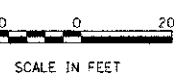
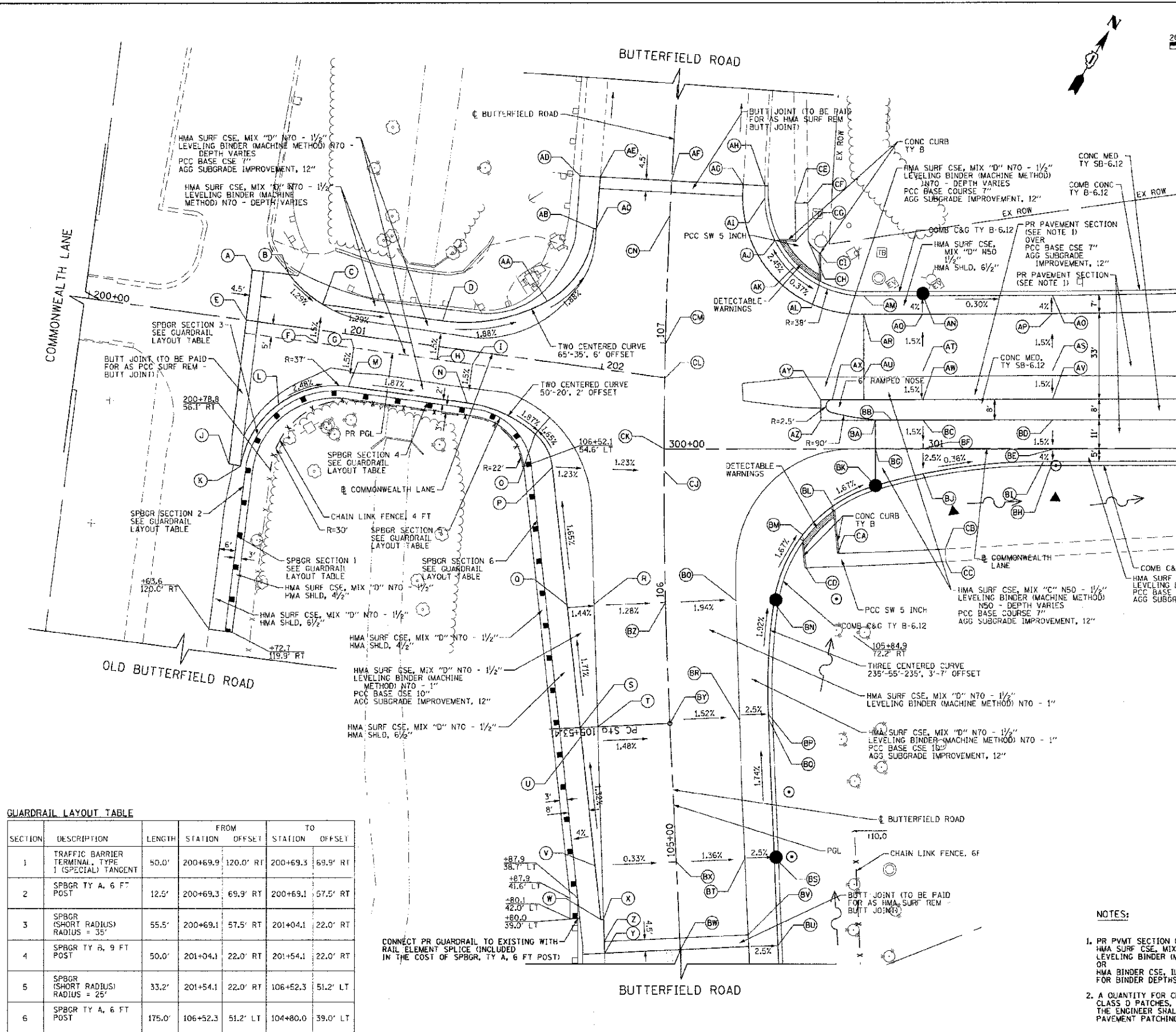


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| DATE | |
| BY | |
| REVIEWED | |
| NOISE CHECKED | |
| ALIGNMENT CHECKED | |
| RT. OF WAY CHECKED | |
| NO. | |

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| DATE | |
| BY | |
| REVIEWED | |
| NOISE CHECKED | |
| ALIGNMENT CHECKED | |
| RT. OF WAY CHECKED | |
| NO. | |



| SECTION | DESCRIPTION | LENGTH | FROM STATION | TO STATION | FROM OFFSET | TO OFFSET |
|---------|--|--------|--------------|------------|-------------|-----------|
| 1 | TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT | 50.0' | 200+69.9 | 200+69.9 | 120.0' RT | 69.9' RT |
| 2 | SPBGR TY A, 6 FT POST | 12.5' | 200+69.3 | 200+69.1 | 69.9' RT | 57.5' RT |
| 3 | SPBGR (SHORT RADIUS) RADIUS = 35' | 55.5' | 200+69.1 | 201+04.1 | 57.5' RT | 22.0' RT |
| 4 | SPBGR TY A, 9 FT POST | 50.0' | 201+04.1 | 201+54.1 | 22.0' RT | 22.0' RT |
| 5 | SPBGR (SHORT RADIUS) RADIUS = 25' | 33.2' | 201+54.1 | 106+52.3 | 22.0' RT | 51.2' LT |
| 6 | SPBGR TY A, 6 FT POST | 175.0' | 106+52.3 | 104+80.0 | 51.2' LT | 39.0' LT |

| STATION | OFFSET | ELEVATION |
|---------|----------|------------------|
| A | 200+61.2 | 19.4' LT 667.33 |
| B | 200+75.5 | 19.7' LT 667.24 |
| C | 200+90.0 | 10.0' LT 667.01 |
| D | 201+36.7 | 10.0' LT 666.41 |
| E | 200+61.5 | 0.0' RT 667.76 |
| F | 200+90.0 | 5.0' RT 667.24 |
| G | 201+04.1 | 5.0' RT 667.06 |
| H | 201+36.7 | 5.0' RT 666.64 |
| I | 201+50.4 | 5.0' RT 666.20 |
| J | 200+62.5 | 55.8' RT 668.32 |
| K | 200+67.1 | 56.0' RT 668.31 |
| L | 200+78.3 | 30.5' RT 667.54 |
| M | 201+04.1 | 20.0' RT 666.83 |
| N | 201+50.4 | 20.0' RT 665.97 |
| O | 106+64.6 | 49.8' LT 665.42 |
| P | 106+52.0 | 43.1' LT 665.66 |
| Q | 106+00.0 | 38.0' LT 666.95 |
| R | 106+00.0 | 27.5' LT 666.80 |
| S | 105+53.4 | 34.6' LT 667.35 |
| T | 105+53.4 | 27.8' LT 667.55 |
| U | 105+50.0 | 34.4' LT 667.41 |
| V | 105+00.0 | 31.4' LT 668.15 |
| W | 104+79.2 | 30.1' LT 668.36 |
| X | 104+79.2 | 29.1' LT 668.39 |
| Y | 104+66.2 | 29.2' LT 668.54 |
| Z | 104+70.7 | 29.1' LT 668.35 |
| AA | 107+41.7 | 46.1' LT 665.68 |
| AB | 107+42.8 | 29.2' LT 664.94 |
| AC | 107+42.8 | 28.2' LT 664.97 |
| AD | 107+62.6 | 35.4' LT 664.65 |
| AE | 107+62.6 | 27.6' LT 664.81 |
| AF | 107+62.6 | 0.0' RT 664.51 |
| AG | 107+62.6 | 27.7' RT 663.66 |
| AH | 107+62.6 | 36.0' RT 663.75 |
| AI | 107+57.8 | 36.3' RT 663.85 |
| AJ | 107+41.3 | 40.9' RT 664.27 |
| AK | 107+31.1 | 48.7' RT 664.58 |
| AL | 107+24.3 | 58.7' RT 664.54 |
| AM | 300+77.1 | 59.0' LT 664.47 |
| AN | 301+00.0 | 59.0' LT 664.39 |
| AO | 301+50.0 | 59.0' LT 664.24 |
| AP | 301+50.0 | 52.0' LT 664.52 |
| AQ | 301+00.0 | 51.9' LT 664.68 |
| AR | 300+77.1 | 51.9' LT 664.75 |
| AS | 301+50.0 | 30.0' LT 664.86 |
| AT | 301+00.0 | 30.0' LT 665.01 |
| AU | 300+77.1 | 27.8' LT 665.05 |
| AV | 301+50.0 | 19.0' LT 664.69 |
| AW | 301+00.0 | 19.0' LT 664.84 |
| AX | 300+65.0 | 19.0' LT 664.95 |
| AY | 300+60.5 | 19.0' LT 664.96 |
| AZ | 300+60.5 | 11.0' LT 664.98 |
| BA | 300+81.6 | 11.0' LT 664.90 |
| BB | 300+88.0 | 11.0' LT 664.87 |
| BC | 301+00.0 | 11.0' LT 664.58 |
| BD | 301+50.0 | 11.0' LT 664.36 |
| BE | 301+50.0 | 0.0' RT 664.20 |
| BF | 301+00.0 | 0.0' RT 664.42 |
| BG | 300+81.6 | 0.0' RT 664.73 |
| BH | 301+50.0 | 5.0' RT 664.00 |
| BI | 301+46.5 | 5.0' RT 664.01 |
| BJ | 301+00.0 | 9.6' RT 664.18 |
| BK | 300+81.6 | 14.1' RT 664.50 |
| BL | 300+65.1 | 22.0' RT 664.83 |
| BM | 300+53.5 | 33.1' RT 665.10 |
| BN | 105+88.8 | 41.3' RT 665.58 |
| BO | 106+00.0 | 27.2' RT 665.92 |
| BP | 105+55.2 | 38.1' RT 666.51 |
| BO | 105+53.4 | 38.1' RT 666.54 |
| BR | 105+53.4 | 27.0' RT 666.73 |
| BS | 105+00.0 | 37.9' RT 667.38 |
| BT | 105+00.0 | 26.5' RT 667.66 |
| BU | 104+66.2 | 37.7' RT 668.28 |
| BV | 104+70.7 | 26.4' RT 668.28 |
| BW | 104+66.2 | 0.0' RT 668.59 |
| BX | 105+00.0 | 0.0' RT 668.05 |
| BY | 105+53.4 | 0.0' RT 667.14 |
| BZ | 106+00.0 | 0.0' RT 666.43 |
| CA | 106+17.8 | 66.8' RT 665.17 |
| CB | 106+18.9 | 108.3' RT 664.24 |
| CC | 106+13.8 | 108.5' RT 664.55 |
| CD | 106+12.4 | 54.5' RT 665.33 |
| CE | 107+56.4 | 48.1' RT 664.36 |
| CF | 107+56.7 | 52.9' RT 664.83 |
| CG | 107+41.3 | 48.9' RT 663.99 |
| CH | 107+41.3 | 53.9' RT 664.09 |
| CI | 107+37.1 | 58.2' RT 664.49 |
| CJ | 106+50.0 | 0.0' RT 665.70 |
| CK | 106+59.0 | 0.0' RT 665.58 |
| CL | 106+86.6 | 0.0' RT 665.30 |
| CM | 107+00.0 | 0.0' RT 665.04 |
| CN | 107+50.0 | 0.0' RT 664.61 |

NOTES:

- PR PAVMT SECTION CONSISTS OF:
HMA SURF CSE, MIX "D" N50 - 1 1/2"
LEVELING BINDER (MACHINE METHOD) N50 - DEPTH VARIES
OR
HMA BINDER CSE, IL-19.0 N50 - DEPTH VARIES
FOR BINDER DEPTHS, SEE SHEET NO. 38
- A QUANTITY FOR CLASS D PATCHES, TYPE I, 8" AND CLASS D PATCHES, TYPE II, 8" IS INCLUDED IN THE PLANS. THE ENGINEER SHALL APPROVE THE LOCATIONS OF PAVEMENT PATCHING PRIOR TO PLACEMENT.