

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		COOK	22	1
ILLINOIS PROJECT		HPP-2902 (002)		
VILLAGE SECTION		06-00071-00-FP		
CONTRACT NO. 83983				



LOCATION OF SECTION INDICATED THUS: [Symbol]

FOR INDEX OF SHEETS SEE SHEET NO. 2

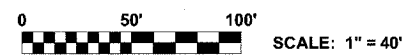
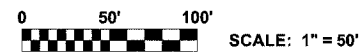
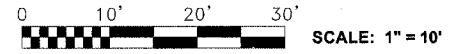
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PLANS FOR
PROPOSED FEDERAL AID PROJECT
BRISTOL AVENUE - CERMAK-FRONTAGE ROAD
TO DICKENS STREET
DOVER STREET - WESTCHESTER BLVD. (FAU 2713)
TO GARDNER ROAD (FAU 1005)
RESURFACING PROJECT
VILLAGE OF WESTCHESTER
SECTION 06-00071-00-FP
PROJECT HPP-2902 (002)
COOK COUNTY
C-91-178-07**

TRAFFIC DATA

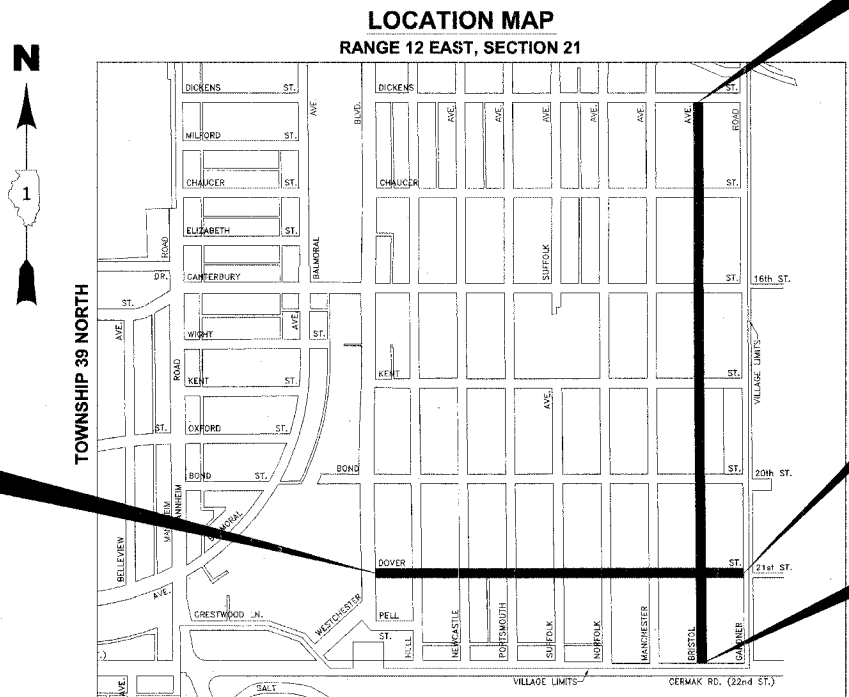
2030 ADT = 500
POSTED SPEED LIMIT: 25 MPH
DESIGN SPEED LIMIT: 30 MPH

DESIGN DESIGNATION :
LOCAL STREET

PROJECT LOCATED IN
THE VILLAGE OF WESTCHESTER



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.



PROJECT BEGINS
STATION 0+90
DOVER STREET

PROJECT ENDS
STATION 39+68
BRISTOL AVENUE

PROJECT ENDS
STATION 27+04
DOVER STREET

PROJECT BEGINS
STATION 0+89
BRISTOL AVENUE

PROVISO TOWNSHIP MAP SCALE: (NOT TO SCALE)

█ - AREA OF IMPROVEMENT

GROSS AND NET LENGTH OF PROJECT = 6,493 FT. = 1.23 MI.

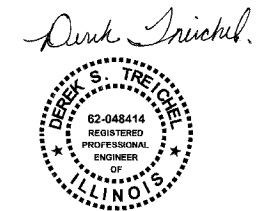
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED October 3RD 2007
Paul Mattina
VILLAGE OF WESTCHESTER, VILLAGE ADMINISTRATOR

PASSED November 16 2007
C. J. H. Cramer, Esq.
DISTRICT ONE ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID
BASED ON LIMITED
REVIEW Nov. 7 2007
Diane O'Keefe, Esq.
DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER

(PRINTED BY AUTHORITY OF THE STATE OF ILLINOIS)



DATE SIGNED: 10-03-07
LICENSE EXPIRES: 11-30-07

EDWIN HANCOCK ENGINEERING COMPANY
9933 ROOSEVELT ROAD PHONE: (708)865-0300
WESTCHESTER, ILLINOIS 60154

E.H.E. NO. 838-06-38501



**Know what's below.
Call before you dig.**
CALL 48 HOURS IN ADVANCE

CONTRACT NO. 83983

Drawing file: W:\Projects\23002303 - High Priority Project Bristol & Dover\1 - COVER.dwg Oct 03, 2007 - 11:26am ASSOCIATE FIELD ENGINEER: JESSICA FELICIANO (847) 705-4487

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
---	---	COOK	22	2
ILLINOIS PROJECT		HPP 2902 (002)		
VILLAGE SECTION		06-00071-00-FP		
CONTRACT NO. 83983				

INDEX OF SHEETS

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3	GENERAL NOTES
4	SUMMARY OF QUANTITIES
5	TYPICAL CROSS SECTIONS
6	ROADWAY DETAILS
7-8	DRAINAGE DETAILS
9-13	BRISTOL AVENUE - PLAN & PROFILE
14-17	DOVER AVENUE - PLAN & PROFILE
18-20	ASPHALT & LANDSCAPING, PAVEMENT MARKING, & EROSION CONTROL PLAN
21	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
22	BUTT JOINTS AND HOT-MIX ASPHALT TAPER DETAILS

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174	EAST-NORTHEAST FLANGE BOLT ON FIRE HYDRANT AT SOUTHEAST CORNER OF BRISTOL AVENUE & GLADSTONE STREET	626.77
175	EAST-NORTHEAST FLANGE BOLT ON FIRE HYDRANT AT SOUTHEAST CORNER OF BRISTOL AVENUE & KITCHENER STREET	627.07
183	EAST-NORTHEAST FLANGE BOLT ON FIRE HYDRANT AT NORTHEAST CORNER OF CANTERBURY STREET & BRISTOL AVENUE	627.93

LEGEND OF SYMBOLS

(TO BE USED IN CONJUNCTION WITH I.D.O.T. STANDARD 000001-04)

SYMBOL	DESCRIPTION
	EXISTING POWER POLE
	EXISTING GAS VALVE
	EXISTING TRAFFIC SIGNAL HEAD
	EXISTING TRAFFIC HAND HOLE
	EXISTING TRAFFIC SIGNAL MAST ARM
	EXISTING TRAFFIC SIGNAL POLE W/HEAD
	EXISTING TRAFFIC CONTROL BOX
	EXISTING TRAFFIC CONDUIT
	EXISTING TRAFFIC LOOP DETECTOR
	EXISTING STREET LIGHT
	EXISTING WATER MAIN BUFFALO BOX
	EXISTING SPRINKLER
	EXISTING WATER MAIN VALVE VAULT
	EXISTING BUSH
	EXISTING TREE
	EXISTING EVERGREEN TREE
	EXISTING HOT-MIX ASPHALT AREA
	EXISTING CONCRETE AREA
	EXISTING GRASS AREA
	EXISTING STONE OR GRAVEL AREA
	EXISTING STORM SEWER
	EXISTING COMBINATION SEWER
	EXISTING ELECTRIC LINE
	EXISTING GAS LINE
	EXISTING TELEPHONE LINE
	EXISTING WATER MAIN
	EXISTING CURB AND GUTTER
	EXISTING RIGHT OF WAY
	EXISTING STRUCTURE TO BE ADJUSTED
	EXISTING STRUCTURE TO BE RECONSTRUCTED
	EXISTING STRUCTURE TO BE REMOVED
	EXISTING STRUCTURE TO BE FILLED
	EXISTING STORM SEWER TO BE ABANDONED
	EXISTING WATER MAIN TO BE ABANDONED
	EXISTING CURB AND GUTTER TO BE REMOVED
	HOT-MIX ASPHALT SURFACE TO BE REMOVED
	EXISTING HOT-MIX ASPHALT AREA TO BE REMOVED - BUTT JOINT
	EXISTING CONCRETE AREA TO BE REMOVED
	EXISTING CONCRETE SIDEWALK TO BE REMOVED
	PROPOSED STORM SEWER
	PROPOSED WATER MAIN
	PROPOSED DIRECTION OF FLOW
	PROPOSED SUMMIT
	PROPOSED HOT-MIX ASPHALT AREA
	PROPOSED CONCRETE AREA
	PROPOSED GRASS AREA
	PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE B-4.12
	PROPOSED CATCH BASIN
	PROPOSED INLET
	PROPOSED VALVE VAULT
	PROPOSED FIRE HYDRANT

I.D.O.T. STANDARD DRAWINGS

STANDARD NO.	TITLE OR DESCRIPTION
000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-04	TEMPORARY EROSION CONTROL SYSTEM
424001-05	CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
602601-01	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
604001-02	FRAMES AND LIDS, TYPE 1
701501-04	URBAN LANE CLOSURE, 2-LANE, 2-WAY, UNDIVIDED
701801-03	LANE CLOSURE, MULTILANE, 1-WAY OR 2-WAY CROSSWALK OR SIDEWALK CLOSURE
701901	TRAFFIC CONTROL DEVICES

Drawing file: W:\Projects\03060385 - High Priority Project Bristol & Dover\2_index.dwg Oct 03, 2007 - 10:37am

HANCOCK ENGINEERING

◆ Civil Engineers
◆ Municipal Consultants
◆ Established 1911

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Westchester, Illinois 60154-2780
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HIGH PRIORITY PROJECT HPP-2902 (002)
BRISTOL AVENUE & DOVER STREET
VILLAGE OF WESTCHESTER, ILLINOIS

INDEX OF SHEETS, LEGEND, BENCHMARKS,
& I.D.O.T. STANDARD DRAWINGS

SCALE:	NONE	SHEET	2
DRAWN BY:	ECW/LEV	OF	22
BOOK NO.:	1541		
DATE:	10-2-07		
REVISION:		E.H.E. NO.:	838-06-38501

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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ILLINOIS PROJECT		HPP 2902 (002)		
VILLAGE SECTION		06-00071-00-FP		
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ACCESS

THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN ACCESS FOR EMERGENCY VEHICLES TO INGRESS AND EGRESS ONTO PRIVATE & COMMERCIAL PROPERTY DURING THE CONSTRUCTION PERIOD. ALSO DURING CONSTRUCTION, GARBAGE TRUCKS MUST BE PERMITTED ACCESS TO THE WORK ZONE IN ORDER TO PICK UP RESIDENTIAL AND COMMERCIAL GARBAGE FOR THOSE PROPERTIES FRONTING THE WORK ZONE.

STANDARDS

ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED TO BE THE LATEST STANDARD OF THE DEPARTMENT AS SHOWN ON THE INDEX OF SHEETS IN THE PLANS.

UNDERGROUND UTILITIES

THE LOCATIONS OF THE UNDERGROUND UTILITIES AS SHOWN ON THE PLANS HAVE BEEN OBTAINED BY FIELD SURVEYS AND SEARCHES OF AVAILABLE RECORDS. IT IS BELIEVED THAT THE DATA IS ESSENTIALLY CORRECT, BUT THE VILLAGE OF WESTCHESTER, OR OTHER OFFICES, AND AGENCIES ASSOCIATED WITH THE DEVELOPMENT OF THESE PLANS DO NOT GUARANTEE THEIR ACCURACY OR COMPLETENESS. THE CONTRACTOR WILL BE REQUIRED TO VERIFY THE EXACT LOCATION OF EACH FACILITY WITH THE UTILITY COMPANY, AND SHALL TAKE DUE CARE IN ALL PHASES OF THE CONSTRUCTION TO PROTECT ANY SUCH FACILITIES WHICH MAY BE AFFECTED BY THE WORK. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.

ADJUSTMENTS REQUIRED BY UTILITY COMPANIES WILL BE PERFORMED BY THE COMPANY INVOLVED OR ITS CONTRACTOR.

COORDINATION OF ALL UTILITY WORK INVOLVED IN THE CONSTRUCTION AREA WILL BE DISCUSSED AT A PRE-CONSTRUCTION CONFERENCE.

THE CONTRACTOR SHALL USE EXTREME CAUTION IN THE REMOVAL OF ABANDONED EXISTING GAS LINES SINCE RESIDUAL MATERIALS CONTAINED THEREIN ARE HIGHLY EXPLOSIVE, FLAMMABLE, AND TOXIC. ONCE THE MAINS ARE ABANDONED BY THE OWNER, THE CONTRACTOR SHALL ASSUME ALL RESPONSIBILITY FOR ANY DAMAGE AND/OR INJURY OCCURRING ON THE PROJECT DUE TO HIS OPERATIONS NEXT TO THE MAINS AND/OR THE METHOD OF REMOVAL OF THE ABANDONED MAINS. UNDER NO CIRCUMSTANCE SHALL THE UTILITY COMPANIES BE HELD LIABLE FOR ANY DAMAGE AND/OR INJURY ONCE THEIR MAINS HAVE BEEN RELOCATED AND THE EXISTING MAINS HAVE BEEN ABANDONED.

STORM SEWER

THE VERTICAL AND HORIZONTAL CLEARANCES BETWEEN WATER MAINS AND PROPOSED OR EXISTING STORM SEWERS SHALL CONFORM TO THE REQUIREMENTS OF THE I.E.P.A. AS STATED IN THEIR POLICY STATEMENTS, SECTION 31-1.02A THROUGH 31-1.02D.

FRAMES AND GRATES

THE TYPE OF FRAMES AND GRATES REQUIRED FOR ALL CATCH BASINS AND MANHOLES LISTED IN THE SUMMARY OF QUANTITIES MAY BE FOUND ON THE PLANS AT THEIR RESPECTIVE LOCATIONS. WHERE LIDS ARE CALLED FOR ON THE PLANS, THEY SHALL BE IN ACCORDANCE WITH ARTICLE 604.01 OF THE STANDARD SPECIFICATIONS AND THE TERM LID IS USED IN LIEU OF GRATE.

ON ALL IMPROVEMENTS, THE FRAMES AND LIDS OF EXISTING CATCH BASINS, INLETS, MANHOLES, AND VALVE VAULTS WHICH ARE TO BE ABANDONED DUE TO CONSTRUCTION OF THIS IMPROVEMENT ARE TO REMAIN THE PROPERTY OF THE VILLAGE OF WESTCHESTER AND BE SALVAGED.

THE WORD "WATER", "SANITARY", OR "STORM" SHALL BE CAST INTO THE LID OF EACH RESPECTIVE MANHOLE OR VALVE VAULT.

MAINTENANCE OF SEWER FLOWS

THE CONTRACTOR SHALL CONDUCT HIS OPERATIONS SO AS TO MAINTAIN AT ALL TIMES FLOWS THROUGH EXISTING STORM AND SANITARY SEWER SYSTEMS. HE SHALL ALSO PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT IF NECESSARY AND A TEMPORARY OUTLET AND BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER COLLECTED IN A SAFE MANNER WITHOUT DAMAGE OF ANY KIND TO ADJACENT PROPERTIES. THE ENDS OF EXISTING DRAINAGE LINES WHICH ARE NOT TO BE INCORPORATED INTO THE PROJECT ARE TO BE SEALED AS SPECIFIED IN THE SPECIAL PROVISIONS. THE COST OF ALL THE PREVIOUSLY MENTIONED WORK SHALL BE INCLUDED IN THE CONTRACT. ALL ACCUMULATION OF MATERIAL IN THE STRUCTURE DUE TO CONSTRUCTION OPERATIONS AS WELL AS MATERIAL EXISTING BEFORE CONSTRUCTION, SHALL BE REMOVED BY THE CONTRACTOR AT HIS EXPENSE.

EXISTING STRUCTURE MODIFICATIONS

ALL KNOWN EXISTING STRUCTURES IN THE PAVEMENT OR ADJACENT AREAS WHICH ARE INVOLVED IN THE CONSTRUCTION HAVE BEEN SHOWN ON THE PLANS AND NOTED AS TO BE REMOVED, FILLED, RECONSTRUCTED, OR ADJUSTED BY THE CONTRACTOR EXCEPT THOSE OF AMERITECH, COMED, AND THE NICOR GAS COMPANY, WHICH ARE TO BE ADJUSTED BY THE APPROPRIATE UTILITY FORCE. WHERE EXISTING STRUCTURES ARE TO BE REMOVED OR FILLED, OR THE EXISTING CASTING REPLACED, THE CASTINGS REMOVED FROM THE STRUCTURE SHALL BECOME THE PROPERTY OF THE VILLAGE OF WESTCHESTER.

SHEETING OR SHORING

IT SHOULD BE NOTED THAT ANY SHEETING OR SHORING REQUIRED FOR THE STORM SEWER INSTALLATION OR OTHER CONSTRUCTION ELEMENTS REQUIRING RELATIVELY DEEP EXCAVATIONS, SHALL BE INCLUDED IN THE PARTICULAR PAYMENT ITEM AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR ANY SUPPLEMENTAL WORK ASSOCIATED WITH THE MAINTENANCE OF TRENCH SIDES OR OTHER EXCAVATED AREAS.

MAINTENANCE OF EXISTING DRAINAGE STRUCTURES

WHEN DURING THE CONSTRUCTION OPERATIONS, ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF ANY GUTTERS OR DRAINAGE STRUCTURE SO THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF THE CONSTRUCTION OPERATIONS, ALL DRAINAGE FACILITIES SHALL BE CLEAN AND FREE OF ALL OBSTRUCTIONS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE CONTRACT.

SAW CUTTING

THE CONTRACTOR SHALL SAW CUT ASPHALT PAVEMENT AND DRIVEWAY PAVEMENT AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER, TO SEPARATE THE EXISTING PAVEMENT TO BE REMOVED BY MEANS OF AN APPROVED CONCRETE SAW TO A DEPTH AS DIRECTED BY THE ENGINEER. SUITABLE GUIDELINES OR DEVICES SHALL BE USED TO ASSURE CUTTING A NEAT, STRAIGHT LINE AS SHOWN ON THE PLANS. CARE SHALL BE TAKEN BY THE CONTRACTOR SO AS NOT TO DAMAGE THE REMAINING PAVEMENT DIRECTLY ADJACENT TO THE PAVEMENT TO BE REMOVED. ANY DAMAGE TO THE EXISTING PAVEMENT RESULTING FROM PAVEMENT REMOVAL OPERATIONS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. THE PRICE OF SAW CUTTING, AS NOTED ABOVE, SHALL BE INCLUDED IN THE PARTICULAR PAY ITEMS.

PRIME COAT

PRIME COAT MUST BE INSTALLED NO EARLIER THAN TWENTY-FOUR (24) HOURS PRIOR TO PLACEMENT OF HOT-MIX ASPHALT

TRAFFIC PROTECTION

CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT WHEN WORK COMMENCES, THE CONTRACTOR SHALL ASSUME THE MAINTENANCE OF ANY PAVEMENT, SHOULDERS, DRAINAGE FACILITIES, TRAFFIC CONTROL SIGNS, PAVEMENT MARKINGS, AND OTHER APPURTENANCES ON ROADWAYS WITHIN THE LIMITS OF THE CONTRACT WHICH ARE TO BE USED BY THE PUBLIC DURING CONSTRUCTION AND TO RETAIN THIS MAINTENANCE RESPONSIBILITY UNTIL THE VILLAGE ASSUMES THE MAINTENANCE. NEED FOR SNOW AND ICE CONTROL DURING THE CONSTRUCTION PERIOD SHALL BE ACCOMMODATED FOR BY OTHERS. ALL UNBALLASTED TYPE I & TYPE II BARRICADES SHALL HAVE TWO SANDBAGS ONE ACROSS EACH BOTTOM RAIL.

PLUGGING EXISTING SEWER LINES

UNLESS OTHERWISE SPECIFIED, ABANDONED SEWER LINES, AS DESIGNATED BY THE ENGINEER, SHALL BE PLUGGED WITH CLASS "SI" CONCRETE OR BRICK AND SUITABLE MORTAR TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE PAY ITEMS FOR REMOVING AND/OR FILLING THE VARIOUS TYPES OF STRUCTURES.

REMOVAL OF EXISTING STRUCTURES

AT LOCATIONS DESIGNATED FOR NEW DRAINAGE STRUCTURES WHERE EXISTING STRUCTURES ARE CURRENTLY LOCATED, THE EXISTING STRUCTURE SHALL BE REMOVED AS PART OF THE SITE PREPARATION FOR THE NEW STRUCTURE. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE PARTICULAR PAY ITEMS.

HOT-MIX ASPHALT PAVING OPERATIONS

THE LEVELING BINDER WILL BE PLACED IN TWO PASSES WITH A COLD JOINT LOCATED WITHIN THREE INCHES (3") OF THE CENTER LINE OF THE PAVEMENT. THE HOT-MIX ASPHALT SURFACE COURSE WILL BE PLACED IN TWO PASSES WITH A COLD JOINT LOCATED AT THE CENTER LINE OF THE PAVEMENT. THE CONTRACTOR WILL BE REQUIRED TO SCHEDULE HIS OPERATIONS SO THAT NO SECTIONS OF PAVEMENT ALONG THE CENTER LINE WILL HAVE A COLD JOINT OVERNIGHT.

OPEN EXCAVATIONS

THE CONTRACTOR WILL NOT BE ALLOWED TO LEAVE ANY EXCAVATION NECESSARY FOR PAVEMENT PATCHES OR STRUCTURE ADJUSTMENTS OVERNIGHT. THE CONTRACTOR WILL BE RESPONSIBLE FOR COMPLETELY BACKFILLING OR INSTALLING A PLATE OVER ALL EXCAVATIONS AT THE END OF THE DAY.

CONCRETE BREAKERS

WHEN REMOVING PAVEMENT, CURB AND GUTTER, AND/OR ANY STRUCTURES, THE USE OF ANY TYPE OF CONCRETE BREAKERS, SUCH AS DROP HAMMERS, WHICH MIGHT DAMAGE UNDERGROUND PUBLIC OR PRIVATE UTILITIES, WILL NOT BE PERMITTED. UNDER NO CIRCUMSTANCES WILL THE USE OF A FROST BALL BE PERMITTED.

GENERAL NOTES



◆ Civil Engineers
◆ Municipal Consultants
◆ Established 1911

9933 Roosevelt Road
Westchester, Illinois 60154-2780
Phone: 708/865-0300
Fax: 708/865-1212

HIGH PRIORITY PROJECT HPP-2902 (002)
BRISTOL AVENUE & DOVER STREET
VILLAGE OF WESTCHESTER, ILLINOIS

SCALE: NONE
DRAWN BY: ECW/LEV
BOOK NO.: 1541
DATE: 10-2-07
E.H.E. NO.: 838-06-38501

SHEET
3
OF
22

SUMMARY OF QUANTITIES				
CODE	PAY ITEM	UNIT	TOTAL QUANTITY	1000-2A 80% FEDERAL 20% LOCAL
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CUYD	100	100
20800150	TRENCH BACKFILL	CUYD	825	825
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQYD	700	700
21101610	TOPSOIL FURNISH AND PLACE, 3"	SQYD	5,850	5,850
25200100	SODDING	SQYD	5,850	5,850
25200200	SUPPLEMENTAL WATERING	UNIT	45	45
28000510	INLET FILTERS	EACH	60	60
35101800	AGGREGATE BASE COURSE, TYPE B 6"	SQYD	700	700
35300300	PORTLAND CEMENT CONCRETE BASE COURSE, 8"	SQYD	1,230	1,230
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GAL	2,200	2,200
40600300	AGGREGATE (PRIME COAT)	TON	45	45
40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	1,350	1,350
40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	175	175
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	2,025	2,025
42101300	PROTECTIVE COAT	SQYD	7,175	7,175
42300300	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH	SQYD	2,700	2,700
42400200	PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH	SQFT	12,750	12,750
42400800	DETECTABLE WARNINGS	SQFT	1,100	1,100
44000100	PAVEMENT REMOVAL	SQYD	1,400	1,400
44000198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQYD	14,250	14,250
44000200	DRIVEWAY PAVEMENT REMOVAL	SQYD	2,700	2,700
44000600	SIDEWALK REMOVAL	SQFT	12,750	12,750
44001900	COMBINATION CURB AND GUTTER REMOVAL (SPECIAL)	FOOT	13,775	13,775
60213800	RESTRICTED DEPTH CATCH BASINS, 4' DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	39	39
60224600	RESTRICTED DEPTH MANHOLES, 4' DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	3	3
60234200	INLETS, TYPE A, TYPE 1 FRAME, OPEN LID	EACH	13	13
60250200	CATCH BASINS TO BE ADJUSTED	EACH	9	9
60255500	MANHOLES TO BE ADJUSTED	EACH	13	13
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	24	24
60265700	VALVE VAULTS TO BE ADJUSTED	EACH	3	3
60406500	FRAMES AND LIDS, SPECIAL	EACH	30	30
60500050	REMOVING CATCH BASINS	EACH	6	6
67100100	MOBILIZATION	LUSM	1	1

* DENOTES SPECIALTY ITEM

SUMMARY OF QUANTITIES				
CODE	PAY ITEM	UNIT	TOTAL QUANTITY	1000-2A 80% FEDERAL 20% LOCAL
70101800	TRAFFIC CONTROL AND PROTECTION (SPECIAL)	L SUM	1	1
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	2,842	2,842
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	375	375
X0323426	SEDIMENT CONTROL, DRAINAGE STRUCTURE INLET FILTER CLEANING	EACH	60	60
X0325207	TELEVISION INSPECTION OF SEWER	FOOT	2,900	2,900
X4420132	CLASS D PATCHES (SPECIAL)	SQYD	5,820	5,820
XX001286	SPECIAL EXCAVATION	CUYD	300	300
XX003036	STORM SEWERS, PVC, SDR 26, 10 INCH	FOOT	1,255	1,255
XX004208	SEWER SERVICE LATERAL REPAIR, 6" POLYVINYL CHLORIDE	FOOT	200	200
XX005565	REMOVE AND RESET BRICK PAVERS	SQFT	150	150
XX005634	12" X 6" PVC SEWER SERVICE CONNECTIONS	EACH	20	20
XX005770	STRUCTURE TO BE ABANDONED	EACH	12	12
XX006452	15" X 6" PVC SEWER SERVICE CONNECTIONS	EACH	10	10
XX006464	DOMESTIC WATER SERVICE BOX TO BE ADJUSTED (SPECIAL)	EACH	10	10
Z0018900	DRILL AND GROUT DOWEL BARS	EACH	685	685
Δ Z0076600	TRAINEES	HOUR	500	500
XX005689	STORM SEWERS, DIP CLASS 52, 10"	FOOT	50	50
XX007154	18" X 6" SEWER SERVICE CONNECTIONS	EACH	3	3
XX007155	27" X 6" SEWER SERVICE CONENCTIONS	EACH	13	13
XX007156	12" DIAMETER, PVC STORM SEWER PIPE REPLACEMENT (POINT REPAIR)	FOOT	375	375
XX007157	15" DIAMETER, PVC STORM SEWER PIPE REPLACEMENT (POINT REPAIR)	FOOT	60	60
XX007158	18" DIAMETER, PVC STORM SEWER PIPE REPLACEMENT (POINT REPAIR)	FOOT	70	70
XX007159	27" DIA. RCP, C-76 W/C-443 JOINTS, CLASS III, STORM SEWER (POINT REPAIR)	FOOT	300	300
XX000671	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-4.12	FOOT	13,775	13,775

* DENOTES SPECIALTY ITEM

Δ Y080

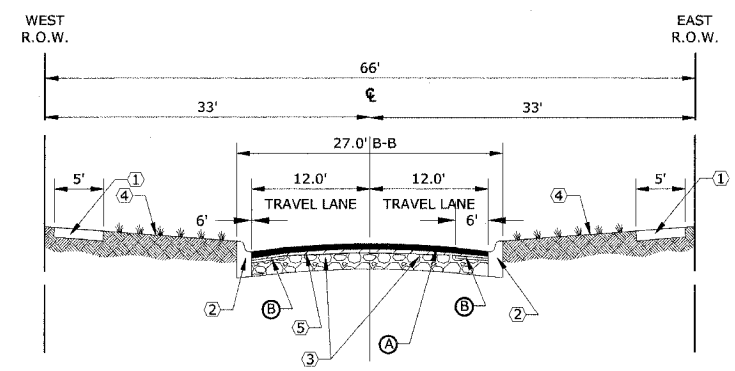
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	◆ Civil Engineers	9933 Roosevelt Road
	◆ Municipal Consultants	Westchester, Illinois 60154-2780
	◆ Established 1911	Phone: 708/865-0300
		Fax: 708/865-1212

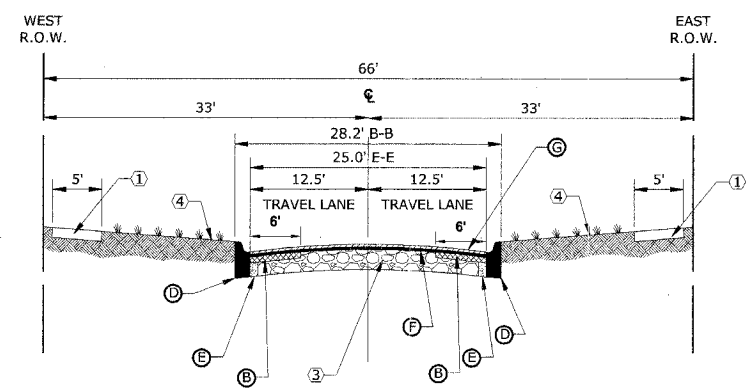
HIGH PRIORITY PROJECT HPP-2902 (002)
BRISTOL AVENUE & DOVER STREET
VILLAGE OF WESTCHESTER, ILLINOIS

SUMMARY OF QUANTITIES

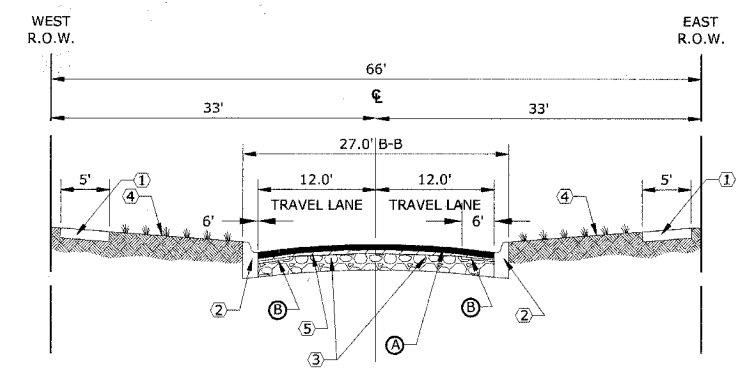
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BOOK NO.:	1541	
DATE:	10-2-07	
REVISION:	E.H.E. NO.: 838-06-38501	



EXISTING TYPICAL CROSS SECTION
BRISTOL AVENUE
 STATION 0+90 TO STATION 26+43
 (FRONTAGE ROAD TO CANTERBURY STREET)

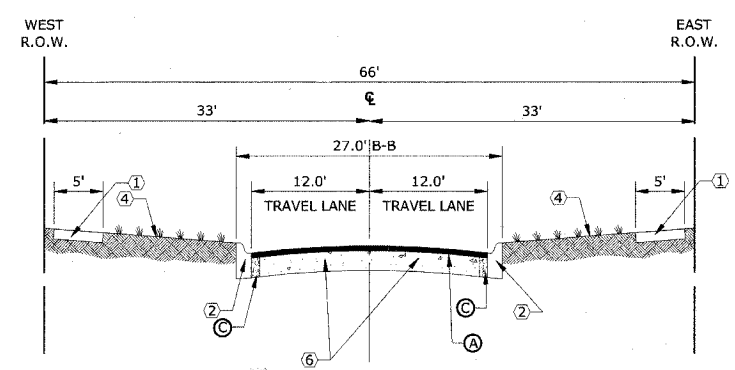


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BRISTOL AVENUE
 STATION 0+90 TO STATION 26+43
 (FRONTAGE ROAD TO CANTERBURY STREET)

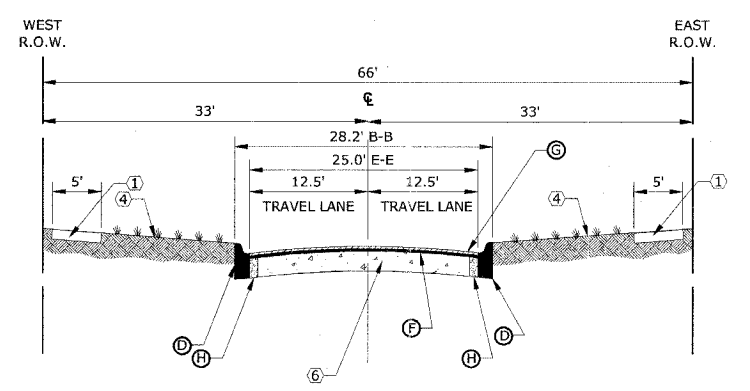


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DOVER STREET
 STATION 13+87 TO STATION 27+00
 (SUFFOLK TO GARDNER ROAD)

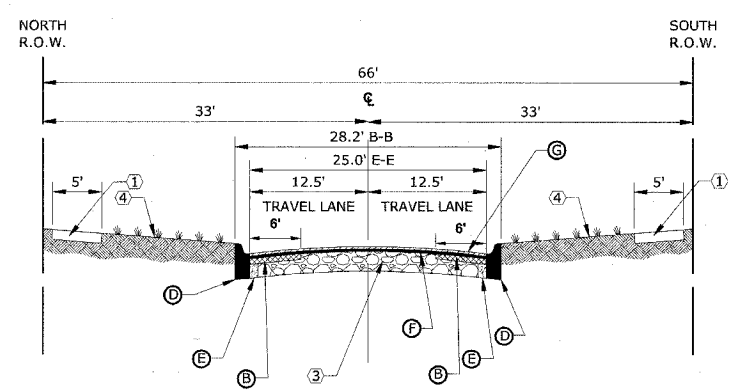
- LEGEND**
- ① EXISTING PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH
 - ② EXISTING COMBINATION CURB AND GUTTER, TYPE B-6.12
 - ③ EXISTING AGGREGATE BASE COURSE TO REMAIN
 - ④ EXISTING SODDED PARKWAY (SOD WILL BE REPLACED ADJACENT TO NEW CONCRETE)
 - ⑤ EXISTING HOT-MIX ASPHALT PAVEMENT TO REMAIN
 - ⑥ EXISTING CONCRETE BASE TO REMAIN
 - Ⓐ HOT-MIX ASPHALT SURFACE REMOVAL (VARIABLE DEPTH)
 - Ⓑ 4" CLASS 'D' PATCH, SPECIAL
 - Ⓒ PAVEMENT REMOVAL
 - Ⓓ PROPOSED COMBINATION CURB AND GUTTER, TYPE B-4.12 (SPECIAL)
 - Ⓔ COMPACTED CA-6 (INCLUDED IN THE COST OF THE CURB AND GUTTER)
 - Ⓕ LEVELING BINDER (MACHINE METHOD), N50 1"
 - Ⓖ HOT-MIX ASPHALT SURFACE COURSE, MIX D, N50, 1 1/2"
 - Ⓗ PORTLAND CEMENT CONCRETE BASE COURSE, 8"



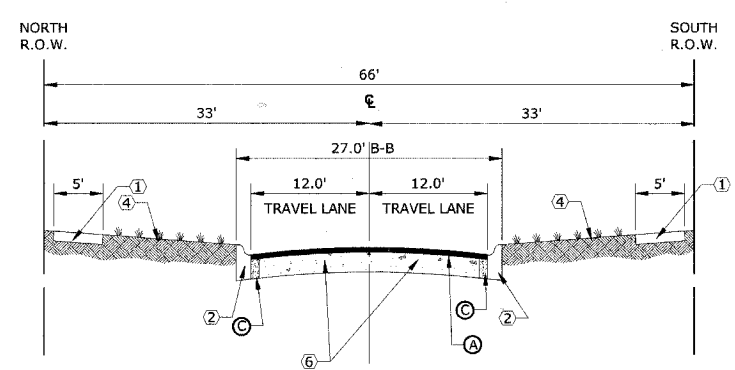
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BRISTOL AVENUE
 STATION 26+43 TO STATION 39+64
 (CANTERBURY STREET TO DICKENS STREET)



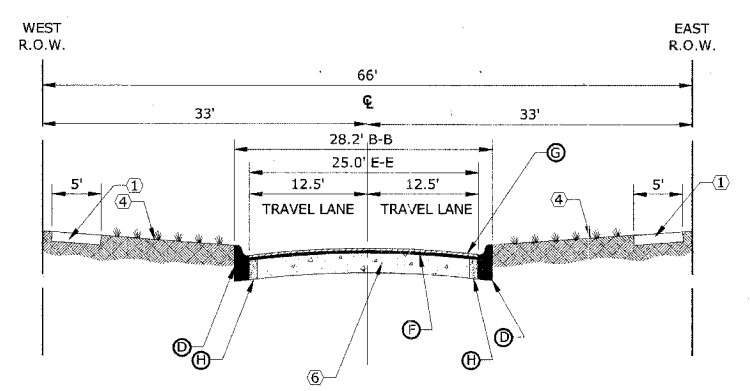
PROPOSED TYPICAL CROSS SECTION
BRISTOL AVENUE
 STATION 26+43 TO STATION 39+64
 (CANTERBURY STREET TO DICKENS STREET)



PROPOSED TYPICAL CROSS SECTION
DOVER STREET
 STATION 13+87 TO STATION 27+00
 (SUFFOLK TO GARDNER ROAD)



EXISTING TYPICAL CROSS SECTION
DOVER STREET
 STATION 0+80 TO STATION 13+87
 (WESTCHESTER BOULEVARD TO SUFFOLK AVENUE)



PROPOSED TYPICAL CROSS SECTION
DOVER STREET
 STATION 0+80 TO STATION 13+87
 (WESTCHESTER BOULEVARD TO SUFFOLK AVENUE)

NOTE: ALL EXISTING AND PROPOSED CROSS SLOPES VARY BETWEEN 2% - 6%.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
MIXTURE TYPE	AC TYPE	AIR VOIDS
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2" (IL - 9.5mm)	PG 64-22	4% @ 50 GYR.
LEVELING BINDER MACHINE METHOD, N50, 1" (IL - 9.5mm)	PG 64-22 *	4% @ 50 GYR.
CLASS D PATCHES, TYPE I, II, III AND IV, 4", (BINDER IL-19 MM)	PG 64-22 *	4% @ 70 GYR.
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 4" (FOR USE IN RECONSTRUCTS)	PG 64-22 *	4% @ 50 GYR.

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQYD/IN.
 * WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22

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ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
---	---	COOK	22	6
ILLINOIS PROJECT		HPP 2902 (002)		
VILLAGE SECTION		06-00071-00-FP		
CONTRACT NO. 83983				

GENERAL NOTES

DEPRESSED CURBS - THE TOP OF CURBS SHALL BE DEPRESSED WHERE THE CURB AND GUTTER IS CONSTRUCTED AT HANDICAP ACCESSIBLE SIDEWALK RAMPS AT ALLEY RETURNS AND STREET INTERSECTIONS, AND FOR PRIVATE DRIVES AND AS DIRECTED BY THE ENGINEER

DRAINAGE OPENINGS - AT ALL LOCATIONS WHERE CASTINGS ARE TO BE INCORPORATED IN THE CURB AND GUTTER, A 3/4" EXPANSION JOINT SHALL BE INSTALLED IN THE CURB AND GUTTER A DISTANCE OF 5 FT. FROM EACH SIDE OF THE CASTING. 2-NO. 4 RE-BARS, 9" IN LENGTH, SHALL BE INCORPORATED IN THE CONTINUOUS PORTION OF CONCRETE CURB BEHIND THE CASTING.

SLIPFORM CONSTRUCTION - VERTICAL FACES MAY BE BATTERED AT THE RATE OF 3/4" PER FOOT OF HEIGHT TO AID IN SLIPFORM OPERATIONS.

DEPRESSED CURB HEIGHT - THE HEIGHT OF THE DEPRESSED CURB SHALL BE 1-1/2" AT DRIVEWAYS. SEE IDOT STANDARD 424001-04 FOR HEIGHT AT SIDEWALK RAMP.

BITUMINOUS EXPANSION JOINTS - THREE QUARTER INCH (3/4") BITUMINOUS PREMOLDED INORGANIC FIBER EXPANSION JOINTS SHALL BE INSTALLED WHERE NEW SIDEWALK OR CURB AND GUTTER OR DRIVEWAY PAVEMENT ABUTS AN EXISTING CONCRETE WALK, DRIVE, OR CURB WHICH IS TO REMAIN IN PLACE, AND AT NOT LESS THAN NINETY FOOT (90') INTERVALS AT LOCATIONS WHERE CURB REPLACEMENT IS IN EXCESS OF NINETY FEET (96'); AT RADIUS POINTS, AT BOTH SIDES OF FRAMES AND GRATES WHICH FALL IN THE CURB; AND AS DIRECTED BY THE ENGINEER.

ALL EXPANSION JOINTS LOCATED IN THE CURB AND GUTTER SHALL HAVE TWO (2) THREE QUARTER INCH (3/4") DIAMETER, SMOOTH, ROUND, EPOXY COATED DOWEL BARS, EIGHTEEN INCHES (18") IN LENGTH, WITH GREASED PLASTIC END CAPS INSERTED TO ALLOW THE CURB AND GUTTER TO EXPAND AND CONTRACT LATERALLY. CONTRACTION JOINTS SHALL BE TOOLED INTO THE CURB AND GUTTER AT INTERVALS NOT TO EXCEED FIFTEEN FEET (15'). THESE CONTRACTION JOINTS SHALL BE SAW CUT TO A DEPTH OF TWO INCHES (2") WITHIN TWENTY-FOUR (24) HOURS OF CONCRETE PLACEMENT. THE COST OF THE ABOVE WORK SHALL BE INCLUDED IN THE RESPECTIVE ITEMS FOR CONCRETE INSTALLATION.

IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PROTECT EXISTING STREET LIGHT CABLES. THE CONTRACTOR WILL BE REQUIRED TO RESTORE WIRES AND CONDUIT IF THEY ARE DAMAGED BY HIS WORK. THE CONTRACTOR WILL NOT BE ALLOWED TO SPLICE ANY WIRES THAT ARE REPAIRED.

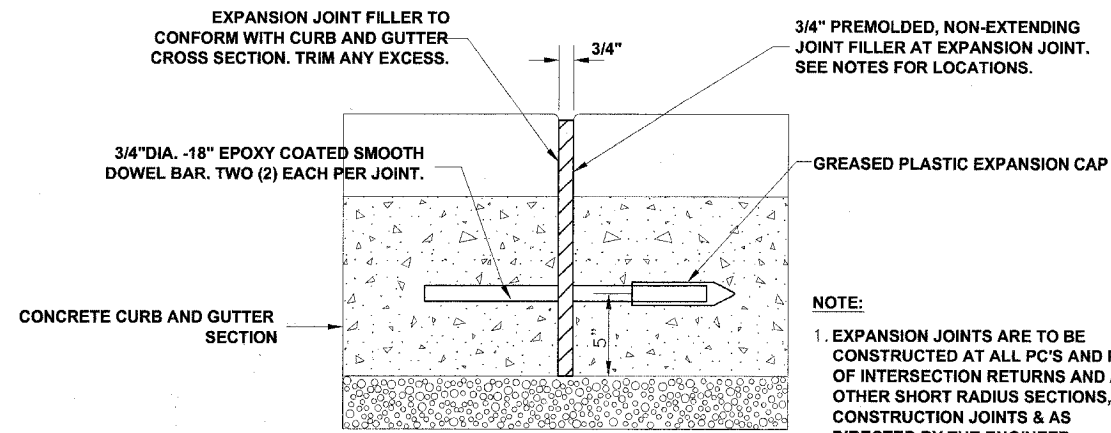
THE VILLAGE WILL PASS OUT A LETTER REQUESTING THAT ALL RESIDENTS WITH SPRINKLER SYSTEMS IN THE PARKWAY NOTIFY THE VILLAGE PRIOR TO CONSTRUCTION. THE VILLAGE WILL THEN MARK THE SPRINKLER HEADS ON THESE SYSTEMS. THE CONTRACTOR WILL BE RESPONSIBLE FOR ALL COSTS FOR REPAIRING ANY DAMAGE THAT IS DONE TO EXISTING SPRINKLER SYSTEMS THAT HAVE BEEN LOCATED BY THE VILLAGE PRIOR TO THE DAMAGE BEING DONE.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO TAKE PRECAUTIONS SO AS NOT TO DAMAGE EXISTING SIDEWALKS, DRIVEWAYS, AND PAVEMENTS OUTSIDE THE LIMITS OF RESTORATION. THE GENERAL LIMITS OF RESTORATION ARE SHOWN ON THE PLANS. THE ENGINEER WILL MARK OUT THE EXACT LIMITS OF REMOVAL FOR THESE ITEMS PRIOR TO THE COMMENCEMENT OF WORK. THE CONTRACTOR SHALL BE RESPONSIBLE FOR RESTORING ANY DAMAGE TO SIDEWALKS, DRIVEWAYS, AND PAVEMENTS OUTSIDE OF THESE LIMITS TO THE SATISFACTION OF THE ENGINEER AND THE VILLAGE AT HIS OWN EXPENSE.

THE COSTS FOR REMOVAL OF ANY ASPHALT OVERLAY THAT EXTENDS INTO THE GUTTER PORTION OF THE CURB AND GUTTER WILL BE INCLUDED IN THE PRICE FOR COMBINATION CURB AND GUTTER REMOVAL.

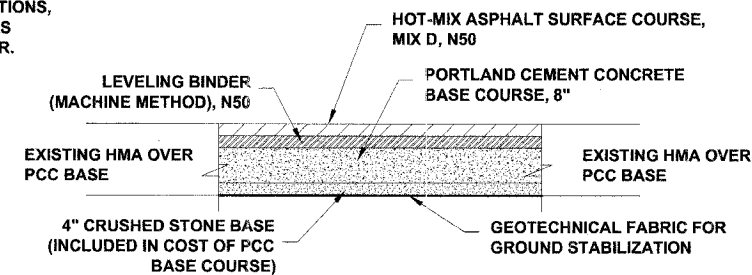
A 2" X 6" BOARD WILL BE USED AS A FORM FOR ALL SIDEWALKS TO BE INSTALLED FIVE INCHES (5") IN THICKNESS. A 2" X 8" BOARD WILL BE USED AS A FORM FOR ALL SIDEWALKS AND DRIVEWAYS TO BE INSTALLED SEVEN INCHES (7") IN THICKNESS. ALL FORMS MUST BE A MINIMUM HEIGHT OF THE PROPOSED THICKNESS OF THE RESPECTIVE CONCRETE ITEM TO BE INSTALLED.

THE CONTRACTOR SHALL HAVE WATCHMEN AT THE JOB SITE AFTER CONCRETE HAS BEEN POURED UNTIL THE CONCRETE HAS CURED SUFFICIENTLY SO THAT NO GRAFFITI CAN BE INSCRIBED IN THE NEWLY POURED CONCRETE SURFACES. THE EXPOSED FACES OF CONCRETE OF ANY ITEMS THAT ARE DEFACED SHALL BE REPLACED BY THE CONTRACTOR AT HIS OWN EXPENSE.

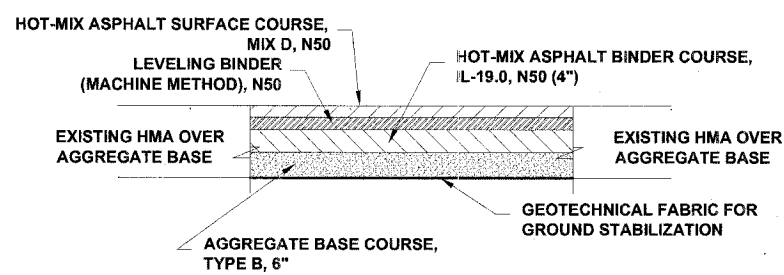


TYPICAL CURB AND GUTTER EXPANSION JOINT

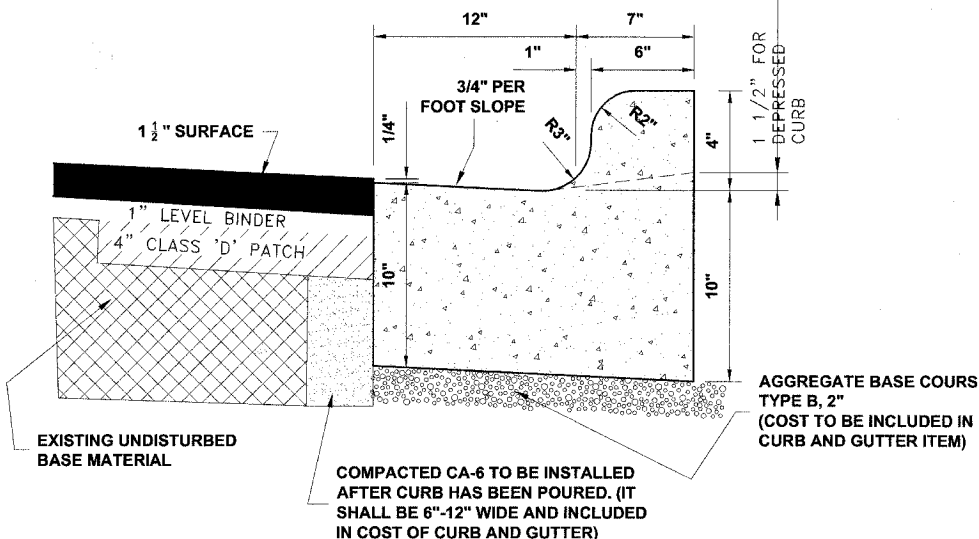
NOTE:
1. EXPANSION JOINTS ARE TO BE CONSTRUCTED AT ALL PC'S AND PT'S OF INTERSECTION RETURNS AND ALL OTHER SHORT RADIUS SECTIONS, CONSTRUCTION JOINTS & AS DIRECTED BY THE ENGINEER.



CONCRETE BASE

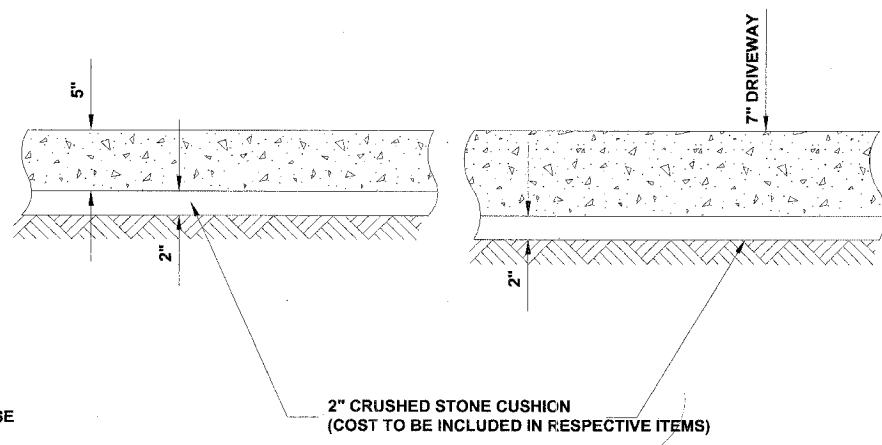


AGGREGATE BASE

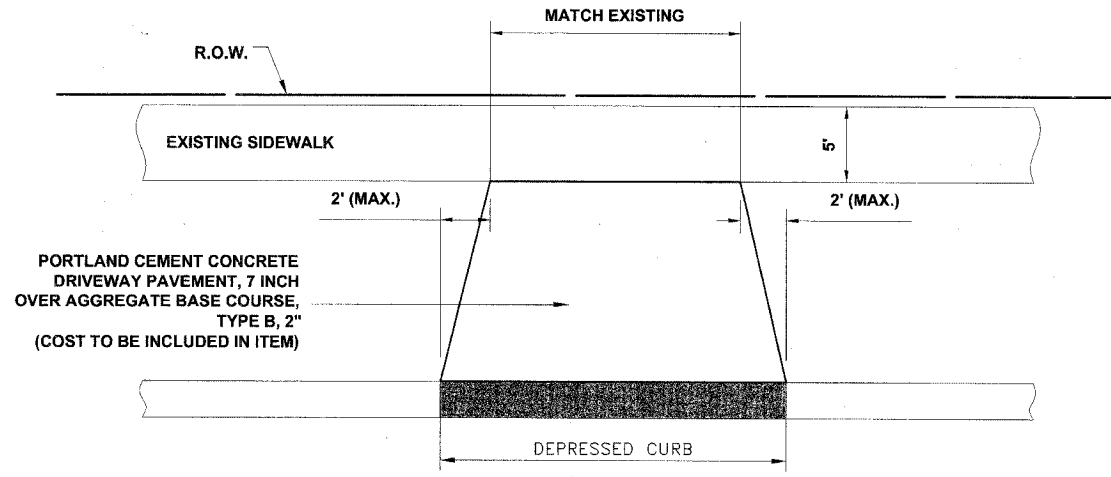


COMBINATION CONCRETE CURB & GUTTER TYPE B-4.12 (ON AGGREGATE BASE PORTIONS OF IMPROVEMENTS)

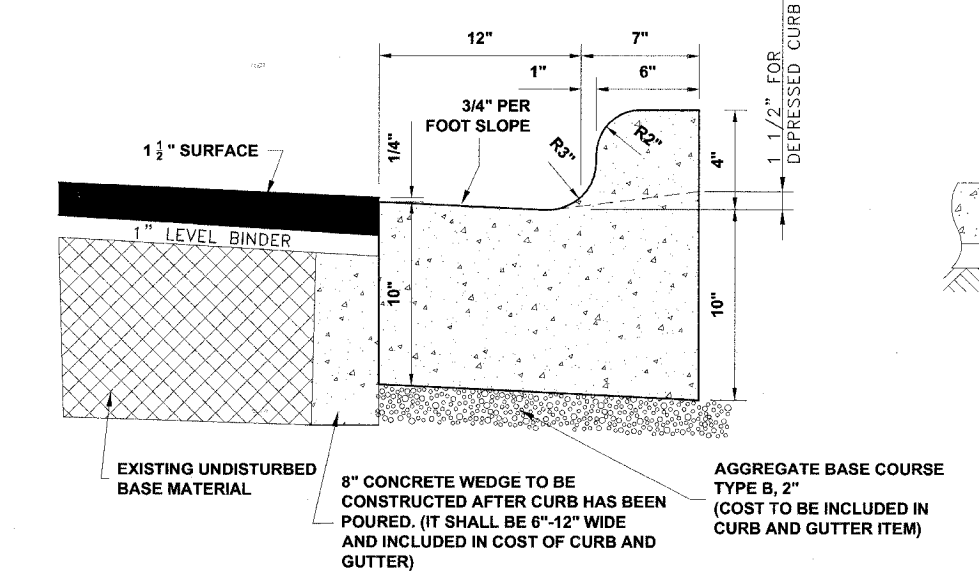
TYPICAL PAVEMENT RECONSTRUCT SECTION (IN AREAS OF EXCESSIVE LOWERING)



P.C.C. SIDEWALK & DRIVEWAY CROSS SECTION



DRIVEWAY DETAIL



COMBINATION CONCRETE CURB & GUTTER TYPE B-4.12 (ON CONCRETE BASE PORTIONS OF IMPROVEMENTS)

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HIGH PRIORITY PROJECT HPP-2902 (002)
BRISTOL AVENUE & DOVER STREET
VILLAGE OF WESTCHESTER, ILLINOIS

ROADWAY DETAILS

SCALE:	NONE	SHEET 6 OF 22
DRAWN BY:	ECW/LEV	
BOOK NO.:	1541	
DATE:	10-2-07	
REVISION:	E.H.E. NO.: 838-06-38501	

DRAINAGE AND UTILITIES NOTES

THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, COMBINED SEWERS, TELEPHONE LINES, COMMUNICATION LINES, ELECTRIC LINES, GAS MAINS, AND WATER SERVICES ARE APPROXIMATE AND THEIR SPECIFIC LOCATIONS ARE TO BE DETERMINED IN THE FIELD AT NO COMPENSATION TO THE CONTRACTOR.

COORDINATION OF ALL UTILITY WORK INVOLVED WITHIN THE CONSTRUCTION AREAS SHALL BE SUBJECT TO DISCUSSION AND CLARIFICATION AT A PRECONSTRUCTION MEETING.

WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE OR PUBLIC DRAINAGE STRUCTURES OR SEWERS UNTIL PERMANENT CONNECTIONS TO SEWERS ARE BUILT AND IN SERVICE. THIS WORK SHALL NOT BE PAID FOR DIRECTLY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

WHEN, DURING CONSTRUCTION OPERATIONS, ANY LOOSE MATERIALS ARE DEPOSITED IN THE FLOW LINES OF GUTTERS OR DRAINAGE STRUCTURES SO THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, THE OBSTRUCTING MATERIALS SHALL BE REMOVED AT THE CLOSE OF EACH WORK DAY. AT THE CONCLUSION OF THE CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES ARE TO BE FREE OF ALL DIRT, DEBRIS, AND OBSTRUCTING MATERIALS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCIDENTAL TO THE CONTRACT.

ALL COSTS INVOLVED IN CONNECTING OF PROPOSED STORM SEWERS AND STORM STRUCTURES TO EXISTING STORM SEWERS OR PROPOSED STORM SEWERS SHALL BE INCIDENTAL TO THE COST OF THE CONTRACT.

ALL PROPOSED WATER MAINS SHALL BE INSTALLED IN ACCORDANCE WITH IN THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS", FIFTH EDITION, DATED 1996, AND ALL REVISIONS THERETO.

THE CONTRACTOR SHALL VERIFY THE TYPE OF ALL WATER MAIN HARDWARE INCLUDING VALVES, FIRE HYDRANTS, VALVE BOXES, CORPORATION STOPS, CURB STOPS, AND WATER SERVICES BOXES WITH THE UTILITY SUPERINTENDENT PRIOR TO ORDERING SUCH MATERIAL.

ANY COSTS FOR SHEETING OR SHORING REQUIRED FOR THE STORM SEWER INSTALLATION OR OTHER CONSTRUCTION ELEMENTS REQUIRING RELATIVELY DEEP EXCAVATIONS SHALL BE INCLUDED IN THE PARTICULAR PAYMENT ITEM AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR ANY SUPPLEMENTAL WORK ASSOCIATED WITH THE MAINTENANCE OF TRENCH SIDES OR OTHER EXCAVATED AREAS.

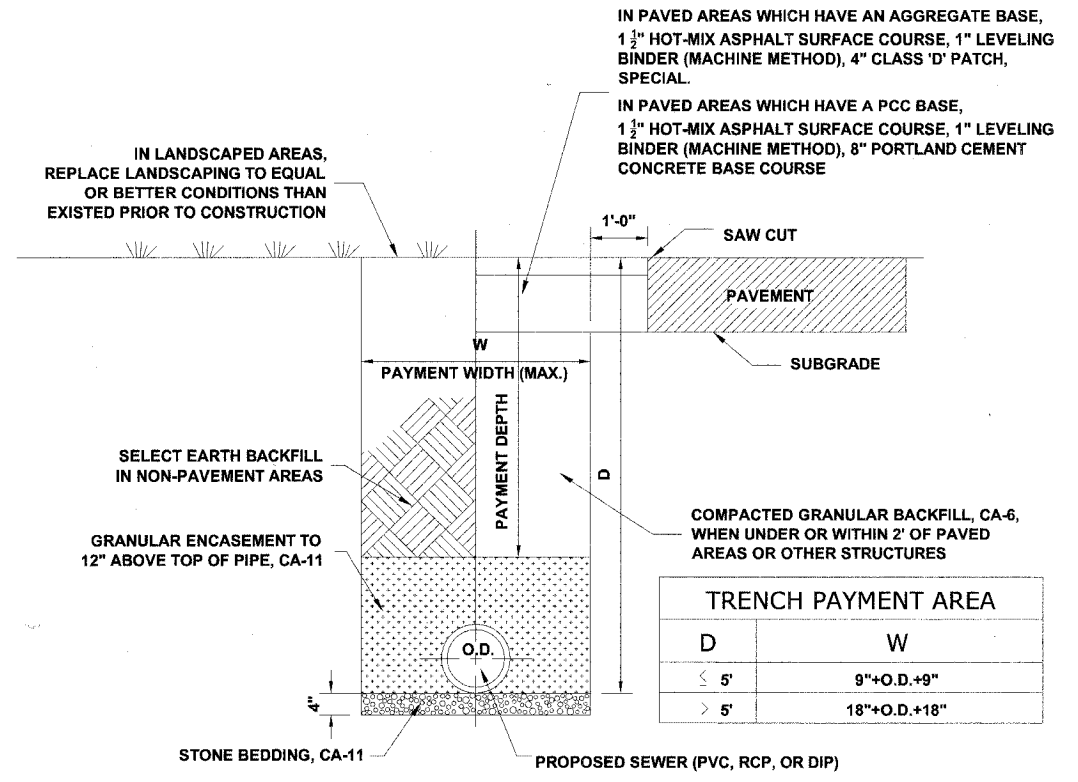
SEWER PIPE INSTALLED ON THIS PROJECT SHALL CONFORM TO THE FOLLOWING STANDARD:

TYPE OF PIPE	MATERIAL STANDARD	JOINT STANDARD
REINFORCED CONCRETE PIPE	ASTM C-76, CLASS III	ASTM C-443
POLYVINYLCHLORIDE PIPE (6"-12")	ASTM D-2241	ASTM D-3139
POLYVINYLCHLORIDE PIPE (15")	ASTM D-3034	ASTM D-3212

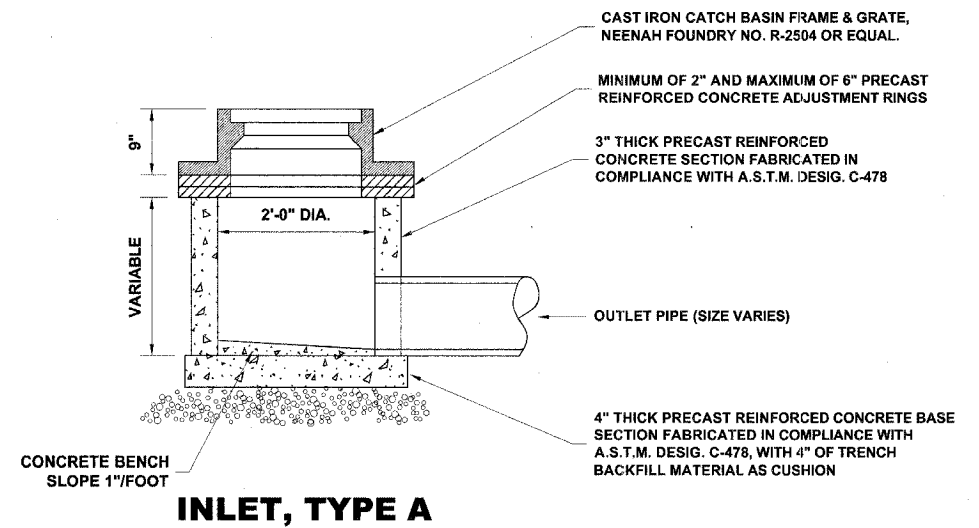
FINAL ADJUSTMENT OF ALL STRUCTURES IN THE PAVEMENT, INCLUDING THOSE IN THE PROPOSED BASE COURSE, SHALL NOT BE COMPLETED UNTIL AFTER THE PLACEMENT OF THE HOT-MIX ASPHALT BINDER COURSE.

THE AREA WITHIN THE ROUNDOUT SHALL BE FILLED WITH CONCRETE SET TO THE ELEVATION OF THE TOP OF THE PROPOSED BINDER. COST IS TO BE INCLUDED IN THE RESPECTIVE PAY ITEM FOR THE NEW, ADJUSTED OR RECONSTRUCTED STRUCTURE.

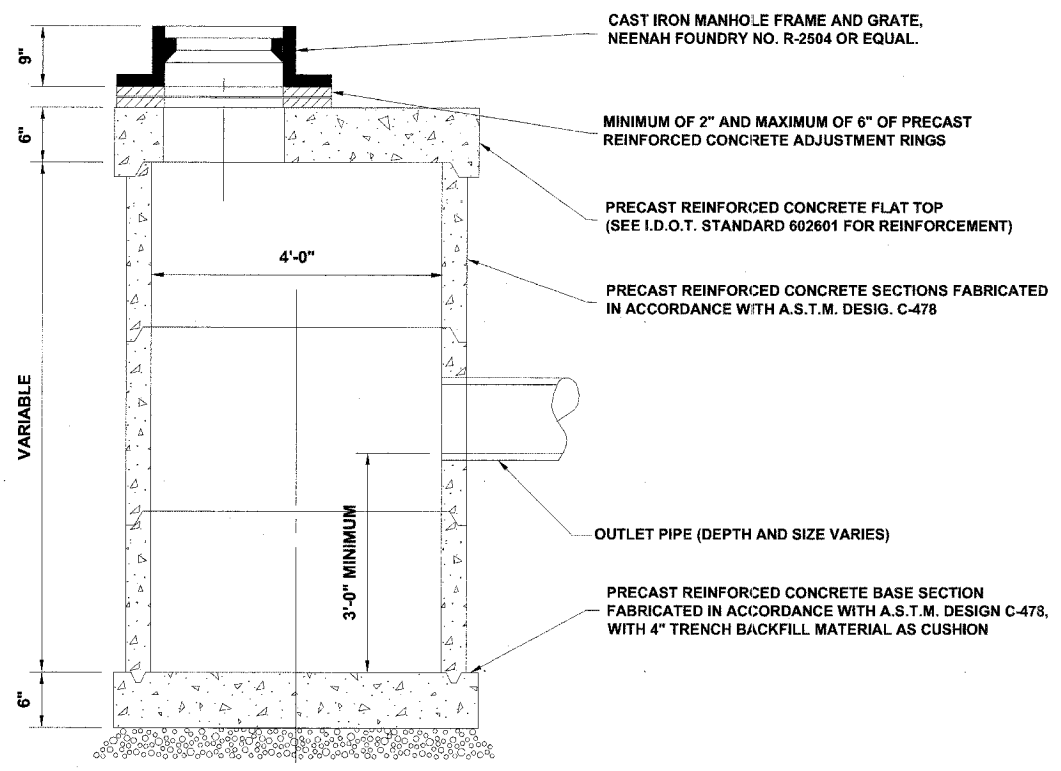
ALL NEW STRUCTURES AND STRUCTURES TO BE RECONSTRUCTED SHALL BE BUILT WITH PRECAST REINFORCED CONCRETE SECTIONS FABRICATED IN ACCORDANCE WITH A.S.T.M. DESIGNATION C-478.



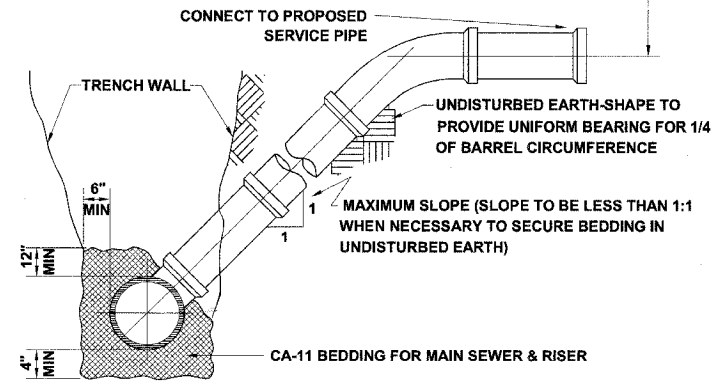
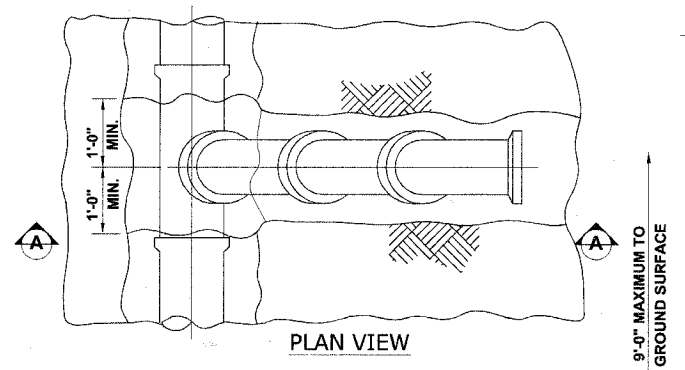
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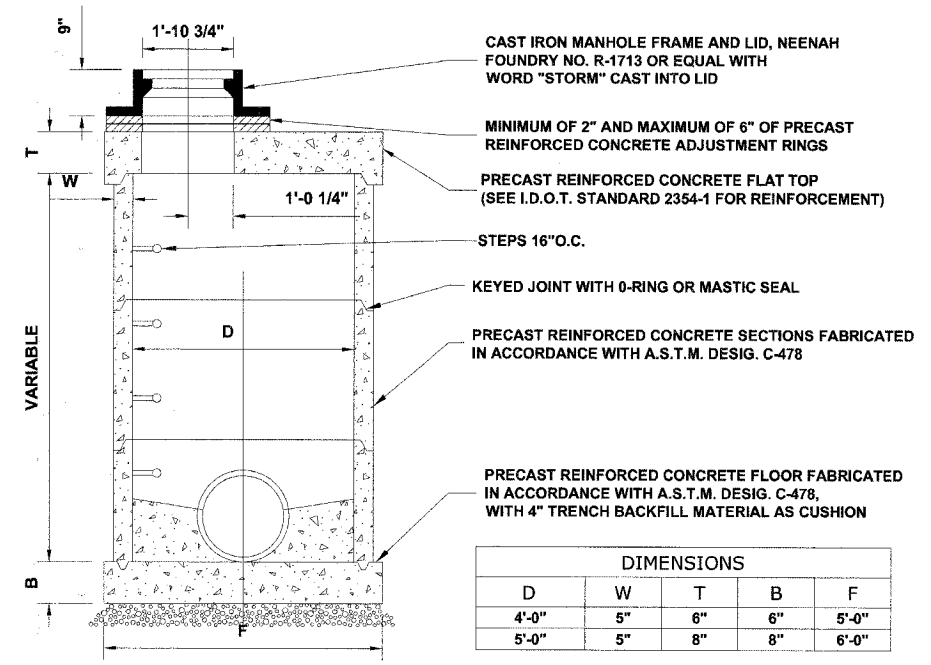
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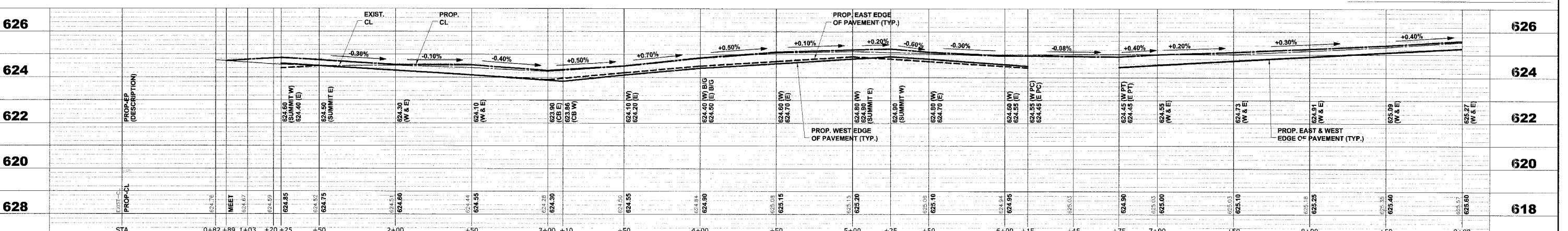
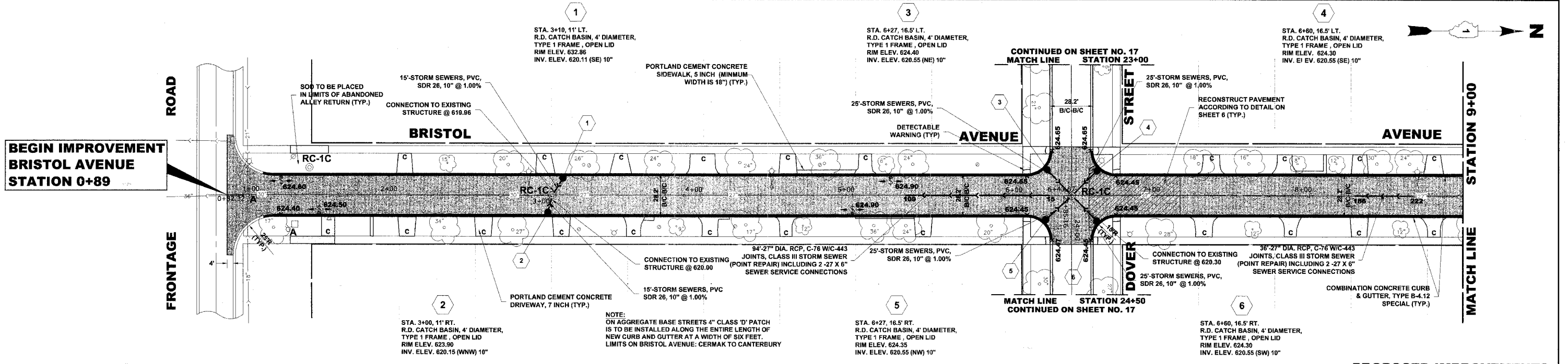
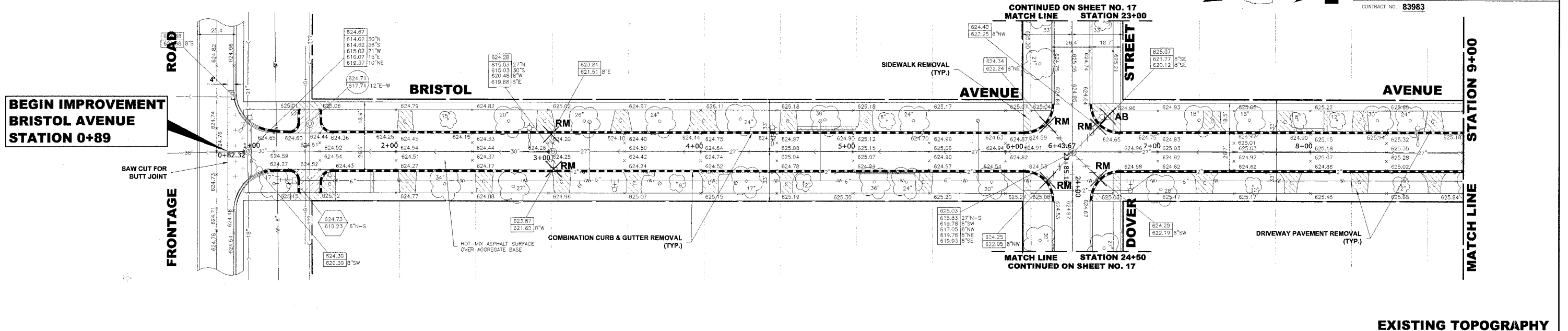
RESTRICTED DEPTH CATCH BASIN



TYPICAL RISER FOR SERVICE LATERAL



RESTRICTED DEPTH STORM MANHOLE



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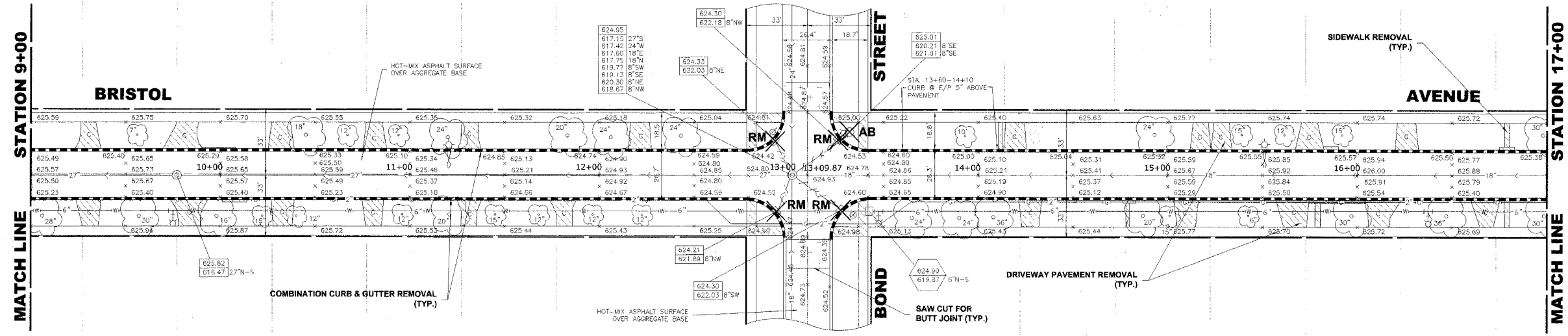
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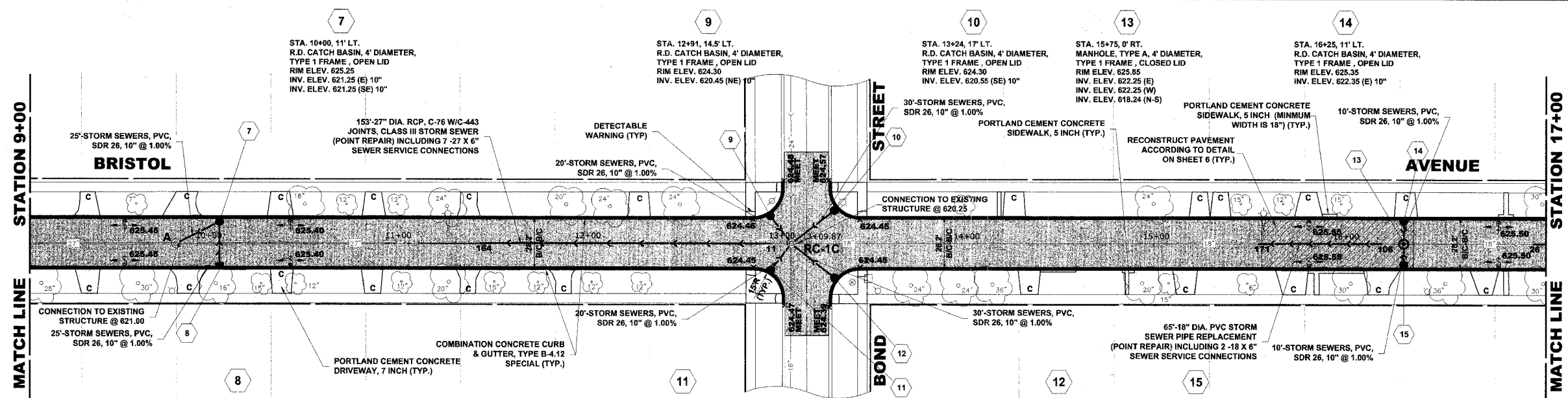
HIGH PRIORITY PROJECT HPP-2902 (002)
BRISTOL AVENUE & DOVER STREET
VILLAGE OF WESTCHESTER, ILLINOIS

BRISTOL AVENUE
PLAN & PROFILE

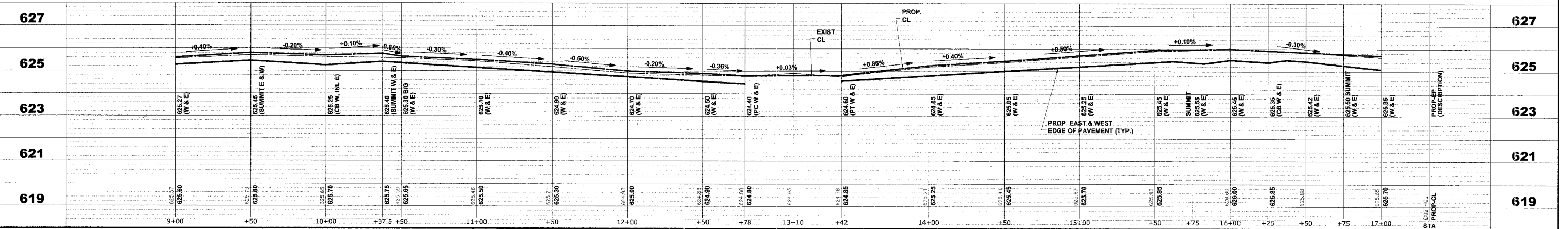
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BOOK NO.: 1541	22
DATE: 10-2-07	
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EXISTING TOPOGRAPHY



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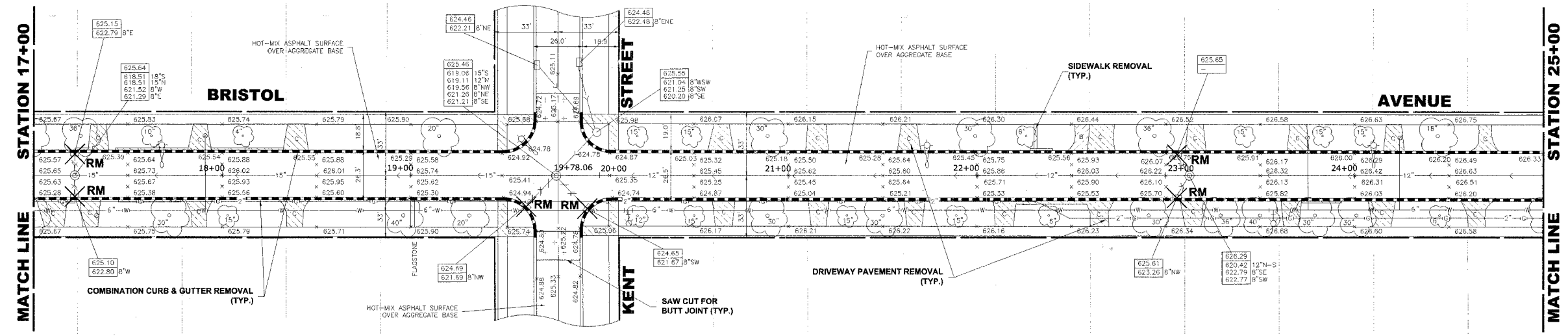
HIGH PRIORITY PROJECT HPP-2902 (002)
BRISTOL AVENUE & DOVER STREET
VILLAGE OF WESTCHESTER, ILLINOIS

BRISTOL AVENUE
PLAN & PROFILE

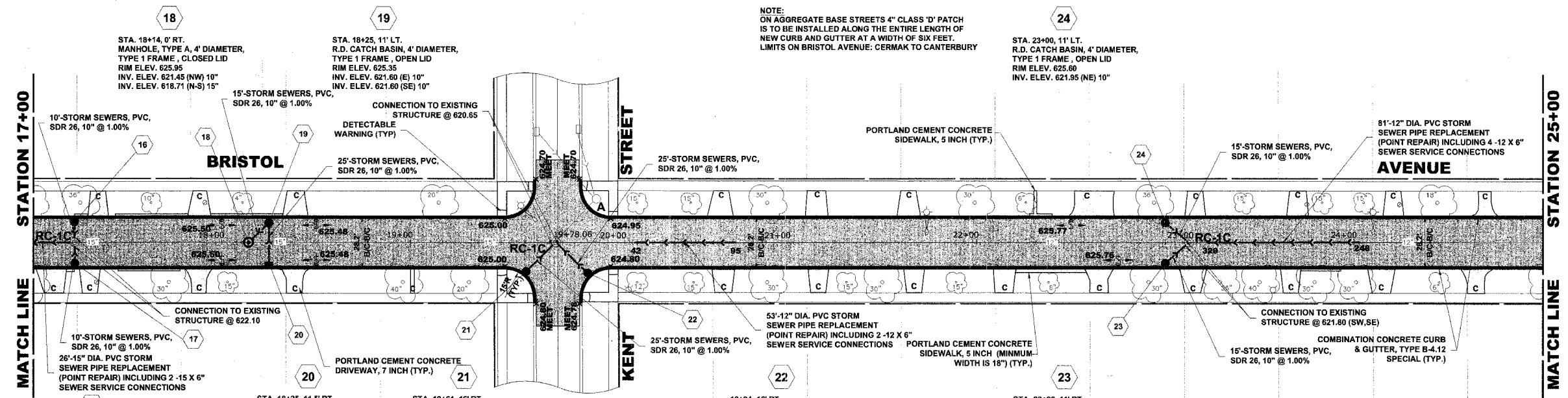
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REVISION: _____
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SHEET **10** OF **22**

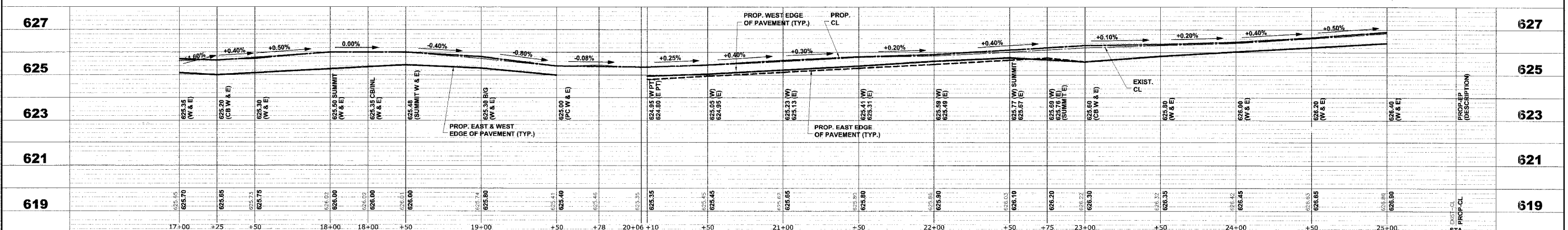
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VILLAGE OF WESTCHESTER, ILLINOIS

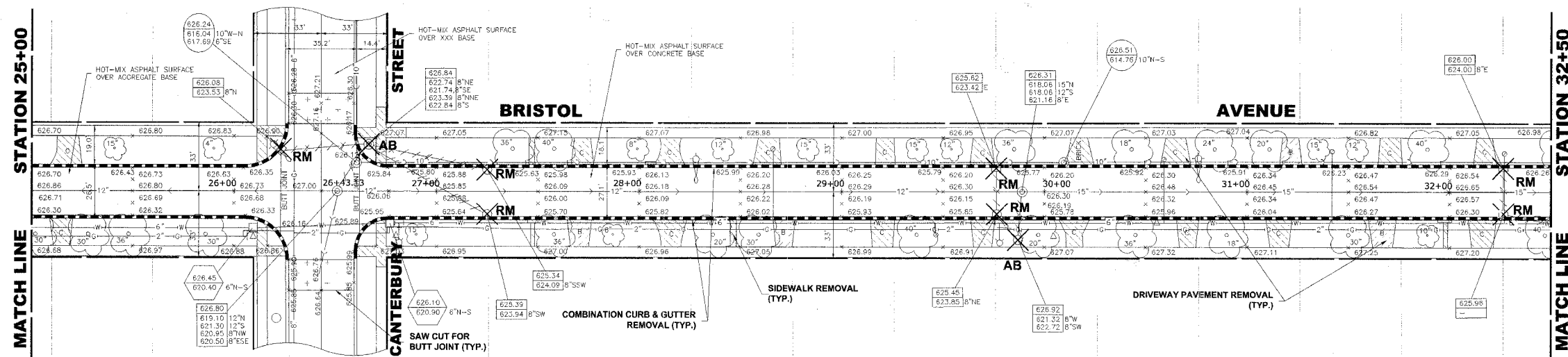
BRISTOL AVENUE
PLAN & PROFILE

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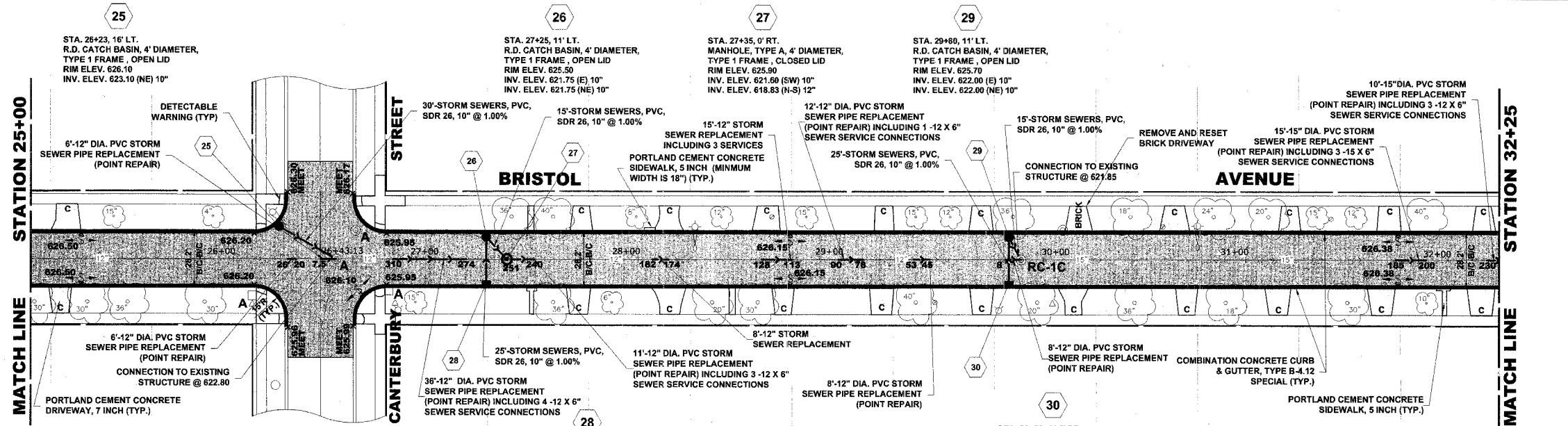
REVISION:

SHEET 11 OF 22

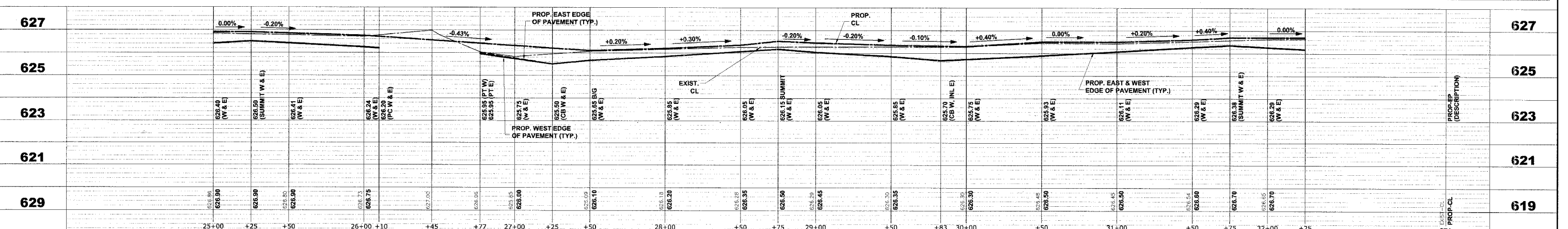
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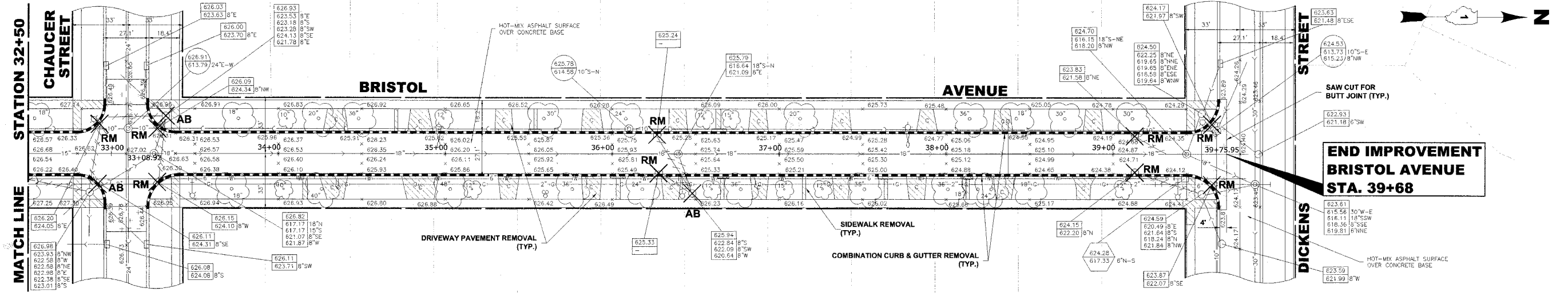
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BRISTOL AVENUE & DOVER STREET
VILLAGE OF WESTCHESTER, ILLINOIS

BRISTOL AVENUE
PLAN & PROFILE

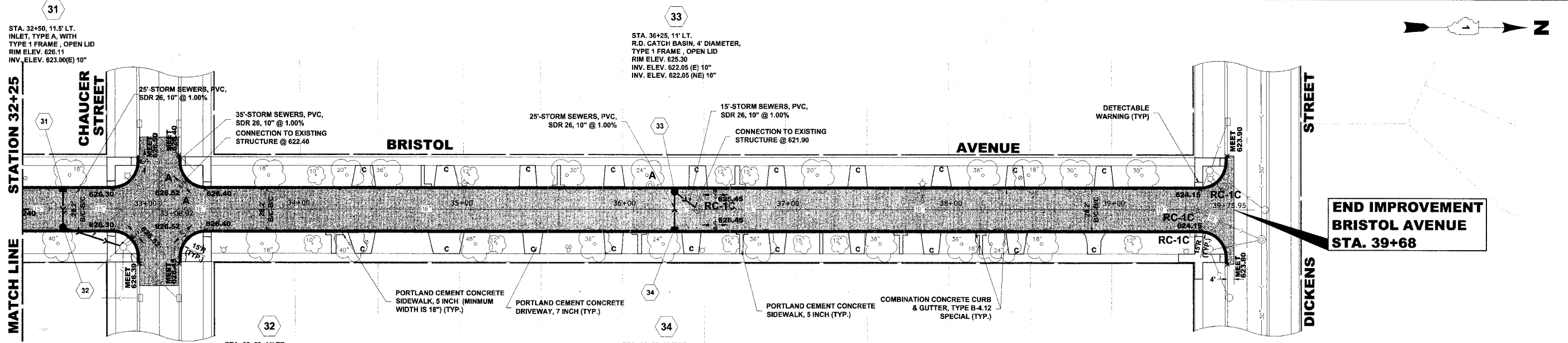
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DATE: 10-2-07
E.H.E. NO.: 838-06-38501

SHEET 12 OF 22

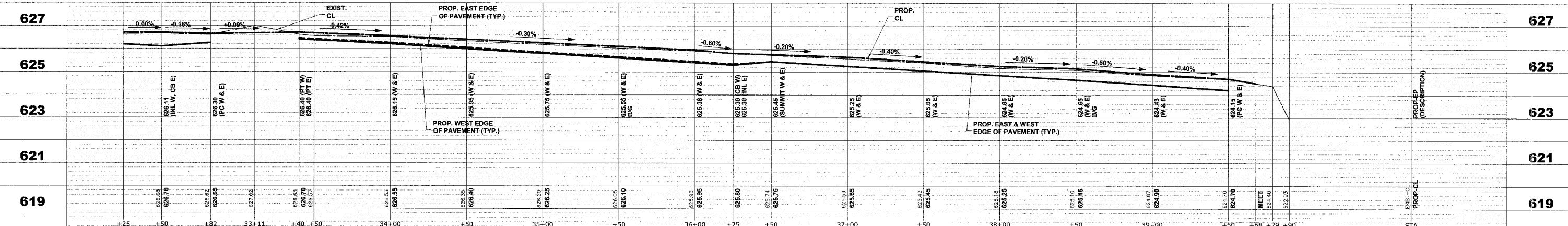
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PROPOSED IMPROVEMENTS



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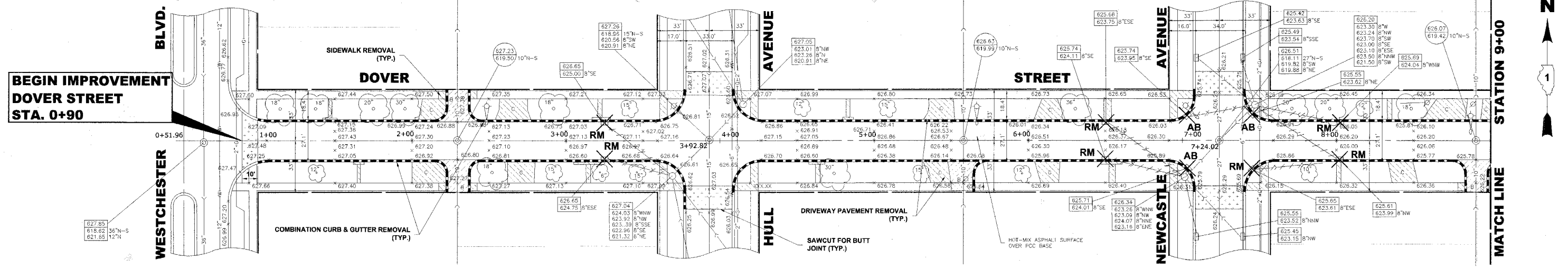
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Phone: 708/865-0300 Fax: 708/865-1122

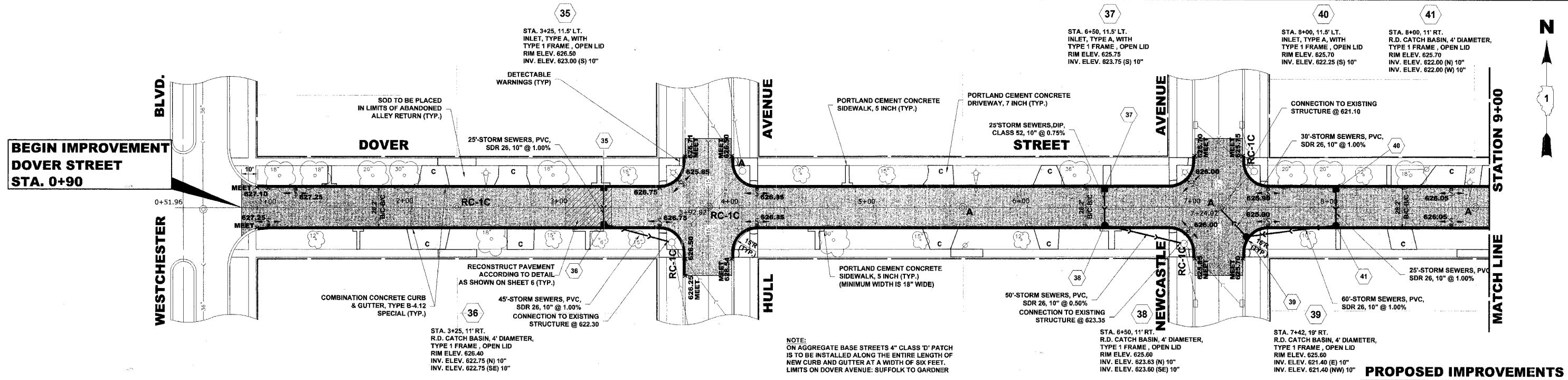
HIGH PRIORITY PROJECT HPP-2902 (002)
BRISTOL AVENUE & DOVER STREET
VILLAGE OF WESTCHESTER, ILLINOIS

BRISTOL AVENUE
PLAN & PROFILE

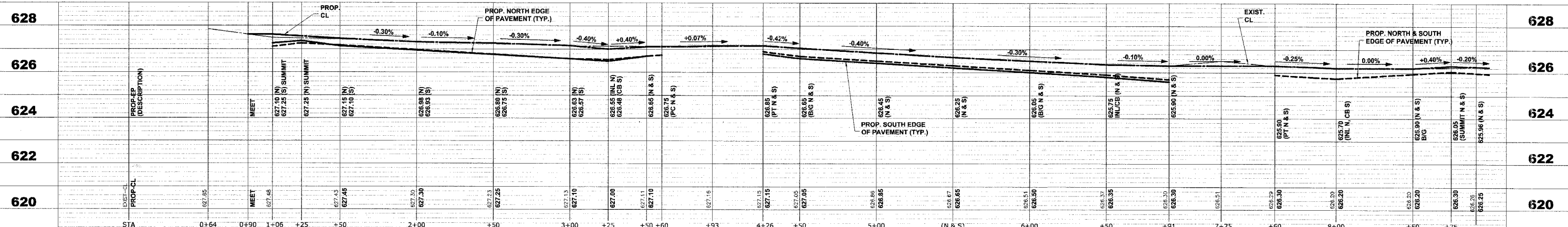
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BOOK NO.: 1541	22
DATE: 10-2-07	
E.H.E. NO.: 838-06-38501	



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PROPOSED IMPROVEMENTS



Drawing file: W:\Projects\33060293 - High Priority Project Bristol & Dover\Bristol And Dover.dwg Oct 03, 2007 11:02am

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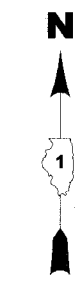
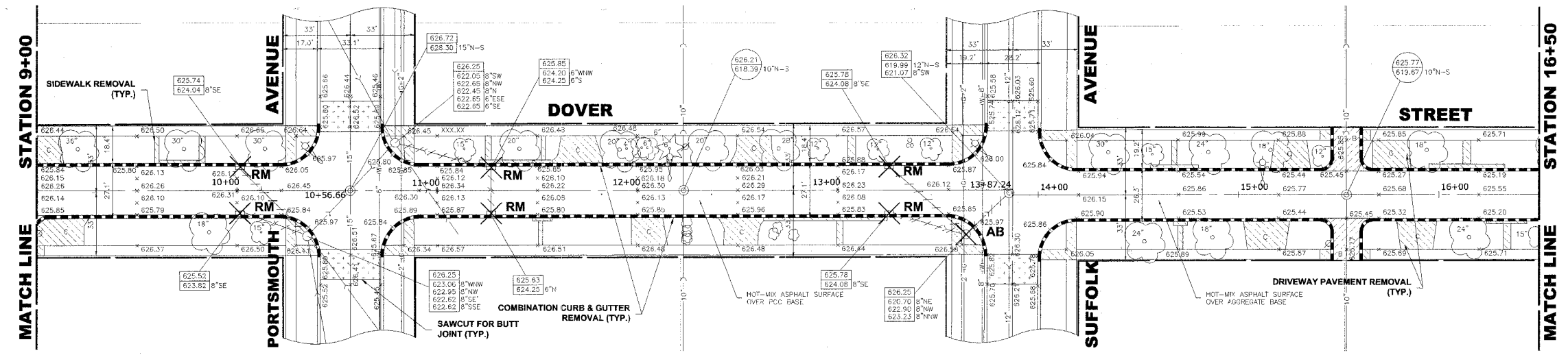
9933 Roosevelt Road
Westchester, Illinois 60154-2780
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Fax: 708/865-1212

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BRISTOL AVENUE & DOVER STREET
VILLAGE OF WESTCHESTER, ILLINOIS

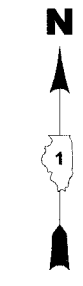
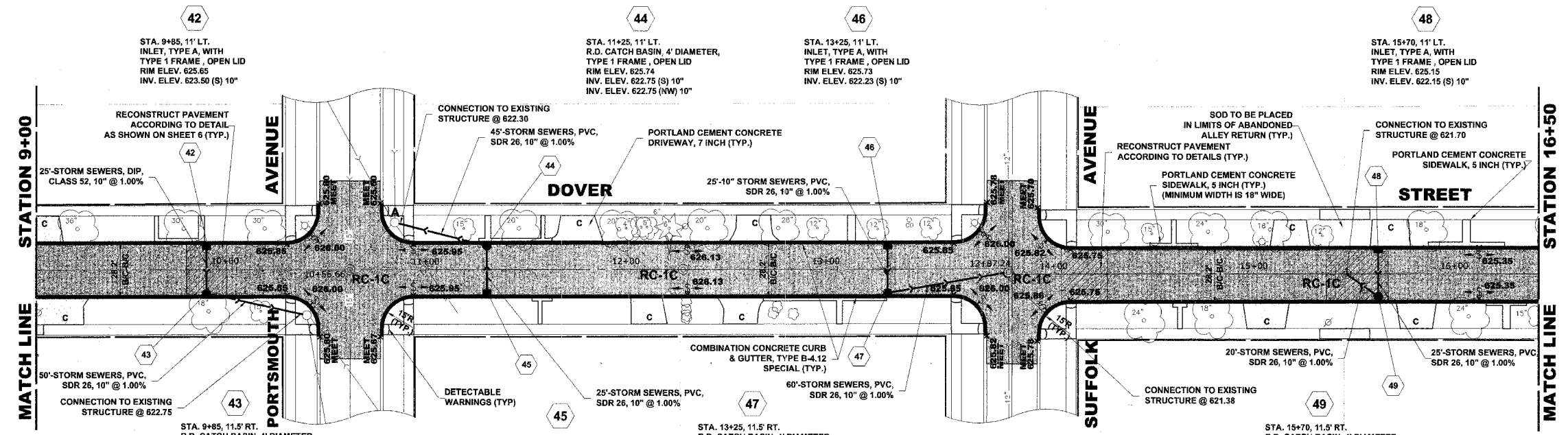
DOVER STREET
PLAN & PROFILE

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BOOK NO.: 1541
DATE: 10-2-07
REVISION:
E.H.E. NO.: 838-06-38501

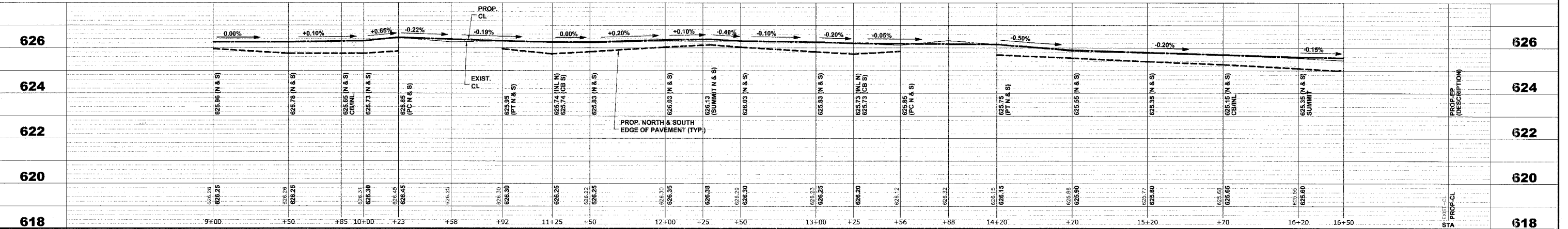
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EXISTING TOPOGRAPHY



PROPOSED IMPROVEMENTS



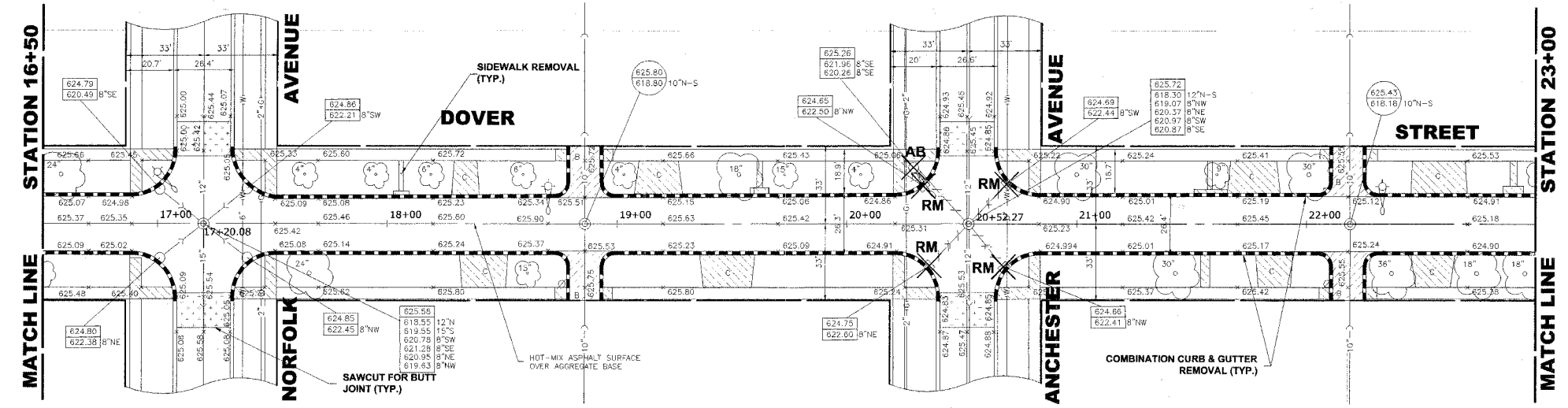
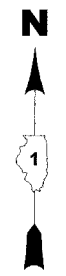
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BRISTOL AVENUE & DOVER STREET
VILLAGE OF WESTCHESTER, ILLINOIS

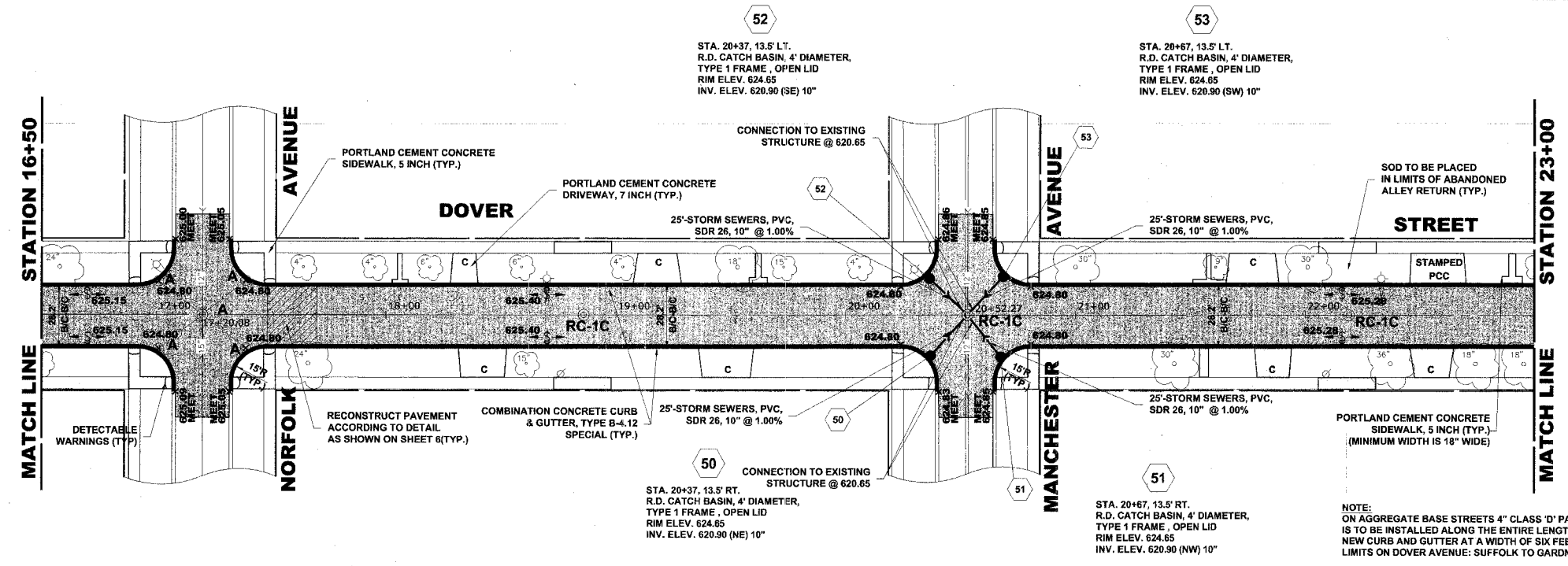
DOVER STREET
PLAN & PROFILE

SCALE: 1"=30'H. & 1"=2'V.	SHEET 15
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DATE: 10-2-07	
E.H.E. NO.: 838-06-38501	

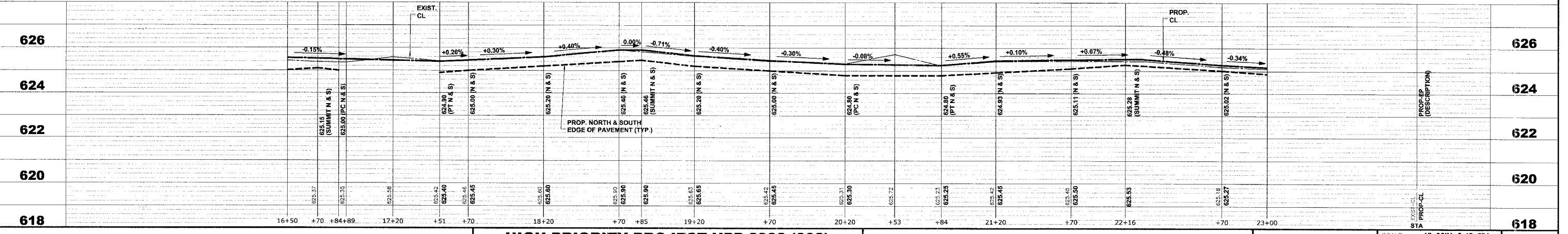
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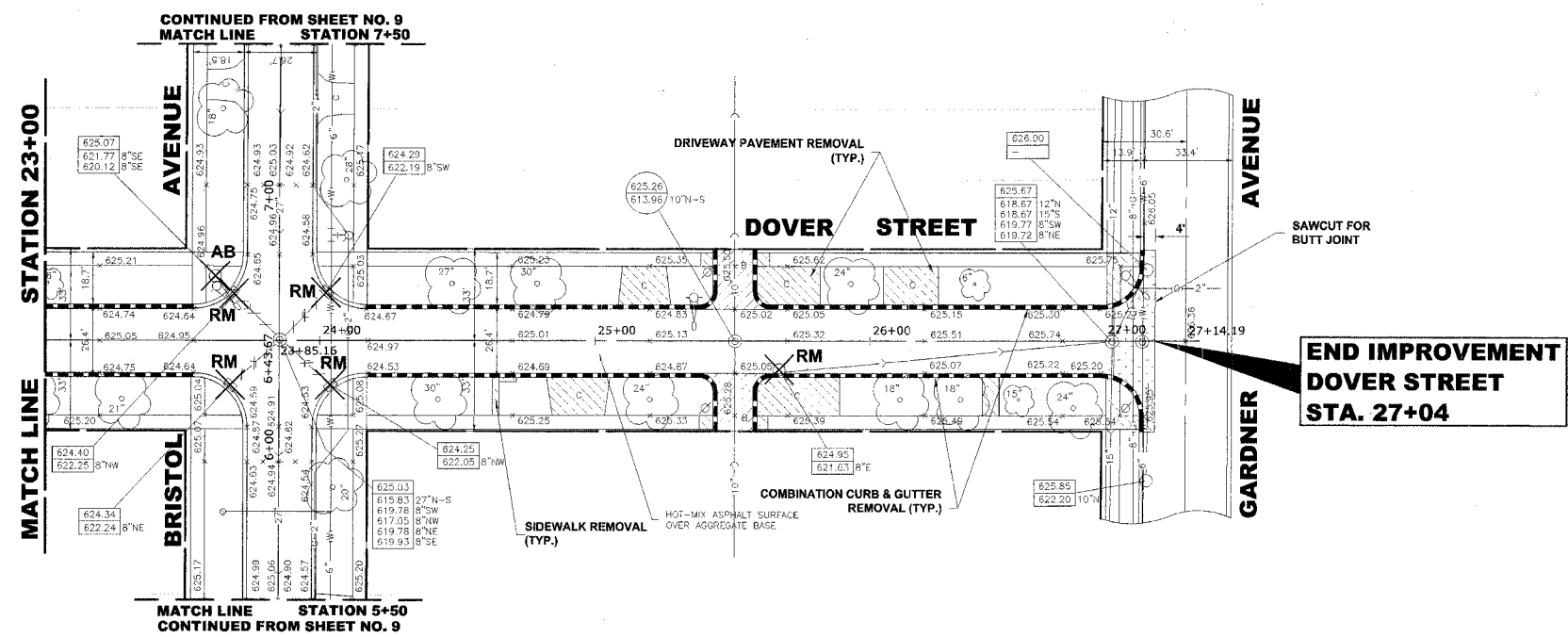
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Westchester, Illinois 60154-2780
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Fax: 708/865-1212

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BRISTOL AVENUE & DOVER STREET
VILLAGE OF WESTCHESTER, ILLINOIS

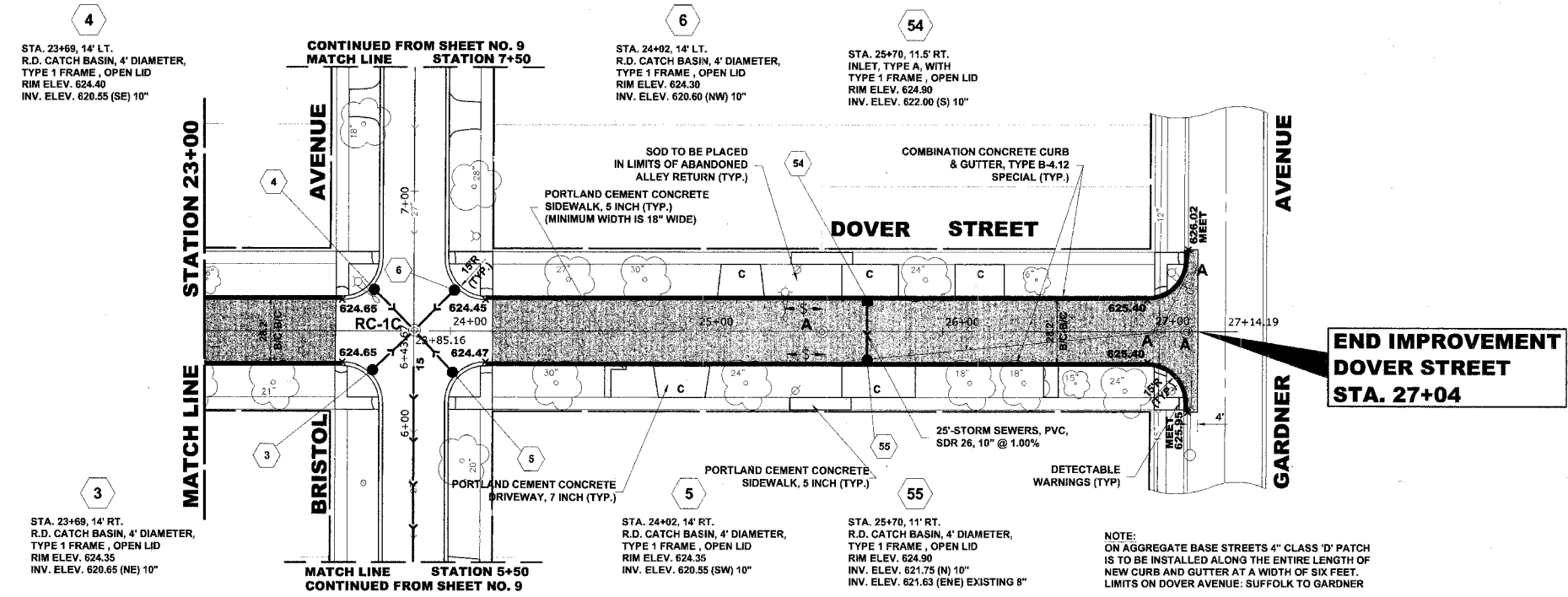
DOVER STREET
PLAN & PROFILE

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BOOK NO.: 1541	22
DATE: 10-2-07	
E.H.E. NO.: 838-06-38501	OF

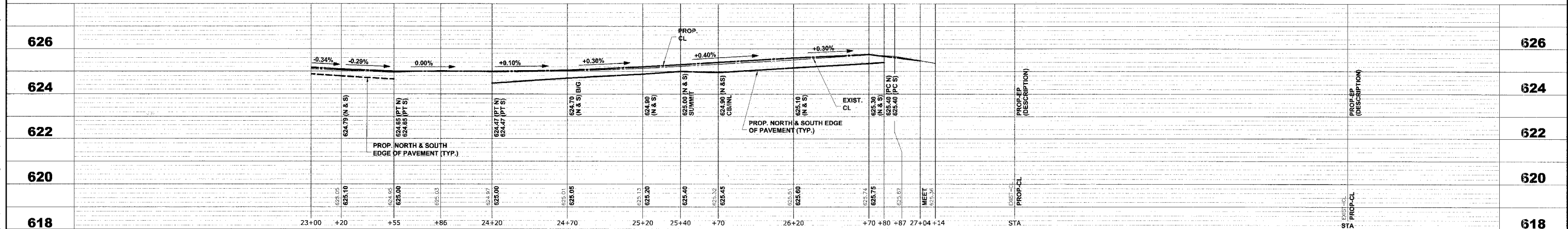
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EXISTING TOPOGRAPHY



PROPOSED IMPROVEMENTS



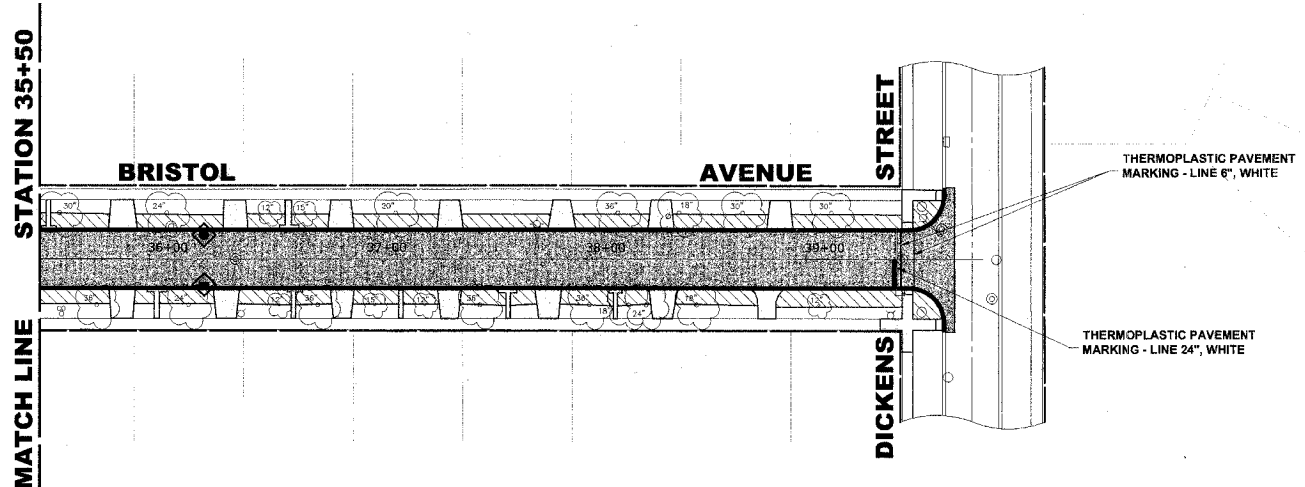
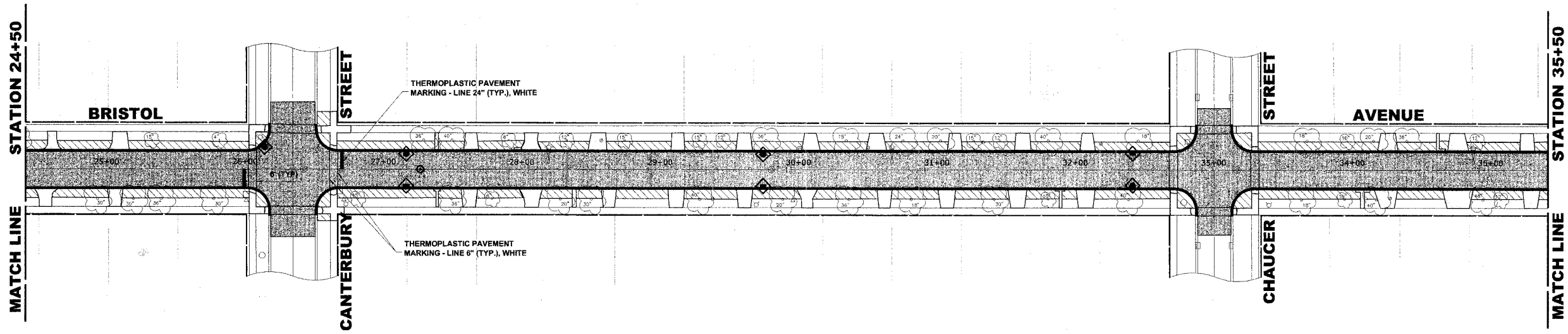
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HIGH PRIORITY PROJECT HPP-2902 (002)
BRISTOL AVENUE & DOVER STREET
VILLAGE OF WESTCHESTER, ILLINOIS

DOVER STREET
PLAN & PROFILE

SCALE: 1"=30'H. & 1"=2'V.	SHEET 17
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BOOK NO.: 1541	
DATE: 10-2-07	
E.H.E. NO.: 838-06-38501	

Drawing file: W:\Projects\83983\850 - High Priority Project Bristol & Dover\Bristol And Dover.dwg Oct 03, 2007 - 11:03am



EROSION CONTROL NOTES

1. SEE IDOT STANDARD 280001-03 FOR TEMPORARY EROSION CONTROL SYSTEMS.
2. THE CONTRACTOR SHALL ENSURE THAT ADJACENT PROPERTIES REMAIN PROTECTED FROM SEDIMENT DEPOSITION.
3. SOIL STOCKPILES SHALL BE PROTECTED WITH PERIMETER EROSION BARRIER OR OTHER EROSION PROTECTION SPECIFIED BY THE RESIDENT ENGINEER. THE COST SHALL BE INCLUDED IN THE UNIT PRICE FOR THE INDIVIDUAL SOIL MATERIALS.
4. WHEREVER CONSTRUCTION VEHICLE ACCESS ROUTES INTERSECT PAVED PUBLIC ROADS, PROVISIONS SHALL BE MADE TO MINIMIZE THE TRANSPORT OF SEDIMENT BY RUNOFF OR VEHICLE TRACKING ONTO THE PAVED SURFACE. THE PROVISIONS MAY INCLUDE SPRAYING VEHICLE WHEELS TO CLEAR SEDIMENT BEFORE EXITING THE CONSTRUCTION SITE OR OTHER MEASURES APPROVED BY THE ENGINEER.

LANDSCAPING NOTES

- SODDING:**
- ALL DISTURBED AREAS INDICATED ON CROSS SECTION OR LANDSCAPING PLANS TO BE SODDED, INCLUDING SUPPLEMENTAL WATERING WHICH IS TO BE USED AFTER THE INITIAL WATERING (AS SPECIFIED AND AS DIRECTED BY THE ENGINEER) AT THE RATE OF 10 GALLONS PER SQUARE YARD SODDING. SODDED SLOPES WHICH ARE 2:1 OR STEEPER SHALL BE STAKED. (PROVIDE A 3" STRIP AROUND CATCH BASINS IN SEEDED AREAS).
- 3" TOP SOIL LAYER:**
- SOIL SHALL BE SPREAD AS INDICATED ON THE TYPICAL CROSS SECTIONS. A MINIMUM OF GRADING TO BE PERMITTED WITHIN AN APPROXIMATE RADIUS OF 6' FROM ALL TREES TO BE SAVED AS DETERMINED BY THE ENGINEER.

EROSION CONTROL LEGEND OF SYMBOLS

SYMBOL	DESCRIPTION
◆	SEDIMENT CONTROL, DRAINAGE STRUCTURE, INLET FILTER CLEANING
◆	INLET FILTER

HOT-MIX ASPHALT & LANDSCAPING LEGEND OF SYMBOLS

SYMBOL	DESCRIPTION
[Blank Box]	PROPOSED CONCRETE AREA
[Stippled Box]	PROPOSED HOT-MIX ASPHALT AREA
[Diagonal Lines Box]	PROPOSED SODDED PARKWAY
[Cross-hatched Box]	PROPOSED STREET RECONSTRUCTION

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HANCOCK ENGINEERING

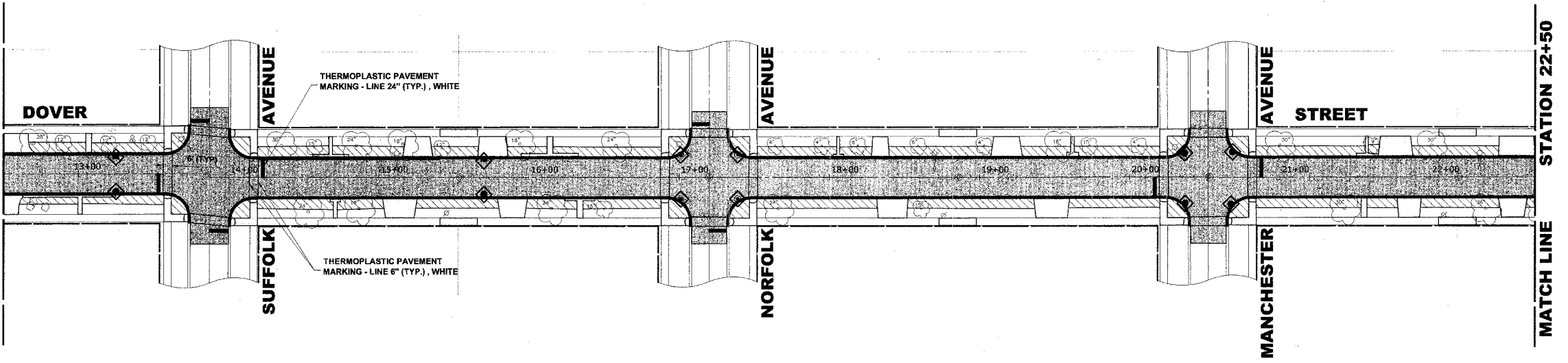
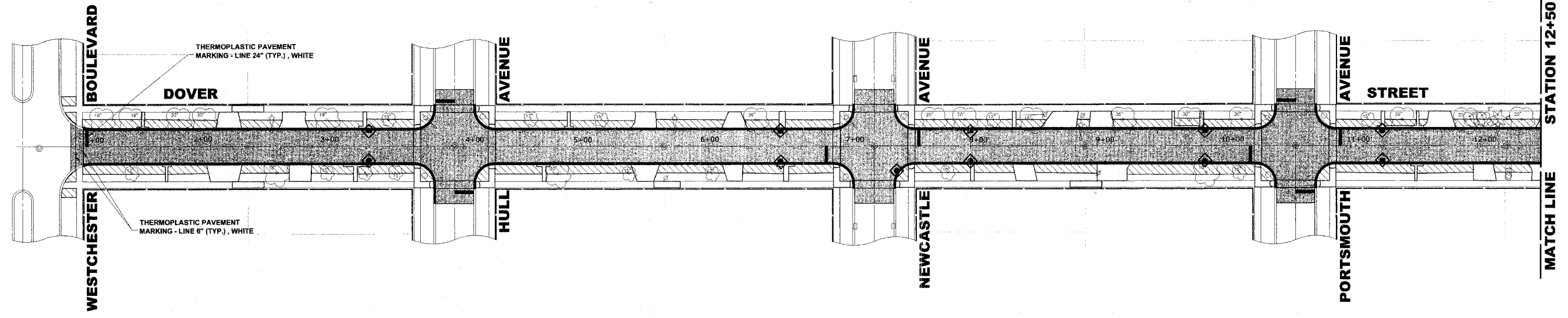
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HIGH PRIORITY PROJECT HPP-2902 (002)
BRISTOL AVENUE & DOVER STREET
VILLAGE OF WESTCHESTER, ILLINOIS

BRISTOL AVENUE
ASPHALT & LANDSCAPING, PAVEMENT MARKING, & EROSION CONTROL PLAN

SCALE:	1"=40'	SHEET	19
DRAWN BY:	ECW/LEV		
BOOK NO.:	1541	OF	22
DATE:	10-2-07		
REVISION:	E.H.E. NO.: 838-06-38501		

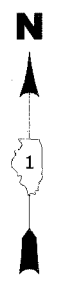
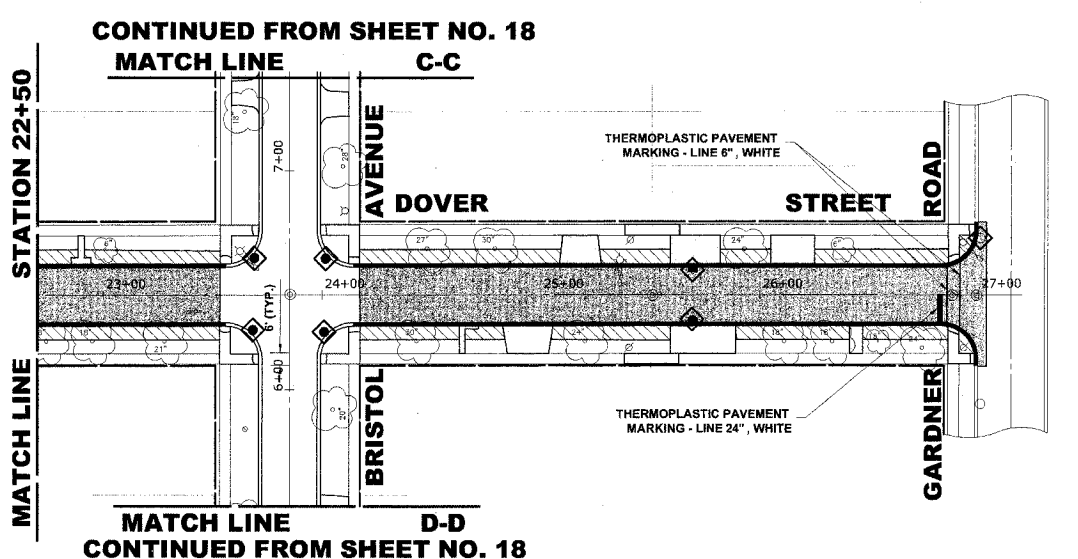


**HOT-MIX ASPHALT & LANDSCAPING
LEGEND OF SYMBOLS**

SYMBOL	DESCRIPTION
[White Box]	PROPOSED CONCRETE AREA
[Stippled Box]	PROPOSED HOT-MIX ASPHALT AREA
[Diagonal Lines Box]	PROPOSED SODDED PARKWAY
[Hatched Box]	PROPOSED STREET RECONSTRUCTION

**EROSION CONTROL
LEGEND OF SYMBOLS**

SYMBOL	DESCRIPTION
[Diamond with X]	SEDIMENT CONTROL, DRAINAGE STRUCTURE, INLET FILTER CLEANING
[Diamond]	INLET FILTER



LANDSCAPING NOTES

SODDING:
ALL DISTURBED AREAS INDICATED ON CROSS SECTION OR LANDSCAPING PLANS TO BE SODDED, INCLUDING SUPPLEMENTAL WATERING WHICH IS TO BE USED AFTER THE INITIAL WATERING (AS SPECIFIED AND AS DIRECTED BY THE ENGINEER) AT THE RATE OF 10 GALLONS PER SQUARE YARD SODDING. SODDED SLOPES WHICH ARE 2:1 OR STEEPER SHALL BE STAKED, (PROVIDE A 3" STRIP AROUND CATCH BASINS IN SEEDED AREAS).

3" TOP SOIL LAYER:
SOIL SHALL BE SPREAD AS INDICATED ON THE TYPICAL CROSS SECTIONS. A MINIMUM OF GRADING TO BE PERMITTED WITHIN AN APPROXIMATE RADIUS OF 6' FROM ALL TREES TO BE SAVED AS DETERMINED BY THE ENGINEER.

EROSION CONTROL NOTES

- SEE IDOT STANDARD 280001-03 FOR TEMPORARY EROSION CONTROL SYSTEMS.
- THE CONTRACTOR SHALL ENSURE THAT ADJACENT PROPERTIES REMAIN PROTECTED FROM SEDIMENT DEPOSITION.
- SOIL STOCKPILES SHALL BE PROTECTED WITH PERIMETER EROSION BARRIER OR OTHER EROSION PROTECTION SPECIFIED BY THE RESIDENT ENGINEER. THE COST SHALL BE INCLUDED IN THE UNIT PRICE FOR THE INDIVIDUAL SOIL MATERIALS.
- WHEREVER CONSTRUCTION VEHICLE ACCESS ROUTES INTERSECT PAVED PUBLIC ROADS, PROVISIONS SHALL BE MADE TO MINIMIZE THE TRANSPORT OF SEDIMENT BY RUNOFF OR VEHICLE TRACKING ONTO THE PAVED SURFACE. THE PROVISIONS MAY INCLUDE SPRAYING VEHICLE WHEELS TO CLEAR SEDIMENT BEFORE EXITING THE CONSTRUCTION SITE OR OTHER MEASURES APPROVED BY THE ENGINEER.

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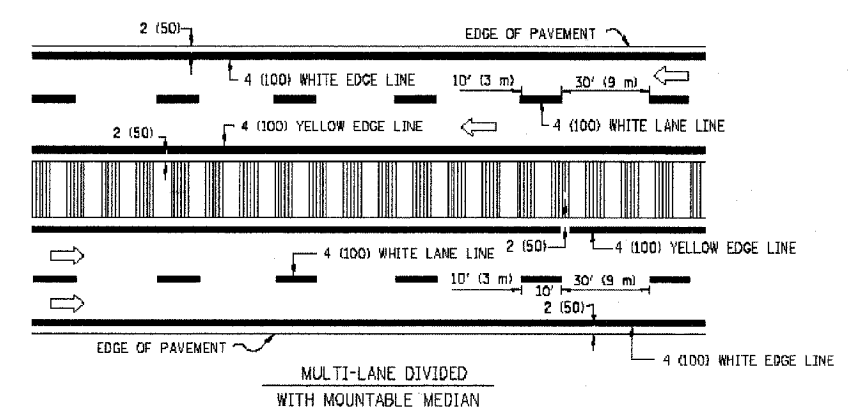
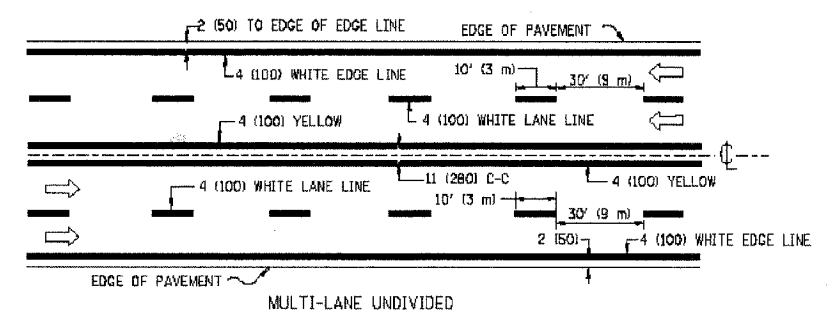
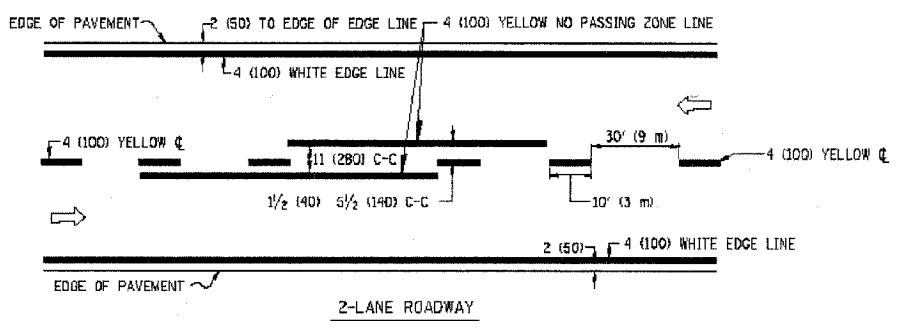
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**HIGH PRIORITY PROJECT HPP-2902 (002)
 BRISTOL AVENUE & DOVER STREET
 VILLAGE OF WESTCHESTER, ILLINOIS**

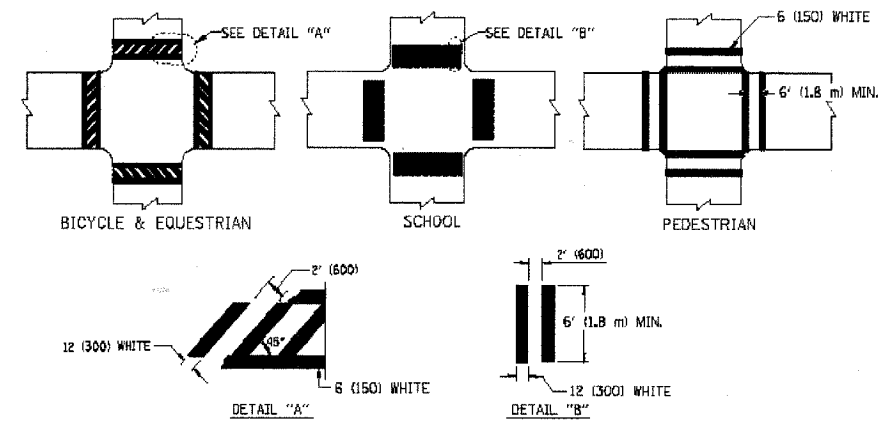
**DOVER STREET
 ASPHALT & LANDSCAPING, PAVEMENT
 MARKING, & EROSION CONTROL PLAN**

SCALE:	1"=40'	SHEET 20
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BOOK NO.:	1541	22
DATE:	10-2-07	
REVISION:	E.H.E. NO.: 838-06-38501	OF

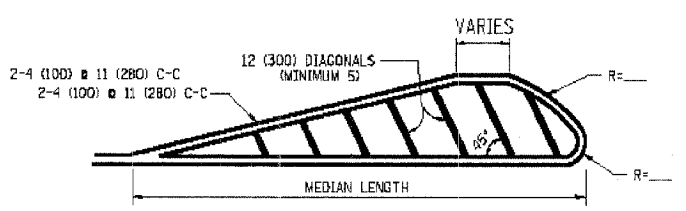
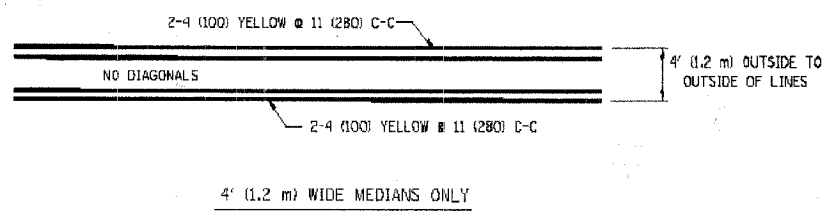


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



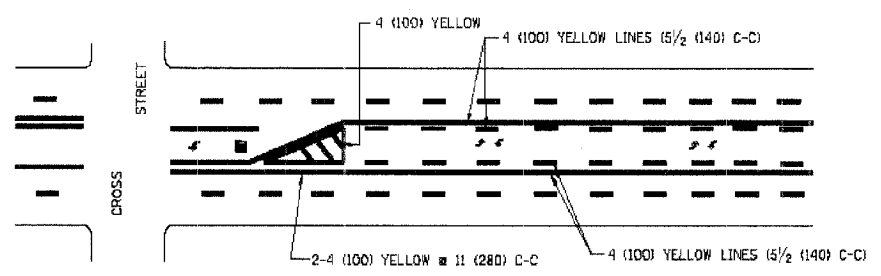
TYPICAL CROSSWALK MARKING



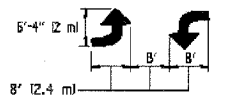
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

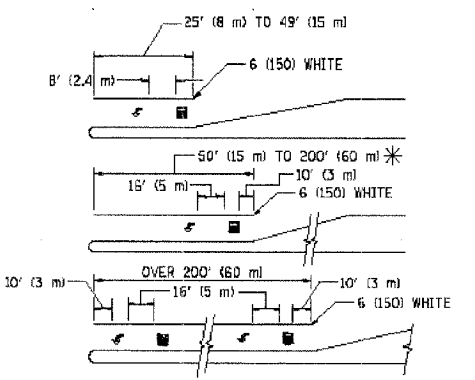


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

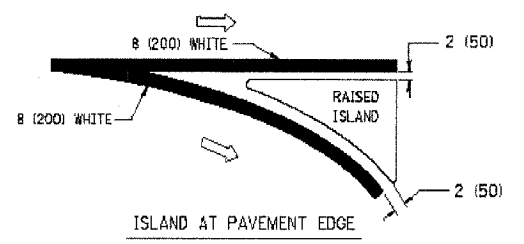
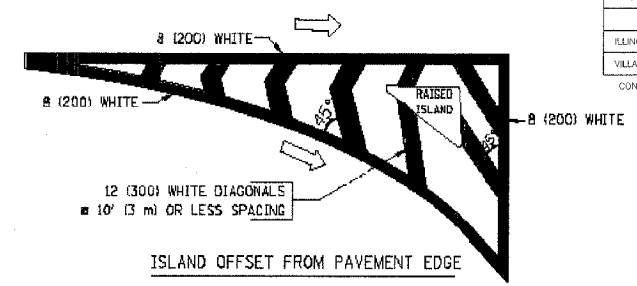


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) | AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

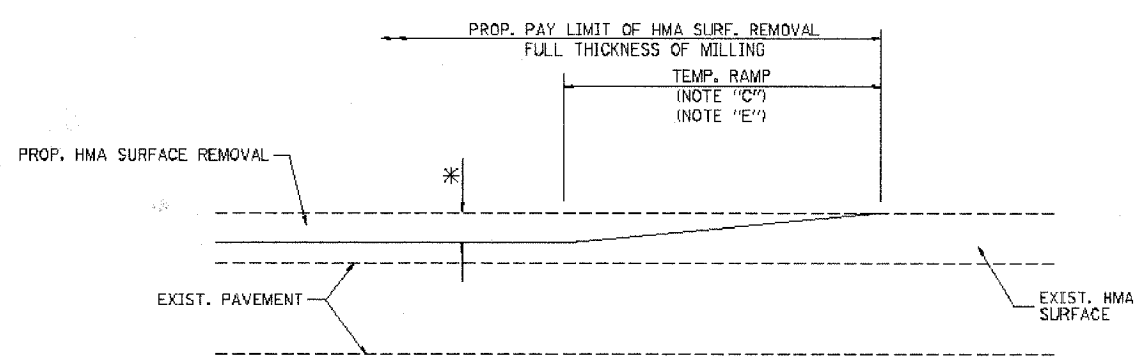
All dimensions are in inches (millimeters) unless otherwise shown.

REVISIONS	
NAME	DATE
EVERS	03-19-90
T. RAMMACHER	10-27-94
ALEX HOUSEH	10-09-96
ALEX HOUSEH	10-17-96
T. RAMMACHER	01-06-00

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT ONE
 TYPICAL PAVEMENT MARKINGS

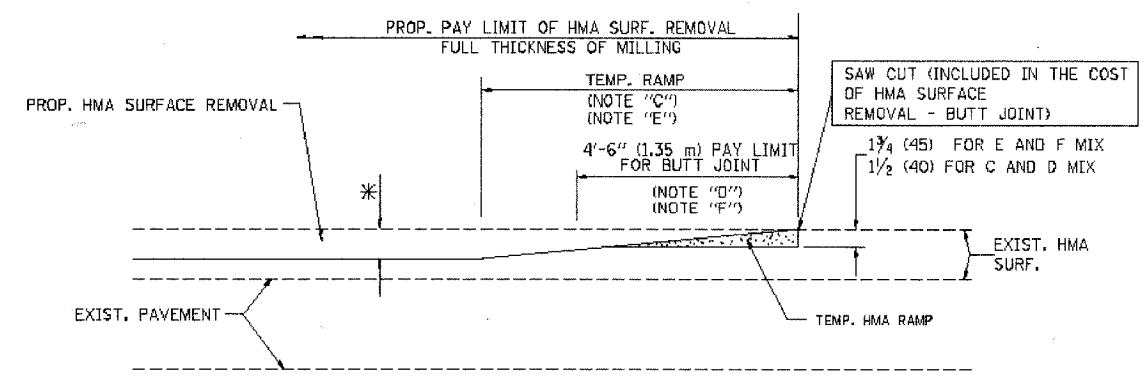
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DRAWN BY CAD0
 CHECKED BY



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

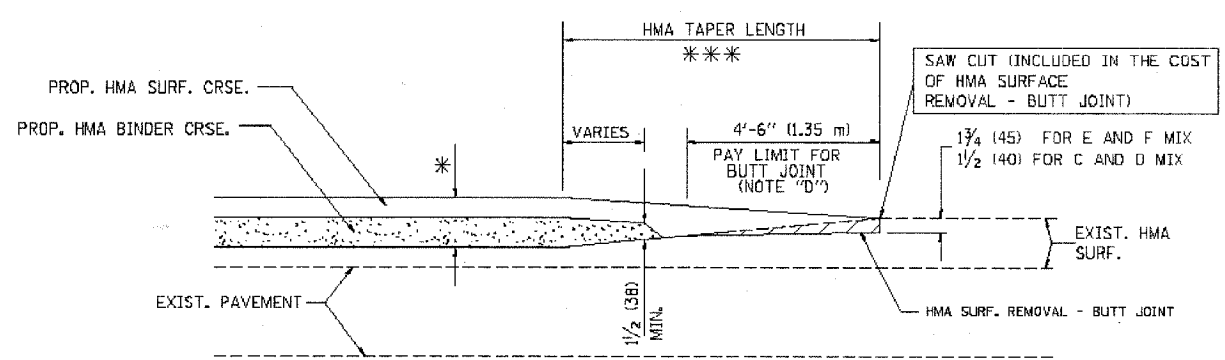
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

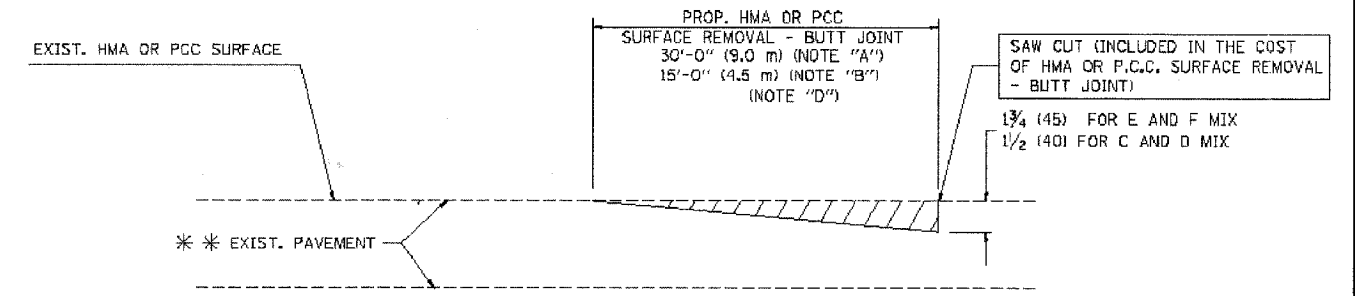
OPTION 2

TYPICAL TEMPORARY RAMP

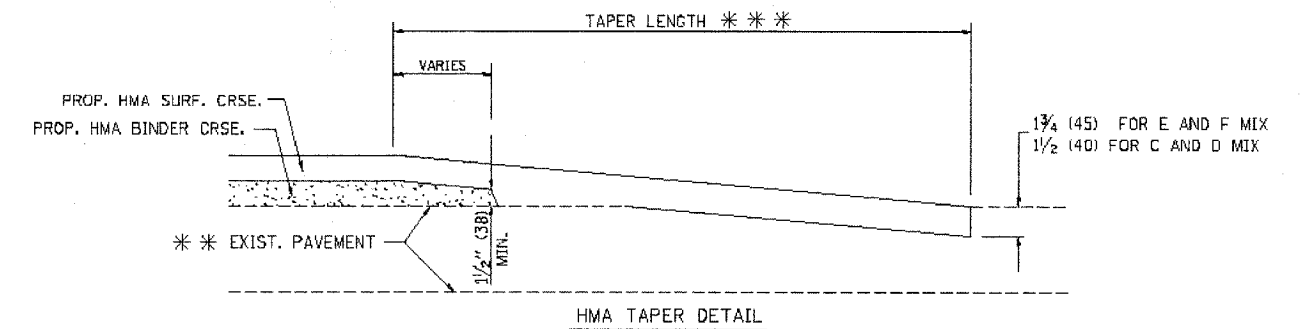


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

SCALE: VERT. NONE
HORIZ.

DRAWN BY
CHECKED BY