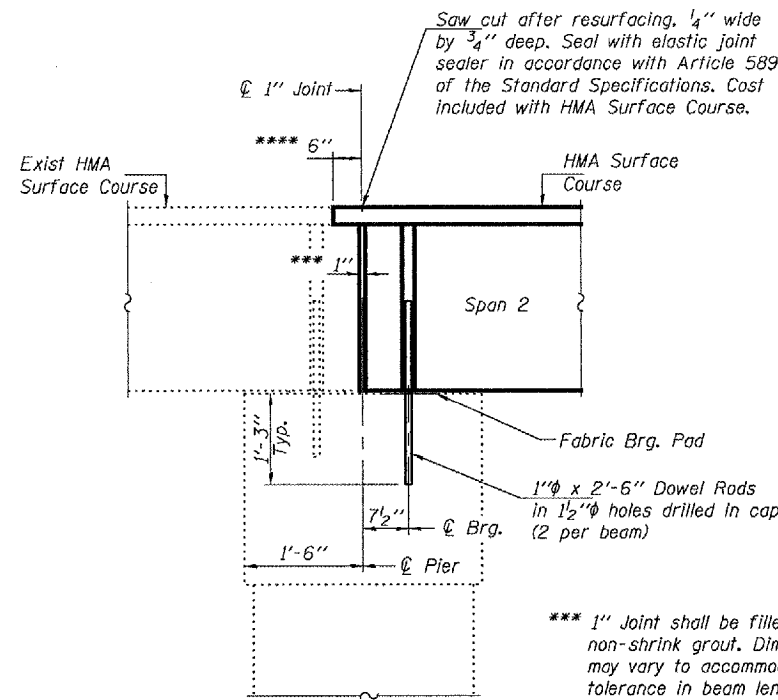


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAP 687		MC DONOUGH	20	20
Contract Number: 68744				

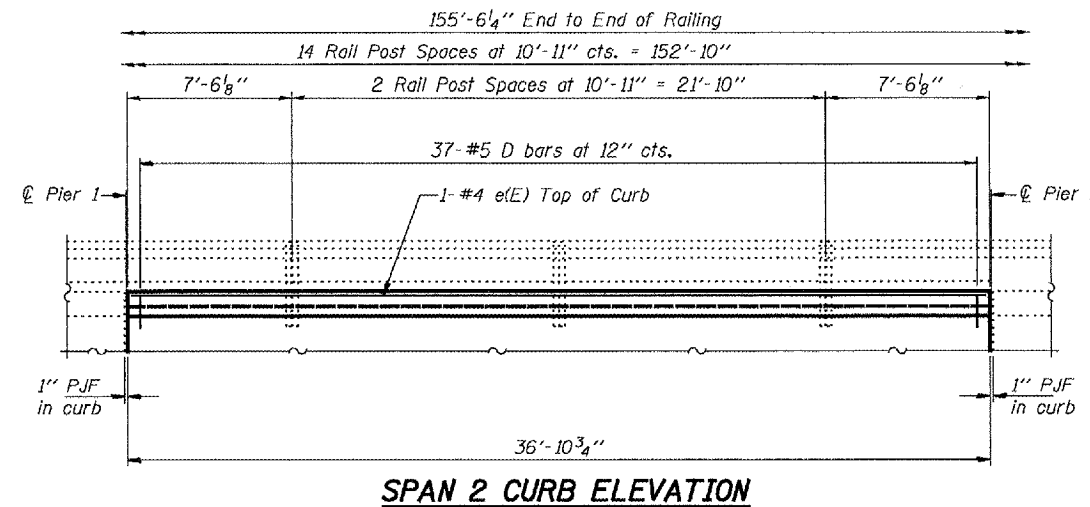


*** 1" Joint shall be filled with non-shrink grout. Dimension may vary to accommodate tolerance in beam lengths.

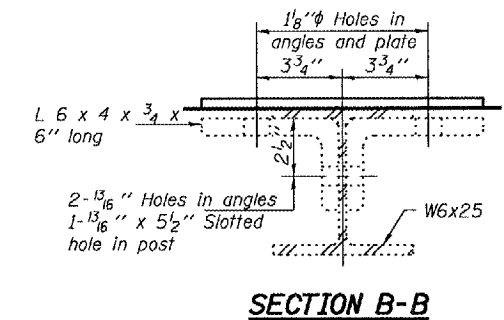
TYPICAL SECTION AT PIERS

Note:
Existing dowel rods are to be burned off, ground flush, and sealed with epoxy prior to placement of new beams. Cost included in Removal of Existing PPC Deck Beams. After beams have been erected holes shall be drilled into cap and dowel rods placed. Dowel holes shall be filled with non-shrink grout to top of beam and allowed to cure a minimum of 24 hours prior to grouting the shear keys.

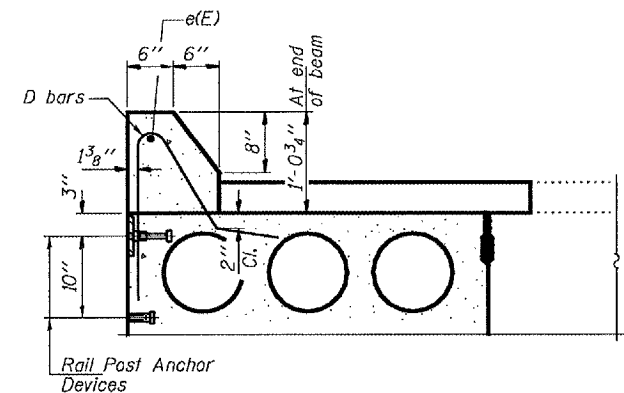
**** Limits of HMA surface removal. Existing waterproofing to remain. Lap new waterproofing 6" over existing.



SPAN 2 CURB ELEVATION

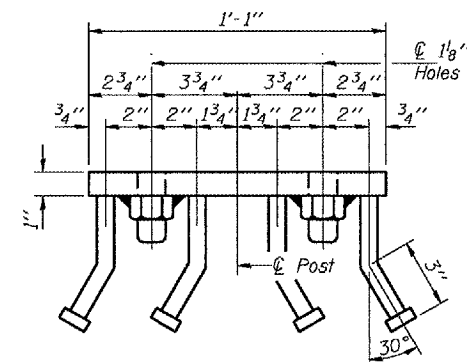


SECTION B-B

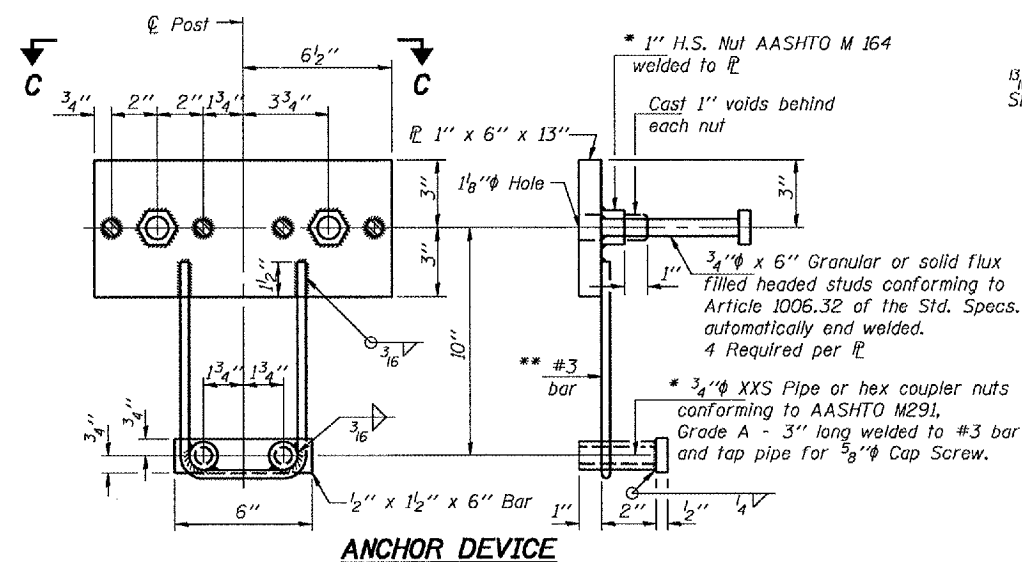


TYPICAL SECTION THRU CURB

** Whenever the lower insert assemblies interfere with strand locations, the #3 bars shall be cut and adjusted in order to allow raising or lowering of the lower inserts. Maximum adjustment not to exceed 1/2".

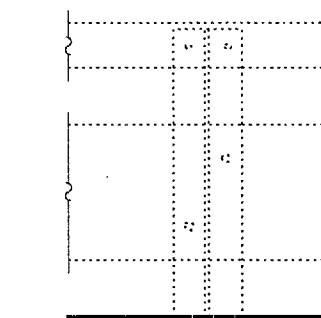


VIEW C-C

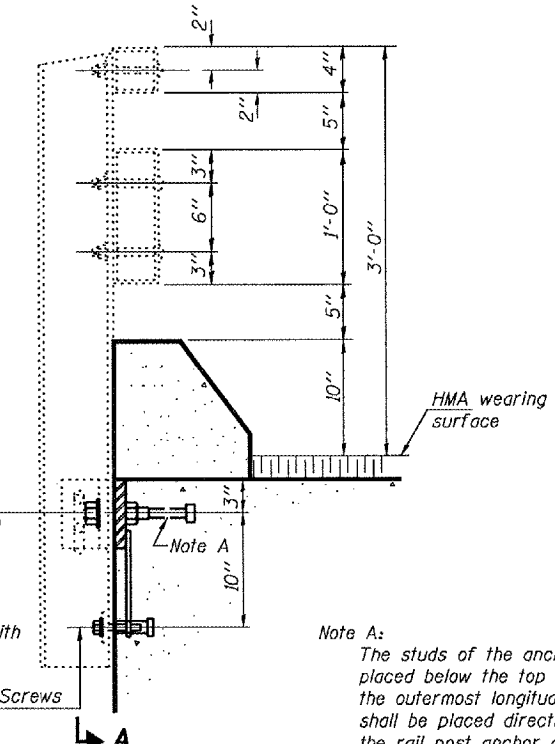


ANCHOR DEVICE

* Threaded areas shall be plugged or blocked off during casting of beam.



SECTION A-A



SECTION AT RAIL POST

Note A:
The studs of the anchor devices shall be placed below the top reinforcement bars and the outermost longitudinal reinforcement bar shall be placed directly above the studs of the rail post anchor device.

Notes:

All field drilled holes shall be coated with an approved zinc rich paint before erection.

For multi-span bridges, sufficient 1/4" x 6" x 1'-2" galvanized steel shims shall be provided to align rail between adjacent spans. Cost included with Removing and Re-erecting Existing Railing.

All steel rail elements shall be galvanized according to Article 509.05 of the Standard Specifications.

Removal and re-erection of the existing railing shall be accomplished in a manner that will avoid scratching, denting or other damage that may affect the durability or appearance of the railing.

The length paid for will be overall length along the rail from end to end, in place, at the location of re-erection.

This work will be paid for at the contract unit price per foot for Removing and Re-erecting Existing Railing, which price shall include removal, temporary storage, re-erection, asphalt paint or new bearing pads, shims and all new hardware required to satisfactorily complete the work.

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
e(E)	2	#4	36'-7"	
Concrete Superstructure			Cu. Yd.	2.5
Reinforcement Bars, Epoxy Coated			Lbs.	50

REPAIR DETAILS
FAP 687
MC DONOUGH COUNTY
SN 055-0017

DESIGNED	ATH
CHECKED	AJB
DRAWN	baliva
CHECKED	ATH AJB

NOVEMBER 7, 2007
EXAMINED *Carl Purvis*
ENGINEER OF STRUCTURAL SERVICES
PASSED *Ralph E. Anderson*
ENGINEER OF BRIDGES AND STRUCTURES