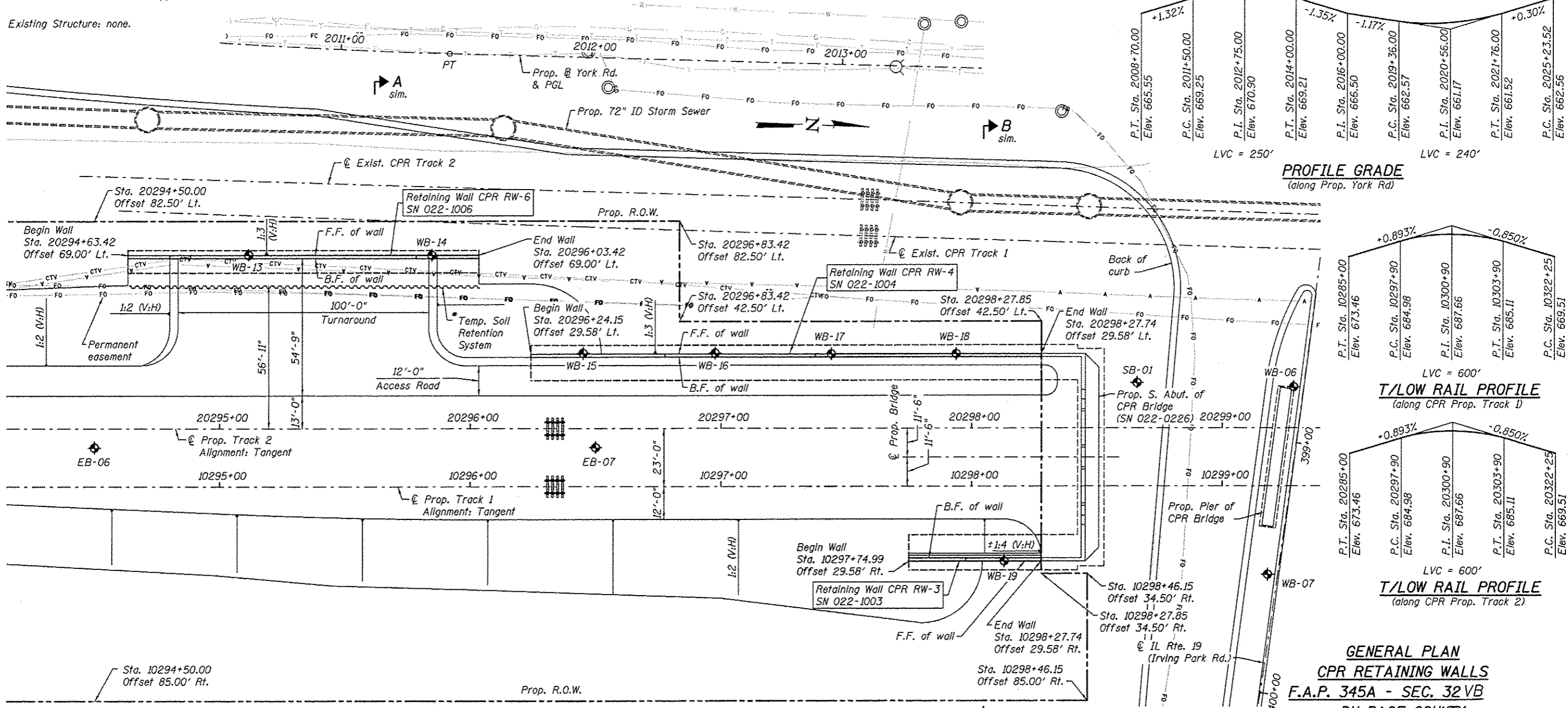


Bench Mark: TBM 4 - Chiseled "□" in southeast portion of cantilever traffic signal concrete foundation at northeast corner of York Rd. & IL 19; approx Sta. 397+64.78, offset 53.82' Lt., Elev. 668.85 (NAVD 88).

Existing Structure: none.



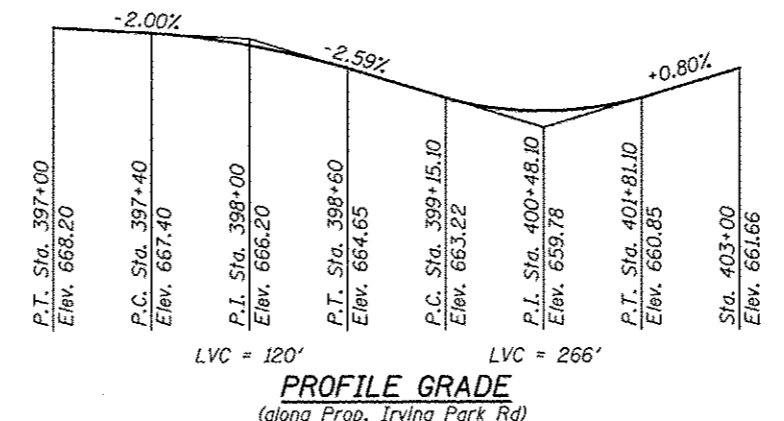
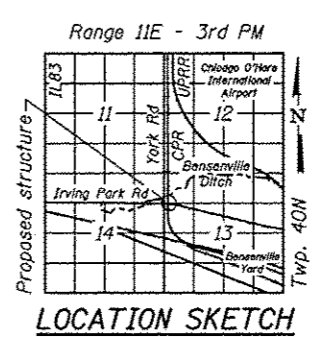
PROFILE GRADE
(along Prop. York Rd)

T/LOW RAIL PROFILE
(along CPR Prop. Track 1)

T/LOW RAIL PROFILE
(along CPR Prop. Track 2)

GENERAL PLAN
CPR RETAINING WALLS
F.A.P. 345A - SEC. 32 VB
DU PAGE COUNTY

- STATION 10300+33.90 TO 10303+95.86
STRUCTURE NO. 022-1001
- STATION 20300+33.90 TO 20303+95.86
STRUCTURE NO. 022-1002
- STATION 10297+74.99 TO 10298+27.74
STRUCTURE NO. 022-1003
- STATION 20296+24.15 TO 20298+27.74
STRUCTURE NO. 022-1004
- STATION 20303+88.61 TO 20305+38.61
STRUCTURE NO. 022-1005
- STATION 20294+63.42 TO 20296+03.42
STRUCTURE NO. 022-1006



APPROVED
For Structural Adequacy Only
[Signature]
Engineer of Bridges & Structures

DESIGN SPECIFICATIONS
2010 AREMA Manual for Railway Engineering
2006 Canadian Pacific Railway Requirements for Design of Steel and Concrete Bridges
2002 AASHTO Standard Specifications for Highway Bridges

DESIGN STRESSES
FIELD UNITS
f'c = 5,000 psi
fy = 60,000 psi (Reinforcement)

BY: *[Signature]* DATE: 11-02-2012
HDR ENGINEERING, INC.
SHTS. 203-238
LICENSE EXPIRES 11-30-2014

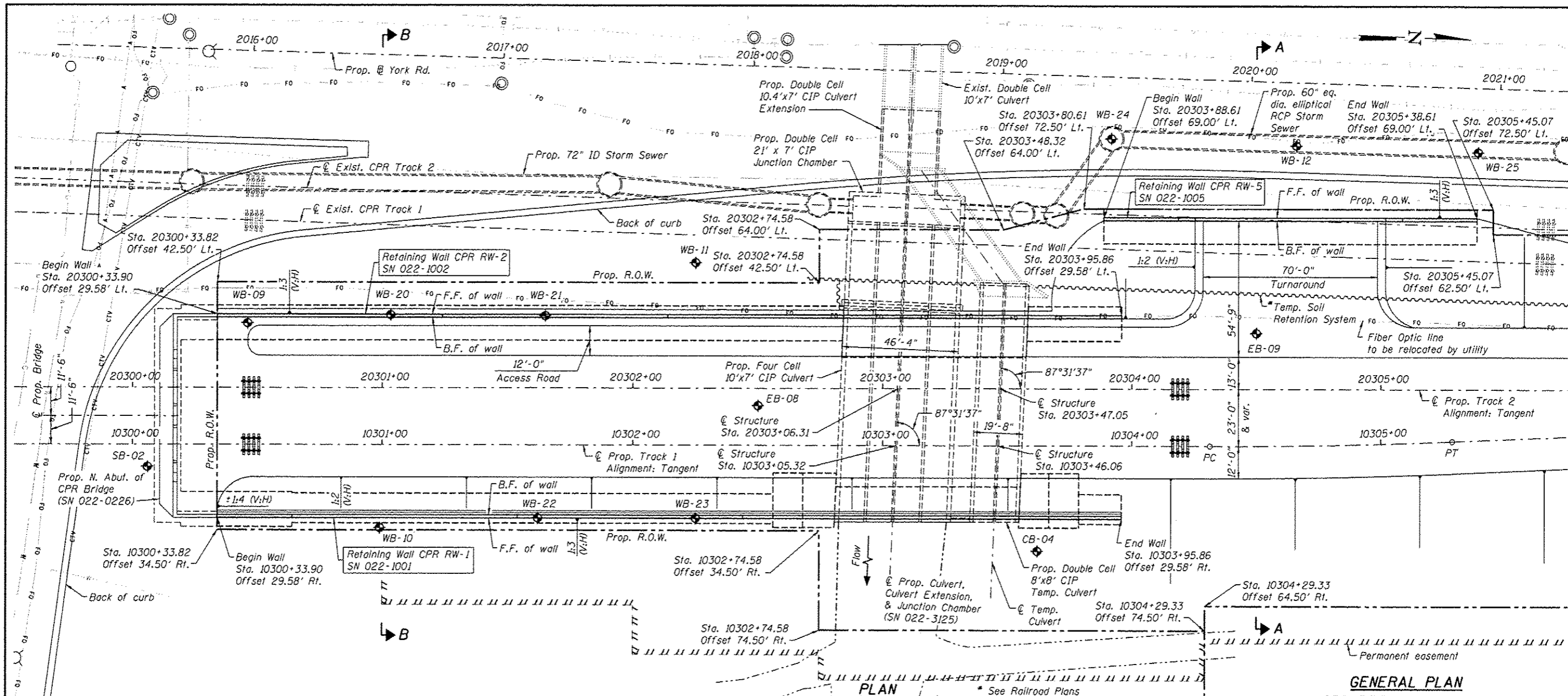
LOADING COOPER E90
(Walls RW-1 thru RW-4)

LOADING HS20-44
(Walls RW-2, RW-4, RW-5, and RW-6)

SEISMIC DATA
Seismic Performance Zone (SPZ) = 1
Design Spectral Acceleration at 1.0 sec. (SD1) = 0.089g
Design Spectral Acceleration at 0.2 sec. (SDS) = 0.152g
Soil Site Class = D

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<p>HDR HDR ENGINEERING, INC.</p>	USER NAME = jmiqus FILE NAME = WXXX-60B42-001-GPE.DGN PLOT SCALE = NONE PLOT DATE = 12/3/2012	DESIGNED - BWC CHECKED - JAR DRAWN - JM CHECKED - JAR	REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL PLAN CPR RETAINING WALLS SHEET NO. 1 OF 42 SHEETS	F.A.P. RTE. 345A SECTION 32VB COUNTY DU PAGE TOTAL SHEETS 388 SHEET NO. 203 CONTRACT NO. 60W01	ILLINOIS FED. AID PROJECT
	12/3/2012 4:35:46 PM					SHEET NO. 1 OF 42 SHEETS	



PLAN * See Railroad Plans

TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Porous Granular Embankment	Cu. Yd.	3,247
Removal and Disposal of Unsuitable Material for Structures	Cu. Yd.	6,214
Form Liner Textured Surface	Sq. Ft.	20,644
Reinforcement Bars, Epoxy Coated	Pound	482,800
Parapet Railing	Foot	750
Furnishing Steel Piles HP14x89	Foot	24,893
Driving Piles	Foot	24,893
Test Pile Steel HP14x89	Each	6
Name Plates	Each	6
Geocomposite Wall Drain	Sq. Yd.	2,093
Chain Link Fence, 4'	Foot	537
Pipe Underdrains for Structures 4"	Foot	1,327
Concrete Structures CPR Special	Cu. Yd.	3,715.1

INDEX OF SHEETS

1. General Plan
2. General Plan
3. Elevations
- 4.-5. Typical Details
6. Typical Sections
- 7.-11. CPR RW-1 Plan and Elevation
12. CPR RW-1 Sections & Bill of Material
- 13.-17. CPR RW-2 Plan and Elevation
18. CPR RW-2 Sections & Bill of Material
19. CPR RW-3 Plan and Elevation
- 20.-22. CPR RW-4 Plan and Elevation
- 23.-24. CPR RW-5 Plan and Elevation
25. CPR RW-5 Sections & Bill of Material
- 26.-27. CPR RW-6 Plan and Elevation
28. CPR RW-6 Sections & Bill of Material
- 29.-35. Rolling Details
36. HP Pile Details
- 37.-42. Soil Boring Logs

GENERAL NOTES

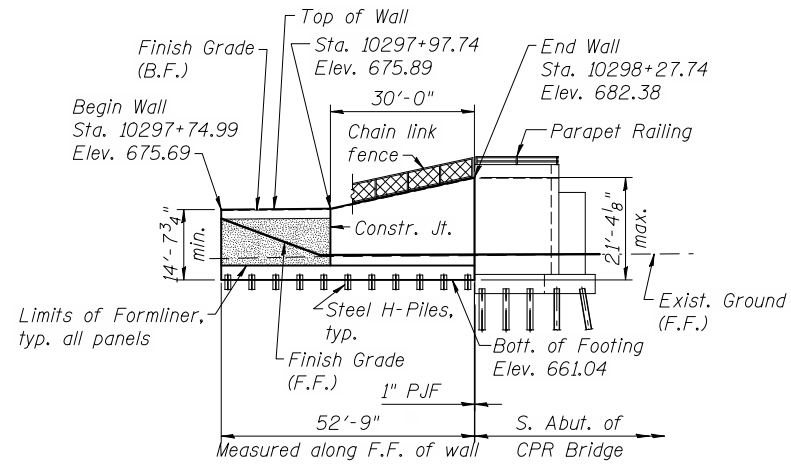
1. Reinforcement bars shall conform to the requirements of ASTM A615, Gr. 60.
2. Reinforcement bars designated (E) shall be epoxy coated (minimum thickness 7 mils).
3. Concrete reinforcement cover shall be 3 inches unless noted otherwise.
4. Pile splices, if needed, shall be a minimum of 15' below ground surface.
5. Form Liner Textured Surface shall be ashlar stone pattern with 1" maximum relief & 3/4" minimum relief.
6. For Sections A-A & B-B, see Sheet 6.

**GENERAL PLAN
CPR RETAINING WALLS
F.A.P. 345A - SEC. 32VB
DU PAGE COUNTY**

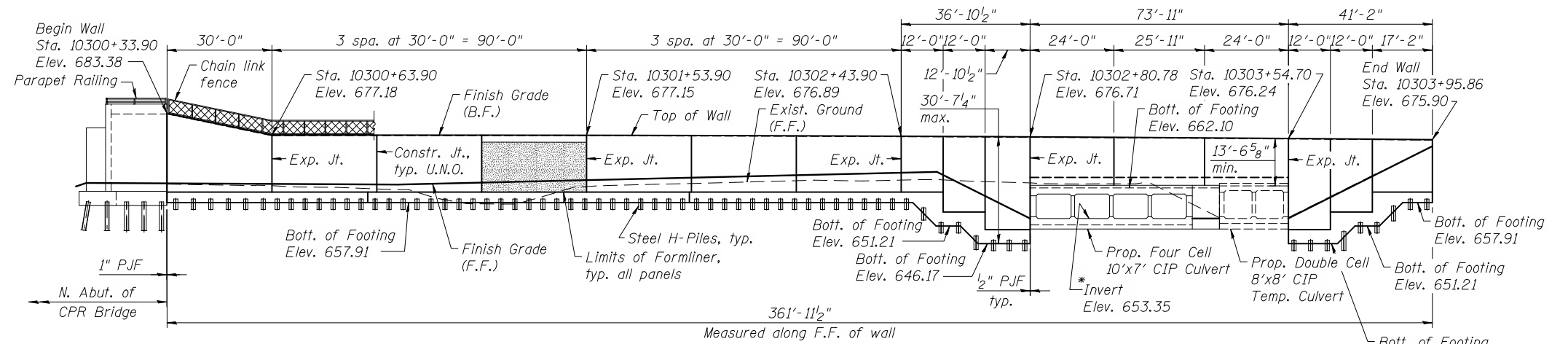
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STRUCTURE NO. 022-1001**
- STATION 20300+33.90 TO 20303+95.86
STRUCTURE NO. 022-1002**
- STATION 10297+74.99 TO 10298+27.74
STRUCTURE NO. 022-1003**
- STATION 20296+24.15 TO 20298+27.74
STRUCTURE NO. 022-1004**
- STATION 20303+88.61 TO 20305+38.61
STRUCTURE NO. 022-1005**
- STATION 20294+63.42 TO 20296+03.42
STRUCTURE NO. 022-1006**

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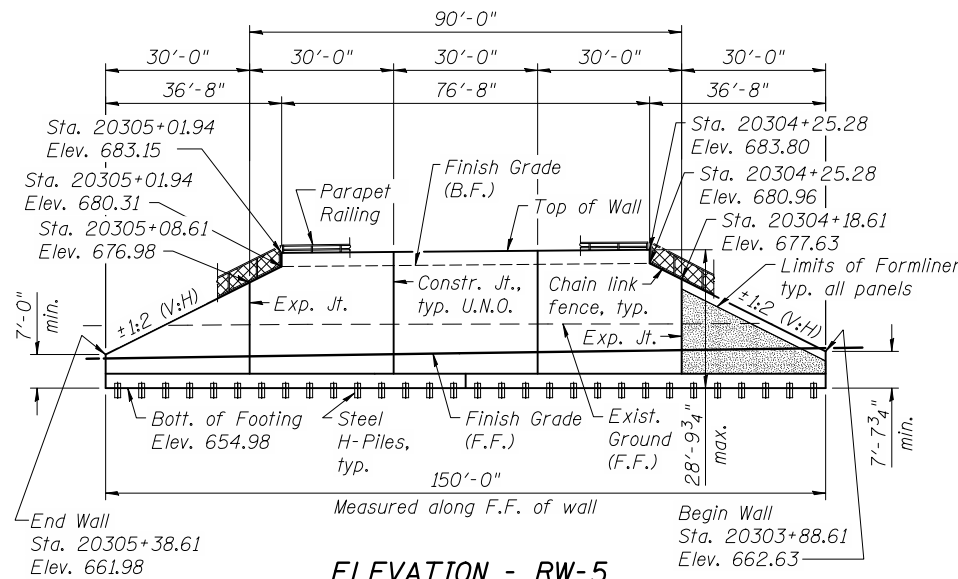


ELEVATION - RW-3
SN 022-1003
(Front Face Looking West)

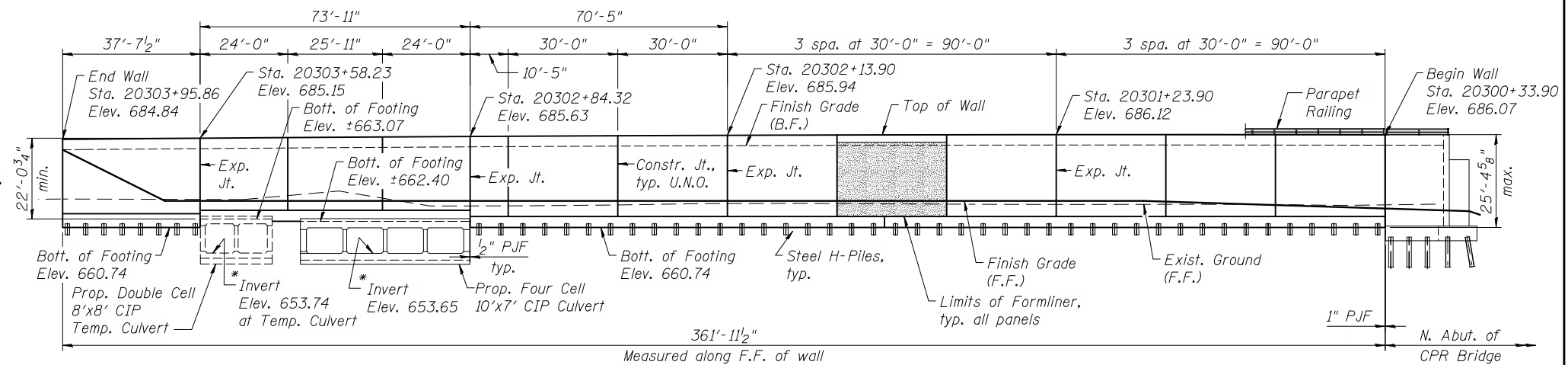


ELEVATION - RW-1
SN 022-1001
(Front Face Looking West)

* Invert elevation taken at front face of retaining wall

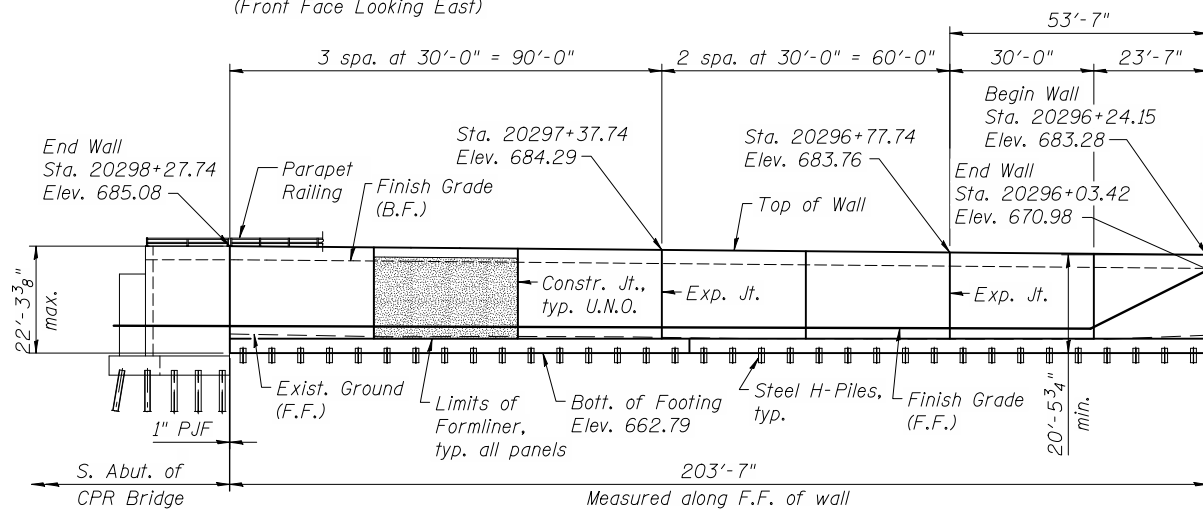


ELEVATION - RW-5
SN 022-1005
(Front Face Looking East)

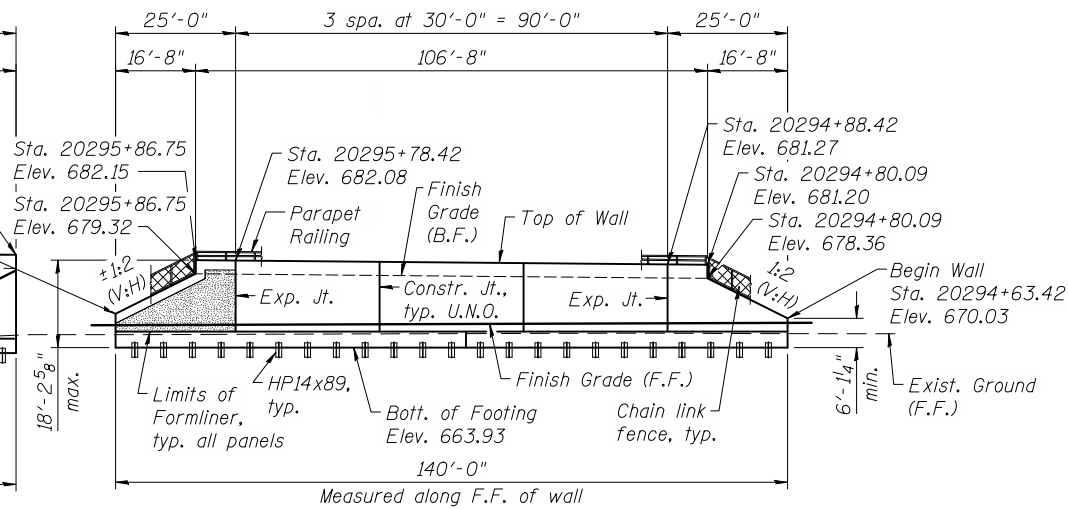


ELEVATION - RW-2
SN 022-1002
(Front Face Looking East)

* Invert elevation taken at front face of retaining wall



ELEVATION - RW-4
SN 022-1004
(Front Face Looking East)



ELEVATION - RW-6
SN 022-1006
(Front Face Looking East)

**ELEVATIONS
CPR RETAINING WALLS
F.A.P. 345A - SEC. 32VB
DU PAGE COUNTY**

- STATION 10300+33.90 TO 10303+95.86
STRUCTURE NO. 022-1001
- STATION 20300+33.90 TO 20303+95.86
STRUCTURE NO. 022-1002
- STATION 10297+74.99 TO 10298+27.74
STRUCTURE NO. 022-1003
- STATION 20296+24.15 TO 20298+27.74
STRUCTURE NO. 022-1004
- STATION 20303+88.61 TO 20305+38.61
STRUCTURE NO. 022-1005
- STATION 20294+63.42 TO 20296+03.42
STRUCTURE NO. 022-1006

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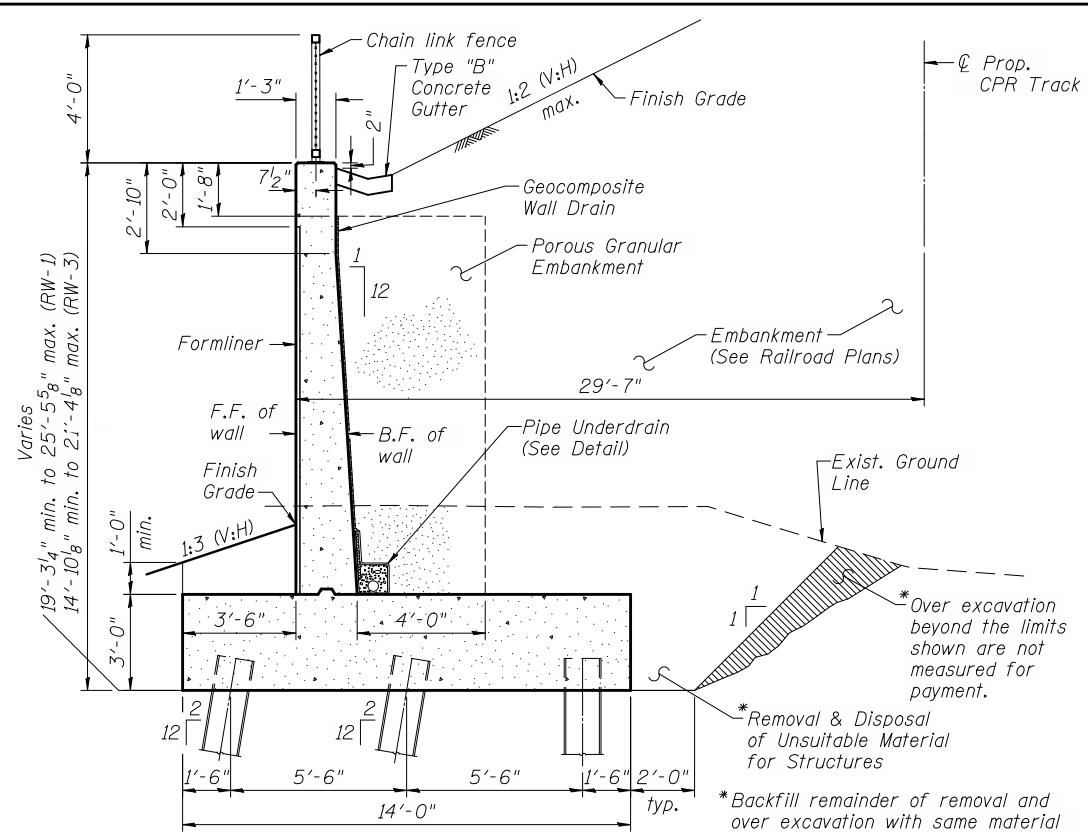
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

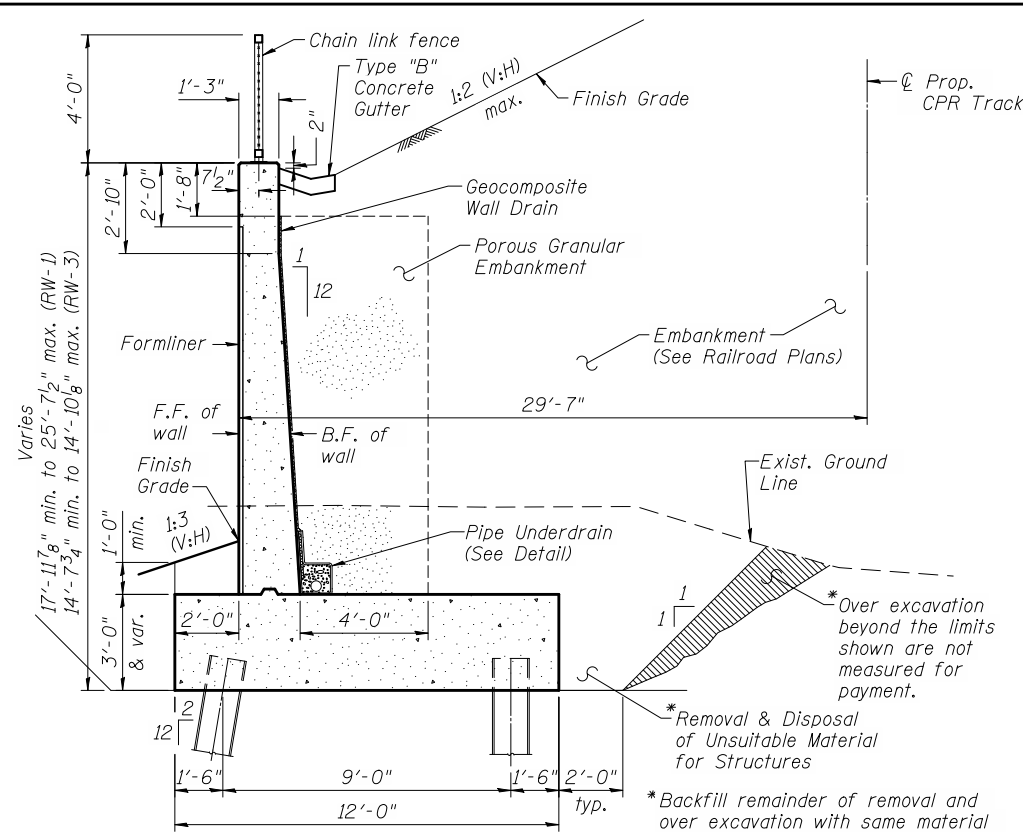
**ELEVATIONS
CPR RETAINING WALLS**
SHEET NO. 3 OF 42 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				



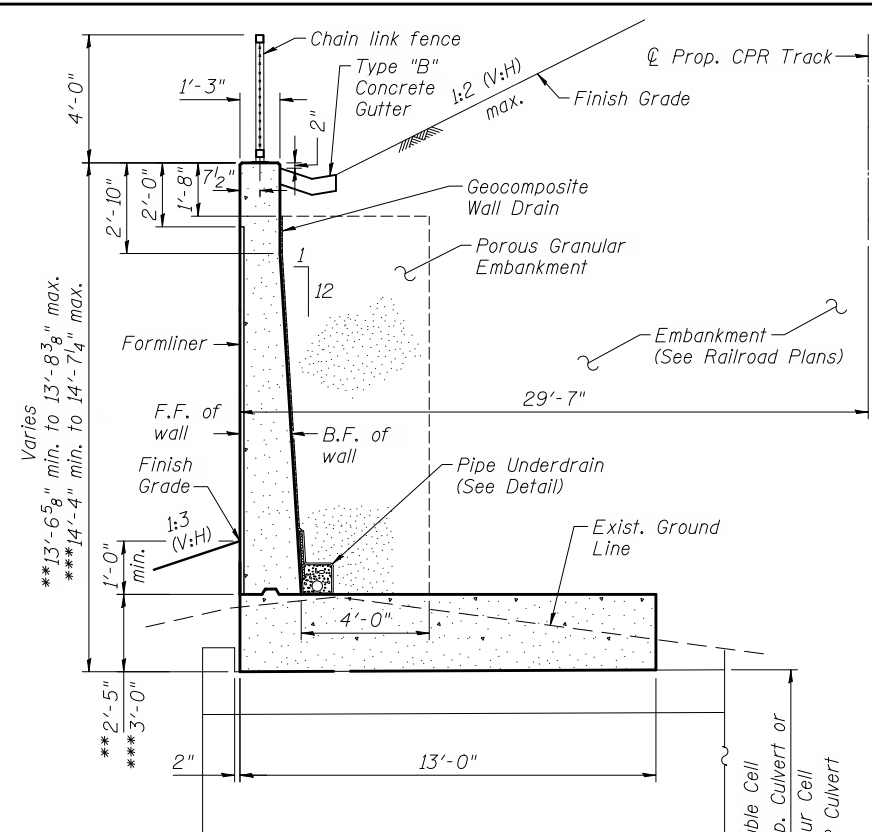
TYPICAL SECTION

Walls RW-1 SN 022-1001 (Sta. 10300+33.90 to Sta. 10300+63.90) & RW-3 SN 022-1003 (Sta. 10297+97.74 to Sta. 10298+27.74) (Looking downstation)



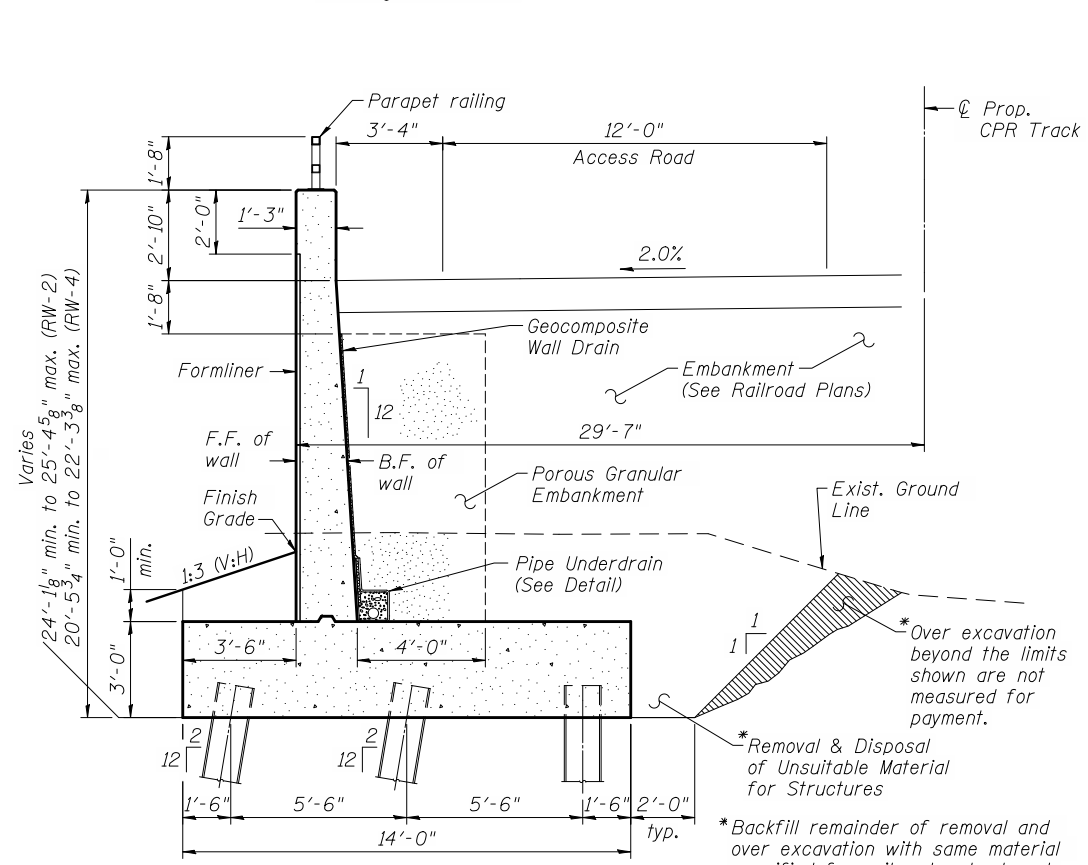
TYPICAL SECTION

Wall RW-1 SN 022-1001 (Sta. 10300+63.90 to Sta. 10302+55.90 and Sta. 10303+78.69 to Sta. 10303+95.86) & RW-3 SN 022-1003 (Sta. 10297+74.99 to Sta. 10297+97.74) (Looking downstation)



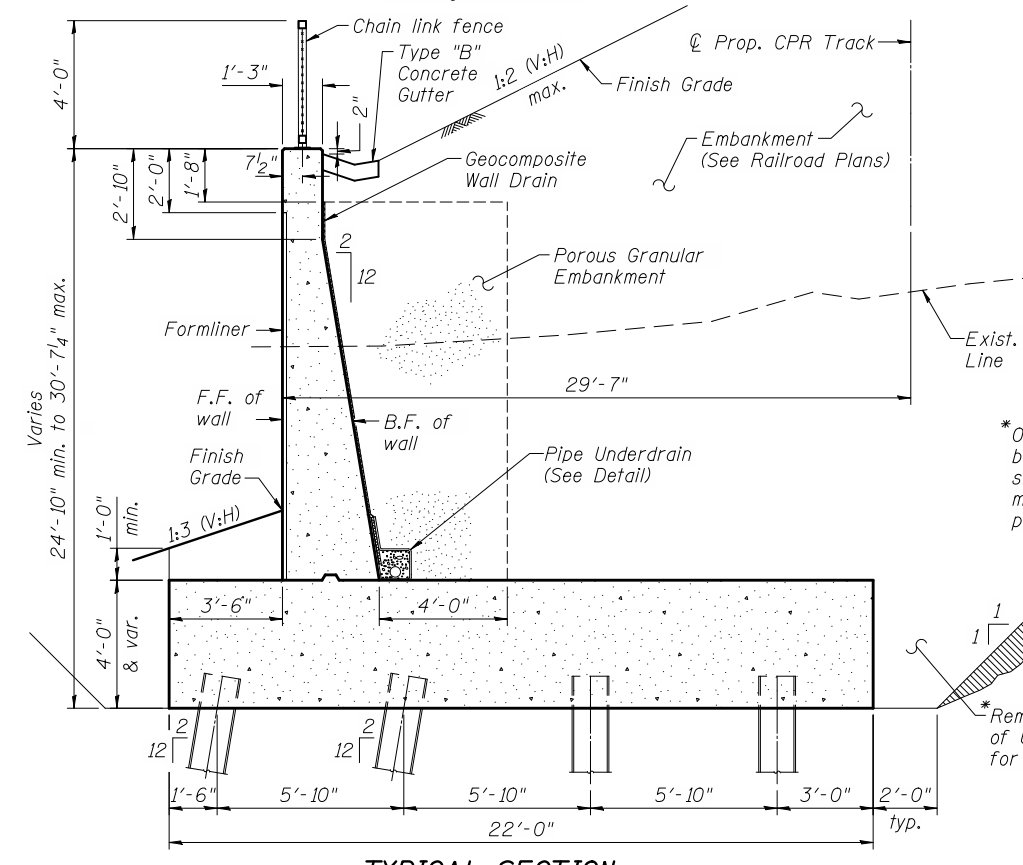
TYPICAL SECTION

Wall RW-1 SN 022-1001 (Sta. 10302+80.78 to Sta. 10303+54.70) (Looking downstation) Wall RW-2 SN 022-1002 similar (Sta. 20302+84.32 to Sta. 20303+58.23)



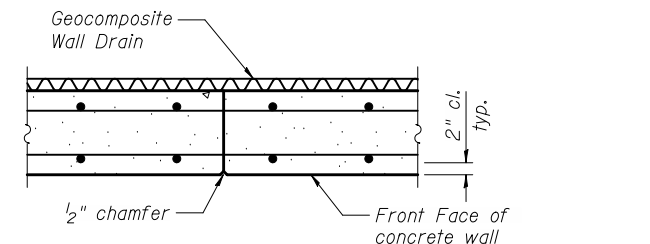
TYPICAL SECTION

Walls RW-2 SN 022-1002 & RW-4 SN 022-1004

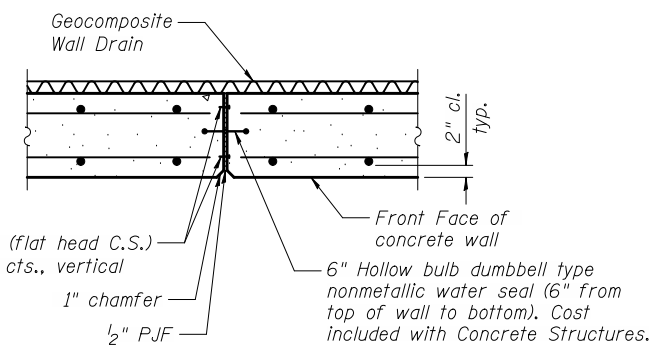


TYPICAL SECTION

Wall RW-1 SN 022-1001 (Sta. 10302+55.90 to Sta. 10302+80.78 and Sta. 10303+54.70 to Sta. 10303+78.69) (Looking downstation)



CONSTRUCTION JOINT DETAIL



EXPANSION JOINT DETAIL

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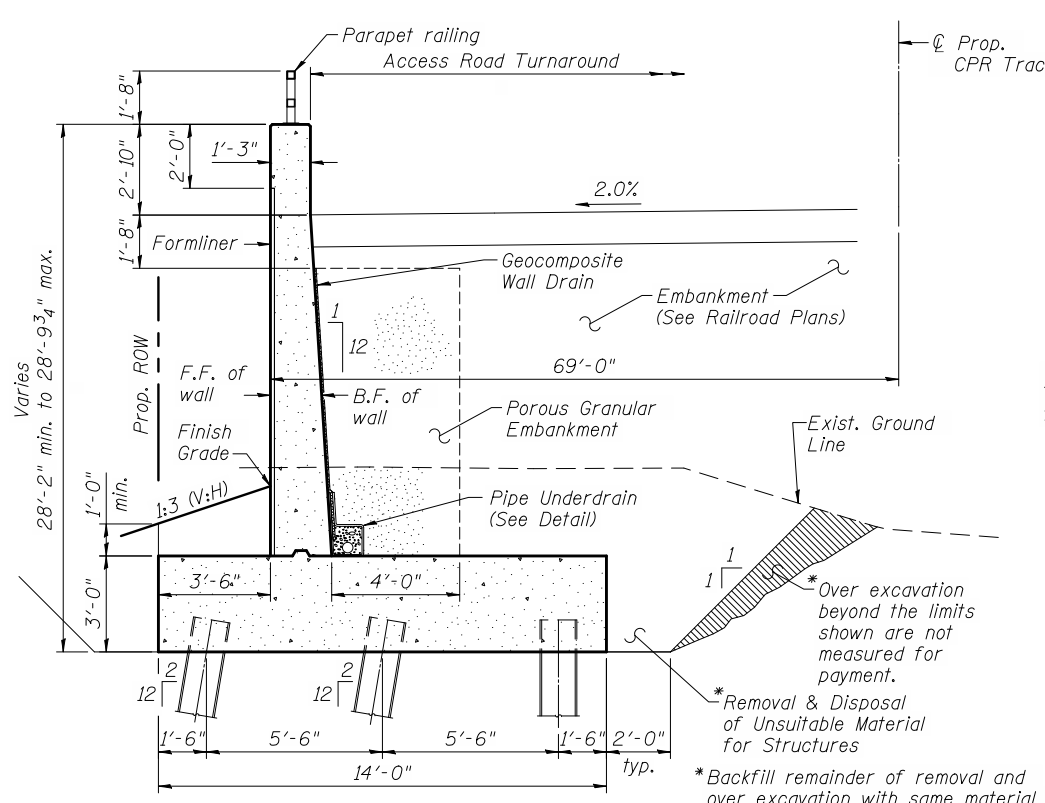
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

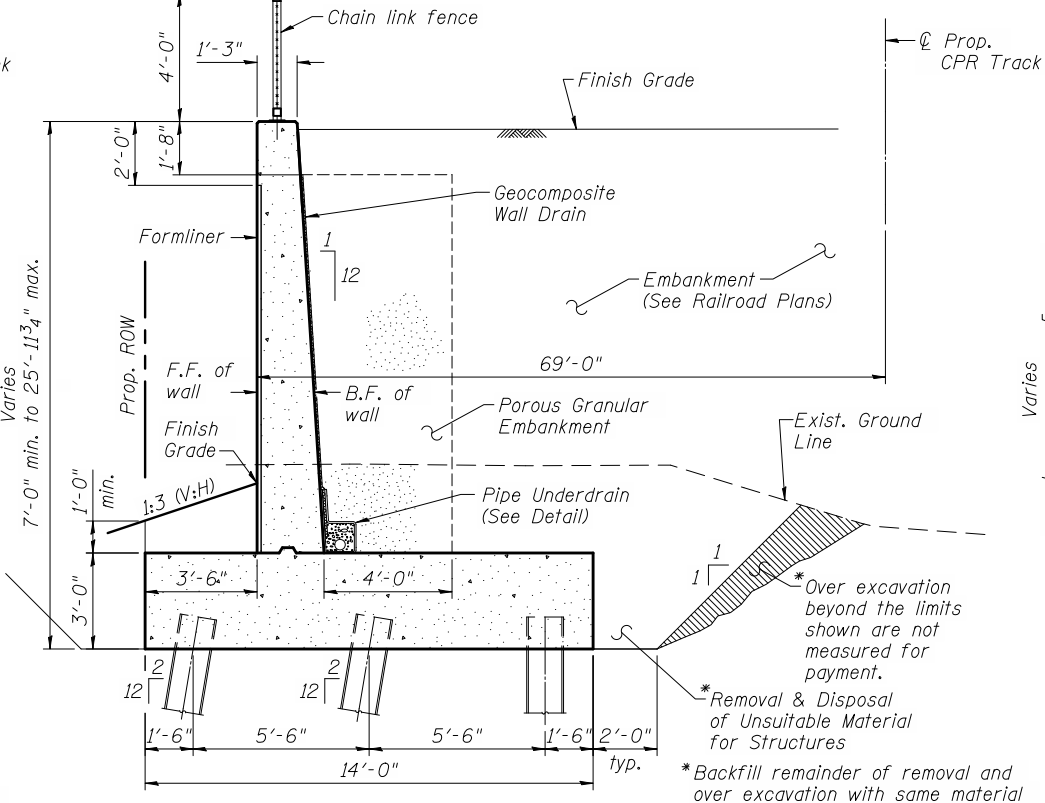
**TYPICAL DETAILS
CPR RETAINING WALLS**

SHEET NO. 4 OF 42 SHEETS

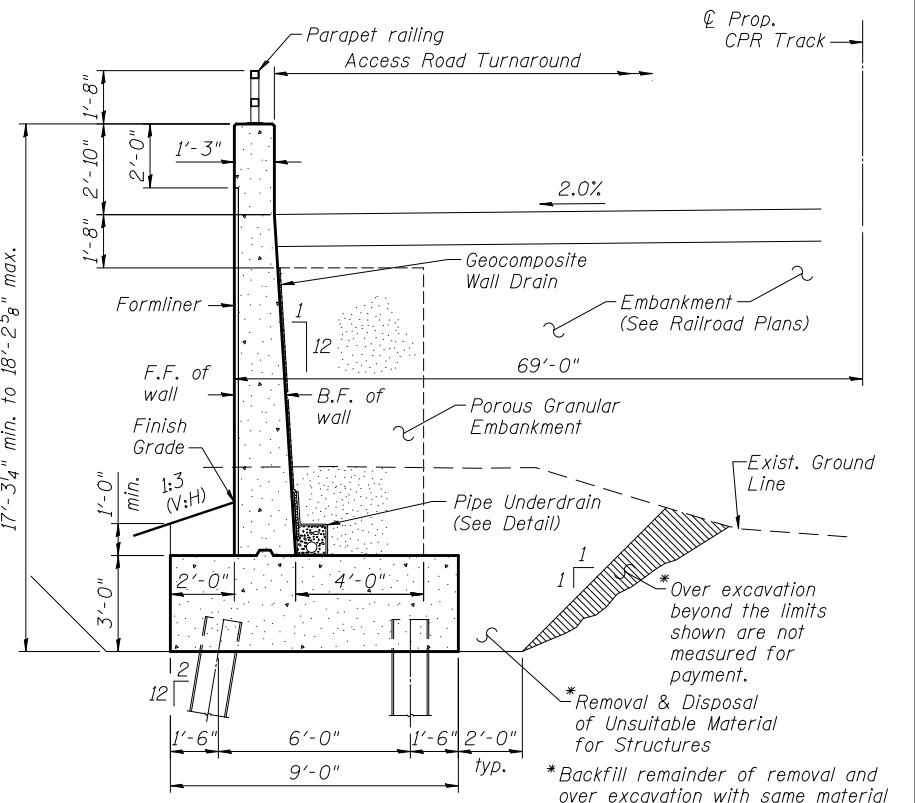
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CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				



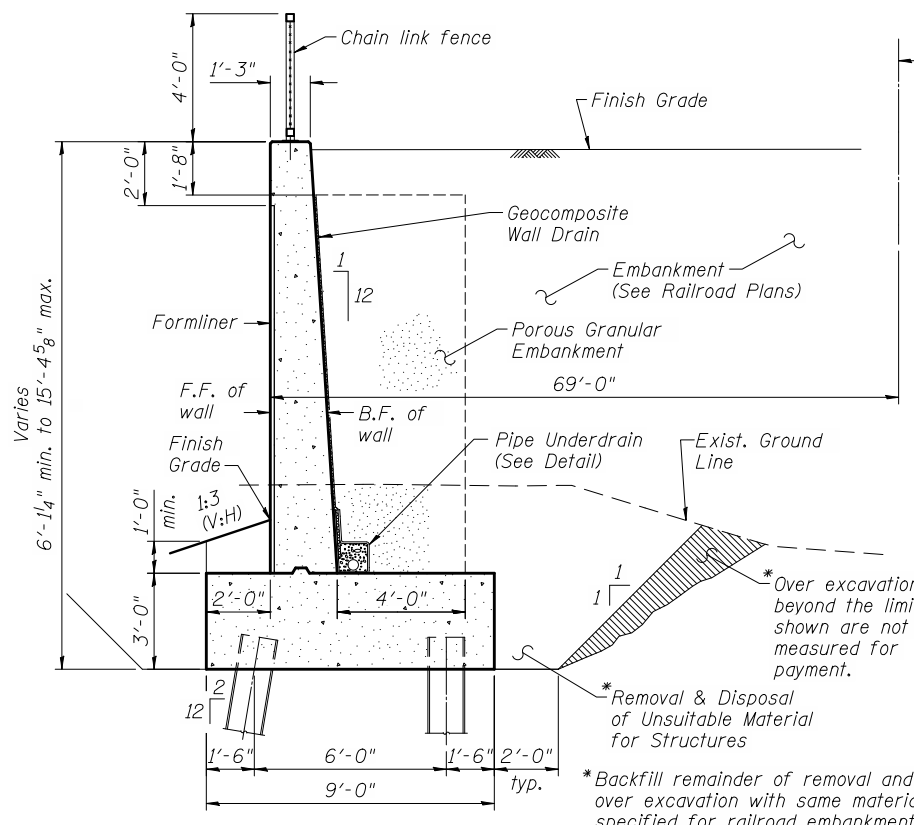
TYPICAL SECTION
Wall RW-5 SN 022-1005
at flat center



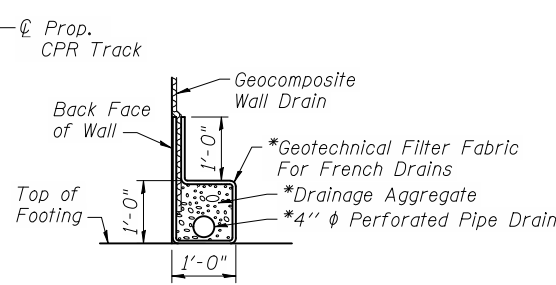
TYPICAL SECTION
Wall RW-5 SN 022-1005
at sloped ends



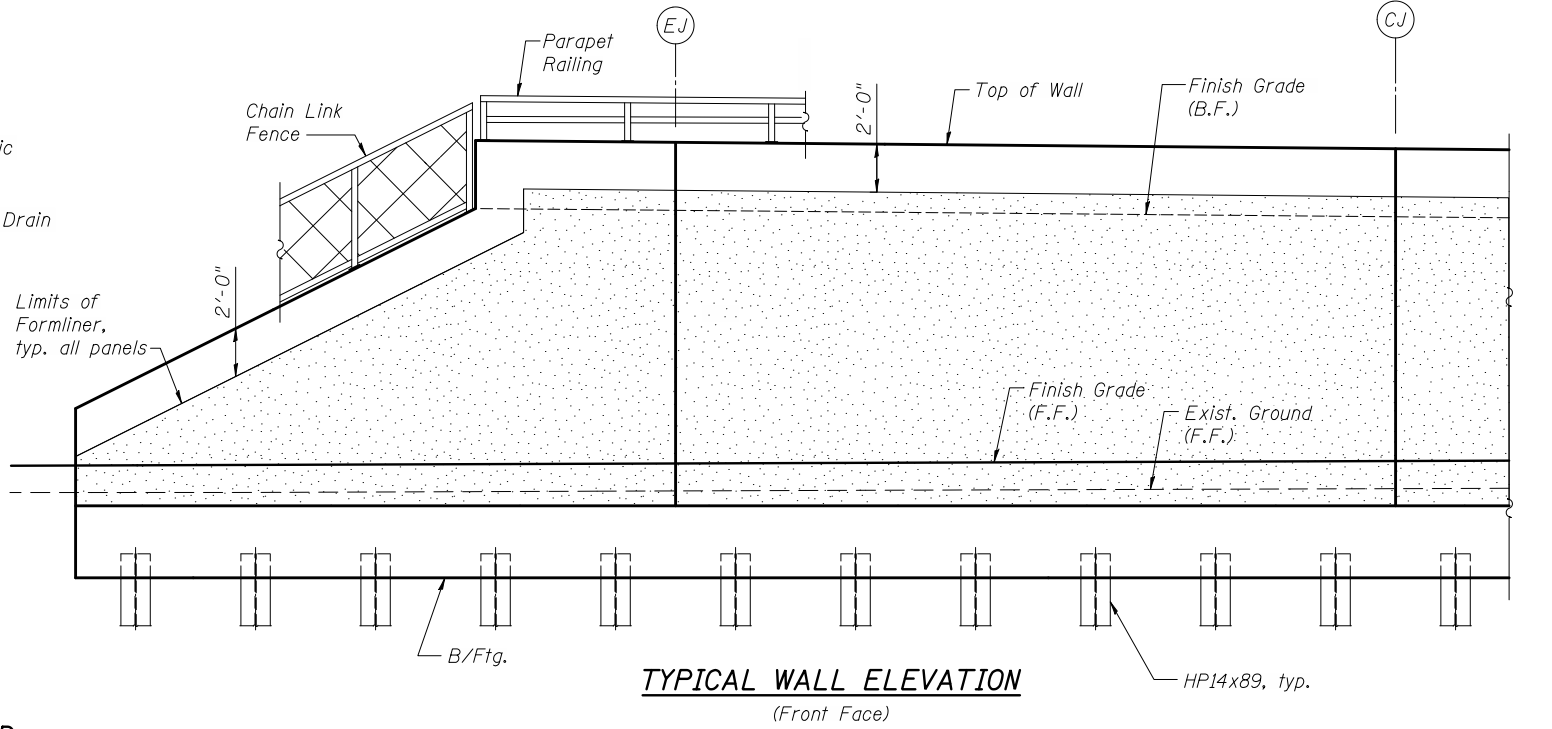
TYPICAL SECTION
Wall RW-6 SN 022-1006
at flat center



TYPICAL SECTION
Wall RW-6 SN 022-1006
at sloped ends



PIPE UNDERDRAIN DETAIL
*Included in the cost of
"Pipe Underdrains for Structures"



TYPICAL WALL ELEVATION
(Front Face)

LEGEND
(EJ) = Expansion joint
(CJ) = Construction joint

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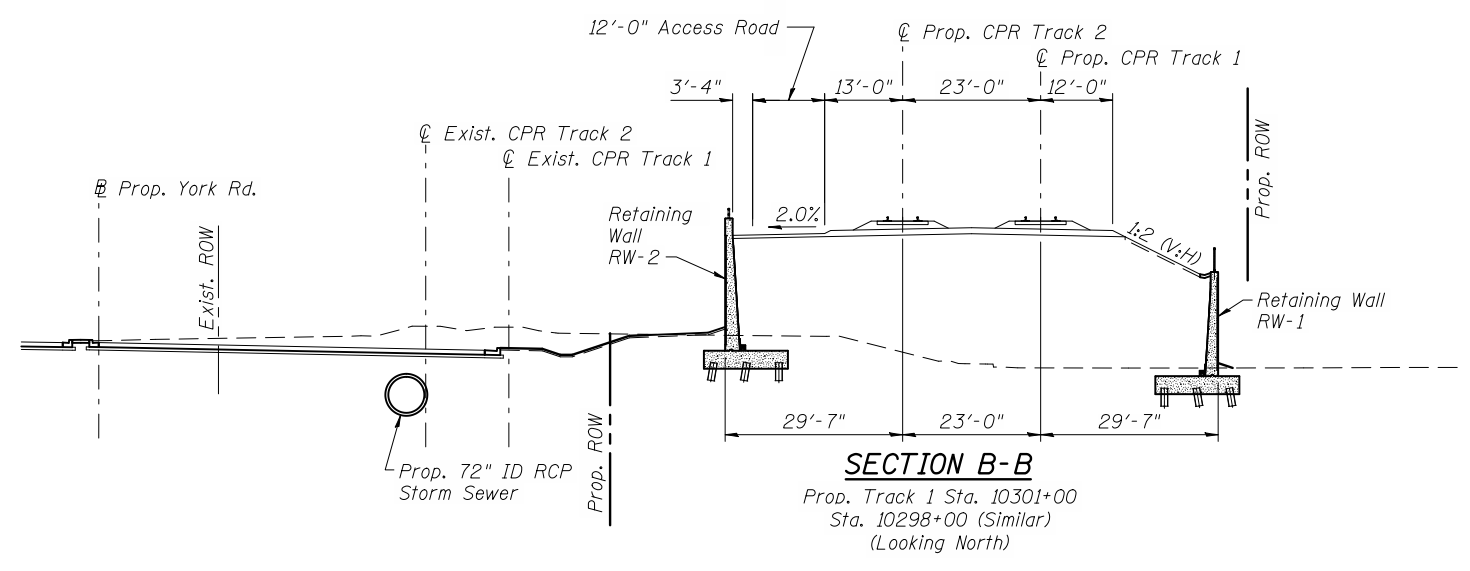
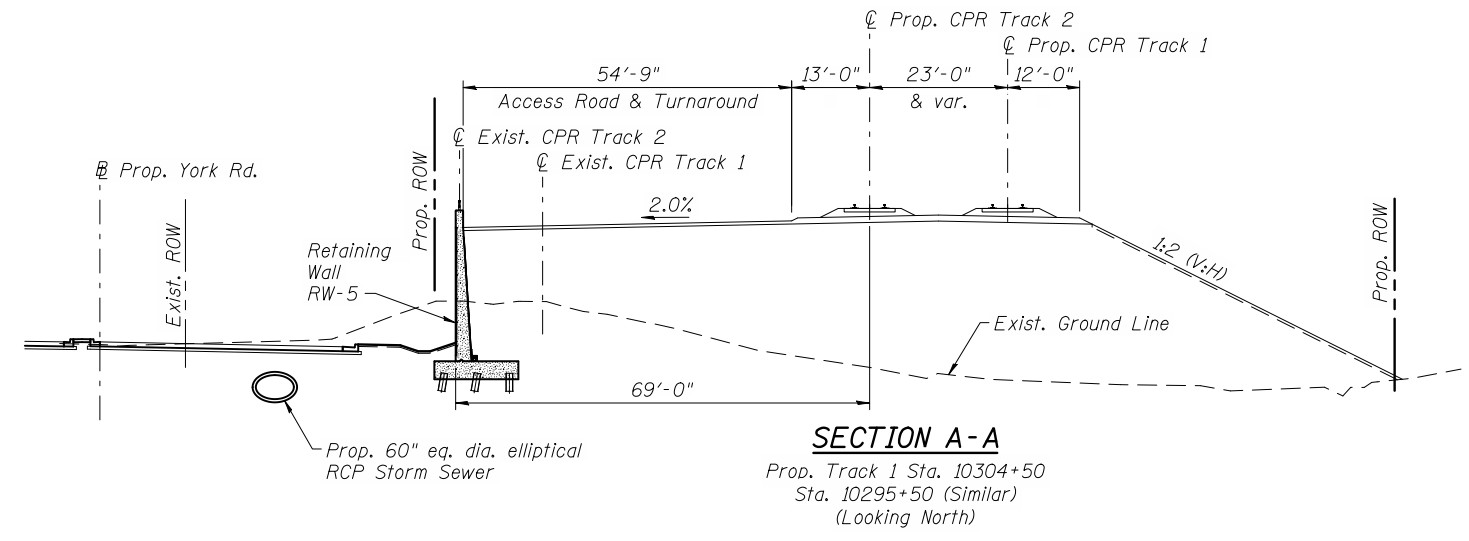
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TYPICAL DETAILS
CPR RETAINING WALLS**
SHEET NO. 5 OF 42 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345A	32VB	DU PAGE	388	207
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ILLINOIS FED. AID PROJECT				

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STATION 10300+33.90
 TO 10303+95.86
 BUILT 20__ BY
 STATE OF ILLINOIS
 LOADING COOPER E90
 STRUCTURE NO. 022-1001

NAME PLATE
 Wall CPR RW-1
 See Std. 515001

STATION 20300+33.90
 TO 20303+95.86
 BUILT 20__ BY
 STATE OF ILLINOIS
 LOADING COOPER E90
 & HS20-44
 STRUCTURE NO. 022-1002

NAME PLATE
 Wall CPR RW-2
 See Std. 515001

STATION 10297+74.99
 TO 10298+27.74
 BUILT 20__ BY
 STATE OF ILLINOIS
 LOADING COOPER E90
 STRUCTURE NO. 022-1003

NAME PLATE
 Wall CPR RW-3
 See Std. 515001

STATION 20296+24.15
 TO 20298+27.74
 BUILT 20__ BY
 STATE OF ILLINOIS
 LOADING COOPER E90
 & HS20-44
 STRUCTURE NO. 022-1004

NAME PLATE
 Wall CPR RW-4
 See Std. 515001

STATION 20303+88.61
 TO 20305+38.61
 BUILT 20__ BY
 STATE OF ILLINOIS
 LOADING HS20-44
 STRUCTURE NO. 022-1005

NAME PLATE
 Wall CPR RW-5
 See Std. 515001

STATION 20294+63.42
 TO 20296+03.42
 BUILT 20__ BY
 STATE OF ILLINOIS
 LOADING HS20-44
 STRUCTURE NO. 022-1006

NAME PLATE
 Wall CPR RW-6
 See Std. 515001

NOTE:
 1. See Sheets 1 and 2 for Locations of Sections A-A and B-B.

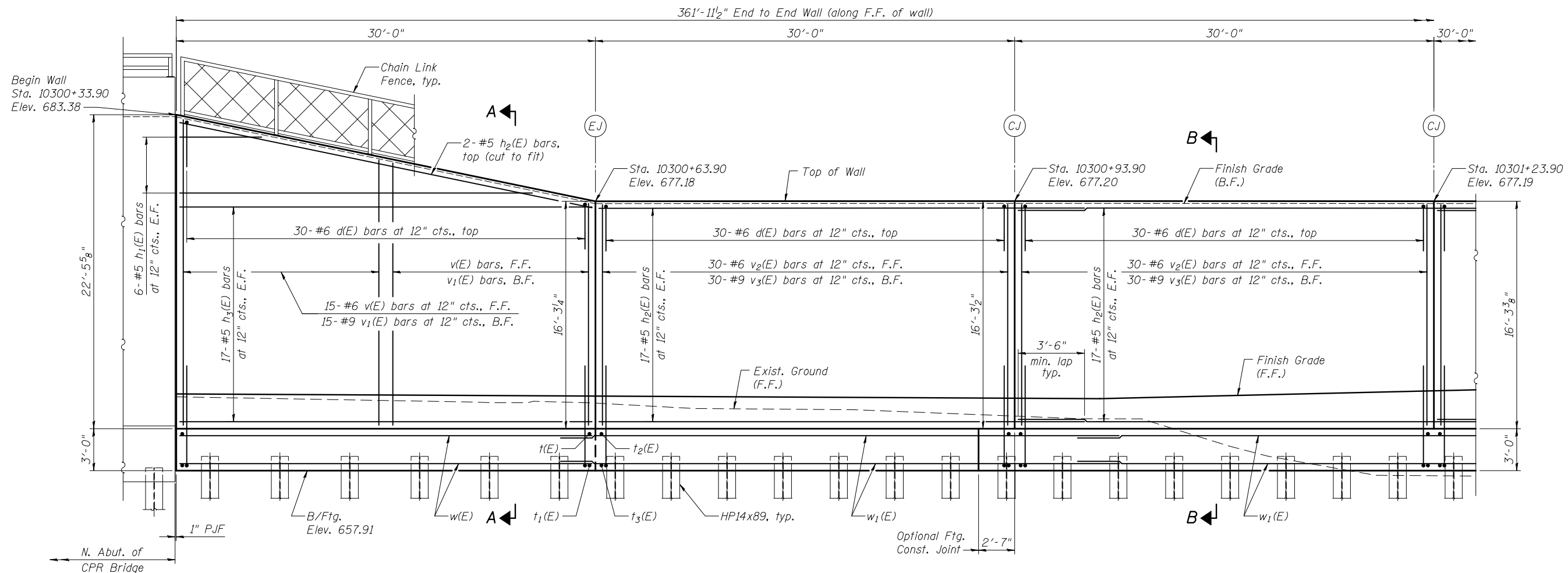


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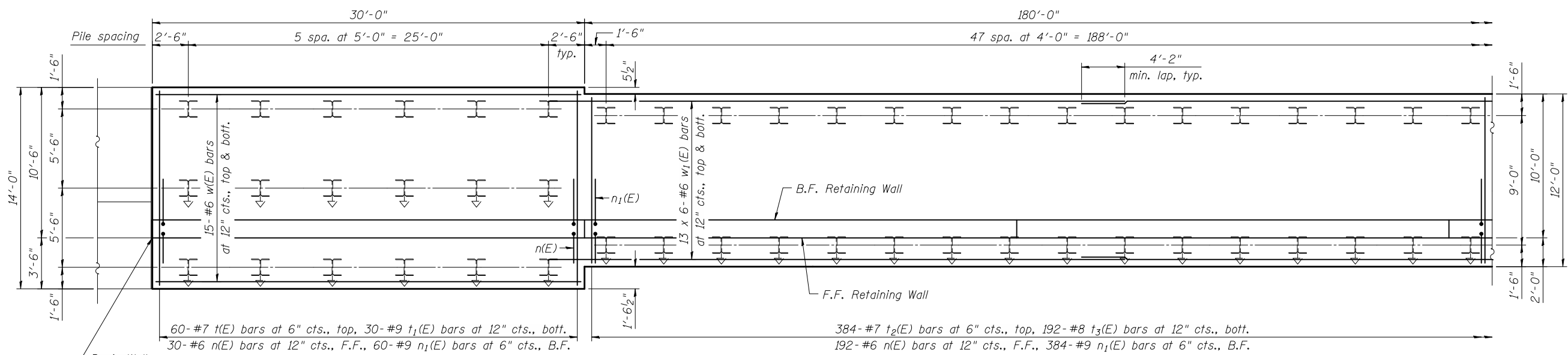
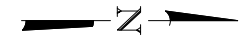
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS
 CPR RETAINING WALLS**
 SHEET NO. 6 OF 42 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345A	32VB	DU PAGE	388	208
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				



PARTIAL ELEVATION - CPR RW-1
(Front Face Looking West)



PARTIAL FOOTING PLAN - CPR RW-1

NOTES:
1. See Sheet 4 for Joint Details.

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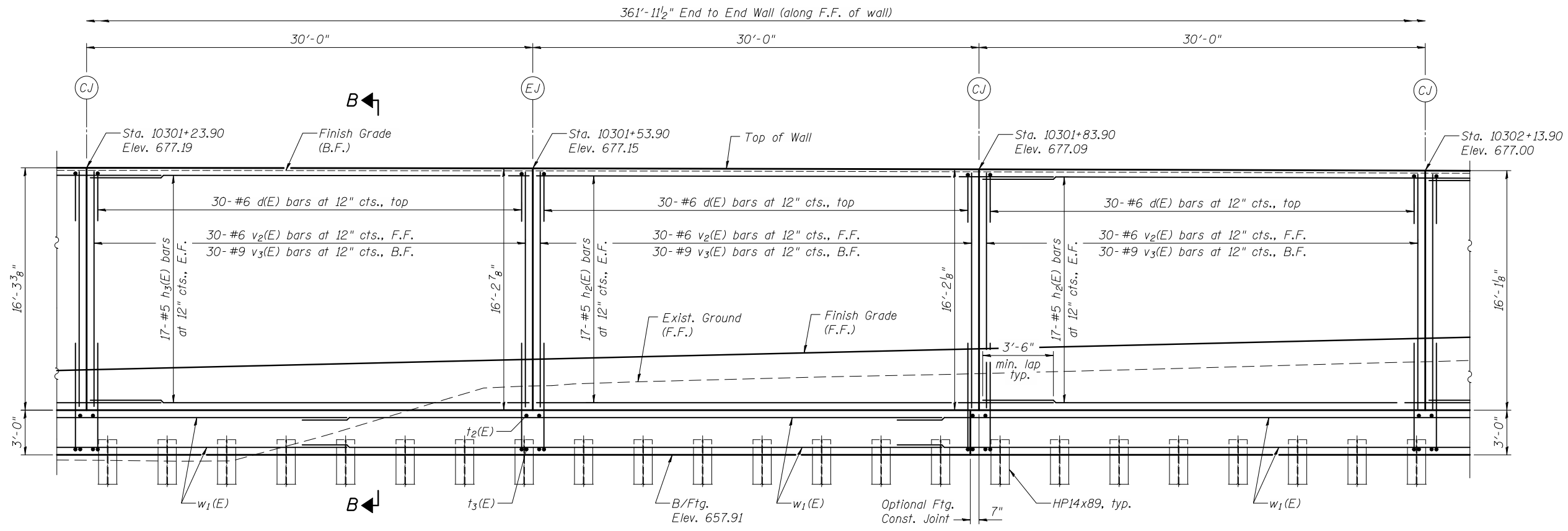
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

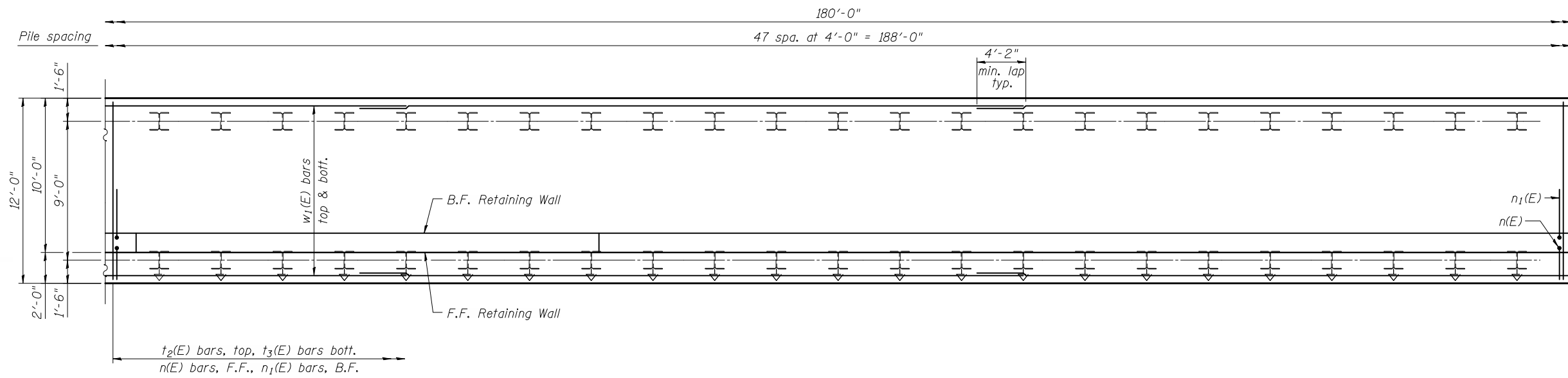
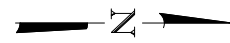
**CPR RW-1 PLAN AND ELEVATION
CPR RETAINING WALLS**

SHEET NO. 7 OF 42 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345A	32VB	DU PAGE	388	209
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				



PARTIAL ELEVATION - CPR RW-1
(Front Face Looking West)



PARTIAL FOOTING PLAN - CPR RW-1

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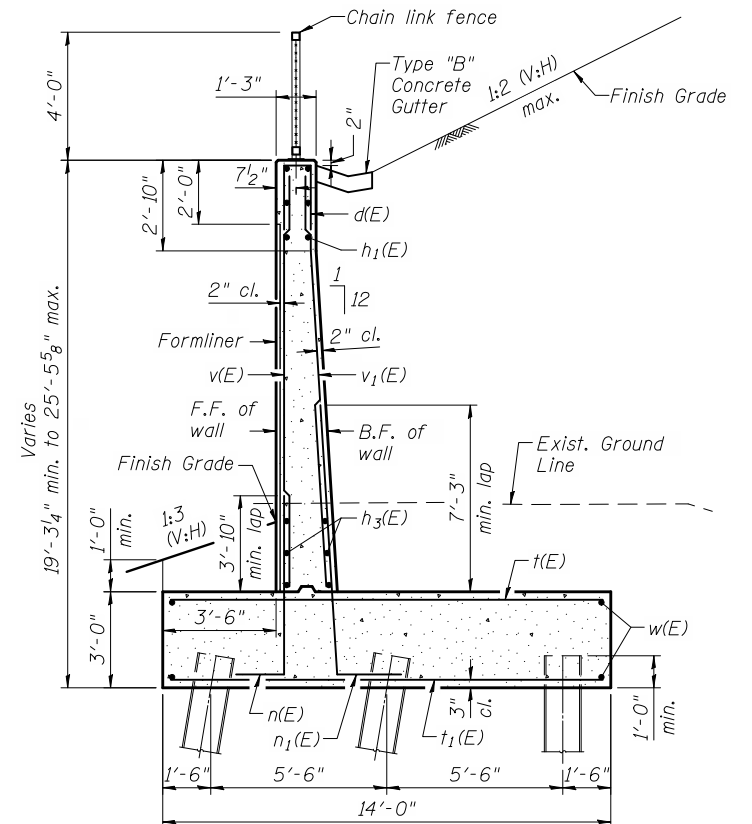
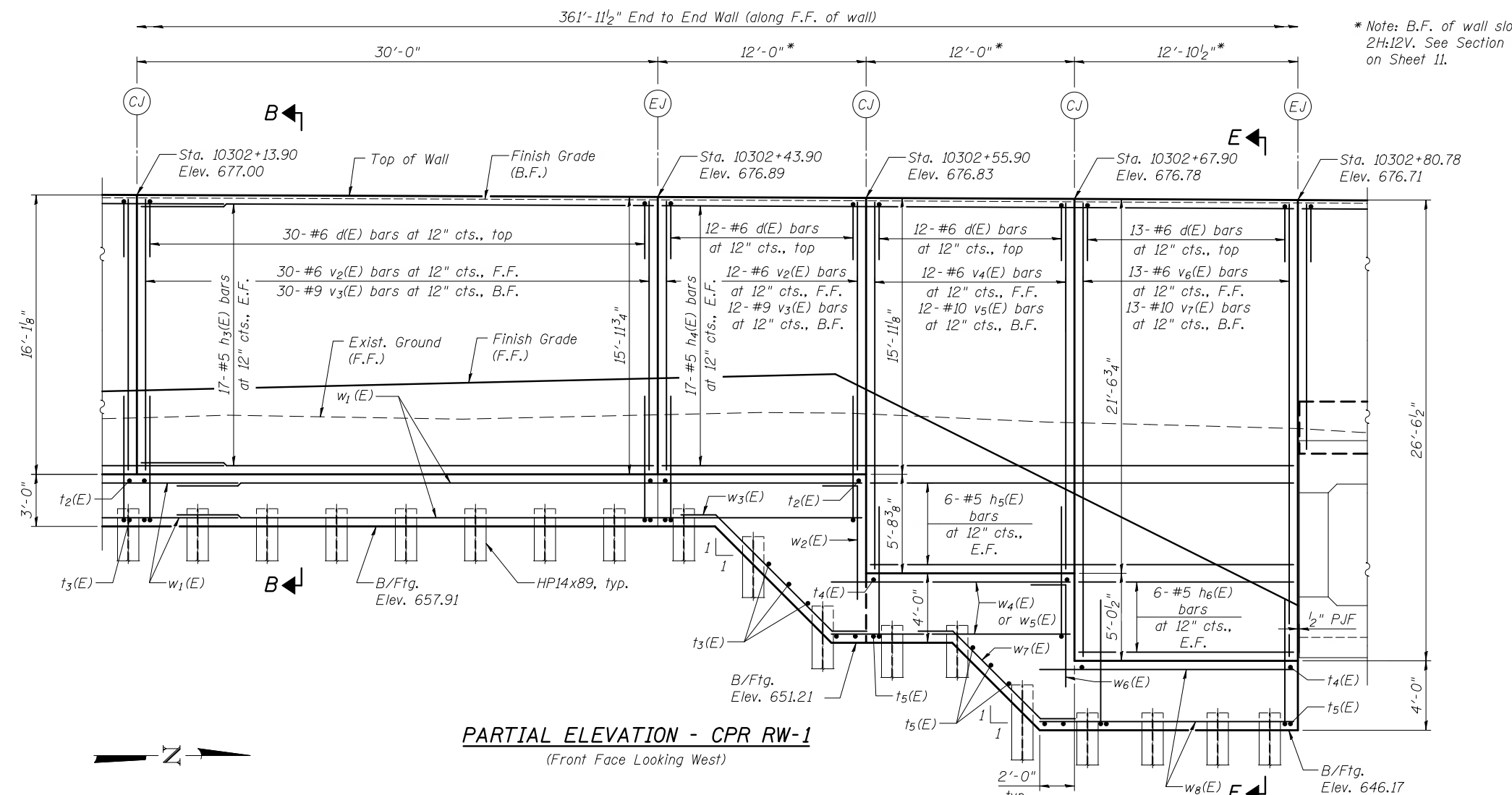
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CPR RW-1 PLAN AND ELEVATION
CPR RETAINING WALLS**

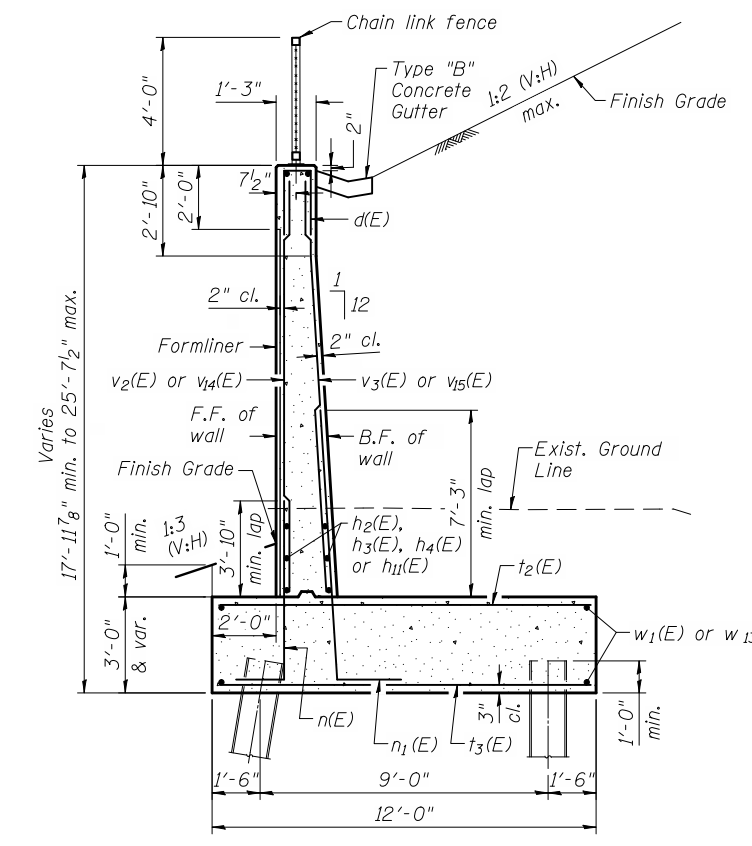
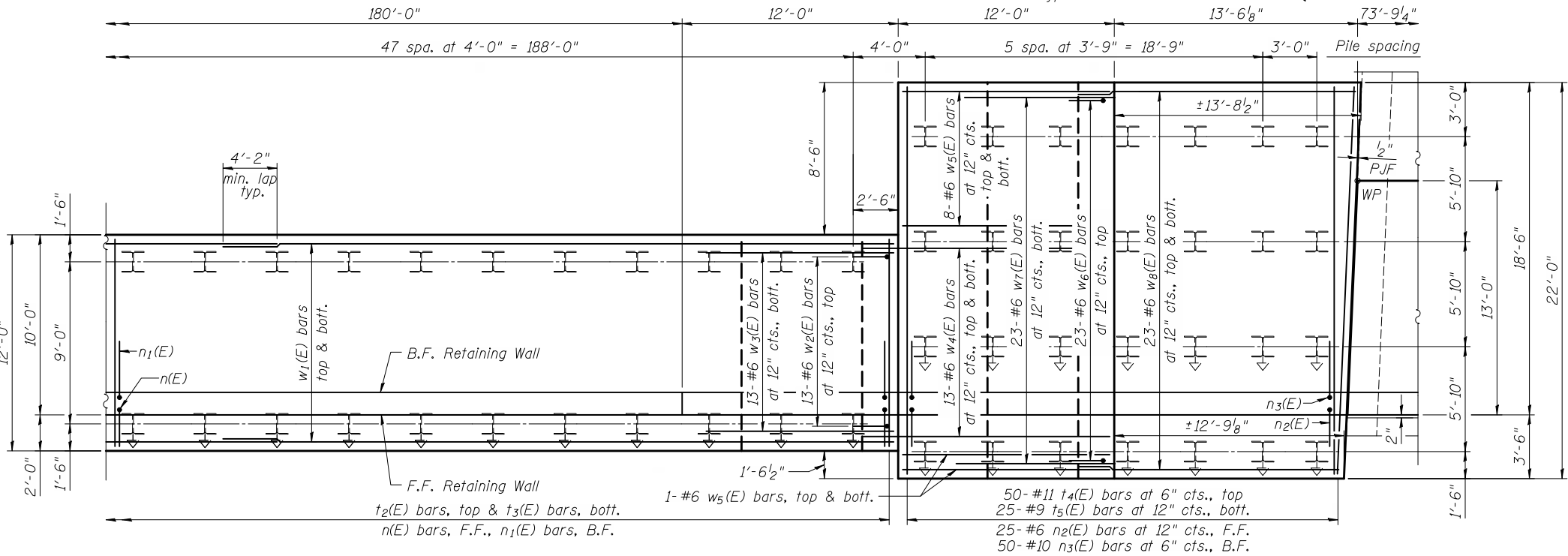
SHEET NO. 8 OF 42 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 60W01				

ILLINOIS FED. AID PROJECT



SECTION A-A
(Looking downstation)



SECTION B-B
(Looking downstation)

* Note: B.F. of wall sloped 2H:12V. See Section E-E on Sheet 11.

PARTIAL ELEVATION - CPR RW-1
(Front Face Looking West)

PARTIAL FOOTING PLAN - CPR RW-1

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jmlgus



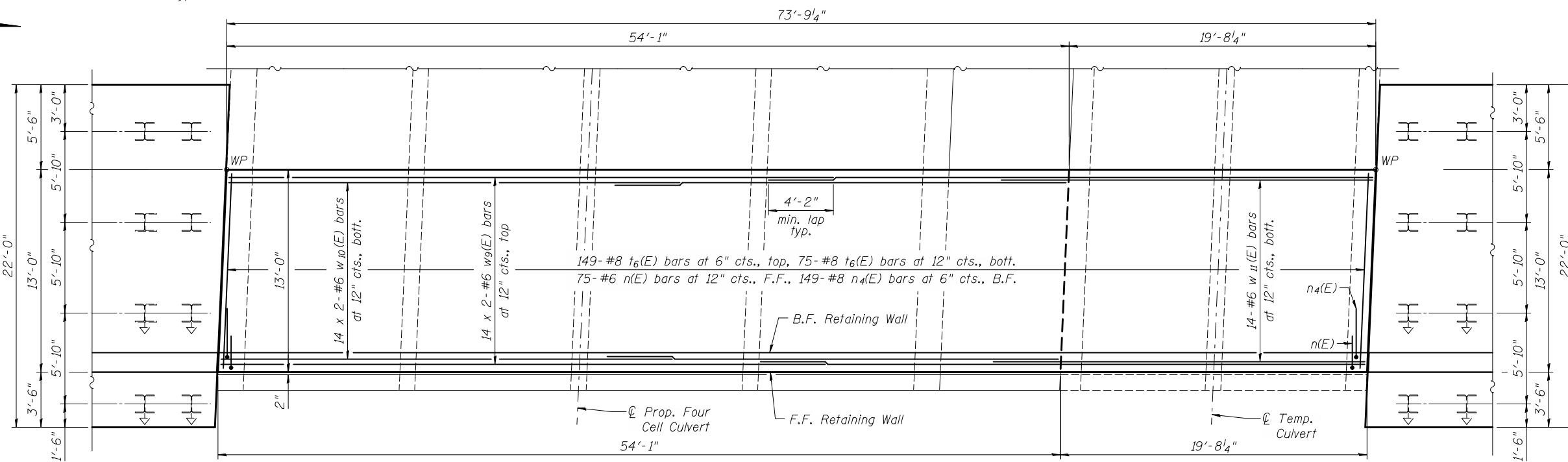
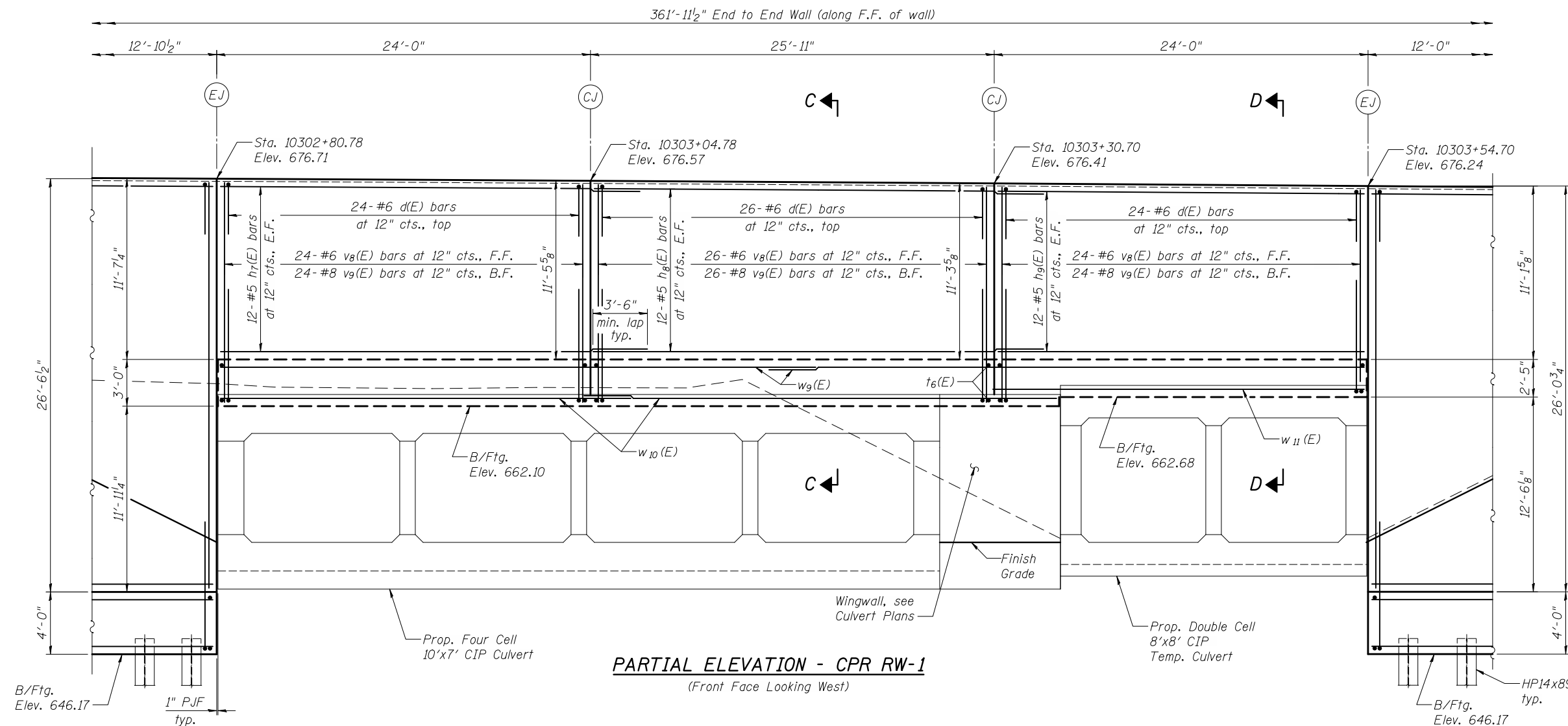
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CPR RW-1 PLAN AND ELEVATION
CPR RETAINING WALLS**

SHEET NO. 9 OF 42 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345A	32VB	DU PAGE	388	211
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				



PARTIAL ELEVATION - CPR RW-1
(Front Face Looking West)

PARTIAL FOOTING PLAN - CPR RW-1

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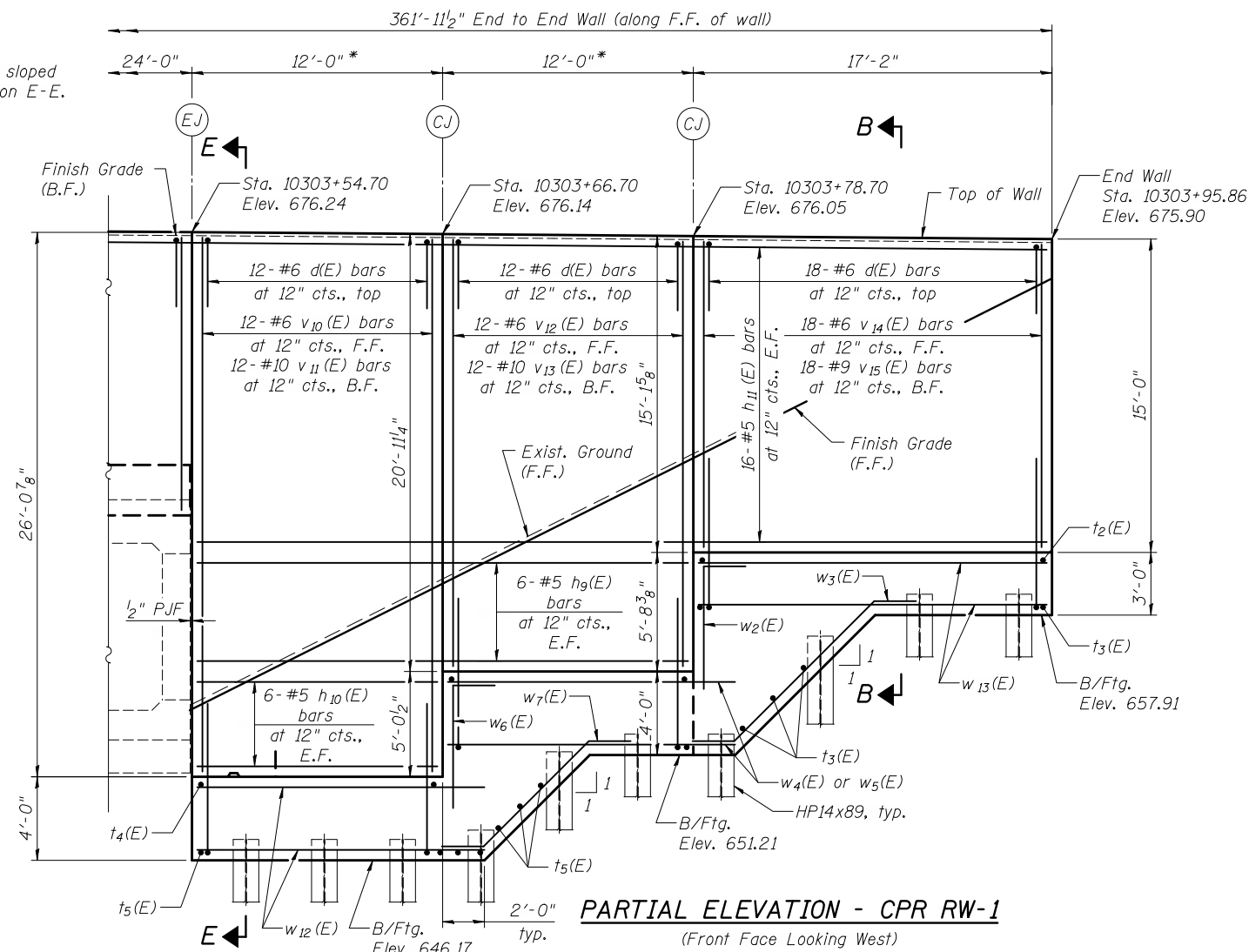
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CPR RW-1 PLAN AND ELEVATION
CPR RETAINING WALLS**

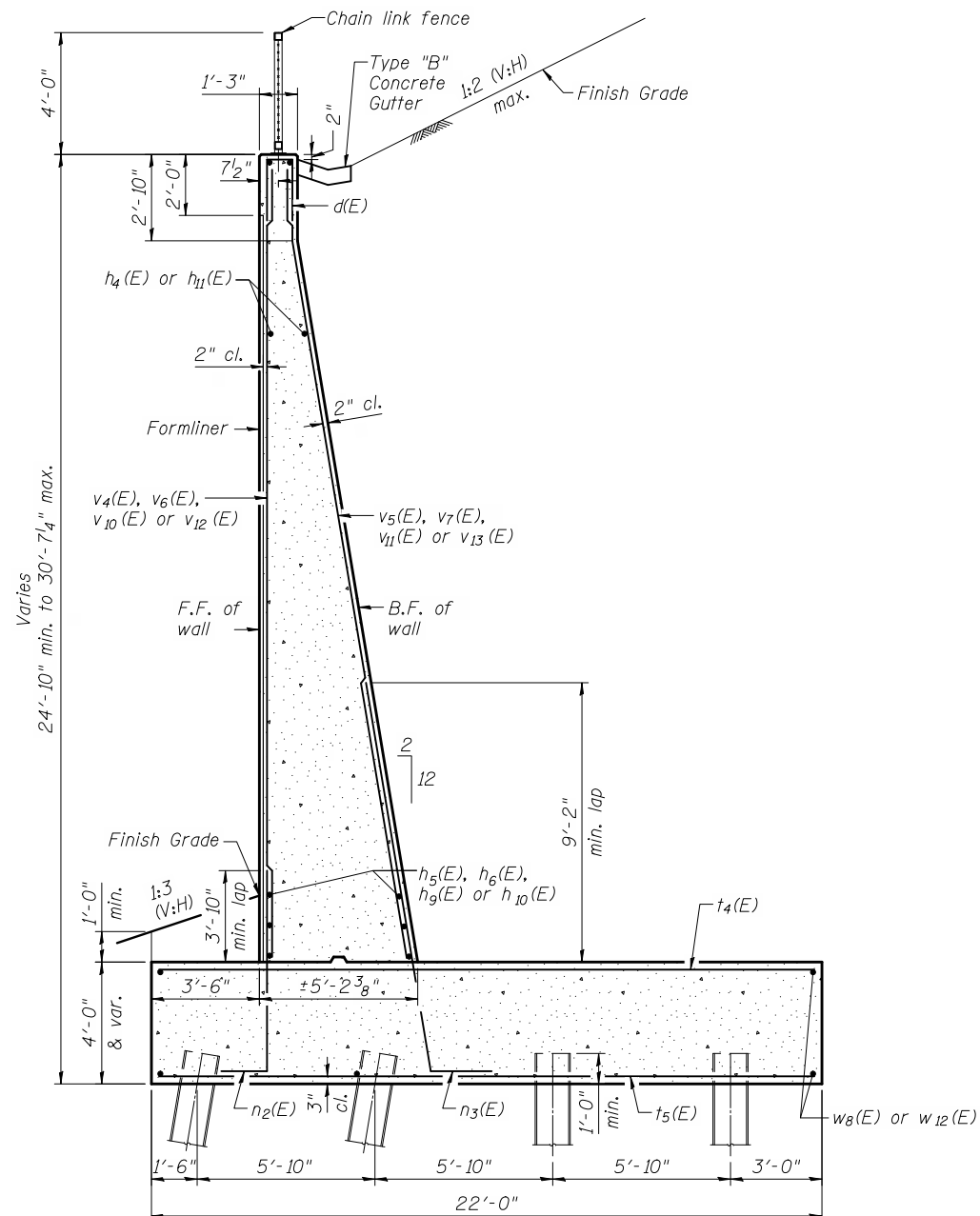
SHEET NO. 10 OF 42 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				

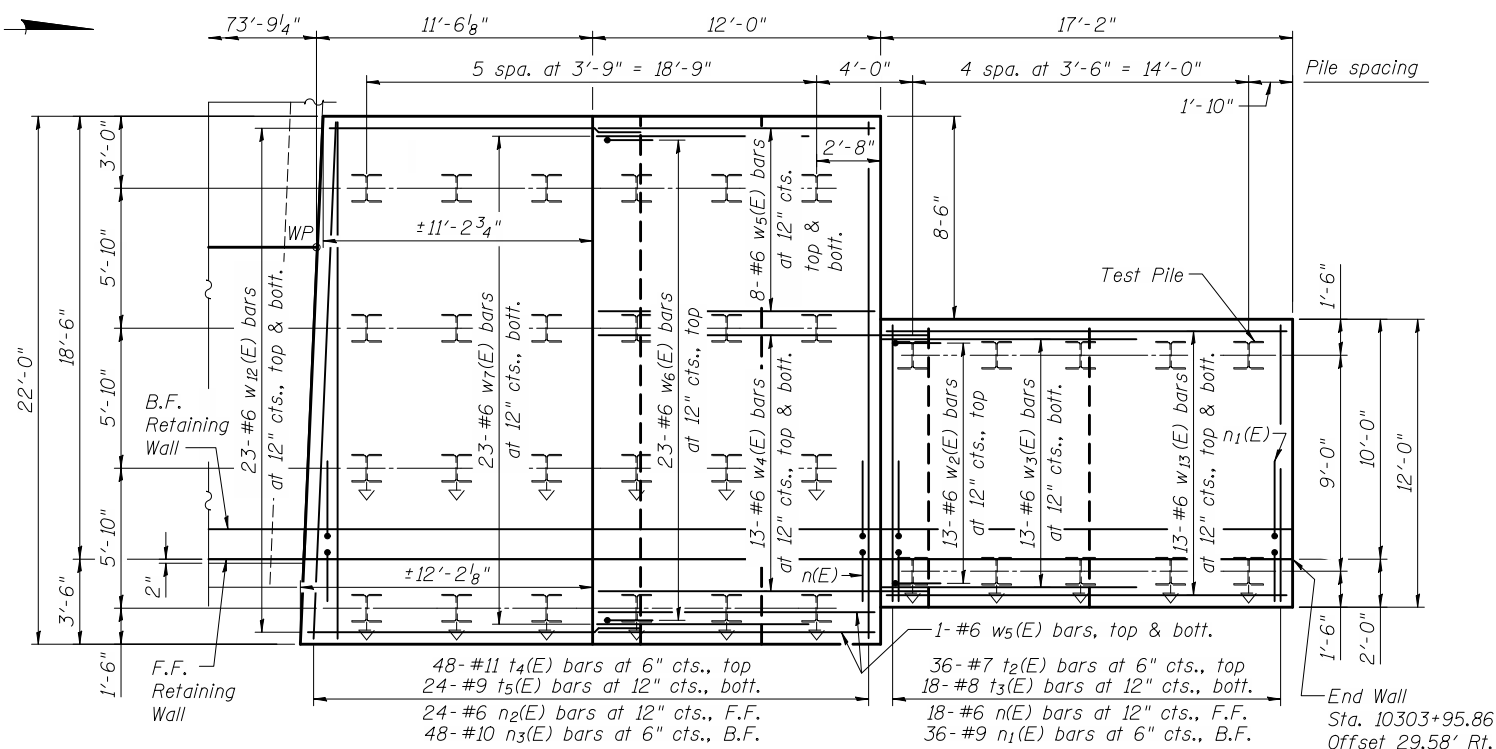
* Note: B.F. of wall sloped 2H:12V. See Section E-E.



PARTIAL ELEVATION - CPR RW-1
(Front Face Looking West)



SECTION E-E
(Looking downstation)



PARTIAL FOOTING PLAN - CPR RW-1

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CPR RW-1 PLAN AND ELEVATION
CPR RETAINING WALLS**

SHEET NO. 11 OF 42 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345A	32VB	DU PAGE	388	213
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				

**CPR RW-1
BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
d(E)	363	#6	7'-7"	□
h ₁ (E)	6	#5	31'-4"	—
h ₂ (E)	138	#5	33'-6"	—
h ₃ (E)	102	#5	29'-8"	—
h ₄ (E)	34	#5	36'-6"	—
h ₅ (E)	12	#5	24'-6"	—
h ₆ (E)	12	#5	12'-6"	—
h ₇ (E)	24	#5	26'-10"	—
h ₈ (E)	24	#5	28'-9"	—
h ₉ (E)	36	#5	23'-8"	—
h ₁₀ (E)	12	#5	11'-8"	—
h ₁₁ (E)	32	#5	40'-10"	—
n(E)	315	#6	7'-7"	J
n ₁ (E)	480	#9	11'-7"	L
n ₂ (E)	49	#6	8'-7"	J
n ₃ (E)	98	#10	14'-9"	L
n ₄ (E)	149	#8	10'-2"	L
t(E)	60	#7	13'-8"	—
t ₁ (E)	30	#9	13'-8"	—
t ₂ (E)	420	#7	11'-8"	—
t ₃ (E)	210	#8	11'-8"	—
t ₄ (E)	98	#11	21'-8"	—
t ₅ (E)	49	#9	21'-8"	—
t ₆ (E)	224	#8	12'-8"	—
v(E)	15	#6	38'-2"	—
v ₁ (E)	15	#9	38'-2"	—
v ₂ (E)	192	#6	15'-9"	—
v ₃ (E)	192	#9	15'-9"	—
v ₄ (E)	12	#6	21'-4"	—
v ₅ (E)	12	#9	21'-4"	—
v ₆ (E)	13	#6	26'-4"	—
v ₇ (E)	13	#10	26'-4"	—
v ₈ (E)	74	#6	10'-11"	—
v ₉ (E)	74	#8	10'-11"	—
v ₁₀ (E)	12	#6	25'-9"	—
v ₁₁ (E)	12	#10	25'-9"	—
v ₁₂ (E)	12	#6	20'-8"	—
v ₁₃ (E)	12	#10	20'-8"	—
v ₁₄ (E)	18	#6	14'-9"	—
v ₁₅ (E)	18	#9	14'-9"	—

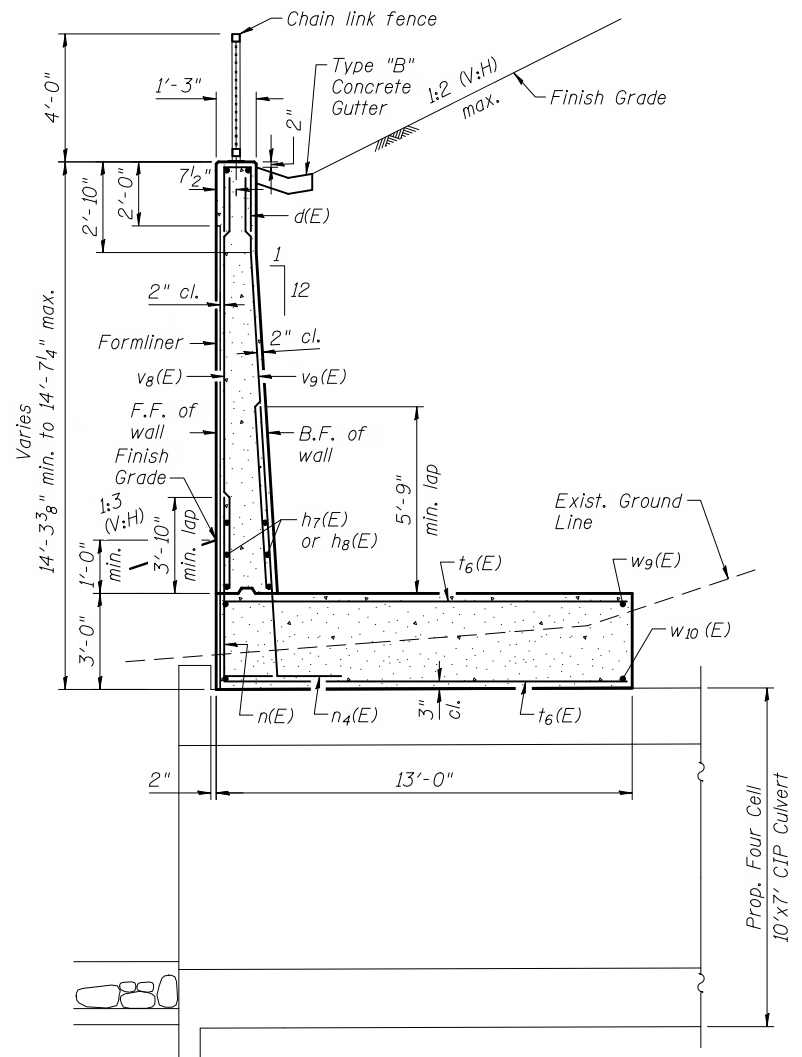
Bar	No.	Size	Length	Shape
w(E)	30	#6	29'-8"	—
w ₁ (E)	156	#6	36'-2"	—
w ₂ (E)	26	#6	10'-2"	└
w ₃ (E)	26	#6	15'-6"	└
w ₄ (E)	52	#6	13'-8"	—
w ₅ (E)	40	#6	11'-8"	—
w ₆ (E)	46	#6	9'-6"	└
w ₇ (E)	46	#6	13'-2"	└
w ₈ (E)	23	#6	29'-7"	—
w ₉ (E)	28	#6	38'-10"	—
w ₁₀ (E)	28	#6	29'-0"	—
w ₁₁ (E)	14	#6	23'-11"	—
w ₁₂ (E)	23	#6	26'-5"	—
w ₁₃ (E)	26	#6	16'-10"	—
Porous Granular Embankment				Cu. Yd. 923
Removal and Disposal of Unsuitable Material for Structures				Cu. Yd. 1,840
Form Liner Textured Surface				Sq. Ft. 5,385
Reinforcement Bars, Epoxy Coated				Pound 142,210
Furnishing Steel Piles HP14x89				Foot 7,350
Driving Piles				Foot 7,350
Test Pile Steel HP14x89				Each 1
Geocomposite Wall Drain				Sq. Yd. 598
Pipe Underdrains for Structures 4"				Foot 414
Chain Link Fence, 4'				Foot 363
Concrete Structures CPR Special				Cu. Yd. 1,176.1

PILE DATA

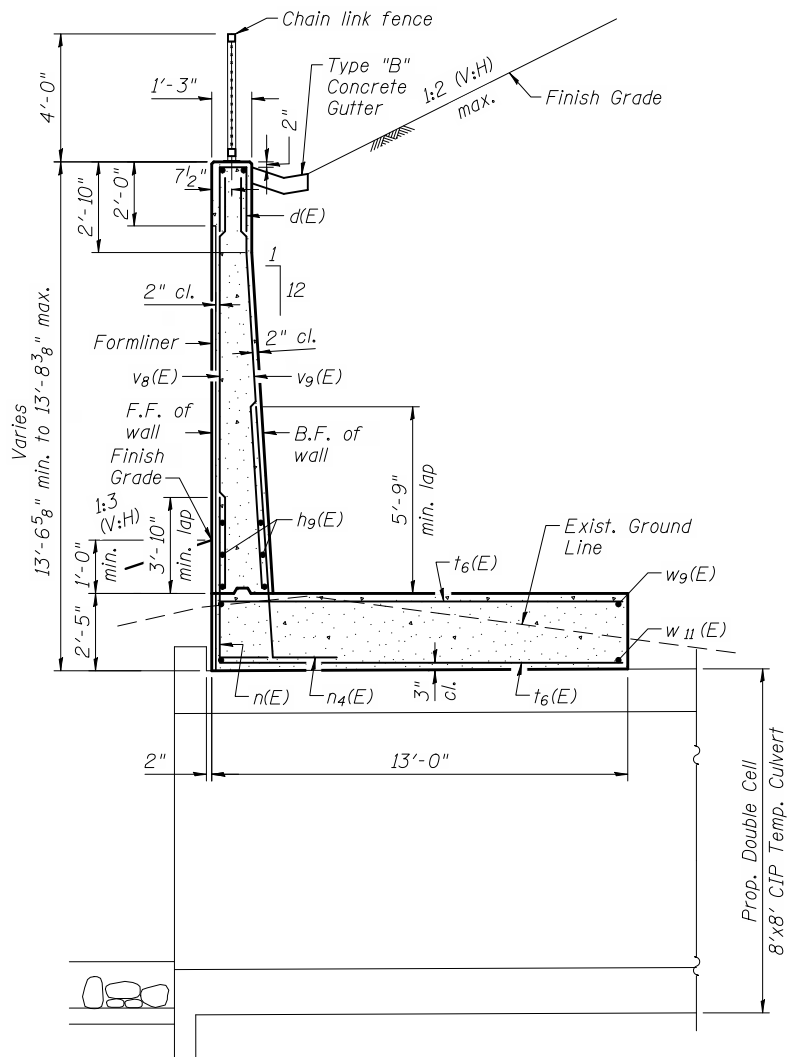
Type: HP14x89
 Min. Tip Elevation: 639.0
 Required Resistance: 356 kips
 Allowable Resistance Available: 178 kips
 Est. Length: 42 feet
 No. Piles: 175 + 1 Test Pile

NOTES:

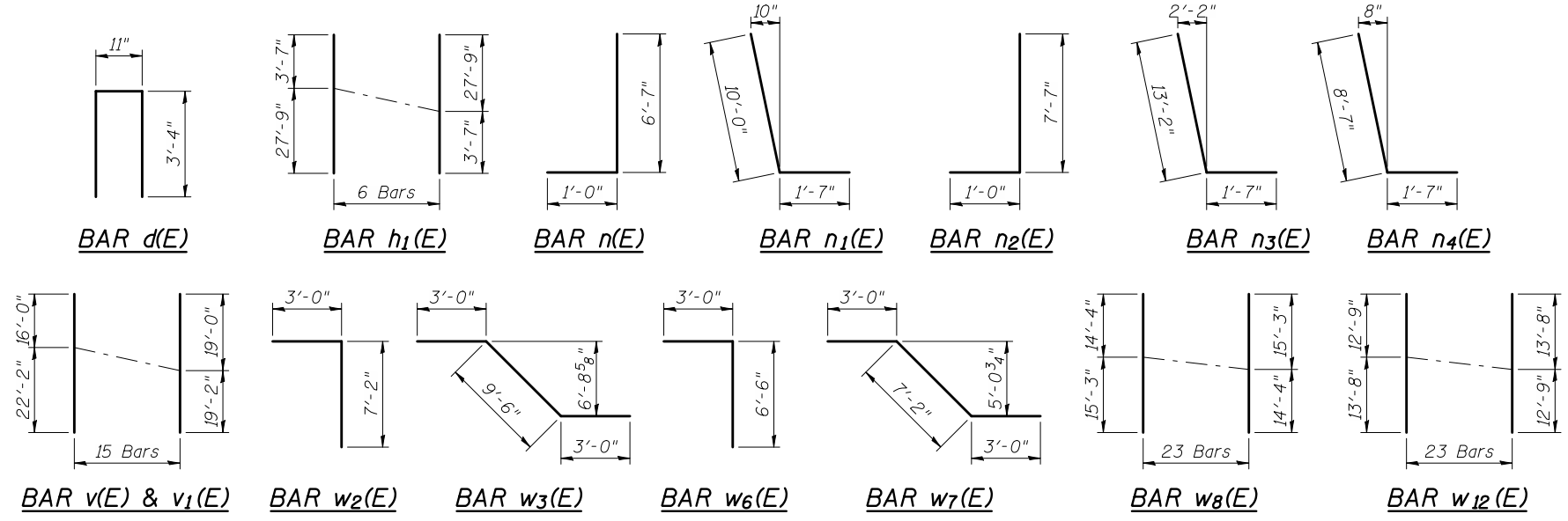
- All piles must be driven to the minimum tip elevation shown. At the minimum tip elevation, driving shall be continued until the required resistance is achieved. "Required resistance" is equivalent to "nominal required bearing" in the IDOT Standard Specifications. The nominal driven bearing, as determined by the pile driving formula specified in Art. 512.14 of the IDOT Standard Specifications and modified by the Special Provisions, must be greater than or equal to the required resistance shown.
- Bars indicated thus 13 x 5-#6 etc. indicates 13 lines of bars with 5 lengths per line.
- Pile driving near the proposed culvert shall be performed prior to culvert construction.



SECTION C-C
(Looking downstation)



SECTION D-D
(Looking downstation)



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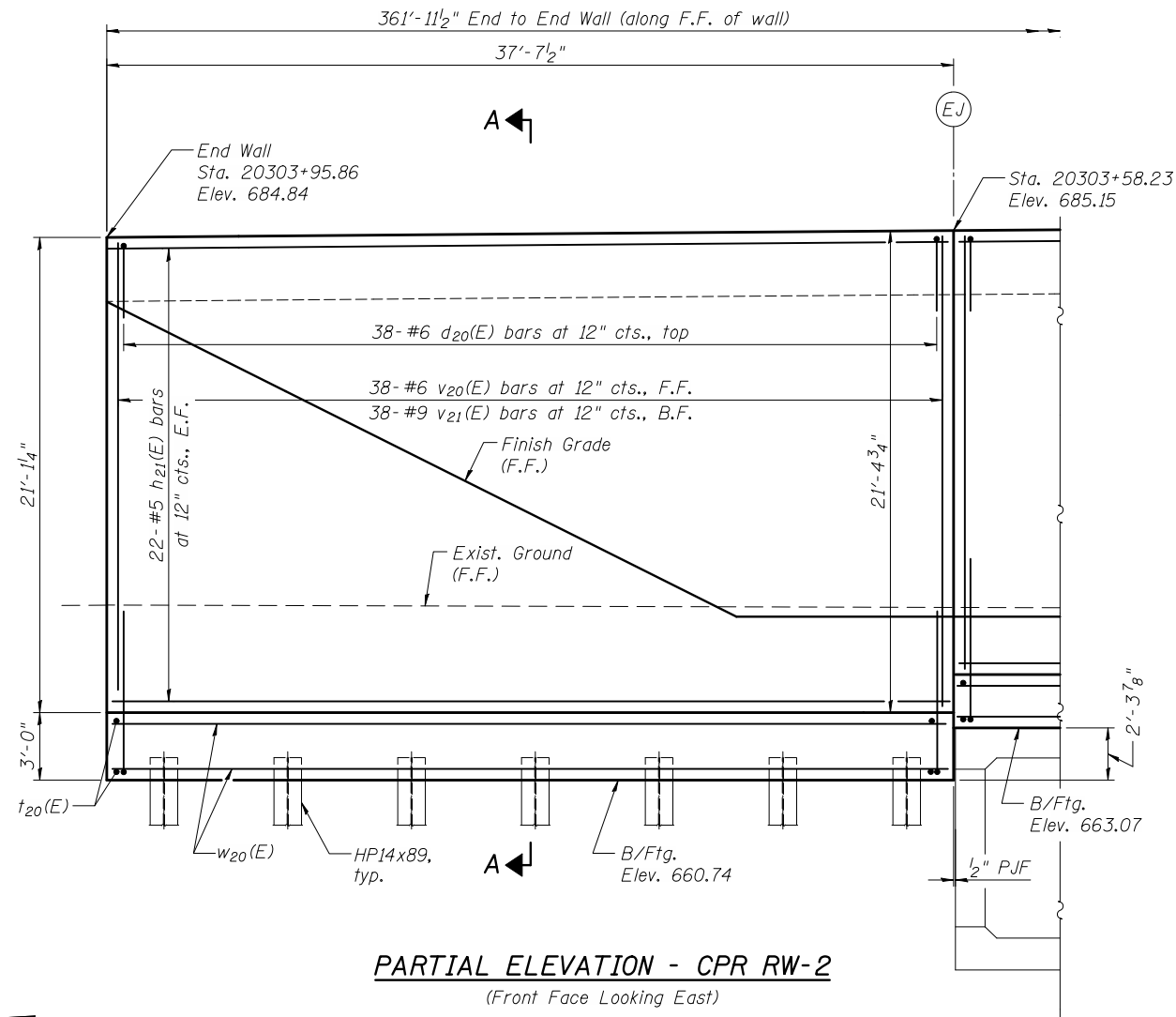
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CPR RW-1 SECTIONS & BILL OF MATERIAL
CPR RETAINING WALLS**

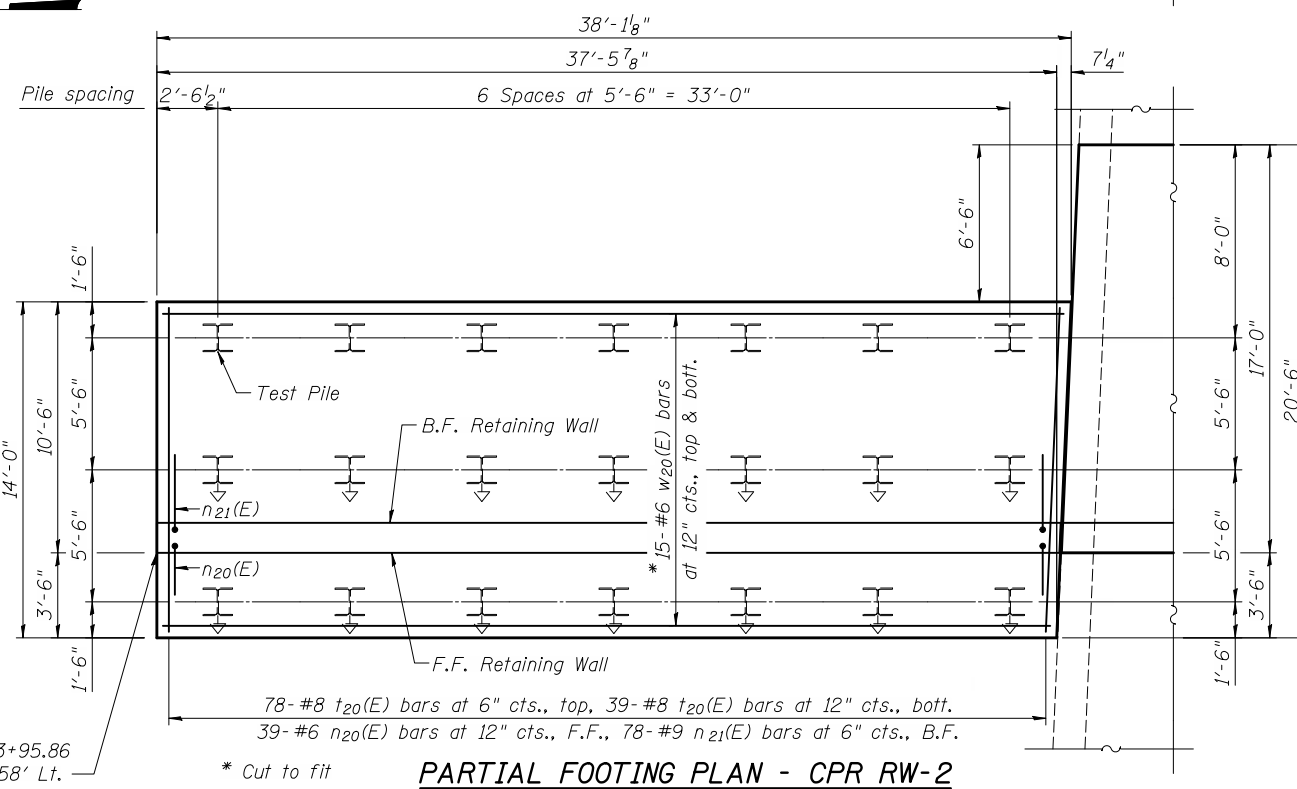
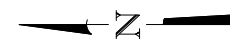
SHEET NO. 12 OF 42 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 60W01				

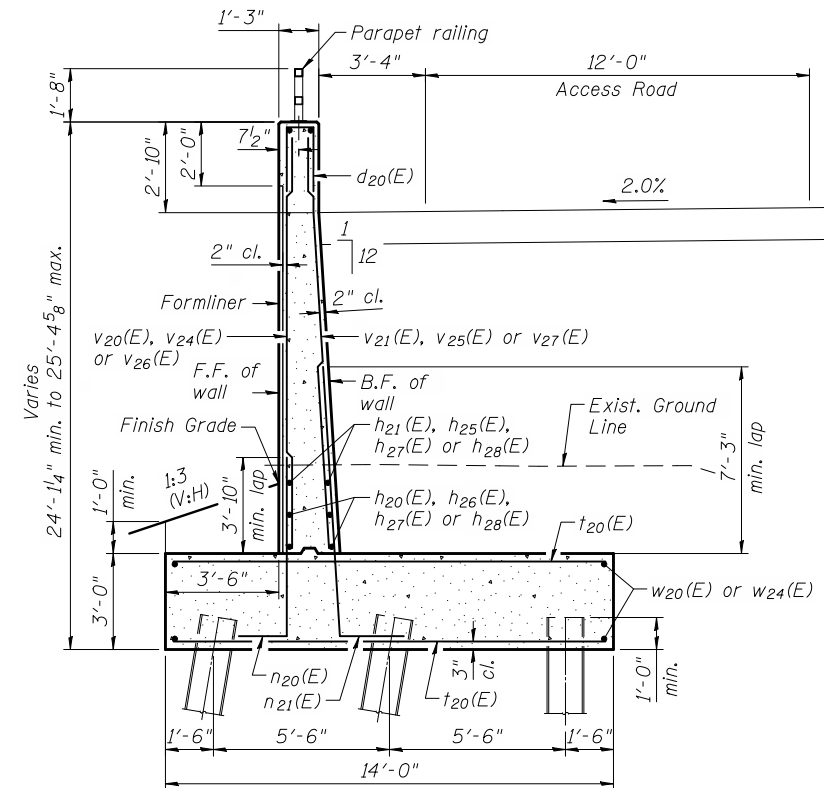
ILLINOIS FED. AID PROJECT



PARTIAL ELEVATION - CPR RW-2
(Front Face Looking East)



PARTIAL FOOTING PLAN - CPR RW-2
* Cut to fit



SECTION A-A
(Looking upstation)

NOTES:

1. See Sheet 4 for Joint Details.

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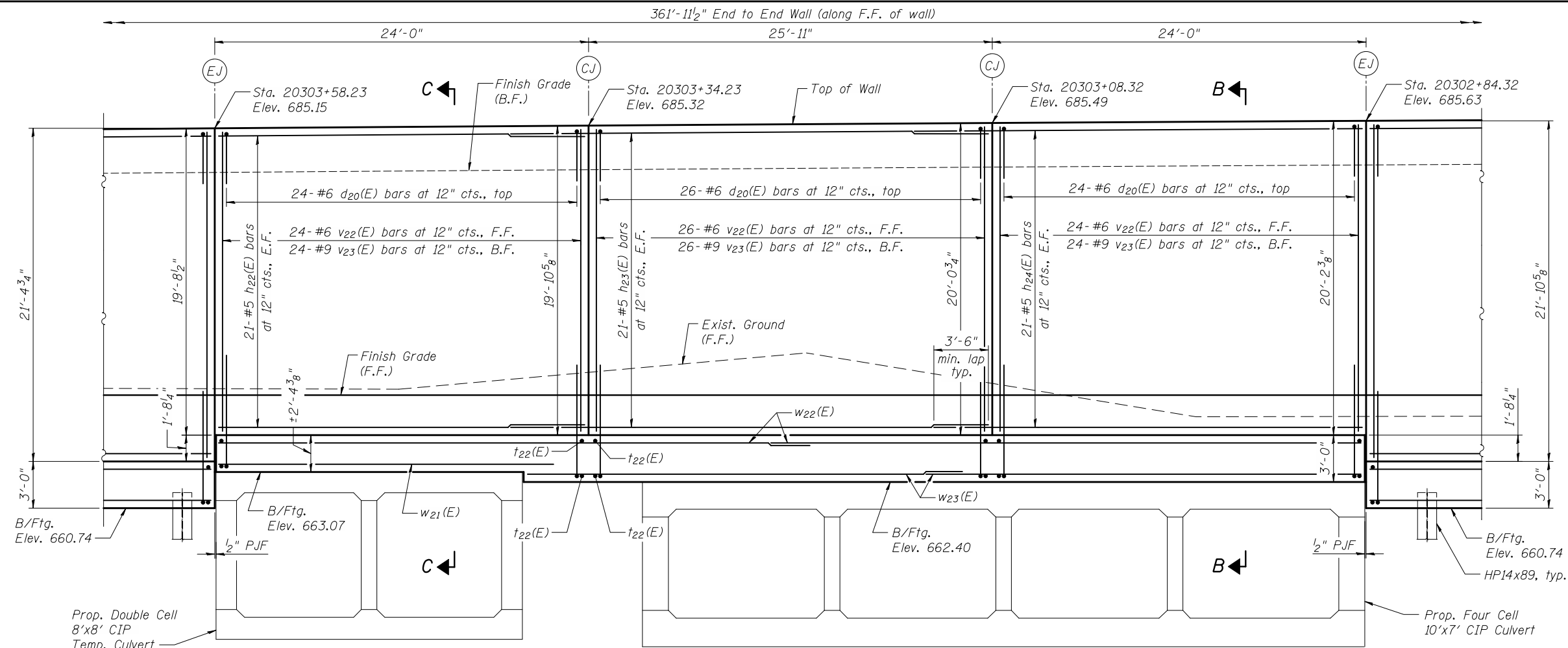
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CPR RW-2 PLAN AND ELEVATION
CPR RETAINING WALLS**

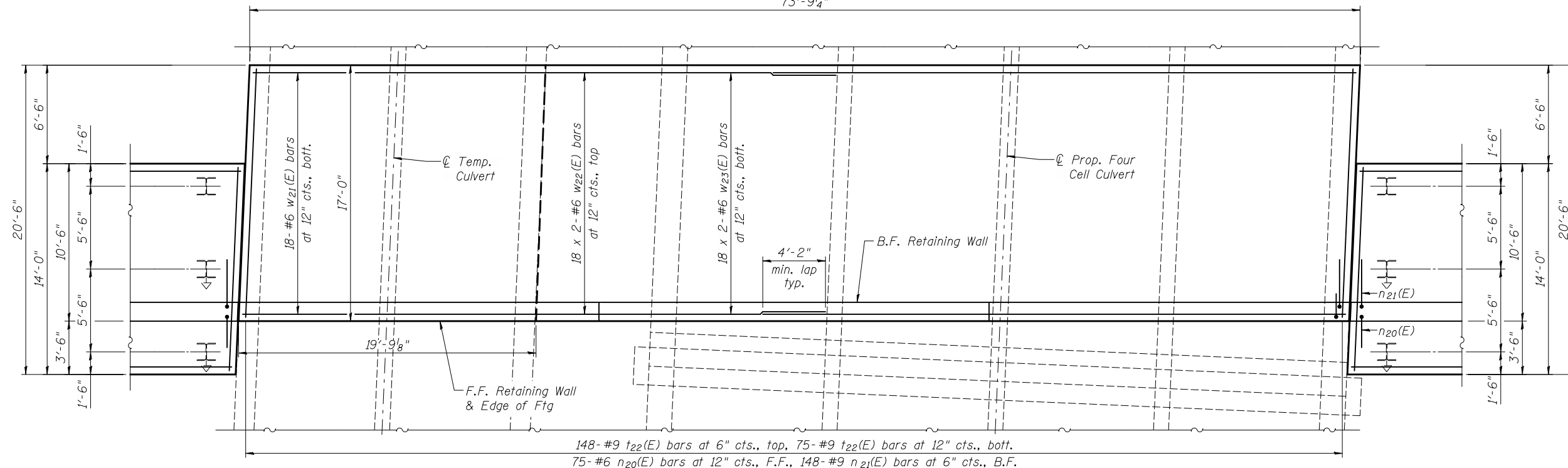
SHEET NO. 13 OF 42 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345A	32VB	DU PAGE	388	215
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				



PARTIAL ELEVATION - CPR RW-2

(Front Face Looking East)
73'-9 1/4"



PARTIAL FOOTING PLAN - CPR RW-2

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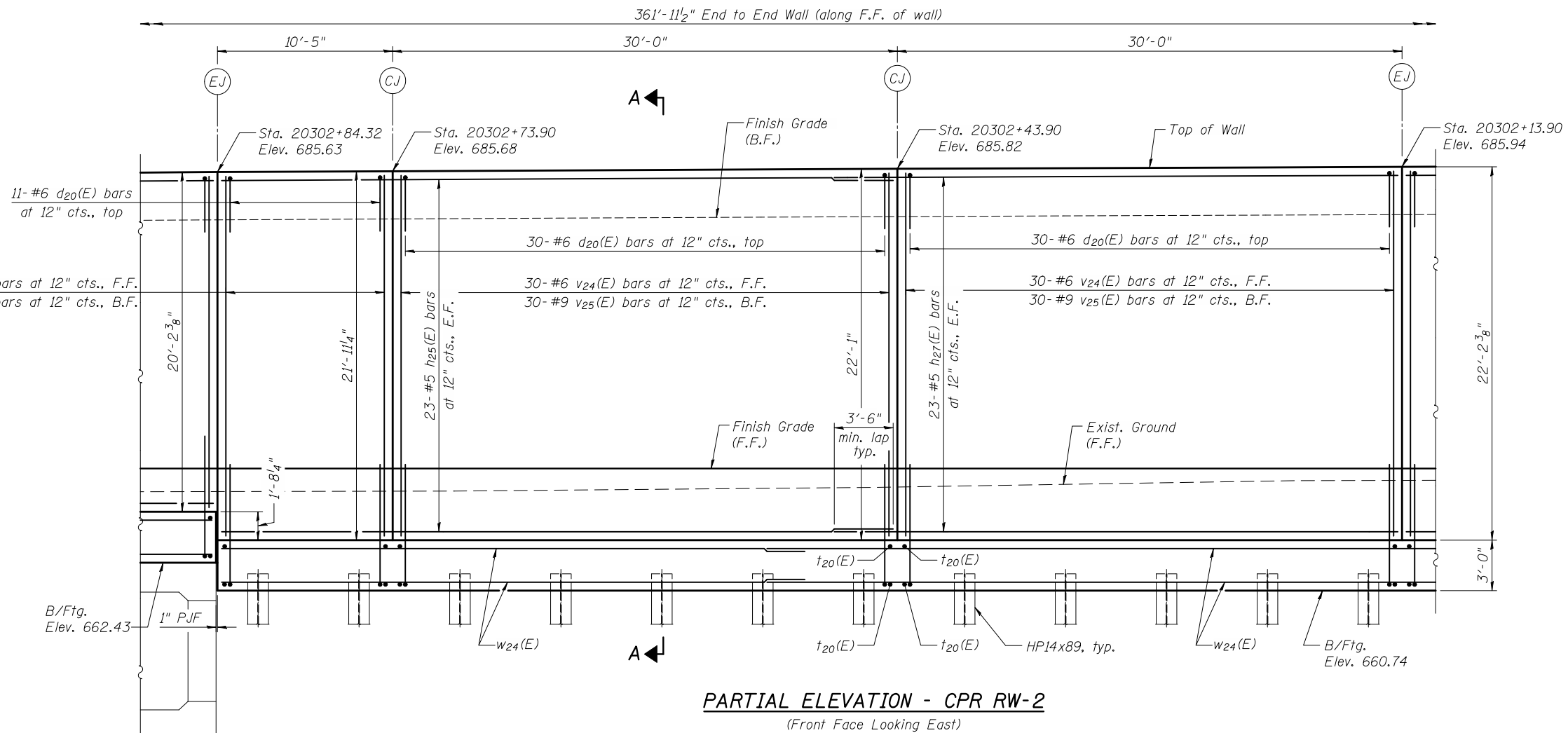
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CPR RW-2 PLAN AND ELEVATION
CPR RETAINING WALLS**

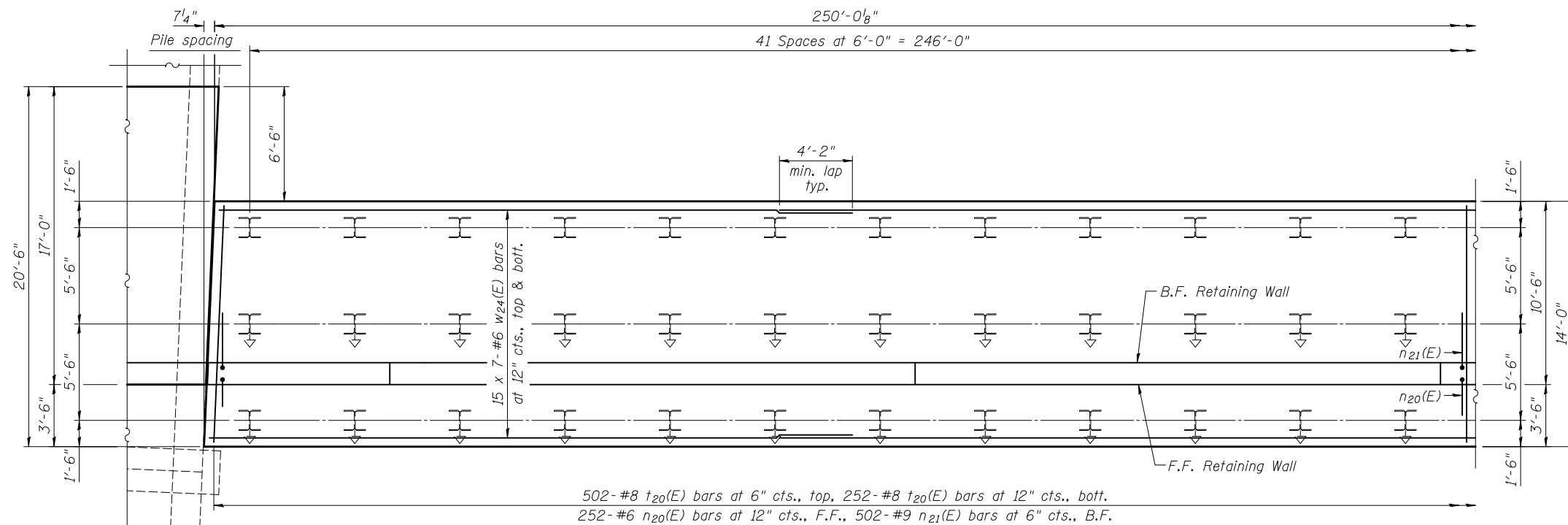
SHEET NO. 14 OF 42 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345A	32VB	DU PAGE	388	216
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				

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PARTIAL ELEVATION - CPR RW-2
 (Front Face Looking East)



PARTIAL FOOTING PLAN - CPR RW-2



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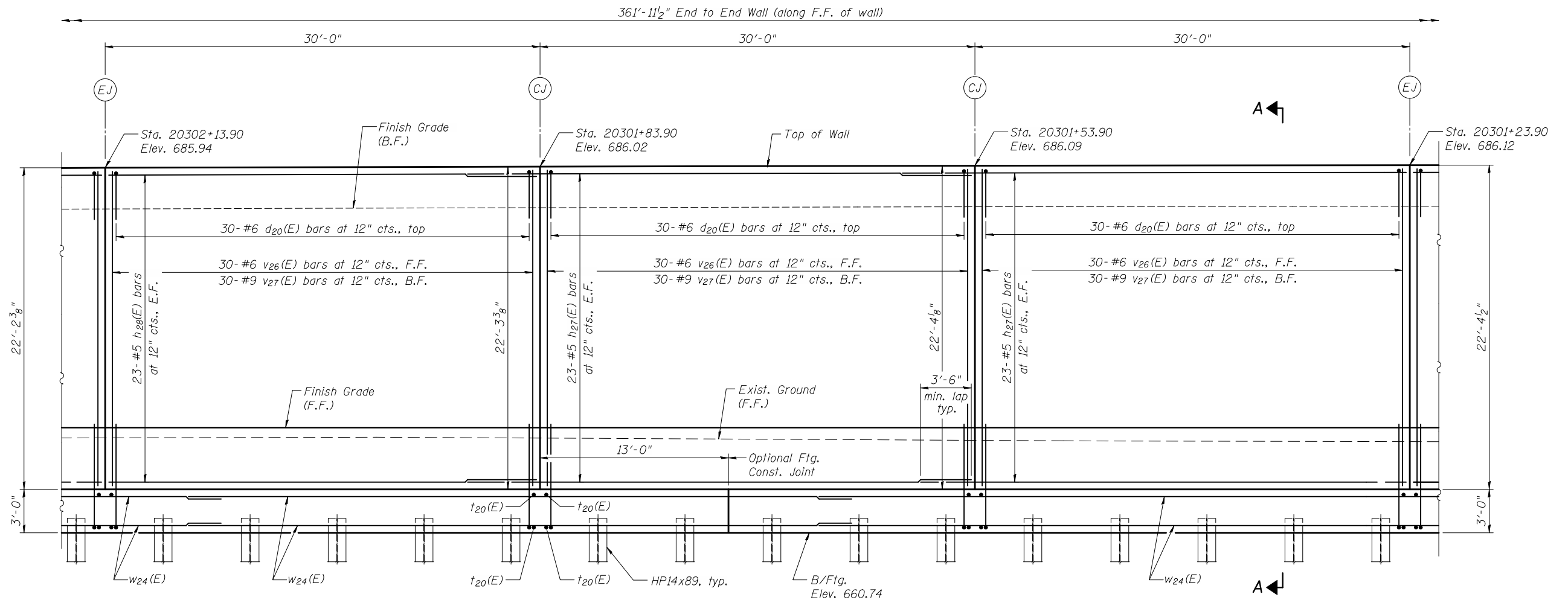
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

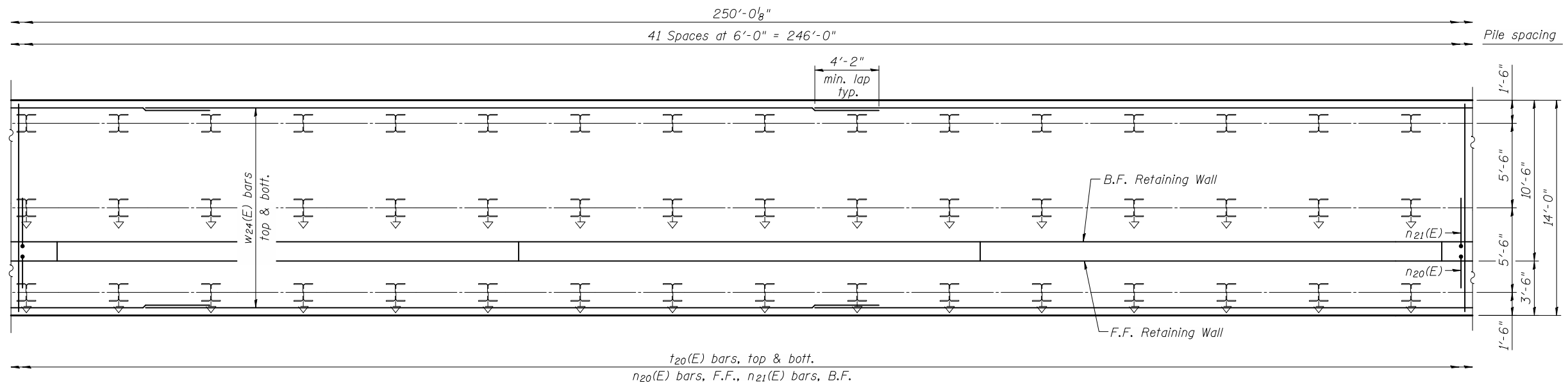
**CPR RW-2 PLAN AND ELEVATION
 CPR RETAINING WALLS**

SHEET NO. 15 OF 42 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345A	32VB		388	217
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				



PARTIAL ELEVATION - CPR RW-2
(Front Face Looking East)



PARTIAL FOOTING PLAN - CPR RW-2

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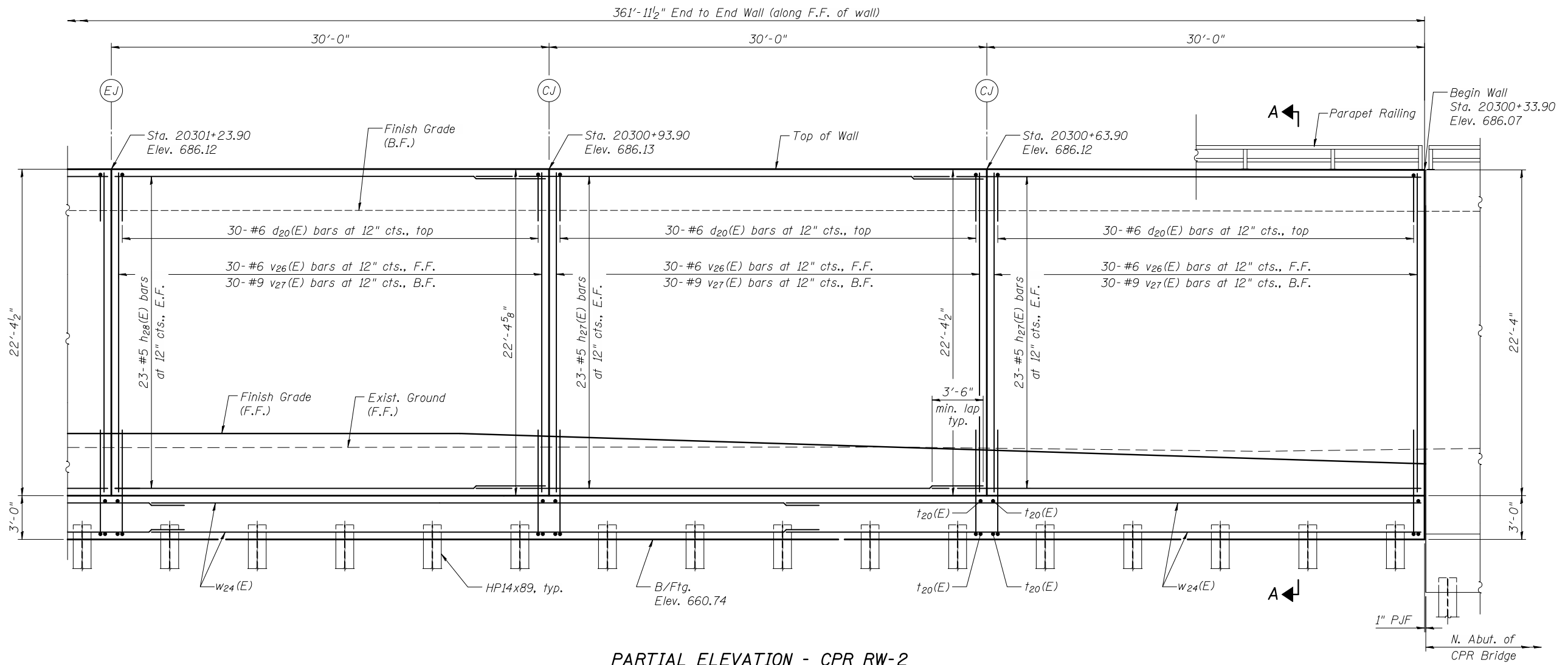
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DEPARTMENT OF TRANSPORTATION**

**CPR RW-2 PLAN AND ELEVATION
CPR RETAINING WALLS**

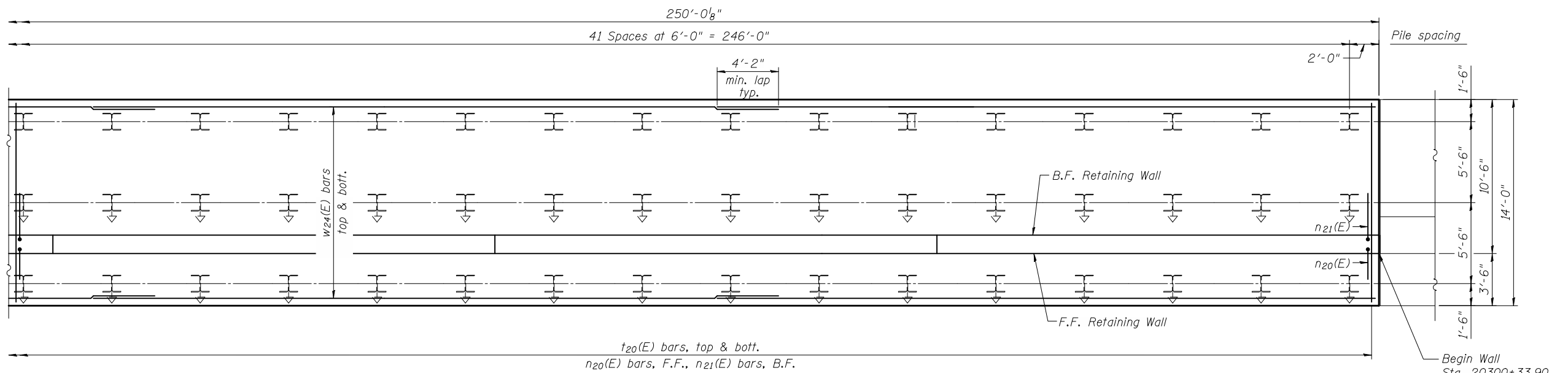
SHEET NO. 16 OF 42 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345A	32VB	DU PAGE	388	218
CONTRACT NO. 60W01				

ILLINOIS FED. AID PROJECT



PARTIAL ELEVATION - CPR RW-2
(Front Face Looking East)



PARTIAL FOOTING PLAN - CPR RW-2

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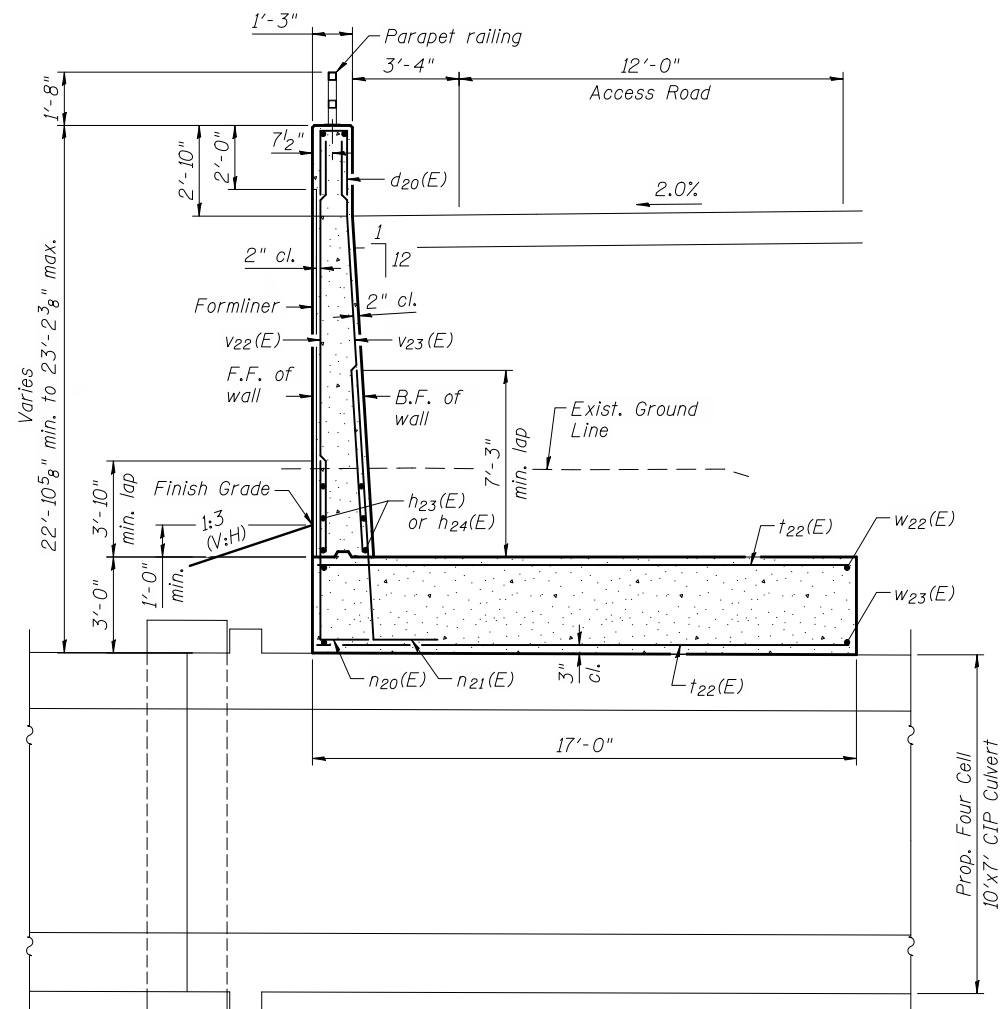
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CPR RW-2 PLAN AND ELEVATION
CPR RETAINING WALLS**

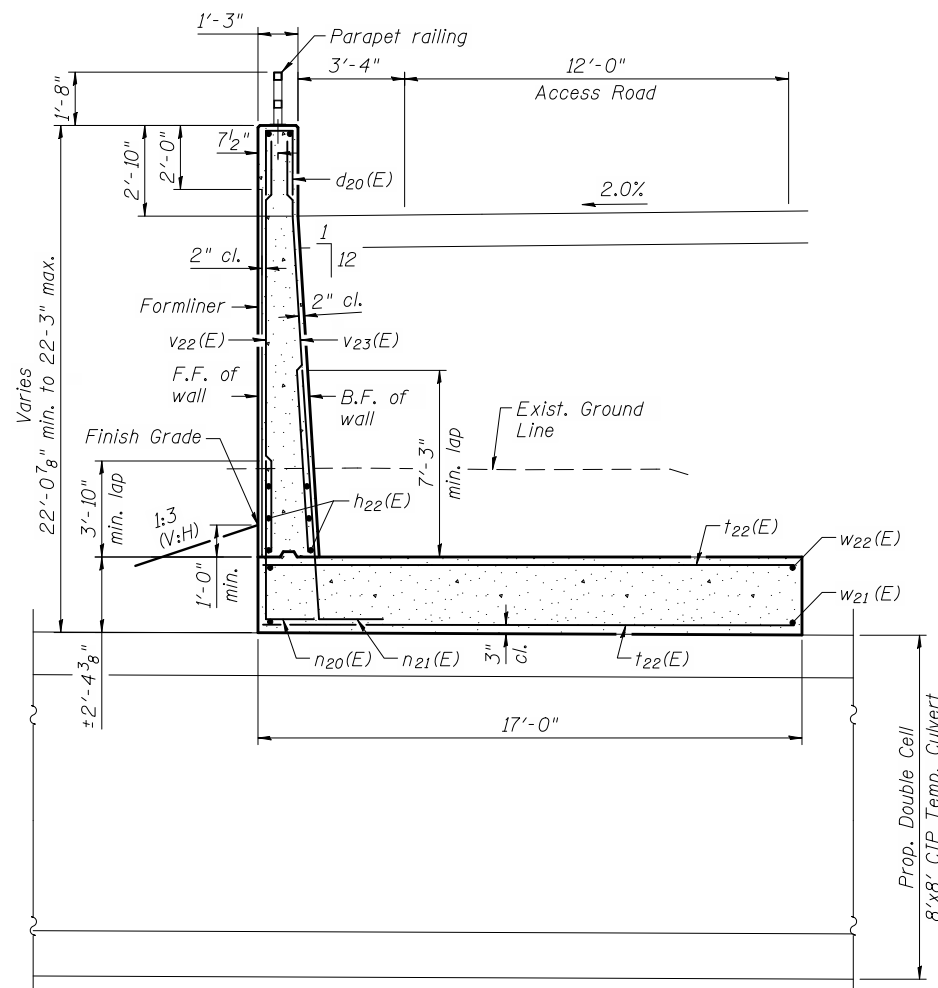
SHEET NO. 17 OF 42 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				

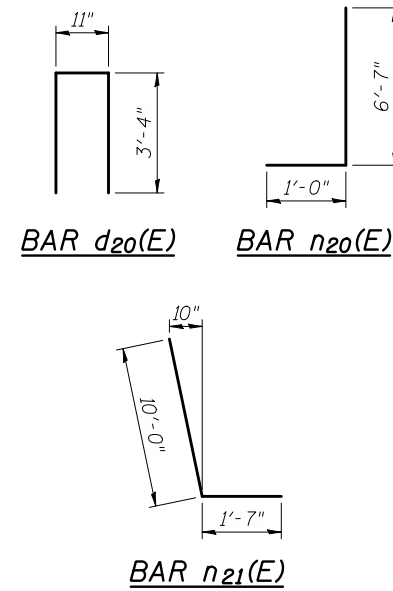
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SECTION B-B
 (Looking upstation)



SECTION C-C
 (Looking upstation)



**CPR RW-2
 BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
d20(E)	363	#6	7'-7"	Π
h21(E)	44	#5	37'-3"	—
h22(E)	42	#5	23'-8"	—
h23(E)	42	#5	29'-5"	—
h24(E)	42	#5	27'-6"	—
h25(E)	46	#5	40'-1"	—
h27(E)	230	#5	33'-6"	—
h28(E)	92	#5	29'-8"	—
n20(E)	366	#6	7'-7"	J
n21(E)	728	#9	11'-7"	L
t20(E)	871	#8	13'-8"	—
t22(E)	223	#9	16'-8"	—
v20(E)	38	#6	20'-11"	—
v21(E)	38	#9	20'-11"	—
v22(E)	74	#6	19'-6"	—
v23(E)	74	#9	19'-6"	—
v24(E)	71	#6	21'-9"	—
v25(E)	71	#9	21'-9"	—
v26(E)	180	#6	22'-0"	—
v27(E)	180	#9	22'-0"	—
w20(E)	30	#6	37'-8"	—
w21(E)	18	#6	23'-9"	—
w22(E)	36	#6	38'-10"	—
w23(E)	36	#6	31'-4"	—
w24(E)	210	#6	39'-4"	—
Porous Granular Embankment			Cu. Yd.	1,083
Removal and Disposal of Unsuitable Material for Structures			Cu. Yd.	1,538
Form Liner Textured Surface			Sq. Ft.	7,111
Reinforcement Bars, Epoxy Coated			Pound	155,940
Furnishing Steel Piles HP14x89			Foot	6,716
Driving Piles			Foot	6,716
Test Pile Steel HP14x89			Each	1
Geocomposite Wall Drain			Sq. Yd.	692
Pipe Underdrains for Structures 4"			Foot	366
Parapet Railing			Foot	362
Concrete Structures CPR Special			Cu. Yd.	1,142.0

PILE DATA

Type: HP14x89
 Min. Tip Elevation: 630.0
 Required Resistance: 310 kips
 Allowable Resistance Available: 155 kips
 Est. Length: 46 feet
 No. Piles: 146 + 1 Test Pile

NOTES:

- All piles must be driven to the minimum tip elevation shown. At the minimum tip elevation, driving shall be continued until the required resistance is achieved. "Required resistance" is equivalent to "nominal required bearing" in the IDOT Standard Specifications. The nominal driven bearing, as determined by the pile driving formula specified in Art. 512.14 of the IDOT Standard Specifications and modified by the Special Provisions, must be greater than or equal to the required resistance shown.
- Bars indicated thus 13 x 5-#6 etc. indicates 13 lines of bars with 5 lengths per line.
- Pile driving near the proposed culvert shall be performed prior to culvert construction.



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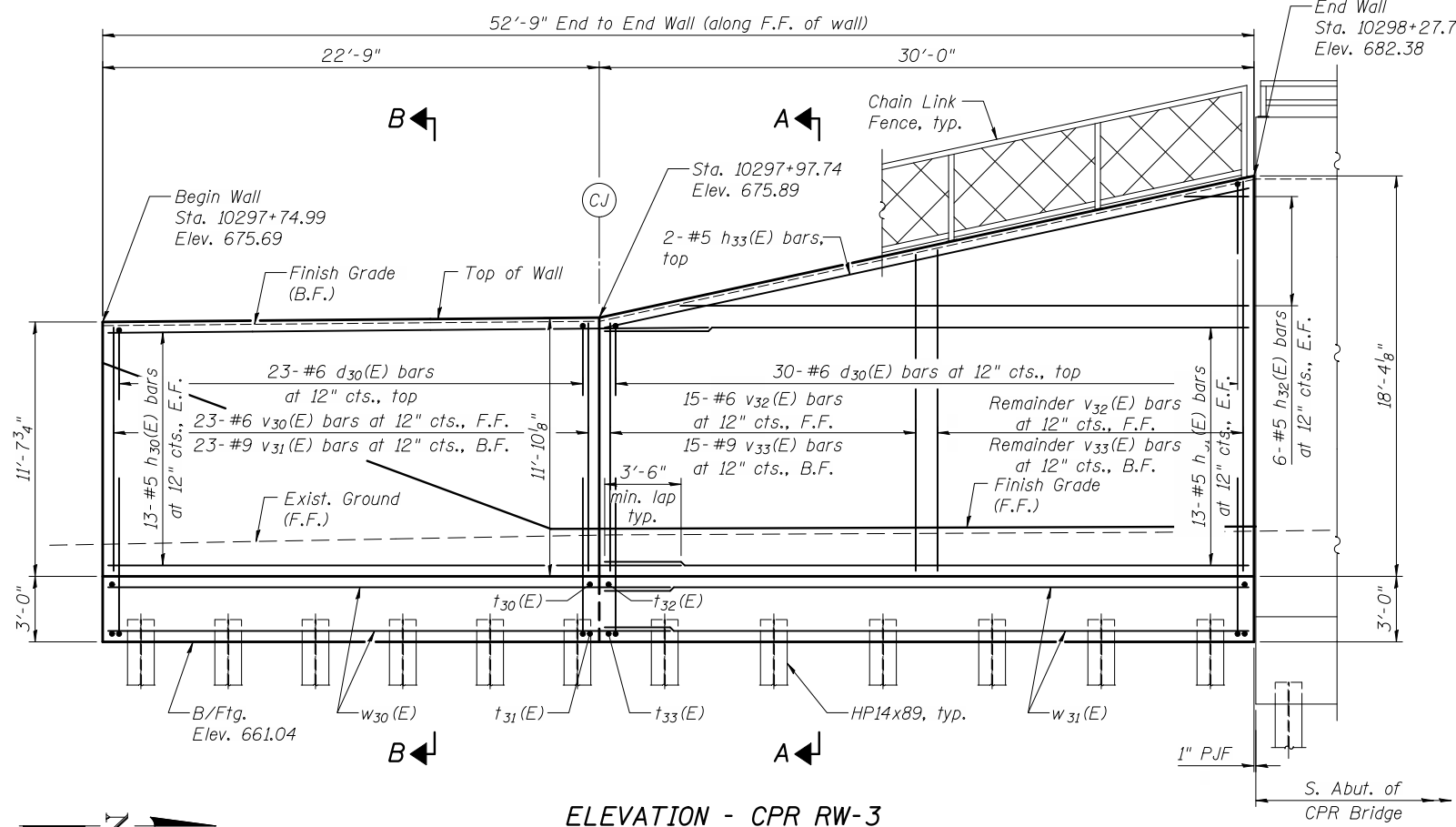
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CPR RW-2 SECTIONS & BILL OF MATERIAL
 CPR RETAINING WALLS**

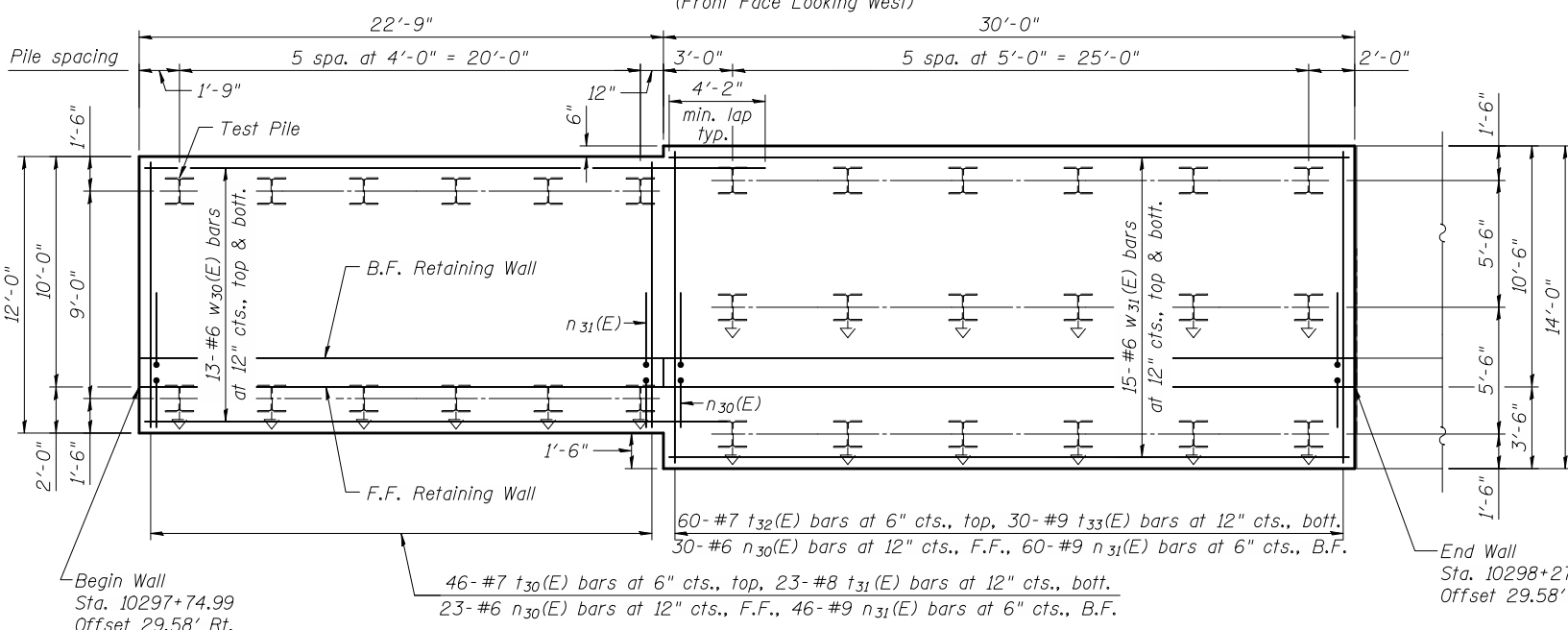
SHEET NO. 18 OF 42 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345A	32VB	DU PAGE	388	220
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				

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ELEVATION - CPR RW-3
(Front Face Looking West)



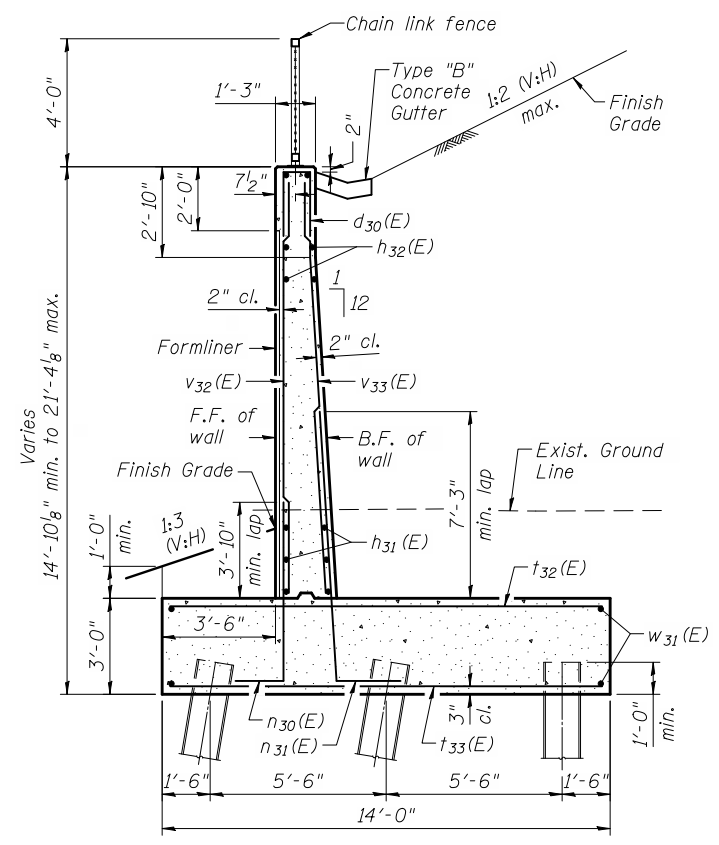
FOOTING PLAN - CPR RW-3

PILE DATA

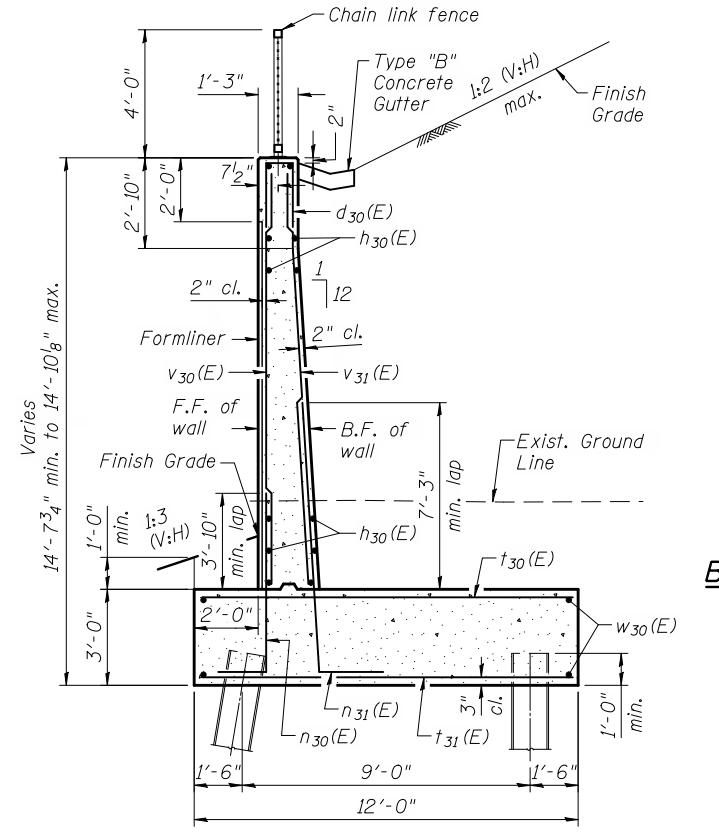
Type: HP14x89
Min. Tip Elevation: 650.0
Required Resistance: 272 kips
Allowable Resistance Available: 136 kips
Est. Length: 54 feet
No. Piles: 29 + 1 Test Pile

NOTES:

- All piles must be driven to the minimum tip elevation shown. At the minimum tip elevation, driving shall be continued until the required resistance is achieved. "Required resistance" is equivalent to "nominal required bearing" in the IDOT Standard Specifications. The nominal driven bearing, as determined by the pile driving formula specified in Art. 512.14 of the IDOT Standard Specifications and modified by the Special Provisions, must be greater than or equal to the required resistance shown.
- Bars indicated thus 13 x 5-#6 etc. indicates 13 lines of bars with 5 lengths per line.
- See Sheet 4 for Joint Details.



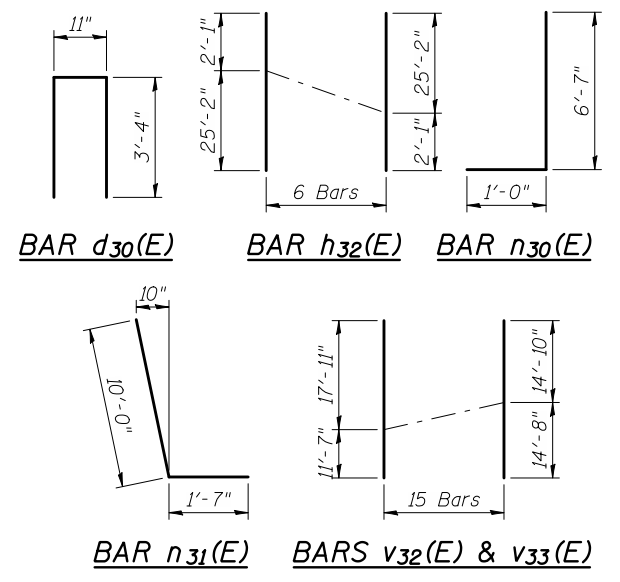
SECTION A-A
(Looking downstation)



SECTION B-B
(Looking downstation)

CPR RW-3
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
d ₃₀ (E)	53	#6	7'-7"	Π
h ₃₀ (E)	26	#5	26'-3"	—
h ₃₁ (E)	26	#5	29'-8"	—
h ₃₂ (E)	6	#5	27'-3"	—
h ₃₃ (E)	2	#5	30'-4"	—
n ₃₀ (E)	53	#6	7'-7"	J
n ₃₁ (E)	106	#9	11'-7"	L
t ₃₀ (E)	46	#7	11'-8"	—
t ₃₁ (E)	23	#8	11'-8"	—
t ₃₂ (E)	60	#7	13'-8"	—
t ₃₃ (E)	30	#9	13'-8"	—
v ₃₀ (E)	23	#6	11'-5"	—
v ₃₁ (E)	23	#9	11'-5"	—
v ₃₂ (E)	15	#6	29'-6"	—
v ₃₃ (E)	15	#9	29'-6"	—
w ₃₀ (E)	26	#6	26'-11"	—
w ₃₁ (E)	30	#6	29'-8"	—
Porous Granular Embankment			Cu. Yd.	106
Removal and Disposal of Unsuitable Material for Structures			Cu. Yd.	220
Form Liner Textured Surface			Sq. Ft.	615
Reinforcement Bars, Epoxy Coated			Pound	17,860
Furnishing Steel Piles HP14x89			Foot	1,566
Driving Piles			Foot	1,566
Test Pile Steel HP14x89			Each	1
Geocomposite Wall Drain			Sq. Yd.	71
Pipe Underdrains for Structures 4"			Foot	53
Chain Link Fence, 4'			Foot	54
Concrete Structures CPR Special			Cu. Yd.	120.6

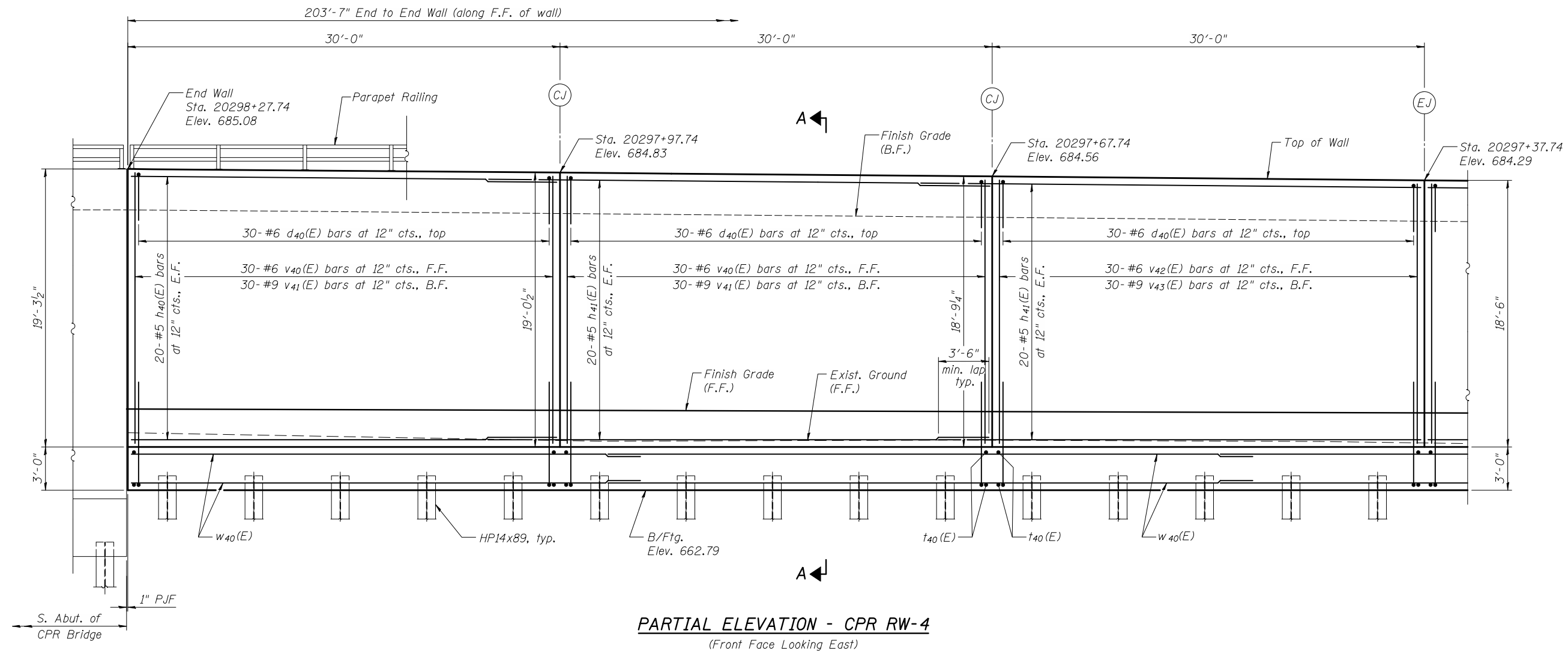


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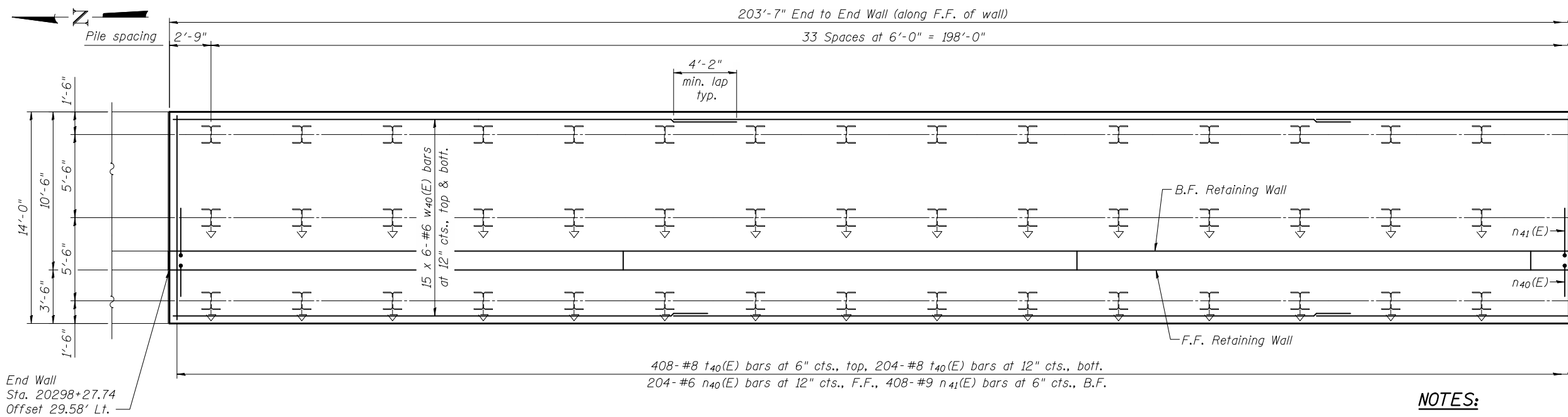
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CPR RW-3 PLAN AND ELEVATION
CPR RETAINING WALLS
SHEET NO. 19 OF 42 SHEETS

F.A.P. RTE. 345A	SECTION 32VB	COUNTY DU PAGE	TOTAL SHEETS 388	SHEET NO. 221
CONTRACT NO. 60W01				ILLINOIS FED. AID PROJECT



PARTIAL ELEVATION - CPR RW-4
(Front Face Looking East)



PARTIAL FOOTING PLAN - CPR RW-4

NOTES:

1. See Sheet 4 for Joint Details.

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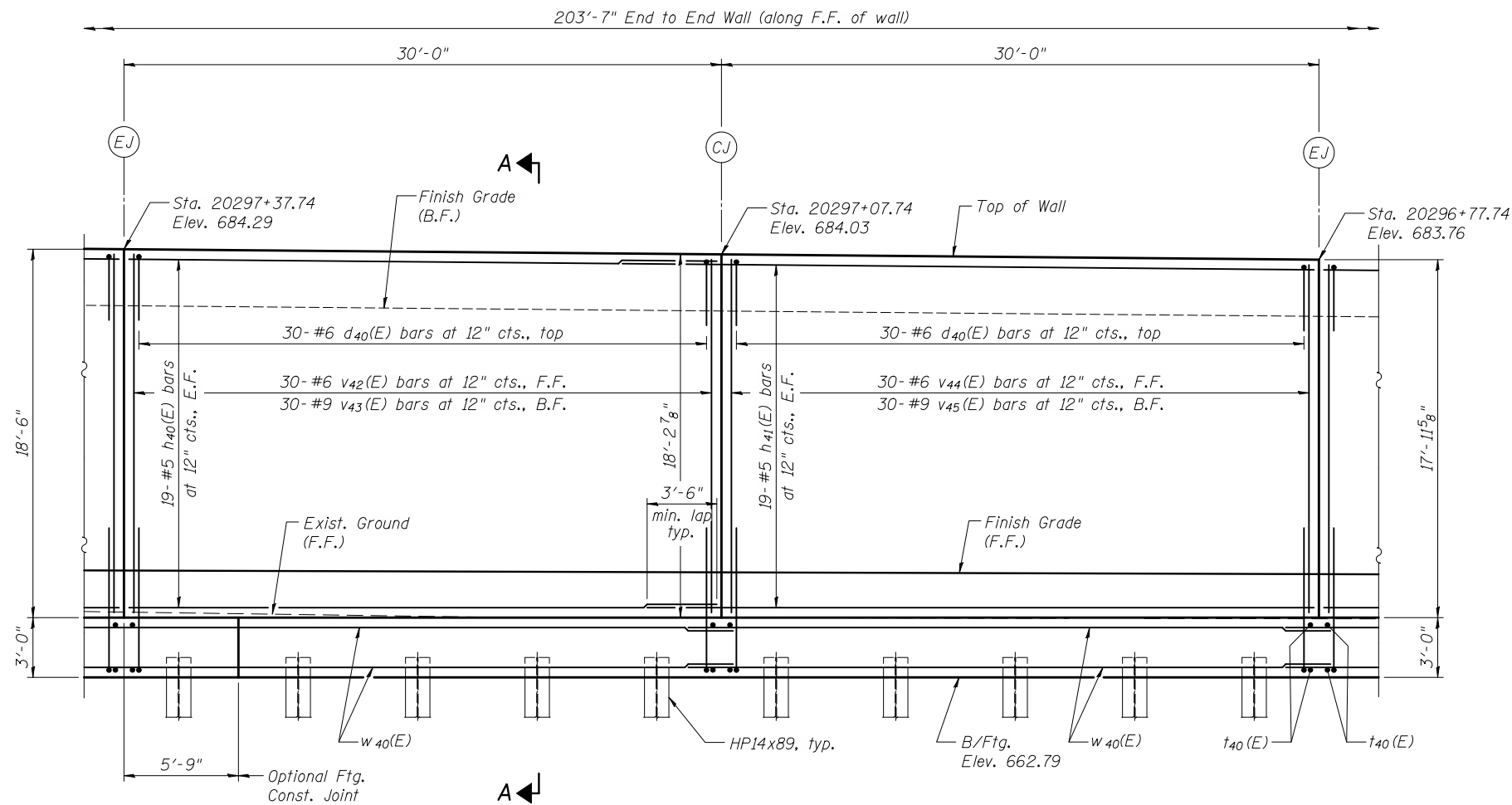
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

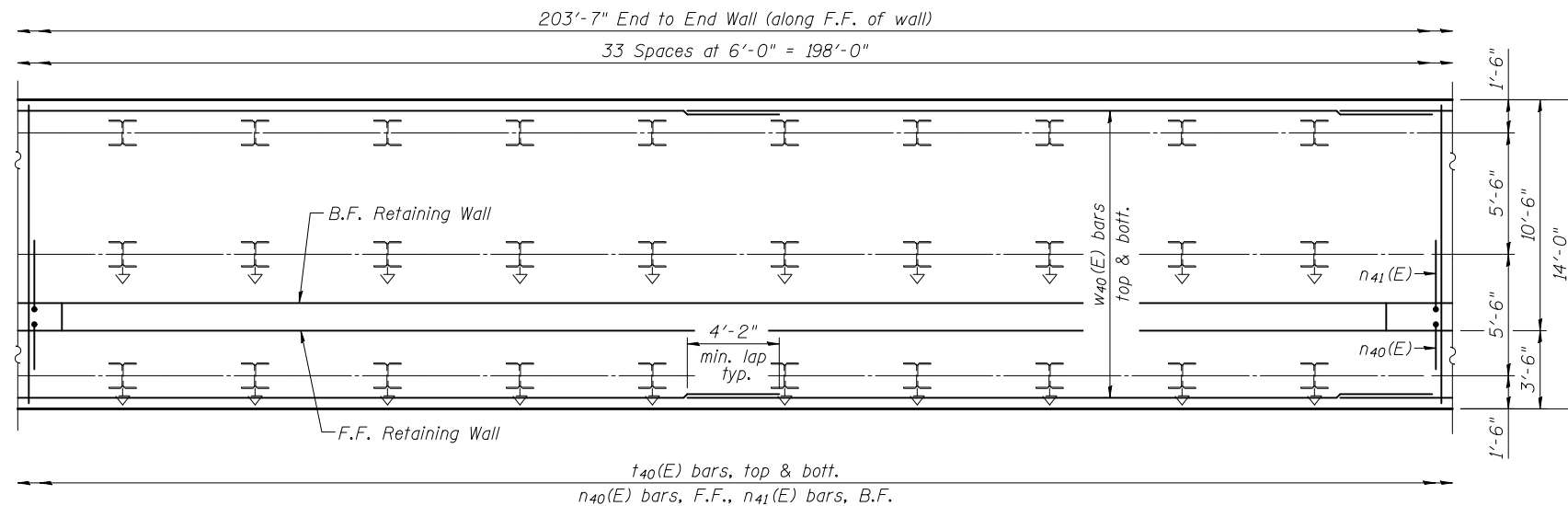
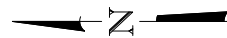
**CPR RW-4 PLAN AND ELEVATION
CPR RETAINING WALLS**

SHEET NO. 20 OF 42 SHEETS

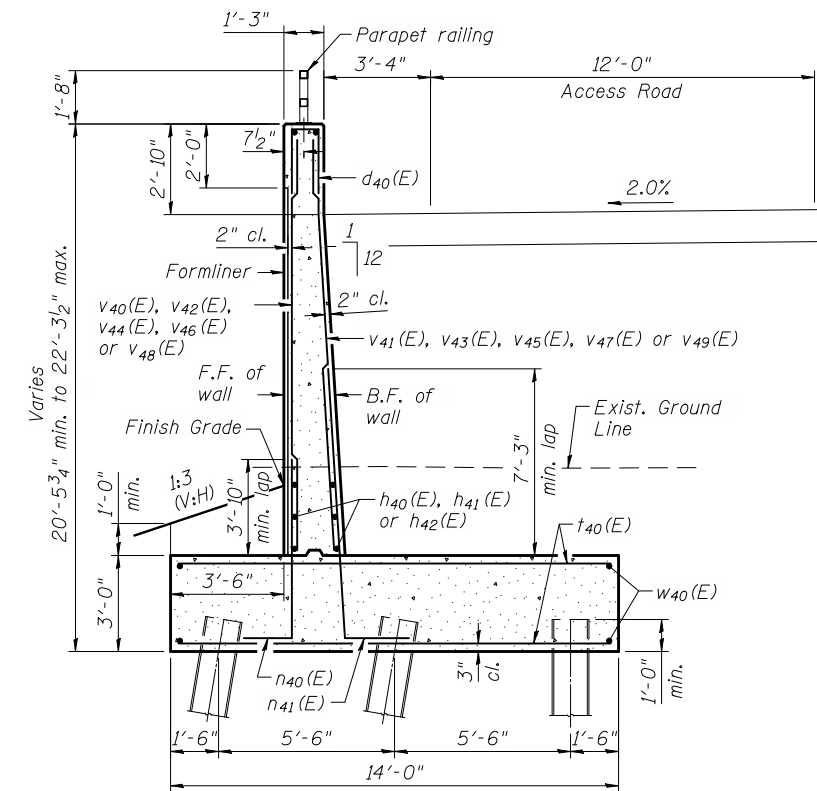
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345A	32VB		388	222
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				



PARTIAL ELEVATION - CPR RW-4
(Front Face Looking East)



PARTIAL FOOTING PLAN - CPR RW-4



SECTION A-A
(Looking upstation)

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

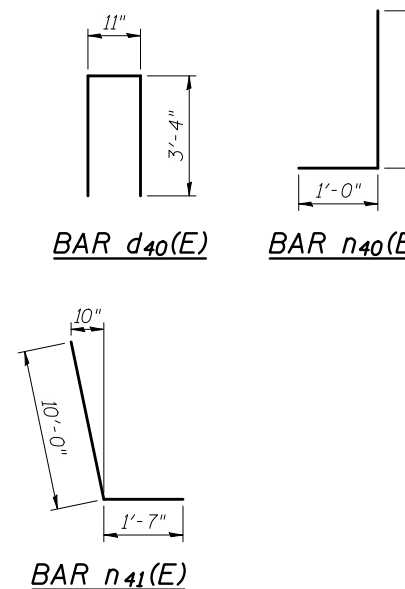
**CPR RW-4 PLAN AND ELEVATION
CPR RETAINING WALLS**

SHEET NO. 21 OF 42 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345A	32VB	DU PAGE	388	223
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				

**CPR RW-4
BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
d ₄₀ (E)	204	#6	7'-7"	⌐
h ₄₀ (E)	114	#5	29'-8"	—
h ₄₁ (E)	118	#5	33'-6"	—
h ₄₂ (E)	36	#5	27'-1"	—
n ₄₀ (E)	204	#6	7'-7"	J
n ₄₁ (E)	408	#9	11'-7"	L
t ₄₀ (E)	612	#8	13'-8"	—
v ₄₀ (E)	60	#6	18'-7"	—
v ₄₁ (E)	60	#9	18'-7"	—
v ₄₂ (E)	60	#6	18'-0"	—
v ₄₃ (E)	60	#9	18'-0"	—
v ₄₄ (E)	60	#6	17'-6"	—
v ₄₅ (E)	60	#9	17'-6"	—
v ₄₆ (E)	24	#6	17'-3"	—
v ₄₇ (E)	24	#9	17'-3"	—
w ₄₀ (E)	180	#6	37'-5"	—
Porous Granular Embankment			Cu. Yd.	480
Removal and Disposal of Unsuitable Material for Structures			Cu. Yd.	514
Form Liner Textured Surface			Sq. Ft.	3,337
Reinforcement Bars, Epoxy Coated			Pound	79,770
Furnishing Steel Piles HP14x89			Foot	4,444
Driving Piles			Foot	4,444
Test Pile Steel HP14x89			Each	1
Geocomposite Wall Drain			Sq. Yd.	316
Pipe Underdrains for Structures 4"			Foot	204
Parapet Railing			Foot	204
Concrete Structures CPR Special			Cu. Yd.	566.4

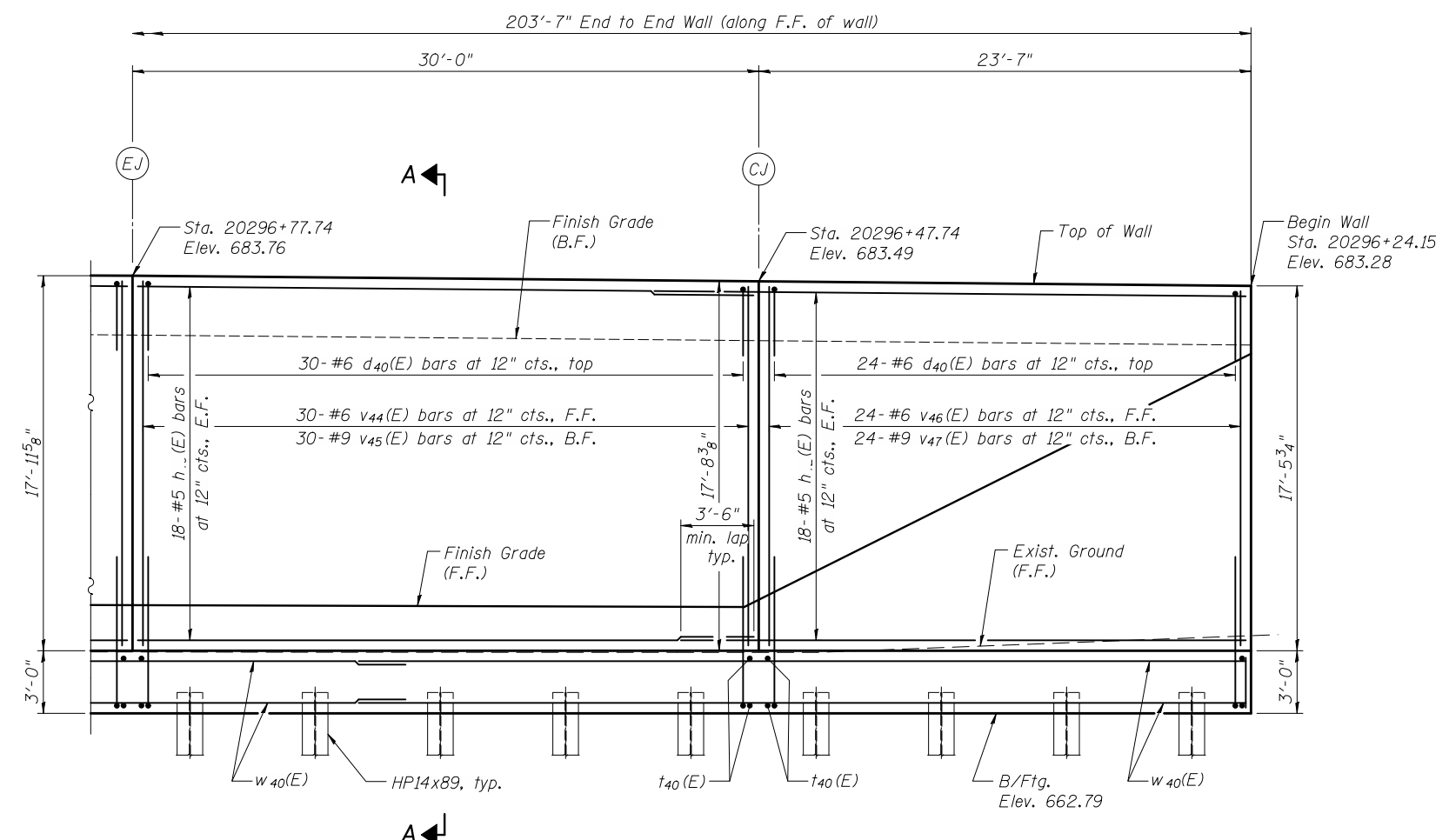


PILE DATA

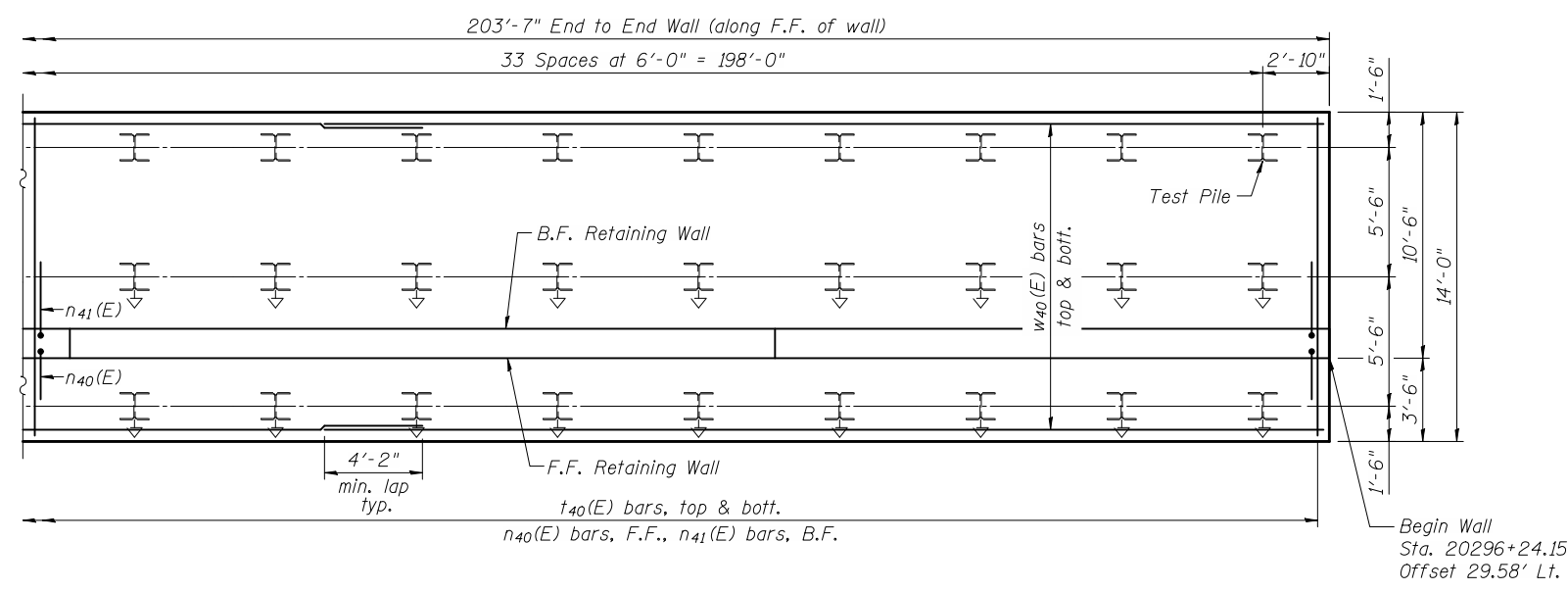
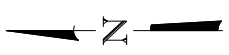
Type: HP14x89
 Min. Tip Elevation: 641.0
 Required Resistance: 258 kips
 Allowable Resistance: 129 kips
 Est. Length: 44 feet
 No. Piles: 101 + 1 Test Pile

NOTES:

- All piles must be driven to the minimum tip elevation shown. At the minimum tip elevation, driving shall be continued until the required resistance is achieved. "Required resistance" is equivalent to "nominal required bearing" in the IDOT Standard Specifications. The nominal driven bearing, as determined by the pile driving formula specified in Art. 512.14 of the IDOT Standard Specifications and modified by the Special Provisions, must be greater than or equal to the required resistance shown.
- Bars indicated thus 13 x 5-#6 etc. indicates 13 lines of bars with 5 lengths per line.



PARTIAL ELEVATION - CPR RW-4
(Front Face Looking East)



PARTIAL FOOTING PLAN - CPR RW-4

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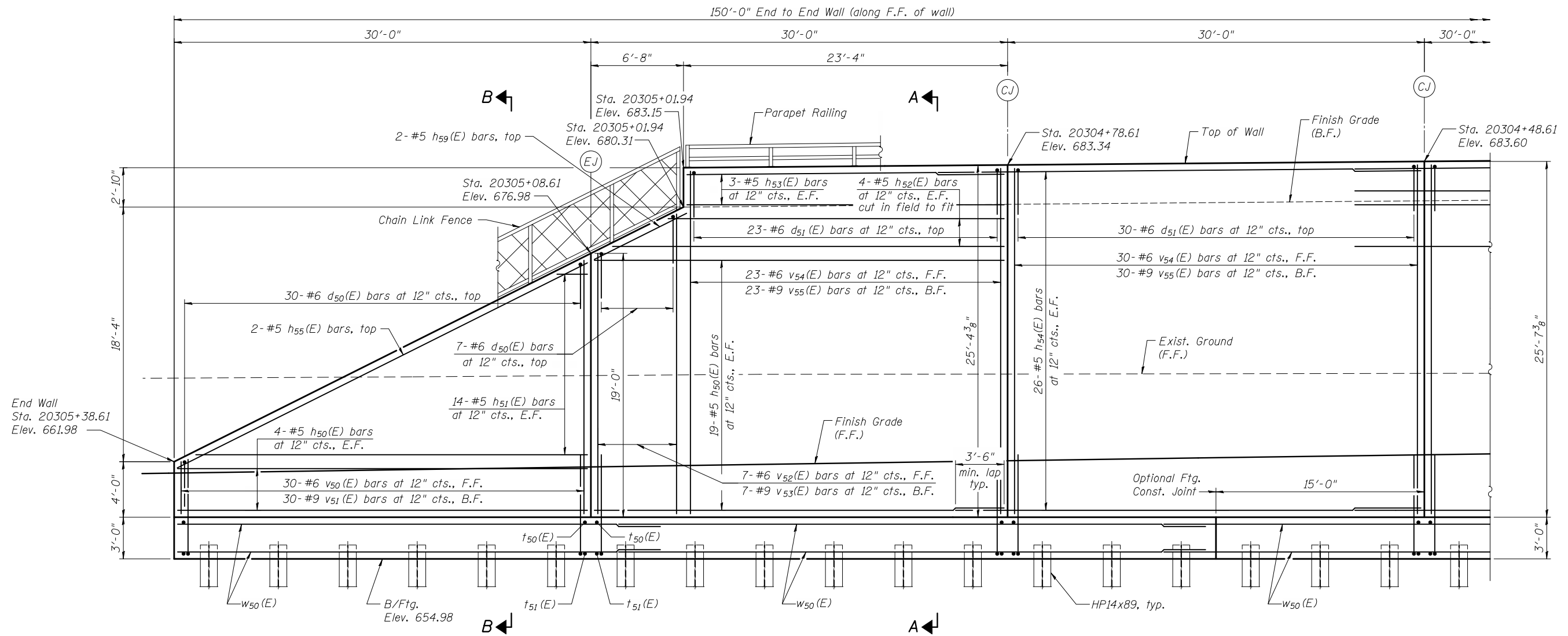
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CPR RW-4 PLAN AND ELEVATION
CPR RETAINING WALLS**

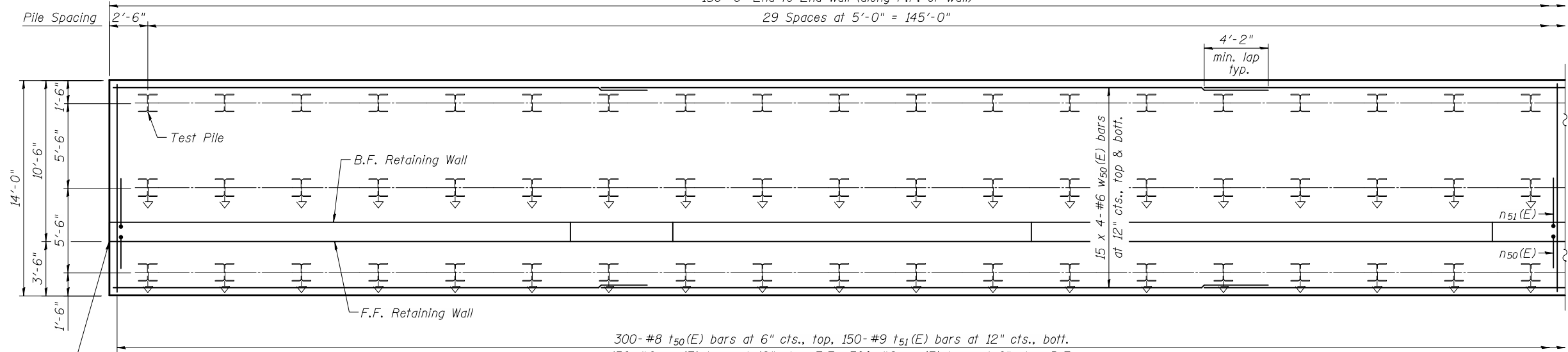
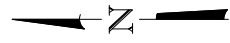
SHEET NO. 22 OF 42 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345A	32VB	DU PAGE	388	224
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				



PARTIAL ELEVATION - CPR RW-5

(Front Face Looking East)
 150'-0" End to End Wall (along F.F. of wall)
 29 Spaces at 5'-0" = 145'-0"



PARTIAL FOOTING PLAN - CPR RW-5

NOTES:

1. See Sheet 4 for Joint Details.

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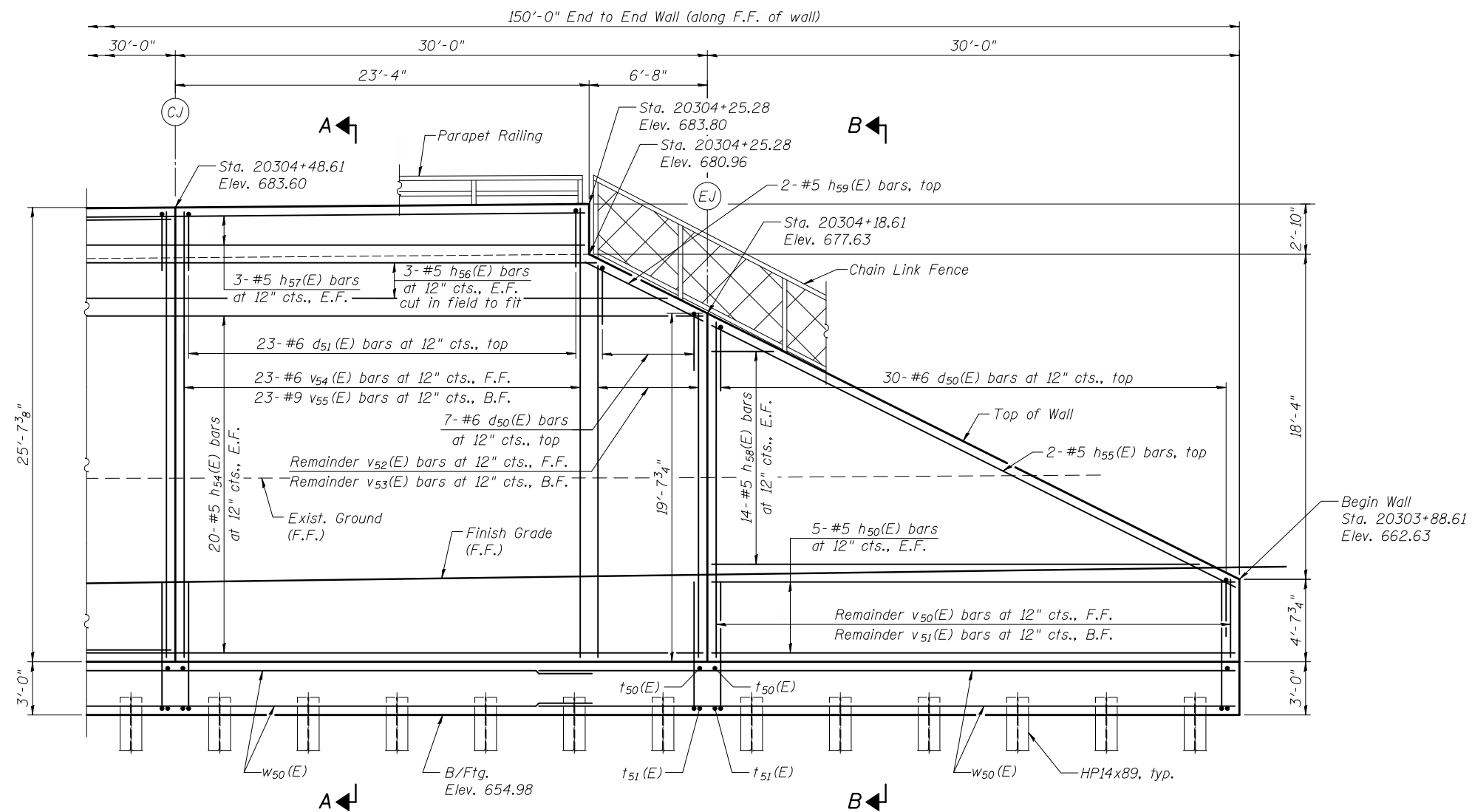
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

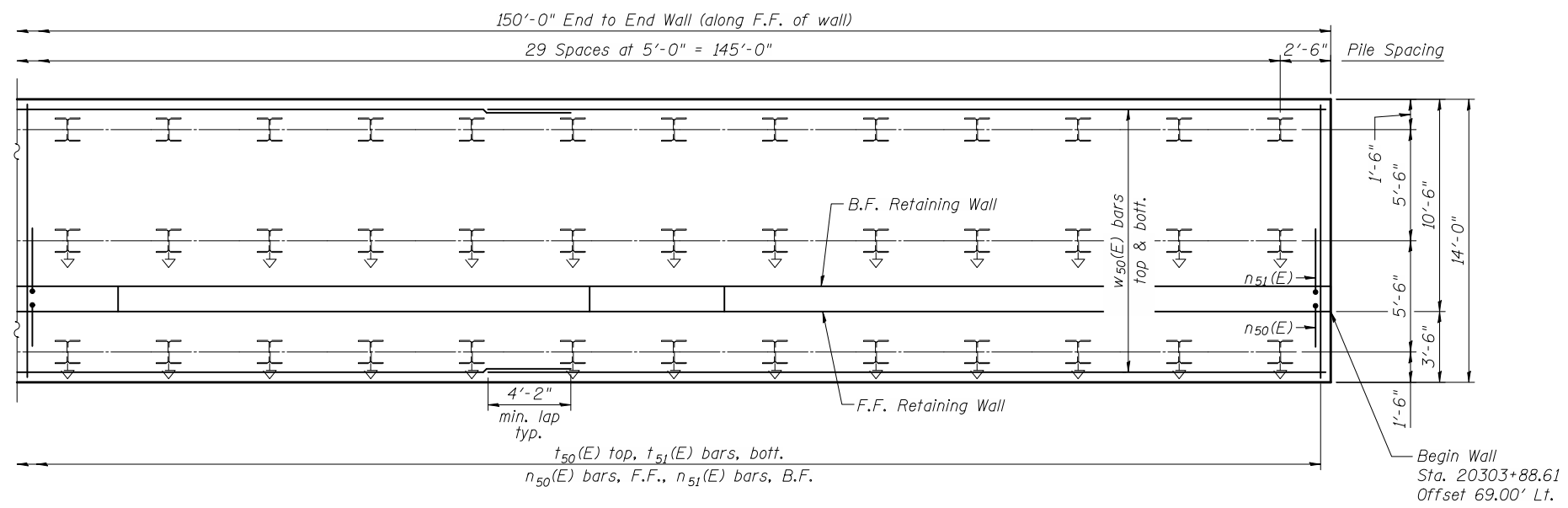
**CPR RW-5 PLAN AND ELEVATION
 CPR RETAINING WALLS**

SHEET NO. 23 OF 42 SHEETS

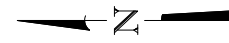
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345A	32VB	DU PAGE	388	225
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				



PARTIAL ELEVATION - CPR RW-5
(Front Face Looking East)



PARTIAL FOOTING PLAN - CPR RW-5



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CPR RW-5 PLAN AND ELEVATION
CPR RETAINING WALLS

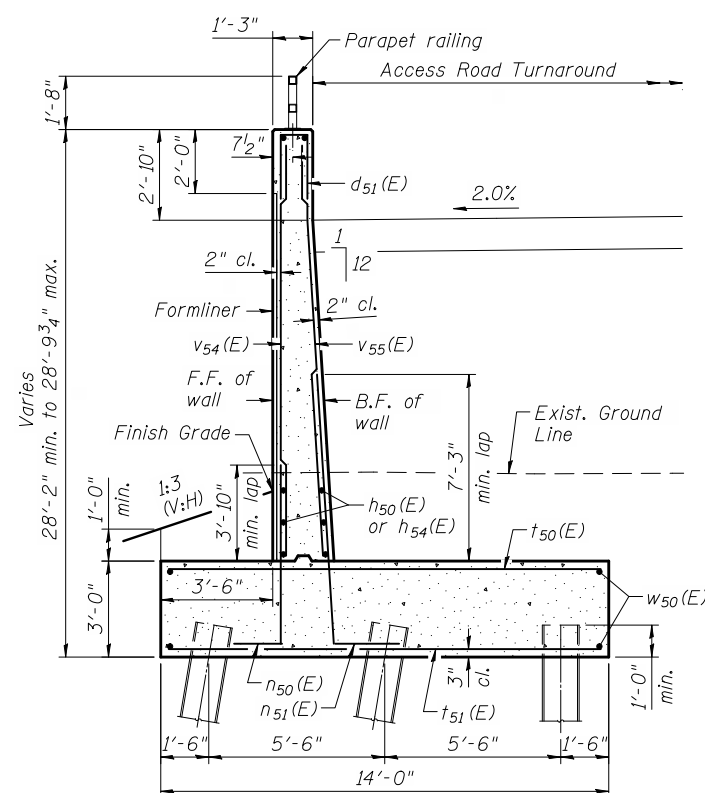
SHEET NO. 24 OF 42 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345A	32VB	DU PAGE	388	226
CONTRACT NO. 60W01				

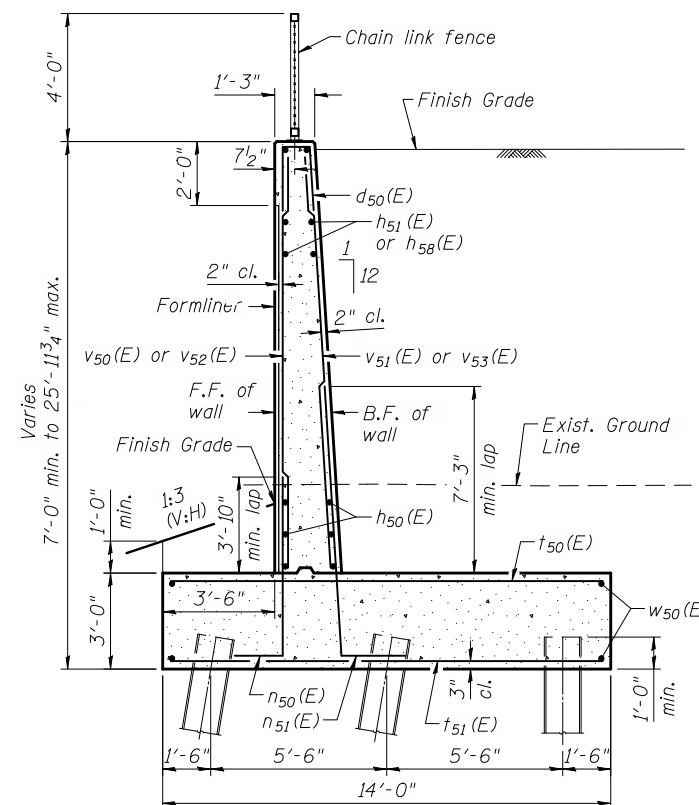
ILLINOIS FED. AID PROJECT

**CPR RW-5
BILL OF MATERIAL**

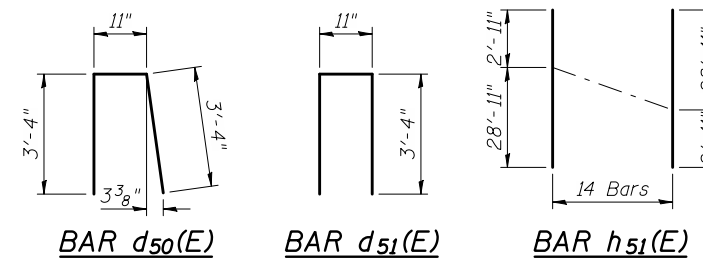
Bar	No.	Size	Length	Shape
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d ₅₁ (E)	76	#6	7'-7"	Π
h ₅₀ (E)	56	#5	29'-8"	—
h ₅₁ (E)	14	#5	31'-10"	—
h ₅₂ (E)	8	#5	28'-11"	—
h ₅₃ (E)	6	#5	23'-0"	—
h ₅₄ (E)	92	#5	33'-6"	—
h ₅₅ (E)	4	#5	33'-0"	—
h ₅₆ (E)	6	#5	33'-2"	—
h ₅₇ (E)	6	#5	26'-10"	—
h ₅₈ (E)	14	#5	30'-6"	—
h ₅₉ (E)	4	#5	7'-6"	—
n ₅₀ (E)	150	#6	7'-7"	J
n ₅₁ (E)	300	#9	11'-7"	L
t ₅₀ (E)	300	#8	13'-8"	—
t ₅₁ (E)	150	#9	13'-8"	—
v ₅₀ (E)	30	#6	23'-0"	—
v ₅₁ (E)	30	#9	23'-0"	—
v ₅₂ (E)	7	#6	41'-6"	—
v ₅₃ (E)	7	#9	41'-6"	—
v ₅₄ (E)	76	#6	25'-0"	—
v ₅₅ (E)	76	#9	25'-0"	—
w ₅₀ (E)	120	#6	40'-7"	—
Porous Granular Embankment		Cu. Yd.	442	
Removal and Disposal of Unsuitable Material for Structures		Cu. Yd.	1,858	
Form Liner Textured Surface		Sq. Ft.	2,644	
Reinforcement Bars, Epoxy Coated		Pound	61,380	
Furnishing Steel Piles HP14x89		Foot	3,827	
Driving Piles		Foot	3,827	
Test Pile Steel HP14x89		Each	1	
Geocomposite Wall Drain		Sq. Yd.	277	
Pipe Underdrains for Structures 4"		Foot	150	
Chain Link Fence, 4'		Foot	82	
Parapet Railing		Foot	77	
Concrete Structures CPR Special		Cu. Yd.	457.8	



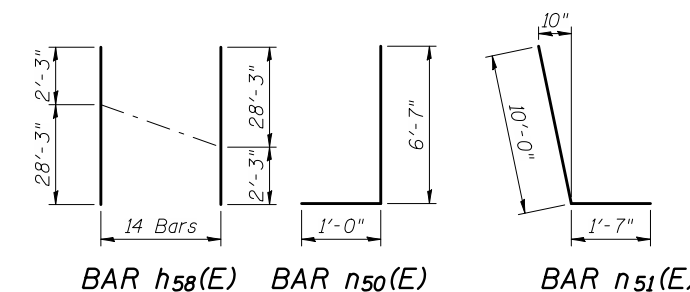
SECTION A-A
(Looking upstation)



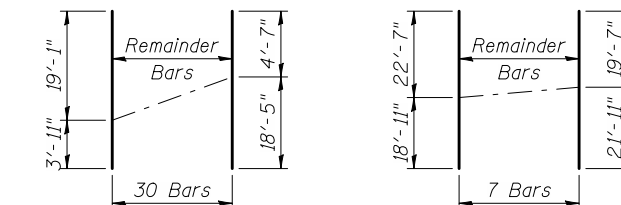
SECTION B-B
(Looking upstation)



BAR d₅₀(E) BAR d₅₁(E) BAR h₅₁(E)



BAR h₅₈(E) BAR n₅₀(E) BAR n₅₁(E)



BARS v₅₀(E) & v₅₁(E) BARS v₅₂(E) & v₅₃(E)

PILE DATA

Type: HP14x89
 Min. Tip Elevation: 640.0
 Required Resistance: 284 kips
 Allowable Resistance Available: 142 kips
 Est. Length: 43 feet
 No. Piles: 89 + 1 Test Pile

NOTES:

- All piles must be driven to the minimum tip elevation shown. At the minimum tip elevation, driving shall be continued until the required resistance is achieved. "Required resistance" is equivalent to "nominal required bearing" in the IDOT Standard Specifications. The nominal driven bearing, as determined by the pile driving formula specified in Art. 512.14 of the IDOT Standard Specifications and modified by the Special Provisions, must be greater than or equal to the required resistance shown.
- Bars indicated thus 13 x 5-#6 etc. indicates 13 lines of bars with 5 lengths per line.

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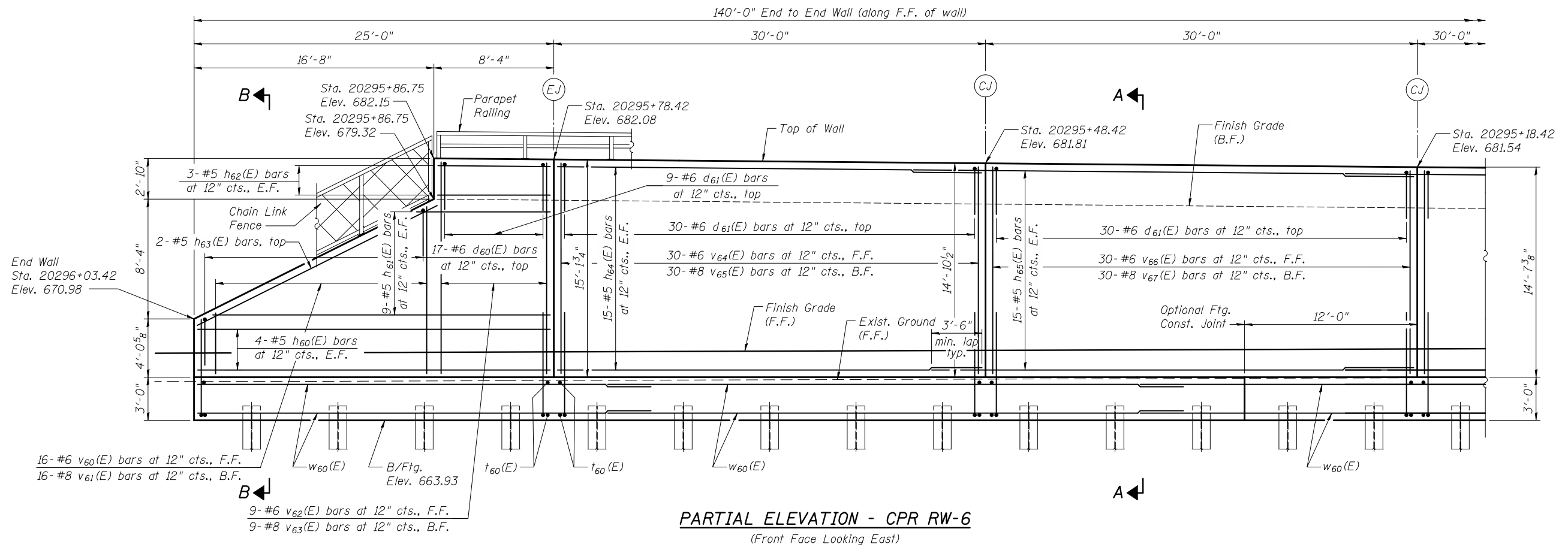
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

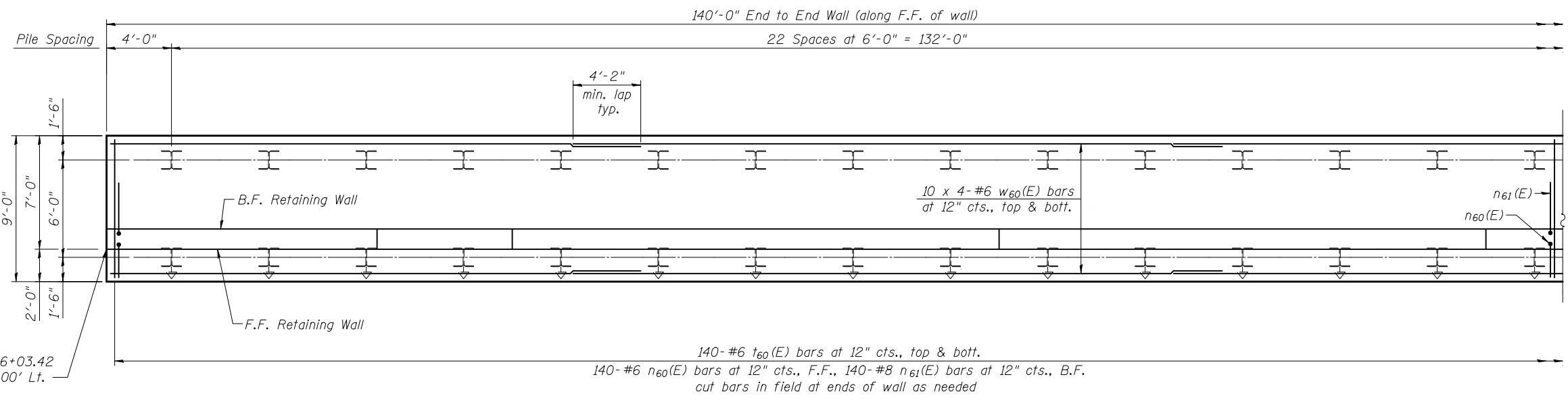
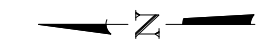
**CPR RW-5 SECTIONS & BILL OF MATERIAL
CPR RETAINING WALLS**

SHEET NO. 25 OF 42 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345A	32VB	DU PAGE	388	227
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				



PARTIAL ELEVATION - CPR RW-6
(Front Face Looking East)



PARTIAL FOOTING PLAN - CPR RW-6

NOTES:

- See Sheet 4 for Joint Details.

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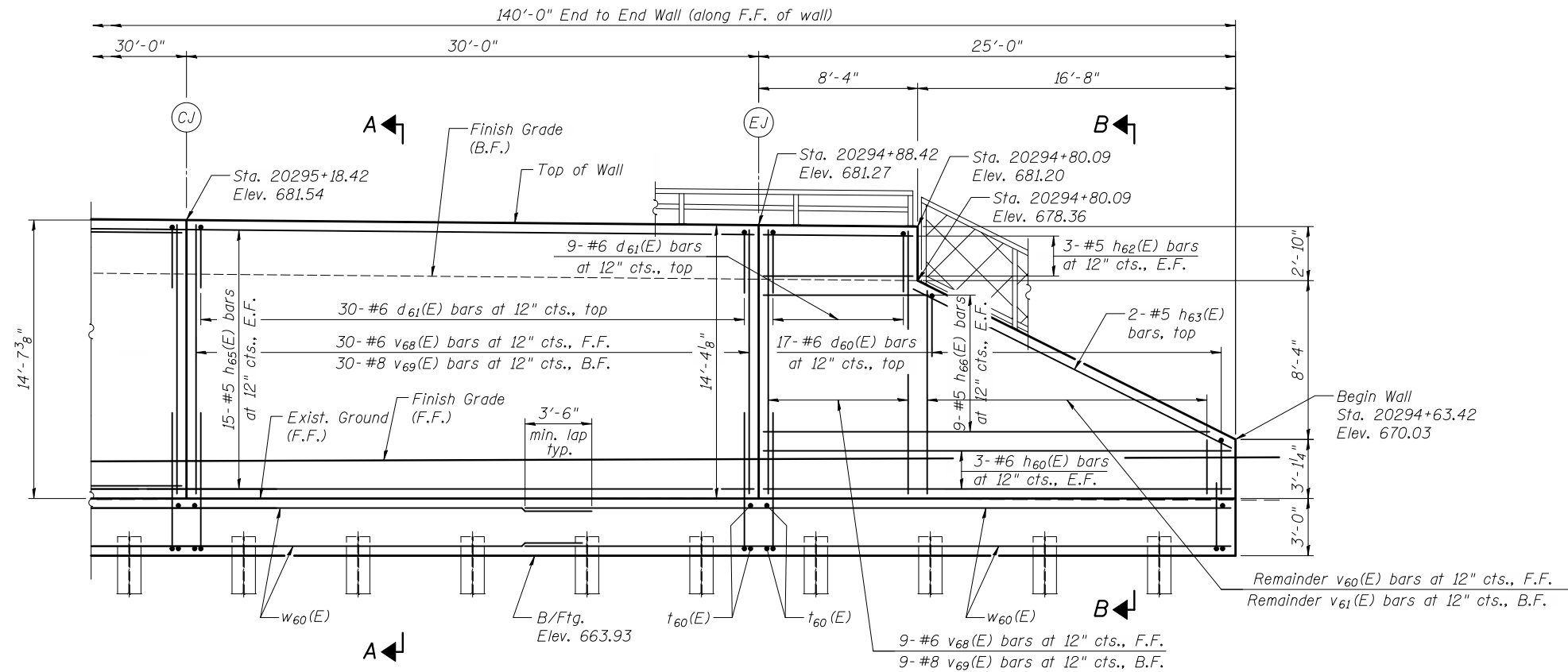
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

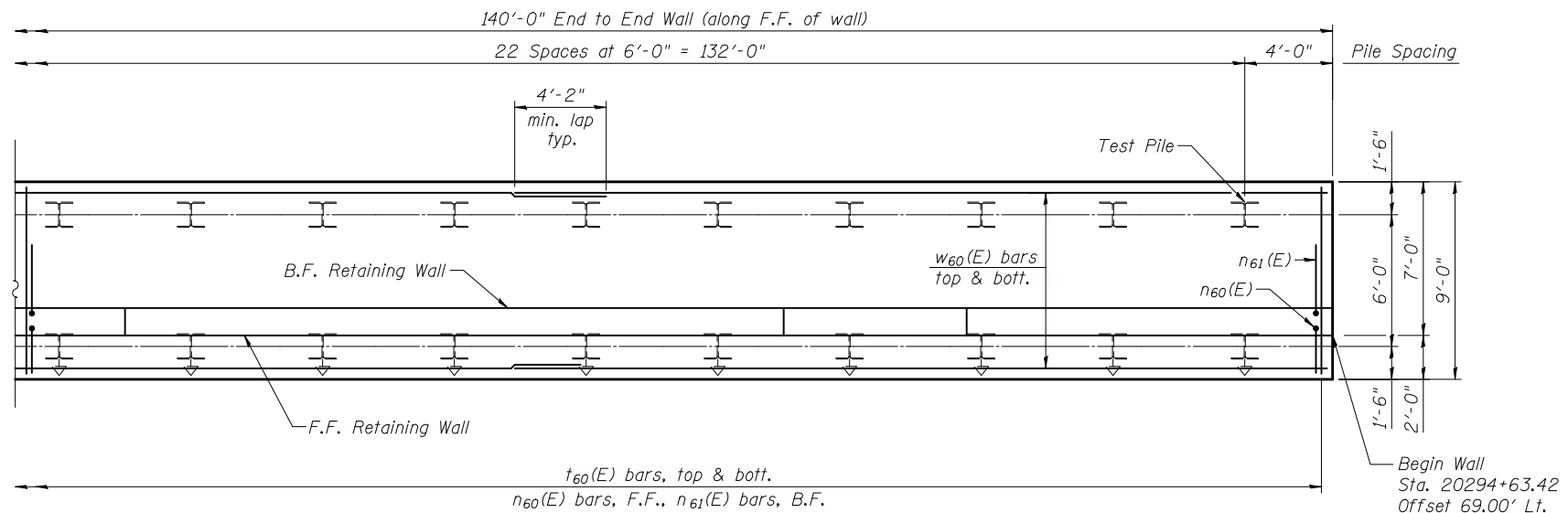
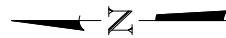
**CPR RW-6 PLAN AND ELEVATION
CPR RETAINING WALLS**

SHEET NO. 26 OF 42 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345A	32VB		388	228
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				



PARTIAL ELEVATION - CPR RW-6
(Front Face Looking East)



PARTIAL FOOTING PLAN - CPR RW-6

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

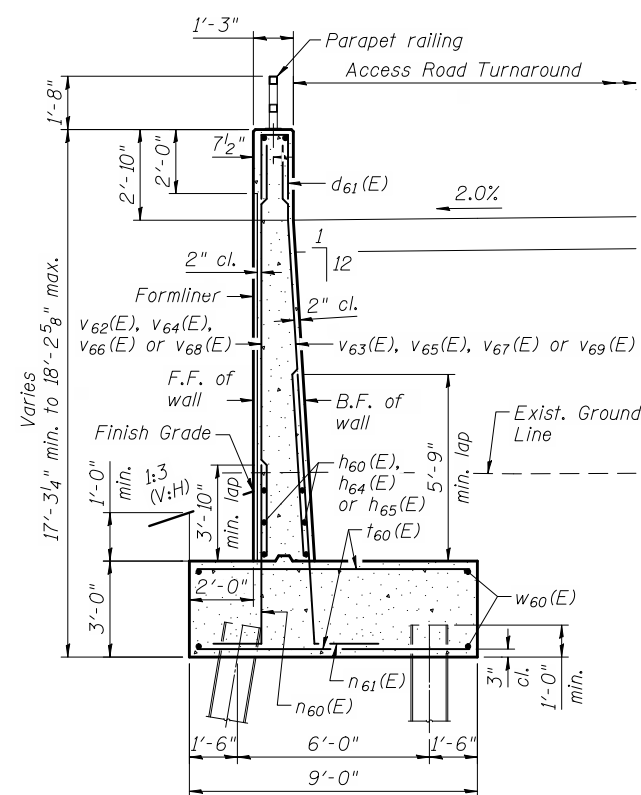
**CPR RW-6 PLAN AND ELEVATION
CPR RETAINING WALLS**

SHEET NO. 27 OF 42 SHEETS

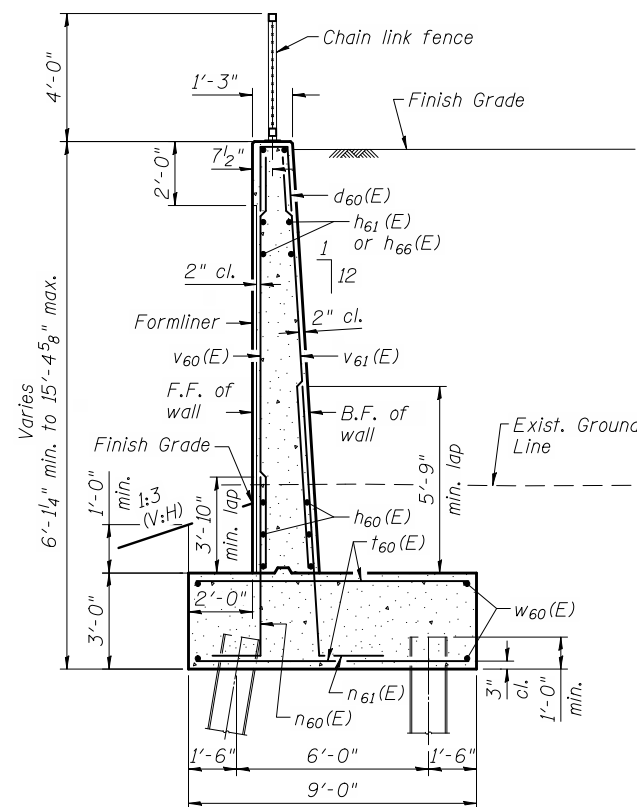
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345A	32VB		388	229
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				

**CPR RW-6
BILL OF MATERIAL**

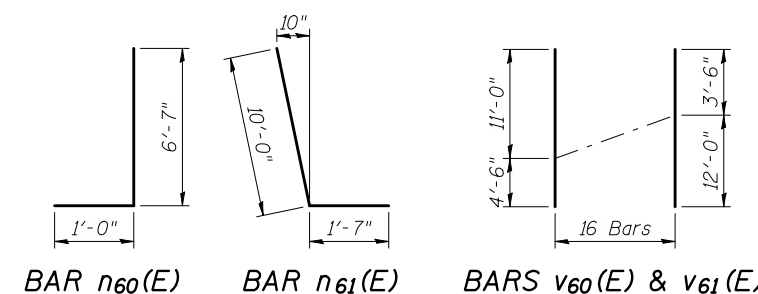
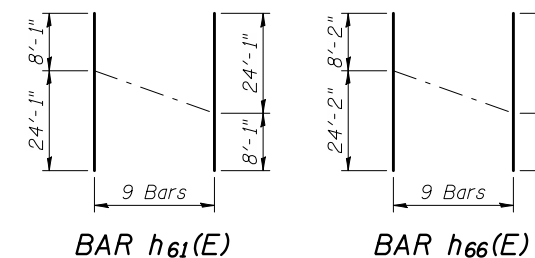
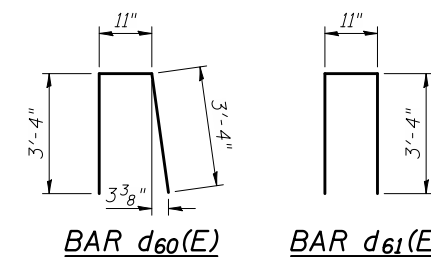
Bar	No.	Size	Length	Shape
d ₆₀ (E)	34	#6	7'-7"	Π
d ₆₁ (E)	108	#6	7'-7"	Π
h ₆₀ (E)	14	#5	24'-8"	—
h ₆₁ (E)	9	#5	32'-2"	—
h ₆₂ (E)	12	#5	8'-0"	—
h ₆₃ (E)	4	#5	18'-8"	—
h ₆₄ (E)	30	#5	29'-8"	—
h ₆₅ (E)	60	#5	33'-6"	—
h ₆₆ (E)	9	#5	32'-4"	—
n ₆₀ (E)	140	#6	7'-7"	J
n ₆₁ (E)	140	#8	11'-7"	L
t ₆₀ (E)	140	#6	8'-8"	—
v ₆₀ (E)	16	#6	15'-6"	—
v ₆₁ (E)	16	#8	15'-6"	—
v ₆₂ (E)	9	#6	14'-11"	—
v ₆₃ (E)	9	#8	14'-11"	—
v ₆₄ (E)	30	#6	14'-8"	—
v ₆₅ (E)	30	#8	14'-8"	—
v ₆₆ (E)	30	#6	14'-5"	—
v ₆₇ (E)	30	#8	14'-5"	—
v ₆₈ (E)	39	#6	14'-1"	—
v ₆₉ (E)	39	#8	14'-1"	—
w ₆₀ (E)	80	#6	38'-1"	—
Porous Granular Embankment		Cu. Yd.	213	
Removal and Disposal of Unsuitable Material for Structures		Cu. Yd.	244	
Form Liner Textured Surface		Sq. Ft.	1,552	
Reinforcement Bars, Epoxy Coated		Pound	25,640	
Furnishing Steel Piles HP14x89		Foot	990	
Driving Piles		Foot	990	
Test Pile Steel HP14x89		Each	1	
Geocomposite Wall Drain		Sq. Yd.	139	
Pipe Underdrains for Structures 4"		Foot	140	
Chain Link Fence, 4'		Foot	38	
Parapet Railing		Foot	107	
Concrete Structures CPR Special		Cu. Yd.	252.2	



SECTION A-A
(Looking upstasion)



SECTION B-B
(Looking upstasion)



PILE DATA

Type: HP14x89
 Min. Tip Elevation: 648.0
 Required Resistance: 230 kips
 Allowable Resistance
 Available: 115 kips
 Est. Length: 22 feet
 No. Piles: 45 + 1 Test Pile

NOTES:

- All piles must be driven to the minimum tip elevation shown. At the minimum tip elevation, driving shall be continued until the required resistance is achieved. "Required resistance" is equivalent to "nominal required bearing" in the IDOT Standard Specifications. The nominal driven bearing, as determined by the pile driving formula specified in Art. 512.14 of the IDOT Standard Specifications and modified by the Special Provisions, must be greater than or equal to the required resistance shown.
- Bars indicated thus 13 x 5-#6 etc. indicates 13 lines of bars with 5 lengths per line.

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jmgus

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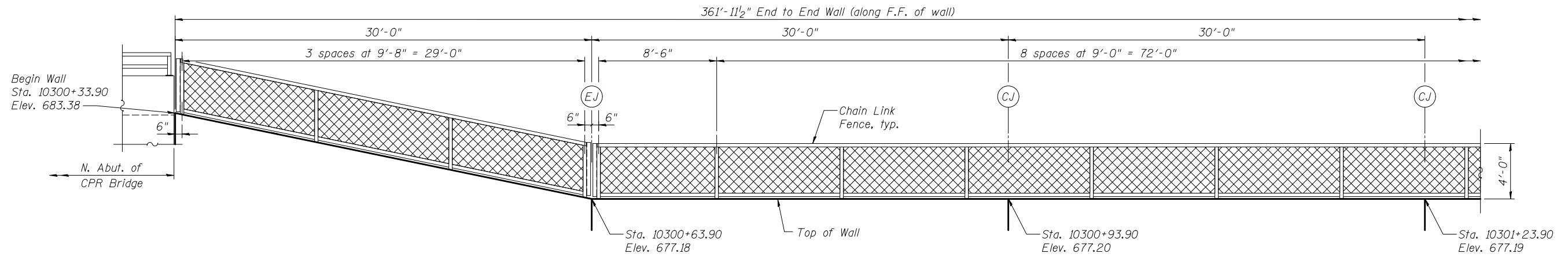
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

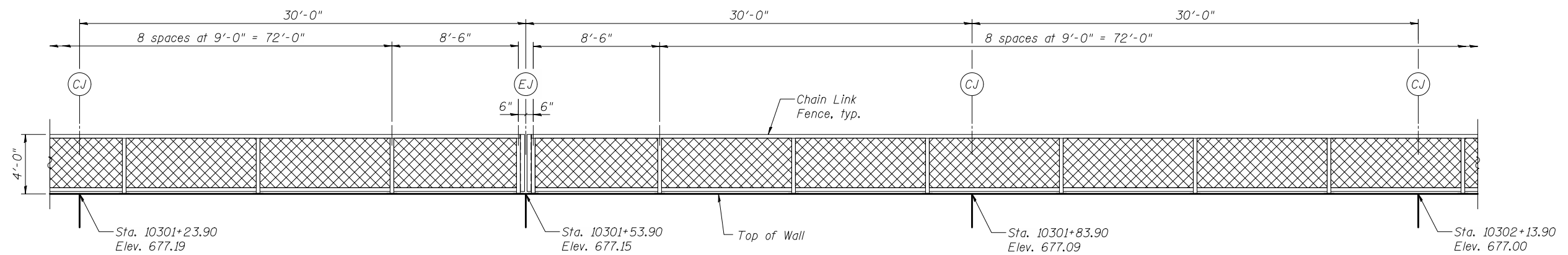
**CPR RW-6 SECTIONS & BILL OF MATERIAL
CPR RETAINING WALLS**

SHEET NO. 28 OF 42 SHEETS

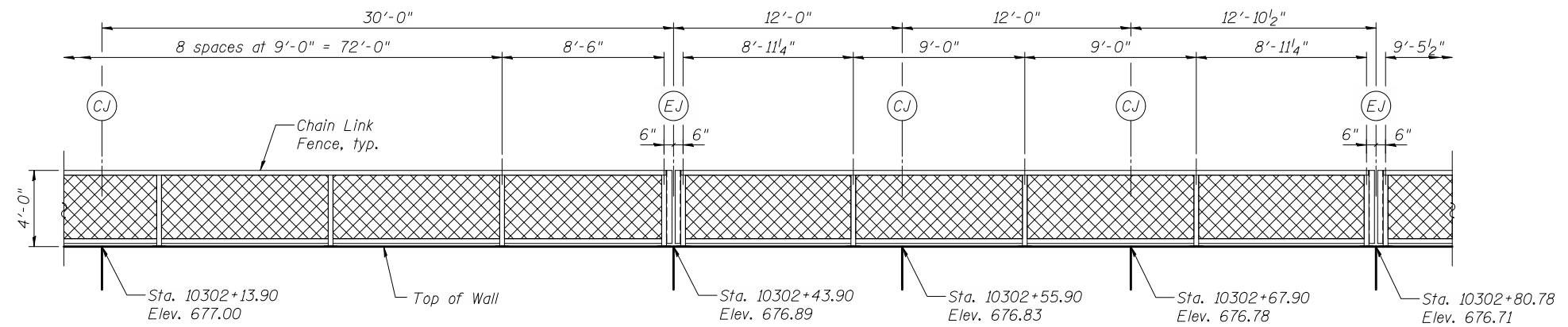
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345A	32VB		388	230
				CONTRACT NO. 60W01
ILLINOIS FED. AID PROJECT				



PARTIAL ELEVATION - CPR RW-1
(Front Face Looking West)



PARTIAL ELEVATION - CPR RW-1
(Front Face Looking West)



PARTIAL ELEVATION - CPR RW-1
(Front Face Looking West)

NOTES:
1. See Sheet 35 for additional Railing Details.

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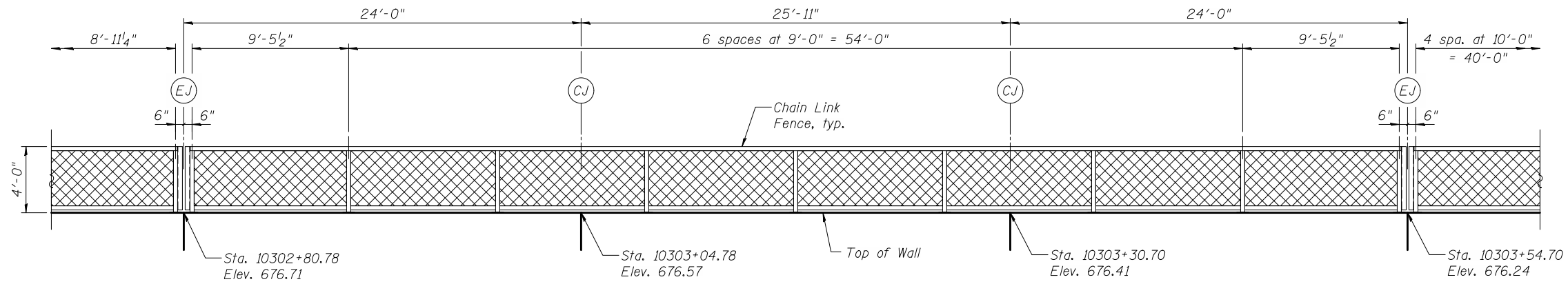
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**RAILING DETAILS
CPR RETAINING WALLS**

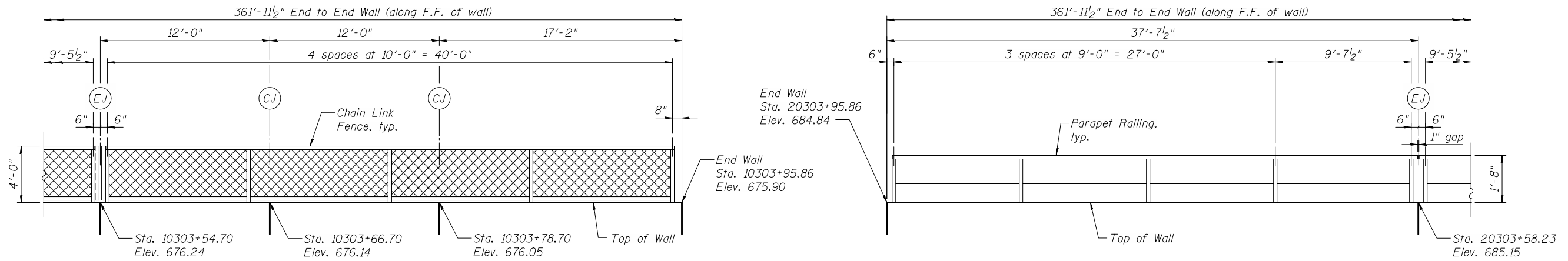
SHEET NO. 29 OF 42 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345A	32VB		388	231
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				



PARTIAL ELEVATION - CPR RW-1

(Front Face Looking West)

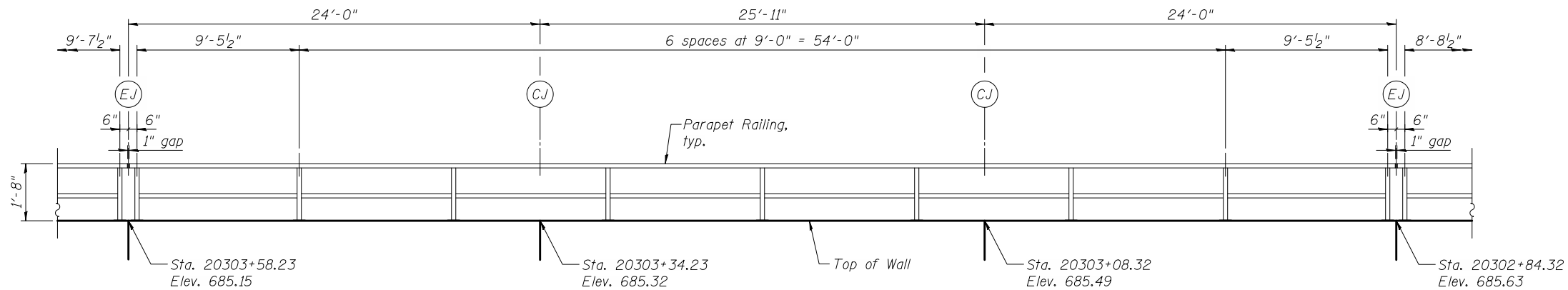


PARTIAL ELEVATION - CPR RW-1

(Front Face Looking West)

PARTIAL ELEVATION - CPR RW-2

(Front Face Looking East)



PARTIAL ELEVATION - CPR RW-2

(Front Face Looking East)

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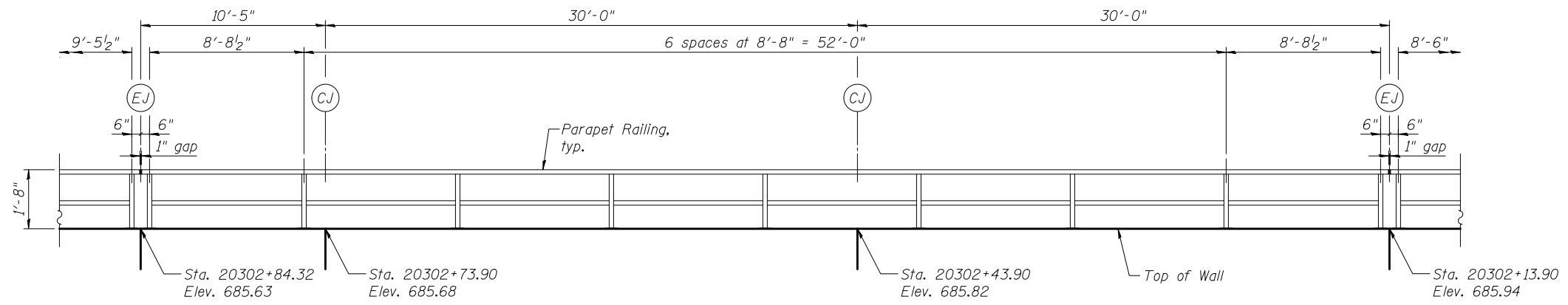
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

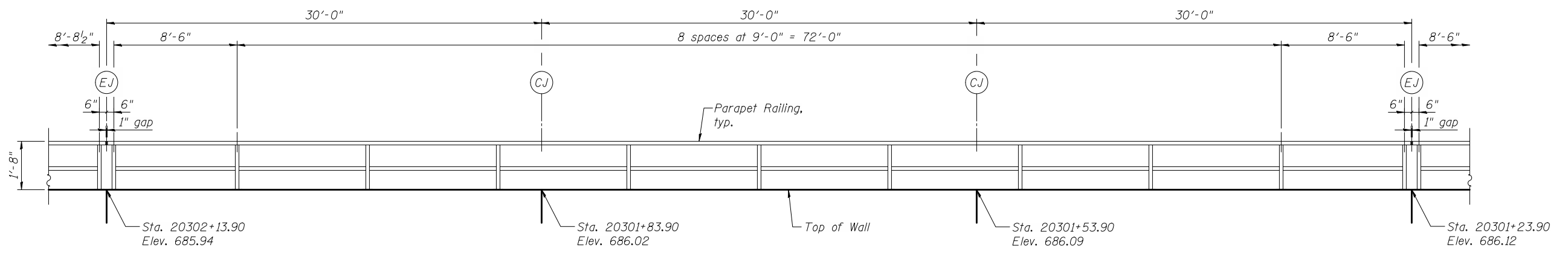
**RAILING DETAILS
CPR RETAINING WALLS**

SHEET NO. 30 OF 42 SHEETS

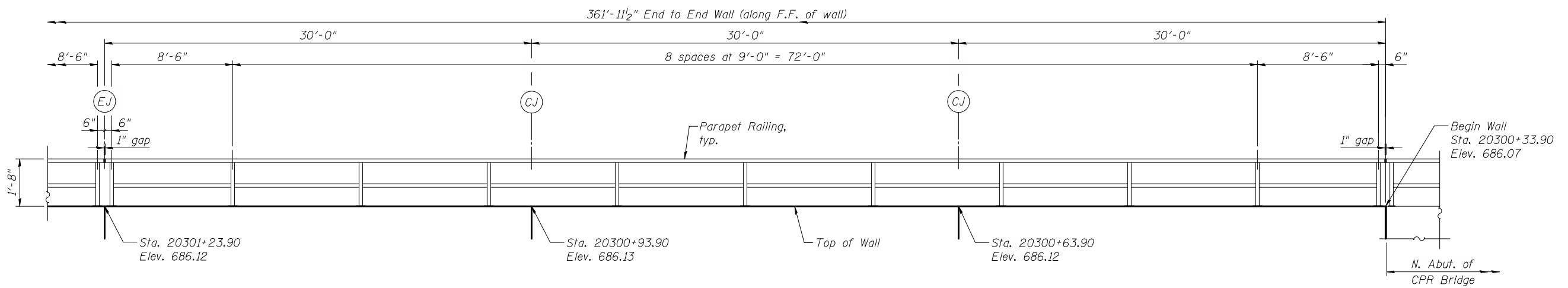
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345A	32VB	DU PAGE	388	232
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				



PARTIAL ELEVATION - CPR RW-2
(Front Face Looking East)



PARTIAL ELEVATION - CPR RW-2
(Front Face Looking East)



PARTIAL ELEVATION - CPR RW-2
(Front Face Looking East)

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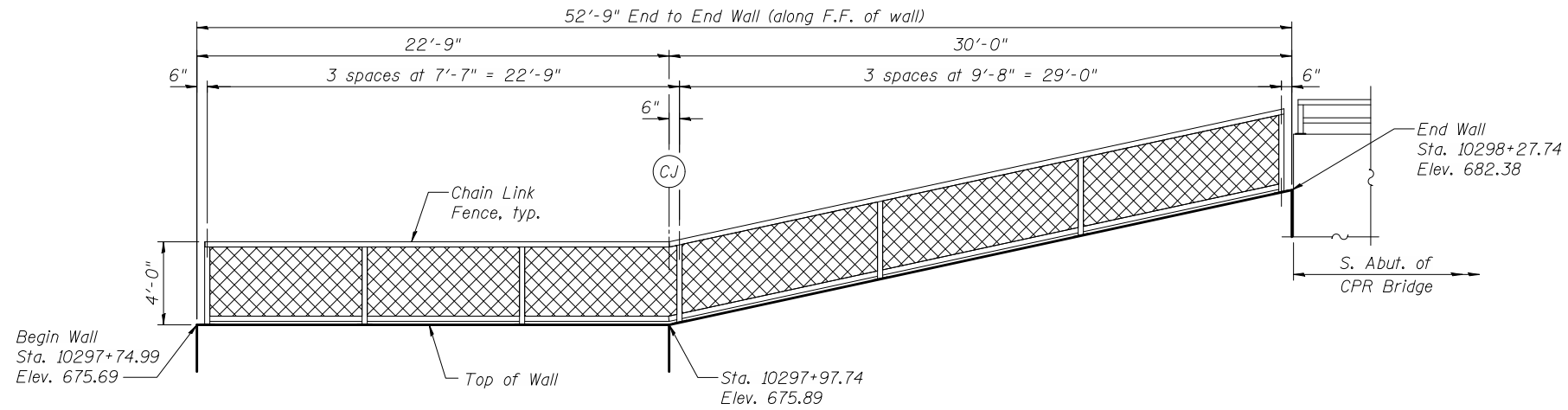
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

RAILING DETAILS
CPR RETAINING WALLS

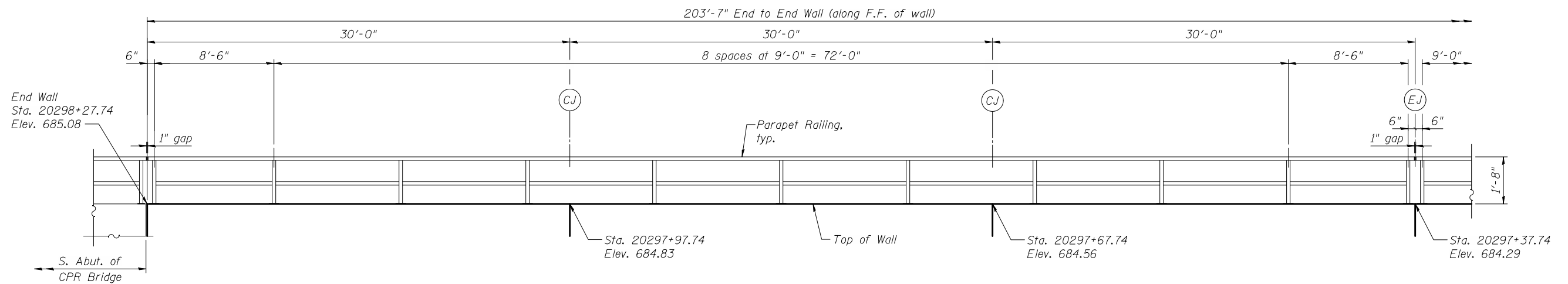
SHEET NO. 31 OF 42 SHEETS

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CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				



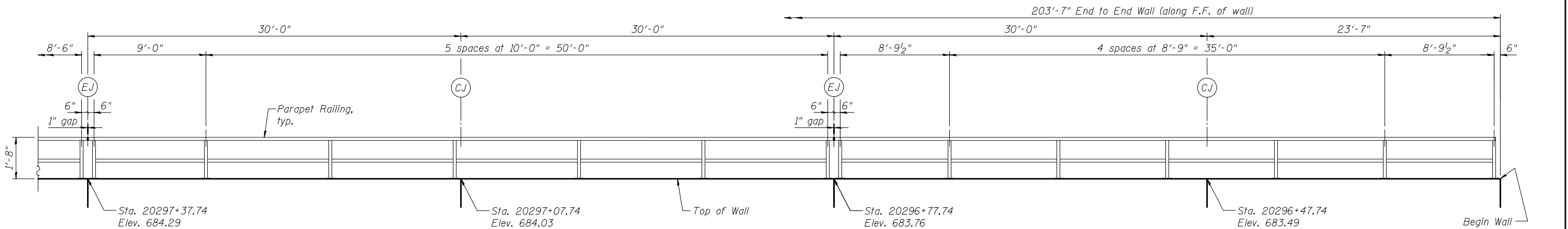
ELEVATION - CPR RW-3

(Front Face Looking West)



PARTIAL ELEVATION - CPR RW-4

(Front Face Looking East)



PARTIAL ELEVATION - CPR RW-4

(Front Face Looking East)

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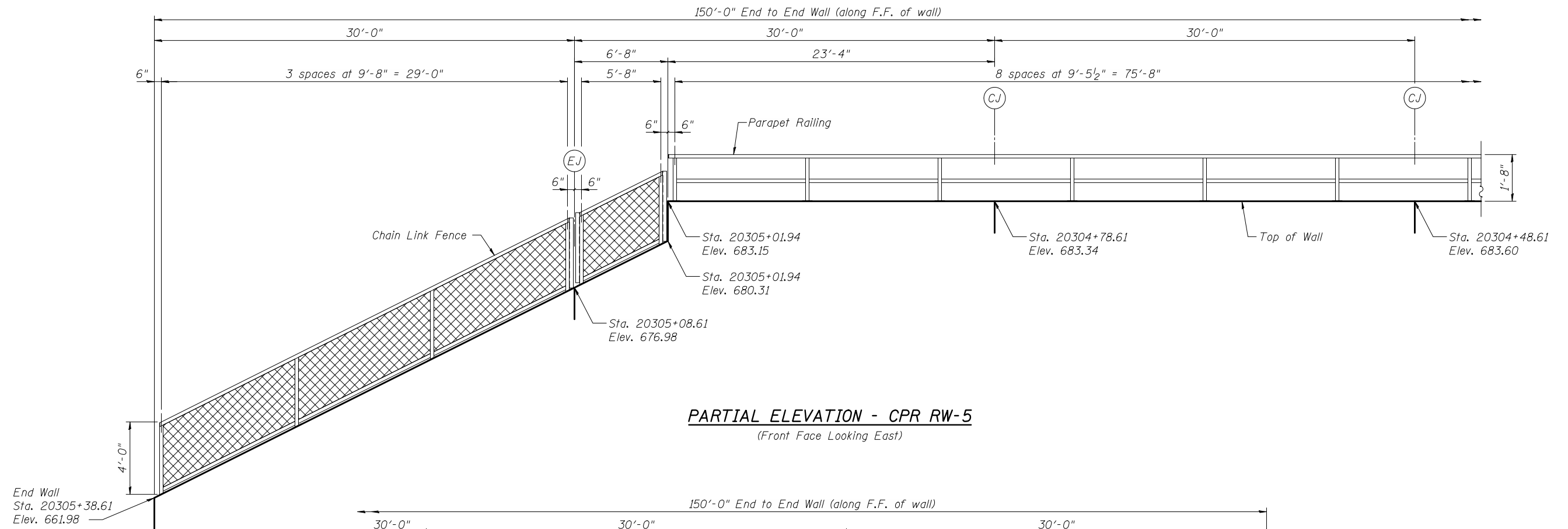
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

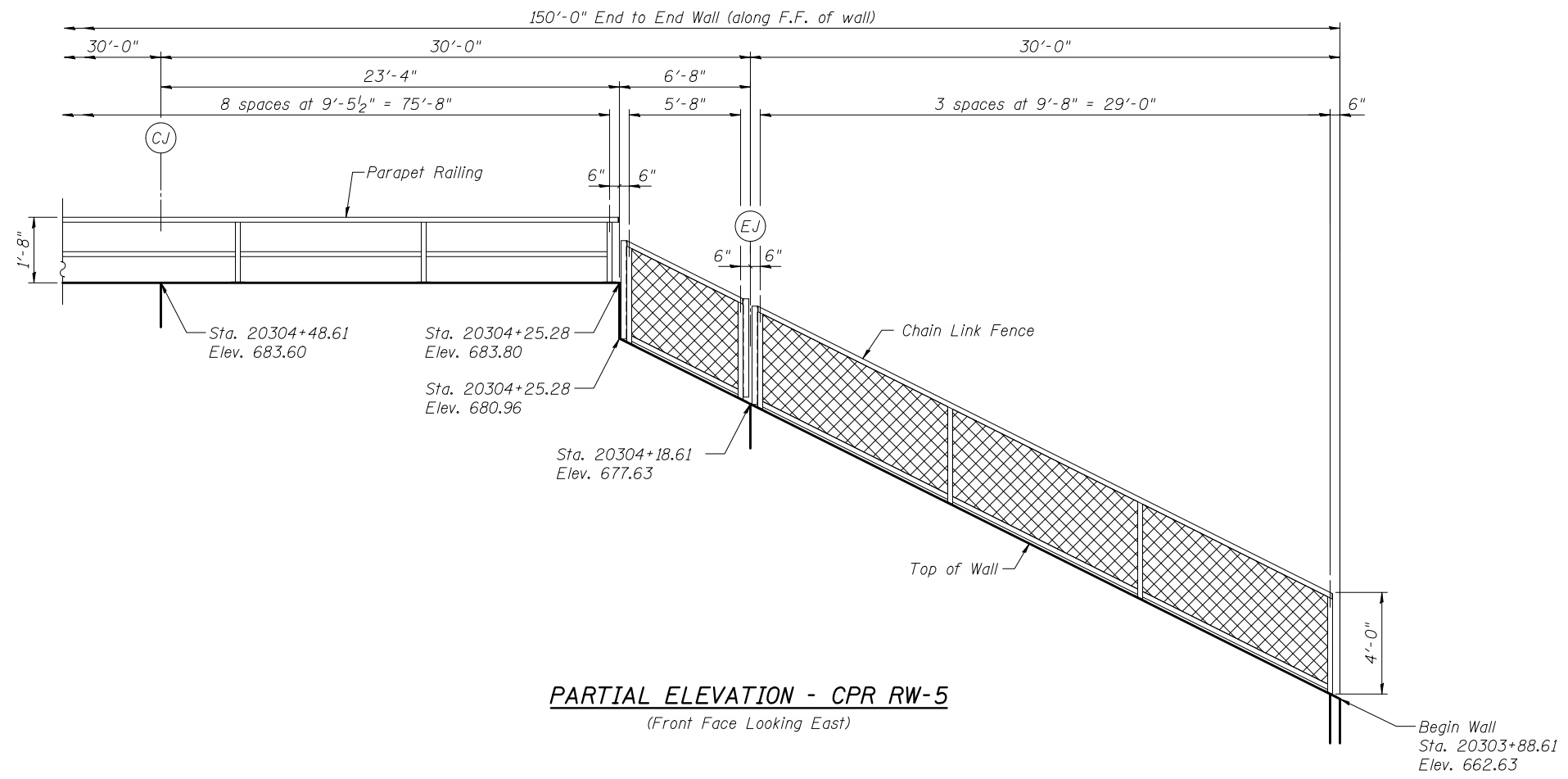
**RAILING DETAILS
CPR RETAINING WALLS**

SHEET NO. 32 OF 42 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345A	32VB	DU PAGE	388	234
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				



PARTIAL ELEVATION - CPR RW-5
(Front Face Looking East)



PARTIAL ELEVATION - CPR RW-5
(Front Face Looking East)

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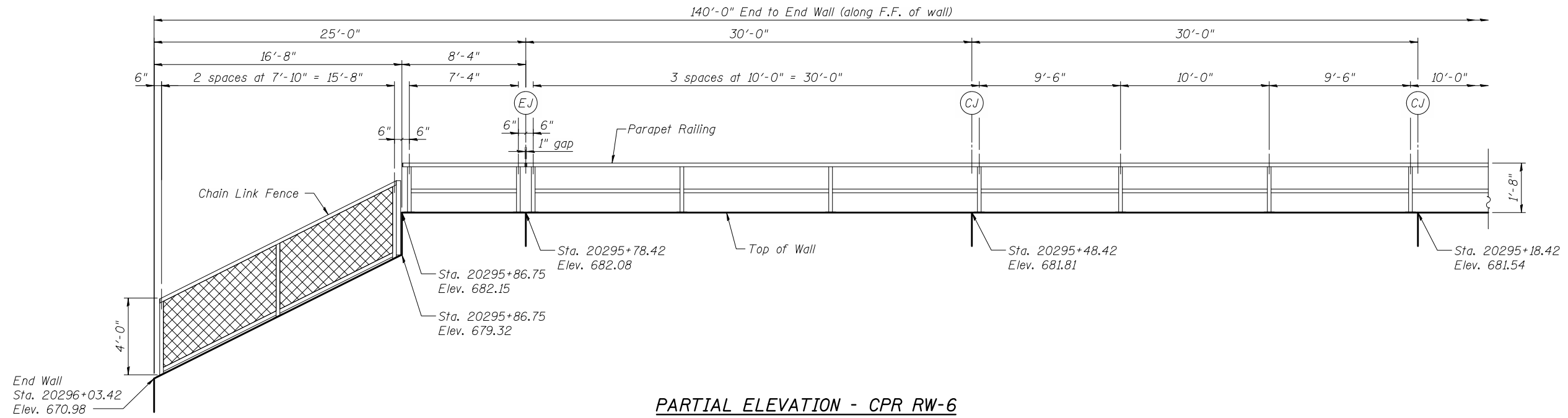
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

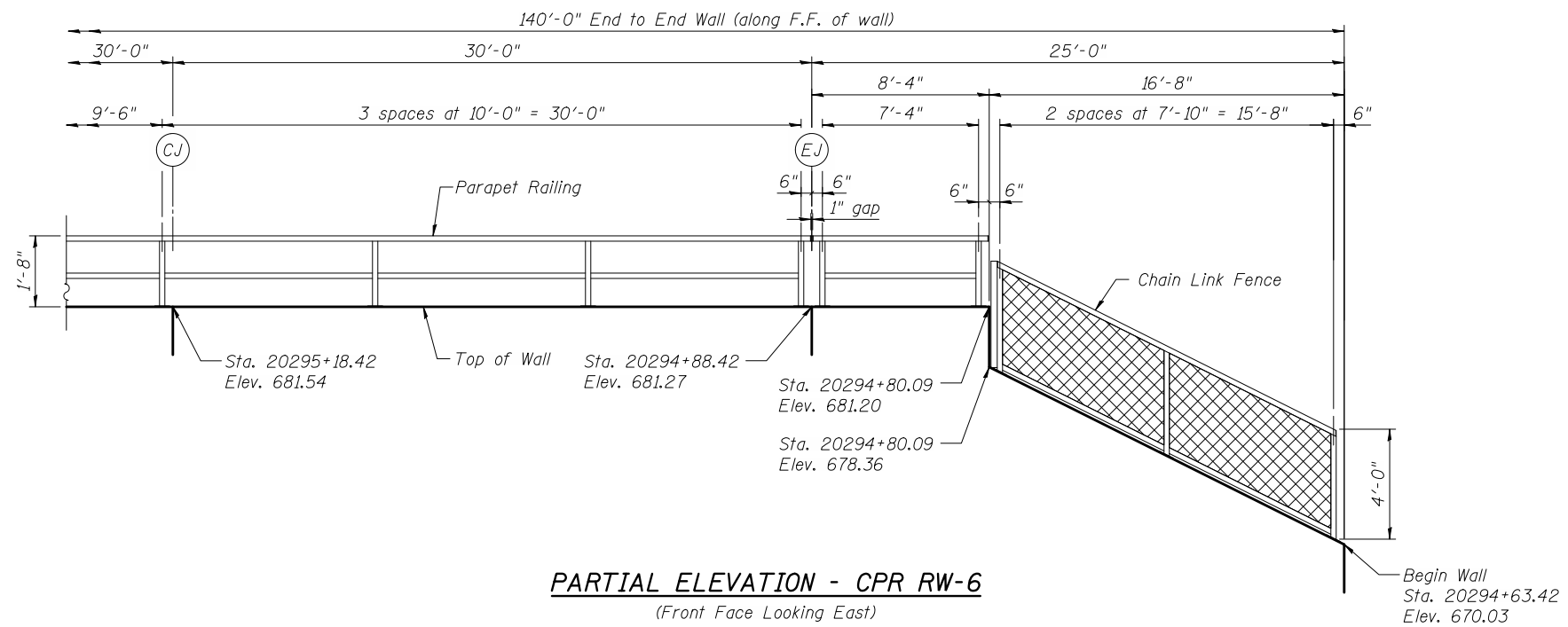
**RAILING DETAILS
CPR RETAINING WALLS**

SHEET NO. 33 OF 42 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345A	32VB	DU PAGE	388	235
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				



PARTIAL ELEVATION - CPR RW-6
(Front Face Looking East)



PARTIAL ELEVATION - CPR RW-6
(Front Face Looking East)

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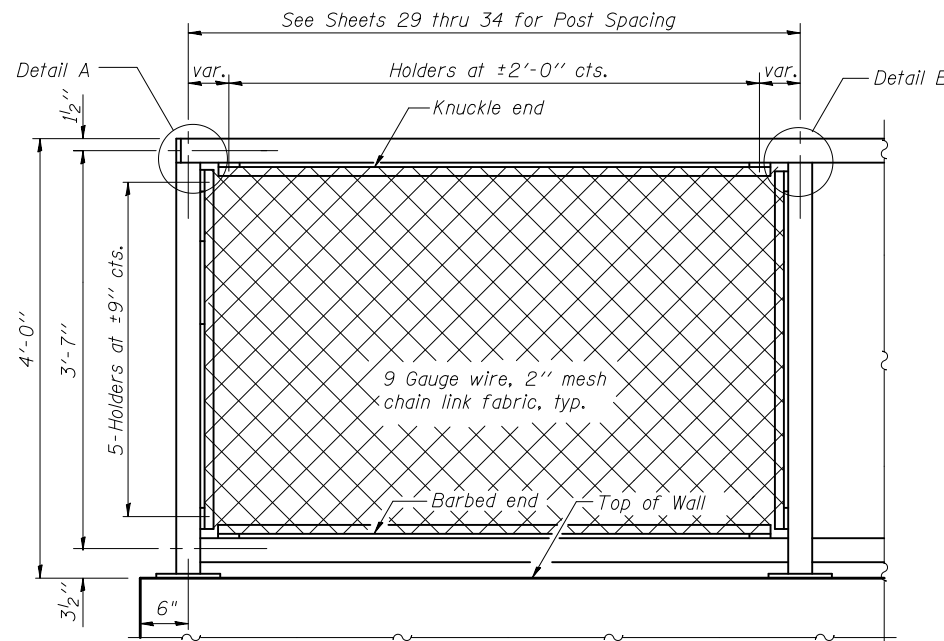
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

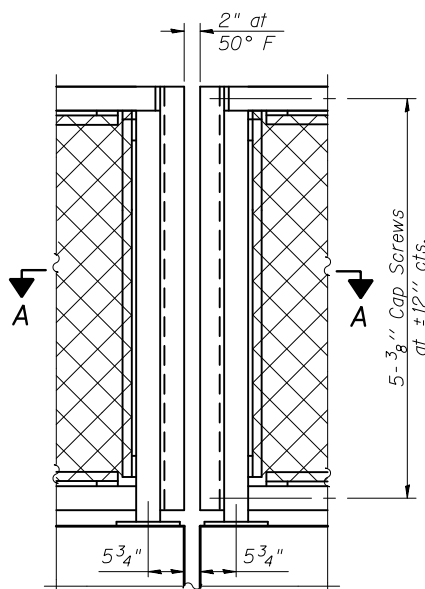
**RAILING DETAILS
CPR RETAINING WALLS**

SHEET NO. 34 OF 42 SHEETS

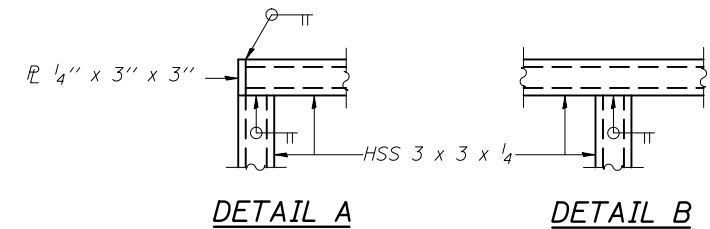
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345A	32VB		388	236
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				



CHAIN LINK FENCE

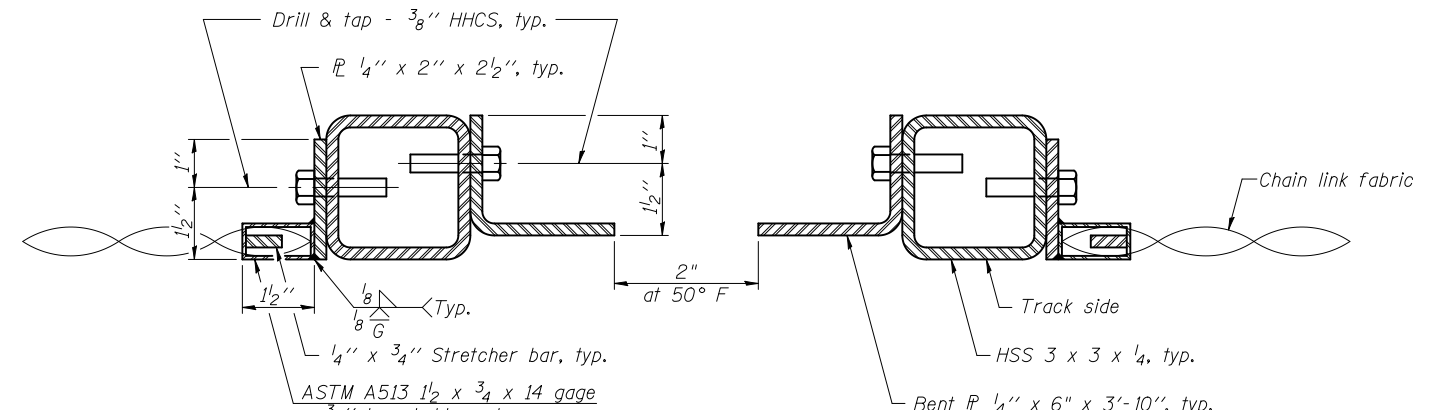


CHAIN LINK FENCE

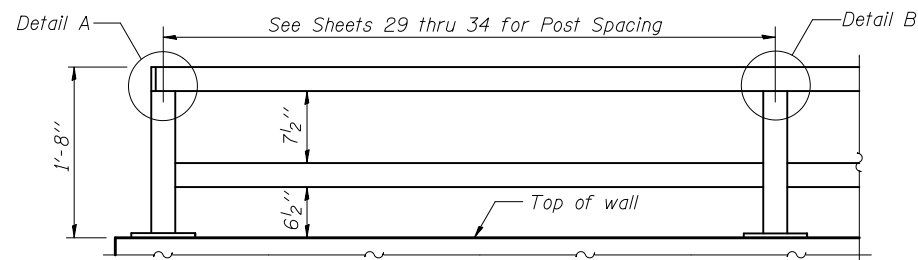


DETAIL A

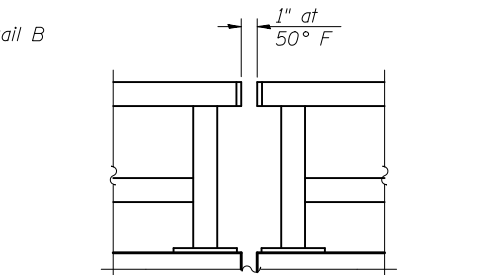
DETAIL B



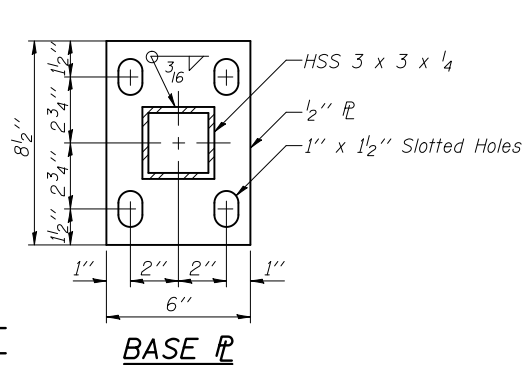
SECTION A-A



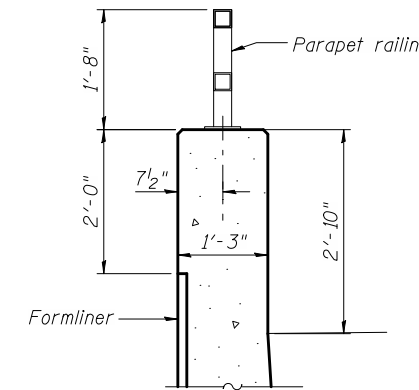
**PARAPET RAILING
ELEVATION
(Inside Face)**



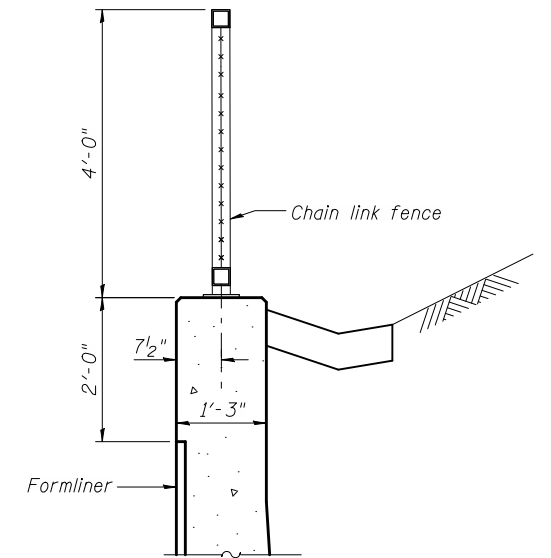
**PARAPET RAILING
ELEVATION AT EXPANSION JOINT**



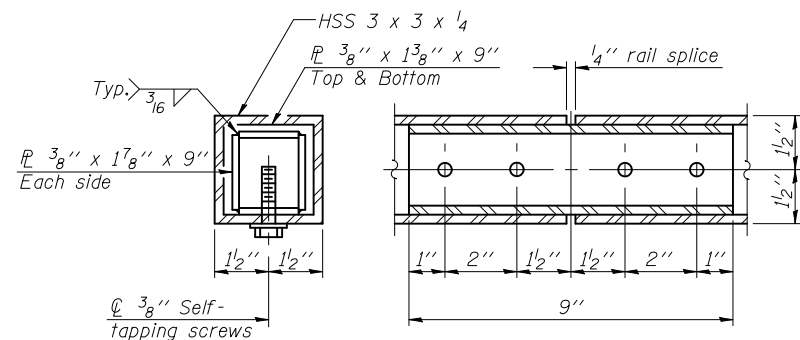
BASE PLATE



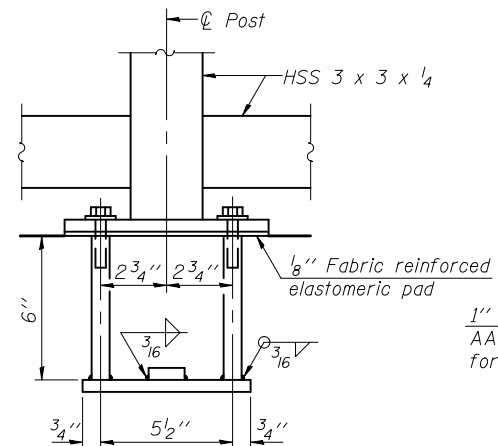
SECTION THRU WALL



SECTION THRU WALL

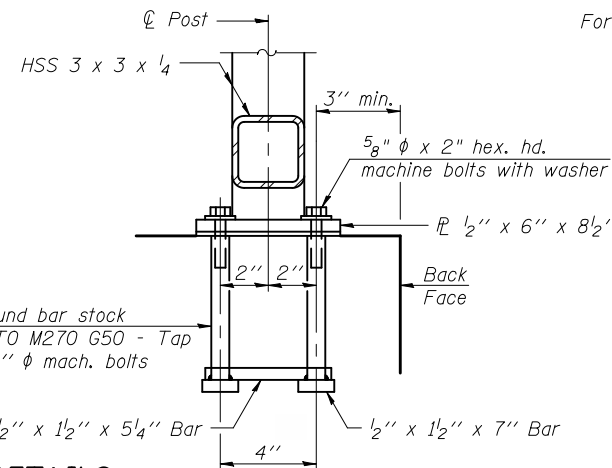


RAIL SPLICE



ANCHOR BOLT DETAILS

In lieu of the cast-in-place anchor device shown, the Contractor has the option of drilling and setting 5/8" diameter anchor rods according to Article 509.06 of the Standard Specifications. Embedment shall be according to the manufacturer's specifications.



NOTES:

- All steel rail elements shall be galvanized according to Article 509.05 of the Standard Specifications.

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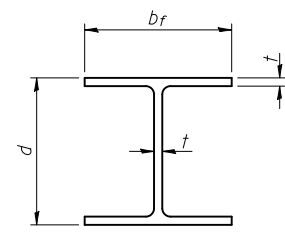
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**RAILING DETAILS
CPR RETAINING WALLS**

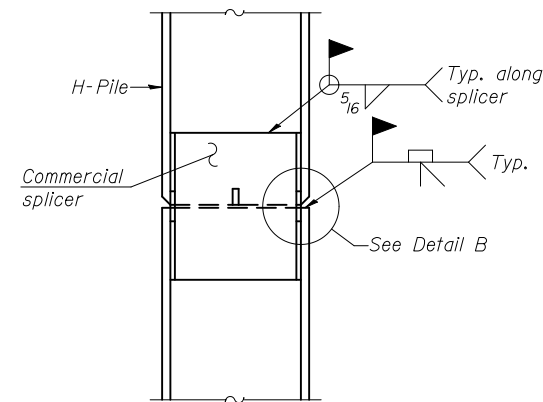
SHEET NO. 35 OF 42 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				

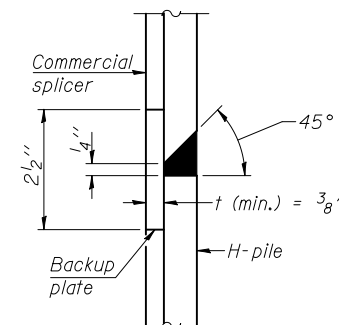


STEEL PILE TABLE

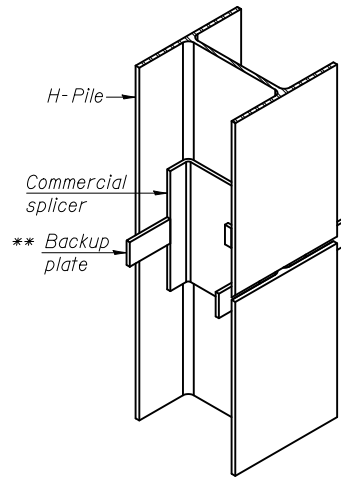
Designation	Depth d	Flange width br	Web and Flange thickness t	Encasement diameter A
HP 14x117	14 1/4"	14 7/8"	1 3/16"	30"
x102	14"	14 3/4"	1/16"	30"
x89	13 7/8"	14 3/4"	5/8"	30"
x73	13 5/8"	14 5/8"	1/2"	30"
HP 12x84	12 1/4"	12 1/4"	1/16"	24"
x74	12 1/8"	12 1/4"	5/8"	24"
x63	12"	12 1/8"	1/2"	24"
x53	11 3/4"	12"	7/16"	24"
HP 10x57	10"	10 1/4"	9/16"	24"
x42	9 3/4"	10 1/8"	7/16"	24"
HP 8x36	8"	8 1/8"	7/16"	18"



ELEVATION

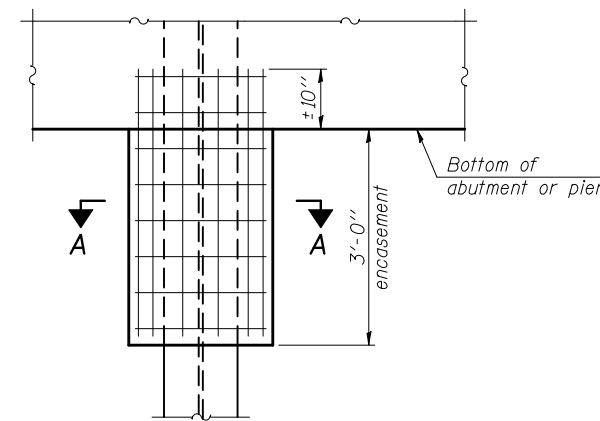


DETAIL "B"

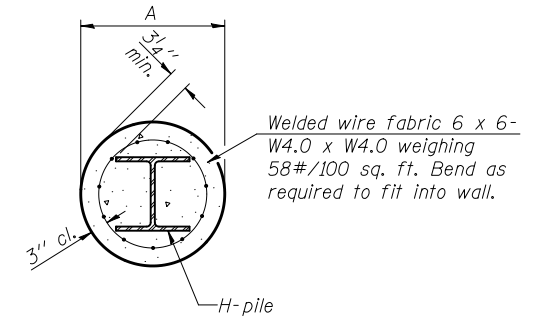


ISOMETRIC VIEW

WELDED COMMERCIAL SPLICE

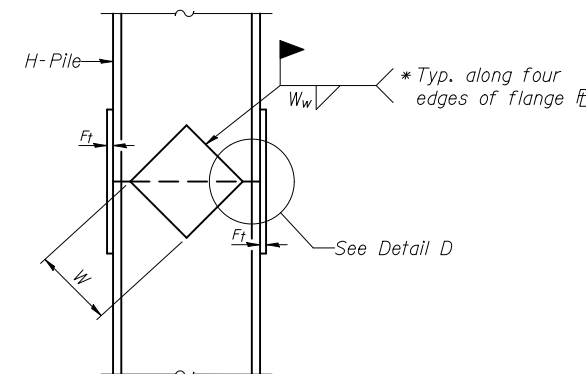


ELEVATION

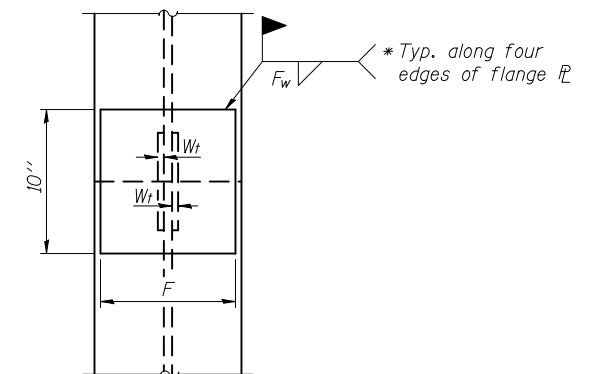


SECTION A-A

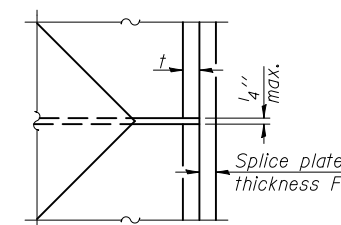
PILE ENCASEMENT



ELEVATION



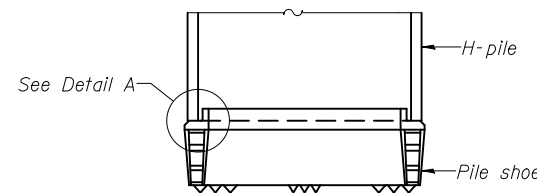
END VIEW



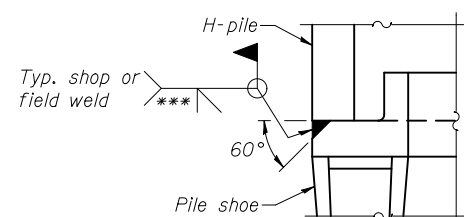
DETAIL D

WELDED PLATE FIELD SPLICE

Designation	F	Ft	Fw	W	Wt	Ww
HP 14x117	12 1/2"	1"	7/8"	7 3/4"	5/8"	1/2"
x102	12 1/2"	7/8"	3/4"	7 3/4"	5/8"	1/2"
x89	12 1/2"	3/4"	1/16"	7 3/4"	5/8"	1/2"
x73	12 1/2"	5/8"	9/16"	7 3/4"	5/8"	1/2"
HP 12x84	10"	7/8"	1/16"	6 1/2"	5/8"	1/2"
x74	10"	7/8"	1/16"	6 1/2"	5/8"	1/2"
x63	10"	5/8"	1/2"	6 1/2"	1/2"	3/8"
x53	10"	5/8"	1/2"	6 1/2"	1/2"	3/8"
HP 10x57	8"	3/4"	9/16"	5 1/4"	1/2"	3/8"
x42	8"	5/8"	9/16"	5 1/4"	1/2"	3/8"
HP 8x36	7"	5/8"	7/16"	4 1/4"	1/2"	3/8"

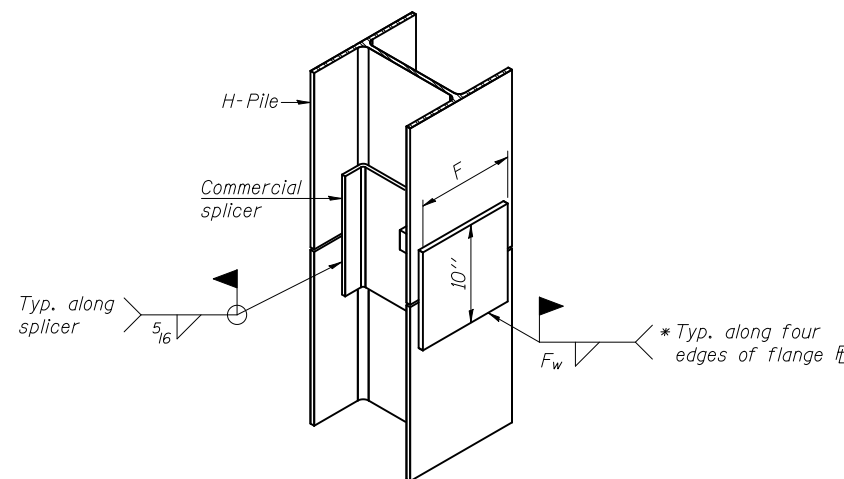


ELEVATION



DETAIL A

H-PILE SHOE ATTACHMENT



ISOMETRIC VIEW

WELDED COMMERCIAL SPLICE ALTERNATE

- * Interrupt welds 1/4" from end of web and/or each flange.
- ** Remove portions of backup plates that extend outside the flanges.
- *** Weld size per pile shoe manufacturer (5/16" min.).

Note:
The steel H-piles shall be according to AASHTO M270 Grade 50.

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jmiqus

11/2/2012
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F-HP 7-1-10



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CHECKED - JAR

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REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

HP PILE DETAILS
CPR RETAINING WALLS

SHEET NO. 36 OF 42 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345A	32VB	DU PAGE	388	238
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				

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<p>Geo Services, Inc. Geotechnical, Environmental & Civil Engineering 805 Armerist-Coplin Drive 204 Naperville, Illinois 60565 (630) 355-2838</p>		<p>PAGE 1 of 1</p> <p>DATE 5/4/2010</p> <p>LOGGED BY DR</p> <p>GSI JOB No. 09165</p>					
<p>ROUTE II RTE 19 DESCRIPTION Irving Park Rd., York Rd., CPR Grade Separation Pri D-91-332-06</p> <p>SECTION 32 WRS-5 LOCATION Addison Township T 40 N, R 11 E, NW 1/4 Section 13, 3rd P.M.</p> <p>COUNTY DuPage DRILLING METHOD Hollow Stem Auger/ Rotary HAMMER TYPE CME Automatic</p>							
<p>STRUCT. NO. 022-1002 Station 20300+34 to 20302+88</p> <p>BORING NO. WB-09 Station: 20300+46 Offset: 26.0' Left Ground Surface Elev. 666.9</p>		<p>Surface Water Elev. n/a Stream Bed Elev. n/a</p> <p>Groundwater Elevation: First Encounter n/a Upon Completion n/a After Hrs. n/a</p>					
DEPTH (ft)	BULGE (ft)	UCS (tsf)	MOIST (%)	DEPTH (ft)	BULGE (ft)	UCS (tsf)	MOIST (%)
665.9	AS	-	37	646.4			
3				5			
2				7			
2	NP	18		10	NP	17	
663.9				643.9			
2				5			111
3				10			
5			20	17	2.9B	19	
661.4				25			
4				6			
6				6			
7	2.75P	27		8			24
658.9				8			
2			92	4			117
3				6			
10	3	1.3B	28	8	2.7B	17	
653.9				30			
2							
1							
2	1.0P	26					
653.9							
4			109	3			
5				5			
15	9	2.2B	20	15	3	1.5B	15
651.4				35			22
5							
4							
6	3.5P	18					
648.9							
5							
9							
20	9	NP	22				
648.9							
5							
9							
20							

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) ST-Shelby Tube Sample VS-Vane Shear Test
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) The Unit Dry Weight (pcf) is noted in italics above moist (%)
NR-No Recovery

<p>Geo Services, Inc. Geotechnical, Environmental & Civil Engineering 805 Armerist-Coplin Drive 204 Naperville, Illinois 60565 (630) 355-2838</p>		<p>PAGE 1 of 1</p> <p>DATE 4/26/2010</p> <p>LOGGED BY DR</p> <p>GSI JOB No. 09165</p>					
<p>ROUTE II RTE 19 DESCRIPTION Irving Park Rd., York Rd., CPR Grade Separation Pri D-91-332-06</p> <p>SECTION 32 WRS-5 LOCATION Addison Township T 40 N, R 11 E, NW 1/4 Section 13, 3rd P.M.</p> <p>COUNTY DuPage DRILLING METHOD Hollow Stem Auger/ Rotary HAMMER TYPE CME Automatic</p>							
<p>STRUCT. NO. 022-1001 Station 10300+34 to 10302+88</p> <p>BORING NO. WB-10 Station: 20300+99 Offset: 56.5' Right Ground Surface Elev. 661.9</p>		<p>Surface Water Elev. n/a Stream Bed Elev. n/a</p> <p>Groundwater Elevation: First Encounter 655.9 Upon Completion n/a After Hrs. n/a</p>					
DEPTH (ft)	BULGE (ft)	UCS (tsf)	MOIST (%)	DEPTH (ft)	BULGE (ft)	UCS (tsf)	MOIST (%)
661.0				661.0			
5			109	4			110
4				5			
4	2.7B	14		7	2.0B	16	
3				4			110
5				4			
5	2.5P	18		25	4	1.6B	17
655.9				25			
2				3			110
2				3			
2	NP	19		4	1.5B	18	
653.9				4			
3			114	3			108
3				5			
10	3	0.4B	13	10	8	2.4B	19
651.4				30			
3			111				
3							
4	1.7B	15					
651.4							
2			110	2			109
2				3			
15	3	1.5B	15	15	5	1.4B	18
651.4				35			
3			108				
3							
6	1.7B	17					
624.9							
3			113				
3							
20	3	1.7B	16				
621.9							
3			108				
3							
9			NP	20			

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) ST-Shelby Tube Sample VS-Vane Shear Test
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) The Unit Dry Weight (pcf) is noted in italics above moist (%)
NR-No Recovery

<p>Geo Services, Inc. Geotechnical, Environmental & Civil Engineering 805 Armerist-Coplin Drive 204 Naperville, Illinois 60565 (630) 355-2838</p>		<p>PAGE 1 of 1</p> <p>DATE 4/29/2010</p> <p>LOGGED BY DR</p> <p>GSI JOB No. 09165</p>					
<p>ROUTE II RTE 19 DESCRIPTION Irving Park Rd., York Rd., CPR Grade Separation Pri D-91-332-06</p> <p>SECTION 32 WRS-5 LOCATION Addison Township T 40 N, R 11 E, NW 1/4 Section 13, 3rd P.M.</p> <p>COUNTY DuPage DRILLING METHOD Hollow Stem Auger/ Rotary HAMMER TYPE CME Automatic</p>							
<p>STRUCT. NO. 022-1002 Station 20300+34 to 20302+88</p> <p>BORING NO. WB-11 Station: 20302+25 Offset: 50.5' Left Ground Surface Elev. 667.0</p>		<p>Surface Water Elev. n/a Stream Bed Elev. n/a</p> <p>Groundwater Elevation: First Encounter n/a Upon Completion n/a After Hrs. n/a</p>					
DEPTH (ft)	BULGE (ft)	UCS (tsf)	MOIST (%)	DEPTH (ft)	BULGE (ft)	UCS (tsf)	MOIST (%)
667.0				667.0			
AS	-		12	AS	-		12
4				4			
5				3			110
6	NP	10		3			
664.0				4			107
4				4			
4				3			107
5				4			
5	2.5P	11		5	1.6B	19	
661.4				25			
4				3			107
4				4			
6	NP	16		5	1.4B	18	
659.0				5			
3				3			106
4				4			
10	5	NP	15	10	4	2.0B	21
656.5				30			
3			108				
4							
5	1.7B	20					
654.0							
2			112				
4							
15	6	2.6B	18				
654.0							
3							
4							
6	1.5P	21					
630.0							
3							
2			110				
3							
4	1.3B	20					
627.0							
4			NP	20			

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) ST-Shelby Tube Sample VS-Vane Shear Test
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) The Unit Dry Weight (pcf) is noted in italics above moist (%)
NR-No Recovery



USER NAME = jmlgus
FILE NAME = WXXX-60B42-037-SBL.DGN
PLOT SCALE = NONE
PLOT DATE = 11/2/2012

DESIGNED - BWC
CHECKED - JAR
DRAWN - JM
CHECKED - JAR

REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOIL BORING LOGS
CPR RETAINING WALLS

SHEET NO. 37 OF 42 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345A	32VB	DU PAGE	388	239
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				

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jmlgus
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Geotechnical, Environmental & Civil Engineering
805 Antwerp - Ogilvie Suite 204
Naperville, Illinois 60565
(330) 355-2838

SOIL BORING LOG

PAGE 1 of 1
DATE 5/5/2010
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GSI JOB No. 09165

ROUTE II RTE 19 DESCRIPTION Irving Park Rd., York Rd., CPR Grade Separation Pri. D-91-332-06
SECTION 32 WRS-5 LOCATION Addison Township T 40 N, R 11 E, NW 1/4 Section 13, 3rd P.M.
COUNTY DuPage DRILLING METHOD Hollow Stem Auger/ Rotary HAMMER TYPE CME Automatic

STRUCT. NO. 022-1005
Station 20303+84 to 20305+43
BORING NO. **WB-12**
Station: 20304+66
Offset: 98.0' Left
Ground Surface Elev. 660.5

DEPTH (ft)	BLU (ft)	UCS (tsf)	MOIST (%)	Surface Water Elev. (ft)	Stream Bed Elev. (ft)	Groundwater Elevation: First Encounter Upon Completion After Hrs.	DEPTH (ft)	BLU (ft)	UCS (tsf)	MOIST (%)
2.0				n/a	n/a					
2.0' TOPSOIL-black										
	AS	-	17							
	5						5			112
	6						5			
	5	4.5P	18				9	2.3B		19
CLAY-brown & gray-hard (A-6) Fill										
	7						4			108
	7						5			
	7	4.5P	21				-25	9	2.75B	19
655.0										
	2		89				4			100
CLAY-gray-stiff to very stiff (A-6)										
	3						6			
	3	1.5P	29				7	2.0B		21
SILTY CLAY-brown & gray-stiff to very stiff (A-6) Wet										
	3						6			113
	3						8			
	-10	3	1.5P	33			-10	10	6.7B	17
	1		96				6			109
	1						9			
	2	1.0P	28				11	6.0B		18
647.5										
	2						5			107
Clayey SAND & GRAVEL-brown & gray-loose (A-2)										
	4						5			
	-15	5	NP	14			-15	6	2.5B	20
645.0										
	3		106				4			108
	4						5			
	4	1.0B	21				7	2.2B		20
CLAY-gray-stiff to very stiff (A-6)										
	3		115				4			109
	4						5			
	5	3.3B	17				7			102
620.5 -40										

End Of Boring @ -40.0'
Hollow Stem Augers To -10.0'
Rotary Drilling To Completion
CME Automatic Hammer
10.0' Of 4.0" Casing Used

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) ST-Shelby Tube Sample VS-Vane Shear Test
The SP1 (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) The Unit Dry Weight (pcf) is noted in italics above moist (%)
NR-No Recovery

Geo Services, Inc.
Geotechnical, Environmental & Civil Engineering
805 Antwerp - Ogilvie Suite 204
Naperville, Illinois 60565
(330) 355-2838

SOIL BORING LOG

PAGE 1 of 1
DATE 6/2/2011
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GSI JOB No. 09165

ROUTE II RTE 19 DESCRIPTION Irving Park Rd., York Rd., CPR Grade Separation Pri. D-91-332-06
SECTION 32 WRS-5 LOCATION Addison Township T 40 N, R 11 E, NW 1/4 Section 13, 3rd P.M.
COUNTY DuPage DRILLING METHOD Hollow Stem Auger HAMMER TYPE CME Automatic

STRUCT. NO. 022-1006
Station 20294+86 to 20296+15
BORING NO. **WB-13**
Station: 20295+12
Offset: 69.0' Left
Ground Surface Elev. 666.9

DEPTH (ft)	BLU (ft)	UCS (tsf)	MOIST (%)	Surface Water Elev. (ft)	Stream Bed Elev. (ft)	Groundwater Elevation: First Encounter Upon Completion After Hrs.	DEPTH (ft)	BLU (ft)	UCS (tsf)	MOIST (%)
12.0' SAND & GRAVEL-dark brown (Fill)										
	AS	-	6							
	2						2			
Clayey TOPSOIL-dark brown to black										
	3						5	3.0P		28
							3			101
	5						5			
	-8	5	2.25S	24			-8	5	2.25S	24
663.9										
	3		108				3			108
CLAY-brown & gray-very stiff to hard (A-6)										
	5						5			
	7	4.9S	19				5			113
	5						8			
	-10	10	6.7B	17			-10	10	6.7B	17
	6		109				6			109
	9						11	6.0B		18
653.9										
	5		107				5			107
CLAY-gray-very stiff (A-6)										
	5						5			
	-15	6	2.5B	20			-15	6	2.5B	20
	4		108				4			108
	5						5			
	7	2.2B	20				7	2.2B		20
End Of Boring @ -20.0' Hollow Stem Augers CME Automatic Hammer										
	4		102				4			102
	7						7			
	10	2.3B	23				10	2.3B		23
646.9 -20										

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) ST-Shelby Tube Sample VS-Vane Shear Test
The SP1 (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) The Unit Dry Weight (pcf) is noted in italics above moist (%)
NR-No Recovery

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Naperville, Illinois 60565
(330) 355-2838

SOIL BORING LOG

PAGE 1 of 1
DATE 6/2/2011
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ROUTE II RTE 19 DESCRIPTION Irving Park Rd., York Rd., CPR Grade Separation Pri. D-91-332-06
SECTION 32 WRS-5 LOCATION Addison Township T 40 N, R 11 E, NW 1/4 Section 13, 3rd P.M.
COUNTY DuPage DRILLING METHOD Hollow Stem Auger HAMMER TYPE CME Automatic

STRUCT. NO. 022-1006
Station 20294+86 to 20296+15
BORING NO. **WB-14**
Station: 20295+87
Offset: 69.0' Left
Ground Surface Elev. 665.2

DEPTH (ft)	BLU (ft)	UCS (tsf)	MOIST (%)	Surface Water Elev. (ft)	Stream Bed Elev. (ft)	Groundwater Elevation: First Encounter Upon Completion After Hrs.	DEPTH (ft)	BLU (ft)	UCS (tsf)	MOIST (%)
12.0' Clayey SAND & GRAVEL-brown & black (Fill)										
	AS	-	19							
	2						2			
	3	1.25P	27				3			
CLAY-brown & gray-stiff to very stiff (A-6)										
	3						3			
	4						4			109
	6						6			
	8	3.7B	20				8	3.7B		20
	5		106				5			106
	9						9			
	-10	13	3.9B	21			-10	13	3.9B	21
	6		106				6			106
	9						9			
	12	2.9B	21				12	2.9B		21
652.7										
	4		108				4			108
CLAY-gray-stiff to very stiff (A-6)										
	7						7			
	-15	10	3.0B	19			-15	10	3.0B	19
	4		111				4			111
	6						6			
	8	2.4B	19				8	2.4B		19
End Of Boring @ -20.0' Hollow Stem Augers CME Automatic Hammer										
	4		107				4			107
	6						6			
	9	1.6B	21				9	1.6B		21
645.7 -20										

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) ST-Shelby Tube Sample VS-Vane Shear Test
The SP1 (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) The Unit Dry Weight (pcf) is noted in italics above moist (%)
NR-No Recovery



USER NAME = jmlgus	DESIGNED - BWC	REVISED -
FILE NAME = WXXX-60B42-038-SBL.DGN	CHECKED - JAR	REVISED -
PLOT SCALE = NONE	DRAWN - JM	REVISED -
PLOT DATE = 11/2/2012	CHECKED - JAR	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOIL BORING LOGS
CPR RETAINING WALLS

SHEET NO. 38 OF 42 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345A	32VB	DU PAGE	388	240
CONTRACT NO. 60W01				

ILLINOIS FED. AID PROJECT

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SOIL BORING LOG

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805 Antwerp - Ogilvie Suite 204
Naperville, Illinois 60565
(330) 355-2833

PAGE 1 of 1
DATE 5/11/2011
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ROUTE II RTE 19 DESCRIPTION Irving Park Rd., York Rd., CPR Grade Separation Pri. D-91-332-06
SECTION 32 WRS-5 LOCATION Addison Township T 40 N, R 11 E, NW 1/4 Section 13, 3rd P.M.
COUNTY DuPage DRILLING METHOD Hollow Stem Auger HAMMER TYPE CME Automatic

STRUCT. NO. 022-1002
Station 20300+34 to 20302+88
BORING NO. **WB-21**
Station: 20301+65
Offset: 29.0' Left
Ground Surface Elev. 667.2

DEPTH (ft)	BULGE (ft)	UCS (tsf)	MOIST (%)	Surface Water Elev. (ft)	Stream Bed Elev. (ft)	Groundwater Elevation (ft)	First Encounter Upon Completion (Hrs)	DEPTH (ft)	BULGE (ft)	UCS (tsf)	MOIST (%)
AS	-	24		n/a	n/a						
2											
4		27									
1											
5				661.7							
7	2.9B	25									
2		93									
2	1.25										
10	12.7%	27		656.7							
2		109									
6	6.5B	18		654.2							
4		110									
6	3.9B	18									
15											
3		107									
5											
7	4.0B	20									
5		105									
5											
20	1.1B	21		647.2							

End Of Boring @ -20.0'
Hollow Stem Augers
CME Automatic Hammer 647.2

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) ST-Shelby Tube Sample VS-Vane Shear Test
The SP (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) The Unit Dry Weight (pcf) is noted in italics above moist (%)
NR-No Recovery

SOIL BORING LOG

Geo Services, Inc. Geotechnical, Environmental & Civil Engineering
805 Antwerp - Ogilvie Suite 204
Naperville, Illinois 60565
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PAGE 1 of 1
DATE 5/11/2011
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ROUTE II RTE 19 DESCRIPTION Irving Park Rd., York Rd., CPR Grade Separation Pri. D-91-332-06
SECTION 32 WRS-5 LOCATION Addison Township T 40 N, R 11 E, NW 1/4 Section 13, 3rd P.M.
COUNTY DuPage DRILLING METHOD Hollow Stem Auger HAMMER TYPE CME Automatic

STRUCT. NO. 022-1001
Station 10300+34 to 10302+88
BORING NO. **WB-22**
Station: 20301+62
Offset: 52.5' Right
Ground Surface Elev. 663.0

DEPTH (ft)	BULGE (ft)	UCS (tsf)	MOIST (%)	Surface Water Elev. (ft)	Stream Bed Elev. (ft)	Groundwater Elevation (ft)	First Encounter Upon Completion (Hrs)	DEPTH (ft)	BULGE (ft)	UCS (tsf)	MOIST (%)
AS	-	22		n/a	n/a						
3											
4	2.5P	20									
5											
5											
8		16		-25							
2											
3											
3		17									
2											
2											
10	0.25P	20		-30							
1		110									
2		18									
4	2.5B	18									
3		109									
3											
15	3.2B	20		-35							
4		110									
6											
8	2.75B	19									
3		111									
5											
20	2.8B	18		643.0							

End Of Boring @ -20.0'
Hollow Stem Augers
CME Automatic Hammer 643.0

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) ST-Shelby Tube Sample VS-Vane Shear Test
The SP (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) The Unit Dry Weight (pcf) is noted in italics above moist (%)
NR-No Recovery

SOIL BORING LOG

Geo Services, Inc. Geotechnical, Environmental & Civil Engineering
805 Antwerp - Ogilvie Suite 204
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PAGE 1 of 1
DATE 5/11/2011
LOGGED BY RJ
GSI JOB No. 09165

ROUTE II RTE 19 DESCRIPTION Irving Park Rd., York Rd., CPR Grade Separation Pri. D-91-332-06
SECTION 32 WRS-5 LOCATION Addison Township T 40 N, R 11 E, NW 1/4 Section 13, 3rd P.M.
COUNTY DuPage DRILLING METHOD Hollow Stem Auger HAMMER TYPE CME Automatic

STRUCT. NO. 022-1001
Station 10300+34 to 10302+88
BORING NO. **WB-23**
Station: 20302+25
Offset: 52.5' Right
Ground Surface Elev. 663.2

DEPTH (ft)	BULGE (ft)	UCS (tsf)	MOIST (%)	Surface Water Elev. (ft)	Stream Bed Elev. (ft)	Groundwater Elevation (ft)	First Encounter Upon Completion (Hrs)	DEPTH (ft)	BULGE (ft)	UCS (tsf)	MOIST (%)
AS	-	23		n/a	n/a						
2											
3											
3	3.0P	22									
5											
5											
8		11		-25							
2											
6											
6		15									
2											
4											
10		16		-30							
2											
4											
4		13									
4											
15		14		-35							
5		123									
5											
7	1.2B	11									
5		120									
7	1.75										
11	12.7%	14		648.2							

End Of Boring @ -20.0'
Hollow Stem Augers
CME Automatic Hammer 643.2

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) ST-Shelby Tube Sample VS-Vane Shear Test
The SP (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) The Unit Dry Weight (pcf) is noted in italics above moist (%)
NR-No Recovery



USER NAME = jmgus	DESIGNED - BWC	REVISED -
FILE NAME = WXXX-60B42-041-SBL.DGN	CHECKED - JAR	REVISED -
PLOT SCALE = NONE	DRAWN - JM	REVISED -
PLOT DATE = 11/2/2012	CHECKED - JAR	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SOIL BORING LOGS
CPR RETAINING WALLS**

SHEET NO. 41 OF 42 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345A	32VB	DU PAGE	388	243
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				

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 jmlgus

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 (630) 355-2838

SOIL BORING LOG

PAGE 1 of 1
 DATE 5/31/2011
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 GSI JOB No. 09165

ROUTE II RTE 19 DESCRIPTION Irving Park Rd., York Rd., CPR Grade Separation Pri. D-91-332-06
 SECTION 32 WRS-5 LOCATION Addison Township T 40 N. R 11 E, NW 1/4 Section 13, 3rd P.M.
 COUNTY DuPage DRILLING METHOD Hollow Stem Auger HAMMER TYPE CME Automatic

STRUCT. NO. 022-1005
 Station 20303+84 to 20305+43
 BORING NO. **WB-24**
 Station: 20303+81
 Offset: 100.5' Left
 Ground Surface Elev. 661.8

DEPTH (ft)	BLU (ft)	UCS (tsf)	MOIST (%)	Surface Water Elev. (ft)	Stream Bed Elev. (ft)	DEPTH (ft)	BLU (ft)	UCS (tsf)	MOIST (%)
12.0"	AS	-	38	n/a	n/a				
	2		97						
CLAY LOAM-brown, gray & black-very stiff (A-6) Fill, Wet	4								
	5	2.2B	26						
658.8									
Clayey TOPSOIL-dark gray to black	3		84						
	3								
	4	1.6B	32						
656.3									
SILTY CLAY-brown & gray-stiff (A-6) Wet	2		88						
	2								
	3	1.0S	34						
	1		93						
	2								
	10	1.0S	28						
650.8									
CLAY-brown & gray-stiff (A-6)	2		113						
	4								
	5	1.7B	17						
648.8									
CLAY-gray-stiff to very stiff (A-6)	3		109						
	5								
	15	1.8B	19						
645.8									
CLAY-gray-stiff to very stiff (A-6)	3		107						
	5								
	6	2.2B	20						
641.8									
End Of Boring @ -20.0' Hollow Stem Augers CME Automatic Hammer	3		106						
	5								
	7	1.4B	20						
641.3									

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B)-Bulge, (S)-Shear, (P)-Penetrometer, (ST)-Shelby Tube Sample, (VS)-Vane Shear Test. The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206). The Unit Dry Weight (pcf) is noted in italics above moist (%). NR-No Recovery.

Geo Services, Inc.
 Geotechnical, Environmental & Civil Engineering
 805 Armet-Copel-Sigle 204
 Naperville, Illinois 60565
 (630) 355-2838

SOIL BORING LOG

PAGE 1 of 1
 DATE 5/31/2011
 LOGGED BY MD
 GSI JOB No. 09165

ROUTE II RTE 19 DESCRIPTION Irving Park Rd., York Rd., CPR Grade Separation Pri. D-91-332-06
 SECTION 32 WRS-5 LOCATION Addison Township T 40 N. R 11 E, NW 1/4 Section 13, 3rd P.M.
 COUNTY DuPage DRILLING METHOD Hollow Stem Auger HAMMER TYPE CME Automatic

STRUCT. NO. 022-005
 Station 20303+84 to 20305+43
 BORING NO. **WB-25**
 Station: 20305+39
 Offset: 95.5' Left
 Ground Surface Elev. 661.3

DEPTH (ft)	BLU (ft)	UCS (tsf)	MOIST (%)	Surface Water Elev. (ft)	Stream Bed Elev. (ft)	DEPTH (ft)	BLU (ft)	UCS (tsf)	MOIST (%)
12.0"	AS	-	25	n/a	n/a				
	2		103						
CLAY LOAM-brown-hard (A-6) Fill	4								
	5	4.1B	19						
658.3									
Clayey TOPSOIL-black	2		85						
	3								
	4	1.6B	31						
655.3									
SILTY CLAY-brown & gray-medium stiff to stiff (A-6) Wet	1		90						
	1								
	2	1.2S	32						
	1		89						
	1								
	10	0.75B	30						
650.8									
CLAY-brown & gray-stiff (A-6)	2		105						
	2								
	4	1.4S	20						
648.3									
SILTY CLAY LOAM-brown & gray-medium dense (A-4)	4								
	4								
	15		21						
645.8									
CLAY-gray-stiff to very stiff (A-6)	4		103						
	6								
	6	2.6B	19						
641.3									
End Of Boring @ -20.0' Hollow Stem Augers CME Automatic Hammer	4		102						
	5								
	7	1.4B	20						
641.3									

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B)-Bulge, (S)-Shear, (P)-Penetrometer, (ST)-Shelby Tube Sample, (VS)-Vane Shear Test. The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206). The Unit Dry Weight (pcf) is noted in italics above moist (%). NR-No Recovery.



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FILE NAME = WXXX-60B42-042-SBL.DGN	CHECKED - JAR	REVISED -
PLOT SCALE = NONE	DRAWN - JM	REVISED -
PLOT DATE = 11/2/2012	CHECKED - JAR	REVISED -

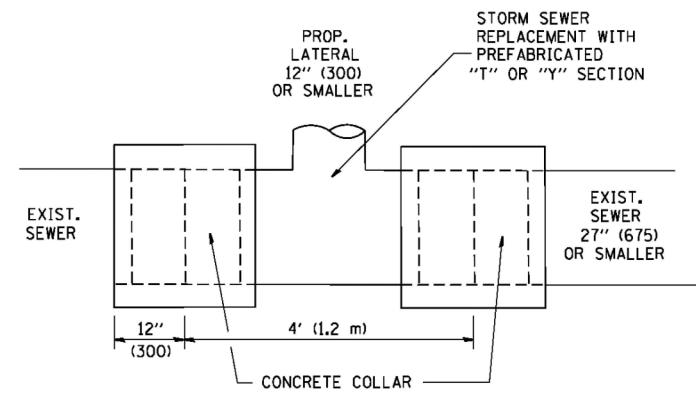
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**SOIL BORING LOGS
 CPR RETAINING WALLS**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345A	32VB	DU PAGE	388	244
CONTRACT NO. 60W01				

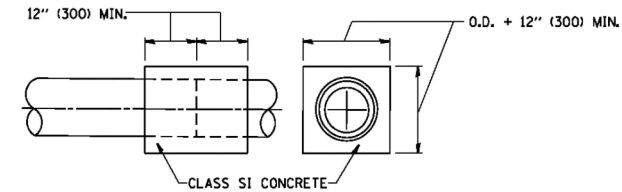
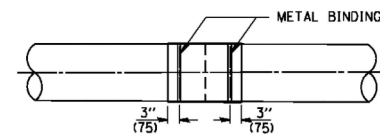
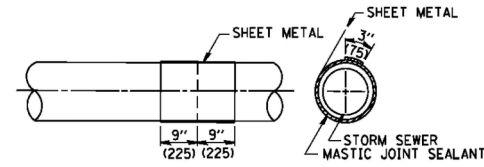
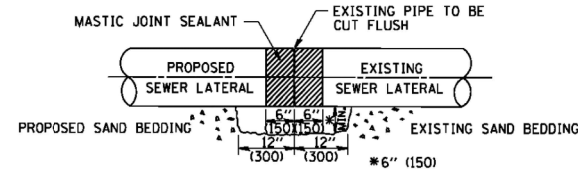
SHEET NO. 42 OF 42 SHEETS

ILLINOIS FED. AID PROJECT



DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER

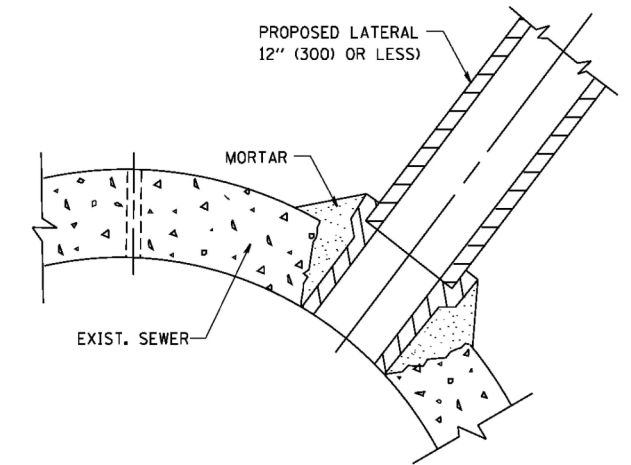


DETAIL "B"

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

1. CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
2. APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
3. BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12" x 6" (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
4. CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
5. WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
6. LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
7. PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
8. WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OZZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
9. PLACE CLASS SI CONCRETE AROUND THE JOINT.



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- I. THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- II. CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - A) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - B) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS, THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

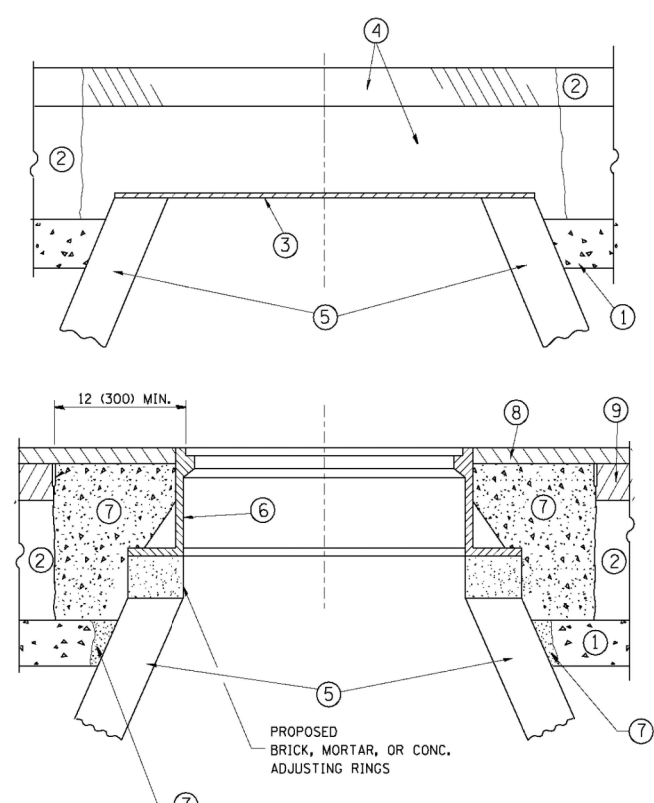
REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\data\22x34\bd07.dgn	USER NAME = gogliemobt	DESIGNED - M. DE YONG	REVISED - M. DE YONG 05-08-92	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - R. SHAH 09-09-94		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	BD500-01 (BD-7)		CONTRACT NO.	
		PLOT SCALE = 50,000' / IN.	REVISED - R. SHAH 10-25-94									
		PLOT DATE = 1/4/2008	REVISED - R. SHAH 06-12-96									
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT												



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL" NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

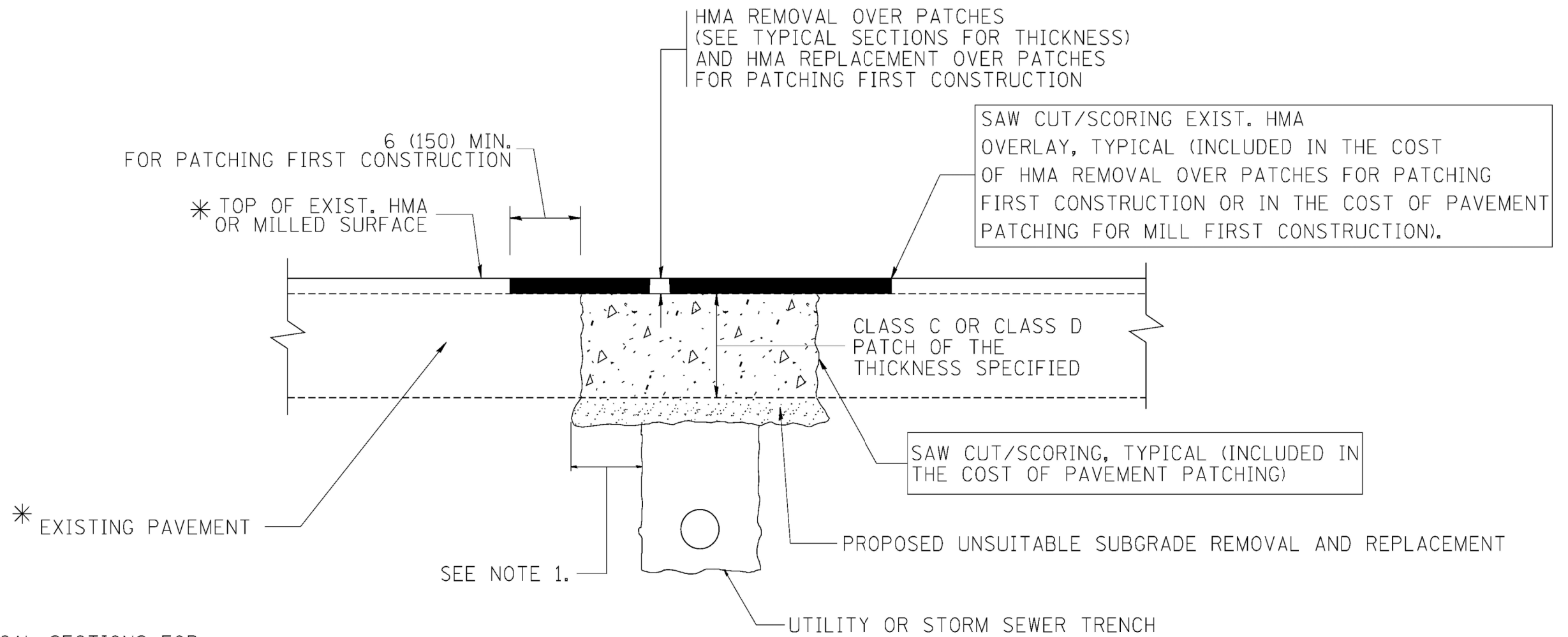
THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME = c:\pwork\pwork\1eysa\d0128315\bd08.dwg	USER NAME = 1eysa	DESIGNED - R. SHAH	REVISED - A. ABBAS 03-21-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING	F.A. RTE.:	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - R. WIEDEMAN 05-14-04						388	246
	PLOT SCALE = 49.9999' / 1IN.	CHECKED -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS		STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	
	PLOT DATE = 3/16/2011	DATE - 10-25-94	REVISED - R. BORO 03-09-11			BD600-03 (BD-8)		CONTRACT NO.		



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

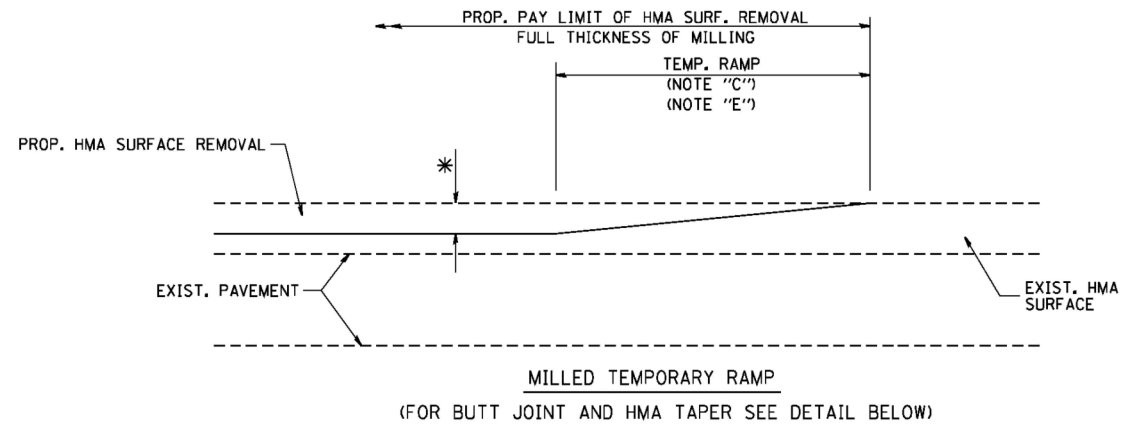
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

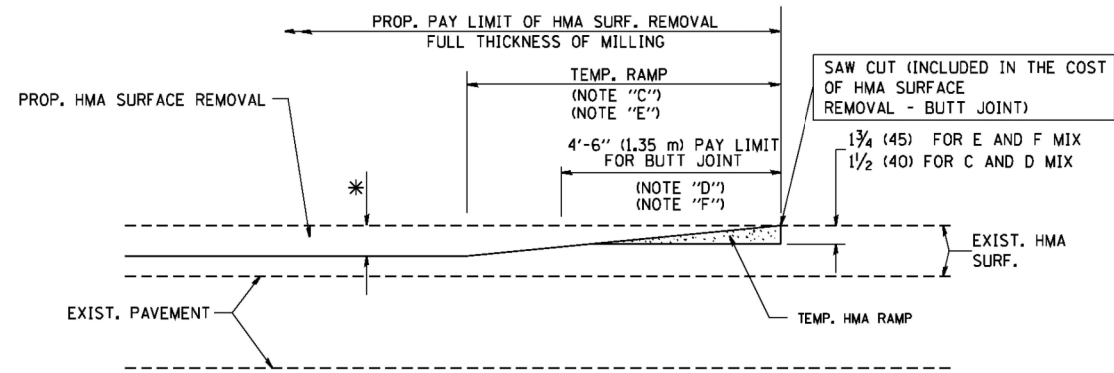
1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = m:\projects\distate\22x34\bd22.dgn	USER NAME = baerd1	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1	OF 1	SHEETS	STA.	TO STA.	388	247
		PLOT SCALE = 50,000' / IN.	CHECKED -		REVISED - R. BORO 09-04-07	BD400-04 (BD-22)			CONTRACT NO.			
		PLOT DATE = 10/27/2008	DATE - 10-25-94		REVISED - K. ENG 10-27-08	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						

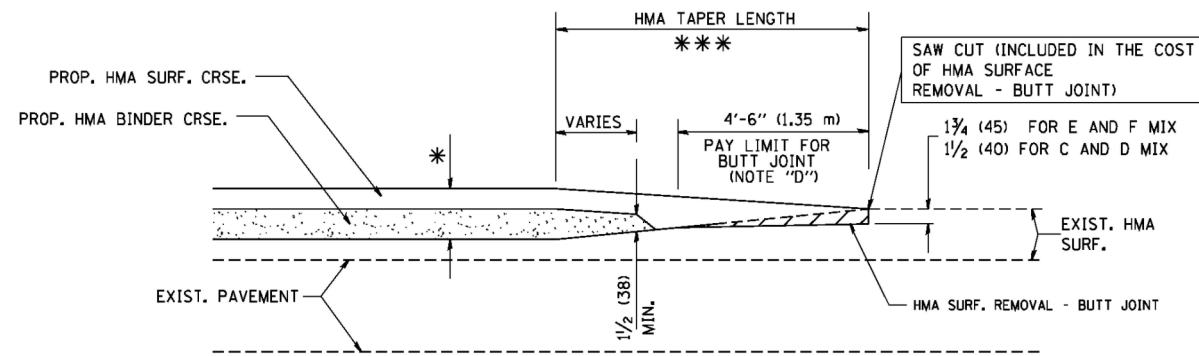


OPTION 1

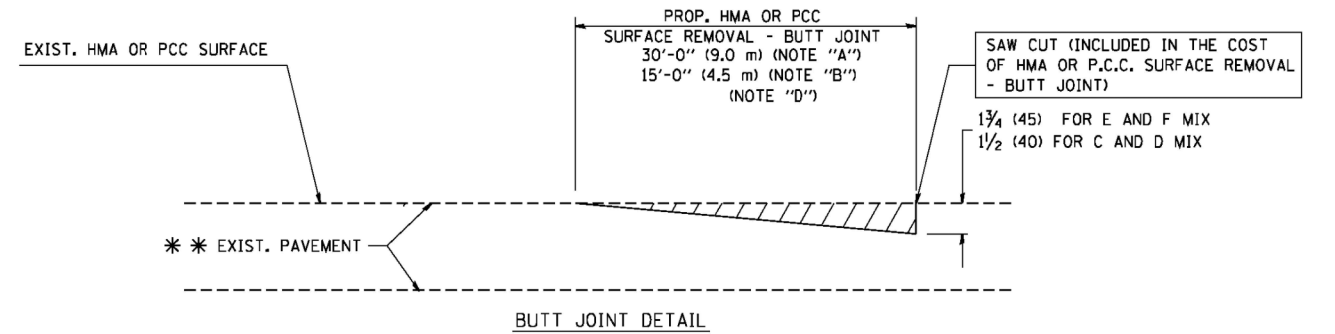


OPTION 2

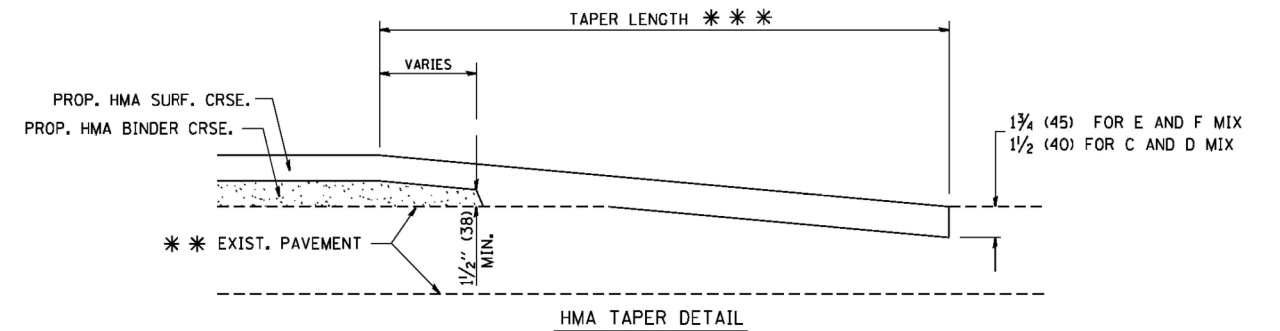
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- * * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

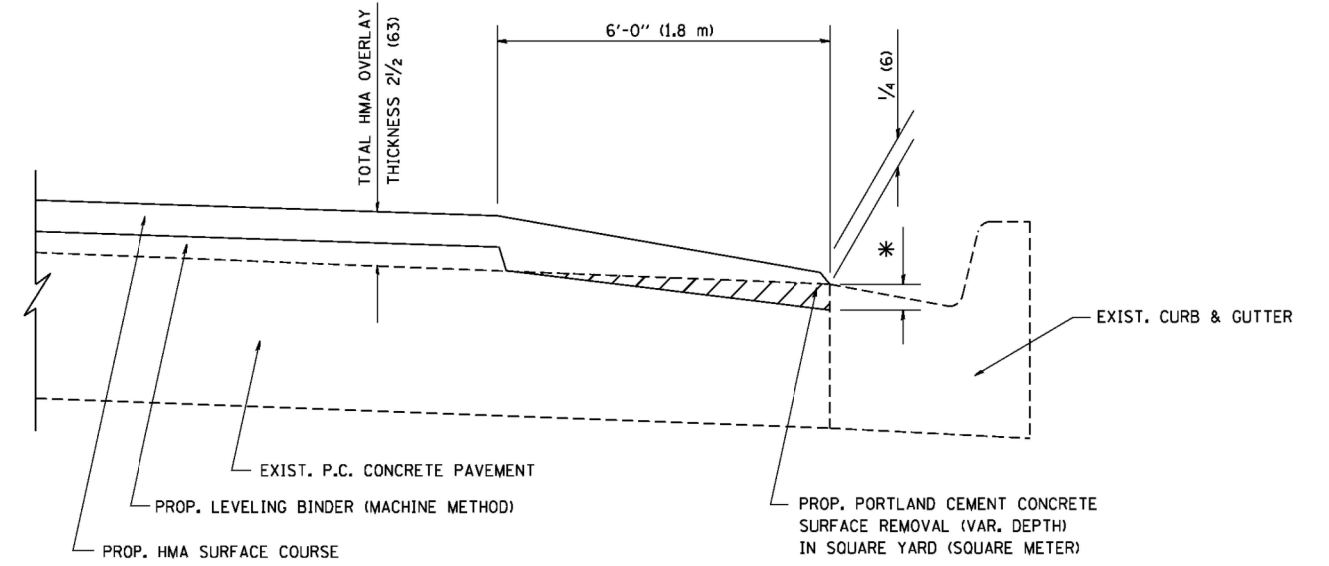
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		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50,0000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			388	248
BD400-05 BD32		CONTRACT NO.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**HMA TAPER AT
EDGE OF P.C.C. PAVEMENT**

HMA SURFACE	LEVELING BINDER	* MILLING AT GUTTER FLAG
MIX	THICKNESS	THICKNESS
C OR D	1 1/2 (38)	1 (25)
F	1 3/4 (44)	3/4 (19)

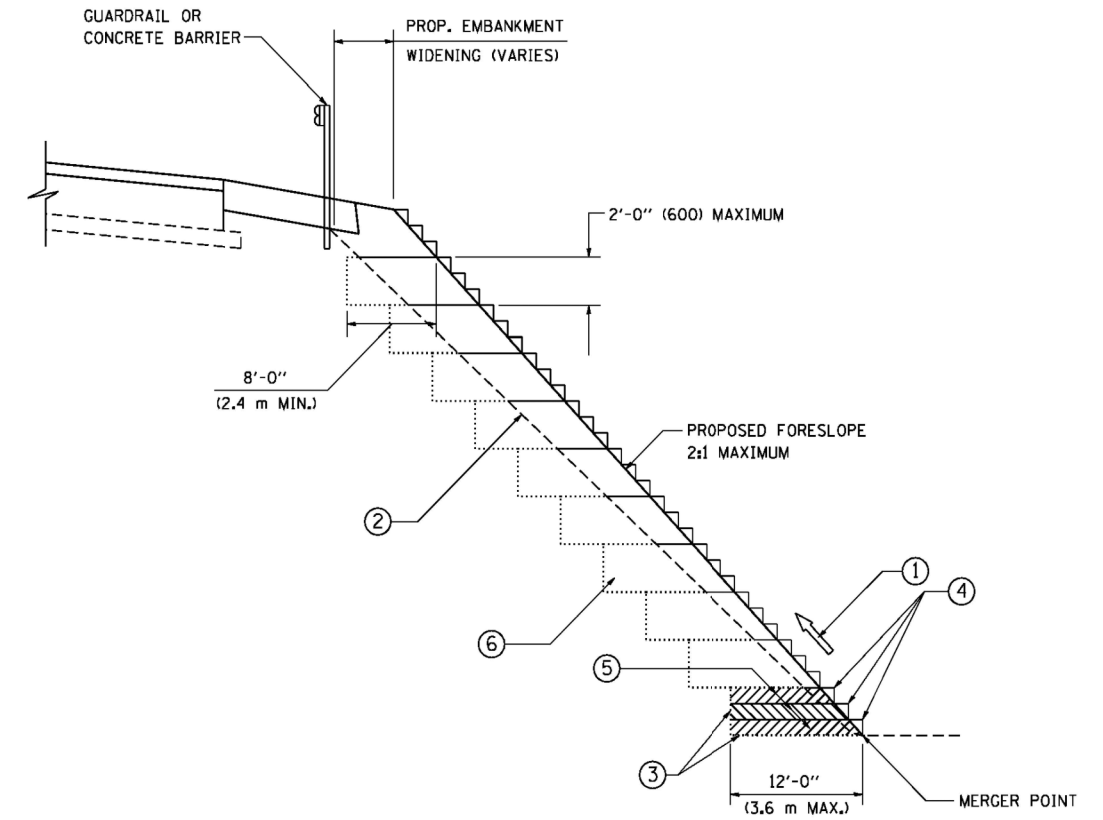
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		DRAWN - JIS	REVISED - A. ABBAS 05-05-99
	PLOT SCALE = 50,0000 ' / IN.	CHECKED - A. ABBAS	REVISED - E. GOMEZ 12-21-00
	PLOT DATE = 1/4/2008	DATE - 09-10-94	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

HMA TAPER AT EDGE OF P.C.C. PAVEMENT	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			388	249
BD400-06 (BD33)		CONTRACT NO.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**TYPICAL BENCHING DETAIL
FOR EMBANKMENT**

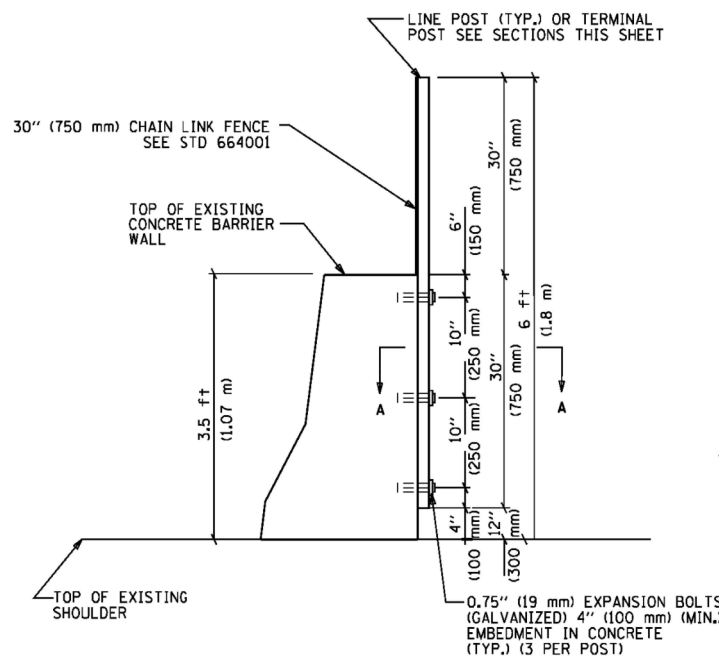
NOTES:

- ① CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
- ② EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS.
- ③ BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
- ④ TRIM TO FINAL SLOPE.
- ⑤ EQUAL 8-INCH (200) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.
- ⑥ EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR "EARTH EXCAVATION". THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL, NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ⑦ SLOPES SHALL BE BENCHED ACCORDING TO THIS DETAIL WHEN THE SLOPE IS STEEPER THAN 4:1 AND THE HEIGHT IS GREATER THAN 5' (1.5 m).

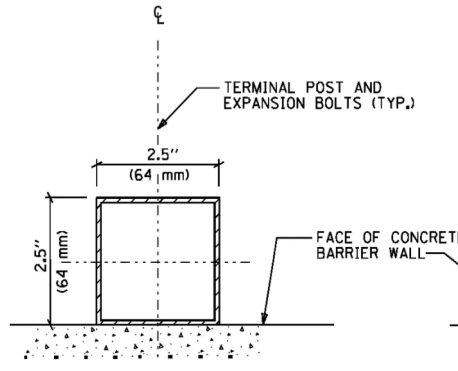
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

FILE NAME = W:\detroit\22x34\bd51.dgn	USER NAME = gaglianobt	DESIGNED - DRAWN - CADD	REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BENCHING DETAIL FOR EMBANKMENT WIDENING			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE = 50,0000 ' / IN.	CHECKED - S.E.B.	REVISED -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	BD-51	CONTRACT NO.	388	250
PLOT DATE = 1/4/2008	DATE - 06-16-04	REVISED -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							

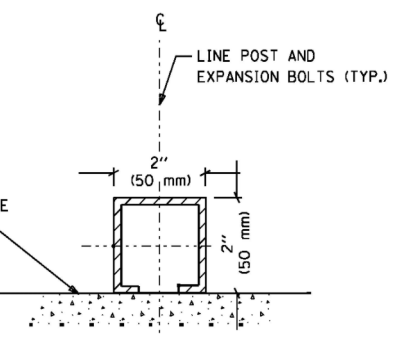
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			401	222
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



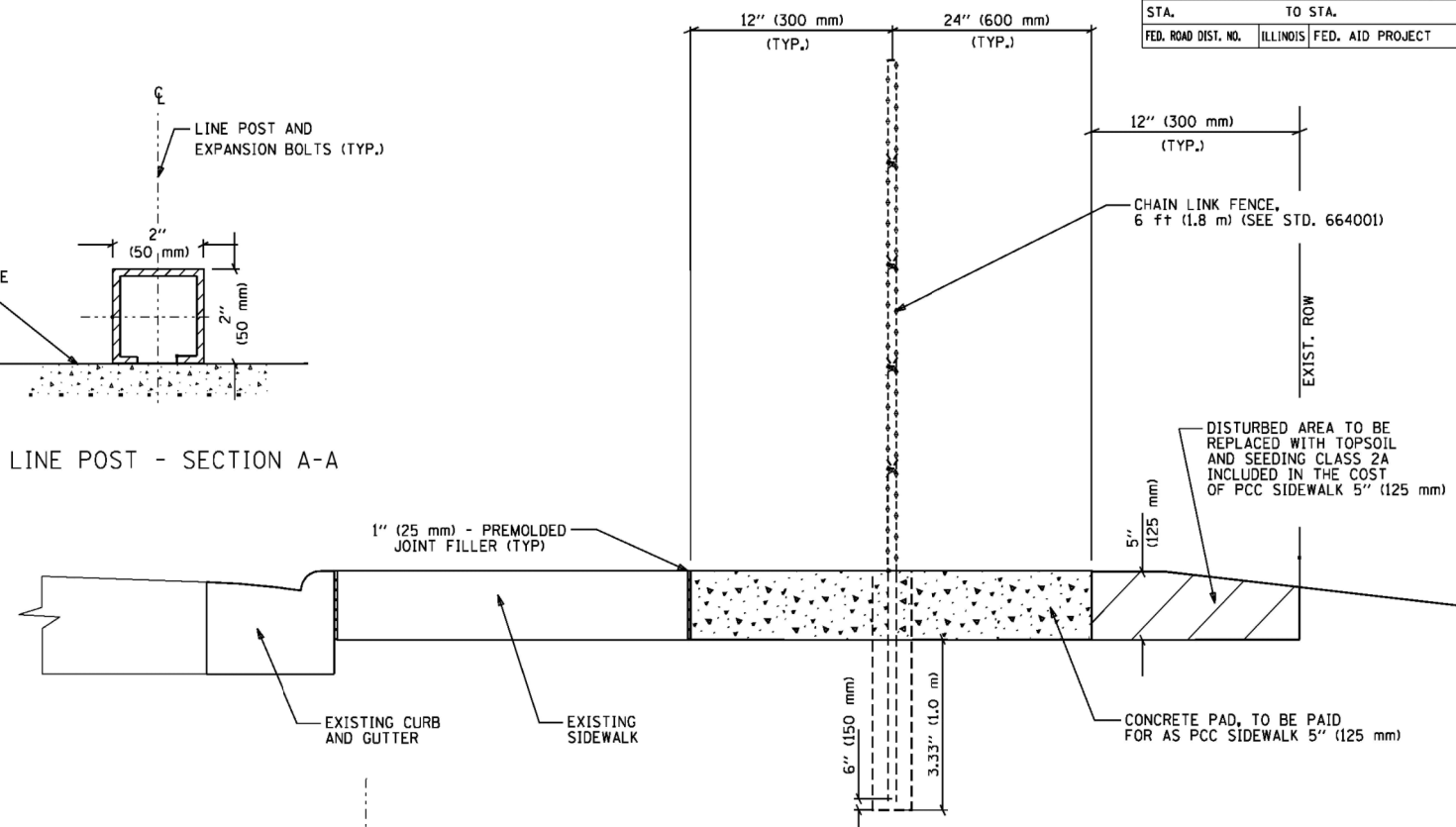
FENCE ATTACHED TO BACK OF EXISTING WALL - SECTION



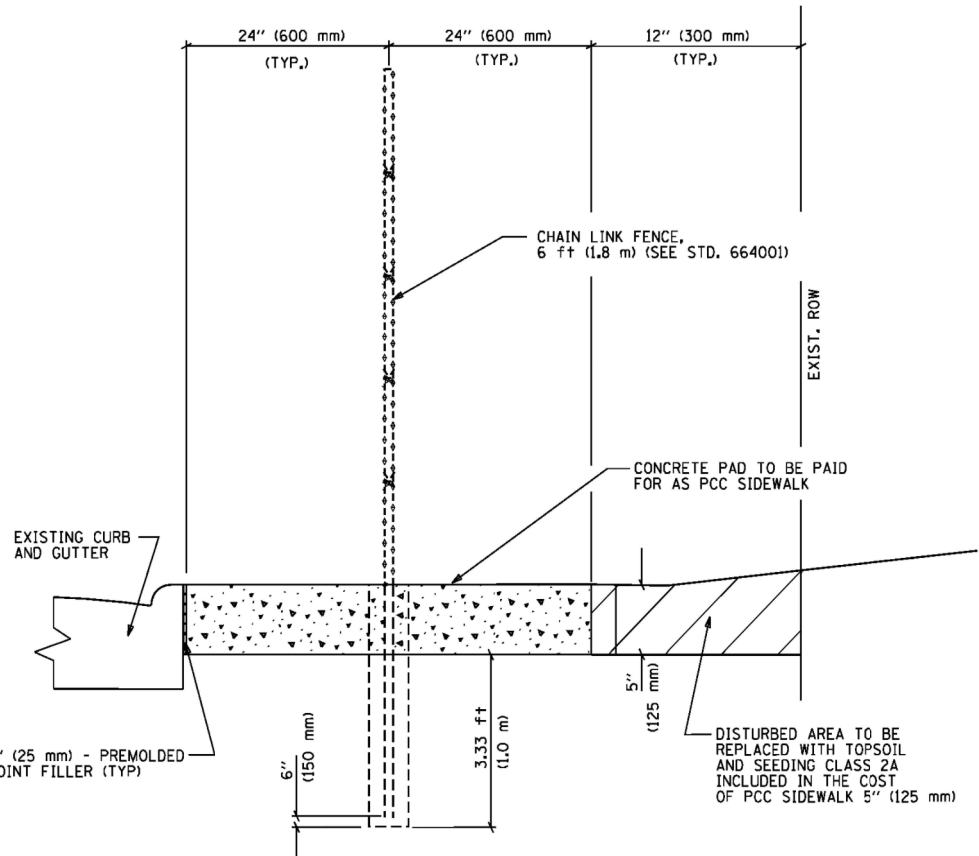
TERMINAL POST - SECTION A-A



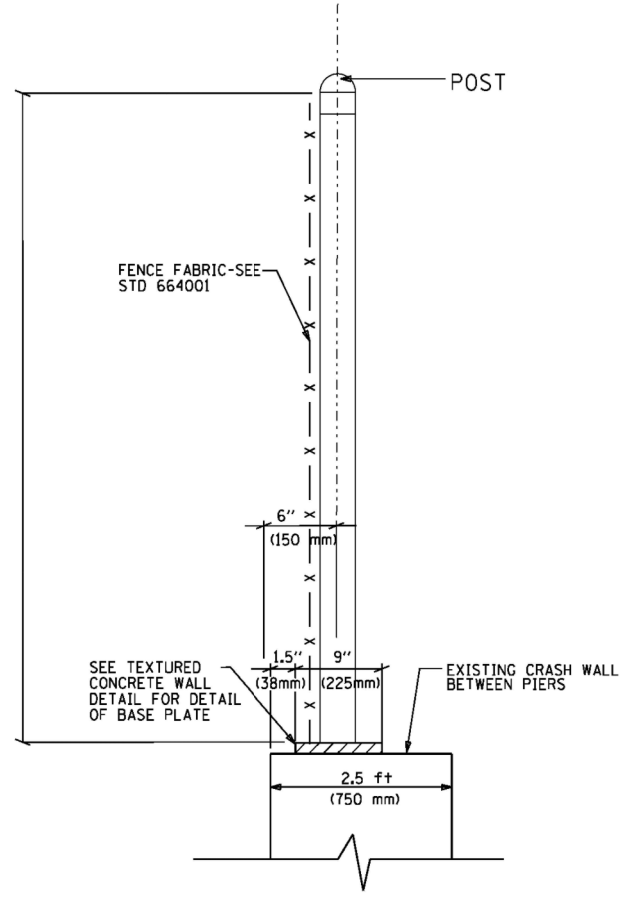
LINE POST - SECTION A-A



FENCE MOUNTED ON CONCRETE PAD ALONG EXISTING SIDEWALK - SECTION



FENCE MOUNTED ON CONCRETE PAD - SECTION



FENCE MOUNTED ON CRASH WALL BETWEEN EXISTING PIERS

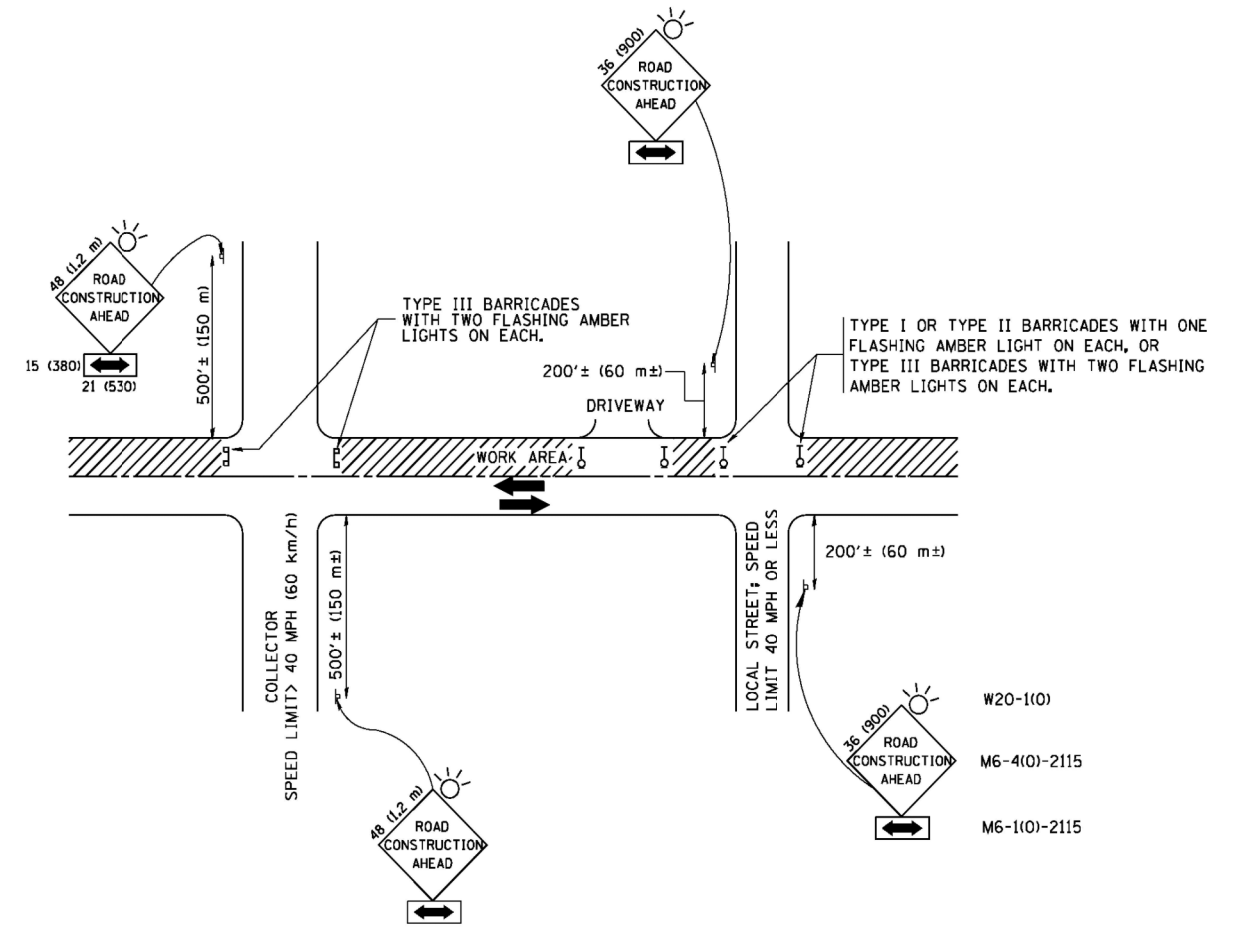
- NOTES:**
- FENCE MOUNTED ON CONCRETE PAD SHALL BE PAID FOR UNDER THE TWO PAY ITEMS, "CHAIN LINK FENCE, 6 ft (1.8 m) AND "PCC SIDEWALK 5 in (125 mm) IN ACCORDANCE WITH THE STANDARD SPECIFICATION FOR EACH.
 - FENCE MOUNTED ON THE BACK OF WALL SHALL BE PAID FOR AS "CHAIN LINK FENCE 4 ft (1.2 m) "ATTACHED TO STRUCTURE".
 - ALL DIMENSIONS IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
 - EARTH EXCAVATION FOR CONSTRUCTION OF THE CONCRETE PAD IS TO BE INCLUDED IN THE COST OF PCC SIDEWALK 5 in (125 mm).
 - CONCRETE PAD JOINT SPACING SHALL BE IN ACCORDANCE WITH STD SPECIFICATIONS SEC 424.
 - FENCE MOUNTED ON CRASH WALL BETWEEN PIERS SHALL BE PAID FOR AS "CHAIN LINK FENCE, 6 ft (1.8 m) ATTACHED TO STRUCTURE".

REVISIONS	
NAME	DATE
LoFleur	

ILLINOIS DEPARTMENT OF TRANSPORTATION
FENCE MOUNTED ON WALL/PAD
FENCE ATTACHED TO EXISTING WALL
 SCALE: VERT. N.T.S. DRAWN BY CADD
 HORIZ. DATE 12/27/2006 CHECKED BY 388 - 251

PLOT DATE = 12/27/2006
 FILE NAME = c:\projects\11ststd\bd54.dgn
 PLOT SCALE = 50.0000 / 1"
 REFERENCE = 11ststd

12/27/2006
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 ulrlchkd



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

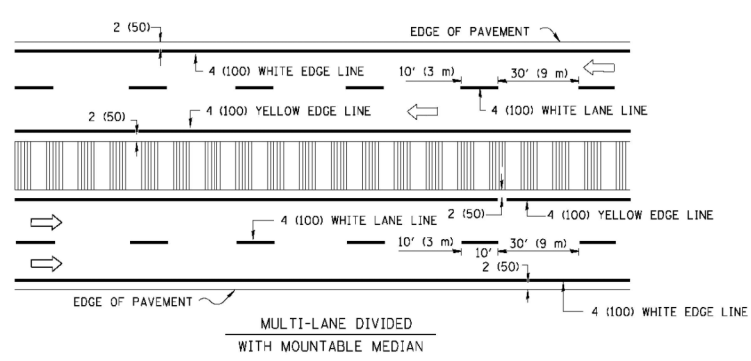
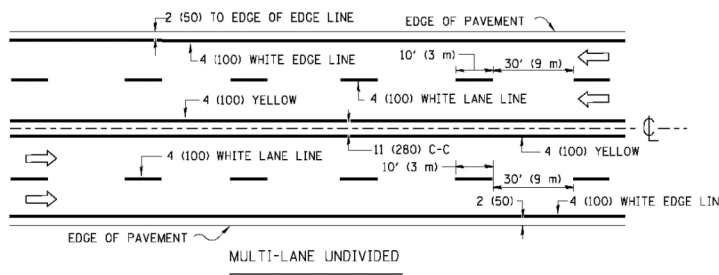
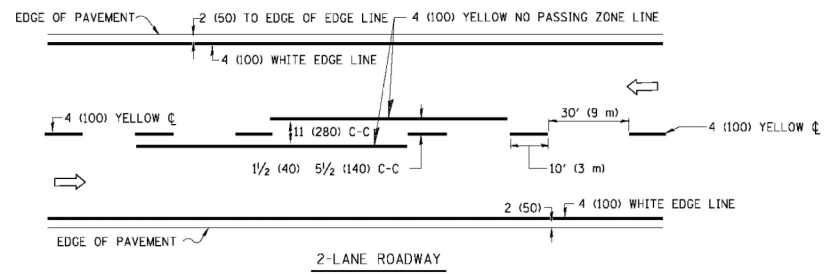
NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 - 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

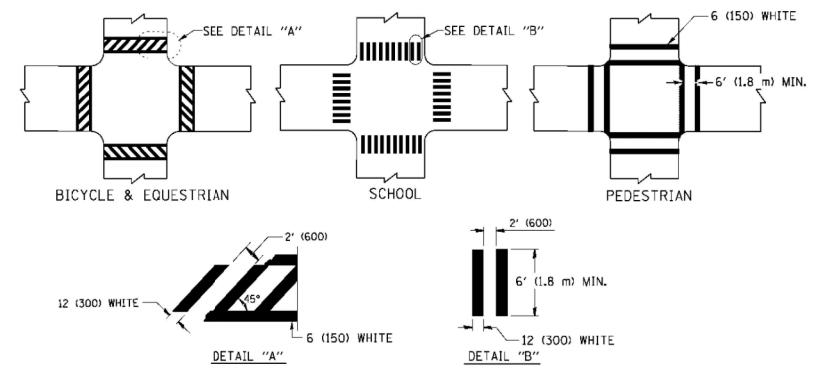
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
 - USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

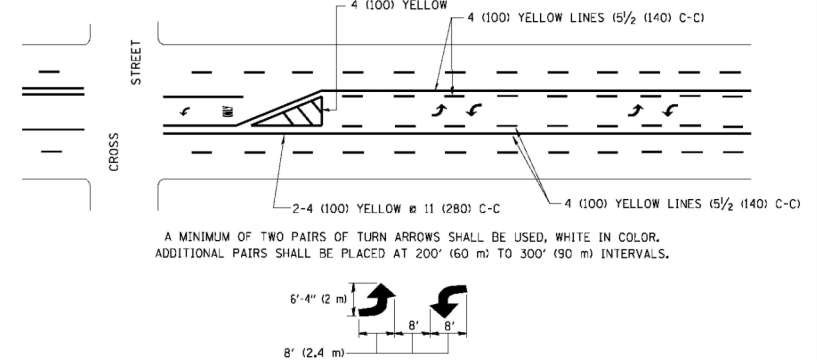
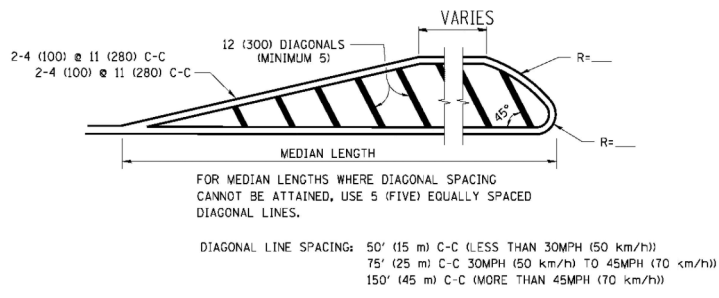
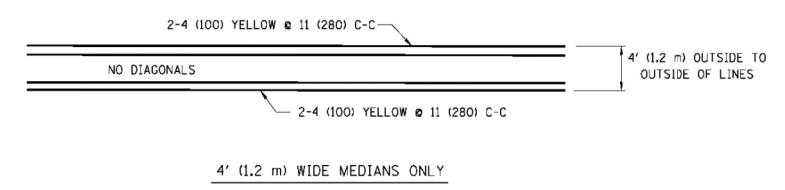
FILE NAME = W:\distrat\22x34\1c18.dgn	USER NAME = goglienobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED - A. HOUSEH 03-06-96							388	252
PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	TC-10		CONTRACT NO.		
							FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



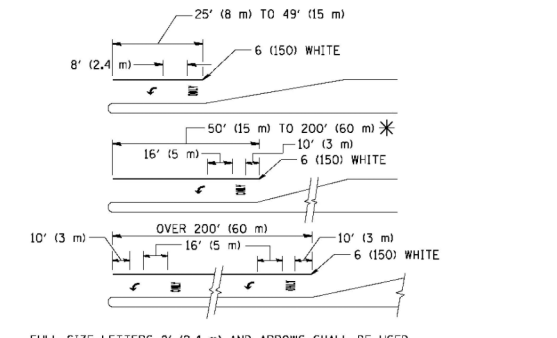
TYPICAL LANE AND EDGE LINE MARKING



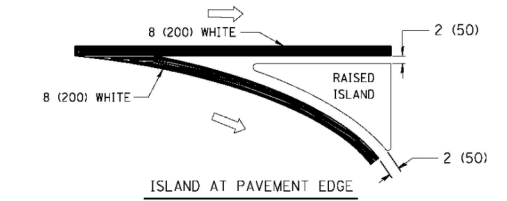
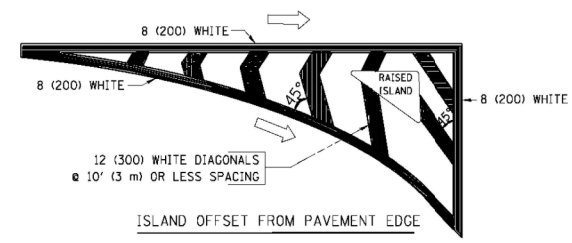
TYPICAL CROSSWALK MARKING



TYPICAL PAINTED MEDIAN MARKING



TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT; OTHERWISE PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

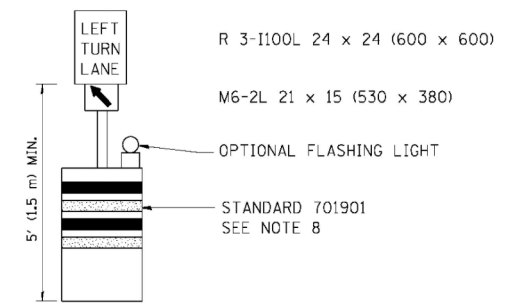
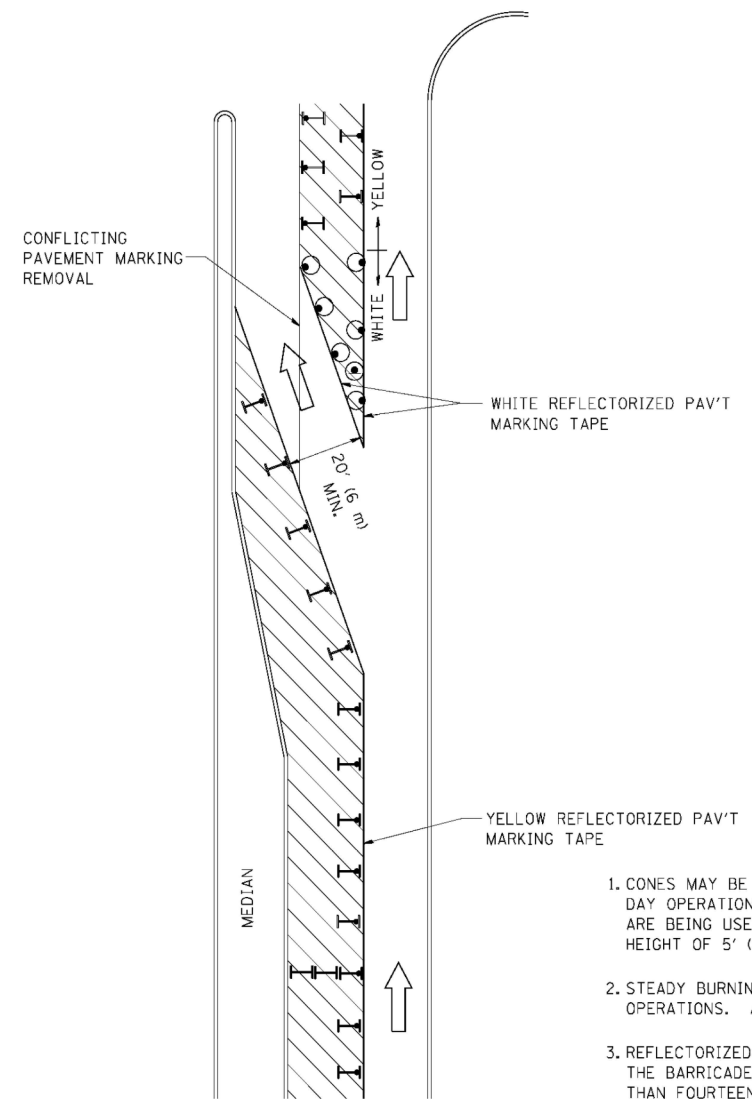
All dimensions are in inches (millimeters) unless otherwise shown.

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	PLOT SCALE = 50,000 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS		
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-13		388	253
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO.	









GENERAL NOTES

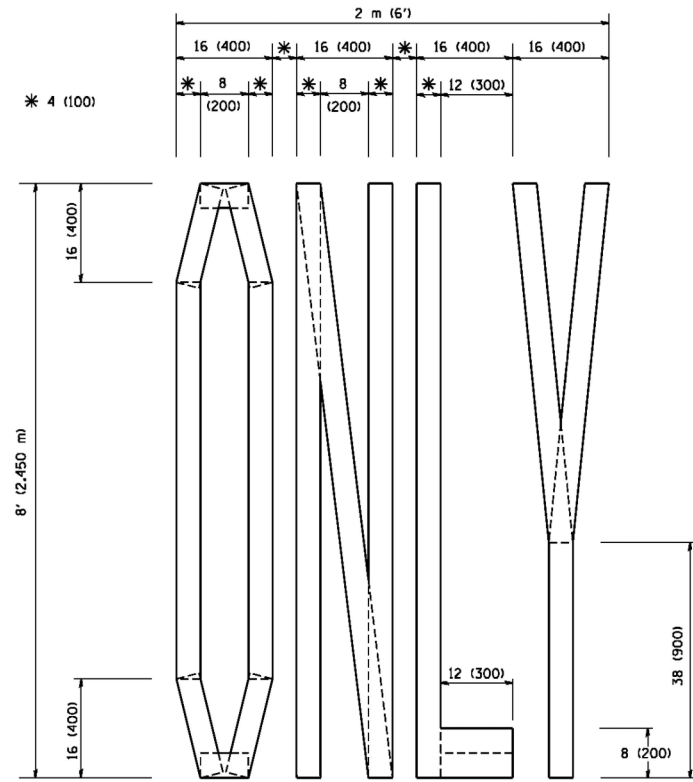
1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHR 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

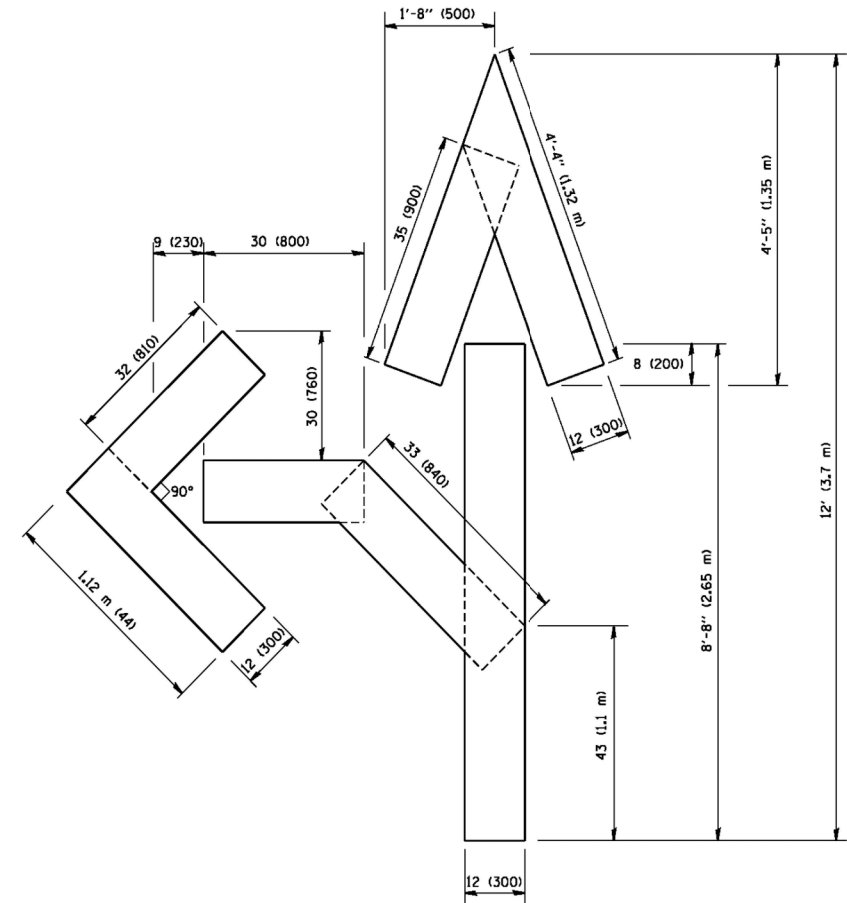
LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

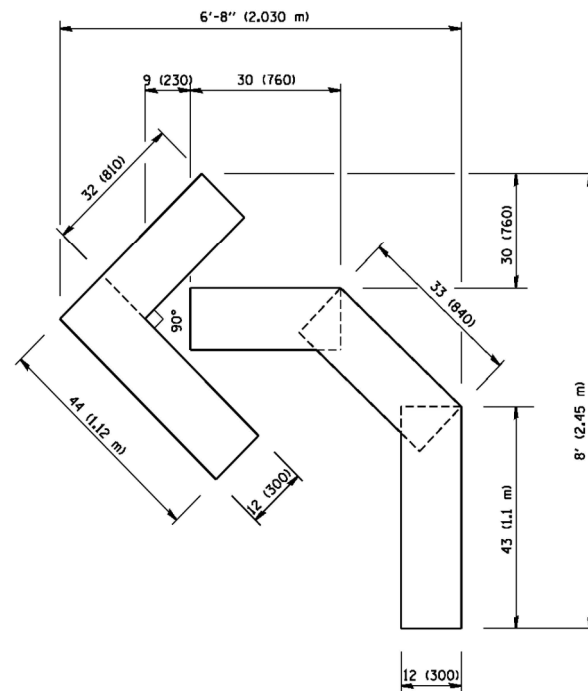
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es:\pwork\pwork\DOT\DRIVAKOSGN\0100315\14.dgn	REVISED - A. HOUSEH 11-07-95	REVISED -								388	254		
PLOT SCALE = 49.9999 / 1 IN.	REVISED - A. HOUSEH 10-12-96	REVISED -				TC-14							
PLOT DATE = 9/14/2009	REVISED - T. RAMMACHER 01-06-00	REVISED -				CONTRACT NO.							
						SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\dstat\22x34\16.dgn	USER NAME = gaglienobt	DESIGNED - DRAWN -	REVISED -T, RAMMACHER 06-05-96 REVISED -T, RAMMACHER 11-04-97
PLOT SCALE = 50,0000 ' / IN.	CHECKED -	REVISED -T, RAMMACHER 03-02-98	REVISED -E, GOMEZ 08-28-00
PLOT DATE = 1/4/2008	DATE - 09-18-94		

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

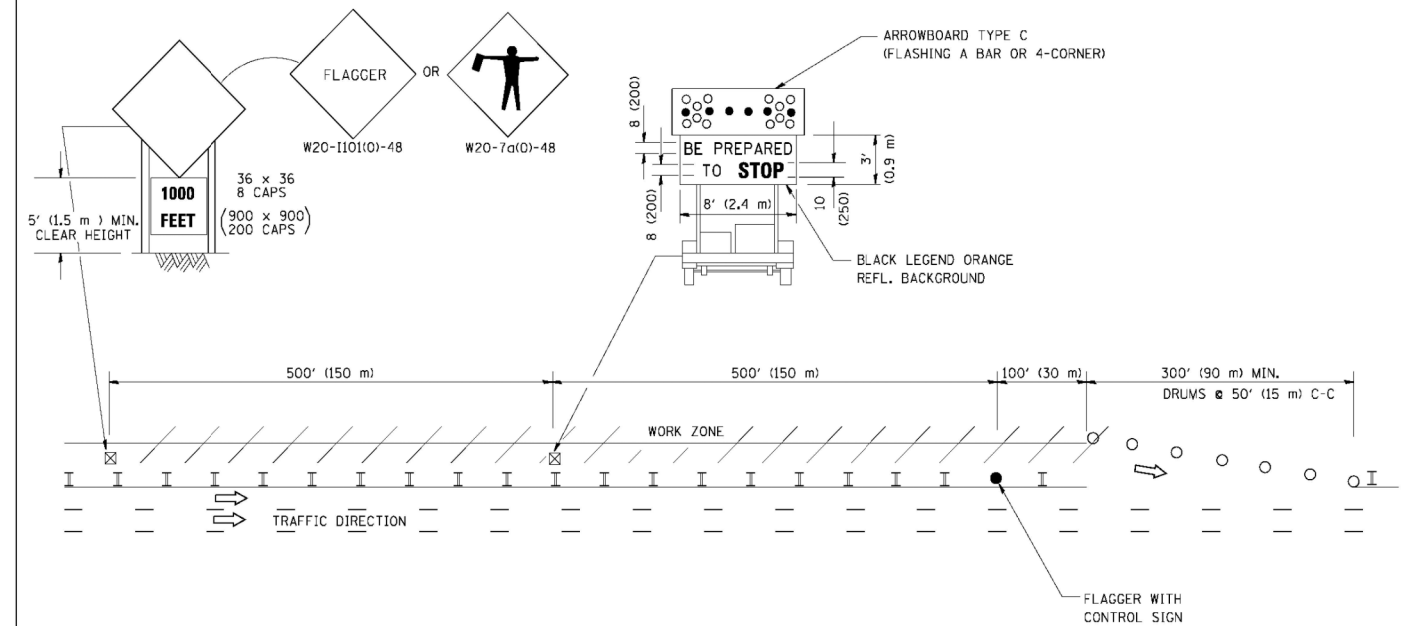
PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

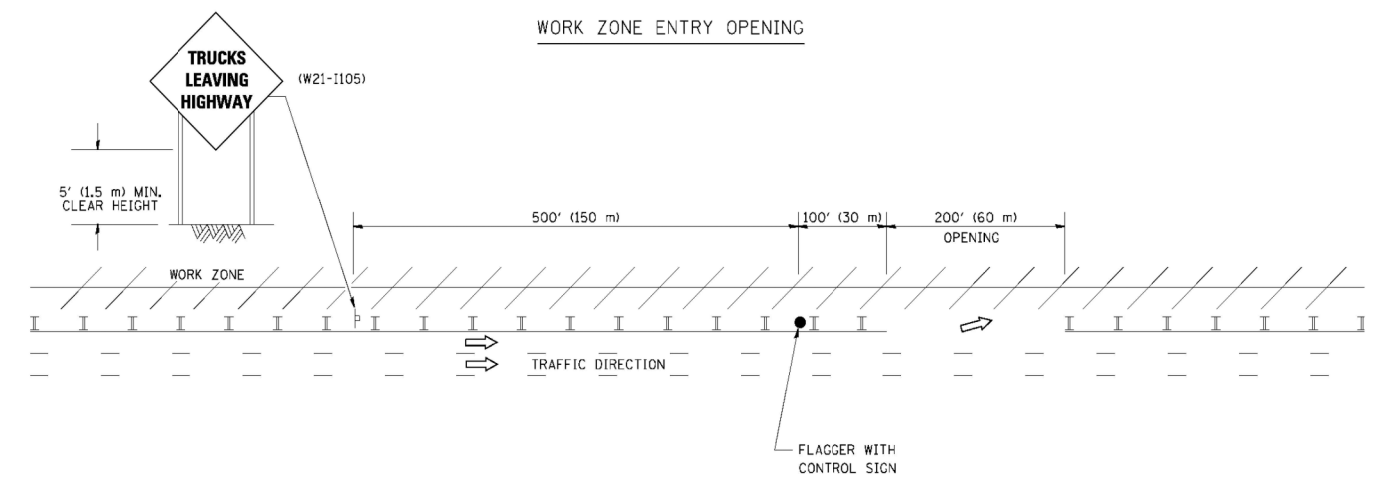
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			388	255
TC-16			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING

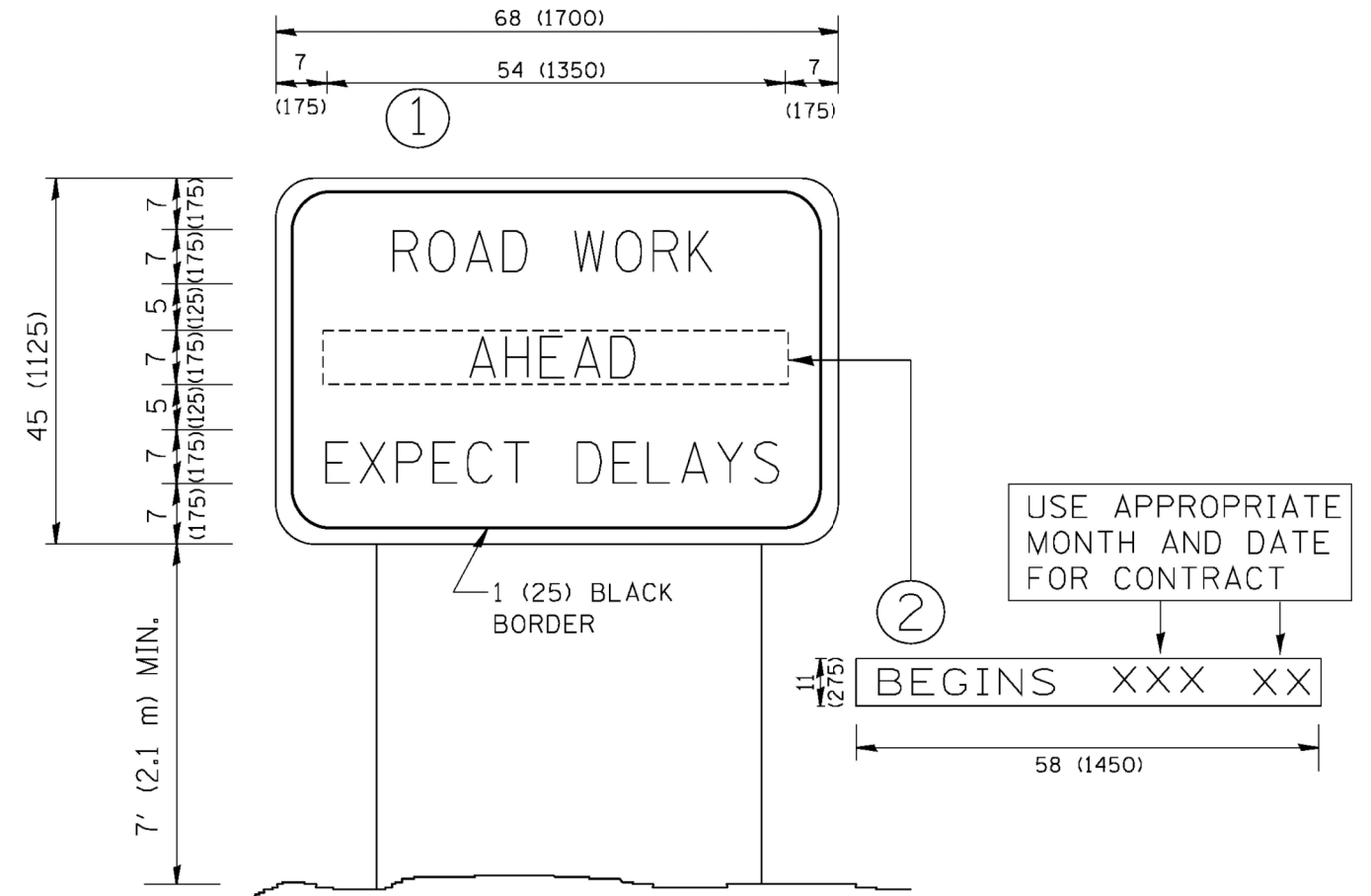


NOTES:

1. THE ARROWBOARD, THE FLAGGER AHEAD SIGN AND THE TRUCKS LEAVING HIGHWAY SIGN SHALL BE REMOVED OR TURNED AWAY FROM TRAFFIC AND THE EXIT AND ENTRY OPENINGS SHALL BE CLOSED WHEN THE FLAGGING OPERATION CEASES. NON OPERATING EQUIPMENT SHALL COMPLY WITH ARTICLE 701.11
2. WORK ZONE EXIT OPENINGS SHOULD BE A MINIMUM OF ONE HALF MILE APART.
3. EXITING THE WORK ZONE AT ANY PLACE OTHER THAN AT A WORK ZONE EXIT OPENING WILL BE PROHIBITED.
4. ALL VEHICLES SHALL ENTER THE WORK ZONE AT ENTRY OPENINGS, USING THEIR TURN SIGNALS TO WARN MOTORISTS

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME = W:\diststd\22x34\to18.dgn	USER NAME = lujms	DESIGNED -	REVISED - J.A.F. 04-03	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS 388	SHEET NO. 256
	PLOT SCALE = 50.000' / IN.	DRAWN -	REVISED - J.A.F. 02-06		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	TC-18		CONTRACT NO.	
	PLOT DATE = 1/26/2010	CHECKED -	REVISED - S.P.B. 01-07		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
		DATE -	REVISED - S.P.B. 12-09									



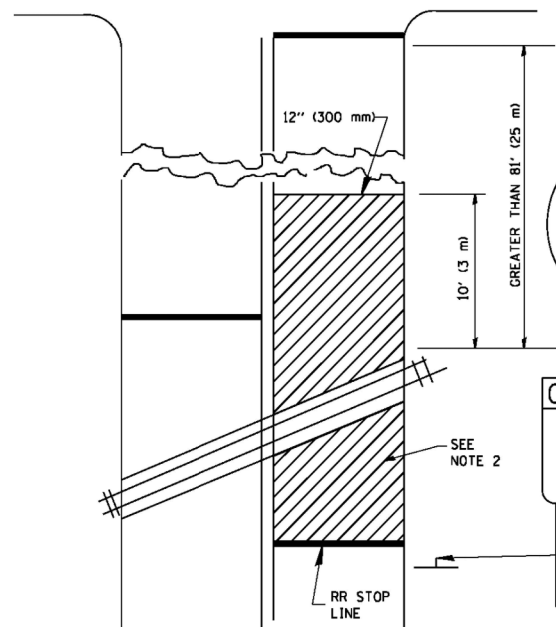
NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

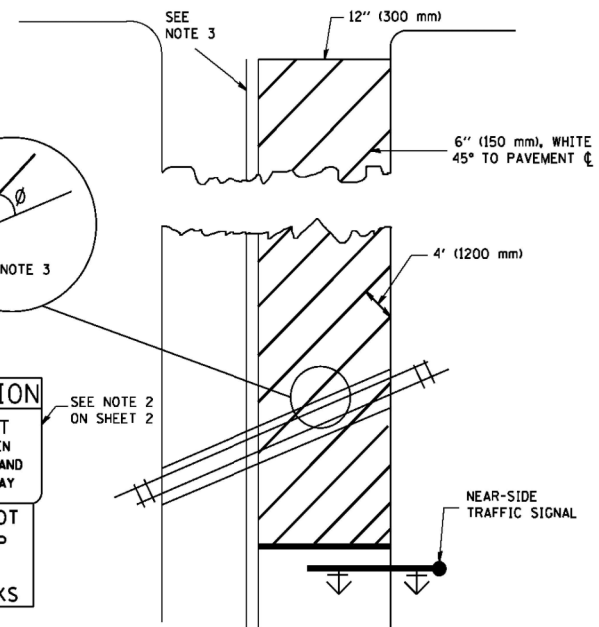
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\dstatat\22x34\to22.dgn	USER NAME = gaglienobt	DESIGNED - DRAWN -	REVISED - REVISED -	R. MIRS 09-15-97 R. MIRS 12-11-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED -	T. RAMMACHER 02-02-99		SCALE: NONE	SHEET NO. 1	OF 1	SHEETS	STA.	TO STA.	388	257
	PLOT DATE = 1/4/2008	DATE -	REVISED -	C. JUCIUS 01-31-07					TC-22		CONTRACT NO.		
											FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

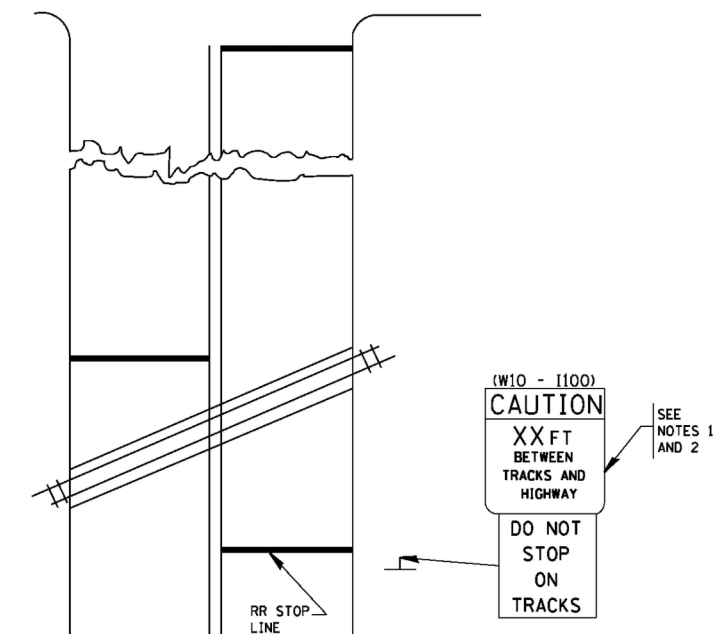
WITH INTERSECTION TRAFFIC SIGNALS



WITH NEAR-SIDE TRAFFIC SIGNALS



WITH NONSIGNALIZED INTERSECTION
81' (25 m) OR LESS TO CLOSEST RAIL



CAUTION
XX FT
BETWEEN
TRACKS AND
HIGHWAY
DO NOT
STOP
ON
TRACKS

(W10 - 1100)
CAUTION
XX FT
BETWEEN
TRACKS AND
HIGHWAY
DO NOT
STOP
ON
TRACKS

NOTES:

- PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
- WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED, THE PAVEMENT MARKINGS EXTENDS TO THE INTERSECTION.
- WHERE THE ANGLE BETWEEN THE DIAGONAL STRIPES AND THE TRACK (Ø) WOULD BE LESS THAN APPROXIMATELY 20°, THE STRIPES SHOULD BE SLOPED IN THE OPPOSITE DIRECTION FROM THAT SHOWN.

NOTE :

- DISTANCE TO BE SHOWN ON SIGN MEASURED FROM A POINT 6 FEET (1.8 m) FROM THE RAIL CLOSEST TO THE INTERSECTION TO THE STOP LINE OR CROSSWALK, WHICHEVER IS CLOSEST, ROUNDED DOWN TO THE NEAREST 5 FEET (1.5 m). WHERE THERE IS NO STOP LINE, MEASURE TO POINT WHERE THE DRIVER HAS A VIEW OF APPROACHING TRAFFIC.
- THE CLEARANCE SIGN IS ALSO TO BE USED AS AN INTERIM MEASURE AT LOCATIONS WITH INTERCONNECTED INTERSECTION TRAFFIC SIGNALS WHERE IT IS PLANNED TO CHANGE THEM TO NEAR-SIDE SIGNALS AT A FUTURE TIME. IN THIS CASE, THE DISTANCE TO BE SHOWN ON THE SIGN IS MEASURED FROM THE EDGE OF THE STRIPED-OUT AREA INSTEAD OF 6-FEET FROM THE RAIL. THE SIGN IS TO BE REMOVED WHEN THE NEAR-SIDE SIGNALS ARE INSTALLED AND THE PAVEMENT MARKINGS EXTEND TO THE INTERSECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\dstatat\22x34\tr23.dgn	USER NAME = gaglienobt	DESIGNED -	REVISED - 01-01-07
		DRAWN -	REVISED -
	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 1/4/2008	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			388	258
TC-23			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

WB IL ROUTE 19 INTERIM PROFILE GRADE LINE COORDINATES (FOR PROFILES INTERIM #1 AND INTERIM #2)

CURVE NAME	POINT		NORTHING	EASTING	BEARING	DISTANCE (FEET)
	POT STA.	397+00.00 8.79' LT	1,928,601.0223	1,091,318.2803		
					N 89° 04' 55.11" E	76.07
	PC STA.	397+75.66 15.00' LT	1,928,602.2411	1,091,394.3409		
WB19-01	PT STA.	398+90.82 15.00' LT	1,928,592.7424	1,091,509.4127		
		405+58.20 15.00' LT	1,928,531.1593	1,092,173.9390	S 84° 42' 19.43" E	667.37

PROP. CURVE WB19-01
 $\Delta = 1^\circ 09' 5.84''$ (RT)
 $D = 0^\circ 59' 50.55''$
 $R = 5,744.65'$
 $T = 57.73'$
 $L = 115.46'$
 $E = 0.29'$

EB IL ROUTE 19 INTERIM PROFILE GRADE LINE COORDINATES (FOR PROFILE INTERIM #1)

CURVE NAME	POINT		NORTHING	EASTING	BEARING	DISTANCE (FEET)
	POT STA.	397+00.00 7.22' RT	1,928,585.0455	1,091,317.3350		
					S 81° 28' 44.81" E	259.62
	POT STA.	399+59.49 25.00' RT	1,928,546.5769	1,091,574.0940		
		405+58.20 25.00' RT	1,928,491.3300	1,092,170.2480	S 84° 42' 19.43" E	598.71

NOTES:

STATIONS AND OFFSETS ARE GIVEN TO THE PROPOSED CENTERLINE OF IL ROUTE 19 (IRVING PARK ROAD). REFER TO ALIGNMENT AND TIES SHEETS FOR IL ROUTE 19 ALIGNMENT DATA.

EB IL ROUTE 19 INTERIM PROFILE GRADE LINE COORDINATES (FOR PROFILE INTERIM #2)

CURVE NAME	POINT		NORTHING	EASTING	BEARING	DISTANCE (FEET)
	PC STA.	397+00.00 25.00' RT	1,928,567.2916	1,091,316.2844		
EB19-02	PT STA.	398+90.82 25.00' RT	1,928,552.9131	1,091,505.7216		
		405+58.20 25.00' RT	1,928,491.3300	1,092,170.2480	S 84° 42' 19.43" E	598.71

PROP. CURVE EB19-02
 $\Delta = 1^\circ 54' 29.56''$ (RT)
 $D = 1^\circ 00' 15.73''$
 $R = 5,704.65'$
 $T = 57.73'$
 $L = 188.99'$
 $E = 0.79'$



USER NAME = user	DESIGNED - MPK	REVISED -
	DRAWN - ML	REVISED -
PLOT SCALE = *SCALE*	CHECKED - LGP	REVISED -
PLOT DATE = 11/2/2012	DATE - 11-02-2012	REVISED -

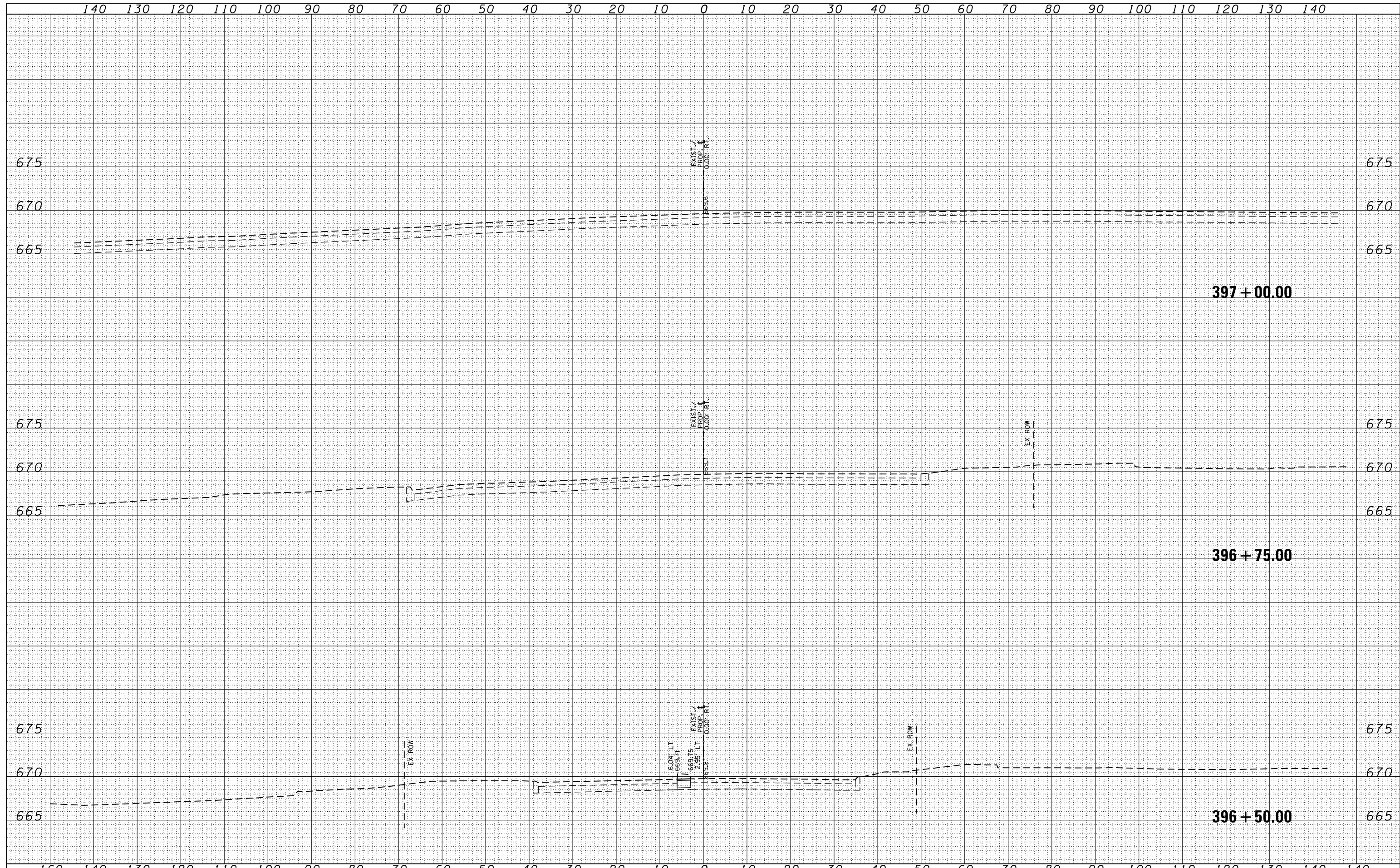
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 19 /YORK ROAD INTERIM PROFILE PGL DETAILS	
SCALE: NTS	SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32VB	DUPAGE	388	259
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				

DATE	
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FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED

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ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
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	AREAS CHECKED



USER NAME = user	DESIGNED - MPK	REVISED -
	DRAWN - ML	REVISED -
PLOT SCALE = *SCALE*	CHECKED - LGP	REVISED -
PLOT DATE = 11/2/2012	DATE - 11-02-2012	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

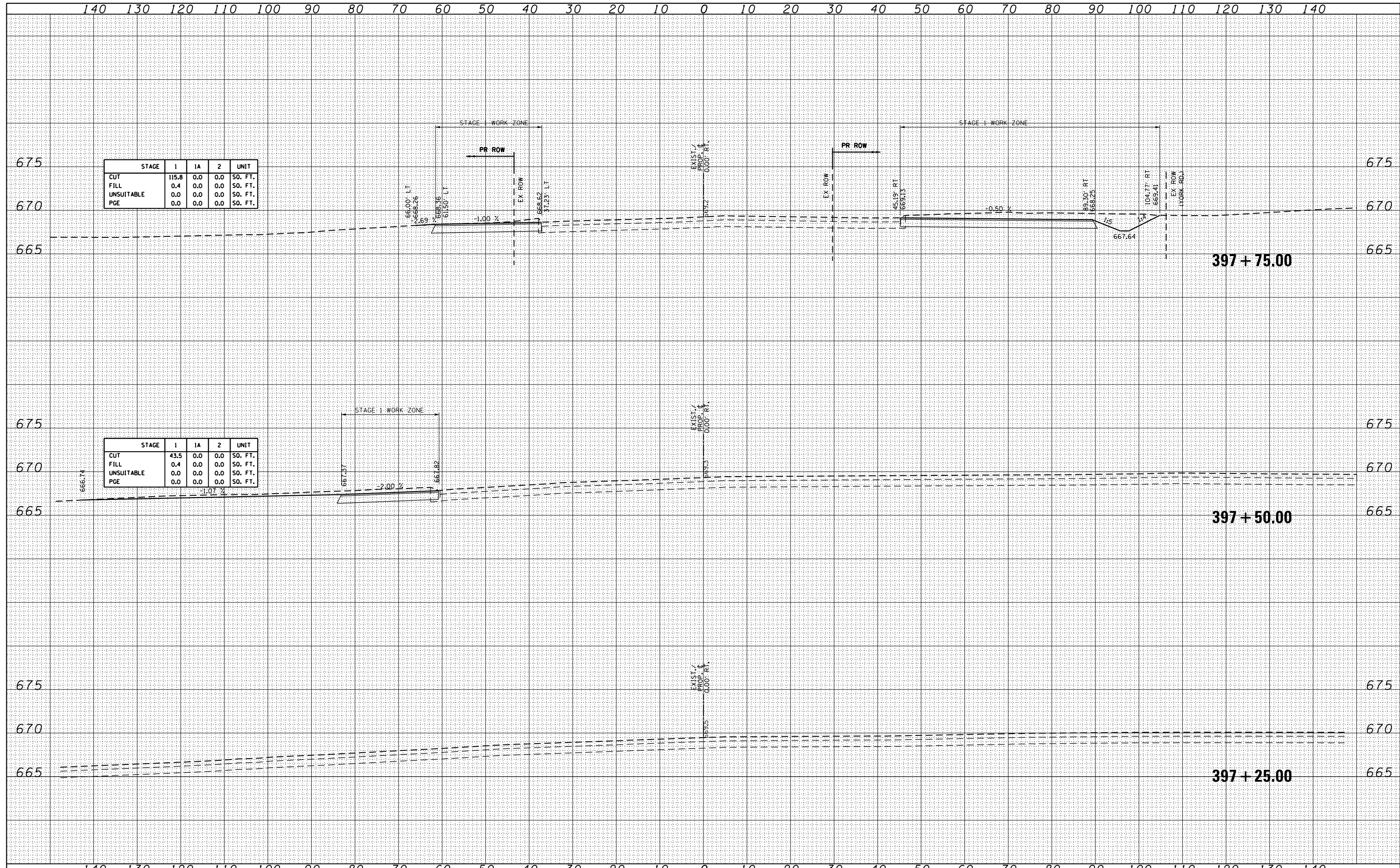
**IRVING PARK (IL 19)
CROSS SECTIONS - STAGES 1 THROUGH 2**

SCALE: 1"=10'H; 1"=5'V SHEET NO. 1 OF 19 SHEETS STA. 396+50.00 TO STA. 397+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32VB	DUPAGE	388	260
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				

DATE	
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STAGE	1	1A	2	UNIT
CUT	115.8	0.0	0.0	SO. FT.
FILL	0.4	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

STAGE	1	1A	2	UNIT
CUT	43.5	0.0	0.0	SO. FT.
FILL	0.4	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.



USER NAME = user
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 DATE - 11-02-2012

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

IRVING PARK (IL 19)
 CROSS SECTIONS - STAGES 1 THROUGH 2

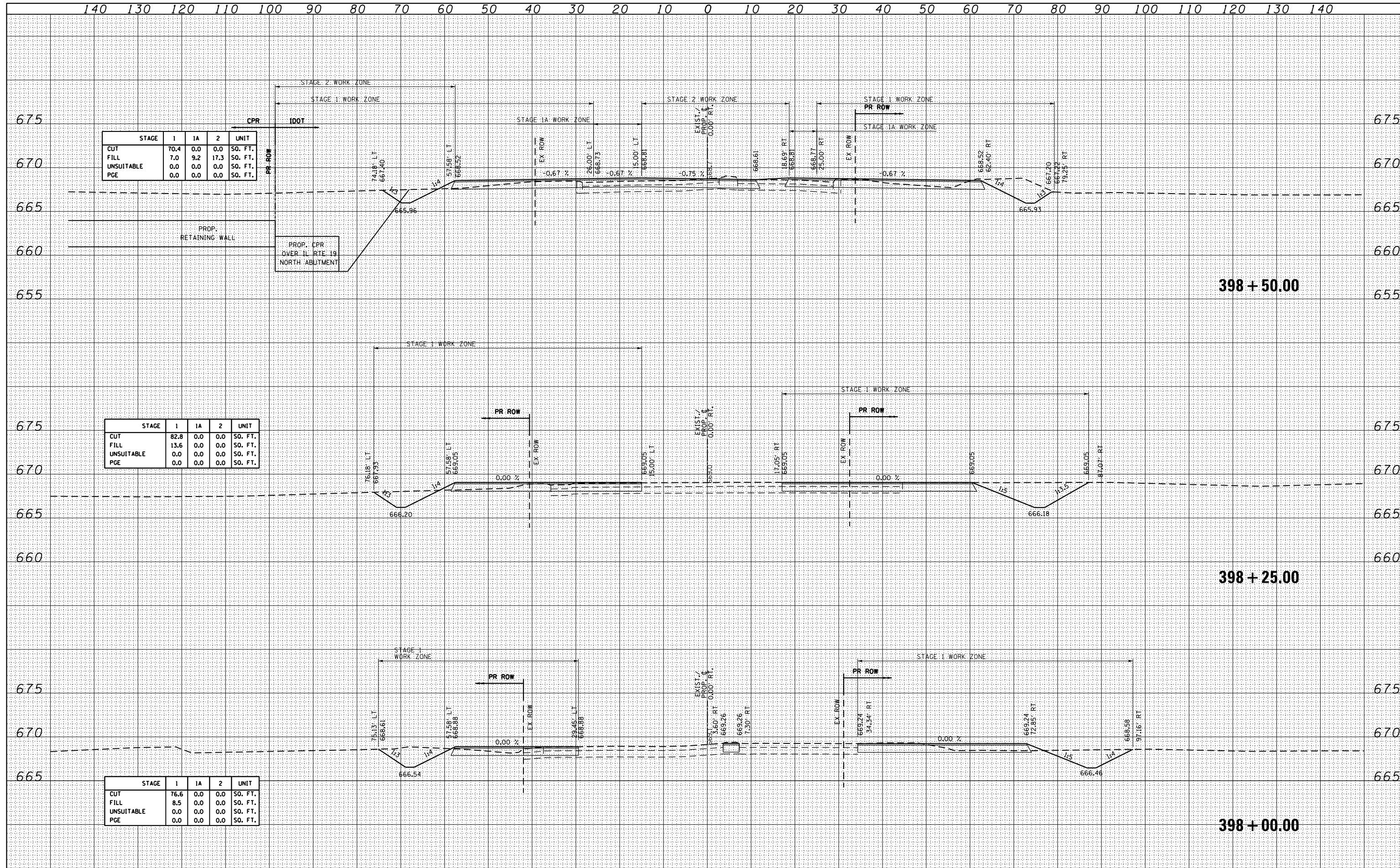
SCALE: 1"=10'H; 1"=5'V SHEET NO. 2 OF 19 SHEETS STA. 397+25.00 TO STA. 397+75.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32VB	DUPAGE	388	261
CONTRACT NO. 60W01				

ILLINOIS FED. AID PROJECT

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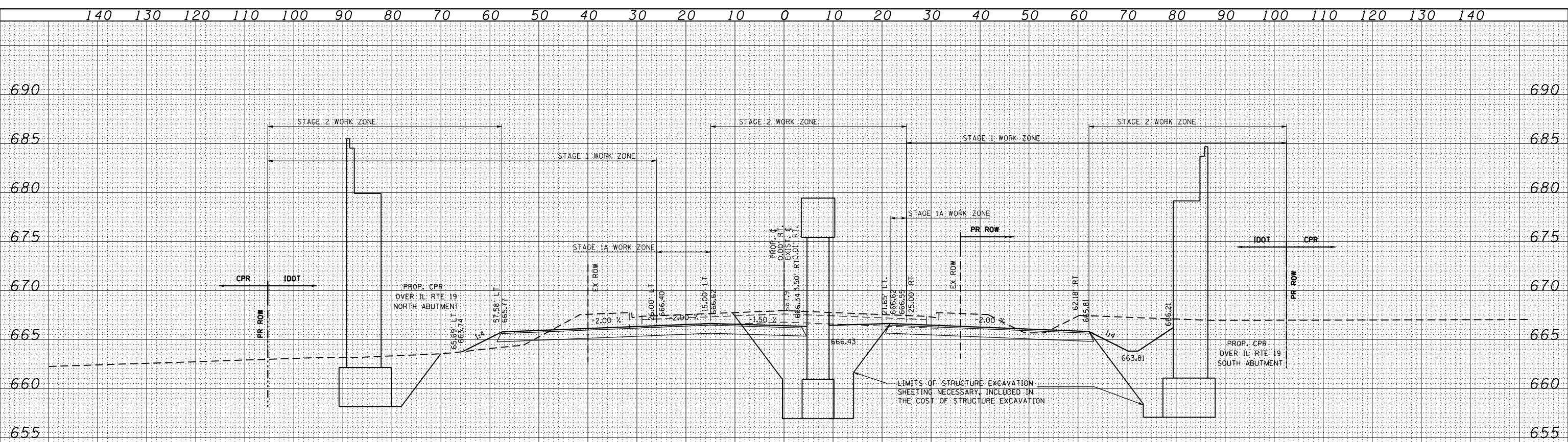


STAGE	1	1A	2	UNIT
CUT	70.4	0.0	0.0	SO. FT.
FILL	7.0	9.2	17.3	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

STAGE	1	1A	2	UNIT
CUT	82.8	0.0	0.0	SO. FT.
FILL	13.6	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

STAGE	1	1A	2	UNIT
CUT	76.6	0.0	0.0	SO. FT.
FILL	8.5	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

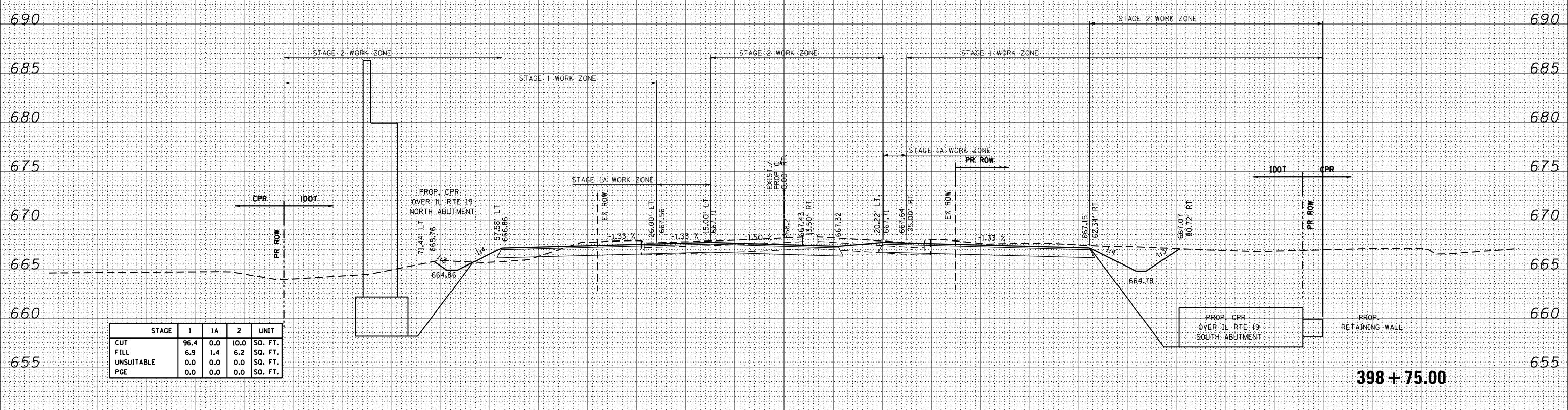
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STAGE	1	1A	2	UNIT
CUT	142.3	40.6	0.0	SO. FT.
FILL	8.4	5.5	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

399 + 00.00

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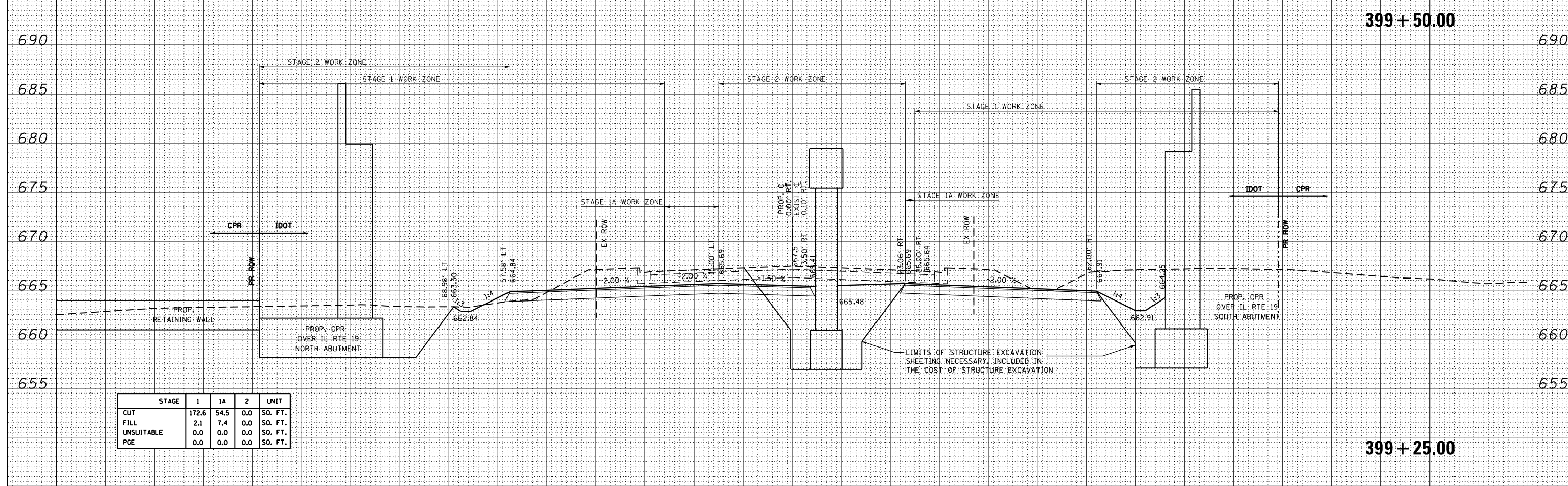
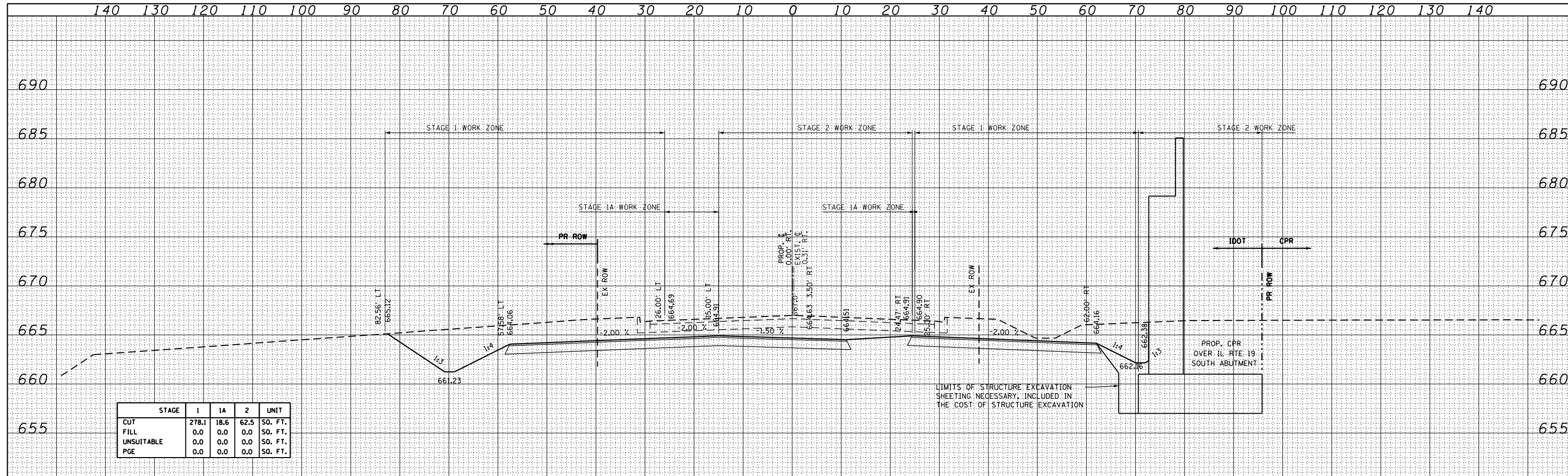


STAGE	1	1A	2	UNIT
CUT	96.4	0.0	10.0	SO. FT.
FILL	6.9	1.4	6.2	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

398 + 75.00

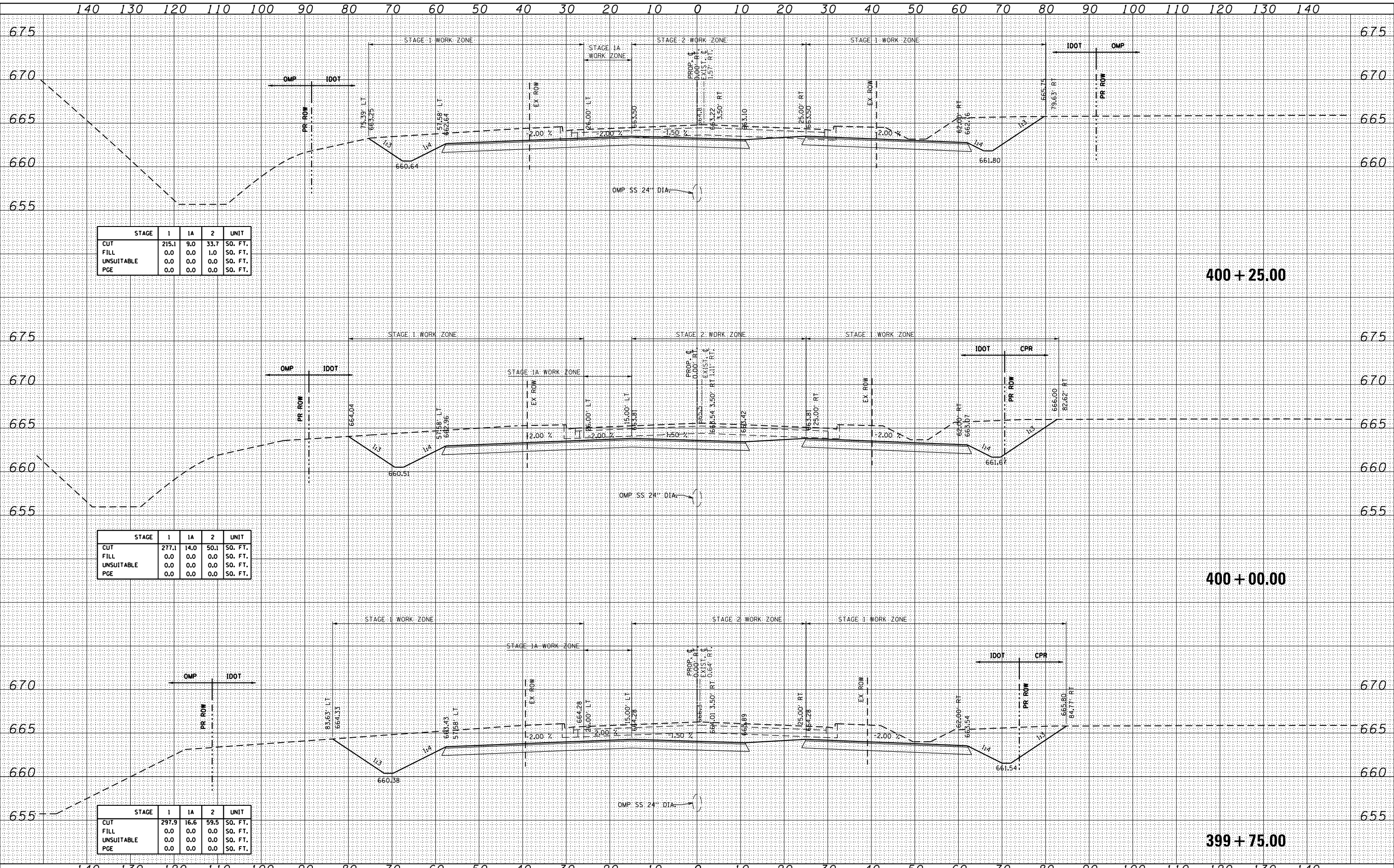
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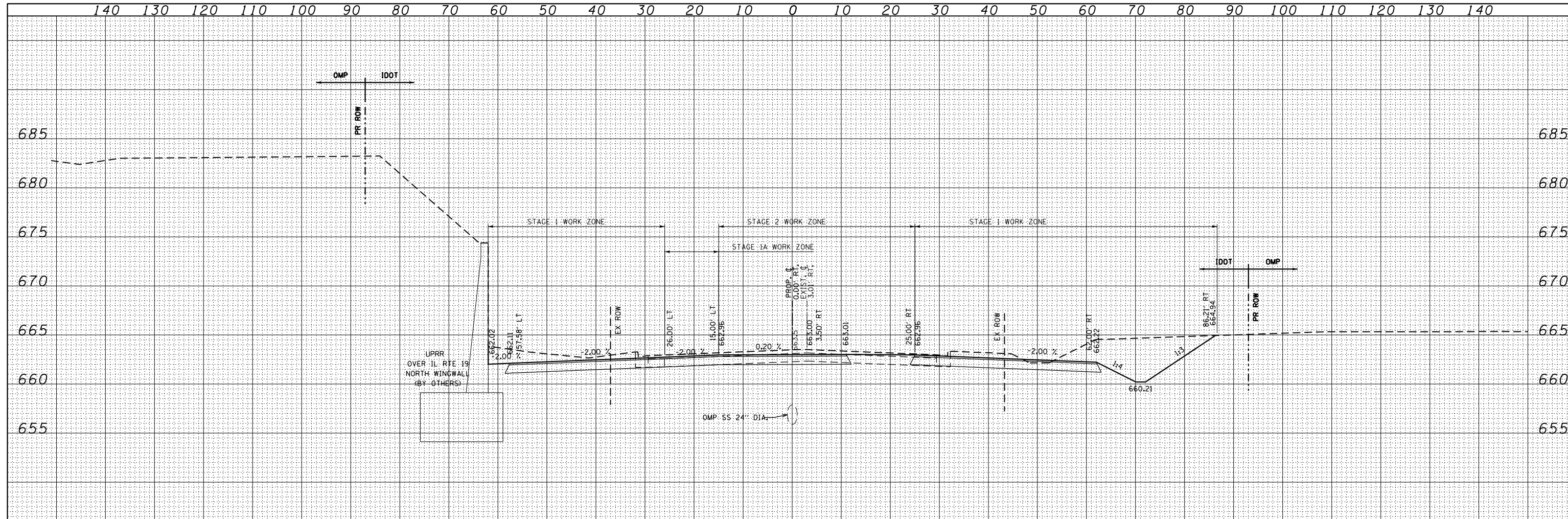
STAGE	1	1A	2	UNIT
CUT	215.1	9.0	33.7	SO. FT.
FILL	0.0	0.0	1.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

STAGE	1	1A	2	UNIT
CUT	277.1	14.0	50.1	SO. FT.
FILL	0.0	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

STAGE	1	1A	2	UNIT
CUT	297.9	16.6	59.5	SO. FT.
FILL	0.0	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

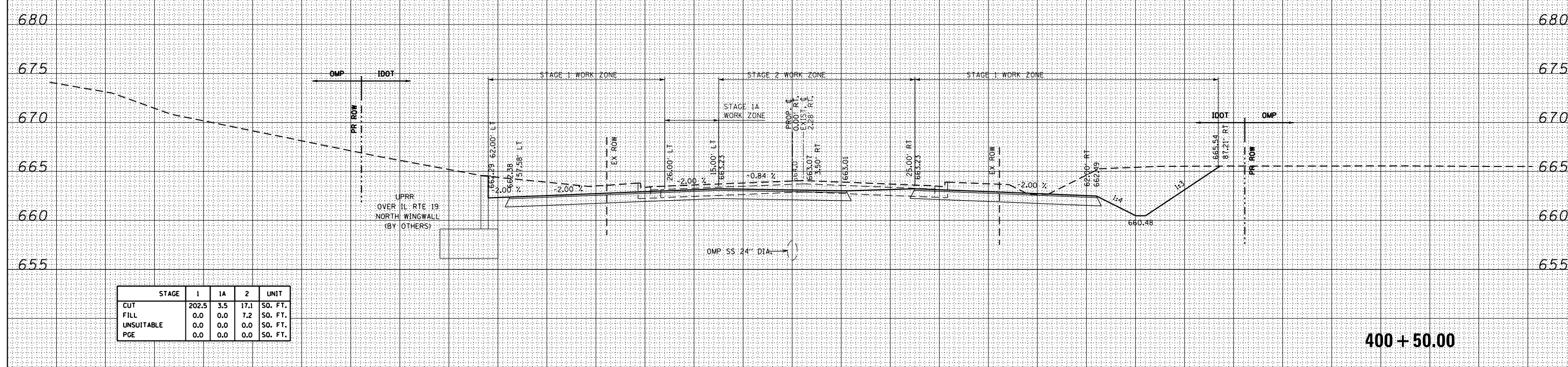
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STAGE	1	1A	2	UNIT
CUT	164.5	0.1	3.6	SO. FT.
FILL	0.5	0.0	12.5	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

400+75.00

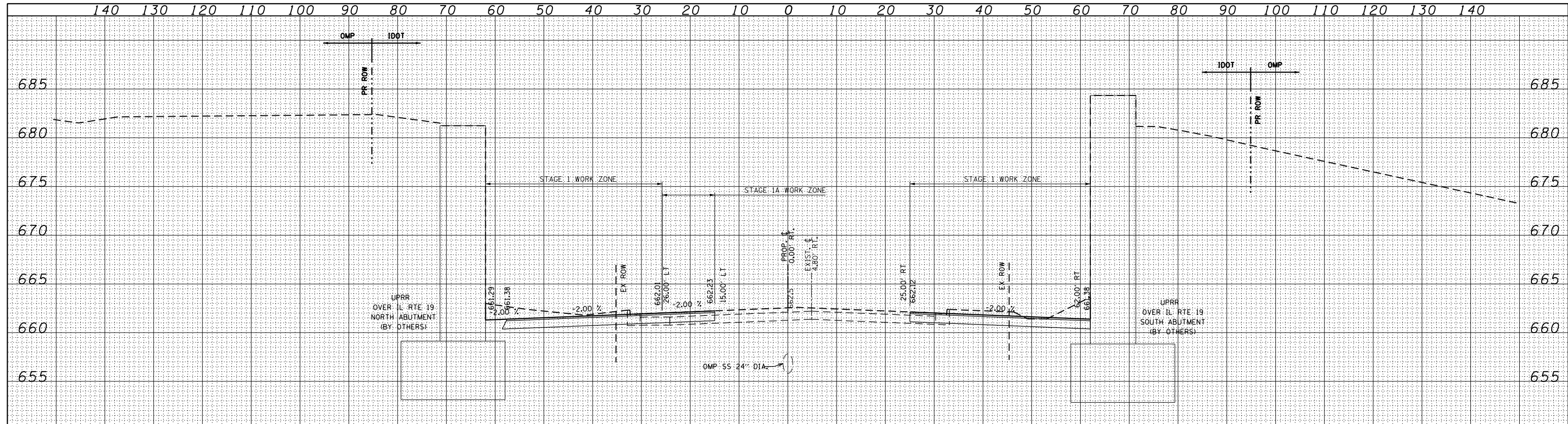


STAGE	1	1A	2	UNIT
CUT	202.5	3.5	17.1	SO. FT.
FILL	0.0	0.0	7.2	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

400+50.00

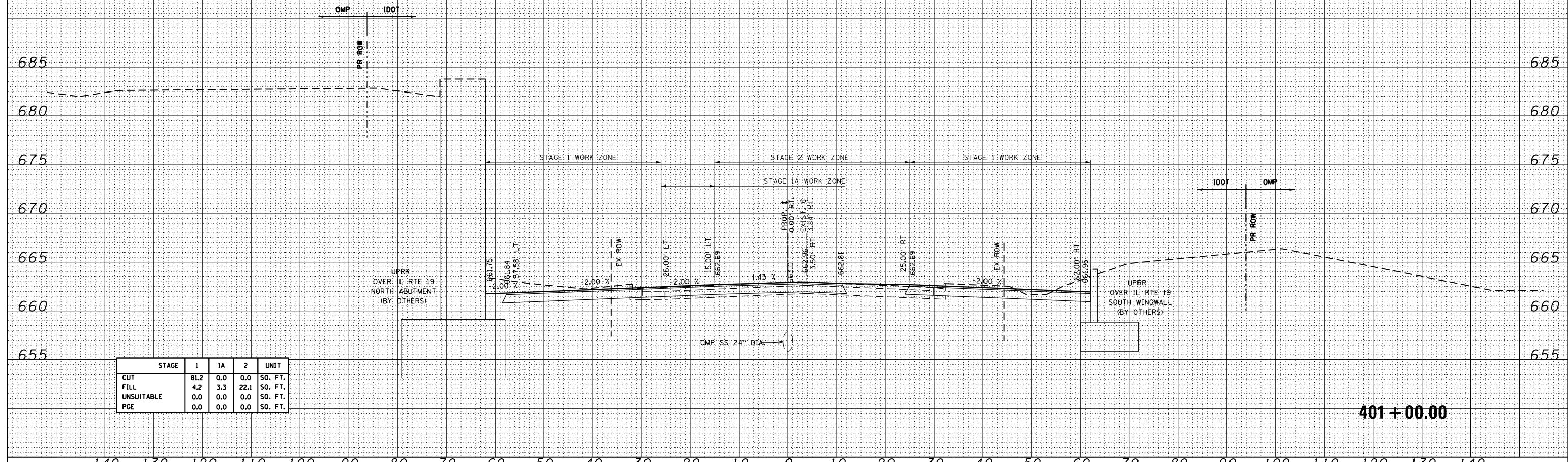
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STAGE	1	1A	2	UNIT
CUT	88.0	0.0	0.0	SO. FT.
FILL	2.7	2.9	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

401+25.00

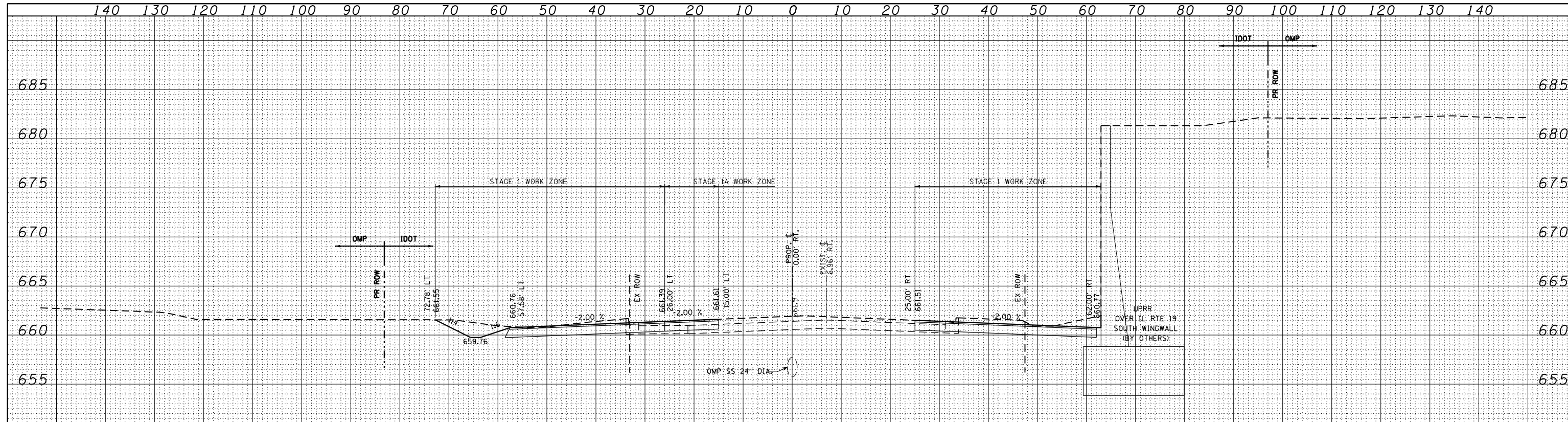


STAGE	1	1A	2	UNIT
CUT	81.2	0.0	0.0	SO. FT.
FILL	4.2	3.3	22.1	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

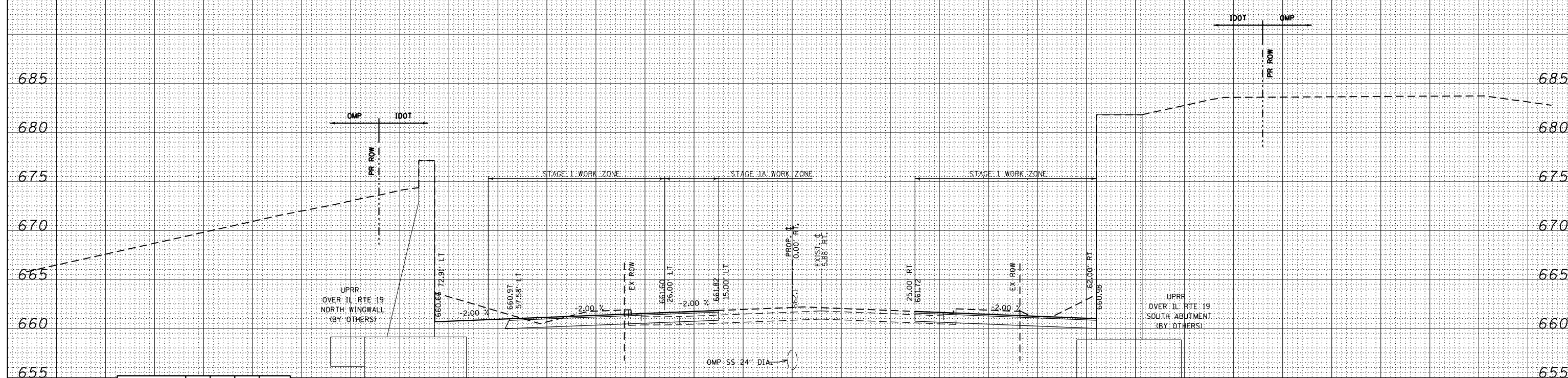
401+00.00

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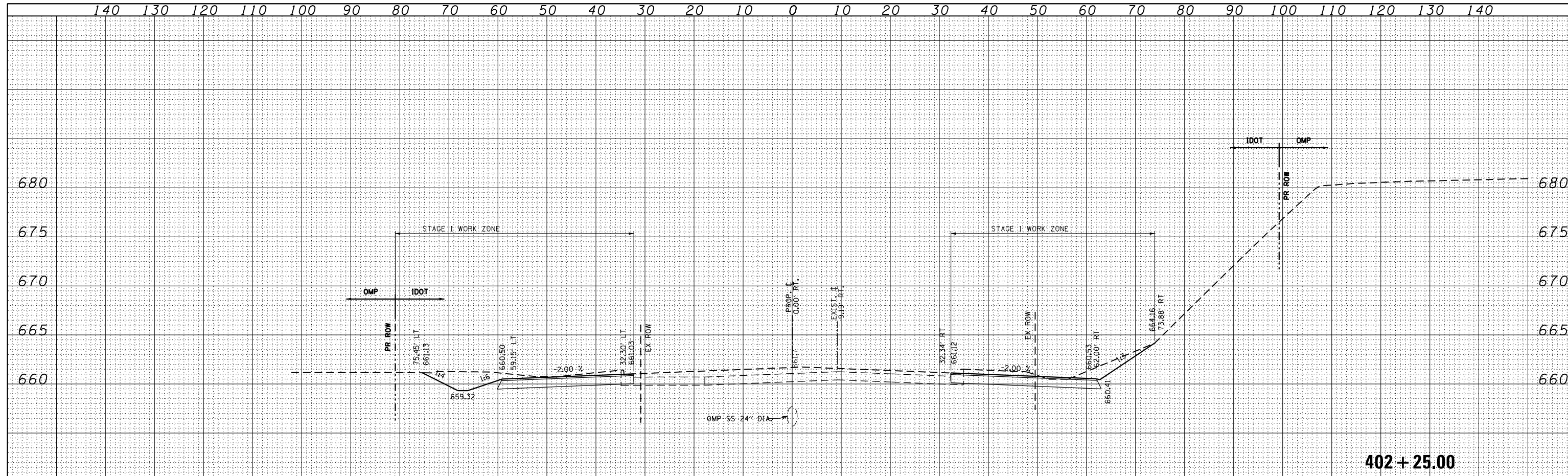
STAGE	1	1A	2	UNIT
CUT	80.1	0.0	0.0	SO. FT.
FILL	3.1	3.6	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.



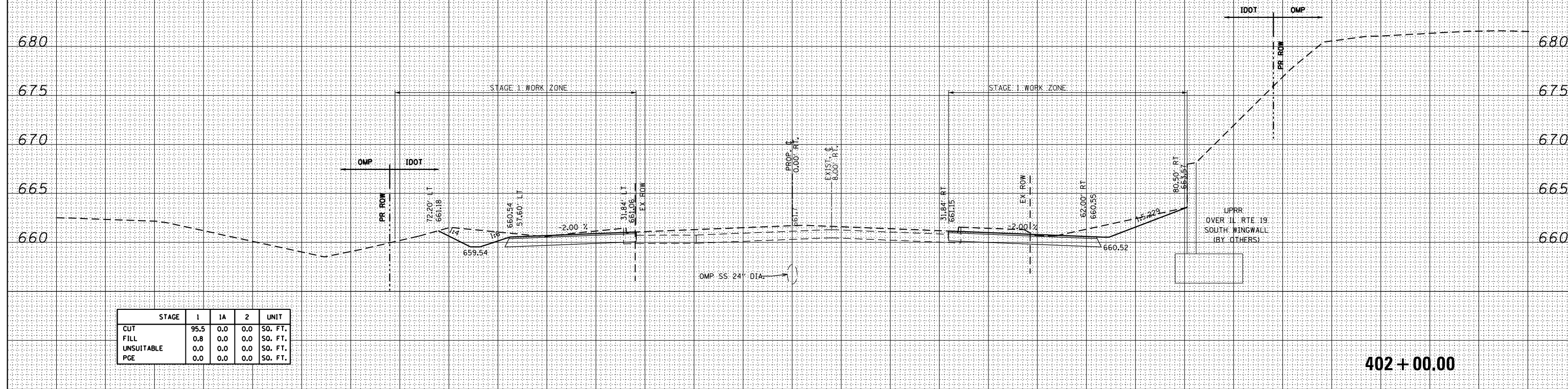
STAGE	1	1A	2	UNIT
CUT	98.3	0.0	0.0	SO. FT.
FILL	2.8	3.3	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

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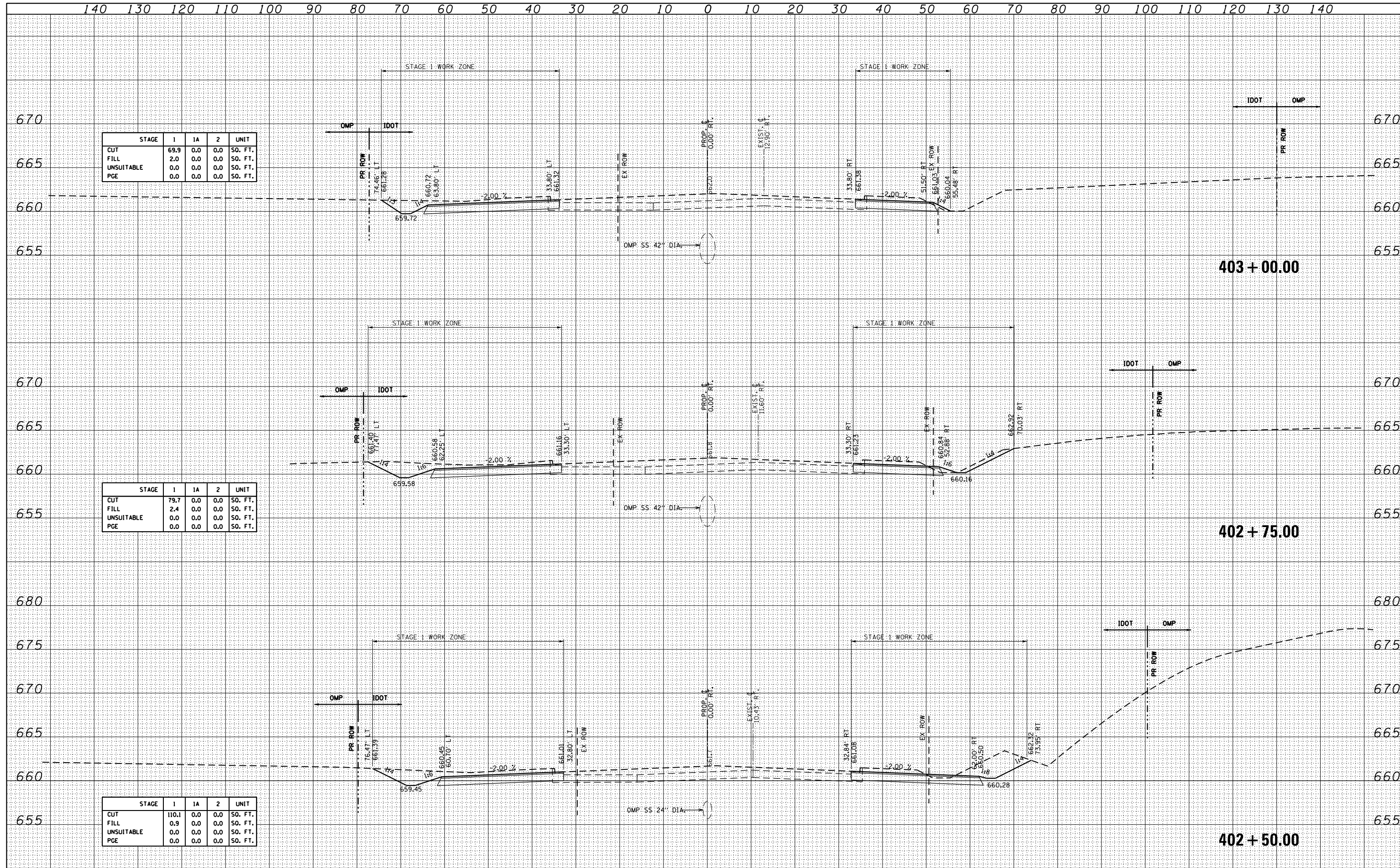
STAGE	1	1A	2	UNIT
CUT	95.1	0.0	0.0	SO. FT.
FILL	0.9	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.



STAGE	1	1A	2	UNIT
CUT	95.5	0.0	0.0	SO. FT.
FILL	0.8	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

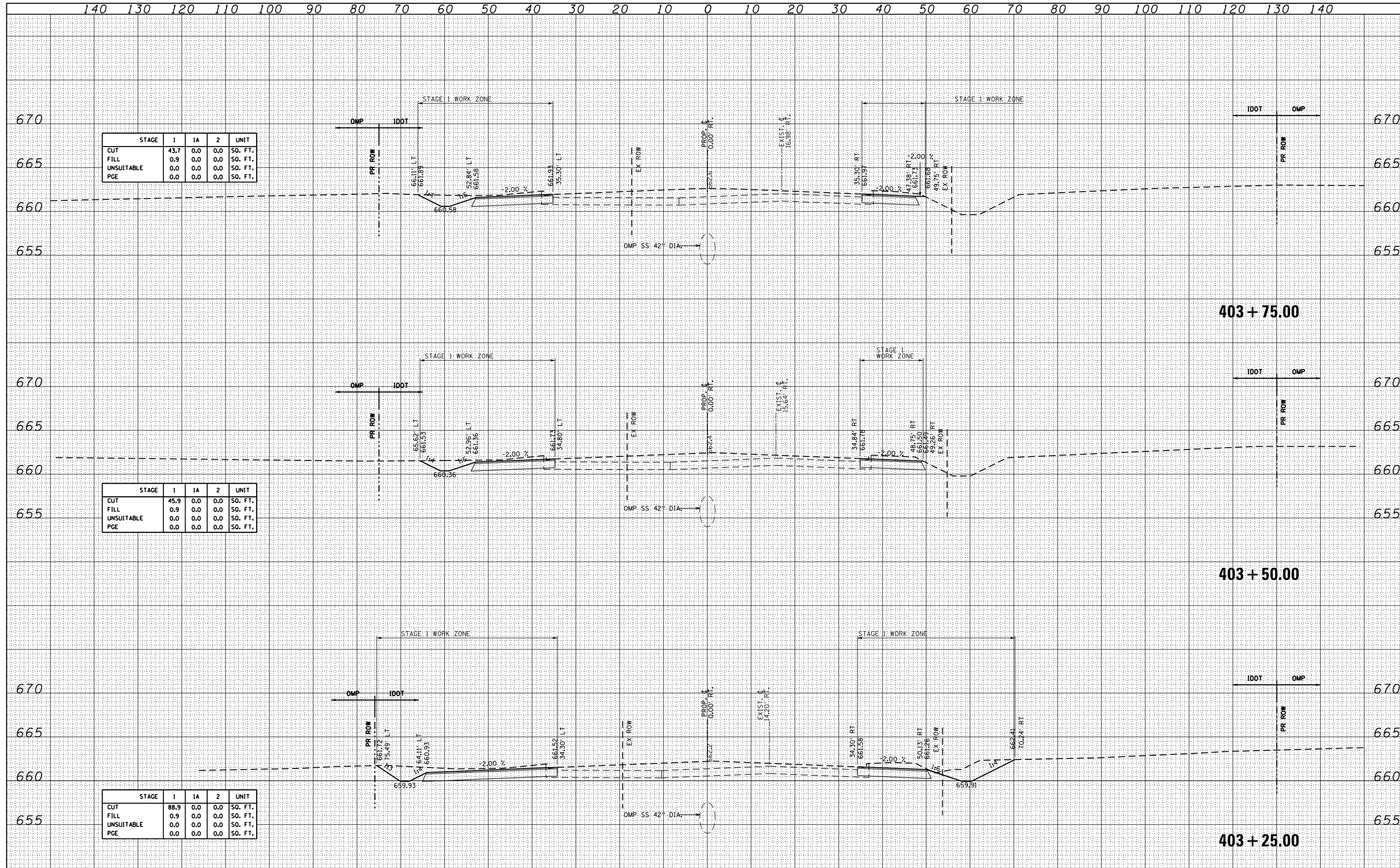
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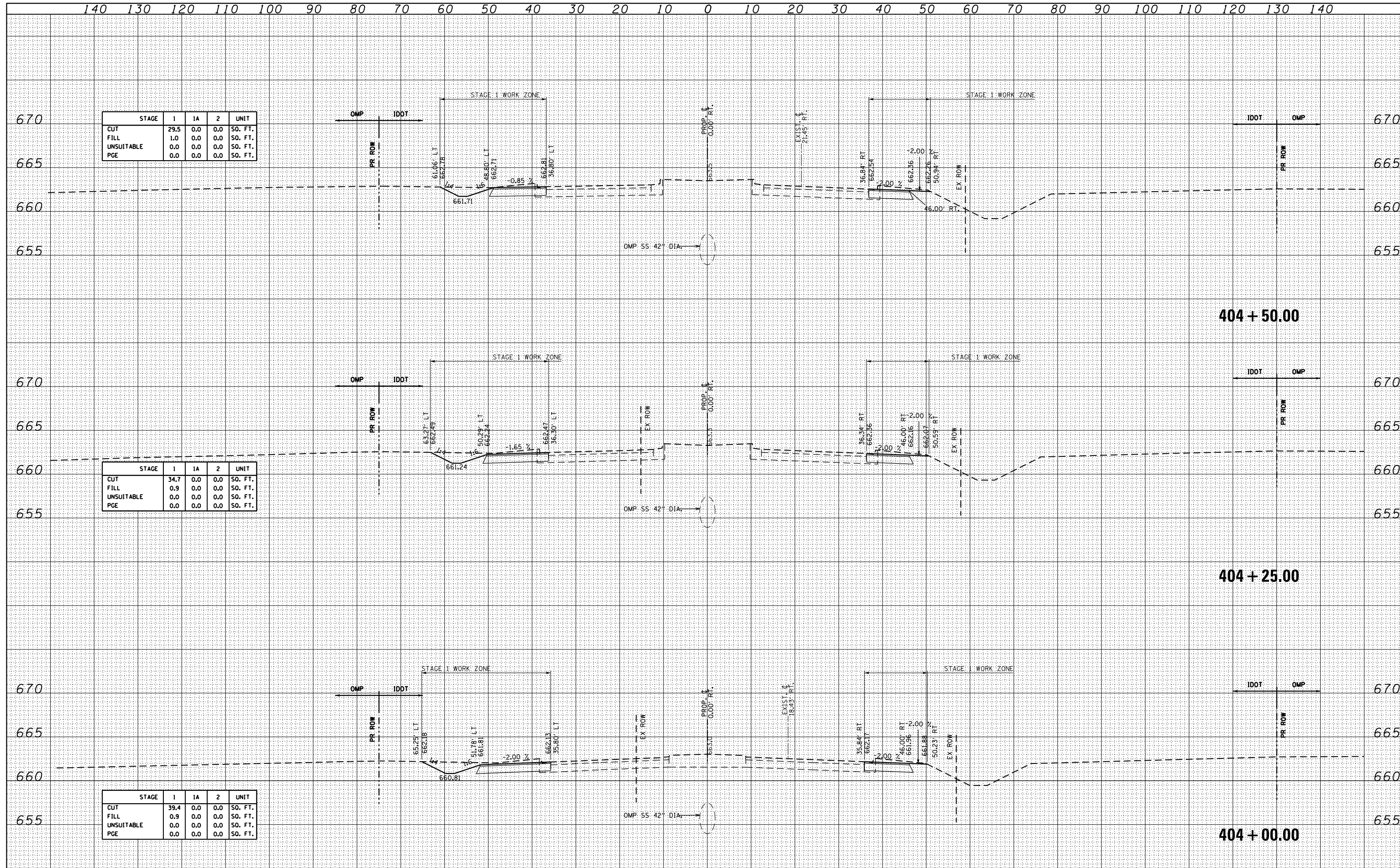
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CUT		43.7	0.0	0.0	SO. FT.
FILL		0.9	0.0	0.0	SO. FT.
UNSUITABLE		0.0	0.0	0.0	SO. FT.
PGE		0.0	0.0	0.0	SO. FT.

	STAGE	1	1A	2	UNIT
CUT		45.9	0.0	0.0	SO. FT.
FILL		0.9	0.0	0.0	SO. FT.
UNSUITABLE		0.0	0.0	0.0	SO. FT.
PGE		0.0	0.0	0.0	SO. FT.

	STAGE	1	1A	2	UNIT
CUT		88.9	0.0	0.0	SO. FT.
FILL		0.9	0.0	0.0	SO. FT.
UNSUITABLE		0.0	0.0	0.0	SO. FT.
PGE		0.0	0.0	0.0	SO. FT.

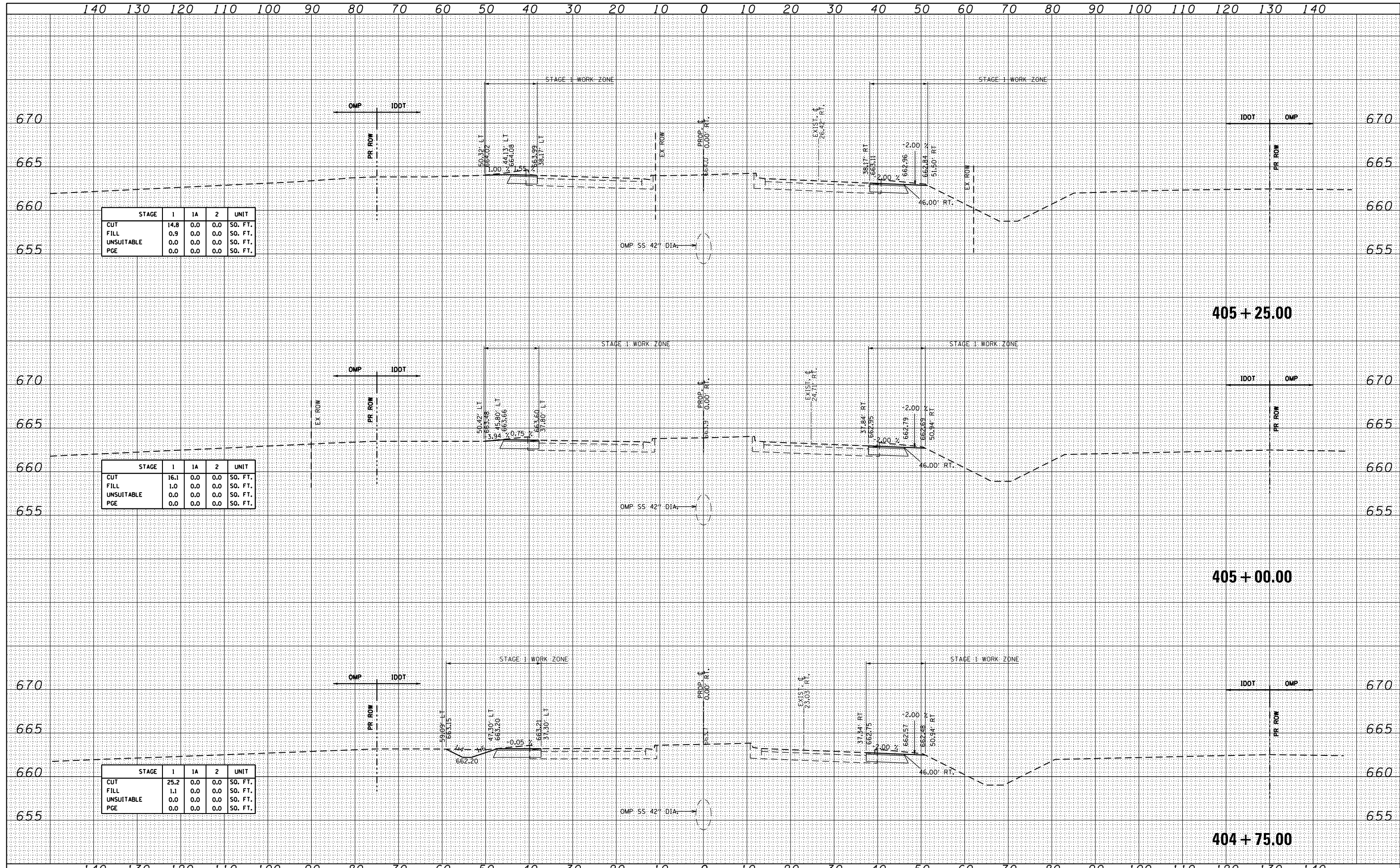
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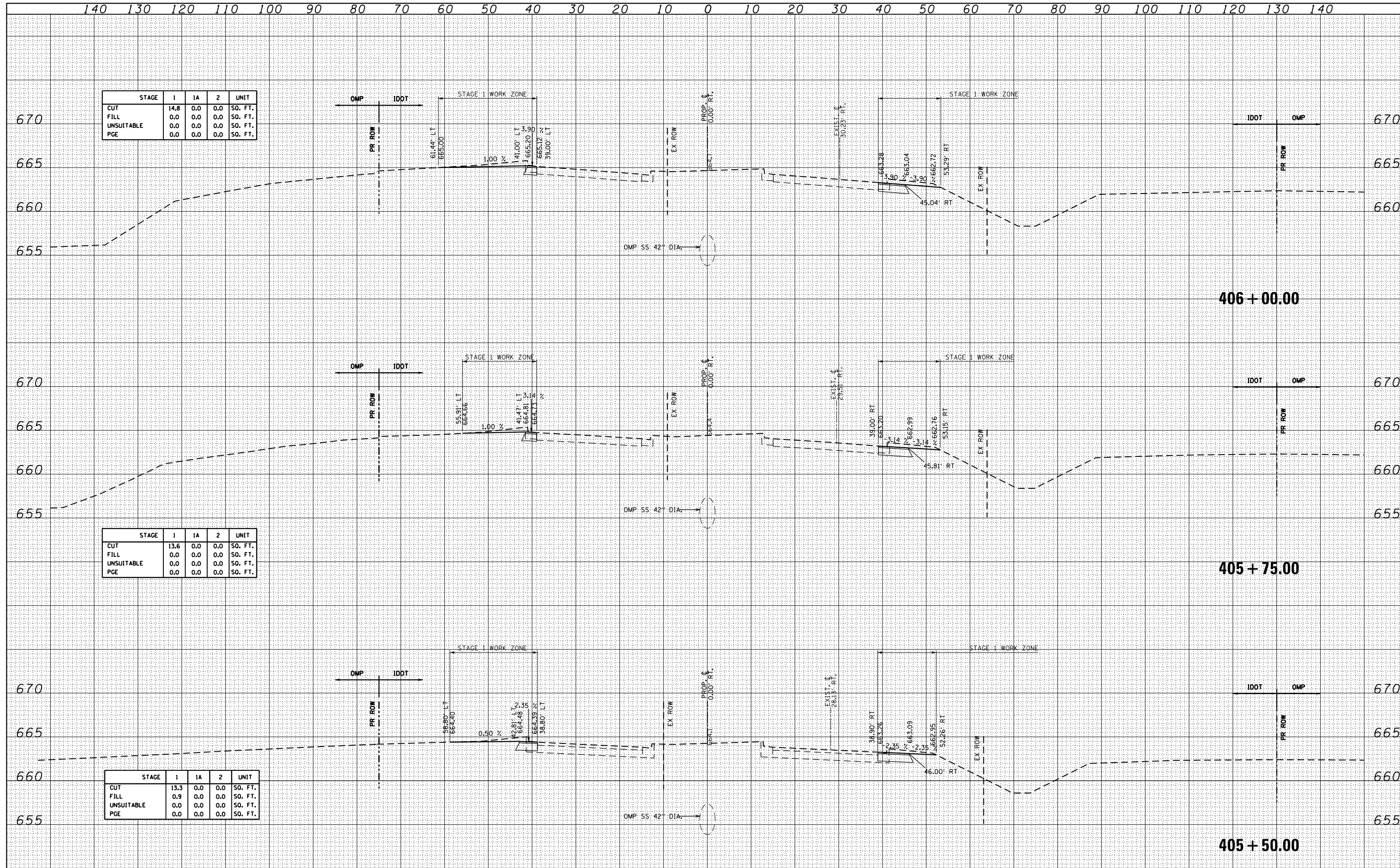
STAGE	1	1A	2	UNIT
CUT	14.8	0.0	0.0	SO. FT.
FILL	0.9	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

STAGE	1	1A	2	UNIT
CUT	16.1	0.0	0.0	SO. FT.
FILL	1.0	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

STAGE	1	1A	2	UNIT
CUT	25.2	0.0	0.0	SO. FT.
FILL	1.1	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

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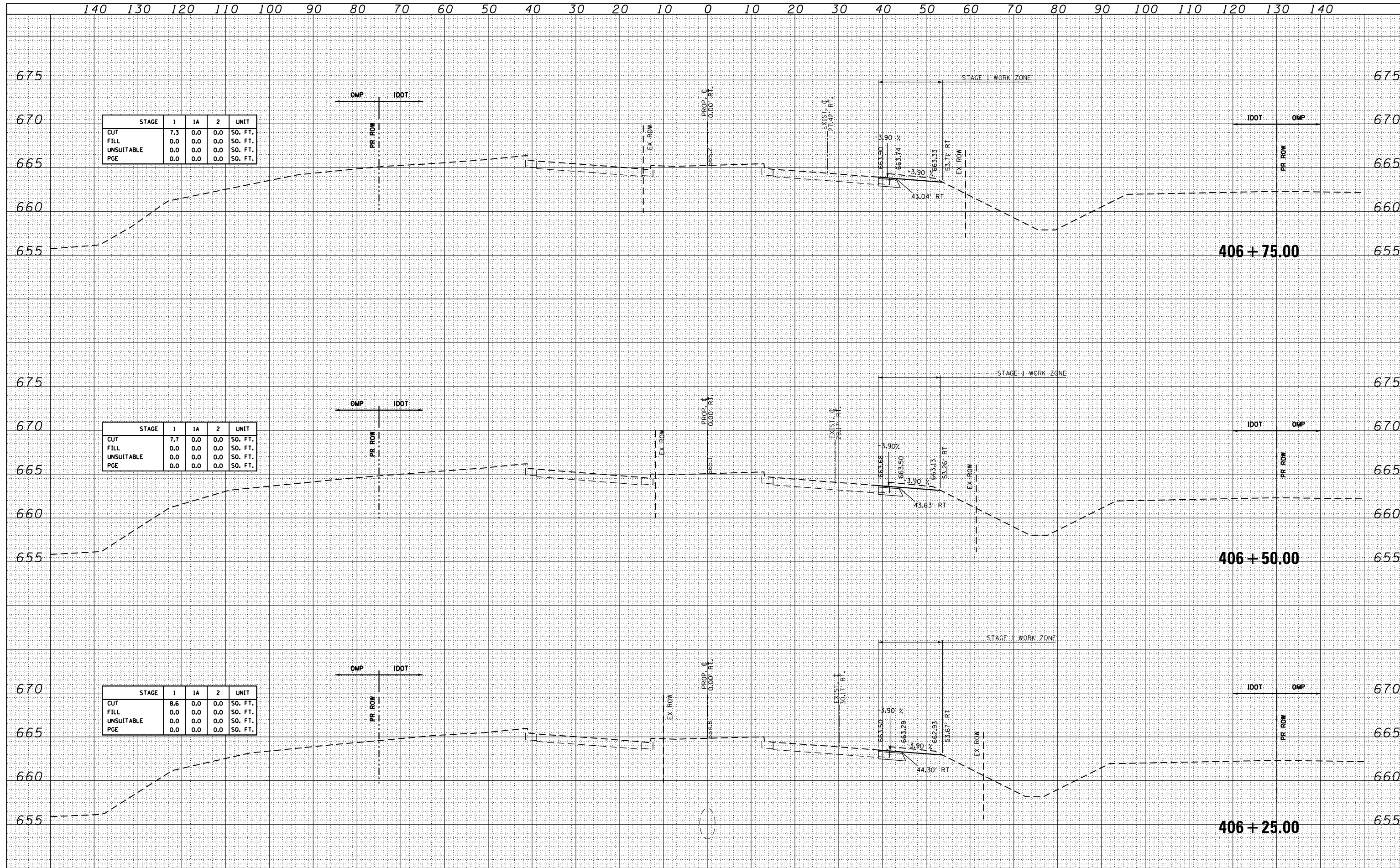
STAGE	1	1A	2	UNIT
CUT	14.8	0.0	0.0	SO. FT.
FILL	0.0	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

STAGE	1	1A	2	UNIT
CUT	13.6	0.0	0.0	SO. FT.
FILL	0.0	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

STAGE	1	1A	2	UNIT
CUT	13.3	0.0	0.0	SO. FT.
FILL	0.9	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

DATE	
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BY	
ORIGINAL SURVEY	
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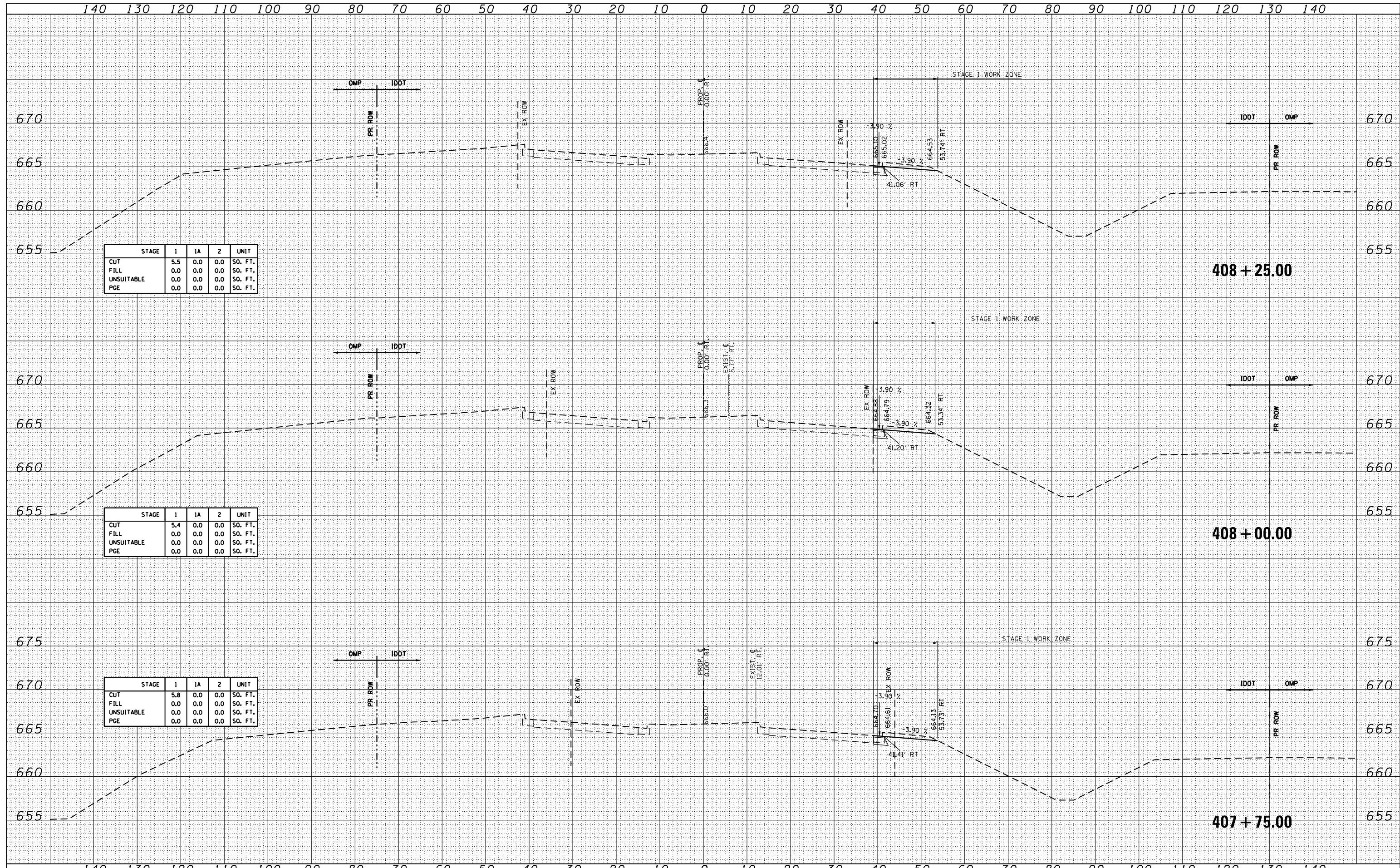
STAGE	1	1A	2	UNIT
CUT	7.3	0.0	0.0	SO. FT.
FILL	0.0	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

STAGE	1	1A	2	UNIT
CUT	7.7	0.0	0.0	SO. FT.
FILL	0.0	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

STAGE	1	1A	2	UNIT
CUT	8.6	0.0	0.0	SO. FT.
FILL	0.0	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

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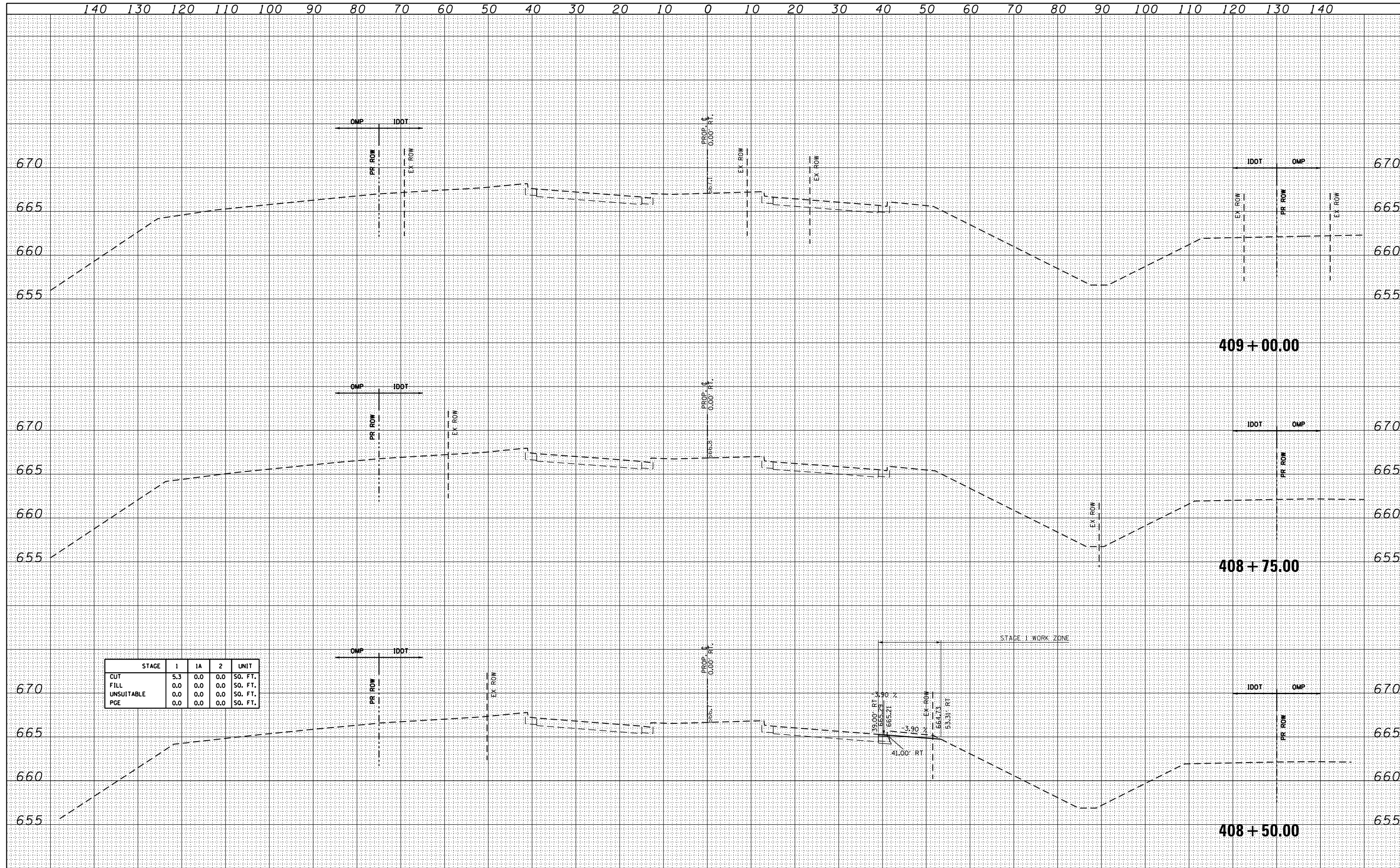
STAGE	1	1A	2	UNIT
CUT	5.5	0.0	0.0	SQ. FT.
FILL	0.0	0.0	0.0	SQ. FT.
UNSUITABLE	0.0	0.0	0.0	SQ. FT.
PGE	0.0	0.0	0.0	SQ. FT.

STAGE	1	1A	2	UNIT
CUT	5.4	0.0	0.0	SQ. FT.
FILL	0.0	0.0	0.0	SQ. FT.
UNSUITABLE	0.0	0.0	0.0	SQ. FT.
PGE	0.0	0.0	0.0	SQ. FT.

STAGE	1	1A	2	UNIT
CUT	5.8	0.0	0.0	SQ. FT.
FILL	0.0	0.0	0.0	SQ. FT.
UNSUITABLE	0.0	0.0	0.0	SQ. FT.
PGE	0.0	0.0	0.0	SQ. FT.

DATE	
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FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
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ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED



STAGE	1	1A	2	UNIT
CUT	5.3	0.0	0.0	SO. FT.
FILL	0.0	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.



USER NAME = user	DESIGNED - MPK	REVISED -
	DRAWN - ML	REVISED -
PLOT SCALE = *SCALE*	CHECKED - LGP	REVISED -
PLOT DATE = 11/2/2012	DATE - 11-02-2012	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

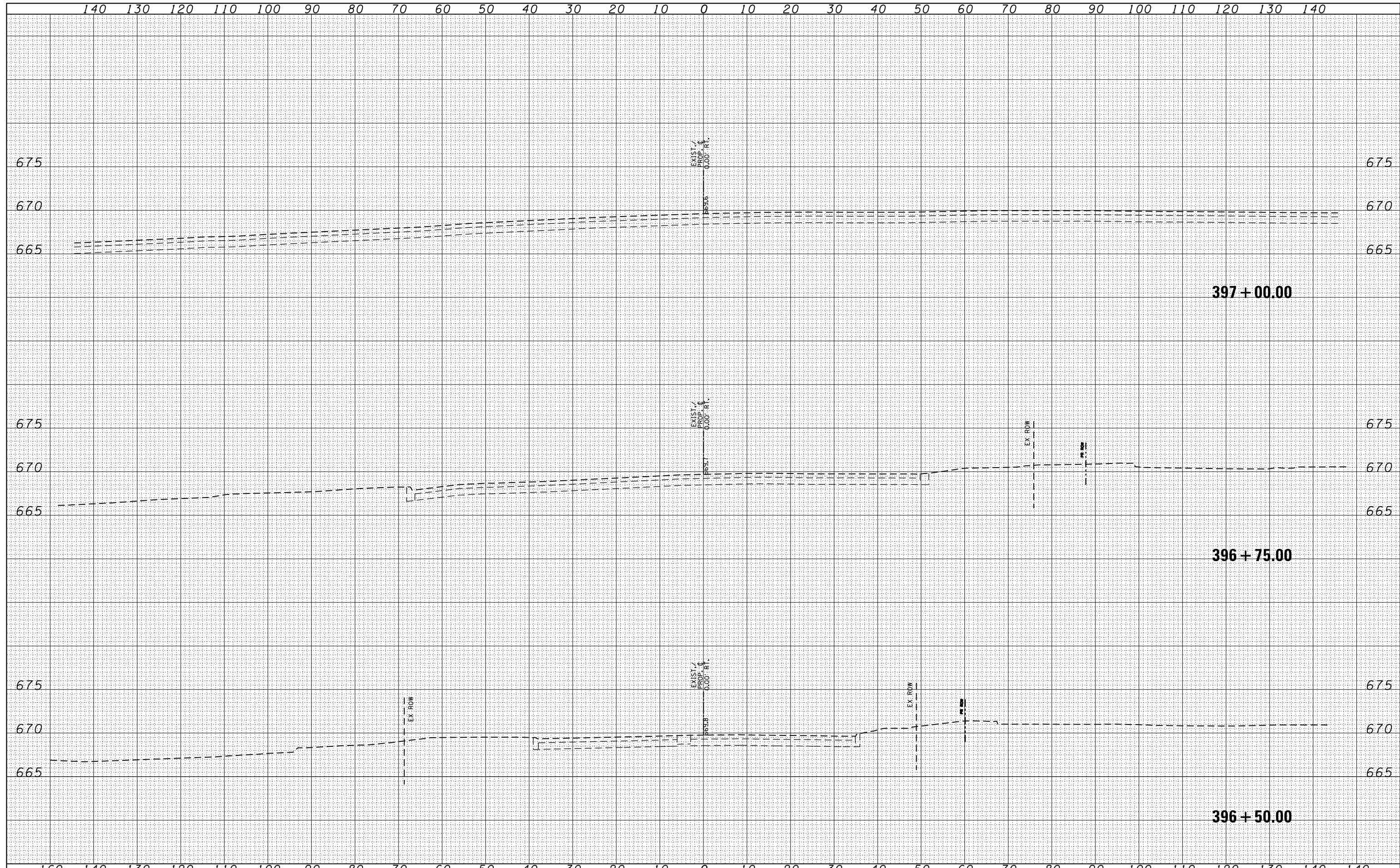
**IRVING PARK (IL 19)
CROSS SECTIONS - STAGES 1 THROUGH 2**

SCALE: 1"=10'H; 1"=5'V SHEET NO. 19 OF 19 SHEETS STA. 408+50.00 TO STA. 409+25.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32VB		388	278
CONTRACT NO. 60W01			ILLINOIS FED. AID PROJECT	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
FINAL SURVEY	
NOTE BOOK	
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DATE	
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NOTE BOOK	
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USER NAME - user	DESIGNED - MPK	REVISED -
	DRAWN - ML	REVISED -
PLOT SCALE - *SCALE*	CHECKED - LGP	REVISED -
PLOT DATE - 11/2/2012	DATE - 11-02-2012	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

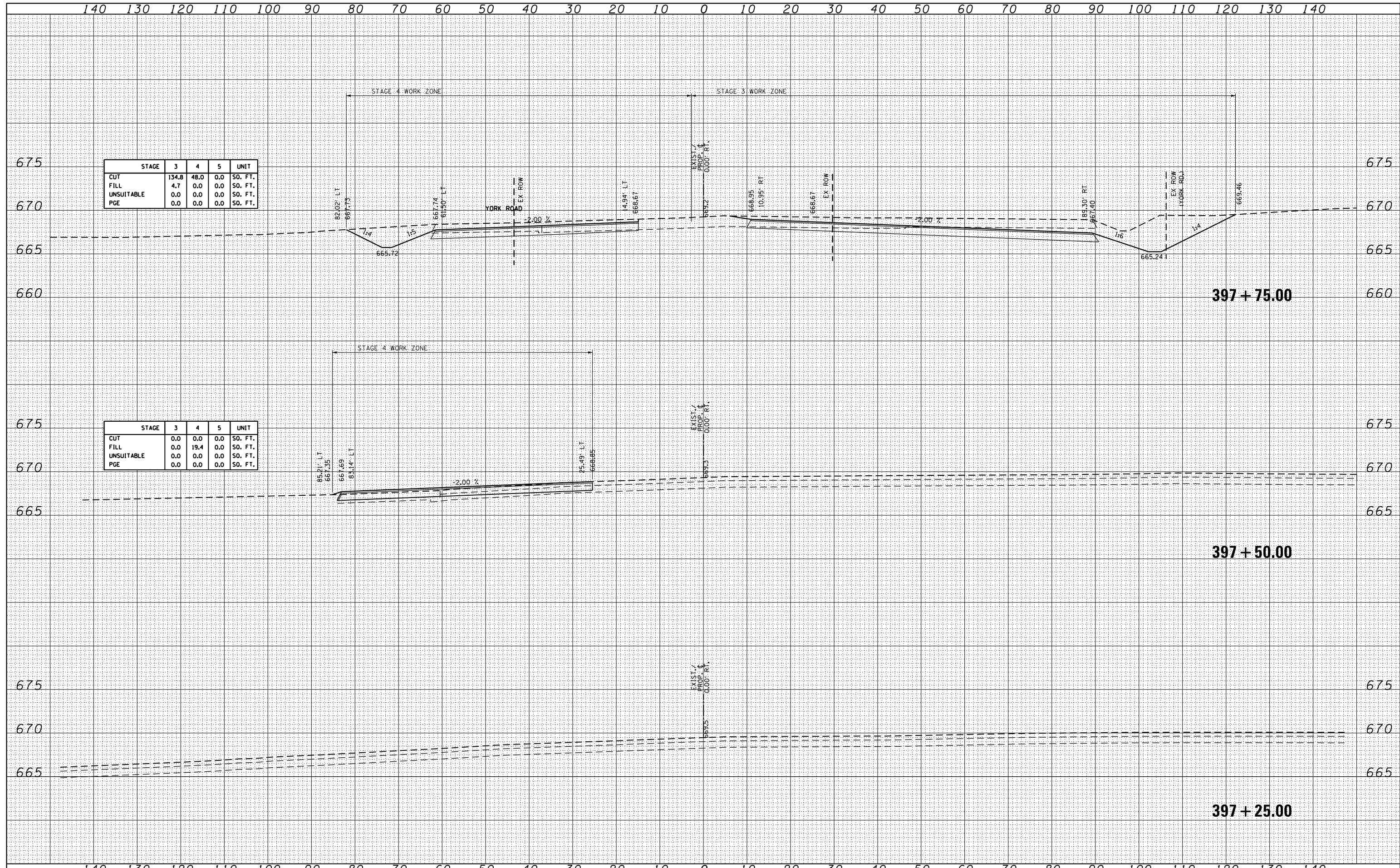
**IRVING PARK (IL 19)
CROSS SECTIONS- STAGES 3 THROUGH 5**

SCALE: 1"=10'H; 1"=5'V SHEET NO. 1 OF 19 SHEETS STA. 396+50.00 TO STA. 397+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32VB	DUPAGE	388	279
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				

DATE	
BY	
SURVEYED	
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TEMPLATE	
NOTE BOOK	
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NOTE BOOK	
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STAGE	3	4	5	UNIT
CUT	134.8	48.0	0.0	SO. FT.
FILL	4.7	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

STAGE	3	4	5	UNIT
CUT	0.0	0.0	0.0	SO. FT.
FILL	0.0	19.4	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

USER NAME = user	DESIGNED - MPK	REVISED -
	DRAWN - ML	REVISED -
PLOT SCALE = *SCALE*	CHECKED - LGP	REVISED -
PLOT DATE = 11/2/2012	DATE - 11-02-2012	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

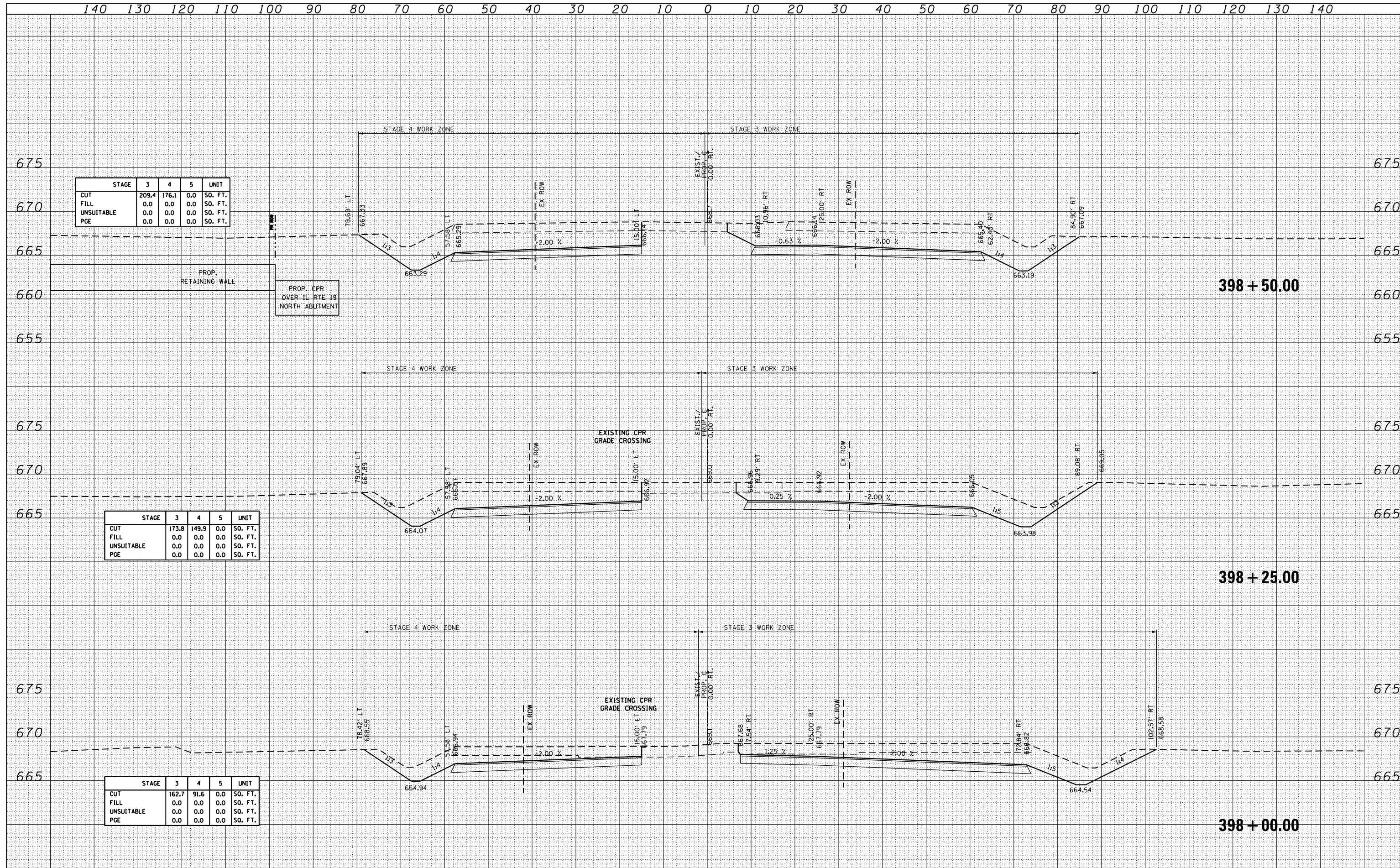
**IRVING PARK (IL 19)
CROSS SECTIONS- STAGES 3 THROUGH 5**

SCALE: 1"=10'H; 1"=5'V SHEET NO. 2 OF 19 SHEETS STA. 397+25.00 TO STA. 397+75.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32VB	DUPAGE	388	280
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK NO.	PLOTTED
	TEMPLATE
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK NO.	PLOTTED
	TEMPLATE
	AREAS CHECKED



398 + 50.00

398 + 25.00

398 + 00.00

STAGE	3	4	5	UNIT
CUT	209.4	176.1	0.0	SO. FT.
FILL	0.0	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

STAGE	3	4	5	UNIT
CUT	173.8	149.9	0.0	SO. FT.
FILL	0.0	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

STAGE	3	4	5	UNIT
CUT	162.7	91.6	0.0	SO. FT.
FILL	0.0	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IRVING PARK (IL 19)
CROSS SECTIONS- STAGES 3 THROUGH 5

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32VB	DUPAGE	388	281
CONTRACT NO. 60W01				

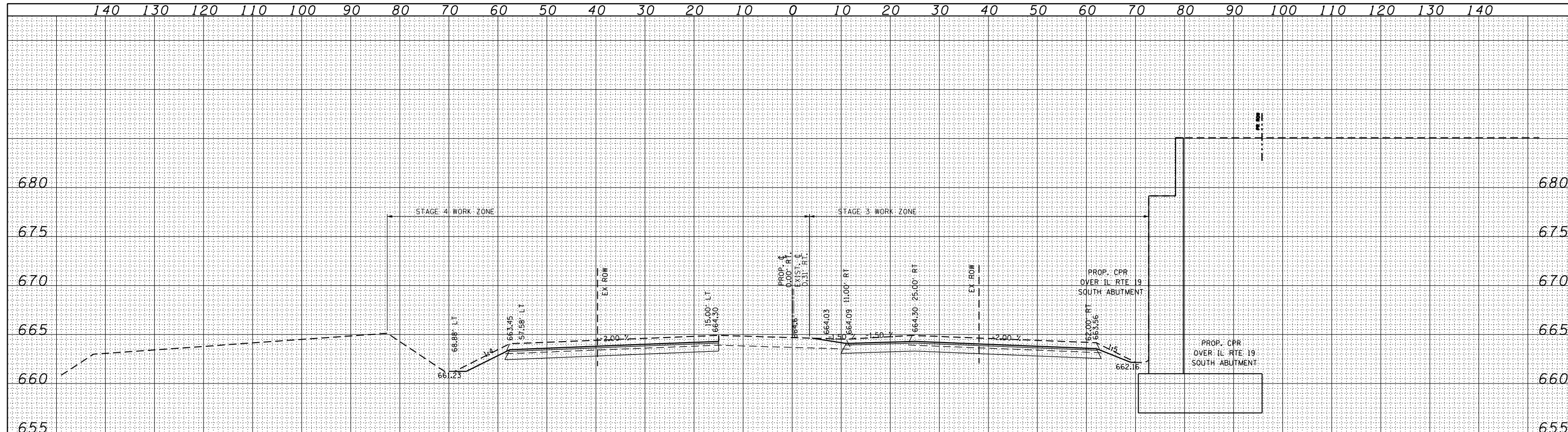
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	DRAWN - ML	REVISED -
PLOT SCALE = *SCALE*	CHECKED - LGP	REVISED -
PLOT DATE = 11/2/2012	DATE - 11-02-2012	REVISED -

SCALE: 1"=10'H; 1"=5'V SHEET NO. 3 OF 19 SHEETS STA. 398+00.00 TO STA. 398+50.00

ILLINOIS FED. AID PROJECT

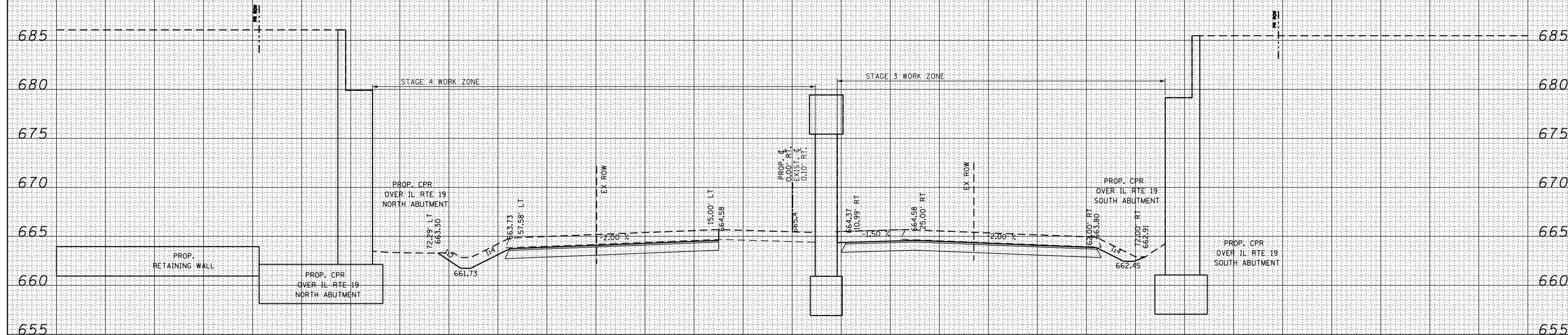
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	STAGE 3	4	5	UNIT
CUT	45.6	32.2	0.0	SO. FT.
FILL	5.3	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

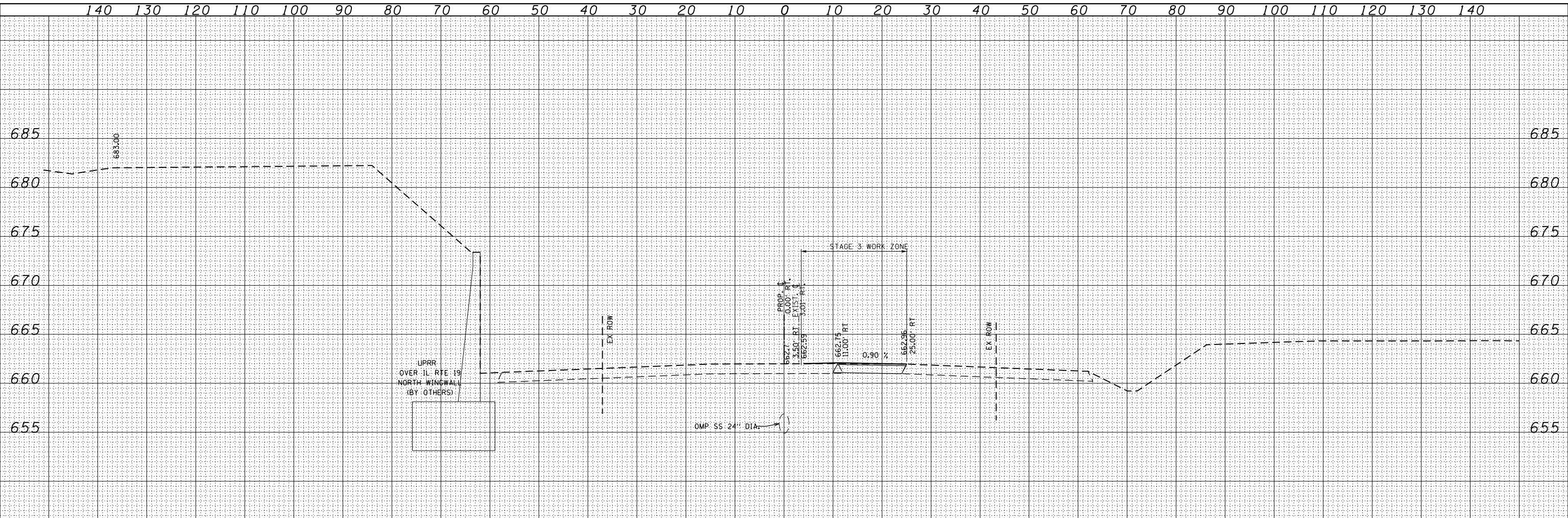
399 + 50.00



	STAGE 3	4	5	UNIT
CUT	77.5	61.5	0.0	SO. FT.
FILL	0.0	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

399 + 25.00

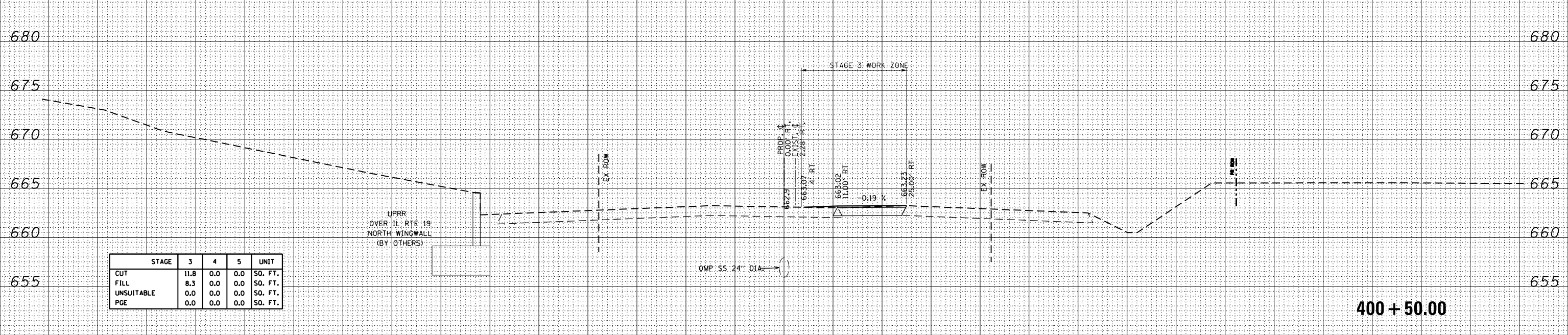
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FINISHED SURVEY	
PLOTTED	
NOTE BOOK	
AREAS CHECKED	
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	STAGE 3	4	5	UNIT
CUT	12.5	0.0	0.0	SO. FT.
FILL	7.8	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

400+75.00

DATE	
BY	
ORIGINAL SURVEY	
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NOTE BOOK	
AREAS CHECKED	
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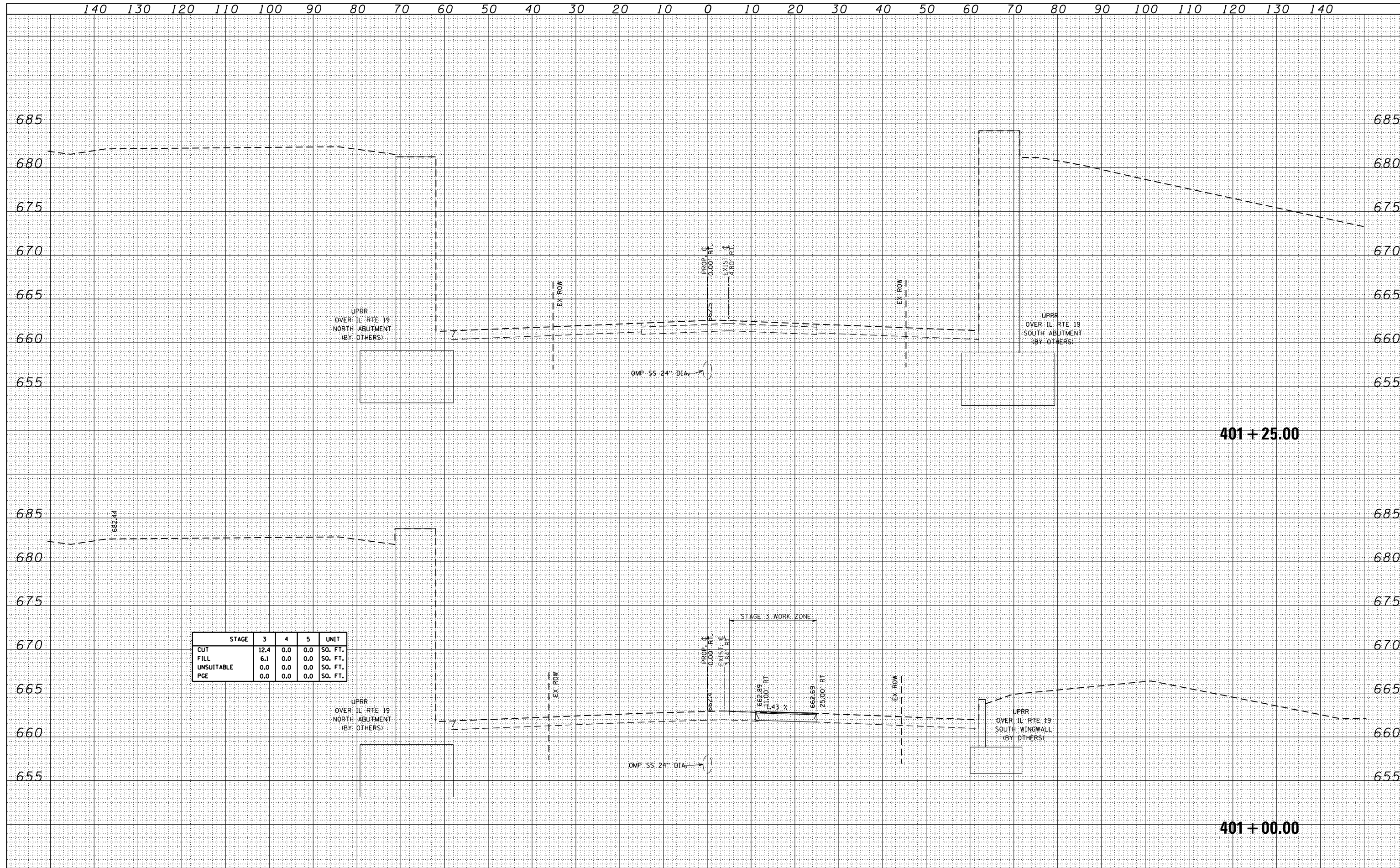


	STAGE 3	4	5	UNIT
CUT	11.8	0.0	0.0	SO. FT.
FILL	8.3	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

400+50.00

DATE	
BY	
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	STAGE 3	4	5	UNIT
CUT	12.4	0.0	0.0	SO. FT.
FILL	6.1	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	SO. FT.
PGE	0.0	0.0	0.0	SO. FT.

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

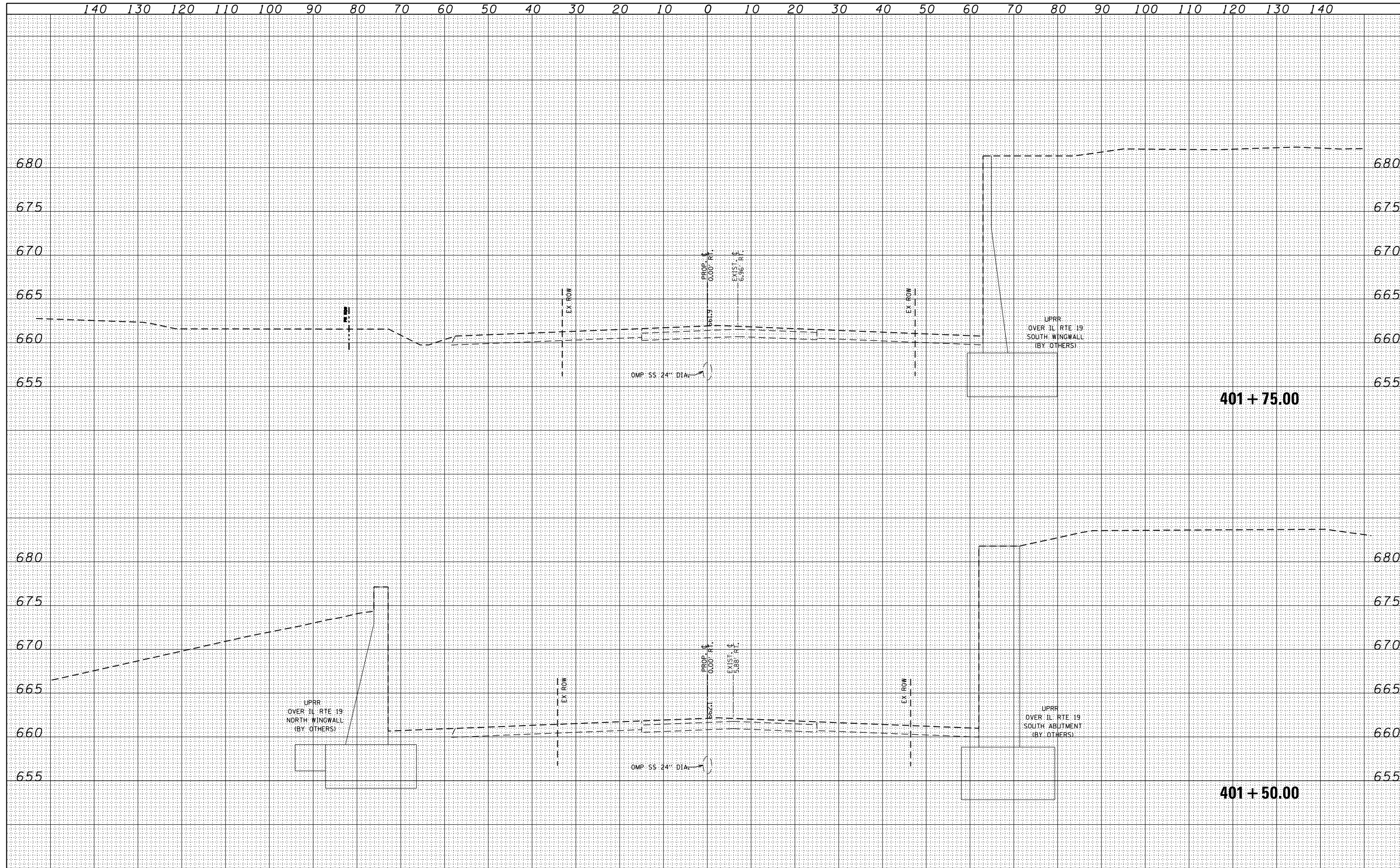
**IRVING PARK (IL 19)
CROSS SECTIONS- STAGES 3 THROUGH 5**

SCALE: 1"=10'H; 1/4"=5'V SHEET NO. 8 OF 19 SHEETS STA. 401+00.00 TO STA. 401+25.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32VB	DUPAGE	388	286
CONTRACT NO. 60W01				

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
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USER NAME	= user
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

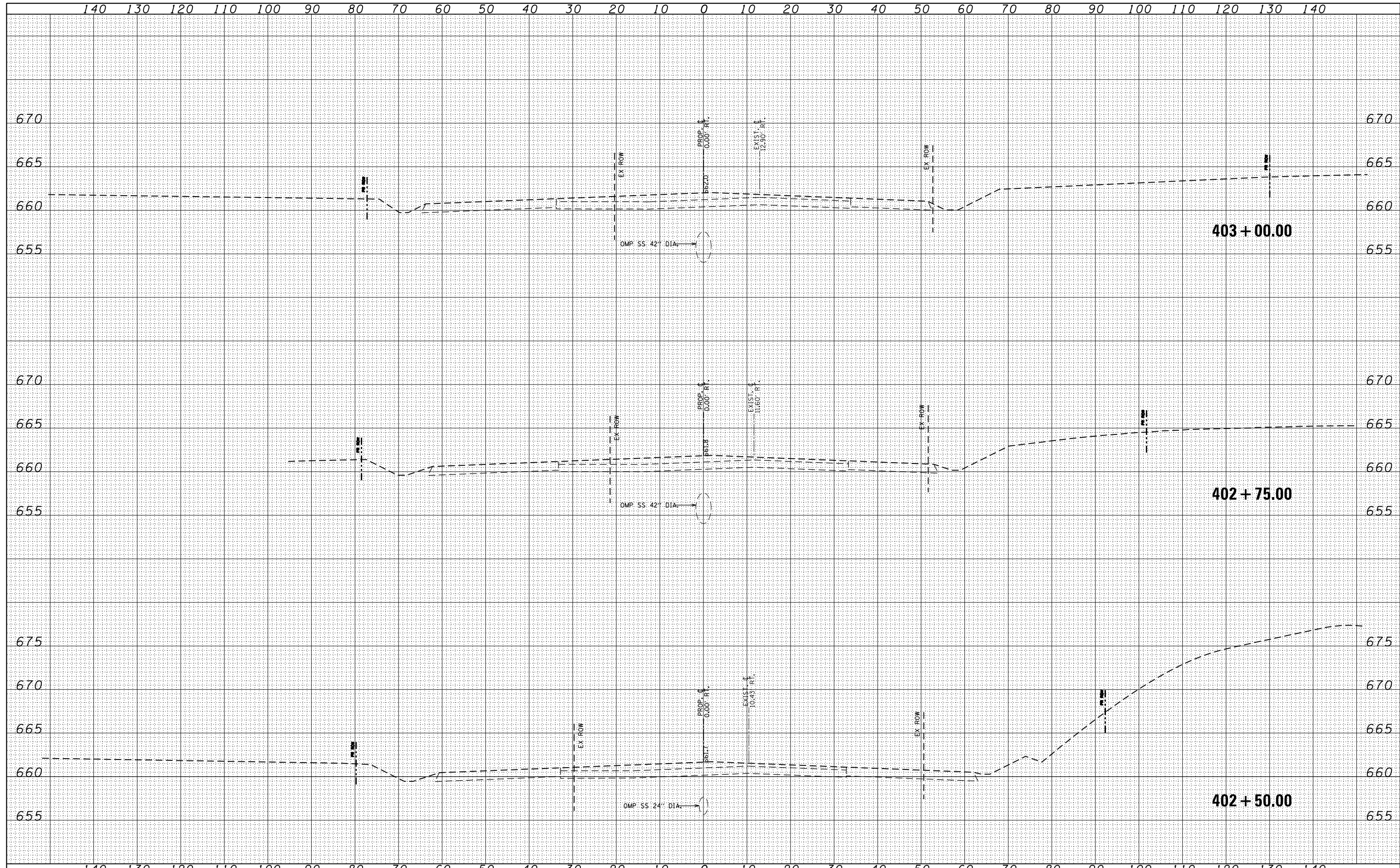
**IRVING PARK (IL 19)
CROSS SECTIONS- STAGES 3 THROUGH 5**

SCALE: 1"=10'H; 1"=5'V SHEET NO. 9 OF 19 SHEETS STA. 401+50.00 TO STA. 401+75.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32VB	DUPAGE	388	287
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				

DATE	
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FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
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DRAWN	- ML
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DATE	- 11-02-2012

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

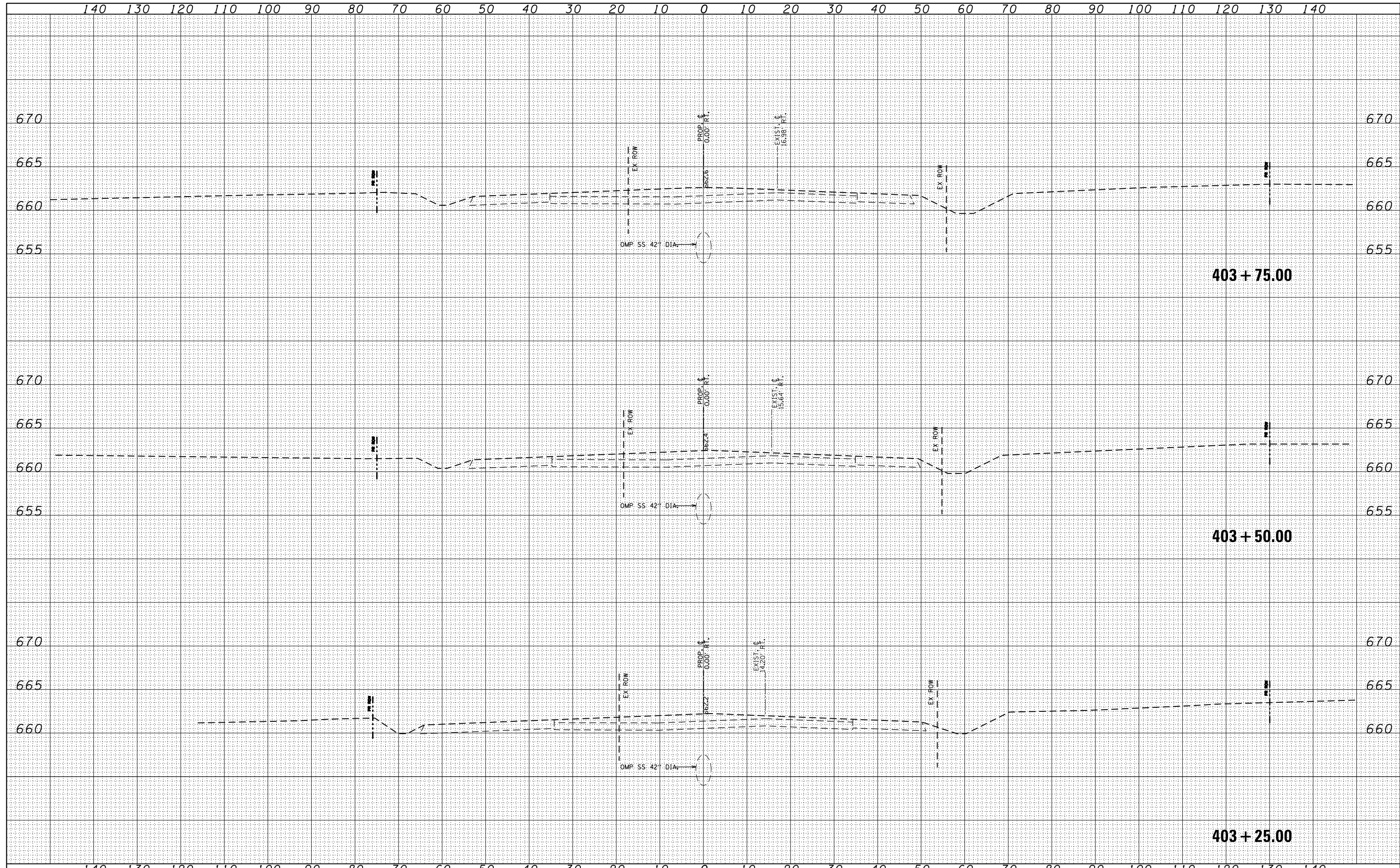
**IRVING PARK (IL 19)
CROSS SECTIONS- STAGES 3 THROUGH 5**

SCALE: 1"=10'H; 1"=5'V SHEET NO. 11 OF 19 SHEETS STA. 402+50.00 TO STA. 403+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32VB	DUPAGE	388	289
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

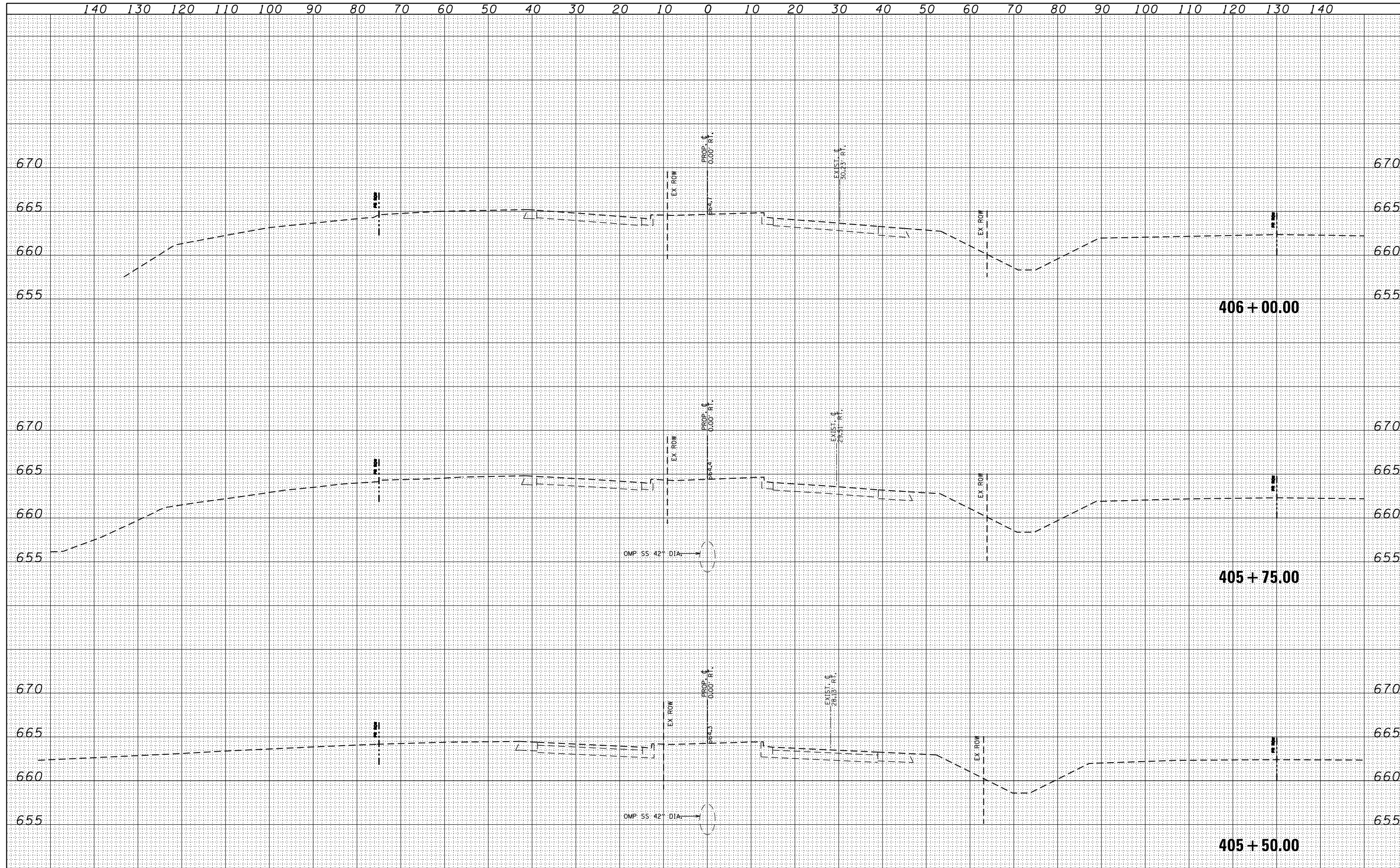
**IRVING PARK (IL 19)
CROSS SECTIONS- STAGES 3 THROUGH 5**

SCALE: 1"=10'H; 1"=5'V SHEET NO. 12 OF 19 SHEETS STA. 403+25.00 TO STA. 403+75.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32VB	DUPAGE	388	290
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				

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FINAL SURVEY	SURVEYED
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

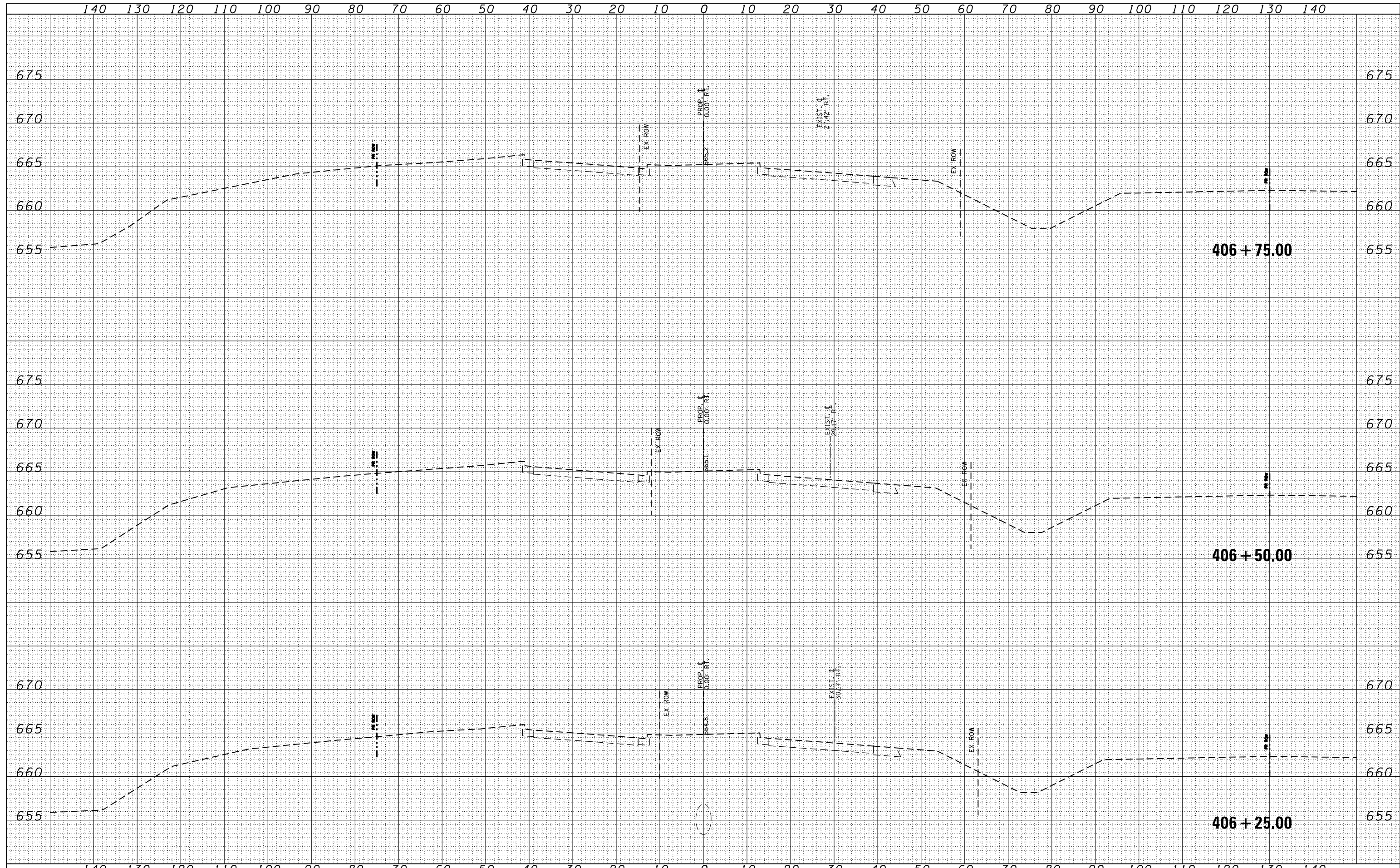
**IRVING PARK (IL 19)
CROSS SECTIONS- STAGES 3 THROUGH 5**

SCALE: 1"=10'H; 1"=5'V SHEET NO. 15 OF 19 SHEETS STA. 405+50.00 TO STA. 406+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32VB	DUPAGE	388	293
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				

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FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
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ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

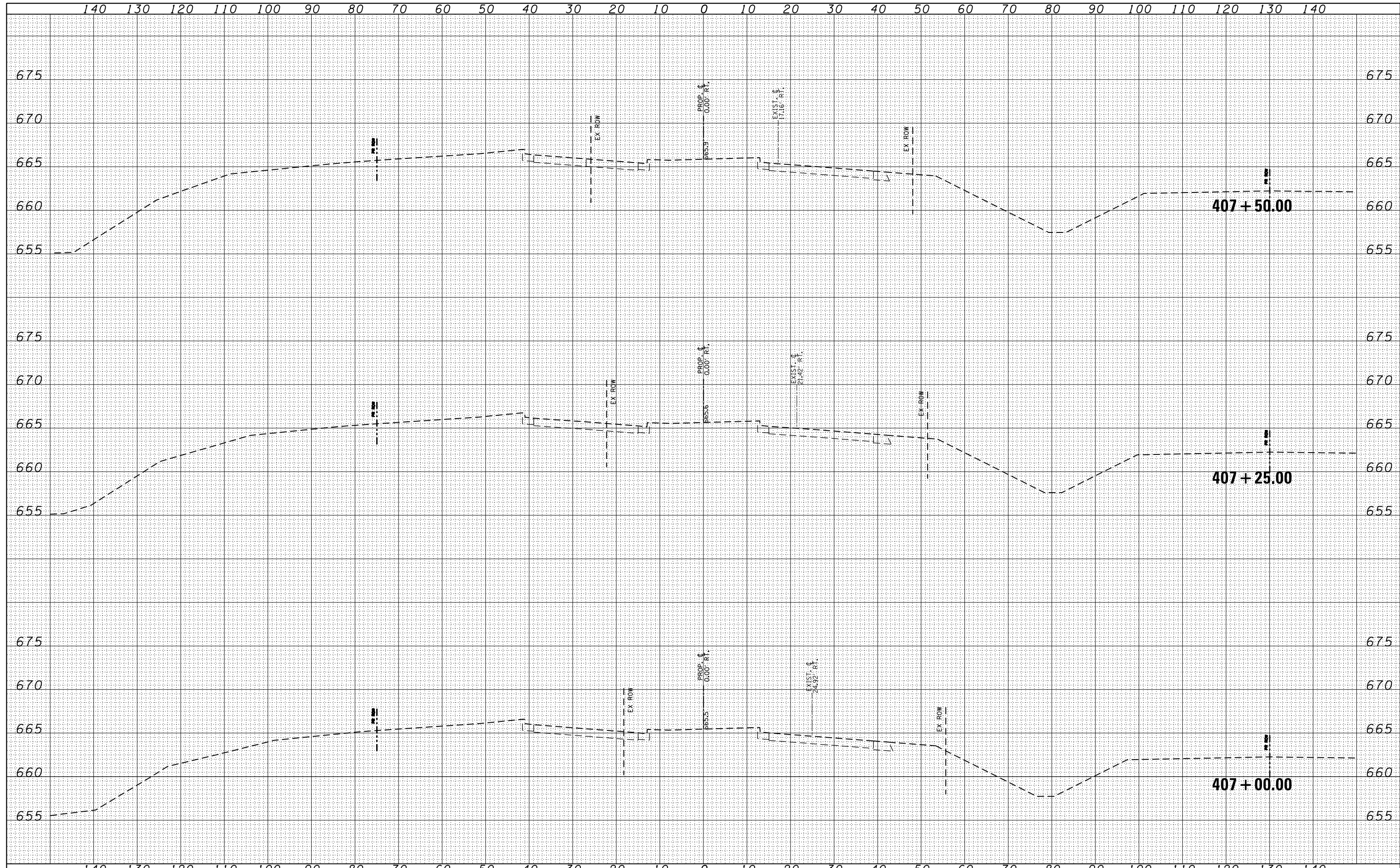
**IRVING PARK (IL 19)
CROSS SECTIONS- STAGES 3 THROUGH 5**

SCALE: 1"=10'H; 1"=5'V SHEET NO. 16 OF 19 SHEETS STA. 406+25.00 TO STA. 406+75.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32VB	DUPAGE	388	294
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				

BY	DATE
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FINAL SURVEY NO.	
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ORIGINAL SURVEY NO.	
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

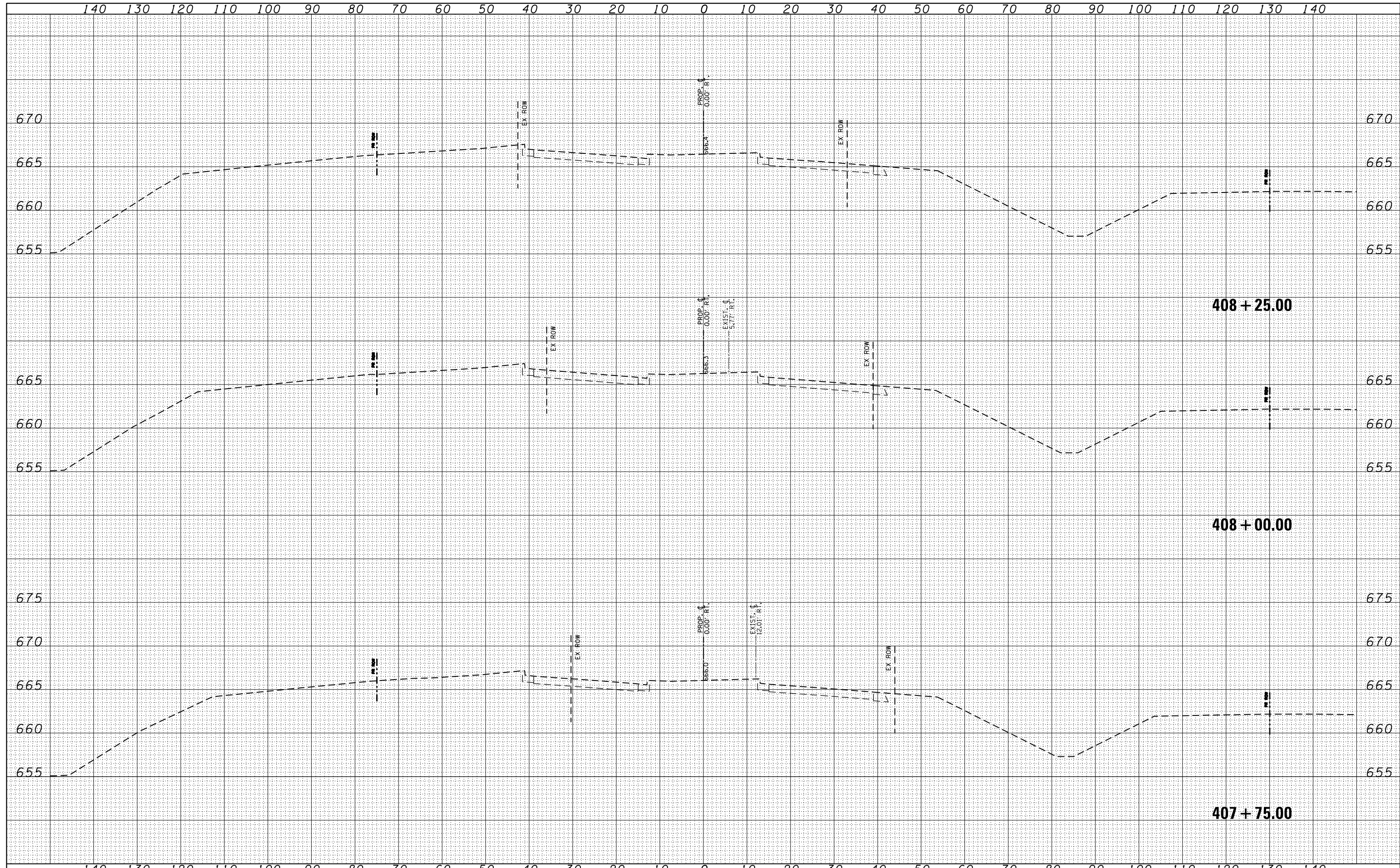
**IRVING PARK (IL 19)
CROSS SECTIONS- STAGES 3 THROUGH 5**

SCALE: 1"=10'H; 1"=5'V SHEET NO. 17 OF 19 SHEETS STA. 407+00.00 TO STA. 407+50.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32VB	DUPAGE	388	295
CONTRACT NO. 60W01				ILLINOIS FED. AID PROJECT

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED

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ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
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USER NAME	= user
DESIGNED	- MPK
DRAWN	- ML
CHECKED	- LGP
DATE	- 11-02-2012

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IRVING PARK (IL 19)
CROSS SECTIONS- STAGES 3 THROUGH 5**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32VB	DUPAGE	388	296
CONTRACT NO. 60W01				

SCALE: 1"=10'H; 1"=5'V SHEET NO. 18 OF 19 SHEETS STA. 407+75.00 TO STA. 408+25.00

ILLINOIS FED. AID PROJECT

EARTHWORK SCHEDULE SUMMARY

CANADIAN PACIFIC RAILWAY EARTHWORK SUMMARY								
CANADIAN PACIFIC RAILWAY			REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	EARTH EXCAVATION	FURNISHED EXCAVATION	FURNISHED EXCAVATION, SPECIAL	SUBBALLAST	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL ASSUMED LIMITS WITH ASSOCIATED SOIL BORINGS
STA.	to	STA.	(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)	
10271+95.48		10272+00.00	1.3	0.0	0.9	0.0	0.5	
10272+00.00		10273+00.00	33.8	0.0	36.5	0.0	19.1	
10273+00.00		10274+00.00	51.2	1.6	69.8	0.0	39.5	
10274+00.00		10275+00.00	81.8	2.3	201.0	0.0	64.0	
10275+00.00		10276+00.00	97.0	11.0	235.9	0.0	84.9	
10276+00.00		10277+00.00	77.2	30.9	115.6	-17.6	102.1	
10277+00.00		10278+00.00	49.0	32.7	55.7	-24.7	118.5	
10278+00.00		10279+00.00	34.7	28.8	24.0	-18.9	139.8	
10279+00.00		10280+00.00	30.9	51.8	9.0	-40.6	170.4	
10280+00.00		10281+00.00	30.9	45.6	15.9	-25.0	190.2	
10281+00.00		10282+00.00	31.0	10.5	21.8	30.7	194.6	
10282+00.00		10283+00.00	30.4	0.0	142.8	98.3	210.8	
10283+00.00		10284+00.00	79.5	16.4	330.5	164.0	225.8	
10284+00.00		10285+00.00	117.3	35.5	302.8	148.8	227.1	
10285+00.00		10286+00.00	91.9	37.3	226.6	119.3	227.3	
10286+00.00		10287+00.00	70.3	55.9	247.4	114.3	226.7	
10287+00.00		10288+00.00	52.4	83.0	276.9	70.5	225.1	
10288+00.00		10289+00.00	40.1	74.9	290.0	19.4	222.6	
10289+00.00		10290+00.00	32.1	35.0	195.7	0.0	219.3	
10290+00.00		10291+00.00	26.9	24.8	80.2	0.0	215.1	
10291+00.00		10292+00.00	42.1	19.5	56.2	0.0	210.2	
10292+00.00		10293+00.00	70.1	5.7	34.8	0.0	206.2	
10293+00.00		10294+00.00	84.2	8.2	12.5	0.0	204.9	
10294+00.00		10295+00.00	46.6	39.2	779.9	0.0	245.5	
10295+00.00		10295+48.42	2.8	37.8	1124.3	0.0	138.8	
10295+48.42		10296+00.00	0.7	45.5	2032.6	0.0	126.9	
10296+00.00		10297+00.00	212.3	65.8	4762.4	0.0	207.5	
10297+00.00		10298+00.00	357.4	60.9	4635.9	0.0	210.2	
10298+00.00		10298+45.32	118.9	38.2	1997.5	0.0	95.3	
SOUTH BRIDGE ABUTMENT ADJUSTMENT:					-285.0			<--[[{(15x15)/2]*38.4]/27 --- 15' L BY 15' H TRIANGLE 68.4' W
NORTH BRIDGE ABUTMENT ADJUSTMENT:					-503.7			<--[[{(20x20)/2]*68.0]/27 --- 20' L BY 20' H TRIANGLE 68.0' W
10300+16.32		10301+00.00	200.3	99.8	4375.8	0.0	176.8	
10301+00.00		10302+00.00	243.1	239.0	5083.7	0.0	211.3	
10302+00.00		10303+00.00	322.1	1001.3	3802.3	0.0	211.4	
10303+00.00		10303+11.16	46.2	196.2	310.7	0.0	23.6	
10303+11.16		10304+00.00	1016.9	806.3	6749.1	0.0	184.9	
10304+00.00		10304+64.89	1213.6	5.6	7287.0	0.0	164.5	
10304+64.89		10305+00.00	601.7	2.0	3531.6	0.0	102.6	
10305+00.00		10306+00.00	1595.5	179.0	9080.8	0.0	234.3	
10306+00.00		10307+00.00	1587.0	353.6	7506.2	0.0	180.8	
10307+00.00		10308+00.00	997.8	321.7	6112.4	0.0	175.4	
10308+00.00		10309+00.00	434.2	228.4	5070.7	0.0	175.3	
10309+00.00		10310+00.00	663.7	84.7	5103.8	0.0	175.3	
10310+00.00		10311+00.00	889.6	38.0	4729.2	0.0	175.3	
10311+00.00		10311+73.52	449.1	34.0	2572.3	0.0	128.9	
10311+73.52		10312+00.00	87.8	8.7	715.6	0.0	46.4	
10312+00.00		10313+00.00	302.9	39.8	2367.5	0.0	175.3	
10313+00.00		10314+00.00	286.7	26.3	1961.7	0.0	175.3	
10314+00.00		10315+00.00	258.8	14.0	1587.9	0.0	175.3	
10315+00.00		10316+00.00	349.0	9.9	1578.7	0.0	175.3	
10316+00.00		10317+00.00	343.7	13.2	1359.2	147.0	175.3	
10317+00.00		10318+00.00	360.5	15.3	1109.8	304.6	175.3	
10318+00.00		10319+00.00	465.9	8.8	1120.3	310.1	175.3	
10319+00.00		10320+00.00	420.1	1.5	870.8	298.9	175.3	
10320+00.00		10321+00.00	390.5	0.0	676.0	264.9	175.3	
10321+00.00		10322+00.00	319.3	140.8	504.0	173.9	175.3	
10322+00.00		10323+00.00	231.8	149.5	289.5	76.6	175.3	
10323+00.00		10324+00.00	201.8	24.0	153.4	26.9	175.3	
10324+00.00		10325+00.00	205.1	39.2	126.2	9.3	175.3	
10325+00.00		10326+00.00	208.2	38.8	131.6	12.5	174.2	
10326+00.00		10326+53.81	126.7	15.9	145.6	10.6	105.7	
10326+53.81		10327+00.00	95.8	12.1	107.7	0.8	66.9	
10327+00.00		10327+12.44	18.6	2.9	8.0	-2.2	8.7	
TOTAL:			16928	5049	103655	2273	9770	
					734.0			<--FROM ADV CONTRACT QTY. 28000200
					1396.4			<--[25,136*(2/36)] --- LIMITS(25,662 SY) * 2" AVG. SETTLEMENT
					1621.1			<--[29,180*(2/36)] --- LIMITS(19,012 SY) * 2" AVG. SETTLEMENT
					107407			

FILE NAME = ...\\D160M01-SHT-5CH00-RR02.DGN



USER NAME = nraterrm
 PLOT SCALE = 2.0000' / IN.
 PLOT DATE = 11/2/2012

DESIGNED - NMR
 DRAWN - NMR
 CHECKED - MPH
 DATE - 11-02-2012

REVISED -
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CANADIAN PACIFIC RAILWAY
 EARTHWORK SCHEDULE**

SCALE: N.T.S. SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32VB		388	299
CONTRACT NO. 60W01			ILLINOIS FED. AID PROJECT	

CANADIAN PACIFIC RAILWAY FORCE ACCOUNT ITEMS

ITEM NO.	ITEM	UNIT	TOTAL QUANTITY
FA-01	FURNISH AND INSTALL TRACK (136#RE CWR ON WOOD TIES) <i>INCLUDES RAIL, TIES, BALLAST AND ANY OTHER NECESSARY OTHER TRACK MATERIAL</i>	FOOT	8,986
FA-02	FURNISH AND INSTALL TRACK (136#RE CWR ON STEEL TIES) <i>INCLUDES RAIL, TIES, BALLAST AND ANY OTHER NECESSARY OTHER TRACK MATERIAL</i>	FOOT	412
FA-05	FURNISH AND INSTALL #15 RH PO TURNOUT (136#RE RAIL) <i>INCLUDES RAIL, TIES, BALLAST AND ANY OTHER NECESSARY OTHER TRACK MATERIAL</i> <i>INCLUDES RELOCATION OF SWITCH HEATING APPARATUS (IF APPLICABLE)</i>	EACH	2
FA-08	REMOVE TURNOUT (SALVAGE) <i>INCLUDES REMOVAL AND DISPOSAL OF TURNOUT AND ALL ASSOCIATED EQUIPMENT</i> <i>INCLUDES REMOVAL AND DISPOSAL OF BALLAST</i>	EACH	2
FA-09	INSTALL TEMPORARY TIMBER CROSSING SURFACE FOR TEMP. CONSTRUCTION ENTRANCE <i>INCLUDES FURNISHING AND INSTALLING TIMBER CROSSING SURFACE FOR TEMPORARY CONST. ENTRANCE</i>	FOOT	48
FA-11	FURNISH AND INSTALL SIGNAL <i>INCLUDES FURNISHING AND INSTALLING PROPOSED SIGNALS AND ANY ASSOCIATED EQUIPMENT</i> <i>INCLUDES DESIGNING AND CONSTRUCTION OF ANY NECESSARY SIGNAL MOUNDS NECESSARY</i>	EACH	4
FA-12	TRACK TO BE REMOVED (SALVAGE) <i>INCLUDES REMOVAL OF RAIL TIES AND OTHER EXISTING OTHER TRACK MATERIAL</i> <i>INCLUDES REMOVING BALLAST DOWN TO THE TOP OF SUBBALLAST</i> <i>TRACK MATERIAL REMOVED (EXCL. BALLAST) SHALL BE SALVAGED AND THE COST ADJUSTED ACCORDINGLY</i>	FOOT	9,425
FA-13	GRADE CROSSING REMOVAL (ROOSEVELT AVENUE) <i>INCLUDES REMOVAL OF EXIST. CONCRETE PANEL CROSSING SURFACE AT ROOSEVELT AVE. (112 TF)</i>	L SUM	1
FA-14	GRADE CROSSING REMOVAL (IRVING PARK ROAD) <i>INCLUDES REMOVAL OF EXIST. CONCRETE PANEL CROSSING SURFACE AT IRVING PARK RD. (144 TF)</i> <i>INCLUDES REMOVAL OF EXIST. TIMBER AND ASPHALT CROSSING SURFACE AT IRVING PARK RD. (32 TF)</i> <i>INCLUDES REMOVAL OF TEMP. TIMBER AND ASPHALT CROSSING SURFACE AT IRVING PARK RD. (80 TF)</i>	L SUM	1
FA-15	REMOVE GRADE CROSSING SIGNALS AND GATES (SALVAGE) (ROOSEVELT AVENUE) <i>INCLUDES REMOVAL OF CROSSING SIGNALS AND GATES AT ROOSEVELT AVE. (2-EACH)</i> <i>INCLUDES REMOVAL OF EXIST. CROSSING SIGNAL BUNGALOW AT ROOSEVELT AVE. (1 EACH)</i> <i>INCLUDES REMOVAL OF ANY OTHER ASSOCIATED CROSSING SIGNAL EQUIPMENT AT ROOSEVELT AVE.</i> <i>EQUIPMENT REMOVED SHALL BE SALVAGED AND COST ADJUSTED ACCORDINGLY</i>	L SUM	1
FA-16	REMOVE GRADE CROSSING SIGNALS AND GATES (SALVAGE) (IRVING PARK ROAD) <i>INCLUDES REMOVAL OF EXISTING CANTILEVER CROSSING SIGNALS AT IRVING PARK ROAD (2-EACH)</i> <i>INCLUDES REMOVAL OF EXIST. GATES AT IRVING PARK ROAD (2-EACH)</i> <i>INCLUDES REMOVAL OF EXIST. CROSSING SIGNAL BUNGALOW AT IRVING PARK ROAD (1-EACH)</i> <i>INCLUDES REMOVAL OF ANY OTHER ASSOCIATED CROSSING SIGNAL EQUIPMENT AT IRVING PARK RD.</i> <i>EQUIPMENT REMOVED SHALL BE SALVAGED AND COST ADJUSTED ACCORDINGLY</i>	L SUM	1
FA-17	INSTALL TEMPORARY GRADE CROSSING (IRVING PARK ROAD) <i>INCLUDES FURN. AND INST. TEMPORARY CROSSING SURFACE EXTENSIONS AT IRVING PARK RD. (80 TF)</i>	L SUM	1
FA-18	FURNISH AND INSTALL TEMPORARY GRADE CROSSING SIGNALS AND GATES <i>INCLUDES FURNISHING AND INSTALLING TEMPORARY GATES WITH LIGHTS AND CROSSBUCK SIGNS (4-EACH)</i> <i>INCLUDES FURNISHING PREFABRICATED STEEL FOUNDATIONS FOR THE TEMPORARY GATES (4-EACH)</i> <i>INCLUDES FURNISHING AND INSTALLING ANY CABLE AND CONDUITS AS REQUIRED</i> <i>INCLUDES ANY OTHER ADJUSTMENTS TO THE EXISTING CROSSING SIGNAL SYSTEM</i> <i>INCLUDES ANY NECESSARY SIGNAL TESTING FOR TEMPORARY SIGNAL EQUIPMENT</i>	L SUM	1
FA-19	RELOCATE TEMPORARY GRADE CROSSING SIGNALS AND GATES <i>INCLUDES RELOCATING TEMPORARY GATES WITH LIGHTS AND CROSSBUCK SIGNS (3-EACH)</i> <i>INCLUDES FURNISHING PREFABRICATED STEEL FOUNDATIONS FOR THE RELOCATED GATES (3-EACH)</i> <i>INCLUDES FURNISHING AND INSTALLING ANY CABLE AND CONDUITS AS REQUIRED</i> <i>INCLUDES ANY OTHER ADJUSTMENTS TO THE EXISTING CROSSING SIGNAL SYSTEM</i> <i>INCLUDES ANY NECESSARY SIGNAL TESTING FOR RELOCATED SIGNAL EQUIPMENT</i>	L SUM	1
FA-20A	FURNISH AND INSTALL SIGNAL SYSTEM FOR JUNCTION B-17 NORTH <i>INCLUDES ALL WORK NECESSARY TO CREATE SIGNAL CONTROL SYSTEM FOR JCT B-17 NORTH</i> <i>INCLUDES FURNISHING AND INSTALLING ANY CABLE AND CONDUITS AS REQUIRED</i> <i>INCLUDES FURNISHING AND INSTALLING A SIGNAL BUNGALOW AT CPR CROSSOVER SIGNALS</i> <i>INCLUDES ANY ASSOCIATED SIGNAL SOFTWARE INSTALLATION</i> <i>INCLUDES ANY NECESSARY SIGNAL TESTING FOR NEW SIGNALS INCORPORATED WITH JCT B-17 NORTH</i>	L SUM	1
FA-21	RESURFACE TRACK <i>INCLUDES RAISING THE EXISTING CPR TRACKS AT THE NORTH LIMIT BY APPROXIMATELY 12"</i> <i>INCLUDES FURNISHING AND INSTALLING ANY BALLAST NECESSARY TO ADJUST TRACK PROFILE</i> <i>INCLUDES ANY DESIGN NECESSARY TO ADJUST THE EXISTING TRACK PROFILE</i> <i>PROFILE CHANGES NOT DESIGNED BUT ASSUMED TO BE 1,500' PER TRACK FOR TWO TRACKS</i>	FOOT	3,000

METRA FORCE ACCOUNT ITEMS

ITEM NO.	ITEM	UNIT	TOTAL QUANTITY
FA-01	FURNISH AND INSTALL TRACK (136#RE CWR ON WOOD TIES) <i>INCLUDES RAIL, TIES, BALLAST AND ANY OTHER NECESSARY OTHER TRACK MATERIAL</i>	FOOT	2,390
FA-03	REALIGN TRACK <i>INCLUDES REALIGNING EXISTING CPR LEAD TRACKS AS SHOWN ON THE PLANS</i> <i>INCLUDES FURN. AND INSTALLING BALLAST NECESSARY TO ADJUST TRACK PROFILES</i> <i>INCLUDES RELOCATION OF TWO GREASE BOXES</i>	FOOT	1,439
FA-04	FURNISH AND INSTALL #20 LH PO CROSSOVER (136#RE RAIL) <i>INCLUDES FURNISHING AND INSTALLING #20 LH POWER OPERATED TUNOUTS (2-EACH)</i> <i>INCLUDES RAIL, TIES, BALLAST AND ANY OTHER NECESSARY OTHER TRACK MATERIAL</i> <i>INCLUDES RELOCATION OF SWITCH HEATING APPARATUS (2-EACH)</i>	EACH	1
FA-05	FURNISH AND INSTALL #15 RH PO TURNOUT (136#RE RAIL) <i>INCLUDES FURNISHING AND INSTALLING #15 RH POWER OPERATED TUNOUTS (4-EACH)</i> <i>INCLUDES RAIL, TIES, BALLAST AND ANY OTHER NECESSARY OTHER TRACK MATERIAL</i> <i>INCLUDES RELOCATION OF SWITCH HEATING APPARATUS (4-EACH)</i>	EACH	4
FA-06	FURNISH AND INSTALL #15 RH PO CROSSOVER <i>INCLUDES FURNISHING AND INSTALLING #15 RH POWER OPERATED TURNOUTS (2 PER XO) (4 TOTAL RH PT TO)</i> <i>INCLUDES RAIL, TIES, BALLAST AND ANY OTHER NECESSARY OTHER TRACK MATERIAL</i> <i>INCLUDES RELOCATION OF SWITCH HEATING APPARATUS (4-EACH)</i>	EACH	2
FA-07	FURNISH AND INSTALL #11 LH PO TURNOUT (136#RE RAIL) <i>INCLUDES FURNISHING AND INSTALLING #11 LH POWER OPERATED TUNOUTS (2-EACH)</i> <i>INCLUDES RAIL, TIES, BALLAST AND ANY OTHER NECESSARY OTHER TRACK MATERIAL</i>	EACH	1
FA-08	REMOVE TURNOUT (SALVAGE) <i>INCLUDES REMOVAL AND DISPOSAL OF TURNOUT AND ALL ASSOCIATED EQUIPMENT</i> <i>INCLUDES REMOVAL AND DISPOSAL OF BALLAST</i> <i>TRACK MATERIAL REMOVED (EXCL. BALLAST) SHALL BE SALVAGED AND THE COST ADJUSTED ACCORDINGLY</i>	EACH	11
FA-10	SIGNAL TO BE REMOVED (SALVAGE) <i>INCLUDES REMOVAL OF SIGNAL AT LOCATIONS SHOWN ON THE PLANS</i> <i>INCLUDES REMOVAL OF ASSOCIATED EQUIPMENT</i> <i>EXISTING SIGNALS SHALL BE SALVAGED AND COST ADJUSTED ACCORDINGLY</i>	EACH	4
FA-11	FURNISH AND INSTALL SIGNAL <i>INCLUDES FURNISHING AND INSTALLING PROPOSED SIGNALS AND ANY ASSOCIATED EQUIPMENT</i> <i>INCLUDES DESIGNING AND CONSTRUCTION OF ANY NECESSARY SIGNAL MOUNDS NECESSARY</i>	EACH	4
FA-12	TRACK TO BE REMOVED (SALVAGE) <i>INCLUDES REMOVAL OF RAIL TIES AND OTHER EXISTING OTHER TRACK MATERIAL</i> <i>INCLUDES REMOVING BALLAST DOWN TO THE TOP OF SUBBALLAST</i> <i>TRACK MATERIAL REMOVED (EXCL. BALLAST) SHALL BE SALVAGED AND THE COST ADJUSTED ACCORDINGLY</i>	FOOT	653
FA-20	UPDATE JUNCTION B-17 SIGNAL SYSTEM <i>INCLUDES ALL WORK NECESSARY TO INCORPORATE NEW SIGNALS INTO THE JCT. B-17 SIGNAL SYSTEM</i> <i>INCLUDES ALL WORK NECESSARY TO INCORPORATE RELOCATED SIGNALS INTO THE JCT B-17 SIGNAL SYSTEM</i> <i>INCLUDES FURNISHING AND INSTALLING ANY CABLE AND CONDUITS AS REQUIRED</i> <i>INCLUDES REMOVING A SIGNAL BUNGALOW AT EXISTING CPR SB SIGNALS</i> <i>INCLUDES ANY ASSOCIATED SIGNAL SOFTWARE UPGRADES</i> <i>INCLUDES ANY NECESSARY SIGNAL TESTING FOR UPDATED SIGNAL LOCATIONS</i>	L SUM	1
FA-21	RESURFACE TRACK <i>INCLUDES CHANGES TO THE EXISTING TRACK PROFILES ALONG THE EXISTING ALIGNMENTS</i> <i>INCLUDES FURNISHING AND INSTALLING ANY BALLAST NECESSARY TO ADJUST TRACK PROFILE</i>	FOOT	4,117
FA-22	FURNISH AND INSTALL NEW CANTILEVER SIGNAL BRIDGE AND REMOVE EXISTING <i>INCLUDES CONSTRUCTING NEW FOUNDATION</i> <i>INCLUDES FURNISHING AND INSTALLING NEW CANTILEVER SIGNAL BRIDGE</i> <i>INCLUDES FURNISHING AND INSTALLING ANY ASSOCIATED EQUIPMENT NECESSARY TO OPERATE THE SIGNAL</i> <i>INCLUDES RELOCATION OF THE SIGNAL BUNGALOW IF NECESSARY</i> <i>INCLUDES REMOVAL OF CANTILEVER SIGNAL BRIDGE AT LOCATION SHOWN ON THE PLANS</i> <i>INCLUDES REMOVAL OF EXISTING FOUNDATION</i>	L SUM	1

FILE NAME = ...\\D160W01-SHT-5CH00-RR03.DGN



USER NAME = nraterrm	DESIGNED - NMR	REVISED -
PLOT SCALE = 2.0000' / IN.	DRAWN - NMR	REVISED -
PLOT DATE = 11/2/2012	CHECKED - MPH	REVISED -
	DATE - 11-02-2012	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

RAILROAD FORCE ACCOUNT SUMMARY

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32VB	DUPAGE	388	300
CONTRACT NO. 60W01				
ILLINOIS FED. AID PROJECT				