

THE FOLLOWING TRAFFIC CONTROL STANDARDS ARE THE MINIMUM REQUIREMENTS FOR TRAFFIC CONTROL FOR THIS PROJECT:

701301 LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS  
 701501 URBAN LANE CLOSURE, 2L, 2W UNDIVIDED  
 701601 URBAN LANE CLOSURE, MULTILANE, 2W, WITH NON-TRAVERSABLE MEDIAN  
 701701 URBAN LANE CLOSURE, MULTILANE INTERSECTION  
 701801 LANE CLOSURE, MULTILANE, 1W OR 2W, CROSSWALK OR SIDEWALK CLOSURE  
 701901 TRAFFIC CONTROL DEVICES

**TRAFFIC CONTROL AND PROTECTION NOTES**

TRAFFIC CONTROL AND PROTECTION SHALL BE PERFORMED IN ACCORDANCE WITH THE TRAFFIC CONTROL PLAN, TRAFFIC SIGNAL PLANS, THESE NOTES, APPLICABLE SPECIAL PROVISIONS, AND SECTION 701 OF THE STANDARD SPECIFICATIONS AS AMENDED BY THE SPECIAL PROVISION FOR WORK ZONE TRAFFIC CONTROL (CHECK SHEET LRS 3).

THE TYPE III BARRICADES ARE TO BE PLACED IN ACCORDANCE WITH STANDARD 701901 UNLESS AUTHORIZED BY THE ENGINEER TO USE AN ALTERNATE ARRANGEMENT.

EXISTING TRAFFIC CONTROL SIGNS AND DEVICES WILL BE REMOVED BY THE CONTRACTOR AFTER THE TRAFFIC CONTROL REQUIREMENTS ARE MET OR AS AUTHORIZED BY THE ENGINEER; ANY SIGNS OR DEVICES LEFT IN PLACE AT THIS TIME ARE TO BE RELOCATED, MAINTAINED AND PROTECTED FROM DAMAGE BY THE CONTRACTOR AND ANY DAMAGED OR LOST SIGNS SHALL BE REPLACED BY THE CONTRACTOR.

TYPE I OR TYPE II BARRICADES, DRUMS, OR VERTICAL PANELS WITH MONODIRECTIONAL STEADY-BURN LIGHTS SHALL BE REQUIRED ALONG TEMPORARY ROADS, DETOURS AND SIDE STREETS TO DELINEATE THE TRAVELED WAY WITHIN THE CONSTRUCTION ZONE. THE MAXIMUM SPACING FOR THESE DEVICES SHALL BE 100 FEET CENTER TO CENTER.

ANY DROP OFF GREATER THAN THREE (3) INCHES BUT LESS THAN SIX (6) INCHES, WITHIN EIGHT (8) FEET OF THE PAVEMENT EDGE, SHALL BE PROTECTED BY TYPE I OR TYPE II BARRICADES, DRUMS OR VERTICAL PANELS WITH MONODIRECTIONAL STEADY-BURN LIGHTS AT 100 FOOT CENTER TO CENTER SPACING. IF THE DROP OFF WITHIN EIGHT (8) FEET OF THE PAVEMENT EDGE EXCEEDS SIX (6) INCHES, THE BARRICADES, DRUMS OR VERTICAL PANELS MENTIONED ABOVE SHALL BE PLACED AT FIFTY (50) FOOT CENTER TO CENTER SPACING. BARRICADES THAT MUST BE PLACED IN EXCAVATED AREAS SHALL HAVE LEG EXTENSIONS INSTALLED SUCH THAT THE TOP OF THE BARRICADE IS IN COMPLIANCE WITH THE HEIGHT REQUIREMENTS OF STANDARD 701901.

TYPE I OR TYPE II BARRICADES WITH TWO-WAY FLASHING LIGHTS SHALL BE REQUIRED AT ALL OPEN TRENCHES, EXCAVATIONS, OPEN OR EXPOSED SEWER STRUCTURES, TRANSVERSE PAVEMENT JOINTS, MATERIALS OR EQUIPMENT WITHIN THE RIGHT-OF-WAY (NUMBER AND SPACING DEPENDS ON THE CONDITIONS); AND AT LOCATIONS DESIGNATED BY THE ENGINEER OR LOCAL LAW ENFORCEMENT AGENCIES.

TYPE I, II AND/OR III BARRICADES WITH TWO-WAY FLASHING LIGHTS WILL BE REQUIRED TO GUIDE TRAFFIC AWAY FROM PAVEMENT AREAS CLOSED FOR CONSTRUCTION.

THE COST OF SUPPLYING, ERECTING, AND MAINTAINING BARRICADES, WARNING LIGHTS, AND SIGNS SHALL BE INCLUDED IN THE CONTRACT LUMP SUM PRICE FOR TRAFFIC CONTROL AND PROTECTION.

WHERE REQUIRED, TRAFFIC SIGNS SHALL BE RELOCATED FOR EACH STAGE OF CONSTRUCTION.

ARROW BOARDS SHALL BE REQUIRED WHEN IMPLEMENTING ALL LANE CLOSURES.

CHANGEABLE L.E.D. MESSAGE BOARDS SHALL BE IN OPERATION A MINIMUM 3 DAYS IN ADVANCE OF CONSTRUCTION, AND REMAIN OPERATIONAL THROUGHOUT CONSTRUCTION. SEE MOT SHEET 2 FOR NUMBER AND GENERAL LOCATION OF MESSAGE BOARDS.

IN THE INTEREST OF PEDESTRIAN SAFETY ALL SIDEWALKS WITHIN ACTIVE CONSTRUCTION WORK ZONES SHALL BE CLOSED IN ACCORDANCE WITH STANDARD DRAWING 701801. FOR PEDESTRIAN CONVENIENCE EVERY EFFORT SHALL BE MADE TO KEEP SIDEWALKS OPEN THAT ARE NOT WITHIN ACTIVE CONSTRUCTION WORK ZONES AND WHEN APPROVED BY THE ENGINEER.

**SUGGESTED CONSTRUCTION STAGING - WORK ZONE 3\*\***

MAINTAIN 1 LANE OF THROUGH TRAFFIC IN EACH DIRECTION AT ALL TIMES ON KINGS ROAD. MAINTAIN SB TRAFFIC AT ALL TIMES ON 95TH STREET. WHEN POSSIBLE, BOUGHTON ROAD SHALL REMAIN FULLY OPEN AT ALL TIMES.

STAGE 1:  
SHIFT SB 95TH STREET TRAFFIC TO EXISTING NB LANE, AND RECONSTRUCT OR MILL/RESURFACE SB THROUGH LANES.

CHANNEL KINGS ROAD TRAFFIC TO ALLOW FOR WIDENING AND RECONSTRUCTING OF BOTH EDGES OF PAVEMENT.

USING DAILY CONSTRUCTION OPERATIONS, MAINTAIN ALL TRAFFIC ALONG BOUGHTON ROAD WHILE CONSTRUCTING OR EXTENDING LEFT AND/OR RIGHT TURN LANES.

STAGE 2:  
SHIFT SB 95TH STREET TRAFFIC TO NEW SB LANES, AND COMPLETE CONSTRUCTION OF NB LANES AND MEDIANS.

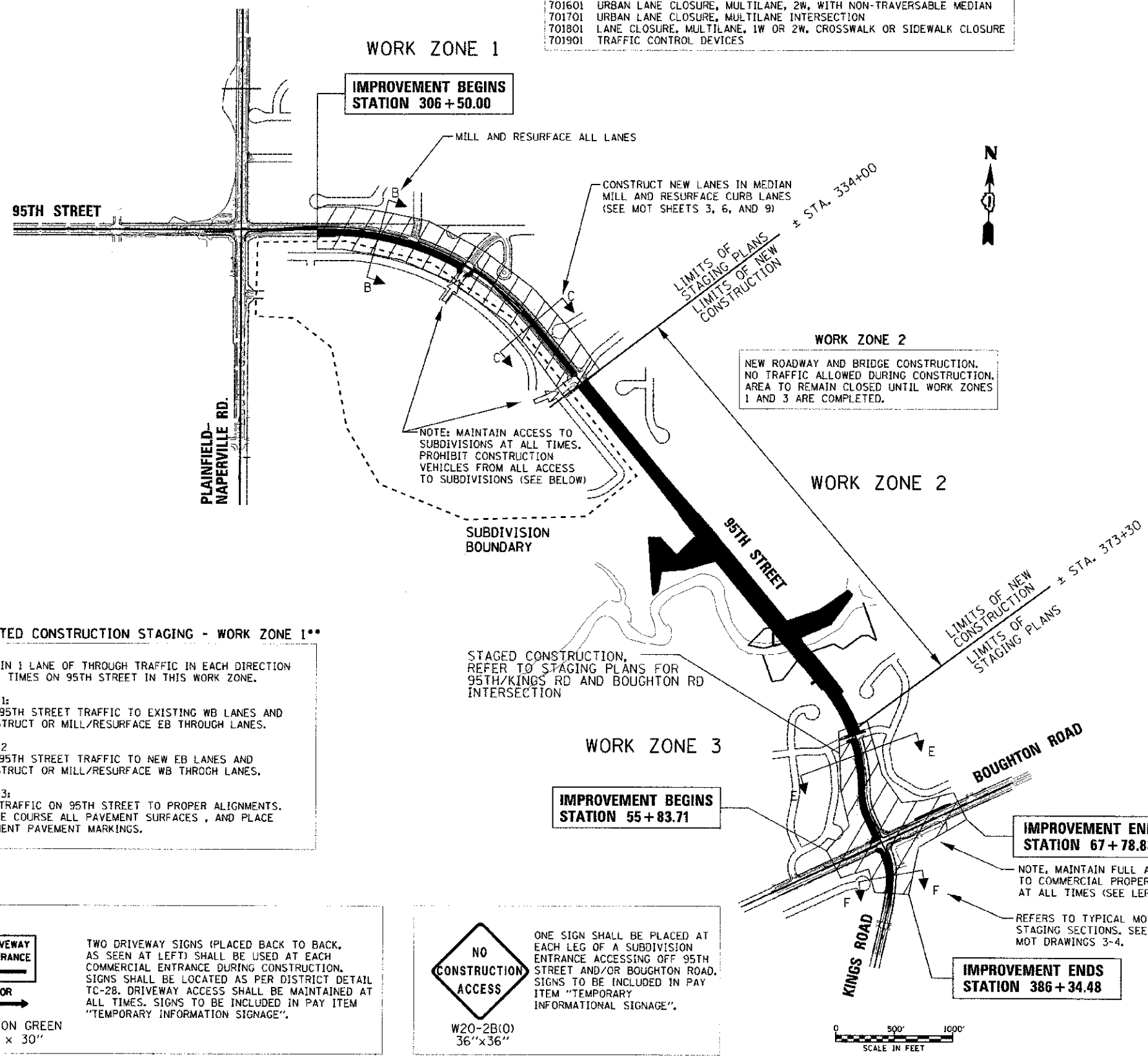
CHANNEL KINGS ROAD TRAFFIC TO ALLOW FOR MILLING/RESURFACING CENTER PAVEMENT OF KINGS ROAD.

CONTINUE CONSTRUCTION ON BOUGHTON ROAD AS NEEDED.

UPON COMPLETION OF ALL STAGE 2 CONSTRUCTION, SURFACE COURSE ALL PAVEMENT SURFACES, AND PLACE PERMANENT PAVEMENT MARKINGS.

\*\* NOTE: THE TIMING AND DURATION OF THE SUGGESTED STAGES CAN ACT INDEPENDENTLY BETWEEN WORK ZONES 1 AND 3

THIS CONCEPT STAGING PLAN DOES NOT LABEL TEMPORARY STRIPING, SHOW BARRELS, TEMPORARY SIGNING, OR LABEL TAPERS.



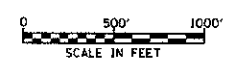
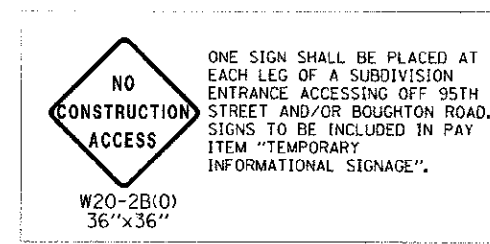
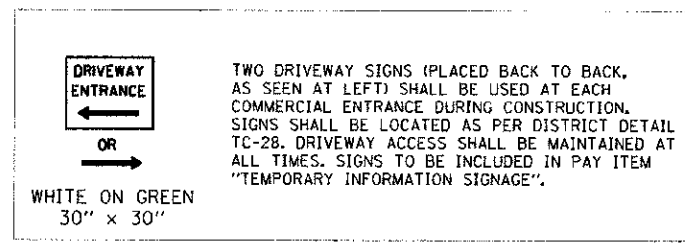
**SUGGESTED CONSTRUCTION STAGING - WORK ZONE 1\*\***

MAINTAIN 1 LANE OF THROUGH TRAFFIC IN EACH DIRECTION AT ALL TIMES ON 95TH STREET IN THIS WORK ZONE.

STAGE 1:  
SHIFT 95TH STREET TRAFFIC TO EXISTING WB LANES AND RECONSTRUCT OR MILL/RESURFACE EB THROUGH LANES.

STAGE 2:  
SHIFT 95TH STREET TRAFFIC TO NEW EB LANES AND RECONSTRUCT OR MILL/RESURFACE WB THROUGH LANES.

STAGE 3:  
SHIFT TRAFFIC ON 95TH STREET TO PROPER ALIGNMENTS. SURFACE COURSE ALL PAVEMENT SURFACES, AND PLACE PERMANENT PAVEMENT MARKINGS.



<b>URS</b>	100 S. WACKER DR. SUITE 500 CHICAGO, IL 60606 TEL (312) 939-4000 FAX (312) 939-4998	USER NAME = james.push	DESIGNED - JRP	REVISED -
		PLOT SCALE = 1/8" = 1'-0"	DRAWN - MRF	REVISED -
		PLOT DATE = 10/15/2012	CHECKED - JRP	REVISED -
			DATE - 10/19/2012	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED CONSTRUCTION STAGING  
GENERAL PLAN AND STAGING NOTES**

SCALE: 1" = 500' DRAWING NO. 1 OF 10 STA. TO STA.

F.A.U. RTE. 1644	SECTION 01-00181-00-FP	COUNTY WILL	TOTAL SHEETS 328	SHEET NO. 53
ILLINOIS FED. AID PROJECT				CONTRACT NO. 63647