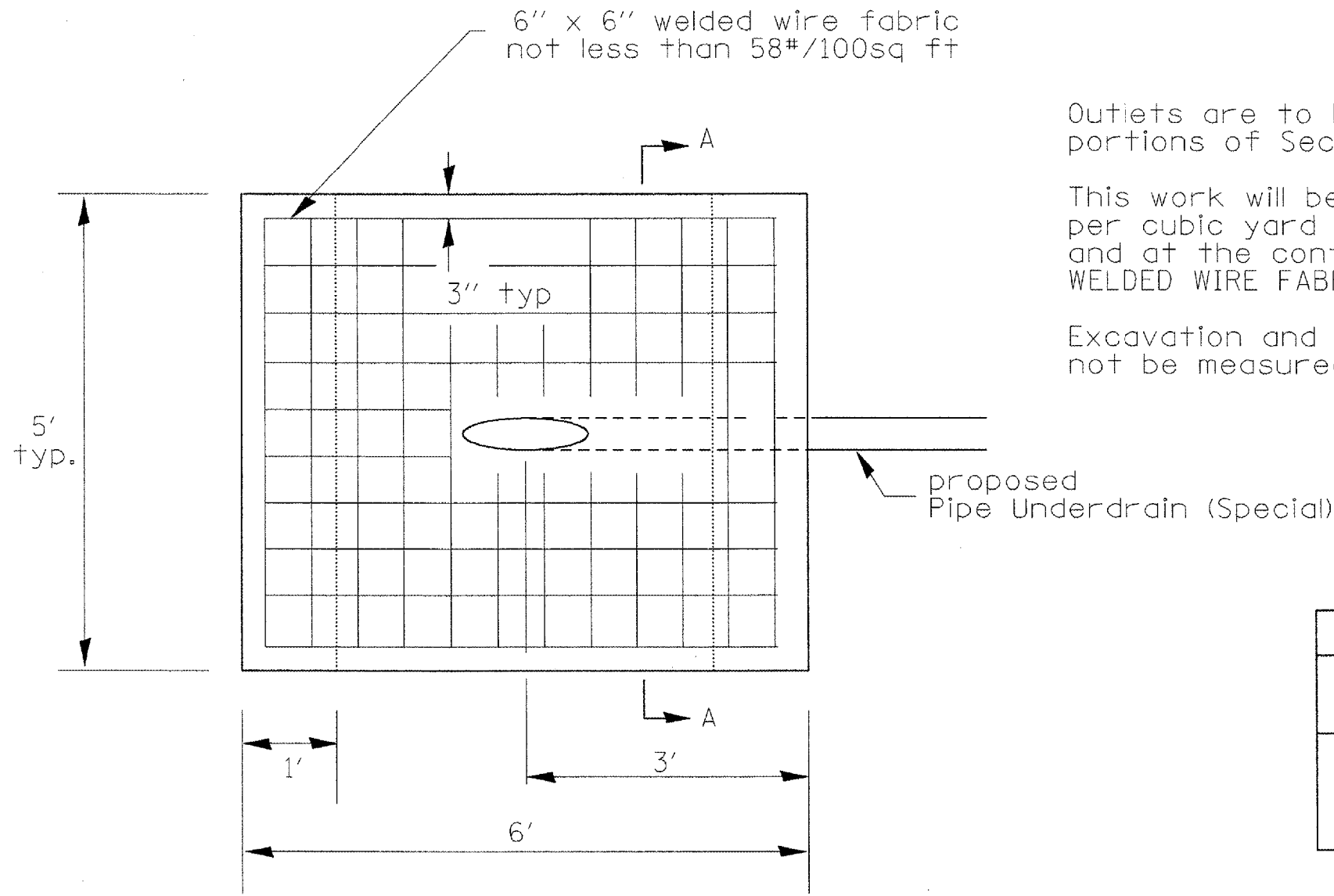
ILLINOIS DEPARTMENT OF TRANSPORTATION
Gutter Outlet Detail
Ramp B 1+00 RT
(not to scale)

SCALE: VERT. HORIZ.
 DATE

DRAWN BY
 CHECKED BY

PLOT DATE = 18/08/2007
 FILE NAME = c:\puro\puro\313\general\general12.dgn
 PLOT SCALE = 1/8" = 1'-0" / IN.
 USER NAME = hndkstone

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18RS,(18HBI)	KNOX	70	51
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

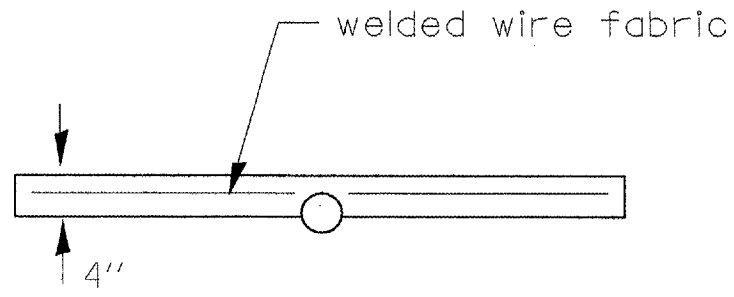
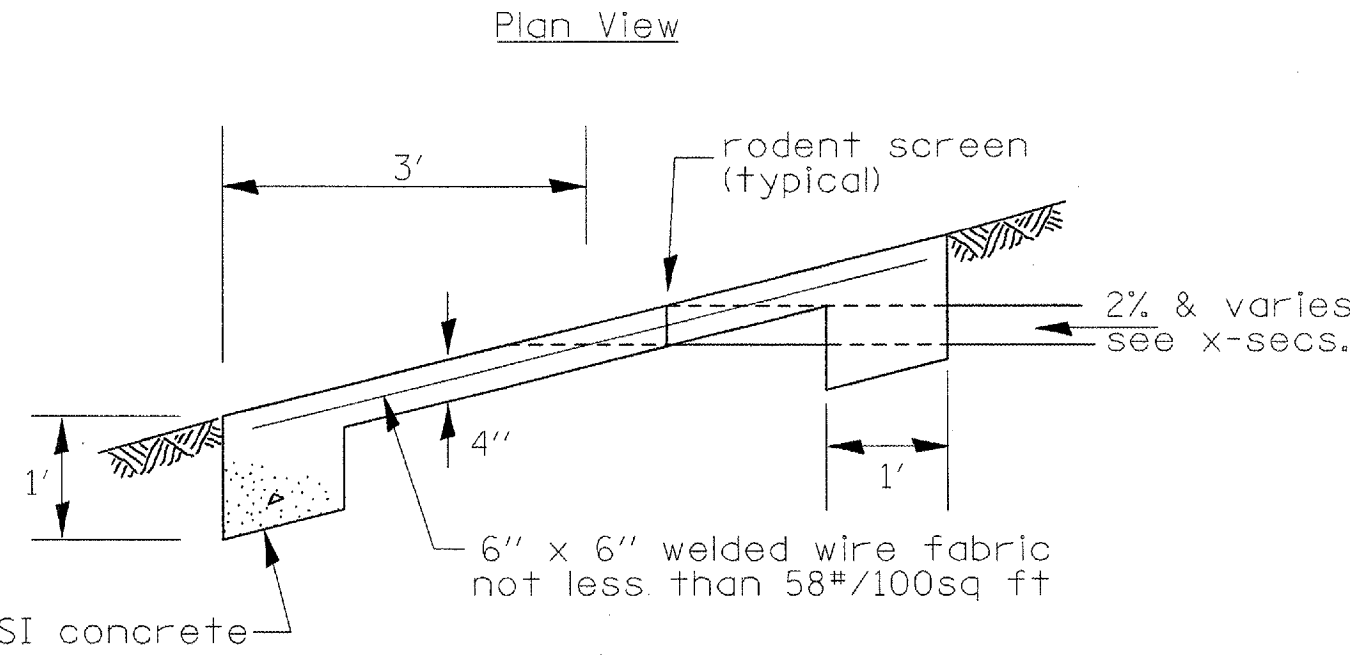


Outlets are to be constructed in accordance with applicable portions of Section 606 of the Standard Specifications.

This work will be measured for payment at the contract unit price per cubic yard for CLASS SI CONCRETE OUTLET PROTECTOR and at the contract unit price per square yard for WELDED WIRE FABRIC 6 X 6.

Excavation and backfill required for this work will not be measured for payment.

Type	Class SI Cu Yd	WWF Sq Yd
I	.61	2.75
II	.86	3.75
III	1.35	6.75



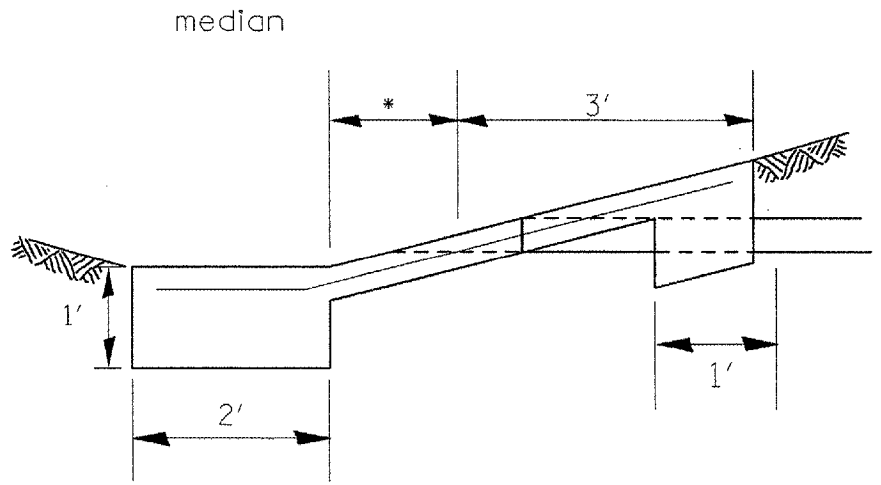
section A-A

REVISIONS	
NAME	DATE

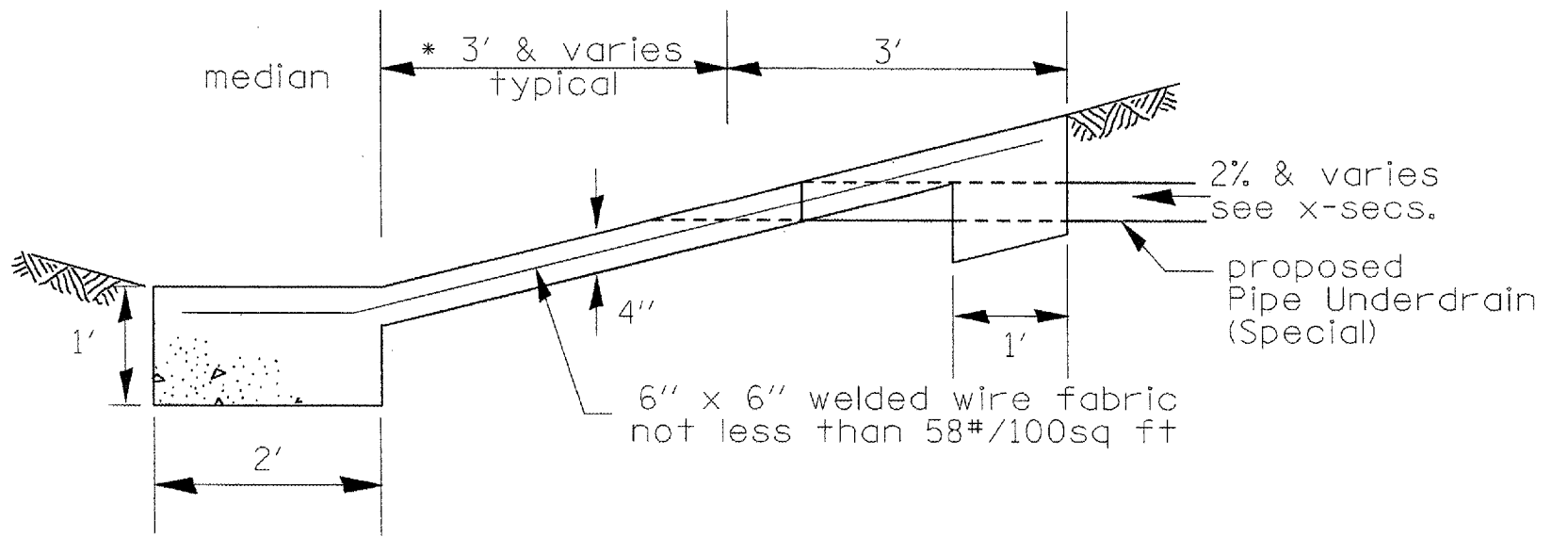
ILLINOIS DEPARTMENT OF TRANSPORTATION
Outlet Protector Type I
 SCALE: not drawn to scale
 DATE: _____
 DRAWN BY: dml 8/07
 CHECKED BY: _____

PLOT DATE: 10/18/2007
 FILE NAME: S:\GENERAL\PROJECTS\18RS\18RS.dwg
 PLOT SCALE: 1/8" = 1'-0"
 USER: NAME

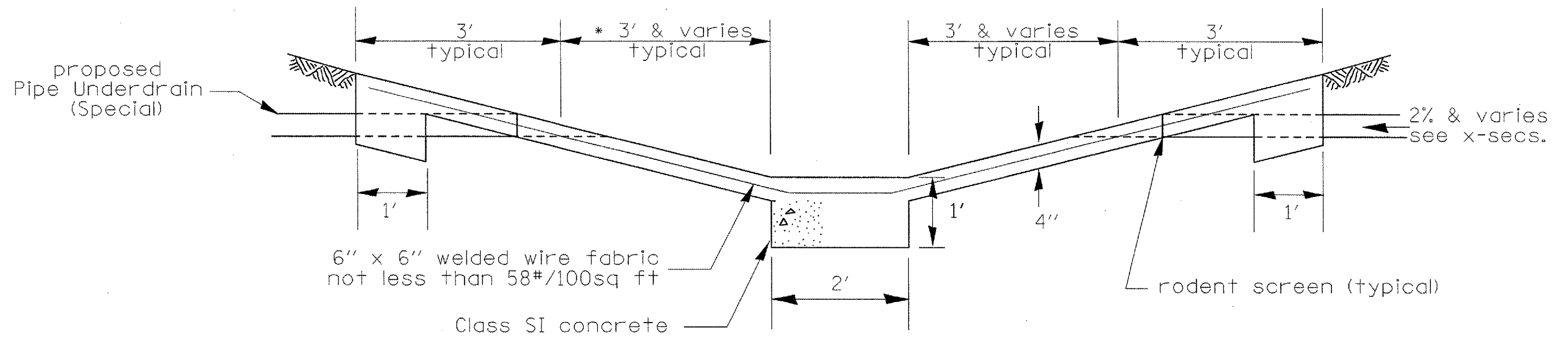
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18RS,18HB11	Knox	70	52
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			



* Varies at locations where the outlet is less than 3 feet from the ditch flow line.



Type II



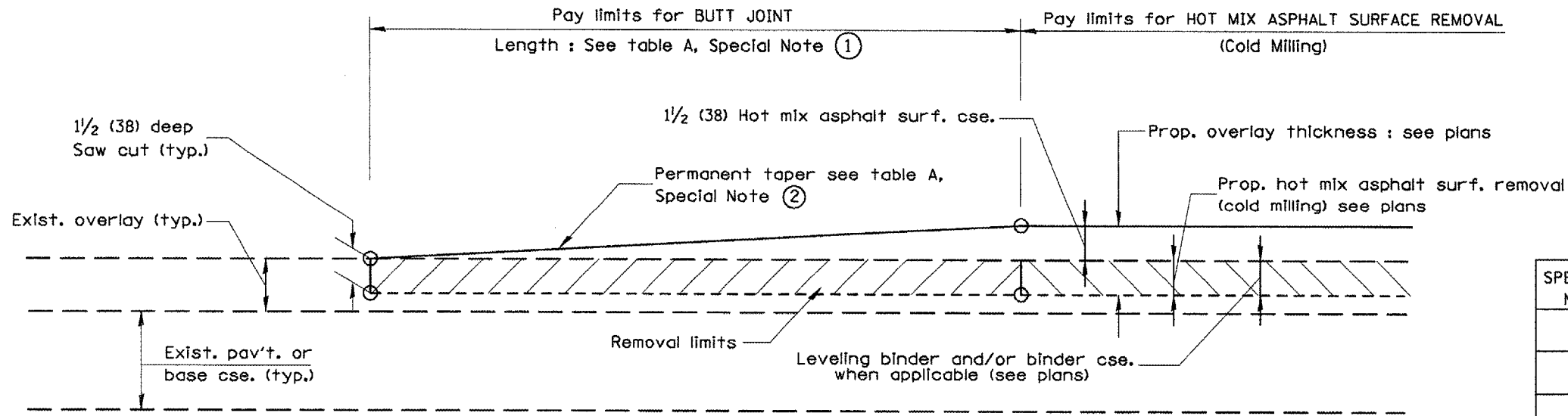
Type III

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
Outlet Protector
Type II & III
 SCALE: not drawn to scale
 DATE:
 DRAWN BY: dml 8/07
 CHECKED BY:

PLOT DATE = 10/12/2007
 PLOT SCALE = 1/8" = 1'-0"
 USER NAME = bruceba

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18 RS, (18HB)	KNOX	70	53
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



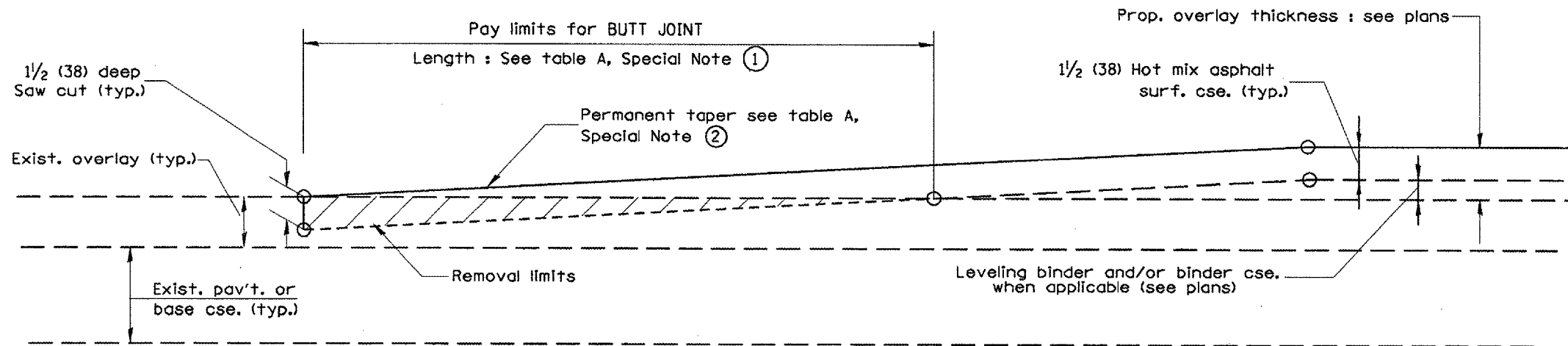
CASE 1 : WITH HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

**TABLE A
(LENGTHS AND TAPER RATES)**

SPECIAL NOTE NUMBER	ELEMENT	MAINLINE INTERSTATES & 4-LANE EXPRESSWAYS	ALL OTHERS
①	LENGTH OF BUTT JOINT	60'(18.0 m)	30'(9.0 m)
②	PERMANENT TAPER RATE	1:480	1:240
③	TEMPORARY RAMP TAPER RATE	1:80	1:40
④	TEMPORARY RAMP LENGTH	10'(3.0 m)	5'(1.5 m)
⑤	LENGTH OF BUTT JOINT	10'(3.0 m)	10'(3.0 m)

GENERAL NOTES

1. The work shall be done in accordance with Article 406.08 and the Special Provision for Butt Joints.
2. The pavement surface to be removed may be either bituminous or P.C. concrete. The work shall be performed in accordance with Article 440.04 and the Special Provisions for Butt Joints.
3. The saw cut joints shall be primed just prior to the placing of bituminous material. The work will be in accordance with the applicable portions of Article 406.05.



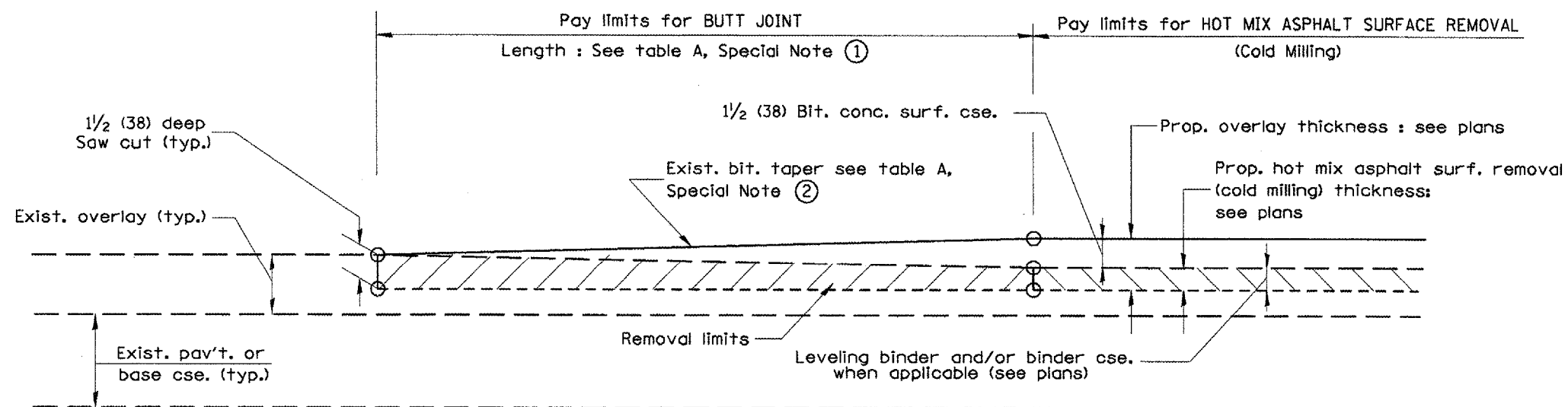
CASE 2 : NO HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

All dimensions are in inches (millimeters) unless otherwise noted.

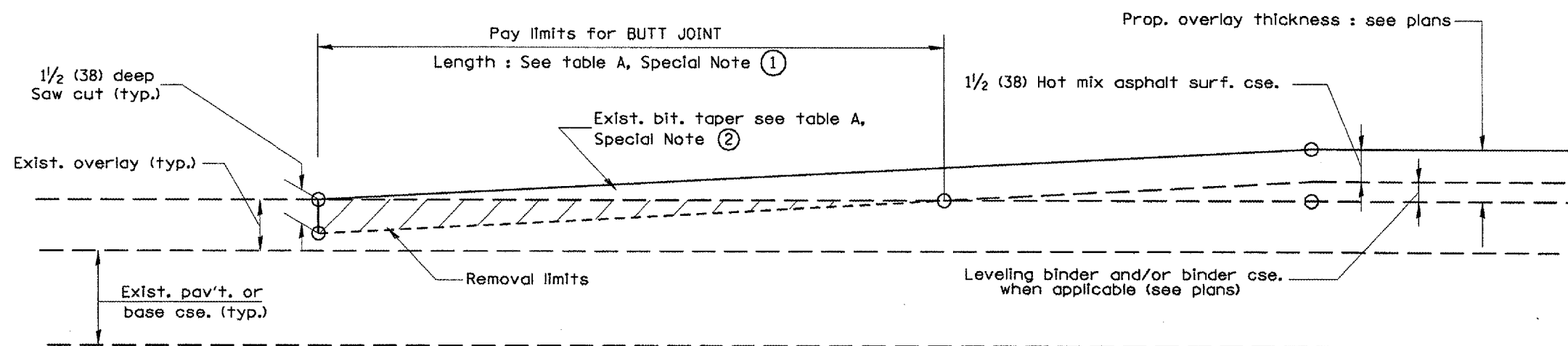
DATE	REVISIONS	BY
1-1-97	RENUM. C-23.01, NEW REVISION BOX	T.P.
4-1-97	CORRECTION TO DEPTH	J.A.
9-15-05	REVISED DESIGNER NOTE	M.M.A.
10-16-06	REVISED TO 2007 SPEC.	M.A.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT CADD STANDARD
BUTT JOINTS
 CADD STD NO. 406101-D4 SHEET 1 OF 3
 SCALE: NOT DRAWN TO SCALE DRAWN BY CADD
 DATE CHECKED BY

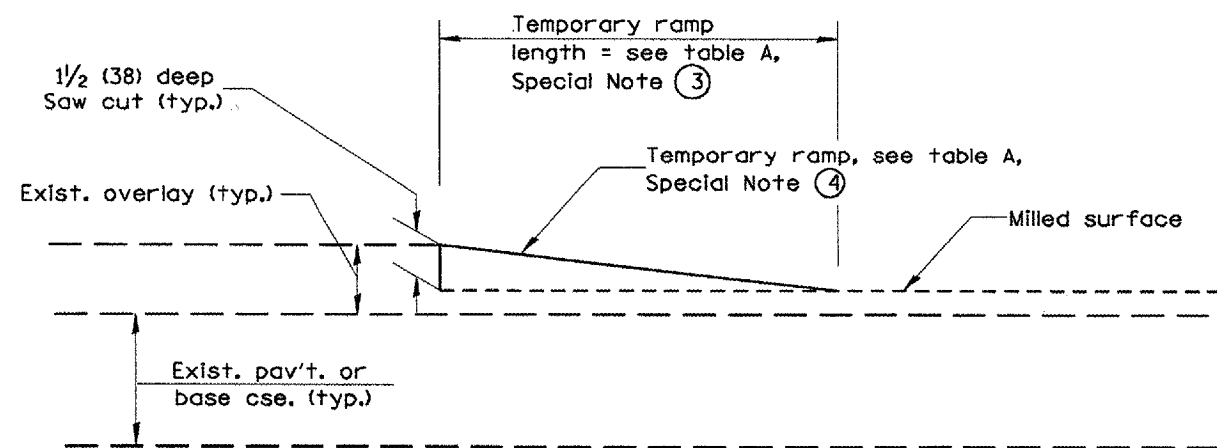
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18 RS. (18H80)	KNOX	70	54
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



**CASE 3 : WITH HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)
TIE-IN TO EXISTING BITUMINOUS TAPER**



**CASE 4 : NO HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)
TIE-IN TO EXISTING BITUMINOUS TAPER**



DETAIL TEMPORARY RAMP

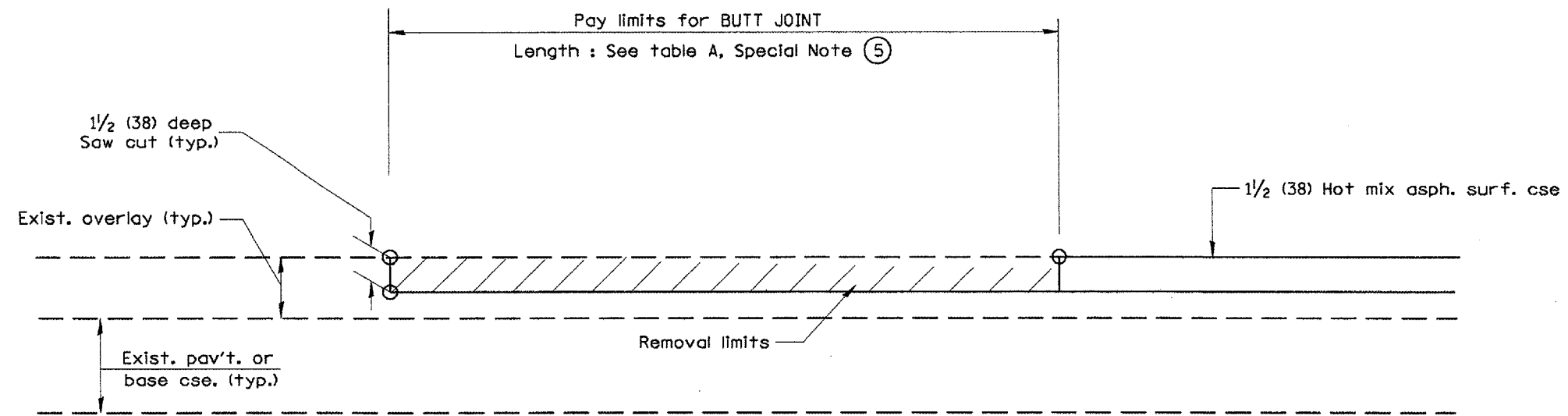
All dimensions are in inches (millimeters) unless otherwise noted.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT CADD STANDARD

BUTT JOINTS

CADD STD NO. 406101-D4 SHEET 2 OF 3
SCALE: NOT DRAWN TO SCALE DRAWN BY CADD
CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18 RS, (18HD)	KNOX	70	55
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



CASE 5 : WITH HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)
TIE-IN TO EXISTING BITUMINOUS TAPER

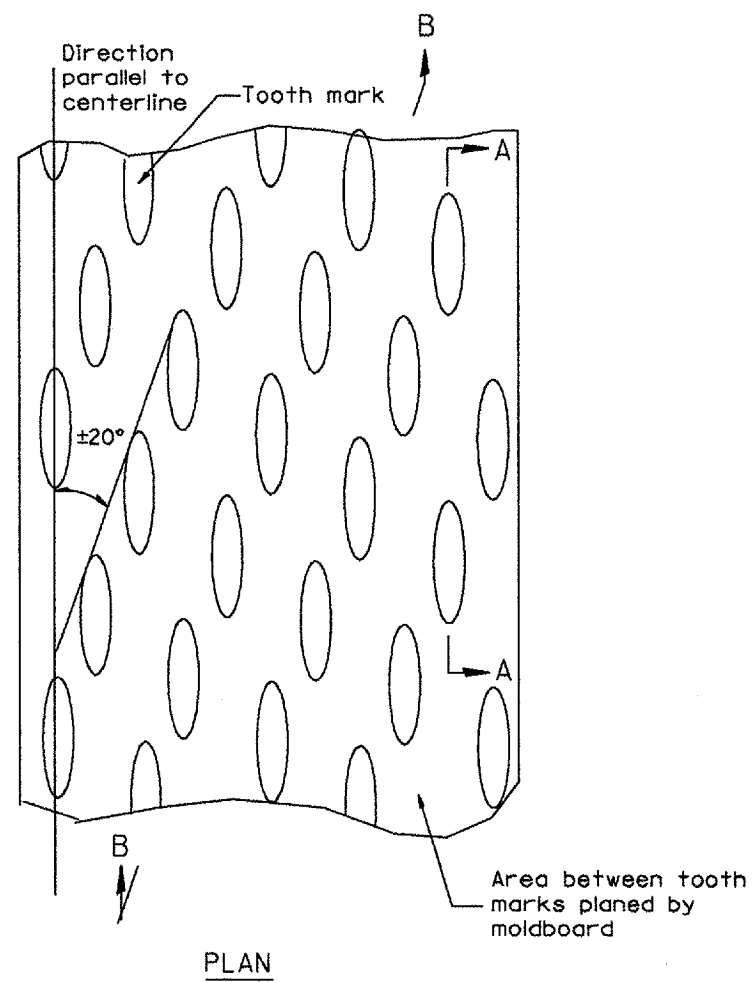
All dimensions are in inches (millimeters) unless otherwise noted.

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT CADD STANDARD

BUTT JOINTS

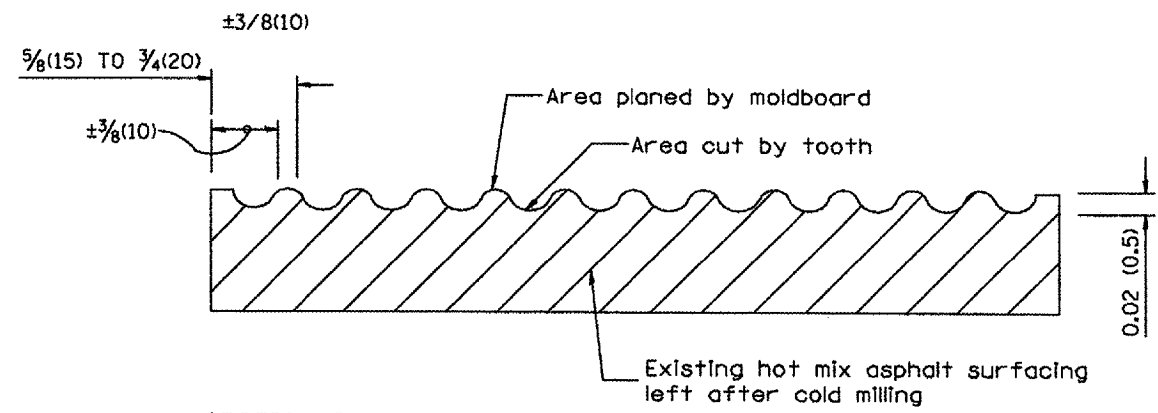
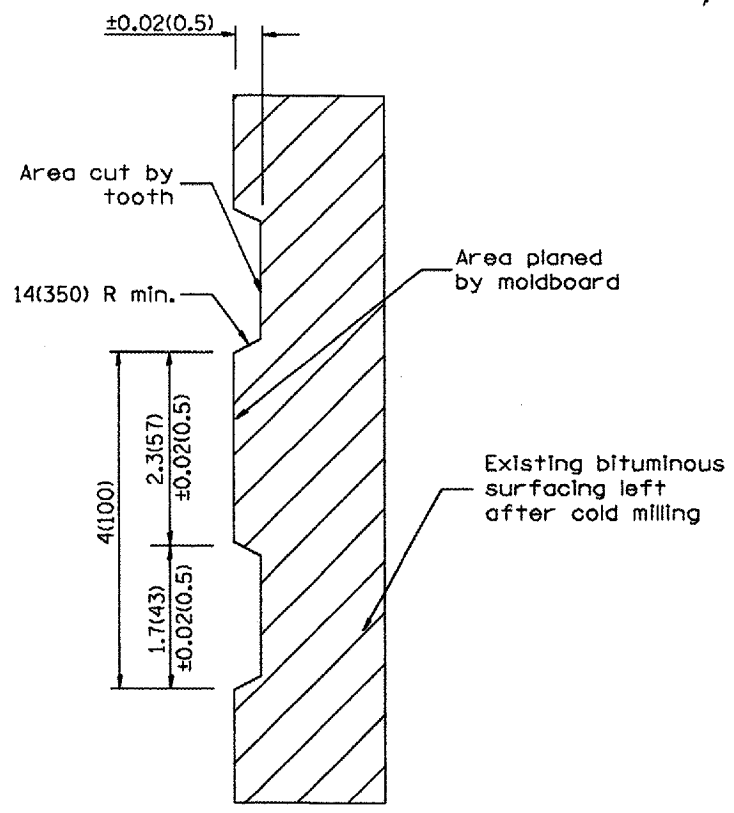
CADD STD NO. 406101-D4 SHEET 3 OF 3
 DRAWN BY CADD
 SCALE: NOT DRAWN TO SCALE
 CHECKED BY

F.A.P. RYE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18 RS. (18HB)	KNOX	70	56
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



General notes:

1. Coldmilling shall consist of two processes: Cutting with carbide teeth mounted on a rotating drum, and planing with a moldboard mounted immediately behind the cutting drum.
2. Other similar patterns will be acceptable if they consist of a smooth, flat, planed surface interspersed with a pattern of discontinuous longitudinal striations.



All dimensions are in inches (millimeters) unless otherwise noted.

DATE	REVISIONS	BY
1-1-97	RENUM. C-104.01. NEW REVISION BOX	T. P.
4-20-98	REMOVED MILLING DETAIL FROM STD.	J. A.
9-08-98	CORRECT NOTE LEADER PLACEMENT	R. W.
10-16-06	REVISED TO 2007 SPEC.	M.A.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT CADD STANDARD

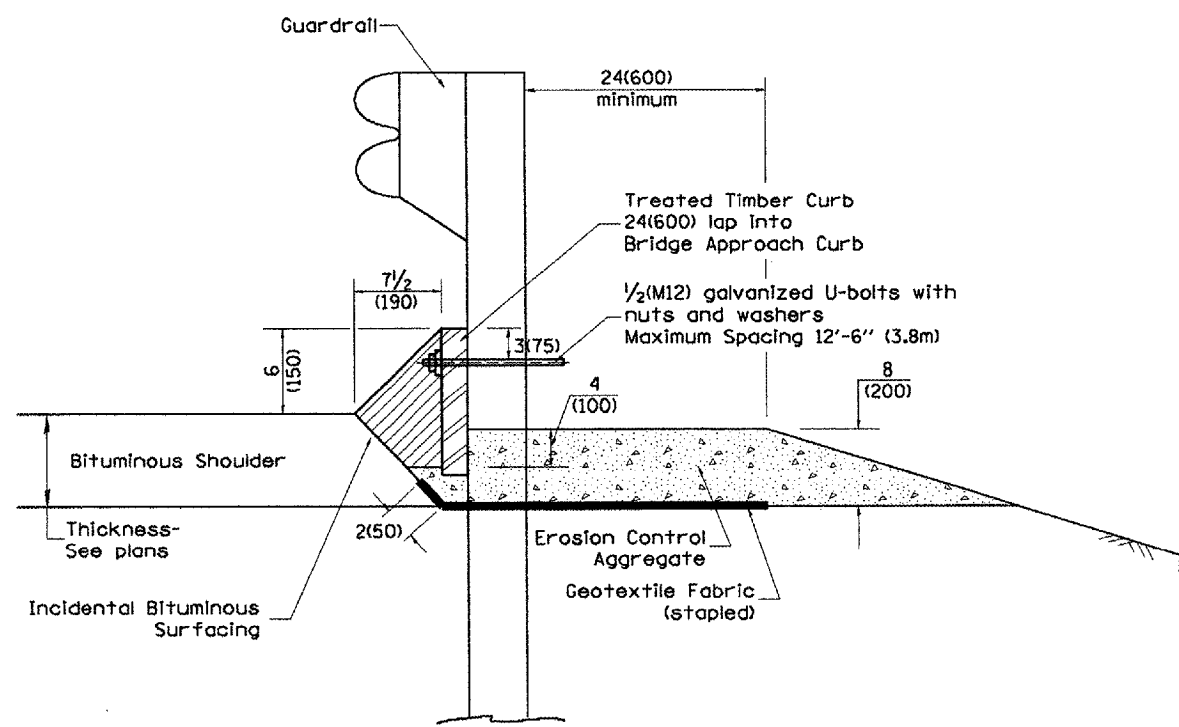
**HOT MIX ASPHALT
SURFACE REMOVAL
(COLD MILLING)**

CADD STD NO. 440001-D4

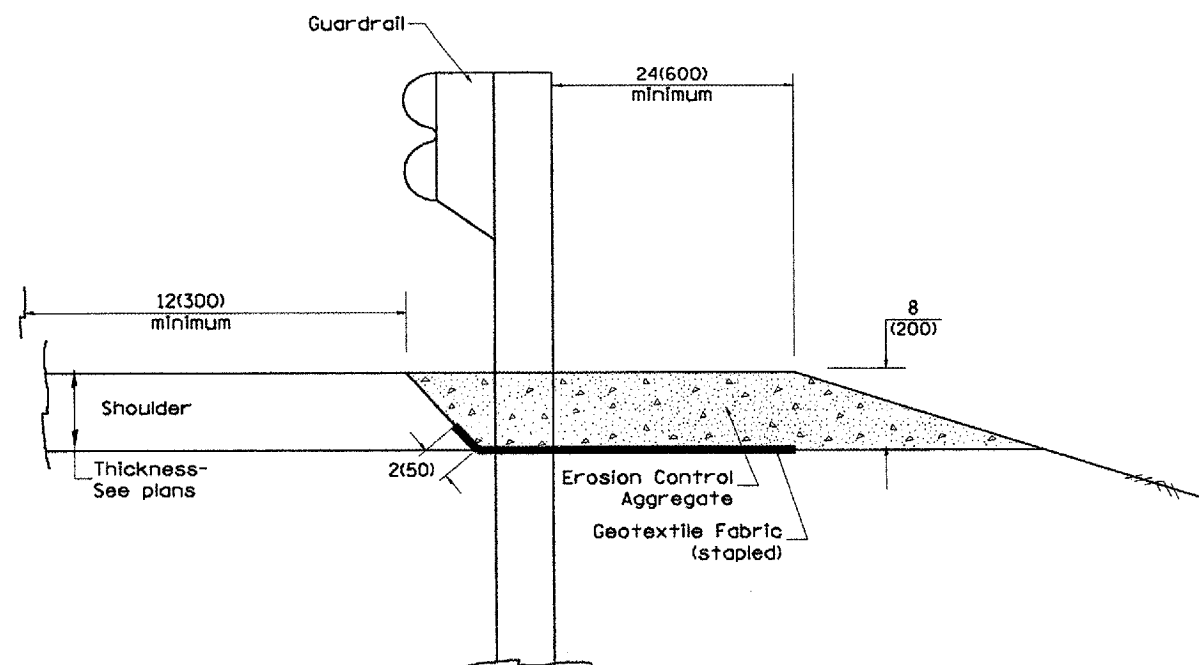
SCALE: NOT DRAWN TO SCALE
DATE

DRAWN BY CADD
CHECKED BY

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	1B RS, (18HB)	KNOX	70	57
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



TYPICAL SECTION WITH EROSION CONTROL CURB



TYPICAL SECTION WITHOUT EROSION CONTROL CURB

GENERAL NOTES: EROSION CONTROL CURB

1. This work shall consist of grading as needed, installing hardware and treated timber boards, furnishing and placing mastic material and incidental bituminous surfacing in front of Steel Plate Beam Guardrail in accordance with Plan Details.
2. Timber shall be treated in accordance with Article 1007.12. All preservatives specified in the article will be allowed. Waterborne preservatives "asa" and "cca" shall have a minimum retention of 0.40 lbs./cu. ft. (6.4 kg/m³)

GENERAL NOTES: GUARDRAIL AGGREGATE EROSION CONTROL

1. This work shall consist of grading as needed, furnishing and installing geotextile fabric and staples, and furnishing, placing and shaping crushed aggregate around and behind Steel Plate Beam Guardrail posts in accordance with Plan Details.
2. Before placing the aggregate and the Geotextile Fabric, weeds and grass shall be removed from the area to be covered.
3. After the area has been prepared, and in a dry condition, the Geotextile fabric shall be placed with a 12(300) minimum overlap. A knife cut for guardrail post installation is necessary.
4. The aggregate shall be deposited, compacted and shaped by either mechanical or hand methods, in a manner reasonably true to line and grade.
5. The Contractor shall have the option of placing the guardrail before or after the Geotextile Fabric and Aggregate are in place. If the guardrail is placed after the Geotextile Fabric and Aggregate, then any voids must be filled and the aggregate returned to line and grade.
6. Materials shall meet the following requirements:
 - A. The crushed aggregate shall be CA1 gradation in accordance with Article 1004.01(c) of the Standard Specifications.
 - B. The Geotextile Fabric shall be nonwoven fabric in accordance with Article 1080.02 of the Standard Specifications.

All dimensions are in inches (millimeters) unless otherwise noted.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT CADD STANDARD

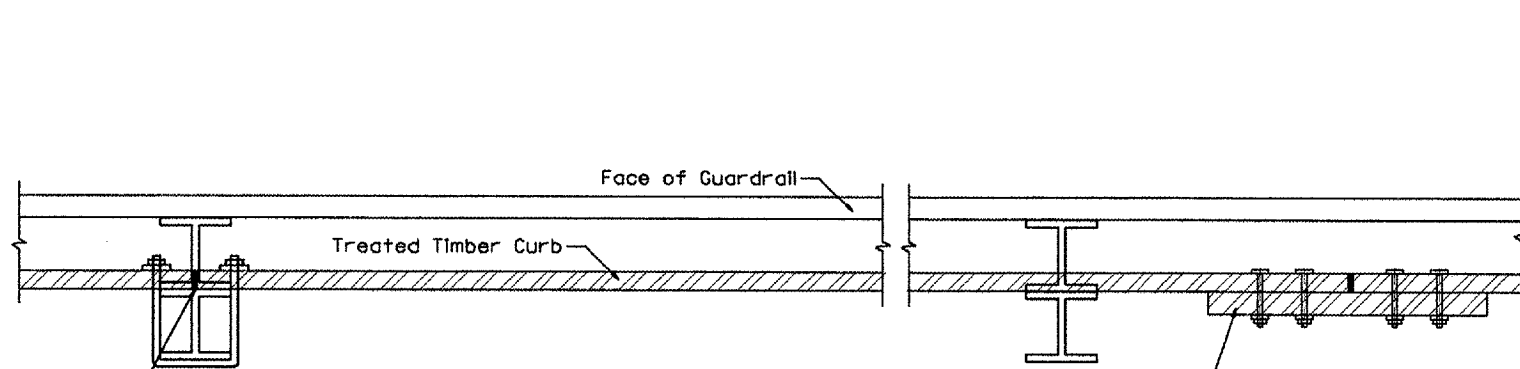
GUARDRAIL EROSION
CONTROL TREATMENTS

DATE	REVISIONS	BY
1-1-97	RENUM. C-22.01, NEW REVISION BOX	T.P.
3-1-97	CORRECT STD. NUMBERS IN NOTES PG. 2	J.A.
11-3-00	CORRECTION TO NOTES	M.A.
10-16-06	REVISED TO 2007 SPEC.	M.A.

CADD STD NO. 630101-D4(1)
SCALE: NOT DRAWN TO SCALE

SHEET 1 OF 2
DRAWN BY CADD
CHECKED BY

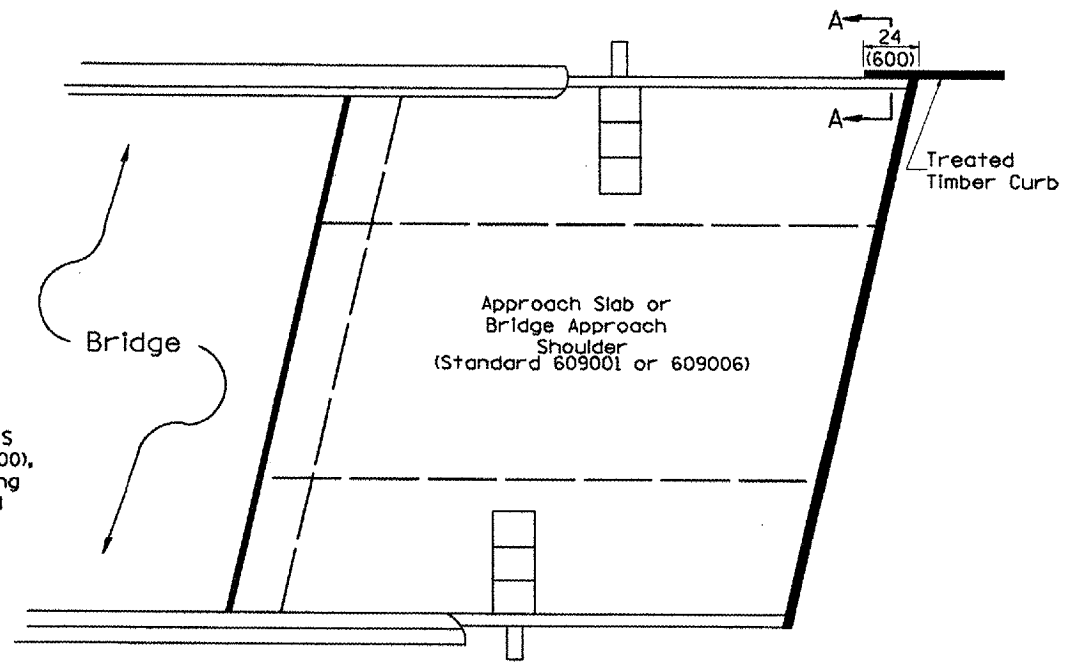
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18 RS. (18HB)	KNOX	70	58
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



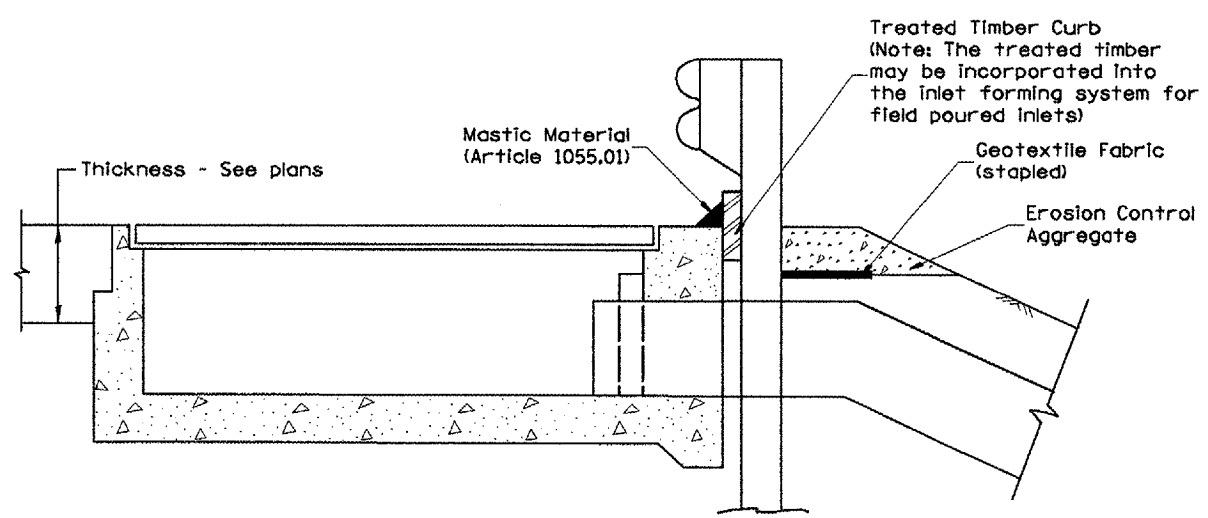
SPLICE LOCATED AT GUARDRAIL POST
1/2(M12) galvanized U-bolt with
nut & washer

SPLICE LOCATED BETWEEN GUARDRAIL POSTS
treated timber splice plate 2x12 (50x300),
actual size 1 1/2x1 1/2 (40x290), 24(600) long
with 8 evenly spaced 1/2(M12) galvanized
bolts with nuts & washers.

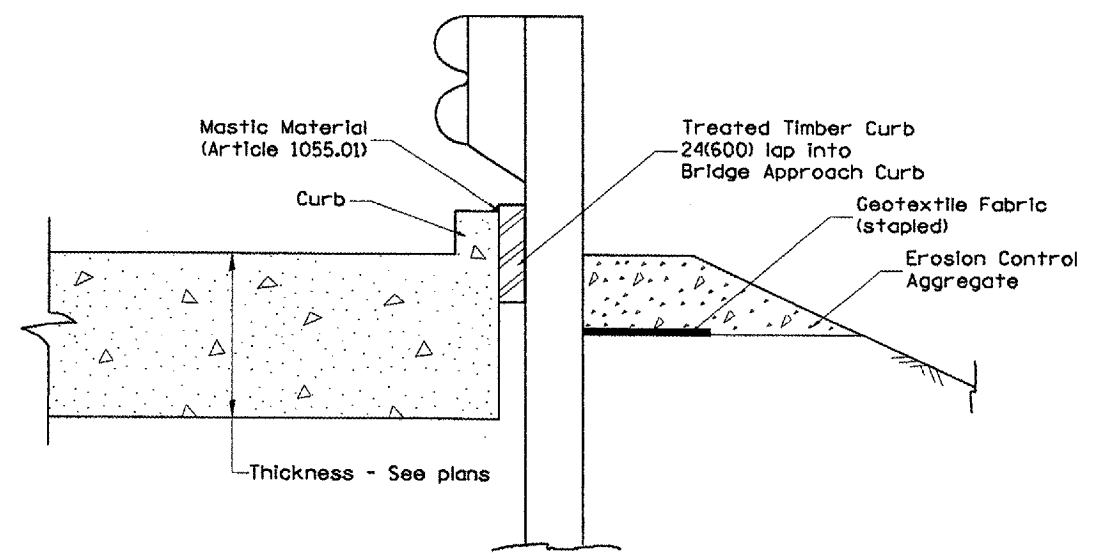
DETAIL A
(Typical Treated Timber Splices)



PLAN VIEW
APPROACH SLAB OR BRIDGE APPROACH SHOULDER
(STANDARD 609001 or 609006)



TYPICAL SECTION WITH EROSION CONTROL CURB
AT INLETS TYPE E & F (STANDARD 610001)



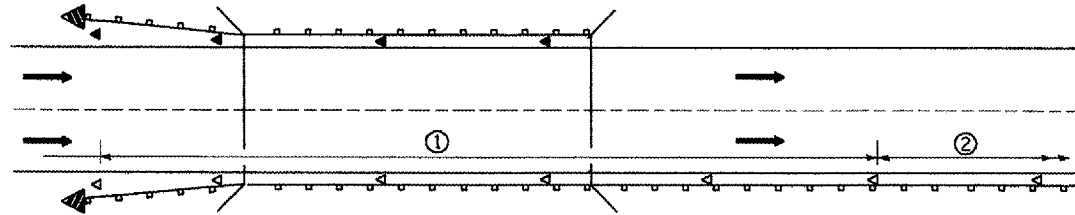
SECTION A-A
TYPICAL SECTION WITH EROSION CONTROL CURB
AT BRIDGE APPROACH CURB
(STANDARD 609001 OR 609006)

All dimensions are in inches (millimeters) unless otherwise noted.

ILLINOIS DEPARTMENT OF TRANSPORTATION	
DISTRICT CADD STANDARD	
GUARDRAIL EROSION CONTROL TREATMENTS	
CADD STD NO. 630101-D4(2)	SHEET 2 OF 2
SCALE: NOT DRAWN TO SCALE	DRAWN BY CADD
	CHECKED BY

\$\$\$DATE\$\$\$

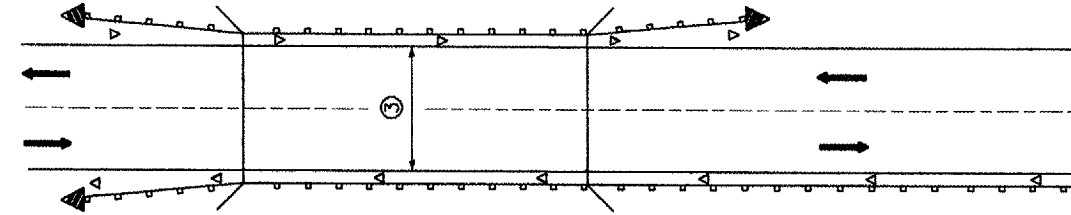
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18 RS. (18HB1)	KNOX	70	59
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



① Spacing 80 ft. (24 m) max. for first 400 ft. (122 m) or curve spacing shown in Standard 635001, whichever is less (min. 4 reflectors regardless of length).

② After 400 ft. (122 m), transition to normal delineator spacing shown in Standard 635001, and continue as required.

ONE-WAY TRAFFIC



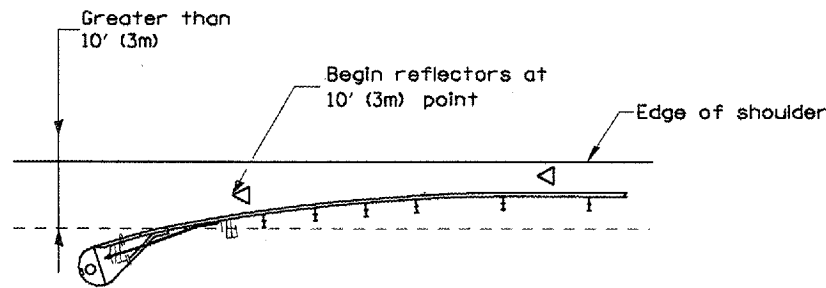
③ Bidirectional silver/silver should be used in lieu of monodirectional silver on both sides of two-lane bridges where the bridge pavement is less than 24 (610) wider than the pavement approaching the bridge.

TWO-WAY TRAFFIC

GUARDRAIL / BARRIER WALL / BRIDGE RAIL REFLECTORS

LEGEND

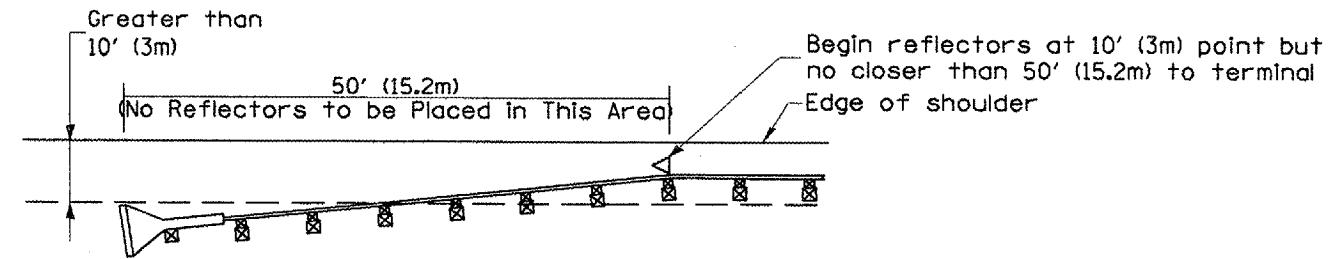
- ◁ Monodirectional silver
- ◄ Monodirectional amber
- ◄ Terminal Marker - Black/Yellow
Left or Right as appropriate



NOTE: Omit terminal marker when terminal over 10' (3m) from edge of paved shoulder or break point of unpaved shoulder, or when terminal buried in backslope.

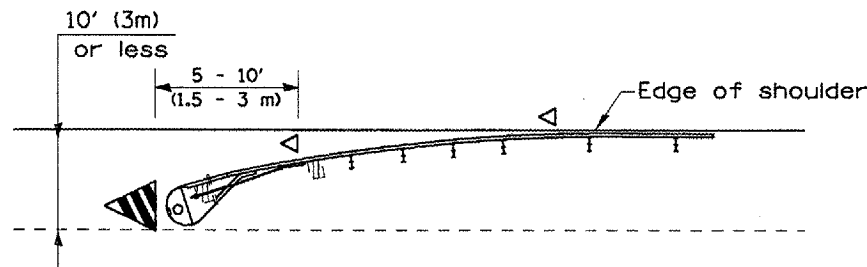
Traffic Barrier Terminal Type(*) and/or Turned-Down Terminal

[Terminal over 10' (3m) from edge of shoulder]
*See Plans for Type

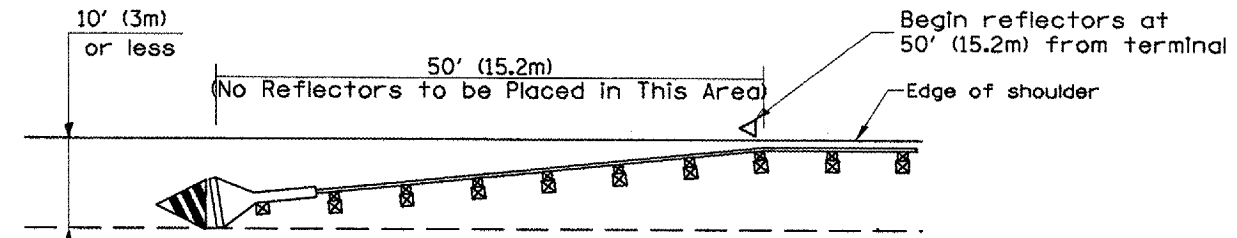


NOTE: Omit terminal marker when terminal over (10') from edge of paved shoulder or break point of unpaved shoulder.

Traffic Barrier Terminal Type 1 (Special)
[Terminal over 10' (3m) from edge of shoulder]



Traffic Barrier Terminal Type(*) and/or Turned-Down Terminal
[Terminal over 10' (3m) or less from edge of shoulder]
*See Plans for Type



Traffic Barrier Terminal Type 1 (Special)
[Terminal 10' (3m) or less from edge of shoulder]

All dimensions are in inches (millimeters) unless otherwise noted.

**ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT CADD STANDARD**

**GUARDRAIL AND
BARRIER WALL DELINEATION**

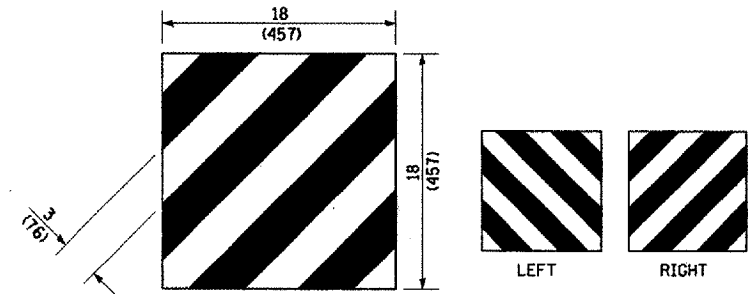
CADD STD. NO. 635101-04
SCALE: NOT DRAWN TO SCALE

SHEET 1 OF 3
DRAWN BY CADD
CHECKED BY

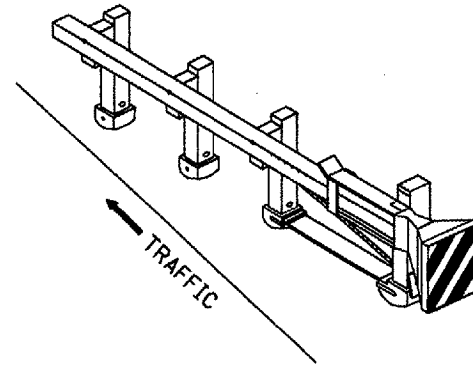
DATE	REVISIONS	BY
1-1-97	RENUM. E-10.02, NEW REVISION BOX	T.P.
3-1-97	CORRECT STD. SPEC. *	J.A.
10-16-06	REVISED TO 2007 SPEC.	M.A.

TERMINAL MARKER PLACEMENT

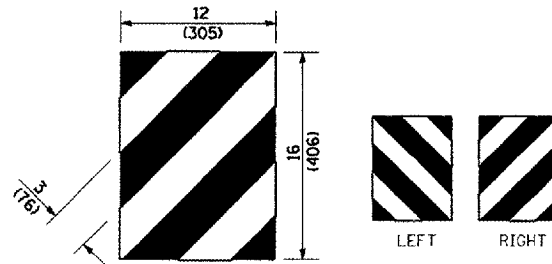
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18 RS. (18HB1)	KNOX	70	60
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



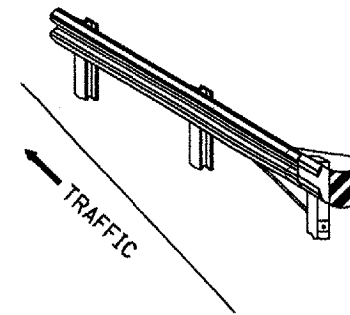
For Traffic Barrier Terminal Type 1 (Special)



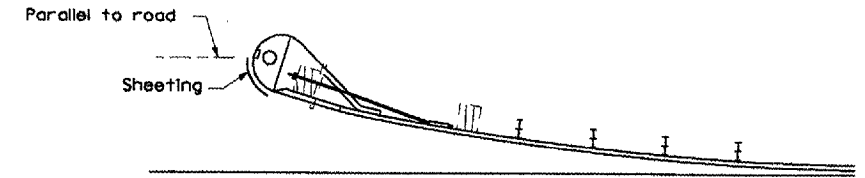
Standard Treatment - Direct Applied Sheeting
Traffic Barrier Terminal Type 1 (Special)



For Traffic Barrier Terminal Type (*)
and Post Mount
• See Plans for Type



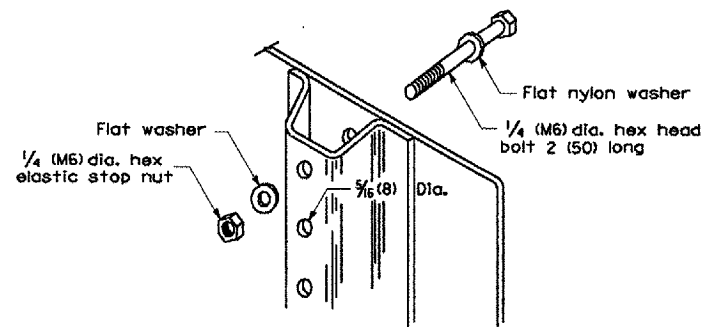
Standard Treatment - Direct Applied Sheeting
Traffic Barrier Terminal Type (*)
• See Plans for Type



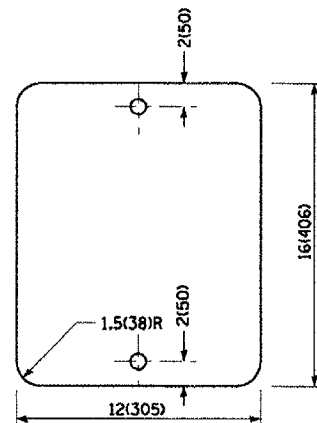
Sheeting Position for
Traffic Barrier Terminal Type (*)
• See Plans for Type

TERMINAL MARKER DETAILS

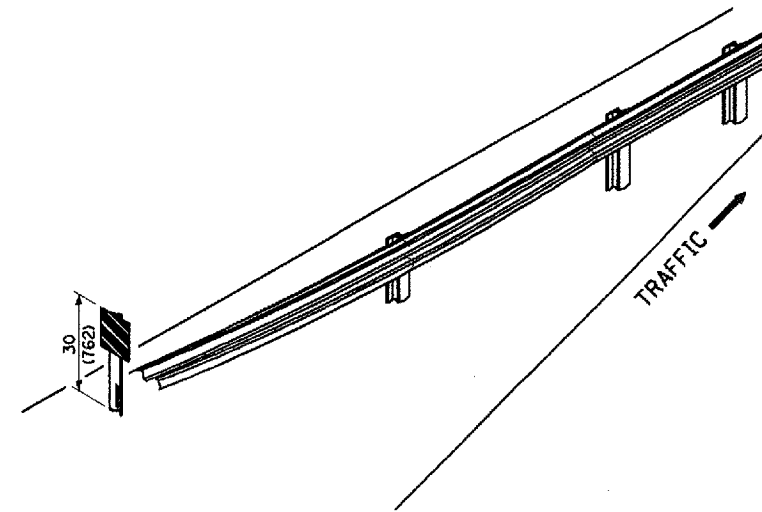
- Color: Black / Yellow reflectorized
- OM - 1100 (L or R) Direct applied reflective sheeting
- OM - 1200 (L or R) Post mounted



DETAIL OF MOUNTING TERMINAL MARKER TO POST



STANDARD TERMINAL MARKER



ALTERNATE TREATMENT - POST MOUNTED
(For turned-down terminal where sheeting
cannot be direct applied)

TERMINAL MARKER TREATMENTS

GENERAL NOTES

All dimensions are in inches (millimeters) unless otherwise noted.

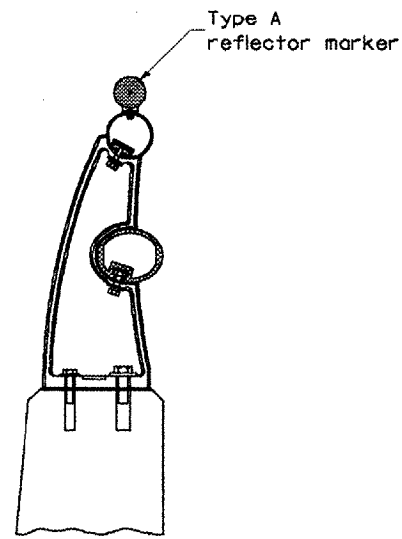
ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT CADD STANDARD

GUARDRAIL AND
BARRIER WALL DELINEATION

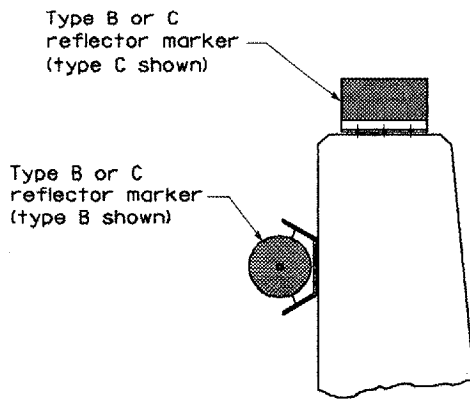
CADD STD. NO. 635101-04 SHEET 2 OF 3
SCALE: NOT DRAWN TO SCALE DRAWN BY CADD
CHECKED BY

\$\$\$DATE\$\$\$

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18 RS, (18HBI)	KNOX	70	61
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

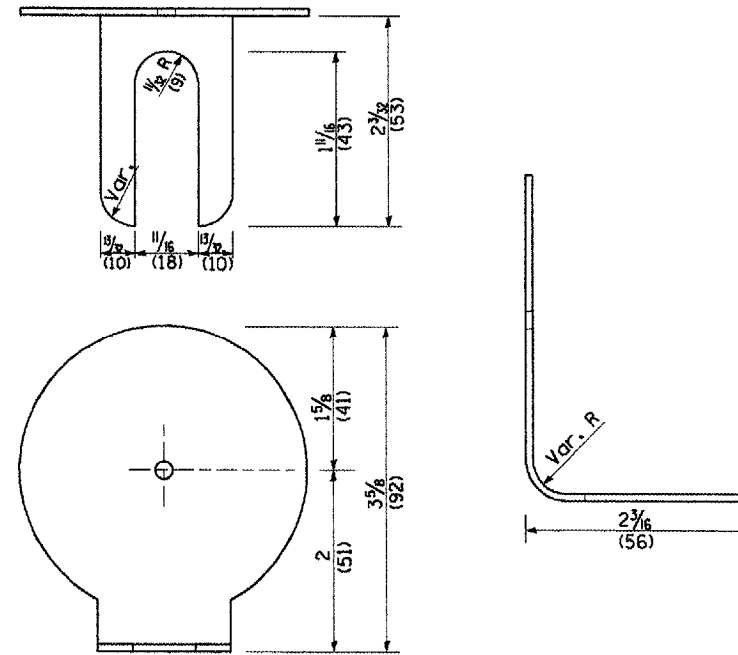


TYPICAL MOUNTING DETAIL FOR BRIDGE RAIL REFLECTOR



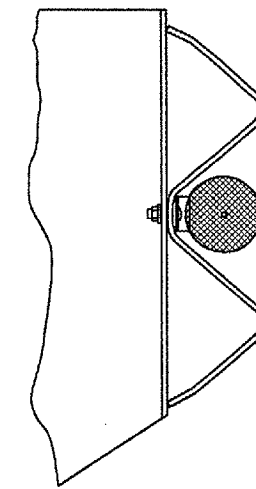
TYPICAL MOUNTING DETAIL FOR BARRIER WALL REFLECTOR

REFLECTOR MOUNTING

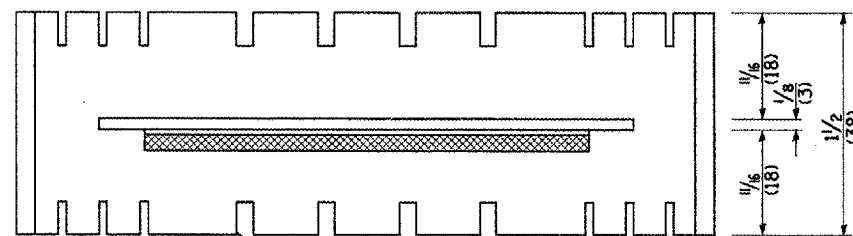


REFLECTOR MARKER TYPE A

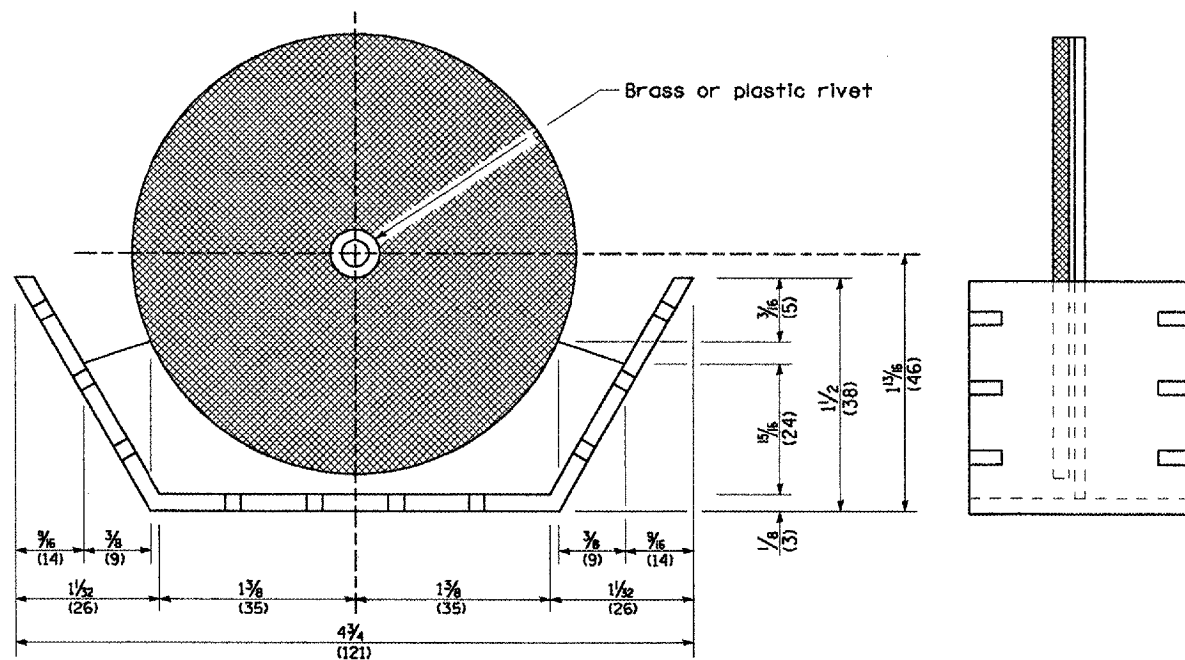
Min. reflective area 6 1/2 sq. in. (4,194 mm²) each side. May be rectangular or slight trapezoid.



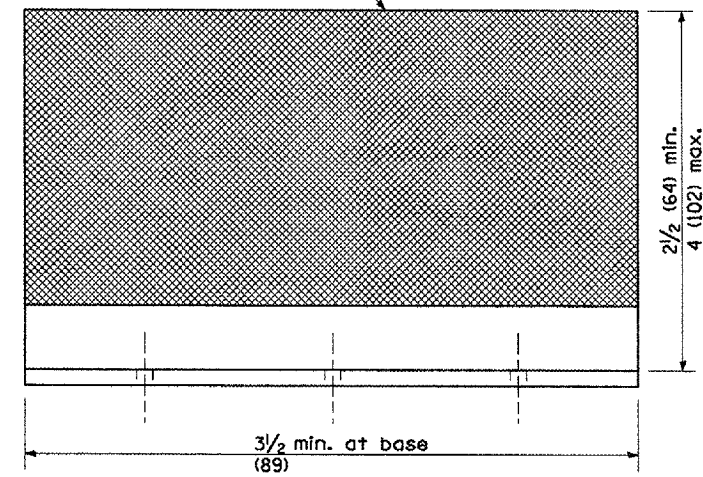
TYPICAL GUARDRAIL MOUNTING WITH REFLECTOR MARKER TYPE A



Adhesive weep slots or holes equally spaced on both sides

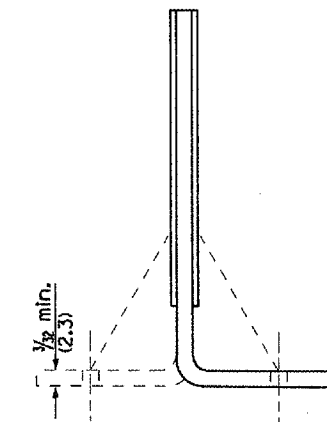


REFLECTOR MARKER TYPE B



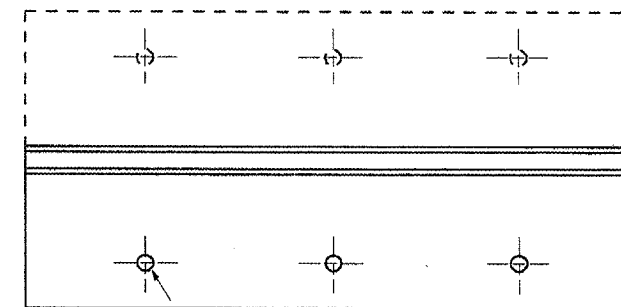
3 min. adhesive weep holes or slots each side, variable spacing.

REFLECTOR MARKER TYPE C



Cross section may be "T" or "L" shaped and may have side supports at ends.

REFLECTORS

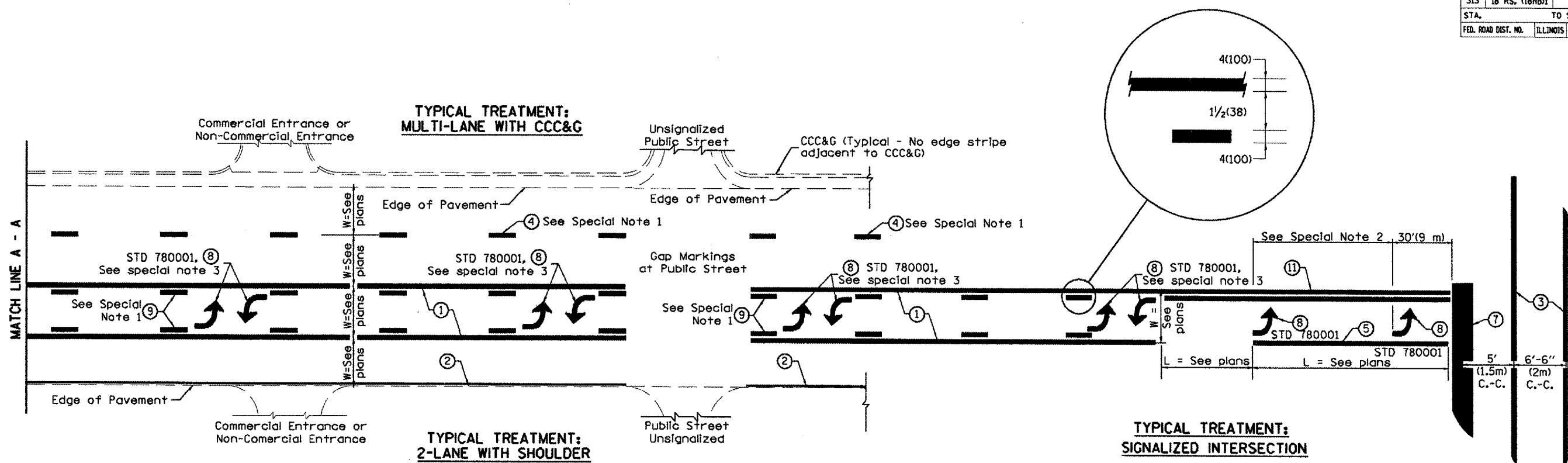


Minimum total area of base 7.0 Sq. in. (4,516 mm²)

All dimensions are in inches (millimeters) unless otherwise noted.

ILLINOIS DEPARTMENT OF TRANSPORTATION	
DISTRICT CADD STANDARD	
GUARDRAIL AND BARRIER WALL DELINEATION	
CADD STD. NO. 635101-04	SHEET 3 OF 3
SCALE: NOT DRAWN TO SCALE	DRAWN BY CADD
	CHECKED BY

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18 RS. (18HB)	KNOX	70	62
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



FLUSH PAVED MEDIAN: TWO-WAY LEFT TURN LANE WITH ONE-WAY LEFT TURN LANE AT SIGNALIZED INTERSECTION

TYPICAL PAVEMENT MARKING LEGEND

(Note: This is a District Standard Legend. Some elements may not apply to specific project.)

- ① 4(100) Solid (Yellow)
- ② 4(100) Solid (White)
- ③ 2-6(150) Crosswalk @ 6'-6" (2m)min C.-C. (White)
2-8(200) Crosswalk @ 6'-6" (2m)min C.-C. (White) (When traffic signals are present.)
- ④ 6(150) Skip-Dash (White) (See Special Note 1)
- ⑤ 8(200) Solid (White)
- ⑥ 12(300) Diagonal (White) (Item ⑥ is shown on Std. 780001)
- ⑦ 24(600) Stop Bar (White)
- ⑧ Letters & Arrows (See Std. 780001 and Special Notes 2 & 3)
- ⑨ 4(100) Skip-Dash (Yellow) (See Special Note 1)
- ⑩ 12(300) Diagonal (Yellow) (See Table A)
- ⑪ 4(100) Double Solid (Yellow) (See Table A)

SPECIAL NOTES

1. Skip-Dash markings will be centered between both ends of city blocks and shall be placed in alignment transversely across the pavement.
2. The following shall apply to arrows located in one-way left turn lanes:
 - A. A minimum of two (2) arrows is required.
 - B. The maximum spacing between arrows is 80' (24 m).
 - C. Arrows shall be evenly spaced if three (3) or more are required.
3. The following shall apply to arrow pairs located in two-way left turn lanes:
 - A. A minimum of two (2) arrow pairs is required.
 - B. The maximum spacing between arrow pairs is 200' (61 m).
 - C. Arrow pairs shall be evenly spaced if three (3) or more are required.
 - D. The spacing between Bi Directional Left Turn Arrows is 33' (10 m).

GENERAL NOTES

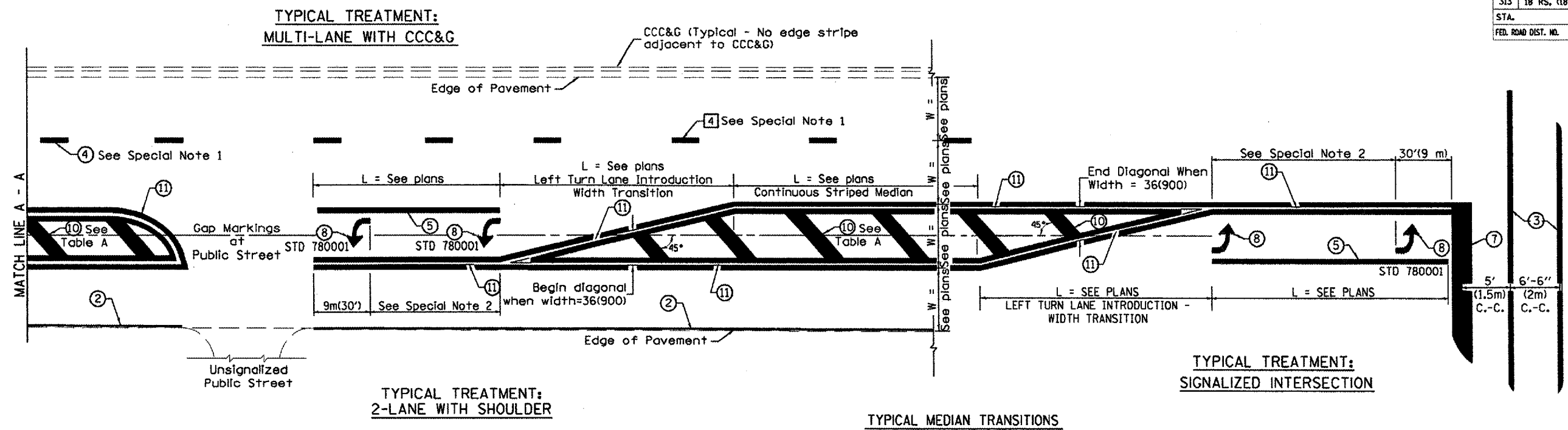
1. Refer to State Standard 780001 for additional Pavement Markings including letters & arrows.
2. See Plans for Pavement Markings adjacent to curbed islands and medians, and through lane reductions.

All dimensions are in Inches (millimeters) unless otherwise noted.

ILLINOIS DEPARTMENT OF TRANSPORTATION	
DISTRICT CADD STANDARD	
TYPICAL PAVEMENT MARKINGS	
CADD STANDARD 780001-D4	SHEET 1 OF 2
SCALE: NOT DRAWN TO SCALE	DRAWN BY CADD
	CHECKED BY

DATE	REVISIONS	BY
1-1-97	RENUM. F-8.03, NEW REVISION BOX	T.P.
2-7-97	ADD BI DIRECTIONAL DIMENSION	J.A.
10-97	CORRECT BI DIRECTIONAL DIMENSION	J.A.
8-02	ADD CROSSWALK DMNS. WITH T.S.	M.A.
10-16-06	REVISED TO 2007 SPEC.	M.A.

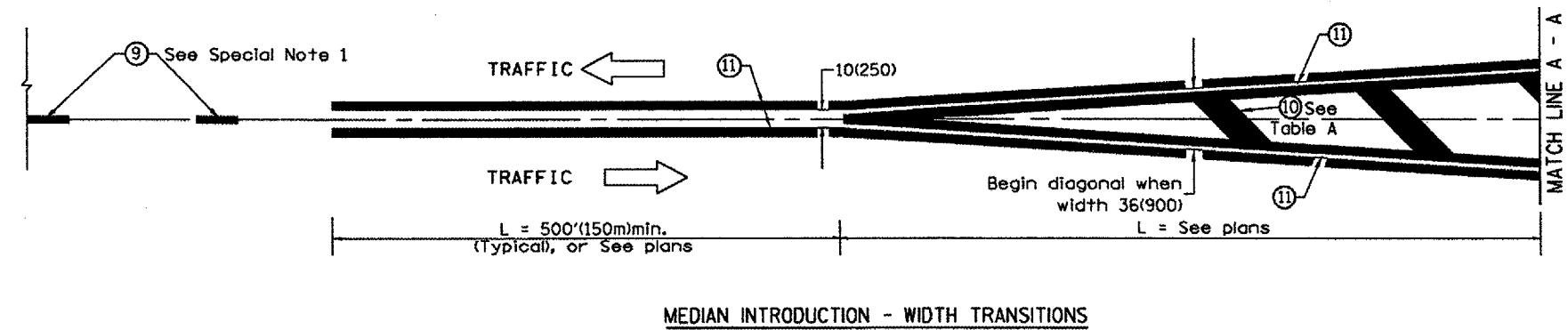
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18 RS, (18HD)	KNOX	70	63
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	



FLUSH PAVED MEDIAN: RESTRICTED LEFT TURN LANE

TABLE A
RECOMMENDED SPACING BETWEEN DIAGONAL LINES

SPEED LIMIT RANGE	CONTINUOUS	INTERSECTION CHANNELIZATION (Includes Width Transitions for Median and Left Turn Lane Introductions)
Less Than 30 mph (50 km/h)	50' (15m)	15' (5m)
30 - 45 mph (50 - 70 km/h)	75' (23m)	20' (6m)
Over 45 mph (70 km/h)	150' (46m)	30' (9m)



All dimensions are in inches (millimeters) unless otherwise noted.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT CADD STANDARD

TYPICAL PAVEMENT MARKINGS

CADD STANDARD 780001-D4
SCALE: NOT DRAWN TO SCALE

SHEET 2 OF 2
DRAWN BY CADD
CHECKED BY

\$\$\$DATE\$\$\$

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18RS(18HD)I	KNOX	70	64
STA. 367+50.00		TO STA. 380+44.62		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

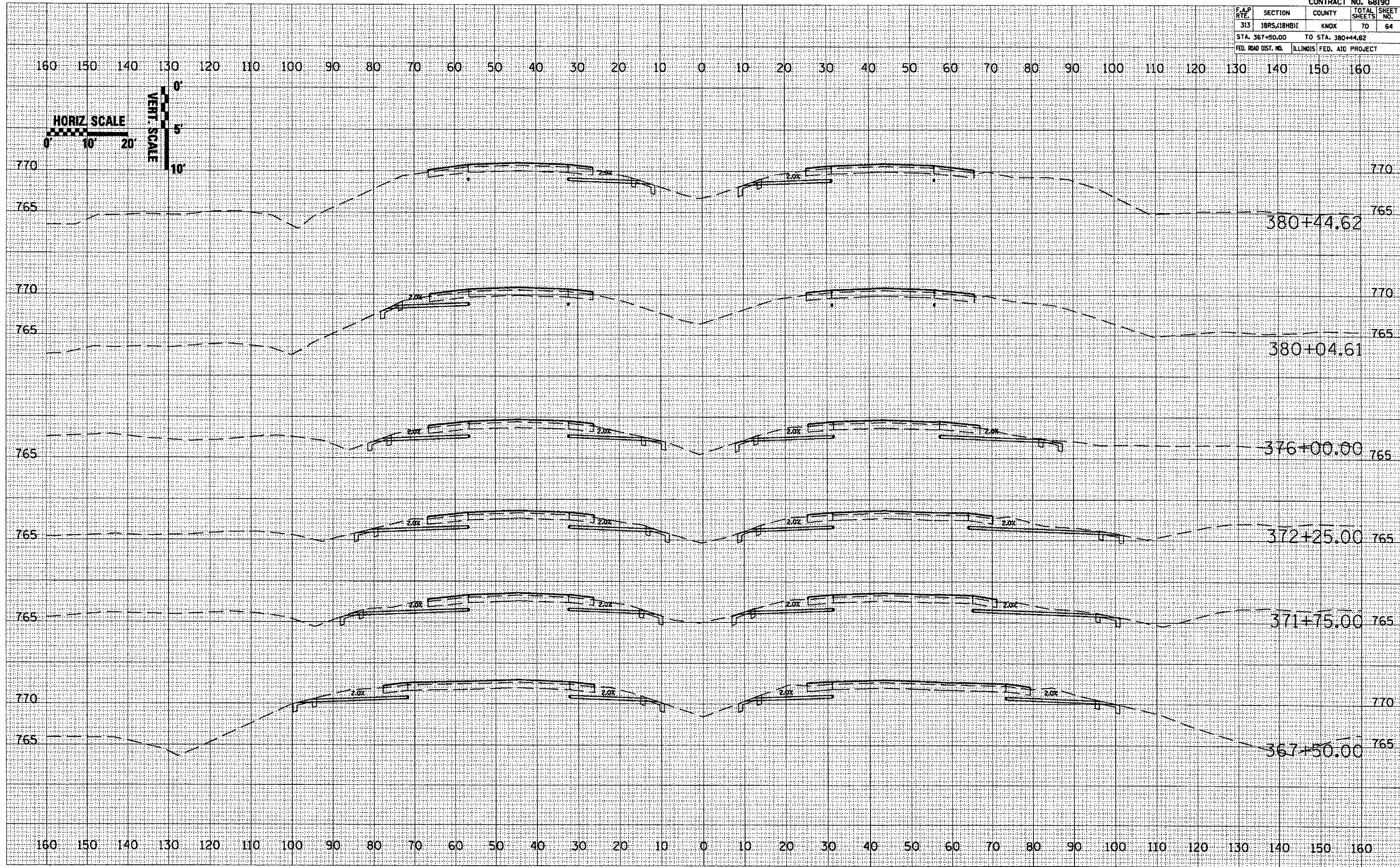
DATE	BY

DATE	BY

DATE	BY

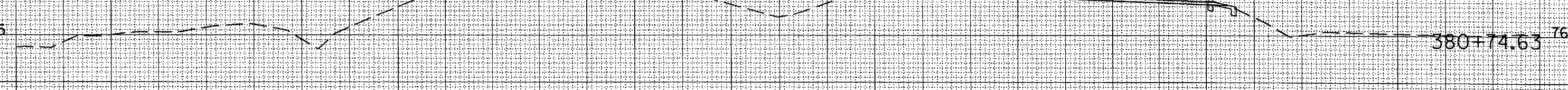
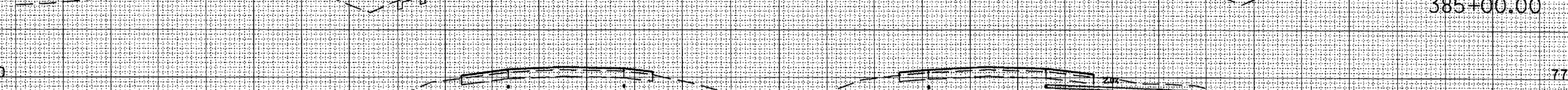
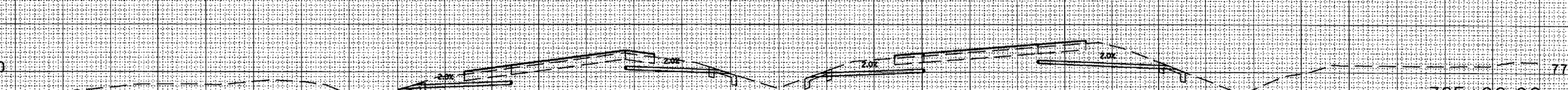
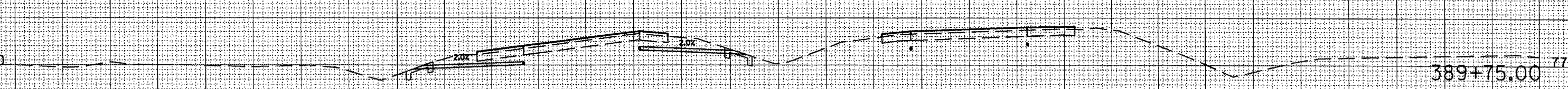
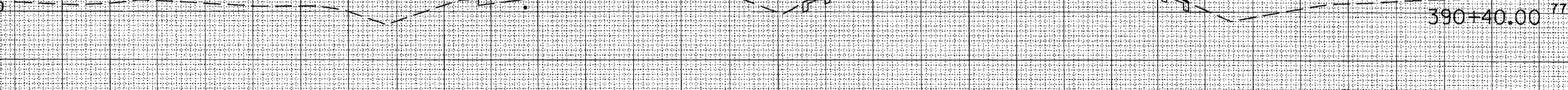
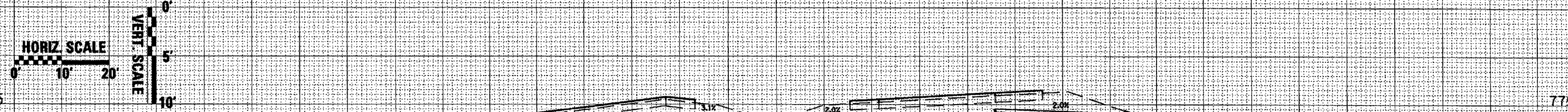
DATE	BY

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PLOT SCALE = 1/8" = 1'-0" / IN.
USER NAME = mduhanna



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18RS(18HB)1	KNOX	70	65
STA. 380+74.63		TO STA. 397+00.00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160



160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

DATE	
BY	
NO.	
AREAS CHECKED	
AREAS	
DATE	
BY	
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BY	
NO.	
AREAS CHECKED	
AREAS	

DATE: 10/18/2007
 PLOT DATE: 10/18/2007
 PLOT TIME: 10:20:00
 USER: NWC

F.A.P. R/YE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18RS, 018BH1	KNOX	70	67
STA. 417+40.00		TO STA. 421+00.00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

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HORIZ SCALE



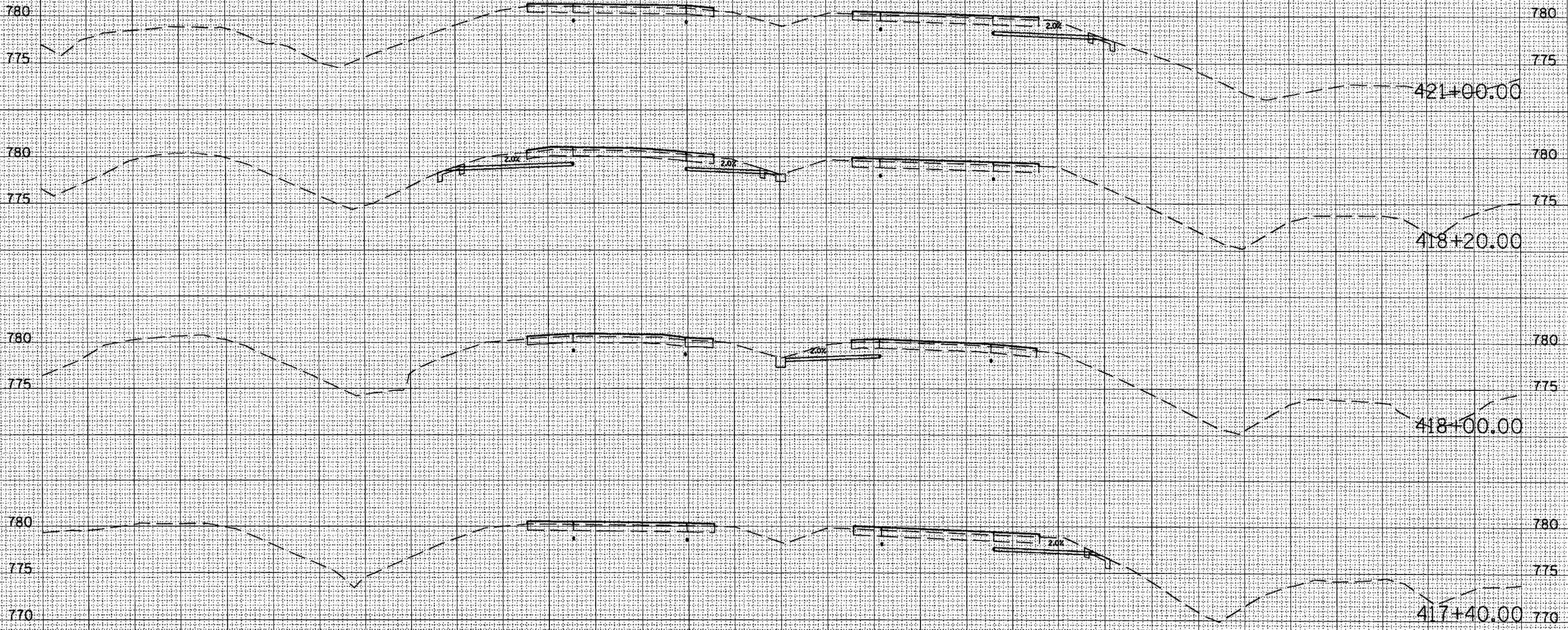
VERT. SCALE



DATE	BY

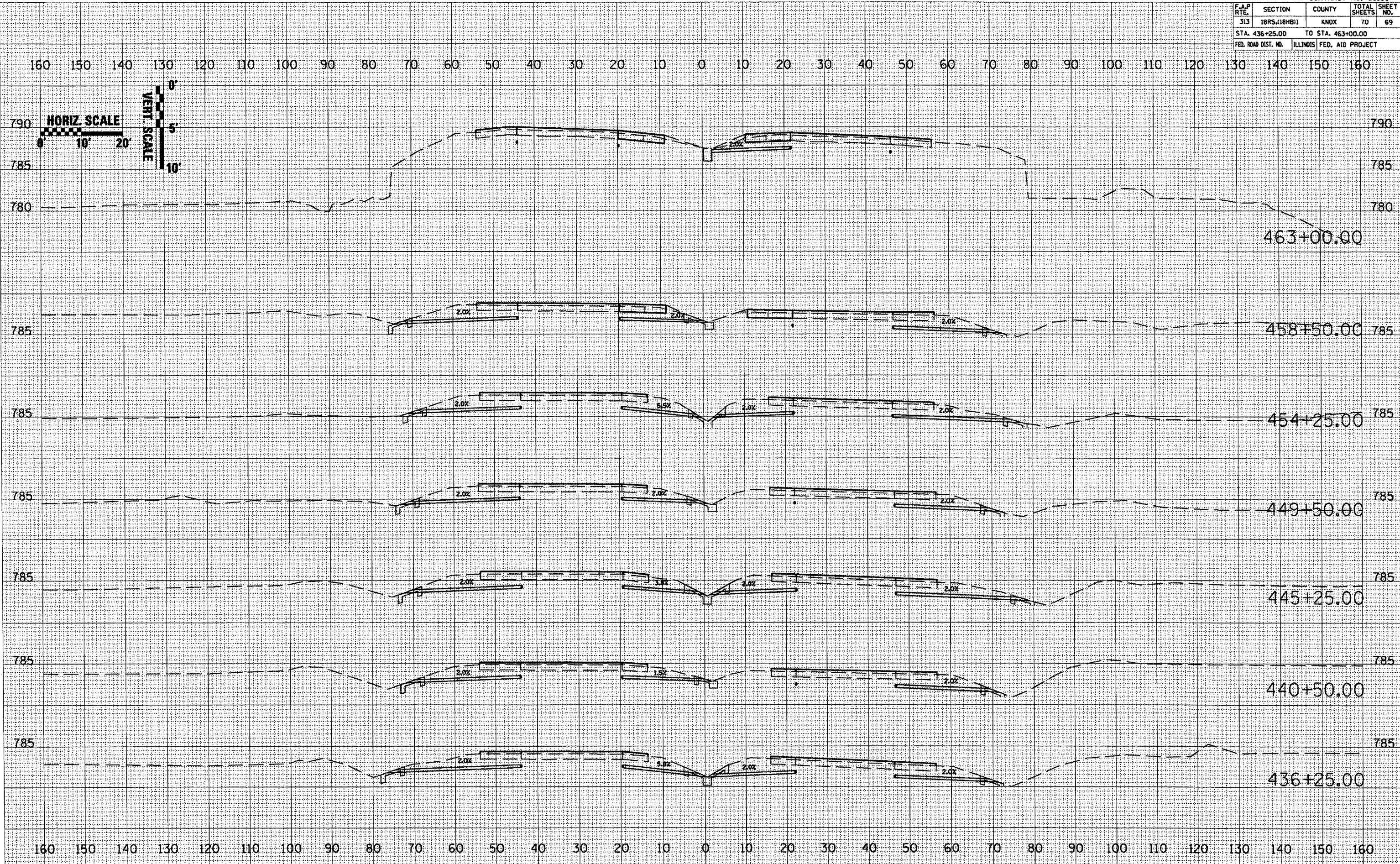
DATE	BY

PLT DATE: 12/18/2007
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 USER NAME: jerry



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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18RS.(18HB1)	KNOX	70	69
STA. 436+25.00		TO STA. 463+00.00		
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			



DATE	
BY	
NO.	
ORIGINAL SURVEY	
FLUOTED	
NOTE BOOK	
AREAS CHECKED	

DATE	
BY	
NO.	
ORIGINAL SURVEY	
FLUOTED	
NOTE BOOK	
AREAS CHECKED	

PLOT DATE = 10/18/2007
 USER NAME = subhaneem
 USER MAIL = subhaneem@statest.com
 USER PHONE = 618.303.7777
 USER FAX = 618.303.7777

CONTRACT NO. 68190			
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS
313	18RS(18HB)I	KNOX	70
STA. 463+25.00		TO STA. 475+30.00	
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		

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HORIZ SCALE



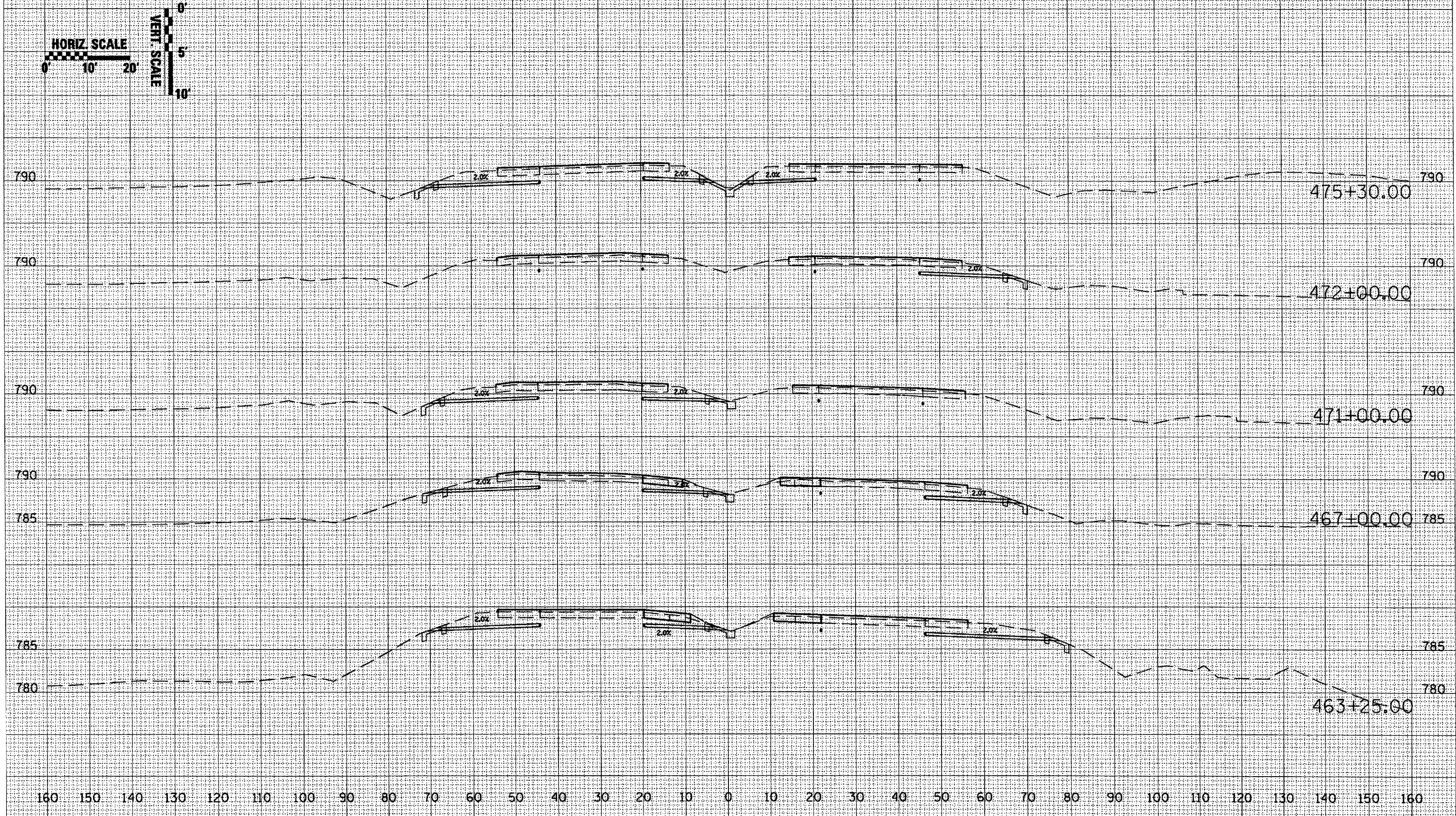
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BY	DATE

BY	DATE

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