

EXISTING SIGN REMOVALS

EXISTING SIGN NUMBER	PANEL DESCRIPTION	STATION	OFFSET	FACING TRAFFIC	WIDTH (IN)	HEIGHT (IN)	REMOVE SIGN PANEL			REMOVE SIGN PANEL ASSEMBLY		REMOVE GROUND MOUNTED SIGN SUPPORT (EACH)	REMOVE CONCRETE FOUNDATION - GROUND MOUNT (EACH)	REMOVE AND RE-ERECT		REMOVE OVERHEAD SIGN STRUCTURE - WALKWAY (FT)		
							TYPE 1 (SQ FT)	TYPE 2 (SQ FT)	TYPE 3 (SQ FT)	TYPE A (EACH)	TYPE B (EACH)			EXISTING SIGN (EACH)	BRIDGE MOUNTED SIGN (EACH)			
WILLOW ROAD (STA 625+00 TO STA 637+00)																		
E1-7	SPEED LIMIT 40	630+44	49.0'	RT	EB	30	36				1							
E1-9	RIGHT LANE MUST TURN RIGHT	631+98	50.3'	RT	EB	36	36				1							
E1-11	RIGHT LANE MUST TURN RIGHT	635+41	55.8'	RT	EB	36	36				1							
WILLOW ROAD (STA 637+00 TO STA 649+00)																		
E2-5	ADDED LANE	640+06	61.8'	RT	EB	36	36				1							
E2-6	ILL ROUTE 43 / WAUKEGAN RD	640+42	45.5'	LT	WB	48	30		10									
E2-8	NO TURN ON RED	640+73	61.0'	LT	SB	30	30	7										
E2-9a	NO TURN ON RED	640+78	83.8'	LT	SB	30	30	7										
E2-13a	WILLOW RD	640+93	43.9'	RT	SB	44	18											
E2-13c	ADDED LANE				SB	36	36				1							
E2-13d	NO TURN ON RED				SB	24	24											
E2-15a	WILLOW RD	641+83	65.9'	LT	NB	44	18	6										
E2-15c	NO TURN ON RED	641+83	65.9'	LT	NB	24	24	4										
E2-16a	NO TURN ON RED	641+94	66.7'	RT	NB	36	36	9										
E2-19	ILL ROUTE 43 / WAUKEGAN RD	642+18	44.7'	RT	EB	48	30		10									
E2-21	SPEED LIMIT 40	643+00	39.3'	RT	EB	30	36				1							
E2-24	RIGHT TURN LANE	644+53	49.9'	LT	WB	24	24				1							
E2-26	BUCKLE UP	646+16	36.3'	RT	EB	30	30	7										
WILLOW ROAD (STA 649+00 TO STA 661+00)																		
E3-3a	THREE LAKES DRIVE	651+97	44.2'	LT	EB	48	30											
E3-3b	THREE LAKES DRIVE				WB	48	30				1							
E3-7a	THREE LAKES DR	652+43	73.1'	LT	WB	36	10				1							
E3-7b	WILLOW RD				SB	36	10											
E3-9	WILLOW RD	652+60	52.5'	RT	SB	36	12	3										
E3-15	WILLOW RD	653+65	99.9'	LT	NB	36	12	3										
E3-19	LANE ENDS	654+62	37.1'	RT	EB	36	36				1							
E3-20	DIVIDED HIGHWAY ENDS	656+61	38.1'	RT	EB	36	36				1							
E3-22	SPEED LIMIT 40	658+16	49.7'	LT	WB	30	36				1							
E3-23	KEEP RIGHT	658+19	0.8'	RT	WB	24	30				1							
E3-24	SPEED LIMIT 35	658+28	38.2'	RT	EB	30	36				1							
WILLOW ROAD (STA 661+00 TO STA 672+00)																		
E4-2	SPEED LIMIT 35	664+02	46.5'	LT	WB	30	36				1							
E4-4a	SUNSET RIDGE RD	664+60	46.6'	LT	WB	60	18	8										
E4-8c	NO TURN ON RED WHEN PEDESTRIANS PRESENT	665+42	48.4'	RT	NB	24	30	5										
E4-11	SUNSET RIDGE RD	665+74	40.8'	RT	EB	60	18	8										
E4-15a	SPEED LIMIT 35	666+86	21.0'	RT	EB	30	36				1							
E4-15b	NO PARKING ALONG HIGHWAY				EB	12	18											
E4-16	NO PARKING ALONG HIGHWAY	667+52	32.9'	LT	WB	12	18				1							
E4-17	NO PARKING ALONG HIGHWAY	667+75	29.4'	RT	EB	12	18				1							
E4-18b	NO PARKING ALONG HIGHWAY	668+78	29.2'	RT	EB	12	18	2										
E4-19a	NO PASSING ZONE	669+09	23.3'	LT	EB	42	34	5										
E4-22	SPEED LIMIT 35	671+01	22.4'	RT	EB	30	36				1							
WILLOW ROAD (STA 672+00 TO STA 683+00)																		
E5-1a	NO LEFT TURN	672+39	19.0'	RT	EB	36	36											
E5-1b	NO LEFT TURN HOURS				EB	36	36				1							
E5-2a	NO LEFT TURN	673+90	42.2'	LT	EB	30	30											
E5-2b	NO LEFT TURN HOURS				EB	30	24											
E5-2c	OLD WILLOW RD WITH ARROW				EB	30	30							1				
E5-2d	STOP SIGN (BLACK FRAME AROUND 1ST 3 LETTERS)				WB	30	30											
E5-6a	NO LEFT TURN	674+75	38.4'	LT	EB	30	30											
E5-6b	NO LEFT TURN HOURS				EB	30	24							1				
E5-8	DO NOT DRIVE ON SHOULDER	675+47	20.9'	RT	EB	30	24				1							
E5-9	SPEED LIMIT 35	679+22	27.6'	LT	WB	30	36				1							
WILLOW ROAD (STA 683+00 TO STA 693+00)																		
E6-2	DO NOT DRIVE ON SHOULDER	686+45	25.8'	LT	WB	30	24				1							
E6-3	SPEED LIMIT 35	687+18	21.2'	RT	EB	30	36				1							
E6-4b	STOP SIGN	691+15	36.9'	LT	SB	30	30	7										

EXISTING SIGN REMOVALS

EXISTING SIGN NUMBER	PANEL DESCRIPTION	STATION	OFFSET	FACING TRAFFIC	WIDTH (IN)	HEIGHT (IN)	REMOVE SIGN PANEL			REMOVE SIGN PANEL ASSEMBLY		REMOVE GROUND MOUNTED SIGN SUPPORT (EACH)	REMOVE CONCRETE FOUNDATION - GROUND MOUNT (EACH)	REMOVE AND RE-ERECT		REMOVE OVERHEAD SIGN STRUCTURE - WALKWAY (FT)
							TYPE 1 (SQ FT)	TYPE 2 (SQ FT)	TYPE 3 (SQ FT)	TYPE A (EACH)	TYPE B (EACH)			EXISTING SIGN (EACH)	BRIDGE MOUNTED SIGN (EACH)	
E7-3	SPEED LIMIT 35	695+61	32.5'	LT	WB	30	36			1						
E7-14a	NO TURN ON RED	697+00	51.3'	LT	NB	24	24			1						
E7-14b	ON SCHOOL DAY WHEN CHILDREN ARE PRESENT	697+13	36.9'	RT	EB	44	18			1						
E7-15a	WAGNER RD	697+19	32.5'	LT	WB	24	30	5								
E7-17c	STOP HERE ON RED	698+28	23.8'	RT	EB	12	18			1						
E7-20a	SPEED LIMIT 35	698+31	35.5'	LT	WB	12	18			1						
E7-20b	NO PARKING ANY TIME	699+71	32.4'	LT	WB	12	18			1						
E7-21	NO PARKING ANY TIME	701+56	24.7'	LT	WB	12	18	2								
E7-23	NO PARKING ANY TIME	701+60	24.5'	RT	EB	12	18			1						
E7-27b	NO PARKING ANY TIME	703+24	23.4'	RT	EB	12	18			1						
E7-28	NO PARKING ANY TIME	703+24	23.4'	RT	WB	42	34									
E7-30a	NO PARKING ANY TIME	703+26	21.0'	LT	WB	12	18			1						
E7-30b	NO PASSING ZONE															
E7-31	NO PARKING ANY TIME									1						
WILLOW ROAD (STA 704+00 TO STA 716+00)																
E8-1	NO PARKING ANY TIME	705+91	23.3'	LT	WB	12	18			1						
E8-5	NO PARKING ANY TIME	709+02	22.5'	RT	EB	12	18			1						
E8-8	NO PARKING ANY TIME	709+64	21.4'	LT	WB	12	18			1						
E8-9	SPEED LIMIT 35	709+70	21.4'	LT	WB	30	36			1						
E8-22	STOP SIGN	711+16	54.5'	RT	NB	30	30			1						
E8-23	DO NOT DRIVE ON SHOULDER	711+49	25.0'	LT	WB	30	24			1						
E8-25	SPEED LIMIT 35	711+56	20.2'	RT	EB	30	36			1						
WILLOW ROAD (STA 716+00 TO STA 728+00)																
E9-1	LANE ENDS	716+26	36.1'	LT	WB	36	36			1						
E9-3	LEFT TURN LANE	717+52	0.8'	LT	EB	24	30			1						
E9-10	NORTHFIELD RD / OLD WILLOW RD	717+95	35.8'	LT	WB	64	30		14							
E9-16a	RIGHT TURN ONLY	719+01	41.3'	LT	WB	36	36	9								
E9-21	SPEED LIMIT 35	720+26	43.4'	LT	WB	30	36	8								
E9-23	ONE WAY	720+48	5.5'	RT	SB	24	30			1						
E9-34d	NO TURN ON RED	722+73	50.8'	LT	SB	24	30	5								
E9-39	CENTRAL (POOR QUALITY, COULD NOT READ)	723+62	45.7'	LT	WB	30	15	4								
E9-45a	NO TURN ON RED WHEN PEDESTRIANS PRESENT	723+98	68.4'	RT	NB	24	30	5								
E9-46a	CENTRAL AVE / S. HAPP RD	724+13	51.5'	RT	EB	48	30									
E9-46b	SPEED LIMIT 35	724+13	51.5'	RT	EB	30	36			1						
E9-48	RIGHT TURN ONLY	724+14	90.1'	RT	NB	24	30			1						
E9-52	ADVANCE INTERSECTION LANE CONTROL	724+47	134.5'	RT	NB	42	30			1						
E9-53	ONE WAY	724+61	11.6'	RT	SB	24	30			1						
E9-55	ONE WAY	725+59	3.9'	RT	SB	24	30			1						
E9-56	PEDESTRIAN CROSSING	725+65	41.4'	RT	EB	30	30			1						
E9-66d	NO LEFT TURN	726+54	50.7'	RT	NB	30	30	7								
E9-68	PEDESTRIAN CROSSING	726+93	39.6'	LT	WB	30	30			1						
E9-71e	EAST INTERSTATE 94 / SOUTH 41 / CHICAGO	727+56	43.0'	LT	WB	96	60		40							
WILLOW ROAD (STA 728+00 TO STA 740+00)																
E10-8	MERGE	734+91	42.3'	RT	EB	36	36			1						
E10-10	MERGE	735+08	32.8'	LT	WB	36	36			1						
E10-12a	STOP SIGN	735+40	38.5'	RT	NB	36	36	9								
E10-21	NO PARKING	737+02	36.5'	RT	NB	12	18			1						
ILLINOIS ROUTE 43 (STA 505+00 TO STA 514+00)																
E12-11	LEFT TURN ONLY	512+06	5.9'	LT	NB	30	36			1						
INTERSTATE 94 (STA 424+00 TO STA 435+00)																
E20-1	KOHL CHILDREN'S MUSEUM	425+89	65.9'	RT	NB	48	60									
E20-3	41 SOUTH / SKOKIE ROAD / EXIT 34A	432+44	72.4'	LT	SB	168	150		175		2	2				
INTERSTATE 94 (STA 435+00 TO STA 446+00)																
E21-2	EXIT 30A-B / DUNDEE ROAD	439+47	23.0'	RT	NB	174	144							1		79
E21-3	EXIT 33A / WEST WILLOW ROAD	439+47	39.5'	RT	NB	168	144							1		
E21-4	EXIT 33B / EAST WILLOW ROAD	439+47	63.0'	RT	NB	210	144									
E21-5	EXIT 33B	440+17	62.9'	RT	NB	90	60									
E21-10	EXIT 33A / WEST WILLOW ROAD	443+91	42.0'	RT	NB	210	96				1			1		19
E21-12	EAST INTERSTATE 94 / SOUTH 41 / CHICAGO	802+89	32.3'	LT	SB	96	60		40		2					

FILE NAME =	USER NAME = BAWtor.t	DESIGNED - JLV	REVISED -
g:\ch11\0158\road\Sheets\0160T35-SHT-SIGN-SCH14.dgn		DRAWN - MTS	REVISED -
	PLOT SCALE = 40.000' / in.	CHECKED - RCB	REVISED -
	PLOT DATE = 11/30/2012	DATE - 10/31/2012	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SIGN SCHEDULE

SCALE: N.T.S SHEET NO. 302 OF 919 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920.01,1518,2022&1922.4)R	COOK	919	302
FED. ROAD DIST. NO. 1 ILLINOIS			CONTRACT NO. 60T35	
FED. AID PROJECT				

EXISTING SIGN RELOCATIONS

EXISTING SIGN NUMBER	PANEL DESCRIPTION	STATION	OFFSET	FACING TRAFFIC	WIDTH (IN)	HEIGHT (IN)	RELOCATE SIGN PANEL		RELOCATE SIGN PANEL ASSEMBLY	
							TYPE 1 (SQ FT)	TYPE 2 (SQ FT)	TYPE A (EACH)	TYPE B (EACH)
WILLOW ROAD (STA 625+00 TO STA 637+00)										
E1-4	ONE WAY	628+64	13.8'	LT	SB	30	36			1
E1-10	RIGHT TURN ONLY	633+43	59.9'	LT	WB	30	36			1
WILLOW ROAD (STA 637+00 TO STA 649+00)										
E2-1a	LOVE THE LAND OF LINCOLN	637+17	47.2'	LT	WB	30	36			1
E2-1b	ADOPT-A-HIGHWAY				WB	30	18			
E2-1c	KEEP ILLINOIS CLEAN				WB	50	25			
E2-2	RIGHT TURN ONLY	636+55	47.3'	RT	EB	30	36			1
E2-4	SPEED LIMIT 45	639+19	46.8'	LT	WB	30	36			1
E2-9b	RIGHT TURN ONLY	640+71	80.6'	LT	SB	30	36			1
E2-11	FOLDED STOP SIGN	640+82	92.7'	LT	SB	36	36			1
E2-12a	ILLINOIS 43	640+58	52.8'	RT	EB	24	24			1
E2-12b	DOUBLE ARROW				EB	21	15			
E2-12c	FOLDED STOP SIGN				EB	36	36			
E2-14a	NORTH	641+84	162.7'	LT	NB	24	12			1
E2-14b	ILLINOIS 43	641+83	65.9'	LT	WB	24	24			1
E2-15d	ILLINOIS 43				WB	24	24			
E2-15e	DOUBLE ARROW				WB	21	15			
E2-16b	FOLDED STOP SIGN	642+18	78.5'	RT	NB	36	36			1
E2-18a	RIGHT TURN ONLY	642+33	61.3'	LT	WB	30	36			1
E2-18b	FOLDED STOP SIGN	643+75	36.3'	RT	EB	24	30			1
E2-23a	NO PARKING SNOW ROUTE				EB	30	36			
E2-23b	NO PARKING ON ANY VILLAGE STREET HOURS				EB	30	36			
E2-25a	LOVE THE LAND OF LINCOLN	644+87	37.2'	RT	EB	30	36			1
E2-25b	ADOPT-A-HIGHWAY				EB	30	18			
E2-25c	KEEP ILLINOIS CLEAN				EB	50	25			
WILLOW ROAD (STA 649+00 TO STA 661+00)										
E3-5a	LEFT TURN ONLY	652+41	6.0'	LT	EB	24	30			1
E3-5b	KEEP RIGHT				WB	24	30			
E3-6a	DO NOT BLOCK INTERSECTION	652+55	42.1'	RT	EB	24	30			1
E3-6b	FOLDED STOP SIGN				EB	30	30			
E3-8	FOLDED STOP SIGN	652+52	67.7'	LT	SB	30	30			1
E3-10a	FIRE LANE	652+62	96.6'	LT	WB	24	30			1
E3-10b	FOR ACCESS TO THIS AREA				WB	24	30			
E3-14	FOLDED STOP SIGN	653+61	48.7'	RT	NB	36	36	9		
E3-16	DO NOT BLOCK INTERSECTION	653+72	41.5'	RT	EB	24	30	5		
E3-17a	RIGHT TURN ONLY	653+95	50.8'	LT	WB	30	36			1
E3-17b	FOLDED STOP SIGN				WB	30	30			
E3-18a	KEEP RIGHT	653+98	5.0'	RT	EB	24	30			1
E3-18b	LEFT TURN ONLY				WB	24	30			
E3-21	RIGHT TURN LANE	655+48	47.1'	LT	WB	24	24			1
E3-25	SCHOOL CROSSING	660+31	33.6'	RT	EB	36	36			1
WILLOW ROAD (STA 661+00 TO STA 672+00)										
E4-4b	NO TURN ON RED	664+55	43.0'	LT	WB	24	24			1
E4-4c	ON SCHOOL DAY WHEN CHILDREN ARE PRESENT				WB	24	12			
E4-5	PACE BUS STOP	664+53	32.8'	RT	EB	18	18			1
E4-6	FOLDED STOP SIGN	664+55	34.6'	RT	EB	36	36	9		
E4-7a	SCHOOL CROSSING	664+79	53.0'	LT	SB	30	30			1
E4-7b	ARROW				SB	24	12			
E4-7c	NO TURN ON RED				SB	24	30			
E4-7d	FOLDED STOP SIGN				SB	30	30			
E4-8a	SCHOOL CROSSING	665+56	48.0'	RT	NB	30	30			1
E4-8b	ARROW				NB	24	12			
E4-8d	FOLDED STOP SIGN				NB	30	30			
E4-9a	SCHOOL SPEED ZONE AHEAD	665+51	137.7'	LT	NB	24	30			1
E4-9b	20 M.P.H.				NB	18	18			
E4-12a	SCHOOL CROSSING	665+84	33.6'	RT	EB	36	36			1
E4-12b	ARROW				EB	24	12			
E4-13a	SCHOOL CROSSING	665+68	39.0'	LT	WB	36	36			1
E4-13b	ARROW				WB	21	15			
E4-13c	NO TURN ON RED				WB	24	24			
E4-13d	ON SCHOOL DAY WHEN CHILDREN ARE PRESENT				WB	24	12			
E4-13e	FOLDED STOP SIGN				WB	36	36			
E4-18a	HORIZONTAL ALIGNMENT CURVE	668+78	33.6'	RT	EB	30	30			1
E4-19b	SCHOOL CROSSING	669+09	44.8'	LT	WB	36	36			1

EXISTING SIGN RELOCATIONS

EXISTING SIGN NUMBER	PANEL DESCRIPTION	STATION	OFFSET	FACING TRAFFIC	WIDTH (IN)	HEIGHT (IN)	RELOCATE SIGN PANEL		RELOCATE SIGN PANEL ASSEMBLY	
							TYPE 1 (SQ FT)	TYPE 2 (SQ FT)	TYPE A (EACH)	TYPE B (EACH)
WILLOW ROAD (STA 672+00 TO STA 683+00)										
E5-4a	STOP SIGN	674+24	61.6'	LT	SB	24	24			1
E5-4b	SLOW CHILDREN				NB	18	24			
E5-5	NO OUTLET	674+42	92.1'	LT	NB	30	30			1
E5-7	SPEED LIMIT 20	675+25	62.0'	LT	EB	24	30			1
E5-11	STOP SIGN	679+90	47.2'	RT	NB	30	30			1
WILLOW ROAD (STA 683+00 TO STA 693+00)										
E6-1	HORIZONTAL ALIGNMENT CURVE	684+52	31.3'	LT	WB	30	30			1
E6-4a	CHAPEL HILL LN	691+09	39.8'	LT	SB	36	10			1
E6-5	SCHOOL CROSSING	691+18	31.6'	RT	EB	36	36			1
E6-6a	NO OUTLET	691+46	56.6'	LT	NB	33	33			1
E6-6b	SLOW CHILDREN				NB	18	24			
WILLOW ROAD (STA 693+00 TO STA 704+00)										
E7-5	FOLDED STOP SIGN	696+24	43.4'	RT	EB	30	30	7		
E7-6b	NO TURN ON RED	696+08	40.6'	LT	WB	24	24			1
E7-6c	ON SCHOOL DAY WHEN CHILDREN ARE PRESENT				WB	24	12			
E7-7a	NO TURN ON RED	696+47	52.8'	LT	SB	24	24			1
E7-7b	ON SCHOOL DAY WHEN CHILDREN ARE PRESENT				SB	24	12			
E7-7c	FOLDED STOP SIGN				SB	30	30			
E7-8	SPEED LIMIT 35				696+48	105.6'	RT	SB	30	
E7-10a	SCHOOL CROSSING	696+95	74.2'	RT	NB	36	36			1
E7-10b	AHEAD				NB	24	12			
E7-12a	NO TURN ON RED	697+09	39.3'	RT	NB	24	24			1
E7-12b	FOLDED STOP SIGN				NB	30	30			
E7-13	SPEED LIMIT 20	696+94	94.7'	LT	NB	18	24			1
E7-16a	NO TURN ON RED	697+35	36.6'	LT	WB	24	24			1
E7-16b	ON SCHOOL DAY WHEN CHILDREN ARE PRESENT				WB	24	12			
E7-16c	FOLDED STOP SIGN				WB	30	30			
E7-17a	SCHOOL CROSSING	697+37	33.6'	LT	WB	36	36			1
E7-17b	ARROW				WB	21	15			
E7-18a	SCHOOL CROSSING	697+35	36.6'	RT	EB	36	36			1
E7-18b	ARROW				EB	21	15			
E7-24a	WILLOW RD (2 SIGN PANELS BACK TO BACK)	699+77	36.8'	RT	NB/SB	24	10			1
E7-24b	EATON ST (2 SIGN PANELS BACK TO BACK)				EB/WB	24	10			
E7-25	SPEED LIMIT 20	699+90	76.5'	RT	SB	24	30			1
E7-26	STOP SIGN	700+18	54.1'	RT	NB	30	30			1
E7-27a	SCHOOL CROSSING	701+56	31.2'	LT	WB	36	36			1
E7-32	SPEED LIMIT 20	703+54	74.0'	RT	SB	24	30			1
E7-33a	WILLOW RD (2 SIGN PANELS BACK TO BACK)	703+84	54.0'	RT	NB/SB	24	10			1
E7-33b	DICKENS ST (2 SIGN PANELS BACK TO BACK)				EB/WB	24	10			
E7-33c	STOP SIGN				NB	30	30			
WILLOW ROAD (STA 704+00 TO STA 716+00)										
E8-2	SPEED LIMIT 20	707+21	75.7'	RT	SB	24	30			1
E8-3	STOP SIGN	707+49	53.5'	RT	NB	30	30			1
E8-4a	WILLOW RD (2 SIGN PANELS BACK TO BACK)	707+49	53.5'	RT	NB/SB	24	10			1
E8-4b	CHURCHILL ST (2 SIGN PANELS BACK TO BACK)				EB/SB	24	10			
E8-10a	N. BRISTOL ST (2 SIGN PANELS BACK TO BACK)	709+84	36.0'	LT	EB/WB	36	10			1
E8-10b	TO ROBINHOOD LN (2 SIGN PANELS BACK TO BACK)				EB/WB	30	8			
E8-10c	STOP SIGN				SB	30	30			
E8-18a	BRISTOL ST (2 SIGN PANELS BACK TO BACK)	711+15	54.0'	RT	EB/WB	24	10			1
E8-18b	WILLOW RD (2 SIGN PANELS BACK TO BACK)				NB/SB	24	10			
E8-19	SPEED LIMIT 20	710+83	72.8'	RT	SB	24	30			1
WILLOW ROAD (STA 716+00 TO STA 728+00)										
E9-4a	WILLOW RD (2 SIGN PANELS BACK TO BACK)	717+65	45.5'	RT	NB/SB	24	10			1
E9-4b	ALICE PL (2 SIGN PANELS BACK TO BACK)				EB/WB	24	10			
E9-4c	NO OUTLET				EB	24	6			
E9-4d	STOP SIGN				NB	30	30			
E9-11a	LEFT TURN ONLY	717+97	1.4'	LT	EB	24	30			1
E9-11b	KEEP RIGHT				WB	24	30			
E9-12	FOLDED STOP SIGN	718+07	48.6'	LT	SB	36	36	1		
E9-13	FOLDED STOP SIGN	718+14	46.0'	RT	EB	36	36	1		
E9-16b	FOLDED STOP SIGN	719+08	29.6'	LT	WB	30	30			1
E9-17a	KEEP RIGHT	719+19	12.8'	RT	EB	24	30			1
E9-17b	LEFT TURN ONLY				WB	24	30			
E9-18	FOLDED STOP SIGN	719+05	64.8'	RT	NB	36	36	1		

FILE NAME =	USER NAME = BAW\rtort	DESIGNED - JLV	REVISED -
g:\ch11\0158\road\Sheets\0160T35-SHT-SIGN-SCH15.dgn		DRAWN - MTS	REVISED -
	PLOT SCALE = 40.000' / in.	CHECKED - RCB	REVISED -
	PLOT DATE = 11/11/2012	DATE - 10/31/2012	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SIGN SCHEDULE

SCALE: N.T.S SHEET NO. 303 OF 919 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920.01,1518,2022&1922,4BR)	COOK	919	303
CONTRACT NO. 60T35				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

EXISTING SIGN RELOCATIONS

EXISTING SIGN NUMBER	PANEL DESCRIPTION	STATION	OFFSET	FACING TRAFFIC	WIDTH (IN)	HEIGHT (IN)	RELOCATE SIGN PANEL		RELOCATE SIGN PANEL ASSEMBLY	
							TYPE 1 (SQ FT)	TYPE 2 (SQ FT)	TYPE A (EACH)	TYPE B (EACH)
E9-19a	TO	719+25	46.9'	RT	EB	24	12			1
E9-19b	WEST									
E9-19c	INTERSTATE 94									
E9-19d	ADVANCE TURN ARROW									
E9-22	POST OFFICE / LIBRARY DIRECTION	720+86	46.9'	RT	EB	24	18	1		
E9-32a	LEFT TURN ONLY	722+58	2.6'	RT	EB	24	30			1
E9-32b	KEEP RIGHT									
E9-34a	EAST	722+67	47.7'	LT	SB	24	12			1
E9-34b	INTERSTATE 94									
E9-34c	DIRECTIONAL ARROW									
E9-34e	FOLDED STOP SIGN									
E9-36a	TO	723+07	64.4'	RT	EB	24	24			1
E9-36b	WEST									
E9-36c	INTERSTATE 94									
E9-36d	DIRECTIONAL ARROW									
E9-36e	FOLDED STOP SIGN									
E9-42a	TO	723+66	33.2'	LT	WB	24	12			1
E9-42b	WEST									
E9-42c	INTERSTATE 94									
E9-42d	DIRECTIONAL ARROW									
E9-42e	FOLDED STOP SIGN									
E9-44a	KEEP RIGHT	723+89	15.8'	RT	EB	24	30			1
E9-44b	LEFT TURN ONLY									
E9-45b	FOLDED STOP SIGN	724+00	71.9'	RT	NB	30	30			1
E9-49a	LOVE THE LAND OF LINCOLN	723+97	31.0'	LT	WB	30	36			1
E9-49b	ADOPT-A-HIGHWAY									
E9-49c	KEEP ILLINOIS CLEAN									
E9-57	RIGHT TURN ONLY	725+43	33.1'	LT	SB	30	36			1
E9-58a	POST OFFICE / LIBRARY DIRECTION	725+43	33.1'	LT	WB	24	18			1
E9-58b	HAPP RD									
E9-58c	CENTRAL AVE									
E9-63a	KEEP RIGHT	726+06	4.2'	RT	WB	24	24			1
E9-63b	NO U TURN									
E9-63c	NO U TURN									
E9-65	NO LEFT TURN	726+38	29.7'	LT	NB	30	30			1
E9-66a	WILLOW RD (2 SIGN PANELS BACK TO BACK)	726+55	52.6'	RT	NB/SB	24	10			1
E9-66b	WALNUT ST (2 SIGN PANELS BACK TO BACK)									
E9-66c	STOP SIGN									
E9-69a	EAST	727+23	36.4'	RT	EB	24	24			1
E9-69b	INTERSTATE 94									
E9-69c	DIRECTIONAL ARROW									
E9-69d	SHARE THE DRIVE									
E9-71a	TO	727+53	42.9'	LT	WB	24	12			1
E9-71b	WEST									
E9-71c	INTERSTATE 94									
E9-71d	DIRECTIONAL ARROW									
WILLOW ROAD (STA 728+00 TO STA 740+00)										
E10-12b	DO NOT ENTER	734+67	43.3'	RT	EB	36	36			1
E10-13	AUTHORIZED VEHICLES ONLY	735+52	73.9'	RT	NB	36	24			1
E10-14a	STOP SIGN	735+11	43.4'	RT	NB	36	36			1
E10-14b	DO NOT ENTER									
E10-15a	RESERVED PARKING HANDICAP	735+71	62.5'	RT	NB	12	18			1
E10-15b	\$250 FINE									
E10-17a	RESERVED PARKING HANDICAP	735+90	60.8'	RT	NB	12	18			1
E10-17b	\$250 FINE									
E10-18	NO PARKING ANY TIME	736+40	43.1'	RT	NB	12	18			1
E10-20	ONE WAY	736+95	34.7'	RT	NB	24	30			1

EXISTING SIGN RELOCATIONS

EXISTING SIGN NUMBER	PANEL DESCRIPTION	STATION	OFFSET	FACING TRAFFIC	WIDTH (IN)	HEIGHT (IN)	RELOCATE SIGN PANEL		RELOCATE SIGN PANEL ASSEMBLY	
							TYPE 1 (SQ FT)	TYPE 2 (SQ FT)	TYPE A (EACH)	TYPE B (EACH)
ILLINOIS ROUTE 43 (STA 505+00 TO STA 514+00)										
E12-2a	NO U TURN	505+43	4.0'	RT	NB	24	24			1
E12-2b	KEEP RIGHT									
E12-6a	KEEP RIGHT	506+18	5.9'	RT	NB	24	30			1
E12-6b	LEFT TURN ONLY									
E12-9a	SOUTH	509+83	53.9'	LT	SB	24	12			1
E12-9b	ILLINOIS 43									
E12-10	ILLINOIS TOLLWAY	511+28	46.6'	RT	NB	30	36			1
E12-12a	GLENVIEW POPULATION	512+04	57.1'	LT	SB	48	24			1
E12-12b	SPEED LIMIT 40									
ILLINOIS ROUTE 43 (STA 518+00 TO STA 530+00)										
E13-1a	HORIZONTAL ALIGNMENT CURVE	519+11	44.3'	RT	NB	30	30			1
E13-1b	40 M.P.H.									
E13-2	SPEED LIMIT 40	521+16	45.8'	LT	SB	30	36			1
E13-3	SPEED LIMIT 45	521+25	43.6'	RT	NB	30	36			1
E13-5a	LOVE THE LAND OF LINCOLN	522+37	42.0'	RT	NB	30	36			1
E13-5b	ADOPT-A-HIGHWAY									
E13-5c	KEEP ILLINOIS CLEAN									
E13-6a	ADVANCE TRAFFIC CONTROL	524+68	39.9'	LT	SB	30	30			1
E13-6b	WILLOW RD									
SUNSET RIDGE ROAD (STA 30+50 TO STA 34+50)										
E15-5	WILLOW RD	33+84	23.5'	RT	NB	48	18			1
INTERSTATE 94 (STA 413+00 TO STA 424+00)										
E19-1	SPEED LIMIT 55	422+68	60.8'	LT	SB	48	60			1
E19-2	TRUCKS USE 2 RIGHT LANES	421+60	71.3'	RT	NB	48	60			1
E19-3	TRUCKS USE 2 RIGHT LANES	420+23	61.2'	LT	SB	48	60			1
INTERSTATE 94 (STA 424+00 TO STA 435+00)										
E20-2a	EAST	434+50	78.0'	LT	SB	15	30			1
E20-2b	94 SHIELD									
E20-2c	SOUTH									
E20-2d	RTE 41									
INTERSTATE 94 (STA 435+00 TO STA 446+00)										
E21-8	WRONG WAY	872+20	11.5'	RT	SB	36	24			1
E21-9	WRONG WAY	872+20	23.5'	RT	SB	36	24			1
E21-11	14FT 4IN	444+03	27.0'	RT	NB	78	24		13	



PROPOSED SIGNS

PROPOSED SIGN NUMBER	SIGN DESIGNATION	PANEL DESCRIPTION	STATION	OFFSET	FACING TRAFFIC	COLOR	WIDTH (IN)	HEIGHT (IN)	SIGN PANEL			NUMBER OF POSTS	GROUND MOUNTED SIGN SUPPORT					DRILLED SHAFT CONCRETE FOUNDATION (CU YD)
									TYPE 1 (SQ FT)	TYPE 2 (SQ FT)	TYPE 3 (SQ FT)		TELESCOPING STEEL (FT)	METAL POST TYPE A (FT)	METAL POST TYPE B (FT)	WOOD (FT)	STRUCTURAL STEEL - BREAKAWAY (LB)	
WILLOW ROAD (STA 625+00 TO STA 637+00)																		
P1-1	R3-7	RIGHT LANE MUST TURN RIGHT	628+79	49.8'	RT	EB	WHITE	36	36	9		2	27					
P1-2	R2-1	SPEED LIMIT 40	630+45	49.5'	RT	EB	WHITE	30	36	8		2	27					
P1-3	R3-7	RIGHT LANE MUST TURN RIGHT	632+66	48.3'	RT	EB	WHITE	36	36	9		2	27					
P1-4	R3-8LL	DUAL LEFT TURN ONLY	636+77	16.0'	LT	EB	WHITE	30	30	7		1		13.5				
WILLOW ROAD (STA 637+00 TO STA 649+00)																		
P2-1a	R3-8LL	DUAL LEFT TURN ONLY	640+47	15.5'	LT	EB	WHITE	30	30	7		1	16					
P2-1b	R4-7	KEEP RIGHT				WB	WHITE	24	30	5								
P2-2	R3-5R	RIGHT TURN ONLY	640+58	52.8'	RT	EB	WHITE	30	36	8								
P2-3a	R3-8LL	DUAL LEFT TURN ONLY	641+44	97.1'	RT	NB	WHITE	30	30	7		1	16					
P2-3b	R4-7	KEEP RIGHT				SB	WHITE	24	30	5								
P2-4a	R3-8LL	DUAL LEFT TURN ONLY	641+52	97.4'	LT	SB	WHITE	30	30	7		1	16					
P2-4b	R4-7	KEEP RIGHT				NB	WHITE	24	30	5								
P2-5a	R3-8LL	DUAL LEFT TURN ONLY	642+45	5.5'	RT	WB	WHITE	30	30	7		1	16					
P2-5b	R4-7	KEEP RIGHT				EB	WHITE	24	30	5								
P2-6	R2-1	SPEED LIMIT 40	643+01	43.3'	RT	EB	WHITE	30	36	8		2	27					
P2-7	R3-5R	RIGHT TURN ONLY	645+62	57.3'	LT	WB	WHITE	30	36	8		2	27					
WILLOW ROAD (STA 649+00 TO STA 661+00)																		
P3-1	R2-1	SPEED LIMIT 40	651+18	37.3'	LT	WB	WHITE	30	36	8		2	27					
P3-2	R2-1	SPEED LIMIT 30	651+18	36.3'	RT	EB	WHITE	30	36	8		2	27					
P3-3	R3-5L	LEFT TURN ONLY	660+51	5.0'	LT	EB	WHITE	30	36	8		2	27					
WILLOW ROAD (STA 6619+00 TO STA 672+00)																		
P4-1	R2-1	SPEED LIMIT 30	664+02	33.3'	LT	WB	WHITE	30	36	8		2	27					
P4-2a	R10-11b	NO TURN ON RED	664+55	34.6'	RT	EB	WHITE	36	36	9								
P4-2b	S4-1103	ON SCHOOL DAYS WHEN CHILDREN PRESENT				EB	WHITE	24	12	2								
P4-3a	R3-5L	LEFT TURN ONLY	664+54	5.0'	LT	EB	WHITE	30	36	8		2	39					
P4-3b	R3-4	NO U TURN				EB	WHITE	36	36	9								
P4-3c	R4-7	KEEP RIGHT				WB	WHITE	24	30	5								
P4-4	S4-1103	ON SCHOOL DAYS WHEN CHILDREN PRESENT	664+79	53.0'	LT	SB	WHITE	24	12	2								
P4-5a	R10-11b	NO TURN ON RED	664+82	51.0'	RT	SB	WHITE	36	36	9								
P4-5b	S4-1103	ON SCHOOL DAYS WHEN CHILDREN PRESENT				SB	WHITE	24	12	2								
P4-6a	R10-11b	NO TURN ON RED	665+56	48.0'	RT	NB	WHITE	36	36	9								
P4-6b	S4-1103	ON SCHOOL DAYS WHEN CHILDREN PRESENT				NB	WHITE	24	12	2								
P4-7a	R10-11b	NO TURN ON RED	665+63	56.0'	LT	NB	WHITE	36	36	9								
P4-7b	S4-1103	ON SCHOOL DAYS WHEN CHILDREN PRESENT				NB	WHITE	24	12	2								
P4-8a	R3-5L	LEFT TURN ONLY	665+83	5.0'	RT	WB	WHITE	30	36	8		2	39					
P4-8b	R3-4	NO U TURN				WB	WHITE	36	36	9								
P4-8c	R4-7	KEEP RIGHT				EB	WHITE	24	30	5								
P4-9a	R10-11b	NO TURN ON RED	665+80	41.0'	RT	EB	WHITE	36	36	9								
P4-9b	S4-1103	ON SCHOOL DAYS WHEN CHILDREN PRESENT				EB	WHITE	24	12	2								
P4-10	R2-1	SPEED LIMIT 30	666+86	33.3'	RT	EB	WHITE	30	36	8		2	27					
P4-11a	R4-7	KEEP RIGHT	669+51	0.0'	LT	WB	WHITE	24	30	5		2	37					
P4-11b	R3-4	NO U TURN				EB	WHITE	36	36	9								
P4-12	S4-1106	SCHOOL ENTRANCE	669+96	42.7'	LT	WB	FYG	24	12	2		1	11.5					
P4-13	R3-5R	RIGHT TURN ONLY	670+99	42.9'	LT	WB	WHITE	30	36	8		2	27					
P4-14	R2-1	SPEED LIMIT 30	671+01	31.3'	RT	EB	WHITE	30	36	8		2	27					
WILLOW ROAD (STA 672+00 TO STA 683+00)																		
P5-1a	R4-7	KEEP RIGHT	672+24	3.0'	RT	EB	WHITE	24	30	5		2	33					
P5-1b	R3-4	NO U TURN				WB	WHITE	36	36	9								
P5-2	R2-1	SPEED LIMIT 30	673+26	31.3'	LT	WB	WHITE	30	36	8		2	27					
P5-3a	R1-1	WILLOW ROAD (2 SIGN PANELS BACK TO BACK)	674+28	44.0'	LT	SB	GREEN	24	10	2		1	14.5	27				
P5-3b		STOP SIGN				SB	RED	30	30	7								
P5-4	R3-1	NO RIGHT TURN	674+96	31.6'	LT	WB	WHITE	36	36	9		2	27					
P5-5a	R4-7	KEEP RIGHT	678+91	0.0'	RT	WB	WHITE	24	30	5		2	37					
P5-5b	R3-4	NO U TURN				EB	WHITE	36	36	9								
P5-6	R2-1	SPEED LIMIT 30	680+93	31.3'	RT	EB	WHITE	30	36	8		2	27					
P5-7a	R4-7	KEEP RIGHT	681+81	3.0'	RT	EB	WHITE	24	30	5		2	33					
P5-7b	R3-4	NO U TURN				WB	WHITE	36	36	9								
WILLOW ROAD (STA 683+00 TO STA 693+00)																		
P6-1a	R4-7	KEEP RIGHT	689+16	3.0'	LT	WB	WHITE	24	30	5		2	33					
P6-1b	R3-4	NO U TURN				EB	WHITE	36	36	9								
P6-2	R2-1	SPEED LIMIT 30	690+09	38.8'	LT	WB	WHITE	30	36	8		2	27					
P6-3	R1-1	STOP SIGN	691+10	40.5'	LT	SB	RED	30	30	7								
P6-4a	R4-7	KEEP RIGHT	691+94	0.0'	LT	EB	WHITE	24	30	5		2	37					
P6-4b	R3-4	NO U TURN				WB	WHITE	36	36	9								

- NOTE:**
- SIGN SHALL BE RE-ERECTED ACCORDING TO ARTICLE 107.25 OF THE STANDARD SPECIFICATION (PROTECTION AND RESTORATION OF TRAFFIC SIGNS). ADDITIONAL SIGNS THAT HAVE NOT BEEN SURVEYED BUT ARE OBTAINED BY CONSTRUCTION OPERATIONS SHALL ALSO BE RE-ERECTED ACCORDING TO ARTICLE 107.25.
  - ALL EXISTING PRIVATE SIGNS WITHIN THE STATE ROW THAT INTERFERE WITH CONSTRUCTION OPERATIONS WILL BE REMOVED AND REPLACED BY OTHERS. THE CONTRACTOR SHALL NOTIFY THE OWNERS PRIOR TO THE START OF CONSTRUCTION.
  - EXISTING SIGN PANEL AND ASSEMBLY TO BE REMOVED SHALL BE DELIVERED TO AN IDOT MAINTENANCE YARD SPECIFIED BY THE ENGINEER.

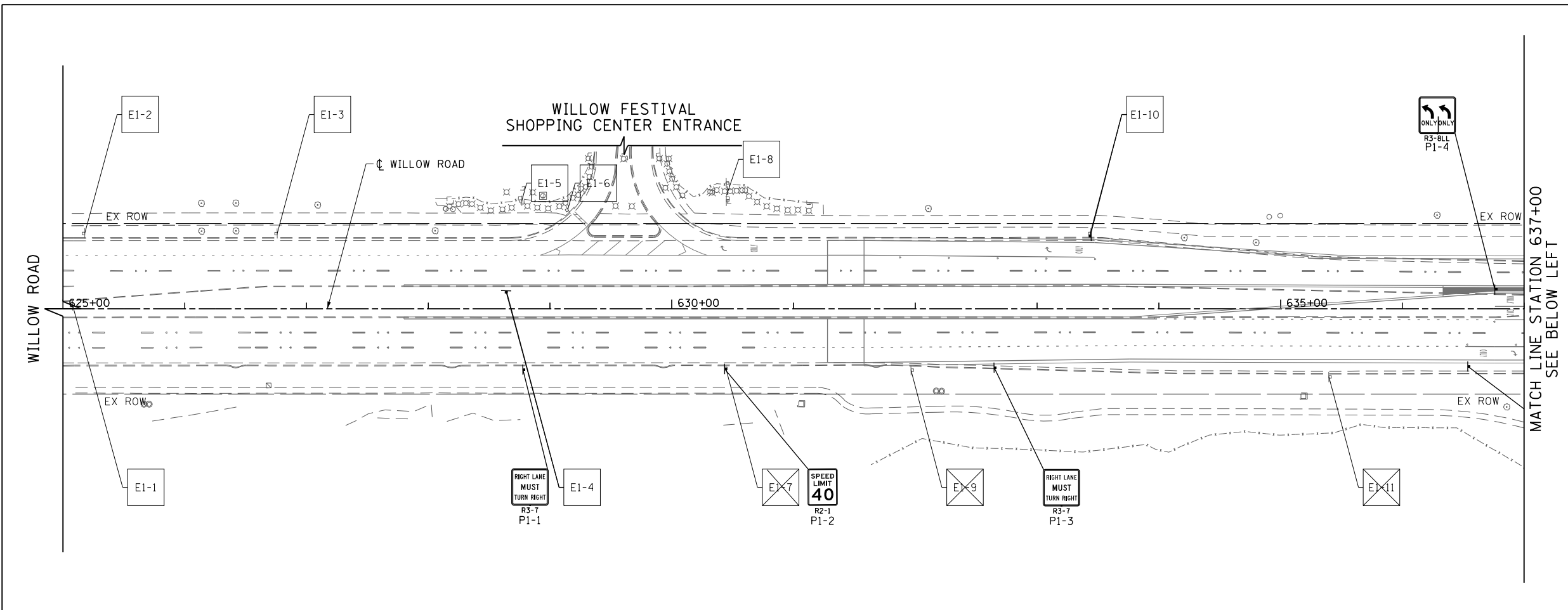
PROPOSED SIGNS

PROPOSED SIGN NUMBER	SIGN DESIGNATION	PANEL DESCRIPTION	STATION	OFFSET	FACING TRAFFIC	COLOR	WIDTH (IN)	HEIGHT (IN)	SIGN PANEL			NUMBER OF POSTS	GROUND MOUNTED SIGN SUPPORT					DRILLED SHAFT CONCRETE FOUNDATION (CU YD)
									TYPE 1 (SQ FT)	TYPE 2 (SQ FT)	TYPE 3 (SQ FT)		TELESCOPING STEEL (FT)	METAL POST TYPE A (FT)	METAL POST TYPE B (FT)	WOOD (FT)	STRUCTURAL STEEL - BREAKAWAY (LB)	
WILLOW ROAD (STA 693+00 TO STA 704+00)																		
P7-1	R2-1	SPEED LIMIT 30	695+60	34.3'	LT	WB	WHITE	30	36	8		2		27				
P7-2a	R3-5L	LEFT TURN ONLY				EB	WHITE	30	36	8								
P7-2b	R3-4	NO U TURN	696+07	5.0'	LT	EB	WHITE	36	36	9		2	39					
P7-2c	R4-7	KEEP RIGHT				WB	WHITE	24	30	5								
P7-3a	R10-11b	NO TURN ON RED				WB	WHITE	36	36	9								
P7-3b	S4-103	ON SCHOOL DAYS WHEN CHILDREN PRESENT	696+24	43.4'	RT	WB	WHITE	24	12	2								
P7-4a	R10-11b	NO TURN ON RED				SB	WHITE	36	36	9								
P7-4b	S4-103	ON SCHOOL DAYS WHEN CHILDREN PRESENT	696+31	51.3'	RT	SB	WHITE	24	12	2								
P7-5	S4-103	ON SCHOOL DAYS WHEN CHILDREN PRESENT	697+09	39.3'	RT	NB	WHITE	24	12	2								
P7-6a	R10-11b	NO TURN ON RED				NB	WHITE	36	36	9								
P7-6b	S4-103	ON SCHOOL DAYS WHEN CHILDREN PRESENT	697+14	52.8'	LT	NB	WHITE	24	12	2								
P7-7a	R3-5L	LEFT TURN ONLY				WB	WHITE	30	36	8								
P7-7b	R3-4	NO U TURN	697+36	5.0'	RT	WB	WHITE	36	36	9		2	39					
P7-7c	R4-7	KEEP RIGHT				EB	WHITE	24	30	5								
P7-8a	R10-11b	NO TURN ON RED				EB	WHITE	36	36	9								
P7-8b	S4-103	ON SCHOOL DAYS WHEN CHILDREN PRESENT	697+35	36.6'	RT	EB	WHITE	24	12	2								
P7-9	R2-1	SPEED LIMIT 30	698+28	33.3'	RT	EB	WHITE	30	36	8		2		27				
P7-10	R3-5R	RIGHT TURN ONLY	700+18	54.0'	RT	NB	WHITE	30	36	8								
P7-11	R3-5R	RIGHT TURN ONLY	703+84	54.0'	RT	NB	WHITE	30	36	8								
WILLOW ROAD (STA 704+00 TO STA 716+00)																		
P8-1	R3-5R	RIGHT TURN ONLY	707+49	54.0'	RT	NB	WHITE	30	36	8								
P8-2a	R4-7	KEEP RIGHT				WB	WHITE	24	30	5								
P8-2b	R3-4	NO U TURN	707+59	2.0'	LT	EB	WHITE	36	36	9		2	33					
P8-3	R2-1	SPEED LIMIT 30	708+82	36.8'	LT	WB	WHITE	30	36	8		2		27				
P8-4		WILLOW ROAD (2 SIGN PANELS BACK TO BACK)	709+77	38.6'	LT	SB	GREEN	24	10	2								
P8-5	R1-1	STOP SIGN	711+15	54.0'	RT	NB	RED	30	30	7								
P8-6	R2-1	SPEED LIMIT 30	711+94	36.9'	RT	EB	WHITE	30	36	8		2		27				
P8-7a	R4-7	KEEP RIGHT				EB	WHITE	24	30	5								
P8-7b	R3-4	NO U TURN	713+45	2.0'	RT	WB	WHITE	36	36	9		2	33					
WILLOW ROAD (STA 716+00 TO STA 728+00)																		
P9-1	R2-1	SPEED LIMIT 30	716+74	40.3'	LT	WB	WHITE	30	36	8								
P9-2	R3-5R	RIGHT TURN ONLY	717+65	45.5'	RT	NB	WHITE	30	36	8								
P9-3	R3-4	NO U TURN	717+97	1.4'	LT	EB	WHITE	36	36	9								
P9-4	R2-1	SPEED LIMIT 30	720+86	46.9'	RT	EB	WHITE	30	36	8								
P9-5	R2-1	SPEED LIMIT 30	720+86	30.0'	LT	WB	WHITE	30	36	8								
P9-6	R2-1	SPEED LIMIT 30	725+78	39.8'	RT	EB	WHITE	30	36	8								
P9-7	R4-7	KEEP RIGHT	726+06	4.2'	RT	EB	WHITE	24	30	5								
P9-8	R2-1	SPEED LIMIT 30	726+93	39.6'	LT	WB	WHITE	30	36	8								
P9-9	R3-5R	RIGHT TURN ONLY	726+55	52.6'	RT	NB	WHITE	30	36	8								
P9-10		EAST INTERSTATE 94 / SOUTH 41 / CHICAGO	727+53	42.9'	LT	WB	GREEN	132	90									
P9-11	R3-8R	ADVANCE INTERSECTION LANE CONTROL	724+14	90.1'	RT	NB	WHITE	30	30	7								
WILLOW ROAD (STA 728+00 TO STA 740+00)																		
P10-1	W4-1R	MERGE	735+08	32.9'	LT	WB	YELLOW	36	36	9		2		27				
P10-2	R7-2	NO PARKING	737+02	36.3'	RT	NB	WHITE	12	18	2		1		12				
ILLINOIS ROUTE 43 (STA 505+00 TO STA 514+00)																		
P12-1	R3-8LL	DUAL LEFT TURN ONLY	511+09	12.0'	LT	NB	WHITE	30	30	7		1			13.5			
P12-2	R3-5R	RIGHT TURN ONLY	512+84	56.3'	RT	NB	WHITE	30	36	8		2		27				
ILLINOIS ROUTE 43 (STA 518+00 TO STA 530+00)																		
P13-1	R3-5R	RIGHT TURN ONLY	519+48	56.3'	LT	SB	WHITE	30	36	8		2		27				
P13-2	R3-8LL	DUAL LEFT TURN ONLY	520+93	12.0'	RT	SB	WHITE	30	30	7		1			13.5			
P13-3	R4-7	KEEP RIGHT	525+15	1.5'	LT	SB	WHITE	24	30	5		1			13.5			
P13-4	R3-5R	RIGHT TURN ONLY	526+20	56.1'	RT	NB	WHITE	30	36	8		2		27				
INTERSTATE 94 (STA 424+00 TO STA 435+00)																		
P20-1		41 SOUTH / SKOKIE ROAD / EXIT 34A	425+00	73.3'	LT	SB	GREEN	168	150							1414	3.8	
P20-2		KOHL CHILDREN'S MUSEUM	426+47	77.0'	RT	NB	BROWN	60	60				25					
INTERSTATE 94 (STA 435+00 TO STA 446+00)																		
P21-1		EXIT 30A-B / DUNDEE ROAD	439+47	23.0'	RT	NB	GREEN	180	150									
P21-2		EXIT 33A / WEST WILLOW ROAD	439+47	39.5'	RT	NB	GREEN	168	132									
P21-3		EXIT 33B / EAST WILLOW ROAD	439+47	63.0'	RT	NB	GREEN	210	108									
P21-4		EXIT 33B	440+16	62.8'	RT	NB	GREEN	90	60							17		
P21-5		EXIT 33A / WEST WILLOW ROAD	443+91	42.0'	RT	NB	GREEN	210	108									
P21-6		EAST INTERSTATE 94 / SOUTH 41 / CHICAGO	802+94	32.4'	LT	EB	GREEN	132	90							42		

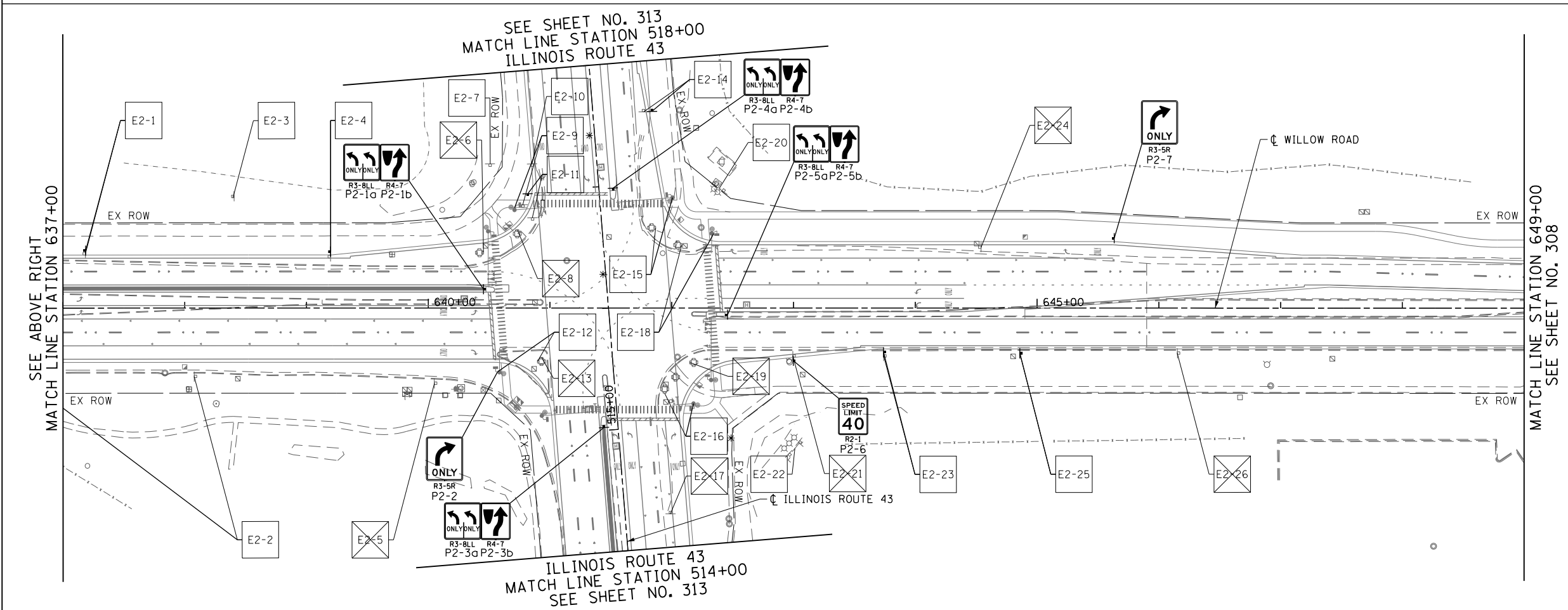
- NOTE:**
- SIGN SHALL BE RE-ERECTED ACCORDING TO ARTICLE 107.25 OF THE STANDARD SPECIFICATION (PROTECTION AND RESTORATION OF TRAFFIC SIGNS). ADDITIONAL SIGNS THAT HAVE NOT BEEN SURVEYED BUT ARE OBTAINED BY CONSTRUCTION OPERATIONS SHALL ALSO BE RE-ERECTED ACCORDING TO ARTICLE 107.25.
  - ALL EXISTING PRIVATE SIGNS WITHIN THE STATE ROW THAT INTERFERE WITH CONSTRUCTION OPERATIONS WILL BE REMOVED AND REPLACED BY OTHERS. THE CONTRACTOR SHALL NOTIFY THE OWNERS PRIOR TO THE START OF CONSTRUCTION.
  - EXISTING SIGN PANEL AND ASSEMBLY TO BE REMOVED SHALL BE DELIVERED TO AN IDOT MAINTENANCE YARD SPECIFIED BY THE ENGINEER.

TOTAL PROPOSED SIGNS

SIGN DESIGNATION	QUANTITY
R1-1	3
R2-1	19
R3-1	1
R3-4	13
R3-5L	5
R3-5R	11
R3-7	2
R3-8R	1
R3-8LL	7
R4-7	18
R7-2	1
R10-11b	9
S4-1103	11
S4-1106	1
W4-1R	1
OTHER	13

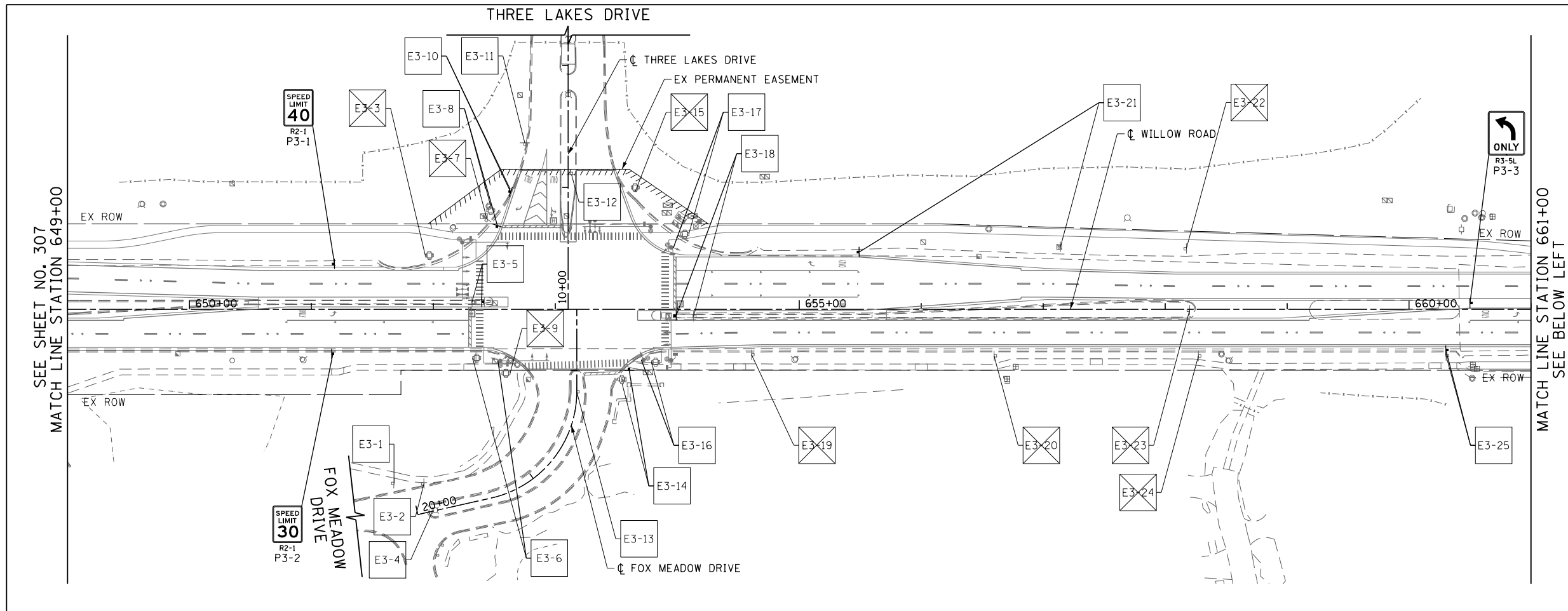


- LEGEND**
- ▬ PROPOSED / RELOCATED SIGN LOCATION  
SEE SHEET NO. 303 TO 306
  - \* SIGN PANEL REMOVED WHILE SIGN PANEL ASSEMBLY REMAINS OR IS RELOCATED  
SEE SHEET NO. 289 TO 306
  - NEW SIGN PANEL WITH MUTCD CODE AND PR SIGN NUMBER  
SEE SHEET NO. 305 TO 306
  - EXISTING SIGN  
SEE SHEET NO. 289 TO 300
  - EXISTING SIGN TO BE REMOVED  
SEE SHEET NO. 301 TO 302

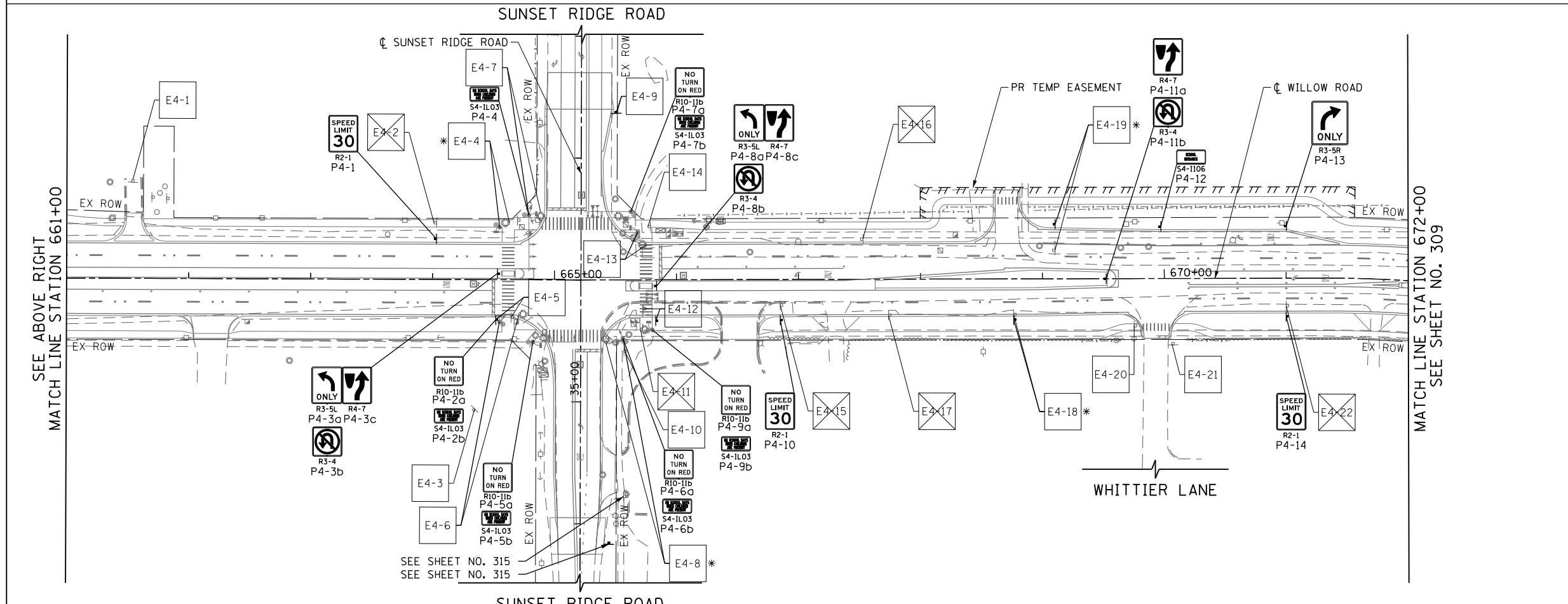


- NOTE:**
1. ALL SIGNS RELOCATED, REPLACED, OR FURNISHED NEW BY THE CONTRACTOR SHALL BE PLACED FIRM INTO THE GROUND. ANY LOOSE SIGN POSTS WITHIN THE PROJECT LIMITS SHALL BE FIXED INTO THE GROUND BY THE CONTRACTOR SO THAT THE SIGNS DO NOT MOVE OR SWAY AT NO ADDITIONAL COST TO THE CONTRACT.
  2. FOR SIGN PANEL AND STRUCTURAL MOUNTING DETAILS SEE SHEET NO. 321 TO 333

FILE NAME = G:\CH11\0158\Road\Sheets\0160T35-SHT-SIGN02.dgn	USER NAME = BAWtor.t	DESIGNED - JLV	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>WILLOW ROAD SIGNING PLAN</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE = 100.000' / in.	CHECKED - RCB	REVISED -	305			(1920.01,1518,2022&1922,4)R	COOK	919	307	
PLOT DATE = 11/30/2012	DATE - 10/31/2012	REVISED -	CONTRACT NO. 60T35							
SCALE: 1" = 50'						SHEET NO. 307 OF 919 SHEETS		STA. 625+00 TO STA. 649+00		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



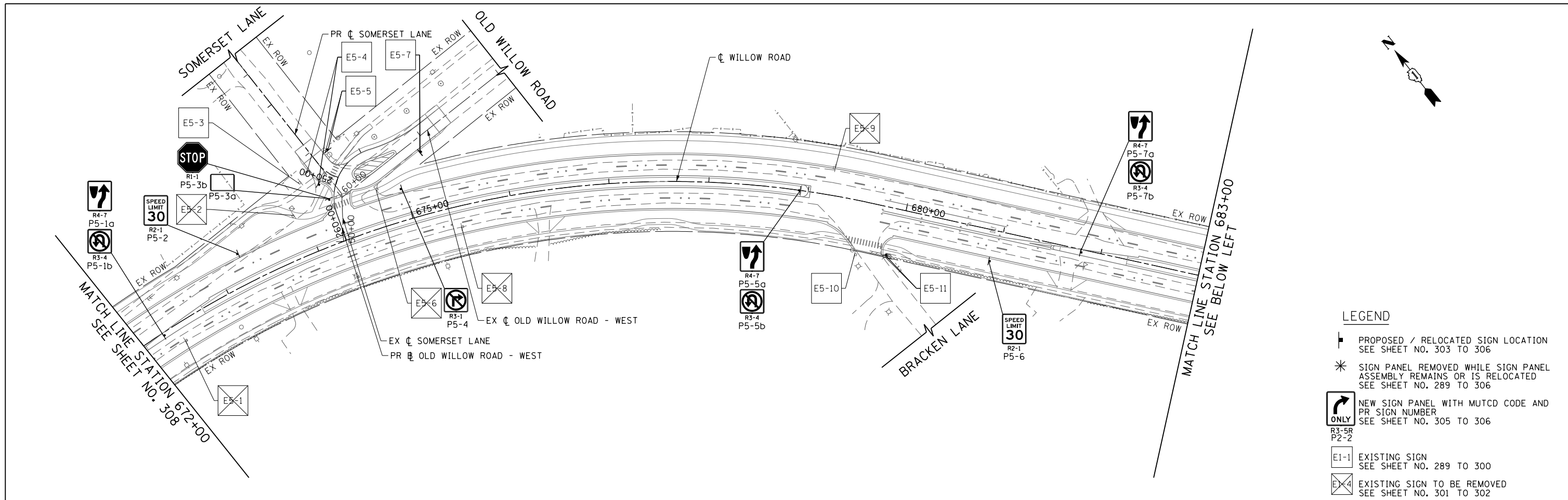
- LEGEND**
- ▬ PROPOSED / RELOCATED SIGN LOCATION  
SEE SHEET NO. 303 TO 306
  - \* SIGN PANEL REMOVED WHILE SIGN PANEL ASSEMBLY REMAINS OR IS RELOCATED  
SEE SHEET NO. 289 TO 306
  - ONLY R3-5L P3-3  
NEW SIGN PANEL WITH MUTCD CODE AND PR SIGN NUMBER  
SEE SHEET NO. 305 TO 306
  - E1-1 EXISTING SIGN  
SEE SHEET NO. 289 TO 300
  - E4 EXISTING SIGN TO BE REMOVED  
SEE SHEET NO. 301 TO 302



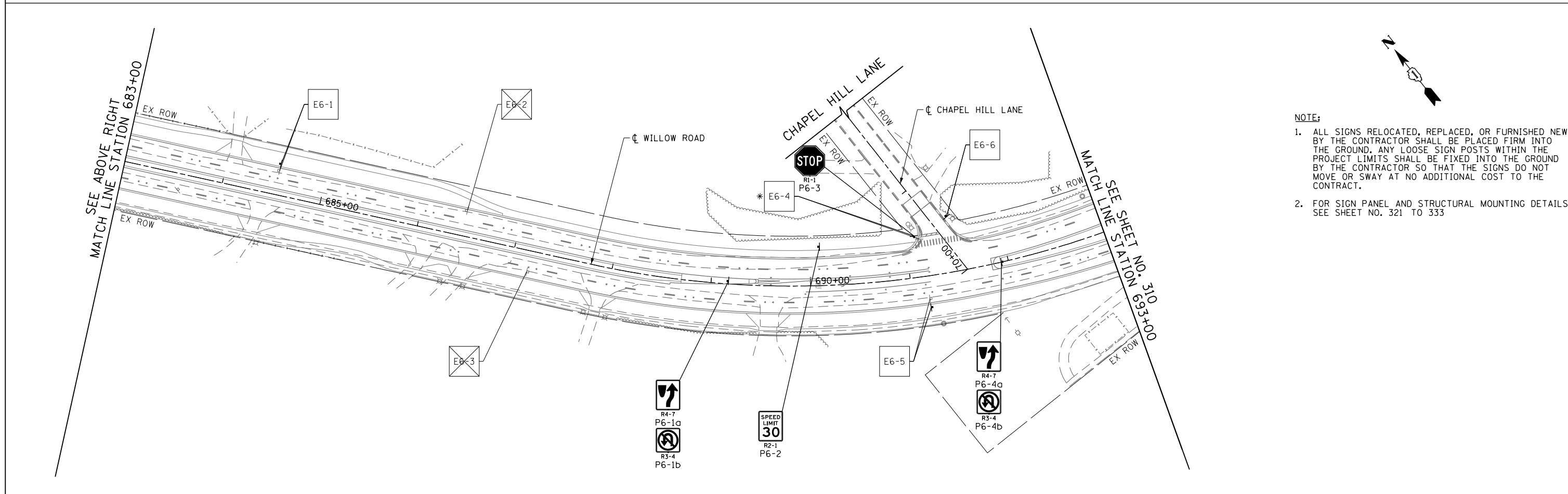
- NOTE:**
1. ALL SIGNS RELOCATED, REPLACED, OR FURNISHED NEW BY THE CONTRACTOR SHALL BE PLACED FIRM INTO THE GROUND. ANY LOOSE SIGN POSTS WITHIN THE PROJECT LIMITS SHALL BE FIXED INTO THE GROUND BY THE CONTRACTOR SO THAT THE SIGNS DO NOT MOVE OR SWAY AT NO ADDITIONAL COST TO THE CONTRACT.
  2. FOR SIGN PANEL AND STRUCTURAL MOUNTING DETAILS SEE SHEET NO. 321 TO 333

FILE NAME = G:\CH11\0158\Road\Sheets\160T35-SHT-SIGN03.dgn	USER NAME = BAW\ort	DESIGNED - JLV	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>WILLOW ROAD SIGNING PLAN</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
PLOT SCALE = 100.000' / in.	CHECKED - RCB	REVISIED -	REVISIED -			305	(1920.01,1518,2022&1922,4BR)	COOK	919	308	
PLOT DATE = 11/30/2012	DATE - 10/31/2012	REVISIED -	REVISIED -			CONTRACT NO. 60T35					
						FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT					

SCALE: 1" = 50' SHEET NO. 308 OF 919 SHEETS STA. 649+00 TO STA. 672+00

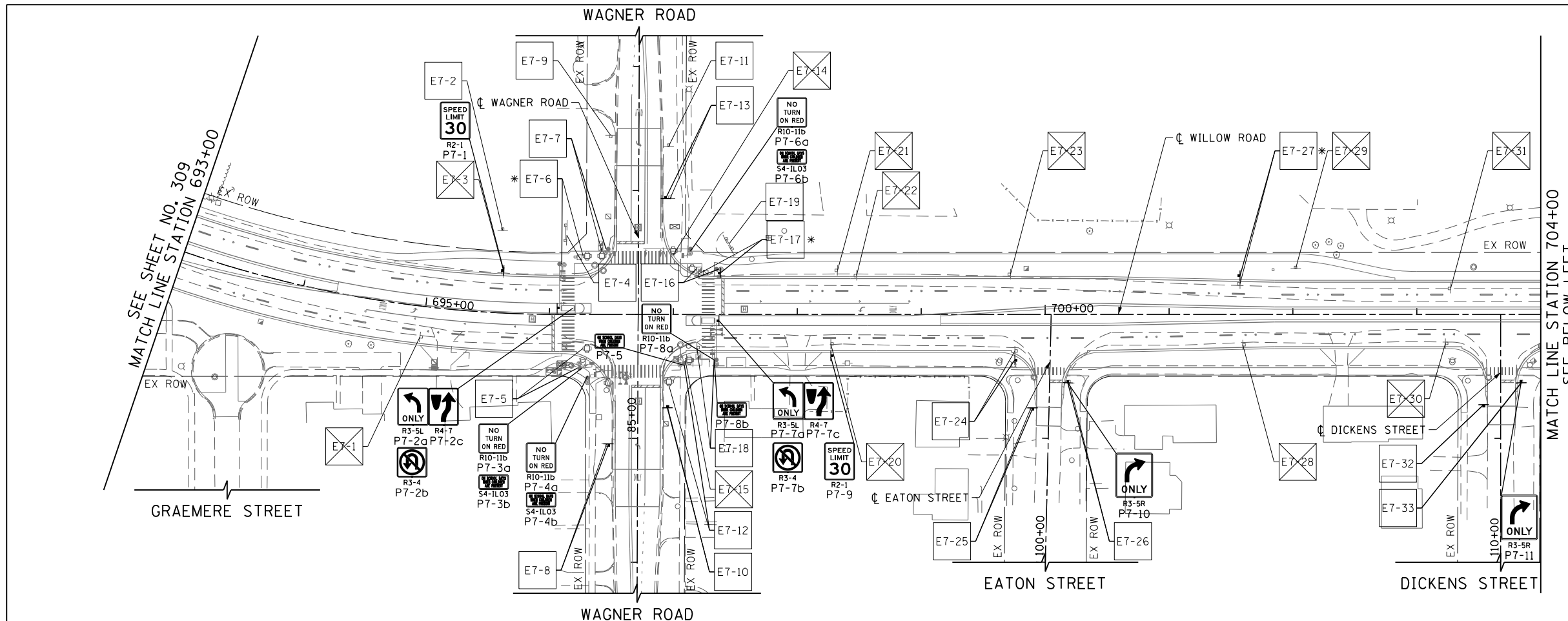


- LEGEND**
- ▬ PROPOSED / RELOCATED SIGN LOCATION  
SEE SHEET NO. 303 TO 306
  - \* SIGN PANEL REMOVED WHILE SIGN PANEL  
ASSEMBLY REMAINS OR IS RELOCATED  
SEE SHEET NO. 289 TO 306
  - ONLY  
R3-5R  
P2-2  
NEW SIGN PANEL WITH MUTCD CODE AND  
PR SIGN NUMBER  
SEE SHEET NO. 305 TO 306
  - E1-1 EXISTING SIGN  
SEE SHEET NO. 289 TO 300
  - E4 EXISTING SIGN TO BE REMOVED  
SEE SHEET NO. 301 TO 302

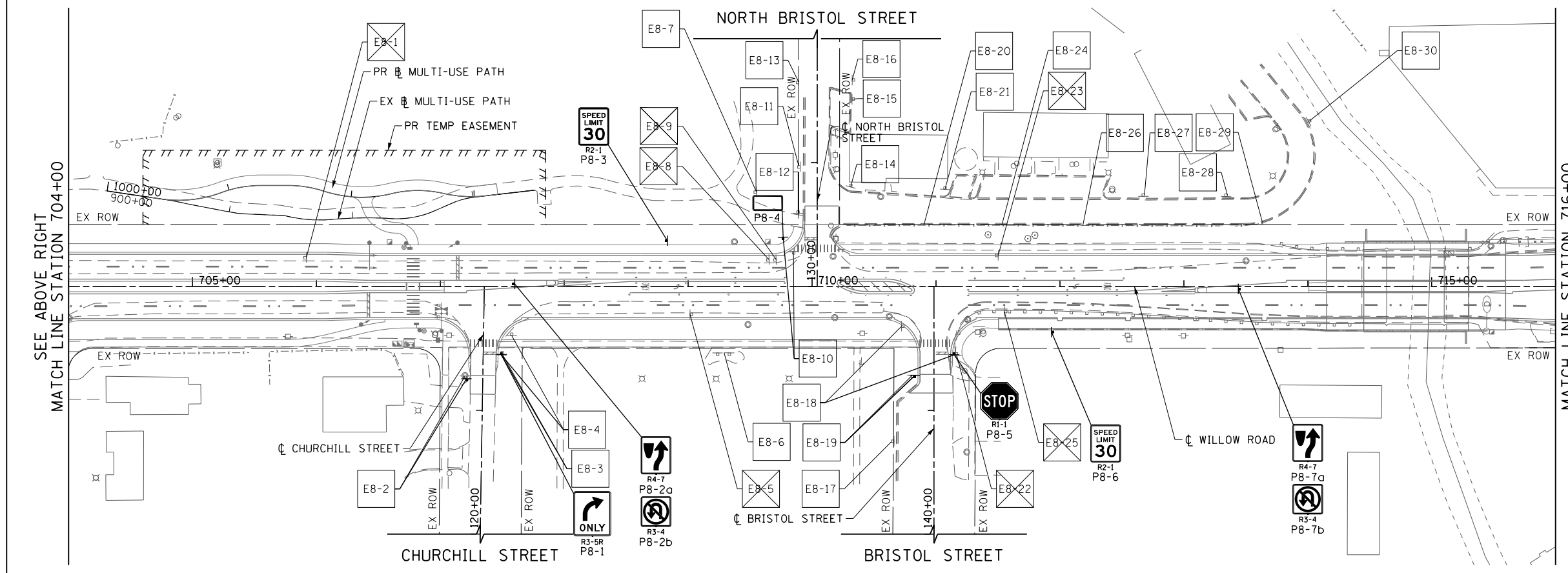


- NOTE:**
1. ALL SIGNS RELOCATED, REPLACED, OR FURNISHED NEW BY THE CONTRACTOR SHALL BE PLACED FIRM INTO THE GROUND. ANY LOOSE SIGN POSTS WITHIN THE PROJECT LIMITS SHALL BE FIXED INTO THE GROUND BY THE CONTRACTOR SO THAT THE SIGNS DO NOT MOVE OR SWAY AT NO ADDITIONAL COST TO THE CONTRACT.
  2. FOR SIGN PANEL AND STRUCTURAL MOUNTING DETAILS SEE SHEET NO. 321 TO 333

FILE NAME =	USER NAME = BAWtor.t	DESIGNED - JLV	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>WILLOW ROAD SIGNING PLAN</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
G:\CH11\0158\Road\Sheets\0160T35-SHT-SIGN04.dgn	DRAWN - MTS	REVISED -	305			(1920.01,1518,2022&1922.4)R	COOK	919	309	
PLOT SCALE = 100.000' / in.	CHECKED - RCB	REVISED -	CONTRACT NO. 60T35							
PLOT DATE = 11/1/2012	DATE - 10/31/2012	REVISED -	FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT							
				SCALE: 1" = 50'		SHEET NO. 309 OF 919 SHEETS		STA. 672+00 TO STA. 693+00		



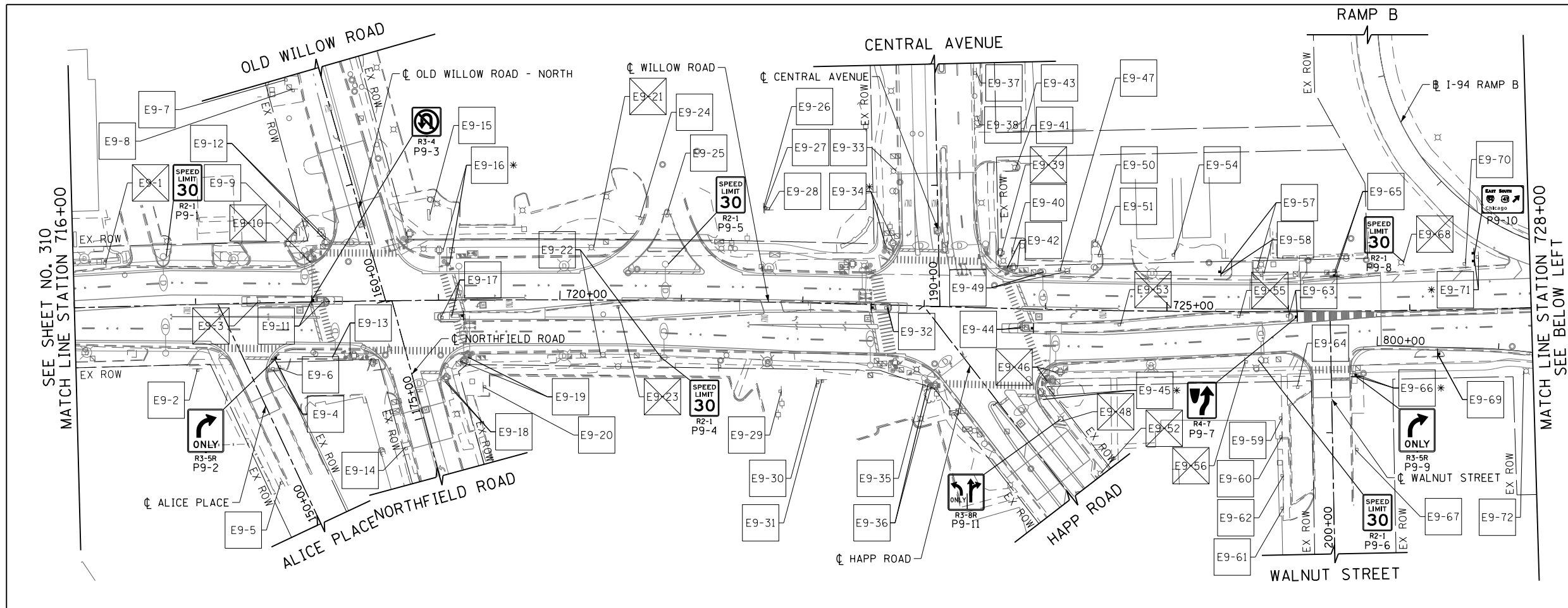
- LEGEND**
- ▬ PROPOSED / RELOCATED SIGN LOCATION  
SEE SHEET NO. 303 TO 306
  - \*
  - NEW SIGN PANEL WITH MUTCD CODE AND PR SIGN NUMBER  
SEE SHEET NO. 305 TO 306
  - R3-5R  
P2-2
  - E1-1 EXISTING SIGN  
SEE SHEET NO. 289 TO 300
  - E4 EXISTING SIGN TO BE REMOVED  
SEE SHEET NO. 301 TO 302



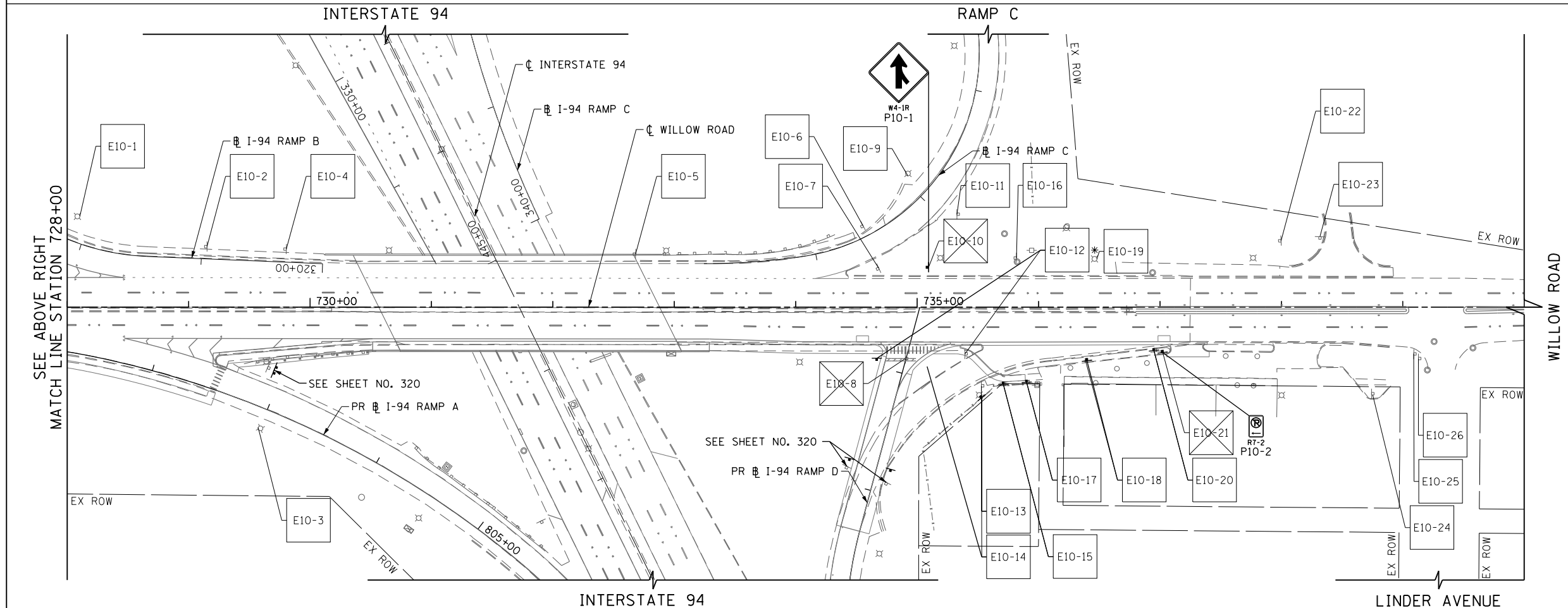
- NOTE:**
1. ALL SIGNS RELOCATED, REPLACED, OR FURNISHED NEW BY THE CONTRACTOR SHALL BE PLACED FIRM INTO THE GROUND. ANY LOOSE SIGN POSTS WITHIN THE PROJECT LIMITS SHALL BE FIXED INTO THE GROUND BY THE CONTRACTOR SO THAT THE SIGNS DO NOT MOVE OR SWAY AT NO ADDITIONAL COST TO THE CONTRACT.
  2. FOR SIGN PANEL AND STRUCTURAL MOUNTING DETAILS SEE SHEET NO. 321 TO 333

FILE NAME =	USER NAME = BAW\ort	DESIGNED - JLV	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>WILLOW ROAD SIGNING PLAN</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
G:\CH11\0158\Road\Sheets\160T35-SHT-SIGN05.dgn		DRAWN - MTS	REVISED -			305	(1920.01,1518,2022&1922,4BR)	COOK	919	310	
PLOT SCALE = 100.000' / in.		CHECKED - RCB	REVISED -			CONTRACT NO. 60T35					
PLOT DATE = 11/1/2012		DATE - 10/31/2012	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					

SCALE: 1" = 50'    SHEET NO. 310 OF 919 SHEETS    STA. 693+00    TO STA. 716+00



- LEGEND**
- ▬ PROPOSED / RELOCATED SIGN LOCATION  
SEE SHEET NO. 303 TO 306
  - \* SIGN PANEL REMOVED WHILE SIGN PANEL  
ASSEMBLY REMAINS OR IS RELOCATED  
SEE SHEET NO. 289 TO 306
  - NEW SIGN PANEL WITH MUTCD CODE AND  
PR SIGN NUMBER  
SEE SHEET NO. 305 TO 306
  - ◻ E1-1 EXISTING SIGN  
SEE SHEET NO. 289 TO 300
  - ⊗ EXISTING SIGN TO BE REMOVED  
SEE SHEET NO. 301 TO 302



- NOTE:**
1. ALL SIGNS RELOCATED, REPLACED, OR FURNISHED NEW BY THE CONTRACTOR SHALL BE PLACED FIRM INTO THE GROUND. ANY LOOSE SIGN POSTS WITHIN THE PROJECT LIMITS SHALL BE FIXED INTO THE GROUND BY THE CONTRACTOR SO THAT THE SIGNS DO NOT MOVE OR SWAY AT NO ADDITIONAL COST TO THE CONTRACT.
  2. FOR SIGN PANEL AND STRUCTURAL MOUNTING DETAILS SEE SHEET NO. 321 TO 333

FILE NAME =	USER NAME = BAW\rtort	DESIGNED - JLV	REVISED -
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		CHECKED - RCB	REVISED -
		DATE - 10/31/2012	REVISED -






**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

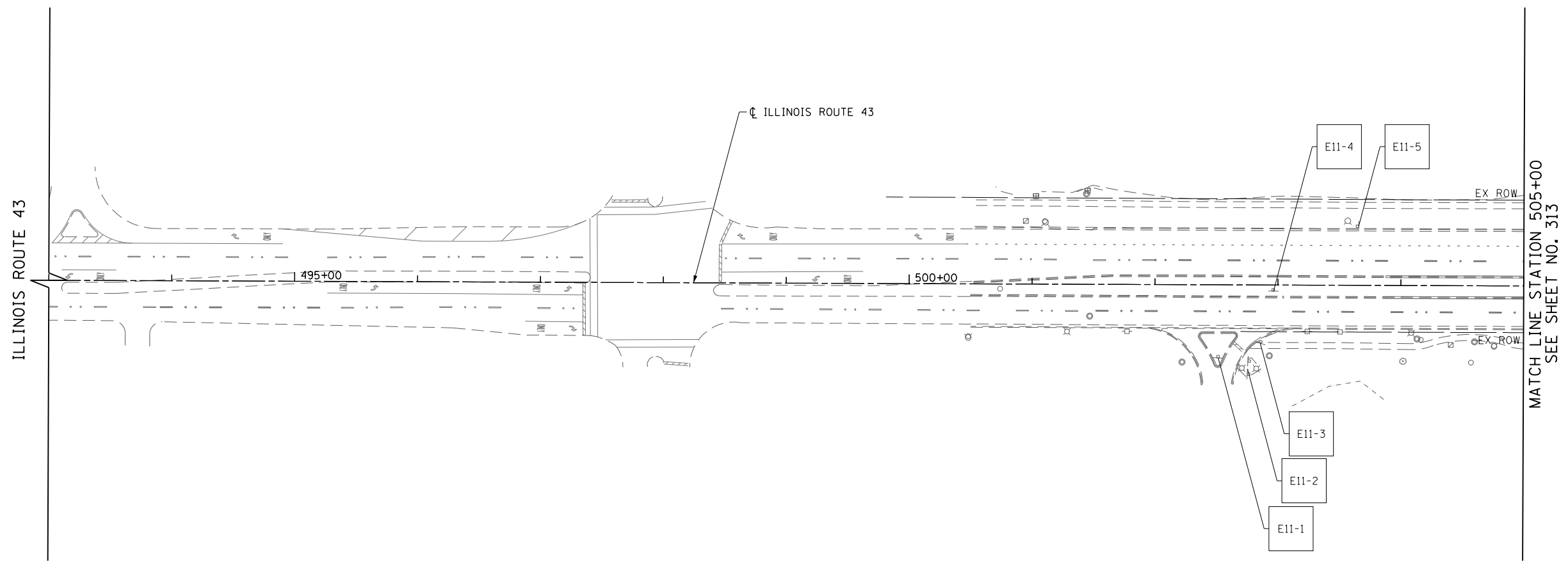
**WILLOW ROAD SIGNING PLAN**

SCALE: 1" = 50'    SHEET NO. 311 OF 919 SHEETS    STA. 716+00    TO STA. 740+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920.01,1518,2022&1922.4)R	COOK	919	311
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60T35	

**LEGEND**

-  PROPOSED / RELOCATED SIGN LOCATION  
SEE SHEET NO. 303 TO 306
-  SIGN PANEL REMOVED WHILE SIGN PANEL  
ASSEMBLY REMAINS OR IS RELOCATED  
SEE SHEET NO. 289 TO 306
-  NEW SIGN PANEL WITH MUTCD CODE AND  
PR SIGN NUMBER  
SEE SHEET NO. 305 TO 306  
R3-5R  
P2-2
-  EXISTING SIGN  
SEE SHEET NO. 289 TO 300
-  EXISTING SIGN TO BE REMOVED  
SEE SHEET NO. 301 TO 302

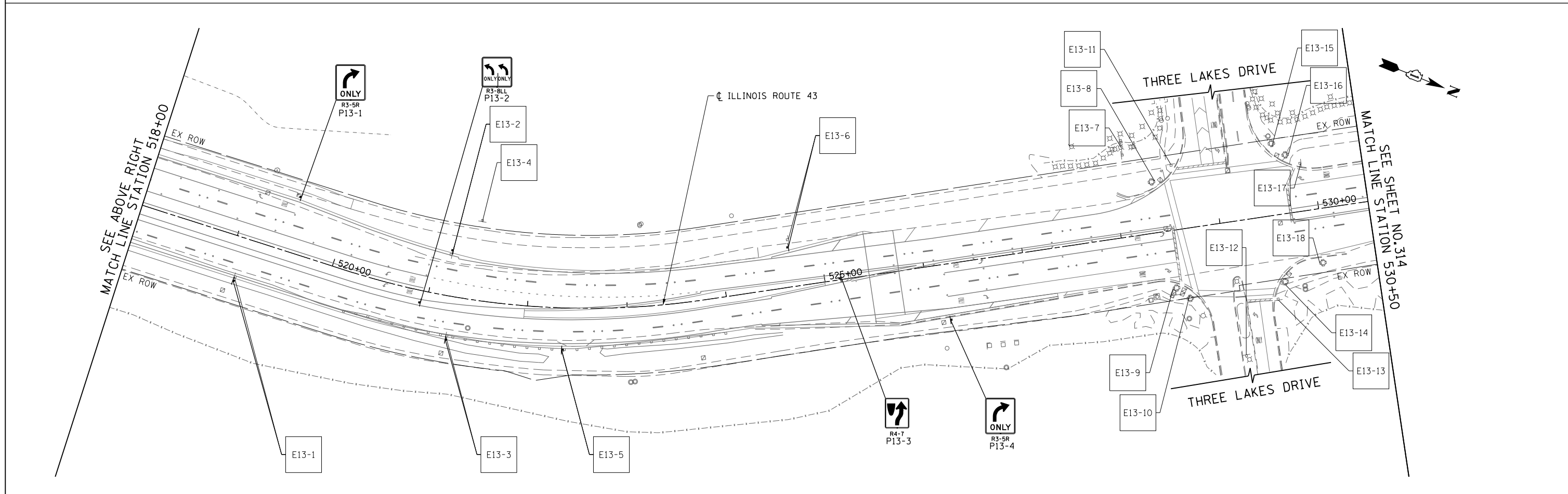
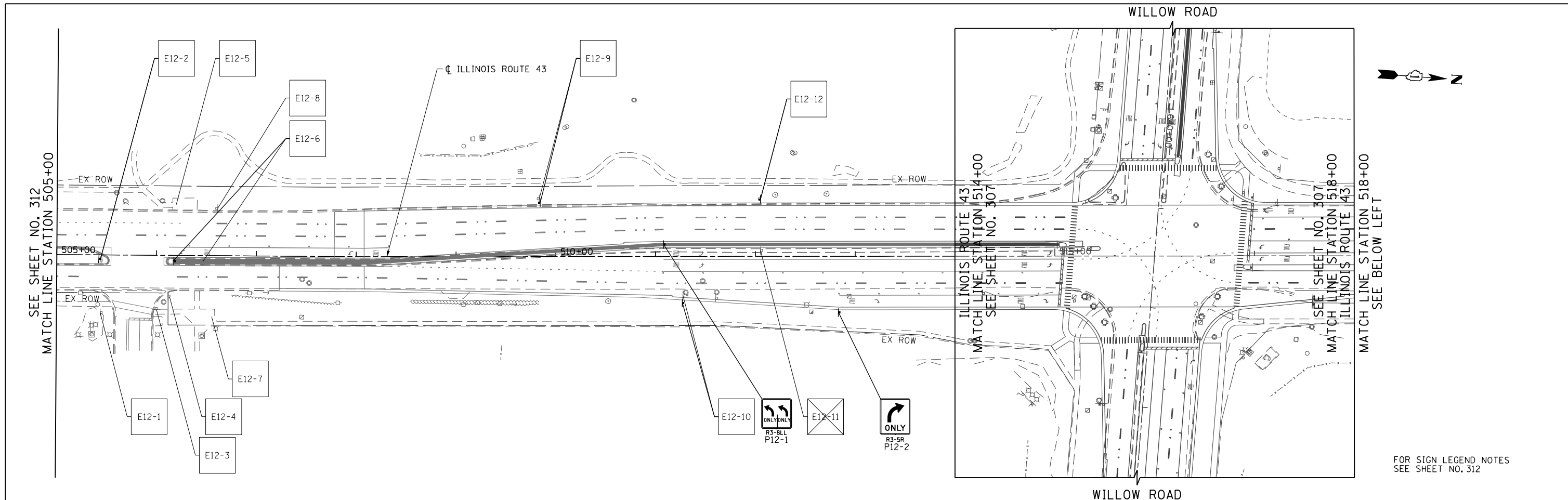


- NOTE:**
1. ALL SIGNS RELOCATED, REPLACED, OR FURNISHED NEW BY THE CONTRACTOR SHALL BE PLACED FIRM INTO THE GROUND. ANY LOOSE SIGN POSTS WITHIN THE PROJECT LIMITS SHALL BE FIXED INTO THE GROUND BY THE CONTRACTOR SO THAT THE SIGNS DO NOT MOVE OR SWAY AT NO ADDITIONAL COST TO THE CONTRACT.
  2. FOR SIGN PANEL AND STRUCTURAL MOUNTING DETAILS SEE SHEET NO. 321 TO 333

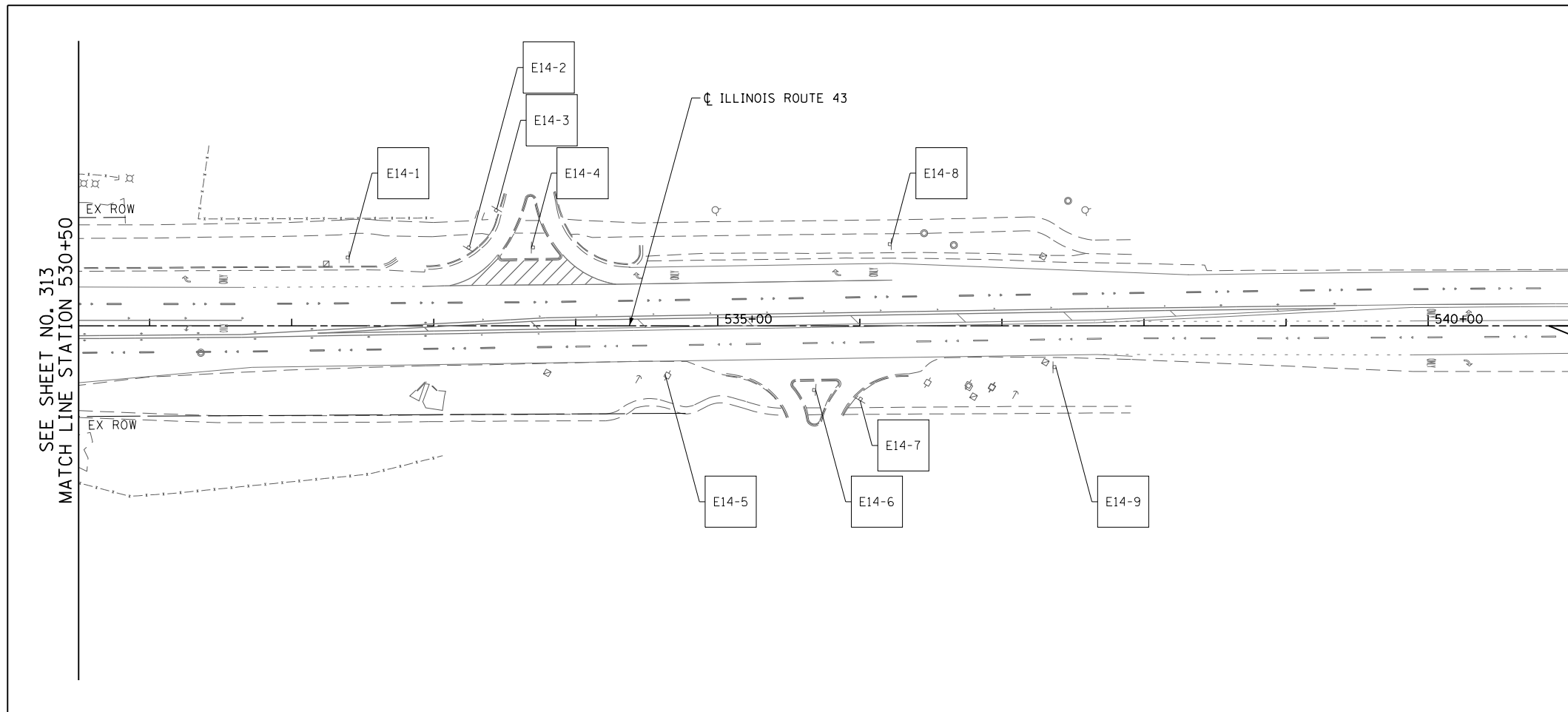
FILE NAME =	USER NAME = BAWtor.t	DESIGNED - JLV	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ILLINOIS ROUTE 43 SIGNING PLAN</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
G:\CH11\0158\Road\Sheets\0160T35-SHT-SIGN08.dgn	DRAWN - MTS	REVISIED -				305	(1920.01,1518,2022&1922.4)R	COOK	919	312	
PLOT SCALE = 100.000' / in.	CHECKED - RCB	REVISIED -				CONTRACT NO. 60T35					
PLOT DATE = 11/1/2012	DATE - 10/31/2012	REVISIED -				FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT					

SCALE: 1" = 50'    SHEET NO. 312 OF 919 SHEETS    STA. 493+00    TO STA. 505+00





FILE NAME = G:\CH11\0158\Road\Sheets\0160T35-SHT-SIGN09.dgn	USER NAME = BAW\ort	DESIGNED - JLV	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ILLINOIS ROUTE 43 SIGNING PLAN</b>		F.A.P. RTE. 305	SECTION (1920.01,1518,2022&1922.4)R	COUNTY COOK	TOTAL SHEETS 919	SHEET NO. 313	
PLOT SCALE = 100.000' / in.	CHECKED - RCB	REVISIED -	REVISIED -		SCALE: 1" = 50'	SHEET NO. 313 OF 919 SHEETS	STA. 505+00 TO STA. 530+50	CONTRACT NO. 60T35				
PLOT DATE = 11/30/2012	DATE - 10/31/2012	REVISIED -	REVISIED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							



- LEGEND**
- ▬ PROPOSED / RELOCATED SIGN LOCATION  
SEE SHEET NO. 303 TO 306
  - \* SIGN PANEL REMOVED WHILE SIGN PANEL  
ASSEMBLY REMAINS OR IS RELOCATED  
SEE SHEET NO. 289 TO 306
  - NEW SIGN PANEL WITH MUTCD CODE AND  
PR SIGN NUMBER  
SEE SHEET NO. 305 TO 306  
R3-5R  
P2-2
  - E1-1 EXISTING SIGN  
SEE SHEET NO. 289 TO 300
  - E1-4 EXISTING SIGN TO BE REMOVED  
SEE SHEET NO. 301 TO 302

- NOTE:**
1. ALL SIGNS RELOCATED, REPLACED, OR FURNISHED NEW BY THE CONTRACTOR SHALL BE PLACED FIRM INTO THE GROUND. ANY LOOSE SIGN POSTS WITHIN THE PROJECT LIMITS SHALL BE FIXED INTO THE GROUND BY THE CONTRACTOR SO THAT THE SIGNS DO NOT MOVE OR SWAY AT NO ADDITIONAL COST TO THE CONTRACT.
  2. FOR SIGN PANEL AND STRUCTURAL MOUNTING DETAILS SEE SHEET NO. 321 TO 333

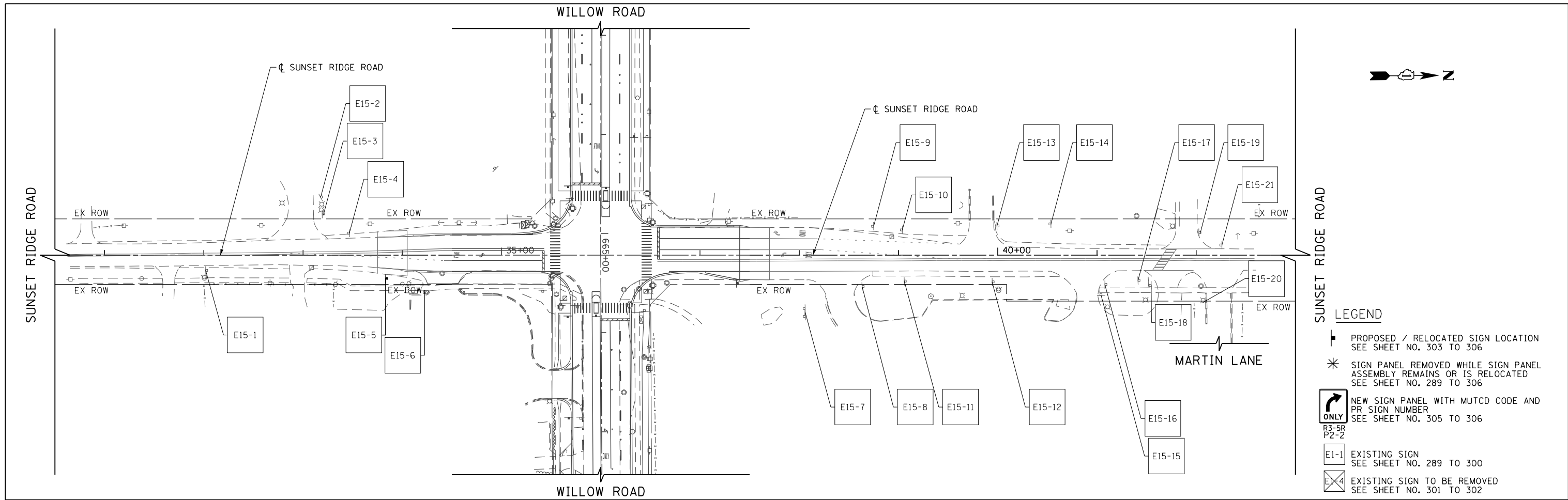
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G:\CH11\0158\Road\Sheets\0160T35-SHT-SIGN\0.dgn		DRAWN - MTS	REVISED -
		CHECKED - RCB	REVISED -
		DATE - 10/31/2012	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ILLINOIS ROUTE 43 SIGNING PLAN**

SCALE: 1" = 50'    SHEET NO. 314 OF 919 SHEETS    STA. 530+50    TO STA. 541+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920.01,1518,2022&1922.4)R	COOK	919	314
CONTRACT NO. 60T35				
FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT				



- LEGEND**
- ▬ PROPOSED / RELOCATED SIGN LOCATION  
SEE SHEET NO. 303 TO 306
  - \* SIGN PANEL REMOVED WHILE SIGN PANEL  
ASSEMBLY REMAINS OR IS RELOCATED  
SEE SHEET NO. 289 TO 306
  - NEW SIGN PANEL WITH MUTCD CODE AND  
PR SIGN NUMBER  
SEE SHEET NO. 305 TO 306
  - EXISTING SIGN  
SEE SHEET NO. 289 TO 300
  - EXISTING SIGN TO BE REMOVED  
SEE SHEET NO. 301 TO 302

- NOTE:**
1. ALL SIGNS RELOCATED, REPLACED, OR FURNISHED NEW BY THE CONTRACTOR SHALL BE PLACED FIRM INTO THE GROUND. ANY LOOSE SIGN POSTS WITHIN THE PROJECT LIMITS SHALL BE FIXED INTO THE GROUND BY THE CONTRACTOR SO THAT THE SIGNS DO NOT MOVE OR SWAY AT NO ADDITIONAL COST TO THE CONTRACT.
  2. FOR SIGN PANEL AND STRUCTURAL MOUNTING DETAILS SEE SHEET NO. 321 TO 333

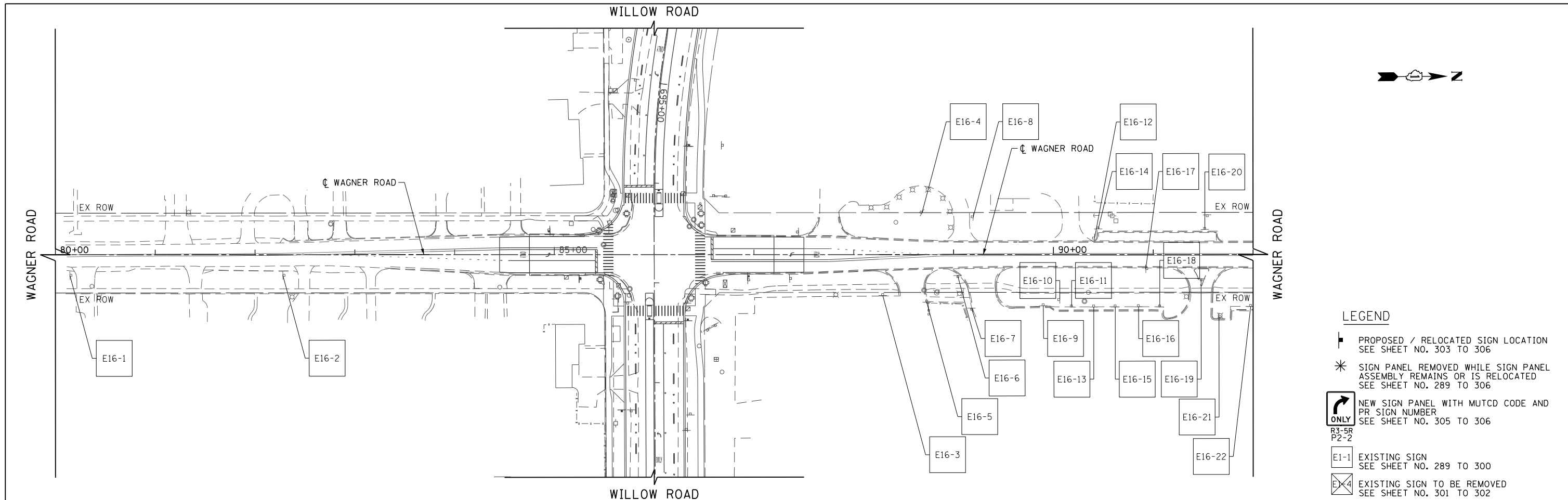
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G:\CH11\0158\Road\Sheets\0160T35-SHT-SIGN1.dgn		DRAWN - MTS	REVISED -
	PLOT SCALE = 100.000' / in.	CHECKED - RCB	REVISED -
	PLOT DATE = 11/30/2012	DATE - 10/31/2012	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUNSET RIDGE ROAD SIGNING PLAN**

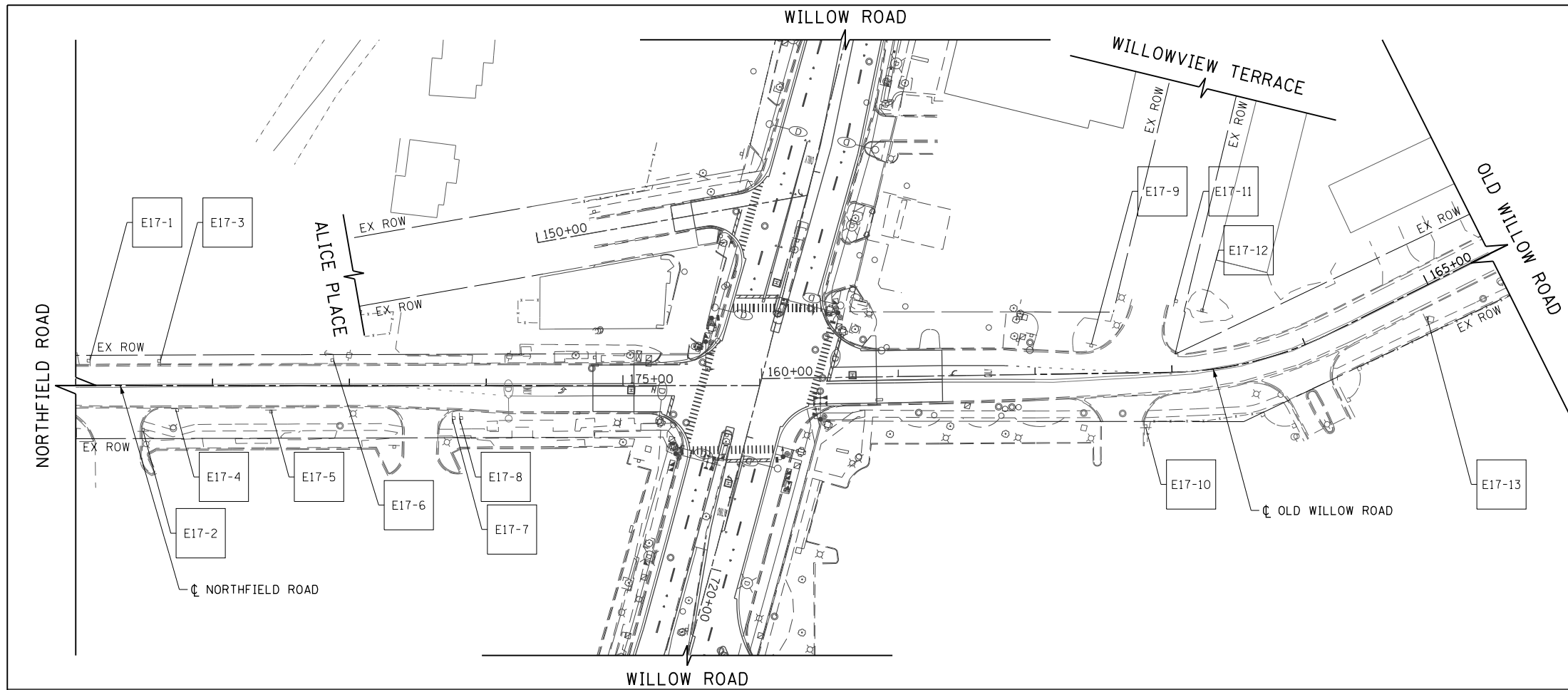
SCALE: 1" = 50'    SHEET NO. 315 OF 919 SHEETS    STA. 30+50    TO STA. 43+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920.01,1518,2022&1922.4)R	COOK	919	315
CONTRACT NO. 60T35				
FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT				








- NOTE:**
1. ALL SIGNS RELOCATED, REPLACED, OR FURNISHED NEW BY THE CONTRACTOR SHALL BE PLACED FIRM INTO THE GROUND. ANY LOOSE SIGN POSTS WITHIN THE PROJECT LIMITS SHALL BE FIXED INTO THE GROUND BY THE CONTRACTOR SO THAT THE SIGNS DO NOT MOVE OR SWAY AT NO ADDITIONAL COST TO THE CONTRACT.
  2. FOR SIGN PANEL AND STRUCTURAL MOUNTING DETAILS SEE SHEET NO. 321 TO 333

FILE NAME = G:\CH11\0158\Road\Sheets\0160T35-SHT-SIGN12.dgn	USER NAME = BAW\rtort	DESIGNED - JLV	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>WAGNER ROAD SIGNING PLAN</b>		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN - MTS	REVISED -		SCALE: 1" = 50'	SHEET NO. 316 OF 919 SHEETS	STA. 80+00	TO STA. 92+00	305	(1920.01,1518,2022&1922.4)R	COOK	919 316
		PLOT SCALE = 100.000' / in.	CHECKED - RCB		REVISED -	CONTRACT NO. 60T35						
		PLOT DATE = 11/30/2012	DATE - 10/31/2012		REVISED -	FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT						



**LEGEND**

- 
 PROPOSED / RELOCATED SIGN LOCATION  
 SEE SHEET NO. 303 TO 306
- 
 SIGN PANEL REMOVED WHILE SIGN PANEL  
 ASSEMBLY REMAINS OR IS RELOCATED  
 SEE SHEET NO. 289 TO 306
- 
 NEW SIGN PANEL WITH MUTCD CODE AND  
 PR SIGN NUMBER  
 SEE SHEET NO. 305 TO 306  
R3-5R  
P2-2
- 
 EXISTING SIGN  
 SEE SHEET NO. 289 TO 300
- 
 EXISTING SIGN TO BE REMOVED  
 SEE SHEET NO. 301 TO 302

**NOTE:**

1. ALL SIGNS RELOCATED, REPLACED, OR FURNISHED NEW BY THE CONTRACTOR SHALL BE PLACED FIRM INTO THE GROUND. ANY LOOSE SIGN POSTS WITHIN THE PROJECT LIMITS SHALL BE FIXED INTO THE GROUND BY THE CONTRACTOR SO THAT THE SIGNS DO NOT MOVE OR SWAY AT NO ADDITIONAL COST TO THE CONTRACT.
2. FOR SIGN PANEL AND STRUCTURAL MOUNTING DETAILS SEE SHEET NO. 321 TO 333

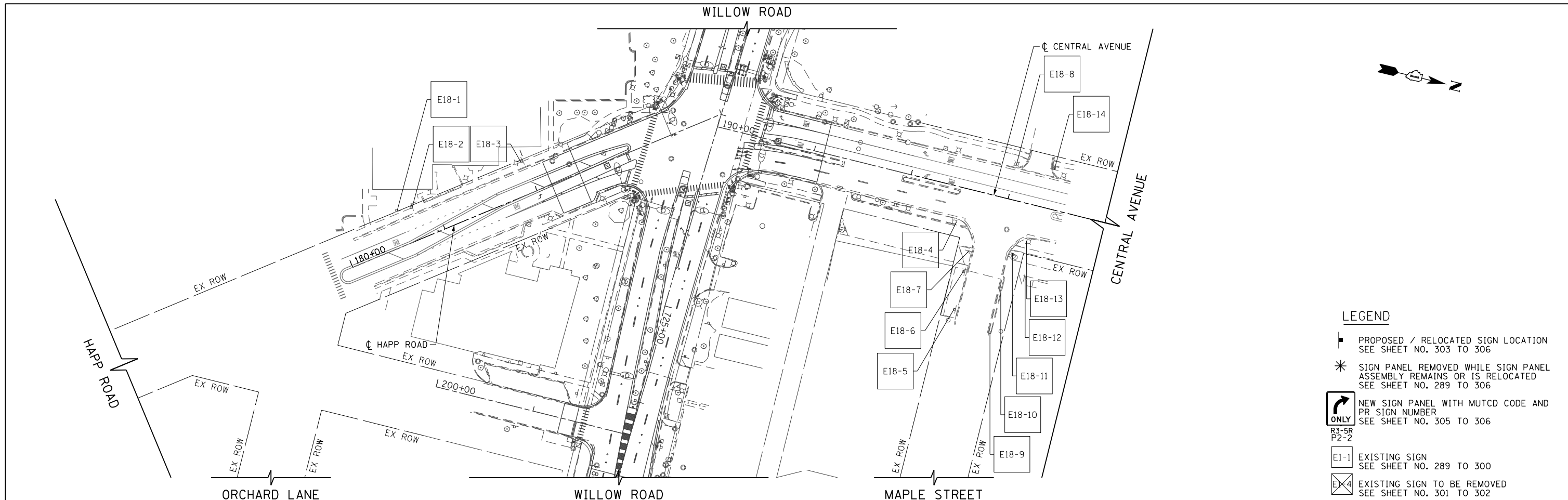
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G:\CH11\0158\Road\Sheets\0160T35-SHT-SIGN\N12A.dgn		DRAWN - MTS	REVISED -
	PLOT SCALE = 100.000' / in.	CHECKED - RCB	REVISED -
	PLOT DATE = 11/1/2012	DATE - 10/31/2012	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**NORTHFIELD ROAD / OLD WILLOW ROAD SIGNING PLAN**

SCALE: 1" = 50'    SHEET NO. 317 OF 919 SHEETS    STA. 165+50    TO STA. 171+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920.01,1518,2022&1922.4B)R	COOK	919	317
				CONTRACT NO. 60T35
FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT				



- LEGEND**
- ▬ PROPOSED / RELOCATED SIGN LOCATION  
SEE SHEET NO. 303 TO 306
  - \* SIGN PANEL REMOVED WHILE SIGN PANEL ASSEMBLY REMAINS OR IS RELOCATED  
SEE SHEET NO. 289 TO 306
  - NEW SIGN PANEL WITH MUTCD CODE AND PR SIGN NUMBER  
SEE SHEET NO. 305 TO 306
  - E1-1 EXISTING SIGN  
SEE SHEET NO. 289 TO 300
  - E1-4 EXISTING SIGN TO BE REMOVED  
SEE SHEET NO. 301 TO 302

- NOTE:**
1. ALL SIGNS RELOCATED, REPLACED, OR FURNISHED NEW BY THE CONTRACTOR SHALL BE PLACED FIRM INTO THE GROUND. ANY LOOSE SIGN POSTS WITHIN THE PROJECT LIMITS SHALL BE FIXED INTO THE GROUND BY THE CONTRACTOR SO THAT THE SIGNS DO NOT MOVE OR SWAY AT NO ADDITIONAL COST TO THE CONTRACT.
  2. FOR SIGN PANEL AND STRUCTURAL MOUNTING DETAILS SEE SHEET NO. 321 TO 333

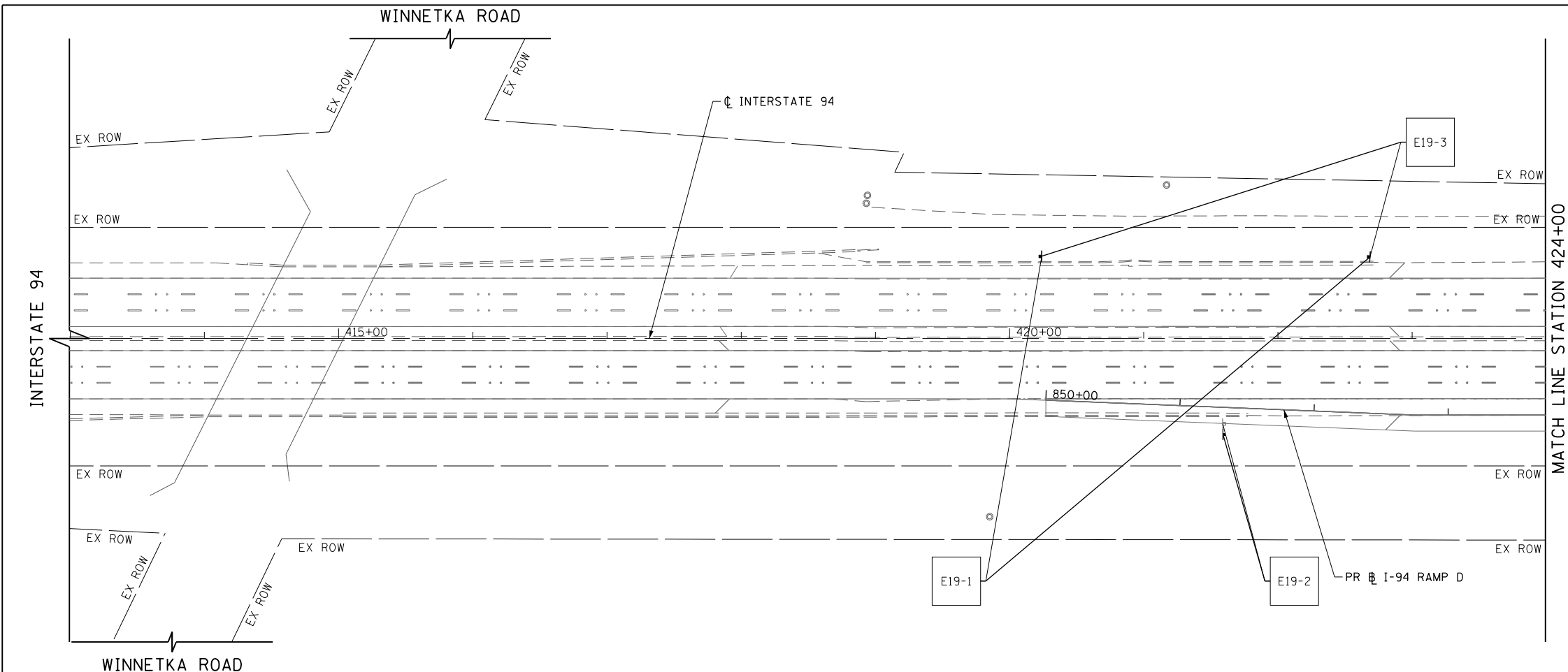
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G:\CH11\0158\Road\Sheets\0160T35-SHT-SIGN\13A.dgn		DRAWN - MTS	REVISED -
		CHECKED - RCB	REVISED -
		DATE - 10/31/2012	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**






**HAPP ROAD / CENTRAL AVENUE SIGNING PLAN**

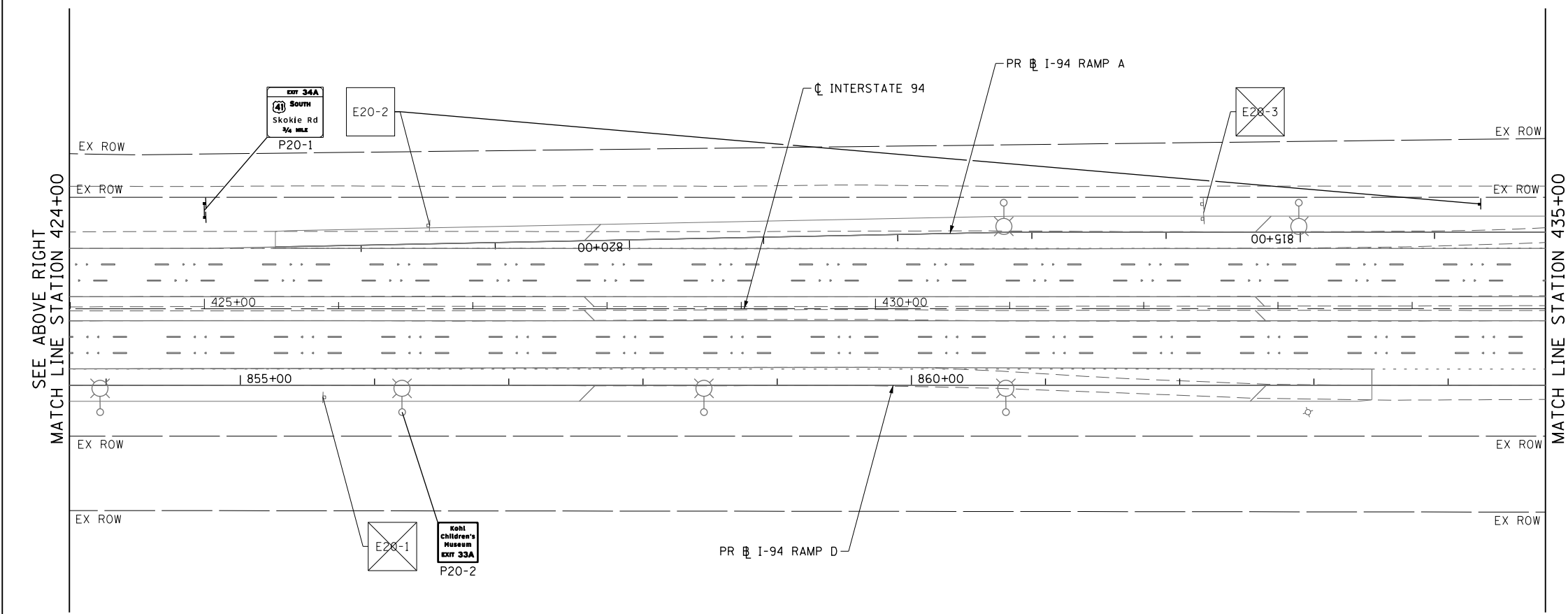
SCALE: 1" = 50'    SHEET NO. 318 OF 919 SHEETS    STA. 180+00    TO STA. 194+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920.01,1518,2022&1922.4)R	COOK	919	318
				CONTRACT NO. 60T35
FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT				



MATCH LINE STATION 424+00  
SEE BELOW LEFT

- LEGEND**
-  PROPOSED / RELOCATED SIGN LOCATION  
SEE SHEET NO. 303 TO 306
  -  SIGN PANEL REMOVED WHILE SIGN PANEL  
ASSEMBLY REMAINS OR IS RELOCATED  
SEE SHEET NO. 289 TO 306
  -  NEW SIGN PANEL WITH MUTCD CODE AND  
PR SIGN NUMBER  
SEE SHEET NO. 305 TO 306
  -  EXISTING SIGN  
SEE SHEET NO. 289 TO 300
  -  EXISTING SIGN TO BE REMOVED  
SEE SHEET NO. 301 TO 302



MATCH LINE STATION 435+00  
SEE SHEET NO. 320

- NOTE:**
1. ALL SIGNS RELOCATED, REPLACED, OR FURNISHED NEW BY THE CONTRACTOR SHALL BE PLACED FIRM INTO THE GROUND. ANY LOOSE SIGN POSTS WITHIN THE PROJECT LIMITS SHALL BE FIXED INTO THE GROUND BY THE CONTRACTOR SO THAT THE SIGNS DO NOT MOVE OR SWAY AT NO ADDITIONAL COST TO THE CONTRACT.
  2. FOR SIGN PANEL AND STRUCTURAL MOUNTING DETAILS SEE SHEET NO. 321 TO 333

SEE ABOVE RIGHT  
MATCH LINE STATION 424+00

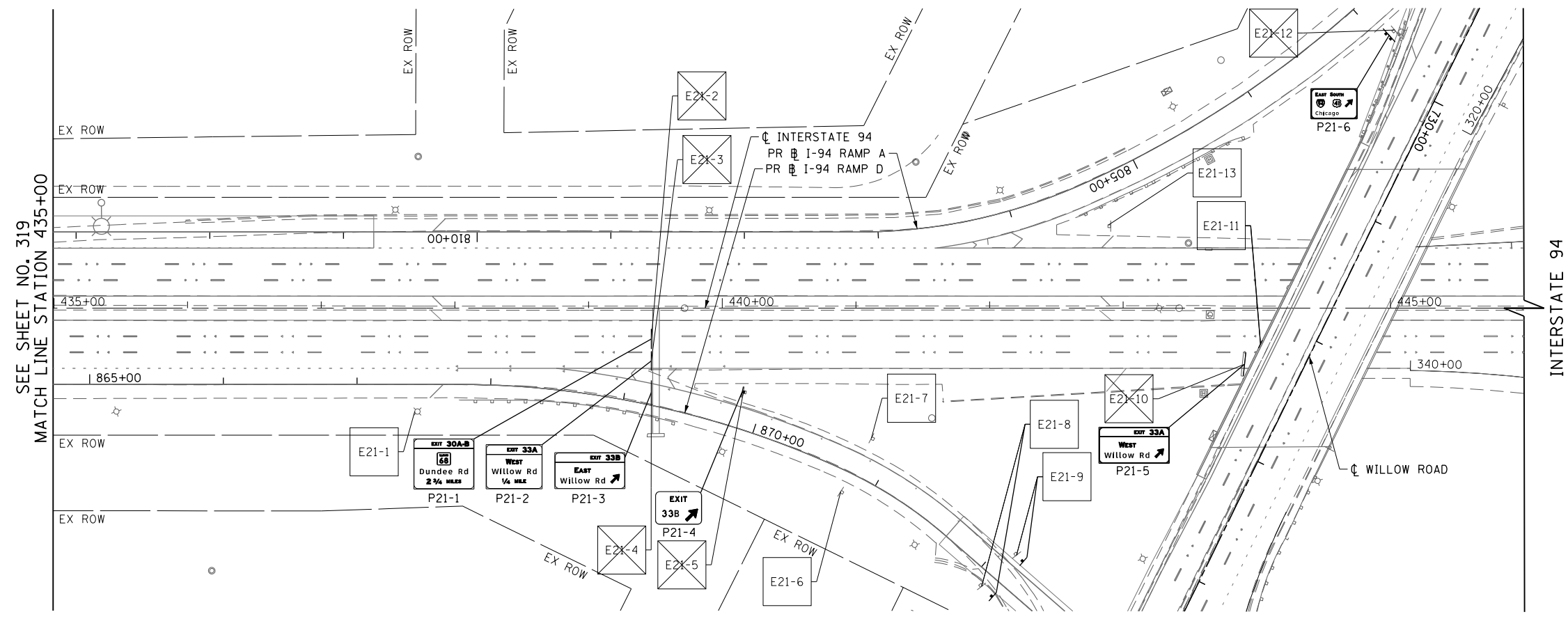
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	PLOT SCALE = 100.0000' / in.	CHECKED - RCB	REVISED -
	PLOT DATE = 11/1/2012	DATE - 10/31/2012	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**INTERSTATE 94 SIGNING PLAN**

SCALE: 1" = 50'    SHEET NO. 319 OF 919 SHEETS    STA. 413+00    TO STA. 435+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920.01,1518,2022&1922.4)R	COOK	919	319
CONTRACT NO. 60T35				
FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT				



- LEGEND**
- PROPOSED / RELOCATED SIGN LOCATION  
SEE SHEET NO. 303 TO 306
  - SIGN PANEL REMOVED WHILE SIGN PANEL  
ASSEMBLY REMAINS OR IS RELOCATED  
SEE SHEET NO. 289 TO 306
  - NEW SIGN PANEL WITH MUTCD CODE AND  
PR SIGN NUMBER  
SEE SHEET NO. 305 TO 306
  - EXISTING SIGN  
SEE SHEET NO. 289 TO 300
  - EXISTING SIGN TO BE REMOVED  
SEE SHEET NO. 301 TO 302

- NOTE:**
1. ALL SIGNS RELOCATED, REPLACED, OR FURNISHED NEW BY THE CONTRACTOR SHALL BE PLACED FIRM INTO THE GROUND. ANY LOOSE SIGN POSTS WITHIN THE PROJECT LIMITS SHALL BE FIXED INTO THE GROUND BY THE CONTRACTOR SO THAT THE SIGNS DO NOT MOVE OR SWAY AT NO ADDITIONAL COST TO THE CONTRACT.
  2. FOR SIGN PANEL AND STRUCTURAL MOUNTING DETAILS SEE SHEET NO. 321 TO 333

FILE NAME =	USER NAME = BAW\tor.t	DESIGNED - JLV	REVISED -
G:\CH11\0158\Road\Sheets\0160T35-SHT-SIGN\14.dgn		DRAWN - MNB	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED - RCB	REVISED -
	PLOT DATE = 11/1/2012	DATE - 10/31/2012	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

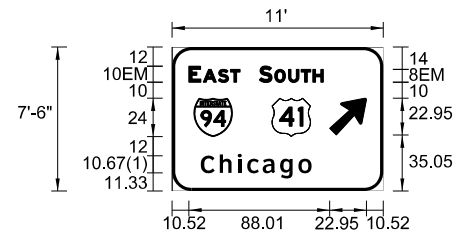
**INTERSTATE 94 SIGNING PLAN**

SCALE: 1" = 50'    SHEET NO. 320 OF 919 SHEETS    STA. 435+00    TO STA. 446+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920.01,1518,2022&1922.4B)R	COOK	919	320
				CONTRACT NO. 60T35
FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT				



SIGN DETAIL  
1:100



FONT:  
(1) ClearviewHwy-5-W

Panel Style: guide\_exp\_interchange.ssi

Dimensions are in Inches, Tenths

Word lengths and spaces between words are shown

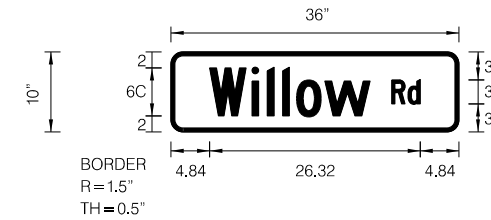
M.U.T.C.D.: 2009 Edition

SIGN NUMBER	P9-11 & P21-6
WIDTH x HGHT.	132" x 90"
BORDER WIDTH	2"
CORNER RADIUS	12"
MOUNTING	Ground (Wood Posts)
BACKGROUND	TYPE: ZZ COLOR: Green
LEGEND/BORDER	TYPE: ZZ COLOR: White

SYMBOL	ROT	X	Y	WID	HT
AR_Type A	45	98.53	35.05	18.25	29.25
M1_1	0	13.62	34	24	24
M1_4	0	62.49	34	24	24

LETTER POSITIONS (X)												LENGTH	SERIESSIZE
	E		A	S	T							EM 2000	
10.52	7.4	1.6	22.88								89.6	31.88	10.8
	S		O	U	T	H						EM 2000	
54.4	8.11	1.59	30.4								37.5	40.1	10.8
	C	h	i	c	a	g	o					ClearviewHwy-5-W	
17.86	69.3										44.84	69.3	10.67/8.68

SIGN DETAIL  
1:20



Panel Style: Street Name 6-4in.ssi

Dimensions are in Inches, Tenths

Word lengths and spaces between words are shown

M.U.T.C.D.: 2009 Edition

SIGN NUMBER	P5-3a, P8-4
WIDTH x HGHT.	36" x 10"
BORDER WIDTH	0.5"
CORNER RADIUS	1.5"
MOUNTING	Ground
BACKGROUND	TYPE: A COLOR: Green
LEGEND/BORDER	TYPE: A COLOR: White

SYMBOL	ROT	X	Y	WID	HT

LETTER POSITIONS (X)												LENGTH	SERIESSIZE
	W	i	l	l	o	w						C 2000	
4.84	20.05										11.11	20.05	64.5
	R	d										C 2000	
27.59	3.57		4.84									3.57	32.25

FILE NAME = G:\CH11\0158\Road\Sheets\0160T35-SHT-SIGNDETAILS.dgn	USER NAME = BAWtor.t	DESIGNED - JLV	REVISED -
		DRAWN - MNB	REVISED -
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	PLOT DATE = 11/1/2012	DATE - 10/31/2012	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

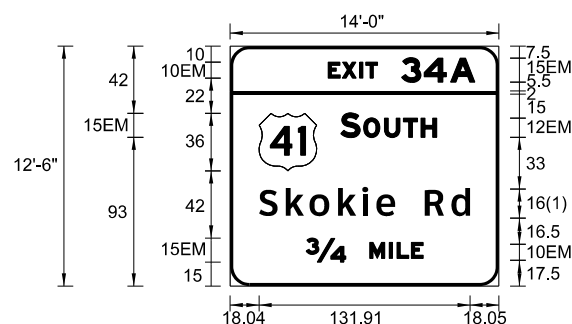
INTERSTATE 94 SIGN DETAILS

SCALE: N.T.S SHEET NO. 321 OF 919 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920,01,1518,2022&1922.4)R	COOK	919	321
CONTRACT NO. 60T35			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

**SIGN DETAIL**

1:100



FONT:  
(1) ClearviewHwy-5-W

Panel Style: guide\_exp\_overhead.ssi

Dimensions are in Inches, Tenths

Word lengths and spaces between words are shown

M.U.T.C.D.: 2009 Edition

SIGN NUMBER	P20-1
WIDTH x HGHT.	168" x 150"
BORDER WIDTH	2"
CORNER RADIUS	12"
MOUNTING	Ground (Steel Posts)
BACKGROUND	TYPE: ZZ COLOR: Green
LEGEND/BORDER	TYPE: ZZ COLOR: White

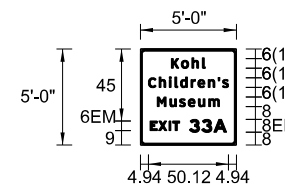
SYMBOL	ROT	X	Y	WID	HT
M1_4	0	18.04	72	36	36

**LETTER POSITIONS (X)**

	E	X	I	T	3	4	A			LENGTH	SERIESSIZE
											EM 2000
61.3	30.81				14.99	45.91		15		91.71	10,15
	S		O	U	T	H					EM 2000
69.05	12.15	2.25	45.61					38.95		60.01	15,12
	S	k	o	k	i	e		R	d		ClearviewHwy-5-W
18.04	131.92								18.05	131.92	16'13
	34		M	I	L	E					EM 2000
47.36	24.98	14.99	33.31					47.36		73.28	15,10

**SIGN DETAIL**

1:100



FONT:  
(1) ClearviewHwy-5-W

Panel Style: guide\_con\_recreational.ssi

Dimensions are in Inches, Tenths

Word lengths and spaces between words are shown

M.U.T.C.D.: 2009 Edition

SIGN NUMBER	P20-2
WIDTH x HGHT.	60" x 60"
BORDER WIDTH	1"
CORNER RADIUS	6"
MOUNTING	Ground (Light Pole)
BACKGROUND	TYPE: ZZ COLOR: Brown
LEGEND/BORDER	TYPE: ZZ COLOR: White

SYMBOL	ROT	X	Y	WID	HT
--------	-----	---	---	-----	----

**LETTER POSITIONS (X)**

	K	o	h	l						LENGTH	SERIESSIZE
											ClearviewHwy-5-W
19.43	21.14				19.43					21.14	6'4.88
	C	h	i	l	d	r	e	n	'	s	ClearviewHwy-5-W
4.94	50.12								4.94	50.12	6'4.88
	M	u	s	e	u	m					ClearviewHwy-5-W
10.56	38.89						10.55			38.89	6'4.88
	E	X	I	T	3	3	A				EM 2000
6.08	18.48				6	23.37		6.07		47.85	6,8

FILE NAME =	USER NAME = BAW:ort	DESIGNED - JLV	REVISED -
G:\CH11\0158\Road\Sheets\0160T35-SHT-SIGNDETAILS.dgn		DRAWN - MNB	REVISED -
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	PLOT DATE = 11/1/2012	DATE - 10/31/2012	REVISED -

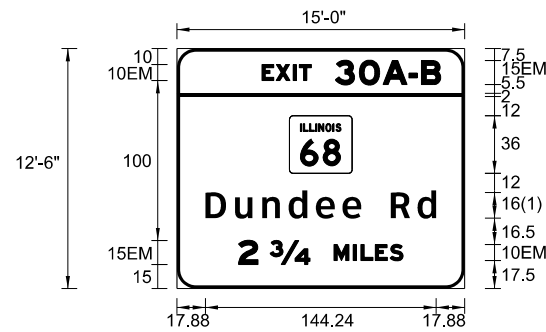
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**INTERSTATE 94 SIGN DETAILS**

SCALE: N.T.S SHEET NO. 322 OF 919 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920,01,1518,2022&1922,4BR	COOK	919	322
CONTRACT NO. 60T35				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SIGN DETAIL  
1:100



FONT:  
(1) ClearviewHwy-5-W

Panel Style: guide\_exp\_overhead.ssi

Dimensions are in Inches, Tenths

Word lengths and spaces between words are shown

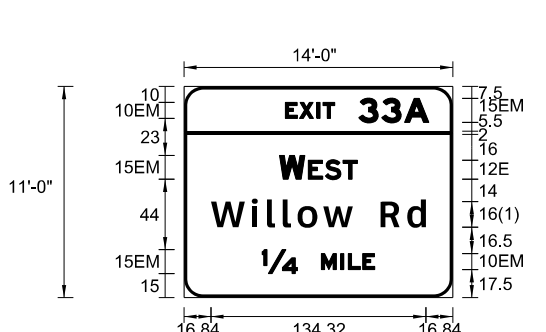
M.U.T.C.D.: 2009 Edition

SIGN NUMBER	P21-1
WIDTH x HGHT.	180" x 150"
BORDER WIDTH	2"
CORNER RADIUS	12"
MOUNTING	Overhead (Truss Mount)
BACKGROUND	TYPE: ZZ COLOR: Green
LEGEND/BORDER	TYPE: ZZ COLOR: White

SYMBOL	ROT	X	Y	WID	HT
M1-H100A-2-22-10D	0	73.37	72	39.29	36

LETTER POSITIONS (X)														LENGTH	SERIESSIZE	
	E	X	I	T		3	0	A	-	B					EM 2000	
53.2	30.8				15	66									111.8	10,15
	D	u	n	d	e	e		R	d						144.24	ClearviewHwy-5-W
17.88	144.24														17.88	1613
	2		34		M	I	L	E	S							EM 2000
38.59	12.15	7.5	24.98	14.99	43.21										102.83	15,10

SIGN DETAIL  
1:100



FONT:  
(1) ClearviewHwy-5-W

Panel Style: guide\_exp\_overhead.ssi

Dimensions are in Inches, Tenths

Word lengths and spaces between words are shown

M.U.T.C.D.: 2009 Edition

SIGN NUMBER	P21-2
WIDTH x HGHT.	168" x 132"
BORDER WIDTH	2"
CORNER RADIUS	12"
MOUNTING	Overhead (Truss Mount)
BACKGROUND	TYPE: ZZ COLOR: Green
LEGEND/BORDER	TYPE: ZZ COLOR: White

SYMBOL	ROT	X	Y	WID	HT

LETTER POSITIONS (X)														LENGTH	SERIESSIZE		
	E	X	I	T		3	3	A							EM 2000		
63.4	30.8				15	43.8									89.6	10,15	
	W		E	S	T											EM 2000	
59.22	15.91	1.59	31.45			59.83									48.95	15,12	
	W	i	l	l	o	w		R	d							ClearviewHwy-5-W	
16.84	134.32														16.84	1613	
	1/4		M	I	L	E										EM 2000	
48.86	21.99	14.99	33.31												48.85	70.29	15,10

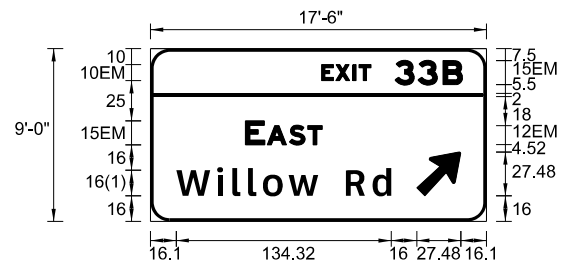
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PLOT SCALE = 10.0000" / in.		CHECKED - RCB	REVISED -
PLOT DATE = 11/1/2012		DATE - 10/31/2012	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**INTERSTATE 94 SIGN DETAILS**  
SCALE: N.T.S SHEET NO. 323 OF 919 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920,01,1518,2022&1922,4)BR	COOK	919	323
CONTRACT NO. 60T35				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SIGN DETAIL  
1:100



FONT:  
(1) ClearviewHwy-5-W

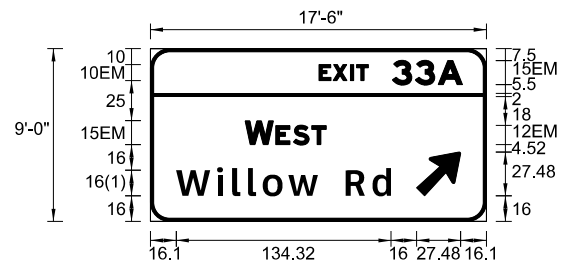
Panel Style: guide\_exp\_overhead.ssi  
 Dimensions are in Inches, Tenths  
 Word lengths and spaces between words are shown M.U.T.C.D.: 2009 Edition

SIGN NUMBER	P21-3
WIDTH x HGHT.	210" x 108"
BORDER WIDTH	2"
CORNER RADIUS	12"
MOUNTING	Overhead (Truss Mount)
BACKGROUND	TYPE: ZZ COLOR: Green
LEGEND/BORDER	TYPE: ZZ COLOR: White

SYMBOL	ROT	X	Y	WID	HT
AR_Type A	45	166.42	16	22.16	34.91

		LETTER POSITIONS (X)		LENGTH		SERIES SIZE						
	E	X	I	T	3	3	B			EM 2000		
106.9	30.8				15	42.3		15		88.1	10,15	
	E		A	S	T						EM 2000	
59.22	11.11	1.59	34.33			103.75				47.03	15,12	
	W	i	l	l	o	w		R	d		ClearviewHwy-5-W	
16.1	134.32									59.58	134.32	16/13

SIGN DETAIL  
1:100



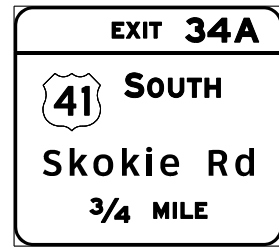
FONT:  
(1) ClearviewHwy-5-W

Panel Style: guide\_exp\_overhead.ssi  
 Dimensions are in Inches, Tenths  
 Word lengths and spaces between words are shown M.U.T.C.D.: 2009 Edition

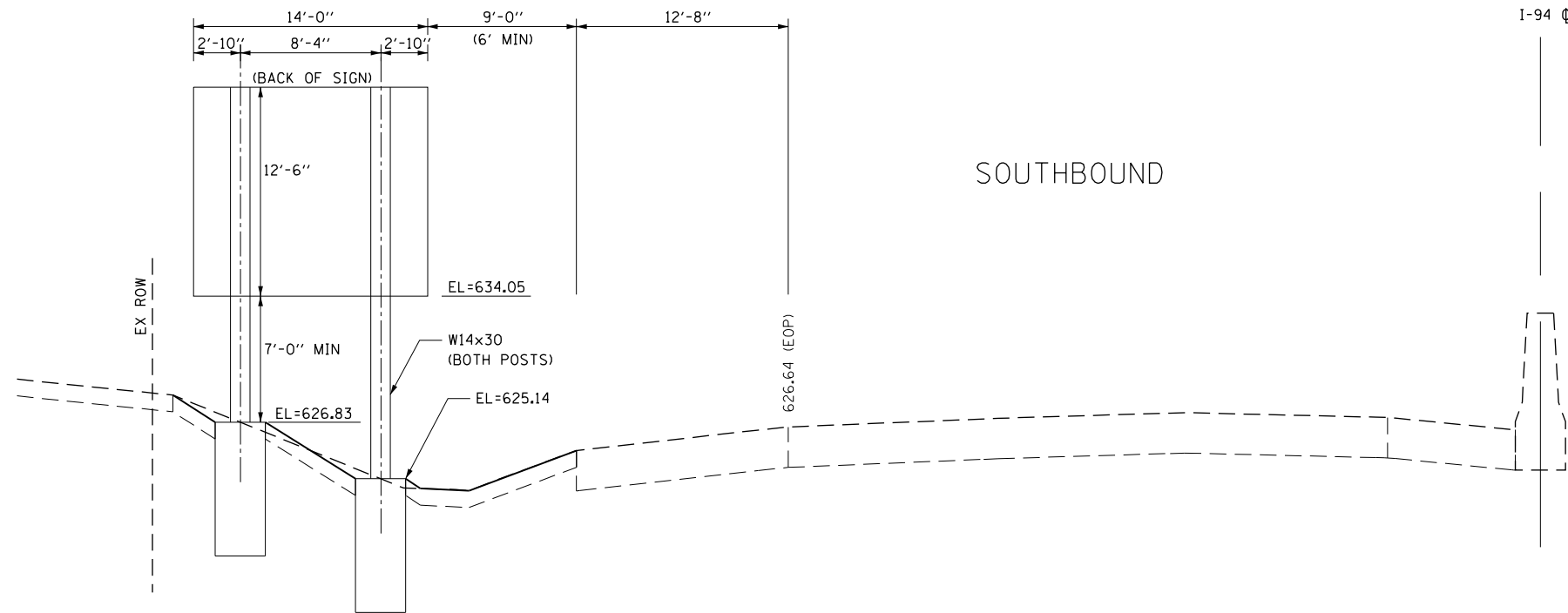
SIGN NUMBER	P21-5
WIDTH x HGHT.	210" x 108"
BORDER WIDTH	2"
CORNER RADIUS	12"
MOUNTING	Overhead (Bridge Mount)
BACKGROUND	TYPE: ZZ COLOR: Green
LEGEND/BORDER	TYPE: ZZ COLOR: White

SYMBOL	ROT	X	Y	WID	HT
AR_Type A	45	166.42	16	22.16	34.91

		LETTER POSITIONS (X)		LENGTH		SERIES SIZE						
	E	X	I	T	3	3	A			EM 2000		
105.42	30.8				15	43.8		15		89.6	10,15	
	W		E	S	T						EM 2000	
59.23	15.91	1.59	31.45			101.82				48.95	15,12	
	W	i	l	l	o	w		R	d		ClearviewHwy-5-W	
16.1	134.32									59.6	134.32	16/13

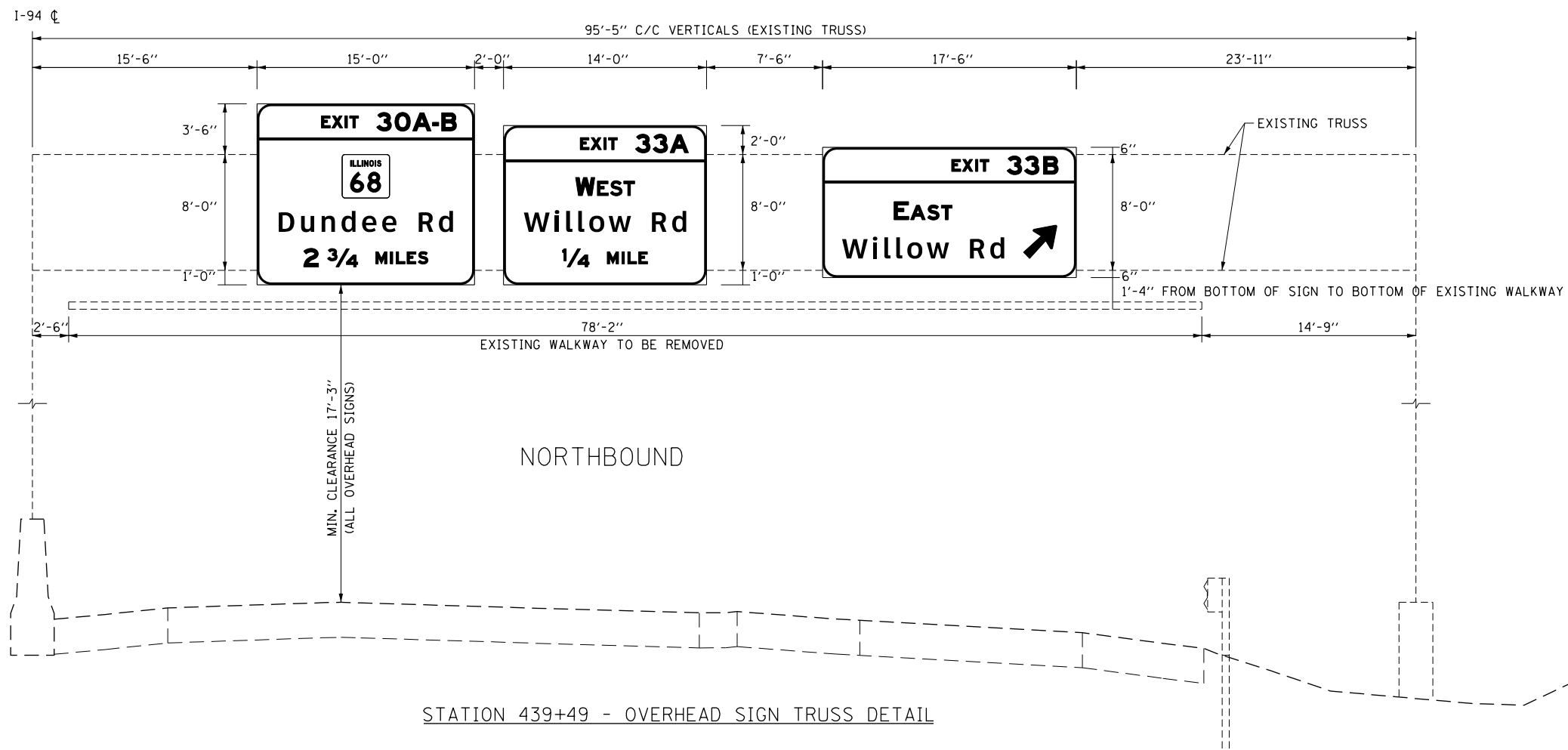


FRONT OF SIGN  
(FACING NORTH)



STATION 425+00 - GROUND MOUNT SIGN WITH BREAKAWAY POSTS

NOTE: SEE SHEET NO. 332 AND  
333 FOR SIGN POST DETAILS.



STATION 439+49 - OVERHEAD SIGN TRUSS DETAIL

FILE NAME =	USER NAME = BAW:ort	DESIGNED - JLV	REVISED -
G:\CH11\0158\Road\Sheets\0160T35-SHT-SIGNDETAILS.dgn		DRAWN - MNB	REVISED -
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	PLOT DATE = 11/1/2012	DATE - 10/31/2012	REVISED -

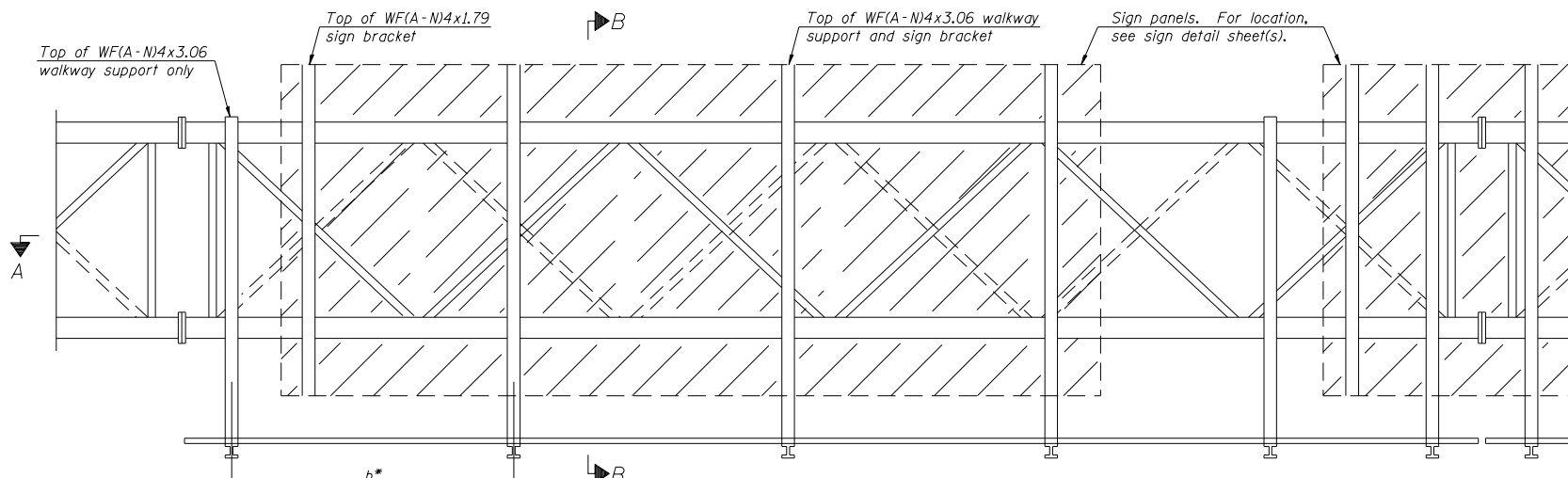
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

INTERSTATE 94 SIGN DETAILS

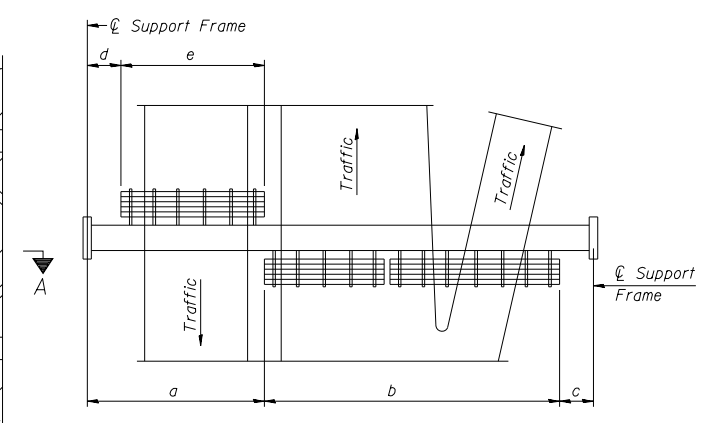
SCALE: N.T.S. SHEET NO. 325 OF 919 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920.01,1518,2022&1922.4)R	COOK	919	325
CONTRACT NO. 60T35				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





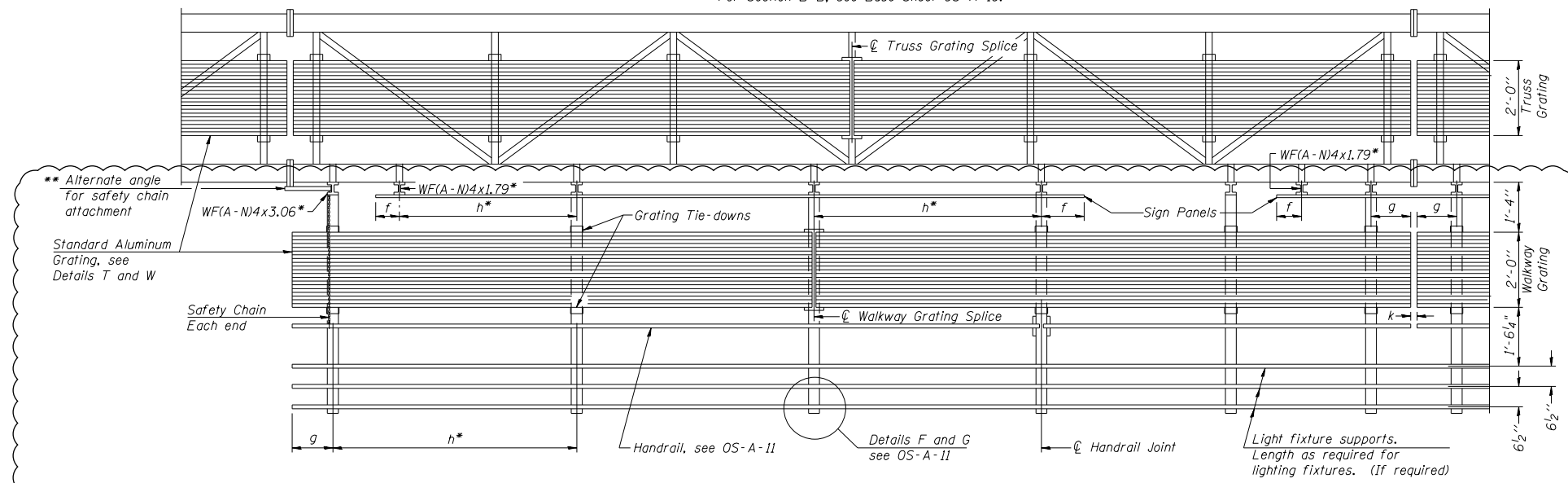
**TYPICAL FRONT ELEVATION**  
With lights and handrail omitted for clarity.  
For Section B-B, see Base Sheet OS-A-10.



**PLAN WALKWAY AND HANDRAIL SKETCH**  
(Road plan beneath truss varies)

**BRACKET TABLE**

WF(A-N)4x1.79 or WF(A-N)4x3.06 ASTM B308, Alloy 6061-T6		
Sign Width		Number Brackets Required
Greater Than	Less Than or Equal To	
	8'-0"	2
8'-0"	14'-0"	3
14'-0"	20'-0"	4
20'-0"	26'-0"	5
26'-0"	32'-0"	6



**SECTION A-A**

Handrail and walkway shall span a minimum of three brackets between splices and/or gap joints. Place all sign and walkway brackets as close to panel points as practical. Handrail joints, grating, and light support splices placed as needed.

Structure Number	Station	a	b	c	d	e	Walkway Grating and Handrail Lengths
1S0161094L033.0	439+49	N/A	N/A	N/A	N/A	N/A	N/A

- Notes:
- Space walkway brackets WF(A-N)4x3.06 and sign brackets WF(A-N)4x1.79 for efficiency and within limits shown:
  - f = 12" maximum, 4" minimum (End of sign to center of nearest bracket)
  - g = 12" maximum, 4" minimum (End of walkway grating to center of nearest support bracket)
  - h = 6'-0" maximum (center to center of sign and/or walkway support brackets, WF(A-N)4x1.79 or WF(A-N)4x3.06)
  - k = 2" maximum gap between adjacent walkway grating sections and handrail ends
  - \*\* If walkway bracket at safety chain location is behind sign, add angle to bracket, see Alternate Safety Chain Attachment on Base Sheet OS-A-11.

For Details T and W, Section B-B and Grating Splice Details see Base Sheet OS-A-10.  
For Handrail Details see Base Sheet OS-A-11.

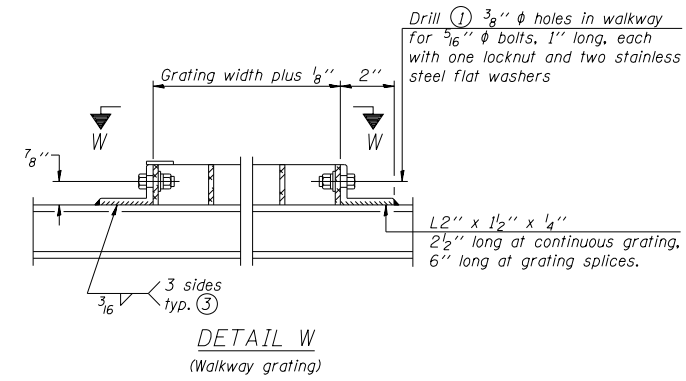
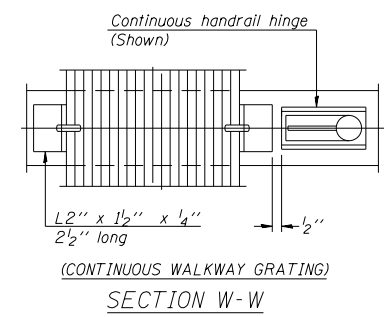
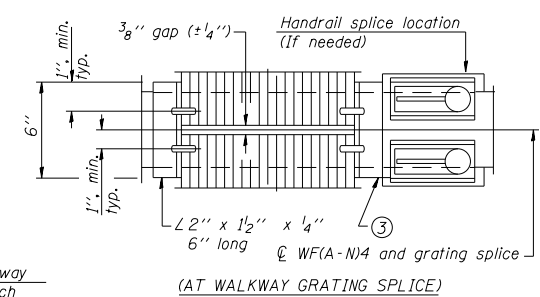
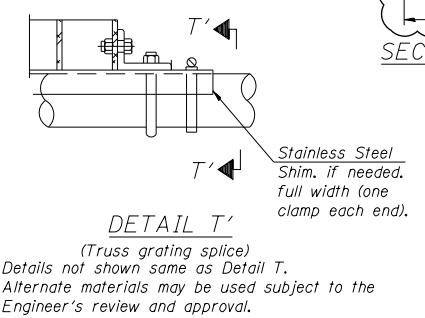
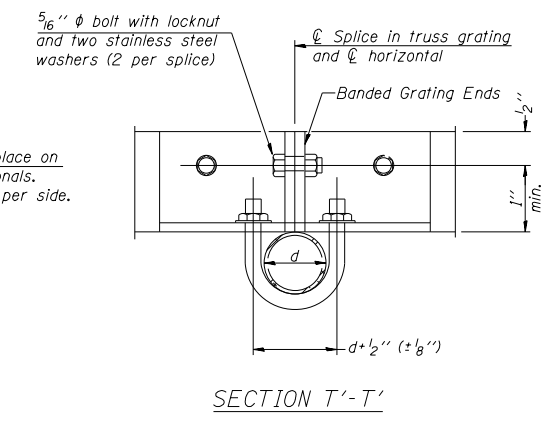
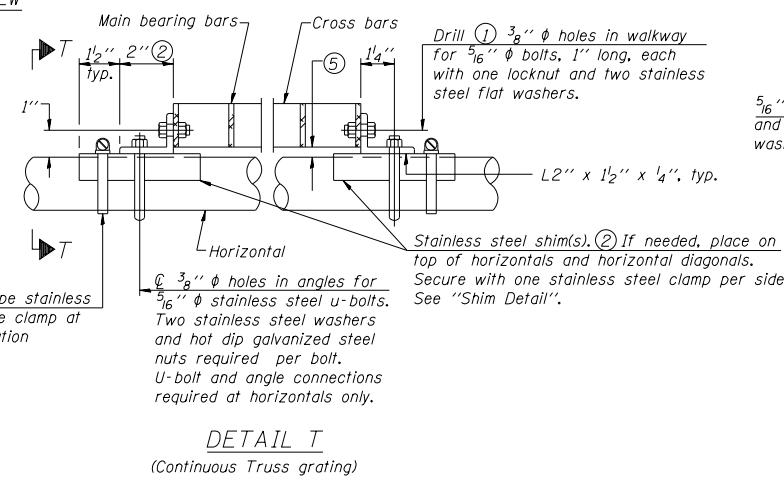
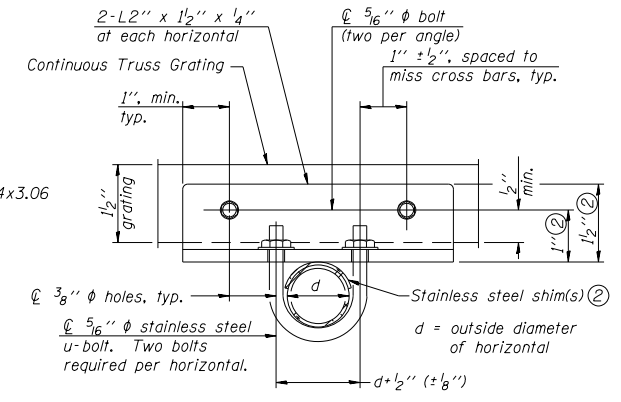
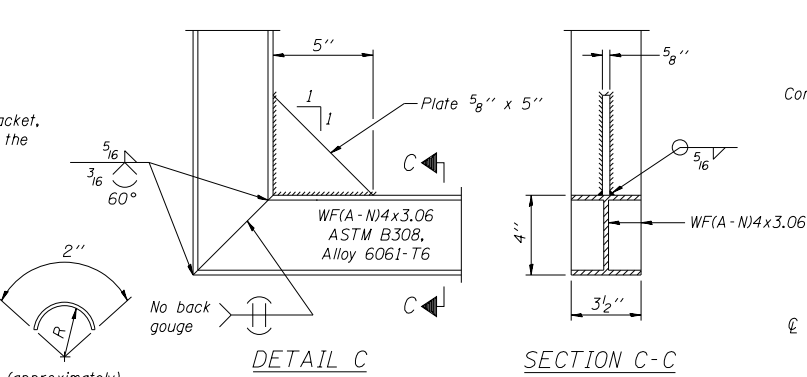
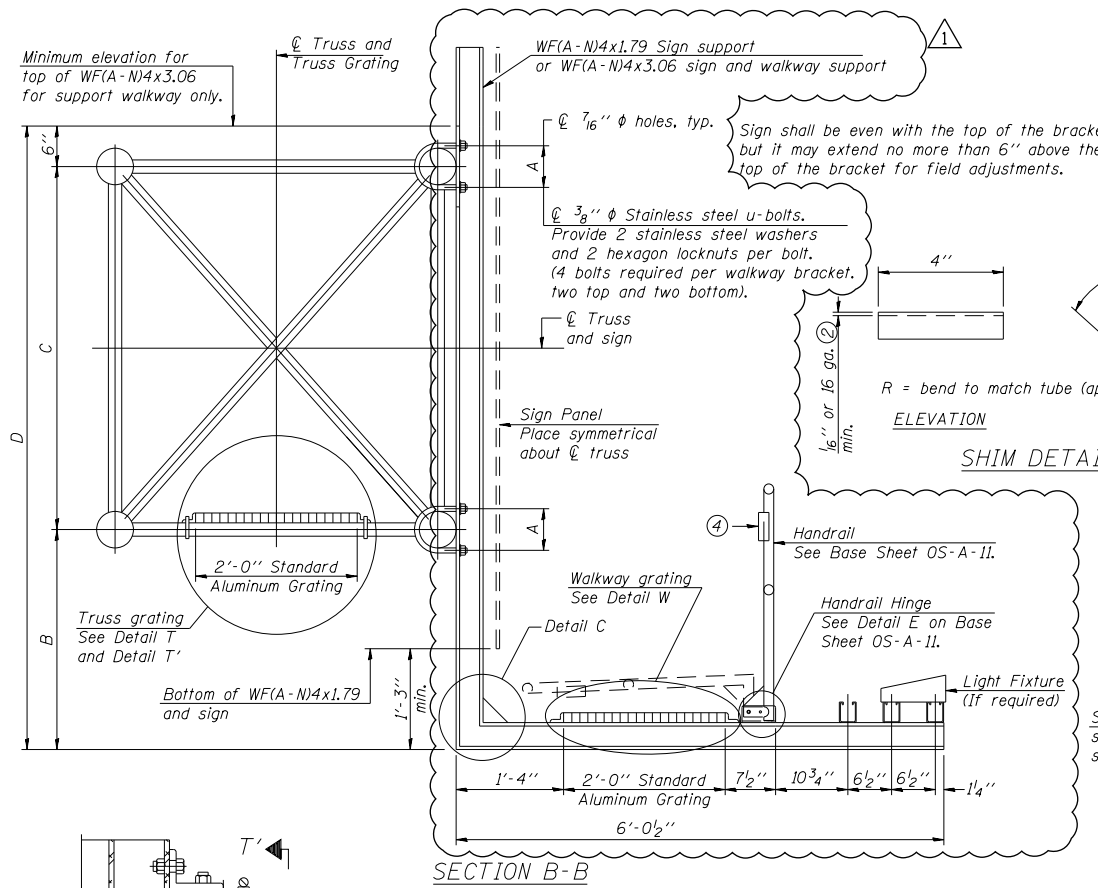
Truss grating to facilitate inspection shall run full length (center to center of support frames) ±12" on overhead trusses. Cost of truss grating is included in "Overhead Sign Structure".

Walkway and Truss Grating width dimensions are nominal and may vary ±1/2" based on available standard widths.

**REVISION NOTES:**

- 1 EXISTING WALKWAY TO BE REMOVED (PAID FOR AS REMOVE OVERHEAD SIGN STRUCTURE - WALKWAY). THE EXISTING WALKWAY WILL NOT BE REPLACED.
- ALL EXISTING SIGN PANELS, BRACKETS SUPPORTING WALKWAY, SIGN, AND RELATED HARDWARE ARE TO BE REMOVED (PAID FOR AS REMOVE AND RE-ERECT EXISTING SIGN, PER EACH SIGN). ITEM WILL INCLUDE NEW BRACKETS AND ALL NECESSARY HARDWARE TO MOUNT PROPOSED SIGN PANELS. SIGN PANELS WILL BE PAID FOR AS SIGN PANEL - TYPE 3.

OS-A-9 6-1-12



**SPECIFICATIONS FOR STANDARD ALUMINUM GRATING**

Main Bearing Bars shall be 3/16" x 1 1/2" on 1 3/4" centers and conform to ASTM B221 Alloy 6061-T6.  
 Cross bars shall be 3/16" x 1 1/2" on 4" centers and conform to ASTM B221 Alloy 6063-T5 or 6061-T6.

OR

Aluminum Grating with modified "T" sections for main bearing bars shall meet the following requirements:  
 Main bars shall conform to ASTM B221 Alloy 6061-T6 and have a minimum section modulus equal to 0.0705 in.<sup>3</sup> per bar, a depth of 1 1/2", spaced on 1 3/4" centers.  
 Cross bars shall conform to ASTM B221 Alloy 6063-T5 or T-42 and spaced on 4" centers.

Structure Number	Station	A	⑥ B	C	⑥ D
IS0161094L033.0	439+49	MATCH EX	N/A	8'	N/A

- Drilling holes in grating may be done in shop or field, based on Contractor's preference and subject to accurate alignment.
- Stainless steel shims shall be placed as shown in Detail T if needed to compensate for alignment variations between horizontal and diagonal pipes beyond adjustment provided by angles. Thicker shims may be used subject to shims performing properly.
- If Handrail Joint present, weld angle to WF(A-N)4 and 1/4" extension bars. (See Base Sheet OS-A-11.)
- 1/8" x 1/2" x 2" welded to handrail posts to protect locations that contact grating.
- Tube to grating gap may vary from 0 to 1/2", max. to align walkway, allow for camber, etc.
- Based on actual height of tallest sign given on OS-A-1.

**REVISION NOTES:**

1 EXISTING WALKWAY TO BE REMOVED (PAID FOR AS REMOVE OVERHEAD SIGN STRUCTURE - WALKWAY). THE EXISTING WALKWAY WILL NOT BE REPLACED.

ALL EXISTING SIGN PANELS, BRACKETS SUPPORTING WALKWAY, SIGN, AND RELATED HARDWARE ARE TO BE REMOVED (PAID FOR AS REMOVE AND RE-ERECT EXISTING SIGN, PER EACH SIGN). ITEM WILL INCLUDE NEW BRACKETS AND ALL NECESSARY HARDWARE TO MOUNT PROPOSED SIGN PANELS. SIGN PANELS WILL BE PAID FOR AS SIGN PANEL - TYPE 3.

OS-A-10 6-1-12

FILE NAME =	USER NAME = BAW:ort	DESIGNED - JLV	REVISED - 10/31/2012
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PLOT DATE = 11/1/2012	DATE - 10/31/2012	REVISOR -	REVISOR -

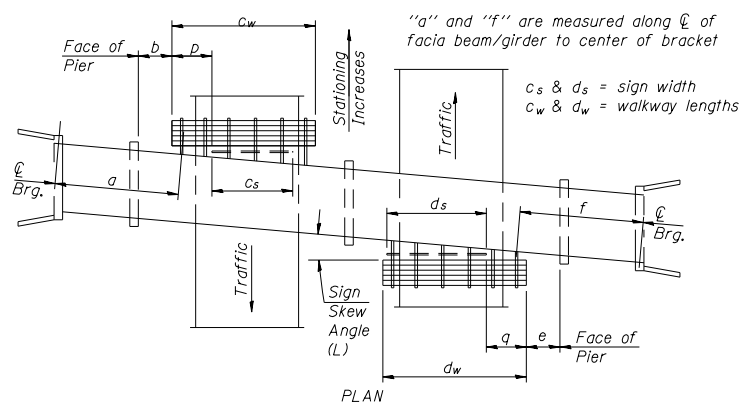
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**OVERHEAD SIGN STRUCTURES  
ALUMINUM WALKWAY DETAILS**

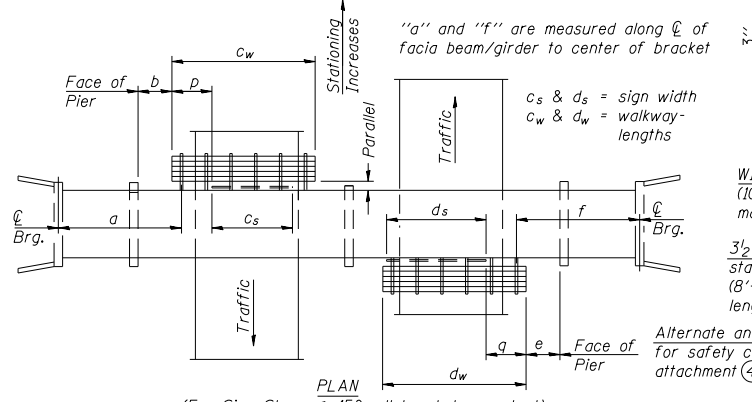
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FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60T35	

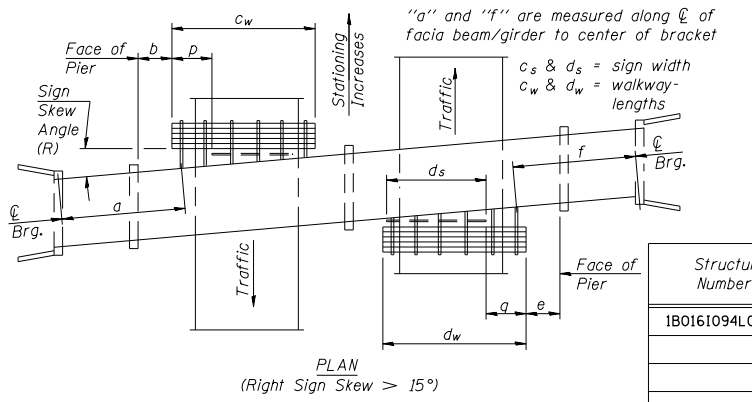




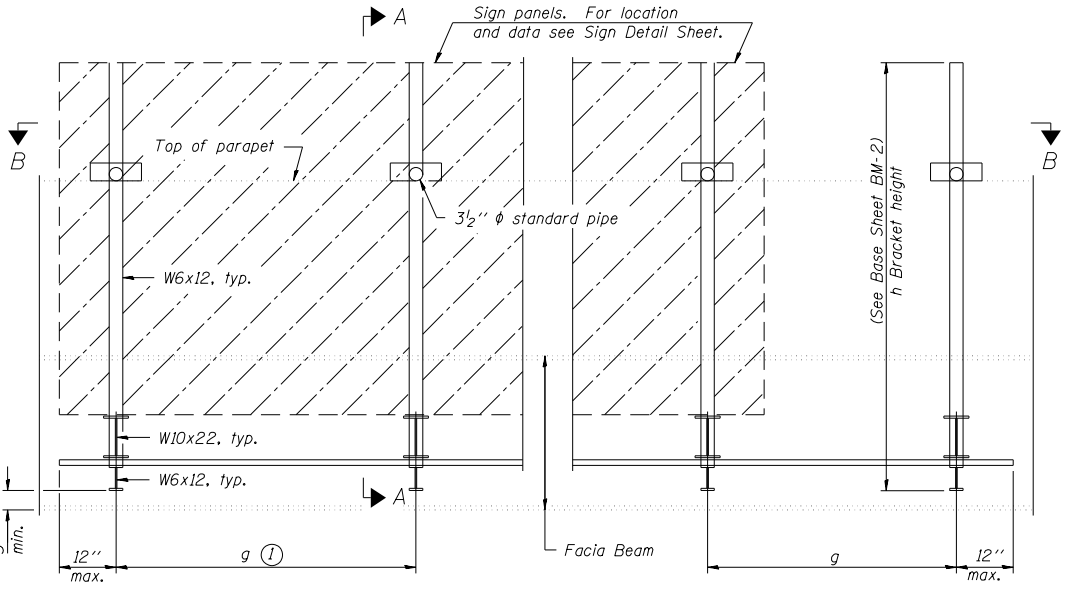
PLAN  
(Left Sign Skew > 15°)  
WALKWAY AND HANDRAIL SKETCH  
(Road plan beneath structure varies.)



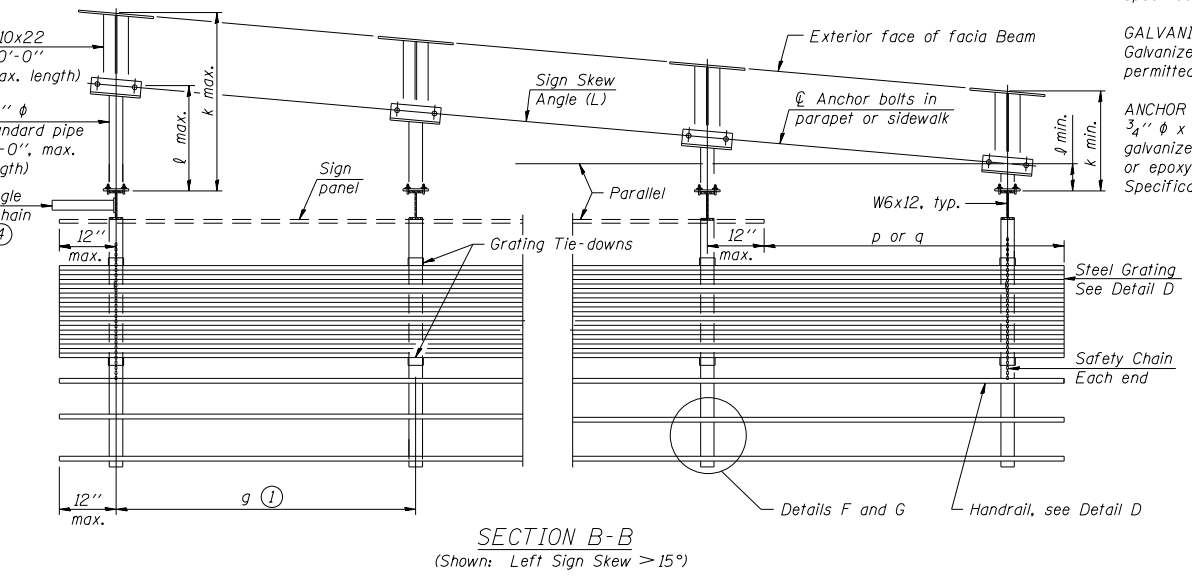
PLAN  
(For Sign Skew ≤ 15°, all brackets constant)  
WALKWAY AND HANDRAIL SKETCH  
(Road plan beneath structure varies.)



PLAN  
(Right Sign Skew > 15°)  
WALKWAY AND HANDRAIL SKETCH  
(Road plan beneath structure varies.)



TYPICAL FRONT ELEVATION  
(With lights, safety chain and handrail omitted for clarity.)



SECTION B-B  
(Shown: Left Sign Skew > 15°)

Structure Number	Sign Skew Angle (L) or (R)	Bridge Station	Bridge Structure Number	Contract Route Designation	a	b	c <sub>s</sub>	c <sub>w</sub>	d <sub>s</sub>	d <sub>w</sub>	** e	f	g	No. of Brackets (Total)	p	q	Total Grating/Hndrl. Lengths (c <sub>w</sub> + d <sub>w</sub> )
1B0161094L032.9	7°-07'-34"L	444+00	016-0536	I-94	N/A	N/A	N/A	N/A	17'-6"	N/A	8'-6"	FIELD VERIFY	4'	5	N/A	N/A	N/A

Dimensions a, b, e, f & g may vary as approved by the Engineer, see ①.  
When c<sub>w</sub> < c<sub>s</sub> and/or d<sub>w</sub> < d<sub>s</sub>, use alternate brackets without walkway supports where applicable, see ③.  
\*\* DIMENSION PROVIDED FROM CENTER OF LAST SUPPORT BEAM TO FACE OF PIER

GENERAL NOTES

**SPECIFICATIONS:**  
**DESIGN:** AASHTO Standard Specifications for Structural Supports for Highway Signs, Luminaires and Traffic Signals. ("AASHTO Specifications") ②  
**CONSTRUCTION:** Current (at time of letting) Illinois Department of Transportation Standard Specifications for Road and Bridge Construction, Supplemental Specifications and Special Provisions. ("Standard Specifications")  
**LOADING:** 90 M.P.H. WIND VELOCITY  
**WALKWAY LOADING:** Dead load plus 500 lbs. concentrated live load.  
**MINIMUM CLEARANCE:** 3" greater than bridge members at all locations. (All Obstructions)  
**WELDING:** All welds to be continuous unless otherwise shown. All welding to be done in accordance with current AWS D1.1 Structural Welding Code (Steel) and the Standard Specifications.  
**MATERIALS:** All Structural Steel Pipe shall be ASTM A53 Grade B with a minimum yield of 35,000 p.s.i., or A500 Grade B or C with a minimum yield of 46,000 p.s.i. If A500 pipe is substituted for A53, then the outside diameter shall be as detailed and wall thickness greater than or equal to A53.  
All Structural Steel Plates and Shapes shall conform to AASHTO M270 Gr. 36, Gr. 50 (M183, M223 Gr. 50.).  
**HIGH STRENGTH BOLTS:** All bolts, washers, nuts and locknuts shall satisfy the requirements of ASTM designation A307 unless noted as "H.S." which shall require AASHTO M164 (A325), ASTM A449, or approved alternate. All fasteners shall be hot dip galvanized per AASHTO M232 unless otherwise specified.  
**GALVANIZING:** All Steel Grating, Plates, Shapes and Pipe shall be Hot Dip Galvanized after fabrication in accordance with AASHTO M111. Painting is not permitted.  
**ANCHOR RODS:** All threaded rod shall conform to ASTM F1554 Grade 105, 3/4" φ x 12" long, each with one plate washer and locknut and be hot dip galvanized per AASHTO M232. They shall be either cast into the concrete or epoxy grouted in accordance with Section 584 of the Standard Specifications. Minimum embedment in concrete shall be 9".

- ① Bracket spacing g ≤ 6'-0", max. Spacing shall be uniform if possible but may vary ±6" to miss existing obstruction (rail post, light poles, web stiffeners, splice plates, etc.). Adjust bracket lengths accordingly on skewed structures.
- ② Any design modifications shall be based on the current version of applicable specifications and submitted for the Engineer's approval.
- ③ Unit price includes grating, handrail, brackets, supports, anchor bolts, fasteners, fabrication, delivery, erection, field drilling and other necessary items. Limits of payment are based on grating length (c<sub>w</sub>, d<sub>w</sub>) unless otherwise specified. For Safety Chain Details and Details D, F and G, see Base Sheet BM-4.
- ④ If walkway bracket at safety chain location is behind sign, add angle to bracket. See detail on Base Sheet BM-4.

TOTAL BILL OF MATERIAL

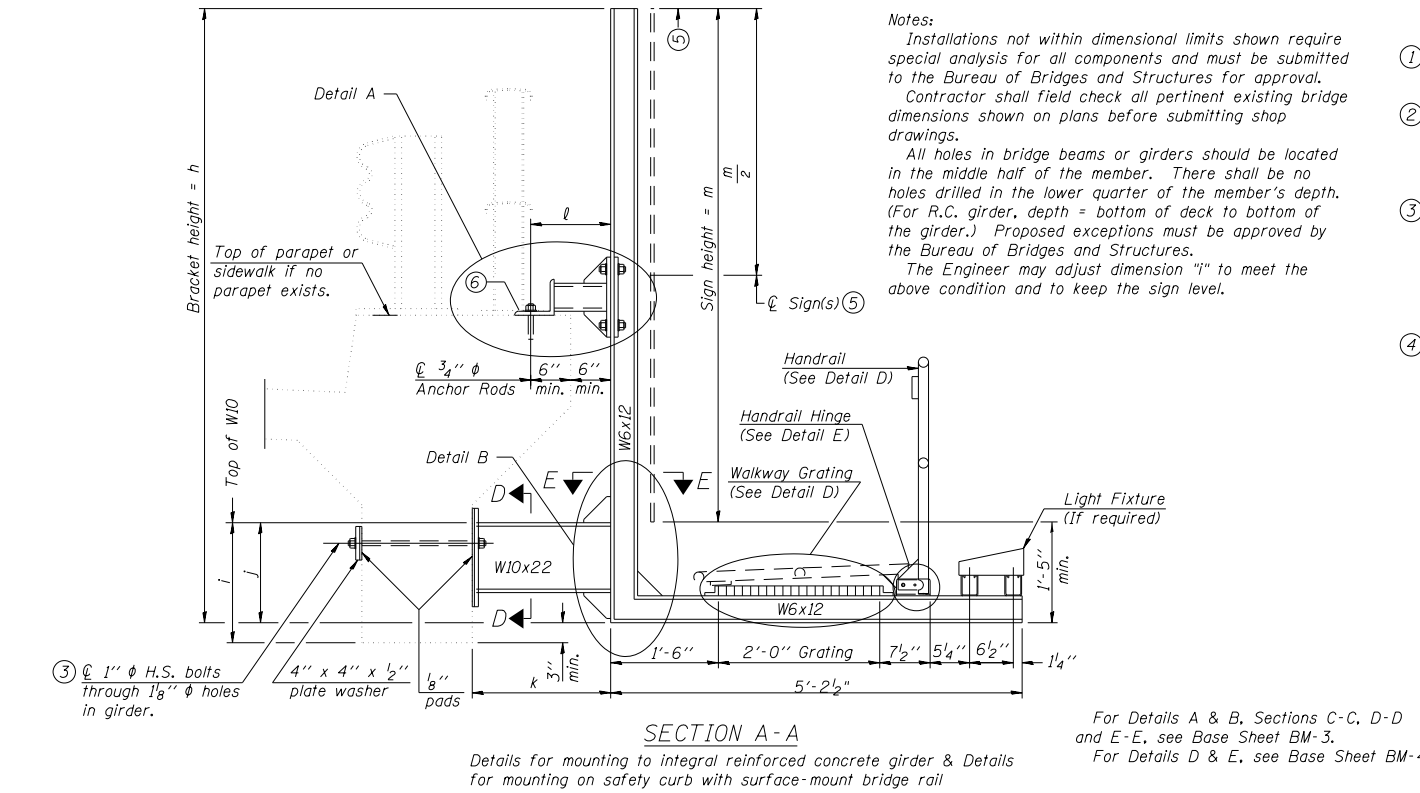
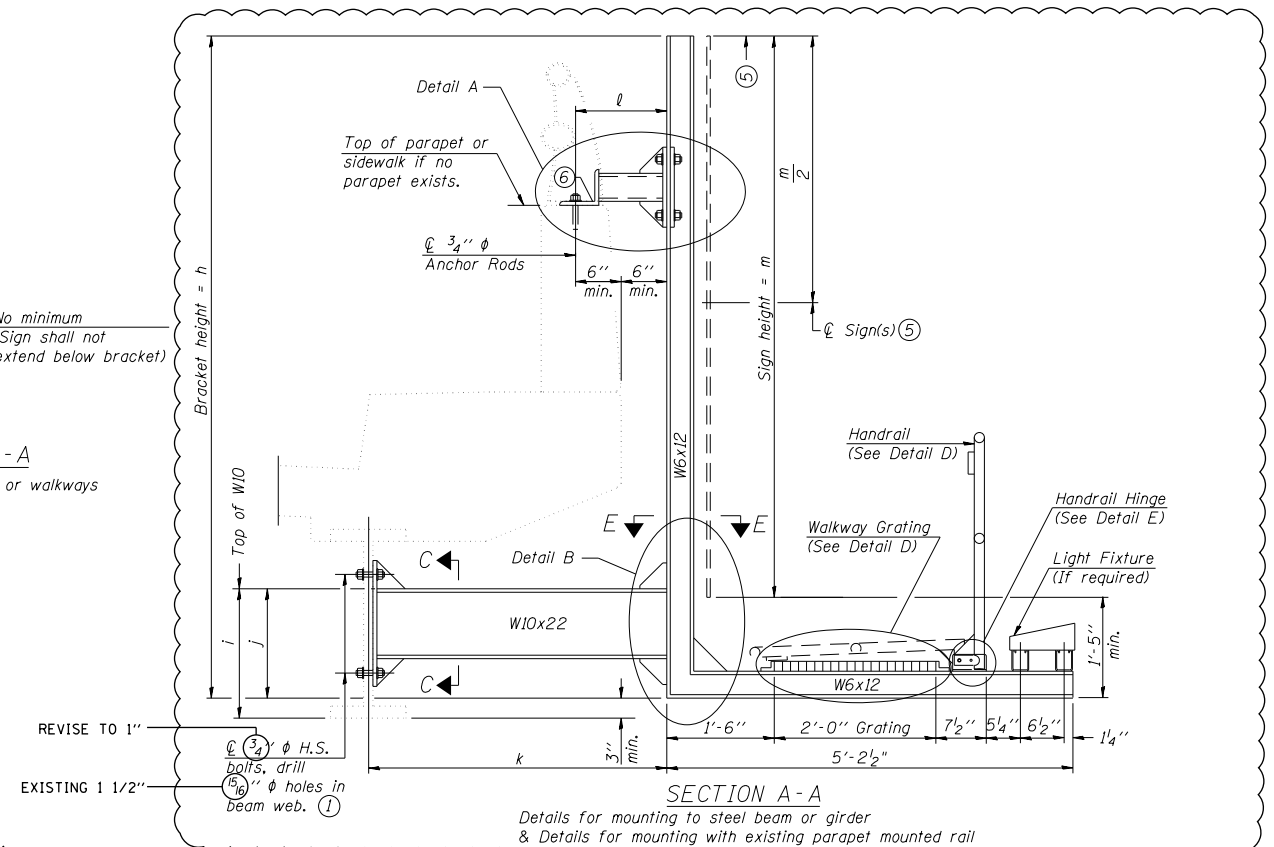
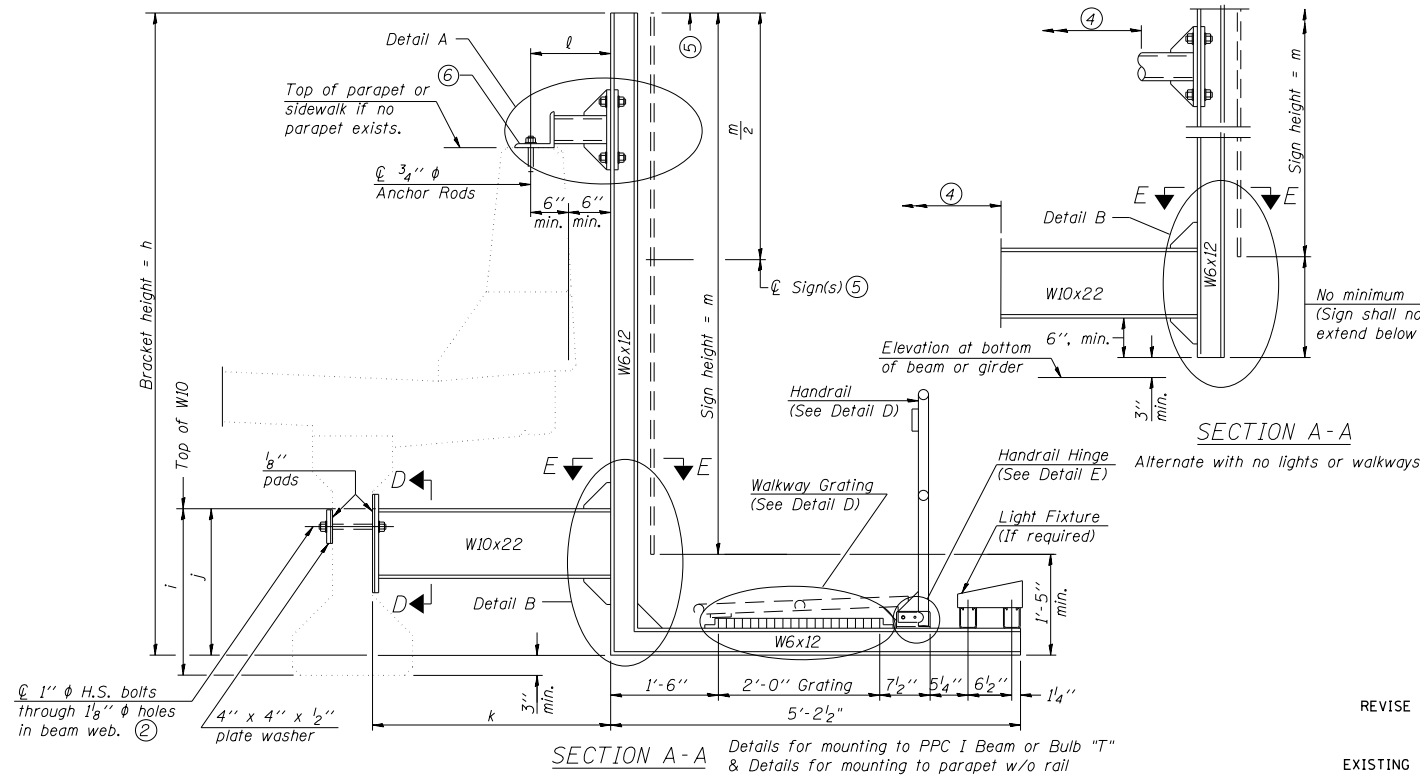
③ OVERHEAD SIGN STRUCTURE - BRIDGE MOUNTED	Foot	17.5'
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REVISION NOTES:

- ① EXISTING WALKWAY TO BE REMOVED (PAID FOR AS REMOVE OVERHEAD SIGN STRUCTURE - WALKWAY). THE EXISTING WALKWAY WILL NOT BE REPLACED.
- EXISTING LIGHTING UNITS TO BE REMOVED (PAID FOR AS REMOVAL OF EXISTING SIGN LIGHTING UNIT WITH NO SALVAGE). THE EXISTING LIGHTING WILL NOT BE REPLACED.
- ALL EXISTING SIGN PANELS, BRACKETS SUPPORTING WALKWAY, SIGN, AND RELATED HARDWARE ARE TO BE REMOVED (PAID FOR AS REMOVE AND RE-ERECT BRIDGE MOUNTED SIGN). ITEM WILL INCLUDE NEW BRACKETS AND ALL NECESSARY HARDWARE TO MOUNT PROPOSED SIGN PANELS. SIGN PANELS WILL BE PAID FOR AS SIGN PANEL - TYPE 3.

EXISTING W10x22 BEAMS CONNECTED TO STEEL BRIDGE BEAM ARE TO BE REPLACED WITH NEW BEAMS AS SHOWN IN PLAN DETAILS (PAID FOR AS REMOVE AND RE-ERECT BRIDGE MOUNTED SIGN).  
 PROPOSED SIGN BRACKETS WILL BE FABRICATED TO MATCH HEIGHT PROVIDED IN PLAN DETAILS AND WILL NOT BE "L" SHAPED TO MATCH EXISTING BRACKETS.  
 SIGN CONNECTIONS TO MATCH EXISTING FIELD CONDITIONS. ALL DIMENSIONS SHALL BE FIELD VERIFIED BY THE CONTRACTOR PRIOR TO FABRICATING ANY MATERIALS REQUIRED FOR MOUNTING SIGN.

BM-1 6-1-12



Notes:  
 Installations not within dimensional limits shown require special analysis for all components and must be submitted to the Bureau of Bridges and Structures for approval. Contractor shall field check all pertinent existing bridge dimensions shown on plans before submitting shop drawings.  
 All holes in bridge beams or girders should be located in the middle half of the member. There shall be no holes drilled in the lower quarter of the member's depth. (For R.C. girder, depth = bottom of deck to bottom of the girder.) Proposed exceptions must be approved by the Bureau of Bridges and Structures.  
 The Engineer may adjust dimension "i" to meet the above condition and to keep the sign level.

- Holes in new steel members may be drilled in the fabrication shop or in the field. Field drill existing members.
- For new PPC I beams, holes shall be formed during casting. For existing PPC I beams, prestressing strand locations shall be determined and spaced to miss strands by 6", min. Minimize spalling during field drilling of existing beams.
- For new construction, form holes. For existing RC beams, locate primary reinforcement and space holes to miss by 6", min. Minimize spalling and concrete fracturing/damage during field drilling of existing concrete. Spalls over 1/4" deep or beyond the coverage of the 4x4 plate washer shall be repaired with epoxy mortar before installing washer.
- For attachment details of 3/2" pipe and W10x22, see other sections as applicable.
- Sign shall not extend more than 6" above top of bracket, and this dimension may vary to keep sign level if bridge is on grade or vertical curve. Multiple signs of various heights shall share a common horizontal centerline and use equal bracket heights. If no sign is attached to a W6x12 vertical (bracket only supporting walkway), dimension h shall be the same as an adjacent bracket with a sign attached, unless Engineer specifically directs shorter brackets due to locational restraints on future uses. (See Detail A for minimum bracket height.)
- For bridge mounted sign structures installed on new bridges with railing, during design, bracket spacing must be coordinated with railing post spacing and the Contractor must install upper brackets prior to railing installation. For bridge mounted sign structures installed on existing bridges with railing, during design, brackets spacing must be coordinated with railing post spacing and the Contractor must temporarily remove sections of railing to facilitate upper bracket installation. If it is determined during design that existing railings can't be removed, alternate upper connection details must be developed for the contract plans and approved by the Bureau of Bridges and Structures.

Structure Number	Station	* h	i	j	k max. (10'-0" max.)	l max. (8'-0" max.)	m (15'-0" max.)
1B0161094L032.9	444+00	9'-0"	1'-9 1/2"	1'-5 1/4"	VARIES 2'-0 3/4"	VARIES 1'-6"	9'-0"
			MATCH EXIST.	MATCH EXIST.	TO 4'-0 3/4"	TO 3'-6"	

**REVISION NOTES:**

1 EXISTING WALKWAY TO BE REMOVED (PAID FOR AS REMOVE OVERHEAD SIGN STRUCTURE - WALKWAY). THE EXISTING WALKWAY WILL NOT BE REPLACED.

EXISTING LIGHTING UNITS TO BE REMOVED (PAID FOR AS REMOVAL OF EXISTING SIGN LIGHTING UNIT WITH NO SALVAGE). THE EXISTING LIGHTING WILL NOT BE REPLACED.

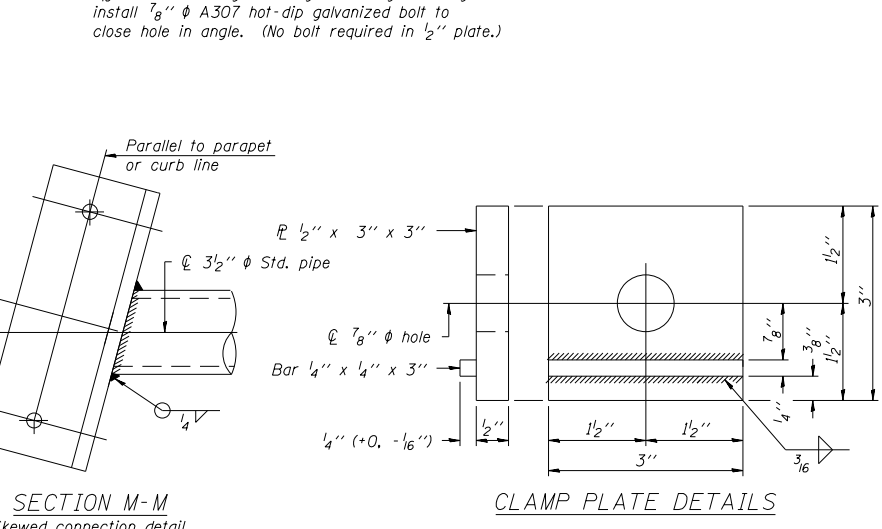
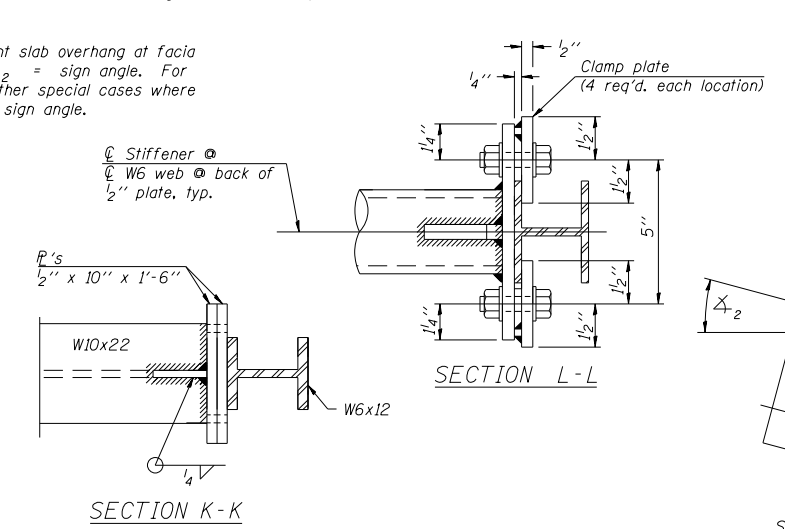
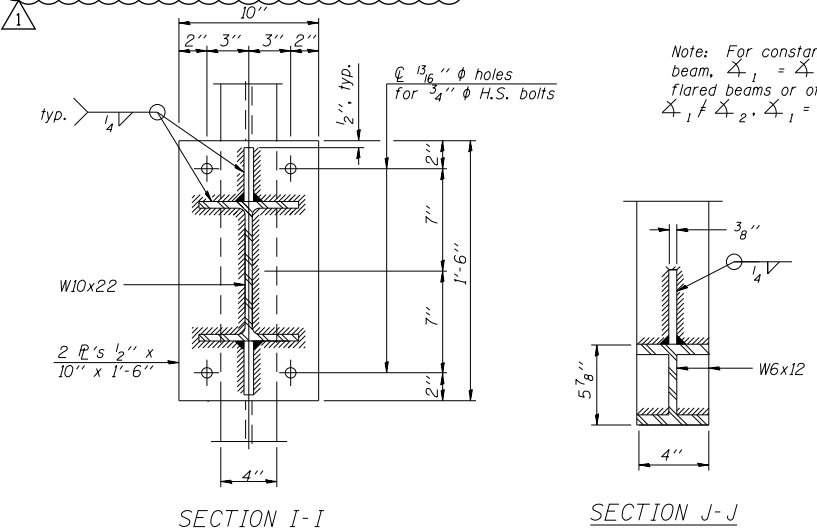
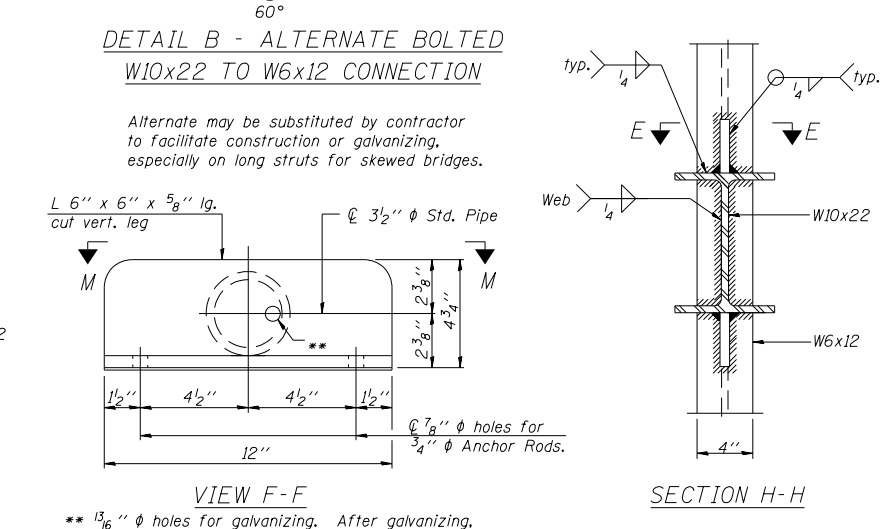
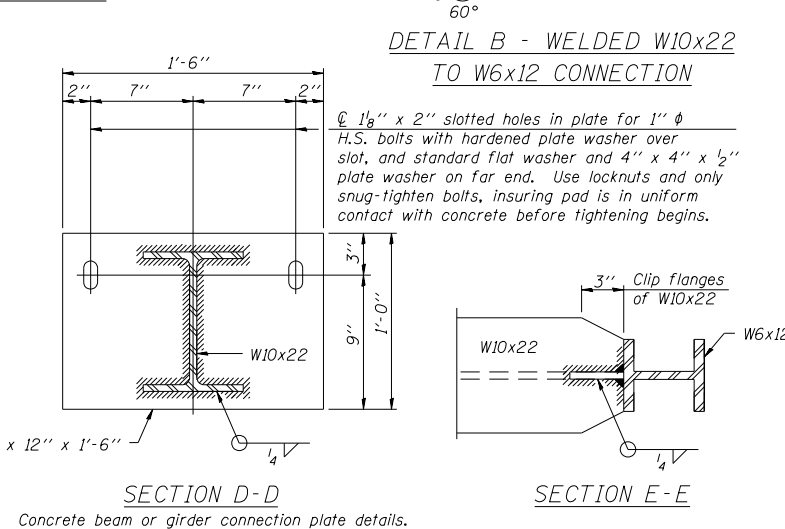
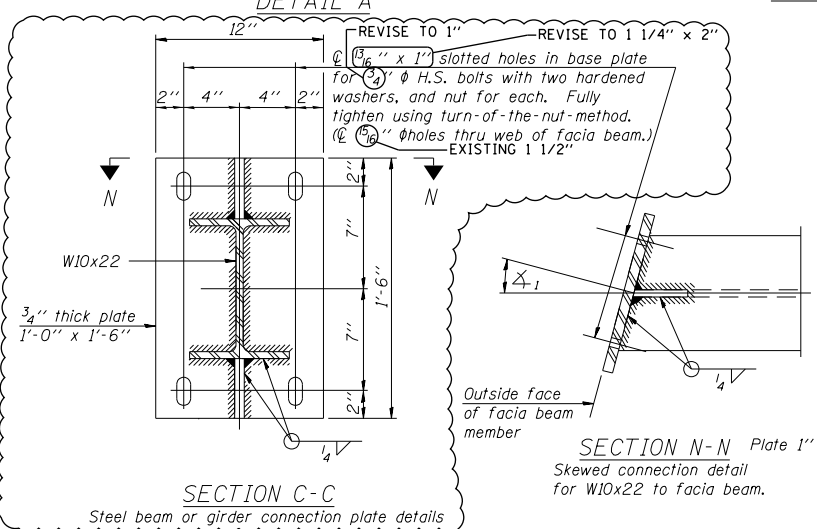
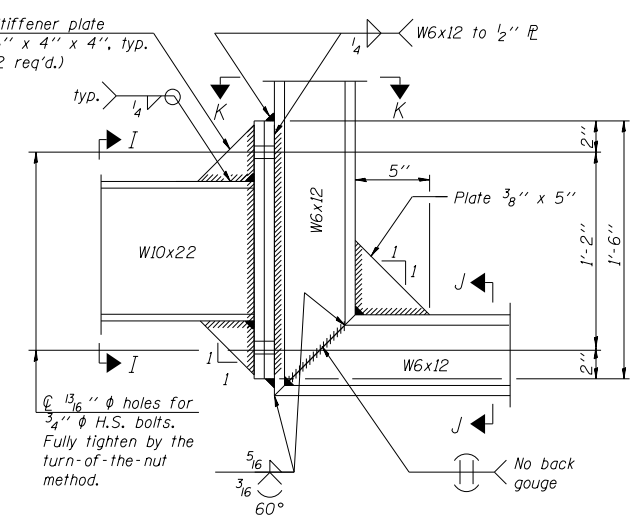
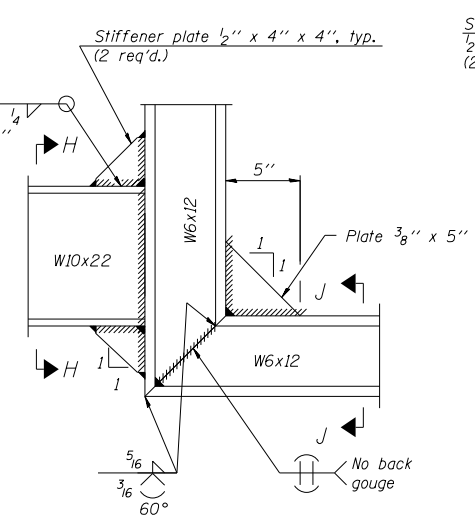
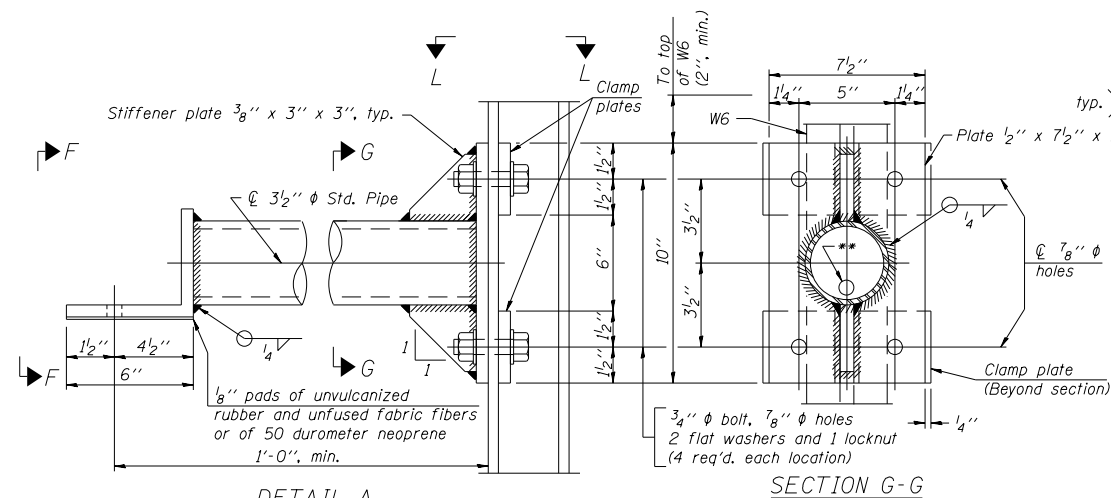
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PROPOSED SIGN BRACKETS WILL BE FABRICATED TO MATCH HEIGHT PROVIDED IN PLAN DETAILS AND WILL NOT BE "L" SHAPED TO MATCH EXISTING BRACKETS.

SIGN CONNECTIONS TO MATCH EXISTING FIELD CONDITIONS. ALL DIMENSIONS SHALL BE FIELD VERIFIED BY THE CONTRACTOR PRIOR TO FABRICATING ANY MATERIALS REQUIRED FOR MOUNTING SIGN.

\* BOTTOM OF SIGN PANEL SHALL MATCH BOTTOM OF W6x12 SIGN BRACKET. TOP OF SIGN PANEL SHALL MATCH THE TOP OF THE SIGN BRACKET AS SHOWN IN SECTION A-A.



Note: For constant slab overhang at facia beam,  $\Delta_1 = \Delta_2 =$  sign angle. For flared beams or other special cases where  $\Delta_1 \neq \Delta_2$ ,  $\Delta_1 =$  sign angle.

\*\*  $\frac{13}{16}$ "  $\phi$  holes for galvanizing. After galvanizing, install  $\frac{7}{8}$ "  $\phi$  A307 hot-dip galvanized bolt to close hole in angle. (No bolt required in 1/2" plate.)

BM-3 6-1-12

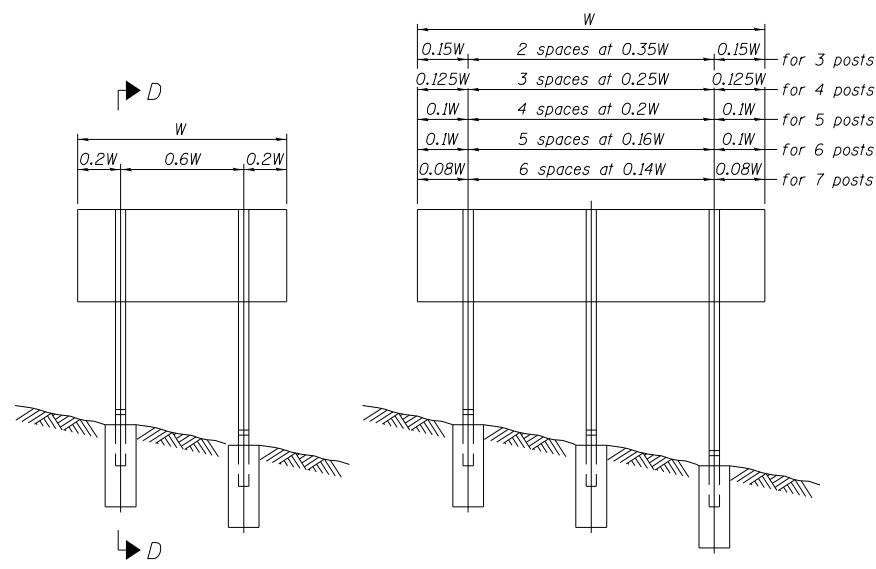
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PLOT DATE = 11/1/2012		DATE - 10/31/2012	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

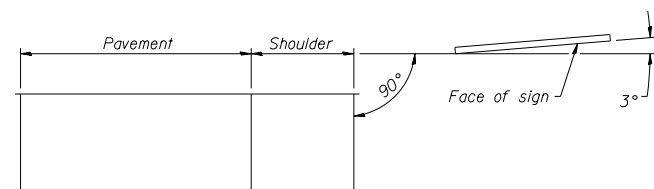
BRIDGE MOUNT SIGN STRUCTURES  
CONNECTION DETAILS

SCALE: N.T.S SHEET NO. 331 OF 919 SHEETS

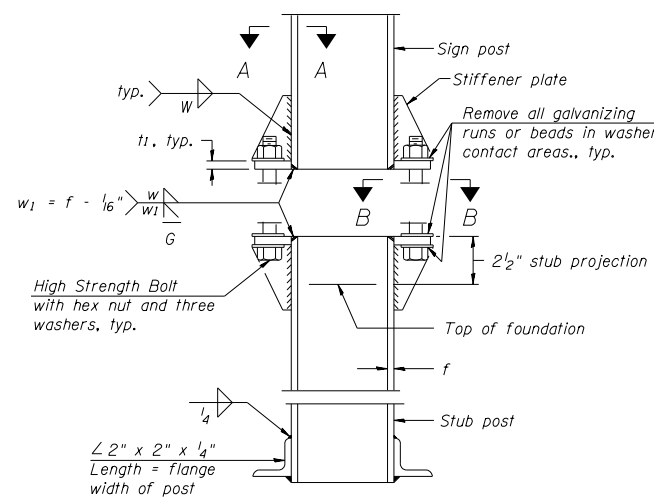
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CONTRACT NO. 60T35				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



ELEVATION

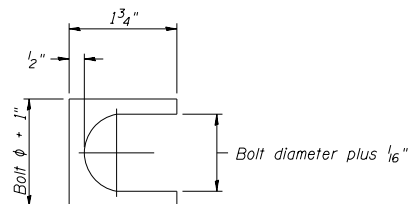


LOCATION SKETCH



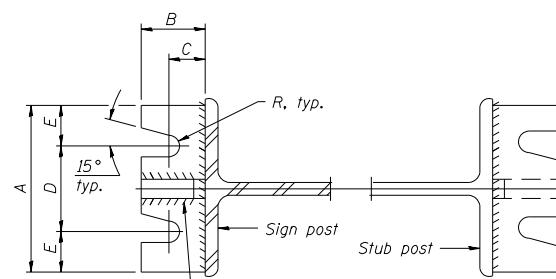
ELEVATION  
SIGN POST & STUB POST

0.15W	2 spaces at 0.35W	0.15W	for 3 posts
0.125W	3 spaces at 0.25W	0.125W	for 4 posts
0.1W	4 spaces at 0.2W	0.1W	for 5 posts
0.1W	5 spaces at 0.16W	0.1W	for 6 posts
0.08W	6 spaces at 0.14W	0.08W	for 7 posts



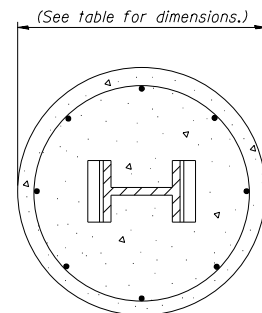
SHIM DETAIL

Furnish two 0.01" thick and two 0.03" thick stainless steel or brass (ASTM B36) shims per post.

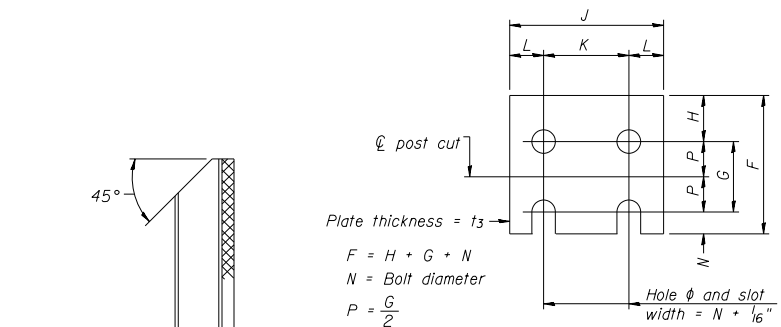


SECTION A-A

SECTION B-B



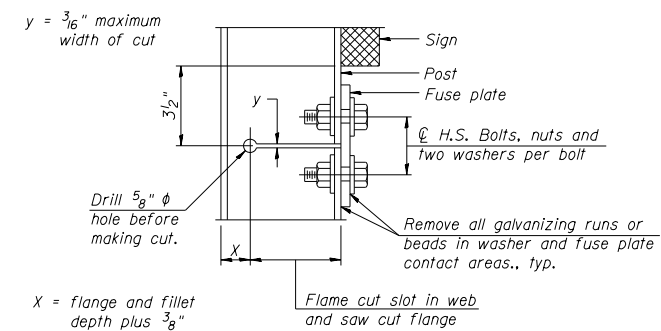
SECTION C-C



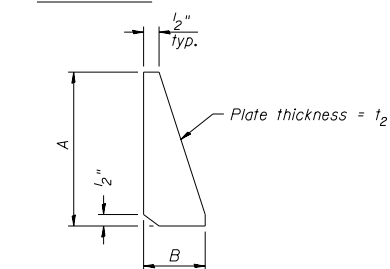
FUSE PLATE DETAIL  
(Install with notches down.)

N = Bolt Diameter	G	H
1/2"	2"	1 1/8"
5/8"	2 1/4"	1 1/4"
3/4"	2 1/2"	1 3/8"
7/8"	2 3/4"	1 1/2"
1"	3"	1 5/8"
1 1/8"	3 1/4"	1 3/4"
1 1/4"	3 1/2"	1 7/8"

Plate thickness =  $t_3$   
 $F = H + G + N$   
 $N = \text{Bolt diameter}$   
 $P = \frac{G}{2}$



DETAIL H



STIFFENER PLATE DETAIL  
Diameter

GENERAL NOTES

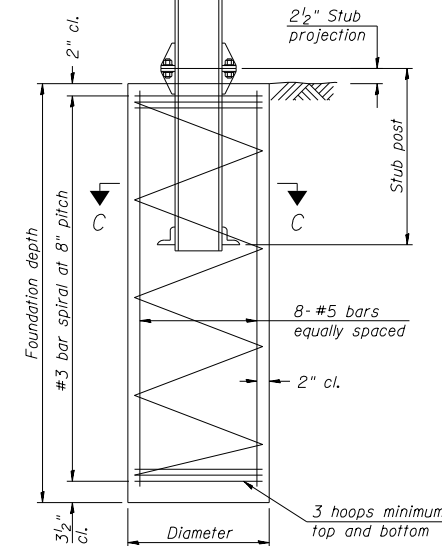
Posts shall be plumbed by using shims with post-to-stub post connection bolts snug tight only. Final tightening of all High Strength Bolts shall be in accordance with Article 727.05 and threads at the junction of the bolt and nut shall be burred or center punched to prevent the nut from loosening.

LOADING: 80 m.p.h. wind with 30% gust factor, normal to sign.

DESIGN STRESSES:  
 Structural steel - 20,000 p.s.i.  
 Reinforcing steel - 20,000 p.s.i.  
 Concrete - 1,400 p.s.i.  
 Footing soil pressure - 2,000 p.s.f.

After fabrication, the post, fuse plate and upper 6", min. of the stub post shall be hot-dip galvanized in accordance with AASHTO M111. All bolts, nuts and washers shall be hot-dip galvanized in accordance with AASHTO M232.

Work this sheet with Base Sheet BAW-A-2.



SECTION D-D

BAW-A-1

6-1-12

(Sheet 1 of 2)

FILE NAME =	USER NAME = BAW1ort	DESIGNED - JLV	REVISED -
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PLOT SCALE = 10.0000 "/in.	CHECKED - RCB	REVISOR -	REVISED -
PLOT DATE = 11/1/2012	DATE - 10/31/2012	REVISOR -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BREAK-AWAY WIDE FLANGE  
STEEL SIGN POST DETAILS

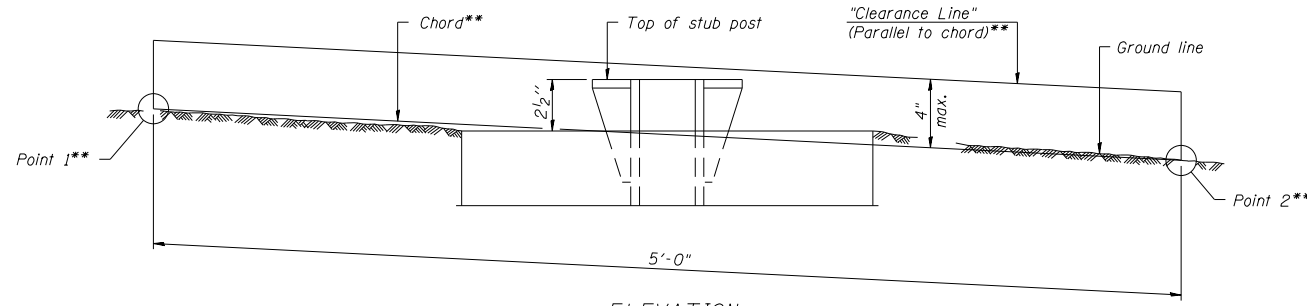
SCALE: N.T.S SHEET NO. 332 OF 919 SHEETS

F.A.P. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920.01,1518,2022&1922.4)R	COOK	919	332
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60T35	

POST	CONCRETE FOUNDATION TABLE							POST TO STUB POST CONNECTION DATA										FUSE PLATE DATA				
	Foundation			Reinforcement			Stub Post Length	Bolt Size	A	B	C	D	E	t <sub>1</sub>	t <sub>2</sub>	R	W	J	K	L	t <sub>3</sub>	
	Diameter	Minimum Depth	Concrete (1) cu. yds.	Vertical Bars Length	Bar Spirals Diameter	Bar Spirals Length																lbs. (2)
W6x9	2'-0"	6'-0"	0.70	5'-9"	1'-8 1/2"	79'-0"	78	2'-3"	5/8" x 3 1/4"	6"	2 1/4"	1 1/4"	3 1/2"	1 1/4"	3/4"	1/2"	5/32"	1 1/4"	4"	2 1/4"	7/8"	1/4"
W6x15	2'-0"	6'-0"	0.70	5'-9"	1'-8 1/2"	79'-0"	78	2'-6"	5/8" x 3 1/4"	6"	2 1/4"	1 1/4"	3 1/2"	1 1/4"	3/4"	1/2"	5/32"	1 1/4"	6"	3 1/2"	1 1/4"	3/8"
W8x18	2'-0"	6'-0"	0.70	5'-9"	1'-8 1/2"	79'-0"	78	2'-6"	3/4" x 3 3/4"	6"	2 1/2"	1 3/8"	3 1/4"	1 3/8"	1"	1/2"	1/32"	5/16"	5 1/4"	2 3/4"	1 1/4"	3/8"
W10x22	2'-6"	6'-6"	1.18	6'-3"	2'-2 1/2"	105'-0"	92	3'-0"	3/4" x 3 3/4"	6"	2 1/2"	1 3/8"	3 1/4"	1 3/8"	1"	1/2"	1/32"	5/16"	5 3/4"	2 3/4"	1 1/2"	1/2"
W10x26	2'-6"	7'-0"	1.27	6'-9"	2'-2 1/2"	112'-0"	98	3'-0"	7/8" x 4"	7"	2 3/4"	1 1/2"	4"	1 1/2"	1"	3/4"	5/32"	3/8"	5 3/4"	2 3/4"	1 1/2"	5/8"
W12x26	2'-6"	7'-9"	1.41	7'-6"	2'-2 1/2"	119'-0"	107	3'-0"	7/8" x 4"	7"	2 3/4"	1 1/2"	4"	1 1/2"	1"	3/4"	5/32"	3/8"	6 1/2"	3 1/2"	1 1/2"	5/8"
W14x30	3'-0"	7'-3"	1.90	7'-0"	2'-8 1/2"	145'-0"	113	3'-0"	7/8" x 4"	7"	2 3/4"	1 1/2"	4"	1 1/2"	1"	3/4"	5/32"	3/8"	6 3/4"	3 1/2"	1 5/8"	1/2"
W14x38	3'-0"	8'-0"	2.09	7'-9"	2'-8 1/2"	153'-0"	122	3'-6"	1" x 4 1/2"	7 1/2"	3"	1 3/4"	4"	1 3/4"	1 1/4"	3/4"	1/32"	3/8"	6 3/4"	3 1/2"	1 5/8"	1/2"
W16x45	3'-0"	8'-6"	2.23	8'-3"	2'-8 1/2"	162'-0"	130	3'-6"	1" x 4 1/2"	7 1/2"	3"	1 3/4"	4"	1 3/4"	1 1/4"	3/4"	1/32"	3/8"	7"	3 1/2"	1 3/4"	1/2"

\*Dimensional changes required for varying site conditions shall be approved by the Engineer.

POST	FUSE PLATE BOLT SIZE																				
	Sign Height																				
	4'-0"	5'-0"	6'-0"	7'-0"	8'-0"	9'-0"	10'-0"	11'-0"	12'-0"	13'-0"	14'-0"	15'-0"	16'-0"	17'-0"	18'-0"	19'-0"	20'-0"	21'-0"	22'-0"	23'-0"	24'-0"
W6x9	1/2" x 1 1/2"	1/2" x 1 1/2"	1/2" x 1 1/2"	1/2" x 1 1/2"	---	---	---	---	---	---	---	---	---	---	---	---	---	---	---	---	---
W6x15	1/2" x 1 3/4"	1/2" x 1 3/4"	1/2" x 1 3/4"	5/8" x 2"	5/8" x 2"	3/4" x 2"	3/4" x 2"	3/4" x 2"	3/4" x 2"	3/4" x 2"	---	---	---	---	---	---	---	---	---	---	---
W8x18	1/2" x 1 3/4"	1/2" x 1 3/4"	1/2" x 1 3/4"	5/8" x 2"	5/8" x 2"	3/4" x 2"	3/4" x 2"	3/4" x 2"	3/4" x 2"	3/4" x 2"	---	---	---	---	---	---	---	---	---	---	---
W10x22	1/2" x 2"	1/2" x 2"	1/2" x 2"	1/2" x 2"	5/8" x 2"	5/8" x 2"	3/4" x 2 1/4"	3/4" x 2 1/4"	3/4" x 2 1/4"	3/4" x 2 1/4"	3/4" x 2 1/4"	3/4" x 2 1/4"	3/4" x 2 1/4"	---	---	---	---	---	---	---	---
W10x26	1/2" x 2"	1/2" x 2"	1/2" x 2"	1/2" x 2"	5/8" x 2 1/4"	5/8" x 2 1/4"	3/4" x 2 1/2"	3/4" x 2 1/2"	3/4" x 2 1/2"	3/4" x 2 1/2"	3/4" x 2 1/2"	3/4" x 2 1/2"	3/4" x 2 1/2"	3/4" x 2 1/2"	---	---	---	---	---	---	---
W12x26	1/2" x 2"	1/2" x 2"	1/2" x 2"	1/2" x 2"	5/8" x 2 1/4"	5/8" x 2 1/4"	3/4" x 2 1/2"	3/4" x 2 1/2"	3/4" x 2 1/2"	3/4" x 2 1/2"	3/4" x 2 1/2"	3/4" x 2 1/2"	3/4" x 2 1/2"	3/4" x 2 1/2"	3/4" x 2 1/2"	---	---	---	---	---	---
W14x30	1/2" x 2"	1/2" x 2"	1/2" x 2"	1/2" x 2"	5/8" x 2"	5/8" x 2"	3/4" x 2 1/4"	3/4" x 2 1/4"	3/4" x 2 1/4"	3/4" x 2 1/4"	3/4" x 2 1/4"	3/4" x 2 1/4"	3/4" x 2 1/4"	3/4" x 2 1/4"	3/4" x 2 1/4"	3/4" x 2 1/4"	---	---	---	---	---
W14x38	1/2" x 2"	1/2" x 2"	1/2" x 2"	1/2" x 2"	5/8" x 2 1/4"	5/8" x 2 1/4"	3/4" x 2 1/2"	3/4" x 2 1/2"	3/4" x 2 1/2"	3/4" x 2 1/2"	7/8" x 2 1/2"	7/8" x 2 1/2"	1" x 2 3/4"	1" x 2 3/4"	1" x 2 3/4"	1" x 2 3/4"	1" x 2 3/4"	1" x 2 3/4"	1" x 2 3/4"	1" x 2 3/4"	1" x 2 3/4"
W16x45	---	1/2" x 2"	1/2" x 2"	1/2" x 2"	1/2" x 2"	1/2" x 2"	5/8" x 2 1/4"	5/8" x 2 1/4"	5/8" x 2 1/4"	3/4" x 2 1/2"	3/4" x 2 1/2"	7/8" x 2 1/2"	7/8" x 2 1/2"	7/8" x 2 1/2"	1" x 2 3/4"	1" x 2 3/4"	1" x 2 3/4"	1" x 2 3/4"	1" x 2 3/4"	1" x 2 3/4"	1" x 2 3/4"

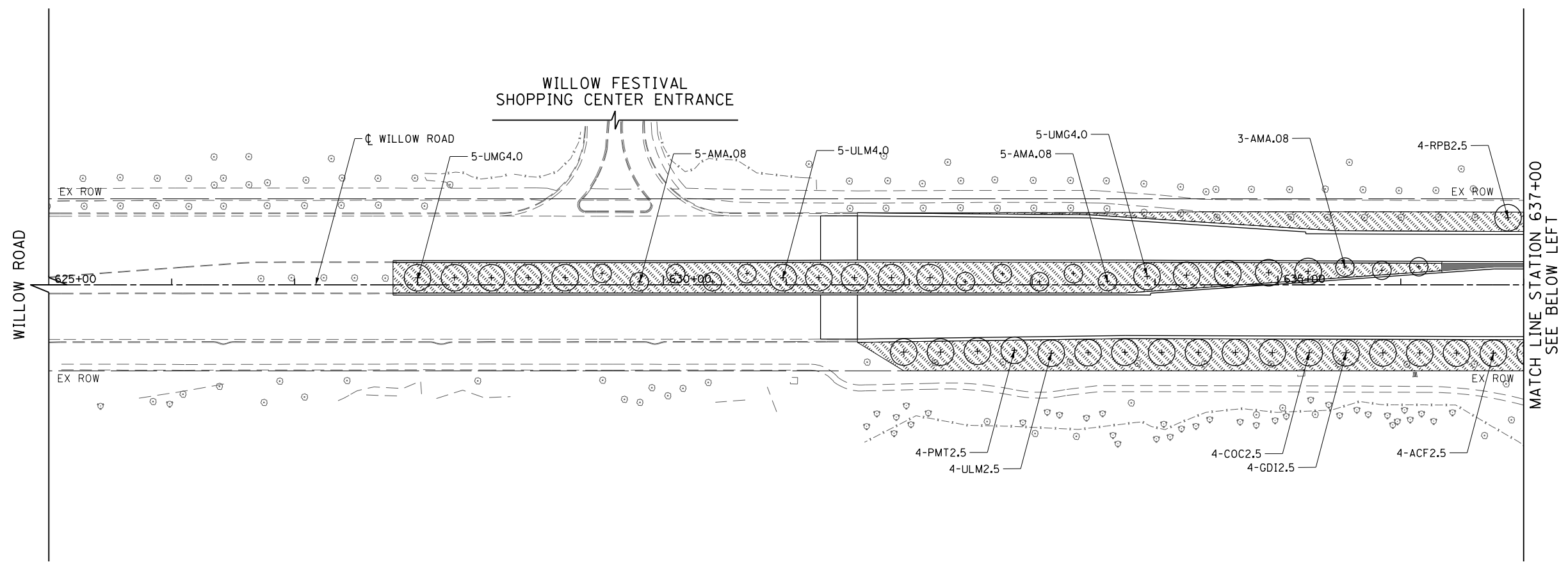


ELEVATION  
GROUND LINE & STUB POST

\*\* For all "Point 1" and "Point 2" locations, "Clearance Line" must be at or above top of stub post.

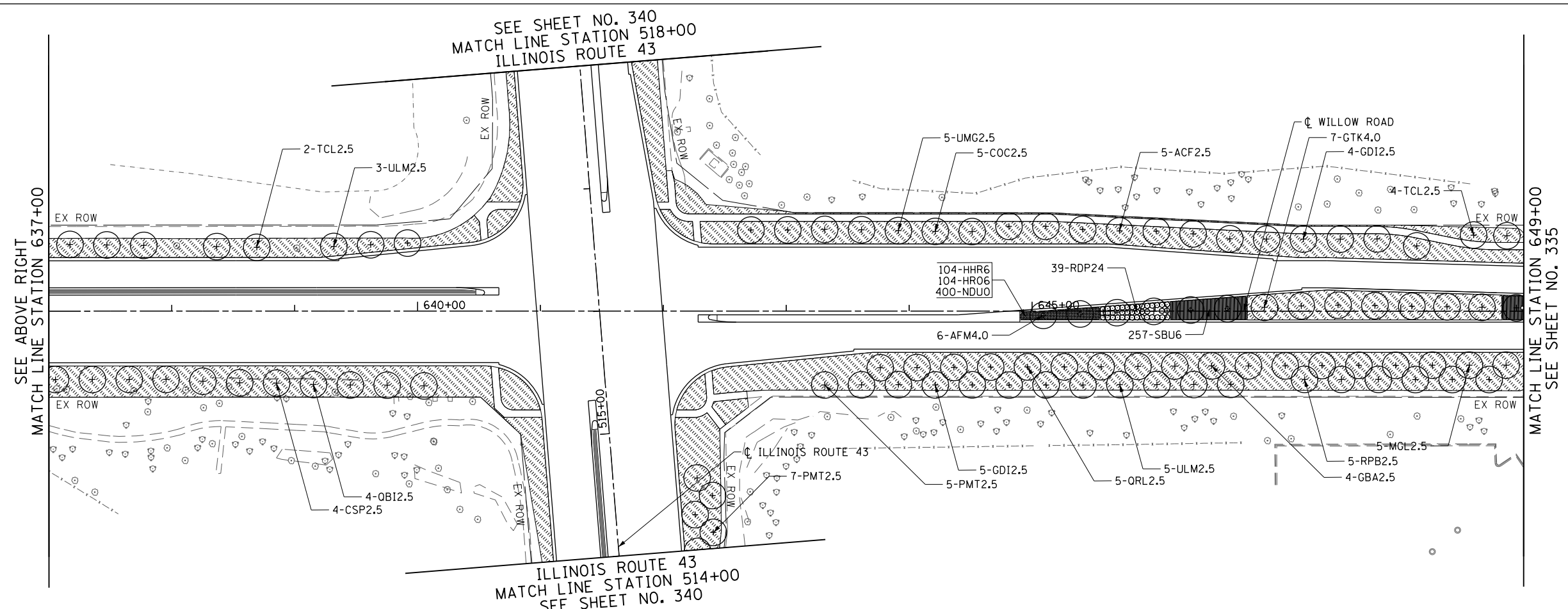
- (1) Quantity includes all concrete necessary for one foundation.
- (2) Includes reinforcement bars and spiral hooping for one foundation.

FILE NAME = G:\CH11\0158\Road\Sheets\0160T35-SHT-STUBUCT-SIGNDETAILS.dgn	USER NAME = BAW\jrt	DESIGNED - JLV	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BREAK-AWAY WIDE FLANGE STEEL SIGN POST TABLES</b>	F.A.P. RTE. 305	SECTION (1920.01,1518,2022&1922.4B)R	COUNTY COOK	TOTAL SHEETS 919	SHEET NO. 333
PLOT SCALE = 10.0000' / in.	CHECKED - RCB	REVISIED -	SCALE: N.T.S			SHEET NO. 333 OF 919 SHEETS	CONTRACT NO. 60T35			
PLOT DATE = 11/11/2012	DATE - 10/31/2012	REVISIED -	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							



- LEGEND**
- PROPOSED TREE
  - PROPOSED SHRUB
  - SODDING, SALT TOLERANT NITROGEN, PHOSPHORUS, AND POTASSIUM FERTILIZER NUTRIENT
  - SEEDING, CLASS 2A NITROGEN, PHOSPHORUS, AND POTASSIUM FERTILIZER NUTRIENT

MATCH LINE STATION 637+00  
SEE BELOW LEFT



NOTE: IMPORT TOPSOIL FROM OFFSITE SOURCES. OBTAIN TOPSOIL DISPLACED FROM NATURALLY WELL-DRAINED CONSTRUCTION OR MINING SITES WHERE TOPSOIL OCCURS AT LEAST 4 INCHES DEEP; DO NOT OBTAIN FROM AGRICULTURAL LAND, BOGS OR MARSHES.

MATCH LINE STATION 649+00  
SEE SHEET NO. 335

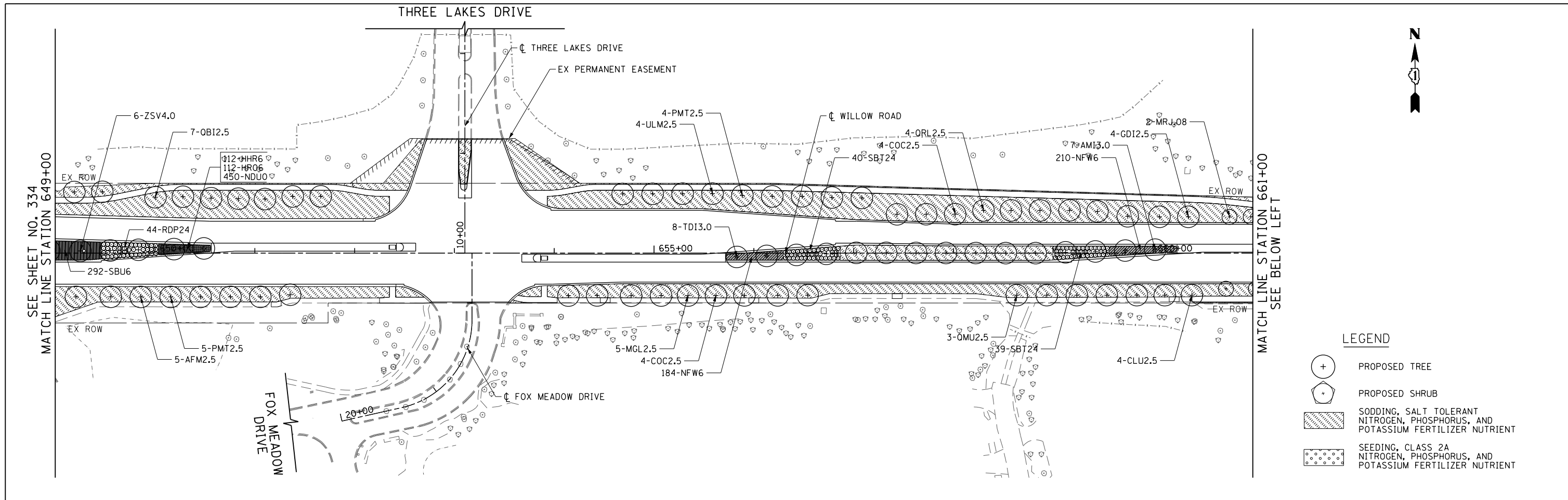
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	PLOT DATE = 11/1/2012	DATE - 10/31/2012	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

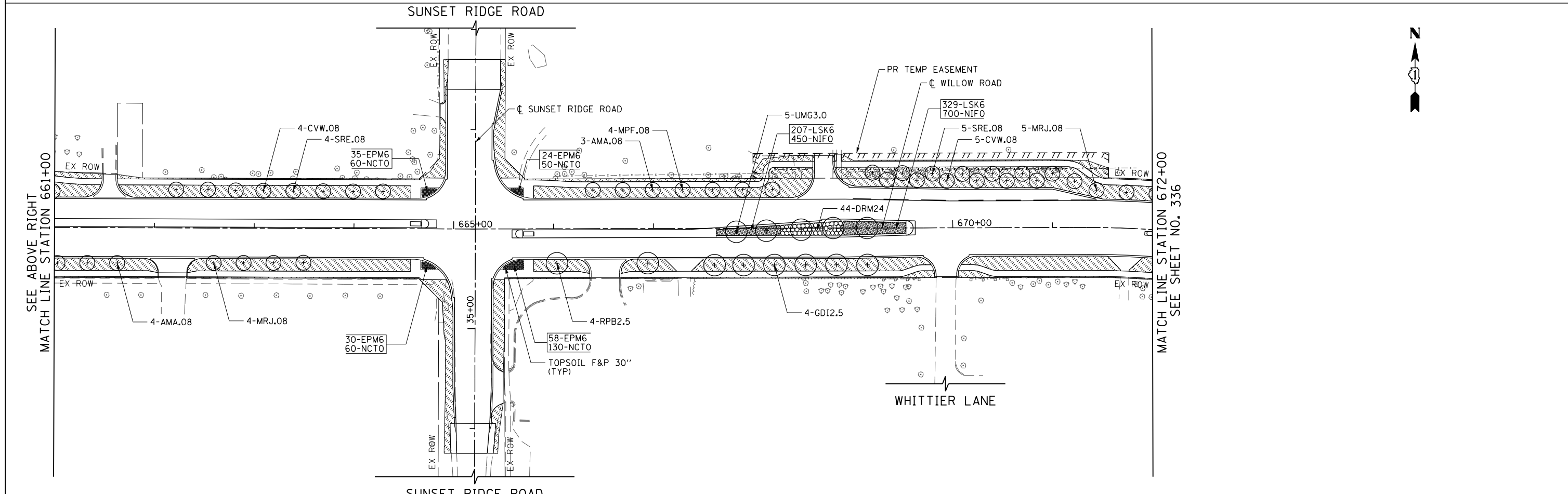
**WILLOW ROAD LANDSCAPING PLAN**

SCALE: 1" = 50'    SHEET NO. 334 OF 919 SHEETS    STA. 625+00    TO STA. 649+00

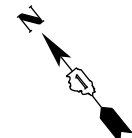
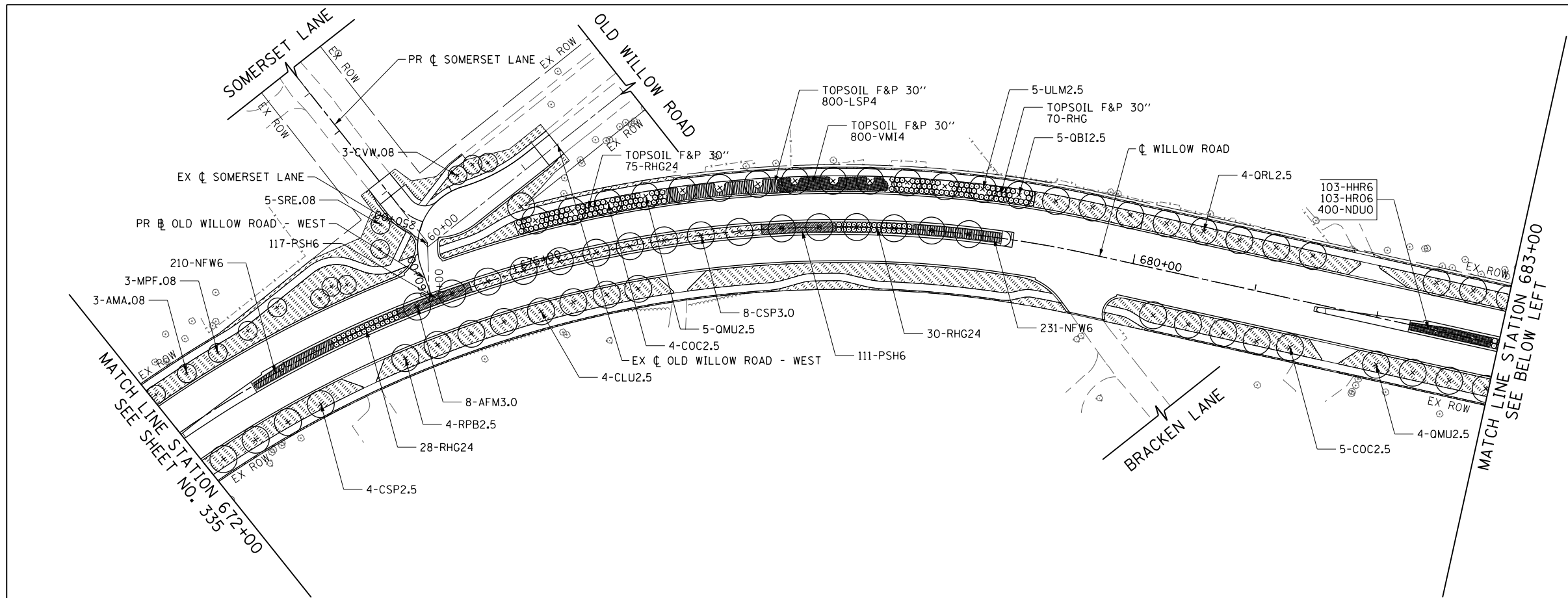
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920.01,1518,2022&1922.4)R	COOK	919	334
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60T35	



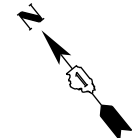
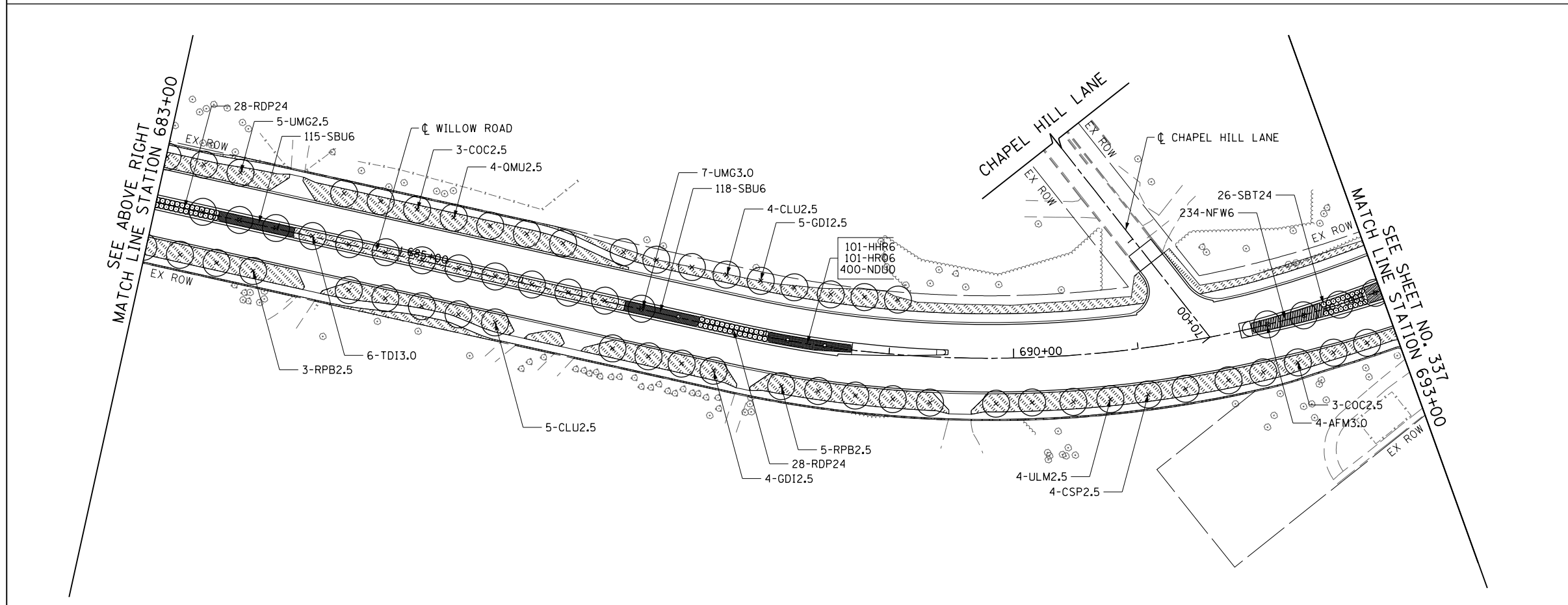
- LEGEND**
- PROPOSED TREE
  - PROPOSED SHRUB
  - SODDING, SALT TOLERANT NITROGEN, PHOSPHORUS, AND POTASSIUM FERTILIZER NUTRIENT
  - SEEDING, CLASS 2A NITROGEN, PHOSPHORUS, AND POTASSIUM FERTILIZER NUTRIENT



FILE NAME =	USER NAME = BAWtor.t	DESIGNED - JLV	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>WILLOW ROAD LANDSCAPING PLAN</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
g:\ch11\0158\road\Sheets\0160T35-SHT-LND\SCP-02.dgn		DRAWN - KAL	REVISED -			305	(1920.01,1518,2022&1922.4)BR	COOK	919	335	
		CHECKED - RCB	REVISED -			CONTRACT NO. 60T35					
		DATE - 10/31/2012	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					
					SCALE: 1" = 50'	SHEET NO. 335 OF 919 SHEETS		STA. 649+00		TO STA. 672+00	



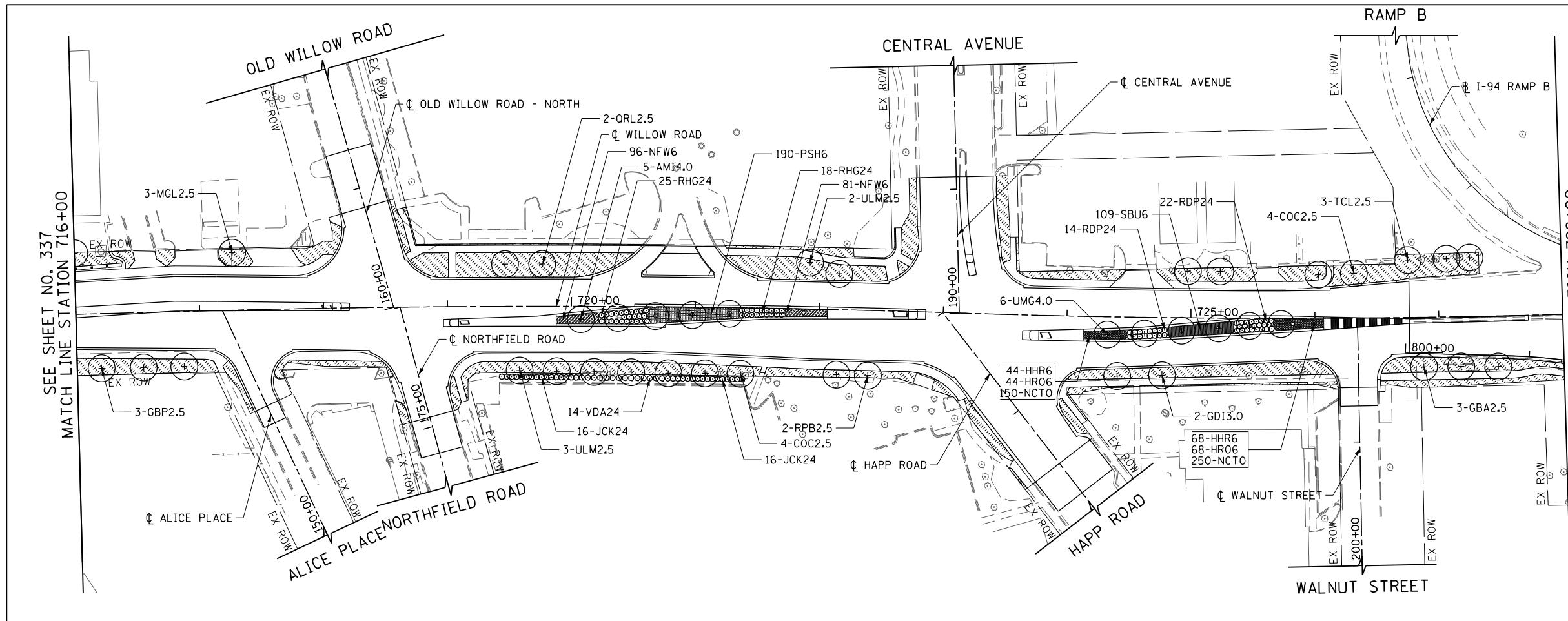
- LEGEND**
- PROPOSED TREE
  - PROPOSED SHRUB
  - SODDING, SALT TOLERANT NITROGEN, PHOSPHORUS, AND POTASSIUM FERTILIZER NUTRIENT
  - SEEDING, CLASS 2A NITROGEN, PHOSPHORUS, AND POTASSIUM FERTILIZER NUTRIENT



FILE NAME =	USER NAME = BAW\rtort	DESIGNED - JLV	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>WILLOW ROAD LANDSCAPING PLAN</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
g:\ch11\0158\road\Sheets\016\T35-SHT-LND\SCP-03.dgn		DRAWN - KAL	REVISED -			305	(1920,01,1518,2022&1922,4BR	COOK	919	336	
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PLOT DATE = 11/1/2012		DATE - 10/31/2012	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					
						SCALE: 1" = 50'	SHEET NO. 336 OF 919 SHEETS	STA. 672+00	TO STA. 693+00		



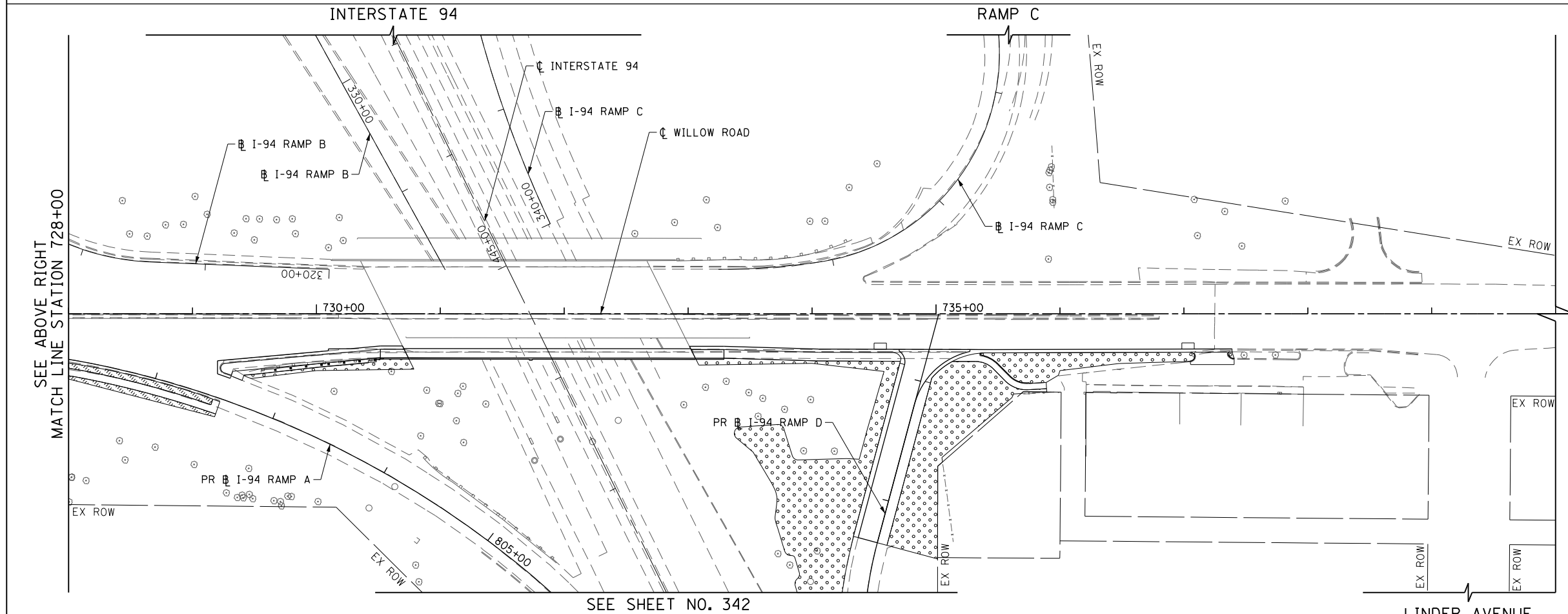




MATCH LINE STATION 728+00  
SEE BELOW LEFT

LEGEND

- PROPOSED TREE
- PROPOSED SHRUB
- SODDING, SALT TOLERANT NITROGEN, PHOSPHORUS, AND POTASSIUM FERTILIZER NUTRIENT
- SEEDING, CLASS 2A NITROGEN, PHOSPHORUS, AND POTASSIUM FERTILIZER NUTRIENT







MATCH LINE STATION 728+00  
SEE ABOVE RIGHT

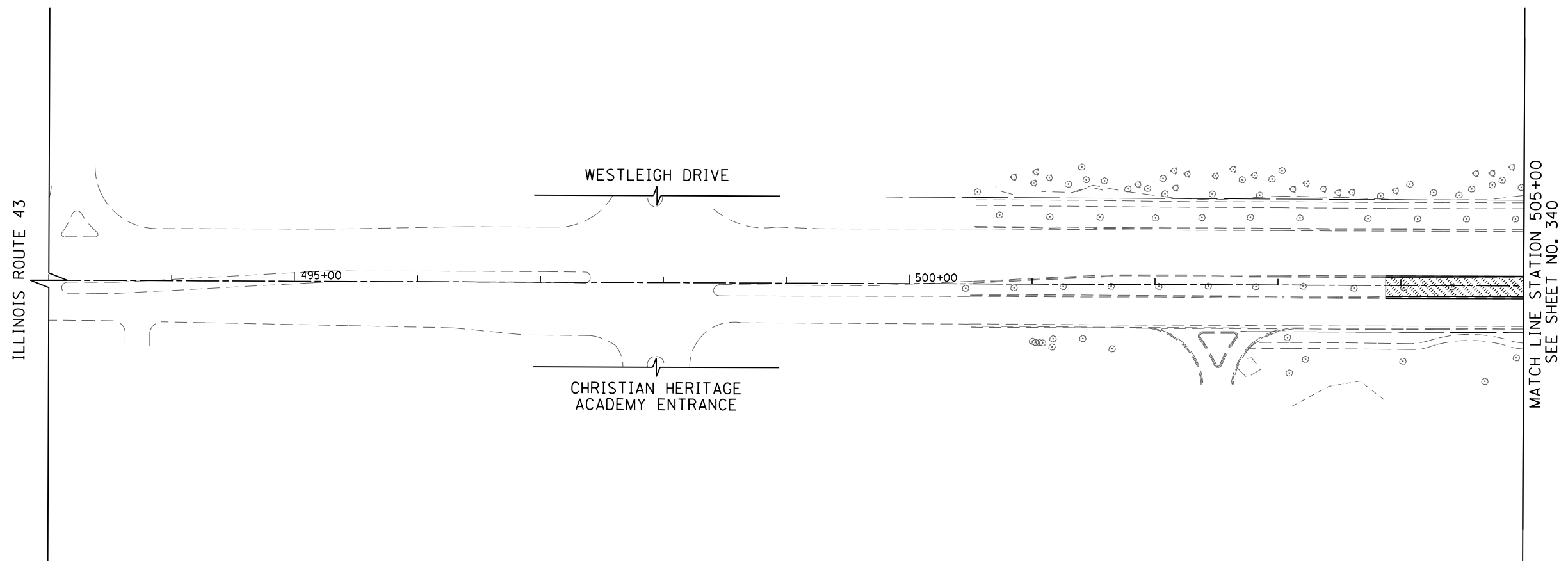
LEGEND

- PROPOSED TREE
- PROPOSED SHRUB
- SODDING, SALT TOLERANT NITROGEN, PHOSPHORUS, AND POTASSIUM FERTILIZER NUTRIENT
- SEEDING, CLASS 2A NITROGEN, PHOSPHORUS, AND POTASSIUM FERTILIZER NUTRIENT

FILE NAME =	USER NAME = BAW:tor.t	DESIGNED - JLV	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>WILLOW ROAD LANDSCAPING PLAN</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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		CHECKED - RCB	REVISED -			CONTRACT NO. 60T35					
		DATE - 10/31/2012	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					

**LEGEND**

-  PROPOSED TREE
-  PROPOSED SHRUB
-  SODDING, SALT TOLERANT  
NITROGEN, PHOSPHORUS, AND  
POTASSIUM FERTILIZER NUTRIENT
-  SEEDING, CLASS 2A  
NITROGEN, PHOSPHORUS, AND  
POTASSIUM FERTILIZER NUTRIENT



MATCH LINE STATION 505+00  
SEE SHEET NO. 340

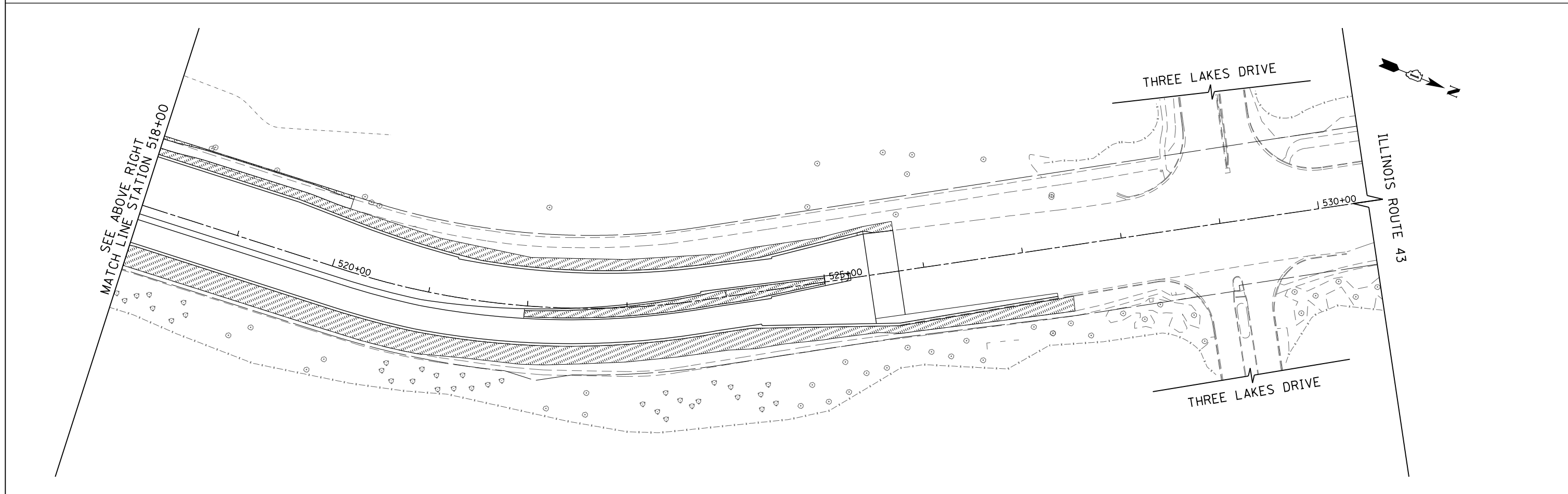
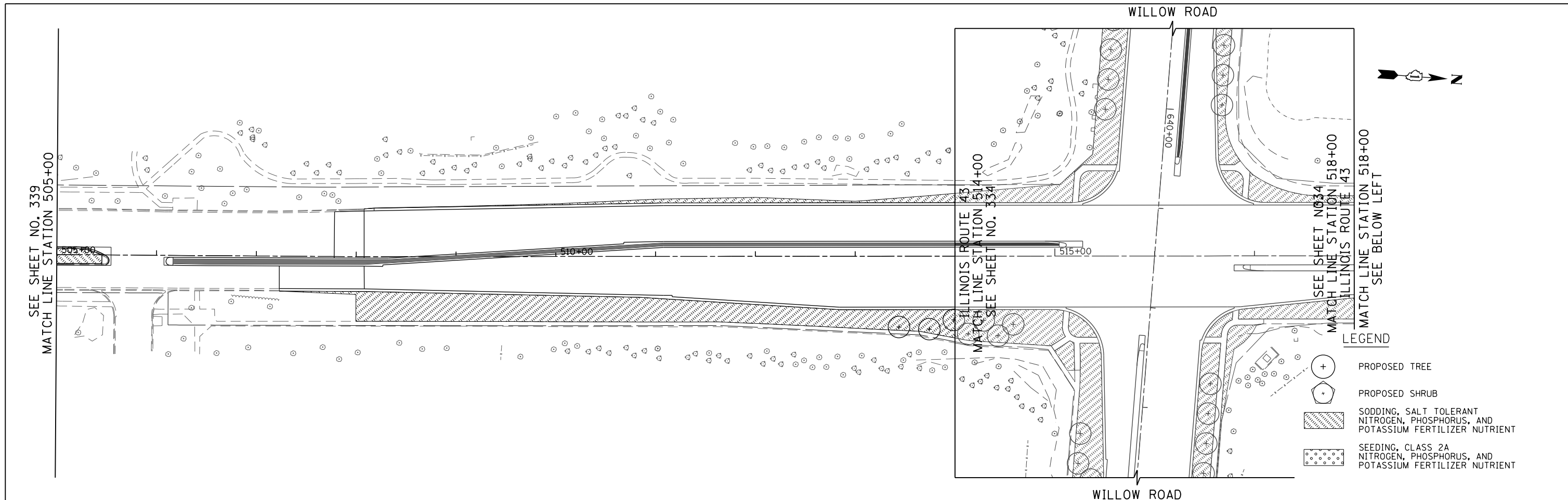
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	PLOT DATE = 11/1/2012	DATE - 10/31/2012	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

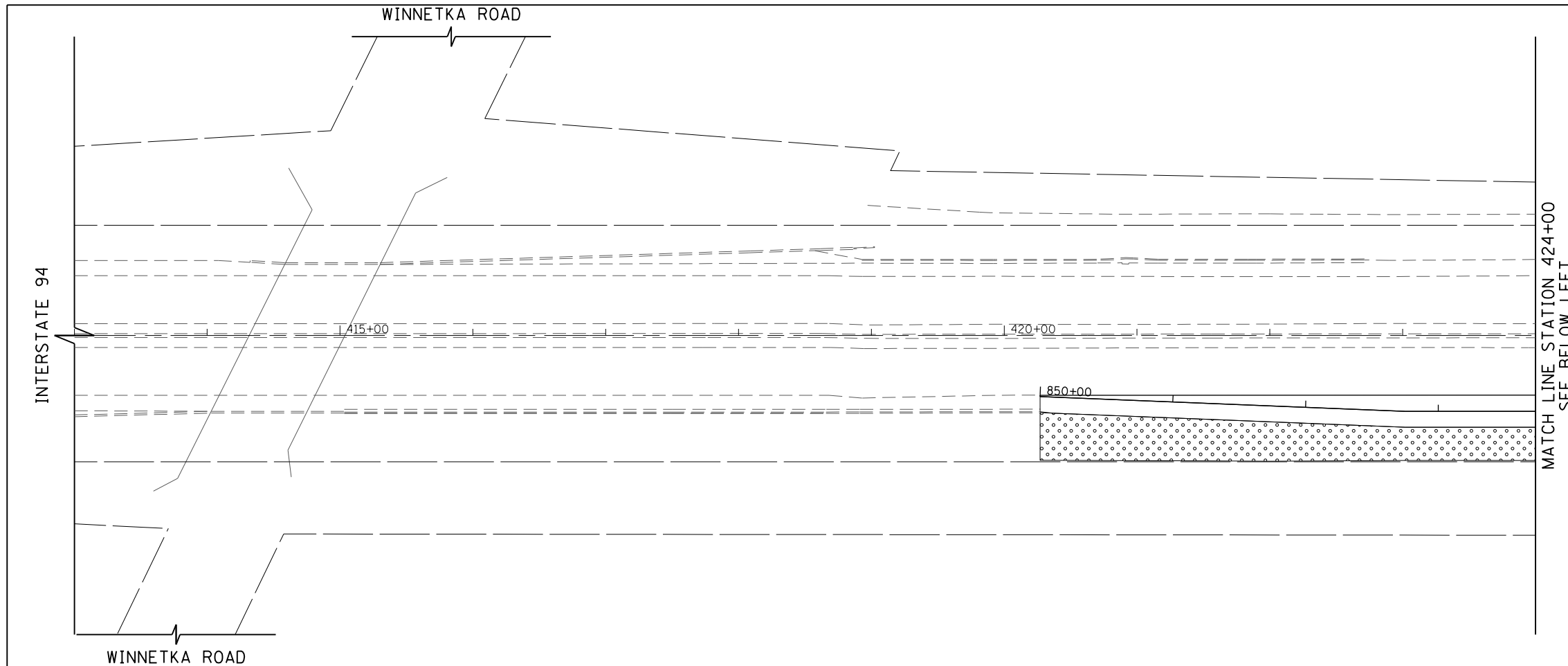
**ILLINOIS ROUTE 43 LANDSCAPING PLAN**

SCALE: 1" = 50'    SHEET NO. 339 OF 919 SHEETS    STA. 493+00    TO STA. 505+00





F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920.01,1518,2022&1922.4B)R	COOK	919	339
FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT			CONTRACT NO. 60T35	

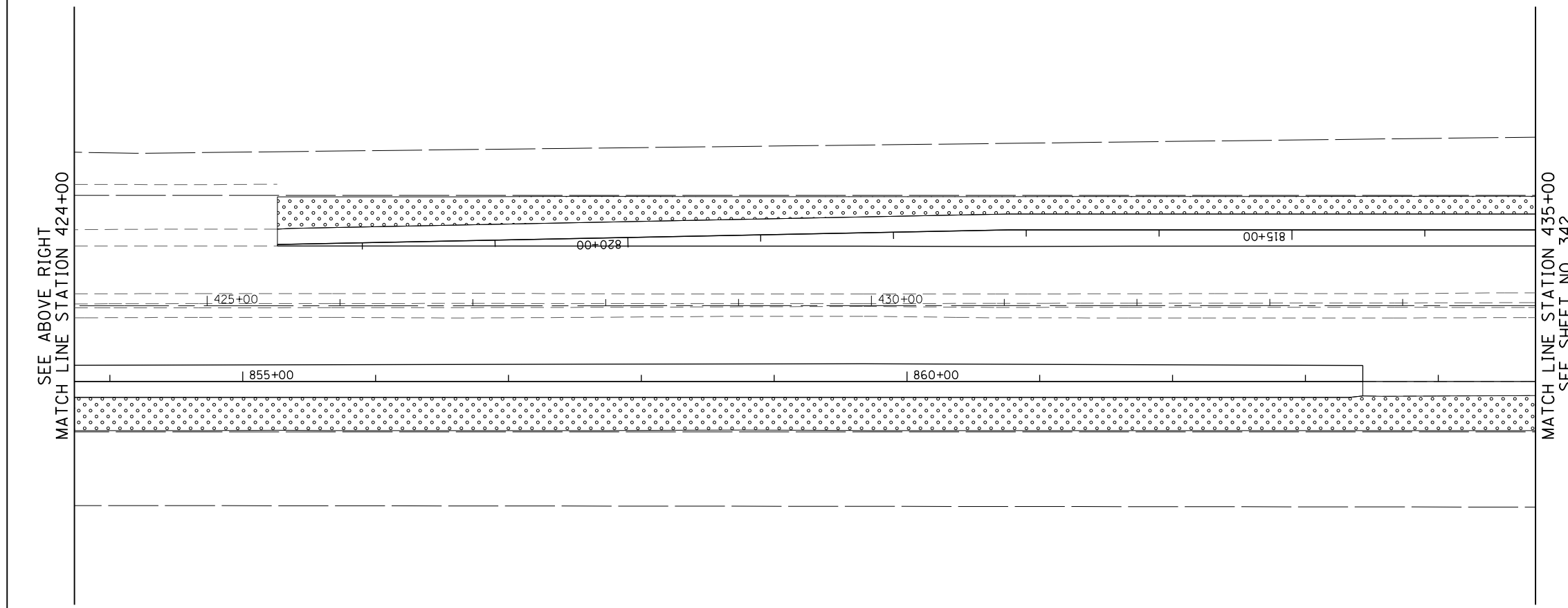


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	PLOT SCALE = 100.000' / in.	DRAWN - KAL	REVISED -			305	(1920,01,1518,2022&1922,4)R	COOK	919	340	
	PLOT DATE = 11/1/2012	CHECKED - RCB	REVISED -			CONTRACT NO. 60T35					
		DATE - 10/31/2012	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					



**LEGEND**

-  PROPOSED TREE
-  PROPOSED SHRUB
-  SODDING, SALT TOLERANT NITROGEN, PHOSPHORUS, AND POTASSIUM FERTILIZER NUTRIENT
-  SEEDING, CLASS 2A NITROGEN, PHOSPHORUS, AND POTASSIUM FERTILIZER NUTRIENT



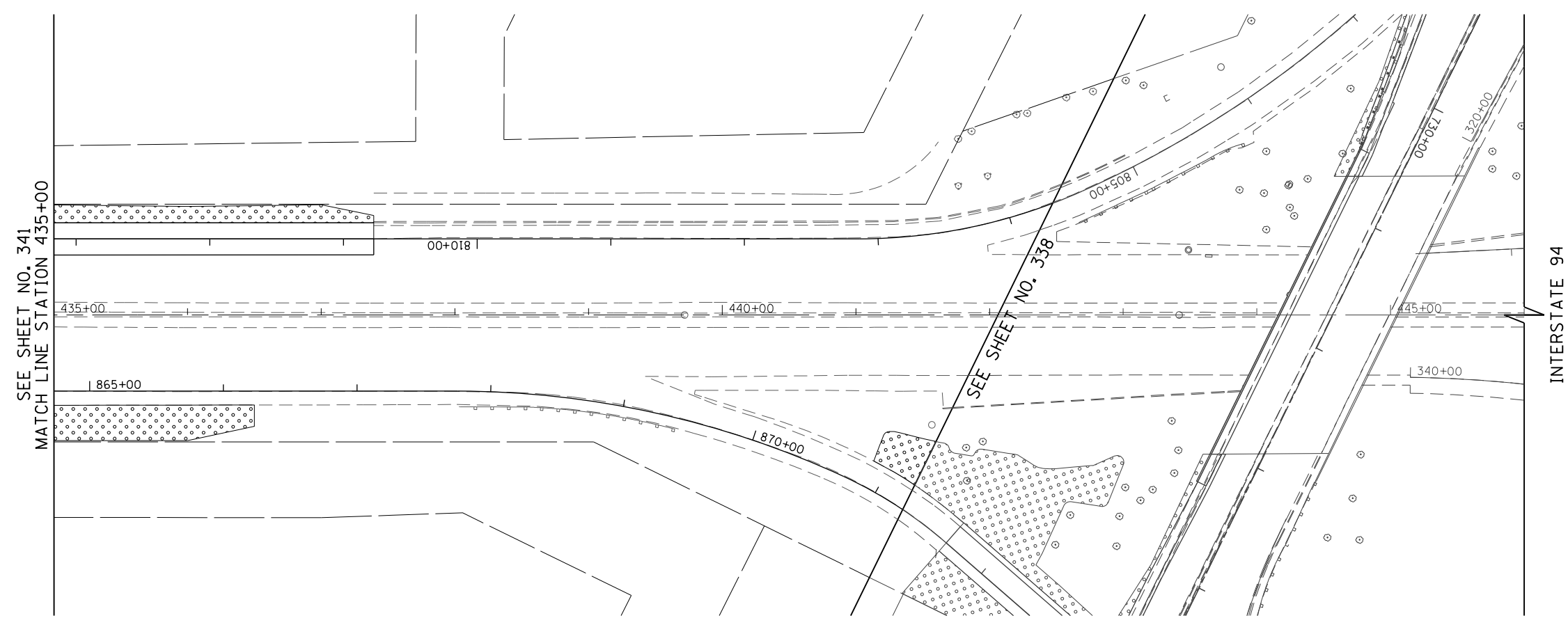
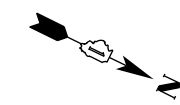
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	PLOT SCALE = 100.0000' / in.	CHECKED - RCB	REVISED -
	PLOT DATE = 11/1/2012	DATE - 10/31/2012	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**INTERSTATE 94 LANDSCAPING PLAN**

SCALE: 1" = 50'    SHEET NO. 341 OF 919 SHEETS    STA. 413+00    TO STA. 435+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920.01,1518,2022&1922.4)R	COOK	919	341
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60T35	



- LEGEND**
- PROPOSED TREE
  - PROPOSED SHRUB
  - SODDING, SALT TOLERANT NITROGEN, PHOSPHORUS, AND POTASSIUM FERTILIZER NUTRIENT
  - SEEDING, CLASS 2A NITROGEN, PHOSPHORUS, AND POTASSIUM FERTILIZER NUTRIENT

FILE NAME =	USER NAME = BAW:ort	DESIGNED - JLV	REVISED -
G:\CH11\0158\Road\Sheets\0160T35-SHT-LN\SC-11.dgn		DRAWN - MNB	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED - RCB	REVISED -
	PLOT DATE = 11/1/2012	DATE - 10/31/2012	REVISED -

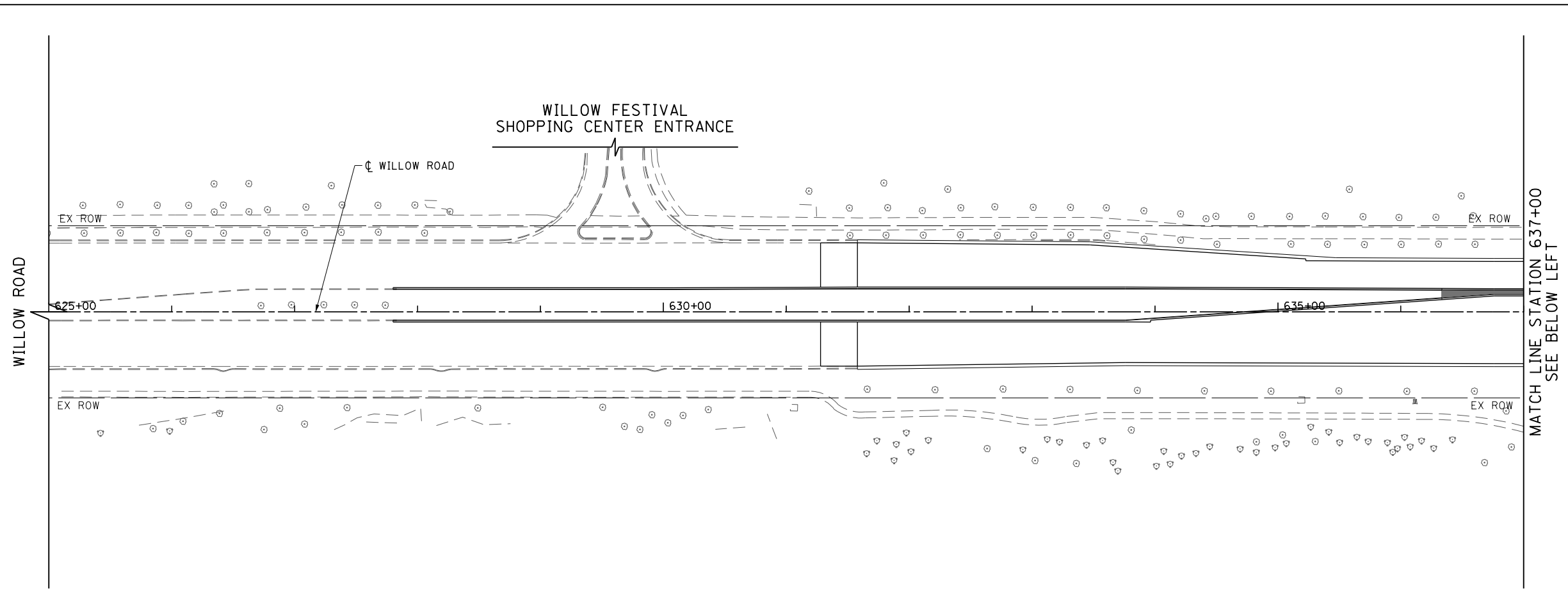
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**INTERSTATE 94 LANDSCAPING PLAN**

SCALE: 1" = 50'    SHEET NO. 342 OF 919 SHEETS    STA. 435+00    TO STA. 446+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920.01,1518,2022&1922.4)R	COOK	919	342
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60T35	

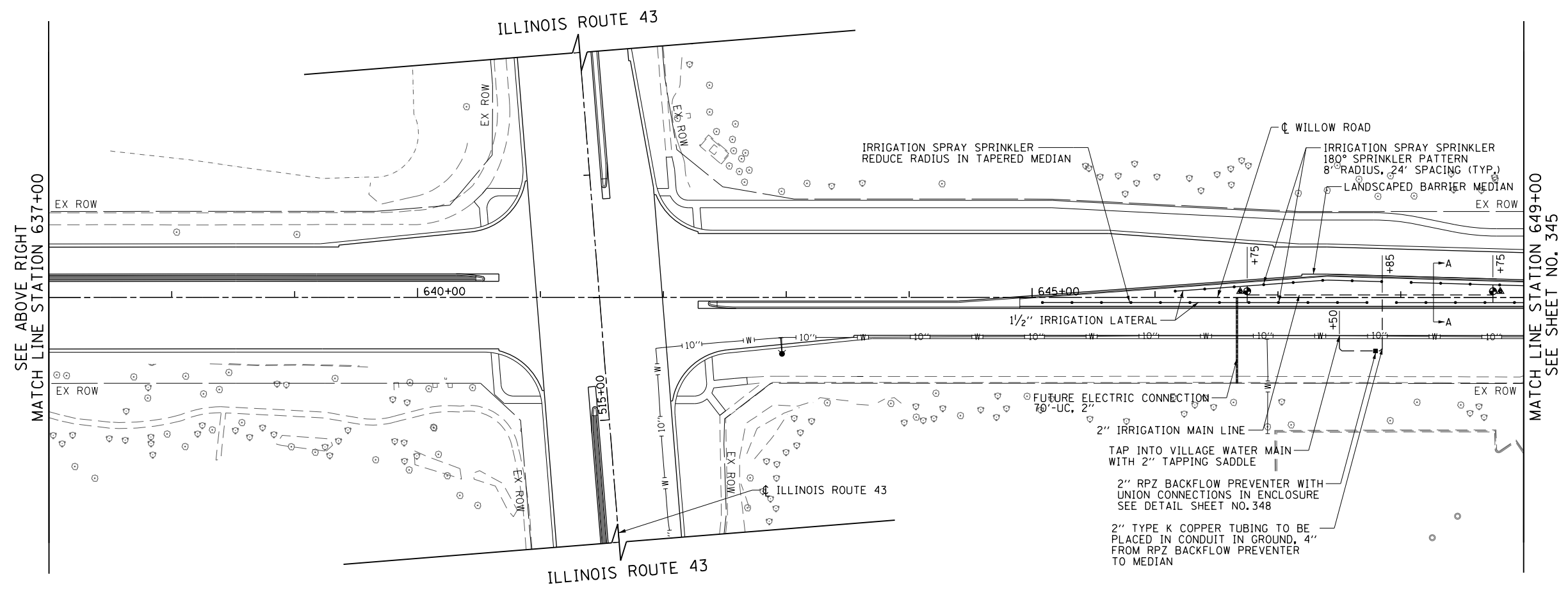




**IRRIGATION LEGEND**

- 10" VALVE IN VALVE BOX
- CONTROLLER FOR VALVE IN VALVE BOX
- 2" PVC
- 1/2" PVC
- 2" UG CONDUIT, GALVANIZED STEEL
- SPRAY SPRINKLER
- BACKFLOW PREVENTER IN ENCLOSURE
- QUICK COUPLER

MATCH LINE STATION 637+00  
SEE BELOW LEFT



**NOTES**

1. ALL LABOR AND MATERIALS REQUIRED FOR INSTALLATION OF A COMPLETE IRRIGATION SYSTEM SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR IRRIGATION SYSTEM.
2. THE CONDUIT FOR FUTURE MEDIAN ELECTRIC CONNECTIONS WILL BE MEASURED FOR PAYMENT SEPARATELY.

MATCH LINE STATION 649+00  
SEE SHEET NO. 345



FILE NAME =	USER NAME = BAWtor.t	DESIGNED - JLV	REVISED -
G:\CH11\0158\Road\Sheets\0160T35-SHT-IRRIG-01.dgn		DRAWN - KAL	REVISED -
PLOT SCALE = 100.000' / in.		CHECKED - RCB	REVISED -
PLOT DATE = 11/28/2012		DATE - 10/31/2012	REVISED -

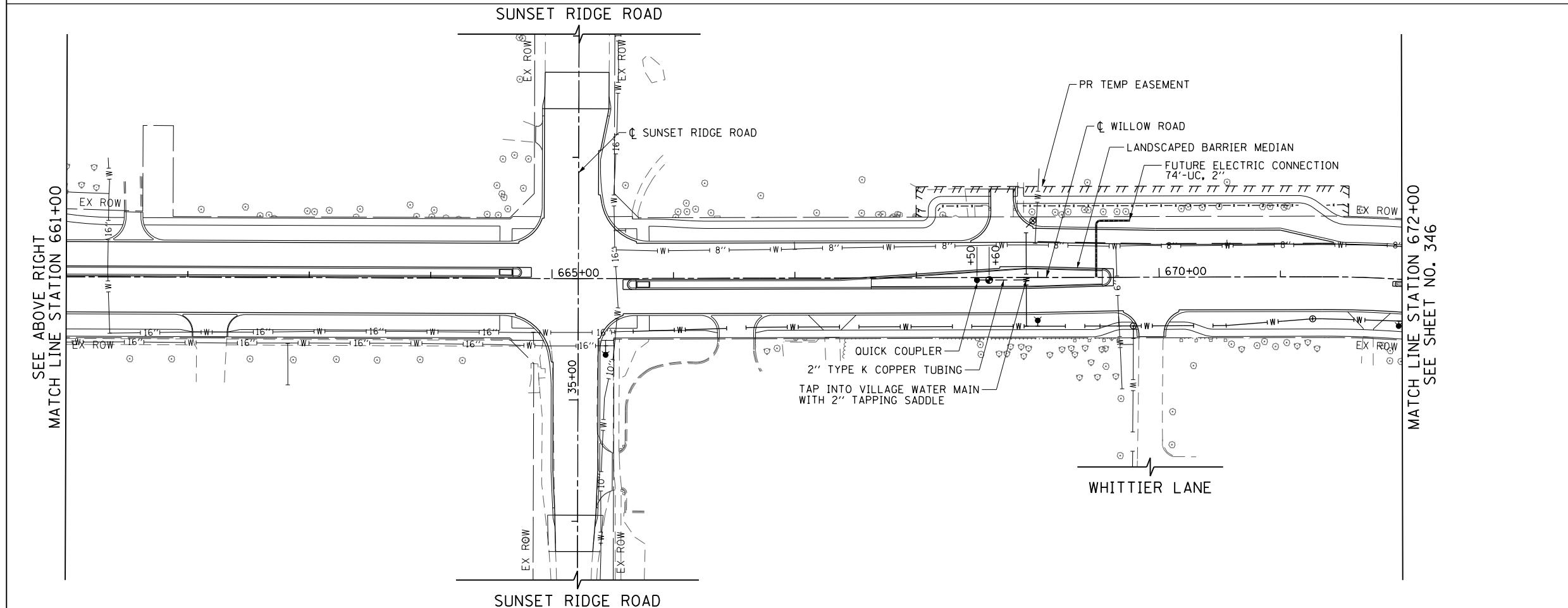
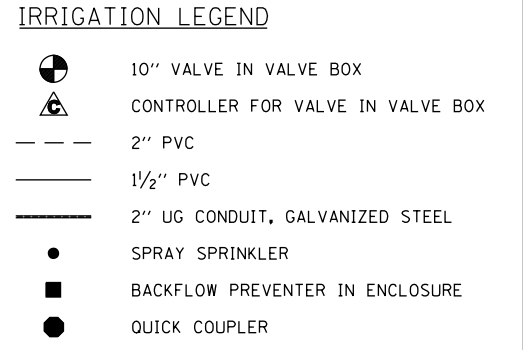
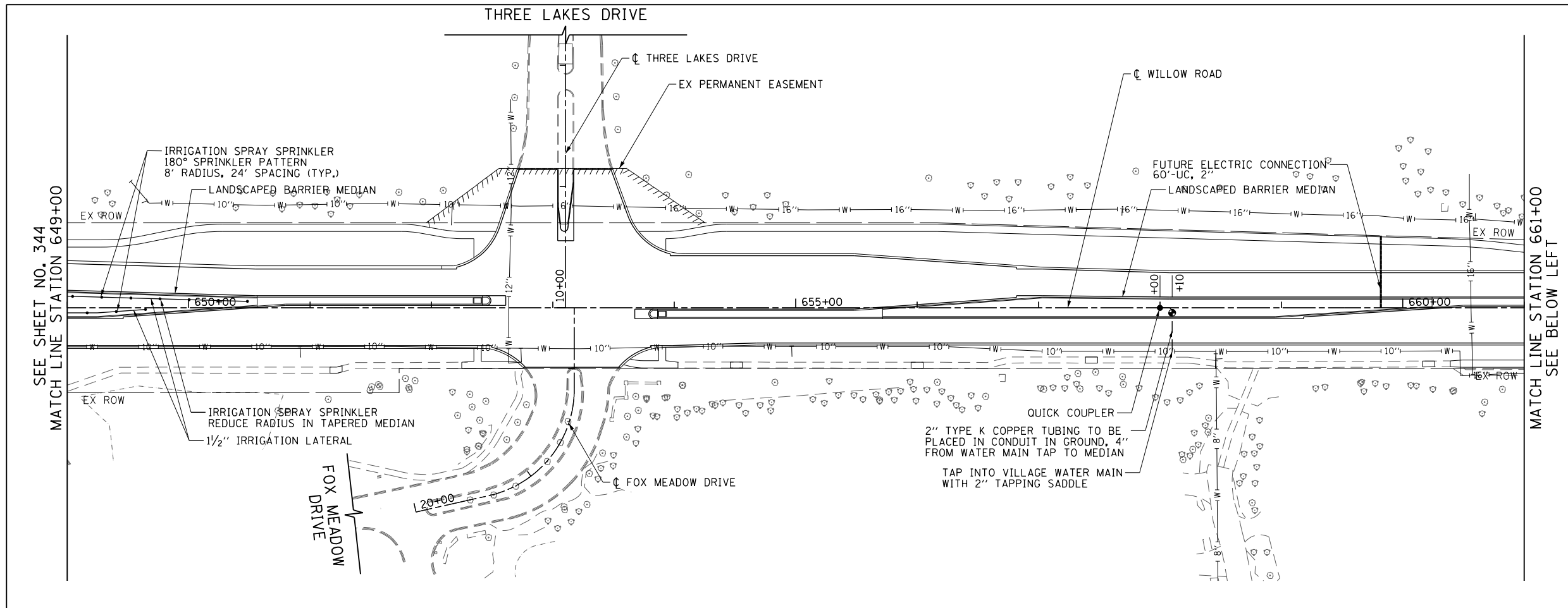
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**WILLOW ROAD IRRIGATION PLAN**

SCALE: 1" = 50'    SHEET NO. 344 OF 919 SHEETS    STA. 625+00    TO STA. 649+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920.01,1518,2022&1922.4)BR	COOK	919	344
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60T35	





- NOTES**
1. ALL LABOR AND MATERIALS REQUIRED FOR INSTALLATION OF A COMPLETE IRRIGATION SYSTEM SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR IRRIGATION SYSTEM.
  2. THE CONDUIT FOR FUTURE MEDIAN ELECTRIC CONNECTIONS WILL BE MEASURED FOR PAYMENT SEPARATELY.

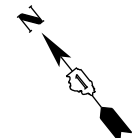
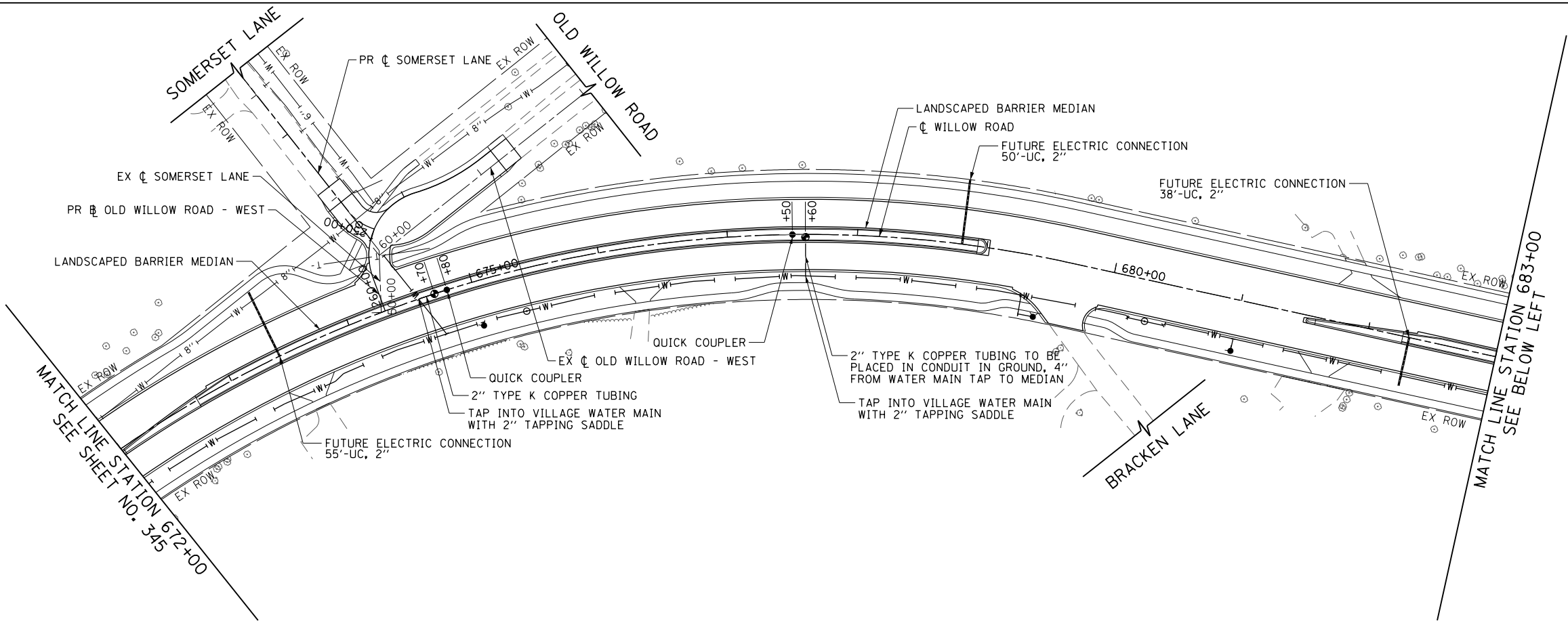
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		CHECKED - RCB	REVISED -
		DATE - 10/31/2012	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

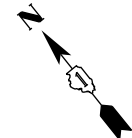
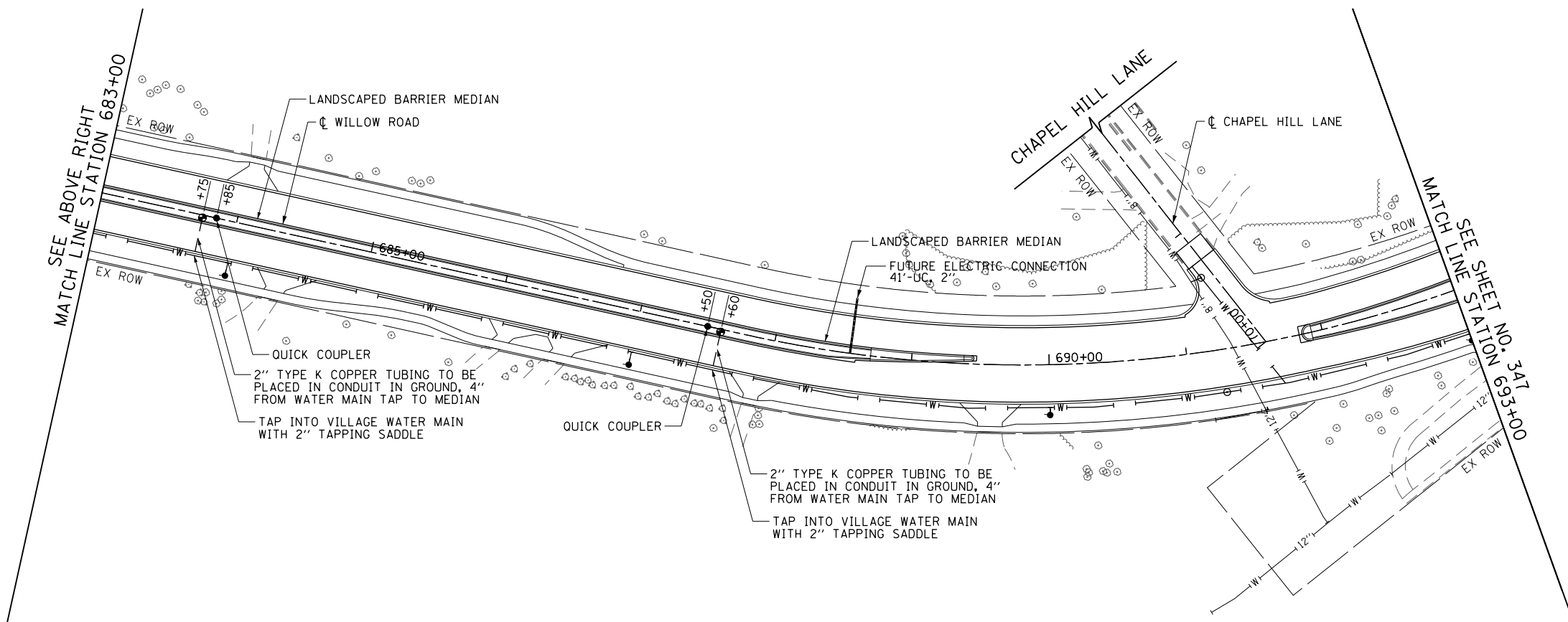
**WILLOW ROAD IRRIGATION PLAN**

SCALE: 1" = 50'    SHEET NO. 345 OF 919 SHEETS    STA. 649+00    TO STA. 672+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920.01,1518,2022&1922.4)BR	COOK	919	345
CONTRACT NO. 60T35				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



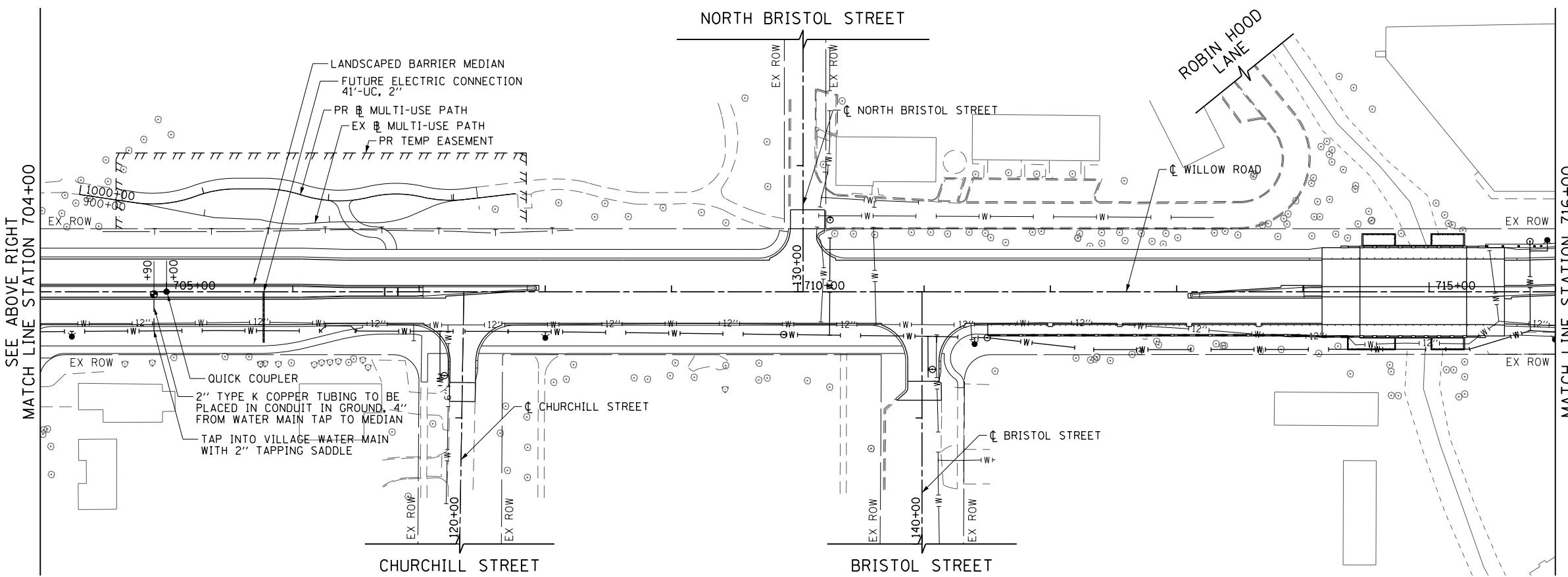
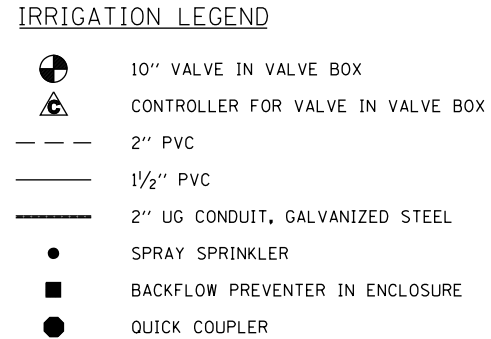
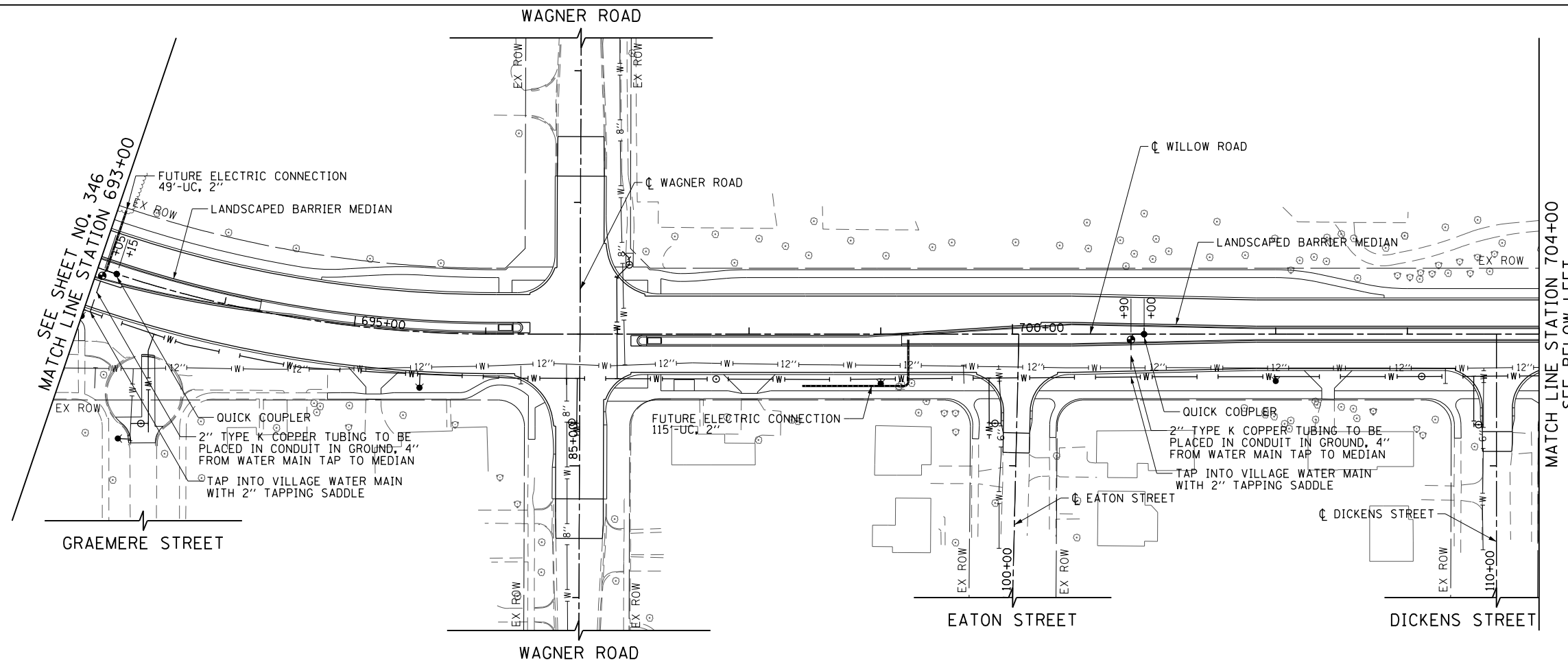
- IRRIGATION LEGEND**
- 10" VALVE IN VALVE BOX
  - CONTROLLER FOR VALVE IN VALVE BOX
  - 2" PVC
  - 1/2" PVC
  - 2" UG CONDUIT, GALVANIZED STEEL
  - SPRAY SPRINKLER
  - BACKFLOW PREVENTER IN ENCLOSURE
  - QUICK COUPLER



- NOTES**
1. ALL LABOR AND MATERIALS REQUIRED FOR INSTALLATION OF A COMPLETE IRRIGATION SYSTEM SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR IRRIGATION SYSTEM.
  2. THE CONDUIT FOR FUTURE MEDIAN ELECTRIC CONNECTIONS WILL BE MEASURED FOR PAYMENT SEPARATELY.

FILE NAME =	USER NAME = BAWtor.t	DESIGNED - JLV	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>WILLOW ROAD IRRIGATION PLAN</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
G:\CH11\0158\Road\Sheets\0160T35-SHT-IRRIG-03.dgn		DRAWN - KAL	REVISED -			305	(1920.01,1518,2022&1922.4)R	COOK	919	346	
PLOT SCALE = 100.000' / in.		CHECKED - RCB	REVISED -			CONTRACT NO. 60T35					
PLOT DATE = 11/28/2012		DATE - 10/31/2012	REVISED -			FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT					

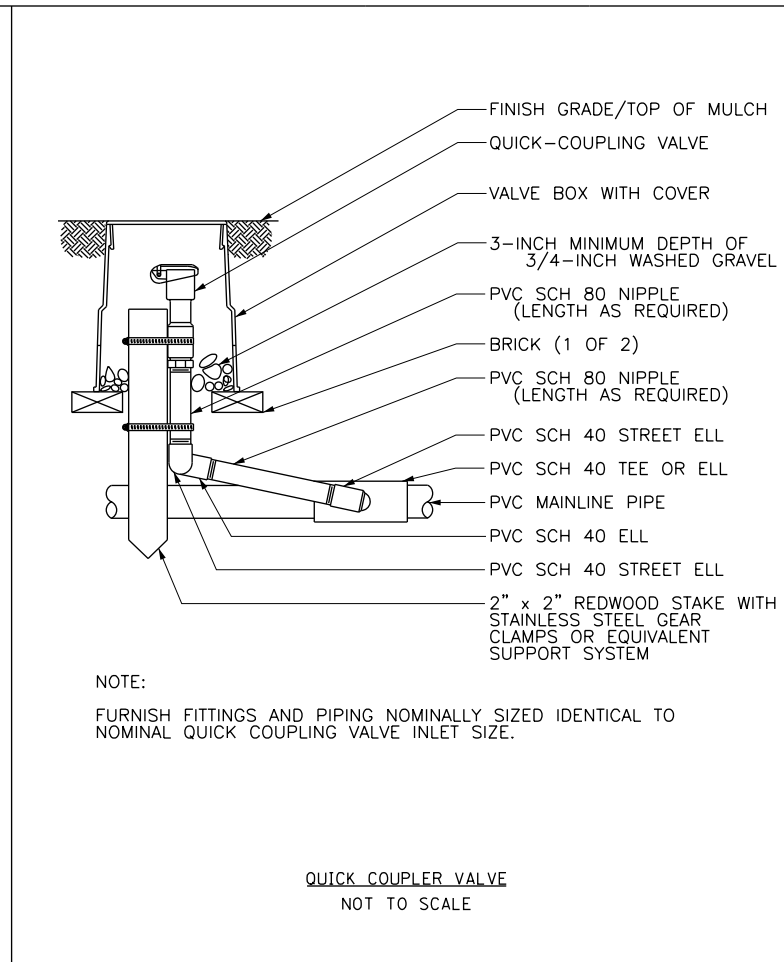
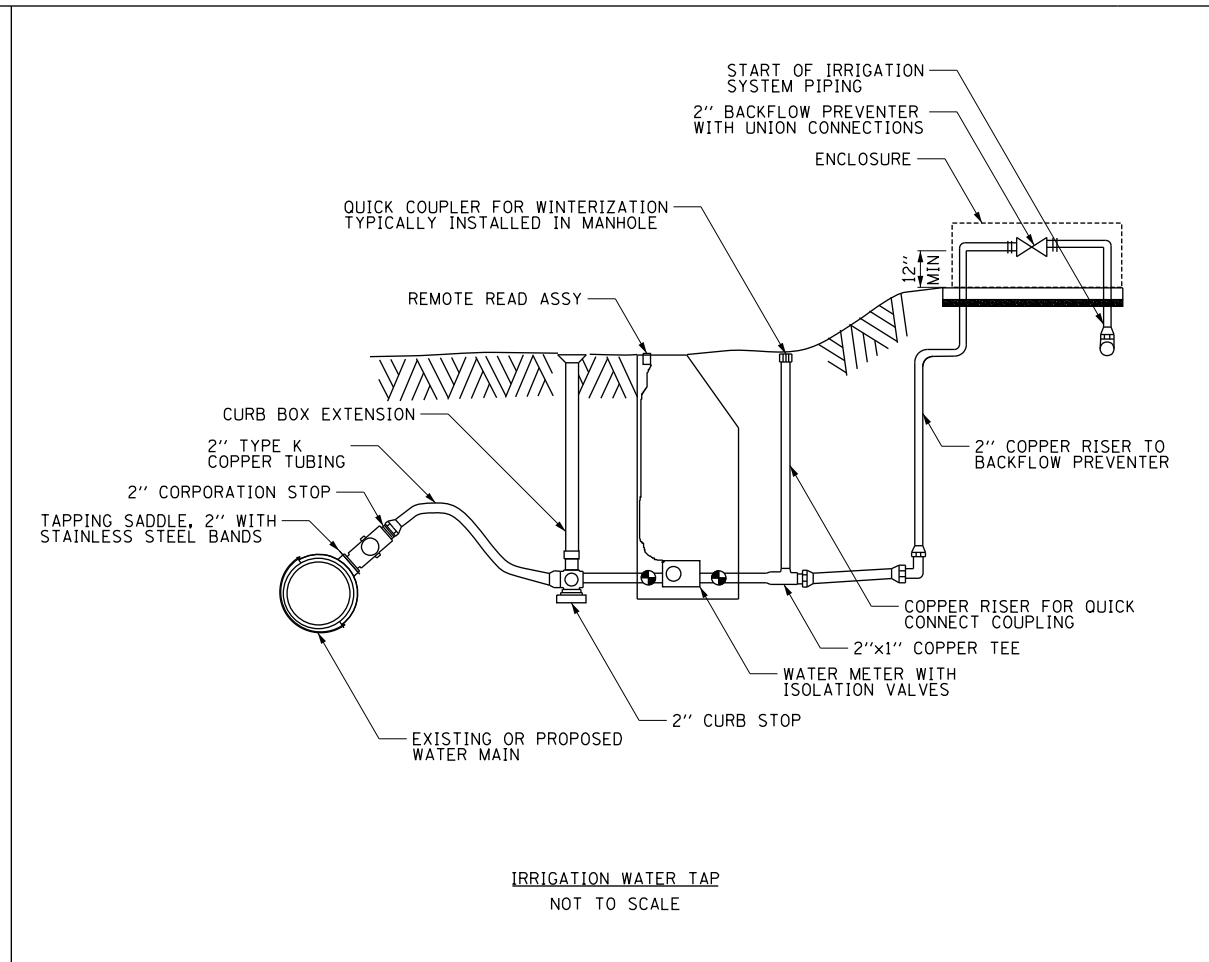
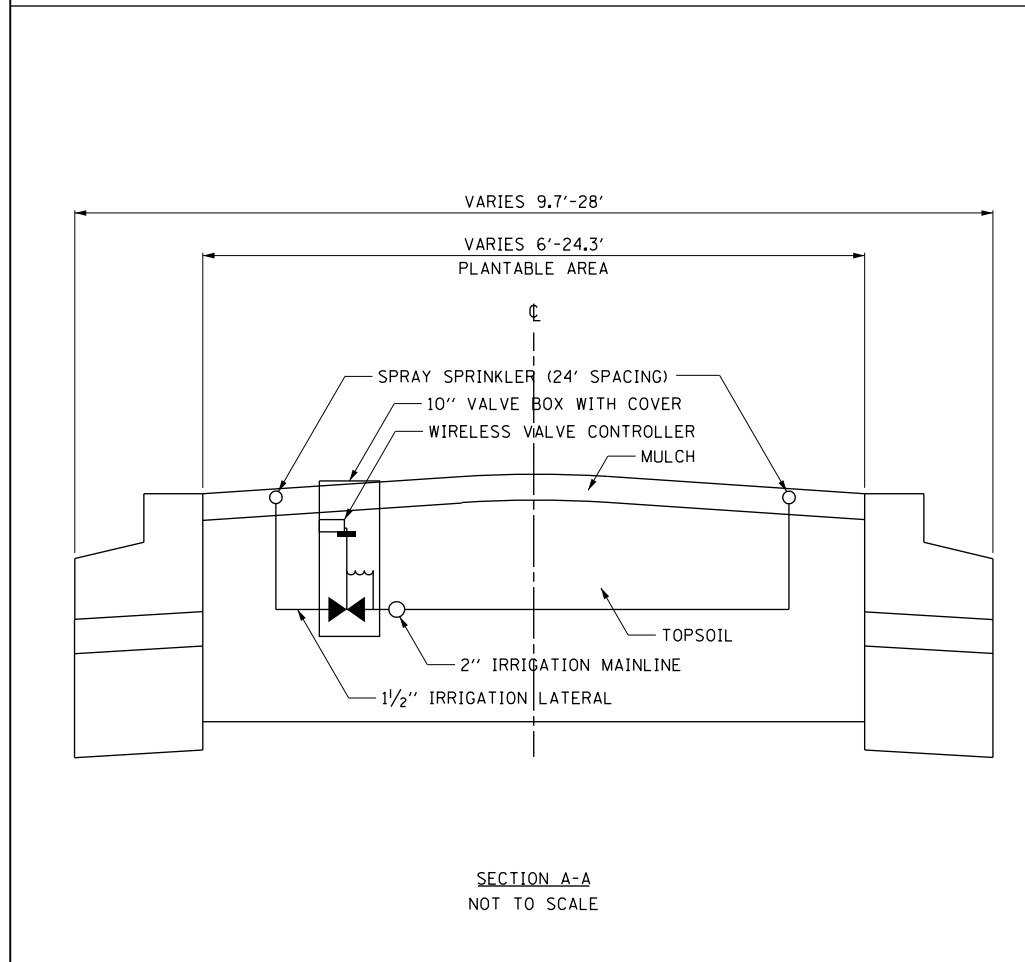
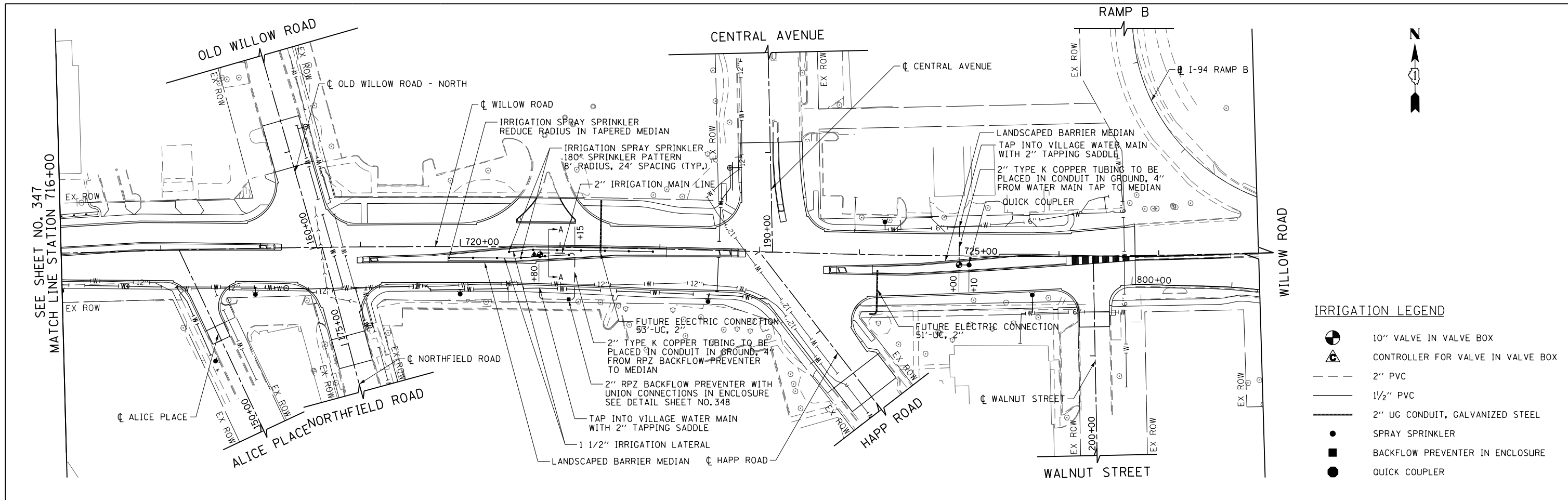
SCALE: 1" = 50'    SHEET NO. 346 OF 919 SHEETS    STA. 672+00    TO STA. 693+00



- NOTES**
1. ALL LABOR AND MATERIALS REQUIRED FOR INSTALLATION OF A COMPLETE IRRIGATION SYSTEM SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR IRRIGATION SYSTEM.
  2. THE CONDUIT FOR FUTURE MEDIAN ELECTRIC CONNECTIONS WILL BE MEASURED FOR PAYMENT SEPARATELY.



FILE NAME = G:\CH11\0158\Road\Sheets\0160T35-SHT-IRRIG-04.dgn	USER NAME = BAWtor.t	DESIGNED - JLV	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>WILLOW ROAD IRRIGATION PLAN</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
PLOT SCALE = 100.000' / in.	CHECKED - RCB	REVISIED -	REVISIED -		SCALE: 1" = 50'	SHEET NO. 347 OF 919 SHEETS	STA. 693+00	TO STA. 716+00	305	(1920.01,1518,2022&1922.4BR)	COOK	919	347
PLOT DATE = 11/28/2012	DATE - 10/31/2012	REVISIED -	REVISIED -					CONTRACT NO. 60T35					
								FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT					





**TRAFFIC SIGNALS SUMMARY OF QUANTITIES**

LOCATION OF WORK			WILLOW @ IL R3 (WAUKEGAN RD)	WILLOW @ THREE LAKES/ FOX MEADOW DR	WILLOW @ SUNSET RIDGE RD	WILLOW @ WAGNER RD	WILLOW @ CLARKSON PARK/ PEDESTRIAN SIGNAL	WILLOW @ OLD WILLOW RD/ NORTHFIELD RD	WILLOW @ CENTRAL AVE/ HAPP RD	IL 43 @ THREE LAKES/ KRAFT TEMPORARY SIGNAL	WILLOW RD INTERCONNECT
ITEM	UNIT	GRAND TOTAL									
SIGN PANEL - TYPE 1	SO FT	73.5	73.5								
SIGN PANEL - TYPE 2	SO FT	30	30								
SERVICE INSTALLATION - GROUND MOUNTED	EACH	2	1	1							
SERVICE INSTALLATION - POLE MOUNTED	EACH	5			1	1	1	1	1		
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	13,695	1818	725	732	804	664	962	824		7166
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	436	89	39	52	37	70	57	92		
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	479	68	60	75	98	68	65	45		
UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	3,433	866	587	411	430	110	434	595		
CONDUIT ATTACHED TO STRUCTURE, 2" DIA., PVC COATED GALVANIZED STEEL	FOOT	90									90
JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 16"X12"X8"	EACH	2									2
HANDHOLE	EACH	36	4	4	4	4	3	4	4		9
HEAVY-DUTY HANDHOLE	EACH	23	4	3	4	4		4	4		
DOUBLE HANDHOLE	EACH	15	4	2	2	2	1	2	2		
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2									2
TRANSCEIVER - FIBER OPTIC	EACH	7	1	1	1	1	1	1	1		
ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	14,662									14662
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	8,954	1660	1608	1274	1303	180	1308	1621		
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	12,894	2780	2545	1635	1679	290	1645	2320		
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	13,532	3943	2874	1197	1270	1052	1174	2022		
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	9,904	1802	923	1985	1523		1514	2157		
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	18,256	6328	1707	2460	2407	692	2154	2508		
ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	749	80	45	60	113	115	234	102		
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	5,418	925	781	669	769	519	894	861		
TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	4	4								
STEEL MAST ARM ASSEMBLY AND POLE 46 FT.	EACH	1	1								
STEEL MAST ARM ASSEMBLY AND POLE 48 FT.	EACH	1	1								
STEEL MAST ARM ASSEMBLY AND POLE 50 FT.	EACH	1	1								
STEEL MAST ARM ASSEMBLY AND POLE 54 FT.	EACH	1	1								
CONCRETE FOUNDATION, TYPE A	FOOT	96	20	12	12	16	16	8	12		
CONCRETE FOUNDATION, TYPE C	FOOT	28	4	4	4	4	4	4	4		
CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	51						37	14		
CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	260	56	38	48	44	22	13	39		
CONCRETE FOUNDATION, TYPE E 42-INCH DIAMETER	FOOT	25		25							
DRILL EXISTING HANDHOLE	EACH	3									3
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	50	12	9	6	6	4	6	7		
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	1		1							
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	12			3	3		4	2		
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	24	4	2	5	4		4	5		
SIGNAL HEAD, LED, 2-FACE, 3 SECTION, BRACKET MOUNTED	EACH	2					2				
SIGNAL HEAD, LED, 2-FACE, 5 SECTION, BRACKET MOUNTED	EACH	2			1				1		
SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED	EACH	8	4	2		1			1		
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	38	8	4	6	8	2	4	6		
PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	6		2	1			2	1		
TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	74	16	11	11	10	4	10	12		
INDUCTIVE LOOP DETECTOR	EACH	68	19	7	11	10	2	9	10		
DETECTOR LOOP, TYPE I	FOOT	93		93							
PREFORMED DETECTOR LOOP	FOOT	5,828	936	861	1007	901	118	931	1074		
* LIGHT DETECTOR	EACH	17	4	3	2	2	1	2	3		
* LIGHT DETECTOR AMPLIFIER	EACH	7	1	1	1	1	1	1	1		
PEDESTRIAN PUSH-BUTTON	EACH	50	8	8	8	8	2	8	8		
TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	7	1	1	1	1		1	1	1	
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	6,542									6542
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	6	1	1	1	1		1	1		

\* 100% VILLAGE COST

**TRAFFIC SIGNALS SUMMARY OF QUANTITIES (CONT.)**

LOCATION OF WORK			WILLOW @ IL R3 (WAUKEGAN RD)	WILLOW @ THREE LAKES/ FOX MEADOW DR	WILLOW @ SUNSET RIDGE RD	WILLOW @ WAGNER RD	WILLOW @ WILLOW PARK/ CLARKSON PARK PEDESTRIAN SIGNAL	WILLOW @ OLD WILLOW RD/ NORTHFIELD RD	WILLOW @ CENTRAL AVE/ HAPP RD	IL 43 @ THREE LAKES/ KRAFT TEMPORARY SIGNAL	WILLOW RD INTERCONNECT
ITEM	UNIT	GRAND TOTAL									
REBUILD EXISTING HANDHOLE	EACH	1									1
REMOVE EXISTING HANDHOLE	EACH	67	14	9	10	10		8	14		2
REMOVE EXISTING CONCRETE FOUNDATION	EACH	55	10	9	9	9		9	9		
* EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO 20 3/C	FOOT	3,589	1064	888	305	320	96	303	613		
FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL	EACH	6		1	1	1	1	1	1		
FULL-ACTUATED CONTROLLER AND TYPE V CABINET, SPECIAL	EACH	1	1								
MASTER CONTROLLER (SPECIAL)	EACH	1	1								
UNINTERRUPTABLE POWER SUPPLY, SPECIAL	EACH	7	1	1	1	1	1	1	1		
FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM24F	FOOT	14,777									14777
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 24 FT. (SPECIAL)	EACH	2					2				
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 30 FT. (SPECIAL)	EACH	1				1					
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 34 FT. (SPECIAL)	EACH	2			1	1					
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 36 FT. (SPECIAL)	EACH	2		1	1						
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 38 FT. (SPECIAL)	EACH	2				2					
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 44 FT. (SPECIAL)	EACH	1			1						
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 46 FT. (SPECIAL)	EACH	1			1						
* ILLUMINATED STREET NAME SIGN	EACH	20		4	4	4		4	4		
OPTIMIZE TRAFFIC SIGNAL SYSTEM	EACH	1									1
TEMPORARY TRAFFIC SIGNAL TIMING	EACH	7	1	1	1	1		1	1	1	
TRAFFIC SIGNAL POST, ALUMINUM 10 FT. (SPECIAL)	EACH	4		1			2		1		
TRAFFIC SIGNAL POST, ALUMINUM 14 FT. (SPECIAL)	EACH	2					2				
TRAFFIC SIGNAL POST, ALUMINUM 16 FT. (SPECIAL)	EACH	11			3	4		2	2		
TRAFFIC SIGNAL POST, ALUMINUM 18 FT. (SPECIAL)	EACH	1		1							
STEEL MAST ARM ASSEMBLY AND POLE 26 FT. (SPECIAL)	EACH	1						1			
STEEL MAST ARM ASSEMBLY AND POLE 30 FT. (SPECIAL)	EACH	1						1			
STEEL MAST ARM ASSEMBLY AND POLE 34 FT. (SPECIAL)	EACH	1						1			
STEEL MAST ARM ASSEMBLY AND POLE 36 FT. (SPECIAL)	EACH	1							1		
STEEL MAST ARM ASSEMBLY AND POLE 40 FT. (SPECIAL)	EACH	1						1			
STEEL MAST ARM ASSEMBLY AND POLE 42 FT. (SPECIAL)	EACH	3							3		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 40 FT. (SPECIAL)	EACH	1		1							
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 65 FT. (SPECIAL)	EACH	1		1							
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH DUAL MAST ARMS, 48 FT. AND 30 FT. (SPECIAL)	EACH	1		1							

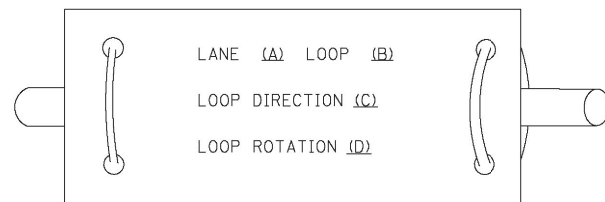
\* 100% VILLAGE COST



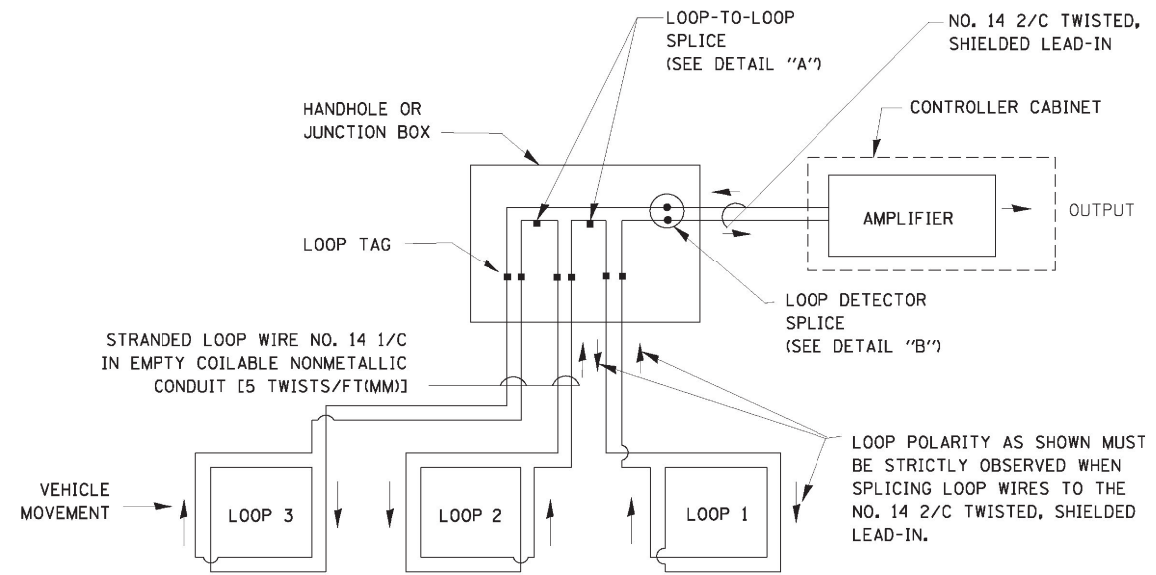
### LOOP DETECTOR NOTES

- EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

#### LOOP LEAD-IN CABLE TAG

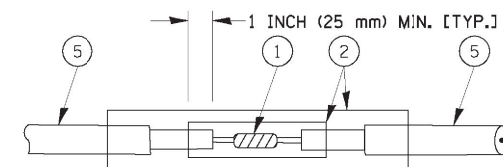


- LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- LOOP \*1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

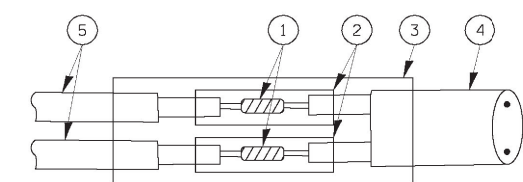


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

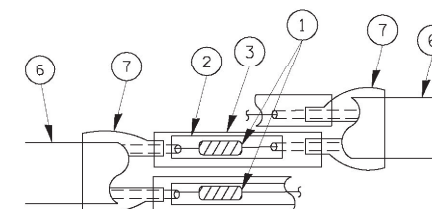


DETAIL "A"  
LOOP-TO-LOOP SPLICE

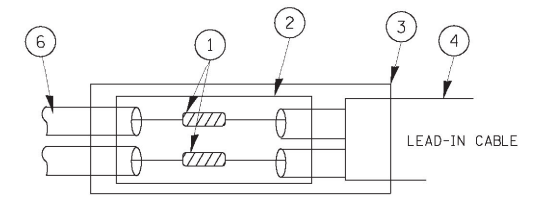


DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

#### TYPE I LOOP



DETAIL "A"  
LOOP-TO-LOOP SPLICE



DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

#### LOOP DETECTOR SPLICE

- WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- NO. 14 2/C TWISTED, SHIELDED CABLE.
- LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- PREFORMED LOOP
- XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

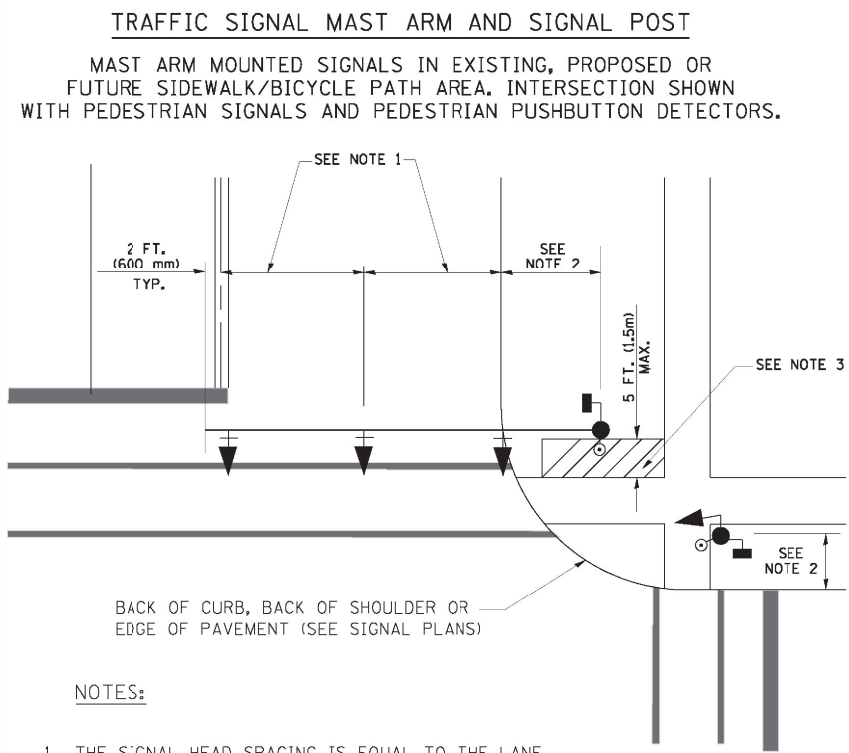
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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

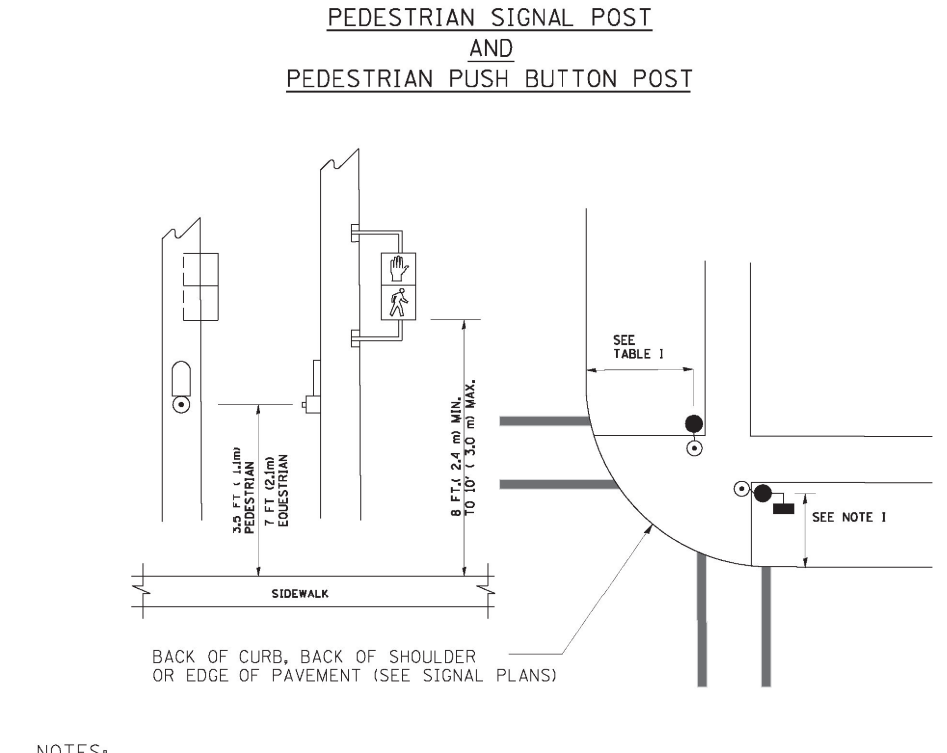
DISTRICT ONE - STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: N.T.S. SHEET NO. 1 OF 6 SHEETS STA. TO STA.

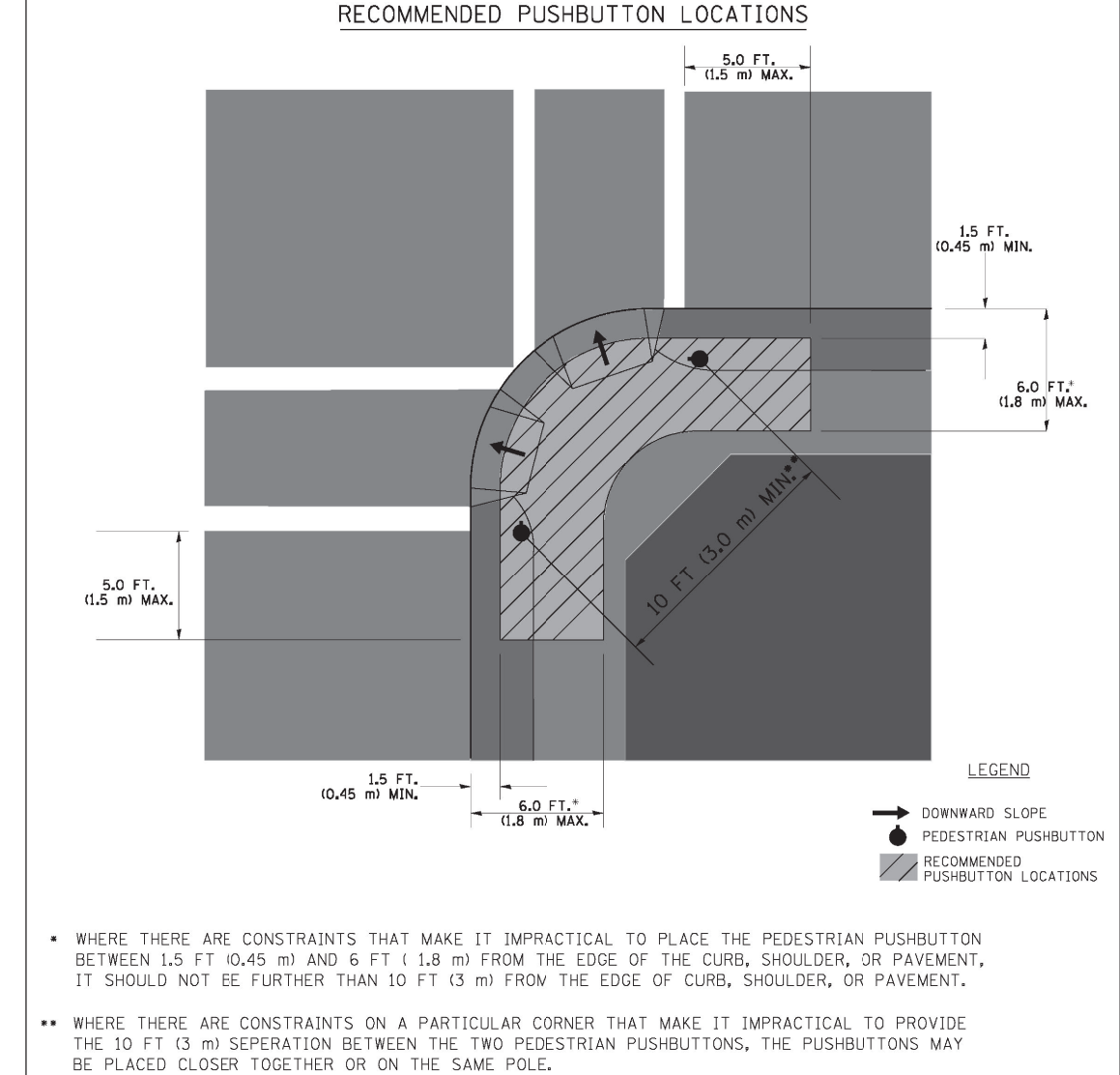
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920, .01, 1518, 2022 & 1922, 4BIR)	COOK	919	351
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60T35	



- NOTES:**
1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
  2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
  3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
  4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
  5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."



- NOTES:**
1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
  2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
  3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
  4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."



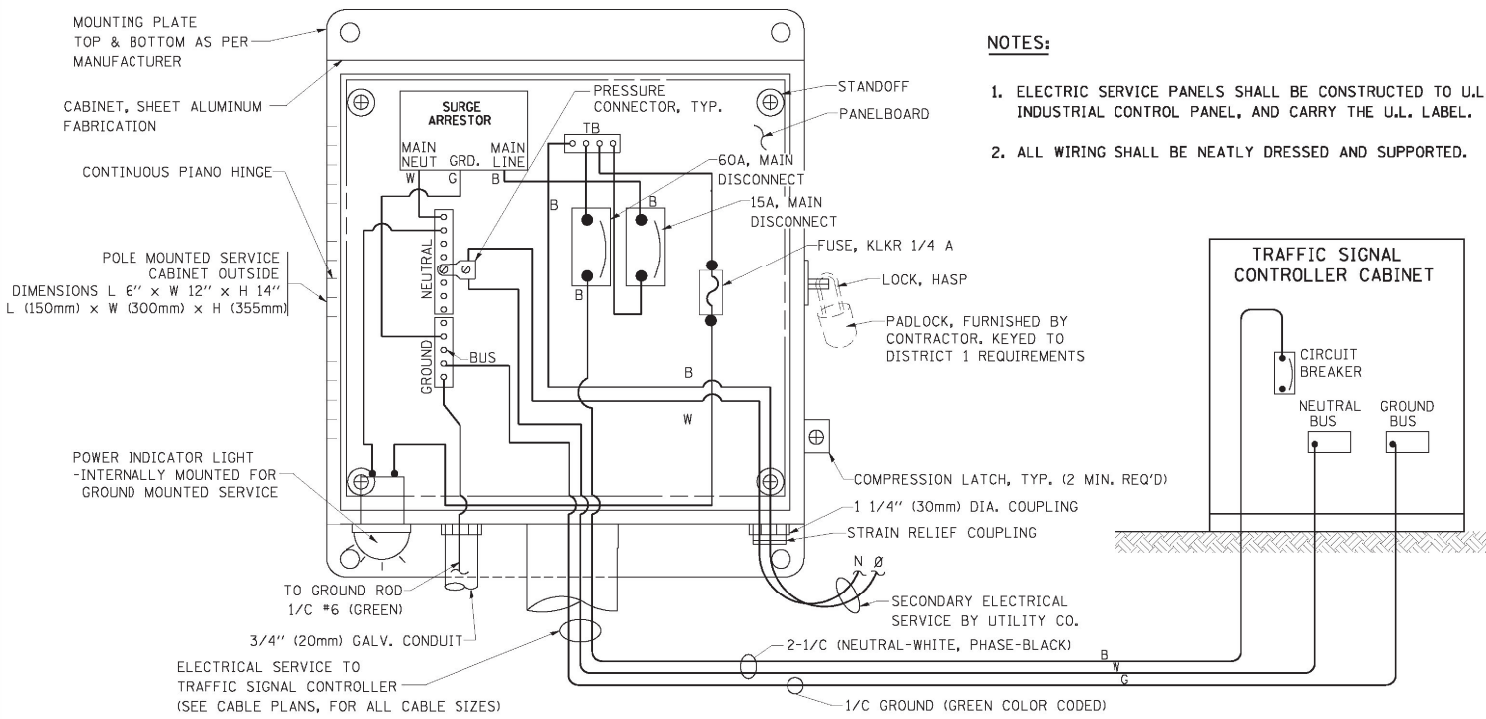
- NOTES:**
1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
  2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
  3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
  4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
  5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

### TRAFFIC SIGNAL EQUIPMENT OFFSET

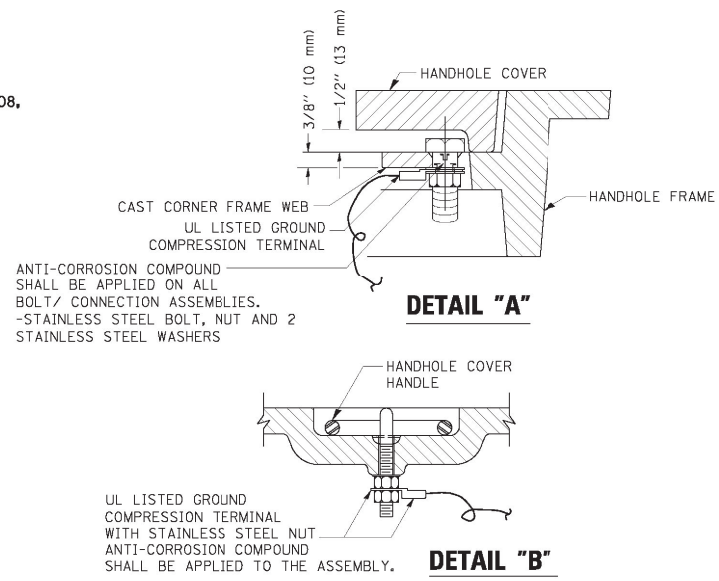
TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

- NOTES:**
1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
  2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
  3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
  4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.



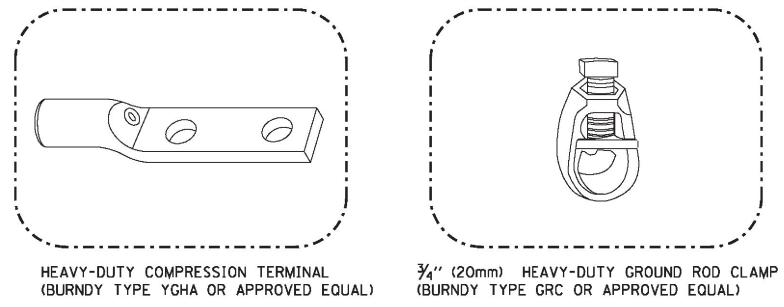
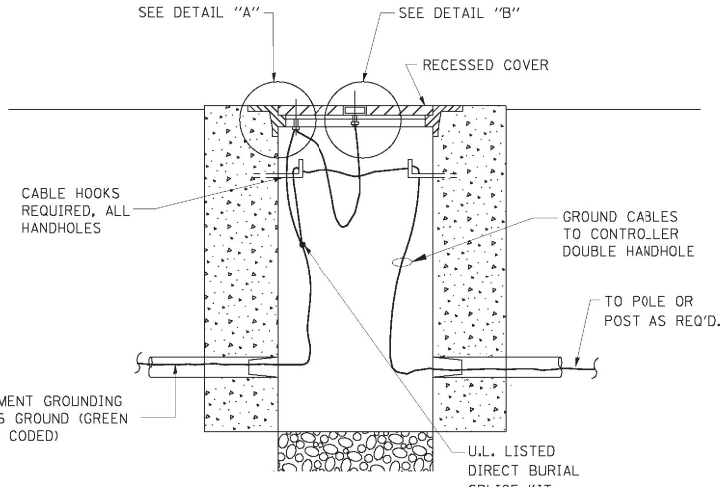


- NOTES:**
1. ELECTRIC SERVICE PANELS SHALL BE CONSTRUCTED TO U.L. STD 508, INDUSTRIAL CONTROL PANEL, AND CARRY THE U.L. LABEL.
  2. ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.



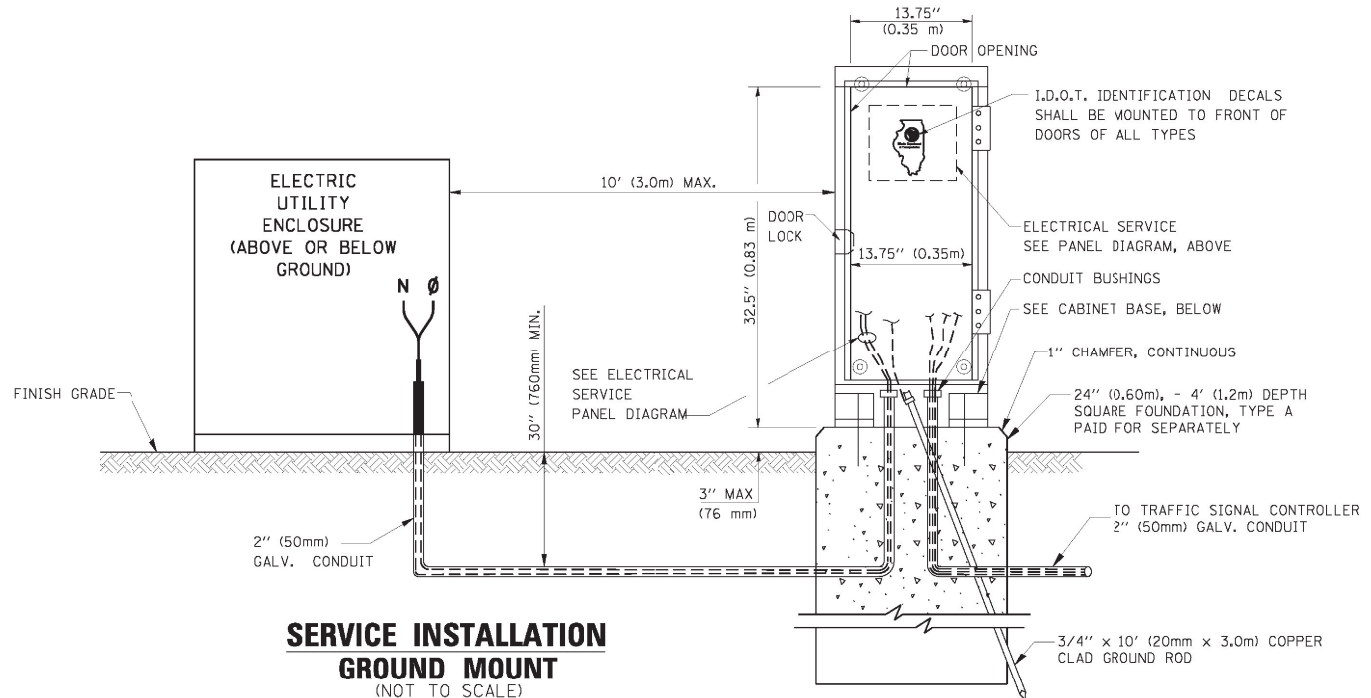
- NOTES:**
- GROUNDING SYSTEM**
1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
  2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
  3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
  4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.

**ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE) SERVICE INSTALLATION POLE MOUNT (SHOWN) (NOT TO SCALE)**

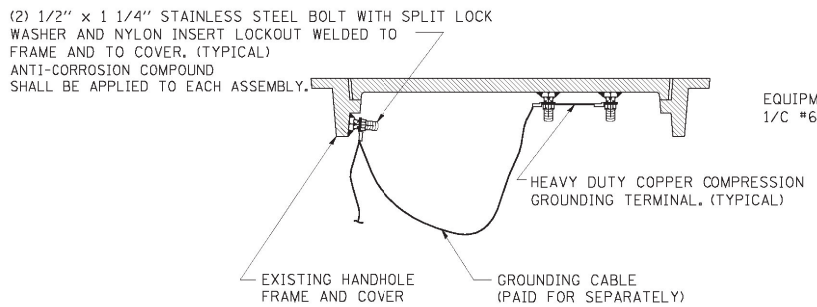


- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
  - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES. 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES. 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.

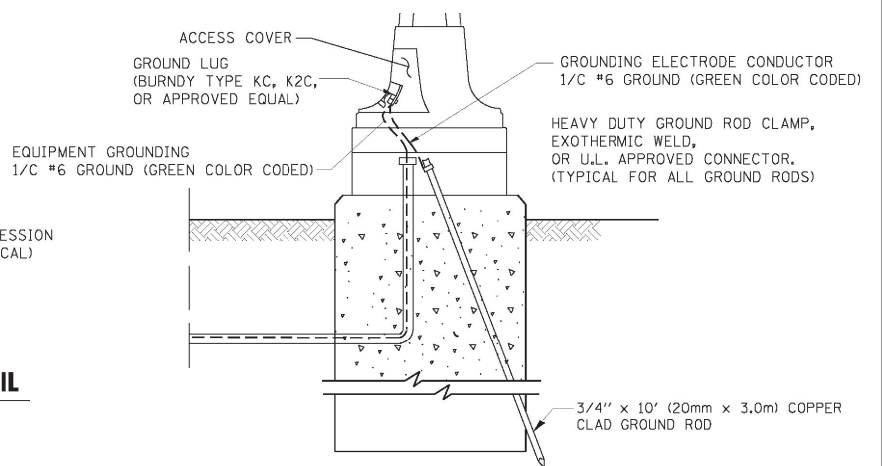
**HANDHOLE COVER & FRAME - GROUNDING DETAIL (NOT TO SCALE)**



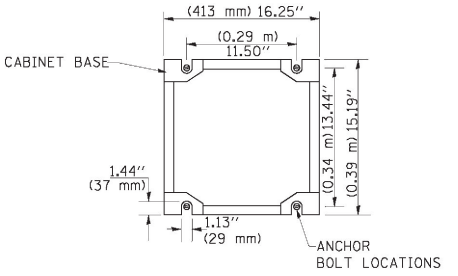
**SERVICE INSTALLATION GROUND MOUNT (NOT TO SCALE)**



**EXISTING HANDHOLE COVER & FRAME - GROUNDING DETAIL (NOT TO SCALE)**



**MAST ARM POLE / POST-GROUNDING DETAIL (NOT TO SCALE)**



**CABINET - BASE BOLT PATTERN (NOT TO SCALE)**

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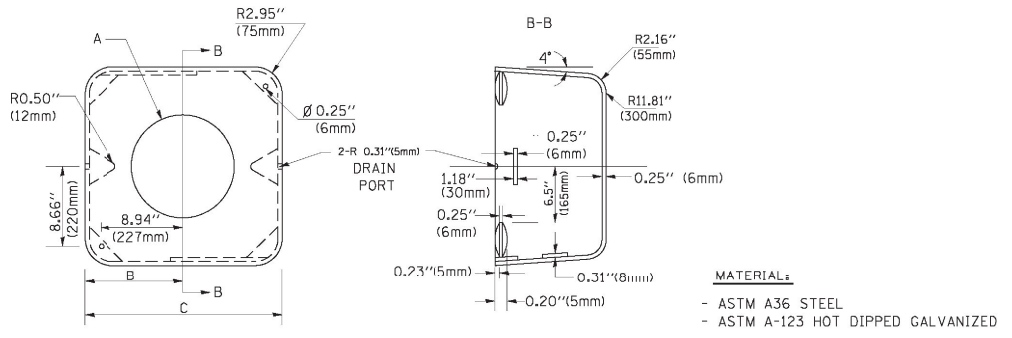
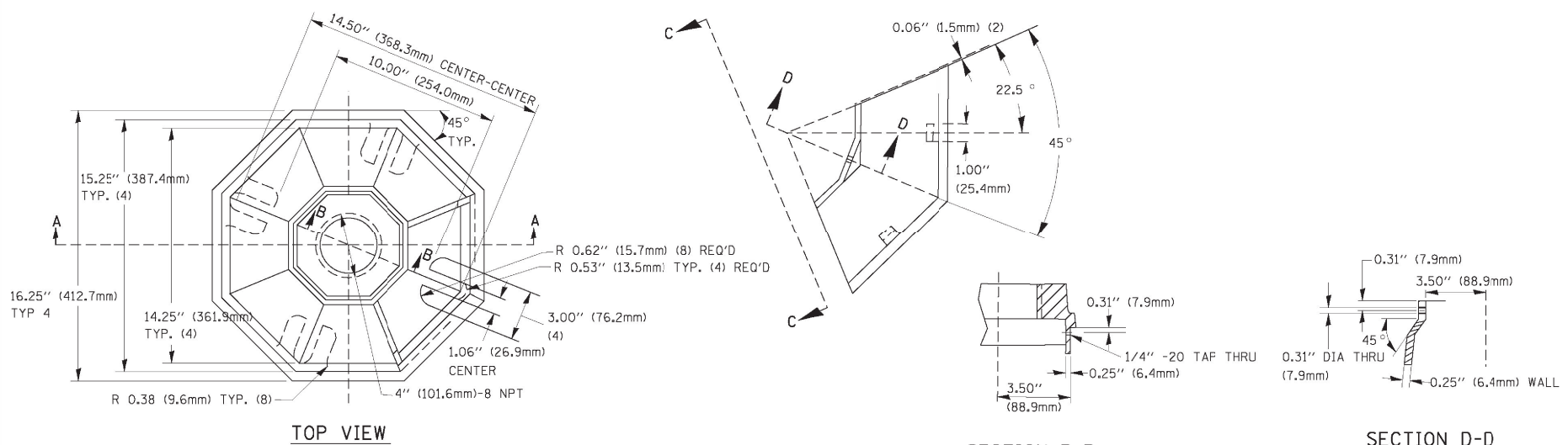
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE - STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: N.T.S. SHEET NO. 3 OF 6 SHEETS STA. TO STA.

TS-5

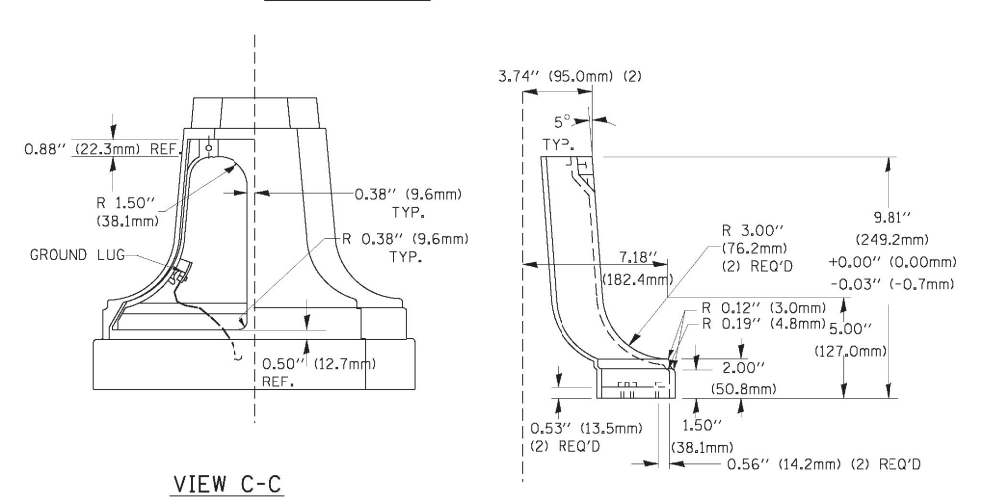
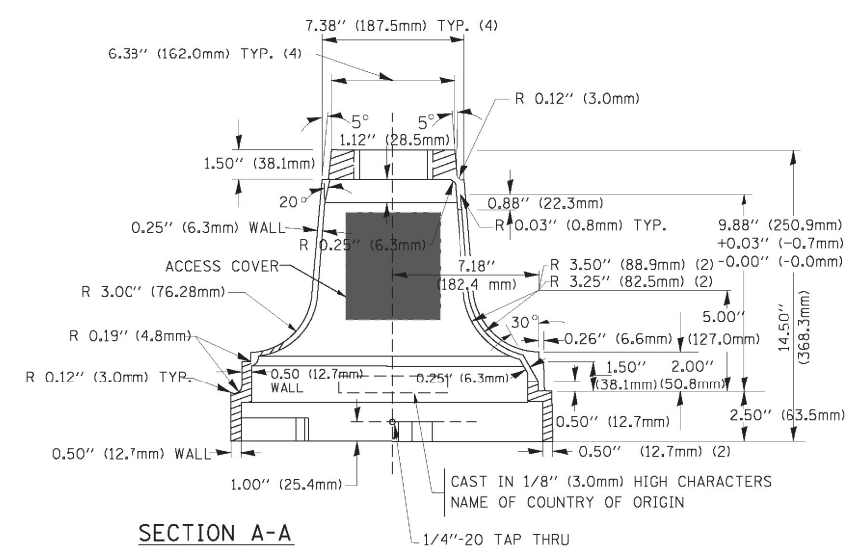
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920, .01, 1518, 2022 & 1922, 4BR)	COOK	919	353
CONTRACT NO. 60T35				
ILLINOIS FED. AID PROJECT				



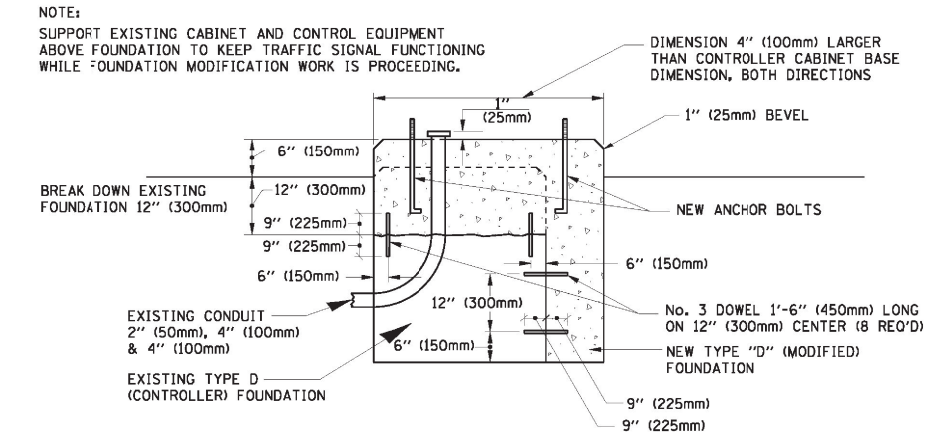
A	B	C	HEIGHT	WEIGHT
VARIES	9.5\"(241mm)	19\"(483mm)	7\"(178mm) - 12\"(300mm)	53 lbs (24kg)
VARIES	10.75\"(273mm)	21.5\"(546mm)	7\"(178mm) - 12\"(300mm)	68 lbs (31 kg)
VARIES	13.0\"(330mm)	26\"(660mm)	7\"(178mm) - 12\"(300mm)	81 lbs (37 kg)
VARIES	18.5\"(470mm)	37\"(940mm)	7\"(178mm) - 12\"(300mm)	126 lbs (57 kg)

**NOTES:**

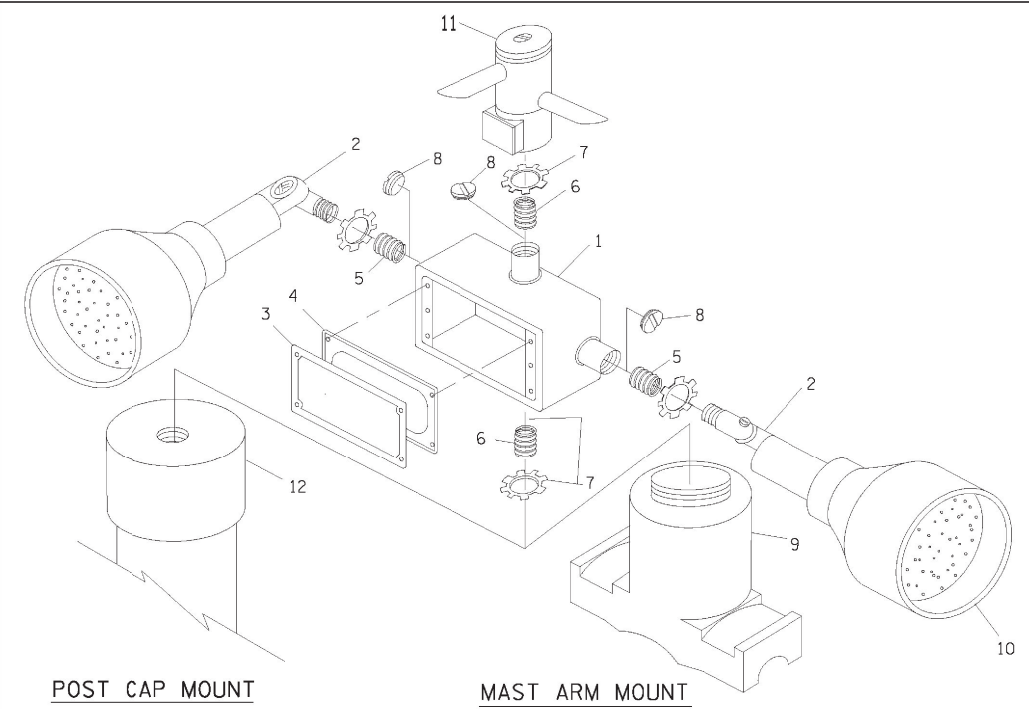
- DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
- THE SUPPLIER SHALL VERIFY THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
- THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.



TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A



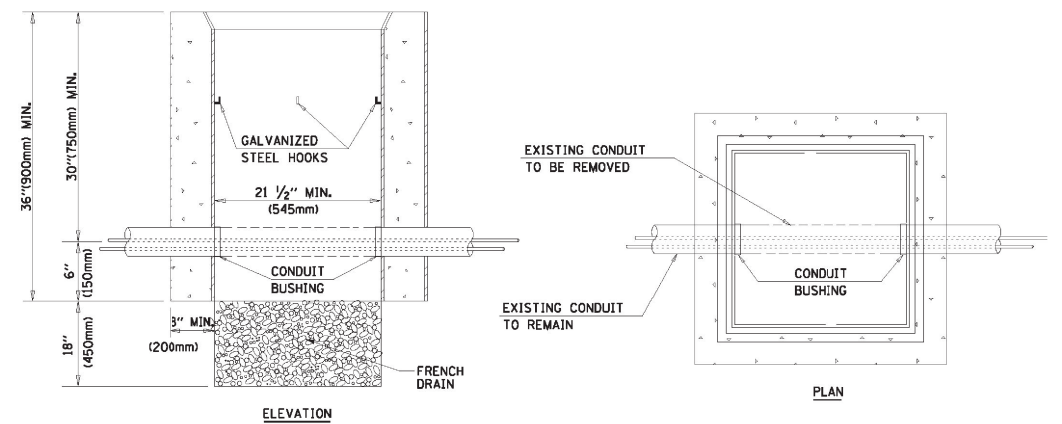
MODIFY EXISTING TYPE "D" FOUNDATION



ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV, 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4\"(19 mm) CLOSE NIPPLE
7	3/4\"(19 mm) LOCKNUT
8	3/4\"(19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

**NOTES:**

- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
- ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT  
 ITEM #2- MULBERRY CON-0-SHADE LAMP SHIELD OR EQUIVALENT  
 ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
- WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4\"(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



**NOTES:**

- HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
- REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCIDENTAL TO THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

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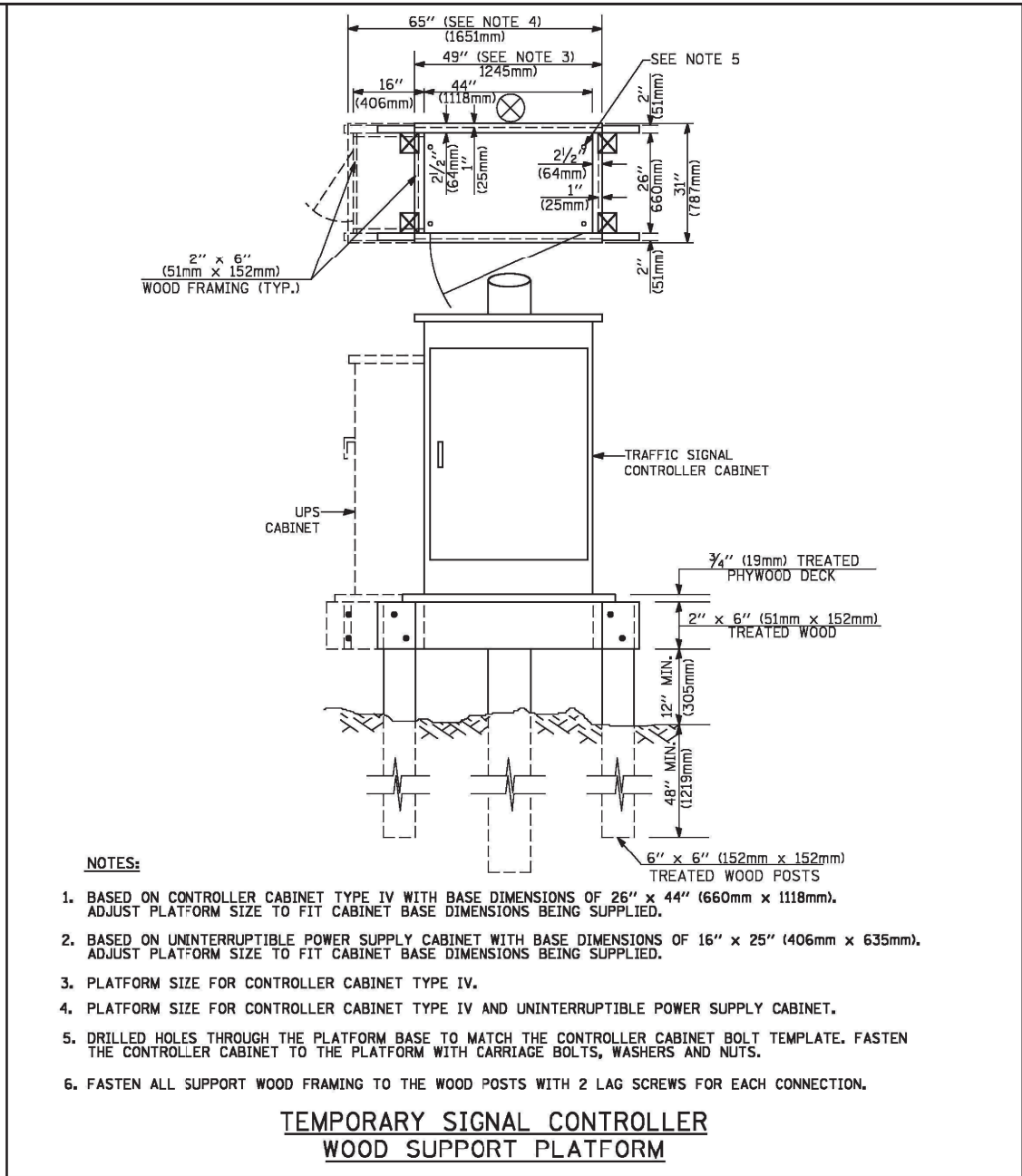
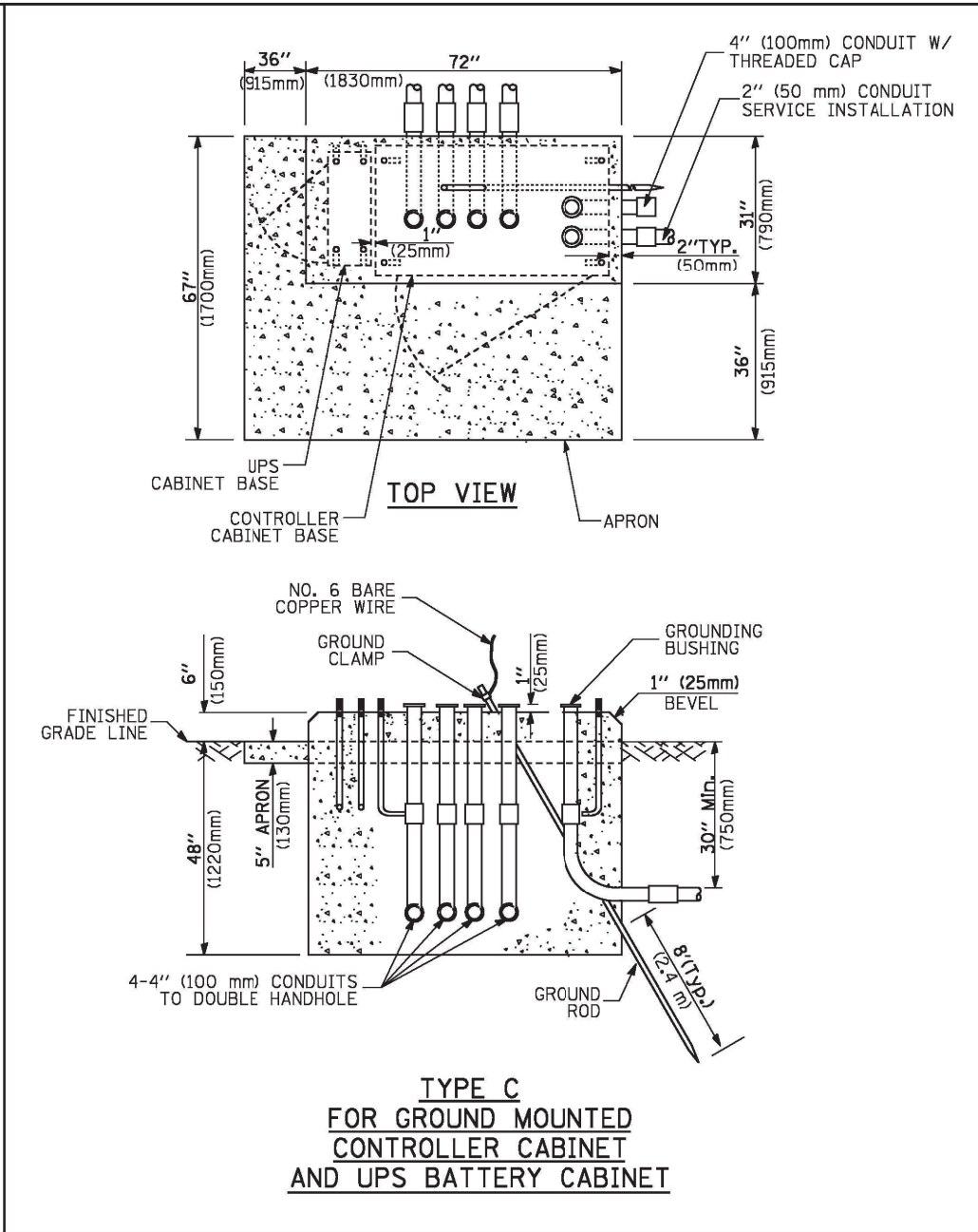
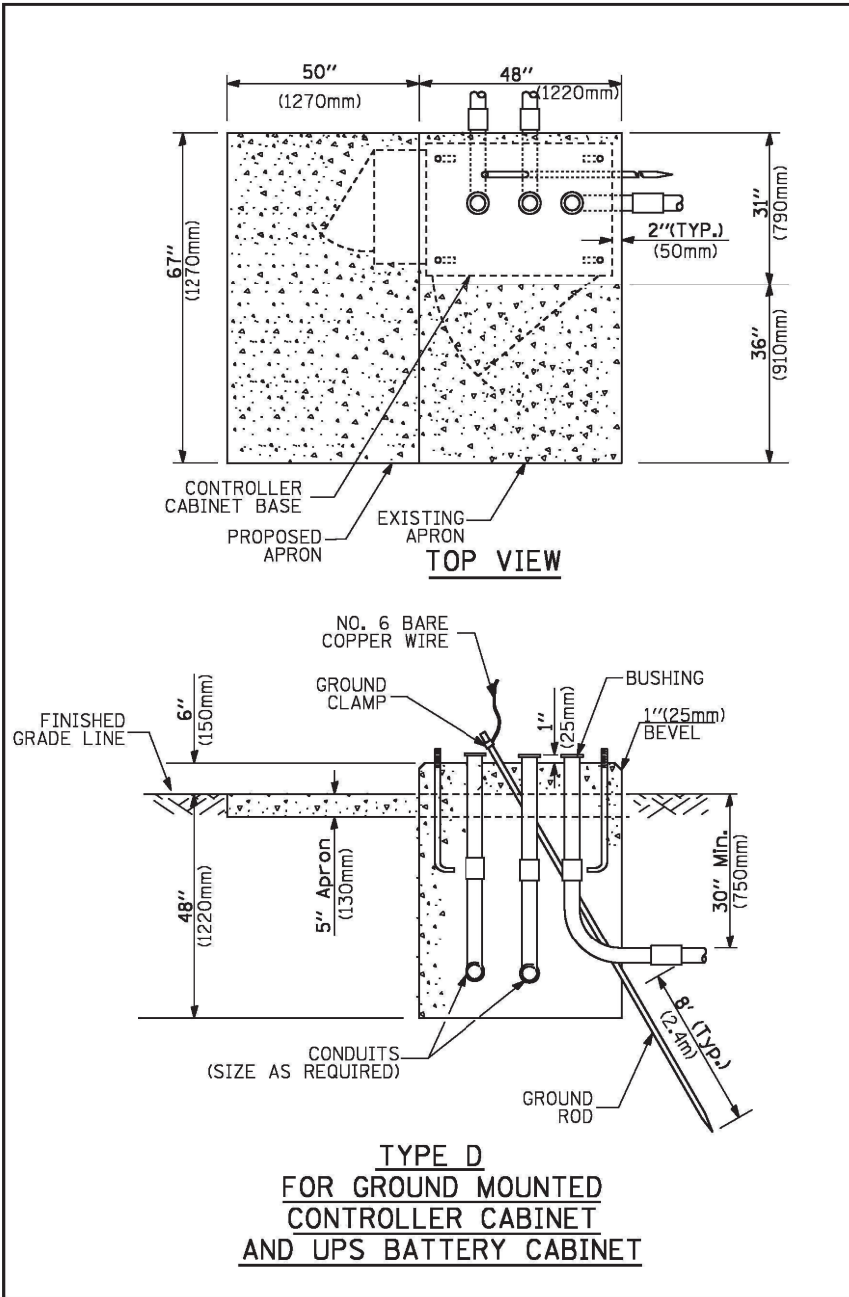
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

DISTRICT ONE - STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: N.T.S. SHEET NO. 4 OF 6 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920, .01, 1518, 2022 & 1922, 4BR)	COOK	919	354
CONTRACT NO. 60T35				
ILLINOIS FED. AID PROJECT				





- NOTES:**
1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
  2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
  3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
  4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
  5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
  6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

Mast Arm Length	① Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 55' (16.8 m) and less than 65' (19.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

- NOTES:**
1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength ( $Q_u$ ) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & structures should be contacted for a revised design if other conditions are encountered.
  2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
  3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
  4. For mast arm assemblies with dual arms refer to state standard 878001.

**CABLE SLACK**

**VERTICAL CABLE LENGTH**

**DEPTH OF FOUNDATION**

**DEPTH OF MAST ARM FOUNDATIONS, TYPE E TS-7**

# TRAFFIC SIGNAL LEGEND

ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED
CONTROLLER CABINET				EMERGENCY VEHICLE LIGHT DETECTOR				ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE			
RAILROAD CONTROL CABINET				CONFIRMATION BEACON				COAXIAL CABLE			
COMMUNICATIONS CABINET				HANDHOLE				VENDOR CABLE FOR CAMERA			
MASTER CONTROLLER				HEAVY DUTY HANDHOLE				COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED			
MASTER MASTER CONTROLLER				DOUBLE HANDHOLE				FIBER OPTIC CABLE NO. 62.5/125, MM12F			
UNINTERRUPTIBLE POWER SUPPLY				JUNCTION BOX				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F			
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT				GALVANIZED STEEL CONDUIT IN TRENCH (T) OR PUSHED (P)				FIBER OPTIC CABLE NO. 62.5/125, MM12F (NUMBER OF FIBERS & TYPE TO BE NOTED ON PLANS)			
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT				TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE				GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE			
STEEL MAST ARM ASSEMBLY AND POLE				COMMON TRENCH				CONTROLLER CABINET AND FOUNDATION TO BE REMOVED			
ALUMINUM MAST ARM ASSEMBLY AND POLE				COILABLE NONMETALLIC CONDUIT (EMPTY)				STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE				SYSTEM ITEM				ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA				INTERSECTION ITEM				STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED			
SIGNAL POST				REMOVE ITEM				SIGNAL POST AND FOUNDATION TO BE REMOVED			
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM				RELOCATE ITEM				INTERSECTION & SAMPLING (SYSTEM) DETECTOR			
GUY WIRE				ABANDON ITEM				SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD				12" (300mm) TRAFFIC SIGNAL SECTION				EXISTING INTERSECTION LOOP DETECTOR			
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)				12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE				PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD WITH BACKPLATE				SIGNAL FACE				EXISTING PREFORMED INTERSECTION LOOP DETECTOR			
SIGNAL HEAD OPTICALLY PROGRAMMED				SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD				PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
FLASHER INSTALLATION (S DENOTES SOLAR POWER)				12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL				PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
PEDESTRIAN SIGNAL HEAD				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED				PREFORMED SAMPLING (SYSTEM) DETECTOR			
PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID				RAILROAD CONTROL CABINET			
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR				PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER				RAILROAD CANTILEVER MAST ARM			
ILLUMINATED SIGN "NO LEFT TURN"				RADIO INTERCONNECT				FLASHING SIGNAL			
ILLUMINATED SIGN "NO RIGHT TURN"				RADIO REPEATER				CROSSING GATE			
DETECTOR LOOP, TYPE I				DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED				CROSSBUCK			
PREFORMED DETECTOR LOOP				GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)							
MICROWAVE VEHICLE SENSOR											
VIDEO DETECTION CAMERA											
VIDEO DETECTION ZONE											
PAN, TILT, ZOOM CAMERA											
WIRELESS DETECTOR SENSOR											
WIRELESS ACCESS POINT											

## RAILROAD SYMBOLS

	EXISTING	PROPOSED
RAILROAD CONTROL CABINET		
RAILROAD CANTILEVER MAST ARM		
FLASHING SIGNAL		
CROSSING GATE		
CROSSBUCK		

FILE NAME =	USER NAME = \$USER\$	DESIGNED - KMM	REVISED -
N:\Projects\312027\DesignCAD\Sheet Files\0160T351DOT DETAIL_6.dgn		DRAWN - CDC	REVISED -
		CHECKED - JDH	REVISED -
PLOT DATE = 10/30/2012		DATE - 10/31/2012	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE - STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: N.T.S. SHEET NO. 6 OF 6 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920, .01, 1518, 2022 & 1922, 4BIR)	COOK	919	356
CONTRACT NO. 60T35				
ILLINOIS FED. AID PROJECT				



**REMOVAL OF EXISTING TRAFFIC SIGNAL EQUIPMENT**

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE AGENCY LISTED BELOW. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR PICK UP OF ALL EQUIPMENT TO BE RETURNED TO THE LISTED AGENCY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS. AGENCY: VILLAGE OF NORTHFIELD

- 2 EACH LIGHT DETECTOR AND BEACON
- 1 EACH LIGHT DETECTOR AMPLIFIER

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 1 EACH SIGNAL HEAD, 1 - FACE, 3 - SECTION
- 9 EACH SIGNAL HEAD, 1 - FACE, 5 - SECTION
- 1 EACH SIGNAL HEAD, 2 - FACE, 1-3 SECTION, 1-5 SECTION
- 2 EACH SIGNAL HEAD, 2 - FACE, 2-5 SECTION
- 2 EACH PEDESTRIAN SIGNAL HEAD, 1 - FACE
- 1 EACH PEDESTRIAN SIGNAL HEAD, 2 - FACE
- 9 EACH TRAFFIC SIGNAL BACKPLATE
- 4 EACH STEEL MAST ARM AND POLE
- 1 EACH ALUMINUM MAST ARM AND POLE
- 3 EACH SIGNAL POST
- 1 EACH SERVICE INSTALLATION
- 1 EACH CONTROLLER AND CABINET COMPLETE
- 3 EACH PEDESTRIAN PUSH-BUTTON

TEMPORARY WIRELESS INTERCONNECT TO WILLOW/WESTLEIGH, WILLOW/THREE LAKES, IL 43/THREE LAKES AND IL 43/CHRISTIAN HERITAGE ACADEMY

AUTOMATIC ENFORCEMENT CAMERAS TO BE REMOVED BY OTHERS(2)

TEMPORARY MASTER CONTROLLER

AUTOMATIC ENFORCEMENT EQUIPMENT TO BE REMOVED BY OTHERS (1)

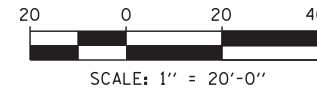
EXISTING GROUND MOUNTED ELECTRICAL SERVICE

\* THE COST OF THE 2" CONDUIT AND CONNECTION TO THE EXISTING GROUND MOUNTED ELECTRIC SERVICE SHALL BE INCLUDED IN THE COST FOR THE TEMPORARY TRAFFIC SIGNAL INSTALLATION.

**LEFT ON GREEN ARROW ONLY**

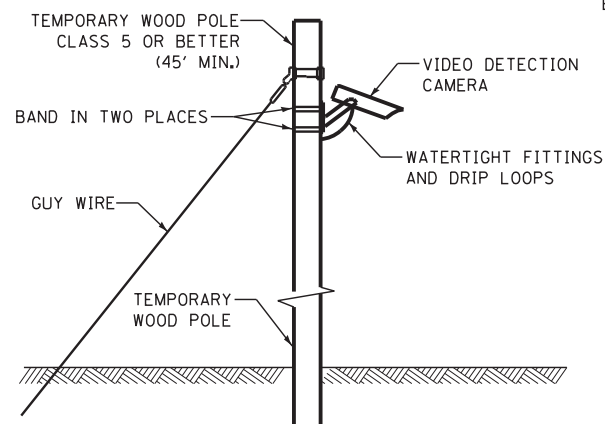
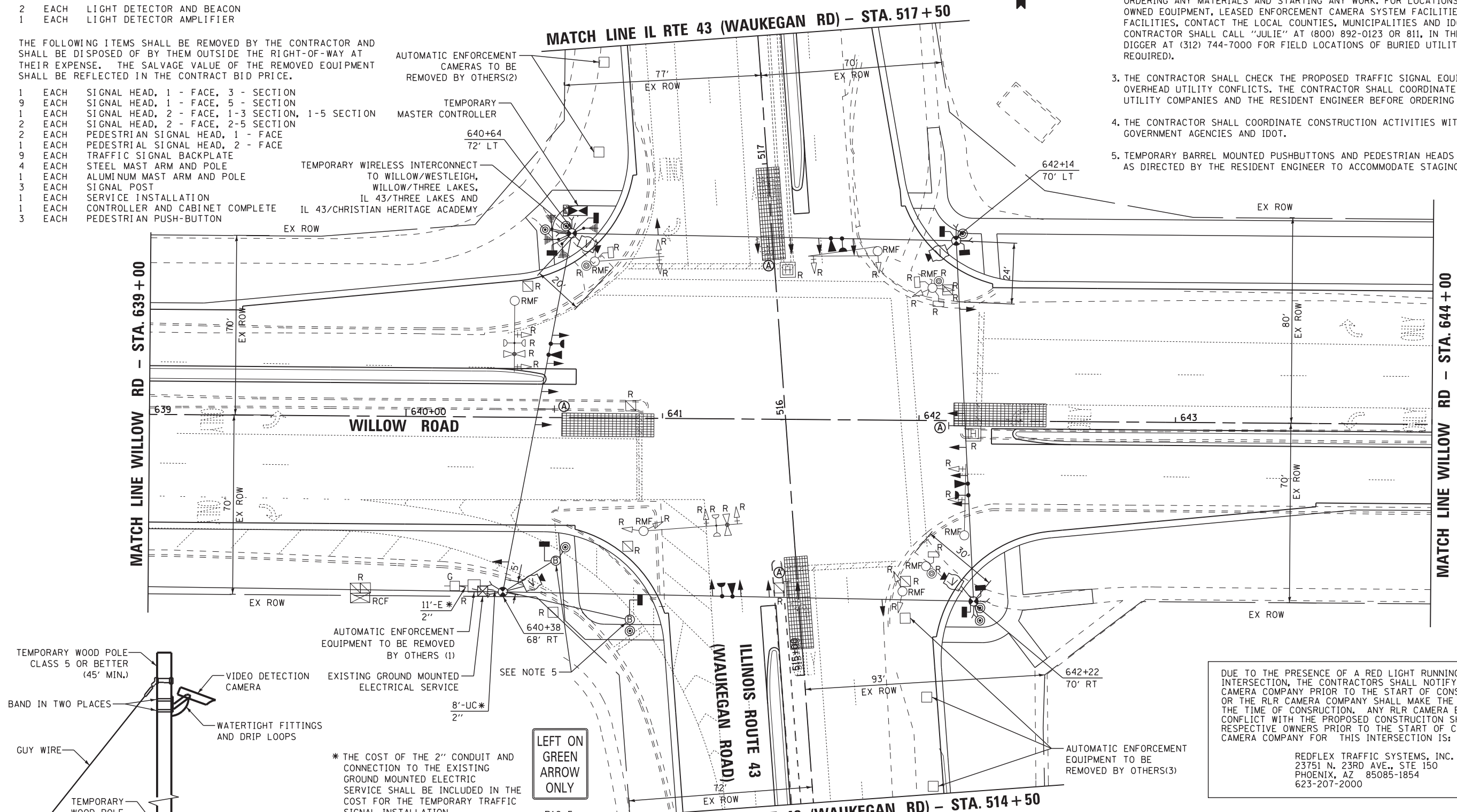
R10-5  
 4 REQUIRED  
 30"X36"  
 SIGN PANEL TYPE 1

INCLUDED IN THE COST OF TEMPORARY TRAFFIC SIGNAL INSTALLATION



**GENERAL NOTES**

1. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARM LENGTHS.
2. THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES. THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811, IN THE CITY OF CHICAGO CONTACT DIGGER AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION REQUIRED).
3. THE CONTRACTOR SHALL CHECK THE PROPOSED TRAFFIC SIGNAL EQUIPMENT LOCATIONS FOR OVERHEAD UTILITY CONFLICTS. THE CONTRACTOR SHALL COORDINATE ANY CONFLICTS WITH THE UTILITY COMPANIES AND THE RESIDENT ENGINEER BEFORE ORDERING MATERIALS.
4. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, LOCAL GOVERNMENT AGENCIES AND IDOT.
5. TEMPORARY BARREL MOUNTED PUSHBUTTONS AND PEDESTRIAN HEADS ON POSTS TO BE INSTALLED AS DIRECTED BY THE RESIDENT ENGINEER TO ACCOMMODATE STAGING OF SIDEWALK CONSTRUCTION.



**TEMPORARY VIDEO DETECTION MOUNTING DETAIL (NOT TO SCALE)**

DUE TO THE PRESENCE OF A RED LIGHT RUNNING (RLR) CAMERA AT THIS INTERSECTION, THE CONTRACTORS SHALL NOTIFY THE VILLAGE AND RLR CAMERA COMPANY PRIOR TO THE START OF CONSTRUCTION. THE VILLAGE OR THE RLR CAMERA COMPANY SHALL MAKE THE CAMERA INOPERATIVE FOR THE TIME OF CONSTRUCTION. ANY RLR CAMERA EQUIPMENT THAT IS IN CONFLICT WITH THE PROPOSED CONSTRUCTION SHALL BE REMOVED BY ITS RESPECTIVE OWNERS PRIOR TO THE START OF CONSTRUCTION. THE RLR CAMERA COMPANY FOR THIS INTERSECTION IS:

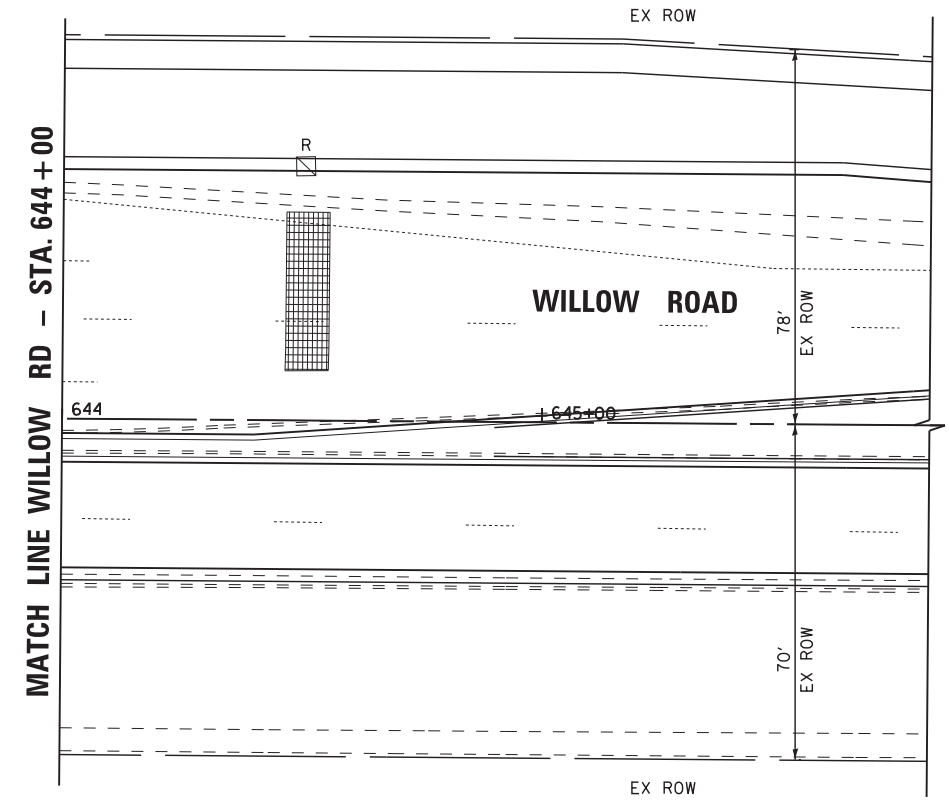
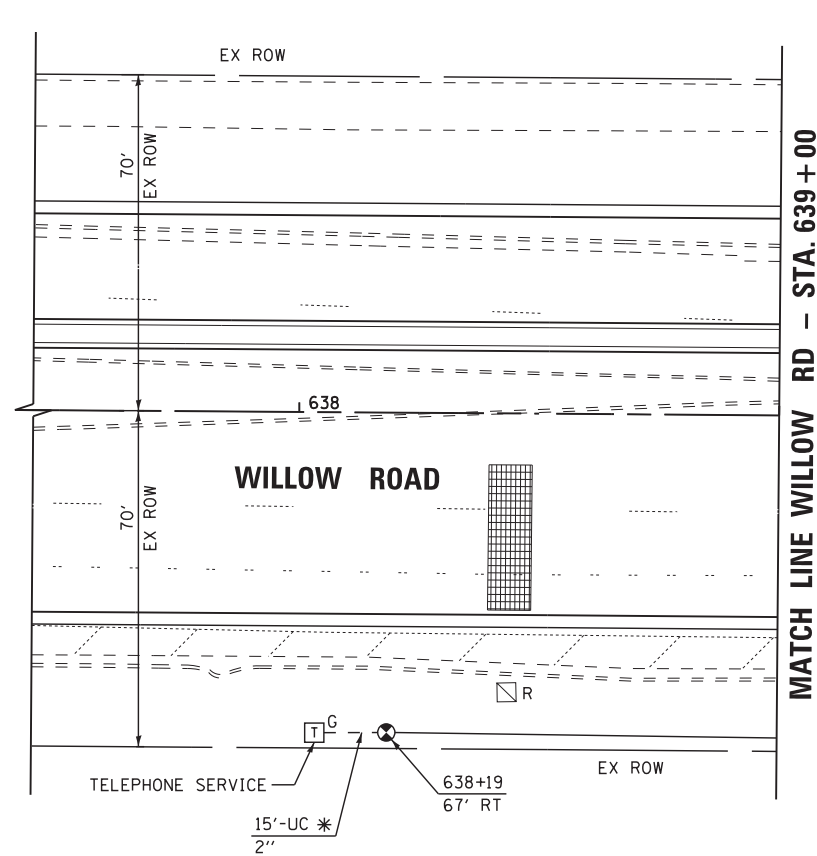
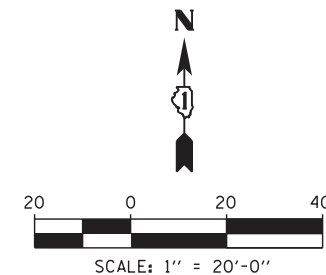
REFLEX TRAFFIC SYSTEMS, INC.  
 23751 N. 23RD AVE., STE 150  
 PHOENIX, AZ 85085-1854  
 623-207-2000

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

THE CONTRACTOR SHALL INSTALL A WOOD SUPPORT PLATFORM AS SHOWN ON THE DISTRICT 1 SIGNAL DETAIL SHEETS FOR BOTH THE CONTROLLER CABINET AND UPS.

THE TEMPORARY TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

FILE NAME - N:\Projects\312027\Design\CAD\Sheet Files\0160T35\wWLtemp43-1.dgn	USER NAME - *SUSER*	DESIGNED - KMM	REVISED -	<b>STATE OF ILLINOIS                  DEPARTMENT OF TRANSPORTATION</b>		<b>TEMPORARY TRAFFIC SIGNAL INSTALLATION &amp; REMOVAL PLAN                  MAINTENANCE OF TRAFFIC STAGE 1                  WILLOW ROAD AT ILLINOIS ROUTE 43 (WAUKEGAN ROAD)</b>		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE - *SCALE*	CHECKED - JDH	REVISED -	305					(1920, .01, 1518, 2022 & 1922, 4BR)	COOK	919	357	
PLOT DATE - 10/30/2012	DATE - 10/31/2012	REVISED -	CONTRACT NO. 60T35									
								ILLINOIS FED. AID PROJECT				

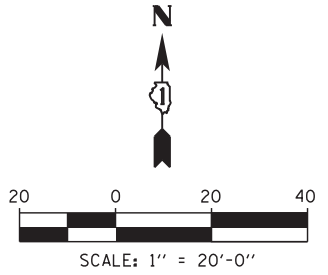
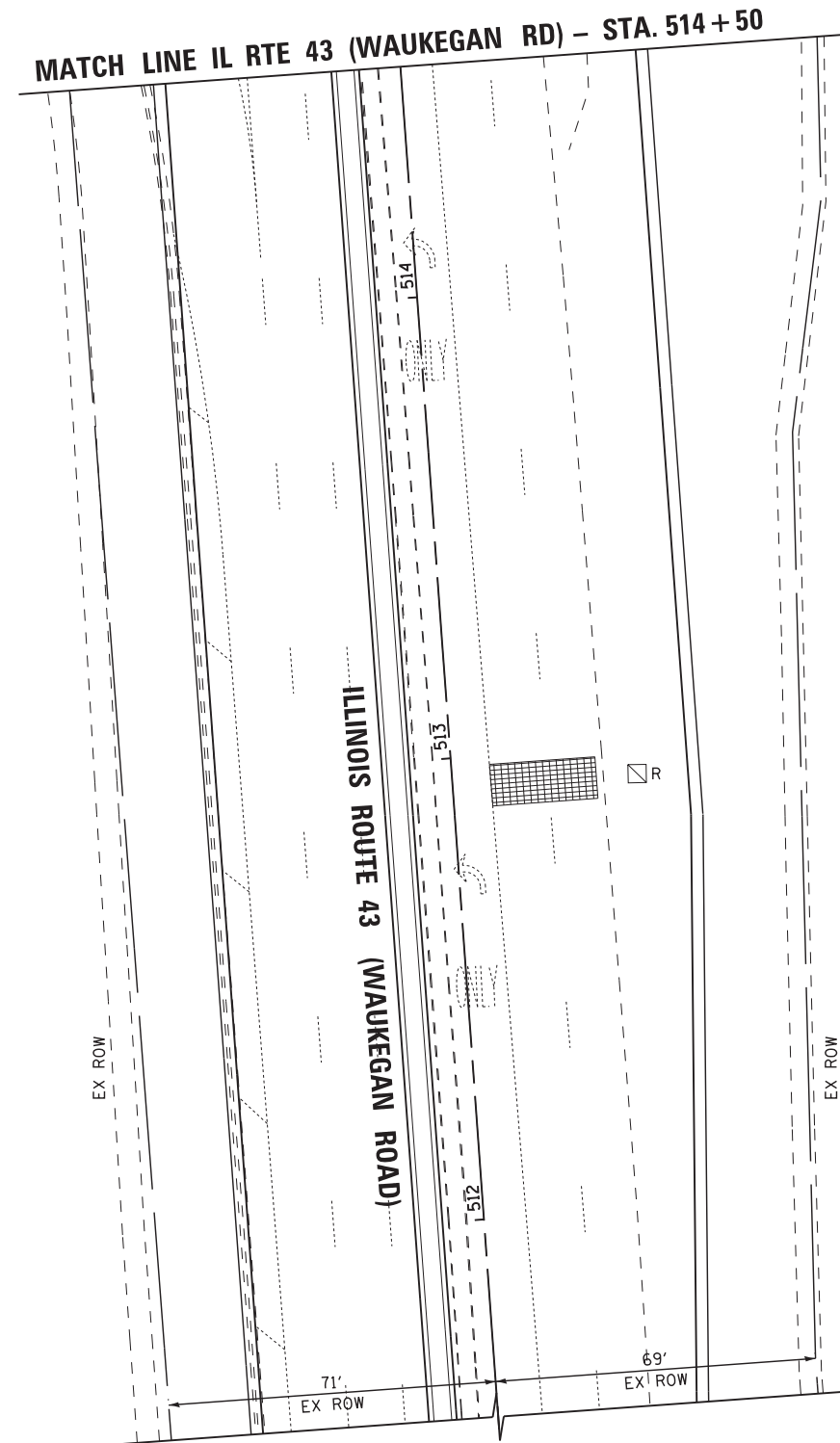
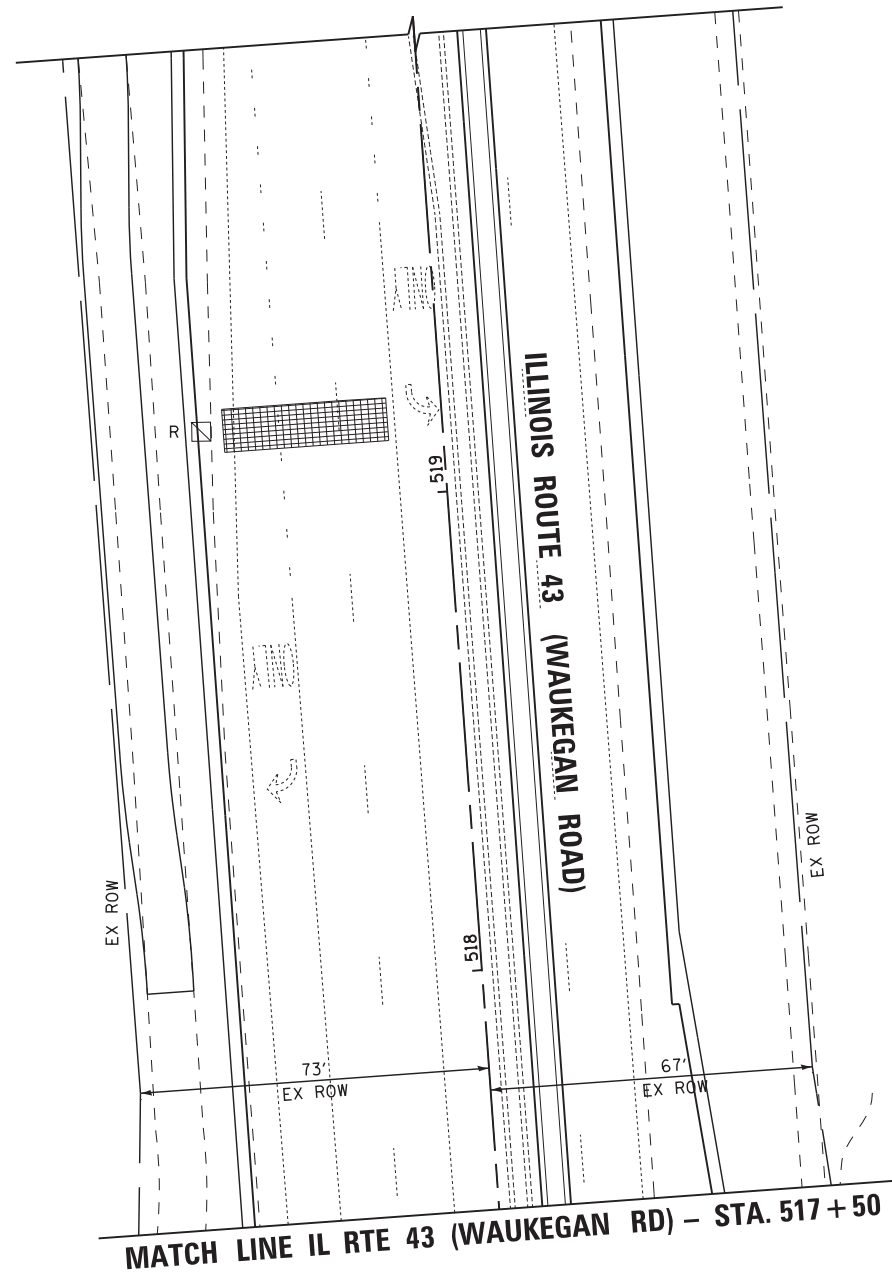


\* THE COST OF THE 2" CONDUIT SHALL BE INCLUDED IN THE COST FOR THE TEMPORARY TRAFFIC SIGNAL INSTALLATION.

THE TEMPORARY TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

**RESTORATION OF WORK AREA.** RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

FILE NAME =	USER NAME = \$USER\$	DESIGNED - KMM	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TEMPORARY TRAFFIC SIGNAL INSTALLATION &amp; REMOVAL PLAN MAINTENANCE OF TRAFFIC STAGE 1 WILLOW ROAD AT ILLINOIS ROUTE 43 (WAUKEGAN ROAD)</b>	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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PLOT DATE = 10/30/2012	DATE - 10/31/2012	CHECKED - JDH	REVISED -			CONTRACT NO. 60T35					
						SCALE: 1"=20'	SHEET NO. 2 OF 3 SHEETS	STA.	TO STA.		ILLINOIS FED. AID PROJECT



THE TEMPORARY TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

RESTORATION OF WORK AREA, RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

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	PLOT DATE = 10/30/2012	DATE - 10/31/2012	REVISED -

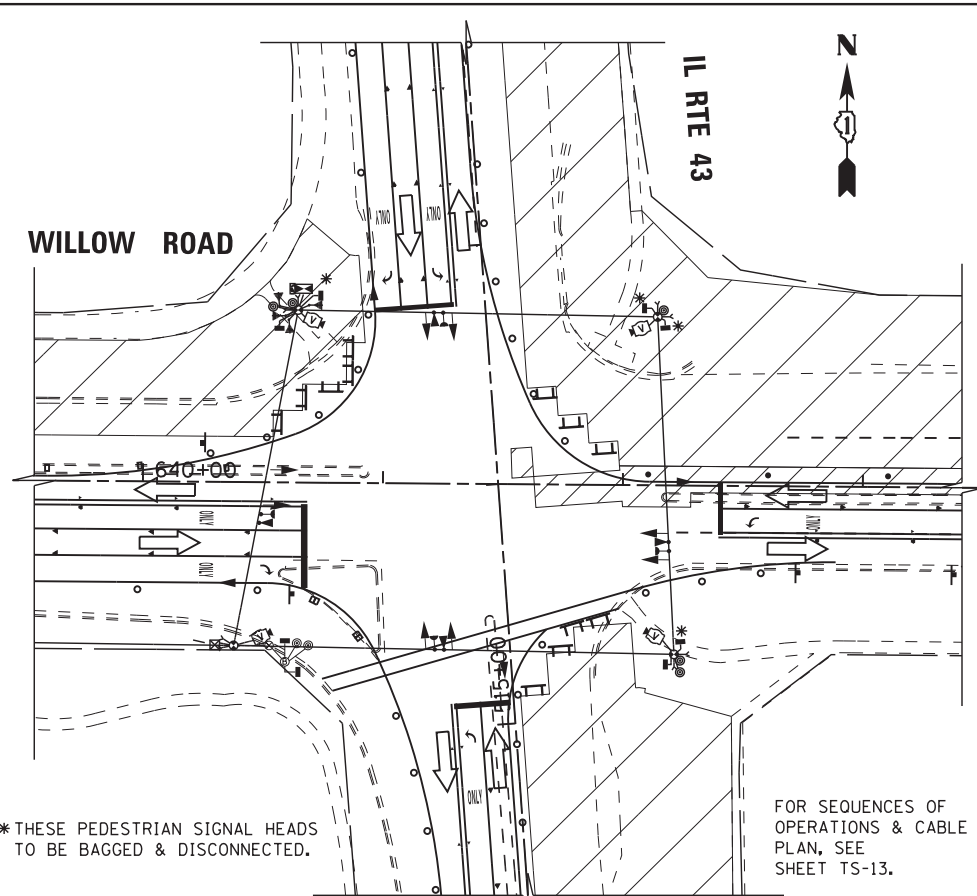
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION & REMOVAL PLAN  
 MAINTENANCE OF TRAFFIC STAGE 1  
 WILLOW ROAD AT ILLINOIS ROUTE 43 (WAUKEGAN ROAD)**

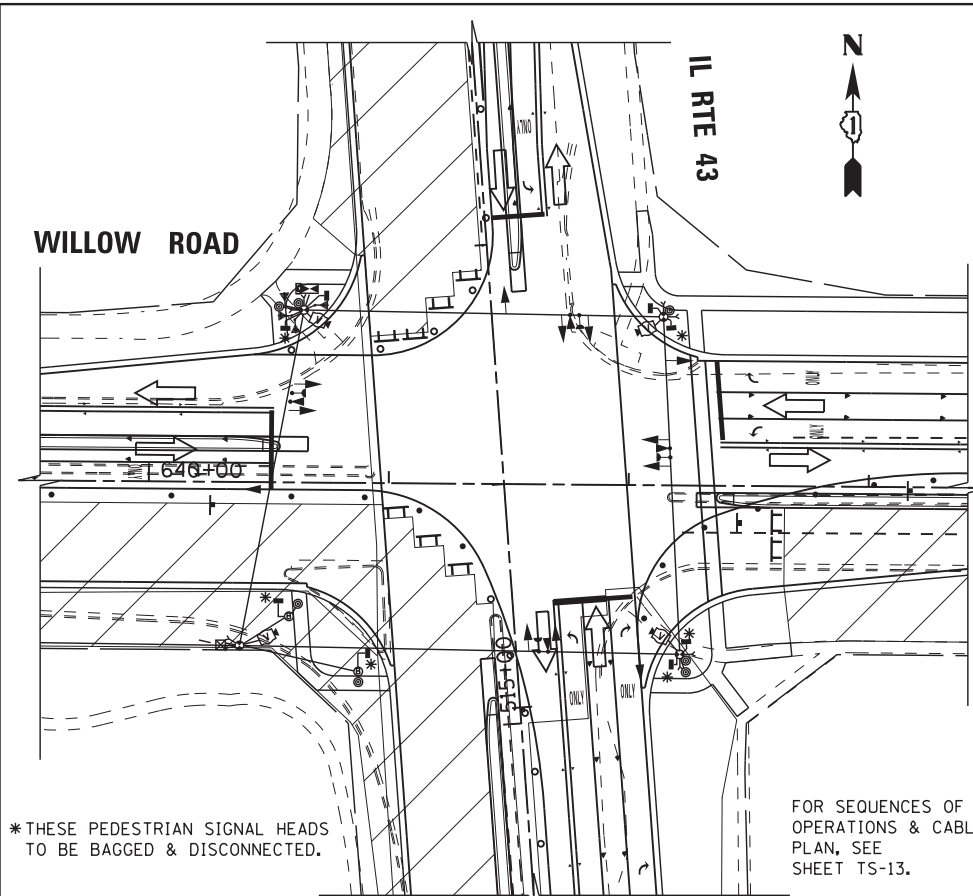
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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ILLINOIS FED. AID PROJECT			CONTRACT NO. 60T35	

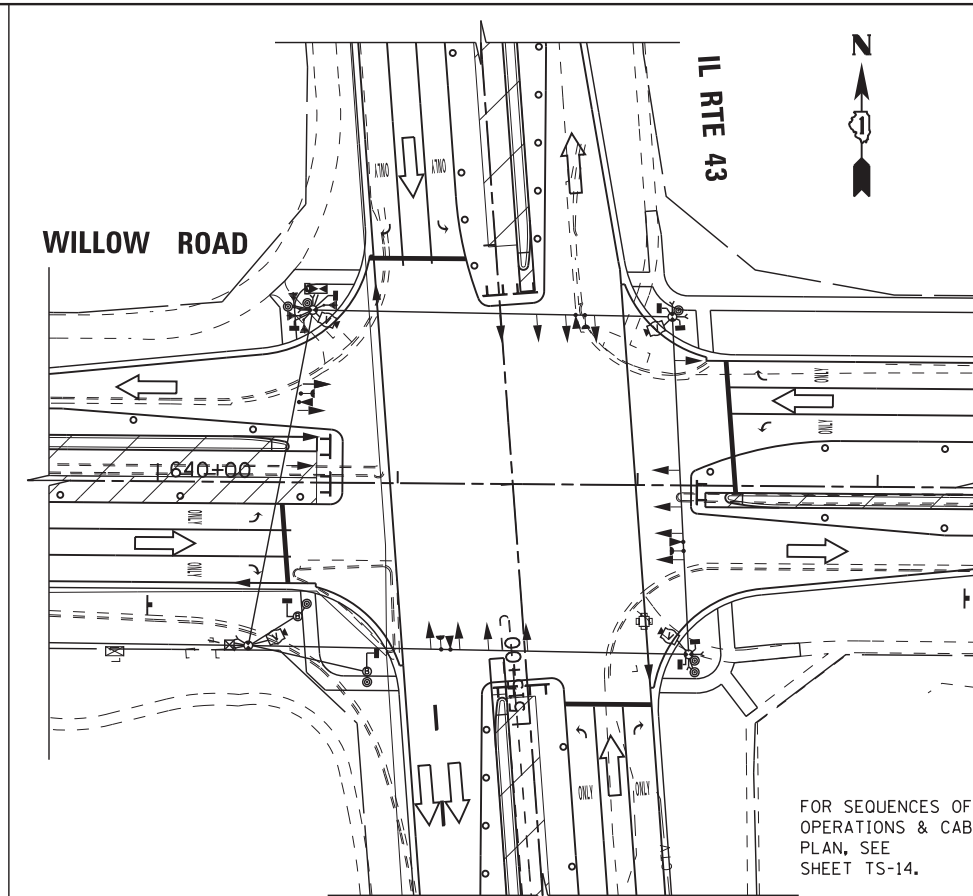
TS-11



**TEMPORARY TRAFFIC SIGNAL STAGE 1**



**TEMPORARY TRAFFIC SIGNAL STAGE 2**



**TEMPORARY TRAFFIC SIGNAL STAGE 3**

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	PLOT DATE = 10/30/2012	DATE - 10/31/2012	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TEMPORARY TRAFFIC SIGNAL INSTALLATION  
 M.O.T. STAGING PLAN  
 WILLOW ROAD AT IL ROUTE 43

SCALE: 1"=40' SHEET NO. 1 OF X SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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				CONTRACT NO. 60T35
ILLINOIS FED. AID PROJECT				

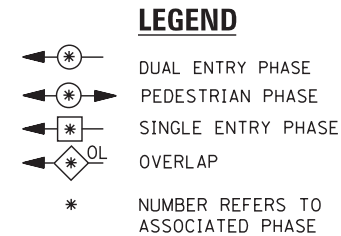
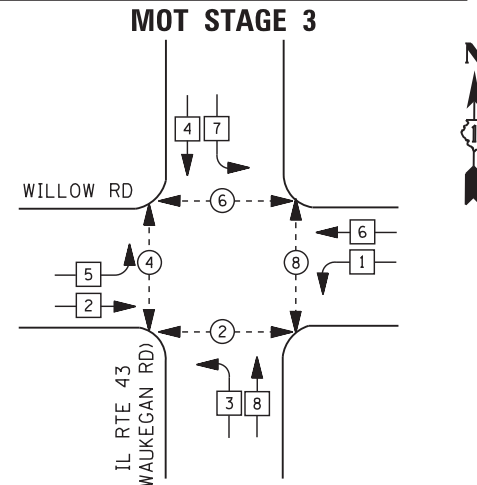




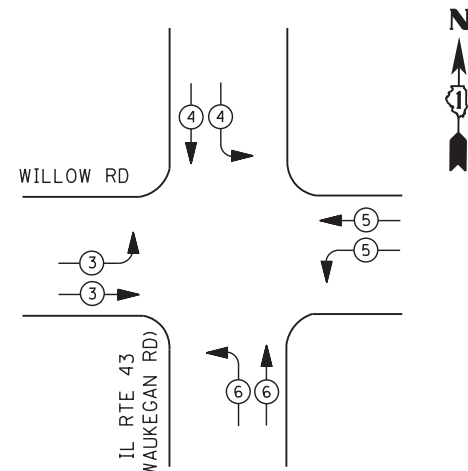
**NOTES FOR TEMPORARY TRAFFIC SIGNALS**

- 1) ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
- 2) ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1. INSTALLED IN A NEMA TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
- 3) ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE LED AND 12" (300mm) DIAMETER. HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. PEDESTRIAN SIGNALS SHALL INCLUDE SOLID INTERNATIONAL SYMBOLS. PEDESTRIAN SIGNALS WITH COUNTDOWN TIMERS SHALL BE USED WHEN THE EXISTING INSTALLATION UTILIZES COUNTDOWN TYPE OR AS DIRECTED BY THE ENGINEER. COUNTDOWN TYPE PEDESTRIAN SIGNALS ARE NOT TO BE INSTALLED AT RAILROAD INTERSECTIONS. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
- 4) ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES. RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
- 5) ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
- 6) THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL, AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
- 7) UNINTERRUPTIBLE POWER SUPPLY (UPS) SYSTEM SHALL BE INSTALLED AND MADE OPERATIONAL AT TEMPORARY TRAFFIC SIGNAL INSTALLATIONS WHERE UPS IS INSTALLED AT THE EXISTING TRAFFIC SIGNAL, TEMPORARY TRAFFIC SIGNALS AT RAILROAD INTERSECTIONS, AND TEMPORARY TRAFFIC SIGNALS AT INTERSECTIONS WITH FIRE STATION ACTUATED EMERGENCY VEHICLE PRE-EMPTION, OR WHEN INDICATED ON THE PLANS.
- 8) TRAFFIC SIGNAL MANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
- 9) DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER.
- 10) WHEN PAN, TILT, ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.

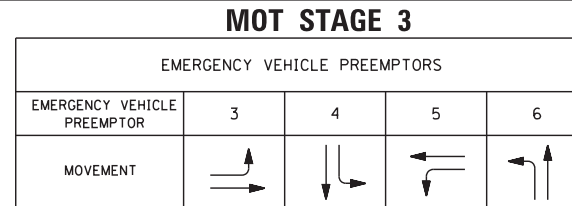
**TEMPORARY CONTROLLER SEQUENCE**



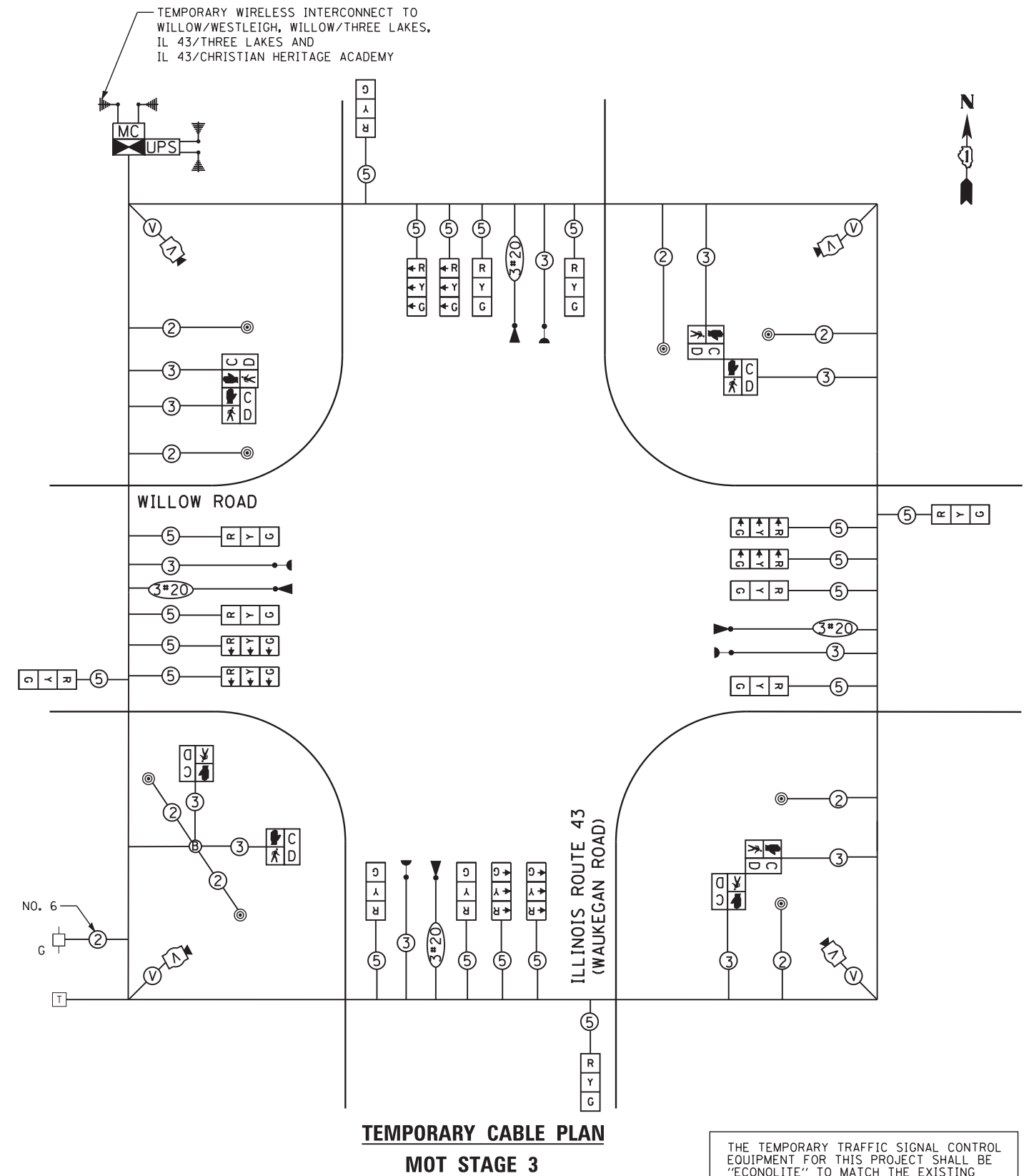
**TEMPORARY PHASE DESIGNATION DIAGRAM**



**TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE**



RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



THE TEMPORARY TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

I. D. O. T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					
TYPE	NO. LAMPS	WATTAGE		% OPERATIONS	TOTAL WATTAGE
		INCAND.	LED		
SIGNAL (RED)	20	150	17	0.50	128
(YELLOW)	20		25	0.25	94
(GREEN)	20		15	0.25	56
ARROW			12	0.10	
PED. SIGNAL	8		25	1.00	200
CONTROLLER	8		100	1.00	200
ILLUM. SIGN			25	0.05	--
VIDEO SYSTEM	1		-	1.00	150
TOTAL =					862

ENERGY COSTS- BILLED TO: IDOT - DISTRICT 1  
 201 W. CENTER CT.  
 SCHAUMBURG, IL 60196

ENERGY SUPPLY - CONTACT COMED  
 PHONE 630-723-2128

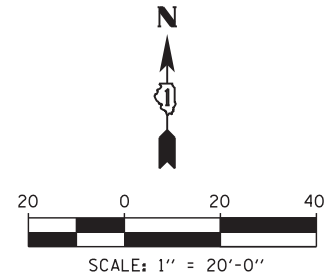
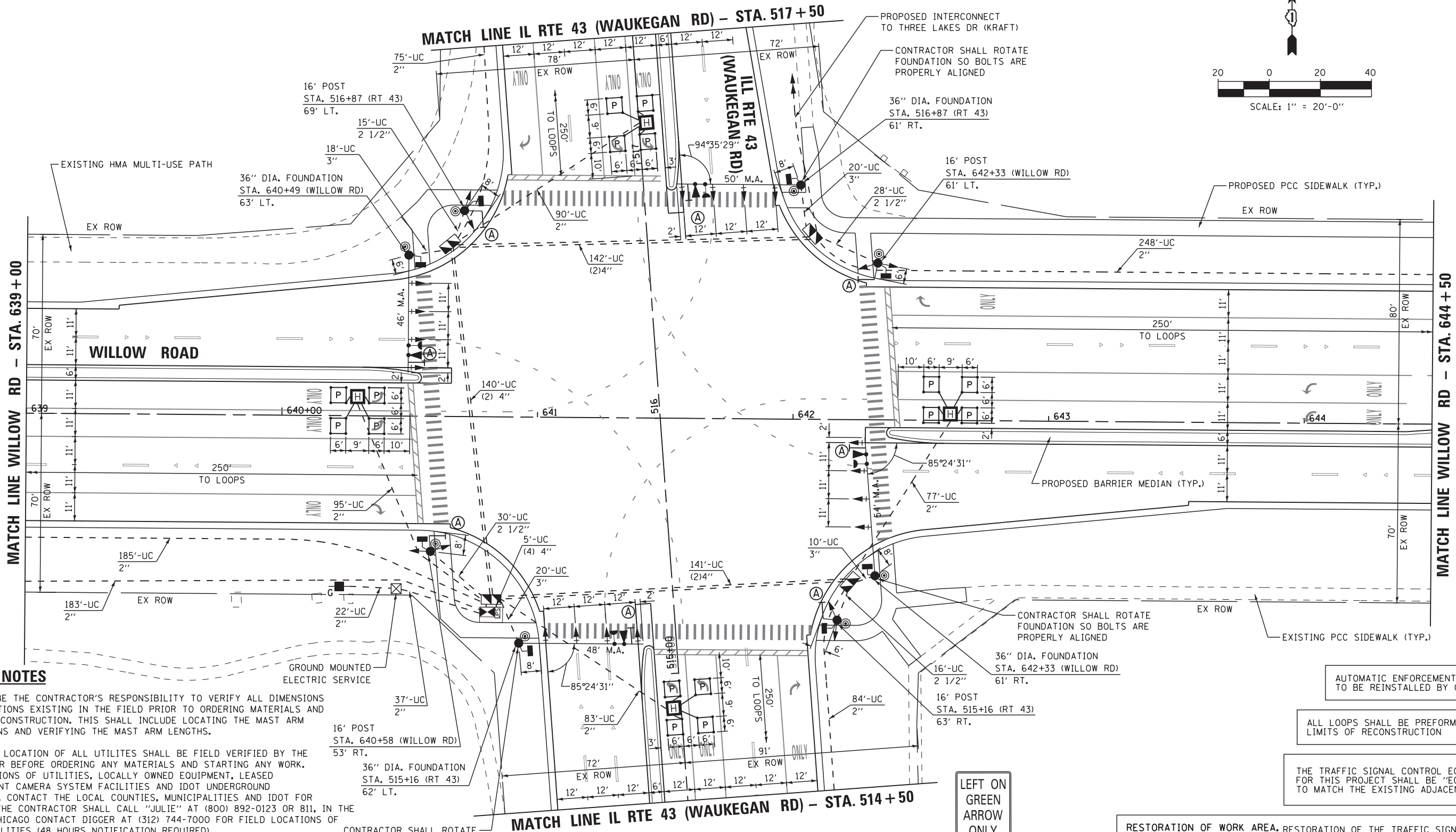
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	PLOT DATE = 10/30/2012	DATE -	10/31/2012	REVISED -	

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

STAGE 3 - TEMPORARY CABLE PLAN, TEMPORARY PHASE DESIGNATION DIAGRAM & TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE  
 WILLOW ROAD AT ILLINOIS ROUTE 43 (WAUKEGAN ROAD)

SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920, .01, 1518, 2022 & 1922, 4BR)	COOK	919	362
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60T35	



**GENERAL NOTES**

- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARM LENGTHS.
- THE EXACT LOCATION OF ALL UTILITES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES. THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811. IN THE CITY OF CHICAGO CONTACT DIGGER AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION REQUIRED).
- THE CONTRACTOR SHALL CHECK THE PROPOSED TRAFFIC SIGNAL EQUIPMENT LOCATIONS FOR OVERHEAD UTILITY CONFLICTS. THE CONTRACTOR SHALL COORDINATE ANY CONFLICTS WITH THE UTILITY COMPANIES AND THE RESIDENT ENGINEER BEFORE ORDERING MATERIALS.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, LOCAL GOVERNMENT AGENCIES AND IDOT.

AUTOMATIC ENFORCEMENT CAMERAS TO BE REINSTALLED BY OTHERS

ALL LOOPS SHALL BE PERFORMED WITHIN LIMITS OF RECONSTRUCTION

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

**RESTORATION OF WORK AREA.** RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

**LEFT ON GREEN ARROW ONLY**  
 R10-5  
 8 REQUIRED  
 30"x36"  
 SIGN PANEL TYPE 1  
 (A)

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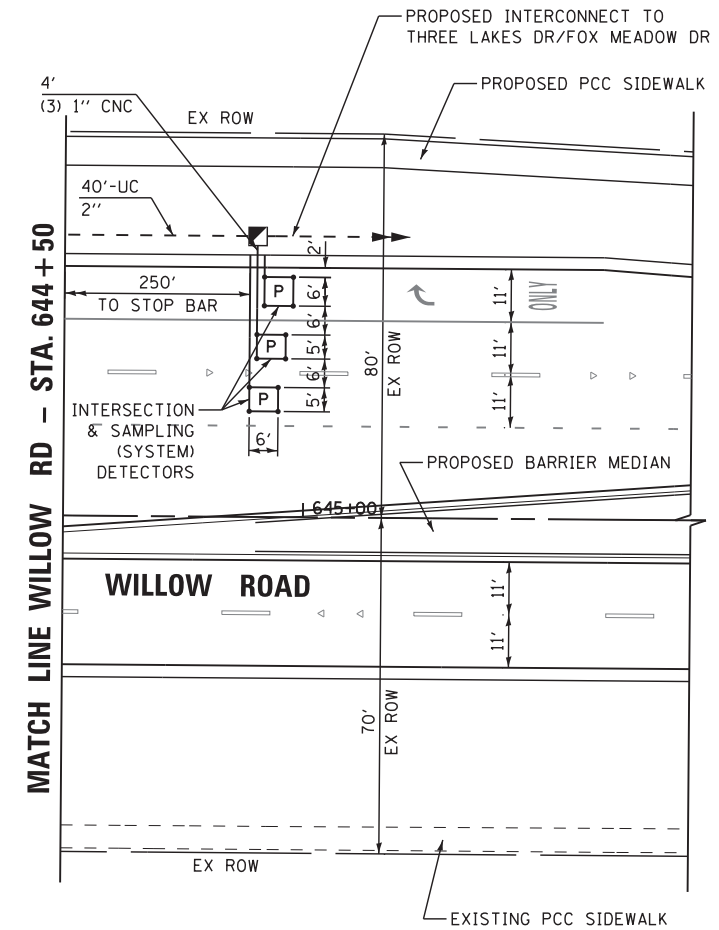
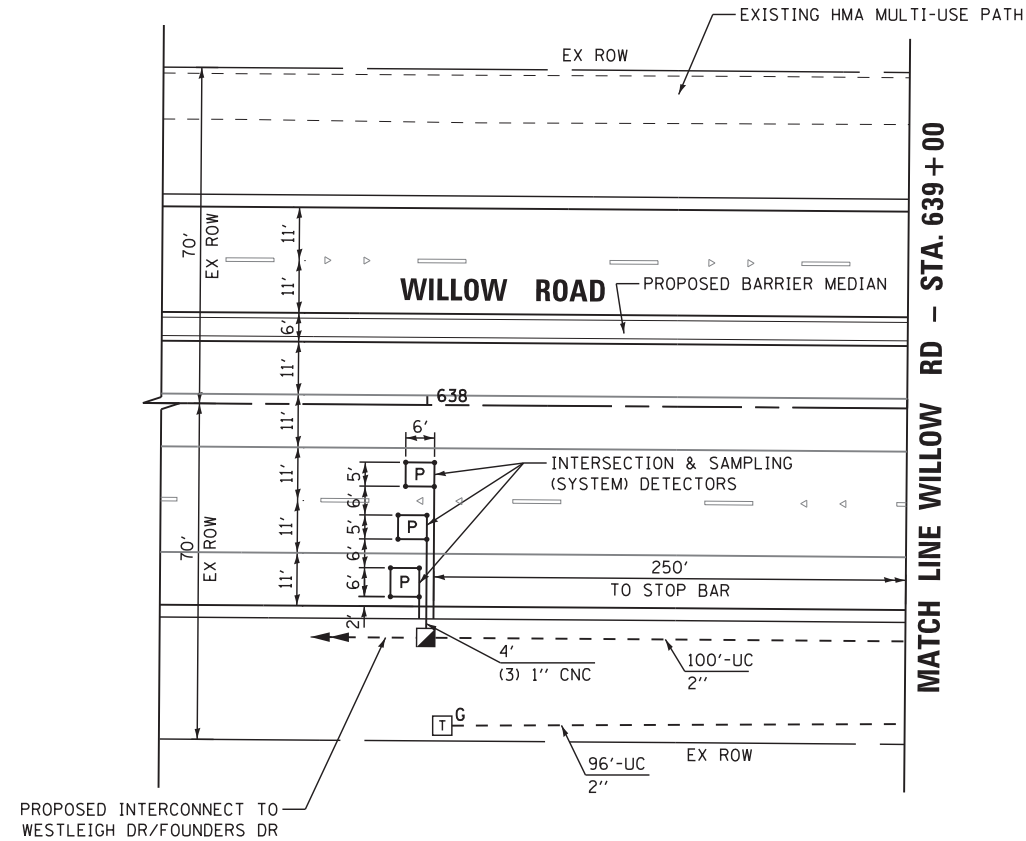
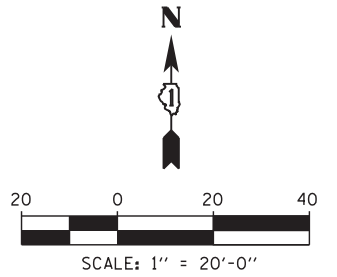
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL MODERNIZATION PLAN  
 WILLOW ROAD AT ILLINOIS ROUTE 43 (WAUKEGAN ROAD)  
 SCALE: SHEET NO. 1 OF 3 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920, .01, 1518, 2022 & 1922, 4BR)	COOK	919	363
				CONTRACT NO. 60T35
ILLINOIS FED. AID PROJECT				

TS-15





ALL LOOPS SHALL BE PERFORMED WITHIN LIMITS OF RECONSTRUCTION

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

RESTORATION OF WORK AREA, RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

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	PLOT DATE = 10/30/2012	DATE - 10/31/2012	REVISED -

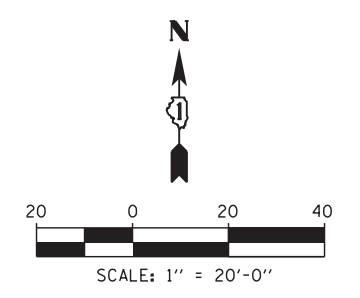
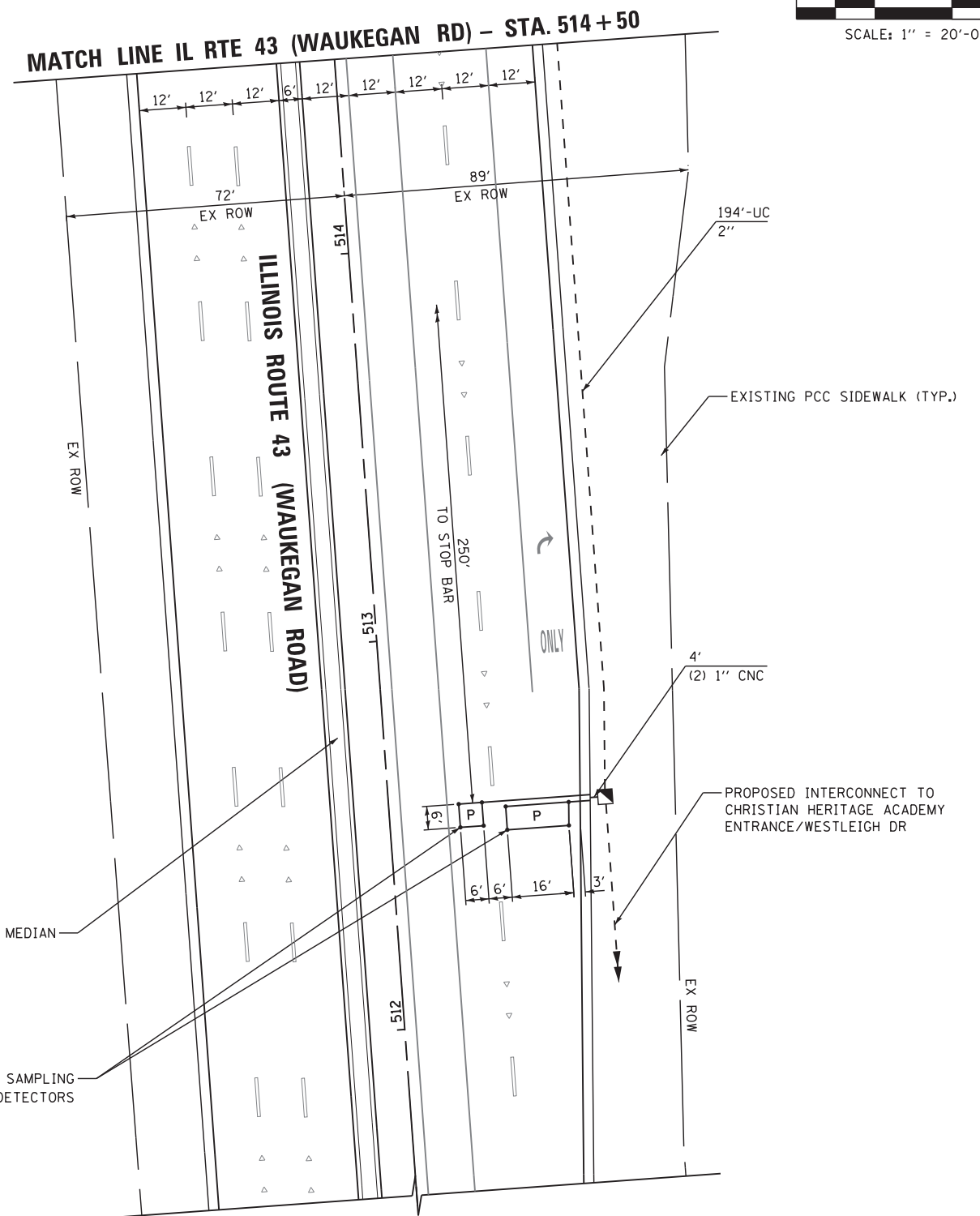
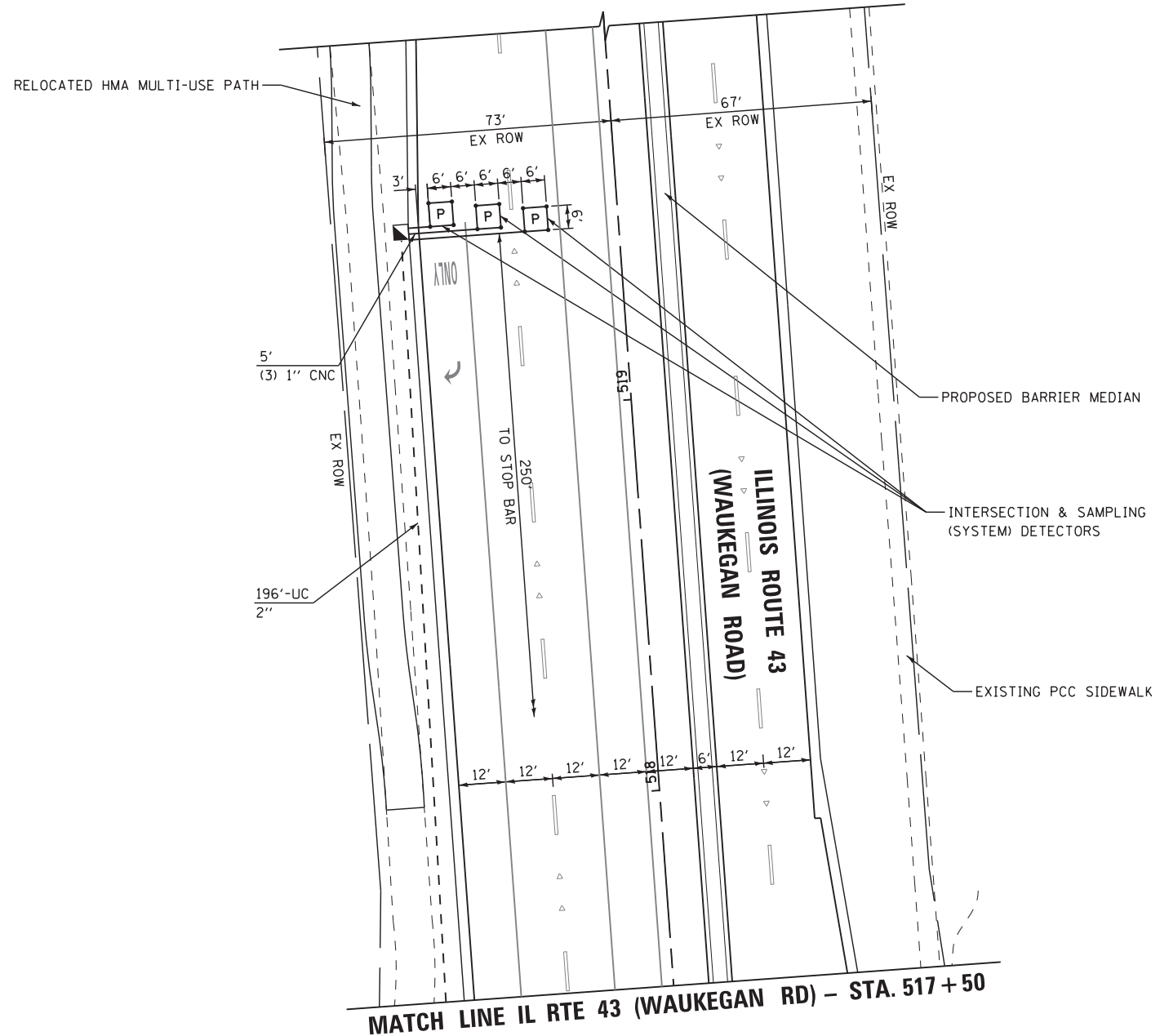
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL MODERNIZATION PLAN  
 WILLOW ROAD AT ILLINOIS ROUTE 43 (WAUKEGAN ROAD)

SCALE: 1"=20' SHEET NO. 2 OF 3 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920, .01, 1518, 2022 & 1922, 4BIR	COOK	919	364
ILLINOIS FED. AID PROJECT				CONTRACT NO. 60T35

TS-16



ALL LOOPS SHALL BE PERFORMED WITHIN LIMITS OF RECONSTRUCTION

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

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	PLOT DATE = 10/30/2012	DATE -	10/31/2012	REVISED -	

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**TRAFFIC SIGNAL MODERNIZATION PLAN  
 WILLOW ROAD AT ILLINOIS ROUTE 43 (WAUKEGAN ROAD)**

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920, .01, 1518, 2022 & 1922, 4BIR	COOK	919	365
CONTRACT NO. 60T35				
ILLINOIS FED. AID PROJECT				

TS-17

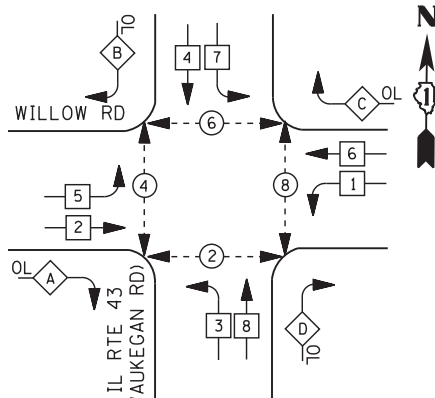
SCALE: 1"=20' SHEET NO. 3 OF 3 SHEETS STA. TO STA.

**SCHEDULE OF QUANTITIES**

QUANTITY	UNIT	ITEM
73.5	SO FT	SIGN PANEL - TYPE 1
30	SO FT	SIGN PANEL - TYPE 2
1	EACH	SERVICE INSTALLATION - GROUND MOUNTED
1818	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.
89	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.
68	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.
866	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.
4	EACH	HANDHOLE
4	EACH	HEAVY-DUTY HANDHOLE
4	EACH	DOUBLE HANDHOLE
1	EACH	TRANSCEIVER - FIBER OPTIC
1660	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C
2780	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
3943	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
1802	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C
6328	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
80	FOOT	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C
925	FOOT	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C
4	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE 46 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE 48 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE 50 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE 54 FT.
20	FOOT	CONCRETE FOUNDATION, TYPE A
4	FOOT	CONCRETE FOUNDATION, TYPE C
56	FOOT	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER
12	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED
4	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED
4	EACH	SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED
8	EACH	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
16	EACH	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM
19	EACH	INDUCTIVE LOOP DETECTOR
936	FOOT	PREFORMED DETECTOR LOOP
* 4	EACH	LIGHT DETECTOR
* 1	EACH	LIGHT DETECTOR AMPLIFIER
8	EACH	PEDESTRIAN PUSH-BUTTON
1	EACH	TEMPORARY TRAFFIC SIGNAL INSTALLATION
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
14	EACH	REMOVE EXISTING HANDHOLE
10	EACH	REMOVE EXISTING CONCRETE FOUNDATION
*1064	FOOT	EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO 20 3/C
1	EACH	MASTER CONTROLLER (SPECIAL)
1	EACH	FULL-ACTUATED CONTROLLER AND TYPE V CABINET, SPECIAL
1	EACH	UNINTERRUPTIBLE POWER SUPPLY, SPECIAL
1	EACH	TEMPORARY TRAFFIC SIGNAL TIMING

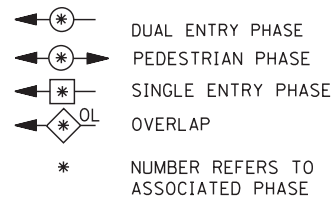
\* 100% VILLAGE OF NORTHFIELD COST

**CONTROLLER SEQUENCE**



**PHASE DESIGNATION DIAGRAM**

**LEGEND**



**RIGHT TURN OVERLAP PHASE DESIGNATION**

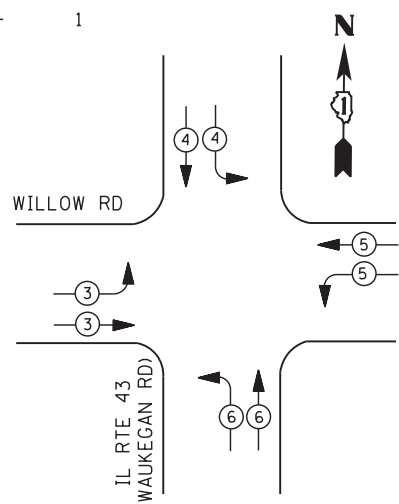
OVERLAP LETTER	PERMISSIVE PHASE	PROTECTED PHASE
A =	2	3
B =	4	5
C =	6	7
D =	8	1

I. D. O. T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					
TYPE	NO. LAMPS	WATTAGE		% OPERATIONS	TOTAL WATTAGE
		INCAND.	LED		
SIGNAL (RED)	24	135	17	0.50	204
(YELLOW)	24	135	25	0.25	150
(GREEN)	24	135	15	0.25	90
ARROW	16	135	12	0.10	19
PED. SIGNAL	8	90	25	1.00	200
CONTROLLER	2	100	100	1.00	200
ILLUM. SIGN		100	90	1.00	
TOTAL =					863

ENERGY COSTS- BILLED TO: IDOT - DISTRICT 1  
201 W. CENTER CT.  
SCHAUMBURG, IL 60196

ENERGY SUPPLY - CONTACT COMED  
PHONE 630-723-2128

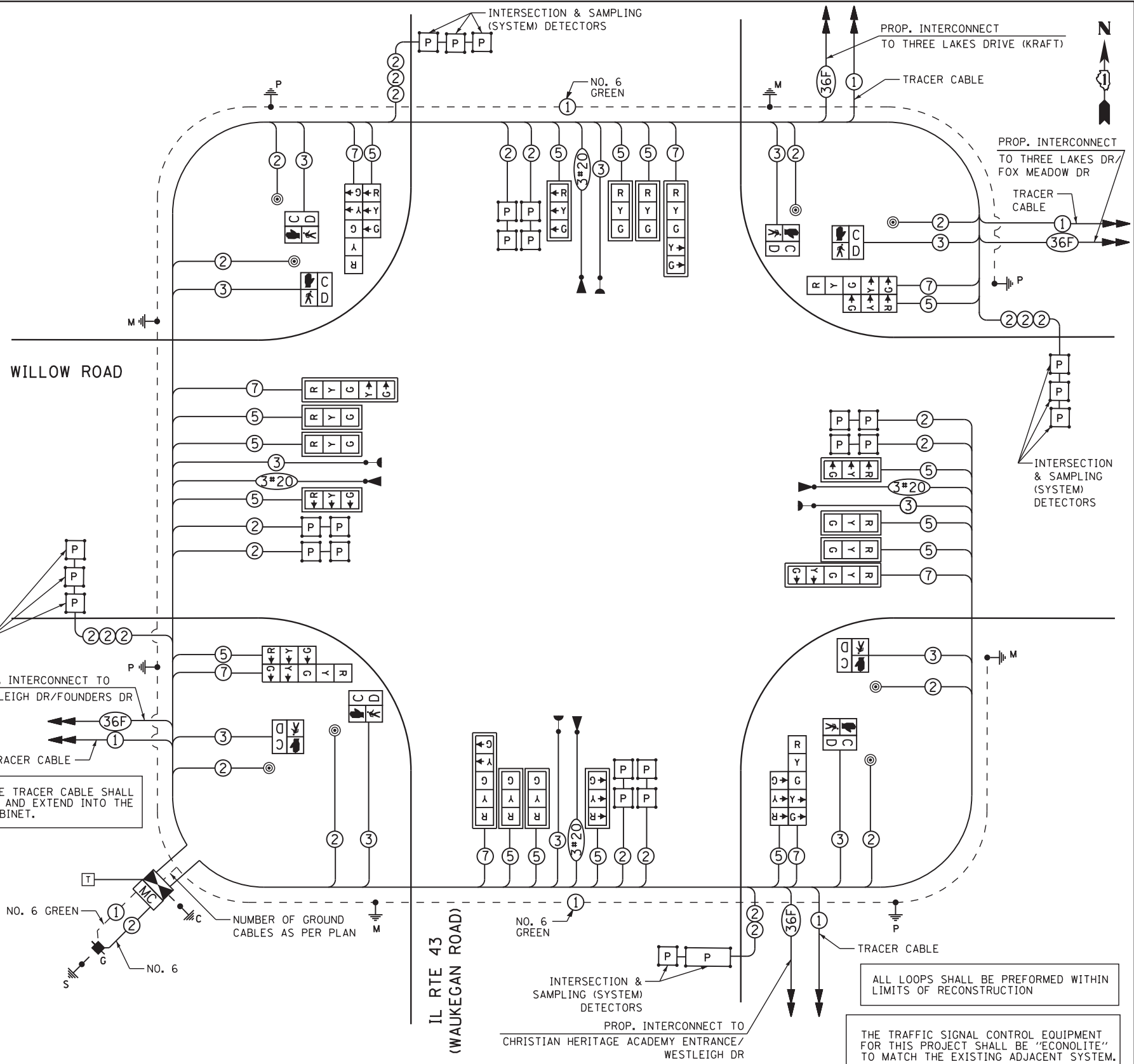
**EMERGENCY VEHICLE PREEMPTION SEQUENCE**



EMERGENCY VEHICLE PREEMPTORS				
EMERGENCY VEHICLE PREEMPTOR	3	4	5	6
MOVEMENT	→	↓	←	↑

**WILLOW ROAD**

**IL RTE 43 (WAUKEGAN ROAD)**



**CABLE PLAN**

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



### REMOVAL OF EXISTING TRAFFIC SIGNAL EQUIPMENT

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE AGENCY LISTED BELOW. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR PICK UP OF ALL EQUIPMENT TO BE RETURNED TO THE LISTED AGENCY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS. AGENCY: VILLAGE OF NORTHFIELD

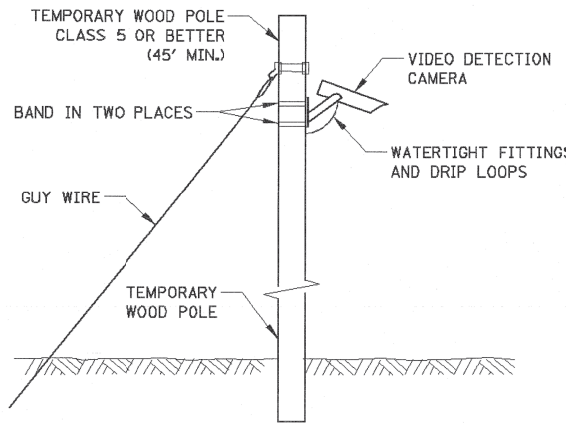
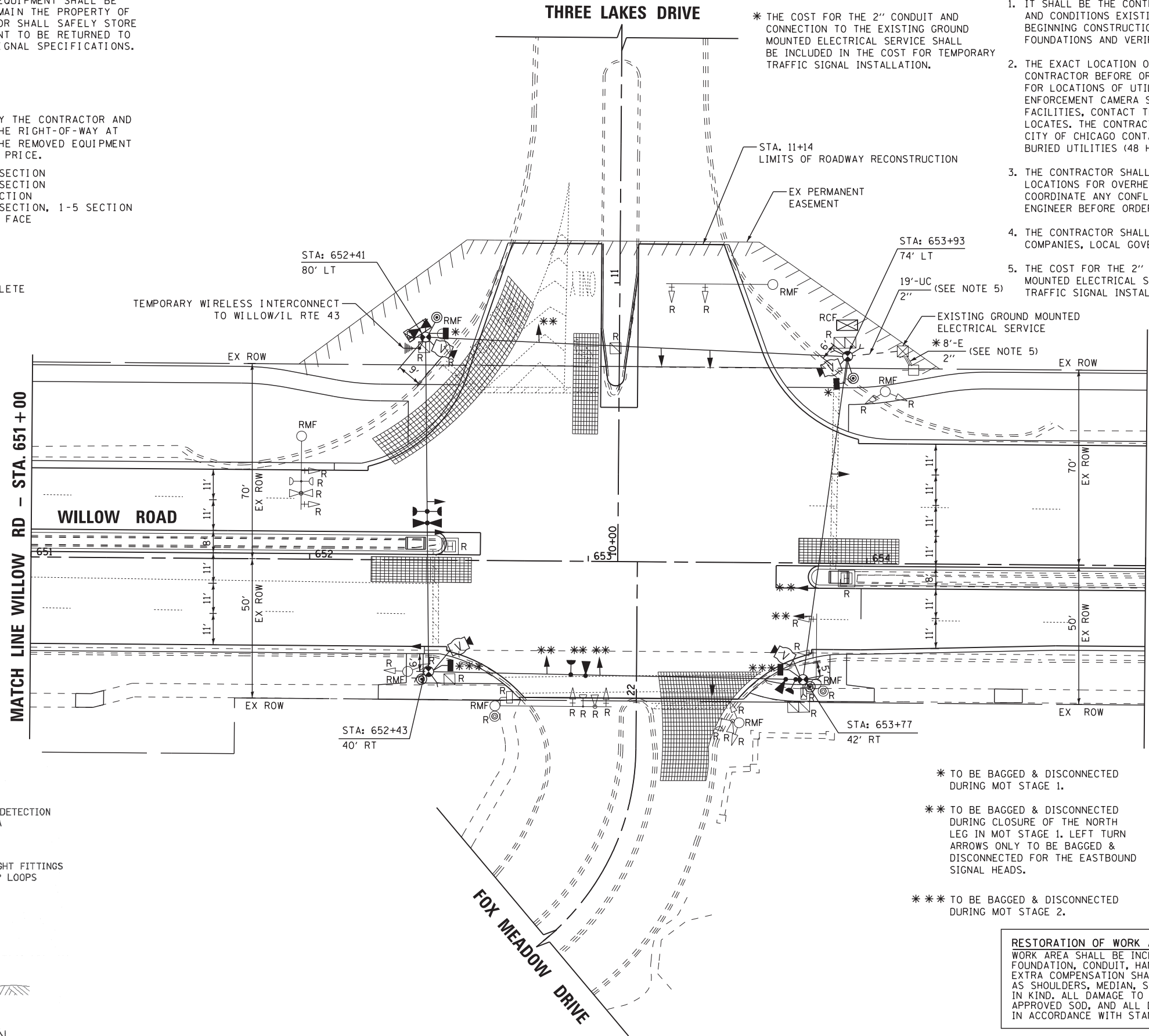
- 1 EACH LIGHT DETECTOR AMPLIFIER
- 3 EACH LIGHT DETECTOR AND BEACON

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 7 EACH SIGNAL HEAD, 1 - FACE, 3 - SECTION
- 1 EACH SIGNAL HEAD, 2 - FACE, 2-3 SECTION
- 2 EACH SIGNAL HEAD, 1 - FACE, 5 SECTION
- 2 EACH SIGNAL HEAD, 2 - FACE, 1-3 SECTION, 1-5 SECTION
- 2 EACH PEDESTRIAN SIGNAL HEAD, 1 - FACE
- 2 EACH PEDESTRIAN PUSH-BUTTON
- 8 EACH TRAFFIC SIGNAL BACKPLATE
- 4 EACH STEEL MAST ARM AND POLE
- 4 EACH SIGNAL POST
- 1 EACH SERVICE INSTALLATION
- 1 EACH CONTROLLER AND CABINET COMPLETE

### GENERAL NOTES

1. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARM LENGTHS.
2. THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES. THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811, IN THE CITY OF CHICAGO CONTACT DIGGER AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION REQUIRED).
3. THE CONTRACTOR SHALL CHECK THE PROPOSED TRAFFIC SIGNAL EQUIPMENT LOCATIONS FOR OVERHEAD UTILITY CONFLICTS. THE CONTRACTOR SHALL COORDINATE ANY CONFLICTS WITH THE UTILITY COMPANIES AND THE RESIDENT ENGINEER BEFORE ORDERING MATERIALS.
4. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, LOCAL GOVERNMENT AGENCIES AND IDOT.
5. THE COST FOR THE 2" CONDUIT AND CONNECTION TO THE EXISTING GROUND MOUNTED ELECTRICAL SERVICE SHALL BE INCLUDED IN THE COST FOR TEMPORARY TRAFFIC SIGNAL INSTALLATION.



TEMPORARY VIDEO DETECTION MOUNTING DETAIL (NOT TO SCALE)

- \* TO BE BAGGED & DISCONNECTED DURING MOT STAGE 1.
- \*\* TO BE BAGGED & DISCONNECTED DURING CLOSURE OF THE NORTH LEG IN MOT STAGE 1. LEFT TURN ARROWS ONLY TO BE BAGGED & DISCONNECTED FOR THE EASTBOUND SIGNAL HEADS.
- \*\*\* TO BE BAGGED & DISCONNECTED DURING MOT STAGE 2.

THERE IS AN EXISTING PERMANENT EASEMENT FOR FOX MEADOW DRIVE DEDICATED AS PART OF THE FOX MEADOW DEVELOPMENT WHICH ALLOWS INGRESS AND EGRESS FOR PROVIDING MUNICIPAL SERVICES.

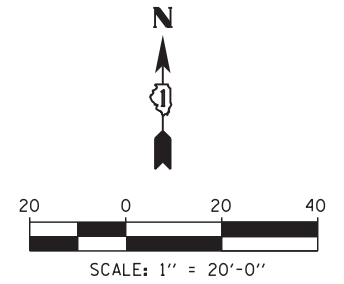
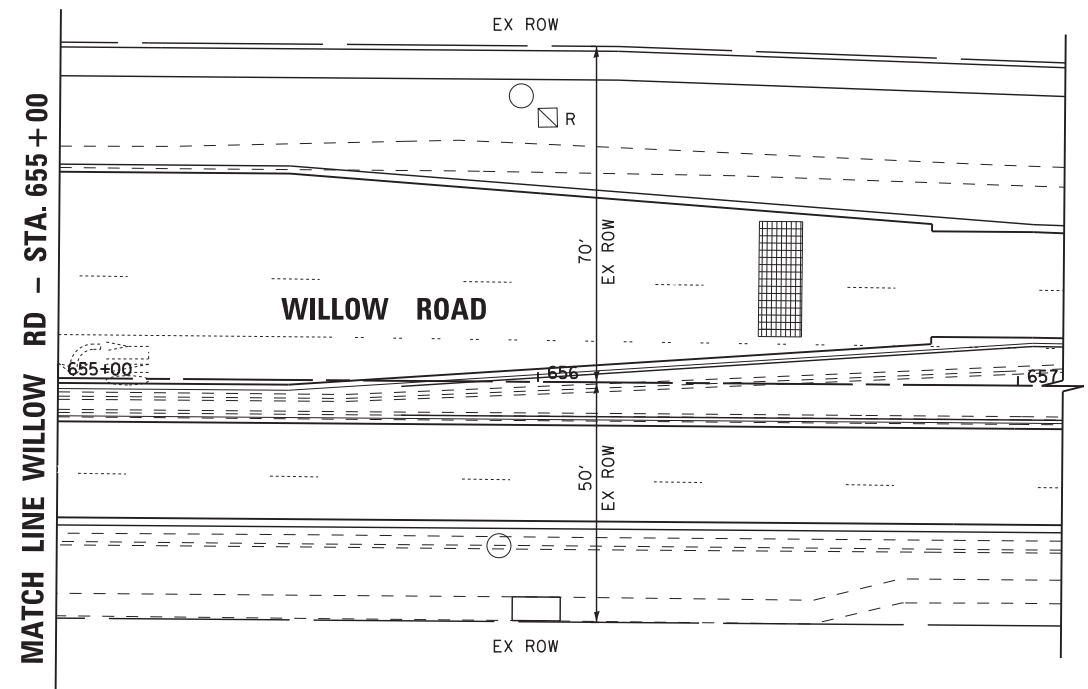
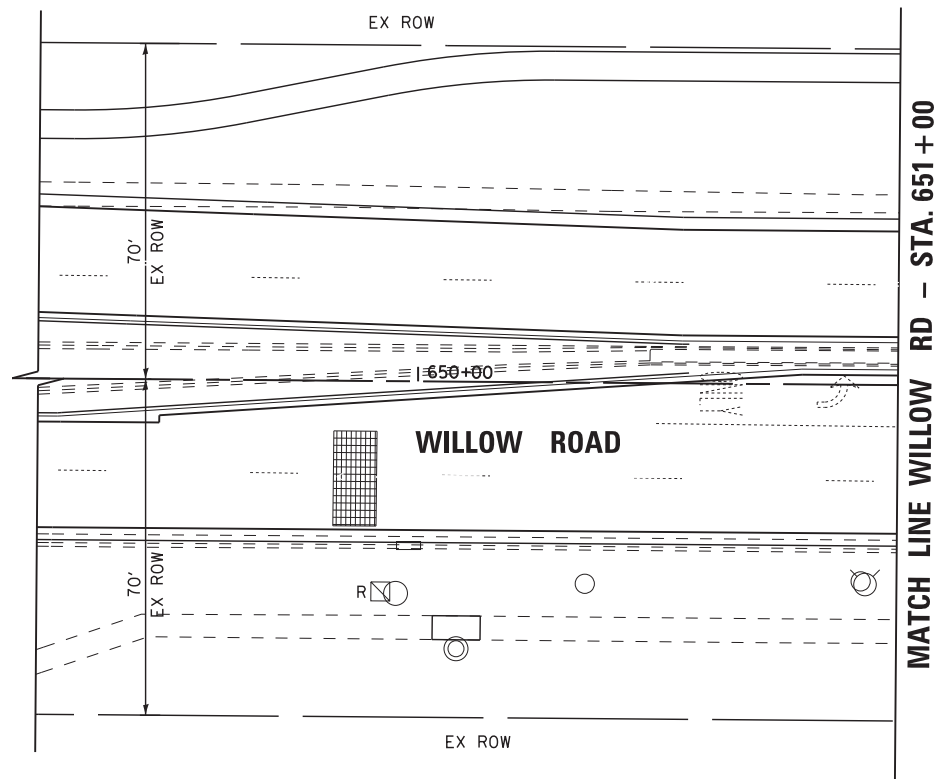
THE CONTRACTOR SHALL INSTALL A WOOD SUPPORT PLATFORM AS SHOWN ON THE DISTRICT 1 SIGNAL DETAIL SHEETS FOR BOTH THE CONTROLLER CABINET AND UPS.

THE TEMPORARY TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDING IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

FILE NAME =	USER NAME = \$USER\$	DESIGNED - KMM	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION			TEMPORARY TRAFFIC SIGNAL INSTALLATION & REMOVAL PLAN MAINTENANCE OF TRAFFIC STAGE 1 WILLOW ROAD AT THREE LAKES DRIVE /FOX MEADOW DRIVE			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT DATE = 10/30/2012	CHECKED - JDH	REVISED -	SCALE: 1"=20'			SHEET NO. 1 OF 2 SHEETS			CONTRACT NO. 60T35				
		DATE - 10/31/2012	REVISED -				STA. TO STA.			ILLINOIS FED. AID PROJECT				





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	PLOT DATE = 10/30/2012	DATE - 10/31/2012	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

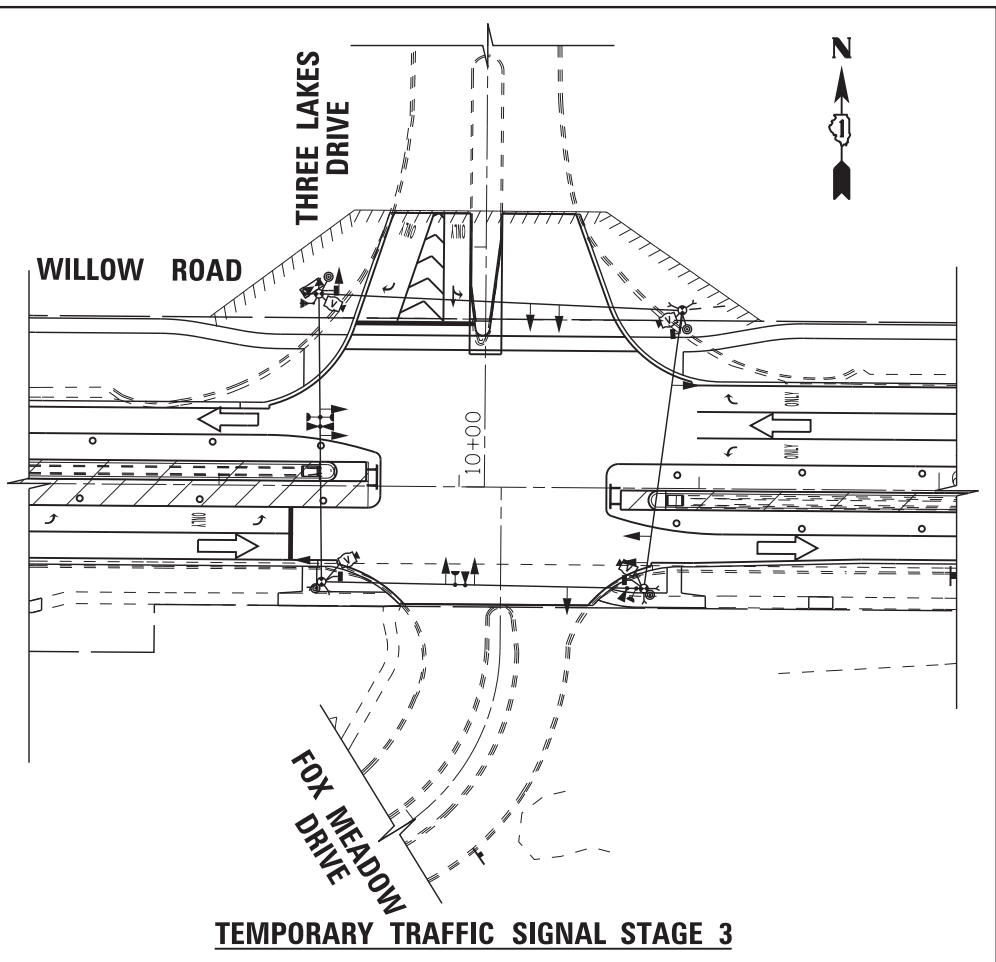
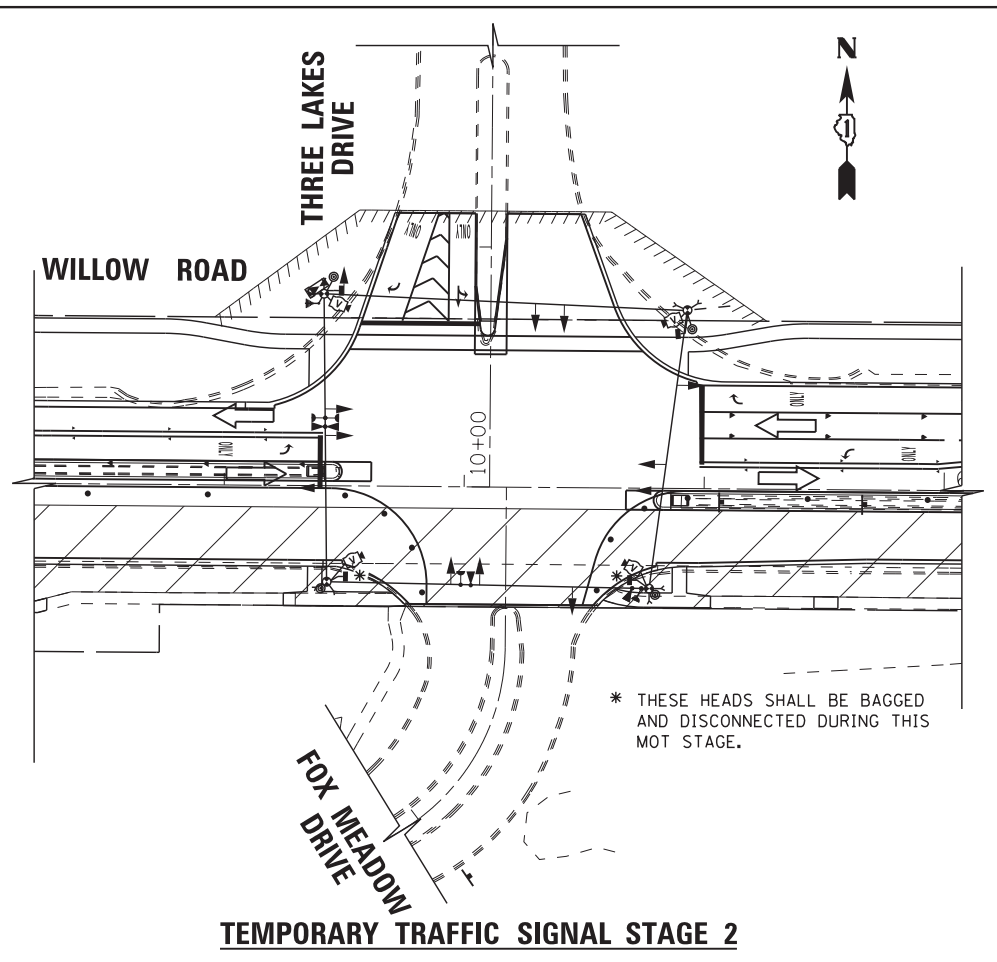
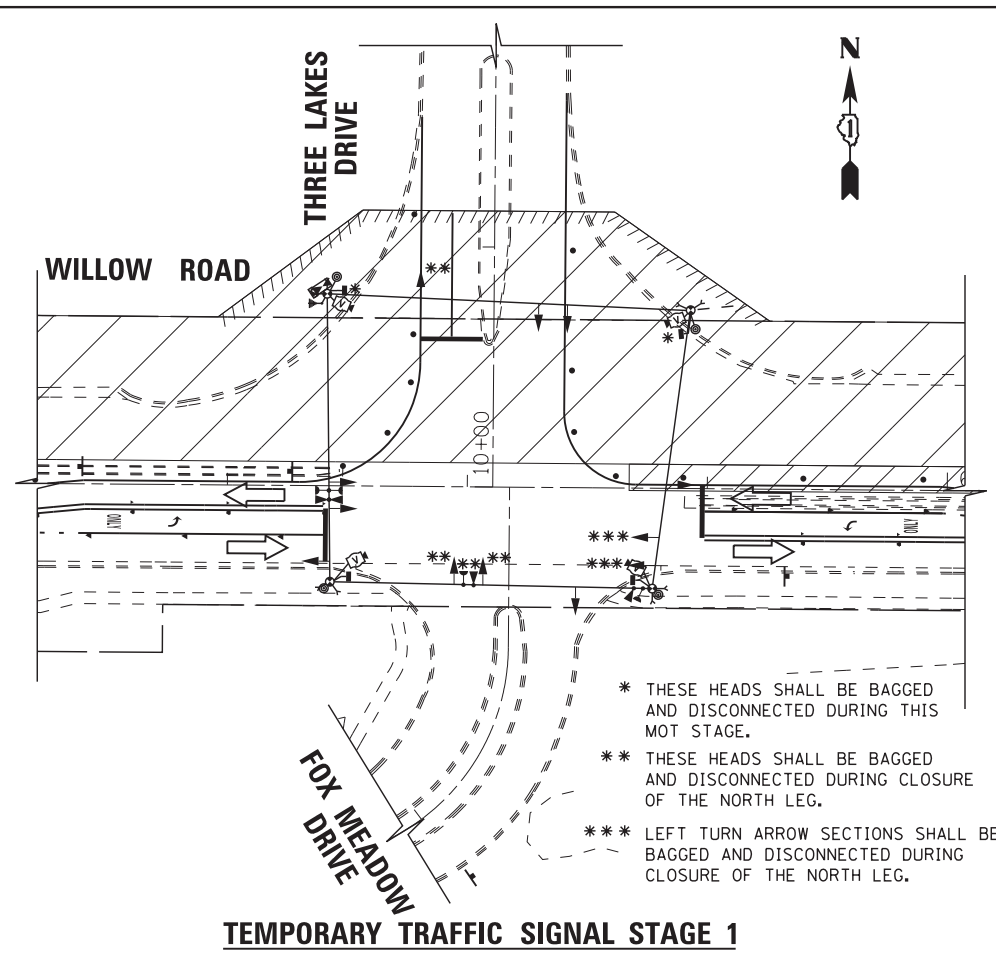
TEMPORARY TRAFFIC SIGNAL INSTALLATION & REMOVAL PLAN  
 MAINTENANCE OF TRAFFIC STAGE 1  
 WILLOW ROAD AT THREE LAKES DRIVE /FOX MEADOW DRIVE

SCALE: 1"=20' SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920, .01, 1518, 2022 & 1922, 4BIR)	COOK	919	368
CONTRACT NO. 60T35			ILLINOIS FED. AID PROJECT	

TS-20





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	PLOT DATE = 10/30/2012	DATE - 10/31/2012	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION  
 M.O.T. STAGING PLAN  
 WILLOW ROAD AT THREE LAKES DRIVE /FOX MEADOW DRIVE**

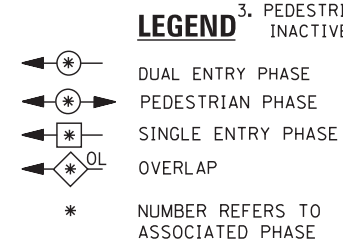
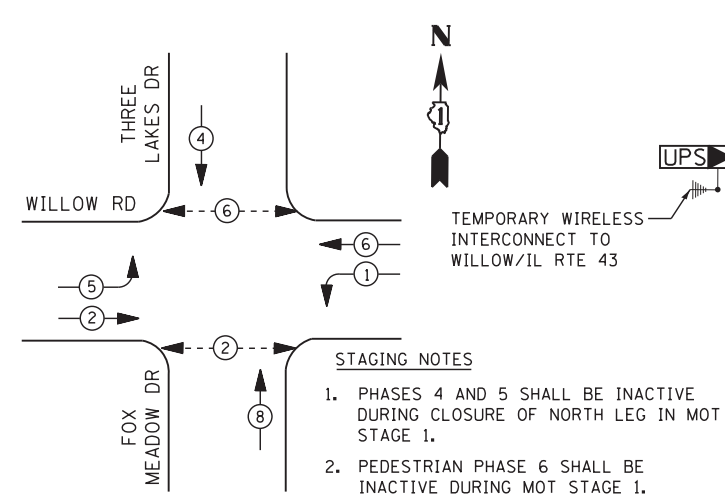
SCALE: 1"=40' SHEET NO. 1 OF X SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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				CONTRACT NO. 60T35
ILLINOIS FED. AID PROJECT				

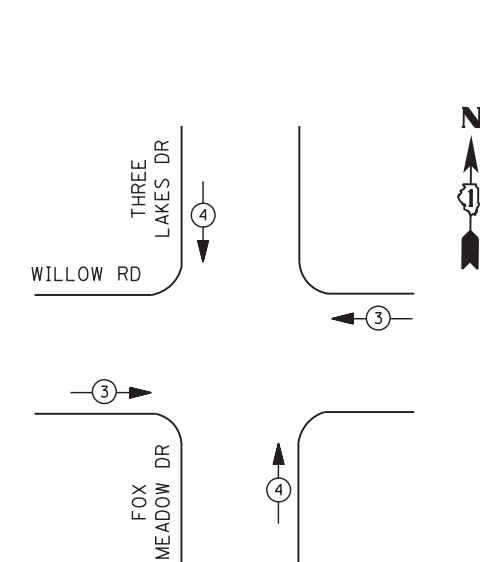
**NOTES FOR TEMPORARY TRAFFIC SIGNALS**

- 1) ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
- 2) ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1. INSTALLED IN A NEMA TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
- 3) ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE LED AND 12" (300mm) DIAMETER. HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. PEDESTRIAN SIGNALS SHALL INCLUDE SOLID INTERNATIONAL SYMBOLS. PEDESTRIAN SIGNALS WITH COUNTDOWN TIMERS SHALL BE USED WHEN THE EXISTING INSTALLATION UTILIZES COUNTDOWN TYPE OR AS DIRECTED BY THE ENGINEER. COUNTDOWN TYPE PEDESTRIAN SIGNALS ARE NOT TO BE INSTALLED AT RAILROAD INTERSECTIONS. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
- 4) ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES. RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
- 5) ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
- 6) THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL, AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
- 7) UNINTERRUPTIBLE POWER SUPPLY (UPS) SYSTEM SHALL BE INSTALLED AND MADE OPERATIONAL AT TEMPORARY TRAFFIC SIGNAL INSTALLATIONS WHERE UPS IS INSTALLED AT THE EXISTING TRAFFIC SIGNAL, TEMPORARY TRAFFIC SIGNALS AT RAILROAD INTERSECTIONS, AND TEMPORARY TRAFFIC SIGNALS AT INTERSECTIONS WITH FIRE STATION ACTUATED EMERGENCY VEHICLE PRE-EMPTION, OR WHEN INDICATED ON THE PLANS.
- 8) TRAFFIC SIGNAL MANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
- 9) DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER.
- 10) WHEN PAN, TILT, ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.

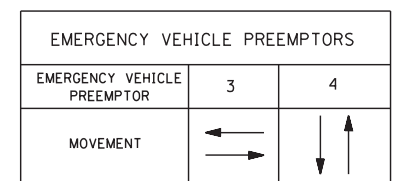
**TEMPORARY CONTROLLER SEQUENCE**



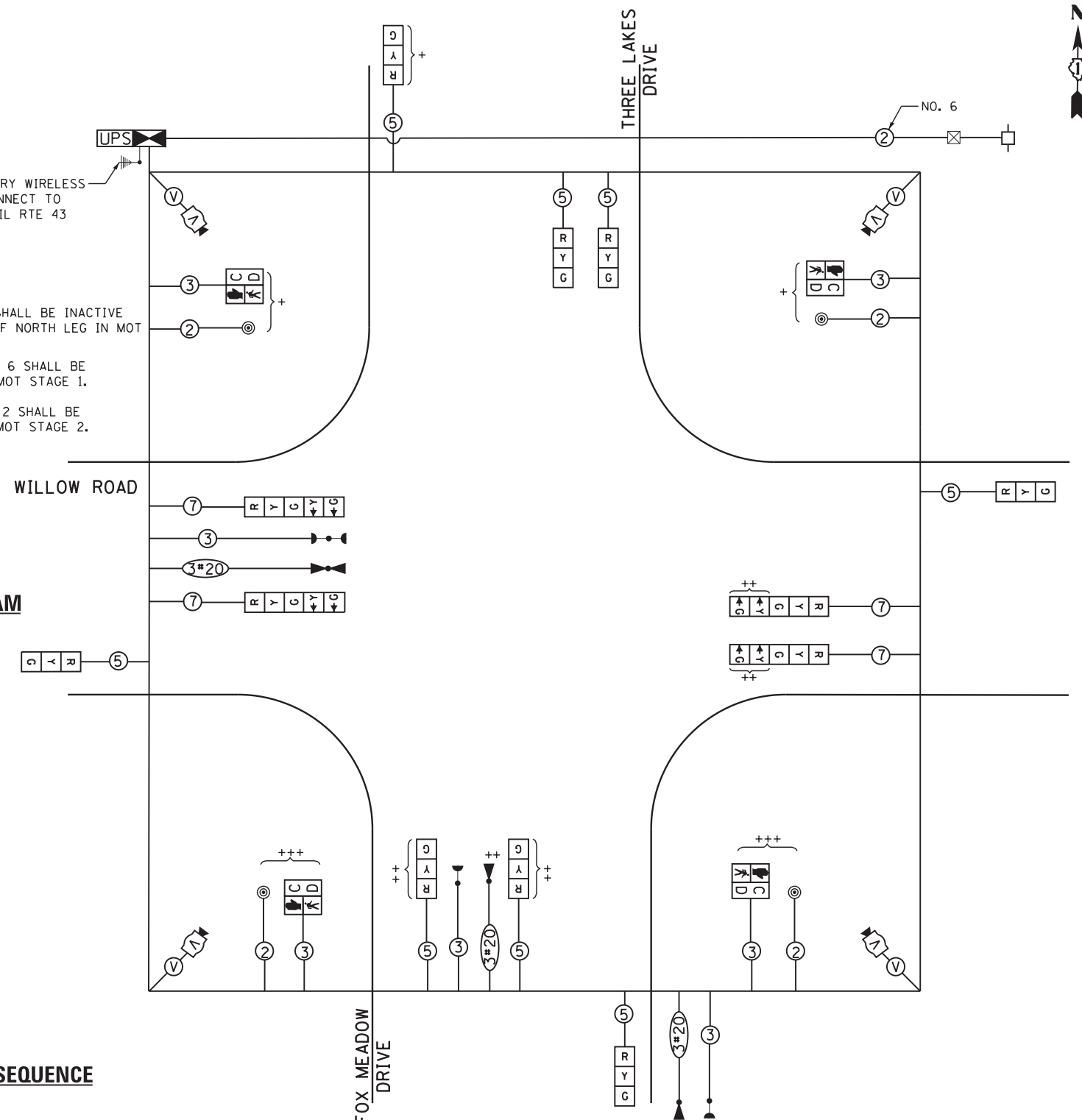
**TEMPORARY PHASE DESIGNATION DIAGRAM**



**TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE**



**RESTORATION OF WORK AREA.** RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



**TEMPORARY CABLE PLAN**

- + THESE HEADS SHALL BE BAGGED & DISCONNECTED DURING MOT STAGE 1.
- ++ THESE HEADS OR SECTIONS SHALL BE BAGGED & DISCONNECTED DURING CLOSURE OF THE NORTH LEG IN MOT STAGE 1.
- +++ THESE HEADS SHALL BE BAGGED & DISCONNECTED DURING MOT STAGE 2.

THE TEMPORARY TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

I. D. O. T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					
TYPE	NO. LAMPS	WATTAGE		% OPERATIONS	TOTAL WATTAGE
		INCAND.	LED		
SIGNAL (RED)	12	135	17	0.50	102
(YELLOW)	12	135	25	0.25	75
(GREEN)	12	135	15	0.25	45
ARROW	8	135	12	0.10	10
PED. SIGNAL	4	90	25	1.00	100
CONTROLLER	1	100	100	1.00	100
ILLUM. SIGN		252	90	1.00	--
VIDEO SYSTEM	1	150	-	1.00	150
FLASHER LED					
TOTAL =					582

ENERGY COSTS- BILLED TO: IDOT - DISTRICT 1  
 201 W. CENTER CT.  
 SCHAUMBURG, IL 60196

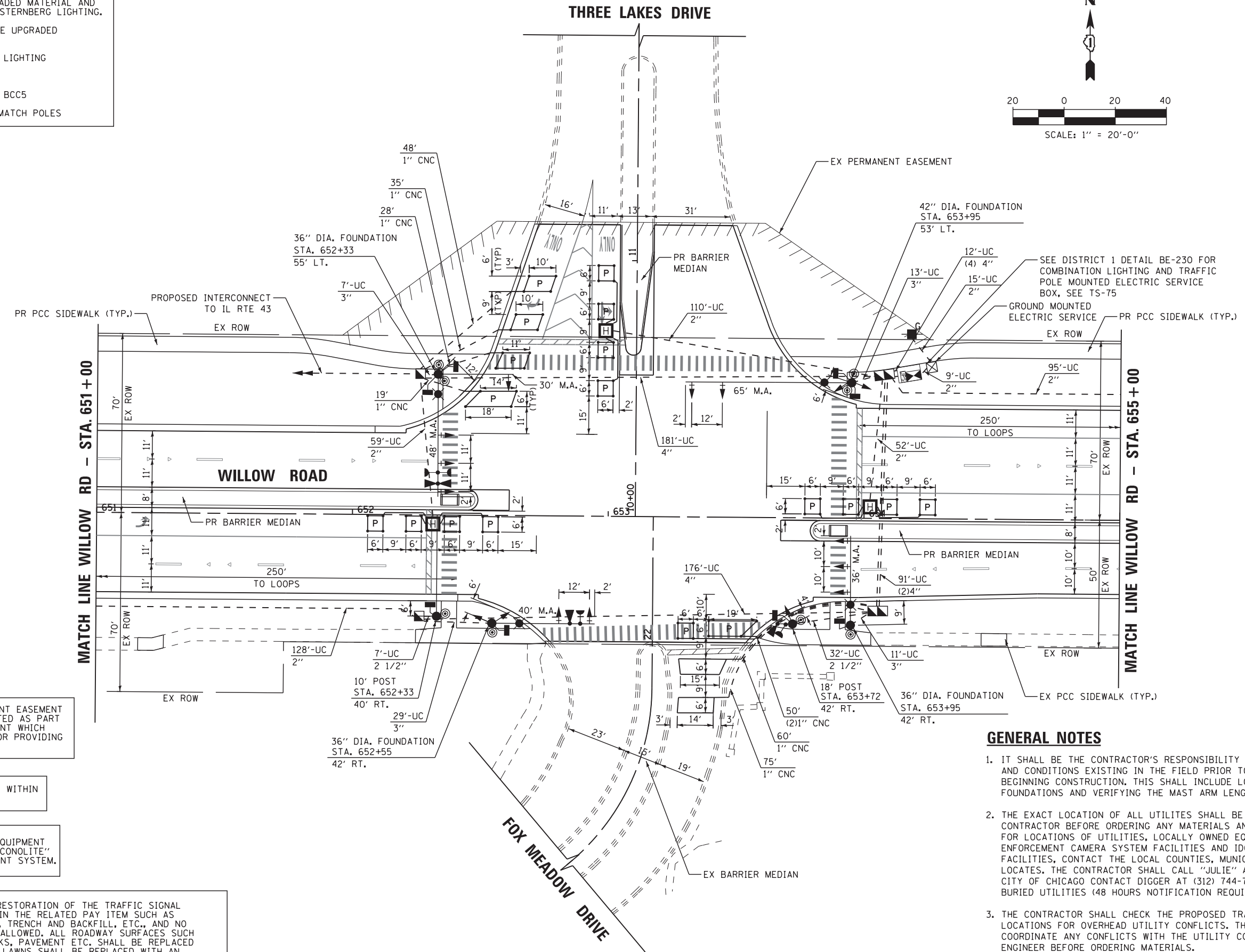
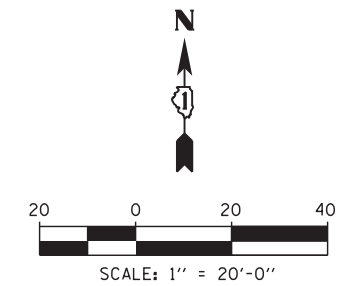
ENERGY SUPPLY - CONTACT COMED  
 PHONE 630-723-2128



ALL TRAFFIC SIGNAL POLES AT THIS INTERSECTION SHALL BE CONSTRUCTED OF UPGRADED MATERIAL AND SPECIFICATION AS PRODUCED BY STERNBERG LIGHTING.

THE SPECIFICATIONS FOR THE UPGRADED MATERIAL ARE AS FOLLOWS:

MANUFACTURER: STERNBERG LIGHTING  
 COLOR: VERDE GREEN  
 BASE PLATE: BIRMINGHAM  
 POLES: STRAIGHT FLUTE  
 POST CENTER CAP: BCC OR BCC5  
 ROADWAY ARM: CA  
 STRAPS: VERDE GREEN TO MATCH POLES



THERE IS AN EXISTING PERMANENT EASEMENT FOR FOX MEADOW DRIVE DEDICATED AS PART OF THE FOX MEADOW DEVELOPMENT WHICH ALLOWS INGRESS AND EGRESS FOR PROVIDING MUNICIPAL SERVICES.

ALL LOOPS SHALL BE PERFORMED WITHIN LIMITS OF RECONSTRUCTION

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

**RESTORATION OF WORK AREA.** RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

**GENERAL NOTES**

- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARM LENGTHS.
- THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES. THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811, IN THE CITY OF CHICAGO CONTACT DIGGER AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION REQUIRED).
- THE CONTRACTOR SHALL CHECK THE PROPOSED TRAFFIC SIGNAL EQUIPMENT LOCATIONS FOR OVERHEAD UTILITY CONFLICTS. THE CONTRACTOR SHALL COORDINATE ANY CONFLICTS WITH THE UTILITY COMPANIES AND THE RESIDENT ENGINEER BEFORE ORDERING MATERIALS.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, LOCAL GOVERNMENT AGENCIES AND IDOT.

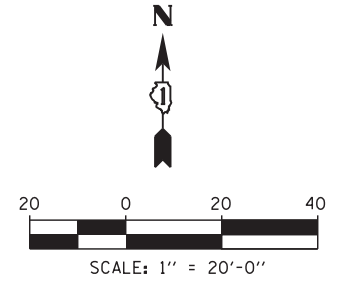
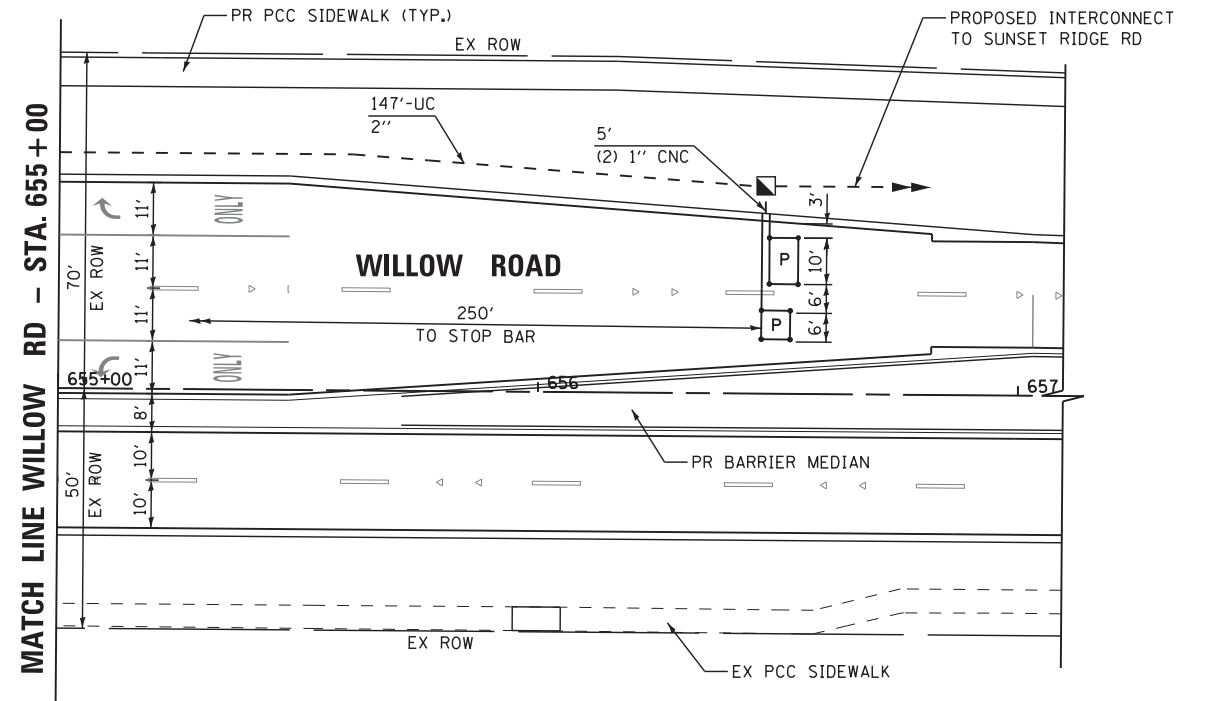
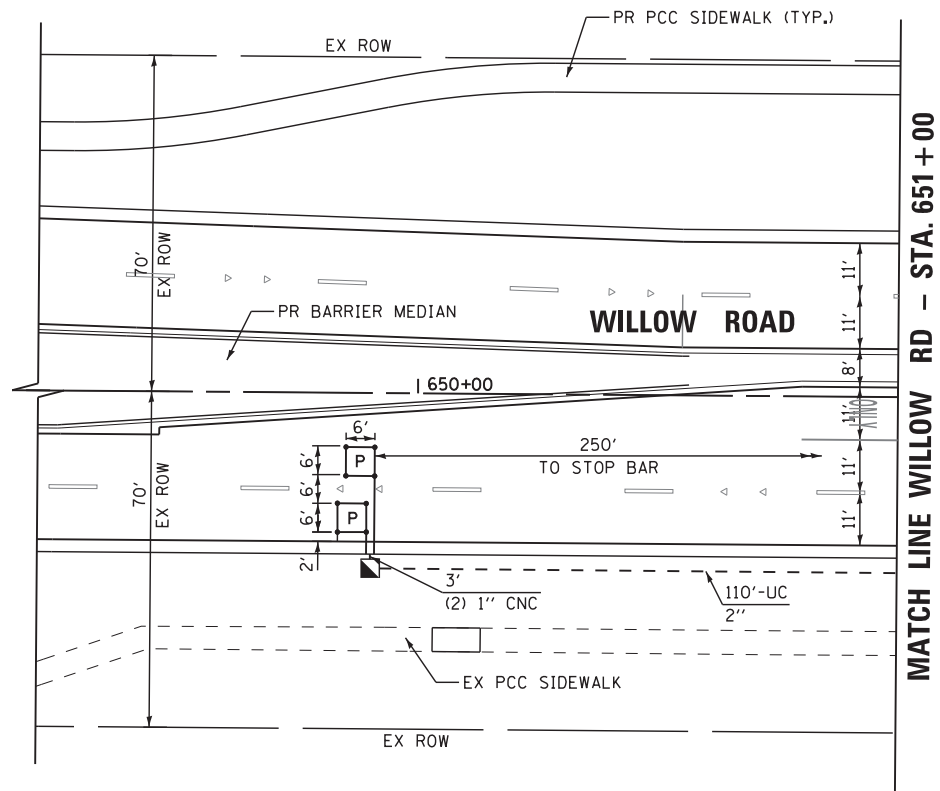
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	PLOT DATE = 10/30/2012	DATE -	10/31/2012	REVISED -	

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL MODERNIZATION PLAN  
 WILLOW ROAD AT THREE LAKES DRIVE /FOX MEADOW DRIVE  
 SCALE: 1"=20' SHEET NO. 1 OF 2 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920, .01, 1518, 2022 & 1922, 4BR)	COOK	919	371
				CONTRACT NO. 60T35
ILLINOIS FED. AID PROJECT				

TS-23



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	PLOT DATE = 10/30/2012	DATE - 10/31/2012	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL MODERNIZATION PLAN  
 WILLOW ROAD AT THREE LAKES DRIVE / FOX MEADOW DRIVE

SCALE: 1"=20' SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920, .01, 1518, 2022 & 1922, 481R)	COOK	919	372
CONTRACT NO. 60T35				
ILLINOIS FED. AID PROJECT				

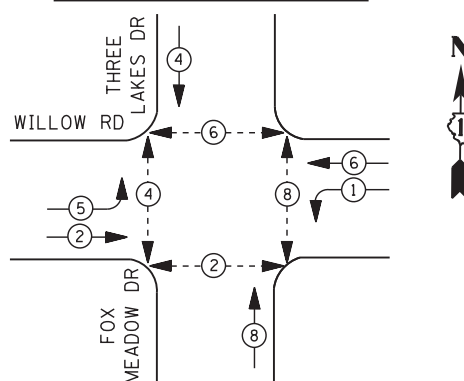


**SCHEDULE OF QUANTITIES**

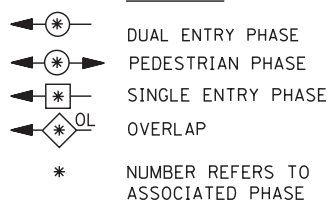
QUANTITY	UNIT	ITEM
1	EACH	SERVICE INSTALLATION - GROUND MOUNTED
725	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.
39	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.
60	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.
987	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.
4	EACH	HANDHOLE
4	EACH	HEAVY-DUTY HANDHOLE
2	EACH	DOUBLE HANDHOLE
1	EACH	TRANSCEIVER - FIBER OPTIC
1601	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C
2545	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
2874	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
923	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C
1707	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
45	FOOT	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C
781	FOOT	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C
12	FOOT	CONCRETE FOUNDATION, TYPE A
4	FOOT	CONCRETE FOUNDATION, TYPE C
38	FOOT	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER
25	FOOT	CONCRETE FOUNDATION, TYPE E 42-INCH DIAMETER
9	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED
1	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED
2	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED
2	EACH	SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED
4	EACH	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
2	EACH	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
11	EACH	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM
7	EACH	INDUCTIVE LOOP DETECTOR
93	FOOT	DETECTOR LOOP, TYPE I
861	FOOT	PREFORMED DETECTOR LOOP
3	EACH	LIGHT DETECTOR
1	EACH	LIGHT DETECTOR AMPLIFIER
8	EACH	PEDESTRIAN PUSH-BUTTON
1	EACH	TEMPORARY TRAFFIC SIGNAL INSTALLATION
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
9	EACH	REMOVE EXISTING HANDHOLE
9	EACH	REMOVE EXISTING CONCRETE FOUNDATION
888	FOOT	EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO 20 3/C
1	EACH	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL
1	EACH	UNINTERRUPTABLE POWER SUPPLY, SPECIAL
1	EACH	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 36 FT. (SPECIAL)
1	EACH	TEMPORARY TRAFFIC SIGNAL TIMING
1	EACH	TRAFFIC SIGNAL POST, ALUMINUM 10FT. (SPECIAL)
1	EACH	TRAFFIC SIGNAL POST, ALUMINUM 18FT. (SPECIAL)
1	EACH	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 36 FT. (SPECIAL)
1	EACH	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 40 FT. (SPECIAL)
1	EACH	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 65 FT. (SPECIAL)
1	EACH	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH DUAL MAST ARMS, 48 FT. AND 30 FT. (SPECIAL)
4	EACH	ILLUMINATED STREET NAME SIGN

\* 100% VILLAGE OF NORTHFIELD COST

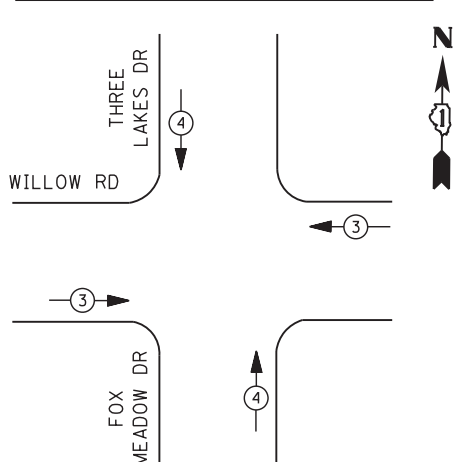
**CONTROLLER SEQUENCE**



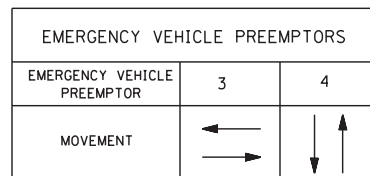
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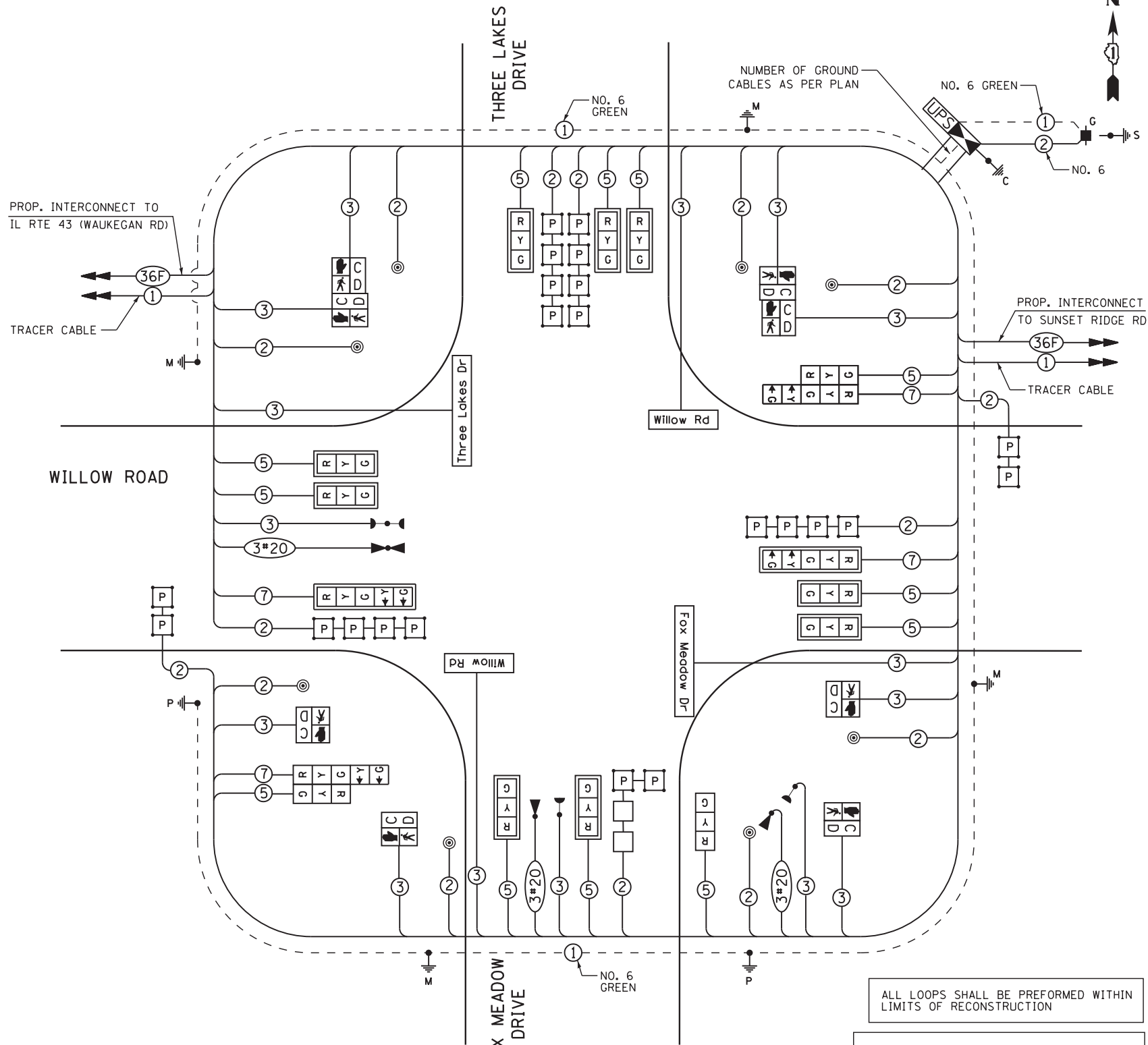
**PHASE DESIGNATION DIAGRAM**



**EMERGENCY VEHICLE PREEMPTION SEQUENCE**



**CABLE PLAN**



ALL LOOPS SHALL BE PREFORMED WITHIN LIMITS OF RECONSTRUCTION

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

FOR COMBINATION POLE LIGHTING AND WIRING, SEE ROADWAY LIGHTING PLANS.

THE END OF THE TRACER CABLE SHALL BE CONTINUOUS AND EXTEND INTO THE CONTROLLER CABINET.

**RESTORATION OF WORK AREA.** RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

I. D. O. T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					
TYPE	NO. LAMPS	WATTAGE		% OPERATIONS	TOTAL WATTAGE
		INCAND.	LED		
SIGNAL (RED)	16	135	17	0.50	136
(YELLOW)	16	135	25	0.25	100
(GREEN)	16	135	15	0.25	60
ARROW	8	135	12	0.10	10
PED. SIGNAL	8	90	25	1.00	200
CONTROLLER	1	100	100	1.00	100
ILLUM. SIGN	4		90	0.50	180
TOTAL =					786

ENERGY COSTS- BILLED TO: IDOT - DISTRICT 1  
201 W. CENTER CT.  
SCHAUMBURG, IL 60196

ENERGY SUPPLY - CONTACT COMED  
PHONE 630-723-2128

FILE NAME -	USER NAME - \$USER\$	DESIGNED - KMM	REVISED -
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		CHECKED - JDH	REVISED -
		DATE - 10/31/2012	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

CABLE PLAN, SCHEDULE OF QUANTITIES,  
PHASE DESIGNATION DIAGRAM & EVP SEQUENCE  
WILLOW ROAD AT THREE LAKES DRIVE / FOX MEADOW DRIVE

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920, .01, 1518, 2022 & 1922, 48R)	COOK	919	373
				CONTRACT NO. 60T35
ILLINOIS FED. AID PROJECT				

TS-25

SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.



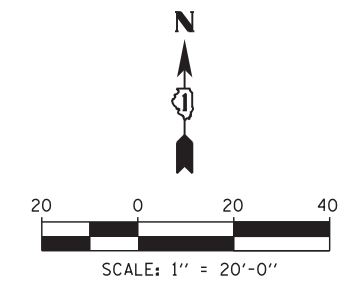
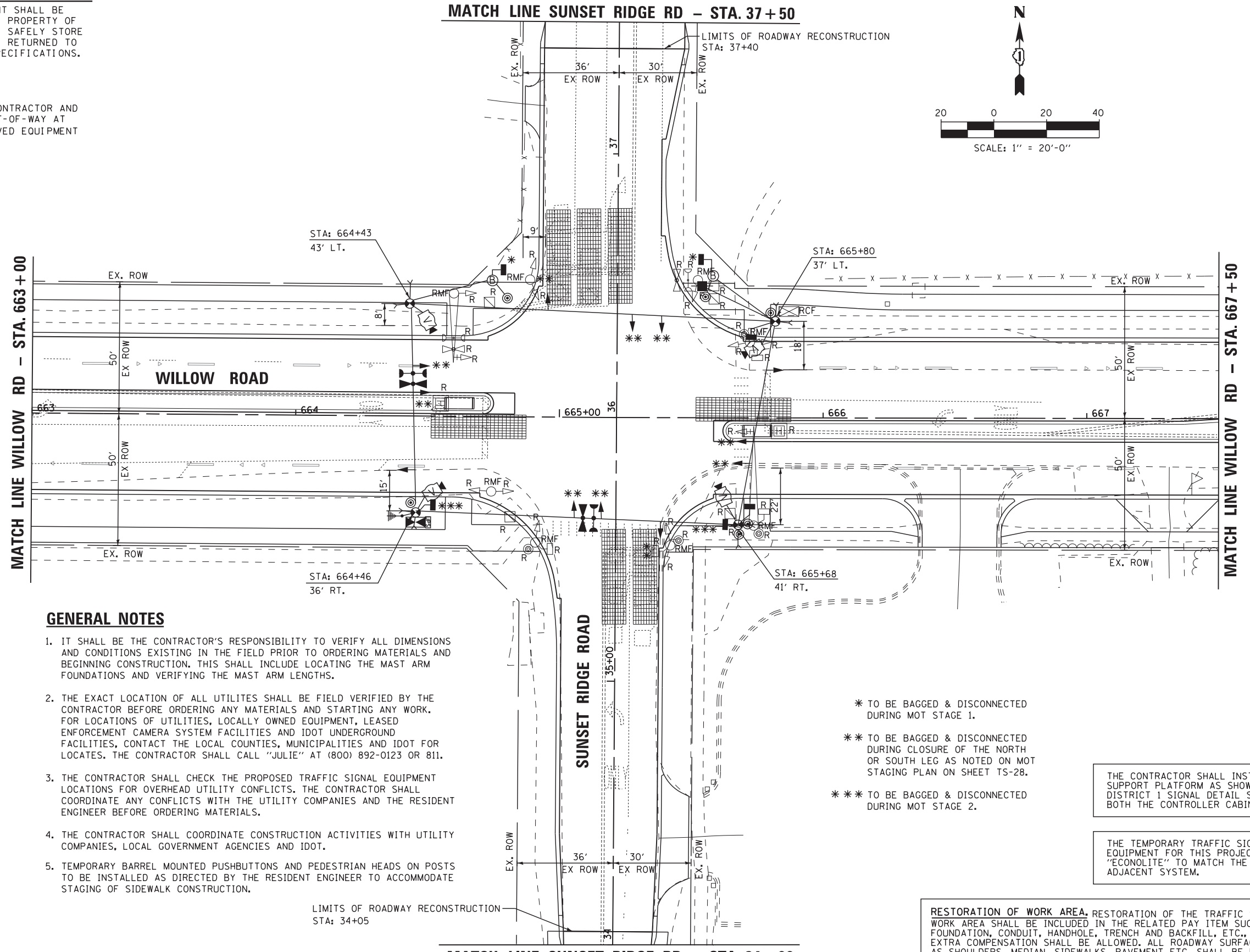
### REMOVAL OF EXISTING TRAFFIC SIGNAL EQUIPMENT

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE AGENCY LISTED BELOW. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR PICK UP OF ALL EQUIPMENT TO BE RETURNED TO THE LISTED AGENCY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS. AGENCY: VILLAGE OF NORTHFIELD

- 2 EACH LIGHT DETECTOR AND BEACON
- 1 EACH LIGHT DETECTOR AMPLIFIER

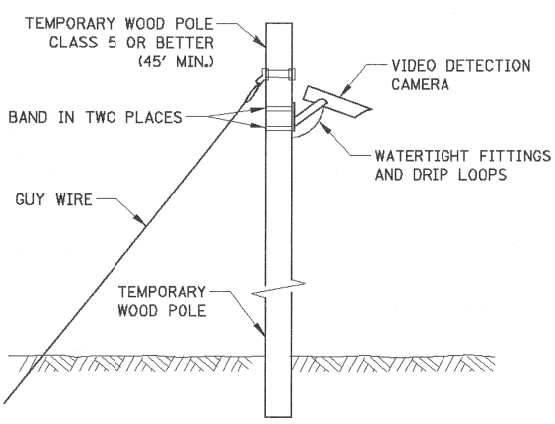
THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 4 EACH SIGNAL HEAD, 1 - FACE, 3 - SECTION
- 2 EACH SIGNAL HEAD, 2 - FACE, 2-3 SECTION
- 2 EACH SIGNAL HEAD, 1 - FACE, 5 - SECTION
- 2 EACH SIGNAL HEAD, 2 - FACE, 1-3 SECTION, 1-5 SECTION
- 4 EACH PEDESTRIAN SIGNAL HEAD, 1 - FACE
- 4 EACH PEDESTRIAN PUSH-BUTTON
- 2 EACH TRAFFIC SIGNAL BACKPLATE
- 2 EACH ALUMINUM MAST ARM AND POLE
- 6 EACH SIGNAL POST
- 1 EACH SERVICE INSTALLATION
- 1 EACH CONTROLLER AND CABINET COMPLETE
- 1 EACH UNINTERRUPTIBLE POWER SUPPLY



### GENERAL NOTES

1. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARM LENGTHS.
2. THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES. THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811.
3. THE CONTRACTOR SHALL CHECK THE PROPOSED TRAFFIC SIGNAL EQUIPMENT LOCATIONS FOR OVERHEAD UTILITY CONFLICTS. THE CONTRACTOR SHALL COORDINATE ANY CONFLICTS WITH THE UTILITY COMPANIES AND THE RESIDENT ENGINEER BEFORE ORDERING MATERIALS.
4. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, LOCAL GOVERNMENT AGENCIES AND IDOT.
5. TEMPORARY BARREL MOUNTED PUSHBUTTONS AND PEDESTRIAN HEADS ON POSTS TO BE INSTALLED AS DIRECTED BY THE RESIDENT ENGINEER TO ACCOMMODATE STAGING OF SIDEWALK CONSTRUCTION.



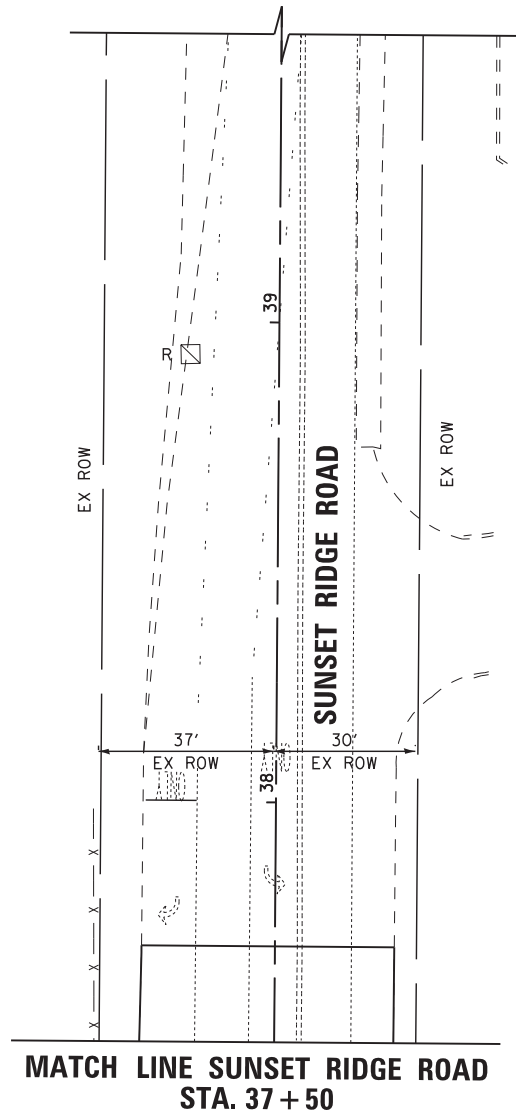
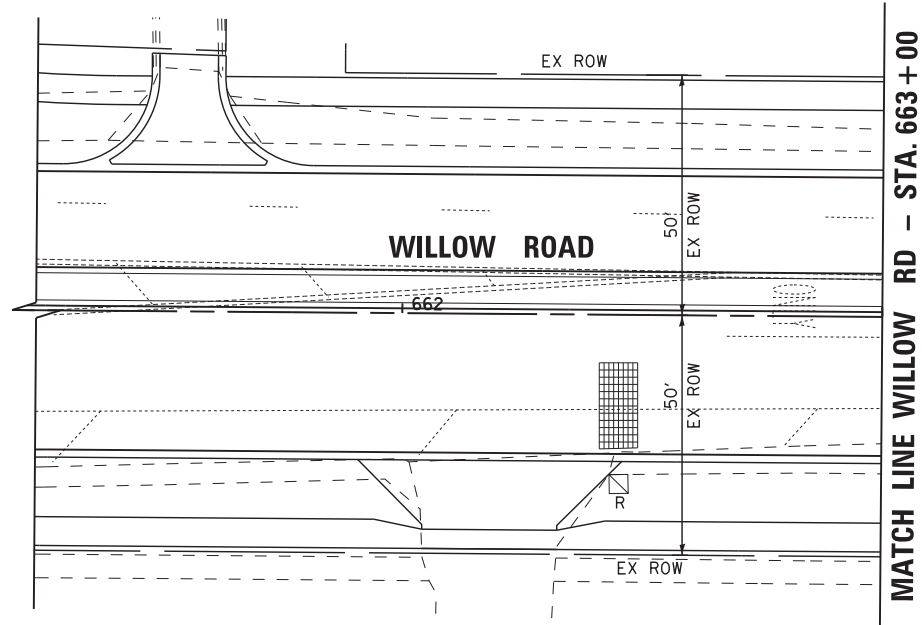
- \* TO BE BAGGED & DISCONNECTED DURING MOT STAGE 1.
- \*\* TO BE BAGGED & DISCONNECTED DURING CLOSURE OF THE NORTH OR SOUTH LEG AS NOTED ON MOT STAGING PLAN ON SHEET TS-28.
- \*\*\* TO BE BAGGED & DISCONNECTED DURING MOT STAGE 2.

THE CONTRACTOR SHALL INSTALL A WOOD SUPPORT PLATFORM AS SHOWN ON THE DISTRICT 1 SIGNAL DETAIL SHEETS FOR BOTH THE CONTROLLER CABINET AND UPS.

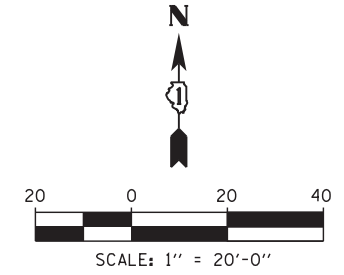
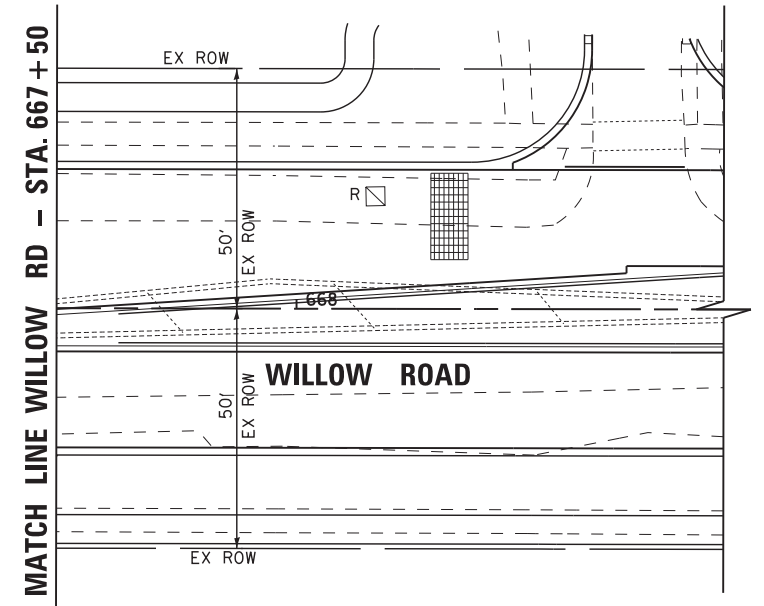
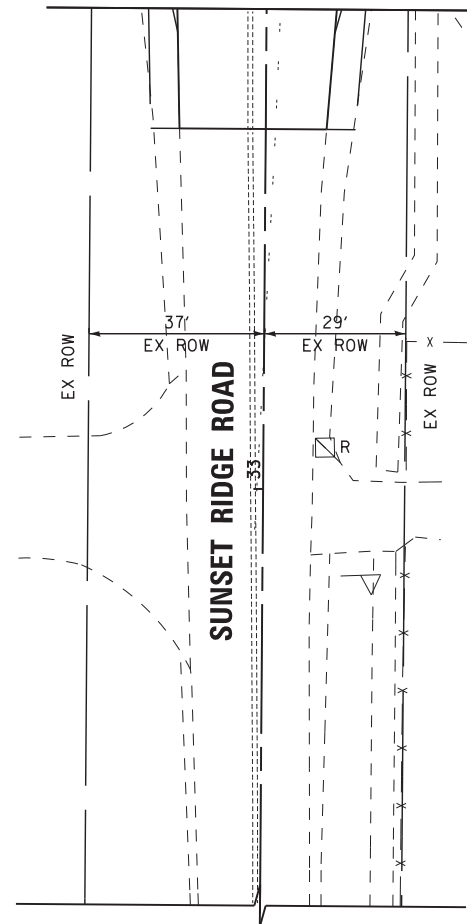
THE TEMPORARY TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

RESTORATION OF WORK AREA, RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SECTED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

FILE NAME =	USER NAME = \$USER\$	DESIGNED - KMM	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TEMPORARY TRAFFIC SIGNAL INSTALLATION &amp; REMOVAL PLAN MAINTENANCE OF TRAFFIC STAGE 1 WILLOW ROAD AT SUNSET RIDGE ROAD</b>	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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	PLOT DATE = 10/30/2012	CHECKED - JDH	REVISED -			CONTRACT NO. 60T35					
		DATE - 10/31/2012	REVISED -			ILLINOIS FED. AID PROJECT					



MATCH LINE SUNSET RIDGE ROAD  
 STA. 34 + 00



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	PLOT DATE = 10/30/2012	DATE - 10/31/2012	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TEMPORARY TRAFFIC SIGNAL INSTALLATION & REMOVAL PLAN  
 MAINTENANCE OF TRAFFIC STAGE 1  
 WILLOW ROAD AT SUNSET RIDGE ROAD

SCALE: 1"=20'

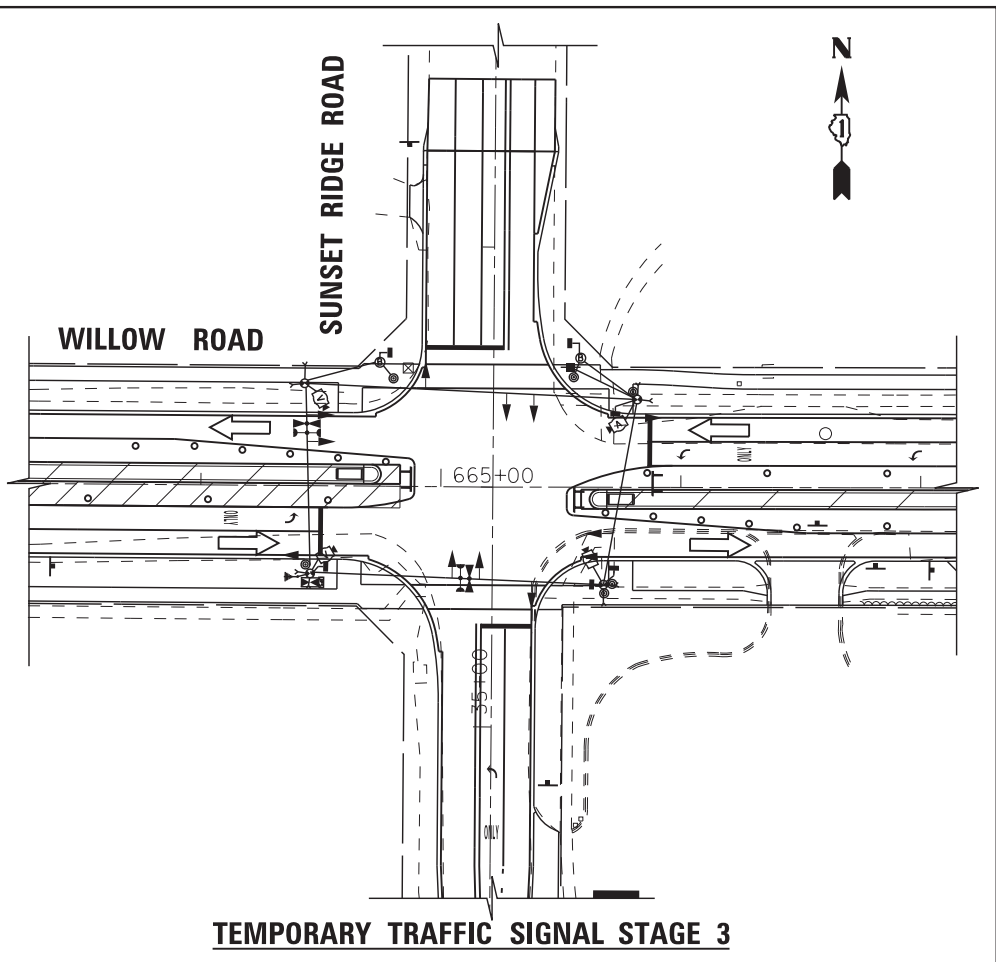
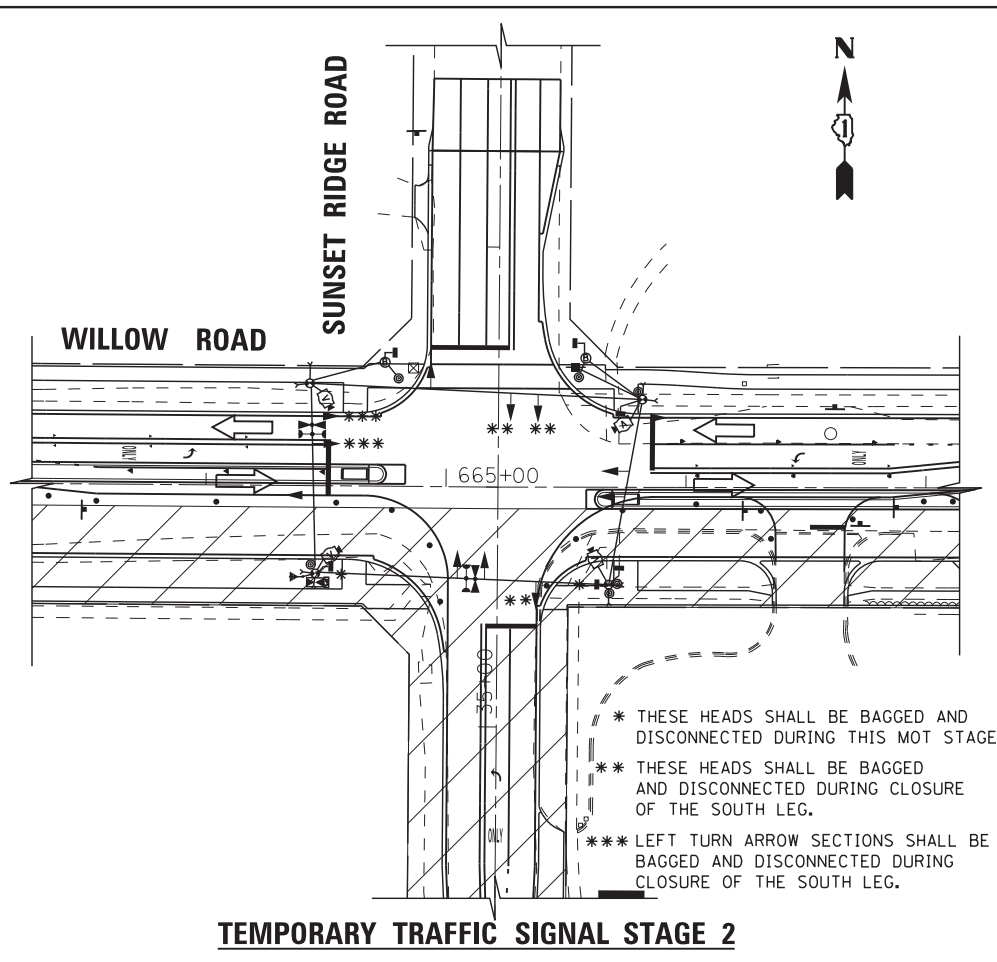
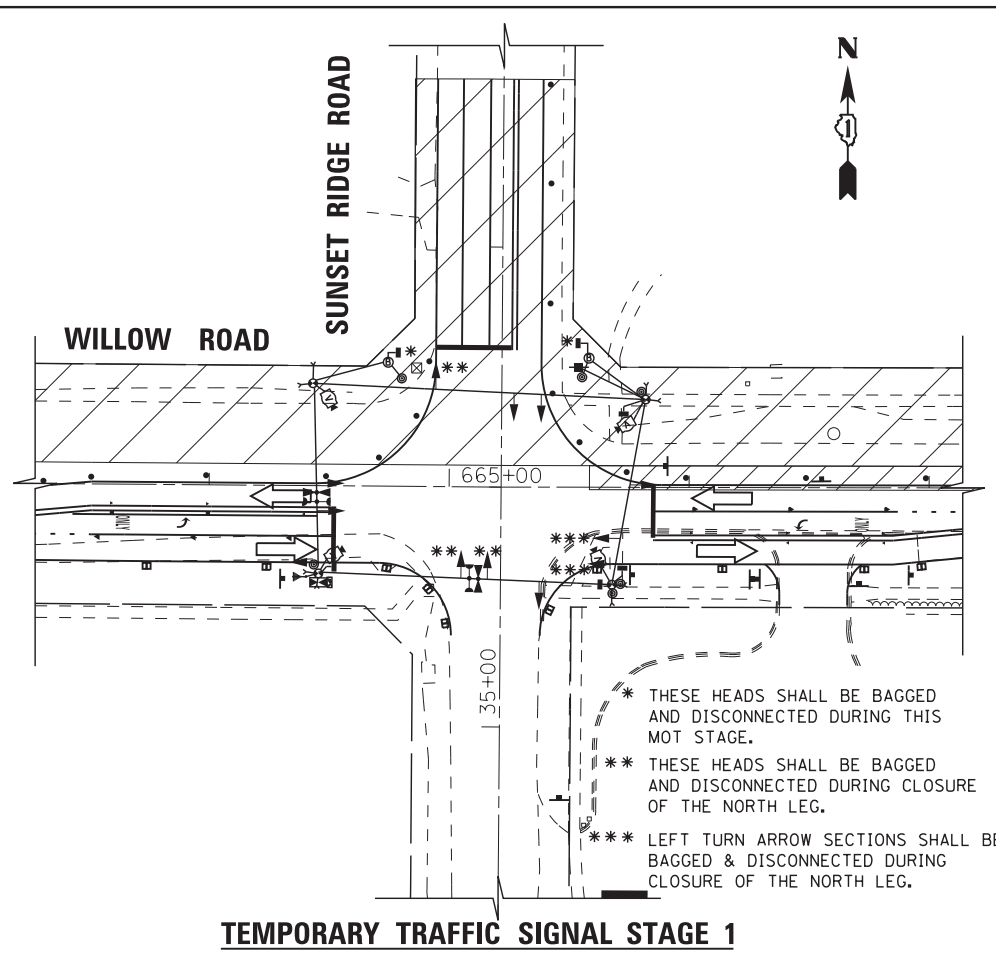
SHEET NO. 2 OF 2 SHEETS

STA.

TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 60T35				
ILLINOIS FED. AID PROJECT				

TS-27



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	PLOT SCALE = *SCALE*	CHECKED - JDH	REVISED -
	PLOT DATE = 10/30/2012	DATE - 10/31/2012	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TEMPORARY TRAFFIC SIGNAL INSTALLATION  
 M.O.T. STAGING PLAN  
 WILLOW ROAD AT SUNSET RIDGE ROAD

SCALE: 1"=40' SHEET NO. 1 OF X SHEETS STA. TO STA.

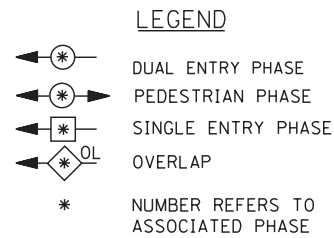
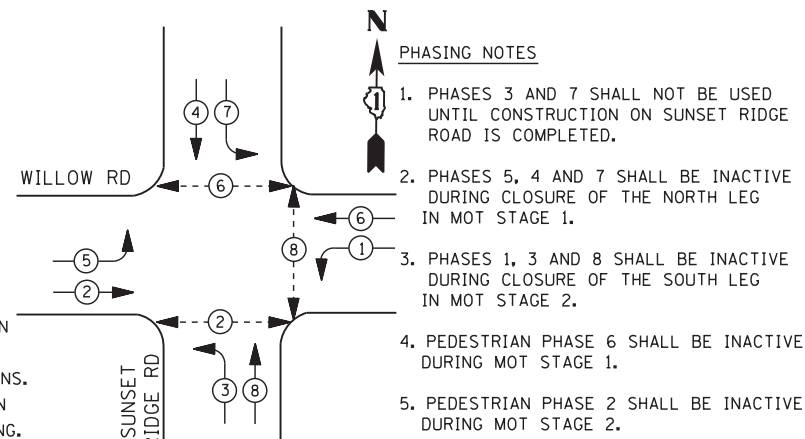
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920, .01, 1518, 2022 & 1922, 4BIR)	COOK	919	376
				CONTRACT NO. 60T35
ILLINOIS FED. AID PROJECT				



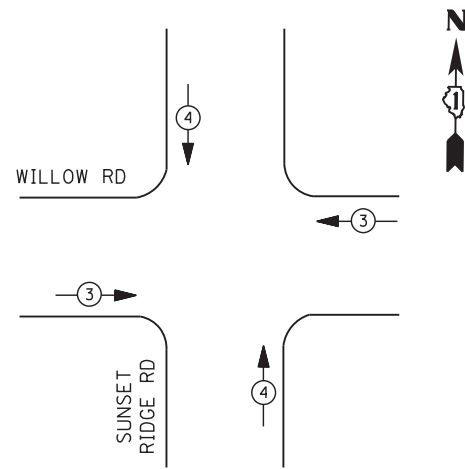
**NOTES FOR TEMPORARY TRAFFIC SIGNALS**

- 1) ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
- 2) ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
- 3) ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE LED AND 12" (300mm) DIAMETER. HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. PEDESTRIAN SIGNALS SHALL INCLUDE SOLID INTERNATIONAL SYMBOLS. PEDESTRIAN SIGNALS WITH COUNTDOWN TIMERS SHALL BE USED WHEN THE EXISTING INSTALLATION UTILIZES COUNTDOWN TYPE OR AS DIRECTED BY THE ENGINEER. COUNTDOWN TYPE PEDESTRIAN SIGNALS ARE NOT TO BE INSTALLED AT RAILROAD INTERSECTIONS. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
- 4) ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
- 5) ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
- 6) THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL, AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
- 7) UNINTERRUPTIBLE POWER SUPPLY (UPS) SYSTEM SHALL BE INSTALLED AND MADE OPERATIONAL AT TEMPORARY TRAFFIC SIGNAL INSTALLATIONS WHERE UPS IS INSTALLED AT THE EXISTING TRAFFIC SIGNAL, TEMPORARY TRAFFIC SIGNALS AT RAILROAD INTERSECTIONS, AND TEMPORARY TRAFFIC SIGNALS AT INTERSECTIONS WITH FIRE STATION ACTUATED EMERGENCY VEHICLE PRE-EMPTION, OR WHEN INDICATED ON THE PLANS.
- 8) TRAFFIC SIGNAL MANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
- 9) DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER.
- 10) WHEN PAN, TILT, ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.

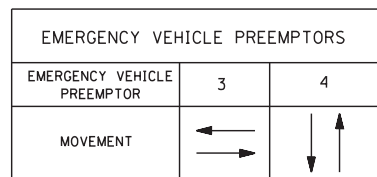
**TEMPORARY CONTROLLER SEQUENCE**



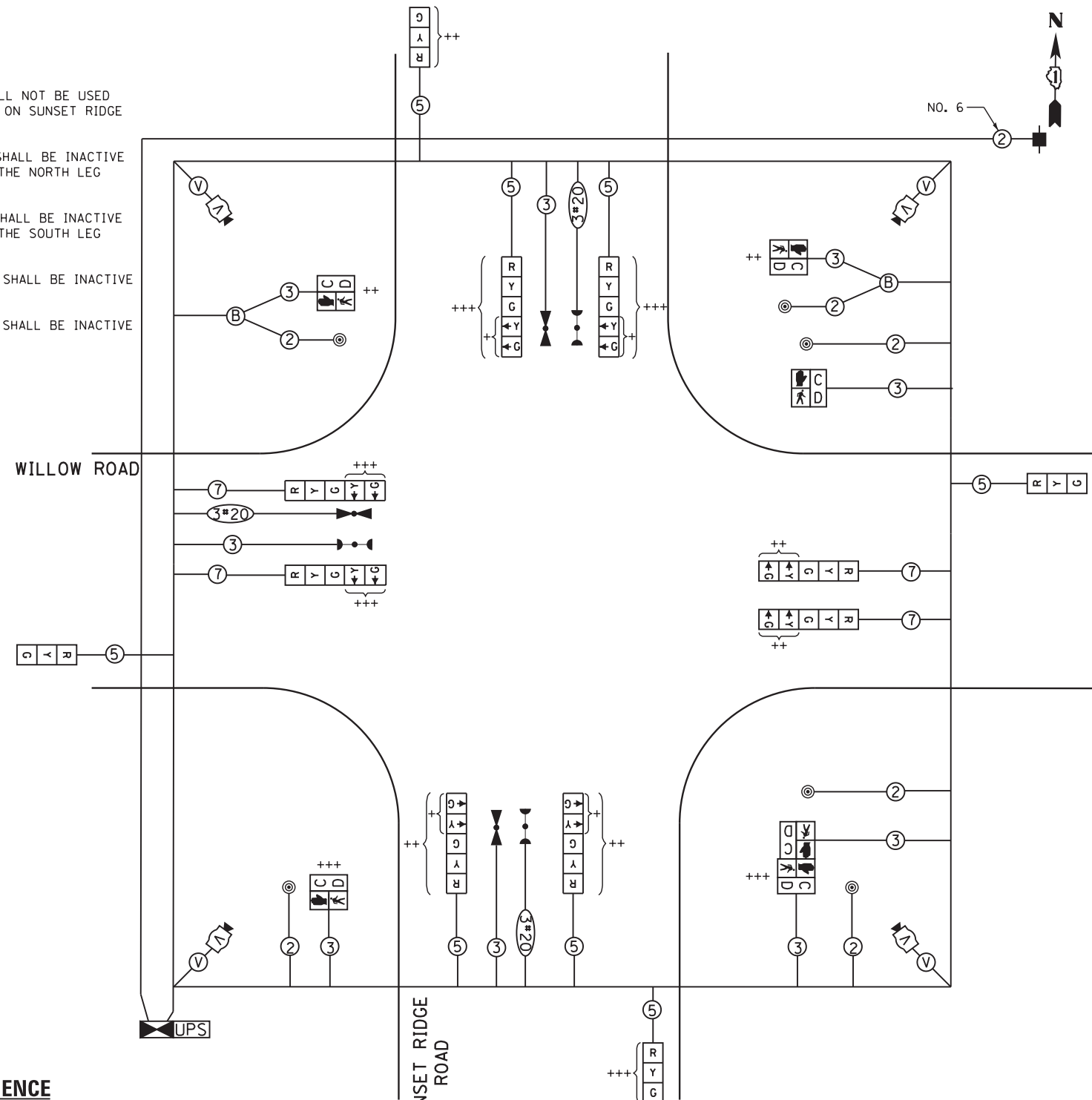
**TEMPORARY PHASE DESIGNATION DIAGRAM**



**TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE**



RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



**TEMPORARY CABLE PLAN**

- + THESE SIGNAL SECTIONS SHALL BE BAGGED & DISCONNECTED UNTIL SUNSET RIDGE ROAD CONSTRUCTION IS COMPLETE OR AS DIRECTED BY THE RESIDENT ENGINEER.
- ++ THESE SIGNAL HEADS OR SECTIONS SHALL BE BAGGED & DISCONNECTED DURING CLOSURE OF THE NORTH LEG IN MOT STAGE 1, WITH THE EXCEPTION OF THE PEDESTRIAN HEADS FOR THE NORTH LEG WHICH WILL BE BAGGED & DISCONNECTED FOR THE ENTIRE MOT STAGE 1.
- +++ THESE SIGNAL HEADS OR SECTIONS SHALL BE BAGGED & DISCONNECTED DURING CLOSURE OF THE SOUTH LEG IN MOT STAGE 2, WITH THE EXCEPTION OF THE PEDESTRIAN HEADS FOR THE SOUTH LEG WHICH WILL BE BAGGED & DISCONNECTED FOR THE ENTIRE MOT STAGE 2.

THE TEMPORARY TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

I. D. O. T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS				
TYPE	NO. LAMPS	WATTAGE		TOTAL WATTAGE
		INCAND.	LED	
SIGNAL (RED)	12	135	17	102
(YELLOW)	12	135	25	75
(GREEN)	12	135	15	45
ARROW	16	135	12	20
PED. SIGNAL	6	90	25	150
CONTROLLER	1	100	100	100
ILLUM. SIGN		252	90	--
VIDEO SYSTEM	1	150	-	150
FLASHER LED				
TOTAL =				642

ENERGY COSTS- BILLED TO: IDOT - DISTRICT 1  
 201 W. CENTER CT.  
 SCHAUMBURG, IL 60196

ENERGY SUPPLY - CONTACT COMED  
 PHONE 630-723-2128

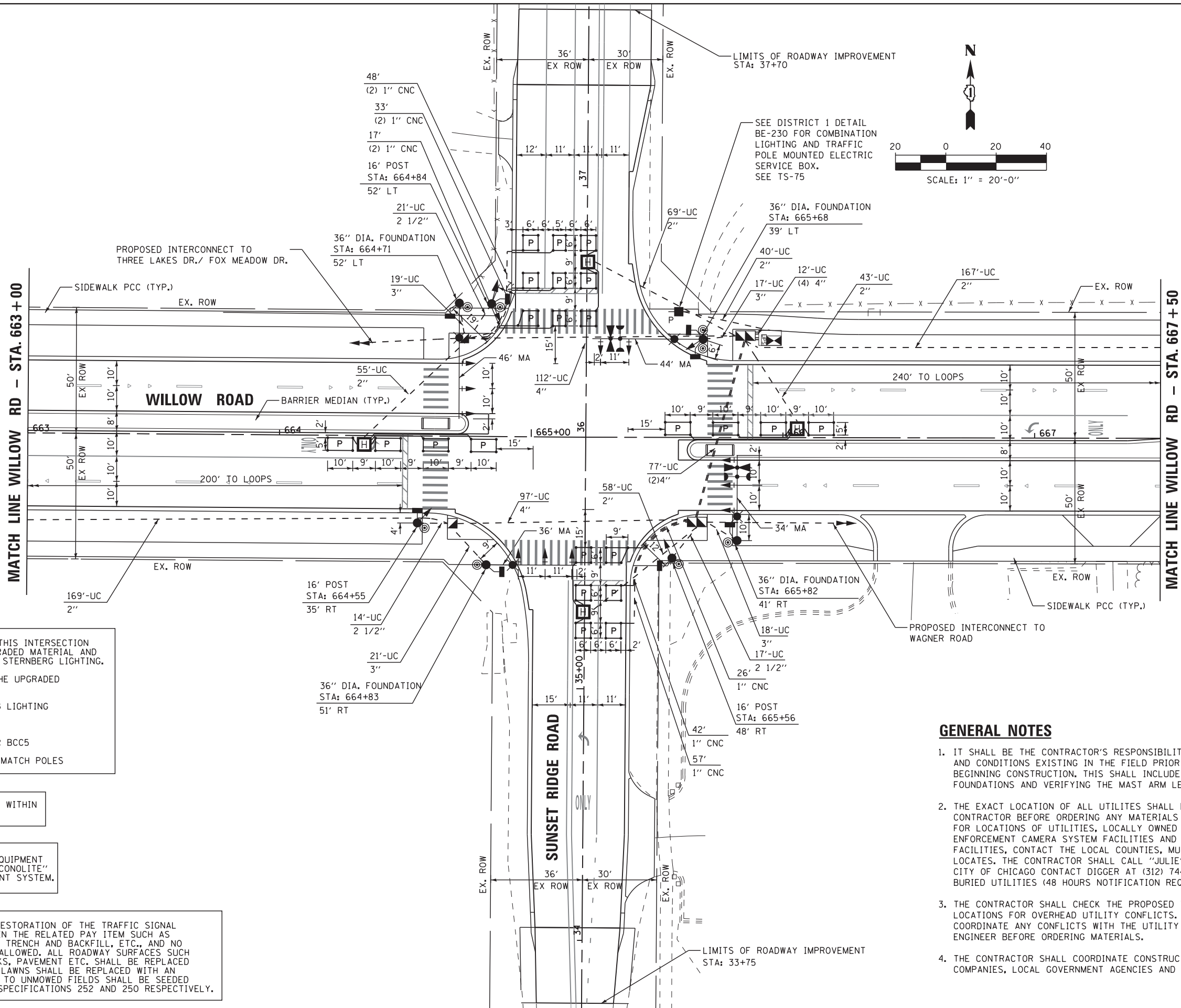
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	PLOT SCALE = *SCALE*	CHECKED -	JDH	REVISED -	
	PLOT DATE = 10/30/2012	DATE -	10/31/2012	REVISED -	

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TEMPORARY CABLE PLAN, TEMPORARY PHASE DESIGNATION DIAGRAM  
 & TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE  
 WILLOW ROAD AT SUNSET RIDGE ROAD

SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920, .01, 1518, 2022 & 1922, 48R)	COOK	919	377
				CONTRACT NO. 60T35
ILLINOIS FED. AID PROJECT				



ALL TRAFFIC SIGNAL POLES AT THIS INTERSECTION SHALL BE CONSTRUCTED OF UPGRADED MATERIAL AND SPECIFICATION AS PRODUCED BY STERNBERG LIGHTING.

THE SPECIFICATIONS FOR THE UPGRADED MATERIAL ARE AS FOLLOWS:

MANUFACTURER: STERNBERG LIGHTING  
 COLOR: VERDE GREEN  
 BASE PLATE: BIRMINGHAM  
 POLES: STRAIGHT FLUTE  
 POST CENTER CAP: BCC OR BCC5  
 ROADWAY ARM: CA  
 STRAPS: VERDE GREEN TO MATCH POLES

ALL LOOPS SHALL BE PERFORMED WITHIN LIMITS OF RECONSTRUCTION

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

**RESTORATION OF WORK AREA.** RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

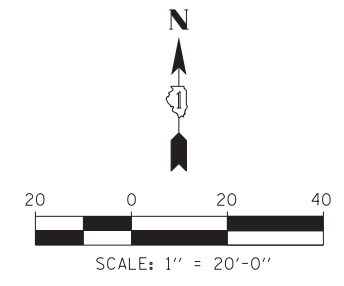
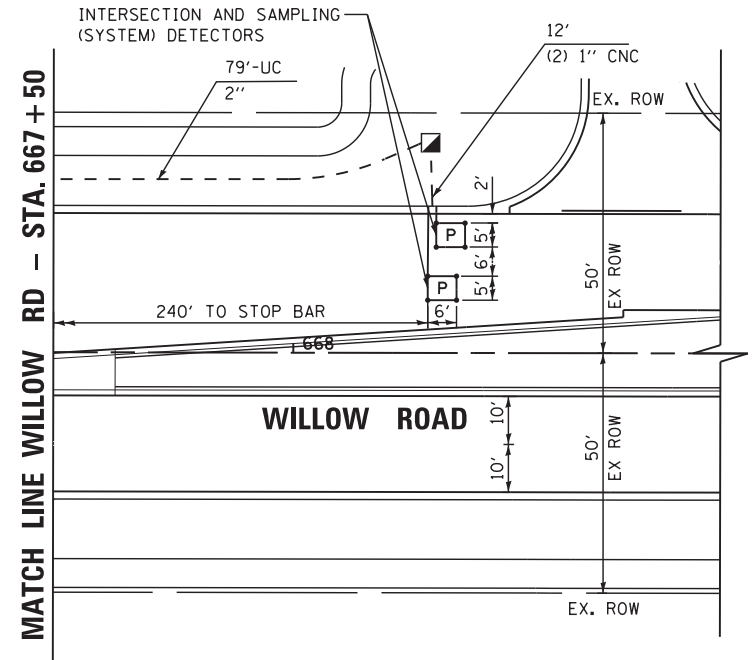
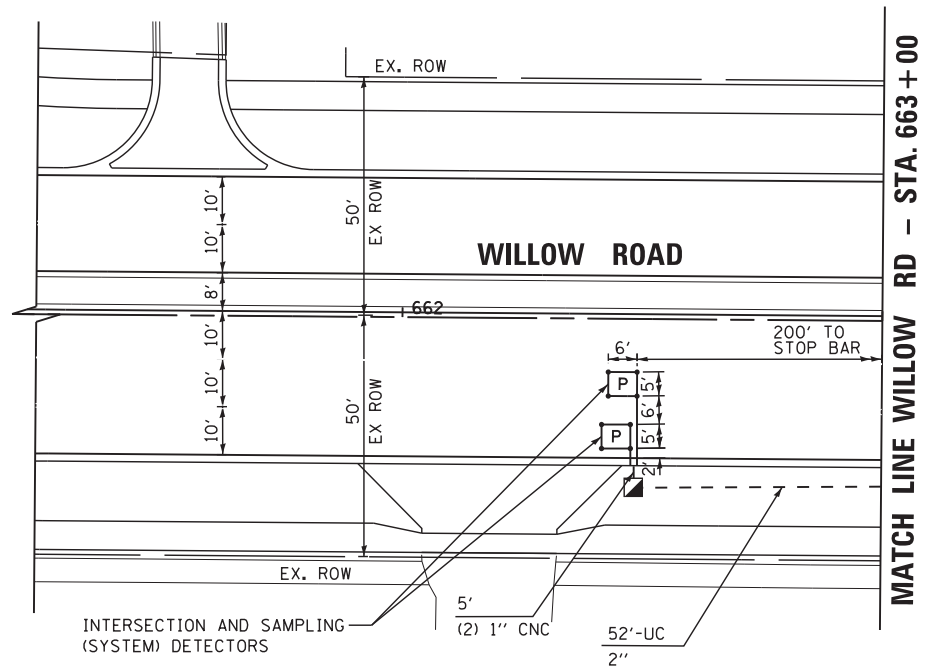
**GENERAL NOTES**

1. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARM LENGTHS.
2. THE EXACT LOCATION OF ALL UTILITES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES. THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811, IN THE CITY OF CHICAGO CONTACT DIGGER AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION REQUIRED).
3. THE CONTRACTOR SHALL CHECK THE PROPOSED TRAFFIC SIGNAL EQUIPMENT LOCATIONS FOR OVERHEAD UTILITY CONFLICTS. THE CONTRACTOR SHALL COORDINATE ANY CONFLICTS WITH THE UTILITY COMPANIES AND THE RESIDENT ENGINEER BEFORE ORDERING MATERIALS.
4. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, LOCAL GOVERNMENT AGENCIES AND IDOT.

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	PLOT SCALE = \$SCALE\$	DRAWN - CDC	REVISED -			305	(1920, 01, 1518, 2022 & 1922, 4BR)	COOK	919	378	
	PLOT DATE = 10/30/2012	CHECKED - JDH	REVISED -			CONTRACT NO. 60T35					
		DATE - 10/31/2012	REVISED -			ILLINOIS FED. AID PROJECT					

TS-30

SCALE: 1"=20' SHEET NO. 1 OF 2 SHEETS STA. TO STA.



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	PLOT DATE = 10/30/2012	DATE - 10/31/2012	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**TRAFFIC SIGNAL MODERNIZATION PLAN  
 WILLOW ROAD AT SUNSET RIDGE ROAD**

SCALE: 1"=20'      SHEET NO. 2 OF 2 SHEETS      STA.      TO STA.

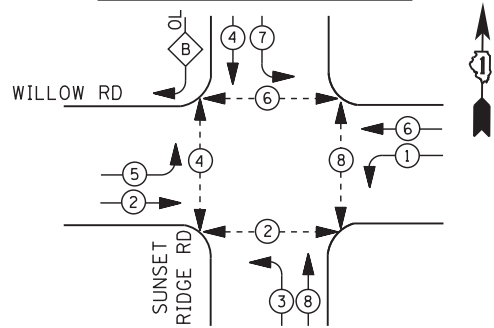
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920, .01, 1518, 2022 & 1922, 4BIR)	COOK	919	379
CONTRACT NO. 60T35				
ILLINOIS FED. AID PROJECT				

**SCHEDULE OF QUANTITIES**

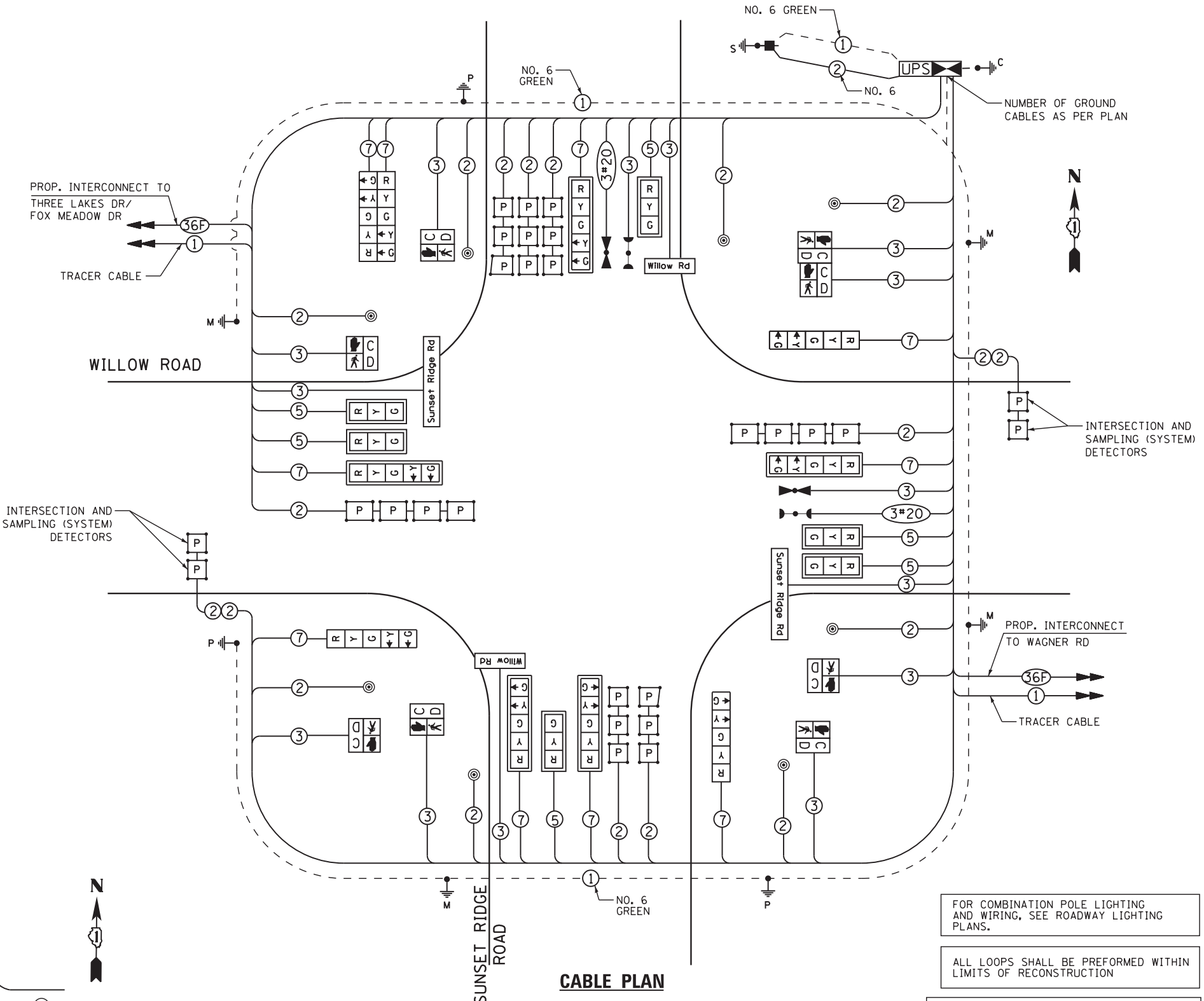
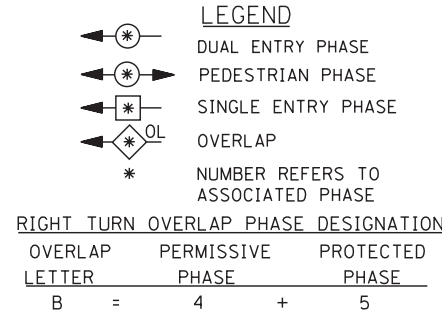
QUANTITY	UNIT	ITEM
1	EACH	SERVICE INSTALLATION - POLE MOUNTED
732	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.
52	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.
75	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.
411	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.
4	EACH	HANDHOLE
4	EACH	HEAVY-DUTY HANDHOLE
2	EACH	DOUBLE HANDHOLE
1	EACH	TRANSCEIVER - FIBER OPTIC
1274	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C
1635	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
1197	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
1985	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C
2460	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
60	FOOT	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C
669	FOOT	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C
12	FOOT	CONCRETE FOUNDATION, TYPE A
4	FOOT	CONCRETE FOUNDATION, TYPE C
48	FOOT	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER
6	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED
3	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED
5	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED
1	EACH	SIGNAL HEAD, LED, 2-FACE, 5 SECTION, BRACKET MOUNTED
6	EACH	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
1	EACH	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
11	EACH	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM
11	EACH	INDUCTIVE LOOP DETECTOR
1007	FOOT	PERFORMED DETECTOR LOOP
* 2	EACH	LIGHT DETECTOR
* 1	EACH	LIGHT DETECTOR AMPLIFIER
8	EACH	PEDESTRIAN PUSH-BUTTON
1	EACH	TEMPORARY TRAFFIC SIGNAL INSTALLATION
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
10	EACH	REMOVE EXISTING HANDHOLE
9	EACH	REMOVE EXISTING CONCRETE FOUNDATION
* 305	FOOT	EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO 20 3/C
1	EACH	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL
1	EACH	UNINTERRUPTIBLE POWER SUPPLY, SPECIAL
1	EACH	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 34 FT. (SPECIAL)
1	EACH	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 36 FT. (SPECIAL)
1	EACH	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 44 FT. (SPECIAL)
1	EACH	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 46 FT. (SPECIAL)
1	EACH	TEMPORARY TRAFFIC SIGNAL TIMING
3	EACH	TRAFFIC SIGNAL POST, ALUMINUM 16 FT. (SPECIAL)
* 4	EACH	ILLUMINATED STREET NAME SIGN

\* 100% VILLAGE OF NORTHFIELD COST

**CONTROLLER SEQUENCE**



**PHASE DESIGNATION DIAGRAM**



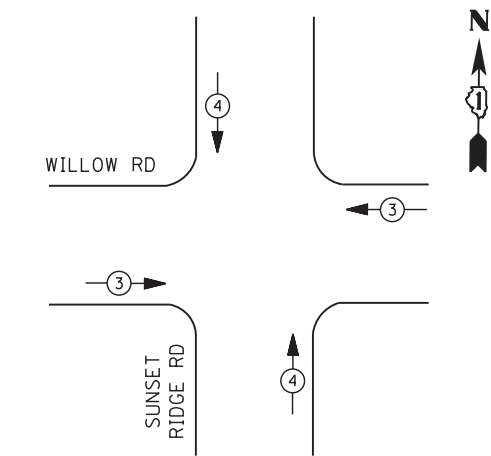
**CABLE PLAN**

FOR COMBINATION POLE LIGHTING AND WIRING, SEE ROADWAY LIGHTING PLANS.

ALL LOOPS SHALL BE PERFORMED WITHIN LIMITS OF RECONSTRUCTION

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



**EMERGENCY VEHICLE PREEMPTION SEQUENCE**

**EMERGENCY VEHICLE PREEMPTORS**

EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	← →	↑ ↓

THE END OF THE TRACER CABLE SHALL BE CONTINUOUS AND EXTEND INTO THE CONTROLLER CABINET.

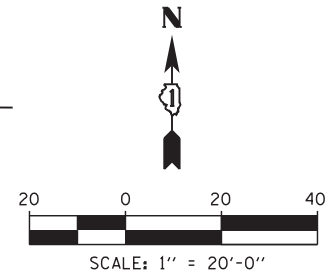
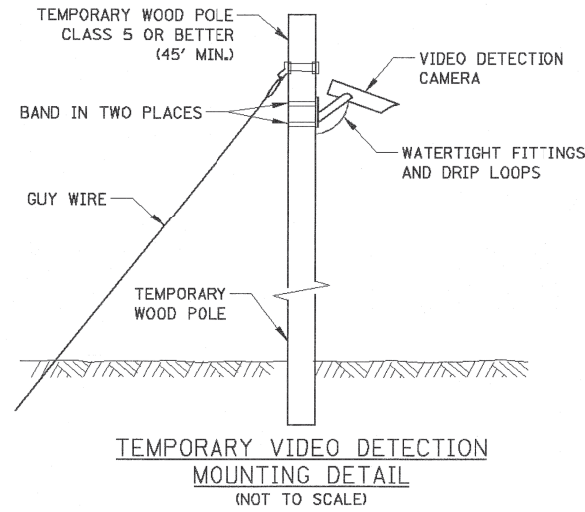
I. D. O. T.  
 TRAFFIC SIGNAL INSTALLATION  
 ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. LAMPS	WATTAGE		% OPERATIONS	TOTAL WATTAGE
		INCAND.	LED		
SIGNAL (RED)	16	135	17	0.50	136
(YELLOW)	16	135	25	0.25	100
(GREEN)	16	135	15	0.25	60
ARROW	20	135	12	0.10	24
PED. SIGNAL	8	90	25	1.00	200
CONTROLLER	1	100	100	1.00	100
ILLUM. SIGN	4	--	90	0.50	180
VIDEO SYSTEM		150	-	1.00	--
TOTAL =					800

ENERGY COSTS- BILLED TO: IDOT - DISTRICT 1  
 201 W. CENTER CT.  
 SCHAUMBURG, IL 60196

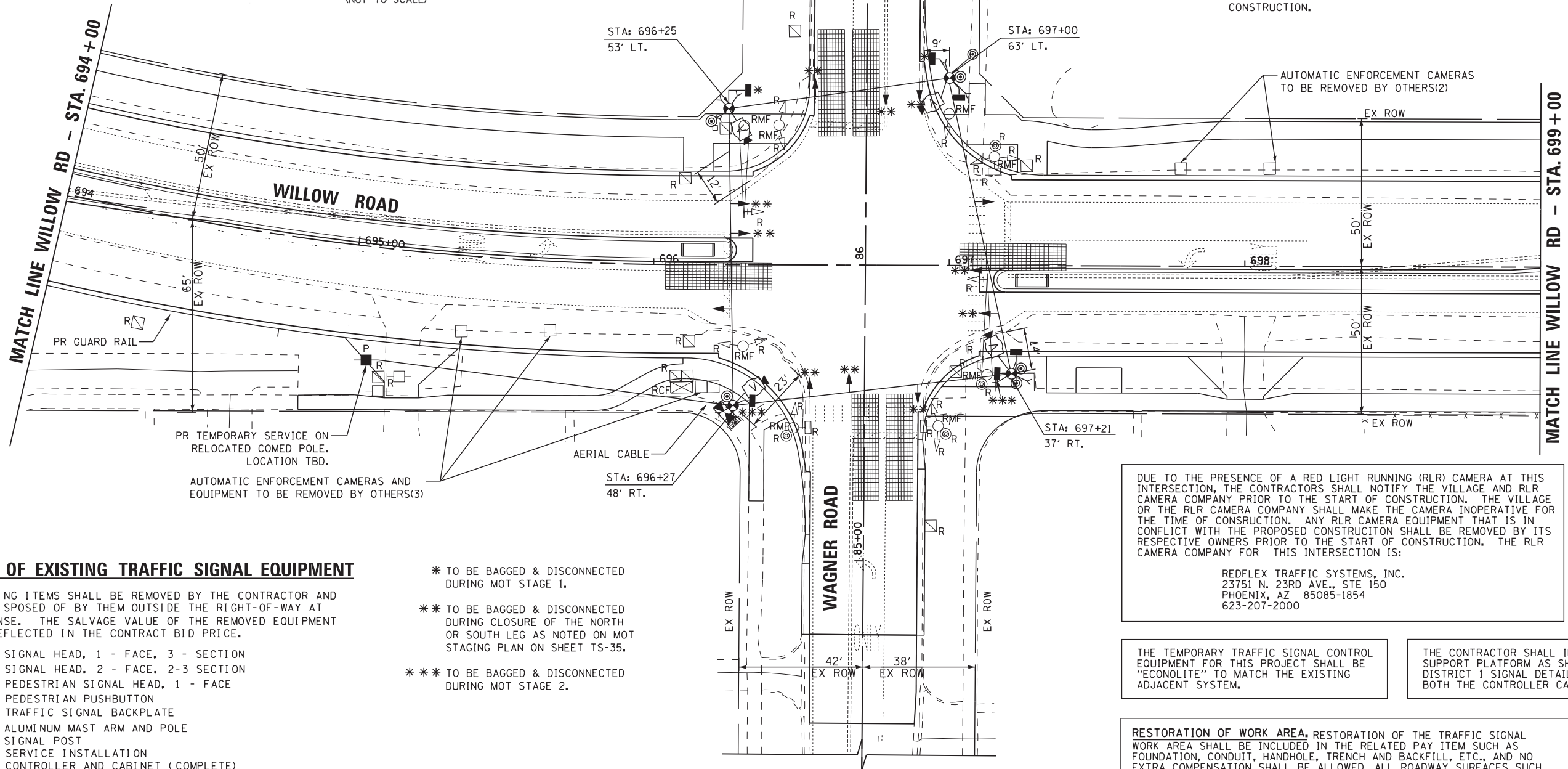
ENERGY SUPPLY - CONTACT COMED  
 PHONE 630-723-2128





**GENERAL NOTES**

1. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARM LENGTHS.
2. THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES. THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811, IN THE CITY OF CHICAGO CONTACT DIGGER AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION REQUIRED).
3. THE CONTRACTOR SHALL CHECK THE PROPOSED TRAFFIC SIGNAL EQUIPMENT LOCATIONS FOR OVERHEAD UTILITY CONFLICTS. THE CONTRACTOR SHALL COORDINATE ANY CONFLICTS WITH THE UTILITY COMPANIES AND THE RESIDENT ENGINEER BEFORE ORDERING MATERIALS.
4. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, LOCAL GOVERNMENT AGENCIES AND IDOT.
5. TEMPORARY BARREL MOUNTED PUSHBUTTONS ON POSTS TO BE INSTALLED AS DIRECTED BY THE RESIDENT ENGINEER TO ACCOMMODATE STAGING OF SIDEWALK CONSTRUCTION.



**REMOVAL OF EXISTING TRAFFIC SIGNAL EQUIPMENT**

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 4 EACH SIGNAL HEAD, 1 - FACE, 3 - SECTION
- 4 EACH SIGNAL HEAD, 2 - FACE, 2-3 SECTION
- 4 EACH PEDESTRIAN SIGNAL HEAD, 1 - FACE
- 4 EACH PEDESTRIAN PUSHBUTTON
- 2 EACH TRAFFIC SIGNAL BACKPLATE
- 2 EACH ALUMINUM MAST ARM AND POLE
- 6 EACH SIGNAL POST
- 1 EACH SERVICE INSTALLATION
- 1 EACH CONTROLLER AND CABINET (COMPLETE)

- \* TO BE BAGGED & DISCONNECTED DURING MOT STAGE 1.
- \*\* TO BE BAGGED & DISCONNECTED DURING CLOSURE OF THE NORTH OR SOUTH LEG AS NOTED ON MOT STAGING PLAN ON SHEET TS-35.
- \*\*\* TO BE BAGGED & DISCONNECTED DURING MOT STAGE 2.

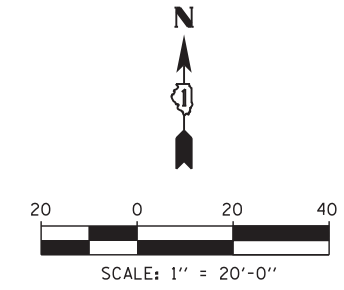
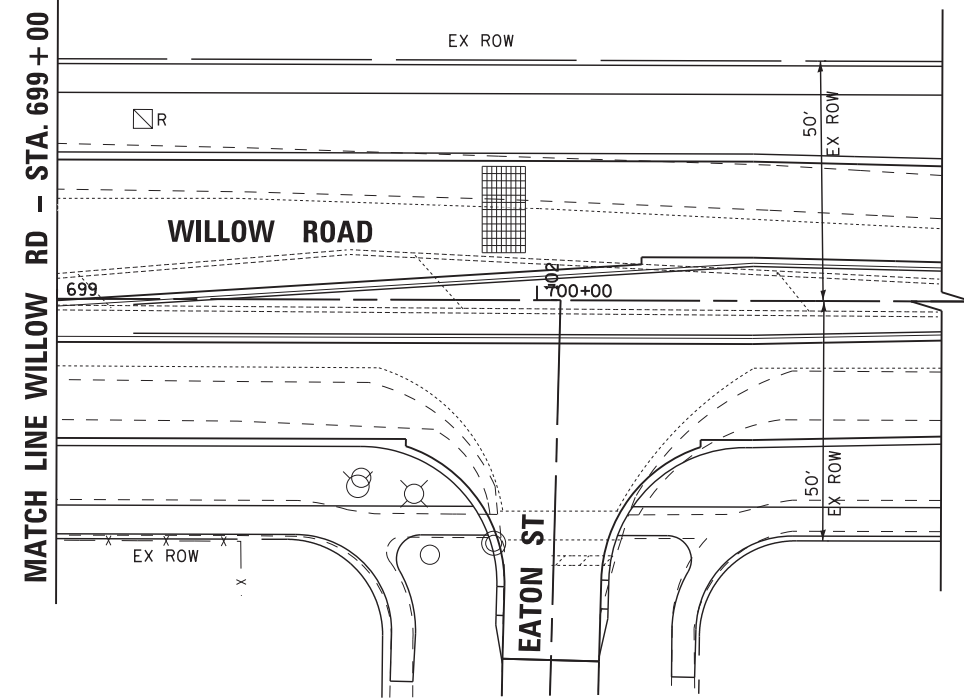
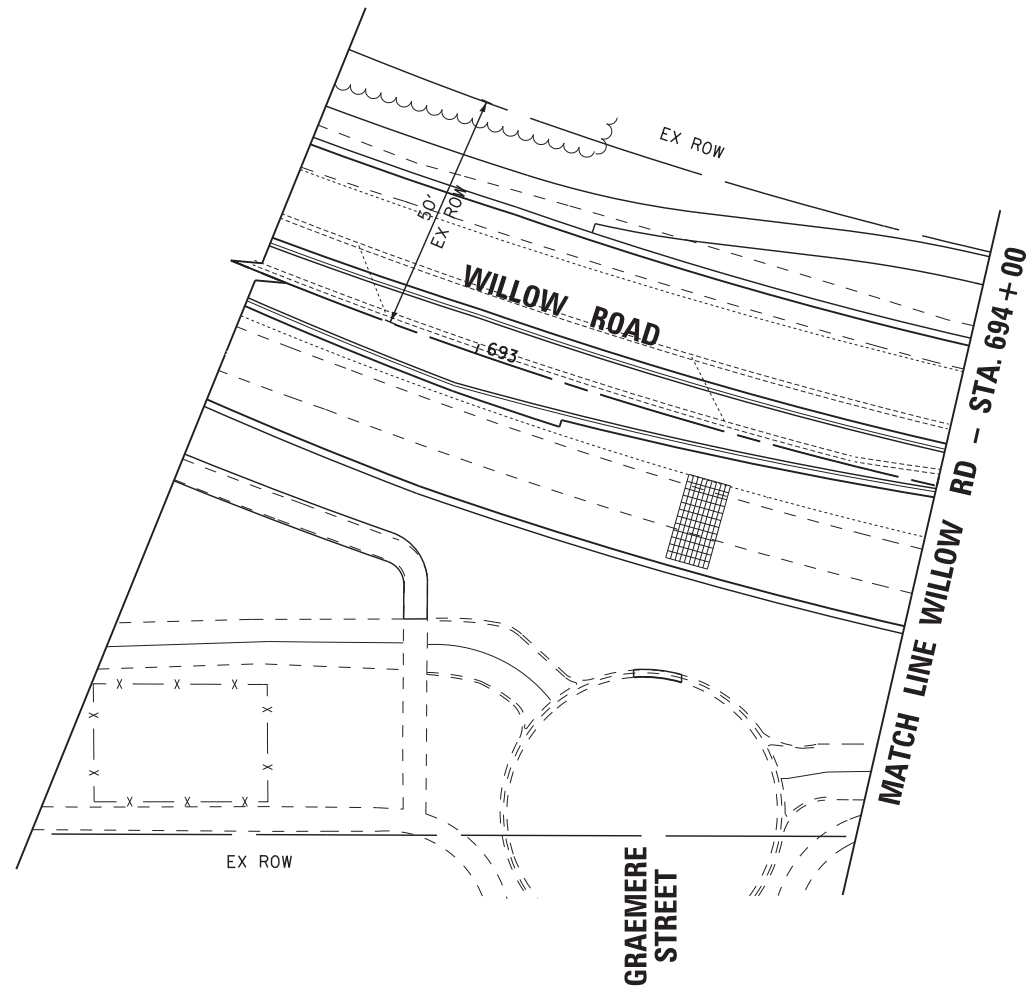
DUE TO THE PRESENCE OF A RED LIGHT RUNNING (RLR) CAMERA AT THIS INTERSECTION, THE CONTRACTORS SHALL NOTIFY THE VILLAGE AND RLR CAMERA COMPANY PRIOR TO THE START OF CONSTRUCTION. THE VILLAGE OR THE RLR CAMERA COMPANY SHALL MAKE THE CAMERA INOPERATIVE FOR THE TIME OF CONSTRUCTION. ANY RLR CAMERA EQUIPMENT THAT IS IN CONFLICT WITH THE PROPOSED CONSTRUCTION SHALL BE REMOVED BY ITS RESPECTIVE OWNERS PRIOR TO THE START OF CONSTRUCTION. THE RLR CAMERA COMPANY FOR THIS INTERSECTION IS:

REFLEX TRAFFIC SYSTEMS, INC.  
 23751 N. 23RD AVE., STE 150  
 PHOENIX, AZ 85085-1854  
 623-207-2000

THE TEMPORARY TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

THE CONTRACTOR SHALL INSTALL A WOOD SUPPORT PLATFORM AS SHOWN ON THE DISTRICT 1 SIGNAL DETAIL SHEETS FOR BOTH THE CONTROLLER CABINET AND UPS.

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



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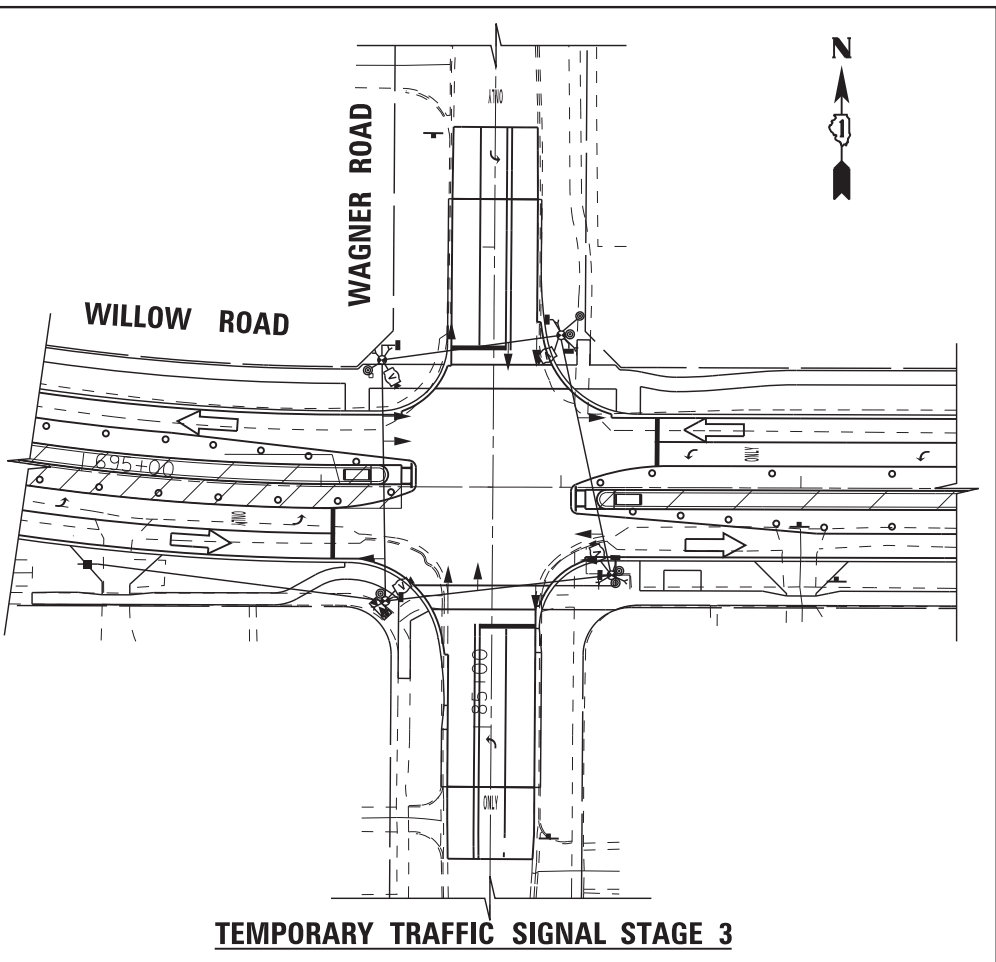
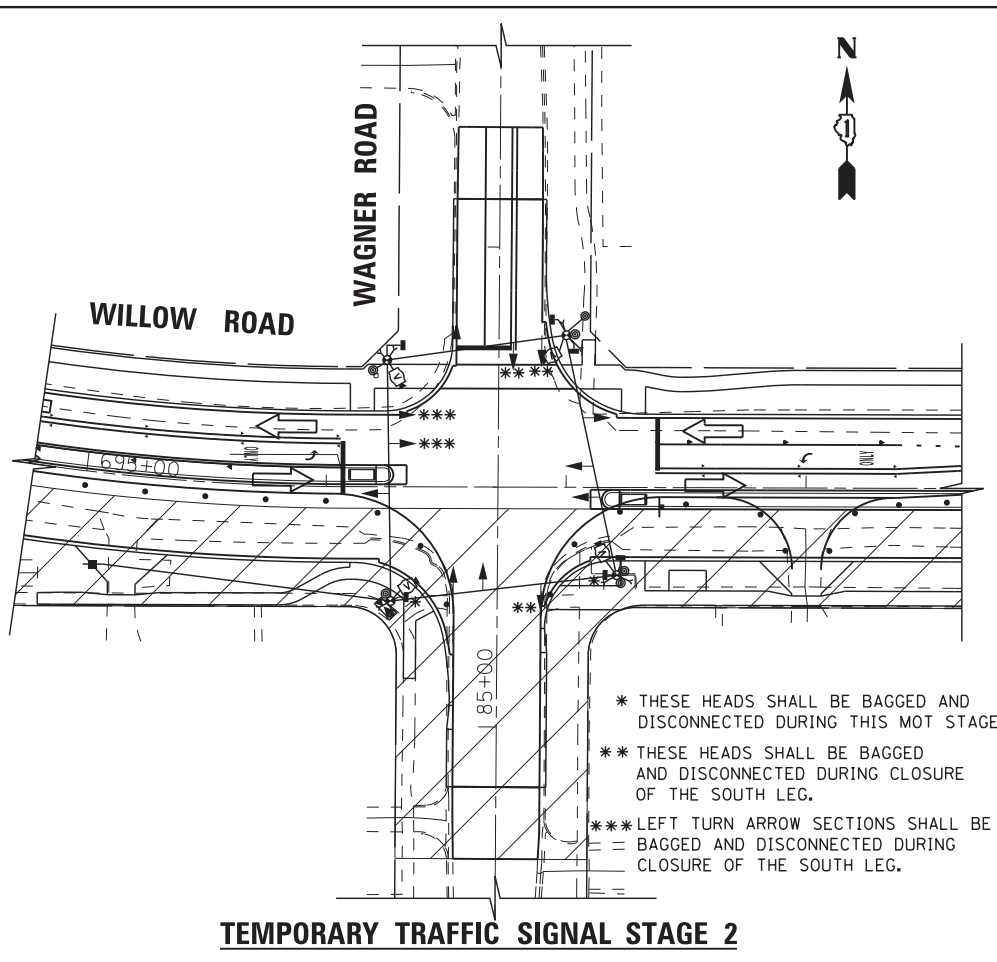
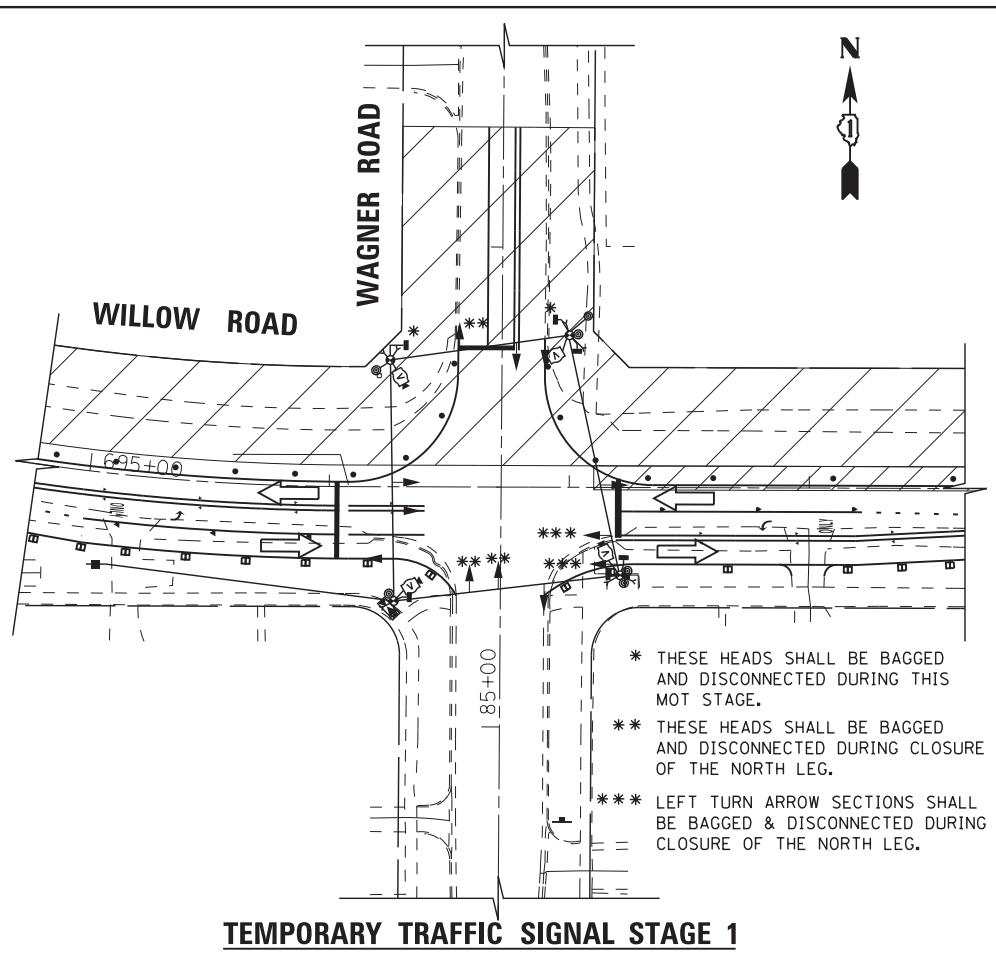
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TEMPORARY TRAFFIC SIGNAL INSTALLATION & REMOVAL PLAN  
 MAINTENANCE OF TRAFFIC STAGE 1  
 WILLOW ROAD AT WAGNER ROAD

SCALE: 1"=20' SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920, .01, 1518, 2022 & 1922, 481R)	COOK	919	382
				CONTRACT NO. 60T35
ILLINOIS FED. AID PROJECT				

TS-34



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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TEMPORARY TRAFFIC SIGNAL INSTALLATION  
 M.O.T. STAGING PLAN  
 WILLOW ROAD AT WAGNER ROAD

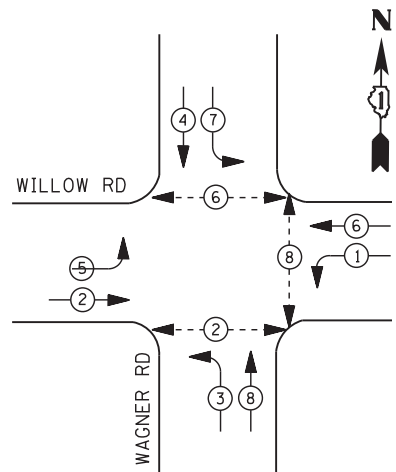
SCALE: 1"=40' SHEET NO. 1 OF X SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920, .01, 1518, 2022 & 1922, 4BIR)	COOK	919	383
				CONTRACT NO. 60T35
ILLINOIS FED. AID PROJECT				

**NOTES FOR TEMPORARY TRAFFIC SIGNALS**

- 1) ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
- 2) ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1. INSTALLED IN A NEMA TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
- 3) ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE LED AND 12" (300mm) DIAMETER. HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. PEDESTRIAN SIGNALS SHALL INCLUDE SOLID INTERNATIONAL SYMBOLS. PEDESTRIAN SIGNALS WITH COUNTDOWN TIMERS SHALL BE USED WHEN THE EXISTING INSTALLATION UTILIZES COUNTDOWN TYPE OR AS DIRECTED BY THE ENGINEER. COUNTDOWN TYPE PEDESTRIAN SIGNALS ARE NOT TO BE INSTALLED AT RAILROAD INTERSECTIONS. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
- 4) ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES. RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
- 5) ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
- 6) THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL, AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
- 7) UNINTERRUPTIBLE POWER SUPPLY (UPS) SYSTEM SHALL BE INSTALLED AND MADE OPERATIONAL AT TEMPORARY TRAFFIC SIGNAL INSTALLATIONS WHERE UPS IS INSTALLED AT THE EXISTING TRAFFIC SIGNAL, TEMPORARY TRAFFIC SIGNALS AT RAILROAD INTERSECTIONS, AND TEMPORARY TRAFFIC SIGNALS AT INTERSECTIONS WITH FIRE STATION ACTUATED EMERGENCY VEHICLE PRE-EMPTION, OR WHEN INDICATED ON THE PLANS.
- 8) TRAFFIC SIGNAL MANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
- 9) DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER.
- 10) WHEN PAN, TILT, ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.

**TEMPORARY CONTROLLER SEQUENCE**



**LEGEND**

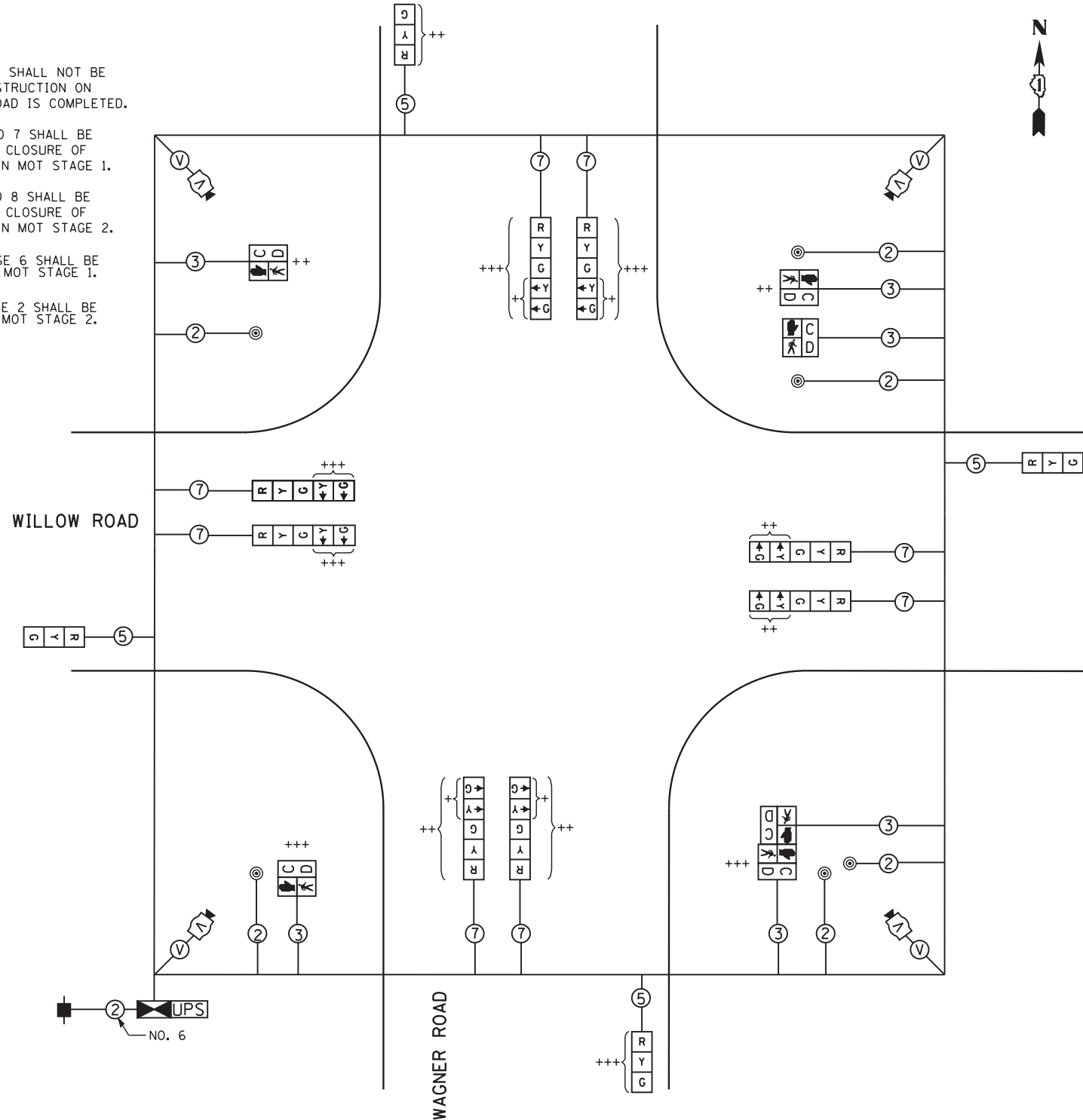
- DUAL ENTRY PHASE
- PEDESTRIAN PHASE
- SINGLE ENTRY PHASE
- OVERLAP
- \* NUMBER REFERS TO ASSOCIATED PHASE

**TEMPORARY PHASE DESIGNATION DIAGRAM**

**PHASING NOTES**

1. PHASES 3 AND 7 SHALL NOT BE USED UNTIL CONSTRUCTION ON SUNSET RIDGE ROAD IS COMPLETED.
2. PHASES 5, 4 AND 7 SHALL BE INACTIVE DURING CLOSURE OF THE NORTH LEG IN MOT STAGE 1.
3. PHASES 1, 3 AND 8 SHALL BE INACTIVE DURING CLOSURE OF THE SOUTH LEG IN MOT STAGE 2.
4. PEDESTRIAN PHASE 6 SHALL BE INACTIVE DURING MOT STAGE 1.
5. PEDESTRIAN PHASE 2 SHALL BE INACTIVE DURING MOT STAGE 2.

**WILLOW ROAD**



**TEMPORARY CABLE PLAN**

+ THESE SIGNAL SECTIONS SHALL BE BAGGED & DISCONNECTED UNTIL WAGNER ROAD CONSTRUCTION IS COMPLETE OR AS DIRECTED BY THE RESIDENT ENGINEER.

++ THESE SIGNAL HEADS OR SECTIONS SHALL BE BAGGED & DISCONNECTED DURING CLOSURE OF THE NORTH LEG IN MOT STAGE 1, WITH THE EXCEPTION OF THE PEDESTRIAN HEADS FOR THE NORTH LEG WHICH WILL BE BAGGED & DISCONNECTED FOR THE ENTIRE MOT STAGE 1.

+++ THESE SIGNAL HEADS OR SECTIONS SHALL BE BAGGED & DISCONNECTED DURING CLOSURE OF THE SOUTH LEG IN MOT STAGE 2, WITH THE EXCEPTION OF THE PEDESTRIAN HEADS FOR THE SOUTH LEG WHICH WILL BE BAGGED & DISCONNECTED FOR THE ENTIRE MOT STAGE 2.

THE TEMPORARY TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

TS-36

I. D. O. T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					
TYPE	NO. LAMPS	WATTAGE		% OPERATIONS	TOTAL WATTAGE
		INCAND.	LED		
SIGNAL (RED)	12	135	17	0.50	102
(YELLOW)	12	135	25	0.25	75
(GREEN)	12	135	15	0.25	45
ARROW	16	135	12	0.10	20
PED. SIGNAL	6	90	25	1.00	150
CONTROLLER	1	100	100	1.00	100
ILLUM. SIGN		252	25	0.05	--
VIDEO SYSTEM	1	150	-	1.00	150
FLASHER LED					
TOTAL =					642

ENERGY COSTS- BILLED TO: IDOT - DISTRICT 1  
 201 W. CENTER CT.  
 SCHAUMBURG, IL 60196

ENERGY SUPPLY - CONTACT COMED  
 PHONE 630-723-2128

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TEMPORARY CABLE PLAN & TEMPORARY PHASE DESIGNATION DIAGRAM  
 WILLOW ROAD AT WAGNER ROAD

SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.

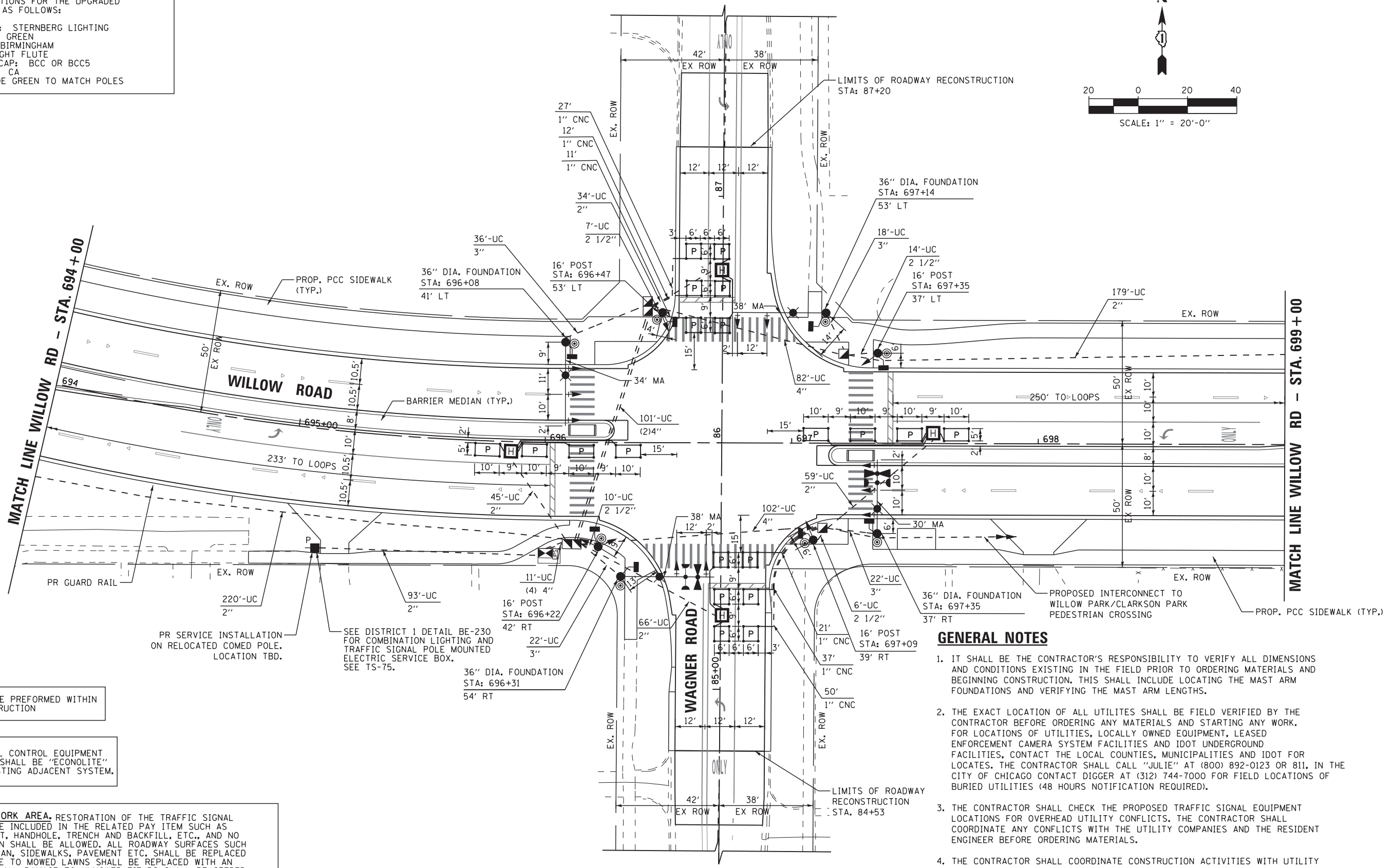
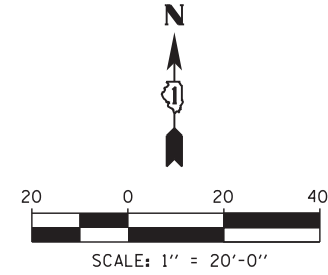
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920, .01, 1518, 2022 & 1922, 4BIR	COOK	919	384
CONTRACT NO. 60T35				
ILLINOIS FED. AID PROJECT				



ALL TRAFFIC SIGNAL POLES AT THIS INTERSECTION SHALL BE CONSTRUCTED OF UPGRADED MATERIAL AND SPECIFICATION AS PRODUCED BY STERNBERG LIGHTING.

THE SPECIFICATIONS FOR THE UPGRADED MATERIAL ARE AS FOLLOWS:

MANUFACTURER: STERNBERG LIGHTING  
 COLOR: VERDE GREEN  
 BASE PLATE: BIRMINGHAM  
 POLES: STRAIGHT FLUTE  
 POST CENTER CAP: BCC OR BCC5  
 ROADWAY ARM: CA  
 STRAPS: VERDE GREEN TO MATCH POLES



ALL LOOPS SHALL BE PERFORMED WITHIN LIMITS OF RECONSTRUCTION

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

SEE DISTRICT 1 DETAIL BE-230 FOR COMBINATION LIGHTING AND TRAFFIC SIGNAL POLE MOUNTED ELECTRIC SERVICE BOX. SEE TS-75.

**GENERAL NOTES**

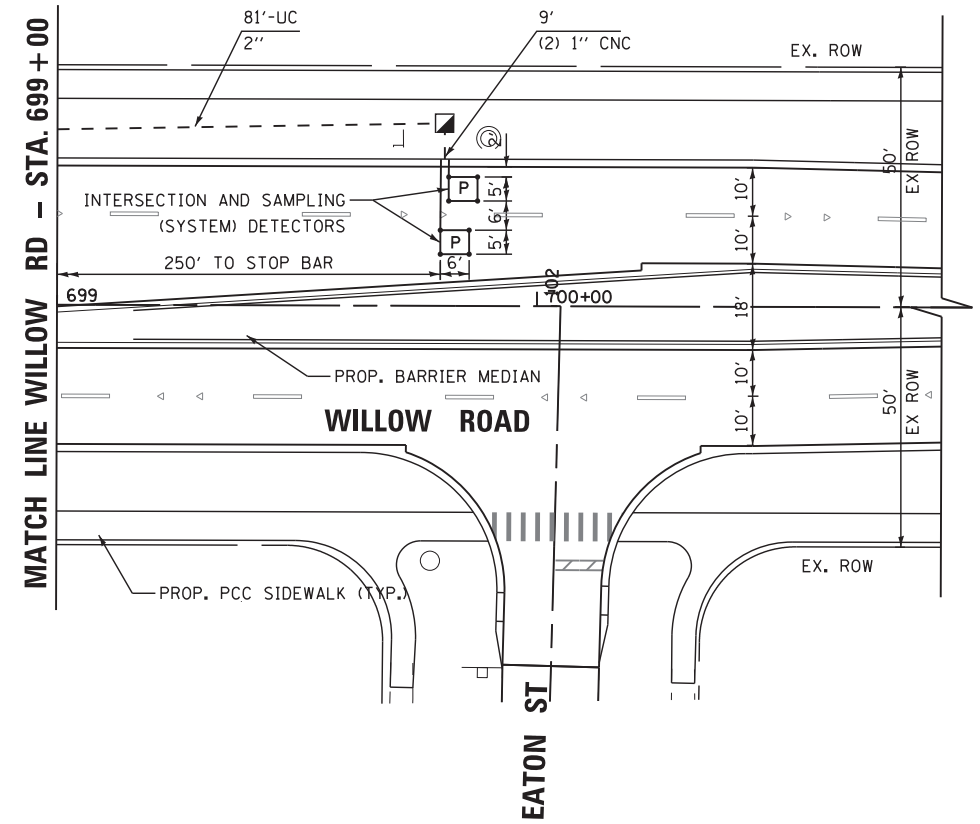
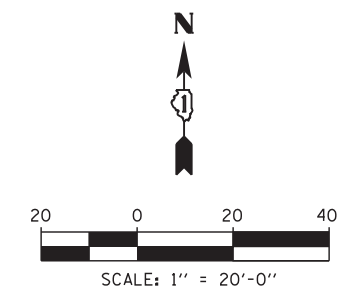
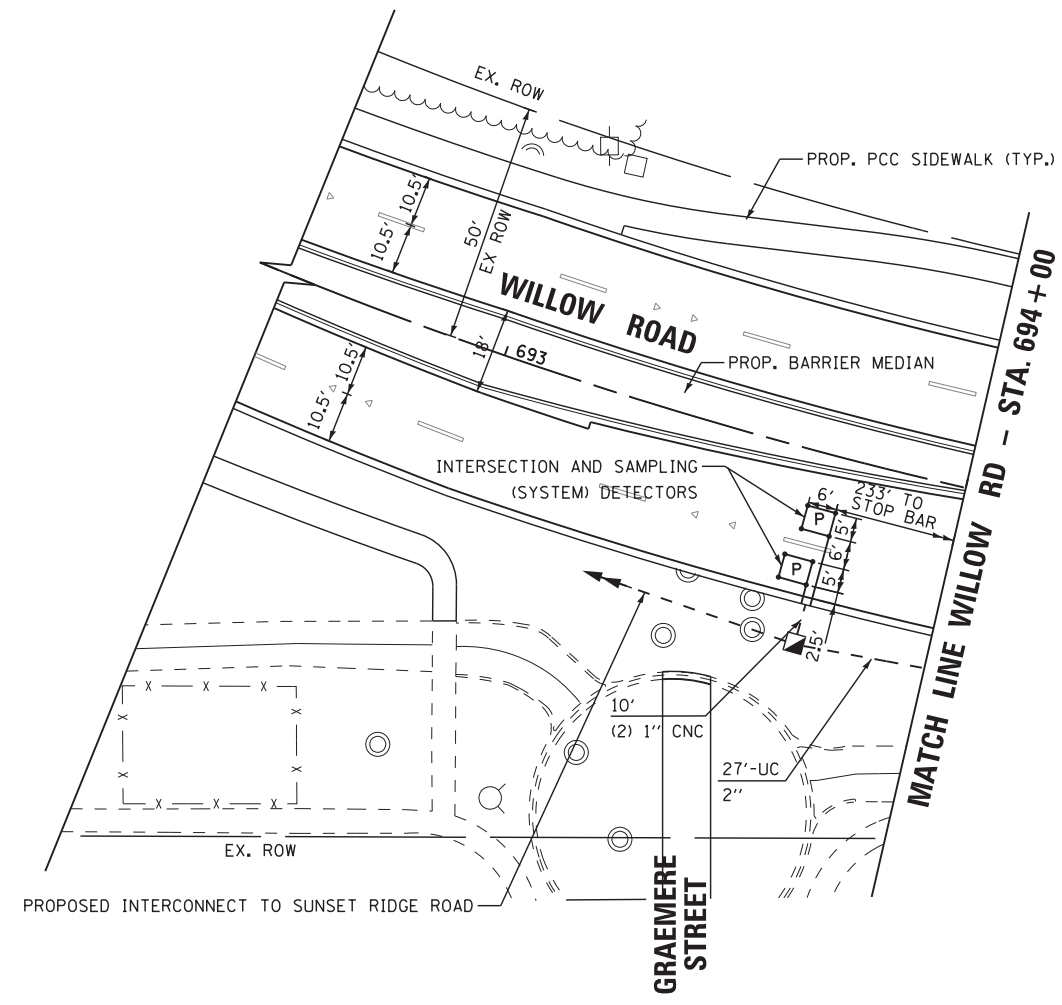
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARM LENGTHS.
- THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES. THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811, IN THE CITY OF CHICAGO CONTACT DIGGER AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION REQUIRED).
- THE CONTRACTOR SHALL CHECK THE PROPOSED TRAFFIC SIGNAL EQUIPMENT LOCATIONS FOR OVERHEAD UTILITY CONFLICTS. THE CONTRACTOR SHALL COORDINATE ANY CONFLICTS WITH THE UTILITY COMPANIES AND THE RESIDENT ENGINEER BEFORE ORDERING MATERIALS.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, LOCAL GOVERNMENT AGENCIES AND IDOT.

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	PLOT DATE = 10/30/2012	DATE -	10/31/2012	REVISED -	

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**TRAFFIC SIGNAL MODERNIZATION PLAN  
 WILLOW ROAD AT WAGNER ROAD**  
 SCALE: 1"=20' SHEET NO. 1 OF 2 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920, .01, 1518, 2022 & 1922, 4BR)	COOK	919	385
CONTRACT NO. 60T35				
ILLINOIS FED. AID PROJECT				



TS-38

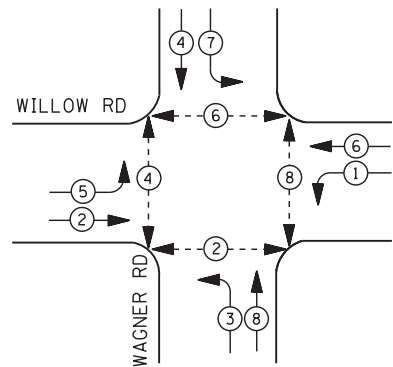
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PLOT SCALE = *SCALE*	CHECKED - JDH	REVISED -	SCALE: 1"=20'			SHEET NO. 2 OF 2 SHEETS	STA. TO STA.	CONTRACT NO. 60T35		
PLOT DATE = 10/30/2012	DATE - 10/31/2012	REVISED -						ILLINOIS FED. AID PROJECT		

**SCHEDULE OF QUANTITIES**

QUANTITY	UNIT	ITEM
1	EACH	SERVICE INSTALLATION - POLE MOUNTED
804	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.
37	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.
98	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.
430	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.
4	EACH	HANDHOLE
4	EACH	HEAVY-DUTY HANDHOLE
2	EACH	DOUBLE HANDHOLE
1	EACH	TRANSCEIVER - FIBER OPTIC
1303	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C
1679	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
1270	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
1523	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C
2407	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
113	FOOT	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C
769	FOOT	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C
16	FOOT	CONCRETE FOUNDATION, TYPE A
4	FOOT	CONCRETE FOUNDATION, TYPE C
44	FOOT	CONCRETE FOUNDATION, TYPE E, 36-INCH DIAMETER
6	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED
3	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED
4	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED
1	EACH	SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED
8	EACH	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
10	EACH	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM
10	EACH	INDUCTIVE LOOP DETECTOR
901	FOOT	PREFORMED DETECTOR LOOP
* 2	EACH	LIGHT DETECTOR
* 1	EACH	LIGHT DETECTOR AMPLIFIER
8	EACH	PEDESTRIAN PUSH-BUTTON
1	EACH	TEMPORARY TRAFFIC SIGNAL INSTALLATION
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
10	EACH	REMOVE EXISTING HANDHOLE
9	EACH	REMOVE EXISTING CONCRETE FOUNDATION
* 320	FOOT	EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO 20 3/C
1	EACH	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL
1	EACH	UNINTERRUPTIBLE POWER SUPPLY, SPECIAL
1	EACH	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 30 FT. (SPECIAL)
1	EACH	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 34 FT. (SPECIAL)
2	EACH	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 38 FT. (SPECIAL)
1	EACH	TEMPORARY TRAFFIC SIGNAL TIMING
4	EACH	TRAFFIC SIGNAL POST, ALUMINUM 16FT. (SPECIAL)
* 4	EACH	ILLUMINATED STREET NAME SIGN

\* 100% VILLAGE COST OF NORTHFIELD COST

**CONTROLLER SEQUENCE**

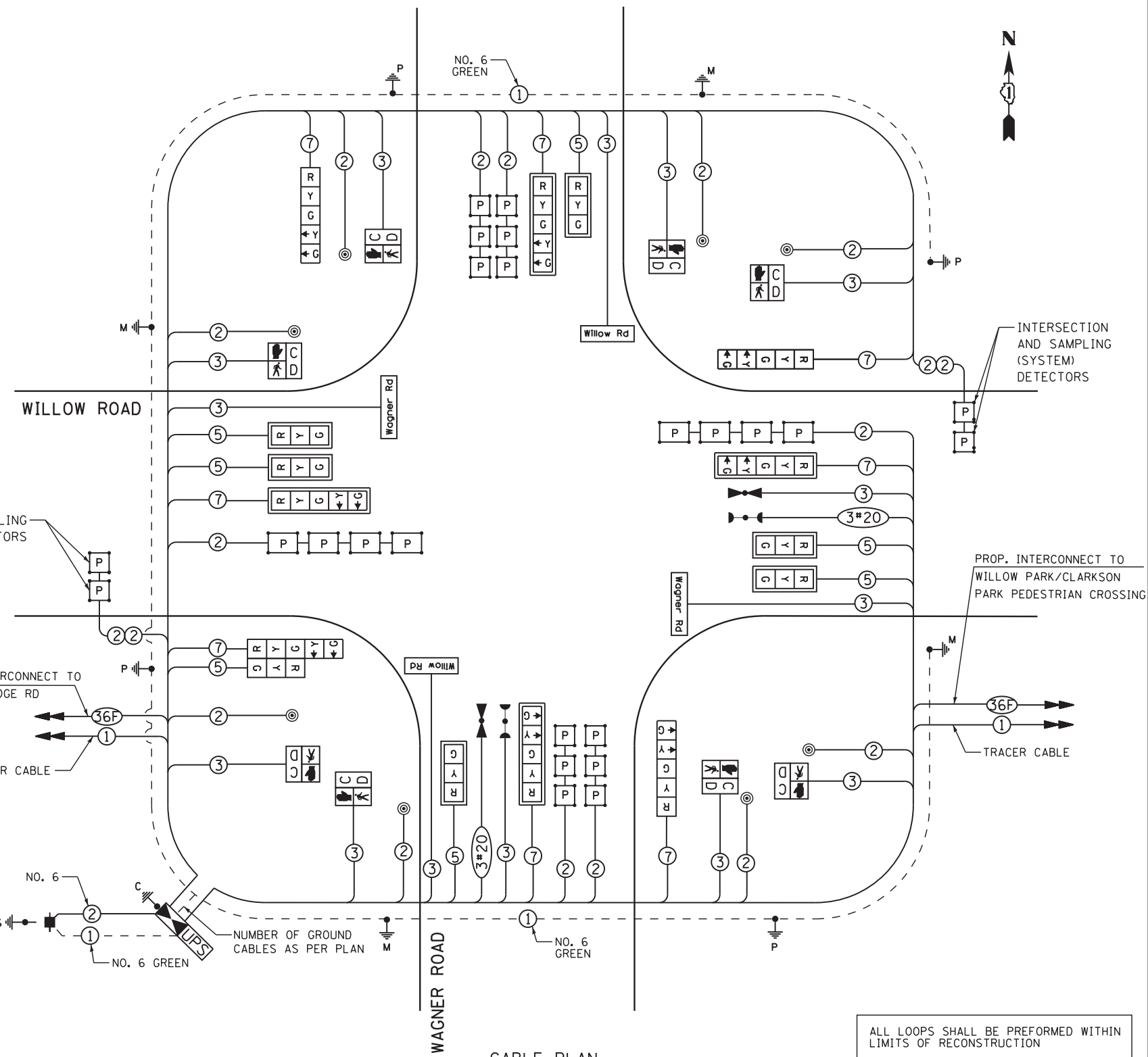
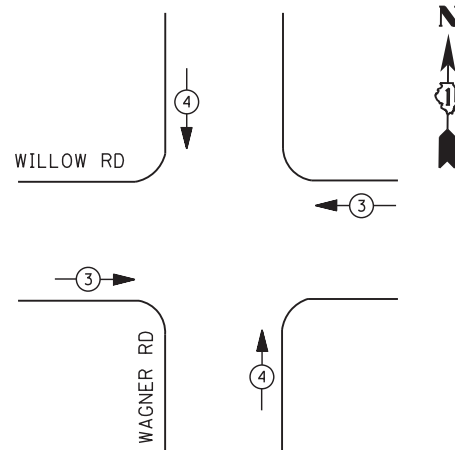


**PHASE DESIGNATION DIAGRAM**

**LEGEND**

- DUAL ENTRY PHASE
- PEDESTRIAN PHASE
- SINGLE ENTRY PHASE
- OVERLAP
- \* NUMBER REFERS TO ASSOCIATED PHASE

**EMERGENCY VEHICLE PREEMPTION SEQUENCE**



**CABLE PLAN**

EMERGENCY VEHICLE PREEMPTORS	
EMERGENCY VEHICLE PREEMPTOR	3 4
MOVEMENT	← → ↓ ↑

ALL LOOPS SHALL BE PREFORMED WITHIN LIMITS OF RECONSTRUCTION

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

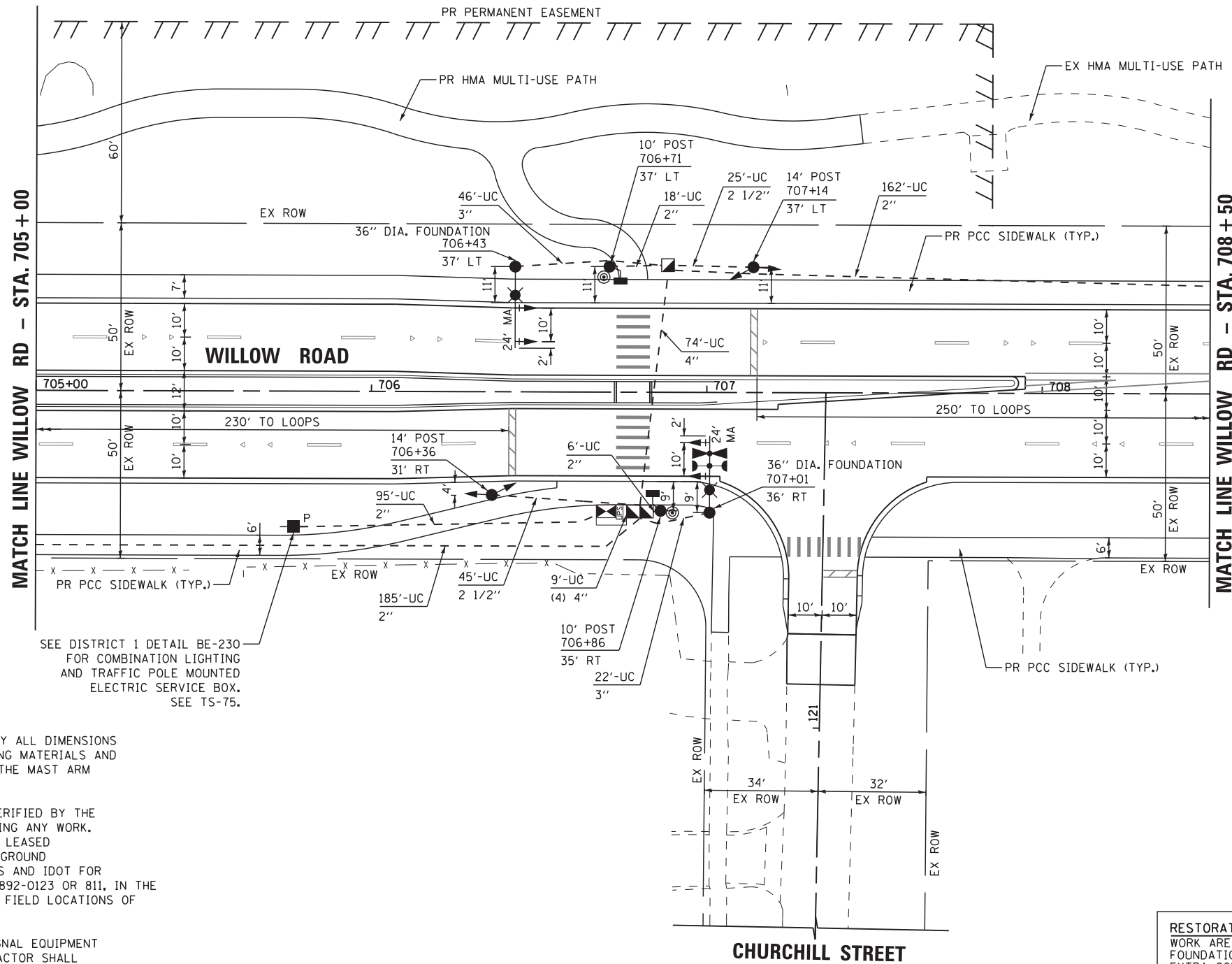
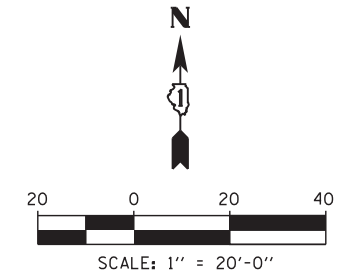
RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

THE END OF THE TRACER CABLE SHALL BE CONTINUOUS AND EXTEND INTO THE CONTROLLER CABINET.

I. D. O. T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					
TYPE	NO. LAMPS	WATTAGE		% OPERATIONS	TOTAL WATTAGE
		INCAND.	LED		
SIGNAL ( RED )	15	135	17	0.50	128
( YELLOW )	15	135	25	0.25	94
( GREEN )	15	135	15	0.25	56
ARROW	16	135	12	0.10	19
PED. SIGNAL	8	90	25	1.00	200
CONTROLLER	1	100	100	1.00	100
ILLUM. SIGN	4	--	90	0.50	180
VIDEO SYSTEM		150	-	1.00	--
TOTAL =					827

ENERGY COSTS- BILLED TO: IDOT - DISTRICT 1  
 201 W. CENTER CT.  
 SCHAUMBURG, IL 60196

ENERGY SUPPLY - CONTACT COMED  
 PHONE 630-723-2128



**GENERAL NOTES**

- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARM LENGTHS.
- THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES. THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811, IN THE CITY OF CHICAGO CONTACT DIGGER AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION REQUIRED).
- THE CONTRACTOR SHALL CHECK THE PROPOSED TRAFFIC SIGNAL EQUIPMENT LOCATIONS FOR OVERHEAD UTILITY CONFLICTS. THE CONTRACTOR SHALL COORDINATE ANY CONFLICTS WITH THE UTILITY COMPANIES AND THE RESIDENT ENGINEER BEFORE ORDERING MATERIALS.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, LOCAL GOVERNMENT AGENCIES AND IDOT.

SEE DISTRICT 1 DETAIL BE-230 FOR COMBINATION LIGHTING AND TRAFFIC POLE MOUNTED ELECTRIC SERVICE BOX. SEE TS-75.

ALL TRAFFIC SIGNAL POLES AT THIS INTERSECTION SHALL BE CONSTRUCTED OF UPGRADED MATERIAL AND SPECIFICATION AS PRODUCED BY STERNBERG LIGHTING.

THE SPECIFICATIONS FOR THE UPGRADED MATERIAL ARE AS FOLLOWS:

MANUFACTURER: STERNBERG LIGHTING  
 COLOR: VERDE GREEN  
 BASE PLATE: STERNBERG  
 POLES: STRAIGHT FLUTE  
 POST CENTER CAP: BCC OR BCC5  
 ROADWAY ARM: CA  
 STRAPS: VERDE GREEN TO MATCH POLES

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

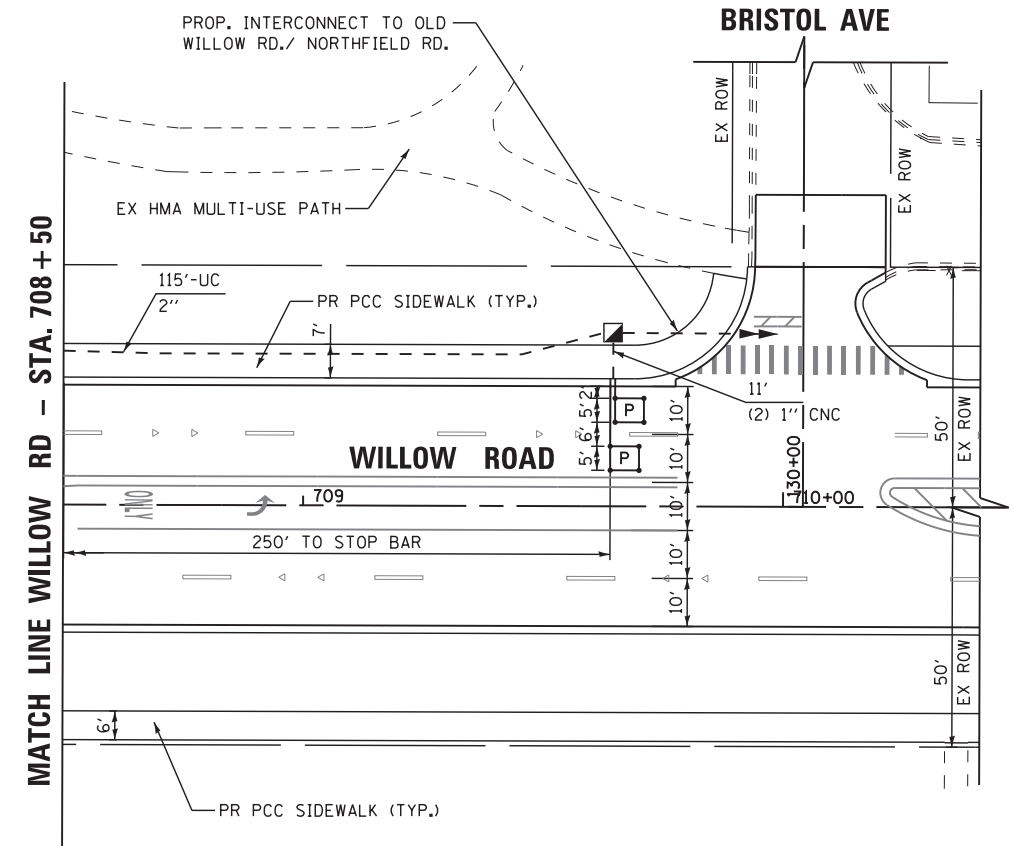
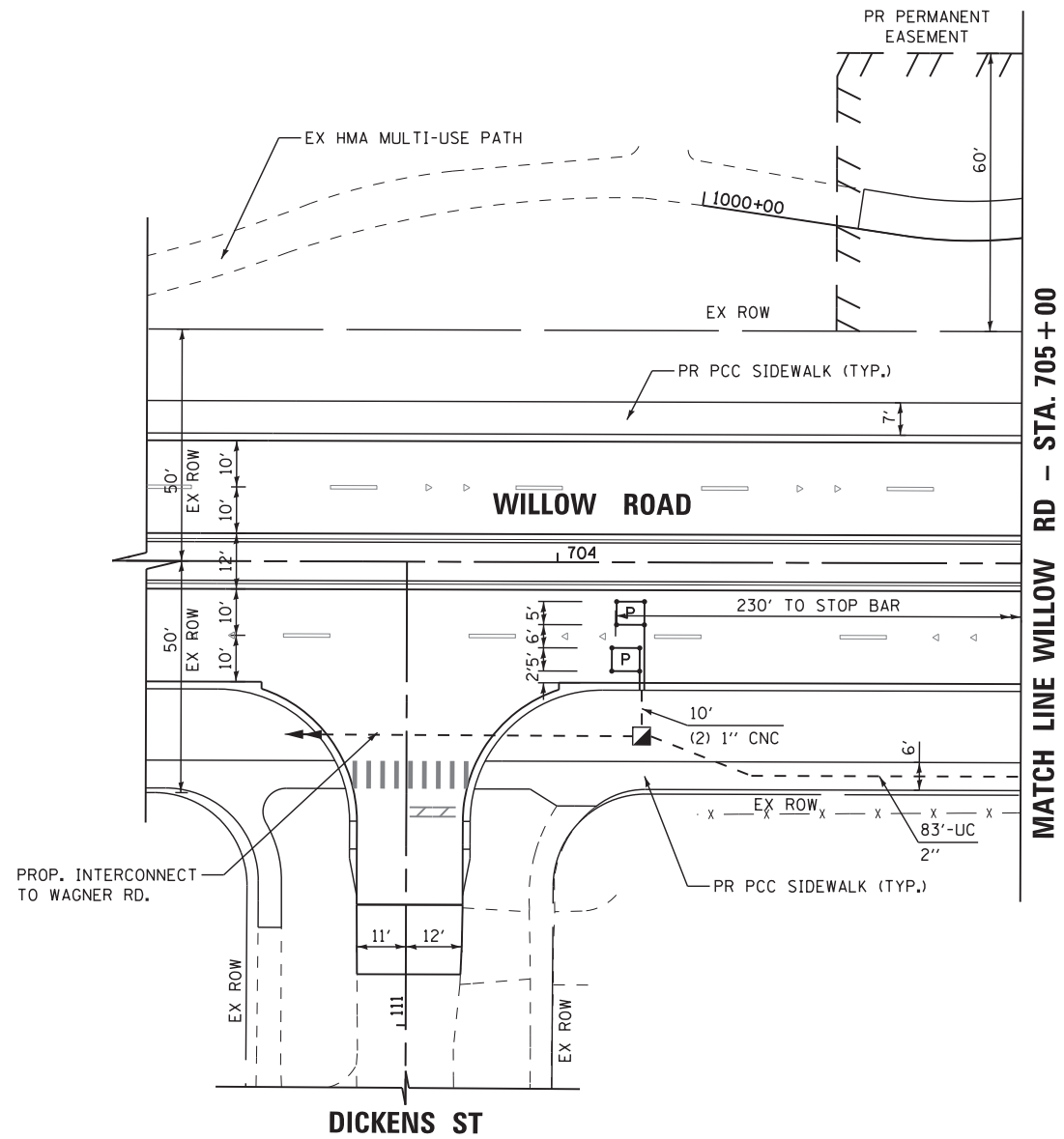
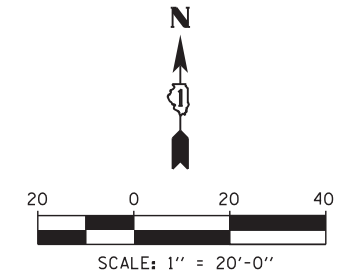
RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

FILE NAME =	USER NAME = \$USER\$	DESIGNED - KMM	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC SIGNAL MODERNIZATION PLAN WILLOW RD. AT WILLOW PARK/CLARKSON PARK PEDESTRIAN CROSSING</b>	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\Projects\312027\DesignCAD\Sheet Files\0160T35r\wLproCH-1.dgn	D160T35r\wLproCH-1.dgn	DRAWN - CDC	REVISED -			305	(1920, .01, 1518, 2022 & 1922, 4BR)	COOK	919	388
PLOT SCALE = \$SCALE\$		CHECKED - JDH	REVISED -			CONTRACT NO. 60T35				
PLOT DATE = 10/30/2012		DATE - 10/31/2012	REVISED -			ILLINOIS FED. AID PROJECT				

TS-40

SCALE: 1"=20' SHEET NO. 1 OF 2 SHEETS STA. TO STA.





IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

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PLOT DATE = 10/30/2012		DATE - 10/31/2012	REVISED -

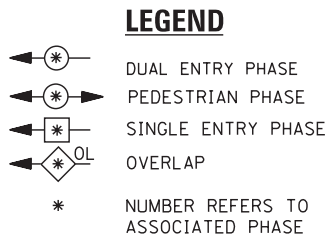
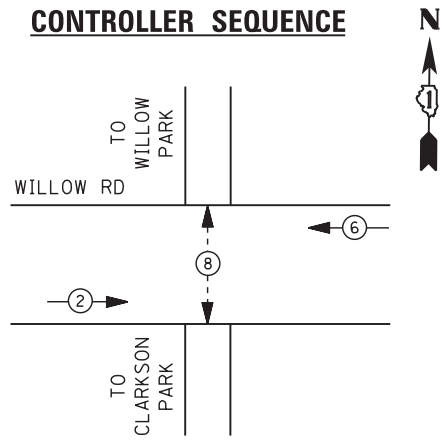
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL MODERNIZATION PLAN  
 WILLOW RD AT WILLOW PARK/CLARKSON PARK PEDESTRIAN CROSSING  
 SCALE: 1"=20' SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920, 01, 1518, 2022 & 1922, 4BIR	COOK	919	389
CONTRACT NO. 60T35				
ILLINOIS FED. AID PROJECT				

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**CONTROLLER SEQUENCE**

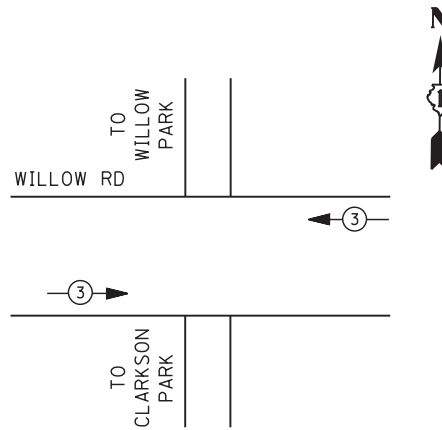


**SCHEDULE OF QUANTITIES**

QUANTITY	UNIT	ITEM
1	EACH	SERVICE INSTALLATION - POLE MOUNTED
664	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.
70	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.
68	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.
110	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.
3	EACH	HANDHOLE
1	EACH	DOUBLE HANDHOLE
1	EACH	TRANSCEIVER - FIBER OPTIC
180	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C
290	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
1052	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
692	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
115	FOOT	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C
519	FOOT	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C
16	FOOT	CONCRETE FOUNDATION, TYPE A
4	FOOT	CONCRETE FOUNDATION, TYPE C
22	FOOT	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER
4	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED
2	EACH	SIGNAL HEAD, LED, 2-FACE, 3-SECTION, BRACKET MOUNTED
2	EACH	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
4	EACH	TRAFFIC SIGNAL BACKPLATE, LED, 1-FACE, BRACKET MOUNTED
2	EACH	INDUCTIVE LOOP DETECTOR
118	FOOT	PREFORMED DETECTOR LOOP
* 1	EACH	LIGHT DETECTOR
* 1	EACH	LIGHT DETECTOR AMPLIFIER
2	EACH	PEDESTRIAN PUSH-BUTTON
* 96	FOOT	EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO 20 3/C
1	EACH	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL
1	EACH	UNINTERRUPTIBLE POWER SUPPLY, SPECIAL
2	EACH	TRAFFIC SIGNAL POST, ALUMINUM 14 FT. (SPECIAL)
2	EACH	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 24 FT. (SPECIAL)
2	EACH	TRAFFIC SIGNAL POST, ALUMINUM 10 FT. (SPECIAL)

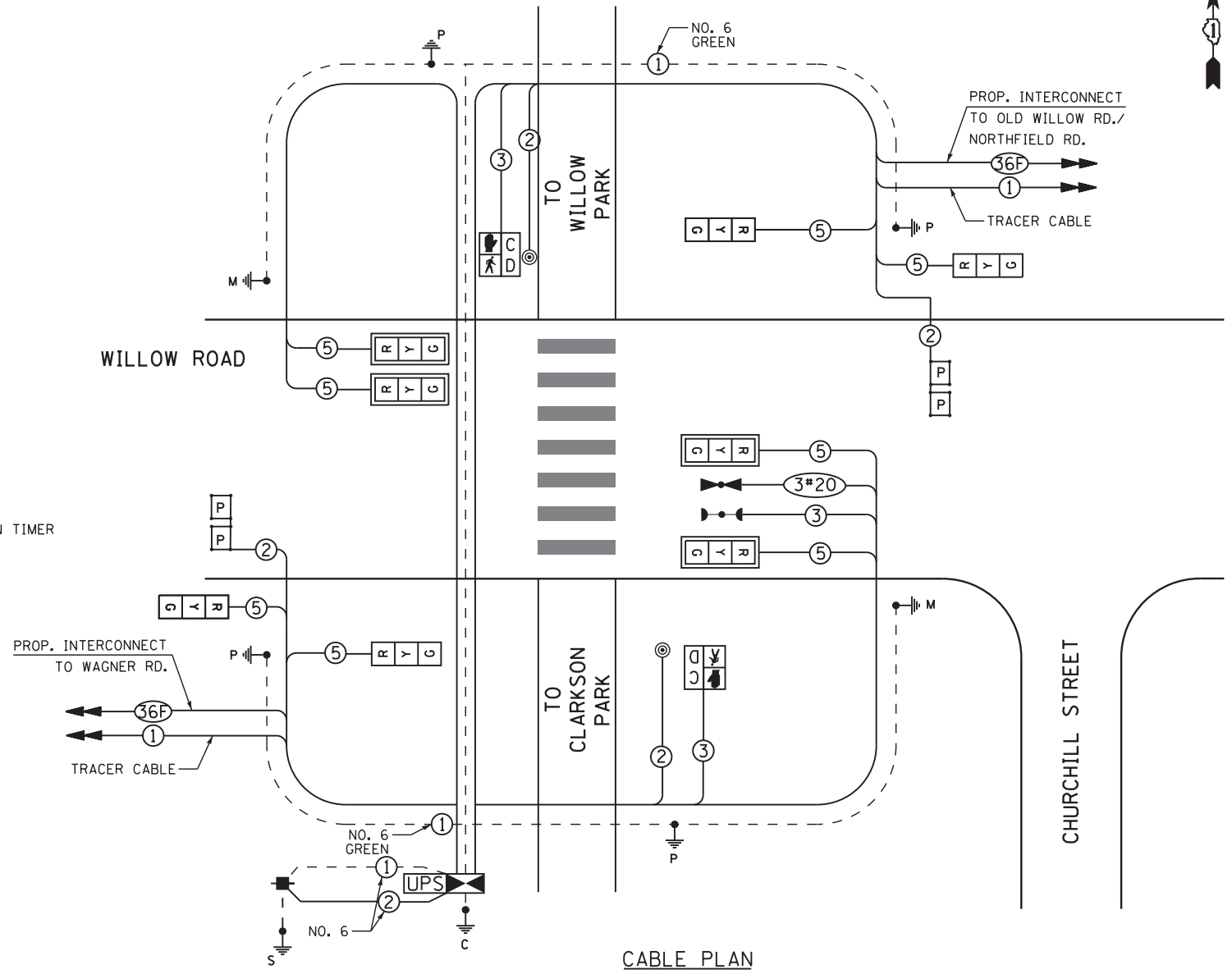
\* 100% VILLAGE OF NORTHFIELD COST

**PHASE DESIGNATION DIAGRAM**



**EMERGENCY VEHICLE PREEMPTION SEQUENCE**

EMERGENCY VEHICLE PREEMPTORS	
EMERGENCY VEHICLE PREEMPTOR	3
MOVEMENT	← →



CABLE PLAN

I. D. O. T.  
 TRAFFIC SIGNAL INSTALLATION  
 ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. LAMPS	WATTAGE		% OPERATIONS	TOTAL WATTAGE
		INCAND.	LED		
SIGNAL (RED)	8	135	17	0.50	68
(YELLOW)	8	135	25	0.25	50
(GREEN)	8	135	15	0.25	30
ARROW		135	12	0.10	
PED. SIGNAL	2	90	25	1.00	50
CONTROLLER	1	100	100	1.00	100
TOTAL =					262

ENERGY COSTS- BILLED TO: IDOT - DISTRICT 1  
 201 W. CENTER CT.  
 SCHAUMBURG, IL 60196

ENERGY SUPPLY - CONTACT COMED  
 PHONE 630-723-2128

THE END OF THE TRACER CABLE SHALL BE CONTINUOUS AND EXTEND INTO THE CONTROLLER CABINET.

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

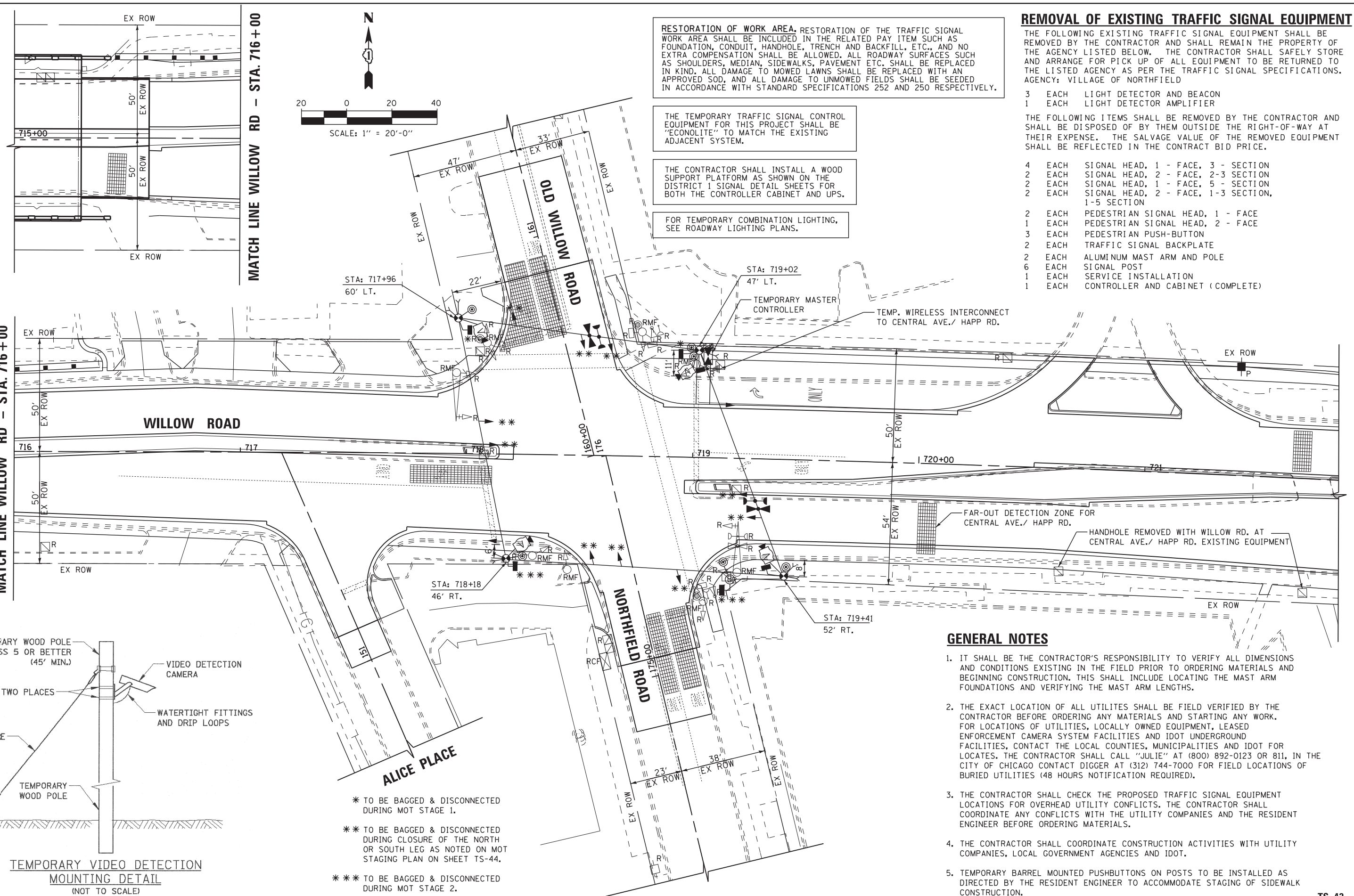
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	PLOT DATE = 10/30/2012	DATE -	10/31/2012	REVISED -	

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

CABLE PLAN, SCHEDULE OF QUANTITIES,  
 PHASE DESIGNATION DIAGRAM & EVP SEQUENCE  
 WILLOW RD AT WILLOW PARK/CLARKSON PARK PEDESTRIAN CROSSING  
 SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920, .01, 1518, 2022 & 1922, 48R)	COOK	919	390
CONTRACT NO. 60T35				
ILLINOIS FED. AID PROJECT				

TS-42



RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

THE TEMPORARY TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

THE CONTRACTOR SHALL INSTALL A WOOD SUPPORT PLATFORM AS SHOWN ON THE DISTRICT 1 SIGNAL DETAIL SHEETS FOR BOTH THE CONTROLLER CABINET AND UPS.

FOR TEMPORARY COMBINATION LIGHTING, SEE ROADWAY LIGHTING PLANS.

**REMOVAL OF EXISTING TRAFFIC SIGNAL EQUIPMENT**

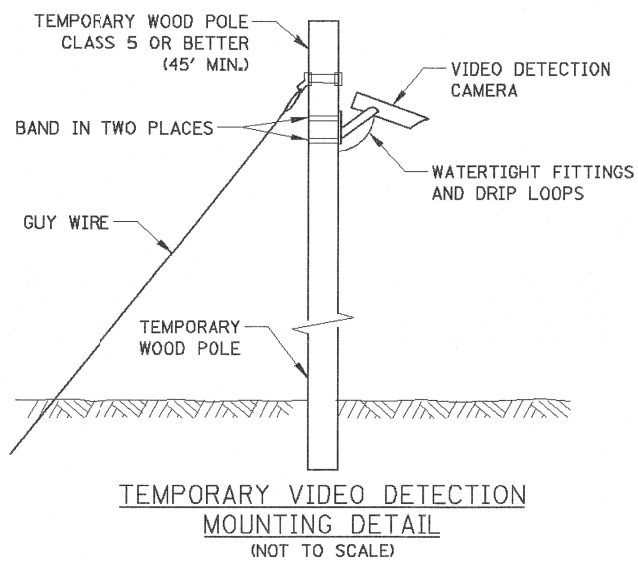
THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE AGENCY LISTED BELOW. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR PICK UP OF ALL EQUIPMENT TO BE RETURNED TO THE LISTED AGENCY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS. AGENCY: VILLAGE OF NORTHFIELD

- 3 EACH LIGHT DETECTOR AND BEACON
  - 1 EACH LIGHT DETECTOR AMPLIFIER
- THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.
- 4 EACH SIGNAL HEAD, 1 - FACE, 3 - SECTION
  - 2 EACH SIGNAL HEAD, 2 - FACE, 2-3 SECTION
  - 2 EACH SIGNAL HEAD, 1 - FACE, 5 - SECTION
  - 2 EACH SIGNAL HEAD, 2 - FACE, 1-3 SECTION, 1-5 SECTION
  - 2 EACH PEDESTRIAN SIGNAL HEAD, 1 - FACE
  - 1 EACH PEDESTRIAN SIGNAL HEAD, 2 - FACE
  - 3 EACH PEDESTRIAN PUSH-BUTTON
  - 2 EACH TRAFFIC SIGNAL BACKPLATE
  - 2 EACH ALUMINUM MAST ARM AND POLE
  - 6 EACH SIGNAL POST
  - 1 EACH SERVICE INSTALLATION
  - 1 EACH CONTROLLER AND CABINET (COMPLETE)

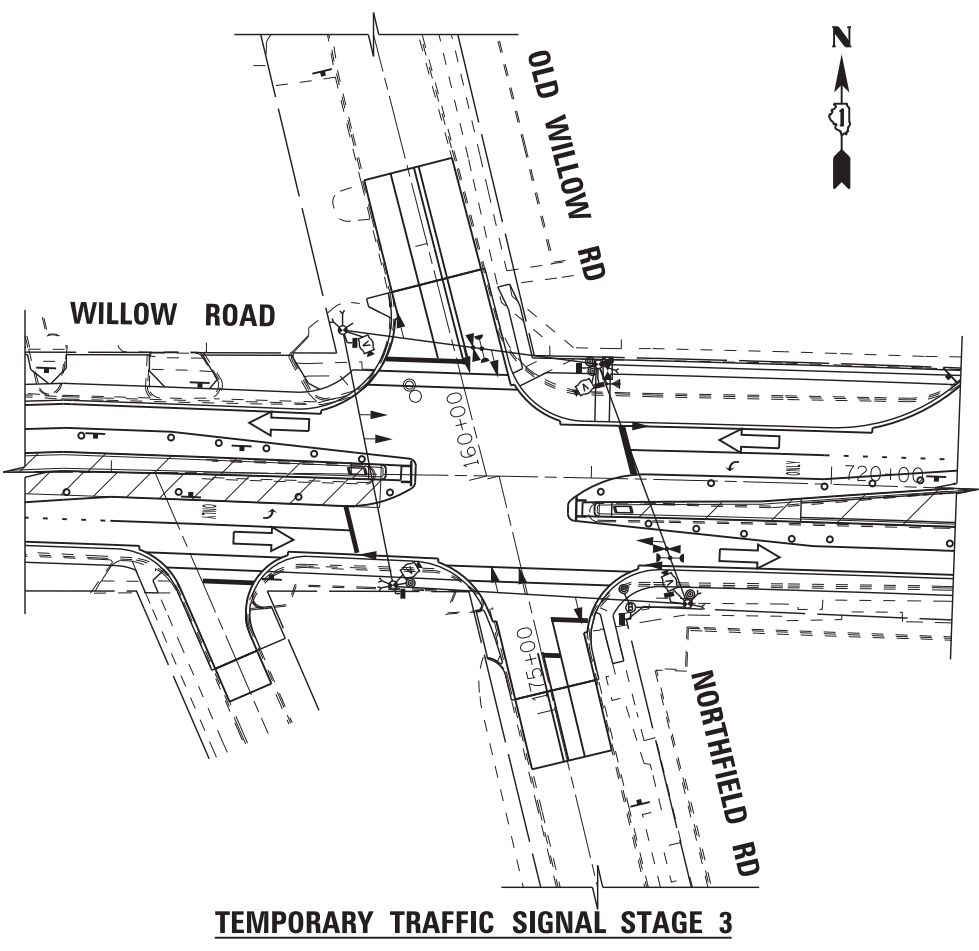
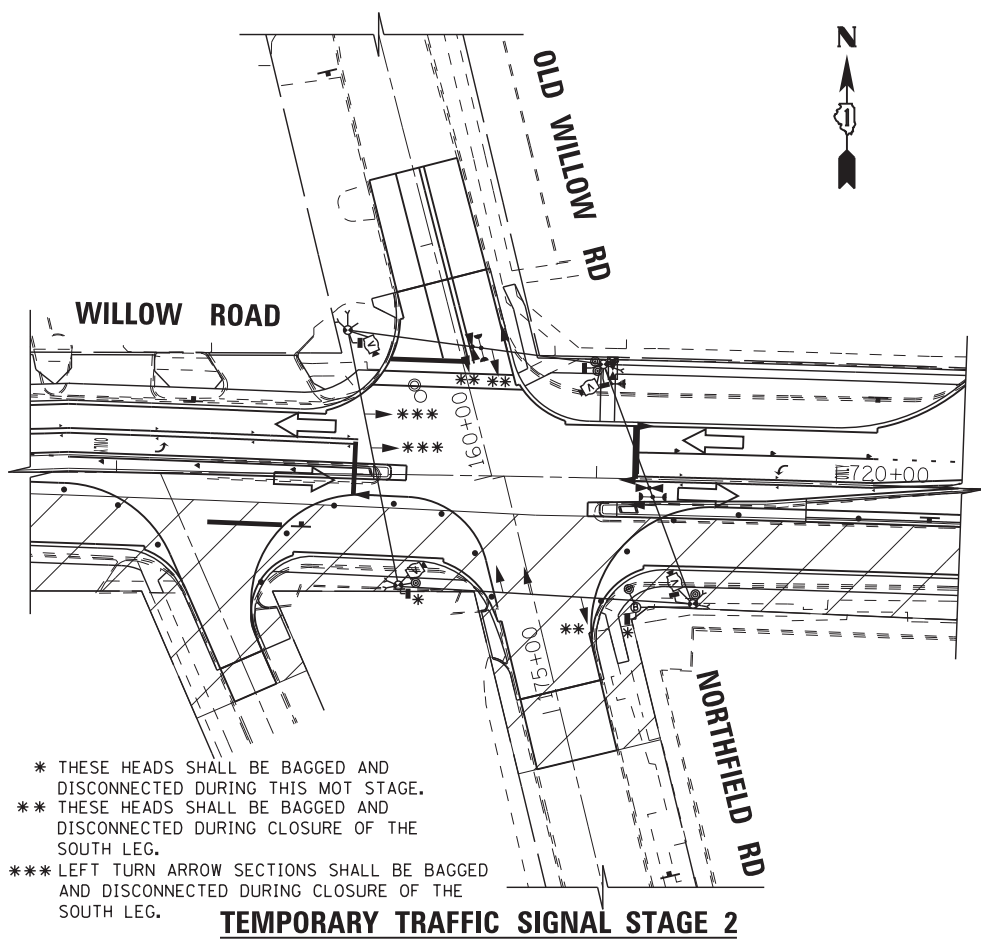
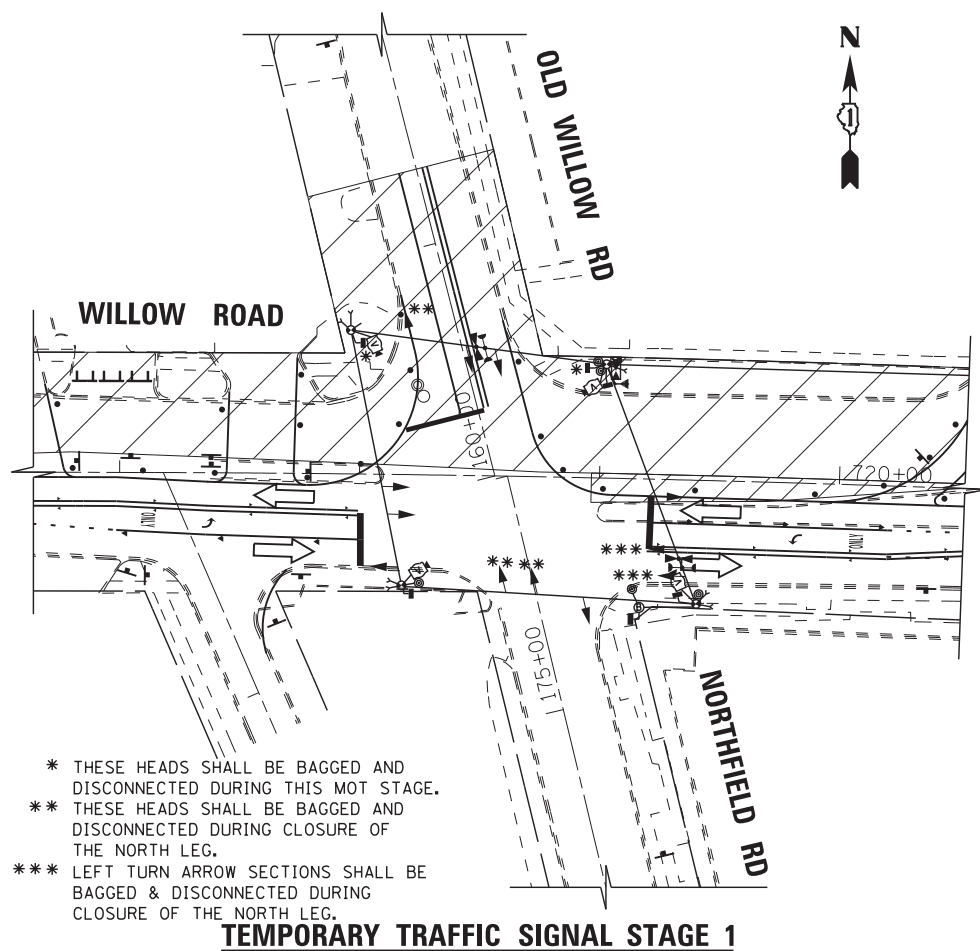
**GENERAL NOTES**

1. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARM LENGTHS.
2. THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES. THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811, IN THE CITY OF CHICAGO CONTACT DIGGER AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION REQUIRED).
3. THE CONTRACTOR SHALL CHECK THE PROPOSED TRAFFIC SIGNAL EQUIPMENT LOCATIONS FOR OVERHEAD UTILITY CONFLICTS. THE CONTRACTOR SHALL COORDINATE ANY CONFLICTS WITH THE UTILITY COMPANIES AND THE RESIDENT ENGINEER BEFORE ORDERING MATERIALS.
4. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, LOCAL GOVERNMENT AGENCIES AND IDOT.
5. TEMPORARY BARREL MOUNTED PUSHBUTTONS ON POSTS TO BE INSTALLED AS DIRECTED BY THE RESIDENT ENGINEER TO ACCOMMODATE STAGING OF SIDEWALK CONSTRUCTION.

- \* TO BE BAGGED & DISCONNECTED DURING MOT STAGE 1.
- \*\* TO BE BAGGED & DISCONNECTED DURING CLOSURE OF THE NORTH OR SOUTH LEG AS NOTED ON MOT STAGING PLAN ON SHEET TS-44.
- \*\*\* TO BE BAGGED & DISCONNECTED DURING MOT STAGE 2.







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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

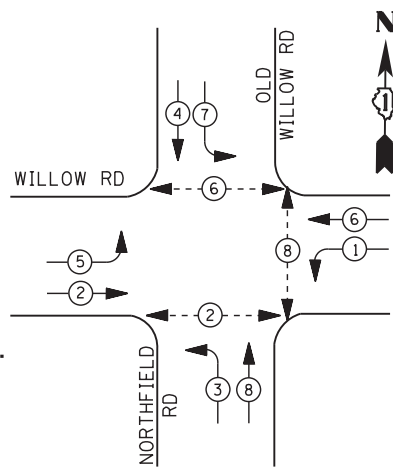
<b>TEMPORARY TRAFFIC SIGNAL INSTALLATION</b>			
<b>M.O.T. STAGING PLAN</b>			
<b>WILLOW ROAD AT OLD WILLOW ROAD /NORTHFIELD ROAD</b>			
SCALE: 1"=40'	SHEET NO. 1	OF X SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920, .01, 1518, 2022 & 1922, 4BIR	COOK	919	392
CONTRACT NO. 60T35				
ILLINOIS FED. AID PROJECT				

**NOTES FOR TEMPORARY TRAFFIC SIGNALS**

- 1) ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
- 2) ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1. INSTALLED IN A NEMA TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
- 3) ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE LED AND 12" (300mm) DIAMETER. HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. PEDESTRIAN SIGNALS SHALL INCLUDE SOLID INTERNATIONAL SYMBOLS. PEDESTRIAN SIGNALS WITH COUNTDOWN TIMERS SHALL BE USED WHEN THE EXISTING INSTALLATION UTILIZES COUNTDOWN TYPE OR AS DIRECTED BY THE ENGINEER. COUNTDOWN TYPE PEDESTRIAN SIGNALS ARE NOT TO BE INSTALLED AT RAILROAD INTERSECTIONS. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
- 4) ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES. RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
- 5) ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
- 6) THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL, AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
- 7) UNINTERRUPTIBLE POWER SUPPLY (UPS) SYSTEM SHALL BE INSTALLED AND MADE OPERATIONAL AT TEMPORARY TRAFFIC SIGNAL INSTALLATIONS WHERE UPS IS INSTALLED AT THE EXISTING TRAFFIC SIGNAL, TEMPORARY TRAFFIC SIGNALS AT RAILROAD INTERSECTIONS, AND TEMPORARY TRAFFIC SIGNALS AT INTERSECTIONS WITH FIRE STATION ACTUATED EMERGENCY VEHICLE PRE-EMPTION, OR WHEN INDICATED ON THE PLANS.
- 8) TRAFFIC SIGNAL MANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
- 9) DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER.
- 10) WHEN PAN, TILT, ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.

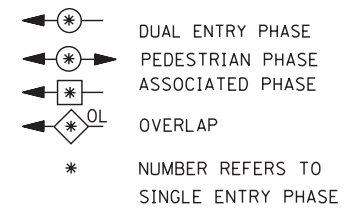
**TEMPORARY CONTROLLER SEQUENCE**



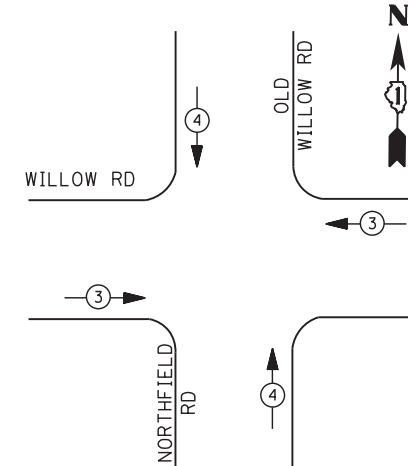
**PHASING NOTES**

1. PHASES 3 AND 7 SHALL NOT BE USED UNTIL CONSTRUCTION ON OLD WILLOW ROAD/NORTHFIELD IS COMPLETED.
2. PHASES 5, 4 AND 7 SHALL BE INACTIVE DURING MOT STAGE 1.
3. PHASES 1, 3 AND 8 SHALL BE INACTIVE DURING MOT STAGE 2.
4. PEDESTRIAN PHASE 6 SHALL BE INACTIVE DURING MOT STAGE 1.
5. PEDESTRIAN PHASE 2 SHALL BE INACTIVE DURING MOT STAGE 2.

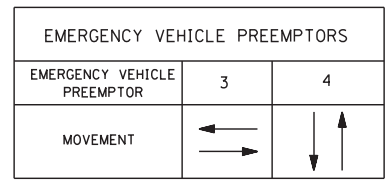
**LEGEND**



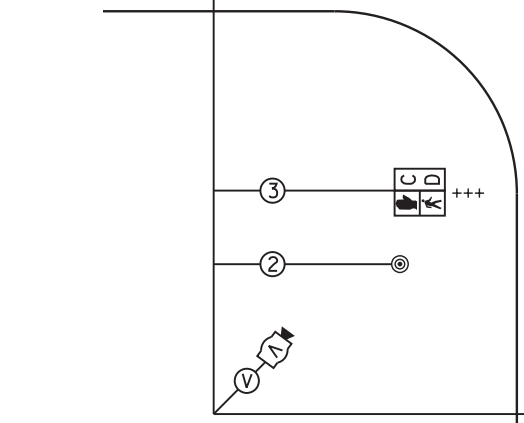
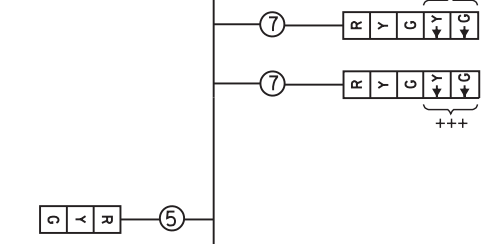
**TEMPORARY PHASE DESIGNATION DIAGRAM**



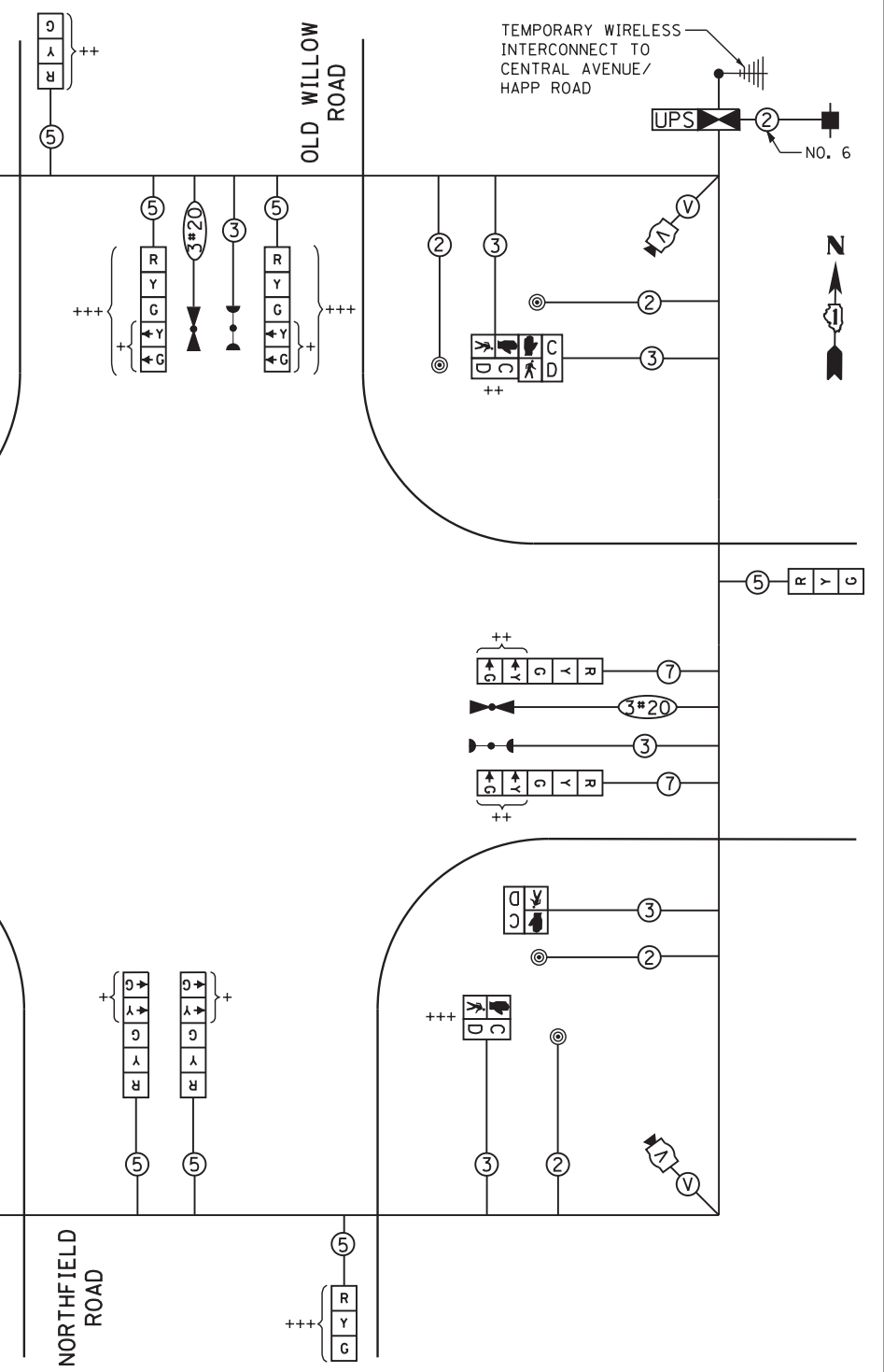
**TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE**



**WILLOW ROAD**



**TEMPORARY CABLE PLAN**



+ THESE SIGNAL SECTIONS SHALL BE BAGGED & DISCONNECTED UNTIL OLD WILLOW ROAD/NORTHFIELD ROAD CONSTRUCTION IS COMPLETE OR AS DIRECTED BY THE RESIDENT ENGINEER.

++ THESE SIGNAL HEADS OR SECTIONS SHALL BE BAGGED & DISCONNECTED DURING CLOSURE OF THE NORTH LEG IN MOT STAGE 1, WITH THE EXCEPTION OF THE PEDESTRIAN HEADS FOR THE NORTH LEG WHICH WILL BE BAGGED & DISCONNECTED FOR THE ENTIRE MOT STAGE 1.

+++ THESE SIGNAL HEADS OR SECTIONS SHALL BE BAGGED & DISCONNECTED DURING CLOSURE OF THE SOUTH LEG IN MOT STAGE 2, WITH THE EXCEPTION OF THE PEDESTRIAN HEADS FOR THE SOUTH LEG WHICH WILL BE BAGGED & DISCONNECTED FOR THE ENTIRE MOT STAGE 2.

THE TEMPORARY TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

RESTORATION OF WORK AREA, RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

I. D. O. T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					
TYPE	NO. LAMPS	WATTAGE		TOTAL WATTAGE	
		INCAND.	LED		
SIGNAL (RED)	12		17	0.50	102
(YELLOW)	12		25	0.25	75
(GREEN)	12		15	0.25	45
ARROW	16		12	0.10	20
PED. SIGNAL	4		25	1.00	100
CONTROLLER	1		100	1.00	100
ILLUM. SIGN			25	0.05	--
VIDEO SYSTEM	1		-	1.00	150
FLASHER LED					
TOTAL =					592

ENERGY COSTS- BILLED TO: IDOT - DISTRICT 1  
 201 W. CENTER CT.  
 SCHAUMBURG, IL 60196

ENERGY SUPPLY - CONTACT COMED  
 PHONE 630-723-2128

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	PLOT DATE = 10/30/2012	DATE -	10/31/2012	REVISED -	

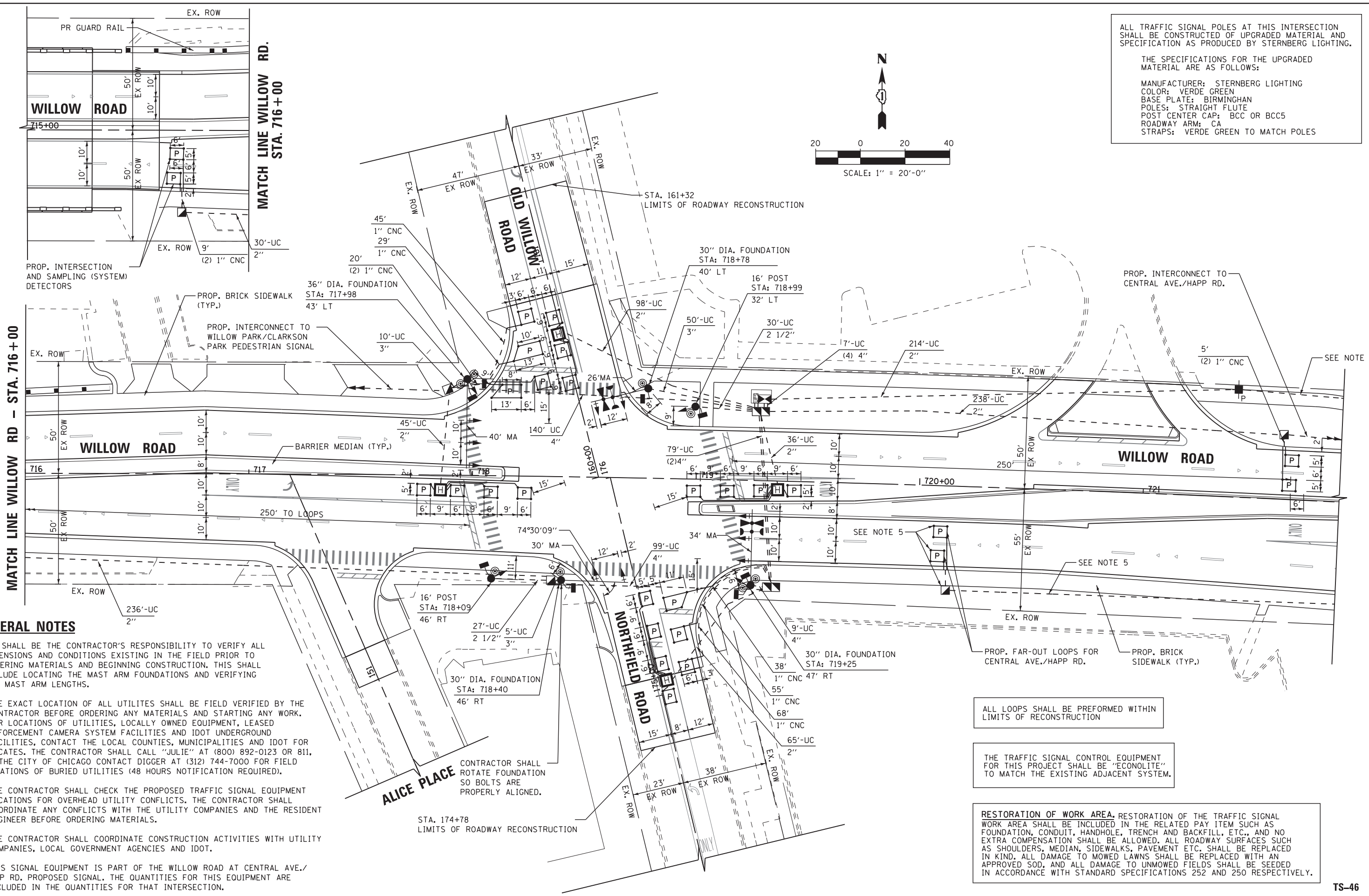
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

**TEMPORARY CABLE PLAN, TEMPORARY PHASE DESIGNATION DIAGRAM & TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE**  
**WILLOW ROAD AT OLD WILLOW ROAD /NORTHFIELD ROAD**

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920, .01, 1518, 2022 & 1922, 4BR)	COOK	919	393
				CONTRACT NO. 60T35
ILLINOIS FED. AID PROJECT				

TS-45

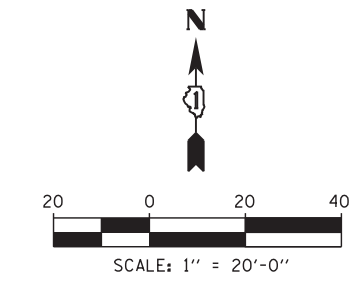
SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.



ALL TRAFFIC SIGNAL POLES AT THIS INTERSECTION SHALL BE CONSTRUCTED OF UPGRADED MATERIAL AND SPECIFICATION AS PRODUCED BY STERNBERG LIGHTING.

THE SPECIFICATIONS FOR THE UPGRADED MATERIAL ARE AS FOLLOWS:

MANUFACTURER: STERNBERG LIGHTING  
 COLOR: VERDE GREEN  
 BASE PLATE: BIRMINGHAM  
 POLES: STRAIGHT FLUTE  
 POST CENTER CAP: BCC OR BCC5  
 ROADWAY ARM: CA  
 STRAPS: VERDE GREEN TO MATCH POLES



**GENERAL NOTES**

- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARM LENGTHS.
- THE EXACT LOCATION OF ALL UTILITES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES. THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811. IN THE CITY OF CHICAGO CONTACT DIGGER AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION REQUIRED).
- THE CONTRACTOR SHALL CHECK THE PROPOSED TRAFFIC SIGNAL EQUIPMENT LOCATIONS FOR OVERHEAD UTILITY CONFLICTS. THE CONTRACTOR SHALL COORDINATE ANY CONFLICTS WITH THE UTILITY COMPANIES AND THE RESIDENT ENGINEER BEFORE ORDERING MATERIALS.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, LOCAL GOVERNMENT AGENCIES AND IDOT.
- THIS SIGNAL EQUIPMENT IS PART OF THE WILLOW ROAD AT CENTRAL AVE./HAPP RD. PROPOSED SIGNAL. THE QUANTITIES FOR THIS EQUIPMENT ARE INCLUDED IN THE QUANTITIES FOR THAT INTERSECTION.

ALL LOOPS SHALL BE PREFORMED WITHIN LIMITS OF RECONSTRUCTION

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

FILE NAME - N:\Projects\312027\Design\CAD\Sheet Files	USER NAME - \$USER\$	DESIGNED - KMM	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC SIGNAL MODERNIZATION PLAN			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	D160T35\w\lproNF-1.dgn	DRAWN - CDC	REVISED -		WILLOW ROAD AT OLD WILLOW ROAD /NORTHFIELD ROAD	305	(1920, .01, 1518, 2022 & 1922, 4BR)	COOK	919	394		
	PLOT SCALE - \$SCALE\$	CHECKED - JDH	REVISED -	SCALE: 1"=20'	SHEET NO.	OF SHEETS	STA.	TO STA.	CONTRACT NO. 60T35			
	PLOT DATE - 10/30/2012	DATE - 10/31/2012	REVISED -		ILLINOIS FED. AID PROJECT							

TS-46

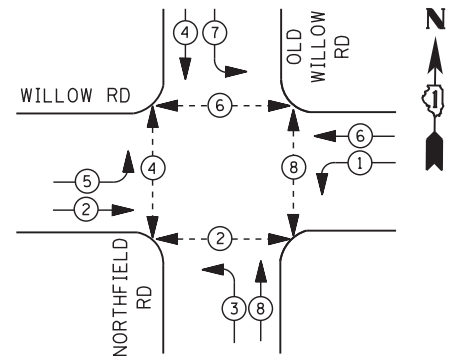


**SCHEDULE OF QUANTITIES**

QUANTITY	UNIT	ITEM
1	EACH	SERVICE INSTALLATION - POLE MOUNTED
962	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.
57	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.
65	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.
434	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.
4	EACH	HANDHOLE
4	EACH	HEAVY-DUTY HANDHOLE
2	EACH	DOUBLE HANDHOLE
1	EACH	TRANSCEIVER - FIBER OPTIC
1308	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C
1645	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
1174	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
1514	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C
2154	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
234	FOOT	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C
894	FOOT	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C
8	FOOT	CONCRETE FOUNDATION, TYPE A
4	FOOT	CONCRETE FOUNDATION, TYPE C
37	FOOT	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER
13	FOOT	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER
6	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED
4	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED
4	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED
4	EACH	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
2	EACH	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
10	EACH	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM
9	EACH	INDUCTIVE LOOP DETECTOR
931	FOOT	PREFORMED DETECTOR LOOP
2	EACH	LIGHT DETECTOR
1	EACH	LIGHT DETECTOR AMPLIFIER
8	EACH	PEDESTRIAN PUSH-BUTTON
1	EACH	TEMPORARY TRAFFIC SIGNAL INSTALLATION
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
8	EACH	REMOVE EXISTING HANDHOLE
9	EACH	REMOVE EXISTING CONCRETE FOUNDATION
303	FOOT	EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO 20 3/C
1	EACH	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL
1	EACH	UNINTERRUPTIBLE POWER SUPPLY, SPECIAL
1	EACH	TEMPORARY TRAFFIC SIGNAL TIMING
2	EACH	TRAFFIC SIGNAL POST, ALUMINUM 16 FT. (SPECIAL)
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 26 FT. (SPECIAL)
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 30 FT. (SPECIAL)
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 34 FT. (SPECIAL)
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 40 FT. (SPECIAL)
4	EACH	ILLUMINATED STREET NAME SIGN

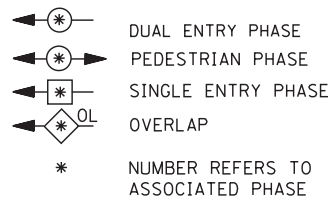
\* 100% VILLAGE OF NORTHFIELD COST

**CONTROLLER SEQUENCE**



**PHASE DESIGNATION DIAGRAM**

**LEGEND**

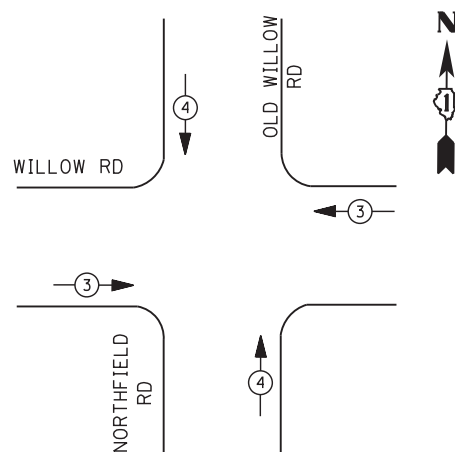


**I. D. O. T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS**

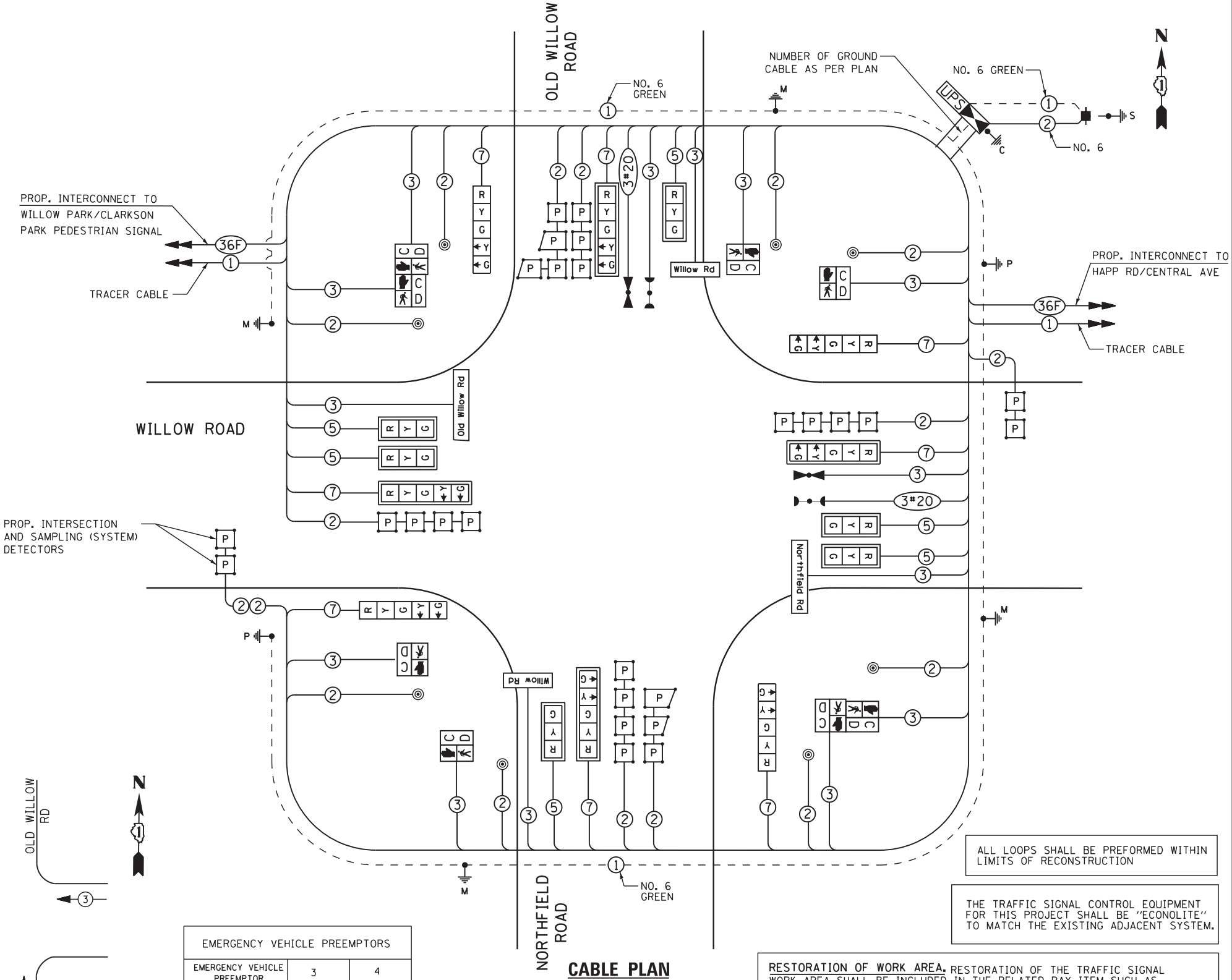
TYPE	NO. LAMPS	WATTAGE		% OPERATIONS	TOTAL WATTAGE
		INCAND.	LED		
SIGNAL (RED)	14	135	17	0.50	119
(YELLOW)	14	135	25	0.25	88
(GREEN)	14	135	15	0.25	53
ARROW	16	135	12	0.10	19
PED. SIGNAL	8	90	25	1.00	200
CONTROLLER	1	100	100	1.00	100
ILLUM. SIGN	4	--	90	0.50	180
TOTAL =					758

ENERGY COSTS- BILLED TO: IDOT - DISTRICT 1  
 201 W. CENTER CT.  
 SCHAUMBURG, IL 60196  
 ENERGY SUPPLY - CONTACT COMED  
 PHONE 630-723-2128

**EMERGENCY VEHICLE PREEMPTION SEQUENCE**



EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	←	↑



**CABLE PLAN**

THE END OF THE TRACER CABLE SHALL BE CONTINUOUS AND EXTEND INTO THE CONTROLLER CABINET.

ALL LOOPS SHALL BE PREFORMED WITHIN LIMITS OF RECONSTRUCTION

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

**REMOVAL OF EXISTING TRAFFIC SIGNAL EQUIPMENT**

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE AGENCY LISTED BELOW. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR PICK UP OF ALL EQUIPMENT TO BE RETURNED TO THE LISTED AGENCY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS. AGENCY: VILLAGE OF NORTHFIELD

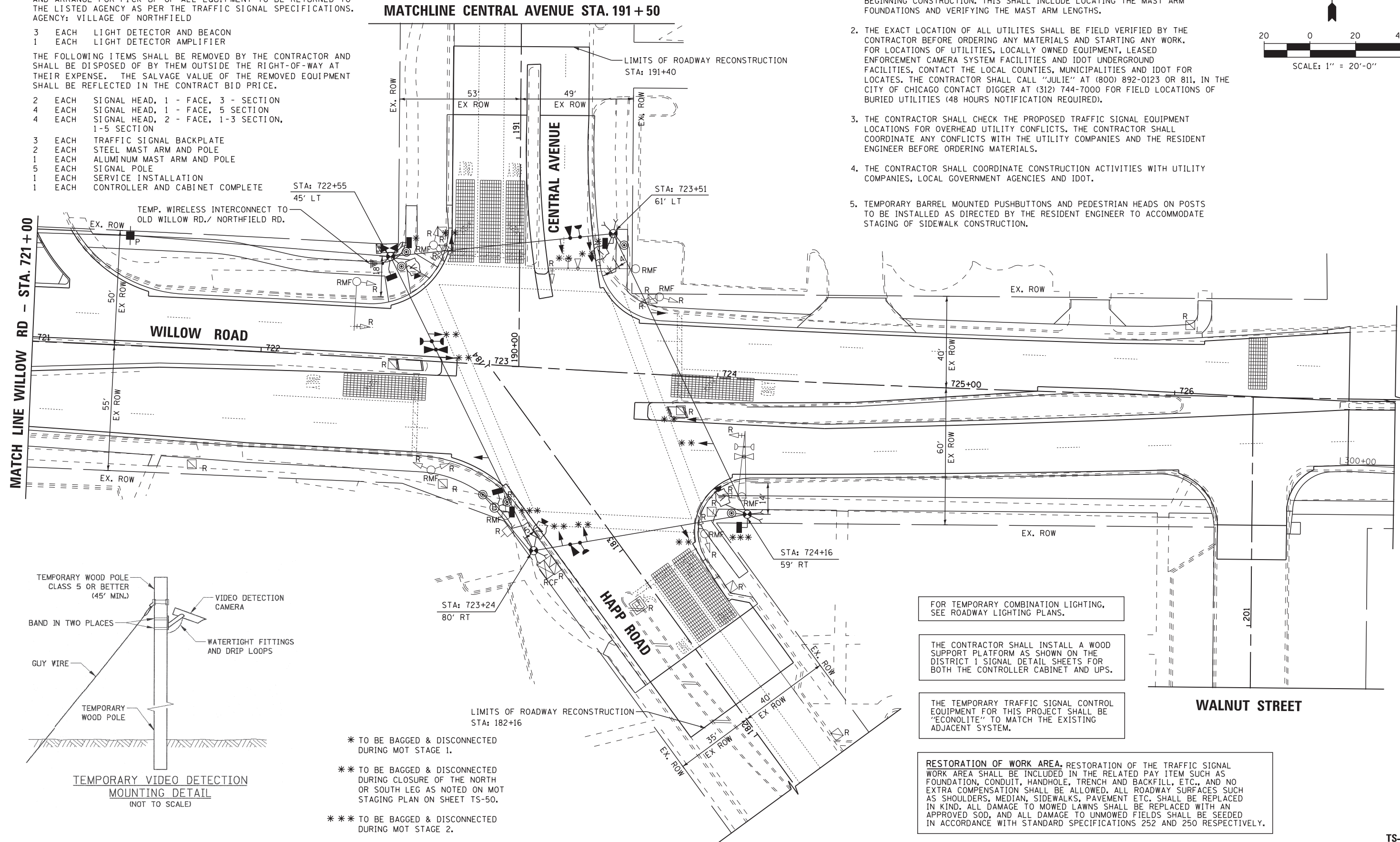
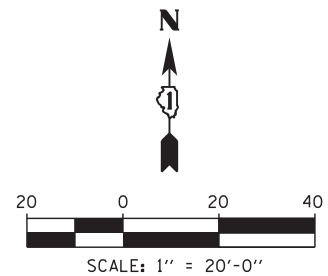
- 3 EACH LIGHT DETECTOR AND BEACON
- 1 EACH LIGHT DETECTOR AMPLIFIER

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 2 EACH SIGNAL HEAD, 1 - FACE, 3 - SECTION
- 4 EACH SIGNAL HEAD, 1 - FACE, 5 SECTION
- 4 EACH SIGNAL HEAD, 2 - FACE, 1-3 SECTION, 1-5 SECTION
- 3 EACH TRAFFIC SIGNAL BACKPLATE
- 2 EACH STEEL MAST ARM AND POLE
- 1 EACH ALUMINUM MAST ARM AND POLE
- 5 EACH SIGNAL POLE
- 1 EACH SERVICE INSTALLATION
- 1 EACH CONTROLLER AND CABINET COMPLETE

**GENERAL NOTES**

1. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARM LENGTHS.
2. THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES. THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811, IN THE CITY OF CHICAGO CONTACT DIGGER AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION REQUIRED).
3. THE CONTRACTOR SHALL CHECK THE PROPOSED TRAFFIC SIGNAL EQUIPMENT LOCATIONS FOR OVERHEAD UTILITY CONFLICTS. THE CONTRACTOR SHALL COORDINATE ANY CONFLICTS WITH THE UTILITY COMPANIES AND THE RESIDENT ENGINEER BEFORE ORDERING MATERIALS.
4. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, LOCAL GOVERNMENT AGENCIES AND IDOT.
5. TEMPORARY BARREL MOUNTED PUSHBUTTONS AND PEDESTRIAN HEADS ON POSTS TO BE INSTALLED AS DIRECTED BY THE RESIDENT ENGINEER TO ACCOMMODATE STAGING OF SIDEWALK CONSTRUCTION.



FOR TEMPORARY COMBINATION LIGHTING, SEE ROADWAY LIGHTING PLANS.

THE CONTRACTOR SHALL INSTALL A WOOD SUPPORT PLATFORM AS SHOWN ON THE DISTRICT 1 SIGNAL DETAIL SHEETS FOR BOTH THE CONTROLLER CABINET AND UPS.

THE TEMPORARY TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

- \* TO BE BAGGED & DISCONNECTED DURING MOT STAGE 1.
- \*\* TO BE BAGGED & DISCONNECTED DURING CLOSURE OF THE NORTH OR SOUTH LEG AS NOTED ON MOT STAGING PLAN ON SHEET TS-50.
- \*\*\* TO BE BAGGED & DISCONNECTED DURING MOT STAGE 2.

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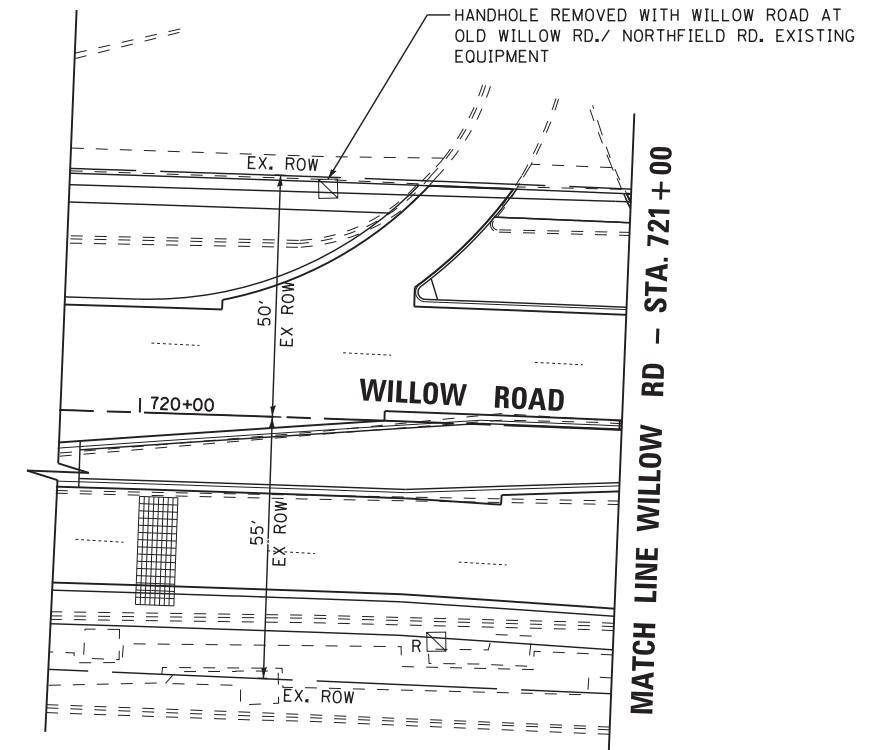
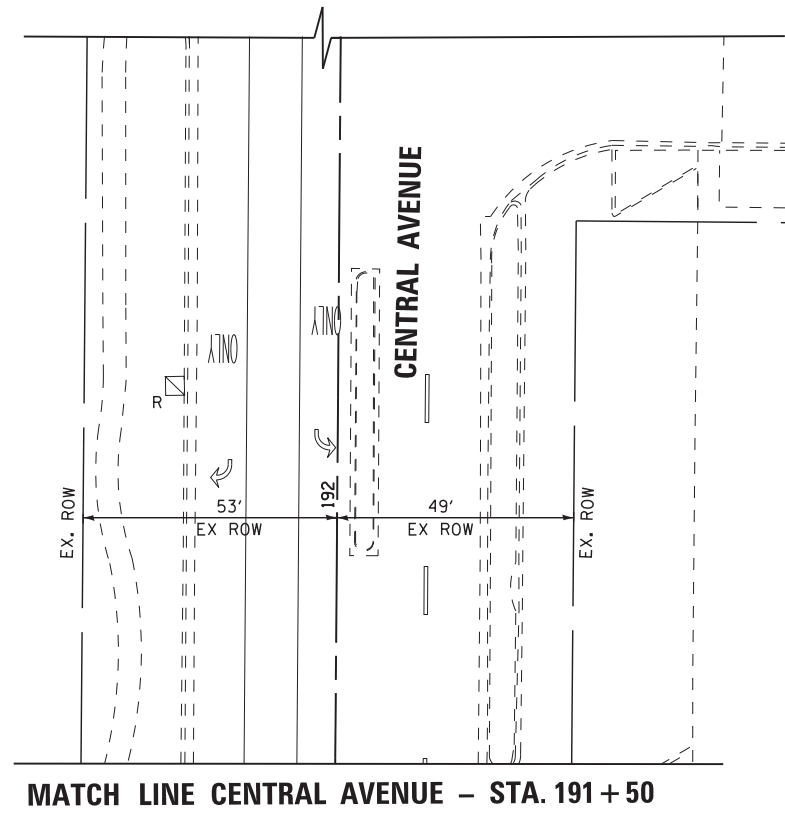
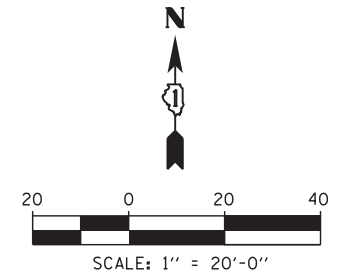
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

**TEMPORARY TRAFFIC SIGNAL INSTALLATION & REMOVAL PLAN**  
**MAINTENANCE OF TRAFFIC STAGE 1**  
**WILLOW ROAD AT CENTRAL AVENUE /HAPP ROAD**

SCALE: 1"=20' SHEET NO. 1 OF 2 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920, .01, 1518, 2022 & 1922, 4BR)	COOK	919	396
			CONTRACT NO. 60T35	
ILLINOIS FED. AID PROJECT				

TS-48



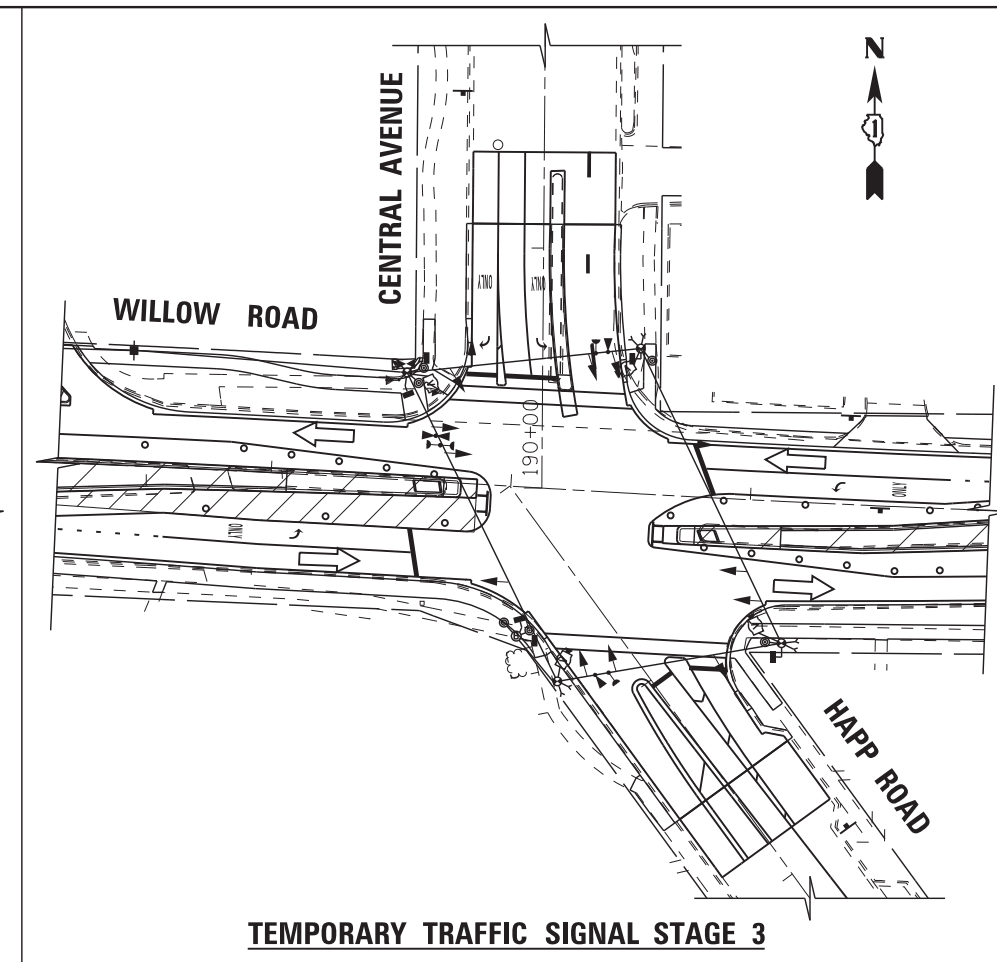
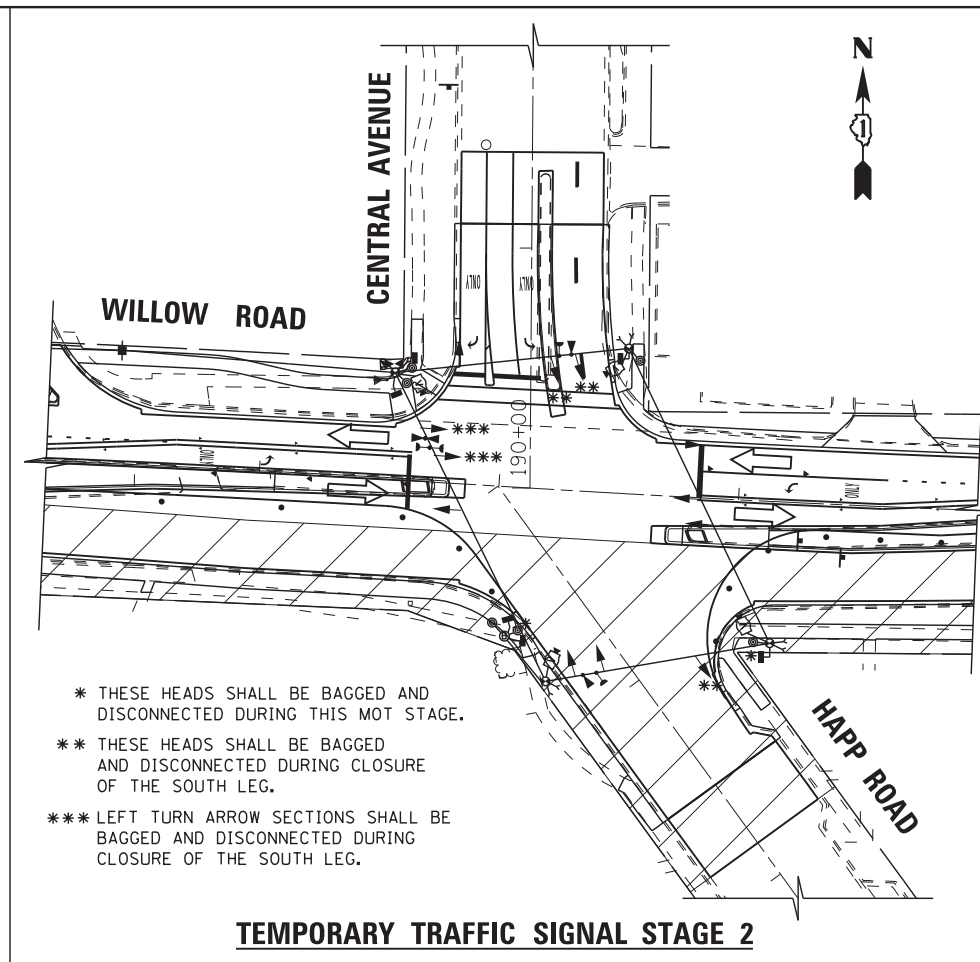
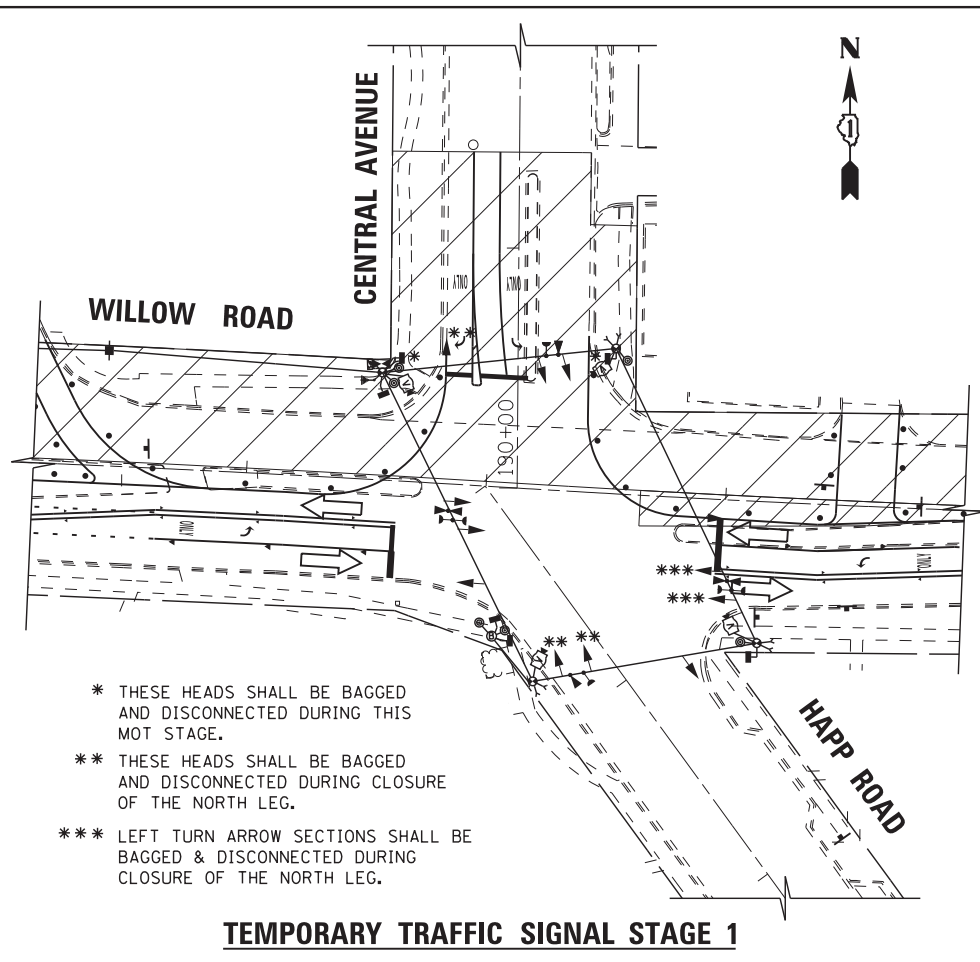
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	PLOT DATE = 10/30/2012	DATE - 10/31/2012	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>TEMPORARY TRAFFIC SIGNAL INSTALLATION &amp; REMOVAL PLAN          MAINTENANCE OF TRAFFIC STAGE 1          WILLOW ROAD AT CENTRAL AVENUE /HAPP ROAD</b>			
SCALE: 1"=20'	SHEET NO.	OF SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920, .01, 1518, 2022 & 1922, 4BIR	COOK	919	397
CONTRACT NO. 60T35				
ILLINOIS FED. AID PROJECT				





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		CHECKED - JDH	REVISED -
		DATE - 10/31/2012	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION  
 M.O.T. STAGING PLAN  
 WILLOW ROAD AT CENTRAL AVENUE /HAPP ROAD**

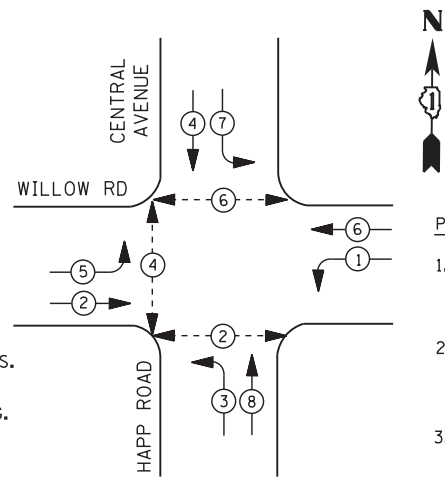
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920, .01, 1518, 2022 & 1922, 4BIR)	COOK	919	398
CONTRACT NO. 60T35				
ILLINOIS FED. AID PROJECT				

**NOTES FOR TEMPORARY TRAFFIC SIGNALS**

- ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
- ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1. INSTALLED IN A NEMA TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
- ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE LED AND 12" (300mm) DIAMETER. HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. PEDESTRIAN SIGNALS SHALL INCLUDE SOLID INTERNATIONAL SYMBOLS. PEDESTRIAN SIGNALS WITH COUNTDOWN TIMERS SHALL BE USED WHEN THE EXISTING INSTALLATION UTILIZES COUNTDOWN TYPE OR AS DIRECTED BY THE ENGINEER. COUNTDOWN TYPE PEDESTRIAN SIGNALS ARE NOT TO BE INSTALLED AT RAILROAD INTERSECTIONS. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
- ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
- ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
- THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL, AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
- UNINTERRUPTIBLE POWER SUPPLY (UPS) SYSTEM SHALL BE INSTALLED AND MADE OPERATIONAL AT TEMPORARY TRAFFIC SIGNAL INSTALLATIONS WHERE UPS IS INSTALLED AT THE EXISTING TRAFFIC SIGNAL, TEMPORARY TRAFFIC SIGNALS AT RAILROAD INTERSECTIONS, AND TEMPORARY TRAFFIC SIGNALS AT INTERSECTIONS WITH FIRE STATION ACTUATED EMERGENCY VEHICLE PRE-EMPTION, OR WHEN INDICATED ON THE PLANS.
- TRAFFIC SIGNAL MANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
- DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER.
- WHEN PAN, TILT, ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.

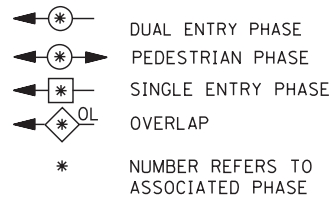
**TEMPORARY CONTROLLER SEQUENCE**



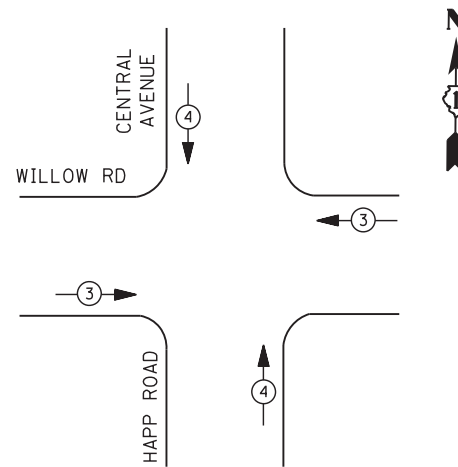
**PHASING NOTES**

- PHASES 5, 4 AND 7 SHALL BE INACTIVE DURING CLOSURE OF THE NORTH LEG IN MOT STAGE 1.
- PHASES 1, 3 AND 8 SHALL BE INACTIVE DURING CLOSURE OF THE SOUTH LEG IN MOT STAGE 2.
- PEDESTRIAN PHASE 6 SHALL BE INACTIVE DURING MOT STAGE 1.
- PEDESTRIAN PHASE 2 SHALL BE INACTIVE DURING MOT STAGE 2.

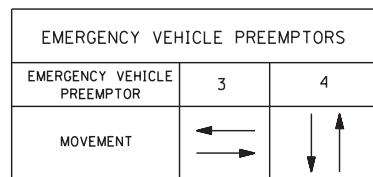
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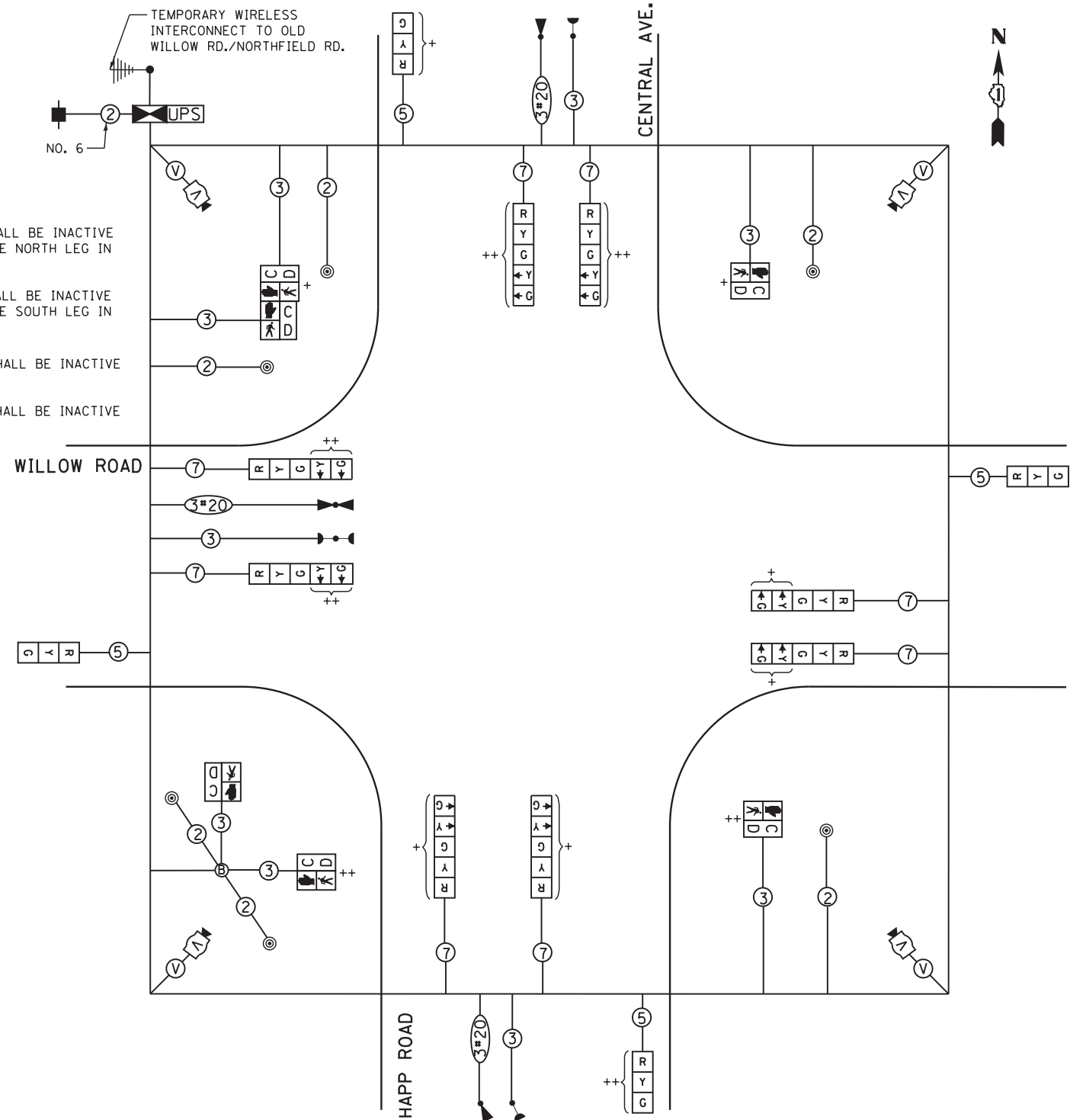
**TEMPORARY PHASE DESIGNATION DIAGRAM**



**TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE**



**RESTORATION OF WORK AREA.** RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



**TEMPORARY CABLE PLAN**

+ THESE SIGNAL HEADS OR SECTIONS SHALL BE BAGGED & DISCONNECTED DURING CLOSURE OF THE NORTH LEG IN MOT STAGE 1, WITH THE EXCEPTION OF THE PEDESTRIAN HEADS FOR THE NORTH LEG WHICH WILL BE BAGGED & DISCONNECTED FOR THE ENTIRE MOT STAGE 1.

++ THESE SIGNAL HEADS OR SECTIONS SHALL BE BAGGED & DISCONNECTED DURING CLOSURE OF THE SOUTH LEG IN MOT STAGE 2, WITH THE EXCEPTION OF THE PEDESTRIAN HEADS FOR THE SOUTH LEG WHICH WILL BE BAGGED & DISCONNECTED FOR THE ENTIRE MOT STAGE 2.

THE TEMPORARY TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

I. D. O. T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS				
TYPE	NO. LAMPS	WATTAGE		TOTAL WATTAGE
		INCAND.	LED	
SIGNAL (RED)	12		17	102
(YELLOW)	12		25	75
(GREEN)	12		15	45
ARROW	16		12	19
PED. SIGNAL	--		25	--
CONTROLLER	1		100	100
ILLUM. SIGN			25	--
VIDEO SYSTEM	1		-	150
TOTAL =				491

ENERGY COSTS- BILLED TO: IDOT - DISTRICT 1  
 201 W. CENTER CT.  
 SCHAUMBURG, IL 60196

ENERGY SUPPLY - CONTACT COMED  
 PHONE 630-723-2128

FILE NAME -	USER NAME - \$USER\$	DESIGNED - KMM	REVISED -
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	PLOT SCALE = \$SCALE\$	CHECKED - JDH	REVISED -
	PLOT DATE = 10/30/2012	DATE - 10/31/2012	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TEMPORARY CABLE PLAN, TEMPORARY PHASE DESIGNATION DIAGRAM  
 & TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE  
 WILLOW ROAD AT CENTRAL AVENUE /HAPP ROAD

SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920, .01, 1518, 2022 & 1922, 48R)	COOK	919	399
ILLINOIS FED. AID PROJECT				CONTRACT NO. 60T35

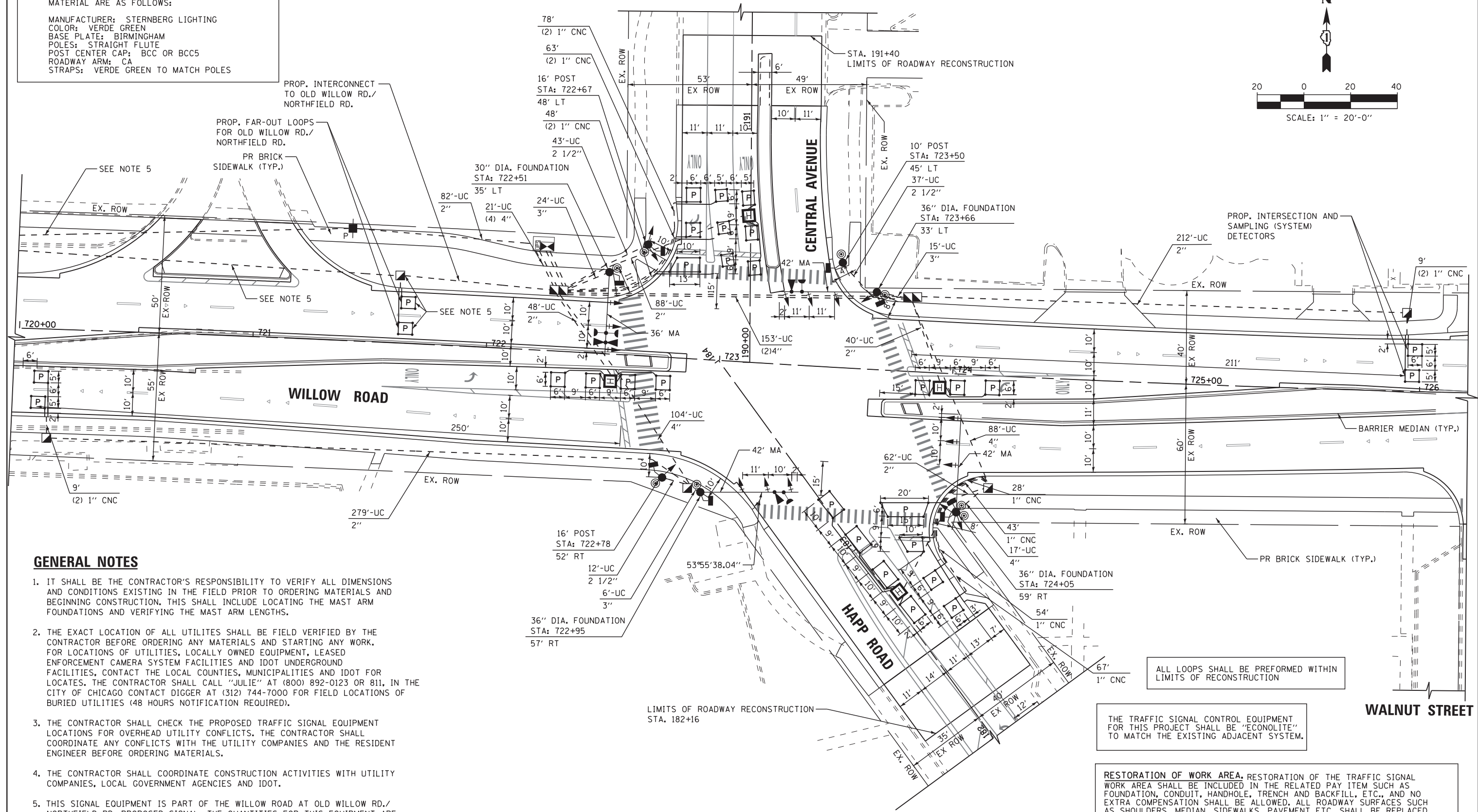
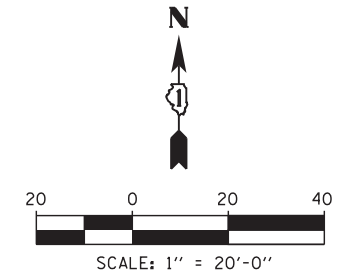
TS-51



ALL TRAFFIC SIGNAL POLES AT THIS INTERSECTION SHALL BE CONSTRUCTED OF UPGRADED MATERIAL AND SPECIFICATION AS PRODUCED BY STERNBERG LIGHTING.

THE SPECIFICATIONS FOR THE UPGRADED MATERIAL ARE AS FOLLOWS:

MANUFACTURER: STERNBERG LIGHTING  
 COLOR: VERDE GREEN  
 BASE PLATE: BIRMINGHAM  
 POLES: STRAIGHT FLUTE  
 POST CENTER CAP: BCC OR BCC5  
 ROADWAY ARM: CA  
 STRAPS: VERDE GREEN TO MATCH POLES



**GENERAL NOTES**

1. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARM LENGTHS.
2. THE EXACT LOCATION OF ALL UTILITES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES. THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811, IN THE CITY OF CHICAGO CONTACT DIGGER AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION REQUIRED).
3. THE CONTRACTOR SHALL CHECK THE PROPOSED TRAFFIC SIGNAL EQUIPMENT LOCATIONS FOR OVERHEAD UTILITY CONFLICTS. THE CONTRACTOR SHALL COORDINATE ANY CONFLICTS WITH THE UTILITY COMPANIES AND THE RESIDENT ENGINEER BEFORE ORDERING MATERIALS.
4. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, LOCAL GOVERNMENT AGENCIES AND IDOT.
5. THIS SIGNAL EQUIPMENT IS PART OF THE WILLOW ROAD AT OLD WILLOW RD./NORTHFIELD RD. PROPOSED SIGNAL. THE QUANTITIES FOR THIS EQUIPMENT ARE INCLUDED IN THE QUANTITIES FOR THAT INTERSECTION.

ALL LOOPS SHALL BE PERFORMED WITHIN LIMITS OF RECONSTRUCTION

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

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	PLOT DATE = 10/30/2012	DATE -	10/31/2012	REVISED -	

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL MODERNIZATION PLAN  
 WILLOW ROAD AT CENTRAL AVENUE/HAPP ROAD  
 SCALE: 1"=20' SHEET NO. 1 OF 2 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	(1920, .01, 1518, 2022 & 1922, 4BR)	COOK	919	400
CONTRACT NO. 60T35				
ILLINOIS FED. AID PROJECT				

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