

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	09-00170-00-CH	DUPAGE	108	1

1-18-13 LETTING ITEM 130

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DIVISION OF HIGHWAYS

**PLANS FOR PROPOSED  
FEDERAL AID HIGHWAY**

F.A.U. ROUTE 3545 (ILLINOIS ROUTE 56) - (BUTTERFIELD ROAD)  
AT COMMONWEALTH LANE  
CHANNELIZATION / TRAFFIC SIGNALS

SECTION 09-00170-00-CH

PROJECT M-9003(266)

CITY OF ELMHURST

DuPAGE COUNTY

JOB NO. C-91-389-09

R11E 3rd PM



FOR INDEX OF SHEETS, SEE SHEET NO. 2

DESIGN DESIGNATION

BUTTERFIELD ROAD  
2735 (30) ARTERIAL 2.54 (COMP-20)

COMMONWEALTH LANE  
870 (30) COLLECTOR 1.06 (COMP-20)

TRAFFIC DATA

BUTTERFIELD ROAD  
DESIGN SPEED: 40 MPH  
POSTED SPEED: 35 MPH  
ADT: 16,100 (2008) 17,000 (2030)

COMMONWEALTH LANE  
DESIGN SPEED: 30 MPH  
POSTED SPEED: 25 MPH  
ADT: 6,650 (2008) 8,000 (2030)

PROJECT BEGINS  
COMMONWEALTH LANE  
STATION 200+61.54

PROJECT IS LOCATED IN  
THE CITY OF ELMHURST

PROJECT BEGINS  
BUTTERFIELD ROAD  
STATION 100+20.20

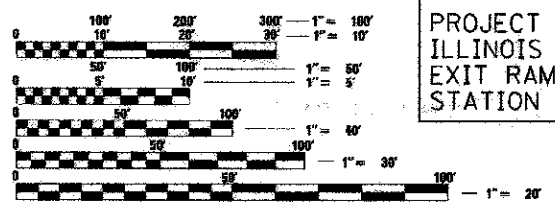
EX SN 022-0114

PROJECT ENDS  
BUTTERFIELD ROAD  
STATION 107+62.56

PROJECT ENDS  
COMMONWEALTH LANE  
STATION 309+67.95

DESCRIPTION OF IMPROVEMENT

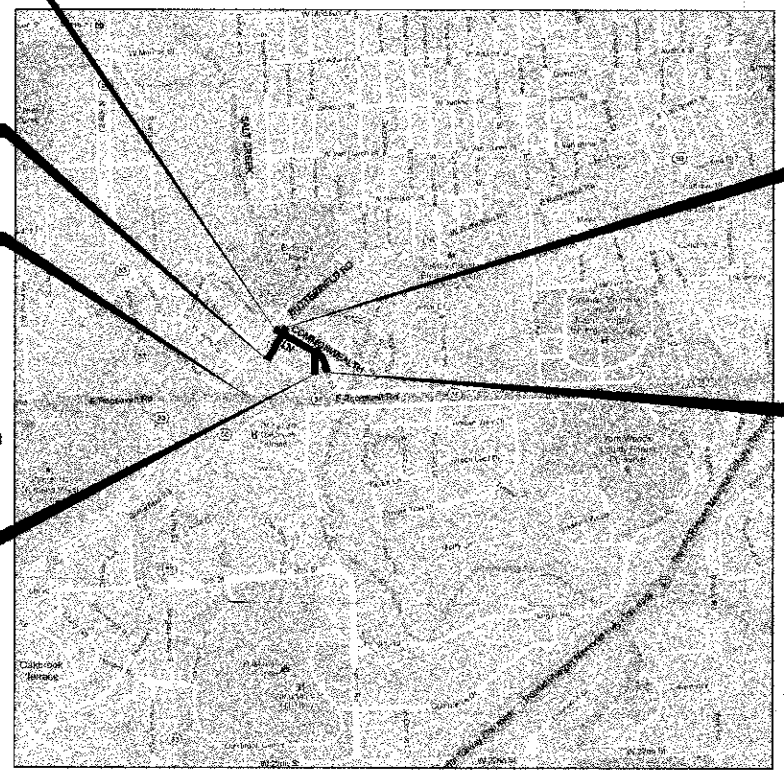
THIS IMPROVEMENT CONSISTS OF ROADWAY RESURFACING AND WIDENING, STORM SEWER AND DRAINAGE STRUCTURE ADJUSTMENTS AND INSTALLATION, ROADWAY LIGHTING, TRAFFIC SIGNAL INSTALLATION, LANDSCAPING, STRIPING, AND ALL INCIDENTAL AND COLLATERAL WORK AS NECESSARY TO COMPLETE THE IMPROVEMENT SHOWN HEREIN AND AS DESCRIBED IN THE SPECIFICATIONS.



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

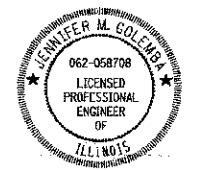
J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

CONTRACT NO. 63742

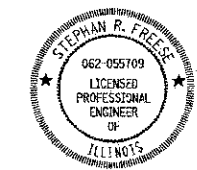


R11E  
YORK TOWNSHIP  
**LOCATION MAP**  
NOT TO SCALE

PROJECT LENGTH (GROSS / NET)  
BUTTERFIELD ROAD 742.36 FT (0.141 MILES)



JENNIFER M. GOLEMBA, P.E.  
NO. 062-058708  
EXP. DATE 11/30/13  
(ALL SHEETS EXCEPT NOS. 61 TO 68)



STEPHAN R. FREESE, P.E.  
NO. 062-055709  
EXP. DATE 11/30/13  
(SHEET NOS. 61 TO 68)

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

APPROVED ON: October 22, 2012  
*Carie Tiboni*  
CITY OF ELMHURST, CITY ENGINEER

PASSED November 9, 2012  
*John C. Holt*  
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID  
BASED ON LIMITED  
REVIEW November 8, 2012  
*John C. Holt*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

1475 EAST WOODFIELD ROAD, SUITE 800  
SCHAUMBURG, ILLINOIS 60173  
(847) 805-9800

PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. (847) 705-4406 SCHAMBERG, IL

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\* ROW NOT ACQUIRED, PLAT OF HIGHWAYS IS TO BE USED FOR EXISTING ROW INFORMATION ONLY

STATE STANDARDS

STD. NO.	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
353001-04	PCC BASE COURSE WITH HMA CONCRETE BINDER AND SURFACE COURSES
420111-03	PCC PAVEMENT ROUNDOUTS
424001-07	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424021-01	DEPRESSED CORNER FOR SIDEWALKS
442201-03	CLASS C & D PATCHES
482006-03	HMA SHOULDER ADJACENT TO RIGID PAVEMENT
542301-03	PRECAST REINFORCED CONCRETE FLARED END SECTION
602011-02	CATCH BASIN, TYPE C
602301-03	INLET, TYPE A
602401-03	MANHOLE, TYPE A
602406-05	MANHOLE, TYPE A, 1.8m (6') DIAMETER
602601-02	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
602701-02	MANHOLE STEPS
604001-03	FRAME AND LIDS, TYPE 1
604036-02	GRATE TYPE B
604086-02	FRAME AND GRATE, TYPE 23
606001-05	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
606301-04	PC CONCRETE ISLANDS AND MEDIANS
606306-03	CORRUGATED PC CONCRETE MEDIANS
630001-10	STEEL PLATE BEAM GUARDRAIL
630201-06	PCC / HMA STABILIZATION AT STEEL PLATE BEAM GUARDRAIL
630301-06	SHOULDER WIDENING FOR TYPE I (SPECIAL) GUARDRAIL TERMINALS
637001-05	CONCRETE BARRIER, DOUBLE FACE, 32 IN. (813mm) HEIGHT
643001	SAND MODULE IMPACT ATTENUATORS
664001-02	CHAIN LINK FENCE
701001-02	OFF-ROAD OPERATIONS, 2L, 2W, MORE THAN 15' (4.5m) AWAY
701006-04	OFF-ROAD OPERATIONS, 2L, 2W, 15' (4.5m) TO 24" (600mm) FROM PAVEMENT EDGE
701101-03	OFF-ROAD OPERATIONS, MULTILANE, 15' (4.5m) TO 24" (600mm) FROM PAVEMENT EDGE
701106-02	OFF-ROAD OPERATIONS, MULTILANE, MORE THAN 15' (4.5m) AWAY
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701502-05	URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
701601-08	URBAN LANE CLOSURE, MULTI-LANE, 1W OR 2W WITH NON TRAVERSABLE MEDIAN
701701-05	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-02	TRAFFIC CONTROL DEVICES
704001-07	TEMPORARY CONCRETE BARRIER
720001-01	SIGN PANEL MOUNTING DETAILS
720006-03	SIGN PANEL ERECTION DETAILS
720011-01	METAL POSTS FOR SIGNS, MARKERS, AND DELINEATORS
729001-01	APPLICATIONS OF TYPE A AND B METAL POSTS (FOR SIGNS AND MARKERS)
805001-01	ELECTRICAL SERVICE INSTALLATION DETAILS
814001-02	HANDHOLES
814006-02	DOUBLE HANDHOLES
825011-02	LIGHTING CONTROLLER, PEDESTAL MOUNTED, 240V
836001-02	LIGHT POLE FOUNDATION
857001-01	STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
862001-01	UNINTERRUPTIBLE POWER SUPPLY (UPS)
873001-02	TRAFFIC SIGNAL GROUNDING AND BONDING
877011-05	STEEL COMB. MAST ARM ASSEMBLY AND POLE 16' THROUGH 55'
877012-02	STEEL COMB. MAST ARM ASSEMBLY AND POLE 56' THROUGH 75'
878001-09	CONCRETE FOUNDATION DETAILS
880001-01	SPAN WIRE MOUNTED SIGNALS AND FLASHING BEACON INSTALLTION
880006-01	TRAFFIC SIGNAL MOUNTING DETAILS
886001-01	DETECTOR LOOP INSTALLATIONS

DISTRICT ONE STANDARD DETAILS

STD. NO.	DESCRIPTION
80-01	DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5m)
80-07	DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER
80-08	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
80-12	MANHOLE WITH RESTRICTOR PLATE
80-22	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
80-32	BUTT JOINT AND HMA TAPER DETAILS
80-34	DETAILS FOR DEPRESSED CURB & GUTTER AND SHOULDER TREATMENT AT TBT TY 1 SPL
80-48	PCC PAVEMENT ROUNDOUTS AT CURB AND GUTTER
BE-220	ELECTRIC SERVICE INSTALLATION AERIAL, REMOTE DISCONNECT
BE-301	LIGHT POLE FOUNDATION 40' (12.192m) TO 47 1/2' (14.478m) M.H. 15" (381mm) BOLT CIRCLE
BE-401	LIGHT POLE, ALUMINUM, TRUSS TYPE, 40 FT. M.H.
BE-702	MISC. ELECTRICAL DETAILS SHEET A
TC-10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
TC-11	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
TC-13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
TC-14	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
TC-16	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
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TC-26	DRIVEWAY ENTRANCE SIGNING
TS-02	DISTRICT ONE MAST ARM MOUNTED STREET NAME SIGNS
TS-05	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS

GENERAL NOTES

- ALL REFERENCES TO "STANDARD SPECIFICATIONS" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, JANUARY 1, 2012.
- ALL ELEVATIONS SHOWN ON THE PLANS ARE IN NGVD29 DATUM UNLESS OTHERWISE NOTED.
- ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED TO MEAN THE RESIDENT ENGINEER.
- PRIOR TO COMMENCEMENT OF CONSTRUCTION, THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS AFFECTING THE WORK WITH THE ACTUAL CONDITIONS AT THE JOB SITE. IN ADDITION, THE CONTRACTOR MUST VERIFY THE ENGINEER'S LINE AND GRADE STAKES. IF THERE ARE DISCREPANCIES FROM WHAT IS SHOWN ON THE CONSTRUCTION PLANS, HE MUST IMMEDIATELY REPORT THEM TO THE ENGINEER BEFORE DOING ANY WORK. OTHERWISE THE CONTRACTOR ASSUMES FULL RESPONSIBILITY. IN THE EVENT OF DISAGREEMENT BETWEEN THE CONSTRUCTION PLANS, STANDARD SPECIFICATIONS AND/OR SPECIAL DETAILS, THE CONTRACTOR SHALL SECURE WRITTEN INSTRUCTIONS FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF THE WORK AFFECTED BY OMISSIONS OR DISCREPANCIES. FAILING TO SECURE SUCH INSTRUCTIONS, THE CONTRACTOR WILL BE CONSIDERED TO HAVE PROCEEDED AT HIS OWN RISK AND EXPENSE IN THE EVENT OF ANY DOUBT OR QUESTION ARISING WITH RESPECT TO THE TRUE MEANING OF THE CONSTRUCTION PLANS OR SPECIFICATIONS. THE DECISION OF THE ENGINEER SHALL BE FINAL AND CONCLUSIVE.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS PRIOR TO BIDDING ON THE PROJECT.
- THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST 3 DOCUMENTED LEAKING UNDERGROUND STORAGE TANKS (LUST) CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION. SEE SPECIAL PROVISIONS AND SUPPLEMENTAL SPECIFICATIONS FOR DETAILS.
- SAW CUTTING OF PAVEMENTS, SIDEWALK, ETC. SHALL BE TO FULL DEPTH. SHALL BE COMPLETED WITH A WETSAW AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEM REMOVED.
- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION) AT 8-1-1 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION IS REQUIRED).
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE OR CITY PROPERTY OR ROW WITHOUT WRITTEN PERMISSION FROM THE ENGINEER.
- THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARKERS UNTIL THE OWNER, HIS AGENT, OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
- WHEREVER CONCRETE MASONRY WALLS, HEADWALLS OR OTHER OBSTRUCTIONS ARE ENCOUNTERED, THEY SHALL BE REMOVED TO A DEPTH OF 1 FOOT BELOW THE EXISTING GRADE OR SUBGRADE AS SHOWN ON THE PLANS. SUCH REMOVAL SHALL BE CONSIDERED INCLUDED IN THE COST OF EARTH EXCAVATION.
- HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- WHEN REMOVING PAVEMENT, CURB AND GUTTER, SHOULDER AND/OR ANY OTHER STRUCTURES, THE USE OF ANY TYPE OF CONCRETE BREAKERS WHICH MIGHT DAMAGE UNDERGROUND PUBLIC OR PRIVATE UTILITIES AND BUILDING FOUNDATIONS WILL NOT BE PERMITTED. UNDER NO CIRCUMSTANCES WILL THE USE OF A FROST BALL BE PERMITTED.
- MATERIALS RESULTING FROM THE REMOVAL OF CONCRETE SURFACES, UTILITY STRUCTURE ADJUSTMENT, RESTORATION WORK, ETC. SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IF THE CONTRACTOR DOES NOT REMOVE THESE MATERIALS AT THE REQUEST OF THE ENGINEER, THE CITY OF ELMHURST WILL HIRE A CONTRACTOR TO HAVE THE MATERIAL REMOVED AND THE CONTRACTOR WILL BE ASSESSED THE TRAFFIC CONTROL DEFICIENCY DEDUCTION FOR NON-COMPLIANCE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR SWEEPING AND CLEANING STREETS OF ANY DEBRIS AND MATERIAL THAT HAS ACCUMULATED AS A RESULT OF THE CONSTRUCTION ACTIVITY. A MECHANICAL SWEEPER, MECHANICALLY DRIVEN AIR AND HANDWORK WITH SHOVEL AND BROOM SHALL BE UTILIZED TO PROVIDE A CLEAN STREET FOR THE MOTORING PUBLIC. WITHIN 24 HOURS OF PLACING PRIME COAT AND THE LAYERING OF HMA, THE CONTRACTOR SHALL SWEEP THE PAVEMENT AND REMOVE STANDING WATER, EARTH, WEEDS, LEAVES, DIRT, CONSTRUCTION DEBRIS AND ALL LOOSE MATERIAL. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.
- THE BITUMINOUS MATERIAL PRIME COAT QUANTITIES HAVE BEEN DETERMINED USING A RATE OF 0.10 GAL/50 YD (0.5 L/50 M).
- THE AGGREGATE PRIME COAT QUANTITIES HAVE BEEN DETERMINED USING A RATE OF 0.002 TON/50 YD.
- ALL PROPOSED CONCRETE MEDIANS SIX FEET WIDE OR LESS SHALL BE POURED MONOLITHICALLY AND THE OPTIONAL CONSTRUCTION JOINT SHOWN ON STANDARD 606301 WILL NOT BE ALLOWED.
- THE CONTRACTOR'S ATTENTION IS CALLED TO THE FACT THAT THE PRESERVATION OF EXISTING TREES IS OF UTMOST IMPORTANCE TO THE CITY OF ELMHURST. ALL TREE PROTECTION, TREE REMOVAL, PRUNING AND ROOT PRUNING SHALL BE COMPLETED BEFORE CONSTRUCTION OPERATIONS COMMENCE IN ANY AREA. AT NO TIME SHALL THE CONTRACTOR PRUNE OR REMOVE ANY TREES UNLESS SPECIFICALLY DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY AND ALL EXISTING ITEMS WHICH WILL NOT BE REMOVED. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE.
- IN ANY AREA WHERE THERE IS NO PROPOSED GRADING, THE EXISTING GROUND COVER SHALL REMAIN.

GENERAL NOTES (CONTINUED)

- TEN FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED ITEMS OF WORK TO EXISTING ITEMS IN THE FIELD. UNLESS OTHERWISE SHOWN, THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEM OF WORK SPECIFIED.
- A QUANTITY OF HIGH-EARLY STRENGTH PORTLAND CEMENT CONCRETE PAVEMENT IS INCLUDED IN THE PLANS. THE ENGINEER SHALL APPROVE THE USE OF THIS MATERIAL PRIOR TO PLACEMENT.
- A QUANTITY FOR CLASS D PATCHES, TYPE I, 8" IS INCLUDED IN THE PLANS. THE ENGINEER SHALL APPROVE THE LOCATIONS OF PAVEMENT PATCHING PRIOR TO PLACEMENT.
- THE CONTRACTOR SHALL NOT STOCKPILE MATERIALS ON THE CONSTRUCTION JOB SITE.
- THE INDISCRIMINATE USE OF FIRE HYDRANTS, EXISTING STREAMS, CREEKS, WETLANDS OR PONDS AS A WATER SOURCE IS STRICTLY PROHIBITED. THE CONTRACTOR SHALL PROVIDE A WATER TRUCK AND DRIVER AS REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING WATER FROM AN APPROVED SOURCE. IF THIS WATER IS FROM A SOURCE OTHER THAN FROM THE CONTRACTOR'S YARD, WRITTEN APPROVAL FROM THE AGENCY HAVING JURISDICTION FOR THE SOURCE OF THE WATER MUST BE RECEIVED BY THE CONTRACTOR PRIOR TO THE USE OF THE WATER. THE CONTRACTOR SHALL SECURE A WATER METER FROM THE CITY OF ELMHURST (630-530-3020) WITH APPROPRIATE DEPOSIT IF THE CONTRACTOR USES A CITY OF ELMHURST FIRE HYDRANT AS A WATER SOURCE.
- UNLESS OTHERWISE NOTED IN THE PLANS, ALL DRIVEWAYS SHALL REMAIN ACCESSIBLE AT ALL TIMES. OTHER DRIVEWAY CONSTRUCTION WORK SHALL BE STAGED.
- THE STORMWATER MANAGEMENT PERMIT AS ISSUED BY THE CITY OF ELMHURST AND DUPAGE COUNTY DIVISION OF ENVIRONMENTAL CONCERNS IS ON FILE WITH THE CITY OF ELMHURST AND DUPAGE COUNTY (PERMIT NO. 12-16-0007, DEC TRACKING NUMBER 137327). ALL WORK DONE WITHIN THE PROJECT LIMITS MUST COMPLY WITH THE PERMIT AND WITH THE DUPAGE COUNTY CITYWIDE STORMWATER AND FLOOD PLAIN ORDINANCE.
  - ALL COMPENSATORY STORAGE SHALL BE OPERATIONAL PRIOR TO ANY PLACEMENT OF FILL, STRUCTURES OR OTHER MATERIALS IN THE REGULATORY FLOOD PLAIN. GRADING IN WETLANDS OR BUFFER AREAS SHALL BE COMPLETED SUCH THAT THE EXISTING FLOOD PLAIN STORAGE IS MAINTAINED AT ALL TIMES.
  - ALL EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO THE BEGINNING OF WORK.
- DELETE THE SECOND AND THIRD SENTENCE OF THE SECOND PARAGRAPH OF ARTICLE 1020.14(c) OF THE STANDARD SPECIFICATIONS.
- FOR WORK OUTSIDE THE LIMITS OF BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB AND GUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT, SHALL BE EPOXY COATED UNLESS NOTED ON THE PLAN.
- EXISTING TRAFFIC SIGNAGE AND ROADWAY NAME SIGNAGE NOT BEING REUSED SHALL REMAIN THE PROPERTY OF THE CITY OF ELMHURST AND BE DELIVERED TO THE CITY OF ELMHURST PUBLIC WORKS FACILITY AT 985 SOUTH RIVERSIDE DRIVE.

UTILITY NOTES

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN IF NOT SHOWN ON THE PLANS. UTILITY PROPERTY DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER IN ACCORDANCE WITH SPECIAL PROVISION LR 105.
- ALL UTILITY OWNERS SHALL BE NOTIFIED AT LEAST 3 DAYS PRIOR TO THE START OF CONSTRUCTION.
- THE CONTRACTOR SHALL USE ALL NECESSARY PRECAUTIONS AND PROTECTION MEASURES REQUIRED TO MAINTAIN EXISTING UTILITIES, SEWERS, AND APPURTENANCES THAT MUST BE KEPT IN OPERATION.

DRAINAGE NOTES

- THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR. THIS WORK SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- THE CONTRACTOR SHALL PROVIDE ACCESS TO ALL WATER SYSTEM VALVES AND VALVE VAULTS, SANITARY SEWER MANHOLES, AND STORM SEWER MANHOLES, CATCH BASINS AND STORM INLETS FOR EMERGENCY OPERATIONS. THE LOCATIONS OF ALL WATER AND SANITARY FACILITIES SHALL BE MARKED AND READILY VISIBLE AT ALL TIMES.
- ALL LOOSE MATERIAL DEPOSITED IN THE FLOWLINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.
- THE EXISTING FRAMES AND LIDS, INCLUDING TRAFFIC SIGNAL FRAMES AND LIDS, SHALL REMAIN AS PROPERTY OF THE CITY OF ELMHURST. ALL OLD FRAMES AND LIDS NOT BEING REUSED SHALL BE REMOVED FROM PARKWAYS BY THE CONTRACTOR, DELIVERED TO AND STOCKPILED AT THE CITY MUNICIPAL SERVICE FACILITY WITHIN SEVEN (7) DAYS OF THEIR REMOVAL. THE UTILITY DEPARTMENT YARD IS LOCATED AT THE NORTH END OF THE WASTE WATER TREATMENT PLANT FACILITY, 625 S. ROUTE 83.
- THE CONNECTION OF EXISTING DRAIN TILES, PIPE CULVERTS, OR STORM SEWERS TO THE PROPOSED DRAINAGE SYSTEM SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PROPOSED DRAINAGE ITEMS.
- STORM SEWER (WATER MAIN REQUIREMENTS) IS TO BE USED AT LOCATIONS WHERE LATERAL SEPARATION BETWEEN THE SEWER AND WATER MAIN IS LESS THAN 10 FT (3.0 M) AND THE WATER MAIN BOTTOM OF PIPE IS LESS THAN 1.5 FT (0.45 M) ABOVE THE STORM SEWER CROWN.
- BEFORE ORDERING STORM SEWERS, CATCH BASIN, PIPE CULVERTS, PIPE DRAINS, AND MANHOLES THE CONTRACTOR SHALL VERIFY THE EXACT LENGTH AND QUANTITY REQUIRED.
- ONLY PRECAST CONCRETE ADJUSTMENT RINGS OR BEVELED RINGS WILL BE ALLOWED IN THE ADJUSTMENT OR RECONSTRUCTION OF CATCH BASIN, MANHOLE, INLET AND VALVE VAULT STRUCTURES. COMMON BRICK, STEEL SHIMS AND ROCKS WILL NOT BE ALLOWED.

NOTE: BOXED ITEMS ARE INCLUDED IN THE COST AS DESIGNATED



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PL07 SCALE = 5/8"=1'-0"	CHECKED - JMG	REVISED -
PL07 DATE = 12/4/2012	DATE - 10/22/2012	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BUTTERFIELD ROAD AT COMMONWEALTH LANE  
INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES

SCALE: NOT TO SCALE SHEET NO. 2 OF 108 SHEETS

F.A.I.I. RFE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	09-00170-00-CH	DUPAGE	108	2
			CONTRACT NO. 63742	
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

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PLAN  
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 BY  
 NO. OF M.A. CHECKED  
 M.A. FILE NAME

SUMMARY OF QUANTITIES				ROADWAY#	LIGHTING#	TRAFFIC SIGNALS		EMERGENCY VEHICLE PREEMPTION	NON-PARTICIPATING
CODE NO.	PAY ITEM	UNIT	QUANTITY	70% FED 30% LA	70% FED 30% LA	BUTTERFIELD RD &# COMMONWEALTH LN 70% FED 30% LA	INTERCONNECT# 70% FED 30% LA	100% ELMHURST	100% ELMHURST
				0004	0021		0021	0021	0043
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	40	40					
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	20	20					
20101000	TEMPORARY FENCE	FOOT	1,693	1,693					
20101100	TREE TRUNK PROTECTION	EACH	22	22					
Δ 20101200	TREE ROOT PRUNING	EACH	11	11					
Δ 20101300	TREE PRUNING (1 TO 10 INCH DIAMETER)	EACH	6	6					
Δ 20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	5	5					
20200100	EARTH EXCAVATION	CU YD	1,914	1,914					
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	1,030	1,030					
20400800	FURNISHED EXCAVATION	CU YD	209	209					
20800150	TRENCH BACKFILL	CU YD	119	119					
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	250	250					
Δ 21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	5,276	5,276					
Δ 25000210	SEEDING, CLASS 2A	ACRE	1.25	1.25					
Δ 25000312	SEEDING, CLASS 4A	ACRE	0.25	0.25					
Δ 25000400	NITROGEN FERTILIZER NUTRIENT	POUND	135	135					
Δ 25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	135	135					
Δ 25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	135	135					
Δ 25100630	EROSION CONTROL BLANKET	SQ YD	5,932	5,932					
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	150	150					
• 28000305	TEMPORARY DITCH CHECKS	FOOT	65	65					
28000510	INLET FILTERS	EACH	22	22					
28100105	STONE RIPRAP, CLASS A3	SQ YD	393	393					
28200200	FILTER FABRIC	SQ YD	602	602					
• 30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	83	83					
• 30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	2,889	2,889					
35300200	PORTLAND CEMENT CONCRETE BASE COURSE 7"	SQ YD	1,317	1,317					
35300500	PORTLAND CEMENT CONCRETE BASE COURSE 10"	SQ YD	936	936					
35301000	HIGH-EARLY-STRENGTH PORTLAND CEMENT CONCRETE BASE COURSE 7"	SQ YD	132	132					
35301300	HIGH-EARLY-STRENGTH PORTLAND CEMENT CONCRETE BASE COURSE 10"	SQ YD	94	94					
35800100	PREPARATION OF BASE	SQ YD	7,043	7,043					
40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	116	116					
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	470	470					
40600300	AGGREGATE (PRIME COAT)	TON	1	1					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	30	30					
40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	176	176					

\* STP (70% FED, 30% LA)  
 CMAQ (80% FED, 20% LA)  
 PER LOCAL AGENCY  
 AGREEMENT

• SPECIAL PROVISION

† CONSTRUCTION TYPE CODE = 0042 Δ SPECIALTY ITEMS



USER NAME = kollerenz	DESIGNED TKS	REVISED
FILE NAME = D:\X2008-SHT-SumQty.dgn	DRAWN - TKS	REVISED -
PLOT SCALE = 5/8" = 1' in.	CHECKED - JMG	REVISED -
PLOT DATE = 12/4/2012	DATE - 10/22/2012	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

BUTTERFIELD ROAD AT COMMONWEALTH LANE  
 SUMMARY OF QUANTITIES

SCALE: NOT TO SCALE SHEET NO. 3 OF 108 SHEETS

F.A.U. RTE. 3545	SECTION 09-00170-00-CH	COUNTY DUPAGE	TOTAL SHEETS 108	SHEET NO. 3
CONTRACT NO. 63742			FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT	

FILE PATH = G:\CH2M\BENTON\PHASE 2\Sheets\01\X2008-SHT-SumQty.dgn

PLAN  
 DATE  
 BY  
 CHECKED  
 DATE  
 NO.

CODE NO.	PAY ITEM	UNIT	QUANTITY	SUMMARY OF QUANTITIES						
				ROADWAY#		LIGHTING#		TRAFFIC SIGNALS		NON-PARTICIPATING
				70% FED 30% LA	0004	70% FED 30% LA	0021	BUTTERFIELD RD &# COMMONWEALTH LN	INTERCONNECT#	EMERGENCY VEHICLE PREEMPTION
								0021	0021	0043
40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	235	235						
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	84	84						
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SO YD	38	38						
40600990	TEMPORARY RAMP	SO YD	142	142						
40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	1,068	1,068						
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	566	566						
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	355	355						
42001300	PROTECTIVE COAT	SO YD	2,986	2,986						
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	710	710						
42400800	DETECTABLE WARNINGS	SO FT	96	96						
44000100	PAVEMENT REMOVAL	SO YD	331	331						
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SO YD	4,275	4,275						
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	244	244						
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	2,060	2,060						
44000600	SIDEWALK REMOVAL	SO FT	691	691						
44003100	MEDIAN REMOVAL	SO FT	332	332						
44004250	PAVED SHOULDER REMOVAL	SO YD	1,902	1,902						
44201737	CLASS D PATCHES, TYPE I, 8 INCH	SO YD	200	200						
44201741	CLASS D PATCHES, TYPE II, 8 INCH	SO YD	500	500						
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	12,020	12,020						
48101600	AGGREGATE SHOULDERS, TYPE B 8"	SO YD	23	23						
48203015	HOT-MIX ASPHALT SHOULDERS, 4 1/2"	SO YD	119	119						
48203023	HOT-MIX ASPHALT SHOULDERS, 6 1/2"	SO YD	1,221	1,221						
50105220	PIPE CULVERT REMOVAL	FOOT	21	21						
54213657	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12"	EACH	1	1						
54213660	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 15"	EACH	1	1						
54213666	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 21"	EACH	1	1						
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	612	612						
550A0070	STORM SEWERS, CLASS A, TYPE 1 15"	FOOT	10	10						
550A0110	STORM SEWERS, CLASS A, TYPE 1 21"	FOOT	52	52						
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	76	76						
550A0360	STORM SEWERS, CLASS A, TYPE 2 15"	FOOT	199	199						
55100500	STORM SEWER REMOVAL 12"	FOOT	13	13						
56106400	ADJUSTING WATER MAIN 8"	FOOT	20	20						
56106600	ADJUSTING WATER MAIN 12"	FOOT	20	20						
56400500	FIRE HYDRANTS TO BE REMOVED	EACH	2							2

\* STP (70% FED, 30% LA)  
 CMAQ (80% FED, 20% LA)  
 PER LOCAL AGENCY  
 AGREEMENT

\* SPECIAL PROVISION

+ CONSTRUCTION TYPE CODE = 0042    Δ SPECIALTY ITEMS



USER NAME = kaorenz	DESIGNED - TKS	REVISED -
FILE NAME = 01X0006-SHT-SumQty.dgn	DRAWN - TKS	REVISED -
PLOT SCALE = 98.000 / 1"	CHECKED - JMG	REVISED -
PLOT DATE = 12/4/2012	DATE - 10/22/2012	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

BUTTERFIELD ROAD AT COMMONWEALTH LANE  
 SUMMARY OF QUANTITIES

SCALE: NOT TO SCALE    SHEET NO. 4 OF 108 SHEETS

F.A.I. RTE. 3545	SECTION 09-00170-00-CH	COUNTY DUPAGE	TOTAL SHEETS 108	SHEET NO. 4
FED. ROAD DIST. NO. 7 (ILLINOIS) FED. AID PROJECT			CONTRACT NO. 63742	

FILE PATH = G:\CH08\0808\Road\Phase 2\Sheet\01X0006-SHT-SumQty.dgn



DATE: \_\_\_\_\_ BY: \_\_\_\_\_  
 PLAN: \_\_\_\_\_  
 SURVEYED: \_\_\_\_\_  
 CHECKED: \_\_\_\_\_  
 PLAN FILE NAME: \_\_\_\_\_

SUMMARY OF QUANTITIES				ROADWAY#	LIGHTING#	TRAFFIC SIGNALS			NON-PARTICIPATING
CODE NO.	PAY ITEM	UNIT	QUANTITY			BUTTERFIELD RD & COMMONWEALTH LN	INTERCONNECT#	EMERGENCY VEHICLE PREEMPTION	
				70% FED 30% LA	70% FED 30% LA	70% FED 30% LA	70% FED 30% LA	100% ELMHURST	100% ELMHURST
				0004	0021		0021	0021	0043
Δ 56400825	FIRE HYDRANT WITH AUXILIARY VALVE, VALVE BOX AND TEE	EACH	2						2
60107600	PIPE UNDERDRAINS 4"	FOOT	770	770					
60200105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	1	1					
60201330	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 23 FRAME AND GRATE	EACH	2	2					
60208230	CATCH BASINS, TYPE C, TYPE 23 FRAME AND GRATE	EACH	9	9					
60218400	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	5	5					
60219000	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 8 GRATE	EACH	1	1					
60219530	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 23 FRAME AND GRATE	EACH	2	2					
60234200	INLETS, TYPE A, TYPE 1 FRAME, OPEN LID	EACH	1	1					
60236200	INLETS, TYPE A, TYPE 8 GRATE	EACH	3	3					
60261530	INLETS TO BE ADJUSTED WITH NEW TYPE 23 FRAME AND GRATE	EACH	2	2					
60265700	VALVE VAULTS TO BE ADJUSTED	EACH	3	3					
60500205	FILLING CATCH BASINS	EACH	2	2					
60600605	CONCRETE CURB, TYPE B	FOOT	27	27					
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	1,628	1,628					
60608600	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06	FOOT	83	83					
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SQ FT	652	652					
60619600	CONCRETE MEDIAN, TYPE SB-6.12	SQ FT	2,995	2,995					
60624600	CORRUGATED MEDIAN	SQ FT	144	144					
Δ 63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	188	188					
Δ 63000009	STEEL PLATE BEAM GUARDRAIL, TYPE B, 9 FOOT POSTS	FOOT	50	50					
Δ 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	1	1					
63200310	GUARDRAIL REMOVAL	FOOT	400	400					
Δ 66400105	CHAIN LINK FENCE, 4'	FOOT	304	304					
Δ 66400305	CHAIN LINK FENCE, 6'	FOOT	330	330					
Δ 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	860	860					
Δ 66900450	SPECIAL WASTE PLANS AND REPORTS	L SUM	1	1					
Δ 66900530	SOIL DISPOSAL ANALYSIS	EACH	3	3					
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	9	9					
67100100	MOBILIZATION	L SUM	1	1					
70300100	SHORT TERM PAVEMENT MARKING	FOOT	2,552	2,552					
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	218	218					
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	29,727	29,727					
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	1,089	1,089					
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	3,336	3,336					
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	450	450					

# STP (70% FED, 30% LA)  
 CMAQ (80% FED, 20% LA)  
 PER LOCAL AGENCY  
 AGREEMENT

\* SPECIAL PROVISION

+ CONSTRUCTION TYPE CODE = 0042 Δ SPECIALTY ITEMS



USER NAME = kalorenz	DESIGNED - TKS	REVISED -
FILE NAME = D:\X0008-SHT-SumQty.dgn	DRAWN - TKS	REVISED -
PLOT SCALE = 50.000' / in.	CHECKED - JMG	REVISED -
PLOT DATE = 12/4/2012	DATE - 10/22/2012	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

BUTTERFIELD ROAD AT COMMONWEALTH LANE  
 SUMMARY OF QUANTITIES

SCALE: NOT TO SCALE SHEET NO. 5 OF 108 SHEETS

F.A.A.I. RTE. 3545	SECTION 09-00170-00-CH	COUNTY DUPAGE	TOTAL SHEETS 108	SHEET NO. 5
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63742	

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 SUBMITTED BY DATE  
 PLOTTED BY DATE  
 NOTE BOOK NO. OF MAY CREATED  
 CARD FILE NAME

SUMMARY OF QUANTITIES				ROADWAY#	LIGHTING#	TRAFFIC SIGNALS			NON-PARTICIPATING
CODE NO.	PAY ITEM	UNIT	QUANTITY	70% FED 30% LA	70% FED 30% LA	BUTTERFIELD RD &# COMMONWEALTH LN	INTERCONNECT#	EMERGENCY VEHICLE PREEMPTION	NON-PARTICIPATING
				0004	0021	70% FED 30% LA	70% FED 30% LA	100% ELMHURST	100% ELMHURST
							0021	0021	0043
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	530	530					
70400100	TEMPORARY CONCRETE BARRIER	FOOT	550	550					
72000100	SIGN PANEL - TYPE 1	SQ FT	104	74		30			
72000200	SIGN PANEL - TYPE 2	SQ FT	55			55			
72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	2	2					
72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	17	17					
72400600	RELOCATE SIGN PANEL ASSEMBLY - TYPE B	EACH	1	1					
72900100	METAL POST - TYPE A	FOOT	153	153					
72900200	METAL POST - TYPE B	FOOT	184	184					
Δ 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	146	146					
Δ 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	2,075	2,075					
Δ 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	909	909					
Δ 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	101	101					
Δ 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	530	530					
Δ 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	88	88					
Δ 78005100	EPOXY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	307	307					
Δ 78005110	EPOXY PAVEMENT MARKING - LINE 4"	FOOT	4,428	4,428					
Δ 78005130	EPOXY PAVEMENT MARKING - LINE 6"	FOOT	956	956					
Δ 78005150	EPOXY PAVEMENT MARKING - LINE 12"	FOOT	325	325					
Δ 78005180	EPOXY PAVEMENT MARKING - LINE 24"	FOOT	55	55					
Δ 78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	59	59					
Δ 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	66	66					
Δ 78100105	RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE)	EACH	2	2					
Δ 78200410	GUARDRAIL MARKERS, TYPE A	EACH	21	21					
78300100	PAVEMENT MARKING REMOVAL	SQ FT	742	742					
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	68	68					
Δ 80400100	ELECTRIC SERVICE INSTALLATION	EACH	1		1				
Δ 80400200	ELECTRIC UTILITY SERVICE CONNECTION	L SUM	1		1				
Δ 80500020	SERVICE INSTALLATION - POLE MOUNTED	EACH	1			1			
Δ 81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	1,105			705	400		
Δ 81028210	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	84		10	74			
Δ 81028220	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	717		600	117			
Δ 81028240	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	395			395			
Δ 81400100	HANDHOLE	EACH	4			4			
Δ 81400200	HEAVY-DUTY HANDHOLE	EACH	5			5			
Δ 81400300	DOUBLE HANDHOLE	EACH	2			2			

\* STP (70% FED, 30% LA)  
 CMAQ (80% FED, 20% LA)  
 PER LOCAL AGENCY  
 AGREEMENT

\* SPECIAL PROVISION

+ CONSTRUCTION TYPE CODE = 0042 Δ SPECIALTY ITEMS



USER NAME = ksloranz	DESIGNED - TKS	REVISED -
FILE NAME = D:\X008-SHT-SumQty.dgn	DRAWN - TKS	REVISED -
PLOT SCALE = 1/8" = 1'	CHECKED - JMG	REVISED -
PLOT DATE = 12/4/2012	DATE - 10/22/2012	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

BUTTERFIELD ROAD AT COMMONWEALTH LANE  
 SUMMARY OF QUANTITIES

SCALE: NOT TO SCALE SHEET NO. 6 OF 108 SHEETS

F.A.U. RTE. 3545	SECTION 09-00170-00-CH	COUNTY DUPAGE	TOTAL SHEETS 108	SHEET NO. 6
CONTRACT NO. 63742				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

FILE PATH = E:\X008-SHT-SumQty.dgn

PLAN SURVEILED BY DATE  
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 NOTE BOOK NO. DATE  
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SUMMARY OF QUANTITIES					ROADWAY#	LIGHTING#	TRAFFIC SIGNALS			NON-PARTICIPATING
CODE NO.	PAY ITEM	UNIT	QUANTITY	70% FED 30% LA 0004			70% FED 30% LA 0021	BUTTERFIELD RD &# COMMONWEALTH LN 70% FED 30% LA 0021	INTERCONNECT# 70% FED 30% LA	
Δ	81702400	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 3-1/2 NO. 2	FOOT	60		60				
Δ	81800240	AERIAL CABLE, 2-1/2 NO. 8 WITH MESSENGER WIRE	FOOT	375		375				
Δ	82102250	LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, 250 WATT	EACH	16		16				
Δ	82500350	LIGHTING CONTROLLER, BASE MOUNTED, 240VOLT, 100AMP	EACH	1		1				
Δ	83008500	LIGHT POLE, ALUMINUM, 40 FT. M.H., 12 FT. MAST ARM	EACH	12		12				
Δ	83600200	LIGHT POLE FOUNDATION, 24" DIAMETER	FOOT	120		120				
Δ	83800205	BREAKAWAY DEVICE, TRANSFORMER BASE, 15 INCH BOLT CIRCLE	EACH	12		12				
Δ	84200500	REMOVAL OF LIGHTING UNIT, SALVAGE	EACH	7		7				
Δ	84400105	RELOCATE EXISTING LIGHTING UNIT	EACH	1		1				
Δ	84500110	REMOVAL OF LIGHTING CONTROLLER	EACH	1		1				
Δ	84500130	REMOVAL OF LIGHTING CONTROLLER FOUNDATION	EACH	1		1				
Δ*	85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1				1		
Δ	86200300	UNINTERRUPTABLE POWER SUPPLY, EXTENDED	EACH	1			1			
Δ	86400100	TRANSCEIVER - FIBER OPTIC	EACH	1				1		
Δ	87300925	ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	1,281				1,281		
Δ	87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	532			532			
Δ	87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	2,173			1,366		807	
Δ	87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	2,816			2,816			
Δ	87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	853			853			
Δ	87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	2,383			2,383			
Δ	87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	558			558			
Δ	87501000	TRAFFIC SIGNAL POST, 14 FT.	EACH	1			1			
Δ	87501200	TRAFFIC SIGNAL POST, 16 FT.	EACH	2			2			
Δ	87702850	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 24 FT.	EACH	1			1			
Δ	87702960	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 46 FT.	EACH	1			1			
Δ	87702980	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 50 FT.	EACH	1			1			
Δ	87703020	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 58 FT.	EACH	1			1			
Δ	87800100	CONCRETE FOUNDATION, TYPE A	FOOT	12			12			
Δ	87800150	CONCRETE FOUNDATION, TYPE C	FOOT	4			4			
Δ	87800400	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	10			10			
Δ*	87800415	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	28			28			
Δ*	87800420	CONCRETE FOUNDATION, TYPE E 42-INCH DIAMETER	FOOT	21			21			
Δ	88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	8			8			
Δ	88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	2			2			
Δ	88030210	SIGNAL HEAD, LED, 2-FACE, 3-SECTION, BRACKET MOUNTED	EACH	2			2			
Δ	88030240	SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED	EACH	2			2			

\* STP (70% FED, 30% LA)  
 CMAQ (80% FED, 20% LA)  
 PER LOCAL AGENCY  
 AGREEMENT

\* SPECIAL PROVISION

+ CONSTRUCTION TYPE CODE = 0042 Δ SPECIALTY ITEMS

FILE PATH = G:\CH08\0808\RoadPhase 2\Sheet\080808-5-11-SumQty.dwg



USER NAME - kaizenz	DESIGNED - TKS	REVISED -
FILE NAME - D:\0808-5-11-SumQty.dwg	DRAWN - TKS	REVISED -
PLOT SCALE = 5/8"=1'-0"	CHECKED - JMC	REVISED -
PLOT DATE = 12/4/2012	DATE - 10/22/2012	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

BUTTERFIELD ROAD AT COMMONWEALTH LANE  
 SUMMARY OF QUANTITIES

SCALE: NOT TO SCALE SHEET NO. 7 OF 108 SHEETS

F.A.U. RTE. 3545	SECTION 09-00170-00-CH	COUNTY DU PAGE	TOTAL SHEETS 108	SHEET NO. 7
FED. ROAD DIST. NO. 7 (ILLINOIS) FED. AID PROJECT				

CONTRACT NO. 63742

DATE: \_\_\_\_\_  
 PLAN: \_\_\_\_\_  
 CHECKED BY: \_\_\_\_\_  
 DATE: \_\_\_\_\_  
 DRAWN BY: \_\_\_\_\_  
 DATE: \_\_\_\_\_  
 FILE NAME: \_\_\_\_\_

SUMMARY OF QUANTITIES					ROADWAY#	LIGHTING#	TRAFFIC SIGNALS			NON-PARTICIPATING
CODE NO.	PAY ITEM	UNIT	QUANTITY	BUTTERFIELD RD & COMMONWEALTH LN			INTERCONNECT#	EMERGENCY VEHICLE PREEMPTION		
				70% FED 30% LA	70% FED 30% LA	70% FED 30% LA	70% FED 30% LA	100% ELMHURST	100% ELMHURST	
				0004	0021	0021	0021	0021	0043	
Δ 88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	2			2				
Δ 88102747	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	1			1				
Δ 88200210	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	10			10				
Δ 88500100	INDUCTIVE LOOP DETECTOR	EACH	12			12				
Δ 88600100	DETECTOR LOOP, TYPE I	FOOT	851			851				
Δ 88700200	LIGHT DETECTOR	EACH	3					3		
Δ 88700300	LIGHT DETECTOR AMPLIFIER	EACH	1					1		
Δ 88800100	PEDESTRIAN PUSH-BUTTON	EACH	4			4				
Δ 89000100	TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1			1				
Δ 89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	1,360		65		1,295			
Δ 89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1			1				
Δ 89502380	REMOVE EXISTING HANDHOLE	EACH	11			11				
Δ 89502385	REMOVE EXISTING CONCRETE FOUNDATION	EACH	10			10				
Δ A2007120	TREE, QUERCUS RUBRA (RED OAK), 2-1/2" CALIPER, BALLED AND BURLAPPED	EACH	2	2						
Δ D2001772	EVERGREEN, PICEA ABIES (NORWAY SPRUCE), 6' HEIGHT, BALLED AND BURLAPPED	EACH	3	3						
Δ K0029634	WEED CONTROL, PRE-EMERGENT GRANULAR HERBICIDE	POUND	1	1						
Δ X0324085	EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3/C	FOOT	807					807		
• X2090210	POROUS GRANULAR BACKFILL, SPECIAL	CU YD	170	170						
Δ X2110100	TOPSOIL FURNISH AND PLACE, SPECIAL	CU YD	515	515						
• X4403800	MEDIAN SURFACE REMOVAL	50 FT	7,823	7,823						
• X5420612	PIPE CULVERTS TO BE CLEANED 12"	FOOT	89	89						
• X5420624	PIPE CULVERTS TO BE CLEANED 24"	FOOT	104	104						
• X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	72	72						
• X5537900	STORM SEWERS TO BE CLEANED 15"	FOOT	218	218						
• X6020096	MANHOLES, TYPE A, 6'-DIAMETER, WITH 2 TYPE J FRAME, CLOSED LID, RESTRICTOR PLATE	EACH	1	1						
• X6026050	SANITARY MANHOLES TO BE ADJUSTED	EACH	3	3						
• X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	8	8						
Δ X6330725	STEEL PLATE BEAM GUARDRAIL (SHORT RADIUS)	FOOT	89	89						
• X6640300	CHAIN LINK FENCE REMOVAL	FOOT	700	700						
• X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1						
Δ X7030030	WET REFLECTIVE TEMPORARY TAPE TYPE III, 4 INCH	FOOT	10,851	10,851						
Δ X7830068	GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS AND SYMBOLS	50 FT	307						307	
Δ X7830070	GROOVING FOR RECESSED PAVEMENT MARKING 5"	FOOT	4,428						4,428	
Δ X7830074	GROOVING FOR RECESSED PAVEMENT MARKING 7"	FOOT	956						956	
Δ X7830078	GROOVING FOR RECESSED PAVEMENT MARKING 13"	FOOT	325						325	
Δ X7830090	GROOVING FOR RECESSED PAVEMENT MARKING 25"	FOOT	55						55	

\* STP (70% FED, 30% LA) Δ CMAQ (80% FED, 20% LA) PER LOCAL AGENCY AGREEMENT Δ SPECIAL PROVISION

† CONSTRUCTION TYPE CODE = 0042 Δ SPECIALTY ITEMS



DESIGNED - TKS	REVISED -
DRAWN - TKS	REVISED -
CHECKED - JMG	REVISED -
DATE - 10/22/2012	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

BUTTERFIELD ROAD AT COMMONWEALTH LANE  
 SUMMARY OF QUANTITIES

SCALE: NOT TO SCALE SHEET NO. 8 OF 108 SHEETS

F.A.U. RTE. 3545	SECTION 09-00170-00-CH	COUNTY DUPAGE	TOTAL SHEET NO. 108	SHEET NO. 8
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63742	

FILE PATH: G:\C:\800\0001\Road\Phase 2\Sheet\01\X8000-SHT-SumQty.dgn

PLAN SUBMITTED BY DATE  
 DATE: \_\_\_\_\_  
 PLAN NO.: \_\_\_\_\_  
 NOTE BOOK NO.: \_\_\_\_\_  
 REVISIONS: \_\_\_\_\_  
 CHECKED BY: \_\_\_\_\_  
 DATE: \_\_\_\_\_

SUMMARY OF QUANTITIES				ROADWAY#	LIGHTING#	TRAFFIC SIGNALS			NON-PARTICIPATING
CODE NO.	PAY ITEM	UNIT	QUANTITY	70% FED 30% LA 0004	70% FED 30% LA 0021	BUTTERFIELD RD & COMMONWEALTH LN 70% FED 30% LA	INTERCONNECT# 70% FED 30% LA	EMERGENCY VEHICLE PREEMPTION 100% ELMHURST 0021	100% ELMHURST 0043
Δ • X8210005	TEMPORARY LUMINAIRE, HIGH PRESSURE SODIUM VAPOR, HORIZONTAL MOUNT, 250 WATT	EACH	4		4				
Δ • X8710024	FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM24F	FOOT	1,304				1,304		
	Z0013798 CONSTRUCTION LAYOUT	L SUM	1	1					
	Z0018500 DRAINAGE STRUCTURES TO BE CLEANED	EACH	5	5					
	<del>Z0000400 PERIMETER EROSION BARRIER</del>	FOOT	4,146	4,146					
Δ • <del>70600240</del>	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE), TEST LEVEL 2	EACH	1	1					
	Z0030850 TEMPORARY INFORMATION SIGNING	SO FT	50	50					
Δ • Z0033046	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 2	EACH	1				1		
	Z0038117 PORTLAND CEMENT CONCRETE SURFACE REMOVAL 1"	SO YD	2,768	2,768					
	Z0056608 STORM SEWER (WATER MAIN REQUIREMENTS) 12 INCH	FOOT	38	38					
	Z0056610 STORM SEWER (WATER MAIN REQUIREMENTS) 15 INCH	FOOT	139	139					
	Z0062456 TEMPORARY PAVEMENT	SO YD	900	900					
Δ • Z0073510	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1			1			
+ • Z0076600	TRAINEES	HOOR	500	500					
+ • Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	HOOR	500	500					
	<del>XY008812</del> RELOCATE LANDSCAPING STONE	SO FT	271	271					
Δ • <del>X8570226</del>	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL	EACH	1			1			
Δ • <del>81603100</del>	UNIT DUCT, 600V, 4-1C NO.6, 1/C NO.10 GROUND, (XLP-TYPE USE), 1 1/4" DIA. POLYETHYLENE	FOOT	2,450		2,450				

# STP (70% FED, 30% LA)  
 CMAQ (80% FED, 20% LA)  
 PER LOCAL AGENCY  
 AGREEMENT

• SPECIAL PROVISION

‡ CONSTRUCTION TYPE CODE = 0042 Δ SPECIALTY ITEMS

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PLOT DATE = 12/4/2012	DATE - 10/22/2012	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

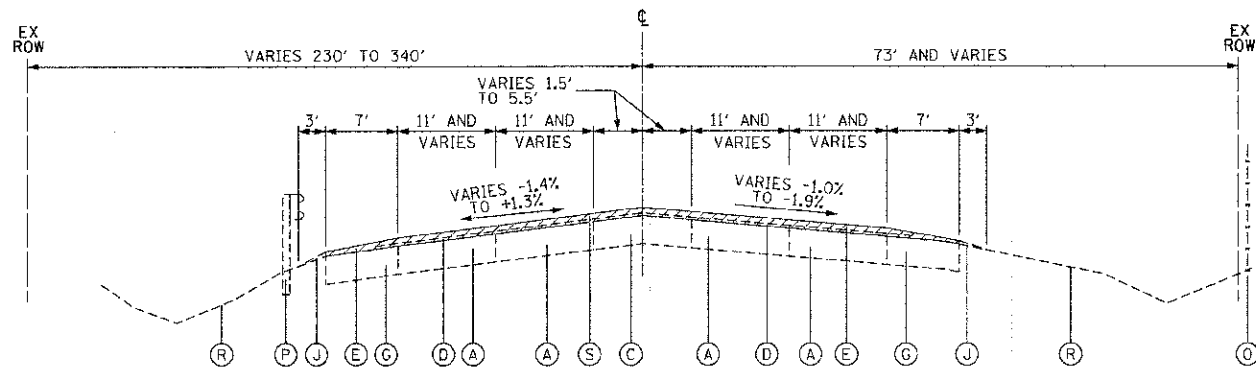
BUTTERFIELD ROAD AT COMMONWEALTH LANE  
 SUMMARY OF QUANTITIES

SCALE: NOT TO SCALE SHEET NO. 9 OF 108 SHEETS

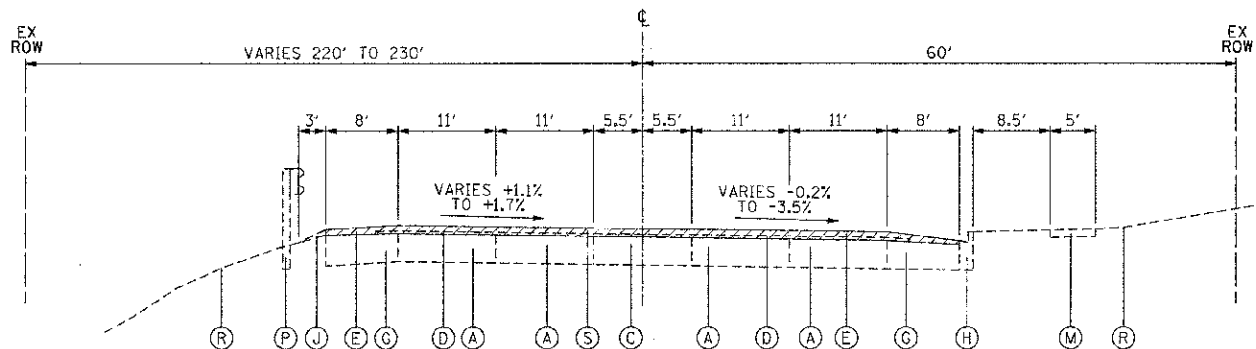
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CONTRACT NO. 63742			FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT	



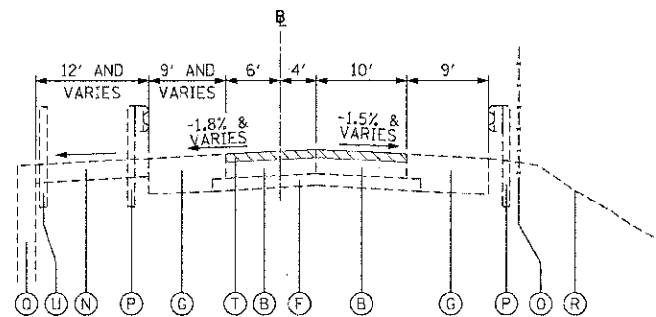
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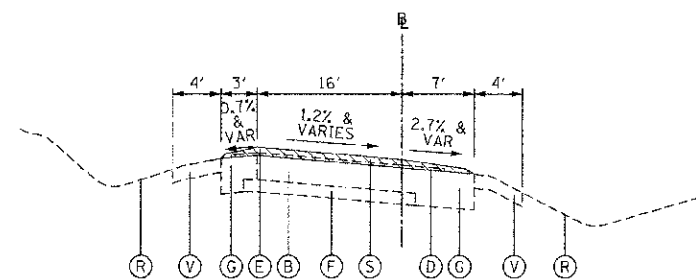
EXISTING TYPICAL SECTION  
BUTTERFIELD ROAD (ILLINOIS ROUTE 56)  
STATION 100+20.20 TO 106+86.57



EXISTING TYPICAL SECTION  
BUTTERFIELD ROAD (ILLINOIS ROUTE 56)  
STATION 106+86.57 TO 107+62.56



EXISTING TYPICAL SECTION  
COMMONWEALTH LANE  
STATION 200+61.54 TO 201+89.62



EXISTING TYPICAL SECTION  
ILLINOIS ROUTE 38 EXIT RAMP  
STATION 402+42.61 TO 403+00.00

- EXISTING LEGEND**
- (A) CONTINUOUSLY REINFORCED CEMENT CONCRETE PAVEMENT (8")
  - (B) PORTLAND CEMENT CONCRETE PAVEMENT (8")
  - (C) HOT-MIX ASPHALT BINDER COURSE (10" AND VARIES)
  - (D) HOT-MIX ASPHALT LEVELING BINDER COURSE (3/2" AND VARIES)
  - (E) HOT-MIX ASPHALT SURFACE COURSE (1 3/4" AND VARIES)
  - (F) STABILIZED SUB-BASE (BITUMINOUS AGGREGATE MIXTURE) 4"
  - (G) STABILIZED SHOULDER (BITUMINOUS AGGREGATE MIXTURE) (11 3/4" AND VARIES)
  - (H) COMBINATION CONCRETE CURB & GUTTER, TY B-6.12
  - (I) COMBINATION CONCRETE CURB & GUTTER, TY B-9.06
  - (J) AGGREGATE SHOULDER, TY B
  - (K) PORTLAND CEMENT CONCRETE MEDIAN, TY C-4 MODIFIED
  - (L) PORTLAND CEMENT CONCRETE MEDIAN SURFACE, 4"
  - (M) PORTLAND CEMENT CONCRETE SIDEWALK, 5"
  - (N) HOT-MIX ASPHALT BIKE PATH
  - (O) CHAIN LINK FENCE
  - (P) STEEL PLATE BEAM GUARDRAIL
  - (Q) RETAINING WALL
  - (R) GROUND SURFACE (ASSUME EXISTING 7" TOPSOIL DEPTH)
  - (S) HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
  - (T) PORTLAND CEMENT CONCRETE SURFACE REMOVAL 1"
  - (U) HANDRAIL
  - (V) AGGREGATE SHOULDER

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USER NAME - tkandell	DESIGNED - TKH	REVISIONS -
FILE NAME - 090808-SHT-1.dwg	DRAWN - TKH	REVISIONS -
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PLOT DATE - 10/21/2012	DATE - 10/22/2012	REVISIONS -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BUTTERFIELD ROAD AT COMMONWEALTH LANE  
EXISTING TYPICAL SECTIONS

SCALE: NOT TO SCALE SHEET NO. 10 OF 108 SHEETS

F.A.J. RTE. 3545	SECTION 09-00170-00-CH	COUNTY DUPAGE	TOTAL SHEETS 108	SHEET NO. 10
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63742	







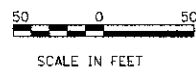
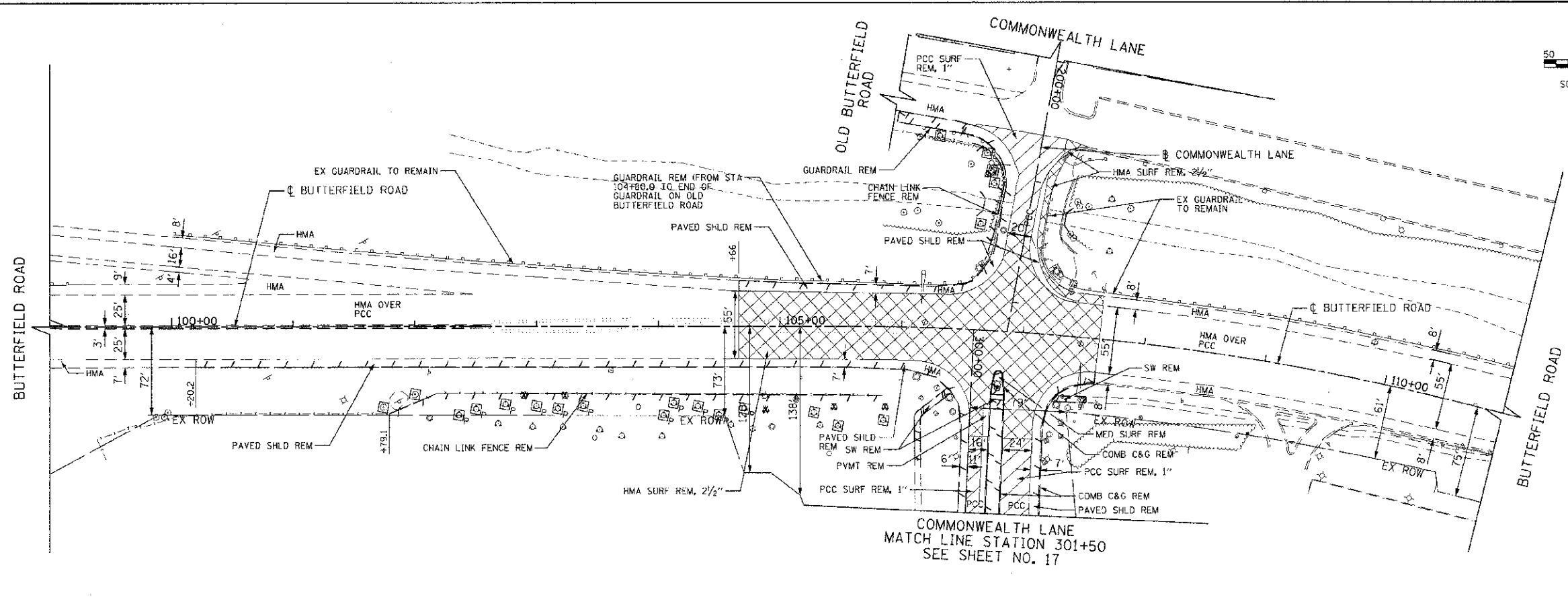




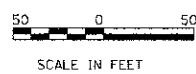
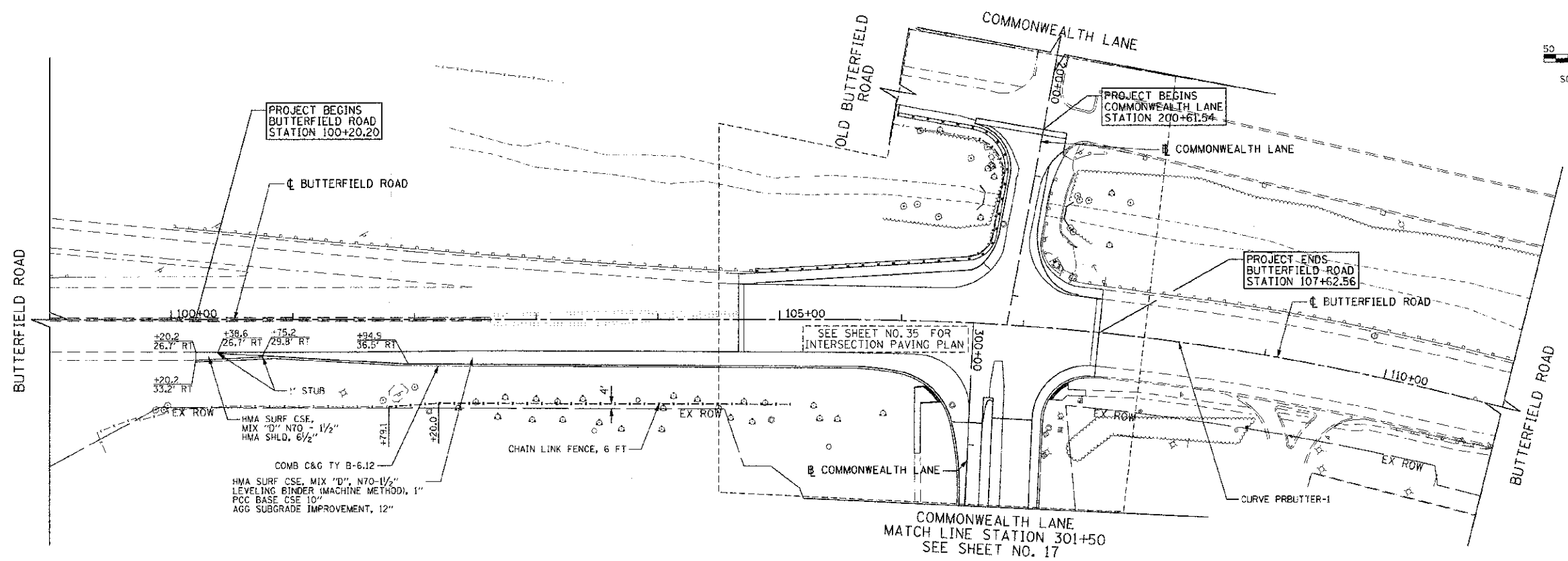


PLAN	DATE
REVISIONS	BY
NOTE BOOK	ALIGNMENT CHECKED
NO.	BY FILE NAME

PROFILE	DATE
REVISIONS	BY
NOTE BOOK	GRADES CHECKED
NO.	BY FILE NAME



COMMONWEALTH LANE  
MATCH LINE STATION 301+50  
SEE SHEET NO. 17



EX CURVE PRBUTTER-1  
PI STA. = 108+08.84  
 $\Delta = 14^\circ 04' 08''$  (RT)  
 $D = 2^\circ 46' 04''$   
 $R = 2,070.00'$   
 $T = 255.43'$   
 $L = 508.29'$   
 $E = 15.70'$   
 $\theta =$  MATCH EXISTING  
T.R. = MATCH EXISTING  
S.E. RUN = MATCH EXISTING  
P.C. STA. = 105+53.41  
P.T. STA. = 110+61.70

- NOTES:
- FOR PROPOSED RIGHT-OF-WAY INFORMATION, SEE PLAT OF HIGHWAY SHEETS NO. 30 TO 34.
  - FOR PROFILE INFORMATION, SEE ROADWAY PROFILE SHEET NO. 18 AND INTERSECTION PAVING PLAN SHEET NO. 35.
  - PROPOSED PAVEMENT SECTION CONSISTS OF:  
HMA SURFACE COURSE, MIX "D" N50 - 1 1/2"  
LEVELING BINDER (MACHINE METHOD) N50 - DEPTH VARIES  
OR  
HMA BINDER COURSE, IL-19.0 N50 - DEPTH VARIES FOR BINDER DEPTHS, SEE SHEET NO. 36.
  - A QUANTITY FOR CLASS D PATCHES, TYPE 1, 8" AND CLASS D PATCHES, TYPE II, 8" IS INCLUDED IN THE PLANS. THE ENGINEER SHALL APPROVE THE LOCATIONS OF PAVEMENT PATCHING PRIOR TO PLACEMENT.

COMMONWEALTH LANE  
MATCH LINE STATION 301+50  
SEE SHEET NO. 17

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PLOT DATE = 10/21/2012		DATE = 10/22/2012	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

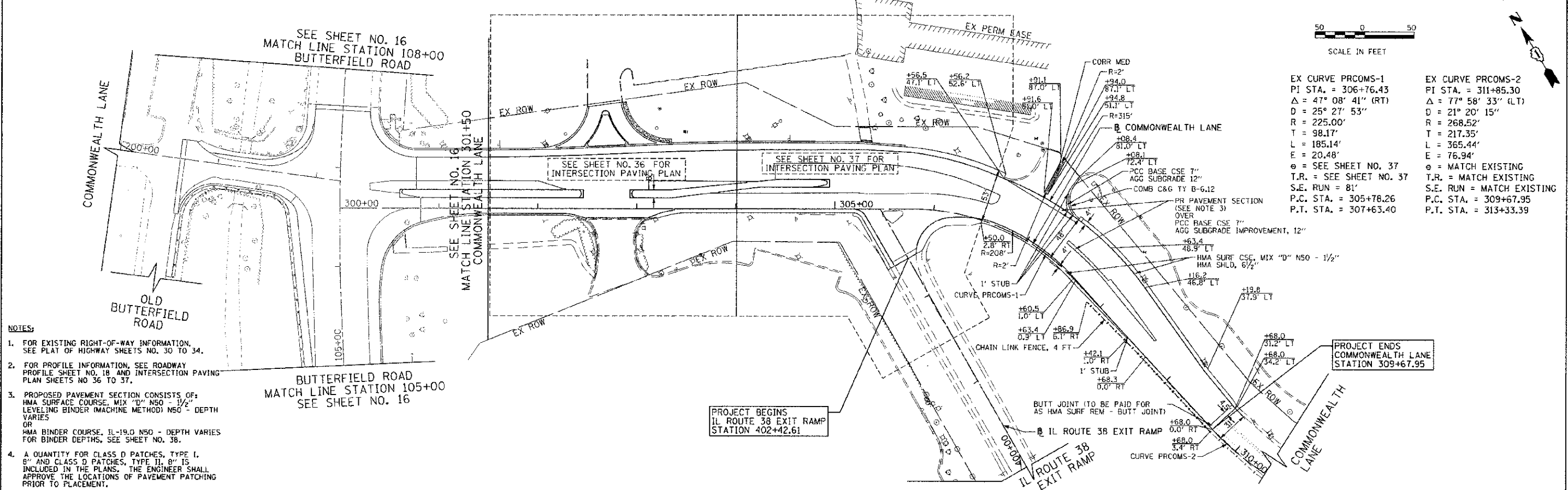
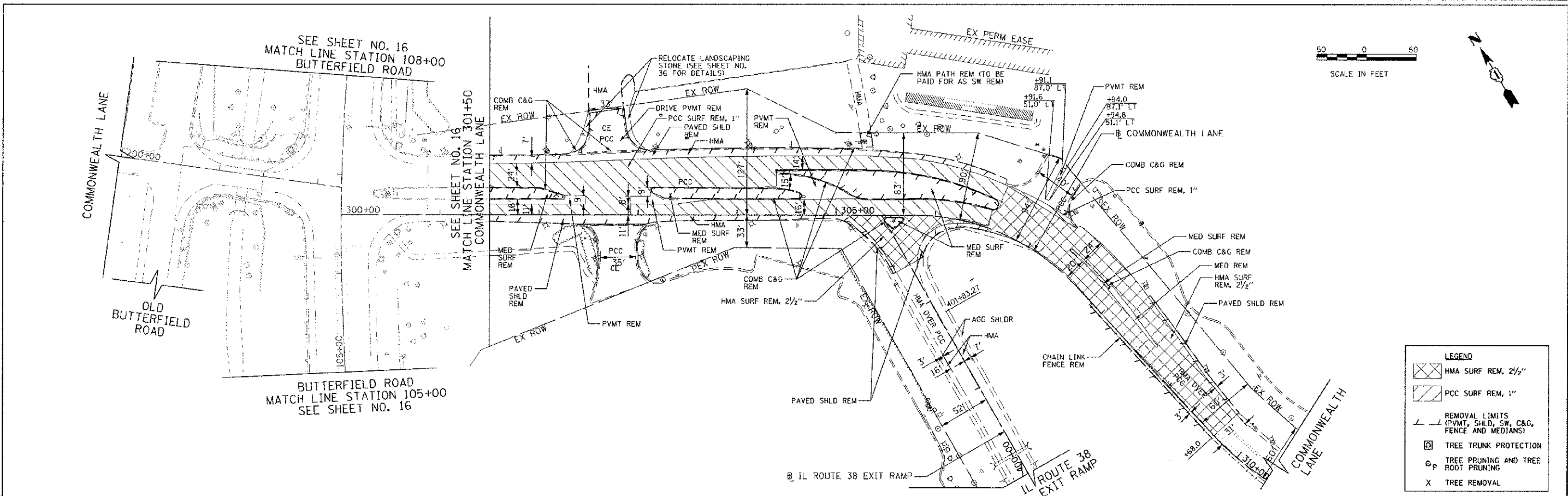
BUTTERFIELD ROAD AT COMMONWEALTH LANE  
REMOVAL PLAN AND ROADWAY PLAN

SCALE: 1"=50' SHEET NO. 16 OF 108 SHEETS STA. 99+00 TO STA. 111+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	NO.
3545	09-00170-00-CH	DUPAGE	108	16
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63742	

DATE	BY
DESIGNED	TKS
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CHECKED	JMC
DATE	10/22/2012

DATE	BY
DESIGNED	TKS
DRAWN	TKS
CHECKED	JMC
DATE	10/22/2012

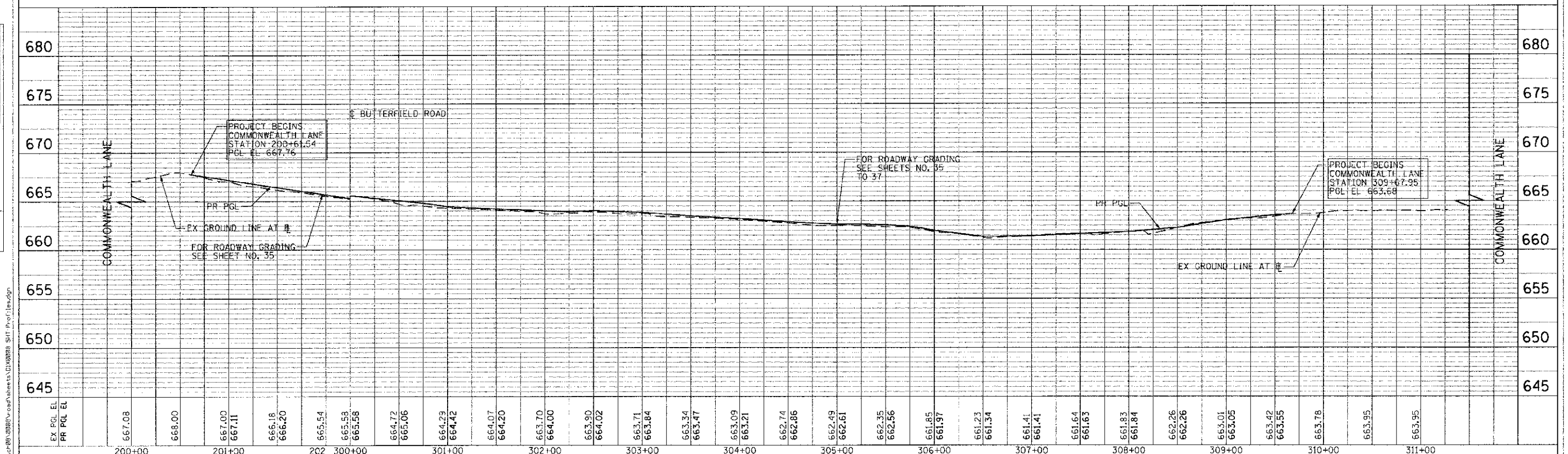
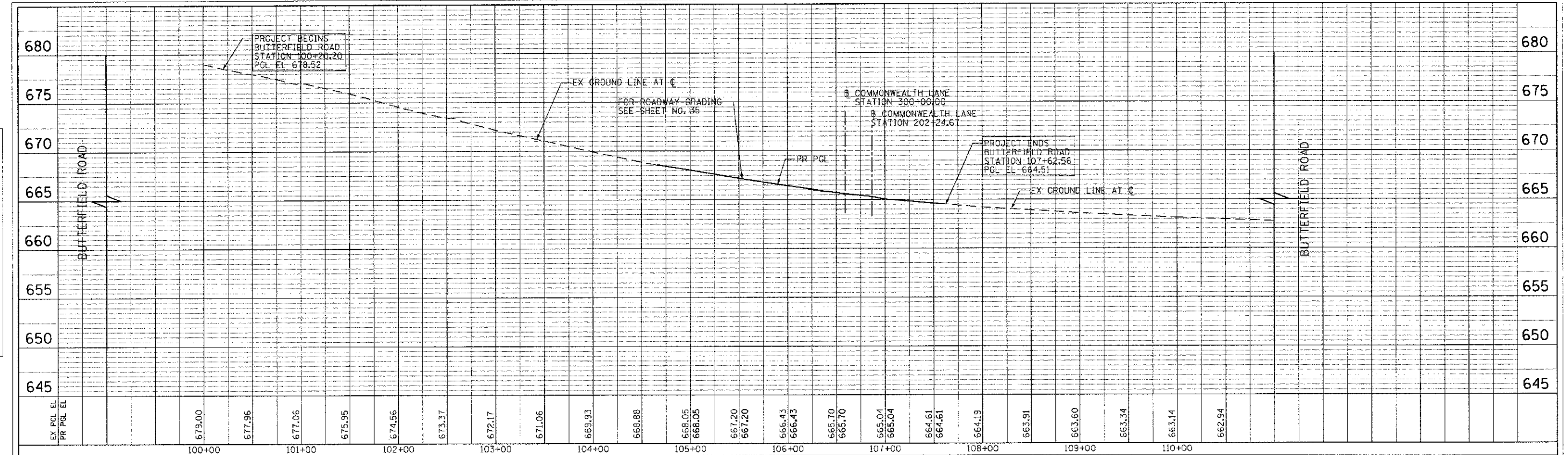


- NOTES:**
- FOR EXISTING RIGHT-OF-WAY INFORMATION, SEE PLAT OF HIGHWAY SHEETS NO. 30 TO 34.
  - FOR PROFILE INFORMATION, SEE ROADWAY PROFILE SHEET NO. 18 AND INTERSECTION PAVING PLAN SHEETS NO 36 TO 37.
  - PROPOSED PAVEMENT SECTION CONSISTS OF:  
HMA SURFACE COURSE, MIX "D" NSO - 1 1/2" LEVELING BINDER (MACHINE METHOD) NSO - DEPTH VARIES  
OR  
HMA BINDER COURSE, IL-19.0 NSO - DEPTH VARIES FOR BINDER DEPTHS, SEE SHEET NO. 38.
  - A QUANTITY FOR CLASS D PATCHES, TYPE I, 6" AND CLASS D PATCHES, TYPE II, 9" IS INCLUDED IN THE PLANS. THE ENGINEER SHALL APPROVE THE LOCATIONS OF PAVEMENT PATCHING PRIOR TO PLACEMENT.

FILE NAME -	USER NAME = kalorenz	DESIGNED - TKS	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BUTTERFIELD ROAD AT COMMONWEALTH LANE REMOVAL PLAN AND ROADWAY PLAN</b>	F.A.U.:	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
GA:\CH08\0808\Road\Phase 2\Sheets\18\1808R	SHT-Plan2.dgn	DRAWN - TKS	REVISED -			3545	09-00170-00-CH	DUPAGE	108	17	
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PLOT DATE = 12/4/2012	DATE = 10/22/2012	REVISED -	REVISED -			FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT					

PLAN	SURVEYED	DATE
NOTED	BY	
W/TE BOOK	BY	
NO.	DATE	

PROFILE	SURVEYED	DATE
NOTED	BY	
W/TE BOOK	BY	
NO.	DATE	



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 PLOT DATE = 10/21/2012

DESIGNED - TKS  
 DRAWN - TKS  
 CHECKED - JMG  
 DATE - 10/22/2012

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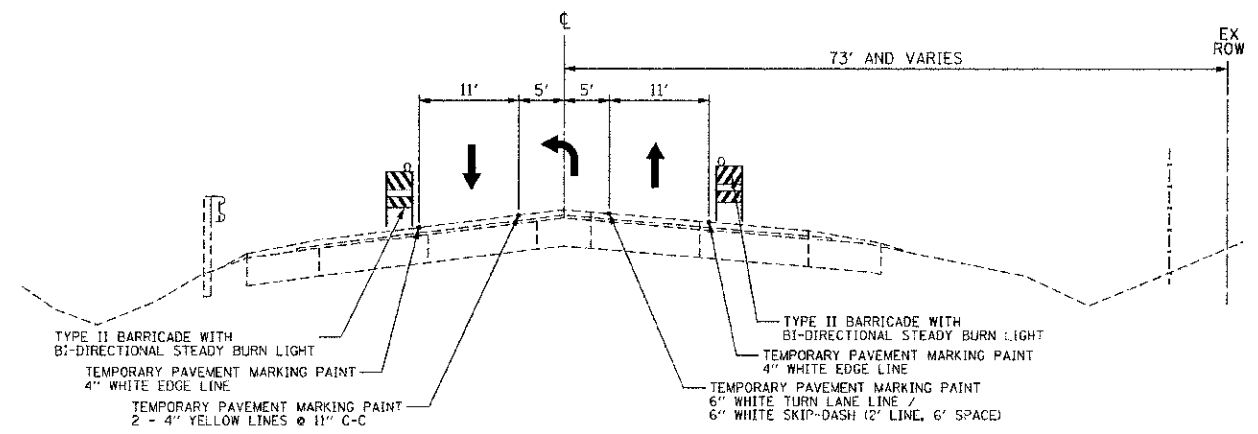
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

BUTTERFIELD ROAD AT COMMONWEALTH LANE  
 PROPOSED PROFILE

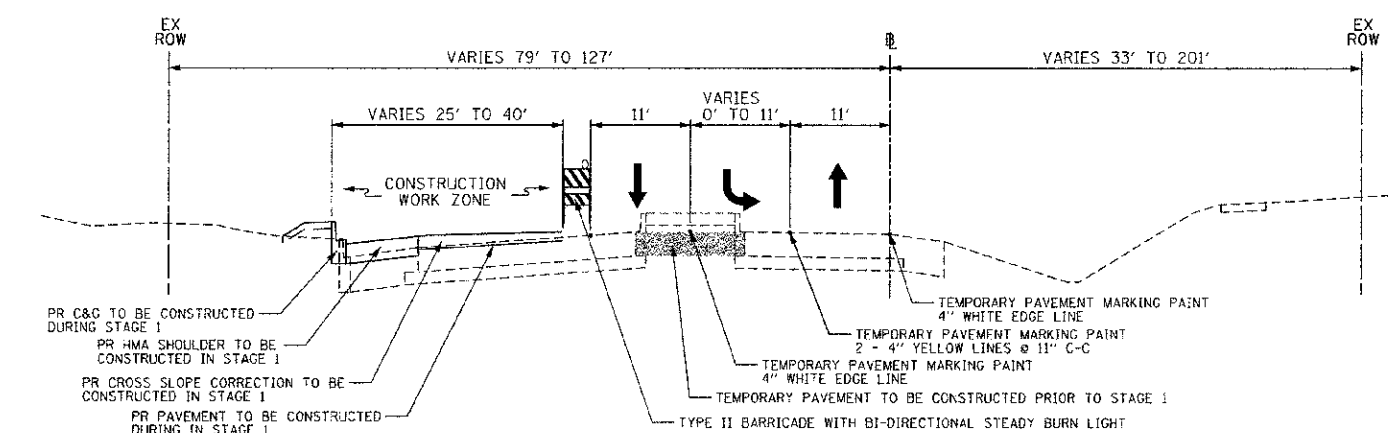
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 VERT: 1"=20'  
 SHEET NO. 18 OF 108 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 63742				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

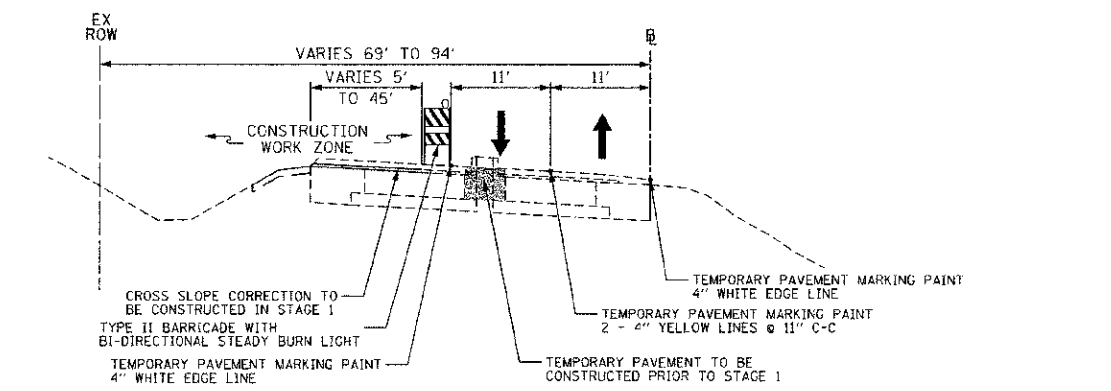
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DESCRIPTION	



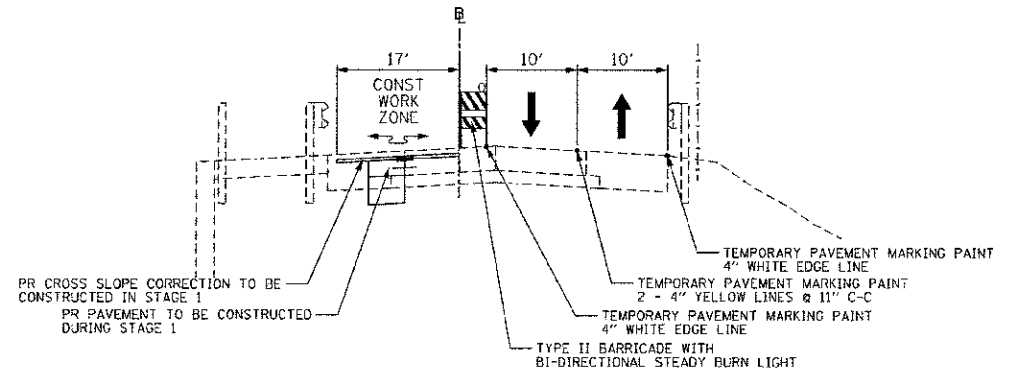
**TRAFFIC CONTROL TYPICAL SECTION - STAGE 1**  
BUTTERFIELD ROAD  
STATION 100+20 TO STATION 107+63



**TRAFFIC CONTROL TYPICAL SECTION - STAGE 1**  
COMMONWEALTH LANE  
STATION 300+32 TO STATION 304+65



**TRAFFIC CONTROL TYPICAL SECTION - STAGE 1**  
COMMONWEALTH LANE  
STATION 304+65 TO 309+68



**TRAFFIC CONTROL TYPICAL SECTION - STAGE 1**  
COMMONWEALTH LANE  
STATION 200+61 TO 201+90

**PRIOR TO STAGE 1**

1. MAINTAIN TWO-WAY TRAFFIC FLOW ON BUTTERFIELD ROAD, COMMONWEALTH LANE AND ONE-WAY TRAFFIC ON THE ILLINOIS ROUTE 38 EXIT RAMP.
2. THE FOLLOWING SHALL BE COMPLETED USING STATE STANDARD 701502-05 FOR URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE. A MINIMUM OF ONE LANE SHALL BE MAINTAINED IN EACH DIRECTION.
  - A. REMOVE THE MEDIAN ISLANDS ALONG COMMONWEALTH LANE. REPLACE WITH TEMPORARY PAVEMENT.
  - B. UTILITIES SHALL BE RELOCATED AS SHOWN ON THE DRAINAGE AND UTILITY PLANS (SEE SHEETS NO. 28 AND 29)
3. INSTALL TEMPORARY TRAFFIC SIGNALS (SEE SHEETS NO. 43 TO 47).

**STAGE 1**

1. USE TRAFFIC CONTROL STAGE 1 PLANS (SEE SHEET NO. 22 TO 24).
2. ESTABLISH TWO-WAY TRAFFIC FLOW ALONG THE CENTERLINE OF BUTTERFIELD ROAD. MAINTAIN A MINIMUM OF ONE 11' THROUGH LANE IN EACH DIRECTION THROUGHOUT THE ENTIRE LENGTH.
3. ESTABLISH TWO-WAY TRAFFIC FLOW ALONG THE SOUTH EDGE OF PAVEMENT OF COMMONWEALTH LANE NORTH OF BUTTERFIELD ROAD. MAINTAIN A MINIMUM OF ONE 10' THROUGH LANE IN EACH DIRECTION THROUGHOUT THE ENTIRE LENGTH.
4. ESTABLISH TWO-WAY TRAFFIC FLOW ALONG THE SOUTH EDGE OF PAVEMENT OF COMMONWEALTH LANE SOUTH OF BUTTERFIELD ROAD. MAINTAIN A MINIMUM OF ONE 11' THROUGH LANE THROUGHOUT THE ENTIRE LENGTH.
5. MAINTAIN EXISTING TRAFFIC FLOW ALONG THE ILLINOIS ROUTE 38 EXIT RAMP.
6. ESTABLISH EROSION CONTROL METHODS WITHIN THE STAGE 1 CONSTRUCTION WORK ZONE AS SHOWN ON THE EROSION CONTROL PLAN (SEE SHEET NO. 27).
7. CONSTRUCT PROPOSED STORM SEWER WITHIN THE STAGE 1 CONSTRUCTION WORK ZONE AS SHOWN ON THE DRAINAGE AND UTILITY PLANS (SEE SHEETS NO. 28 AND 29).
8. REMOVE EXISTING CURB & GUTTER AND SHOULDERS WITHIN THE STAGE 1 CONSTRUCTION WORK ZONE. MILL 1" OF THE CONCRETE SURFACE AND MILL THE HMA SURFACE AND BINDER COURSE WITHIN THE STAGE 1 CONSTRUCTION WORK ZONE.
9. CONSTRUCT PAVEMENT PATCHES AS DIRECTED BY THE ENGINEER WITHIN THE STAGE 1 WORK ZONE.
10. CONSTRUCT PROPOSED SHOULDER, CURB & GUTTER AND PCC BASE COURSE ALONG COMMONWEALTH LANE WITHIN THE STAGE 1 CONSTRUCTION WORK ZONE.
11. ALONG COMMONWEALTH LANE NORTH OF BUTTERFIELD ROAD, REMOVE ALL DRUMS AND TYPE II BARRICADES ONCE STAGE 1 CONSTRUCTION IS COMPLETE.
12. BEGIN CROSS SLOPE CORRECTION WITH THE EXCEPTION OF THE HMA SURFACE COURSE ALONG THE WESTBOUND LANES OF COMMONWEALTH LANE. FOR PAVING DETAILS, SEE SHEETS NO. 35 TO 38.
13. CONSTRUCT THE PROPOSED DRIVEWAY AT STATION 302+70. THE DRIVEWAY MUST REMAIN OPEN TO TRAFFIC AT ALL TIMES.
14. CONSTRUCT THE PROPOSED PAVEMENT AND CURB & GUTTER ALONG THE SIDESTREET AT STATION 306+90 BY ALIGNING TRAFFIC ALONG THE WEST EDGE OF PAVEMENT. WHEN THE PAVEMENT AND CURB & GUTTER IS CONSTRUCTED, SPLIT TRAFFIC ALONG THE OUTSIDE EDGES OF PAVEMENT OF THE SIDESTREET TO CONSTRUCT THE CORRUGATED MEDIAN. THE ROAD SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES.
15. REMOVE EXISTING AND INSTALL PROPOSED LIGHT POLES ALONG THE NORTH SIDE OF COMMONWEALTH LANE.
16. COMPLETE LANDSCAPING ALONG THE NORTH SIDE OF COMMONWEALTH LANE.

**GENERAL NOTES**

1. THE CONTRACTOR SHALL MAINTAIN A MINIMUM OF ONE 11' THROUGH LANE IN EACH DIRECTION FOR TWO-WAY TRAFFIC FLOW UNLESS OTHERWISE SPECIFIED. A MINIMUM OF 3' SHALL BE MAINTAINED BETWEEN TRAFFIC AND CONSTRUCTION AT ALL TIMES UNLESS OTHERWISE NOTED IN THE PLANS.
2. THE CONTRACTOR SHALL MAINTAIN TRAFFIC IN ACCORDANCE WITH THE SPECIAL PROVISIONS, STATE STANDARDS, STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER.
3. THE ENGINEER SHALL BE INFORMED 48 HOURS IN ADVANCE OF ANY CHANGE IN CONSTRUCTION STAGING.
4. THE ENGINEER SHALL CONTACT THE ILLINOIS DEPARTMENT OF TRANSPORTATION TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 IN ADVANCE OF BEGINNING WORK.
5. TEMPORARY LANE CLOSURES FOR ANY REASON SHALL BE RESTRICTED TO THE WEEKDAY HOURS OF 9:00 AM TO 3:30 PM, AS APPROVED IN ADVANCE BY THE ENGINEER.
6. DRUMS OR TYPE II BARRICADES SHALL BE EQUIPPED WITH BI-DIRECTIONAL STEADY BURN LIGHTS AND SHALL BE PLACED AT 25' INTERVALS ALONG THE PROPOSED CONSTRUCTION WORK ZONE AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. TYPE II BARRICADES SHALL HAVE A MINIMUM REFLECTORIZED AREA OF 288 SQUARE INCHES. TYPE II BARRICADES, 2' IN WIDTH, SHALL BE USED IN ALL STAGES OF CONSTRUCTION.
7. ALL DRIVEWAYS SHALL BE OPEN TO TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED.
8. WET REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE, TYPE III, SHALL BE USED ON ALL FINAL PAVEMENT WEARING SURFACES. TEMPORARY PAVEMENT MARKING PAINT SHALL BE USED ELSEWHERE.
9. THE CONTRACTOR SHALL BE REQUIRED TO REMOVE ALL EXISTING PAVEMENT MARKINGS WHICH CONFLICT WITH THE DESIGNATED TRAFFIC CONTROL PLAN. THIS WORK SHALL BE PAID FOR AS PAVEMENT MARKING REMOVAL.
10. ALL TEMPORARY PAVEMENT MARKINGS SHOWING DETEIORATION AFTER 7 DAYS SHALL BE REPLACED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER. SUFFICIENT QUANTITIES FOR 1 PLACEMENT AND 2 REPLACEMENTS HAVE BEEN PROVIDED FOR EACH STAGE. ALL MARKINGS THAT REQUIRE REPLACEMENT PRIOR TO 7 DAYS OF SERVICE OR REPLACEMENT AFTER THE THIRD REPLACEMENT SHALL BE REPLACED BY THE CONTRACTOR AT HIS OWN EXPENSE.
11. TEMPORARY PAVEMENT SHALL ADHERE TO THE TEMPORARY PAVEMENT SPECIAL PROVISION. TEMPORARY PAVEMENT SHALL CONSIST OF 2 INCHES OF HMA SURFACE COURSE AND 8 INCHES OF HMA BINDER COURSE.
12. THE FURNISHING, INSTALLING, AND RELOCATION OF ALL TRAFFIC SIGNS SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND THE STANDARD SPECIFICATIONS. THE WORK SHALL BE INCLUDED IN THE COST FOR TRAFFIC CONTROL AND PROTECTION. (SPECIAL) ALL CONFLICTING TRAFFIC SIGNS SHALL BE COVERED AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION. (SPECIAL).
13. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING DRAINAGE OF THE ROADWAY DURING ALL STAGES OF CONSTRUCTION. A QUANTITY OF 3 INLETS, TYPE A, TYPE B GRATE AND 200' OF STORM SEWERS, CLASS A, TYPE 1 12" HAS BEEN PROVIDED FOR TEMPORARY USE. REMOVAL OF THESE ITEMS SHALL BE INCLUDED IN THEIR COST. ALL EXISTING STORM SEWER SHALL REMAIN FUNCTIONAL AND INTACT UNTIL NEW STORM SEWER IS FUNCTIONAL. AT THE END OF EACH DAYS WORK, THE STORM SEWER SHALL BE CONNECTED AND FUNCTIONAL.
14. ALL TRAFFIC CONTROL WARNING SIGNS AND ASSOCIATED SIGNING MOUNTED WITH THE WARNING SIGNS SHALL HAVE BLACK LEGENDS AND BORDERS ON FLUORESCENT ORANGE REFLECTIVE SHEETING.
15. ALL CONSTRUCTION SIGNS, BARRICADES, AND OTHER DEVICES REQUIRED TO CONTROL TRAFFIC SHALL BE FURNISHED, INSTALLED, AND MAINTAINED BY THE CONTRACTOR.
16. ALL TRAFFIC CONTROL DEVICES SHALL BE REMOVED, COVERED OR TURNED AWAY FROM THE TRAFFIC IMMEDIATELY WHEN THEY ARE NO LONGER NECESSARY. WHEN A SIGN IS COVERED, ITS POST SHALL HAVE A REFLECTIVE 3 INCH X 6 INCH DELINEATOR INSTALLED.
17. THE TRAFFIC CONTROL DEPICTED HEREIN IS THE MINIMUM REQUIREMENT. ADDITIONAL TRAFFIC CONTROL DEVICES AS SPECIFIED BY THE SPECIAL PROVISIONS SHALL BE PLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER. ALL TRAFFIC CONTROL DEVICES SHALL BE CONSIDERED INCLUDED IN THE COST OF THE LUMP SUM PAY ITEM "TRAFFIC CONTROL AND PROTECTION. (SPECIAL)" UNLESS OTHERWISE INDICATED IN THE PLANS OR SPECIAL PROVISIONS.
18. A QUANTITY OF HOT-MIX ASPHALT SURFACE COURSE MIX "D", NSO HAS BEEN INCLUDED FOR TEMPORARY INTERSECTION GRADING FOR ELEVATION DIFFERENCES BETWEEN EXISTING, TEMPORARY AND PROPOSED PAVEMENT.
19. A BIKE PATH CONNECTION SHALL BE MAINTAINED THROUGHOUT CONSTRUCTION FROM THE INTERSECTION OF BUTTERFIELD ROAD AND COMMONWEALTH LANE TO THE EXISTING SALT CREEK TRAIL WITHIN THE PARKING LOT AT STATION 305+20 LT. A QUANTITY OF AGGREGATE FOR TEMPORARY ACCESS HAS BEEN INCLUDED TO MAINTAIN THE BIKE PATH CONNECTION.

**NOTE:**

BOXED ITEMS ARE INCLUDED IN THE COST AS DESIGNATED.



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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**SUGGESTED CONSTRUCTION PROCEDURES AND TRAFFIC CONTROL**  
**TYPICAL SECTIONS AND NOTES**

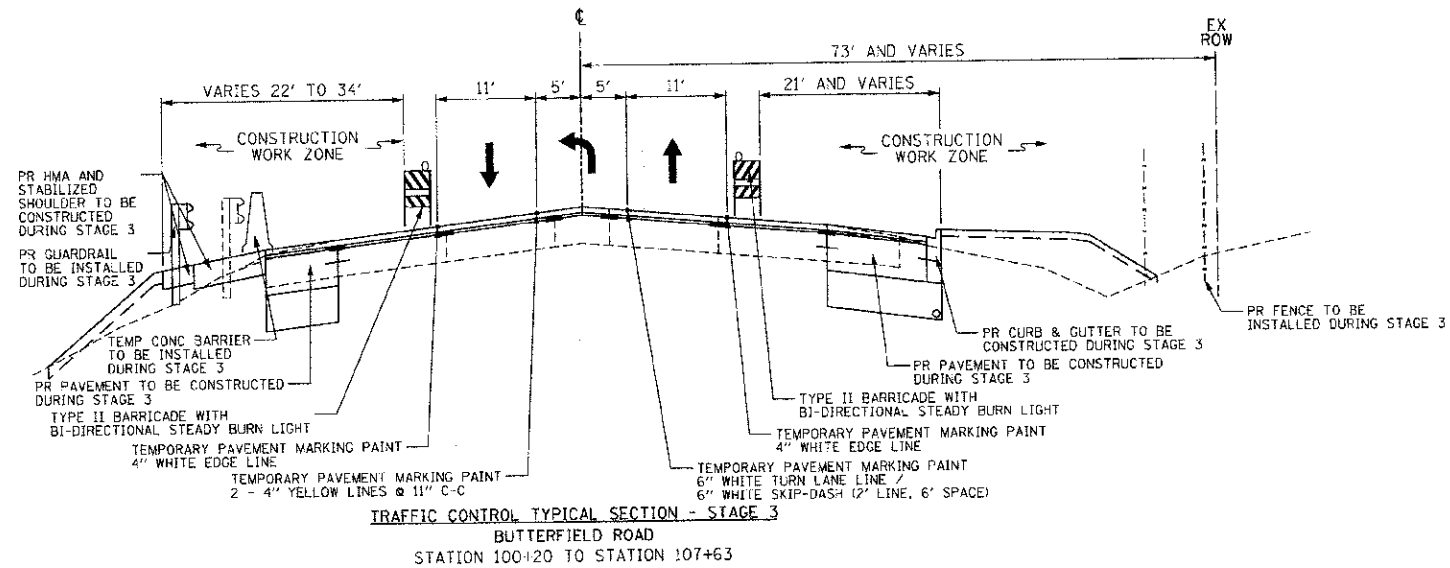
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FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63742

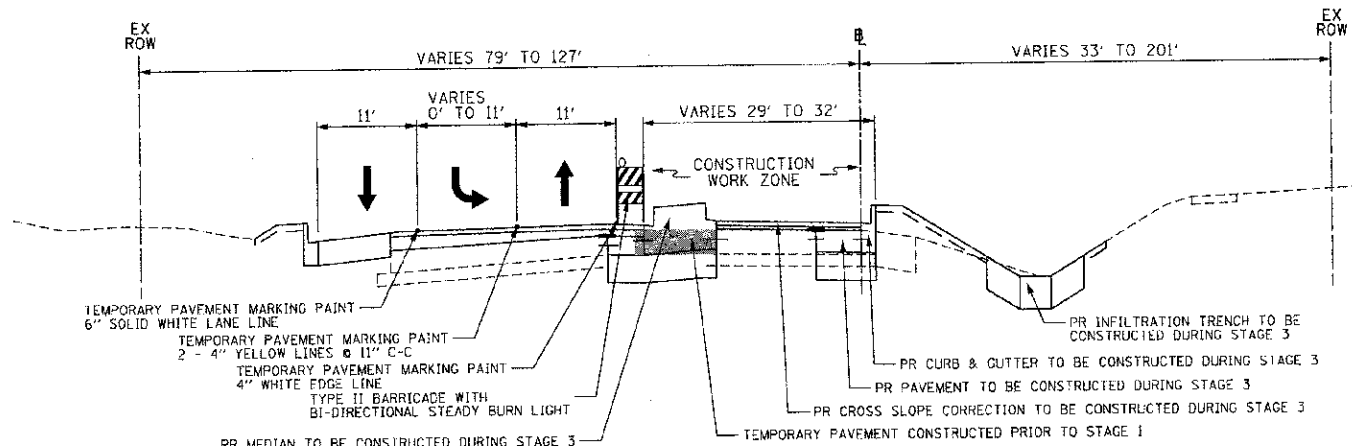




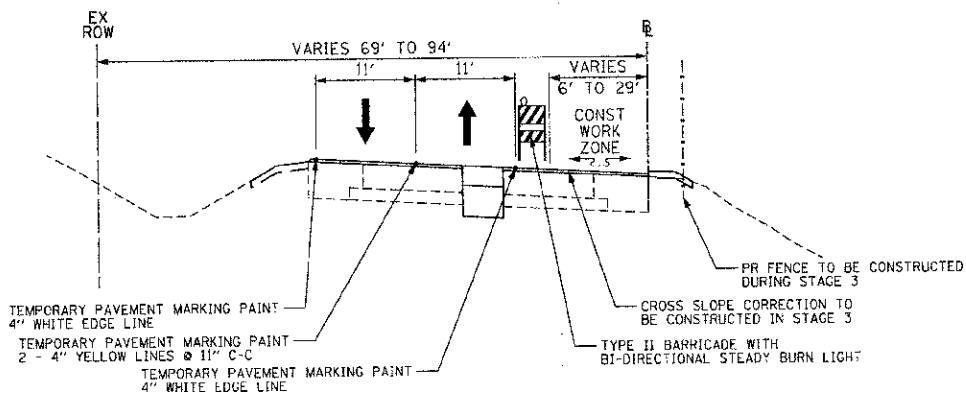
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TRAFFIC CONTROL TYPICAL SECTION - STAGE 3  
BUTTERFIELD ROAD  
STATION 100+20 TO STATION 107+63



TRAFFIC CONTROL TYPICAL SECTION - STAGE 3  
COMMONWEALTH LANE  
STATION 300+32 TO 304+65



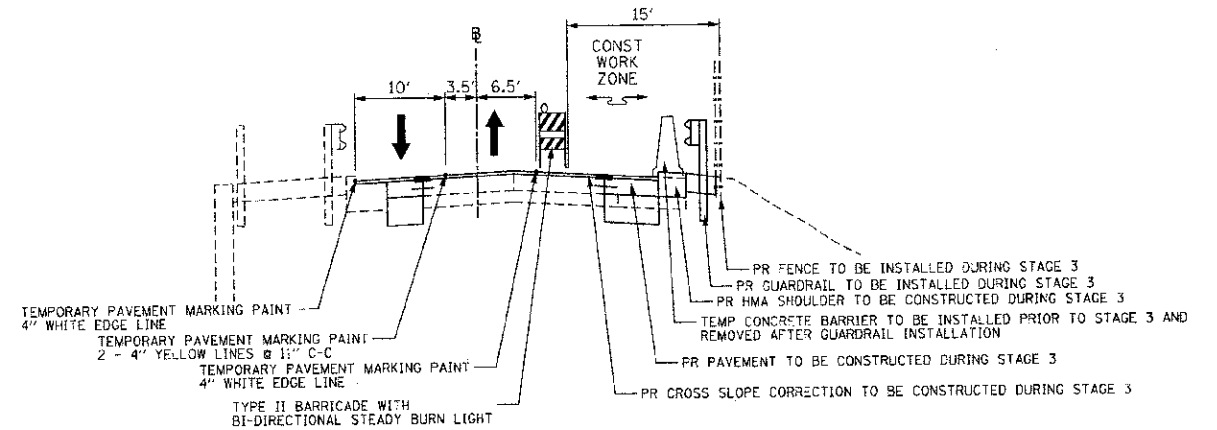
TRAFFIC CONTROL TYPICAL SECTION - STAGE 3  
COMMONWEALTH LANE  
STATION 304+65 TO 309+68

STAGE 3

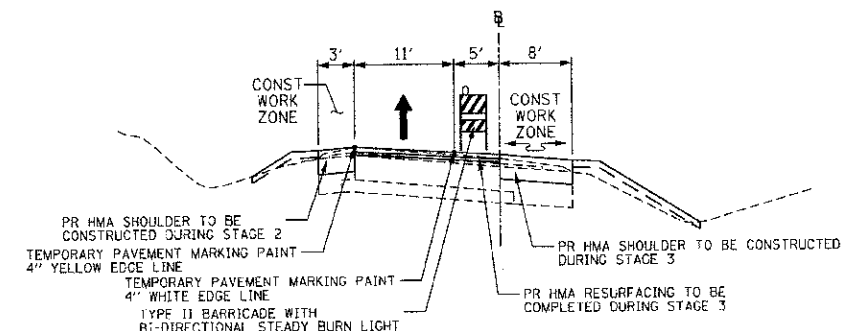
- USE TRAFFIC CONTROL STAGE 3 PLANS (SEE SHEET NO. 26 AND SHEETS NO. 22 AND 24).
- MAINTAIN THE STAGE 1 BUTTERFIELD ROAD TRAFFIC CONFIGURATION.
- ESTABLISH TWO-WAY TRAFFIC FLOW ALONG THE NORTH EDGE OF PAVEMENT OF COMMONWEALTH LANE NORTH OF BUTTERFIELD ROAD. MAINTAIN A MINIMUM OF ONE 10' THROUGH LANE IN EACH DIRECTION THROUGHOUT THE ENTIRE LENGTH.
- ESTABLISH TWO-WAY TRAFFIC FLOW ALONG THE NORTH SIDE OF COMMONWEALTH LANE SOUTH OF BUTTERFIELD ROAD. MAINTAIN ONE 11' LANE THROUGHOUT THE ENTIRE LENGTH.
- ESTABLISH ONE 11' LANE ALONG THE WEST EDGE OF PAVEMENT OF THE ILLINOIS ROUTE 38 EXIT RAMP. MAINTAIN ONE 11' LANE THROUGHOUT THE ENTIRE LENGTH.
- INSTALL TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATOR AS SHOWN ON THE TRAFFIC CONTROL STAGE 3 PLANS (SEE SHEET NO. 26). THE TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATOR SHALL REMAIN IN PLACE FOR THE DURATION OF PROPOSED GUARDRAIL INSTALLATION AND SHALL BE REMOVED ONCE GUARDRAIL INSTALLATION IS COMPLETE.
- ESTABLISH EROSION CONTROL METHODS AS SHOWN ON THE EROSION CONTROL PLAN FOR THE STAGE 3 CONSTRUCTION WORK ZONE (SEE SHEET NO. 27).
- CONSTRUCT PROPOSED STORM SEWER AND INFILTRATION TRENCH WITHIN THE STAGE 3 CONSTRUCTION WORK ZONE AS SHOWN ON THE DRAINAGE AND UTILITY PLANS (SEE SHEET NO. 28 AND 29).
- CONSTRUCT REMAINING PAVEMENT AND PAVEMENT PATCHES AS DIRECTED BY THE ENGINEER WITHIN THE STAGE 3 CONSTRUCTION WORK ZONE.

STAGE 3 (CONTINUED)

- REMOVE EXISTING CURB & GUTTER, PAVEMENT AND SHOULDERS WITHIN THE STAGE 3 CONSTRUCTION WORK ZONE. MILL 1" OF THE CONCRETE SURFACE AND MILL THE HMA SURFACE AND BINDER COURSE WITHIN THE STAGE 3 CONSTRUCTION WORK ZONE.
- CONSTRUCT PAVEMENT PATCHES AS DIRECTED BY THE ENGINEER WITHIN THE STAGE 3 CONSTRUCTION WORK ZONE.
- CONSTRUCT THE PROPOSED SHOULDER, STABILIZED SHOULDER AND INSTALL THE GUARDRAIL ALONG BUTTERFIELD ROAD AND COMMONWEALTH LANE NORTH OF BUTTERFIELD ROAD.
- ALONG COMMONWEALTH LANE NORTH OF BUTTERFIELD ROAD, REMOVE ALL DRUMS AND TYPE II BARRICADES ONCE STAGE 3 CONSTRUCTION IS COMPLETE.
- CONSTRUCT THE PROPOSED RIGHT TURN LANE AND CURB AND GUTTER ALONG BUTTERFIELD ROAD. CONSTRUCT PAVEMENT WIDENING, PROPOSED SHOULDER, PROPOSED BARRIER MEDIAN AND CROSS SLOPE CORRECTION (WITH THE EXCEPTION OF THE HMA SURFACE COURSE) ALONG THE EASTBOUND LANE OF COMMONWEALTH LANE.
- CONSTRUCT THE PAVEMENT ALONG THE DRIVEWAY AT STATION 302+80 RIGHT OF THE CENTERLINE ONE HALF AT A TIME. THE DRIVEWAY MUST REMAIN OPEN TO TRAFFIC AT ALL TIMES. SEE SUBSTAGE 3 PLANS FOR THE LANE CONFIGURATION TO COMPLETE DRIVEWAY CONSTRUCTION (SEE SHEET NO. 26).
- CONSTRUCT PROPOSED PAVEMENT ALONG THE ILLINOIS ROUTE 38 EXIT RAMP WHERE THE MEDIAN ISLAND HAS BEEN REMOVED.
- CONSTRUCT HMA SURFACE COURSE ALONG COMMONWEALTH LANE, BUTTERFIELD ROAD AND THE ILLINOIS ROUTE 38 EXIT RAMP.
- REMOVE EXISTING AND INSTALL PROPOSED LIGHT POLES ALONG THE SOUTH SIDE OF COMMONWEALTH LANE.
- COMPLETE LANDSCAPING ALONG THE SOUTH SIDE OF COMMONWEALTH LANE AND ALONG BUTTERFIELD ROAD.



TRAFFIC CONTROL TYPICAL SECTION - STAGE 3  
COMMONWEALTH LANE  
STATION 200+61 TO 201+90



TRAFFIC CONTROL TYPICAL SECTION - STAGE 3  
ILLINOIS ROUTE 38 EXIT RAMP  
STATION 402+42 TO 403+00



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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

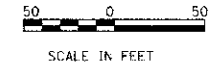
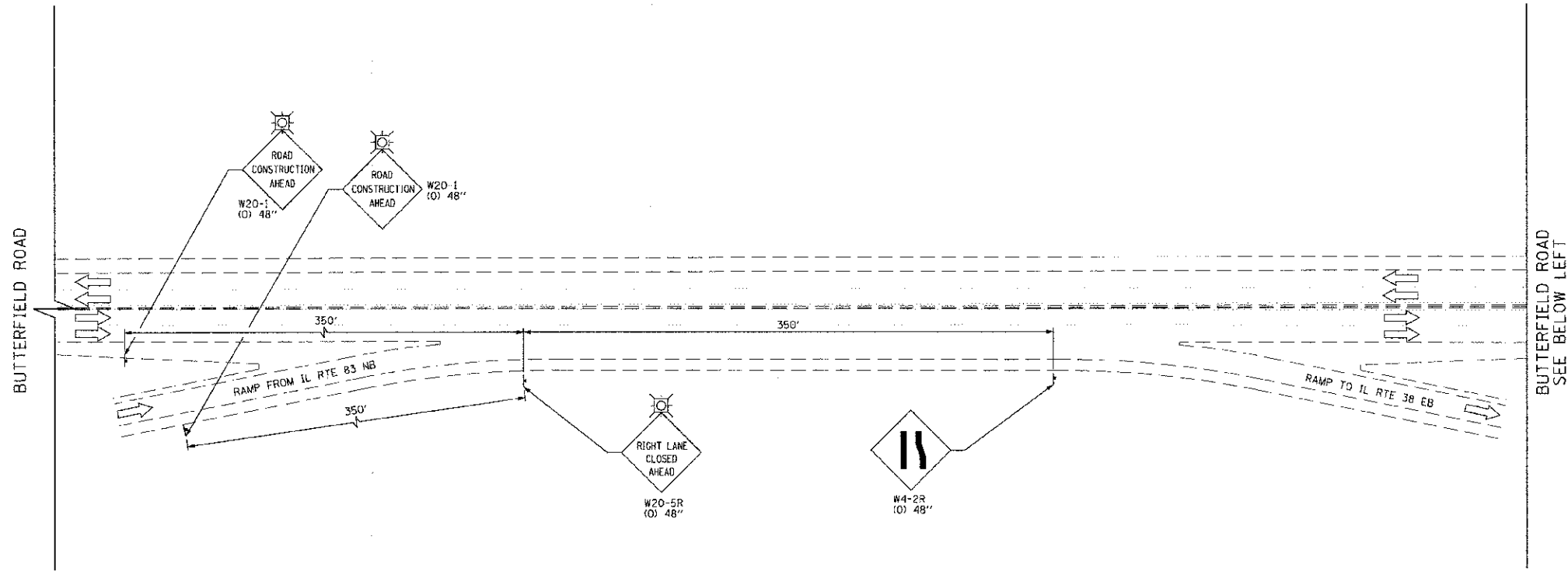
SUGGESTED CONSTRUCTION PROCEDURES AND TRAFFIC CONTROL  
TYPICAL SECTIONS AND NOTES

SCALE: NOT TO SCALE SHEET NO. 21 OF 108 SHEETS

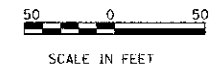
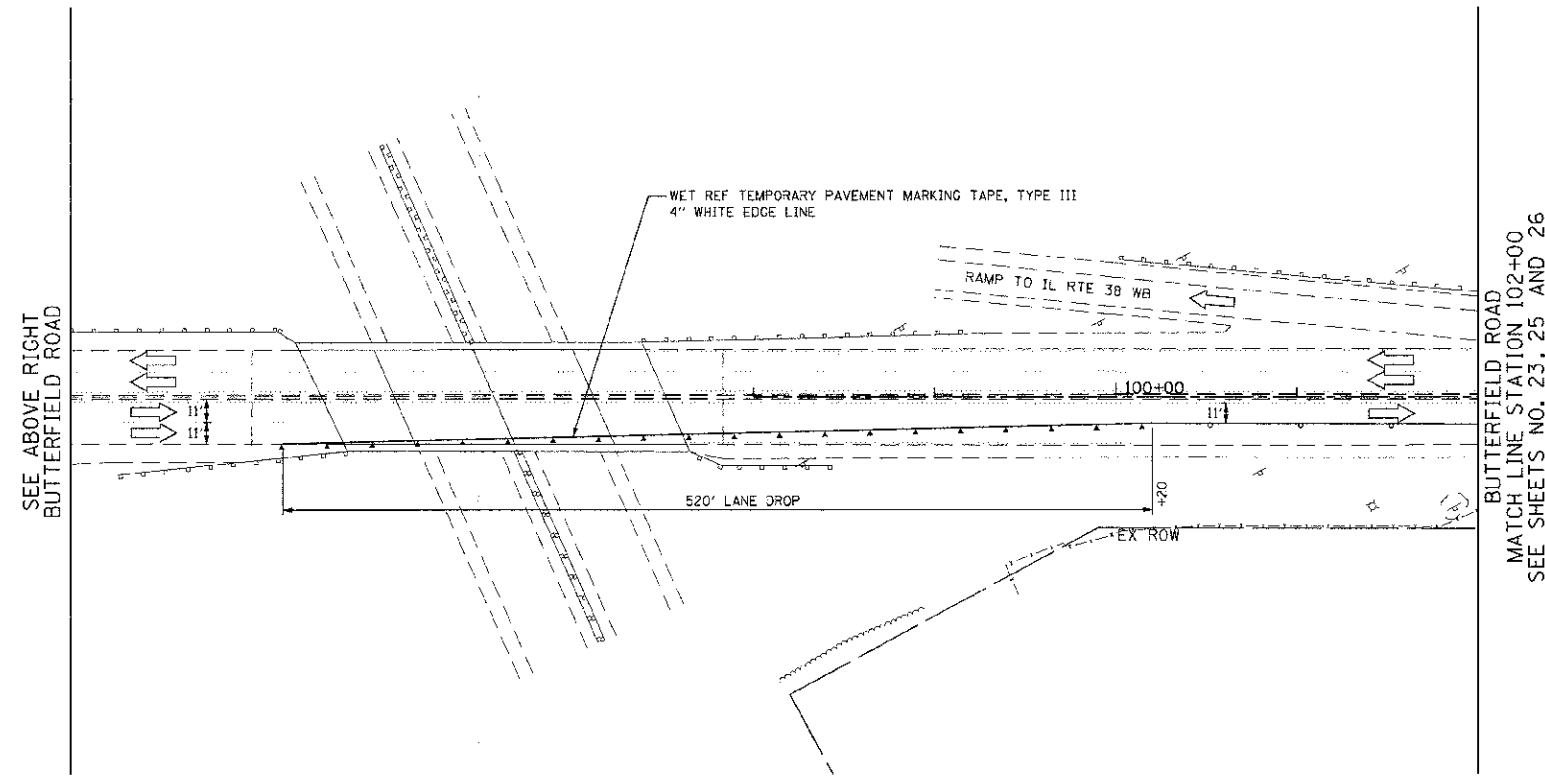
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FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63742	

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NO. OF THIS SHEET	
NO.	



LEGEND	
	CONSTRUCTION WORK ZONE
	TEMPORARY PAVEMENT TO BE CONSTRUCTED PRIOR TO STAGE 1
	ARROW BOARD
	SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
	MONODIRECTIONAL FLASHING LIGHT
	DRUMS OR TYPE II BARRICADE WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 25' C-C
	TYPE III BARRICADE
	DIRECTION INDICATOR BARRICADES WITH STEADY BURN MONO-DIRECTIONAL LIGHTS AT 25' C-C
	DIRECTION OF TRAFFIC FLOW
	TEMPORARY CONCRETE BARRIER

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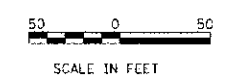
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

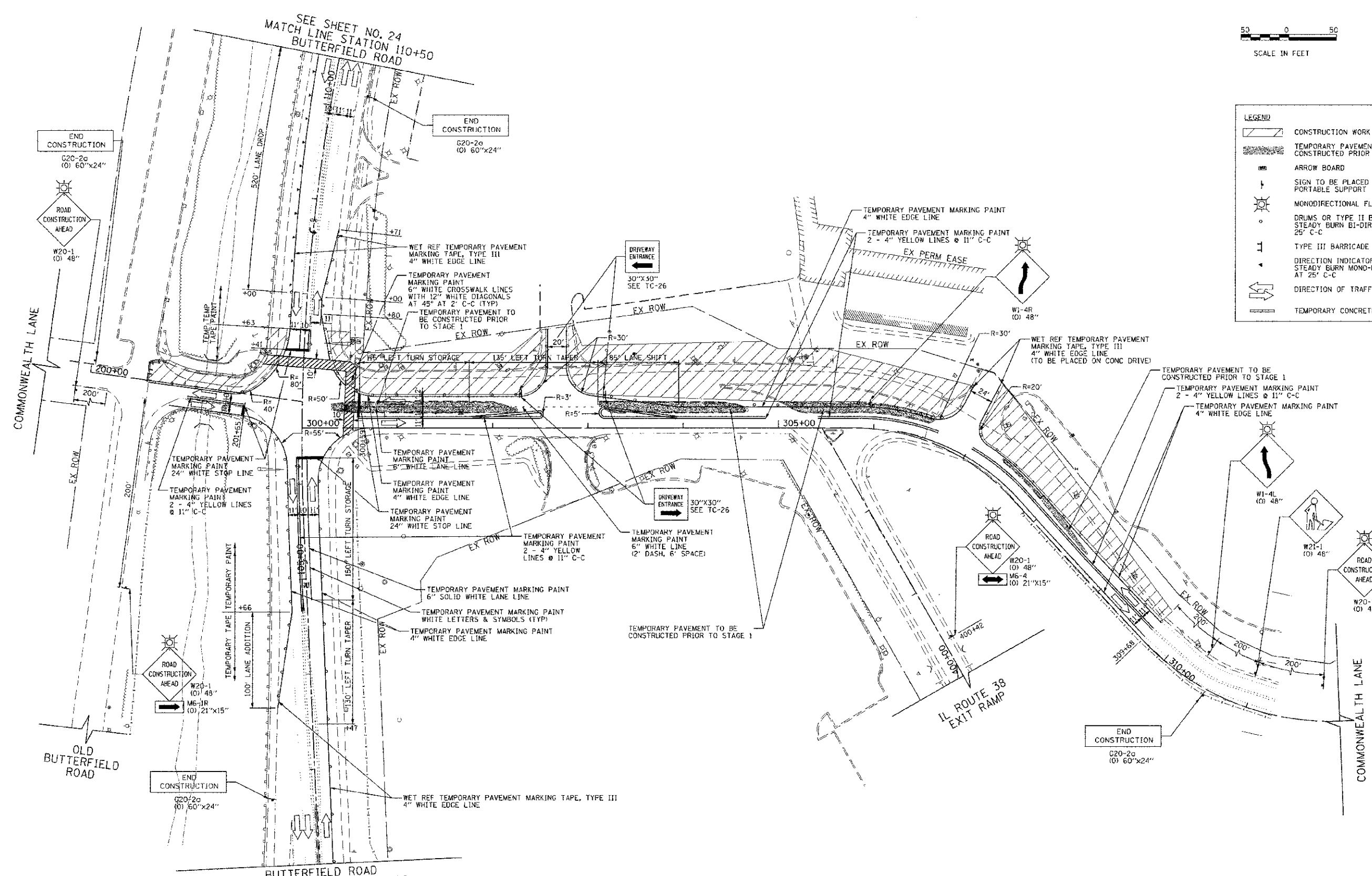
**SUGGESTED CONSTRUCTION PROCEDURES AND TRAFFIC CONTROL  
 BUTTERFIELD ROAD AT COMMONWEALTH LANE - STAGES 1, 2 & 3**

SCALE: 1"=50' SHEET NO. 22 OF 108 SHEETS STA. 84+00 TO STA. 102+00

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	09-00170-00-CH	DUPAGE	108	22
FED. ROAD DIST. NO. 7 (ILLINOIS) FED. AID PROJECT			CONTRACT NO. 63742	



LEGEND	
	CONSTRUCTION WORK ZONE
	TEMPORARY PAVEMENT TO BE CONSTRUCTED PRIOR TO STAGE 1
	ARROW BOARD
	SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
	MONODIRECTIONAL FLASHING LIGHT
	DRUMS OR TYPE II BARRICADE WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 25' C-C
	TYPE III BARRICADE
	DIRECTION INDICATOR BARRICADES WITH STEADY BURN MONO-DIRECTIONAL LIGHTS AT 25' C-C
	TEMPORARY CONCRETE BARRIER



DATE	BY	REVISIONS

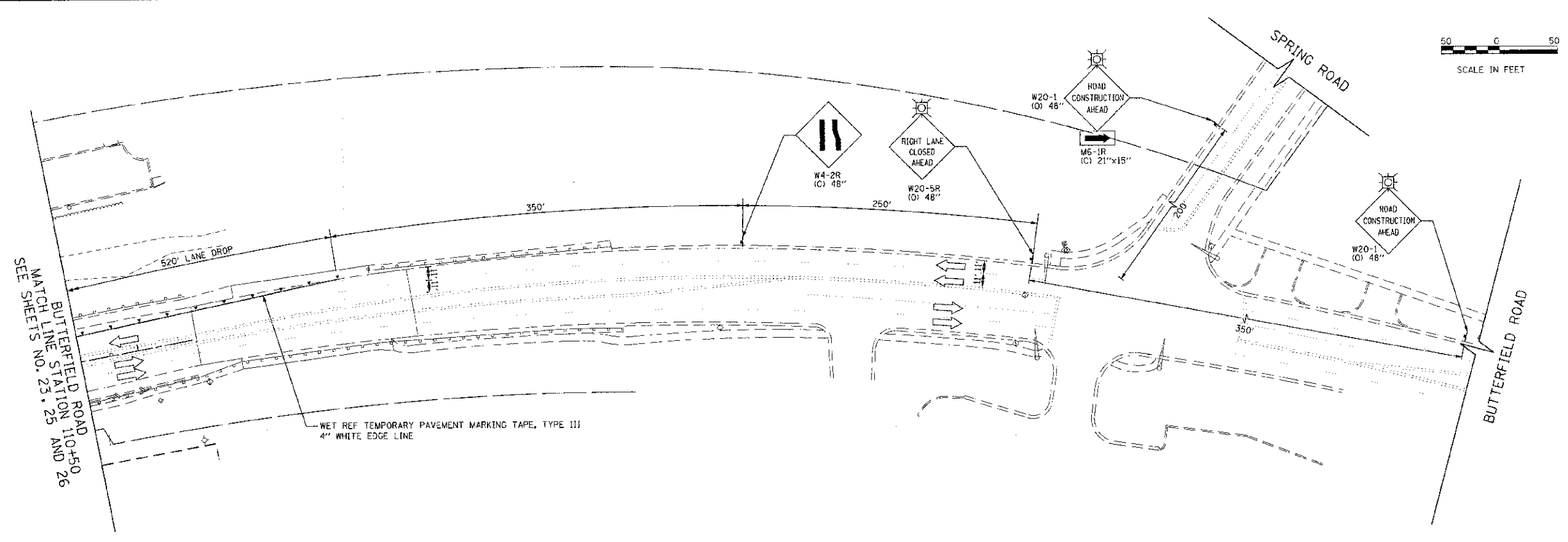
DATE	BY	REVISIONS

NOTES:  
 1. "WORKERS" SIGN (W21-1) SHALL BE REMOVED WHEN WORKERS ARE NOT PRESENT FOR MORE THAN 1 HOUR.

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		CHECKED - JMG	REVISED -			CONTRACT NO. 63742					
		DATE - 10/22/2012	REVISED -			FED. ROAD DIST. NO. 7 (ILLINOIS) FED. AID PROJECT					

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SCALE IN FEET



LEGEND	
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	TEMPORARY PAVEMENT TO BE CONSTRUCTED PRIOR TO STAGE 1
	ARROW BOARD
	SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
	MONODIRECTIONAL FLASHING LIGHT
	DRUMS OR TYPE II BARRICADE WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 25' C-C
	TYPE III BARRICADE
	DIRECTION INDICATOR BARRICADES WITH STEADY BURN MONO-DIRECTIONAL LIGHTS AT 25' C-C
	DIRECTION OF TRAFFIC FLOW
	TEMPORARY CONCRETE BARRIER

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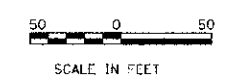
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED CONSTRUCTION PROCEDURES AND TRAFFIC CONTROL  
BUTTERFIELD ROAD AT COMMONWEALTH LANE - STAGES 1, 2 & 3**

SCALE: 1"=50'    SHEET NO. 24 OF 108 SHEETS    STA. 110+50    TO STA. 123+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEET NO.
3545	03-00170-00-CH	DUPAGE	108 24
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63742

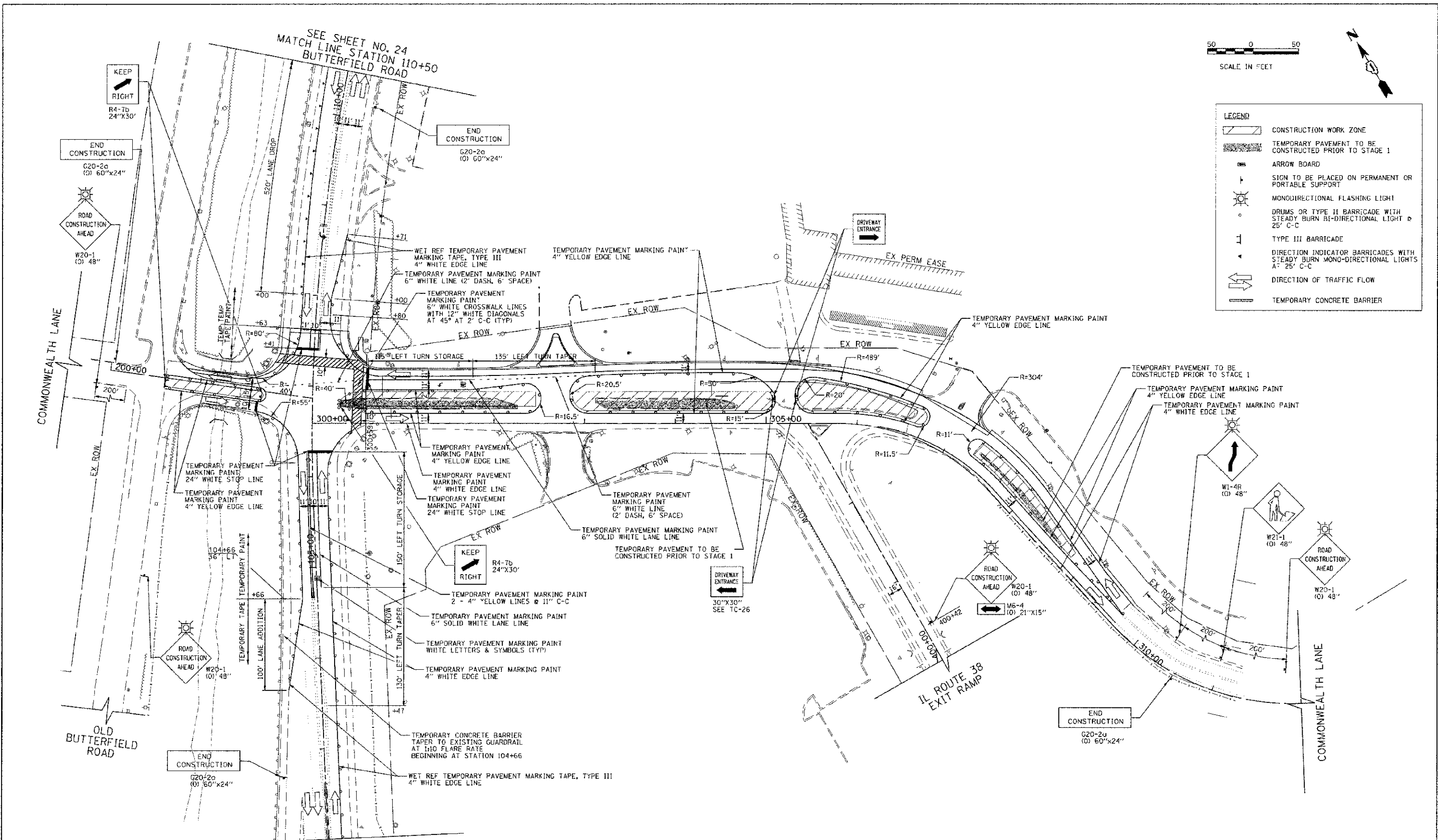




LEGEND	
	CONSTRUCTION WORK ZONE
	TEMPORARY PAVEMENT TO BE CONSTRUCTED PRIOR TO STAGE 1
	ARROW BOARD
	SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
	MONODIRECTIONAL FLASHING LIGHT
	DRUMS OR TYPE II BARRICADE WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 25' C-C
	TYPE III BARRICADE
	DIRECTION INDICATOR BARRICADES WITH STEADY BURN MONO-DIRECTIONAL LIGHTS @ 25' C-C
	DIRECTION OF TRAFFIC FLOW
	TEMPORARY CONCRETE BARRIER

DATE	BY
DATE	BY

DATE	BY
DATE	BY



**NOTES:**

- "WORKERS" SIGN (W21-1) SHALL BE REMOVED WHEN WORKERS ARE NOT PRESENT FOR MORE THAN 1 HOUR.

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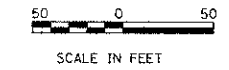
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED CONSTRUCTION PROCEDURES AND TRAFFIC CONTROL  
BUTTERFIELD ROAD AT COMMONWEALTH LANE - STAGE 2**

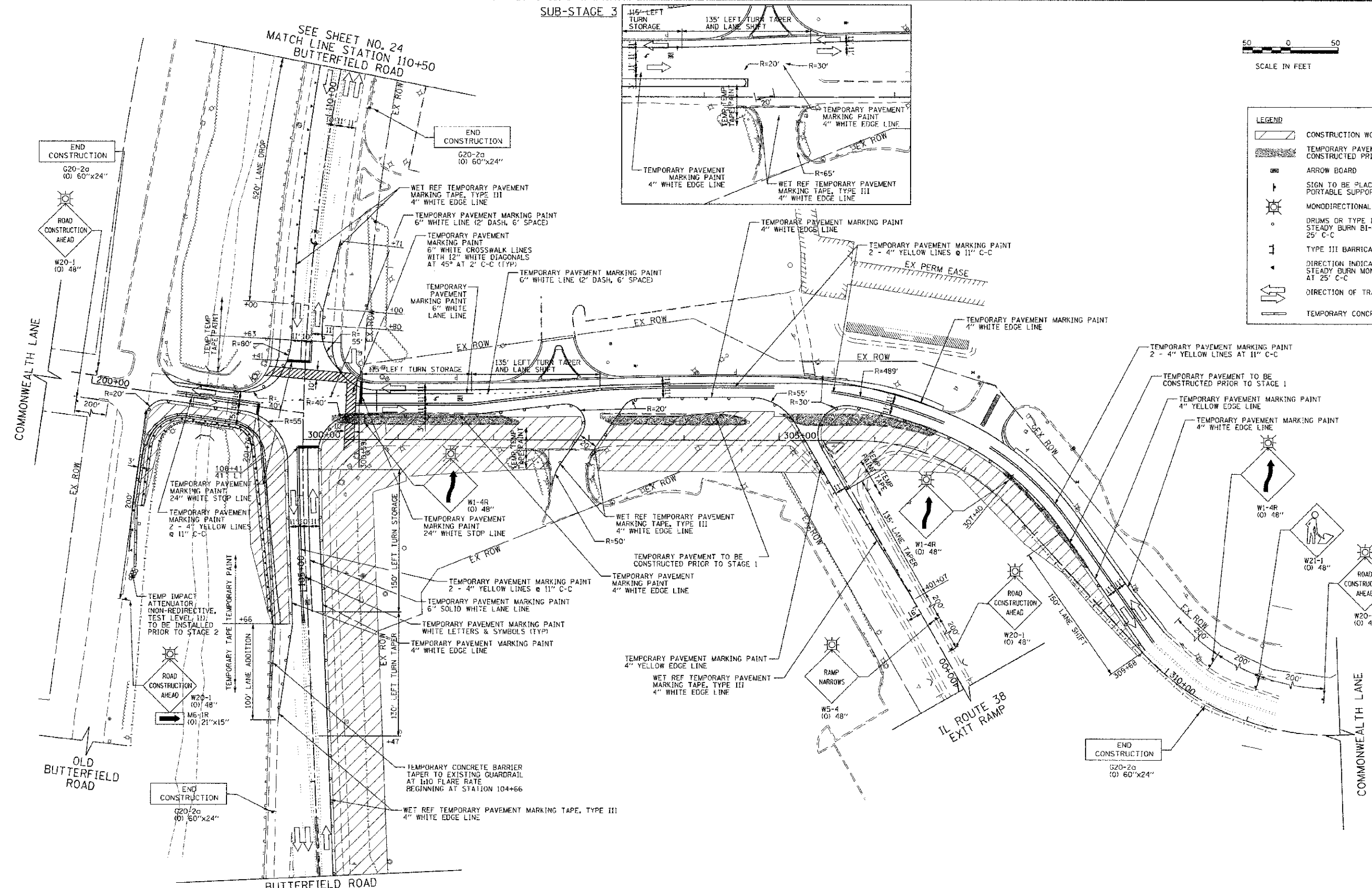
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	09-00170-00-CH	DUPAGE	108	25
FED. ROAD DIST. NO. 7 (ILLINOIS) FED. AID PROJECT			CONTRACT NO. 63742	

SUB-STAGE 3



LEGEND	
	CONSTRUCTION WORK ZONE
	TEMPORARY PAVEMENT TO BE CONSTRUCTED PRIOR TO STAGE 1
	ARROW BOARD
	SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
	MONODIRECTIONAL FLASHING LIGHT
	DRUMS OR TYPE II BARRICADE WITH STEADY BURN BI-DIRECTIONAL LIGHTS @ 25' C-C
	TYPE III BARRICADE
	DIRECTION INDICATOR BARRICADES WITH STEADY BURN MONO-DIRECTIONAL LIGHTS @ 25' C-C
	DIRECTION OF TRAFFIC FLOW
	TEMPORARY CONCRETE BARRIER

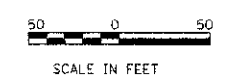


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	CHECKED	JMG
	DATE	10/22/2012

DATE	BY	REVISIONS
PROFILE	DESIGNED	TKS
	DRAWN	TKS
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	DATE	10/22/2012

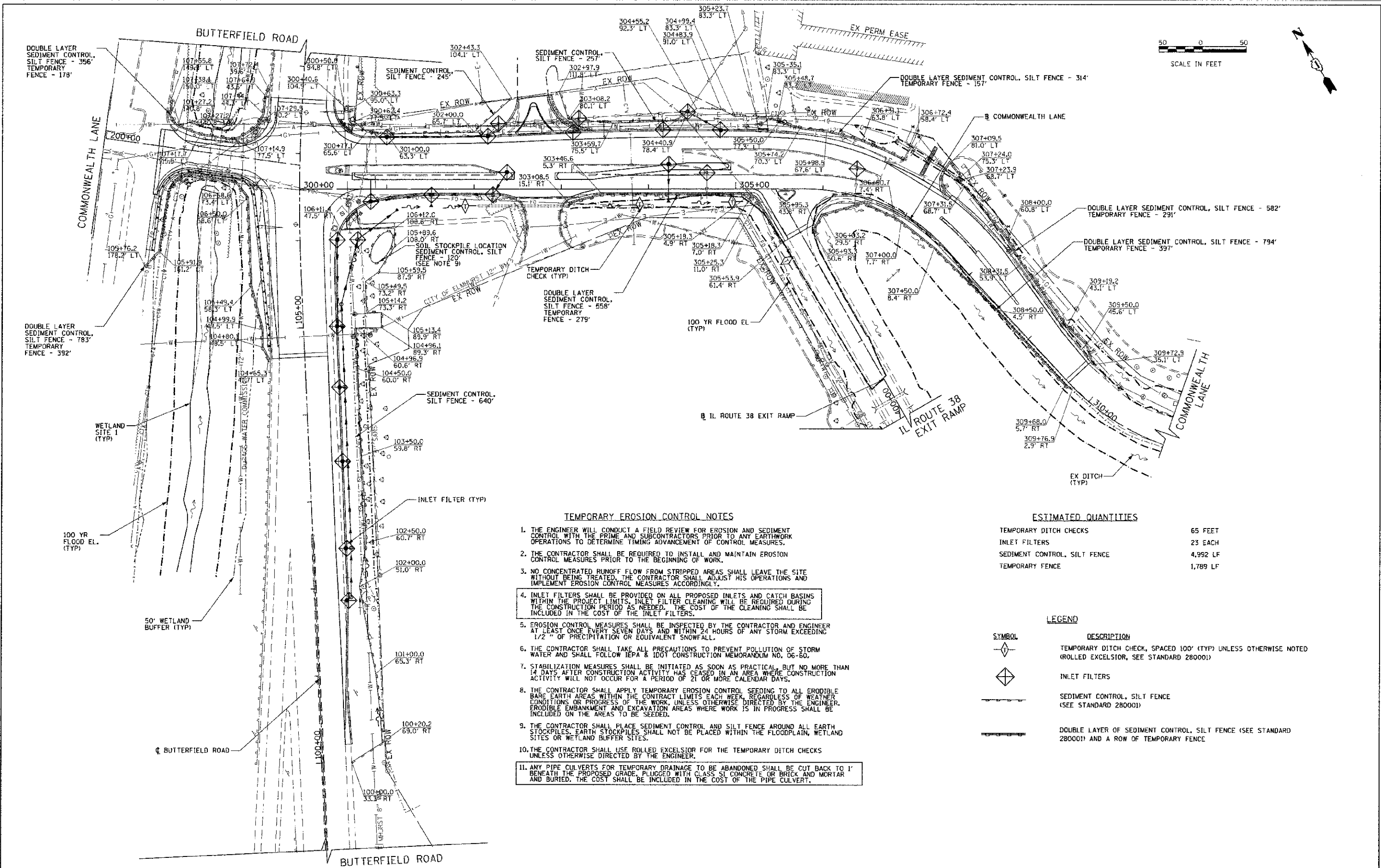
NOTES:  
 1. "WORKERS" SIGN (W21-1) SHALL BE REMOVED WHEN WORKERS ARE NOT PRESENT FOR MORE THAN 1 HOUR.

FILE NAME -	USER NAME = kcalorenz	DESIGNED - TKS	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUGGESTED CONSTRUCTION PROCEDURES AND TRAFFIC CONTROL BUTTERFIELD ROAD AT COMMONWEALTH LANE - STAGE 3</b>	F.A.J. R.T.C.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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PLOT DATE = 12/4/2012		DATE - 10/22/2012	REVISED -			CONTRACT NO. 63742 FED. ROAD DIST. NO. 7 (ILLINOIS) FED. AID PROJECT					



DATE	BY	REVISION
		1. PLOTTED
		2. CHECKED
		3. APPROVED

DATE	BY	REVISION
		1. PLOTTED
		2. CHECKED
		3. APPROVED



- TEMPORARY EROSION CONTROL NOTES**
- THE ENGINEER WILL CONDUCT A FIELD REVIEW FOR EROSION AND SEDIMENT CONTROL WITH THE PRIME AND SUBCONTRACTORS PRIOR TO ANY EARTHWORK OPERATIONS TO DETERMINE TIMING ADVANCEMENT OF CONTROL MEASURES.
  - THE CONTRACTOR SHALL BE REQUIRED TO INSTALL AND MAINTAIN EROSION CONTROL MEASURES PRIOR TO THE BEGINNING OF WORK.
  - NO CONCENTRATED RUNOFF FLOW FROM STRIPPED AREAS SHALL LEAVE THE SITE WITHOUT BEING TREATED. THE CONTRACTOR SHALL ADJUST HIS OPERATIONS AND IMPLEMENT EROSION CONTROL MEASURES ACCORDINGLY.
  - INLET FILTERS SHALL BE PROVIDED ON ALL PROPOSED INLETS AND CATCH BASINS WITHIN THE PROJECT LIMITS. INLET FILTER CLEANING WILL BE REQUIRED DURING THE CONSTRUCTION PERIOD AS NEEDED. THE COST OF THE CLEANING SHALL BE INCLUDED IN THE COST OF THE INLET FILTERS.
  - EROSION CONTROL MEASURES SHALL BE INSPECTED BY THE CONTRACTOR AND ENGINEER AT LEAST ONCE EVERY SEVEN DAYS AND WITHIN 24 HOURS OF ANY STORM EXCEEDING 1/2" OF PRECIPITATION OR EQUIVALENT SNOWFALL.
  - THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO PREVENT POLLUTION OF STORM WATER AND SHALL FOLLOW IEPA & IDOT CONSTRUCTION MEMORANDUM NO. 06-80.
  - STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICAL, BUT NO MORE THAN 14 DAYS AFTER CONSTRUCTION ACTIVITY HAS CEASED IN AN AREA WHERE CONSTRUCTION ACTIVITY WILL NOT OCCUR FOR A PERIOD OF 21 OR MORE CALENDAR DAYS.
  - THE CONTRACTOR SHALL APPLY TEMPORARY EROSION CONTROL SEEDING TO ALL ERODIBLE BARE EARTH AREAS WITHIN THE CONTRACT LIMITS EACH WEEK REGARDLESS OF WEATHER CONDITIONS OR PROGRESS OF THE WORK, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. ERODIBLE EMBANKMENT AND EXCAVATION AREAS WHERE WORK IS IN PROGRESS SHALL BE INCLUDED ON THE AREAS TO BE SEEDING.
  - THE CONTRACTOR SHALL PLACE SEDIMENT CONTROL AND SILT FENCE AROUND ALL EARTH STOCKPILES. EARTH STOCKPILES SHALL NOT BE PLACED WITHIN THE FLOODPLAIN, WETLAND SITES OR WETLAND BUFFER SITES.
  - THE CONTRACTOR SHALL USE ROLLED EXCELSTOR FOR THE TEMPORARY DITCH CHECKS UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
  - ANY PIPE CULVERTS FOR TEMPORARY DRAINAGE TO BE ABANDONED SHALL BE CUT BACK TO 1' BENEATH THE PROPOSED GRADE, PLUGGED WITH CLASS SI CONCRETE OR BRICK AND MORTAR AND BURIED. THE COST SHALL BE INCLUDED IN THE COST OF THE PIPE CULVERT.

**ESTIMATED QUANTITIES**

TEMPORARY DITCH CHECKS	65 FEET
INLET FILTERS	23 EACH
SEDIMENT CONTROL, SILT FENCE	4,992 LF
TEMPORARY FENCE	1,789 LF

**LEGEND**

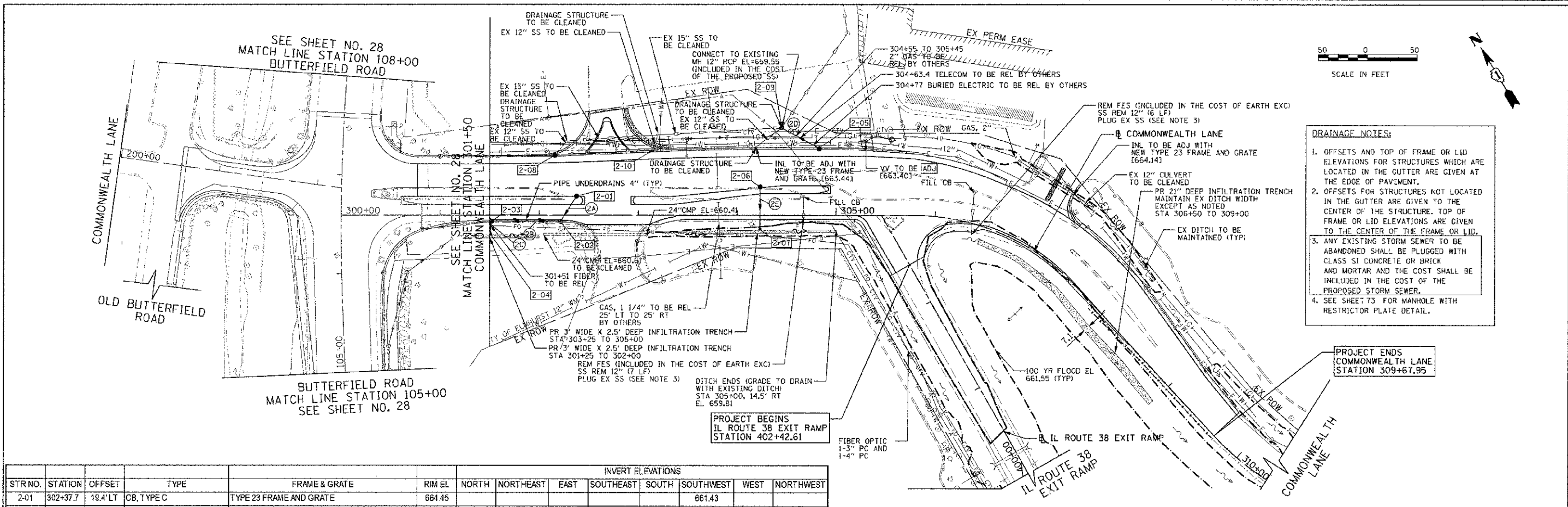
SYMBOL	DESCRIPTION
	TEMPORARY DITCH CHECK, SPACED 100' (TYP) UNLESS OTHERWISE NOTED (ROLLED EXCELSTOR, SEE STANDARD 280001)
	INLET FILTERS
	SEDIMENT CONTROL, SILT FENCE (SEE STANDARD 280001)
	DOUBLE LAYER OF SEDIMENT CONTROL, SILT FENCE (SEE STANDARD 280001) AND A ROW OF TEMPORARY FENCE





DATE	
BY	
REVISION	
NO.	
DESCRIPTION	
DATE	
BY	
REVISION	
NO.	
DESCRIPTION	
DATE	
BY	
REVISION	
NO.	
DESCRIPTION	

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DESCRIPTION	
DATE	
BY	
REVISION	
NO.	
DESCRIPTION	

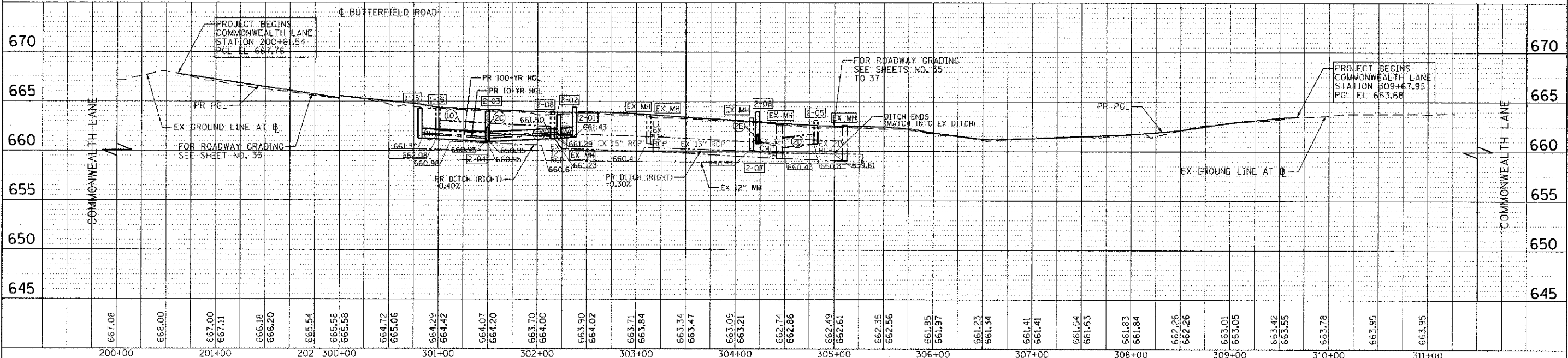


- DRAINAGE NOTES:**
1. OFFSETS AND TOP OF FRAME OR LID ELEVATIONS FOR STRUCTURES WHICH ARE LOCATED IN THE CUTTER ARE GIVEN AT THE EDGE OF PAVEMENT.
  2. OFFSETS FOR STRUCTURES NOT LOCATED IN THE CUTTER ARE GIVEN TO THE CENTER OF THE STRUCTURE. TOP OF FRAME OR LID ELEVATIONS ARE GIVEN TO THE CENTER OF THE FRAME OR LID.
  3. ANY EXISTING STORM SEWER TO BE ABANDONED SHALL BE PLUGGED WITH CLASS SI CONCRETE OR BRICK AND MORTAR AND THE COST SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.
  4. SEE SHEET 73 FOR MANHOLE WITH RESTRICTOR PLATE DETAIL.

STR NO.	STATION	OFFSET	TYPE	FRAME & GRATE	RIM EL	INVERT ELEVATIONS						
						NORTH	NORTHEAST	EAST	SOUTHEAST	SOUTH	SOUTHWEST	WEST
2-01	302+37.7	19.4' LT	CB, TYPE C	TYPE 23 FRAME AND GRATE	864.45					861.43		
2-02	302+21.6	5.0' RT	MH, TYPE A, 4'-DIAMETER	TYPE 23 FRAME AND GRATE	863.71		661.29			661.29		
2-03	301+51.0	5.0' RT	MH, TYPE A, 4'-DIAMETER	TYPE 23 FRAME AND GRATE	864.00				660.95	660.95		660.95
2-04	301+51.0	20.3' RT	PRC FES 15"	NONE						660.90		
2-05	304+83.0	66.7' LT	CB, TYPE C	TYPE 23 FRAME AND GRATE	863.23	660.81						
2-06	304+23.0	28.5' LT	CB, TYPE C	TYPE 23 FRAME AND GRATE	864.06					660.82		
2-07	304+23.0	15.4' RT	PRC FES 12"	NONE						660.40		
2-08	302+15.9	60.8' LT	CB, TYPE A, 4'-DIAMETER	TYPE 1 FRAME OPEN LID	864.04		661.23					661.50
2-09	304+44.9	88.8' LT	EX-MH	EX TYPE 8 FRAME AND GRATE	862.75		659.35		660.43	659.55	659.35	
2-10	303+13.5	64.1' LT	INLET, TYPE A	TYPE 1 FRAME OPEN LID	863.74		660.31					

PIPE NO.	STRUCTURE		DESCRIPTION	CLASS	TYPE	SIZE (IN)	LENGTH (FT)	SLOPE (%)	TBF (CU YD)
	FROM	TO							
2A	2-01	2-02	STORM SEWERS	A	1	12	27.4	0.50%	4.1
2B	2-02	2-03	STORM SEWERS	A	1	12	67.4	0.50%	10.2
2C	2-03	2-04	STORM SEWERS	A	1	15	10.0	0.50%	0.8
2D	2-05	EX-MH	STORM SEWERS	WM REQ	1	12	38.3	1.00%	1.4
2E	2-06	2-07	STORM SEWERS	A	1	12	43.0	0.50%	9.2

- LEGEND**
- STRUCTURE NUMBER
  - PIPE NUMBER
  - RIPRAP

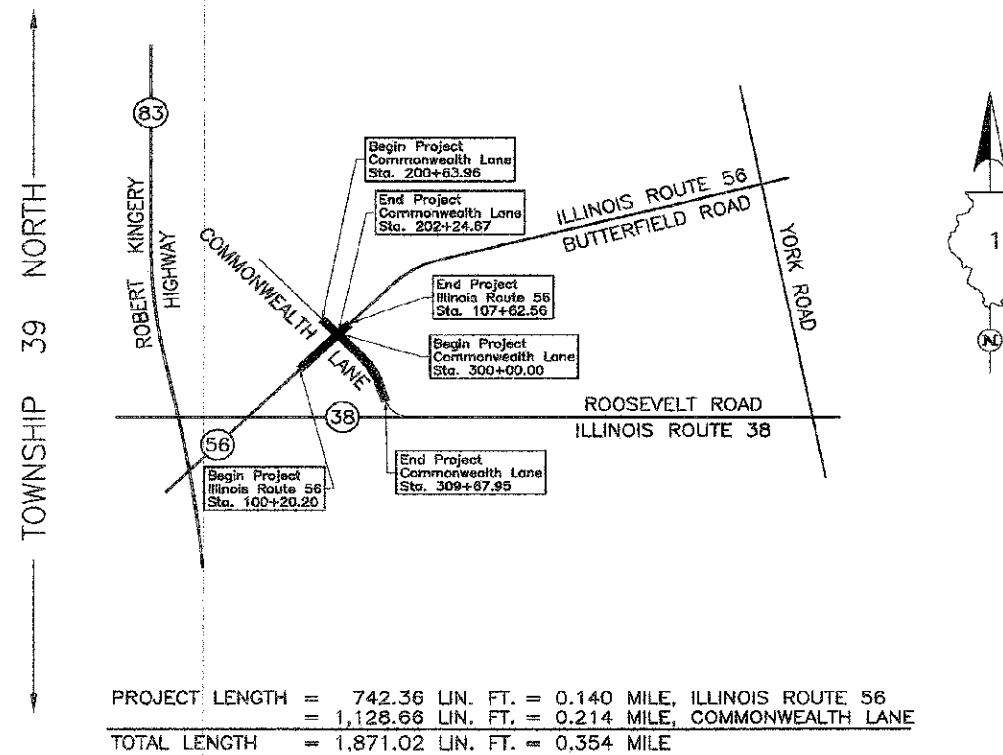


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PILOT SCALE = 50.0000 ft / in.	CHECKED = JMG	DATE = 10/22/2012	REVISED =			SCALE: HORIZ. 1"=50'	FED. ROAD DIST. NO. 7 (ILLINOIS) FED. AID PROJECT					
PILOT DATE = 12/4/2012	DATE = 10/22/2012	REVISED =	REVISED =									

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
RIGHT OF WAY PLANS  
FOR PROPOSED  
FEDERAL AID HIGHWAY

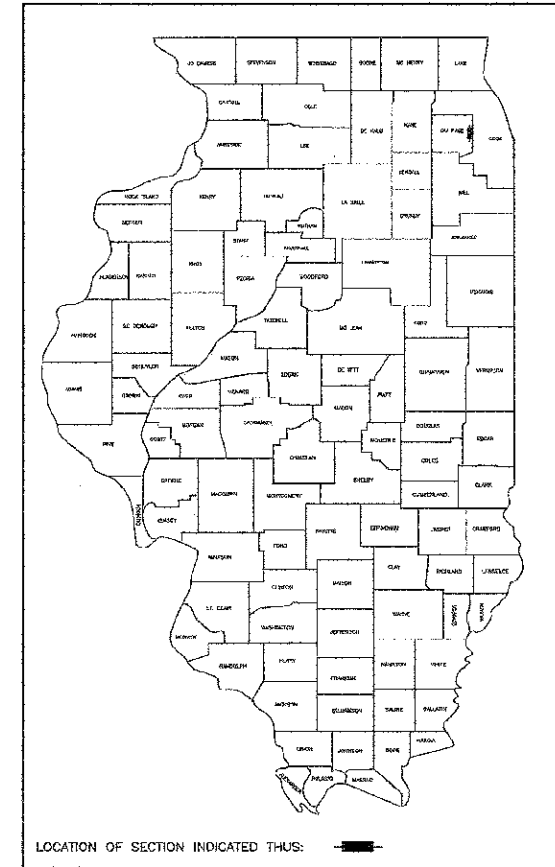
ROUTE: F.A.U. 3545 (ILLINOIS ROUTE 56)  
SECTION: 09-00170-00-CH  
PROJECT NO.:  
JOB NO.: R-91-016-12  
COUNTY: DuPAGE  
LIMITS: @ COMMONWEALTH LANE

← RANGE 11 EAST →



* F.A.U. 3545 (ILLINOIS ROUTE 56)				
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	09-00170-00-CH	DUPAGE	108	30

\*\* 09-00170-00-CH  
R-91-016-12



APPROVED \_\_\_\_\_ 20 \_\_\_\_\_  
LOCAL AGENCY OFFICIAL

APPROVED \_\_\_\_\_ 20 \_\_\_\_\_  
ENGINEER OF LAND ACQUISITION

APPROVED \_\_\_\_\_ 20 \_\_\_\_\_  
ENGINEER OF LOCAL ROADS & STREETS

APPROVED \_\_\_\_\_ 20 \_\_\_\_\_  
DISTRICT ENGINEER

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

COUNTY DuPAGE SECTION 09-00170-00-CH ROUTE F.A.U. 3545 (ILLINOIS ROUTE 56) R-91-016-12

PLAN	DATE
BY	
DATE	

FILE PATH: g:\d\p\03\00170\00-CH\09-00170-00-CH-SHT-PLAT.dgn



USER NAME = tksonid11	DESIGNED - TKS	REVISED -
FILE NAME = D:\0008-SHT-Plat.dgn	DRAWN - TKS	REVISED -
PLOT SCALE = 50.200 / IN.	CHECKED - JMC	REVISED -
PLOT DATE = 11/7/2012	DATE - 10/22/2012	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BUTTERFIELD ROAD AT COMMONWEALTH LANE PLAT OF HIGHWAYS	
SCALE: NOT TO SCALE	SHEET NO. 30 OF 108 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	09-00170-00-CH	DUPAGE	108	30

CONTRACT NO. 63742  
FED. ROAD DIST. NO. 7 (ILLINOIS) FED. AID PROJECT



PART OF THE SW 1/4 OF SEC. 14, TWP. 39 N., R. 11 E. OF THE 3RD. P.M., IN DuPAGE COUNTY, ILLINOIS.

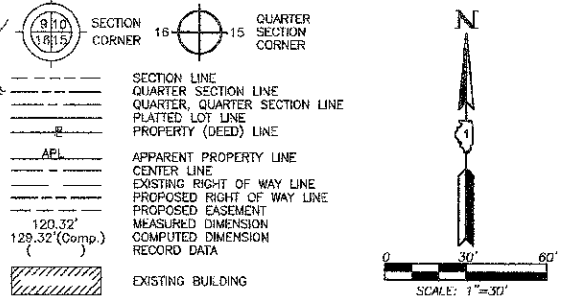
Point Number	Tie to point	Tie Distance (feet)
1	T1	55.75
	T2	52.07
	T3	49.33
2	T1	43.49
	T2	37.84
	T3	41.32
3	T1	33.48
	T2	23.19
	T3	24.51
4	T1	24.62
	T2	13.99
	T3	19.60
5	T1	24.32
	T2	20.45
	T3	21.95
6	T1	18.17
	T2	21.25
	T3	27.60

T1 to Point Nos. 3 & 4 is a Cross Cut (Set) in Concrete Walk.  
T2 & T3 to Point Nos. 3 & 4 are Cross Cuts (Set) in B-6.12 Concrete Curb & Gutter.

**THE BUTTERFIELD EXCHANGE ASSESSMENT PLAT**  
Recorded September 14, 1989  
as Document No. R89-114438

**ASTORIA PLACE TOWNHOMES PLANNED UNIT DEVELOPMENT**  
Recorded January 20, 1995  
as Document No. R95-008413

**LEGEND**



- IRON PIPE OR ROD FOUND
- ⊕ "MAG" NAIL SET
- + CUT CROSS FOUND OR SET
- 5/8" REBAR SET
- T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION, SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- T2 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION, BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- T3 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION, BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY, SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS, BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- PERMANENT SURVEY MARKER, I.D.C.T STD 2135 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS }  
COUNTY OF LAKE }  
THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 14, TOWNSHIP 39N., RANGE 11E., OF THE THIRD PRINCIPAL MERIDIAN, DU PAGE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS, DATED AT LAKE VILLA, ILLINOIS THIS 22nd DAY OF December 20, 2012.



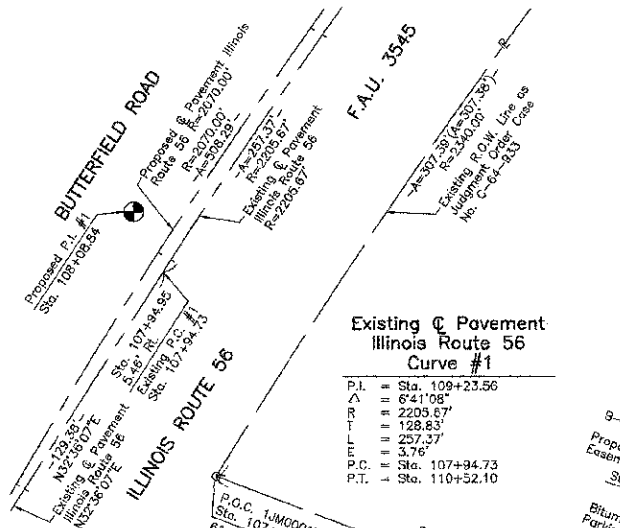
CHRISTIAN R. JORGENSEN, PRESIDENT  
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 36-2797  
LICENSE EXPIRATION DATE NOVEMBER 30, 2012  
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.  
Note: Surface Coordinates are Shown

STATION	OFFSET	NORTH	EAST
1071+40.80	81.89' Rt.	1,893,106.840	1,088,176.619
107+94.95	5.46' Rt.	1,893,181.750	1,088,156.907
302+37.76	143.71' Lt.	1,893,080.055	1,088,360.170
302+43.32	104.13' Lt.	1,893,042.846	1,088,346.036
302+97.17	152.08' Lt.	1,893,058.863	1,088,416.300
303+02.73	112.47' Lt.	1,893,021.444	1,088,402.165
303+97.24	32.85' Rt.	1,892,848.596	1,088,415.322

JORGENSEN & ASSOCIATES, INC.  
120 PARK AVENUE  
LAKE VILLA, ILLINOIS 60046  
(847) 356-3371

**PLAT OF HIGHWAYS**  
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
F.A.U. 3545 (ILLINOIS ROUTE 56)  
SECTION 09-00170-00-CH DuPAGE COUNTY  
PROJECT JOB NO. R-91-016-12  
STATION 107+00 TO STATION 109+00  
STATION 300+00 TO STATION 307+00  
SCALE: 1"=30'  
SHEET 2 OF 5

**BUREAU OF LAND ACQUISITION**  
201 WEST CENTER COURT  
SCHAUMBURG, ILLINOIS 60196



**Existing & Pavement Illinois Route 56 Curve #1**  
P.I. = Sta. 109+23.56  
Δ = 6°41'08"  
R = 2205.67'  
T = 128.83'  
L = 257.37'  
E = 3.76'  
P.C. = Sta. 107+94.73  
P.T. = Sta. 110+52.10

**Proposed & Pavement Illinois Route 56 Curve #1**  
P.I. = Sta. 108+08.84  
Δ = 14°04'09"  
R = 2070.00'  
T = 255.43'  
L = 508.29'  
E = 15.70'  
P.C. = Sta. 105+53.41  
P.T. = Sta. 110+61.70

STATION	OFFSET	NORTH	EAST
304+09.72	127.48' Lt.	1,892,983.270	1,088,503.226
304+70.24	183.35' Lt.	1,893,003.226	1,088,583.138
304+83.88	90.96' Lt.	1,892,915.634	1,088,550.764
304+97.18	187.50' Lt.	1,892,993.944	1,088,608.762
304+97.90	154.39' Lt.	1,892,964.549	1,088,593.489
304+99.44	85.30' Lt.	1,892,901.449	1,088,560.735
305+05.06	33.70' Rt.	1,892,796.099	1,088,509.509
305+22.89	154.93' Lt.	1,892,853.029	1,088,615.686
305+32.12	153.51' Lt.	1,892,947.357	1,088,623.101
305+34.49	193.26' Lt.	1,892,981.086	1,088,644.253
305+40.51	93.25' Lt.	1,892,850.458	1,088,601.538
305+46.95	83.24' Lt.	1,892,878.585	1,088,602.385
305+50.73	83.23' Lt.	1,892,876.766	1,088,605.697
305+54.84	83.23' Lt.	1,892,874.790	1,088,609.299
305+55.59	29.14' Lt.	1,892,826.931	1,088,584.095
306+25.21	89.93' Lt.	1,892,832.127	1,088,687.019
306+69.56	90.37' Lt.	1,892,787.598	1,088,730.196
306+87.11	93.97' Lt.	1,892,769.660	1,088,747.588

**LEXINGTON SQUARE OF ELMHURST**  
Recorded November 2, 1989  
as Document No. R89-138836

Parcel	Document No.	Date Recorded
1JM0001	C-64-933	*November 30, 1986
1JM0002	C-64-933	*November 30, 1986
1JM0002	R89-021280	February 24, 1989
---	R89-138836	November 2, 1989

PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
1JM0001E-A 1JM0001E-B	360 Butterfield, LLC, an Illinois limited liability company	4.008	N/A	N/A	4.006	A=0.055 B=0.048	Driveway Construction Construction Purposes	06-14-318-010 06-14-318-011	
1JM0002P.E. 1JM0002T.E.	Astoria Place Townhome Owner Association, an Illinois not-for-profit association	6.631	N/A	N/A	6.631	F.E. 0.063 T.E. = 0.046	Highway Purposes Construction Purposes	06-14-320-091(p1)	

REVISION DATE November 3, 2012  
REVISION Changed Parcel 1JM0002 to 1JM0002P.E.  
MADE BY

ROUTE F.A.U. 3545 (ILLINOIS ROUTE 56) SECTION 09-00170-00-CH COUNTY DuPAGE JOB NO. R-91-016-12 RECORDING: RECORDED ON AS DOCUMENT NO.



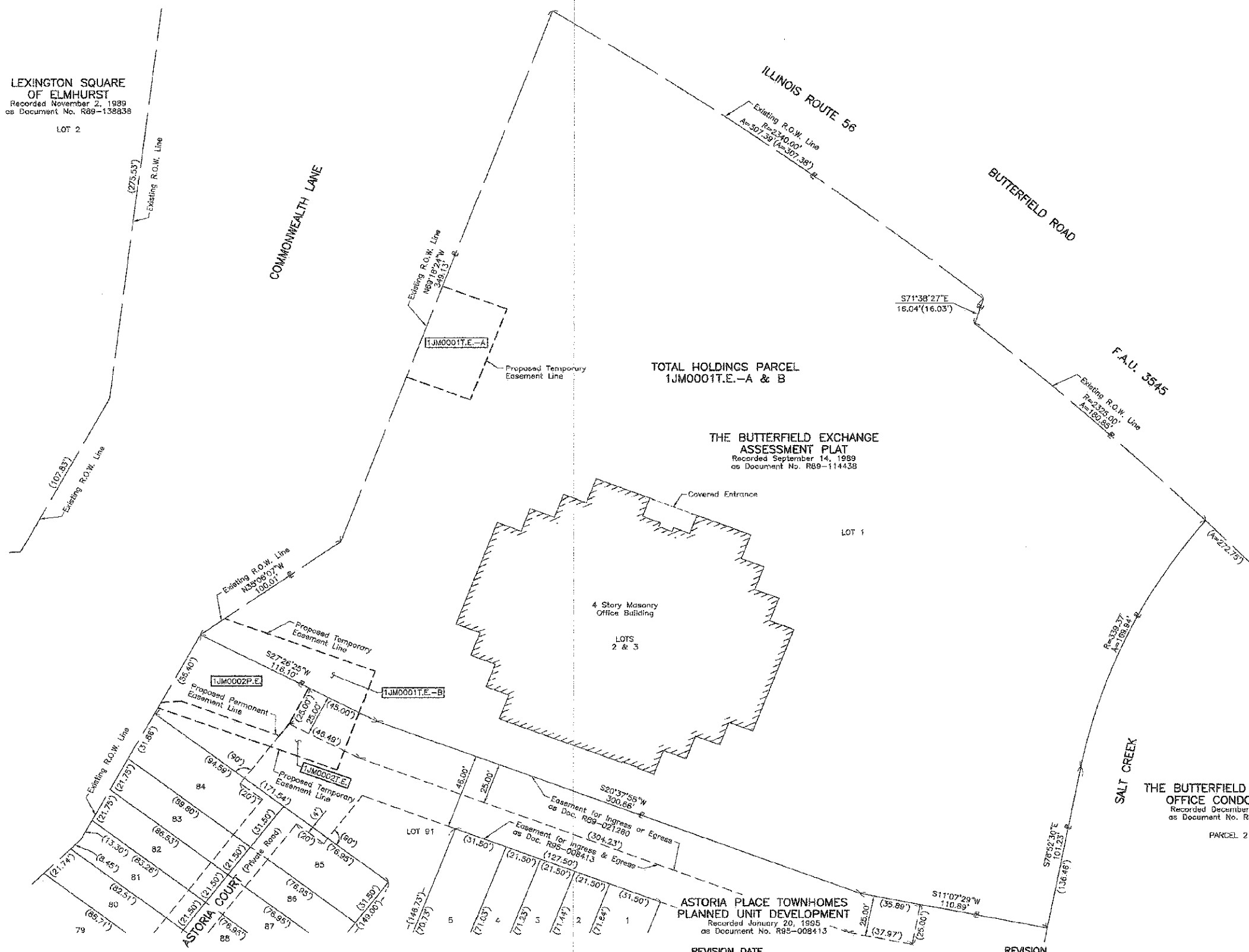
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kalorenz	TKS	-
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PLAT SCALE = 5/8"=1'-0"	CHECKED	JMG
PLAT DATE = 12/24/2012	DATE	10/22/2012
	REVISION	-

**STATE OF ILLINOIS**  
DEPARTMENT OF TRANSPORTATION

**BUTTERFIELD ROAD AT COMMONWEALTH LANE**  
PLAT OF HIGHWAYS  
SCALE: NOT TO SCALE; SHEET NO. 31 OF 108 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	09-00170-00-CH	DUPAGE	108	31
FED. ROAD DIST. NO. 7 ILLINOISIFIED AID PROJECT			CONTRACT NO. 63742	

PART OF THE SW 1/4 OF SEC. 14, TWP. 39 N., R. 11 E. OF THE 3RD. P.M., IN DuPAGE COUNTY, ILLINOIS.



**LEGEND**

- SECTION CORNER
  - QUARTER SECTION CORNER
  - SECTION LINE
  - QUARTER SECTION LINE
  - PLATTED LOT LINE
  - PROPERTY (DEED) LINE
  - APPARENT PROPERTY LINE
  - CENTER LINE
  - EXISTING RIGHT OF WAY LINE
  - PROPOSED RIGHT OF WAY LINE
  - PROPOSED EASEMENT
  - MEASURED DIMENSION
  - COMPUTED DIMENSION
  - RECORD DATA
  - EXISTING BUILDING
- Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.
- IRON PIPE OR ROD FOUND
  - CUI CROSS FOUND ON SET
  - "MAG" NAIL SET
  - 5/8" REBAR SET
  - T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - M STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
  - M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)
  - RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS }  
 COUNTY OF LAKE }  
 THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 14, TOWNSHIP 39N., RANGE 11E., OF THE THIRD PRINCIPAL MERIDIAN, DU PAGE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS. DATED AT LAKE VILLA, ILLINOIS THIS 20th DAY OF December 2012 A.D.

*Christian H. Jorgensen* - PRESIDENT  
 ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797  
 LICENSE EXPIRATION DATE: NOVEMBER 30, 2012  
 THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.



JORGENSEN & ASSOCIATES, INC.  
 120 PARK AVENUE  
 LAKE VILLA, ILLINOIS 60046  
 (847) 356-3371

**PLAT OF HIGHWAYS**  
 STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
 F.A.U. 3545 (ILLINOIS ROUTE 56)  
 SECTION 09-00170-00-CH DuPAGE COUNTY  
 PROJECT JOB NO. R-91-016-12  
 STATION NONE TO STATION  
 SCALE: 1"=30'  
 SHEET 3 OF 6

**BUREAU OF LAND ACQUISITION**  
 201 WEST CENTER COURT  
 SCHAUMBURG, ILLINOIS 60196

DATE	BY

PLAN  
 SURVEYED  
 ADJUSTED  
 PLATTED  
 CHECKED  
 BY: J.M.C.  
 DATE: 11/17/2012

ROUTE F.A.U. 3545 (ILLINOIS ROUTE 56) SECTION 09-00170-00-CH COUNTY DuPAGE JOB NO. R-91-016-12 RECORDING: RECORDED ON AS DOCUMENT NO.

	USER NAME = tksandall	DESIGNED - TKS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTTERFIELD ROAD AT COMMONWEALTH LANE PLAT OF HIGHWAYS	F.A.U. R.T.E. = 3545	SECTION = 09-00170-00-CH	COUNTY = DUPAGE	TOTAL SHEETS = 108	SHEET NO. = 32
	FILE NAME = C:\J\2008-SHT-Plat3.dgn	DRAWN - TKS	REVISED -			SCALE: NOT TO SCALE SHEET NO. 32 OF 108 SHEETS	FED. ROAD DIST. NO. 7 (ILLINOIS) FED. AID PROJECT			
	PLOT SCALE = 50.0000 1" = 50.0000'	CHECKED - JMG	REVISED -						CONTRACT NO. 63742	
	PLSI DATE = 11/17/2012	DATE = 10/22/2012	REVISED -							

PART OF THE SOUTH 1/2 OF SEC. 14, TWP. 39 N., R. 11 E. OF THE 3RD. P.M., IN DuPAGE COUNTY, ILLINOIS.

LEXINGTON SQUARE  
OF ELMHURST  
Recorded November 2, 1959  
as Document No. R89-136B36

LOT 2  
(528.12')

Existing R.O.W. Line

COMMONWEALTH LANE

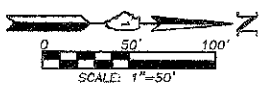
THE BUTTERFIELD EXCHANGE  
ASSESSMENT PLAT  
Recorded September 14, 1989  
as Document No. R89-114438

LOT 1

THE BUTTERFIELD EXCHANGE  
OFFICE CONDOMINIUM  
Recorded December 4, 1984  
as Document No. R84-97595

LEGEND

- SECTION CORNER 16
- QUARTER SECTION CORNER
- SECTION LINE
- QUARTER SECTION LINE
- QUARTER, QUARTER SECTION LINE
- PLATTED LOT LINE
- PROPERTY (DEED) LINE
- APPARENT PROPERTY LINE
- CENTER LINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED EASEMENT
- MEASURED DIMENSION
- COMPUTED DIMENSION
- RECORD DATA
- EXISTING BUILDING



Bearings and Coordinates are referenced to the Illinois  
Coordinate System NAD 83(2007) East Zone.

- O IRON PIPE OR ROD FOUND
- ⊕ "MAG" NAIL SET
- + CUT CROSS FOUND OR SET
- 5/8" REBAR SET
- ⊙ T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ⊙ T2
- ⊙ T3
- ⊙ BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ⊙ BT2
- ⊙ BT3
- ⊙ STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- ⊙ M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ⊙ PERMANENT SURVEY MARKER, I.D.O.T ST 2135 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS }  
COUNTY OF LAKE }  
THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 14, TOWNSHIP 39N., RANGE 11E., OF THE THIRD PRINCIPAL MERIDIAN, DuPAGE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS. DATED AT LAKE VILLA, ILLINOIS THIS 20th DAY OF December 20, 2012.



Christian H. Jorgensen, PRESIDENT  
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2787  
LICENSE EXPIRATION DATE: NOVEMBER 30, 2012  
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

JORGENSEN & ASSOCIATES, INC.  
120 PARK AVENUE  
LAKE VILLA, ILLINOIS 60048  
(847) 356-3371

PLAT OF HIGHWAYS  
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
F.A.U. 3545 (ILLINOIS ROUTE 56)  
SECTION 09-00170-00-CH DuPAGE COUNTY  
PROJECT JOB NO. R-91-016-12  
STATION NONE TO STATION  
SCALE: 1"=50' SHEET 4 OF 5

BUREAU OF LAND ACQUISITION  
201 WEST CENTER COURT  
SCHAMBURG, ILLINOIS 60196

ROOSEVELT ROAD  
ILLINOIS ROUTE 38

Existing R.O.W. Line

Existing R.O.W. Line

Existing R.O.W. Line

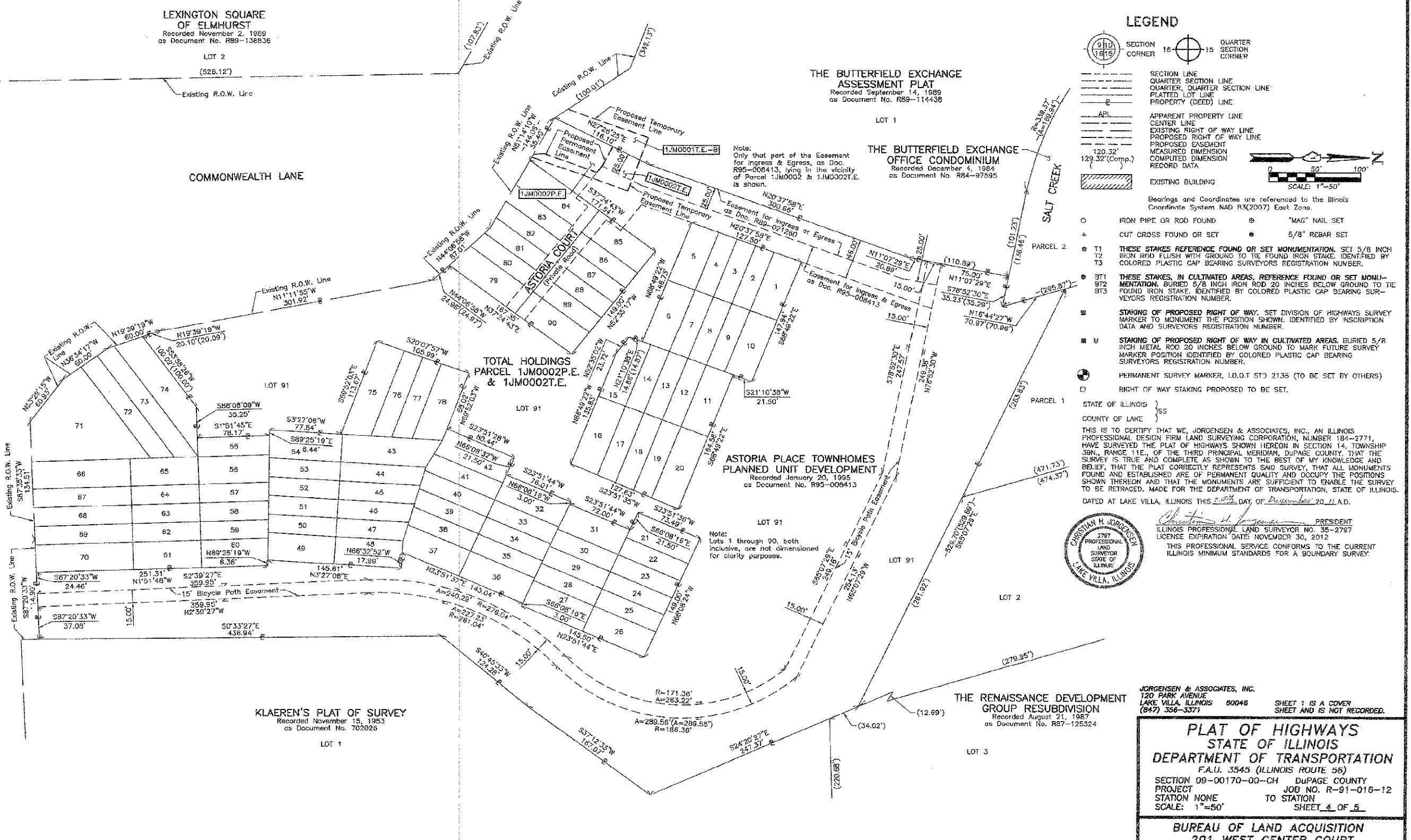
Existing R.O.W. Line

Existing R.O.W. Line

Existing R.O.W. Line

Existing R.O.W. Line

Existing R.O.W. Line



TOTAL HOLDINGS  
PARCEL 1JM0002P.E.  
& 1JM0002T.E.

ASTORIA PLACE TOWNHOMES  
PLANNED UNIT DEVELOPMENT  
Recorded January 20, 1995  
as Document No. R95-006413

KLAEREN'S PLAT OF SURVEY  
Recorded November 15, 1953  
as Document No. 702026

THE RENAISSANCE DEVELOPMENT  
GROUP RESUBDIVISION  
Recorded August 21, 1987  
as Document No. R87-125324

REVISION DATE November 3, 2012

REVISION Changed Parcel 1JM0002 to 1JM0002P.E.

MADE BY

ROUTE F.A.U. 3545 (ILLINOIS ROUTE 56)

SECTION 09-00170-00-CH

COUNTY DuPAGE

JOB NO. R-91-016-12

RECORDING: RECORDED ON

AS DOCUMENT NO.



USER NAME = tksunde1	DESIGNED - TKS	REVISED -
FILE NAME = C:\X0208-SHT-Plat4.dgn	DRAWN - TKS	REVISED -
PLOT SCALE = 50/000' / IN.	CHECKED - JMG	REVISED -
PLOT DATE = 11/7/2012	DATE - 10/22/2012	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

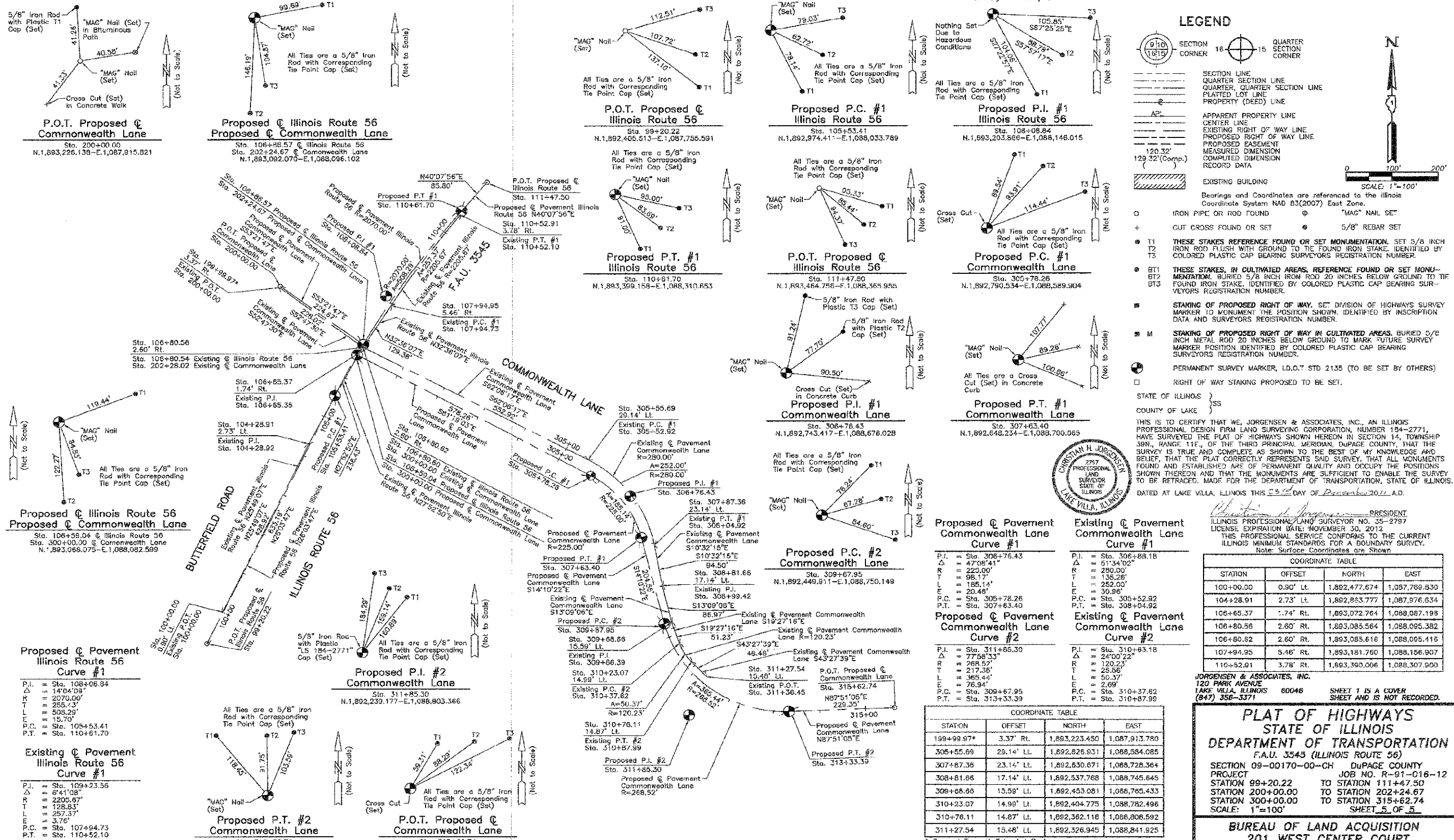
BUTTERFIELD ROAD AT COMMONWEALTH LANE  
PLAT OF HIGHWAYS

SCALE: NOT TO SCALE SHEET NO. 33 OF 108 SHEETS

F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	09-00170-00-CH	DUPAGE	108	33
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63742	

FILE PATH: g:\CH09\UR\Res\Road\Shore\14\DX\1008P-SHT-4.dwg

PART OF THE SOUTH 1/2 OF SEC. 14, TWP. 39 N., R. 11 E. OF THE 3RD. P.M., IN DuPAGE COUNTY, ILLINOIS.



**LEGEND**

- SECTION CORNER 16 QUARTER SECTION CORNER
- SECTION LINE
- QUARTER SECTION LINE
- QUARTER SECTION LINE
- PLATTED LOT LINE
- PROPERTY (DEED) LINE
- APPARENT PROPERTY LINE
- CENTER LINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED EASEMENT
- MEASURED DIMENSION
- COMPUTED DIMENSION
- RECORD DATA
- EXISTING BUILDING
- Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.
- IRON PIPE OR ROD FOUND
- ⊙ "MAG" NAIL SET
- + CUT CROSS FOUND OR SET
- ⊙ 5/8" REBAR SET
- T1 THESE STAKES REFERENCED FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- T2
- T3
- BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCED FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- BT2
- BT3
- STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET.



THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 154-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 14, TOWNSHIP 39N., RANGE 11E., OF THE THIRD PRINCIPAL MERIDIAN, DU PAGE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS. DATED AT LAKE VILLA, ILLINOIS THIS 23rd DAY OF December 2011 A.D.

ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797  
LICENSE EXPIRATION DATE: NOVEMBER 30, 2012  
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.  
Note: Surface Coordinates are Shown

COORDINATE TABLE			
STATION	OFFSET	NORTH	EAST
100+00.00	0.90' Lt.	1,892,477.674	1,087,769.530
104+28.91	2.73' Lt.	1,892,963.777	1,087,976.634
106+65.37	1.74' Rt.	1,893,072.764	1,088,087.198
106+80.56	2.60' Rt.	1,893,085.564	1,088,095.382
106+80.52	2.60' Rt.	1,893,085.616	1,088,095.415
107+94.95	5.46' Rt.	1,893,181.760	1,088,156.907
110+52.01	3.78' Rt.	1,893,390.006	1,088,307.900

Proposed Pavement Commonwealth Lane Curve #1		Existing Pavement Commonwealth Lane Curve #1	
P.I. = Sta. 306+76.43	P.I. = Sta. 306+88.18		
Δ = 47°34'14"	Δ = 51°34'02"		
R = 225.00'	R = 280.00'		
T = 98.17'	T = 136.28'		
L = 185.14'	L = 252.00'		
E = 20.48'	E = 30.96'		
P.C. = Sta. 305+78.28	P.C. = Sta. 305+52.92		
P.T. = Sta. 307+63.40	P.T. = Sta. 308+04.92		

Proposed Pavement Commonwealth Lane Curve #2		Existing Pavement Commonwealth Lane Curve #2	
P.I. = Sta. 310+65.30	P.I. = Sta. 310+63.18		
Δ = 77°58'33"	Δ = 24°00'22"		
R = 268.57'	R = 120.23'		
T = 217.35'	T = 25.56'		
L = 365.44'	L = 50.37'		
E = 76.94'	E = 2.69'		
P.C. = Sta. 309+67.95	P.C. = Sta. 310+37.82		
P.T. = Sta. 313+33.39	P.T. = Sta. 310+87.99		

COORDINATE TABLE			
STATION	OFFSET	NORTH	EAST
199+99.97*	3.37' Rt.	1,893,223.450	1,087,913.780
305+55.69	29.14' Lt.	1,892,826.931	1,088,584.085
307+87.36	23.14' Lt.	1,892,630.671	1,088,728.364
308+81.66	17.14' Lt.	1,892,537.768	1,088,745.845
309+66.66	13.59' Lt.	1,892,453.081	1,088,765.433
310+23.07	14.99' Lt.	1,892,404.775	1,088,782.496
310+76.11	14.87' Lt.	1,892,362.118	1,088,806.592
311+27.54	15.48' Lt.	1,892,326.945	1,088,841.925

JORGENSEN & ASSOCIATES, INC.  
120 PARK AVENUE  
LAKE VILLA, ILLINOIS 60046  
(847) 358-3371

**PLAT OF HIGHWAYS**  
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
F.A.U. 3545 (ILLINOIS ROUTE 56)

SECTION 09-00170-00-CH DuPAGE COUNTY  
PROJECT JOB NO. R-91-016-12  
STATION 99+20.22 TO STATION 111+47.50  
STATION 200+00.00 TO STATION 202+24.67  
STATION 300+00.00 TO STATION 315+62.74  
SCALE: 1"=100' SHEET 5 OF 5

**BUREAU OF LAND ACQUISITION**  
201 WEST CENTER COURT  
SCHAUMBURG, ILLINOIS 60196

DATE	BY
NO. 1 BOOK	NO. 1 BOOK
NO. 2 BOOK	NO. 2 BOOK
NO. 3 BOOK	NO. 3 BOOK
NO. 4 BOOK	NO. 4 BOOK
NO. 5 BOOK	NO. 5 BOOK
NO. 6 BOOK	NO. 6 BOOK
NO. 7 BOOK	NO. 7 BOOK
NO. 8 BOOK	NO. 8 BOOK
NO. 9 BOOK	NO. 9 BOOK
NO. 10 BOOK	NO. 10 BOOK

ROUTE F.A.U. 3545 (ILLINOIS ROUTE 56) SECTION 09-00170-00-CH COUNTY DuPAGE JOB NO. R-91-016-12 RECORDING: RECORDED ON AS DOCUMENT NO.



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FILE NAME = D:\10003-SHT-Plat5.dgn	DRAWN - TKS	REVISED -
PLOT SCALE = 1"=100'	CHECKED - JMG	REVISED -
PLOT DATE = 11/7/2012	DATE = 10/22/2012	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**BUTTERFIELD ROAD AT COMMONWEALTH LANE**  
PLAT OF HIGHWAYS

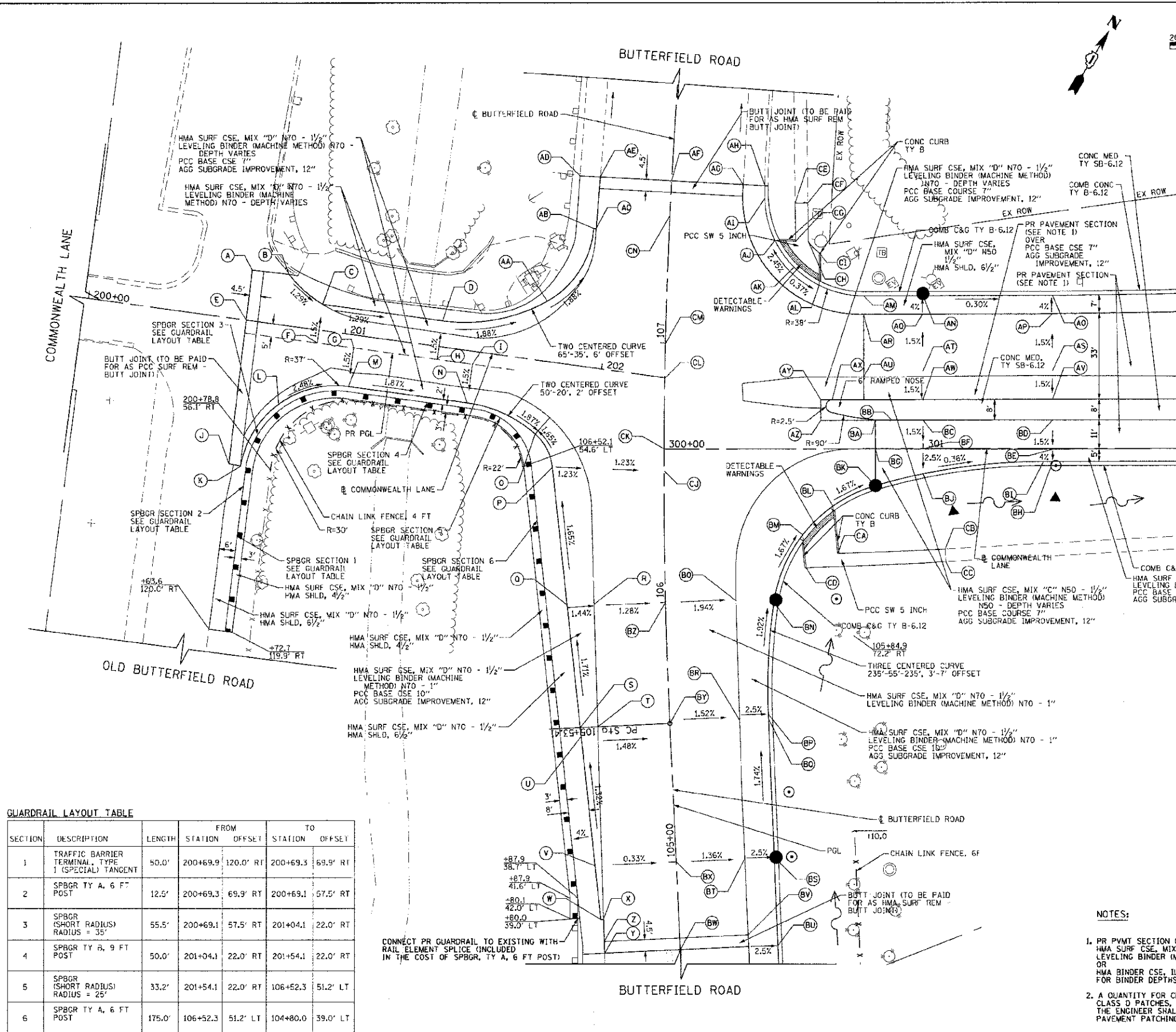
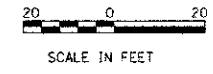
SCALE: NOT TO SCALE SHEET NO. 34 OF 108 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	09-00170-00-CH	DUPAGE	108	34
FED. ROAD DIST. NO. 7 (ILLINOIS) FED. AID PROJECT			CONTRACT NO. 63742	



DATE	
BY	
REVIEWED	
NOISE CHECKED	
ALIGNMENT CHECKED	
RT. OF WAY CHECKED	
NO.	

DATE	
BY	
REVIEWED	
NOISE CHECKED	
ALIGNMENT CHECKED	
STRUCTURE INTERFERS (PFD)	
NO.	



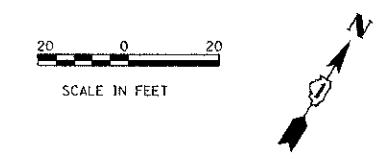
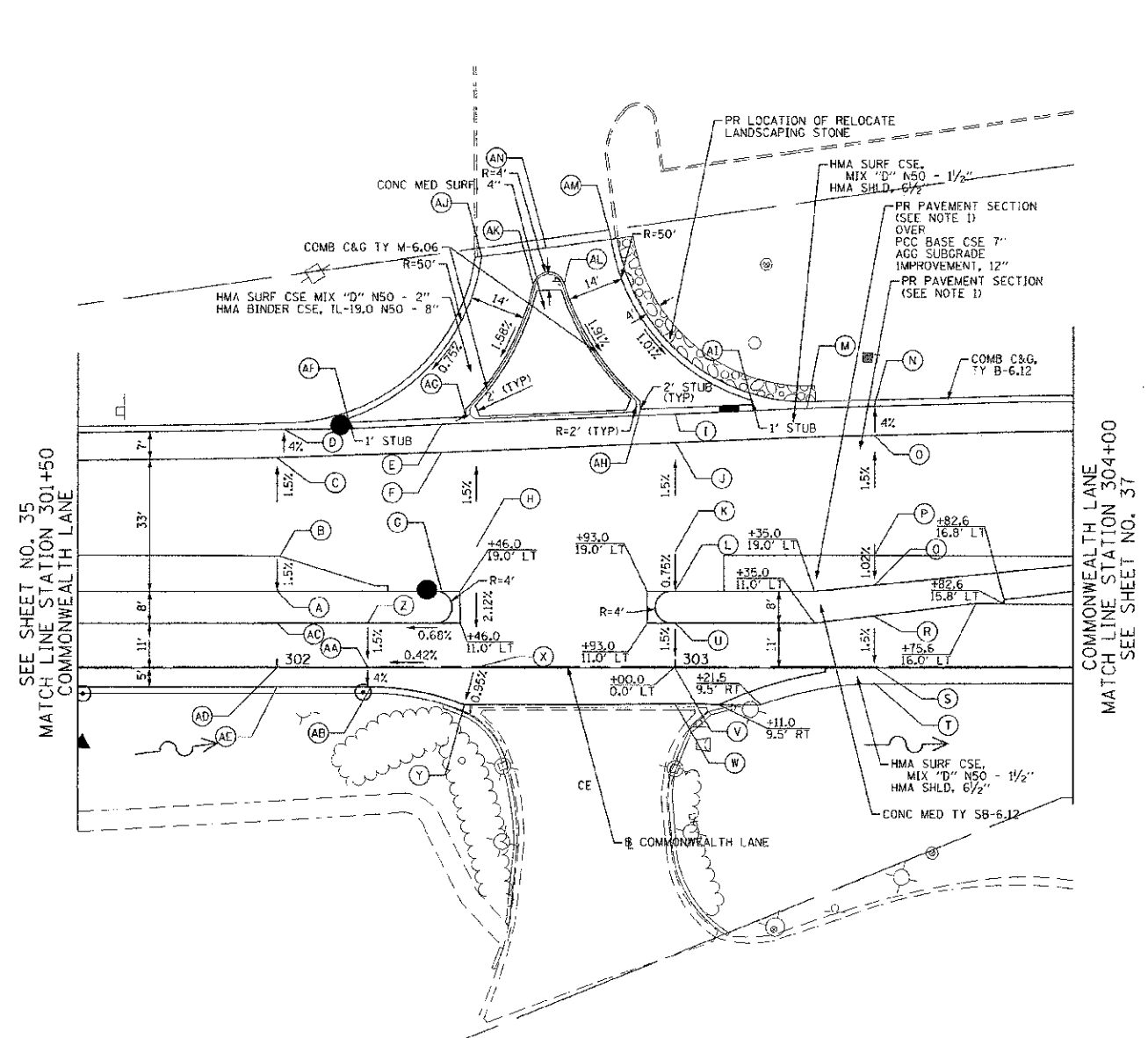
SECTION	DESCRIPTION	LENGTH	FROM STATION	TO STATION	FROM OFFSET	TO OFFSET
1	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	50.0'	200+69.9	200+69.9	120.0' RT	69.9' RT
2	SPBGR TY A, 6 FT POST	12.5'	200+69.3	200+69.1	69.9' RT	57.5' RT
3	SPBGR (SHORT RADIUS) RADIUS = 35'	55.5'	200+69.1	201+04.1	57.5' RT	22.0' RT
4	SPBGR TY A, 9 FT POST	50.0'	201+04.1	201+54.1	22.0' RT	22.0' RT
5	SPBGR (SHORT RADIUS) RADIUS = 25'	33.2'	201+54.1	106+52.3	22.0' RT	51.2' LT
6	SPBGR TY A, 6 FT POST	175.0'	106+52.3	104+80.0	51.2' LT	39.0' LT

STATION	OFFSET	ELEVATION
A	200+61.2	19.4' LT 667.33
B	200+75.5	19.7' LT 667.24
C	200+90.0	10.0' LT 667.01
D	201+36.7	10.0' LT 666.41
E	200+61.5	0.0' RT 667.76
F	200+90.0	5.0' RT 667.24
G	201+04.1	5.0' RT 667.06
H	201+36.7	5.0' RT 666.64
I	201+50.4	5.0' RT 666.20
J	200+62.5	55.8' RT 668.32
K	200+67.1	56.0' RT 668.31
L	200+78.3	30.5' RT 667.54
M	201+04.1	20.0' RT 666.83
N	201+50.4	20.0' RT 665.97
O	106+64.6	49.8' LT 665.42
P	106+52.0	43.1' LT 665.66
Q	106+00.0	38.0' LT 666.95
R	106+00.0	27.5' LT 666.80
S	105+53.4	34.6' LT 667.35
T	105+53.4	27.8' LT 667.55
U	105+50.0	34.4' LT 667.41
V	105+00.0	31.4' LT 668.15
W	104+79.2	30.1' LT 668.36
X	104+79.2	29.1' LT 668.39
Y	104+66.2	29.2' LT 668.54
Z	104+70.7	29.1' LT 668.35
AA	107+41.7	46.1' LT 665.68
AB	107+42.8	29.2' LT 664.94
AC	107+42.8	28.2' LT 664.97
AD	107+62.6	35.4' LT 664.65
AE	107+62.6	27.6' LT 664.81
AF	107+62.6	0.0' RT 664.51
AG	107+62.6	27.7' RT 663.66
AH	107+62.6	36.0' RT 663.75
AI	107+57.8	36.3' RT 663.85
AJ	107+41.3	40.9' RT 664.27
AK	107+31.1	48.7' RT 664.58
AL	107+24.3	58.7' RT 664.54
AM	300+77.1	59.0' LT 664.47
AN	301+00.0	59.0' LT 664.39
AO	301+50.0	59.0' LT 664.24
AP	301+50.0	52.0' LT 664.52
AQ	301+00.0	51.9' LT 664.68
AR	300+77.1	51.9' LT 664.75
AS	301+50.0	30.0' LT 664.86
AT	301+00.0	30.0' LT 665.01
AU	300+77.1	27.8' LT 665.05
AV	301+50.0	19.0' LT 664.69
AW	301+00.0	19.0' LT 664.84
AX	300+65.0	19.0' LT 664.95
AY	300+60.5	19.0' LT 664.96
AZ	300+60.5	11.0' LT 664.98
BA	300+81.6	11.0' LT 664.90
BB	300+88.0	11.0' LT 664.87
BC	301+00.0	11.0' LT 664.58
BD	301+50.0	11.0' LT 664.36
BE	301+50.0	0.0' RT 664.20
BF	301+00.0	0.0' RT 664.42
BG	300+81.6	0.0' RT 664.73
BH	301+50.0	5.0' RT 664.00
BI	301+46.5	5.0' RT 664.01
BJ	301+00.0	9.6' RT 664.18
BK	300+81.6	14.1' RT 664.50
BL	300+65.1	22.0' RT 664.83
BM	300+53.5	33.1' RT 665.10
BN	105+88.8	41.3' RT 665.58
BO	106+00.0	27.2' RT 665.92
BP	105+55.2	38.1' RT 666.51
BO	105+53.4	38.1' RT 666.54
BR	105+53.4	27.0' RT 666.73
BS	105+00.0	37.9' RT 667.38
BT	105+00.0	26.5' RT 667.66
BU	104+66.2	37.7' RT 668.28
BV	104+70.7	26.4' RT 668.28
BW	104+66.2	0.0' RT 668.59
BX	105+00.0	0.0' RT 668.05
BY	105+53.4	0.0' RT 667.14
BZ	106+00.0	0.0' RT 666.43
CA	106+17.8	66.8' RT 665.17
CB	106+18.9	108.3' RT 664.24
CC	106+13.8	108.5' RT 664.55
CD	106+12.4	54.5' RT 665.33
CE	107+56.4	48.1' RT 664.36
CF	107+56.7	52.9' RT 664.83
CG	107+41.3	48.9' RT 663.99
CH	107+41.3	53.9' RT 664.09
CI	107+37.1	58.2' RT 664.49
CJ	106+50.0	0.0' RT 665.70
CK	106+59.0	0.0' RT 665.58
CL	106+86.6	0.0' RT 665.30
CM	107+00.0	0.0' RT 665.04
CN	107+50.0	0.0' RT 664.61

NOTES:  
 1. PR PAVT SECTION CONSISTS OF:  
 HMA SURF CSE, MIX "D" N50 - 1 1/2"  
 LEVELING BINDER (MACHINE METHOD) N50 - DEPTH VARIES  
 OR  
 HMA BINDER CSE, IL-19.0 N50 - DEPTH VARIES  
 FOR BINDER DEPTHS, SEE SHEET NO. 38  
 2. A QUANTITY FOR CLASS D PATCHES, TYPE I, 8" AND  
 CLASS D PATCHES, TYPE II, 8" IS INCLUDED IN THE PLANS.  
 THE ENGINEER SHALL APPROVE THE LOCATIONS OF  
 PAVEMENT PATCHING PRIOR TO PLACEMENT.

DATE	BY
DESIGNED	TKH
CHECKED	JMG
DATE	10/22/2012

DATE	BY
DESIGNED	TKH
CHECKED	JMG
DATE	10/22/2012



	STATION	OFFSET	ELEVATION
A	302+00.0	19.0' LT	664.55
B	302+00.0	30.0' LT	664.72
C	302+00.0	52.7' LT	664.38
D	302+00.0	39.7' LT	664.09
E	302+50.0	61.3' LT	663.95
F	302+50.0	54.2' LT	664.23
G	302+40.0	19.0' LT	664.45
H	302+50.0	30.0' LT	664.59
I	303+00.0	63.5' LT	663.78
J	303+00.0	56.4' LT	664.07
K	303+00.0	30.0' LT	664.47
L	303+00.0	19.0' LT	664.39
M	303+33.1	64.9' LT	663.69
N	303+50.0	65.5' LT	663.64
O	303+50.0	58.2' LT	663.93
P	303+50.0	30.0' LT	664.35
Q	303+50.0	20.5' LT	664.25
R	303+50.0	12.8' LT	663.65
S	303+50.0	0.3' RT	663.47
T	303+50.0	4.3' RT	663.31
U	303+00.0	11.0' LT	664.00
V	303+00.0	0.2' RT	663.84
W	303+00.0	9.2' RT	663.75
X	302+50.0	0.2' RT	664.02
Y	302+47.1	9.5' RT	663.93
Z	302+23.7	11.0' LT	664.07
AA	302+23.7	0.2' RT	663.91
AB	302+23.7	5.0' RT	663.71
AC	302+00.0	11.0' LT	664.16
AD	302+00.0	0.1' RT	664.00
AE	302+00.0	5.0' RT	663.80
AF	302+18.1	61.5' LT	664.10
AG	302+47.4	62.8' LT	664.00
AH	302+91.3	64.7' LT	663.88
AI	303+19.4	65.9' LT	663.80
AJ	302+51.3	103.1' LT	664.52
AK	302+64.3	96.1' LT	664.60
AL	302+72.0	96.4' LT	664.60
AM	302+83.8	107.4' LT	664.43
AN	302+67.5	105.2' LT	664.55

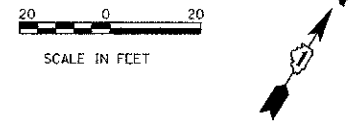
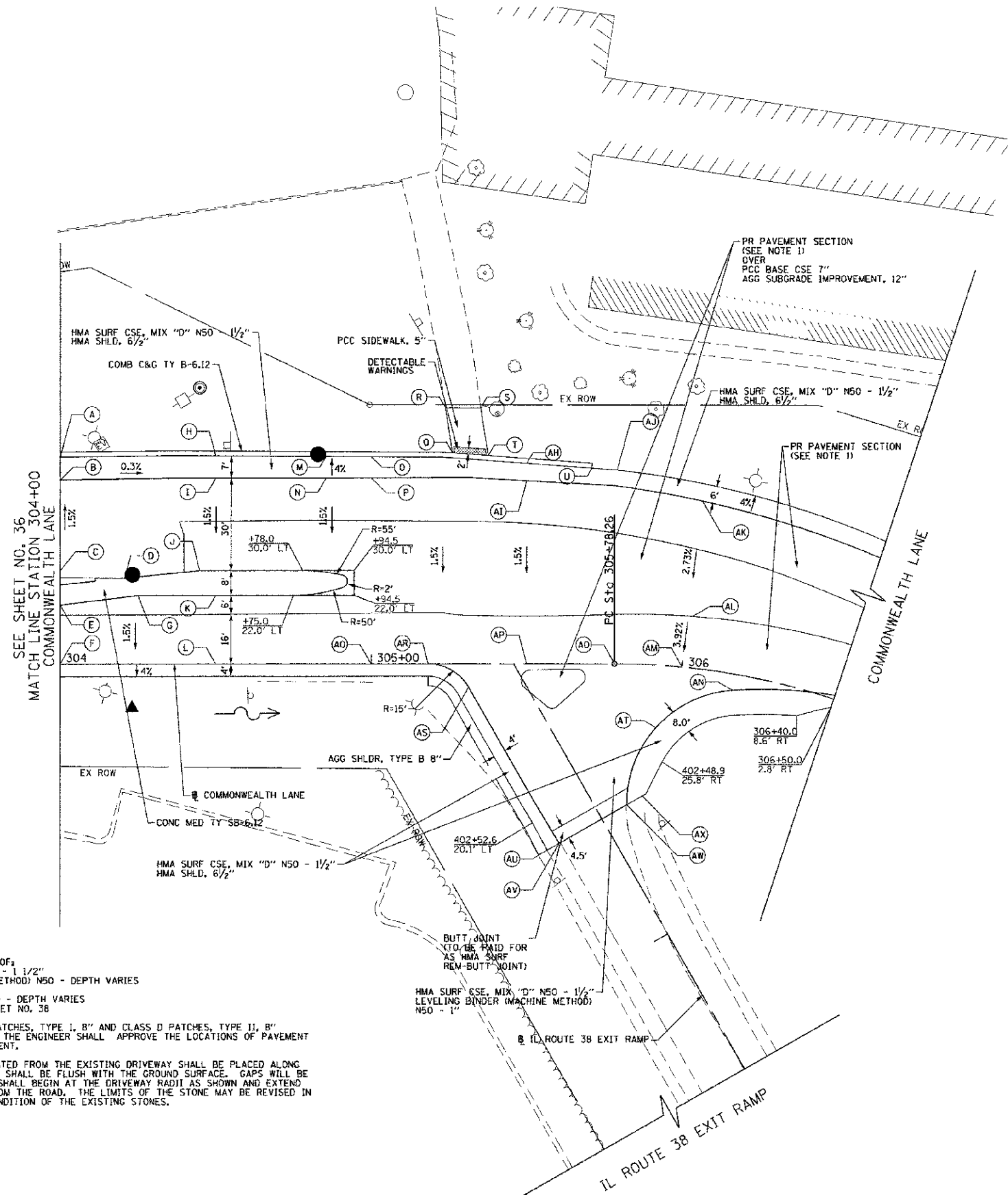
- NOTES:**
- PR PAVT SECTION CONSISTS OF:  
 HMA SURF CSE, MIX "D" N50 - 1 1/2"  
 LEVELING BINDER (MACHINE METHOD) N50 - DEPTH VARIES  
 OR  
 HMA BINDER CSE, IL-19.0 N50 - DEPTH VARIES  
 FOR BINDER DEPTHS, SEE SHEET NO. 38
  - A QUANTITY FOR CLASS D PATCHES, TYPE I, 8" AND CLASS D PATCHES, TYPE II, 8" IS INCLUDED IN THE PLANS. THE ENGINEER SHALL APPROVE THE LOCATIONS OF PAVEMENT PATCHING PRIOR TO PLACEMENT.

FILE NAME =	USER NAME = khalorenz	DESIGNED = TKH	REVISED =	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BUTTERFIELD ROAD AT COMMONWEALTH LANE INTERSECTION PAVING PLAN</b>		F.A.J. RTE. = 3545	SECTION = 09-00170-00-CH	COUNTY = DUPAGE	TOTAL SHEETS = 108	SHEET NO. = 36
CONTRACT NO. = 63742	SCALE = 1"=20'	SHEET NO. = 36 OF 108 SHEETS	STA. 301+50 TO STA. 304+00		FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT						
CONTRACT NO. = 63742	SCALE = 1"=20'	SHEET NO. = 36 OF 108 SHEETS	STA. 301+50 TO STA. 304+00		FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT						
CONTRACT NO. = 63742	SCALE = 1"=20'	SHEET NO. = 36 OF 108 SHEETS	STA. 301+50 TO STA. 304+00		FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT						



DATE	BY	CHECKED	DATE
PLAN	DESIGNED	CHECKED	DATE
NOTE BOOK	NO. 1	NO. 2	NO. 3

DATE	BY	CHECKED	DATE
PROFILE	DESIGNED	CHECKED	DATE
NOTE BOOK	NO. 1	NO. 2	NO. 3



STATION	OFFSET	ELEVATION
A	304+00.0	66.6' LT 663.49
B	304+00.0	59.2' LT 663.78
C	304+00.0	30.0' LT 664.21
D	304+20.7	27.6' LT 664.06
E	304+00.0	18.9' LT 663.50
F	304+00.0	0.1' RT 663.21
G	304+25.0	22.0' LT 663.38
H	304+50.0	66.9' LT 663.34
I	304+50.0	59.7' LT 663.63
J	304+50.0	30.0' LT 663.18
K	304+50.0	22.0' LT 663.24
L	304+50.0	0.1' RT 662.86
M	304+85.5	66.7' LT 663.23
N	304+85.5	59.7' LT 663.54
O	305+00.0	66.6' LT 663.27
P	305+00.0	59.8' LT 663.51
Q	305+26.1	67.8' LT 663.41
R	305+23.8	82.3' LT 663.17
S	305+35.2	82.3' LT 663.00
T	305+37.6	66.9' LT 663.51
U	305+71.0	62.9' LT 663.38
V	NOT USED	
W	NOT USED	
X	NOT USED	
Y	NOT USED	
Z	NOT USED	
AA	NOT USED	
AB	NOT USED	
AC	NOT USED	
AD	NOT USED	
AE	NOT USED	
AF	NOT USED	
AG	NOT USED	
AH	305+50.0	64.5' LT 663.42
AI	305+50.0	58.9' LT 663.45
AJ	305+78.9	62.3' LT 663.36
AK	306+00.0	53.9' LT 663.64
AL	306+00.0	16.5' LT 662.62
AM	306+00.0	0.6' LT 661.97
AN	402+57.7	56.9' RT 661.59
AO	305+78.3	0.0' RT 662.36
AP	305+50.0	0.0' RT 662.56
AQ	305+00.0	0.0' RT 662.61
AR	305+18.3	0.0' LT 662.62
AS	403+00.0	16.3' LT 662.56
AT	402+59.5	29.6' RT 661.85
AU	402+42.6	18.9' LT 662.21
AV	402+42.6	15.9' LT 662.48
AW	402+42.6	9.2' RT 662.07
AX	402+42.6	14.7' RT 661.75

- NOTES:**
- PR PAVT SECTION CONSISTS OF:  
HMA SURF CSE, MIX "D" N50 - 1 1/2"  
LEVELING BINDER (MACHINE METHOD) N50 - DEPTH VARIES  
OR  
HMA BINDER CSE, IL-19.0 N50 - DEPTH VARIES  
FOR BINDER DEPTHS, SEE SHEET NO. 38
  - A QUANTITY FOR CLASS D PATCHES, TYPE I, B" AND CLASS D PATCHES, TYPE II, B" IS INCLUDED IN THE PLANS. THE ENGINEER SHALL APPROVE THE LOCATIONS OF PAVEMENT PATCHING PRIOR TO PLACEMENT.
  - LANDSCAPING STONES RELOCATED FROM THE EXISTING DRIVEWAY SHALL BE PLACED ALONG THE PROPOSED DRIVEWAY AND SHALL BE FLUSH WITH THE GROUND SURFACE. GAPS WILL BE FILLED WITH FINES. STONE SHALL BEGIN AT THE DRIVEWAY RADII AS SHOWN AND EXTEND APPROXIMATELY 50' AWAY FROM THE ROAD. THE LIMITS OF THE STONE MAY BE REVISED IN THE FIELD BASED ON THE CONDITION OF THE EXISTING STONES.

COMMONWEALTH LANE

STATION	LEVELING BINDER (MACHINE METHOD) N50							
	FROM		TO		FROM		TO	
	OFFSET	DEPTH	OFFSET	DEPTH	OFFSET	DEPTH	OFFSET	DEPTH
300+50.0	11.5' LT	2.25"	36.0' LT	2.25"				
301+00.0	9.6' RT	1.00"	11.0' LT	1.00"				
301+50.0	5.0' RT	1.00"	11.0' LT	1.00"				
302+00.0	1.6' LT	2.25"	11.0' LT	1.00"				
302+50.0	9.5' RT	1.00"	4.5' LT	2.25"				
302+67.6	9.5' RT	1.00"	6.1' LT	2.25"				
303+00.0	9.5' RT	1.00"	5.7' LT	2.25"				
303+50.0	0.1' RT	1.00"	12.7' LT	2.19"				
304+00.0	0.0' RT	1.00"	18.9' LT	2.00"				
304+50.0	0.0' RT	1.00"	5.8' LT	2.25"	57.9' LT	2.25"	59.7' LT	2.00"
305+00.0	0.0' RT	1.00"	10.5' LT	2.25"	56.3' LT	2.25"	59.8' LT	1.00"
305+22.4	0.5' RT	1.00"	8.2' LT	2.25"				
305+50.0	40.4' RT	1.00"	1.4' LT	2.25"	55.2' LT	2.25"	58.9' LT	1.00"
306+00.0	13.4' RT	1.00"	4.6' LT	2.25"				
306+50.0	1.3' LT	1.00"	23.1' LT	2.25"	42.2' LT	2.25"	47.8' LT	1.00"
306+90.7	2.1' LT	1.00"	46.9' LT	1.00"				
307+00.0	2.1' LT	1.00"	46.9' LT	1.00"				
307+50.0	1.4' LT	1.00"	48.2' LT	1.00"				
308+00.0	0.3' LT	1.00"	48.3' LT	1.00"				
308+50.0	0.5' RT	1.00"	42.9' LT	1.00"				
309+00.0	0.7' RT	1.00"	36.6' LT	1.00"				
309+50.0	0.2' RT	1.00"	32.2' LT	1.00"				
309+68.0	0.0' RT	1.00"	31.2' LT	1.00"				

COMMONWEALTH LANE

STATION	HOT MIX ASPHALT BINDER COURSE, IL-19.0, N50							
	FROM		TO		FROM		TO	
	OFFSET	DEPTH	OFFSET	DEPTH	OFFSET	DEPTH	OFFSET	DEPTH
300+50.0	38.1' RT	4.83"	11.5' LT	2.25"	36.0' LT	2.25"	59.8' LT	4.36"
301+00.0	19.0' LT	6.24"	51.9' LT	2.94"				
301+50.0	19.0' LT	7.66"	52.0' LT	2.71"				
302+00.0	5.0' RT	3.11"	1.6' LT	2.25"	19.0' LT	7.38"	52.7' LT	5.38"
302+50.0	4.5' LT	2.25"	54.2' LT	7.29"				
302+67.6	6.1' LT	2.25"	54.9' LT	7.60"				
303+00.0	5.7' LT	2.25"	11.0' LT	3.29"	19.0' LT	6.88"	56.4' LT	7.08"
303+50.0	20.4' LT	11.14"	58.2' LT	4.72"				
304+00.0	25.5' LT	10.96"	59.2' LT	3.26"				
304+50.0	5.8' LT	2.25"	22.0' LT	3.96"	30.0' LT	3.81"	57.9' LT	2.25"
305+00.0	10.5' LT	2.25"	56.3' LT	2.25"				
305+22.4	8.2' LT	2.25"	59.9' LT	2.25"				
305+50.0	1.4' LT	2.25"	55.2' LT	2.25"				
306+00.0	4.6' LT	2.25"	53.9' LT	4.21"				
306+50.0	23.1' LT	2.25"	42.2' LT	2.25"				
306+90.7								
307+00.0								
307+50.0								
308+00.0								
308+50.0								
309+00.0								
309+50.0								
309+68.0								

PLAN	DATE
REVISIONS	BY
ALIGNMENT CHECKED	
BY	
DATE	
NOTE BOOK	
NO.	

PROFILE	DATE
REVISIONS	BY
GRADES CHECKED	
BY	
DATE	
NOTE BOOK	
NO.	

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		DRAWN - TKH	REVISED -
		CHECKED - JMC	REVISED -
		DATE - 10/22/2012	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

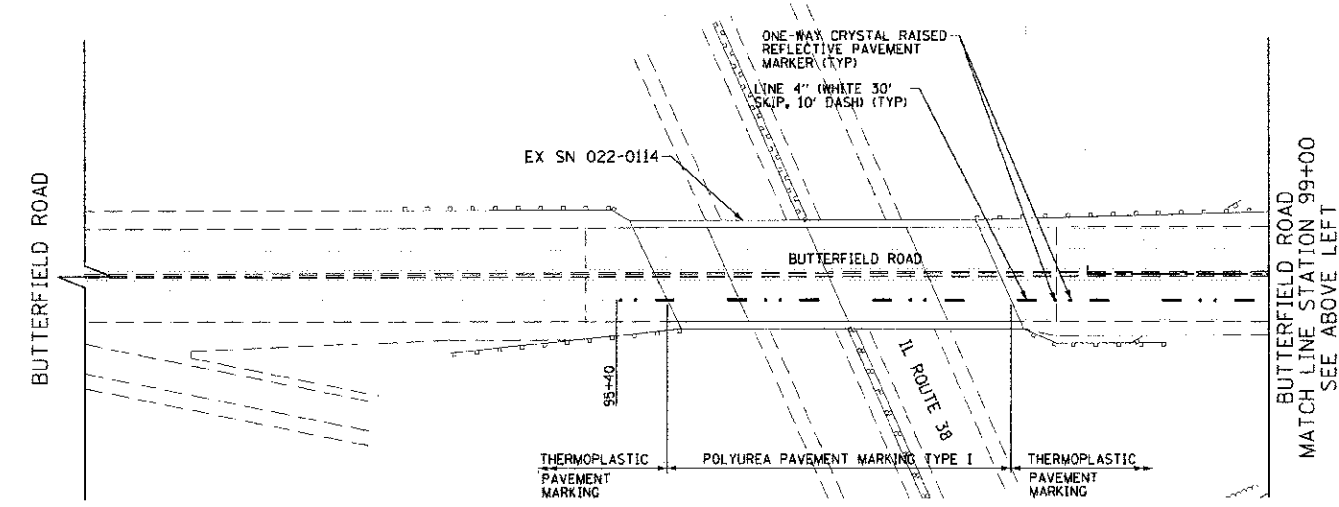
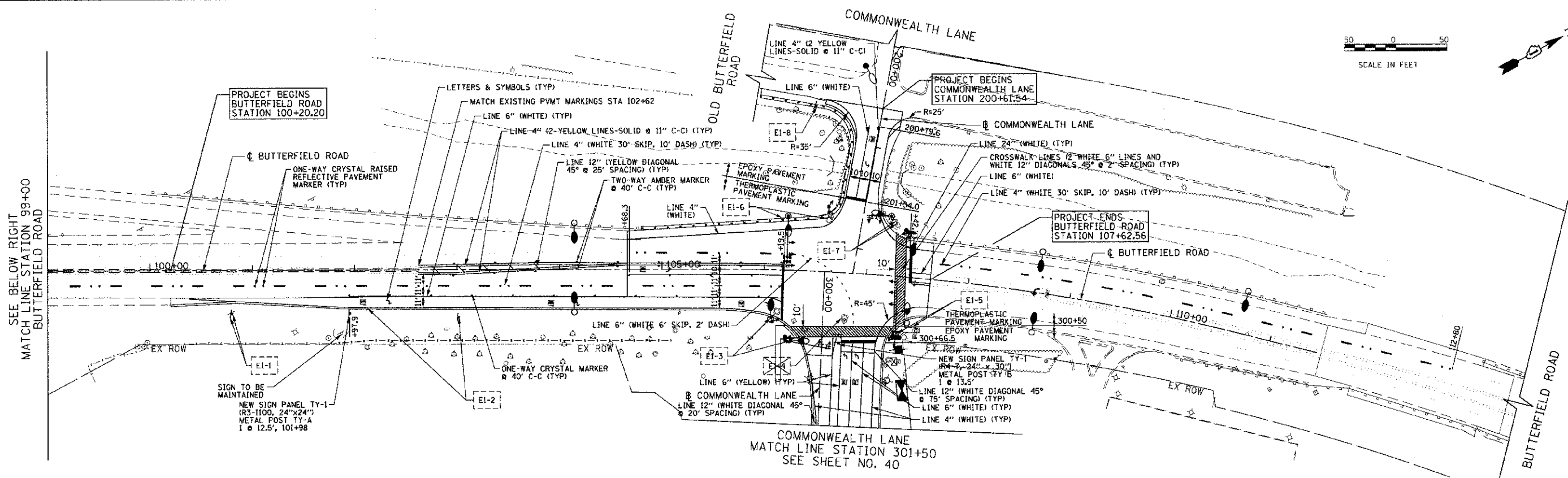
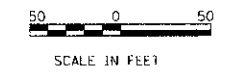
**BUTTERFIELD ROAD AT COMMONWEALTH LANE  
COMMONWEALTH LANE PAVING DETAILS**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	09-00170-00-CH		108	38
FED. ROAD DIST. NO. 7 (ILLINOIS) FED. AID PROJECT			CONTRACT NO. 63742	

SCALE: NOT TO SCALE SHEET NO. 38 OF 108 SHEETS

DATE	3/1/12	
BY	TKS	
REVISIONS		
NO. 1	DATE	DESCRIPTION
1	3/1/12	ISSUED FOR BIDDING
2		REVISED PER COMMENTS
3		REVISED PER COMMENTS
4		REVISED PER COMMENTS
5		REVISED PER COMMENTS
6		REVISED PER COMMENTS
7		REVISED PER COMMENTS
8		REVISED PER COMMENTS
9		REVISED PER COMMENTS
10		REVISED PER COMMENTS

DATE	3/1/12	
BY	TKS	
REVISIONS		
NO. 1	DATE	DESCRIPTION
1	3/1/12	ISSUED FOR BIDDING
2		REVISED PER COMMENTS
3		REVISED PER COMMENTS
4		REVISED PER COMMENTS
5		REVISED PER COMMENTS
6		REVISED PER COMMENTS
7		REVISED PER COMMENTS
8		REVISED PER COMMENTS
9		REVISED PER COMMENTS
10		REVISED PER COMMENTS



**LEGEND**

PR SIGN PANEL ASSEMBLY AND POST

NEW SIGN PANEL TYPE 1 OR 2 WITH MUTCD CODE

EXISTING SIGN ASSEMBLY TO BE RELOCATED; SEE BELOW TABLE

EXISTING SIGN ASSEMBLY TO BE REMOVED; SEE BELOW TABLE

RAISED REFLECTIVE PAVEMENT MARKERS (SEE DISTRICT DETAIL TC-1)

R4-7 (24"x30")

R3-1100 (24"x24")

RIGHT TURN LANE

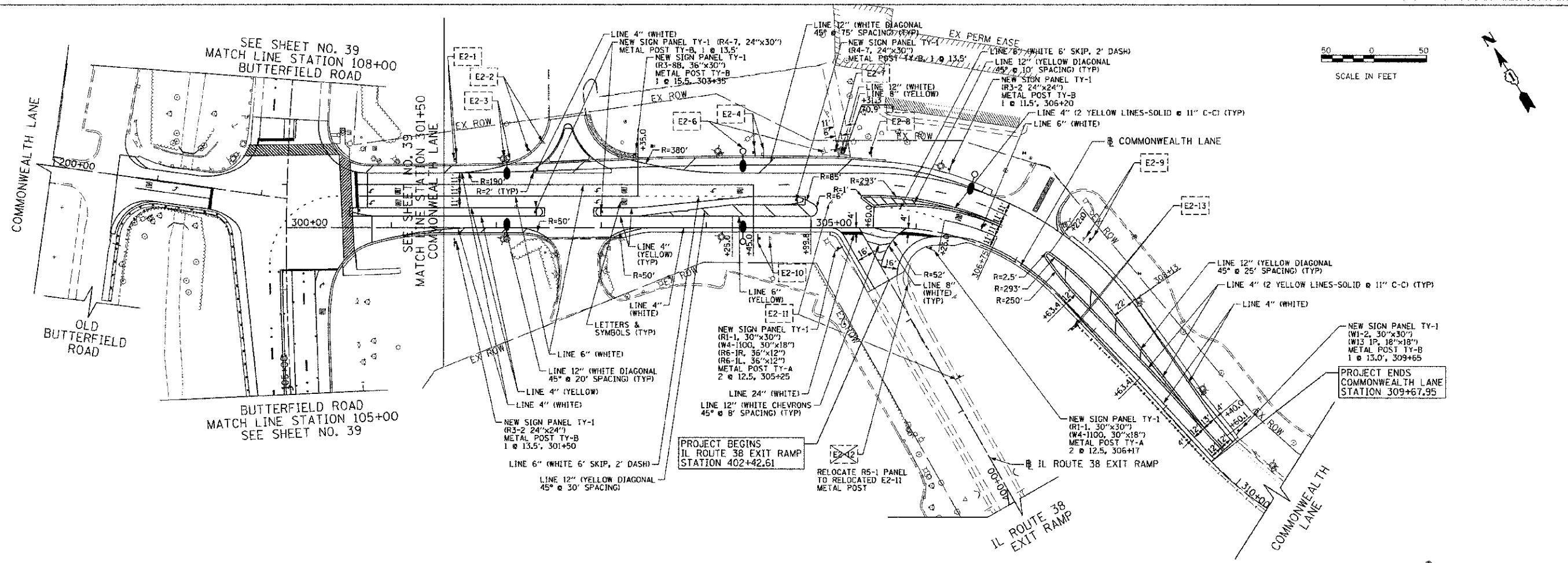
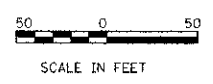
10' 10' 10' 10'

30' 40'

PAVEMENT MARKING ITEM	CORRESPONDING RECESSED GROOVING ITEM
EPOXY PAVEMENT MARKING - LETTERS AND SYMBOLS	GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS AND SYMBOLS
EPOXY PAVEMENT MARKING - LINE 4"	GROOVING FOR RECESSED PAVEMENT MARKING 5"
EPOXY PAVEMENT MARKING - LINE 6"	GROOVING FOR RECESSED PAVEMENT MARKING 7"
EPOXY PAVEMENT MARKING - LINE 12"	GROOVING FOR RECESSED PAVEMENT MARKING 13"
EPOXY PAVEMENT MARKING - LINE 24"	GROOVING FOR RECESSED PAVEMENT MARKING 25"

- NOTES:**
- EPOXY PAVEMENT MARKINGS SHALL BE USED ON COMMONWEALTH LANE, EXCEPT AS NOTED.
  - EPOXY PAVEMENT MARKINGS SHALL BE INSTALLED IN GROOVED RECESSED CHANNELS CONSTRUCTED 0.040 INCHES BELOW THE SURFACE AND 1 INCH WIDER THAN THE PAVEMENT MARKING LINE. CONSTRUCTION OF THE RECESSED CHANNELS SHALL BE PAID FOR SEPARATELY PER FOOT AS GROOVING FOR RECESSED PAVEMENT MARKING OF THE WIDTH SPECIFIED AND PER SQUARE FOOT FOR GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS AND SYMBOLS.
  - THERMOPLASTIC PAVEMENT MARKING SHALL BE USED ON BUTTERFIELD ROAD, EXCEPT AS NOTED.
  - THERMOPLASTIC PAVEMENT MARKING SHALL BE USED ON THE ILLINOIS ROUTE 38 EXIT RAMP. THIS TYPE OF PAVEMENT MARKING SHALL BE PLACED RIGHT OF THE CENTERLINE OF COMMONWEALTH LANE, FROM COMMONWEALTH LANE STATION 305+18 TO 306+54.

ACTION	MUTCD CODE	DESCRIPTION	HEIGHT (IN)	WIDTH (IN)	PANEL ASSEMBLY TYPE	EX SIGN SUPPORT	EX LOCATION STATION	EX OFFSET	PR LOCATION STATION	PR OFFSET	PR METAL POST TYPE	NO. OF POSTS	PR POST LENGTH	
E1-1	RELOCATE	11-H07a	"LOVE THE LAND OF LINCOLN"	36	30	B	POST MOUNTED (GROUND)	100+80	42.0' RT	100+80	42.0' RT	B	2	18.5'
E1-2	RELOCATE	11-H07b	"ADOPT-A-HIGHWAY / MORIARTY FAMILY / ILLINOIS DEPARTMENT OF TRANSPORTATION"	18	30	B	POST MOUNTED (GROUND)	102+11	37.5' RT	103+00	42.0' RT	A	2	15.0'
E1-3	RELOCATE	W1-2	"KEEP ILLINOIS CLEAN"	48"	DIAMOND	A	MOUNTED ON SIGNAL POST	106+13	40.3' RT	106+29	68.2' RT	---	---	---
E1-4	REMOVE	R4-7	CURVE - RIGHT	30	30	A	MOUNTED ON SIGNAL POST	106+81	43.7' RT	---	---	---	---	---
E1-5	RELOCATE	W17-H00	"BUTTERFIELD ROAD"	9	30	A	MOUNTED ON SIGNAL POST	107+32	58.6' RT	107+50	47.2' RT	---	---	---
E1-6	RELOCATE	R1-1	STOP SIGN (FOLDED)	30	30	A	MOUNTED ON SIGNAL POST	106+61	50.6' LT	106+15	51.4' LT	---	---	---
E1-7	RELOCATE	R1-1	STOP SIGN (FOLDED)	30	30	A	MOUNTED ON SIGNAL POST	107+37	43.1' LT	107+24	53.1' LT	---	---	---
E1-8	RELOCATE	R1-1	STOP SIGN	30	30	A	POST MOUNTED (GROUND)	200+70	64.9' RT	200+70	65.0' RT	B	1	12.5'
		W4-4aP	TRAFFIC FROM RIGHT DOES NOT STOP	12	24	A								



DATE	BY
DATE	BY
DATE	BY
DATE	BY
DATE	BY

DATE	BY
DATE	BY
DATE	BY
DATE	BY
DATE	BY

ACTION	MUTCD CODE	DESCRIPTION	HEIGHT (IN)	WIDTH (IN)	PANEL ASSEMBLY TYPE	EX SIGN SUPPORT	EX LOCATION		PR LOCATION		PR METAL POST TYPE	NO. OF POSTS	PR POST LENGTH
							STATION	OFFSET	STATION	OFFSET			
E2-1	W11-1	BICYCLE	8	12	A	POST MOUNTED (GROUND)	301+61	64.3'LT	301+61	64.0'LT	A	1	14.0'
	N/A	"SALT CREEK TRAIL"	18	12									
	N/A	"TRAIL SEGMENT BY"	8	12									
E2-2	N/A	STOP SIGN	18	18	A	POST MOUNTED (GROUND)	302+44	80.7'LT	302+30	74.0'LT	---	---	---
	N/A	NO PARKING	18	12									
	N/A	DIRECTIONAL ARROW	8	8									
E2-3	W11-1	BICYCLE	8	12	A	POST MOUNTED (GROUND)	304+54	70.3'LT	304+85	70.7'LT	A	1	14.0'
	N/A	"SALT CREEK TRAIL"	18	12									
	N/A	"TRAIL SEGMENT BY"	8	12									
E2-4	N/A	NO PARKING	18	12	A	MOUNTED ON LIGHT POLE	302+07	85.7'LT	302+10	66.8'LT	---	---	---
	N/A	"SALT CREEK TRAIL"	18	12									
	N/A	"TRAIL SEGMENT BY"	8	12									
E2-5	W11-1	BICYCLE	8	12	A	POST MOUNTED (GROUND)	305+12	72.7'LT	304+35	72.9'LT	---	---	---
	N/A	NO PARKING	18	12									
	N/A	STOP SIGN	18	18									
E2-6	N/A	NO PARKING	18	12	A	MOUNTED ON LIGHT POLE	305+23	73.8'LT	305+23	73.8'LT	---	---	---
	N/A	"SALT CREEK TRAIL"	18	12									
	N/A	DIRECTIONAL ARROW	8	8									
E2-7	W11-1	BICYCLE	8	12	A	POST MOUNTED (GROUND)	305+66	71.4'LT	305+56	71.4'LT	---	---	---
	N/A	DIRECTIONAL ARROW	8	8									
	N/A	BICYCLE	8	12									
E2-8	N/A	NO PARKING	18	12	A	POST MOUNTED (GROUND)	307+36	57.0'LT	307+40	60.3'LT	A	1	14.0'
	N/A	"SALT CREEK TRAIL"	18	12									
	N/A	"TRAIL SEGMENT BY"	8	12									
E2-9	N/A	NO PARKING	18	12	A	POST MOUNTED (GROUND)	304+60	10.2'RT	304+50	8.5'RT	A	1	12.0'
	M4-5	"T"	12	24									
	M1-1100	"ILLINOIS 55"	24	24									
E2-10	M6-2L	DIRECTIONAL ARROW	15	21	A	POST MOUNTED (MEDIAN)	305+19	25.1'LT	IL RTE 38 RAMP	IL RTE 38 RAMP	B	1	14.5'
	R1-1	STOP SIGN	30	30									
	W4-4P	CROSS TRAFFIC DOES NOT STOP	18	30									
E2-11	R6-2	ONE WAY	24	18	A	POST MOUNTED (GROUND)	307+85	1.7'RT	307+85	12.2'RT	A	2	15.5'
	W1-2	CURVE - LEFT	30	30									
	W13-1P	25 MPH	18	18									

**LEGEND**

- PR SIGN PANEL ASSEMBLY AND POST
- NEW SIGN PANEL TYPE 1 OR 2 WITH MUTCD CODE
- EXISTING SIGN ASSEMBLY TO BE RELOCATED; SEE BELOW TABLE
- EXISTING SIGN ASSEMBLY TO BE REMOVED; SEE BELOW TABLE
- RAISED REFLECTIVE PAVEMENT MARKERS (SEE DISTRICT DETAIL TC-11)

PAYMENT MARKING ITEM	CORRESPONDING RECESSED GROOVING ITEM
EPOXY PAYMENT MARKING - LETTERS AND SYMBOLS	GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS AND SYMBOLS
EPOXY PAYMENT MARKING - LINE 4"	GROOVING FOR RECESSED PAVEMENT MARKING 5"
EPOXY PAYMENT MARKING - LINE 6"	GROOVING FOR RECESSED PAVEMENT MARKING 7"
EPOXY PAYMENT MARKING - LINE 12"	GROOVING FOR RECESSED PAVEMENT MARKING 13"
EPOXY PAYMENT MARKING - LINE 24"	GROOVING FOR RECESSED PAVEMENT MARKING 25"

- NOTES:**
- EPOXY PAVEMENT MARKINGS SHALL BE USED ON COMMONWEALTH LANE, EXCEPT AS NOTED.
  - EPOXY PAVEMENT MARKINGS SHALL BE INSTALLED IN GROOVED RECESSED CHANNELS CONSTRUCTED 0.040 INCHES BELOW THE SURFACE AND 1 INCH WIDER THAN THE PAVEMENT MARKING LINE. CONSTRUCTION OF THE RECESSED CHANNELS SHALL BE PAID FOR SEPARATELY PER FOOT AS GROOVING FOR RECESSED PAVEMENT MARKING OF THE WIDTH SPECIFIED AND PER SQUARE FOOT FOR GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS AND SYMBOLS.
  - THERMOPLASTIC PAVEMENT MARKING SHALL BE USED ON BUTTERFIELD ROAD, EXCEPT AS NOTED.
  - THERMOPLASTIC PAVEMENT MARKING SHALL BE USED ON THE ILLINOIS ROUTE 38 EXIT RAMP. THIS TYPE OF PAVEMENT MARKING SHALL BE PLACED RIGHT OF THE CENTERLINE OF COMMONWEALTH LANE, FROM COMMONWEALTH LANE STATION 305+18 TO 306+54.



# TRAFFIC SIGNAL LEGEND

ITEM	REMOVAL	EXISTING	PROPOSED
CONTROLLER CABINET			
RAILROAD CONTROL CABINET			
COMMUNICATIONS CABINET			
MASTER CONTROLLER			
MASTER MASTER CONTROLLER			
UNINTERRUPTIBLE POWER SUPPLY			
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT			
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT			
STEEL MAST ARM ASSEMBLY AND POLE			
ALUMINUM MAST ARM ASSEMBLY AND POLE			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA			
SIGNAL POST			
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM			
GLY WIRE			
SIGNAL HEAD			
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)			
SIGNAL HEAD WITH BACKPLATE			
SIGNAL HEAD OPTICALLY PROGRAMMED			
FLASHER INSTALLATION (S DENOTES SOLAR POWER)			
PEDESTRIAN SIGNAL HEAD			
PEDESTRIAN PUSHBUTTON DETECTOR			
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR			
ILLUMINATED SIGN "NO LEFT TURN"			
ILLUMINATED SIGN "NO RIGHT TURN"			
DETECTOR LOOP, TYPE I			
PERFORMED DETECTOR LOOP			
MICROWAVE VEHICLE SENSOR			
VIDEO DETECTION CAMERA			
VIDEO DETECTION ZONE			
PAN, TILT, ZOOM CAMERA			
WIRELESS DETECTOR SENSOR			
WIRELESS ACCESS POINT			

ITEM	REMOVAL	EXISTING	PROPOSED
EMERGENCY VEHICLE LIGHT DETECTOR			
CONFIRMATION BEACON			
HANDHOLE			
HEAVY DUTY HANDHOLE			
DOUBLE HANDHOLE			
JUNCTION BOX			
GALVANIZED STEEL CONDUIT IN TRENCH (T) OR PUSHED (P)			
TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE			
COMMON TRENCH			CT
CONCRETE NONMETALLIC CONDUIT (EMPTY)			CNC
SYSTEM ITEM		S	S
INTERSECTION ITEM		I	IP
REMOVE ITEM	R		
RELOCATE ITEM	RL		
ABANDON ITEM	A		
12" (300mm) TRAFFIC SIGNAL SECTION			
12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE			
SIGNAL FACE			
SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD			
12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL			
12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED			
12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID			
PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER			
RADIO INTERCONNECT			
RADIO REPEATER			
DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED			
GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)			

ITEM	REMOVAL	EXISTING	PROPOSED
ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE			
COAXIAL CABLE			
VENDOR CABLE FOR CAMERA			
COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED			
FIBER OPTIC CABLE NO. 62.5/125, MM12F			
FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F			
FIBER OPTIC CABLE NO. 62.5/125, (NUMBER OF FIBERS & TYPE TO BE NOTED ON PLANS)			
GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE			
CONTROLLER CABINET AND FOUNDATION TO BE REMOVED			
STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED			
ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED			
SIGNAL POST AND FOUNDATION TO BE REMOVED			
INTERSECTION & SAMPLING (SYSTEM) DETECTOR			
SAMPLING (SYSTEM) DETECTOR			
EXISTING INTERSECTION LOOP DETECTOR PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
EXISTING PREFORMED INTERSECTION LOOP DETECTOR PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
PREFORMED SAMPLING (SYSTEM) DETECTOR			

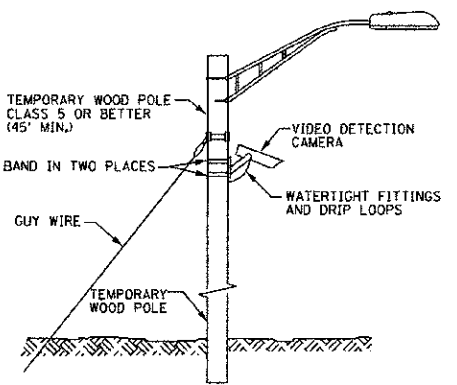
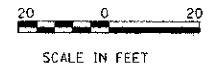
## RAILROAD SYMBOLS

	EXISTING	PROPOSED
RAILROAD CONTROL CABINET		
RAILROAD CANTILEVER MAST ARM		
FLASHING SIGNAL		
CROSSING GATE		
CROSSBUCK		



**NOTES FOR TEMPORARY TRAFFIC SIGNALS**

- 1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNALS SHALL BE FURNISHED BY THE CONTRACTOR.
- 2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1. INSTALLED IN A NEMA TS1 OR TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
- 3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE 12". HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
- 4. ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
- 5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL.
- 6. THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS, AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL AT THE TIME OF THE TURN ON IF NO TRAFFIC STAGING IS IN PLACE OR WILL BE STAGED ON THE DAY OF THE TURN ON.
- 7. UNINTERRUPTIBLE POWER SUPPLY (UPS) SYSTEMS SHALL BE INSTALLED AND MADE OPERATIONAL AT TEMPORARY TRAFFIC SIGNAL INSTALLATIONS AS INDICATED ON THE PLANS.
- 8. TRAFFIC SIGNAL MANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
- 9. DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER.
- 10. WHEN PAN, TILT, ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.
- 11. THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.

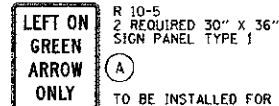


**TEMPORARY VIDEO DETECTION MOUNTING DETAIL**  
(NOT TO SCALE)

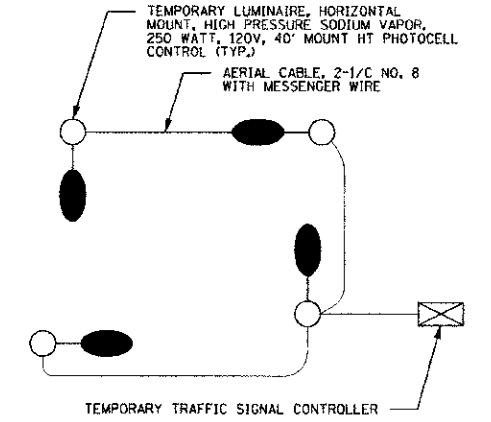
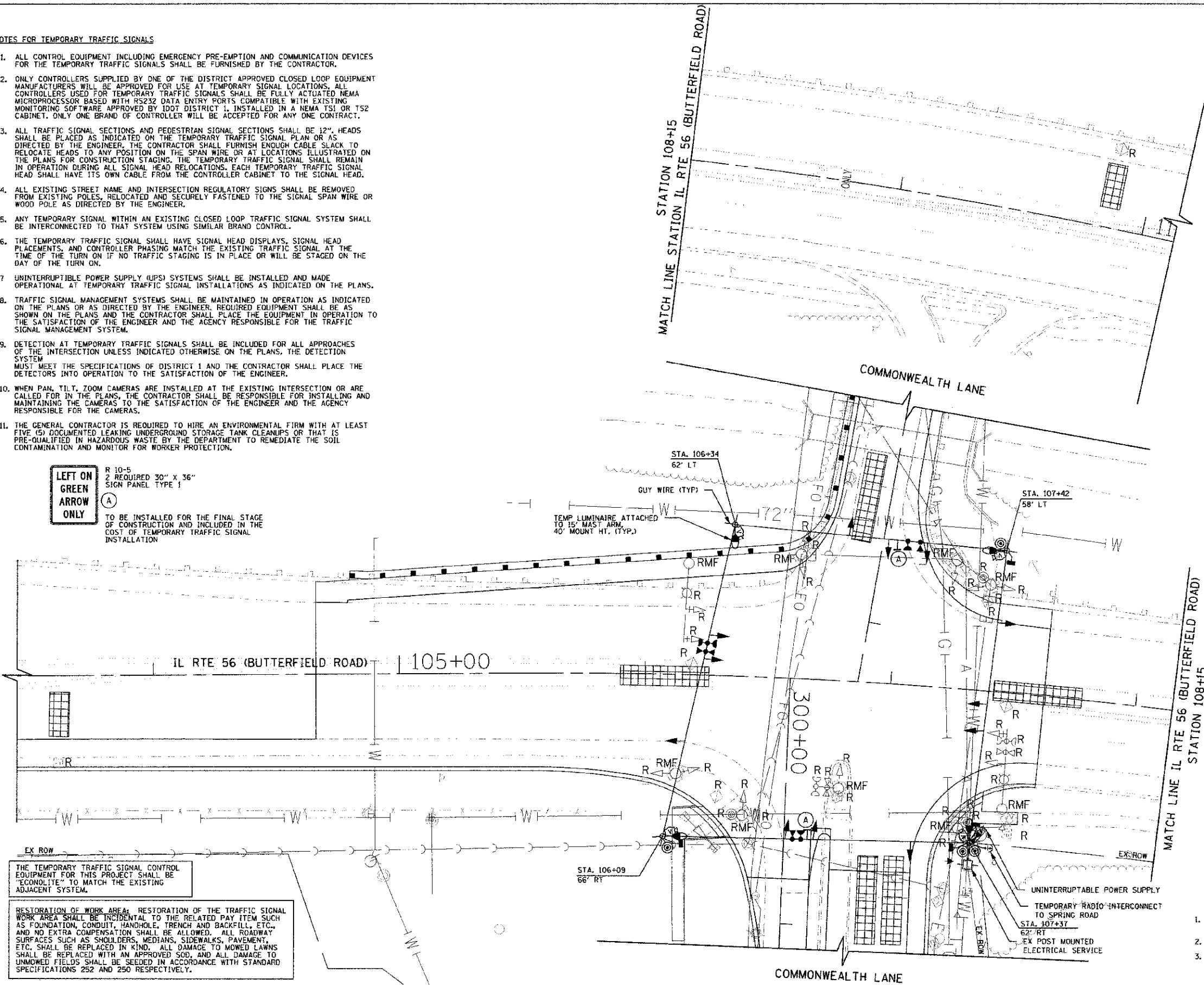
THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE AGENCY LISTED BELOW. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE DELIVERY OF ALL EQUIPMENT TO THE LISTED AGENCY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

AGENCY: CITY OF ELMHURST  
DELIVER EQUIPMENT TO:  
CITY OF ELMHURST  
DEPT. OF PUBLIC WORKS  
985 S. RIVERSIDE DRIVE  
ELMHURST, IL 60126

- 1 EACH CONTROLLER AND CABINET (COMPLETE)
- 9 EACH INDUCTIVE LOOP AMPLIFIERS
- 2 EACH STEEL COMBINATION MAST ARM ASSEMBLY AND POLE
- 7 EACH SIGNAL POSTS
- 6 EACH SIGNAL HEAD, 1-FACE, 3-SECTION
- 2 EACH SIGNAL HEAD, 1-FACE, 5-SECTION
- 1 EACH SIGNAL HEAD, 2-FACE, 2-3 SECTION
- 2 EACH SIGNAL HEAD, 2-FACE, 1-3 SECTION, 1-5 SECTION
- 2 EACH PEDESTRIAN SIGNAL HEAD, 1-FACE
- 1 EACH PEDESTRIAN SIGNAL HEAD, 2-FACE
- 3 EACH PEDESTRIAN PUSH-BUTTON
- 2 EACH LIGHT DETECTOR AND BEACON
- 1 EACH LIGHT DETECTOR AMPLIFIER
- 1 EACH SERVICE INSTALLATION
- 2 EACH LUMINAIRE



TO BE INSTALLED FOR THE FINAL STAGE OF CONSTRUCTION AND INCLUDED IN THE COST OF TEMPORARY TRAFFIC SIGNAL INSTALLATION



**TEMPORARY LIGHTING WIRING PLAN**  
NOT TO SCALE

**TEMPORARY LIGHTING NOTES**

- 1. A 15' MAST ARM SHALL BE MOUNTED ON EACH TEMPORARY TRAFFIC SIGNAL WOOD POLE AS SHOWN ON THE PLAN, AND SHALL BE INCLUDED IN THE COST FOR THE TEMPORARY TRAFFIC SIGNAL INSTALLATION.
- 2. PROVIDE 15 AMP, 120V, CIRCUIT BREAKER IN TEMPORARY TRAFFIC SIGNAL CONTROLLER.
- 3. THE AERIAL CABLE AND TEMPORARY LUMINAIRES SHALL BE REMOVED ONCE THE PERMANENT LIGHTING CABLE AND CONDUIT IS INSTALLED AND OPERATIONAL. REMOVAL OF THE AERIAL CABLE AND TEMPORARY LUMINAIRE SHALL BE INCLUDED IN THE COST FOR THE TEMPORARY TRAFFIC SIGNAL INSTALLATION.

THE TEMPORARY TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

RESTORATION OF WORK AREA: RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOB, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ILLINOIS ROUTE 56 (BUTTERFIELD ROAD) AT COMMONWEALTH LANE  
TEMPORARY TRAFFIC SIGNAL INSTALLATION AND REMOVAL PLAN**

SCALE: 1"=20' SHEET NO. 06 OF 05

F.A.D. RTE.	SECTION	COUNTY	TOTAL SHEETS	NO.
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FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63742	

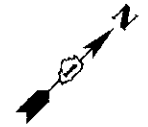
DATE	BY

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DATE PLOTTED: [Blank]  
PLOT DATE: [Blank]



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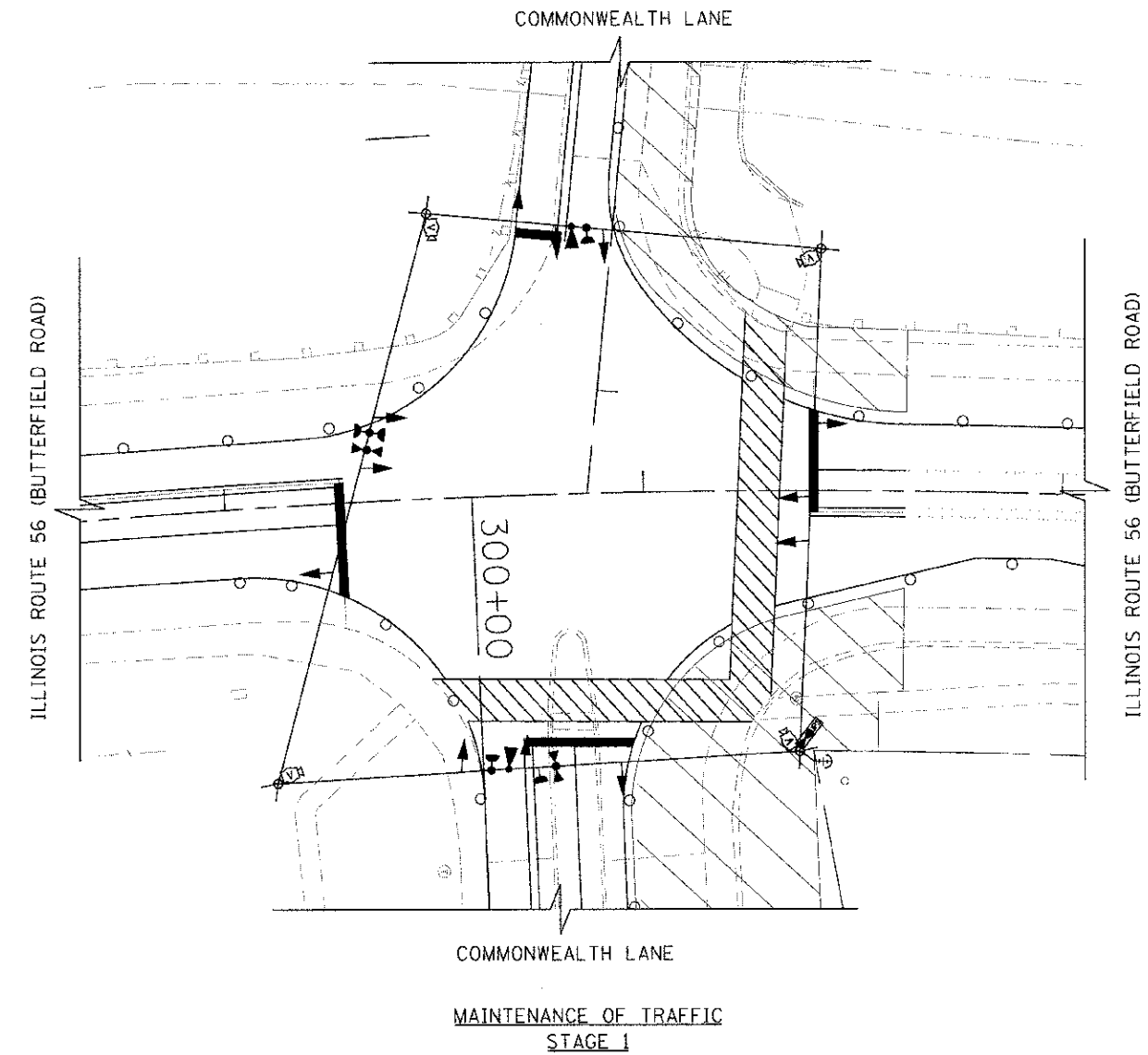
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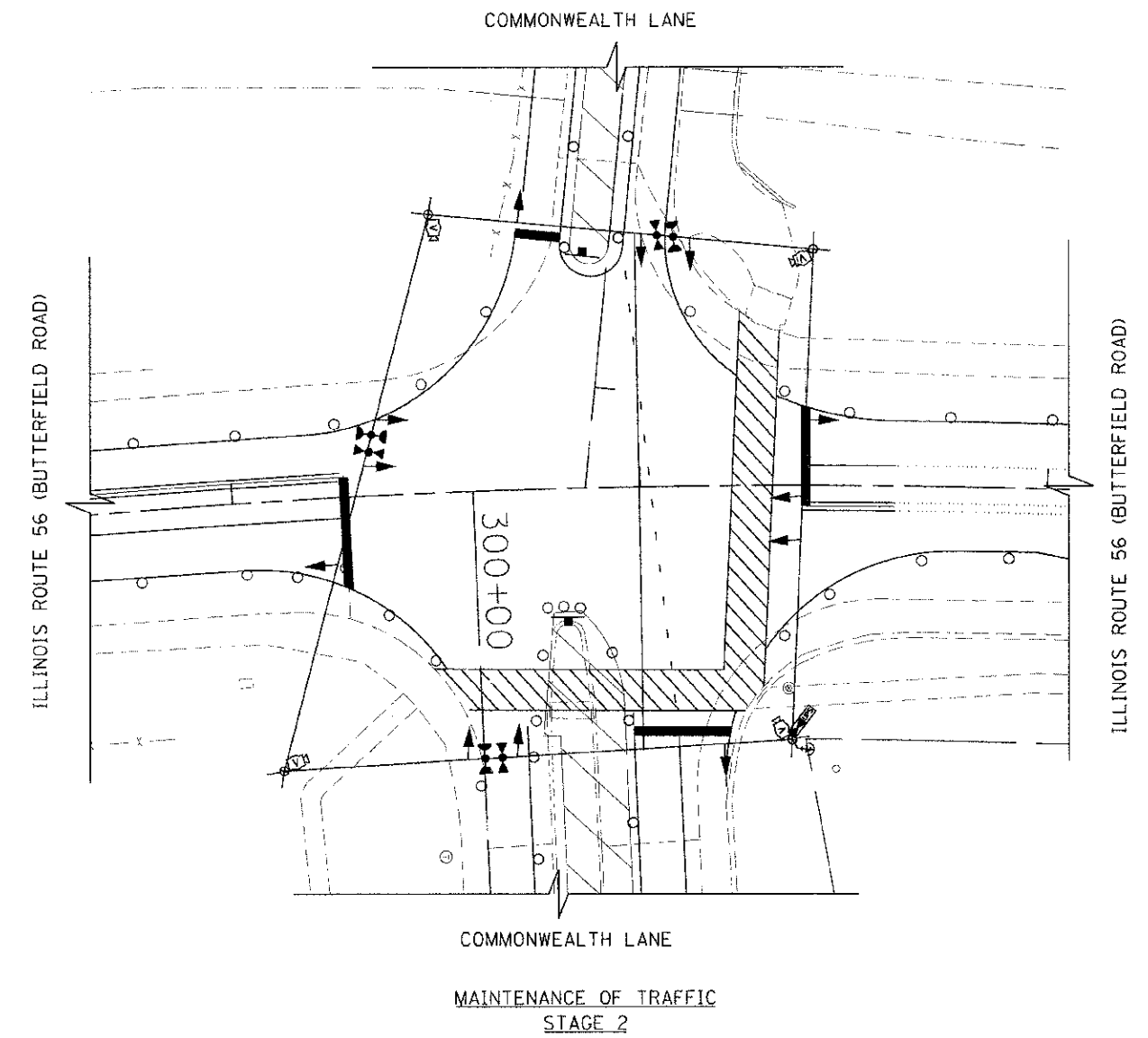
THE CONTRACTOR SHALL BE RESPONSIBLE FOR PERFORMING ALL NECESSARY EQUIPMENT AND PHASING CHANGES TO THE INSTALLATION TO COVER THE DIFFERENT SIGNAL HEAD REQUIREMENTS AND SIGNAL PHASING. THE COST OF THIS WORK WILL BE INCLUDED IN THE COST OF TEMPORARY TRAFFIC SIGNAL INSTALLATION.

PLAN	DATE
BY	
REVISIONS	
NO.	DATE
1	
2	
3	
4	
5	
6	
7	
8	
9	
10	

PROFILE	DATE
BY	
REVISIONS	
NO.	DATE
1	
2	
3	
4	
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10	



MAINTENANCE OF TRAFFIC  
STAGE 1



MAINTENANCE OF TRAFFIC  
STAGE 2

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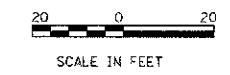


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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

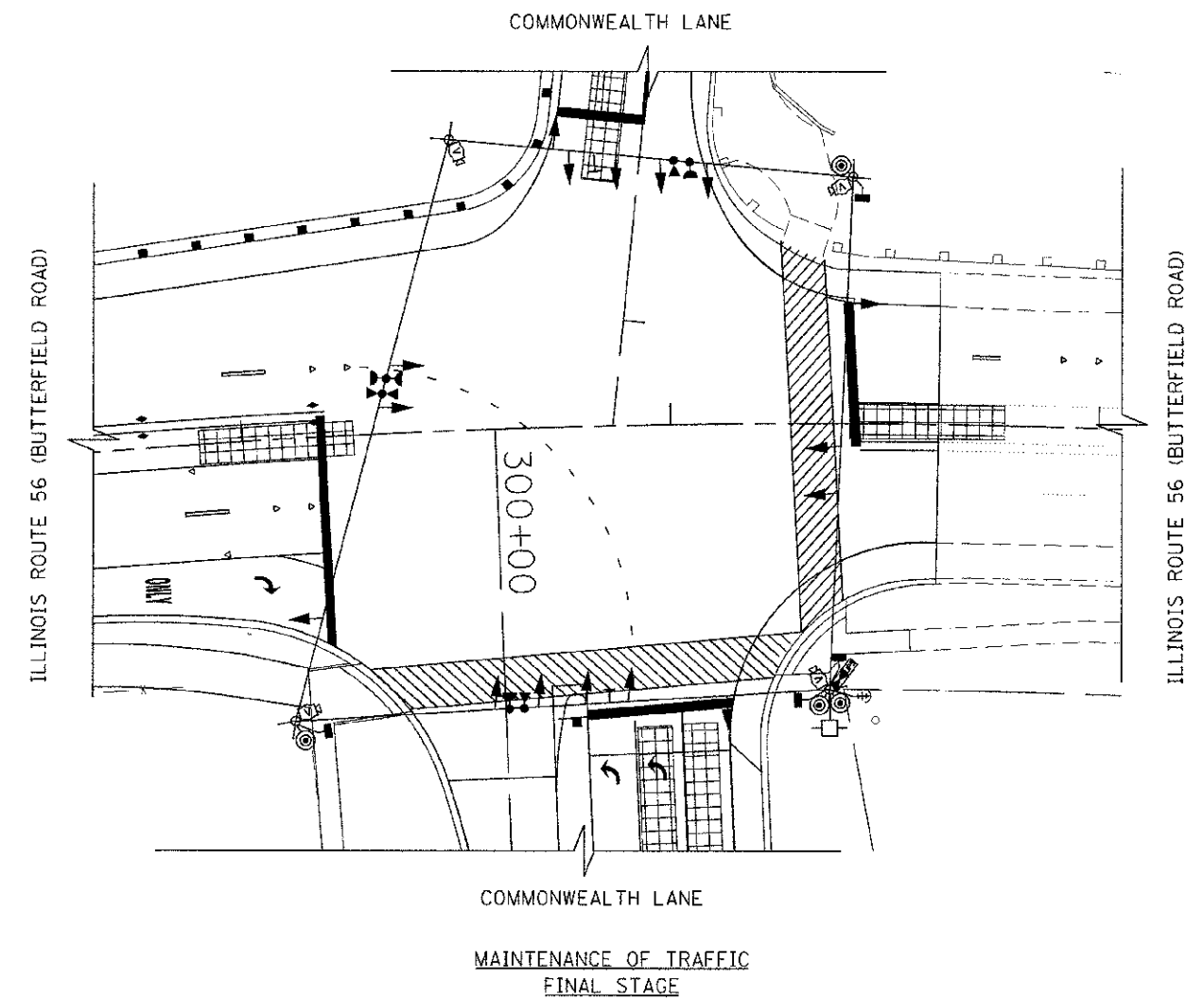
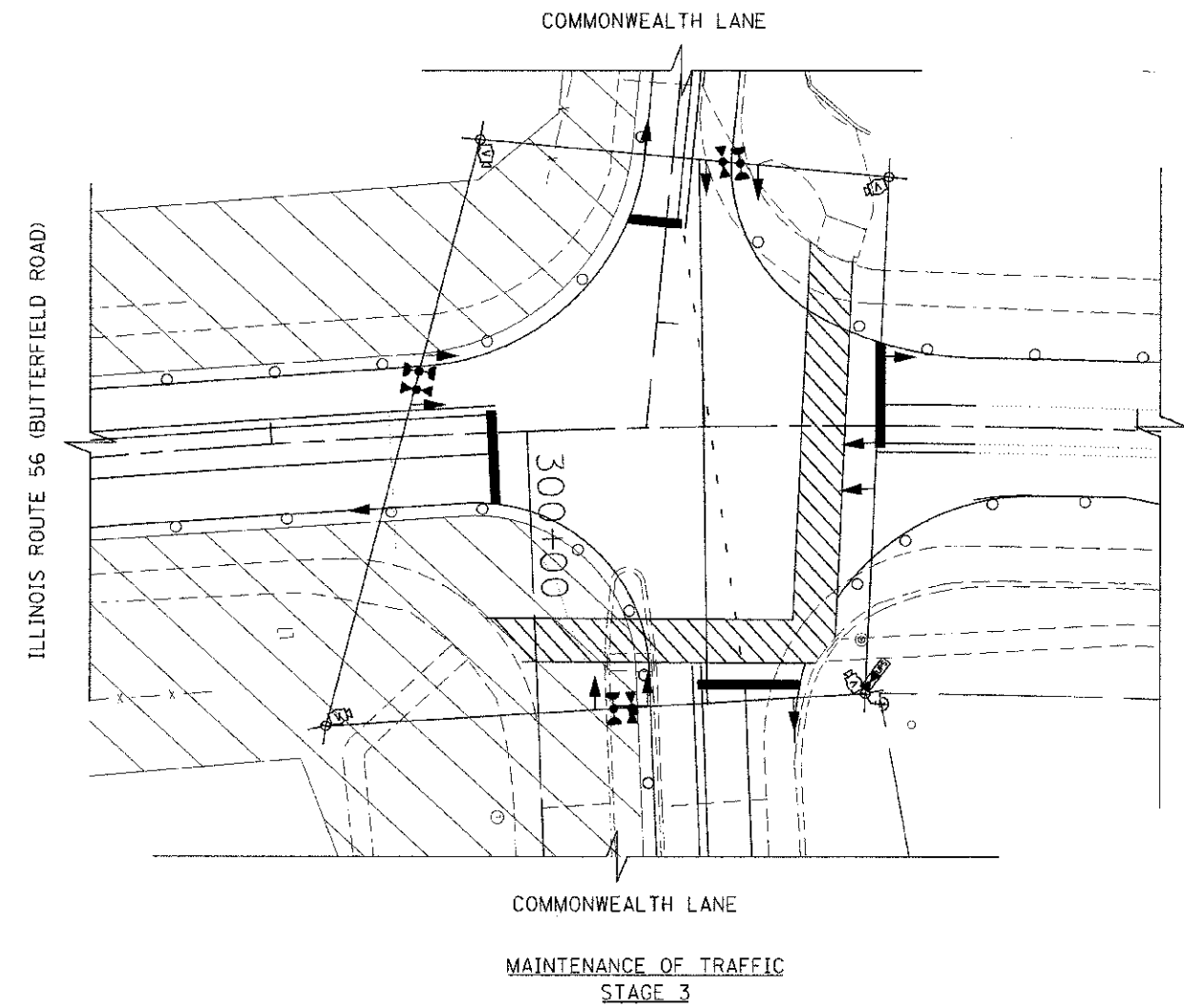
ILLINOIS ROUTE 56 (BUTTERFIELD ROAD) AT COMMONWEALTH LANE  
TEMPORARY TRAFFIC SIGNAL M.O.T. STAGING PLAN (1 of 2)  
SCALE: 1"=20' SHEET NO. 44 OF 108 SHEETS

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	09-00170-00-CH	DUPAGE	108	44
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63742	



THE CONTRACTOR SHALL BE RESPONSIBLE FOR PERFORMING ALL NECESSARY EQUIPMENT AND PHASING CHANGES TO THE INSTALLATION TO COVER THE DIFFERENT SIGNAL HEAD REQUIREMENTS AND SIGNAL PHASING. THE COST OF THIS WORK WILL BE INCLUDED IN THE COST OF TEMPORARY TRAFFIC SIGNAL INSTALLATION.

PLAN	DATE
BY	
REVISIONS	
NO.	
DESCRIPTION	
DATE	



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PLT DATE = 11/6/2012	DATE - 10/22/2012	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ILLINOIS ROUTE 56 (BUTTERFIELD ROAD) AT COMMONWEALTH LANE  
TEMPORARY TRAFFIC SIGNAL M.O.T. STAGING PLAN (2 of 2)**

SCALE: 1"=20' SHEET NO. 45 OF 108 SHEETS

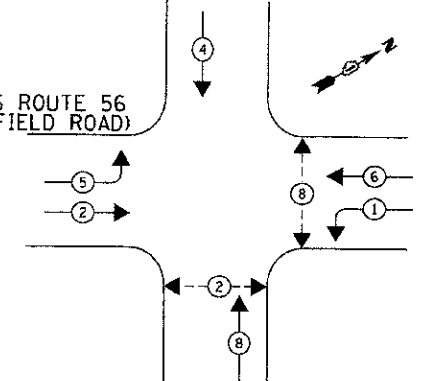
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FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63742	

**NOTES FOR TEMPORARY TRAFFIC SIGNALS**

- ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
- ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT I, INSTALLED IN A NEMA TS1 OR TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
- ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE LED AND 12" (300mm) DIAMETER. HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. PEDESTRIAN SIGNALS SHALL INCLUDE SOLID INTERNATIONAL SYMBOLS. PEDESTRIAN SIGNALS WITH COUNTDOWN TIMERS SHALL BE USED WHEN THE EXISTING INSTALLATION UTILIZES COUNTDOWN TYPE OR AS DIRECTED BY THE ENGINEER. COUNTDOWN TYPE PEDESTRIAN SIGNALS ARE NOT TO BE INSTALLED AT A RAILROAD INTERSECTION. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
- ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES AND RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
- ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
- THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
- UNINTERRUPTIBLE POWER SUPPLY (UPS) SYSTEMS SHALL BE INSTALLED AND MADE OPERATIONAL AT TEMPORARY TRAFFIC SIGNAL INSTALLATIONS WHERE UPS IS INSTALLED AT THE EXISTING TRAFFIC SIGNAL. TEMPORARY TRAFFIC SIGNALS AT RAILROAD INTERSECTIONS, AND TEMPORARY TRAFFIC SIGNALS AT INTERSECTIONS WITH FIRE STATION ACTUATED EMERGENCY VEHICLE PRE-EMPTION, OR WHEN INDICATED ON THE PLANS.
- TRAFFIC SIGNAL MANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
- DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT I AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER.
- WHEN PAN, TILT, ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.

**TEMPORARY CONTROLLER SEQUENCE STAGE 1, 2 AND 3 COMMONWEALTH LANE**

ILLINOIS ROUTE 56 (BUTTERFIELD ROAD)



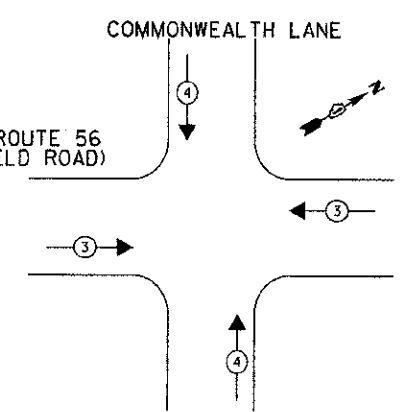
**PHASE DESIGNATION DIAGRAM**

**LEGEND**

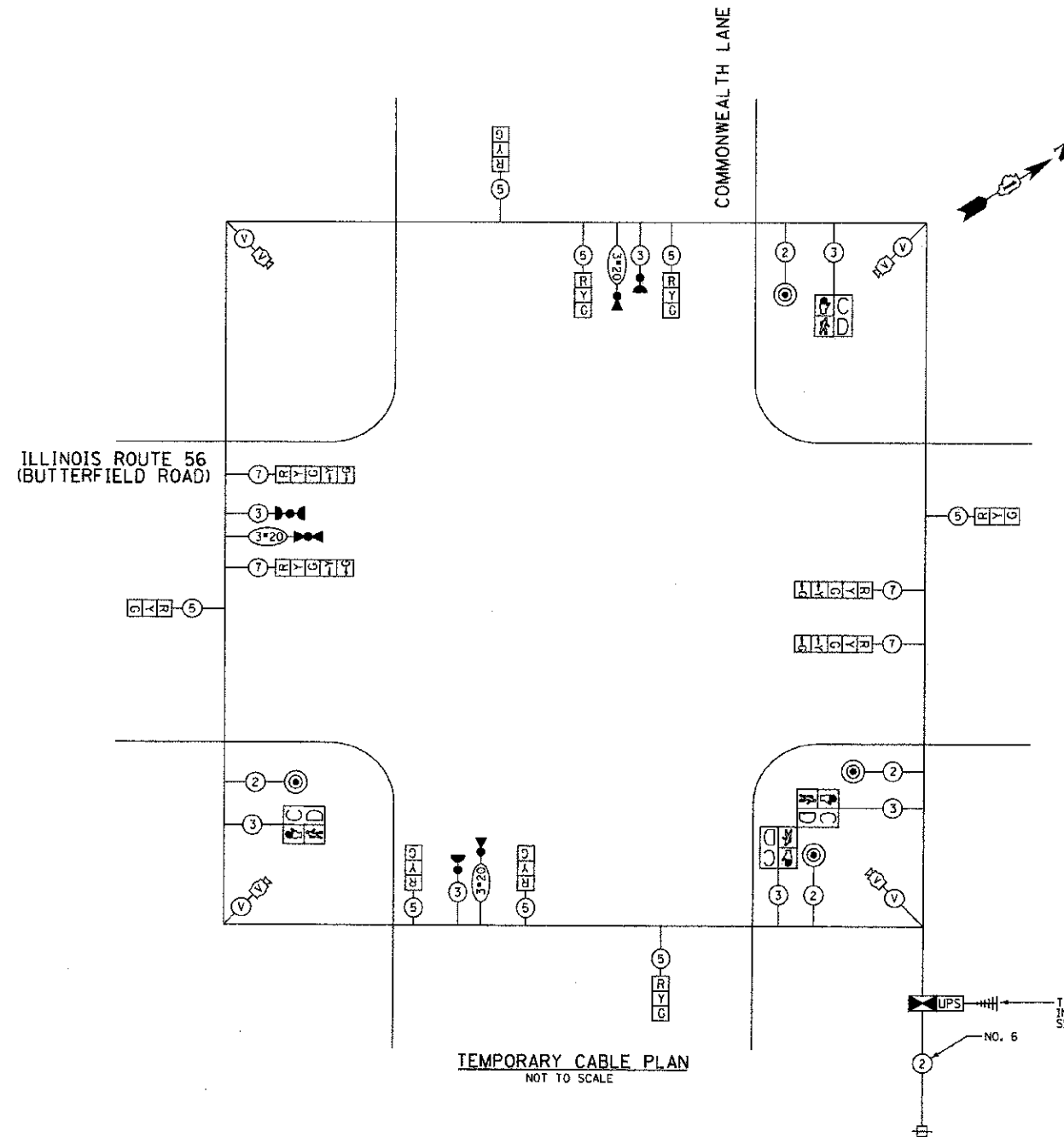
- ← \* → DUAL ENTRY PHASE
- ← \* → PEDESTRIAN PHASE
- ← \* → SINGLE ENTRY PHASE
- ← \* OL → OVERLAP
- \* NUMBER REFERS TO ASSOCIATED PHASE

**TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE STAGE 1, 2 AND 3 COMMONWEALTH LANE**

ILLINOIS ROUTE 56 (BUTTERFIELD ROAD)



TEMPORARY EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	← →	↑ ↓



**TEMPORARY CABLE PLAN**  
NOT TO SCALE

THE TEMPORARY TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH ADJACENT SYSTEM.

RESTORATION OF WORK AREA: RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOO, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE (INCANDESCENT)	WATTAGE (LED)	x % OPERATION	
SIGNAL (RED)	12	135	17	0.50	102
(YELLOW)	12	135	25	0.25	75
(GREEN)	12	135	15	0.25	45
ARROW	8	135	12	0.10	10
PED. SIGNAL	4	90	25	1.00	100
CONTROLLER	1	100	100	1.00	100
ILLUM. SIGN	0	64		0.05	0
FLASHER	0			0.50	0
<b>TOTAL =</b>					<b>432</b>

ENERGY COSTS TO:  
CITY OF ELMHURST  
209 N. YORK STREET  
ELMHURST, IL 60126

ENERGY SUPPLY CONTACT: DEB RANKIN  
PHONE: (630) 691-4379  
COMPANY: COMMONWEALTH EDISON COMPANY



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DATE - 10/22/2012

REVISED - 10/10/12  
REVISED -  
REVISED -  
REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**ILLINOIS ROUTE 56 (BUTTERFIELD ROAD) AT COMMONWEALTH LANE**  
**STAGES 1, 2 AND 3 - TEMPORARY CABLE PLAN**  
**PHASE DESIGNATION DIAGRAM AND SEQUENCE OF OPERATIONS**

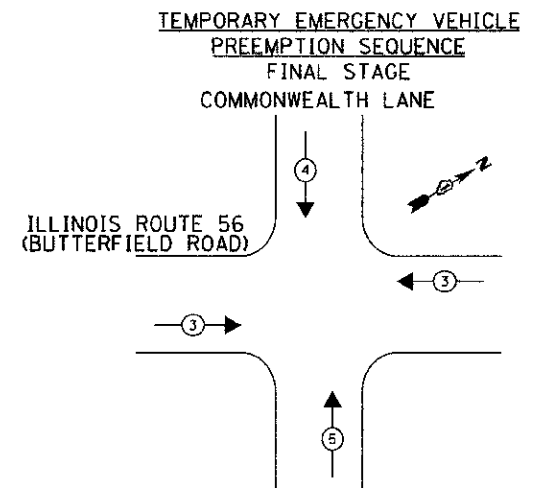
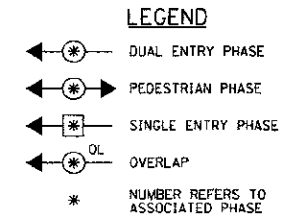
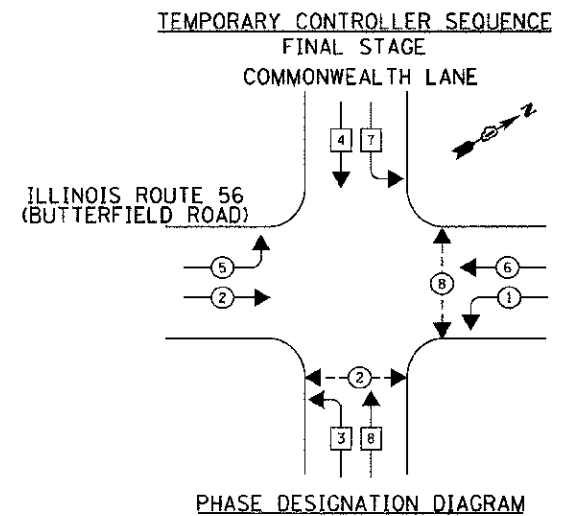
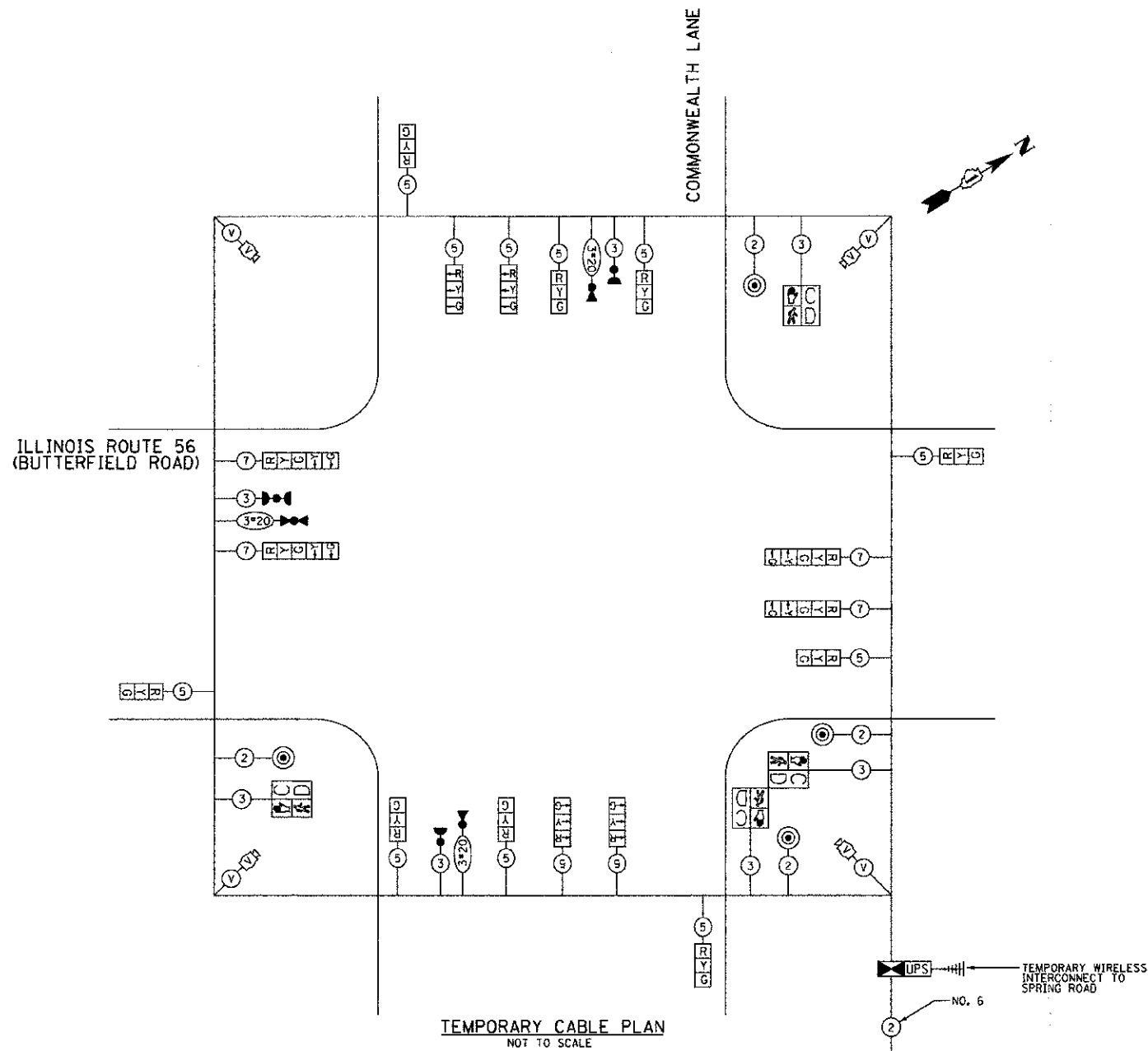
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SCALE: NONE			SHEET NO. 46 OF 108 SHEETS	
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

CONTRACT NO. 63742

PLAN	REVISION	DATE
NOTED		
PLANTED		
REVISIONS		
NO. OF WAYS CHECKED		
DATE FILED		
NO.		

**NOTES FOR TEMPORARY TRAFFIC SIGNALS**

- ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
- ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1. INSTALLED IN A NEMA TS1 OR TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
- ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE LED AND 12" (300mm) DIAMETER. HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. PEDESTRIAN SIGNALS SHALL INCLUDE SOLID INTERNATIONAL SYMBOLS. PEDESTRIAN SIGNALS WITH COUNTDOWN TIMERS SHALL BE USED WHEN THE EXISTING INSTALLATION UTILIZES COUNTDOWN TYPE OR AS DIRECTED BY THE ENGINEER. COUNTDOWN TYPE PEDESTRIAN SIGNALS ARE NOT TO BE INSTALLED AT A RAILROAD INTERSECTION. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
- ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES AND RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
- ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
- THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
- UNINTERRUPTIBLE POWER SUPPLY (UPS) SYSTEMS SHALL BE INSTALLED AND MADE OPERATIONAL AT TEMPORARY TRAFFIC SIGNAL INSTALLATIONS WHERE UPS IS INSTALLED AT THE EXISTING TRAFFIC SIGNAL. TEMPORARY TRAFFIC SIGNALS AT RAILROAD INTERSECTIONS, AND TEMPORARY TRAFFIC SIGNALS AT INTERSECTIONS WITH FIRE STATION ACTUATED EMERGENCY VEHICLE PRE-EMPTION, OR WHEN INDICATED ON THE PLANS.
- TRAFFIC SIGNAL MANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
- DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER.
- WHEN PAN, TILT, ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.



PROPOSED EMERGENCY VEHICLE PREEMPTORS			
EMERGENCY VEHICLE PREEMPTOR	3	4	5
MOVEMENT	↔	↓	↗

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE (INDICATED)		% OPERATION	
SIGNAL (RED)	17	135	17	0.50	145
(YELLOW)	17	135	25	0.25	106
(GREEN)	17	135	15	0.25	64
ARROW	8	135	12	0.10	10
PED. SIGNAL	4	90	25	1.00	100
CONTROLLER	1	100	100	1.00	100
ILLUM. SIGN	0	64		0.05	0
TOTAL					525

ENERGY COSTS TO:  
 CITY OF ELMHURST  
 209 N. YORK STREET  
 ELMHURST, IL 60126

ENERGY SUPPLY CONTACT: DEB RANKIN  
 PHONE: 630 691-4379  
 COMPANY: COMMONWEALTH EDISON COMPANY

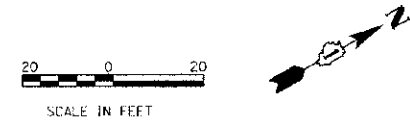
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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

ILLINOIS ROUTE 56 (BUTTERFIELD ROAD) AT COMMONWEALTH LANE  
 FINAL STAGE - TEMPORARY CABLE PLAN  
 PHASE DESIGNATION DIAGRAM AND SEQUENCE OF OPERATIONS  
 SCALE: NONE SHEET NO. 47 OF 108 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	09-C0170-00-CH	DUPAGE	108	47
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63742	

- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARM LENGTHS.
- THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES. THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811, IN THE CITY OF CHICAGO CONTACT DIGGER AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION REQUIRED).
- THE CONTRACTOR SHALL CHECK THE PROPOSED TRAFFIC SIGNAL EQUIPMENT LOCATIONS FOR OVERHEAD UTILITY CONFLICTS. THE CONTRACTOR SHALL COORDINATE ANY CONFLICTS WITH THE UTILITY COMPANIES AND THE RESIDENT ENGINEER BEFORE ORDERING MATERIALS.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, LOCAL GOVERNMENT AGENCIES AND IDOT
- THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.

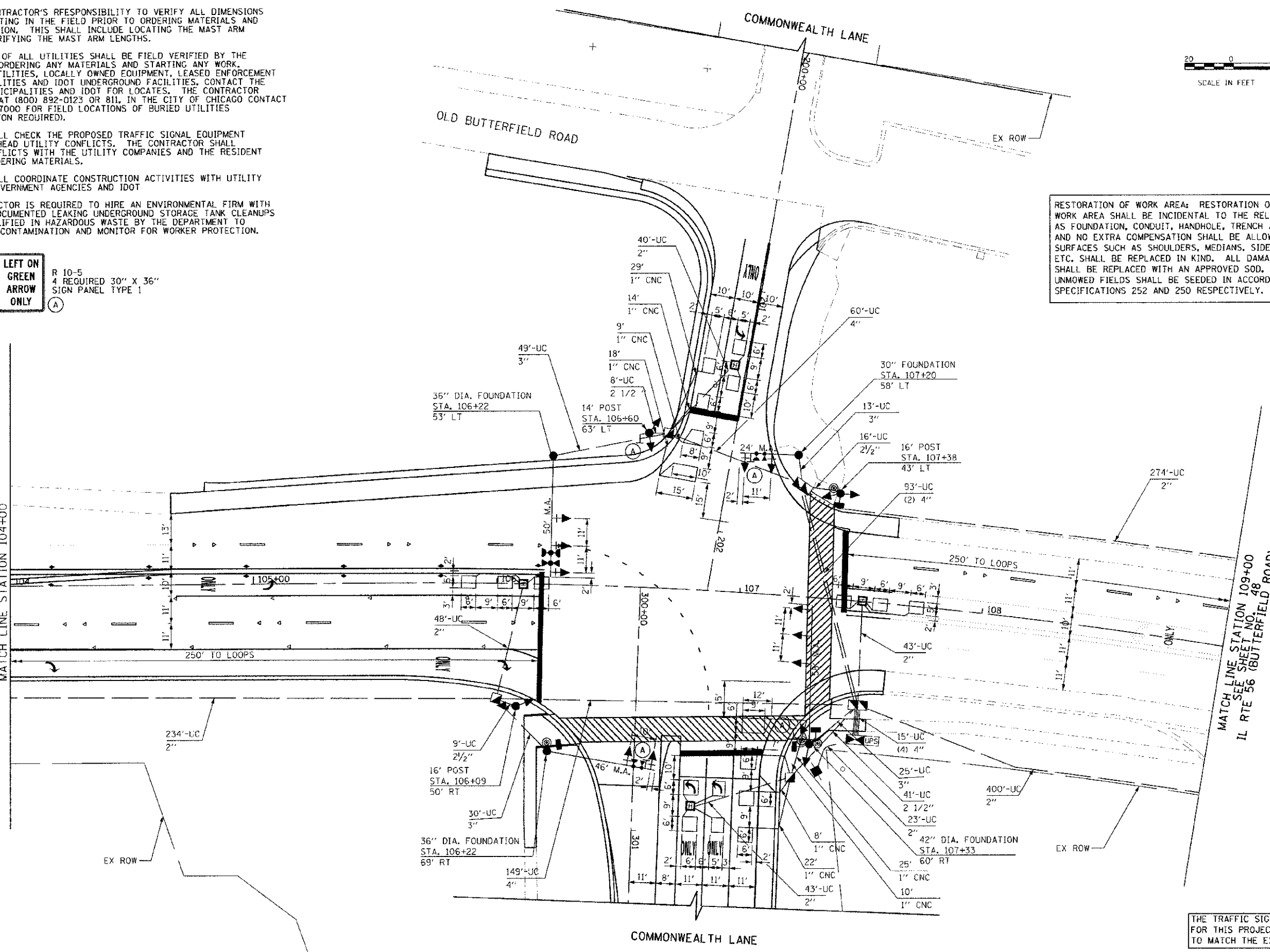


RESTORATION OF WORK AREA: RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

**LEFT ON GREEN ARROW ONLY**  
 R 10-5  
 4 REQUIRED 30" X 36"  
 SIGN PANEL TYPE 1  
 (A)

IL RTE 56 (BUTTERFIELD ROAD)  
 SEE SHEET NO. 48  
 MATCH LINE STATION 104+00

MATCH LINE STATION 109+00  
 SEE SHEET NO. 48  
 IL RTE 56 (BUTTERFIELD ROAD)



THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

DATE	
BY	
PLANNED	
NOTED	
REVISIONS	
NO.	

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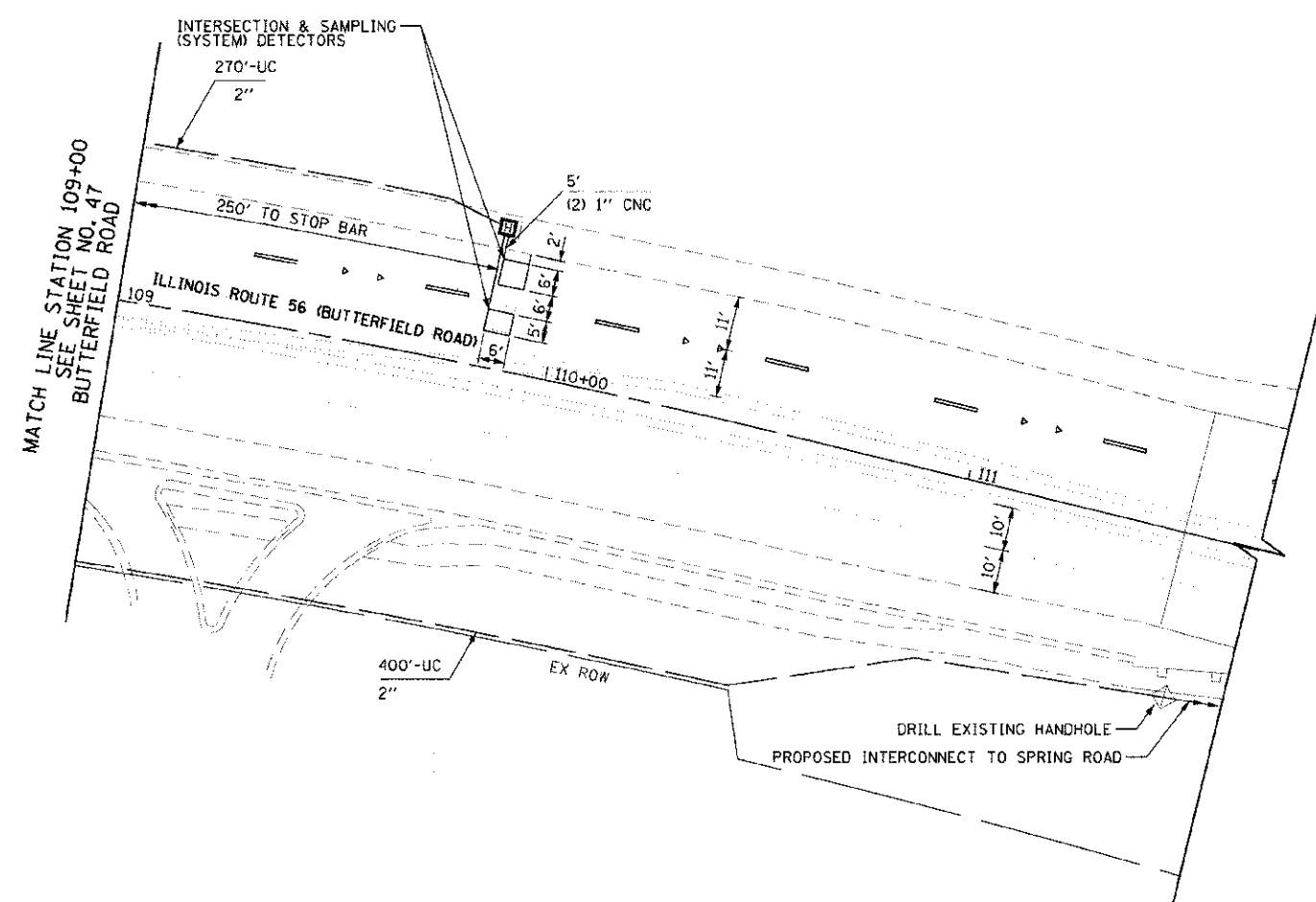
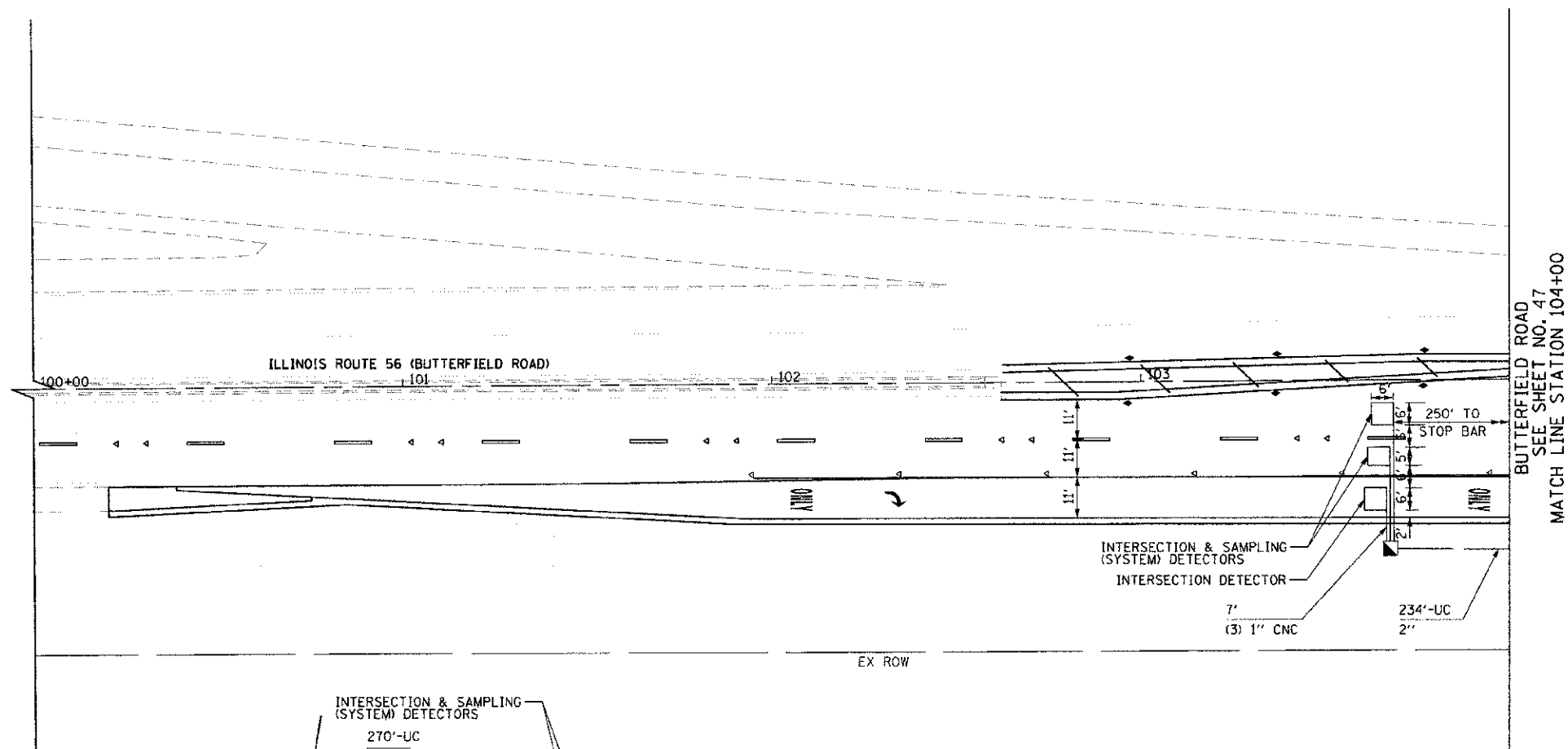
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

ILLINOIS ROUTE 56 (BUTTERFIELD ROAD) AT COMMONWEALTH LANE  
 PROPOSED TRAFFIC SIGNAL INSTALLATION PLAN (1 OF 2)  
 SCALE: 1"=20' SHEET NO. 48 OF 108 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	09-00170-00-CH	DUPAGE	108	48
CONTRACT NO. 63742			FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT	



PLAN	SUBMITTED	DATE
NOTE BOOK	PLOTTED	
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	DESIGNED	
	CHECKED	
	DATE	



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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ILLINOIS ROUTE 56 (BUTTERFIELD ROAD) AT COMMONWEALTH LANE  
PROPOSED TRAFFIC SIGNAL INSTALLATION PLAN (2 OF 2)

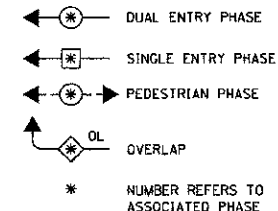
SCALE: 1"=20' SHEET NO. 49 OF 108 SHEETS

F.A.U. RTE. 3545	SECTION 09-00170-00-CH	COUNTY DUPAGE	TOTAL SHEETS 108	SHEET NO. 49
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63742	

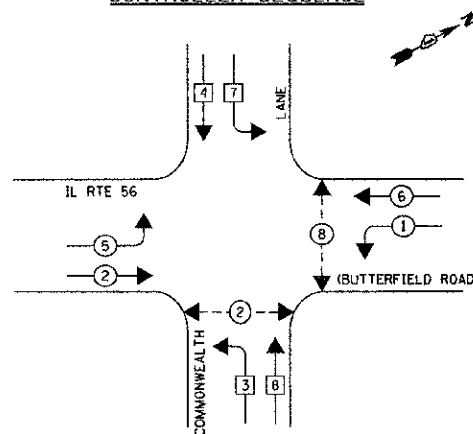
**SCHEDULE OF QUANTITIES**

QUANTITY	UNIT	ITEM
30	SQ. FT.	SIGN PANEL-TYPE 1
55	SQ. FT.	SIGN PANEL-TYPE 2
1105	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.
74	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.
117	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.
395	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.
4	EACH	HANDHOLE
5	EACH	HEAVY-DUTY HANDHOLE
2	EACH	DOUBLE HANDHOLE
1	EACH	FULL-ACTUATED DOUBLE DOOR CONTROLLER AND TYPE IV CABINET, SPECIAL
532	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C
1366	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
2816	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
853	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C
2383	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
558	FOOT	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C
1	EACH	TRAFFIC SIGNAL POST, 14 FT.
2	EACH	TRAFFIC SIGNAL POST, 16 FT.
1	EACH	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE, 24 FT.
1	EACH	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE, 46 FT.
1	EACH	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE, 50 FT.
1	EACH	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE, 58 FT.
12	FOOT	CONCRETE FOUNDATION, TYPE A
4	FOOT	CONCRETE FOUNDATION, TYPE C
10	FOOT	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER
28	FOOT	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER
21	FOOT	CONCRETE FOUNDATION, TYPE E 42-INCH DIAMETER
8	EACH	SIGNAL HEAD, L.E.D., 1-FACE, 3-SECTION, MAST ARM MOUNTED
2	EACH	SIGNAL HEAD, L.E.D., 1-FACE, 5-SECTION, MAST ARM MOUNTED
2	EACH	SIGNAL HEAD, L.E.D., 2-FACE, 3-SECTION, BRACKET MOUNTED
2	EACH	SIGNAL HEAD, L.E.D., 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED
2	EACH	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
1	EACH	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
10	EACH	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM
12	EACH	INDUCTIVE LOOP DETECTOR
851	FOOT	DETECTOR LOOP, TYPE 1
3	EACH	LIGHT DETECTOR
1	EACH	LIGHT DETECTOR AMPLIFIER
4	EACH	PEDESTRIAN PUSH-BUTTON
1	EACH	TEMPORARY TRAFFIC SIGNAL INSTALLATION
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
11	EACH	REMOVE EXISTING HANDHOLE
10	EACH	REMOVE EXISTING CONCRETE FOUNDATION
807	FOOT	EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3/C
1	EACH	SERVICE INSTALLATION- POLE MOUNTED
1	EACH	UNINTERRUPTABLE POWER SUPPLY, EXTENDED
1	EACH	TEMPORARY TRAFFIC SIGNAL TIMING

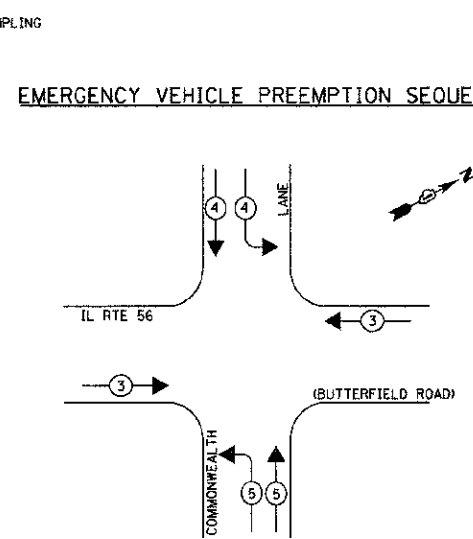
**LEGEND**



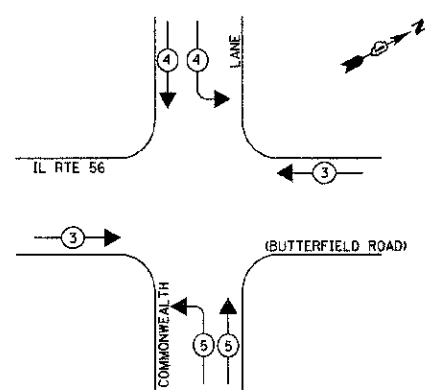
**CONTROLLER SEQUENCE**



**PHASE DESIGNATION DIAGRAM**



**EMERGENCY VEHICLE PREEMPTION SEQUENCE**

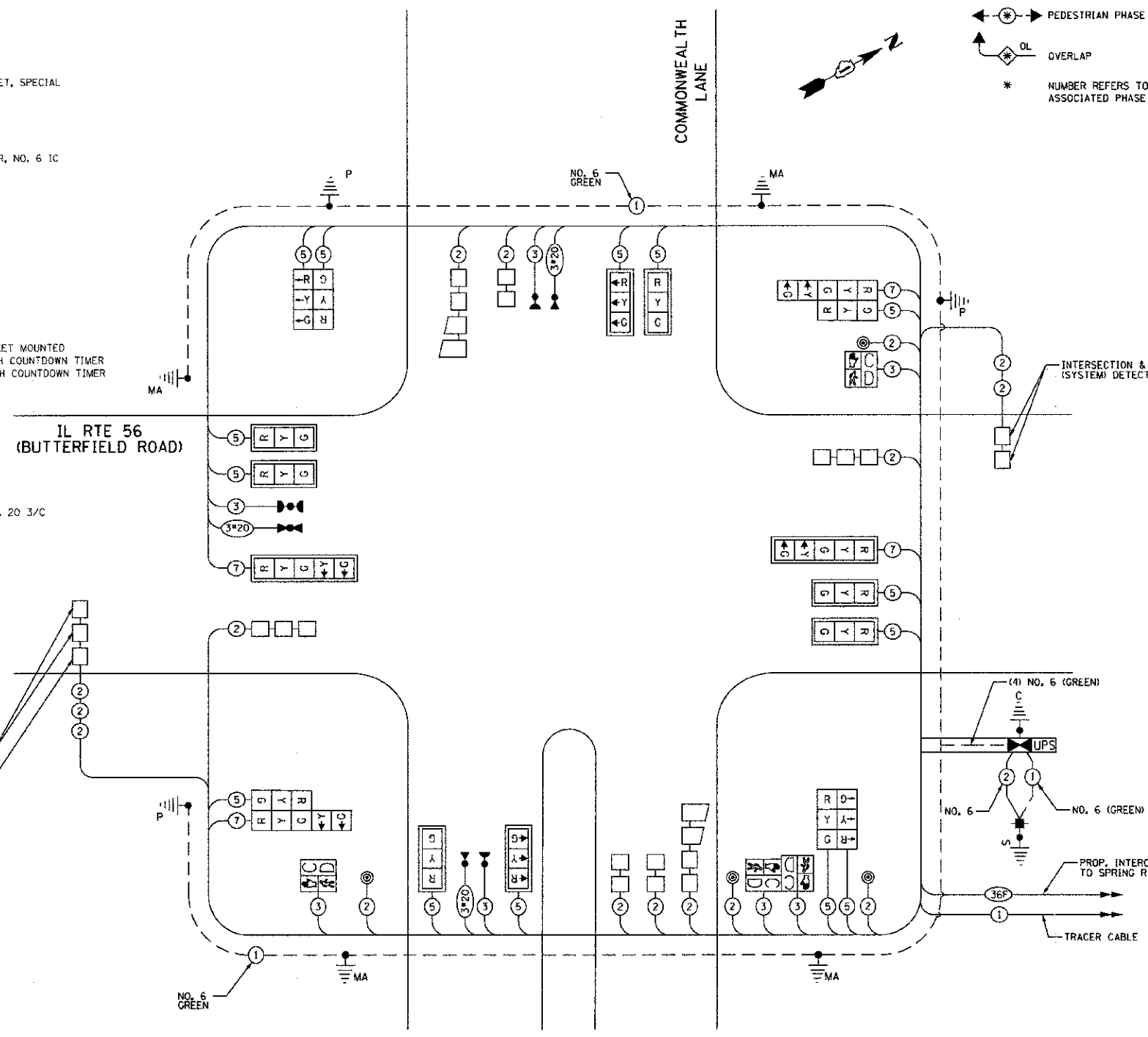


PROPOSED EMERGENCY VEHICLE PREEMPTORS			
EMERGENCY VEHICLE PREEMPTOR	3	4	5
MOVEMENT	↔	↕	↗

THE END OF THE TRACER CABLE SHALL BE CONTINUOUS AND EXTEND INTO THE CONTROLLER CABINET.

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

RESTORATION OF WORK AREA: RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



**CABLE PLAN**  
NOT TO SCALE

- NOTES:**
- ALL TRAFFIC SIGNAL HEADS SHALL BE ALL BLACK.
  - ALL OTHER TRAFFIC SIGNAL EQUIPMENT SHALL BE PAINTED ALPHA BRONZE IN ACCORDANCE WITH CITY OF ELMHURST STANDARDS.
  - ALL EVP EQUIPMENT SHALL BE OPTICOM.

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE		% OPERATIONS	
		INCAND.	LED		
SIGNAL (RED)	18	135	17	0.50	153
(YELLOW)	18	135	25	0.25	113
(GREEN)	18	135	15	0.25	68
ARROW	8	135	12	0.10	10
PED. SIGNAL	4	90	25	1.00	100
CONTROLLER	1	100	100	1.00	100
ILLUM. SIGN	0	100	90	1.00	0
EMERGENCY COSTS TO:					TOTAL = 544

CITY OF ELMHURST  
209 N. YORK STREET  
ELMHURST, IL 60126

EMERGENCY SUPPLY CONTACT: DEB RANKIN  
PHONE: (630) 691-4379  
COMPANY: COMMONWEALTH EDISON COMPANY

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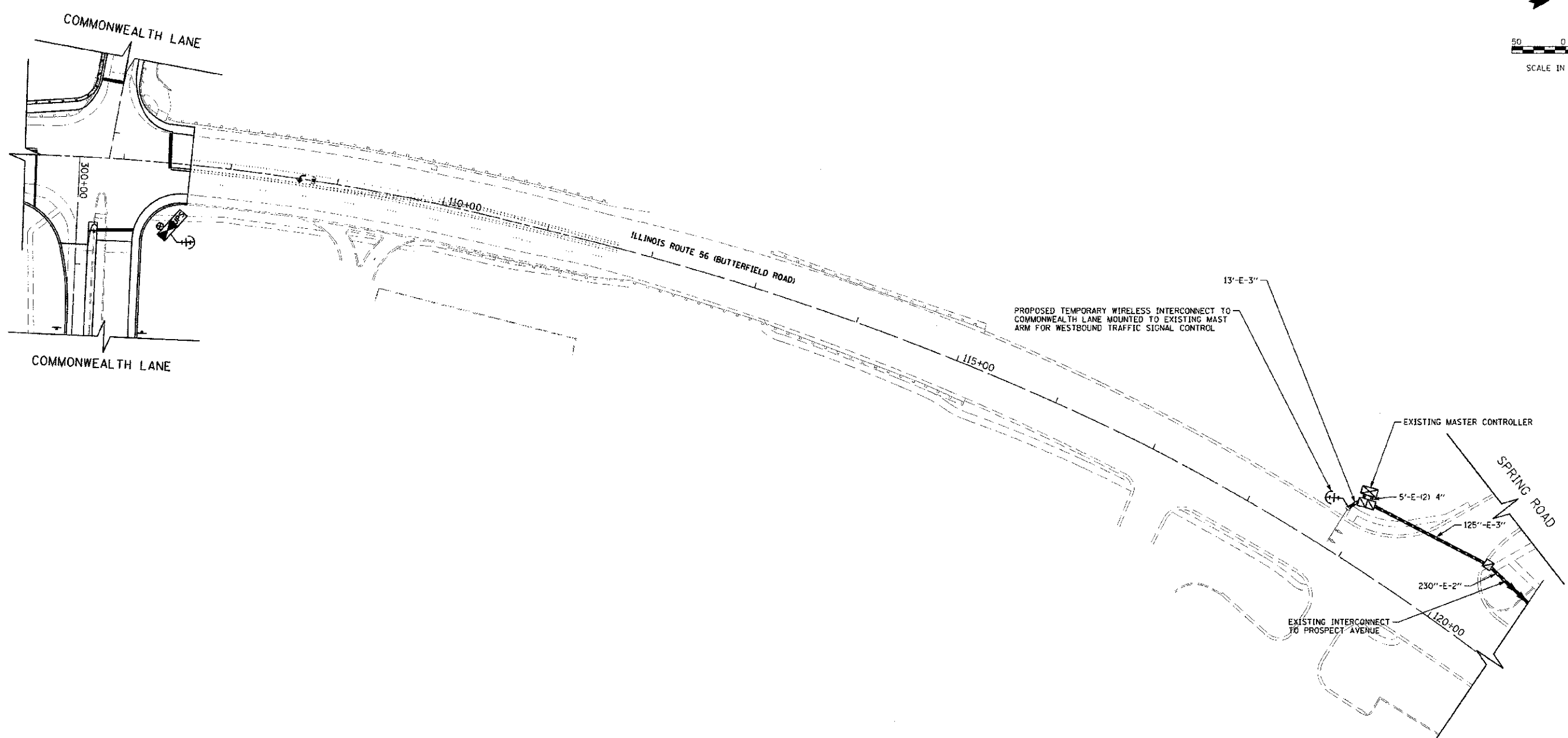
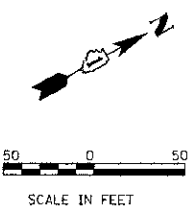
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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**ILLINOIS ROUTE 56 (BUTTERFIELD ROAD) AT COMMONWEALTH LANE**  
**PROPOSED TRAFFIC SIGNAL CABLE PLAN,**  
**SEQUENCE OF OPERATIONS AND SUMMARY OF QUANTITIES**

SCALE: NONE SHEET NO. 50 OF 108 SHEETS

F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	09-00170-00-CH	DUPAGE	108	50
CONTRACT NO. 63742				
FED. ROAD DIST. NO. 7 (ILLINOIS) FED. AID PROJECT				



PLAN	SUPPRESSED	DATE
NOTE BOOK	NO. OF SHEETS	
NO.	NO. OF SHEETS CHECKED	
	AND FILE NAME	

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THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.



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PLOT DATE - 11/6/2012	DATE - 10/22/2012	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

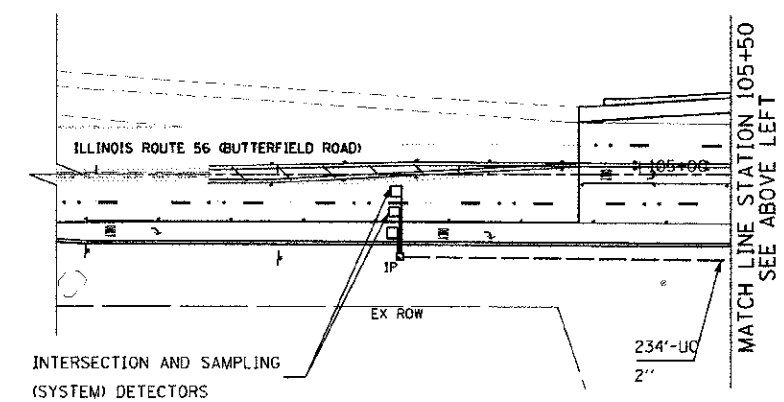
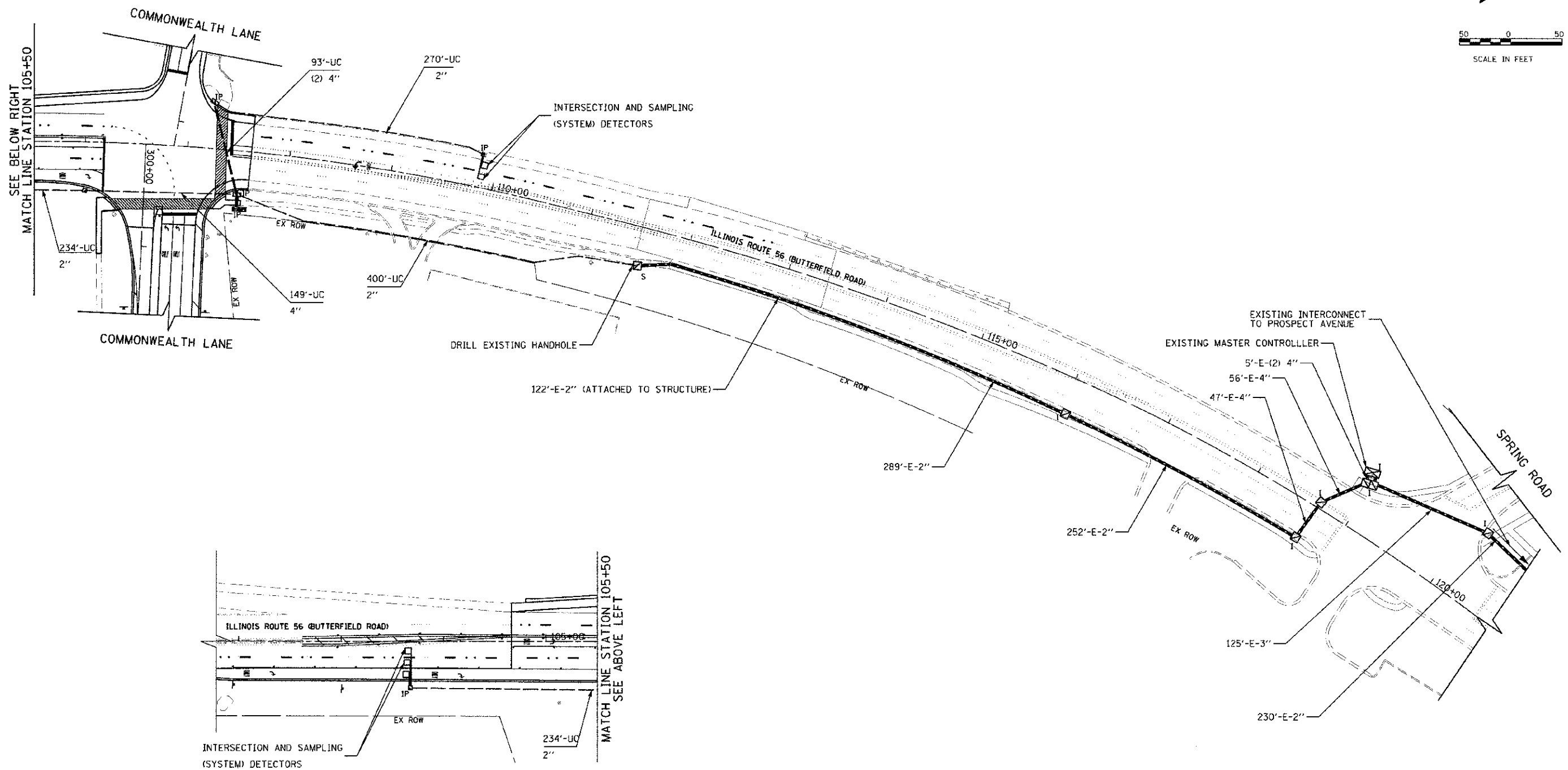
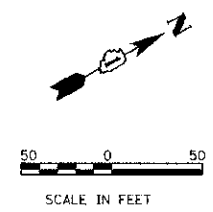
ILLINOIS ROUTE 56 (BUTTERFIELD ROAD) AT COMMONWEALTH LANE  
TEMPORARY INTERCONNECT PLAN

F.A.J. RTE. 3545	SECTION 09-00170-00-CH	COUNTY DU PAGE	TOTAL SHEETS 108	SHEET NO. 51
CONTRACT NO. 63742				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

SCALE: 1"=50' SHEET NO. 51 OF 108 SHEETS



PLAN	SURVEYED	DATE
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	DATE	
	ADD FILE NAME	



THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ILLINOIS ROUTE 56 (BUTTERFIELD RD) COMMONWEALTH LANE TO PROSPECT AVENUE INTERCONNECT PLAN	
SCALE: 1"=50'	SHEET NO. 53 OF 108 SHEETS

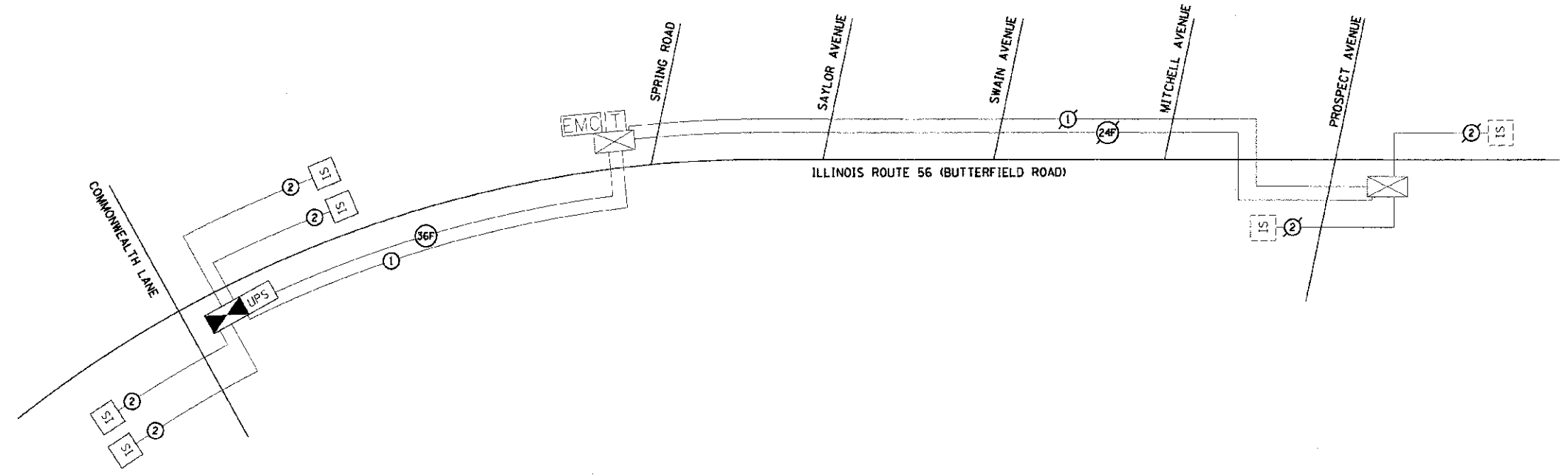
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3545	09-00170-00-CH	DUPAGE	108	53
CONTRACT NO. 63742			FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT	

INTERCONNECT SCHEDULE OF QUANTITIES

QUANTITY	UNIT	ITEM
400	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2"
1	EACH	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION
1	EACH	TRANSCEIVER - FIBER OPTIC
1	EACH	DRILL EXISTING HANDHOLE
1295	FOOT	REMOVE ELECTRIC CABLE FROM CONDUIT
1304	FOOT	FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125 MM12F & SM24F
1281	FOOT	ELECTRIC CABLE IN CONDUIT, TRACER NO. 14 1C
1	EACH	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 2



PLAN	DATE
BY	
REVISION	
DATE	
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REVISION	
DATE	
BY	
REVISION	
DATE	



THE TEMPORARY TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH ADJACENT SYSTEM.

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ILLINOIS ROUTE 56 (BUTTERFIELD ROAD) AT COMMONWEALTH LANE  
INTERCONNECT SCHEMATIC PLAN

F.A.J.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	09-00170-00-CH	DUPAGE	108	54
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63742	

SCALE: NONE SHEET NO. 54 OF 108 SHEETS

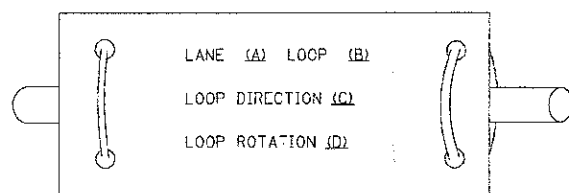




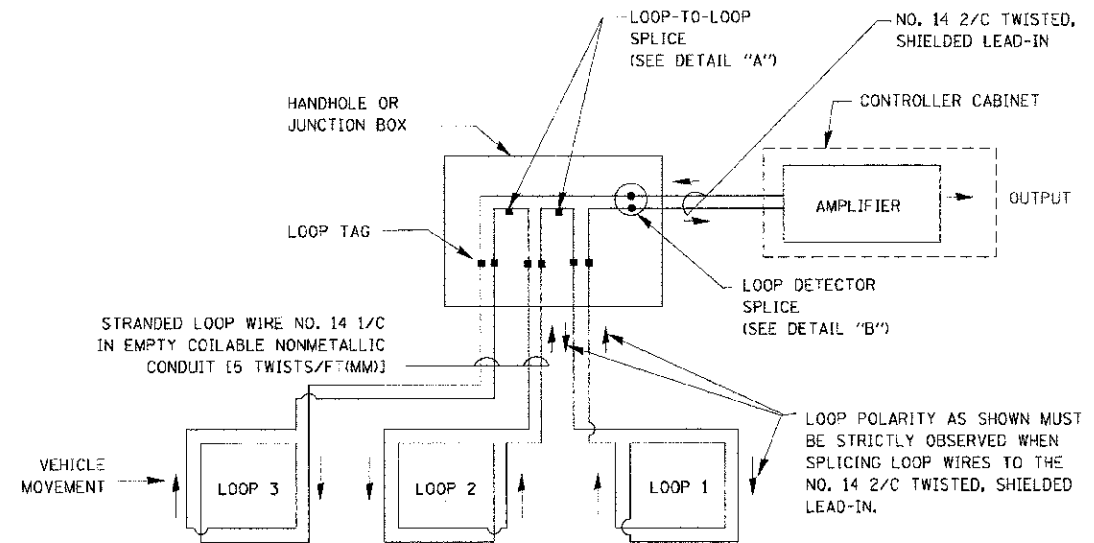
**LOOP DETECTOR NOTES**

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVESHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

**LOOP LEAD-IN CABLE TAG**

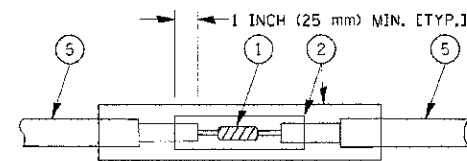


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

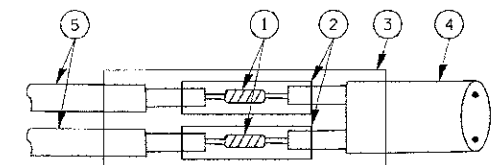


**DETECTOR LOOP WIRING SCHEMATIC**

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

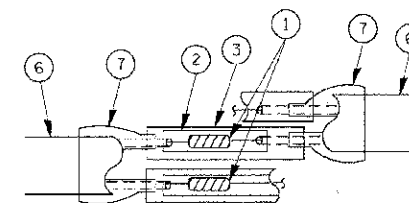


**DETAIL "A"  
LOOP-TO-LOOP SPLICE**

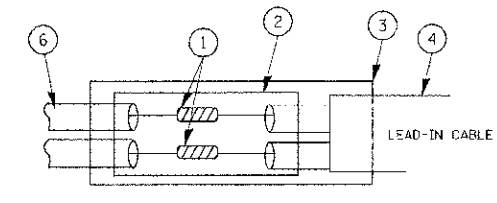


**DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE**

**TYPE I LOOP**



**DETAIL "A"  
LOOP-TO-LOOP SPLICE**



**DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE**

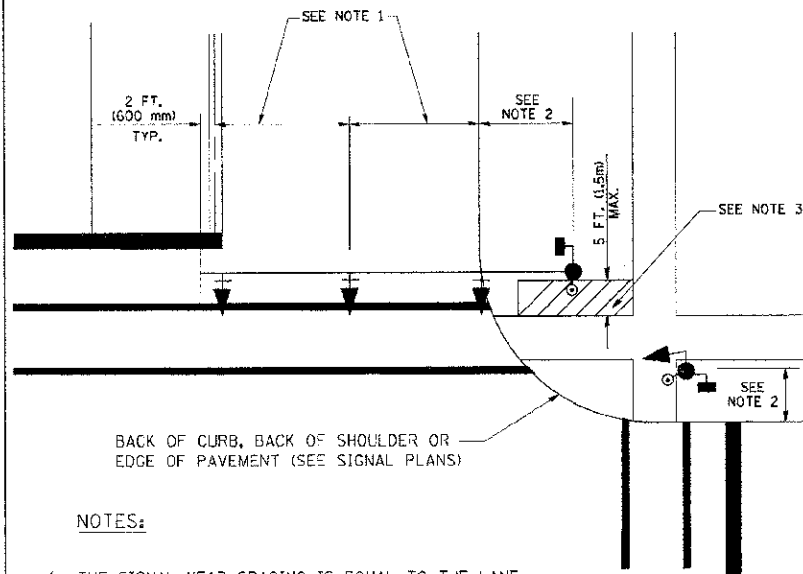
**LOOP DETECTOR SPLICE**

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = bauerdl	DESIGNED - DAD	REVISIONS -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS</b>		F.A.U. RTE. 3545	SECTION 09-00170-00-CH	COUNTY	TOTAL SHEETS 108	SHEET NO. 56
PROJECT WORKSPACE\PROJECT\BAUERDL\0108315\ts-05.dgn	PLOT SCALE = 5/8"=1' / IN.	DRAWN - BCK	REVISIONS -		SCALE: NONE	SHEET NO. 1 OF 6 SHEETS	STA. TO STA.	<b>TS-05</b>		CONTRACT NO. 63742	
	PLOT DATE = 11/4/2009	CHECKED - DAD	REVISIONS -		FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT						
		DATE - 10-28-09	REVISIONS -								

**TRAFFIC SIGNAL MAST ARM AND SIGNAL POST**

MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.

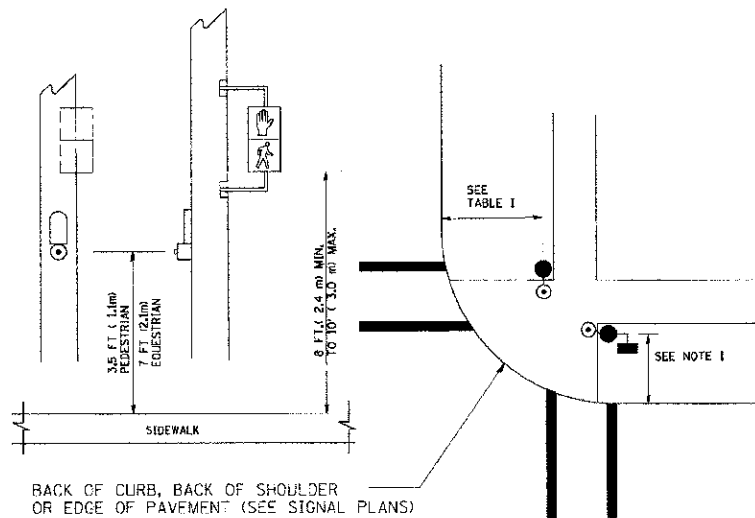


BACK OF CURB, BACK OF SHOULDER OR EDGE OF PAVEMENT (SEE SIGNAL PLANS)

**NOTES:**

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

**PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST**

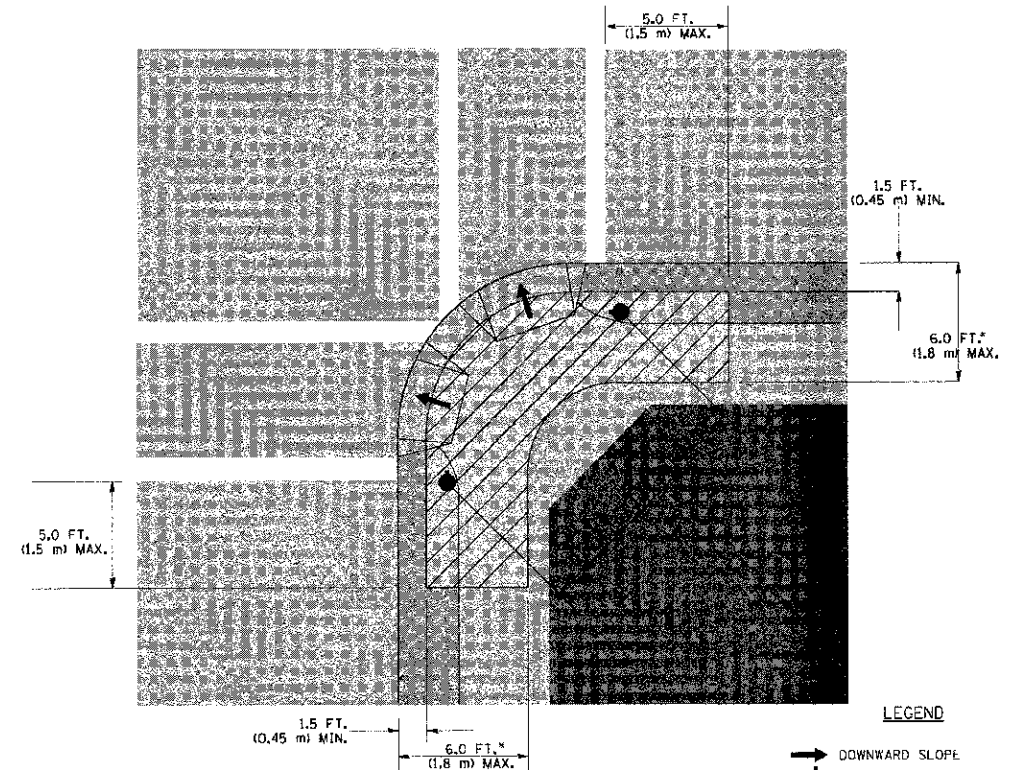


BACK OF CURB, BACK OF SHOULDER OR EDGE OF PAVEMENT (SEE SIGNAL PLANS)

**NOTES:**

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

**RECOMMENDED PUSHBUTTON LOCATIONS**



**LEGEND**

- DOWNWARD SLOPE
- PEDESTRIAN PUSHBUTTON
- RECOMMENDED PUSHBUTTON LOCATIONS

- \* WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- \*\* WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

**NOTES:**

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

**TRAFFIC SIGNAL EQUIPMENT OFFSET**

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT* TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

**NOTES:**

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

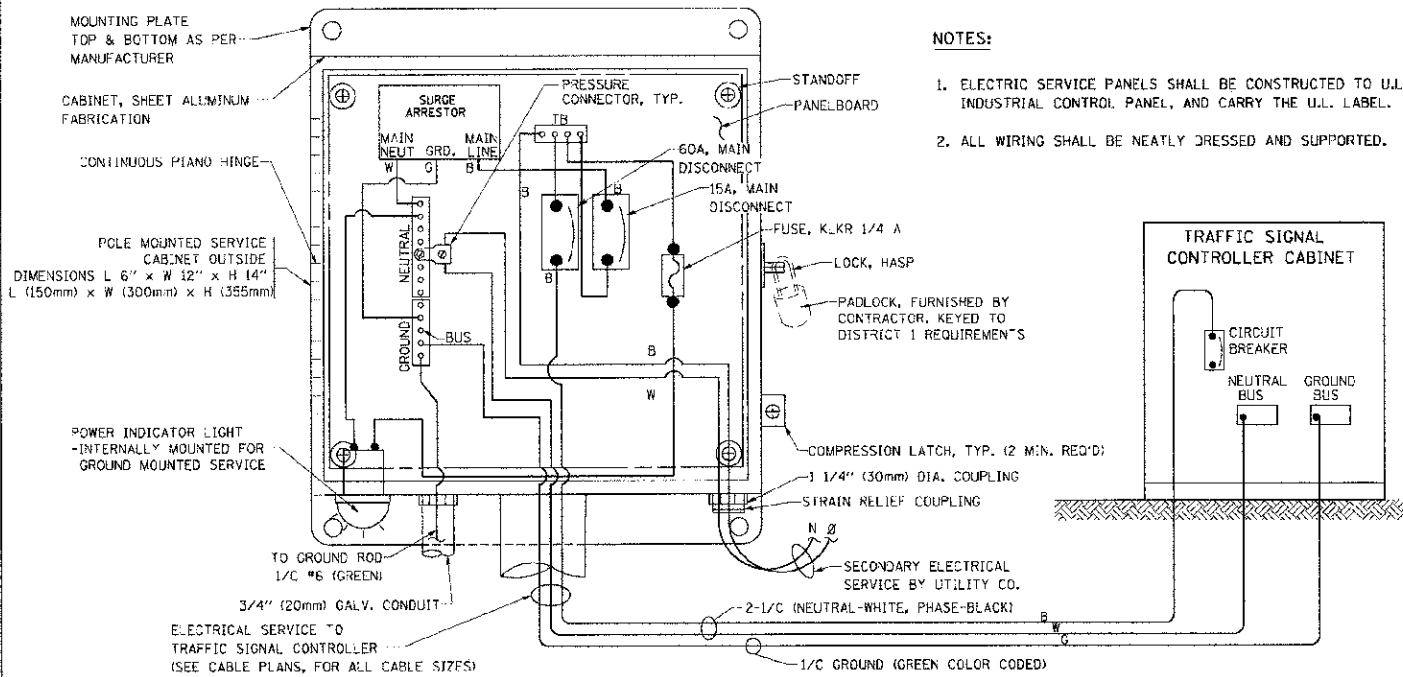
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PROJECT NAME =	DATE = 11/4/2009	DRAWN - BCK	REVISED -
PLOT SCALE = 50.0000 1/2 IN.	DATE = 10-28-09	CHECKED - DAD	REVISED -
		DATE = 10-28-09	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

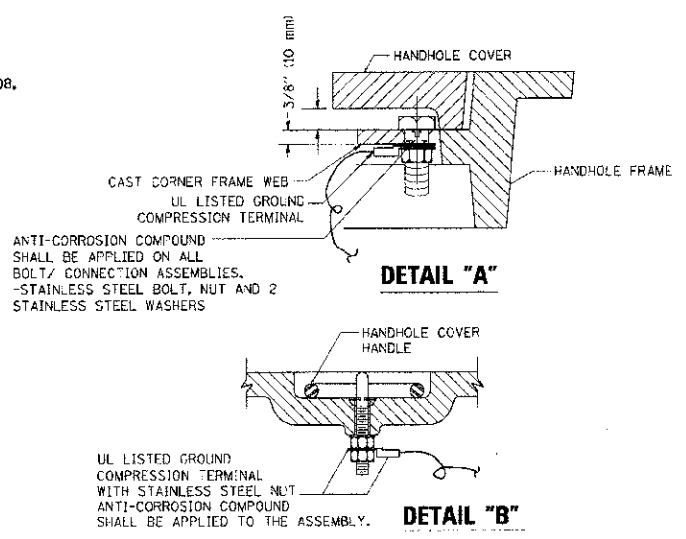
**DISTRICT ONE  
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET NO. 2 OF 6 SHEETS STA. TO STA.

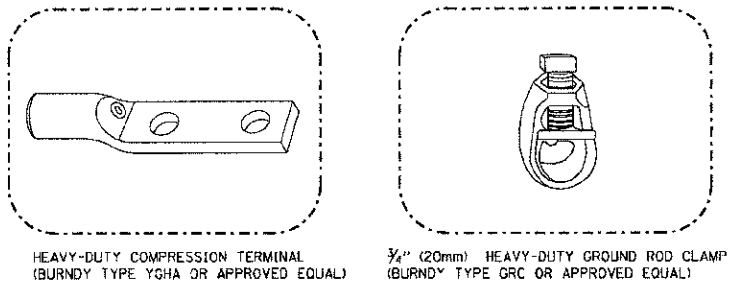
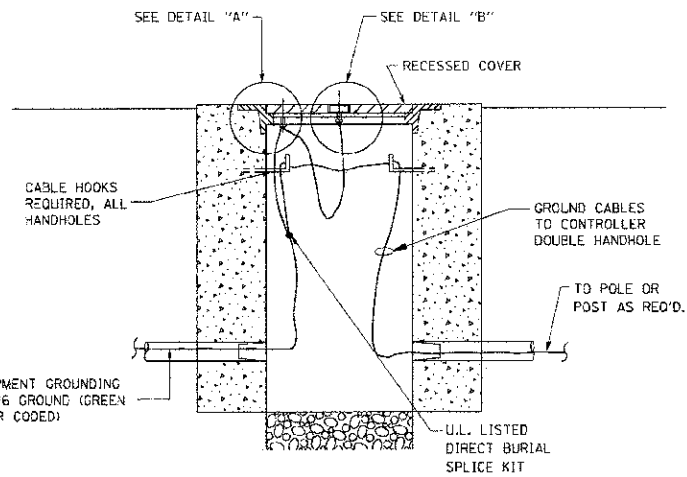
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	09-00170-00-C1	DUPAGE	108	57
TS-05		CONTRACT NO. 63742		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



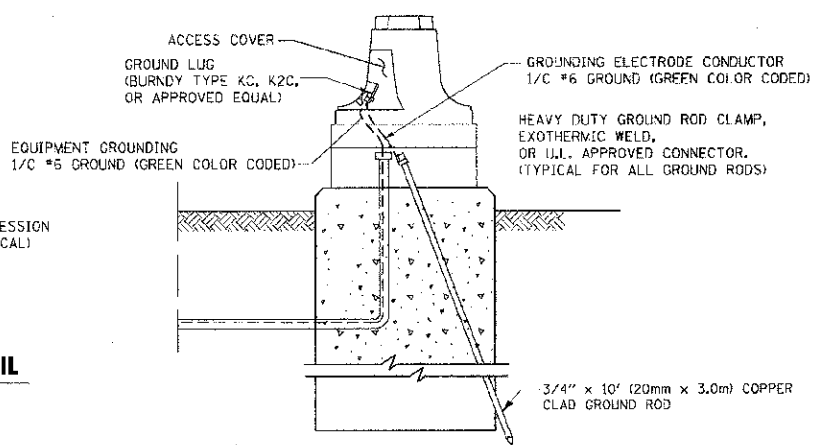
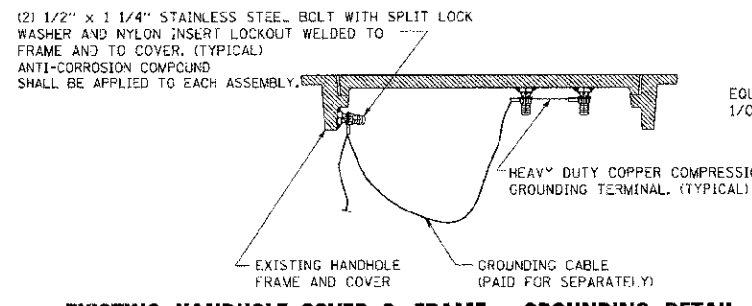
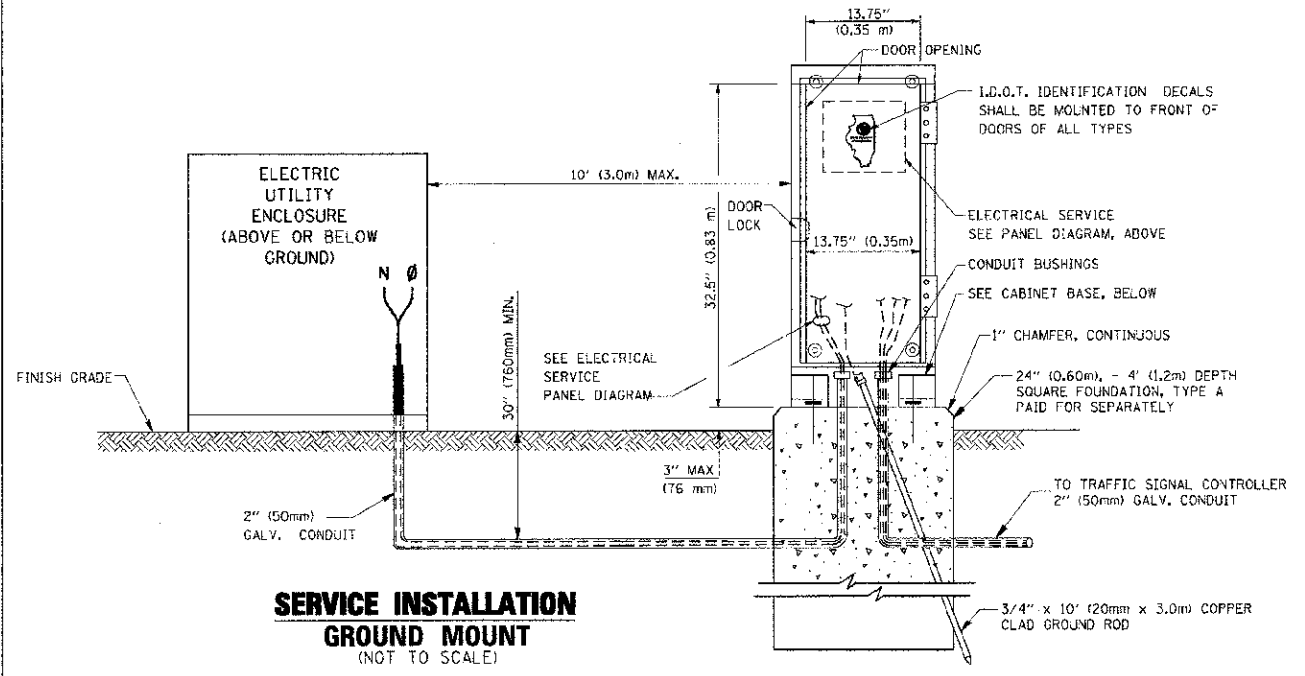
**ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)**  
**SERVICE INSTALLATION POLE MOUNT (SHOWN)**  
 (NOT TO SCALE)



- NOTES:**
- GROUNDING SYSTEM**
1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
  2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
  3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
  4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.

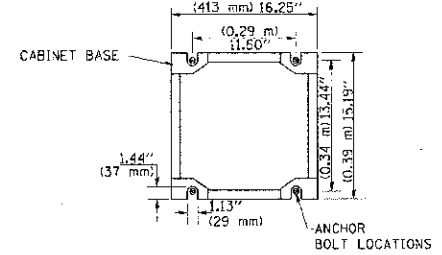


- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, U.L. APPROVED.
  - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES
  - 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES
  - 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES.
  - 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.

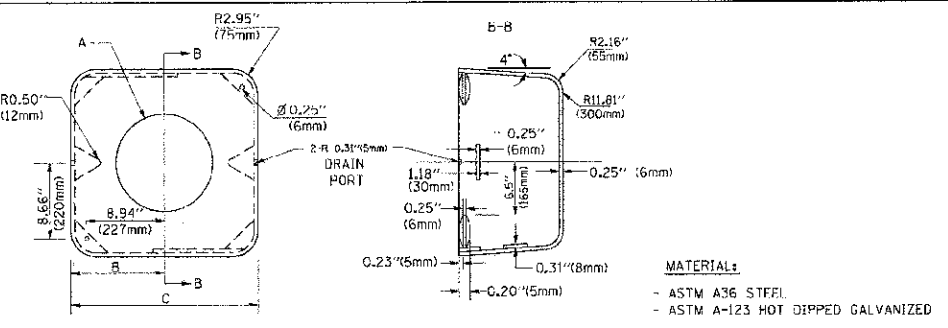
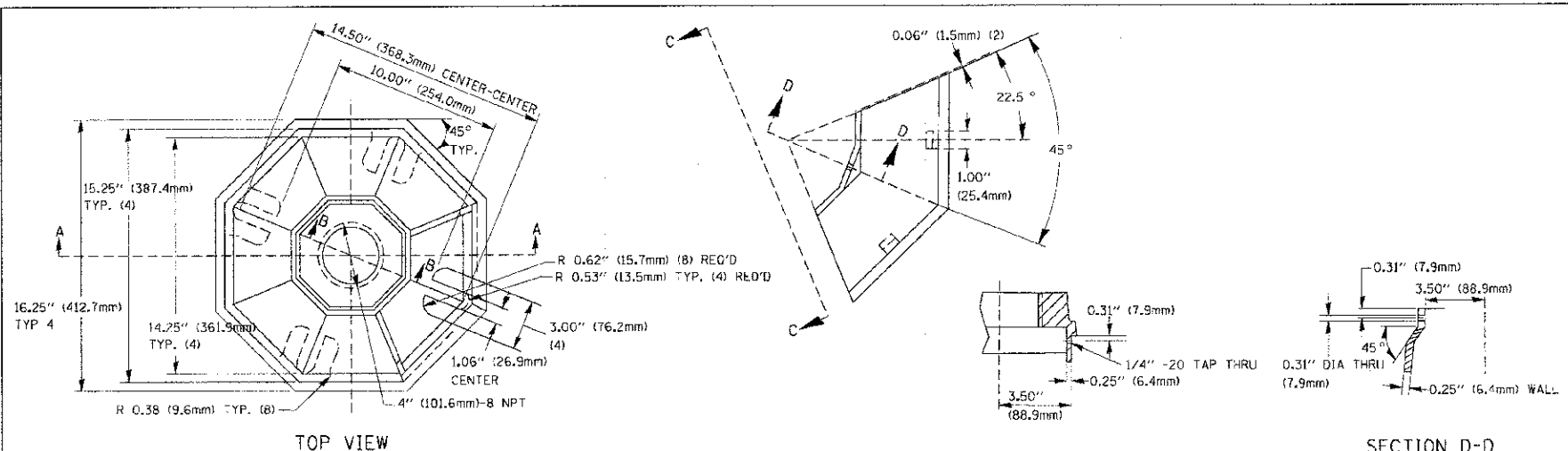


**SERVICE INSTALLATION GROUND MOUNT**  
 (NOT TO SCALE)

**CABINET - BASE BOLT PATTERN**  
 (NOT TO SCALE)



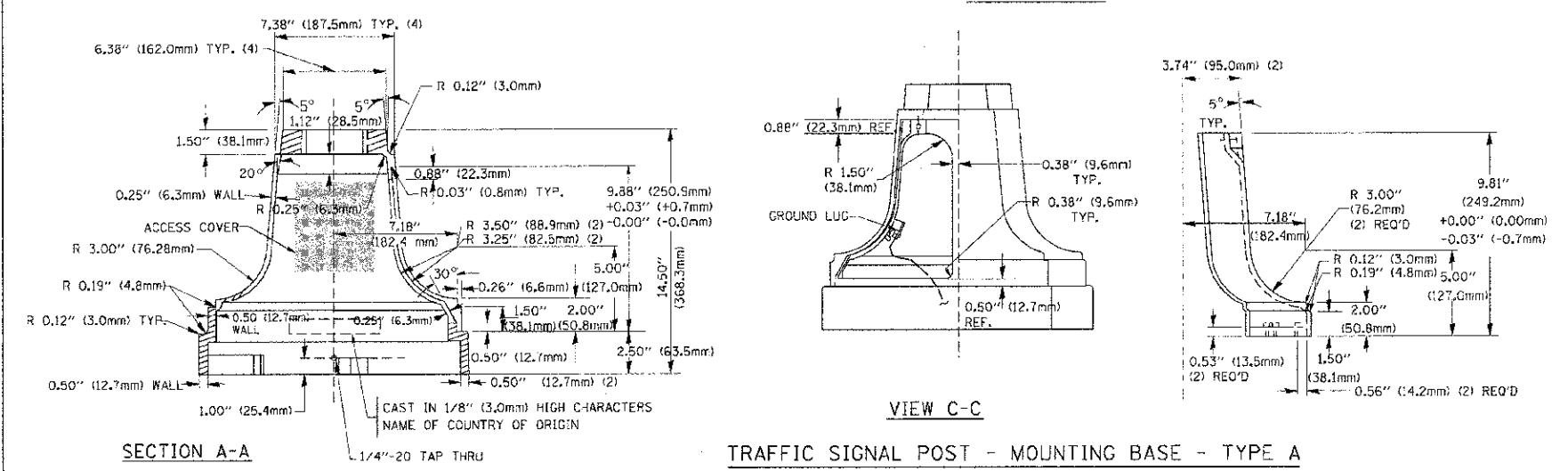
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PLOT SCALE = 80.0000 / IN.		CHECKED - DAD	REVISED -		SCALE: NONE	SHEET NO. 3 OF 6 SHEETS	STA. FD STA.	CONTRACT NO. 63742			
PLOT DATE = 11/4/2009		DATE - 10-28-09	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						



A	B	C	HEIGHT	WEIGHT
VARIABLES	9.5\" (241mm)	19\" (483mm)	7\" (178mm) - 12\" (300mm)	53 lbs (24kg)
VARIABLES	10.75\" (273mm)	21.5\" (546mm)	7\" (178mm) - 12\" (300mm)	68 lbs (31 kg)
VARIABLES	13.0\" (330mm)	26\" (660mm)	7\" (178mm) - 12\" (300mm)	81 lbs (37 kg)
VARIABLES	18.5\" (470mm)	37\" (940mm)	7\" (178mm) - 12\" (300mm)	126 lbs (57 kg)

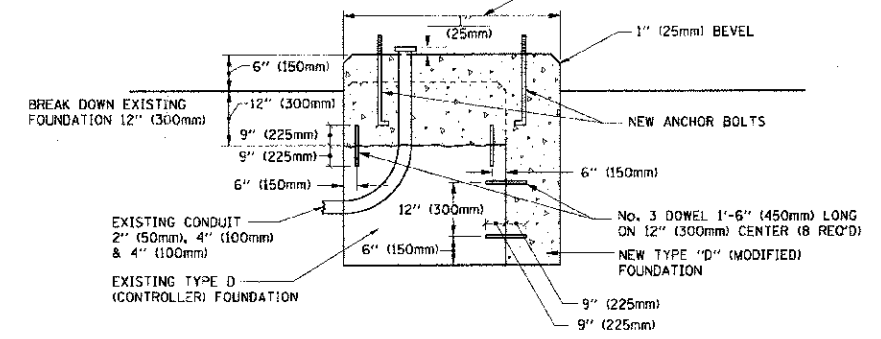
**SHROUD**

- NOTES:**
1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
  2. THE SUPPLIER SHALL VERIFY THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
  3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

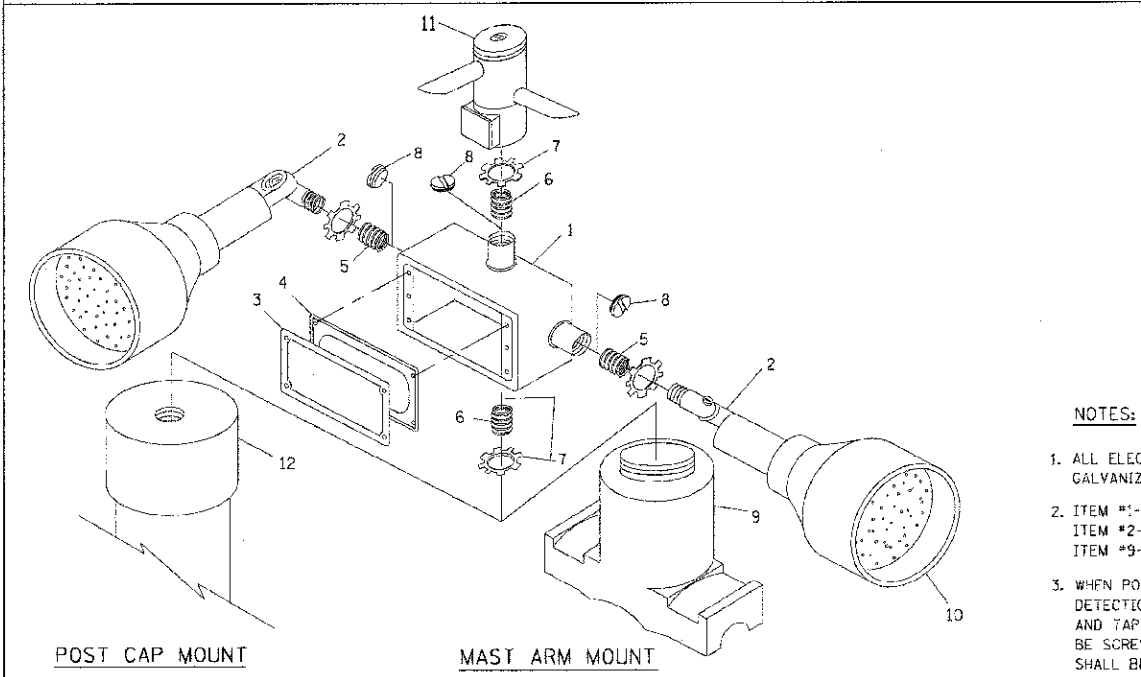


**TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A**

**NOTE:**  
SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.

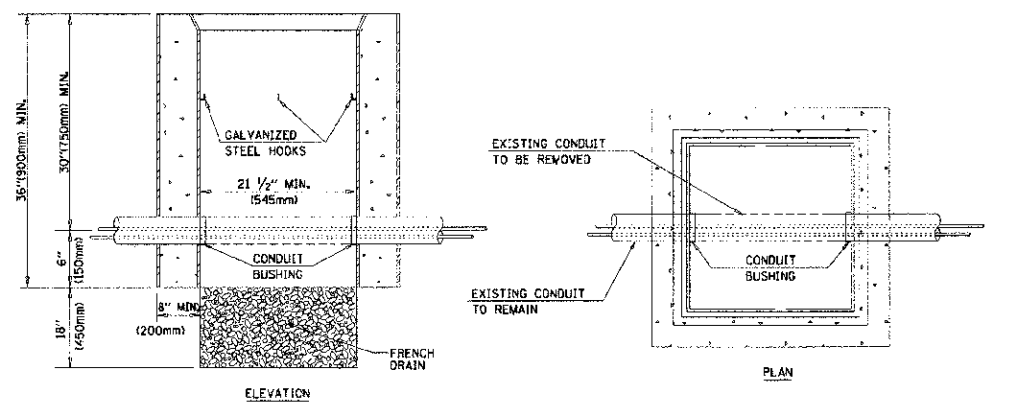


**MODIFY EXISTING TYPE "D" FOUNDATION**



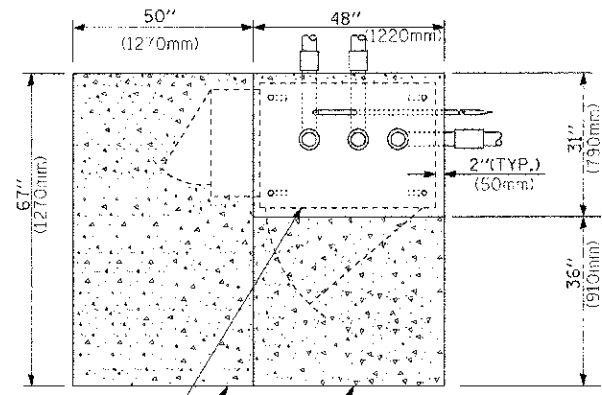
ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU. IN. (0.000344 CU. M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4\" (19 mm) CLOSE NIPPLE
7	3/4\" (19 mm) LOCKNUT
8	3/4\" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP (18 FT. (5.4 m) POST MIN.)

- NOTES:**
1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
  2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT  
ITEM #2- MULBERRY CON-0 SHADE LAMP SHIELD OR EQUIVALENT  
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
  3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4\" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.

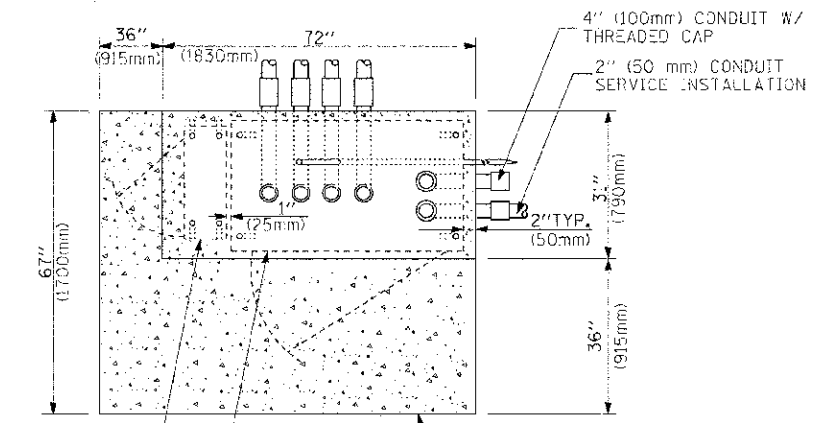


- NOTES:**
1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
  2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCIDENTAL TO THE HANDHOLE.

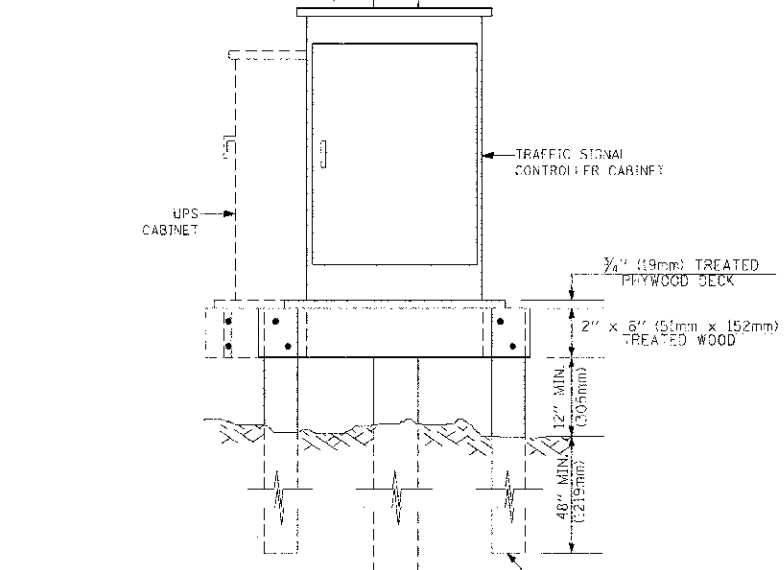
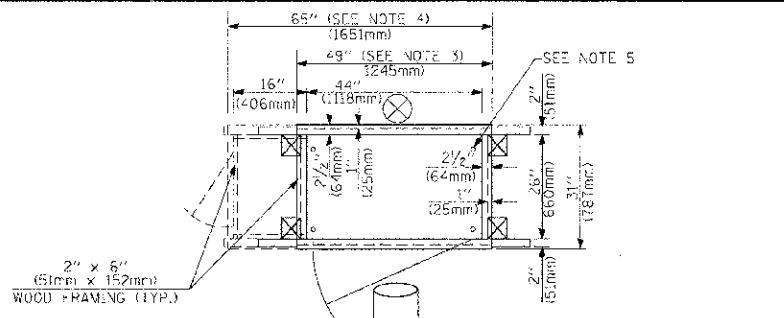
**HANDHOLE TO INTERCEPT EXISTING CONDUIT**



TOP VIEW  
CONTROLLER CABINET BASE  
EXISTING APRON  
PROPOSED APRON

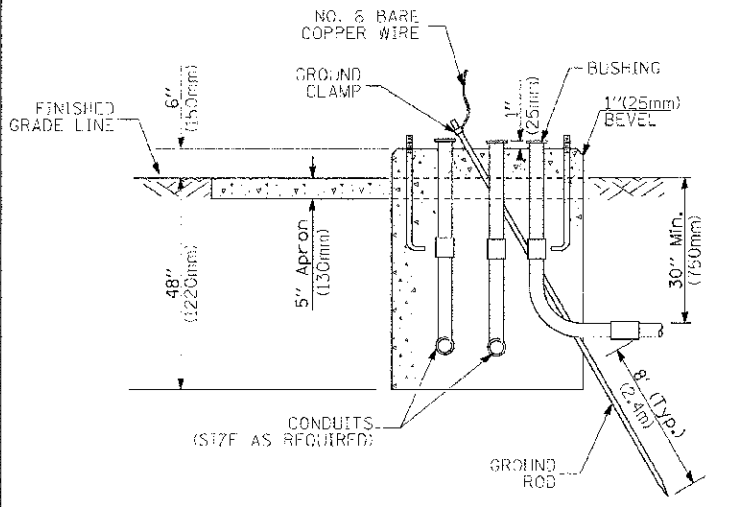


TOP VIEW  
UPS CABINET BASE  
CONTROLLER CABINET BASE  
APRON

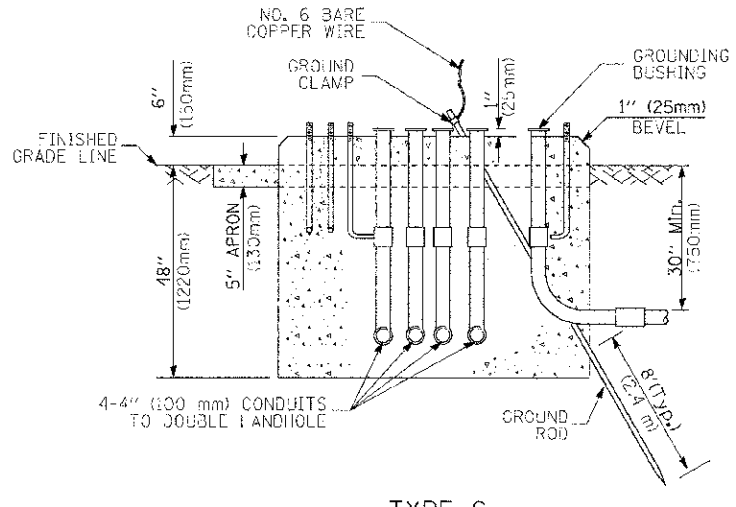


- NOTES:
- BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
  - BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
  - PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
  - PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
  - DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
  - FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

TEMPORARY SIGNAL CONTROLLER  
WOOD SUPPORT PLATFORM



TYPE D  
FOR GROUND MOUNTED  
CONTROLLER CABINET  
AND UPS BATTERY CABINET



TYPE C  
FOR GROUND MOUNTED  
CONTROLLER CABINET  
AND UPS BATTERY CABINET

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

MAST ARM LENGTH	① FOUNDATION DEPTH	FOUNDATION DIAMETER	SPIRAL DIAMETER	QUANTITY OF REBARS	SIZE OF REBARS
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 55' (16.8 m) and less than 65' (19.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 75' (22.9 m) and up to 85' (25.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

- NOTES:
- These foundation depths are for sites which have cohesive soils (clayey silty, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
  - Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
  - Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
  - For mast arm assemblies with dual arms refer to state standard 878001.

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

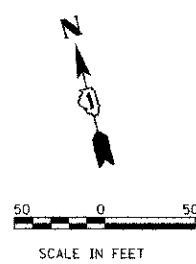


**LEGEND**

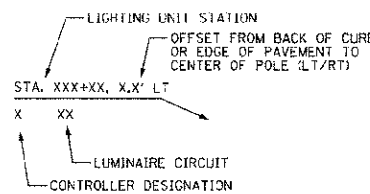
- LIGHT POLE  
40 FT MOUNTING HEIGHT, 12 FT MAST ARM, 250W HPS LUMINAIRE, 240V WITH BREAKAWAY DEVICE (UNLESS OTHERWISE NOTED)
- TRAFFIC SIGNAL AND STREET LIGHTING COMBINATION POLE  
40 FT MOUNTING HEIGHT, 12 FT MAST ARM, 250W HPS LUMINAIRE, 240V
- UNIT DUCT WITH 4-1/2" NO. 6 AND 1/2" NO. 8 GROUND,  
600V (EPR-TYPE RHW), 1 1/4" DIA. POLYETHYLENE
- RIGID STEEL CONDUIT OF THE DIAMETER AND LENGTH  
INDICATED ON THE DRAWINGS.
- PR LIGHTING CONTROLLER  
120V/240V, 1Ø, 3-WIRE
- PR SERVICE INSTALLATION  
120V/240V, 1Ø, 3-WIRE
- EXISTING LIGHTING UNIT TO REMAIN
- ⊗ REM EXISTING DECORATIVE STREET LIGHT TO BE  
REMOVED AND SALVAGE (SEE NOTE 10)
- ⊗ EX LIGHTING CONTROLLER TO BE REMOVED
- ⊗ "E" EXISTING DECORATIVE STREET LIGHT  
TO BE MAINTAINED
- EXISTING UNIT DUCT
- ⊗ REL EXISTING DECORATIVE STREET LIGHT  
TO BE RELOCATED

**GENERAL NOTES:**

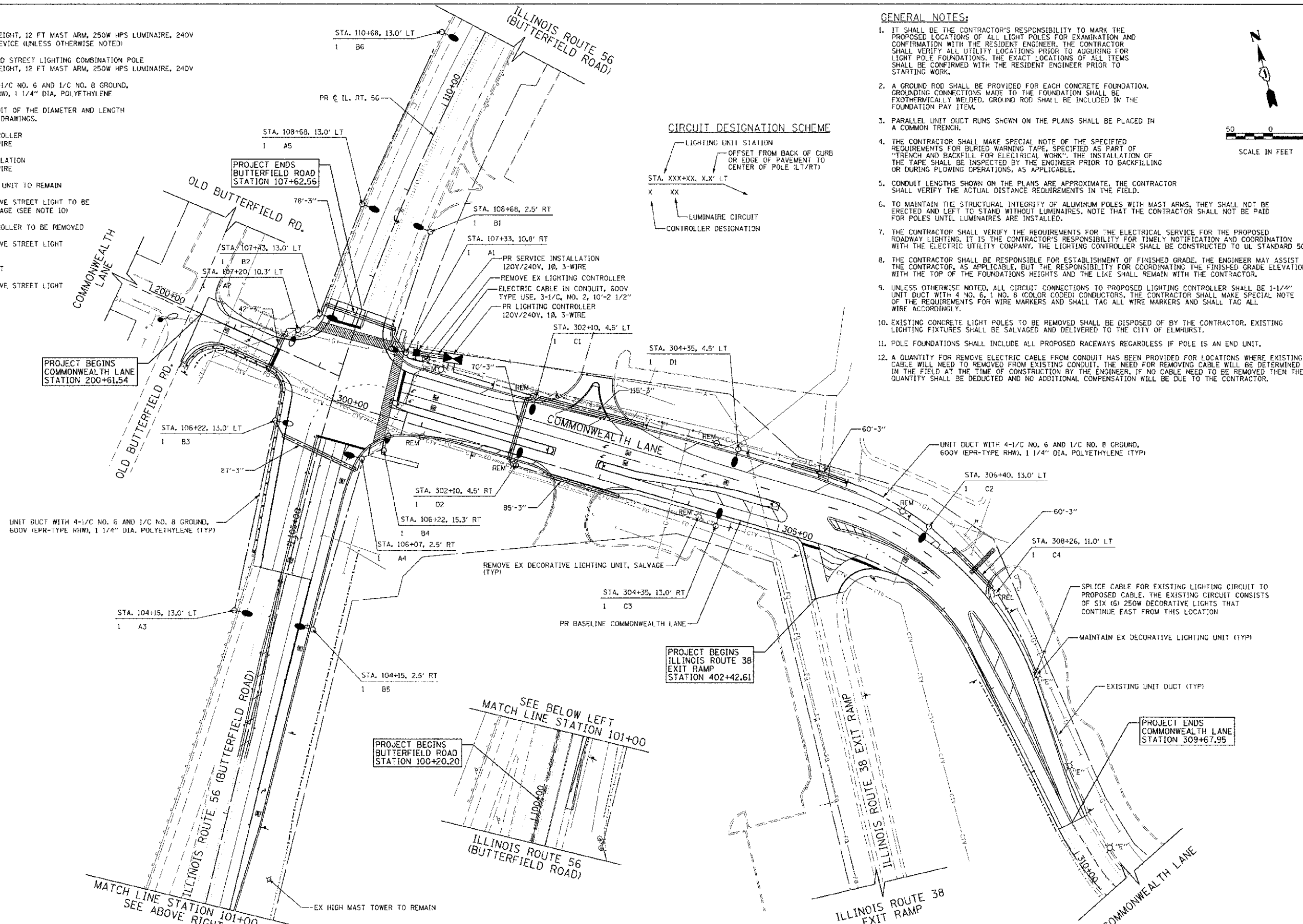
1. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO MARK THE PROPOSED LOCATIONS OF ALL LIGHT POLES FOR EXAMINATION AND CONFIRMATION WITH THE RESIDENT ENGINEER. THE CONTRACTOR SHALL VERIFY ALL UTILITY LOCATIONS PRIOR TO ADJURING FOR LIGHT POLE FOUNDATIONS. THE EXACT LOCATIONS OF ALL ITEMS SHALL BE CONFIRMED WITH THE RESIDENT ENGINEER PRIOR TO STARTING WORK.
2. A GROUND ROD SHALL BE PROVIDED FOR EACH CONCRETE FOUNDATION. GROUNDING CONNECTIONS MADE TO THE FOUNDATION SHALL BE EXOTHERMICALLY WELDED. GROUND ROD SHALL BE INCLUDED IN THE FOUNDATION PAY ITEM.
3. PARALLEL UNIT DUCT RUNS SHOWN ON THE PLANS SHALL BE PLACED IN A COMMON TRENCH.
4. THE CONTRACTOR SHALL MAKE SPECIAL NOTE OF THE SPECIFIED REQUIREMENTS FOR BURIED WARNING TAPE, SPECIFIED AS PART OF "TRENCH AND BACKFILL FOR ELECTRICAL WORK". THE INSTALLATION OF THE TAPE SHALL BE INSPECTED BY THE ENGINEER PRIOR TO BACKFILLING OR DURING PLOWING OPERATIONS, AS APPLICABLE.
5. CONDUIT LENGTHS SHOWN ON THE PLANS ARE APPROXIMATE. THE CONTRACTOR SHALL VERIFY THE ACTUAL DISTANCE REQUIREMENTS IN THE FIELD.
6. TO MAINTAIN THE STRUCTURAL INTEGRITY OF ALUMINUM POLES WITH MAST ARMS, THEY SHALL NOT BE ERECTED AND LEFT TO STAND WITHOUT LUMINAIRES. NOTE THAT THE CONTRACTOR SHALL NOT BE PAID FOR POLES UNTIL LUMINAIRES ARE INSTALLED.
7. THE CONTRACTOR SHALL VERIFY THE REQUIREMENTS FOR THE ELECTRICAL SERVICE FOR THE PROPOSED ROADWAY LIGHTING. IT IS THE CONTRACTOR'S RESPONSIBILITY FOR TIMELY NOTIFICATION AND COORDINATION WITH THE ELECTRIC UTILITY COMPANY. THE LIGHTING CONTROLLER SHALL BE CONSTRUCTED TO UL STANDARD 508.
8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ESTABLISHMENT OF FINISHED GRADE. THE ENGINEER MAY ASSIST THE CONTRACTOR, AS APPLICABLE, BUT THE RESPONSIBILITY FOR COORDINATING THE FINISHED GRADE ELEVATION WITH THE TOP OF THE FOUNDATIONS HEIGHTS AND THE LIKE SHALL REMAIN WITH THE CONTRACTOR.
9. UNLESS OTHERWISE NOTED, ALL CIRCUIT CONNECTIONS TO PROPOSED LIGHTING CONTROLLER SHALL BE 1-1/4" UNIT DUCT WITH 4 NO. 6, 1 NO. 8 (COLOR CODED) CONDUCTORS. THE CONTRACTOR SHALL MAKE SPECIAL NOTE OF THE REQUIREMENTS FOR WIRE MARKERS AND SHALL TAG ALL WIRE MARKERS AND SHALL TAG ALL WIRE ACCORDINGLY.
10. EXISTING CONCRETE LIGHT POLES TO BE REMOVED SHALL BE DISPOSED OF BY THE CONTRACTOR. EXISTING LIGHTING FIXTURES SHALL BE SALVAGED AND DELIVERED TO THE CITY OF ELMHURST.
11. POLE FOUNDATIONS SHALL INCLUDE ALL PROPOSED RACEWAYS REGARDLESS IF POLE IS AN END UNIT.
12. A QUANTITY FOR REMOVE ELECTRIC CABLE FROM CONDUIT HAS BEEN PROVIDED FOR LOCATIONS WHERE EXISTING CABLE WILL NEED TO BE REMOVED FROM EXISTING CONDUIT. THE NEED FOR REMOVING CABLE WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE ENGINEER. IF NO CABLE NEED TO BE REMOVED THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.



**CIRCUIT DESIGNATION SCHEME**



PLAN	DATE
SURVEY	BY
PLotted	BY
NOTE BOOK	NO.
FILE NAME	FILE NAME



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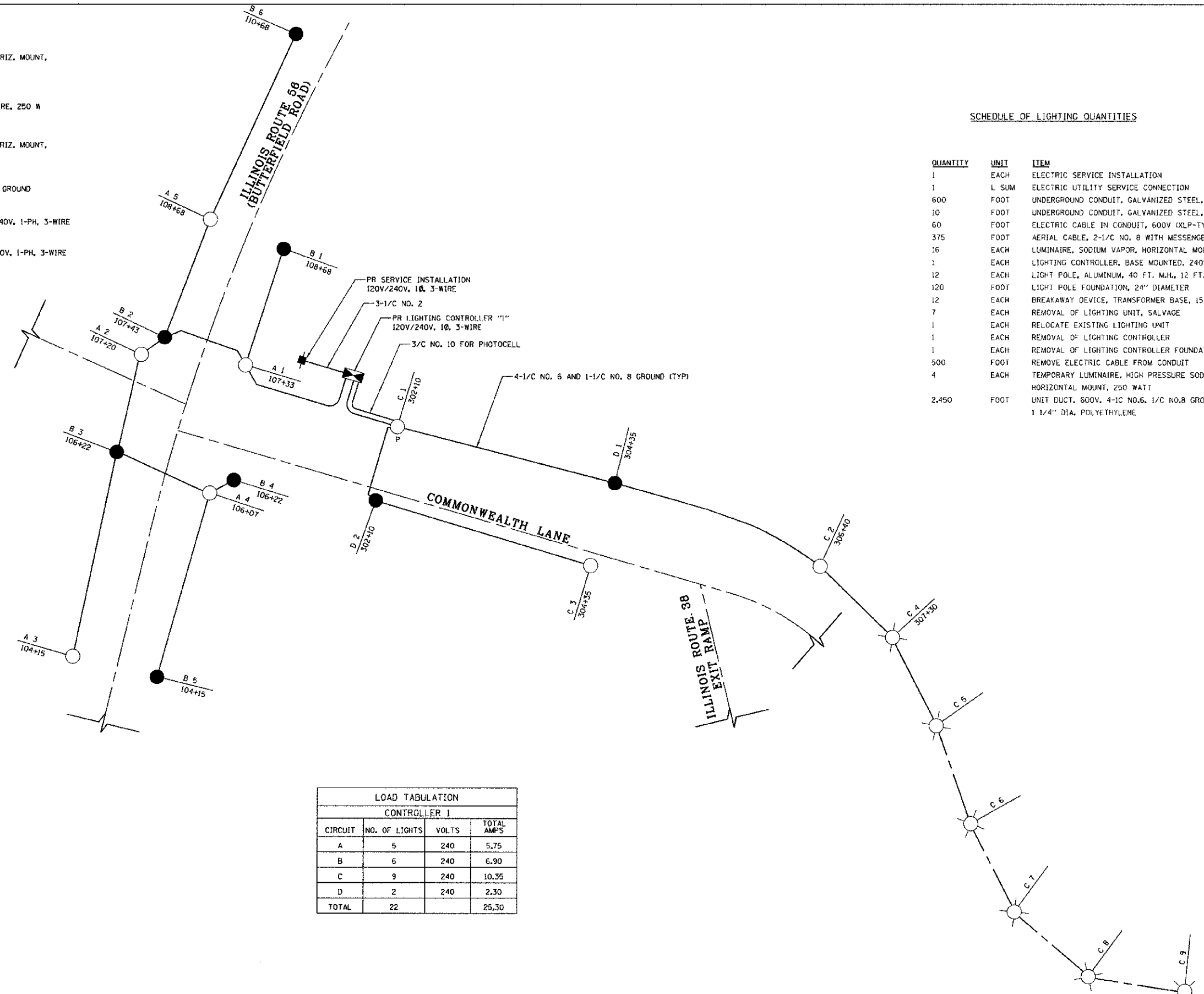
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	PLOT DATE = 12/4/2012	DATE = 10/22/2012	REVISED =		FED. ROAD DIST. NO. 7 (ILLINOIS) FED. AID PROJECT					

**LEGEND**

- LUMINAIRE, SODIUM VAPOR, HORIZ. MOUNT, 250 W. ON RED/BLACK WIRE
- ⊗ EXISTING DECORATIVE LUMINAIRE, 250 W
- LUMINAIRE, SODIUM VAPOR, HORIZ. MOUNT, 250 W. ON BLUE/ORANGE WIRE
- 4-1/2" NO. 6 AND 1-1/2" NO. 8 GROUND
- SERVICE INSTALLATION, 120/240V, 1-PH, 3-WIRE
- ⊞ LIGHTING CONTROLLER, 120/240V, 1-PH, 3-WIRE
- P PHOTO CELL LOCATION
- - - EXISTING CABLE

**SCHEDULE OF LIGHTING QUANTITIES**

QUANTITY	UNIT	ITEM
1	EACH	ELECTRIC SERVICE INSTALLATION
1	L SUM	ELECTRIC UTILITY SERVICE CONNECTION
600	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.
10	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.
60	FOOT	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 3-1/2" NO. 2
375	FOOT	AERIAL CABLE, 2-1/2" NO. 8 WITH MESSENGER WIRE
16	EACH	LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, 250 WATT
1	EACH	LIGHTING CONTROLLER, BASE MOUNTED, 240VOLT, 100AMP
12	EACH	LIGHT POLE, ALUMINUM, 40 FT. M.H., 12 FT. MAST ARM
120	FOOT	LIGHT POLE FOUNDATION, 24" DIAMETER
12	EACH	BREAKAWAY DEVICE, TRANSFORMER BASE, 15 INCH BOLT CIRCLE
7	EACH	REMOVAL OF LIGHTING UNIT, SALVAGE
1	EACH	RELOCATE EXISTING LIGHTING UNIT
1	EACH	REMOVAL OF LIGHTING CONTROLLER
1	EACH	REMOVAL OF LIGHTING CONTROLLER FOUNDATION
500	FOOT	REMOVE ELECTRIC CABLE FROM CONDUIT
4	EACH	TEMPORARY LUMINAIRE, HIGH PRESSURE SODIUM VAPOR, HORIZONTAL MOUNT, 250 WATT
2,450	FOOT	UNIT DUCT, 600V, 4-1/2" NO.6, 1/2" NO.8 GROUND, (EPR-TYPE RHW), 1 1/4" DIA. POLYETHYLENE



LOAD TABULATION			
CONTROLLER J			
CIRCUIT	NO. OF LIGHTS	VOLTS	TOTAL AMPS
A	5	240	5.75
B	6	240	6.90
C	9	240	10.35
D	2	240	2.30
<b>TOTAL</b>	<b>22</b>		<b>25.30</b>

PLAN SURVEYED BY DATE  
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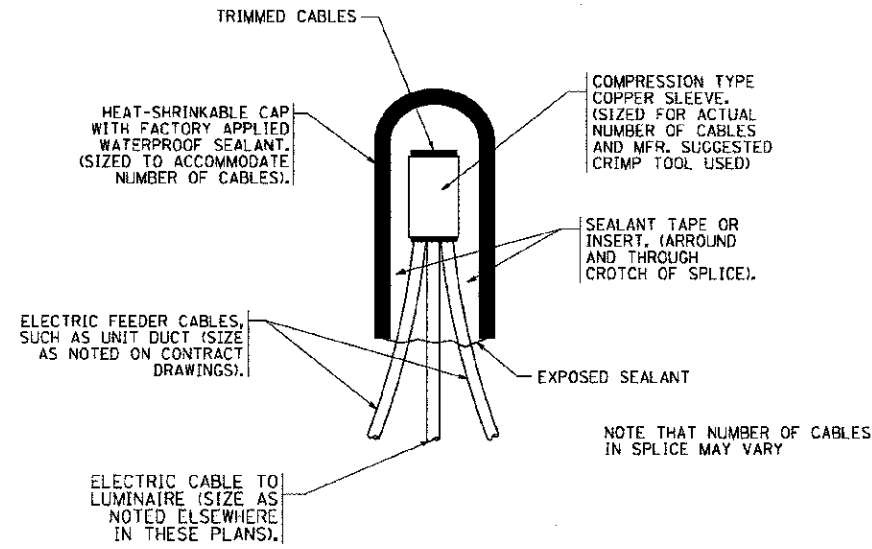
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**BUTTERFIELD ROAD AT COMMONWEALTH LANE  
 WIRING DIAGRAM**

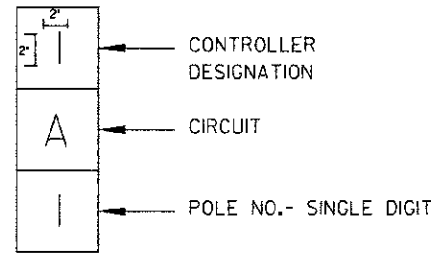
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CONTRACT NO. 63742				

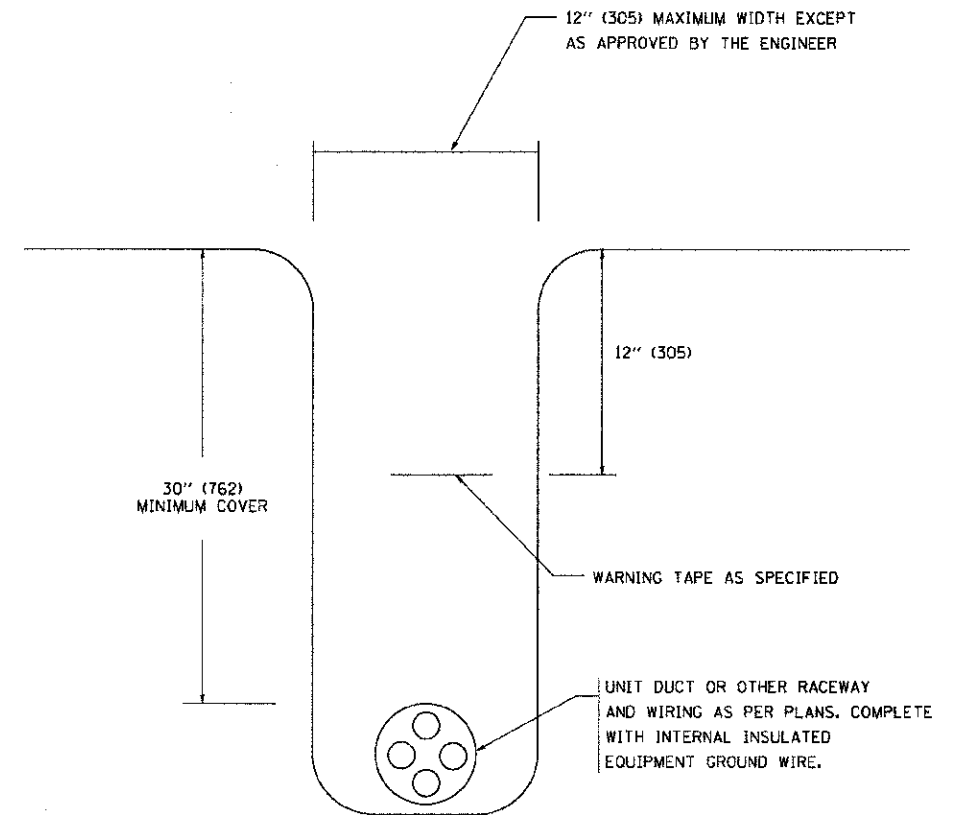
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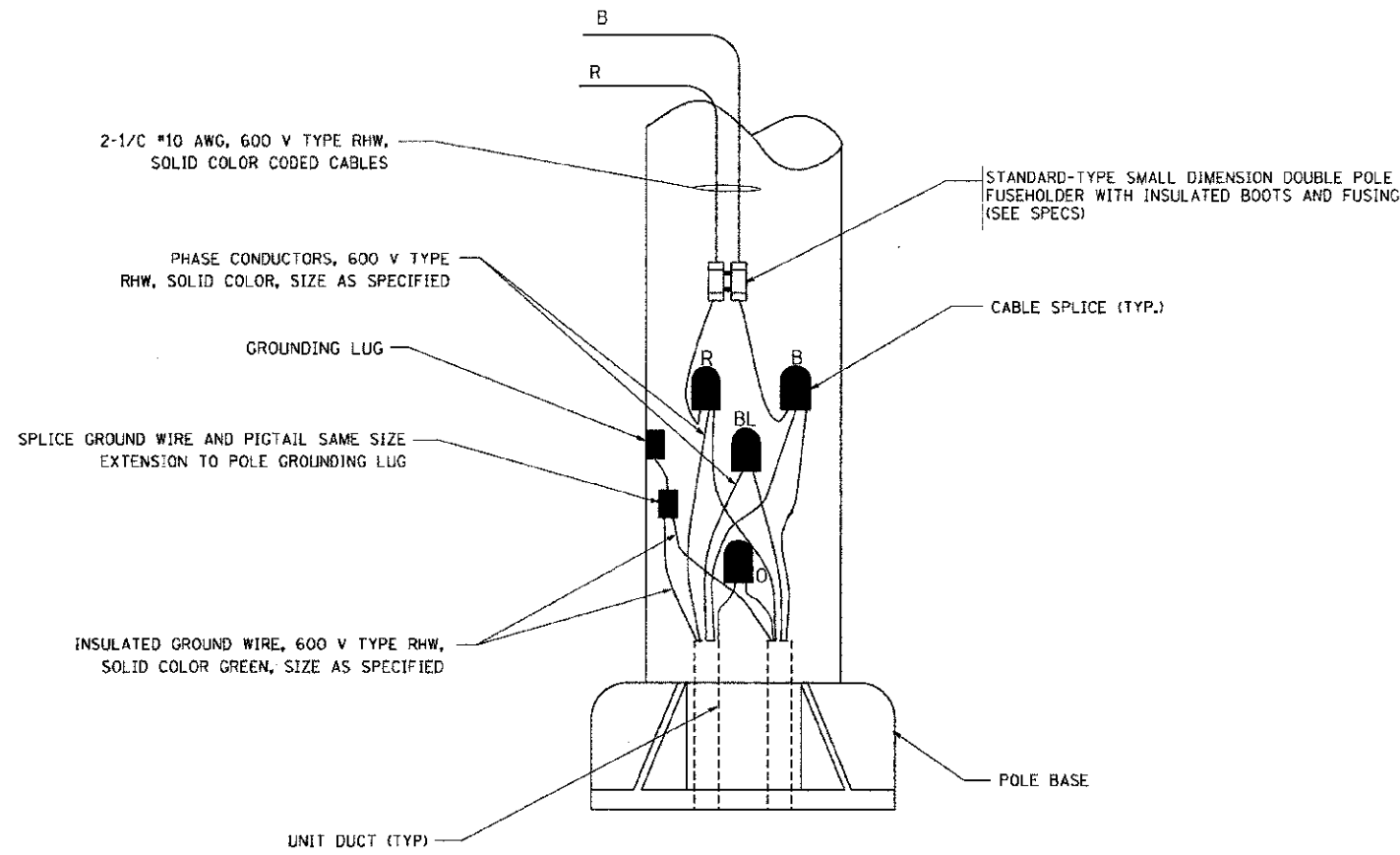
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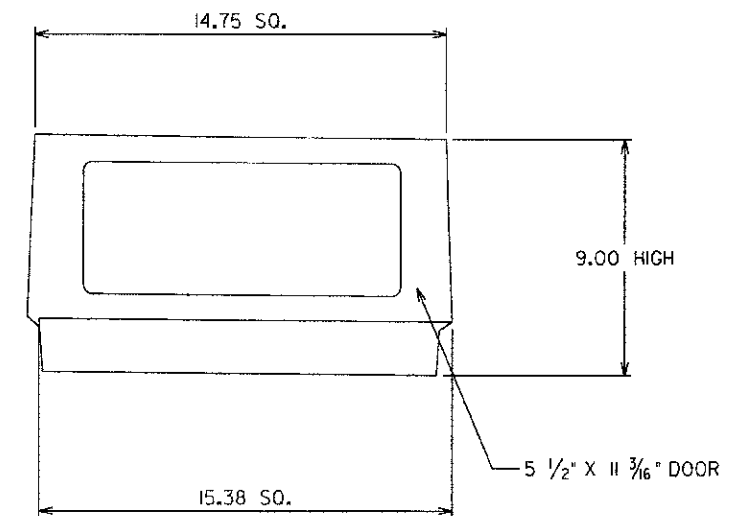
POLE NUMBER DETAIL



TYPICAL WIRING IN TRENCH DETAIL  
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POLE WIRING DETAIL  
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TRANSFORMER BASE DETAIL

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DATE - 10/22/2012

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REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BUTTERFIELD ROAD AT COMMONWEALTH LANE  
MISC. ELECTRICAL DETAILS

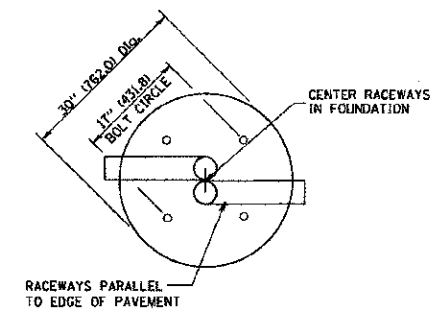
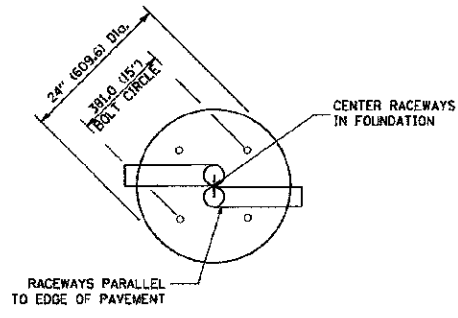
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3545	09-00170-00-CH	DUPAGE	108	63
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

CONTRACT NO. 63742

**LIGHT POLE FOUNDATION DEPTH TABLE**  
40 FT. (12.192 m) TO 47.5 FT. (14.478 m) MOUNTING HEIGHT

SOIL CONDITIONS	DESIGN DEPTH "D" OF FOUNDATION	
	SINGLE ARM POLE	TWIN ARM POLE
SOFT CLAY Qu = 0.375 TON/SQ. FT.	13'-0" (3.96 m)	15'-0" (4.57 m)
MEDIUM CLAY Qu = 0.75 TON/SQ. FT.	9'-6" (2.93 m)	10'-9" (3.23 m)
STIFF CLAY Qu = 1.50 TON/SQ. FT.	7'-0" (2.13 m)	8'-0" (2.44 m)
LOOSE SAND φ = 34°	9'-0" (2.74 m)	10'-0" (3.05 m)
MEDIUM SAND φ = 37.5°	8'-3" (2.52 m)	9'-0" (2.74 m)
DENSE SAND φ = 40°	7'-9" (2.36 m)	9'-0" (2.74 m)

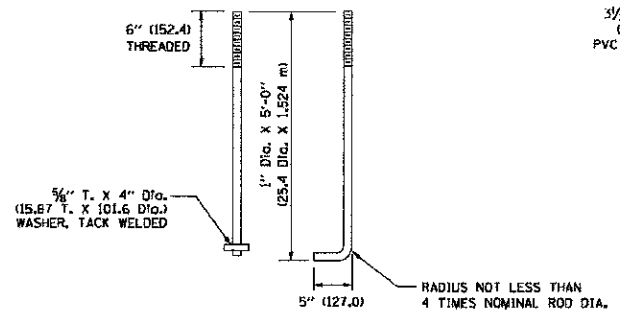


**TOP VIEW**

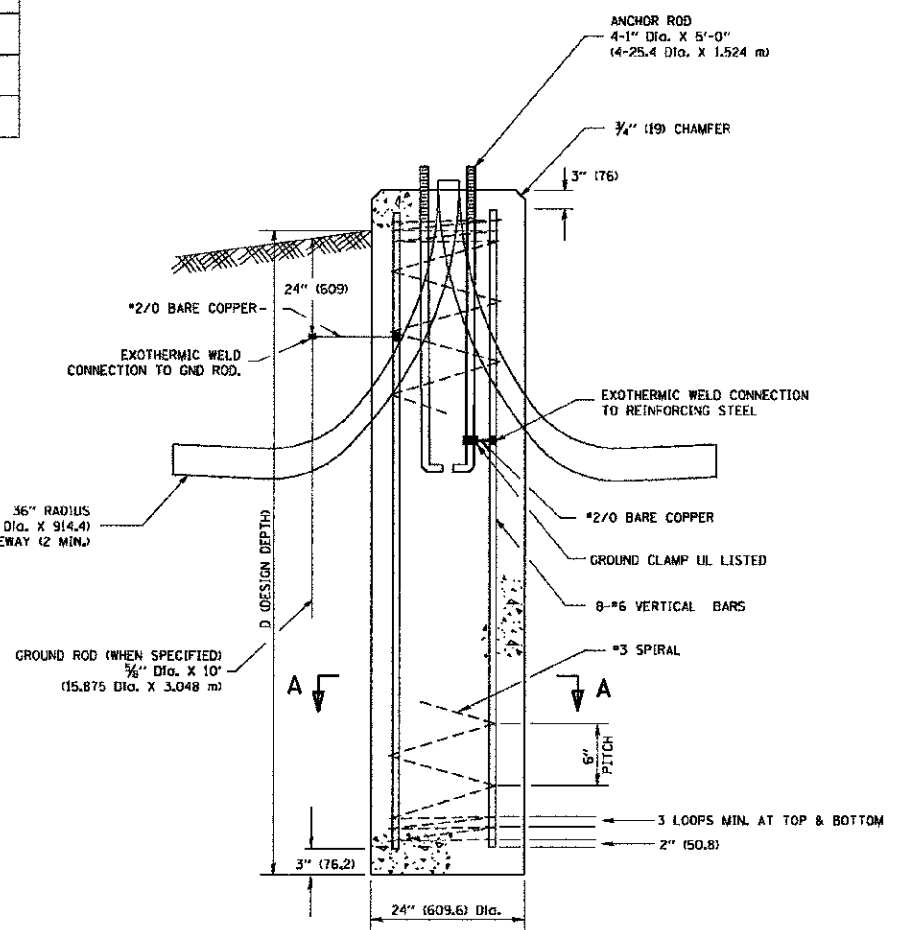
**TOP VIEW**

**NOTES**

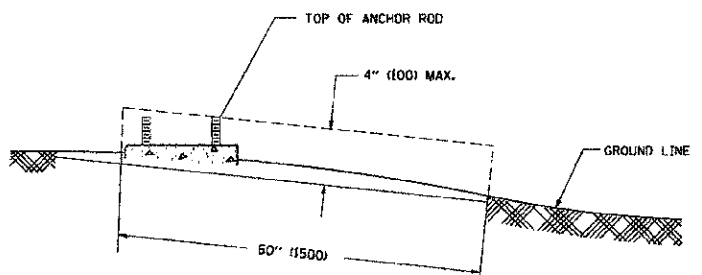
- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- THE ANCHOR RODS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED.
- THE FOUNDATION SHALL NOT PROTRUDE MORE THAN 100MM (4 IN.) ABOVE THE FINISHED GRADE WITHIN A 60 IN. (1.5 m) CHORD ACROSS THE FOUNDATION, WITH ANCHOR RODS INCLUDED, IN ACCORDANCE WITH AASHTO GUIDELINES. IF THE FOUNDATION HEIGHT, INCLUDING ANCHOR RODS, EXTENDS BEYOND THESE SPECIFIED LIMITS, THE FOUNDATION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE. SEE FOUNDATION EXTENSION DETAIL.
- THE HOLE FOR THE FOUNDATION SHALL BE MADE BY DRILLING WITH AN AUGER, OF THE SAME DIAMETER AS THE FOUNDATION. IF SOIL CONDITIONS REQUIRE THE USE OF A LINER TO FORM THE HOLE, THE LINER SHALL BE WITHDRAWN AS THE CONCRETE IS DEPOSITED.
- THE TOP OF THE FOUNDATION SHALL BE CONSTRUCTED LEVEL. A LINER OR FORM SHALL BE USED TO PRODUCE A UNIFORM SMOOTH SIDE TO THE TOP OF THE FOUNDATION. FOUNDATION TOP SHALL BE CHAMFERED 1/4-IN. (20 mm).
- THE CONCRETE SHALL BE CLASS SI. CONCRETE SHALL CURE ACCORDING TO ARTICLE 1020.13 BEFORE LIGHT POLES ARE INSTALLED.
- THE ANCHOR ROD SHALL BE A HOOK ROD TYPE. COLD BENDING OF THE ANCHOR ROD WILL NOT BE ALLOWED. THE RADIUS OF THE HOOK BEND SHALL NOT BE LESS THAN 4 TIMES THE NOMINAL DIAMETER OF THE ANCHOR ROD. A TACK WELDED ANCHOR ROD MAY BE SUBSTITUTED WITH THE APPROVAL OF THE ENGINEER.
- THE ANCHOR RODS SHALL BE ACCORDING TO ASTM F1554 GRADE 725 (GRADE 105). NUTS SHALL BE HEXAGON NUTS ACCORDING TO ASTM A 194 2H OR ASTM A 563 DH, AND WASHERS SHALL BE ACCORDING TO ASTM F 436.
- ANCHOR RODS, NUTS AND WASHERS SHALL BE COMPLETELY GALVANIZED BY EITHER THE HOT-DIPPED PROCESS CONFORMING WITH AASHTO M 232, THE MECHANICAL PLATING METHOD CONFORMING TO AASHTO M 298, CLASS 50 WITH A MAXIMUM COATING THICKNESS OF 150 UMG (MILS) OR THE ELECTROLYTIC PROCESS ACCORDING TO ASTM F 1136.
- THE ANCHOR RODS SHALL BE THREADED A MINIMUM OF 6 INCHES (150 mm) WITH A MINIMUM OF 3 INCHES (75 mm) OF THREADED ANCHOR ROD EMBEDDED IN THE FOUNDATION.
- ANCHOR RODS SHALL PROJECT 2 3/4" (69.9 mm) ABOVE THE TOP OF THE FOUNDATION. IF BREAKAWAY COUPLINGS ARE SPECIFIED, THE CONTRACTOR SHALL CAREFULLY COORDINATE THE ANCHOR ROD PROJECTION WITH THE INSTALLATION REQUIREMENTS OF THE BREAKAWAY COUPLINGS.
- THE CONTRACTOR SHALL USE A #3 SPIRAL AT 6" (152.4 mm) PITCH OR MAY SUBSTITUTE #3 TIES AT 12" (304.8 mm) O.C. WITH THE APPROVAL OF THE ENGINEER.
- THE CABLE TRENCHES AND FOUNDATION SHALL BE BACK FILLED AND COMPACTED AS SPECIFIED BEFORE THE LIGHT POLE IS ERECTED.
- THE RACEWAYS SHALL PROJECT 1" (25.4 mm) ABOVE THE TOP OF THE FOUNDATION.



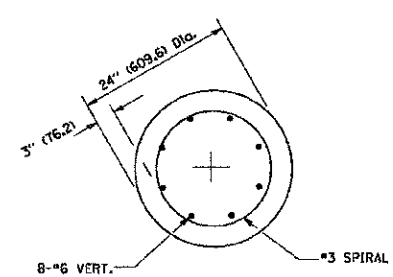
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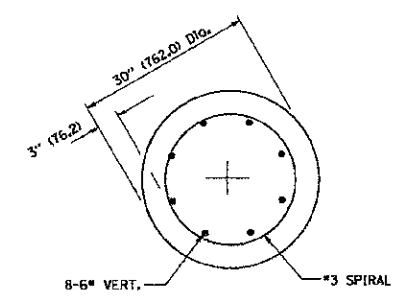
**FOUNDATION DETAIL**



**FOUNDATION EXTENSION DETAIL**

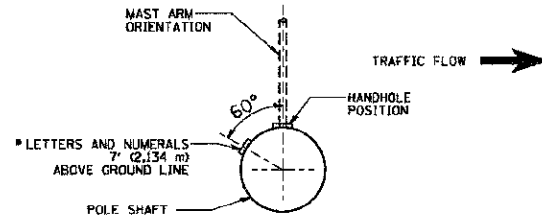


**SECTION A-A**

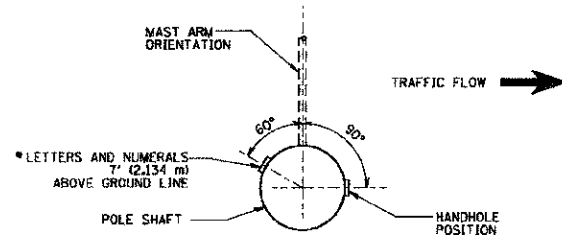


**SECTION A-A**

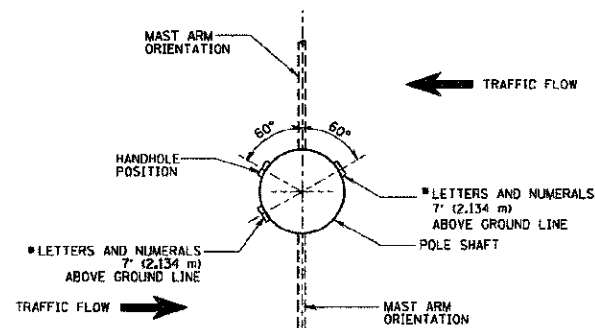
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		CHECKED -	REVISED -		SCALE: NONE			SHEET NO. 1 OF 1 SHEETS		CONTRACT NO. 63742		
		DATE -	REVISED -		STA. TO STA.			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
					PLOT SCALE = 50.0000" / 1"							
				PLOT DATE = 1/4/2008								



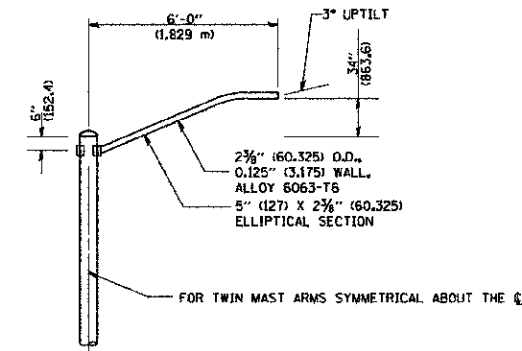
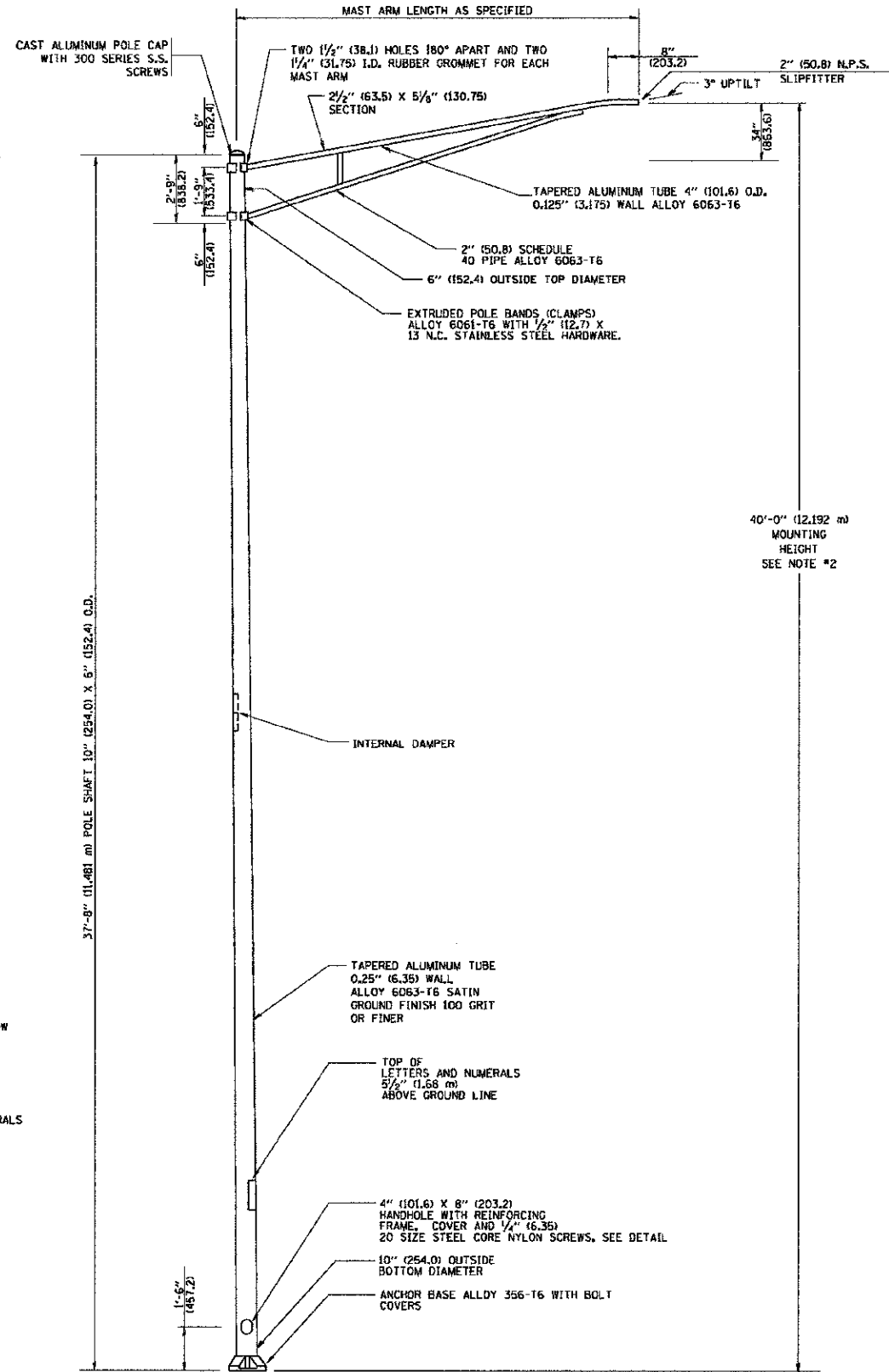
POSITION OF HANDHOLE AND POLE NUMBER FOR SINGLE MAST ARM POLES MOUNTED ON BRIDGE PARAPET OR BARRIER WALL



POSITION OF HANDHOLE AND POLE NUMBER FOR SINGLE MAST ARM POLES



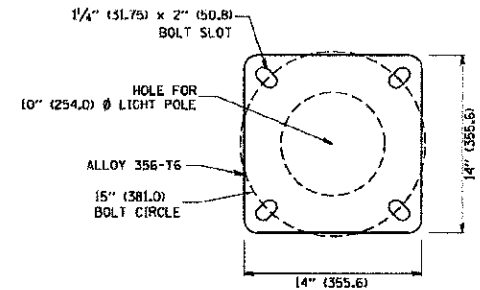
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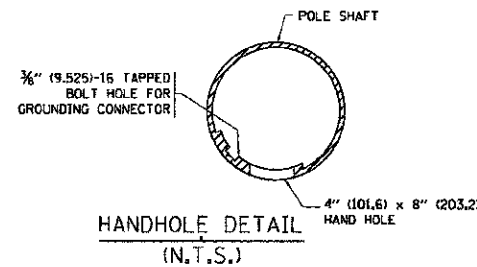
6' (1.8 m) SINGLE MEMBER MAST ARM (N.T.S.)

NOTES:

1. ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
2. MOUNTING HEIGHT IS DEFINED AS THE DISTANCE FROM THE CENTERLINE OF THE TENDON TO THE BOTTOM OF THE ANCHOR BASE.
3. THE LIGHT POLE WILL MEET AASHTO DESIGN CRITERIA AS SPECIFIED.
4. THE INSTALLING CONTRACTOR WILL PROVIDE A UL LISTED GROUNDING CONNECTOR, BURNDY K2C23, T&B SP4DL OR APPROVED EQUAL.
5. LIGHT POLES WILL NOT BE INSTALLED WITHOUT MAST ARMS AND LUMINAIRES.
6. LIGHT POLES WILL BE SET PLUMB ON THE FOUNDATION WITHOUT THE USE OF LEVELING NUTS, WASHERS OR SHIMS.
7. LIGHTING UNIT IDENTIFICATION NUMBERS SHALL BE INSTALLED BEFORE THE LIGHTING UNIT IS ENERGIZED.



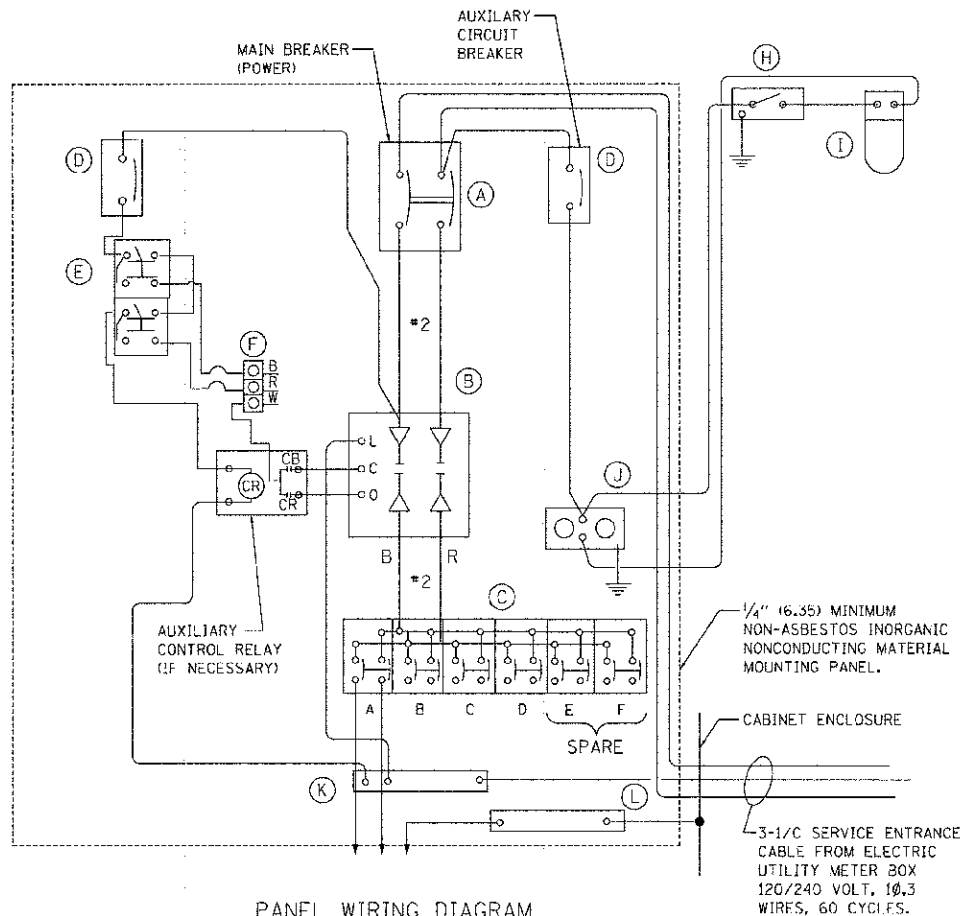
LIGHT POLE BASE PLATE DETAIL  
15 INCH (381.0) BOLT CIRCLE



HANDHOLE DETAIL (N.T.S.)

FILE NAME = W:\distatd\22x31\be481.dgn	USER NAME = gog\lanobt	DESIGNED -	REVISED - R. TOMSONS 09-06-00	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ALUMINUM LIGHT POLE 40'-0" (12.192 m) MOUNTING HEIGHT</b>		F.A.I. RTE. 3545	SECTION 09-00170-00-CH	COUNTY DUPAGE	TOTAL SHEETS 108	SHEET NO. 65
PLOT SCALE = 5/8" = 1' IN.		DRAWN -	REVISED - R. TOMSONS 09-02-03		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	<b>BE-401</b>		CONTRACT NO. 63742	
PLOT DATE = 1/4/2008		CHECKED -	REVISED -				FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				
		DATE -	REVISED -								

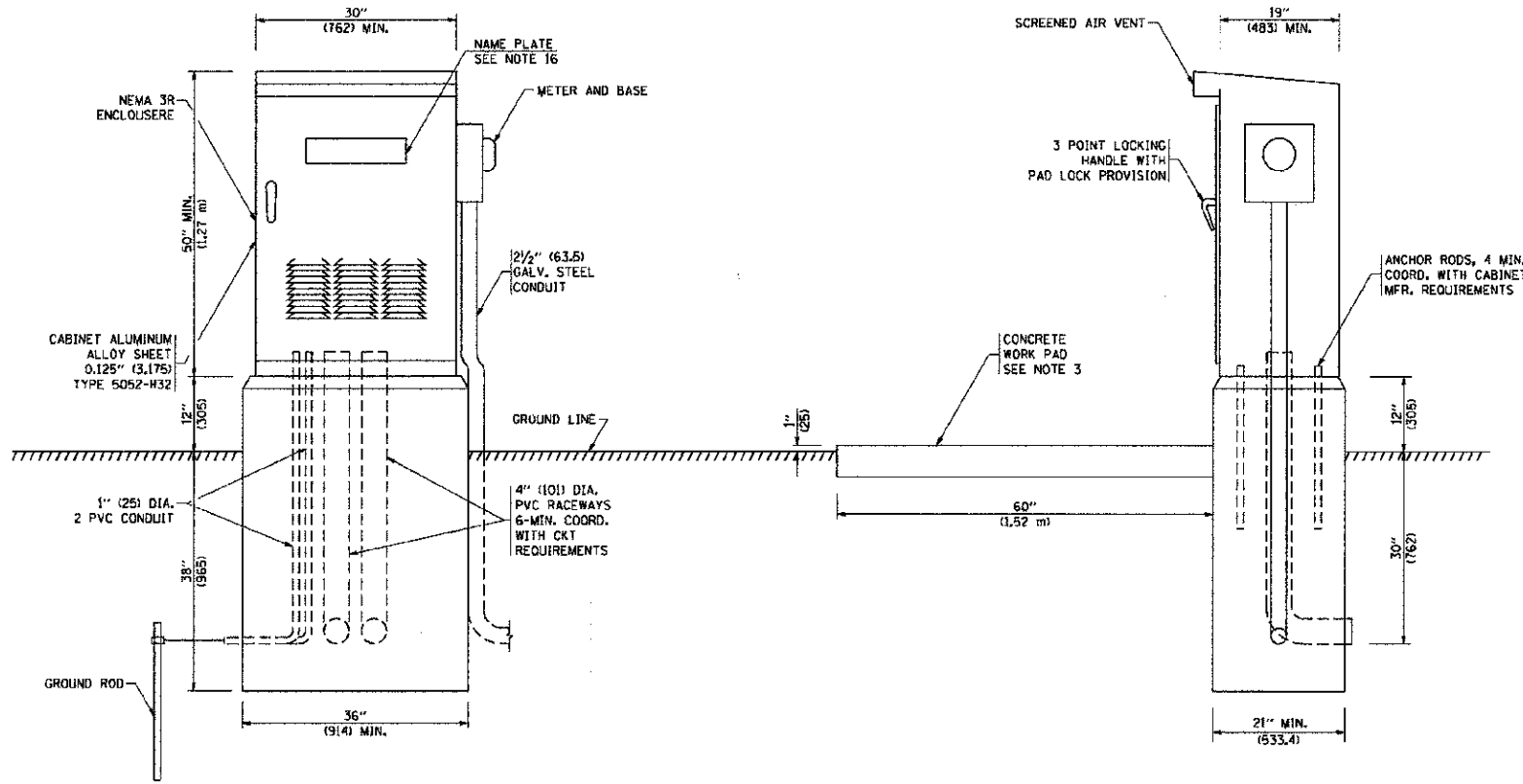
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REVISIONS	
APPROVED	
ATTORNEY CERTIFIED	
NO. FILE NAME	
PLAN	
NOTE BOOK	
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PANEL WIRING DIAGRAM

PANEL EQUIPMENT

BILL OF MATERIAL		
ITEM	QUANTITY	DESCRIPTION
A	1	MAIN CIRCUIT BREAKER, 2 POLE, 600 VOLT 100 AMP. FRAME, 100 AMP. NON-INTERCHANGEABLE TRIP INTERRUPTING RATING NEMA-22000 AMP. AT 240 VOLT.
B	1	REMOTE CONTROL SWITCH, ELECTRICALLY OPERATED, MECHANICALLY HELD, 2 POLE, SINGLE THROW, 100 AMP., 600 VOLTS CONTROL CIRCUIT 120 VOLT.
C	6	CIRCUIT BREAKERS, 2 POLE, 100AMP. FRAME, 20 AMP. NON-INTERCHANGEABLE TRIP INTERRUPTING RATING NEMA-10,000 AMP. AT 240 V.
D	2	CONTROL CIRCUIT-CIRCUIT BREAKER, 1 POLE, 120 V., 200 AMP. FRAME, 15 AMP. NON-INTERCHANGEABLE TRIP INTERRUPTING RATING NEMA-10,000 AMP. AT 120 V.
E	1	H-O-A SWITCH
F	1	PHOTOCELL
G	-	-
H	1	SPST 20A SWITCH ON DOOR, TO TURN LIGHT ON WHEN DOOR IS OPEN,
I	1	INCANDESCENT LIGHTING FIXTURE ENCLOSED AND GASKETED WITH 60 WATT, 120 V. LAMP.
J	1	20 A., 120 V., DUPLEX RECEPTACLE, GFCI.
K	1	COPPER NEUTRAL BUS 1/4" (6.35) X 1" (25.4) X 12" (304.8 mm) LONG MOUNTED ON PANEL WITH LUGS AND 4 SPARE LUGS
L	1	COPPER GROUND BUS 1/4" (6.35) X 1" (25.4) X 12" (304.8 mm) LONG MOUNTED ON PANEL WITH LUGS AND SPARE LUGS



NOTES:

- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- FOUNDATION SIZE SHALL BE COORDINATED WITH CABINET SIZE AND MFR.
- IN FRONT OF CONTROL CABINET DOOR, REMOVE VEGETATION AND 2" (50.8 mm) TOP SOIL. LEVEL THE AREA AND ON TOP, PLACE LENGTH WISE PARALLEL TO CONTROL CABINET, A CONCRETE PAD 36" (914.4 mm) x 60" (18.288 m) x 4" (101 mm) MIN. SIZE. THE COST OF LABOR AND MATERIALS ARE INCLUDED IN THE COST OF THE CONTROLLER.
- DOOR SHALL BE CONSTRUCTED FROM SAME TYPE OF MATERIAL AND THICKNESS AS CABINET.
- DOOR SHALL BE EQUIPPED WITH THREE POINT LATCHING MECHANISM WITH NYLON ROLLERS AT TOP THE BOTTOM.
- DOOR HINGE SHALL BE A HEAVY GAUGE CONTINUOUS HINGE WITH A 1/4" (6.35 mm) DIA. STAINLESS STEEL HINGE PIN.
- ALL EXTERNAL HARDWARE SHALL BE STAINLESS STEEL.
- CONTROL WIRING TO BE #12 AWG, 600V, TYPE "SIS" GRAY SWITCH BOARD WIRE, STRANDED COPPER.
- METER BOX SHALL BE MOUNTED ON THE SIDE OF CONTROL CABINET, NEAR TO THE SERVICE POLE.
- NOT USED
- THE HEADS OF CONNECTORS SCREWS SHALL BE PAINTED WHITE FOR NEUTRAL BAR CONNECTION AND GREEN FOR GROUND BAR CONNECTORS.
- ALL WIRING WITHIN THE CABINET SHALL BE COLOR CODED AS INDICATED.  
R = RED      BL = BLUE      W = WHITE  
B = BLACK      Y = YELLOW      G = GREEN
- PROVIDE SEALING GROMMETS FOR ALL OPEN WIRING EXTENDED FROM DEVICES IN BOXES OR CABINETS WITHIN THE CONTROL CABINET.
- ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.
- THE CONTROLLER SHALL BE CONSTRUCTED TO U.L. STD. 508 AND BEAR THE U.L. LABEL "ENCLOSED INDUSTRIAL CONTROL PANEL".
- 12" (304.8) X 16" (406.4 mm) STAINLESS STEEL EXTERIOR NAMEPLATE SHALL BE ENGRAVED TO "CITY OF ELMHURST LIGHTING CONTROLS" UNLESS OTHERWISE SPECIFIED.

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PLOT SCALE = 600.000 / FT.	CHECKED - JMG	REVISED -
PLOT DATE = 10/21/2012	DATE - 10/22/2012	REVISED -

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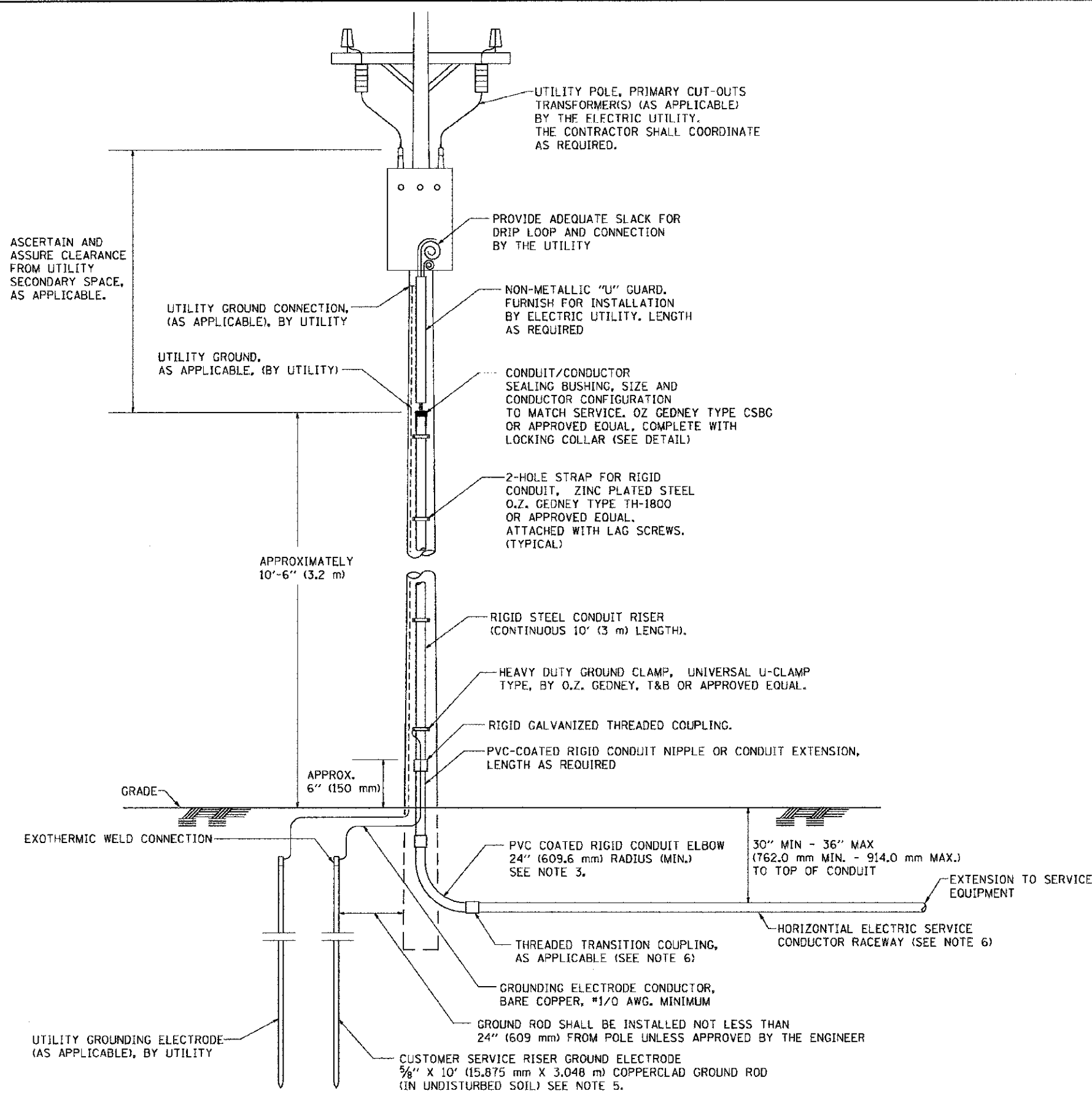
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BUTTERFIELD ROAD AT COMMONWEALTH LANE  
LIGHTING CONTROLLER

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

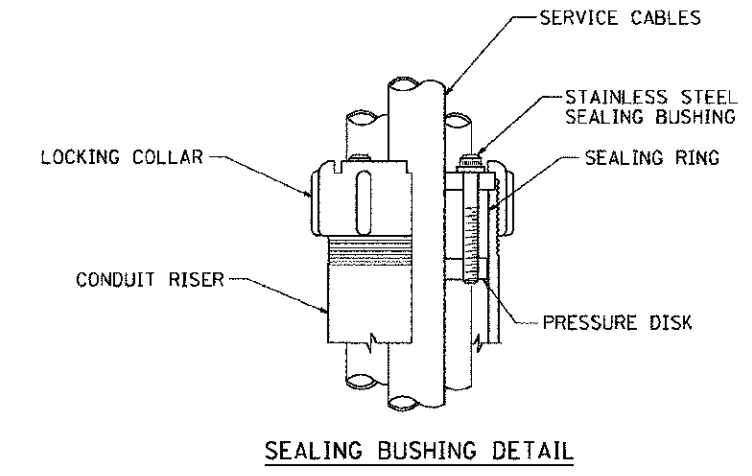
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEET NO.
3545	09-00170-00-CH	DUPAGE	108 66
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63742





**APPLICATION**  
 THIS DETAIL APPLIES FOR LOW VOLTAGE ELECTRIC SERVICE (660 V OR LESS) FROM AN OVERHEAD UTILITY SUPPLY TO SEPERATLY-MOUNTED SERVICE EQUIPMENT.

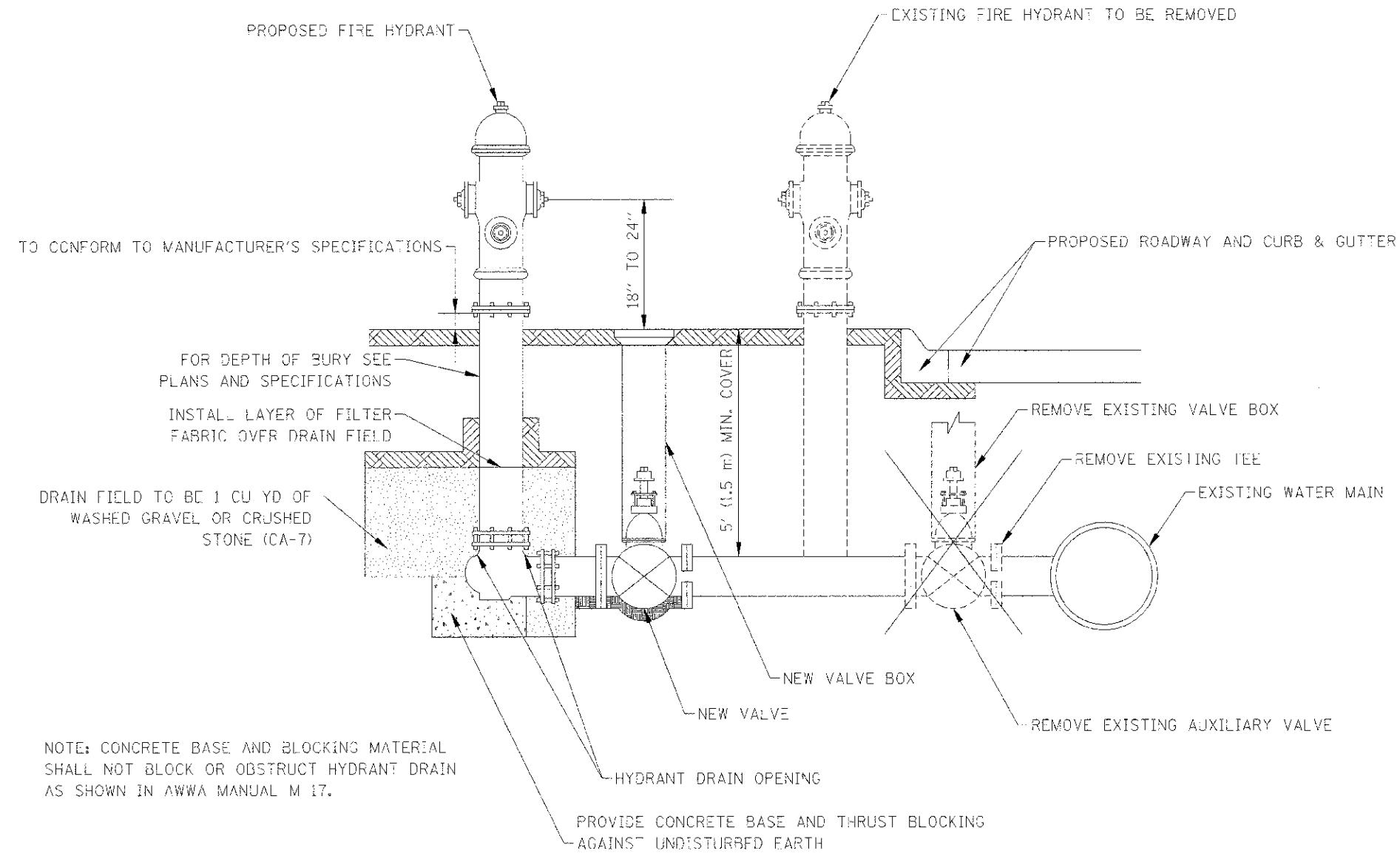
- NOTES**
- SERVICE VOLTAGE SHALL BE AS INDICATED ELSEWHERE IN THE DRAWINGS.
  - UNLESS OTHERWISE INDICATED, ITEMS AND WORK SHALL BE INCLUDED AND PAID AS PART OF THE ELECTRIC UTILITY SERVICE INSTALLATION PAY ITEM.
  - CONDUIT AND CONNECTOR DIAMETER SHALL MATCH THE DIAMETER OF THE SERVICE CONDUCTOR RACEWAY AS INDICATED ON THE PLANS.
  - PVC COATED RACEWAYS AND ACCESSORIES SHALL BE CAREFULLY INSTALLED WITH MFR RECOMMENDED TOOLS AND PROCEDURES TO AVOID DAMAGE. ANY DAMAGE SHALL BE REPAIRED WITH COMPATIBLE PVC TOUCH-UP MATERIAL TO THE SATISFACTION OF THE ENGINEER OR THE DAMAGED MATERIAL SHALL BE REPLACED AT NO ADDITIONAL COST.
  - THE CONTRACTOR SHALL OBTAIN INSPECTION AND APPROVAL BY THE ENGINEER OF SERVICE RISER GROUND ELECTRODE, RISER ELBOW, NIPPLE AND CONNECTION TO SERVICE CONDUCTOR RACEWAY EXTENSION BEFORE BACKFILL AND SHALL ALSO OBTAIN INSPECTION OF SERVICE RISER AND SEALING BUSHING BEFORE UTILITY "U" GUARD INSTALLATION AND SERVICE CONNECTION.
  - THE HORIZONTAL ELECTRIC SERVICE CONDUCTOR RACEWAY SHALL BE AS INDICATED AND SHALL BE MEASURED SEPARATELY FOR PAYMENT. WHEN THE RACEWAY IS PVC-COATED RIGID GALVANIZED STEEL, THE COUPLING SHALL BE THE SAME. WHEN THE RACEWAY IS PVC CONDUIT (IN CONCRETE), THE COUPLING SHALL BE A METALIC TO NON METALIC ADAPTER. WHEN THE RACEWAY IS ENCASED IN CONCRETE, THE CONCRETE SHALL EXTEND TO COVER THE COUPLING.
  - PLANS AND DETAILS INDICATE THE GENERAL NATURE AND REQUIREMENTS. THEY DO NOT SHOW EVERY ACCESSORY AND ATTACHMENT, AND THEY DO NOT RELIEVE THE CONTRACTOR OF THE REQUIREMENTS OF THE SPECIFICATIONS AND SPECIAL PROVISIONS TO ASCERTAIN UTILITY REQUIREMENTS AND TO COORDINATE ACCORDINGLY, FURNISHING ALL ITEMS AND WORK NOT PROVIDED BY THE UTILITY, BUT NECESSARY FOR A COMPLETE SERVICE INSTALLATION IS REQUIRED AND SHALL BE INCLUDED IN THE ELECTRIC UTILITY SERVICE INSTALLATION PAY ITEM.



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	PLOT SCALE = 5/8" = 1' IN.	DRAWN -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	3545	09-00170-00-CH	DLPAGE	108	67
	PLOT DATE = 1/4/2008	CHECKED - MEA	REVISED -				TO STA.	BE-220		CONTRACT NO. 63742		
		DATE -	REVISED -					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



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DATE	
NO. OF MAN. CHECKED	
NO.	
AUT. FILE NAME	



NOTE: CONCRETE BASE AND BLOCKING MATERIAL SHALL NOT BLOCK OR OBSTRUCT HYDRANT DRAIN AS SHOWN IN AWWA MANUAL M 17.

- SEQUENCE OF CONSTRUCTION:
1. SHUT DOWN EXISTING WATER MAIN
  2. REMOVE EXISTING HYDRANT, EXISTING VALVE BOX, AUXILIARY VALVE AND EXISTING TEE
  3. INSTALL HYDRANT EXTENSION, NEW VALVE AND NEW HYDRANT
  4. BACKFILL

- NOTES:
1. THE CONTRACTOR SHALL NOTIFY THE ENGINEER A MINIMUM OF 72 HOURS PRIOR TO WATER MAIN WORK
  2. THE CONTRACTOR SHALL NOTIFY THE BUSINESSES AND PROPERTY OWNERS AFFECTED BY THE WATER MAIN WORK A MINIMUM OF 48 HOURS PRIOR TO THE WORK.
  3. IF BENDS ARE NEEDED, 22° BENDS ARE PREFERRED, 45° BENDS ARE ACCEPTABLE AND 90° BENDS WILL NOT BE ALLOWED
  4. SEE SPECIAL PROVISION FOR ADDITIONAL DETAILS REGARDING THE PROPOSED HYDRANT AND SHUT DOWN RESTRICTIONS

FIRE HYDRANT INSTALLATION

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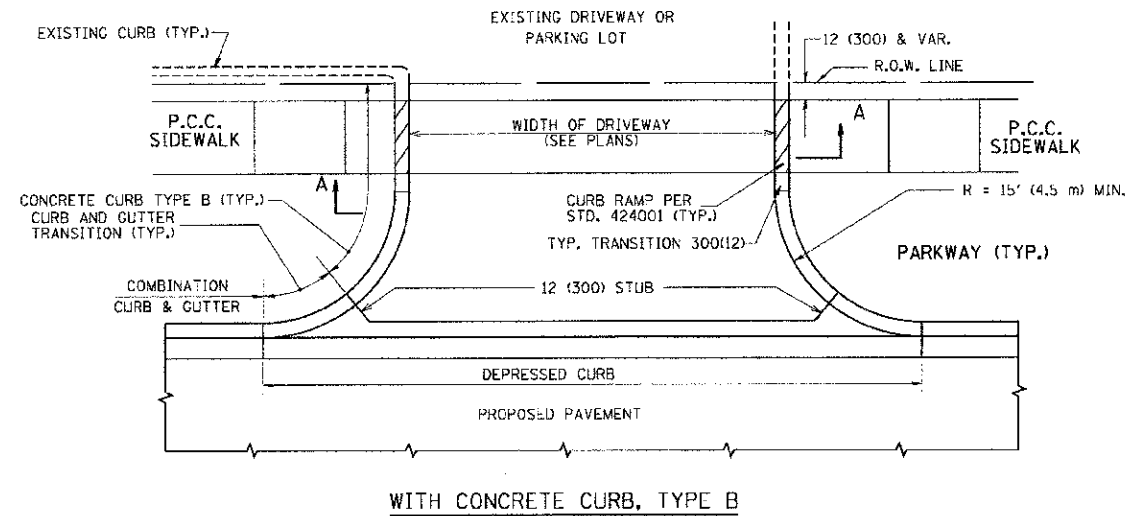
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

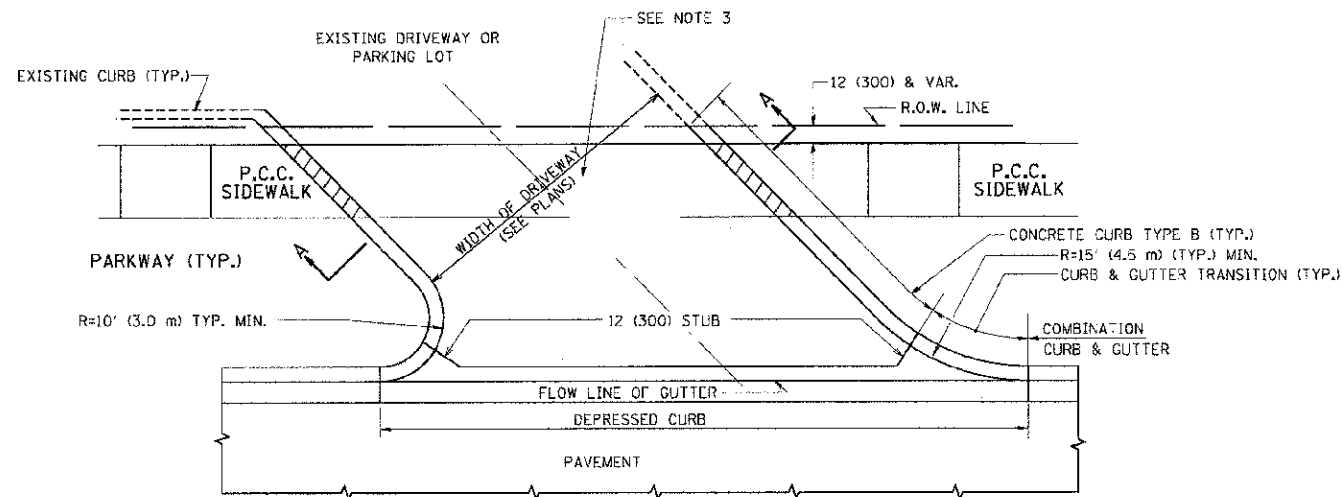
BUTTERFIELD ROAD AT COMMONWEALTH LANE  
PROJECT DETAILS

NOT TO SCALE SHEET NO. 69 OF 108 SHEETS

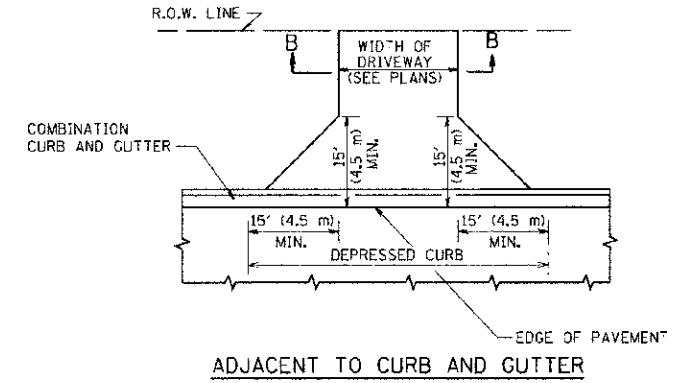
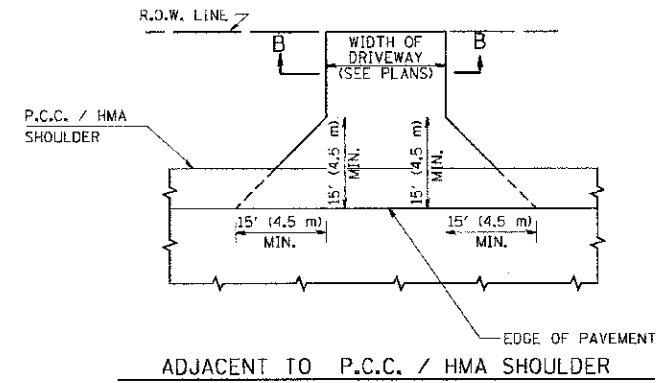
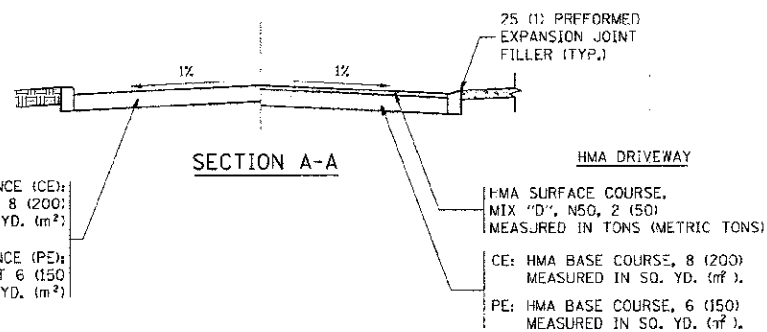
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	09-00170-00-CH	DUPAGE	108	69
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63742	



WITH CONCRETE CURB, TYPE B



WITH CONCRETE CURB, TYPE B



**GENERAL NOTES:**

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

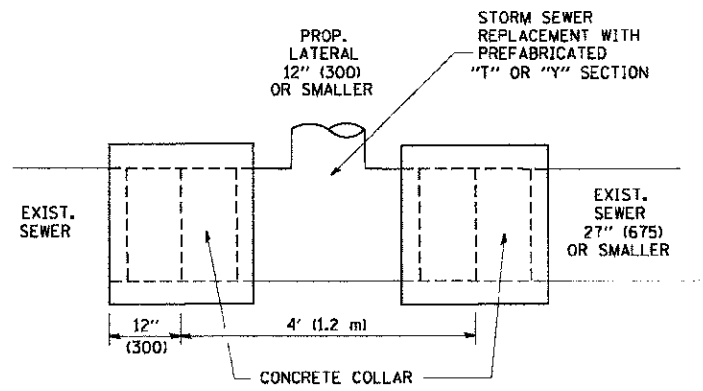
WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

FILE NAME =	USER NAME = lsguo	DESIGNED - R. SHAH	REVISED - P. LaFLUER 04-15-03
PROJECT WORK\proj\cvt\lsguo\20100915\6221.dwg		DRAWN -	REVISED - R. BCRO 01-01-07
	PLOT SCALE = 50.0000 1/1 in.	CHECKED -	REVISED - R. BCRO 06-11-08
	PLOT DATE = 9/6/2011	DATE - 11-04-95	REVISED - R. BCRO 09-06-11

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

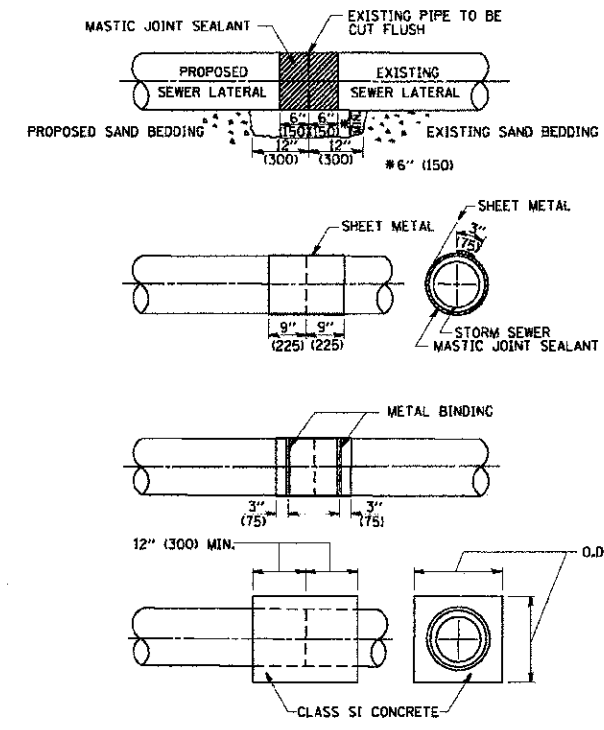
<b>DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB &amp; EDGE OF SHOULDER &gt;= 15' (4.5 m)</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	09-00170-00-CH		108	70
<b>BD0156-07 (BD-01)</b>			CONTRACT NO. 63742	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**DETAIL "A"**

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER

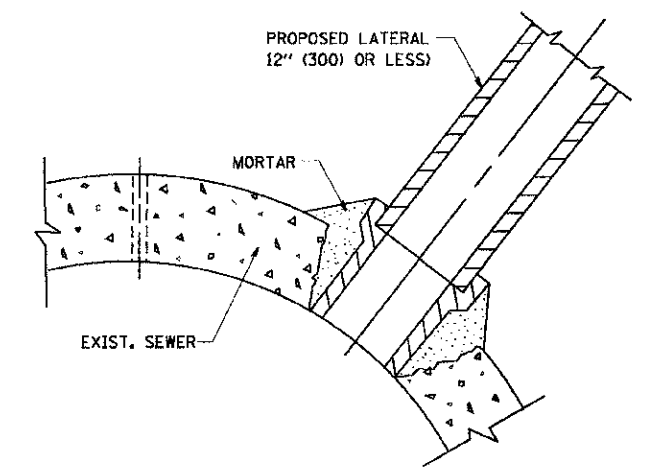


**DETAIL "B"**

CLASS SI CONCRETE COLLAR

**CONSTRUCTION SEQUENCE**

1. CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
2. APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
3. BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12" x 6" (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
4. CUT A PIECE OF SHEET METAL GAGE NO. 19 L1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
5. WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
6. LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
7. PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
8. WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OOZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
9. PLACE CLASS SI CONCRETE AROUND THE JOINT.



**DETAIL "C"**

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

**NOTES**

**MATERIAL**

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

**CONSTRUCTION METHODS**

- I. THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- II. CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
  - A) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
  - B) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".
- IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

**GENERAL**

- CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.
- CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

**BASIS OF PAYMENT**

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS. THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

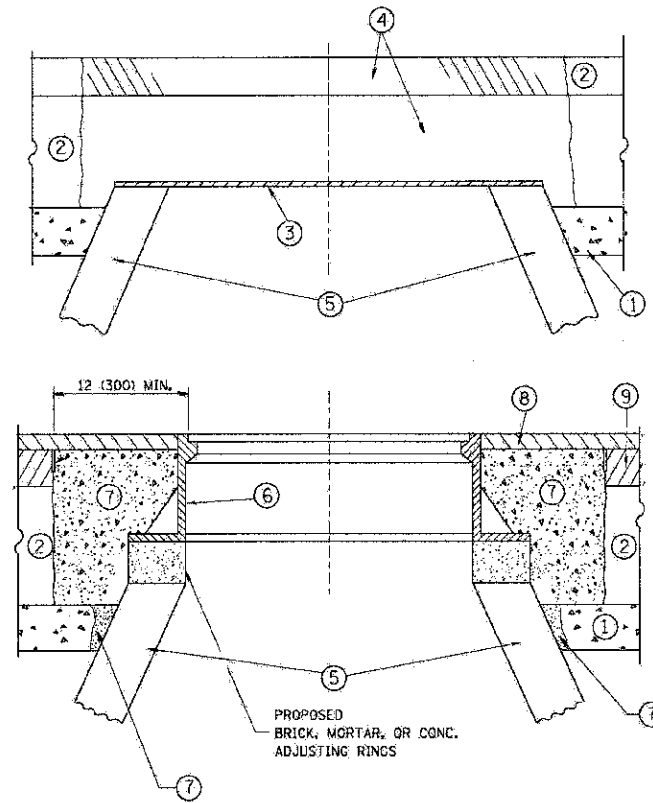
REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\data\22x34\bd07.dgn	USER NAME = gaglianob	DESIGNED - M. DE YONG	REVISED - M. DE YONG 05-08-92	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER</b>		F.A.U. RTE. 3545	SECTION 09-00170-00-CH	COUNTY DUPAGE	TOTAL SHEETS 108	SHEET NO. 71
	PLOT SCALE = 5/8" = 1' IN.	CHECKED -	REVISED - R. SHAH 10-25-94		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	<b>BD500-01 (BD-7)</b>		CONTRACT NO. 63742	
PLOT DATE = 1/4/2008	DATE - 07-25-90	REVISED - R. SHAH 06-12-96					FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				



**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1\* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

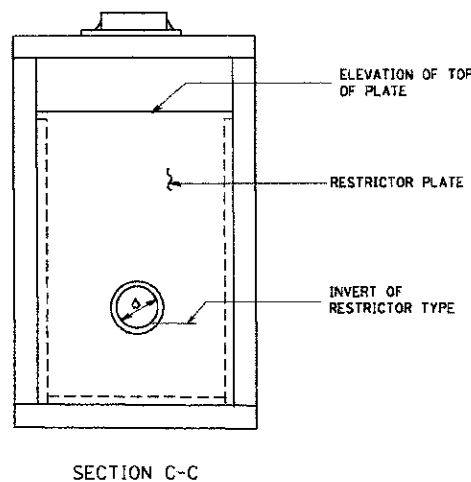
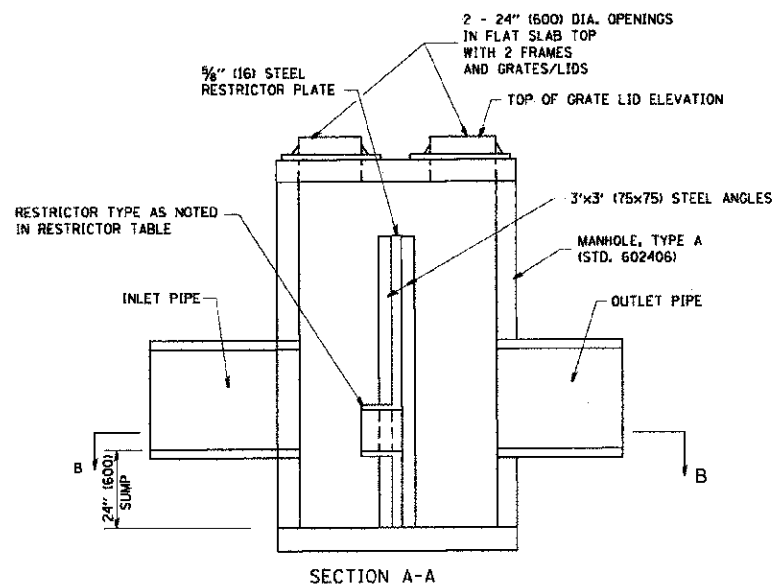
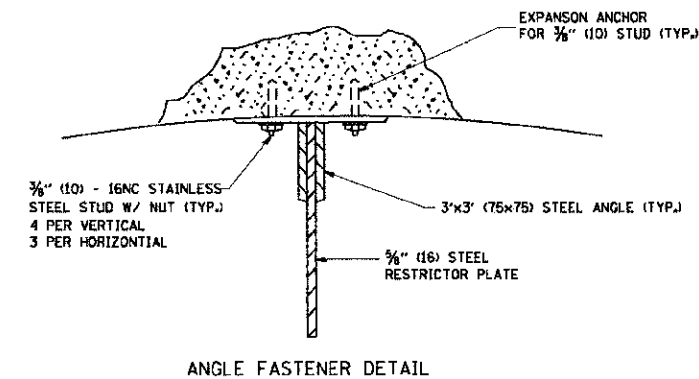
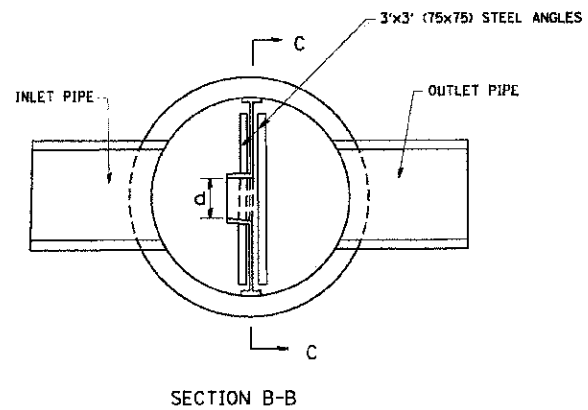
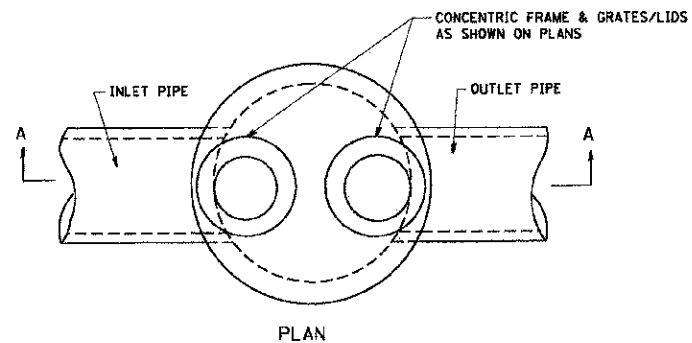
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = bouerdj	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
ca:\pwwork\p\p\do\ba\ca\ca\2108315\bd08.dgn		DRAWN -	REVISED - R. BORO 01-01-07
		CHECKED -	REVISED - R. BORO 03-09-11
		DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

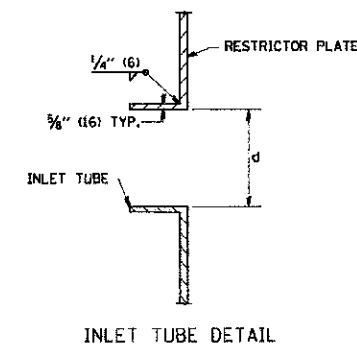
SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA.	TO STA.	F.A.J. RTE. 3545	SECTION 09-00170-00-CH	COUNTY	TOTAL SHEETS 108	SHEET NO. 72
						<b>BD600-03 (BD-8)</b>		CONTRACT NO. 63742		
FED. ROAD DIST. NO. 1 ILLINOIS/FED. AID PROJECT										



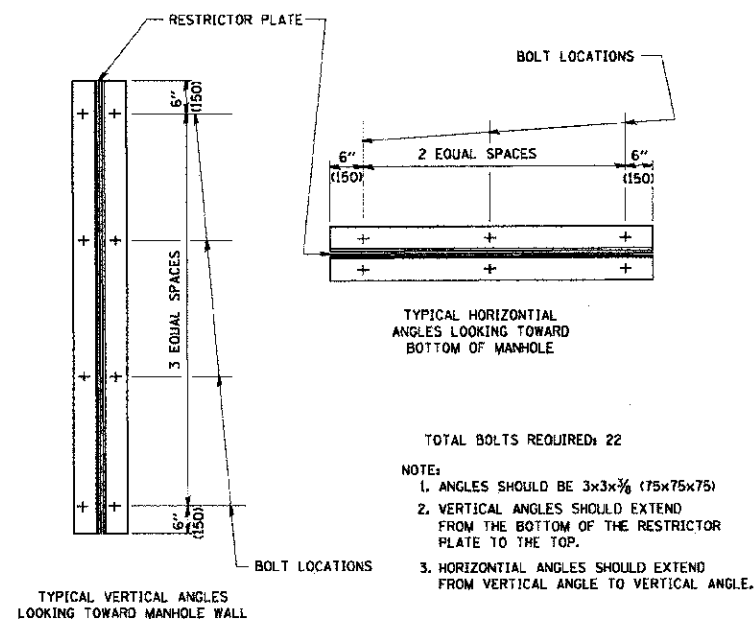


NOTES:

1. ALL STEEL ANGLES AND PLATES TO BE GALVANIZED AFTER FABRICATION.
2. ALL RESTRICTOR PLATES, ANGLES AND HARDWARE TO BE INCLUDED IN THE COST OF THE MANHOLE.
3. BASIS OF PAYMENT: "MANHOLES, TYPE A, 6 FT. (1.8 m)-DIAMETER, TYPE I FRAME, CLOSED LID, RESTRICTOR PLATE" EACH



STATION	MANHOLE DIAMETER	FRAME AND GRATE	RESTRICTOR TYPE	INSIDE RESTRICTOR TYPE DIAMETER in. (mm)	INVERT OF RESTRICTOR TYPE	ELEVATION OF TOP OF PLATE OVERFLOW
105+25.0, 55.0' RT	6'	TIF CL	SHARP EDGED	7.375	661.38	664.55

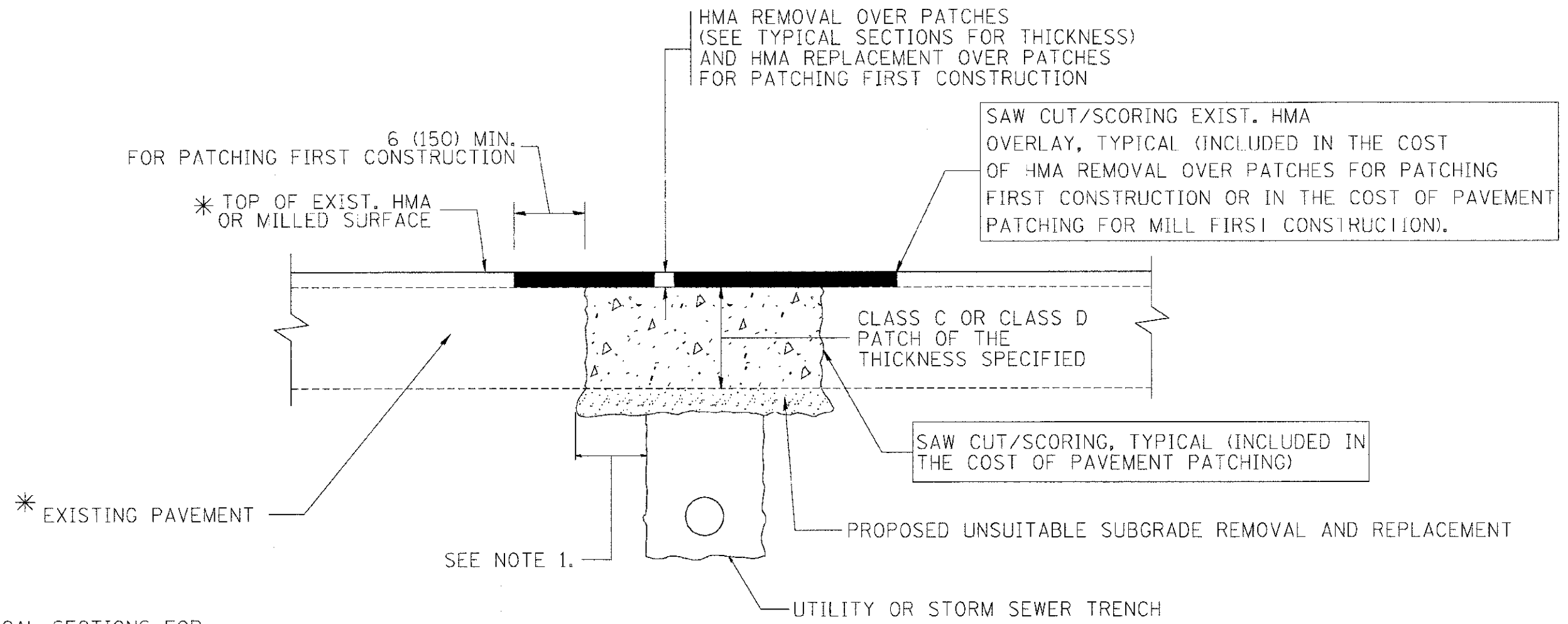


STEEL ANGLE BOLTING DETAILS

RESTRICTOR TYPE					
1	2	3	4	5	6
RE-ENTRANT TUBE	SHARP EDGED	SQUARE EDGED	RE-ENTRANT TUBE	SQUARE EDGED	ROUNDED
LENGTH 1/2 TO 1 DIA.		STREAM CLEARS SIDES	LENGTH: 2-1/2 DIA.	LENGTH: 2-1/2 DIA.	
C=.52	C=.61	C=.61	C=.73	C=.82	C=.98

VALUES OF "C" FOR CIRCULAR AND SQUARE ORIFICES

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

**SEQUENCE OF CONSTRUCTION (PATCHING FIRST)**

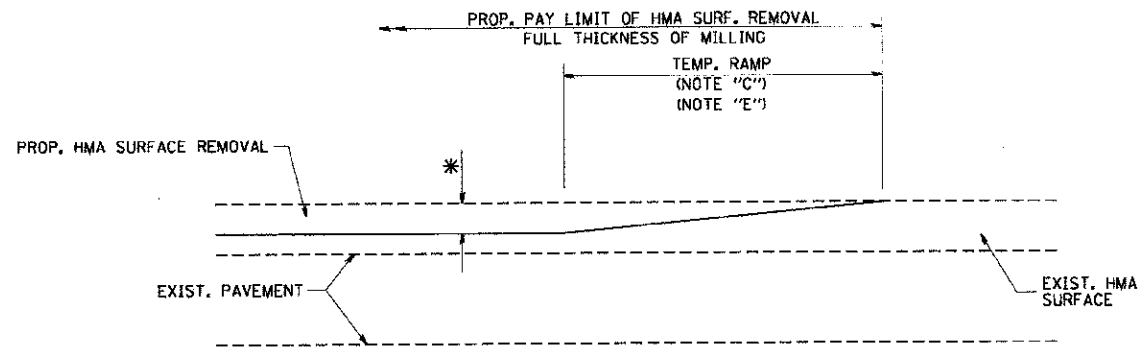
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

**SEQUENCE OF CONSTRUCTION (MILLING FIRST)**

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

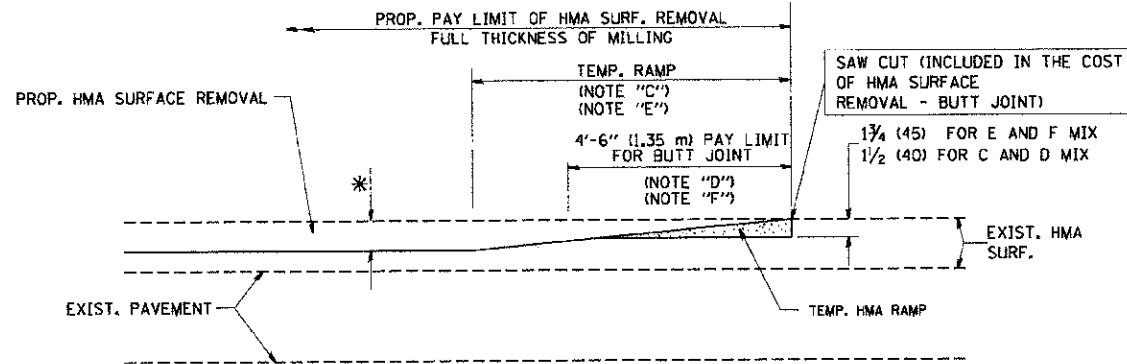
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\distatd22x34\bd22.dgn	USER NAME = bousrdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>			F.A.U. RTE. 3545	SECTION 09-00170-00-CH	COUNTY DUPAGE	TOTAL SHEETS 108	SHEET NO. 74
	PLOT SCALE = 50.000' / 1"	DRAWN -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1	OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT		
	PLOT DATE = 10/27/2008	CHECKED -	REVISED - R. BORO 09-04-07					<b>BD400-04 (BD-22)</b>			CONTRACT NO. 63742	
		DATE - 10-25-94	REVISED - K. ENG 10-27-08									



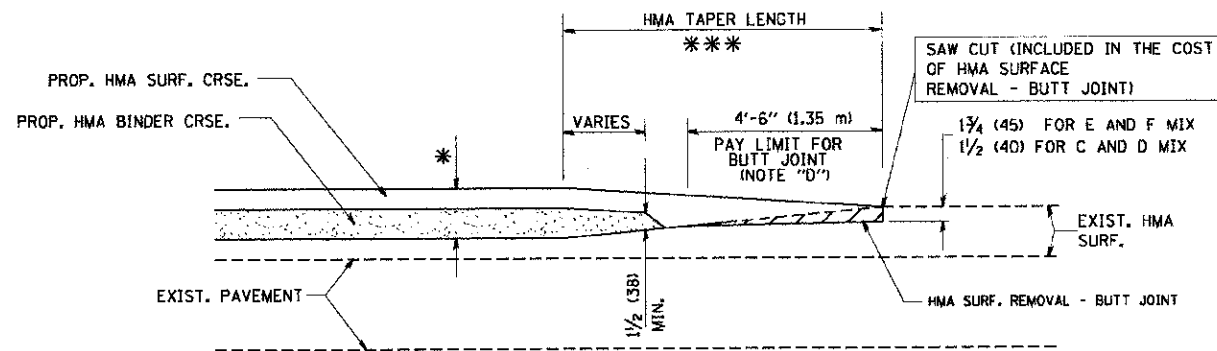
MILLED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

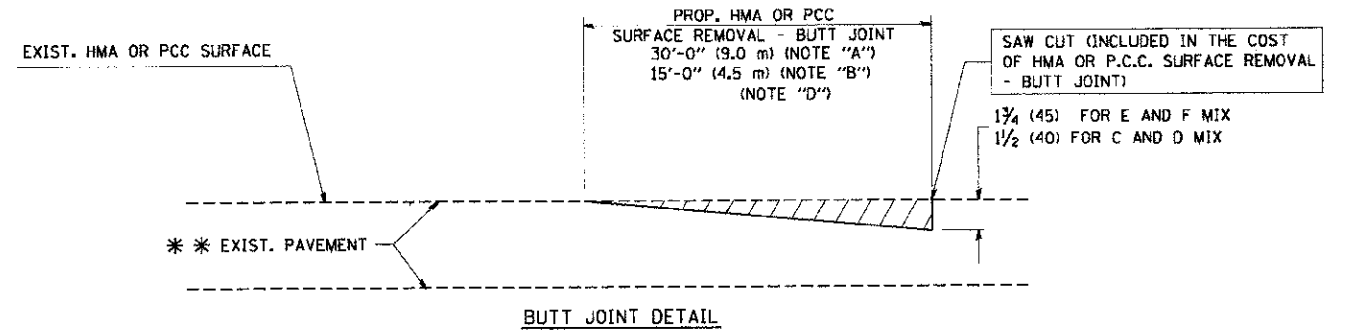


HMA CONSTRUCTED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

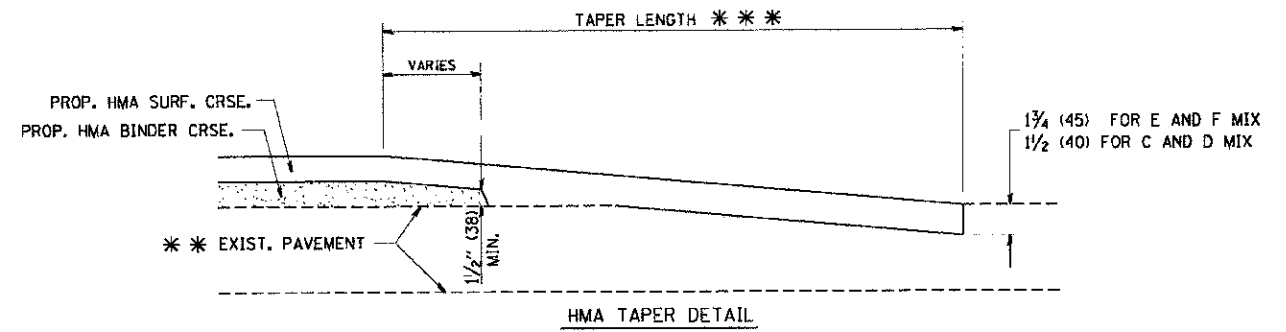
OPTION 2  
TYPICAL TEMPORARY RAMP



BUTT JOINT AND HMA TAPER  
TYPICAL BUTT JOINT AND HMA TAPER  
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER  
FOR RESURFACING ONLY

\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\a\tsd\22x34\bu32.dgn	USER NAME = gaglionobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 80.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2006	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	3545	09-00170-00-CH	DUPAGE	108	75
STA.	TO STA.	BD400-05 BD32		CONTRACT NO. 63742		
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT						

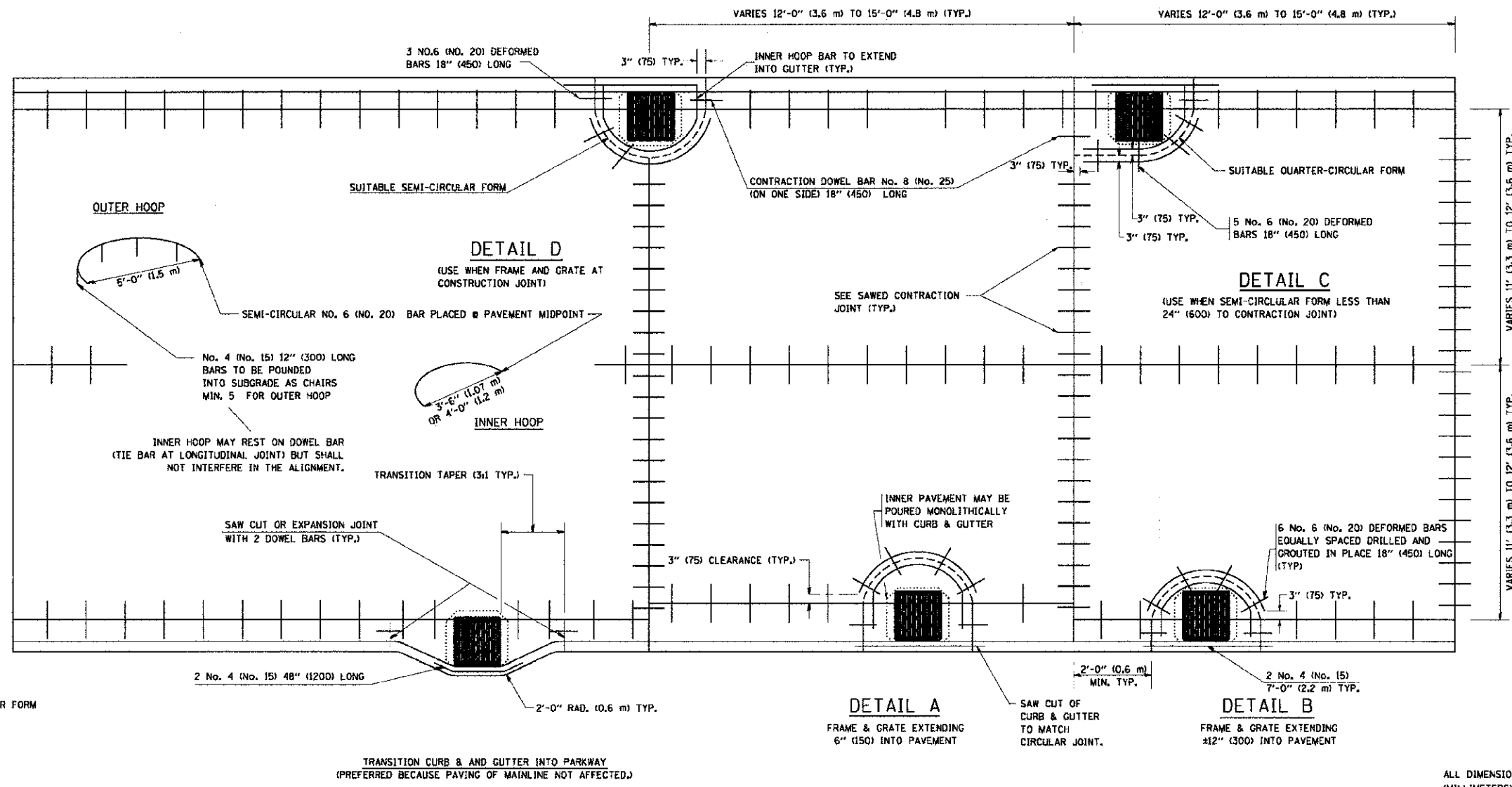


FRAME EXTENSION INTO PAVEMENT	INNER HOOP REINFORCEMENT DIAMETER	SEMI CIRCULAR FORM DIAMETER	OUTER HOOP REINFORCEMENT DIAMETER
UP TO 8" (200)	3'-6" (1.1 m)	4'-0" (1.2 m)	5'-0" (1.5 m)
> 8" (200) TO 14" (360)	4'-0" (1.2 m)	4'-6" (1.4 m)	5'-0" (1.5 m)

**DESIGNER NOTE:**  
THIS DETAIL IS TO BE USED  
WHEN THE GUTTER FLAG IS  
LESS THAN 24"

**NOTES:**

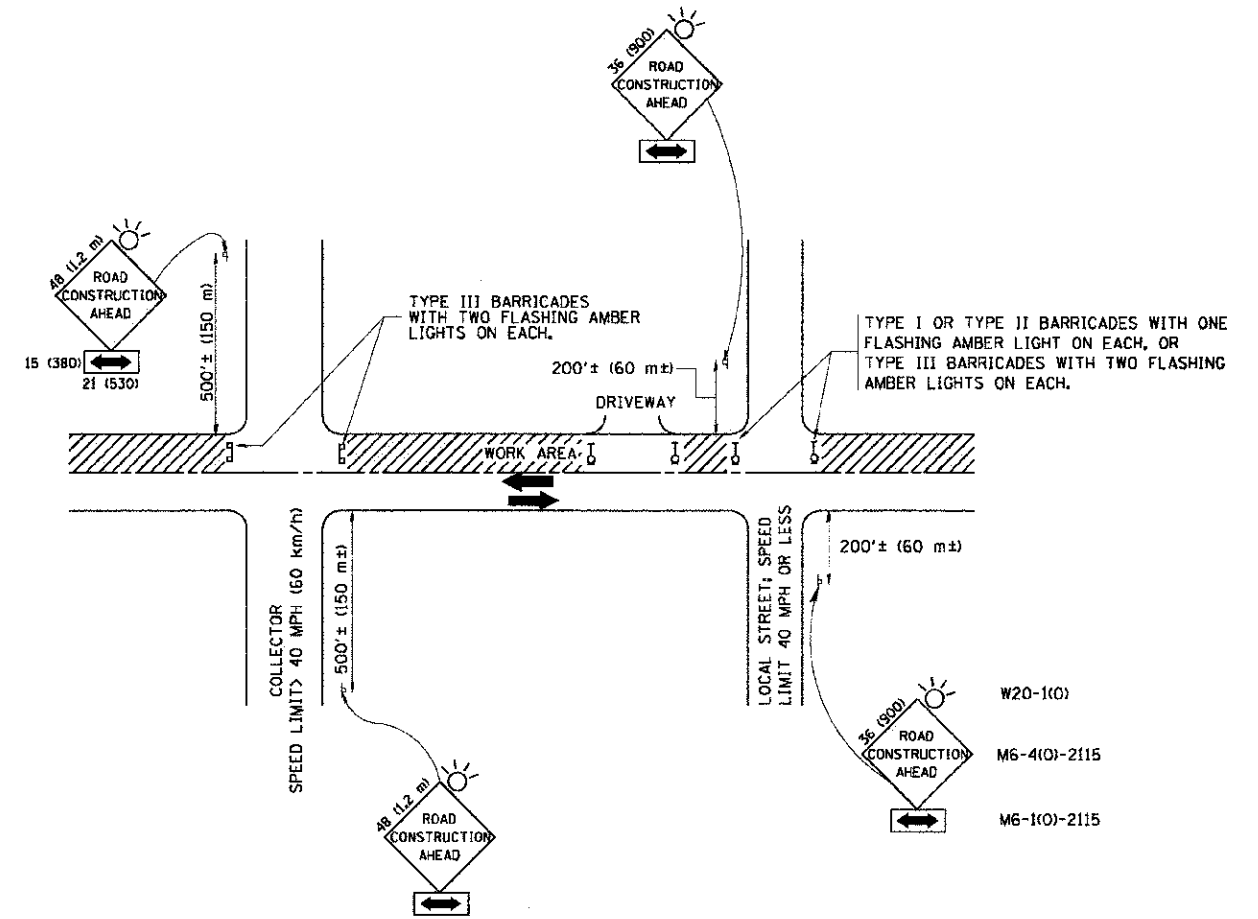
1. THE ROUNDOUT AND ADDED REINFORCEMENT WILL NOT BE PAID SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE PAVEMENT.
2. TRANSVERSE JOINTS MAY BE MOVED TO ACCOMMODATE ROUNDOUT, EDGE OF CIRCULAR JOINT SHALL BE MINIMUM 12" (300) FROM TRANSVERSE JOINT. RELOCATED TRANSVERSE JOINT SHALL BE CONTINUOUS FROM EDGE OF PAVEMENT TO EDGE OF PAVEMENT.
3. SEMI-CIRCULAR FORM SHALL BE REMOVED PRIOR TO DRILL AND GROUT OF TIE BARS.
4. ALL REINFORCED BARS SHALL BE EPOXY COATED.
5. DRILL AND GROUT IS PREFERRED, HOWEVER TIE BARS CAN BE POURED IN PLACE IF CLEARANCE IS PROVIDED TO OUTER EDGE OF FRAME. MINIMUM 2" (50) CLEARANCE.
6. WOOD SHIMS SHALL BE USED TO ADJUST ALL FRAMES. AFTER ADJUSTING MORTAR HAS CURED, THE WOOD SHIMS SHALL BE REMOVED AND THE VOIDS UNDER THE FRAMES FILLED WITH NON SHRINK GROUT.
7. HOOP REINFORCEMENT SHALL BE ONE PIECE CONSTRUCTION.
8. CIRCULAR FRAMES AND GRATES MAY BE SUBSTITUTED.
9. CURB DOWELS MUST BE PLACED LEVEL & TRUE TO ALLOW CONTRACTION MOVEMENT.



**LEGEND:**  
 ..... CASTING  
 - - - - - SUITABLE SEMI-CIRCULAR FORM

ALL DIMENSIONS ARE IN INCHES  
(MILLIMETERS) UNLESS OTHERWISE NOTED

FILE NAME = W:\dats\d\22x34\bd48.dgn	USER NAME = gaghiarob	DESIGNED - A. ABBAS	REVISED - T. MATOUSEK 08-28-00	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PCC PAVEMENT ROUNDOUTS AT CURB AND GUTTER</b>			F.A.U. RTE. 3545	SECTION 09-00170-00-CH	COUNTY DUPAGE	TOTAL SHEETS 108	SHEET NO. 77
PLOT SCALE = 50.0000' / 1"	CHECKED - A. ABBAS	DRAWN - TOM MATOUSEK	REVISED - T. MATOUSEK 10-02-00		SCALE: NONE	SHEET NO. 1	OF 1	SHEETS	STA.	TO STA.	CONTRACT NO. 63742	
PLOT DATE = 1/4/2008	DATE - 01-04-99	REVISOR - P. LAFLEUR 08-27-02	REVISOR - T. MATOUSEK 04-25-02		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							



**TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

**NOTES:**

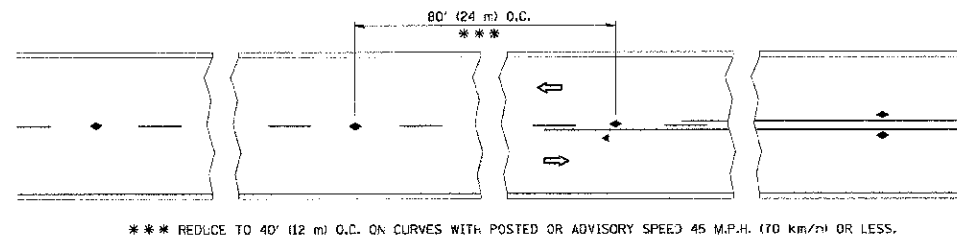
- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
  1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
 

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

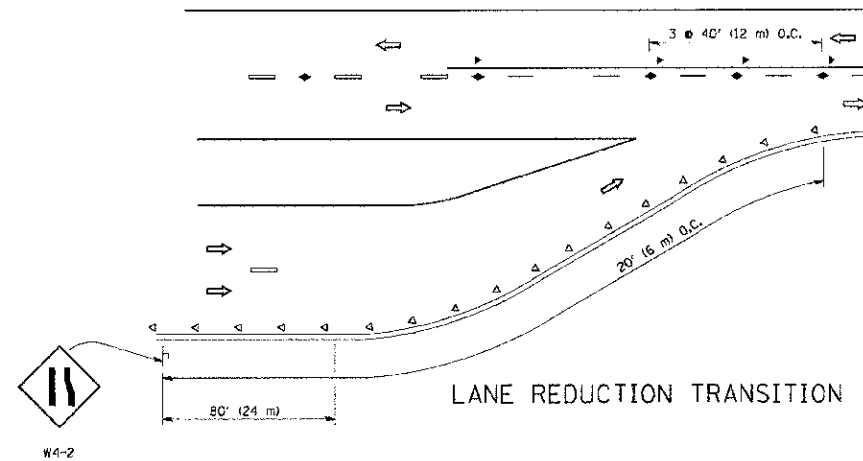
All dimensions are in millimeters (Inches) unless otherwise shown.

FILE NAME = W:\d:\state\22\34\to18.dgn	USER NAME = gsg11anobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS</b>			F.A.U. RTE. 3545	SECTION 09-00170-00-CH	COUNTY DUPAGE	TOTAL SHEETS 108	SHEET NO. 78
	PLOT SCALE = 50.000' / IN.	DRAWN -	REVISED - A. HOUSEH 03-06-96		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	TC-10		CONTRACT NO. 63742		
	PLOT DATE = 1/4/2008	CHECKED -	REVISED - A. HOUSEH 10-15-96									
		DATE - 06-89	REVISED - T. RAMWACHER 01-06-00									

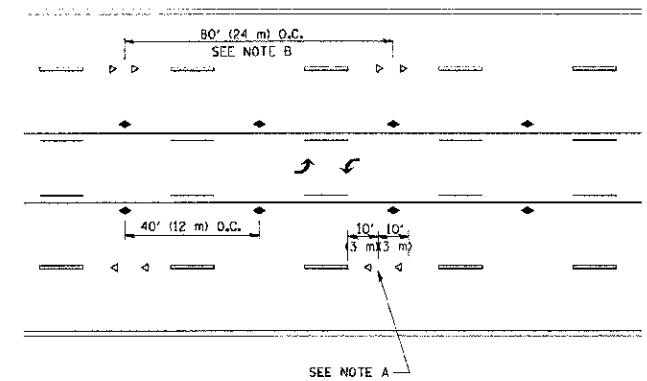




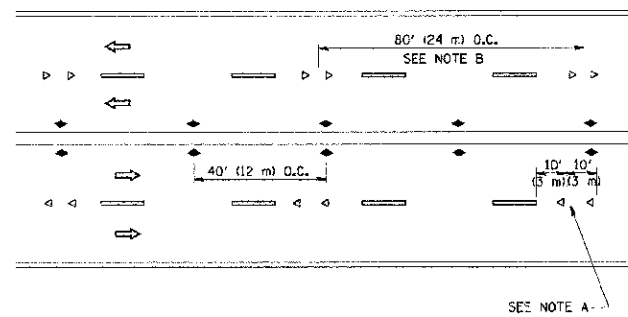
TWO-LANE/TWO-WAY



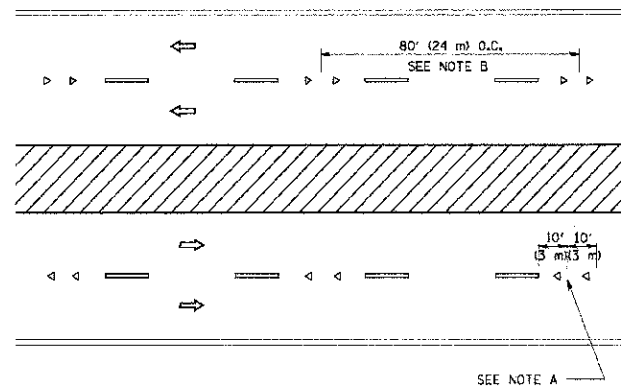
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

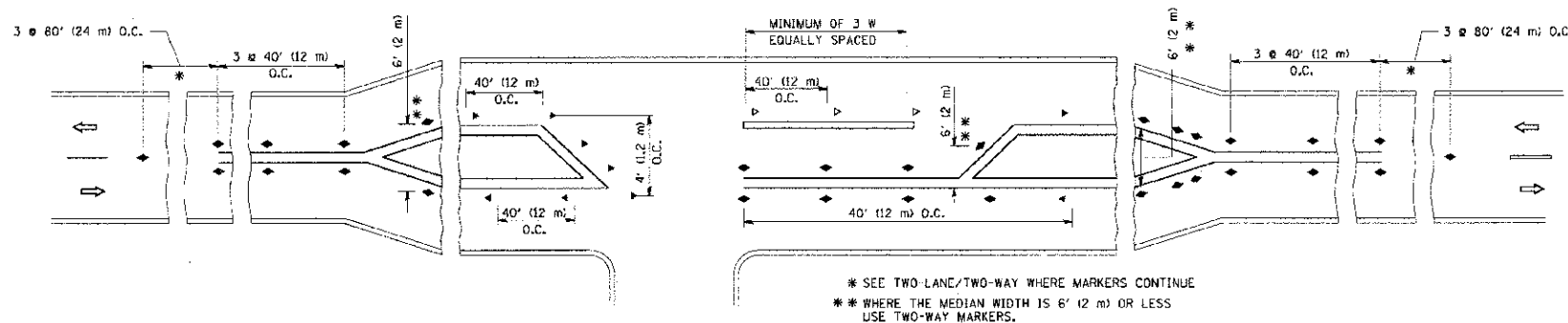
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

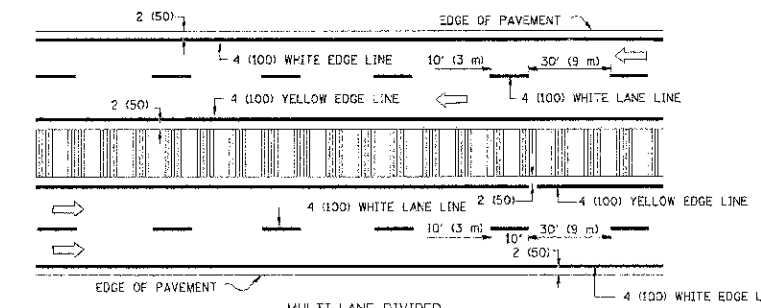
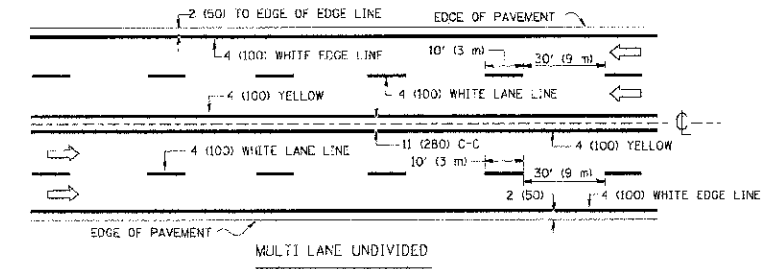
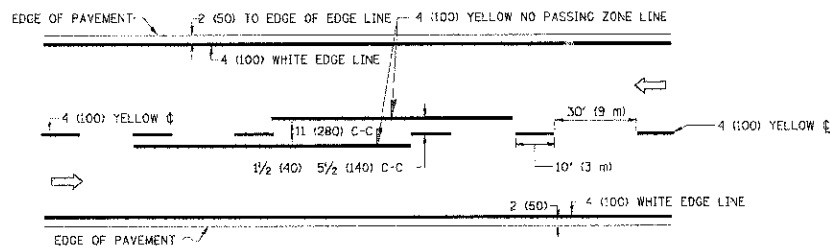
1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

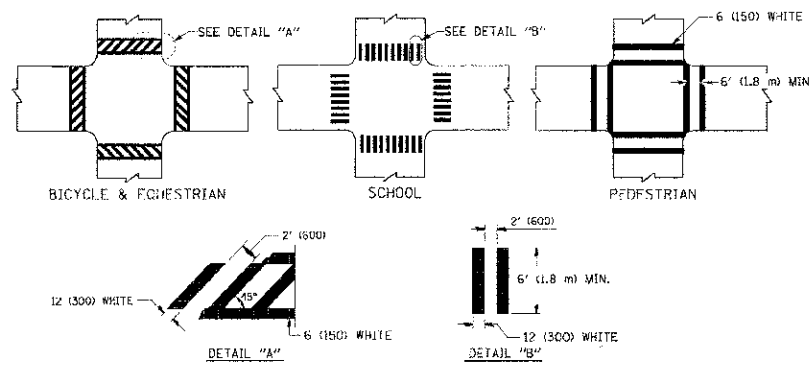
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = jaygo	DESIGNED =	REVISED - T. RAMMACHER 09-19-94	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TYPICAL APPLICATIONS</b>			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
en:\pwork\pwork\jaygo\61082015\tdt.tdgn		DRAWN =	REVISED - T. RAMMACHER 03-12-99		<b>RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)</b>			3545	09-00170-00-CH	DUPAGE	108	79
		CHECKED =	REVISED - T. RAMMACHER 01-06-00		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	<b>TC-11</b>			
		DATE = 3/2/2011	REVISED - C. JUCCIUS 09-09-09		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							

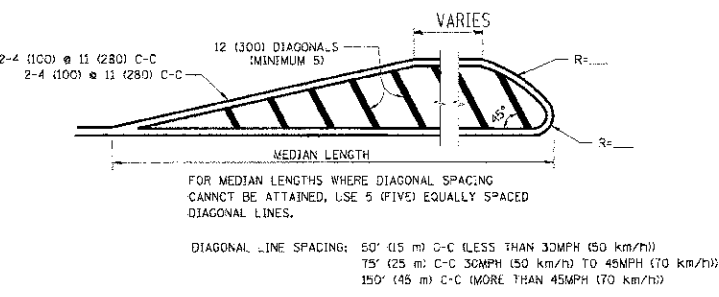
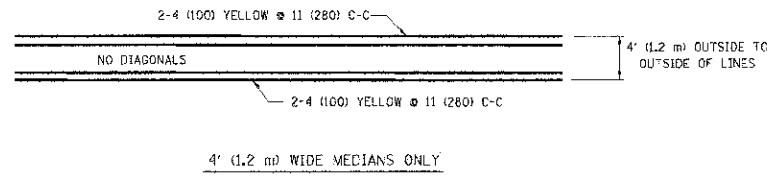


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

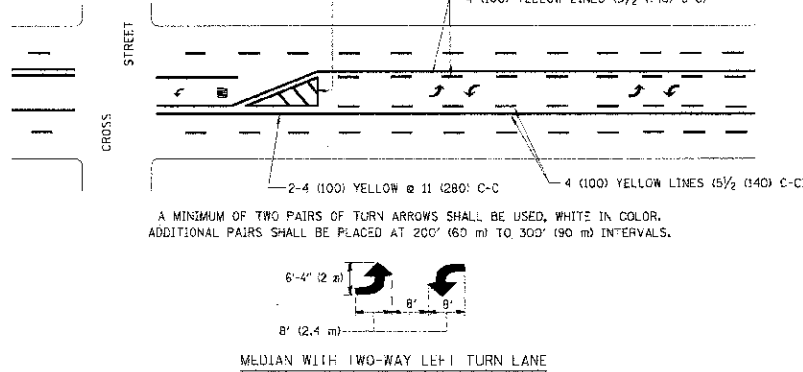
TYPICAL LANE AND EDGE LINE MARKING



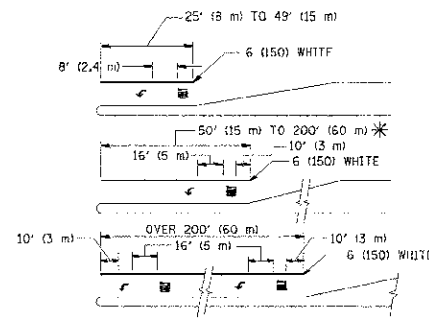
TYPICAL CROSSWALK MARKING



MEDIANS OVER 4' (1.2 m) WIDE



TYPICAL PAINTED MEDIAN MARKING

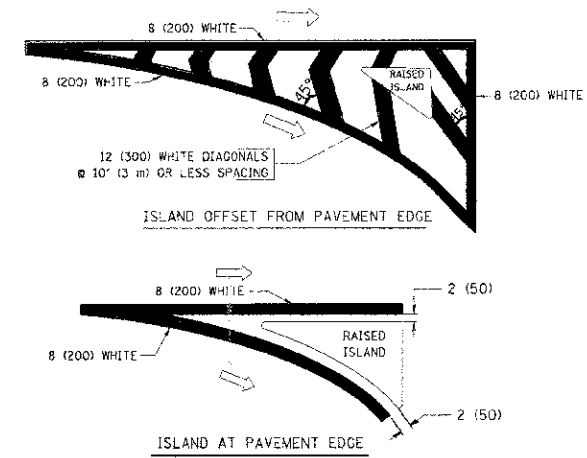


FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	3/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8" (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8" (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GOPE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 7800D. AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

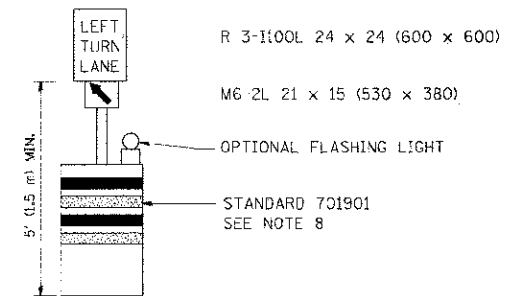
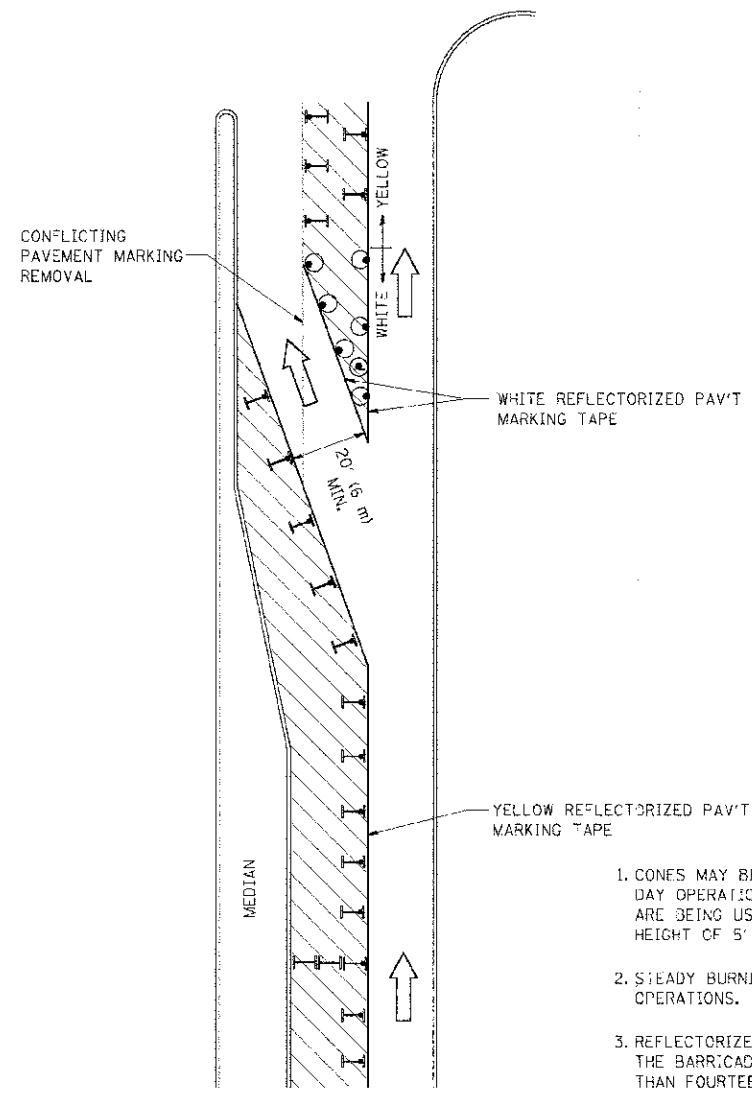
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 7800D.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME =	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
		DRAWN -	REVISED - C. JUCIUS 09-09-09
		CHECKED -	REVISED -
		DATE - 03-19-90	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE		F.A.I. RTE.:	SECTION	COUNTY	TOTAL SHEETS
TYPICAL PAVEMENT MARKINGS		3545	09-00170-00-CH	DUPAGE	108 / 90
SCALE: NONE		TC-13		CONTRACT NO. 63742	
SHEET NO. 1 OF 1 SHEETS		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

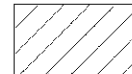







**GENERAL NOTES**

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 360) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

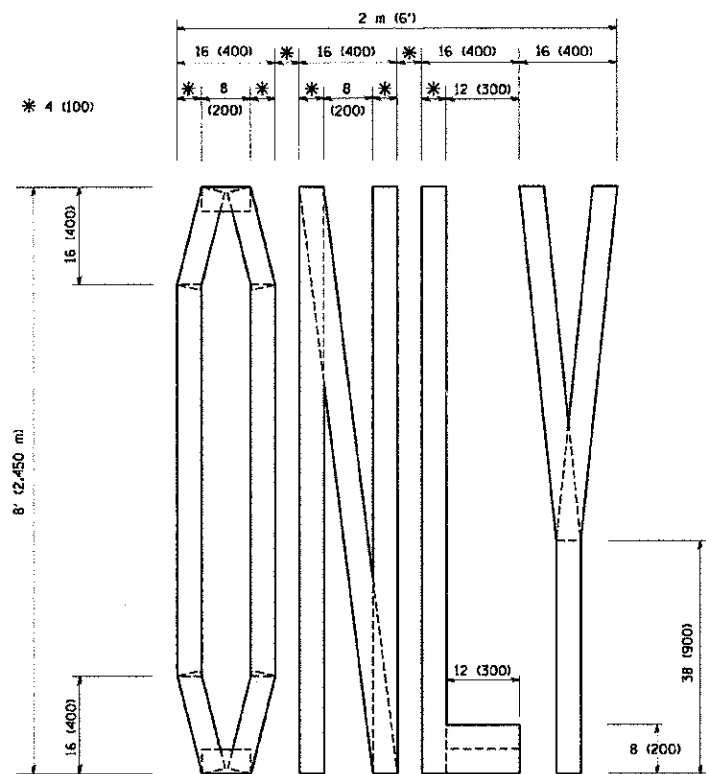
**LEGEND**

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

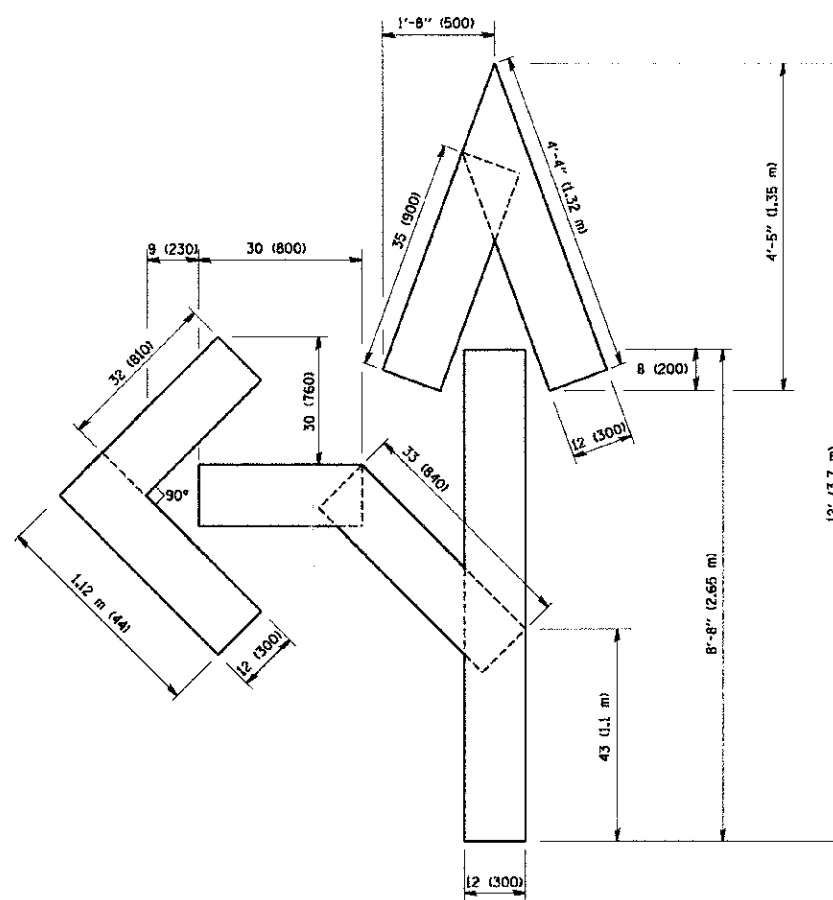
FILE NAME =	USER NAME = drsvkcsgh	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
en:\p\work\1\1\1001\DRY\AK050\00000010\1014.dgn		REVISED - A. HOUSEH 11-07-95	REVISED -
	PLCT SCALE = 49.9999 1/4 IN.	REVISED - A. HOUSEH 10-12-96	REVISED -
	PLCT DATE = 9/14/2009	REVISED - T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

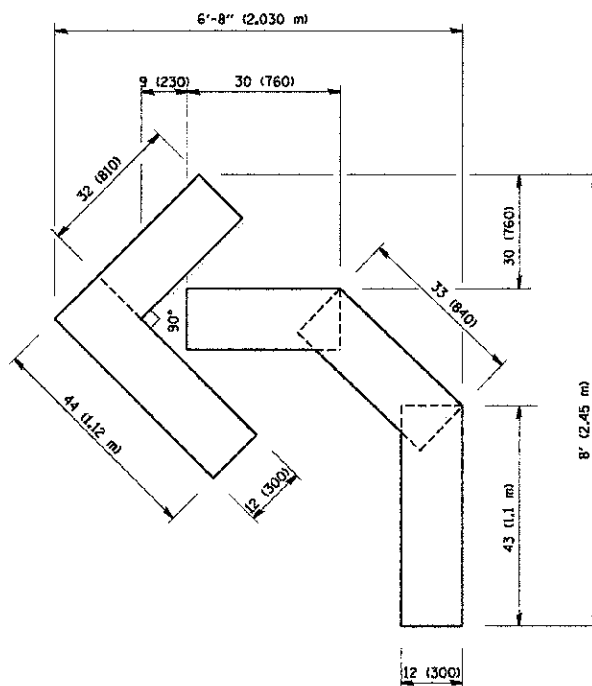
SCALE: NONE		SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	F.A.A.I. R.T.C.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
				3545	09-00170-00-CH	DUPAGE	108	81
				<b>TC-14</b>		<b>CONTRACT NO. 63742</b>		
				FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				



QUANTITY  
 4 (100) LINE = 64.1 ft. (19.7 m)  
 21.1 sq. ft. (1.97 sq. m)



QUANTITY  
 4 (100) LINE = 82.5 ft. (25.3 m)  
 27.5 sq. ft. (2.53 sq. m)



QUANTITY  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.39 sq. m)

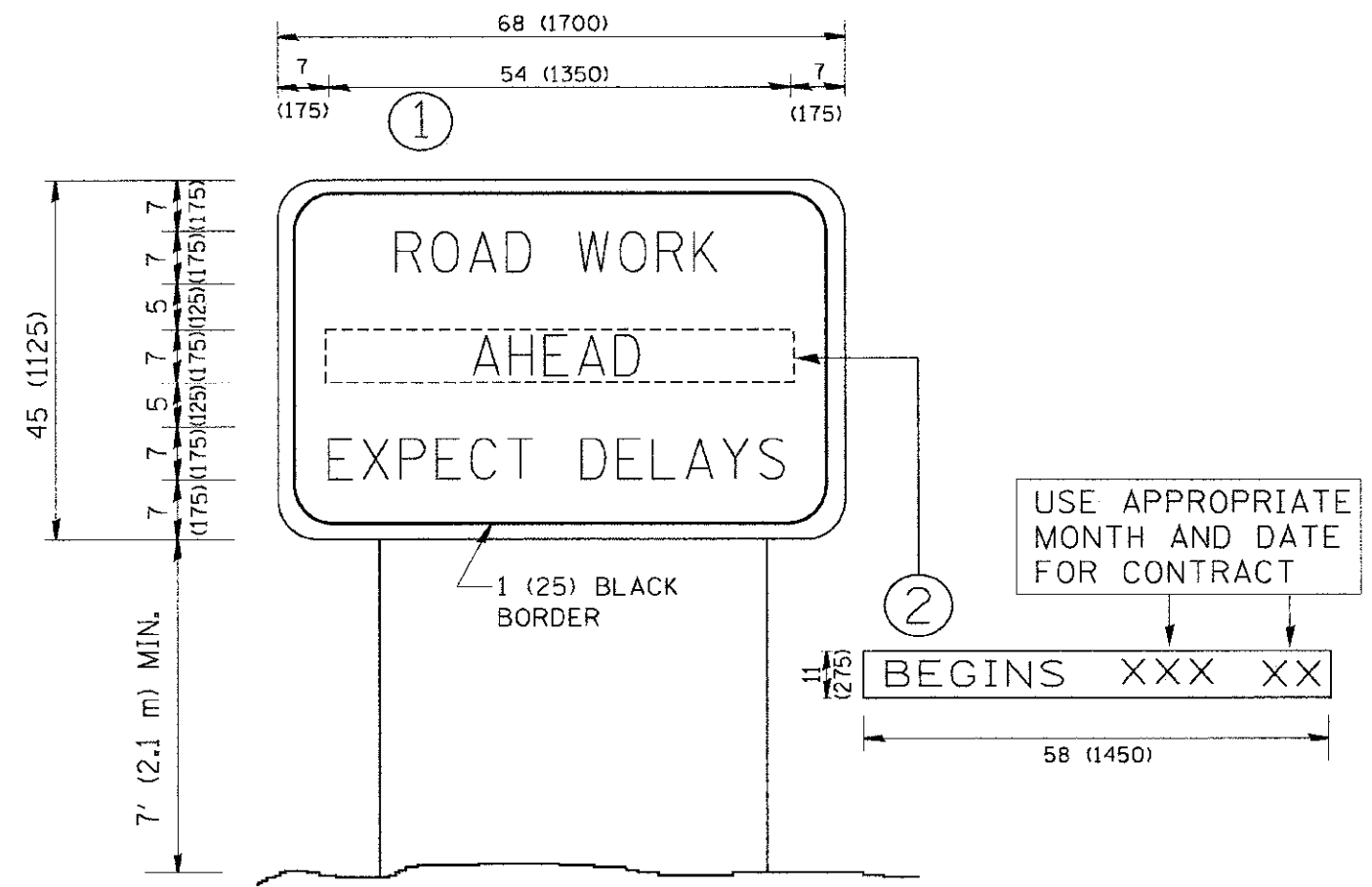
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = M:\data\22\34\1\16.dgn	USER NAME = geglianabt	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
		DRAWN -	REVISED -T. RAMMACHER 11-04-97
	PLOT SCALE = 50.0000' / 1"	CHECKED -	REVISED -T. RAMMACHER 03-02-98
	PLOT DATE = 1/4/2008	DATE = 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	09-00170-00-CH	DUPAGE	108	82
TC-16			CONTRACT NO. 63742	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

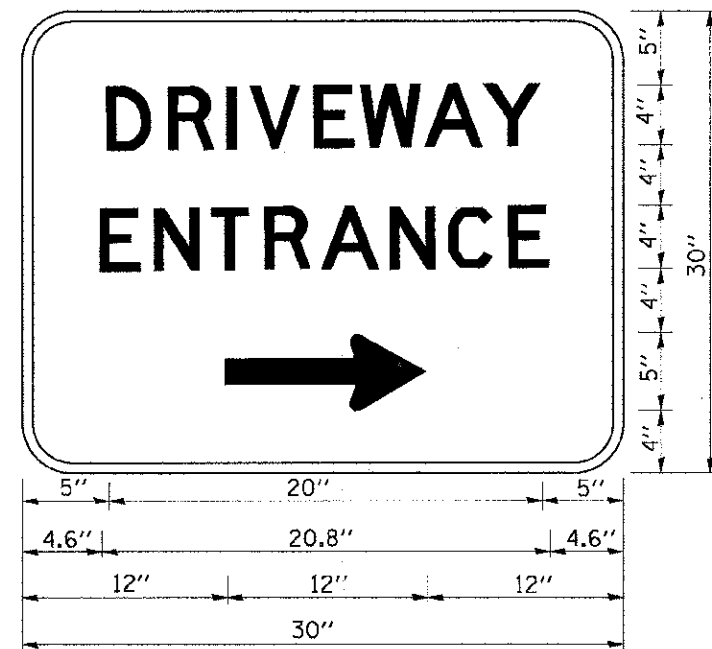


**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\dis\std\22x34\to22.dgn	USER NAME = gagliarobt	DESIGNED -	REVISED - R. MIRS 09-15-97	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ARTERIAL ROAD INFORMATION SIGN</b>			F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50,000' / IN.	DRAWN -	REVISED - R. MIRS 12-11-97		3545	09-00170-00-CH	DUPAGE	108	83			
	PLOT DATE = 1/4/2008	CHECKED -	REVISED - T. RAMMACHER 02-02-99		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.			TC-22		CONTRACT NO. 63742		
		DATE -	REVISED - C. JUCIUS 01-31-07		FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT							



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED  
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

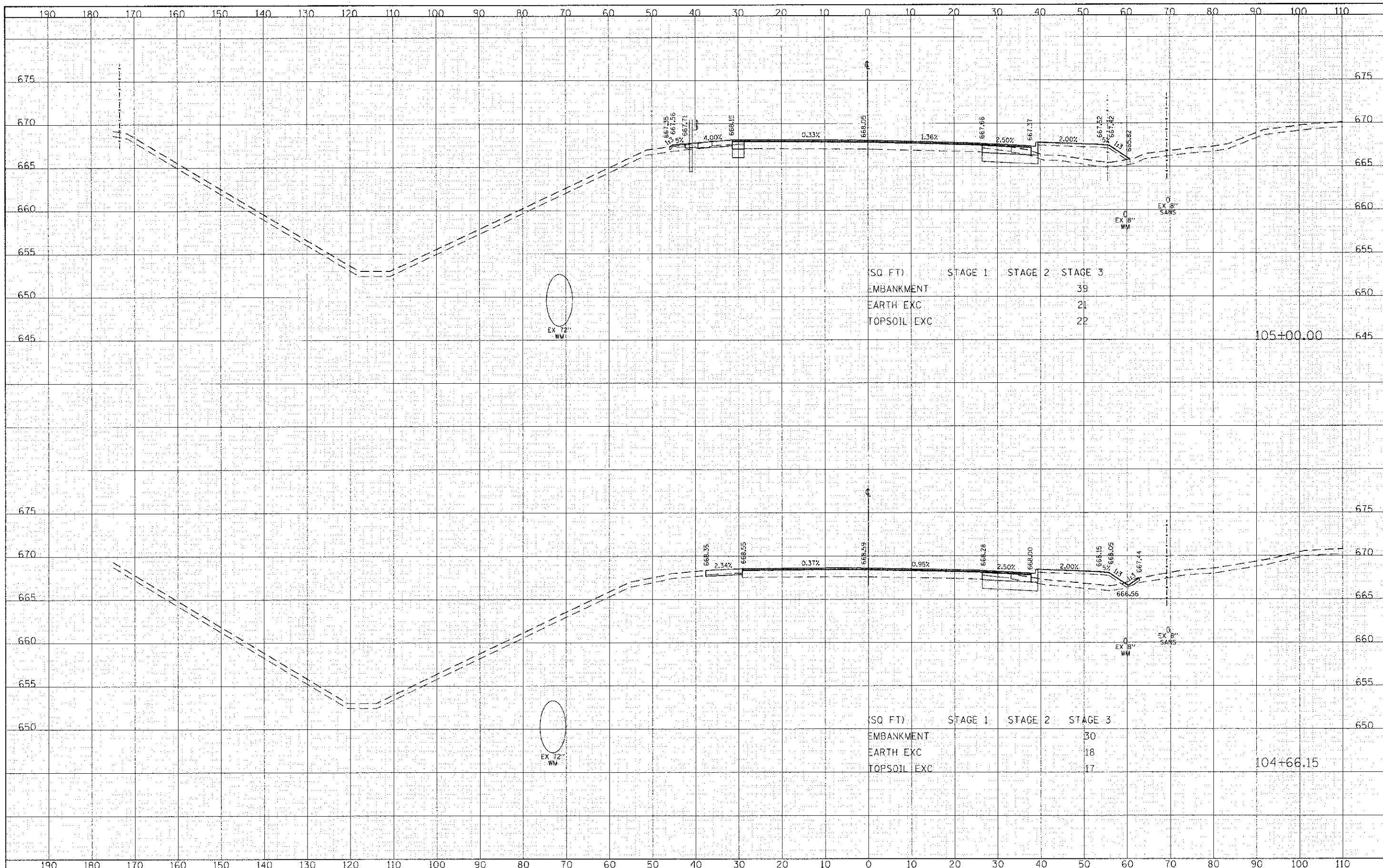
**NOTES:**

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME = W:\distatd\22x34\to28.dgn	USER NAME = goglianob	DESIGNED -	REVISED - C. JUCIUS 02-15-07	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DRIVEWAY ENTRANCE SIGNING</b>				F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50.000' / IN.	DRAWN -	REVISED -		3545	09-00170-00-CH	DUPAGE	108	84				
	PLOT DATE = 1/4/2008	CHECKED -	REVISED -		TC-26				CONTRACT NO. 63742				
		DATE -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1	SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

DATE	
BY	
FINAL SURVEY	
NOTE BOOK	
AREAS CHECKED	

DATE	
BY	
ORIGINAL SURVEY	
NOTE BOOK	
AREAS CHECKED	

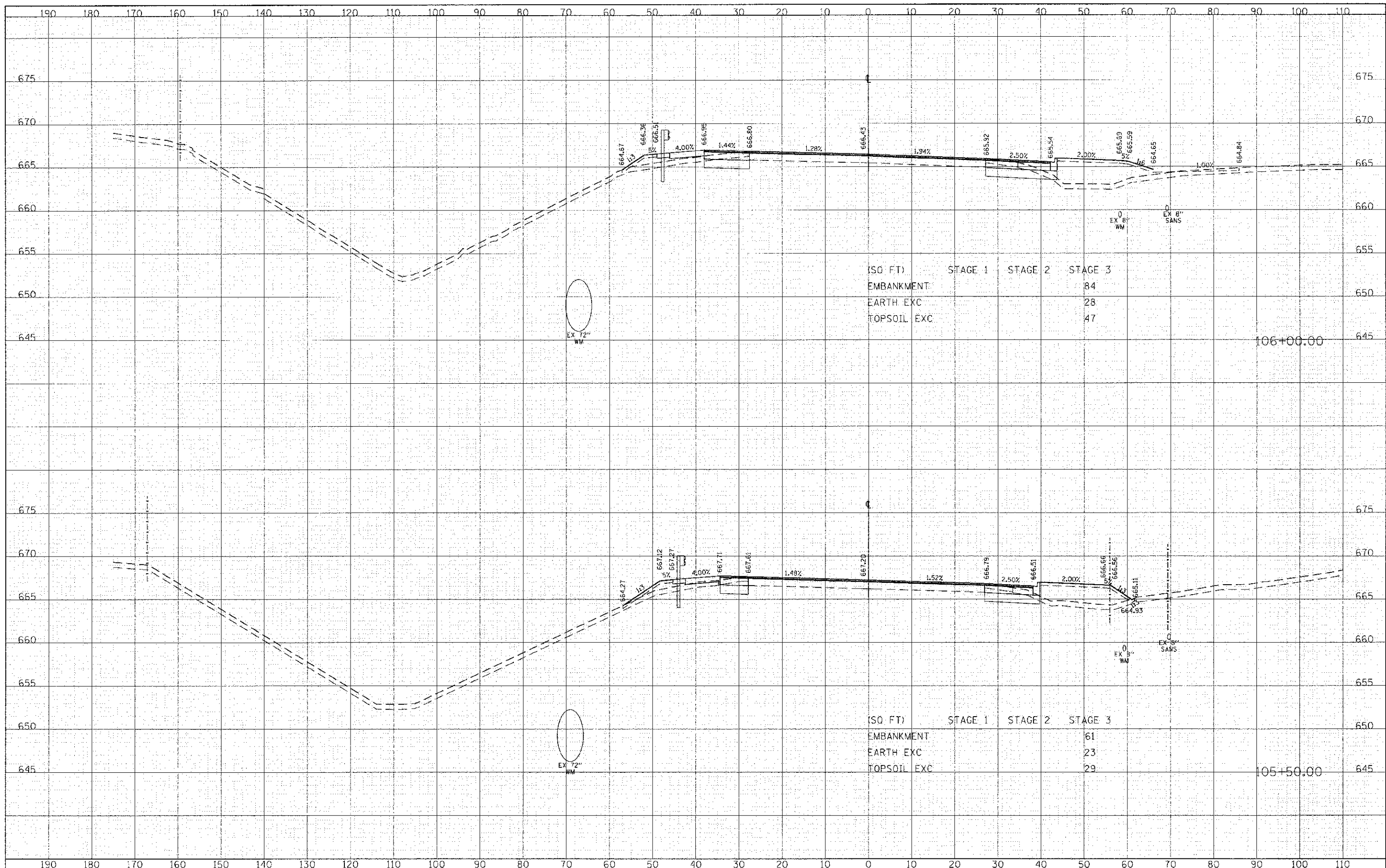


FILE NAME =	USER NAME = tksondell	DESIGNED - YKS	REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>BUTTERFIELD ROAD AT COMMONWEALTH LANE</b> <b>BUTTERFIELD ROAD CROSS SECTIONS</b>	F.A.U. R.I.E. 3545	SECTION 09-00170-00-CH	COUNTY DUPAGE	TOTAL SHEETS 108	SHEET NO. 91		
g:\chrs\0008\road\sheet\01\0008-S-1-XS-ILb.dwg	PLLOT SCALE = 1/2" = 10' / FT.	DRAWN - TKS	REVISED -			SCALE: H 1" = 10' V 1" = 5'	SHEET 91 OF 108 SHEETS	STA. 104+66 TO STA. 105+00	CONTRACT NO. 63742			
PLLOT DATE = 12/21/2012	DATE 10/22/2012	CHECKED - JMG	REVISED -			[ILLINOIS] FED. AID PROJECT						
		DATE 10/22/2012	REVISED -									



DATE	
BY	
SURVEYED	
DESIGNED	
DRAWN	
CHECKED	
PLotted	
DATE	
SCALE	
PROJECT	
CONTRACT	
SHEET	
OF	
TOTAL	
SHEETS	
NO.	

DATE	
BY	
SURVEYED	
DESIGNED	
DRAWN	
CHECKED	
PLotted	
DATE	
SCALE	
PROJECT	
CONTRACT	
SHEET	
OF	
TOTAL	
SHEETS	
NO.	



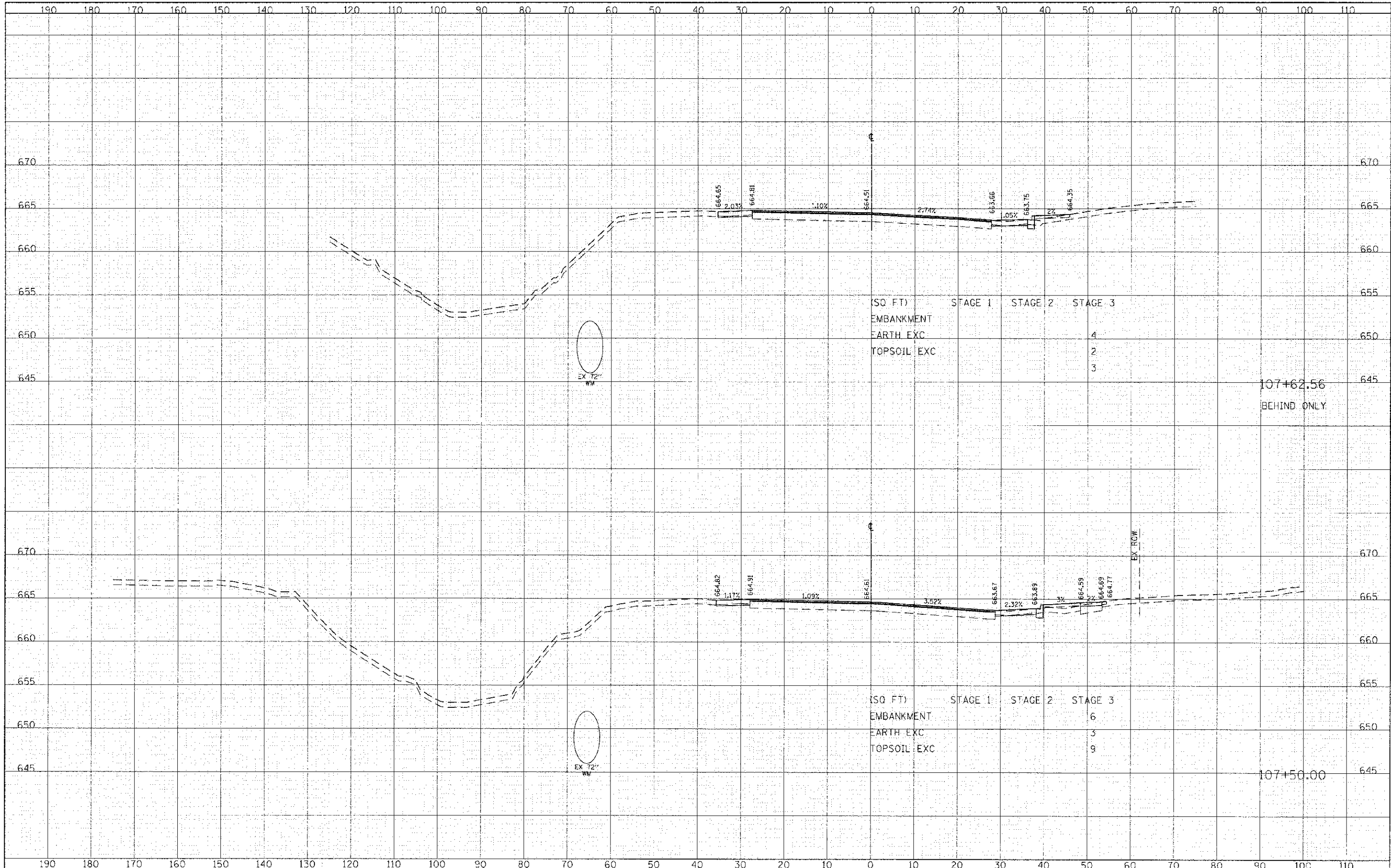
(SQ FT)	STAGE 1	STAGE 2	STAGE 3
EMBANKMENT			84
EARTH EXC			28
TOPSOIL EXC			47

(SQ FT)	STAGE 1	STAGE 2	STAGE 3
EMBANKMENT			61
EARTH EXC			23
TOPSOIL EXC			29



DATE	
BY	
NO.	
APPROVED	
DESIGNED	
DRAWN	
CHECKED	
DATE	

DATE	
BY	
NO.	
APPROVED	
DESIGNED	
DRAWN	
CHECKED	
DATE	

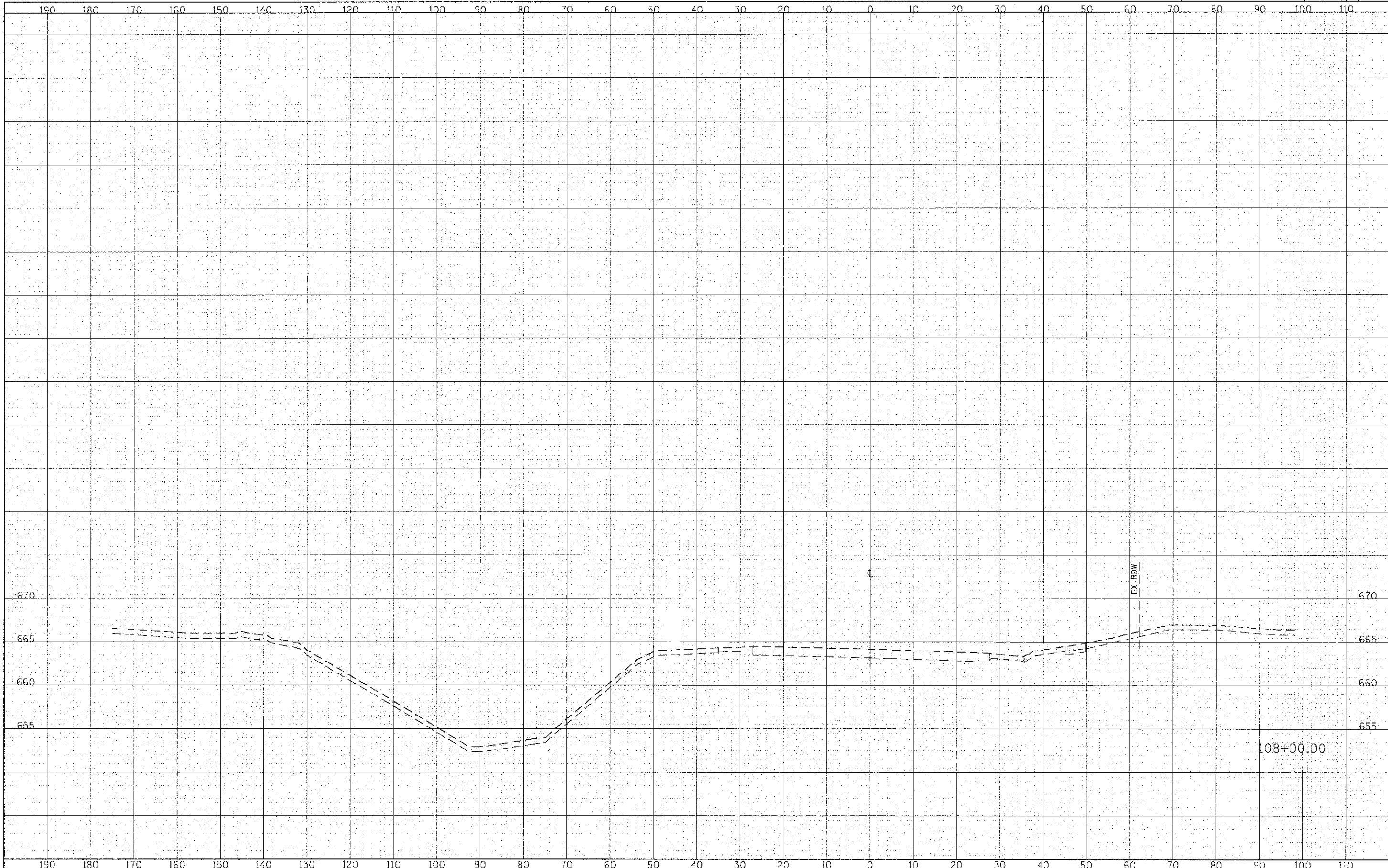


(SQ FT)	STAGE 1	STAGE 2	STAGE 3
EMBANKMENT			
EARTH EXC			4
TOPSOIL EXC			2
			3

(SQ FT)	STAGE 1	STAGE 2	STAGE 3
EMBANKMENT			6
EARTH EXC			5
TOPSOIL EXC			9

FINAL SURVEY	DATE
SCHEMATIC	
PLOTTED	
NO. 1 BOOK	
AREAS	
AREAS CHECKED	
NO.	

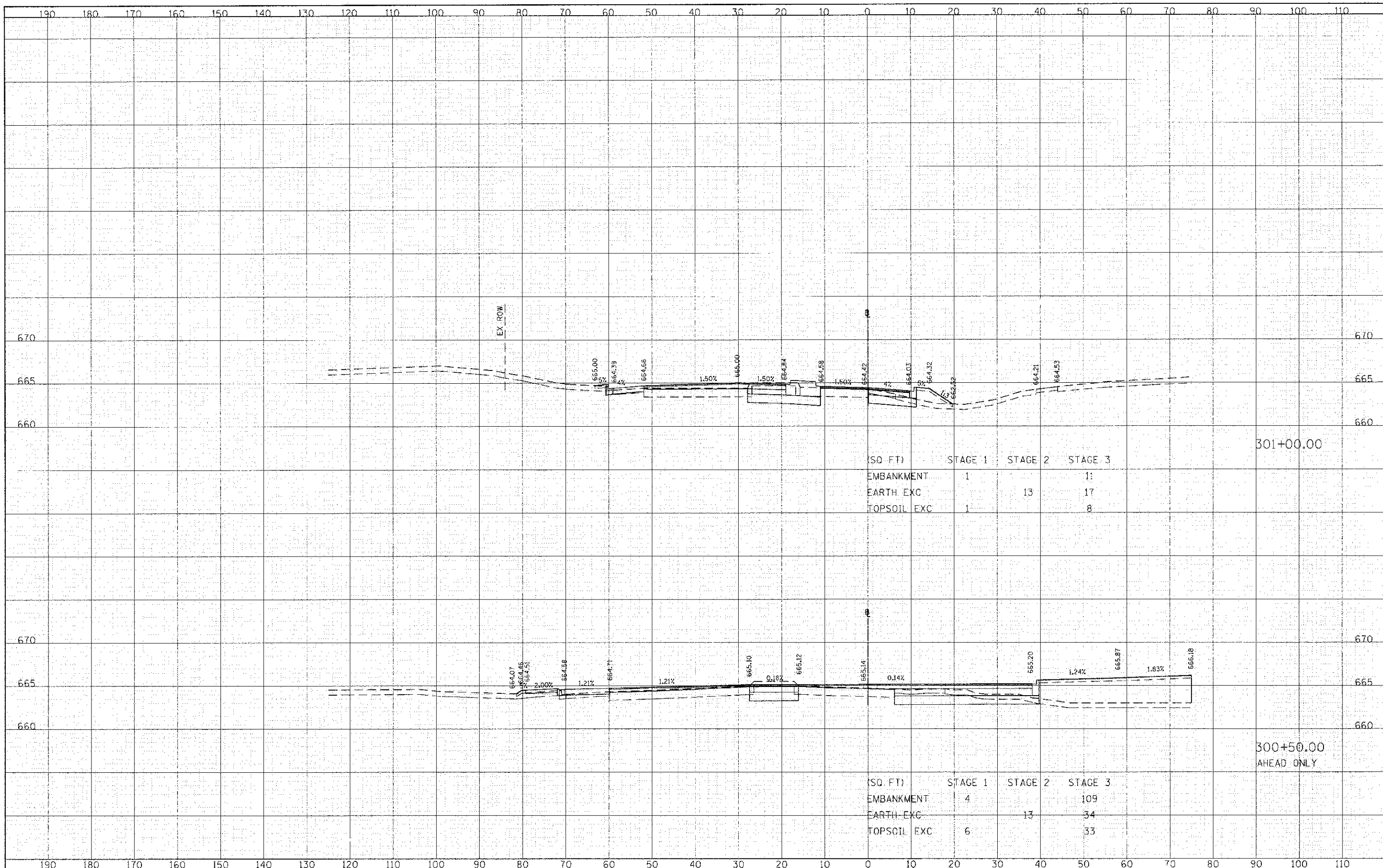
ORIGINAL SURVEY	DATE
SCHEMATIC	
PLOTTED	
NO. 1 BOOK	
AREAS	
AREAS CHECKED	
NO.	



FILE NAME =	USER NAME = aksane.l	DESIGNED - TKS	REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>BUTTERFIELD ROAD AT COMMONWEALTH LANE</b> <b>BUTTERFIELD ROAD CROSS SECTIONS</b>	F.A.U.:	SECTION	COUNTY	TOTAL SHEET	
at\CH20\B025\Road\Sheets\108008-SHT-XS-1L56.dwg		DRAWN - TKS	REVISED -			3545	09-00170-00-CH	DUPAGE	108	95
PLOT SCALE = 1/2" = 10' / FT.		CHECKED - JMG	REVISED -			CONTRACT NO. 63742				
PLOT DATE = 10/21/2012		DATE 10/22/2012	REVISED -			ILLINOIS FED. AID PROJECT				
				SCALE: H <sub>v</sub> 1" = 10'		SHEET 95 OF 108 SHEETS		STA. 108+00 TO STA. 108+00		

FINAL SURVEYED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 SURVEYED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 NOTE BOOK NO.: \_\_\_\_\_  
 TEMPLE'S AREAS CHECKED: \_\_\_\_\_  
 AREAS CHECKED: \_\_\_\_\_

ORIGINAL SURVEYED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 SURVEYED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 NOTE BOOK NO.: \_\_\_\_\_  
 TEMPLE'S AREAS CHECKED: \_\_\_\_\_  
 AREAS CHECKED: \_\_\_\_\_



(SQ. FT)	STAGE 1	STAGE 2	STAGE 3
EMBANKMENT	1		11
EARTH EXC		13	17
TOPSOIL EXC	1		8

(SQ. FT)	STAGE 1	STAGE 2	STAGE 3
EMBANKMENT	4		109
EARTH EXC		13	34
TOPSOIL EXC	6		33

FILE NAME = g:\c\08\0208\road\shasta\212\0208-SHT-VS-Cons.dgn  
 USER NAME = tkandell  
 DESIGNED - TKX  
 DRAWN - TKX  
 CHECKED - JMG  
 PLOT DATE = 10/21/2012  
 DATE 10/22/2012

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

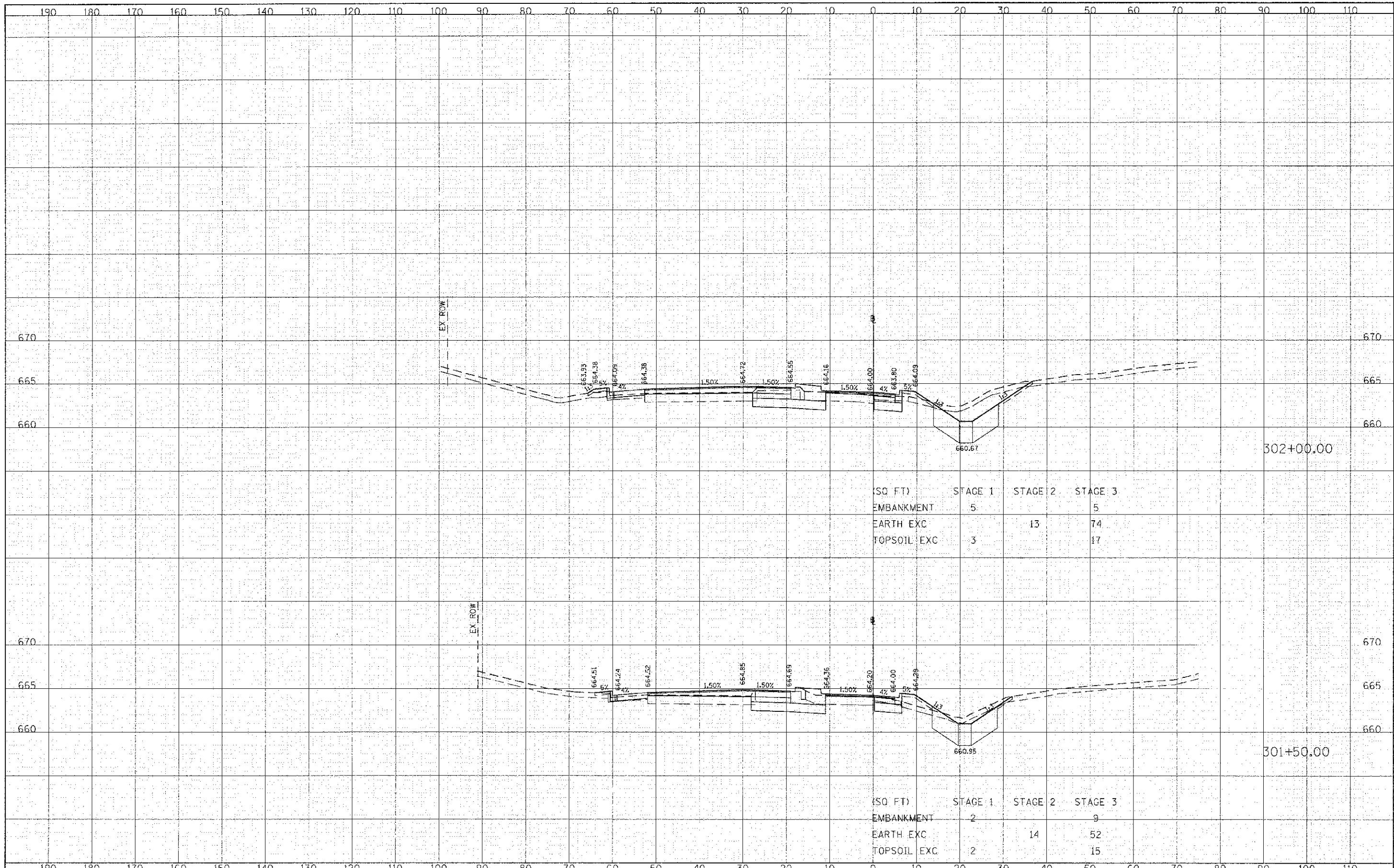
**BUTTERFIELD ROAD AT COMMONWEALTH LANE**  
**COMMONWEALTH LANE CROSS SECTIONS**

F.A.U. RT. 3545 SECTION 09-00170-00-CH COUNTY DUPAGE CONTRACT NO. 63742  
 TOTAL SHEETS 108 SHEETS NO. 96  
 SCALE: H<sub>v</sub> 1"=10' V<sub>v</sub> 1"=5' SHEET 96 OF 108 SHEETS STA. 300+50 TO STA. 301+00  
 ILLINOIS FED. AID PROJECT



DATE	
BT	
FINAL SURVEY	
SURVEY	
40' E BOOK	
AREAS	
AREAS CHECKED	

DATE	
BT	
ORIGINAL SURVEY	
SURVEY	
40' E BOOK	
AREAS	
AREAS CHECKED	



(SQ FT)	STAGE 1	STAGE 2	STAGE 3
EMBANKMENT	5		5
EARTH EXC		13	74
TOPSOIL EXC	3		17

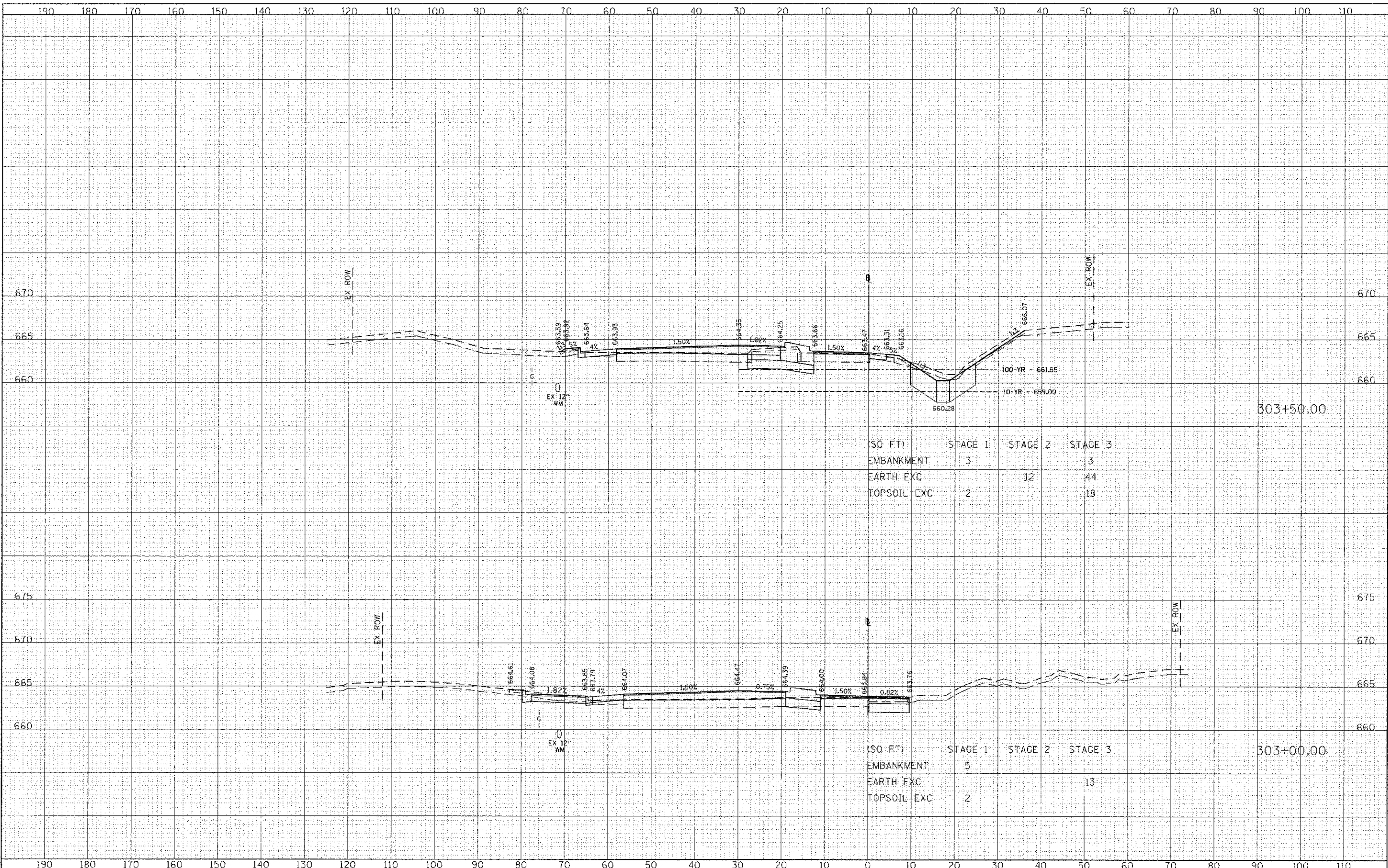
(SQ FT)	STAGE 1	STAGE 2	STAGE 3
EMBANKMENT	2		9
EARTH EXC		14	52
TOPSOIL EXC	2		15





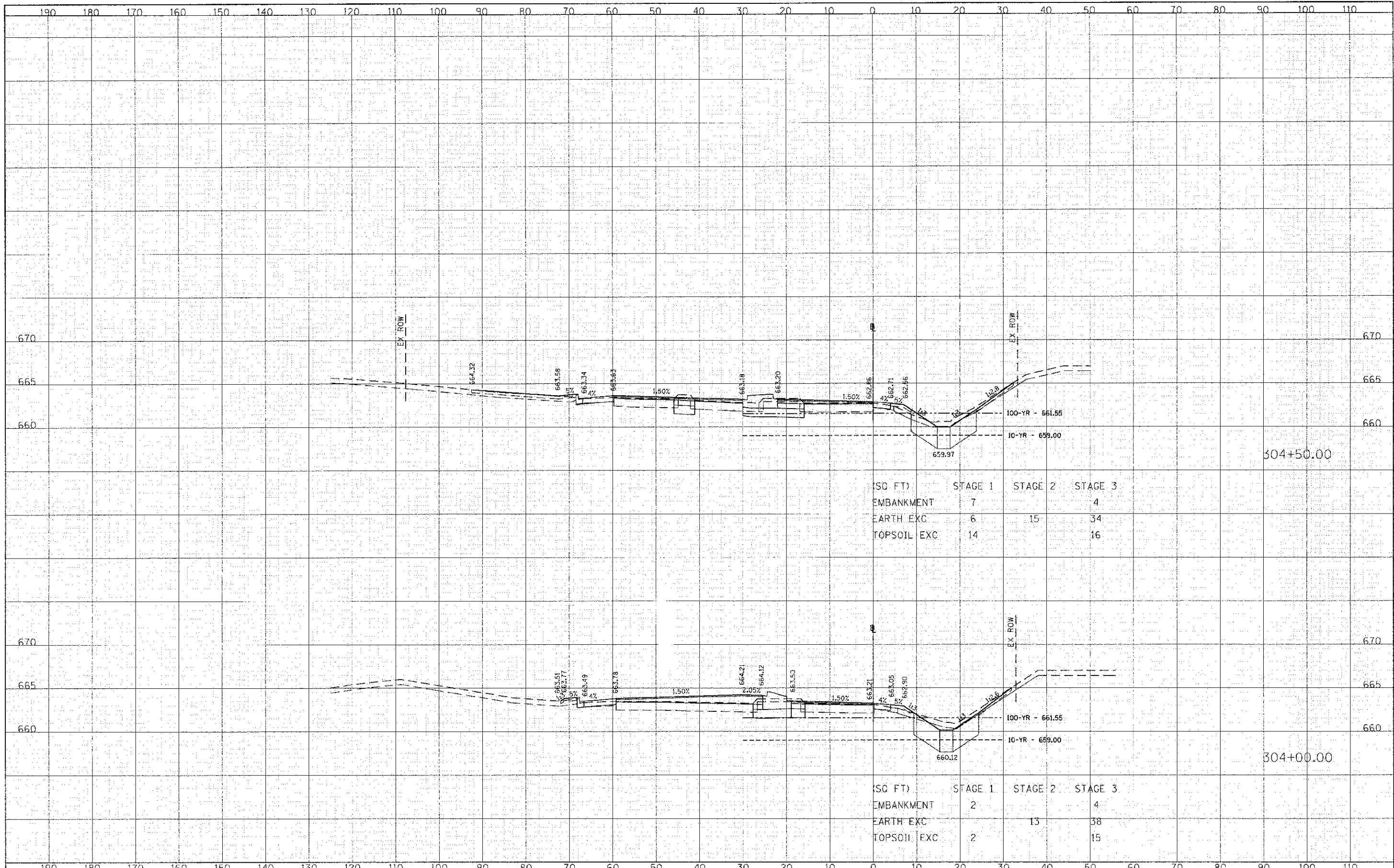
FINAL SURVEY DATE  
 SURVEYED BY  
 NOTE BOOK NO.  
 TEMPL. NO.  
 AREAS CHECKED

ORIGIN BY DATE  
 SURVEYED BY  
 NOTE BOOK NO.  
 TEMPL. NO.  
 AREAS CHECKED



DATE: \_\_\_\_\_  
 BY: \_\_\_\_\_  
 SURVEYED: \_\_\_\_\_  
 SUPPLY: \_\_\_\_\_  
 NOTE BOOK: \_\_\_\_\_  
 TEMPLATE: \_\_\_\_\_  
 AREAS: \_\_\_\_\_  
 AREAS CHECKED: \_\_\_\_\_

DATE: \_\_\_\_\_  
 BY: \_\_\_\_\_  
 SURVEYED: \_\_\_\_\_  
 SUPPLY: \_\_\_\_\_  
 NOTE BOOK: \_\_\_\_\_  
 TEMPLATE: \_\_\_\_\_  
 AREAS: \_\_\_\_\_  
 AREAS CHECKED: \_\_\_\_\_

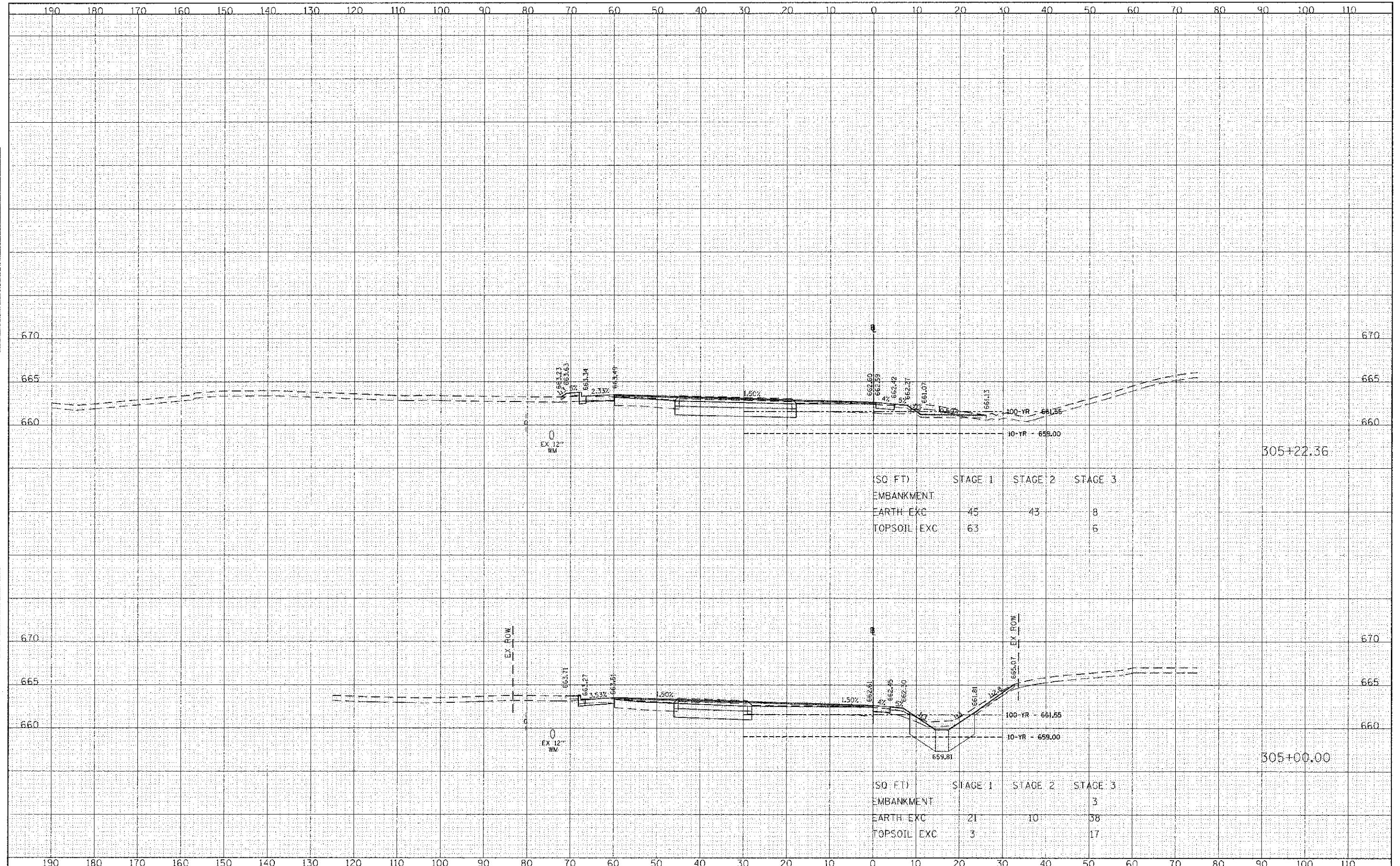


(SQ FT)	STAGE 1	STAGE 2	STAGE 3
EMBANKMENT	7		4
EARTH EXC	6	15	34
TOPSOIL EXC	14		16

(SQ FT)	STAGE 1	STAGE 2	STAGE 3
EMBANKMENT	2		4
EARTH EXC		13	38
TOPSOIL EXC	2		15

DATE	
BY	
REVIEWED	
DATE	
NOTE BOOK	
NO.	
AREAS CHECKED	

DATE	
BY	
REVIEWED	
DATE	
NOTE BOOK	
NO.	
AREAS CHECKED	



SO FT)	STAGE 1	STAGE 2	STAGE 3
EMBANKMENT			
EARTH EXC	45	43	8
TOPSOIL EXC	63		6

SO FT)	STAGE 1	STAGE 2	STAGE 3
EMBANKMENT			
EARTH EXC	21	10	38
TOPSOIL EXC	3		17

FILE NAME =	USER NAME = kalaroz	DESIGNED - TKS	REVISED -
G:\CH2S\0088\Road\PHASE 2\Sheets\01X0008-SHT	KS-ComS.dgn	DRAWN - TKS	REVISED -
PLOT SCALE = 10.0000' / 1"		CHECKED - JMG	REVISED -
PLOT DATE = 12/4/2012		DATE 12/22/2012	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BUTTERFIELD ROAD AT COMMONWEALTH LANE  
COMMONWEALTH LANE CROSS SECTIONS**

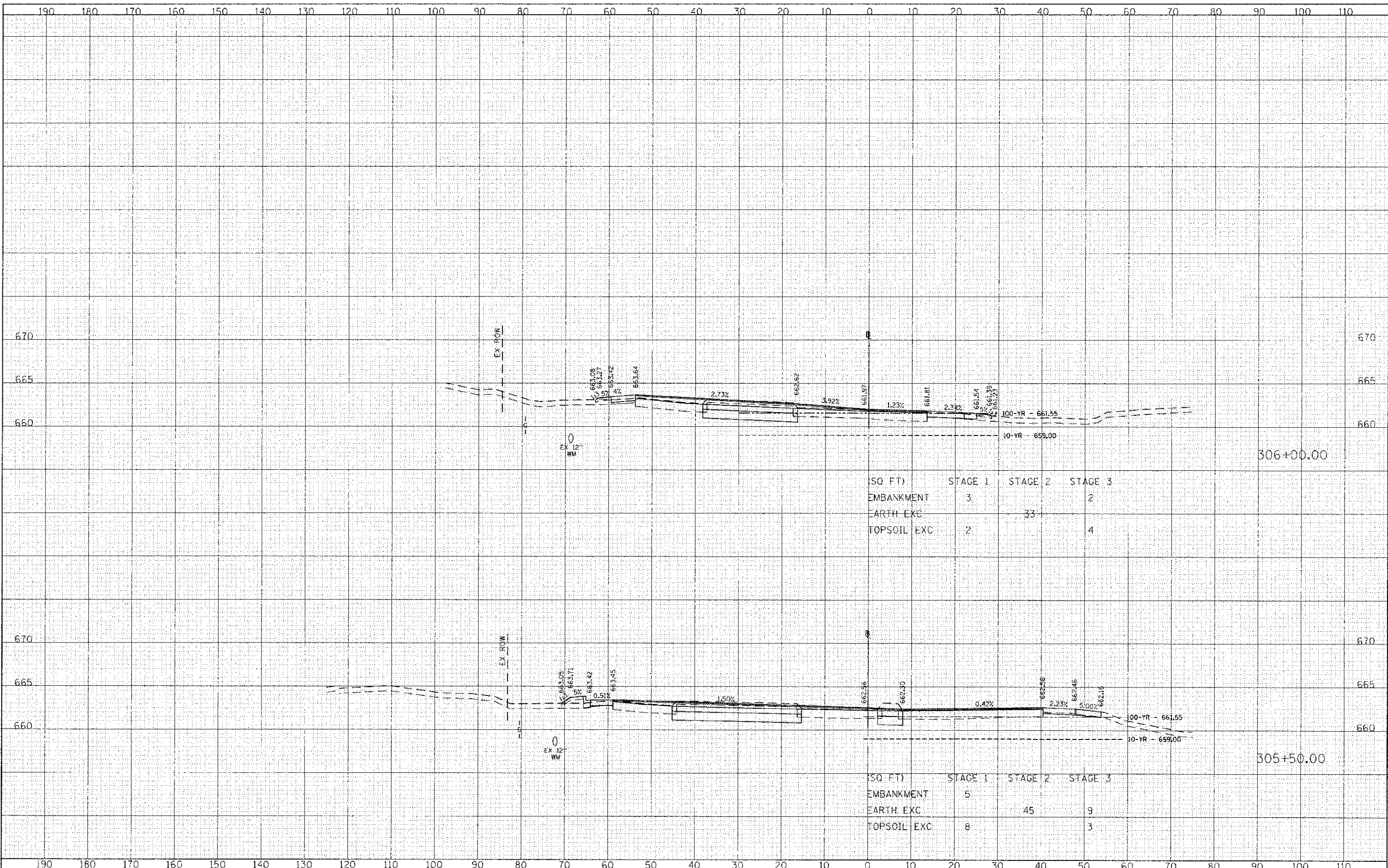
SCALE: 1" = 10' HORIZ. / 1" = 5' VERT. SHEET 101 OF 108 SHEETS STA. 305+00 TO STA. 305+22

FAU-RT#	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	09-00170-00-CH	DUPAGE	108	101
CONTRACT NO. 63742			ILLINOIS FED. AID PROJECT	



BY	DATE
DESIGNED	
DRAWN	
CHECKED	
DATE	
REVISIONS	
NO.	
DATE	
DESCRIPTION	

BY	DATE
DESIGNED	
DRAWN	
CHECKED	
DATE	
REVISIONS	
NO.	
DATE	
DESCRIPTION	



(SQ FT)	STAGE 1	STAGE 2	STAGE 3
EMBANKMENT	3		2
EARTH EXC		33	
TOPSOIL EXC	2		4

(SQ FT)	STAGE 1	STAGE 2	STAGE 3
EMBANKMENT	5		
EARTH EXC		45	9
TOPSOIL EXC	8		3

FILE NAME = G:\CH\09\0026\Road\PHASE 2\Sheets\DX0000-SHT-15-Com.dgn  
 USER NAME = halorenz  
 PLOT SCALE = 10.0000' / 1" = 120  
 PLOT DATE = 12/4/2012

DESIGNED - TRS  
 DRAWN - TRS  
 CHECKED - JMG  
 DATE 10/22/2012

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

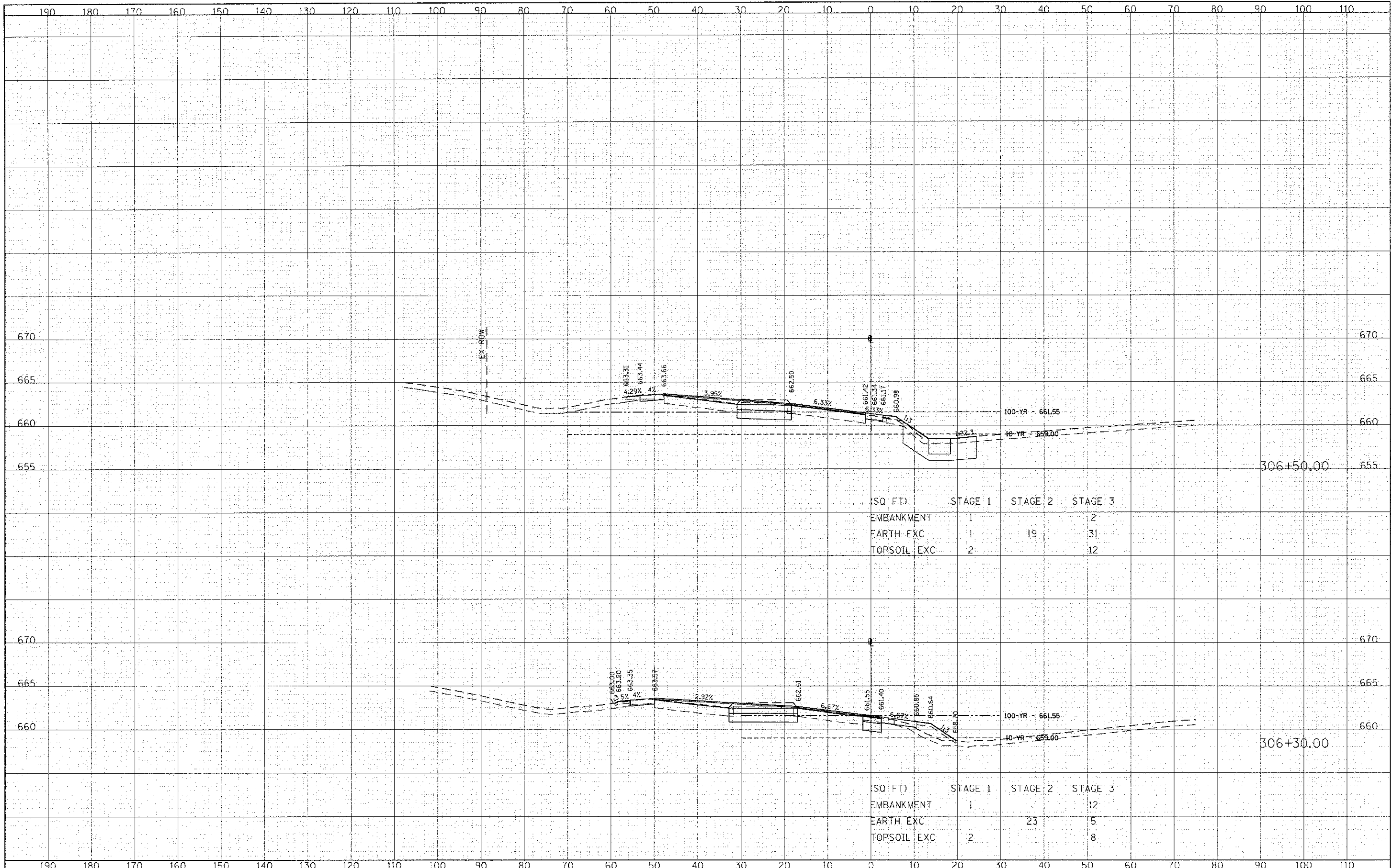
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**BUTTERFIELD ROAD AT COMMONWEALTH LANE  
 COMMONWEALTH LANE CROSS SECTIONS**  
 SCALE: H 1"=10'  
 V 1"=5'  
 SHEET 102 OF 108 SHEETS | STA. 305+50 TO STA. 306+00

FAA J. RTE. 3545	SECTION 09-00170-00-CH	COUNTY DUPAGE	TOTAL SHEETS 108	SHEET NO. 102
CONTRACT NO. 63742			ILLINOIS FED. AID PROJECT	

DATE	
BY	
DESIGNED	
DRAWN	
CHECKED	
DATE	
FILE NAME	
USER NAME	
DESIGNED	
DRAWN	
CHECKED	
DATE	

DATE	
BY	
DESIGNED	
DRAWN	
CHECKED	
DATE	
FILE NAME	
USER NAME	
DESIGNED	
DRAWN	
CHECKED	
DATE	



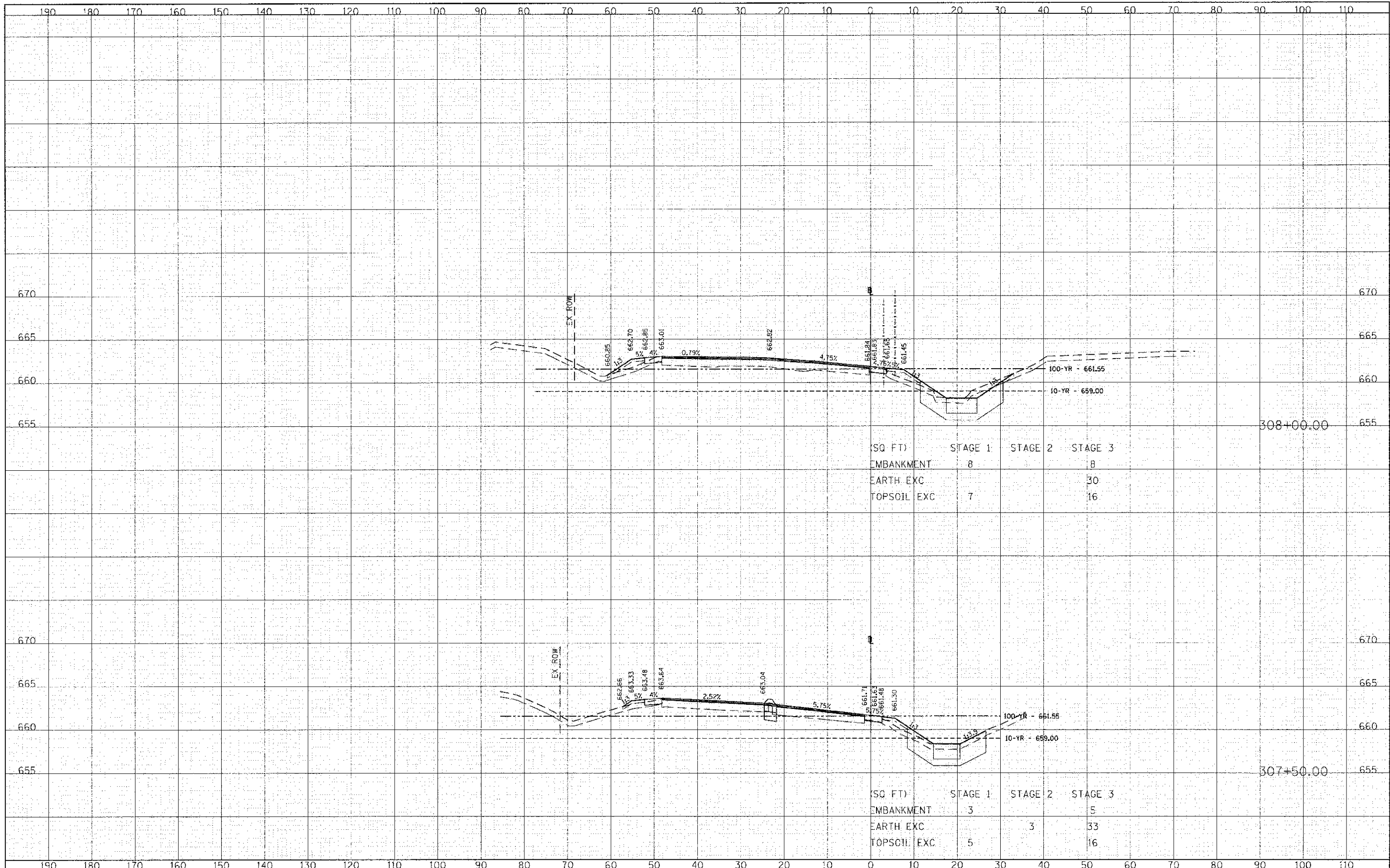
(SQ FT)	STAGE 1	STAGE 2	STAGE 3
EMBANKMENT	1		2
EARTH EXC	1	19	31
TOPSOIL EXC	2		12

(SQ FT)	STAGE 1	STAGE 2	STAGE 3
EMBANKMENT	1		12
EARTH EXC		23	5
TOPSOIL EXC	2		8



FINAL	DATE
SURVEY	BY
NOTE BOOK	NO.
AREAS CHECKED	

ORIGINAL	DATE
SURVEY	BY
NOTE BOOK	NO.
AREAS CHECKED	



FILE NAME =	USER NAME = tkrendell	DESIGNED - TKS	REVISED -
g:\ch28\2008\road\sheet\01\2008-SHT-XS-Cons.dgn		DRAWN - TKS	REVISED -
PLOT SCALE = 1/20,000' / FT.		CHECKED - JMG	REVISED -
PLOT DATE = 10/22/2012		DATE 10/22/2012	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BUTTERFIELD ROAD AT COMMONWEALTH LANE  
COMMONWEALTH LANE CROSS SECTIONS**

SCALE: HORIZ. 1"=100' VERT. 1"=5'

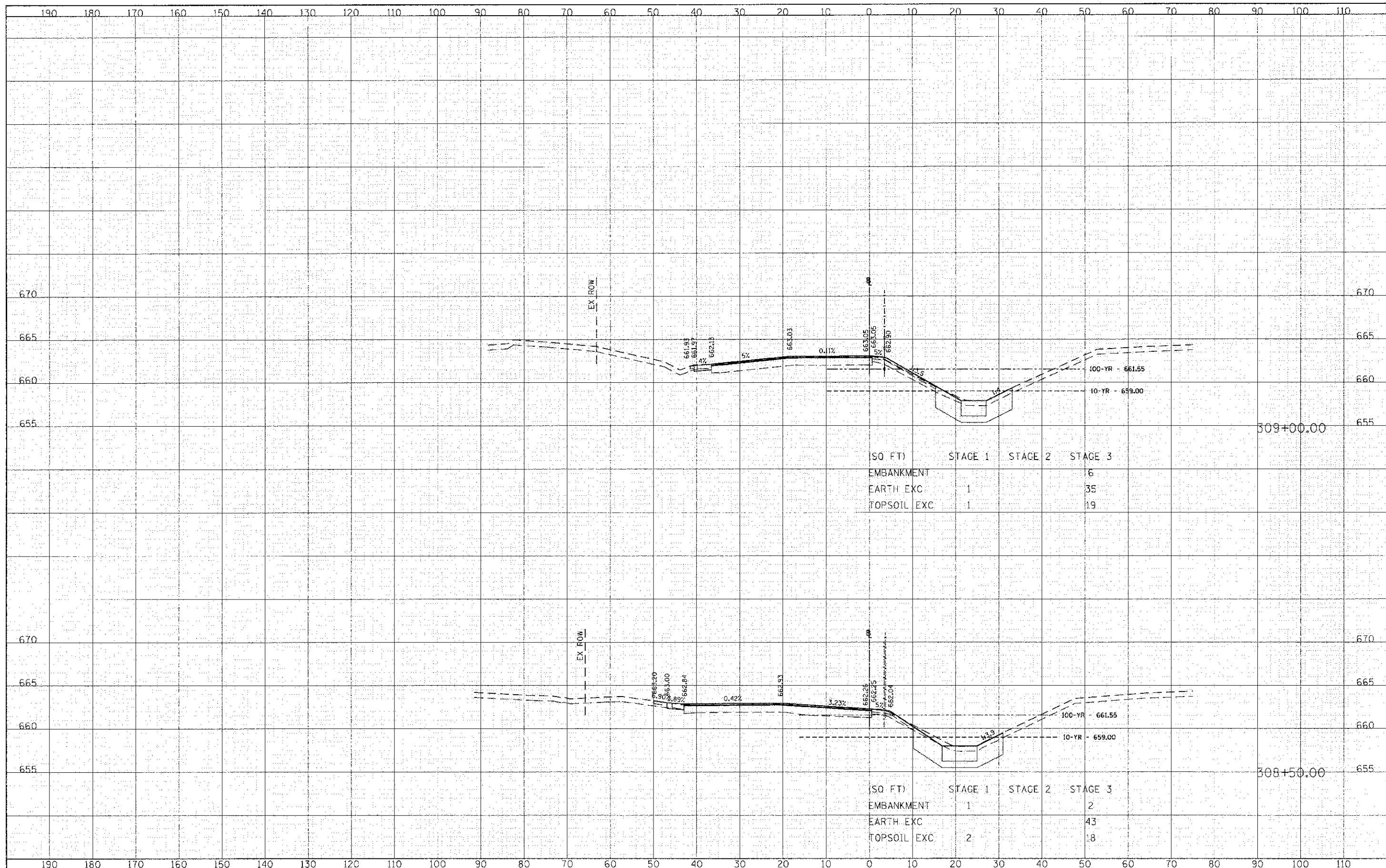
SHEET 105 OF 108 SHEETS; STA. 307+50 TO STA. 308+00

F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS
3545	09-00170-00-CH	DUPAGE	108
		DUPAGE	105
CONTRACT NO. 63742			
ILLINOIS FED. AID PROJECT			



FINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
NOTE BOOK		
AREAS		
AREAS CHECKED		
NO.		

ORIGINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
NOTE BOOK		
AREAS		
AREAS CHECKED		
NO.		



(SQ FT)	STAGE 1	STAGE 2	STAGE 3
EMBANKMENT			6
EARTH EXC	1		35
TOPSOIL EXC	1		19

(SQ FT)	STAGE 1	STAGE 2	STAGE 3
EMBANKMENT	1		2
EARTH EXC			43
TOPSOIL EXC	2		18

FILE NAME =	USER NAME = akardell	DESIGNED - TKS	REVISED -
g:\ch08\0001\road\sheet\DIW0008-SHT-KS-Cos5.dgn		DRAWN - TKS	REVISED -
		CHECKED - JMG	REVISED -
		DATE 10/22/2012	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

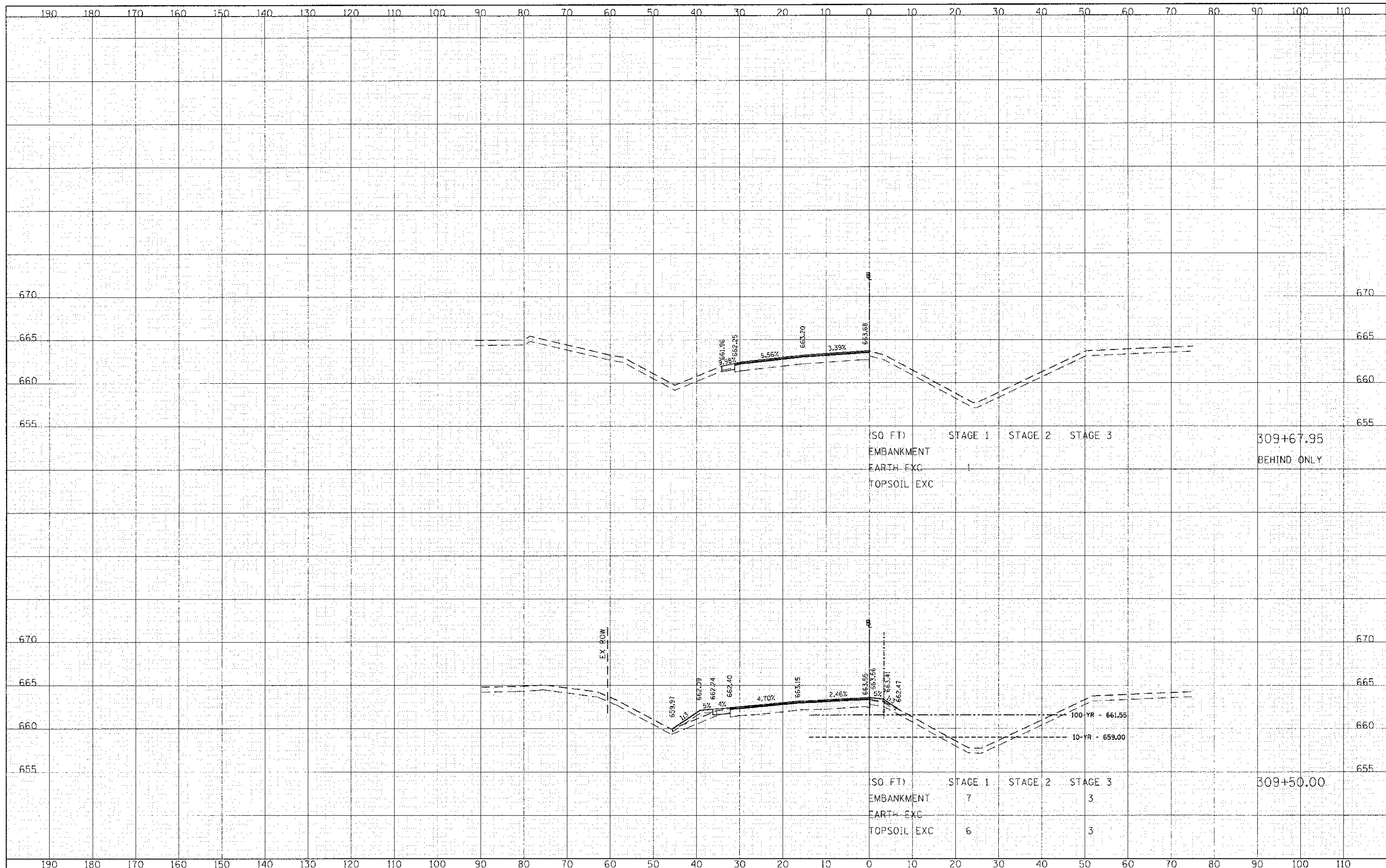
**BUTTERFIELD ROAD AT COMMONWEALTH LANE  
COMMONWEALTH LANE CROSS SECTIONS**

SCALE:  $\frac{H}{V} = \frac{1''}{45'}$  SHEET 106 OF 108 SHEETS STA. 308+50 TO STA. 309+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	09-00170-00-CH		108	106
CONTRACT NO. 63742			ILLINOIS FED. AID PROJECT	

FINAL SURVEY	DATE
SURVEYED	BY
PLOTTED	
NOTE BOOK	
AREAS CHECKED	

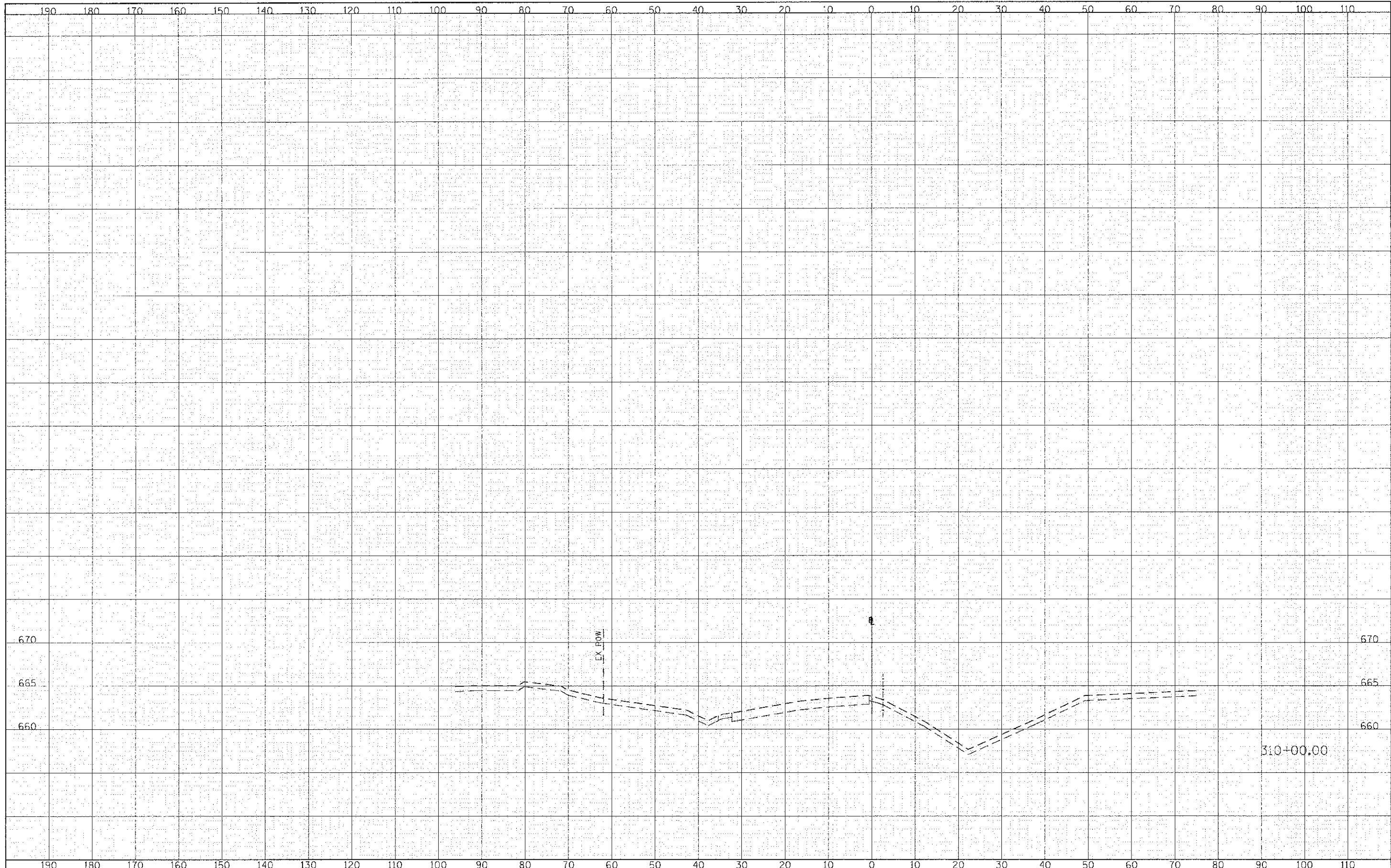
ORIGINAL SURVEY	DATE
SURVEYED	BY
PLOTTED	
NOTE BOOK	
AREAS CHECKED	



FILE NAME = g:\ah08\0088\road\sheet\Dir\008-SHT-XS-Com5.dgn	USER NAME = tkandoll	DESIGNED - TKS	REVISIED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BUTTERFIELD ROAD AT COMMONWEALTH LANE COMMONWEALTH LANE CROSS SECTIONS</b>	F.A.U. 3545	SECTION 09-00170-00-CH	COUNTY DUPAGE	TOTAL SHEET NO. 108	SHEET NO. 107		
PLLOT SCALE = 1/20,000 1" = 20'	CHECKED - JMG	REVISIED -	REVISIED -			SCALE: H 1"=10' V 1"=5'	SHEET 107 OF 108 SHEETS	STA. 309+50 TO STA. 309+68	CONTRACT NO. 63742			
PLLOT DATE = 10/22/2012	DATE 10/22/2012	REVISIED -	REVISIED -			ILLINOIS FED. AID PROJECT						

FINAL SURVEY	DATE
BY	
NO. AREAS CHECKED	
NOTE BOOK	
PLANT	
AREA	
NO.	

ORIGINAL SURVEY	DATE
BY	
NO. AREAS CHECKED	
NOTE BOOK	
PLANT	
AREA	
NO.	



FILE NAME =	USER NAME = %s-dell	DESIGNED - TKS	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BUTTERFIELD ROAD AT COMMONWEALTH LANE COMMONWEALTH LANE CROSS SECTIONS</b>		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
g:\a26\0203\road\sheet\11X\0003-SH1-XS-CorS.dgn		DRAWN - TKS	REVISED -		3545	09-00170-00-CH	DUPAGE	108	108		
PLOT SCALE = 120.0000 1 / FT.		CHECKED - JMG	REVISED -		SCALE: $\frac{1}{4}'' = 10'$		SHEET 108 OF 108 SHEETS		STA. 310+00 TO STA. 310+00	CONTRACT NO. 63742	
PLOT DATE = 10/22/2012		DATE 10/22/2012	REVISED -						ILLINOIS FED. AID PROJECT		