

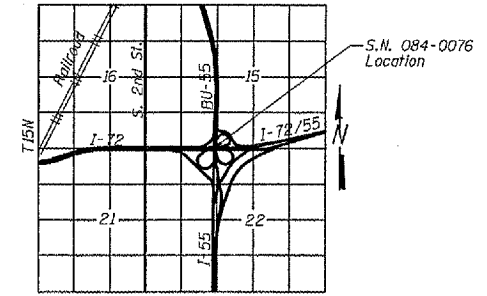
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CONTRACT NO. 72659

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**	SANGAMON	248	213

Sheet No. 1
of 7 Sheets

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT
* F.A.I. 55, F.A.I. 72, F.A.P. 666
** D6 Interstate R5, BR, M CAB 2008

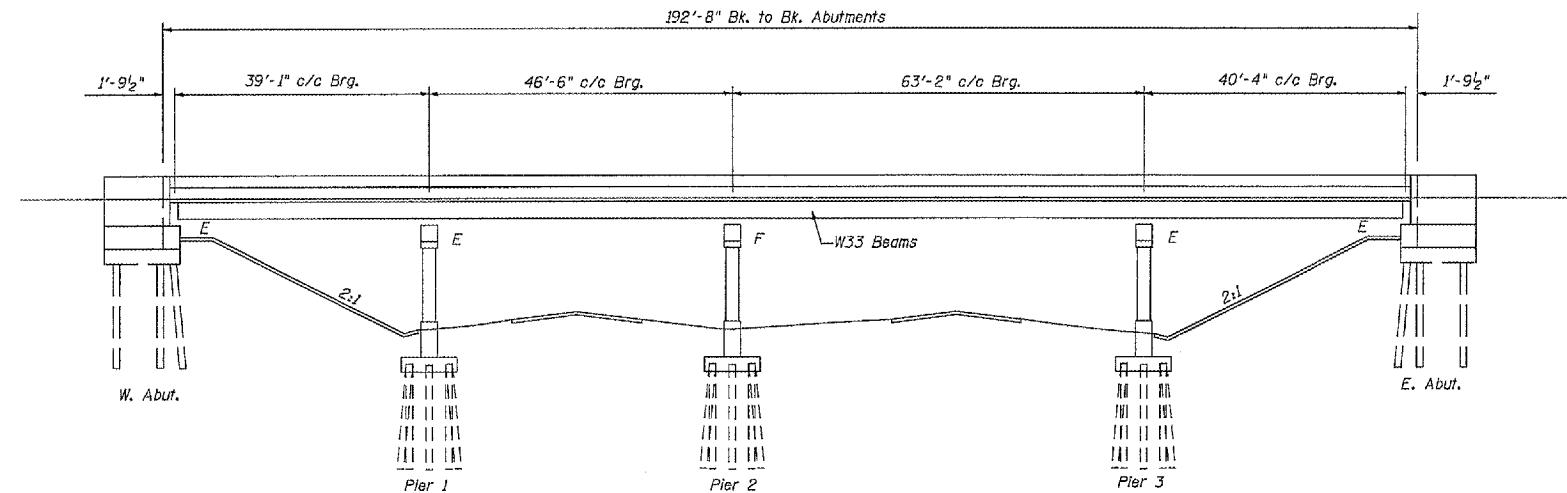


Existing Structure:

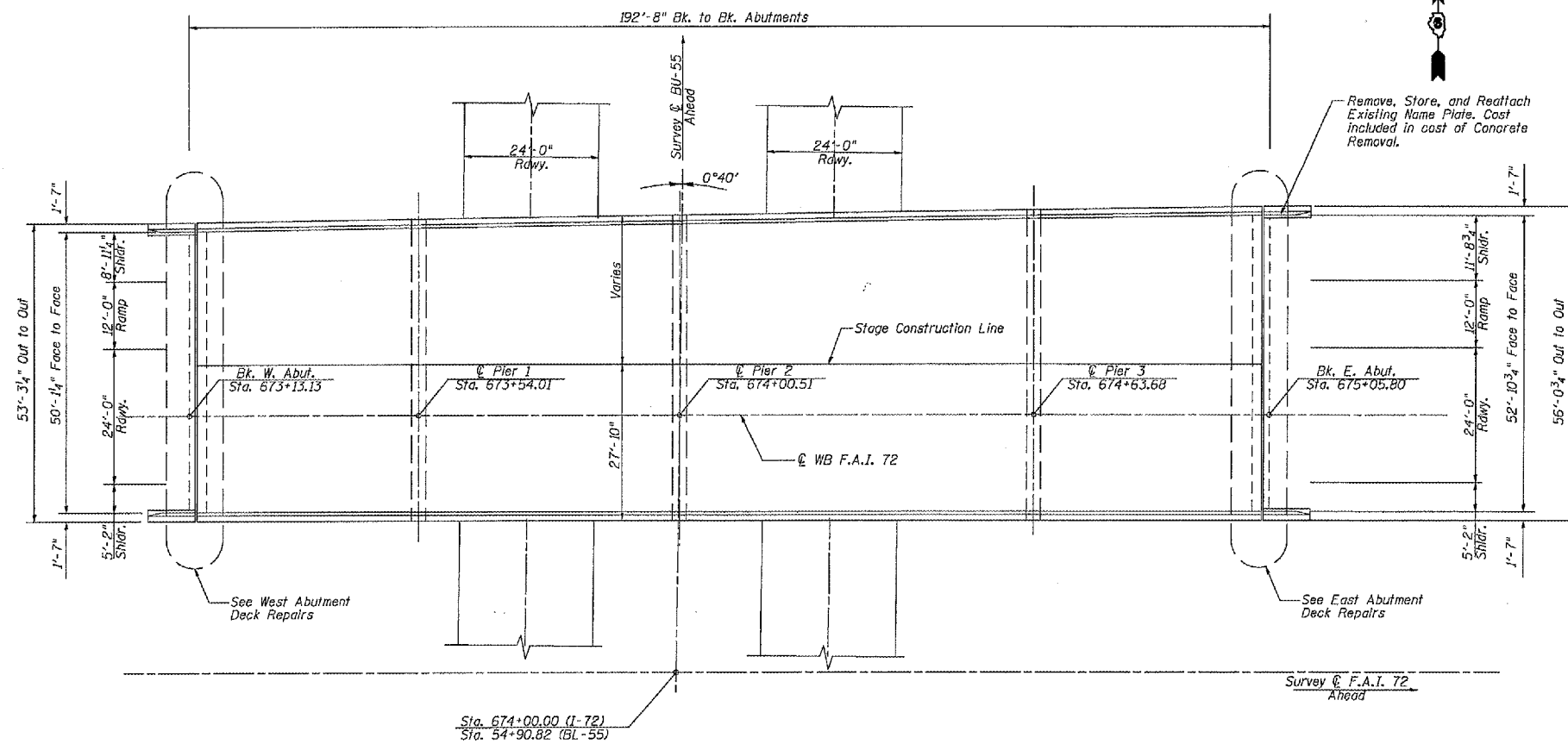
The existing structure, built in 1963 and deck replaced in 1981, is a four span rolled beam bridge with spill-through abutments on steel piles and hammerhead piers on timber piles. 192'-8" back to back of abutments, 53'-3 3/4" to 56'-0 3/4" out to out.

Proposed Improvements:

Work to be completed at this bridge includes: remove and replace portions of the existing deck, hatch blocks, and expansion joint replacement.



ELEVATION



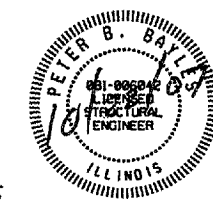
PLAN

GENERAL NOTES

1. Work will be completed utilizing stage construction with two lanes of WB I-72 open to traffic. See Traffic Control Plan details.
2. Plan dimensions and details relative to existing plans are subject to routine variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished based upon the unit price bid for the work.
3. Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.
4. As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by an individual acceptable to the Engineer. Any cracks that can not be removed by grinding 1/4 in. deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.
5. The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.
6. No field welding is permitted except as specified in the contract documents.
7. Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60 (IL Modified). See Special Provisions.
8. Reinforcement bars designated (E) shall be epoxy coated.

TOTAL BILL OF MATERIAL (S.N. 084-0076)

Item	Unit	Quantity
Concrete Removal	Cu. Yd.	18.3
Concrete Superstructure	Cu. Yd.	18.3
Bridge Deck Grooving	Sq. Yd.	37
Protective Coat	Sq. Yd.	60
Stud Shear Connectors	Each	243
Reinforcement Bars, Epoxy Coated	Pound	3180
Bar Splicers	Each	39
Polymer Concrete	Cu. Ft.	7.5
Silicone Joint Sealer, 2"	Foot	105



Peter B. Bayles
Peter B. Bayles, P.E., S.E.
Structural Engineer License No. 081-006042
Expiration Date: 11/30/2008

INDEX OF SHEETS

- 1 General Plan and Elevation
- 2 Typical Bridge Sections
- 3 Temporary Concrete Barrier
- 4 E. Abutment Deck Repair
- 5 E. Abutment Deck Repair Details
- 6 W. Abutment Deck Repairs
- 7 Bar Splicer Details

GENERAL PLAN & ELEVATION
F.A.I. 55, F.A.I. 72, F.A.P. 666
Section D6 Interstate, R5, BR, M CAB 2008
Sangamon County
WB I-72 Over BU-55
S.N. 084-0076