

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
**PROPOSED  
HIGHWAY PLANS**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2018-098-RS&SW	COOK	35	1
ILLINOIS			CONTRACT NO. 62H47	

D-91-052-19

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THIS PROJECT IS LOCATED IN THE VILLAGES OF PALATINE AND ARLINGTON HEIGHTS

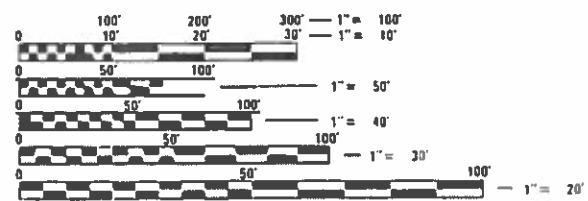
F.A.P. ROUTE 305: PALATINE ROAD  
US 14 (NORTHWEST HIGHWAY) TO US 12 (RAND ROAD)  
SECTION: 2018-098-RS&SW  
PROJECT: NHPP-IDYP(407)  
RESURFACING (3P)/PEDESTRIAN RAMPS  
COOK COUNTY  
C-91-251-19

TRAFFIC DATA  
2014 ADT = 45,000  
SPEED LIMIT = 35-45 MPH



PROJECT BEGINS  
STA 12+90

PROJECT ENDS  
STA 167+60



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER: ALAIN MIDY (847) 221-3056  
PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247

PALATINE AND WHEELING TOWNSHIPS  
GROSS & NET LENGTH OF IMPROVEMENT = 15,470 FT. = 2.93 MILES

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED *December 18, 2018*

*Anthony Rungler* REGIONAL ENGINEER  
Dec 7 2018

*[Signature]* ENGINEER OF DESIGN AND ENVIRONMENT  
Dec 7 2018

*[Signature]* DIRECTOR OF TRANSPORTATION PROJECT IMPLEMENTATION

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OF THE STATE OF ILLINOIS

CONTRACT NO. 62H47



SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		ROADWAY 0005 80% FED 20% STATE				
20100110	TREE REMOVAL ( 6 TO 15 UNITS DIAMETER)	UNIT	26	26				
20100210	TREE REMOVAL ( OVER 15 UNITS DIAMETER)	UNIT	165	165				
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	2090	2090				
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	26	26				
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	26	26				
25200110	SODDING, SALT TOLERANT	SO YD	2090	2090				
25200200	SUPPLEMENTAL WATERING	UNIT	21	21				
35501311	HOT-MIX ASPHALT BASE COURSE, 6 3/4"	SO YD	150	150				
40600290	BITUMINOUS MATERIALS ( TACK COAT)	POUND	69850	69850				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	156	156				
40600827	POLYMERIZED LEVELING BINDER ( MACHINE METHOD), IL-4, 75, N50	TON	4280	4280				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	945	945				
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	662	662				

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		ROADWAY 0005 80% FED 20% STATE				
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	17	17				
42001300	PROTECTIVE COAT	SO YD	4000	4000				
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SO YD	72	72				
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	1150	1150				
* 42400800	DETECTABLE WARNINGS	SO FT	208	208				
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SO YD	103500	103500				
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	222	222				
44000600	SIDEWALK REMOVAL	SO FT	1150	1150				
44002216	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 4"	SO YD	2955	2955				
44003100	MEDIAN REMOVAL	SO FT	2910	2910				
44003510	MEDIAN REMOVAL PARTIAL DEPTH	SO FT	6300	6300				
44201815	CLASS D PATCHES, TYPE II, 14 INCH	SO YD	1795	1795				
44201819	CLASS D PATCHES, TYPE III, 14 INCH	SO YD	330	330				

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2018-098-RS&SW	COOK	35	3
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 62H47	

\* = SPECIALTY ITEM

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		ROADWAY 0005 80% FED 20% STATE					
44201821	CLASS D PATCHES, TYPE IV, 14 INCH	SO YD	320	320					
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	25	25					
60255500	MANHOLES TO BE ADJUSTED	EACH	2	2					
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	1	1					
60262700	INLETS TO BE RECONSTRUCTED	EACH	8	8					
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	95	95					
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	48	48					
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	58	58					
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SO FT	6300	6300					
60619200	CONCRETE MEDIAN, TYPE SB-6.06	SO FT	2850	2850					
60624600	CORRUGATED MEDIAN	SO FT	60	60					
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	42	42					
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1					
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	LSUM	1	1					
* 66901002	ON-SITE MONITORING OF REGULATED SUBSTANCES	CAL DA	1	1					

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		ROADWAY 0005 80% FED 20% STATE					
66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	LSUM	1	1					
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	12	12					
67100100	MOBILIZATION	L SUM	1	1					
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	1	1					
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1					
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1					
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1					
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1					
70300100	SHORT TERM PAVEMENT MARKING	FOOT	32800	32800					
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	10935	10935					
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	554.9	554.9					
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	35000	35000					

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>SUMMARY OF QUANTITIES</b>			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

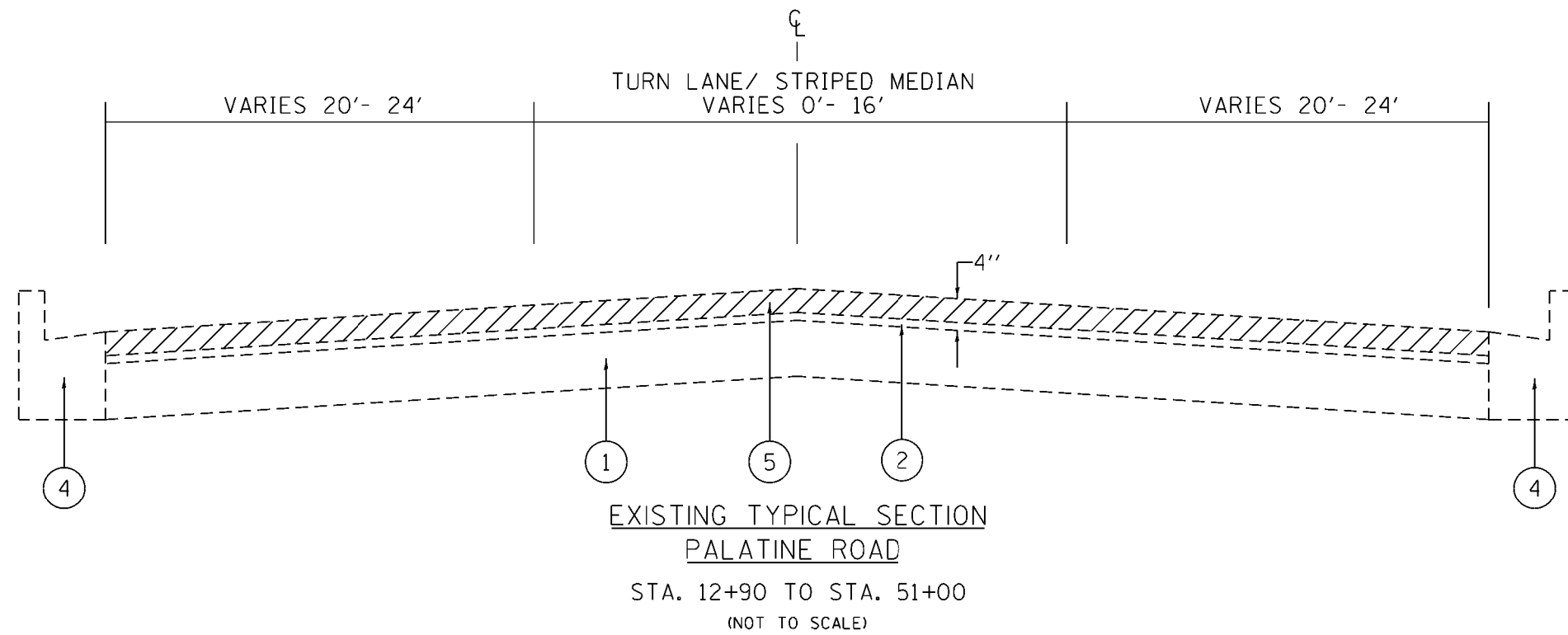
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305	2018-098-RS&SW	COOK	35	4
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62H47	

\*= SPECIALTY ITEM

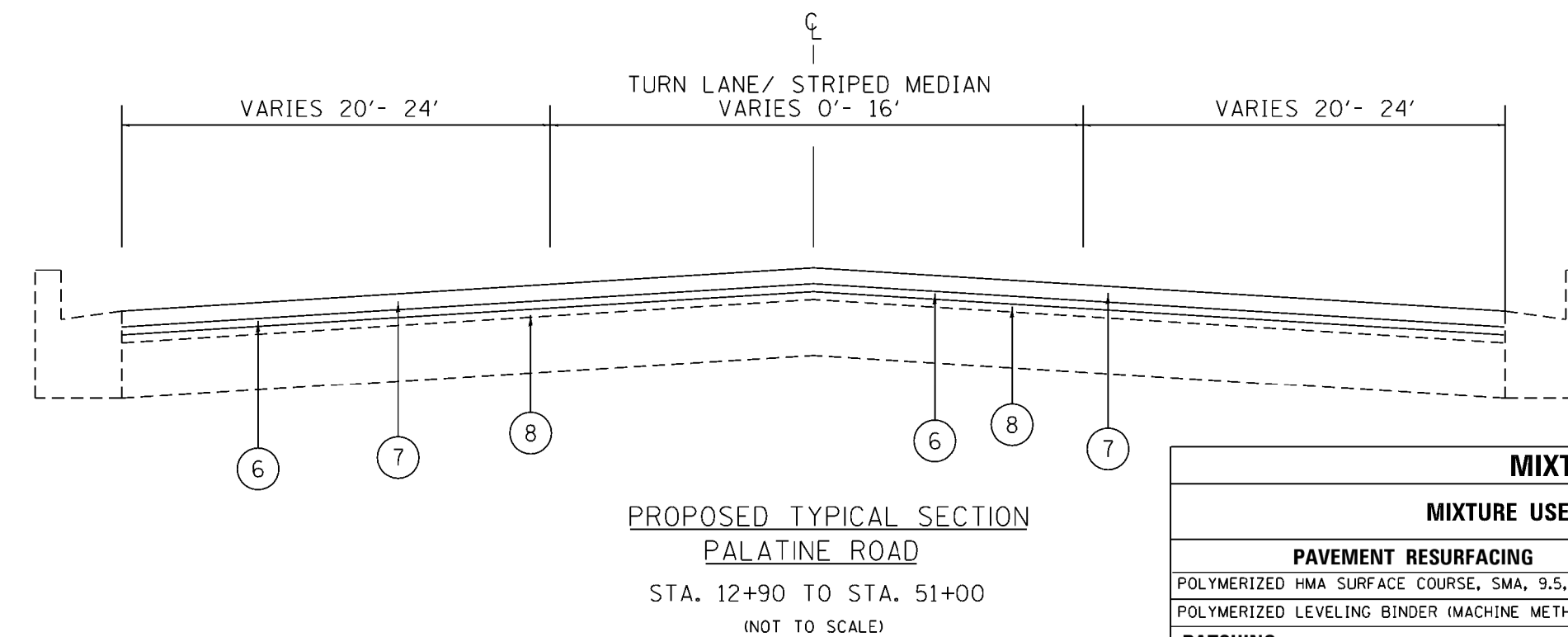
SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		ROADWAY 0005 80% FED 20% STATE					
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	2500	2500					
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	3500	3500					
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	1600	1600					
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	800	800					
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	8200	8200					
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	554.9	554.9					
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	35000	35000					
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	2500	2500					
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	3500	3500					
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1600	1600					
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	800	800					
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	700	700					

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		ROADWAY 0005 80% FED 20% STATE					
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	500	500					
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	2081	2081					
K0029624	WEED CONTROL, TEASEL	GALLON	0.5	0.5					
X4060004	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, N80	TON	10150	10150					
X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	500	500					
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	119	119					
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SO FT	21190	21190					
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	12190	12190					
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1					
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	179	179					
Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	154.2	154.2					
Z0076600	TRAINEES	HOURS	500	500					
Z0033700	LONGITUDINAL JOINT SEALANT	FOOT	35020	35020					
Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	HOURS	500	500					
Z0064800	SELECTIVE CLEARING	UNIT	4	4					
	△ NON PARTICIPATING								
	* SPECIALTY ITEM * =0042								

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		DATE -	REVISED -					CONTRACT NO. 62H47				
								FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT				



- LEGEND**
- ① EXISTING P.C.C. PAVEMENT, 10"
  - ② EXISTING HMA PAVEMENT, ±4"
  - ③ EXISTING CONCRETE MEDIAN
  - ④ EXISTING CURB AND GUTTER
  - ⑤ PROPOSED HMA SURFACE REMOVAL, 2 1/2"
  - ⑥ PROPOSED POLYMERIZED LEVELING BINDER (MM), IL 4.75, 3/4"
  - ⑦ PROPOSED POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, N80, 1 3/4"
  - ⑧ HMA OVERLAY AFTER MILLING, ±1 1/2"



MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE USES	VOIDS @ Ndes	
<b>PAVEMENT RESURFACING</b>		
POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, N80	3.5% AT 80 GYR.	<b>PFP</b>
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	3.5% AT 50 GYR.	<b>QCP</b>
<b>PATCHING</b>		
CLASS D PATCHES (HMA BINDER, IL-19.0 mm)	4% AT 70 GYR.	<b>QC/QA</b>
HMA REPLACEMENT OVER PATCHES (HMA BINDER, IL-19.0 mm)	4% AT 70 GYR.	<b>QC/QA</b>
<b>HMA DRIVEWAY</b>		
HMA BASE COURSE, 6" (HMA BINDER IL-19 mm)	4% AT 50 GYR.	<b>QC/QA</b>
HMA SURFACE COURSE, MIX "D", N50 (IL 9.5 mm)	4% AT 50 GYR.	<b>QC/QA</b>
<b>HMA MEDIAN SURFACE, 4"</b>		
HMA SURFACE COURSE, MIX "D", N50 (IL 9.5 mm)	4% AT 50 GYR.	<b>QC/QA</b>

QMP Designation: Quality Control/Quality Assurance (QC/QA); Quality Control for Performance (QCP); PAY FOR PERFORMANCE (PFP)

NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SOYD/IN

NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS. QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE. THE CONTRACTOR SHALL PATCH FIRST BEFORE MILLING. THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE POLYMERIZED LEVELING BINDER (MM) IL-4.75, N50 WHERE THE SURFACE MIX JOINT WILL BE LOCATED.

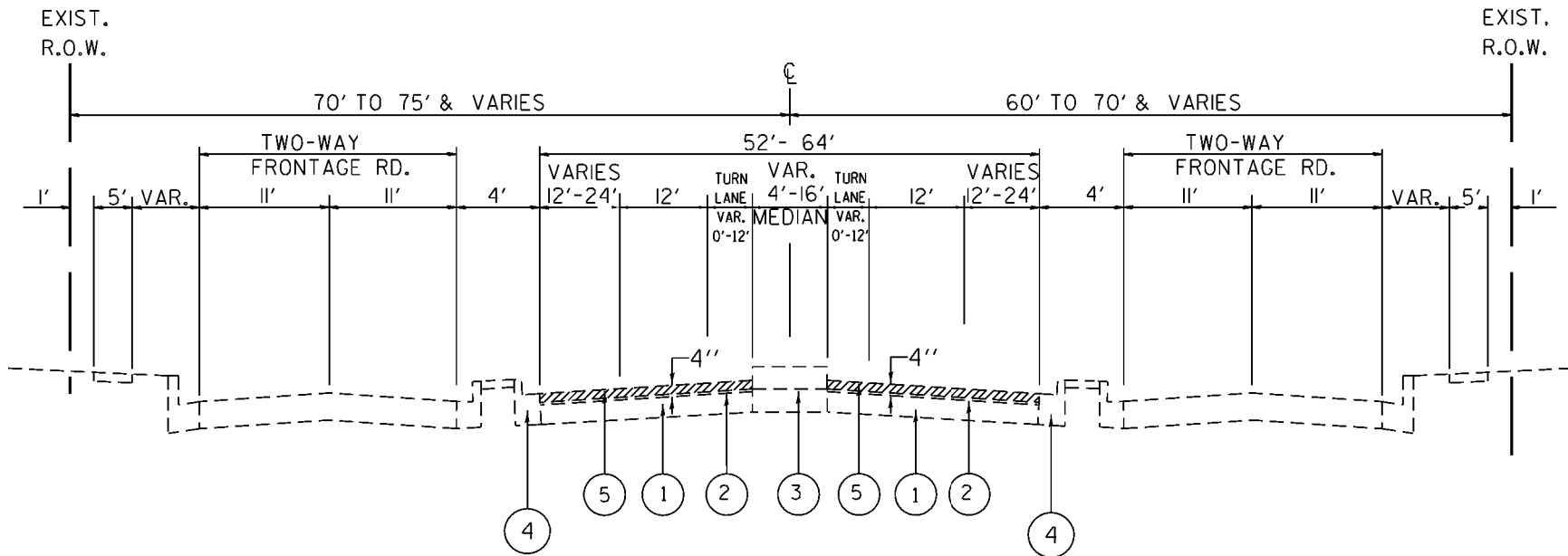
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

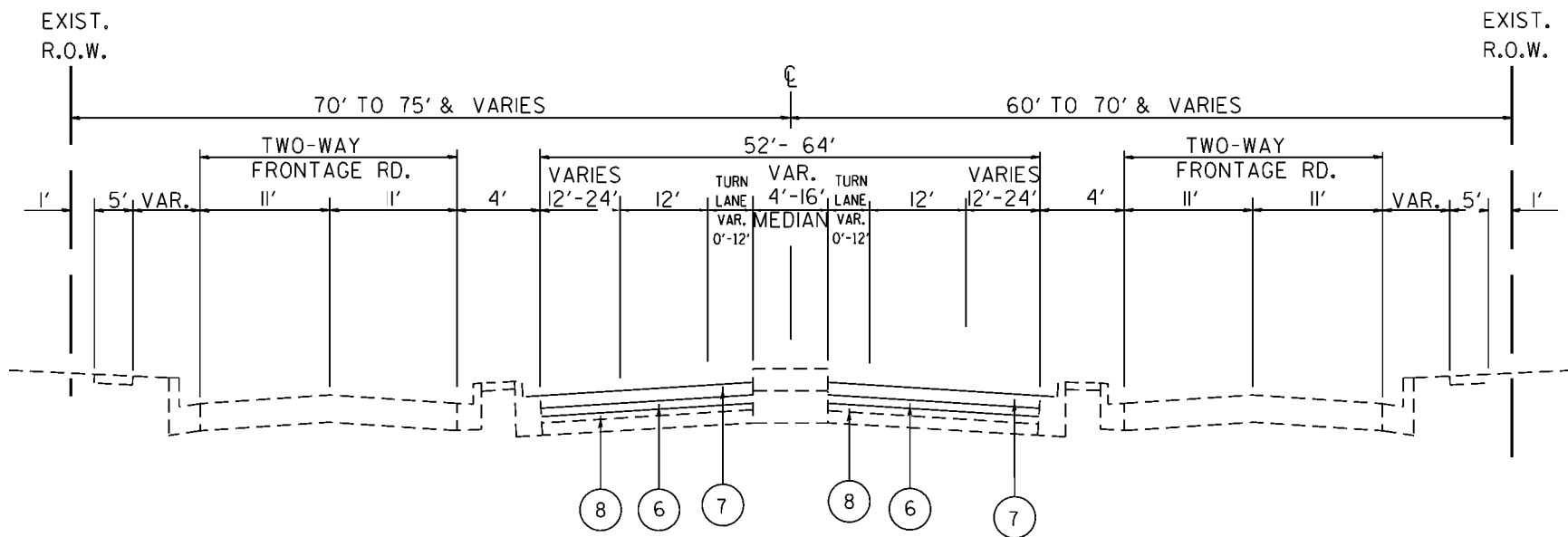
TYPICAL SECTIONS	
PALATINE ROAD (US 14 (NORTHWEST HIGHWAY) TO US 12 (RAND RD.))	
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2018-098-R565W	COOK	35	6
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62H47	



EXISTING TYPICAL SECTION  
PALATINE ROAD  
STA. 51+00 TO STA. 167+60  
(NOT TO SCALE)

- LEGEND**
- ① EXISTING P.C.C. PAVEMENT, 10"
  - ② EXISTING HMA PAVEMENT, ±4"
  - ③ EXISTING CONCRETE MEDIAN
  - ④ EXISTING CURB AND GUTTER
  - ⑤ PROPOSED HMA SURFACE REMOVAL, 2½"
  - ⑥ PROPOSED POLYMERIZED LEVELING BINDER (MM), IL 4.75, ¾"
  - ⑦ PROPOSED POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, N80, 1¾"
  - ⑧ HMA OVERLAY AFTER MILLING, ±1½"



PROPOSED TYPICAL SECTION  
PALATINE ROAD  
STA. 51+00 TO STA. 167+60  
(NOT TO SCALE)

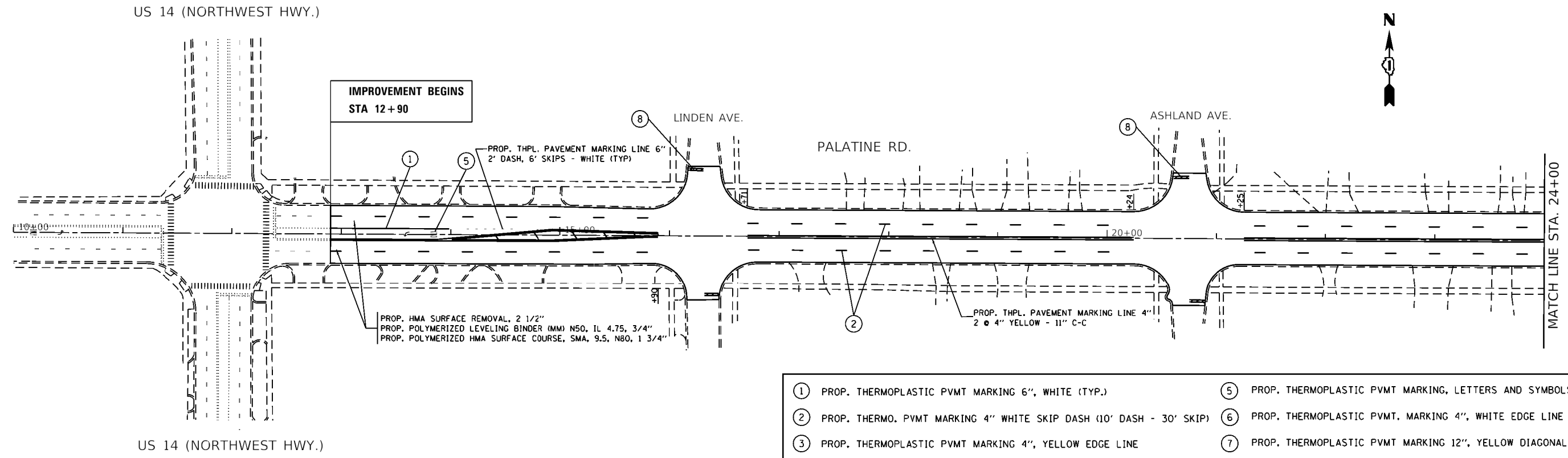
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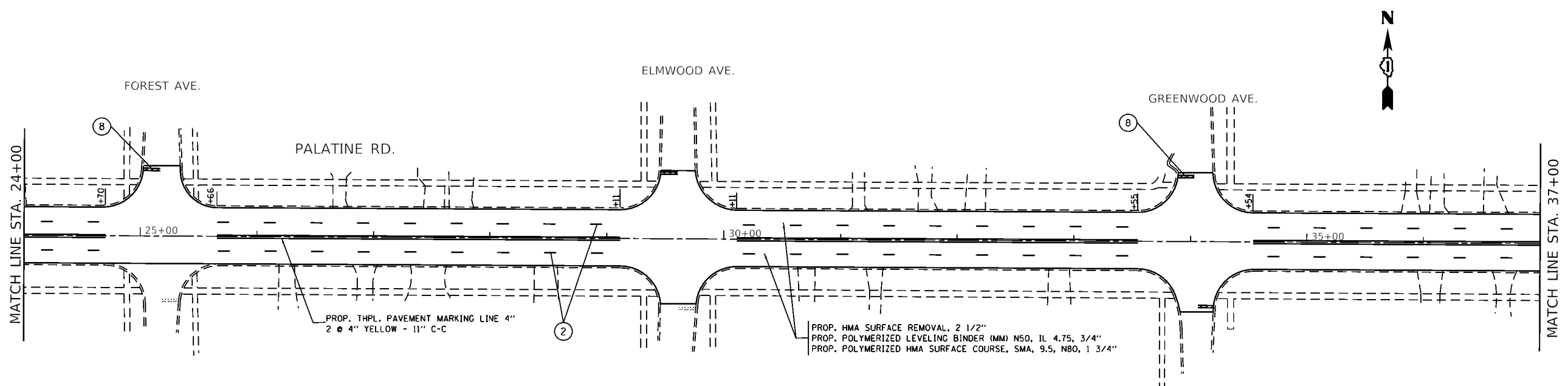
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>TYPICAL SECTIONS</b>	
<b>PALATINE ROAD (US 14 (NORTHWEST HIGHWAY) TO US 12 (RAND RD.))</b>	
SCALE: NONE	SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2018-098-R5&SW	COOK	35	7
CONTRACT NO. 62H47				
ILLINOIS FED. AID PROJECT				



- ① PROP. THERMOPLASTIC PVMT MARKING 6", WHITE (TYP.)
- ② PROP. THERMO. PVMT MARKING 4" WHITE SKIP DASH (10' DASH - 30' SKIP)
- ③ PROP. THERMOPLASTIC PVMT MARKING 4", YELLOW EDGE LINE
- ④ PROP. THERMOPLASTIC PVMT MARKING 8", WHITE (TYP.)
- ⑤ PROP. THERMOPLASTIC PVMT MARKING, LETTERS AND SYMBOLS (TYP.)
- ⑥ PROP. THERMOPLASTIC PVMT. MARKING 4", WHITE EDGE LINE
- ⑦ PROP. THERMOPLASTIC PVMT MARKING 12", YELLOW DIAGONAL (75' C/C)
- ⑧ PROP. THERMOPLASTIC PVMT MARKING 24", WHITE (TYP.)



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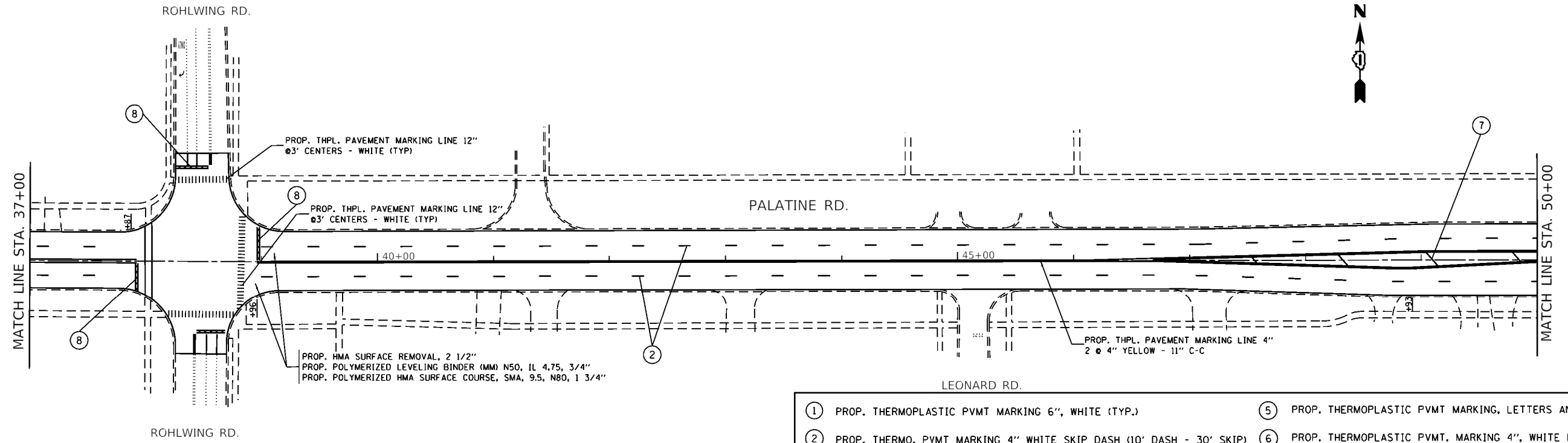
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED ROADWAY PLAN**  
**PALATINE RD. (U.S. ROUTE 14 (NORTHWEST HWY) TO US 12 (RAND RD.))**

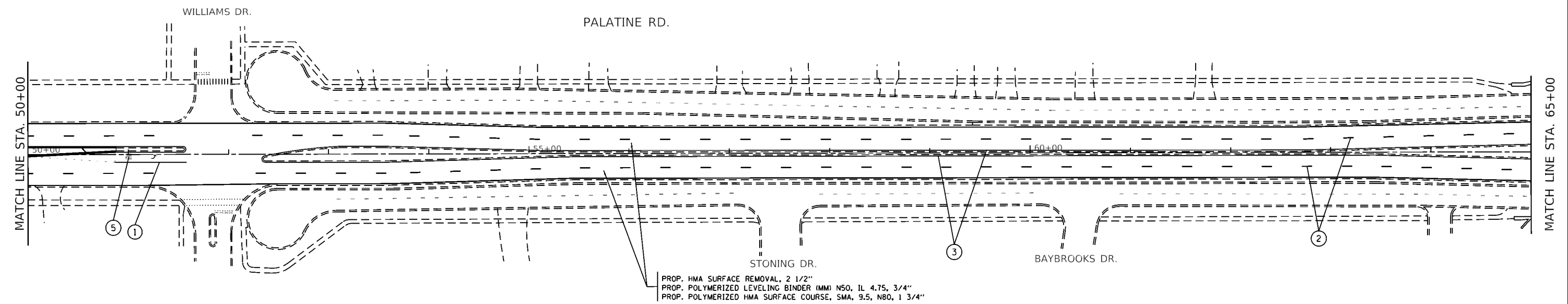
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2018-098-R56SW	COOK	35	8
CONTRACT NO. 62H47				
ILLINOIS FED. AID PROJECT				





- ① PROP. THERMOPLASTIC PVMT MARKING 6", WHITE (TYP.)
- ② PROP. THERMO. PVMT MARKING 4" WHITE SKIP DASH (10' DASH - 30' SKIP)
- ③ PROP. THERMOPLASTIC PVMT MARKING 4", YELLOW EDGE LINE
- ④ PROP. THERMOPLASTIC PVMT MARKING 8", WHITE (TYP.)
- ⑤ PROP. THERMOPLASTIC PVMT MARKING, LETTERS AND SYMBOLS (TYP.)
- ⑥ PROP. THERMOPLASTIC PVMT. MARKING 4", WHITE EDGE LINE
- ⑦ PROP. THERMOPLASTIC PVMT MARKING 12", YELLOW DIAGONAL (75' C/C)
- ⑧ PROP. THERMOPLASTIC PVMT MARKING 24", WHITE (TYP.)



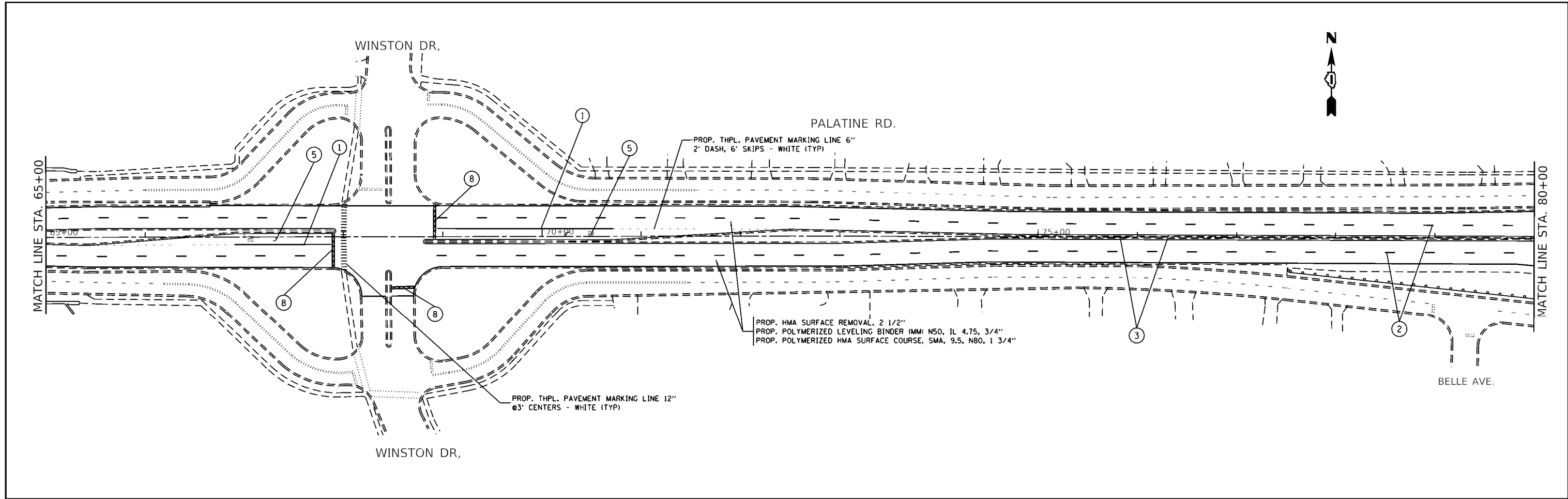
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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED ROADWAY PLAN**  
**PALATINE RD. (U.S. ROUTE 14 (NORTHWEST HWY) TO US 12 (RAND RD.))**  
 SCALE: 1"=50'    SHEET    OF    SHEETS    STA.    TO    STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2018-098-R56SW	COOK	35	9
CONTRACT NO. 62H47				
ILLINOIS FED. AID PROJECT				



- |   |   |
|---|---|
| ① PROP. THERMOPLASTIC PVMT MARKING 6", WHITE (TYP.)                   | ⑤ PROP. THERMOPLASTIC PVMT MARKING, LETTERS AND SYMBOLS (TYP.)    |
| ② PROP. THERMO. PVMT MARKING 4" WHITE SKIP DASH (10' DASH - 30' SKIP) | ⑥ PROP. THERMOPLASTIC PVMT. MARKING 4", WHITE EDGE LINE           |
| ③ PROP. THERMOPLASTIC PVMT MARKING 4", YELLOW EDGE LINE               | ⑦ PROP. THERMOPLASTIC PVMT MARKING 12", YELLOW DIAGONAL (75' C/C) |
| ④ PROP. THERMOPLASTIC PVMT MARKING 8", WHITE (TYP.)                   | ⑧ PROP. THERMOPLASTIC PVMT MARKING 24", WHITE (TYP.)              |

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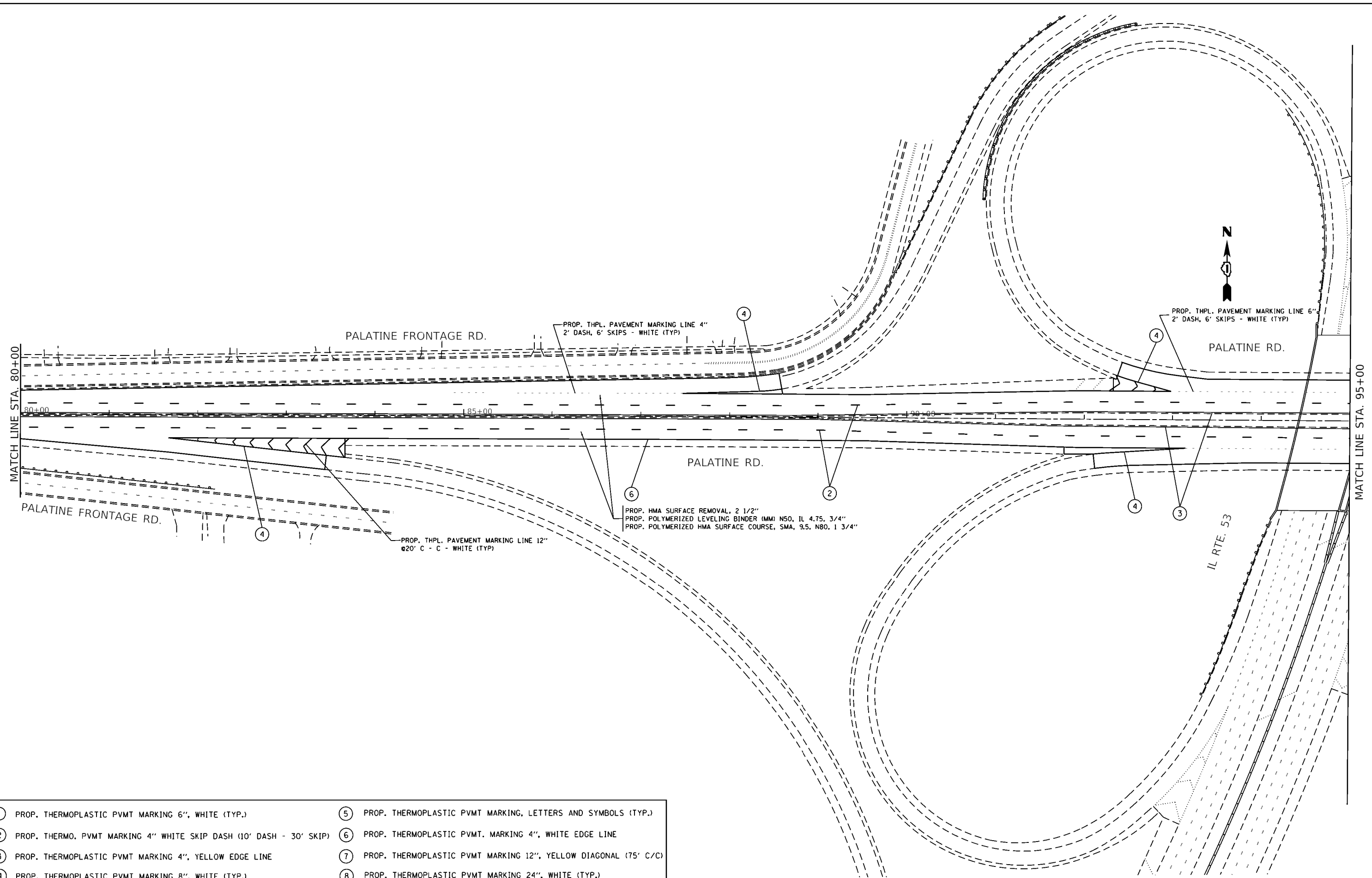
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED ROADWAY PLAN  
PALATINE RD. (U.S. ROUTE 14 (NORTHWEST HWY) TO US 12 (RAND RD.))**

SCALE: 1"=50'      SHEET      OF      SHEETS      STA.      TO      STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2018-098-R56SW	COOK	35	10
CONTRACT NO. 62H47			ILLINOIS FED. AID PROJECT	

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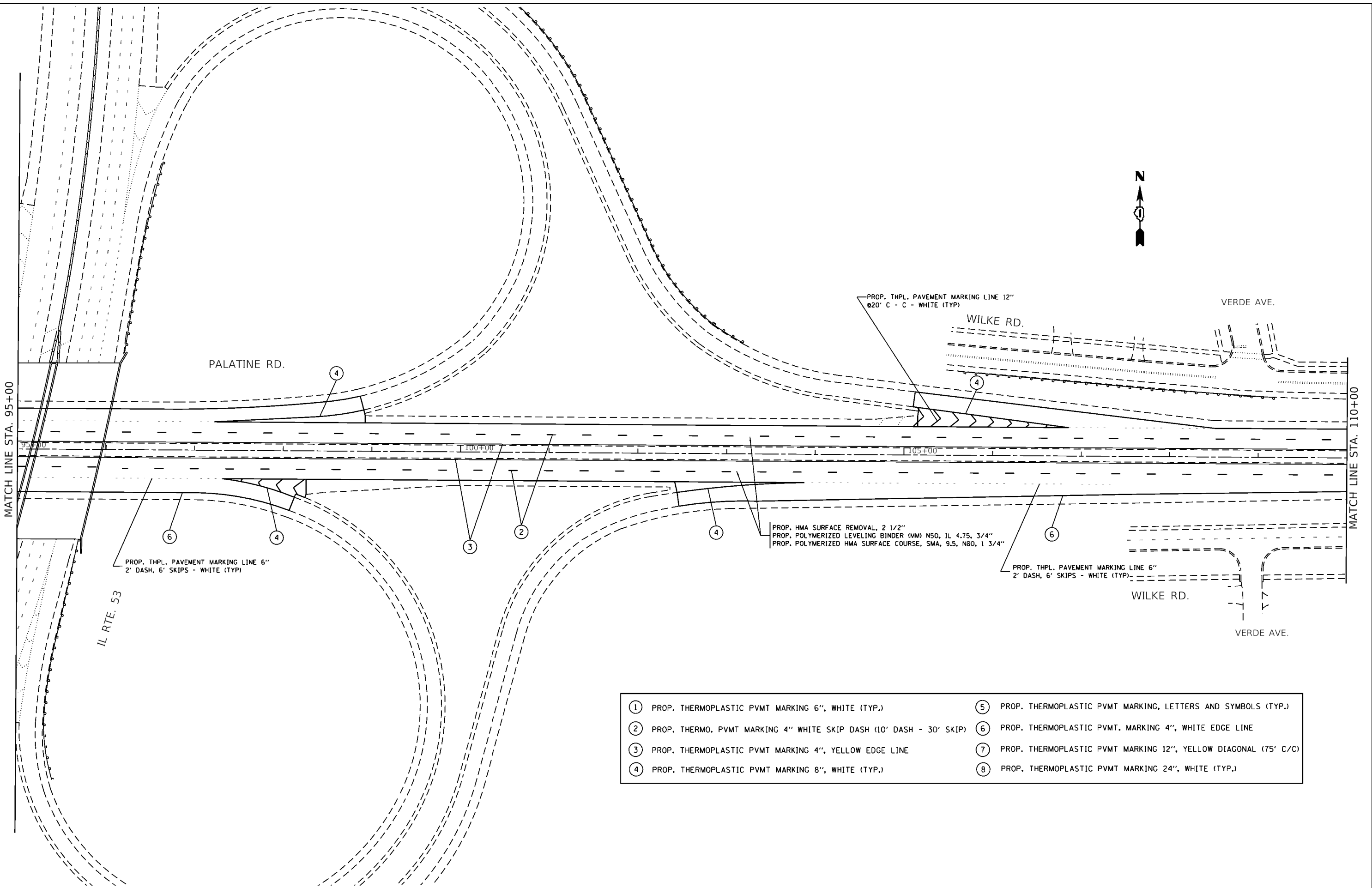
- |   |   |
|---|---|
| ① PROP. THERMOPLASTIC PVMT MARKING 6", WHITE (TYP.)                   | ⑤ PROP. THERMOPLASTIC PVMT MARKING, LETTERS AND SYMBOLS (TYP.)    |
| ② PROP. THERMO. PVMT MARKING 4" WHITE SKIP DASH (10' DASH - 30' SKIP) | ⑥ PROP. THERMOPLASTIC PVMT. MARKING 4", WHITE EDGE LINE           |
| ③ PROP. THERMOPLASTIC PVMT MARKING 4", YELLOW EDGE LINE               | ⑦ PROP. THERMOPLASTIC PVMT MARKING 12", YELLOW DIAGONAL (75' C/C) |
| ④ PROP. THERMOPLASTIC PVMT MARKING 8", WHITE (TYP.)                   | ⑧ PROP. THERMOPLASTIC PVMT MARKING 24", WHITE (TYP.)              |

USER NAME = guillaumefp	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/19/2018	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED ROADWAY PLAN**  
**PALATINE RD. (U.S. ROUTE 14 (NORTHWEST HWY) TO US 12 (RAND RD.))**  
 SCALE: 1"=50'    SHEET    OF    SHEETS    STA.    TO    STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	2018-098-R5&SW	COOK	35	11
CONTRACT NO. 62H47				
ILLINOIS FED. AID PROJECT				



PROP. THPL. PAVEMENT MARKING LINE 6"  
2' DASH, 6' SKIPS - WHITE (TYP)

PROP. HMA SURFACE REMOVAL, 2 1/2"  
PROP. POLYMERIZED LEVELING BINDER (MM) N50, IL 4.75, 3/4"  
PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, N80, 1 3/4"

PROP. THPL. PAVEMENT MARKING LINE 6"  
2' DASH, 6' SKIPS - WHITE (TYP)

- |   |   |   |   |
|---|---|---|---|
| ① | PROP. THERMOPLASTIC PVMT MARKING 6", WHITE (TYP.)                   | ⑤ | PROP. THERMOPLASTIC PVMT MARKING, LETTERS AND SYMBOLS (TYP.)    |
| ② | PROP. THERMO. PVMT MARKING 4" WHITE SKIP DASH (10' DASH - 30' SKIP) | ⑥ | PROP. THERMOPLASTIC PVMT. MARKING 4", WHITE EDGE LINE           |
| ③ | PROP. THERMOPLASTIC PVMT MARKING 4", YELLOW EDGE LINE               | ⑦ | PROP. THERMOPLASTIC PVMT MARKING 12", YELLOW DIAGONAL (75' C/C) |
| ④ | PROP. THERMOPLASTIC PVMT MARKING 8", WHITE (TYP.)                   | ⑧ | PROP. THERMOPLASTIC PVMT MARKING 24", WHITE (TYP.)              |

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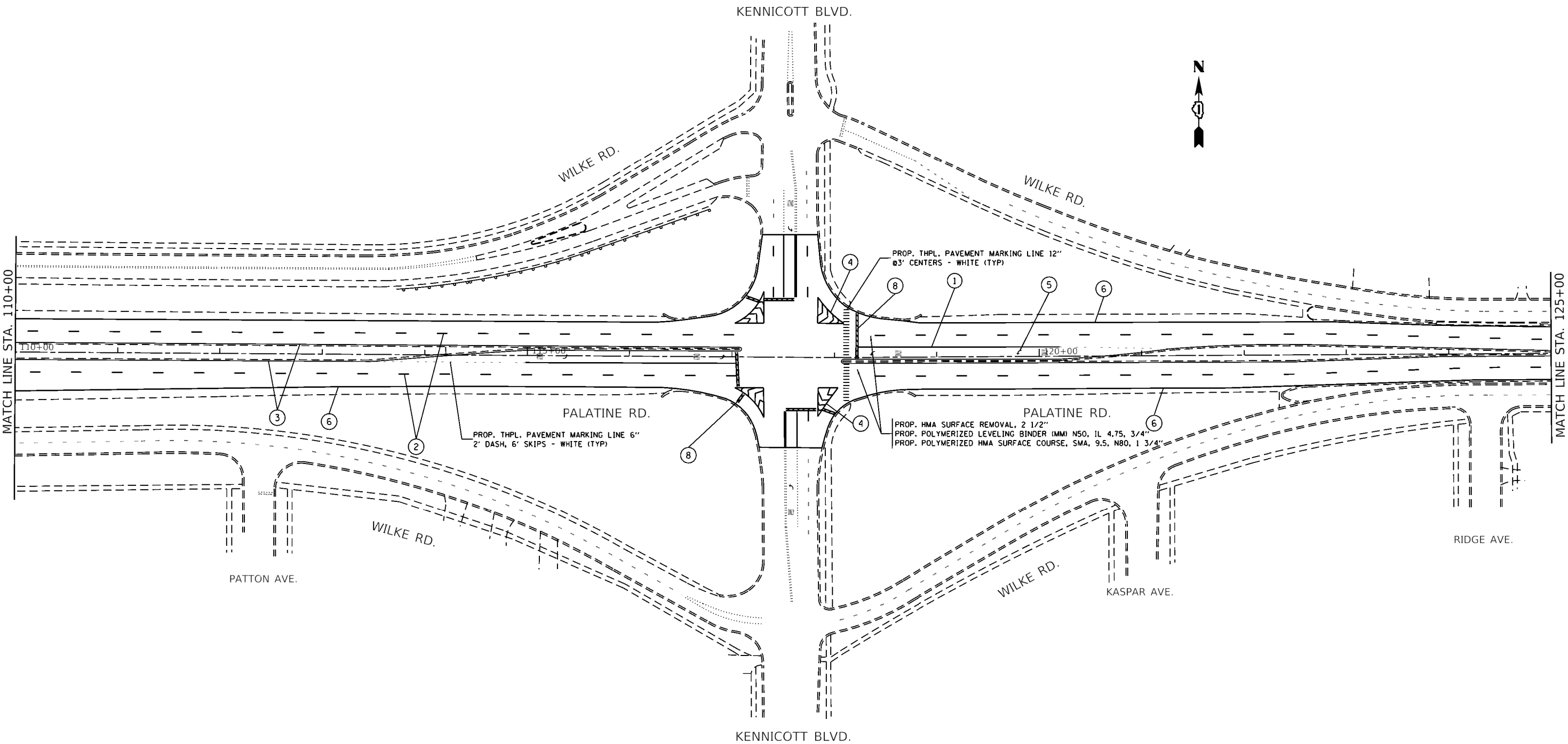
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DRAWN -		REVISD -	
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PLOT DATE =	10/19/2018	DATE -	REVISD -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED ROADWAY PLAN  
PALATINE RD. (U.S. ROUTE 14 (NORTHWEST HWY) TO US 12 (RAND RD.))**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2018-098-R5&SW	COOK	35	12
CONTRACT NO. 62H47			ILLINOIS FED. AID PROJECT	

SCALE: 1"=50'    SHEET    OF    SHEETS    STA.    TO    STA.



- |   |   |
|---|---|
| ① PROP. THERMOPLASTIC PVMT MARKING 6", WHITE (TYP.)                   | ⑤ PROP. THERMOPLASTIC PVMT MARKING, LETTERS AND SYMBOLS (TYP.)    |
| ② PROP. THERMO. PVMT MARKING 4" WHITE SKIP DASH (10' DASH - 30' SKIP) | ⑥ PROP. THERMOPLASTIC PVMT. MARKING 4", WHITE EDGE LINE           |
| ③ PROP. THERMOPLASTIC PVMT MARKING 4", YELLOW EDGE LINE               | ⑦ PROP. THERMOPLASTIC PVMT MARKING 12", YELLOW DIAGONAL (75' C/C) |
| ④ PROP. THERMOPLASTIC PVMT MARKING 8", WHITE (TYP.)                   | ⑧ PROP. THERMOPLASTIC PVMT MARKING 24", WHITE (TYP.)              |

USER NAME = quillaumefp	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/19/2018	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

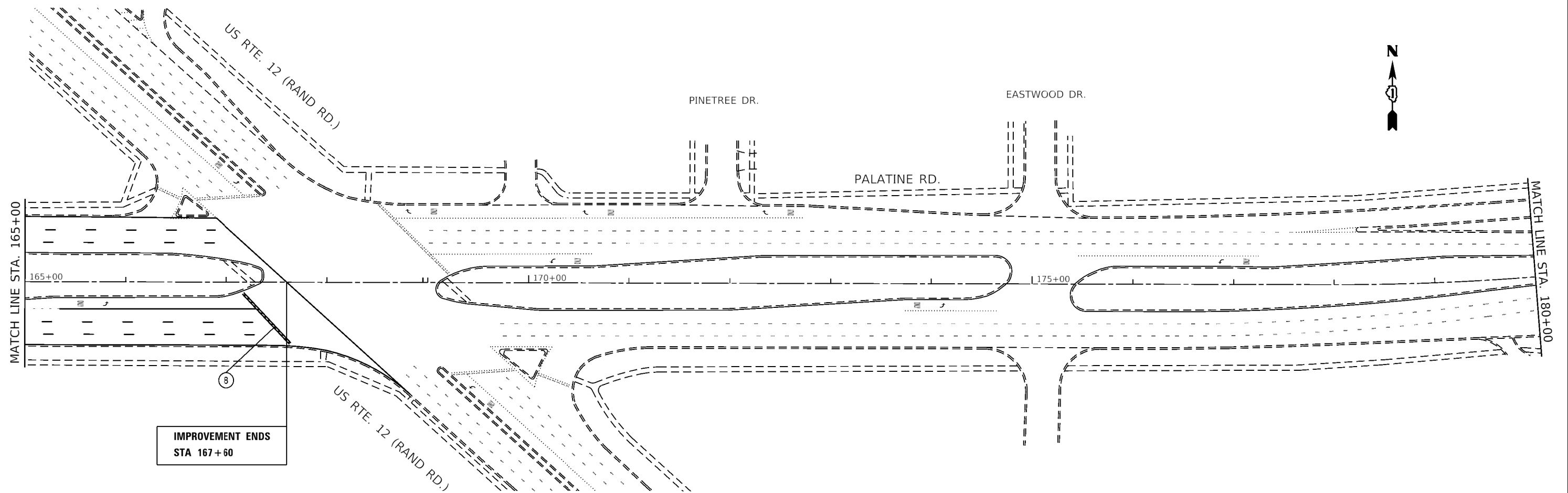
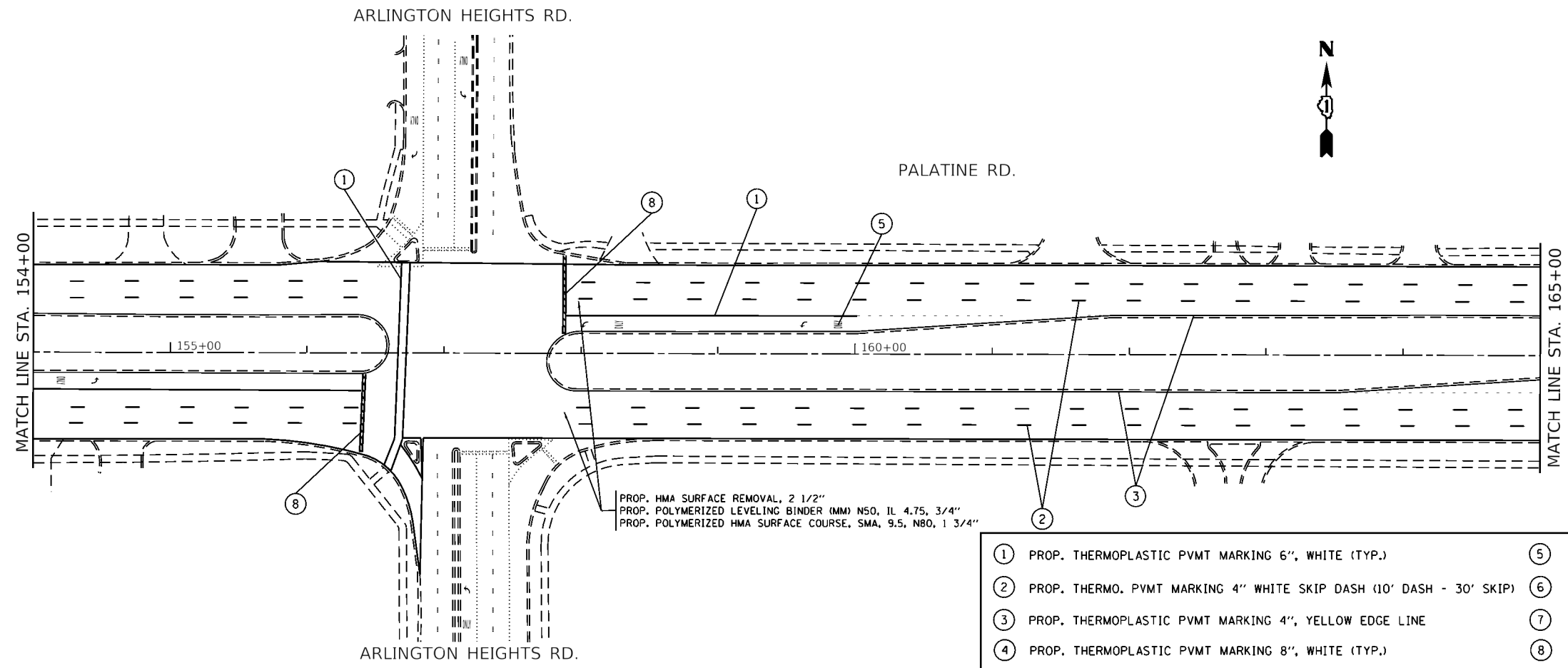
**EXISTING AND PROPOSED ROADWAY PLAN  
PALATINE RD. (U.S. ROUTE 14 (NORTHWEST HWY) TO US 12 (RAND RD.))**

SCALE: 1"=50'    SHEET    OF    SHEETS    STA.    TO    STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2018-098-R5&SW	COOK	35	13
CONTRACT NO. 62H47			ILLINOIS FED. AID PROJECT	

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 PROJECT: 2018-098-R56SW  
 OFFICE: District 1  
 DATE: 10/19/2018

USER NAME = quillaumefp	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/19/2018	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED ROADWAY PLAN  
PALATINE RD. (U.S. ROUTE 14 (NORTHWEST HWY) TO US 12 (RAND RD.))**

SCALE: 1"=50'    SHEET    OF    SHEETS    STA.    TO    STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2018-098-R56SW	COOK	35	15
CONTRACT NO. 62H47				
ILLINOIS FED. AID PROJECT				

### TREE REMOVAL (6 TO 15 UNITS DIAMETER)

<u>STATION</u>	<u>QUANTITY</u>
135+00 TO 136+00 RT	26 UNITS

### TREE REMOVAL (OVER 15 UNITS DIAMETER)

<u>STATION</u>	<u>QUANTITY</u>
45+00 TO 47+00 RT	125 UNITS
135+00 TO 136+00 RT	40 UNITS

### SELECTIVE CLEARING

<u>STATION</u>	<u>QUANTITY</u>
105+00 TO 114+00 LT	4 UNITS

### WEED CONTROL, TEASEL

<u>STATION</u>	<u>AREA (SQ FT) TO SPRAY</u>
105+00 TO 114+00 LT	12750 SQ FT

MODEL: Default  
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USER NAME = quillaumefp	DESIGNED -	REVISED -
	DRAWN -	REVISED -
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PLOT DATE = 10/19/2018	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

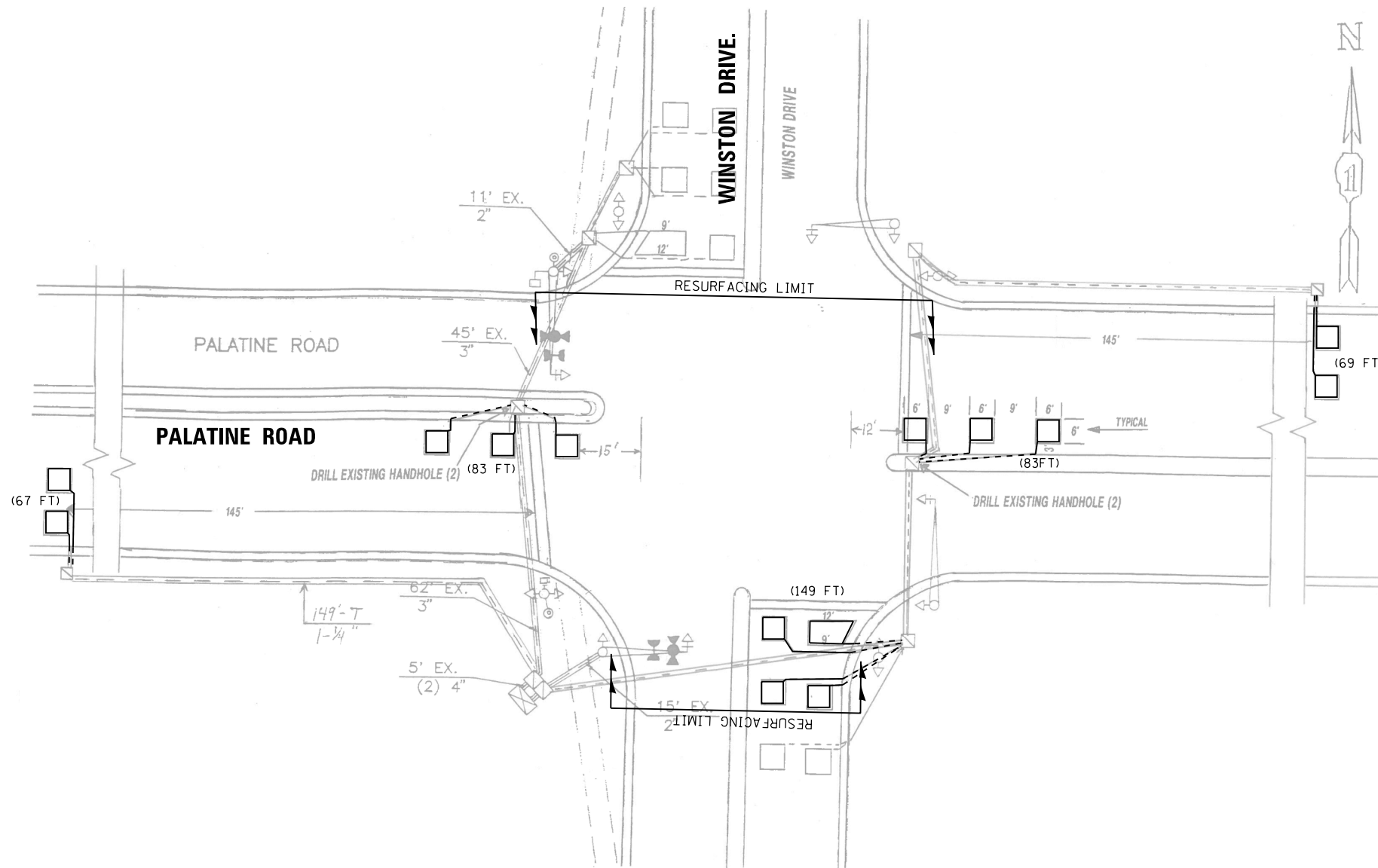
SCHEDULE OF QUANTITIES			
SCALE: NONE	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2018-098-R5&SW	COOK	35	16
				CONTRACT NO. 62H47
				ILLINOIS FED. AID PROJECT



**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



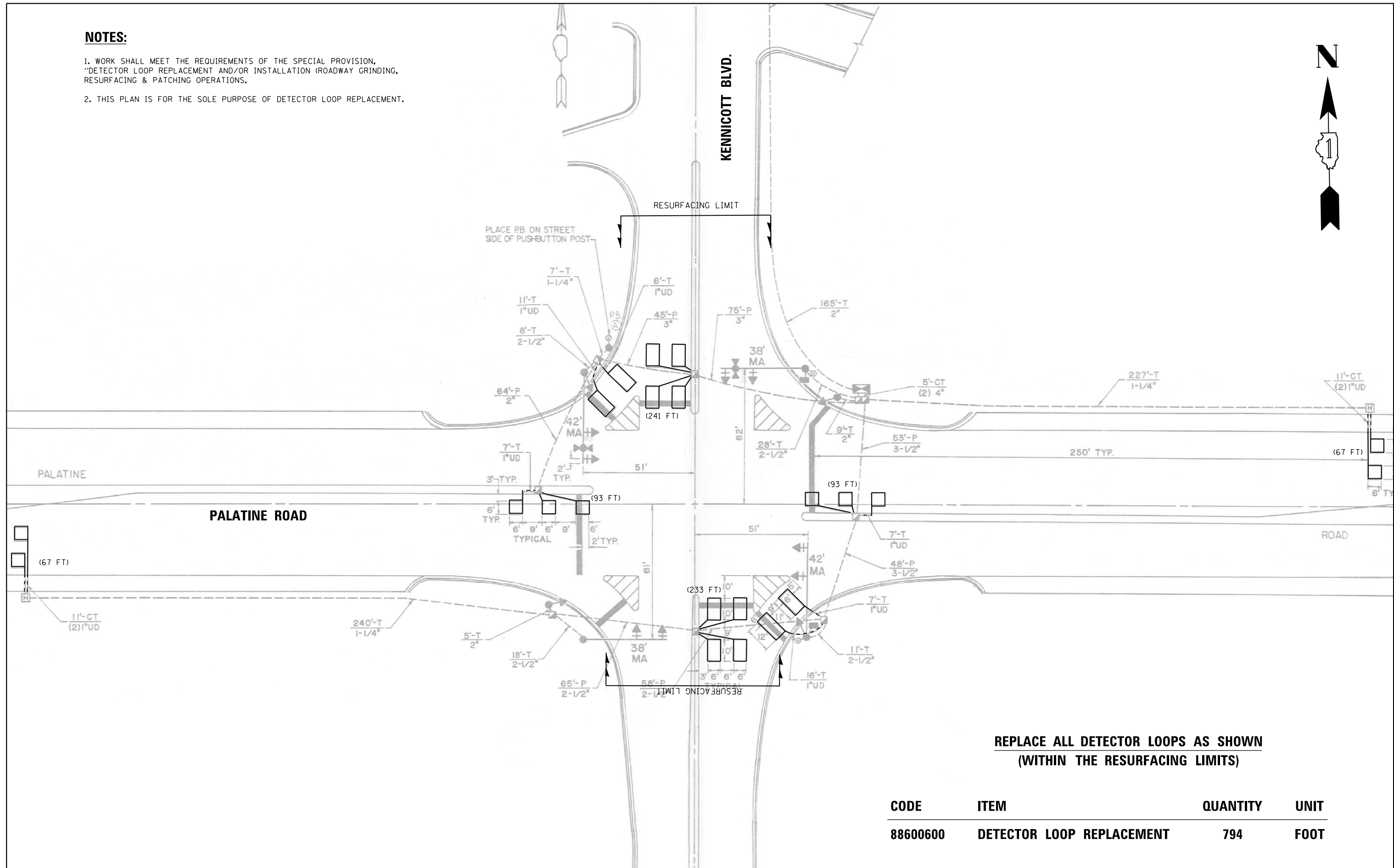
**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	451	FOOT

FILE NAME = 62F23 - Detector Loop Replacement Sheets.dgn	USER NAME = vargosa	DESIGNED - AV	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DETECTOR LOOP REPLACEMENT PLAN WINSTON DRIVE AT PALATINE RD.</b>	F.A.P. RTE. 305	SECTION 2018 - 098-RS&SW	COUNTY COOK	TOTAL SHEETS 35	SHEET NO. 17		
PLOT SCALE = 40.0069' / in.	CHECKED - LP	REVISED -	SCALE:			SHEET OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT				
Default	PLOT DATE = 10/5/2018	DATE - 04/24/2018	REVISED -									

**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	794	FOOT

FILE NAME = 62F23 - Detector Loop Replacement Sheets.dgn	USER NAME = vargosa	DESIGNED - AV	REVISED -
		DRAWN - AV	REVISED -
		CHECKED - LP	REVISED -
		DATE - 04/23/2018	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

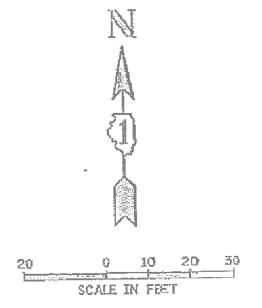
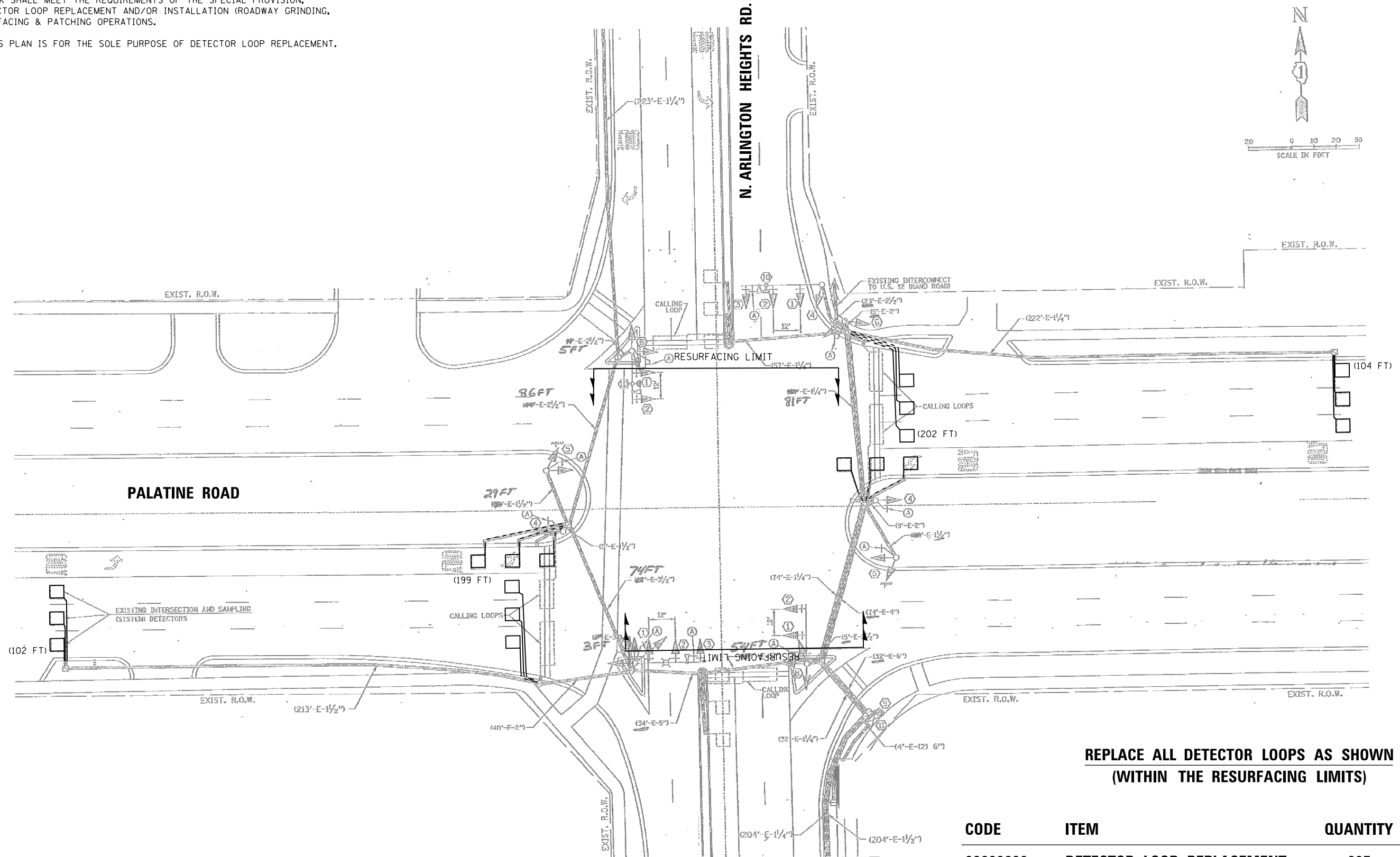
**DETECTOR LOOP REPLACEMENT PLAN  
KENNICOTT BLVD. AT PALATINE RD.**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 305	SECTION 2018 - 098-RS&SW	COUNTY COOK	TOTAL SHEETS 35	SHEET NO. 18
				CONTRACT NO. 62H47
ILLINOIS FED. AID PROJECT				

**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

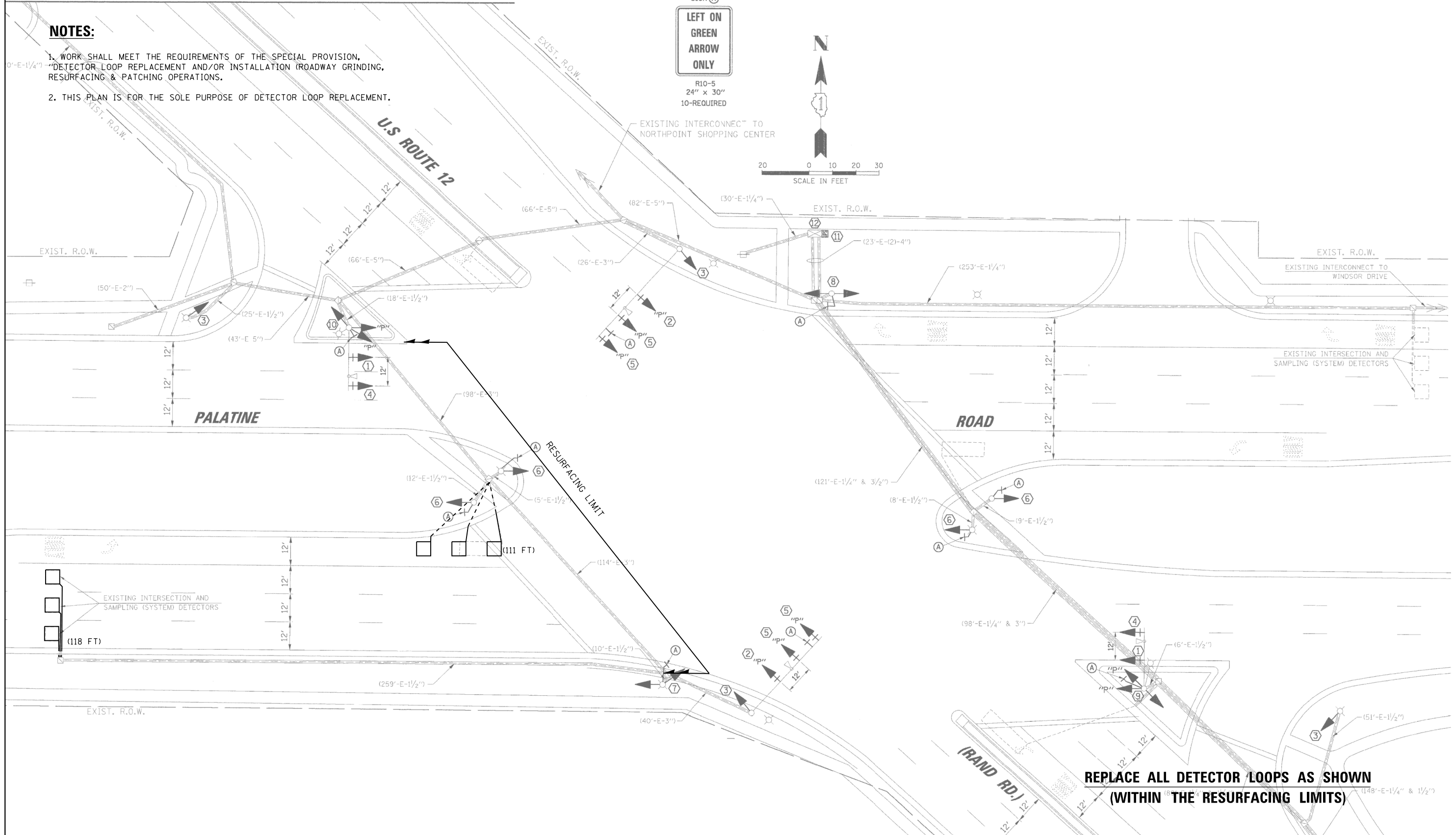
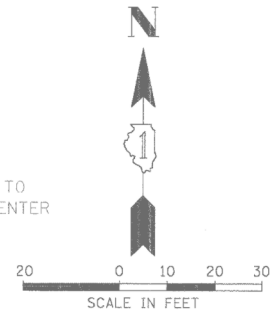
CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	607	FOOT

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Default	PLOT SCALE = 40.0204' / in.	CHECKED - LP	REVISED -			SCALE:	SHEET OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT			
	PLOT DATE = 10/5/2018	DATE - 04/20/2018	REVISED -									
CONTRACT NO. 62H47												

MATCH LINE A-A

**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	229	FOOT

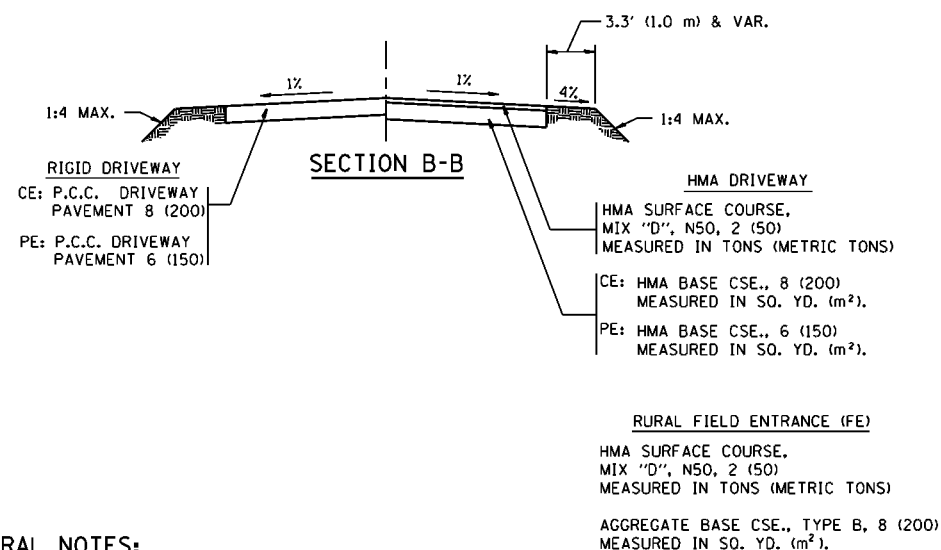
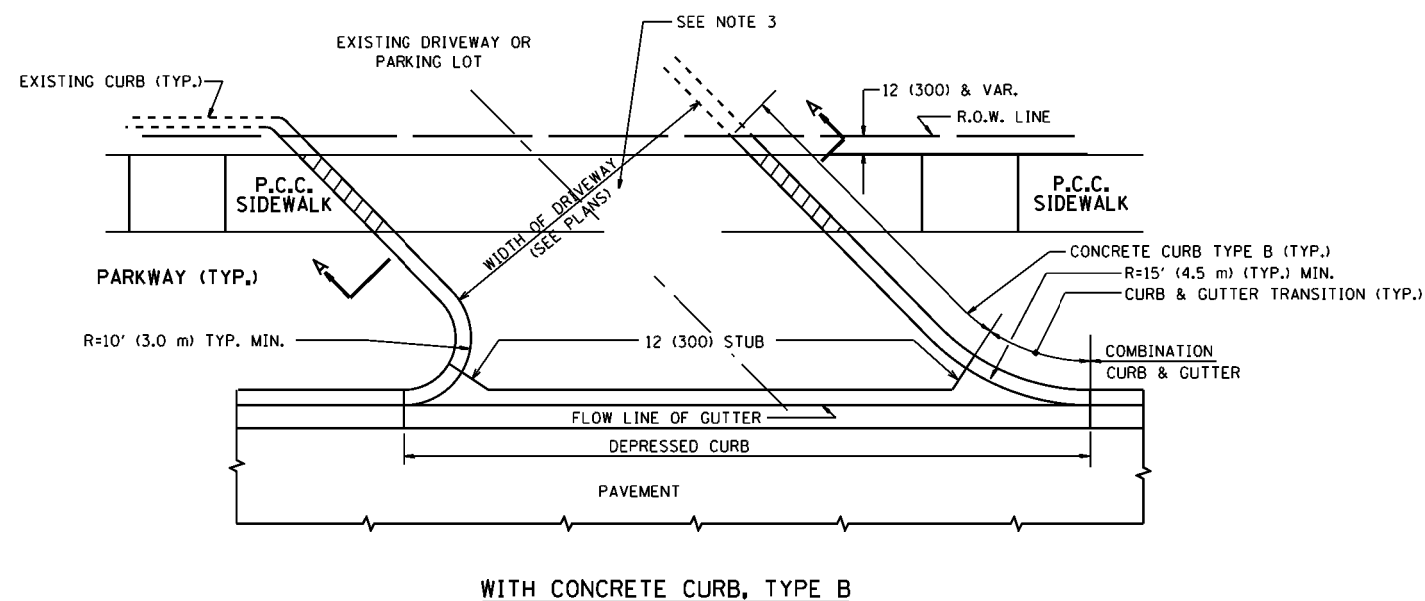
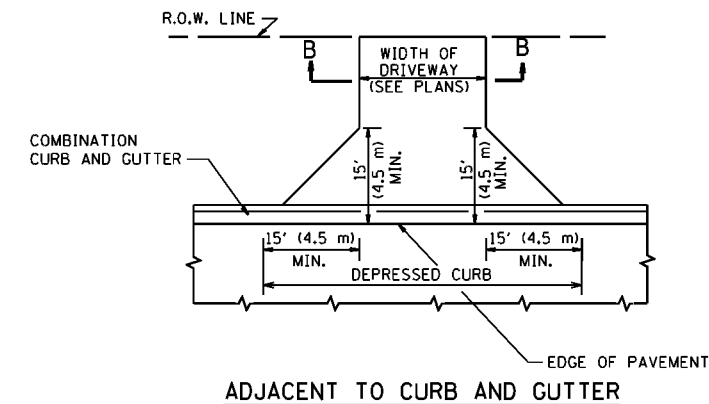
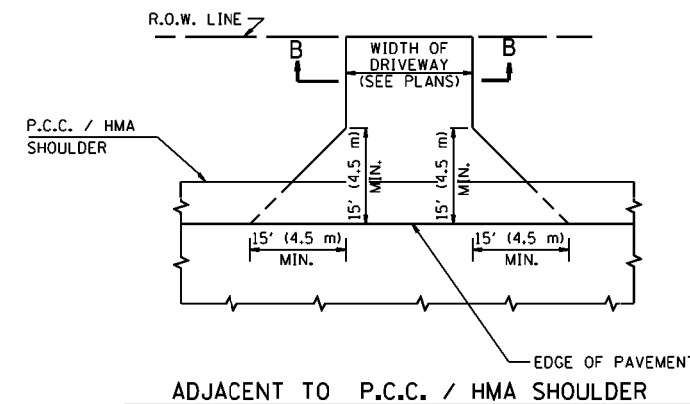
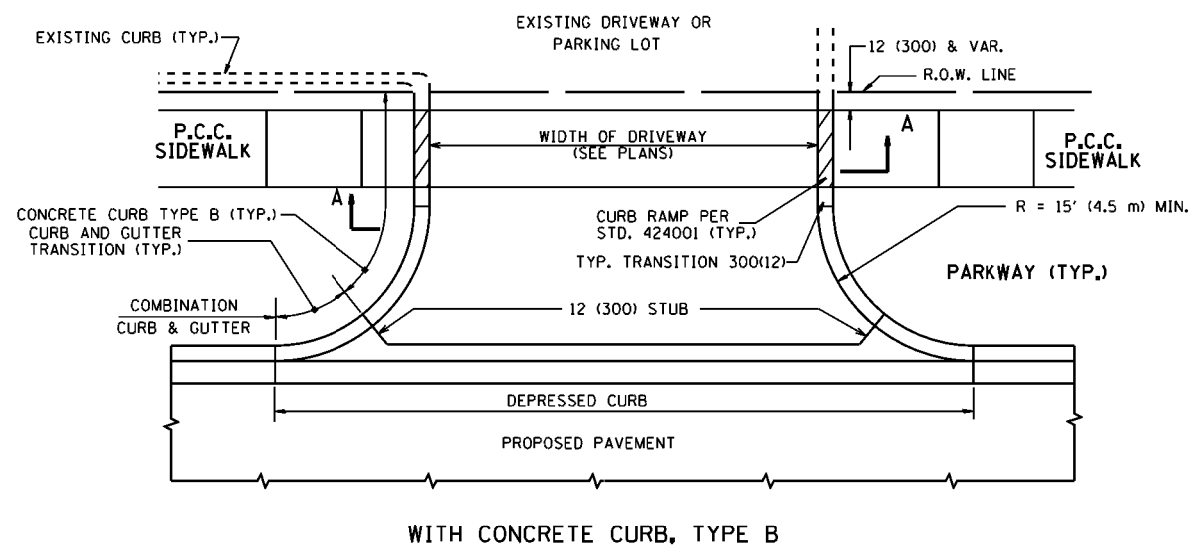
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	PLOT DATE = 10/5/2018	DATE - 04/23/2018	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN  
E RAND RD. AT PALATINE RD.**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 305	SECTION 2018 - 098-RS&SW	COUNTY COOK	TOTAL SHEETS 35	SHEET NO. 20
CONTRACT NO. 62H47				
ILLINOIS FED. AID PROJECT				



**GENERAL NOTES:**

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

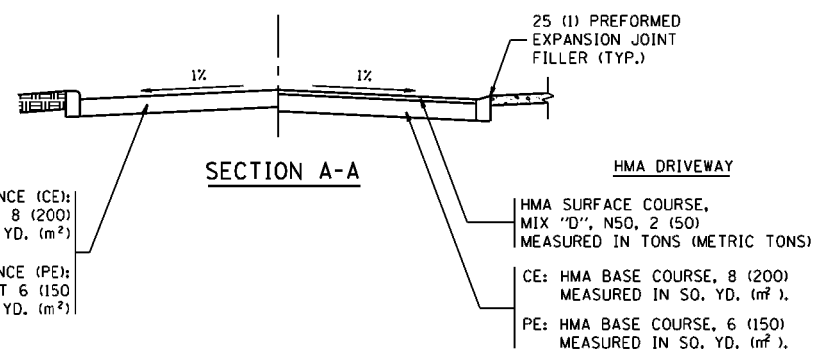
COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

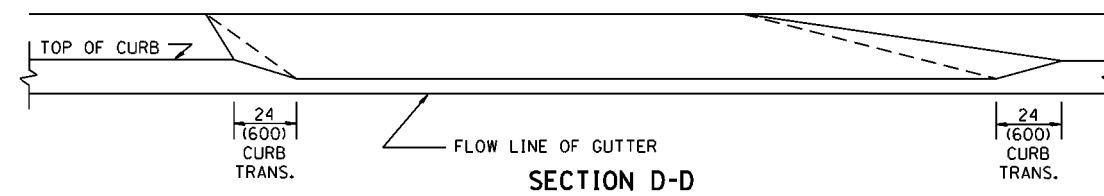
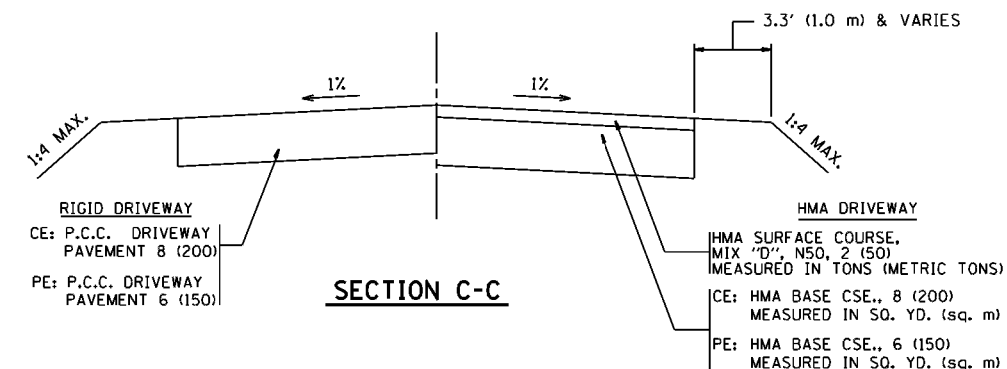
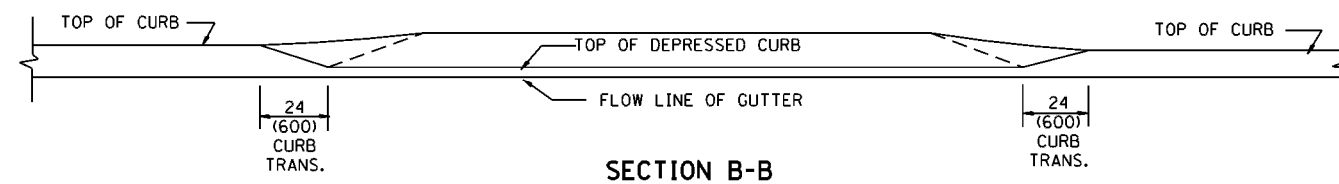
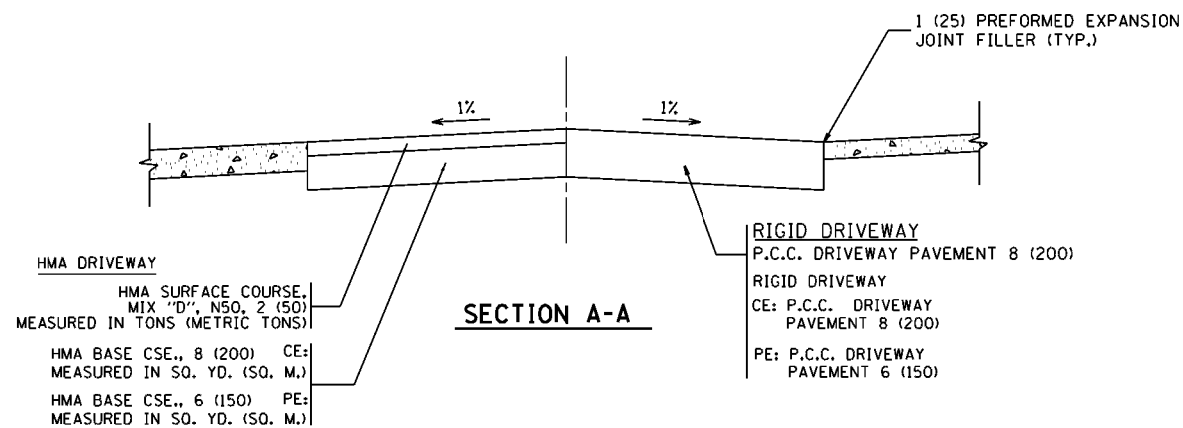
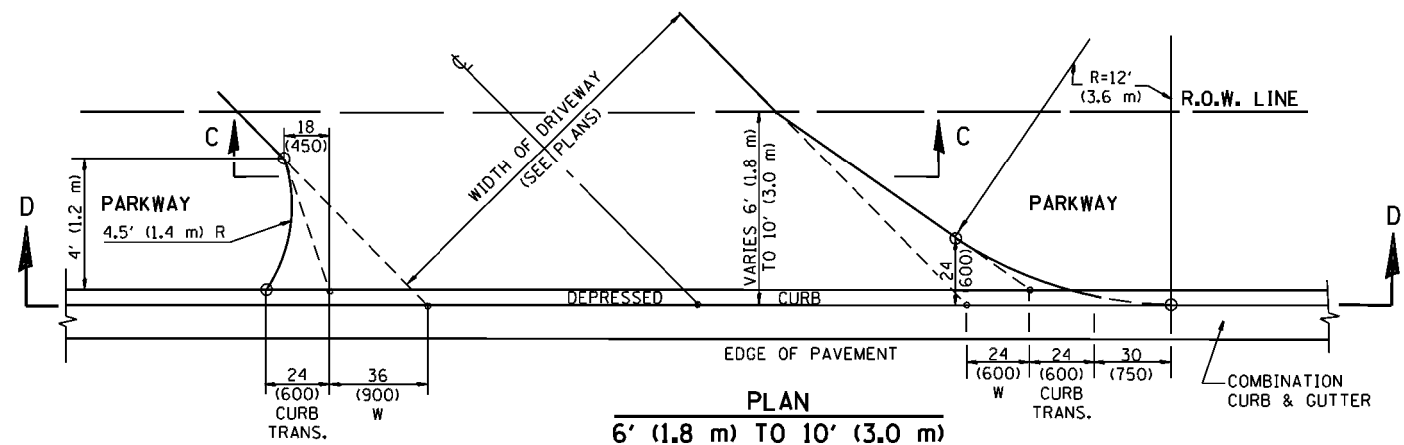
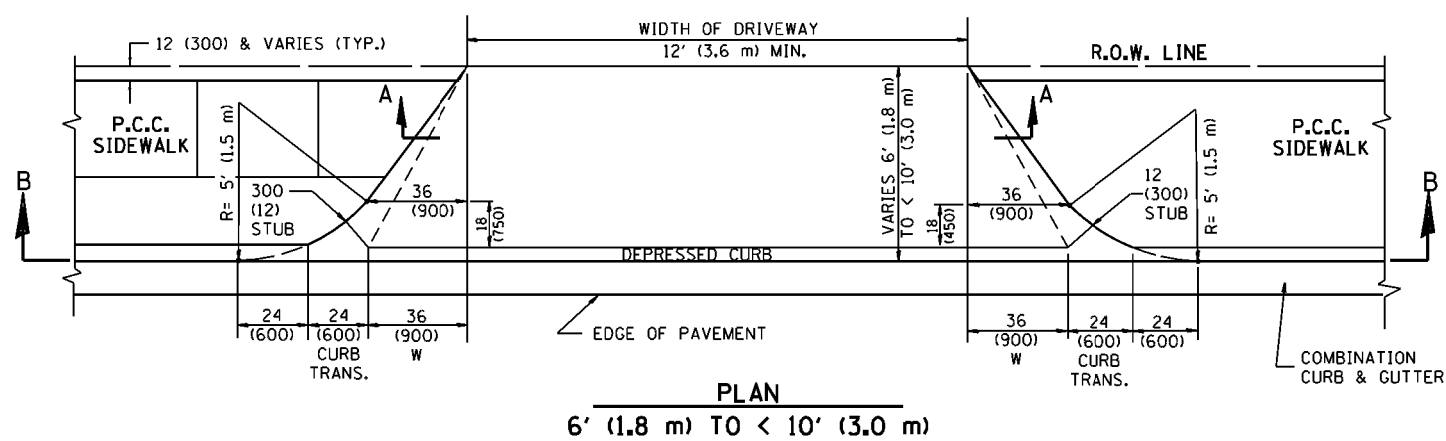
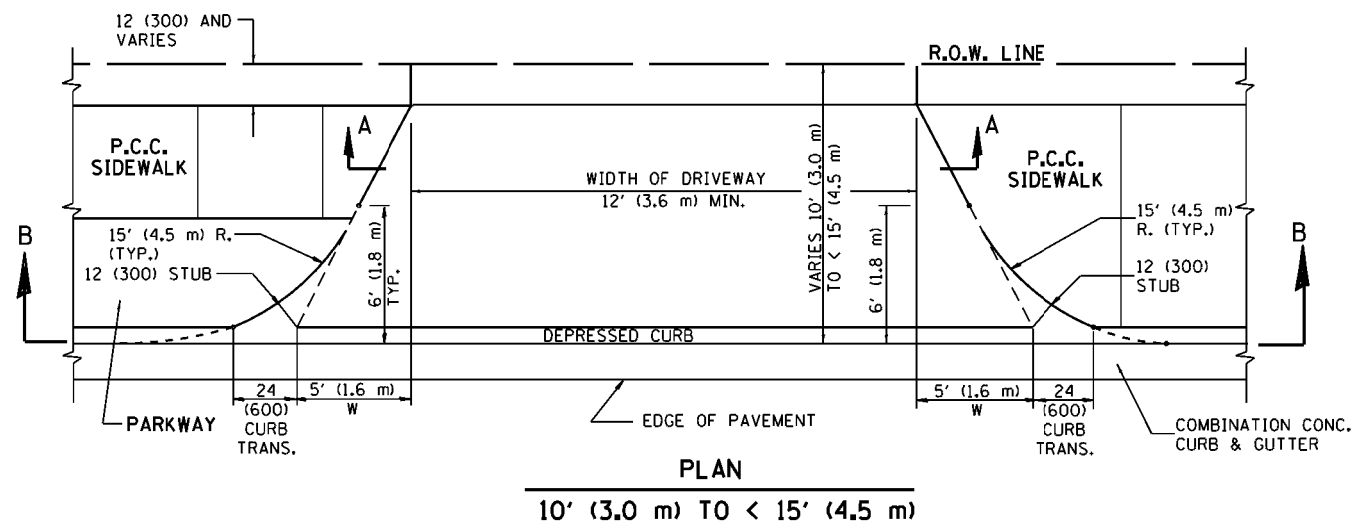


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	PLOT DATE = 10/5/2018	DATE - 11-04-95	REVISED - R. BORO 09-06-11

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB &amp; EDGE OF SHOULDER &gt;= 15' (4.5 m)</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2018-09B-RS&SW	COOK	35	21
<b>BD0156-07 (BD-01)</b>		<b>CONTRACT NO.</b>	62H47	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**GENERAL NOTES**

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

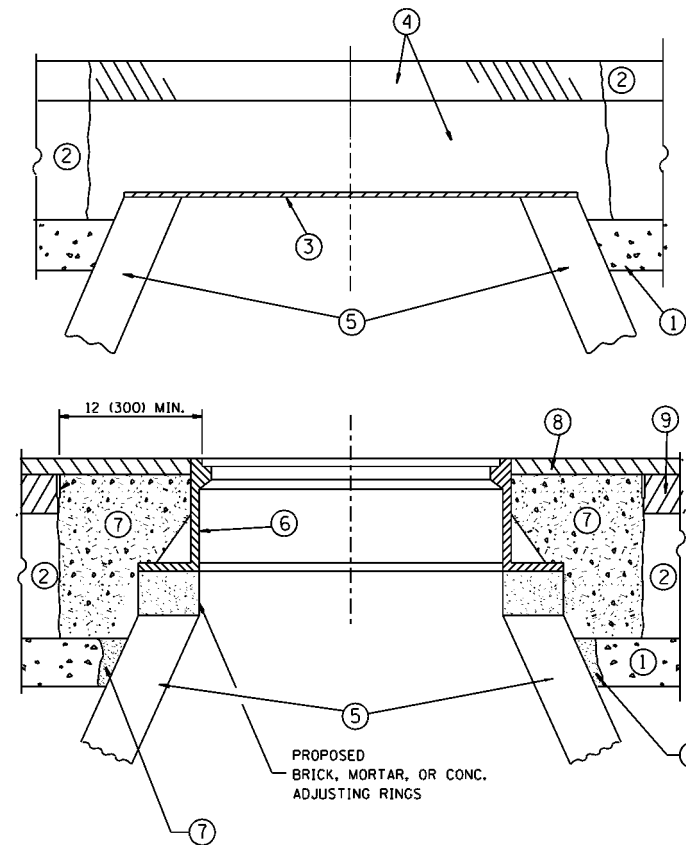
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

FILE NAME =	USER NAME = gulloumefp	DESIGNED - R. SHAH	REVISED - M. GOMEZ 04-05-01
p:\11\084EBID\INTEG\illinois.gov\FWIDOT\Documents\IDOT Offices\District 1\Projects\DR02\DR02\Design\DistStd.dgn		REVISOR - P. LOFLEUR 04-15-03	
PLOT SCALE = 100.0000' / 1"		CHECKED -	REVISED - R. BORO 01-01-07
PLOT DATE = 10/5/2018		DATE - 11-06-95	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

DRIVEWAY DETAILS			
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2018-098-RS&SW	COOK	35	22
BD400-02 (BD-02)			CONTRACT NO. 62H47	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1\* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

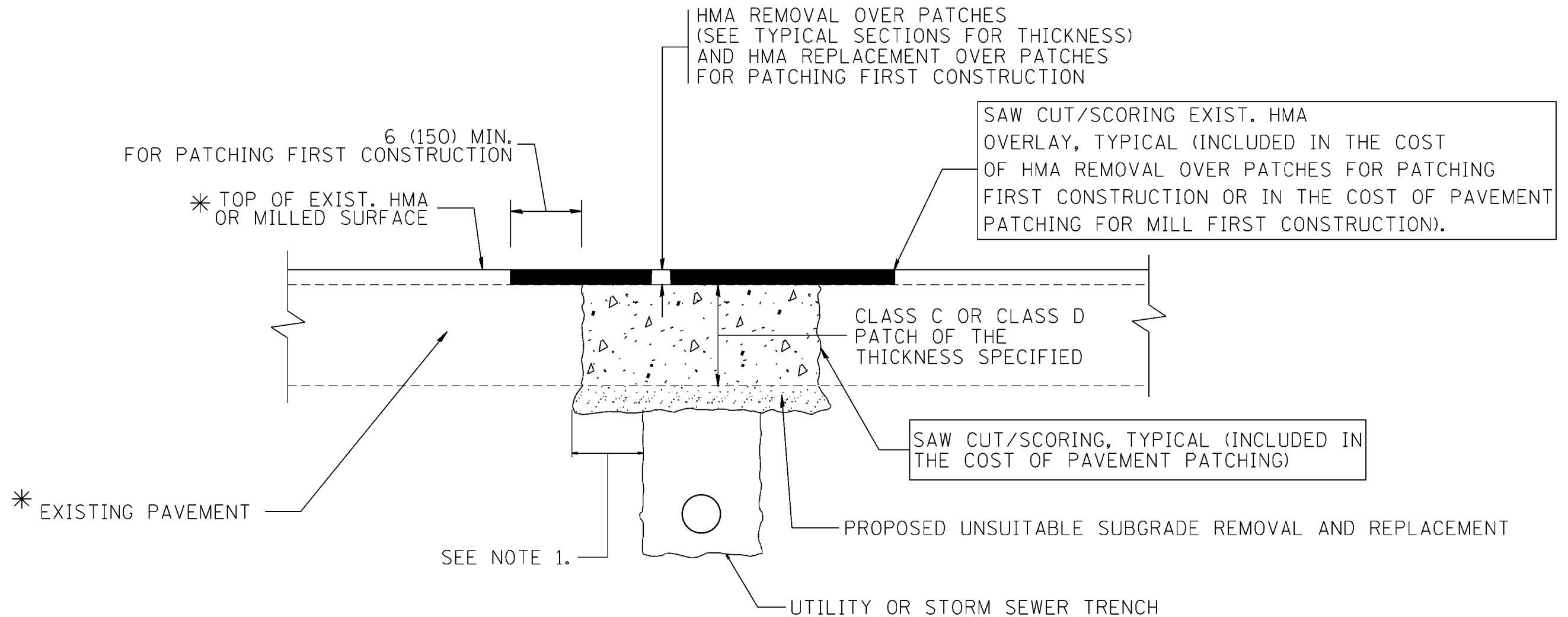
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = gulloumefp	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
p:\11\084EBID\INTEG\illinois.gov\FWIDOT\Documents\IDOT Offices\District 1\Projects\DI052\Drawings\Design\DistStd.dgn			REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 10/5/2018	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2018-098-RS&SW	COOK	35	23
BD600-03 (BD-8)		CONTRACT NO. 62H47		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

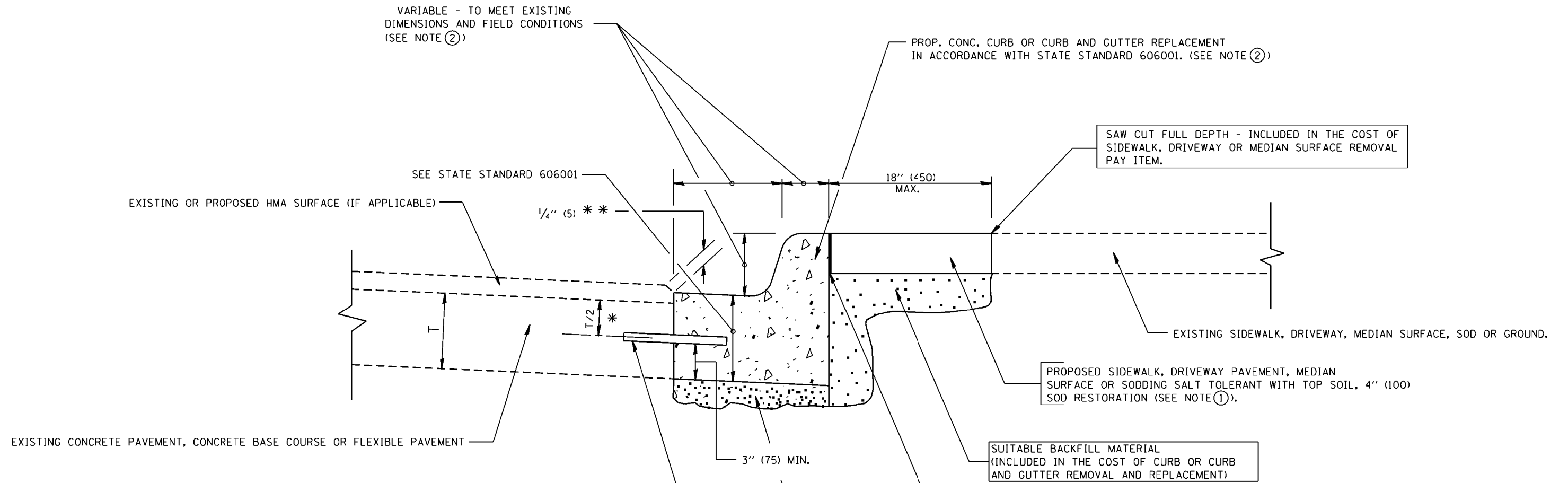
SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = gulloumefp	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>			F.A.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\11084EBID\INTEG\illinois.gov\FWIDOT\Documents\IDOT Offices\District 1\Projects\1082\Drawings\Design\DistStd.dgn		REVISOR - R. BORO 01-01-07	REVISED - R. BORO 01-01-07					305	2018-098-RS&SW	COOK	35	24
PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISOR - R. BORO 09-04-07	REVISED - R. BORO 09-04-07		BD400-04 (BD-22)		CONTRACT NO. 62H47					
PLOT DATE = 10/5/2018	DATE - 10-25-94	REVISOR - K. ENG 10-27-08	REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT			





- \* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
  - \*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
- SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
  - ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
  - ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
  - ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
  - ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
  - ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
  - ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

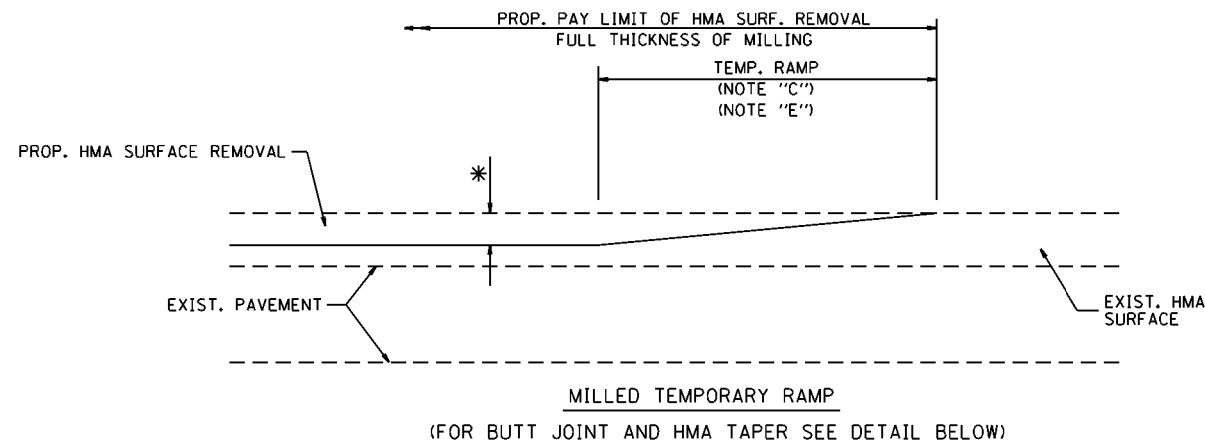
- PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)
- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

**BASIS OF PAYMENT:**  
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

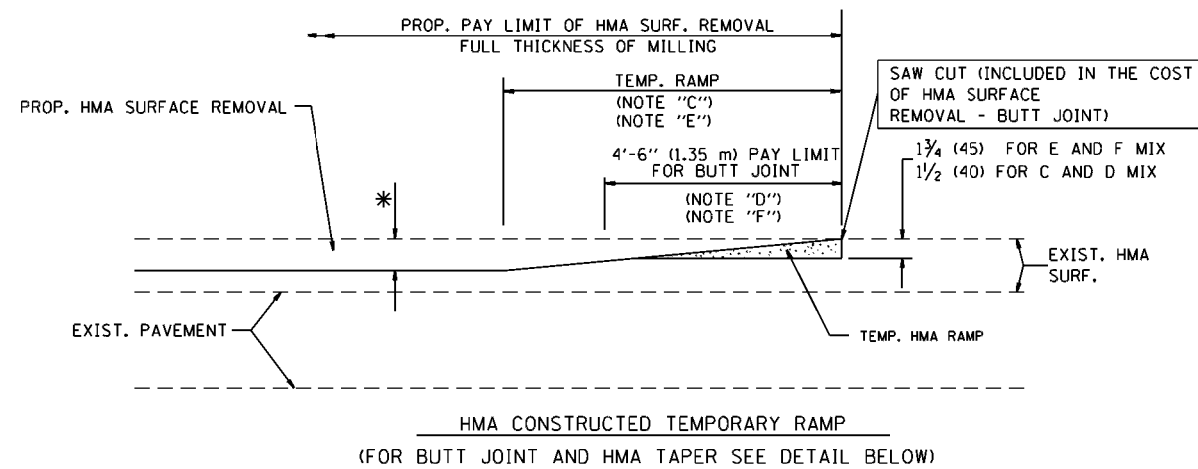
## CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = gulloumefp	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>	F.A.R. RTE. 305	SECTION 2018-09B-RS&SW	COUNTY COOK	TOTAL SHEETS 35	SHEET NO. 25	
			REVISED - A. ABBAS 03-21-97					BD600-06 (BD-24)		CONTRACT NO. 62H47	
			REVISED - M. GOMEZ 01-22-01					FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT			
			REVISED - R. BORO 12-15-09					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

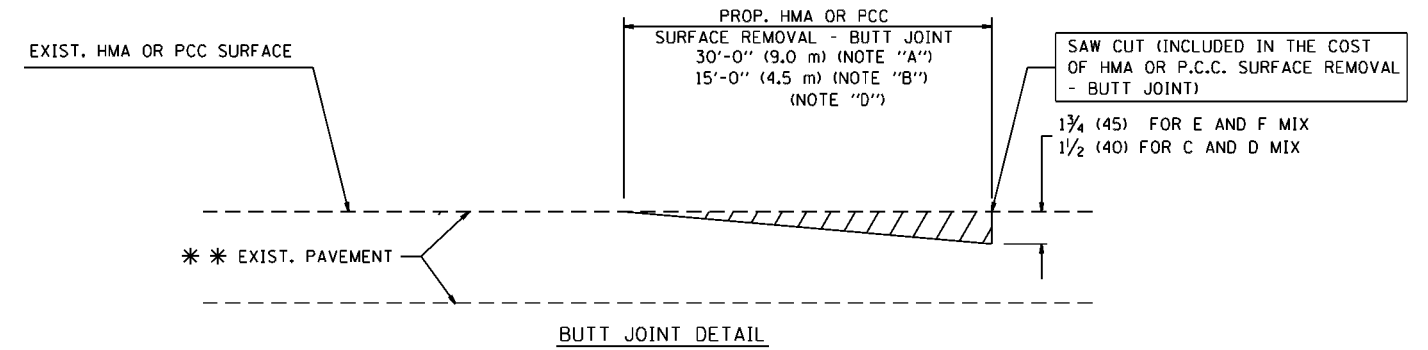


**OPTION 1**

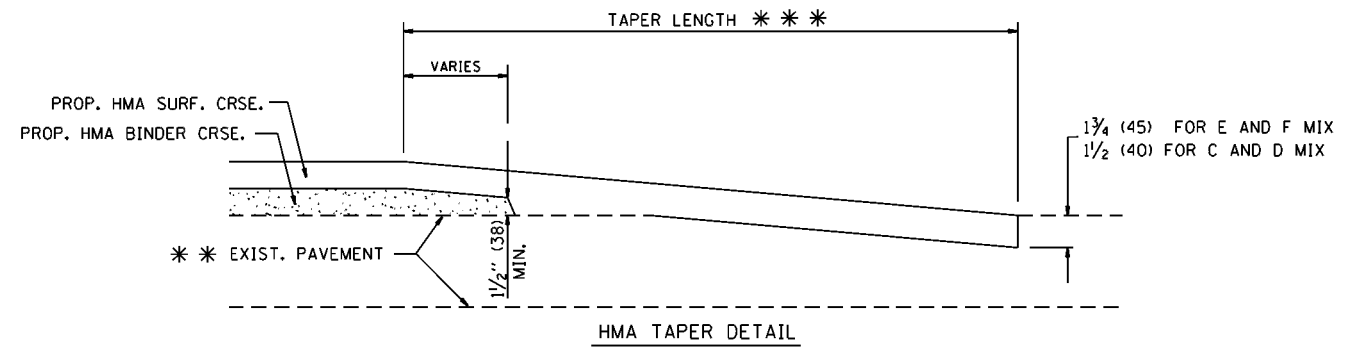


**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

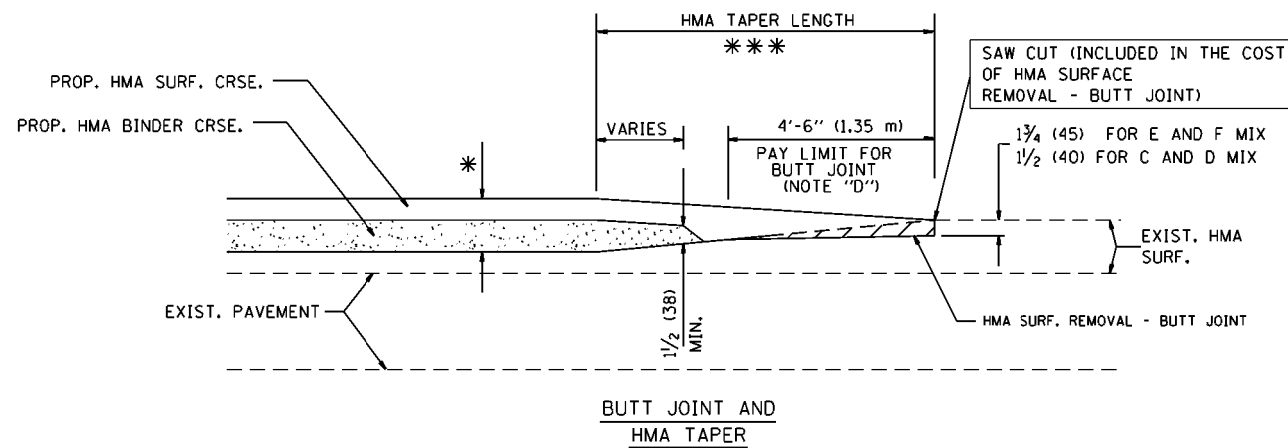
**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



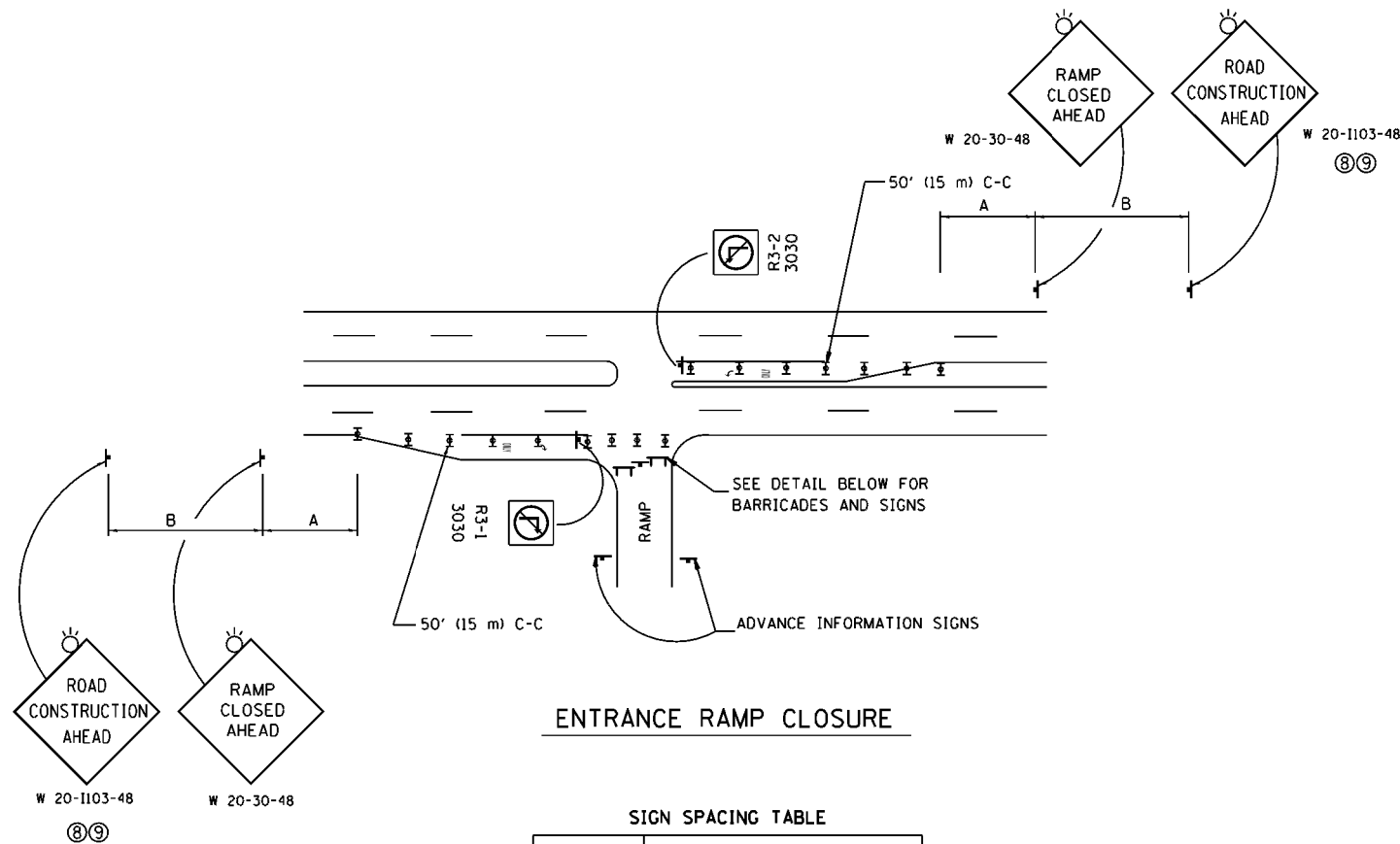
**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

FILE NAME =	USER NAME = gulloumf	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
p:\IL\084EBID\INTEG\illinois.gov\FWIDOT\Documents\DOT Offices\District 1\Projects\1082\Draw\Detail\Design\DistStd.dgn			REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 10/5/2018	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>BUTT JOINT AND HMA TAPER DETAILS</b>	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2018-09B-RS&SW	COOK	35	26
BD400-05 BD32		CONTRACT NO. 62H47		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

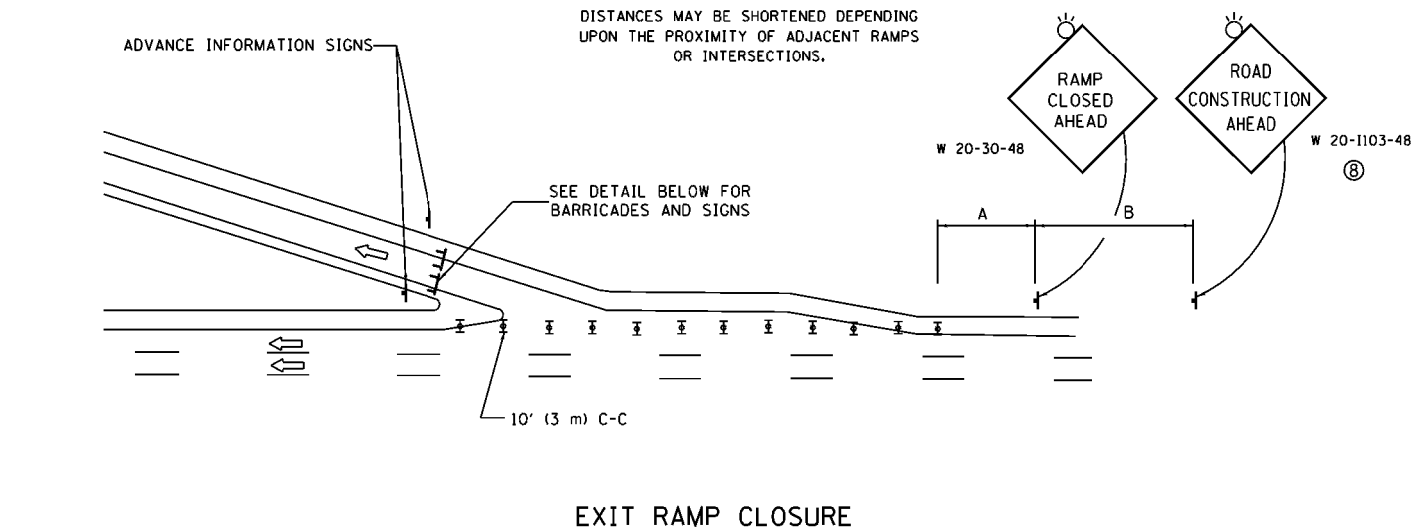


ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY <24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

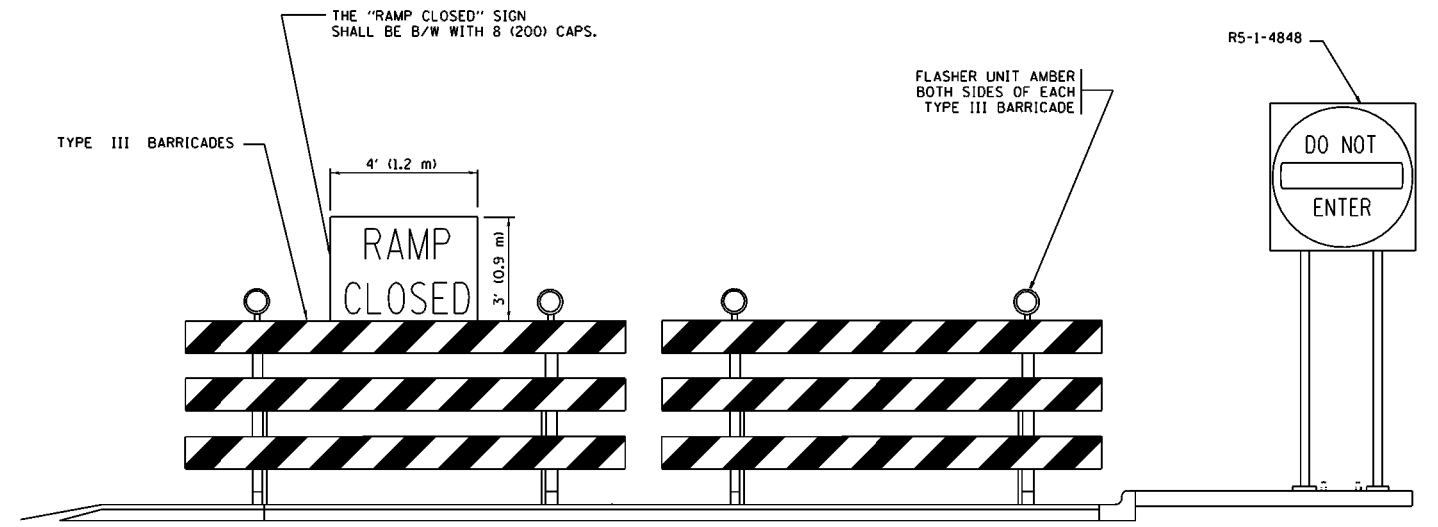
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



EXIT RAMP CLOSURE

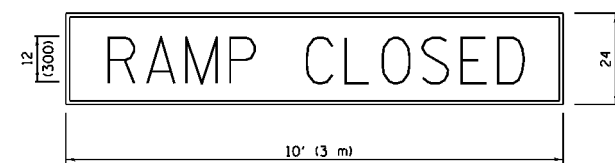
**SYMBOLS**

- ☐ TYPE II BARRICADE OR DRUM
- ☐ TYPE III BARRICADE WITH 2 FLASHING LIGHTS



DETAIL FOR REQUIRED BARRICADES & SIGNS

RAMP CLOSURE ADVANCE WARNING SIGN

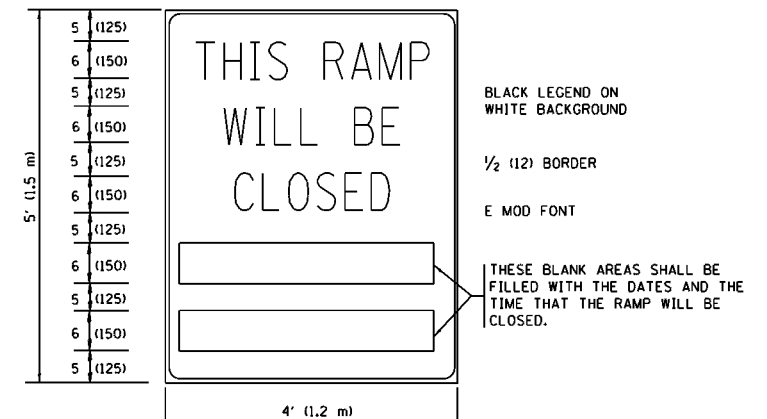


BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY E MOD FONT 1 (25) BORDER  
 THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.

THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

RAMP CLOSURE ADVANCE INFORMATION SIGN



BLACK LEGEND ON WHITE BACKGROUND

1/2 (12) BORDER

E MOD FONT

THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

**GENERAL NOTES:**

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② VERTICAL BARRICADES SHALL NOT BE USED FOR RAMP CLOSURES.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

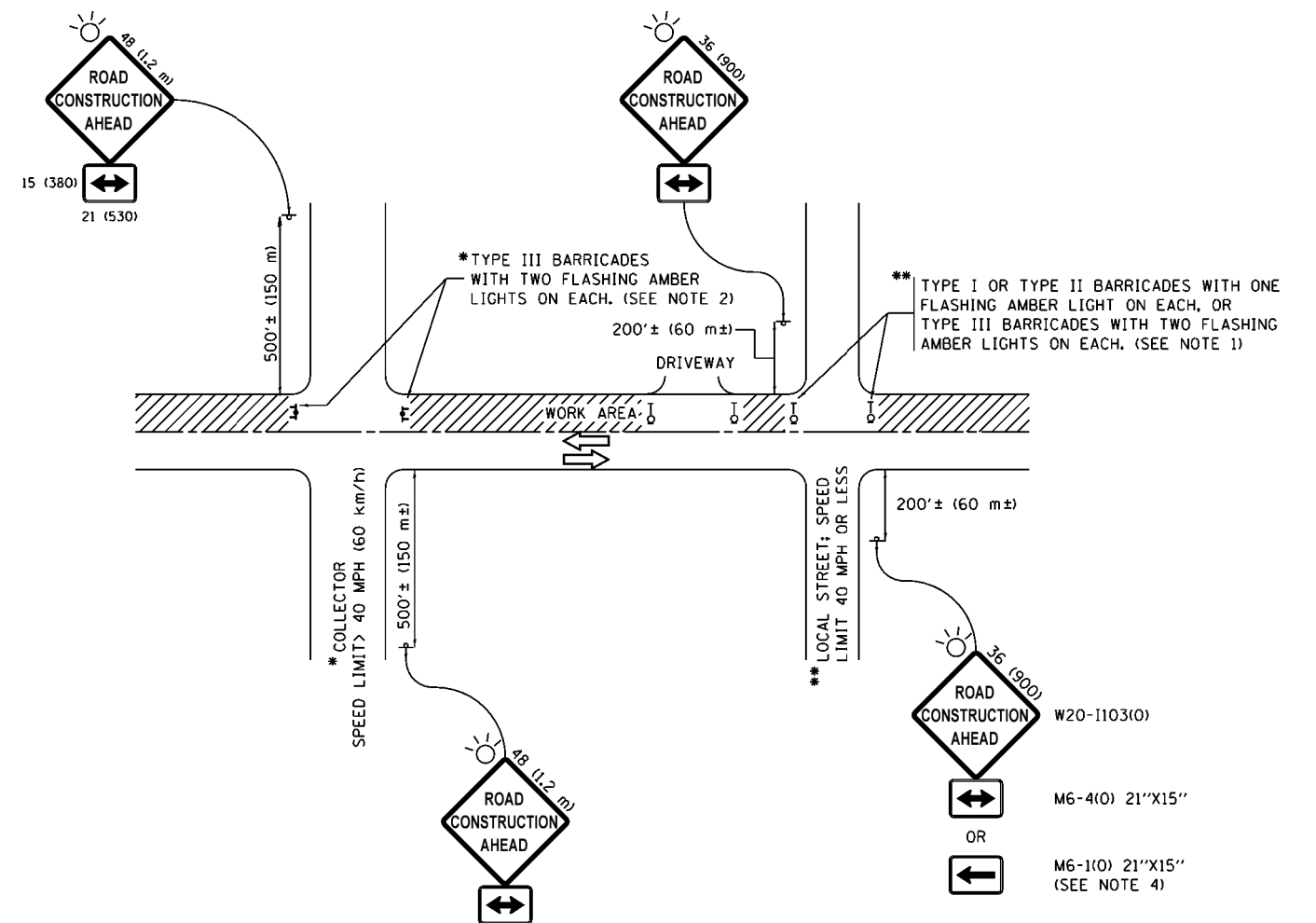
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Default	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - M.D. 06-13
	PLOT DATE = 10/5/2018	DATE - 02-83	REVISED - M.D. 01-18

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ENTRANCE AND EXIT RAMP  
CLOSURE DETAILS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2018-098-RS&SW	COOK	35	27
TC-08			CONTRACT NO. 62H47	
ILLINOIS FED. AID PROJECT				



**NOTES:**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

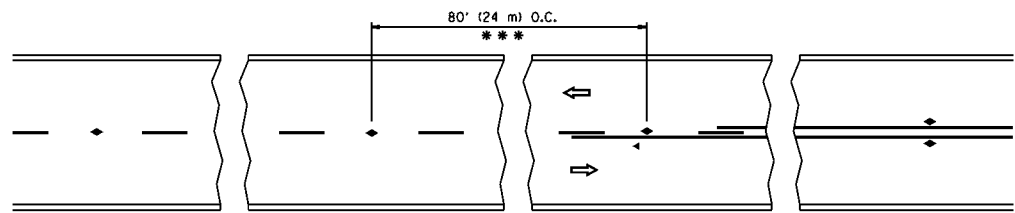
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Default	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 10/5/2018	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

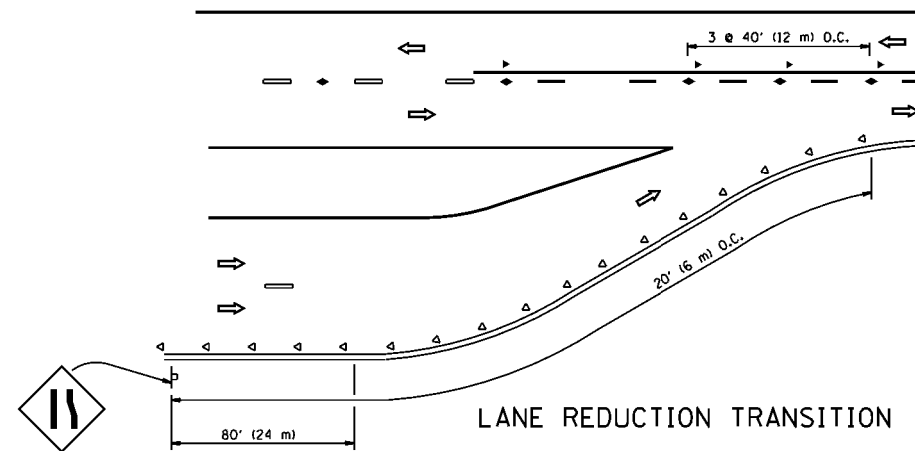
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO. 62H47	
ILLINOIS FED. AID PROJECT				

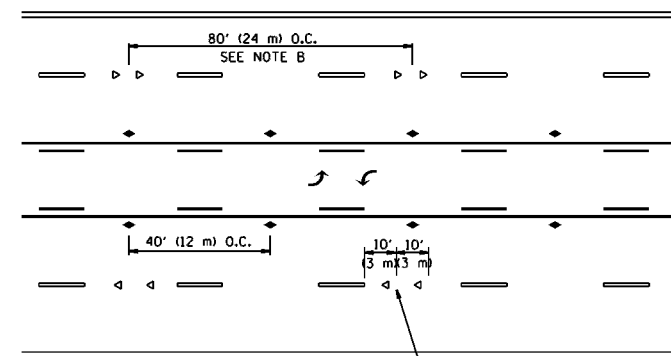


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

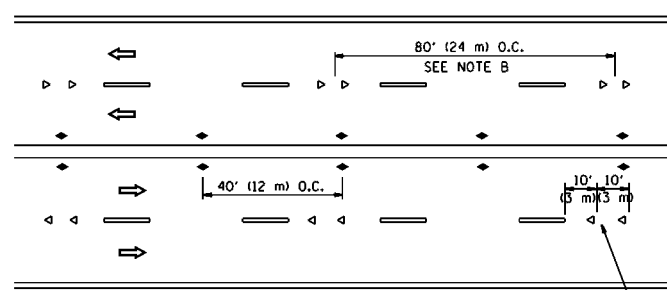
TWO-LANE/TWO-WAY



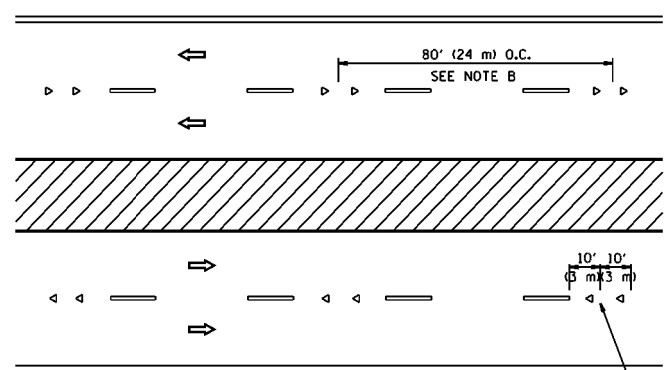
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

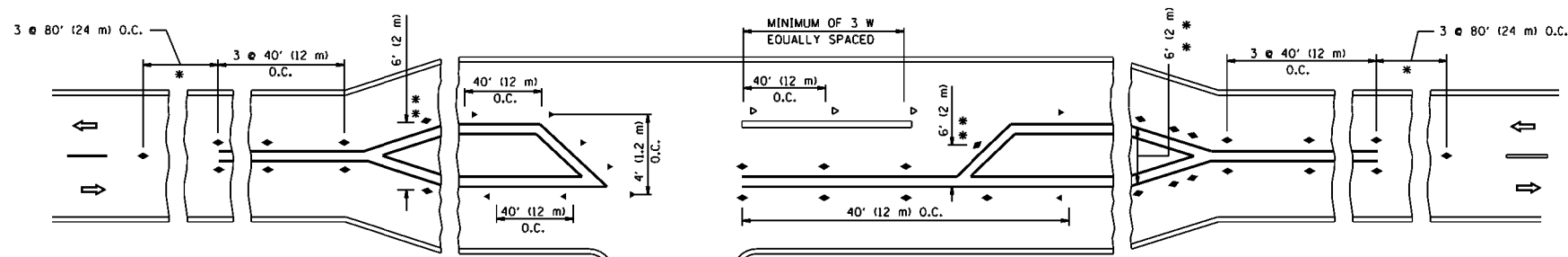
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

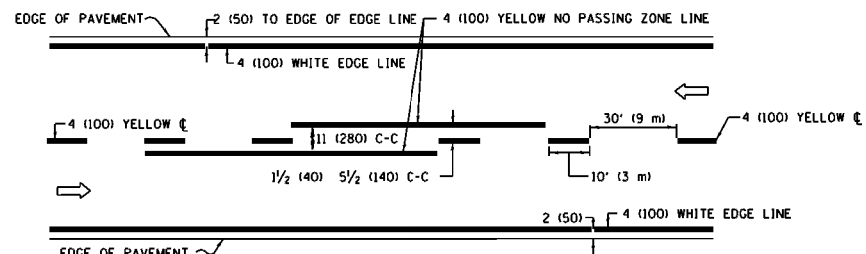


\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

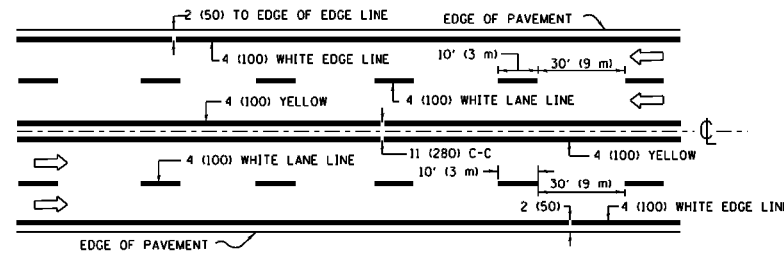
LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

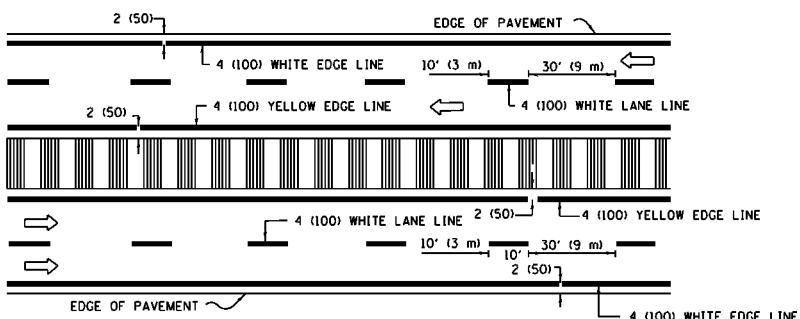
FILE NAME = p:\11\084EBID\INTEG\illinois.gov\FWIDOT\Documents\DOT Offices\District 1\Projects\DI022\Drawings\Design\DistStd.dgn	USER NAME = guillaumejp	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS			F.A.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - T. RAMMACHER 03-12-99		RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)	305	2018-098-RS&SW	COOK	35	29		
PLOT DATE = 10/5/2018	DATE -	REVISED - T. RAMMACHER 01-06-00	REVISED - C. JUCCIUS 09-09-09	SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	TC-11		CONTRACT NO. 62H47			
							FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT			



**2-LANE ROADWAY**

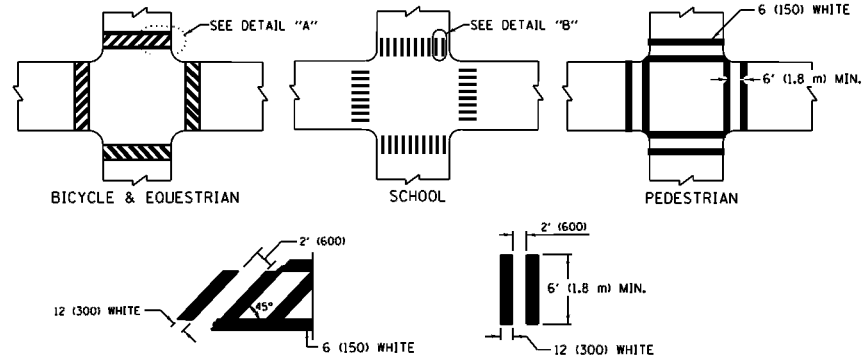


**MULTI-LANE UNDIVIDED**



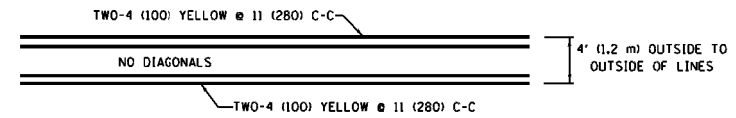
**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

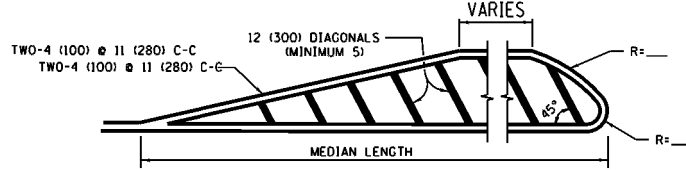


**TYPICAL CROSSWALK MARKING**

\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

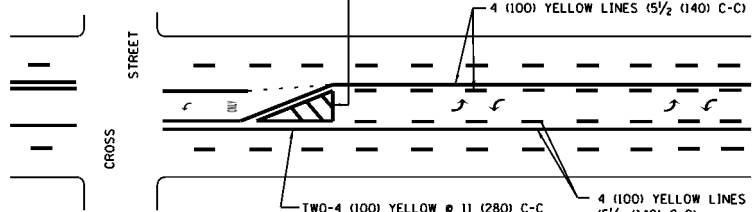


**4' (1.2 m) WIDE MEDIANS ONLY**



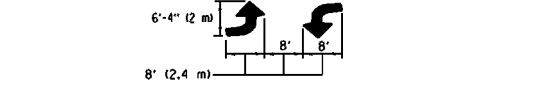
**MEDIANS OVER 4' (1.2 m) WIDE**

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



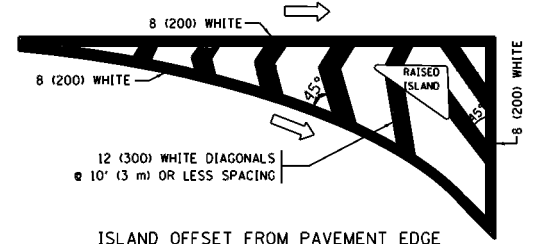
**MEDIAN WITH TWO-WAY LEFT TURN LANE**  
**TYPICAL PAINTED MEDIAN MARKING**

A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

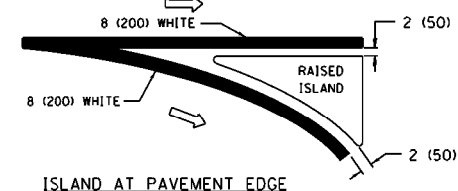


**TYPICAL LEFT (OR RIGHT) TURN LANE**  
**TYPICAL TURN LANE MARKING**

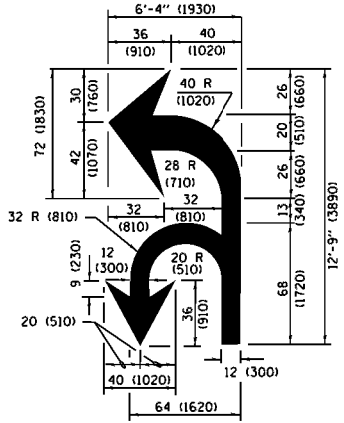
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".



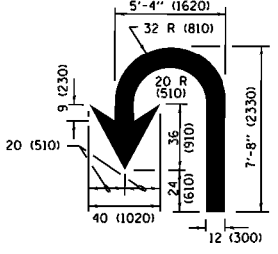
**ISLAND OFFSET FROM PAVEMENT EDGE**



**ISLAND AT PAVEMENT EDGE**  
**TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**U-TURN**

**LANE REDUCTION TRANSITION**

\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = gulloumefp	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
p:\1\084EBID\INTEG\illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\DI152\DRW\Design\DistStd.dgn		CHECKED -	REVISED - C. JUCIUS 07-01-13
Default	PLOT SCALE = 100.0000' / 1in.	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 10/5/2018		REVISED - C. JUCIUS 04-12-16

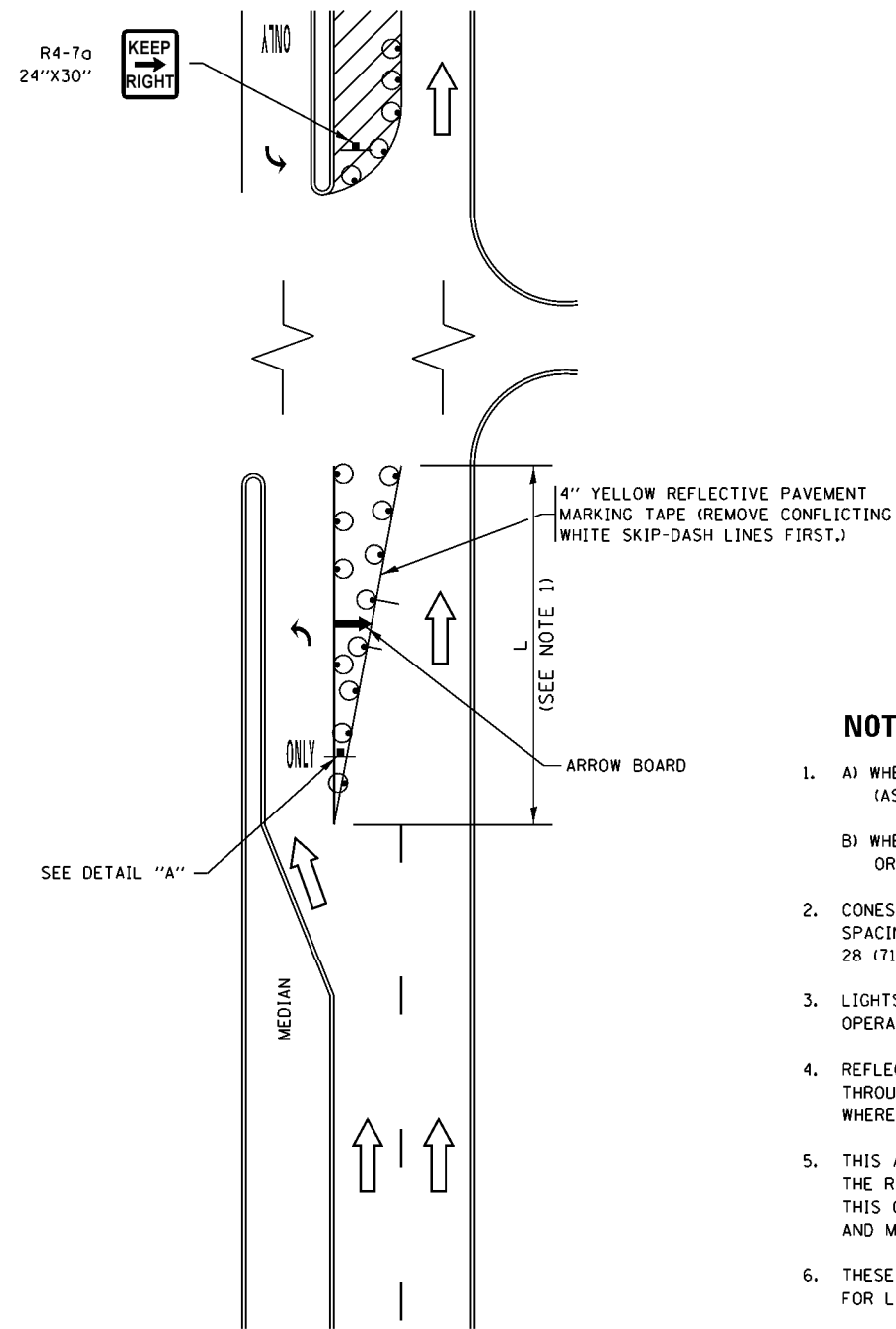
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

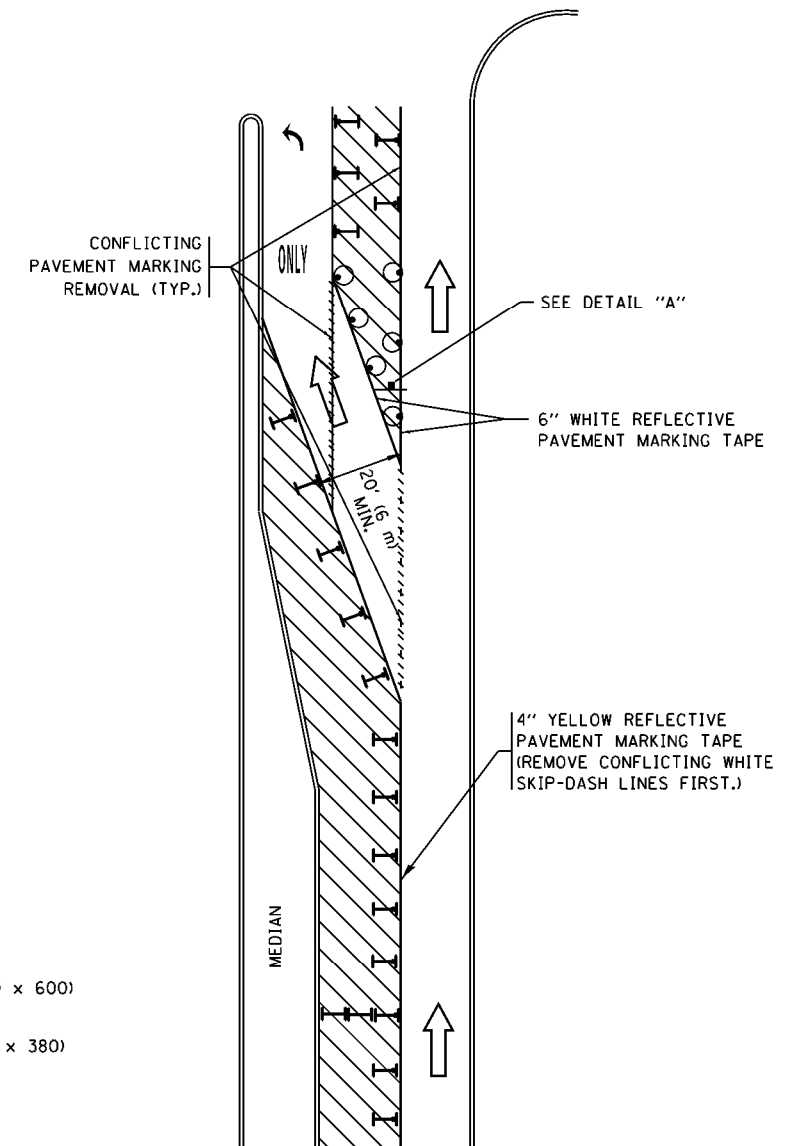
F.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2018-098-RS&SW	COOK	35	30
TC-13		CONTRACT NO. 62H47		
ILLINOIS FED. AID PROJECT				

# TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER



**FIGURE 1**

# TURN BAY ENTRANCE WITHIN A LANE CLOSURE



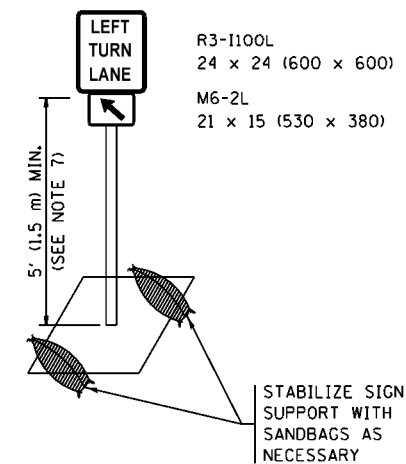
**FIGURE 2**

### LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

### NOTES:

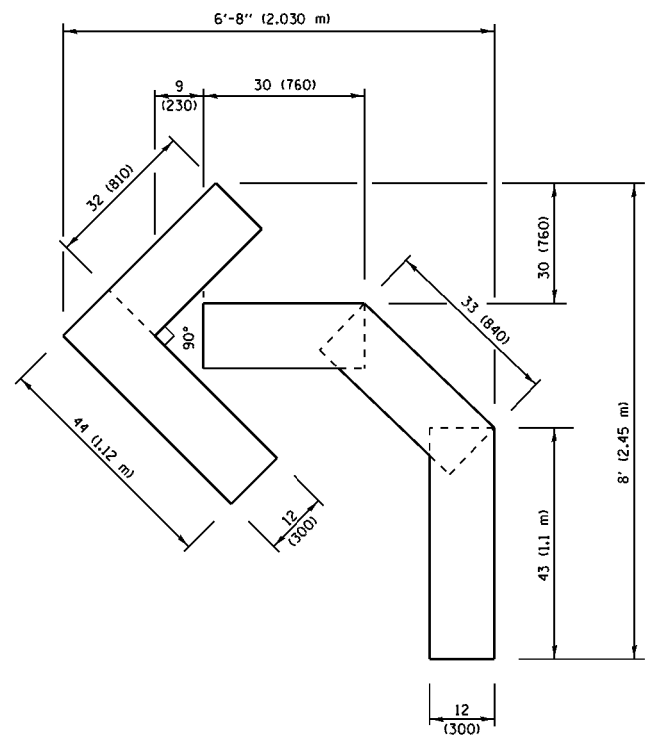
1. A) WHEN "L" IS  $\leq$  THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.  
B) WHEN "L" IS  $>$  THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



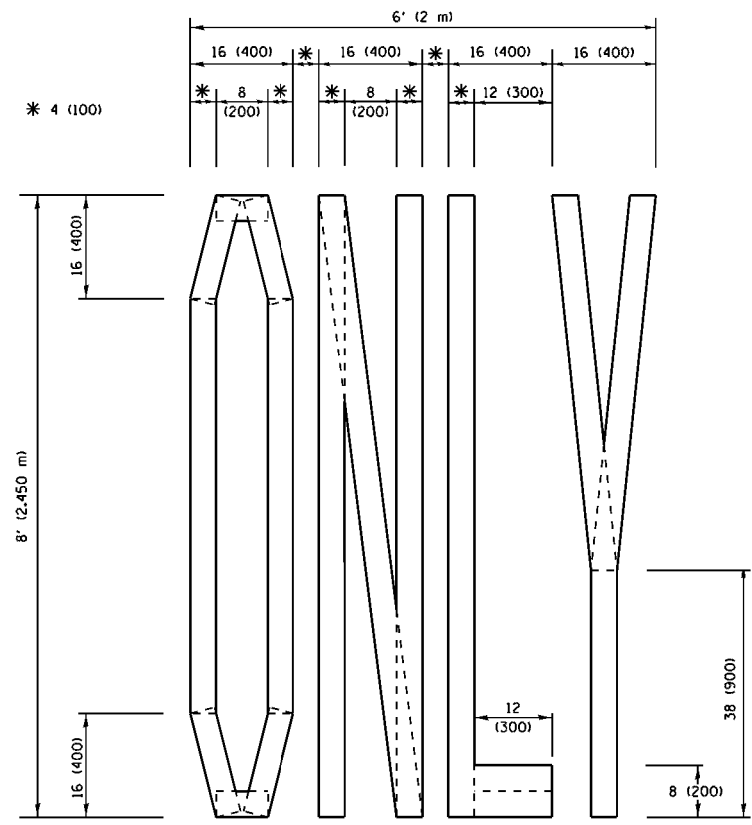
**DETAIL A**

All dimensions are in inches (millimeters) unless otherwise shown.

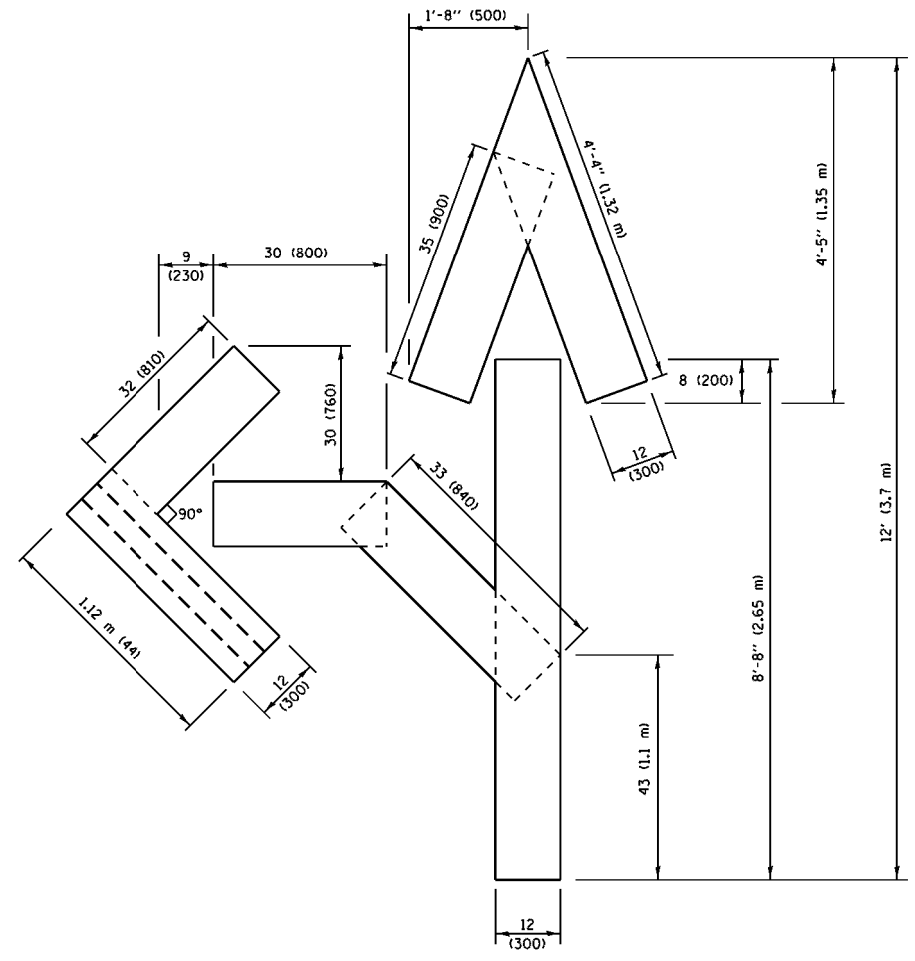
FILE NAME =	USER NAME = gulloumefp	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	Default	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16					305	2018-098-RS&SW	COOK	35	31
		REVISED - T. RAMMACHER 01-06-00	REVISED -		TC-14			CONTRACT NO. 62H47				
					ILLINOIS FED. AID PROJECT							



**QUANTITY**  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.41 sq. m)

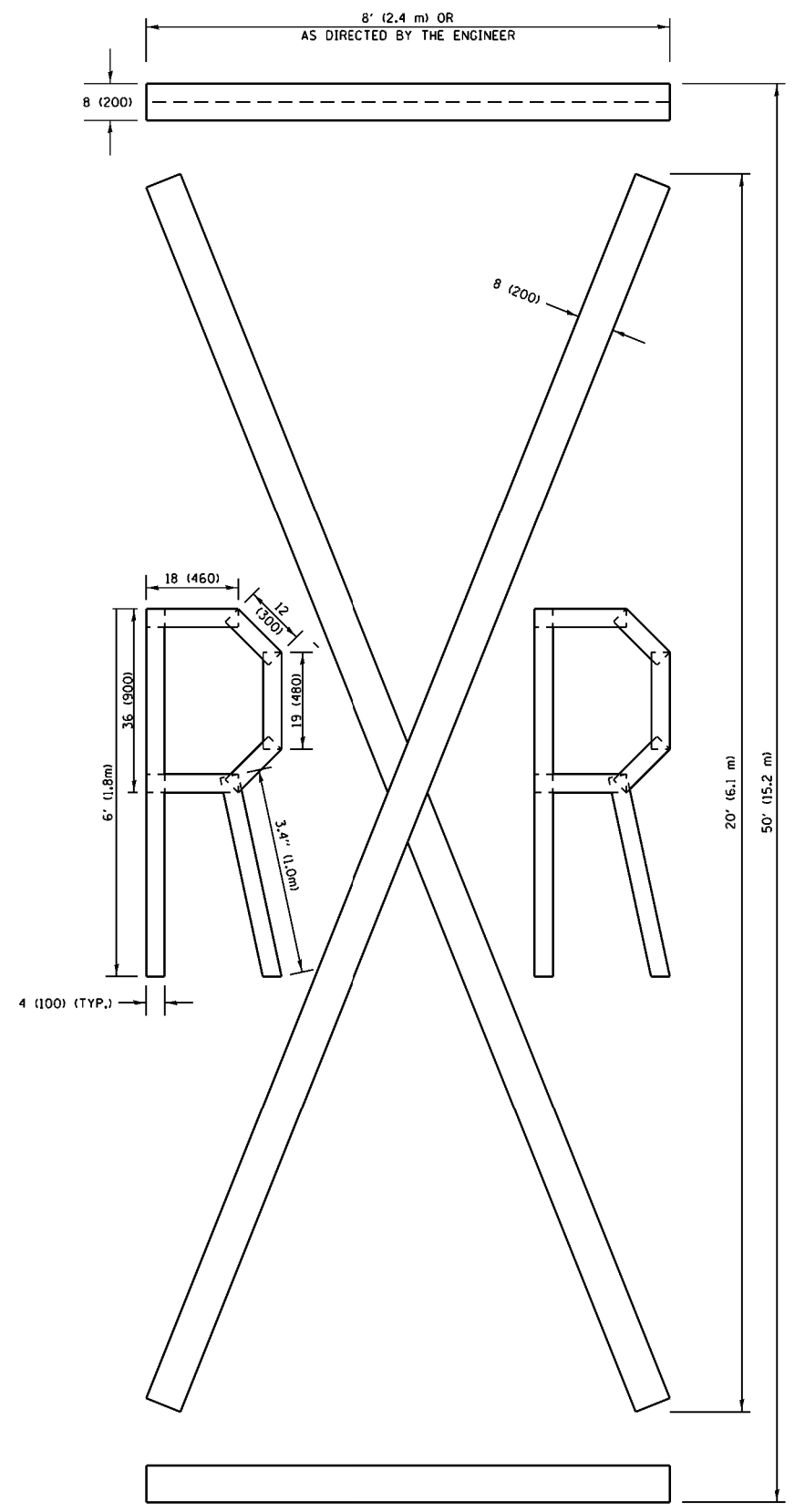


**QUANTITY**  
 4 (100) LINE = 64.1 ft. (19.5 m)  
 21.4 sq. ft. (1.99 sq. m)



**QUANTITY**  
 4 (100) LINE = 82.5 ft. (25.1 m)  
 27.5 sq. ft. (2.53 sq. m)

**NOTE:**  
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**  
 4 (100) LINE = 225.9 ft. (68.9 m)  
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = gulloumefp	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
p:\11084EBID\INTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\Dist 1\1022\Drawings\Design\DistStd.dgn		CHECKED -	REVISED - E. GOMEZ 08-28-00
		DATE -	REVISED - E. GOMEZ 08-28-00
			REVISED - A. SCHUETZE 09-15-16

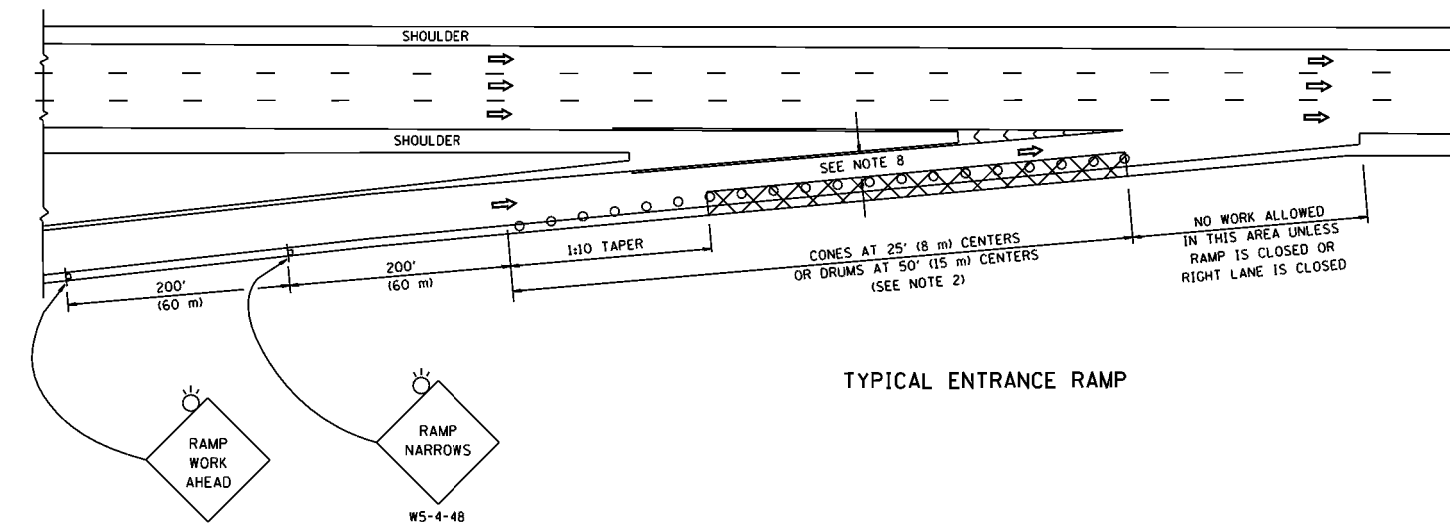
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

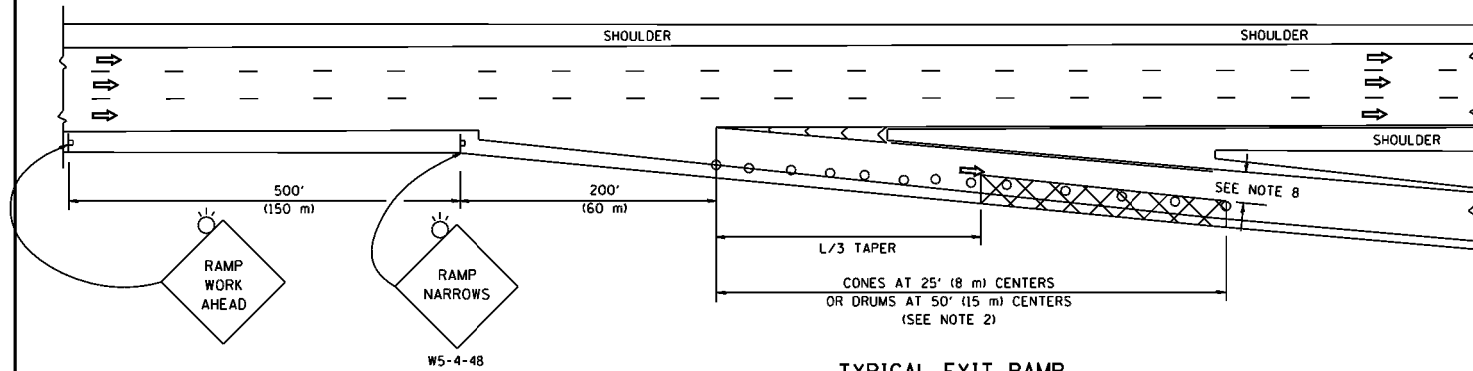
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2018-098-RS&SW	COOK	35	32
TC-16		CONTRACT NO. 62H47		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



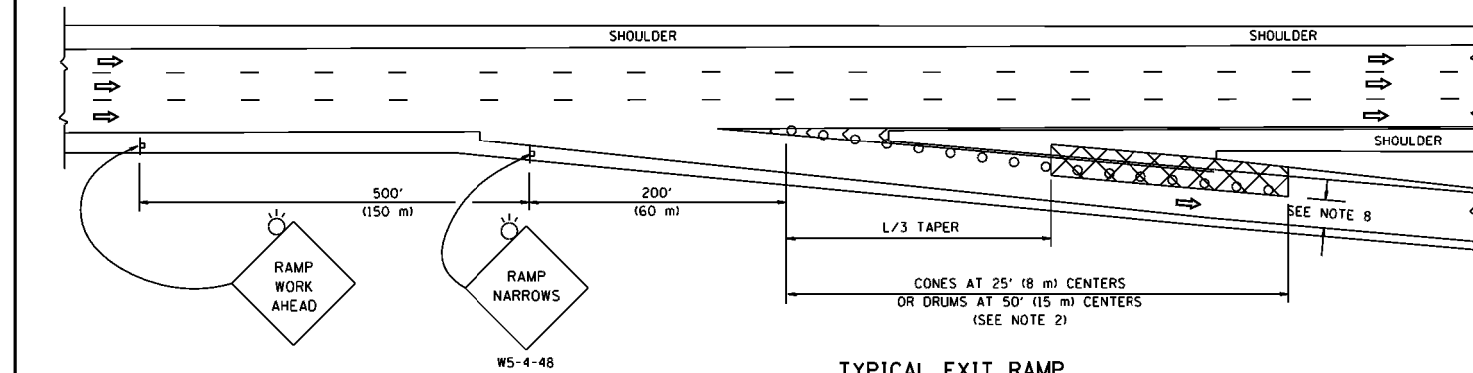
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

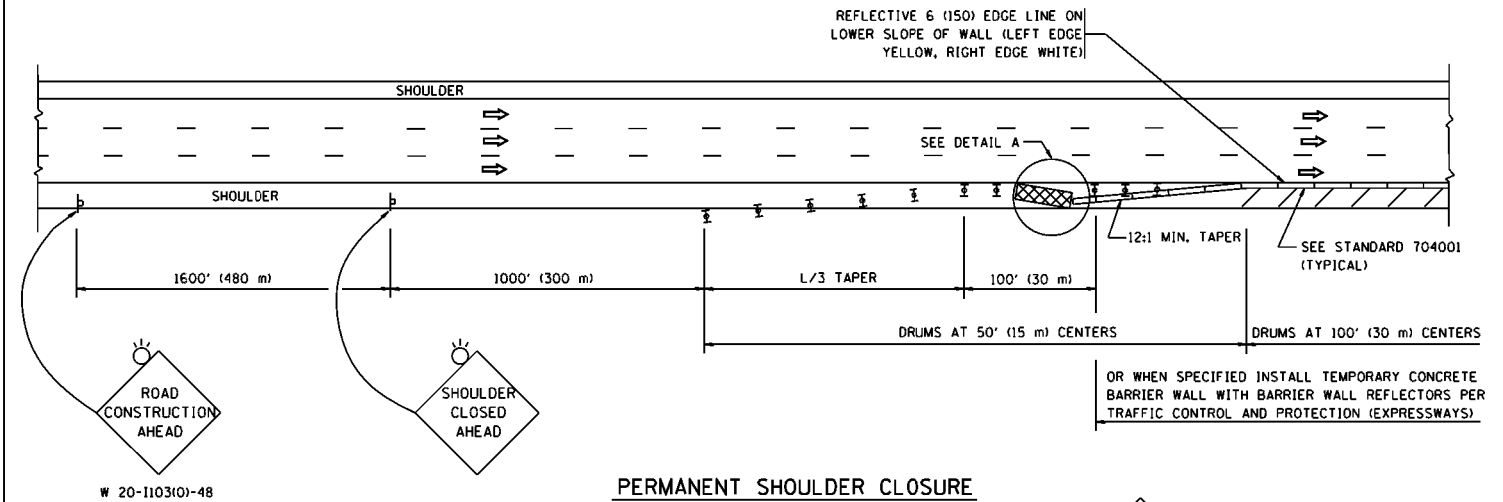
GENERAL NOTES

1. THE "L" DISTANCE EQUALS:
 

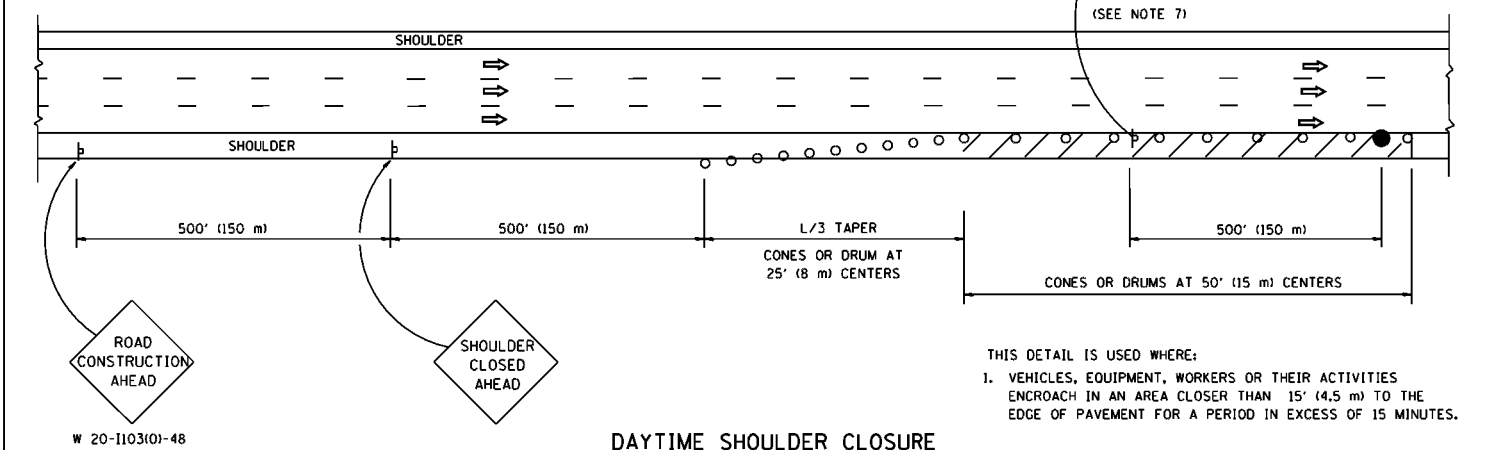
SPEED LIMIT	FORMULAS
45 mph (80 km/h)	METRIC ENGLISH
OR GREATER:	L=0.65(WXS) L=(WX5)

W = WIDTH OF OFFSET IN FEET (METERS)  
S = NORMAL POSTED SPEED MPH (KM/H)
2. TYPE II BARRICADES OR DRUMS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES. TYPE II BARRICADES OR DRUMS WITH MONODIRECTIONAL STEADY BURN LIGHTS ARE REQUIRED FOR DELINEATING OBSTACLES, EXCAVATIONS, OR HAZARDS EXCEEDING 100 FT (30m) IN LENGTH AT NIGHT.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

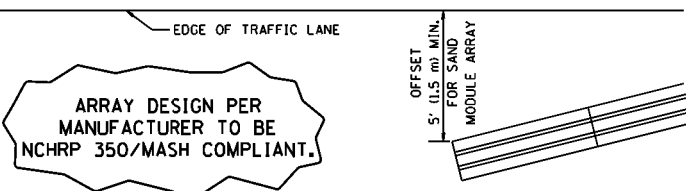
SHOULDER CLOSURE DETAILS



PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

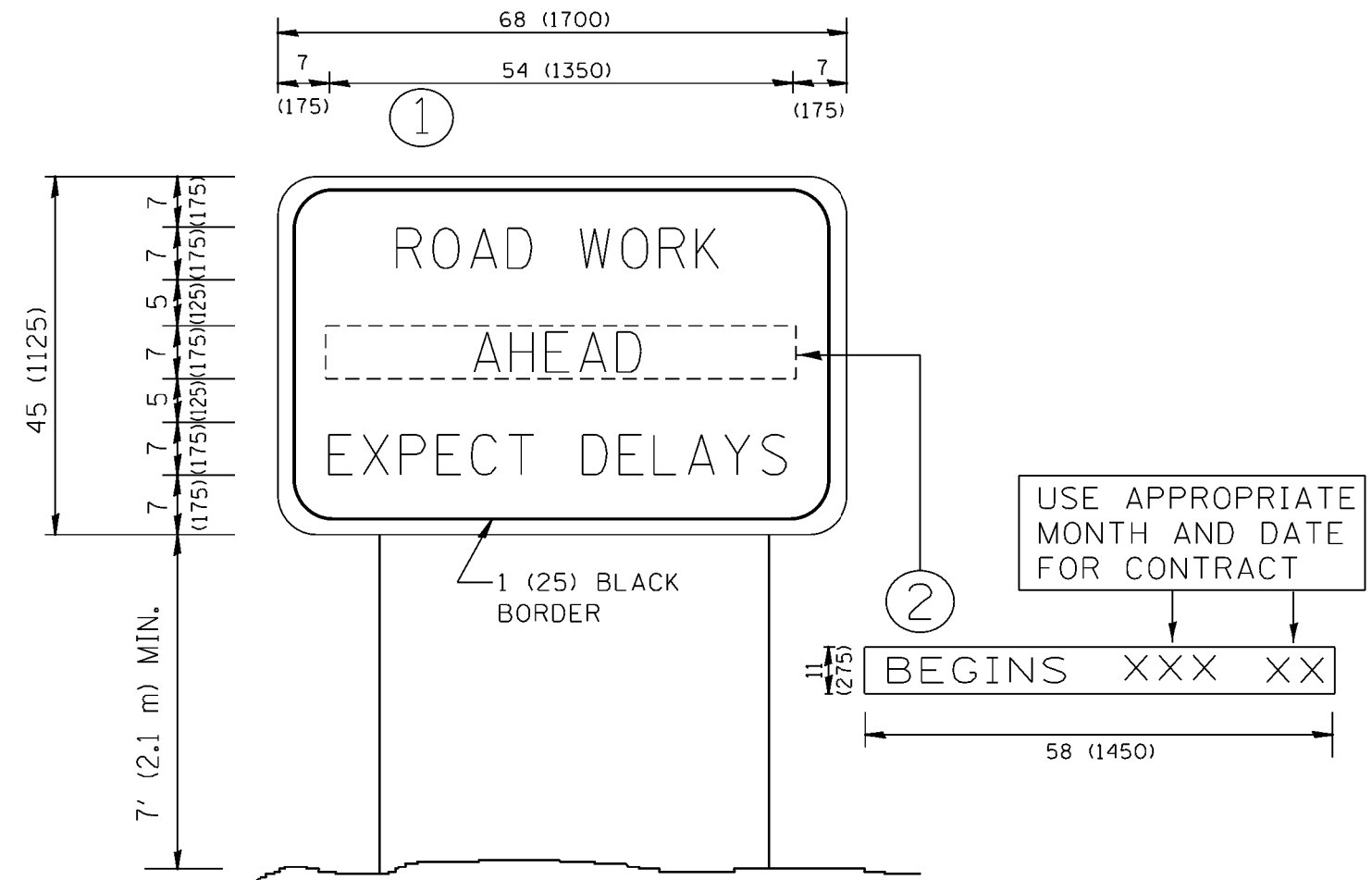


DETAIL "A"  
IMPACT ATTENUATOR, TEMPORARY  
(SEE NOTE 5)

5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
  - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
  - b. THE WORK ACTIVITY REQUIRES FREQUENT ENCROACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
8. 12' MIN. WIDTH TANGENT SECTION  
16' MIN. WIDTH CURVE SECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = gulloumefp	DESIGNED -	REVISED - S.P.B. 01-07	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
			REVISED - S.P.B. 12-09			305	2018-098-RS&SW	COOK	35	33	
			REVISED - M.D. 06-13			TC-17		CONTRACT NO. 62H47			
			REVISED - M.D. 01-18			SCALE: NONE		SHEET 1 OF 1 SHEETS		STA. TO STA.	



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = gulloumefp	DESIGNED -	REVISED - R. MIRS 09-15-97
p:\11\084EBID\INTEG\illinois.gov\FWIDOT\Documents\IDOT Offices\District 1\Projects\DI02\DRW\Info\Design\DistStd.dgn			REVISED - R. MIRS 12-11-97
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 10/5/2018	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

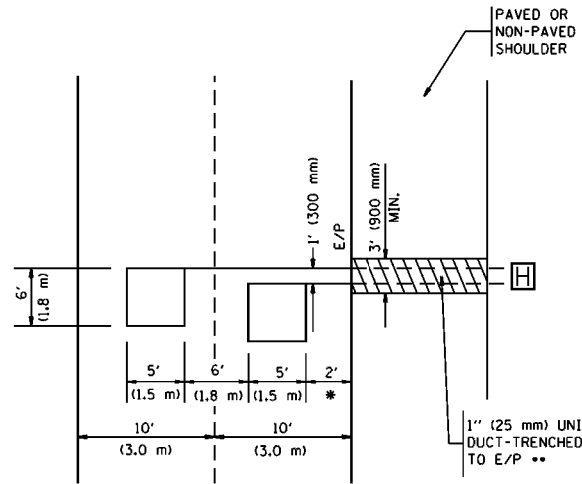
**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2018-098-RS&SW	COOK	35	34
TC-22		CONTRACT NO. 62H47		
FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT				

**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



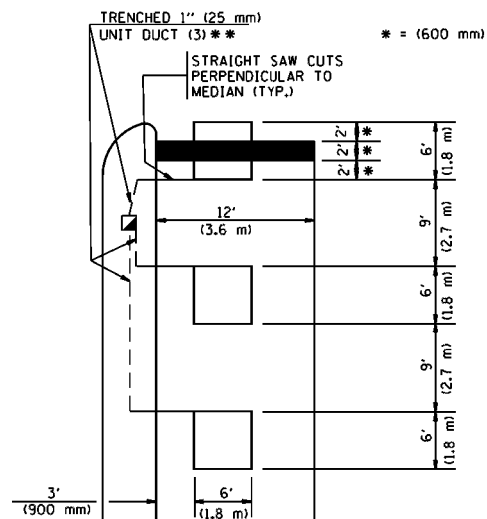
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

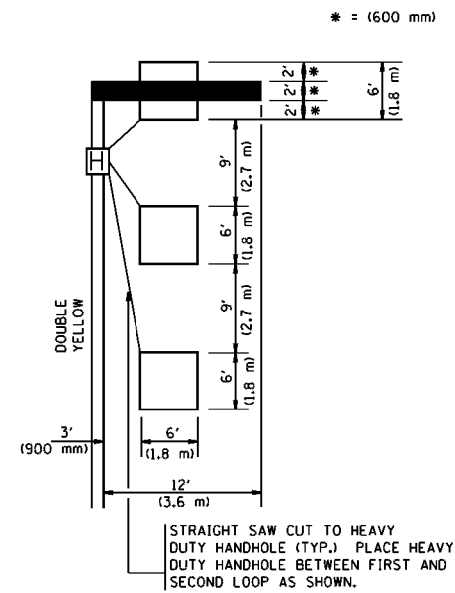


\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)



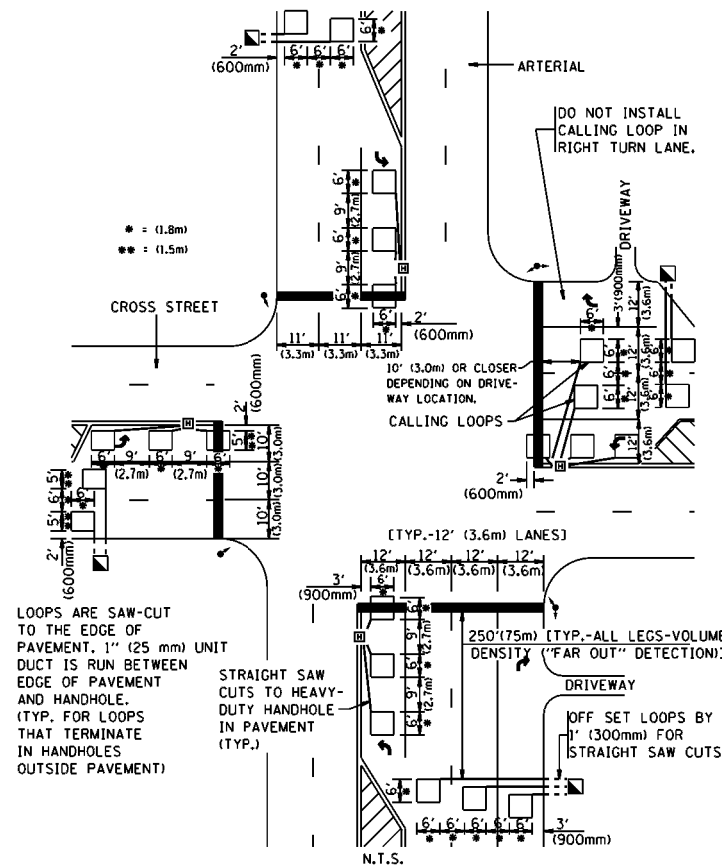
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**NOTES:**

**VEHICLES LOOP DETECTORS**

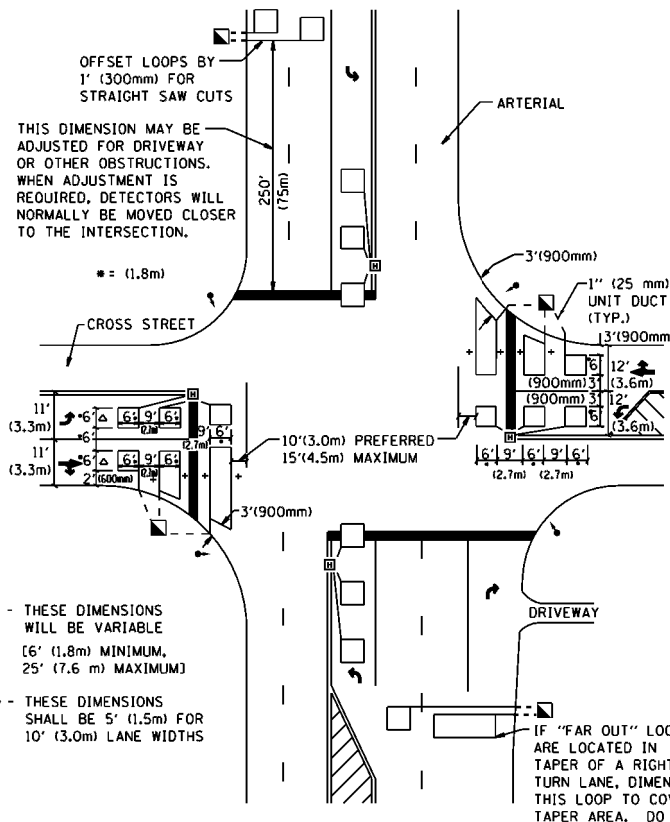
- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1**  
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2**  
N.T.S.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

**NOTE:**

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = gulloumefp	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING</b>		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBID\INTEG\illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\1082\DRM\01\Design\DistStd.dgn		CHECKED - R.K.F.	REVISED -				305	2018-09B-RS&SW	COOK	35	35
PLOT SCALE = 100.0000' / 1in.		DATE -	REVISED -				TS-07		CONTRACT NO. 62H47		
PLOT DATE = 10/5/2018							SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	