

FAI R/T	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1;(2HVB)BRR]	WOODFORD	71*	1
	NUMER	CONTRACT NO.	68D79	

*71 + 1 = 72 TOTAL SHEETS
D-94-046-17

FOR INDEX OF SHEETS SEE SHEET NO. 2

STATE OF ILLINOIS 01-18-2019 LETTING ITEM 101

DEPARTMENT OF TRANSPORTATION

HIGHWAY STANDARDS

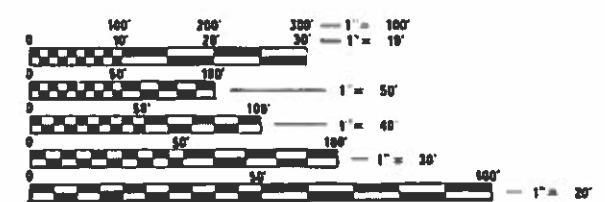
000001-07	542401-03	701106-02	701901-08
001001-02	606001-07	701201-05	780001-05
001006	630001-12	701400-09	781001-04
280001-07	631011-10	701401-12	
420001-09	631031-15	701402-12	
420701-03	642001-02	701406-12	
442001-04	665001-02	701411-09	
442101-09	701001-02	701426-09	
442201-03	701006-05	701451-05	
482001-02	701101-05	701456-05	

DISTRICT STANDARDS

630101-D4
635002-D4
780001-D4

DESIGN DESIGNATION

CLASS: INTERSTATE
ADT: 18,500 (2017)
HCV: 5,850 (31.62%)
SU: 649 (3.51%)
MU: 5,200 (28.11%)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

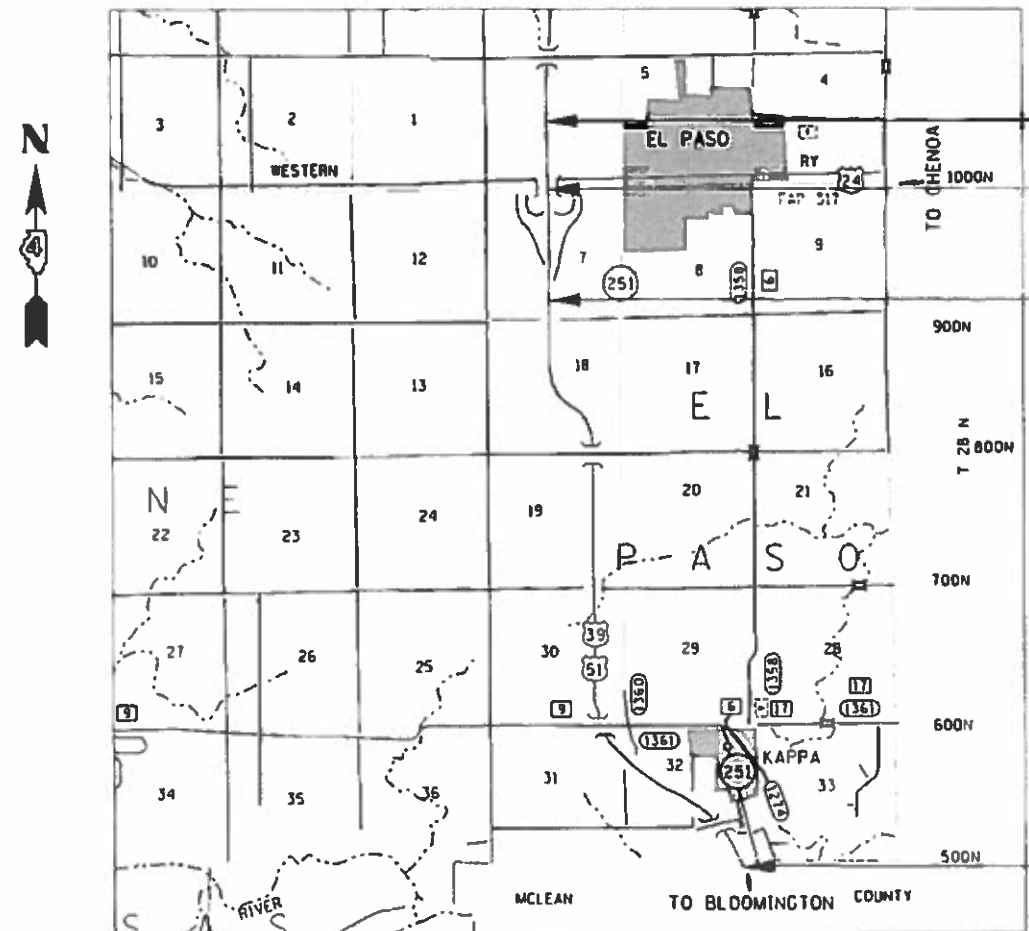
PROJECT ENGINEER: RICH DOTSON (309) 671-3455
PROJECT MANAGER: RAY SAMARA (309) 671-3460
SQUAD LEADER: MIKE JACOBS (309) 671-3460

CATALOG NO. 035503-00D
CONTRACT NO. 68D79

PROPOSED HIGHWAY PLANS

FAI ROUTE 39 (I-39)
SECTION 102[1RS,2RS-1;(2HVB)BRR]
PROJECT NHPP-YP1W(899)
RESURFACING
WOODFORD COUNTY

C-94-080-17



PROJECT ENDS:
NB /SB STA. 461+00
I-39 OVER US 24:
NB SN 102-0059
SB SN 102-0058
STA. 448+23 TO 452+71
STATION EQUATION:
STA. 821+27 BK.
STA. 417+00 AH.

PROJECT BEGINS:
NB STA. 555+26
SB STA. 555+56

GROSS LENGTH = 31,001 FT. = 5.871 MILE
NET LENGTH = 31,001 FT. = 5.871 MILE

PROJECT DESCRIPTION
RESURFACING I-39 FROM THE MCLEAN COUNTY LINE TO 0.25 MILES NORTH OF US 24, INCLUDES BRIDGE REPAIR OF SN 102-8058 AND SN 102-0059 CARRYING I-39 OVER US 24 /TP&W RR AT THE EL PASO INTERCHANGE.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
SUBMITTED October 10, 2018
Ray A. Samara REGIONAL ENGINEER
Dec 7 2018
[Signature] ENGINEER OF DESIGN AND ENVIRONMENT
Dec 7 2018
[Signature] DIRECTOR OF HIGHWAYS PROJECT ADMINISTRATION

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

INDEX OF SHEETS

1 COVER SHEET
 2-3 INDEX OF SHEETS, COMMITMENTS, GENERAL NOTES, AND JOB SPECIFIC NOTES
 4-13 SUMMARY OF QUANTITIES
 14-17 PROPOSED TYPICAL SECTIONS
 18 LINE DIAGRAM
 19 CURVE DATA
 20-31 SCHEDULE OF QUANTITIES
 32-33 PAVING SEQUENCE
 34-38 BUTT JOINT DETAILS
 39 PAVEMENT OVERLAY UNDER STRUCTURES DETAIL
 40 CLASS D PATCHES (SPECIAL) DETAIL
 41 JOINT FILLING DETAIL
 42 TRAFFIC COUNTER DETAIL
 43-44 PAVEMENT MARKING DETAILS
 45-56A BRIDGE REPAIR PLANS
 57-66 BRIDGE TRAFFIC CONTROL STAGING /TRAFFIC MANAGEMENT PLAN
 67-71 DISTRICT STANDARDS

COMMITMENTS

Commitments are not to be altered without the written approval of all parties to which the commitment was made.
NO COMMITMENTS HAVE BEEN MADE FOR THIS PROJECT.

BRIDGE OVERLAY NOTIFICATION

After placement of the bridge deck overlay, the Resident Engineer shall notify the District Bridge Maintenance Engineer of the "as constructed" milling depth and overlay thickness for updating the Illinois Highway Information System.

MINIMUM VERTICAL CLEARANCE

The Contractor shall verify the existing structure vertical clearance prior to placing bituminous overlay under any structure. A minimum (16') vertical clearance shall be maintained under all structures.

BUTT JOINT CUTTING TIME RESTRICTION

Butt joints shall not be milled more than three (3) days prior to placement of the HMA surface course.

ENVIRONMENTAL REVIEWS

Prior to the use of any proposed borrow areas, use areas (temporary access roads, detours, run-arounds, etc.) and/or waste areas, the Contractor shall file the required environmental resource request surveys according to Section 107.22 of the Standard Specifications. These surveys are required in order for the Department to conduct cultural and biological resource surveys for the proposed site.

Prior to any waste materials being removed from the construction site the required environmental resource surveys will need to be obtained and filed by the Contractor. Excess waste products removed from the construction site shall be disposed of as required in Section 202.03 of the Standard Specifications.

Any protruding metal bars shall be removed prior to the disposal of broken concrete at approved disposal sites.

The required environmental resource documentation shall include the following:

- * BDE Form 2289 (Environmental Survey Request)
- * BDE Form 2290 (Waste/Use Area Review)
- * A location map showing the size limits and location of the use area
- * Color photographs depicting the use area
- * Borrow Area Entry Agreement form-D4 PI0101

Please note that a minimum of four weeks shall be allowed for the District to obtain the required environmental clearances and six weeks for the required borrow site environmental clearances.

PAVEMENT STATIONING NUMBERS & PLACEMENT

The Contractor shall provide labor and materials required to imprint pavement station numbers in the finished surface of the pavement and/or overlay. The numbers shall be approximately 3/4 inch (20mm) wide, 5 inches (125 mm) high and 5/8 inch (15 mm) deep.

The pavement station numbers shall be installed as specified herein:

Interval – 200 feet (English stationing) or 100 meters (metric stationing)

Bottom of Numbers – 6 inches (150 mm) from the inside edge of the pavement marking

Location:

- * 2,3, & 5 Lane Pavements – right edge of pavement in direction of increasing stations
- * Multi-Lane Divided Roadways – outside edge of pavement in both directions
- * Ramps – along baseline edge of pavement

Position – stations shall be placed so they can be read from the adjacent shoulder

Format – English (Metric) pavement stations shall use this format "XXX (XX+X00)" where X represents the pavement station

This work will not be paid for separately, but will be considered included in the cost of the associated pavement and/or overlay pay items.

MODEL: \$MODELNAME\$
FILE NAME: \$FILE\$

USER NAME = \$USERS\$	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, COMMITMENTS, GENERAL NOTES, AND JOB SPECIFIC NOTES	F A I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
DRAWN -	REVISED -	39			102[1RS,2RS-1;(2HVB)BRR]	WOODFORD	71	2	
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -			CONTRACT NO. 68D79				
PLOT DATE = \$DATE\$	DATE -	REVISED -			ILLINOIS FED. AID PROJECT				
				SCALE:	SHEET 1	OF 2	SHEETS	STA.	TO STA.

Hot-Mix Asphalt Mixture Requirements

Mixture Use(s)	Polymerized Leveling Binder (1 Inch)	Polymerized Stone Matrix Asphalt Binder Course (2 Inch)	Polymerized Stone Matrix Asphalt Surface Course (2 Inch)	Hot-Mix Asphalt Shoulder (Lower Lift)	Hot-Mix Asphalt Shoulder (Upper Lift)	I-39 / US 24 Ramps Surface Course (1.5 Inch)	Incidental Surface Course	Class D Patches and Class D Patches (Special)
AC/PG	SBS or SBR 76-22	SBS or SBR 76-28	SBS or SBR 76-28	PG 64-22	PG 64-22	SBS or SBR 76-28	PG 64-22	SBS or SBR 76-28
Design Air Voids	4.0% @ N=50	4.0% @ N=80	4.0% @ N=80	4.0% @ N=50	4.0% @ N=50	4.0% @ N=70	4.0% @ N=50	4.0% @ N=70
Mixture Composition (Mixture Gradation)	IL 4.75	IL 12.50	IL 12.50	IL 9.5 FG	IL 9.5 FG	IL 9.5	IL 9.5 FG	IL 9.5
Friction Aggregate	N/A	N/A	Mixture E	N/A	Mixture C	Mixture E	Mixture C	N/A
Quality Management Program	PFP	PFP	PFP	QCP	QCP	QC/QA	QC/QA	QC/QA

- Note: 1) Individual lift thickness of each mix type will be no less than 3 times nominal maximum aggregate size and no more than 6 times nominal maximum aggregate size, unless otherwise approved by the engineer.
 2) For design purposes, mixture weight for all mixes is determined to be 112.0 lb / sy / in, unless otherwise noted.
 3) Sublot sizes for PFP and QCP mixes will be 1000 tons, unless otherwise agreed to by the engineer and the paving contractor.

POLYMERIZED BITUMINOUS MATERIALS (TACK COAT) RATES

Surface Type	Residual Rate
Milled (HMA or PCC)	0.08 lb /sq ft
Existing Pavement	0.08 lb /sq ft
Fog Coat (between lifts)	0.08 lb /sq ft

PAVING SURFACE COURSE

Continuous paving operations on the main roadway shall be maintained at all times during the construction of the hot-mix asphalt surface. No interruptions for side roads, entrances, turn lanes, etc. will be allowed.

CROSSING EXISTING STRUCTURES WITH EQUIPMENT

SN 057-0203, SN 102-0058, and SN 102-0059 may be crossed with the empty MTD. Any structures not listed above shall be verified by the resident prior to beginning work.

ENGINEERS FIELD OFFICE

Add the following sentence to the end of paragraph 670.02 (i) and 670.04 (e):
 All of the telephone lines provided shall have unpublished numbers.

JOB SPECIFIC NOTES

PAVEMENT SCRATCH COAT DIP CORRECTIONS

At the locations (shown in plans) where there is pavement dips, the contractor shall place and compact a scratch pass of leveling binder prior to placing the proposed leveling binder lift as directed by the Resident Engineer. This work will be paid for at the contract unit price per ton for "Polymerized Leveling Binder (Machine Method), IL-4.75, N50".

WINTER SHUTDOWN RESTRICTIONS

Prior to winter shutdown the following steps shall be taken:

- * All level binder surfaces shall be overlaid with binder course
- * All lanes shall be reopened to traffic.
- * Temporary or permanent pavement marking shall be placed as applicable.

MODEL: \\MODELS\BAMES
FILE NAME: 31113

USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALES	CHECKED -	REVISED -
PLOT DATE = \$DATES	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

INDEX OF SHEETS, COMMITMENTS, GENERAL NOTES, AND JOB SPECIFIC NOTES			
SCALE:	SHEET 2	OF 2	SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	3
			CONTRACT NO. 68D79	
		ILLINOIS	FED. AID PROJECT	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE			
				90% FEDERAL	100% STATE	SN 102-0058	
				10% STATE	100% STATE	SN 102-0059	
				0006	0006	90% FEDERAL	
						10% STATE	
						0013	
21101615	TOPSOIL FURNISH & PLACE, 4"	SQ YD	423	423			
21400100	GRADING AND SHAPING DITCHES	FOOT	950	950			
25000210	SEEDING, CLASS 2A	ACRE	0.1	0.1			
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	9	9			
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	9	9			
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	9	9			
25000750	MOWING	ACRE	98		98		
25100635	HEAVY DUTY EROSION CONTROL BLANKET	SQ YD	423	423			
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	9	9			
28000305	TEMPORARY DITCH CHECKS	FOOT	56	56			
28000400	PERIMETER EROSION BARRIER	FOOT	250	250			
28000500	INLET AND PIPE PROTECTION	EACH	45	45			
28100225	STONE RIPRAP, CLASS B3	TON	895	895			
28100227	STONE RIPRAP, CLASS B4	TON	137	137			

MODEL: #MODELNAME\$
FILE NAME: \$FILE\$

USER NAME = \$USER\$	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS;12HVBJBRR]	WOODFORD	71	4
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				90% FEDERAL	100% STATE	90% FEDERAL
				10% STATE	100% STATE	10% STATE
				0006	0006	0013
40600295	POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)	POUND	540492	540492		
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	10087	10087		
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SQ YD	6337	6337		
40600990	TEMPORARY RAMP	SQ YD	994	994		
40603148	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, STONE MATRIX ASPHALT, N80	TON	16799	16799		
40603153	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80	TON	18951	18951		
40603565	POLYMERIZED HOT - MIX ASPHALT SURFACE COURSE, MIX "E", N70	TON	1178	1178		
40800050	INCIDENTAL HOT - MIX ASPHALT SURFACING	TON	161	161		
42000060	WELDED WIRE REINFORCEMENT	SQ YD	497	497		
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	400	400		
44004250	PAVED SHOULDER REMOVAL	SQ YD	445	445		
44200549	CLASS A PATCHES, TYPE I, 10 INCH	SQ YD	5	5		
44200553	CLASS A PATCHES, TYPE II, 10 INCH	SQ YD	348	348		
44200557	CLASS A PATCHES, TYPE III, 10 INCH	SQ YD	40	40		

MODEL: #MODELNAME\$
FILE NAME: \$FILE\$

USER NAME = \$USER\$	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS;12HVBJBRR]	WOODFORD	71	5
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				90% FEDERAL	100% STATE	SN 102-0058
				10% STATE	100% STATE	SN 102-0059
				0006	0006	90% FEDERAL 10% STATE 0013
44200559	CLASS A PATCHES, TYPE IV, 10 INCH	SQ YD	2308	2308		
44200970	CLASS B PATCHES, TYPE II, 10 INCH	SQ YD	139	139		
44200974	CLASS B PATCHES, TYPE III, 10 INCH	SQ YD	20	20		
44200976	CLASS B PATCHES, TYPE IV, 10 INCH	SQ YD	477	477		
44201299	DOWEL BARS 1 1/2"	EACH	440	440		
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	211	211		
44213000	PATCHING REINFORCEMENT	SQ YD	2700	2700		
44213200	SAW CUTS	FOOT	12817	12817		
44213204	TIE BARS 3/4"	EACH	2157	2157		
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	2101	2101		
45200300	JOINT OR CRACK FILLING	POUND	310	310		
48101200	AGGREGATE SHOULDERS, TYPE B	TON	11305	11305		
48203100	HOT - MIX ASPHALT SHOULDERS	TON	27437	27437		
48300500	PORTLAND CEMENT CONCRETE SHOULDERS 10"	SQ YD	445	445		

MODEL: #MODELNAME\$
FILE NAME: \$FILE\$

USER NAME = \$USER\$	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS;12HVBJBRR]	WOODFORD	71	6
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				90% FEDERAL	100% STATE	90% FEDERAL
				10% STATE	0006	10% STATE
				0006	0006	0013
50102400	CONCRETE REMOVAL	CU YD	21.8			21.8
50157300	PROTECTIVE SHIELD	SQ YD	1191			1191
50300255	CONCRETE SUPERSTRUCTURE	CU YD	21.8			21.8
50300260	BRIDGE DECK GROOVING	SQ YD	4614.7			4614.7
50300300	PROTECTIVE COAT	SQ YD	71.1			71.1
50500405	FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	1730			1730
50800105	REINFORCEMENT BARS	POUND	2860			2860
50800515	BAR SPLICERS	EACH	44			44
52000110	PREFORMED JOINT STRIP SEAL	FOOT	198			198
52100010	ELASTOMERIC BEARING ASSEMBLY, TYPE 1	EACH	32			32
52100520	ANCHOR BOLTS, 1"	EACH	64			64
54262712	METAL FLARED END SECTIONS 12"	EACH	4	4		

MODEL: #MODEL NAME
FILE NAME: #FILE #

USER NAME = #USER#	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = #SCALE#	CHECKED -	REVISED -
PLOT DATE = #DATE#	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS]x[2HVB]BRRJ	WOODFORD	71	7
CONTRACT NO. 68079				
ILLINOIS FED. AID PROJECT				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				90% FEDERAL	100% STATE	90% FEDERAL
				10% STATE	100% STATE	10% STATE
				0006	0006	0013
60260100	INLETS TO BE ADJUSTED	EACH	17	17		
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	400	400		
* 63000001	STEEL PLATE BEAM GUARD RAIL, TYPE A, 6 FOOT POSTS	FOOT	2325	2325		
* 63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	3	3		
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	7	7		
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	4	4		
63200310	GUARDRAIL REMOVAL	FOOT	2906	2906		
64200116	SHOULDER RUMBLE STRIPS, 16 INCH	FOOT	120768	120768		
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	10	10		
67100100	MOBILIZATION	L SUM	1	1		
70100207	TRAFFIC CONTROL AND PROTECTION, STANDARD 701402	EACH	2			2
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	4			4
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1		
70100700	TRAFFIC CONTROL AND PROTECTION, STANDARD 701406	L SUM	1	1		

*= SPECIALTY ITEM

MODEL: #MODELNAME\$
FILE NAME: \$FILE\$

USER NAME = \$USER\$	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS;2HVB]BRRJ	WOODFORD	71	8
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				90% FEDERAL	100% STATE	90% FEDERAL
				10% STATE	100% STATE	10% STATE
				0006	0006	0013
70100800	TRAFFIC CONTROL AND PROTECTION, STANDARD 701401	L SUM	1	1		
70100820	TRAFFIC CONTROL AND PROTECTION, STANDARD 701451	L SUM	1	1		
70100825	TRAFFIC CONTROL AND PROTECTION, STANDARD 701456	L SUM	1	1		
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	10	10		
70107025	CHANGEABLE MESSAGE SIGN	CAL DAY	73	73		
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	28609	28609		
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	3865	3865		
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	134	134		
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	138652	138652		
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	15498	15498		
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	4290	4290		
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	634	634		
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	64	64		
70300904	PAVEMENT MARKING TAPE, TYPE IV 4"	FOOT	15975			15975

MODEL: #MODELNAME\$
FILE: NAME\$.PLOT\$

USER NAME = #USER*	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = #SCALE*	CHECKED -	REVISED -
PLOT DATE = #DATE*	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS;12HVBJBRR]	WOODFORD	71	9
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				90% FEDERAL	100% STATE	SN 102-0058
				10% STATE	100% STATE	SN 102-0059
				0006	0006	90% FEDERAL 10% STATE 0013
70400100	TEMPORARY CONCRETE BARRIER	FOOT	1525			1525
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	1300			1300
70600250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	3			3
70600350	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2			2
* 72501000	TERMINAL MARKER - DIRECT APPLIED	EACH	4	4		
72700100	STRUCTURAL STEEL SIGN SUPPORT - BREAKAWAY	POUND	520	520		
73400100	CONCRETE FOUNDATIONS	CU YD	1.4	1.4		
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	136	136		
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	137184	137184		
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	15315	15315		
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	3154	3154		
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	684	684		
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	64	64		
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	1792	1792		

*= SPECIALTY ITEM

MODEL: #MODELNAME\$
FILE NAME: \$FILE\$

USER NAME = \$USER\$	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS;2HVB]BRRJ	WOODFORD	71	10
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				90% FEDERAL	100% STATE	SN 102-0058
				10% STATE	100% STATE	SN 102-0059
				0006	0006	90% FEDERAL 10% STATE 0013
* 78009006	MODIFIED URETHANE PAVEMENT MARKING - LINE 6"	FOOT	226	226		
* 78009008	MODIFIED URETHANE PAVEMENT MARKING - LINE 8"	FOOT	836	836		
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1669	1669		
* 78200005	GUARDRAIL REFLECTORS, TYPE A	EACH	27	27		
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	1565	1565		
X0322936	REMOVE EXISTING FLARED END SECTION	EACH	4	4		
X0323388	TRAFFIC COUNTER	EACH	1	1		
X0325893	CLEAN EXISTING END SECTION	EACH	11	11		
X0327980	PAVEMENT MARKING REMOVAL - WATER BLASTING	SO FT	2235			2235
X0600001	MICRO-SURFACING AGGREGATE FOR CENTERLINE JOINT FILLING	TON	4	4		
X0600002	MICRO-SURFACING EMULSIFIED ASPHALT FOR CENTERLINE JOINT FILLING	POUND	886	886		
X0600003	CENTERLINE JOINT CLEANING	FOOT	1171	1171		
X1700071	CLASS D PATCHES, TYPE II, 10 INCH (SPECIAL)	SO YD	32	32		
X2600002	RECOVERABLE DELINEATOR	EACH	322	322		

*= SPECIALTY ITEM

MODEL: #MODELNAME\$
FILE NAME: \$FILE\$

USER NAME = \$USER\$	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS;12HVBJBRR]	WOODFORD	71	11
CONTRACT NO. 68079				
ILLINOIS FED. AID PROJECT				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE			
				90% FEDERAL	100% STATE	SN 102-0058	
				10% STATE	0006	SN 102-0059	
				0006	0006	90% FEDERAL	
						10% STATE	
						0013	
X4400196	HOT - MIX ASPHALT SURFACE REMOVAL, SPECIAL	SQ YD	10181	10181			
X5420612	PIPE CULVERTS TO BE CLEANED 12"	FOOT	270	270			
X5420624	PIPE CULVERTS TO BE CLEANED 24"	FOOT	1417	1417			
X6350120	DELINEATOR REMOVAL	EACH	27	27			
X6650206	WOVEN WIRE FENCE TO BE REMOVED AND RE-ERECTED	FOOT	175	175			
X7010216	TRAFFIC CONTROL AND PROTECTION (SPECIAL)	L SUM	1	1			
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SQ FT	4334			4334	
* X7830060	GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS AND SYMBOLS	SQ FT	170	170			
* X7830070	GROOVING FOR RECESSED PAVEMENT MARKING 5"	FOOT	138976	138976			
* X7830074	GROOVING FOR RECESSED PAVEMENT MARKING 7"	FOOT	15541	15541			
* X7830076	GROOVING FOR RECESSED PAVEMENT MARKING 9"	FOOT	3990	3990			
* X7830078	GROOVING FOR RECESSED PAVEMENT MARKING 13"	FOOT	684	684			

*= SPECIALTY ITEM

MODEL: #MODELNAME
FILE NAME: #FILE#

USER NAME = #USER#	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = #SCALE#	CHECKED -	REVISED -
PLOT DATE = #DATE#	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS]x[2HVB]BRRJ	WOODFORD	71	12
CONTRACT NO. 68079				
ILLINOIS FED. AID PROJECT				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				90% FEDERAL	100% STATE	90% FEDERAL
				10% STATE	0006	10% STATE
				0006	0006	0013
* X7830090	GROOVING FOR RECESSED PAVEMENT MARKING 25"	FOOT	64	64		
Z0001002	GUARDRAIL AGGREGATE EROSION CONTROL	TON	433	433		
Z0001899	JACK AND REMOVE EXISTING BEARINGS	EACH	32			32
Z0001905	STRUCTURAL STEEL REPAIR	POUND	1260			1260
Z0006012	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 1/4 INCHES	SQ YD	4752.3			4752.3
Z0012130	BRIDGE DECK SCARIFICATION 3/4"	SQ YD	4752.3			4752.3
Z0012754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	72			72
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1		
Z0033600	LONGITUDINAL JOINT REPAIR	FOOT	1960	1960		
Z0034105	MATERIAL TRANSFER DEVICE	TON	47071	47071		
Z0038119	PORTLAND CEMENT CONCRETE SURFACE REMOVAL 1 1/2"	SQ YD	259			259
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1		

*= SPECIALTY ITEM

MODEL: #MODEL NAME
FILE NAME: #FILE #

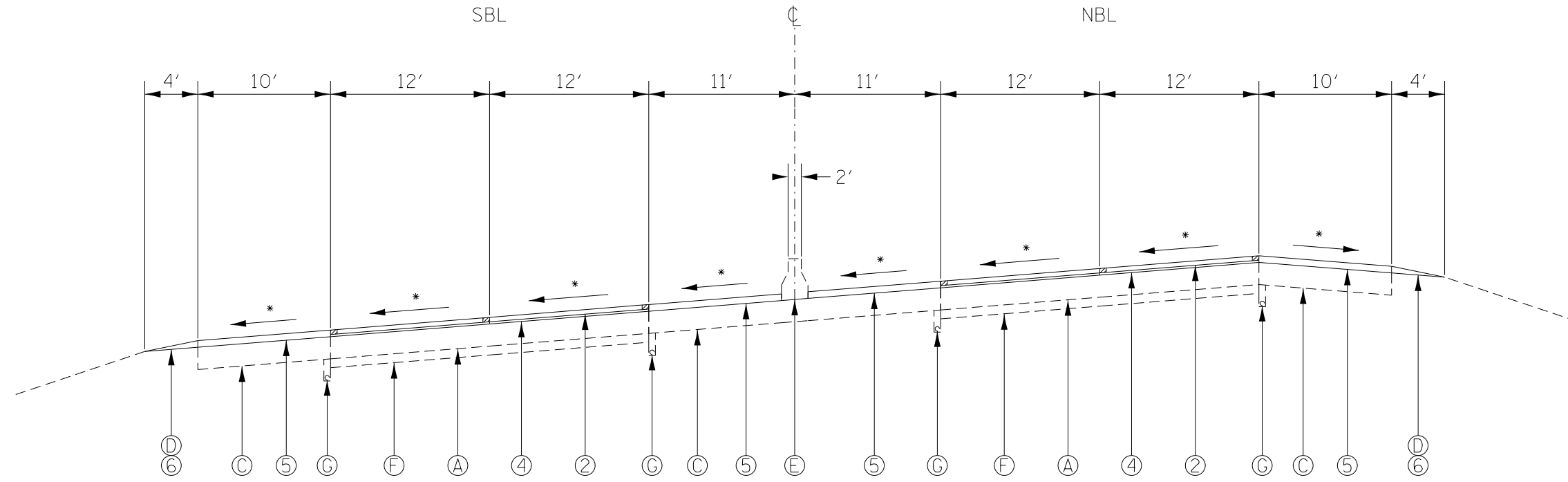
USER NAME = #USER*	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = #SCALE*	CHECKED -	REVISED -
PLOT DATE = #DATE*	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102L1RS,2RS;12HVBJBRRJ	WOODFORD	71	13
CONTRACT NO. 68079				

SUPERELEVATED SECTION



I-39

NB STA. 555+26 TO STA. 581+82
SB STA. 556+56 TO STA. 581+82

LEGEND

- (A) EXISTING CRPCC PAVEMENT, 10"
- (B) EXISTING PCC PAVEMENT (HINGE-JOINT), 10"
- (C) EXISTING PCC SHOULDER, 10"
- (D) EXISTING AGGREGATE SHOULDER
- (E) EXISTING CONCRETE BARRIER
- (F) EXISTING STABILIZED SUB-BASE, 4"
- (G) EXISTING PIPE UNDERDRAIN, 4"

- ① PROPOSED HOT -MIX ASPHALT SURFACE COURSE, MIX "E", N70
- ② PROPOSED POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80
- ③ PROPOSED POLYMERIZED HMA BINDER COURSE, STONE MATRIX ASPHALT, N80
- ④ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50
- ⑤ PROPOSED HOT -MIX ASPHALT SHOULDERS
- ⑥ PROPOSED AGGREGATE SHOULDERS, TYPE B

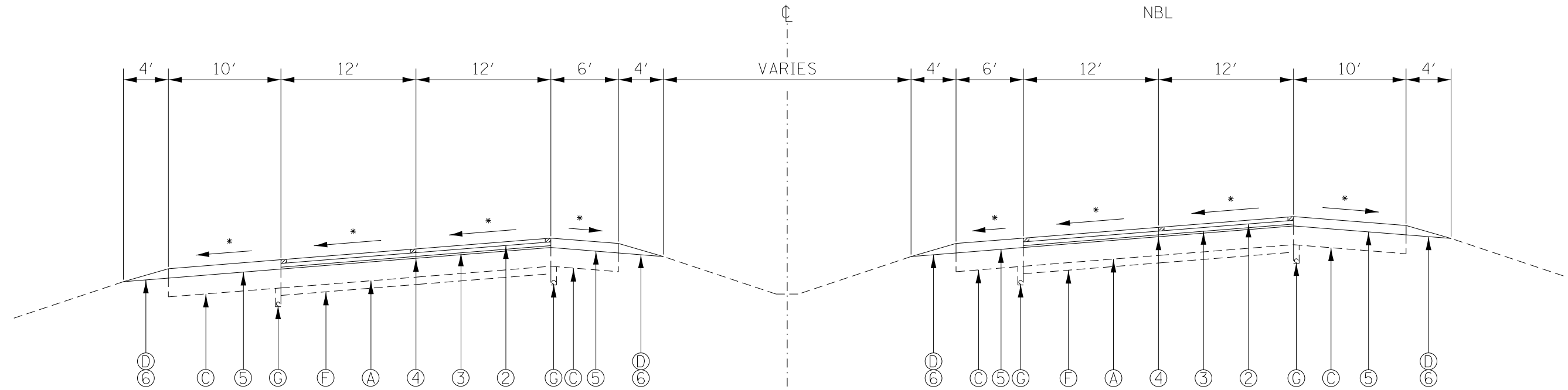
* MATCH EXISTING

SEE "CONSTRUCTION SEQUENCE FOR MAINLINE PAVEMENT AND SHOULDERS" SPECIAL PROVISION

MODEL: \\MODELS\BARRIERS
FILE NAME: 31115

USER NAME = \$USERS	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED TYPICAL SECTIONS			F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
DRAWN -	CHECKED -	REVISED -		39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	14			
PLOT SCALE = \$SCALE\$	DATE -	REVISED -		CONTRACT NO. 68D79			ILLINOIS FED. AID PROJECT				
PLOT DATE = \$DATE\$	SCALE:	REVISED -		SHEET 1	OF 4	SHEETS	STA.	TO STA.			

SUPERELEVATED SECTION



I-39

STA. 581+82 TO STA. 585+14
 STA. 622+11 TO STA. 650+83
 STA. 744+84 TO STA. 783+16

LEGEND

- (A) EXISTING CRPCC PAVEMENT, 10"
- (B) EXISTING PCC PAVEMENT (HINGE-JOINT), 10"
- (C) EXISTING PCC SHOULDER, 10"
- (D) EXISTING AGGREGATE SHOULDER
- (E) EXISTING CONCRETE BARRIER
- (F) EXISTING STABILIZED SUB-BASE, 4"
- (G) EXISTING PIPE UNDERDRAIN, 4"
- ① PROPOSED HOT -MIX ASPHALT SURFACE COURSE, MIX "E", N70
- ② PROPOSED POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80
- ③ PROPOSED POLYMERIZED HMA BINDER COURSE, STONE MATRIX ASPHALT, N80
- ④ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50
- ⑤ PROPOSED HOT -MIX ASPHALT SHOULDERS
- ⑥ PROPOSED AGGREGATE SHOULDERS, TYPE B

* MATCH EXISTING

SEE "CONSTRUCTION SEQUENCE FOR MAINLINE PAVEMENT AND SHOULDERS" SPECIAL PROVISION

SUPERELEVATION RATES FROM 1992 CONSTRUCTION PLANS

SA NB STA. 583+52 TO STA. 586+28 (S.E. 0.046)
 SA SB STA. 584+91 TO STA. 587+67 (S.E. 0.046)

SA STA. 619+95 TO STA. 623+19 (S.E. 0.057)
 SR STA. 649+74 TO STA. 652+96

SA STA. 742+67 TO STA. 745+91 (S.E. 0.054)
 SR STA. 761+09 TO STA. 763+95

SA STA. 763+95 TO STA. 766+82 (S.E. 0.054)
 SR STA. 782+08 TO STA. 785+32

MODEL: 140DELMAMES
FILE NAME: 31113

USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$\$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

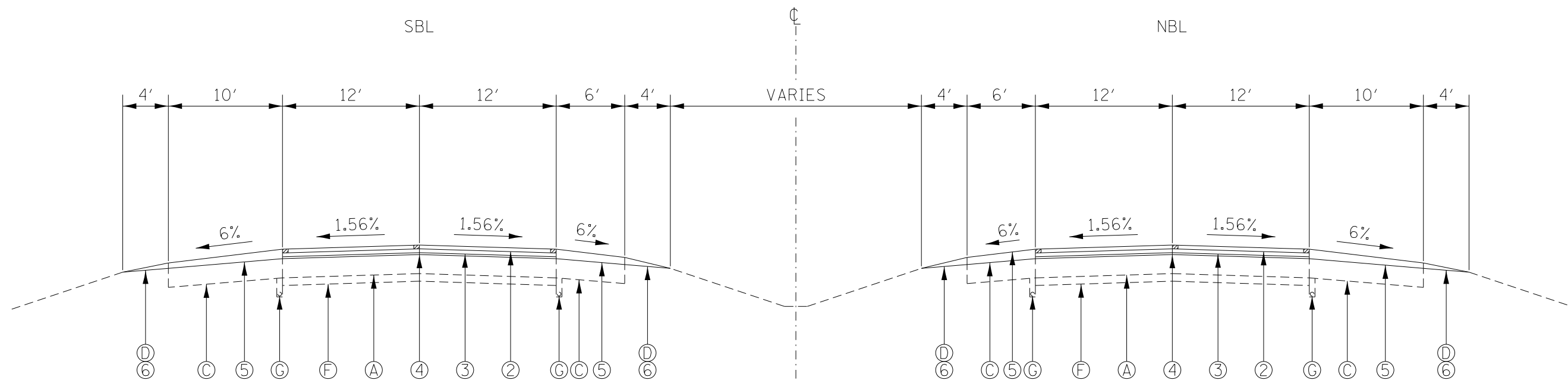
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PROPOSED TYPICAL SECTIONS

SCALE: SHEET 2 OF 4 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	15
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				

TANGENT SECTION



I-39

STA. 585+14 TO STA. 622+11
 STA. 650+83 TO STA. 744+84
 STA. 783+16 TO STA. 821+27 BK.
 STA. 417+00 AH. TO STA. 461+00

HMA OVERLAY BRIDGE OMISSION

STA. 448+23 TO STA. 452+71

SEE "CONSTRUCTION SEQUENCE FOR MAINLINE PAVEMENT AND SHOULDERS" SPECIAL PROVISION

LEGEND

- (A) EXISTING CRPCC PAVEMENT, 10"
- (B) EXISTING PCC PAVEMENT (HINGE-JOINT), 10"
- (C) EXISTING PCC SHOULDER, 10"
- (D) EXISTING AGGREGATE SHOULDER
- (E) EXISTING CONCRETE BARRIER
- (F) EXISTING STABILIZED SUB-BASE, 4"
- (G) EXISTING PIPE UNDERDRAIN, 4"
- ① PROPOSED HOT -MIX ASPHALT SURFACE COURSE, MIX "E", N70
- ② PROPOSED POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80
- ③ PROPOSED POLYMERIZED HMA BINDER COURSE, STONE MATRIX ASPHALT, N80
- ④ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50
- ⑤ PROPOSED HOT -MIX ASPHALT SHOULDERS
- ⑥ PROPOSED AGGREGATE SHOULDERS, TYPE B

* MATCH EXISTING

MODEL: 140DELEMAMES
FILE NAME: 31113

USER NAME = \$USERS	DESIGNED -	REVISED -
PLOT SCALE = \$SCALE\$	DRAWN -	REVISED -
PLOT DATE = \$DATE\$	CHECKED -	REVISED -
	DATE -	REVISED -

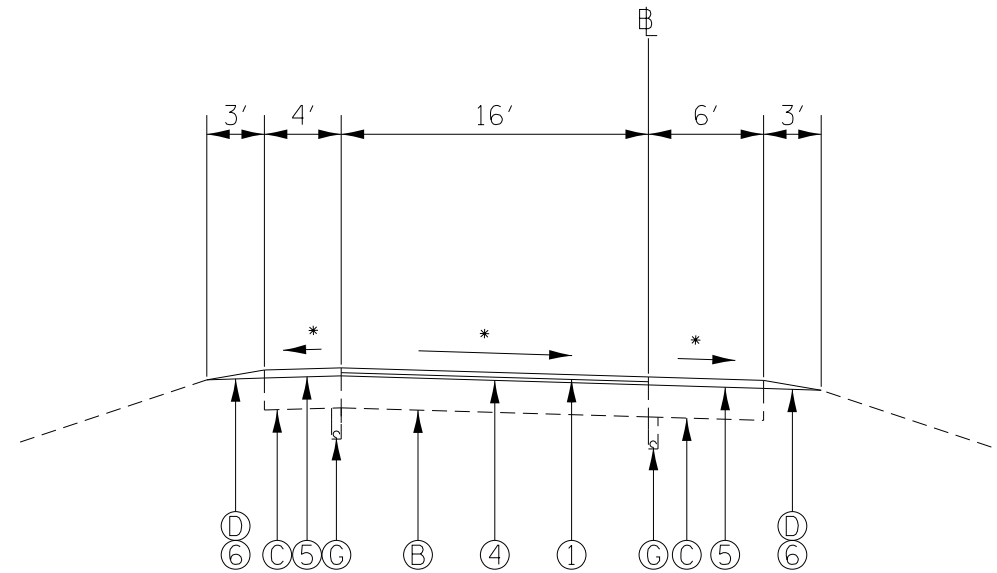
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PROPOSED TYPICAL SECTIONS

SCALE: SHEET 3 OF 4 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	16
			CONTRACT NO. 68D79	
		ILLINOIS	FED. AID PROJECT	

RAMPS



I-39 / US 24 INTERCHANGE

RAMP A STA. 6+61 TO STA. 26+35
 RAMP B STA. 0+00 TO STA. 13+89
 RAMP C STA. 6+63 TO STA. 25+28
 RAMP D STA. 0+00 TO STA. 21+26

LEGEND

- (A) EXISTING CRPCC PAVEMENT, 10"
- (B) EXISTING PCC PAVEMENT (HINGE-JOINT), 10"
- (C) EXISTING PCC SHOULDER, 10"
- (D) EXISTING AGGREGATE SHOULDER
- (E) EXISTING CONCRETE BARRIER
- (F) EXISTING STABILIZED SUB-BASE, 4"
- (G) EXISTING PIPE UNDERDRAIN, 4"

- (1) PROPOSED HOT -MIX ASPHALT SURFACE COURSE, MIX "E", N70
- (2) PROPOSED POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80
- (3) PROPOSED POLYMERIZED HMA BINDER COURSE, STONE MATRIX ASPHALT, N80
- (4) PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50
- (5) PROPOSED HOT -MIX ASPHALT SHOULDERS
- (6) PROPOSED AGGREGATE SHOULDERS, TYPE B

* MATCH EXISTING

MODEL: \\MODEL\NAME\$
 FILE NAME: \$FILE\$

USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PROPOSED TYPICAL SECTIONS

SCALE: SHEET 4 OF 4 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	17
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				



HMA OVERLAY BRIDGE OMISSION
 I-39 OVER US 24 AND TP&W RR
 NB SN 102-0059
 STA. 448+23 TO STA. 452+ 71
 SB SN 102-0058
 STA. 448+23 TO STA. 452+ 71

I-39 OVER MACKINAW RIVER
 NB / SB SN 057-0203
 STA. 551+11 TO STA. 555+41

CH 9 OVER I-39
 SN 102-0061

STATION EQUATION
 STA. 821+27 BK.
 STA. 417+00 AH.

RAMP D
 RAMP A
 RAMP B
 RAMP C

PROJECT ENDS
 STA. 461+00

TR 165 OVER I-39
 SN 102-0057

TR 229 OVER I-39
 SN 102-0062

TR 207 OVER I-39
 SN 102-0060

KAPPA

PROJECT BEGINS
 NB STA. 555+26
 SB STA. 555+56

MODEL: \$MODELNAME\$
 FILE NAME: \$FILE\$

USER NAME = \$USERS	DESIGNED -	REVISED -
PLOT SCALE = \$SCALE\$	DRAWN -	REVISED -
PLOT DATE = \$DATE\$	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

LINE DIAGRAM			
SCALE:	SHEET 1	OF 1	SHEETS
	STA.		TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1;(2HVB)BRR]	WOODFORD	71	18
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				

I-39

CURVE #1	CURVE #2	CURVE #3	CURVE #4	CURVE #5
P.I. STA. 558+06.59	P.I. STA. 580+96.13	P.I. STA. 637+80.55	P.I. STA. 753+97.47	P.I. STA. 774+56.71
$\Delta = 44^\circ 50' 30''$	$\Delta = 12^\circ 34' 40''$	$\Delta = 57^\circ 26' 03''$	$\Delta = 35^\circ 22' 51''$	$\Delta = 35^\circ 33' 37''$
D = 1° 20' 00"	D = 1° 30' 00"	D = 2° 00' 00"	D = 2° 00' 00"	D = 2° 00' 00"
R = 4297.18'	R = 3819.72'	R = 2864.79'	R = 2864.79'	R = 2864.79'
T = 1773.00'	T = 420.95'	T = 1569.54'	T = 913.75'	T = 918.68'
L = 3363.13'	L = 838.52'	L = 2871.71'	L = 1769.05'	L = 1778.01'
E = 351.40'	E = 23.13'	E = 401.78'	E = 142.19'	E = 143.70'
S.E. = 0.042' / FT.	S.E. = 0.046' / FT.	S.E. = 0.057' / FT.	S.E. = 0.054' / FT.	S.E. = 0.054' / FT.
P.C. STA. 540+33.59	P.C. STA. 576+75.18	P.C. STA. 622+11.02	P.C. STA. 744+83.72	P.C. STA. 765+38.02
P.T. STA. 573+96.72	P.T. STA. 585+13.70	P.T. STA. 650+82.73	P.T. STA. 762+52.77	P.T. STA. 783+16.03

I-39 / US 24 INTERCHANGE

RAMP A

CURVE #1	CURVE #2
P.I. STA. 9+12.88	P.I. STA. 11+63.27
$\Delta = 22^\circ 30' 00''$	$\Delta = 30^\circ 00' 00''$
D = 7° 30' 00"	D = 15° 00' 00"
R = 763.94'	R = 381.97'
T = 151.96'	T = 102.35'
L = 300.00'	L = 200.00'
E = 14.97'	E = 13.47'
S.E. = 0.080' / FT.	S.E. = 0.080' / FT.
P.C. STA. 7+60.92	P.C. STA. 10+60.92
P.T. STA. 10+60.92	P.T. STA. 12+60.92

RAMP B

CURVE #1	CURVE #2
P.I. STA. 17+85.55	P.I. STA. 18+36.93
$\Delta = 124^\circ 29' 08''$	$\Delta = 158^\circ 36' 15''$
D = 20° 45' 00"	D = 23° 00' 04"
R = 276.12'	R = 249.10'
T = 524.63'	T = 1318.58'
L = 599.92'	L = 689.55'
E = 316.76'	E = 1092.81'
S.E. = 0.080' / FT.	S.E. = 0.076' / FT.
P.C. STA. 12+60.92	P.C. STA. 5+18.35
P.T. STA. 18+60.84	P.T. STA. 12+07.90

RAMP C

CURVE #1	CURVE #2
P.I. STA. 10+77.19	P.I. STA. 18+32.36
$\Delta = 44^\circ 58' 44''$	$\Delta = 48^\circ 00' 00''$
D = 7° 30' 00"	D = 12° 00' 00"
R = 763.94'	R = 477.46'
T = 316.27'	T = 212.58'
L = 599.72'	L = 400.00'
E = 62.88'	E = 45.19'
S.E. = 0.080' / FT.	S.E. = 0.080' / FT.
P.C. STA. 7+60.92	P.C. STA. 16+19.78
P.T. STA. 13+60.64	P.T. STA. 20+19.78

RAMP D

CURVE #1	CURVE #2
P.I. STA. 10+17.54	P.I. STA. 18+83.09
$\Delta = 53^\circ 45' 00''$	$\Delta = 52^\circ 36' 15''$
D = 12° 00' 00"	D = 6° 45' 00"
R = 477.46'	R = 848.83'
T = 241.97'	T = 419.55'
L = 447.92'	L = 779.32'
E = 57.81'	E = 98.03'
S.E. = 0.080' / FT.	S.E. = 0.076' / FT.
P.C. STA. 7+75.57	P.C. STA. 14+63.54
P.T. STA. 12+23.49	P.T. STA. 22+42.86

MODEL: I39DELMEMES
FILE: I39D79_01.DWG

USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALES	CHECKED -	REVISED -
PLOT DATE = \$DATES	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CURVE DATA

SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	19
CONTRACT NO. 68D79			ILLINOIS FED. AID PROJECT	

RESURFACING TABLE (1 OF 3)																				
LOCATION		LENGTH	WIDTH				AREA			POLYMERIZED BITUMINOUS MATERIALS (TACK COAT) *	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, STONE MATRIX ASPHALT, N80	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80	HOT-MIX ASPHALT SHOULDERS	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX E", N70	INCIDENTAL HOT-MIX ASPHALT SURFACING	HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL	AGGREGATE SHOULDERS, TYPE B	SHOULDER RUMBLE STRIPS, 16 INCH	MATERIAL TRANSFER DEVICE
			ML	SHOULDER		ML	SHOULDER													
STATION	TO	STATION	FOOT	FOOT	FOOT	FOOT	SQ YD	SQ YD	SQ YD	POUND	TON	TON	TON	TON	TON	TON	SQ YD	TON	FOOT	TON
I-39 NORTHBOUND																				
555+26	TO	556+46	120	24	10	10	320	133	133	844.80	8.96		31.36	33.60				7.97	240.00	40.32
556+46	TO	577+32	2086	24	10	10	5563	2318	2318	14685.44	311.51		623.02	778.77				158.38	4172.00	934.53
577+32	TO	581+82	450	24	10 TO 18		1200	700	500	3456.00	67.20		134.40	201.60				34.17	900.00	201.60
581+82	TO	582+62	80	24	6	10	213	53	89	665.60	11.95	23.89	23.89	31.86				14.17	160.00	59.73
582+62	TO	585+14	252	24	6	10	672	168	280	2096.64	37.63	75.26	75.26	125.44				51.02	504.00	188.16
585+14	TO	622+11	3697	24	6	10	9859	2465	4108	30759.04	552.09	1104.17	1104.17	1467.63				659.64	7394.00	2760.43
622+11	TO	637+27	1516	24	6	10	4043	1011	1684	12613.12	226.39	452.78	452.78	754.63				306.94	3032.00	1131.95
637+27	TO	638+07	80	24	6	10	213	53	89	665.60	11.95	23.89	23.89	31.86				14.17	160.00	59.73
638+07	TO	639+65	158	24	6	10	421	105	176	1011.20	23.59		47.19	47.19				23.99	316.00	70.78
639+65	TO	640+45	80	24	6	10	213	53	89	665.60	11.95	23.89	23.89	31.86				14.17	160.00	59.73
640+45	TO	650+83	1038	24	6	10	2768	692	1153	8636.16	155.01	310.02	310.02	516.69				210.16	2076.00	775.04
650+83	TO	743+36	9253	24	6	10	24675	6169	10281	76984.96	1381.78	2763.56	2763.56	3673.24				1650.98	18506.00	6908.91
743+36	TO	744+16	80	24	6	10	213	53	89	665.60	11.95	23.89	23.89	30.86				13.97	160.00	59.73
744+16	TO	744+84	68	24	6	10	181	45	76	435.20	10.15		20.31	20.31				10.33	136.00	30.46
744+84	TO	745+70	86	24	6	10	229	57	96	550.40	12.84		25.69	25.69				13.06	172.00	38.53
745+70	TO	746+50	80	24	6	10	213	53	89	665.60	11.95	23.89	23.89	31.86				14.17	160.00	59.73
746+50	TO	783+16	3666	24	6	10	9776	2444	4073	30501.12	547.46	1094.91	1094.91	1824.85				742.25	7332.00	2737.28
783+16	TO	821+27	3811	24	6	10	10163	2541	4234	31707.52	569.11	1138.22	1138.22	1512.88				679.98	7622.00	2845.55
821+27 BK = 417+00 AH																				
STATION EQUATION																				
417+00	TO	426+04	904	24	6	10	2411	603	1004	7521.28	135.00	269.99	269.99	358.87				161.30	1808.00	674.99
426+04	TO	428+75	271	24	6		723	181		1821.12	40.47	80.94	80.94	43.50				25.03	271.00	202.35
428+75	TO	432+67	392	24	6	1 TO 10		1045	240	2979.20	58.54	117.08	117.08	113.91				36.21	392.00	292.69
432+67	TO	444+92	1225	24	6	10	3267	817	1361	10192.00	182.93	365.87	365.87	486.30				218.57	2450.00	914.67
444+92	TO	446+83	191	24	6	10 TO 5		509	127	1512.72	28.52	57.05	57.05	64.53				17.64	191.00	142.61
446+83	TO	448+23	140	24	6	5 TO 2		373	93	1019.20	10.45	36.59	36.59	26.90				11.07	140.00	83.63
SN 102-0059																				
HMA OVERLAY BRIDGE OMISSION																				
452+71	TO	454+11	140	24	6		373	93		940.80	10.45	36.59	36.59	16.99				11.07	140.00	83.63
454+11	TO	456+61	250	24	6		667	167		1680.00	37.33	74.67	74.67	40.13				23.09	250.00	186.67
456+61	TO	459+00	239	24	6	10	637	159	266	1988.48	35.69	71.38	71.38	94.88				42.64	478.00	178.45
459+00	TO	460+20	120	24	6	10	320	80	133	998.40	8.96	31.36	35.84	41.81				19.74	240.00	76.16
460+20	TO	461+00	80		6	10		53	89	204.80				19.91				11.14	160.00	
PAVEMENT SCRATCH COAT DIP CORRECTIONS ***																				
										58.91										
										2444.24		190.11	380.22			5092.17				190.11
										102.00					12.00					
										36.88										
										54.40					5.29					
										46.02					5.00					
										60.80					5.91					
										54.40					5.29					
										251267	4571	8200	9277	12829	0	34	5093	5198	59722	21989

* 3 APPLICATIONS ON MAINLINE AND 2 APPLICATIONS ON SHOULDERS
 ** LEFT AND RIGHT ARE RELATED TO THE DIRECTION OF TRAFFIC
 *** SEE "PAVEMENT SCRATCH COAT DIP CORRECTIONS" TABLE FOR LOCATIONS

MODEL: \\MODELS\FILES
 FILE NAME: 31115

USER NAME = \$USERS	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES				F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	DRAWN -	REVISED -		39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	20				
PLOT SCALE = \$SCALES	CHECKED -	REVISED -		CONTRACT NO. 68D79								
PLOT DATE = \$DATES	DATE -	REVISED -		SCALE:	SHEET 1	OF 12 SHEETS	STA.	TO STA.	ILLINOIS	FED. AID PROJECT		

RESURFACING TABLE (2 OF 3)																				
LOCATION		LENGTH	WIDTH				AREA			POLYMERIZED BITUMINOUS MATERIALS (TACK COAT) *	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, STONE MATRIX ASPHALT, N80	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80	HOT-MIX ASPHALT SHOULDERS	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX E", N70	INCIDENTAL HOT-MIX ASPHALT SURFACING	HOT-MIX ASPHALT SURFACE REMOVAL (SPECIAL)	AGGREGATE SHOULDERS, TYPE B	SHOULDER RUMBLE STRIPS, 16 INCH	MATERIAL TRANSFER DEVICE
			ML	SHOULDER		ML	SHOULDER													
				LT **	RT **		LT **	RT **												
STATION	TO STATION	FOOT	FOOT	FOOT	FOOT	SQ YD	SQ YD	SQ YD	POUND	TON	TON	TON	TON	TON	TON	SQ YD	TON	FOOT	TON	
I-39 SOUTHBOUND																				
555+56	TO 556+76	120	24	10	10	320	133	133	844.80	8.96		31.36	33.60				7.97	240.00	40.32	
556+76	TO 577+77	2101	24	10	10	5603	2334	2334	14791.04	313.75		627.50	784.37				159.52	4202.00	941.25	
577+77	TO 581+82	405	24	10 TO 20	10	1080	630	450	3110.40	60.48		120.96	181.44				30.75	810.00	181.44	
581+82	TO 582+62	80	24	6	10	213	53	89	665.60	11.95	23.89	23.89	31.86				14.17	160.00	59.73	
582+62	TO 585+14	252	24	6	10	672	168	280	2096.64	37.63	75.26	75.26	125.44				51.02	504.00	188.16	
585+14	TO 622+11	3697	24	6	10	9859	2465	4108	30759.04	552.09	1104.17	1104.17	1467.63				659.64	7394.00	2760.43	
622+11	TO 637+27	1516	24	6	10	4043	1011	1684	12613.12	226.39	452.78	452.78	754.63				306.94	3032.00	1131.95	
637+27	TO 638+07	80	24	6	10	213	53	89	665.60	11.95	23.89	23.89	31.86				14.17	160.00	59.73	
638+07	TO 639+65	158	24	6	10	421	105	176	1011.20	23.59		47.19	47.19				23.99	316.00	70.78	
639+65	TO 640+45	80	24	6	10	213	53	89	665.60	11.95	23.89	23.89	31.86				14.17	160.00	59.73	
640+45	TO 650+83	1038	24	6	10	2768	692	1153	8636.16	155.01	310.02	310.02	516.69				210.16	2076.00	775.04	
650+83	TO 743+36	9253	24	6	10	24675	6169	10281	76984.96	1381.78	2763.56	2763.56	3673.24				1650.98	18506.00	6908.91	
743+36	TO 744+16	80	24	6	10	213	53	89	665.60	11.95	23.89	23.89	30.86				13.97	160.00	59.73	
744+16	TO 744+84	68	24	6	10	181	45	76	435.20	10.15		20.31	20.31				10.33	136.00	30.46	
744+84	TO 745+70	86	24	6	10	229	57	96	550.40	12.84		25.69	25.69				13.06	172.00	38.53	
745+70	TO 746+50	80	24	6	10	213	53	89	665.60	11.95	23.89	23.89	31.86				14.17	160.00	59.73	
746+50	TO 783+16	3666	24	6	10	9776	2444	4073	30501.12	547.46	1094.91	1094.91	1824.85				742.25	7332.00	2737.28	
783+16	TO 821+27	3811	24	6	10	10163	2541	4234	31707.52	569.11	1138.22	1138.22	1512.88				679.98	7622.00	2845.55	
821+27 BK = 417+00 AH																				
STATION EQUATION																				
417+00	TO 418+91	191	24	6	10	509	127	212	1589.12	28.52	57.05	57.05	75.82				34.08	382.00	142.61	
418+91	TO 426+59	768	24	6		2048	512		5160.96	114.69	229.38	229.38	123.29				70.95	1536.00	573.44	
426+59	TO 430+58	399	24	6	1 TO 10	1064	266	244	3032.40	59.58	119.17	119.17	115.94				36.86	798.00	297.92	
430+58	TO 444+48	1390	24	6	10	3707	927	1544	11564.80	207.57	415.15	415.15	551.80				248.01	2780.00	1037.87	
444+48	TO 446+83	235	24	6	10 TO 6	627	157	209	1880.00	35.09	70.19	70.19	82.18				21.71	470.00	175.47	
446+83	TO 448+18	135	24	6	6 TO 1	360	90	53	982.80	10.08	35.28	35.28	25.94				10.68	270.00	80.64	
448+18	TO 448+23	5	24	6		13	3		33.60	0.37	1.31	1.31	0.61				0.40	10.00	2.99	
SN 102-0058																				
HMA OVERLAY BRIDGE OMISSION																				
452+71	TO 454+11	140	24	6		373	93		940.80	10.45	36.59	36.59	16.99				11.07	280.00	83.63	
454+11	TO 459+00	489	24	6	10	1304	326	543	4068.48	73.02	146.05	146.05	194.12				87.25	978.00	365.12	
459+00	TO 460+20	120	24	6	10	320	80	133	998.40	8.96	31.36	35.84	41.81				19.74	240.00	76.16	
460+20	TO 461+00	80		6	10		53	89	204.80				19.91				11.14	160.00		
PAVEMENT SCRATCH COAT DIP CORRECTIONS ***																				
MILL WIDE PAVE WIDE																				
									2441.84			189.92	379.84			5087.17				
									84.00						10.00					
									9.96											
									54.40						5.29					
									46.02						5.00					
									60.80						5.91					
									54.40						5.29					
									SUBTOTAL	250578	4573	8200	9268	12755	0	32	5088	5170	61046	21785

* 3 APPLICATIONS ON MAINLINE AND 2 APPLICATIONS ON SHOULDERS
 ** LEFT AND RIGHT ARE RELATED TO THE DIRECTION OF TRAFFIC
 *** SEE "PAVEMENT SCRATCH COAT DIP CORRECTIONS" TABLE FOR LOCATIONS

MODEL: \\MODEL\NAMES
 FILE: \\MODEL\FILES

USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALES	CHECKED -	REVISED -
PLOT DATE = \$DATES	DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES			
SCALE:	SHEET 2	OF 12 SHEETS	STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	21
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				

RESURFACING TABLE (3 OF 3)																																
STATION	TO STATION	LENGTH FOOT	WIDTH				AREA			POLYMERIZED BITUMINOUS MATERIALS (TACK COAT) *	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, STONE MATRIX ASPHALT, N80	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80	HOT-MIX ASPHALT SHOULDERS	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX E", N70	INCIDENTAL HOT-MIX ASPHALT SURFACING	HOT-MIX ASPHALT SURFACE REMOVAL (SPECIAL)	AGGREGATE SHOULDERS, TYPE B	SHOULDER RUMBLE STRIPS, 16 INCH	MATERIAL TRANSFER DEVICE												
			ML	SHOULDER		ML	SHOULDER		SQ YD												SQ YD	SQ YD	POUND	TON	TON	TON	TON	TON	SQ YD	TON	FOOT	TON
				LT **	RT **		LT **	RT **																								
MEDIAN CROSSOVERS																																
590+00		42	42			196			282.24						47.20		5.31															
793+02		42	42			196			282.24						47.20		5.31															
821+27 BK = 417+00 AH STATION EQUATION																																
SUBTOTAL									565	0	0	0	0	0	95	0	11	0	0													
I-39 / US 24 INTERCHANGE																																
RAMP A																																
SN 102-0058 HMA OVERLAY BRIDGE OMISSION																																
2+86	TO 2+91	5	15 TO 16		6	9		3	17.20		0.72	0.72	0.56						1.45													
2+91	TO 4+26	135	16	1 TO 2	6	240	23	90	680.40	6.72	23.52	23.52	25.20						53.76													
4+26	TO 5+61	135	16	2 TO 3	6	240	38	90	702.00	13.44	26.88	26.88	35.70						67.20													
5+61	TO 6+61	100	16	3 TO 4	6	178	39	67	536.00	9.96	9.96	17.42	22.17				6.41		37.33													
6+61	TO 24+85	1824	16	4	6	3243	811	1216	6128.64	181.59			283.73	272.38			190.42		453.97													
24+85	TO 25+85	100	16 TO 32	4 TO 8	6 TO 12	320	118	199	689.04	17.92			44.38	26.88			10.44		44.80													
25+85	TO 26+35	50	32	8	12	160	59	99	344.16	4.48			17.70	13.44			4.75		17.92													
SUBTOTAL									9098	235	62	69	430	313	0	0	213	0	677													
RAMP B																																
0+00	TO 0+50	50	32 TO 24	4	6	244	32	69	424.08	6.83			11.31	20.50			4.75		27.33													
0+50	TO 0+79	29	24 TO 16	4	6	142	22	42	250.56	7.95			8.96	11.93			3.03		19.88													
0+79	TO 13+89	1310	16	4	6	2329	582	873	4401.60	130.42			203.78	195.63			136.76		326.04													
13+89	TO 14+89	100	16	4 TO 3	6	178	39	67	536.00	4.98	17.42	17.42	23.64				6.64		39.82													
14+89	TO 15+80	91	16	3 TO 2	6	162	25	61	473.20	9.06	18.12	18.12	24.06				6.91		45.30													
15+80	TO 17+20	140	16	2 TO 1	6	249	23	93	705.60	6.97	24.39	24.39	26.13				0.00		55.75													
SN 102-0059 HMA OVERLAY BRIDGE OMISSION																																
21+68	TO 23+08	140	5 TO 3		6	62		93	268.80	1.74	6.10	6.10	20.91				0.00		13.94													
23+08	TO 25+58	250	3 TO 1		6	56		167	360.00	3.11	6.22	6.22	46.67				0.00		15.56													
SUBTOTAL									7420	172	73	73	366	229	0	0	159	0	544													
RAMP C																																
0+00	TO 2+71	271	1 TO 16		6	256		181	813.00	14.33	28.67	28.67	50.59				20.58		71.66													
2+71	TO 5+63	292	16	1 TO 3	6	519	65	195	1495.04	29.07	58.14	58.14	72.68				22.17		145.35													
5+63	TO 6+63	100	16	3 TO 4	6	178	39	67	536.00	4.98	17.42	17.42	23.64				6.64		39.82													
6+63	TO 23+78	1715	16	4	6	3049	762	1143	5762.40	170.74			266.78	256.11			179.04		426.84													
23+78	TO 24+78	100	16 TO 32	4 TO 8	6 TO 12	320	118	199	689.04	17.92			44.38	26.88			10.44		44.80													
24+78	TO 25+28	50	32	8	12	160	59	99	344.16	4.48			17.70	13.44			4.75		17.92													
SUBTOTAL									9640	242	105	105	476	297	0	0	244	0	747													
RAMP D																																
0+00	TO 0+50	50	32 TO 24	4	6	244	32	69	424.08	6.83			11.31	20.50			4.75		27.33													
0+50	TO 0+79	29	24 TO 16	4	6	142	22	42	250.56	7.95			8.96	11.93			3.03		19.88													
0+79	TO 21+26	2047	16	4	6	3639	910	1365	6877.92	203.79			318.42	305.69			213.70		509.48													
21+26	TO 22+26	100	16	4 TO 3	6	178	39	67	536.00	4.98	17.42	17.42	23.64				6.64		39.82													
22+26	TO 25+25	299	16	1 TO 3	6	532	66	199	1530.88	29.77	59.53	59.53	74.42				22.70		148.84													
25+25	TO 32+93	768	1 TO 16		6	725		512	2304.00	40.62	81.24	81.24	143.36				58.31		203.09													
SUBTOTAL									11924	294	159	159	581	339	0	0	310	0	949													
TOTAL									540492	10087	16799	18951	27437	1178	161	10181	11305	120768	46691													

* 3 APPLICATIONS ON MAINLINE AND 2 APPLICATIONS ON SHOULDERS
 ** LEFT AND RIGHT ARE RELATED TO THE DIRECTION OF TRAFFIC
 *** SEE "PAVEMENT SCRATCH COAT DIP CORRECTIONS" TABLE FOR LOCATIONS

MODEL: \$MODELNAME\$
 FILE NAME: \$FILES\$

USER NAME = \$USERS\$	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALES\$	CHECKED -	REVISED -
PLOT DATE = \$DATES\$	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE: SHEET 3 OF 12 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1;(2HVB)BRR]	WOODFORD	71	22
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				

BUTT JOINTS AND TEMPORARY RAMPS							
LOCATION				LENGTH	WIDTH	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	TEMPORARY RAMP
DESCRIPTION	STATION	TO	STATION	FOOT	FOOT	SQ YD	SQ YD
I-39 NORTHBOUND							
START OF PROJECT (SHOULDERS)	555+26	TO	555+86	60	20	133.33	
START OF PROJECT (MAINLINE)	555+26	TO	556+06	80	24	213.33	35.56
AFTER SN 102-0062 (MAINLINE)	577+77	TO	578+57	80	24	213.33	35.56
BEFORE SN 102-0061 (MAINLINE)	637+27	TO	638+07	80	24	213.33	35.56
AFTER SN 102-0061 (MAINLINE)	639+65	TO	640+45	80	24	213.33	35.56
BEFORE SN 102-0060 (MAINLINE)	743+36	TO	744+16	80	24	213.33	35.56
AFTER SN 102-0060 (MAINLINE)	745+70	TO	746+50	80	24	213.33	35.56
STATION EQUATION	821+27 BK = 417+00 AH						
BEFORE SN 102-0059 (MAINLINE)	447+23	TO	448+23	100	24	266.67	44.44
AFTER SN 102-0059 (MAINLINE)	452+71	TO	453+71	100	24	266.67	44.44
END OF PROJECT (SHOULDERS)	460+20	TO	461+00	80	16	142.22	
END OF PROJECT (MAINLINE)	459+40	TO	460+20	80	24	213.33	71.11
SUBTOTAL						2303	374
I-39 SOUTHBOUND							
START OF PROJECT (SHOULDERS)	555+56	TO	556+16	60	20	133.33	
START OF PROJECT (MAINLINE)	555+56	TO	556+36	80	24	213.33	35.56
AFTER SN 102-0062 (MAINLINE)	577+77	TO	578+57	80	24	213.33	35.56
BEFORE SN 102-0061 (MAINLINE)	637+27	TO	638+07	80	24	213.33	35.56
AFTER SN 102-0061 (MAINLINE)	639+65	TO	640+45	80	24	213.33	35.56
BEFORE SN 102-0060 (MAINLINE)	743+36	TO	744+16	80	24	213.33	35.56
AFTER SN 102-0060 (MAINLINE)	745+70	TO	746+50	80	24	213.33	35.56
STATION EQUATION	821+27 BK = 417+00 AH						
SN 102-0058 (MAINLINE)	447+23	TO	448+23	100	24	266.67	44.44
SN 102-0058 (MAINLINE)	452+71	TO	453+71	100	24	266.67	44.44
END OF PROJECT (SHOULDERS)	460+20	TO	461+00	80	16	142.22	
END OF PROJECT (MAINLINE)	459+40	TO	460+20	80	24	213.33	71.11
SUBTOTAL						2303	374
I-39 / US 24 INTERCHANGE							
RAMP A							
SN 102-0058	2+86	TO	3+86	100	16	177.78	29.63
TRANSITION TO MAINLINE	5+81	TO	6+61	80	16	142.22	23.70
END OF RAMP A (SHOULDERS)	26+05	TO	26+35	30	20	66.67	
END OF RAMP A (MAINLINE)	26+05	TO	26+35	30	32	106.67	17.78
SUBTOTAL						494	72
RAMP B							
START OF RAMP B (SHOULDERS)	0+00	TO	0+30	30	20	66.67	
START OF RAMP B (MAINLINE)	0+00	TO	0+30	30	32	106.67	17.78
TRANSITION TO MAINLINE	13+89	TO	14+69	80	16	142.22	23.70
RAMP B SN 102-0059	16+20	TO	17+20	100	16	177.78	29.63
RAMP B SN 102-0059	21+68	TO	22+68	100	10	111.11	18.52
SUBTOTAL						605	90
RAMP C							
TRANSITION TO MAINLINE	5+83	TO	6+63	80	16	142.22	23.70
END OF RAMP C (SHOULDERS)	24+98	TO	25+28	30	20	66.67	
END OF RAMP C (MAINLINE)	24+98	TO	25+28	30	32	106.67	17.78
SUBTOTAL						316	42
RAMP D							
START OF RAMP D (SHOULDERS)	0+00	TO	0+30	30	20	66.67	
START OF RAMP D (MAINLINE)	0+00	TO	0+30	30	32	106.67	17.78
TRANSITION TO MAINLINE	21+26	TO	22+06	80	16	142.22	23.70
SUBTOTAL						316	42
TOTAL						6337	994

PAVEMENT SCRATCH COAT DIP CORRECTIONS LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 *						
LOCATION		LENGTH	WIDTH	AREA	TON **	
STATION	TO	STATION	FOOT	FOOT	SQ YD	
I-39 NORTHBOUND						
612+34	TO	613+13	79	24	210.67	8.85
639+84	TO	640+44	60	24	160.00	6.72
821+27 BK = 417+00 AH			STATION EQUATION			
420+32	TO	421+40	108	24	288.00	12.10
456+50	TO	458+16	166	24	442.67	18.59
458+87	TO	460+00	113	24	301.33	12.66
SUBTOTAL						58.91
I-39 SOUTHBOUND						
428+61	TO	431+55	294	24	784.00	32.93
435+83	TO	436+27	44	24	117.33	4.93
436+55	TO	436+80	25	24	66.67	2.80
437+57	TO	438+00	43	24	114.67	4.82
438+36	TO	438+94	58	24	154.67	6.50
440+90	TO	441+16	26	24	69.33	2.91
441+66	TO	441+92	26	24	69.33	2.91
442+39	TO	443+06	67	24	178.67	7.50
821+27 BK = 417+00 AH			STATION EQUATION			
SUBTOTAL						65.30
TOTAL						124.21

* FINAL LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER

** AVERAGE DEPTH OF 3/4" USED

INLETS TO BE ADJUSTED	
STATION	EACH
I-39 NORTHBOUND	
821+27 BK = 417+00 AH	
427+12	1
445+94	1
448+05	2
452+95	2
455+48	1
457+97	1
459+97	1
SUBTOTAL	
9	
I-39 SOUTHBOUND	
821+27 BK = 417+00 AH	
429+57	1
445+08	1
448+13	2
452+94	2
455+51	1
458+01	1
SUBTOTAL	
8	
TOTAL	
17	

LOCATION *	JOINT OR CRACK FILLING	STRIP REFLECTIVE CRACK CONTROL TREATMENT **
	POUND	FOOT
I-39 NORTHBOUND	100	300
I-39 SOUTHBOUND	100	300
RAMP B	70	210
RAMP C	40	120
TOTAL		930

* FINAL LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER

** PLACE ON JOINT (AFTER FILLING)

LOCATION * ***		LENGTH	AVERAGE DEPTH	CENTERLINE JOINT CLEANING	MICRO-SURFACING AGGREGATE FOR CENTERLINE JOINT FILLING	MICRO-SURFACING EMULSIFIED ASPHALT FOR CENTERLINE JOINT FILLING	STRIP REFLECTIVE CRACK CONTROL TREATMENT **	
STATION	TO	STATION	FOOT	FOOT	FOOT	TON	POUND	FOOT
I-39 NORTHBOUND								
556+45	TO	558+32	187	0.50	187	0.50	120.51	187
558+52	TO	563+97	545	0.60	545	1.76	421.46	545
601+63	TO	601+87	24	0.83	24	0.11	25.67	24
821+27 BK = 417+00 AH			STATION EQUATION					
459+17	TO	460+83	166	0.60	166	0.53	128.37	166
SUBTOTAL					922	3	697	922
I-39 SOUTHBOUND								
556+50	TO	557+99	149	0.48	149	0.38	92.18	149
682+18	TO	682+61	43	0.75	43	0.17	41.57	43
758+09	TO	758+66	57	0.75	57	0.23	55.10	57
821+27 BK = 417+00 AH			STATION EQUATION					
SUBTOTAL					249	1	189	249
TOTAL					1171	4	886	1171

* FINAL LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER

** PLACE ON LEVELING BINDER

*** SEE THE RESURFACING TABLE FOR "POLYMERIZED BITUMINOUS MATERIAL (TACK COAT)" QUANTITIES

MODEL: \\MODEL\NAMES
FILE: \\MODEL\FILES

USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALES	CHECKED -	REVISED -
PLOT DATE = \$DATES	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE: SHEET 4 OF 12 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	23
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				

CLASS A PATCHES, 10 INCH										
LENGTH	WIDTH	NUMBER OF PATCHES		TYPE I	TYPE II	TYPE III	TYPE IV	PATCHING REINFORCEMENT	SAW CUTS	TIE BARS 3/4"
		PL	DL							
I-39 NORTHBOUND										
6	6		1	4.00				4.00	42	
20	6		2		26.67			26.67	168	
40	6		1					26.67	144	40
50	6		3					100.00	522	50
80	6		1					53.33	264	80
100	6		1					66.67	324	100
120	6		1					80.00	384	120
150	6		2					200.00	948	150
6	12		18		144.00			144.00	1188	
8	12		7		74.67			74.67	504	
12	12		1			16.00		16.00	84	
20	12		1					26.67	108	
30	12		1					40.00	138	30
50	12		1					66.67	198	50
80	12	1	2					320.00	864	80
150	12		2					400.00	996	150
200	12		2					533.33	1296	200
15	16		1					26.67	109	
SUBTOTAL				4	246	16	1940	2206	8281	1050
I-39 SOUTHBOUND										
130	4		1					57.78	406	130
50	6		2					66.67	348	50
6	12		6		48.00			48.00	396	
8	12		2		21.33			21.33	144	
15	12		1			20.00		20.00	93	
25	12		1					33.33	123	25
SUBTOTAL				0	70	20	158	248	1510	205
TOTAL				4	316	36	2098	2454	9791	1255
WINTER BREAK-UP (ADD 10%)				1	32	4	210	246	980	126
GRAND TOTAL				5	348	40	2308	2700	10771	1381

CLASS B PATCHES, 10 INCH									
LENGTH	WIDTH	NUMBER OF PATCHES	TYPE II	TYPE III	TYPE IV	SAW CUTS	DOWEL BARS 1 1/2"	TIE BARS 3/4"	WELDED WIRE REINFORCEMENT
I-39 / US 24 INTERCHANGE									
RAMP A									
6	12	6	48.00			324	24		
40	20	1			88.89	180	40	40	88.89
SUBTOTAL			48	0	89	504	64	40	89
RAMP B									
6	12	2	16.00			108	24		
10	16	1		17.78		78	32		17.78
25	20	1			55.56	135	40	25	55.56
SUBTOTAL			16	18	56	321	96	25	74
RAMP C									
6	12	2	16.00			108	24		
6	20	1	13.33			78	40		
50	20	1			111.11	210	40	50	111.11
SUBTOTAL			30	0	112	396	104	50	112
RAMP D									
40	12	1			53.33	156	24	40	53.33
6	16	3	32.00			198	32		
25	20	1			55.56	135	40	25	55.56
30	20	1			66.67	150	40	30	66.67
SUBTOTAL			32	0	176	639	136	95	176
TOTAL			126	18	433	1860	400	210	451
WINTER BREAK-UP (ADD 10%)			13	2	44	186	40	21	46
GRAND TOTAL			139	20	477	2046	440	231	497

CLASS D PATCHES, TYPE II, 10 INCH *						
LOCATION			LENGTH	WIDTH	AREA	
STATION	TO	STATION			DL	PL
FOOT		FOOT	FOOT	SQ YD	SQ YD	
I-39 NORTHBOUND						
556+10	TO	556+16	6	12	8.00	
556+10	TO	556+16	6	12		8.00
821+27 BK = 417+00 AH			STATION EQUATION			
444+40	TO	444+46	6	12	8.00	
444+40	TO	444+46	6	12		8.00
447+11	TO	447+17	6	12	8.00	
447+11	TO	447+17	6	12		8.00
453+63	TO	453+69	6	20	13.33	
453+63	TO	453+69	6	12		8.00
453+95	TO	454+01	6	16	10.67	
453+95	TO	454+01	6	12		8.00
SUBTOTAL					88	
I-39 SOUTHBOUND						
556+67	TO	556+73	6	12	8.00	
556+67	TO	556+73	6	12		8.00
821+27 BK = 417+00 AH			STATION EQUATION			
444+45	TO	444+51	6	12	8.00	
444+45	TO	444+51	6	12		8.00
447+17	TO	447+23	6	12	8.00	
447+17	TO	447+23	6	12		8.00
453+63	TO	453+69	6	12	8.00	
453+63	TO	453+69	6	12		8.00
453+95	TO	454+01	6	12	8.00	
453+95	TO	454+01	6	12		8.00
SUBTOTAL					80	
I-39 / US 24 INTERCHANGE						
RAMP A						
6+58	TO	6+64	6	16	10.67	
SUBTOTAL					11	
RAMP B						
13+86	TO	13+92	6	16	10.67	
SUBTOTAL					11	
RAMP C						
6+60	TO	6+66	6	16	10.67	
SUBTOTAL					11	
RAMP D						
21+23	TO	21+29	6	16	10.67	
SUBTOTAL					11	
TOTAL					211	

* REMOVAL OF THE EXISTING EXPANSION JOINTS AND REPLACEMENT WITH CLASS D PATCHES SHALL BE PAID FOR AS "CLASS D PATCHES, TYPE II, 10 INCH"

CLASS D PATCHES, TYPE II, 10 INCH (SPECIAL) *						
LOCATION			LENGTH	WIDTH	AREA	
STATION	TO	STATION			DL	PL
FOOT		FOOT	FOOT	SQ YD	SQ YD	
I-39 NORTHBOUND						
556+42	TO	556+48	6	12	8.00	
556+42	TO	556+48	6	12		8.00
SUBTOTAL					16	
I-39 SOUTHBOUND						
557+98	TO	558+04	6	12	8.00	
557+98	TO	558+04	6	12		8.00
SUBTOTAL					16	
TOTAL					32	

* SEE "CLASS D PATCHES (SPECIAL) DETAIL"

MODEL: \\MODEL\NAMES
FILE: \\MODEL\FILES

USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALES	CHECKED -	REVISED -
PLOT DATE = \$DATES	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

SCALE: SHEET 5 OF 12 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	24
			CONTRACT NO. 68D79	
		ILLINOIS FED. AID PROJECT		

LOCATION				LENGTH	WIDTH	PAVED SHOULDER REMOVAL	PORTLAND CEMENT CONCRETE SHOULDERS 10"	TIE BARS 3/4" **
STATION	TO	STATION	DESCRIPTION	FOOT	FOOT	SQ YD	SQ YD	EACH
556+60	TO	557+10	SOUTHBOUND	50	10	55.56	55.56	17
642+30	TO	642+70	NORTHBOUND	40	10	44.44	44.44	14
758+85	TO	759+05	SOUTHBOUND	20	10	22.22	22.22	7
760+30	TO	760+50	SOUTHBOUND	20	10	22.22	22.22	7
821+27 BK = 417+00 AH STATION EQUATION								
429+50	TO	432+50	RAMP C GORE *	300	2 TO 16	300.00	300.00	300
TOTAL						445	445	345

* REMOVAL OF EXISTING REINFORCEMENT SHALL BE INCLUDED IN THE UNIT PRICE FOR "PAVED SHOULDER REMOVAL"
 ** TIE BARS AT 24" CENTERS

LOCATION		COMBINATION CURB AND GUTTER REMOVAL **	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	TIE BARS 3/4"
STATION	LT / RT *	FOOT	FOOT	EACH
I-39 NORTHBOUND				
448+05	LT	50	50	25
448+05	RT	50	50	25
452+95	LT	50	50	25
452+95	RT	50	50	25
SUBTOTAL		200	200	100
I-39 SOUTHBOUND				
448+13	LT	50	50	25
448+13	RT	50	50	25
452+94	LT	50	50	25
452+94	RT	50	50	25
SUBTOTAL		200	200	100
TOTAL		400	400	200

* LEFT AND RIGHT ARE RELATED TO THE DIRECTION OF TRAFFIC
 ** REMOVAL OF EXISTING CURB AND PART OF SHOULDER SHALL BE INCLUDED IN THE UNIT PRICE FOR "COMBINATION CURB AND GUTTER REMOVAL"

LONGITUDINAL JOINT REPAIR *		LENGTH
LOCATION	FOOT	
I-39 NORTHBOUND	910	
I-39 SOUTHBOUND	250	
RAMP A	200	
RAMP B	400	
RAMP C	200	
TOTAL		1960

* FINAL LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER

TRAFFIC COUNTER			
STATION	STRUCTURAL STEEL SIGN SUPPORT - BREAKAWAY	CONCRETE FOUNDATIONS	TRAFFIC COUNTER
	POUND	CU YD	EACH
590+00	520	1.4	1
TOTAL		520	1

RECONNECT PIPE UNDERDRAIN TO HEADWALL *		
LOCATION		EACH
STATION	LT / RT / MED	
797+57	RT	1
455+00	MED	1
457+86	MED	1
460+00	MED	1
SUBTOTAL		4
I-39 / US 24 INTERCHANGE		
RAMP A		
22+00	LT	1
SUBTOTAL		1
TOTAL		5

* SHALL BE PAID FOR PER ARTICLE 109.04

CLEAN EXISTING END SECTION			
LOCATION		SIZE	EACH
STATION	LT / RT	INCH	
576+75	LT / RT	48	2
637+40	LT / RT	72 X 36	2
682+50	LT / RT	120 X 36	2
758+35	LT / RT	84 X 36	2
795+06	LT / RT	84 X 36	2
821+27 BK = 417+00 AH			
432+60	LT	36	1
TOTAL			11

REMOVE EXISTING FLARED END SECTION	
STATION	EACH
448+05	2
452+95	2
TOTAL	4

METAL FLARED END SECTIONS 12" *	
STATION	EACH
448+05	2
452+95	2
TOTAL	4

* 24" CONNECTION BAND IS INCLUDED IN THE PRICE FOR "METAL END SECTIONS 12"

WOVEN WIRE FENCE TO BE REMOVED AND RE-ERECTED	
STATION	FOOT
555+11 RT	25
682+50 LT	25
682+50 RT	25
758+35 LT	25
758+35 RT	25
795+06 LT	25
795+06 RT	25
821+27 BK = 417+00 AH	
TOTAL	175

PIPE CULVERTS TO BE CLEANED			
LOCATION		12"	24"
STATION	LT / RT	FOOT	FOOT
I-39			
557+00	LT	62	
562+00	LT	49	
567+00	LT	49	
572+00	LT	56	
576+00	LT	54	
582+00	RT		78
590+53	LT		86
598+00	LT		84
640+00	LT / RT		161
659+50	LT / RT		186
713+00	LT / RT		183
743+80	LT / RT		138
797+57	RT		55
798+45	RT		50
813+00	RT		48
821+27 BK = 417+00 AH			
423+00	RT		46
438+30	LT / RT		142
SUBTOTAL		270	1257
I-39 / US 24 INTERCHANGE			
RAMP A			
22+00	LT / RT		32
SUBTOTAL		0	32
RAMP B			
3+28	LT / RT		32
SUBTOTAL		0	32
RAMP C			
12+00	LT / RT		32
22+00	LT / RT		32
SUBTOTAL		0	64
RAMP D			
18+00	LT / RT		32
SUBTOTAL		0	32
TOTAL		270	1417

MODEL NUMBER
FILE NAME: STLS

USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALES	CHECKED -	REVISED -
PLOT DATE = \$DATES	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES			
SCALE:	SHEET 6	OF 12 SHEETS	STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1;(2HVB)BRR]	WOODFORD	71	25
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				

EROSION CONTROL (1 OF 2)																				
LOCATION				LENGTH	WIDTH	DEPTH	STONE RIPRAP CLASS B3	STONE RIPRAP CLASS B4 *	GRADING AND SHAPING DITCHES	INLET AND PIPE PROTECTION	PERIMETER EROSION BARRIER	TEMPORARY DITCH CHECKS **	TEMPORARY EROSION CONTROL SEEDING	TOP SOIL FURNISH & PLACE, 4"	SEEDING, CLASS 2A	NITROGEN FERTILIZER NUTRIENT	PHOSPHORUS FERTILIZER NUTRIENT	POTASSIAM FERTILIZER NUTRIENT	HEAVY DUTY EROSION CONTROL BLANKET	
STATION	TO	STATION	LT / RT / MEDIAN	FOOT	FOOT	INCH	TON	TON	FOOT	EACH	FOOT	FOOT	POUND	SQ YD	ACRE	POUND	POUND	POUND	SQ YD	
I-39																				
555+11			RT	20	12	16		17.78			50.00									
556+50	TO	558+50	LT	200	4	10	37.04			1.00	50.00									
569+00	TO	571+00	LT	200	4	10	37.04													
572+00			LT	10	10	10	4.63			1.00										
575+50	TO	577+00	LT	150	4	10	27.78			1.00										
576+75			RT	10	10	10	4.63			1.00										
582+00			MEDIAN	50	4	10	9.26			1.00										
582+00			RT	10	10	10	4.63			1.00										
589+50			MEDIAN	50	4				50.00			0.46	22.22	0.01	0.90	0.90	0.90		22.22	
590+50			MEDIAN	50	4	10	9.26			1.00										
590+53			LT	10	10	10	4.63			1.00										
598+00	TO	605+00	RT	700	4				700.00			56.00	6.41	311.11	0.06	5.77	5.77	5.77	311.11	
598+00			LT	10	10	10	4.63			1.00										
619+50			MEDIAN	20	4	10	3.70			1.00										
637+40			LT	10	10	10	4.63			1.00										
637+40			RT	10	10	10	4.63			1.00										
639+75			LT	10	10	10	4.63			1.00										
639+75			RT	10	10	10	4.63			1.00										
659+50			LT	10	10	10	4.63			1.00										
659+50			RT	10	10	10	4.63			1.00										
666+50	TO	667+50	RT	100	4				100.00	1.00		0.92	44.44	0.01	0.82	0.82	0.82		44.44	
682+50			MEDIAN	50	4	10	9.26			1.00										
682+50			LT	40	20	10	37.04			1.00										
682+50			RT	40	20	10	37.04			1.00										
692+50	TO	693+50	RT	100	4				100.00	1.00		0.92	44.44	0.01	0.82	0.82	0.82		44.44	
713+00			LT	10	10	10	4.63			1.00										
743+80			LT	10	10	10	4.63			1.00										
743+80			RT	10	10	10	4.63			1.00										
758+35			LT	40	20	10	37.04			1.00										
758+35			RT	40	20	10	37.04			1.00										
792+50			MEDIAN	50	4	10	9.26													
793+50			MEDIAN	50	4	10	9.26													
795+06			LT	40	20	10	37.04			1.00										
795+06			RT	40	20	10	37.04			1.00										
797+57			RT	10	10	10	4.63			1.00										
798+45			LT	10	10	10	4.63			1.00										
798+45			RT	10	10	10	4.63			1.00										
813+00			RT	10	10	10	4.63			1.00										
821+27 BK = 417+00 AH				STATION EQUATION																
SUBTOTAL							458	18	950	32	100	56	9	423	0.1	9	9	9	423	

* NO FILTER FABRIC OR BEDDING MATERIALS SHALL BE USED

** ROLLED EXCELSIOR SHALL BE USED

MODEL: \\MODEL\NAMES
FILE: \\MODEL\FILES

USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE: SHEET 7 OF 12 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	26
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				

EROSION CONTROL (2 OF 2)																			
LOCATION				LENGTH	WIDTH	DEPTH	STONE RIPRAP CLASS B3	STONE RIPRAP CLASS B4 *	GRADING AND SHAPING DITCHES	INLET AND PIPE PROTECTION	PERIMETER EROSION BARRIER	TEMPORARY DITCH CHECKS **	TEMPORARY EROSION CONTROL SEEDING	TOP SOIL FURNISH AND PLACE, 4"	SEEDING, CLASS 2A	NITROGEN FERTILIZER NUTRIENT	PHOSPHORUS FERTILIZER NUTRIENT	POTTASIAM FERTILIZER NUTRIENT	HEAVY DUTY EROSION CONTROL BLANKET
STATION	TO	STATION	LT / RT / MEDIAN	FOOT	FOOT	INCH	TON	TON	FOOT	EACH	FOOT	FOOT	POUND	SQ YD	ACRE	POUND	POUND	POUND	SQ YD
I-39																			
821+27 BK = 417+00 AH				STATION EQUATION															
423+00			RT	10	10	10	4.63			1.00									
432+60			LT	20	20	10	18.52			1.00									
438+30			LT	10	10	10	4.63			1.00									
444+97			LT	6	20	10	5.56												
448+05			MEDIAN	10	10	10	4.63			2.00									
445+10			LT	6	100	10	27.78												
445+75			RT	10	100	16		74.07											
446+00			RT	6	100	16		44.44											
447+20			LT	6	20	10	5.56												
452+95	TO	460+95	MEDIAN	800	4	10	148.15			2.00	20.00								
455+36			RT	6	25	10	6.94				10.00								
455+37			LT	6	50	10	13.89				10.00								
457+86			RT	6	50	10	13.89				10.00								
459+87			RT	6	50	10	13.89												
460+16			RT	6	50	10	13.89				50.00								
SUBTOTAL							282	119	0	7	100	0	0	0	0	0	0	0	0
I-39 / US 24 INTERCHANGE																			
RAMP A																			
22+00			LT	10	10	10	4.63			1.00									
22+00			RT	10	10	10	4.63			1.00									
SUBTOTAL							10	0	0	2	0	0	0	0	0	0	0	0	0
RAMP B																			
0+75	TO	3+25	RT	250	4	10	46.30												
SUBTOTAL							47	0	0	0	0	0	0	0	0	0	0	0	0
RAMP C																			
12+00			LT	10	10	10	4.63												
10+00	TO	12+50	RT	250	4	10	46.30			1.00									
22+00	TO	24+00	RT	200	4	10	37.04			1.00	50.00								
SUBTOTAL							88	0	0	2	50	0	0	0	0	0	0	0	0
RAMP D																			
18+00			LT	10	10	10	4.63			1.00									
18+00			RT	10	10	10	4.63			1.00									
SUBTOTAL							10	0	0	2	0	0	0	0	0	0	0	0	0
TOTAL							895	137	950	45	250	56	9	423	0.1	9	9	9	423

* NO FILTER FABRIC OR BEDDING MATERIALS SHALL BE USED
 ** ROLLED EXCELSIOR SHALL BE USED

MODEL: \\MODELS\BAMES
 FILE NAME: 31123

USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE: SHEET 8 OF 12 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1;(2HVB)BRR]	WOODFORD	71	27
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				

RECOVERABLE DELINEATORS					
LOCATION				EACH	
STATION	TO	STATION	REMARKS	WHITE	YELLOW
I-39 NORTHBOUND					
555+26	TO	573+97	CURVE #1	9	
573+97	TO	576+75		1	
576+75	TO	585+14	CURVE #2	4	
585+14	TO	622+11		9	
622+11	TO	650+83	CURVE #3	14	
650+83	TO	744+84		24	
744+84	TO	762+53	CURVE #4	9	
762+53	TO	765+38		1	
765+38	TO	783+16	CURVE #5	9	
783+16	TO	821+27		10	
821+27 BK = 417+00 AH			STATION EQUATION		
417+00	TO	426+04		2	
432+67	TO	444+92		3	
456+61	TO	461+00		1	
SUBTOTAL				96	
I-39 SOUTHBOUND					
555+56	TO	573+97	CURVE #1	9	
573+97	TO	576+75		1	
576+75	TO	585+14	CURVE #2	4	
585+14	TO	622+11		9	
622+11	TO	650+83	CURVE #3	14	
650+83	TO	744+84		24	
744+84	TO	762+53	CURVE #4	9	
762+53	TO	765+38		1	
765+38	TO	783+16	CURVE #5	9	
783+16	TO	821+27		10	
821+27 BK = 417+00 AH			STATION EQUATION		
417+00	TO	418+91		1	
430+58	TO	444+48		3	
452+71	TO	461+00		2	
SUBTOTAL				96	
I-39 / US 24 INTERCHANGE					
RAMP A					
2+86	TO	7+61		5	
7+61	TO	10+61	CURVE #1		4
10+61	TO	12+61	CURVE #2		4
12+61	TO	18+61	CURVE #3		12
18+61	TO	26+35		8	
SUBTOTAL				32	
RAMP B					
0+00	TO	5+18		5	
5+18	TO	12+08	CURVE #1		17
12+08	TO	15+08	CURVE #2		4
15+08	TO	17+20		2	
21+68	TO	25+58		4	
SUBTOTAL				32	
RAMP C					
0+00	TO	7+61		8	
7+61	TO	13+61	CURVE #1		8
13+61	TO	16+20		3	
16+20	TO	20+20	CURVE #2		6
20+20	TO	25+28		5	
SUBTOTAL				29	
RAMP D					
0+00	TO	7+76		8	
7+76	TO	12+23	CURVE #1		7
12+23	TO	14+64		2	
14+64	TO	22+43	CURVE #2		10
22+43	TO	32+93		11	
SUBTOTAL				37	
TOTAL				322	

PROPOSED GUARDRAIL											
LOCATION				LENGTH OF NEED (BEGIN / END)	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	TRAFFIC BARRIER TERMINAL, TYPE 2	TRAFFIC BARRIER TERMINAL, TYPE 6	GUARDRAIL REFLECTORS, TYPE A	TERMINAL MARKER - DIRECT APPLIED	GUARDRAIL AGGREGATE EROSION CONTROL
STATION	TO	STATION	LT / RT *	STATION	EACH	FOOT	EACH	EACH	EACH	EACH	TON
I-39 NORTHBOUND											
555+11	TO	555+99	RT	555+87 (END)		37.5	1	1	1		12.6
821+27 BK = 417+00 AH				STATION EQUATION							
445+35	TO	448+23	RT	445+43 (BEGIN)	1	200.0		1	4	1	41.6
445+48	TO	448+23	LT	445+61 (BEGIN)	1	187.5		1	4	1	40.0
452+71	TO	460+21	RT	460+09 (END)		700.0	1	1	10		106.0
SUBTOTAL					2	1125	2	4	19	2	201
I-39 SOUTHBOUND											
821+27 BK = 417+00 AH				STATION EQUATION							
444+98	TO	448+23	RT	444+11 (END)		275.0	1	1	4		70.0
452+71	TO	455+71	LT	455+59 (BEGIN)	1	212.5		1	4	1	43.4
452+71	TO	460+71	RT	460+59 (BEGIN)	1	712.5		1		1	118.0
SUBTOTAL					2	1200	1	3	8	2	232
TOTAL					4	2325	3	7	27	4	433

* LEFT AND RIGHT IS RELATED TO THE DIRCTION OF TRAFFIC

DELINEATOR REMOVAL	
LOCATION	EACH
I-39	6
I-39 / US 24 RAMP A	4
I-39 / US 24 RAMP A	3
I-39 / US 24 RAMP A	5
I-39 / US 24 RAMP A	9
TOTAL	27

GUARDRAIL REMOVAL				
LOCATION				LENGTH
STATION	TO	STATION	LT / RT *	
I-39 NORTHBOUND				
555+11	TO	556+78	RT	167
821+27 BK = 417+00 AH				
445+39	TO	448+23	LT	284
445+39	TO	448+23	RT	284
452+71	TO	460+15	RT	744
SUBTOTAL				1479
I-39 SOUTHBOUND				
821+27 BK = 417+00 AH				
444+97	TO	448+23	RT	326
452+71	TO	455+77	LT	306
452+71	TO	460+66	RT	795
SUBTOTAL				1427
TOTAL				2906

* LEFT AND RIGHT IS RELATED TO THE DIRECTION OF TRAFFIC

STAGING SCHEDULE								
LOCATION	TEMPORARY CONCRETE BARRIER	RELOCATE TEMPORARY CONCRETE BARRIER	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3	PAVEMENT MARKING TAPE, TYPE IV 4"	TEMPORARY PAVEMENT MARKING REMOVAL	PAVEMENT MARKING REMOVAL - WATER BLASTING	PORTLAND CEMENT CONCRETE SURFACE REMOVAL 1 1/2"
	FOOT	FOOT	EACH	EACH	FOOT	SQ FT	SQ FT	SQ YD
I-39 NORTHBOUND (SN 102-0059)	650	650	1	1	7500	2500	1030	142
I-39 SOUTHBOUND (SN 102-0058)	650	650	1	1	8475	2825	1205	117
US 24 EBL	225		1					
TOTAL	1525	1300	3	2	15975	5325	2235	259

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

SCALE: SHEET 9 OF 12 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1;(2HVB)BRR]	WOODFORD	71	28
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				

SHORT TERM PAVEMENT MARKINGS *							
LOCATION			LENGTH	SKIP DASH	SHOULDER DIAGONALS	SHORT TERM PAVEMENT MARKING	SHORT TERM PAVEMENT MARKING REMOVAL
STATION	TO	STATION	FOOT	FOOT	FOOT	FOOT	SQ FT
I-39 NORTHBOUND							
555+26	TO	581+82	2656	266	212	956	159
581+82	TO	821+27	23945	2395	1916	11015	1437
821+27 BK = 417+00 AH							
417+00	TO	448+23	3123	312	250	1437	187
448+23	TO	452+71	448	45	36	81	27
452+71	TO	461+00	829	83	66	381	50
SUBTOTAL						13869	1860
I-39 SOUTHBOUND							
555+56	TO	581+82	2626	263	210	945	158
581+82	TO	821+27	23945	2395	1916	11015	1437
821+27 BK = 417+00 AH							
417+00	TO	448+23	3123	312	250	1437	187
448+23	TO	452+71	448	45	36	81	27
452+71	TO	461+00	829	83	66	381	50
SUBTOTAL						13859	1858
I-39 / US 24 INTERCHANGE							
RAMP A							
0+00	TO	26+35	2635		105	211	35
SUBTOTAL						211	35
RAMP B							
0+00	TO	25+58	2558		102	205	34
SUBTOTAL						205	34
RAMP C							
0+00	TO	25+28	2528		101	202	34
SUBTOTAL						202	34
RAMP D							
0+00	TO	32+93	3293		132	263	44
SUBTOTAL						263	44
TOTAL						28609	3865

* 3 APPLICATIONS ON MAINLINE AND 2 APPLICATION ON RAMPS AND SHOULDERS

TEMPORARY PAVEMENT MARKINGS								
LOCATION			4"	6"	8"	12"	24"	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS
STATION	TO	STATION	FOOT	FOOT	FOOT	FOOT	FOOT	SQ FT
I-39 NORTHBOUND								
555+26	TO	821+27	53202	6651				
821+27 BK = 417+00 AH								
417+00	TO	426+04	1808	226				
426+04	TO	428+75	271	68				
428+75	TO	432+67	392	98				
432+67	TO	444+92	2450	307				
444+92	TO	448+74	382	96				
448+74	TO	456+61	787	197				
456+61	TO	461+00	878	110				
SUBTOTAL			60170	7753	0	0	0	0
I-39 SOUTHBOUND								
555+56	TO	821+27	53142	6643				
821+27 BK = 417+00 AH								
417+00	TO	418+91	382	48				
418+91	TO	426+59	768	192				
426+59	TO	430+58	399	100				
430+58	TO	444+48	2780	348				
444+48	TO	448+18	370	93				
448+18	TO	451+09	291	73				
451+09	TO	461+00	1982	248				
SUBTOTAL			60114	7745	0	0	0	0
I-39 / US 24 INTERCHANGE								
RAMP A								
0+00	TO	2+91	291		97			
2+91	TO	6+61	370		740	238		
6+61	TO	26+35	3948					
INTERSECTION WITH US 24						249	104	32
SUBTOTAL			4609	0	1086	342	32	56
RAMP B								
INTERSECTION WITH US 24								11.5
0+00	TO	13+89	2778					
13+89	TO	17+71	382		764			
17+71	TO	25+58	787		262			
SUBTOTAL			3947	0	1026	0	0	12
RAMP C								
0+00	TO	2+71	271		90			
2+71	TO	6+63	392		784	188		
6+63	TO	25+28	3730					
INTERSECTION WITH US 24						249	104	32
SUBTOTAL			4393	0	1123	292	32	56
RAMP D								
INTERSECTION WITH US 24								11.5
0+00	TO	21+26	4252					
21+26	TO	25+25	399		798			
25+25	TO	32+93	768		256			
SUBTOTAL			5419	0	1054	0	0	12
TOTAL			138652	15498	4290	634	64	134

MODEL: \\MODELS\BAMES
FILE NAME: 31113

USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES			
SCALE:	SHEET 10	OF 12 SHEETS	STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	29
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				

PERMANENT PAVEMENT MARKINGS																			
LOCATION			GROOVING FOR RECESSED PAVEMENT MARKING					THERMOPLASTIC PAVEMENT MARKING					MODIFIED URETHANCE PAVEMENT MARKING			RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	RAISED REFLECTIVE PAVEMENT MARKER *		
			5"	7"	9"	13"	25"	LETTERS AND SYMBOLS	4"	6"	8"	12"	24"	LETTERS AND SYMBOLS	4"			6"	8"
STATION	TO	STATION	FOOT	FOOT	FOOT	FOOT	FOOT	SQ FT	FOOT	FOOT	FOOT	FOOT	FOOT	SQ FT	FOOT	FOOT	FOOT	EACH	EACH
I-39 NORTHBOUND																			
555+26	TO	821+27	53202	6651					53202	6651								666	666
821+27 BK = 417+00 AH																			
417+00	TO	426+04	1808	226					1808	226								23	24
426+04	TO	428+75	271	68					271	68								7	8
428+75	TO	432+67	392	98					392	98								10	10
432+67	TO	444+92	2450	307					2450	307								31	32
444+92	TO	448+23	331	83					331	83								9	10
448+23	TO	448+74	51	13										51	13				
448+74	TO	452+71	397	100										397	100				
452+71	TO	456+61	390	98					390	98								10	10
456+61	TO	461+00	878	110					878	110								11	12
SUBTOTAL			60170	7754	0	0	0	0	59722	7641	0	0	0	0	448	113	0	767	772
I-39 SOUTHBOUND																			
555+56	TO	821+27	53142	6643					53142	6643								665	666
821+27 BK = 417+00 AH																			
417+00	TO	418+91	382	48					382	48								5	6
418+91	TO	426+59	768	192					768	192								20	20
426+59	TO	430+58	399	100					399	100								10	10
430+58	TO	444+48	2780	348					2780	348								35	36
444+48	TO	448+18	370	93					370	93								10	10
448+18	TO	448+23	5	2					5	2								1	2
448+23	TO	451+09	286	72										286	72				8
451+09	TO	452+71	324	41										324	41				6
451+09	TO	461+00	1982	248					1982	248								25	26
SUBTOTAL			60438	7787	0	0	0	0	59828	7674	0	0	0	0	610	113	0	771	790
I-39 / US 24 INTERCHANGE																			
RAMP A																			
0+00	TO	2+86	286	0	144					72				286		72			15
2+86	TO	2+91	5	0	2				5	2									1
2+91	TO	6+61	370	0	740	238			370	740	238							3	37
6+61	TO	26+35	3948	0					3948										
INTERSECTION WITH US 24			0	0	249	104	32	69.4			249	104	32	55.5					
SUBTOTAL			4609	0	1135	342	32	70	4323	0	1063	342	32	56	286	0	72	3	53
RAMP B																			
INTERSECTION WITH US 24								14.4						11.5					
0+00	TO	13+89	2778						2778										
13+89	TO	17+20	331		662				331								662		
17+20	TO	17+71	51		102									51		102			
17+71	TO	21+68	397											397					
21+68	TO	25+58	390						390										
SUBTOTAL			3947	0	764	0	0	15	3499	0	0	0	0	12	448	0	764	0	0
RAMP C																			
0+00	TO	2+71	271		68				271		68								14
2+71	TO	6+63	392		784	238			392		784	238						24	40
6+63	TO	25+28	3730						3730										
INTERSECTION WITH US 24					249	104	32	69.4			249	104	32	55.5					
SUBTOTAL			4393	0	1101	342	32	70	4393	0	1101	342	32	56	0	0	0	24	54
RAMP D																			
INTERSECTION WITH US 24								14.4						11.5					
0+00	TO	21+26	4252						4252										
21+26	TO	25+25	399		798				399		798								
25+25	TO	32+93	768		192				768		192								
SUBTOTAL			5419	0	990	0	0	15	5419	0	990	0	0	12	0	0	0	0	0
TOTAL			138976	15541	3990	684	64	170	137184	15315	3154	684	64	136	1792	226	836	1565	1669

* INSTALL DOUBLE RAISED REFLECTIVE PAVEMENT MARKERS ON MAINLINE

MODEL: \\MODELS\MAMES
FILE: \\MAMES:FILES

USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALES	CHECKED -	REVISED -
PLOT DATE = \$DATES	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE: SHEET 11 OF 12 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	30
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				

MOBILIZATION	
LOCATION	L SUM
JOBSITE	1
TOTAL	1

TRAFFIC CONTROL SURVEILLANCE	
LOCATION	CAL DAYS
JOBSITE	10
TOTAL	10

TRAFFIC CONTROL AND PROTECTION, STANDARD 701406	
LOCATION	L SUM
JOBSITE	1
TOTAL	1

ENGINEER'S FIELD OFFICE, TYPE A	
LOCATION	CAL MO
JOBSITE	10
TOTAL	10

CHANGEABLE MESSAGE SIGN		
LOCATION	NUMBER OF SIGNS	CAL DAYS
I-39	2	14
SN 102-0059 RAMP B	1	30
US 24 (FOR RAMP A / D DETOUR)	2	12
US 24 (FOR RAMP B / C DETOUR)	2	12
US 24	1	5
TOTAL	7	73

TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	
LOCATION	EACH
NB I-39	2
SB I-39	2
TOTAL	4

CONSTRUCTION LAYOUT	
LOCATION	L SUM
JOBSITE	1
TOTAL	1

TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	
LOCATION	L SUM
JOBSITE	1
TOTAL	1

TRAFFIC CONTROL AND PROTECTION, STANDARD 701451	
LOCATION	L SUM
JOBSITE	1
TOTAL	1

RAILROAD PROTECTIVE LIABILITY INSURANCE	
LOCATION	L SUM
JOBSITE	1
TOTAL	1

TRAFFIC CONTROL AND PROTECTION, STANDARD 701401	
LOCATION	L SUM
JOBSITE	1
TOTAL	1

TRAFFIC CONTROL AND PROTECTION, STANDARD 701456	
LOCATION	L SUM
JOBSITE	1
TOTAL	1

MOWING *			
LOCATION	LENGTH FOOT	WIDTH FOOT	ACRE
I-39 MEDIAN	30568	32	22.46
I-39 NB AND SB	30568	30	21.05
I-39 / US 24 INTERCHANGE	7277	30	5.01
SUBTOTAL			49
TOTAL (2 MOWINGS)			98

* ONE MOWING EVERY 50 DAYS, USE 2 MOWINGS

TRAFFIC CONTROL AND PROTECTION, STANDARD 701402	
LOCATION	EACH
NB I-39	1
SB I-39	1
TOTAL	2

TRAFFIC CONTROL AND PROTECTION (SPECIAL)	
LOCATION	L SUM
US 24	1
TOTAL	1

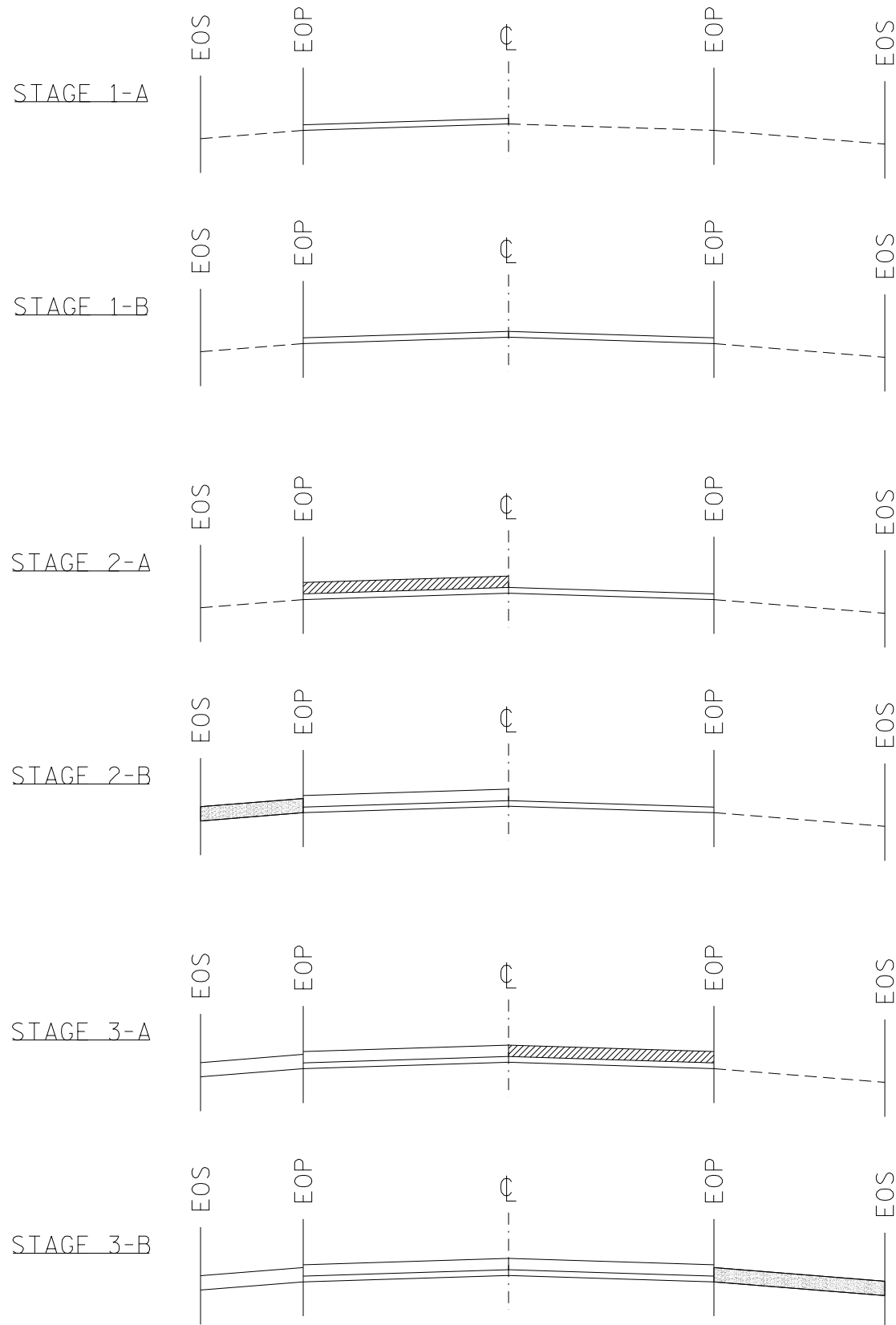
MODEL: \\MODEL\NAMES
FILE: \\MODEL\FILES

USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALES	CHECKED -	REVISED -
PLOT DATE = \$DATES	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES		
SCALE:	SHEET 12 OF 12 SHEETS	STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	31
				CONTRACT NO. 68D79
				ILLINOIS FED. AID PROJECT



PLACE POLY BIT MATERIALS (TACK COAT)
PLACE LEVELING BINDER

PLACE POLY BIT MATERIALS (TACK COAT)
PLACE LEVELING BINDER

PLACE POLY BIT MATERIALS (TACK COAT)
PLACE SMA BINDER COURSE

PLACE POLY BIT MATERIALS (TACK COAT)
PLACE HMA SHOULDER LOWER LIFT

PLACE POLY BIT MATERIALS (TACK COAT)
PLACE SMA BINDER COURSE

PLACE POLY BIT MATERIALS (TACK COAT)
PLACE HMA SHOULDER LOWER LIFT

NOTE:

THE CONTRACTOR MAY ELECT TO CHOOSE
WHAT LANES, INSIDE OR OUTSIDE, TO
BEGIN CONSTRUCTION WORK IN

	HMA SURFACE REMOVAL (SPECIAL)
	HMA LEVELING BINDER
	SMA BINDER COURSE
	SMA SURFACE COURSE
	HMA SHOULDER

MODEL: 140DELMAMES
FILE NAME: 31113

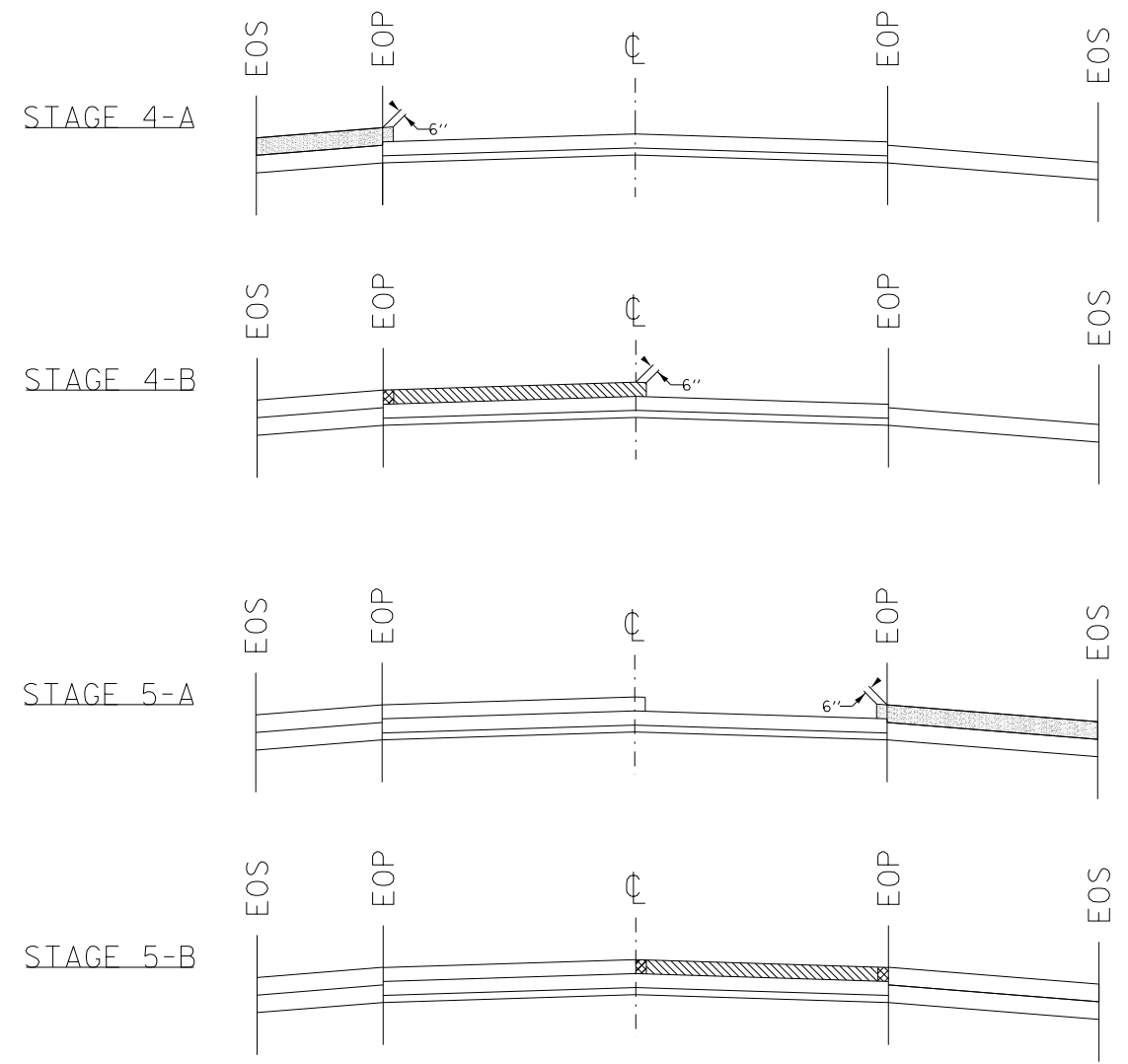
USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALES	CHECKED -	REVISED -
PLOT DATE = \$DATES	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVING SEQUENCE

SCALE: SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	32
ILLINOIS FED. AID PROJECT			CONTRACT NO. 68D79	



PLACE POLY BIT MATERIALS (TACK COAT)
PLACE HMA SHOULDER UPPER LIFT

PLACE POLY BIT MATERIALS (TACK COAT)
PLACE SMA SURFACE COURSE

PLACE POLY BIT MATERIALS (TACK COAT)
PLACE HMA SHOULDER UPPER LIFT

PLACE POLY BIT MATERIALS (TACK COAT)
PLACE SMA SURFACE COURSE

NOTE:
THE CONTRACTOR MAY ELECT TO CHOOSE
WHAT LANES, INSIDE OR OUTSIDE, TO
BEGIN CONSTRUCTION WORK IN

	HMA SURFACE REMOVAL (SPECIAL)
	HMA LEVELING BINDER
	SMA BINDER COURSE
	SMA SURFACE COURSE
	HMA SHOULDER

MODEL: 140DELMNAMES
FILE NAME: 311E13

USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

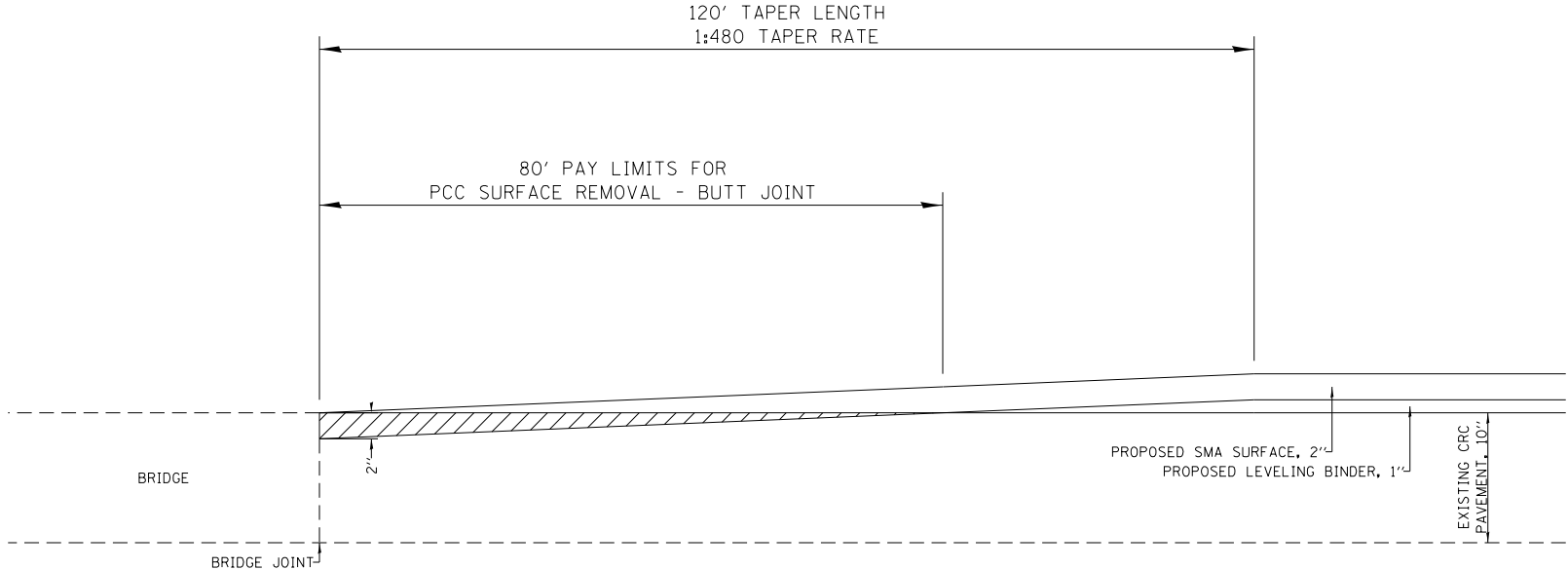
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVING SEQUENCE

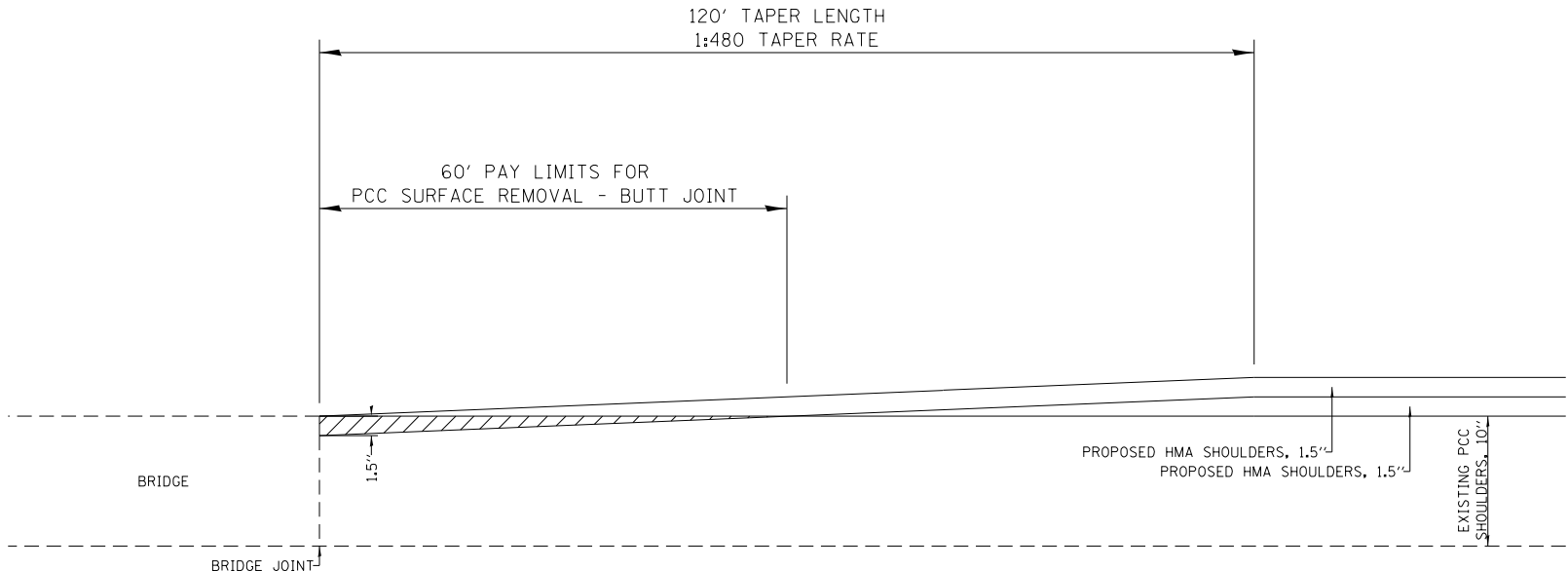
SCALE: SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	33
ILLINOIS FED. AID PROJECT			CONTRACT NO. 68D79	

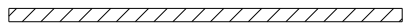
BEGINNING OF PROJECT



MAINLINE



SHOULDERS

 PCC SURFACE REMOVAL - BUTT JOINT

MODEL: 140DELMAME
FILE: 140DMS: 37115

USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

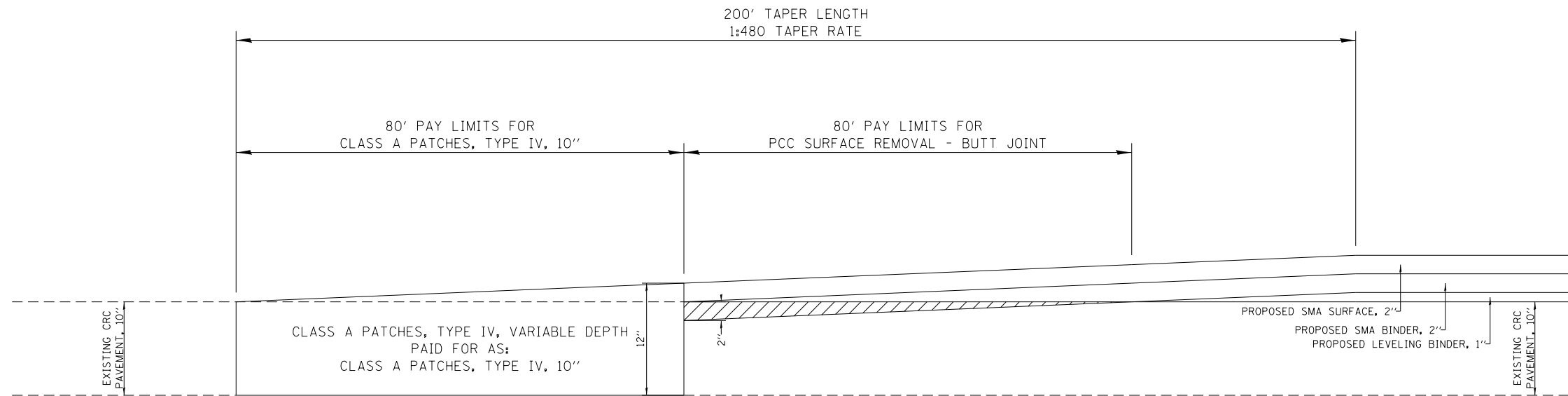
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT DETAILS

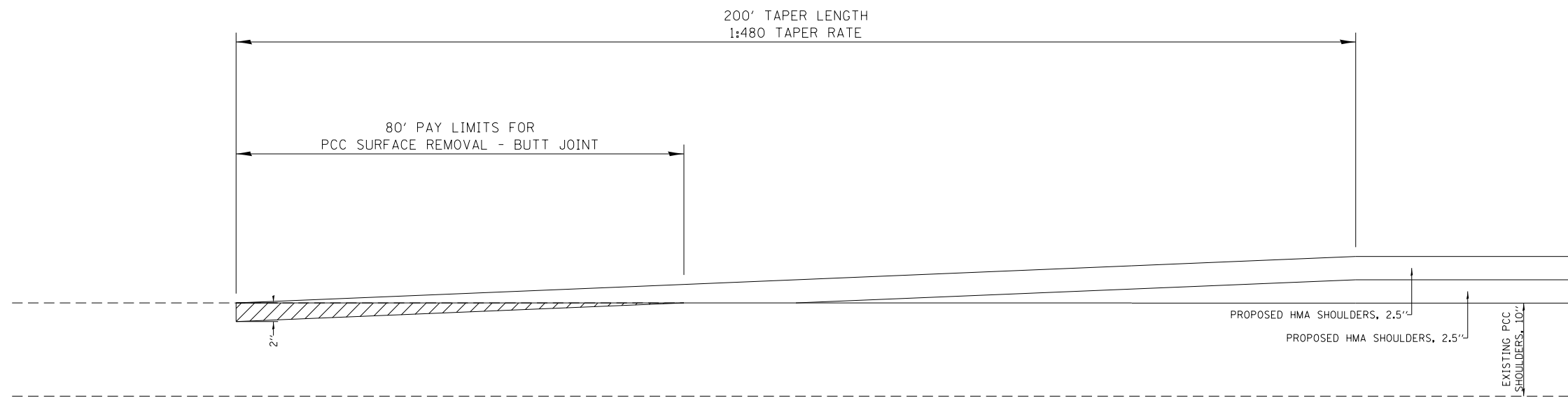
SCALE: SHEET 1 OF 5 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	34
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				

END OF PROJECT



MAINLINE



SHOULDERS



MODEL NUMBER
FILE NUMBER

USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

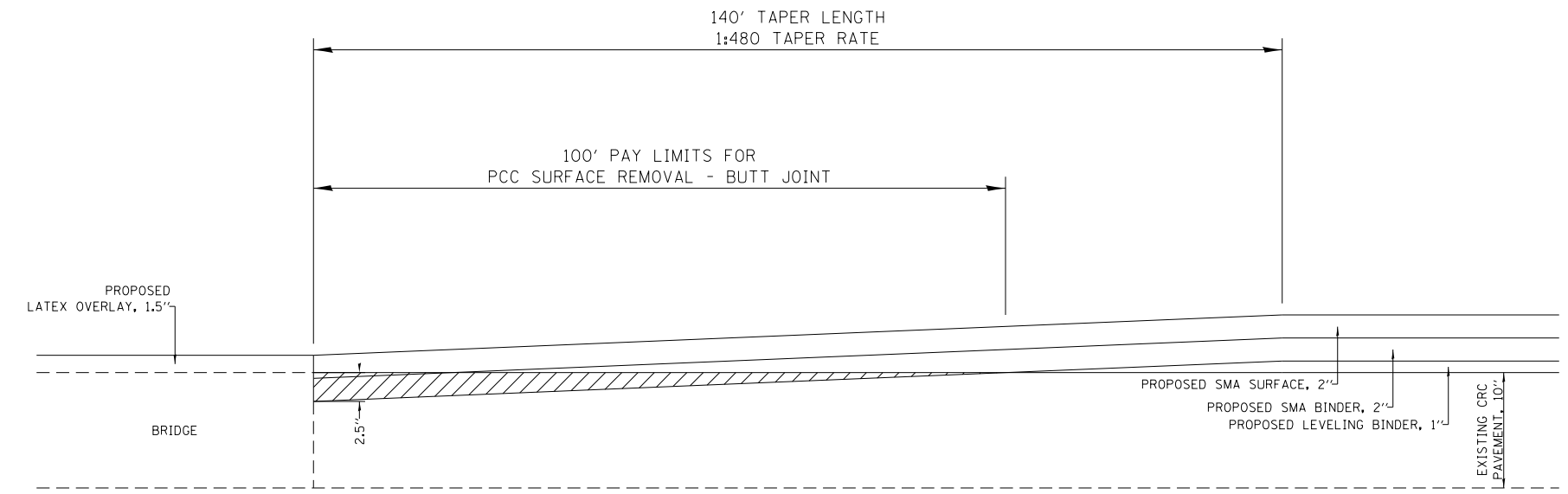
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT DETAILS

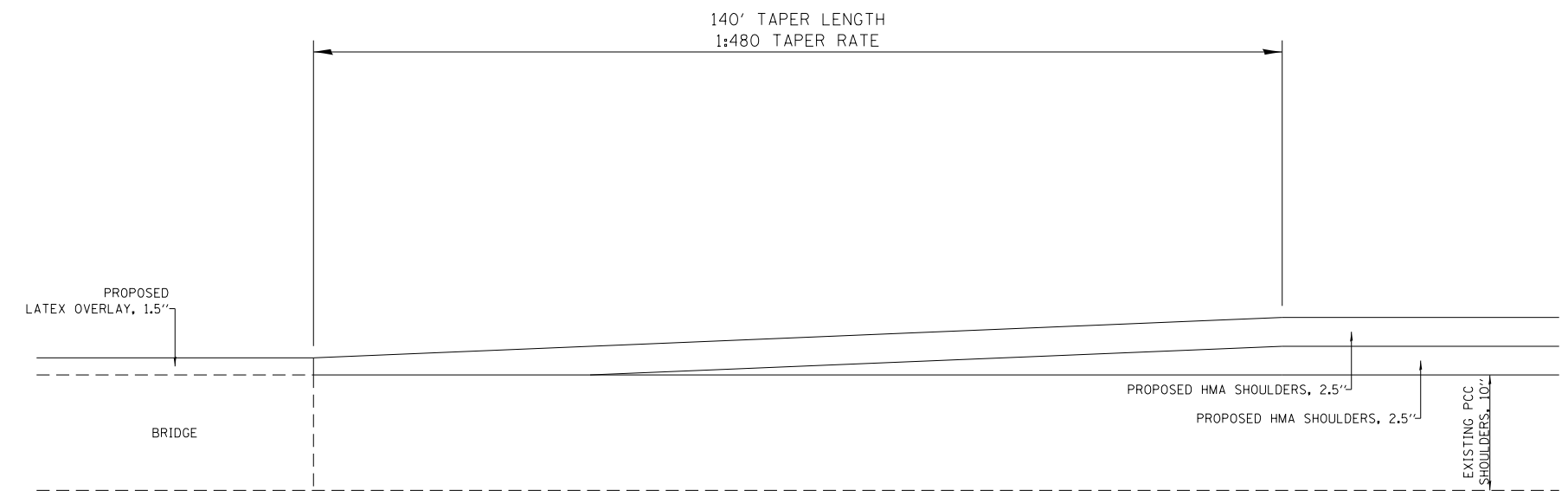
SCALE: SHEET 2 OF 5 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	35
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				

BEFORE AND AFTER BRIDGE (SN 102-0058 AND 102-0059)



MAINLINE



SHOULDERS

 PCC SURFACE REMOVAL - BUTT JOINT

MODEL: \$MODELNAME\$
FILE NAME: \$FILE\$

USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

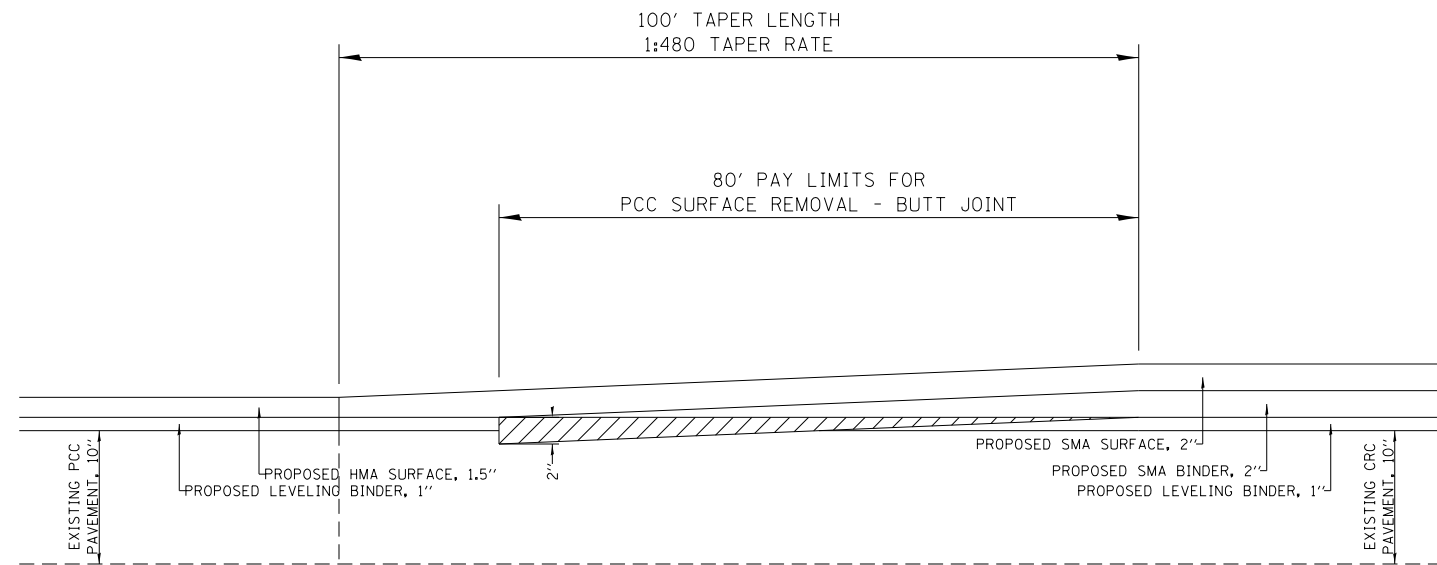
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT DETAILS

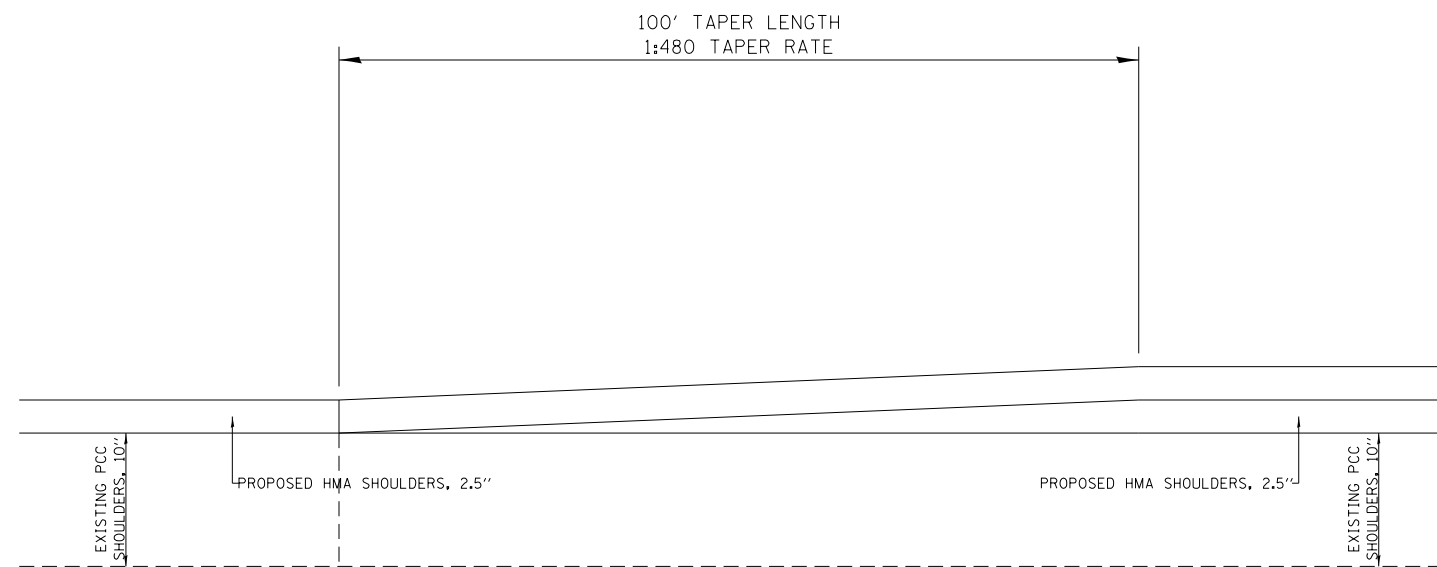
SCALE: SHEET 3 OF 5 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	36
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				

MAINLINE TO RAMP TRANSITION



MAINLINE



SHOULDERS



MODEL: \\MODELS\BMM\FILES
FILE NAME: 31113

USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

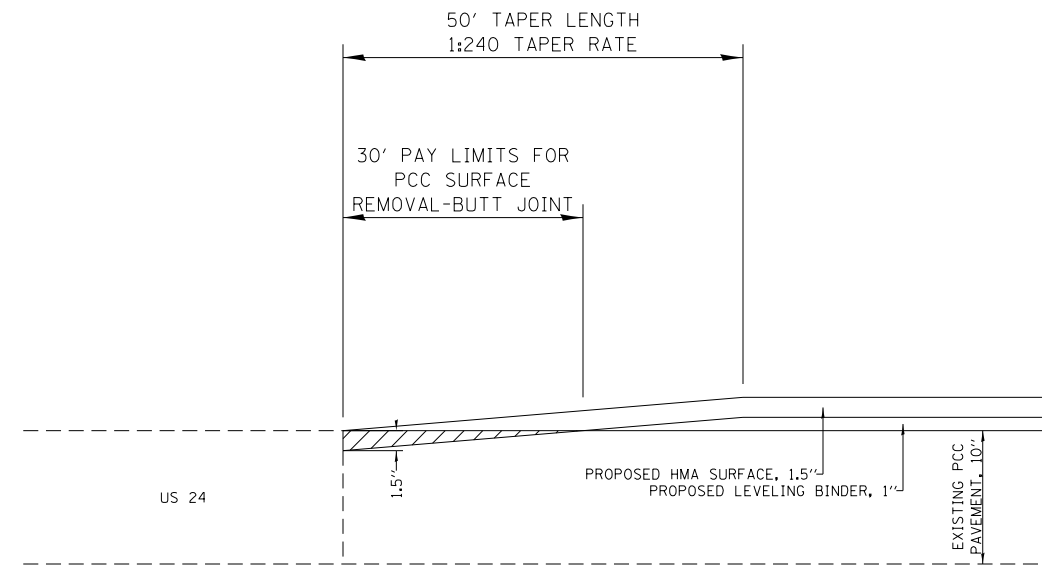
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT DETAILS

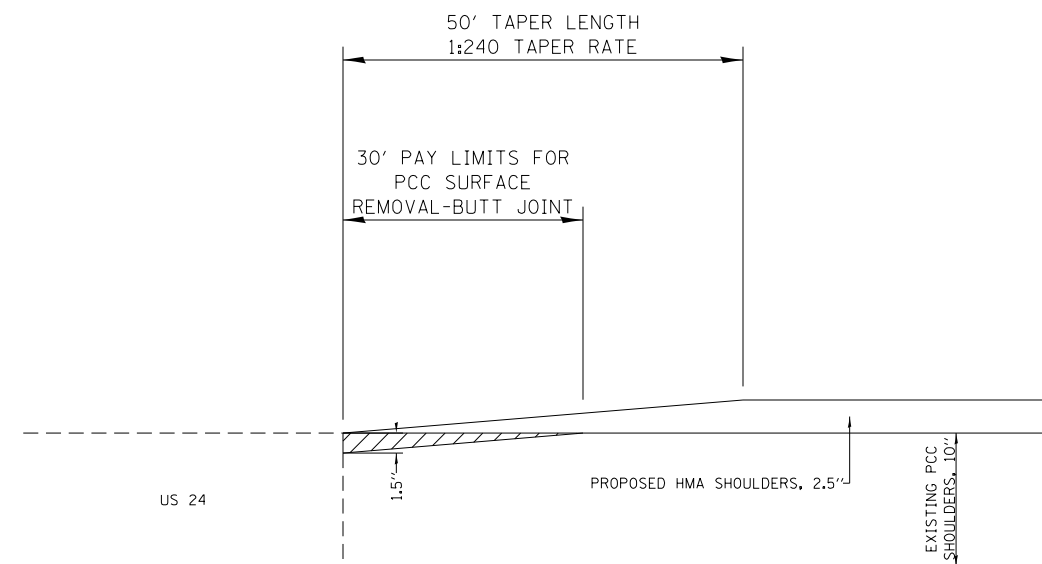
SCALE: SHEET 4 OF 5 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	37
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				

END OF RAMPS



MAINLINE



SHOULDERS

 PCC SURFACE REMOVAL - BUTT JOINT

MODEL: 140DELMAME
FILE NAME: 31115

USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

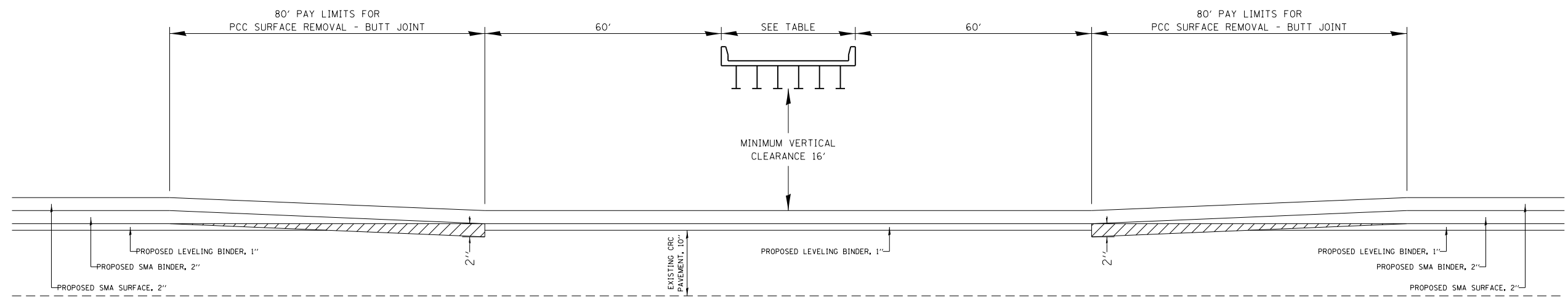
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT DETAILS

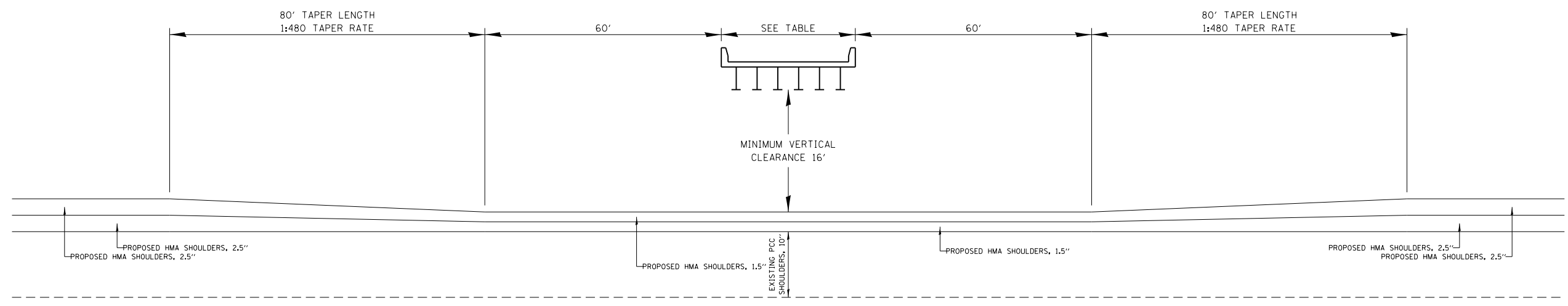
SCALE: SHEET 5 OF 5 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	38
CONTRACT NO. 68D79			ILLINOIS FED. AID PROJECT	

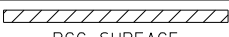
PAVEMENT OVERLAY UNDER STRUCTURES



MAINLINE



SHOULDERS

 PCC SURFACE REMOVAL - BUTT JOINT

OVERHEAD STRUCTURE WIDTH			
STRUCTURE			WIDTH
NUMBER	ROUTE CARRIED	CENTER STATION	FOOT
102-0062	TR 229	577+00	34
102-0061	CH 9	638+86	38
102-0060	TR 207	744+93	34

MODEL: SH02ELNAMES
FILE: SH02L1.SYLS

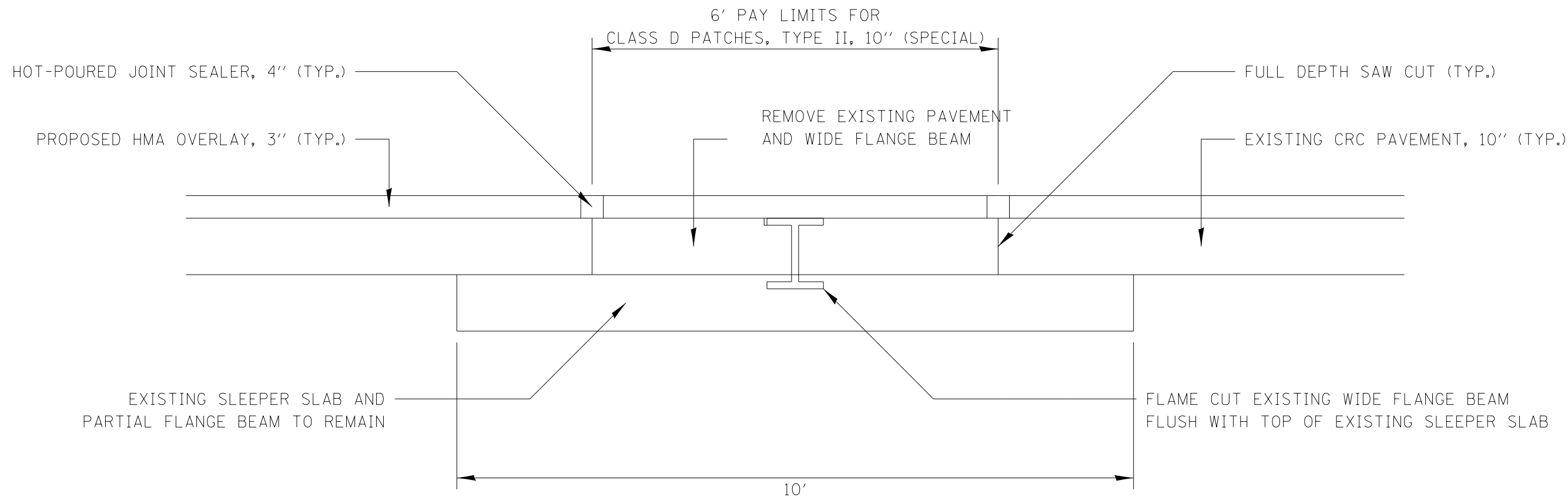
USER NAME = \$USERS	DESIGNED -	REVISED -
PLOT SCALE = \$SCALE\$	DRAWN -	REVISED -
PLOT DATE = \$DATE\$	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT OVERLAY UNDER STRUCTURES DETAIL

SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	39
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				



NOTES:

1. EXISTING WIDE FLANGE BEAM SHALL BE REMOVED AND REPLACED WITH "CLASS D PATCHES, TYPE II, 10" (SPECIAL)."
2. WORK SHALL BE DONE IN ACCORDANCE TO APPLICABLE SECTIONS OF ARTICLE 442.08 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER.
3. ALL WORK, MATERIALS, AND REMOVAL OF THE WIDE FLANGE BEAM SHALL BE INCLUDED IN THE UNIT PRICE FOR "CLASS D PATCHES, TYPE II, 10" (SPECIAL)."

MODEL: 140DELMAME\$
FILE NAME: 31113

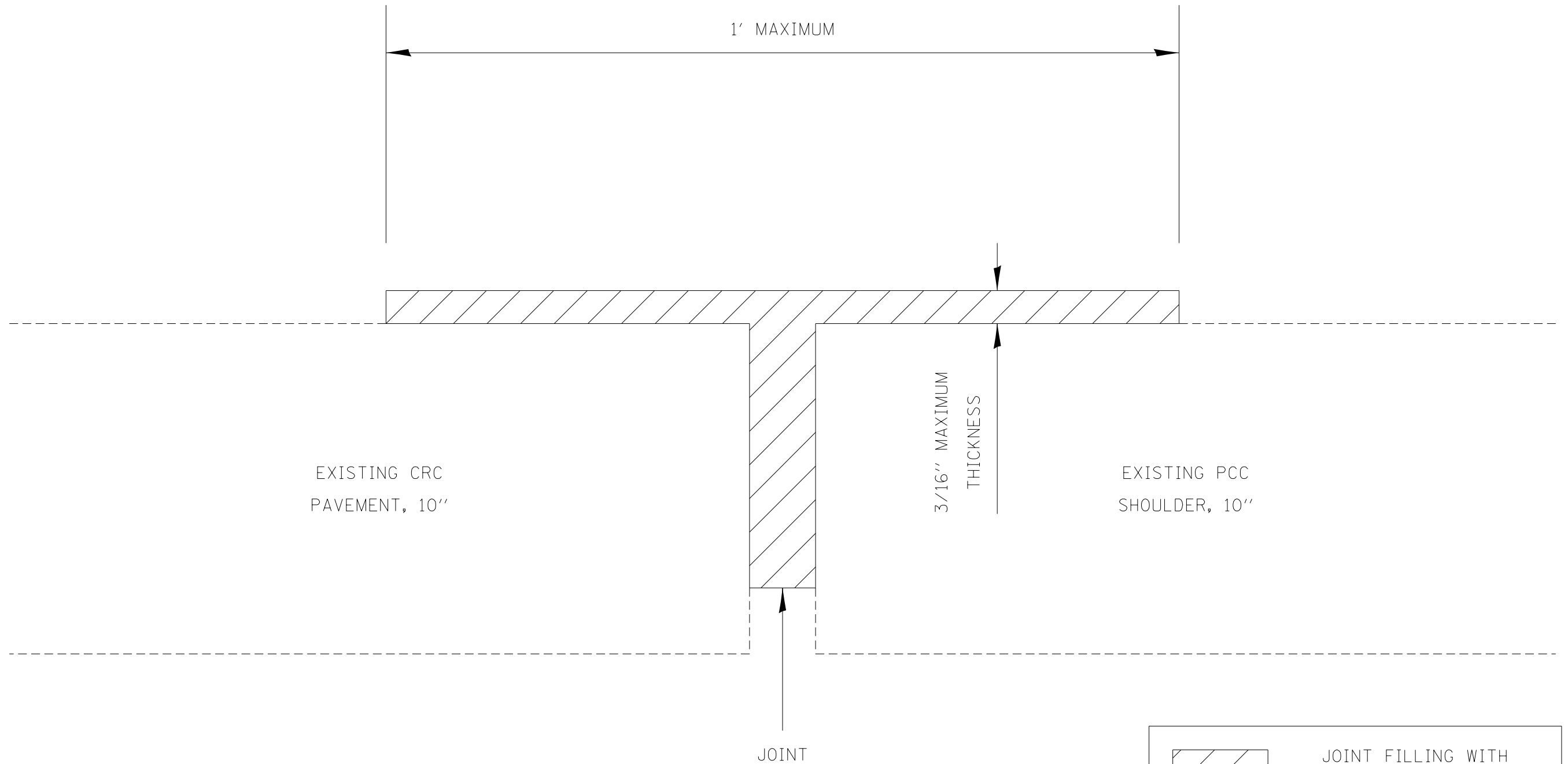
USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -


**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CLASS D PATCHES (SPECIAL) DETAIL

SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	40
			CONTRACT NO. 68D79	
		ILLINOIS	FED. AID PROJECT	



	JOINT FILLING WITH MICROSURFACING MATERIAL
---	---

MODEL: 140DELMAME
FILE: 140MAME.DWG

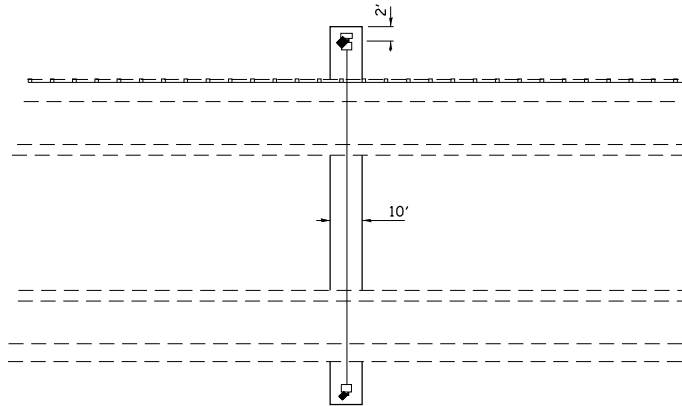
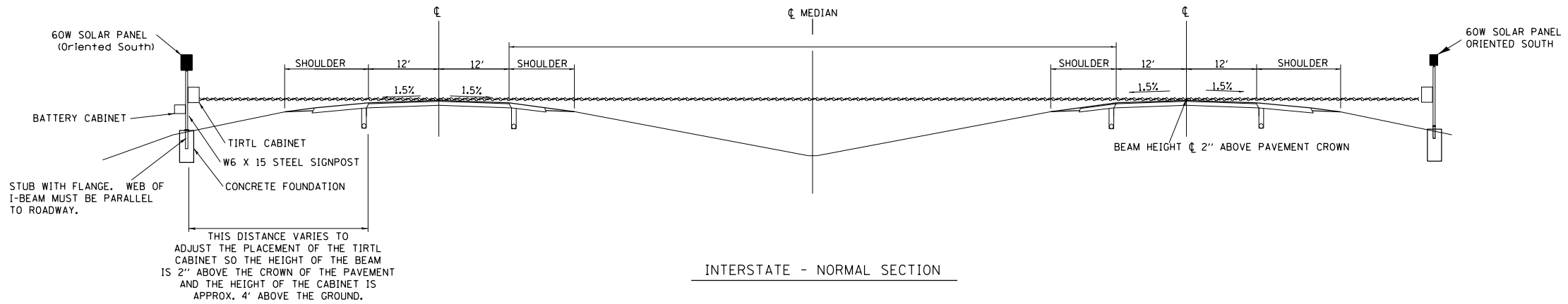
USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

JOINT FILLING DETAIL

SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	41
CONTRACT NO. 68D79			ILLINOIS FED. AID PROJECT	



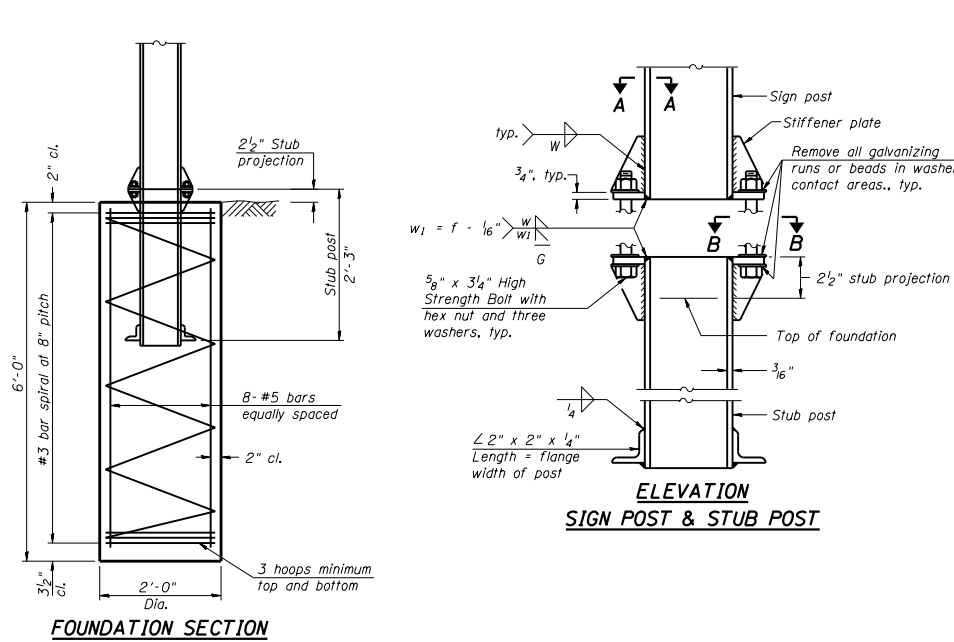
Weed Barrier - PCC concrete shall be placed as shown 8' to 10' wide and 4" thick as per the special provision

NOTES:

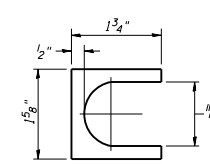
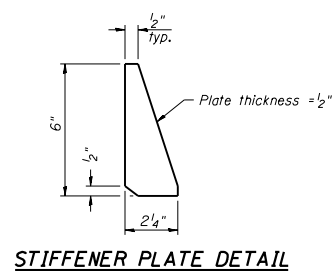
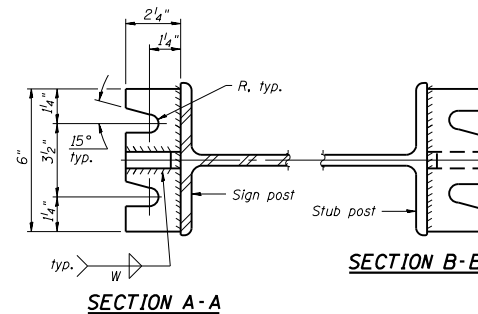
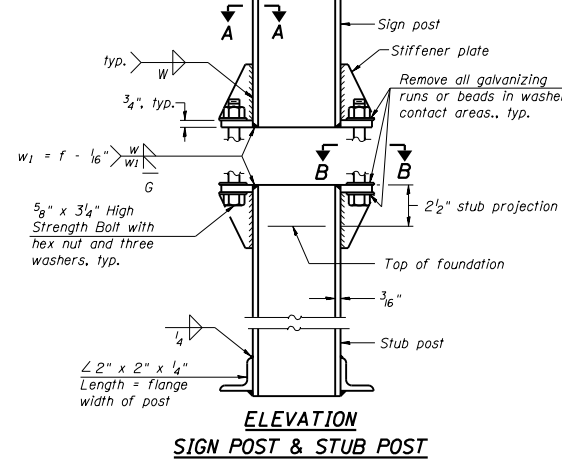
1. SYSTEM CONSISTS OF TWO W6 X 15 I-BEAM SIGNPOSTS WITH CONCRETE FOUNDATION AND A FLANGE WITH BREAKAWAY BOLTS
2. THE I-BEAMS ARE 12' LONG AND DRILLED AS PER THE I-BEAM DETAIL TO ACCOMODATE A PIPE TO ALLOW THE MOUNTING OF THE SOLAR PANEL.
3. THE WEB OF THE I-BEAM IS TO BE PLACED PARALLEL TO THE LANES.
4. THE TWO POSTS MUST BE PLACED DIRECTLY ACROSS FROM EACH OTHER AND PERPENDICULAR TO THE LANES.
5. TWO POSTS ARE REQUIRED LOCATED AS PER THE ENGINEER AND DRAWING.
6. THE CABINETS ARE ATTACHED TO THE I-BEAM USING 8" GALVANIZED J-BOLTS.
7. THE BOTTOM OF THE CABINET HEIGHT MUST BE NO LOWER THAN 4' ABOVE THE GROUND AND BE ABLE TO VIEW ACROSS ALL LANES AT THE CROWN OF THE PAVEMENT.
8. THE IDOT OFFICE OF PLANNING AND PROGRAMMING DATA MANAGEMENT LAB RICH MARX (217-782-2065) SHALL BE NOTIFIED TWO WEEKS PRIOR TO THE LAYOUT AND SHALL BE PRESENT DURING THE PLACEMENT OF THE POST FOUNDATIONS.

TIRTL TRAFFIC COUNTER SCHEDULE OF QUANTITIES (FOR INFORMATION ONLY)

CODE NUMBER	ITEM	UNIT	QUANTITY
72700100	STRUCTURAL STEEL SIGN SUPPORT - BREAKAWAY	POUND	520
73400100	CONCRETE FOUNDATIONS	CU YD	1.4
X0323388	TRAFFIC COUNTER SYSTEM (INCLUDES BELOW)	EACH	1
TIRTL TRAFFIC DATA COLLECTION SYSTEM INCLUDES:			
	Sierra Wireless Modem RV-50	EACH	1
	Panorama Low Profile LTE MIMO LGAM-BC3G-26-3SP Antenna	EACH	1
	TIRTL CABINET 15" X 27" X 16"	EACH	2
	BATTERY CABINET 16.5" X 16" X 11.5" BBA1M w/ #2 Police Lock	EACH	1
	SOLAR PANEL 60 W 12 VDC with Bracket and Junction box	EACH	2
	TIRTL TRAFFIC COUNTER	EACH	1
	SUNSAVER SOLAR CHARGE REGULATOR 10 AMP 12 VDC	EACH	2
	BATTERY - 33 AH ABSORBED ELECTROLYTE 8" X 5.5" X 7" (DEEP CYCLE)	EACH	1
	BATTERY - 104AH ABSORBED ELECTROLYTE 10" X 6" X 8" (DEEP CYCLE)	EACH	1



BREAK-AWAY WIDE FLANGE STEEL POST DETAILS



SHIM DETAIL
Furnish two 0.01" thick and two 0.03" thick stainless steel or brass (ASTM B36) shims per post.

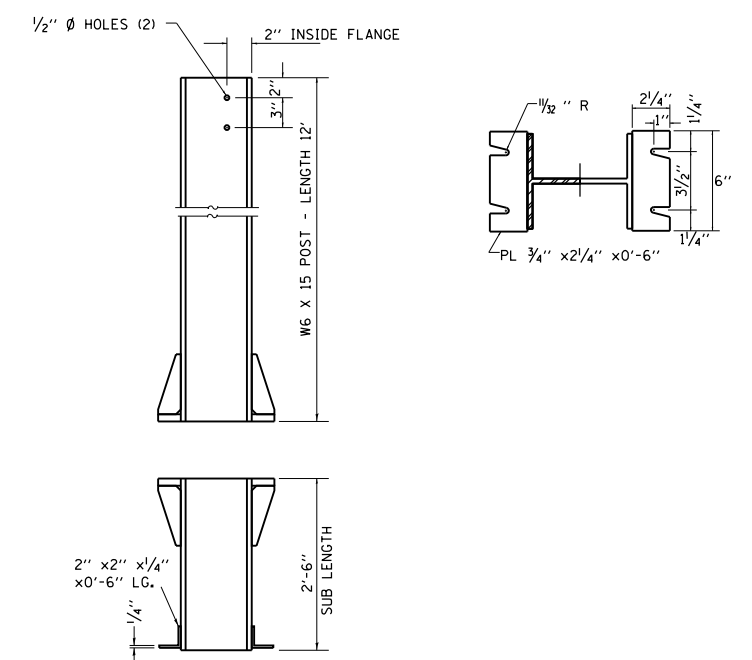
GENERAL NOTES

Posts shall be plumbed by using shims with post-to-stub post connection bolts snug tight only. Final tightening of all High Strength Bolts shall be in accordance with Article 727.05 and threads at the junction of the bolt and nut shall be burred or center punched to prevent the nut from loosening.

LOADING: 80 m.p.h. wind with 30% gust factor, normal to sign.

DESIGN STRESSES:
Structural steel - 20,000 p.s.i.
Reinforcing steel - 20,000 p.s.i.
Concrete - 1,400 p.s.i.
Footing soil pressure - 2,000 p.s.f.

After fabrication, the post and upper 6", min. of the stub post shall be hot-dip galvanized in accordance with AASHTO M111. All bolts, nuts and washers shall be hot-dip galvanized in accordance with AASHTO M232.



MODEL: SMOBELMAMES
FILE NUMBER: STILES

USER NAME = SUSERS	DESIGNED -	REVISED -
PLOT SCALE = SSCALE\$	DRAWN -	REVISED -
PLOT DATE = SDATES	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC COUNTER DETAIL			
SCALE:	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1;(2HVB)BRR]	WOODFORD	71	42
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				

US 24

THERMOPLASTIC PAVEMENT MARKING, 12" (TYP)

THERMOPLASTIC PAVEMENT MARKING, 24" (TYP)

LEFT TURN ARROW
THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS

RIGHT TURN ARROW
THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS

THRU ARROW
THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS

THERMOPLASTIC PAVEMENT MARKING, 8" (TYP)

RAMPS B / D

RAMPS A / C

WRONG WAY ARROW
THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS

16.00'
(TYP)

16.00'
(TYP)

MODEL: \$MODELNAME\$
FILE: \$NAME\$. \$FILE\$

USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

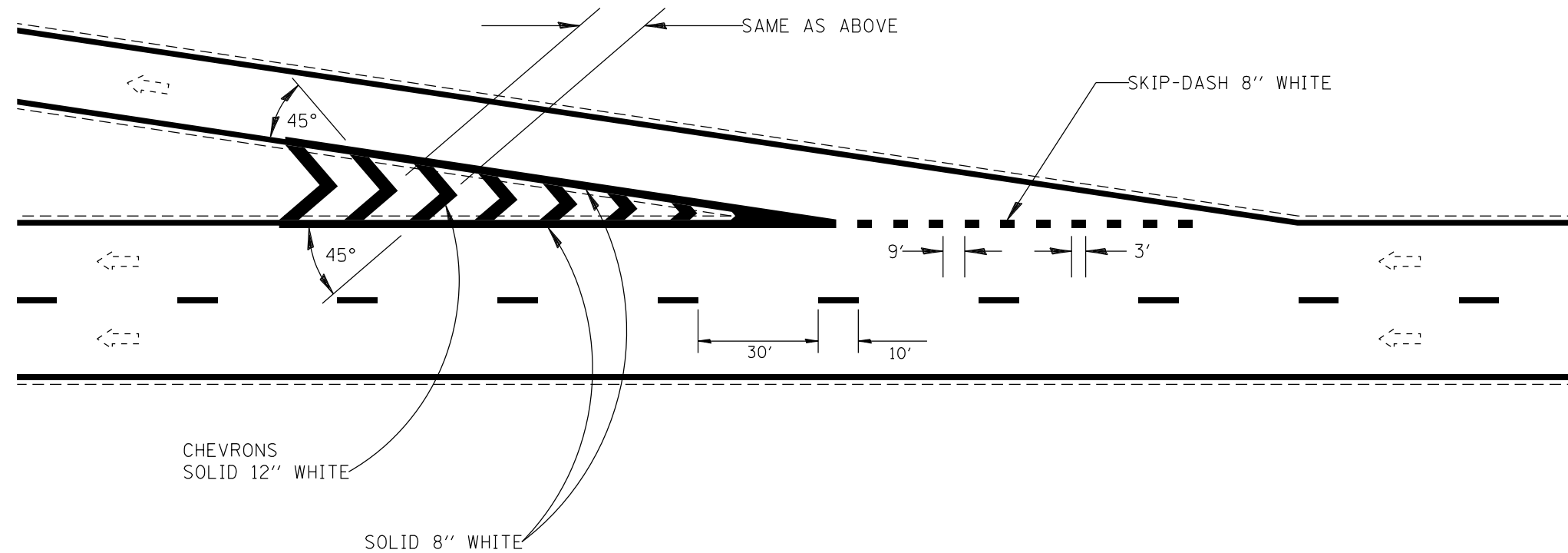
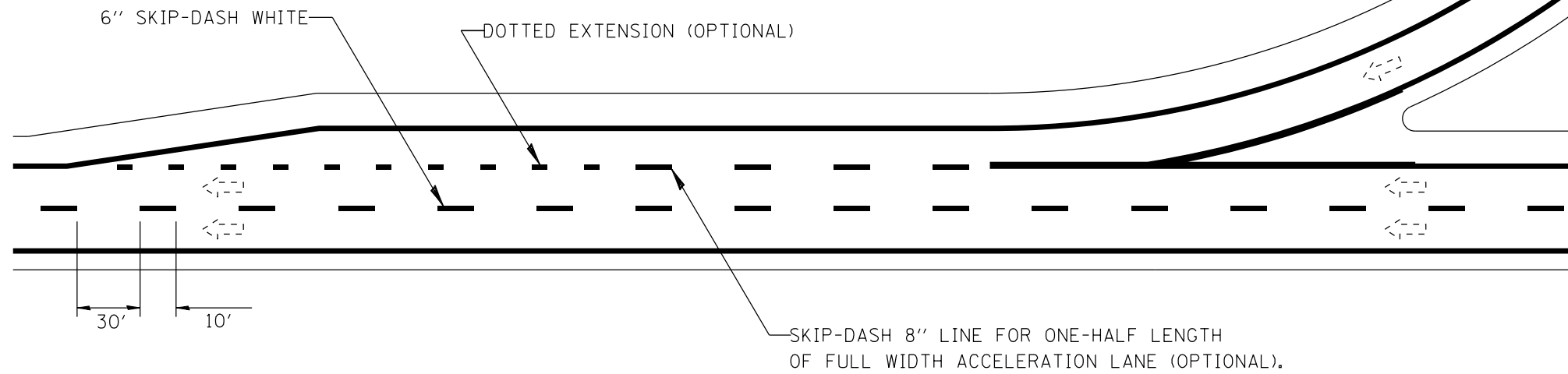
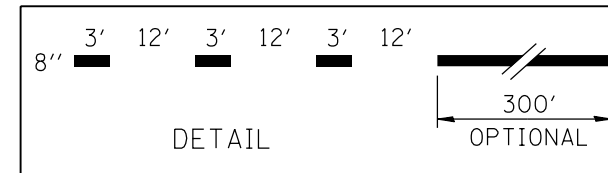
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING DETAILS

SCALE: SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	43
CONTRACT NO. 68D79			ILLINOIS FED. AID PROJECT	

PARALLEL ACCELERATION LANE



GORE MARKINGS AND CHANNELIZING LINES

MODEL: 140DELMARIES
FILE: 140MAR_21.E15

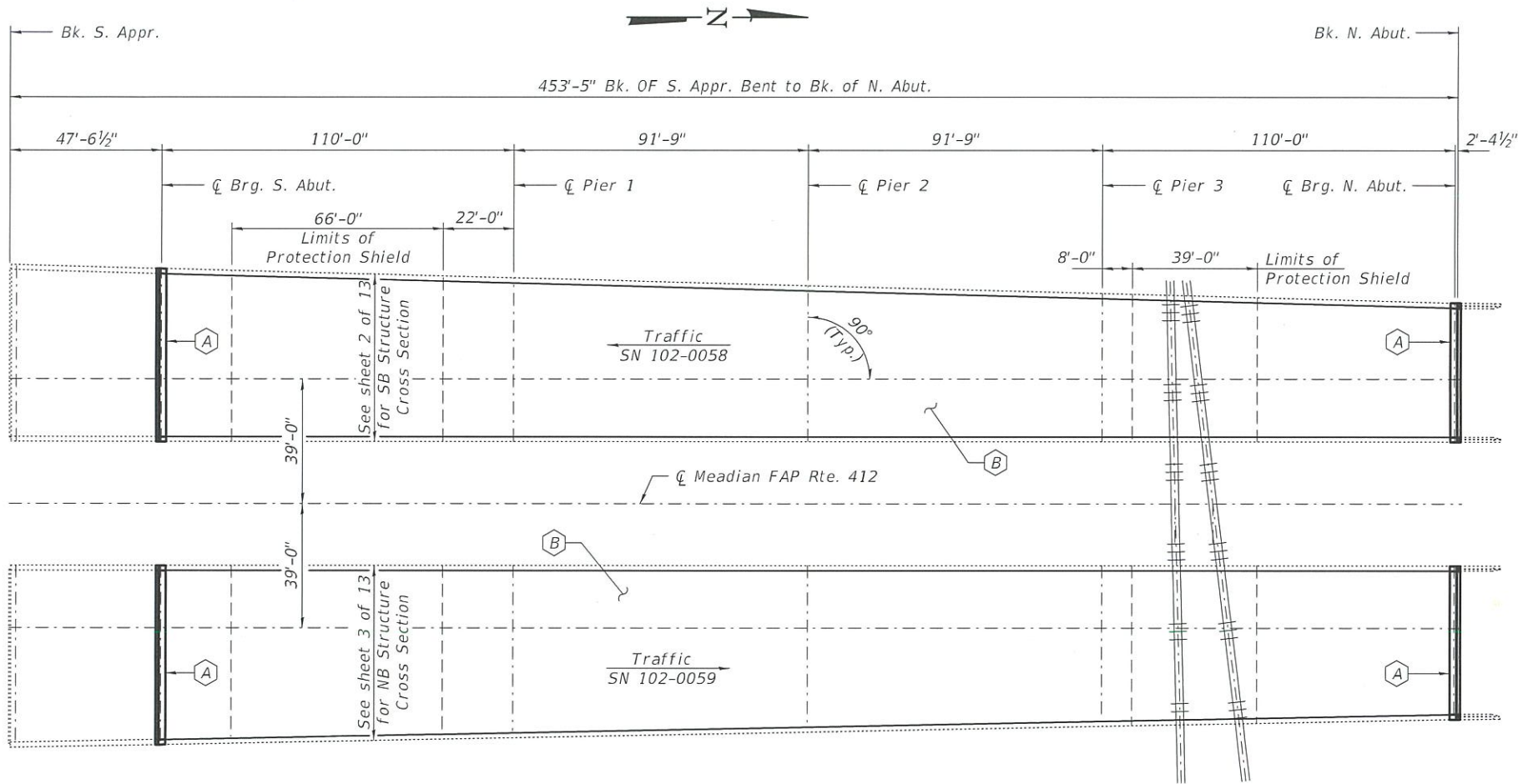
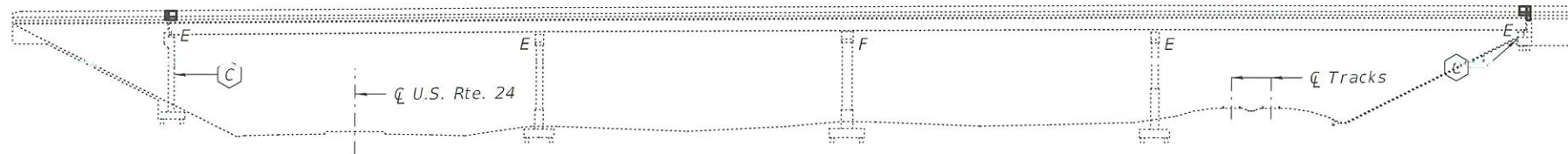
USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING DETAILS

SCALE: SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	44
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				



PLAN

- (A) - Remove and Replace Expansion Joints at Abutments and Remove and Replace bearings with Elastomeric Bearings.
 - (B) - Install a new 2 1/4" Latex Overlay on the existing deck. (See sheet 2 & 3 of 13 for details.)
 - (C) - Structural Repair of Concrete at both Abutments.
- For Beam End Repairs, see sheet 10 of 13.



EXPIRES 11-30-2020

DESIGNED <i>Victor H. Veliz</i>	EXAMINED <i>Tingting Li</i>	DATE - NOVEMBER 30, 2018
CHECKED <i>Victor H. Veliz</i>	PASSED <i>Victor H. Veliz</i>	REVISIONS
DRAWN <i>daburdell</i>		REVISIONS
CHECKED <i>CDK</i>		REVISIONS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN AND ELEVATION
FAP 412 OVER US 24 & ATCHISON TOPEKA AND SANTA FE RR
SN 102-0058 (SB) & 0059 (NB)

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
412	1021RS.2RS-1; (2HV8)BRR	WOODFORD	71	45
CONTRACT NO. 68D79			ILLINOIS FED. AID PROJECT	

GENERAL NOTES

All structural steel shall conform to AASHTO Classification M-270 Gr. 36, unless otherwise noted.

Reinforcement bars designated (E) shall be epoxy coated.

Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50° F.

Cost of removal and re-installation of all members necessary to complete the work as detailed on the plans and as specified in the Special Provisions shall be included with Furnishing and Erecting Structural Steel.

Existing structural steel that will be in contact with new structural steel shall be cleaned and painted prior to erection as required by the special provision "Cleaning and Painting Contact Surface Areas of Existing Steel Structures".

Synthetic fibers shall be added to the Bridge Deck Latex Concrete Overlay. See Special Provisions.

Fasteners shall be high strength bolts. Bolts 3/4"Ø, open holes 13/16"Ø, unless otherwise noted.

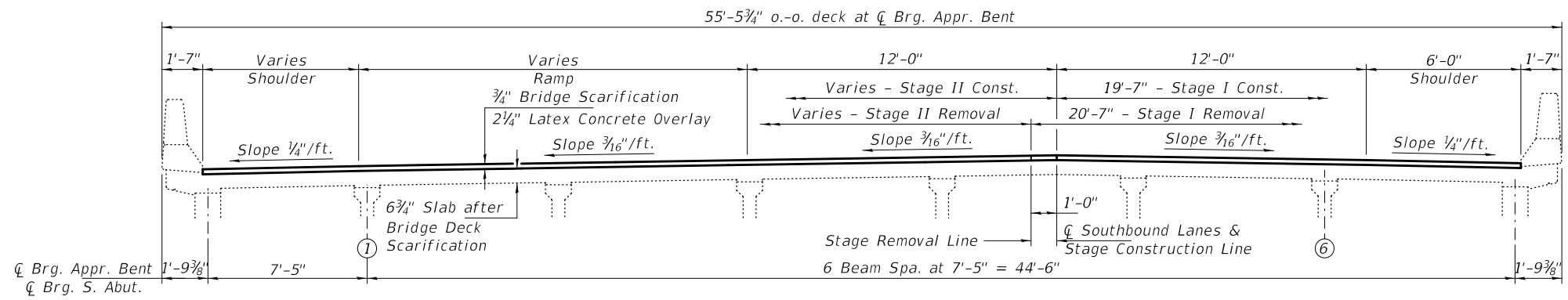
New bearing plates, steel extensions, shim plates, side retainers, anchor bolts, connection bolts, nuts and washers shall be hot dip galvanized according to Special Provision "Hot Dip Galvanizing for Structural Steel".

The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.

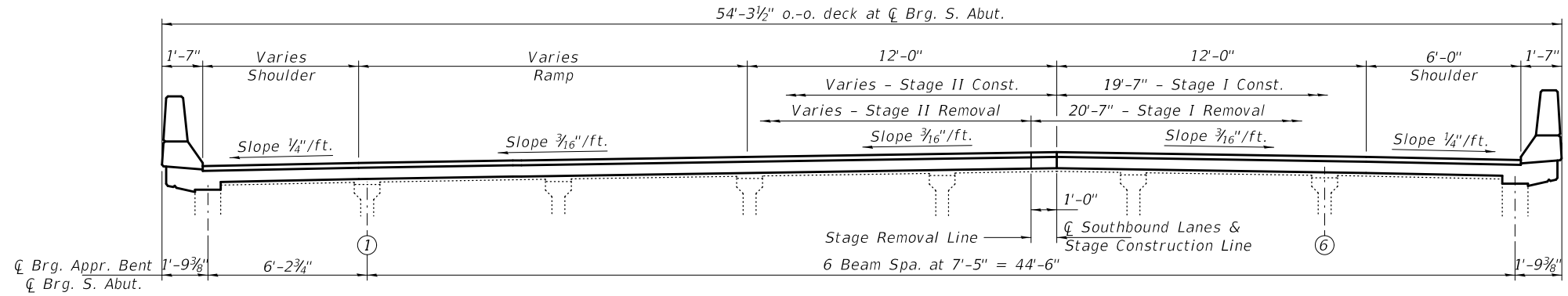
TOTAL BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Concrete Removal	Cu. Yd.	21.8
Concrete Superstructure	Cu. Yd.	21.8
Preformed Joint Strip Seal	Foot	198
Reinforcement Bars, Epoxy Coated	Pound	2860
Bar Splicers	Each	44
* Protective Coat	Sq. Yd.	71.1
Bridge Deck Scarification, 3/4"	Sq. Yd.	4752.3
Bridge Deck Latex Concrete Overlay, 2 1/4"	Sq. Yd.	4752.3
Elastomeric Bearing Assembly, Type I	Each	32
Anchor Bolts, 1"Ø	Each	64
Jack and Remove Existing Bearings	Each	32
Structural Repair of Concrete (Depth ≤ 5")	Sq. Ft.	72
Furnishing and Erecting Structural Steel	Pounds	1730
Bridge Deck Grooving	Sq. Yd.	4614.7
Protective Shield	Sq. Yd.	1191.5
Structural Steel Repair	Pound	1260

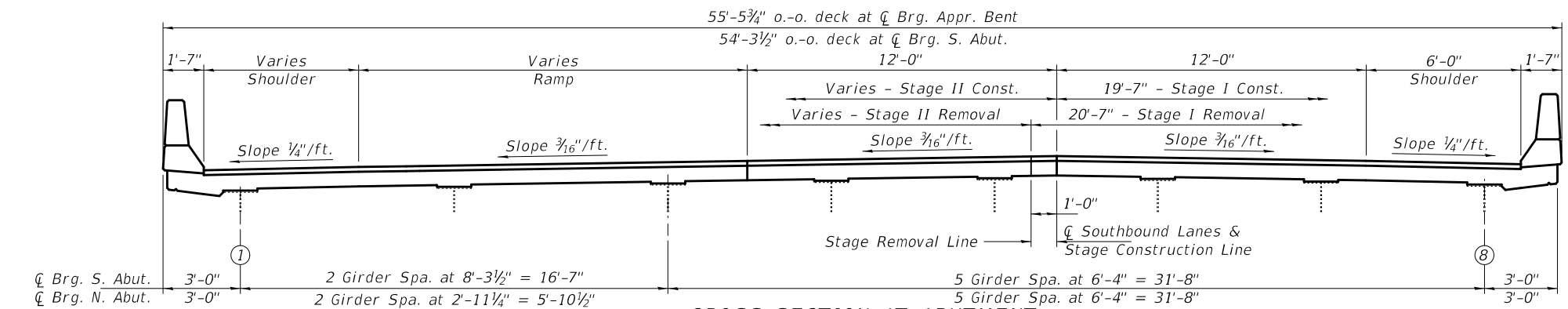
* On new concrete only.



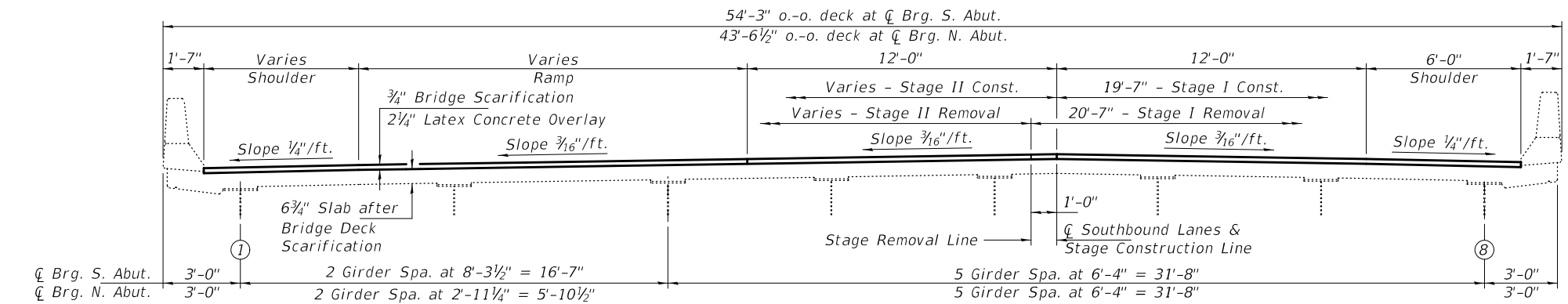
CROSS SECTION AT SOUTH APPROACH BENT
(Looking North)



CROSS SECTION AT SOUTH APPROACH ABUTMENT
(Looking South)





CROSS SECTION AT ABUTMENT
(Looking North)



CROSS SECTION AT MIDSPAN
(Looking North)

DESIGNED - CDK
CHECKED - VHV
DRAWN - daburdell
CHECKED - CDK VHV

EXAMINED
PASSED


 ENGINEER OF STRUCTURAL SERVICES
 ENGINEER OF BRIDGES AND STRUCTURES

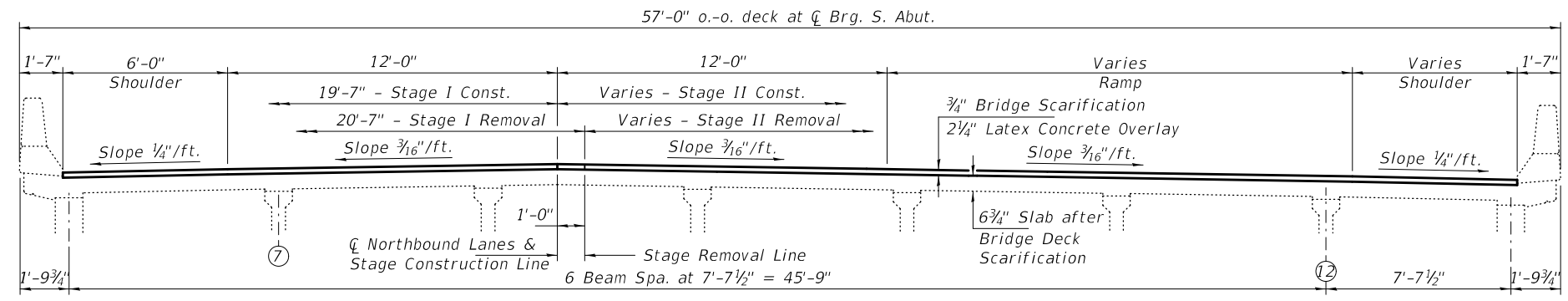
DATE - NOVEMBER 30, 2018
 REVISED -
 REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

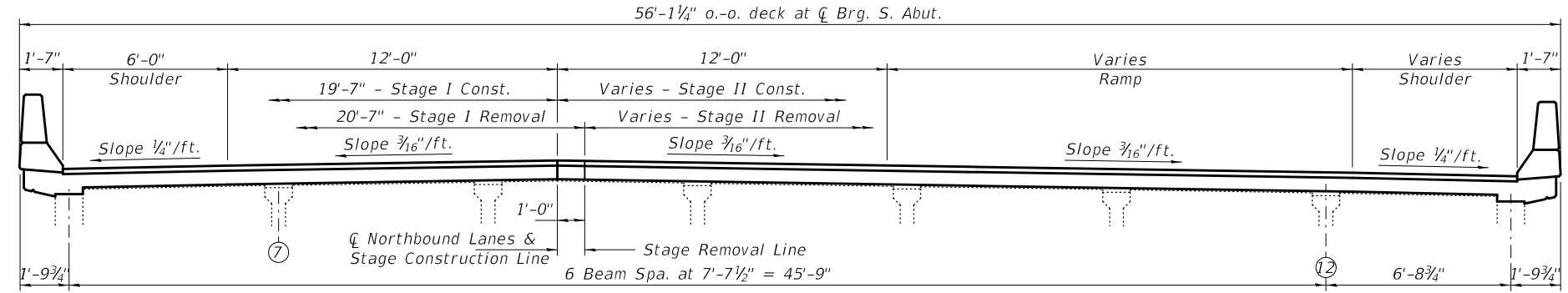
CROSS SECTIONS & STAGING DETAILS
SN 102-0058 (SB)

SHEET NO. 2 OF 13 SHEETS

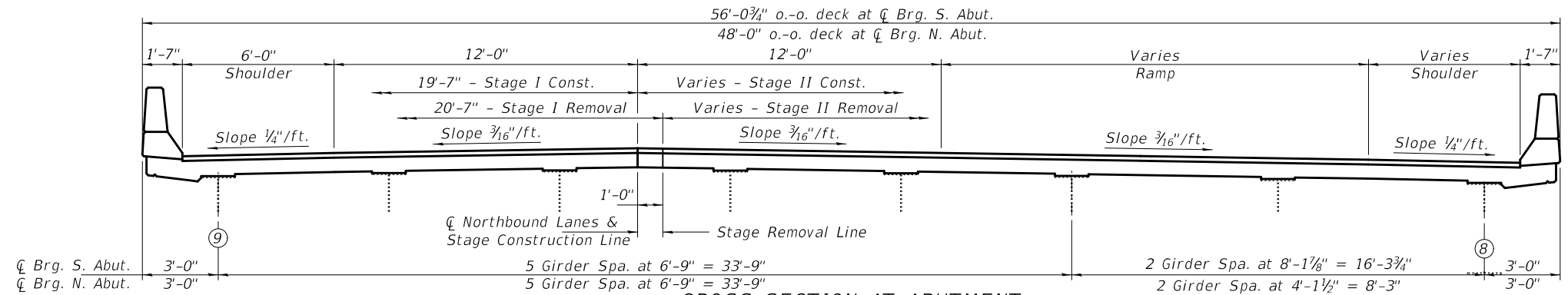
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
412	102(1RS,2RS-1; (2HVB)BRR)	WOODFORD	71	46
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				



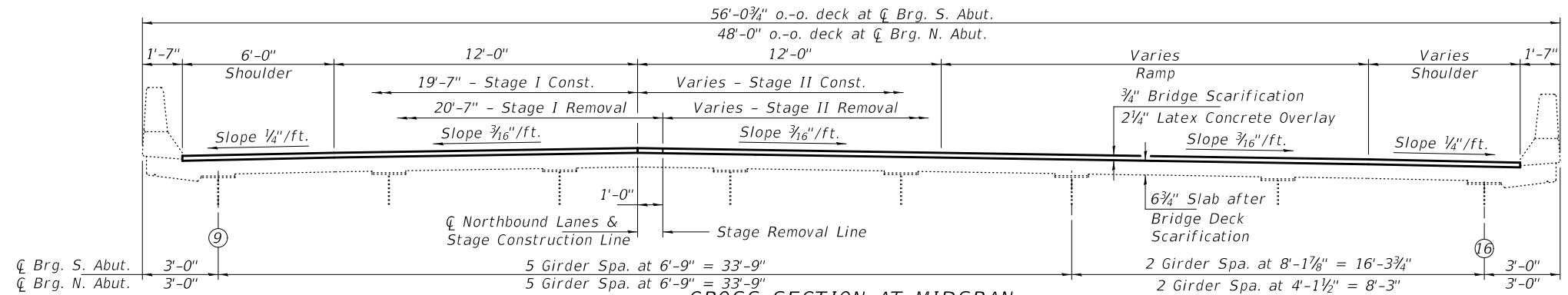
CROSS SECTION AT SOUTH APPROACH BENT
(Looking North)



CROSS SECTION AT SOUTH APPROACH ABUTMENT
(Looking South)



CROSS SECTION AT ABUTMENT
(Looking North)



CROSS SECTION AT MIDSPAN
(Looking North)

DESIGNED - CDK
 CHECKED - VHV
 DRAWN - daburdell
 CHECKED - CDK VHV

EXAMINED
 PASSED
 ENGINEER OF STRUCTURAL SERVICES
 ENGINEER OF BRIDGES AND STRUCTURES

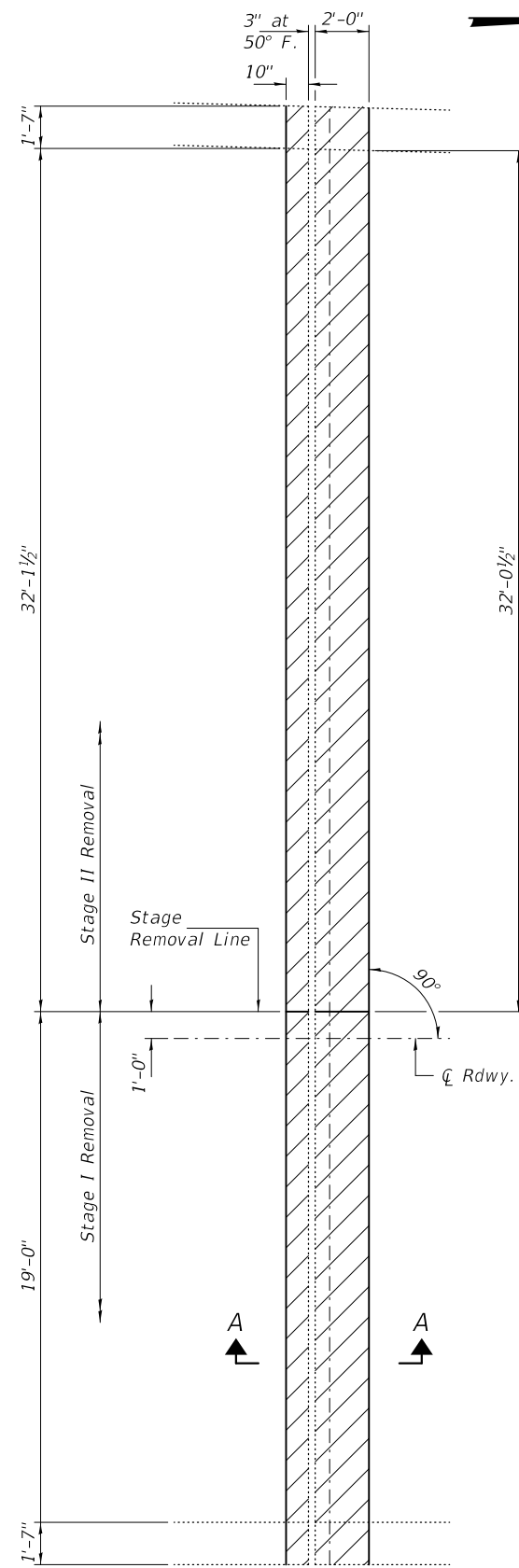
DATE - NOVEMBER 30, 2018
 REVISED -
 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

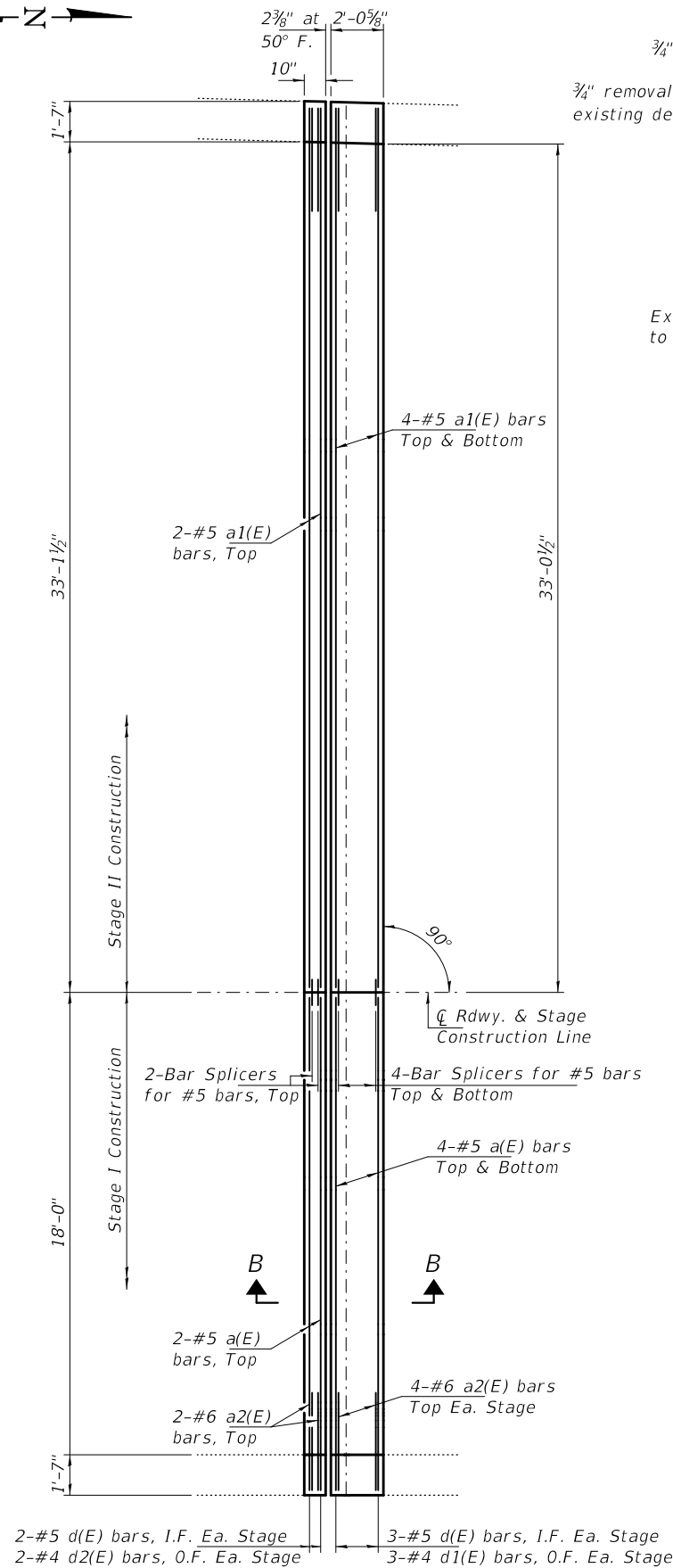
CROSS SECTIONS & STAGING DETAILS
 SN 102-0059 (NB)

SHEET NO. 3 OF 13 SHEETS

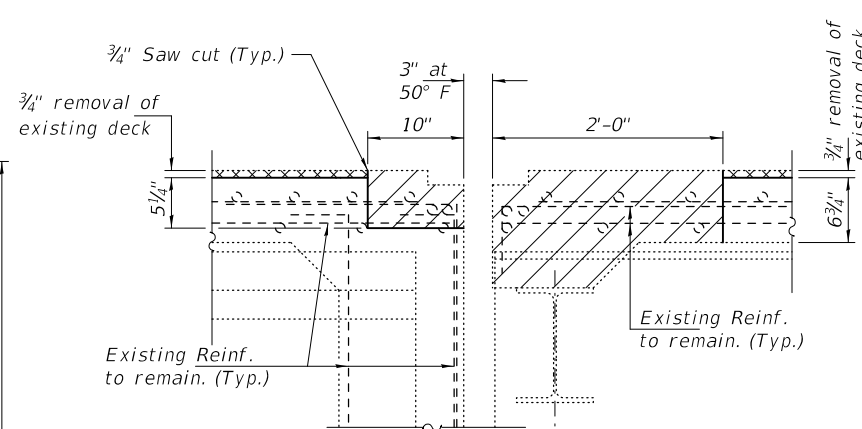
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
412	102(1RS,2RS-1; (2HVB)BRR)	WOODFORD	71	47
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				



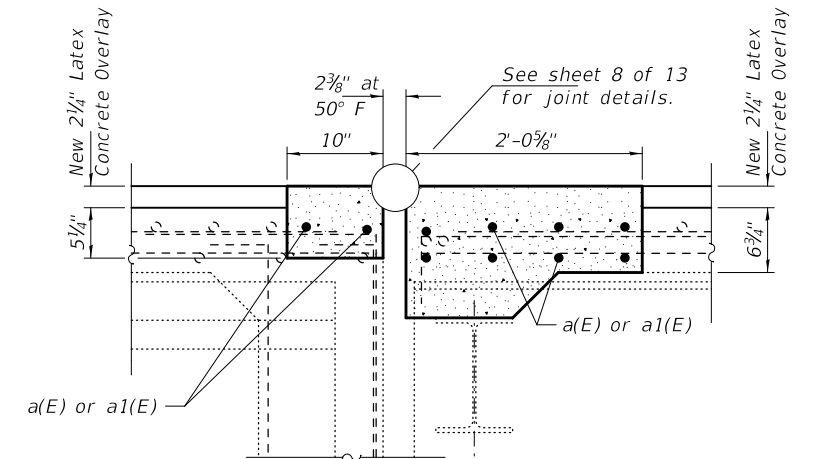
CONCRETE REMOVAL DETAILS
(South Abutment)



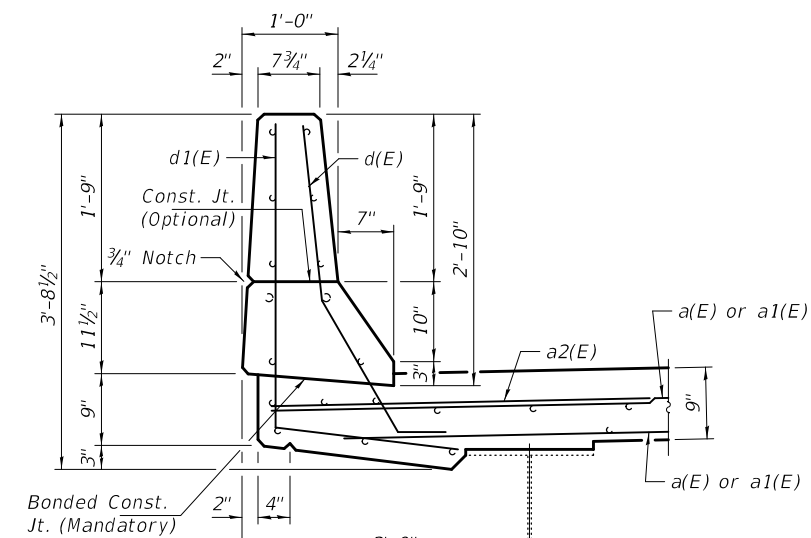
CONCRETE REPLACEMENT DETAILS
(South Abutment)



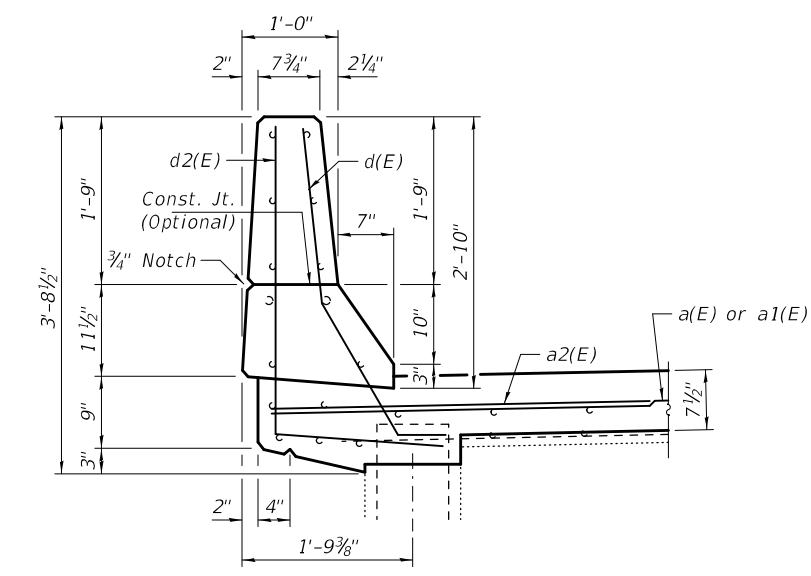
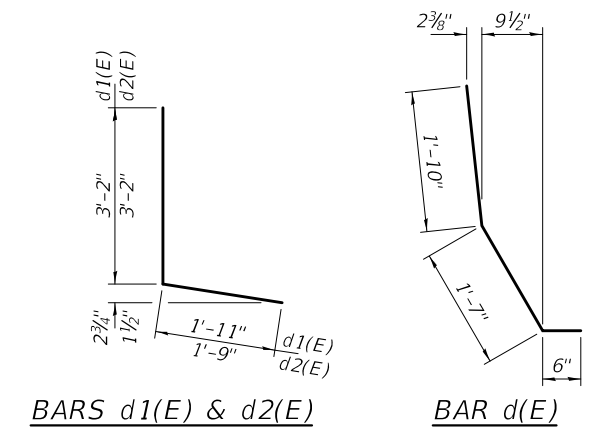
SECTION A-A



SECTION B-B



SECTION THRU BRIDGE PARAPET



SECTION THRU APPROACH PARAPET

BILL OF MATERIAL
(S. ABUT. - 0058)

Bar	No.	Size	Length	Shape
a(E)	10	#5	18'-9"	—
a1(E)	10	#5	33'-9"	—
a2(E)	12	#6	4'-0"	—
d(E)	10	#5	3'-11"	L
d1(E)	6	#4	5'-1"	└
d2(E)	4	#4	4'-11"	└
Concrete Removal			Cu. Yd.	4.4
Concrete Superstructure			Cu. Yd.	4.4
Bar Splicers			Each	10
Reinforcement Bars, Epoxy Coated			Pound	690

DESIGNED - CDK
CHECKED - VHV
DRAWN - daburdell
CHECKED - CDK VHV

EXAMINED
PASSED
ENGINEER OF STRUCTURAL SERVICES
ENGINEER OF BRIDGES AND STRUCTURES

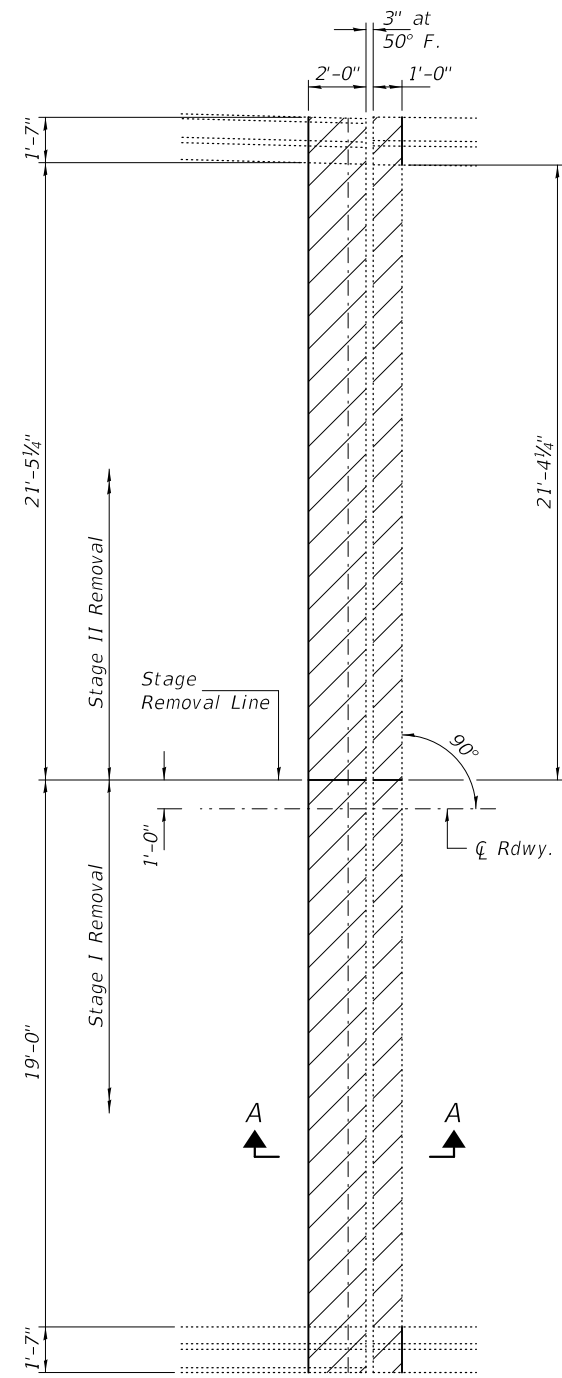
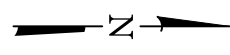
DATE - NOVEMBER 30, 2018
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

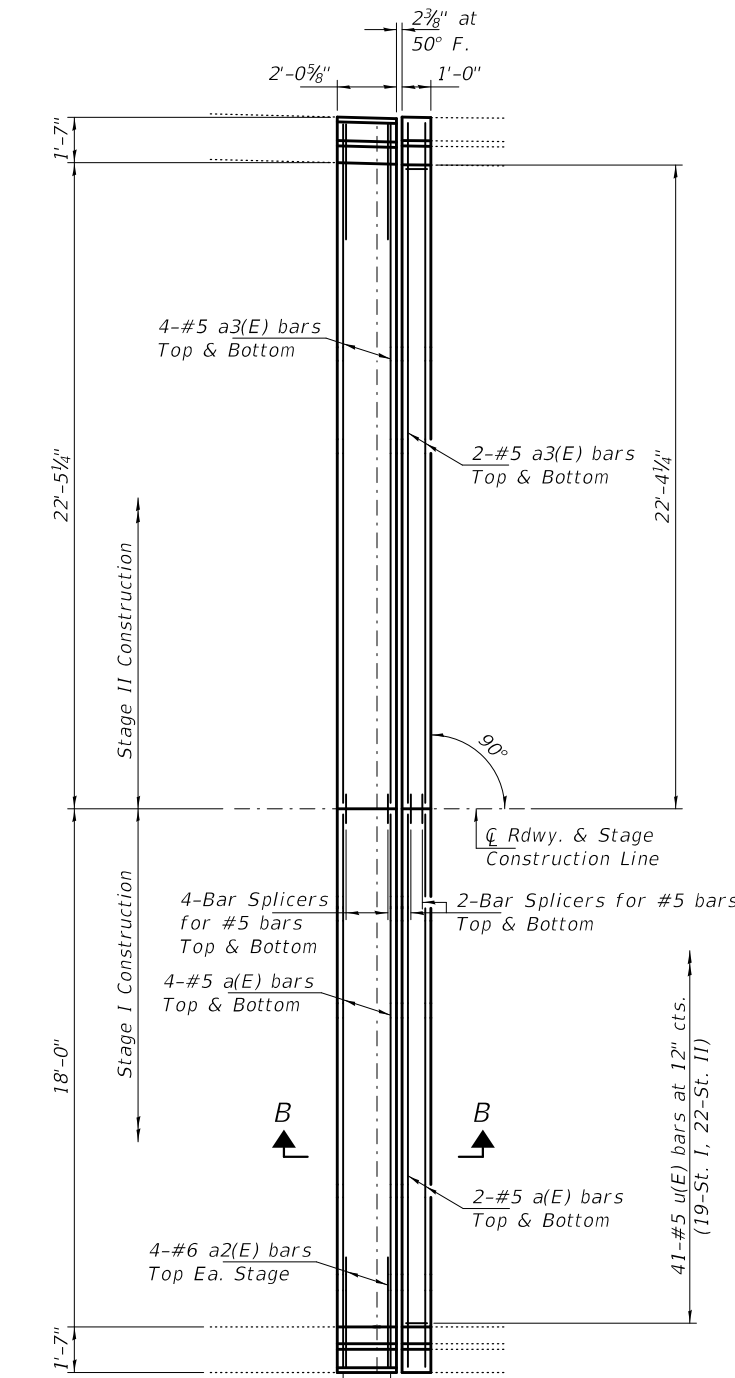
JOINT REPLACEMENT DETAILS - SOUTH ABUTMENT
SN 102-0058

SHEET NO. 4 OF 13 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
412	102(1RS,2RS-1; (2HVB)BRR)	WOODFORD	71	48
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				

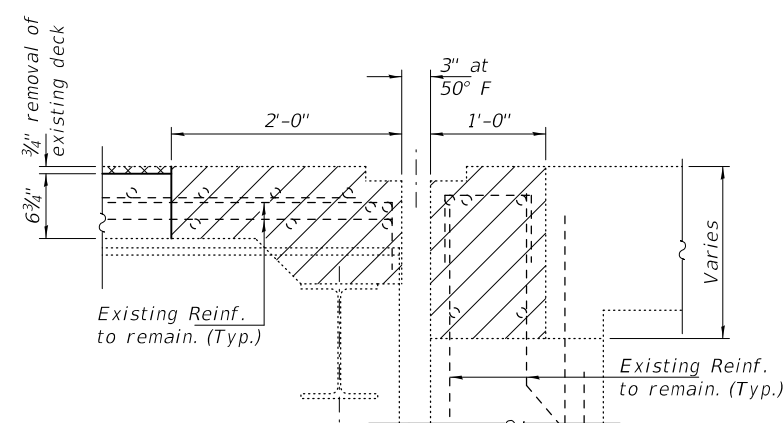


CONCRETE REMOVAL DETAILS
(North Abutment)

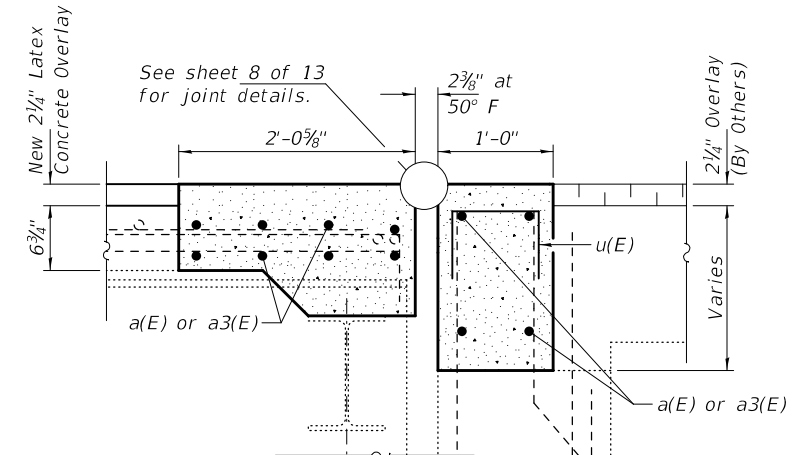


CONCRETE REPLACEMENT DETAILS
(North Abutment)

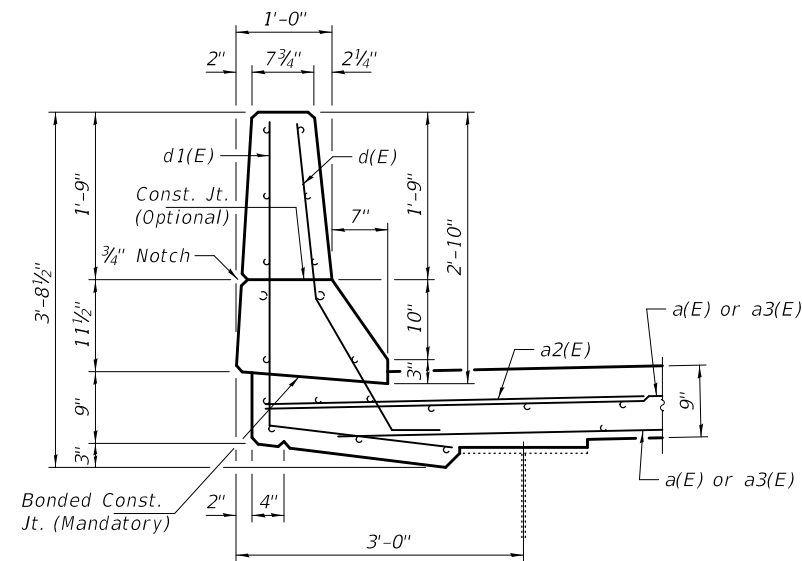
3-#5 d(E) bars, I.F. Ea. Stage
3-#4 d1(E) bars, O.F. Ea. Stage



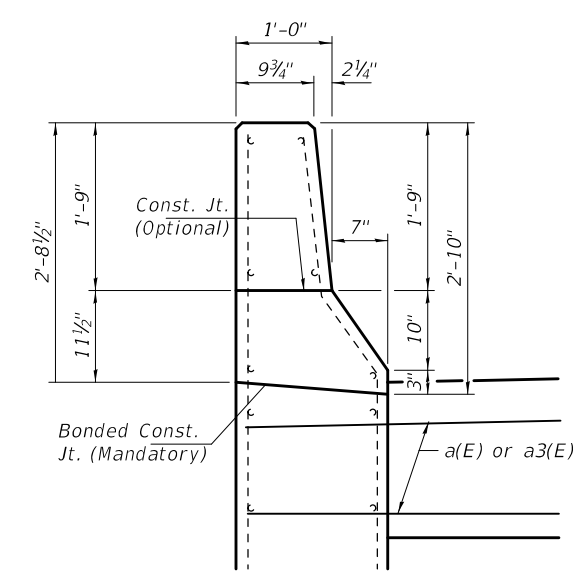
SECTION A-A



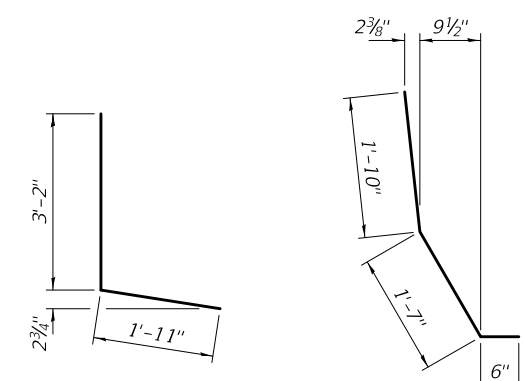
SECTION B-B



SECTION THRU BRIDGE PARAPET

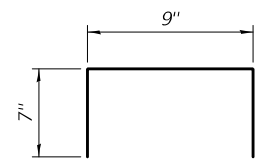


SECTION THRU APPROACH PARAPET



BAR d1(E)

BAR d(E)



BAR u(E)

BILL OF MATERIAL
(N. ABUT. - 0058)

Bar	No.	Size	Length	Shape
a(E)	12	#5	18'-9"	—
a2(E)	8	#6	4'-0"	—
a3(E)	12	#5	23'-1"	—
d(E)	6	#5	3'-11"	L
d1(E)	6	#4	5'-11"	L
u(E)	41	#5	1'-11"	U
Concrete Removal			Cu. Yd.	6.2
Concrete Superstructure			Cu. Yd.	6.2
Bar Splicers			Each	12
Reinforcement Bars, Epoxy Coated			Pound	700

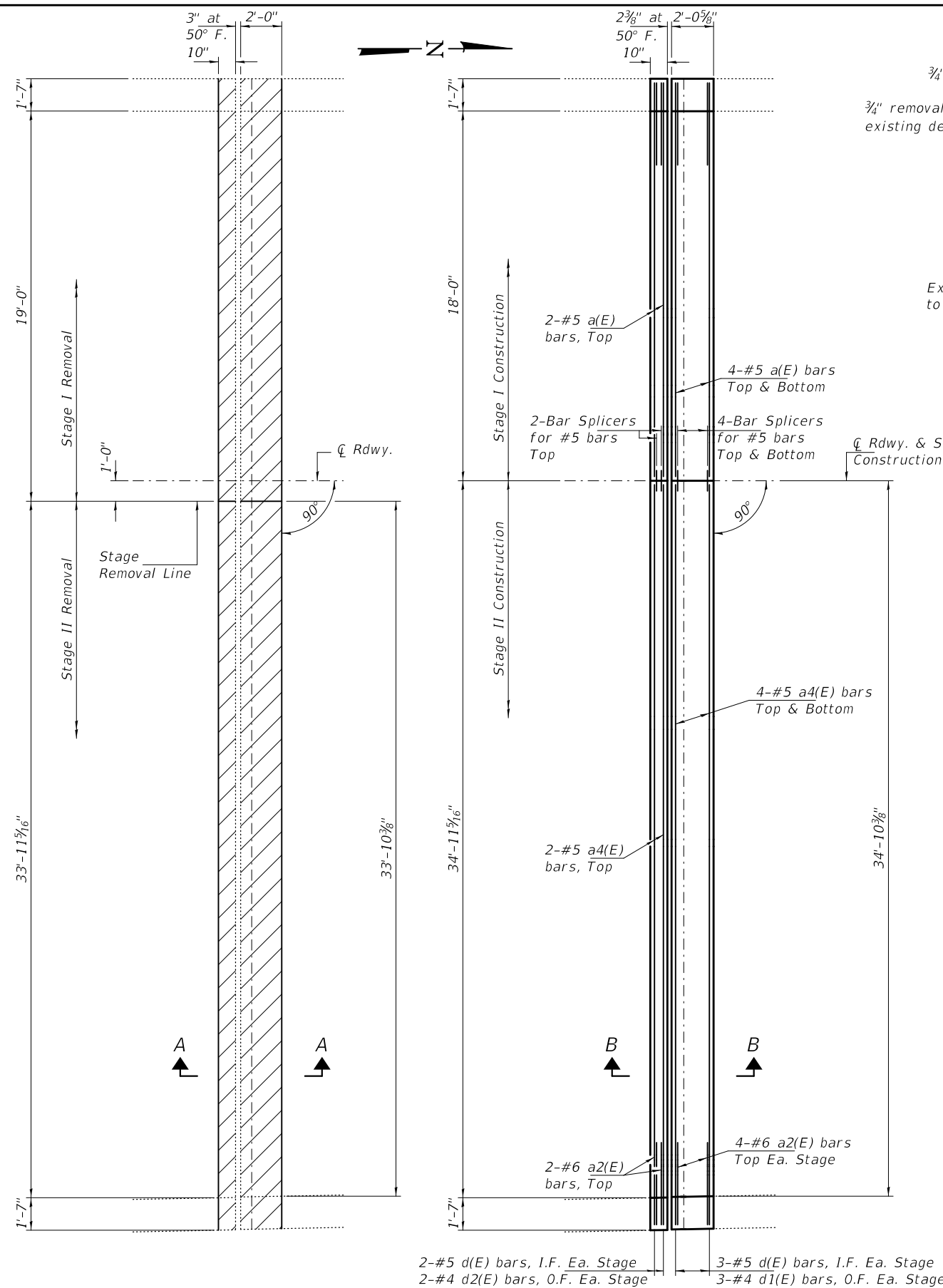
DESIGNED - CDK	EXAMINED - <i>Timothy A. Dill</i>	DATE - NOVEMBER 30, 2018
CHECKED - VHV	ENGINEER OF STRUCTURAL SERVICES	
DRAWN - daburdell	PASSED - <i>Carl Ringer</i>	REVISED -
CHECKED - CDK VHV	ENGINEER OF BRIDGES AND STRUCTURES	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

JOINT REPLACEMENT DETAILS - NORTH ABUTMENT
SN 102-0058

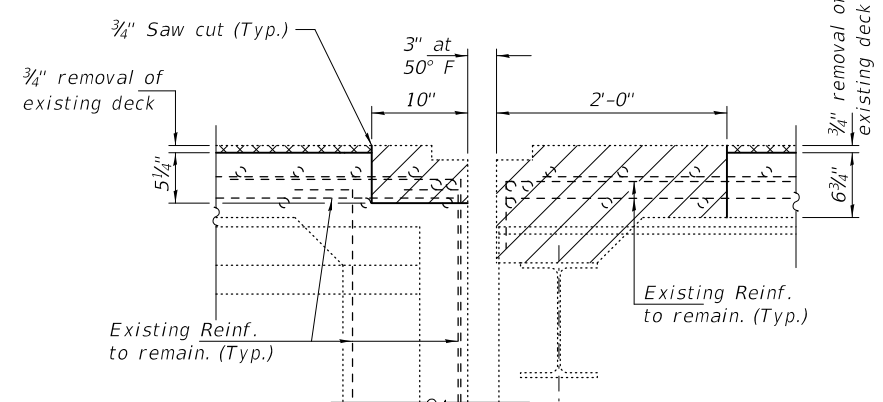
SHEET NO. 5 OF 13 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
412	102(1RS,2RS-1; (2HV)BRR)	WOODFORD	71	49
			CONTRACT NO. 68D79	
ILLINOIS FED. AID PROJECT				

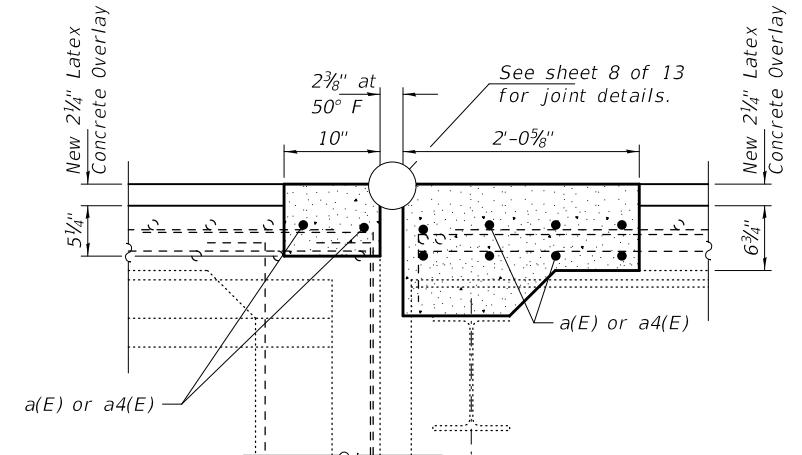


CONCRETE REMOVAL DETAILS
(South Abutment)

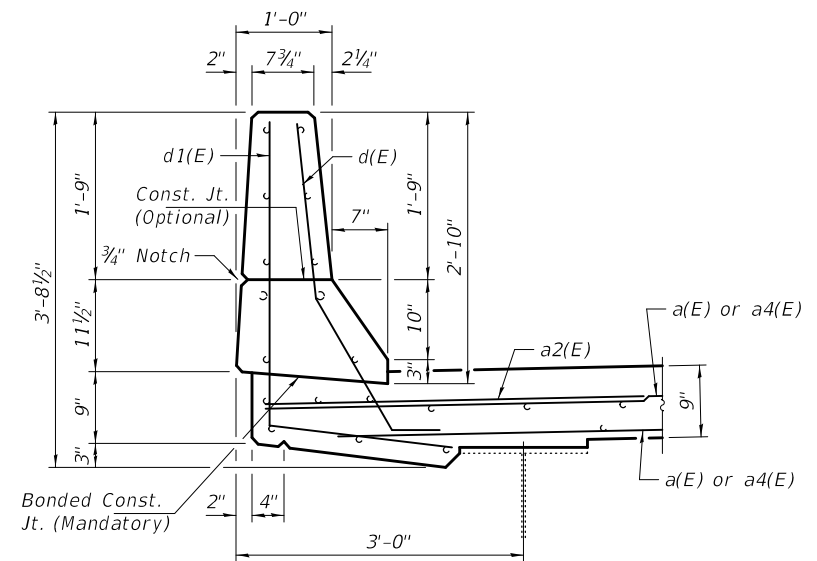
CONCRETE REPLACEMENT DETAILS
(South Abutment)



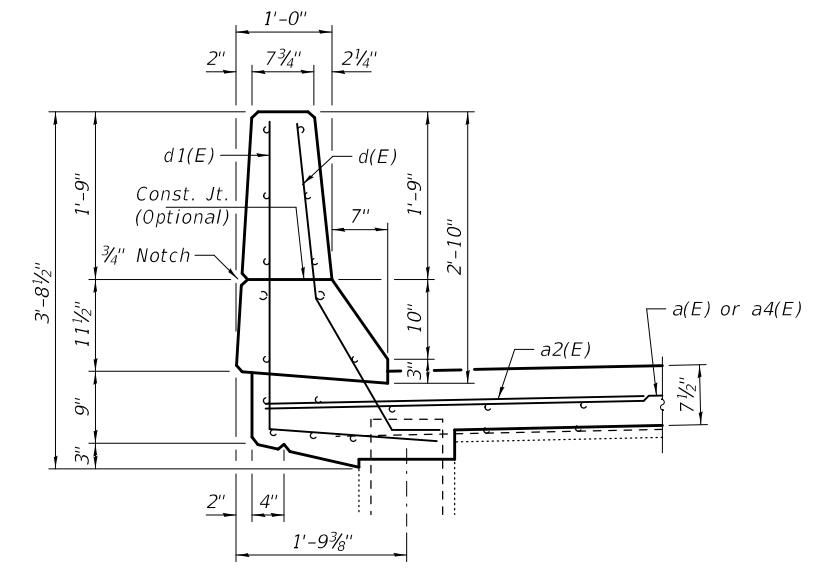
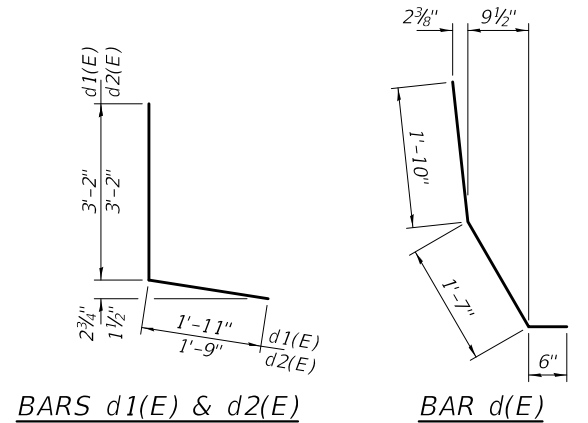
SECTION A-A



SECTION B-B



SECTION THRU BRIDGE PARAPET



SECTION THRU APPROACH PARAPET

BILL OF MATERIAL
(S. ABUT. - 0059)

Bar	No.	Size	Length	Shape
a(E)	10	#5	18'-9"	—
a2(E)	12	#6	4'-0"	—
a4(E)	10	#5	35'-7"	—
d(E)	10	#5	3'-11"	L
d1(E)	6	#4	5'-1"	L
d2(E)	4	#4	4'-11"	L
Concrete Removal			Cu. Yd.	4.7
Concrete Superstructure			Cu. Yd.	4.7
Bar Splicers			Each	10
Reinforcement Bars, Epoxy Coated			Pound	710

DESIGNED - CDK
 CHECKED - VHV
 DRAWN - daburdell
 CHECKED - CDK VHV

EXAMINED
 PASSED

DATE - NOVEMBER 30, 2018

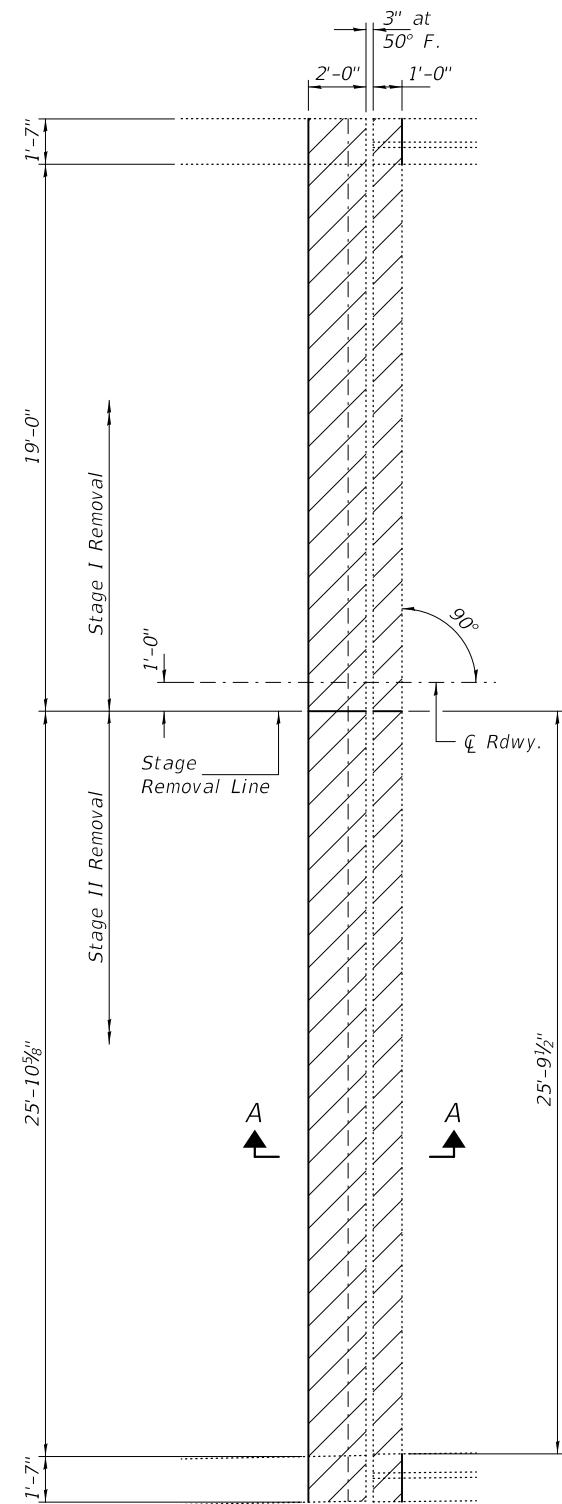
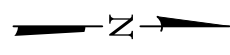
REVISI -
 REVISI -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

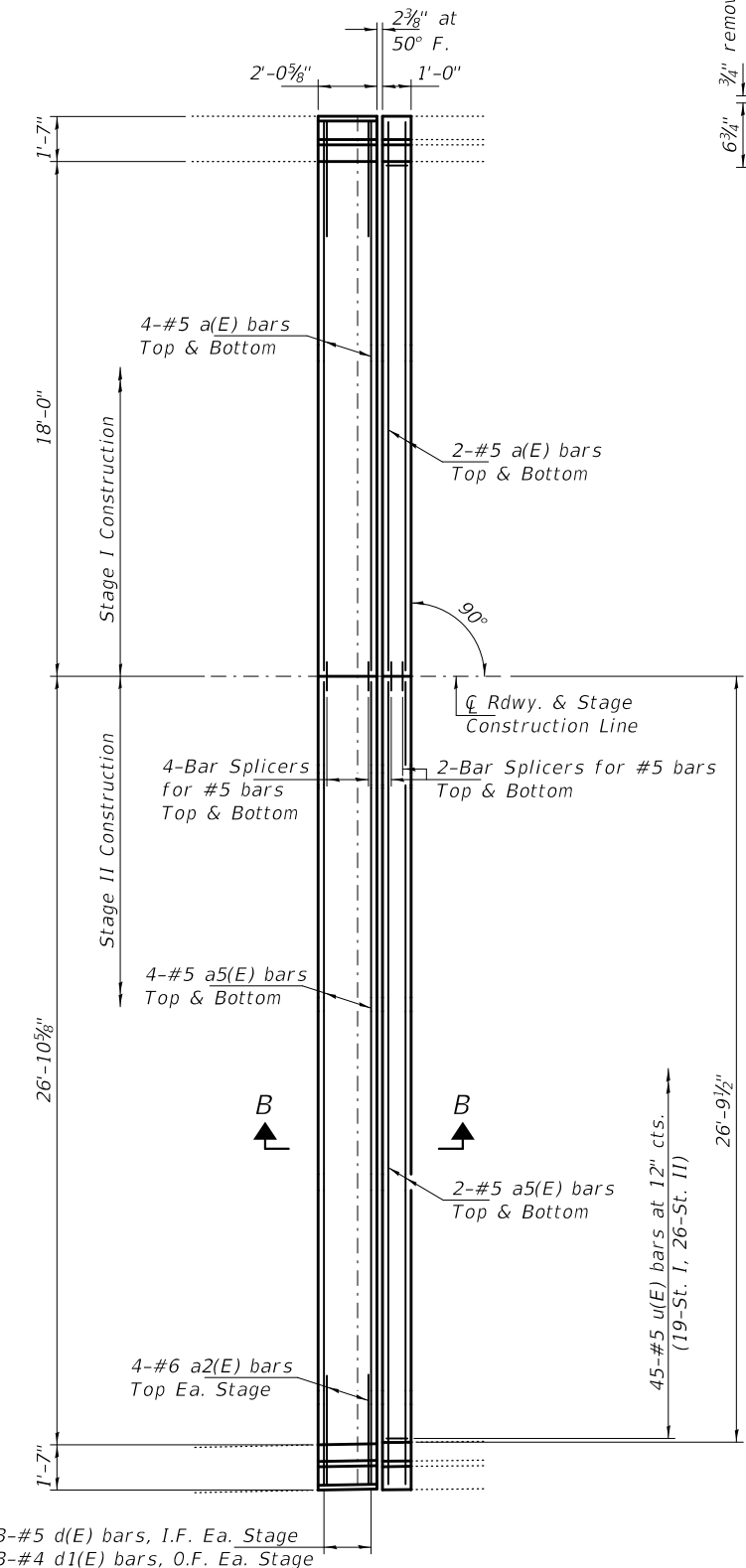
JOINT REPLACEMENT DETAILS - SOUTH ABUTMENT
SN 102-0059

SHEET NO. 6 OF 13 SHEETS

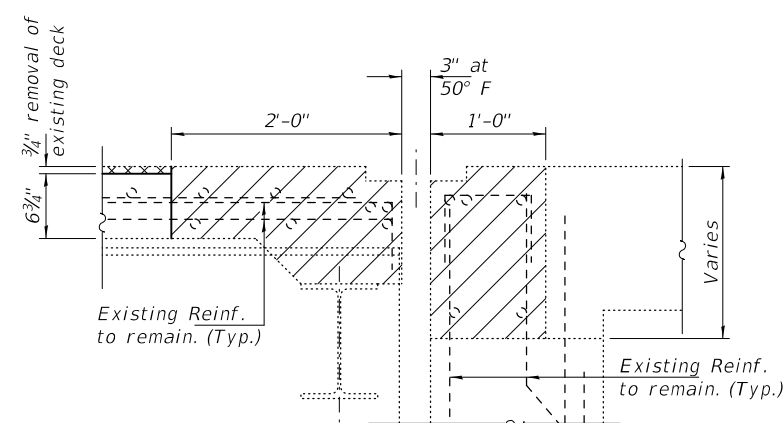
F.A.P. RTE. 412
 SECTION 102(1RS,2RS-1; (2HV)B)RRR
 COUNTY WOODFORD
 TOTAL SHEETS 71
 SHEET NO. 50
 CONTRACT NO. 68D79
 ILLINOIS FED. AID PROJECT



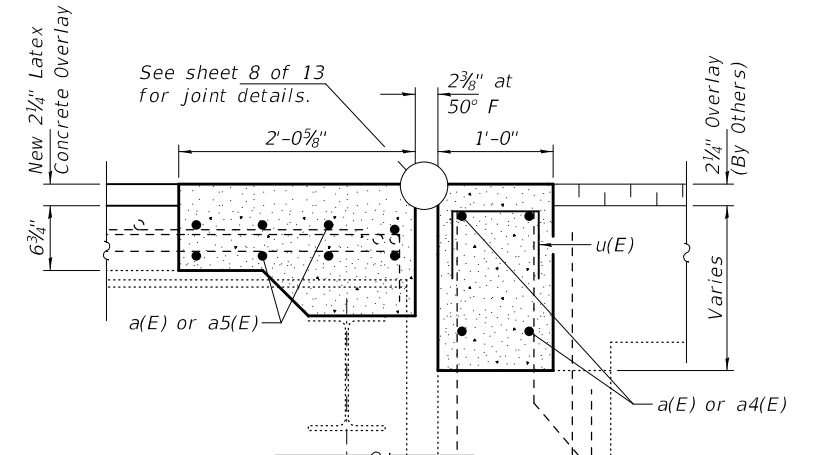
CONCRETE REMOVAL DETAILS
(North Abutment)



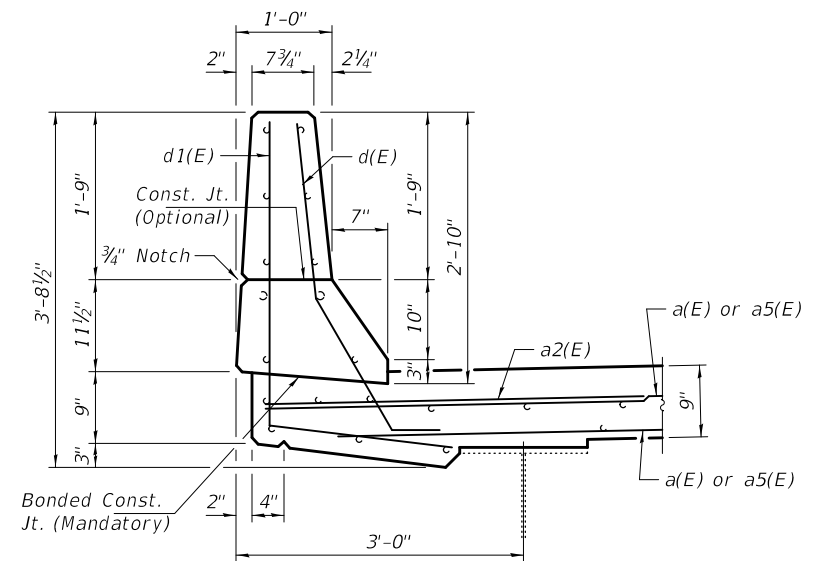
CONCRETE REPLACEMENT DETAILS
(North Abutment)



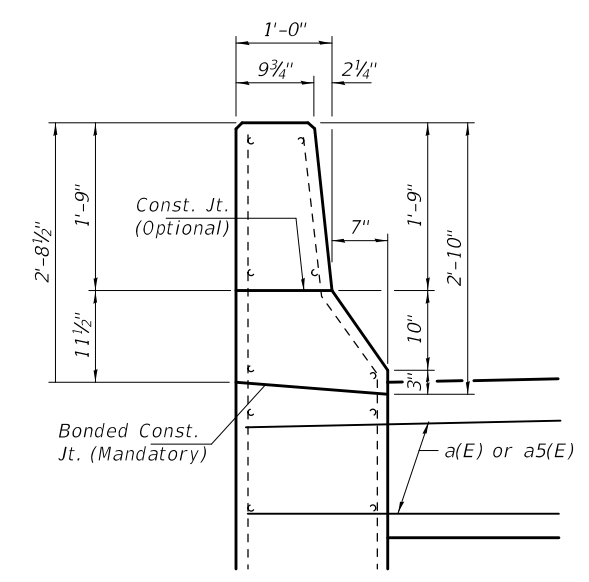
SECTION A-A



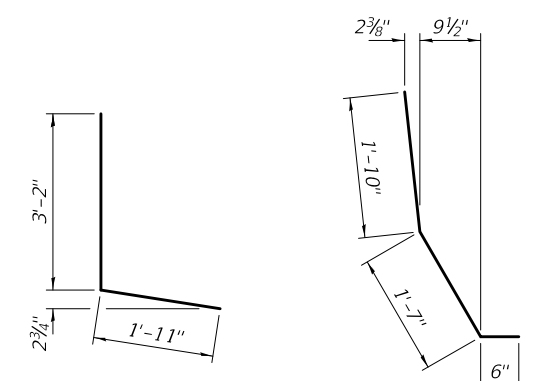
SECTION B-B



SECTION THRU BRIDGE PARAPET

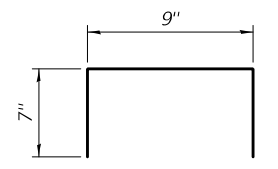


SECTION THRU APPROACH PARAPET



BAR d1(E)

BAR d(E)



BAR u(E)

BILL OF MATERIAL
(N. ABUT. - 0059)

Bar	No.	Size	Length	Shape
a(E)	12	#5	18'-9"	—
a2(E)	8	#6	4'-0"	—
a5(E)	12	#5	27'-6"	—
d(E)	6	#5	3'-11"	L
d1(E)	6	#4	5'-1"	L
u(E)	41	#5	1'-11"	U
Concrete Removal			Cu. Yd.	6.5
Concrete Superstructure			Cu. Yd.	6.5
Bar Splicers			Each	12
Reinforcement Bars, Epoxy Coated			Pound	760

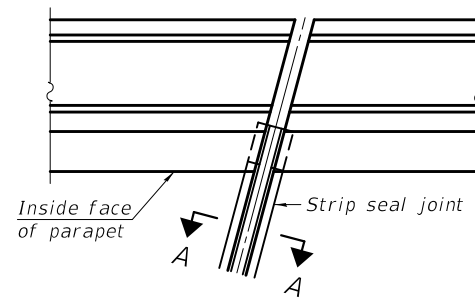
DESIGNED - CDK	EXAMINED - <i>Timothy A. Daulton</i>	DATE - NOVEMBER 30, 2018
CHECKED - VHV	ENGINEER OF STRUCTURAL SERVICES	
DRAWN - daburdell	PASSED - <i>Carl Ringer</i>	REVISED -
CHECKED - CDK VHV	ENGINEER OF BRIDGES AND STRUCTURES	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

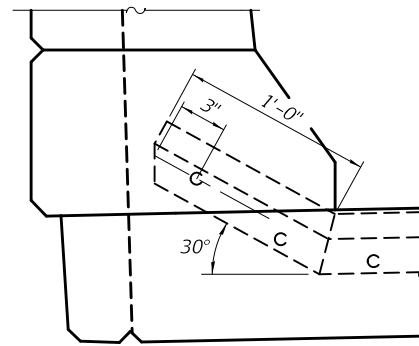
JOINT REPLACEMENT DETAILS - NORTH ABUTMENT
SN 102-0059

SHEET NO. 7 OF 13 SHEETS

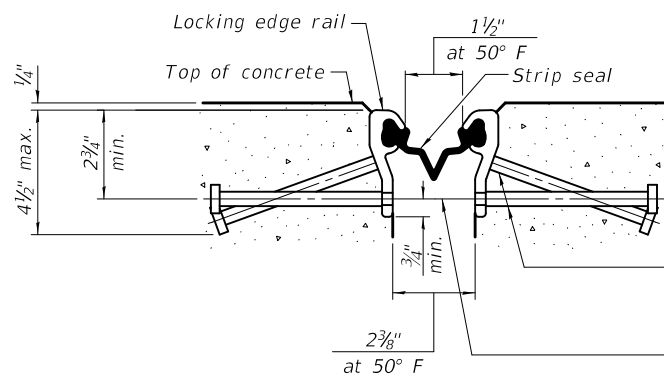
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
412	102(1RS,2RS-1; (2HVB)BRR)	WOODFORD	71	51
CONTRACT NO. 68D79			ILLINOIS FED. AID PROJECT	



FOR SKEWS $\leq 30^\circ$
PLAN AT PARAPET



DETAIL A



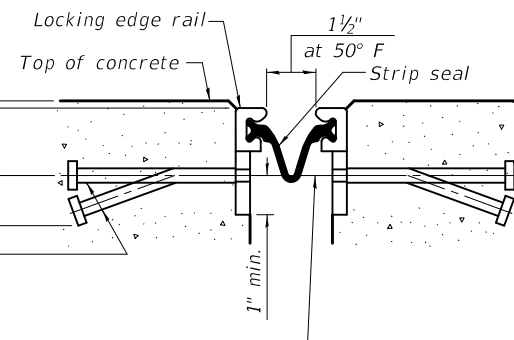
SHOWING ROLLED RAIL JOINT

* $\frac{5}{8}$ " ϕ x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

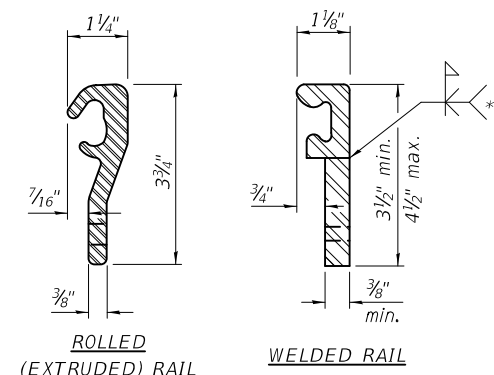
$\frac{3}{8}$ " ϕ threaded rods in $\frac{7}{16}$ " ϕ holes at ± 4 "-0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

SECTION A-A

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

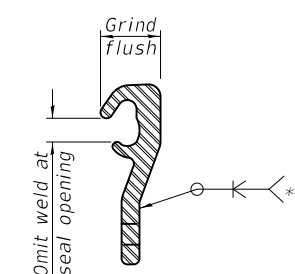


SHOWING WELDED RAIL JOINT



LOCKING EDGE RAILS

** Back gouge not required if complete joint penetration is verified by mock-up.



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	198

EJ-SS1 LT30/REPS

6-22-18

DESIGNED - CDK	CHECKED - VHV
DRAWN - daburdell	CHECKED - CDK VHV

EXAMINED	<i>Timothy A. Daulton</i> ENGINEER OF STRUCTURAL SERVICES
PASSED	<i>Carl R. Meyer</i> ENGINEER OF BRIDGES AND STRUCTURES

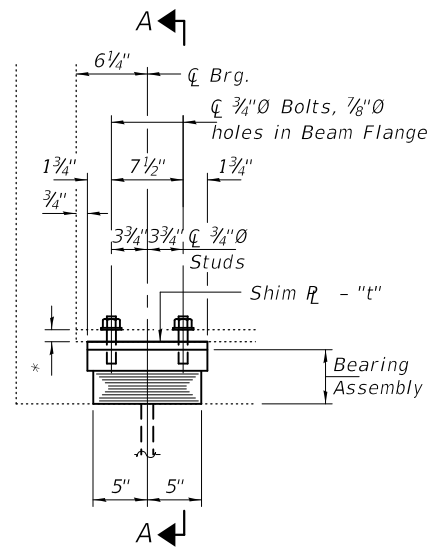
DATE - NOVEMBER 30, 2018
REVISED -
REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PREFORMED JOINT STRIP SEAL
 SN 102-0058 (SB) & 0059 (NB)**

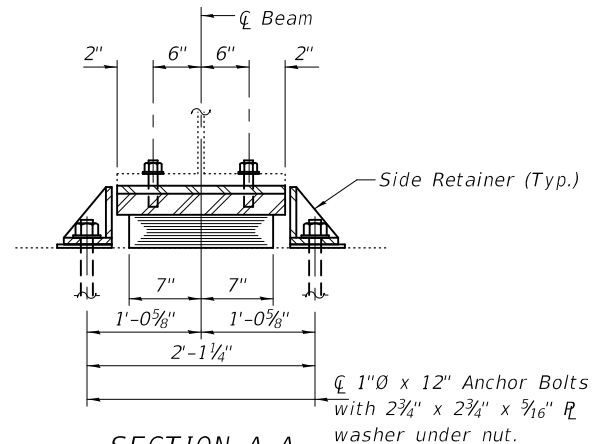
SHEET NO. 8 OF 13 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
412	102(1RS,2RS-1; (2HVB)BRR)	WOODFORD	71	52
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				



ELEVATION AT ABUTMENT

TYPE I ELASTOMERIC EXP. BRG.



SECTION A-A

BEAM REACTIONS (S. ABUTS.)

R _l	(K)	57.5
R _r	(K)	51.5
Imp.	(K)	11.0
R (Total)	(K)	120.0

BEAM REACTIONS (N. ABUTS.)

R _l	(K)	49.8
R _r	(K)	44.6
Imp.	(K)	9.5
R (Total)	(K)	103.9

Notes:

Diaphragm removal and reinstallation may be required to facilitate drilling holes. Cost included with Furnishing and Erecting Structural Steel.

New steel extensions, shim plates and connection bolts are included with Furnishing and Erecting Structural Steel.

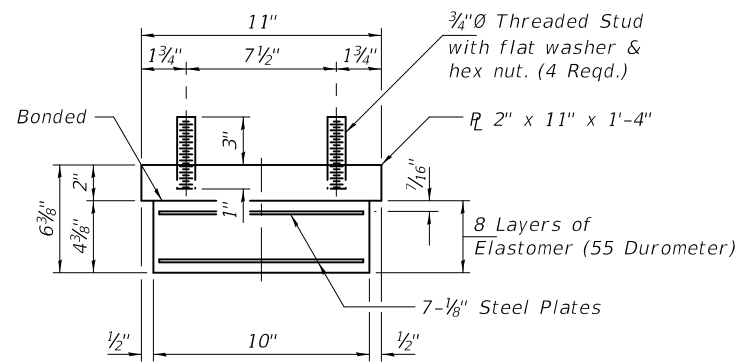
Prior to ordering any material, the Contractor shall verify in the field all bearing height and shim thickness dimensions. Adjustment must account for deck heave due to pack rust (if present).

Min. jack capacity = 60 Tons.

Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (F_y=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.

Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.

Cost of Side retainers and Stainless Steel plates shall be included in the cost of Elastomeric Bearing Assembly, Type I.



BEARING ASSEMBLY

Note:

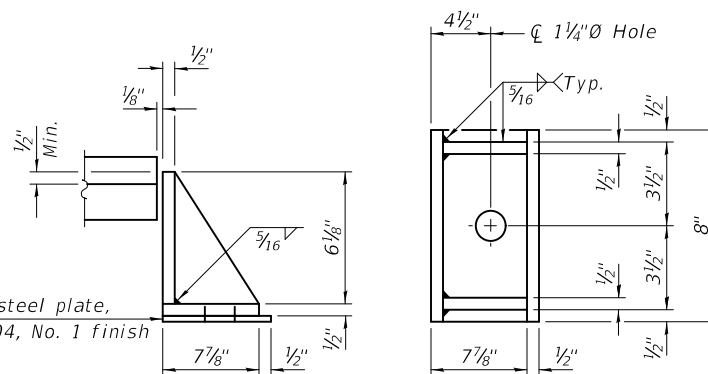
Shim plates shall not be placed under Bearing Assembly.

*** EXIST. BOTTOM FLANGE**

Beams	S. Abut.	N. Abut.
1 - 3	1 1/2"	7/8"
4 - 13	1"	1"
14 - 16	1 1/4"	7/8"

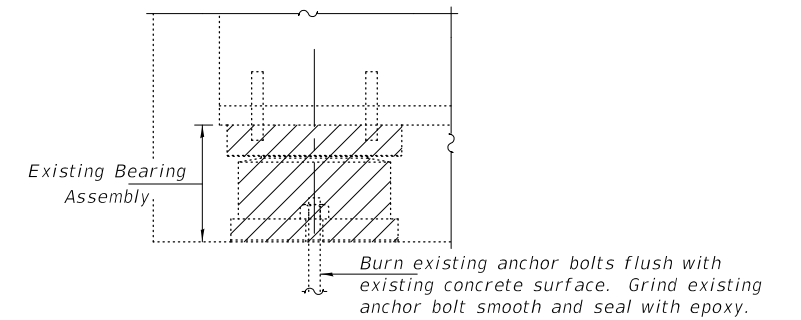
SHIM R "t"

GIRDERS	"t"
1 - 4	1 3/16"
5	1 1/16"
6 - 10	1 3/16"
11	1 1/16"
12 - 16	1 3/16"



SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.



EXISTING BEARING REMOVAL DETAIL

Cost included with Jack and Remove Existing Bearings.

BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly, Type I	Each	32
Jack and Remove Existing Bearings	Each	32
Furnishing and Erecting Structural Steel	Pound	1730
Anchor Bolts, 1"Ø	Each	64

TYI-NO EXT/REPS

10-11-2018

DESIGNED - CDK
CHECKED - VHV
DRAWN - daburdell
CHECKED - CDK VHV

EXAMINED	<i>Timothy A. Daulton</i> ENGINEER OF STRUCTURAL SERVICES
PASSED	<i>Carl R. Berger</i> ENGINEER OF BRIDGES AND STRUCTURES

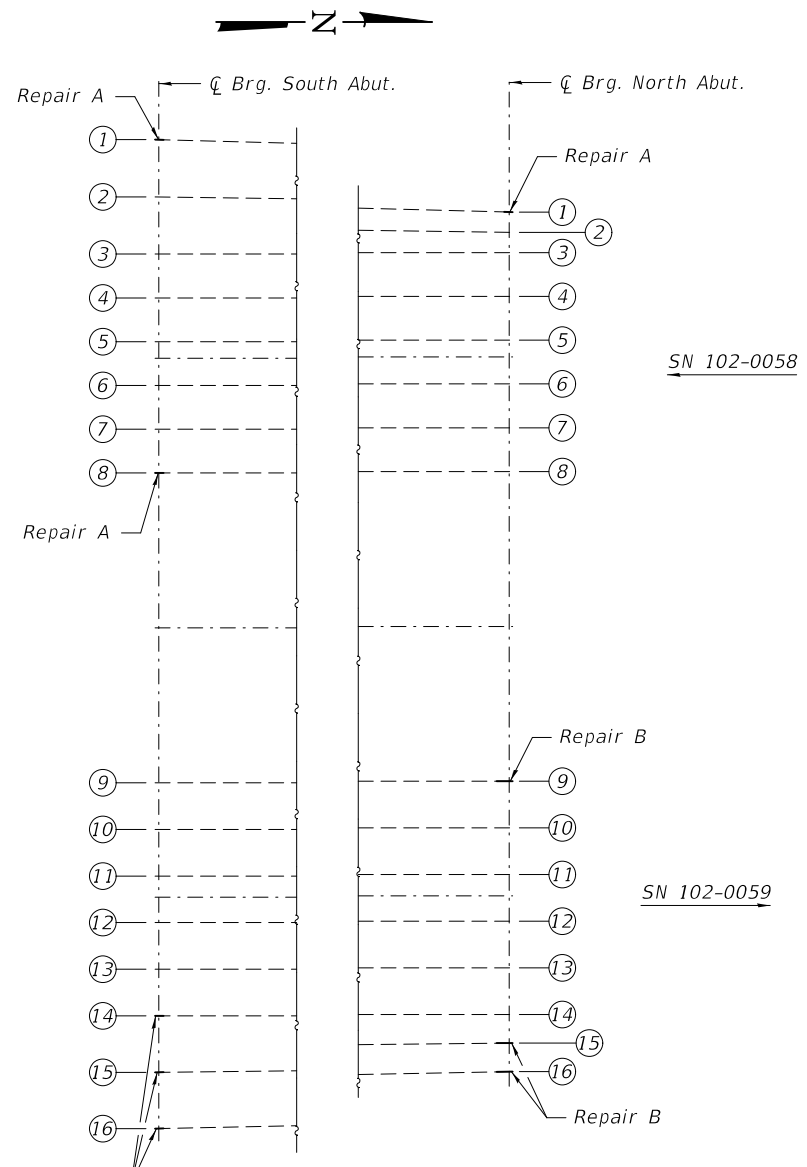
DATE - NOVEMBER 30, 2018
REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

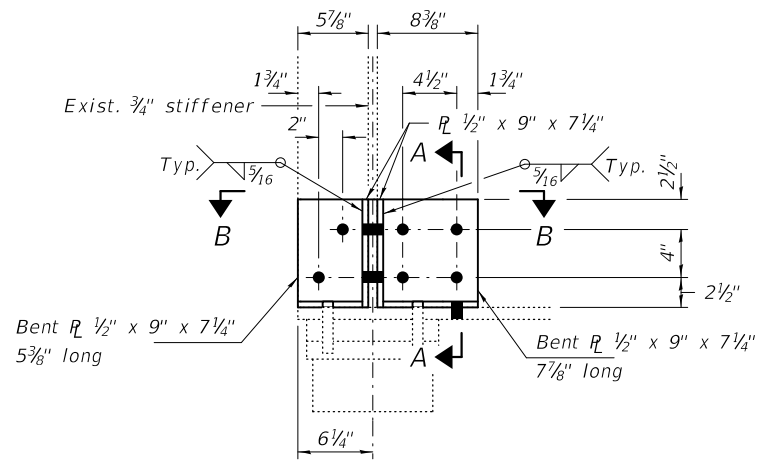
**BEARING DETAILS - ABUTMENTS
SN 102-0058 (SB) & 0059 (NB)**

SHEET NO. 9 OF 13 SHEETS

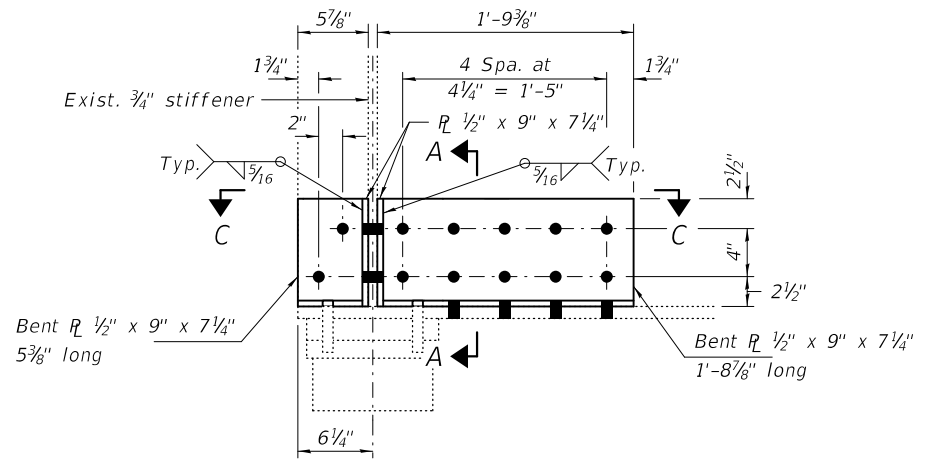
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
412	102(1RS,2RS-1; (2HVB)BRR)	WOODFORD	71	53
				CONTRACT NO. 68D79
				ILLINOIS FED. AID PROJECT



PARTIAL FRAMING PLAN
SN 102-0058 (SB) & 0059 (NB)

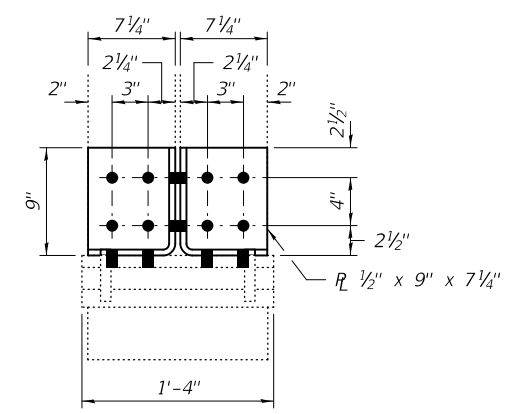


REPAIR A

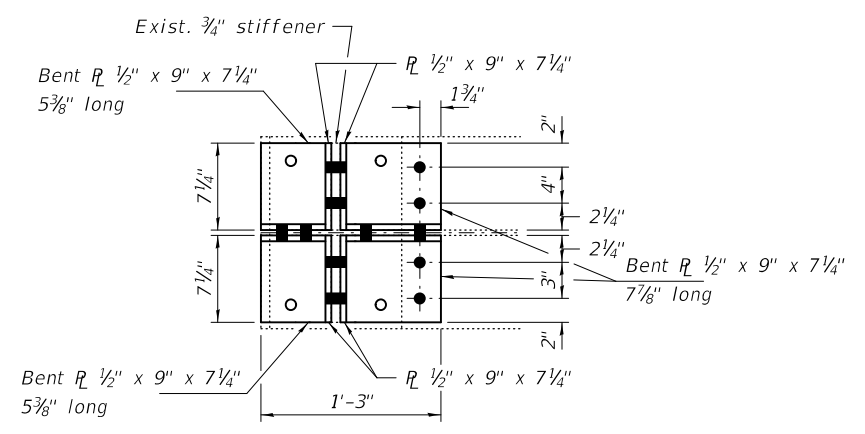


REPAIR B

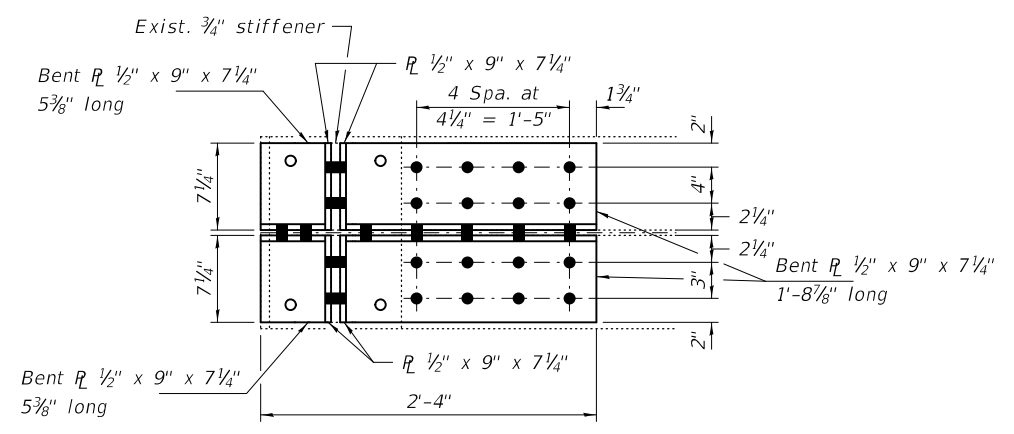
Notes:
Fasteners shall be high strength bolts. Bolts 7/8"Ø, open holes 13/16"Ø, unless otherwise noted.
Cost of all repairs on this sheet are included with Structural Steel Repairs.



SECTION A-A



SECTION B-B



SECTION C-C

BOLT HOLE LEGEND
○ - Field drill using existing steel as template.
● - Field drill using new steel as template.

TOTAL BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Steel Repair	Pound	1260

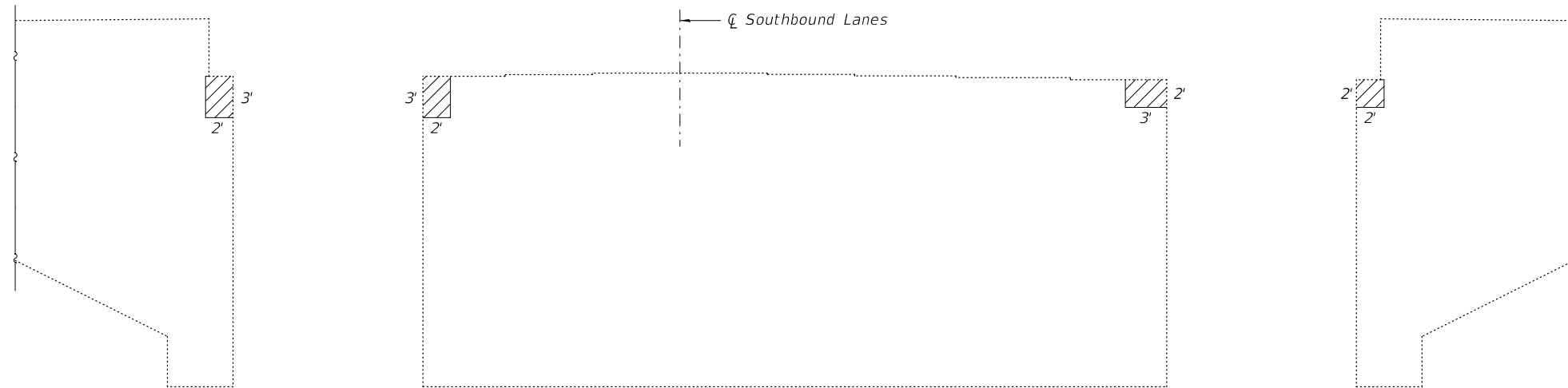
DESIGNED - CDK	EXAMINED - <i>Timothy A. Daburdell</i> ENGINEER OF STRUCTURAL SERVICES	DATE - NOVEMBER 30, 2018
CHECKED - VHV	PASSED - <i>Carl R. King</i> ENGINEER OF BRIDGES AND STRUCTURES	REVISED -
DRAWN - <i>daburdell</i>		REVISED -
CHECKED - CDK VHV		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

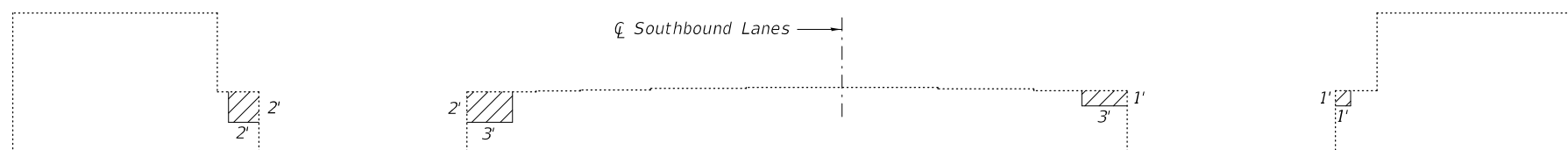
REPAIR DETAILS
SN 102-0058 (SB) & 0059 (NB)

SHEET NO. 10 OF 13 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
412	102(1RS,2RS-1; (2HVB)BRR)	WOODFORD	71	54
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				



SOUTH ABUTMENT
(Looking South)



NORTH ABUTMENT
(Looking North)

Note:
Hatched areas indicate Structural
Repair of Concrete (Depth ≤ 5").

BILL OF MATERIAL

Item	Unit	Total
Structural Repair of Concrete (Depth ≤ 5")	Sq. Ft.	36

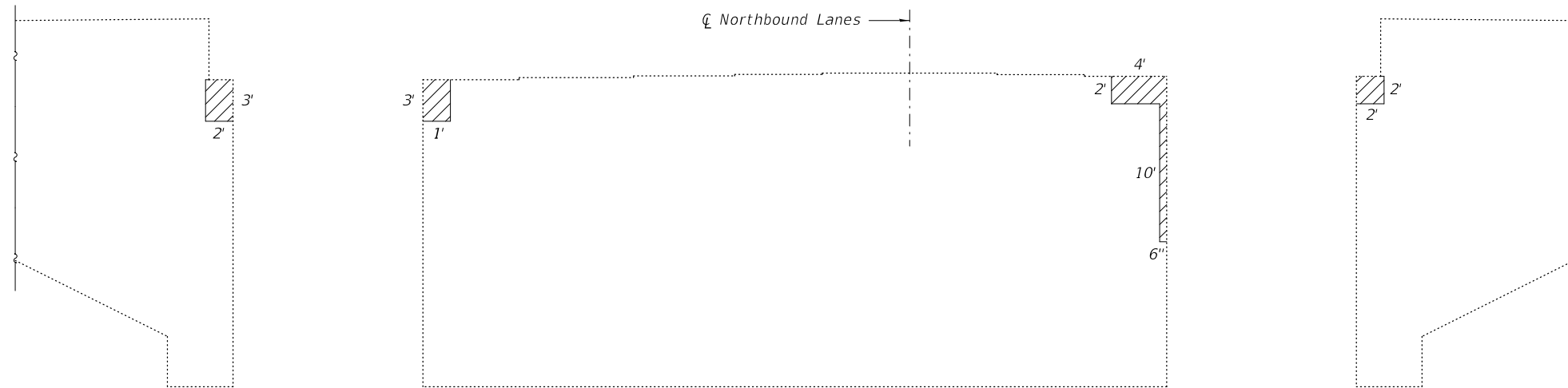
DESIGNED - CDK	EXAMINED	DATE - NOVEMBER 30, 2018
CHECKED - VHV	<i>Timothy A. Daburdell</i> ENGINEER OF STRUCTURAL SERVICES	
DRAWN - daburdell	PASSED	REVISED -
CHECKED - CDK VHV	<i>Carl Ringer</i> ENGINEER OF BRIDGES AND STRUCTURES	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

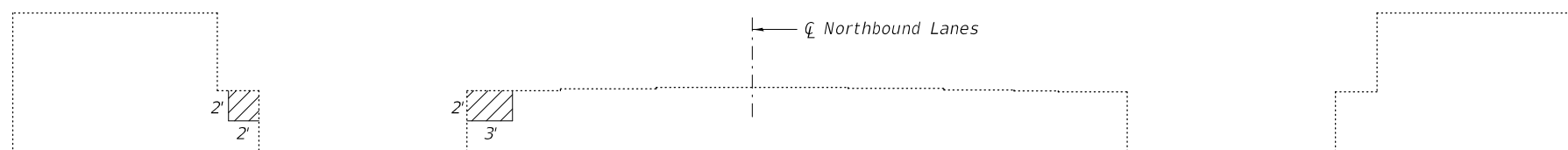
ABUTMENT CONCRETE REPAIR
SN 102-0058 (SB)

SHEET NO. 11 OF 13 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
412	102(1RS,2RS-1; (2HVB)BRR)	WOODFORD	71	55
CONTRACT NO. 68D79				
ILLINOIS		FED. AID PROJECT		



SOUTH ABUTMENT
(Looking South)



NORTH ABUTMENT
(Looking North)

Note:
Hatched areas indicate Structural
Repair of Concrete (Depth \leq 5").

BILL OF MATERIAL

Item	Unit	Total
Structural Repair of Concrete (Depth \leq 5")	Sq. Ft.	36

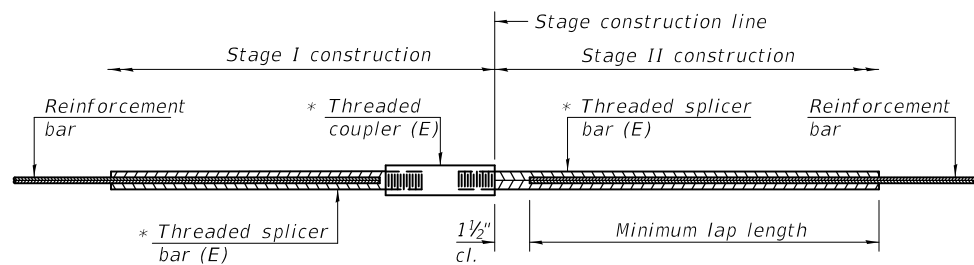
DESIGNED - CDK	EXAMINED	DATE - NOVEMBER 30, 2018
CHECKED - VHV	<i>Timothy A. Daburdell</i> ENGINEER OF STRUCTURAL SERVICES	
DRAWN - daburdell	PASSED	REVISED -
CHECKED - CDK VHV	<i>Carl Ringer</i> ENGINEER OF BRIDGES AND STRUCTURES	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ABUTMENT CONCRETE REPAIR
SN 102-0059 (NB)

SHEET NO. 12 OF 13 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
412	102(1RS,2RS-1; (2HVB)BRR)	WOODFORD	71	56
CONTRACT NO. 68D79				
ILLINOIS		FED. AID PROJECT		

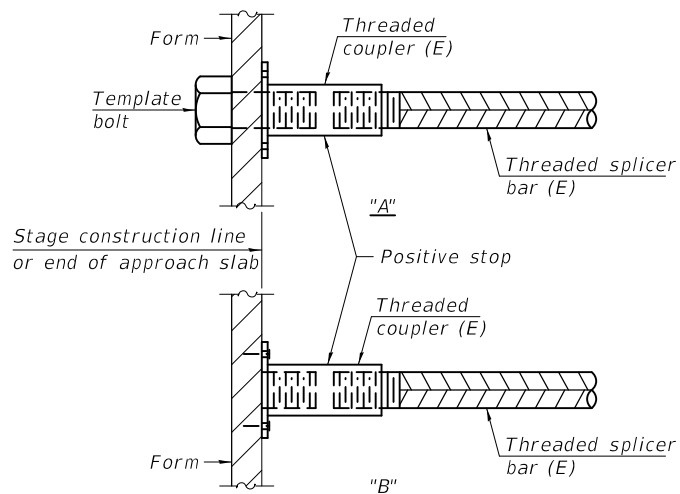


STANDARD BAR SPLICER ASSEMBLY

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

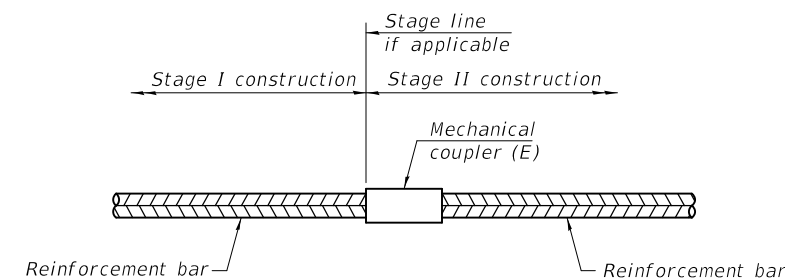
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
S. Abut. (0058)	#5	10	3'-4"
N. Abut. (0058)	#5	12	3'-4"
S. Abut. (0059)	#5	10	3'-4"
N. Abut. (0059)	#5	12	3'-4"



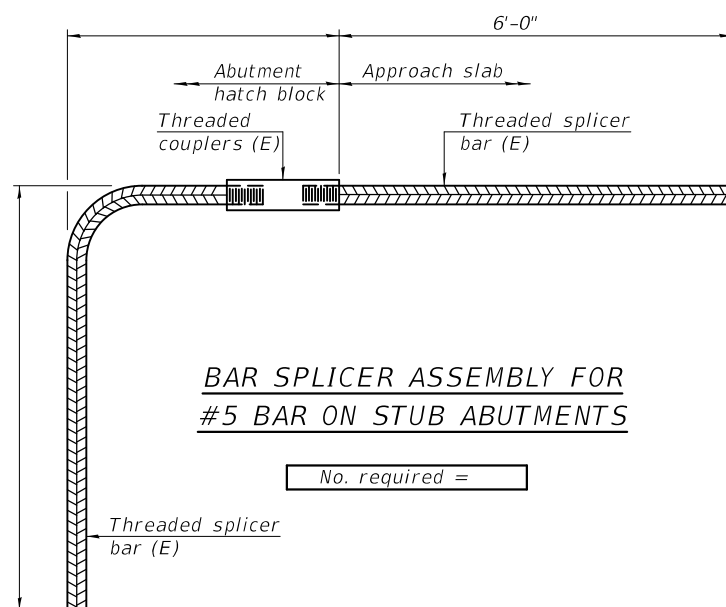
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required =

NOTES

Splicer bars shall be deformed with Threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1 2-17-2017

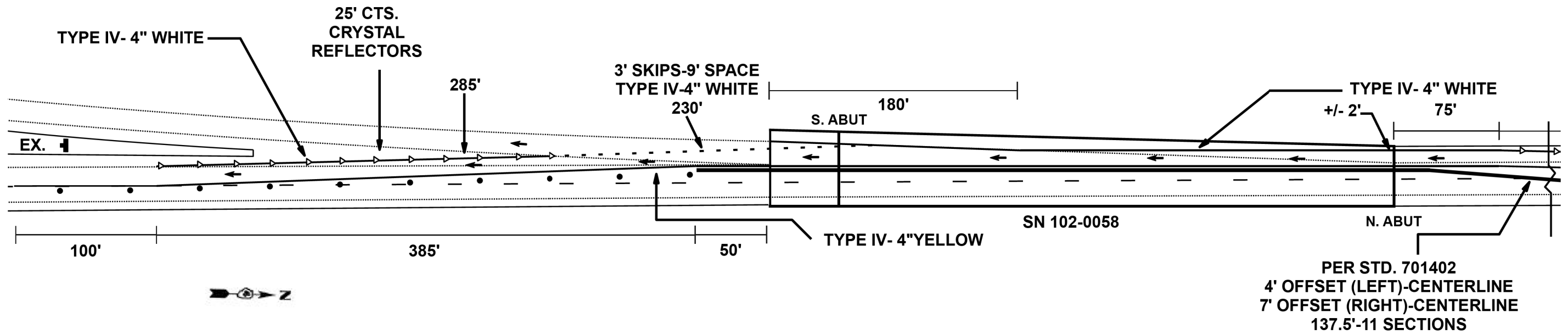
DESIGNED - CDK	EXAMINED	DATE - NOVEMBER 30, 2018
CHECKED - VHV	<i>Timothy A. Daburdell</i> ENGINEER OF STRUCTURAL SERVICES	REVISED -
DRAWN - daburdell	PASSED	REVISED -
CHECKED - CDK VHV	<i>Carl R. Ruyter</i> ENGINEER OF BRIDGES AND STRUCTURES	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
SN 102-0058 (SB) & 0059 (NB)

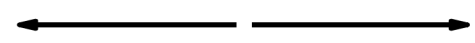
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
412	102(1RS,2RS-1; (2HVB)BRR)	WOODFORD	71	56A
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				

SHEET NO. 13 OF 13 SHEETS

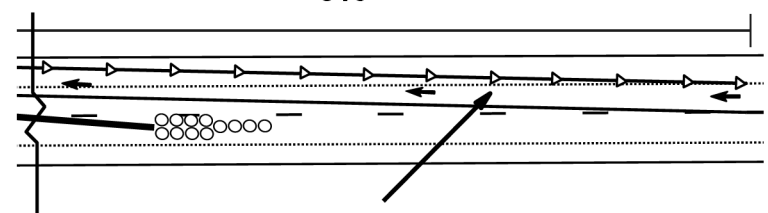


PER STD. 701402
 4' OFFSET (LEFT)-CENTERLINE
 7' OFFSET (RIGHT)-CENTERLINE
 137.5'-11 SECTIONS

STRIPING/SIGNAGE/DEVICES
 AND ETC. AS PER STD. 701402
 AND AS SHOWN



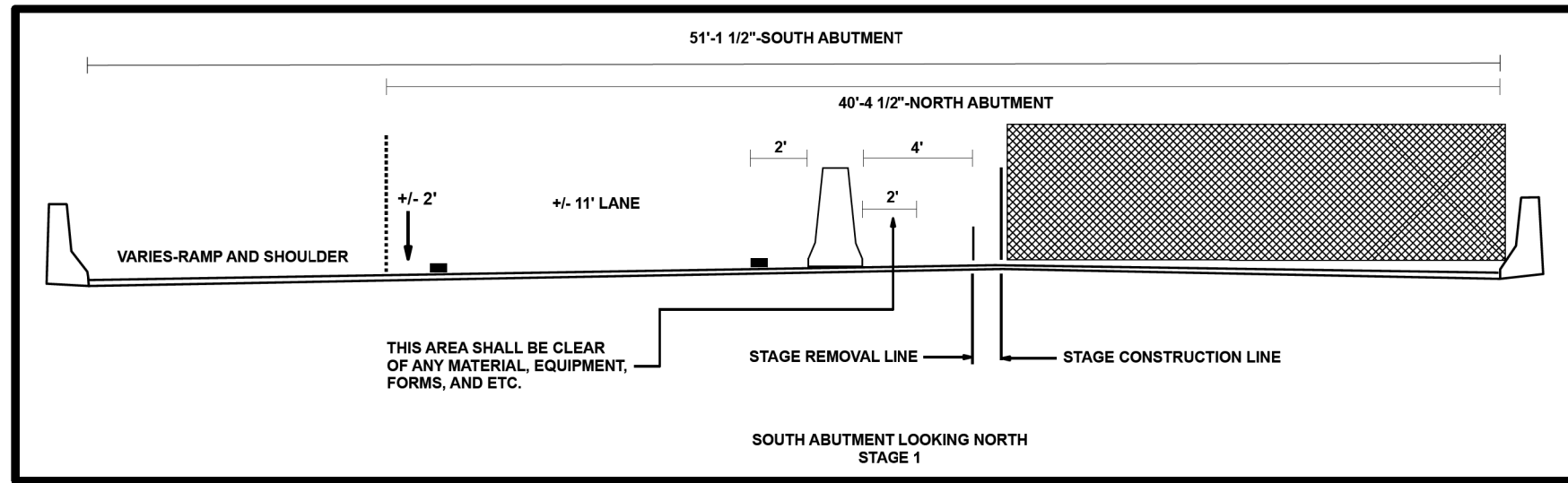
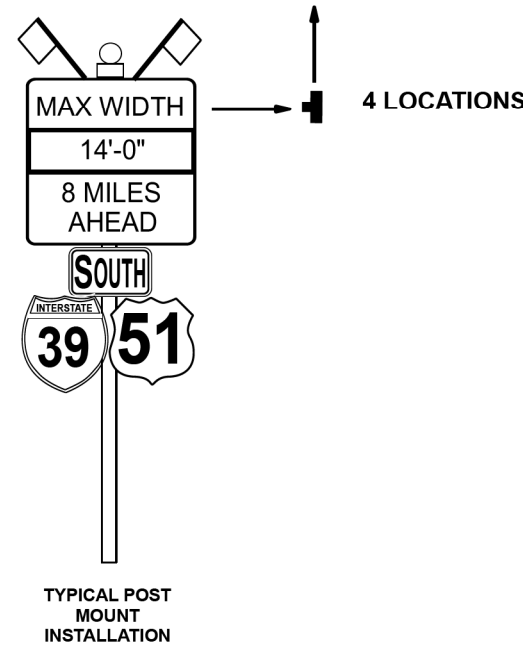
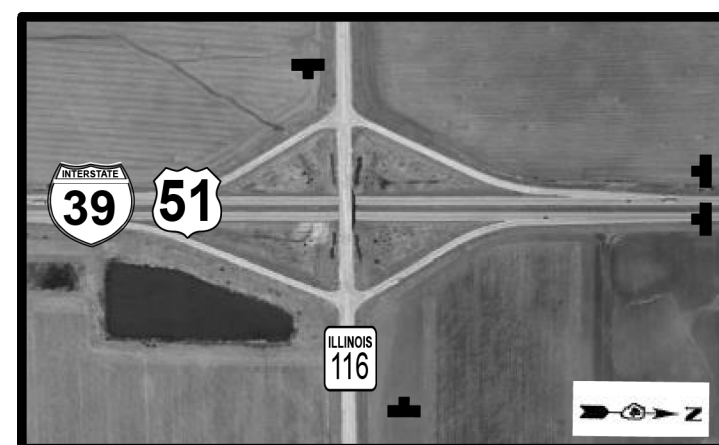
315'



25' CTS.
 CRYSTAL
 REFLECTORS

SUGGESTED TRAFFIC STAGING AND STAGE 1 NOTES:

- 1) CLOSE DRIVING LANE OF SOUTHBOUND I-39 PER STD. 701401. ADDRESS SHOULDER RUMBLE STRIPS AND SECURE DRAINAGE GRATES IN CONFLICT WITH PROPOSED STAGE 1 TRAFFIC. CLEAN SHOULDER OF STRUCTURE TO REMOVE GRAVEL/ROAD DEBRIS-IF PRESENT.
- 2) REMOVE ALL CONFLICTING PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKINGS AS SHOWN AND AS SHOWN ON HIGHWAY STANDARDS. (INCLUDING EXIT RAMP GORE-MARKINGS NOT SHOWN ON DETAIL.)
- 3) PLACE CRYSTAL REFLECTORS ON ON TAPERS AS SHOWN. (PLACEMENT, MAINTENANCE, AND REMOVAL IS INCLUDED WITH STD. 701402.
- 4) USING TWO PACE VEHICLES WITH LIGHTS, SWITCH TRAFFIC TO CLOSE THE PASSING LANE UTILIZING STD. 701402 AND AS SHOWN TO COMPLETE FINAL TRAFFIC CONTROL PLACEMENT.



MODEL: \$MODELNAME\$ FILE NAME: \$FILES\$

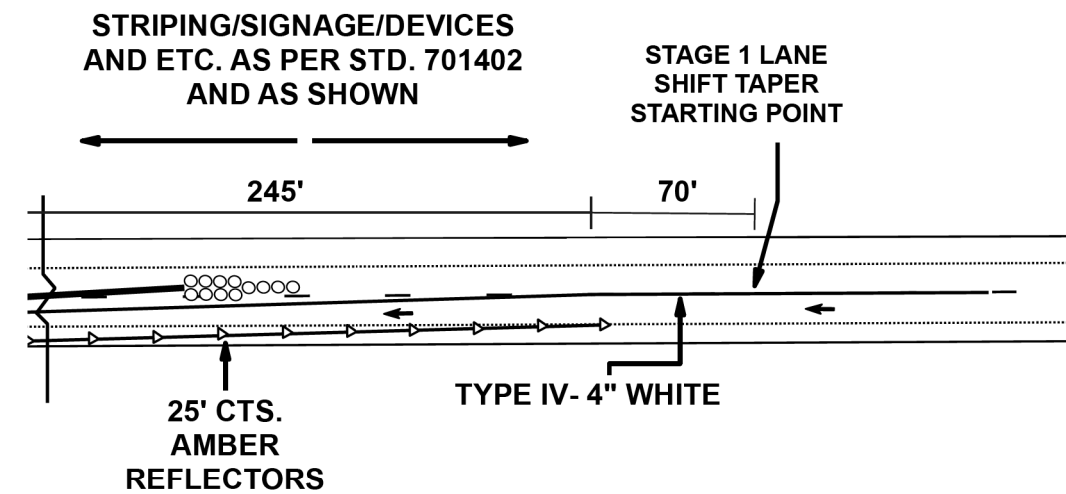
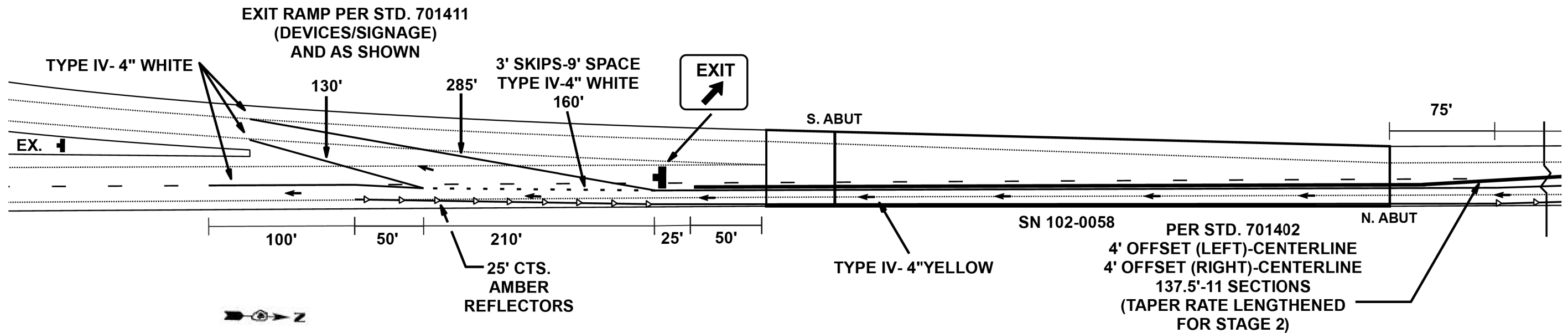
USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

BRIDGE TRAFFIC CONTROL STAGING /TRAFFIC MANAGEMENT PLAN

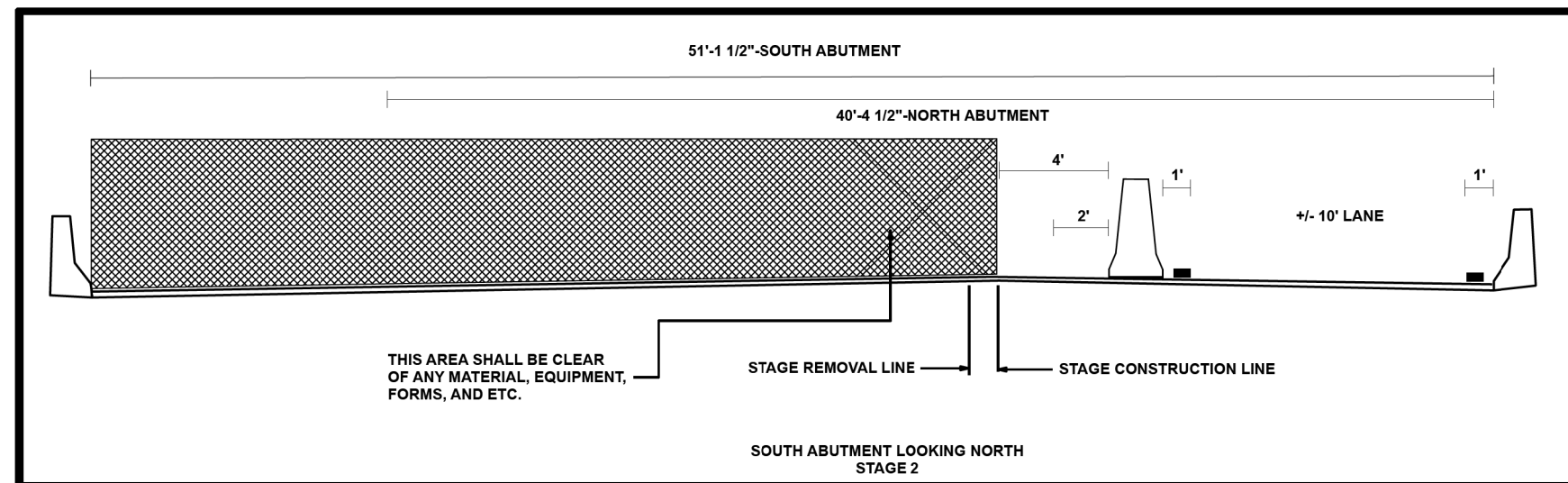
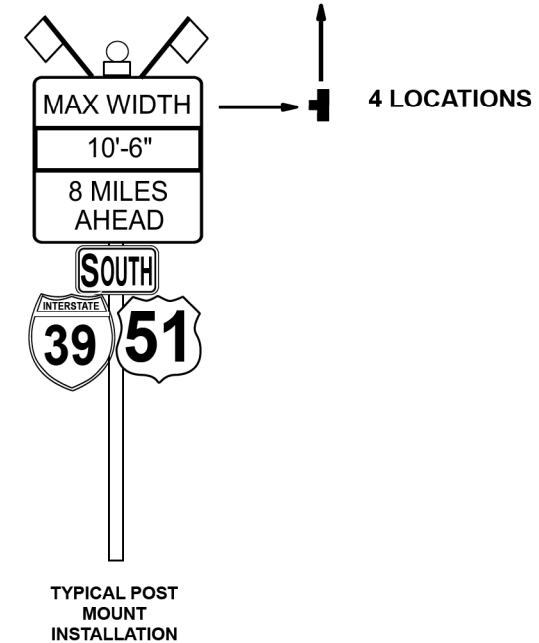
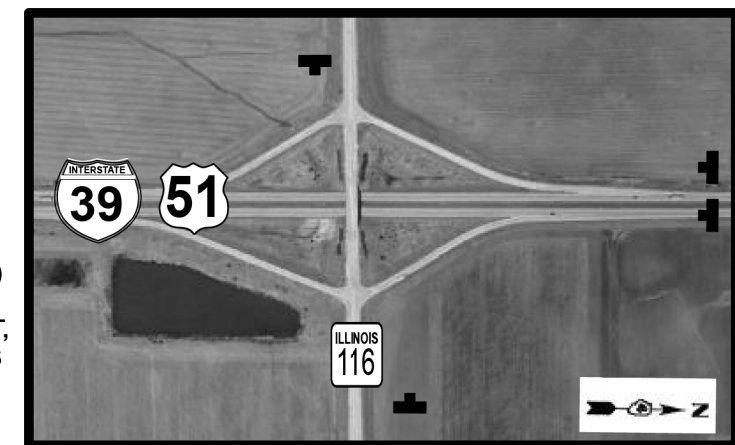
SCALE: SHEET 1 OF 10 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	57
			CONTRACT NO. 68D79	
			ILLINOIS FED. AID PROJECT	



SUGGESTED TRAFFIC STAGING AND STAGE 2 NOTES:

- 1) ADDRESS SHOULDER RUMBLE STRIPS AND SECURE DRAINAGE GRATES IN CONFLICT WITH PROPOSED STAGE 2 TRAFFIC UNDER STAGE 1 LANE CLOSURE
- 2) REMOVE ALL CONFLICTING PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKINGS AS SHOWN AND AS SHOWN ON HIGHWAY STANDARDS. (INCLUDING EXIT RAMP GORE-MARKINGS NOT SHOWN ON DETAIL-MAY HAVE BEEN REMOVED DURING STAGE 1.)
- 3) PLACE AMBER REFLECTORS ON ON TAPERS AS SHOWN. (PLACEMENT, MAINTENANCE, AND REMOVAL IS INCLUDED FOR TAPER REFLECTORS WITH STD. 701402.
- 4) USING TWO PACE VEHICLES WITH LIGHTS, SWITCH TRAFFIC TO CLOSE THE DRIVING LANE UTILIZING STD. 701402 AND AS SHOWN TO COMPLETE FINAL TRAFFIC CONTROL PLACEMENT.



MODEL: 140DELMARIES
FILE NAME: STILES

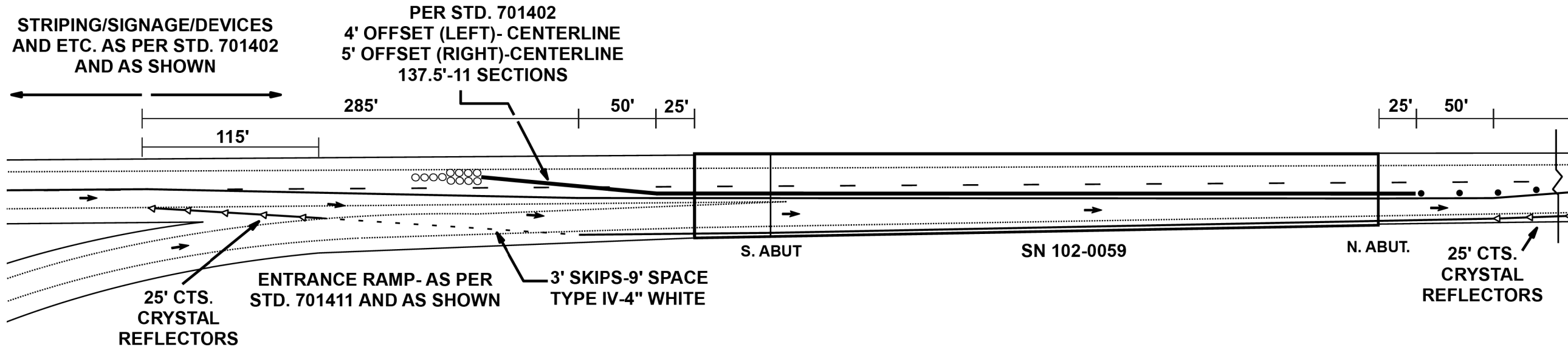
USER NAME = \$USERS	DESIGNED -	REVISED -
PLOT SCALE = \$SCALES	DRAWN -	REVISED -
PLOT DATE = \$DATES	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BRIDGE TRAFFIC CONTROL STAGING /TRAFFIC MANAGEMENT PLAN

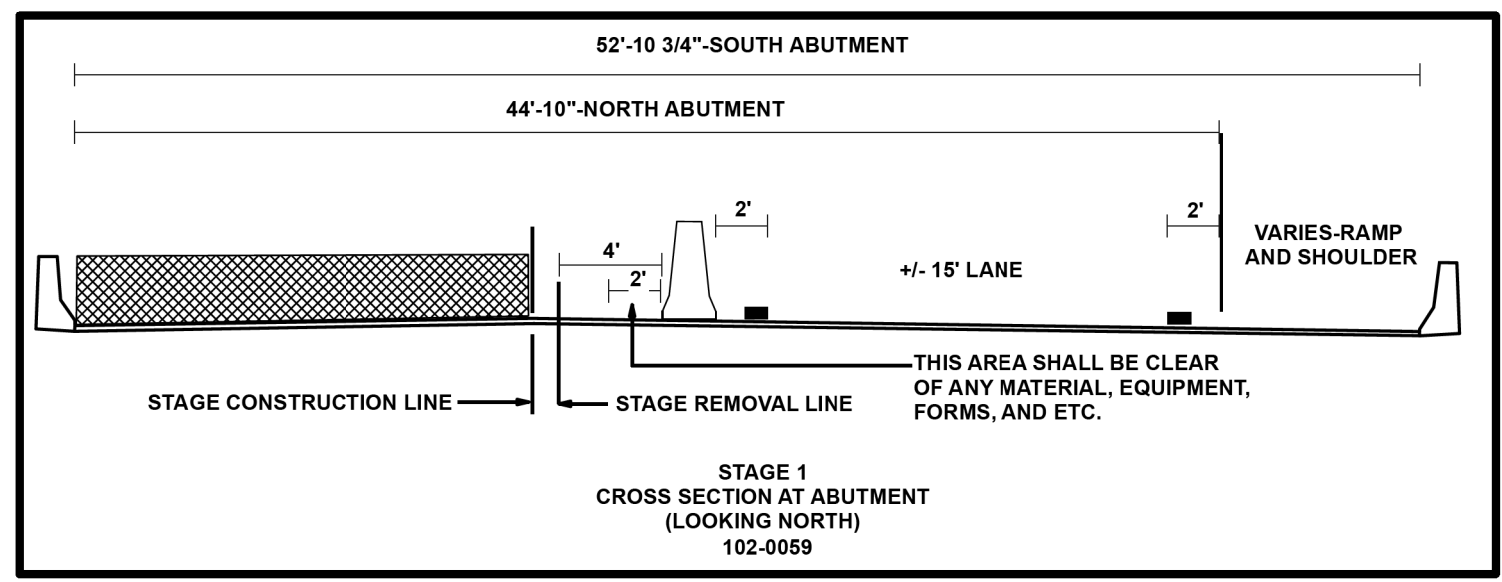
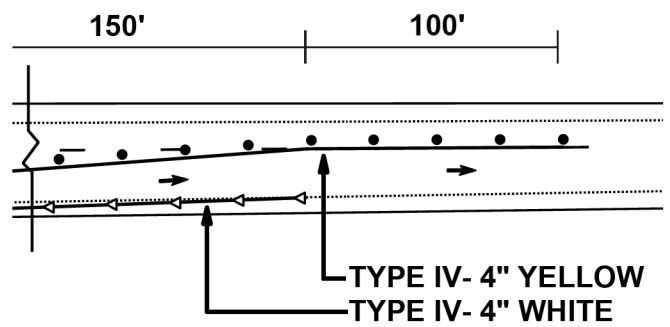
SCALE: SHEET 2 OF 10 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	58
			CONTRACT NO. 68D79	
			ILLINOIS FED. AID PROJECT	



SUGGESTED TRAFFIC STAGING AND STAGE 1 NOTES:

- 1) CLOSE DRIVING LANE OF NORTHBOUND I-39 PER STD. 701401. ADDRESS SHOULDER RUMBLE STRIPS AND SECURE DRAINAGE GRATES IN CONFLICT WITH PROPOSED STAGE 1 TRAFFIC. CLEAN SHOULDER OF STRUCTURE TO REMOVE GRAVEL/ROAD DEBRIS- IF PRESENT.
- 2) REMOVE ALL CONFLICTING PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKINGS AS SHOWN AND AS SHOWN ON HIGHWAY STANDARDS. (INCLUDING EXIT RAMP GORE-MARKINGS NOT SHOWN ON DETAIL.)
- 3) PLACE CRYSTAL REFLECTORS ON TAPERS AS SHOWN. (PLACEMENT, MAINTENANCE, AND REMOVAL IS INCLUDED WITH STD. 701402.)
- 4) USING TWO PACE VEHICLES WITH LIGHTS, SWITCH TRAFFIC TO CLOSE THE PASSING LANE UTILIZING STD. 701402 AND AS SHOWN TO COMPLETE FINAL TRAFFIC CONTROL PLACEMENT.



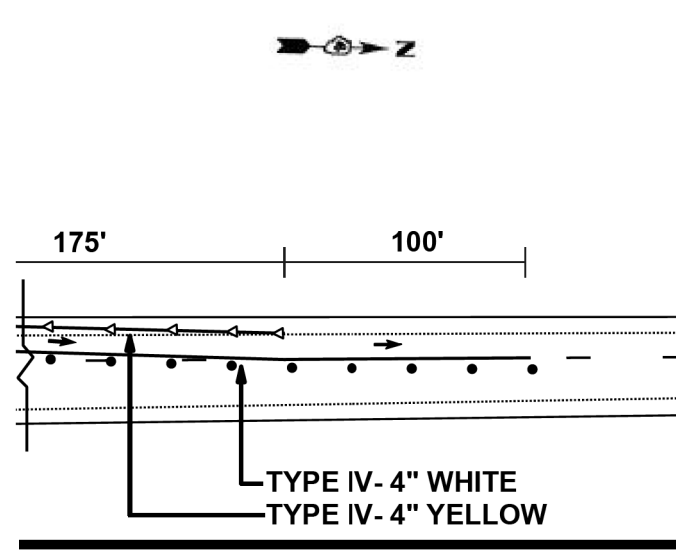
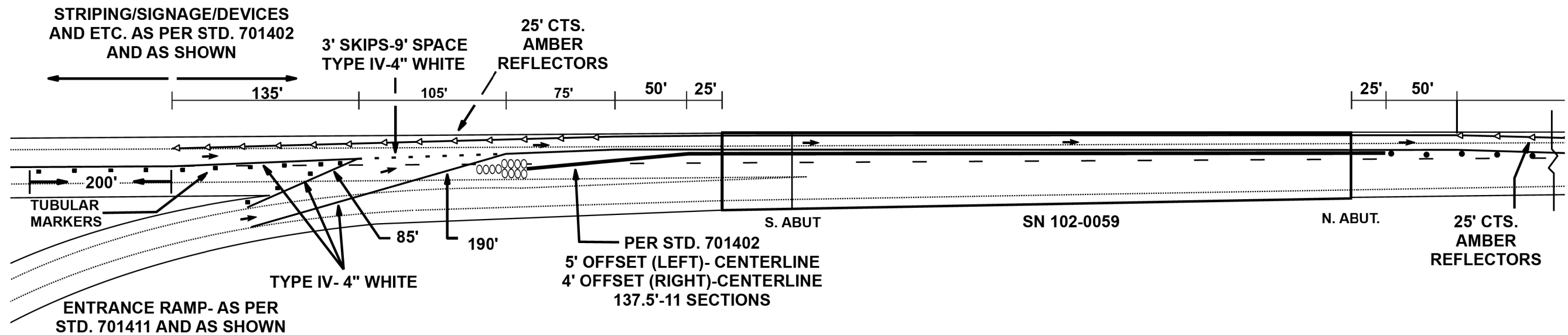
MODEL: \\MODELS\BARRIES
FILE NAME: STILES

USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

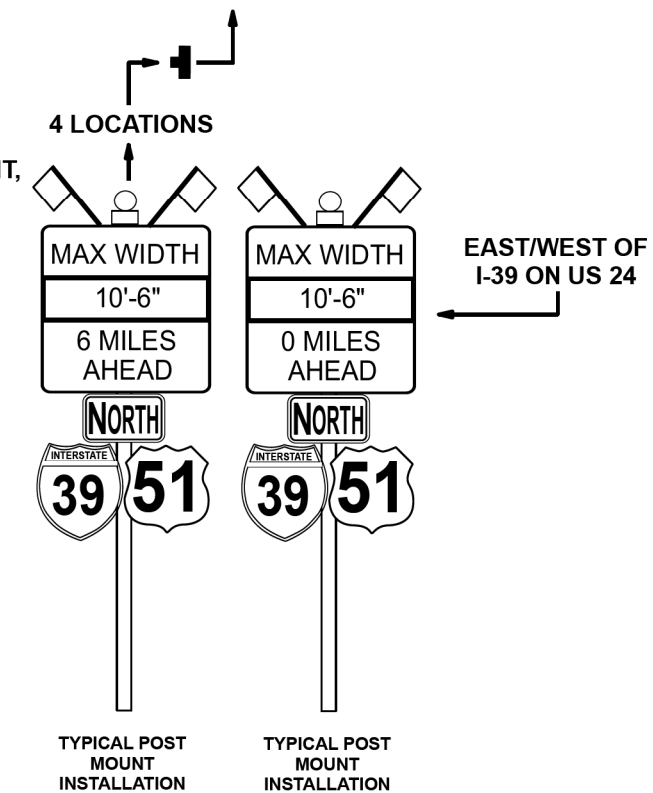
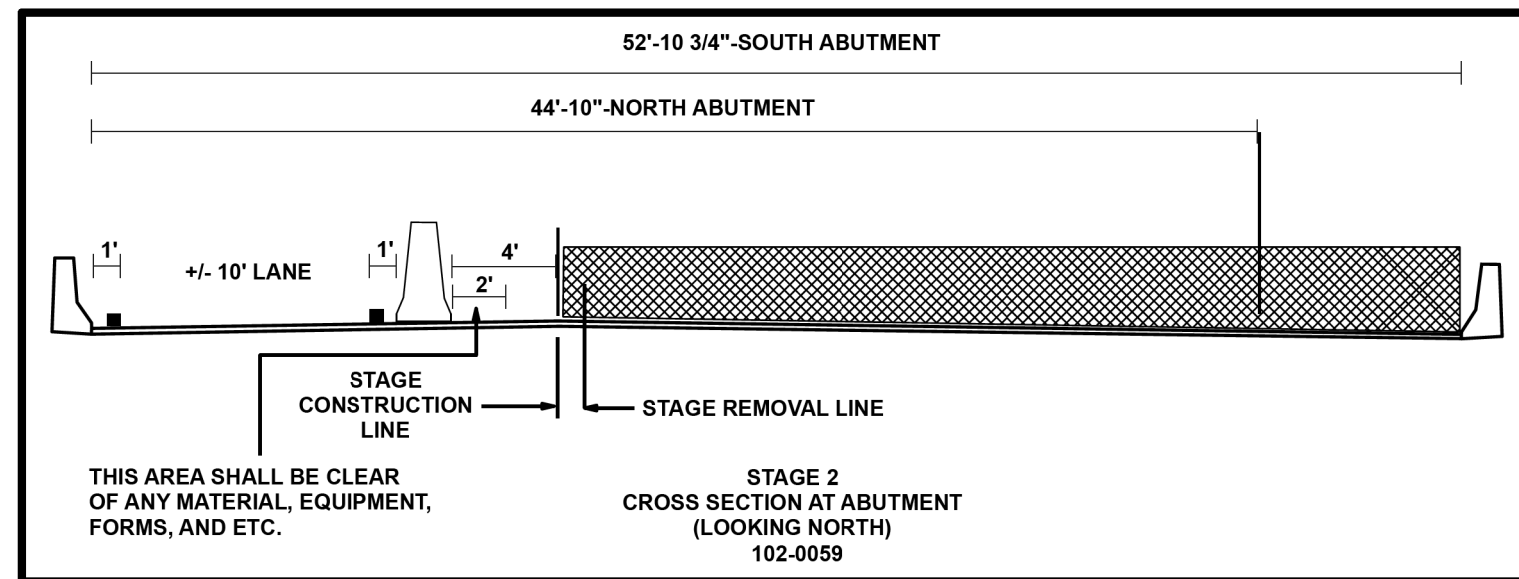
BRIDGE TRAFFIC CONTROL STAGING /TRAFFIC MANAGEMENT PLAN
SCALE: SHEET 3 OF 10 SHEETS STA. TO STA.

F.A.J RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	59
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				



SUGGESTED TRAFFIC STAGING AND STAGE 2 NOTES:

- 1) ADDRESS SHOULDER RUMBLE STRIPS AND SECURE DRAINAGE GRATES IN CONFLICT WITH PROPOSED STAGE 2 TRAFFIC UNDER STAGE 1 LANE CLOSURE
- 2) REMOVE ALL CONFLICTING PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKINGS AS SHOWN AND AS SHOWN ON HIGHWAY STANDARDS. (INCLUDING EXIT RAMP GORE-MARKINGS NOT SHOWN ON DETAIL-MAY HAVE BEEN REMOVED DURING STAGE 1.)
- 3) PLACE AMBER REFLECTORS ON ON TAPERS AS SHOWN. (PLACEMENT, MAINTENANCE, AND REMOVAL IS INCLUDED FOR TAPER REFLECTORS WITH STD. 701402.
- 4) USING TWO PACE VEHICLES WITH LIGHTS, SWITCH TRAFFIC TO CLOSE THE DRIVING LANE UTILIZING STD. 701402 AND AS SHOWN TO COMPLETE FINAL TRAFFIC CONTROL PLACEMENT.
- 5) TUBULAR MARKERS ARE FOR IMPROVING RAMP VISIBILITY. PLACEMENT, MAINTENANCE, AND REMOVAL IS INCLUDED WITH STD. 701402.



MODEL: \\MODELS\BARRIS FILES: \\MODELS: STILES

USER NAME = \$USERS	DESIGNED -	REVISED -
PLOT SCALE = \$SCALES	DRAWN -	REVISED -
PLOT DATE = \$DATES	CHECKED -	REVISED -
	DATE -	REVISED -

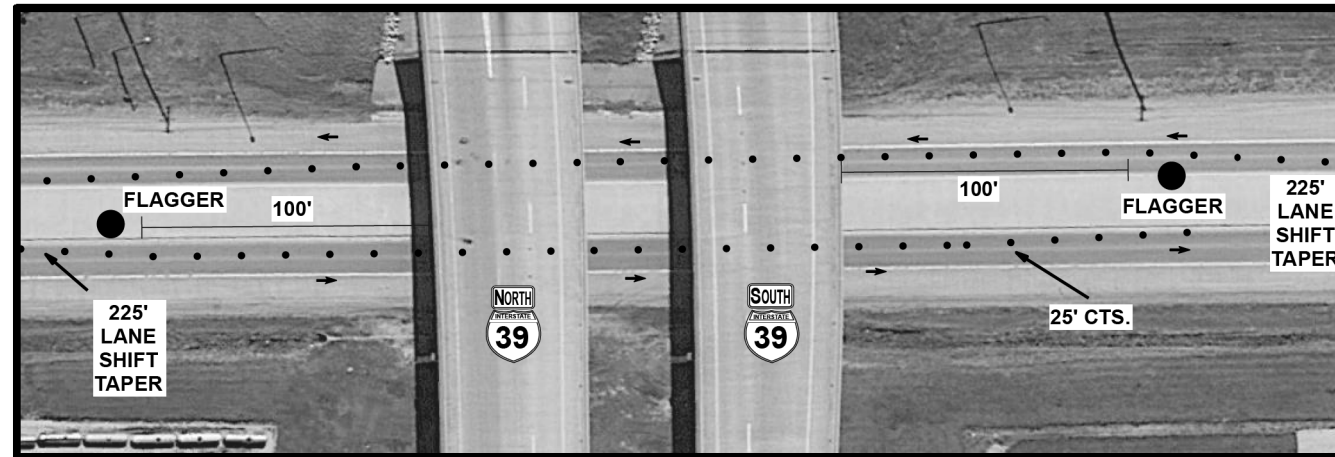
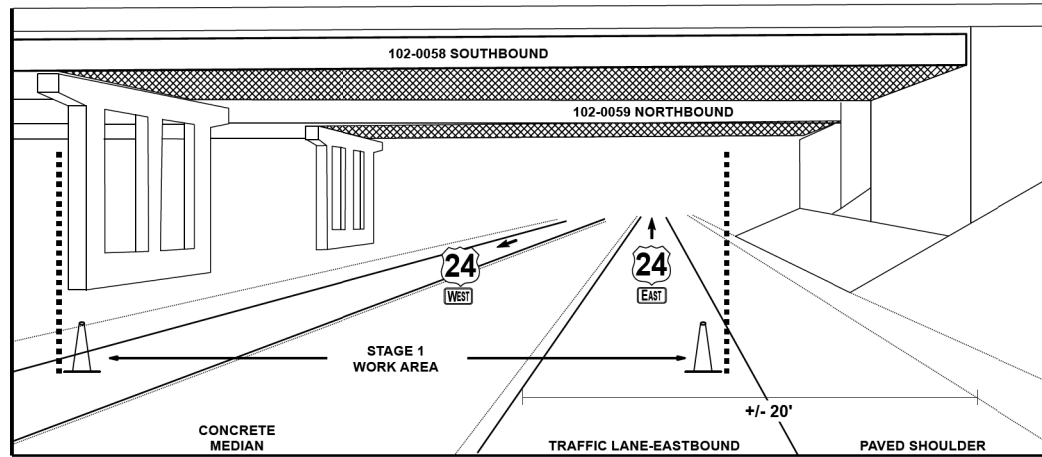
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BRIDGE TRAFFIC CONTROL STAGING /TRAFFIC MANAGEMENT PLAN

SCALE: SHEET 4 OF 10 SHEETS STA. TO STA.

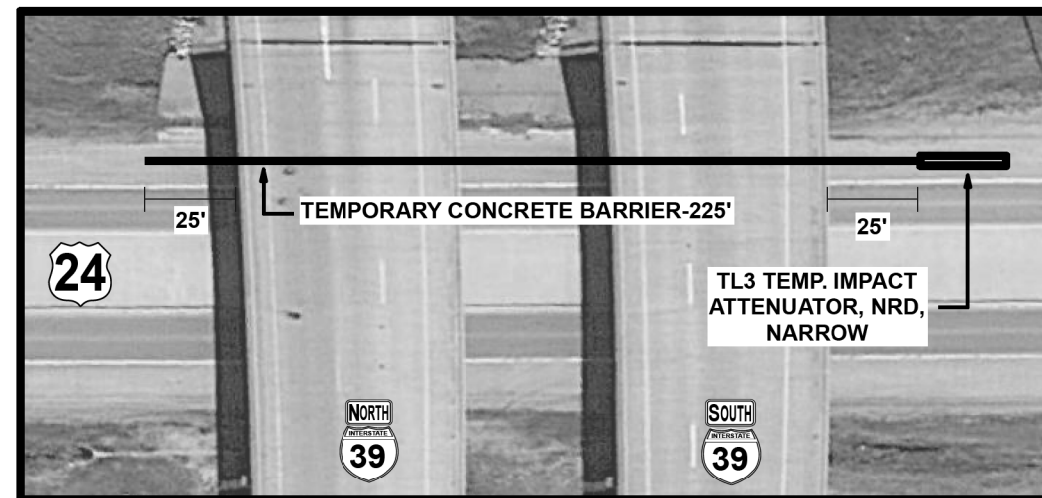
F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1;(2HVB)BRR]	WOODFORD	71	60
			CONTRACT NO. 68D79	
ILLINOIS FED. AID PROJECT				

PROTECTIVE SHIELD WORK



ADVANCED SIGNS- EACH APPROACH

BEARING/PAINTING/SOUTH ABUTMENT WORK



- 1) TEMPORARY CONCRETE BARRIER SHALL BE PLACED AS SHOWN FOR ALL BEARING WORK, PAINTING, AND OTHER COLLATERAL WORK ON THE SOUTH ABUTMENT OF BOTH STRUCTURES. THIS IS FOR THE SOUTH ABUTMENT SIDE ONLY.
- 2) CRYSTAL REFLECTORS SHALL BE PLACED ON THE TEMPORARY CONCRETE BARRIER.
- 3) TEMPORARY CONCRETE BARRIER SHALL BE PLACED 3' FROM THE EDGE OF PAVEMENT.
- 4) ALL WORK SHOWN AND DESCRIBED SHALL BE PAID FOR AS TRAFFIC CONTROL AND PROTECTION SPECIAL.



TYPICAL POST MOUNT INSTALLATION

PLACE ONE SIGN ON EASTBOUND US 24 PRIOR TO I-39 AND ONE SIGN ON SOUTHBOUND I-39 RAMP TO US 24.

PROTECTIVE SHIELD NOTES:

- 1) 1 PORTABLE CHANGEABLE MESSAGE SIGN SHALL BE PLACED 5 DAYS IN ADVANCE (EACH APPROACH) OF FLAGGING OPERATIONS TO ADVANCE WARN THE PUBLIC OF POSSIBLE DELAYS.
- 2) WORK MAY BE PERFORMED WITH A SHOULDER CLOSURE UTILIZING STD. 701006.
- 3) THE STAGING SHOWN/DESCRIBED IS SUGGESTED STAGING AND THE ORDER MAY BE CHANGED PER THE RESIDENT ENGINEER'S APPROVAL.
- 4) STAGE 2 WORK WILL CONSIST OF PARTIALLY CLOSING THE LANE AND WORKING ON THE SHOULDER AREA WITH TRAFFIC BEING SHIFTED TO THE INSIDE LANES.
- 5) THE FLAGGER MAY STOP TRAFFIC FOR BRIEF PERIODS FOR THE SAFETY OF THE WORKERS AND PUBLIC WHEN WORKING NEAR THE CENTERLINE AREA.
- 6) PROTECTIVE SHIELDING WORK SHALL TAKE PLACE PRIOR TO ANY BEARING WORK OR PAINTING.
- 7) ALL WORK SHOWN AND DESCRIBED SHALL BE PAID FOR AS TRAFFIC CONTROL AND PROTECTION SPECIAL.

MODEL: 140DELENAME
FILE NAME: 311213

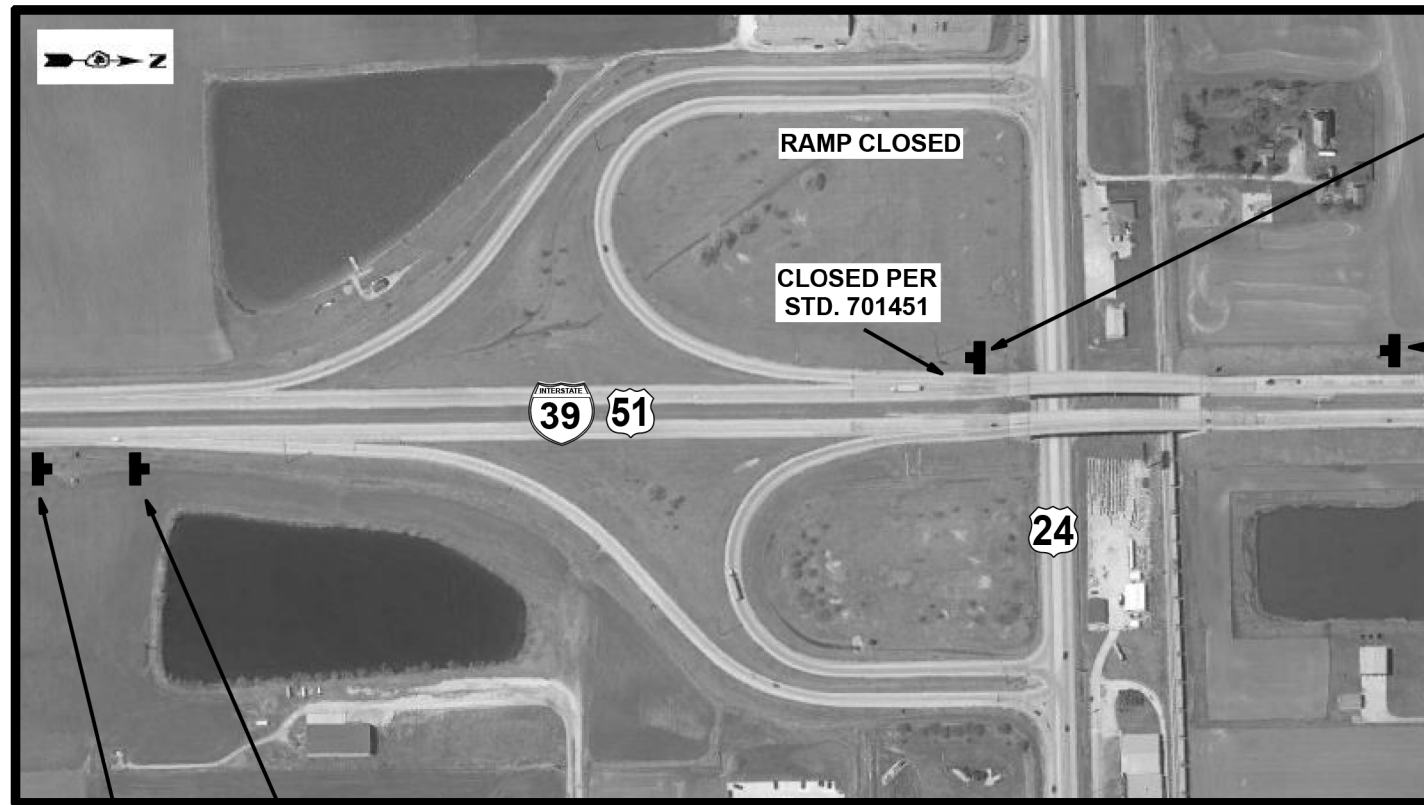
USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALES	CHECKED -	REVISED -
PLOT DATE = \$DATES	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BRIDGE TRAFFIC CONTROL STAGING /TRAFFIC MANAGEMENT PLAN

SCALE: SHEET 5 OF 10 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	61
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				



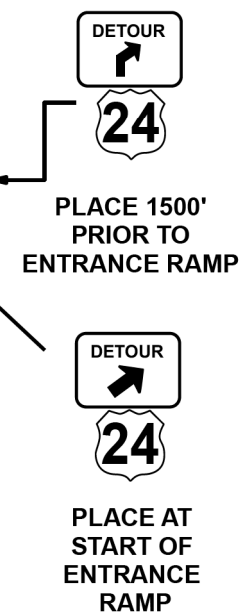
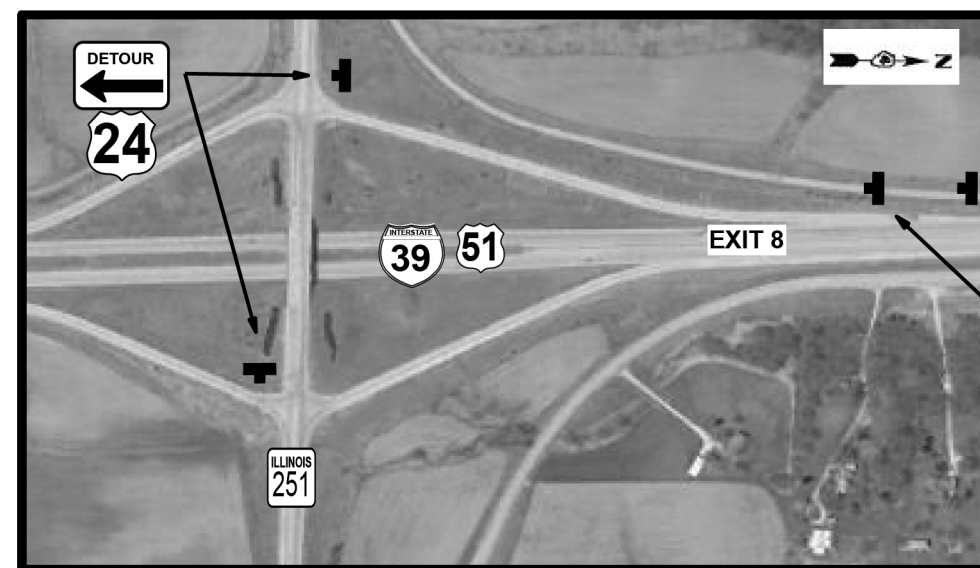
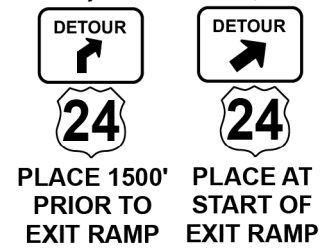
DETOUR/CLOSURE NOTES:

SIGNS MAY BE TEMPORARY MOUNTED. RESIDENT ENGINEER SHALL APPROVE TEMPORARY STANDS ARE SECURE.

LOCATION/DISTANCE MAY BE ALTERED TO FIT EXISTING FIELD CONDITIONS.

ONE PORTABLE CHANGEABLE MESSAGE SIGN SHALL BE PLACED 5 DAYS IN ADVANCE OF THE CLOSURE TO NOTIFY THE MOTORING PUBLIC.

THE RESIDENT ENGINEER AND TRAFFIC CONTROL SUPERVISOR SHALL BE NOTIFIED 21 DAYS IN ADVANCE OF ANTICIPATED CLOSURE.



MODEL: \\MODELS\NAME FILES: \\MODELS\STYLES

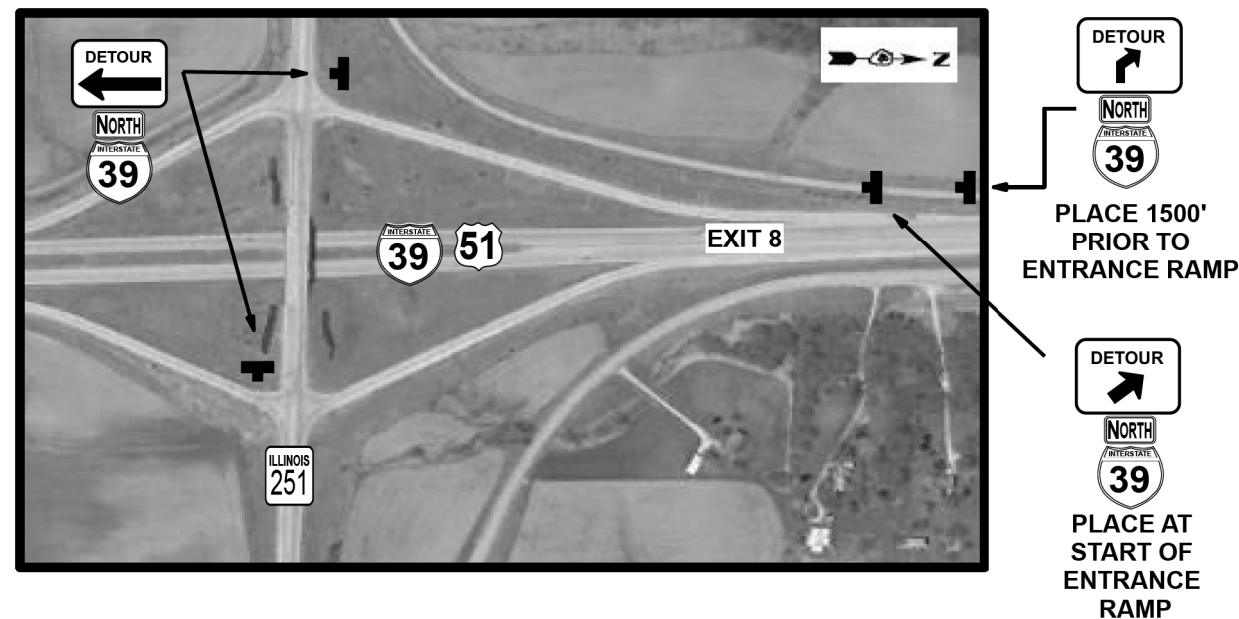
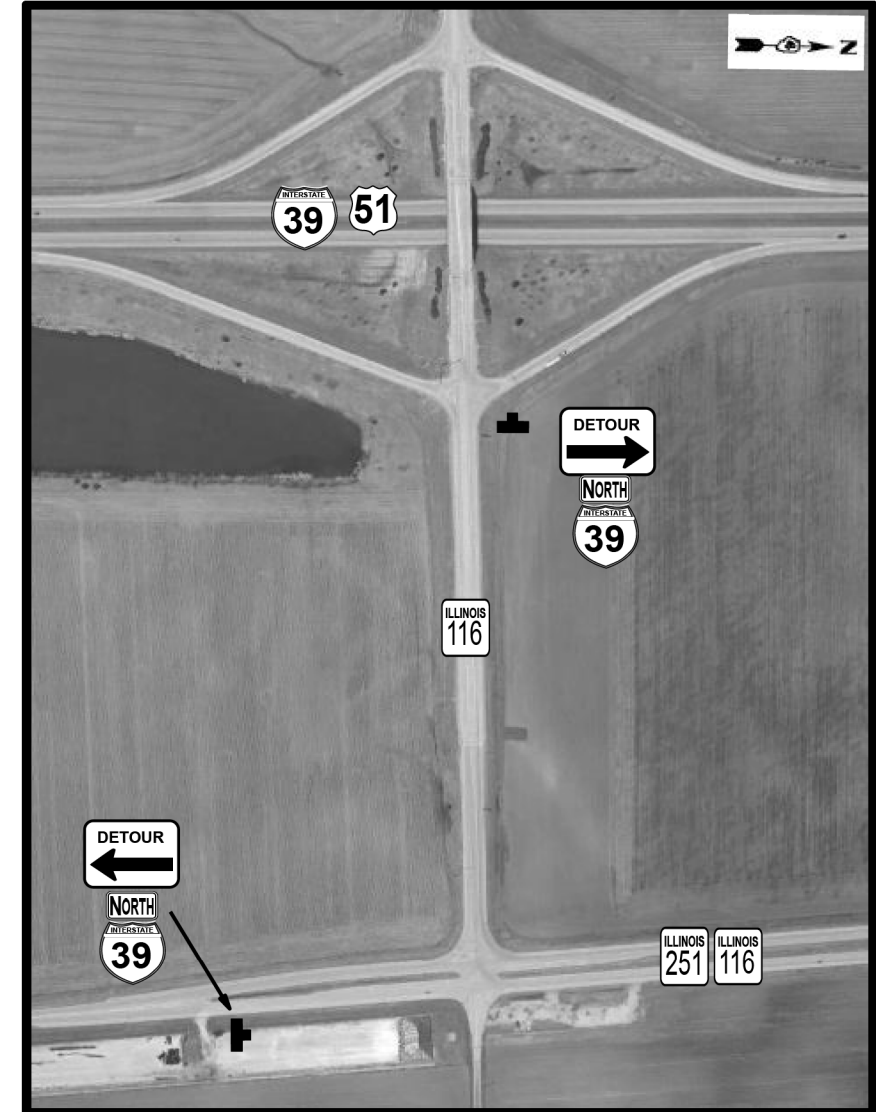
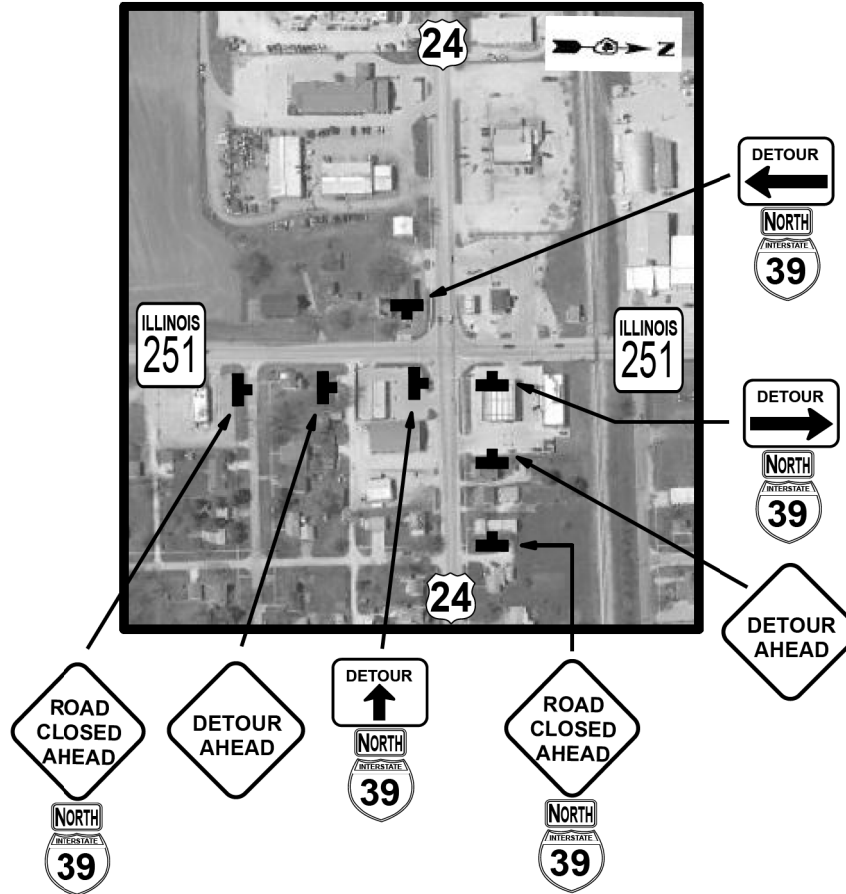
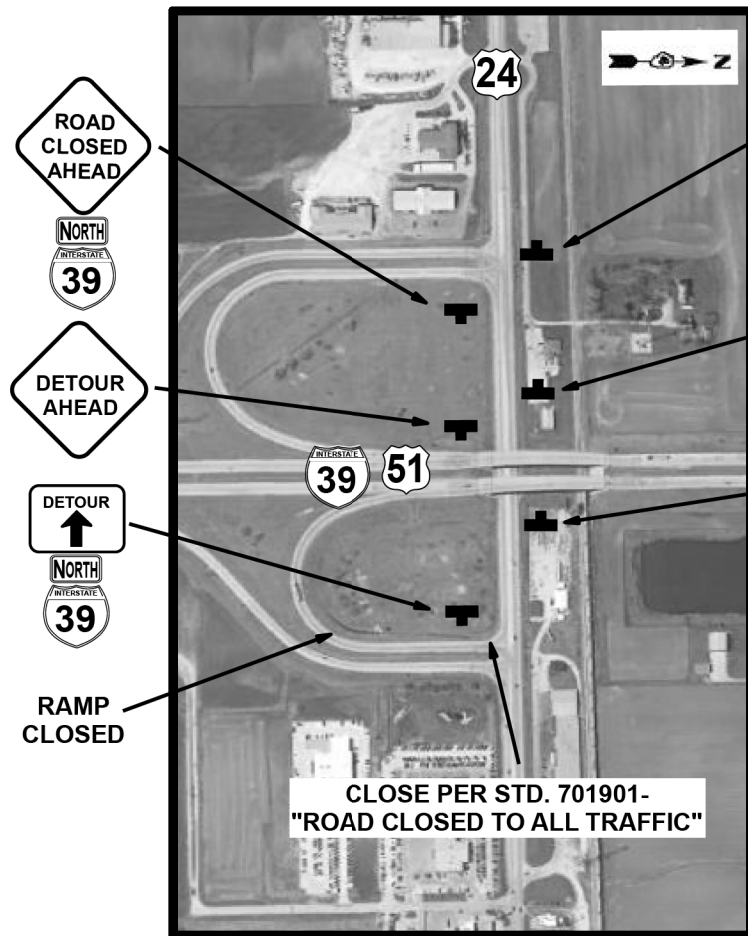
USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALES	CHECKED -	REVISED -
PLOT DATE = \$DATES	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BRIDGE TRAFFIC CONTROL STAGING /TRAFFIC MANAGEMENT PLAN

SCALE: SHEET 6 OF 10 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1;(2HVB)BRR]	WOODFORD	71	62
			CONTRACT NO. 68D79	
		ILLINOIS	FED. AID PROJECT	



DETOUR/CLOSURE NOTES:

SIGNS MAY BE TEMPORARY MOUNTED. RESIDENT ENGINEER SHALL APPROVE TEMPORARY STANDS ARE SECURE.

LOCATION/DISTANCE MAY BE ALTERED TO FIT EXISTING FIELD CONDITIONS.

ONE PORTABLE CHANGEABLE MESSAGE SIGN SHALL BE PLACED 5 DAYS IN ADVANCE OF THE CLOSURE TO NOTIFY THE MOTORING PUBLIC.

THE RESIDENT ENGINEER AND TRAFFIC CONTROL SUPERVISOR SHALL BE NOTIFIED 21 DAYS IN ADVANCE OF ANTICIPATED CLOSURE.

MODEL: \\MODELS\NAME\FILES\NAME.PLT

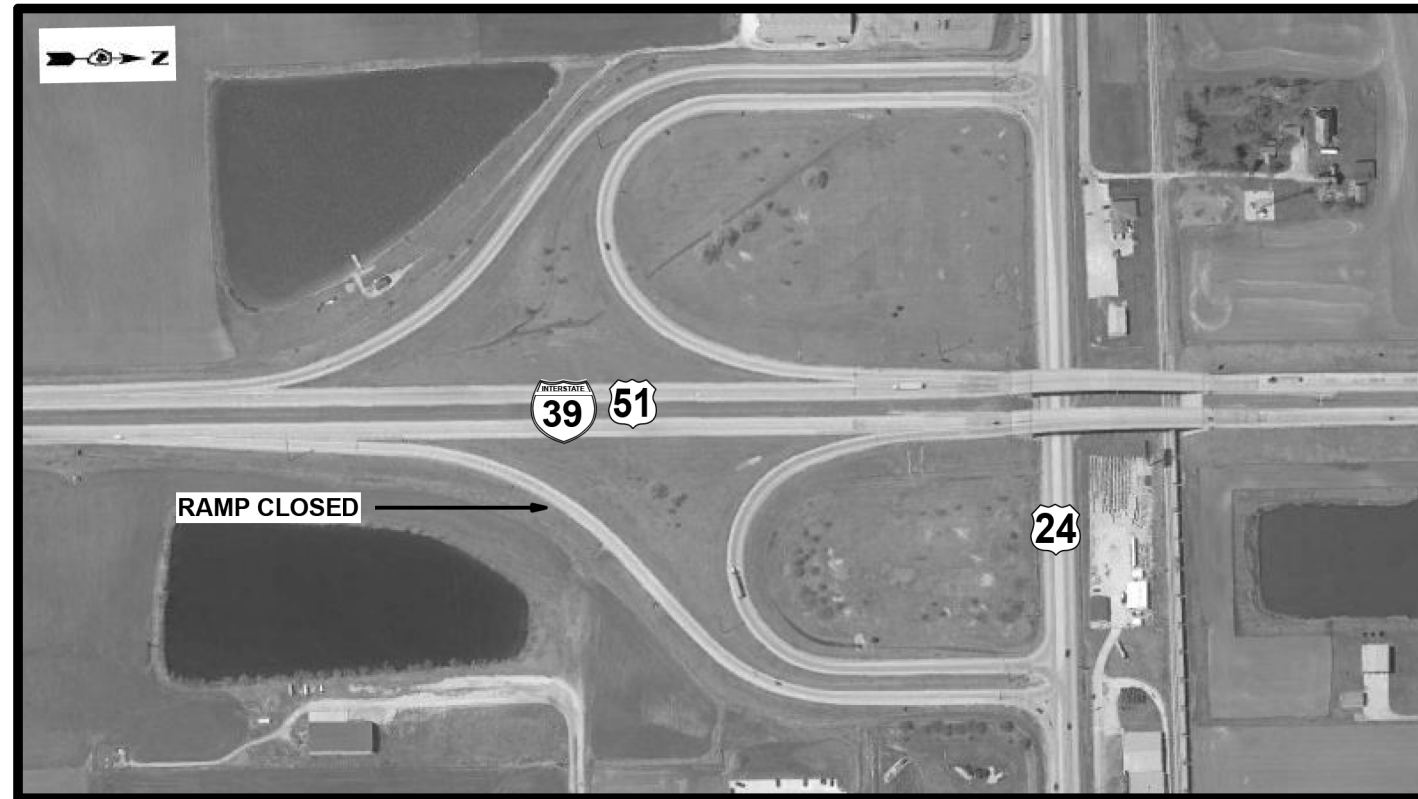
USER NAME = \$USERS	DESIGNED -	REVISED -
PLOT SCALE = \$SCALE\$	DRAWN -	REVISED -
PLOT DATE = \$DATE\$	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

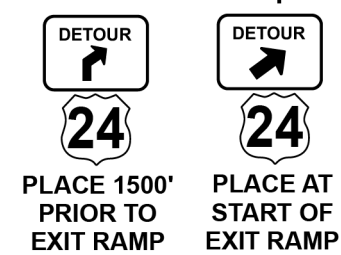
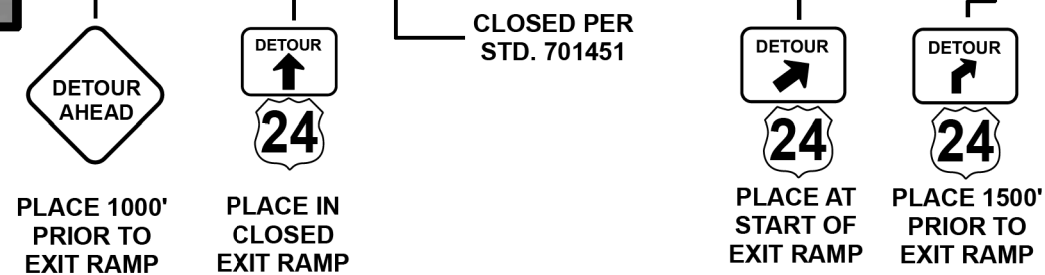
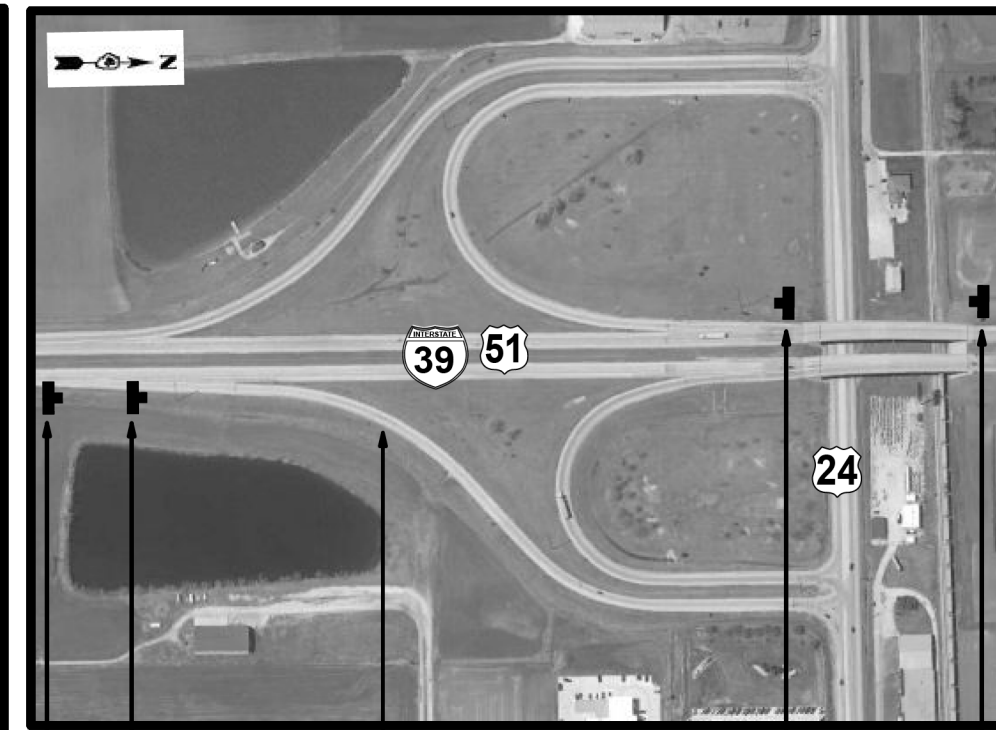
BRIDGE TRAFFIC CONTROL STAGING /TRAFFIC MANAGEMENT PLAN

SCALE: SHEET 7 OF 10 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1;(2HVB)BRR]	WOODFORD	71	63
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				



LOCATION MAP



DETOUR/CLOSURE NOTES:

SIGNS MAY BE TEMPORARY MOUNTED. RESIDENT ENGINEER SHALL APPROVE TEMPORARY STANDS ARE SECURE.

LOCATION/DISTANCE MAY BE ALTERED TO FIT EXISTING FIELD CONDITIONS.

ONE PORTABLE CHANGEABLE MESSAGE SIGN SHALL BE PLACED 5 DAYS IN ADVANCE OF THE CLOSURE TO NOTIFY THE MOTORING PUBLIC.

THE RESIDENT ENGINEER AND TRAFFIC CONTROL SUPERVISOR SHALL BE NOTIFIED 21 DAYS IN ADVANCE OF ANTICIPATED CLOSURE.

MODEL: 140DELMAMES
FILE NAME: 87123

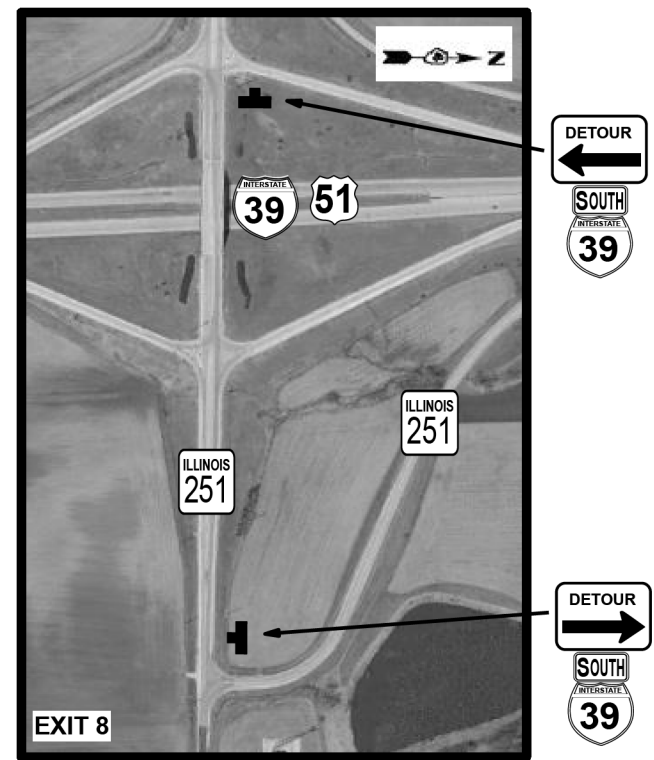
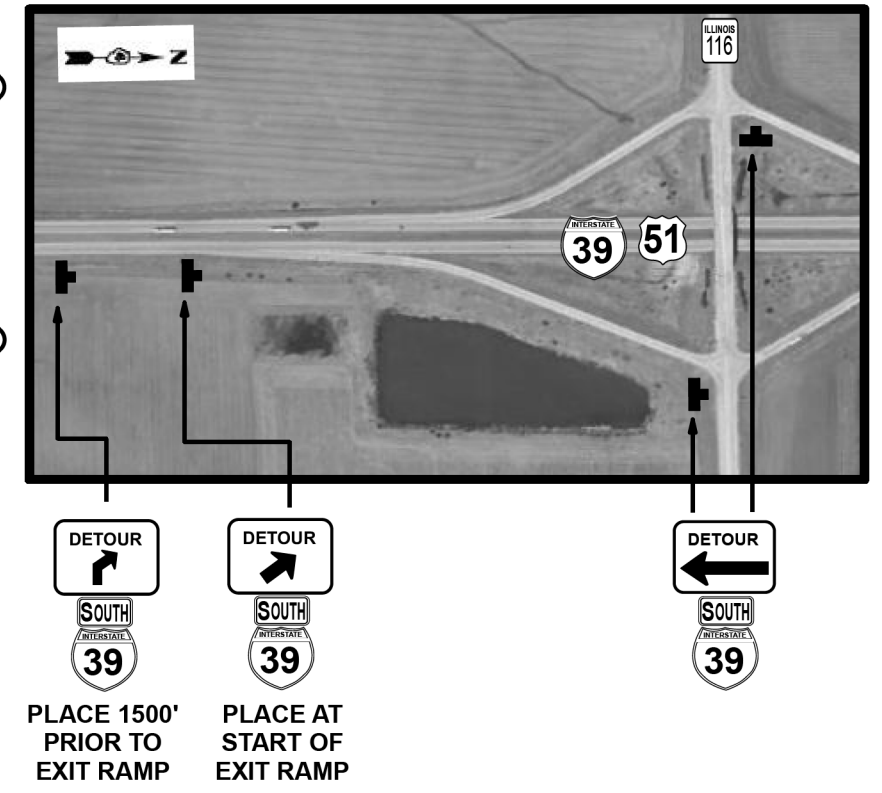
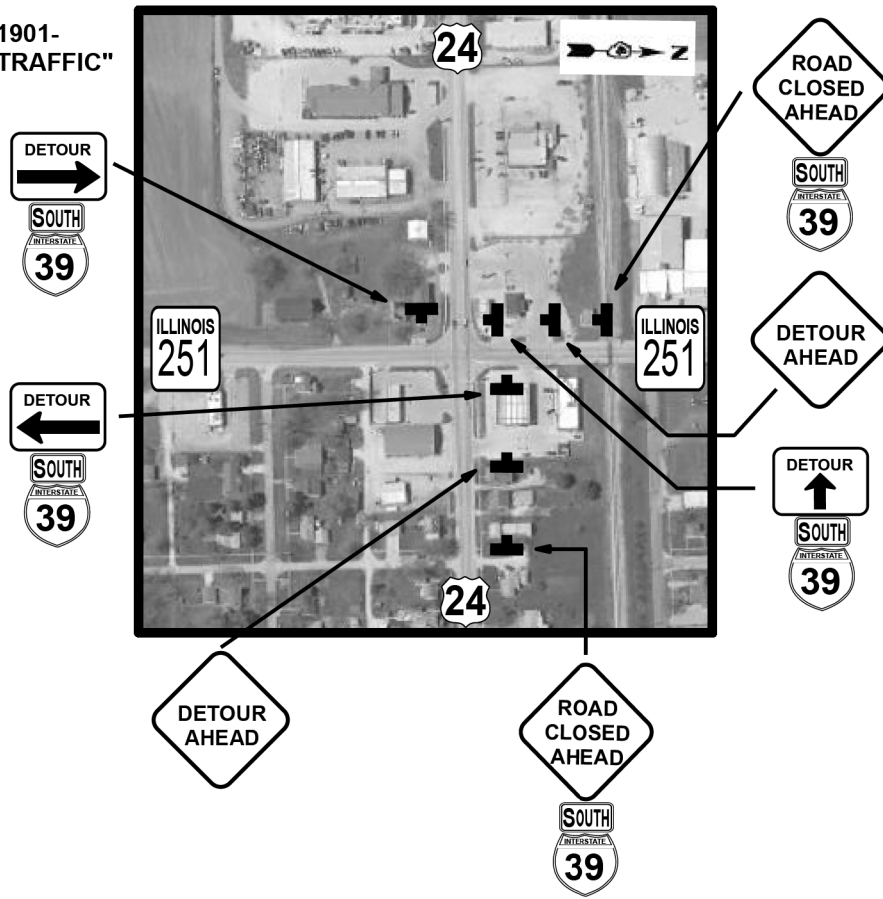
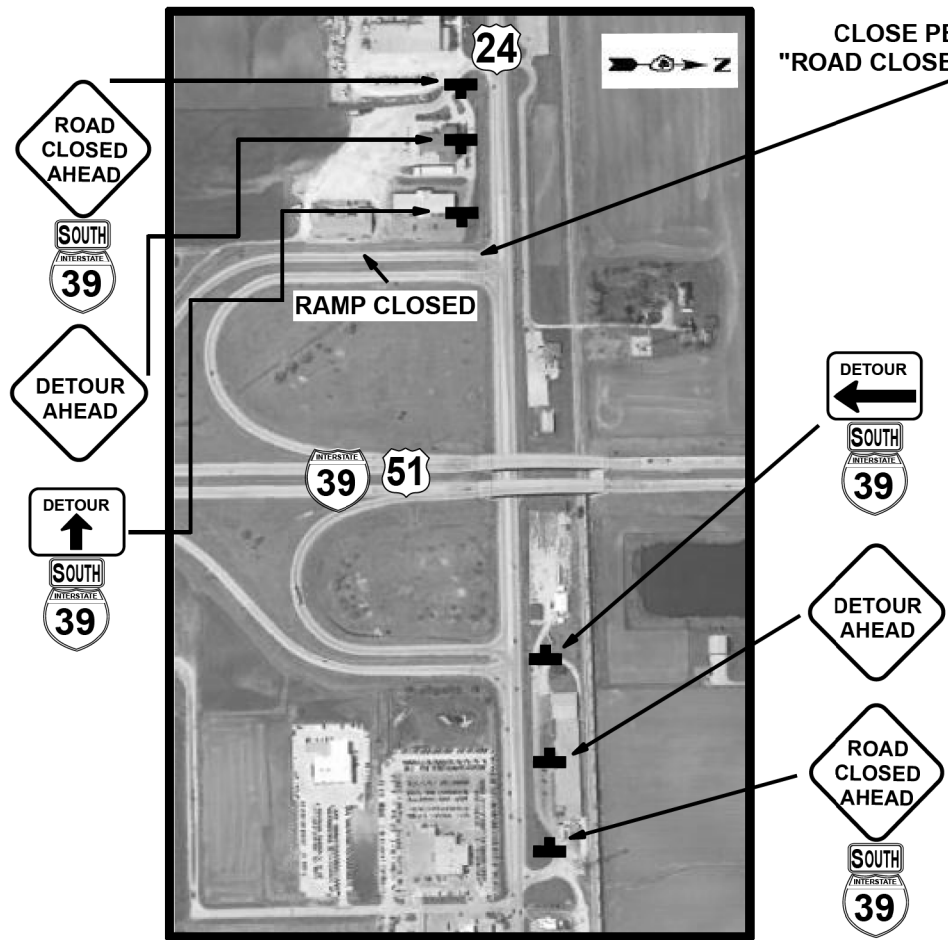
USER NAME = \$USERS	DESIGNED -	REVISED -
PLOT SCALE = \$\$SCALES	DRAWN -	REVISED -
PLOT DATE = \$DATES	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BRIDGE TRAFFIC CONTROL STAGING /TRAFFIC MANAGEMENT PLAN

SCALE: SHEET 8 OF 10 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	64
				CONTRACT NO. 68D79
		ILLINOIS	FED. AID PROJECT	



DETOUR/CLOSURE NOTES:

SIGNS MAY BE TEMPORARY MOUNTED. RESIDENT ENGINEER SHALL APPROVE TEMPORARY STANDS ARE SECURE.

LOCATION/DISTANCE MAY BE ALTERED TO FIT EXISTING FIELD CONDITIONS.

ONE PORTABLE CHANGEABLE MESSAGE SIGN SHALL BE PLACED 5 DAYS IN ADVANCE OF THE CLOSURE TO NOTIFY THE MOTORING PUBLIC.

THE RESIDENT ENGINEER AND TRAFFIC CONTROL SUPERVISOR SHALL BE NOTIFIED 21 DAYS IN ADVANCE OF ANTICIPATED CLOSURE.



← TYPICAL-WHITE ON BLUE



← TYPICAL-BLACK ON WHITE

MODEL: \\MODELS\NAME\FILES\NAME.DWG; PLOT: 11/11/2011 10:11:11 AM

USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BRIDGE TRAFFIC CONTROL STAGING /TRAFFIC MANAGEMENT PLAN

SCALE: SHEET 9 OF 10 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	65
			CONTRACT NO. 68D79	
ILLINOIS FED. AID PROJECT				

TRAFFIC CONTROL AND PROTECTION ADDITIONAL NOTES:

DETAIL DRAWINGS SHOWN ON PLAN SHEETS ARE SHOWN AS ACCURATELY AS POSSIBLE. ACTUAL FIELD CONDITIONS WILL DICTATE FINAL PLACEMENT OF TRAFFIC CONTROL DEVICES AND STAGING LAYOUT. EVERY ATTEMPT WAS MADE TO BE AS ACCURATE AS POSSIBLE.

DETAIL DRAWINGS SHOWN ARE NOT DRAWN TO SCALE. SIGN PLACEMENT SHALL BE AS PER THE HIGHWAY STANDARDS OR AS DIRECTED BY THE RESIDENT ENGINEER.

WIDTH RESTRICTION

THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND TRAFFIC CONTROL SUPERVISOR 21 DAYS IN ADVANCE AS REQUIRED PER THE WIDTH RESTRICTION SPECIAL PROVISION.

MAINLINE PAVING WORK SHALL REQUIRE WIDTH RESTRICTION SIGNING. A TOTAL OF 8 SIGNS SHALL BE REQUIRED AT THE FOLLOWING LOCATIONS:

4 SIGNS- NB I-39 EXIT 8 (2- NB I-39 & 2- IL 251) 4 SIGNS- SB I-39 EXIT 22 (2-SB I-39 & 2-IL 116)

MAX WIDTH FOR THESE LOCATIONS SHALL BE 13'-6". NOTE: STRUCTURE WORK MAY HAVE WIDTH RESTRICTION SIGNING IN PLACE AND THE NARROWEST WIDTH SHALL BE USED.

SN 102-0058 & SN 102-0059

STAGE 2- THE WORK ZONE SPEED LIMIT FOR BOTH STRUCTURES DURING STAGE 2 ONLY SHALL BE 45 MPH SPEED LIMIT.



THE ATTENUATOR SYMBOL SHOWN ON THE PLAN SHEETS IS FOR VISUAL PURPOSES ONLY. THE SUMMARY OF QUANTITIES LISTS AN IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE, NARROW) TL 3 AND THIS SHALL BE USED.

TUBULAR MARKERS AS SHOWN ON THE PLAN SHEETS SHALL MEET THE REQUIREMENTS AS SPECIFIED IN THE STANDARD SPECIFICATIONS. COST OF PLACEMENT, MAINTENANCE, AND REMOVAL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION, STANDARD 701402. THE INTENT OF THE TUBULAR MARKERS IS TO ALLOW BETTER VISIBILITY FOR THE MOTORING PUBLIC WHILE THE ENTRANCE RAMP ACCELERATION LANE IS REDUCED DUE TO STAGED CONSTRUCTION.

ONE PORTABLE CHANGEABLE MESSAGE SIGN (PCMS) SHALL BE PLACED 100' BEYOND THE END OF THE APPROACH TAPER FOR STAGE 1 AND STAGE 2- SN 102-0059. THE PCMS SHALL BE IN PLACE FOR THE DURATION OF BOTH STAGES. THE PCMS SHALL READ THE FOLLOWING:

RAMP	POSSIBLE
MERGING	SLOW
AHEAD	TRAFFIC

PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS)

PCMS (OTHER THAN THOSE SHOWN ON HIGHWAY STANDARDS) SHALL BE UTILIZED AS DESCRIBED HEREIN AND AS SPECIFIED IN THE SPECIAL PROVISIONS AND/OR PLAN SHEETS.

I-39

ONE PCMS SHALL BE PLACED SEVEN DAYS IN ADVANCE OF LANE CLOSURE WORK (EACH DIRECTION) TO ADVANCE WARN THE MOTORING PUBLIC OF UPCOMING ROADWORK.



DUAL DISPLAYS
NB I-39 & SB I-39

SINGLE DISPLAY
US 24 RAMP TO SB I-39



DUAL DISPLAYS
NB I-39 & SB I-39

SINGLE DISPLAY
I-39 RAMPS TO US 24

MODEL: 140DELMAMES
FILE NAME: 31113

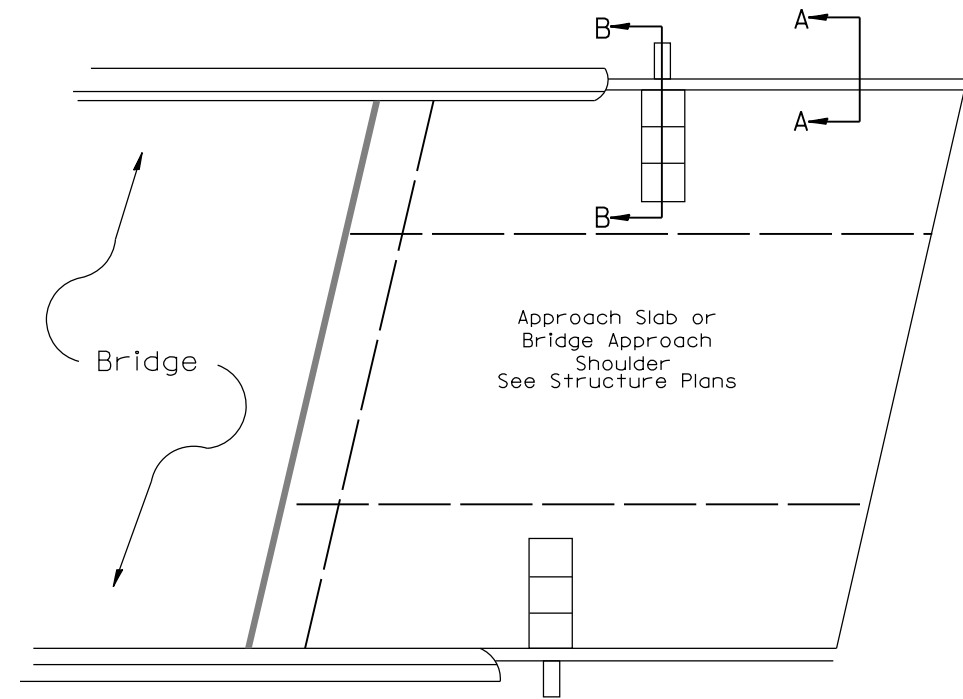
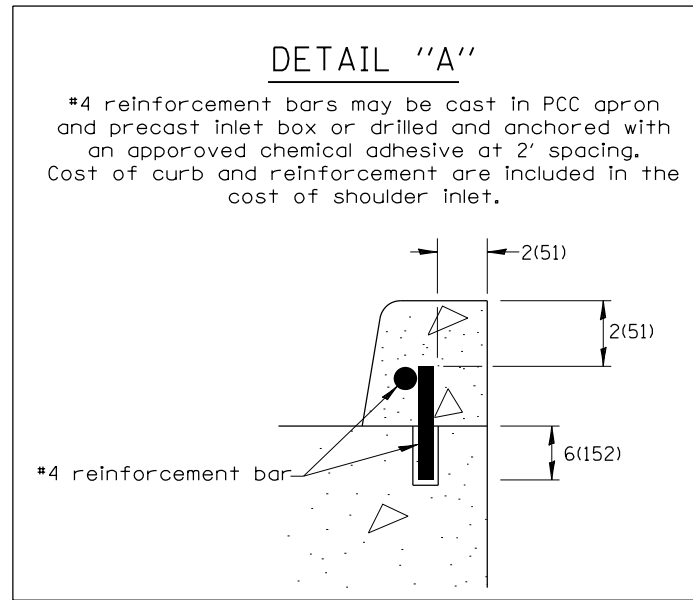
USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

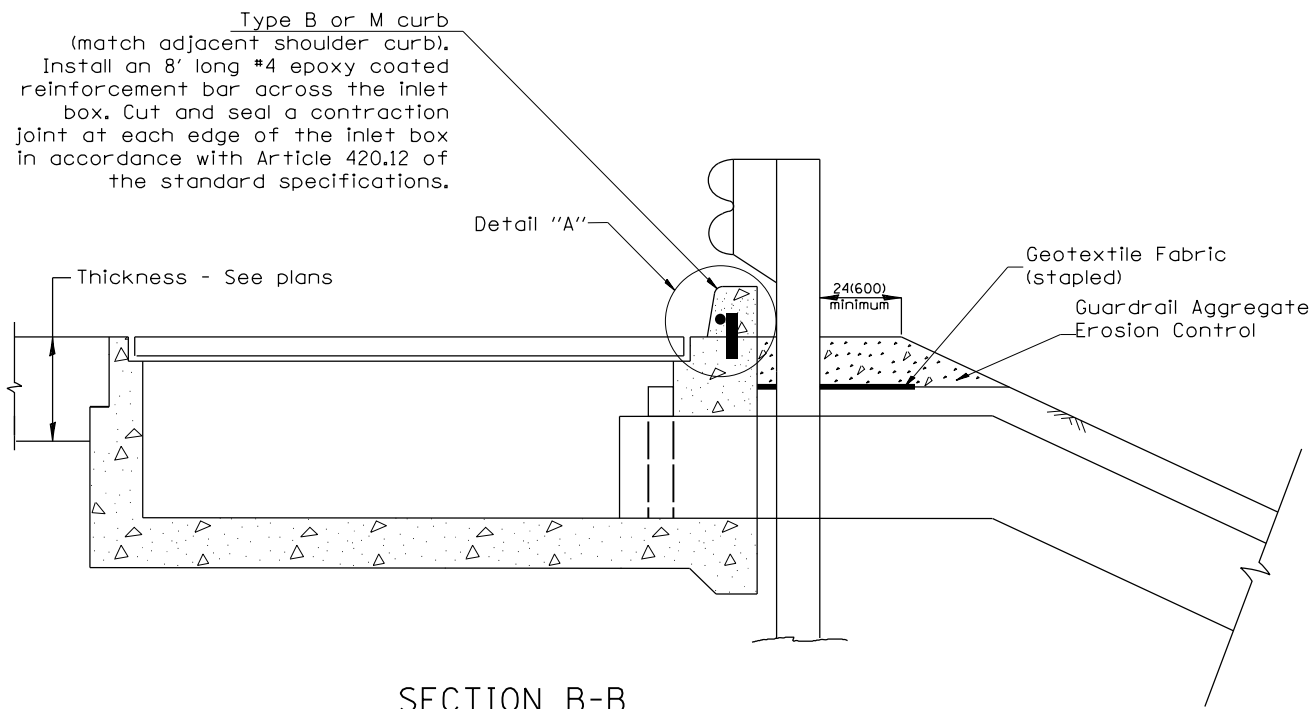
BRIDGE TRAFFIC CONTROL STAGING /TRAFFIC MANAGEMENT PLAN

SCALE: SHEET 10 OF 10 SHEETS STA. TO STA.

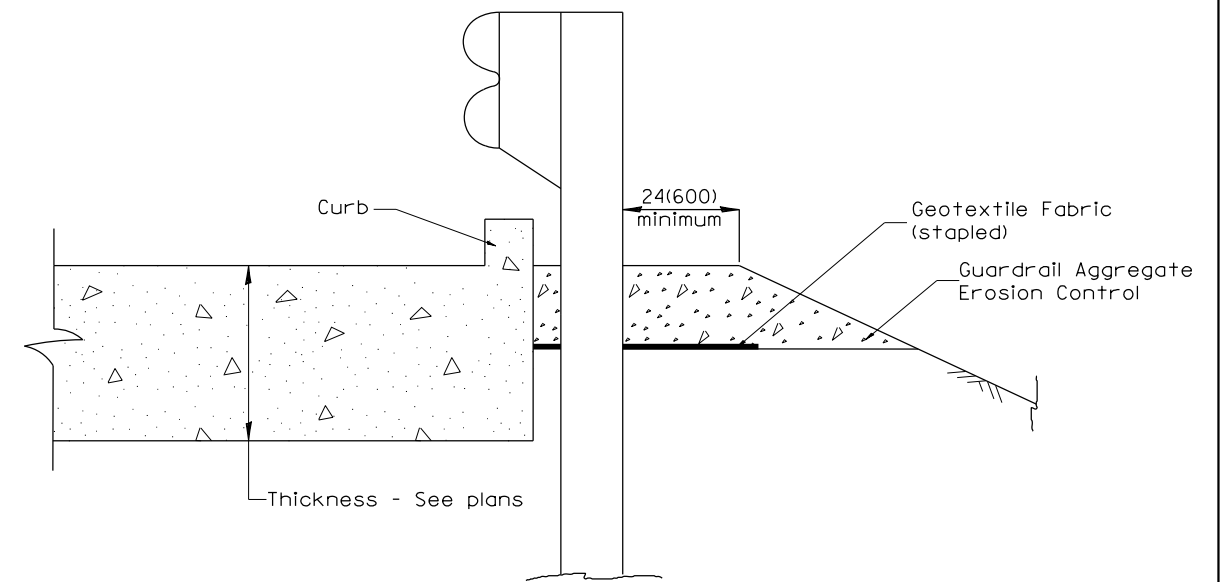
F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	102[1RS,2RS-1(2HVB)BRR]	WOODFORD	71	66
CONTRACT NO. 68D79				
ILLINOIS FED. AID PROJECT				



PLAN VIEW
APPROACH SLAB OR SHOULDER PLACEMENT



SECTION B-B
TYPICAL SECTION AT INLETS
TYPE E, F & G (HIGHWAY STANDARD 610001)



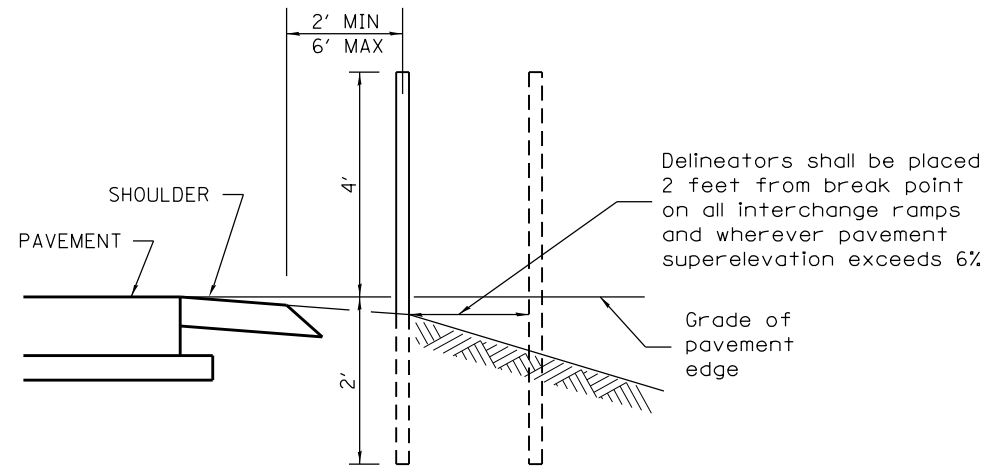
SECTION A-A
TYPICAL SECTION WITH BRIDGE APPROACH CURB

All dimensions are in inches (millimeters) unless otherwise noted.

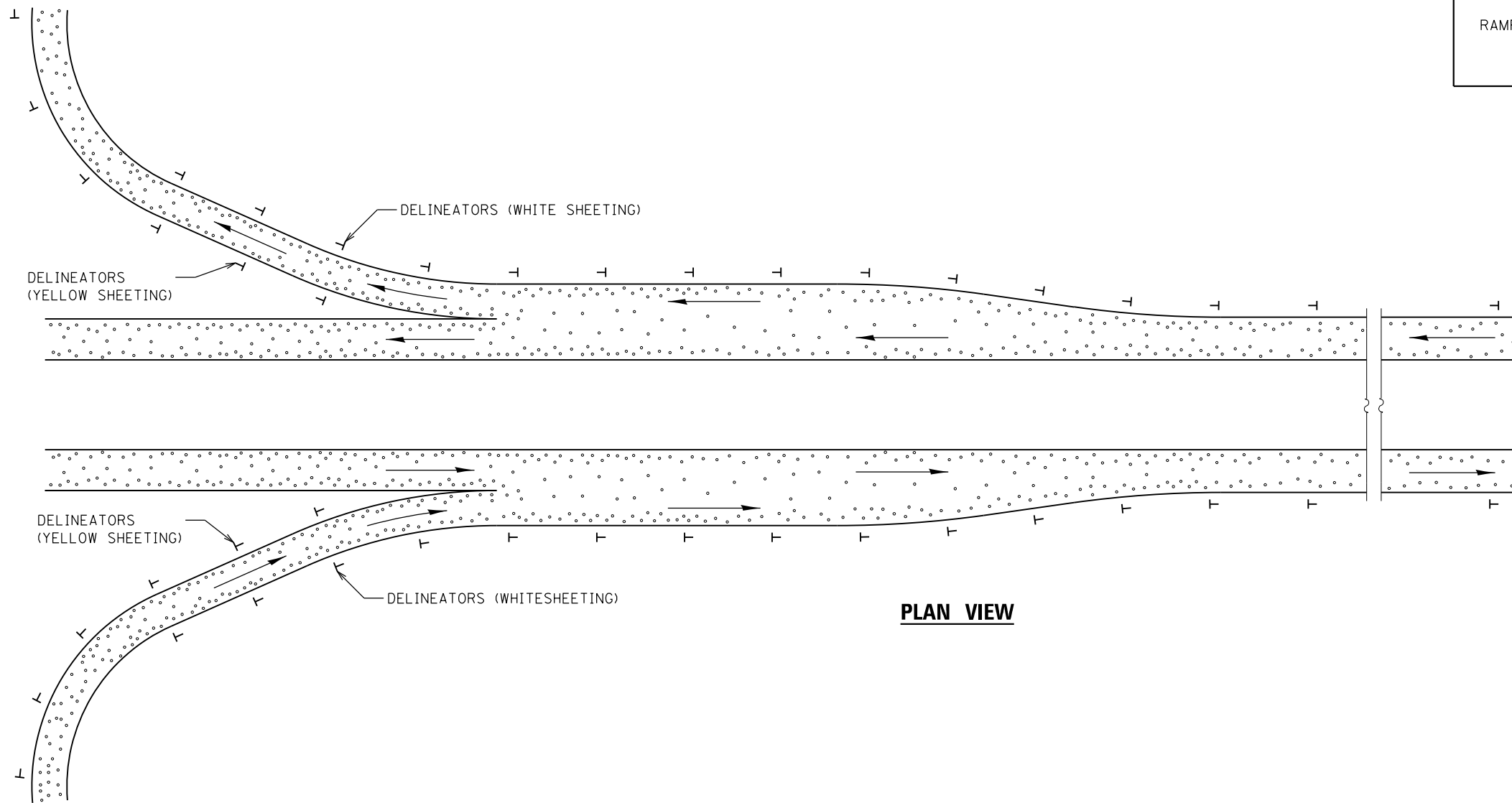
STATE OF ILLINOIS				GUARDRAIL EROSION CONTROL TREATMENTS				SHT. 2 OF 2	
DEPARTMENT OF TRANSPORTATION				NOT TO SCALE				CADD STD. 630101-D4	
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.					
39	1021RS,2RS-1+2HVB[BRR]	WOODFORD	71	68					
					CONTRACT NO. 68D79				
					FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

**SPACING FOR
RECOVERABLE DELINEATORS
ON HORIZONTAL CURVES**

LOCATION	RADIUS OF CURVE (FEET)	SPACING ON CURVE (FEET)
US 24 INTERCHANGE		
RAMP A		
CURVE 1	764	80
CURVE 2	382	55
CURVE 3	276	50
RAMP B		
CURVE 1	249	40
CURVE 2	849	80
RAMP C		
CURVE 1	764	80
CURVE 2	477	65
RAMP D		
CURVE 1	477	65
CURVE 2	849	80



SECTIONAL VIEW



PLAN VIEW

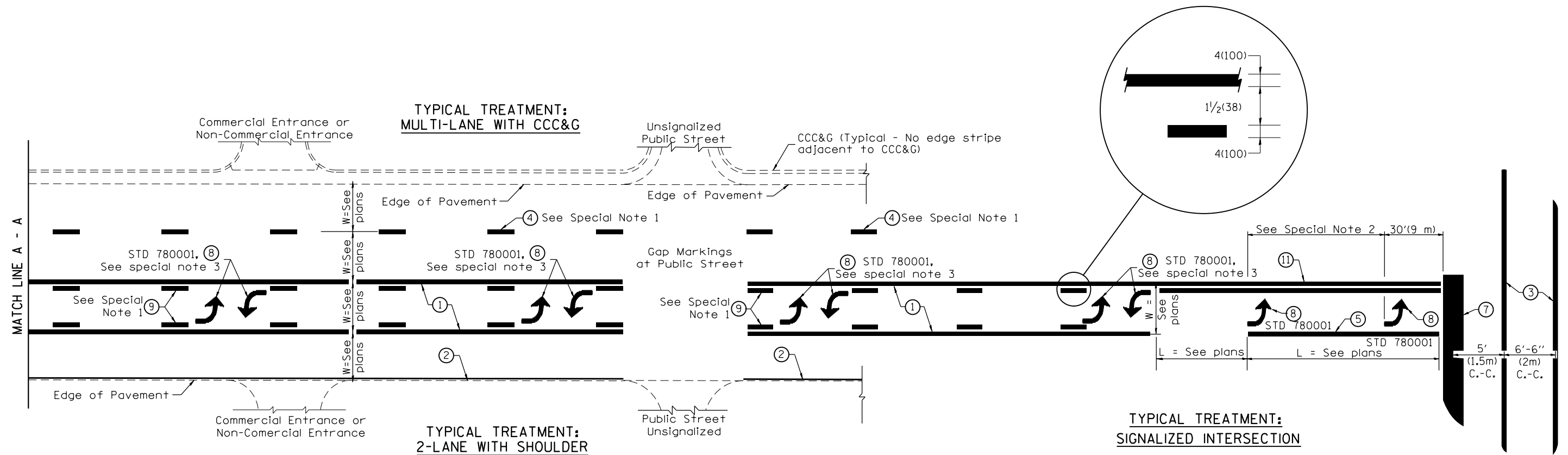
GENERAL NOTES

Delineators on mainline shall be placed at 400' spacing on tangent sections, and 200' spacing on curved sections.

Delineators on ramps, shall be placed as follows:
 1. On tangent sections, acceleration lanes and deceleration lanes at maximum spacing of 100'.
 2. On outside of all curved sections.
 (See table above for spacing)

DESIGNER NOTES:
 1. USE DISTRICT SPECIAL PROVISION FOR RECOVERABLE DELINEATORS WHEN USING THIS CADD STANDARD.
 2. USE ON INTERSTATE AND EXPRESSWAY FACILITIES.

DESIGNER NOTES:
1. Include State Standard 780001 (Typical Pavement Markings)



FLUSH PAVED MEDIAN: TWO-WAY LEFT TURN LANE WITH ONE-WAY LEFT TURN LANE AT SIGNALIZED INTERSECTION

TYPICAL PAVEMENT MARKING LEGEND

(Note: This is a District Standard Legend. Some elements may not apply to specific project.)

- ① 4(100) Solid (Yellow)
- ② 4(100) Solid (White)
- ③ 2-6(150) Crosswalk @ 6'-6" (2m)min C.-C. (White)
2-8(200) Crosswalk @ 6'-6" (2m)min C.-C. (White) (When traffic signals are present.)
- ④ 6(150) Skip-Dash (White) (See Special Note 1)
- ⑤ 8(200) Solid (White)
- ⑥ 12(300) Diagonal (White) (Item ⑥ is shown on Std. 780001)
- ⑦ 24(600) Stop Bar (White)
- ⑧ Letters & Arrows (See Std. 780001 and Special Notes 2 & 3)
- ⑨ 4(100) Skip-Dash (Yellow) (See Special Note 1)
- ⑩ 12(300) Diagonal (Yellow) (See Table A) (See Table A)
- ⑪ 4(100) Double Solid (Yellow) (See Table A)

SPECIAL NOTES

1. Skip-Dash markings will be centered between both ends of city blocks and shall be placed in alignment transversely across the pavement.
2. The following shall apply to arrows located in one-way left turn lanes:
 - A. A minimum of two (2) arrows is required.
 - B. The maximum spacing between arrows is 80' (24 m).
 - C. Arrows shall be evenly spaced if three (3) or more are required.
3. The following shall apply to arrow pairs located in two-way left turn lanes:
 - A. A minimum of two (2) arrow pairs is required.
 - B. The maximum spacing between arrow pairs is 200' (61 m).
 - C. Arrow pairs shall be evenly spaced if three (3) or more are required.
 - D. The spacing between Bi Directional Left Turn Arrows is 33' (10 m).

GENERAL NOTES

1. Refer to State Standard 780001 for additional Pavement Markings including letters & arrows.
2. See Plans for Pavement Markings adjacent to curbed islands and medians, and through lane reductions.
3. Refer to Article 780.13 for letter, number and symbol areas (sq. ft.)
4. Areas are grooved 1" beyond each edge for the following symbols:
 - Through Arrow= 14.8 sq. ft.
 - Large Left or Right Arrow= 21.9 sq. ft.
 - 2 Arrow Combination Left (or Right) and Through= 34.9 sq. ft.
 - Wrong Way Arrow= 29.5 sq. ft.
 - Railroad Crossing Symbol= 69.8 sq. ft.
 (For further information, refer to BDE Special Provision: Grooving for Recessed Pavement Markings)

01-01-97	RENUM. F-8.03, NEW REVISION BOX	T.P.	10-16-06	REVISED TO 2007 SPEC.	
02-07-97	ADD BI DIRECTIONAL DIMENSION	J.A.	2/29/16	ADDED GROOVING AREAS	R.D.
10-97	CORRECT BI DIRECTIONAL DIMENSION	J.A.			
08-02	ADD CROSSWALK DMNS. WITH T.S.	M.A.			

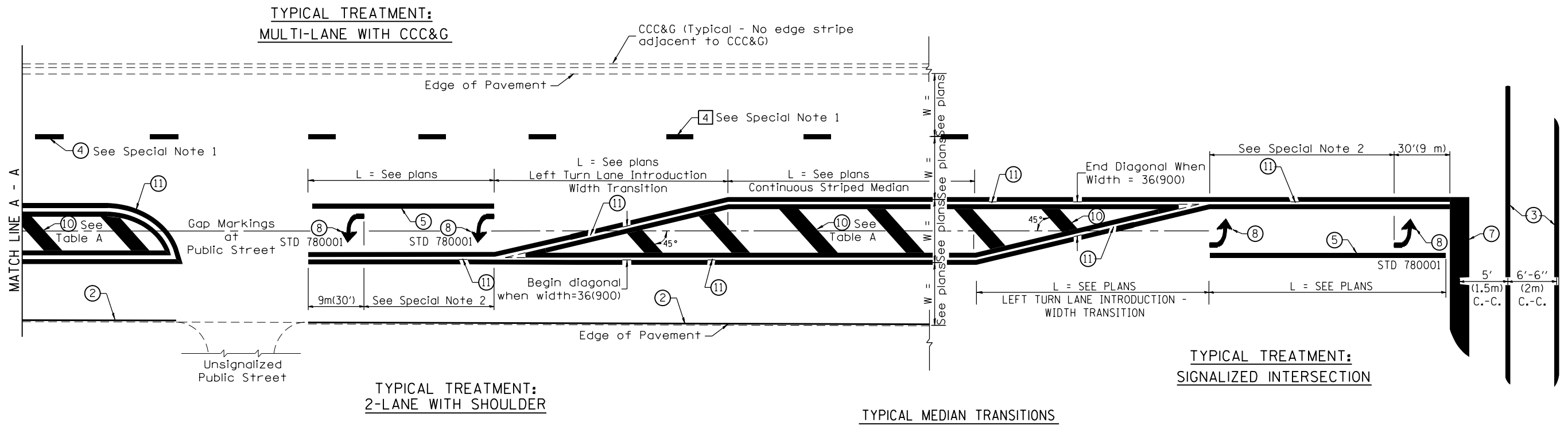
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

NOT TO SCALE

TYPICAL PAVEMENT MARKINGS

SHT. 1 OF 2
CADD STD. 780001-D4

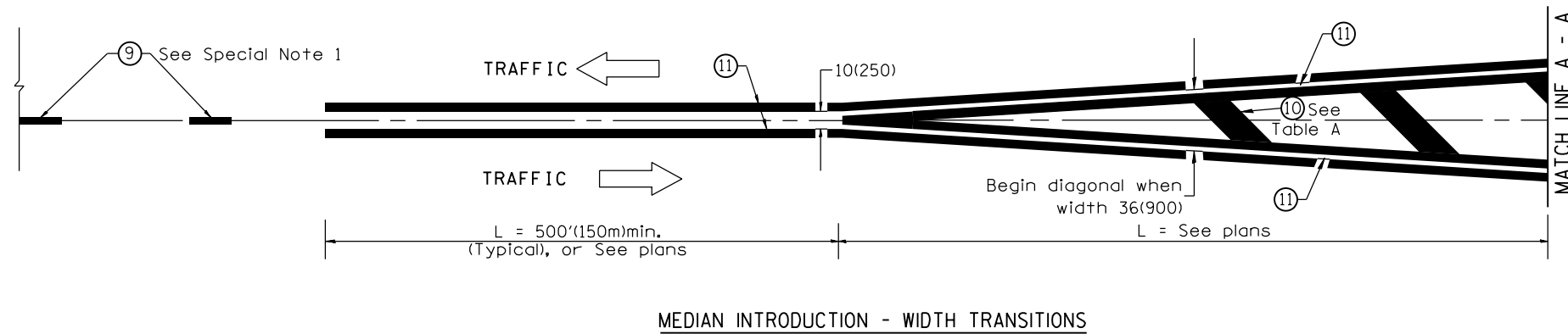
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	1021RS,2RS-1(2HVB)BRRJ	WOODFORD	71	70
			CONTRACT NO. 68D79	
ILLINOIS FED. AID PROJECT				



FLUSH PAVED MEDIAN: RESTRICTED LEFT TURN LANE

TABLE A
RECOMMENDED SPACING BETWEEN DIAGONAL LINES

SPEED LIMIT RANGE	INTERSECTION CHANNELIZATION (Includes Width Transitions for Median and Left Turn Lane Introductions)	
	CONTINUOUS	
Less Than 30 mph (50 km/h)	50' (15m)	15' (5m)
30 - 45 mph (50 - 70 km/h)	75' (23m)	20' (6m)
Over 45 mph (70 km/h)	150' (46m)	30' (9m)



All dimensions are in inches (millimeters) unless otherwise noted.