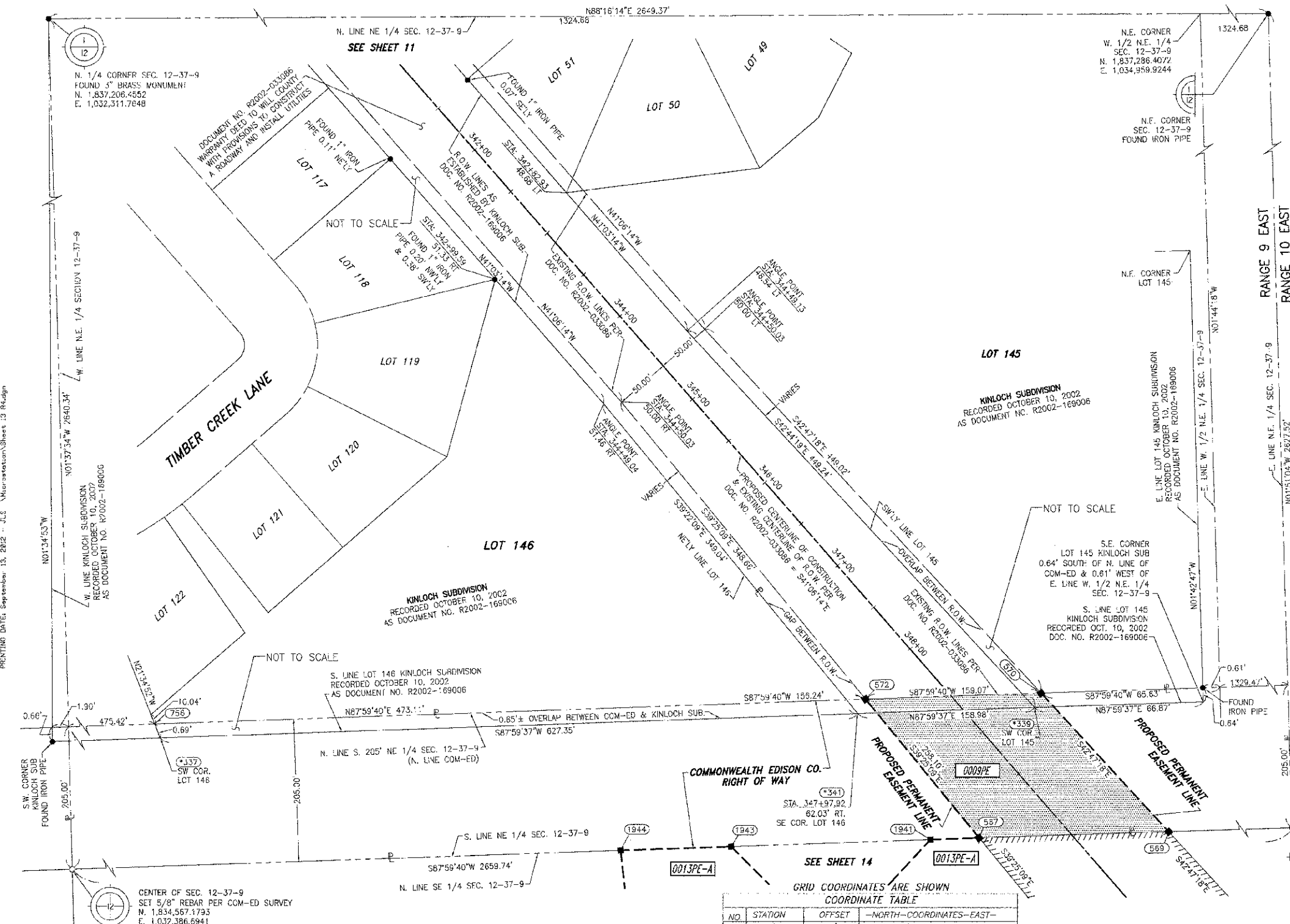


DETAIL OF SHEET 12 - NOT TO SCALE



LEGEND

SECTION CORNER (9/10 16/15)

QUARTER SECTION CORNER (16/15)

SECTION LINE
 QUARTER SECTION LINE
 QUARTER, QUARTER SECTION LINE
 PLATTED LOT LINE
 PROPERTY (DEED) LINE

APL
 APPARENT PROPERTY LINE
 EXISTING CENTERLINE
 PROPOSED CENTERLINE
 EXISTING RIGHT OF WAY LINE
 PROPOSED RIGHT OF WAY LINE
 PROPOSED EASEMENT

MEASURED DIMENSION
 COMPUTED DIMENSION
 RECORD DATA

EXISTING BUILDING
 COMED EASEMENT

● IRON PIPE OR ROD FOUND ○ SET 5/8" x 30" ORP
 + CUT CROSS FOUND OR SET ● PK FOUND PK NAIL ○ PK SET PK NAIL

● T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION
 T2 SET 5/8 INCH REBAR FLUSH WITH GROUND TO THE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP AND BEARING SURVEYORS PROFESSIONAL NUMBER.

● BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION.
 BT2 BURIED 5/8 INCH REBAR 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS PROFESSIONAL NUMBER.

■ STAKING OF PROPOSED RIGHT OF WAY.
 SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS PROFESSIONAL NUMBER.

■ M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS.
 BURIED 5/8 INCH REBAR 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS PROFESSIONAL NUMBER.

● PERMANENT SURVEY MARKER, IDOT STD. 2135 (TO BE SET BY OTHERS)
 □ RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS) SS
 COUNTY OF WILL)

THIS IS TO CERTIFY THAT WE, CLAASSEN, WHITE & ASSOCIATES, P.C., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION NUMBER 184-004039, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 12, TOWNSHIP 37 NORTH, RANGE 9 EAST OF THE THIRD PRINCIPAL MERIDIAN, WILL COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE TRACED. THIS SURVEY CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT JOYET, ILLINOIS THIS 23rd DAY OF MARCH, 2012 A.D.

DAVID A. CLAASSEN
 ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 035-002962
 LICENSE EXPIRES NOVEMBER 30, 2012
 THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

DAVID A. CLAASSEN
 REGISTERED PROFESSIONAL LAND SURVEYOR
 STATE OF ILLINOIS
 LICENSE NO. 2962

Classten, White & Associates, P.C.
 121 AIRPORT DRIVE, UNIT 1
 MAHET, ILLINOIS 60431
 (815) 744-3720

NOTE: BEARINGS, DISTANCES AND COORDINATES SHOWN HEREON ARE BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM EAST ZONE, NAD 83 (2007).

PARCEL NUMBER	OWNER	TOTAL HOLDING ACRES	PART TAKEN ACRES	REMAINDER ACRES	PREV. DEED ACRES	EASEMENT ACRES	EASEMENT PURPOSE	PERMANENT TAX INDEX NUMBER	PROPERTY ACQUIRED BY
CC09PE	COMMONWEALTH EDISON COMPANY	12.515		12.515		0.796	ROADWAY	07-01-12-200-013 07-01-12-200-016	

GRID COORDINATES ARE SHOWN
 COORDINATE TABLE

NO.	STATION	OFFSET	NORTH-COORDINATES-EAST	
756	341+01.63	948.67' RI	N. 1,834,788.7391	E. 1,032,856.0044
337	344+02.29	948.90' RI	N. 1,834,788.0935	E. 1,032,856.2598
1944	347+91.65	393.79' RT	N. 1,834,596.6790	E. 1,033,229.1164
341	347+97.92	62.03' RT	N. 1,834,810.0584	E. 1,033,483.2252
572	347+98.54	60.25' RT	N. 1,834,810.7640	E. 1,033,484.9700
1945	348+54.71	316.18' RT	N. 1,834,500.1786	E. 1,033,329.0551
339	348+56.18	61.35' LT	N. 1,834,813.6221	E. 1,033,842.1096
570	348+58.85	63.20' LT	N. 1,834,816.3310	E. 1,033,843.8452
1941	350+37.80	91.12' RT	N. 1,834,510.3275	E. 1,033,518.8775
567	350+56.52	67.81' RT	N. 1,834,511.3774	E. 1,033,648.8591
569	351+89.48	71.16' LT	N. 1,834,617.6454	E. 1,033,827.8555

SHEET 1 IS A COVER SHEET AND NOT RECORDED.

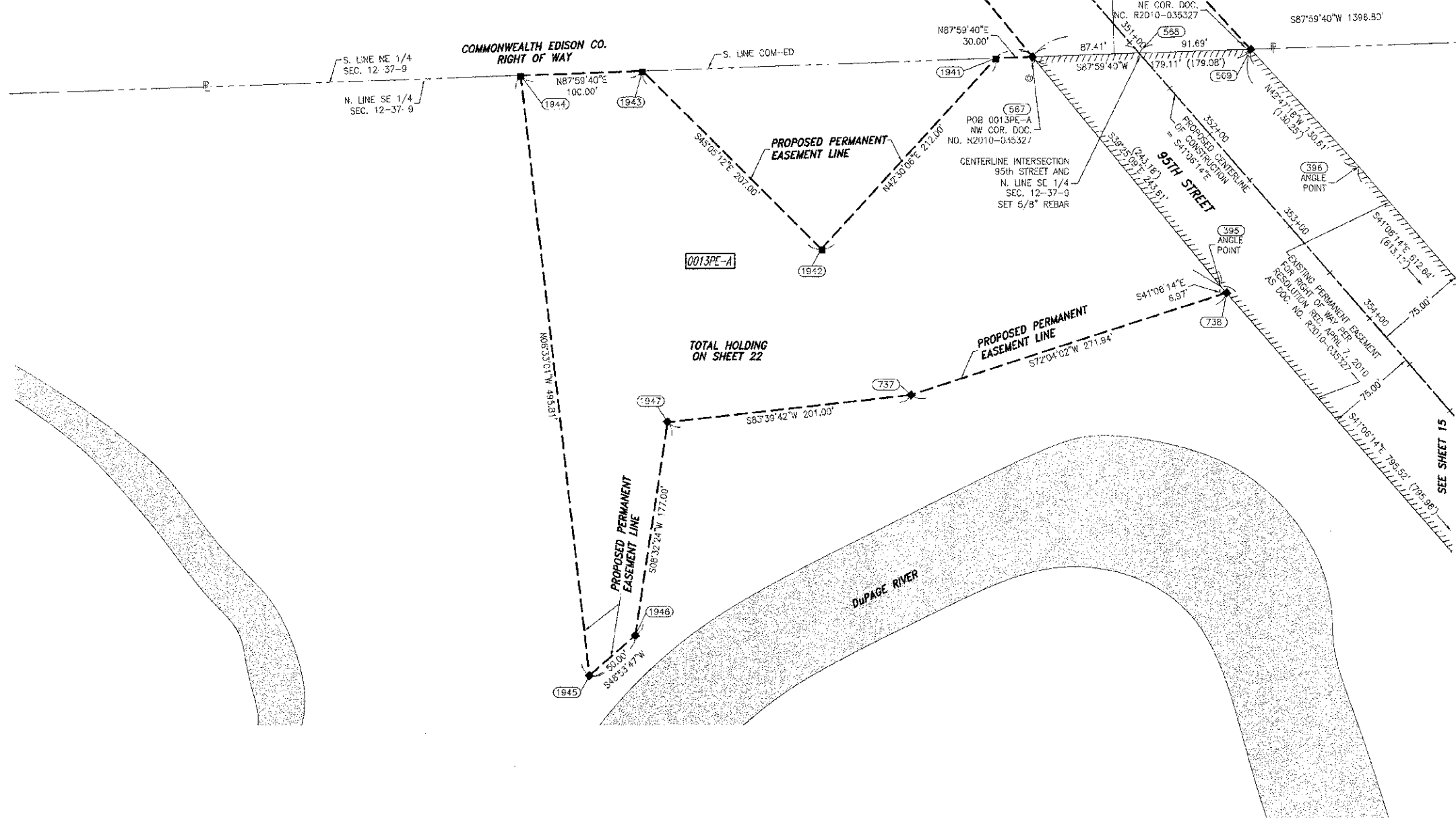
PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
CH 89 (95TH STREET) EXTENSION
PLAINFIELD-NAPERVILLE RD. TO BOUGHTON RD.

SECTION 01-03181-00-PP WILL COUNTY
 PROJECT JOB NO. R-55-001-97
 STATION 342+50 TO STATION 351+50
 SCALE: N/A SHEET 13 OF 27

PART OF THE SE 1/4 SECTION 12, IN T37N, R9E OF THE 3rd PM, WHEATLAND TOWNSHIP, WILL COUNTY, ILLINOIS.

CONTRACT NO. 63647

FOUND STONE E. 1/4 COR. SEC. 12-37-9
N. 1,834,860.2602 E. 1,035,044.6049
POC 0013PE-A

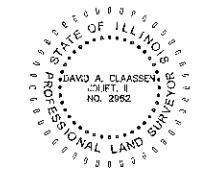


- LEGEND**
- SECTION CORNER
 - QUARTER SECTION CORNER
 - SECTION LINE
 - QUARTER SECTION LINE
 - QUARTER, QUARTER SECTION LINE
 - PLATTED LOT LINE
 - PROPERTY (DEED) LINE
 - APL APPARENT PROPERTY LINE
 - EXISTING CENTERLINE
 - PROPOSED CENTERLINE
 - EXISTING RIGHT OF WAY LINE
 - PROPOSED RIGHT OF WAY LINE
 - PROPOSED EASEMENT
 - MEASURED DIMENSION
 - COMPUTED DIMENSION
 - RECORD DATA
 - EXISTING BUILDING
 - IRON PIPE OR ROD FOUND
 - CUT CROSS FOUND OR SET
 - SET 5/8" x 30" REBAR
 - FOUND PK NAIL
 - SET PK NAIL
 - SET 5/8" INCH REBAR FLUSH WITH GROUND TO THE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP AND BEARING SURVEYORS PROFESSIONAL NUMBER.
 - THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS PROFESSIONAL NUMBER.
 - STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS PROFESSIONAL NUMBER.
 - STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH REBAR 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS PROFESSIONAL NUMBER.
 - PERMANENT SURVEY MARKER, 1/2" DIA. STD. 2135 (TO BE SET BY OTHERS)
 - RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS)
COUNTY OF WILL)

THIS IS TO CERTIFY THAT WE, CLAASSEN, WHITE & ASSOCIATES, P.C., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION NUMBER 184-004039, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 12, TOWNSHIP 37 NORTH, RANGE 9 EAST OF THE THIRD PRINCIPAL MERIDIAN, WILL COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE REFRACED. THIS SURVEY CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED 23rd JULY 2012 ILLINOIS THIS 23rd DAY OF MARCH, 2012 A.D.
DAVID A. CLAASSEN
VICE PRESIDENT
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 035-002962
LICENSE EXPIRES NOVEMBER 30, 2012
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.



0 50 100 150
SCALE: 1" = 50'

Claassen, White & Associates, P.C.
121 AIRPORT DRIVE, UNIT 1
JOLIET, ILLINOIS 60431
(815) 744-3720

SHEET 1 IS A COVER SHEET AND NOT RECORDED.
PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
CH 89 (95TH STREET) EXTENSION
PLAINFIELD-NAPERVILLE RD. TO BOUGHTON RD.
SECTION 01-00181-00-FP WILL COUNTY
PROJECT JOB NO. R-55-C01-97
STATION 351+00 TO STATION 354+50
SCALE: 1" = 50' SHEET 4 OF 27

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196-1096

PARCEL NUMBER	OWNER	TOTAL HOLDING ACRES	PART IAKRN ACRES	REMAINDER ACRES	PREV. DEED ACRES	EASEMENT ACRES	EASEMENT PURPOSE	PERMANENT TAX INDEX NUMBER	PROPERTY ACQUIRED BY
0013PF-A	FOREST PRESERVE DISTRICT OF WILL COUNTY	107.284				7.788	DRAINAGE	07-01-12-400-016 07-01-12-400-015-0010 07-01-12-400-015-0020	

NOTE: PARCELS 0013PE-B DELETED 8-18-2011

GRID COORDINATES ARE SHOWN

COORDINATE TABLE

NO.	STATION	OFFSET	NORTH-COORDINATES-EAST	
1944	347+91.65	393.79' RT.	N. 1,834,506.679C	E. 1,033,229.116A
1943	348+54.71	316.18' RT.	N. 1,834,800.178E	E. 1,033,329.0551
1941	350+37.60	91.12' RT.	N. 1,834,610.3275	E. 1,033,619.8775
567	350+38.52	67.84' RT.	N. 1,834,611.3774	E. 1,033,648.8591
1942	350+61.21	301.80' RT.	N. 1,834,454.0291	E. 1,033,475.5475
1947	350+85.39	490.12' RT.	N. 1,834,312.0093	E. 1,033,349.6368
568	351+11.65	0.00' RT.	N. 1,834,614.4365	E. 1,033,736.2182
569	351+69.48	71.16' LT.	N. 1,834,617.6454	E. 1,033,827.8155
737	352+00.00	323.00' RT.	N. 1,834,334.1592	E. 1,033,549.4082
1945	352+00.00	675.00' RT.	N. 1,834,104.1006	E. 1,033,285.6760
1948	352+00.00	625.00' RT.	N. 1,834,136.9719	E. 1,033,323.3521
395	353+00.03	75.00' RT.	N. 1,834,423.1809	E. 1,033,803.5503
396	353+00.03	75.00' LT.	N. 1,834,521.7846	E. 1,033,916.5784
736	353+07.00	75.00' RT.	N. 1,834,417.9287	E. 1,033,808.1327

- SYMBOL LEGEND**
- INLET
 - LIGHT POLE
 - POWER POLE
 - HIGH TENSION POWER POLE SIGN
 - MANHOLE
 - TRAFFIC SIGNAL HANDHOLE
 - ELECTRICAL OUTLET
 - IRRIGATION CONTROL VALVE
 - PHONE PEDESTAL
 - FIRE HYDRANT
 - VALVE BOX
 - VALVE VAULT
 - FLARED END SECTION
 - TRAFFIC SIGNAL
 - PHONE HANDPOLE
 - GAS VALVE
 - ELECTRIC VAULT
 - GAS HAND-HOLE
 - GAS METER
 - 1/2" DIA. JUNCTION BOX

NOTE: BEARINGS, DISTANCES AND COORDINATES SHOWN HEREON ARE BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM EAST ZONE, NAD 83 (2007).

PART OF THE SE 1/4 SECTION 12, IN T37N, R9E OF THE 3rd PM, WHEATLAND TOWNSHIP, WILL COUNTY, ILLINOIS.

FOUND STONE, E. 1/4 COR.
SEC. 12-37-9
N. 1,834,660.2602 E. 1,035,044.8049
POC 0013PE-C
POC 0014A
POC 0014B
POC 0014C

LEGEND

CONTRACT NO. 63647

SECTION CORNER (9 10 16 15) QUARTER SECTION CORNER (16 15)

SECTION LINE
QUARTER SECTION LINE
QUARTER, QUARTER SECTION LINE
PLATTED LOT LINE
PROPERTY WELD LINE

APL APPARENT PROPERTY LINE
EXISTING CENTERLINE
PROPOSED CENTERLINE
EXISTING RIGHT OF WAY LINE
PROPOSED RIGHT OF WAY LINE
PROPOSED EASEMENT

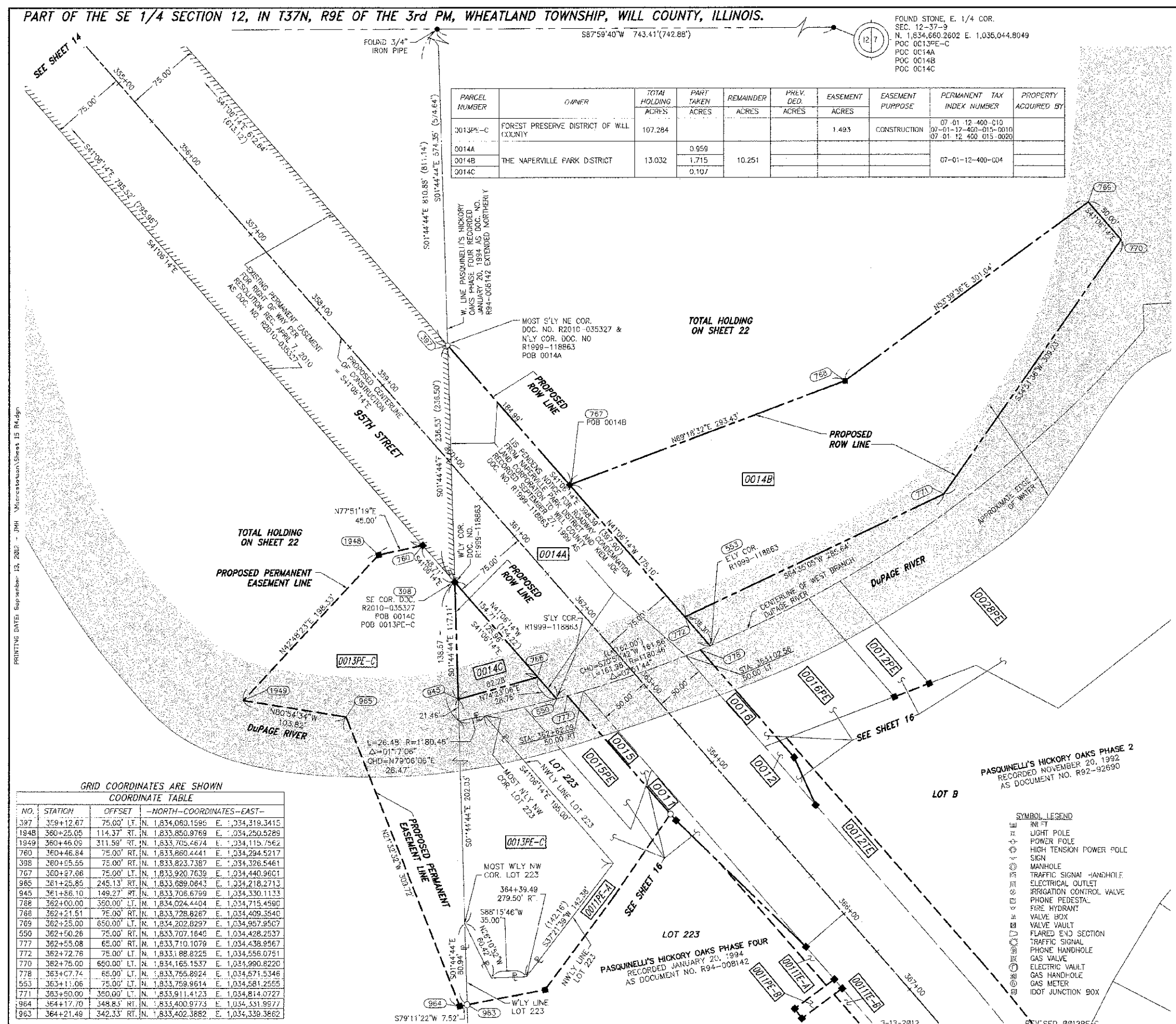
MEASURED DIMENSION (29.32')
COMPUTED DIMENSION (29.32')

EXISTING BUILDING

IRON PIPE OR ROD FOUND
CUT CROSS FOLND OR SET
TI THESE STAKES REFERENCE FOUND OR SET MONUMENTATION
T2 SET 5/8 INCH REBAR FLUSH WITH GROUND TO THE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS PROFESSIONAL NUMBER.
BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BT2 BURIED 5/8 INCH REBAR 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE. BT3 IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS PROFESSIONAL NUMBER.
STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS PROFESSIONAL NUMBER.
STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH REBAR 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS PROFESSIONAL NUMBER.
PERMANENT SURVEY MARKER, IDOT STD. 2135 (TO BE SET BY OTHERS)
RIGHT OF WAY STAKING PROPOSED TO BE SET.

BEARINGS ARE BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM EAST ZONE, NAD 83 (2007)

PARCEL NUMBER	OWNER	TOTAL HOLDING ACRES	PART TAKEN ACRES	REMAINDER ACRES	PRIV. DED. ACRES	EASEMENT ACRES	EASEMENT PURPOSE	PERMANENT TAX INDEX NUMBER	PROPERTY ACQUIRED BY
0013PE-C	FOREST PRESERVE DISTRICT OF WILL COUNTY	107.284				1.493	CONSTRUCTION	07-01-12-400-010 07-01-12-400-015-0010 07-01-12-400-015-0020	
0014A	THE NAPERVILLE PARK DISTRICT	13.032	0.959	10.251				07-01-12-400-004	
0014B			1.715						
0014C			0.107						



GRID COORDINATES ARE SHOWN
COORDINATE TABLE

NO.	STATION	OFFSET	NORTH-COORDINATES	EAST-COORDINATES
397	359+12.67	75.00' LT	N. 1,834,060.1595	E. 1,034,319.3415
1948	360+25.05	114.37' RT	N. 1,833,850.9769	E. 1,034,250.5289
1949	360+46.09	311.59' RT	N. 1,833,705.4674	E. 1,034,115.7562
760	360+46.84	75.00' RT	N. 1,833,860.4441	E. 1,034,294.5217
398	360+55.55	75.00' RT	N. 1,833,823.7387	E. 1,034,326.5461
767	360+87.86	75.00' LT	N. 1,833,920.7639	E. 1,034,440.9601
965	361+25.85	245.13' RT	N. 1,833,689.0843	E. 1,034,218.2713
945	361+66.10	149.27' RT	N. 1,833,708.6799	E. 1,034,330.1133
788	362+00.00	360.00' LT	N. 1,834,024.4404	E. 1,034,715.4590
768	362+21.51	75.00' RT	N. 1,833,728.8287	E. 1,034,409.3540
769	362+25.00	650.00' LT	N. 1,834,202.8297	E. 1,034,957.9507
550	362+50.26	75.00' RT	N. 1,833,707.1645	E. 1,034,428.2537
777	362+55.08	65.00' RT	N. 1,833,710.1079	E. 1,034,438.9567
772	362+72.76	75.00' LT	N. 1,833,188.8225	E. 1,034,556.0751
770	362+75.00	650.00' LT	N. 1,834,165.1537	E. 1,034,990.8220
778	363+07.74	65.00' LT	N. 1,833,755.8924	E. 1,034,571.5346
553	363+11.06	75.00' LT	N. 1,833,759.9614	E. 1,034,581.2555
771	363+50.00	350.00' LT	N. 1,833,911.4123	E. 1,034,814.0727
964	364+17.70	348.83' RT	N. 1,833,400.9773	E. 1,034,331.9877
963	364+21.49	342.33' RT	N. 1,833,402.3682	E. 1,034,339.3662

- IRON PIPE OR ROD FOUND
- CUT CROSS FOLND OR SET
- TI THESE STAKES REFERENCE FOUND OR SET MONUMENTATION
- T2 SET 5/8 INCH REBAR FLUSH WITH GROUND TO THE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS PROFESSIONAL NUMBER.
- BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BT2 BURIED 5/8 INCH REBAR 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE. BT3 IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS PROFESSIONAL NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS PROFESSIONAL NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH REBAR 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS PROFESSIONAL NUMBER.
- PERMANENT SURVEY MARKER, IDOT STD. 2135 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS)
COUNTY OF WILL)

THIS IS TO CERTIFY THAT WE, CLAASSEN, WHITE & ASSOCIATES, P.C., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION NUMBER 184-004039, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 12, TOWNSHIP 37 NORTH, RANGE 9 EAST OF THE THIRD PRINCIPAL MERIDIAN, WILL COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RE-Traced. THIS SURVEY CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT JOLIET, ILLINOIS THIS 23rd DAY OF MARCH, 2012 A.D.

DAVID A. CLAASSEN
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 035-002962
LICENSE EXPIRES NOVEMBER 30, 2012
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.



SCALE: 1" = 50'

NOTE: BEARINGS, DISTANCES AND COORDINATES SHOWN HEREON ARE BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM EAST ZONE, NAD 83 (2007).

CLAASSEN, WHITE & ASSOCIATES, P.C.
121 AIRPORT DRIVE, UNIT 1
JOLIET, ILLINOIS 60431
(815) 744-3720

SHEET 15 IS A COVER SHEET AND NOT RECORDED.

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
CH 89 (95TH STREET) EXTENSION
PLAINFIELD-NAPERVILLE RD. TO BOUGHTON RD.

SECTION 01-00181-00-FP WILL COUNTY
PROJECT JOB NO. R-55-001-97
STATION 354+50 TO STATION 363+00
SCALE: 1" = 50' SHEET 15 OF 27

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196-1096

PART OF THE SE 1/4 SECTION 12, IN T37N, R9E OF THE 3RD PM, WHEATLAND TOWNSHIP, IN WILL COUNTY, ILLINOIS.

CONTRACT NO. 63647

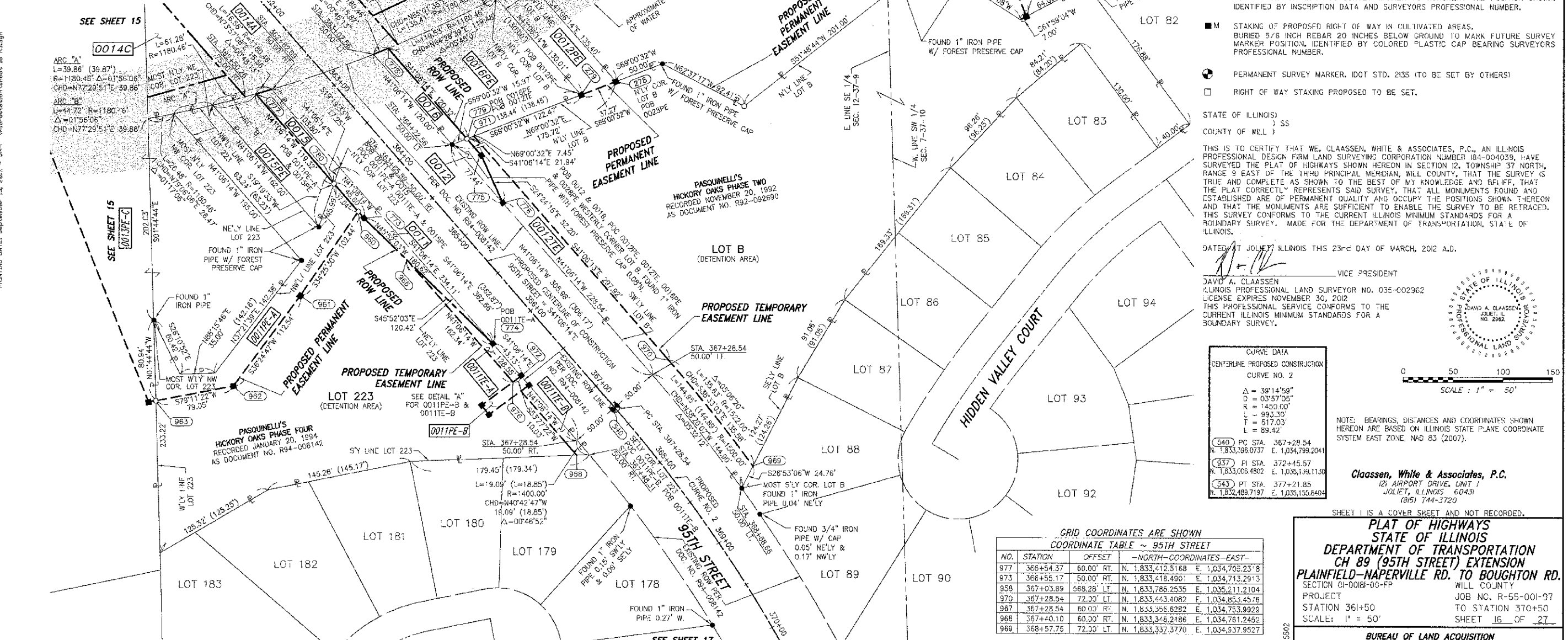
PARCEL NUMBER	OWNER	TOTAL HOLDING ACRES	PART TAKEN ACRES	REMAINDER ACRES	PREV. DED. ACRES	EASEMENT ACRES	EASEMENT PURPOSE	PERMANENT TAX INDEX NUMBER	PROPERTY ACQUIRED BY
0011	THE VILLAGE OF BOLINGBROOK	1.831	0.048	1.783		0.306		07-01-12-404-032	
0011PE-A						0.014			
0011PE-B						0.021			
0011PE-C						0.020			
0012	VILLAGE OF BOLINGBROOK	3.330	0.017	3.313		0.107		07-01-12-401-001	
0012PE						0.206			
0012E						0.129			
0015	FOREST PRESERVE DISTRICT OF WILL COUNTY	0.167	0.038	0.129		0.129		07-01-12-400-008	
0015PE						0.328			
0015E						0.328			
0016PE	FOREST PRESERVE DISTRICT OF WILL COUNTY	0.389	0.041	0.328		0.328		07-01-12-400-008	
0016E						2.257		12-02-07-300-013	

GRID COORDINATES ARE SHOWN
COORDINATE TABLE ~ 95TH STREET

NO.	STATION	OFFSET	NORTH COORDINATES	EAST COORDINATES
777	362+55.08	65.00' RT.	N. 1,833,710.1679	E. 1,034,435.9587
778	363+07.74	65.00' LT.	N. 1,833,755.8824	E. 1,034,571.5346
780	363+74.40	65.00' RT.	N. 1,833,620.1952	E. 1,034,517.4029
959	363+83.47	682.24' T.	N. 1,834,104.5472	E. 1,035,086.3490
960	364+11.44	65.00' RT.	N. 1,833,592.2853	E. 1,034,541.7534
773	364+20.00	65.00' RT.	N. 1,833,685.8347	E. 1,034,547.3814
963	364+21.49	342.33' RT.	N. 1,833,402.3682	E. 1,034,339.3862
779	364+28.06	65.00' LT.	N. 1,833,685.2296	E. 1,034,650.6353
971	364+30.62	72.00' RT.	N. 1,833,667.9000	E. 1,034,657.9951
961	364+37.04	764.9' RT.	N. 1,833,507.7847	E. 1,034,483.8404
775	364+50.00	65.00' LT.	N. 1,833,648.6943	E. 1,034,665.0618
962	364+61.37	274.07' RT.	N. 1,833,417.2154	E. 1,034,417.0352
279	364+70.17	180.00' T.	N. 1,833,709.1004	E. 1,034,704.9702
966	364+80.00	60.00' RT.	N. 1,833,543.9106	E. 1,034,590.5945
278	364+82.89	215.00' T.	N. 1,833,222.4524	E. 1,034,799.7756

GRID COORDINATES ARE SHOWN
COORDINATE TABLE ~ 95TH STREET

NO.	STATION	OFFSET	NORTH COORDINATES	EAST COORDINATES
776	366+00.00	50.00' LT.	N. 1,833,801.1569	E. 1,034,686.6302
774	366+00.00	50.00' RT.	N. 1,833,460.0624	E. 1,034,677.0209
975	366+39.23	98.89' RT.	N. 1,833,398.3587	E. 1,034,665.9758
976	366+42.34	60.00' RT.	N. 1,833,421.5817	E. 1,034,697.3179
972	366+43.3	50.00' RT.	N. 1,833,427.5610	E. 1,034,705.3771
974	366+51.20	99.84' RT.	N. 1,833,389.7179	E. 1,034,673.1211



LEGEND

SECTION CORNER (9/16/15) QUARTER SECTION CORNER (15/15)

SECTION LINE
QUARTER SECTION LINE
QUARTER, QUARTER SECTION LINE
PLATTED LOT LINE
PROPERTY (DEED) LINE

APL APPARENT PROPERTY LINE
EXISTING CENTERLINE
PROPOSED CENTERLINE
EXISTING RIGHT OF WAY LINE
PROPOSED RIGHT OF WAY LINE
PROPOSED EASEMENT
MEASURED DIMENSION
COMPUTED DIMENSION
RECORD DATA

EXISTING BUILDING

IRON PIPE OR ROD FOUND ○ SET 5/8" x 30' REBAR
CUT CROSS FOUND OR SET ● PK FOUND PK NAIL ○ PK SET PK NAIL

TI THESE STAKES REFERENCE FOUND OR SET MONUMENTATION
I2 SET 5/8 INCH REBAR PLUS 4 WITH GROUND TO THE FOUND IRON STAKE, IDENTIFIED
I3 BY COLORED PLASTIC CAP AND BEARING SURVEYORS PROFESSIONAL NUMBER.

BTI THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION.
BT2 IDENTIFIED 5/8 INCH REBAR 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE,
BT3 IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS PROFESSIONAL NUMBER.

STAKING OF PROPOSED RIGHT OF WAY.
SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN,
IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS PROFESSIONAL NUMBER.

STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS.
BURY 5/8 INCH REBAR 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY
MARKER POSITION, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS
PROFESSIONAL NUMBER.

PERMANENT SURVEY MARKER, DOT STD. 2135 (TO BE SET BY OTHERS)

RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS)
COUNTY OF WILL) SS

THIS IS TO CERTIFY THAT WE, CLAASSEN, WHITE & ASSOCIATES, P.C., AN ILLINOIS PROFESSIONAL LAND SURVEYING CORPORATION NUMBER 184-004039, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 12, TOWNSHIP 37 NORTH, RANGE 9 EAST OF THE THIRD PRINCIPAL MERIDIAN, WILL COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. THIS SURVEY CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT JOLIET, ILLINOIS THIS 23rd DAY OF MARCH, 2012 A.D.

DAVID A. CLAASSEN VICE PRESIDENT
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 036-002962
LICENSE EXPIRES NOVEMBER 30, 2012
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

CURVE DATA
CENTERLINE PROPOSED CONSTRUCTION
CURVE NO. 2

Δ = 39°14'58"
D = 03°57'05"
R = 1450.00'
L = 993.30'
T = 517.03'
E = 89.42'

NOTE: BEARINGS, DISTANCES AND COORDINATES SHOWN HEREON ARE BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM EAST ZONE, NAD 83 (2007).

540 PC STA. 367+28.54
N. 1,833,396.0737 E. 1,034,799.2041

937 PI STA. 372+45.57
N. 1,833,006.4802 E. 1,035,139.1150

543 PT STA. 377+21.85
N. 1,832,489.7187 E. 1,035,155.6404

CLAASSEN, WHITE & ASSOCIATES, P.C.
121 AIRPORT DRIVE, UNIT 1
JOLIET, ILLINOIS 60431
(815) 744-3720

GRID COORDINATES ARE SHOWN
COORDINATE TABLE ~ 95TH STREET

NO.	STATION	OFFSET	NORTH COORDINATES	EAST COORDINATES
977	366+54.37	60.00' RT.	N. 1,833,412.5168	E. 1,034,706.2378
973	366+55.17	50.00' RT.	N. 1,833,418.4901	E. 1,034,713.2973
958	367+03.89	568.28' LT.	N. 1,833,788.2536	E. 1,036,211.2104
970	367+28.54	72.00' LT.	N. 1,833,443.4082	E. 1,034,853.4576
967	367+28.54	60.00' RT.	N. 1,833,366.6282	E. 1,034,763.9929
968	367+40.10	60.00' RT.	N. 1,833,348.2486	E. 1,034,761.2452
969	368+57.75	72.00' LT.	N. 1,833,337.3770	E. 1,034,937.9527

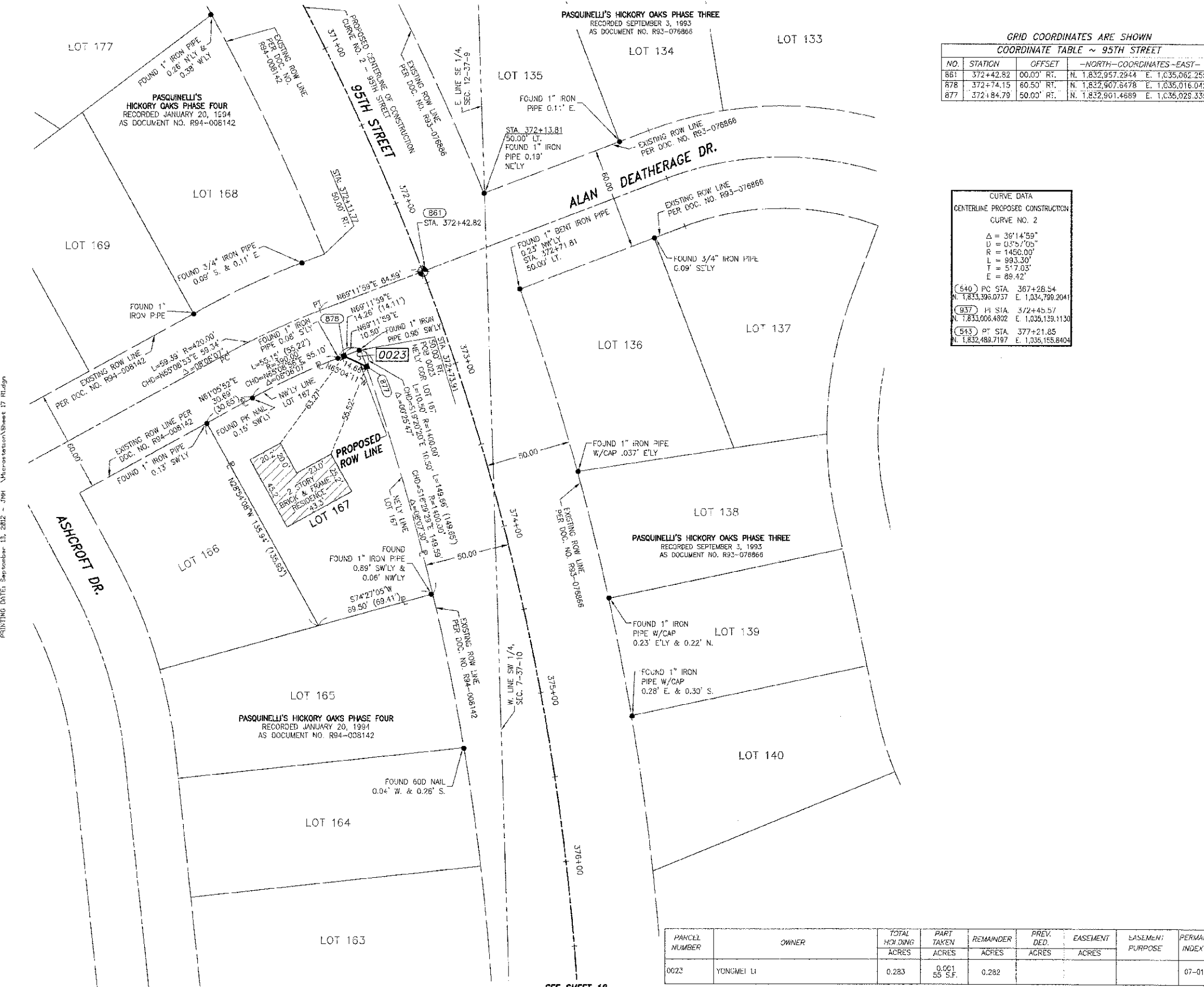
SHEET 1 IS A COVER SHEET AND NOT RECORDED.

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
CH 89 (95TH STREET) EXTENSION
PLAINFIELD-NAPERVILLE RD. TO BOUGHTON RD.

SECTION 01-00181-00-FP WILL COUNTY
PROJECT JOB NO. R-55-001-97
STATION 361+50 TO STATION 370+50
SCALE: 1" = 50' SHEET 16 OF 27

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196-1096

PART OF THE SE 1/4 SECTION 12, IN T37N, R9E OF THE 3rd PM, WHEATLAND TOWNSHIP, AND PART OF THE SW 1/4 OF SECTION 7, IN T37N, R10E, DUPAGE TOWNSHIP, BOTH IN WILL COUNTY, ILLINOIS.



PRINTING DATE: September 13, 2012 - 3:04 PM Microstation/Sheet 17 HJ.dgn

CONTRACT NO. 63647

GRID COORDINATES ARE SHOWN
COORDINATE TABLE ~ 95TH STREET

NO.	STATION	OFFSET	-NORTH-COORDINATES-EAST-
861	372+42.82	00.00' RT.	N. 1,832,957.2944 E. 1,035,062.2558
878	372+74.15	60.50' RT.	N. 1,832,907.8478 E. 1,035,016.0428
877	372+84.79	50.00' RT.	N. 1,832,901.4689 E. 1,035,029.3354

CURVE DATA
CENTERLINE PROPOSED CONSTRUCTION
CURVE NO. 2

Δ = 30°14'59"	PC STA. 367+28.54	N. 1,833,396.0737	E. 1,034,799.2041
D = 03°51'05"	PI STA. 372+45.57	N. 1,833,008.4802	E. 1,035,139.1130
R = 1450.00'	PT STA. 377+21.85	N. 1,832,483.7197	E. 1,035,155.8404
L = 993.30'			
T = 57.03'			
E = 89.42'			

LEGEND

SECTION CORNER (9/10, 16/15)
QUARTER SECTION CORNER (16/15)

SECTION LINE
QUARTER SECTION LINE
PLATTED LOT LINE
PROPERTY BOUNDARY LINE

APPARENT PROPERTY LINE
EXISTING CENTERLINE
PROPOSED CENTERLINE
EXISTING RIGHT OF WAY LINE
PROPOSED RIGHT OF WAY LINE
PROPOSED EASEMENT

MEASURED DIMENSION (29.32')
COMPUTED DIMENSION (29.32')
RECORD DATA

EXISTING BUILDING

BEARINGS ARE BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM EAST ZONE, NAD 83 (2007)

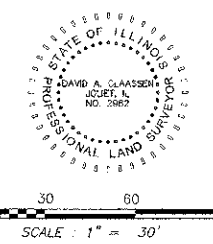
- IRON PIPE OR RCD FOUND ○ SET 5/8" x 30" REBAR
- + CUT CROSS FOUND OR SET ● PK FOUND PK NAIL ○ PK SET PK NAIL
- T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION
T2 SET 5/8 INCH REBAR FLUSH WITH GROUND TO THE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP AND BEARING SURVEYORS PROFESSIONAL NUMBER.
- RT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION.
RT2 BURIED 5/8 INCH REBAR 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE.
RT3 IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS PROFESSIONAL NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY.
SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS PROFESSIONAL NUMBER.
- M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS.
BURIED 5/8 INCH REBAR 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS PROFESSIONAL NUMBER.
- PERMANENT SURVEY MARKER, IDOT STD. 2155 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS)
COUNTY OF WILL) SS

THIS IS TO CERTIFY THAT WE, CLAASSEN, WHITE & ASSOCIATES, P.C., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION NUMBER 184-004039, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 12, TOWNSHIP 37 NORTH, RANGE 9 EAST AND IN SECTION 7, TOWNSHIP 37 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, WILL COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. THIS SURVEY CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT JOLIET, ILLINOIS THIS 23rd DAY OF MARCH, 2012 A.D.

David A. Claassen
DAVID A. CLAASSEN, VICE PRESIDENT
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 035-002962
LICENSE EXPIRES NOVEMBER 30, 2012
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.



NOTE: BEARINGS, DISTANCES AND COORDINATES SHOWN HEREON ARE BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM EAST ZONE, NAD 83 (2007).

Claassen, White & Associates, P.C.
121 AIRPORT DRIVE, UNIT 1
JOLIET, ILLINOIS 60431
(815) 744-3720

SHEET 17 IS A COVER SHEET AND NOT RECORDED.

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
CH 89 (95TH STREET) EXTENSION
PLAINFIELD-NAPERVILLE RD. TO BOUGHTON RD.

SECTION 01-00181-00-FP WILL COUNTY
PROJECT JOB NO. R-55-001-97
STATION 372+00.89 TO STATION 372+88.73
SCALE: 1" = 30' SHEET 17 OF 27

PANCLL NUMBER	OWNER	TOTAL HOLDING ACRES	PART TAKEN ACRES	REMAINDER ACRES	PREV. DED. ACRES	EASEMENT ACRES	EASEMENT PURPOSE	PERMANENT TAX INDEX NUMBER	PROPERTY ACQUIRED BY
0023	YONGMEI LI	0.283	0.001 65 S.F.	0.282				07-01-12-407-002	

PART OF SECTION 7, IN T37N, R10E OF THE 3rd PM, DUPAGE TOWNSHIP, WILL COUNTY, ILLINOIS.
SEE SHEET 17

CONTRACT NO. 63647

CURVE DATA		CURVE DATA	
CENTERLINE PROPOSED CONSTRUCTION	CURVE NO. 2	CENTERLINE PROPOSED CONSTRUCTION	CURVE NO. 3
$\Delta = 38^{\circ}14'59''$		$\Delta = 24^{\circ}45'57''$	
$D = 03^{\circ}57'05''$		$D = 11^{\circ}27'33''$	
$H = 1450.00'$		$R = 500.00'$	
$L = 993.30'$		$L = 218.12'$	
$T = 517.03'$		$T = 109.78'$	
$E = 89.42'$		$E = 11.91'$	
540 PC STA. 367+26.54		581 PC STA. 377+26.40	
N. 1,833,396.8737 E. 1,034,799.2041		N. 1,832,483.1748 E. 1,035,156.0523	
937 PI STA. 372+45.57		938 PI STA. 378+38.18	
N. 1,833,206.4802 E. 1,035,139.1130		N. 1,832,373.4968 E. 1,035,159.8039	
543 PT STA. 377+21.85		585 PT STA. 379+44.52	
N. 1,832,489.7197 E. 1,035,155.8404		N. 1,832,275.3178 E. 1,035,208.7906	

GRID COORDINATES ARE SHOWN
COORDINATE TABLE ~ 95TH STREET

NO.	STATION	OFFSET	NORTH-COORDINATES	EAST
730	380+98.18	50.00' LT.	N. 1,832,180.3465	E. 1,035,322.3418
731	381+42.32	94.14' LT.	N. 1,832,140.6620	E. 1,035,381.5835
351	381+92.32	00.00' RT.	N. 1,832,053.7800	E. 1,035,319.8238

GRID COORDINATES ARE SHOWN
COORDINATE TABLE ~ BOUGHTON ROAD

NO.	STATION	OFFSET	NORTH-COORDINATES	EAST
730	60+84.85	94.14' LT.	N. 1,832,160.3465	E. 1,035,322.3418
731	61+29.00	50.00' LT.	N. 1,832,140.6620	E. 1,035,381.5835
718	63+80.00	50.00' LT.	N. 1,832,283.1288	E. 1,035,605.9814
719	63+80.00	55.00' LT.	N. 1,832,257.5988	E. 1,035,603.7471
720	63+80.00	65.00' LT.	N. 1,832,268.5388	E. 1,035,599.2604
844	64+30.55	65.00' LT.	N. 1,832,289.1908	E. 1,035,644.4564
845	64+37.36	55.00' LT.	N. 1,832,283.3002	E. 1,035,656.0214

SECTION CORNER

QUARTER SECTION CORNER

LEGEND

- SECTION LINE
- QUARTER SECTION LINE
- QUARTER, QUARTER SECTION LINE
- PLATTED LOT LINE
- PROPERTY (DEED) LINE
- APL APPARENT PROPERTY LINE
- EXISTING CENTERLINE
- PROPOSED CENTERLINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED EASEMENT
- MEASURED DIMENSION
- COMPUTED DIMENSION
- RECORD DATA
- EXISTING BUILDING

IRON PIPE OR ROD FOUND

CUT CROSS FOUND OR SET

THESE STAKES REFERENCE FOUND OR SET MONUMENTATION

T2 SET 5/8 INCH REBAR FLUSH WITH GROUND TO TIE FOUND IRON STAKE, IDENTIFIED BY T3 BY COLORED PLASTIC CAP BEARING SURVEYORS PROFESSIONAL NUMBER.

B1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. B2 BURIED 5/8 INCH REBAR 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. B3 IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS PROFESSIONAL NUMBER.

STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS PROFESSIONAL NUMBER.

M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH REBAR 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS PROFESSIONAL NUMBER.

PERMANENT SURVEY MARKER, DOT STD. 2135 (TO BE SET BY OTHERS)

RIGHT OF WAY STAKING PROPOSED TO BE SET.

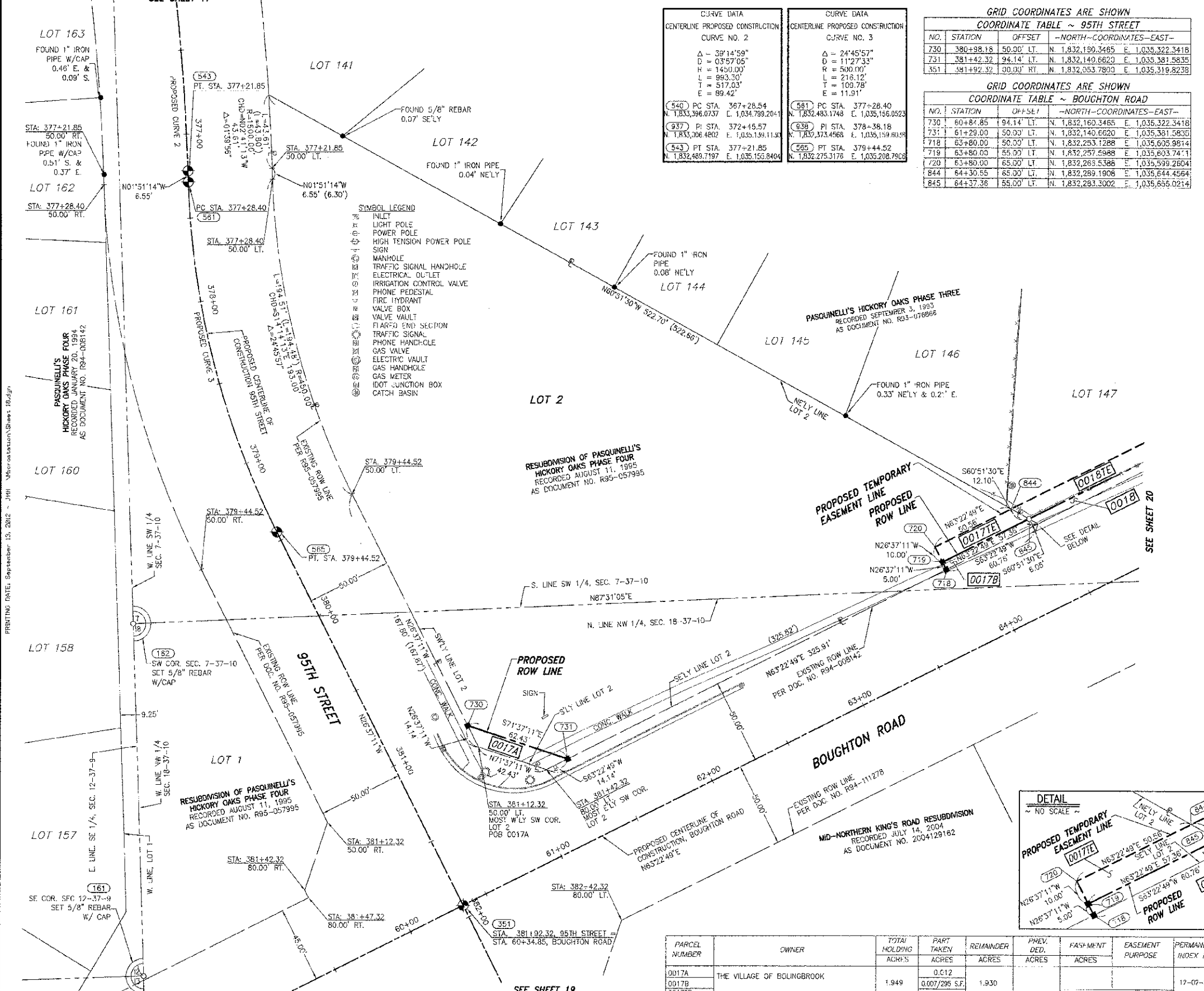
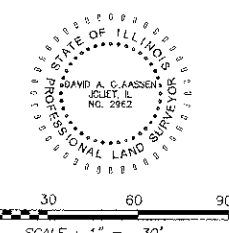
- IRON PIPE OR ROD FOUND
- SET 5/8" x 30" REBAR
- CUT CROSS FOUND OR SET
- FOUND PK NAIL
- SET PK NAIL

STATE OF ILLINOIS)
COUNTY OF WILL)

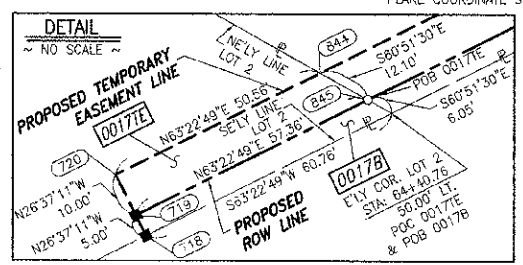
THIS IS TO CERTIFY THAT WE, CLAASSEN, WHITE & ASSOCIATES, P.C., AN ILLINOIS PROFESSIONAL DESIGN FIRM AND SURVEYING CORPORATION NUMBER 184-004039, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 7, TOWNSHIP 37 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, WILL COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN HEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. THIS SURVEY CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT JOLIET, ILLINOIS THIS 23rd DAY OF MARCH, 2012 A.D.

DAVID A. CLAASSEN
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 035-002962
LICENSE EXPIRES NOVEMBER 30, 2012
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.



PARCEL NUMBER	OWNER	TOTAL HOLDING ACRES	PART TAKEN ACRES	REMAINDER ACRES	PHEV. DED. ACRES	FASHPMENT ACRES	EASEMENT PURPOSE	PERMANENT TAX INDEX NUMBER	PROPERTY ACQUIRED BY
0017A	THE VILLAGE OF BOLINGBROOK	1.949	0.012	1.930			CONSTRUCTION	17-07-07-304-316	
0017B									
0017E									



NOTE: BEARINGS, DISTANCES AND COORDINATES SHOWN HEREON ARE BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM EAST ZONE, NAD 83 (2007).

Classten, White & Associates, P.C.
121 AIRPORT DRIVE, UNIT 1
JOLIET, ILLINOIS 60431
(815) 744-3720

SHEET 1 IS A COVER SHEET AND NOT RECORDED.

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
CH 89 (95TH STREET) EXTENSION
PLAINFIELD-NAPERVILLE RD. TO BOUGHTON RD.

SECTION 01-00181-00-FP WILL COUNTY
PROJECT JOB NO. R-55-001-97
STATION 380+98.18 TO STATION 381+92.32
SCALE: 1" = 30' SHEET 18 OF 21

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196-1096

PART OF THE NW 1/4 SECTION 18 IN T37N, R10E OF THE 3rd PM, DUPAGE TOWNSHIP, WILL COUNTY, ILLINOIS.

CONTRACT NO. 63647

GRID COORDINATES ARE SHOWN
COORDINATE TABLE ~ 95TH STREET

NO.	STATION	OFFSET	-NORTH-COORDINATES-EAST-
351	381+92.32	60.00' RT.	N. 1,832,093.7800 E. 1,035,319.8238

LEGEND

SECTION CORNER (9/10/16/15)
QUARTER SECTION CORNER (6/15)

SECTION LINE
QUARTER SECTION LINE
QUARTER, QUARTER SECTION LINE
PLATTED LOT LINE
PROPERTY DEED LINE

AFL APPARENT PROPERTY LINE

EXISTING CENTERLINE
PROPOSED CENTERLINE
EXISTING RIGHT OF WAY LINE
PROPOSED RIGHT OF WAY LINE
PROPOSED EASEMENT

MEASURED DIMENSION (29.32')
COMPUTED DIMENSION (29.32')
RECORD DATA

EXISTING BUILDING

BEARINGS ARE BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM EAST ZONE, NAD 83 (2007)

- SYMBOL LEGEND**
- INLET
 - LIGHT POLE
 - POWER POLE
 - HIGH TENSION POWER POLE SIGN
 - MANHOLE
 - TRAFFIC SIGNAL HANDHOLE
 - ELECTRICAL OUTLET
 - IRRIGATION CONTROL VALVE
 - PHONE PEDESTAL
 - FIRE HYDRANT
 - VALVE BOX
 - VALVE VALV
 - FLARED END SECTION
 - TRAFFIC SIGNAL
 - PHONE HANDHOLE
 - GAS VALVE
 - ELECTRIC VAULT
 - GAS HANDHOLE
 - GAS METER
 - 100' JUNCTION BOX
 - CATCH BASIN

CURVE DATA
CENTERLINE PROPOSED CONSTRUCTION
CURVE NO. 4

$\Delta = 30^{\circ}00'00''$
 $D = 11^{\circ}27'33''$
 $R = 500.00'$
 $L = 261.80'$
 $T = 133.98'$
 $E = 17.64'$

352 PC STA. 382+83.35
N. 1,831,972.3992 E. 1,035,360.6114

353 PI STA. 384+17.33
N. 1,831,852.6259 E. 1,035,420.6410

354 PT STA. 385+45.15
N. 1,831,718.8846 E. 1,035,412.4115

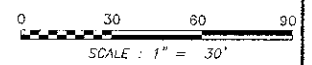
- IRON PIPE OR ROD FOUND ○ SET 5/8" x 30" REBAR
- + CUT CROSS FOUND OR SET ● PK FOUND PK NAIL ○ PK SET PK NAIL
- T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION
T2 SET 5/8 INCH REBAR FLUSH WITH GROUND TO THE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP AND BEARING SURVEYORS PROFESSIONAL NUMBER.
- BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION.
BT2 BURIED 5/8 INCH REBAR 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE.
BT3 IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS PROFESSIONAL NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY.
SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS PROFESSIONAL NUMBER.
- M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS.
BURIED 5/8 INCH REBAR 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS PROFESSIONAL NUMBER.
- PERMANENT SURVEY MARKER, 100' STD. 2135 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS)
COUNTY OF WILL) SS

THIS IS TO CERTIFY THAT WE, CLAASSEN, WHITE & ASSOCIATES, P.C., AN ILLINOIS PROFESSIONAL DESIGN FIRM AND SURVEYING CORPORATION NUMBER 184-004039, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 18, TOWNSHIP 37 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, WILL COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. THIS SURVEY CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT JOLIET, ILLINOIS THIS 23rd DAY OF MARCH, 2012 A.D.

David A. Claassen
DAVID A. CLAASSEN VICE PRESIDENT
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 035-002962
LICENSE EXPIRES NOVEMBER 30, 2012
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.



NOTE: BEARINGS, DISTANCES AND COORDINATES SHOWN HEREON ARE BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM EAST ZONE, NAD 83 (2007).

Claassen, White & Associates, P.C.
121 AIRPORT DRIVE, UNIT 1
JOLIET, ILLINOIS 60431
(815) 744-3720

SHEET 1 IS A COVER SHEET AND NOT RECORDED.

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
CH 89 (95TH STREET) EXTENSION
PLAINFIELD-NAPERVILLE RD. TO BOUGHTON RD.

SECTION 01-00181-00-FP DUPAGE COUNTY
PROJECT JOB NO. R-55-001-97
STATION 380+50 TO STATION 385+00
SCALE: 1" = 30' SHEET 19 OF 27

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196-1096

PRINTING DATE: September 13, 2012 - JMH Microstation/Sheet 19 Ruled

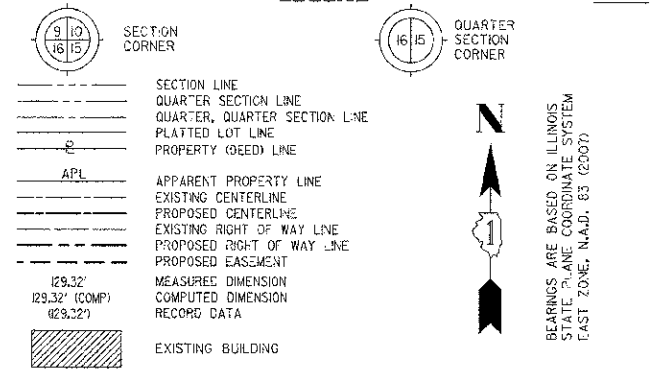
PART OF THE SW 1/4 SECTION 7 IN T37N, R10E OF THE 3rd PM, DUPAGE TOWNSHIP, WILL COUNTY, ILLINOIS.

LEGEND CONTRACT NO. 63647

PARCEL NUMBER	OWNER	TOTAL HOLDING ACRES	PART TAKEN ACRES	REMAINDER ACRES	PREV. DED. ACRES	EASEMENT ACRES	EASEMENT PURPOSE	PERMANENT TAX INDEX NUMBER	PROPERTY ACQUIRED BY
0018	CAROL BLAKLEY AND GREGORY S. BLAKLEY, AS JOINT TENANTS	0.320	0.011	0.309		0.024	CONSTRUCTION	12-02-07-304-013	
0019	SANDEEP MALHOTRA AND RAVINDER MALHOTRA, HUSBAND AND WIFE	0.255	0.008/366 S.F.	0.247		0.025	CONSTRUCTION	12-02-07-304-014	
0020	BLAKE N. HEFFELFINGER AND JANET M. HEFFELFINGER, IN JOINT TENANCY	0.219		0.219		0.019	CONSTRUCTION	12-02-07-304-015	

GRID COORDINATES ARE SHOWN
COORDINATE TABLE ~ BOUGHTON ROAD

NO.	STATION	OFFSET	-NORTH-COORDINATES-EAST-	
844	64+30.56	65.00' LT.	N. 1,832,289.1908	E. 1,035,644.4564
845	64+37.36	55.00' LT.	N. 1,832,283.3002	E. 1,035,655.0214
847	65+37.33	55.00' LT.	N. 1,832,328.0926	E. 1,035,744.3928
846	65+39.86	65.00' LT.	N. 1,832,338.1663	E. 1,035,742.1742
721	63+73.29	55.00' LT.	N. 1,832,344.2057	E. 1,035,778.5424
848	68+33.87	65.00' LT.	N. 1,832,380.2912	E. 1,035,826.2233
849	68+33.87	51.05' LT.	N. 1,832,367.8209	E. 1,035,832.4733
839	68+33.87	50.00' LT.	N. 1,832,366.8815	E. 1,035,832.9443
723	65+55.00	65.00' LT.	N. 1,832,489.7573	E. 1,035,845.1104
724	68+55.00	60.00' LT.	N. 1,832,386.2873	E. 1,035,847.3507
725	67+03.92	60.00' LT.	N. 1,832,407.2052	E. 1,035,891.0821
726	67+03.92	50.00' LT.	N. 1,832,398.2652	E. 1,035,893.5627



- IRON PIPE OR ROD FOUND ○ SET 5/8" x 30" REBAR
- + CUT CROSS FOUND OR SET ● PK FOUND PK NAIL ○ PK SET PK NAIL
- T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION
T2 SET 5/8 INCH REBAR FLUSH WITH GROUND TO THE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP AND BEARING SURVEYORS PROFESSIONAL NUMBER.
- BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION.
BT2 BURIED 5/8 INCH REBAR 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE.
BT3 IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS PROFESSIONAL NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY.
SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS PROFESSIONAL NUMBER.
- M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS.
BURIED 5/8 INCH REBAR 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS PROFESSIONAL NUMBER.
- PERMANENT SURVEY MARKER, 100T STD. 2.35 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS)
COUNTY OF WILL) SS

I, DAVID A. CLAASSEN, PROFESSIONAL LAND SURVEYOR NO. 035-002962, LICENSE EXPIRES NOVEMBER 30, 2022. THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

THIS IS TO CERTIFY THAT WE, CLAASSEN, WHITE & ASSOCIATES, P.C., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION NUMBER 184-004039, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 7, TOWNSHIP 37 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, WILL COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. THIS SURVEY CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT JOLIET, ILLINOIS THIS 23rd DAY OF MARCH, 2012 A.D.

DAVID A. CLAASSEN VICE PRESIDENT
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 035-002962
LICENSE EXPIRES NOVEMBER 30, 2022
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

DAVID A. CLAASSEN
ILLINOIS PROFESSIONAL LAND SURVEYOR
NO. 035-002962
LICENSE EXPIRES NOVEMBER 30, 2022

0 30 60 90
SCALE: 1" = 30'

NOTE: BEARINGS, DISTANCES AND COORDINATES SHOWN HEREON ARE BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM EAST ZONE, NAD 83 (2007).

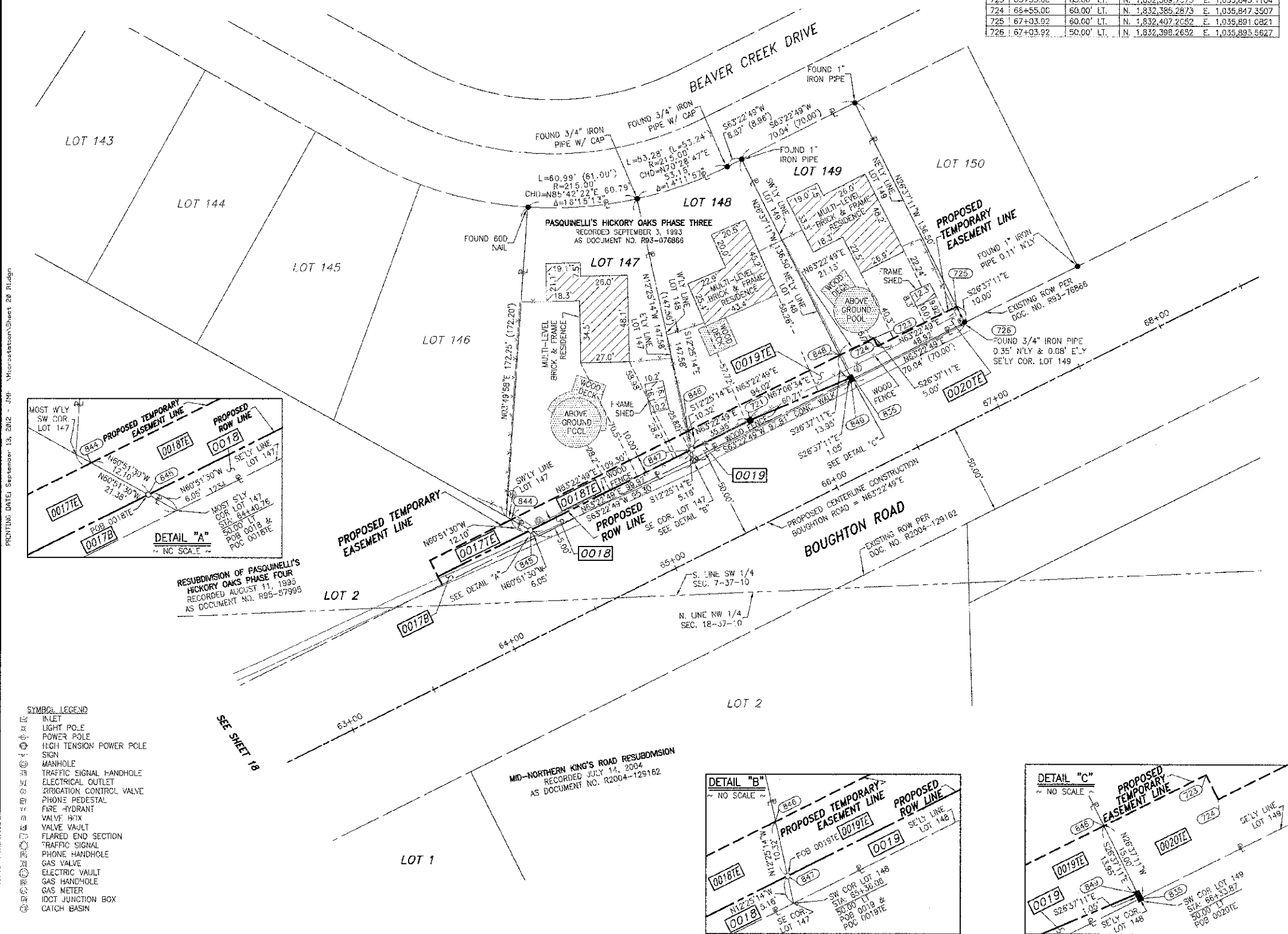
Classten, White & Associates, P.C.
121 AIRPORT DRIVE, UNIT 1
JOLIET, ILLINOIS 60431
(815) 744-3720

SHEET 1 IS A COVER SHEET AND NOT RECORDED.

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
CH 89 (95TH STREET) EXTENSION
PLAINFIELD-NAPERVILLE RD. TO BOUGHTON RD.

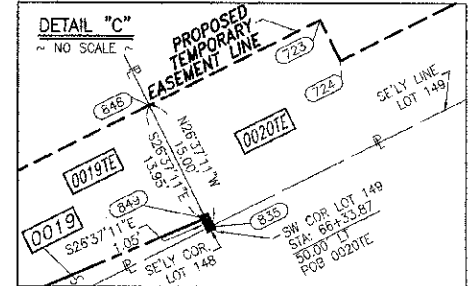
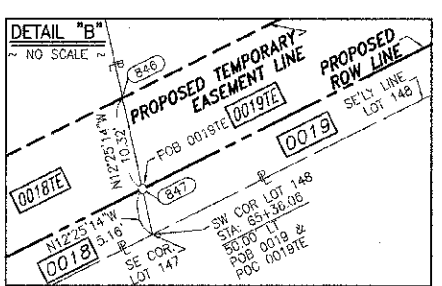
SECTION 01-00181-00-FP WILL COUNTY
PROJECT JOB NO. R-55-001-97
STATION 63+80.00 TO STATION 67+03.92
SCALE: 1" = 30' SHEET 20 OF 27

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196-1096



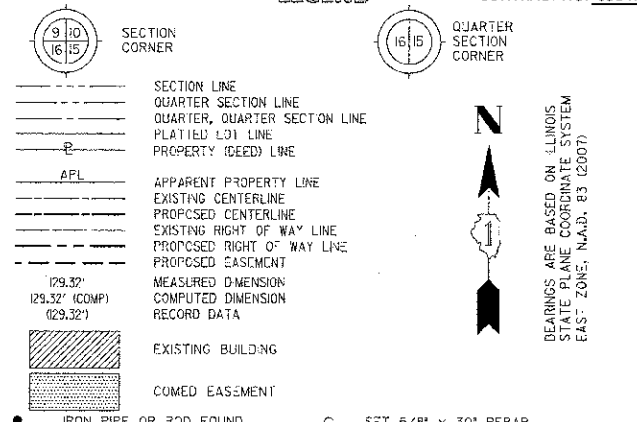
PRINTING DATE: September 13, 2012 - 10P Microstation/Sheet 20 Rldg

- SYMBOL LEGEND
- INLET
 - LIGHT POLE
 - POWER POLE
 - HIGH TENSION POWER POLE
 - SIGN
 - MANHOLE
 - TRAFFIC SIGNAL HANDHOLE
 - ELECTRICAL OUTLET
 - IRRIGATION CONTROL VALVE
 - PHONE PEDESTAL
 - FIRE HYDRANT
 - VALVE BOX
 - VALVE VAULT
 - FLARED END SECTION
 - TRAFFIC SIGNAL
 - PHONE HANDHOLE
 - GAS VALVE
 - ELECTRIC VAULT
 - GAS HANDHOLE
 - GAS METER
 - IDOT JUNCTION BOX
 - CATCH BASIN



PART OF THE NE 1/4 SECTION 12 IN T37N, R9E OF THE 3rd PM, WHEATLAND TOWNSHIP, WILL COUNTY, ILLINOIS.

LEGEND CONTRACT NO. 63647



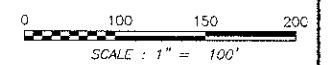
- IRON PIPE OR ROD FOUND ○ SET 5/8" x 30" REBAR
- + CUT CROSS FOUND OR SET ● PK FOUND PK NAIL ○ PK SET PK NAIL
- T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION
- T2 SET 5/8" WCH REBAR FLUSH WITH GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP AND BEARING SURVEYORS PROFESSIONAL NUMBER.
- BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION.
- BT2 BURIED 5/8" INCH REBAR 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE.
- BT3 IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS PROFESSIONAL NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS PROFESSIONAL NUMBER.
- M STAKING OF PROPOSED HIGH OF WAY IN CULTIVATED AREAS. BURIED 5/8" INCH REBAR 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS PROFESSIONAL NUMBER.
- PERMANENT SURVEY MARKER. 100" STD. 2135 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS)
 COUNTY OF WILL) SS

THIS IS TO CERTIFY THAT WE, CLAASSEN, WHITE & ASSOCIATES, P.C., AN ILLINOIS PROFESSIONAL DESIGN FIRM AND SURVEYING CORPORATION NUMBER 184-004039, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 12, TOWNSHIP 37 NORTH, RANGE 9 EAST OF THE THIRD PRINCIPAL MERIDIAN, WILL COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF. THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. THIS SURVEY CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT JOLIET, ILLINOIS THIS _____ DAY OF _____, 2011 A.D.

DAVID A. CLAASSEN
 ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 035-002962
 LICENSE EXPIRES NOVEMBER 30, 2012
 THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.



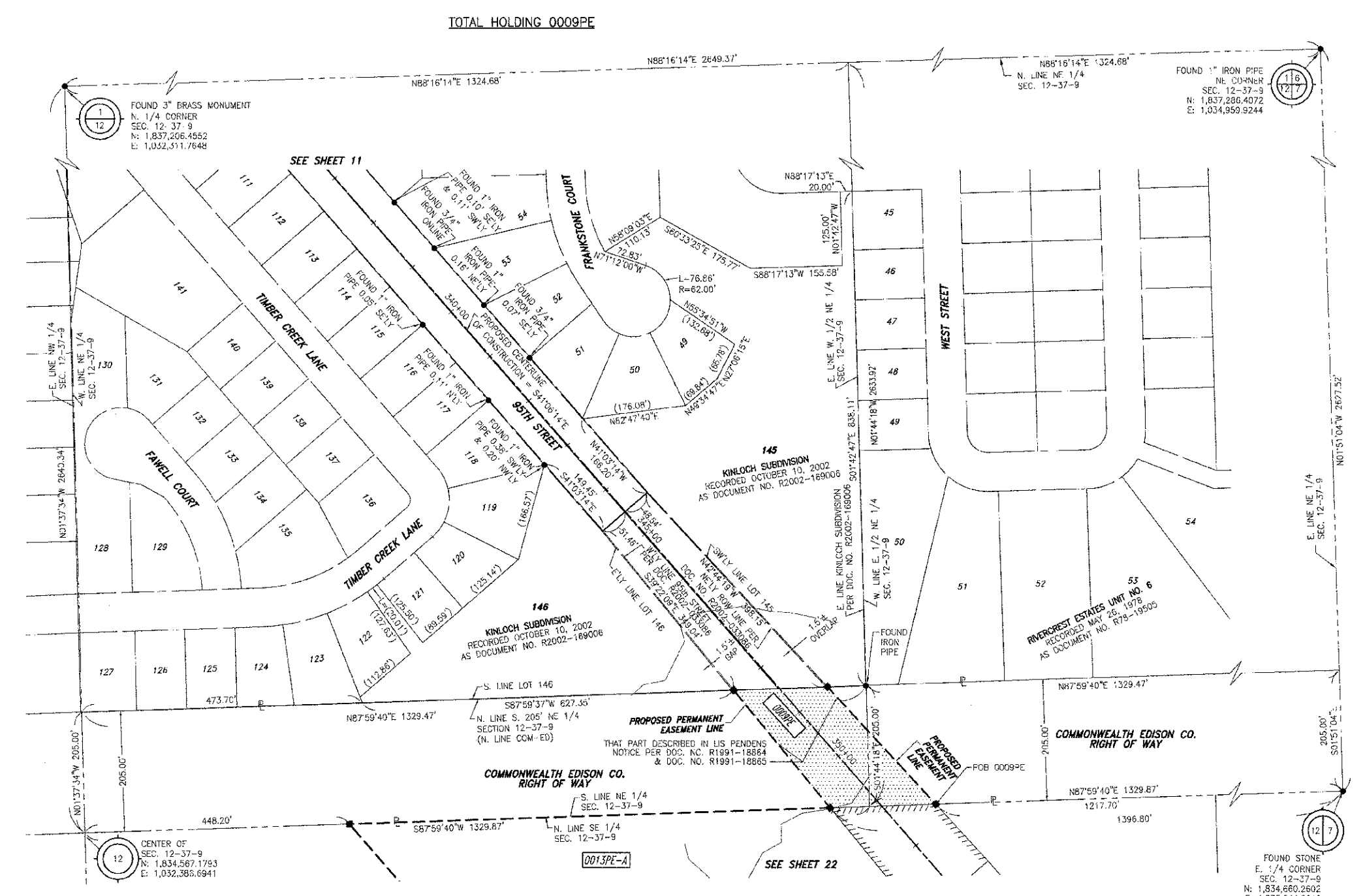
NOTE: BEARINGS, DISTANCES AND COORDINATES SHOWN HEREON ARE BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM EAST ZONE, NAD 83 (2007).

Classsen, White & Associates, P.C.
 121 AIRPORT DRIVE, UNIT 1
 JOLIET, ILLINOIS 60431
 (815) 744-3720

SHEET 1 IS A COVER SHEET AND NOT RECORDED.

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
CH 89 (95TH STREET) EXTENSION
PLAINFIELD-NAPERVILLE RD. TO BOUGHTON RD.
 SECTION 01-00181-00-FP WILL COUNTY
 PROJECT JOB NO. R-55-001-97
 STATION 342+50 TO STATION 351+50
 SCALE: 1" = 100' SHEET 2 OF 27

BUREAU OF LAND ACQUISITION
 201 WEST CENTER COURT
 SCHAUMBURG, ILLINOIS 60196-1096



PARCEL NUMBER	OWNER	TOTAL HOLDING ACRES	PART TAKEN ACRES	REMAINDER ACRES	PREV. DEC. ACRES	EASEMENT ACRES	EASEMENT PURPOSE	PERMANENT TAX INDEX NUMBER	PROPERTY ACQUIRED BY
0009PE	COMMONWEALTH EDISON COMPANY	12.515		12.515		0.796	ROADWAY	07-01-12-200-013 07-01-12-200-016	

PRINTING DATE: September 13, 2012 - 1466 Microstation/Sheet 21 R.dgn

CH 89 (95TH STREET) EXTENSION, SECTION 01-00181-00-FP, JOB. NO. R-55-001-97

REVISION DATE: 04/06/11
 08/23/12
 08/24/11
 08/18/11
 REVISION: CHANGED PARCEL 0009PE C TO 0029

REMOVED 0013PE
 ADDED HATCH TO 0009PE
 REMOVED 0009PE, 0009PE-A & B, 0010PE-R CHANGE 0029 TO 0009PE
 MADE BY: K. GREGORY

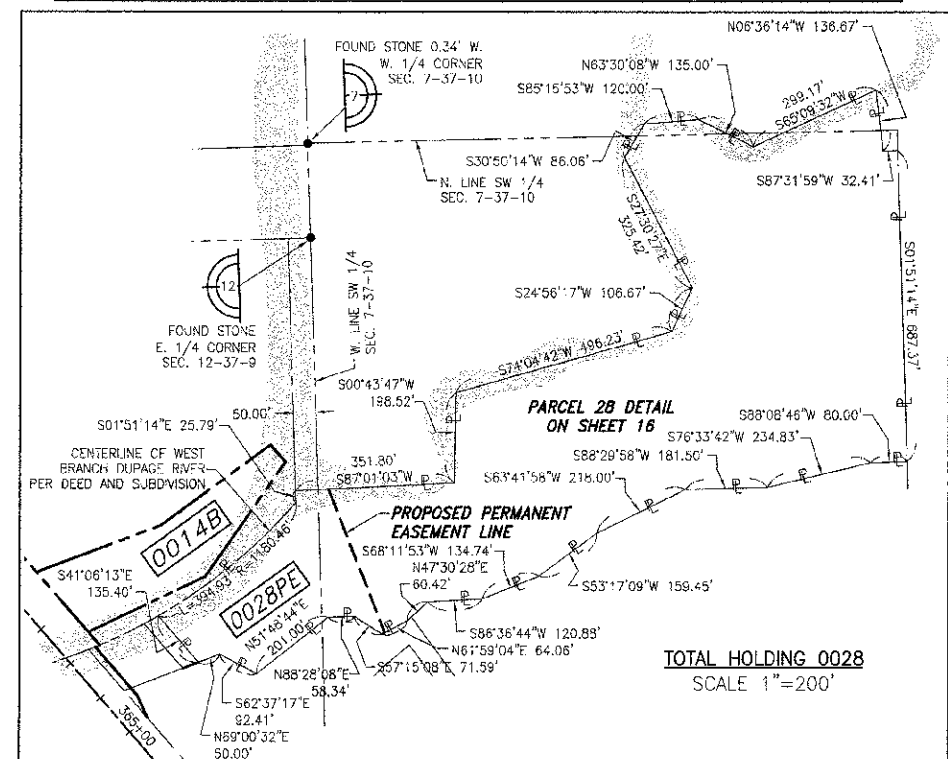
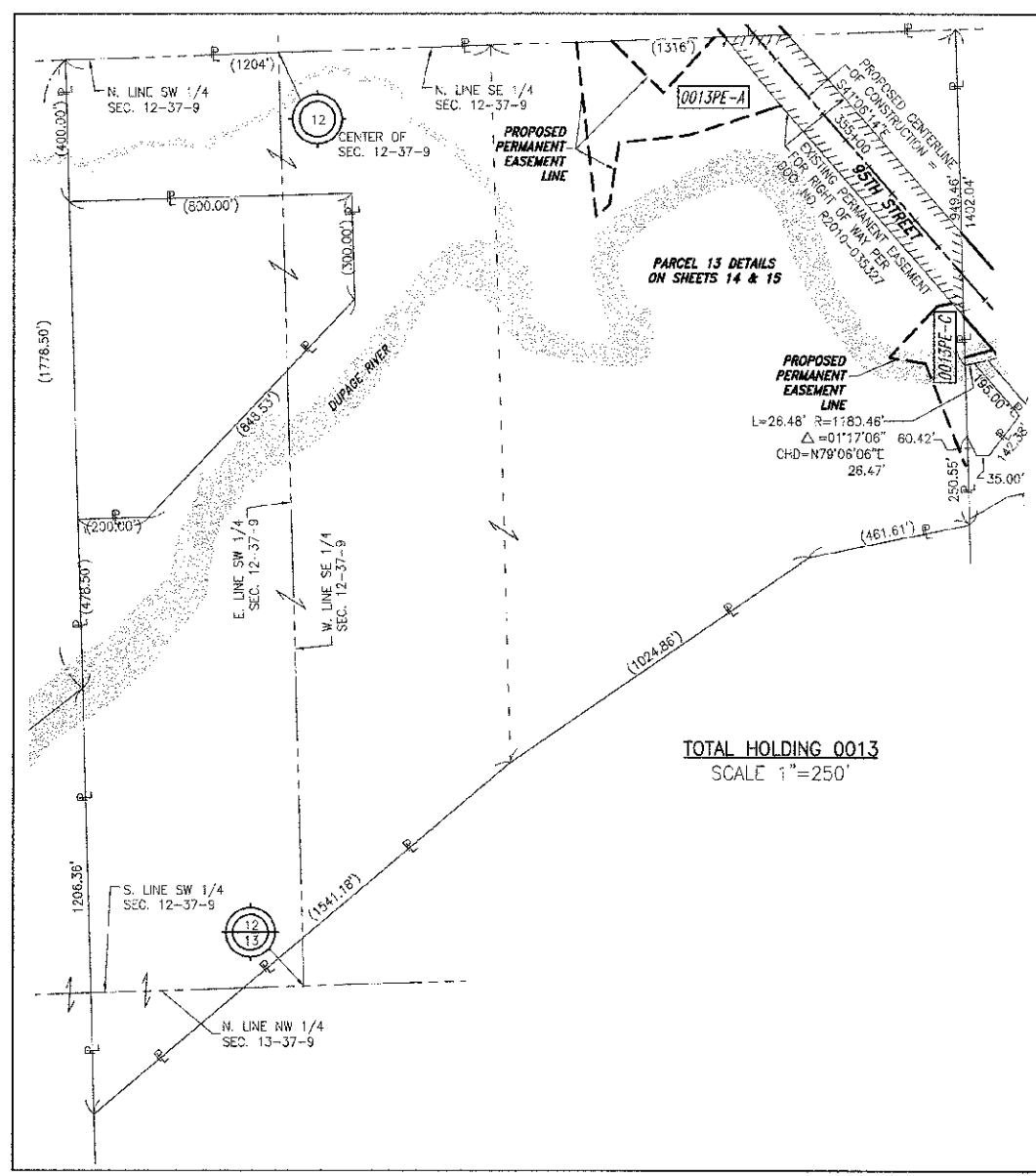
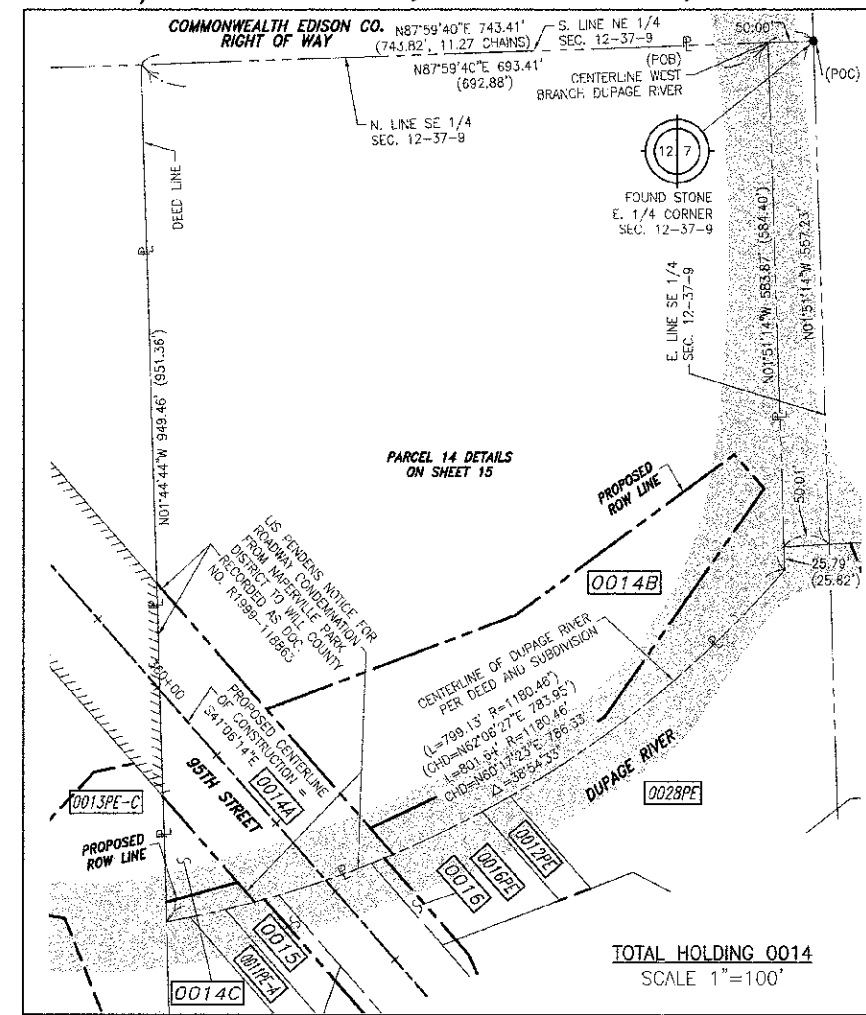
PART OF THE S. 1/4 SECTION 12 AND PART OF THE NW 1/4 SECTION 13 IN T37N, R9E OF THE 3rd PM AND PART OF THE SW 1/4 SECTION 7 IN T37N, R10E OF THE 3rd PM, DuPAGE TOWNSHIP, WILL COUNTY, ILLINOIS.

CONTRACT NO. 63647

LEGEND

- SECTION CORNER
- QUARTER SECTION CORNER
- SECTION LINE
- QUARTER SECTION LINE
- QUARTER, QUARTER SECTION LINE
- PLATTED LOT LINE
- PROPERTY (DEED) LINE
- AFL
- APPARENT PROPERTY LINE
- EXISTING CENTERLINE
- PROPOSED CENTERLINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED EASEMENT
- MEASURED DIMENSION
- COMPUTED DIMENSION
- RECORD DATA
- EXISTING BUILDING

BEARINGS ARE BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM EAST ZONE, NAD 83 (2007)



- IRON PIPE OR ROD FOUND ○ SET 5/8" x 30" REBAR
- + CUT CROSS FOUND OR SET ● PK FOUND PK NAIL ○ PK SET PK NAIL
- T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION
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- STAKING OF PROPOSED RIGHT OF WAY.
- SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS PROFESSIONAL NUMBER.
- M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS.
- BURIED 5/8 INCH REBAR 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS PROFESSIONAL NUMBER.
- PERMANENT SURVEY MARKER. (DOT 510, 2155 (10 BE SET BY OTHERS))
- RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS)
COUNTY OF WILL) SS

THIS IS TO CERTIFY THAT WE, CLAASSEN, WHITE & ASSOCIATES, P.C., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION NUMBER 184-004039, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 12, TOWNSHIP 37 NORTH, RANGE 9 EAST OF THE THIRD PRINCIPAL MERIDIAN, WILL COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RE-TRACED. THIS SURVEY CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT JOLIET, ILLINOIS THIS 23rd DAY OF MARCH, 2012 A.D.

David A. Claassen
VICE PRESIDENT

DAVID A. CLAASSEN
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 035-002962
LICENSE EXPIRES NOVEMBER 30, 2012
THIS PROFESSIONAL SERVICE CONFIRMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.



Claassen, White & Associates, P.C.
127 AIRPORT DRIVE, UNIT 1
JOLIET, ILLINOIS 60431
(815) 744-3720

SCALE: 1" = 100'

NOTE: BEARINGS, DISTANCES AND COORDINATES SHOWN HEREON ARE BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM EAST ZONE, NAD 83 (2007).

PARCEL NUMBER	OWNER	TOTAL HOLDING ACRES	PART TAKEN ACRES	REMAINDER ACRES	PREV. DEED ACRES	EASEMENT ACRES	EASEMENT PURPOSE	PERMANENT TAX INDEX NUMBER	PROPERTY ACQUIRED BY
0013PE-A	FOREST PRESERVE DISTRICT OF WILL COUNTY	107.284				2.798	DRAINAGE	07-01-12-400-010	
0013PE-C						1.493	CONSTRUCTION	07-01-12-400-015-0010 07-01-12-400-015-0020	
0014A	THE NAPERVILLE PARK DISTRICT	13.032	0.959						
0014C			1.715	10.251					07-01-12-400-064
0014D			0.107						
0028PE	FOREST PRESERVE DISTRICT OF WILL COUNTY					2.257	DRAINAGE	12-02-07-300-013	

NOTE: PARCEL 0013PE-B DELETED 8-18-2011

PRINTING DATE: September 23, 2012 - 3:04 Microstation/Sheet 22 RE.dgn

CH 89 (95TH STREET) EXTENSION, SECTION 01-00181-00-FP, JOB. NO. R-55-001-97

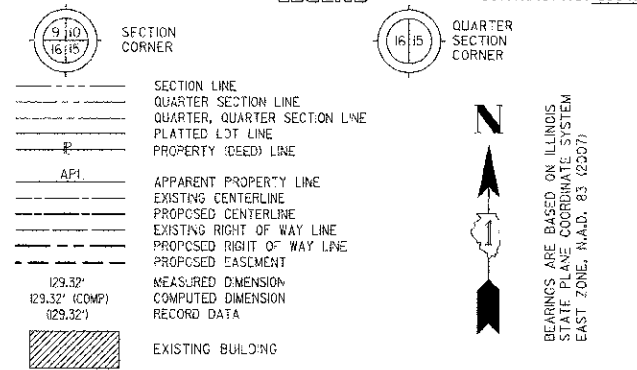
3/13/12 REVISION DATE: 8/18/11 REVISION: 0013PE-A & 0013PE-C REMOVED 0013PE-B

MADE BY: JMH

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196-1096

PART OF SECTIONS 1, 2, 11 AND 12 OF T37N, R9E OF THE 3RD PM, WHEATLAND TOWNSHIP, AND PART OF SECTIONS 7 AND 18 OF T37N, R.10E, OF THE 3RD PM, DUPAGE TOWNSHIP, BOTH IN WILL COUNTY, ILLINOIS.

LEGEND CONTRACT NO. G3647



BEARINGS ARE BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM EAST ZONE, NAD 83 (2011)

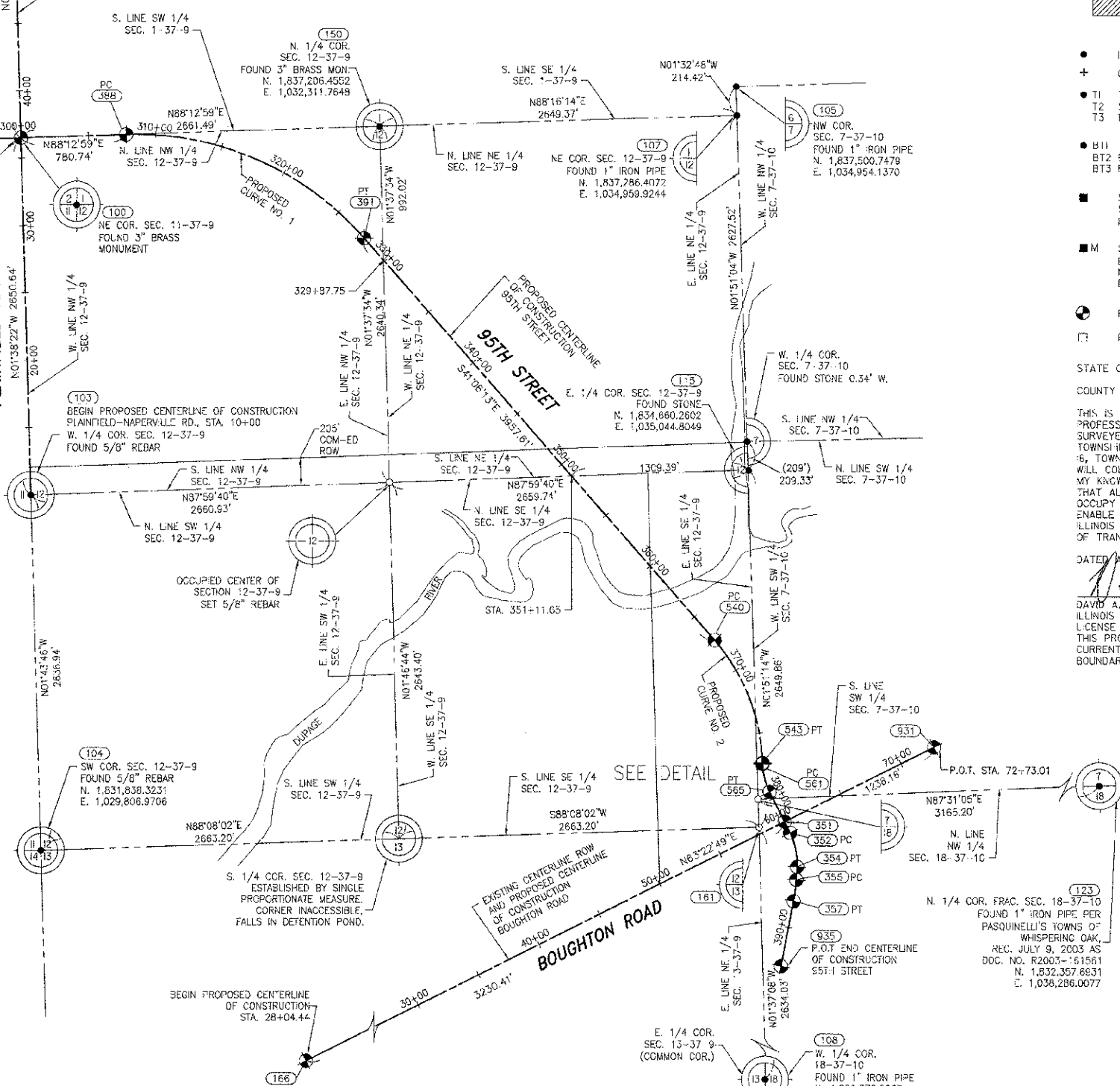
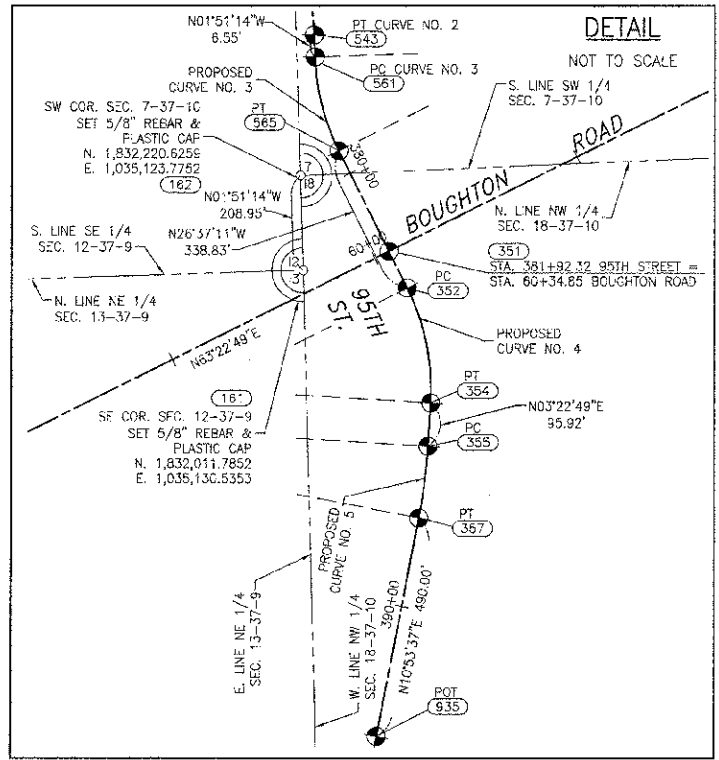
CURVE DATA	CURVE DATA	CURVE DATA	CURVE DATA	CURVE DATA
PROPOSED CENTERLINE CONSTRUCTION	PROPOSED CENTERLINE CONSTRUCTION	PROPOSED CENTERLINE CONSTRUCTION	PROPOSED CENTERLINE CONSTRUCTION	PROPOSED CENTERLINE CONSTRUCTION
CURVE NO. 1	CURVE NO. 2	CURVE NO. 3	CURVE NO. 4	CURVE NO. 5
$\Delta = 50^{\circ}40'48''$ $D = 02^{\circ}32'47''$ $R = 2250.00'$ $L = 990.19'$ $T = 1065.44'$ $E = 239.53'$	$\Delta = 39^{\circ}14'59''$ $D = 05^{\circ}57'05''$ $R = 1450.00'$ $L = 993.30'$ $T = 517.03'$ $E = 89.42'$	$\Delta = 24^{\circ}45'57''$ $D = 11^{\circ}27'33''$ $R = 500.00'$ $L = 216.12'$ $T = 109.78'$ $E = 11.91'$	$\Delta = 37^{\circ}00'00''$ $D = 11^{\circ}27'33''$ $R = 500.00'$ $L = 216.12'$ $T = 109.78'$ $E = 11.91'$	$\Delta = 07^{\circ}30'48''$ $D = 04^{\circ}41'47''$ $R = 1220.00'$ $L = 159.98'$ $T = 89.11'$ $E = 2.63'$
(388) PC STA. 307+80.74 N. 1,837,147.9166 E. 1,030,431.9235 (936) PI STA. 318+46.23 N. 1,837,181.0800 E. 1,031,496.8969 (391) PT STA. 327+70.94 N. 1,836,378.2118 E. 1,032,197.3759	(540) PC STA. 367+28.54 N. 1,833,396.0757 E. 1,034,799.2041 (937) PI STA. 372+45.57 N. 1,833,008.4802 E. 1,035,139.1130 (543) PT STA. 377+21.85 N. 1,832,489.7197 E. 1,035,155.8404	(561) PC STA. 377+28.40 N. 1,832,483.1744 E. 1,035,156.0523 (938) PI STA. 378+38.18 N. 1,832,373.4566 E. 1,035,159.6038 (565) PT STA. 379+44.52 N. 1,832,275.3176 E. 1,035,208.7906	(352) PC STA. 382+83.35 N. 1,831,912.5992 E. 1,035,360.6114 (939) PI STA. 384+17.33 N. 1,831,952.6259 E. 1,035,420.6410 (354) PT STA. 385+45.15 N. 1,831,718.8844 E. 1,035,412.7415	(355) PC STA. 386+41.07 N. 1,831,623.1313 E. 1,035,407.0858 (940) PI STA. 387+21.18 N. 1,831,543.1660 E. 1,035,402.3827 (357) PT STA. 388+01.05 N. 1,831,464.5048 E. 1,035,387.2242

GRID COORDINATES ARE SHOWN

COORDINATE TABLE ~ 95TH STREET		
NO.	STATION	-NORTH-COORDINATES-EAST-
101	273+43.85	N. 1,837,036.6311 E. 1,026,996.8361
100	300+00.00	N. 1,837,231.6190 E. 1,029,651.5602
351	381+92.32	N. 1,832,953.7800 E. 1,035,319.8236
936	392+91.05	N. 1,830,983.3345 E. 1,035,294.6219

COORDINATE TABLE ~ PLAINFIELD-NAPERVILLE RD.		
NO.	STATION	-NORTH-COORDINATES-EAST-
103	104+00.00	N. 1,834,474.0566 E. 1,025,727.3907
100	36+50.64	N. 1,837,123.8180 E. 1,025,651.5502
102	62+98.15	N. 1,839,770.2008 E. 1,029,581.7979

COORDINATE TABLE ~ BOUGHTON RD.		
NO.	STATION	-NORTH-COORDINATES-EAST-
156	28+04.44	N. 1,830,866.3384 E. 1,032,431.8354
351	60+34.85	N. 1,832,953.7800 E. 1,035,319.8236
931	72+73.01	N. 1,832,668.5583 E. 1,035,426.7397



- IRON PIPE OR ROD FOUND ○ SET 5/8" x 30" REBAR
- + CUT CROSS FOUND OR SET ● PK FOUND PK NAIL ○ PK SET PK NAIL
- T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION
T2 SET 5/8" NCH REBAR FLUSH WITH GROUND TO TIE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS PROFESSIONAL NUMBER.
- B11 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION.
B22 BURIED 5/8" INCH REBAR 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE.
B33 IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS PROFESSIONAL NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY.
SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS PROFESSIONAL NUMBER.
- M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS.
BURIED 5/8" INCH REBAR 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS PROFESSIONAL NUMBER.
- PERMANENT SURVEY MARKER, IDOT STD. 2195 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS)
 COUNTY OF WILL) SS

THIS IS TO CERTIFY THAT WE, CLAASSEN, WHITE & ASSOCIATES, P.C., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION NUMBER 184-004039, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTIONS 1, 2, 8 AND 12, TOWNSHIP 37 NORTH, RANGE 9 EAST OF THE THIRD PRINCIPAL MERIDIAN AND IN SECTION 6, TOWNSHIP 37 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, BOTH IN WILL COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. THIS SURVEY CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT JOLIET, ILLINOIS THIS 23rd DAY OF MARCH, 2012 A.D.

DAVID A. CLAASSEN, VICE PRESIDENT
 DAVID A. CLAASSEN
 ILLINOIS PROFESSIONAL LAND SURVEYOR NO. C35-002962
 LICENSE EXPIRES NOVEMBER 30, 2012
 THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

DAVID A. CLAASSEN
 STATE OF ILLINOIS
 PROFESSIONAL LAND SURVEYOR
 NO. C35-002962

0 500 1000 1500
 SCALE: 1" = 500'

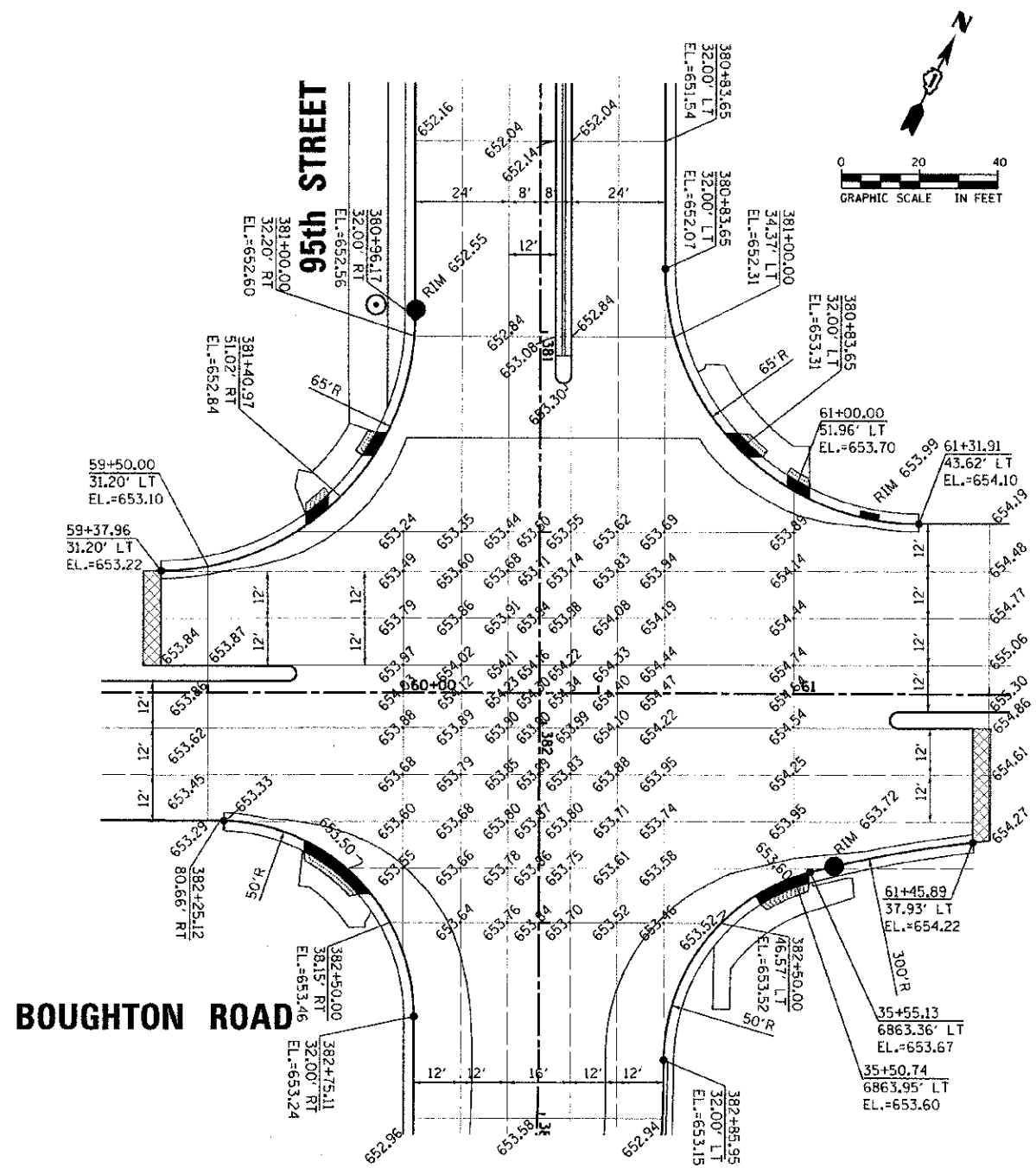
NOTE: BEARINGS, DISTANCES AND COORDINATES SHOWN HEREON ARE BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM EAST ZONE, NAD 83 (2011).

Claassen, White & Associates, P.C.
 121 AIRPORT DRIVE, UNIT 1
 JOLIET, ILLINOIS 60431
 (815) 744 3720

SHEET 1 IS A COVER SHEET AND NOT RECORDED.
PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
CH 89 (95TH STREET) EXTENSION
PLAINFIELD-NAPERVILLE RD. TO BOUGHTON RD.
 SECTION 01-00181-00-FP WILL COUNTY
 PROJECT JOB NO. R-55-001-97
 STATION TO STATION
 SCALE: 1" = 500' SHEET 23 OF 27

BUREAU OF LAND ACQUISITION
 201 WEST CENTER COURT
 SCHAUMBURG, ILLINOIS 60196-1096

PRINTING DATE: September 13, 2012 - JNH MicroStation - Sheets 23.dgn



BOUGHTON ROAD

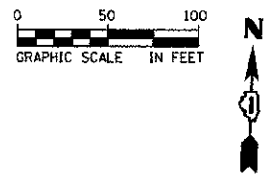
95th STREET

**95th STREET / BOUGHTON ROAD
INTERSECTION DETAILS**

NOTE: SEE DRAINAGE & UTILITIES DRAWINGS FOR
SPOT ELEVATIONS ALONG RADIUS RETURNS
OF RESIDENTIAL INTERSECTIONS.

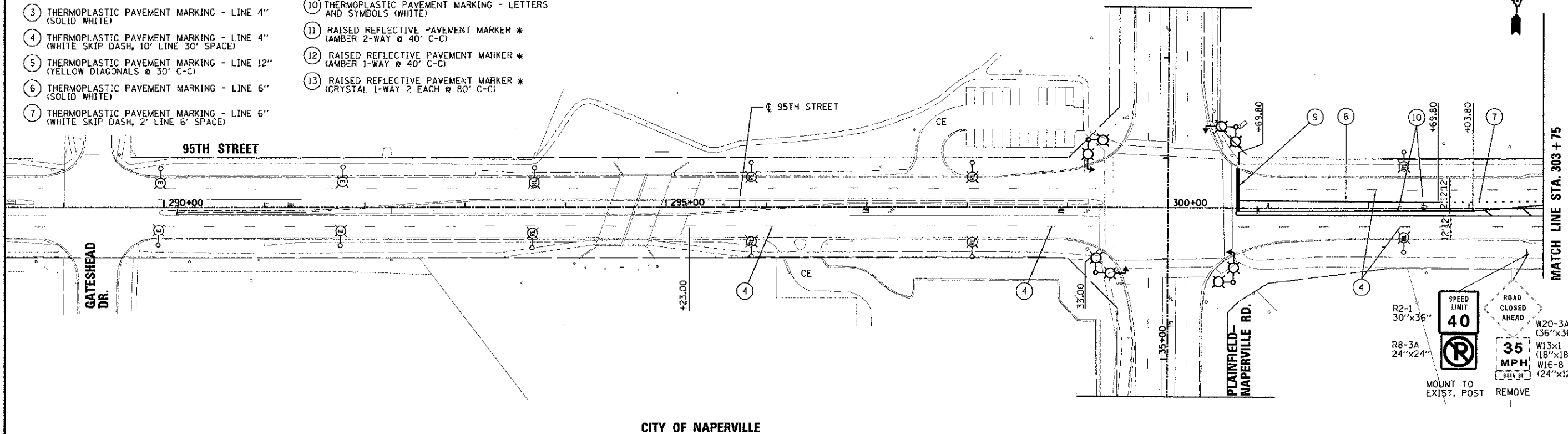
P:\11\110511\110511.dwg, 11/19/2012 10:51:11 AM, 11/19/2012 10:51:11 AM, 11/19/2012 10:51:11 AM, 11/19/2012 10:51:11 AM

	100 S. WACKER DR. SUITE 500 CHICAGO, IL 60606 TEL (312) 939-1000 FAX (312) 939-4998	DESIGNED - JRP DRAWN - MRF CHECKED - JRP DATE - 10/19/2012	REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	95th STREET / BOUGHTON ROAD INTERSECTION DETAILS	F.A.J. RTE. 1644 SECTION 01-00181-00-PP COUNTY WILL TOTAL SHEETS 328 SHEET NO. 112 CONTRACT NO. 63647
	PLOT SCALE = 28,0000 / 1" = 100' PLOT DATE = 10/18/2012	SCALE: DRAWING NO. OF STA. TO STA.	ILLINOIS FED. AID PROJECT			



LEGEND

- ① THERMOPLASTIC PAVEMENT MARKING - LINE 4" (SOLID YELLOW)
- ② THERMOPLASTIC PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW, 11" C-C)
- ③ THERMOPLASTIC PAVEMENT MARKING - LINE 4" (SOLID WHITE)
- ④ THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE SKIP DASH, 10' LINE 30' SPACE)
- ⑤ THERMOPLASTIC PAVEMENT MARKING - LINE 12" (YELLOW DIAGONALS @ 30' C-C)
- ⑥ THERMOPLASTIC PAVEMENT MARKING - LINE 6" (SOLID WHITE)
- ⑦ THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE SKIP DASH, 2' LINE 6' SPACE)
- ⑧ THERMOPLASTIC PAVEMENT MARKING - LINE 12" (WHITE DIAGONALS @ 3' C-C)
- ⑨ THERMOPLASTIC PAVEMENT MARKING - LINE 24" (SOLID WHITE STOP BAR)
- ⑩ THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS (WHITE)
- ⑪ RAISED REFLECTIVE PAVEMENT MARKER * (AMBER 2-WAY @ 40' C-C)
- ⑫ RAISED REFLECTIVE PAVEMENT MARKER * (AMBER 1-WAY @ 40' C-C)
- ⑬ RAISED REFLECTIVE PAVEMENT MARKER * (CRYSTAL 1-WAY 2 EACH @ 80' C-C)
- ⑭ RAISED REFLECTIVE PAVEMENT MARKER * (CRYSTAL 1-WAY @ 40' C-C)
- ⑮ POLYUREA PAVEMENT MARKING - LINE 4" (WHITE SKIP DASH, 10' LINE 30' SPACE)

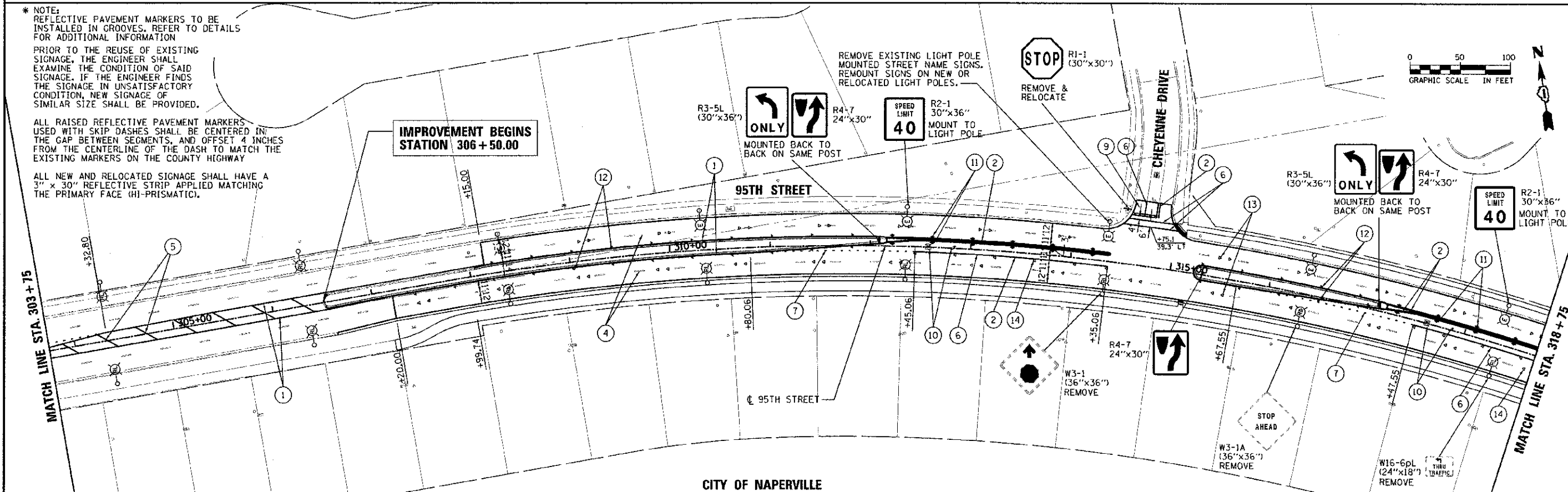


CITY OF NAPERVILLE

* NOTE:
REFLECTIVE PAVEMENT MARKERS TO BE INSTALLED IN GROOVES. REFER TO DETAILS FOR ADDITIONAL INFORMATION.
PRIOR TO THE REUSE OF EXISTING SIGNAGE, THE ENGINEER SHALL EXAMINE THE CONDITION OF SAID SIGNAGE. IF THE ENGINEER FINDS THE SIGNAGE IN UNSATISFACTORY CONDITION, NEW SIGNAGE OF SIMILAR SIZE SHALL BE PROVIDED.

ALL RAISED REFLECTIVE PAVEMENT MARKERS USED WITH SKIP DASHES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS, AND OFFSET 4 INCHES FROM THE CENTERLINE OF THE DASH TO MATCH THE EXISTING MARKERS ON THE COUNTY HIGHWAY

ALL NEW AND RELOCATED SIGNAGE SHALL HAVE A 3" x 30" REFLECTIVE STRIP APPLIED MATCHING THE PRIMARY FACE (HI-PRISMATIC).



CITY OF NAPERVILLE

URS
100 S. WACKER DR.
SUITE 500
CHICAGO, IL 60606
TEL (312) 939-1000
FAX (312) 939-4190

DESIGNED - JRP
DRAWN - MRF
CHECKED - JRP
DATE - 10/19/2012

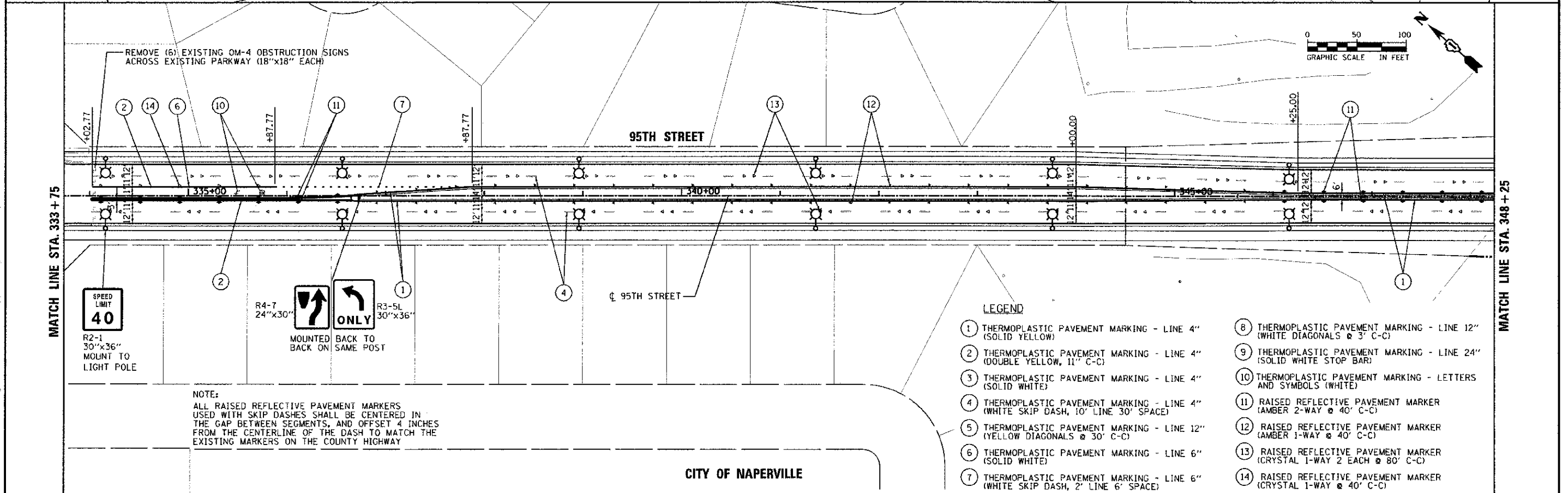
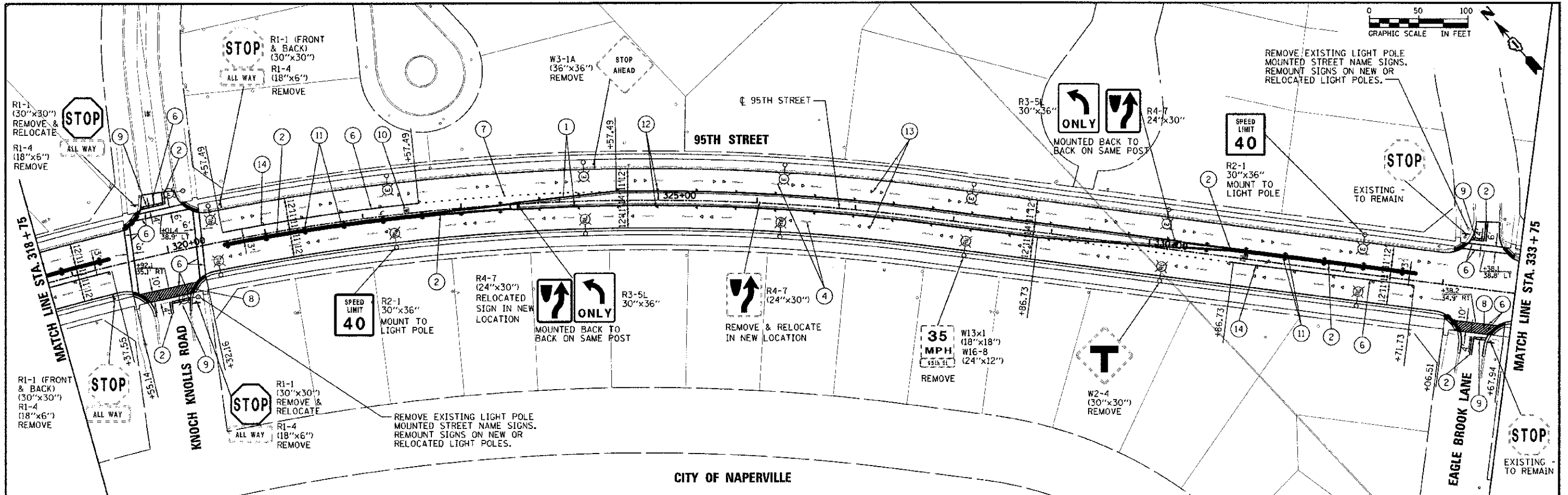
REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

95TH STREET
PAVEMENT MARKING & SIGNING PLAN
SCALE: 1"=50' DRAWING NO. 1 OF 4 STA. 291+70.56 TO STA. 318+75

F.A.J. RTE. 1644	SECTION 01-00181-00-FP	COUNTY WILL	TOTAL SHEETS 328	SHEET NO. 113
CONTRACT NO. 63647				
ILLINOIS FED. AID PROJECT				

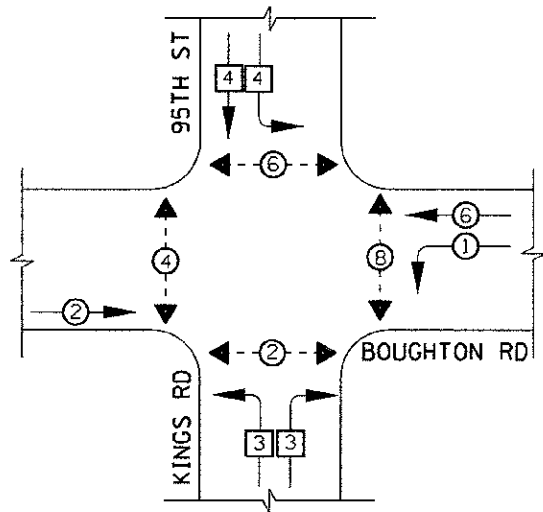
FILE NAME: c:\naper\11_26387381_0514\1.dwg; contractor: acattlv; address: c:\naper\11_26387381_0514\1.dwg; user: jrp



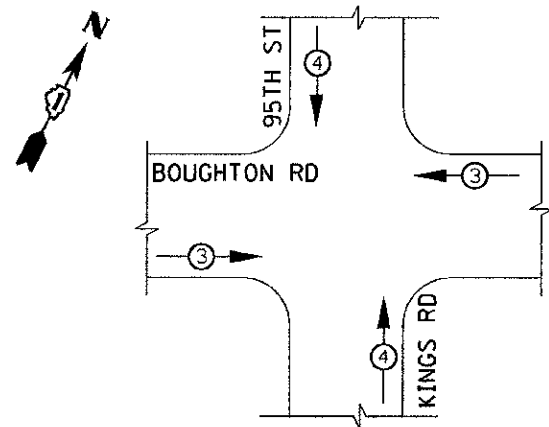
- LEGEND**
- ① THERMOPLASTIC PAVEMENT MARKING - LINE 4" (SOLID YELLOW)
 - ② THERMOPLASTIC PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW, 11" C-C)
 - ③ THERMOPLASTIC PAVEMENT MARKING - LINE 4" (SOLID WHITE)
 - ④ THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE SKIP DASH, 10' LINE 30' SPACE)
 - ⑤ THERMOPLASTIC PAVEMENT MARKING - LINE 12" (YELLOW DIAGONALS @ 30' C-C)
 - ⑥ THERMOPLASTIC PAVEMENT MARKING - LINE 6" (SOLID WHITE)
 - ⑦ THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE SKIP DASH, 2' LINE 6' SPACE)
 - ⑧ THERMOPLASTIC PAVEMENT MARKING - LINE 12" (WHITE DIAGONALS @ 3' C-C)
 - ⑨ THERMOPLASTIC PAVEMENT MARKING - LINE 24" (SOLID WHITE STOP BAR)
 - ⑩ THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS (WHITE)
 - ⑪ RAISED REFLECTIVE PAVEMENT MARKER (AMBER 2-WAY @ 40' C-C)
 - ⑫ RAISED REFLECTIVE PAVEMENT MARKER (AMBER 1-WAY @ 40' C-C)
 - ⑬ RAISED REFLECTIVE PAVEMENT MARKER (CRYSTAL 1-WAY 2 EACH @ 80' C-C)
 - ⑭ RAISED REFLECTIVE PAVEMENT MARKER (CRYSTAL 1-WAY @ 40' C-C)

URS 100 S. WACKER DR. SUITE 500 CHICAGO IL 60606 TEL (312) 939-1000 FAX (312) 339-4998	USER NAME - jones_pash PLT. SCALE - 1"=50' PLT. DATE - 10/15/2012	DESIGNED - JRP DRAWN - MRF CHECKED - JRP DATE - 10/19/2012	REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	95TH STREET PAVEMENT MARKING & SIGNING PLAN	F.A.U. RTE. 1644 SECTION 01-00181-00-FP COUNTY WILL TOTAL SHEETS 328 SHEET NO. 114 CONTRACT NO. 63647
	SCALE: 1"=50' DRAWING NO. 2 OF 4 STA. 318+75 TO STA. 348+25				(ILLINOIS) FED. AID PROJECT	

TEMPORARY CONTROLLER SEQUENCE



TEMPORARY PHASE DESIGNATION DIAGRAM



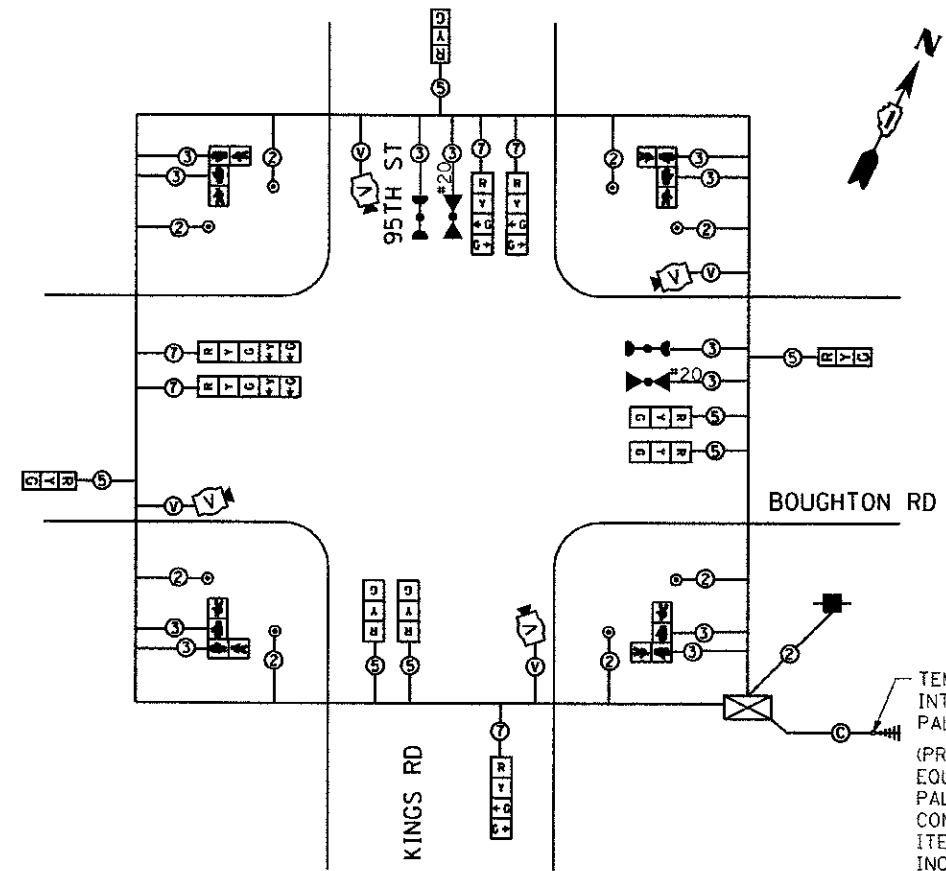
TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE

TEMPORARY EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	← →	↓ ↑

LEGEND

- ← ⊕ → DUAL ENTRY PHASE
- ← ⊕ → SINGLE ENTRY PHASE
- ← ⊕ → PEDESTRIAN PHASE
- * NUMBER REFERS TO ASSOCIATED PHASE

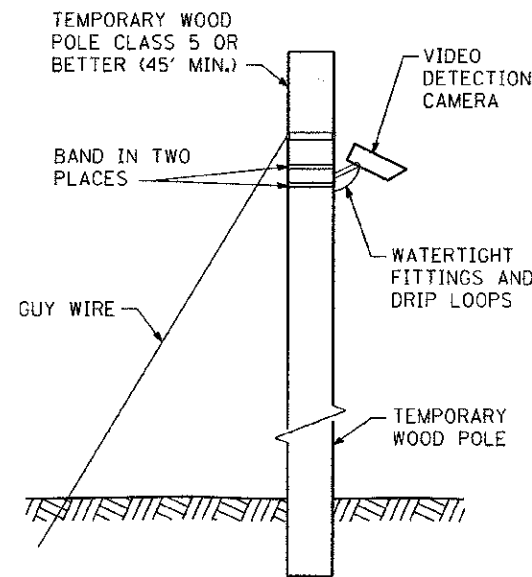
THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE ECONOLITE TO MATCH THE EXISTING SYSTEM.



TEMPORARY CABLE PLAN

TEMPORARY RADIO INTERCONNECT TO PALMER DRIVE
(PROVIDE SIMILAR EQUIPMENT IN PALMER DRIVE CONTROLLER. ALL ITEMS SHALL BE INCLUDED IN PAY ITEM "TEMPORARY TRAFFIC SIGNAL INSTALLATION")

TEMPORARY RADIO INTERCONNECT SHALL NOT BE REMOVED UNTIL FIBER INTERCONNECT TO PALMER DRIVE IS REINSTALLED AND OPERATIONAL.



TEMPORARY VIDEO DETECTION MOUNTING DETAIL (NOT TO SCALE)

RESTORATION OF WORK AREA RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE INCAND.	LED	% OPERATION	
SIGNAL (RED)	12	-	17	0.50	102.00
(YELLOW)	12	-	25	0.25	75.00
(GREEN)	9	-	15	0.25	33.75
ARROW	10	-	12	0.10	12.00
PED. SIGNAL	8	-	25	1.00	200.00
CONTROLLER	1	-	100	1.00	100.00
ILLUM. SIGN	-	-	25	0.50	-
VIDEO SYSTEM	1	150	-	1.00	150.00
ENERGY COSTS TO:					TOTAL = 672.75

VILLAGE OF BOLINGBROOK
375 BRIARCLIFF ROAD
BOLINGBROOK, ILLINOIS 60440-0951
ENERGY SUPPLY CONTACT: DAVE BOLLINGER
PHONE: (815) 724-5717
COMPANY: COMED

URS 100 S. WACKER DR., SUITE 500, CHICAGO, IL 60606
TEL (312) 939-1000 FAX (312) 939-4998

DESIGNED - JRP
DRAWN - MRF
CHECKED - JRP
DATE - 10/19/2012

REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

TEMPORARY TRAFFIC SIGNAL INSTALLATION AND TEMPORARY CABLE PLAN
95TH KINGS AND BOUGHTON RD
SCALE: 1" = 20' DRAWING NO. 2 OF 6 STA. TO STA.

F.A.U. RTE. 1644	SECTION 01-00181-00-FF	COUNTY WILL	TOTAL SHEETS 328	SHEET NO. 118
CONTRACT NO. 63647				
ILLINOIS FED. AID PROJECT				

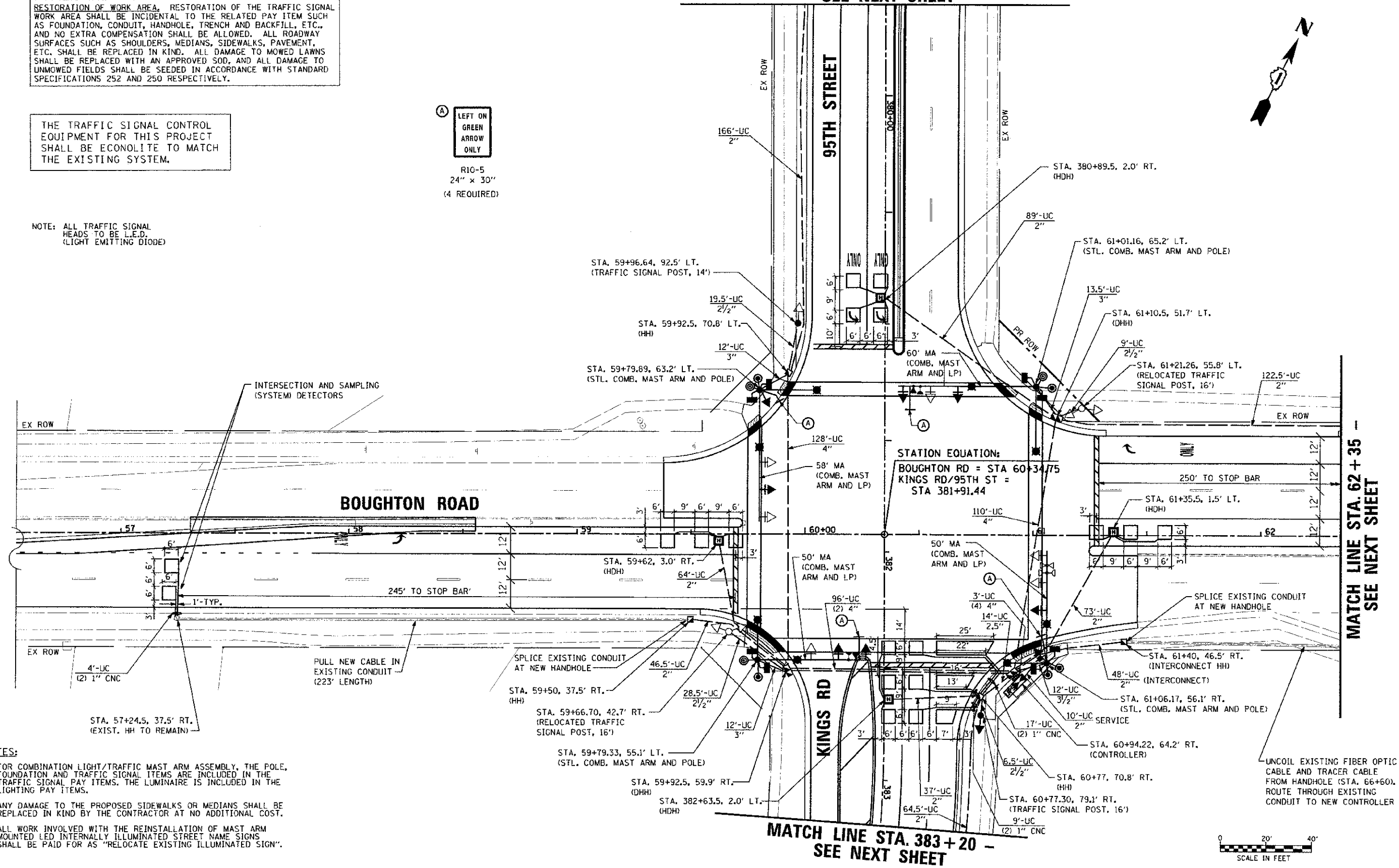
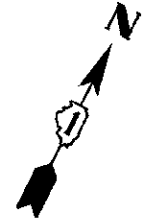
RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE ECONOLITE TO MATCH THE EXISTING SYSTEM.

NOTE: ALL TRAFFIC SIGNAL HEADS TO BE L.E.D. (LIGHT EMITTING DIODE)

A LEFT ON GREEN ARROW ONLY
R10-5
24" x 30"
(4 REQUIRED)

MATCH LINE STA. 379 + 60 -
SEE NEXT SHEET



MATCH LINE STA. 62 + 35 -
SEE NEXT SHEET

- NOTES:**
- FOR COMBINATION LIGHT/TRAFFIC MAST ARM ASSEMBLY, THE POLE, FOUNDATION AND TRAFFIC SIGNAL ITEMS ARE INCLUDED IN THE TRAFFIC SIGNAL PAY ITEMS. THE LUMINAIRE IS INCLUDED IN THE LIGHTING PAY ITEMS.
 - ANY DAMAGE TO THE PROPOSED SIDEWALKS OR MEDIANS SHALL BE REPLACED IN KIND BY THE CONTRACTOR AT NO ADDITIONAL COST.
 - ALL WORK INVOLVED WITH THE REINSTALLATION OF MAST ARM MOUNTED LED INTERNALLY ILLUMINATED STREET NAME SIGNS SHALL BE PAID FOR AS "RELOCATE EXISTING ILLUMINATED SIGN".

MATCH LINE STA. 383 + 20 -
SEE NEXT SHEET



URS
100 S. WACKER DR.
SUITE 500
CHICAGO, IL 60606
TEL (312) 939-1000
FAX (312) 939-4988

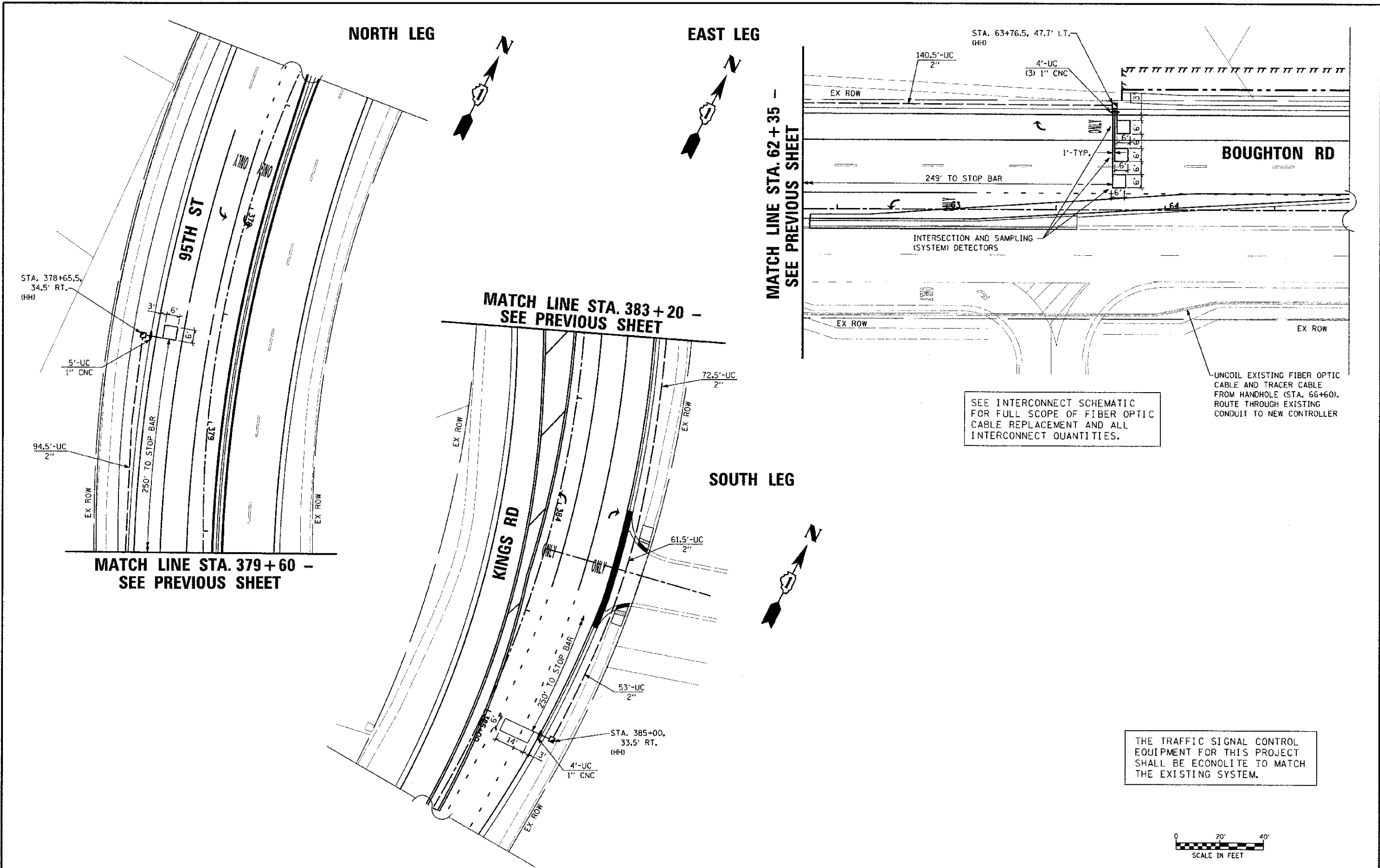
USER NAME: j. jones
DESIGNED - JRP
DRAWN - MRF
CHECKED - JRP
DATE - 10/19/2012

REVISIONS:
REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL PLAN
95TH AND BOUGHTON RD
SCALE: 1" = 20'
DRAWING NO. 3 OF 6
STA. TO STA.

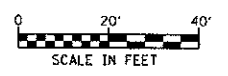
F.A.U. RTE. 1644	SECTION 01-00181-00-FP	COUNTY WILL	TOTAL SHEETS 328	SHEET NO. 119
CONTRACT NO. 63647				ILLINOIS FED. AID PROJECT



SEE INTERCONNECT SCHEMATIC FOR FULL SCOPE OF FIBER OPTIC CABLE REPLACEMENT AND ALL INTERCONNECT QUANTITIES.

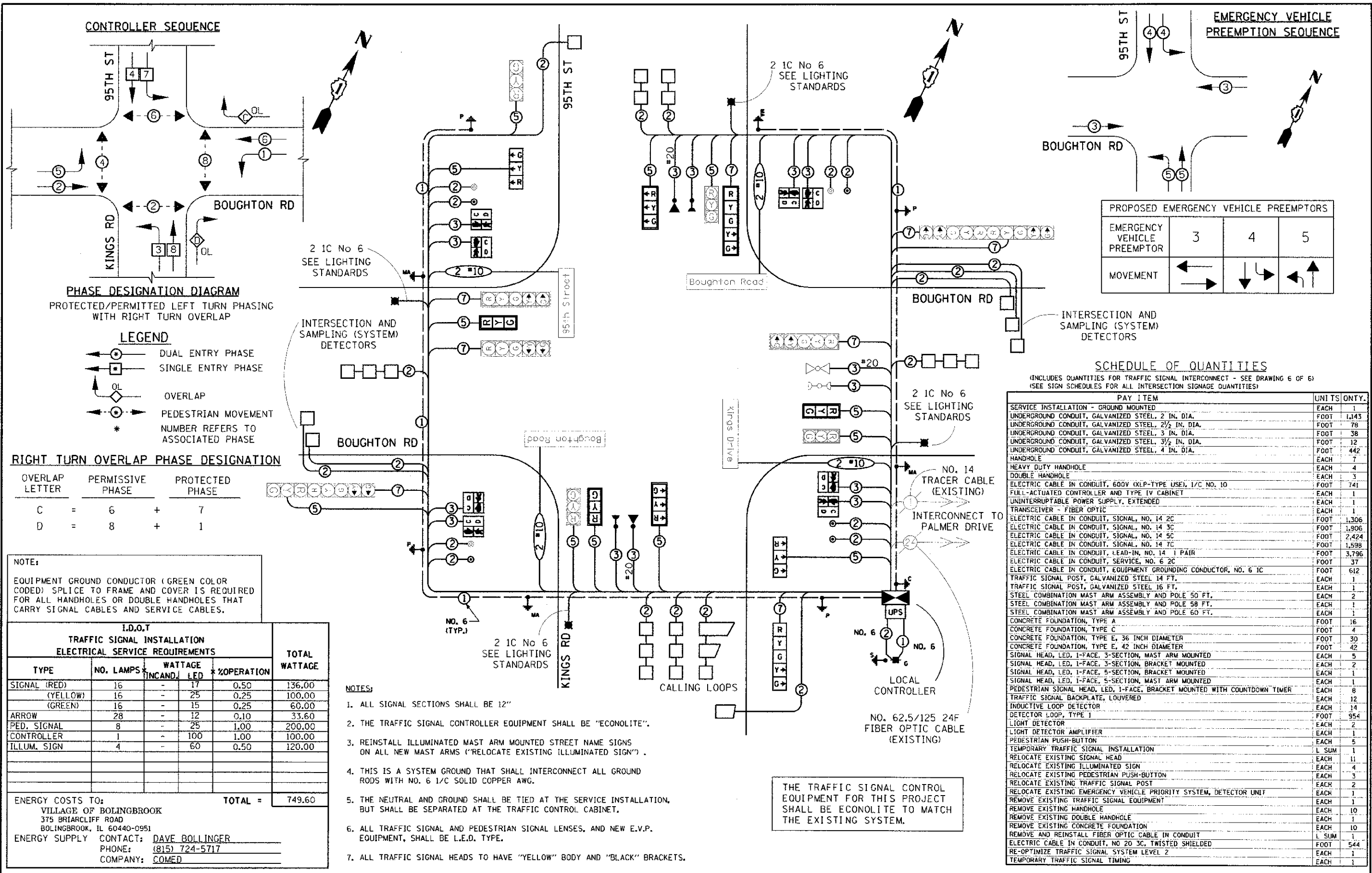
UNCOIL EXISTING FIBER OPTIC CABLE AND TRACER CABLE FROM HANDHOLE (STA. 66+60). ROUTE THROUGH EXISTING CONDUIT TO NEW CONTROLLER

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE ECONOLITE TO MATCH THE EXISTING SYSTEM.

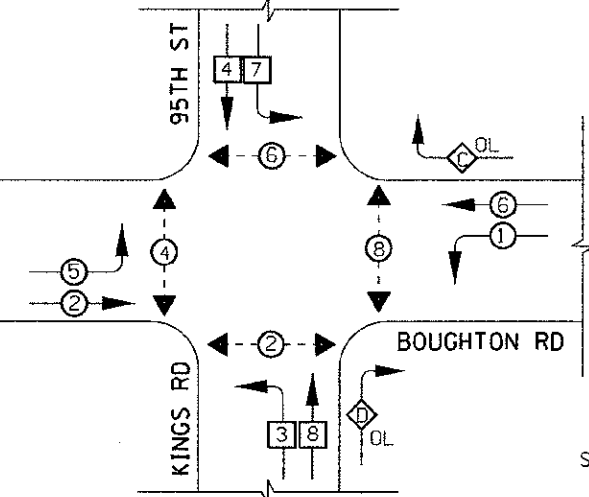


	100 S. WACKER DR. SUITE 500 CHICAGO, IL 60606 TEL: (312) 939-4000 FAX: (312) 939-4198	DESIGNED - JRP DRAWN - MRF CHECKED - JRP DATE - 10/19/2012	REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC SIGNAL PLAN 95TH AND BOUGHTON RD	F.A.J. RTE. 1644 SECTION 01-00181-00-FP COUNTY WILL TOTAL SHEETS 328 SHEET NO. 120 CONTRACT NO. 63647
	USER NAME: jrp PLOT SCALE: 1" = 20' PLOT DATE: 12/16/2012	SCALE: 1" = 20' DRAWING NO. 4 OF 6 STA. TO STA.	ILLINOIS FED. AID PROJECT			

FILE NAME: s:\projects\10111111\10111111.dwg



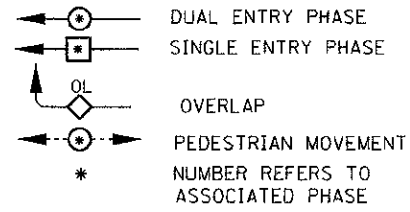
CONTROLLER SEQUENCE



PHASE DESIGNATION DIAGRAM

PROTECTED/PERMITTED LEFT TURN PHASING WITH RIGHT TURN OVERLAP

LEGEND



RIGHT TURN OVERLAP PHASE DESIGNATION

OVERLAP LETTER	PERMISSIVE PHASE	PROTECTED PHASE
C	= 6	+ 7
D	= 8	+ 1

NOTE:
EQUIPMENT GROUND CONDUCTOR (GREEN COLOR CODED) SPLICE TO FRAME AND COVER IS REQUIRED FOR ALL HANDHOLES OR DOUBLE HANDHOLES THAT CARRY SIGNAL CABLES AND SERVICE CABLES.

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE (INCAND.)	LED	% OPERATION	
SIGNAL (RED)	16	-	17	0.50	136.00
(YELLOW)	16	-	25	0.25	100.00
(GREEN)	16	-	15	0.25	60.00
ARROW	28	-	12	0.10	33.60
PED. SIGNAL	8	-	25	1.00	200.00
CONTROLLER	1	-	100	1.00	100.00
ILLUM. SIGN	4	-	60	0.50	120.00
TOTAL =					749.60

ENERGY COSTS TO:
VILLAGE OF BOLINGBROOK
375 BRIARCLIFF ROAD
BOLINGBROOK, IL 60440-0951
ENERGY SUPPLY CONTACT: **DAVE BOLLINGER**
PHONE: (815) 724-5717
COMPANY: **COMED**

2 IC No 6
SEE LIGHTING STANDARDS

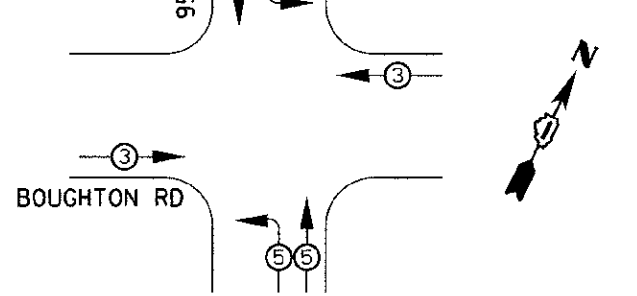
INTERSECTION AND SAMPLING (SYSTEM) DETECTORS

NOTES:

1. ALL SIGNAL SECTIONS SHALL BE 12"
2. THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT SHALL BE "ECONOLITE".
3. REINSTALL ILLUMINATED MAST ARM MOUNTED STREET NAME SIGNS ON ALL NEW MAST ARMS ("RELOCATE EXISTING ILLUMINATED SIGN").
4. THIS IS A SYSTEM GROUND THAT SHALL INTERCONNECT ALL GROUND RODS WITH NO. 6 1/C SOLID COPPER AWG.
5. THE NEUTRAL AND GROUND SHALL BE TIED AT THE SERVICE INSTALLATION, BUT SHALL BE SEPARATED AT THE TRAFFIC CONTROL CABINET.
6. ALL TRAFFIC SIGNAL AND PEDESTRIAN SIGNAL LENSES, AND NEW E.V.P. EQUIPMENT, SHALL BE L.E.D. TYPE.
7. ALL TRAFFIC SIGNAL HEADS TO HAVE "YELLOW" BODY AND "BLACK" BRACKETS.

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE ECONOLITE TO MATCH THE EXISTING SYSTEM.

EMERGENCY VEHICLE PREEMPTION SEQUENCE

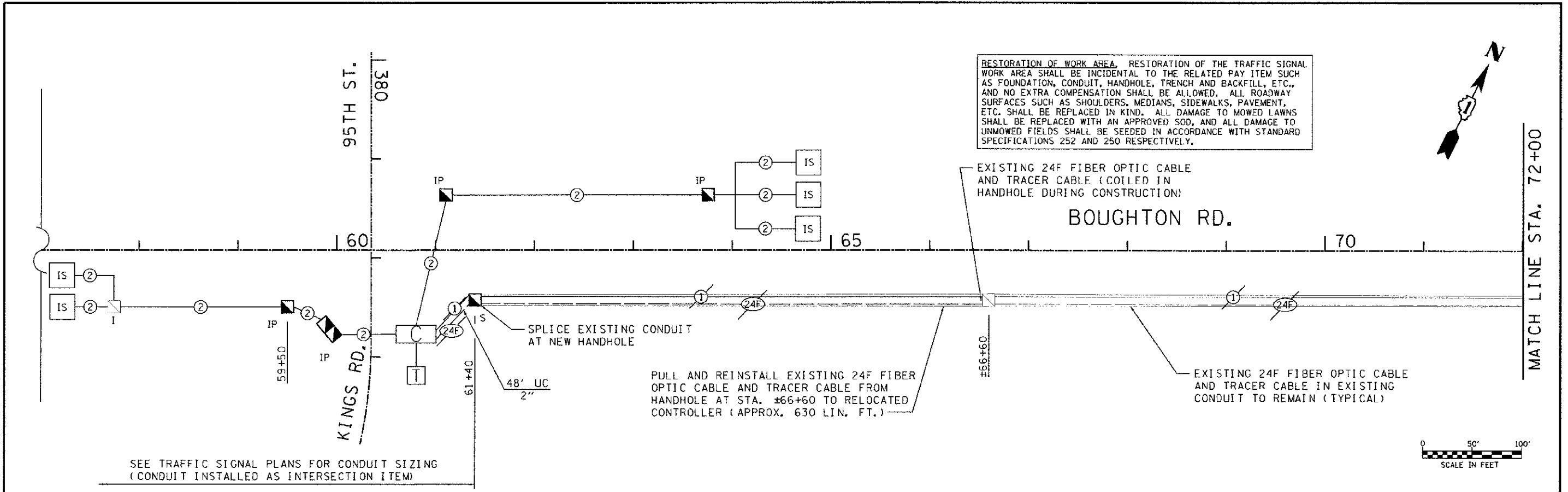


PROPOSED EMERGENCY VEHICLE PREEMPTORS			
EMERGENCY VEHICLE PREEMPTOR	3	4	5
MOVEMENT	←	↓	↶

SCHEDULE OF QUANTITIES

(INCLUDES QUANTITIES FOR TRAFFIC SIGNAL INTERCONNECT - SEE DRAWING 6 OF 6)
(SEE SIGN SCHEDULES FOR ALL INTERSECTION SIGNAGE QUANTITIES)

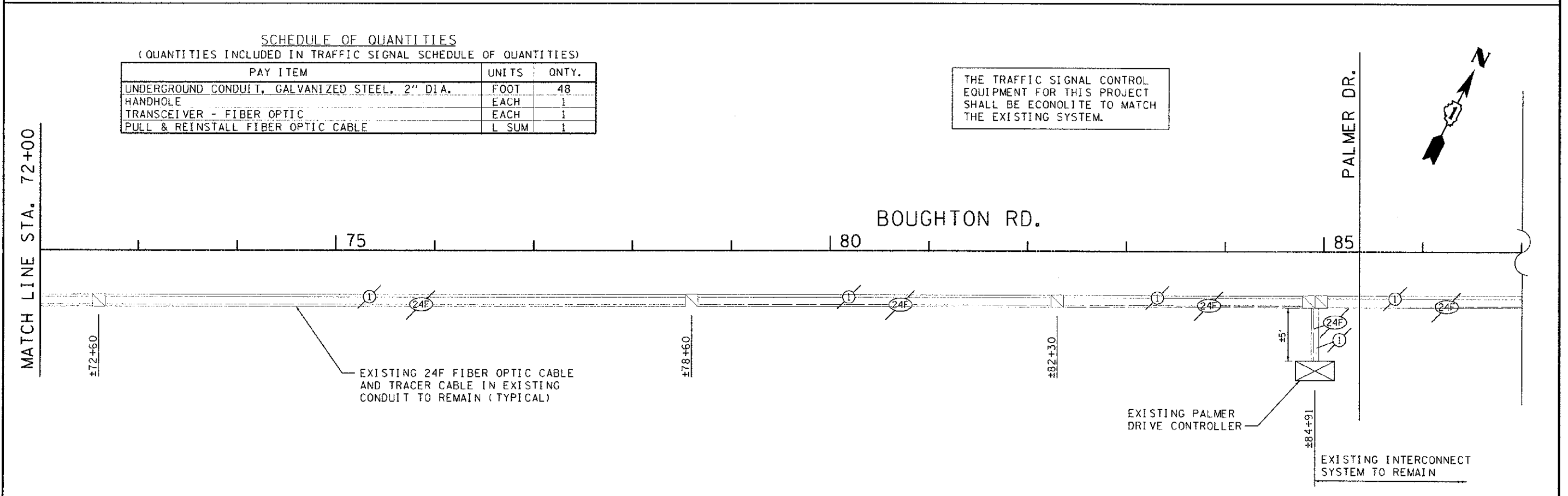
PAY ITEM	UNITS	QTY.
SERVICE INSTALLATION - GROUND MOUNTED	EACH	1
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 IN. DIA.	FOOT	1,143
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2 IN. DIA.	FOOT	78
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3 IN. DIA.	FOOT	38
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3 1/2 IN. DIA.	FOOT	12
UNDERGROUND CONDUIT, GALVANIZED STEEL, 4 IN. DIA.	FOOT	442
HANDHOLE	EACH	7
HEAVY DUTY HANDHOLE	EACH	4
DOUBLE HANDHOLE	EACH	3
ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE), 1/2" NO. 10	FOOT	741
FULL-ACTUATED CONTROLLER AND TYPE IV CABINET	EACH	1
UNINTERRUPTIBLE POWER SUPPLY, EXTENDED	EACH	1
TRANSCEIVER - FIBER OPTIC	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 14 2C	FOOT	1,306
ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 14 3C	FOOT	1,906
ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 14 5C	FOOT	2,424
ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 14 7C	FOOT	1,598
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	3,796
ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C	FOOT	37
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	612
TRAFFIC SIGNAL POST, GALVANIZED STEEL 14 FT.	EACH	1
TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	1
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 50 FT.	EACH	2
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 58 FT.	EACH	1
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 60 FT.	EACH	1
CONCRETE FOUNDATION, TYPE A	FOOT	16
CONCRETE FOUNDATION, TYPE C	FOOT	4
CONCRETE FOUNDATION, TYPE E, 36 INCH DIAMETER	FOOT	30
CONCRETE FOUNDATION, TYPE E, 42 INCH DIAMETER	FOOT	42
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED	EACH	5
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	2
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	1
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED	EACH	1
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	8
TRAFFIC SIGNAL BACKPLATE, LOUVERED	EACH	12
INDUCTIVE LOOP DETECTOR	EACH	14
DETECTOR LOOP, TYPE 1	FOOT	954
LIGHT DETECTOR	EACH	2
LIGHT DETECTOR AMPLIFIER	EACH	1
PEDESTRIAN PUSH-BUTTON	EACH	5
TEMPORARY TRAFFIC SIGNAL INSTALLATION	L SUM	1
RELOCATE EXISTING SIGNAL HEAD	EACH	11
RELOCATE EXISTING ILLUMINATED SIGN	EACH	4
RELOCATE EXISTING PEDESTRIAN PUSH-BUTTON	EACH	3
RELOCATE EXISTING TRAFFIC SIGNAL POST	EACH	2
RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, DETECTOR UNIT	EACH	1
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REMOVE EXISTING HANDHOLE	EACH	10
REMOVE EXISTING DOUBLE HANDHOLE	EACH	1
REMOVE EXISTING CONCRETE FOUNDATION	EACH	10
REMOVE AND REINSTALL FIBER OPTIC CABLE IN CONDUIT	L SUM	1
ELECTRIC CABLE IN CONDUIT, NO 20 3C, TWISTED SHIELDED	FOOT	544
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 2	EACH	1
TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1



SCHEDULE OF QUANTITIES
(QUANTITIES INCLUDED IN TRAFFIC SIGNAL SCHEDULE OF QUANTITIES)

PAY ITEM	UNITS	QNTY.
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	48
HANDHOLE	EACH	1
TRANSCIEVER - FIBER OPTIC	EACH	1
PULL & REINSTALL FIBER OPTIC CABLE	L SUM	1

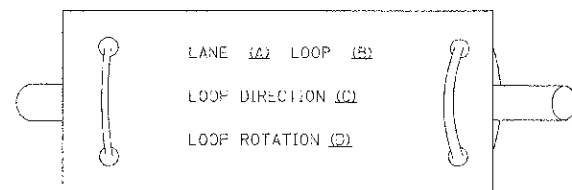
THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE ECONOLITE TO MATCH THE EXISTING SYSTEM.



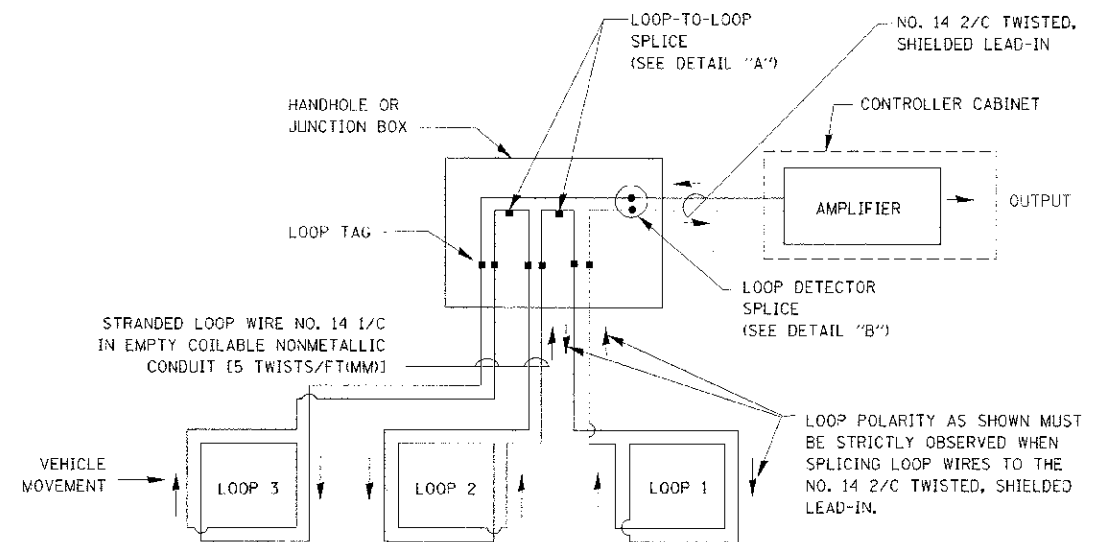
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

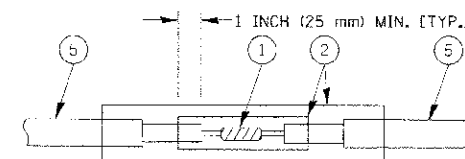


- A. LANE #1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

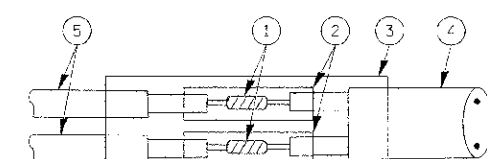


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm), IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

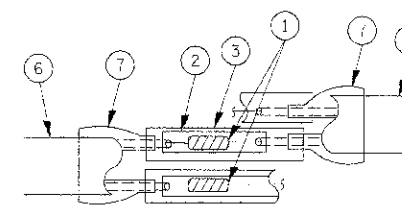


**DETAIL "A"
LOOP-TO-LOOP SPLICE**

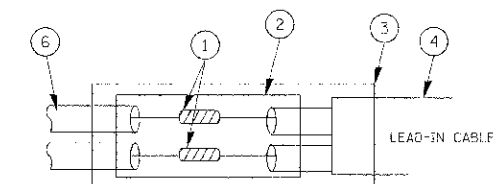


**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

TYPE I LOOP



**DETAIL "A"
LOOP-TO-LOOP SPLICE**



**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

F.I.C. NAME =	1.00% NAME = (bracketed)	DESIGNED - DAD	REVISED -
PROJECT WORK NUMBER: 1001300590L\10180315\10180315	DATE =	DRAWN - BCK	REVISED -
	PROJECT SCALE = 50,000 / 1 IN.	CHECKED - DAD	REVISED -
	PROJECT DATE = 11/27/2009	DATE - 10-28-09	REVISED -

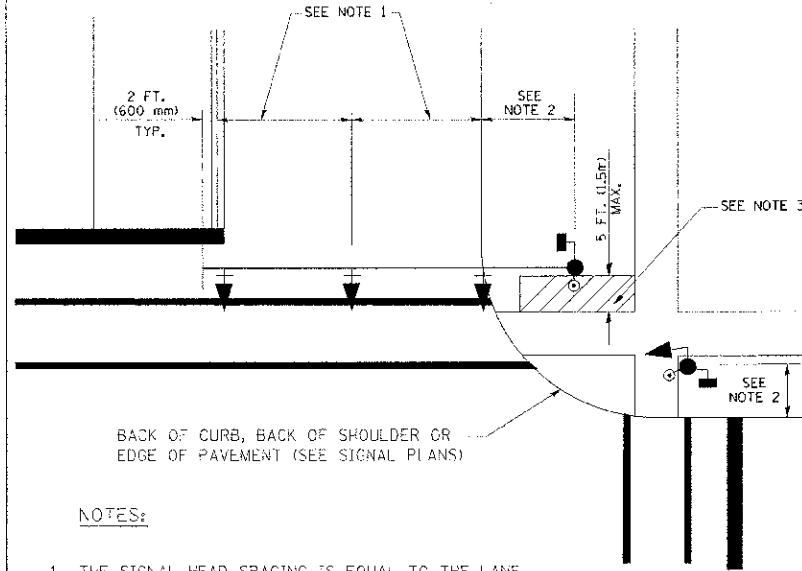
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE	
STANDARD TRAFFIC SIGNAL DESIGN DETAILS	
SCALE: NONE	SHEET NO. 1 OF 6 SHEETS STA. TO STA.

F.A.U. RTE. 1644	SECTION 01-00181-00-FP	COUNTY WILL	TOTAL SHEETS 328	SHEET NO. 123
TS-05		CONTRACT NO. 63647		
FED. ROAD DIST. NO. 1 ILLINOIS' FED. AID PROJECT				

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

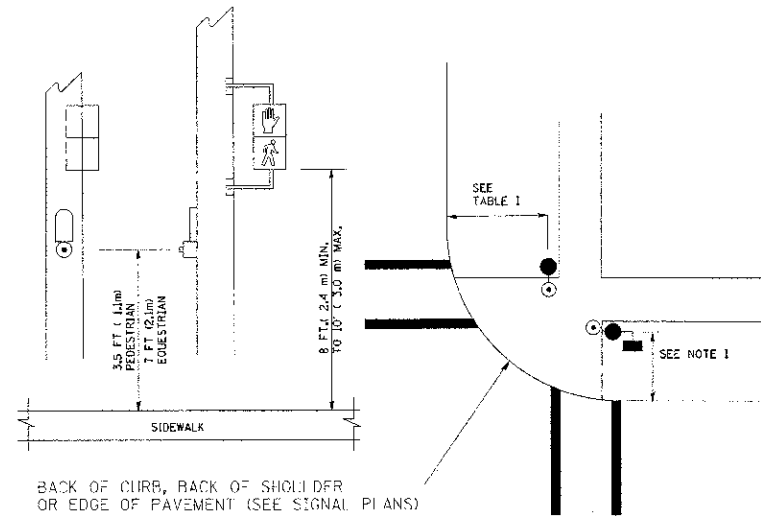
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

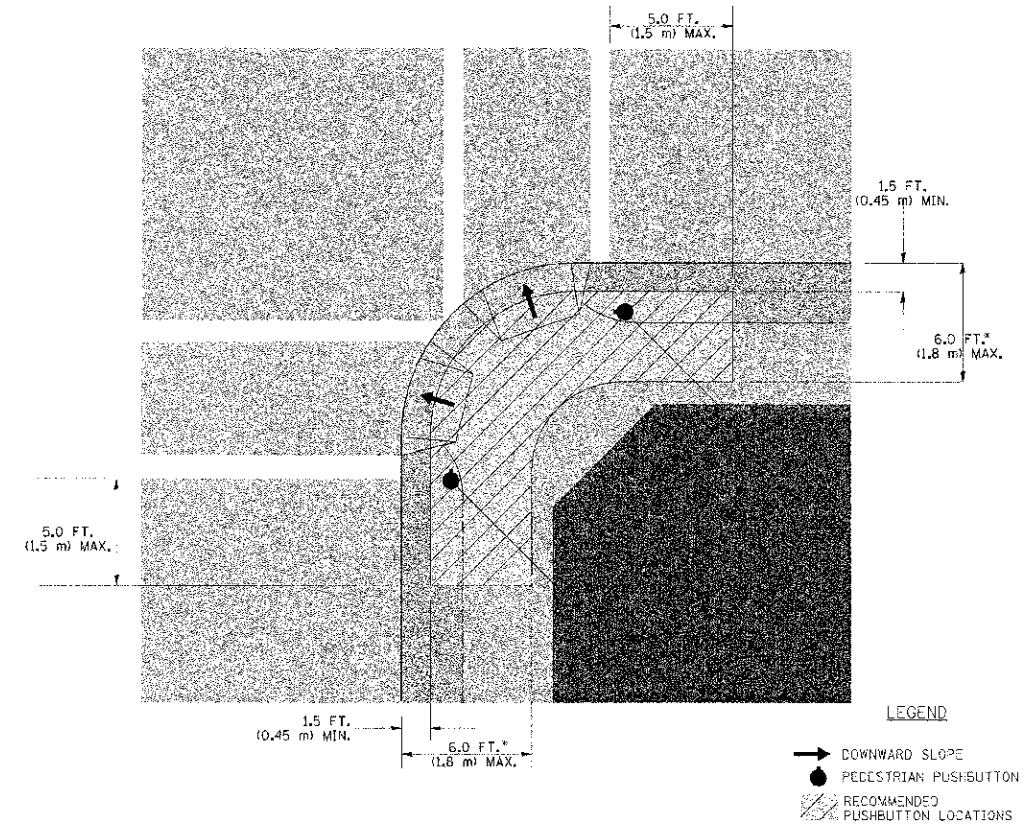
PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



- * WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- ** WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPARATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

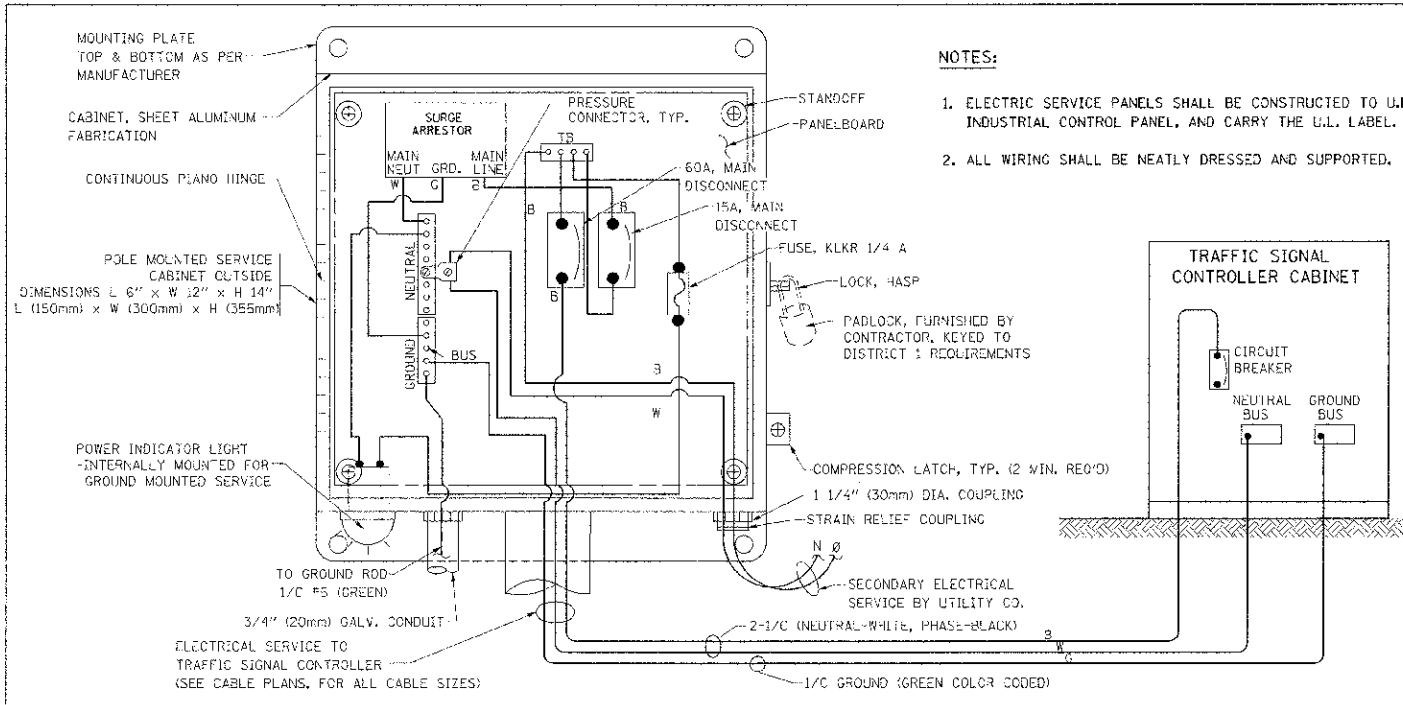
1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877005, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 680001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

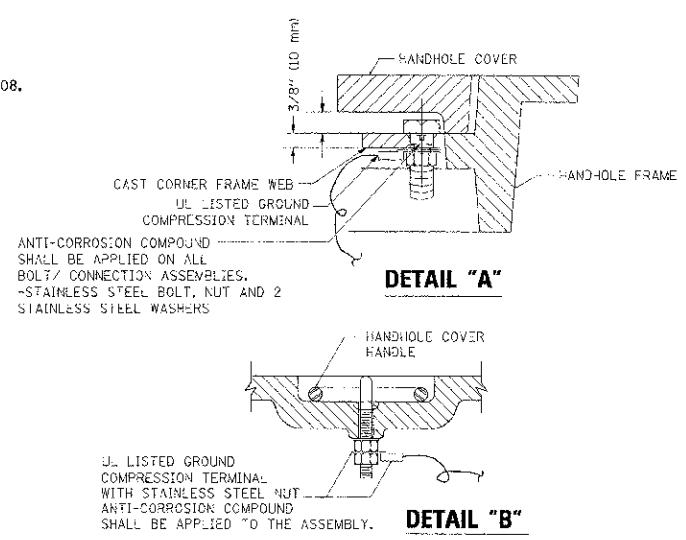
TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICER INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD AFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER "THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

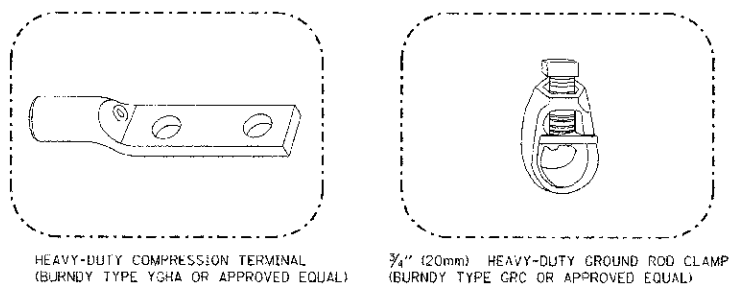


ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
 (NOT TO SCALE)

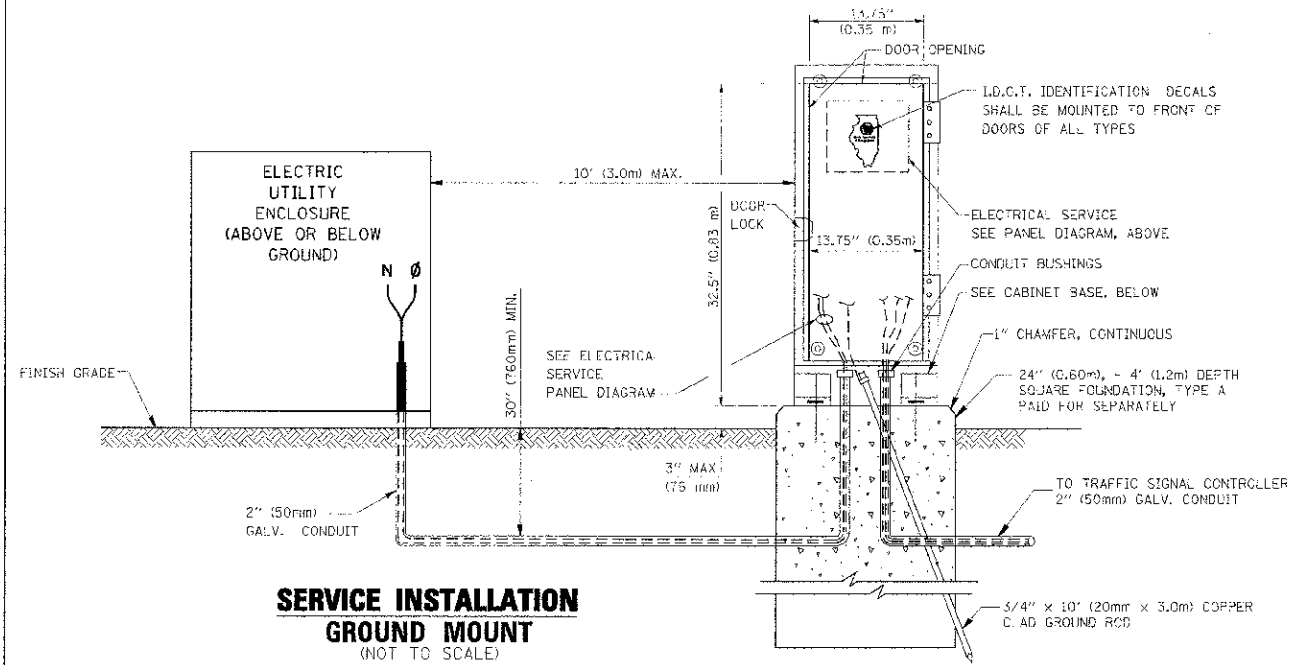
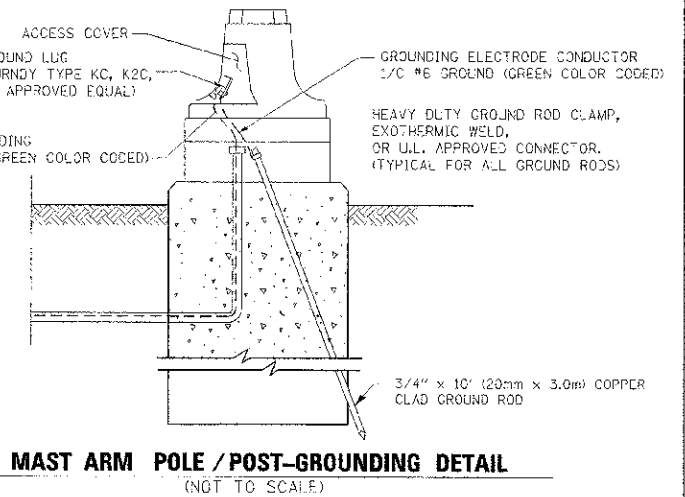
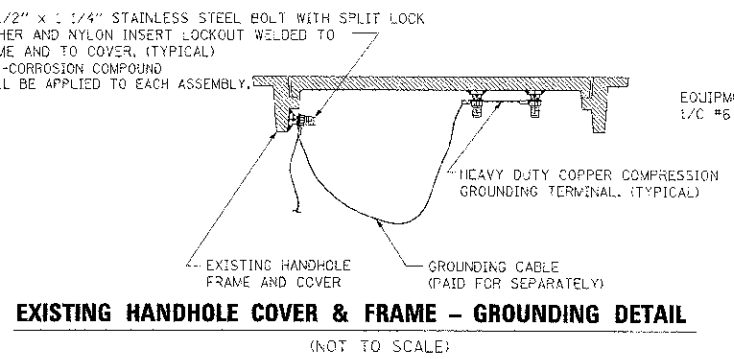
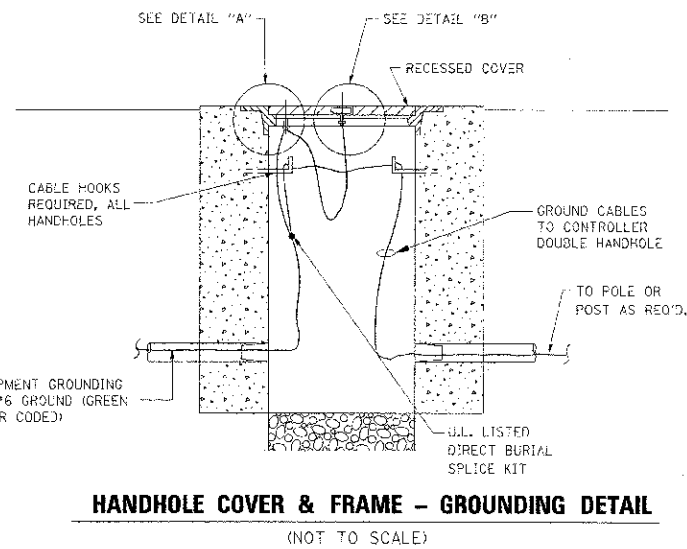


NOTES:
GROUNDING SYSTEM

- THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
- THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
- ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
- THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.

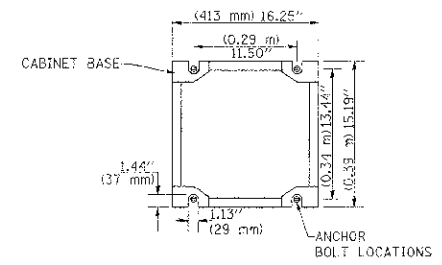


- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, U.L. APPROVED.
 - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES. 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES. 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



SERVICE INSTALLATION GROUND MOUNT
 (NOT TO SCALE)

CABINET - BASE BOLT PATTERN
 (NOT TO SCALE)

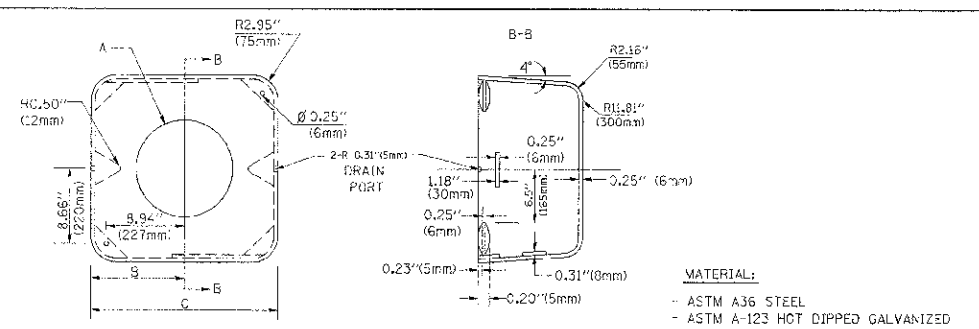
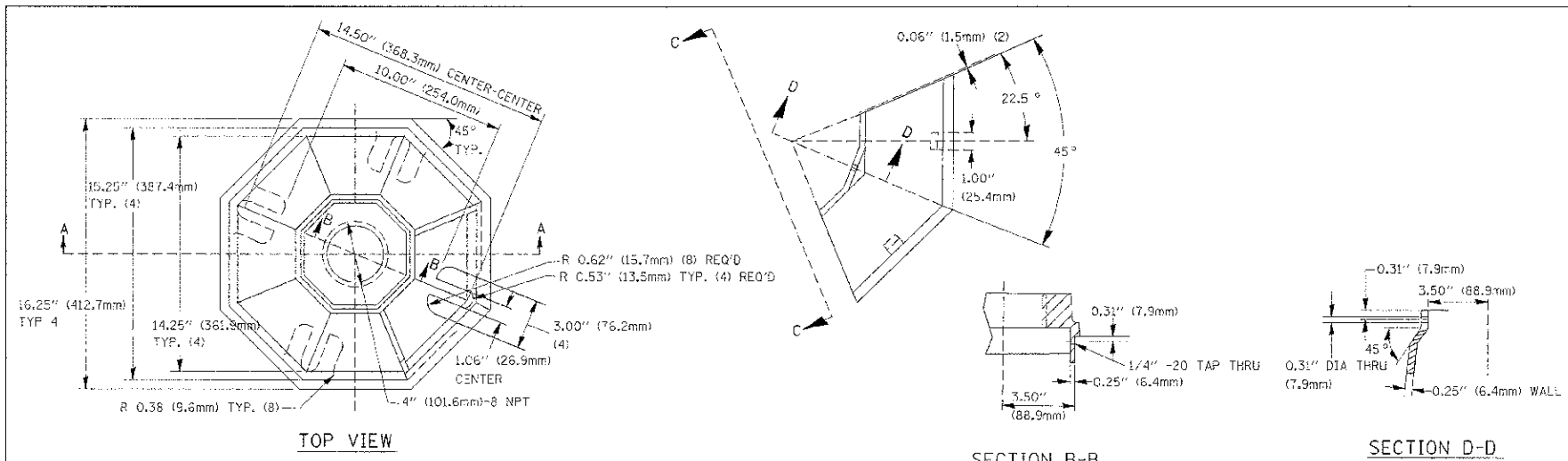


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		CHECKED = DAD	REVISED =
		DATE = 10-28-09	REVISED =

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE			
STANDARD TRAFFIC SIGNAL DESIGN DETAILS			
SCALE: NONE	SHEET NO. 3 OF 6 SHEETS	S.A.	TO STA.

F.A.U. R.T.E. 1644	SECTION 01-00181-00-PP	COUNTY WILL	TOTAL SHEETS 328	SHEET NO. 125
TS-05			CONTRACT NO. 63647	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

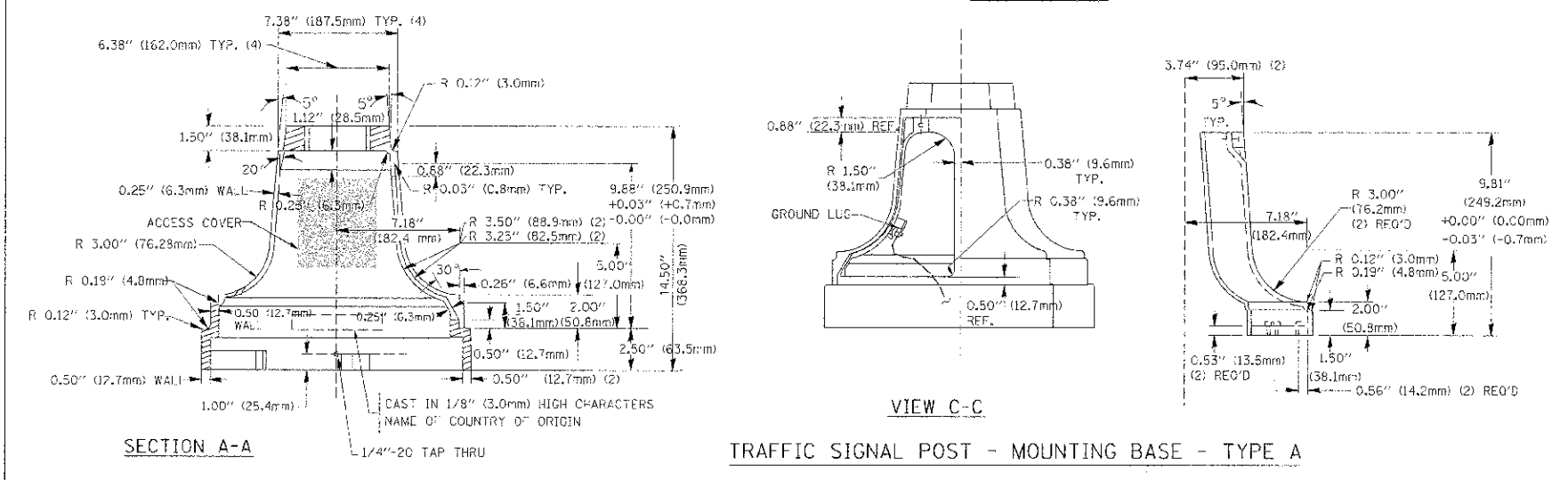


A	B	C	HEIGHT	WEIGHT
VARIABLES	9.5\" (241mm)	19\" (483mm)	7\" (178mm) - 12\" (300mm)	53 lbs (24kg)
VARIABLES	10.75\" (273mm)	21.5\" (546mm)	7\" (178mm) - 12\" (300mm)	68 lbs (31 kg)
VARIABLES	13.0\" (330mm)	26\" (660mm)	7\" (178mm) - 12\" (300mm)	81 lbs (37 kg)
VARIABLES	18.5\" (470mm)	37\" (940mm)	7\" (178mm) - 12\" (300mm)	126 lbs (57 kg)

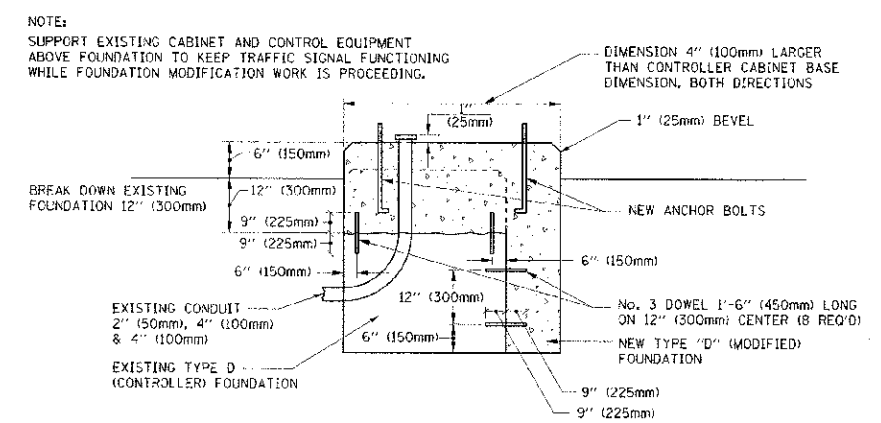
MATERIAL:
 - ASTM A36 STEEL
 - ASTM A-123 HOT DIPPED GALVANIZED

SHROUD

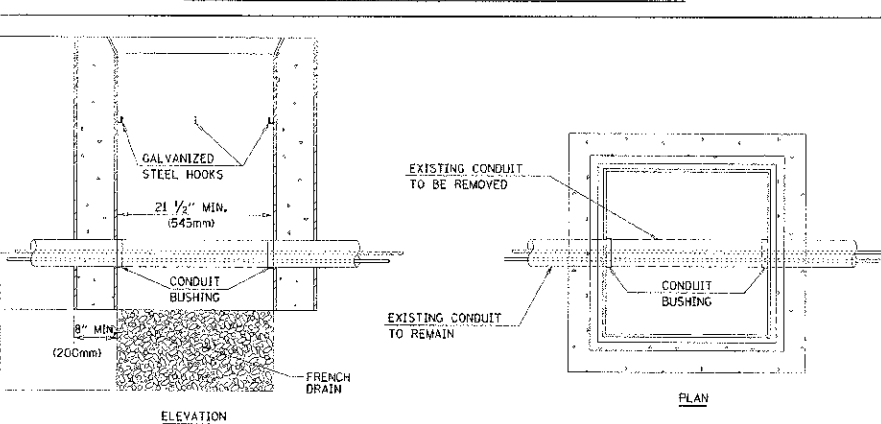
- NOTES:
1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
 2. THE SUPPLIER SHALL VERIFY THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
 3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.



TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A

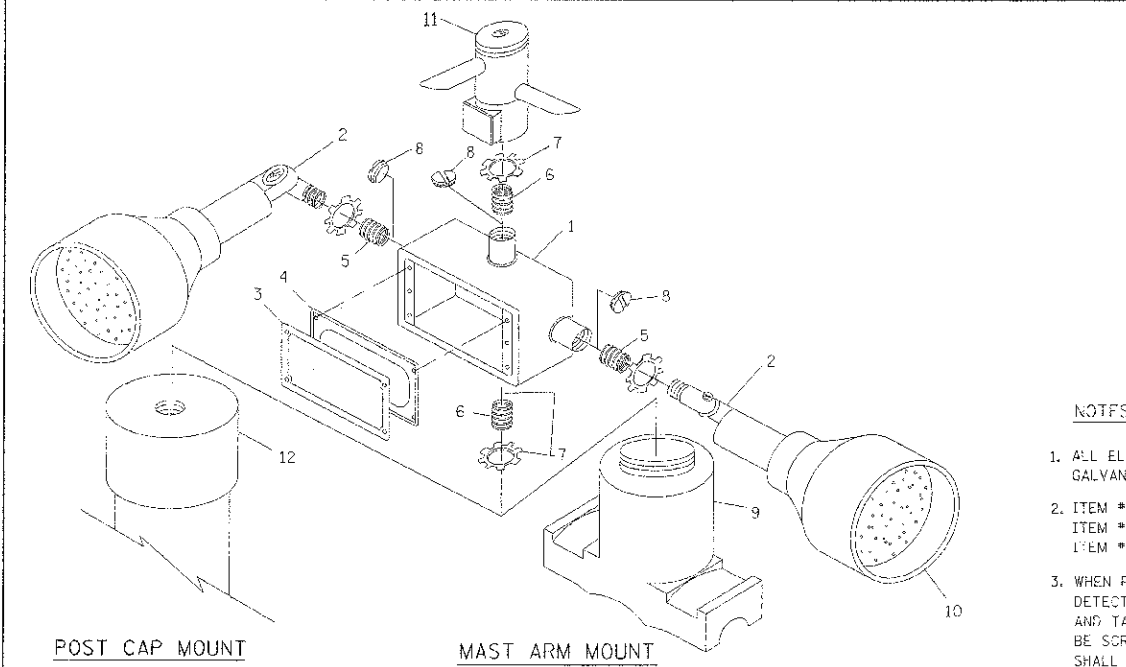


MODIFY EXISTING TYPE "D" FOUNDATION



- NOTES:
1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814COL.
 2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCIDENTAL TO THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT



ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CLIN. (0.000314 CL-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4\" (19 mm) CLOSE NIPPLE
7	3/4\" (19 mm) GASKET
8	3/4\" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP (19 FT. (5.4 m) POST MNL)

- NOTES:
1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
 2. ITEM #1- OZ/GEENEY FSX-1-50 OR EQUIVALENT
 ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
 ITEM #9- "SIANO-11" SADDLE BRACKET OR EQUIVALENT
 3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4\" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.

EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL

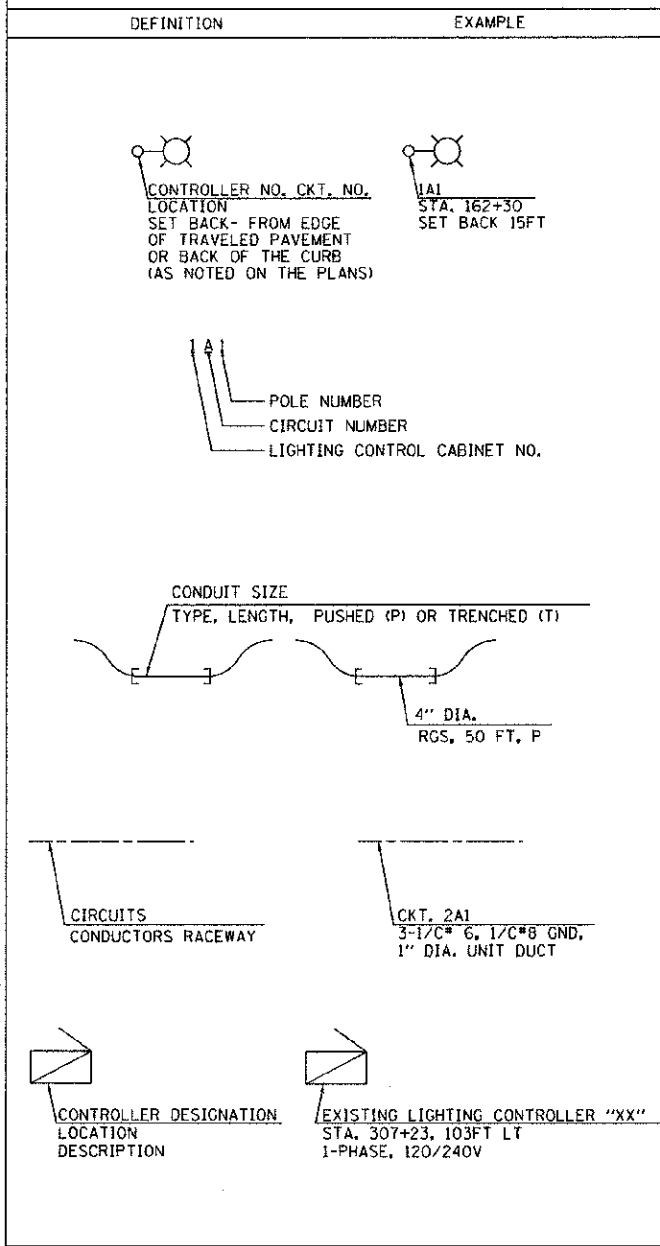
TRAFFIC SIGNAL LEGEND

ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED
CONTROLLER CABINET				EMERGENCY VEHICLE LIGHT DETECTOR				ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE			
RAILROAD CONTROL CABINET				CONFIRMATION BEACON				COAXIAL CABLE			
COMMUNICATIONS CABINET				HANDHOLE				VENDOR CABLE FOR CAMERA			
MASTER CONTROLLER				HEAVY DUTY HANDHOLE				COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED			
MASTER MASTER CONTROLLER				DOUBLE HANDHOLE				FIBER OPTIC CABLE NO. 62.5/125, MM12F			
UNINTERRUPTIBLE POWER SUPPLY				JUNCTION BOX				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F			
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT				GALVANIZED STEEL CONDUIT IN TRENCH (T) OR PUSHED (P)				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F			
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT				TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE				FIBER OPTIC CABLE NO. 62.5/125, (NUMBER OF FIBERS & TYPE TO BE NOTED ON PLANS)			
STEEL MAST ARM ASSEMBLY AND POLE				COMMON TRENCH				GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE			
ALUMINUM MAST ARM ASSEMBLY AND POLE				COILABLE NONMETALLIC CONDUIT (EMPTY)				CONTROLLER CABINET AND FOUNDATION TO BE REMOVED			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE				SYSTEM ITEM				STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA				INTERSECTION ITEM				ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED			
SIGNAL POST				REMOVE ITEM				STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED			
TEMPORARY WOOD POLE (CLASS E OR BETTER) 45 FOOT (13.7m) MINIMUM				RELOCATE ITEM				SIGNAL POST AND FOUNDATION TO BE REMOVED			
GUY WIRE				ABANDON ITEM				INTERSECTION & SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD				12" (300mm) TRAFFIC SIGNAL SECTION				SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)				12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE				EXISTING INTERSECTION LOOP DETECTOR			
SIGNAL HEAD WITH BACKPLATE				SIGNAL FACE				PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD OPTICALLY PROGRAMMED				SIGNAL FACE WITH BACKPLATE, "P" INDICATES PROGRAMMED HEAD				EXISTING PERFORMED INTERSECTION LOOP DETECTOR			
FLASHER INSTALLATION (S DENOTES SOLAR POWER)				12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL				PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
PEDESTRIAN SIGNAL HEAD				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED				PERFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID				PERFORMED SAMPLING (SYSTEM) DETECTOR			
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR				PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER				RAILROAD SYMBOLS			
ILLUMINATED SIGN "NO LEFT TURN"				RADIO INTERCONNECT				EXISTING			
ILLUMINATED SIGN "NO RIGHT TURN"				RADIO REPEATER				RAILROAD CANTILEVER MAST ARM			
DETECTOR LOOP, TYPE I				DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED				FLASHING SIGNAL			
PERFORMED DETECTOR LOOP				GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)				CROSSING GATE			
MICROWAVE VEHICLE SENSOR								CROSSBUCK			
VIDEO DETECTION CAMERA											
VIDEO DETECTION ZONE											
PAN, TILT, ZOOM CAMERA											
WIRELESS DETECTOR SENSOR											
WIRELESS ACCESS POINT											

LEGEND

- PROPOSED LIGHTING UNIT - NAPERVILLE
40' M.H., 12' M.A., 310W, 120V HPS LUMINAIRE
GROUND MOUNTED WITH TRANSFORMER BASE BREAKAWAY
- PROPOSED BRIDGE MOUNTED LIGHTING UNIT - NAPERVILLE
40' M.H., 12' M.A., 310W, 120V HPS LUMINAIRE
- PROPOSED LIGHTING UNIT - BOLINGBROOK
35' M.H., 10' M.A., 250W, 240V HPS LUMINAIRE
GROUND MOUNTED WITH TRANSFORMER BASE BREAKAWAY
- PROPOSED TWIN ARM COMBINATION LIGHTING UNIT
45' M.H., 2-10' M.A., 2-250W, 240V HPS LUMINAIRES
VILLAGE OF BOLINGBROOK
- EXISTING LIGHTING UNIT TO REMAIN
40' M.H., 12' M.A., 310W, 120V HPS LUMINAIRE
CITY OF NAPERVILLE
- EXISTING LIGHTING UNIT TO REMAIN
35' M.H., 10' M.A., 250W, 240V HPS LUMINAIRE
VILLAGE OF BOLINGBROOK
- EXISTING LIGHTING UNIT TO REMAIN AS
TEMPORARY LIGHTING DURING CONSTRUCTION
40' M.H., 12' M.A., 310W, 120V HPS LUMINAIRE
CITY OF NAPERVILLE
- EXISTING LIGHTING UNIT TO REMAIN AS
TEMPORARY LIGHTING DURING CONSTRUCTION
35' M.H., 10' M.A., 250W, 240V HPS LUMINAIRE
VILLAGE OF BOLINGBROOK
- EXISTING LIGHTING UNIT TO BE REMOVED AND RELOCATED
40' M.H., 12' M.A., 310W, 120V HPS LUMINAIRE
CITY OF NAPERVILLE
- EXISTING LIGHTING UNIT TO BE REMOVED AND RELOCATED
35' M.H., 10' M.A., 250W, 240V HPS LUMINAIRE
VILLAGE OF BOLINGBROOK
- RELOCATED LIGHTING UNIT - NAPERVILLE
40' M.H., 12' M.A., 310W, 120V HPS LUMINAIRE
TRANSFORMER BASE BREAKAWAY
- RELOCATED LIGHTING UNIT - BOLINGBROOK
35' M.H., 10' M.A., 250W, 240V HPS LUMINAIRE
TRANSFORMER BASE BREAKAWAY
- TEMPORARY TWIN LIGHT FIXTURES AND MAST ARMS - BOLINGBROOK
35' M.H., 2-10' M.A., 2-250W, 240V HPS LUMINAIRES
ATTACHED TO TEMPORARY TRAFFIC SIGNALS WOOD POLE @ 90°
- RIGID GALVANIZED STEEL (RGS) CONDUIT
PUSHED (P), OR TRENCHED (T)
SIZE AS INDICATED
- UNIT DUCT, AS SPECIFIED IN PLANS
- EXISTING WIRING TO REMAIN
- PROPOSED LIGHTING CONTROLLER CABINET,
SINGLE DOOR, CONSOLE TYPE, 100A, 120/240V, 1Ø
CITY OF NAPERVILLE MAINTAINED
- EXISTING LIGHTING CONTROLLER CABINET,
SINGLE DOOR, CONSOLE TYPE, 100A, 120/240V, 1Ø
CITY OF NAPERVILLE MAINTAINED
- EXISTING LIGHTING CONTROLLER CABINET,
SINGLE DOOR, CONSOLE TYPE, 100A, 240/480V, 1Ø
VILLAGE OF BOLINGBROOK MAINTAINED

CALL-OUT SAMPLES



ABBREVIATIONS

SYMBOL	DESCRIPTION
AC	ALTERNATING CURRENT
A/C	AERIAL CABLE
AFG	ABOVE FINISHED GRADE
CB	CIRCUIT BREAKER
CKT	CIRCUIT
CM	CENTIMETER
CNC	COILABLE NONMETALLIC CONDUIT
CT	CURRENT TRANSFORMER
CP	CONTROL PANEL
DA	DAVIT ARM
DC	DIRECT CURRENT
DIA	DIAMETER
DP	DISTRIBUTION PANEL
E	EXISTING UNIT TO REMAIN
ECA	ELECTRIC CABLE ASSEMBLY
EM	EXISTING UNIT TO BE MODIFIED (e.g. NEW LUMINAIRE, BALLAST OR MAST ARM)
ER	EXISTING RELOCATED UNIT
ET	EXISTING TEMPORARY UNIT TO REMAIN
ETR	EXISTING TEMPORARY RELOCATED UNIT
FT	FEET OR FOOT
FND BW	FOUNDATION BARRIER WALL
FND BW OS	FOUNDATION BARRIER WALL OFFSET
FND CON	FOUNDATION CONCRETE
FND CON OS	FOUNDATION CONCRETE OFFSET
FND MET	FOUNDATION METAL
FND PW	FOUNDATION PARAPET WALL
FU	FUSE
GFCI	GROUND FAULT CIRCUIT INTERRUPTER
GND	GROUND
HID	HIGH INTENSITY DISCHARGE
JB	JUNCTION BOX
KVA	KILOVOLT-AMPERE
KW	KILOWATTS
M	METER
MA	MAST ARM
MM	MILLIMETER
MH	MOUNTING HEIGHT
NO. "	NUMBER
P	PROPOSED
PB	PUSH BUTTON
PNL	PANEL
PVCC RGC	PVC COATED RIGID GALVANIZED CONDUIT
PT	POTENTIAL TRANSFORMER
R	EXISTING UNIT TO BE REMOVED (OWNER SALVAGED U.N.O.)
RL	RELOCATED
RR	EXISTING UNIT TO BE REMOVED AND RELOCATED (REINSTALLED)
RECP	RECEPTACLE
RGC	RIGID GALVANIZED CONDUIT
RGS	RIGID GALVANIZED STEEL
SEL SW	SELECTOR SWITCH
SPARE	SPARE
SPACE	SPACE
SS	STAINLESS STEEL
STA	STATION
T	TEMPORARY LIGHTING UNIT
TB	TRANSFORMER BASE
TMP	TEMPORARY
TR	TEMPORARY UNIT TO BE REMOVED, SALVAGE EQUIPMENT AS SPECIFIED
TRR	TEMPORARY UNIT TO BE REMOVED AND RELOCATED
TUR	TEMPORARY UNIT ON UTILITY POLE TO BE REMOVED
UC	UNDERGROUND CONDUIT
UD	UNIT DUCT
U.N.O.	UNLESS NOTED OTHERWISE
WP	WOOD POLE
XFMR	TRANSFORMER

GENERAL NOTES:

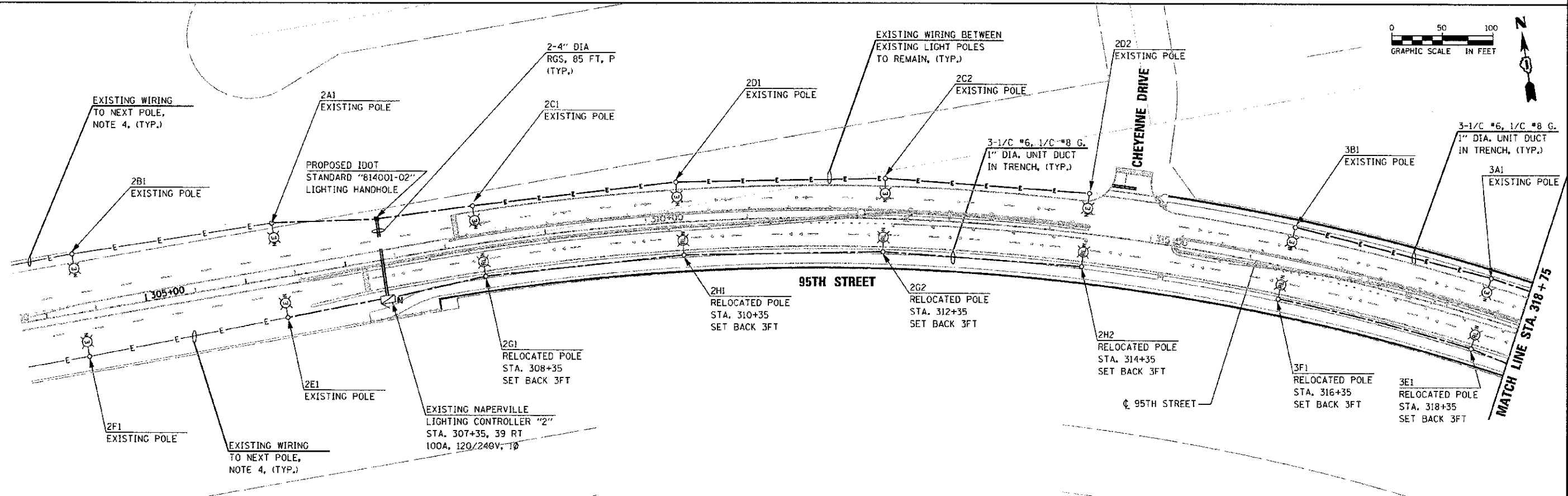
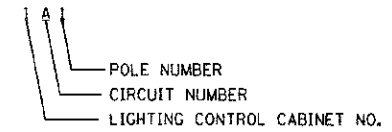
1. THE CONTRACTOR SHALL VERIFY ALL OF THE INFORMATION SHOWN ON THE CONTRACT DRAWINGS WHICH WOULD AFFECT THE WORK UNDER THIS CONTRACT.
2. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ASCERTAIN EXISTING FIELD CONDITIONS BEFORE BIDDING ON THIS PROJECT, SPECIFICALLY AS THEY RELATE TO LUMP SUM ITEMS AND UNIT PRICE ITEMS.
3. ALL NEW CONDUITS, UNIT DUCTS, DIRECT BURIAL CABLES, AND APPURTENANCES ARE INDICATED DIAGRAMMATICALLY ON THE DRAWINGS, THE ACTUAL LOCATIONS IN THE FIELD SHALL MEET WITH APPROVAL OF THE ENGINEER.
4. THE ELECTRICAL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND ASSOCIATED SUPPLEMENTAL CONDITIONS (LATEST EDITION). WORK WITHIN THE CITY OF NAPERVILLE SHALL BE PERFORMED ACCORDING TO CITY STANDARDS AND SPECIAL PROVISIONS FIRST.
5. THE SCALE SHOWN ON PLAN DRAWINGS APPLIES ONLY TO THE FULL SIZE PLANS AND NOT TO REDUCED SIZE PLANS.
6. THE CONTRACTOR SHALL FURNISH AND INSTALL LUMINAIRE LAMPS IN ACCORDANCE WITH THE SUPPLIER'S RECOMMENDATIONS AND IN ACCORDANCE WITH THE SPECIFICATIONS. THE COST OF THIS WORK AND MATERIAL SHALL BE INCLUDED IN THE APPLICABLE LUMINAIRE PAY ITEM. SEPARATE PAYMENT WILL NOT BE MADE.
7. ALL LUMINAIRES SHALL BE ORIENTED WITH THE OPTICS PERPENDICULAR TO THE ROADWAY UNLESS OTHERWISE INDICATED OR DIRECTED BY THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE APPLICABLE LUMINAIRE PAY ITEMS. SEPARATE PAYMENT WILL NOT BE MADE.
8. CONDUITS AND UNIT DUCTS SHALL BE INSTALLED AT A MINIMUM 30" DEPTH BELOW GRADE AND POSITIONED IN THE FIELD TO AVOID CONFLICT WITH ROADWAY UNDERDRAINS AND OTHER EXISTING AND PROPOSED UTILITIES. THE CONTRACTOR SHALL INCREASE DEPTH OF UNIT DUCT AND CONDUIT AS REQUIRED AT NO ADDITIONAL COST. THE CONTRACTOR SHALL COORDINATE RACEWAY DEPTH WITH THE ELECTRICAL DETAILS AND THE ENGINEER.
9. PAYMENT FOR TRENCH AND BACKFILL FOR ELECTRICAL WORK IS INCLUDED AS PART OF THE CONDUIT OR UNIT DUCT BID PRICE. NO SEPARATE MEASUREMENT OR PAYMENT FOR TRENCH AND BACKFILL FOR ELECTRICAL WORK.
10. WHERE THE CONTRACTOR'S EXCAVATION MEETS AN OBSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE ENGINEER FOR DIRECTION IN WRITING PRIOR TO EXCAVATION. THE CONTRACTOR SHALL RESTORE ANY DAMAGE TO EXISTING SYSTEMS OR UTILITIES AND REMOVE EXISTING OBSTRUCTIONS AND FOUNDATIONS TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE APPROPRIATE PAY ITEM.
11. BREAKAWAY DEVICE, TRANSFORMER BASE, 9" HIGH, SHALL BE FURNISHED AND INSTALLED ON ALL NEW AND RELOCATED GROUND MOUNTED POLES. EXISTING BREAKAWAY BASES SHALL BE RELOCATED AND REUSED. EXISTING COUPLINGS SHALL NOT BE REUSED, AND NEW TRANSFORMER BASES SHALL REPLACE COUPLINGS. BRIDGE PARAPET MOUNTED POLES SHALL NOT HAVE BREAKAWAY BASES INSTALLED. BREAKAWAY BASES ARE PAID FOR ON A PER EACH NEW BASE FURNISHED AND INSTALLED.
12. ALL REMOVED EQUIPMENT AND MATERIALS THAT WILL NOT BE REUSED SHALL BE RETURNED TO THE LOCAL MUNICIPALITY THAT OWNS THE EQUIPMENT. THE CONTRACTOR SHALL MAKE ALL NECESSARY ARRANGEMENTS TO SAFELY TRANSPORT AND DELIVER REMOVED EQUIPMENT TO THE MUNICIPALITY'S PUBLIC WORKS STORAGE FACILITY/YARD DESIGNATED BY EACH MUNICIPALITY. COST OF THIS WORK, INCLUDING PROPERLY PACKAGING AND HANDLING OF EQUIPMENT, SHALL BE INCLUDED IN THE UNIT BID PRICE FOR THE ITEMS BEING REMOVED.
13. CONTRACTOR SHALL APPLY FOR AN ELECTRIC SERVICE PERMIT FROM THE NAPERVILLE TRANSPORTATION, ENGINEERING, AND DEVELOPMENT (T.E.D.) BUSINESS GROUP. PERMIT MUST BE OBTAINED FOR ANY NEW, MODIFIED, OR RELOCATED ELECTRIC SERVICE WITHIN THE CITY OF NAPERVILLE LIMITS FOR THE LIGHTING CONTROLLERS SHOWN ON THE PLANS. THE CONTRACTOR MUST ALSO CONTACT THE NAPERVILLE DEPARTMENT OF PUBLIC UTILITIES- ELECTRICAL (NDPU-E) DISPATCH 24 HOURS IN ADVANCE TO SCHEDULE AN ELECTRIC SERVICE CONNECTION BY THE DEPARTMENT. CONTRACTOR SHALL FURNISH AND INSTALL SERVICE WIRING FOR ALL CONTROLLERS UP TO THE NDPU-E ELECTRIC SERVICE PEDESTAL. LEAVE A MINIMUM OF 15 FEET OF SERVICE WIRING COILED OUTSIDE THE PEDESTAL FOR NDPU-E CONNECTION.
14. ALL PROPOSED UNIT DUCTS SHALL BE USE-2 RATED. UNIT DUCTS FOR LIGHTING BRANCH CIRCUITS SHALL CONTAIN #6 CONDUCTORS. NUMBER OF CONDUCTORS SHALL BE AS SHOWN ON THE PLANS. CABLES FOR ELECTRIC SERVICES TO THE LIGHTING CONTROLLER CABINETS SHALL BE USE-2 RATED AND SHALL BE 3-1/2" # 2 CABLES IN 2" RIGID GALVANIZED STEEL CONDUIT.
15. LIGHTING AND ELECTRICAL SYMBOLS ARE OVERSIZED ON THE PLANS FOR CLARITY. CONTRACTOR SHALL USE STATIONS AND SETBACKS TO ACCURATELY LOCATE EQUIPMENT.
16. CONTRACTOR SHALL MAINTAIN 2' MINIMUM CLEARANCE BETWEEN EXISTING UTILITIES AND NEW FOUNDATIONS AND UNDERGROUND FACILITIES. IN AREAS WHERE FOUNDATIONS AND UNDERGROUND FACILITIES ARE PROPOSED ADJACENT TO EXISTING UTILITIES, THE CONTRACTOR SHALL POT HOLE BY VACUUM EXCAVATION OR HAND EXCAVATION TO LOCATE THE EXISTING UTILITY TO VERIFY MINIMUM CLEARANCE REQUIREMENT.

- PROPOSED ELECTRIC UTILITY SERVICE TRANSFORMER OR PEDESTAL
- EXISTING ELECTRIC UTILITY SERVICE TRANSFORMER OR PEDESTAL

- N-RR EXISTING LIGHTING CONTROLLER CABINET TO BE REMOVED AND RELOCATED ON A NEW FOUNDATION, SINGLE DOOR, CONSOLE TYPE, 100A, 120/240V, 1Ø CITY OF NAPERVILLE MAINTAINED
- N-RL NEW LOCATION OF EXISTING LIGHTING CONTROLLER CABINET, CITY OF NAPERVILLE MAINTAINED
- B-RR EXISTING LIGHTING CONTROLLER CABINET TO BE REMOVED AND RELOCATED ON A NEW FOUNDATION, SINGLE DOOR, CONSOLE TYPE, 100A, 240/480V, 1Ø VILLAGE OF BOLINGBROOK MAINTAINED
- B-RL NEW LOCATION OF EXISTING LIGHTING CONTROLLER CABINET, VILLAGE OF BOLINGBROOK MAINTAINED
- H_E EXISTING LIGHTING HANDHOLE
- H_R EXISTING LIGHTING HANDHOLE TO BE REMOVED
- H_P PROPOSED LIGHTING HANDHOLE, IDOT STANDARD

NOTES:

1. ALL LIGHT POLE SETBACKS SHOWN ARE MEASURED FROM THE BACK OF CURB TO THE CENTER OF THE POLE BASE.
2. THE FINAL LOCATIONS OF ALL ELECTRIC SERVICE TRANSFORMERS AND PEDESTALS FEEDING THE LIGHTING CABINETS SHALL BE DETERMINED BY THE UTILITY. ADJUSTMENTS TO THE LENGTH OF THE SERVICE CABLE AND CONDUIT SHALL BE MADE ACCORDINGLY. ALSO SEE GENERAL NOTE 13 ON LIGHTING SHEET 1 FOR ADDITIONAL REQUIREMENTS.
3. ALL MATERIALS, EQUIPMENT, AND LABOR NEEDED TO INSTALL AND CONNECT THE SERVICE FROM THE UTILITY TRANSFORMER OR PEDESTAL TO THE LIGHTING CONTROLLER CABINET SHALL BE PART OF PAY ITEM NO. 80400100 - ELECTRIC SERVICE INSTALLATION. WORK SHALL INCLUDE THE CABLE, CONDUIT AND TRENCHING FOR THE CONDUIT SHOWN ON THE PLANS. ALL CONNECTIONS, SPLICING, AND WIRING TO THE NEW LIGHTING CONTROLLER IS ALSO INCLUDED UNDER THE SAME PAY ITEM. THE SERVICE CABLE AND CONDUIT SIZE SHALL BE AS SHOWN ON THE PLANS. ALSO SEE GENERAL NOTE 13 ON LIGHTING SHEET 1 OF 30 FOR ADDITIONAL SERVICE WIRING AND CONNECTION REQUIREMENTS.
4. CONTRACTOR SHALL VERIFY EXISTING WIRING AND CIRCUITS SHOWN ON THESE PLANS IN THE FIELD. CONTRACTOR SHALL MAKE ALL NECESSARY WIRING ADJUSTMENTS BASED ON FIELD VERIFICATION FINDINGS.
5. LIGHTING HANDHOLE COVER SHALL HAVE "NAPERVILLE LIGHTING" ENGRAVED ON IT.



URS
100 S. WACKER DR.
SUITE 500
CHICAGO, IL 60606
TEL (312) 939-4000
FAX (312) 939-4990

USER NAME: james.purd
DESIGNED: MAE
DRAWN: MAE
CHECKED: DAD
DATE: 2/1/2012

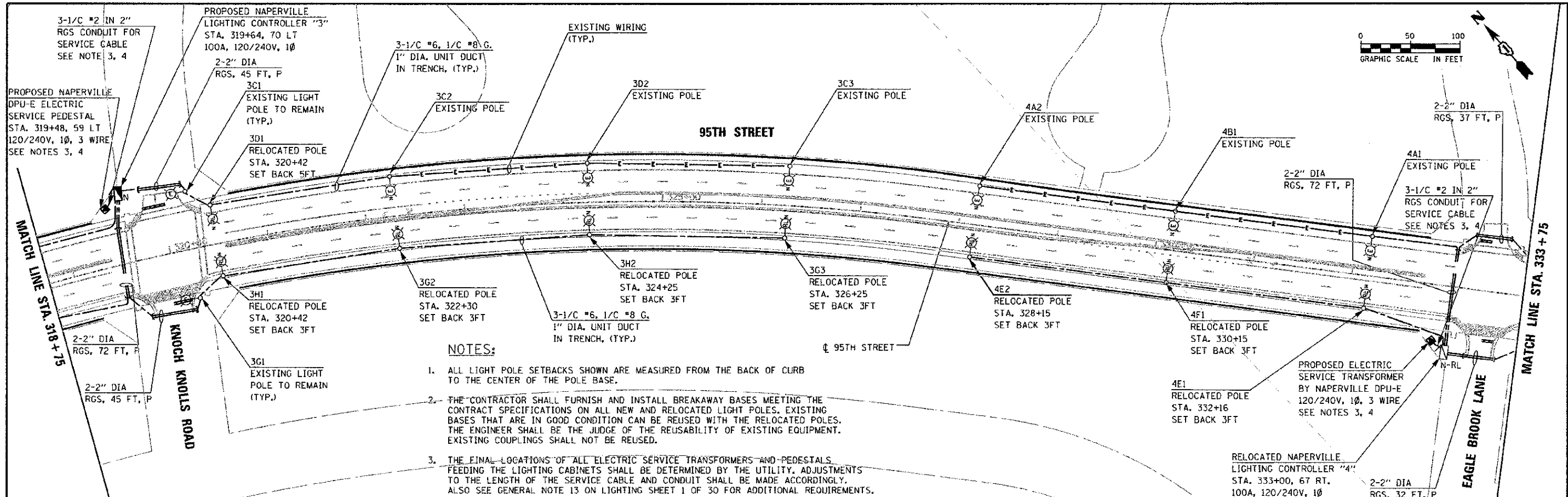
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

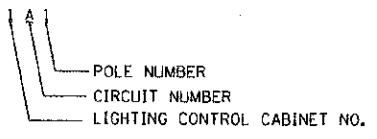
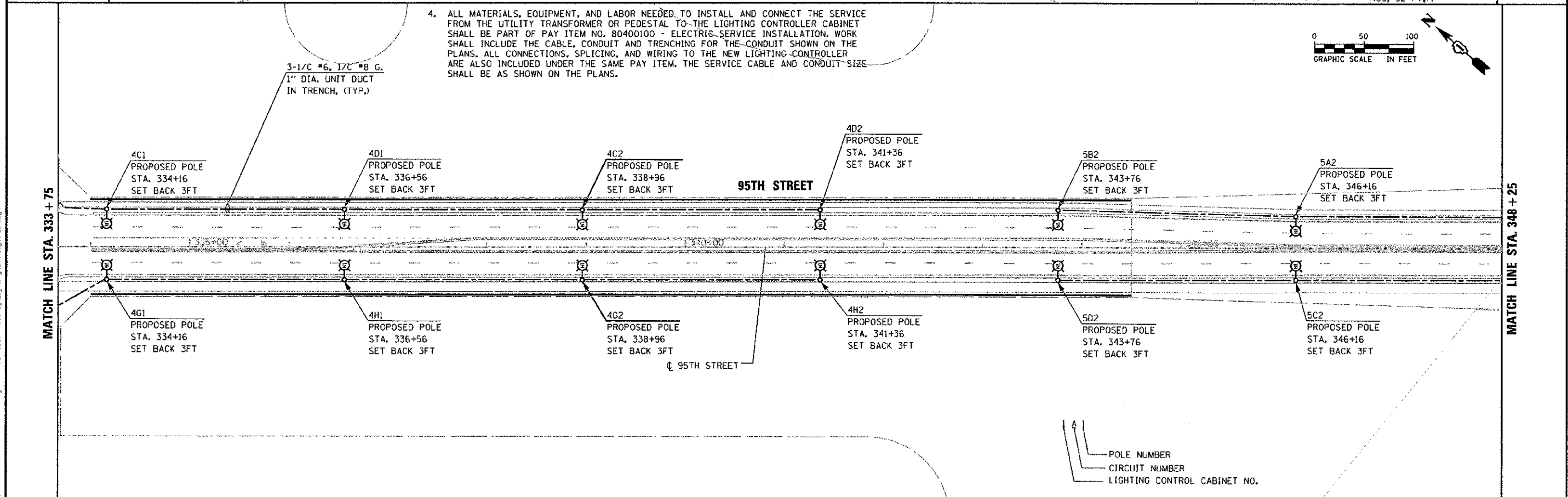
95TH STREET
PROPOSED LIGHTING PLAN (SHEET 1 OF 4)

SCALE: 1"=50'
DRAWING NO. 3 OF 30
STA. 291+70.56 TO STA. 318+75

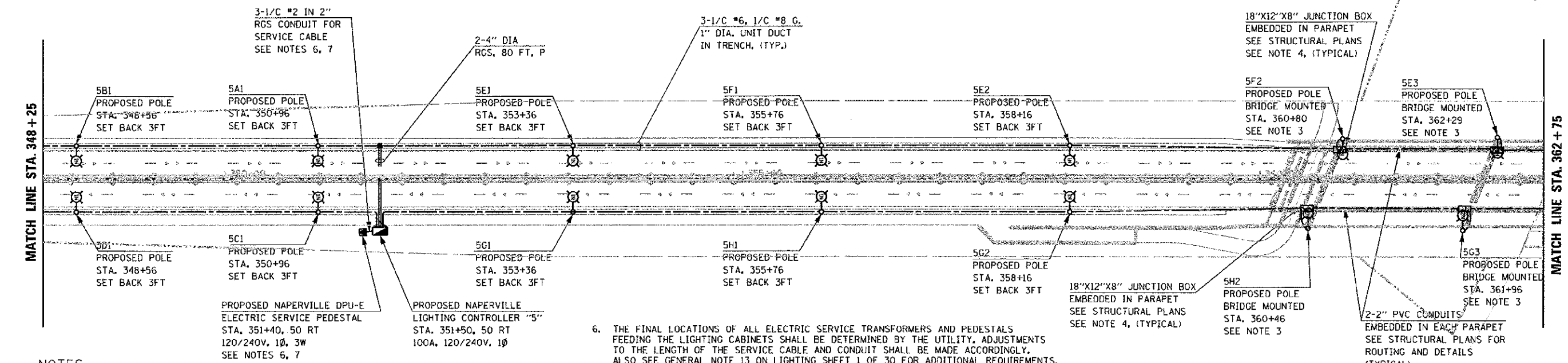
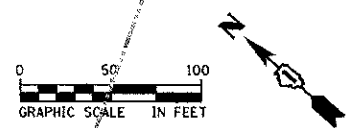
F.A.J. RTE. 1644	SECTION 01-00181-00-PP	COUNTY WILL	TOTAL SHEETS 328	SHEET NO. 131
ILLINOIS FED. AID PROJECT			CONTRACT NO. 63647	



- NOTES:**
- ALL LIGHT POLE SETBACKS SHOWN ARE MEASURED FROM THE BACK OF CURB TO THE CENTER OF THE POLE BASE.
 - THE CONTRACTOR SHALL FURNISH AND INSTALL BREAKAWAY BASES MEETING THE CONTRACT SPECIFICATIONS ON ALL NEW AND RELOCATED LIGHT POLES. EXISTING BASES THAT ARE IN GOOD CONDITION CAN BE REUSED WITH THE RELOCATED POLES. THE ENGINEER SHALL BE THE JUDGE OF THE REUSABILITY OF EXISTING EQUIPMENT. EXISTING COUPLINGS SHALL NOT BE REUSED.
 - THE FINAL LOCATIONS OF ALL ELECTRIC SERVICE TRANSFORMERS AND PEDESTALS FEEDING THE LIGHTING CABINETS SHALL BE DETERMINED BY THE UTILITY. ADJUSTMENTS TO THE LENGTH OF THE SERVICE CABLE AND CONDUIT SHALL BE MADE ACCORDINGLY. ALSO SEE GENERAL NOTE 13 ON LIGHTING SHEET 1 OF 30 FOR ADDITIONAL REQUIREMENTS.
 - ALL MATERIALS, EQUIPMENT, AND LABOR NEEDED TO INSTALL AND CONNECT THE SERVICE FROM THE UTILITY TRANSFORMER OR PEDESTAL TO THE LIGHTING CONTROLLER CABINET SHALL BE PART OF PAY ITEM NO. 80400100 - ELECTRIC SERVICE INSTALLATION. WORK SHALL INCLUDE THE CABLE, CONDUIT AND TRENCHING FOR THE CONDUIT SHOWN ON THE PLANS. ALL CONNECTIONS, SPLICING, AND WIRING TO THE NEW LIGHTING CONTROLLER ARE ALSO INCLUDED UNDER THE SAME PAY ITEM. THE SERVICE CABLE AND CONDUIT SIZE SHALL BE AS SHOWN ON THE PLANS.

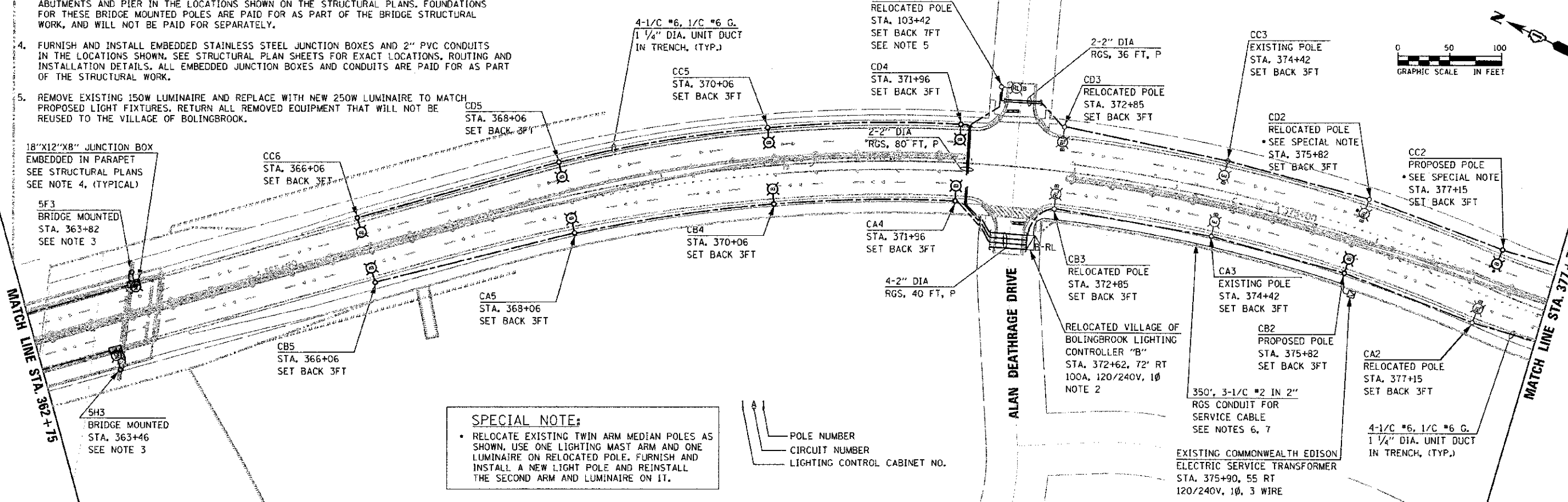
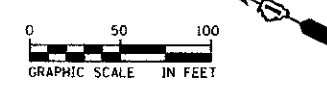


URS 100 S. WACKER DR. SUITE 500 CHICAGO, IL 60606 TEL (312) 939-1000 FAX (312) 939-4198	USER NAME: james.pugh PLOT SCALE: 1"=50' PLOT DATE: 4/16/2012	DESIGNED: MAE DRAWN: MAE CHECKED: DAD DATE: 2/1/2012	REVISED: - REVISED: - REVISED: - REVISED: -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	95TH STREET PROPOSED LIGHTING PLAN (SHEET 2 OF 4) SCALE: 1"=50' DRAWING NO. 4 OF 30 STA. 318+75 TO STA. 348+25	F.A.J. RTE. 1644 SECTION 01-001B1-00-PP COUNTY WILL TOTAL SHEETS 328 SHEET NO. 132 CONTRACT NO. 63647 ILLINOIS FED. AID PROJECT
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NOTES:

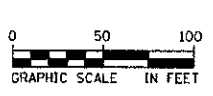
- ALL LIGHT POLE SETBACKS SHOWN ARE MEASURED FROM THE BACK OF CURB TO THE CENTER OF THE POLE BASE.
- REMOVE AND RELOCATE EXISTING LIGHTING CONTROLLER CABINET DUE TO CONFLICT WITH PROPOSED ROADWAY IMPROVEMENTS. REINSTALL CABINET ON A NEW FOUNDATION.
- CONTRACTOR SHALL FURNISH AND INSTALL LIGHT POLES MOUNTED ON TOP OF THE BRIDGE ABUTMENTS AND PIER IN THE LOCATIONS SHOWN ON THE STRUCTURAL PLANS. FOUNDATIONS FOR THESE BRIDGE MOUNTED POLES ARE PAID FOR AS PART OF THE BRIDGE STRUCTURAL WORK, AND WILL NOT BE PAID FOR SEPARATELY.
- FURNISH AND INSTALL EMBEDDED STAINLESS STEEL JUNCTION BOXES AND 2" PVC CONDUITS IN THE LOCATIONS SHOWN. SEE STRUCTURAL PLAN SHEETS FOR EXACT LOCATIONS, ROUTING AND INSTALLATION DETAILS. ALL EMBEDDED JUNCTION BOXES AND CONDUITS ARE PAID FOR AS PART OF THE STRUCTURAL WORK.
- REMOVE EXISTING 150W LUMINAIRE AND REPLACE WITH NEW 250W LUMINAIRE TO MATCH PROPOSED LIGHT FIXTURES. RETURN ALL REMOVED EQUIPMENT THAT WILL NOT BE REUSED TO THE VILLAGE OF BOLINGBROOK.
- THE FINAL LOCATIONS OF ALL ELECTRIC SERVICE TRANSFORMERS AND PEDESTALS FEEDING THE LIGHTING CABINETS SHALL BE DETERMINED BY THE UTILITY. ADJUSTMENTS TO THE LENGTH OF THE SERVICE CABLE AND CONDUIT SHALL BE MADE ACCORDINGLY. ALSO SEE GENERAL NOTE 13 ON LIGHTING SHEET 1 OF 30 FOR ADDITIONAL REQUIREMENTS.
- ALL MATERIALS, EQUIPMENT, AND LABOR NEEDED TO INSTALL AND CONNECT THE SERVICE FROM THE UTILITY TRANSFORMER OR PEDESTAL TO THE LIGHTING CONTROLLER CABINET SHALL BE PART OF PAY ITEM NO. 80400100 - ELECTRIC SERVICE INSTALLATION, WORK SHALL INCLUDE THE CABLE, CONDUIT AND TRENCHING FOR THE CONDUIT SHOWN ON THE PLANS. ALL CONNECTIONS, SPLICING, AND WIRING TO THE NEW LIGHTING CONTROLLER ARE ALSO INCLUDED UNDER THE SAME PAY ITEM. THE SERVICE CABLE AND CONDUIT SIZE SHALL BE AS SHOWN ON THE PLANS.



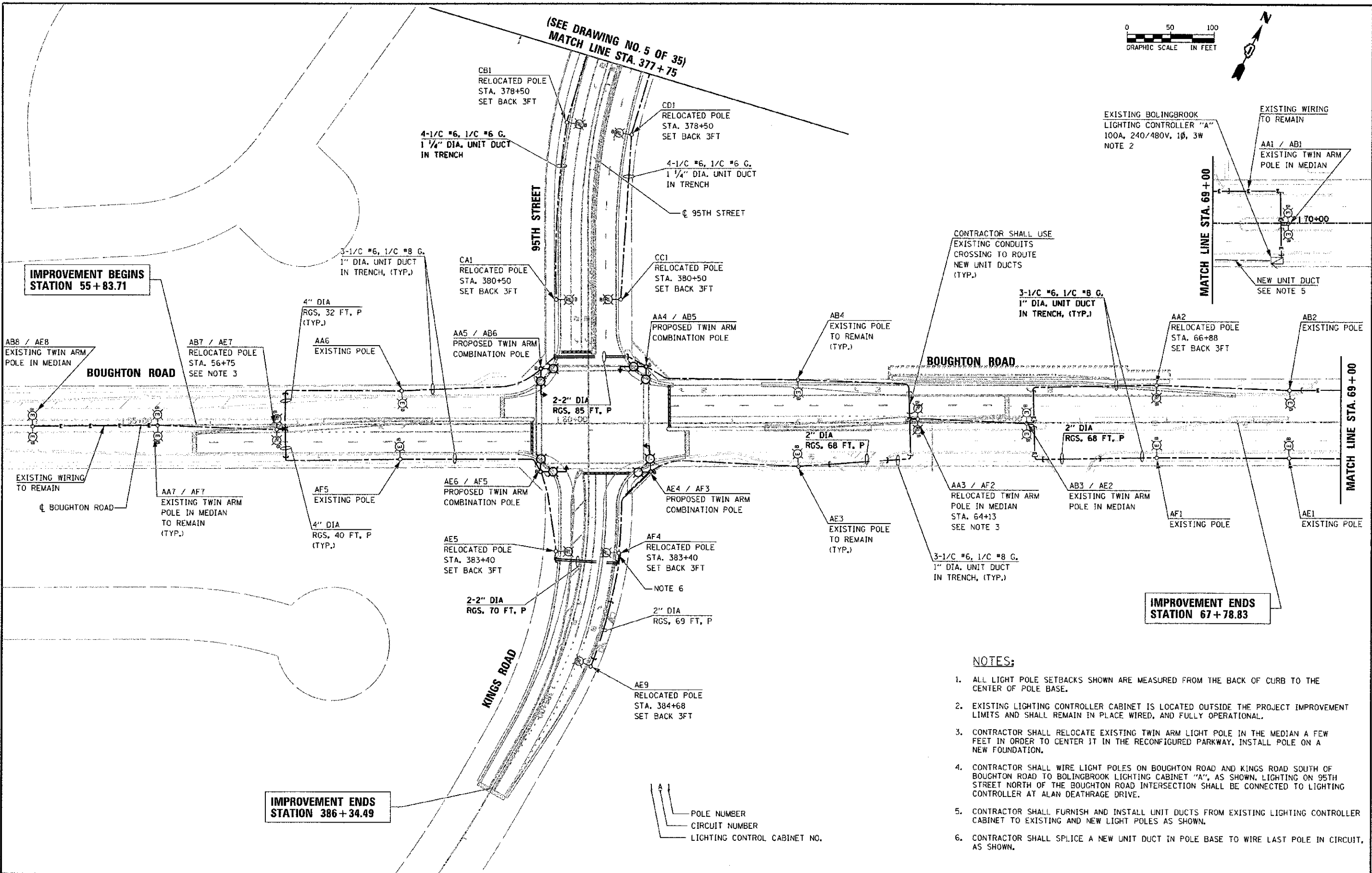
SPECIAL NOTE:

- RELOCATE EXISTING TWIN ARM MEDIAN POLES AS SHOWN. USE ONE LIGHTING MAST ARM AND ONE LUMINAIRE ON RELOCATED POLE. FURNISH AND INSTALL A NEW LIGHT POLE AND REINSTALL THE SECOND ARM AND LUMINAIRE ON IT.

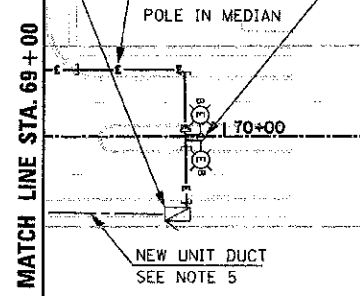
POLE NUMBER
CIRCUIT NUMBER
LIGHTING CONTROL CABINET NO.



(SEE DRAWING NO. 5 OF 35)
MATCH LINE STA. 377+75



EXISTING BOLINGBROOK LIGHTING CONTROLLER "A"
100A, 240/480V, 1Ø, 3W
NOTE 2



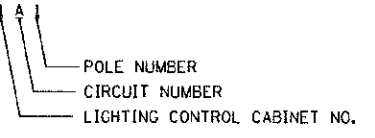
CONTRACTOR SHALL USE EXISTING CONDUITS CROSSING TO ROUTE NEW UNIT DUCTS (TYP.)

IMPROVEMENT ENDS STATION 67+78.83

IMPROVEMENT ENDS STATION 386+34.49

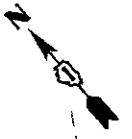
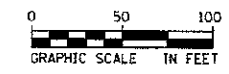
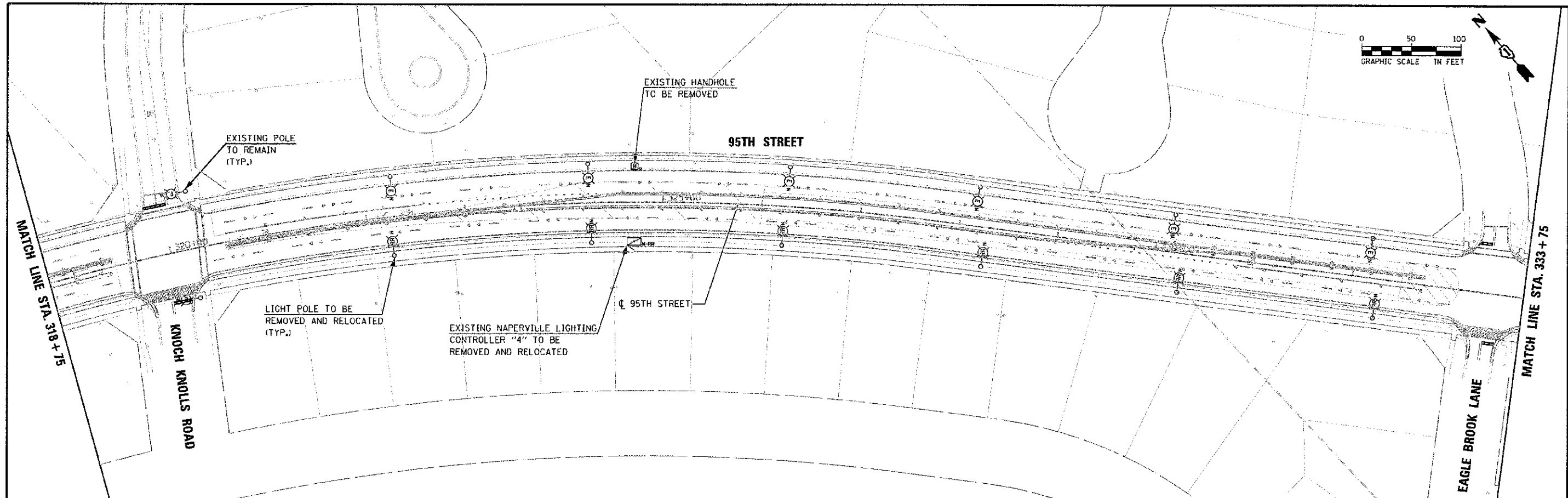
NOTES:

- ALL LIGHT POLE SETBACKS SHOWN ARE MEASURED FROM THE BACK OF CURB TO THE CENTER OF POLE BASE.
- EXISTING LIGHTING CONTROLLER CABINET IS LOCATED OUTSIDE THE PROJECT IMPROVEMENT LIMITS AND SHALL REMAIN IN PLACE WIRED, AND FULLY OPERATIONAL.
- CONTRACTOR SHALL RELOCATE EXISTING TWIN ARM LIGHT POLE IN THE MEDIAN A FEW FEET IN ORDER TO CENTER IT IN THE RECONFIGURED PARKWAY. INSTALL POLE ON A NEW FOUNDATION.
- CONTRACTOR SHALL WIRE LIGHT POLES ON BOUGHTON ROAD AND KINGS ROAD SOUTH OF BOUGHTON ROAD TO BOLINGBROOK LIGHTING CABINET "A", AS SHOWN. LIGHTING ON 95TH STREET NORTH OF THE BOUGHTON ROAD INTERSECTION SHALL BE CONNECTED TO LIGHTING CONTROLLER AT ALAN DEATHRAGE DRIVE.
- CONTRACTOR SHALL FURNISH AND INSTALL UNIT DUCTS FROM EXISTING LIGHTING CONTROLLER CABINET TO EXISTING AND NEW LIGHT POLES AS SHOWN.
- CONTRACTOR SHALL SPLICE A NEW UNIT DUCT IN POLE BASE TO WIRE LAST POLE IN CIRCUIT, AS SHOWN.



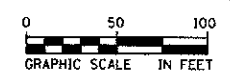
URS 100 S. WACKER DR. SUITE 500 CHICAGO, IL 60606 TEL (312) 539-1000 FAX (312) 539-4938	USER: NME PLT: SCALE: 1"=50' DATE: 10/18/2012	DESIGNED: MAE DRAWN: MAE CHECKED: DAD DATE: 2/1/2012	REVISED: - REVISED: - REVISED: - REVISED: -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	95TH STREET PROPOSED LIGHTING PLAN (SHEET 4 OF 4)	F.A.I. RTE. 1644 SECTION 01-001B1-00-PP COUNTY WILL TOTAL SHEETS 328 SHEET NO. 134 CONTRACT NO. 63647 ILLINOIS FED. AID PROJECT
	SCALE: 1"=50' DRAWING NO. 6 OF 30 STA. 377+75 TO STA. 386+34.49					

FILE NAME: c:\projects\2012\10\18\95th\drawing\95th\roadway\lighting\04.dwg 10/18/2012 2:02:59 PM

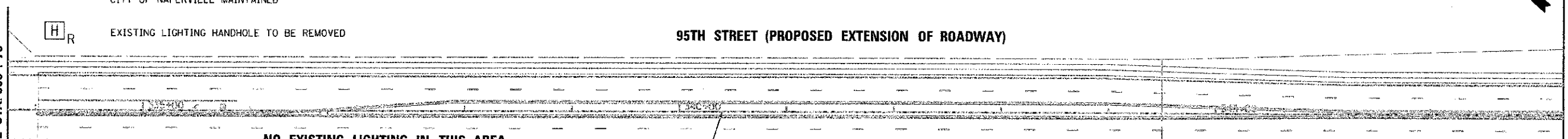


LEGEND

- EXISTING LIGHTING UNIT TO REMAIN
40' M.H., 12' M.A., 310W, 120V HPS LUMINAIRE
CITY OF NAPERVILLE
- EXISTING LIGHTING UNIT TO BE REMOVED AND RELOCATED
40' M.H., 12' M.A., 310W, 120V HPS LUMINAIRE
CITY OF NAPERVILLE
- EXISTING LIGHTING CONTROLLER CABINET
TO BE REMOVED AND RELOCATED
CITY OF NAPERVILLE MAINTAINED
- EXISTING LIGHTING HANDHOLE TO BE REMOVED



MATCH LINE STA. 333+75



MATCH LINE STA. 348+25

95TH STREET (PROPOSED EXTENSION OF ROADWAY)

NO EXISTING LIGHTING IN THIS AREA

CL 95TH STREET (PROPOSED EXTENSION)

NOTES:

1. THE CONTRACTOR SHALL NOT DE-ENERGIZE, DISCONNECT, OR REMOVE ANY EXISTING LIGHTING EQUIPMENT UNLESS A REPLACEMENT EQUIPMENT IS IN PLACE AND MADE OPERATIONAL BEFORE THE EXISTING EQUIPMENT CAN BE REMOVED.
2. NEW LOCATIONS OF ALL RELOCATED LIGHTING EQUIPMENT ARE SHOWN ON THE TEMPORARY AND PROPOSED LIGHTING PLANS.
3. INSTALL RELOCATED EXISTING LIGHTING CONTROLLER CABINETS ON NEW FOUNDATIONS. NEW FOUNDATIONS MUST MATCH EXISTING FOUNDATIONS IN SIZE AND CONDUIT LAYOUT. CONTRACTOR SHALL ALSO FURNISH AND INSTALL CONCRETE WORK PLATFORM IN FRONT OF NEW CONTROLLER CABINET PER THE STANDARDS AND DETAILS SHOWN ON THESE PLANS.
4. THE CONTRACTOR SHALL PROPERLY HANDLE AND DELIVER ALL REMOVED LIGHTING EQUIPMENT THAT WILL NOT BE REUSED TO THE OWNER'S PUBLIC WORKS DEPARTMENT DESIGNATED STORAGE/MAINTENANCE YARD. ALL COSTS RELATED TO REMOVAL, HANDLING, AND TRANSPORTATION OF THE REMOVED EQUIPMENT SHALL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED.
5. EXISTING WIRES AND CABLES THAT WILL NOT BE REUSED SHALL BE ABANDONED IN PLACE OR REMOVED BY THE CONTRACTOR AND DISPOSED OF OUTSIDE THE RIGHT OF WAY AT HIS OWN EXPENSE.



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CHECKED - DAD	REVISED -
DATE - 2/1/2012	REVISED -

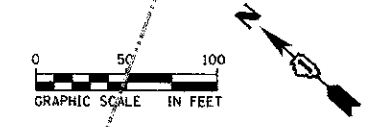
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**95TH STREET
EXISTING & REMOVAL LIGHTING PLAN (SHEET 2 OF 4)**

SCALE: 1"=50' DRAWING NO. 8 OF 30 STA. 318+75 TO STA. 348+25

F.A.U. RTE. 1644	SECTION 01-00181-00-PP	COUNTY WILL	TOTAL SHEETS 328	SHEET NO. 136
CONTRACT NO. 63647			ILLINOIS FED. AID PROJECT	

FILE NAME: c:\awebos\25387381_95th\drawings\transportation\roadway\lighting\Sheet 2 of 4.dwg



95TH STREET (PROPOSED EXTENSION OF ROADWAY)

NO EXISTING LIGHTING IN THIS AREA

MATCH LINE STA. 348+25

MATCH LINE STA. 362+75

95TH STREET (PROPOSED EXTENSION)

DUPAGE RIVER

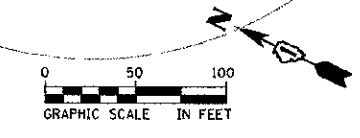
NOTES:

1. THE CONTRACTOR SHALL NOT DE-ENERGIZE, DISCONNECT, OR REMOVE ANY EXISTING LIGHTING EQUIPMENT UNLESS A REPLACEMENT EQUIPMENT IS IN PLACE AND MADE OPERATIONAL BEFORE THE EXISTING EQUIPMENT CAN BE REMOVED.
2. NEW LOCATIONS OF ALL RELOCATED LIGHTING EQUIPMENT ARE SHOWN ON THE TEMPORARY AND PROPOSED LIGHTING PLANS.
3. INSTALL RELOCATED EXISTING LIGHTING CONTROLLER CABINETS ON NEW FOUNDATIONS. NEW FOUNDATIONS MUST MATCH EXISTING FOUNDATIONS IN SIZE AND CONDUIT LAYOUT. CONTRACTOR SHALL ALSO FURNISH AND INSTALL CONCRETE WORK PLATFORM IN FRONT OF NEW CONTROLLER CABINET PER THE STANDARDS AND DETAILS SHOWN ON THESE PLANS.

4. THE CONTRACTOR SHALL PROPERLY HANDLE AND DELIVER ALL REMOVED LIGHTING EQUIPMENT THAT WILL NOT BE REUSED TO THE OWNER'S PUBLIC WORKS DEPARTMENT DESIGNATED STORAGE/MAINTENANCE YARD. ALL COSTS RELATED TO REMOVAL, HANDLING, AND TRANSPORTATION OF THE REMOVED EQUIPMENT SHALL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED.

5. EXISTING WIRES AND CABLES THAT WILL NOT BE REUSED SHALL BE ABANDONED IN PLACE OR REMOVED BY THE CONTRACTOR AND DISPOSED OF OUTSIDE THE RIGHT OF WAY AT HIS OWN EXPENSE.

6. CONTRACTOR SHALL REMOVE THE EXISTING TWIN ARM MEDIAN LIGHT POLES SHOWN AND RELOCATE THE LIGHT POLE AND ONE MAST ARM AND A LUMINAIRE ON A NEW FOUNDATION IN THE NEW LOCATION SHOWN. THE REMAINING LIGHTING ARM AND LUMINAIRE SHALL BE REINSTALLED ON A NEW CONTRACTOR FURNISHED AND INSTALLED LIGHT POLE ON A NEW FOUNDATION, AS SHOWN.



95TH STREET (PROPOSED EXTENSION)

95TH STREET (PROPOSED EXTENSION)

ALAN DEATHRAG DRIVE

EXISTING POLE TO REMAIN (TYP.)

EXISTING LIGHTING CONTROLLER "C" TO BE REMOVED AND RELOCATED. 100A, 240/480V, 1Ø, 3W VILLAGE OF BOLINGBROOK MAINTAINED STA. 373+08, 39' RT.

TWIN ARM LIGHT POLE TO BE REMOVED AND RELOCATED SEE NOTE 6 (TYP.)

MATCH LINE STA. 362+75

MATCH LINE STA. 377+75

LEGEND

- EXISTING LIGHTING UNIT TO REMAIN
35' M.H., 10' M.A., 250W, 240V HPS LUMINAIRE
VILLAGE OF BOLINGBROOK
- EXISTING LIGHTING UNIT TO BE REMOVED AND RELOCATED
35' M.H., 10' M.A., 250W, 240V HPS LUMINAIRE
VILLAGE OF BOLINGBROOK
- EXISTING LIGHTING CONTROLLER CABINET "C",
TO BE REMOVED AND RELOCATED
SINGLE DOOR, CONSOLE TYPE, 100A, 240/480V, 1Ø
VILLAGE OF BOLINGBROOK MAINTAINED



100 S. WACKER DR.
SUITE 500
CHICAGO, IL 60606
TEL (312) 939-1000
FAX (312) 939-4998

USER: NPH: = jpmk:quh
DESIGNED - MAE
DRAWN - MAE
CHECKED - DAD
DATE - 2/1/2012
PLD: SCALE = 50:NEW 7/2 IN.
PLD: DATE = 10/16/2012

DESIGNED - MAE
DRAWN - MAE
CHECKED - DAD
DATE - 2/1/2012

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





STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

95TH STREET
EXISTING & REMOVAL LIGHTING PLAN (SHEET 3 OF 4)

SCALE: 1"=50' DRAWING NO. 9 OF 30 STA. 348+25 TO STA. 377+75

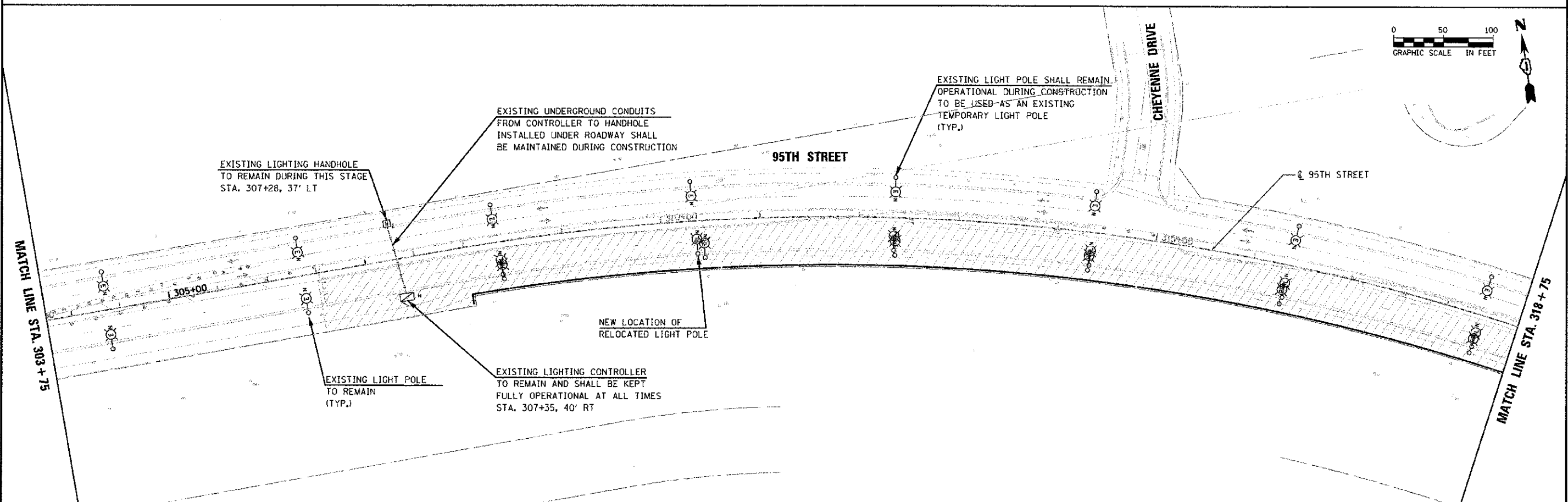
F.A.U. RTE. 1644	SECTION 01-00181-00-FP	COUNTY WILL	TOTAL SHEETS 328	SHEET NO. 137
ILLINOIS FED. AID PROJECT			CONTRACT NO. 63647	

LEGEND

-  EXISTING LIGHTING UNIT TO REMAIN
40' M.H., 12' M.A., 310W, 120V HPS LUMINAIRE
CITY OF NAPERVILLE
-  EXISTING LIGHTING UNIT TO BE REMOVED AND RELOCATED
40' M.H., 12' M.A., 310W, 120V HPS LUMINAIRE
CITY OF NAPERVILLE
-  EXISTING LIGHTING UNIT TO REMAIN DURING CONSTRUCTION
AND BE USED AS TEMPORARY LIGHT POLE
40' M.H., 12' M.A., 310W, 120V HPS LUMINAIRE
CITY OF NAPERVILLE
-  NEW LOCATION OF EXISTING LIGHT POLES
THAT ARE TO BE REMOVED AND RELOCATED
CITY OF NAPERVILLE
-  EXISTING LIGHTING CONTROLLER CABINET,
SINGLE DOOR, CONSOLE TYPE, 100A, 120/240V, 1Ø
CITY OF NAPERVILLE MAINTAINED
-  EXISTING LIGHTING HANDHOLE TO REMAIN DURING THIS STAGE

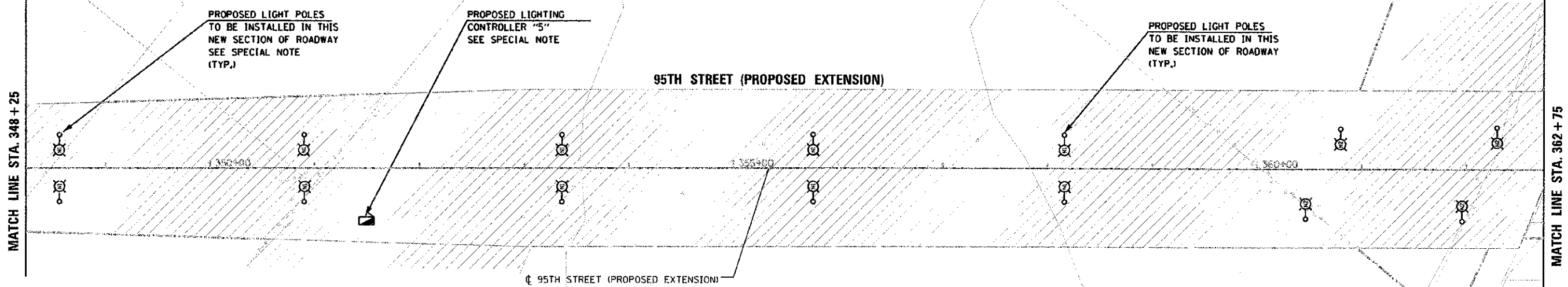
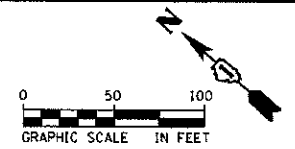
NOTES:

1. THE CONTRACTOR SHALL MAINTAIN THE EXISTING LIGHTING EQUIPMENT FULLY OPERATIONAL ON THE TRAFFIC SIDE OF THE ROADWAY. THE CONTRACTOR SHALL REWIRE EXISTING LIGHTING CIRCUITS, AS NECESSARY, TO KEEP THE LIGHT POLES OPERATIONAL DURING CONSTRUCTION. THE CONTRACTOR SHALL PROVIDE ANY EQUIPMENT, MATERIALS, AND LABOR NECESSARY TO ACCOMPLISH THIS GOAL INCLUDING TEMPORARY WIRING (UNDERGROUND OR AERIAL) OF POLES, LUMINAIRES AND LIGHTING CONTROLLERS.
2. ALL LIGHTING RELATED EQUIPMENT, MATERIALS AND LABOR FURNISHED AND INSTALLED BY THE CONTRACTOR DURING CONSTRUCTION TO MAINTAIN THE OPERATION OF THE EXISTING LIGHTING SYSTEM SHALL BE PAID FOR UNDER PAY ITEM 'MAINTAIN EXISTING LIGHTING SYSTEM'. THIS PAYMENT SHALL INCLUDE ALL MATERIALS AND EQUIPMENT NOT SPECIFICALLY SHOWN ON THE PLANS, BUT ARE NECESSARY TO MAINTAIN THE OPERATION OF THE LIGHTING SYSTEM DURING CONSTRUCTION.
3. FURNISH AND INSTALL AERIAL CABLES AND WOOD POLES TO SUPPORT CABLES IF PERMANENT UNDERGROUND WIRING CANNOT BE INSTALLED OR EXISTING WIRING CANNOT BE MAINTAINED DURING CONSTRUCTION. NUMBER AND LOCATIONS OF TEMPORARY WOOD POLES AND AERIAL WIRING IS SHOWN AS AN EXAMPLE ONLY, AND ARE NOT LIMITED TO WHAT IS SHOWN ON THESE PLANS. PAYMENT FOR THIS WORK SHALL BE AS DESCRIBED IN NOTE 2 ABOVE.



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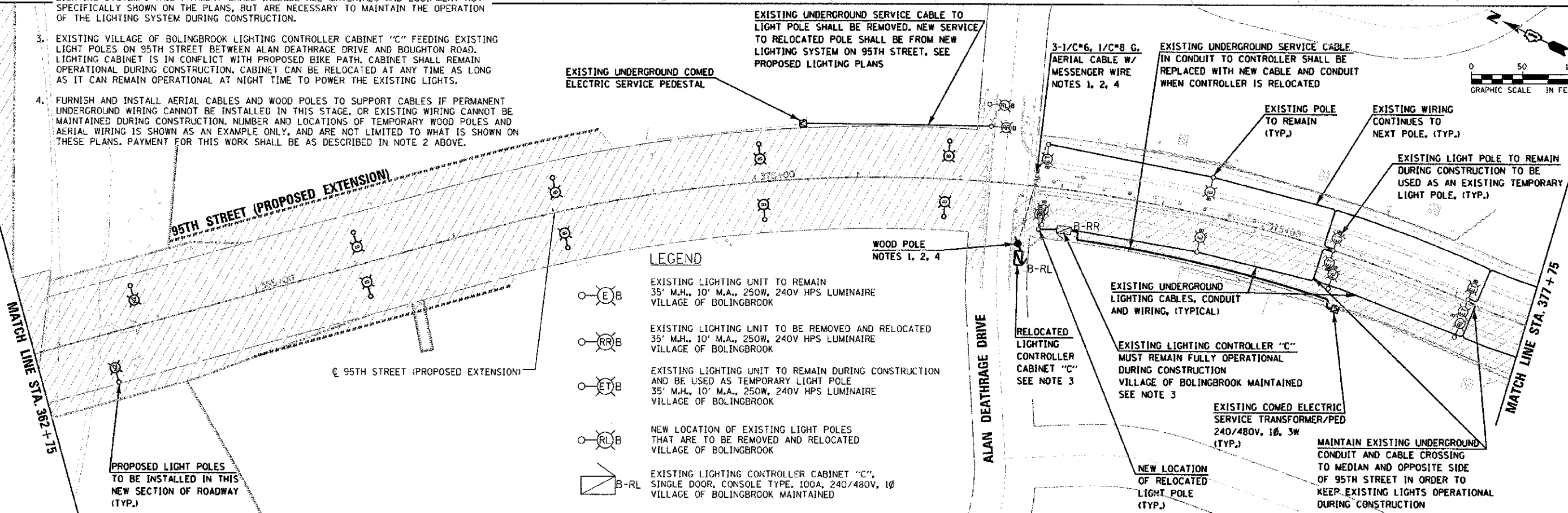
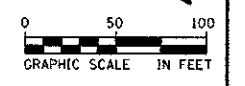
URS	100 S. WACKER DR. SUITE 500 CHICAGO, IL 60606 TEL (312) 939-1000 FAX (312) 939-4998	USER NAME: j.mason.p.usb	DESIGNED - MAE	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	95TH STREET TEMPORARY LIGHTING PLAN - STAGE 1 (SHEET 1 OF 4)	F.A.I.L. RTE. 1644	SECTION 01-00181-00-FP	COUNTY WILL	TOTAL SHEETS 328	SHEET NO. 139
		PLT SCALE: 1" = 50'	CHECKED - DAD	REVISED -		SCALE: 1" = 50' DRAWING NO. 11 OF 30 STA. 291+70.56 TO STA. 318+75					
		PLT DATE: 10/16/2012	DATE: 2/1/2012	REVISED -							ILLINOIS FED. AID PROJECT



NOTES:

1. THE CONTRACTOR SHALL MAINTAIN THE EXISTING LIGHTING EQUIPMENT IN A FULLY OPERATIONAL STATE ON THE TRAFFIC SIDE OF THE ROADWAY. THE CONTRACTOR SHALL REWIRE EXISTING LIGHTING CIRCUITS, AS NECESSARY, TO KEEP THE LIGHT POLES OPERATIONAL DURING CONSTRUCTION. THE CONTRACTOR SHALL PROVIDE AND INSTALL ANY EQUIPMENT, MATERIALS, AND LABOR NECESSARY TO ACCOMPLISH THIS GOAL INCLUDING WOOD POLES, TEMPORARY WIRING (UNDERGROUND AND AERIAL) OF POLES, LUMINAIRES AND LIGHTING CONTROLLERS.
2. ALL LIGHTING RELATED EQUIPMENT, MATERIALS AND LABOR FURNISHED AND INSTALLED BY THE CONTRACTOR DURING CONSTRUCTION TO MAINTAIN THE OPERATION OF THE EXISTING LIGHTING SYSTEM SHALL BE PAID FOR UNDER PAY ITEM "MAINTAIN EXISTING LIGHTING SYSTEM". THIS PAYMENT SHALL INCLUDE ALL MATERIALS AND EQUIPMENT NOT SPECIFICALLY SHOWN ON THE PLANS, BUT ARE NECESSARY TO MAINTAIN THE OPERATION OF THE LIGHTING SYSTEM DURING CONSTRUCTION.
3. EXISTING VILLAGE OF BOLINGBROOK LIGHTING CONTROLLER CABINET "C" FEEDING EXISTING LIGHT POLES ON 95TH STREET BETWEEN ALAN DEATHRAGE DRIVE AND BOUGHTON ROAD. LIGHTING CABINET IS IN CONFLICT WITH PROPOSED BIKE PATH. CABINET SHALL REMAIN OPERATIONAL DURING CONSTRUCTION. CABINET CAN BE RELOCATED AT ANY TIME AS LONG AS IT CAN REMAIN OPERATIONAL AT NIGHT TIME TO POWER THE EXISTING LIGHTS.
4. FURNISH AND INSTALL AERIAL CABLES AND WOOD POLES TO SUPPORT CABLES IF PERMANENT UNDERGROUND WIRING CANNOT BE INSTALLED IN THIS STAGE, OR EXISTING WIRING CANNOT BE MAINTAINED DURING CONSTRUCTION. NUMBER AND LOCATIONS OF TEMPORARY WOOD POLES AND AERIAL WIRING IS SHOWN AS AN EXAMPLE ONLY, AND ARE NOT LIMITED TO WHAT IS SHOWN ON THESE PLANS. PAYMENT FOR THIS WORK SHALL BE AS DESCRIBED IN NOTE 2 ABOVE.

SPECIAL NOTE:
NEW LIGHT POLES AND CONTROLLERS PROPOSED IN THE NEW SECTION OF 95TH STREET DO NOT NEED TO BE OPERATIONAL UNTIL THE ROADWAY IS OPEN TO TRAFFIC.








LEGEND

- EXISTING LIGHTING UNIT TO REMAIN
35' M.H., 10' M.A., 250W, 240V HPS LUMINAIRE
VILLAGE OF BOLINGBROOK
- EXISTING LIGHTING UNIT TO BE REMOVED AND RELOCATED
35' M.H., 10' M.A., 250W, 240V HPS LUMINAIRE
VILLAGE OF BOLINGBROOK
- EXISTING LIGHTING UNIT TO REMAIN DURING CONSTRUCTION
AND BE USED AS TEMPORARY LIGHT POLE
35' M.H., 10' M.A., 250W, 240V HPS LUMINAIRE
VILLAGE OF BOLINGBROOK
- NEW LOCATION OF EXISTING LIGHT POLES
THAT ARE TO BE REMOVED AND RELOCATED
VILLAGE OF BOLINGBROOK
- EXISTING LIGHTING CONTROLLER CABINET "C",
SINGLE DOOR, CONSOLE TYPE, 100A, 240/480V, 1Ø
VILLAGE OF BOLINGBROOK MAINTAINED

100 S. WACKER DR. SUITE 500 CHICAGO, IL 60606 TEL (312) 939-1000 FAX (312) 939-4198	USER NAME - jwr/ps/plp DESIGNED - MAE DRAWN - MAE CHECKED - DAD DATE - 2/1/2012	REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		95TH STREET TEMPORARY LIGHTING PLAN - STAGE 1 (SHEET 3 OF 4)		F.A.J. RTE. 1644 SECTION 01-00181-00-PP COUNTY WILL TOTAL SHEETS 328 SHEET NO. 141 CONTRACT NO. 63647 ILLINOIS FED. AID PROJECT
	PLOT SCALE = 50.0000 / IN. PLOT DATE = 08/16/2012	SCALE: 1"=50' DRAWING NO. 13 OF 30 STA. 348+25 TO STA. 377+75					

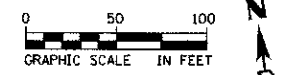
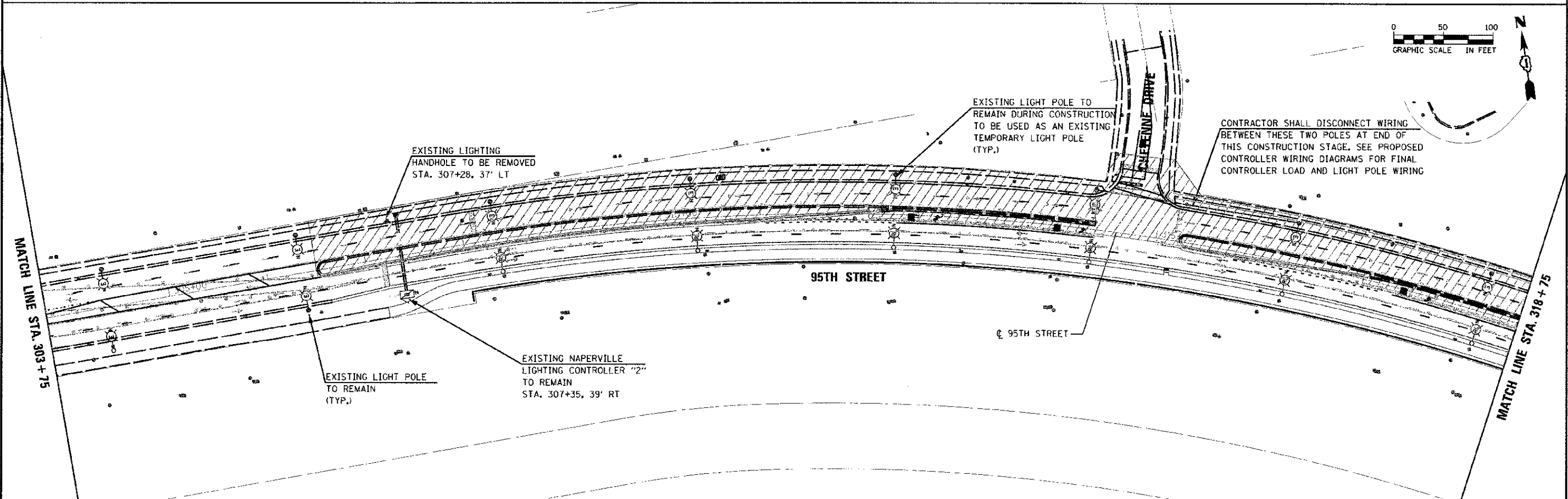
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LEGEND

-  EXISTING LIGHTING UNIT TO REMAIN
40' M.H., 12' M.A., 400W, 240V HPS LUMINAIRE
CITY OF NAPERVILLE
-  EXISTING LIGHTING UNIT TO BE REMOVED AND RELOCATED
40' M.H., 12' M.A., 400W, 240V HPS LUMINAIRE
CITY OF NAPERVILLE
-  EXISTING LIGHTING UNIT TO REMAIN DURING CONSTRUCTION
AND BE USED AS TEMPORARY LIGHT POLE
40' M.H., 12' M.A., 400W, 240V HPS LUMINAIRE
CITY OF NAPERVILLE
-  NEW LOCATION OF EXISTING LIGHT POLES
THAT ARE TO BE REMOVED AND RELOCATED
CITY OF NAPERVILLE
-  EXISTING LIGHTING CONTROLLER CABINET,
SINGLE DOOR, CONSOLE TYPE, 100A, 120/240V, 1Ø
CITY OF NAPERVILLE MAINTAINED

NOTES:

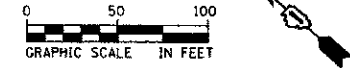
1. FURNISH AND INSTALL A SECOND LIGHTING MAST ARM AND A LUMINAIRE ON THE TEMPORARY TRAFFIC SIGNAL WOOD POLES IN THE LOCATIONS SHOWN. THIS WORK SHALL BE DONE BEFORE THE EXISTING LIGHT POLES ARE DISCONNECTED OR REMOVED.
2. THE CONTRACTOR SHALL MAINTAIN THE EXISTING LIGHTING EQUIPMENT IN A FULLY OPERATIONAL STATE ON THE TRAFFIC SIDE OF THE ROADWAY. THE CONTRACTOR SHALL REWIRE EXISTING LIGHT POLES AND LIGHTING CIRCUITS, AS NECESSARY, TO KEEP THE LIGHT POLES OPERATIONAL DURING CONSTRUCTION. THE CONTRACTOR SHALL PROVIDE AND INSTALL ANY EQUIPMENT, MATERIALS, AND LABOR NECESSARY TO ACCOMPLISH THIS GOAL INCLUDING WOOD POLES, TEMPORARY WIRING (UNDERGROUND AND AERIAL) OF POLES, LUMINAIRES AND LIGHTING CONTROLLERS. THIS WORK IS INCLUDED UNDER PAY ITEM "MAINTAIN EXISTING LIGHTING SYSTEM".
3. ALL LIGHTING RELATED EQUIPMENT, MATERIALS AND LABOR FURNISHED AND INSTALLED BY THE CONTRACTOR DURING CONSTRUCTION TO MAINTAIN THE OPERATION OF THE EXISTING LIGHTING SYSTEM SHALL BE PAID FOR UNDER PAY ITEM "MAINTAIN EXISTING LIGHTING SYSTEM". THIS PAYMENT SHALL INCLUDE ALL MATERIALS AND EQUIPMENT NOT SPECIFICALLY SHOWN ON THE PLANS, BUT ARE NECESSARY TO MAINTAIN THE OPERATION OF THE LIGHTING SYSTEM DURING CONSTRUCTION.
4. FURNISH AND INSTALL AERIAL CABLES AND WOOD POLES TO SUPPORT CABLES IF PERMANENT UNDERGROUND WIRING CANNOT BE INSTALLED IN THIS STAGE, OR EXISTING WIRING CANNOT BE MAINTAINED DURING CONSTRUCTION. NUMBER AND LOCATIONS OF TEMPORARY WOOD POLES AND AERIAL WIRING IS SHOWN AS AN EXAMPLE ONLY, AND ARE NOT LIMITED TO WHAT IS SHOWN ON THESE PLANS. PAYMENT FOR THIS WORK SHALL BE AS DESCRIBED IN NOTE 2 ABOVE.



FILE NAME: \\nasdkk\25187381_0514\work\es\inga\constr\2011\11\11\95th St Lights 01.dwg

URS	100 S. WACKER DR. SUITE 500 CHICAGO IL 60606 TEL (312) 939-1000 FAX (312) 939-4938	USER NAME: jones, push	DESIGNED - MAE	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	95TH STREET TEMPORARY LIGHTING PLAN - STAGE 2 (SHEET 1 OF 4)	F.A.J. RTE. 1644	SECTION 01-00181-00-FP	COUNTY WILL	TOTAL SHEETS 328	SHEET NO. 143
	PLT SCALE: 50.0000 1/2 IN.	CHECKED - DAD	REVISI	REVISI		SCALE: 1"=50'	DRAWING NO. 15 OF 30	STA. 291+70.56 TO STA. 318+75	CONTRACT NO. 63647		ILLINOIS FED. AID PROJECT
	DATE: 08/05/2011	REVISI	REVISI								

SPECIAL NOTE:
 NEW LIGHT POLES AND CONTROLLERS PROPOSED IN THE NEW SECTION OF 95TH STREET DOES NOT NEED TO BE OPERATIONAL UNTIL THE ROADWAY IS OPEN TO TRAFFIC.



MATCH LINE STA. 348+25

MATCH LINE STA. 362+75

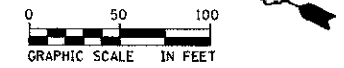
95TH STREET (PROPOSED EXTENSION)

PROPOSED LIGHTING UNIT TO BE INSTALLED AT THE END OF CONSTRUCTION SEE SPECIAL NOTE (TYP.)

CL 95TH STREET (PROPOSED EXTENSION)

NOTES:

1. THE CONTRACTOR SHALL MAINTAIN THE EXISTING LIGHTING EQUIPMENT FULLY OPERATIONAL ON THE TRAFFIC SIDE OF THE ROADWAY. THE CONTRACTOR SHALL REWIRE EXISTING LIGHTING CIRCUITS, AS NECESSARY, TO KEEP THE LIGHT POLES OPERATIONAL DURING CONSTRUCTION. THE CONTRACTOR SHALL PROVIDE ANY EQUIPMENT, MATERIALS, AND LABOR NECESSARY TO ACCOMPLISH THIS GOAL INCLUDING TEMPORARY WIRING (UNDERGROUND OR AERIAL) OF POLES, LUMINAIRES AND LIGHTING CONTROLLERS.
2. ALL TEMPORARY LIGHTING RELATED EQUIPMENT, MATERIALS AND LABOR FURNISHED AND INSTALLED BY THE CONTRACTOR DURING CONSTRUCTION TO MAINTAIN THE OPERATION OF THE EXISTING LIGHTING SYSTEM SHALL BE PAID FOR UNDER PAY ITEM "MAINTAIN EXISTING LIGHTING SYSTEM".



MATCH LINE STA. 362+75

MATCH LINE STA. 377+75

95TH STREET (PROPOSED EXTENSION)

CL 95TH STREET (PROPOSED EXTENSION)

LEGEND

- EXISTING LIGHTING UNIT TO REMAIN
40' M.H., 12' M.A., 400W, 240V HPS LUMINAIRE
CITY OF NAPERVILLE
- EXISTING LIGHTING UNIT TO BE REMOVED AND RELOCATED
40' M.H., 12' M.A., 400W, 240V HPS LUMINAIRE
CITY OF NAPERVILLE
- EXISTING LIGHTING UNIT TO REMAIN DURING CONSTRUCTION AND BE USED AS TEMPORARY LIGHT POLE
40' M.H., 12' M.A., 400W, 240V HPS LUMINAIRE
CITY OF NAPERVILLE
- NEW LOCATION OF EXISTING LIGHT POLES THAT ARE TO BE REMOVED AND RELOCATED
CITY OF NAPERVILLE
- EXISTING LIGHTING CONTROLLER CABINET "XX",
SINGLE DOOR, CONSOLE TYPE, 100A, 120/240V, 1Ø
CITY OF NAPERVILLE MAINTAINED

NEW LOCATION OF RELOCATED LIGHT POLE

EXISTING POLE TO BE REMOVED AND RELOCATED (TYP.)

NEW LOCATION OF RELOCATED LIGHT POLE. USE EXISTING MEDIAN POLE AND ONE MAST ARM AND A LUMINAIRE FROM EXISTING MEDIAN POLE. (TYP.)

ALAN DEATHRAG DRIVE

REUSE EXISTING WIRING, PROVIDE NEW AERIAL CABLES, OR INSTALL PERMANENT UNDERGROUND WIRING TO KEEP LIGHTS OPERATIONAL DURING CONSTRUCTION (TYP.)

EXISTING LIGHTING CONTROLLER "C" TO REMAIN FULLY OPERATIONAL DURING CONSTRUCTION AND CAN BE RELOCATED IN ANY STAGE BEFORE END OF CONSTRUCTION VILLAGE OF BOLINGBROOK MAINTAINED

FURNISH AND INSTALL NEW POLE TO REPLACE EXISTING TWIN FIXTURE POLE IN THE MEDIAN INSTALL ONE ARM AND A LUMINAIRE FROM THE EXISTING TWIN ARM MEDIAN POLE. (TYP.)



100 S. WACKER DR.
 SUITE 500
 CHICAGO, IL 60606
 TEL (312) 939-1000
 FAX (312) 939-4999

USER NAME: james.p.ush
 PLOT SCALE: 1/8"=1'-0"
 PLOT DATE: 12/16/2012

DESIGNED - MAE
 DRAWN - MAE
 CHECKED - DAD
 DATE - 2/1/2012

REVISED -
 REVISED -
 REVISED -
 REVISED -

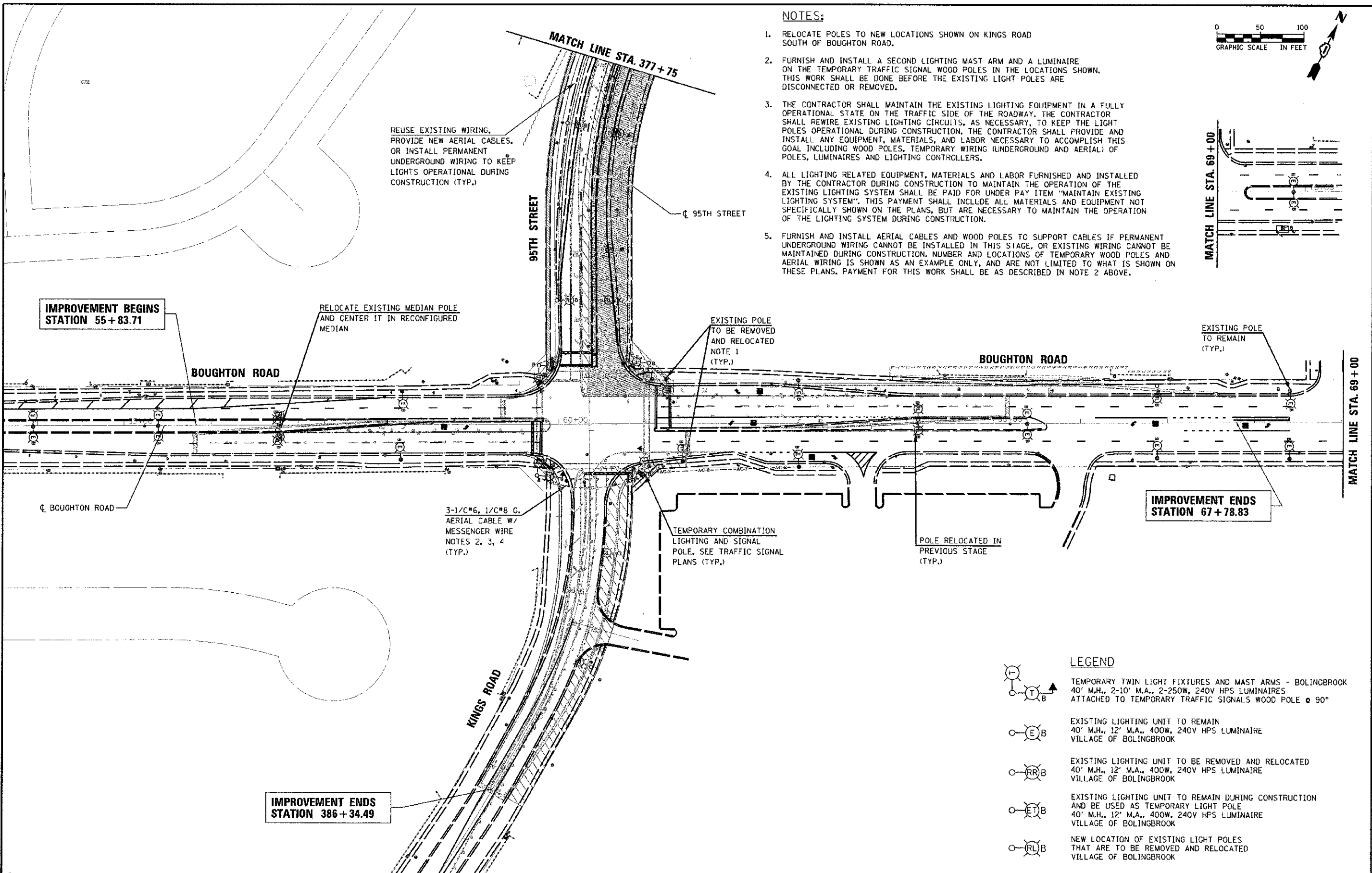
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

95TH STREET
 TEMPORARY LIGHTING PLAN - STAGE 2 (SHEET 3 OF 4)

SCALE: 1"=50' DRAWING NO. 17 OF 30 STA. 348+25 TO STA. 377+75

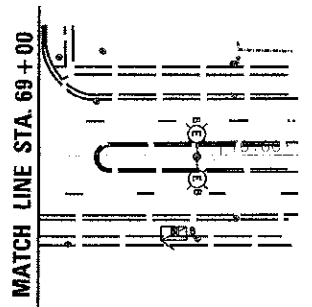
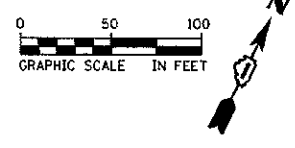
F.A.J. RTE. NO. 1644	SECTION 01-00181-00-FP	COUNTY WILL	TOTAL SHEETS 328	SHEET NO. 145
CONTRACT NO. 63647			ILLINOIS FED. AID PROJECT	

FILE NAME: g:\work\1125167381\95th\95thstage2\temp-lighting-plan-stage-2.dwg



NOTES:

1. RELOCATE POLES TO NEW LOCATIONS SHOWN ON KINGS ROAD SOUTH OF BOUGHTON ROAD.
2. FURNISH AND INSTALL A SECOND LIGHTING MAST ARM AND A LUMINAIRE ON THE TEMPORARY TRAFFIC SIGNAL WOOD POLES IN THE LOCATIONS SHOWN. THIS WORK SHALL BE DONE BEFORE THE EXISTING LIGHT POLES ARE DISCONNECTED OR REMOVED.
3. THE CONTRACTOR SHALL MAINTAIN THE EXISTING LIGHTING EQUIPMENT IN A FULLY OPERATIONAL STATE ON THE TRAFFIC SIDE OF THE ROADWAY. THE CONTRACTOR SHALL REWIRE EXISTING LIGHTING CIRCUITS, AS NECESSARY, TO KEEP THE LIGHT POLES OPERATIONAL DURING CONSTRUCTION. THE CONTRACTOR SHALL PROVIDE AND INSTALL ANY EQUIPMENT, MATERIALS, AND LABOR NECESSARY TO ACCOMPLISH THIS GOAL INCLUDING WOOD POLES, TEMPORARY WIRING (UNDERGROUND AND AERIAL) OF POLES, LUMINAIRES AND LIGHTING CONTROLLERS.
4. ALL LIGHTING RELATED EQUIPMENT, MATERIALS AND LABOR FURNISHED AND INSTALLED BY THE CONTRACTOR DURING CONSTRUCTION TO MAINTAIN THE OPERATION OF THE EXISTING LIGHTING SYSTEM SHALL BE PAID FOR UNDER PAY ITEM "MAINTAIN EXISTING LIGHTING SYSTEM". THIS PAYMENT SHALL INCLUDE ALL MATERIALS AND EQUIPMENT NOT SPECIFICALLY SHOWN ON THE PLANS, BUT ARE NECESSARY TO MAINTAIN THE OPERATION OF THE LIGHTING SYSTEM DURING CONSTRUCTION.
5. FURNISH AND INSTALL AERIAL CABLES AND WOOD POLES TO SUPPORT CABLES IF PERMANENT UNDERGROUND WIRING CANNOT BE INSTALLED IN THIS STAGE, OR EXISTING WIRING CANNOT BE MAINTAINED DURING CONSTRUCTION. NUMBER AND LOCATIONS OF TEMPORARY WOOD POLES AND AERIAL WIRING IS SHOWN AS AN EXAMPLE ONLY, AND ARE NOT LIMITED TO WHAT IS SHOWN ON THESE PLANS. PAYMENT FOR THIS WORK SHALL BE AS DESCRIBED IN NOTE 2 ABOVE.



IMPROVEMENT BEGINS STATION 55+83.71

RELOCATE EXISTING MEDIAN POLE AND CENTER IT IN RECONFIGURED MEDIAN

EXISTING POLE TO BE REMOVED AND RELOCATED NOTE 1 (TYP.)

EXISTING POLE TO REMAIN (TYP.)

IMPROVEMENT ENDS STATION 67+78.83

3-1/2\"/>

TEMPORARY COMBINATION LIGHTING AND SIGNAL POLE. SEE TRAFFIC SIGNAL PLANS (TYP.)

POLE RELOCATED IN PREVIOUS STAGE (TYP.)

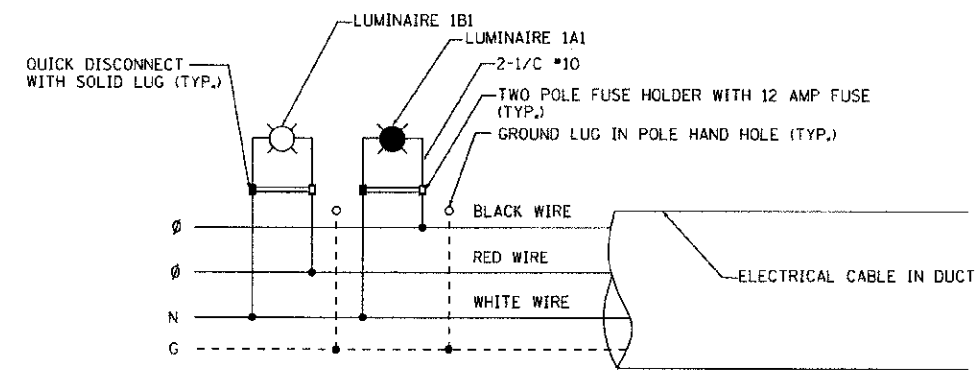
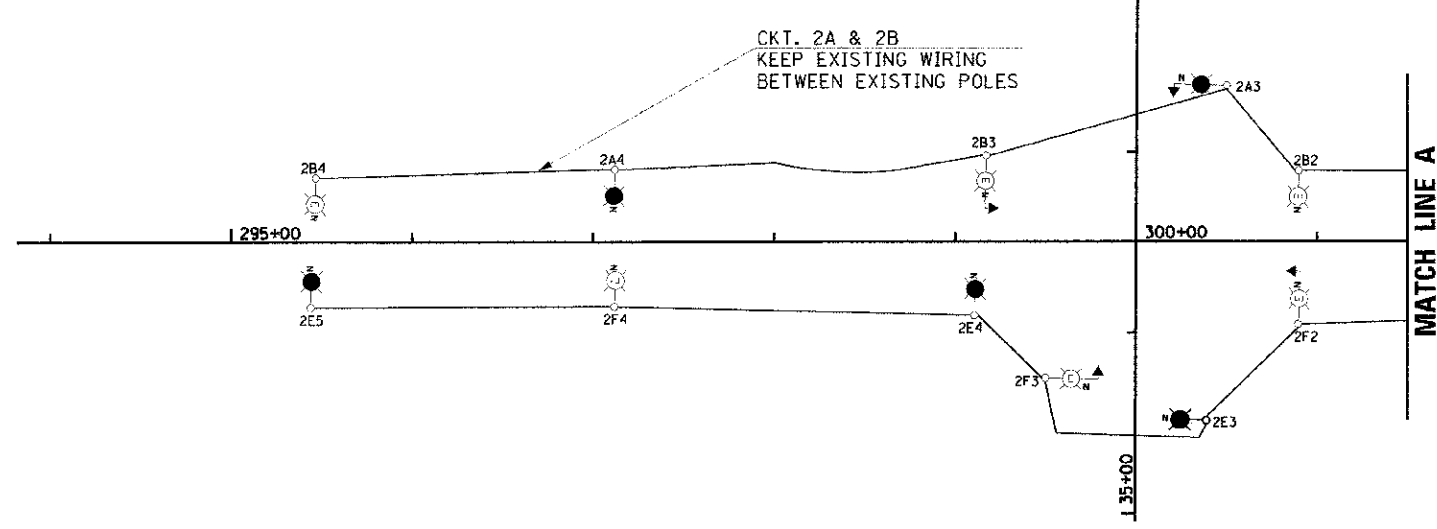
IMPROVEMENT ENDS STATION 386+34.49

LEGEND

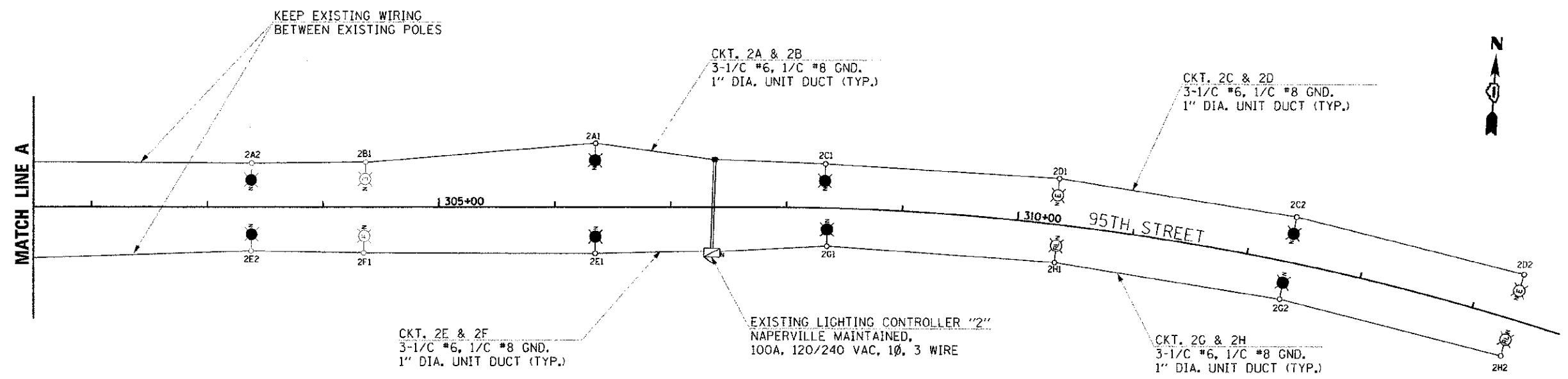
- TEMPORARY TWIN LIGHT FIXTURES AND MAST ARMS - BOLINGBROOK 40' M.H., 2-10' M.A., 2-250W, 240V HPS LUMINAIRES ATTACHED TO TEMPORARY TRAFFIC SIGNALS WOOD POLE @ 90°
- EXISTING LIGHTING UNIT TO REMAIN 40' M.H., 12' M.A., 400W, 240V HPS LUMINAIRE VILLAGE OF BOLINGBROOK
- EXISTING LIGHTING UNIT TO BE REMOVED AND RELOCATED 40' M.H., 12' M.A., 400W, 240V HPS LUMINAIRE VILLAGE OF BOLINGBROOK
- EXISTING LIGHTING UNIT TO REMAIN DURING CONSTRUCTION AND BE USED AS TEMPORARY LIGHT POLE 40' M.H., 12' M.A., 400W, 240V HPS LUMINAIRE VILLAGE OF BOLINGBROOK
- NEW LOCATION OF EXISTING LIGHT POLES THAT ARE TO BE REMOVED AND RELOCATED VILLAGE OF BOLINGBROOK

	100 S. WACKER DR. SUITE 500 CHICAGO, IL 60606 TEL (312) 939-1000 FAX (312) 939-4198	USER NAME: jensie.boyer P.L.D. SCALE: 1/8"=1'-0" P.L.D. DATE: 12/16/2012	DESIGNED: MAE DRAWN: MAE CHECKED: DAD DATE: 2/1/2012	REVISED: - REVISED: - REVISED: - REVISED: -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	95TH STREET TEMPORARY LIGHTING PLAN - STAGE 2 (SHEET 4 OF 4)	F.A.U. RTE.: 1644 SECTION: 01-00181-00-FP	COUNTY: WILL TOTAL SHEETS: 328 SHEET NO.: 146	CONTRACT NO. 63647 ILLINOIS FED. AID PROJECT
	SCALE: 1"=50' DRAWING NO. 18 OF 30 STA. 377+75 TO STA. 386+34.49								

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DETAIL "A"
LUMINAIRE WIRING CONNECTION (TYP.)



LOAD TABLE
PROPOSED LIGHTING CONTROLLER "2"
(@ 120 VOLTS)

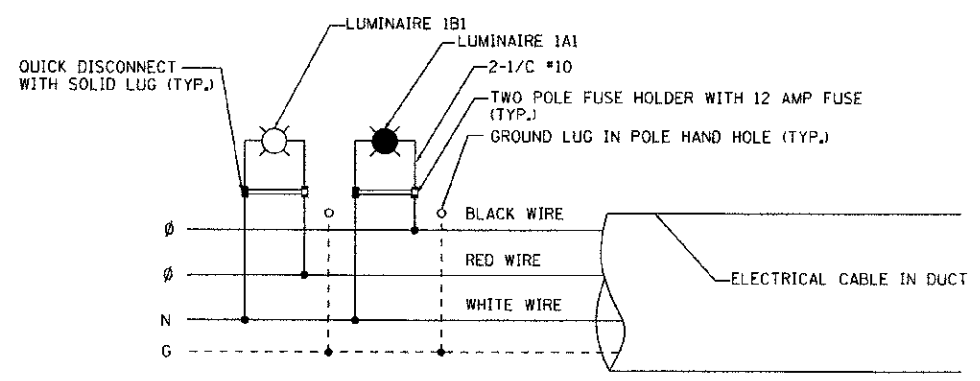
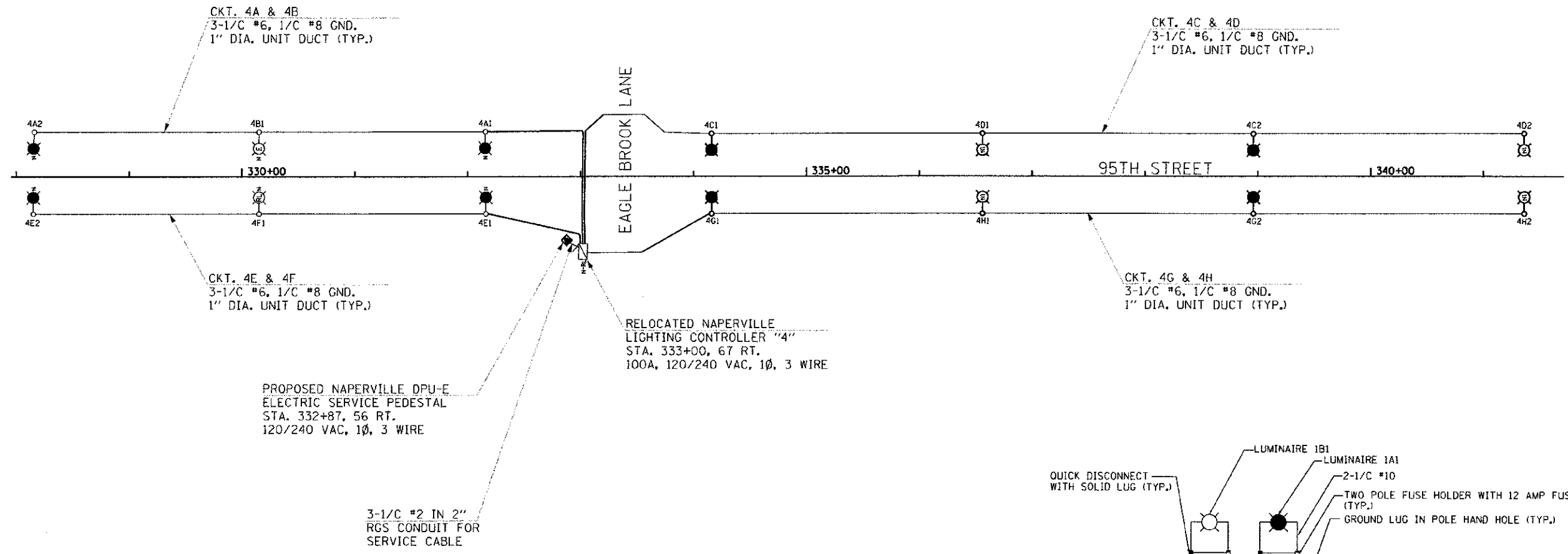
CIRCUIT	BLACK PHASE		RED PHASE		
	AMPS	WATTS	AMPS	WATTS	
A	12.4	1488	B	12.4	1488
C	6.2	744	D	6.2	744
E	15.5	1860	F	12.4	1488
G	6.2	744	H	6.2	744
TOTAL	40.3	2976	TOTAL	37.2	2232

TOTAL LOAD ON CONTROLLER (@120 V): 77.5 AMP

NOTES:

- ELECTRIC SERVICES FOR ALL NAPERVILLE MAINTAINED LIGHTING CONTROLLERS SHALL BE FED FROM NAPERVILLE DEPARTMENT OF PUBLIC UTILITIES-ELECTRICAL (NDPU-E) DESIGNATED SERVICE TRANSFORMERS OR PEDESTALS.
- ALL ELECTRIC SERVICE AND BRANCH CIRCUIT CONDUCTORS SHALL BE XLP TYPE USE-2.
- KEEP EXISTING WIRING BETWEEN EXISTING POLES THAT ARE NOT BEING REMOVED OR RELOCATED. MAKE WIRING ADJUSTMENTS TO KEEP ALL LIGHT POLES OPERATIONAL.

- LEGEND
- EXISTING LIGHTING CONTROLLER, SINGLE DOOR, 100A, 120/240V, 1Ø CITY OF NAPERVILLE MAINTAINED
 - PROPOSED/RELOCATED/EXISTING LIGHTING UNIT 40' M.H., 12' M.A., 310W HPS TYPE MC-III, 120V LUMINAIRE (BLACK PHASE)
 - PROPOSED/RELOCATED/EXISTING LIGHTING UNIT 40' M.H., 12' M.A., 310W HPS TYPE MC-III, 120V LUMINAIRE (RED PHASE)
 - PROPOSED ELECTRIC SERVICE PEDESTAL



DETAIL "A"
LUMINAIRE WIRING CONNECTION (TYP.)

LOAD TABLE PROPOSED LIGHTING CONTROLLER "4" (@ 120 VOLTS)					
CIRCUIT	BLACK PHASE		RED PHASE		CIRCUIT
	AMPS	WATTS	AMPS	WATTS	
A	6.2	744	B	3.1	372
C	6.2	744	D	6.2	744
E	6.2	744	F	3.1	372
G	6.2	744	H	6.2	744
TOTAL	24.8	2976	TOTAL	18.6	2232

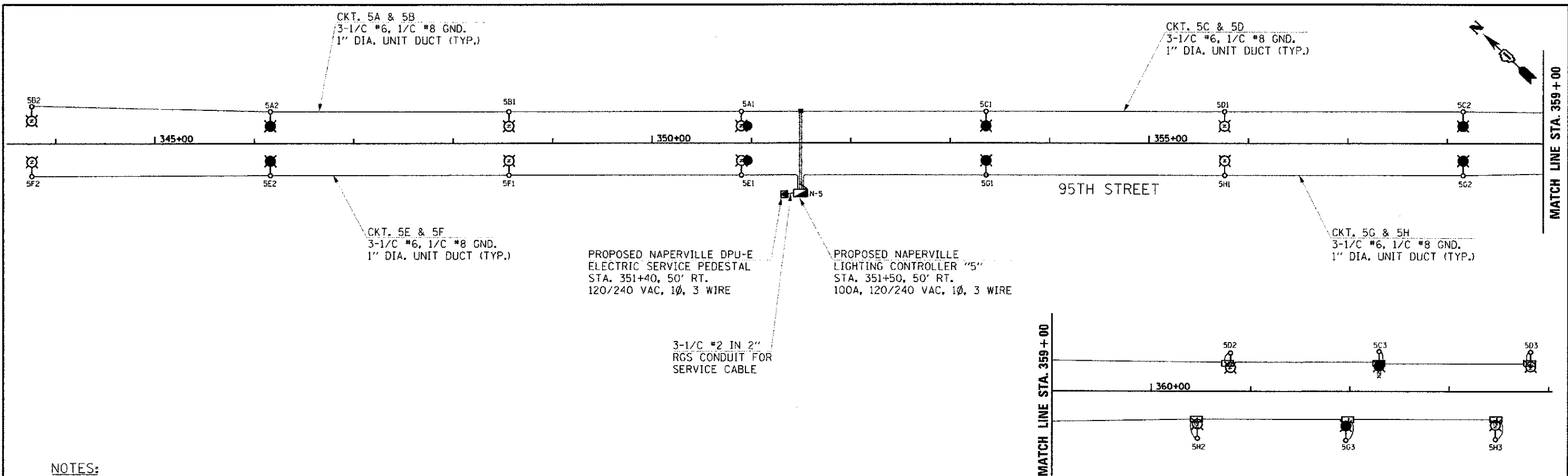
TOTAL LOAD ON CONTROLLER (@120 V): **43.4 AMP**

- LEGEND**
- N-RL RELOCATED LIGHTING CONTROLLER, SINGLE DOOR, 100A, 120/240V, 1Ø CITY OF NAPERVILLE MAINTAINED
 - PROPOSED/RELOCATED/EXISTING LIGHTING UNIT 40' M.H., 12' M.A., 310W HPS TYPE MC-III, 120V LUMINAIRE (BLACK PHASE)
 - PROPOSED/RELOCATED/EXISTING LIGHTING UNIT 40' M.H., 12' M.A., 310W HPS TYPE MC-III, 120V LUMINAIRE (RED PHASE)
 - PROPOSED ELECTRIC SERVICE PEDESTAL

NOTES:

1. ELECTRIC SERVICES FOR ALL NAPERVILLE MAINTAINED LIGHTING CONTROLLERS SHALL BE FED FROM NAPERVILLE DEPARTMENT OF PUBLIC UTILITIES-ELECTRICAL (NDPU-E) DESIGNATED SERVICE TRANSFORMERS OR PEDESTALS.
2. CONTRACTOR SHALL FURNISH AND INSTALL CONDUIT, CABLE AND TRENCHING (OR PUSHING) FOR SERVICE CONNECTION TO UTILITY TRANSFORMER OR PEDESTAL. THE EXACT LOCATION OF THE SERVICE TRANSFORMER SHALL BE DETERMINED BY THE UTILITY. FINAL CONNECTION TO THE TRANSFORMER MUST BE MADE BY THE UTILITY COMPANY.
3. ELECTRIC SERVICES TO THE NAPERVILLE MAINTAINED LIGHTING CABINETS ARE NOT METERED.
4. ALL ELECTRIC SERVICE AND BRANCH CIRCUIT CONDUCTORS SHALL BE XLP TYPE USE-2.

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NOTES:

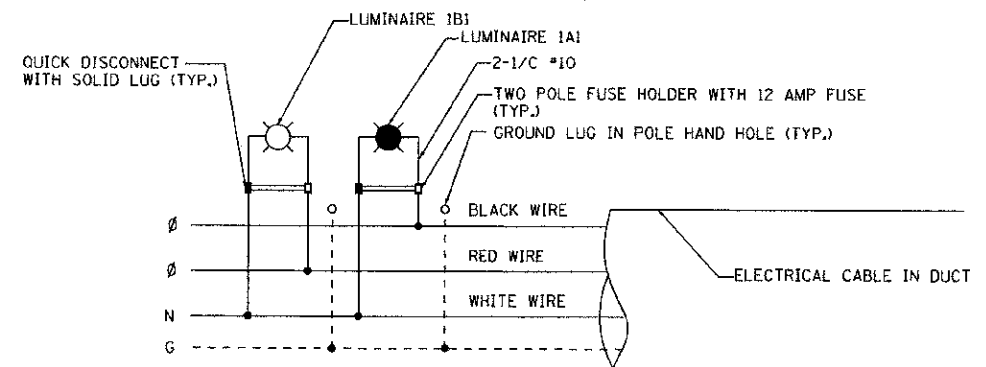
- ELECTRIC SERVICES FOR ALL NAPERVILLE MAINTAINED LIGHTING CONTROLLERS SHALL BE FED FROM NAPERVILLE DEPARTMENT OF PUBLIC UTILITIES-ELECTRICAL (NDPU-E) DESIGNATED SERVICE TRANSFORMERS OR PEDESTALS.
- CONTRACTOR SHALL FURNISH AND INSTALL CONDUIT, CABLE AND TRENCHING (OR PUSHING) FOR SERVICE CONNECTION TO UTILITY TRANSFORMER OR PEDESTAL. THE EXACT LOCATION OF THE SERVICE TRANSFORMER SHALL BE DETERMINED BY THE UTILITY. FINAL CONNECTION TO THE TRANSFORMER MUST BE MADE BY THE UTILITY COMPANY.
- ELECTRIC SERVICES TO THE NAPERVILLE MAINTAINED LIGHTING CABINETS ARE NOT METERED.
- ALL ELECTRIC SERVICE AND BRANCH CIRCUIT CONDUCTORS SHALL BE XLP TYPE USE-2.

LOAD TABLE PROPOSED LIGHTING CONTROLLER "5" (@ 120 VOLTS)					
CIRCUIT	BLACK PHASE		RED PHASE		
	AMPS	WATTS	CIRCUIT	AMPS	
A	6.2	744	B	6.2	744
C	9.3	744	D	9.3	744
E	6.2	1116	F	6.2	1116
G	9.3	1116	H	9.3	1116
TOTAL	31.0	3720	TOTAL	31.0	3720

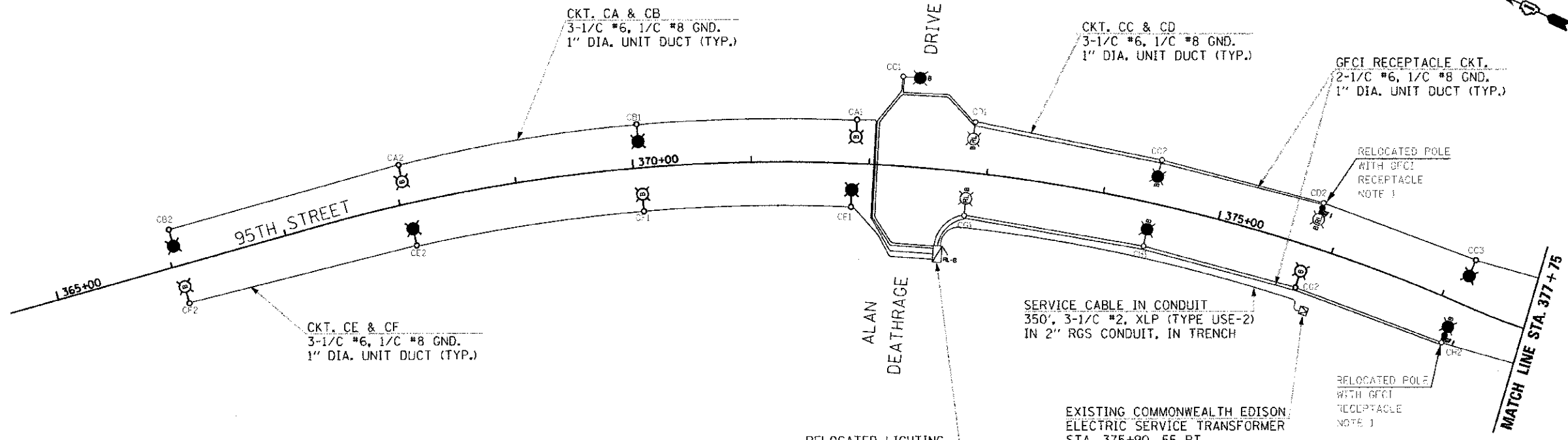
TOTAL LOAD ON CONTROLLER (@120 V): 62.0 AMP

LEGEND

- PROPOSED LIGHTING CONTROLLER, SINGLE DOOR, 100A, 120/240V, 1Ø CITY OF NAPERVILLE MAINTAINED
- PROPOSED LIGHTING UNIT
40' M.H., 12' M.A., 310W HPS, TYPE MC-III, 120V LUMINAIRE (BLACK PHASE)
- PROPOSED LIGHTING UNIT
40' M.H., 12' M.A., 310W HPS, TYPE MC-III, 120V LUMINAIRE (RED PHASE)
- PROPOSED BRIDGE MOUNTED LIGHTING UNIT
40' M.H., 12' M.A., 310W HPS, TYPE MC-III, 120V LUMINAIRE (BLACK PHASE)
- PROPOSED BRIDGE MOUNTED LIGHTING UNIT
40' M.H., 12' M.A., 310W HPS, TYPE MC-III, 120V LUMINAIRE (RED PHASE)
- PROPOSED ELECTRIC SERVICE PEDESTAL



DETAIL "A"
LUMINAIRE WIRING CONNECTION (TYP.)



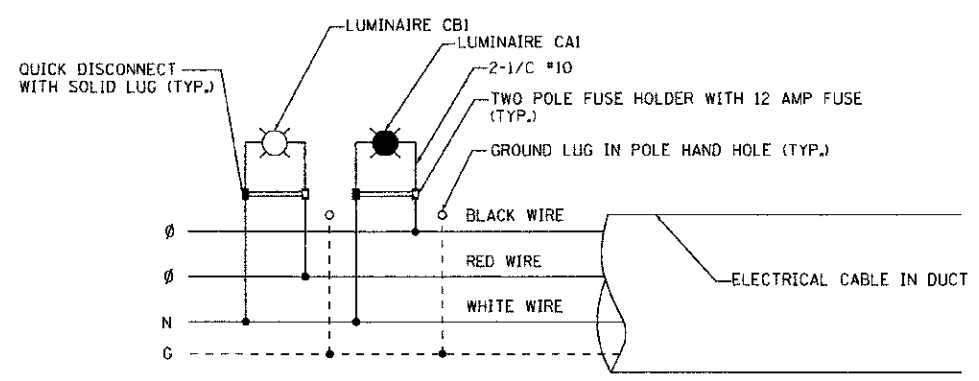
- LEGEND**
- B-RL RELOCATED LIGHTING CONTROLLER "C", SINGLE DOOR, 100A, 240/480V, 1Ø VILLAGE OF BOLINGBROOK MAINTAINED
 - B PROPOSED/RELOCATED/EXISTING LIGHTING UNIT 35' M.H., 10' M.A., 250W HPS TYPE MC-III, 240V LUMINAIRE (BLACK PHASE)
 - R PROPOSED/RELOCATED/EXISTING LIGHTING UNIT 35' M.H., 10' M.A., 250W HPS TYPE MC-III, 240V LUMINAIRE (RED PHASE)
 - GFCI EXISTING POLE MOUNTED GFCI RECEPTACLE TO BE RELOCATED WITH POLE TO NEW LOCATION SHOWN, 120V
 - EXISTING ELECTRIC SERVICE TRANSFORMER/PEDESTAL

LOAD TABLE
PROPOSED LIGHTING CONTROLLER "C"
(@ 240 VOLTS)

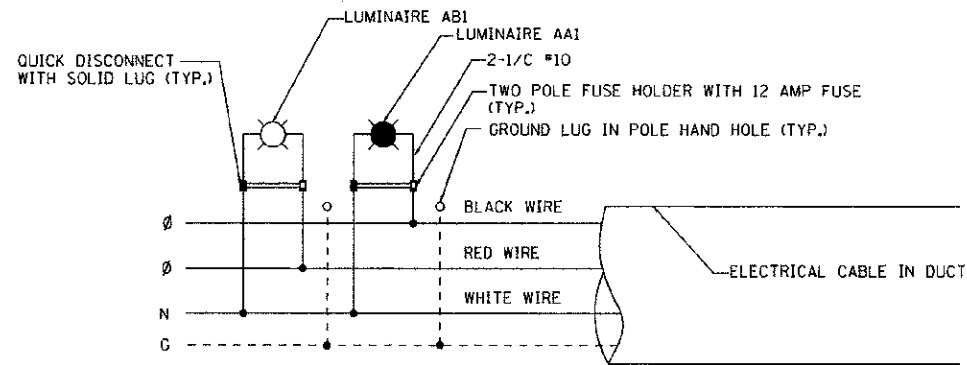
CIRCUIT	BLACK PHASE		CIRCUIT	RED PHASE	
	AMPS	WATTS		AMPS	WATTS
A	2.4	576	B	2.4	576
C	4.8	1152	D	3.6	864
E	2.4	576	F	2.4	576
G	3.6	864	H	3.6	864
TOTAL	13.2	3168	TOTAL	12.0	2880

TOTAL LOAD ON CONTROLLER (@240 V): 25.2 AMP

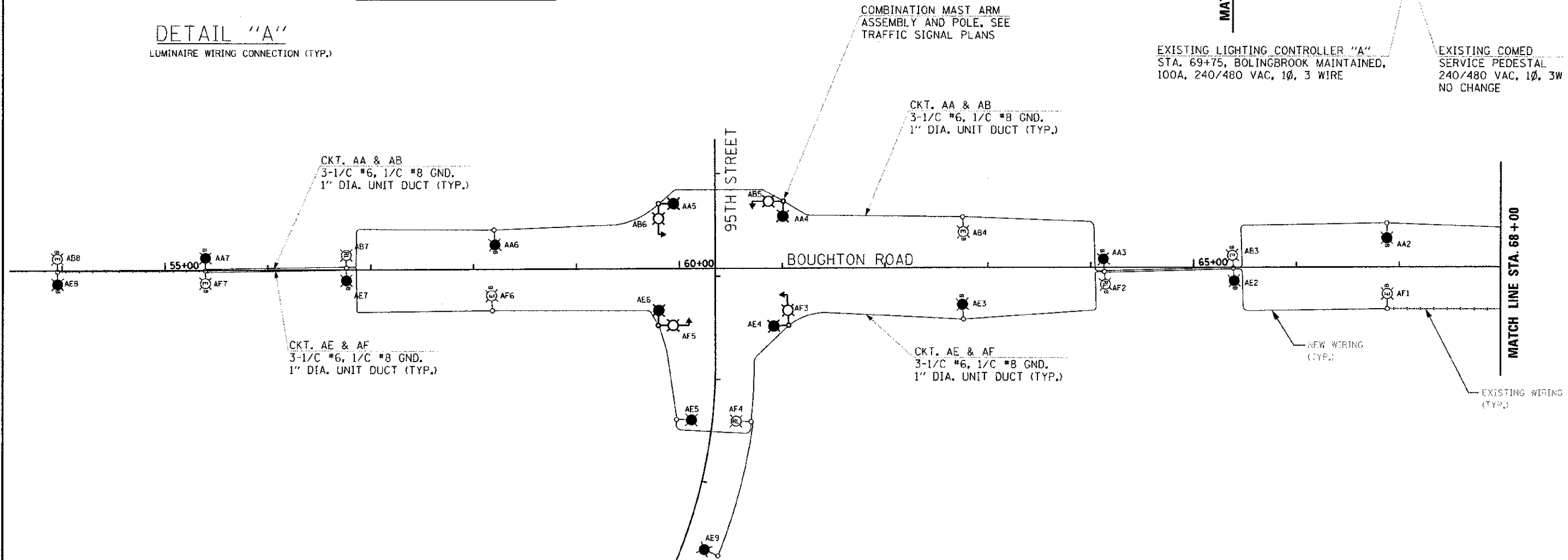
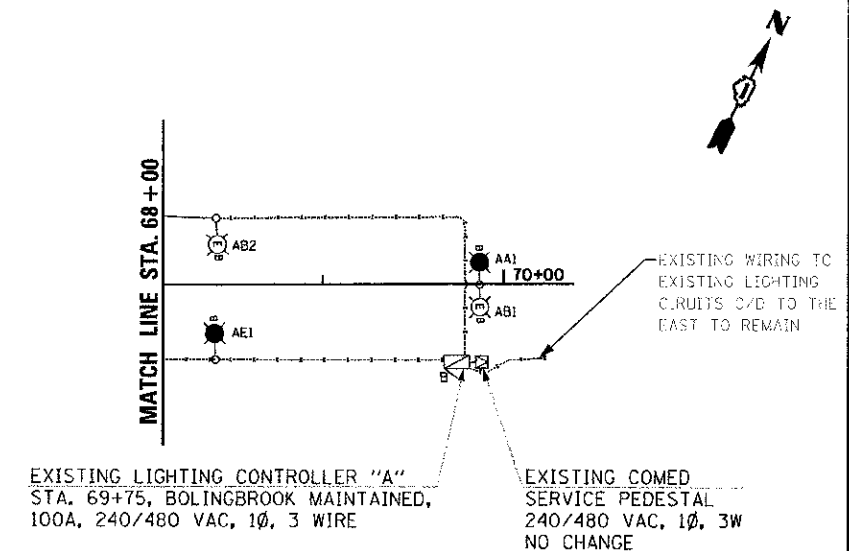
- NOTES:**
- CONTRACTOR SHALL FURNISH AND INSTALL WIRING TO EXISTING 120V, POLE MOUNTED FESTOON RECEPTACLES. TWO RECEPTACLES ARE INSTALLED ON EXISTING MEDIAN POLES TO BE RELOCATED, ONE RECEPTACLE PER POLE. WIRE RECEPTACLES USING 2-1/2" #6, 1/2" #8 G CONDUCTORS FROM EXISTING 120V AUXILIARY CIRCUIT BREAKERS IN LIGHTING CABINET.
 - CONTRACTOR SHALL MODIFY EXISTING LIGHTING CONTROLLER BY ADDING ADDITIONAL CIRCUIT BREAKERS IN THE EXISTING ELECTRICAL PANEL INSIDE THE CABINET. SPACES ARE AVAILABLE TO ADD 6 ADDITIONAL CIRCUIT BREAKERS. NEW CIRCUIT BREAKERS SHALL MATCH EXISTING BREAKER FRAME SIZE, INTERRUPTING RATING, AND AMPERE RATING.
 - ELECTRIC SERVICES FOR BOLINGBROOK MAINTAINED LIGHTING CONTROLLERS SHALL BE FED FROM COMED DESIGNATED SERVICE TRANSFORMERS OR PEDESTALS.
 - CONTRACTOR SHALL FURNISH AND INSTALL CONDUIT, CABLE AND TRENCHING (OR PUSHING) FOR SERVICE CONNECTION TO UTILITY TRANSFORMER. FINAL CONNECTION TO THE TRANSFORMER SHALL BE MADE BY THE UTILITY COMPANY.
 - ELECTRIC SERVICES TO THE BOLINGBROOK MAINTAINED LIGHTING CABINETS ARE METERED.
 - ALL ELECTRIC SERVICE AND BRANCH CIRCUIT CONDUCTORS SHALL BE XLP (TYPE USE-2).



DETAIL "A"
LUMINAIRE WIRING CONNECTION (TYP.)



DETAIL "A"
LUMINAIRE WIRING CONNECTION (TYP.)



LOAD TABLE PROPOSED LIGHTING CONTROLLER "A" (@ 240 VOLTS)					
CIRCUIT	BLACK PHASE		CIRCUIT	RED PHASE	
	AMPS	WATTS		AMPS	WATTS
A	8.4	2016	B	9.6	2304
C	9.6	2304	D	9.6	2304
E	10.8	2592	F	8.4	2016
TOTAL	28.8	6912	TOTAL	27.6	6624

TOTAL LOAD ON CONTROLLER (@240 V): **56.4 AMP**

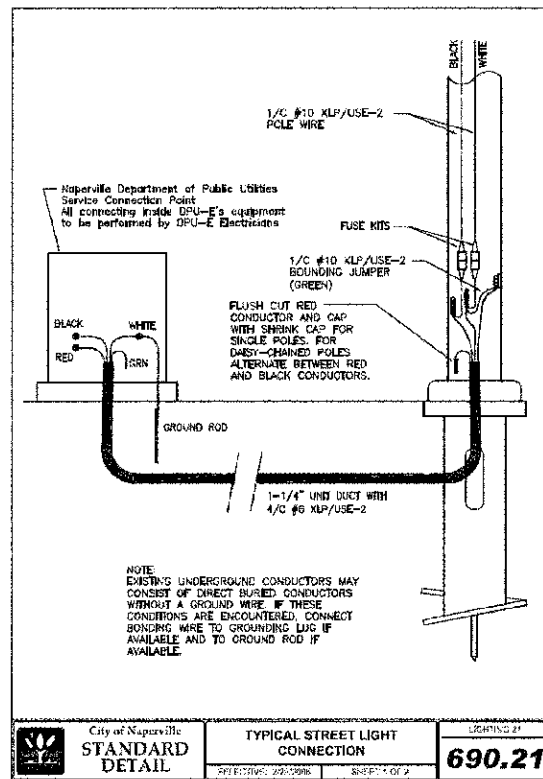
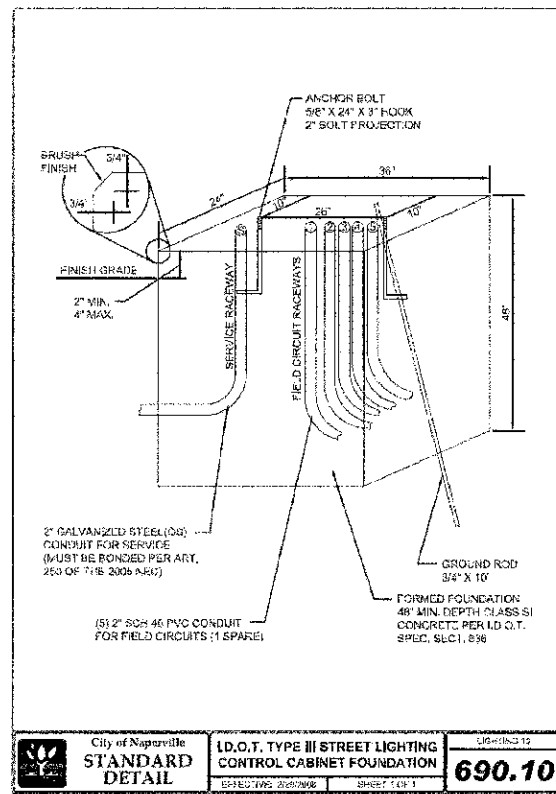
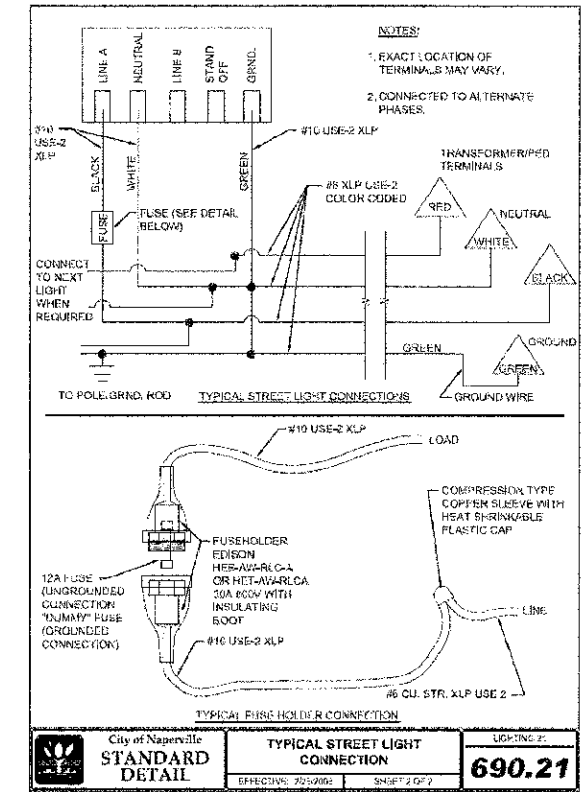
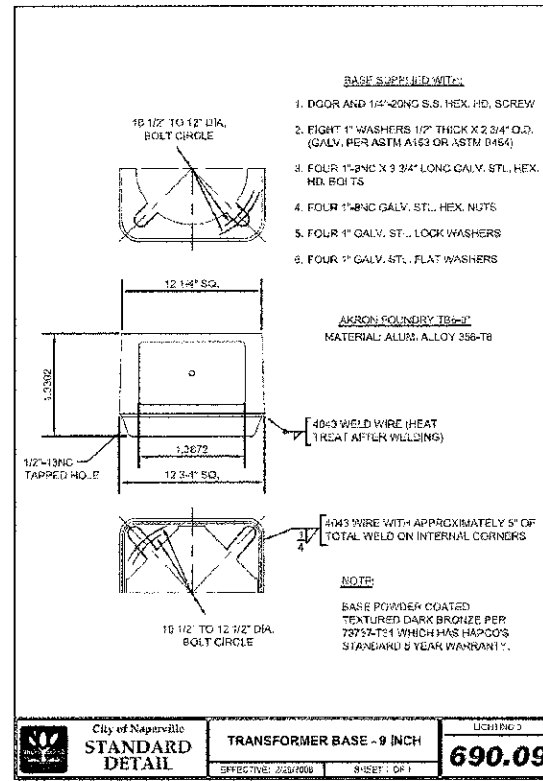
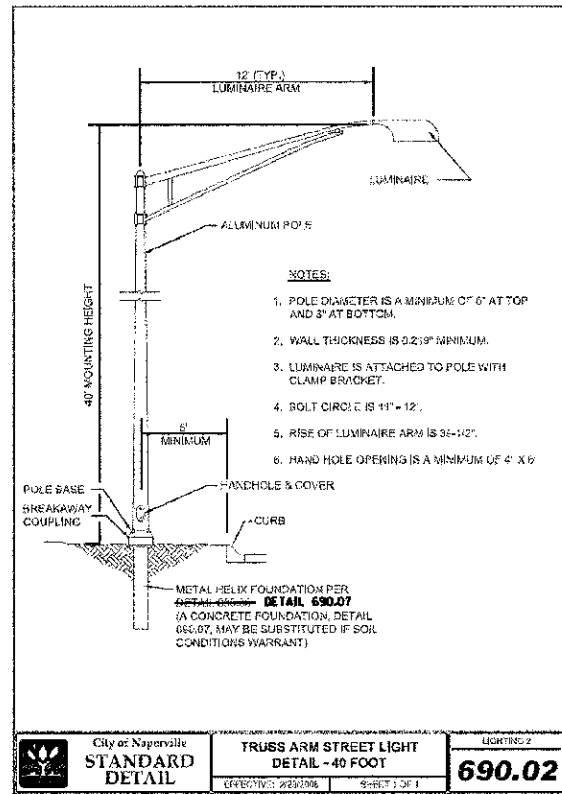
NOTE:
CKTS. C/D ARE FOR EXISTING LIGHT POLES TO THE EAST SHOWN FOR INFORMATION ONLY

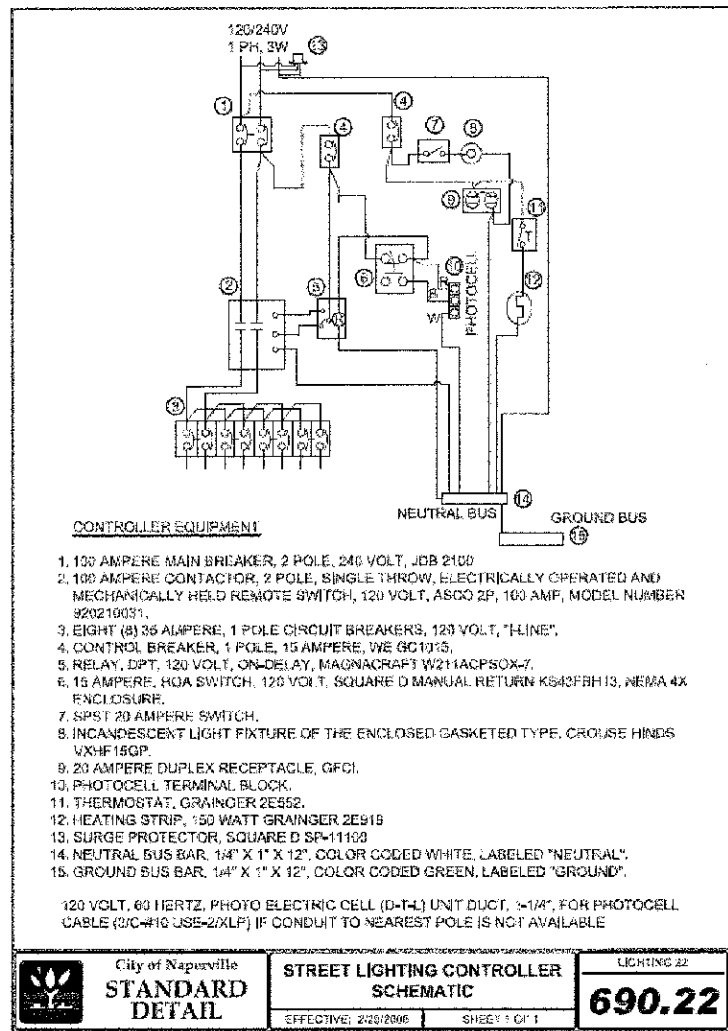
NOTES:

- ELECTRIC SERVICES FOR BOLINGBROOK MAINTAINED LIGHTING CONTROLLERS SHALL BE FED FROM COMED DESIGNATED SERVICE TRANSFORMERS OR PEDESTALS.
- ALL ELECTRIC SERVICE AND BRANCH CIRCUIT CONDUCTORS SHALL BE XLP TYPE USE-2.

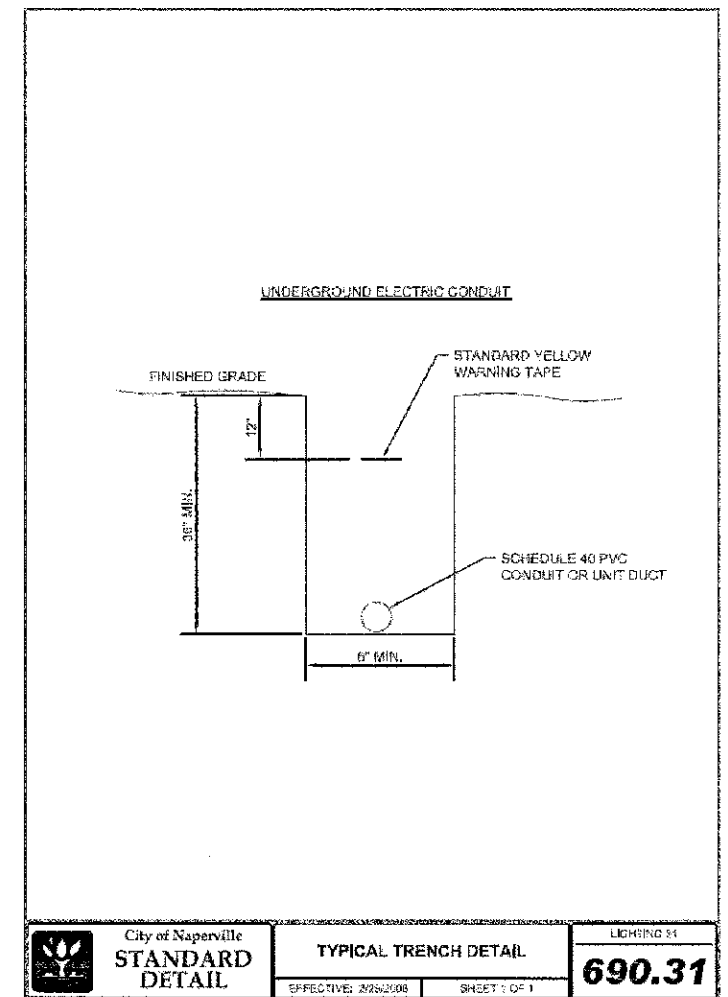
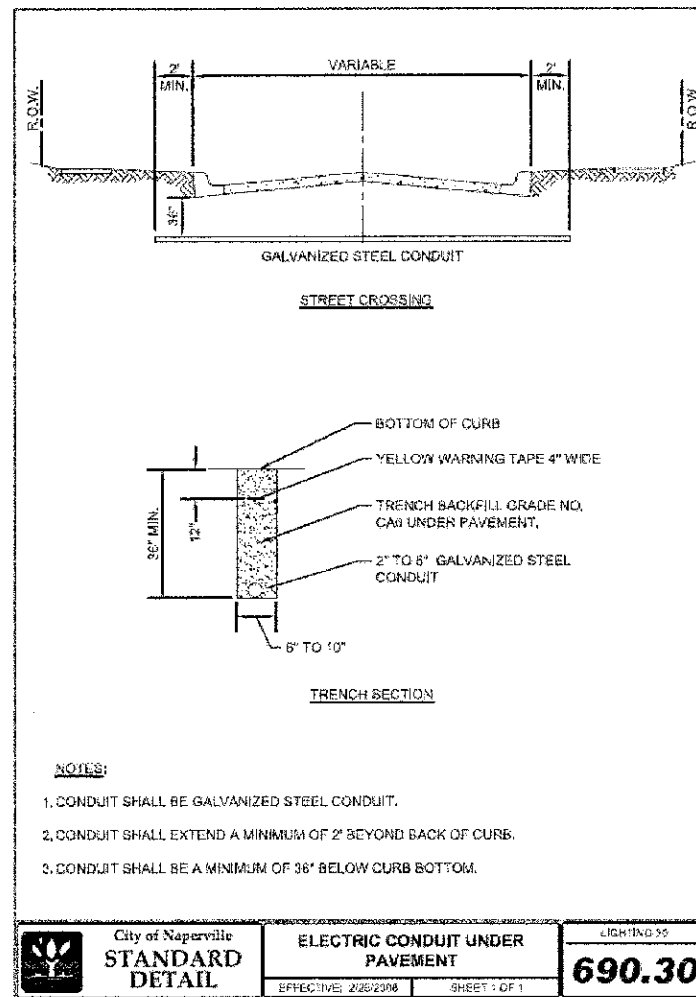
LEGEND

- EXISTING LIGHTING CONTROLLER, SINGLE DOOR, 100A, 240/480V, 1Ø VILLAGE OF BOLINGBROOK MAINTAINED
- PROPOSED/RELOCATED/EXISTING LIGHTING UNIT 35' M.H., 10' M.A., 250W HPS TYPE MC-III, 240V LUMINAIRE (BLACK PHASE)
- PROPOSED/RELOCATED/EXISTING LIGHTING UNIT 35' M.H., 10' M.A., 250W HPS TYPE MC-III, 240V LUMINAIRE (RED PHASE)
- PROPOSED COMBINATION LIGHTING UNIT 45' M.H., TWIN 10' M.A., 2-250W HPS TYPE MC-III, 240V LUMINAIRES (BLACK PHASE AND RED PHASE)
- EXISTING ELECTRIC SERVICE TRANSFORMER/PEDESTAL





ALL ITEMS AS SHOWN OR APPROVED EQUAL.



100 S.WACKER DR.
SUITE 500
CHICAGO, IL 60606
TEL 312-939-4000
FAX 312-939-4190

USER NAME: jmaas-puch
DESIGNED: MAE
DRAWN: MAE
CHECKED: DAD
DATE: 2/17/2012

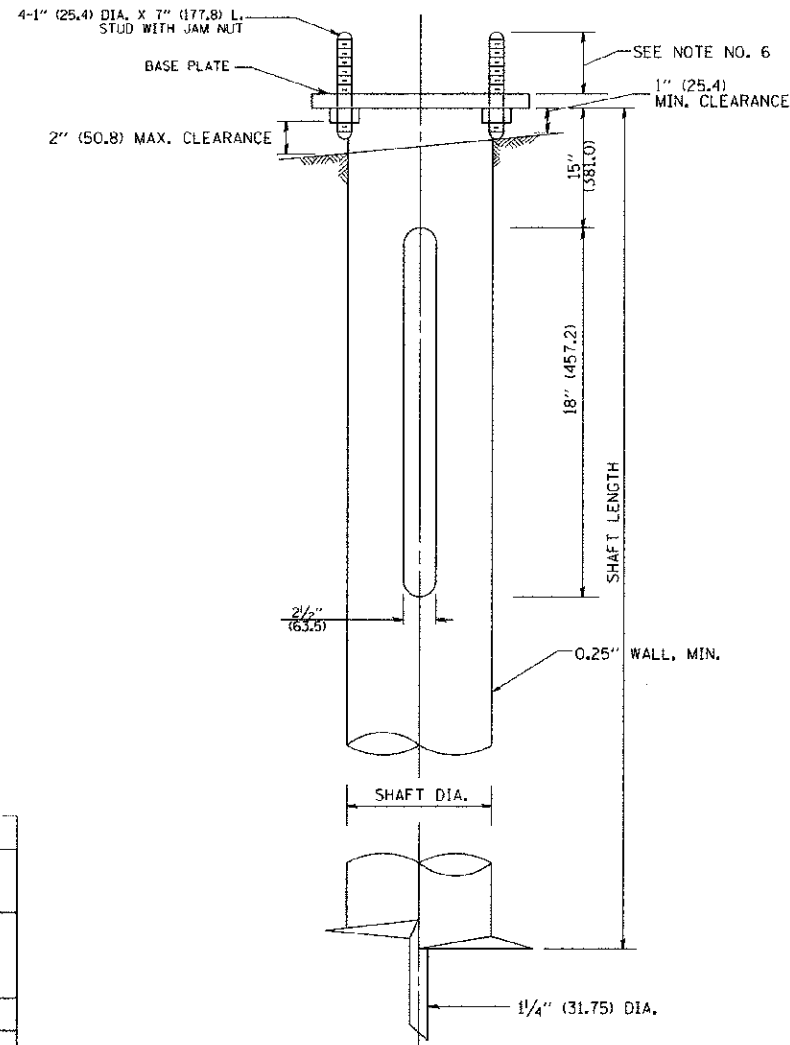
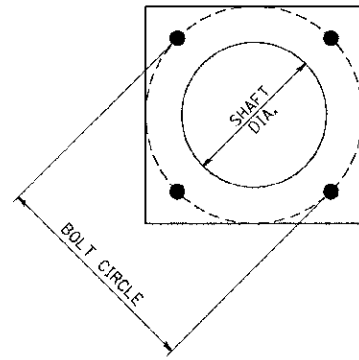
REVISIONS:
REVISED: -
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REVISED: -
REVISED: -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

95TH STREET
LIGHTING STANDARD DETAILS

SCALE: NONE DRAWING NO. 26 OF 30 STA. TO STA.

F.A.U. RTE. 1644	SECTION 01-00181-00-FP	COUNTY WILL	TOTAL SHEETS 328	SHEET NO. 154
CONTRACT NO. 63647				
ILLINOIS FED. AID PROJECT				



HELIX FOUNDATION SIZE

POLE MOUNTING HEIGHT	BOLT CIRCLE	SHAFT DIAMETER	SHAFT LENGTH	BASEPLATE
30 FT.	1 1/2"	8 5/8"	6 FT.	12"x12"x1"
31 FT.-35 FT.	1 1/2"	8 5/8"	6 FT.	12"x12"x1"
36 FT.-40 FT.	15"	8 5/8"	6 FT.	15"x15"x1 1/4"
41 FT.-45 FT.	15"	8 5/8"	6 FT.	15"x15"x1 1/4"
46 FT.-50 FT.	15"	10"	8 FT.	15"x15"x1 1/4"

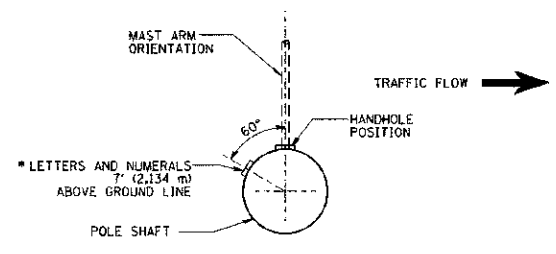
METAL HELIX FOUNDATION MATERIALS

ITEM	MATERIAL REQUIREMENT
BASEPLATE	AASHTO M 270M, GRADE 36 (M270M, GRADE 250)
SHAFT	ASTM A 252, GRADE 2 (PHOSPHOROUS 0.04% MAXIMUM, SULFUR 0.05% MAXIMUM)
HELIX SCREW	AASHTO M 183 (ASTM A 635)
PILOT POINT	AASHTO M 270 (ASTM A 575)
ANCHOR RODS/STUDS	AASHTO M 314 (ASTM F 1554)
HEXAGON NUTS	AASHTO M 291M (ASTM A 563) GRADE DH, OR AASHTO M 292 (ASTM A 194) GRADE 2H
WASHERS	AASHTO M 293 (ASTM F 436)

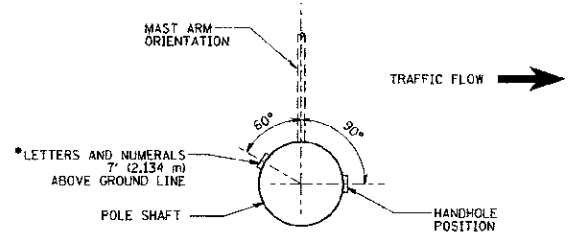
NOTES:

- ALL DIMENSION IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- ALL MATERIAL SHALL BE GALVINIZED ACCORDING TO AASHTO M111, UNLESS OTHERWISE SPECIFIED.
- ALL WELDS SHALL BE CONTINUOUS AND NOT LESS THAN 1/4" (6.35 mm) FILLET WELDS. THE WELDED FOUNDATION SHALL BE CAPABLE OF WITHSTANDING 10,000 FT/LBS (13558.18 n.m) OF INSTALLATION TORQUE APPLIED ABOUT THE AXIS OF THE FOUNDATION.
- THE HELIX FOUNDATION SHAFT SHALL BE INSTALLED VERTICAL AND THE BASE PLATE SHALL BE IN LEVEL. THE BREAKAWAY COUPLINGS AND HARDWARE SHALL NOT BE USED TO ALIGN THE POLE INSTALLATION.
- THE CABLE TRENCH SHALL BE BACKFILLED AND FIRMLY COMPACTED BEFORE THE INSTALLATION OF THE LIGHT POLE.
- THE CONTRACTOR SHALL COORDINATE EXTENSION OF ANCHOR BOLTS ABOVE TOP OF THE BASE PLATE WITH THE BREAKAWAY DEVICE MANUFACTURER'S REQUIREMENTS.
- ANY VOIDS WITHIN THE METAL FOUNDATION SHALL BE FILLED WITH FINE AGGREGATE.
- METAL FOUNDATIONS SHALL BE INSTALLED IN UNDISTURBED SOIL. PREDRILLING A PILOT HOLE AND/OR BACKFILLING AROUND THE FOUNDATION IS NOT ALLOWED.
- THE METAL FOUNDATION SHALL NOT BE INSTALLED TO A TORQUE WHICH EXCEEDS THE MANUFACTURER'S MAXIMUM TORQUE RATING NOR SHALL IT BE INSTALLED TO AN INSTALLATION TORQUE VALUE OF LESS THAN 3,500 FT LB (4,750 KNM). METAL FOUNDATIONS THAT ARE NOT INSTALLED TO FULL INSTALLATION DEPTH OR DO NOT ACHIEVE THE MINIMUM INSTALLATION TORQUE SHALL BE REMOVED AND REPLACED WITH A CONCRETE FOUNDATION AT NO ADDITIONAL COST.
- THE BASEPLATE SHALL BE PERPENDICULAR TO THE SHAFT AXIS ($\pm 1^\circ$) AND THE HOLE CENTERLINE SHALL BE CONCENTRIC (± 0.188) TO THE SHAFT AXIS.
- THE PILOT POINT AND SHAFT AXIS SHALL BE CONCENTRIC (± 0.125) AND IN LINE ($\pm 2^\circ$).
- THE BASEPLATE SHALL BE STAMPED WITH THE MANUFACTURERS NAME AND DATE OF MANUFACTURE.

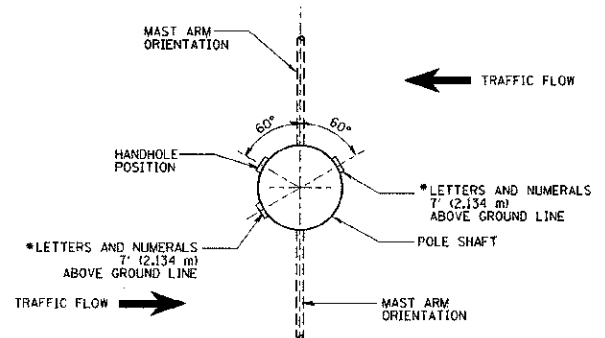
THIS STANDARD APPLIES TO WORK WITHIN VILLAGE OF BOLINGBROOK



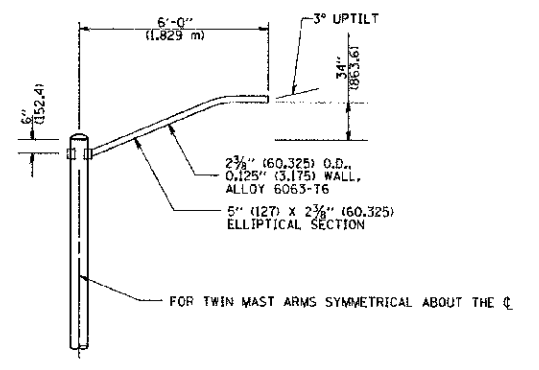
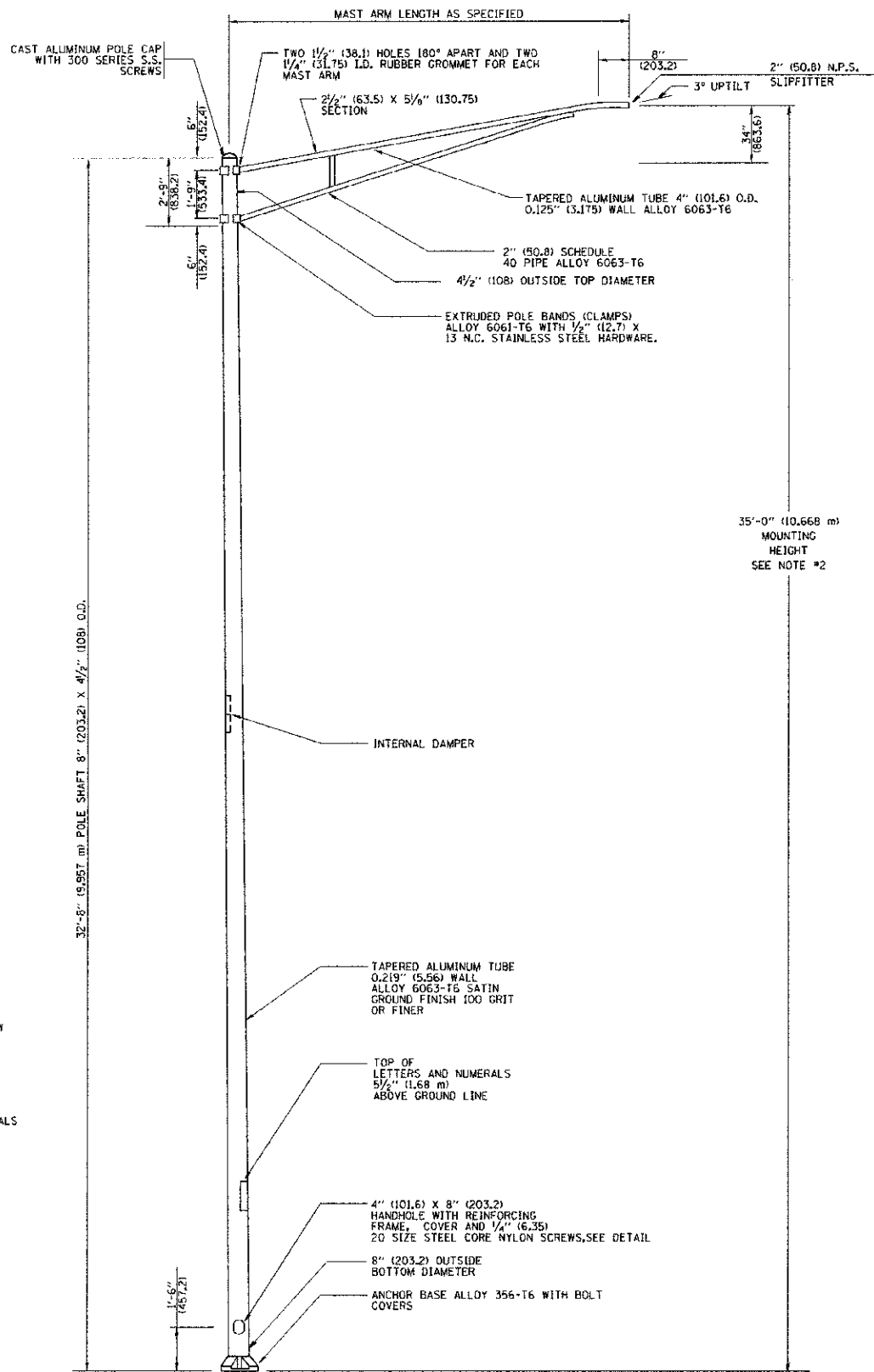
POSITION OF HANDHOLE AND POLE NUMBER FOR SINGLE MAST ARM POLES MOUNTED ON BRIDGE PARAPET OR BARRIER WALL



POSITION OF HANDHOLE AND POLE NUMBER FOR SINGLE MAST ARM POLES

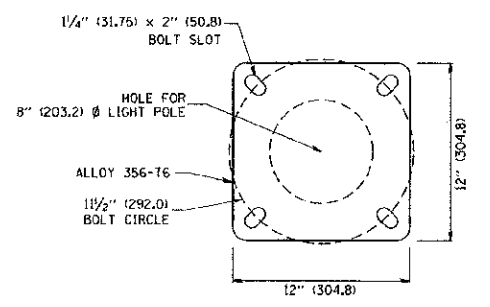


POSITION OF HANDHOLE AND POLE NUMBER FOR TWIN MAST ARM POLES

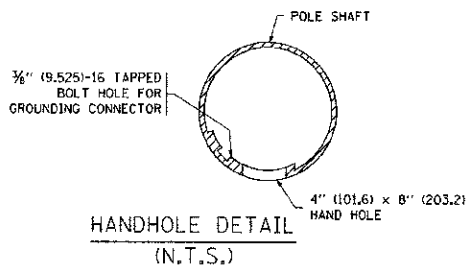


6' (1.8 m) SINGLE MEMBER MAST ARM (N.T.S.)

- NOTES:
1. ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
 2. MOUNTING HEIGHT IS DEFINED AS THE DISTANCE FROM THE CENTERLINE OF THE TENON TO THE BOTTOM OF THE ANCHOR BASE.
 3. THE LIGHT POLE WILL MEET AASHTO DESIGN CRITERIA AS SPECIFIED.
 4. THE INSTALLING CONTRACTOR WILL PROVIDE A UL LISTED GROUNDING CONNECTOR, BURNDY K2C23, T&B SP4DL OR APPROVED EQUAL.
 5. LIGHT POLES WILL NOT BE INSTALLED WITHOUT MAST ARMS AND LUMINAIRES.
 6. LIGHT POLES WILL BE SET PLUMB ON THE FOUNDATION WITHOUT THE USE OF LEVELING NUTS, WASHERS OR SHIMS.
 7. LIGHTING UNIT IDENTIFICATION NUMBERS SHALL BE INSTALLED BEFORE THE LIGHTING UNIT IS ENERGIZED.



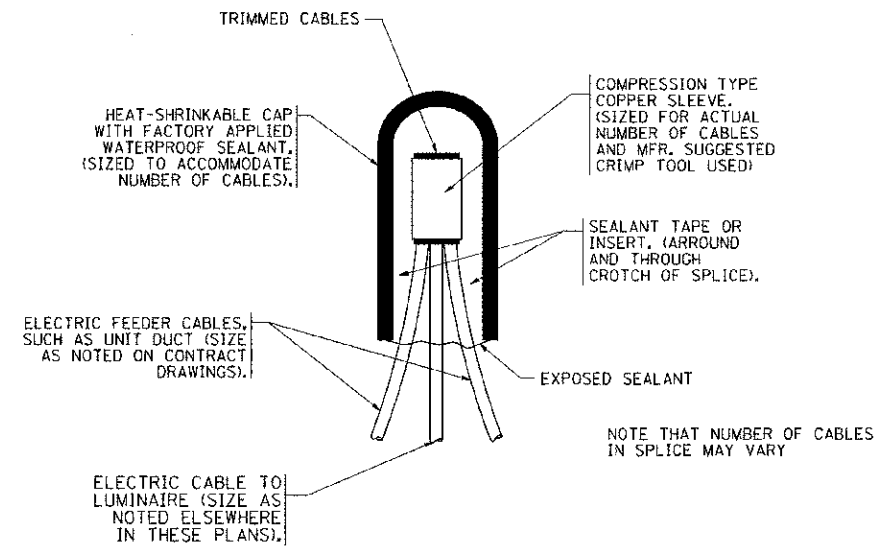
LIGHT POLE BASE PLATE DETAIL
1 1/2" (292.0) BOLT CIRCLE



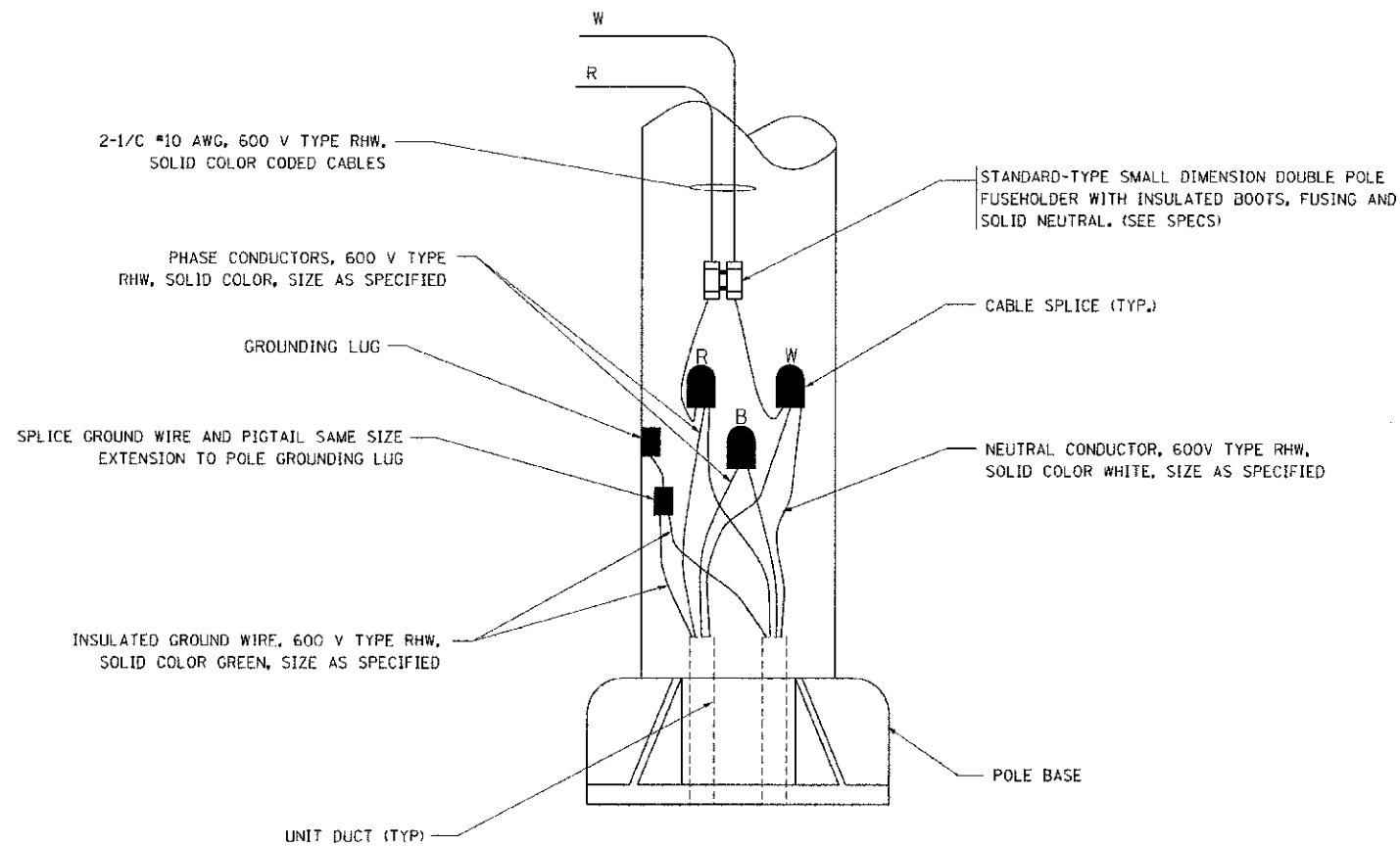
HANDHOLE DETAIL (N.T.S.)

THIS STANDARD APPLIES TO WORK WITHIN VILLAGE OF BOLINGBROOK

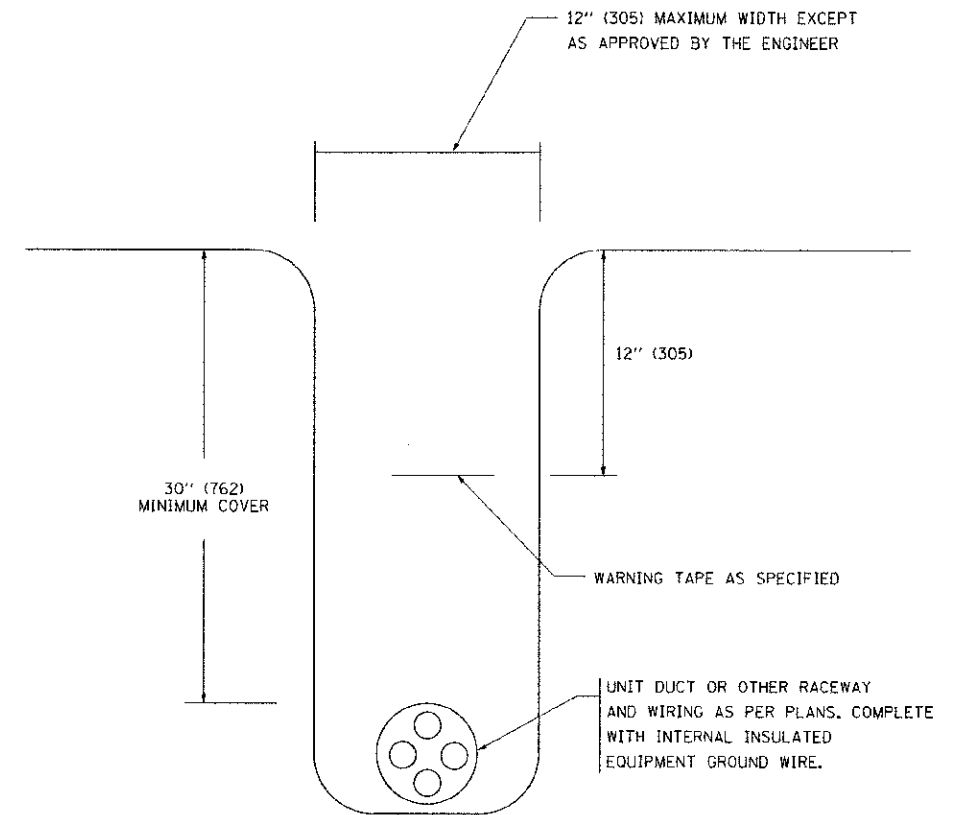
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PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED -	SCALE: NONE		SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	BE-402		CONTRACT NO. 63647		
PLOT DATE = 1/17/2008	DATE -	REVISED -	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT									



TYPICAL SPLICE DETAIL
N.T.S.



POLE WIRING DETAIL
N.T.S.

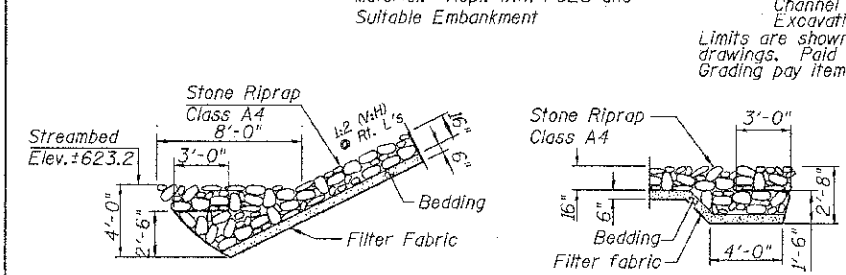
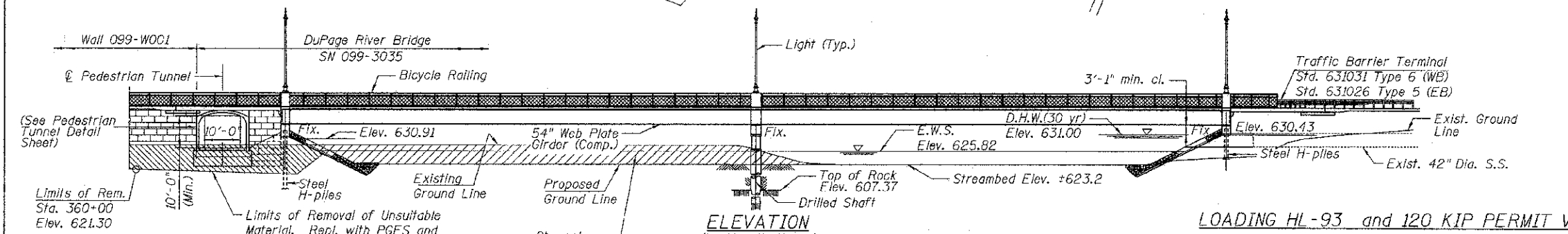
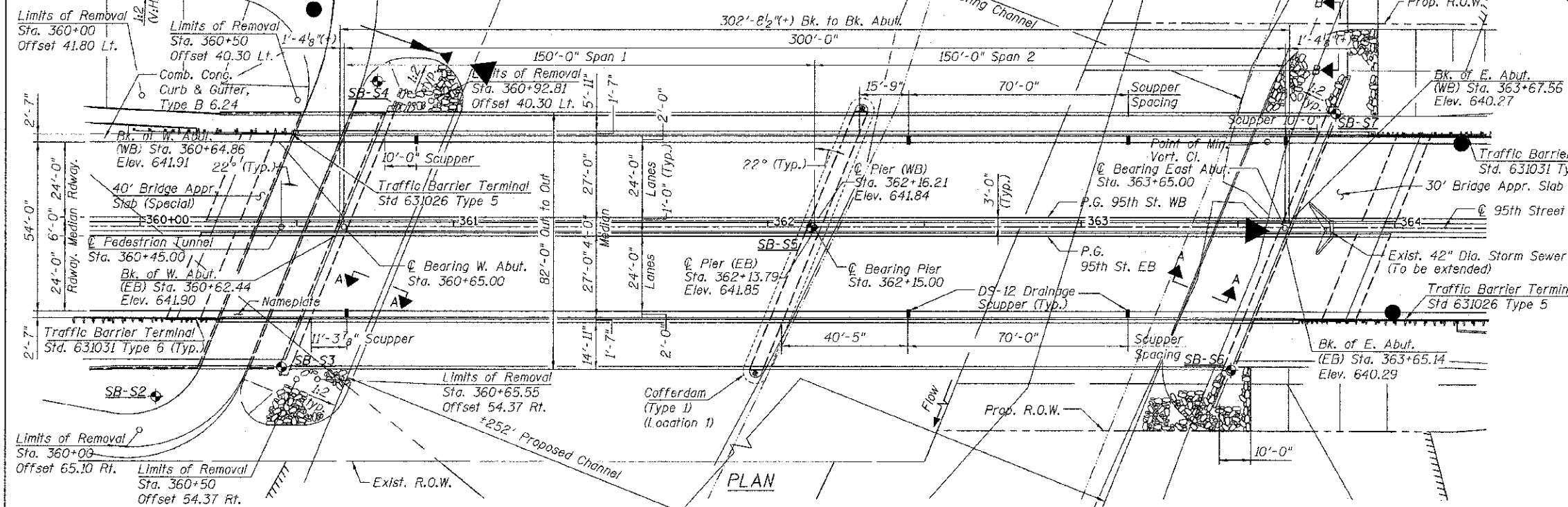


TYPICAL WIRING IN TRENCH DETAIL
N.T.S.

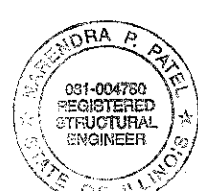
THIS STANDARD APPLIES TO WORK
WITHIN VILLAGE OF BOLINGBROOK

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	PLOT SCALE = 50,000 / IN.	CHECKED -	REVISED -				1644	01-00181-00-PP	WILL	328	158	
	PLOT DATE = 1/4/2008	DATE -	REVISED -		SCALE: NONE		SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	CONTRACT NO. 63647		
								FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

Existing Structure: None
 Bench Mark: B.M. #11 - square cut on culvert outlet headwall near proposed C of 95th St. Extension on south side of DuPage River. Elev. 632.67



STATION 362+15.00
 BUILT 2013 BY
 STATE OF ILLINOIS
 F.A.U. RT. 1644
 SE. 0100181-00-FP
 LOADING HL93
 STR. NO. 099-3035

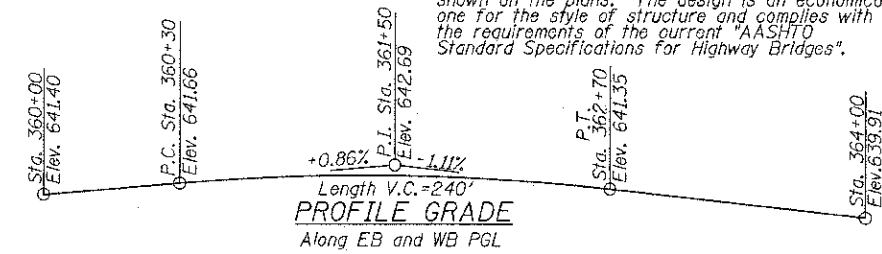


Signature: Narendra P. Patel For drawing sheets 1 through 38
 Current Date: October 19, 2012
 License Expires: November 30, 2014

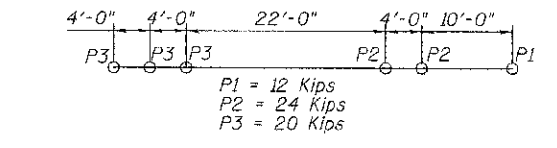
I certify that to the best of my knowledge, information and belief, this bridge design is structurally adequate for the design loading shown on the plans. The design is an economical one for the style of structure and complies with the requirements of the current "AASHTO Standard Specifications for Highway Bridges".

WATERWAY INFORMATION

Flood	Freq. Yr.	Q C.F.S.	Opening Sq. Ft.		Head - Ft.		Headwater El.		
			Exist.	Prop.	Exist.	Prop.	Exist.	Prop.	
Design	10	6020	N/A	1514	630.25	0.00	0.05	630.25	630.30
Base	30	7857	N/A	1678	631.94	0.00	0.06	631.94	631.00
	100	9918	N/A	1873	631.63	0.00	0.09	631.63	631.72
	500	12750	N/A	2016	632.40	0.00	0.11	632.40	632.51



LOADING HL-93 and 120 KIP PERMIT VEHICLE



DESIGN SPECIFICATIONS
 2010 AASHTO LRFD Bridge Design Specifications (5th Edition)

SEISMIC DATA
 Seismic Performance Zone (SPZ) = 1
 Design Spectral Acceleration at 1.0 sec. (SD1) = 0.045g
 Design Spectral Acceleration at 0.2 sec. (SDS) = 0.11g
 Soil Site Class = B

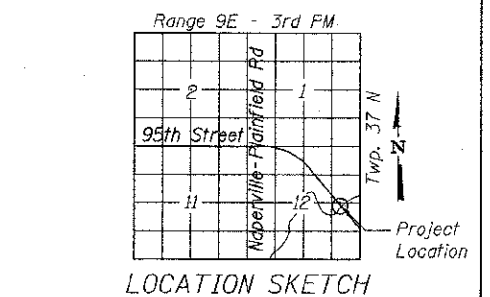
DESIGN STRESSES
 FIELD UNITS
 f' = 3,500 psi
 f' = 4,500 psi (Precast Elements)
 f_y = 60,000 psi (reinforcement)
 f_y = 50,000 psi (M270, Grade 50)

DESIGN SCOUR ELEVATION TABLE

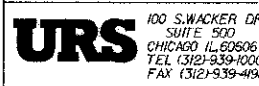
Design Scour Elevation (ft.)	W. Abut.	Pier	S. Abut.
	624.88	607.37	630.43

INDEX OF SHEETS

- General Plan and Elevation
- Estimated quantities and General Notes
- Substructure Layout
- Top of Slab Elevations 1
- Top of Slab Elevations 2
- Top of Slab Elevations 3
- Top of Slab Elevations 4
- Top of Slab Elevations 5
- Top of Slab Elevations 6
- Top of Approach Slab Elevations
- East Approach Slab
- East Approach Slab Details
- West Approach Slab
- West Approach Slab Details
- Deck Plan and Section
- Superstructure Details 1
- Superstructure Details 2
- Framing Plan
- Superstructure Steel Details
- Bearing Details
- Pier
- Pier Details
- East Abutment
- East Abutment Details
- West Abutment
- West Abutment Details
- MSE Retaining Walls 1, 2 & 3 at W. Abut.-1
- MSE Retaining Walls 1, 2, & 3 at W. Abut.-2
- Bridge Deck Drainage Plan and Details
- Bridge Deck Drainage Sections and Details
- Precast Tunnel Details
- Bicycle Railing
- HP Pile Details
- Drainager Scupper, DS-12
- Bar Splicer Assembly and Mechanical Splice Details
- Soil Borings 1 - Boring Nos. SB-S02, and SB-S03
- Soil Borings 2 - Boring Nos. SB-S04 and SB-S05
- Soil Borings 3 - Boring Nos. SB-S06 and SB-S07



GENERAL PLAN & ELEVATION
95TH STREET OVER THE DUPAGE RIVER
 F.A.U. RTE. 1644
 SEC. 01-00181-00-FP
 WILL COUNTY
 STATION 362+15.00
 STRUCTURE NO. 099-3035



DESIGNED	STB	REVISOR	DATE
CHECKED	NPP	REVISOR	DATE
DRAWN	SOI	REVISOR	DATE
CHECKED	NPP	REVISOR	DATE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHEET NO. 1 OF 38 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1644	01-00181-00-FP	WILL	328	159

CONTRACT NO. 63647
 ILLINOIS FED. AID PROJECT

GENERAL NOTES:

- 1 Fasteners shall be AASHTO M164 Type 1, mechanically galvanized bolts (in painted areas and M164 Type 3 in unpainted areas). Bolts $\frac{7}{8}$ " ϕ , holes $\frac{15}{16}$ " ϕ , unless otherwise noted.
- 2 Calculated weight of Structural Steel = 1,083,000 pounds AASHTO M 270 Grade 50W
- 3 All structural steel shall be AASHTO M 270 Grade 50W
- 4 No field welding is permitted except as specified in the contract documents.
- 5 Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions.
- 6 Reinforcement bars designated (E) shall be epoxy coated.
- 7 If the Contractor elects to use cantilever forming brackets on the exterior beams or girders, the brackets shall be placed at the same locations as required for the hardwood blocks in Article 503.06(b) of the Standard Specifications. If additional cantilever forming brackets are required, hardwood blocking shall be wedged between the exterior and first interior girder at each of these additional bracket locations.
- 8 Bearing seal surfaces shall be constructed or adjusted to their designated elevations within a tolerance of $\frac{1}{8}$ inch (0.01 ft.). Adjustment shall be made either by grinding the surface or by shiming the bearings.
- 9 Ends of structural steel girders shall only be painted for a distance equal to the depth of embedment into the concrete cap plus 3 inches. Painted areas shall be primed in the shop with a Department approved zinc rich primer. Field painting shall not be required.
- 10 Layout of slope protection system may be varied in the field to suit ground conditions as directed by the Engineer.
- 11 The embankment configuration shown shall be the minimum that must be placed and compacted prior to construction of the abutments.
- 12 The Contractor shall obtain a construction permit from the Illinois Department of Natural Resources (IDNR), Office of Water Resources for any temporary construction activity placed in the water except cofferdams. This shall include the placement of material for run-arounds, causeways, etc. Any permit application by the Contractor shall refer to the IDNR 3704 Floodway Construction permit number allowing permanent construction as shown in the contract plans.
- 13 All cross frames or diaphragms shall be installed as steel is erected and secured with erection pins and bolts except as otherwise noted. Individual cross frames or diaphragms at supports may be temporarily disconnected to install bearing anchor rods.
- 14 Load carrying components designated "NTR" shall conform to the Supplemental Requirements for Notch Toughness, Zone 2.
- 15 The structural steel plates of the Bearing Assembly shall conform to the requirements of AASHTO M 270 Grade 50 W.
- 16 The foundation design of three sided precast concrete structure is based on the following maximum service reactions applied at the top of the footing: 31 kips/ft (vertical), 2.0 kips/ft (horizontal)
The Contractor shall verify that the selected structure meets these design parameters. If the design parameters are exceeded, a complete foundation design with calculations, details, and the required seals shall be submitted for review and approval.

- 18 Structural Engineer's Seal does not include design of the precast elements.
19. All construction joints shall be bonded.
20. The DuPage River is used at times by canoeists. During construction the Contractor shall prevent debris from falling into the river and shall not dump debris into the river.

BILL OF MATERIAL

PAY ITEM DESCRIPTION	UNIT	SUPERSTRUCTURE	SUBSTRUCTURE	TOTAL
STONE RIPRAP, CLASS A4	Sq. Yd.	0	761	761
STONE RIPRAP, CLASS A4	Ton	0	954	954
FILTER FABRIC	Sq. Yd.	0	1,314	1,314
COFFERDAM EXCAVATION	Cu. Yd.	0	30	30
COFFERDAM (TYPE 1) (LOCATION-1)	Each	0	1	1
CONCRETE STRUCTURES	Cu. Yd.	0.0	415.5	415.5
CONCRETE SUPERSTRUCTURE	Cu. Yd.	1,043.5	0.0	1,043.5
BRIDGE DECK GROOVING	SQ YD	2,071	0	2,071
CONCRETE ENCASEMENT	Cu. Yd.	0	25	25
PROTECTIVE COAT	Sq. Yd.	3,629	0	3,629
FURNISHING AND ERECTING STRUCTURAL STEEL	L. Sum	1	0	1
STUD SHEAR CONNECTORS	Each	12,560	0	12,560
REINFORCEMENT BARS, EPOXY COATED	Pound	254,390	60,230	314,620
BAR SPLICERS	Each	124	0	124
BICYCLE RAILING	Foot	716	0	716
PARAPET RAILING	Foot	690	0	690
FURNISHING STEEL PILES HP14X73	Foot	0	1,379	1,379
DRIVING PILES	Foot	0	1,379	1,379
TEST PILE STEEL HP14X73	Each	0	2	2
PILE SHOES	Each	0	45	45
NAME PLATES	Each	1	0	1
DRILLED SHAFT IN SOIL	Cu. Yd.	0	42.1	42.1
DRILLED SHAFT IN ROCK	Cu. Yd.	0	11.0	11.0
ANCHOR BOLTS, 1"	Each	0	40	40
ANCHOR BOLTS, 1 1/2"	Each	0	20	20
GEOCOMPOSITE WALL DRAIN	Sq. Yd.	190	0	190
CONDUIT EMBEDDED IN STRUCTURE, 2" DIA., PVC	Foot	1,564	0	1,564
JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 18" X 12" X 8"	Each	7	0	7
THREE SIDED PRECAST CONCRETE STRUCTURES 10' X 14'	Foot	0	89	89
DRAINAGE SYSTEM	Lump Sum	1	0	1
POROUS GRANULAR EMBANKMENT, SPECIAL	Cu. Yd.	0	293	293
DRAINAGE SCUPPERS, DS-12	Each	7	0	7
MECHANICALLY STABILIZED EARTH RETAINING WALL	Sq. Ft.	0	499	499
PIPE UNDERDRAIN FOR STRUCTURES 4"	Foot	0	205	205

FILE NAME = g:\vcdob\2036781\96th\drainage\contract\east\structure\bridge (duPage)\Control\Cup\15-82 Estimated Quantities and General Notes.dgn



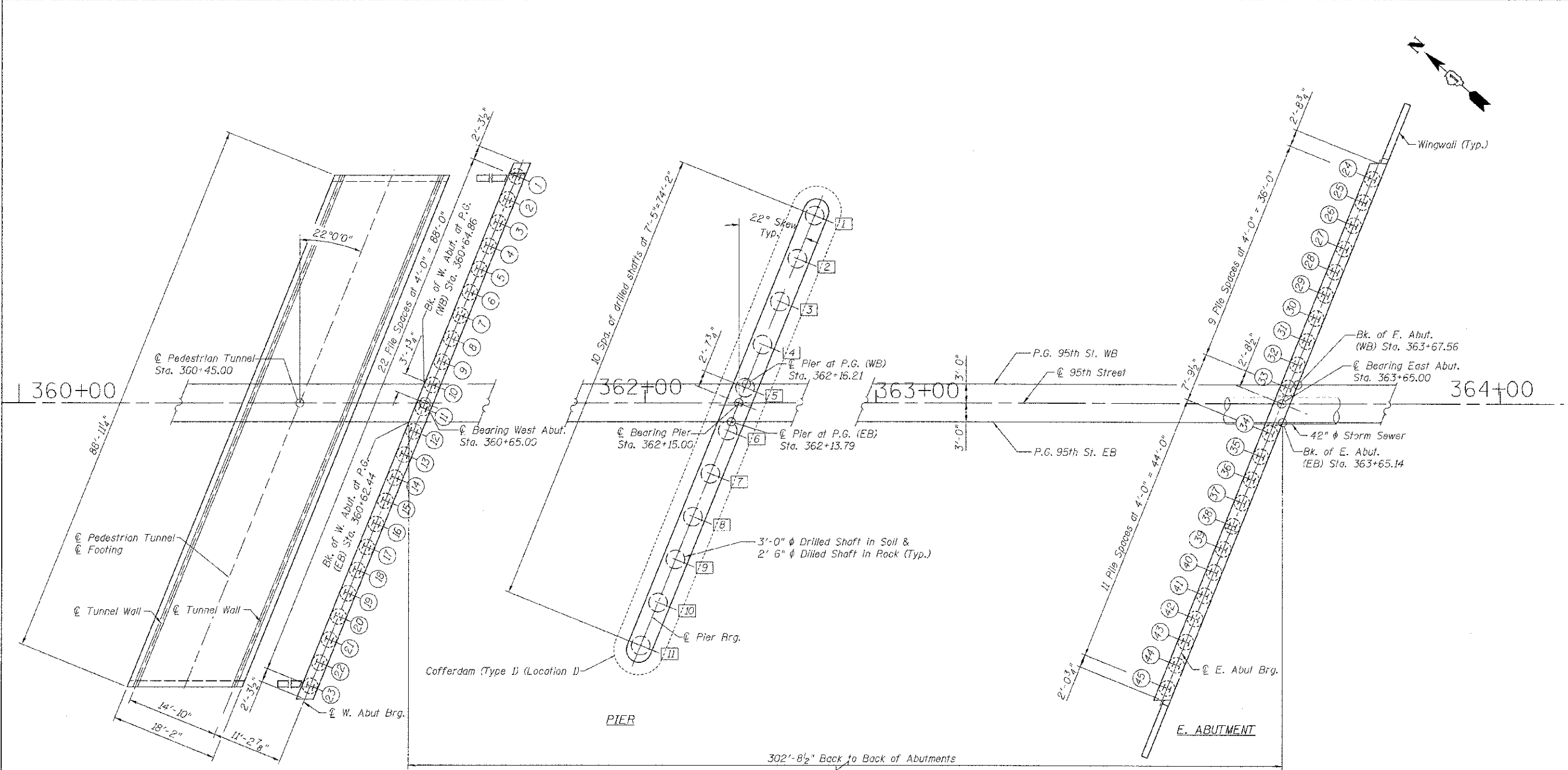
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PILOT DATE = 10/16/2012	DRAWN - SOI	REVISED -
	CHECKED - NPP	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ESTIMATED QUANTITIES AND GENERAL NOTES
STRUCTURE NO. 099-3035**

SHEET NO. 2 OF 38 SHEETS

F.A.U. RTE. 1644	SECTION 01-00181-00-FP	COUNTY WILL	TOTAL SHEETS 328	SHEET NO. 160
CONTRACT NO. 63647				ILLINOIS FED. AID PROJECT



PEDESTRIAN TUNNEL W. ABUTMENT

FOOTING LAYOUT PLAN

LEGENDS

- [1] Drilled shaft
- (1) HP14x73 Pile

FILE NAME = c:\wds\25387381_95th\substructure\contract\sub\Structure\Bridg\Substructure Layout.dgn

URS
100 S. WACKER DR.
SUITE 500
CHICAGO, IL 60605
TEL (312) 930-1000
FAX (312) 939-4996

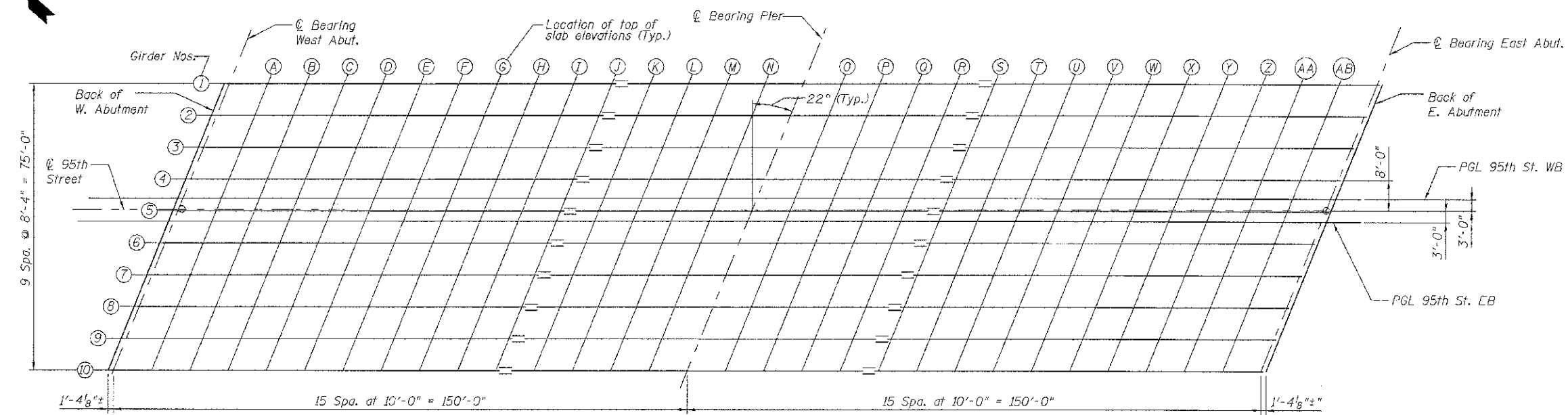
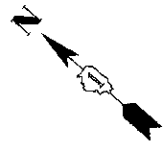
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PLOT DATE = 10/16/2012	DRAWN - SOI	REVISED -
	CHECKED - NFP	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUBSTRUCTURE LAYOUT
STRUCTURE NO. 099-3035**

SHEET NO. 3 OF 38 SHEETS

F.A.U. RTE. 1644	SECTION 01-00181-00-FP	COUNTY WILL	TOTAL SHEETS 328	SHEET NO. 161
				CONTRACT NO. 63647
ILLINOIS FED. AID PROJECT				



DECK PLAN

FILE NAME: g:\waddol_285873BL_1983\Drawings\Contract\msh\Structure\Bridges\Control\Comp\AS 04 Top of Slab Elevation - 1.dgn

URS
 100 S. WACKER DR.
 SUITE 500
 CHICAGO, IL 60606
 TEL (312) 939-1000
 FAX (312) 939-4998

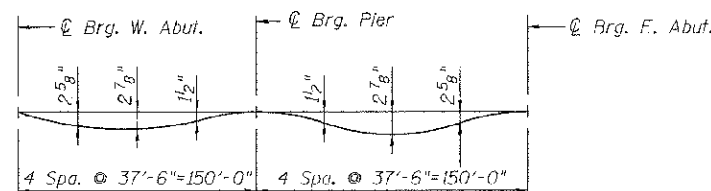
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	CHECKED - NPF	REVISED -
PLOT SCALE = 1/8" = 1' / 1"	DRAWN - SOJ	REVISED -
PLOT DATE = 10/16/2012	CHECKED - NPP	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**TOP OF SLAB ELEVATIONS - 1
 STRUCTURE NO. 099-3035**

SHEET NO. 4 OF 38 SHEETS

F.A.J. RTE. 1644	SECTION 01-00181-00-FP	COUNTY WILL	TOTAL SHEET SHEETS: NO. 328 / 162
			CONTRACT NO. 63647
ILLINOIS FED. AID PROJECT			

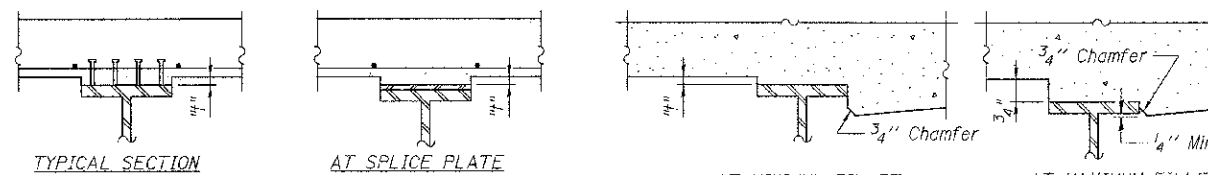


DEAD LOAD DEFLECTION DIAGRAM

(Includes weight of concrete only.)

Note:

The above deflections are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections as shown below.



INTERIOR GIRDERS

EXTERIOR GIRDERS

FILLET HEIGHTS

To determine "t": After all structural steel has been erected, elevations of the top flanges of the beams shall be taken at intervals shown below. These elevations subtracted from the "Theoretical Grade Elevations Adjusted For Dead Load Deflection" shown below, minus slab thickness, equals the fillet heights "t" above top flange of girders.

NORTH EDGE OF DECK

Location	Station	Offset*	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
EAST END OF W. APPROACH PAVEMENT & BK. W. ABUT.	360+76.40	36.50	641.31	641.31
C.L. BRG. W. ABUT.	360+79.75	36.50	641.32	641.32
A	360+89.75	36.50	641.36	641.42
B	360+99.75	36.50	641.39	641.52
C	361+09.75	36.50	641.41	641.60
D	361+19.75	36.50	641.43	641.65
E	361+29.75	36.50	641.44	641.68
F	361+39.75	36.50	641.44	641.70
G	361+49.75	36.50	641.43	641.68
H	361+59.75	36.50	641.41	641.64
I	361+69.75	36.50	641.39	641.59
J	361+79.75	36.50	641.36	641.51
K	361+89.75	36.50	641.32	641.43
L	361+99.75	36.50	641.27	641.34
M	362+09.75	36.50	641.21	641.25
N	362+19.75	36.50	641.14	641.16
C.L. PIER	362+29.75	36.50	641.07	641.07
O	362+39.75	36.50	640.99	641.00
P	362+49.75	36.50	640.90	640.94
Q	362+59.75	36.50	640.80	640.87
R	362+69.75	36.50	640.69	640.81
S	362+79.75	36.50	640.57	640.73
T	362+89.75	36.50	640.46	640.66
U	362+99.75	36.50	640.35	640.58
V	363+09.75	36.50	640.24	640.48
W	363+19.75	36.50	640.13	640.38
X	363+29.75	36.50	640.02	640.26
Y	363+39.75	36.50	639.91	640.12
Z	363+49.75	36.50	639.79	639.98
AA	363+59.75	36.50	639.68	639.81
AB	363+69.75	36.50	639.57	639.64
C.L. BRG. E. ABUT.	363+79.75	36.50	639.46	639.46
WEST END OF E. APPROACH PAVEMENT & BK. E. ABUT.	363+81.10	36.50	639.45	639.45

GIRDER 1

Location	Station	Offset*	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
EAST END OF W. APPROACH PAVEMENT & BK. W. ABUT.	360+76.98	33.00	641.37	641.37
C.L. BRG. W. ABUT.	360+78.33	33.00	641.38	641.38
A	360+88.33	33.00	641.42	641.49
B	360+98.33	33.00	641.46	641.58
C	361+08.33	33.00	641.48	641.66
D	361+18.33	33.00	641.50	641.72
E	361+28.33	33.00	641.51	641.75
F	361+38.33	33.00	641.51	641.77
G	361+48.33	33.00	641.50	641.75
H	361+58.33	33.00	641.49	641.71
I	361+68.33	33.00	641.46	641.66
J	361+78.33	33.00	641.43	641.59
K	361+88.33	33.00	641.39	641.51
L	361+98.33	33.00	641.34	641.42
M	362+08.33	33.00	641.29	641.33
N	362+18.33	33.00	641.22	641.24
C.L. PIER	362+28.33	33.00	641.15	641.15
O	362+38.33	33.00	641.07	641.08
P	362+48.33	33.00	640.98	641.02
Q	362+58.33	33.00	640.88	640.96
R	362+68.33	33.00	640.78	640.89
S	362+78.33	33.00	640.66	640.81
T	362+88.33	33.00	640.55	640.74
U	362+98.33	33.00	640.44	640.66
V	363+08.33	33.00	640.32	640.57
W	363+18.33	33.00	640.21	640.47
X	363+28.33	33.00	640.10	640.35
Y	363+38.33	33.00	639.99	640.21
Z	363+48.33	33.00	639.88	640.06
AA	363+58.33	33.00	639.77	639.90
AB	363+68.33	33.00	639.66	639.73
C.L. BRG. E. ABUT.	363+78.33	33.00	639.55	639.55
WEST END OF E. APPROACH PAVEMENT & BK. E. ABUT.	363+79.68	33.00	639.53	639.53

GIRDER 2

Location	Station	Offset*	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
EAST END OF W. APPROACH PAVEMENT & BK. W. ABUT.	360+73.62	24.67	641.52	641.52
C.L. BRG. W. ABUT.	360+74.97	24.67	641.53	641.53
A	360+84.97	24.67	641.58	641.64
B	360+94.97	24.67	641.61	641.74
C	361+04.97	24.67	641.64	641.82
D	361+14.97	24.67	641.66	641.88
E	361+24.97	24.67	641.67	641.92
F	361+34.97	24.67	641.68	641.93
G	361+44.97	24.67	641.67	641.92
H	361+54.97	24.67	641.66	641.89
I	361+64.97	24.67	641.64	641.84
J	361+74.97	24.67	641.61	641.77
K	361+84.97	24.67	641.57	641.69
L	361+94.97	24.67	641.53	641.60
M	362+04.97	24.67	641.47	641.51
N	362+14.97	24.67	641.41	641.43
C.L. PIER	362+24.97	24.67	641.34	641.34
O	362+34.97	24.67	641.27	641.28
P	362+44.97	24.67	641.18	641.22
Q	362+54.97	24.67	641.08	641.16
R	362+64.97	24.67	640.98	641.10
S	362+74.97	24.67	640.86	641.02
T	362+84.97	24.67	640.75	640.95
U	362+94.97	24.67	640.64	640.87
V	363+04.97	24.67	640.53	640.77
W	363+14.97	24.67	640.42	640.67
X	363+24.97	24.67	640.31	640.55
Y	363+34.97	24.67	640.20	640.41
Z	363+44.97	24.67	640.08	640.27
AA	363+54.97	24.67	639.97	640.10
AB	363+64.97	24.67	639.86	639.93
C.L. BRG. E. ABUT.	363+74.97	24.67	639.75	639.75
WEST END OF E. APPROACH PAVEMENT & BK. E. ABUT.	363+76.32	24.67	639.74	639.74

* A positive value indicates a left offset from the centerline of 95th street. A negative value indicates a right offset.

GIRDER 3

Location	Station	Offset*	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
EAST END OF W. APPROACH PAVEMENT & BK. W. ABUT.	360+70.25	16.33	641.67	641.67
C.L. BRG. W. ABUT.	360+71.60	16.33	641.68	641.68
A	360+81.60	16.33	641.73	641.79
B	360+91.60	16.33	641.77	641.89
C	361+01.60	16.33	641.80	641.98
D	361+11.60	16.33	641.82	642.04
E	361+21.60	16.33	641.84	642.08
F	361+31.60	16.33	641.84	642.10
G	361+41.60	16.33	641.84	642.09
H	361+51.60	16.33	641.83	642.06
I	361+61.60	16.33	641.81	642.01
J	361+71.60	16.33	641.79	641.94
K	361+81.60	16.33	641.75	641.87
L	361+91.60	16.33	641.71	641.78
M	362+01.60	16.33	641.66	641.70
N	362+11.60	16.33	641.60	641.62
C.L. PIER	362+21.60	16.33	641.53	641.53
O	362+31.60	16.33	641.46	641.47
P	362+41.60	16.33	641.38	641.41
Q	362+51.60	16.33	641.28	641.36
R	362+61.60	16.33	641.18	641.30
S	362+71.60	16.33	641.07	641.22
T	362+81.60	16.33	640.95	641.15
U	362+91.60	16.33	640.84	641.07
V	363+01.60	16.33	640.73	640.98
W	363+11.60	16.33	640.62	640.88
X	363+21.60	16.33	640.51	640.75
Y	363+31.60	16.33	640.40	640.62
Z	363+41.60	16.33	640.29	640.47
AA	363+51.60	16.33	640.18	640.31
AB	363+61.60	16.33	640.07	640.13
C.L. BRG. E. ABUT.	363+71.60	16.33	639.96	639.96
WEST END OF E. APPROACH PAVEMENT & BK. E. ABUT.	363+72.95	16.33	639.94	639.94

GIRDER 4

Location	Station	Offset*	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
EAST END OF W. APPROACH PAVEMENT & BK. W. ABUT.	360+66.88	8.00	641.82	641.82
C.L. BRG. W. ABUT.	360+68.23	8.00	641.83	641.83
A	360+78.23	8.00	641.88	641.95
B	360+88.23	8.00	641.92	642.05
C	360+98.23	8.00	641.96	642.14
D	361+08.23	8.00	641.98	642.20
E	361+18.23	8.00	642.00	642.24
F	361+28.23	8.00	642.01	642.26
G	361+38.23	8.00	642.01	642.26
H	361+48.23	8.00	642.00	642.23
I	361+58.23	8.00	641.99	642.19
J	361+68.23	8.00	641.96	642.12
K	361+78.23	8.00	641.93	642.05
L	361+88.23	8.00	641.89	641.97
M	361+98.23	8.00	641.85	641.88
N	362+08.23	8.00	641.79	641.80
C.L. PIER	362+18.23	8.00	641.72	641.72
O	362+28.23	8.00	641.65	641.67
P	362+38.23	8.00	641.57	641.61
Q	362+48.23	8.00	641.48	641.55
R	362+58.23	8.00	641.38	641.50
S	362+68.23	8.00	641.28	641.44
T	362+78.23	8.00	641.16	641.36
U	362+88.23	8.00	641.05	641.27
V	362+98.23	8.00	640.94	641.18
W	363+08.23	8.00	640.83	641.08
X	363+18.23	8.00	640.71	640.96
Y	363+28.23	8.00	640.60	640.82
Z	363+38.23	8.00	640.49	640.67
AA	363+48.23	8.00	640.38	640.51
AB	363+58.23	8.00	640.27	640.34
C.L. BRG. E. ABUT.	363+68.23	8.00	640.16	640.16
WEST END OF E. APPROACH PAVEMENT & BK. E. ABUT.	363+69.58	8.00	640.14	640.14

PROFILE GRADE WB

Location	Station	Offset*	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
EAST END OF W. APPROACH PAVEMENT & BK. W. ABUT.	360+64.86	3.00	641.91	641.91
C.L. BRG. W. ABUT.	360+66.21	3.00	641.92	641.92
A	360+76.21	3.00	641.97	642.04
B	360+86.21	3.00	642.01	642.14
C	360+96.21	3.00	642.05	642.23
D	361+06.21	3.00	642.08	642.25
E	361+16.21	3.00	642.10	642.34
F	361+26.21	3.00	642.11	642.36
G	361+36.21	3.00	642.11	642.36
H	361+46.21	3.00	642.11	642.33
I	361+56.21	3.00	642.09	642.29
J	361+66.21	3.00	642.07	642.23
K	361+76.21	3.00	642.04	642.15
L	361+86.21	3.00	642.00	642.07
M	361+96.21	3.00	641.96	641.99
N	362+06.21	3.00	641.90	641.92
C.L. PIER	362+16.21	3.00	641.84	641.84
O	362+26.21	3.00	641.77	641.78
P	362+36.21	3.00	641.69	641.73
Q	362+46.21	3.00	641.60	641.67
R	362+56.21	3.00	641.51	641.62
S	362+66.21	3.00	641.40	641.56
T	362+76.21	3.00	641.28	641.48
U	362+86.21	3.00	641.17	641.40
V	362+96.21	3.00	641.06	641.31
W	363+06.21	3.00	640.95	641.20
X	363+16.21	3.00	640.84	641.08
Y	363+26.21	3.00	640.73	640.94
Z	363+36.21	3.00	640.62	640.80
AA	363+46.21	3.00	640.50	640.63
AB	363+56.21	3.00	640.39	640.46
C.L. BRG. E. ABUT.	363+66.21	3.00	640.28	640.28
WEST END OF E. APPROACH PAVEMENT & BK. E. ABUT.	363+67.56	3.00	640.27	640.27

* A positive value indicates a left offset from the centerline of 95th street. A negative value indicates a right offset.

FILE NAME = ep\vedobh_28387281_95th\dwg\top of slab\top of slab.dwg (C:\Program Files\Autodesk\AutoCAD 2004\acad.rvt) (C:\Program Files\Autodesk\AutoCAD 2004\acad.rvt)



100 S. WACKER DR.
SUITE 500
CHICAGO, IL 60606
TEL (312) 433-4000
FAX (312) 335-4998

USER NAME = #319*	DESIGNED - STB	REVISIONS -
PILOT SCALE = 1/8" = 1'-0"	CHECKED - NPP	REVISIONS -
PLOT DATE = 10/16/2002	DRAWN - SOI	REVISIONS -
	CHECKED - NPP	REVISIONS -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS - 3
STRUCTURE NO. 099-3035

SHEET NO. 6 OF 38 SHEETS

F.A.I.L. RTE. 1644	SECTION 01-00181-00-FP	COUNTY WILL	TOTAL SHEETS 328	SHEET NO. 164
CONTRACT NO. 63647				ILLINOIS FED. AID PROJECT

© 95th ST. & CROWN

Location	Station	Offset*	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
EAST END OF W. APPROACH PAVEMENT & BK. W. ABUT.	360+63.65	0.00	641.96	641.96
C.L. BRG. W. ABUT.	360+65.00	0.00	641.97	641.97
A	360+75.00	0.00	642.02	642.09
B	360+85.00	0.00	642.07	642.20
C	360+95.00	0.00	642.11	642.29
D	361+05.00	0.00	642.13	642.35
E	361+15.00	0.00	642.15	642.40
F	361+25.00	0.00	642.17	642.42
G	361+35.00	0.00	642.17	642.42
H	361+45.00	0.00	642.17	642.39
I	361+55.00	0.00	642.15	642.35
J	361+65.00	0.00	642.13	642.29
K	361+75.00	0.00	642.10	642.22
L	361+85.00	0.00	642.07	642.14
M	361+95.00	0.00	642.02	642.06
N	362+05.00	0.00	641.97	641.98
C.L. PIER	362+15.00	0.00	641.91	641.91
O	362+25.00	0.00	641.84	641.85
P	362+35.00	0.00	641.76	641.80
Q	362+45.00	0.00	641.67	641.74
R	362+55.00	0.00	641.58	641.69
S	362+65.00	0.00	641.47	641.63
T	362+75.00	0.00	641.35	641.55
U	362+85.00	0.00	641.24	641.47
V	362+95.00	0.00	641.13	641.38
W	363+05.00	0.00	641.02	641.28
X	363+15.00	0.00	640.91	641.15
Y	363+25.00	0.00	640.80	641.02
Z	363+35.00	0.00	640.69	640.87
AA	363+45.00	0.00	640.58	640.71
AB	363+55.00	0.00	640.47	640.53
C.L. BRG. E. ABUT.	363+65.00	0.00	640.36	640.36
WEST END OF E. APPROACH PAVEMENT & BK. E. ABUT.	363+66.35	0.00	640.34	640.34

GIRDER 5

Location	Station	Offset*	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
EAST END OF W. APPROACH PAVEMENT & BK. W. ABUT.	360+63.52	-0.33	641.96	641.96
C.L. BRG. W. ABUT.	360+64.87	-0.33	641.96	641.96
A	360+74.87	-0.33	642.02	642.08
B	360+84.87	-0.33	642.06	642.19
C	360+94.87	-0.33	642.10	642.28
D	361+04.87	-0.33	642.13	642.34
E	361+14.87	-0.33	642.15	642.39
F	361+24.87	-0.33	642.16	642.42
G	361+34.87	-0.33	642.16	642.41
H	361+44.87	-0.33	642.16	642.39
I	361+54.87	-0.33	642.15	642.34
J	361+64.87	-0.33	642.13	642.28
K	361+74.87	-0.33	642.10	642.21
L	361+84.87	-0.33	642.06	642.13
M	361+94.87	-0.33	642.02	642.05
N	362+04.87	-0.33	641.96	641.96
C.L. PIER	362+14.87	-0.33	641.90	641.90
O	362+24.87	-0.33	641.83	641.84
P	362+34.87	-0.33	641.75	641.79
Q	362+44.87	-0.33	641.67	641.74
R	362+54.87	-0.33	641.57	641.69
S	362+64.87	-0.33	641.47	641.63
T	362+74.87	-0.33	641.35	641.55
U	362+84.87	-0.33	641.24	641.46
V	362+94.87	-0.33	641.13	641.37
W	363+04.87	-0.33	641.02	641.27
X	363+14.87	-0.33	640.91	641.15
Y	363+24.87	-0.33	640.79	641.01
Z	363+34.87	-0.33	640.68	640.87
AA	363+44.87	-0.33	640.57	640.70
AB	363+54.87	-0.33	640.46	640.53
C.L. BRG. E. ABUT.	363+64.87	-0.33	640.35	640.35
WEST END OF E. APPROACH PAVEMENT & BK. E. ABUT.	363+66.21	-0.33	640.34	640.34

PROFILE GRADE EB

Location	Station	Offset*	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
EAST END OF W. APPROACH PAVEMENT & BK. W. ABUT.	360+62.44	-3.00	641.90	641.90
C.L. BRG. W. ABUT.	360+63.79	-3.00	641.90	641.90
A	360+73.79	-3.00	641.96	642.02
B	360+83.79	-3.00	642.00	642.13
C	360+93.79	-3.00	642.04	642.22
D	361+03.79	-3.00	642.07	642.29
E	361+13.79	-3.00	642.09	642.33
F	361+23.79	-3.00	642.11	642.36
G	361+33.79	3.00	642.11	642.36
H	361+43.79	-3.00	642.11	642.33
I	361+53.79	-3.00	642.10	642.29
J	361+63.79	-3.00	642.08	642.23
K	361+73.79	-3.00	642.05	642.16
L	361+83.79	-3.00	642.01	642.08
M	361+93.79	-3.00	641.97	642.01
N	362+03.79	-3.00	641.92	641.93
C.L. PIER	362+13.79	-3.00	641.85	641.85
O	362+23.79	-3.00	641.79	641.80
P	362+33.79	-3.00	641.71	641.75
Q	362+43.79	-3.00	641.62	641.69
R	362+53.79	-3.00	641.53	641.64
S	362+63.79	-3.00	641.43	641.58
T	362+73.79	-3.00	641.31	641.50
U	362+83.79	-3.00	641.20	641.42
V	362+93.79	-3.00	641.09	641.33
W	363+03.79	-3.00	640.97	641.23
X	363+13.79	-3.00	640.86	641.11
Y	363+23.79	-3.00	640.75	640.97
Z	363+33.79	-3.00	640.64	640.82
AA	363+43.79	-3.00	640.53	640.66
AB	363+53.79	-3.00	640.42	640.49
C.L. BRG. E. ABUT.	363+63.79	-3.00	640.31	640.31
WEST END OF E. APPROACH PAVEMENT & BK. E. ABUT.	363+65.14	-3.00	640.29	640.29

* A positive value indicates a left offset from the centerline of 95th street. A negative value indicates a right offset.

FILE NAME = c:\msd\01_25357381_95th\sub\av\g\contract\sub\Structure\Bridg\01\Top of Deck Elevations - 4.dgn



DESIGNED - STE	REVISIONS -
CHECKED - NFP	REVISIONS -
DRAWN - SOI	REVISIONS -
CHECKED - NFP	REVISIONS -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS - 4
STRUCTURE NO. 099-3035
SHEET NO. 7 OF 38 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1644	01-00181-00-PP	WILL	328	165
CONTRACT NO. 63647			ILLINOIS FED. AID PROJECT	

GIRDER 6

Location	Station	Offset*	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
EAST END OF W. APPROACH PAVEMENT & BK. W. ABUT.	360+60.15	-8.67	641.77	641.77
C.L. BRG. W. ABUT.	360+61.50	-8.67	641.78	641.78
A	360+71.50	-8.67	641.83	641.90
B	360+81.50	-8.67	641.88	642.01
C	360+91.50	-8.67	641.92	642.10
D	361+01.50	-8.67	641.95	642.17
E	361+11.50	-8.67	641.97	642.22
F	361+21.50	-8.67	641.99	642.25
G	361+31.50	-8.67	642.00	642.24
H	361+41.50	-8.67	642.00	642.22
I	361+51.50	-8.67	641.99	642.18
J	361+61.50	-8.67	641.97	642.12
K	361+71.50	-8.67	641.94	642.06
L	361+81.50	-8.67	641.91	641.98
M	361+91.50	-8.67	641.87	641.90
N	362+01.50	-8.67	641.81	641.83
C.L. PIER	362+11.50	-8.67	641.76	641.76
O	362+21.50	-8.67	641.69	641.70
P	362+31.50	-8.67	641.61	641.65
Q	362+41.50	-8.67	641.53	641.60
R	362+51.50	-8.67	641.44	641.55
S	362+61.50	-8.67	641.34	641.49
T	362+71.50	-8.67	641.22	641.42
U	362+81.50	-8.67	641.11	641.34
V	362+91.50	-8.67	641.00	641.24
W	363+01.50	-8.67	640.89	641.14
X	363+11.50	-8.67	640.78	641.02
Y	363+21.50	-8.67	640.66	640.88
Z	363+31.50	-8.67	640.55	640.74
AA	363+41.50	-8.67	640.44	640.57
AB	363+51.50	-8.67	640.33	640.40
C.L. BRG. E. ABUT.	363+61.50	-8.67	640.22	640.22
WEST END OF E. APPROACH PAVEMENT & BK. E. ABUT.	363+62.85	-8.67	640.21	640.21

GIRDER 7

Location	Station	Offset*	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
EAST END OF W. APPROACH PAVEMENT & BK. W. ABUT.	360+56.78	-17.00	641.58	641.58
C.L. BRG. W. ABUT.	360+58.13	-17.00	641.59	641.59
A	360+68.13	-17.00	641.65	641.71
B	360+78.13	-17.00	641.70	641.83
C	360+88.13	-17.00	641.74	641.92
D	360+98.13	-17.00	641.78	641.99
E	361+08.13	-17.00	641.80	642.04
F	361+18.13	-17.00	641.82	642.07
G	361+28.13	-17.00	641.83	642.07
H	361+38.13	-17.00	641.83	642.06
I	361+48.13	-17.00	641.82	642.02
J	361+58.13	-17.00	641.81	641.96
K	361+68.13	-17.00	641.78	641.90
L	361+78.13	-17.00	641.75	641.83
M	361+88.13	-17.00	641.71	641.75
N	361+98.13	-17.00	641.67	641.68
C.L. PIER	362+08.13	-17.00	641.61	641.61
O	362+18.13	-17.00	641.55	641.56
P	362+28.13	-17.00	641.47	641.51
Q	362+38.13	-17.00	641.39	641.46
R	362+48.13	-17.00	641.30	641.42
S	362+58.13	-17.00	641.21	641.36
T	362+68.13	-17.00	641.10	641.30
U	362+78.13	-17.00	640.98	641.21
V	362+88.13	-17.00	640.87	641.11
W	362+98.13	-17.00	640.76	641.01
X	363+08.13	-17.00	640.65	640.89
Y	363+18.13	-17.00	640.54	640.75
Z	363+28.13	-17.00	640.42	640.61
AA	363+38.13	-17.00	640.31	640.44
AB	363+48.13	-17.00	640.20	640.27
C.L. BRG. E. ABUT.	363+58.13	-17.00	640.09	640.09
WEST END OF E. APPROACH PAVEMENT & BK. E. ABUT.	363+59.48	-17.00	640.08	640.08

GIRDER 8

Location	Station	Offset*	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
EAST END OF W. APPROACH PAVEMENT & BK. W. ABUT.	360+53.42	25.33	641.39	641.39
C.L. BRG. W. ABUT.	360+54.77	-25.33	641.40	641.40
A	360+64.77	-25.33	641.46	641.53
B	360+74.77	-25.33	641.52	641.64
C	360+84.77	-25.33	641.56	641.74
D	360+94.77	-25.33	641.60	641.82
E	361+04.77	-25.33	641.63	641.87
F	361+14.77	-25.33	641.65	641.90
G	361+24.77	-25.33	641.66	641.91
H	361+34.77	-25.33	641.66	641.89
I	361+44.77	-25.33	641.66	641.86
J	361+54.77	-25.33	641.65	641.80
K	361+64.77	-25.33	641.63	641.74
L	361+74.77	25.33	641.60	641.67
M	361+84.77	-25.33	641.56	641.60
N	361+94.77	-25.33	641.52	641.53
C.L. PIER	362+04.77	-25.33	641.46	641.46
O	362+14.77	-25.33	641.40	641.41
P	362+24.77	25.33	641.33	641.37
Q	362+34.77	-25.33	641.25	641.33
R	362+44.77	-25.33	641.17	641.28
S	362+54.77	-25.33	641.07	641.23
T	362+64.77	-25.33	640.97	641.17
U	362+74.77	-25.33	640.85	641.08
V	362+84.77	-25.33	640.74	640.99
W	362+94.77	-25.33	640.63	640.88
X	363+04.77	-25.33	640.52	640.76
Y	363+14.77	-25.33	640.41	640.62
Z	363+24.77	-25.33	640.30	640.48
AA	363+34.77	-25.33	640.18	640.31
AB	363+44.77	-25.33	640.07	640.14
C.L. BRG. E. ABUT.	363+54.77	-25.33	639.96	639.96
WEST END OF E. APPROACH PAVEMENT & BK. E. ABUT.	363+56.11	25.33	639.95	639.95

* A positive value indicates a left offset from the centerline of 95th street. A negative value indicates a right offset.

FILE NAME = c:\waddah\285367381\95th\Drawings\contract\contract\structure\Bridges\Top of Deck Elevation - 5.dwg



USER NAME = *STB*	DESIGNED - STB	REVISIONS
CHECKED - NPP	DRAWN - SOI	REVISIONS
PLOT SCALE = 1/8" = 1'-0"	CHECKED - NPP	REVISIONS
PLOT DATE = 10/16/2012		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS - 5
STRUCTURE NO. 099-3035
SHEET NO. 8 OF 38 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1644	01-30181-00-FF	WILL	328	166
CONTRACT NO. 63647			ILLINOIS FED. AID PROJECT	

GIRDER 9

Location	Station	Offset*	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
EAST END OF W. APPROACH PAVEMENT & BK. W. ABUT.	360+50.05	-33.67	641.20	641.20
C.L. BRG. W. ABUT.	360+51.40	-33.67	641.21	641.21
A	360+61.40	-33.67	641.28	641.34
B	360+71.40	-33.67	641.33	641.46
C	360+81.40	-33.67	641.38	641.56
D	360+91.40	-33.67	641.42	641.64
E	361+01.40	-33.67	641.45	641.69
F	361+11.40	-33.67	641.47	641.73
G	361+21.40	-33.67	641.49	641.74
H	361+31.40	-33.67	641.50	641.72
I	361+41.40	-33.67	641.50	641.69
J	361+51.40	-33.67	641.49	641.64
K	361+61.40	-33.67	641.47	641.58
L	361+71.40	-33.67	641.44	641.51
M	361+81.40	-33.67	641.41	641.45
N	361+91.40	-33.67	641.37	641.38
C.L. PIER	362+01.40	-33.67	641.31	641.31
O	362+11.40	-33.67	641.26	641.27
P	362+21.40	-33.67	641.19	641.23
Q	362+31.40	-33.67	641.11	641.19
R	362+41.40	-33.67	641.03	641.14
S	362+51.40	-33.67	640.94	641.09
T	362+61.40	-33.67	640.84	641.04
U	362+71.40	-33.67	640.72	640.95
V	362+81.40	-33.67	640.61	640.86
W	362+91.40	-33.67	640.50	640.75
X	363+01.40	-33.67	640.39	640.63
Y	363+11.40	-33.67	640.28	640.50
Z	363+21.40	-33.67	640.17	640.35
AA	363+31.40	-33.67	640.06	640.18
AB	363+41.40	-33.67	639.94	640.01
C.L. BRG. E. ABUT.	363+51.40	-33.67	639.83	639.83
WEST END OF E. APPROACH PAVEMENT & BK. E. ABUT.	363+52.74	-33.67	639.82	639.82

GIRDER 10

Location	Station	Offset*	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
EAST END OF W. APPROACH PAVEMENT & BK. W. ABUT.	360+46.68	-42.00	641.01	641.01
C.L. BRG. W. ABUT.	360+48.03	-42.00	641.02	641.02
A	360+58.03	-42.00	641.09	641.16
B	360+68.03	-42.00	641.15	641.27
C	360+78.03	-42.00	641.20	641.38
D	360+88.03	-42.00	641.24	641.46
E	360+98.03	-42.00	641.28	641.52
F	361+08.03	-42.00	641.30	641.56
G	361+18.03	-42.00	641.32	641.56
H	361+28.03	-42.00	641.33	641.55
I	361+38.03	-42.00	641.33	641.53
J	361+48.03	-42.00	641.32	641.48
K	361+58.03	-42.00	641.31	641.42
L	361+68.03	-42.00	641.29	641.36
M	361+78.03	-42.00	641.25	641.29
N	361+88.03	-42.00	641.21	641.23
C.L. PIER	361+98.03	-42.00	641.17	641.17
O	362+08.03	-42.00	641.11	641.12
P	362+18.03	-42.00	641.05	641.08
Q	362+28.03	-42.00	640.97	641.05
R	362+38.03	-42.00	640.89	641.01
S	362+48.03	-42.00	640.80	640.96
T	362+58.03	-42.00	640.71	640.90
U	362+68.03	-42.00	640.60	640.83
V	362+78.03	-42.00	640.48	640.73
W	362+88.03	-42.00	640.37	640.63
X	362+98.03	-42.00	640.26	640.50
Y	363+08.03	-42.00	640.15	640.37
Z	363+18.03	-42.00	640.04	640.22
AA	363+28.03	-42.00	639.93	640.05
AB	363+38.03	-42.00	639.81	639.88
C.L. BRG. E. ABUT.	363+48.03	-42.00	639.70	639.70
WEST END OF E. APPROACH PAVEMENT & BK. E. ABUT.	363+49.38	-42.00	639.69	639.69

SOUTH EDGE OF DECK

Location	Station	Offset*	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
EAST END OF W. APPROACH PAVEMENT & BK. W. ABUT.	360+45.27	-45.50	640.93	640.93
C.L. BRG. W. ABUT.	360+46.62	-45.50	640.94	640.94
A	360+56.62	-45.50	641.01	641.08
B	360+66.62	-45.50	641.07	641.20
C	360+76.62	-45.50	641.12	641.30
D	360+86.62	-45.50	641.17	641.38
E	360+96.62	-45.50	641.20	641.44
F	361+06.62	-45.50	641.23	641.48
G	361+16.62	-45.50	641.25	641.49
H	361+26.62	-45.50	641.26	641.48
I	361+36.62	-45.50	641.26	641.46
J	361+46.62	-45.50	641.25	641.41
K	361+56.62	-45.50	641.24	641.36
L	361+66.62	-45.50	641.22	641.29
M	361+76.62	-45.50	641.19	641.23
N	361+86.62	-45.50	641.15	641.17
C.L. PIER	361+96.62	-45.50	641.10	641.10
O	362+06.62	-45.50	641.05	641.06
P	362+16.62	-45.50	640.99	641.02
Q	362+26.62	-45.50	640.91	640.99
R	362+36.62	-45.50	640.83	640.95
S	362+46.62	-45.50	640.75	640.90
T	362+56.62	-45.50	640.65	640.85
U	362+66.62	-45.50	640.55	640.77
V	362+76.62	-45.50	640.43	640.67
W	362+86.62	-45.50	640.32	640.57
X	362+96.62	-45.50	640.20	640.45
Y	363+06.62	-45.50	640.09	640.31
Z	363+16.62	-45.50	639.98	640.16
AA	363+26.62	-45.50	639.87	640.00
AB	363+36.62	-45.50	639.76	639.83
C.L. BRG. E. ABUT.	363+46.62	-45.50	639.65	639.65
WEST END OF E. APPROACH PAVEMENT & BK. E. ABUT.	363+47.97	-45.50	639.63	639.63

* A positive value indicates a left offset from the centerline of 95th street. A negative value indicates a right offset.

FILE NAME = c:\vesdoh\203307381\95th\sub\winga\con\brack-south\Structure\Bridg\01\Pages\Civil\099999\Top of Deck Elevation - 6.dgn



100 S. WACKER DR.
SUITE 500
CHICAGO, IL 60606
TEL (312) 939-1000
FAX (312) 939-4190

USER NAME = SSTB#
DESIGNED - STB
CHECKED - NPP
DRAWN - SOI
CHECKED - NPP
PLOT SCALE = 3/8" = 1' / IN.
PLOT DATE = 10/16/2012

DESIGNED - STB
CHECKED - NPP
DRAWN - SOI
CHECKED - NPP
REVISIONS

REVISIONS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS - 6
STRUCTURE NO. 099-3035

SHEET NO. 9 OF 38 SHEETS

F.A.U. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1644	01-00181-00-PP	WILL	328	167
ILLINOIS FED. AID PROJECT			CONTRACT NO. 63647	

NORTH EDGE OF SLAB

Location	Station	Offset*	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
BEGIN WEST END OF W. APPROACH PAVEMENT	360+35.74	30.00	641.17	641.17
A1	360+45.74	30.00	641.25	641.25
A2	360+55.74	30.00	641.31	641.31
A3	360+65.74	30.00	641.38	641.38
EAST END OF W. APPROACH PAVEMENT & BK. W. ABUT.	360+75.74	30.00	641.43	641.43

PROFILE GRADE WB

Location	Station	Offset*	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
BEGIN WEST END OF W. APPROACH PAVEMENT	360+24.86	3.00	641.62	641.62
A1	360+34.86	3.00	641.70	641.70
A2	360+44.86	3.00	641.78	641.78
A3	360+54.86	3.00	641.85	641.85
EAST END OF W. APPROACH PAVEMENT & BK. W. ABUT.	360+64.86	3.00	641.91	641.91

95TH STREET & CROWN

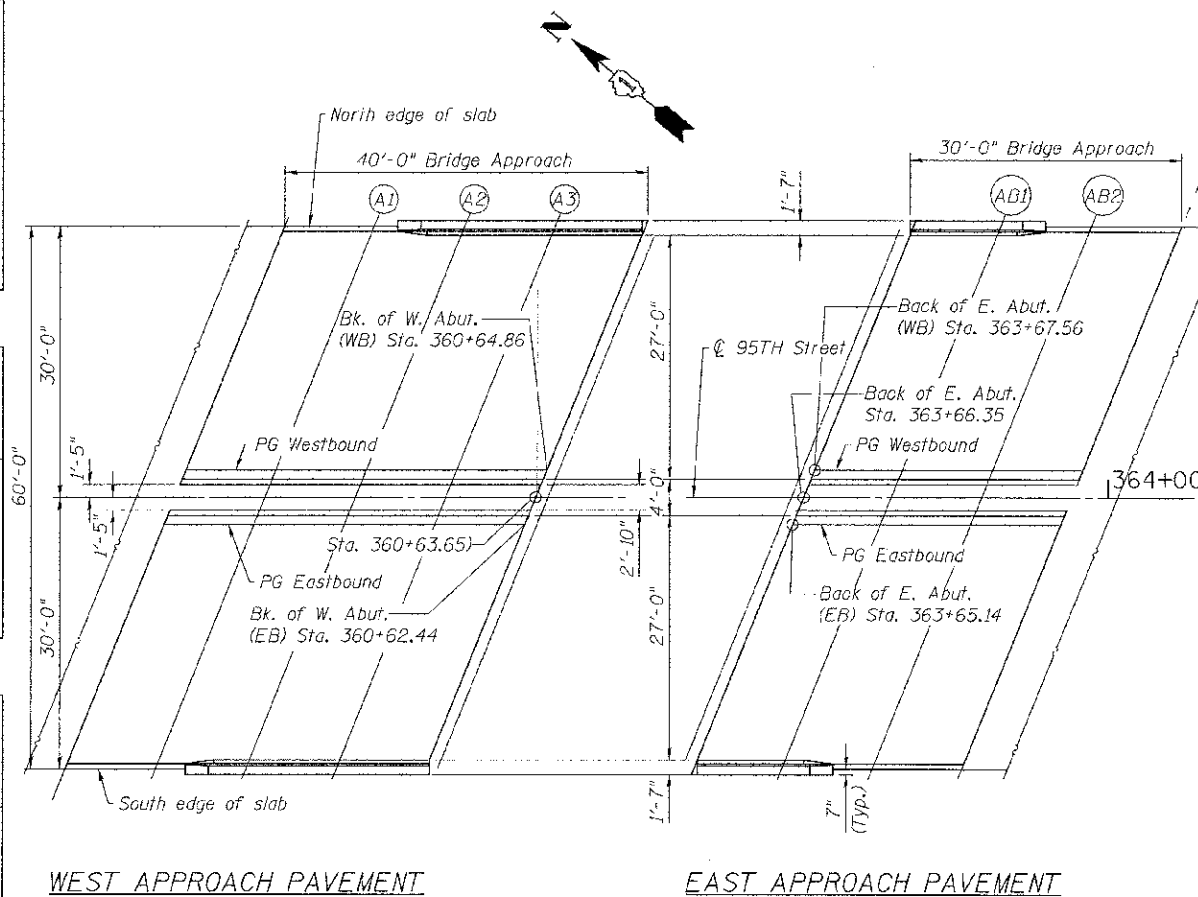
Location	Station	Offset*	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
BEGIN WEST END OF W. APPROACH PAVEMENT	360+23.65	0.00	641.67	641.67
A1	360+33.65	0.00	641.75	641.75
A2	360+43.65	0.00	641.83	641.83
A3	360+53.65	0.00	641.90	641.90
EAST END OF W. APPROACH PAVEMENT & BK. W. ABUT.	360+63.65	0.00	641.96	641.96

PROFILE GRADE EB

Location	Station	Offset*	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
BEGIN WEST END OF W. APPROACH PAVEMENT	360+22.44	-3.00	641.59	641.59
A1	360+32.44	-3.00	641.68	641.68
A2	360+42.44	-3.00	641.76	641.76
A3	360+52.44	-3.00	641.83	641.83
EAST END OF W. APPROACH PAVEMENT & BK. W. ABUT.	360+62.44	-3.00	641.90	641.90

SOUTH EDGE OF SLAB

Location	Station	Offset*	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
BEGIN WEST END OF W. APPROACH PAVEMENT	360+11.56	-30.00	640.96	640.96
A1	360+21.56	-30.00	641.05	641.05
A2	360+31.56	-30.00	641.13	641.13
A3	360+41.56	-30.00	641.21	641.21
EAST END OF W. APPROACH PAVEMENT & BK. W. ABUT.	360+51.56	-30.00	641.29	641.29



NORTH EDGE OF SLAB

Location	Station	Offset*	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
WEST END OF E. APPROACH PAVEMENT & BK. E. ABUT.	363+78.44	30.00	639.61	639.61
AB1	363+88.44	30.00	639.49	639.49
AB2	363+98.44	30.00	639.38	639.38
EAST END OF E. APPROACH PAVEMENT	364+08.44	30.00	639.27	639.27

PROFILE GRADE WB

Location	Station	Offset*	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
WEST END OF E. APPROACH PAVEMENT & BK. E. ABUT.	363+67.56	3.00	640.27	640.27
AB1	363+77.56	3.00	640.16	640.16
AB2	363+87.56	3.00	640.05	640.05
EAST END OF E. APPROACH PAVEMENT	363+97.56	3.00	639.93	639.93

95TH STREET & CROWN

Location	Station	Offset*	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
WEST END OF E. APPROACH PAVEMENT & BK. E. ABUT.	363+66.35	0.00	640.34	640.34
AB1	363+76.35	0.00	640.23	640.23
AB2	363+86.35	0.00	640.12	640.12
EAST END OF E. APPROACH PAVEMENT	363+96.35	0.00	640.01	640.01

PROFILE GRADE EB

Location	Station	Offset*	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
WEST END OF E. APPROACH PAVEMENT & BK. E. ABUT.	363+65.14	-3.00	640.29	640.29
AB1	363+75.14	-3.00	640.18	640.18
AB2	363+85.14	-3.00	640.07	640.07
EAST END OF E. APPROACH PAVEMENT	363+95.14	-3.00	639.96	639.96

SOUTH EDGE OF SLAB

Location	Station	Offset*	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
WEST END OF E. APPROACH PAVEMENT & BK. E. ABUT.	363+54.26	-30.00	639.88	639.88
AB1	363+64.26	-30.00	639.76	639.76
AB2	363+74.26	-30.00	639.65	639.65
EAST END OF E. APPROACH PAVEMENT	363+84.26	-30.00	639.54	639.54

* A Positive value indicates a left offset from the centerline of 95th. street, A negative value indicates a right offset

FILE NAME = g:\wcbh\25367361\95th\Drawings\contract\north\Structures\Bridges (D)\Page\Control Copy\15-18_Top of Approach Slab Elevations.dgn



USER NAME = #STB#	DESIGNED - STB	REVISD -
PLCT SCALE = 1/2" = 1' / IN.	CHECKED - NPP	REVISD -
PLCT DATE = 10/16/2012	DRAWN - SOI	REVISD -
	CHECKED - NPP	REVISD -

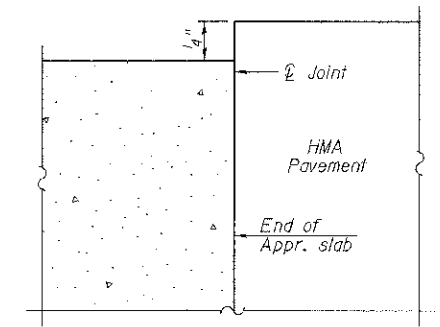
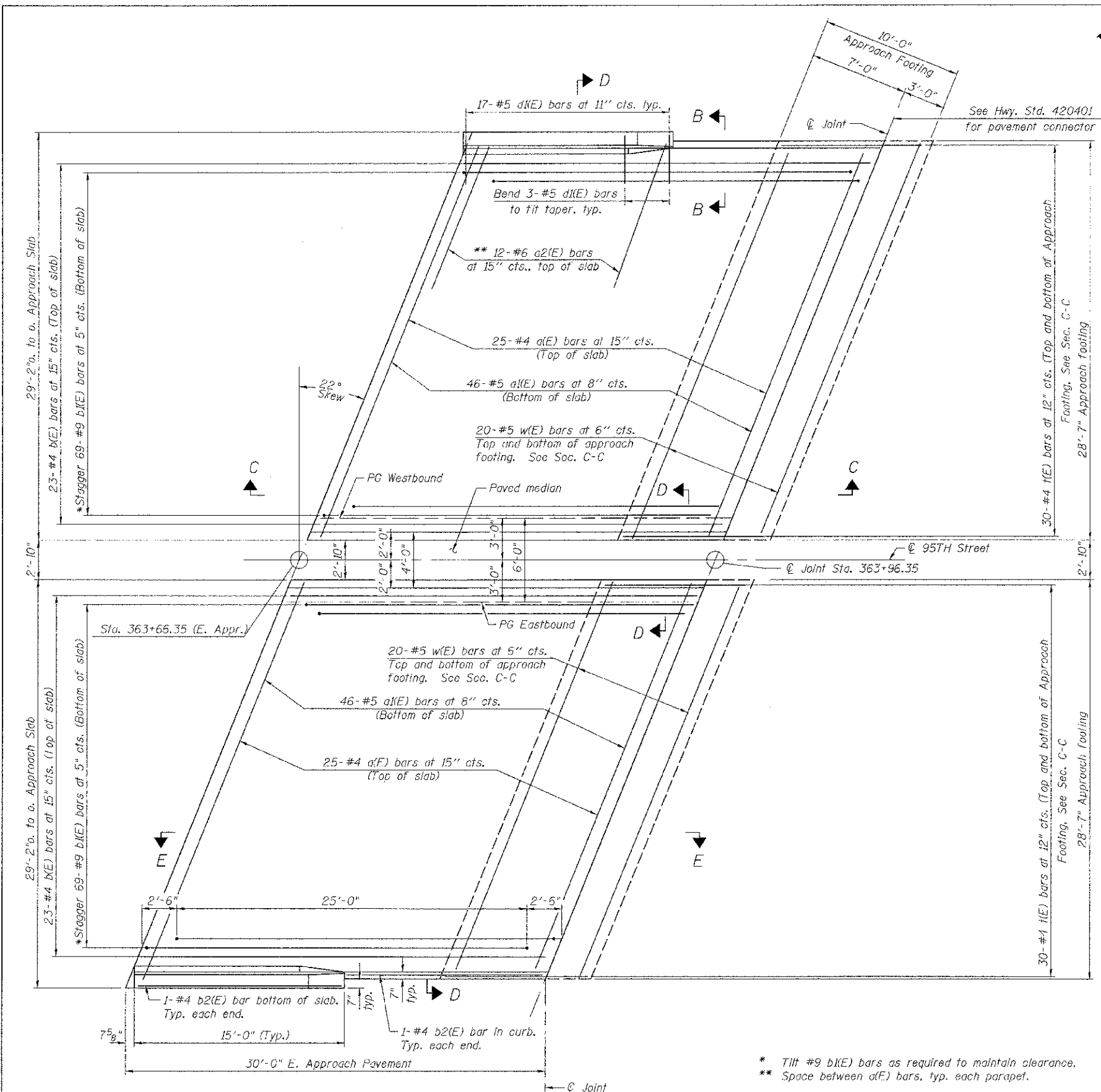
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TOP OF APPROACH SLAB ELEVATIONS
STRUCTURE NO. 099-3035**

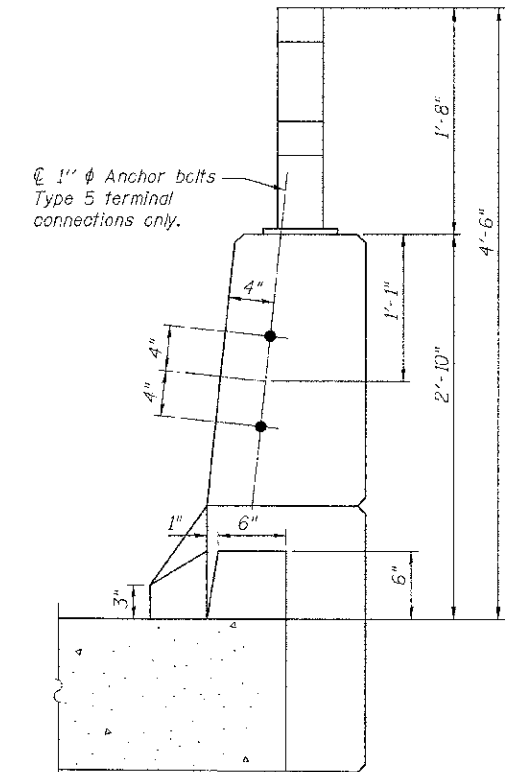
F.A.U. RTE. 1644	SECTION 01-00181-00-FP	COUNTY WIL.	TOTAL SHEETS 328	SHEET NO. 168
CONTRACT NO. 63647			ILLINOIS FED. AID PROJECT	

SHEET NO. 10 OF 38 SHEETS

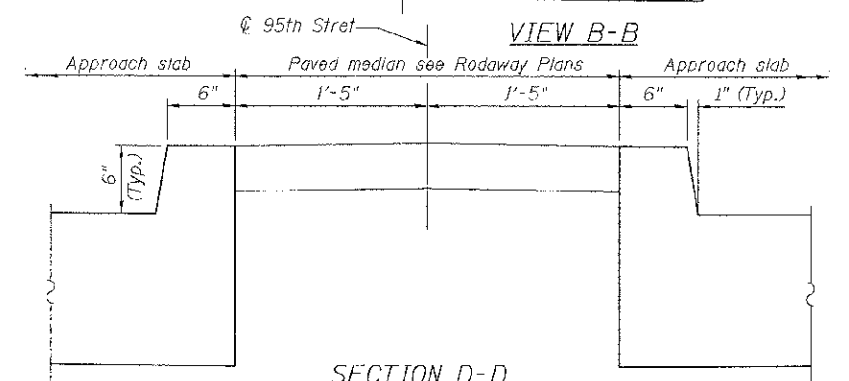
Notes:
 See East Approach Slab Details sheet for Sections C-C & D-D, View E-E and location for Detail A. a(E) and b(E) bar spacings measured along C Rdwy.



FLEXIBLE PAVEMENT
 DETAIL A



VIEW B-B



SECTION D-D

* Tilt #9 b(E) bars as required to maintain clearance.
 ** Space between a(E) bars, typ. each parapet.

PLAN

BA-L 7-1-10



100 S. WACKER DR. SUITE 500 CHICAGO, IL 60606 TEL (312) 539-4000 FAX (312) 939-4138	USER NAME = #STB#	DESIGNED - STB	REVISED -
		CHECKED - NFP	REVISED -
	PLOT SCALE - 1/4" = 1'-0"	DRAWN - SOI	REVISED -
	PILOT DATE = 12/16/2012	CHECKED - NFP	REVISED -

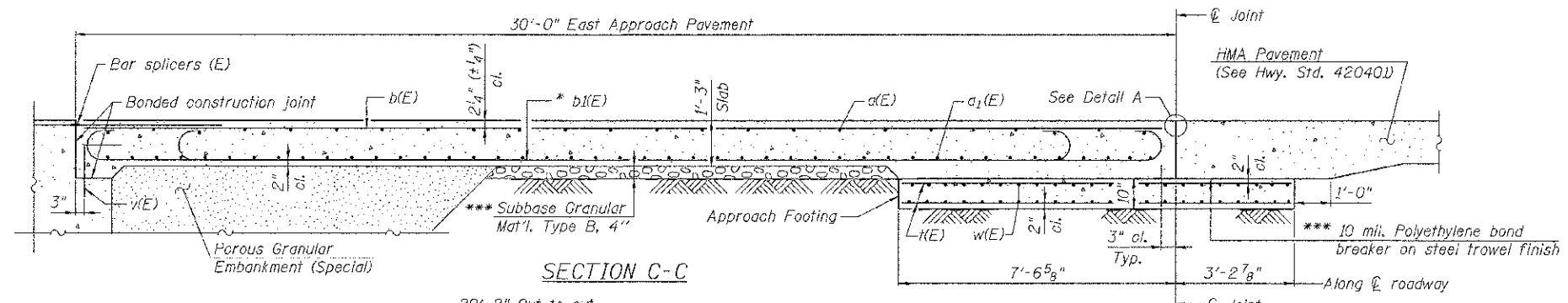
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EAST APPROACH SLAB
 STRUCTURE NO. 099-3035

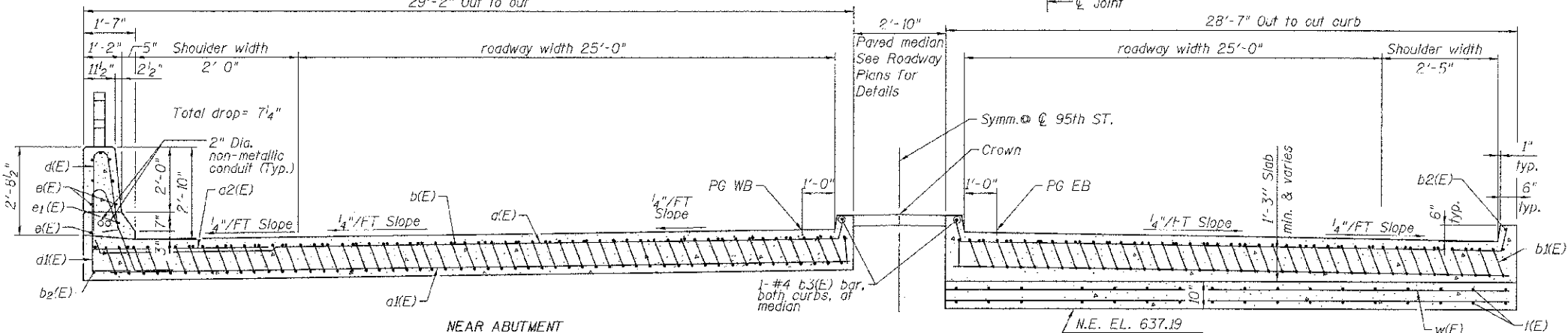
SHEET NO. 11 OF 38 SHEETS

F.A.U. RT. 1644	SECTION 01-CC181-00-FP	COUNTY WILL	TOTAL SHEETS 328	SHEET NO. 169
				CONTRACT NO. 63647
ILLINOIS FED. AID PROJECT				

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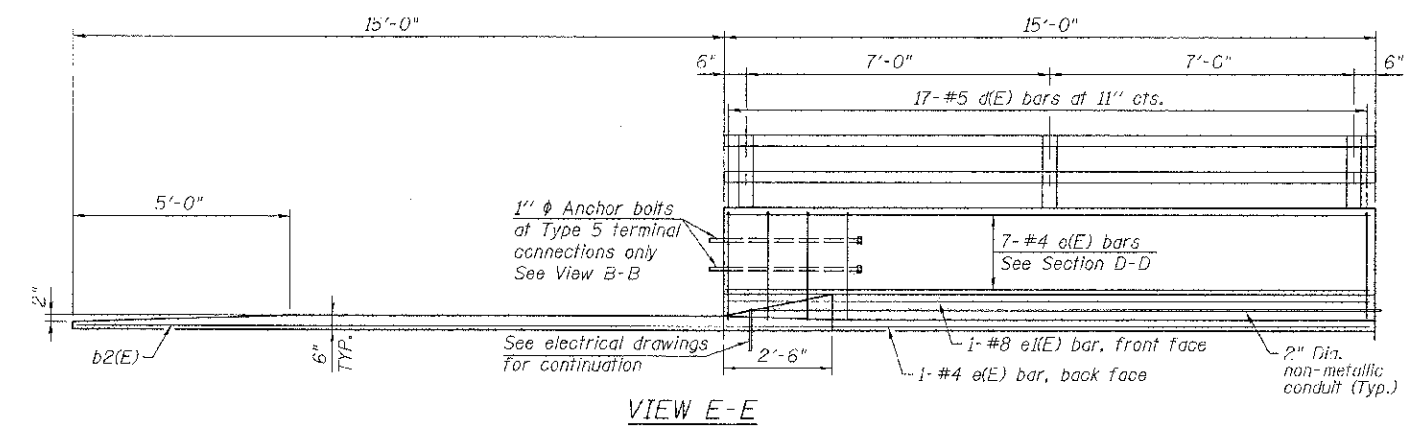


Notes:
 See East Approach Slab Sheet for Detail A and locations of Sections C-C, D-D, and E-E.
 Approach slab and parapet concrete shall be paid for as Concrete Superstructure.
 Approach footing concrete shall be paid for as Concrete Structures.
 Reinforcement shall be paid for as Reinforcement Bars, Epoxy Coated.
 For v(E) bar details, see sheet Superstructure Details 2.
 The approach footing maximum applied service bearing pressure (Q_{max}) = 2.0 ksf.
 For bar splicer details, see Bar Splicer Details sheet.
 Cost of excavation for approach footing included with Concrete Structures.
 For Porous Granular Embankment (Special) and drainage treatment details, see Abutment Detail sheets.
 For additional parapet details, see Superstructure Detail sheet.
 See Bicycle railing sheet for parapet Railing Details.

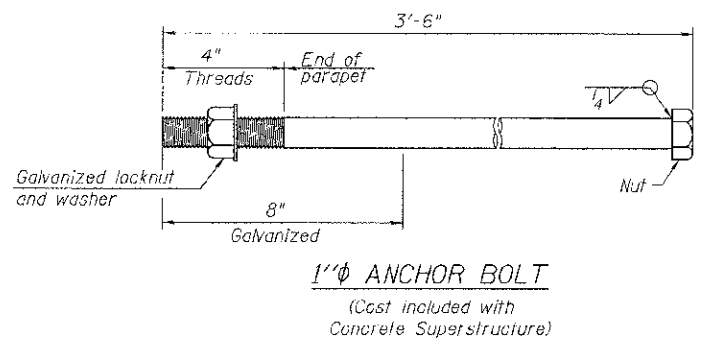


* Tilt #9 b(E) bars as required to maintain clearance.
 ** The cost of expansion anchors/inserts is included in the cost of Reinforcement Bars, Epoxy Coated.
 *** Cost included with Concrete Superstructure.
 **** Full width along joint - Backer rods not required.

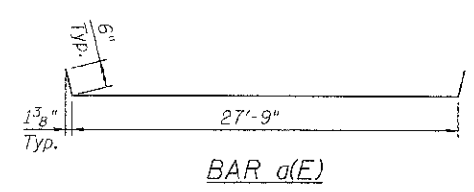
SECTION D-D
 (See Plan for dimensions not shown)



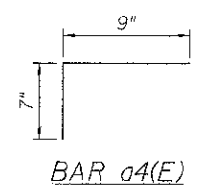
VIEW E-E



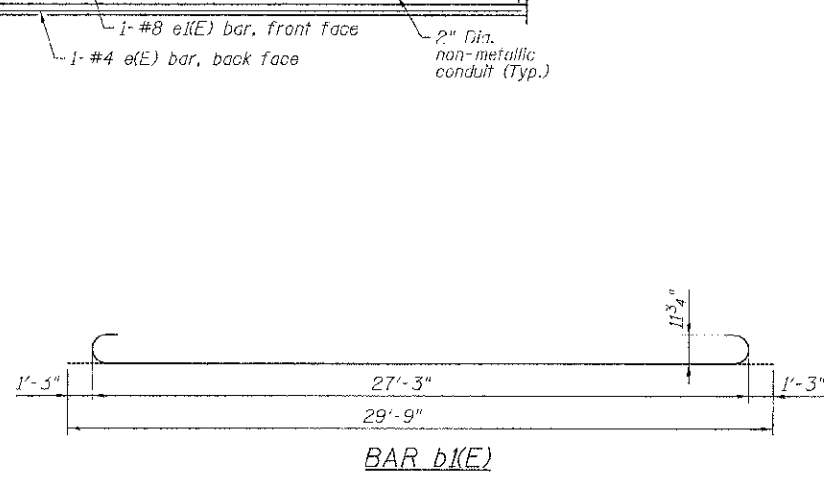
1" ANCHOR BOLT
 (Cost included with Concrete Superstructure)



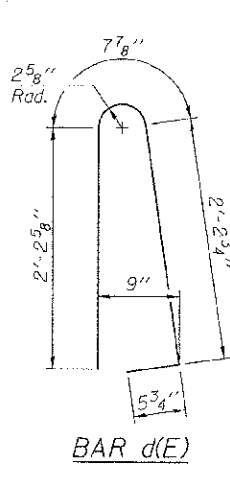
BAR a(E)



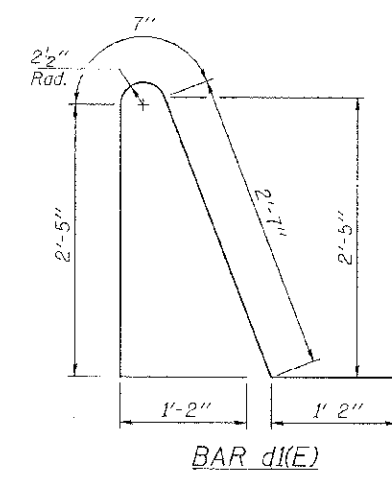
BAR a4(E)



BAR b(E)



BAR d(E)



BAR d1(E)

EAST APPROACH
 BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	50	#4	28'-9"	
c1(E)	92	#5	28'-3"	
a2(E)	24	#6	6'-6"	
b(E)	46	#4	29'-8"	
b1(E)	138	#9	29'-5"	
b2(E)	4	#4	14'-8"	
b3(E)	2	#5	29'-8"	
d(E)	34	#5	5'-7"	
d1(E)	34	#5	7'-11"	
e(E)	16	#4	14'-8"	
e1(E)	2	#8	14'-8"	
k(E)	120	#4	10'-5"	
v(E)	80	#5	30'-4"	
Concrete Superstructure	Cu. Yd.		92.7	
Concrete Structures	Cu. Yd.		19.0	
Reinforcement Bars, Epoxy Coated	Pound		22,960	
Bridge Deck Grooving	Sq. Yd.		167	
Protective Coat	Sq. Yd.		203	

Reinforcement bar designated (E) shall be epoxy coated.
 Bars indicated thus 20X3 - #5 etc. indicates 20 lines of bars with 3 lengths per line.

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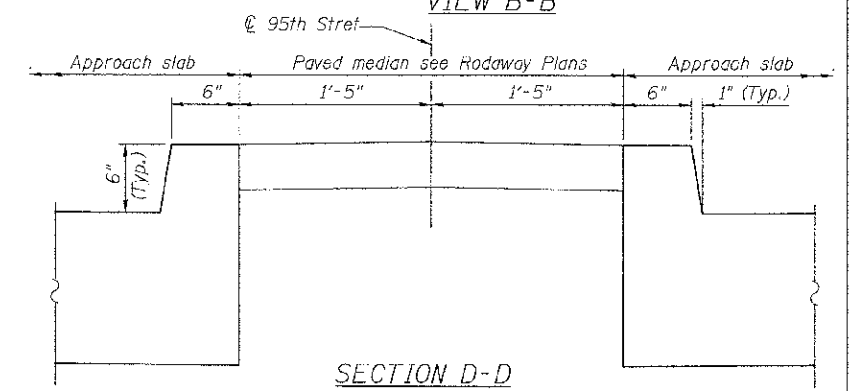
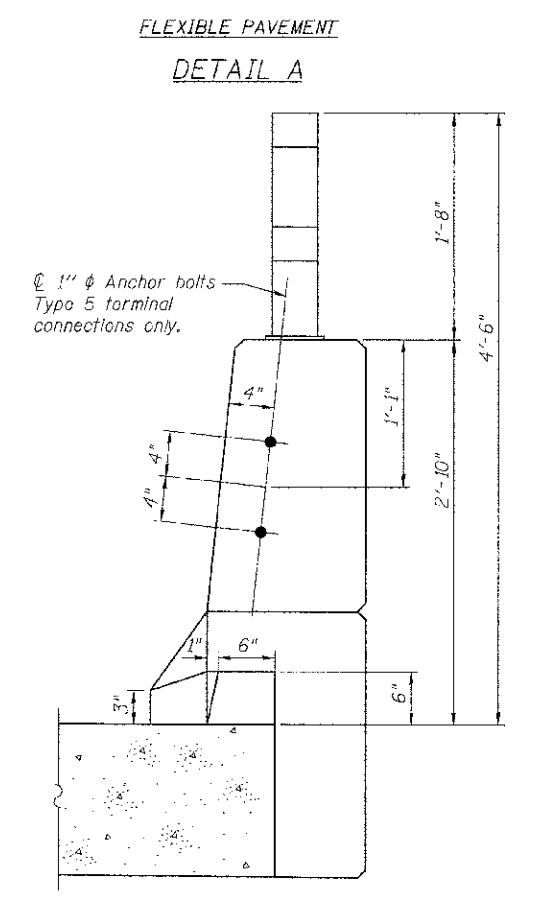
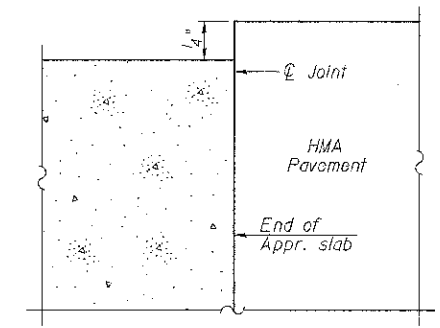
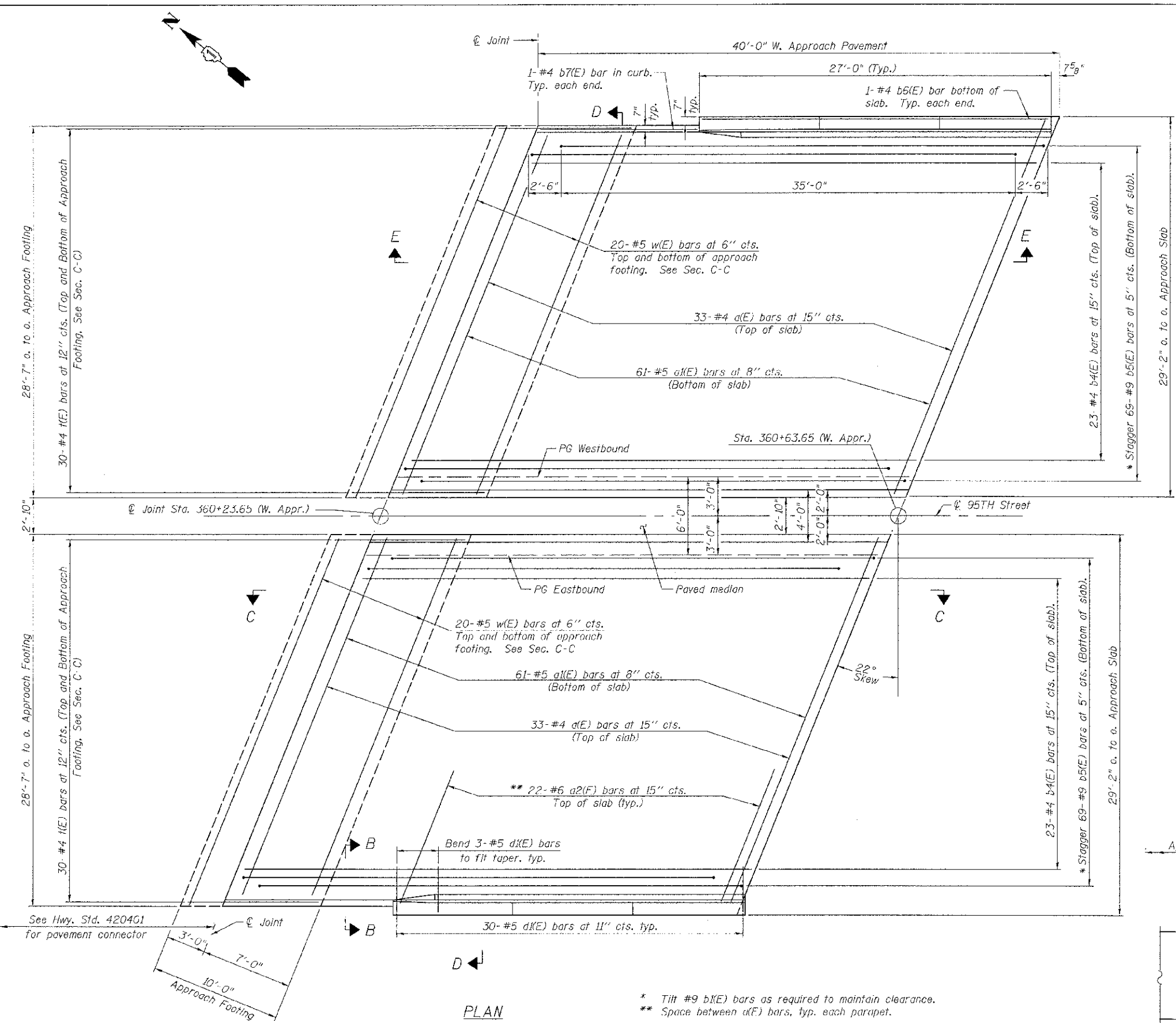
URS 100 S. WACKER DR., SUITE 500 CHICAGO, IL 60606 TEL (312) 939-4000 FAX (312) 939-4998	USER NAME = STB* PLOT SCALE = 2x1 1/2" / IN. PLOT DATE = 10/16/2012	DESIGNED - STB CHECKED - NPP DRAWN - SOI CHECKED - NPP	REVISED - REVISED - REVISED - REVISED -
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EAST APPROACH SLAB DETAILS
 STRUCTURE NO. 099-3035

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS
1644	01-00181-00-FP	WILL	328 / 170
			CONTRACT NO. 63647
ILLINOIS FED. AID PROJECT			

Notes:
See West Approach Slab Details sheet for Sections C-C & D-D, View E-E and location of Detail A. a(E) and a(E) bar spacings measured along \hat{C} Rdwy.



* Tilt #9 b(E) bars as required to maintain clearance.
** Space between a(E) bars, typ. each parapet.

FILE NAME: g:\wadol_26367381_95th\Drawings\Structure\Bridg\09-13 West approach slab.dgn

BA-L 7-1-10

URS
100 S.WACKER DR.
SUITE 500
CHICAGO IL 60606
TEL (312) 939-1000
FAX (312) 939-4938

USER NAME = SIB#
PLOT SCALE = 4x8 1/2" IN.
PLOT DATE = 10/16/2012

DESIGNED - STB
CHECKED - NPP
DRAWN - SOI
CHECKED - NPP

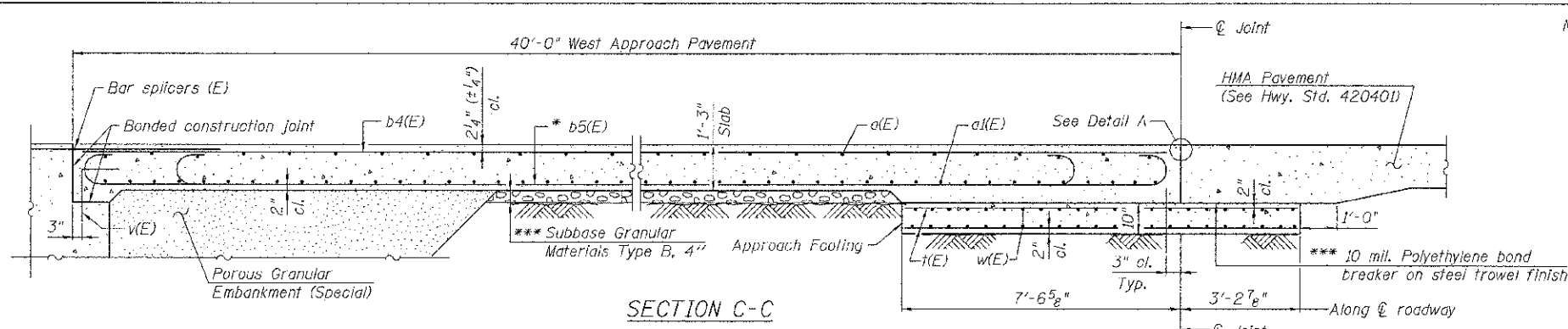
REVISED -
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

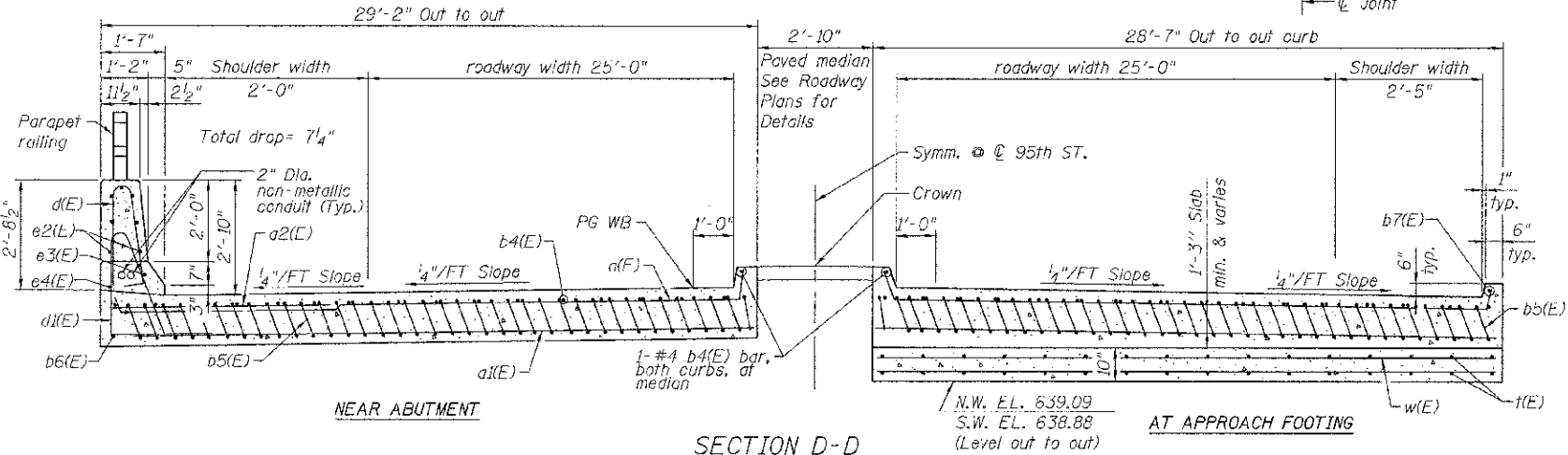
**WEST APPROACH SLAB
STRUCTURE NO. 099-3035**

SHEET NO. 13 OF 38 SHEETS

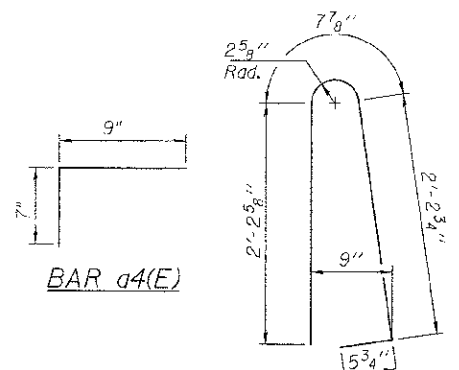
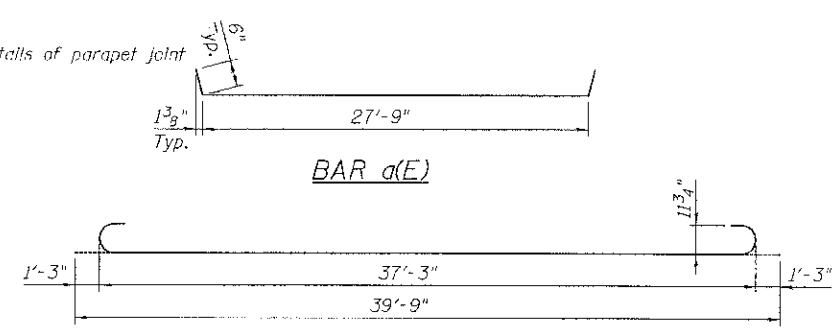
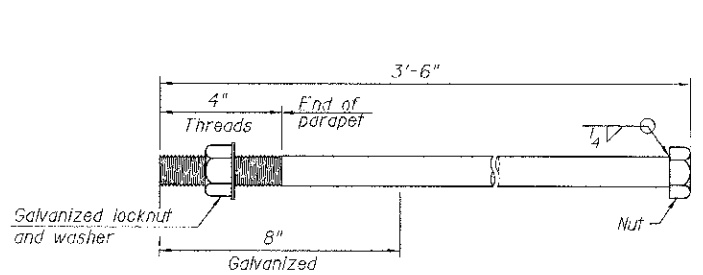
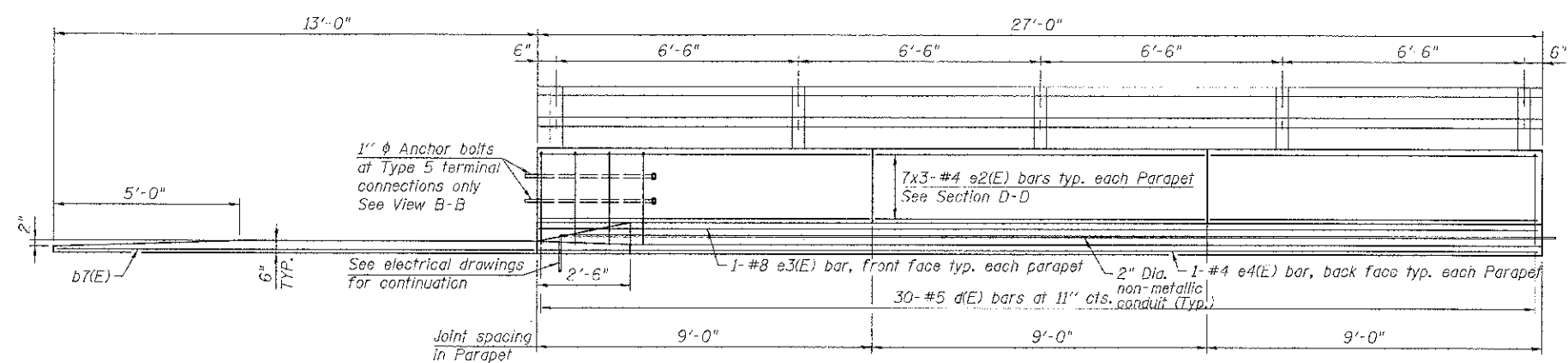
F.A.J. RTE. 1644	SECTION 01-001R1-00-FP	COUNTY WILL	TOTAL SHEETS 328	SHEET NO. 171
CONTRACT NO. 63647			ILLINOIS FED. AID PROJECT	



Notes:
 See West Approach Slab sheet for Detail A and locations of Sections C-C, D-D, C-C & View B-B
 Approach slab and parapet concrete shall be paid for as Concrete Superstructure.
 Approach footing concrete shall be paid for as Concrete Structures.
 Reinforcement shall be paid for as Reinforcement Bars, Epoxy Coated.
 For v(E) bar details, see sheet Superstructure Details 2.
 The approach footing maximum applied service bearing pressure (Qmax) = 2.0 ksf.
 For bar splicer details, see Bar Splicer Details sheet.
 Cost of excavation for approach footing included with Concrete Structures.
 For Porous Granular Embankment (Special) and drainage treatment details, see Abutment Detail sheets.
 For additional parapet details, see Superstructure Detail sheet.
 See Bicycle Railing sheet for parapet railing details.



- * Tilt #9 b1(E) bars as required to maintain clearance.
- ** The cost of expansion anchors/inserts is included in the cost of Reinforcement Bars, Epoxy Coated.
- *** Cost included with Concrete Superstructure.
- **** Full width along joint - Backer rods not required.

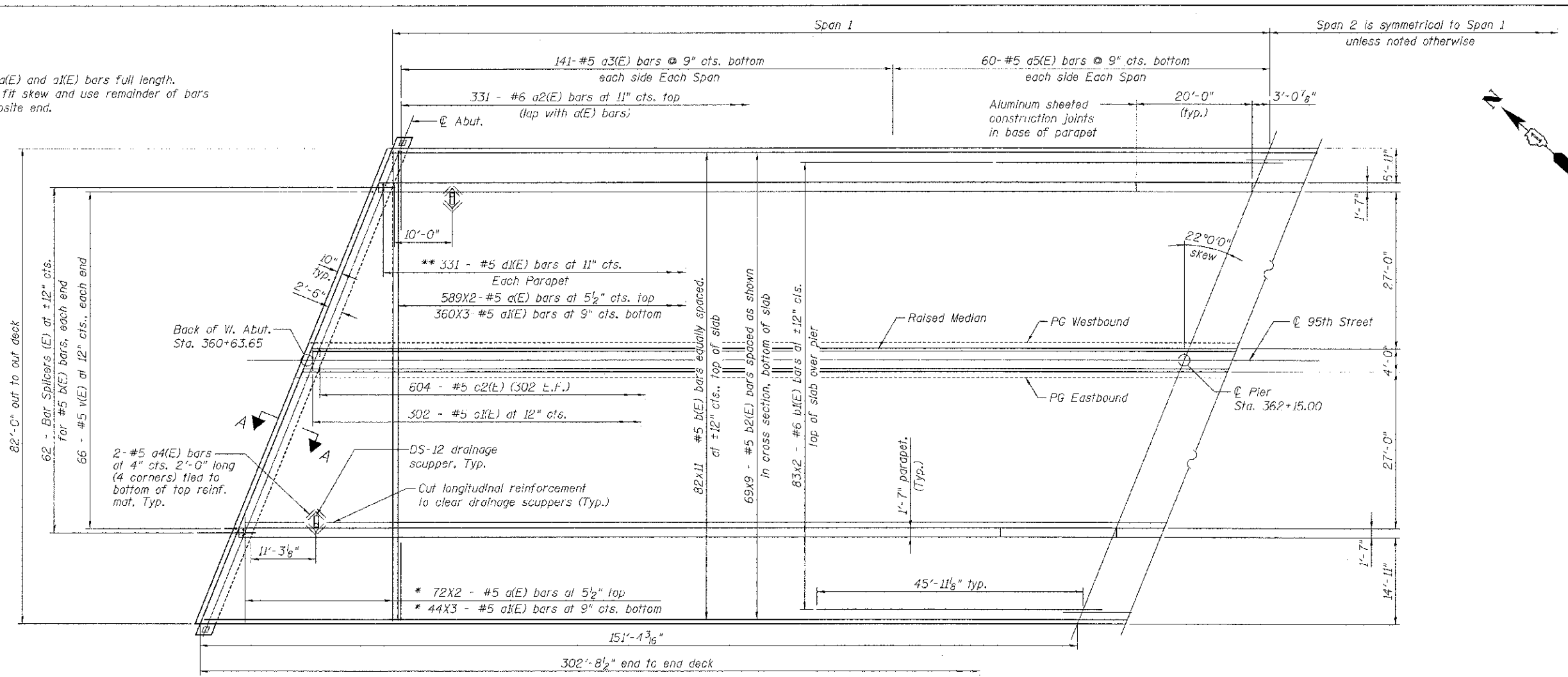


WEST APPROACH BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	66	#4	28'-9"	—
a1(E)	122	#5	28'-3"	—
a2(E)	44	#6	6'-6"	—
b4(E)	48	#4	39'-8"	—
b5(E)	138	#9	39'-9"	—
b6(E)	2	#4	26'-8"	—
b7(E)	2	#4	12'-8"	—
d(E)	60	#5	5'-1"	—
d1(E)	60	#5	7'-11"	—
e2(E)	42	#4	8'-8"	—
e3(E)	2	#8	26'-8"	—
e4(E)	2	#4	26'-8"	—
k(E)	120	#4	10'-5"	—
w(E)	80	#5	30'-4"	—
Concrete Superstructure	Cu. Yd.	123.1		
Concrete Structures	Cu. Yd.	19.0		
Reinforcement Bars, Epoxy Coated	Pound	29,900		
Bridge Deck Grooving	Sq. Yd.	222		
Protective Coat	Sq. Yd.	281		

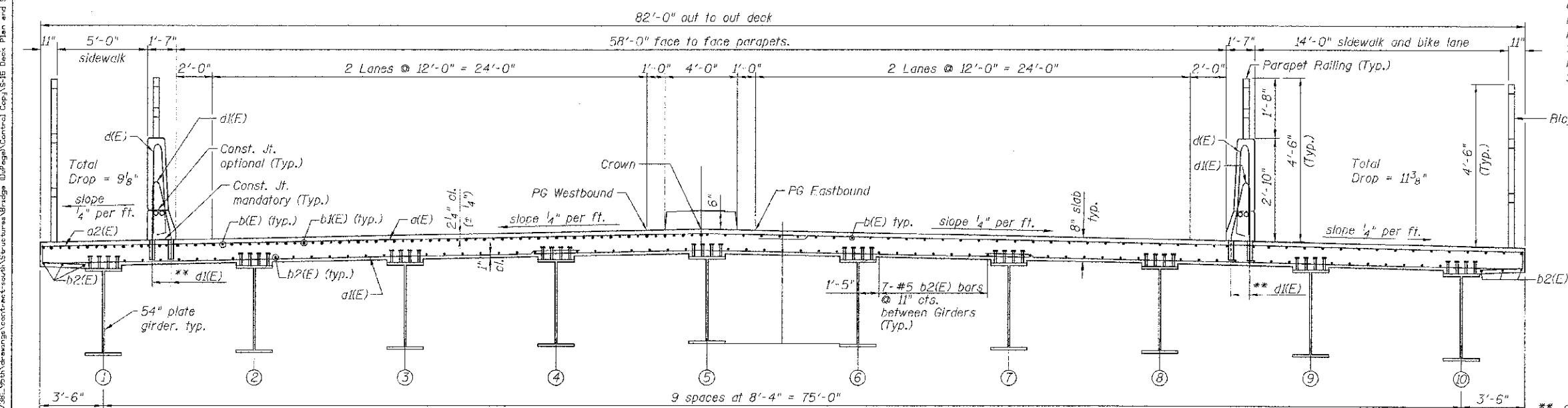
Reinforcement bar designated (E) shall be epoxy coated.

* Order a(E) and a1(E) bars full length.
Cut to fit skew and use remainder of bars
in opposite end.



HALF PLAN

Notes:
See superstructure detail sheet for additional details
and bill of material.
Bars indicated thus 20X3-#5 etc.
Indicates 20 lines of bars with 3 lengths per line.
See superstructure details sheet for parapet and raised
median reinforcement and section A-A.
See GF & E sheet for location of scuppers in Span 2.



CROSS SECTION
(N.T.S. LOOKING SOUTHEAST)

MINIMUM BAR LAP
(Deck Slab)
#5 bar = 2'-7"

** Core and set #5 d1(E) bar according to Article 509.06
of the Standard Specifications. Cored holes shall be
roughened or scored per manufacturer's recommendations.
Maximum depth of hole shall not exceed 6".



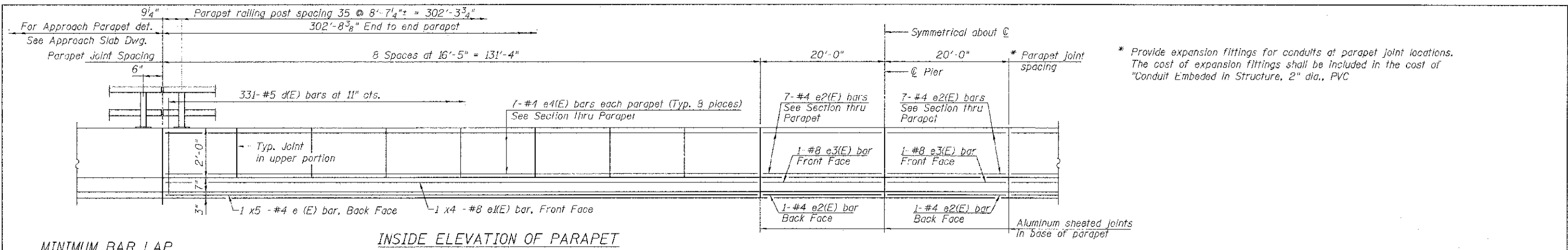
100 S. WACKER DR.
SUITE 500
CHICAGO, IL 60605
TEL (312) 939-1000
FAX (312) 939-4998

DESIGNED - STB	REVISD -
CHECKED - NPP	REVISD -
DRAWN - STB	REVISD -
CHECKED - NPP	REVISD -

STATE OF ILLINOIS
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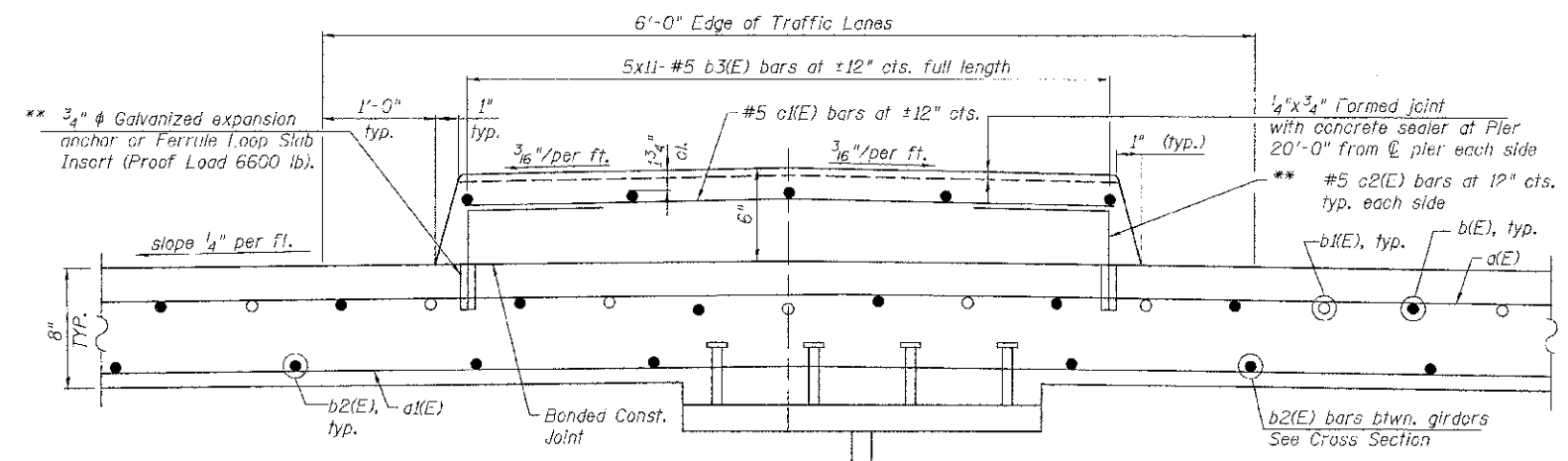
DECK PLAN AND SECTION
STRUCTURE NO. 099-3035
SHEET NO. 15 OF 38 SHEETS

F.A.U. RTE. 1644	SECTION 01-00181-00-FP	COUNTY WILL	TOTAL SHEETS 328	SHEET NO. 173
CONTRACT NO. 63647			ILLINOIS FED. AID PROJECT	



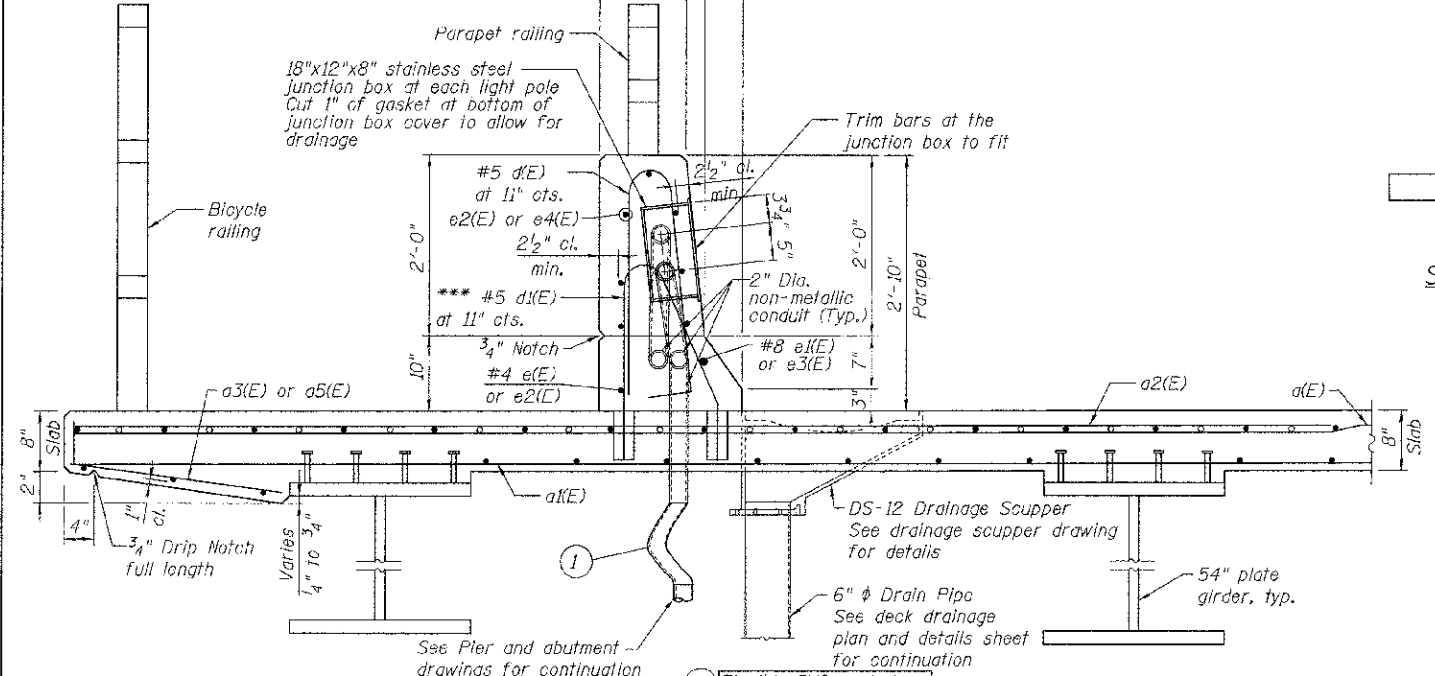
MINIMUM BAR LAP
 (Parapet)
 #4 bar = 2'-0"
 #8 bar = 5'-5"

INSIDE ELEVATION OF PARAPET

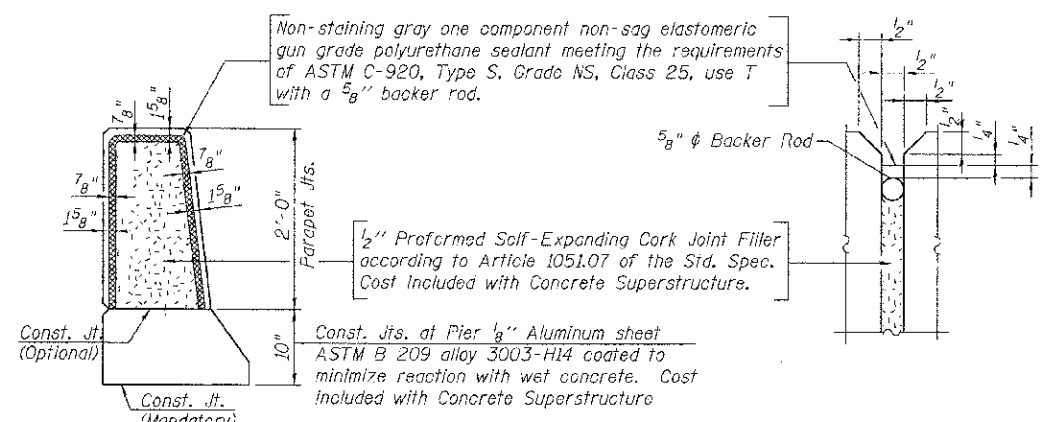


*** Core and set #5 d(E) bars according to Article 509.06 of the Standard Specifications. Corod holes shall be roughened or scored per manufacturer's recommendations. Maximum depth of hole shall not exceed 6".

** The cost of expansion anchors/inserts is included in the cost of "Reinforcement Bars, Epoxy Coated"



SECTION THRU PARAPET
 Flexible PVC coated liquidtight conduit. Cost included with "Conduit embedded in Structure, 2" dia. PVC"



PARAPET JOINT DETAILS

Notes:
 Reinforcement bars designate (E) shall be epoxy coated. Reinforcement bars shall not pass through aluminum sheets and cork joint filler.

FILE NAME = c:\wzdoh\25367381_95\sh\aravinda\contract\road\Structure\Bridg\01\app\Control Copy\5-16 Superstructure Details.dwg

URS
 100 S. WACKER DR.
 SUITE 500
 CHICAGO, IL 60605
 TEL (312) 939-1000
 FAX (312) 939-4998

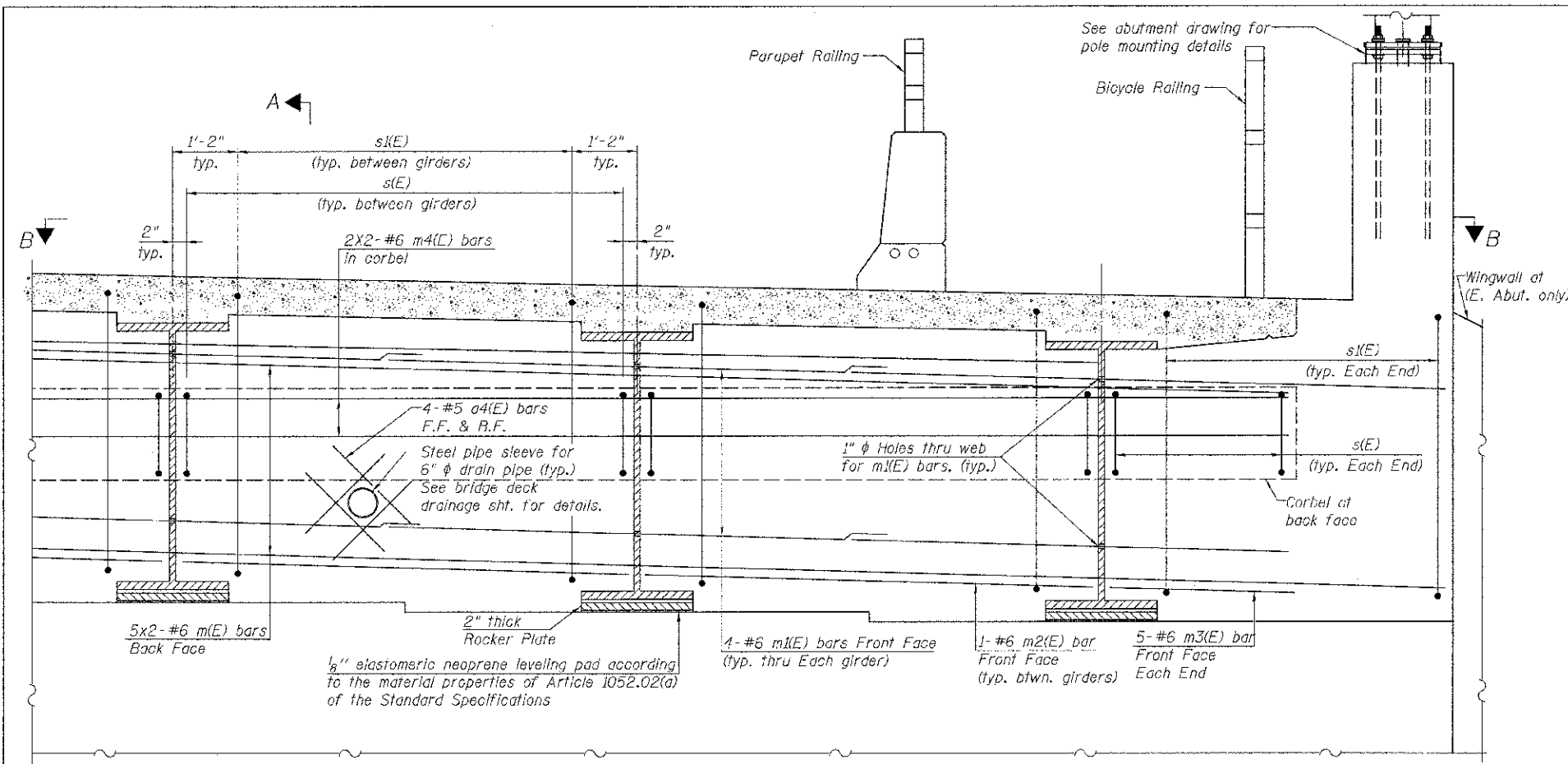
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PLLOT DATE = 10/15/2012	DRAWN - SOI	REVISED -
	CHECKED - NPP	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

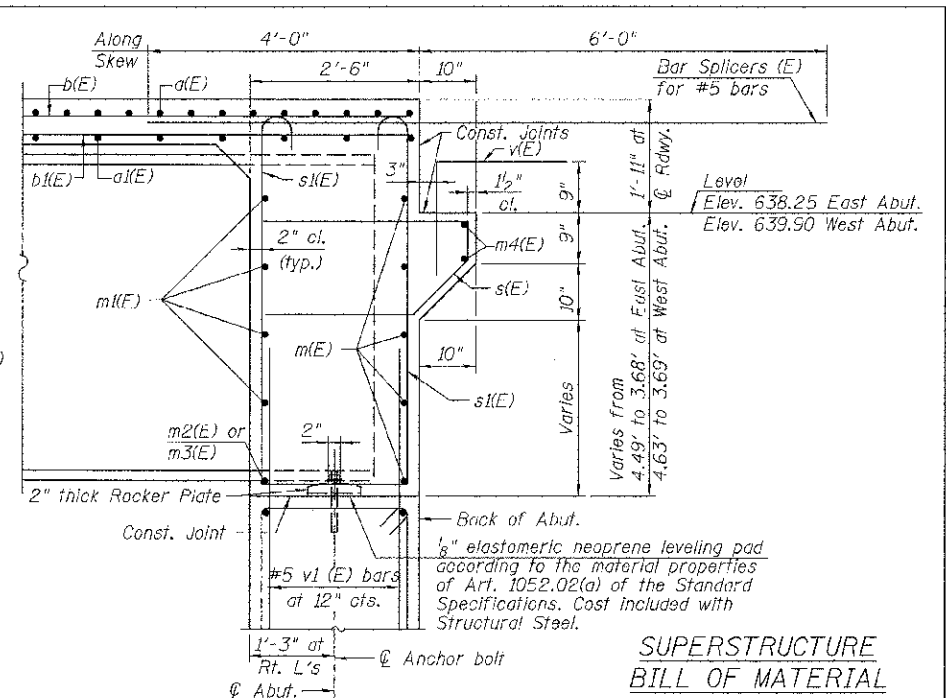
**SUPERSTRUCTURE DETAILS - 1
 STRUCTURE NO. 099-3035**

SHEET NO. 16 OF 38 SHEETS

F.A.U. RTE. 1644	SECTION 01-00181-00-FP	COUNTY WILL	TOTAL SHEETS 328	SHEET NO. 174
				CONTRACT NO. 63647
ILLINOIS FED. AID PROJECT				



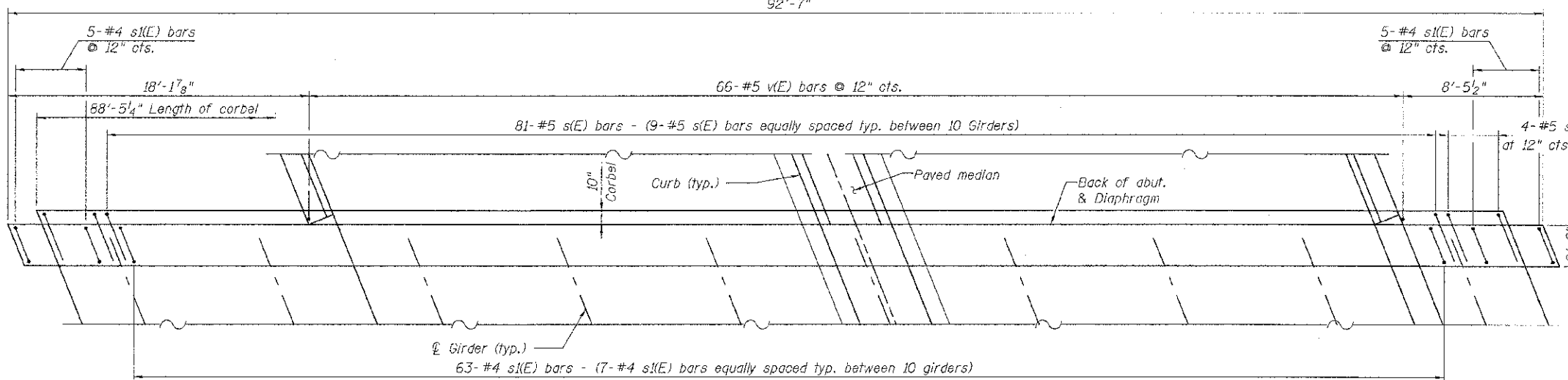
DIAPHRAGM ELEVATION AT ABUTMENT



SECTION A-A
Dimensions at right angles to abutment, except as shown.

SUPERSTRUCTURE BILL OF MATERIAL

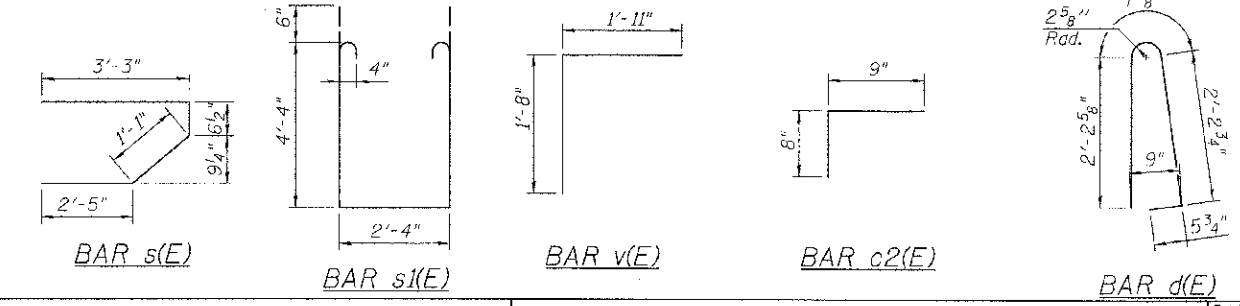
Bar	No.	Size	Length	Shape
a(E)	1322	#5	42'-6"	
a1(E)	1212	#5	29'-5"	
a2(E)	650	#6	14'-0"	
a3(E)	554	#5	2'-6"	
a4(E)	64	#5	2'-0"	
a5(E)	240	#5	2'-9"	
b(E)	902	#5	30'-6"	
b1(E)	166	#6	48'-0"	
b2(E)	621	#5	36'-6"	
b3(E)	55	#5	30'-6"	
c(E)	392	#5	3'-8"	
c2(E)	604	#5	1'-5"	
d(E)	662	#5	5'-7"	
d1(E)	662	#5	4'-8"	
e(E)	20	#4	28'-0"	
e1(E)	16	#8	37'-0"	
e2(E)	32	#4	19'-9"	
e3(E)	4	#8	19'-9"	
e4(E)	224	#4	16'-1"	
m(E)	20	#6	33'-3"	
m1(E)	80	#6	12'-6"	
m2(E)	18	#6	7'-11"	
m3(E)	20	#6	5'-6"	
m4(E)	8	#6	46'-0"	
s(E)	178	#5	7'-3"	
s1(E)	146	#4	12'-0"	
v(E)	132	#5	3'-7"	
Bar Splicer (E)	166	#5	10'-0"	
Reinforcement Bars, Epoxy Coated		Pound	199,820	
Concrete Superstructure		Cu. Yds.	815.4	
Bridge Deck Grooving		Sq. Yds.	1,682	
Protective Coat		Sq. Yds.	3,145	
Conduit Embedded in Structure, 2" dia., PVC		Foot	1548	
Junction Box, Stainless Steel, Embedded in Structure, 18"x12"x8"		Each	6	
Bar Splicers		Each	124	



SECTION B-B

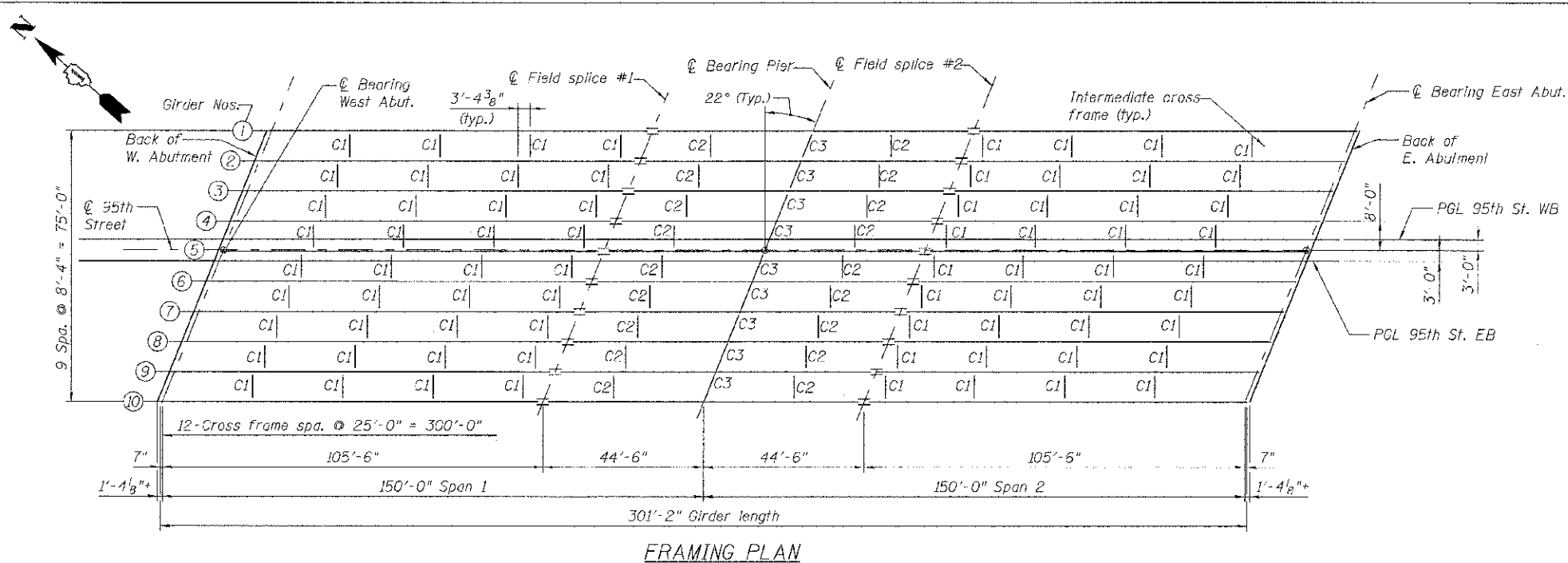
MINIMUM BAR LAP (Diaphragm)

#6 Bar = 3'-10"

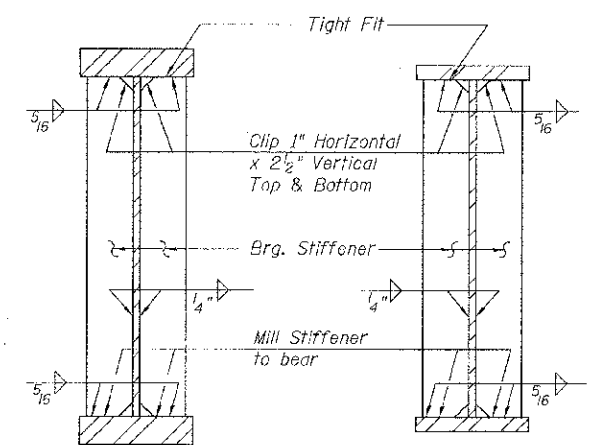


Notes:
 Bars indicated thus 1 x 8 - #5 etc. indicates 1 line of bars with 8 lengths per line.
 Reinforcement bars designated (E) shall be epoxy coated.
 Reinforcement bars in diaphragm are billed with superstructure.
 The s(E) and s1(E) bars shall be placed parallel to the girders. Spacing for these bars shall be at right angle to the girders.

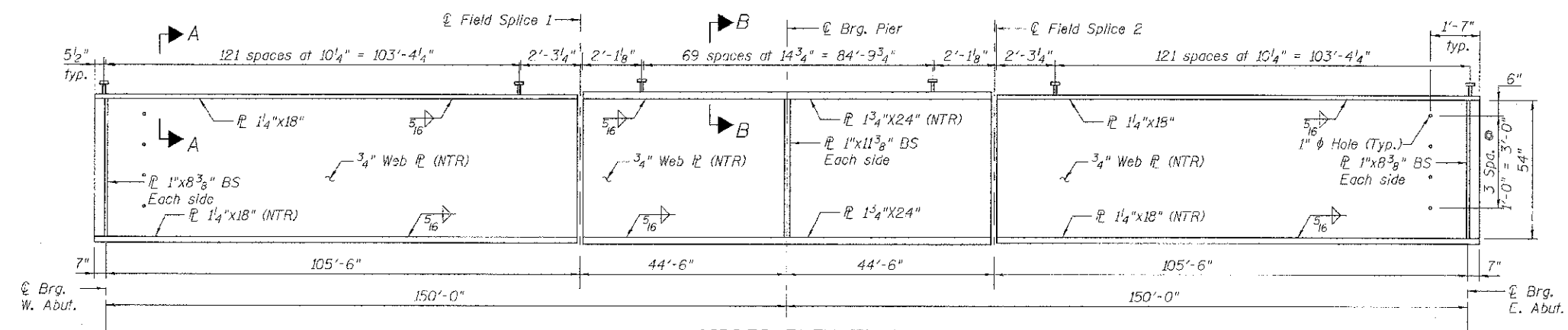
SI-DS1 7-1-10



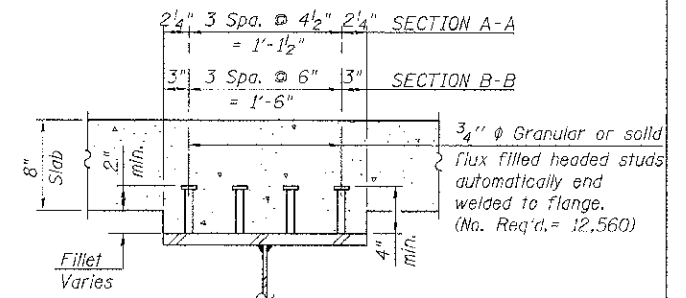
FRAMING PLAN



SECTION AT PIER SECTION AT ABUTMENT

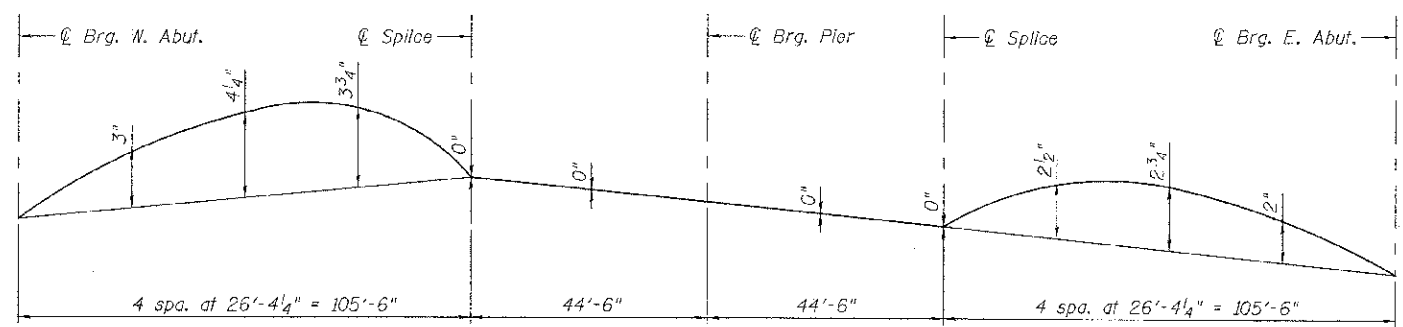


GIRDER ELEVATION



SECTION A-A SECTION B-B

NOTES:
 All Structural steel shall be AASHTO M270 Grade 50W weathering steel.
 "NTR" denotes plates to which notch toughness requirements are applicable.
 "BS" denotes bearing stiffeners
 Bearing stiffeners shall be vertical in the erected position.
 Fascia girders will have connection plates for cross frames only on the inside.



CAMBER

FILE NAME = c:\p\w\cdoh_25367381_9515\Drawings\contract\scs\scs\Drawings\Contract\Copy\18 Framing Plan.dgn



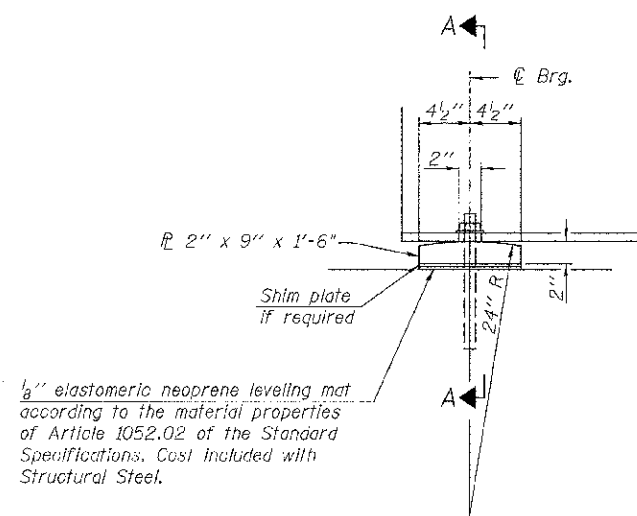
100 S. WACKER DR.
 SUITE 500
 CHICAGO IL 60606
 TEL (312) 339-1000
 FAX (312) 339-4998

USER NAME = *STB*	DESIGNED - STB	REVISED -
PILOT SCALE = 7/8" = 1' / 1/8"	CHECKED - NPP	REVISED -
PLOT DATE = 12/16/2012	DRAWN - SOI	REVISED -
	CHECKED - NPP	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

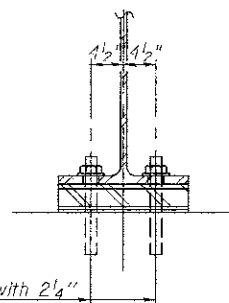
FRAMING PLAN
 STRUCTURE NO. 099-3035
 SHEET NO. 18 OF 38 SHEETS

F.A.U. RTE. 1644	SECTION 01-0018-00-FP	COUNTY WILL	TOTAL SHEETS 328	SHEET NO. 176
CONTRACT NO. 63647				ILLINOIS TOLL AID PROJECT



1/8" elastomeric neoprene leveling mat according to the material properties of Article 1052.02 of the Standard Specifications. Cost included with Structural Steel.

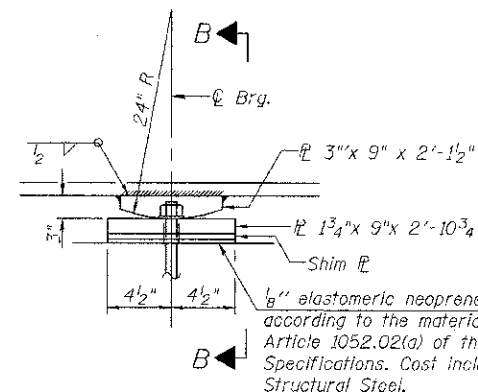
ELEVATION AT ABUTMENT



1" ϕ x 12" anchor bolts with 2 1/4" x 2 1/4" x 3/16" \mathbb{E} washer under nut. 1 3/8" x 2" slotted hole in flange. 1 1/2" ϕ holes in bearing plate.

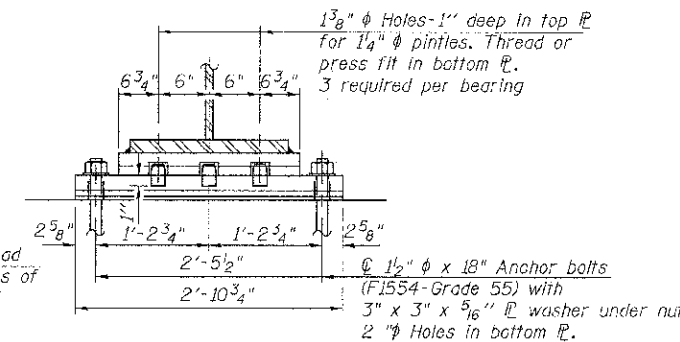
SECTION A-A

FIXED BEARING AT ABUTMENTS
(20 Required)

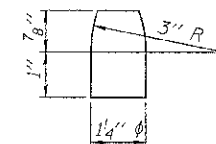


ELEVATION AT PIER

FIXED BEARING AT PIER
(10 Required)



SECTION B-B



PINTLE

Notes:
Anchor bolts shall be ASTM F1554 Grade 55 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. The corresponding specified Grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.
Anchor bolts may be either cast in place or installed in holes drilled after the supported member is in place. Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.
The structural steel plates and pintels of the bearing assembly shall conform to the requirements of AASHTO M270 Grade 50W.

BILL OF MATERIAL

Item	Unit	Total
Anchor Bolts, 1"	Each	40
Anchor Bolts, 1 1/2"	Each	20

FILE NAME = c:\web\04_25367281_06\tdrawings\contract\kuh\Structures\bridge\I-2E-1 & D00090\Bearing Details.dgn

I-2E-1 & D00090 7-1-10



100 S. WACKER DR.
SUITE 500
CHICAGO, IL 60605
TEL (312) 939-1000
FAX (312) 939-4698

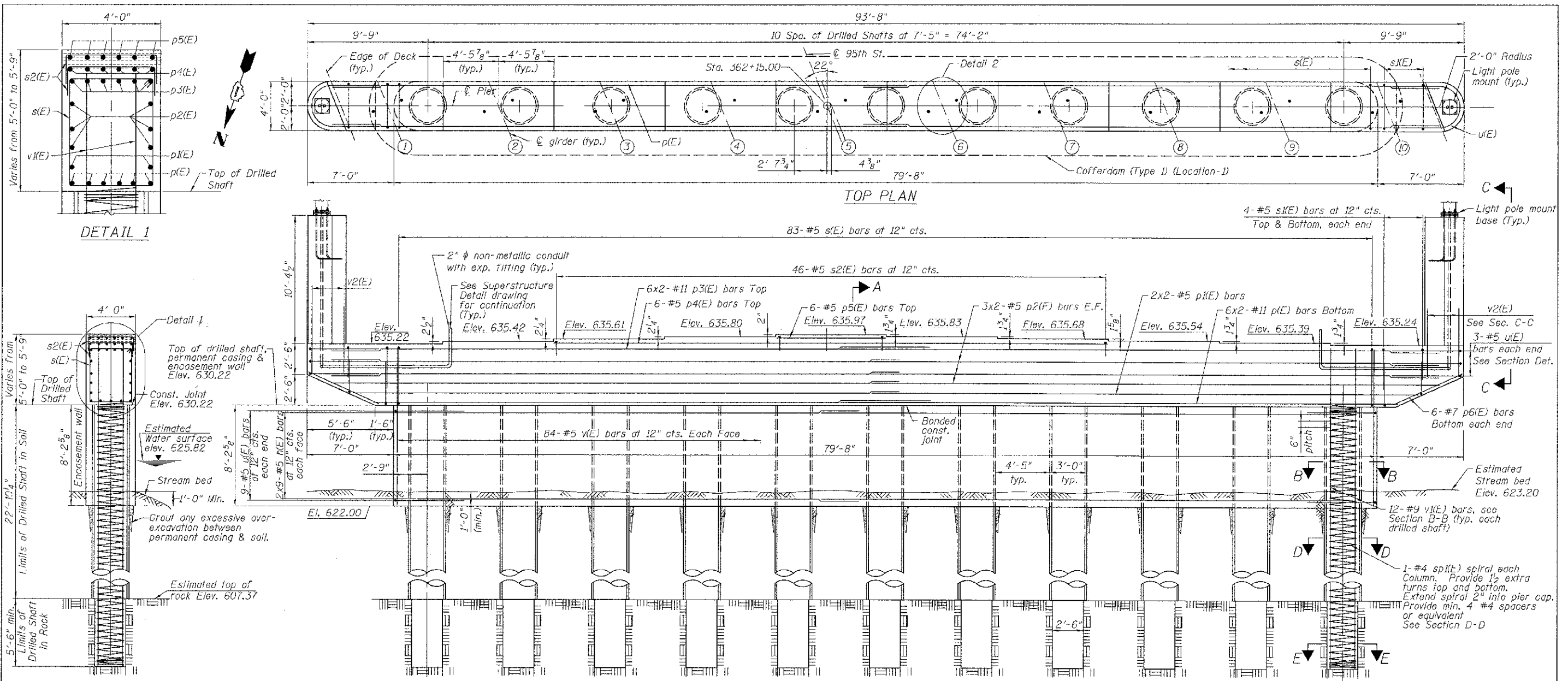
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PLOT DATE = 12/16/2012	DRAWN - SOI	REVISED -
	CHECKED - NPP	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BEARING DETAILS
STRUCTURE NO. 099-3035

SHEET NO. 20 OF 38 SHEETS

P.A.U. RTE. 1644	SECTION 01-00181-00-PP	COUNTY WILL	TOTAL SHEETS 328	SHEET NO. 178
			CONTRACT NO. 63647	
ILLINOIS FED. AID PROJECT				



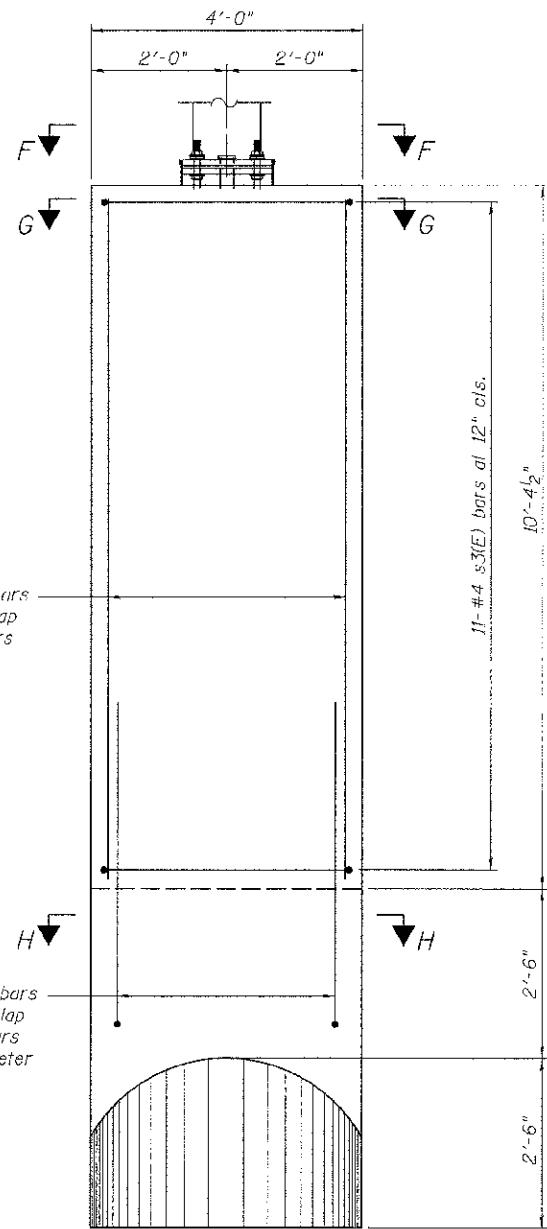
SECTION A-A

Notes:
 Cast steps monolithically with cap.
 Space cap reinforcement to miss anchor bolts.
 Minimum lap for spirals = 2'-0"
 Bars indicated thus 3x2-#5 etc. indicates 3 lines of bars with 2 lengths per line.
 Reinforcement bars designated (E) shall be epoxy coated.
 The cost of anchor rods for the light mount is included with price of "Concrete Structures".
 The cost of conduits is included with the price of "Conduit Embedded in Structure, 2" dia., PVC".
 The top of all drilled shafts shall be constructed to the same elevation and extend above the prevailing water surface. The quantities and reinforcement detailing are based on the top of shaft and the estimated elevations shown and may change based on the actual elevations encountered at each shaft and the final bottom of shaft elevation.
 Minimum bar lap lengths for #11 bars is 7'-2" and for #5 bars is 2'-2".

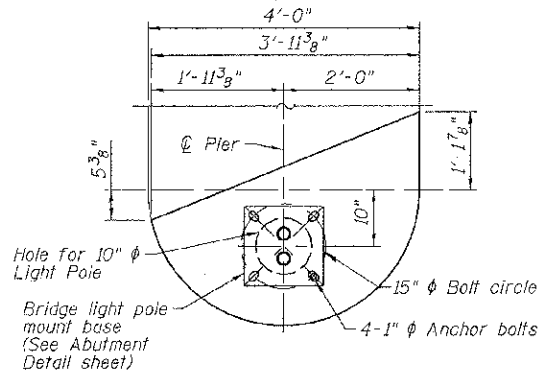
Note:
 For Sections B-B, C-C, D-D & E-E See Pier Detail sheet

FILE NAME: c:\work\01-26-07\361-95th\Drawings\Structure\B-Edge (U)Pier\Control Copy\AS-21.Pier.dgn
 100 S. WACKER DR. SUITE 500 CHICAGO IL 60606 TEL (312) 939-1000 FAX (312) 935-4998

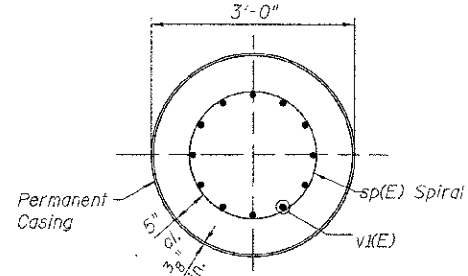
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	PLOT SCALE = 4:8 1/2" = 1'-0" PLOT DATE = 10/16/2012	SHEET NO. 21 OF 36 SHEETS			



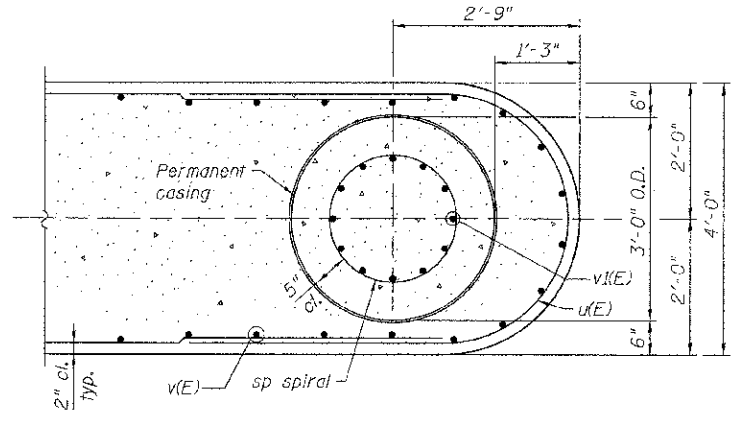
SECTION C-C
SOUTH PIER LIGHT PEDESTAL SHOWN
NORTH PIER LIGHT PEDESTAL SIMILAR



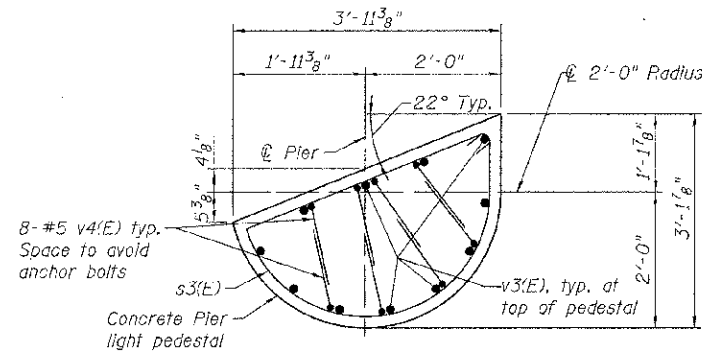
SECTION F-F



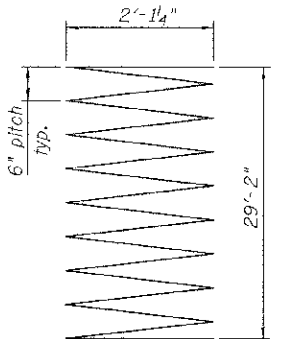
SECTION D-D



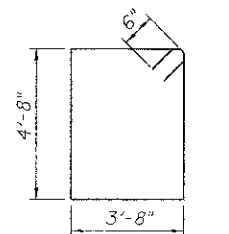
SECTION B-B



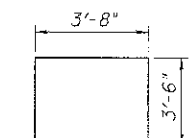
SECTION G-G



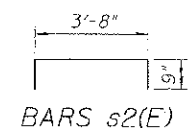
BARS sp1(E) SPIRAL



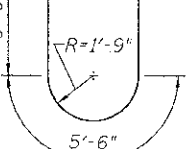
BAR s1(E)



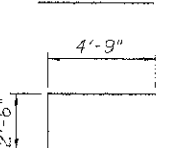
BAR s2(E)



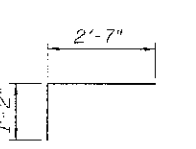
BAR s3(E)



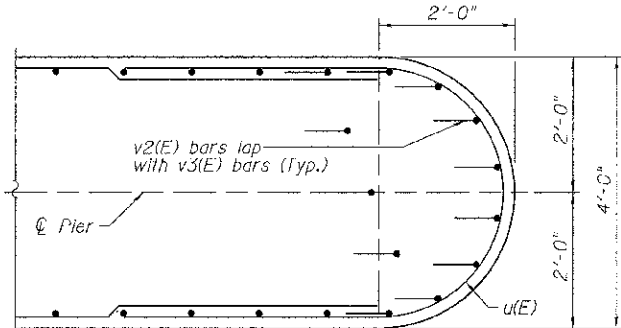
BAR u(E)



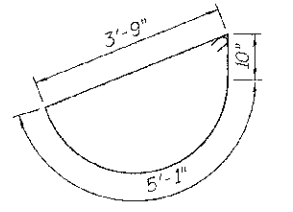
BAR v2(E)



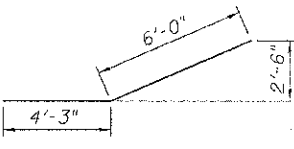
BAR v4(E)



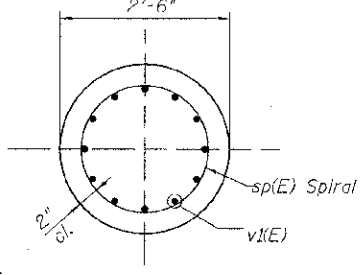
SECTION H-H



BARS s3(E)



BAR p6(E)



SECTION E-E

BILL OF MATERIAL

Bar	Size	No.	Length	Shape
h(F)	#5	36	38'-6"	—
p1(E)	#11	12	48'-3"	—
p2(E)	#5	4	45'-0"	—
p3(E)	#11	12	48'-0"	—
p4(E)	#5	12	44'-6"	—
p5(E)	#5	6	8'-7"	—
p6(E)	#7	12	10'-3"	—
s(E)	#5	83	17'-8"	□
s1(E)	#5	16	10'-8"	U
s2(E)	#5	46	5'-2"	U
s3(E)	#5	22	10'-8"	U
sd1	#4	11	29'-2"	W
u(E)	#5	24	13'-0"	U
v(E)	#5	168	10'-7"	—
v1(E)	#9	132	33'-2"	—
v2(E)	#5	22	7'-3"	—
v3(E)	#5	22	10'-2"	—
v4(E)	#5	16	3'-9"	—
Concrete Structures	Cu. Yd.	172.7		
Cofferdam Excavation	Cu. Yd.	30		
Reinforcement Bars, Epoxy Coated	Pound	32,050		
Cofferdam (Type I) (Location-1)	Each	1		
Drilled Shaft in Soil	Cu. Yd.	42.1		
Drilled Shaft in Rock	Cu. Yd.	11.0		

* Length indicated is height of spiral.
** Weight of spiral bar is based on actual length, not length indicated on bar detail.

NOTES:

For the locations of Sections B-B, C-C, D-D & E-E See Pier sheet

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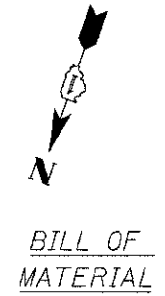
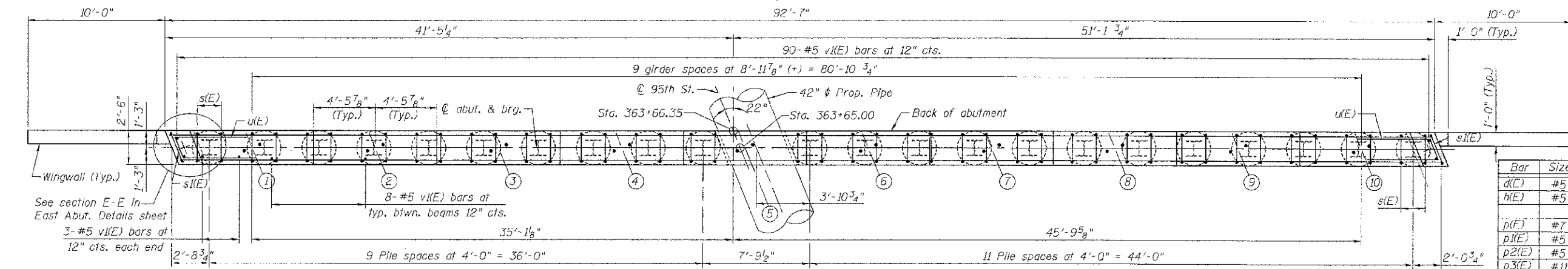
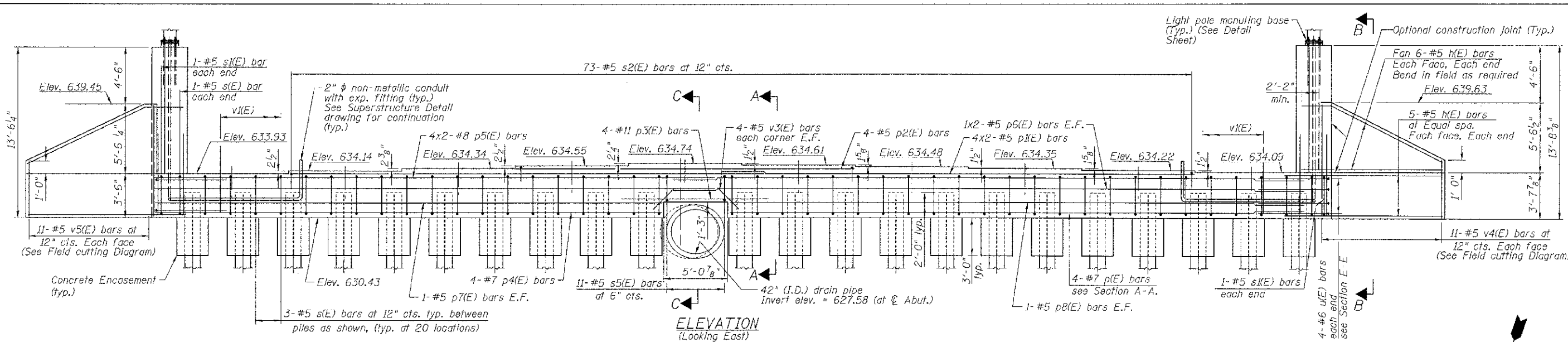
URS
100 S. WACKER DR.
SUITE 500
CHICAGO, IL 60606
TEL (312) 939-1000
FAX (312) 939-4998

USER NAME = STB	DESIGNED - STB	REVISED -
PLOT SCALE = 1/4" = 1'-0"	CHECKED - NPP	REVISED -
PLOT DATE = 10/15/2012	DRAWN - SOI	REVISED -
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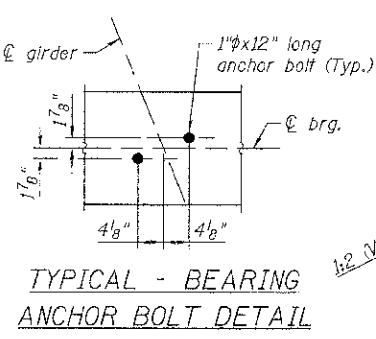
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER DETAILS
STRUCTURE NO. 099-3035
SHEET NO. 22 OF 38 SHEETS

F.A.U. RTE. 1644	SECTION 01-30181-00-PP	COUNTY WILL	TOTAL SHEETS 328	SHEET NO. 180
			CONTRACT NO. 63647	
ILLINOIS FED. AID PROJECT				

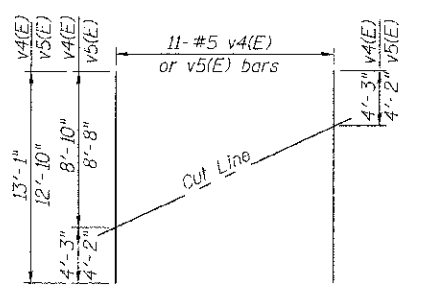
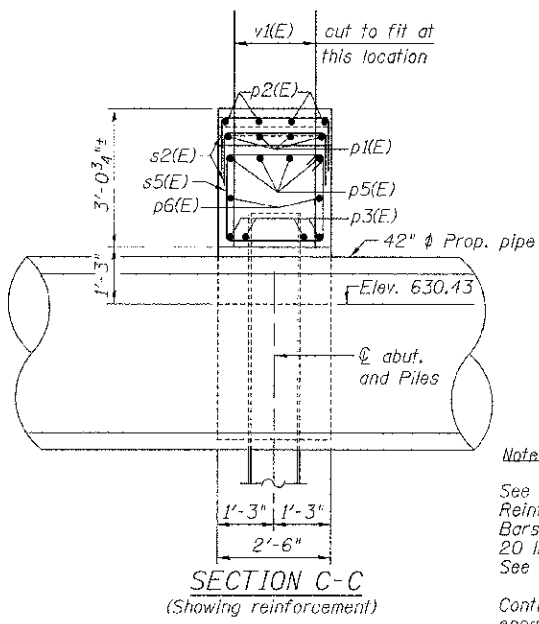
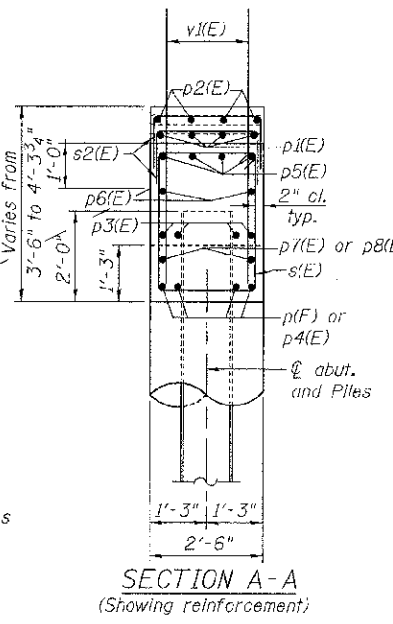


Bar	Size	No.	Length	Shape
d(E)	#5	20	5'-7"	
h(E)	#5	44	13'-6"	
p(E)	#7	4	47'-3"	
p1(E)	#5	8	37'-1"	
p2(E)	#5	1	26'-7"	
p3(E)	#11	4	22'-0"	
p4(E)	#7	4	39'-9"	
p5(E)	#8	8	50'-0"	
p6(E)	#5	4	47'-5"	
p7(E)	#5	2	39'-9"	
p8(E)	#5	2	47'-3"	
s(E)	#5	63	11'-8"	
s1(E)	#5	2	12'-0"	
s2(E)	#5	73	3'-8"	
s3(E)	#5	8	2'-11"	
s4(E)	#5	22	8'-6"	
s5(E)	#5	11	10'-8"	
u(E)	#6	8	10'-0"	
v1(E)	#5	156	5'-11"	
v2(E)	#5	20	9'-10"	
v3(E)	#5	16	2'-0"	
v4(E)	#5	22	13'-1"	
v5(E)	#5	22	12'-10"	
Forous Granular Embankment, Special Concrete Structures	Cu. Yd.	144		
Reinforcement Bars Epoxy Coated	Cu. Yd.	42.0		
Furnishing Steel Piles HP14x73	Foot	609		
Driving Piles HP14x73	Foot	609		
Test Pile Steel HP14x73	Each	1		
Concrete Encasement	Cu. Yd.	12		
Pile Shoes	Each	22		



PILE DATA

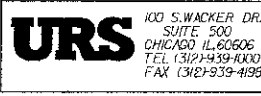
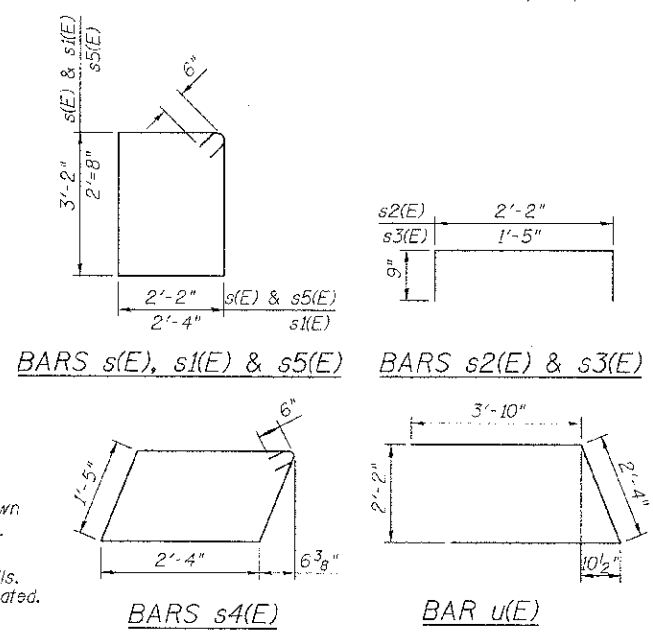
Type: HP14x73 with pile shoes
 Nominal Required Bearing set in rock: 578 kips
 Factored Resistance Available: 193 kips
 Est. Length: 29'
 No. Production Piles: 21
 No. Test Piles: 1
 Estimated top of rock varies from EL. 612.80 to EL. 608.68



Notes:

See Piles and Concrete Encasement sheet for details.
 Reinforcement bar designated (E) shall be epoxy coated.
 Bars indicated thus 20x3 - #5 etc. indicates 20 lines of bars with 3 lengths per line.
 See Sheet East Abutment details for Detail 2 and Section B-B

Contractor is responsible for accurate placement of piles and encasement to avoid conflict with 42" I.D. drain pipe.
 Minimum bar lap lengths for #8 bars is 3'-8" and for #5 bars is 2'-2".



USER NAME = #ST08	DESIGNED - STD	REVISED -
PLOT SCALE = 4/8" = 1" / 16"	CHECKED - NPP	REVISED -
PLOT DATE = 10/16/2012	DRAWN - SOI	REVISED -
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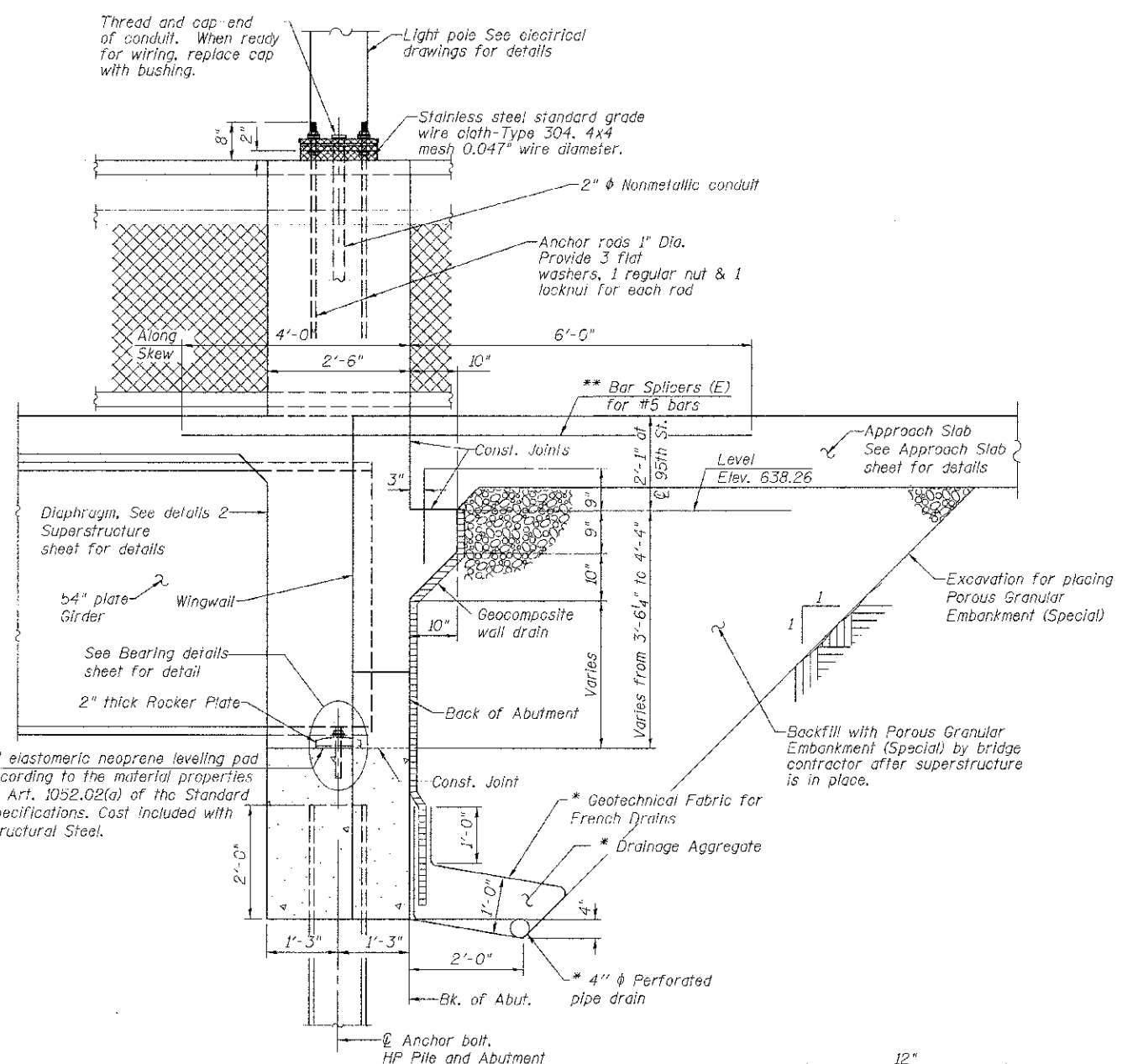
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

EAST ABUTMENT STRUCTURE NO. 099-3035

SHEET NO. 23 OF 36 SHEETS

F.A.U. RTE. 1644	SECTION 01-00181-00-PP	COUNTY WILL	TOTAL SHEETS 328	SHEET NO. 181
CONTRACT NO. 63647			ILLINOIS FED. AID PROJECT	

FILE NAME = c:\p\cd\25357381_1514\Drawings\Structure\Structures\24 East Abutment Details.dwg



SECTION THRU ABUTMENT
(showing drainage details) (Horiz. dim. @ Rt. L's)

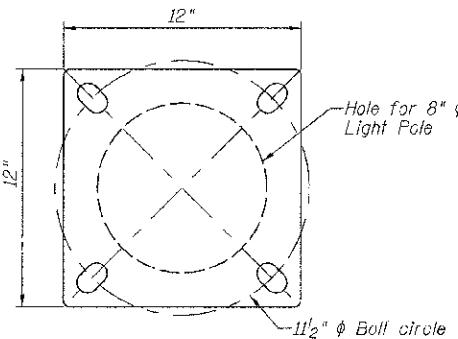
* Included in the cost of "Pipe Underdrains for Structures 4\".

** See bar splicers sheet for details.

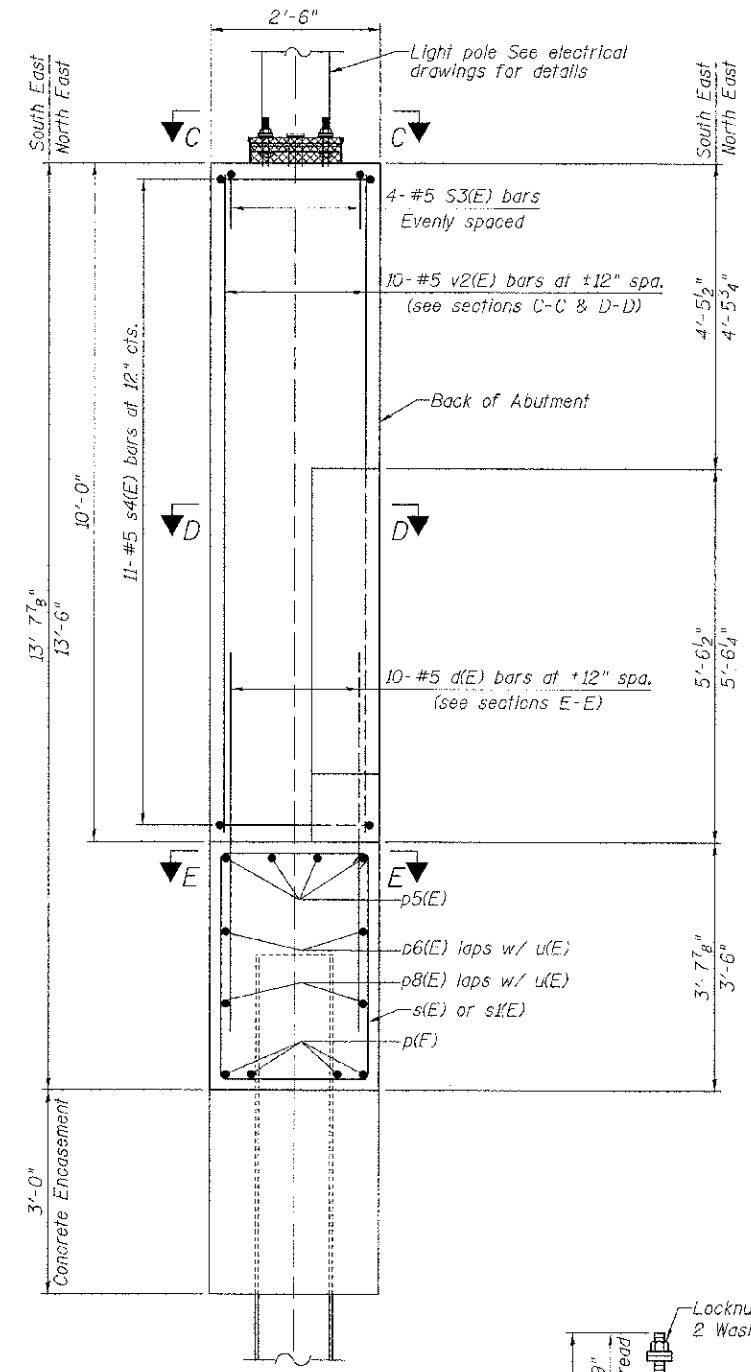
Notes:
All drainage system components shall extend to 2'-0" from the end of each wingwall except an outlet pipe shall extend until intersecting with the side slopes. The pipes shall drain into concrete headwalls.. (See Article 601.05 of the Standard Specifications and Highway Standard 601101).

The cost of anchor rods for light mount is included with price of "Concrete Structures\".

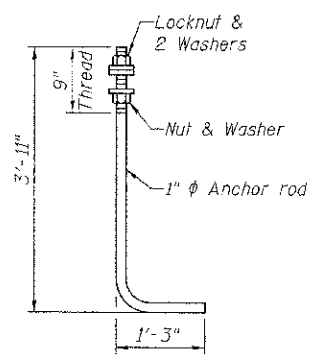
The cost of conduit is included with the price of "Conduit Encased In Concrete, 2\" dia., PVC\".



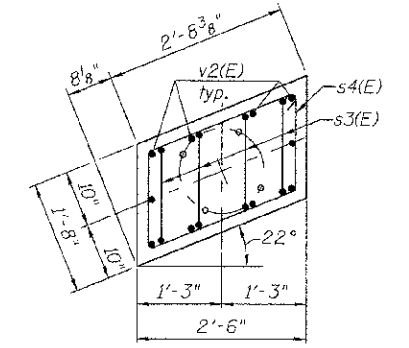
LIGHT POLE BASE PLATE DETAIL
1 1/2" DIA. BOLT CIRCLE
(For reference)



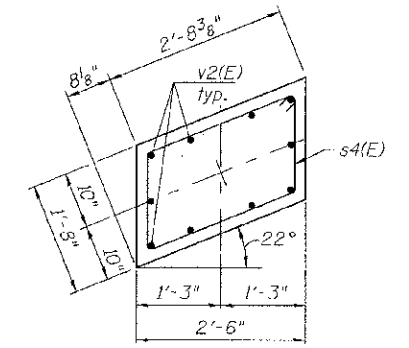
SECTION B-B
(South end shown, North end similar)
See sheet East Abutment for location of Section B-B



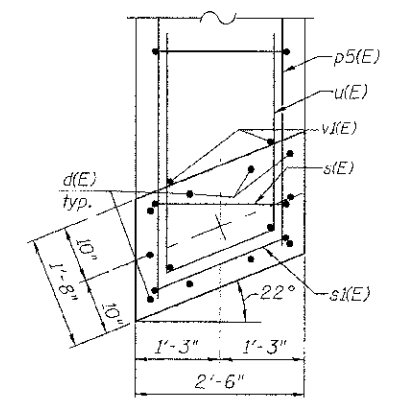
ANCHOR ROD
Diameter as specified for light poles.
(ASTM F 1554 Grade 105)
(24 Required)



SECTION C-C



SECTION D-D



SECTION E-E



100 S. WACKER DR.
SUITE 500
CHICAGO, IL 60605
TEL (312) 939-1000
FAX (312) 939-4998

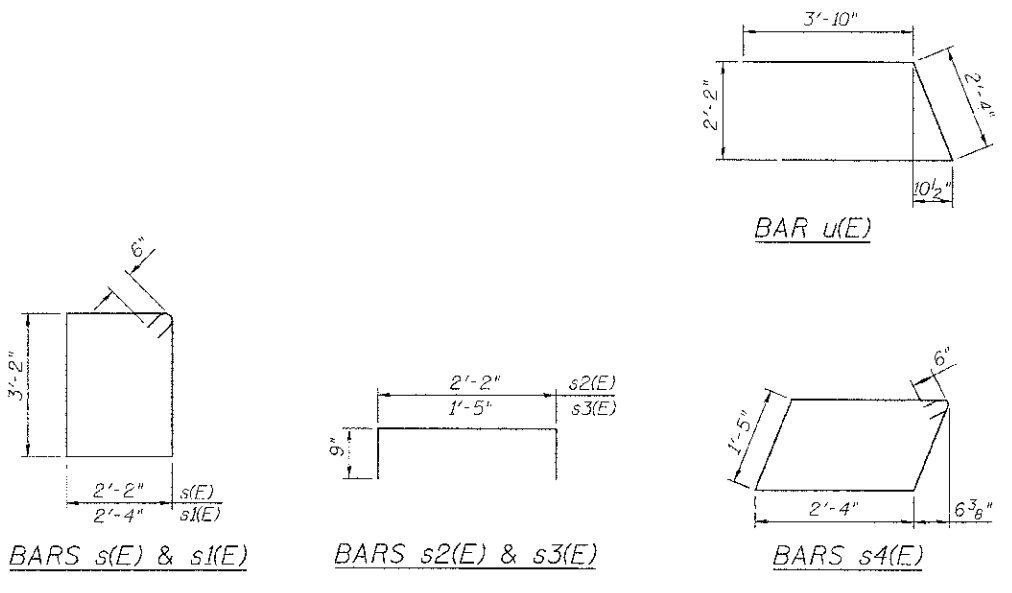
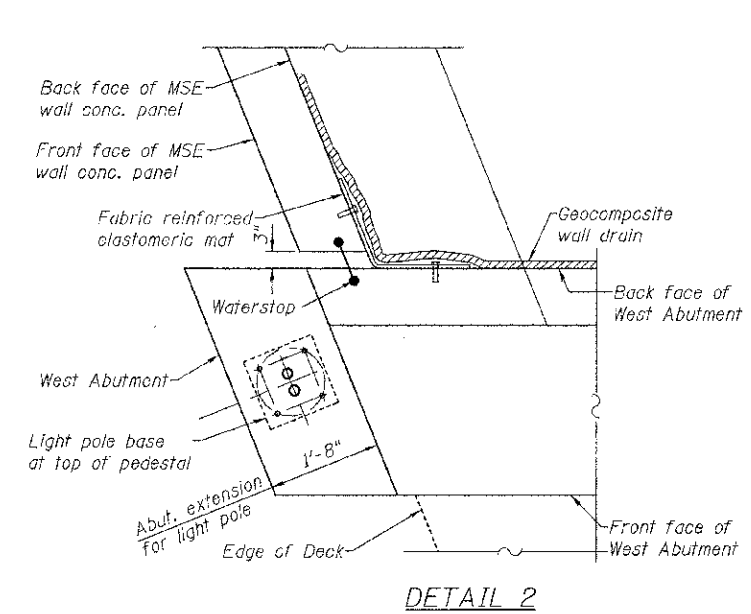
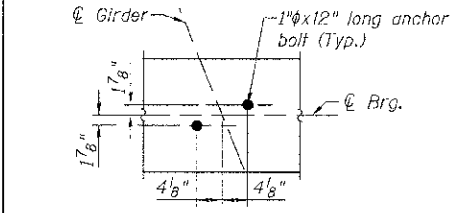
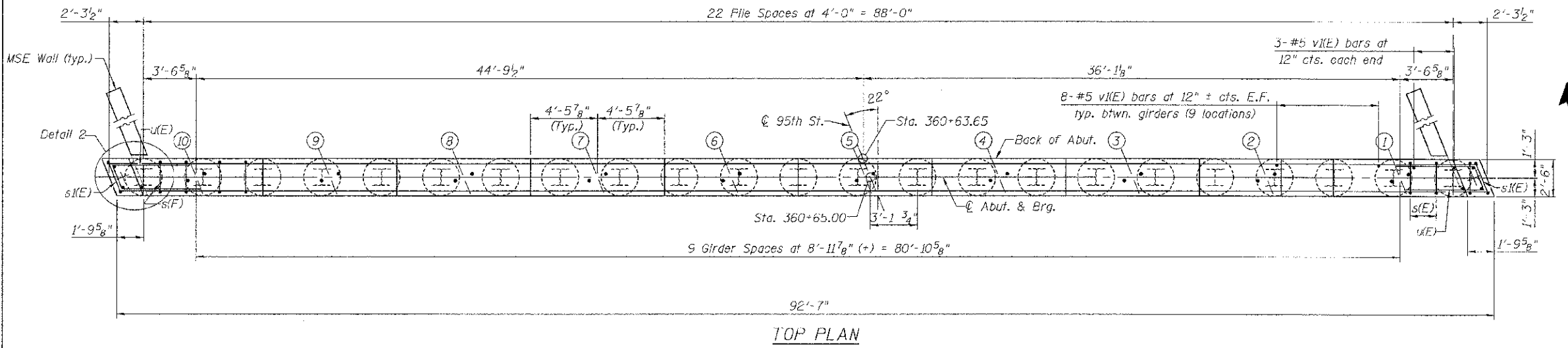
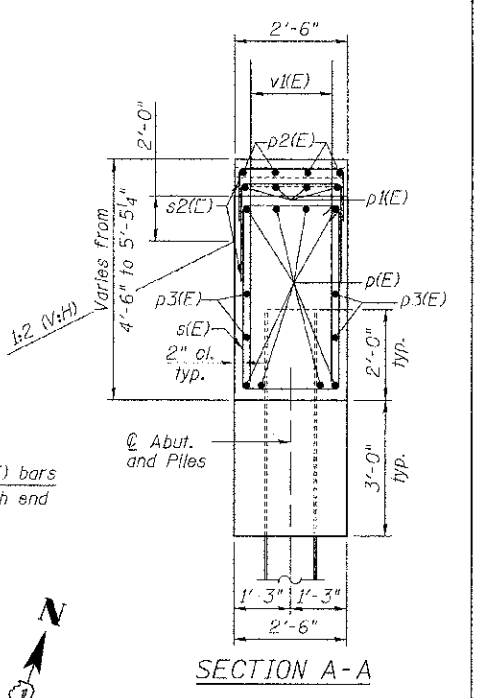
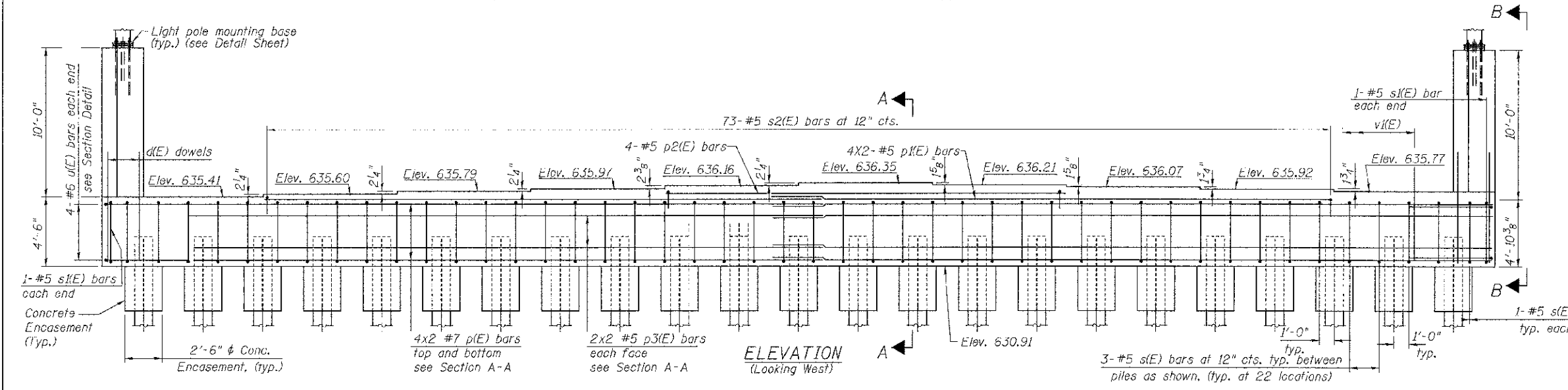
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PL OT DATE = 10/16/2012	DRAWN - SOI	REVISED -
	CHECKED - NPP	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EAST ABUTMENT DETAILS
STRUCTURE NO. 099-3035**

SHEET NO. 24 OF 38 SHEETS

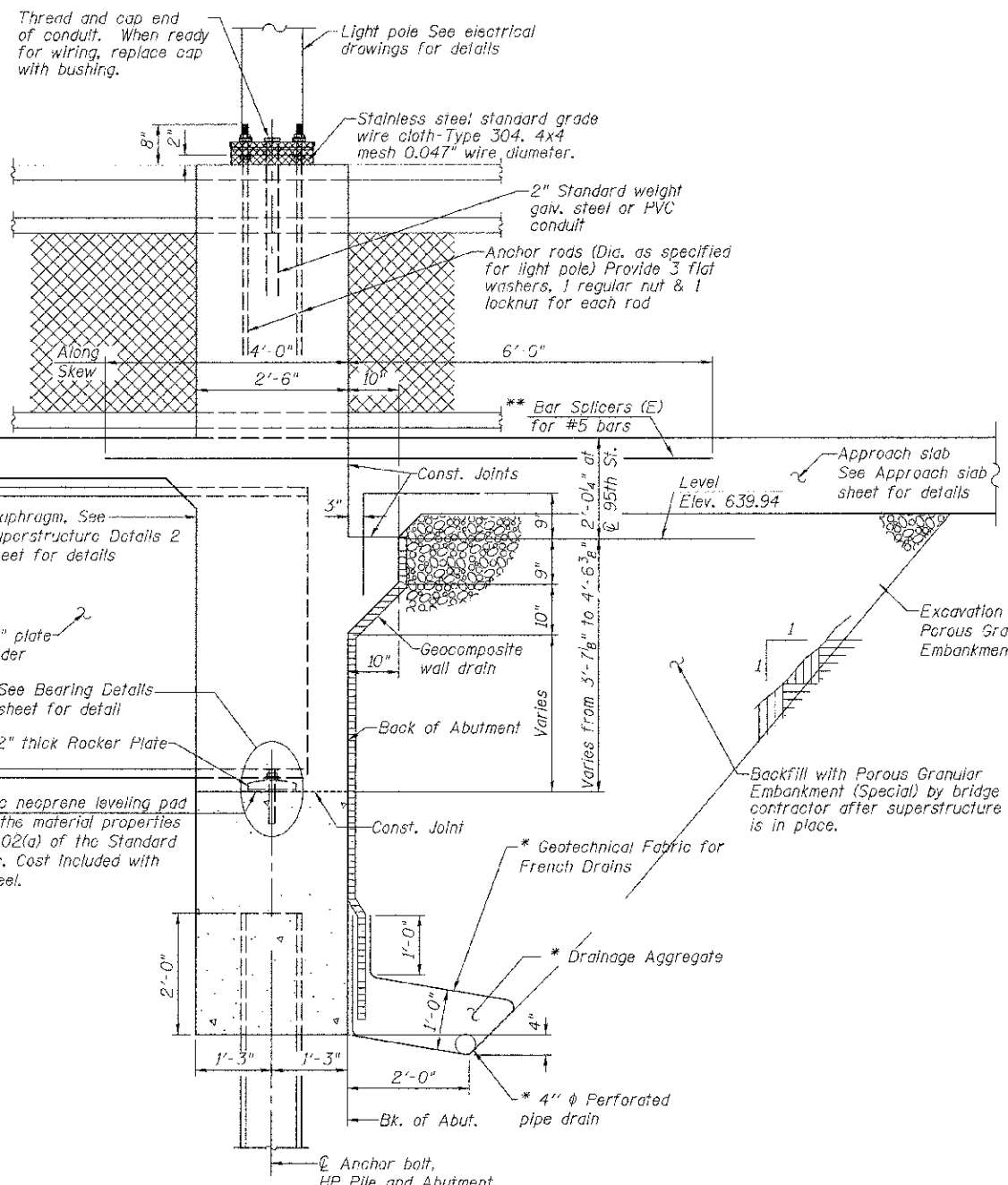
P.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1644	01-0018-00-PP	WILL	328	182
CONTRACT NO. 63647			ILLINOIS FED. AID PROJECT	



**WEST ABUTMENT
BILL OF MATERIAL**

Bar	Size	No.	Length	Shape
d(E)	#5	20	5'-6"	□
p(E)	#7	16	49'-2"	□
p1(E)	#5	8	37'-1"	□
p2(E)	#5	4	26'-7"	□
p3(E)	#5	8	47'-5"	□
s(E)	#5	72	11'-8"	□
s1(E)	#5	2	12'-0"	□
s2(E)	#5	73	3'-8"	□
s3(E)	#5	8	2'-11"	□
s4(E)	#5	22	8'-6"	□
u(E)	#6	8	10'-0"	□
v1(E)	#5	156	5'-11"	□
v2(E)	#5	20	9'-8"	□
Porous Granular Embankment, Special				Cu. Yd. 149
Concrete Structures				Cu. Yd. 37.4
Reinforcement Bars, Epoxy Coated				Pound 5,230
Furnishing Steel Piles HP14x73				Foot 770
Driving Piles				Foot 770
Test Pile Steel HP14x73				Each 1
Concrete Encasement				Cu. Yd. 13
Pile Shoes				Each 23

See Piles and Concrete Encasement sheet for details.
 Reinforcement bar designated (E) shall be epoxy coated.
 Bars indicated thus 20X3 -#5 etc. indicates 20 lines of bars with 3 lengths per line.
 See sheet West abutment details for section B-B
 Minimum bar lap lengths for #7 bars is 2'-9" and for #5 bars is 2'-2".

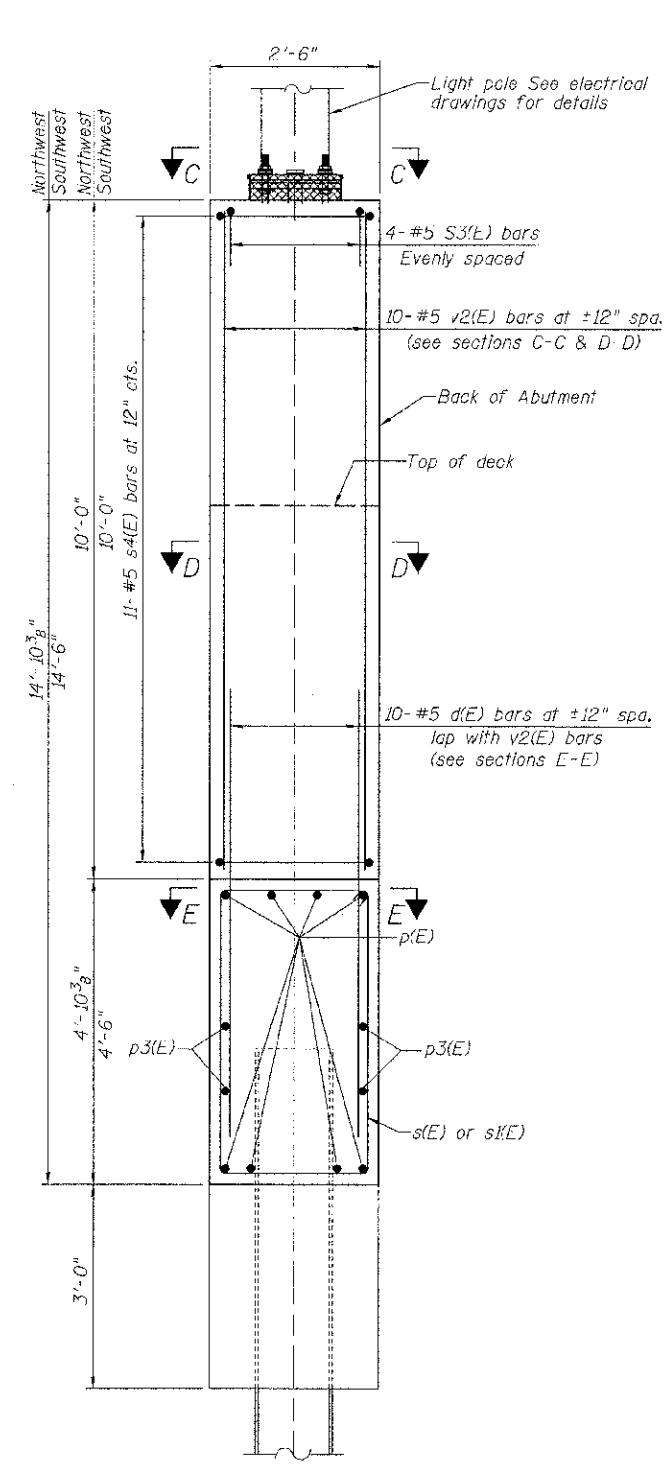


SECTION THRU ABUTMENT
(showing drainage details) (Horiz. dim. @ Rt. L's)

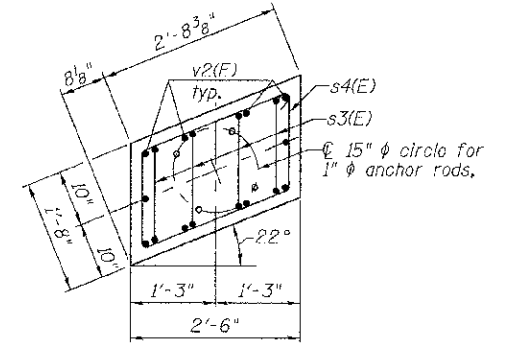
* Included in the cost of "Pipe Underdrains for Structures 4".
** See Bar splicers sheet for details

Note:
All drainage system components shall extend to 2'-0" from the end of each wingwall except an outlet pipe shall extend until intersecting with the side slopes. The pipes shall drain into concrete headwalls. (See Article 601.05 of the Standard Specifications and Highway Standard 601101).

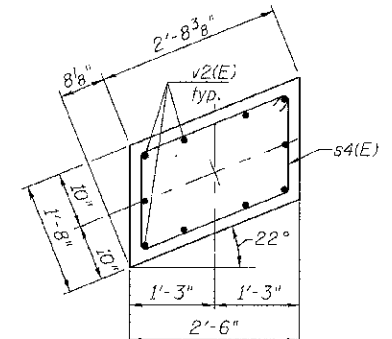
See East Abutment Detail sheet for Anchor Rod Details



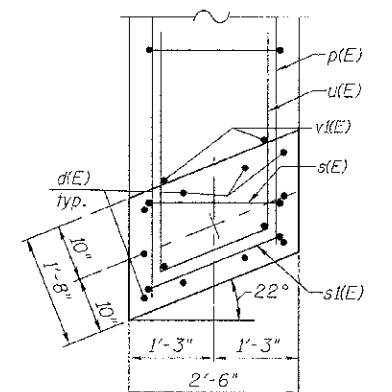
SECTION B-B
(North end shown, South end similar)



SECTION C-C



SECTION D-D



SECTION E-E

FILE NAME: g:\w\edoh_25567381_06\sh\drawings\concrete\abutment\Structure\Structure\B-bridge\01\fig\Central\Copy\S-28 West Abutment Detail.dgn

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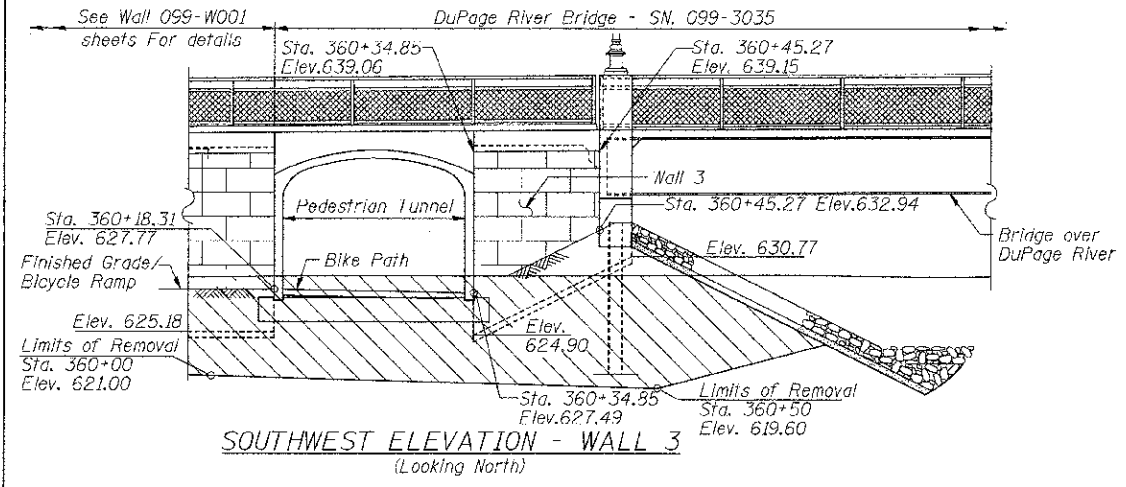
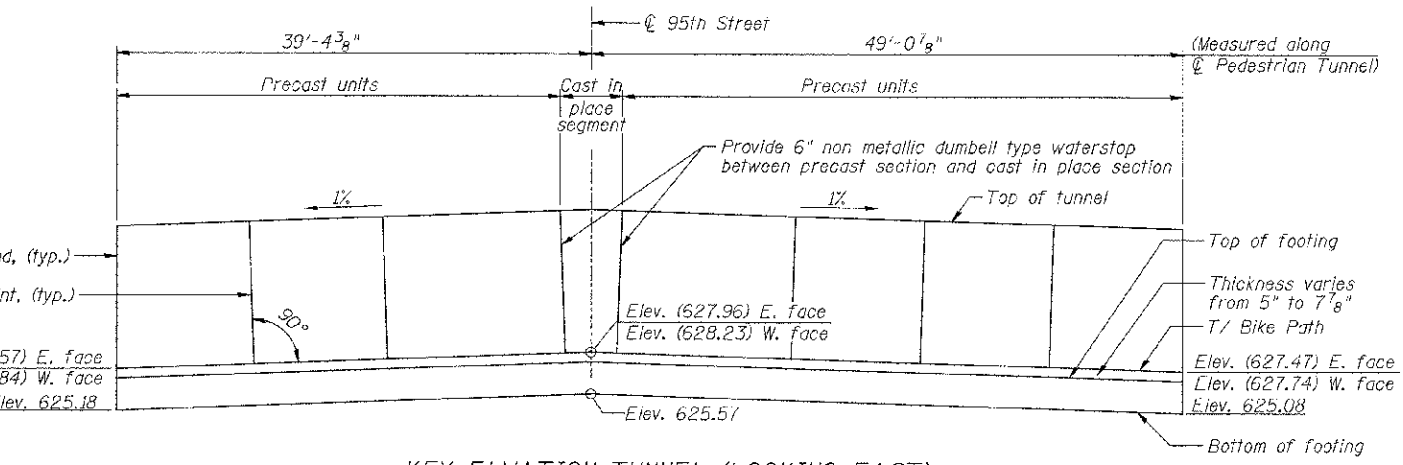
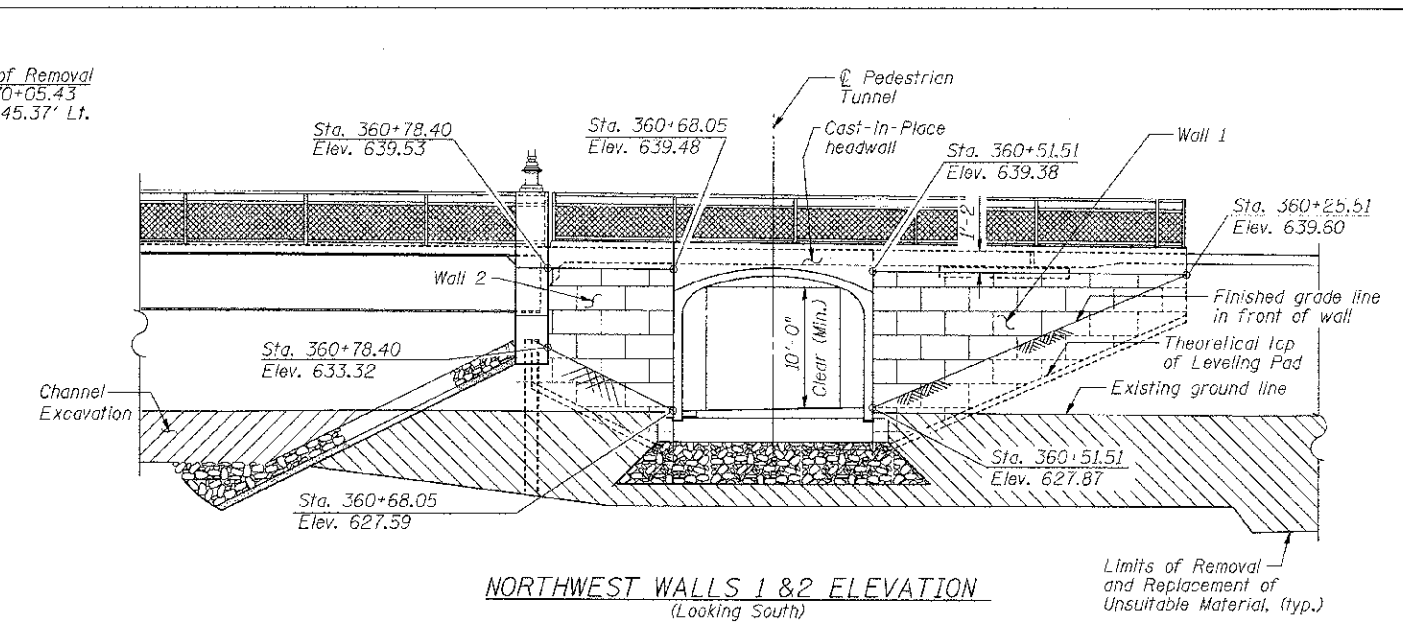
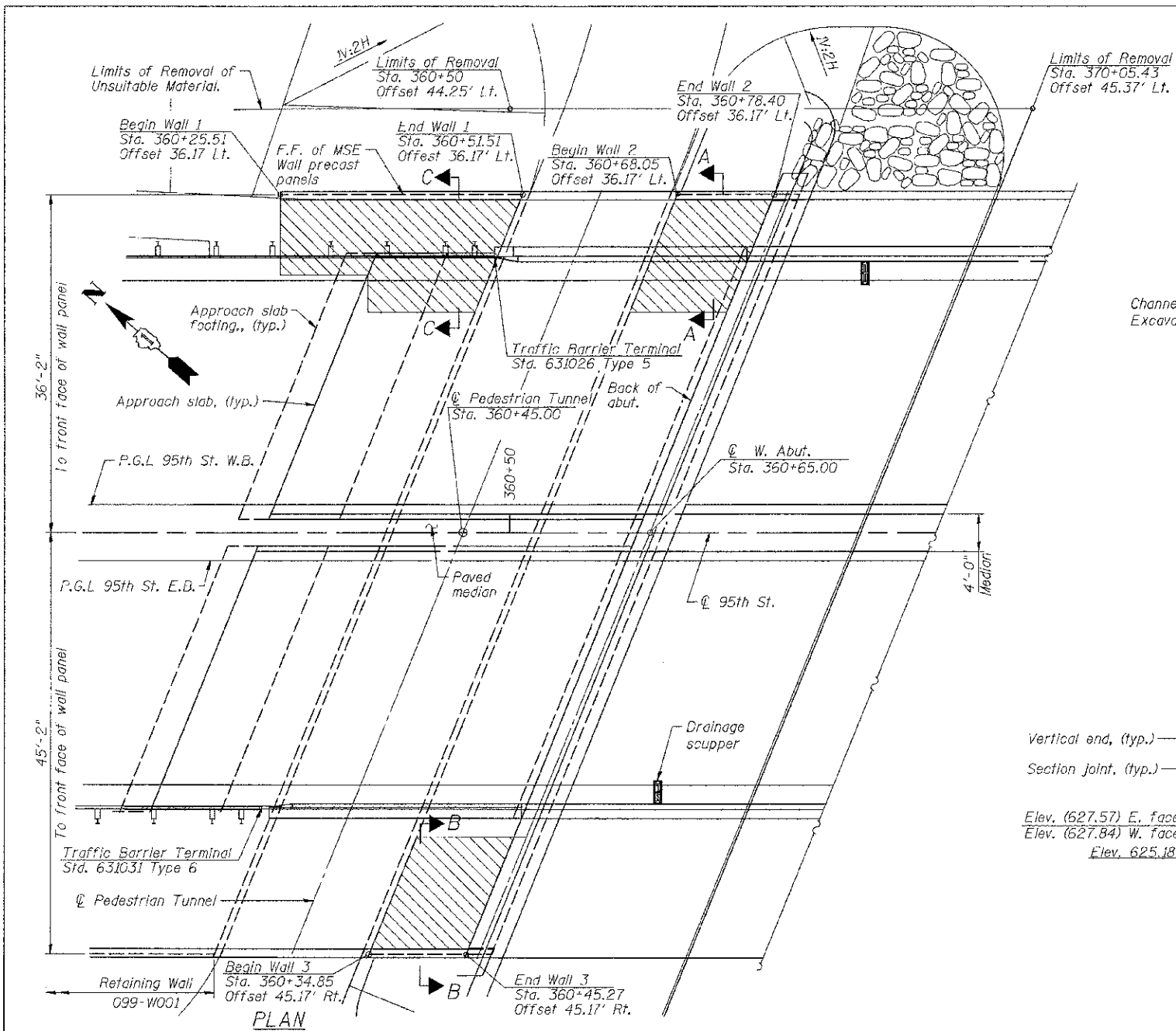
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WEST ABUTMENT DETAILS
STRUCTURE NO. 099-3035

SHEET NO. 26 OF 38 SHEETS

F.A.U. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1644	01-00181-00-PP	WILL	328	184
CONTRACT NO. 63647			ILLINOIS FED. AID PROJECT	



LEGEND

Indicates removal of unsuitable material and placement of Porous Granular Embankment (Special)

- Notes**
- For Sections A-A, B-B and C-C see sheet MSE Retaining Walls 1, 2 & 3 - At W. Abutment - 2
 - See sheet Precast Tunnel Details for details of tunnel.
 - The precast units and cast-in-place sections shall be designed by the provider of the precast tunnel sections and be sealed by a Structural Engineer licensed in the State of Illinois.
 - The cost of design, precast tunnel sections, cast in place tunnel section with waterstop, concrete, reinforcement, sealing the joints between precast units and butyl rubber membrane waterproofing is included in the unit price of "Three Sided Precast and Cast in Place structure 10' x 14'".

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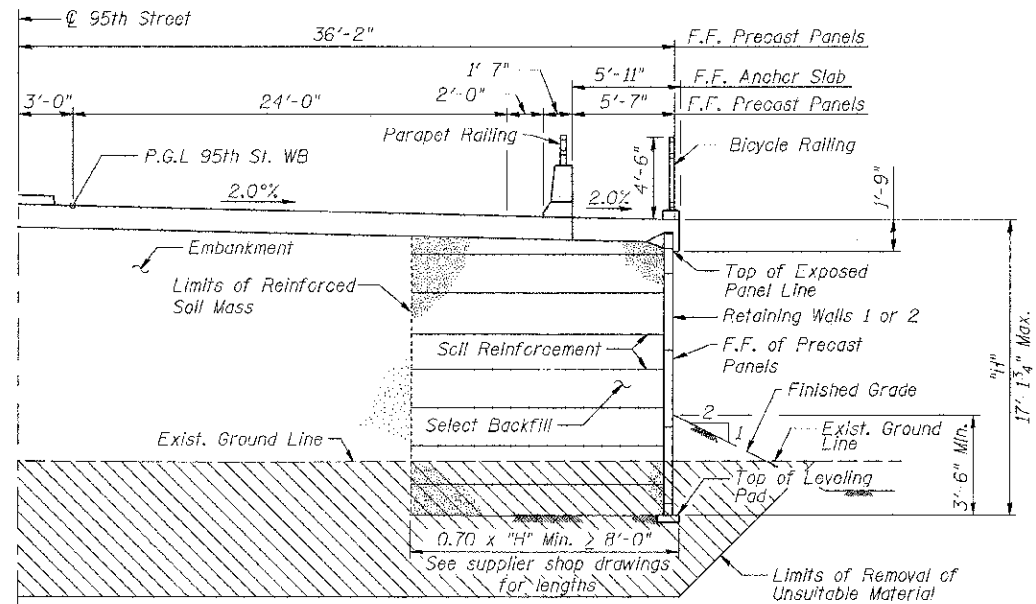
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

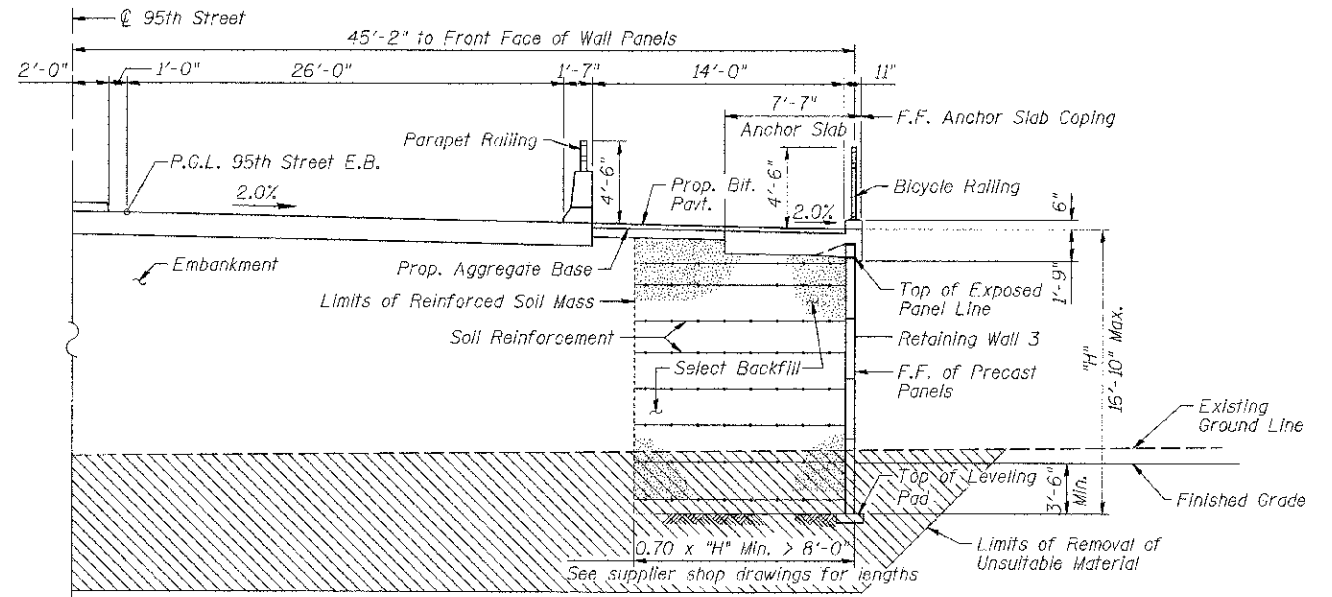
**MSE RETAINING WALLS 1, 2 & 3 - AT W. ABUTMENT - 1
STRUCTURE NO. 099-3035**

F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 63647				
ILLINOIS FED. AID PROJECT				

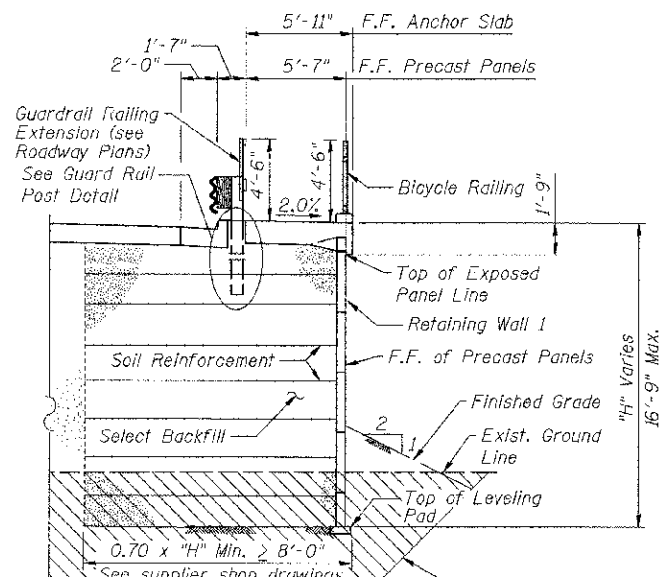
SHEET NO. 27 OF 38 SHEETS



SECTION A-A AT WALL 2
(Looking West)

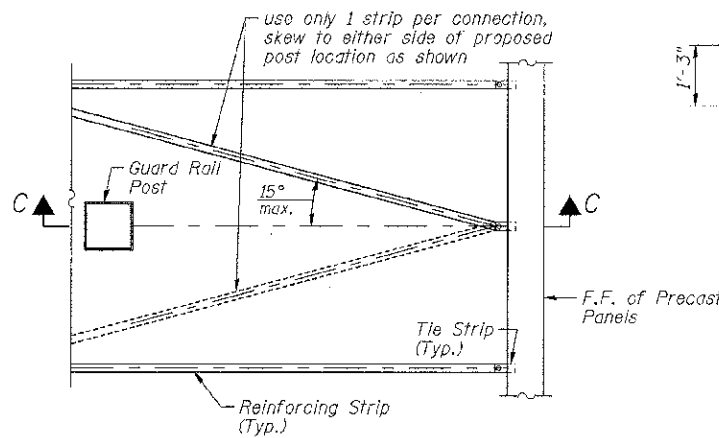


SECTION B-B AT WALL 3
(Looking East)

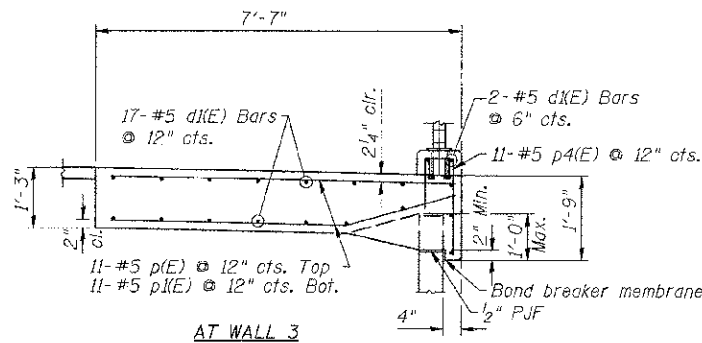
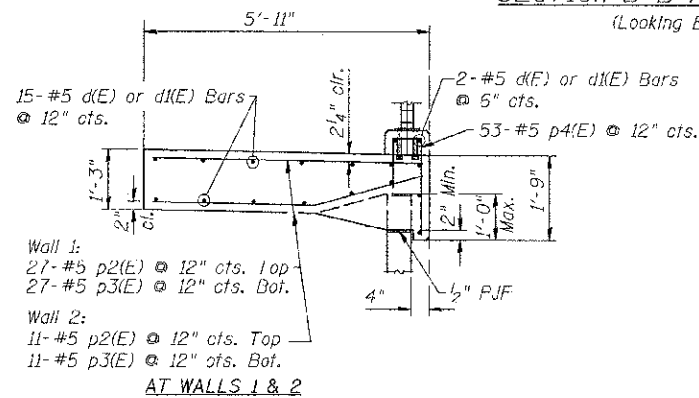


SECTION C-C AT WALL 1
(Looking West)

Guard Rail Post Note:
Prior to placing top row of reinforcing strips, the Contractor must locate all proposed guard rail post and mark them on back face of wall. Place interfering reinforcing strips as shown to provide room for future post installation.



GUARD RAIL POST DETAIL



ANCHOR SLAB DETAIL

LEGEND

Indicates removal and replacement of unsuitable material (See roadway plans)

BILL OF MATERIAL

Bar	Size	No.	Length	Shape
d(E)	#5	17	25'-8"	—
d(E)	#5	35	10'-0"	—
p(E)	#5	11	8'-8"	—
p(E)	#5	11	7'-4"	—
p2(E)	#5	38	7'-0"	—
p3(E)	#5	38	5'-8"	—
p4(E)	#5	64	2'-11"	—
Concrete Superstructure, Cu. Yd.				14.3
Reinforcement Bars, Epoxy Coated				Pound 1,710
Mechanically Stabilized Earth Retaining Wall				Sq. Ft. 499

The MSE wall supplier's internal stability design shall account for the anchor slab's bearing pressure surcharge of 1.5 ksf and horizontal sliding force of 0.5 kips/ft. of wall

Note:
For locations of Sections A-A, B-B and C-C See Sheet MSE Retaining Walls 1, 2 & 3 at Abutment - 1

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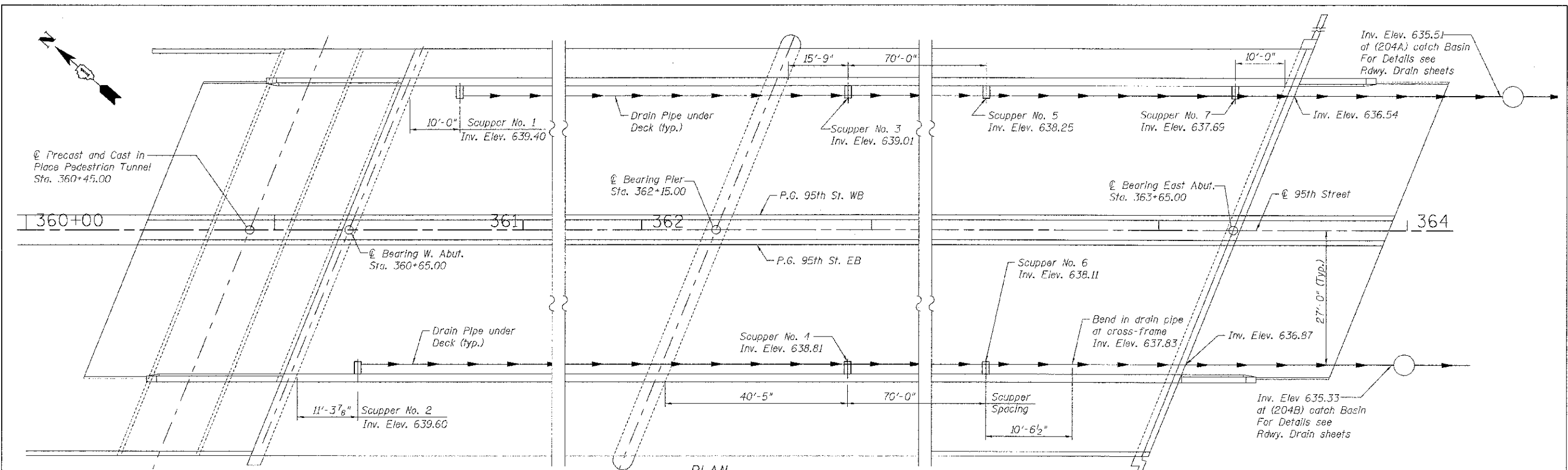
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	CHECKED - NPP	REVISED -

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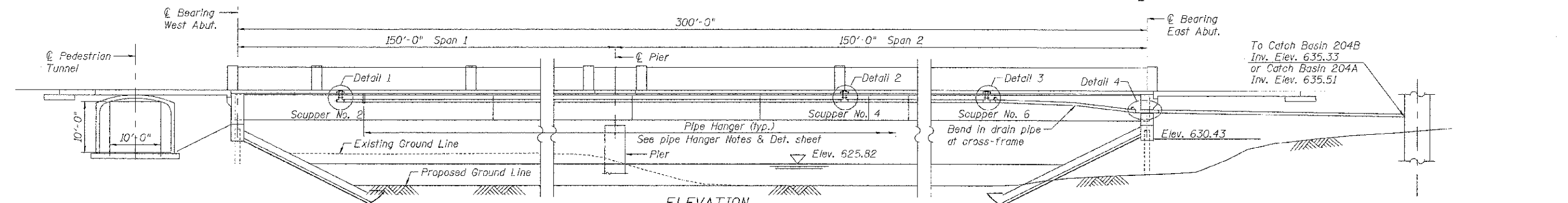
MSE RETAINING WALLS 1, 2 & 3 - AT W. ABUTMENT - 2
STRUCTURE NO. 099-3035

SHEET NO. 28 OF 38 SHEETS

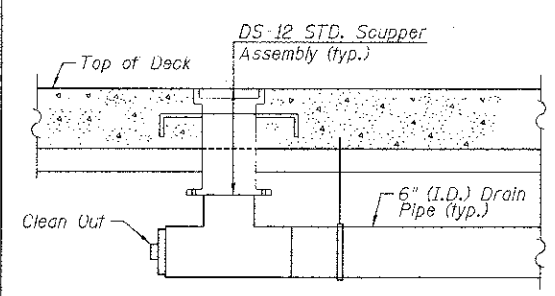
F.A.U. RTE. 1644	SECTION 01-00181-00-FP	COUNTY WILL	TOTAL SHEETS 328	SHEET NO. 186
CONTRACT NO. 63647			ILLINOIS FED. AID PROJECT	



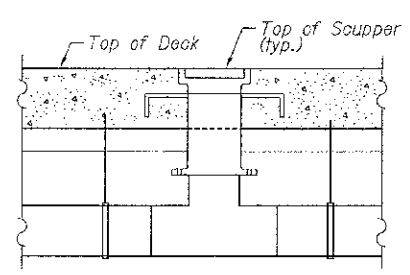
PLAN



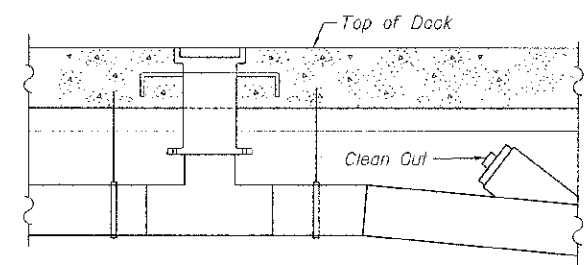
ELEVATION



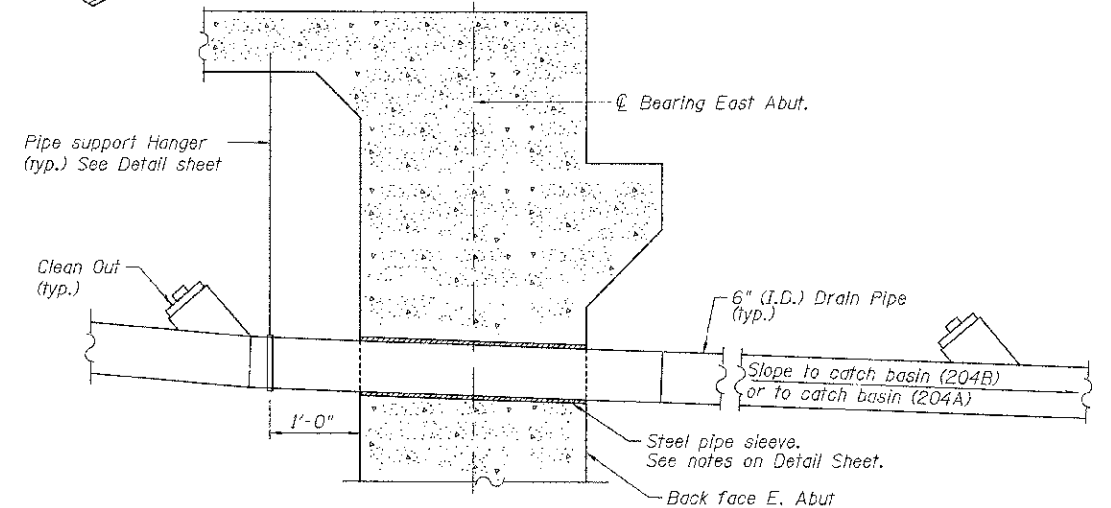
DETAIL 1
At Scupper No. 2
Scupper No. 1 similar



DETAIL 2
At Scupper No. 4
Scupper No. 3 & 5 similar



DETAIL 3
At Scupper No. 6
Scupper No. 7 similar

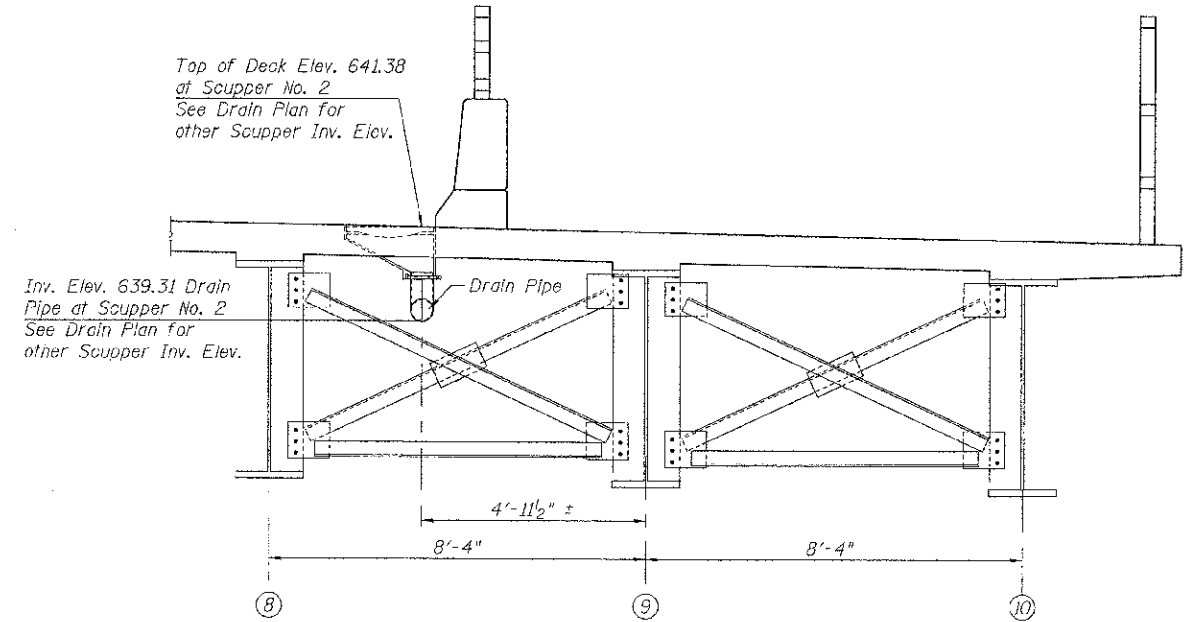
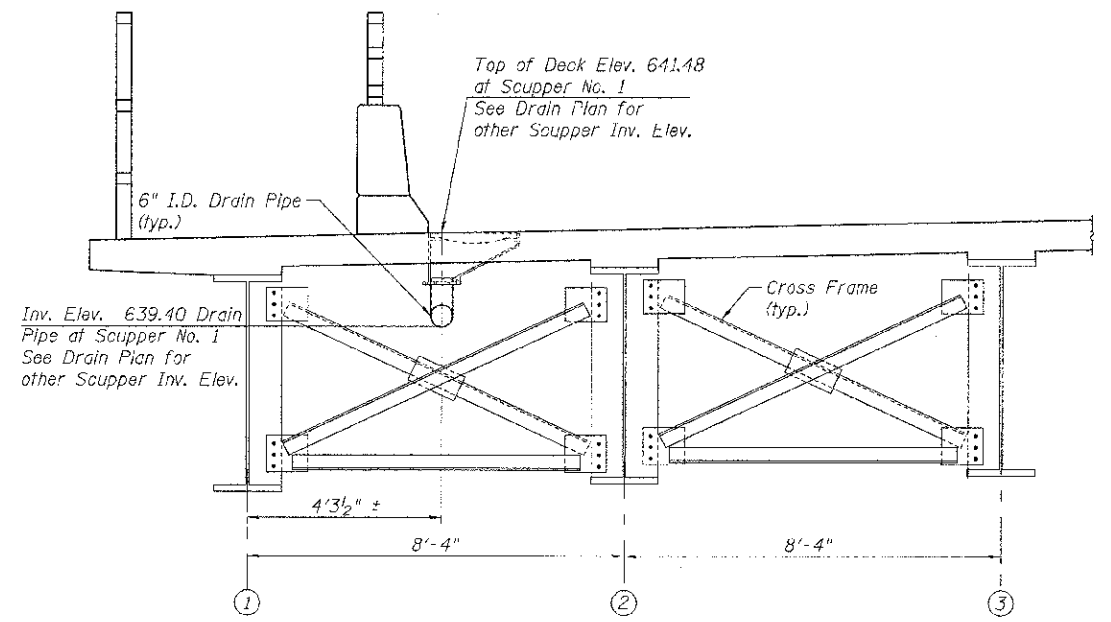


DETAIL 4
At C. Diaphragm both Sides
See sheet 17 for details

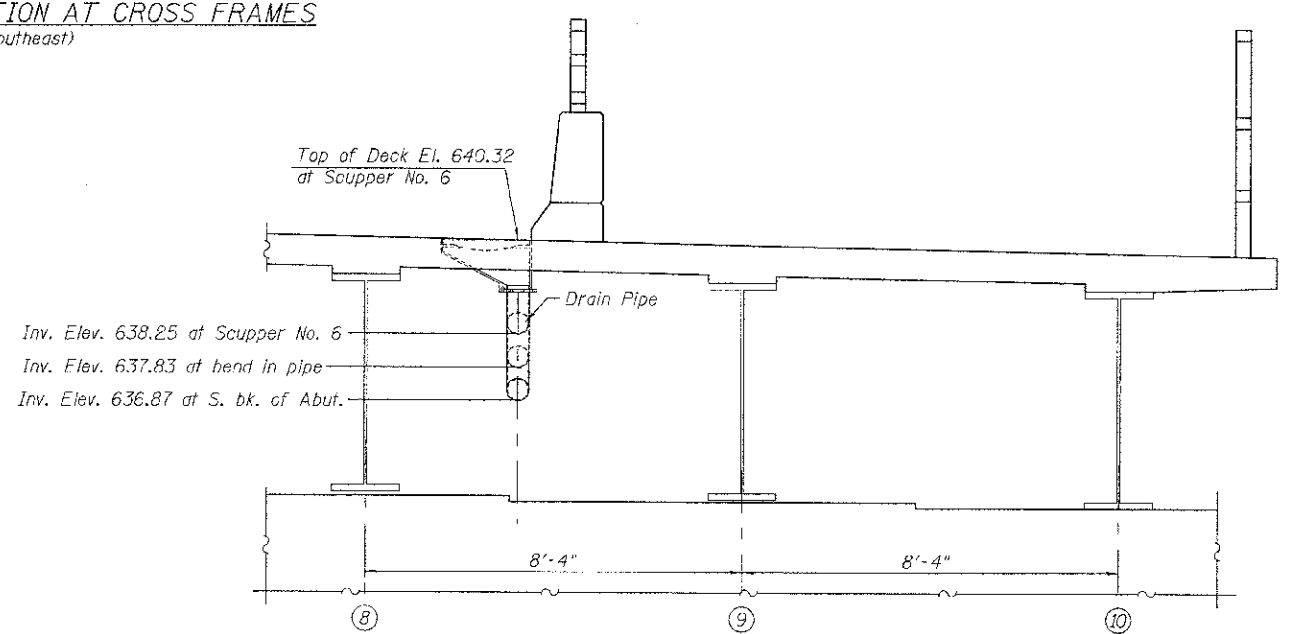
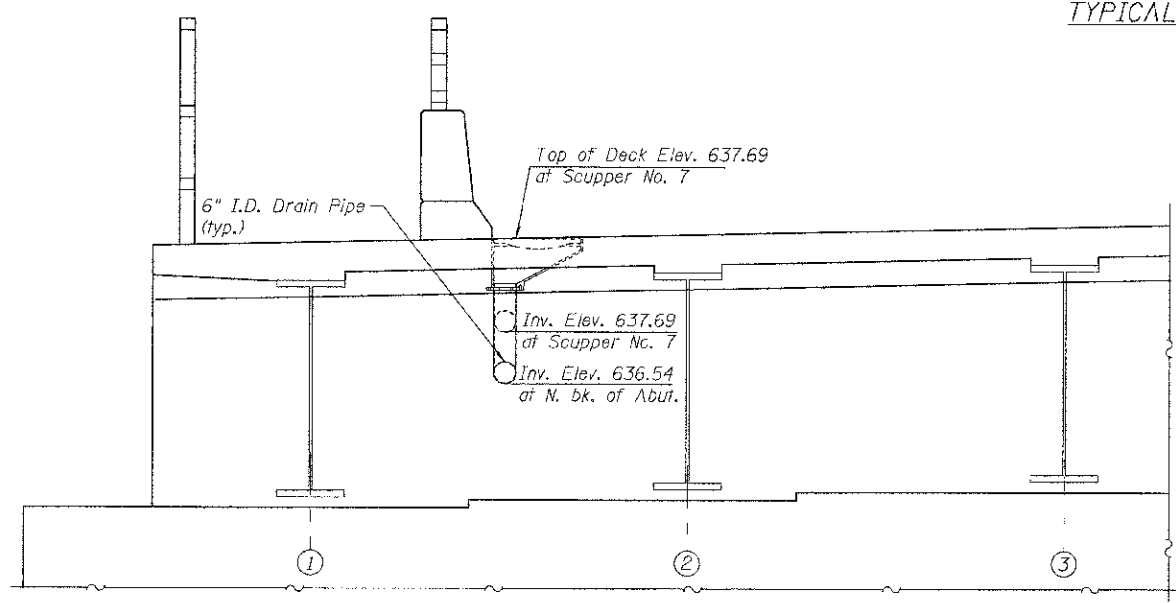
Note:
Quantity and locations of Pipe support Hangers and
Clean outs, shall be provided according to Special
Provision "Drainage System"

FILE NAME = q:\ncd\dot_205367381_95th\Drawings\contract\sub\Structures\Bridge 01\Plan\Drainage Plan & Details.dgn

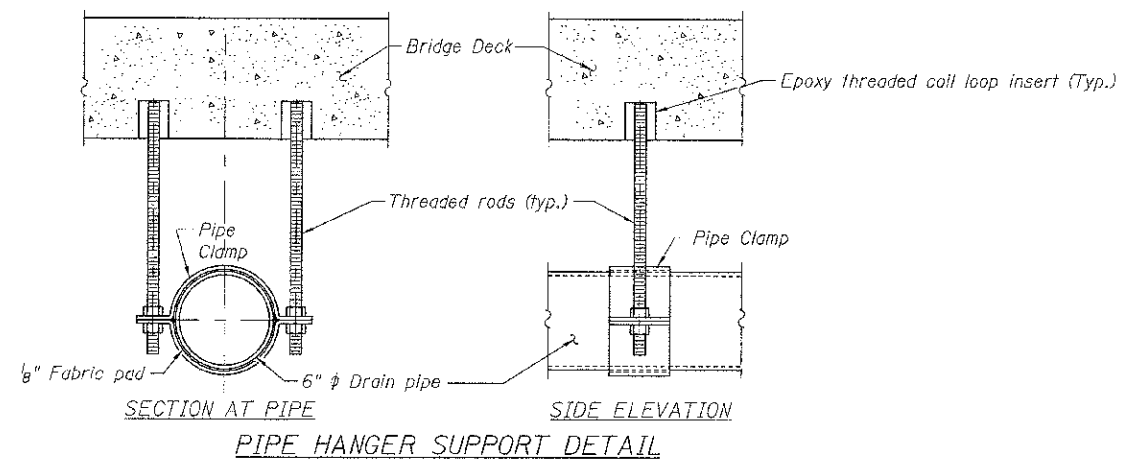
	100 S. WACKER DR. SUITE 500 CHICAGO, IL 60605 TEL (312) 939-1000 FAX (312) 939-4998	USER NAME = #5199 DESIGNED - STB CHECKED - NPP DRAWN - SOI CHECKED - NPP	REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BRIDGE DECK DRAINAGE PLAN AND DETAILS STRUCTURE NO. 099-3035 SHEET NO. 29 OF 38 SHEETS	F.A.J. RTE. 1644	SECTION 01-00181-GO-HP	COUNTY WILL	TOTAL SHEETS 328 SHEETS NO. 187	CONTRACT NO. 63647 ILLINOIS FED. AID PROJECT
	PLOT SCALE = 1/8" = 1' / IN. PLOT DATE = 10/16/2012									



TYPICAL CROSS SECTION AT CROSS FRAMES
(Looking Southeast)



TYPICAL CROSS SECTION AT E. ABUTMENT
(Looking Southeast)



PIPE HANGER SUPPORT DETAIL

Notes:
 The maximum spacing of hangers and supports for the Bridge Drainage system shall be 5'-0".
 For Sections and more Details see Bridge Deck Drainage Plan & Details
 The cost of fiberglass reinforced pipe with all supports, coil loop inserts, fittings, connections and clean-outs shall be included with "Bridge Drainage System".
 Provide steel pipe sleeve for 6" dia. pipe at abutment and seal the space between pipe and sleeve with grout.
 See roadway plans for drainage structures 204A and 204B.
 Provide expansion collar on fiberglass reinforced pipe for thermal movement between bridge pipe and pipe extending beyond abutment.

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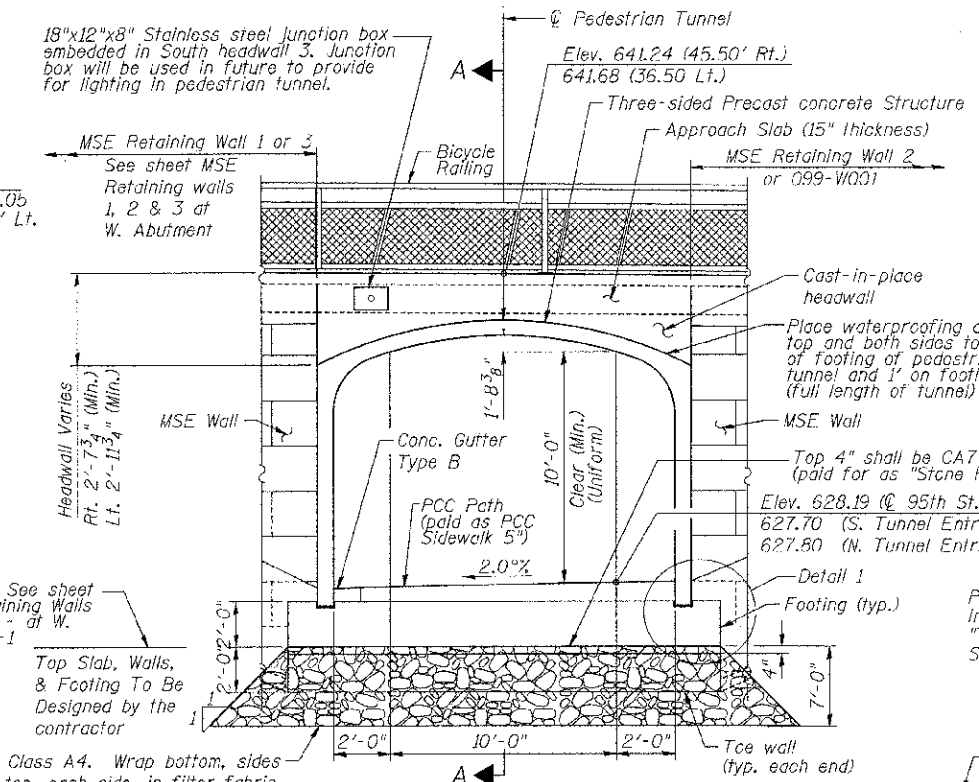
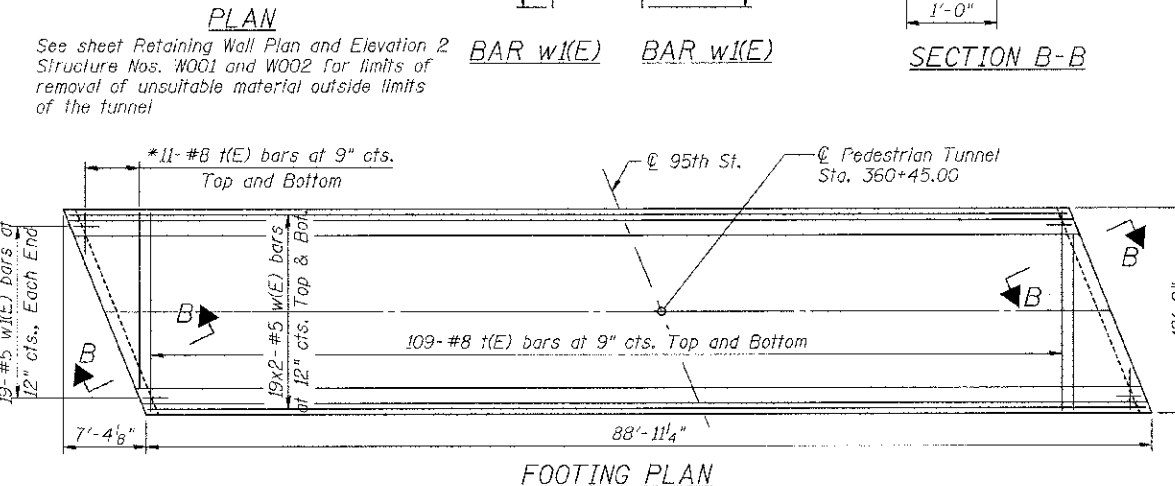
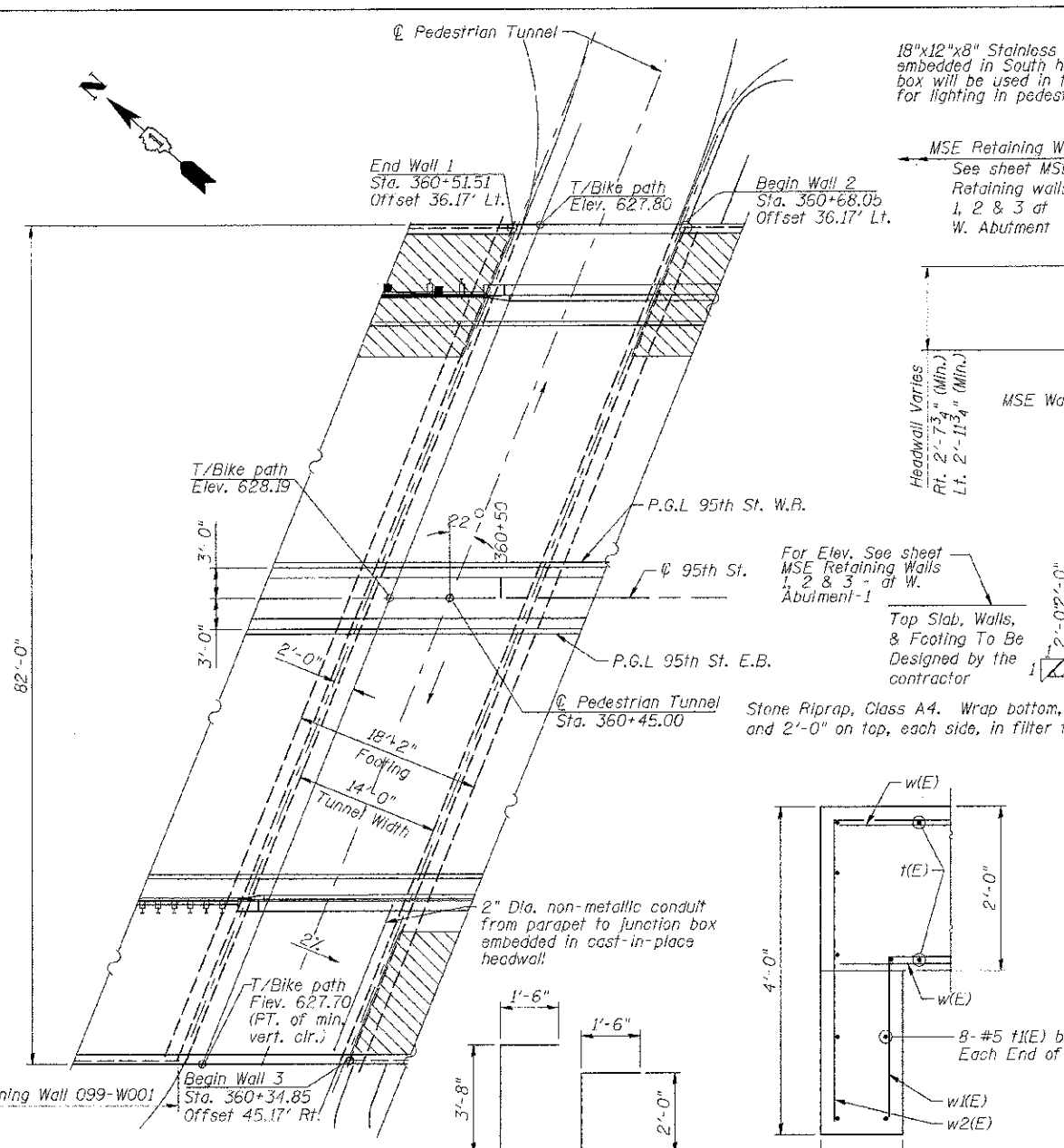
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USER NAME = STB#	DESIGNED - STB	REVISED -
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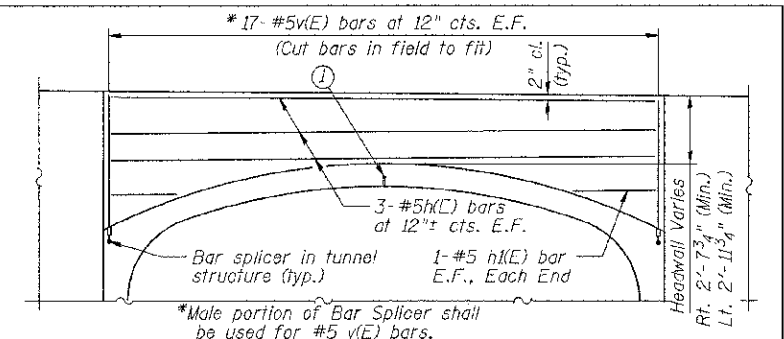
BRIDGE DECK DRAINAGE SECTIONS AND DETAILS
 STRUCTURE NO. 099-3035

F.A.J.L. R.T.E. 1644	SECTION 01-00181-00-FP	COUNTY WILL	TOTAL SHEETS 328	SHEET NO. 189
SHEET NO. 30 OF 30 SHEETS				CONTRACT NO. 63647
ILLINOIS FED. AID PROJECT				

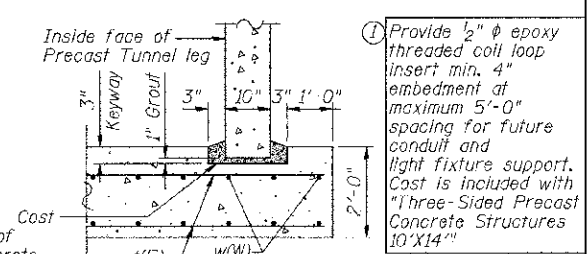


PEDESTRIAN TUNNEL ELEVATION
(Looking South)

Horizontal dimensions are at right angles
Note: Additional PCC Thickness for path to provide cross slope shall be considered included in the cost of the item PCC Sidewalk 5".



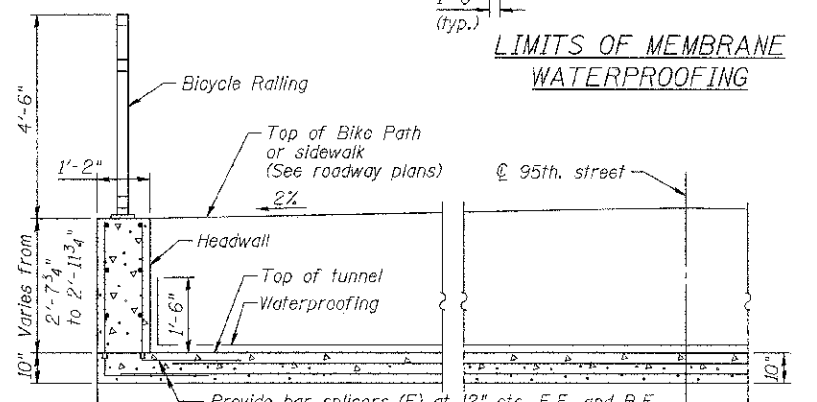
HEADWALL REINFORCEMENT
(Reinforcement is same for both headwalls)



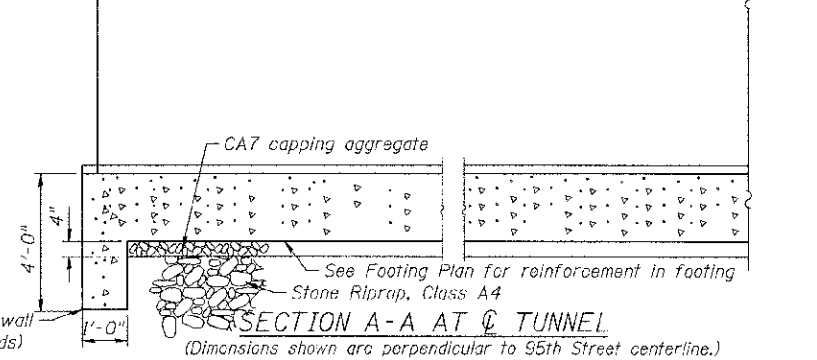
Notes:

- The option of using a precast footing is not allowed.
- All exposed concrete edges shall be chamfered 3/4" except as noted.
- All construction joints shall be bonded.
- Reinforcement Bars designated (E) shall be epoxy coated.
- The footing is based on the following maximum service reactions applied at the top of the footing: 31 kips/ft. (vertical), 2 kips/ft. (horizontal). The Contractor shall verify that the selected structure meets these design parameters. If the design parameters are exceeded a complete footing design with calculations, details, and the required seals shall be submitted for review and approval.
- The cost of backfill material and excavation necessary for the structure shall be included in the pay item Three-Sided Precast Concrete Structures 10'x14'. The backfill material shall be installed as noted in the provisions for Three-Sided Precast Concrete Structures 10'x14'. The backfill material gradation, compaction and installation method shall conform to the precast structure manufacturer's requirements.
- Provide waterproof joint between headwall and tunnel.
- The cost of providing waterproof joints and waterproofing the exterior surface of the Pedestrian Tunnel, as shown and as described in the Special Provisions, shall be included in the pay item Three-Sided Precast Concrete Structures 10'x14'.
- Drilling of holes in precast tunnel will not be permitted.
- See sheet MSE Retaining walls 1, 2 & 3 at w. abutment -1 for tunnel Key elevation.
- Construct precast sections of pedestrian tunnel first, working up grade from ends toward center of tunnel to achieve tight leakproof fit between segments. Construct cast in place section of tunnel after all precast segments have been installed.

LIMITS OF MEMBRANE WATERPROOFING



Provide bar splicers (E) at 12" cts. F.F. and B.F. for #5 v(E) bars. Cost included in the unit price of "Three-Sided Precast Concrete Structures 10'x14"



SECTION A-A AT 95th STREET
(Dimensions shown are perpendicular to 95th Street centerline.)

BILL OF MATERIAL

Bar	Size	No.	Length	Shape
t(E)	#8	240	17'-10"	—
h(E)	#5	16	19'-3"	—
w(E)	#5	76	45'-7"	—
w1(E)	#5	36	3'-6"	L
w2(E)	#5	36	5'-2"	L
h(E)	#5	12	16'-2"	—
h1(E)	#5	8	2'-1"	—
v(E)	#5	68	2'-9"	—
Concrete Structures			Cu. Yd.	124.7
Reinforcement Bars, Epoxy Coated			Pound	16,130
Three-Sided Precast Concrete Structures 10'x14'			Foot	89
Conduit Embedded in Structure, 2" dia., PVC			Foot	16
Junction Box, Stainless Steel, Embedded in Structure, 18"x12"x8"			Each	1
Stone Riprap, Class A4			Ton	954
Filter Fabric			Sq Yd	553

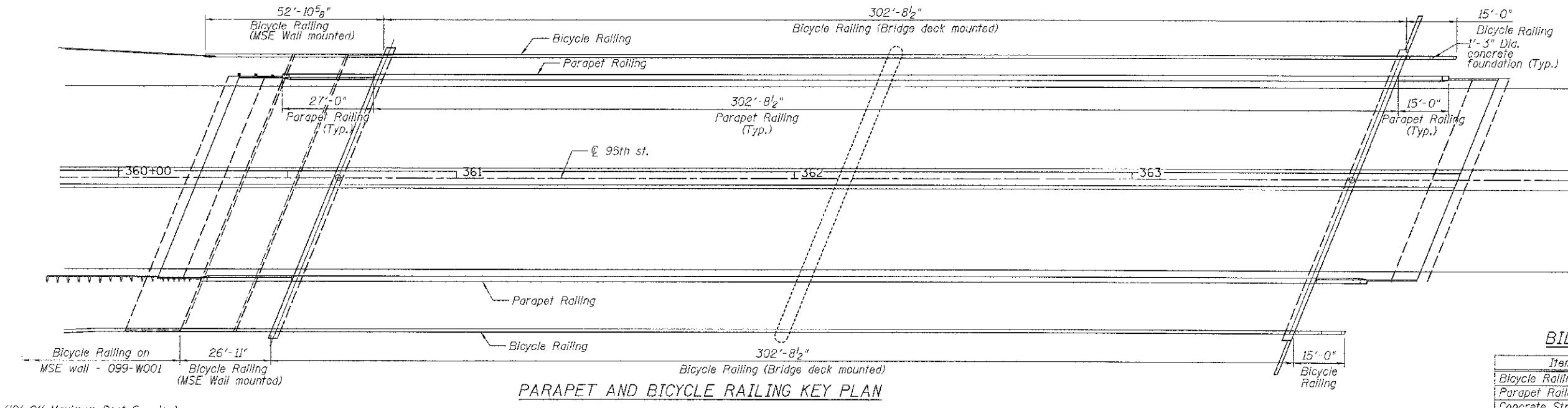
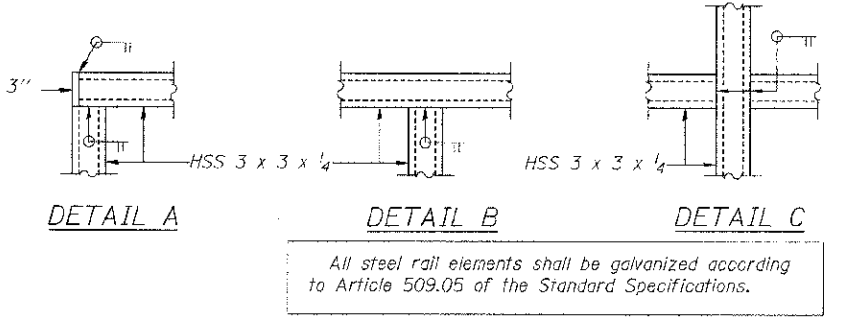
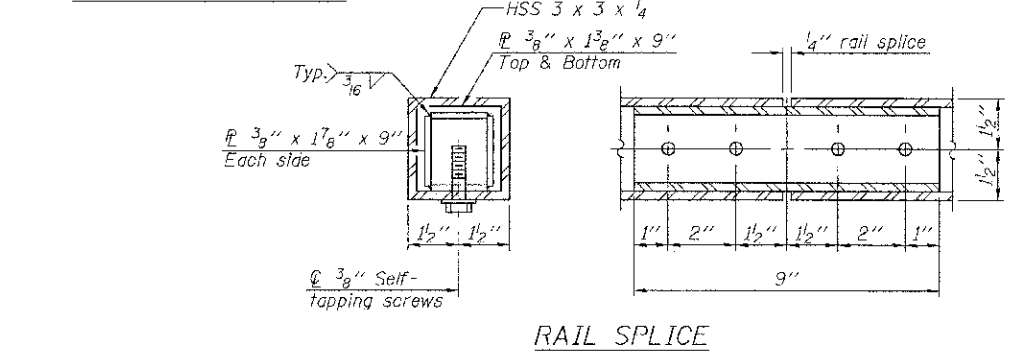
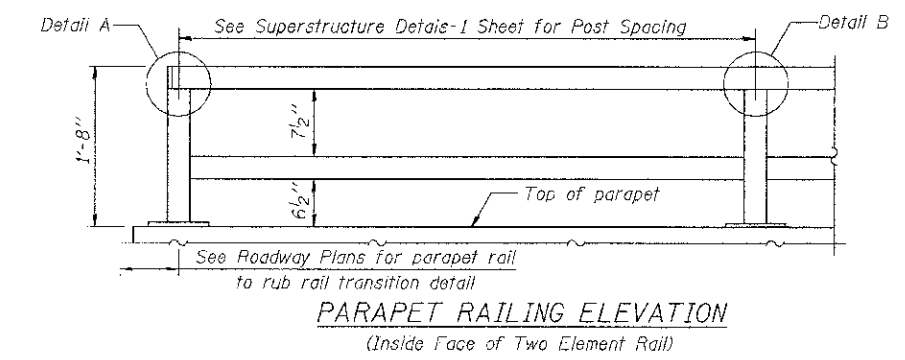
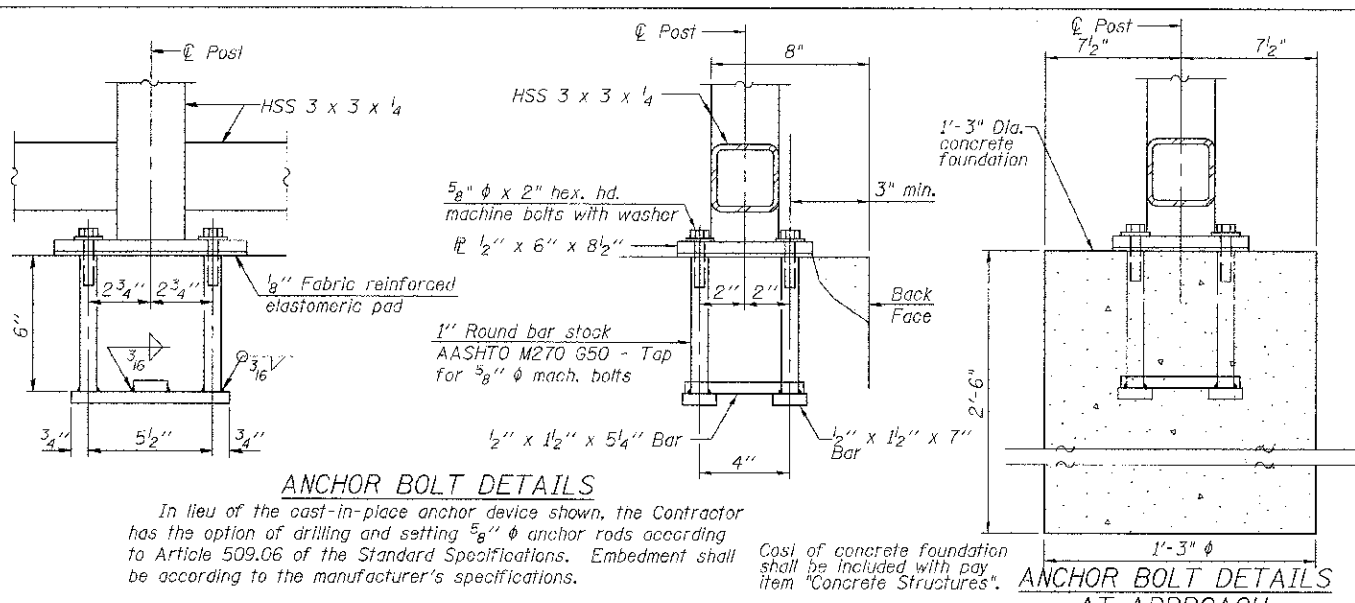
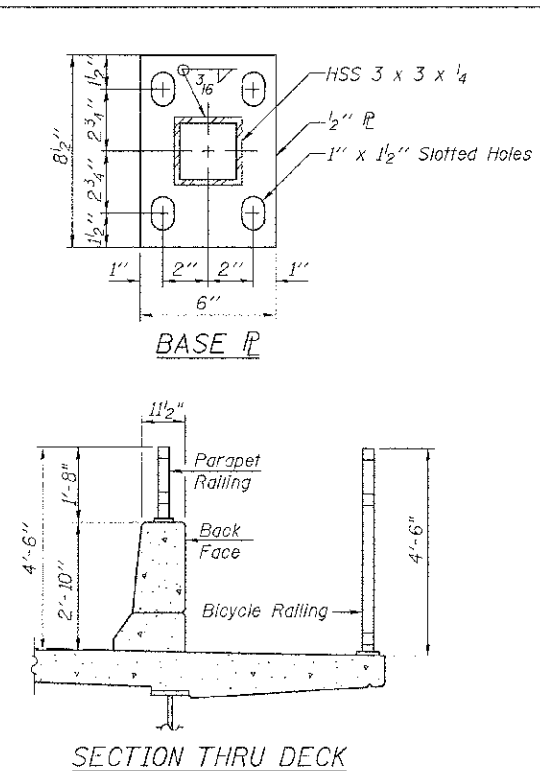
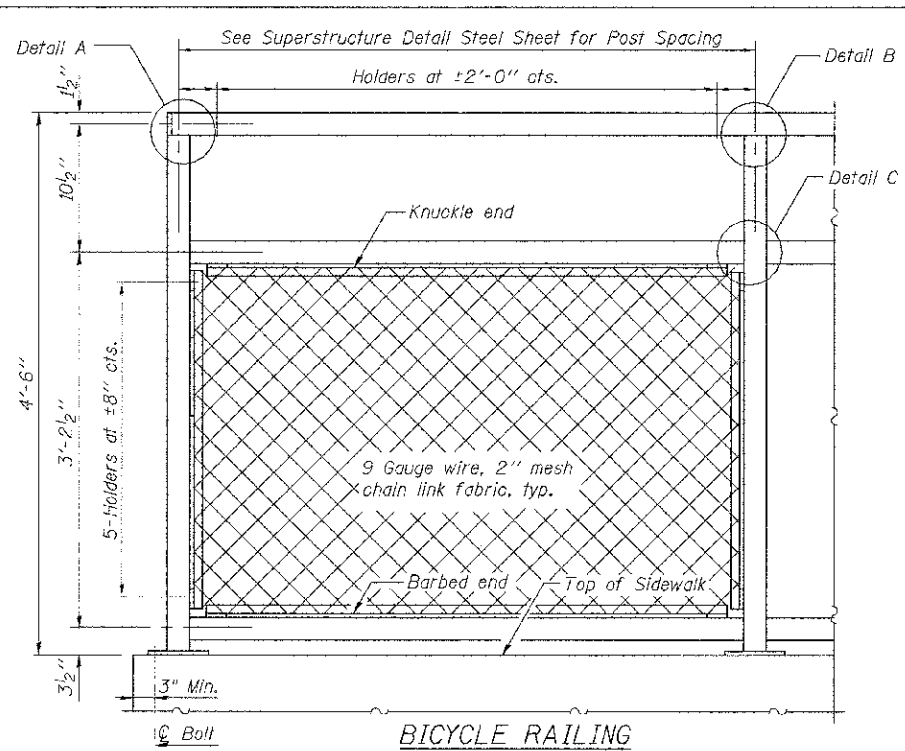
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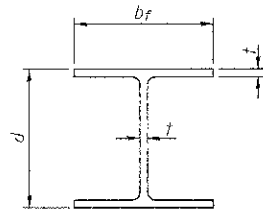
PRECAST TUNNEL DETAILS
STRUCTURE NO. 099-3035
SHEET NO. 31 OF 38 SHEETS

F.A.U. RTE. 1644	SECTION 01-00181-00-FP	COUNTY WILL	TOTAL SHEETS 328	SHEET 289
CONTRACT NO. 63647			ILLINOIS FED. AID PROJECT	



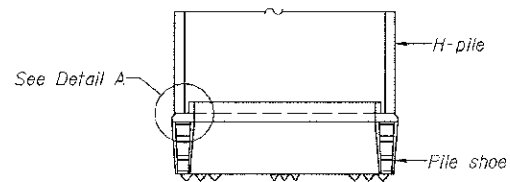
BILL OF MATERIAL

Item	Unit	Quantity
Bicycle Railing	Foot	116
Parapet Railing	Foot	690
Concrete Structures	Cu. Yd.	0.7

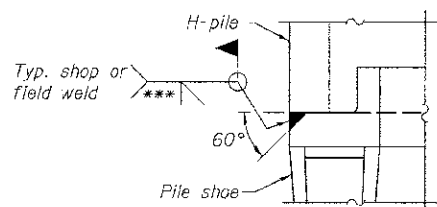


STEEL PILE TABLE

Designation	Depth d	Flange width bf	Web and Flange thickness t	Encasement diameter A
HP 14x117	14 1/4"	14 7/8"	1 3/16"	30"
x102	14"	14 3/4"	1 1/16"	30"
x89	13 7/8"	14 3/4"	5/8"	30"
x73	13 5/8"	14 5/8"	1/2"	30"
HP 12x84	12 1/4"	12 1/4"	1 1/16"	24"
x74	12 1/8"	12 1/4"	5/8"	24"
x63	12"	12 3/8"	1/2"	24"
x53	11 3/4"	12"	7/16"	24"
HP 10x57	10"	10 1/4"	9/16"	24"
x42	9 3/4"	10 1/8"	7/16"	24"
HP 8x36	8"	8 1/8"	7/16"	18"

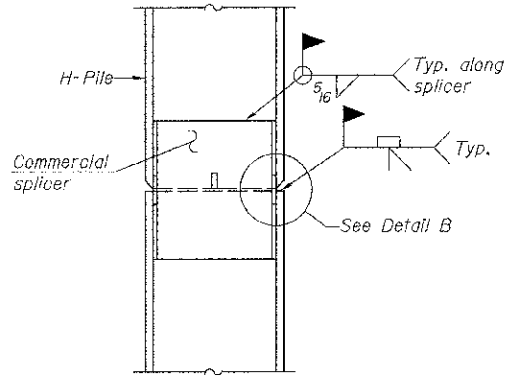


ELEVATION

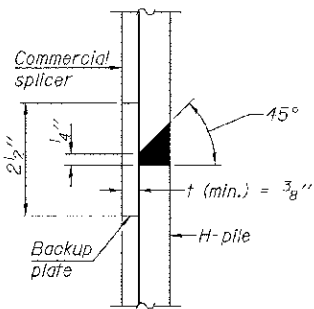


DETAIL A

H-PILE SHOE ATTACHMENT

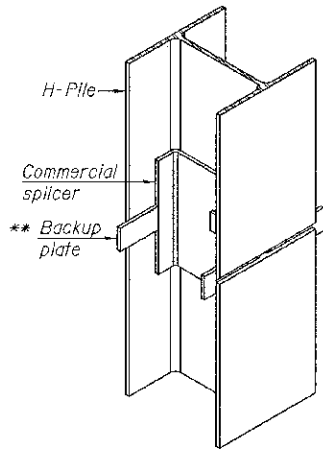


ELEVATION

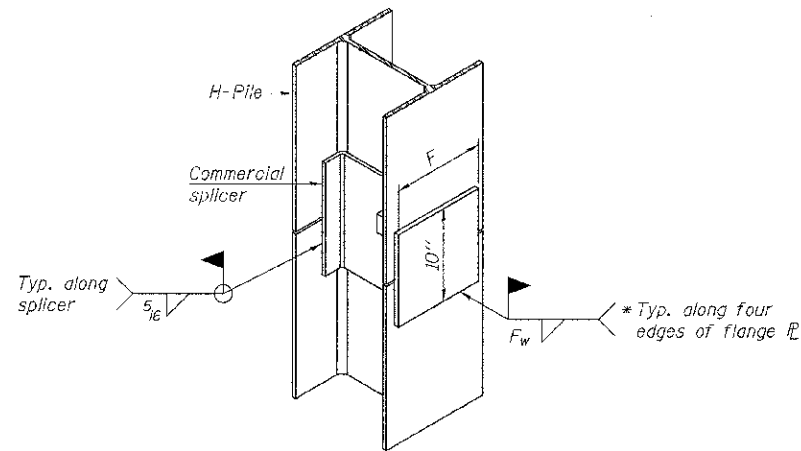


DETAIL "B"

WELDED COMMERCIAL SPLICE



ISOMETRIC VIEW



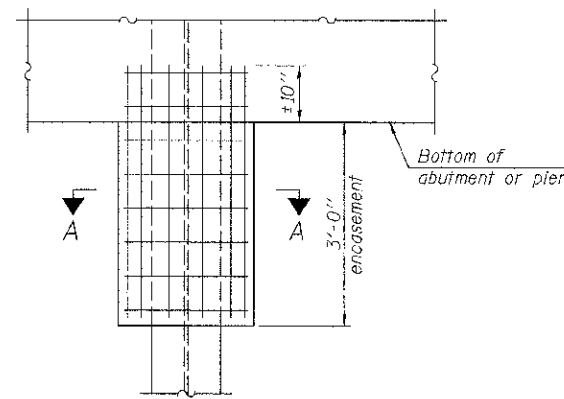
ISOMETRIC VIEW

WELDED COMMERCIAL SPLICE ALTERNATE

* Interrupt welds 1/4" from end of web and/or each flange.

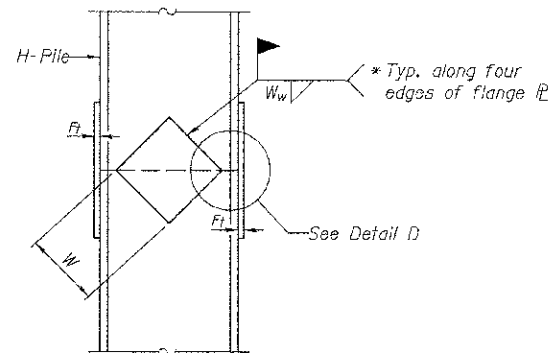
** Remove portions of backup plates that extend outside the flanges.

*** Weld size per pile shoe manufacturer (5/16" min.).



ELEVATION

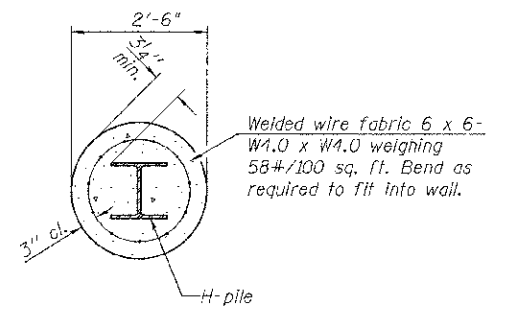
PILE ENCASUREMENT



ELEVATION

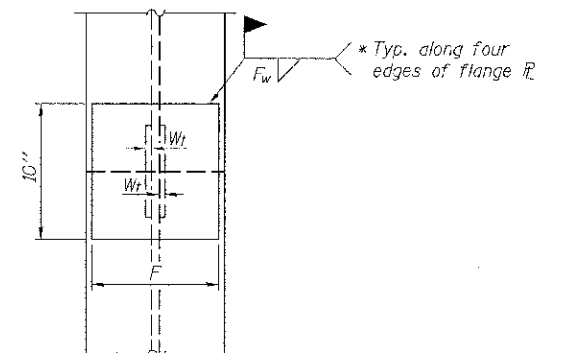
DETAIL D

WELDED PLATE FIELD SPLICE



SECTION A-A

Note: Forms for encasement may be omitted when soil conditions permit.



END VIEW

Designation	F	F _t	F _w	W	W _t	W _w
HP 14x117	12 1/2"	1"	7/8"	7 3/4"	5/8"	1/2"
x102	12 1/2"	7/8"	3/4"	7 3/4"	5/8"	1/2"
x89	12 1/2"	3/4"	1 1/16"	7 3/4"	5/8"	1/2"
x73	12 1/2"	5/8"	9/16"	7 3/4"	5/8"	1/2"
HP 12x84	10"	7/8"	1 1/16"	6 1/2"	5/8"	1/2"
x74	10"	7/8"	1 1/16"	6 1/2"	5/8"	1/2"
x63	10"	5/8"	2"	6 1/2"	1/2"	3/8"
x53	10"	5/8"	1/2"	6 1/2"	1/2"	3/8"
HP 10x57	8"	3/4"	9/16"	5 1/4"	1/2"	3/8"
x42	8"	5/8"	9/16"	5 1/4"	1/2"	3/8"
HP 8x36	7"	5/8"	7/16"	4 1/4"	1/2"	3/8"

Note: The steel H-piles shall be according to AASHTO M270 Grade 50.

FILE NAME = q:\vrs\d01_26367381_0544\Drawings\Contract\South\Structure\A\Structure\A\Bridg\Drawings\Control\Comp\AS-33 HP_Pile_Details.dgn

F-HP

7-1-10



100 S. WACKER DR.
SUITE 500
CHICAGO, IL 60606
TEL (312) 339-1000
FAX (312) 339-4198

USER NAME = *STB*
DESIGNED - STB
CHECKED - NPP
DRAWN - SOJ
PILOT SCALE = 8 1/4" = 1"
PILOT DATE = 10/16/2012

DESIGNED - STB
CHECKED - NPP
DRAWN - SOJ
CHECKED - NPP

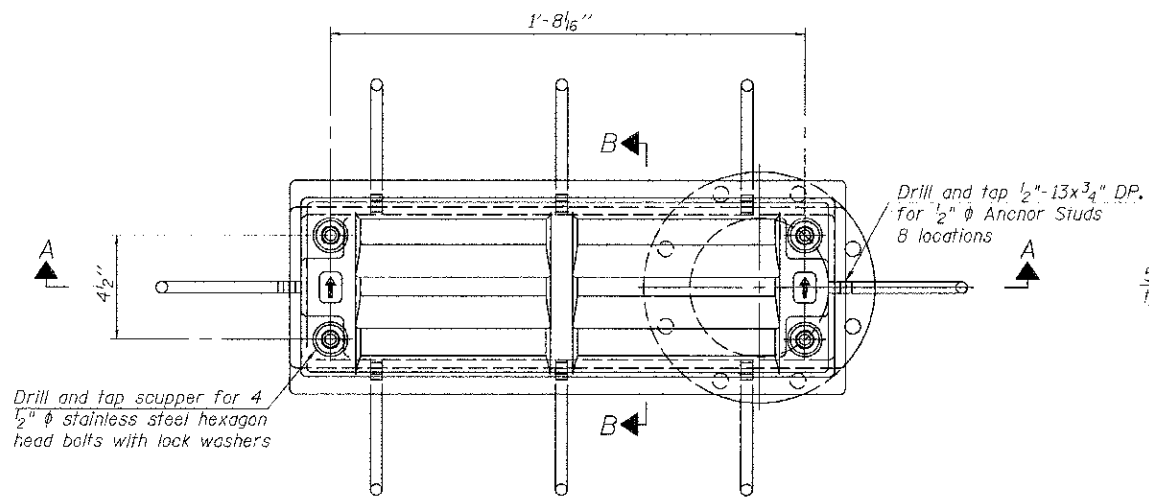
REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

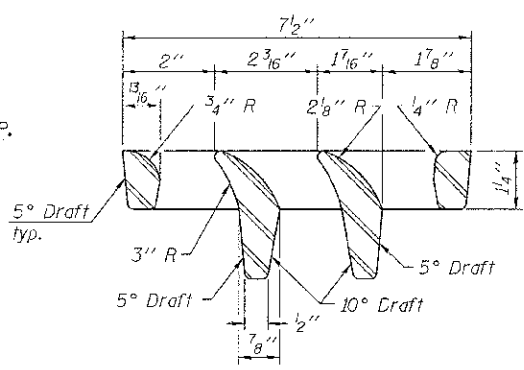
HP PILE DETAILS
STRUCTURE NO. 099-3035

SHEET NO. 33 OF 38 SHEETS

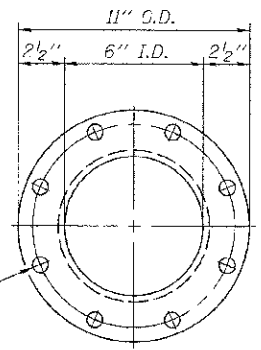
F.A.U. R.T.C.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1644	01-00181-00-FP	WILL	328	191
				CONTRACT NO. 63647
ILLINOIS FED. AID PROJECT				



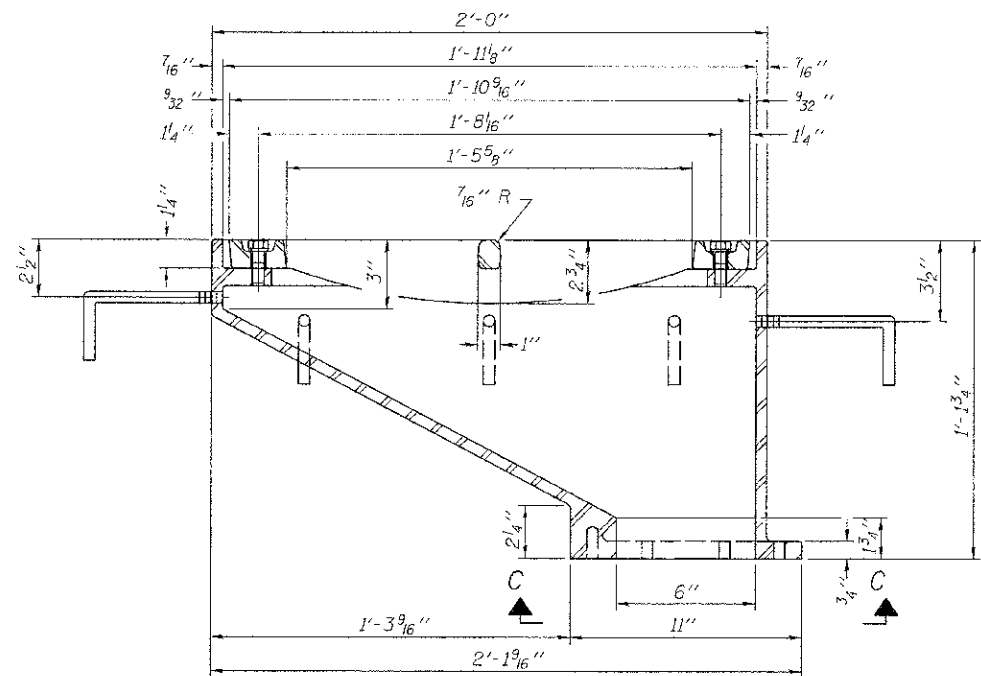
PLAN



VANE GRATE DETAIL

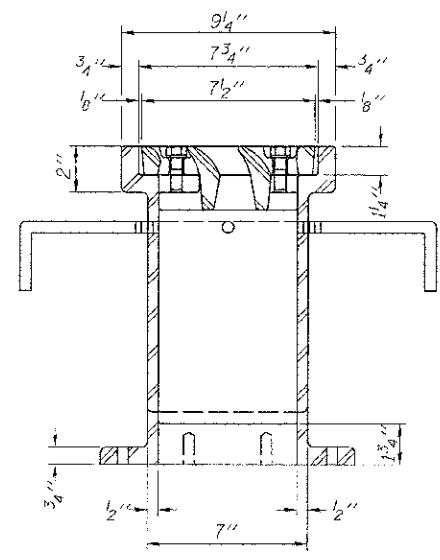


8- 9/16" ϕ holes on an 9 1/2" ϕ bolt circle

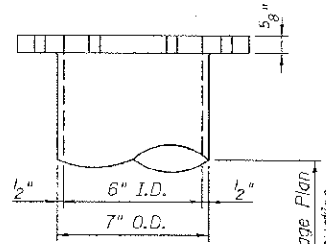


SECTION A-A

See sheet of for scupper location relative to parapet.

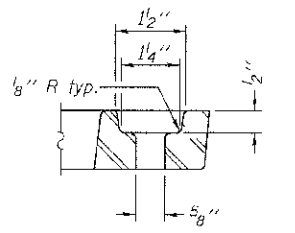


SECTION B-B

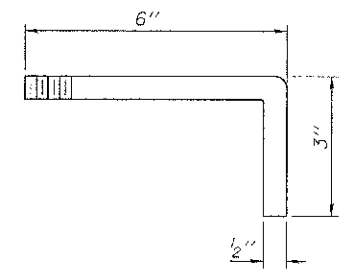


DOWNSPOUT

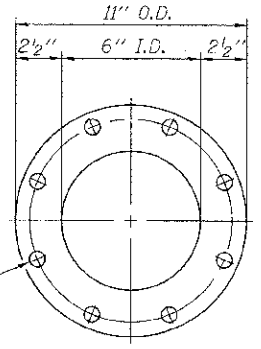
See Bridge deck Drainage Plan and details for continuation



BOLT HOLE DETAIL



ANCHOR STUD DETAIL



VIEW C-C

Drill and tap 6 holes for 2"-13 bolts on a 9 1/2" ϕ bolt circle. (2 blind holes are 1/4" deep, 6 thru holes)

Notes:
 All cast iron parts shall be gray iron conforming to the requirements of AASHTO M 105, Class 35B.
 Bolts, anchor studs, washers and nuts shall conform to the requirements of ASTM A 307 and shall be galvanized according to AASHTO M 232.
 Downspouts located on the exterior side of a painted steel fascia beam shall be painted with the finish coat specified for the exterior side of the fascia beam.
 As an alternate, bolts, anchor studs, washers and nuts may be stainless steel according to Article 1006.29(d) of the Standard Specifications.
 Structural steel weldments of equal sections and of the same configuration may be substituted for the cast iron scupper frame. Fillet or full penetration welds shall be used for the weldments. Details shall be submitted to the Engineer for approval. Structural steel weldments shall not be substituted for the cast iron scupper grate. Structural steel frames and downspouts shall be galvanized according to AASHTO M111.
 The Contractor shall take appropriate measures to assure that Protective Coat is not applied to the scupper.
 Cost of the Grate, Frame, Downspout, Anchor Studs, Bolts, Washers and Nuts including complete installation of the scupper shall be paid for at the contract unit price each for Drainage Scupper, DS-12.
 Alternate fiberglass downspout conforming to ASTM D 2996 with a short-time rupture strength hoop tensile stress of 30,000 psi min. may be used in lieu of the cast iron or steel equivalent.

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Drainage Scupper, DS-12	Each	7

DS-12

7-1-10



100 S. WACKER DR.
 SUITE 500
 CHICAGO, IL 60606
 TEL (312) 939-1000
 FAX (312) 939-4198

USER NAME = #STB#	DESIGNED - STB	REVISIONS -
PLOT SCALE = 1/4" = 1' / 1"	CHECKED - NPP	REVISIONS -
PLOT DATE = 10/16/2012	DRAWN - SOI	REVISIONS -
	CHECKED - NPP	REVISIONS -

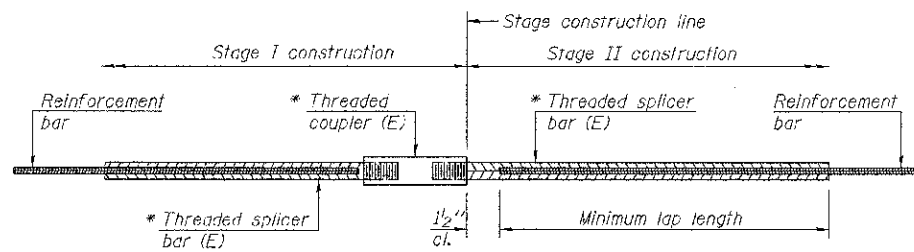
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

DRAINAGE SCUPPER, DS-12
 STRUCTURE NO. 099-3035

SHEET NO. 34 OF 38 SHEETS

F.A.J. RTE. 1644	SECTION C1-00181-00-FP	COUNTY WILL	TOTAL SHEETS 328	SHEET NO. 192
			CONTRACT NO. 63647	
ILLINOIS FED. AID PROJECT				

P:\web\25587381.dwg\drawing\contract\structures\bridge\Drawings\Drawings\34 Drainage Scupper Detail.dwg
 10/16/2012 10:58:31 AM
 P:\web\25587381.dwg\drawing\contract\structures\bridge\Drawings\Drawings\34 Drainage Scupper Detail.dwg



STANDARD BAR SPLICER ASSEMBLY

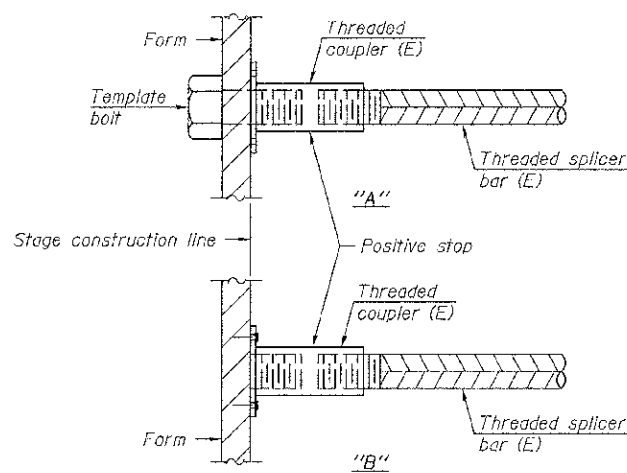
Bar size to be spliced	Minimum Lap Lengths				
	Table 1	Table 2	Table 3	Table 4	Table 5
3, 4	1'-5"	1'-11"	2'-1"	2'-4"	2'-3"
5	1'-9"	2'-5"	2'-7"	2'-11"	2'-10"
6	2'-1"	2'-11"	3'-1"	3'-6"	3'-4"
7	2'-9"	3'-10"	4'-2"	4'-8"	4'-6"
8	3'-8"	5'-1"	5'-5"	6'-2"	5'-10"
9	4'-7"	6'-5"	6'-10"	7'-9"	7'-5"

- Table 1: Black bar, 0.8 Class C
- Table 2: Black bar, Top bar lap, 0.8 Class C
- Table 3: Epoxy bar, 0.8 Class C
- Table 4: Epoxy bar, Top bar lap, 0.8 Class C
- Table 5: Epoxy bar, Top bar lap, Class B

Threaded splicer bar length = min. lap length + 1/2" + thread length

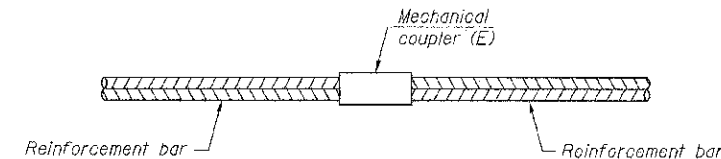
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length



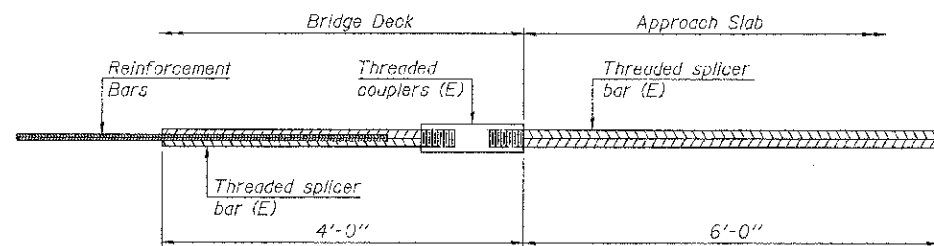
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



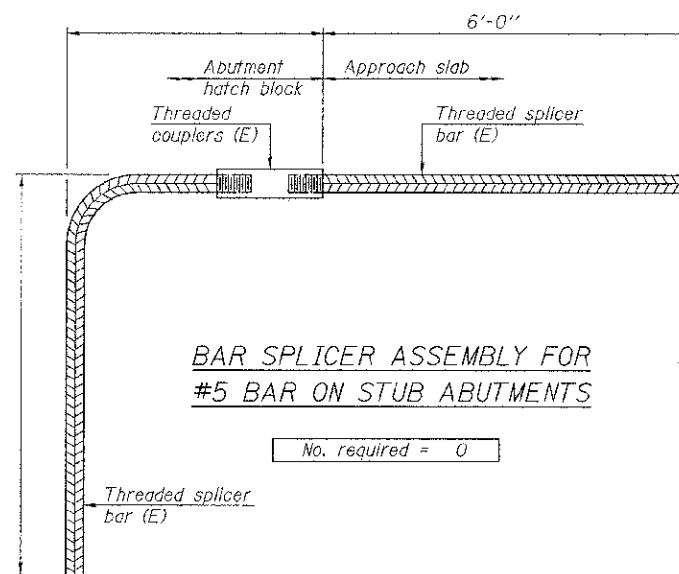
STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required
Deck	#5	0



BAR SPLICER ASSEMBLY FOR #5 BAR ON INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

No. required = 124



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required = 0

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See special provision for Mechanical Splicers.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

FILE NAME: c:\w\ad\01_25367\301_95\sh\drawings\contract\sp\sh\Structure\as\Bridge\BSPlicer\Assembly\Drawings\k1



Illinois Department
of Transportation
Division of Highways
Applied Geoscience

SOIL BORING LOG

Page 1 of 1

Date 10/7/10

ROUTE FAU 1644 DESCRIPTION 95th Street Over the DuPage River LOGGED BY KL

SECTION 01-00181-00-FP LOCATION New DuPage River Bridge, SEC. 12, TWP. 37N, RNG. 9E, 3rd PM

COUNTY Will DRILLING METHOD Hollow Stem Auger HAMMER TYPE

STRUCT. NO. Station	D E P T H	B L O W S	U C S	M O I S T	Surface Water Elev. ft	Stream Bed Elev. ft	Groundwater Elev.: First Encounter Upon Completion After Hrs.	D E P T H	B L O W S	U C S	M O I S T
099-3035 SB-S02 360+00 56.00ft RT 628.91	0										
	3										
	2										
625.41	0										
	2	0.2	34.0								
623.91	1	B									
	0										
	2	0.3	49.0								
	2	B									
620.41	5										
	8										
	11										
	4										
	13										
	22										
	5										
	13										
	35										
612.41	10										
	11	1.7	11.0								
	9	B									
	6										
	9	4.2	15.0								
	13	S									

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
BBS, from 137 (Rev. 8-99)

SB-S02



Illinois Department
of Transportation
Division of Highways
Applied Geoscience

SOIL BORING LOG

Page 1 of 1

Date 10/7/10

ROUTE FAU 1644 DESCRIPTION 95th Street Over the DuPage River LOGGED BY KL

SECTION 01-00181-00-FP LOCATION New DuPage River Bridge, SEC. 12, TWP. 37N, RNG. 9E, 3rd PM

COUNTY Will DRILLING METHOD Hollow Stem Auger HAMMER TYPE

STRUCT. NO. Station	D E P T H	B L O W S	U C S	M O I S T	Surface Water Elev. ft	Stream Bed Elev. ft	Groundwater Elev.: First Encounter Upon Completion After Hrs.	D E P T H	B L O W S	U C S	M O I S T
099-3035 SB-S03 360+47 47.00ft RT 628.91	0										
	2										
	3										
	2		36.0								
625.41	0										
	3	0.8	35.0								
	2	B									
622.91	0										
	1										
	2		88.0								
620.41	2										
	2										
	8										
	11										
	15										
	9										
	15										
	17										
	10										
	6										
	8										
610.91	6										
	8	1.6	12.0								
	9	B									

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
BBS, from 137 (Rev. 8-99)

SB-S03

FILE NAME = g:\work\20387281\95th\Drawings\contract\au\AS\Struct\au\B-ridge\DuPage\Control\Copy\AS_SB_S01_Borings_SB-S02&SB-S03.dgn



Illinois Department
of Transportation
Division of Highways
Applied GeoScience

SOIL BORING LOG

Page 1 of 1

Date 9/14/10

ROUTE FAU 1644 DESCRIPTION 95th Street Over the DuPage River LOGGED BY MG

SECTION 01-00181-00-FP LOCATION New DuPage River Bridge, SEC. 12, TWP. 37N, RNG. 9E, 3rd PM

COUNTY Will DRILLING METHOD Hollow Stem Auger HAMMER TYPE

STRUCT. NO.	D	B	U	M	Surface Water Elev.	D	B	U	M
Station	E	L	C	O	ft	E	L	C	O
	P	O	S	I	ft	P	O	S	I
BORING NO.	T	W	Qu	S	Groundwater Elev.:	T	W	Qu	S
Station	H	S		T	First Encounter	H	S		T
Offset	(ft)	(/6")	(tst)	(%)	Upon Completion	(ft)	(/6")	(tst)	(%)
Ground Surface Elev.					After Hrs.				
099-3035					621.2				
363+47									
45.00ft RT									
628.68									
FILL - Silty loam, topsoil, and gravel, dark brown	4		2.8	18.0					
	4	B							
626.68									
CLAY LOAM, trace gravel, brown, stiff	4		1.4	21.0					
	3	B							
624.68									
SAND AND GRAVEL, brown, medium dense	3								
	3								
622.68									
COARSE SAND, with cobbles, brown, medium dense	8								
	12								
	10								
	12								
	9								
	7								
	-10								
	8								
	10								
	9								
	6								
	9								
	10								
	8								
	-15								
	12								
612.68									
COARSE SAND, with fractured limestone, gray, dense to extremely dense	9								
	23								
	17								
	8								
	20								
	100								
608.68									
	-20								

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, from 137 (Rev. 8-99)

SB-S06



Illinois Department
of Transportation
Division of Highways
Applied GeoScience

SOIL BORING LOG

Page 1 of 1

Date 10/11/10

ROUTE FAU 1644 DESCRIPTION 95th Street Over the DuPage River LOGGED BY MG

SECTION 01-00181-00-FP LOCATION New DuPage River Bridge, SEC. 12, TWP. 37N, RNG. 9E, 3rd PM

COUNTY Will DRILLING METHOD Hollow Stem Auger HAMMER TYPE

STRUCT. NO.	D	B	U	M	Surface Water Elev.	D	B	U	M
Station	E	L	C	O	ft	E	L	C	O
	P	O	S	I	ft	P	O	S	I
BORING NO.	T	W	Qu	S	Groundwater Elev.:	T	W	Qu	S
Station	H	S		T	First Encounter	H	S		T
Offset	(ft)	(/6")	(tst)	(%)	Upon Completion	(ft)	(/6")	(tst)	(%)
Ground Surface Elev.					After Hrs.				
099-3035									
363+80					623.3				
45.00ft LT					624.0				
627.80									
TOPSOIL, sandy loam, black									
		3							
626.40									
FILL - Sandy loam, with gravel, brown, loose		3		13.0					
		3							
624.30									
SANDY LOAM, with gravel, brown-gray, very stiff		4		18.0					
		6	2.1						
		10	B						
		-5							
621.80									
COBBLES, with fractured limestone and sand, brown-gray, medium dense, wet		8							
		8							
		10							
		11							
		16							
		-10							
		22							
616.80									
FINE SAND, with gravel, light gray, medium dense		1							
		5							
		12							
614.30									
SAND AND GRAVEL, with fractured limestone, gray, vary dense to extremely dense, wet		9							
		11							
		100							
612.80									
BEDROCK, auger and spoon refusal at 15 feet									
End of Boring									
		-20							

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, from 137 (Rev. 8-99)

SB-S07

P:\I:\NAME = cp\wzab\203367381_95th\Drawings\Contract\Struct\sa\B-Edge (D:\Pepp)\Central\Copy\15-28 Sal. Boring (SB-06&SB-07).dgn



100 S. WACKER DR. SUITE 500 CHICAGO, IL 60606 TEL (312) 939-1000 FAX (312) 939-4998	USER NAME = *STB*	DESIGNED - STB	REVISED -
		CHECKED - NPP	REVISED -
	PLOT SCALE = 0.8" = 1' / 31.	DRAWN - SOI	REVISED -
	PLOT DATE = 10/16/2012	CHECKED - NPP	REVISED -

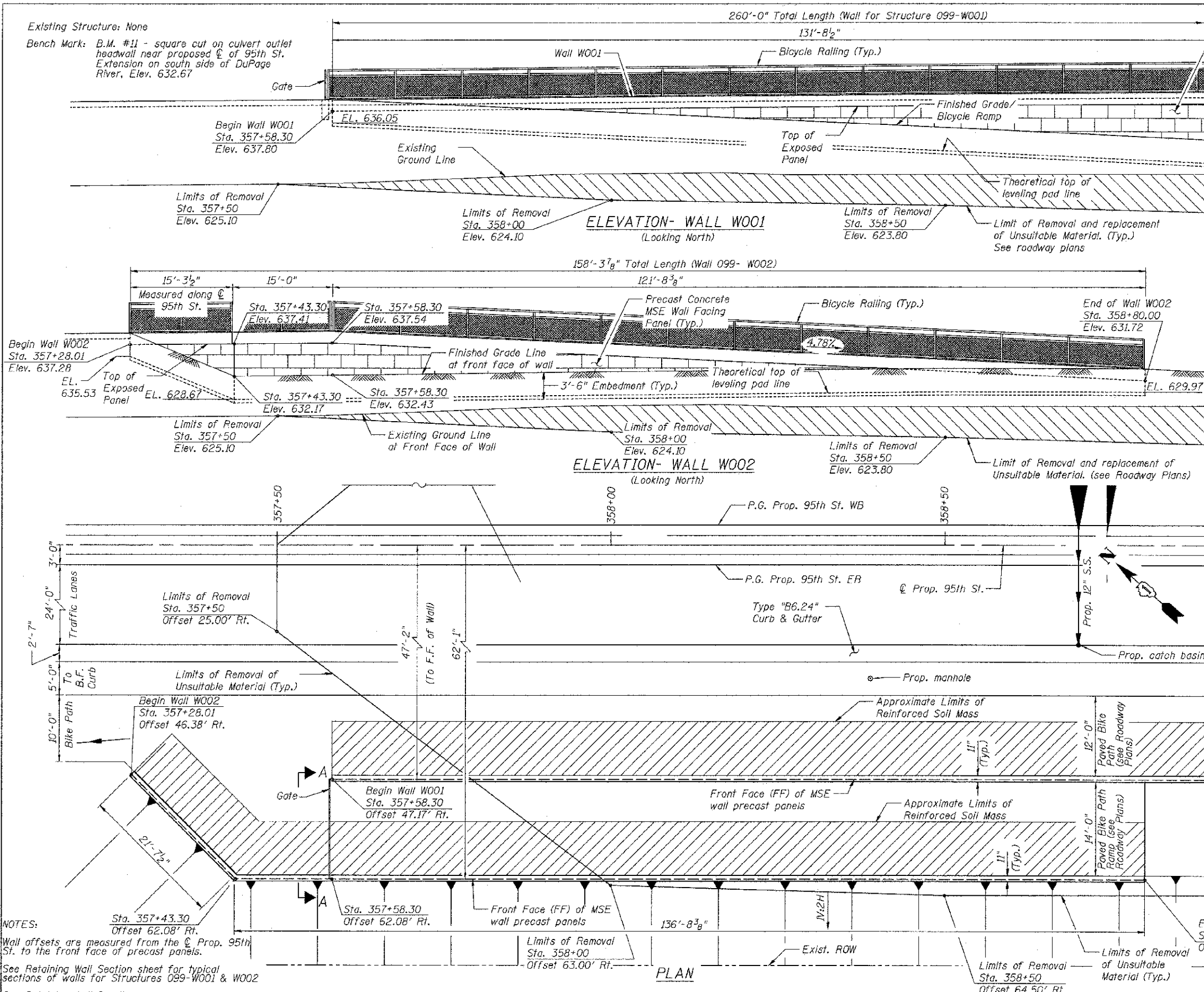
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOIL BORINGS - BORING NOS. SB-S06 AND SB-S07
STRUCTURE NO. 099-3035

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1644	01-00181-00-FP	Will	328	196
CONTRACT NO. 63647			ILLINOIS FED. AID PROJECT	

SHEET NO. 38 OF 38 SHEETS

Existing Structure: None
 Bench Mark: B.M. #11 - square cut on culvert outlet headwall near proposed E of 95th St. Extension on south side of DuPage River, Elev. 632.67



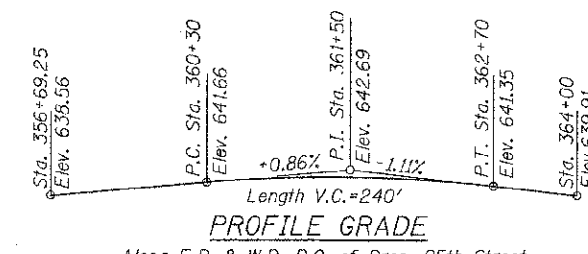
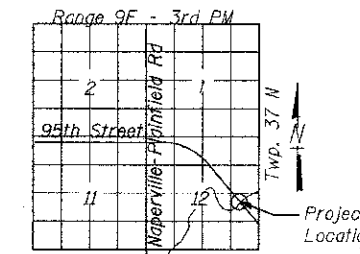
DESIGN SPECIFICATIONS
 2010 AASHTO LRFD Bridge Design Specifications (5th Edition)

DESIGN STRESSES
 FIELD UNITS
 f_c = 3,500 psi
 f_c = 4,500 psi (Precast Elements)
 f_y = 60,000 psi (reinforcement)
 f_y = 50,000 psi (M270, Grade 50)

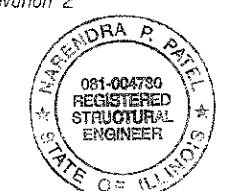
TOTAL BILL OF MATERIAL

Item	Unit	Estimated Quantity
Concrete Superstructure	Cu Yd	113
Reinforcement Bars, Epoxy Coated	Pound	16,650
Bicycle Railing	Foot	420
Mechanically Stabilized Earth Retaining Wall	Sq Ft	3,646
Gate, Special	Each	1

LEGEND
 [Hatched Area] Indicates removal and replacement of unsuitable material (see Roadway Plans)



- INDEX OF SHEETS**
- Retaining Wall Plan & Elevation 1
 - Retaining Wall Plan & Elevation 2
 - Retaining Wall Sections
 - Retaining Wall Details 1
 - Retaining Wall Details 2



Signature: *Narendra P. Patel*
 Current Date: October 19, 2012
 License Expires: November 30, 2014

I certify that to the best of my knowledge, information and belief, this retaining wall design is structurally adequate for the design loading shown on the plans. The design is an economical one for the style of structure and complies with the requirements of the current "AASHTO Standard Specifications for Highway Bridges".

RETAINING WALL PLAN & ELEVATION 1
 F.A.U. RTE. 1644
 SEC. 01-00181-00-FP
 WILL COUNTY
 STATION 357+28.01 TO 360+18.30
 STRUCTURE NOS. 099-W001 & W002

NOTES:
 Wall offsets are measured from the E of Prop. 95th St. to the front face of precast panels.
 See Retaining Wall Section sheet for typical sections of walls for Structures 099-W001 & W002
 See Retaining Wall Details 2 sheet for Section A-A



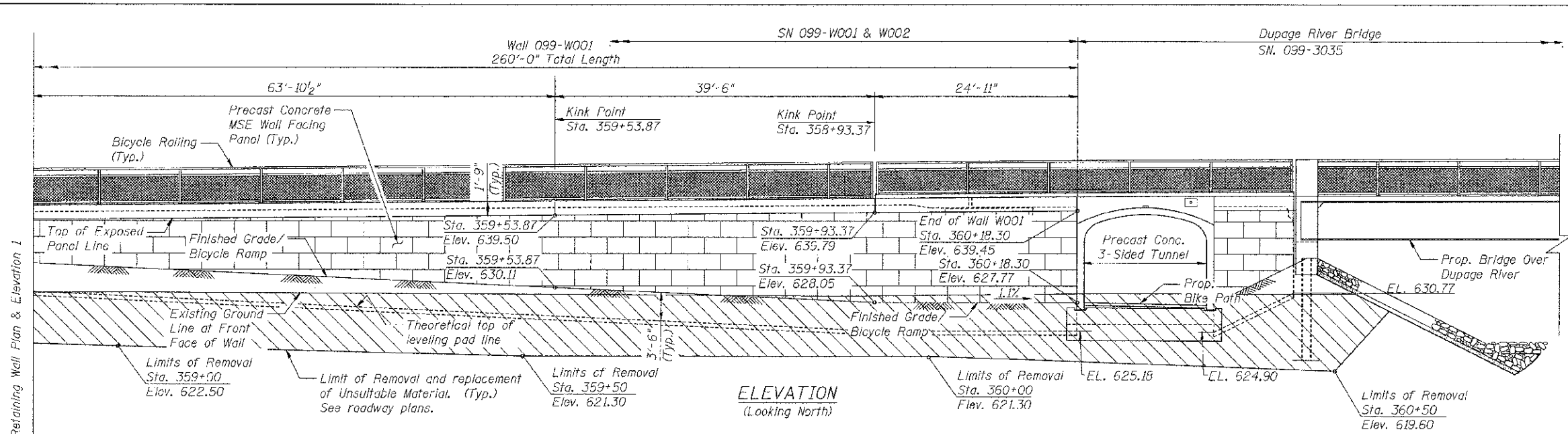
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CHECKED	NPP	REVISIONS
DRAWN	NPP	REVISIONS
CHECKED	NPP	REVISIONS

USER NAME = 95TB4
 PLOT SCALE = 7/8" = 1' IN.
 PLOT DATE = 10/16/2012

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1644	01-00181-00-FP	WILL	328	197

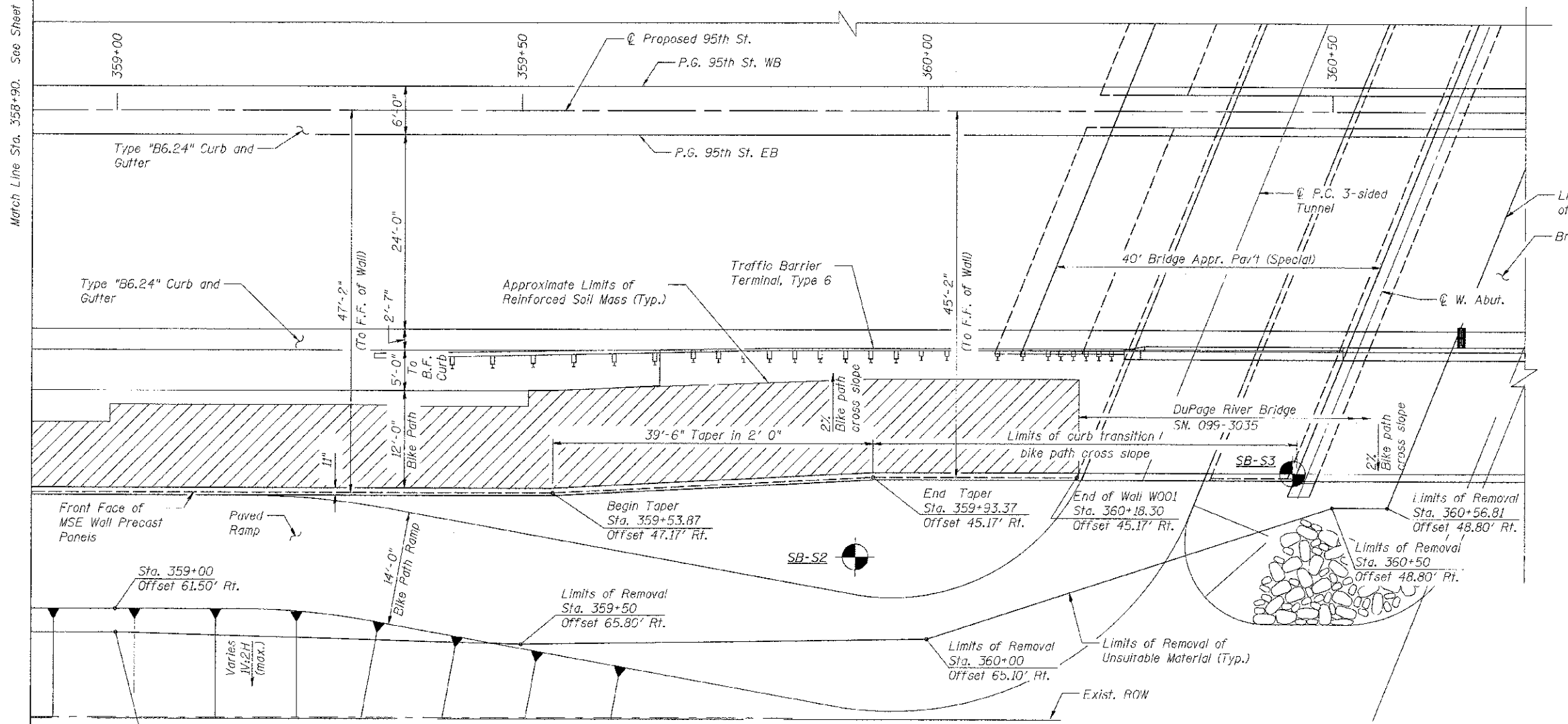
SHEET NO. 1 OF 5 SHEETS
 CONTRACT NO. 63647
 ILLINOIS FED. AID PROJECT



LEGEND

Indicates removal and replacement of unsuitable material. See roadway plans.

Contractor shall provide as part of shop drawings for pedestrian tunnel, headwalls and MSF wall, the joint details between these units. The joints shall be waterproof and keyed to prevent movement.



Match Line Sta. 358+90. See Sheet Retaining Wall Plan & Elevation 1

FILE NAME: q:\vwdoh\21387281_95th\vw\plans\contract\north\retaining\wall\plan & elevation.dwg

ELEVATION
(Looking North)

PLAN

NOTE: Wall offsets are measure from the \odot Prop. 95th St. to the front face of precast panels.



100 S. WACKER DR., SUITE 500
CHICAGO, IL 60606
TEL (312) 939-1000
FAX (312) 939-4998

USER NAME = *STD*	DESIGNED - STB	REVISED -
	CHECKED - NPP	REVISED -
PLOT SCALE = 7/8" = 1' / IN.	DRAWN -	REVISED -
PLOT DATE = 10/16/2012	CHECKED - NPP	REVISED -


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

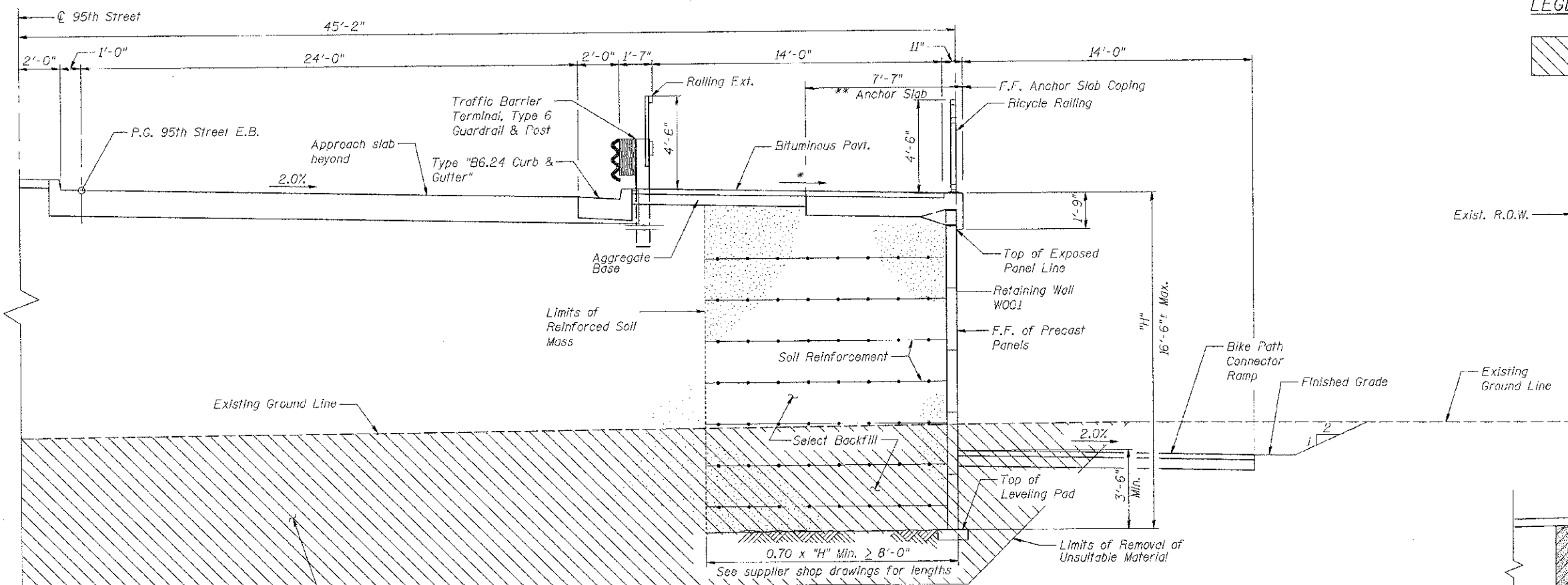
RETAINING WALL PLAN & ELEVATION 2
STRUCTURE NOS. 099-W001 & W002

SHEET NO. 2 OF 5 SHEETS

F.A.U. RTE. 1644	SECTION 01-00181-00-FP	COUNTY WILL	TOTAL SHEETS 328	SHEET NO. 198
CONTRACT NO. 63647				
ILLINOIS FED. AID PROJECT				

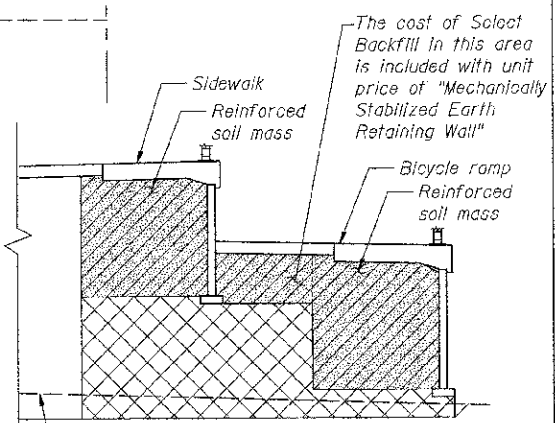
LEGEND

 Indicates removal and replacement of unsuitable material. See roadway plans





SECTION THRU WALL W001
(At Sta. 360+12.91 Typ. at End of Appr. Pavt. - Looking East)

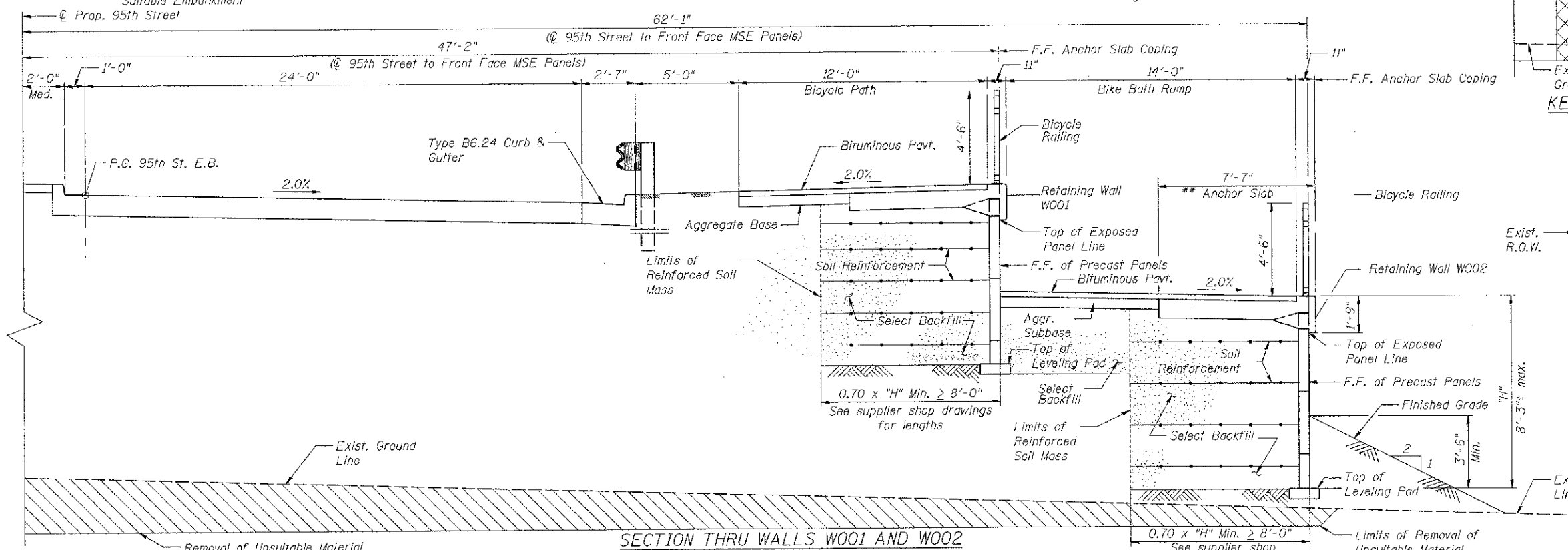
* See Retaining Wall Plan & Elevation 2 sheet for bike path cross slope and limits of curb transition.
** See Retaining Wall Details sheet for reinforcement




KEY ELEVATION- BACKFILL
See roadway plans for details

LEGEND

 Indicates Porous Granular Embankment, Subgrade
 Indicates Select Backfill for MSE walls



SECTION THRU WALLS W001 AND W002
(Sta. 357+58.30 to 358+80.00 Typ. Section - Looking East)

 100 S. WACKER DR., SUITE 500 CHICAGO, IL 60606 TEL (312) 939-1000 FAX (312) 939-4198	USER NAME - #STBR	DESIGNED - STB	REVISED -
	PLOT SCALE - 2 1/2" = 1' IN.	CHECKED - NPP	REVISED -
	PLOT DATE - 10/16/2012	DRAWN -	REVISED -
		CHECKED - NPP	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

RETAINING WALL SECTIONS
STRUCTURE NOS. 099-W001 & W002
SHEET NO. 3 OF 5 SHEETS

F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1644	01-00181-00-FF	WILL	328	199
			CONTRACT NO. 63647	
ILLINOIS FED. AID PROJECT				

FILE NAME - g:\vandal\2012\759L 95th St\95th St\95th St\retaining wall\03 Retaining Wall Section.dgn

