

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1375	18-00065-00-RS	DUPAGE	29	1

01-18-2019 LETTING ITEM 119

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

**PLANS FOR PROPOSED
FEDERAL AID HIGHWAY**
FAU 1375 (LIES ROAD)
FAU 2561 (GARY AVENUE) TO FAU 2566 (SCHMALE ROAD)
RESURFACING
SECTION 18-00065-00-RS
PROJECT NUPC(857)
VILLAGE OF CAROL STREAM
DUPAGE COUNTY
JOB NO. C-91-201-18



FOR INDEX OF SHEETS AND LIST OF
HIGHWAY STANDARDS, SEE SHEET NO. 2

TRAFFIC DATA

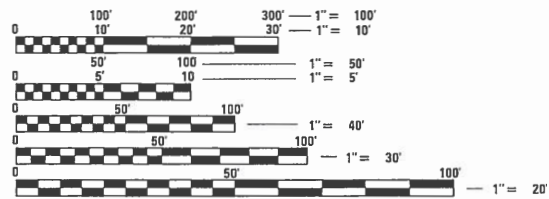
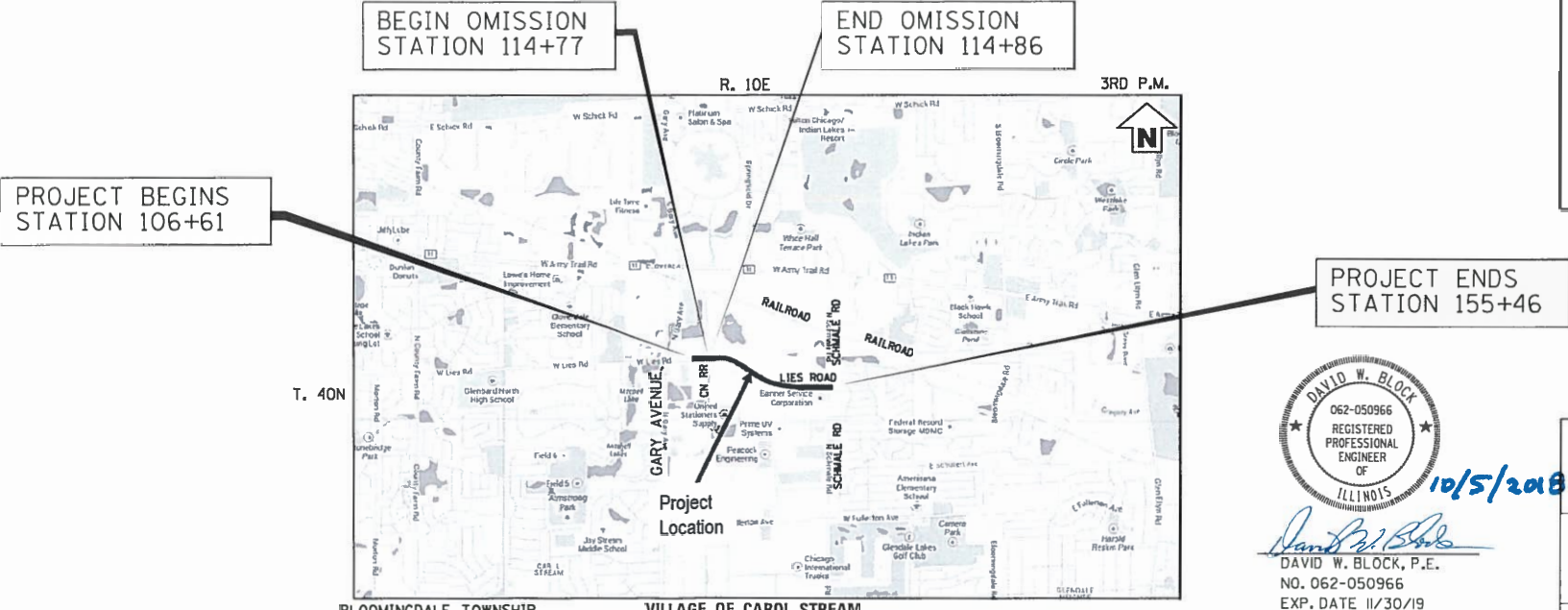
ADT:
LIES ROAD 10,050 VPD (2016)

POSTED SPEED

35 MPH (EXISTING) 40 MPH (EXISTING)
35 MPH (PROPOSED) 40 MPH (PROPOSED)

FUNCTIONAL CLASSIFICATION

MAJOR COLLECTOR



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

CONTRACT NO. 61F30

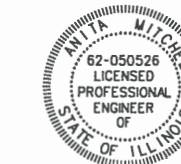
LOCATION MAP

NOT TO SCALE

PROJECT LENGTH (GROSS AND NET)
4,885 FT (0.93 MILES) (GROSS)
OMISSION = 9 FT (0.002 MILES)
TOTAL PROJECT LENGTH = 4,876 FEET (0.92 MILES) (NET)



DAVID W. BLOCK, P.E.
NO. 062-050966
EXP. DATE 11/30/19
(RESPONSIBLE FOR SHEETS: 1-2, 4-II, 13-27)



CYNTHIA MITCHELL, P.E.
NO. 02-050526
EXP. DATE 11-30-2019
(RESPONSIBLE FOR SHEETS: 3, 12, 28-29)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APPROVED 10/4/2018, 2018
VILLAGE OF CAROL STREAM, VILLAGE ENGINEER

PASSED 10/15/2018
DISTRICT ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID
BASED ON LIMITED
REVIEW OCTOBER 16, 2018
REGIONAL ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

BAXTER & WOODMAN
Consulting Engineers

1475 EAST WOODFIELD ROAD, SUITE 600
SCHLAUBURG, ILLINOIS 60173
(847) 605-9800
FEDERAL AID PROGRAM ENGINEER: CARMEN E. RAMOS, P.E., SCHLAUBURG, IL
Tran Systems

GENERAL NOTES

- ALL REFERENCES TO "STANDARD SPECIFICATIONS" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, APRIL 1, 2016.
- ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED TO MEAN THE RESIDENT ENGINEER.
- PRIOR TO COMMENCEMENT OF CONSTRUCTION, THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS AFFECTING THEIR WORK WITH THE ACTUAL CONDITIONS AT THE JOB SITE. IN ADDITION, THE CONTRACTOR SHALL VERIFY THE ENGINEER'S LINE AND GRADE STAKES. IF THERE ARE ANY DISCREPANCIES FROM WHAT IS SHOWN ON THE CONSTRUCTION PLANS, THE CONTRACTOR MUST IMMEDIATELY REPORT SAME TO THE ENGINEER BEFORE DOING ANY WORK, OTHERWISE THE CONTRACTOR ASSUMES FULL RESPONSIBILITY. IN THE EVENT OF DISAGREEMENT WITH THE CONSTRUCTION PLANS, STANDARD SPECIFICATIONS AND/OR SPECIAL DETAILS, THE CONTRACTOR SHALL SECURE WRITTEN INSTRUCTIONS FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF THE WORK AFFECTED BY OMISSIONS OR DISCREPANCIES. FAILING TO SECURE SUCH INSTRUCTIONS, THE CONTRACTOR WILL BE CONSIDERED TO HAVE PROCEEDED AT HIS/HER OWN RISK. IN THE EVENT OF ANY DOUBT OR QUESTION ARISING WITH RESPECT TO THE TRUE MEANING OF THE CONSTRUCTION PLANS OR SPECIFICATIONS, THE DECISION OF THE ENGINEER SHALL BE FINAL AND CONCLUSIVE.
- BEFORE STARTING EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION) AT 8-1-1 AND THE VILLAGE OF CAROL STREAM AT 630-868-2260 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION IS REQUIRED).
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON VILLAGE PROPERTY OR ROW WITHOUT WRITTEN PERMISSION FROM THE ENGINEER.
- SAW CUTTING OF PAVEMENTS, SIDEWALK, ETC. SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING.
- OFFSET LOCATIONS GIVEN IN THE PLANS FOR STRUCTURES, EDGE OF PAVEMENT, ETC. ARE FROM THE ROADWAY CENTERLINE.
- THE ENGINEER WILL VERIFY FINAL PATCH LOCATIONS IN THE FIELD, PRIOR TO REMOVAL.
- THE CONTRACTOR SHALL USE 2 CHANGEABLE MESSAGE SIGNS AT LOCATIONS TO BE DETERMINED BY THE ENGINEER FOR A PERIOD FROM ONE WEEK PRIOR TO THE START OF CONSTRUCTION TO THE CONCLUSION OF THE PROJECT.
- THE CONTRACTOR SHALL NOT BE PERMITTED TO STAGE OR OPERATE CONSTRUCTION EQUIPMENT ON THE HMA BIKE PATH.
- CONTRACTOR SHALL PAY EXTRA PRECAUTION NOT TO DAMAGE ANY TREES DURING CONSTRUCTION. ANY DAMAGES OR DISTURBANCES THAT OCCUR DURING CONSTRUCTION SHALL BE REPLACED IN KIND BY THE CONTRACTOR.

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TRAFFIC CONTROL

- SEE TRAFFIC CONTROL HIGHWAY STANDARDS CONCERNING TRAFFIC CONTROL AND PROTECTION. UNEVEN LANES SIGNS (W8-11) WILL BE REQUIRED WHEN MILLING THE 4" OF PAVEMENT.
- THE CONTRACTOR SHALL SCHEDULE CONSTRUCTION ACTIVITIES SO THAT TWO-WAY TRAFFIC SHALL REMAIN OPEN AT ALL TIMES.

STORM SEWERS, WATER MAINS, AND UTILITIES

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF ANY UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.
- THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN IF NOT SHOWN ON THE PLANS. ALL UTILITY PROPERTY DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER.
- ALL UTILITY OWNERS AND THE ENGINEER SHALL BE NOTIFIED AT LEAST 3 DAYS PRIOR TO THE START OF CONSTRUCTION.
- THE CONTRACTOR SHALL USE ALL NECESSARY PRECAUTIONS AND PROTECTION MEASURES REQUIRED TO MAINTAIN EXISTING UTILITIES, SEWERS, AND APPURTENANCES THAT MUST BE KEPT IN OPERATION.
- ALL LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS.
- THE CONTRACTOR SHALL ENSURE ALL WATER SYSTEM VALVES, VALVE VAULTS, AND SANITARY SEWER MANHOLES REMAIN READILY ACCESSIBLE FOR EMERGENCY OPERATIONS. THE LOCATIONS OF ALL WATER AND SANITARY FACILITIES SHALL BE MARKED AND READILY VISIBLE AT ALL TIMES.

HIGHWAY STANDARDS

- | | |
|-----------|--|
| 000001-07 | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS |
| 280001-07 | TEMPORARY EROSION CONTROL SYSTEMS |
| 424001-11 | PERPENDICULAR CURB RAMPS FOR SIDEWALKS |
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| 424021-05 | DEPRESSED CORNER FOR SIDEWALKS |
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| 442201-03 | CLASS C AND D PATCHES |
| 602001-02 | CATCH BASIN TYPE A |
| 602301-04 | INLET - TYPE A |
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| 604056-04 | FRAME AND GRATE TYPE 11V |
| 606001-07 | CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER |
| 630001-12 | STEEL PLATE BEAM GUARDRAIL |
| 630301-09 | SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS |
| 631011-10 | TRAFFIC BARRIER TERMINAL, TYPE 2 |
| 701101-05 | OFF-RD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE |
| 701602-10 | URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE |
| 701701-10 | URBAN LANE CLOSURE, MULTILANE INTERSECTION |
| 701801-06 | SIDEWALK, CORNER OR CROSSWALK CLOSURE |
| 701901-08 | TRAFFIC CONTROL DEVICES |
| 725001-01 | OBJECT AND TERMINAL MARKERS |
| 780001-05 | TYPICAL PAVEMENT MARKINGS |
| 782006 | GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS |
| 814001-03 | HANDHOLES |
| 886001-01 | DETECTOR LOOP INSTALLATIONS |

MISCELLANEOUS

- MATERIALS RESULTING FROM THE REMOVAL OF CONCRETE SURFACES, UTILITY STRUCTURE ADJUSTMENTS, RESTORATION WORK, ETC. SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IF THE CONTRACTOR DOES NOT REMOVE THESE MATERIALS AT THE REQUEST OF THE ENGINEER, THE ENGINEER WILL HIRE A CONTRACTOR TO HAVE THE MATERIAL REMOVED AND THE CONTRACTOR SHALL BE BILLED (CHARGED) ACCORDINGLY.
- THE INDISCRIMINATE USE OF FIRE HYDRANTS, EXISTING STREAMS, CREEKS, WETLANDS, OR PONDS IS STRICTLY PROHIBITED. THE CONTRACTOR SHALL PROVIDE A WATER TRUCK AND DRIVER AS REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING WATER FROM AN APPROVED SOURCE. IF THIS WATER IS FROM A SOURCE OTHER THAN HIS/HER YARD, WRITTEN APPROVAL FROM THE AGENCY HAVING JURISDICTION FOR THE SOURCE OF THE WATER MUST BE RECEIVED BY THE CONTRACTOR PRIOR TO USE OF THE WATER.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR REGULARLY SWEEPING AND CLEANING STREETS OF ANY DEBRIS AND MATERIAL THAT HAS ACCUMULATED AS A RESULT OF THE CONSTRUCTION ACTIVITY. A MECHANICAL SWEEPER, MECHANICALLY DRIVEN AIR AND HANDWORK WITH SHOVEL AND BROOM SHALL BE UTILIZED TO PROVIDE A CLEAN STREET FOR THE MOTORING PUBLIC. WITHIN 24 HOURS OF PLACING PRIME COAT AND PAVING HMA, THE CONTRACTOR SHALL SWEEP THE PAVEMENT AND REMOVE STANDING WATER, EARTH, WEEDS, LEAVES, DIRT, CONSTRUCTION DEBRIS AND ALL LOOSE MATERIAL.
- IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS, COMMERCIAL PROPERTY OWNERS, AND THE ENGINEER WHEN ACCESS TO THEIR DRIVEWAYS WILL BE ALTERED DUE TO SIDEWALK, DRIVEWAY, AND/OR CURB AND GUTTER REPLACEMENT. AT LOCATIONS WHERE THE SIDEWALK, DRIVEWAY, AND/OR CURB AND GUTTER IS SCHEDULED TO BE REMOVED, THE CONTRACTOR SHALL CONTACT THE PROPERTY OWNER 24 HOURS PRIOR TO THEIR REMOVAL. THESE ITEMS SHALL BE REMOVED AND RECONSTRUCTED HALF AT A TIME SUCH THAT THERE ARE NO DRIVEWAY CLOSURES.
- WHEN REMOVING PAVEMENT, CURB AND GUTTER, SIDEWALK, AND/OR ANY OTHER STRUCTURES, THE USE OF ANY TYPE OF CONCRETE BREAKERS WHICH MIGHT DAMAGE UNDERGROUND PUBLIC OR PRIVATE UTILITIES AND BUILDING FOUNDATIONS WILL NOT BE PERMITTED. UNDER NO CIRCUMSTANCES WILL THE USE OF A FROST BALL BE PERMITTED.
- THE VILLAGE OF CAROL STREAM WILL ALLOW NIGHT PAVING AND THE CONTRACTOR HAS THE OPTION OF PAVING AT NIGHT BETWEEN THE HOURS OF 7 PM AND 6 AM.
- IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY TO COORDINATE WITH THE CANADIAN NATIONAL RAILROAD WHENEVER CONSTRUCTION ACTIVITY IS WITHIN 25 FEET OF THE RAILROAD ROW. THE CONTRACTOR SHALL RETAIN FLAGMEN EMPLOYED AND DESIGNATED BY THE CANADIAN NATIONAL RAILROAD TO MONITOR ON-COMING TRAIN TRAFFIC, AND ADVISE CONTRACTOR PERSONNEL WHEN ACTIVITY ON OR NEAR THE RAILROAD RIGHT-OF-WAY MAY PROCEED. THIS ITEM WILL BE PAID FOR ACCORDING TO ARTICLE 107.12 AND WILL BE REIMBURSED ACCORDING TO ARTICLE 109.05.
- IN ACCORDANCE WITH RESOLUTION FOR PERMANENT EASEMENT WITH CNRR, THE CONTRACTOR SHALL SECURE PERMISSION FROM THE AUTHORIZED REPRESENTATIVE OF THE CANADIAN NATIONAL RAILROAD PRIOR TO ENTERING UPON THE RAILROAD RIGHT OF WAY AND COORDINATE RELATIVE TO ITS REQUIREMENTS FOR CLEARANCES, OPERATION, AND GENERAL SAFETY REGULATIONS.

FILE NAME =	USER NAME = *USER*	DESIGNED - CEC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	LIES ROAD RESURFACING INDEX OF SHEETS, GENERAL NOTES AND HIGHWAY STANDARDS	F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
FILEL		DRAWN - CEC	REVISED -			1375	18-00065-00-RS	DUPAGE	29	2	
	PLOT SCALE = *SCALE*	CHECKED - DWB	REVISED -			CONTRACT NO. 61F30					
MODELNAME	PLOT DATE = 11/22/2018	DATE - 11/02/2018	REVISED -			SCALE: NTS	SHEET 1 OF 1 SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT		

STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. - 184-001121 - EXPIRES 4/30/2019
 7658KS 11/17/2018
 FILE NAME: \\corp.baxwood.com\Projects\Crystal Lake\DP\161262-Various Ph. H1140-Design\WO 4\CAD\Sheets\161262-SHT-Plan1.dgn
 FILE PATH: \\corp.baxwood.com\Projects\Crystal Lake\DP\161262-Various Ph. H1140-Design\WO 4\CAD\Sheets\161262-SHT-Plan1.dgn

GENERAL NOTES

1. NO WORK SHALL COMMENCE UNTIL TRAFFIC CONTROL REQUIREMENTS ARE MET.
2. ALL UTILITIES, SCHOOL DISTRICTS, LOCAL POLICE, AND FIRE DEPARTMENTS SHALL BE NOTIFIED BY THE CONTRACTOR PRIOR TO THE START OF CONSTRUCTION.

TREE REMOVAL CLEARING HEDGE REMOVAL

3. TREES NOT MARKED FOR REMOVAL SHALL BE CONSIDERED AS DESIGNATED TO BE SAVED AND SHALL BE PROTECTED UNDER THE PROVISIONS OF ARTICLE 201.05 OF THE STANDARD SPECIFICATIONS.

TOPSOIL

4. THE CROSS SECTIONS INDICATE THE FINISHED GRADE OF TOPSOIL.

EROSION CONTROL NOTES

5. ALL WORK SHALL BE DONE IN ACCORDANCE WITH ARTICLE VII OF THE DuPAGE COUNTY COUNTYWIDE STORMWATER AND FLOOD PLAIN ORDINANCE, EFFECTIVE APRIL 2013 AND ALL SUBSEQUENT REVISIONS. ALL SEDIMENT AND EROSION CONTROL MEASURES WILL BE INSTALLED PER IDOT STANDARD 280001 OR AS SPECIFIED HEREIN AND PAID FOR IN ACCORDANCE WITH SECTION 280 OF THE STANDARD SPECIFICATIONS. ALL CONSTRUCTION ACTIVITIES WILL BE IN ACCORDANCE WITH THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM STORM WATER PERMITS ILR10 AND ILR40.
6. EROSION CONTROL SHALL BE PROVIDED IN ACCORDANCE WITH THE SEQUENCE OF STAGE CONSTRUCTION. THE CONTRACTOR SHALL SUBMIT A DETAILED SCHEDULE FOR APPROVAL.
7. SEDIMENT AND EROSION CONTROL DEVICES SHALL BE FUNCTIONAL BEFORE THE PROJECT SITE IS OTHERWISE DISTURBED.
8. ALL DISTURBED AREAS SHALL BE SEEDED OR SODDED AS SOON AS PRACTICAL AFTER CONSTRUCTION ACTIVITIES IN THAT AREA HAVE CONCLUDED. IF A TOPSOIL STOCKPILE IS TO REMAIN IN PLACE FOR MORE THAN THREE DAYS, EROSION CONTROL MEASURES WILL BE PROVIDED.
9. STOCKPILES AND MATERIAL STORAGE ARE PROHIBITED IN SPECIAL MANAGEMENT AREAS INCLUDING WETLANDS, FLOOD PLAINS, AND BUFFERS. TOPSOIL SHALL NOT BE STOCKPILED WITHIN THE LIMITS OF CONSTRUCTION. LOCATIONS OF STOCKPILES MUST BE APPROVED BY THE ENGINEER AND HAVE PROPER EROSION CONTROL MEASURES.
10. RECEPTACLES FOR CONSTRUCTION DEBRIS, INCLUDING CONCRETE TRUCK WASHOUT WASTE, SHALL BE PROVIDED AND MAINTAINED BY THE CONTRACTOR. THESE WILL NOT BE ALLOWED IN SPECIAL MANAGEMENT AREAS. RECEPTACLES AND THEIR LOCATIONS MUST BE APPROVED BY THE ENGINEER AND HAVE PROPER EROSION CONTROL MEASURES.
11. HAY OR STRAW BALES WILL NOT BE ALLOWED AS PERIMETER EROSION BARRIER OR AS A DITCH CHECK.
12. WATER PUMPED OR OTHERWISE DISCHARGED FROM THE SITE DURING CONSTRUCTION DEWATERING SHALL BE FILTERED.
13. WHEN TEMPORARY DRAINAGE IS ESTABLISHED, EROSION CONTROL MEASURES MAY BE REQUIRED BY THE ENGINEER.
14. GRAVEL ROADS, ACCESS DRIVES, PARKING AREAS OF SUFFICIENT WIDTH AND LENGTH, AND VEHICLE WASH DOWN FACILITIES IF NECESSARY, SHALL BE PROVIDED TO PREVENT SOIL FROM BEING TRACKED ONTO PUBLIC OR PRIVATE ROADWAYS. ANY SOIL REACHING A PUBLIC OR PRIVATE ROADWAY SHALL BE REMOVED BEFORE THE END OF EACH WORKDAY AND AS NEEDED.
15. CLEANING OF VEHICLES AND EQUIPMENT, INCLUDING CONCRETE MIXERS, SHALL BE PERFORMED IN A MANNER TO REDUCE THE AMOUNT OF POLLUTANTS TRIBUTARY TO STORM SEWERS AND OPEN WATERS TO THE MAXIMUM EXTENT PRACTICAL.
16. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTION RUNOFF. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

17. SEDIMENT COLLECTED DURING CONSTRUCTION BY THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM EROSION CONTROL SYSTEMS WHEN THE HEIGHT OF THE SEDIMENT EXCEEDS ONE-HALF OF THE HEIGHT OF THE FILTER DEVICE.
18. ALL EROSION CONTROL MEASURES SHALL BE KEPT OPERATIONAL AND MAINTAINED CONTINUOUSLY THROUGHOUT THE PERIOD OF LAND DISTURBANCE UNTIL PERMANENT SEDIMENT AND EROSION CONTROL MEASURES ARE OPERATIONAL.
19. ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL STABILIZATION IS ACHIEVED.
20. THE ENGINEER SHALL INSPECT EROSION CONTROL MEASURES PERIODICALLY AND WITHIN 24 HOURS OF ANY STORM EXCEEDING ½ INCH PRECIPITATION. DAMAGED AND INEFFECTIVE EROSION CONTROL MEASURES SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR WITHIN 24 HOURS.



USER NAME = 7658ks	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 11/17/2018	DATE - 11/01/18	FILE 161262-SHT-Plan1.dgn

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**LIES ROAD RESURFACING
 DUPAGE COUNTY GENERAL NOTES**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1375	18-00065-00-RS	DUPAGE	29	3
CONTRACT NO. 61F30			ILLINOIS FED. AID PROJECT	

SUMMARY OF QUANTITIES				0005 ROADWAY 70% STU 30% LA
CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY	
20200100	EARTH EXCAVATION	CU YD	133	133
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	171	171
20400800	FURNISHED EXCAVATION	CU YD	10	10
20800150	TRENCH BACKFILL	CU YD	2	2
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	448	448
21101625	TOPSOIL FURNISH AND PLACE, 6"	SQ YD	190	190
25000210	SEEDING, CLASS 2A	ACRE	0.1	0.1
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	15	15
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	15	15
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	15	15
25100630	EROSION CONTROL BLANKET	SQ YD	448	448
25200100	SODDING	SQ YD	190	190
25200200	SUPPLEMENTAL WATERING	UNIT	6	6
28000400	PERIMETER EROSION BARRIER	FOOT	270	270
28000510	INLET FILTERS	EACH	33	33
30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	106	106
30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	174	174
31101200	SUBBASE GRANULAR MATERIAL, TYPE B 4"	SQ YD	321	321
35401000	HIGH-EARLY-STRENGTH PORTLAND CEMENT CONCRETE BASE COURSE WIDENING 10"	SQ YD	102	102
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	21,509	21,509
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	25	25
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	188	188
40603085	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON	4,035	4,035
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	3,139	3,139
42001300	PROTECTIVE COAT	SQ YD	269	269
42400800	DETECTABLE WARNINGS	SQ FT	25	25
44000100	PAVEMENT REMOVAL	SQ YD	28	28
44000165	HOT-MIX ASPHALT SURFACE REMOVAL, 4"	SQ YD	18,934	18,934
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	1,164	1,164
44000600	SIDEWALK REMOVAL	SQ FT	202	202

SUMMARY OF QUANTITIES				0005 ROADWAY 70% STU 30% LA
CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY	
44201761	CLASS D PATCHES, TYPE I, 10 INCH	SQ YD	794	794
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	794	794
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SQ YD	794	794
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	794	794
550A2320	STORM SEWERS, RUBBER GASKET, CLASS A, TYPE 1 12"	FOOT	8	8
55101200	STORM SEWER REMOVAL 24"	FOOT	5	5
* 56400400	FIRE HYDRANTS TO BE RELOCATED	EACH	1	1
60201110	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 11V FRAME AND GRATE	EACH	1	1
60236825	INLETS, TYPE A, TYPE 11V FRAME AND GRATE	EACH	1	1
60265700	VALVE VAULTS TO BE ADJUSTED	EACH	3	3
60266100	VALVE VAULTS TO BE RECONSTRUCTED	EACH	2	2
60404805	FRAMES AND GRATES, TYPE 11V	EACH	1	1
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	1	1
60500050	REMOVING CATCH BASINS	EACH	1	1
60500060	REMOVING INLETS	EACH	1	1
60600605	CONCRETE CURB, TYPE B	FOOT	46	46
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	1,008	1,008
60604400	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18	FOOT	54	54
60605900	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-9.12	FOOT	92	92
* 63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	125	125
* 63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	1	1
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	1	1
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	10	10
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	3	3
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION	L SUM	1	1
* 66901002	ON-SITE MONITORING OF REGULATED SUBSTANCES	CAL DA	80	80
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1	1
67100100	MOBILIZATION	L SUM	1	1
70100350	TRAFFIC CONTROL AND PROTECTION, STANDARD 701101	EACH	1	1
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	1

* SPECIALTY ITEM

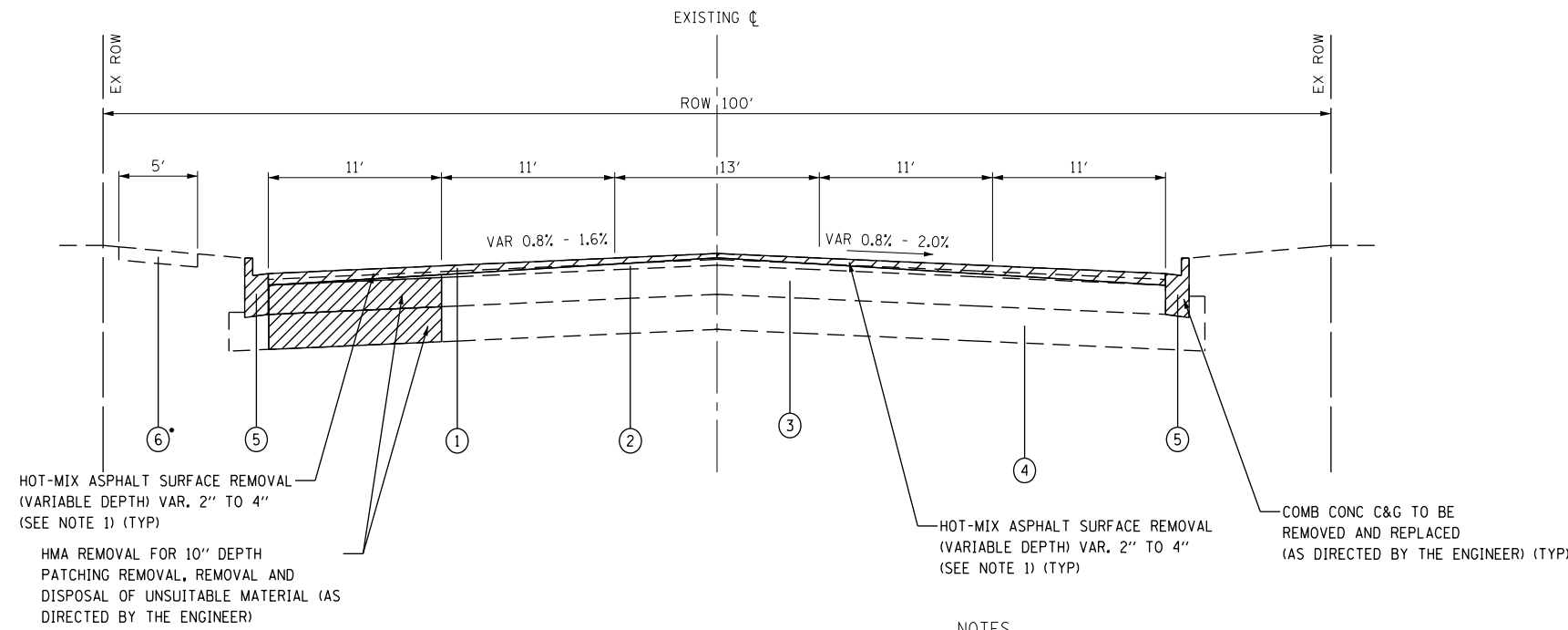
SUMMARY OF QUANTITIES				TOTAL QUANTITY	0005 ROADWAY 70% STU 30% LA
* CODE NUMBER	ITEM	UNIT			
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1	
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1	
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	195	195	
70300100	SHORT TERM PAVEMENT MARKING	FOOT	3,517	3,517	
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	1,670	1,670	
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	2,378	2,378	
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	30,710	30,710	
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	492	492	
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	1,746	1,746	
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	1,812	1,812	
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	404	404	
* 72501000	TERMINAL MARKER - DIRECT APPLIED	EACH	1	1	
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	1,189	1,189	
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	15,355	15,355	
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	246	246	
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	873	873	
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	906	906	
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	202	202	
* 78200005	GUARDRAIL REFLECTORS, TYPE A	EACH	4	4	
* 81400100	HANDHOLE	EACH	1	1	
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	284	284	
* 89502380	REMOVE EXISTING HANDHOLE	EACH	1	1	
K0036120	MULCH PLACEMENT 4"	SQ YD	12	12	
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1	
X0327980	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	1,670	1,670	
X4240430	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL	SQ FT	178	178	
X4401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	12,800	12,800	
Z0018400	DRAINAGE STRUCTURES TO BE ADJUSTED	EACH	13	13	
Z0018600	DRAINAGE STRUCTURES TO BE RECONSTRUCTED	EACH	8	8	
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	379	379	
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1	

* SPECIALTY ITEM

FILE NAME =	USER NAME = #USER#	DESIGNED - CEC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	LIES ROAD RESURFACING SUMMARY OF QUANTITIES			F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILEL#		DRAWN - CEC	REVISED -					1375	18-00065-00-RS	DUPAGE	29	5
	PLOT SCALE = #SCALE#	CHECKED - DWB	REVISED -		SCALE: NTS			SHEET 2 OF 2 SHEETS		CONTRACT NO. 61F30		
#MODELNAME#	PLOT DATE = 11/2/2018	DATE - 11/02/2018	REVISED -		TO STA.			ILLINOIS FED. AID PROJECT				

LEGEND

- ① EXISTING HMA SURFACE COURSE, 2"
- ② EXISTING HMA LEVEL BINDER/BINDER COURSE, 2"
- ③ EXISTING HMA BASE COURSE, 10"
- ④ EXISTING AGGREGATE SUBGRADE, 12"
- ⑤ EXISTING COMBINATION CONCRETE CURB & GUTTER TYPE B-6.12
- ⑥ EXISTING PCC SIDEWALK, 5"



HOT-MIX ASPHALT SURFACE REMOVAL (VARIABLE DEPTH) VAR. 2" TO 4" (SEE NOTE 1) (TYP)

HMA REMOVAL FOR 10" DEPTH PATCHING REMOVAL, REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (AS DIRECTED BY THE ENGINEER)

HOT-MIX ASPHALT SURFACE REMOVAL (VARIABLE DEPTH) VAR. 2" TO 4" (SEE NOTE 1) (TYP)

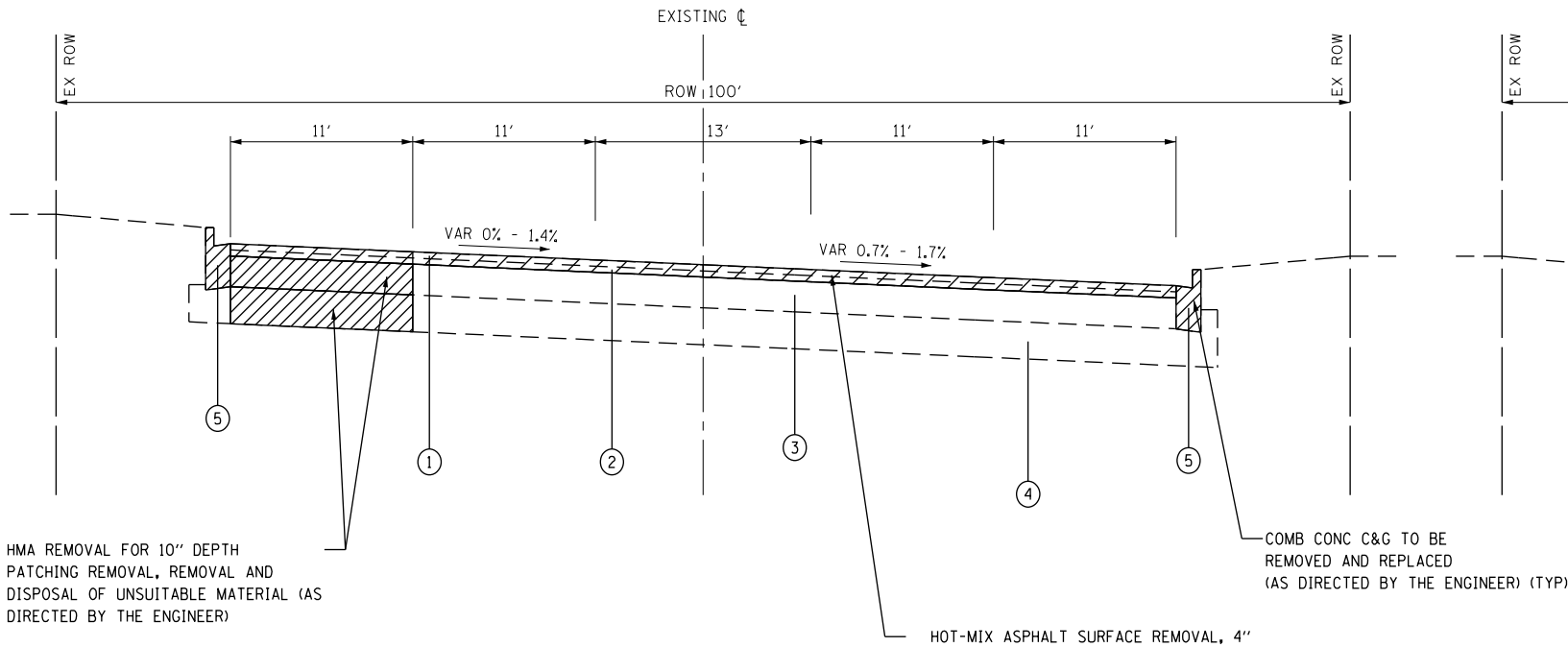
COMB CONC C&G TO BE REMOVED AND REPLACED (AS DIRECTED BY THE ENGINEER) (TYP)

EXISTING SIDEWALK FROM STATION 106+65 TO STATION 113+65 TO REMAIN.

EXISTING TYPICAL SECTION
 STA. 106+61 TO STA. 114+77, LIES ROAD
 STA. 114+86 TO STA. 115+50, LIES ROAD
 STA. 145+00 TO STA. 155+46, LIES ROAD
 NORMAL CROWN

NOTES

- THE CONTRACTOR SHALL MILL A VARIABLE DEPTH OF PAVEMENT BETWEEN THE EDGE OF PAVEMENT AND CENTERLINE. THE MILL DEPTH SHALL BE A CONSISTENT 4" ALONG BOTH EDGES OF PAVEMENT. THE CENTERLINE MILL DEPTH SHALL VARY BETWEEN 2" AND 4" IN NORMAL CROWN SECTIONS (STATION 106+61 TO 115+50 AND STATION 145+00 TO 155+46) TO CORRECT THE EXISTING CROSS SLOPES. SEE PLANS FOR ϕ MILLING DEPTHS.

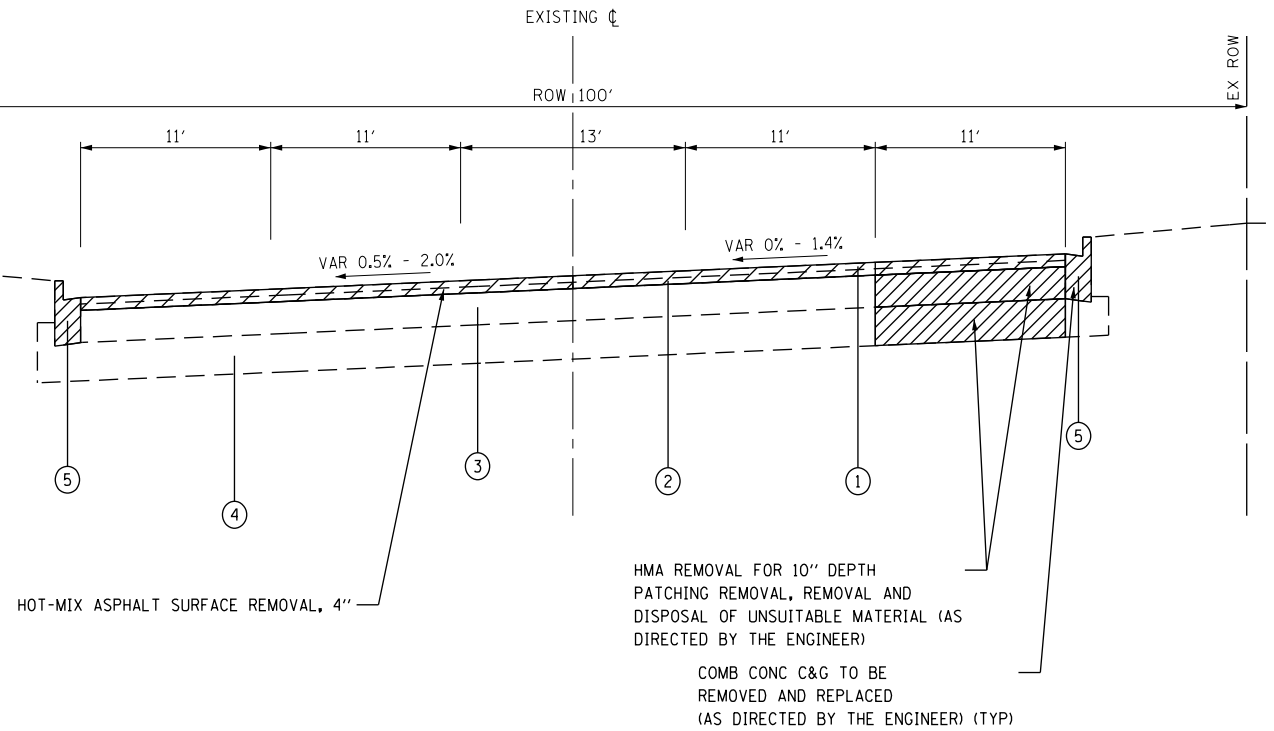


HMA REMOVAL FOR 10" DEPTH PATCHING REMOVAL, REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (AS DIRECTED BY THE ENGINEER)

COMB CONC C&G TO BE REMOVED AND REPLACED (AS DIRECTED BY THE ENGINEER) (TYP)

HOT-MIX ASPHALT SURFACE REMOVAL, 4"

EXISTING TYPICAL SECTION
 STA. 115+50 TO STA. 130+00, LIES ROAD
 SUPERELEVATION RIGHT



HMA REMOVAL FOR 10" DEPTH PATCHING REMOVAL, REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (AS DIRECTED BY THE ENGINEER)

COMB CONC C&G TO BE REMOVED AND REPLACED (AS DIRECTED BY THE ENGINEER) (TYP)

EXISTING TYPICAL SECTION
 STA. 130+00 TO STA. 145+00, LIES ROAD
 SUPERELEVATION LEFT

REMOVAL ITEMS

FILE NAME =	USER NAME = *USER*	DESIGNED - CEC	REVISED -
FILEL		DRAWN - CEC	REVISED -
	PLOT SCALE = *SCALE*	CHECKED - DWB	REVISED -
MODELNAME	PLOT DATE = 11/2/2018	DATE - 11/02/2018	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

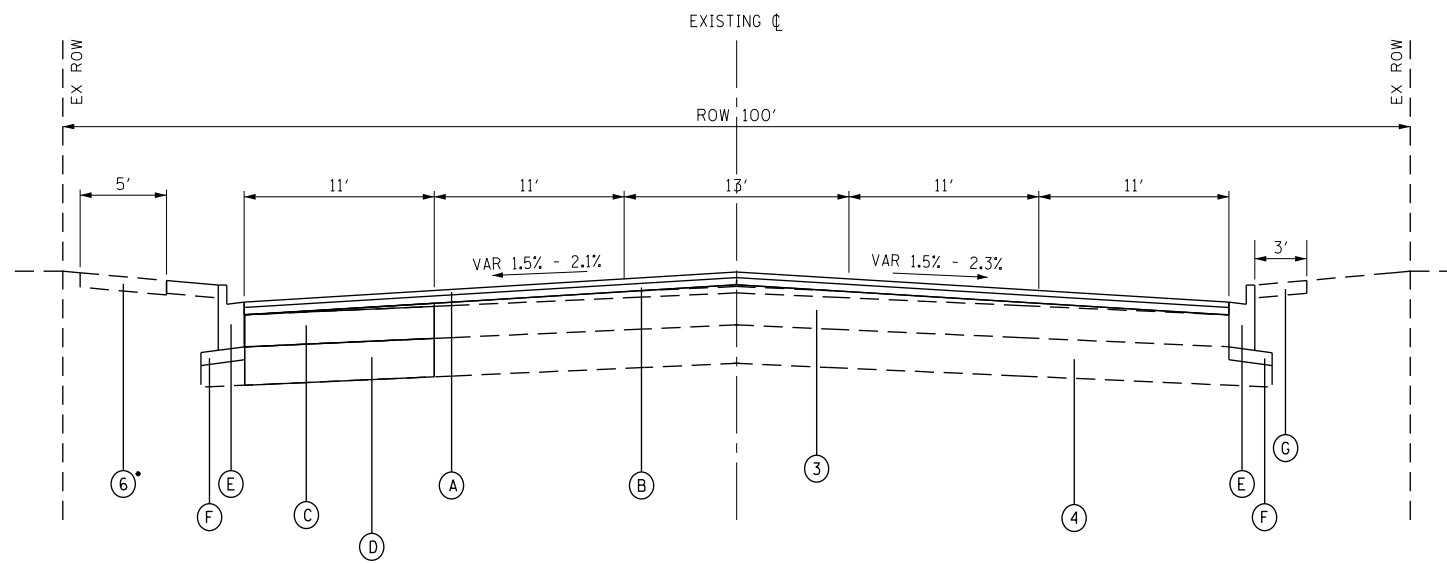
**LIES ROAD RESURFACING
 EXISTING TYPICAL SECTIONS**

SCALE: NTS SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1375	18-00065-00-RS	DUPAGE	29	6
ILLINOIS FED. AID PROJECT			CONTRACT NO. 61F30	

LEGEND

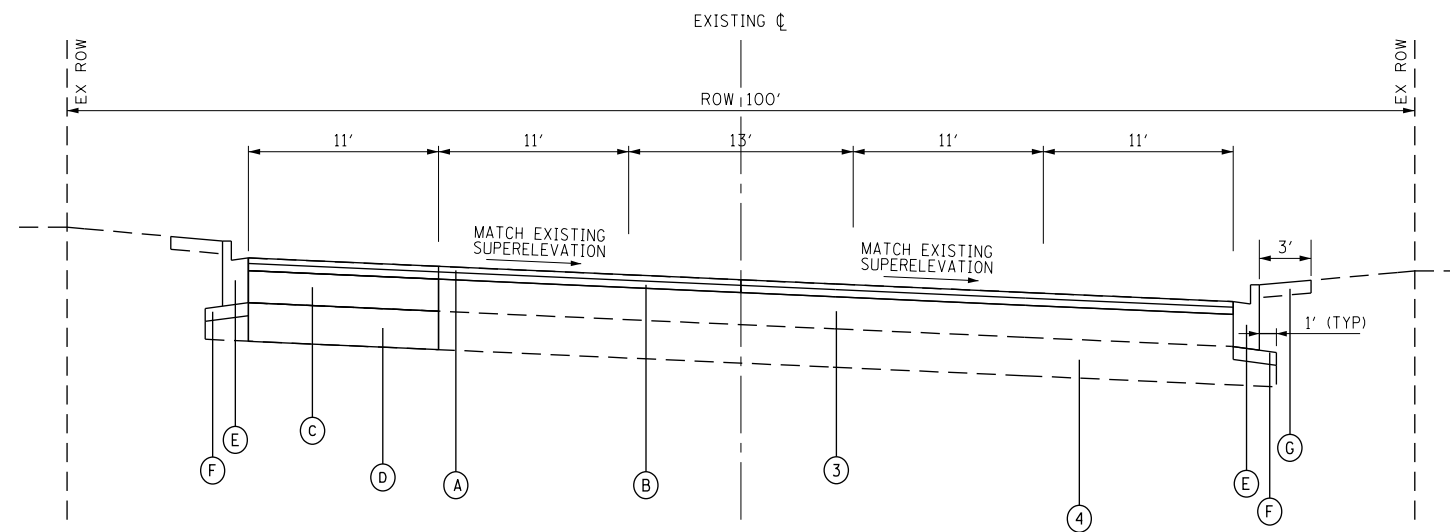
- ① EXISTING HMA SURFACE COURSE, 2"
- ② EXISTING HMA LEVEL BINDER/BINDER COURSE, 2"
- ③ EXISTING HMA BASE COURSE, 10"
- ④ EXISTING AGGREGATE SUBGRADE, 12"
- ⑤ EXISTING COMBINATION CONCRETE CURB & GUTTER TYPE B-6.12
- ⑥ EXISTING PCC SIDEWALK, 5"
- Ⓐ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1.75" (CONTRACTOR SHALL USE ECHELON PAVING FOR THE SURFACE COURSE) (NIGHT PAVING FROM 7 PM TO 6 AM IS AN OPTION)
- Ⓑ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 2.25"
- Ⓒ CLASS D PATCHES, 10" (AS DIRECTED BY ENGINEER)
- Ⓓ REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
AGGREGATE SUBGRADE IMPROVEMENT (AS DIRECTED BY ENGINEER)
- Ⓔ COMBINATION CURB AND GUTTER REMOVAL
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18
(AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)
- Ⓕ SUBBASE GRANULAR MATERIAL, TYPE B 4"
(REMOVAL AND DISPOSAL OF THE MATERIAL UNDER THE PROPOSED CURB & GUTTER IN ORDER TO INSTALL THE PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4" SHALL BE INCLUDED IN THE COST OF COMBINATION CURB & GUTTER REMOVAL)
(AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)
- Ⓖ SEEDING, CLASS 2A
EROSION CONTROL BLANKET
TOPSOIL FURNISH AND PLACE, 4"
(AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)



• EXISTING SIDEWALK FROM STATION 106+65 TO STATION 113+65 TO REMAIN.

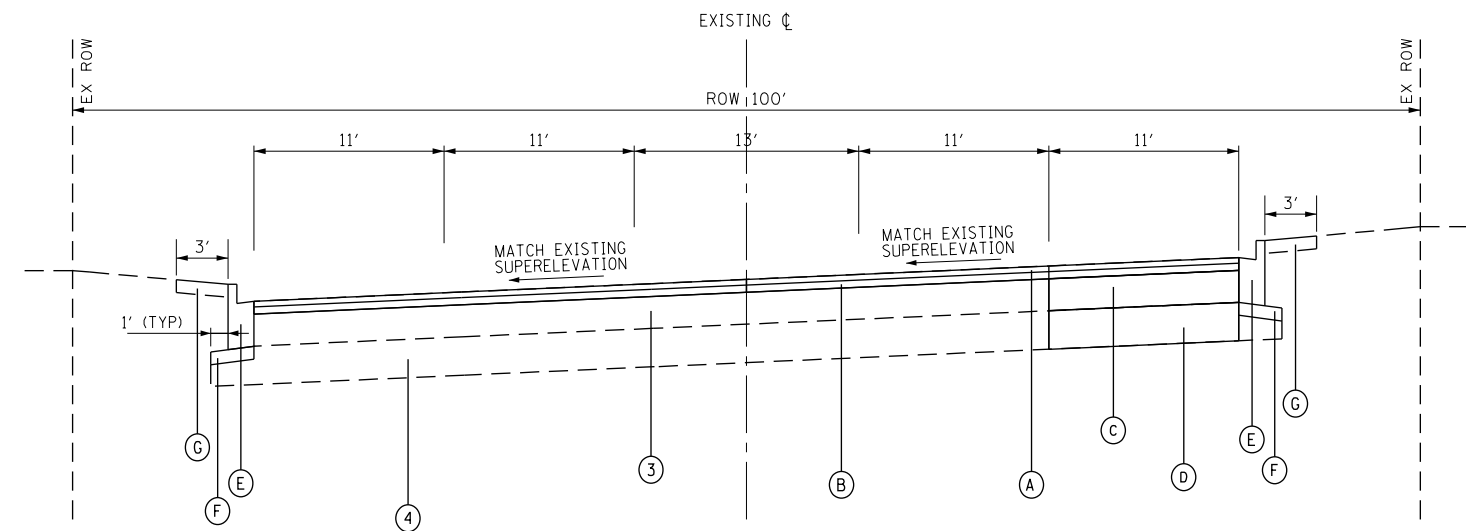
PROPOSED TYPICAL SECTION

STA. 106+61 TO STA. 114+77, LIES ROAD
 STA. 114+86 TO STA. 115+50, LIES ROAD
 STA. 145+00 TO STA. 155+46, LIES ROAD
 NORMAL CROWN



PROPOSED TYPICAL SECTION

STA. 115+50 TO STA. 130+00, LIES ROAD
 SUPERELEVATION RIGHT



PROPOSED TYPICAL SECTION

STA. 130+00 TO STA. 145+00, LIES ROAD
 SUPERELEVATION LEFT

THE CONTRACTOR SHALL MILL BEFORE PATCHING.

MIXTURE TYPE	AIR VOIDS @ Ndes
HMA SURFACE COURSE, MIX "D", N70 (IL 9.5mm), 1.75"	4% @ 70 GYRATIONS
HMA BINDER COURSE, IL-19.0, N70, 2.25"	4% @ 70 GYRATIONS
CLASS D PATCHES (HMA BINDER IL-19 MM), 10"	4% @ 70 GYRATIONS

- NOTES: 1) THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
 2) THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
 3) FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

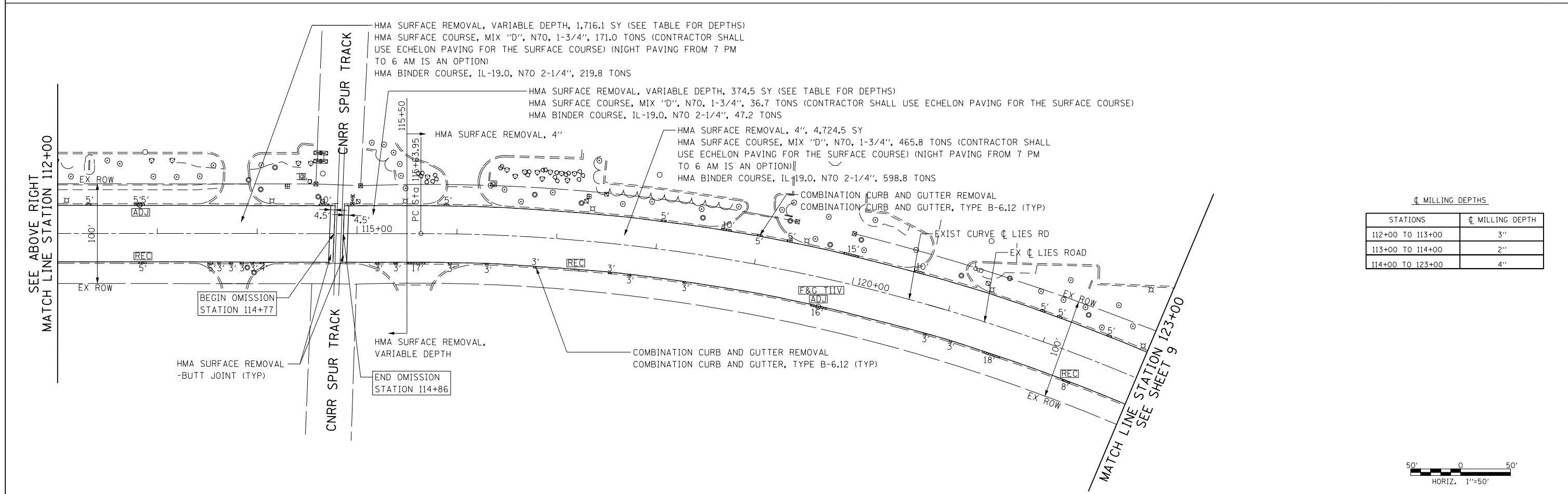
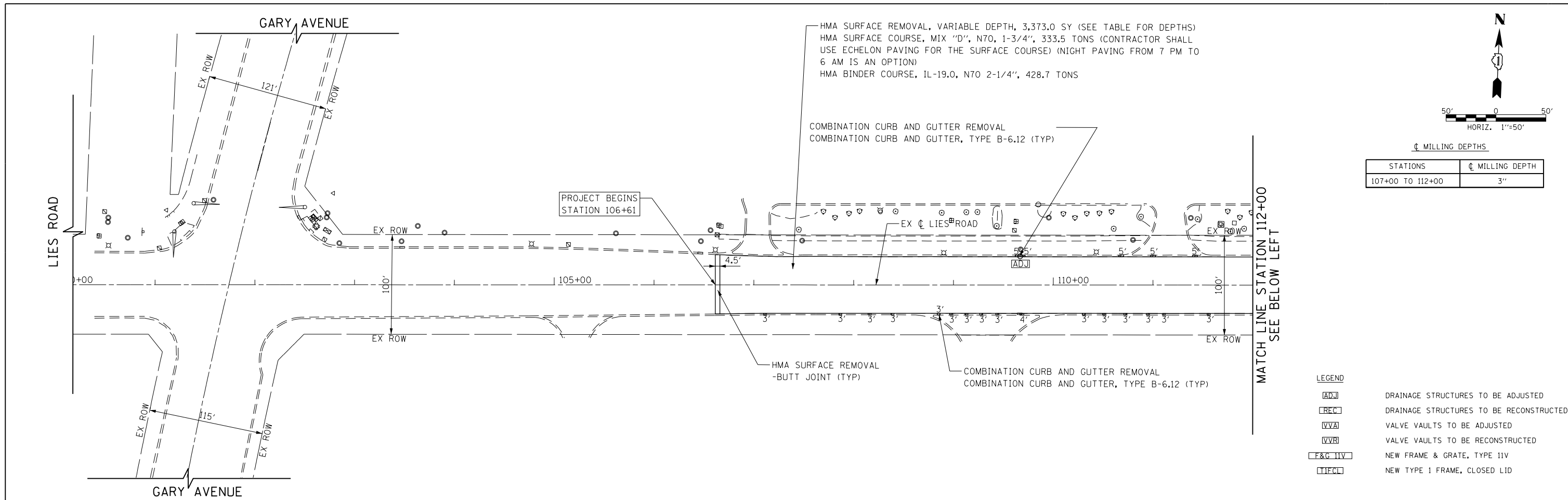
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FILEL		DRAWN - CEC	REVISED -
	PLOT SCALE = *SCALE*	CHECKED - DWB	REVISED -
MODELNAME	PLOT DATE = 11/2/2018	DATE - 11/02/2018	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

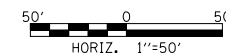
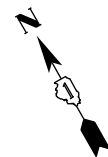
**LIES ROAD RESURFACING
 PROPOSED TYPICAL SECTIONS**

SCALE: NTS SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1375	18-00065-00-RS	DUPAGE	29	7
CONTRACT NO. 61F30			ILLINOIS FED. AID PROJECT	



FILE NAME =	USER NAME = #USER#	DESIGNED - CEC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	LIES ROAD RESURFACING ROADWAY PLAN			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILE#		DRAWN - CEC	REVISED -		SCALE: 1"=50'	SHEET 1 OF 4 SHEETS	STA. 100+00 TO STA. 123+00	1375	18-00065-00-RS	DUPAGE	29	8
#MODELNAME#	PLOT SCALE = #SCALE#	CHECKED - DWB	REVISED -					CONTRACT NO. 61F30				
	PLOT DATE = 11/2/2018	DATE - 11/02/2018	REVISED -		ILLINOIS FED. AID PROJECT							

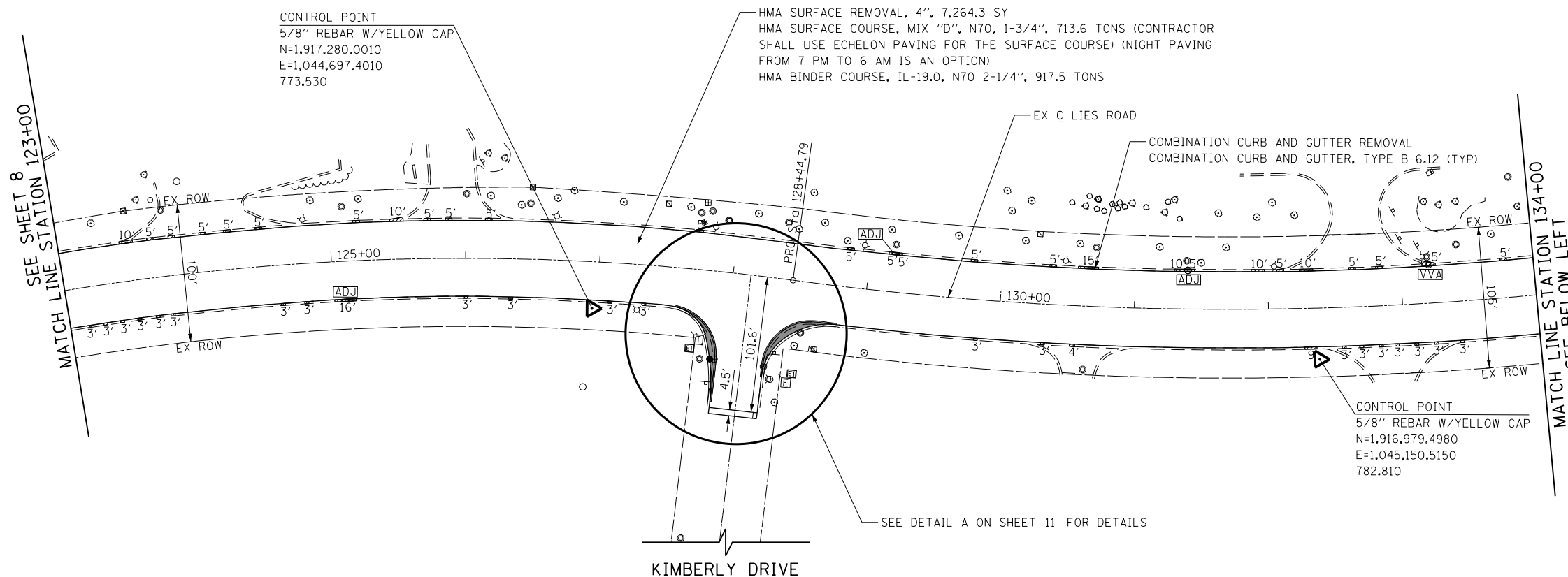


☐ MILLING DEPTHS

STATIONS	☐ MILLING DEPTH
123+00 TO 134+00	4"

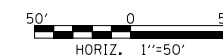
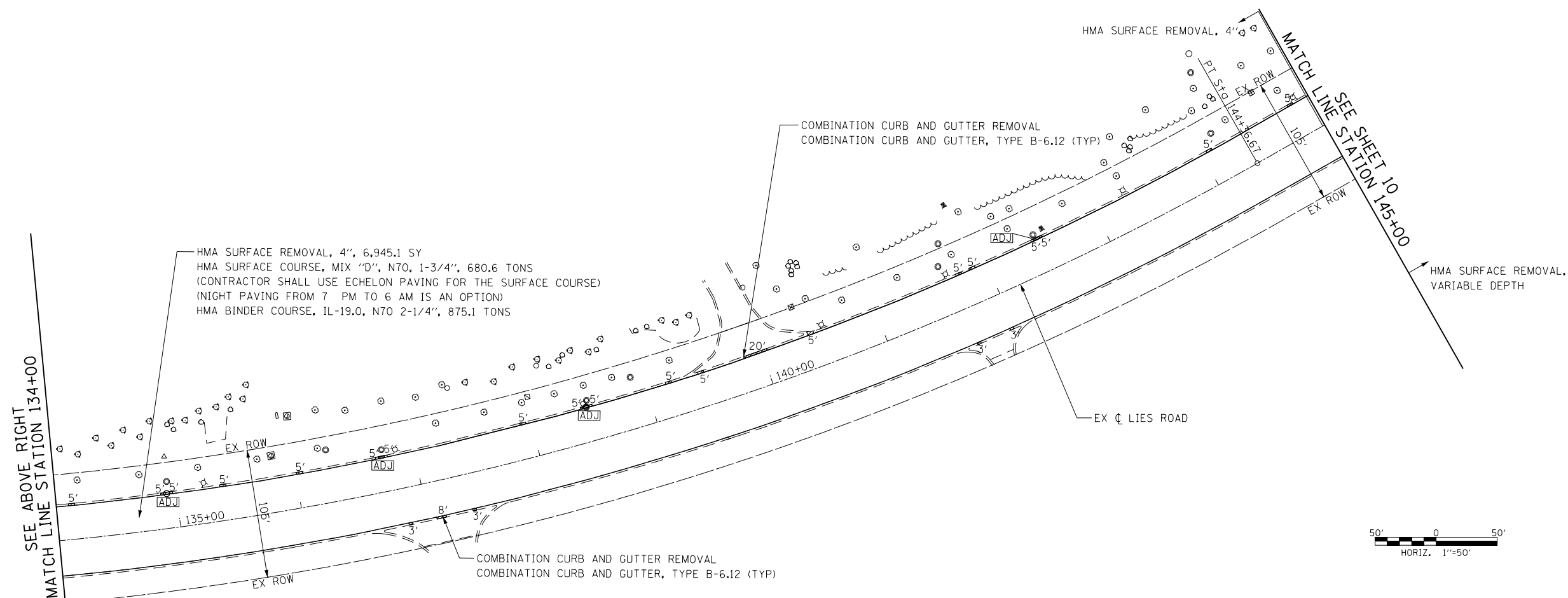
LEGEND

- ☐ ADJ DRAINAGE STRUCTURES TO BE ADJUSTED
- ☐ REC DRAINAGE STRUCTURES TO BE RECONSTRUCTED
- ☐ VVA VALVE VAULTS TO BE ADJUSTED
- ☐ VVR VALVE VAULTS TO BE RECONSTRUCTED
- ☐ F&G 11V NEW FRAME & GRATE, TYPE 11V
- ☐ MEFC NEW TYPE 1 FRAME, CLOSED LID

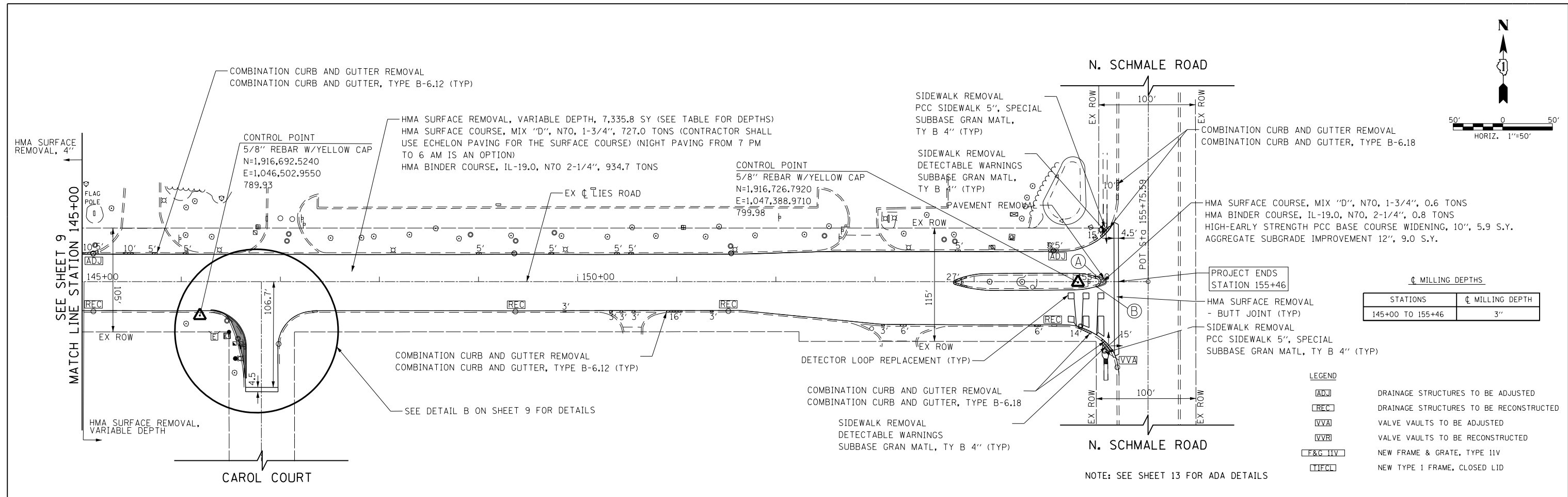
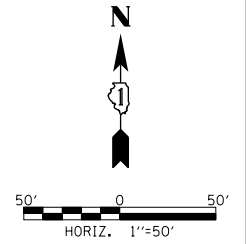


☐ MILLING DEPTHS

STATIONS	☐ MILLING DEPTH
134+00 TO 145+00	4"



FILE NAME = #FILE#	USER NAME = #USER#	DESIGNED - CEC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	LIES ROAD RESURFACING ROADWAY PLAN			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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		DATE - 11/02/2018	REVISED -				ILLINOIS FED. AID PROJECT					



☐ MILLING DEPTHS

STATIONS	☐ MILLING DEPTH
145+00 TO 155+46	3"

LEGEND

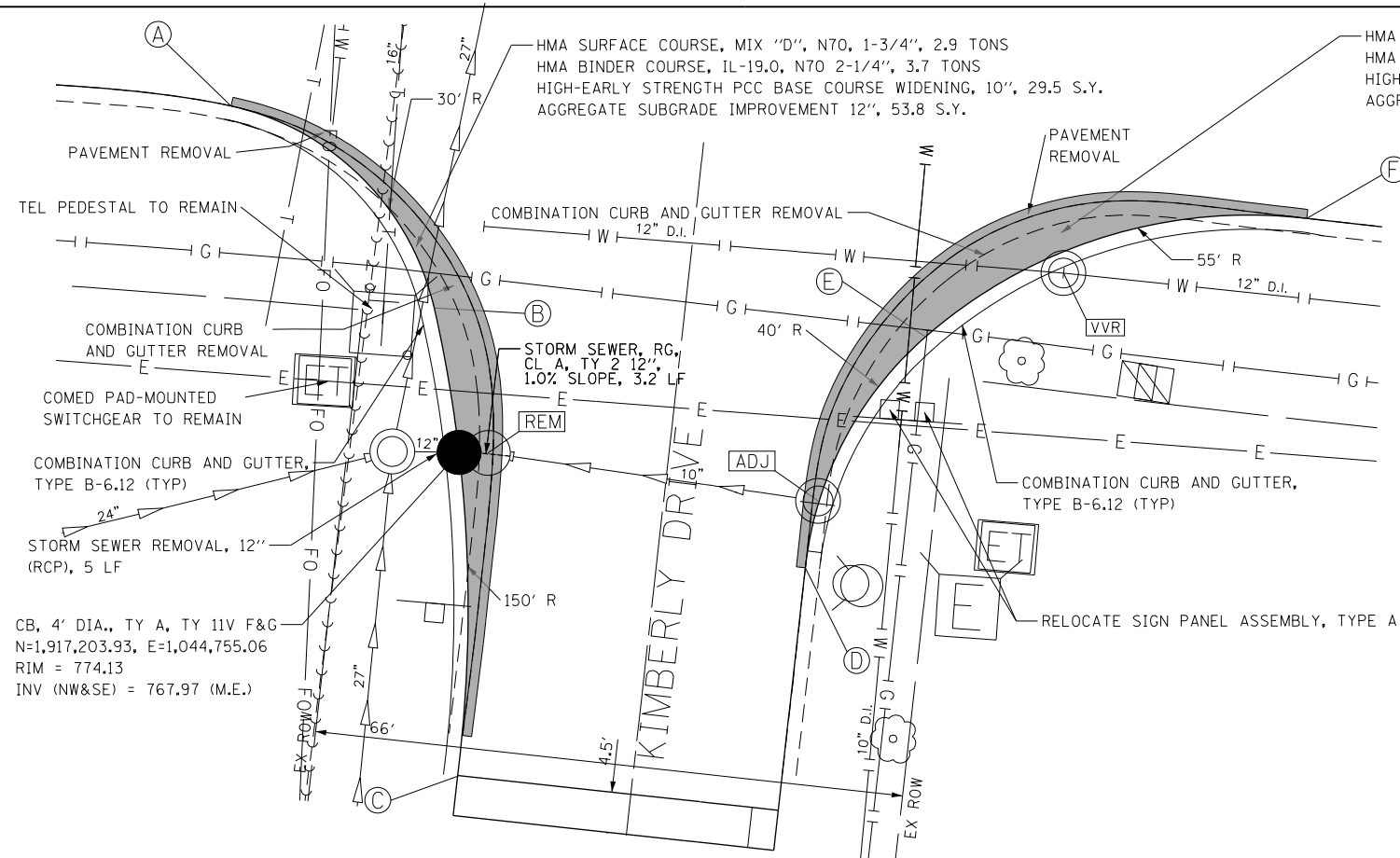
ADJ	DRAINAGE STRUCTURES TO BE ADJUSTED
REC	DRAINAGE STRUCTURES TO BE RECONSTRUCTED
VVA	VALVE VAULTS TO BE ADJUSTED
VVR	VALVE VAULTS TO BE RECONSTRUCTED
F&G 11V	NEW FRAME & GRATE, TYPE 11V
MEFC	NEW TYPE 1 FRAME, CLOSED LID

NOTE: SEE SHEET 13 FOR ADA DETAILS

PROPOSED MEDIAN NOSE MODIFICATIONS

LETTER	LOCATION	NORTHING	EASTING	ELEVATION
A	EOP PC	1,917,125.17	1,044,752.29	773.72 (M.E.)
B	EOP PT	1,917,172.66	1,044,736.88	774.60 (M.E.)

NOTE: PR MEDIAN NOSE RADIUS IS 5 FEET.



LEGEND

ADJ	DRAINAGE STRUCTURES TO BE ADJUSTED
REC	DRAINAGE STRUCTURES TO BE RECONSTRUCTED
VVA	VALVE VAULTS TO BE ADJUSTED
VVR	VALVE VAULTS TO BE RECONSTRUCTED
F&G 11V	NEW FRAME & GRATE, TYPE 11V
TIFCL	NEW TYPE 1 FRAME, CLOSED LID

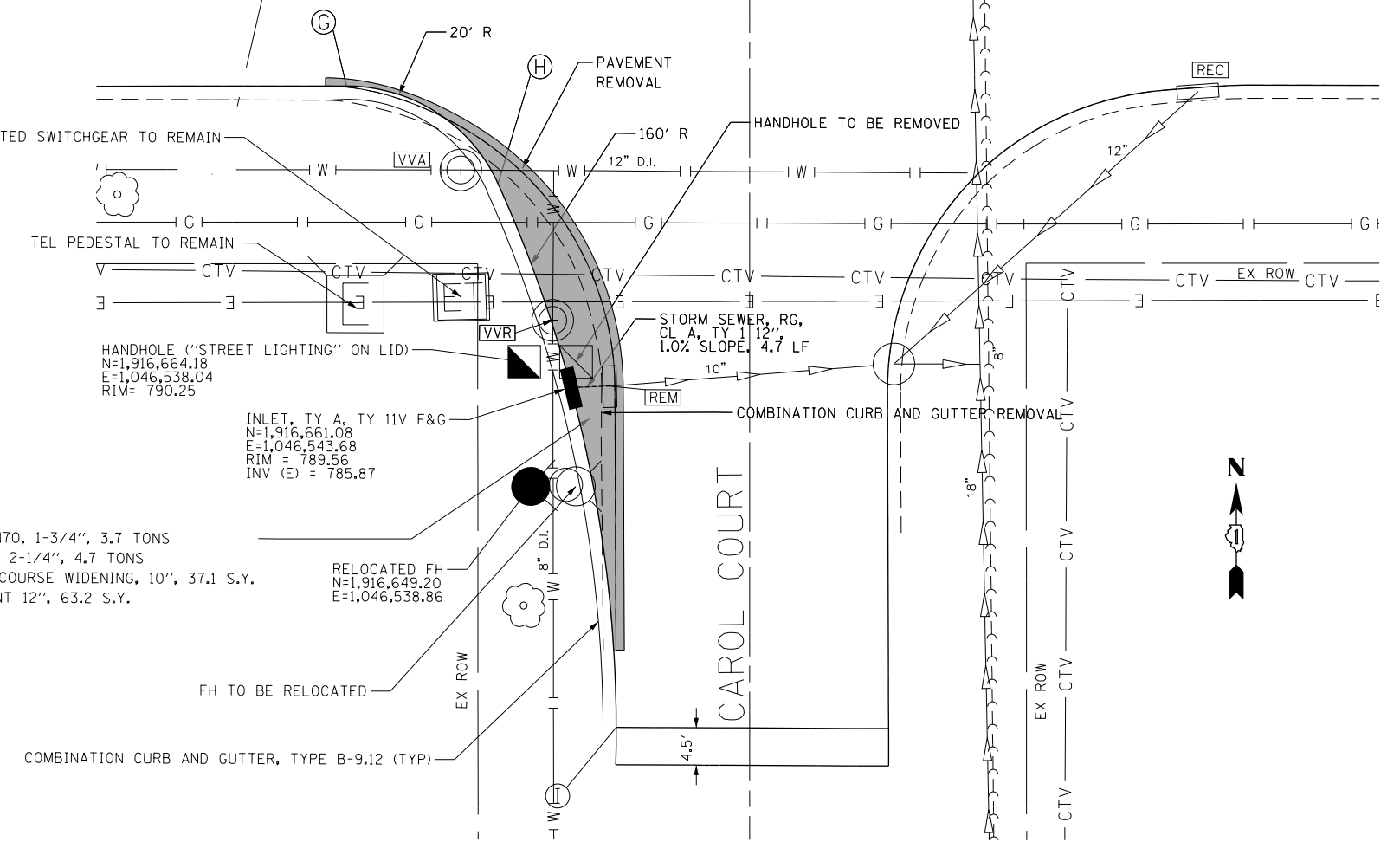
DETAIL A

LETTER	LOCATION	NORTHING	EASTING	ELEVATION
A	EOP PC	1,917,250.17	1,044,752.29	773.72 (M.E.)
B	EOP PCC	1,917,219.30	1,044,760.70	774.00
C	EOP PT	1,917,172.66	1,044,736.88	774.60 (M.E.)
D	EOP PT	1,917,173.48	1,044,782.17	774.38 (M.E.)
E	EOP PCC	1,917,189.15	1,044,806.68	774.71
F	EOP PC	1,917,179.40	1,044,850.18	776.00 (M.E.)



DETAIL B

LETTER	LOCATION	NORTHING	EASTING	ELEVATION
G	EOP PC	1,916,697.34	1,046,516.64	789.39
H	EOP PCC	1,916,685.60	1,046,534.90	789.46
I	EOP PT	1,916,620.24	1,046,549.18	M.E.



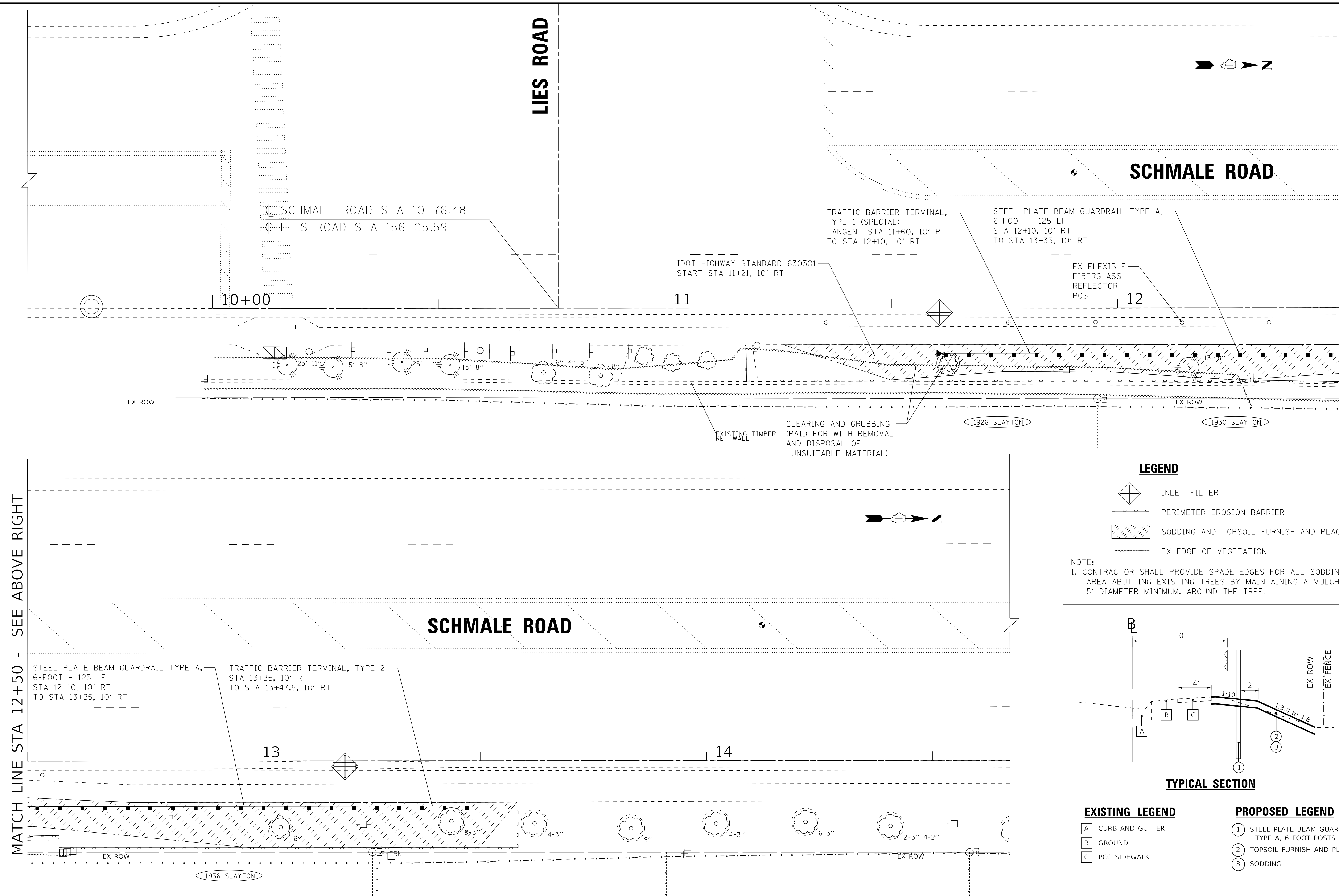
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MODELNAME	PLOT DATE = 11/6/2018	DATE - 11/02/2018	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

LIES ROAD RESURFACING ROADWAY PLAN	
SCALE: 1"=5'	SHEET 4 OF 4 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1375	18-00065-00-RS	DUPAGE	29	11
CONTRACT NO. 61F30				
ILLINOIS FED. AID PROJECT				

STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. - 184-001121 - EXPIRES 4/30/2019
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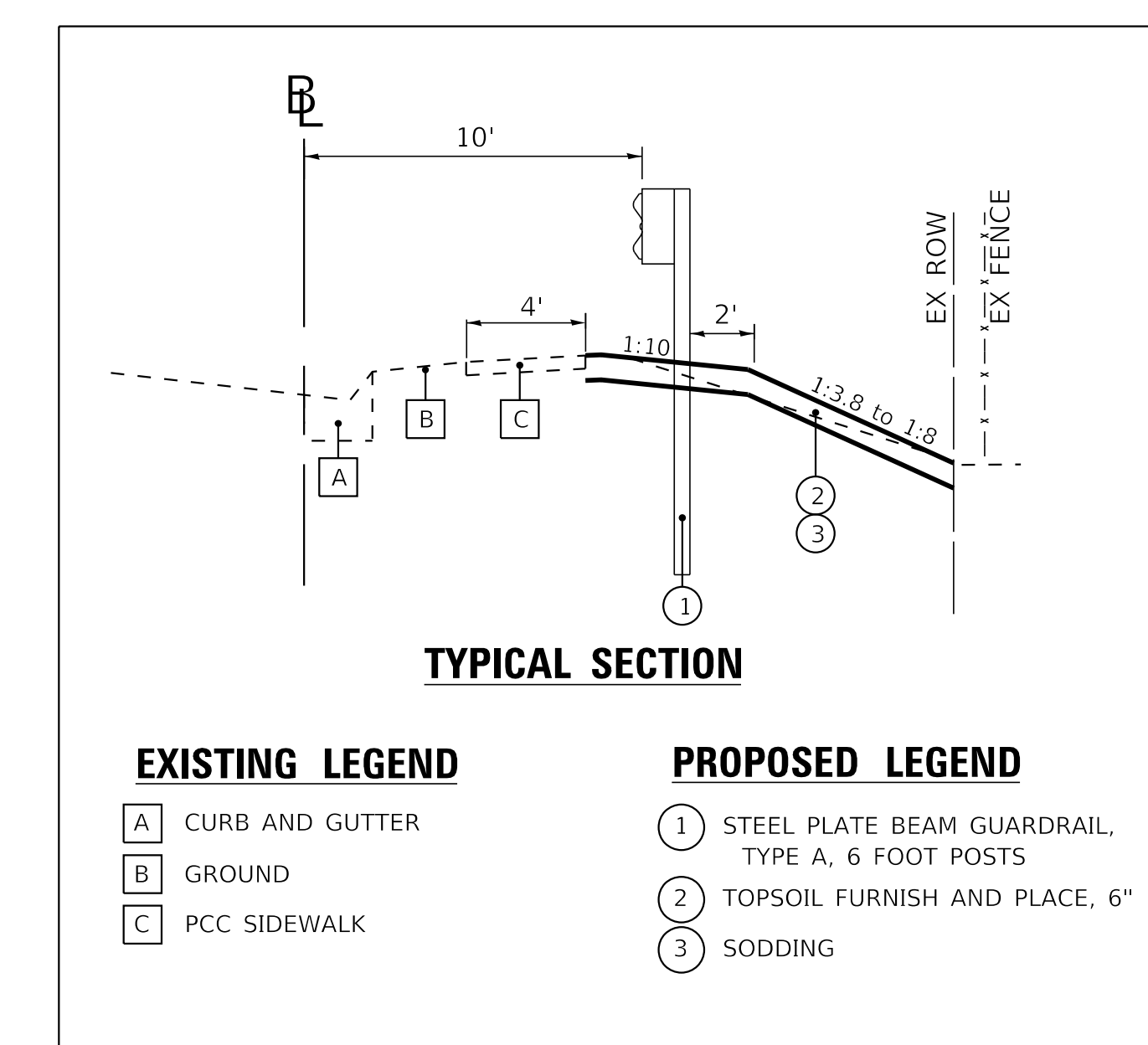
MATCH LINE STA 12+50 - SEE ABOVE RIGHT

MATCH LINE STA 12+50 - SEE BELOW LEFT

LEGEND

- INLET FILTER
- PERIMETER EROSION BARRIER
- SODDING AND TOPSOIL FURNISH AND PLACE, 6"
- EX EDGE OF VEGETATION

NOTE:
 1. CONTRACTOR SHALL PROVIDE SPADE EDGES FOR ALL SODDING AREA ABUTTING EXISTING TREES BY MAINTAINING A MULCH RING, 5' DIAMETER MINIMUM, AROUND THE TREE.



- | | |
|------------------------|--|
| EXISTING LEGEND | PROPOSED LEGEND |
| A CURB AND GUTTER | 1 STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS |
| B GROUND | 2 TOPSOIL FURNISH AND PLACE, 6" |
| C PCC SIDEWALK | 3 SODDING |



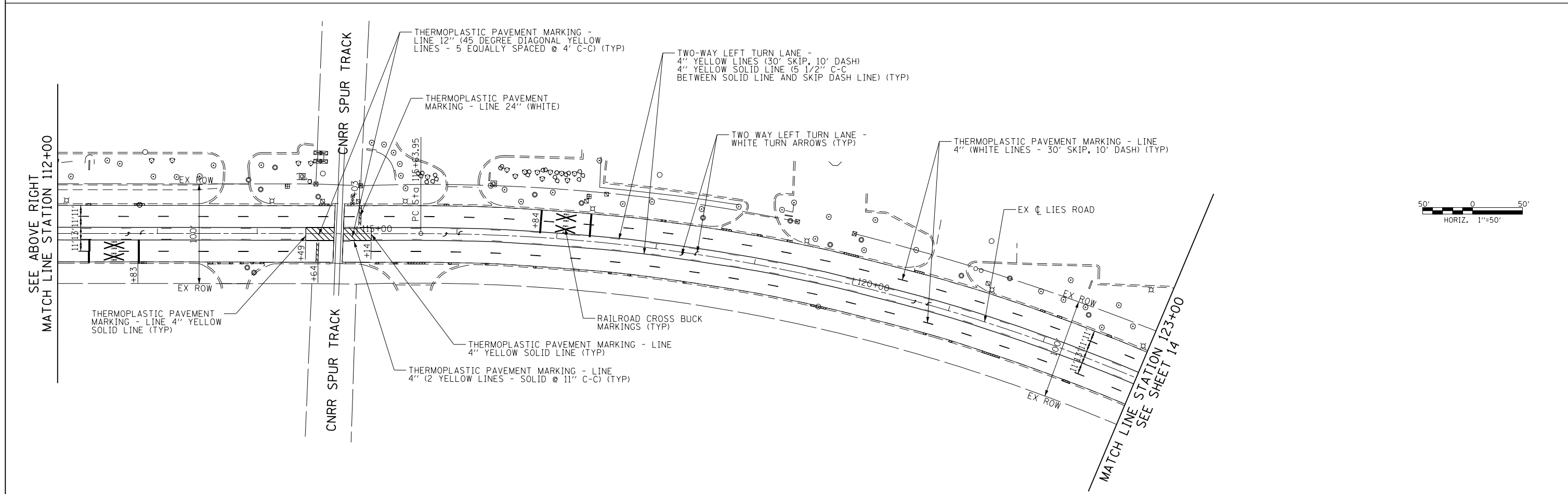
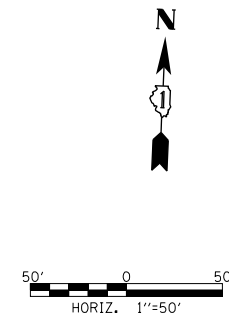
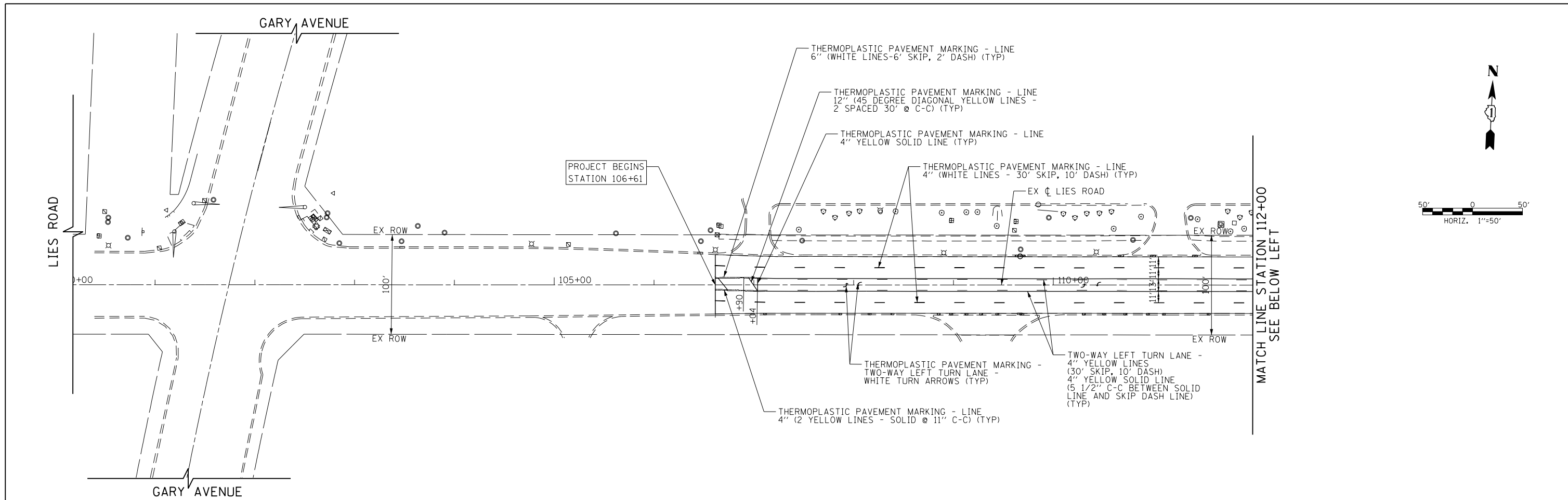
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PLOT DATE = 11/17/2018	CHECKED - DTH	REVISED -
	DATE - 11/01/18	FILE 161262-SHT-Plan1.dgn

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

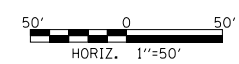
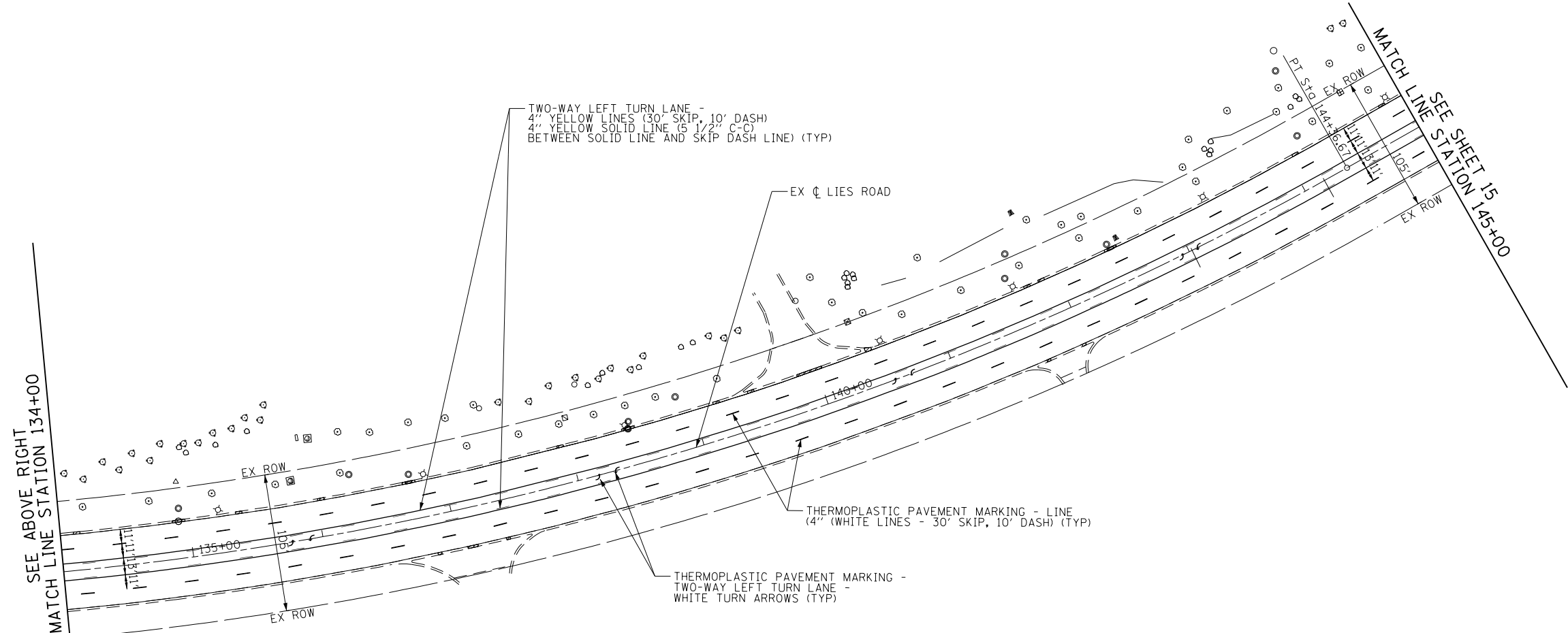
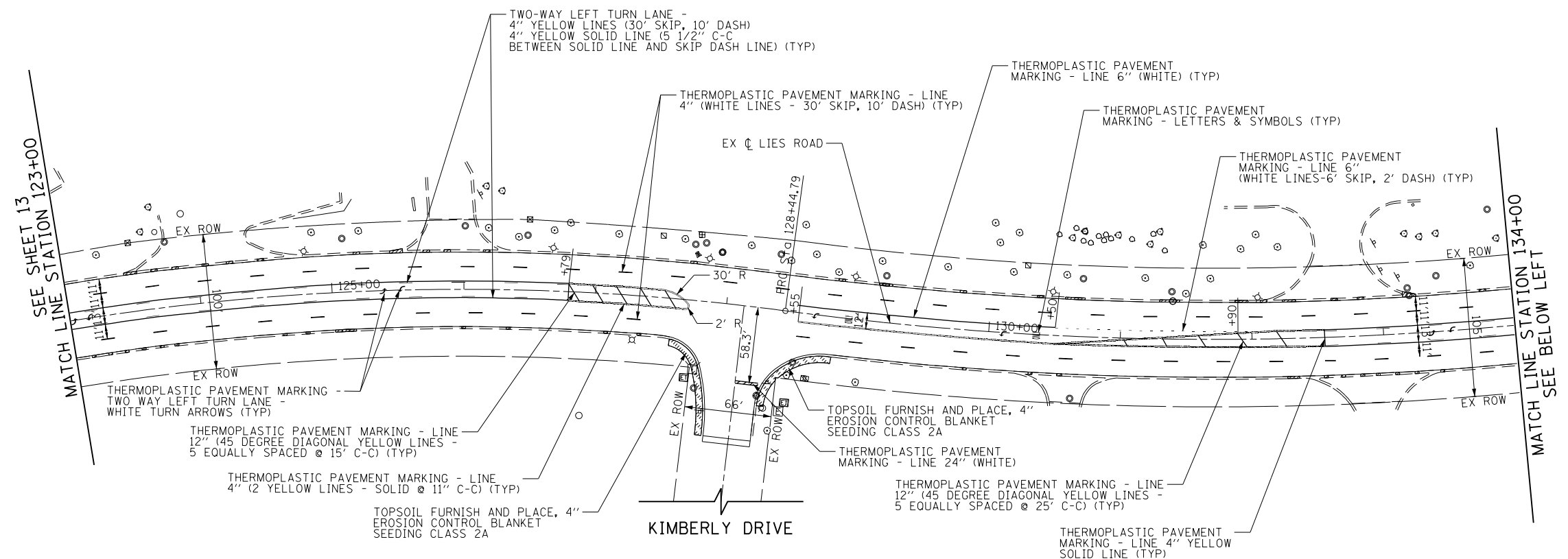
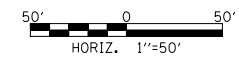
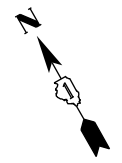
LIES ROAD RESURFACING
 GUARDRAIL INSTALLATION PLAN

SCALE: 1" = 10' SHEET OF SHEETS STA. TO STA.

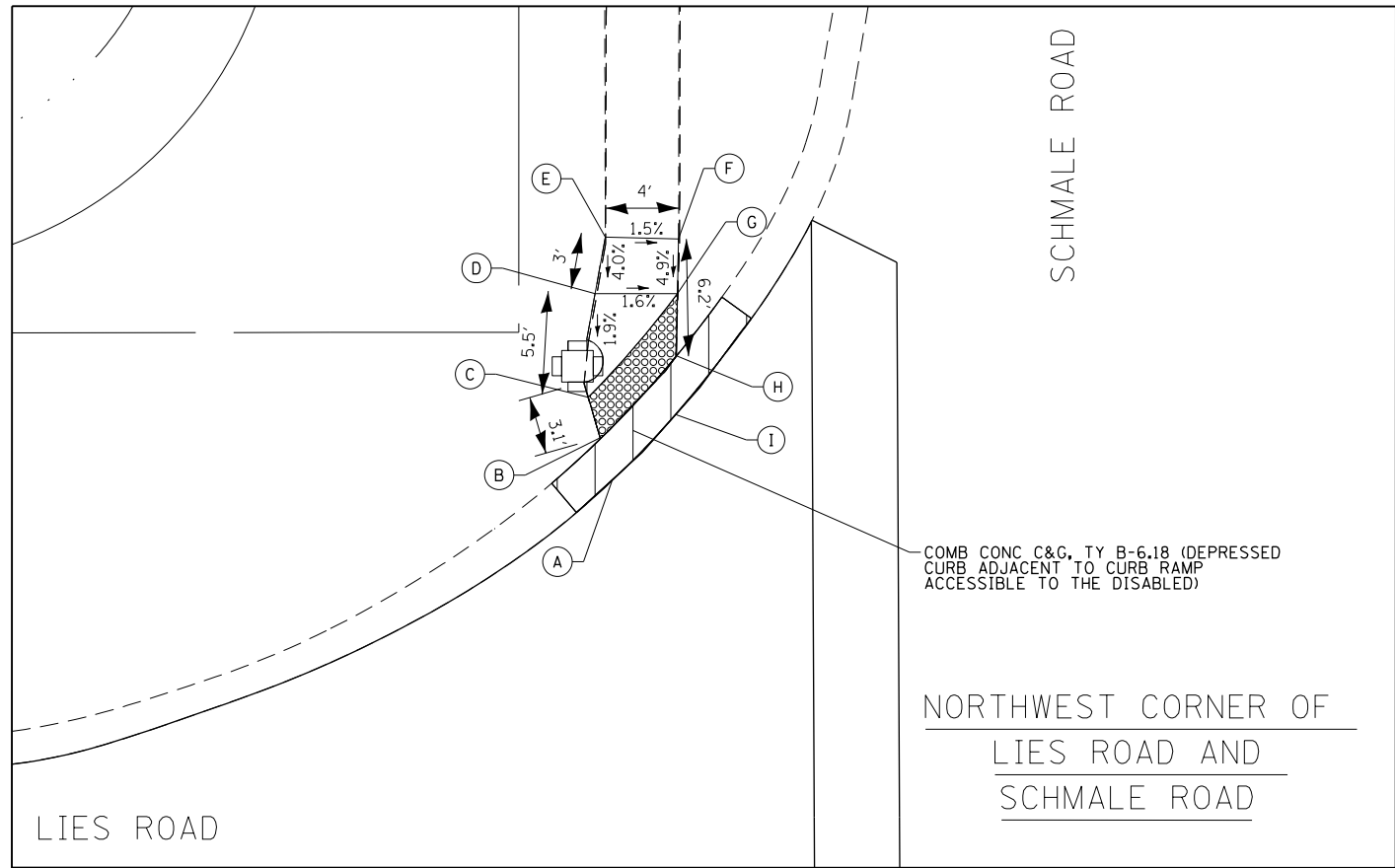
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1375	18-00065-00-RS	DUPAGE	29	12
CONTRACT NO. 61F30			ILLINOIS FED. AID PROJECT	



FILE NAME = #FILE#	USER NAME = #USER#	DESIGNED - CEC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	LIES ROAD RESURFACING PAVEMENT MARKING AND LANDSCAPING PLAN			F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT DATE = 11/2/2018	CHECKED - DWB	REVISED -					CONTRACT NO. 61F30				
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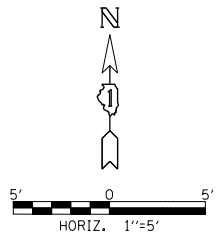


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	ELEVATION
A	(798.14)
B	798.10
C	798.11
D	798.21
E	(798.27)
F	(798.21)
G	798.07
H	798.04
I	798.08

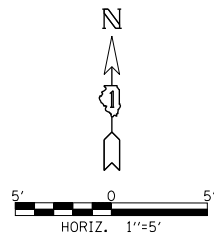
798.15



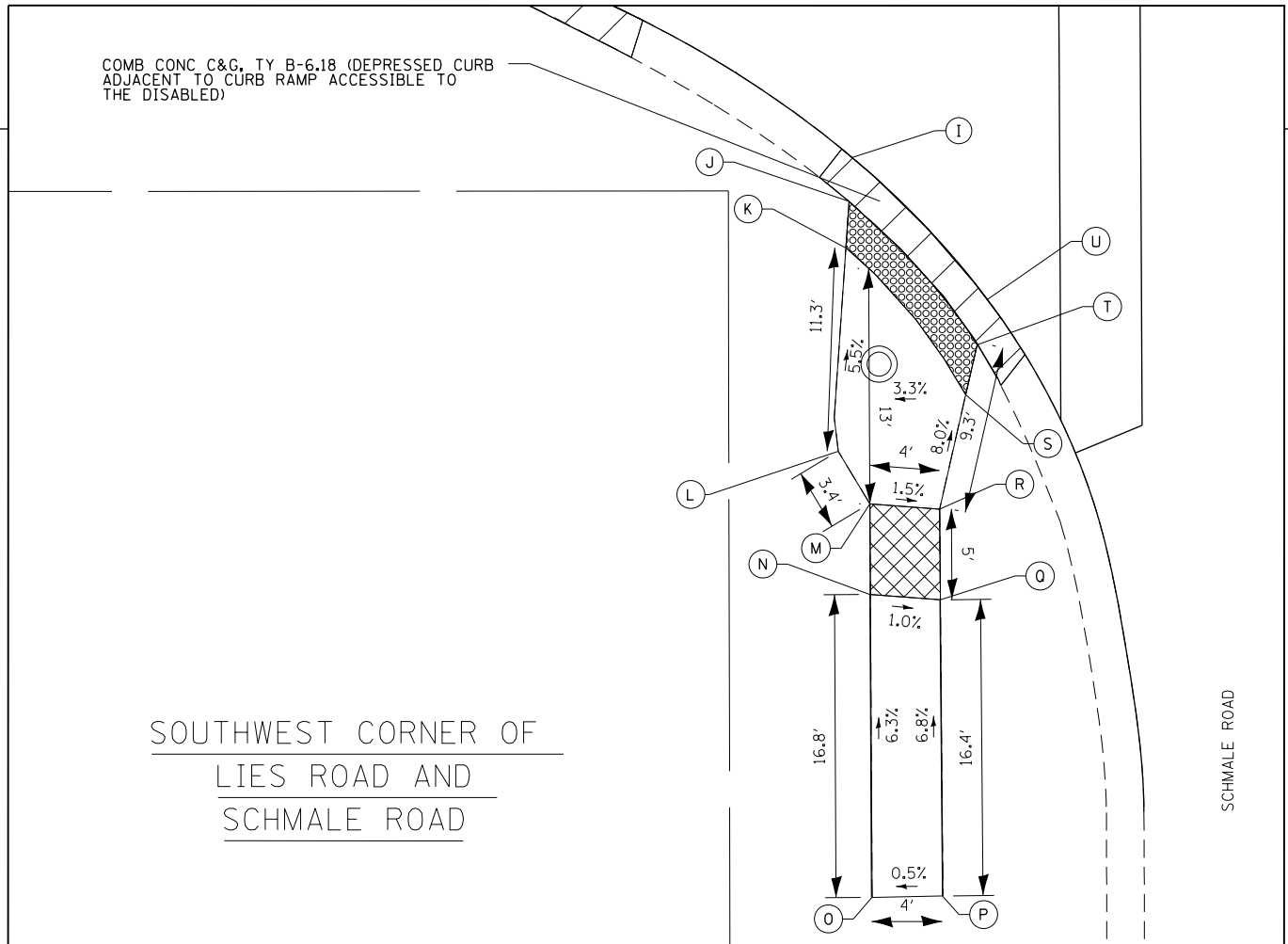
NORTHWEST CORNER OF
LIES ROAD AND
SCHMALE ROAD

LEGEND

(XX.XX')	EXISTING ELEVATION
	PROPOSED SIDEWALK
	DETECTABLE WARNINGS
	ADA LANDING AREA (PCC SIDEWALK)



	ELEVATION
I	(800.35)
J	800.31
K	800.34
L	800.96
M	801.08
N	801.13
O	(802.18)
P	(802.20)
Q	801.09
R	801.02
S	800.50
T	800.47
U	800.51



SOUTHWEST CORNER OF
LIES ROAD AND
SCHMALE ROAD

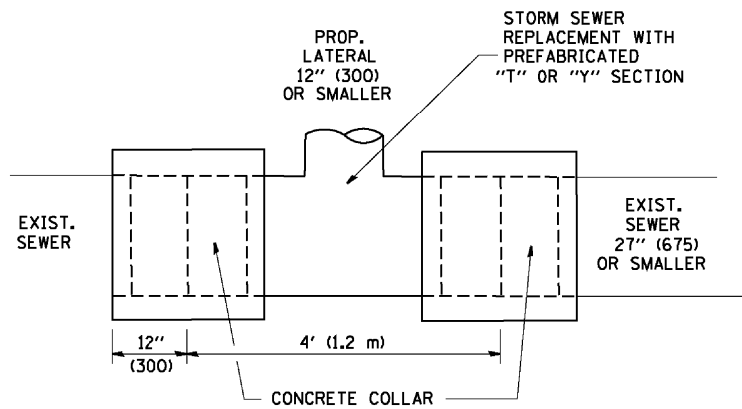
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MODELNAME	PLOT DATE = 11/2/2018	DATE - 11/2/2018	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

LIES ROAD RESURFACING
ADA DETAILS

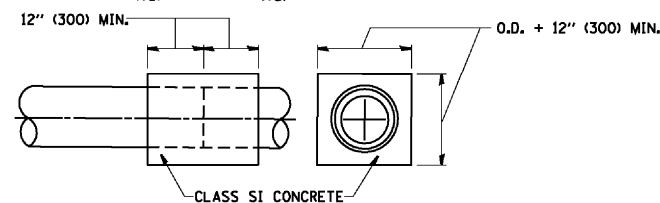
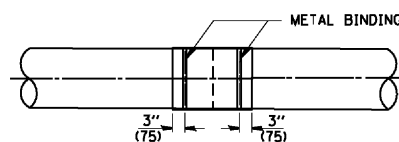
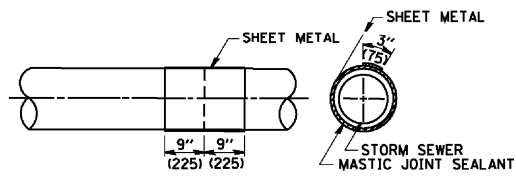
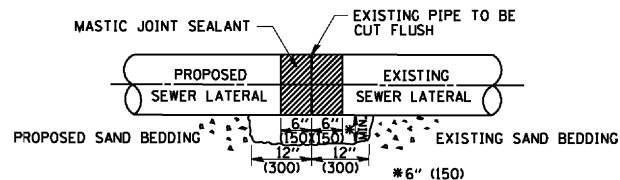
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1375	18-00065-00-RS	DUPAGE	29	16
				CONTRACT NO. 61F30
ILLINOIS FED. AID PROJECT				



DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER

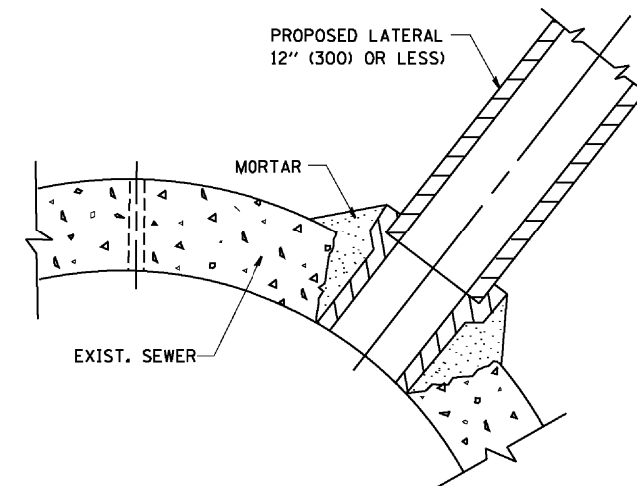


DETAIL "B"

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

- CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
- APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
- BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12' x 6' (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
- CUT A PIECE OF SHEET METAL GAGE NO. 19 L1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
- WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
- LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
- PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
- WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OOOZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
- PLACE CLASS SI CONCRETE AROUND THE JOINT.



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS, THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

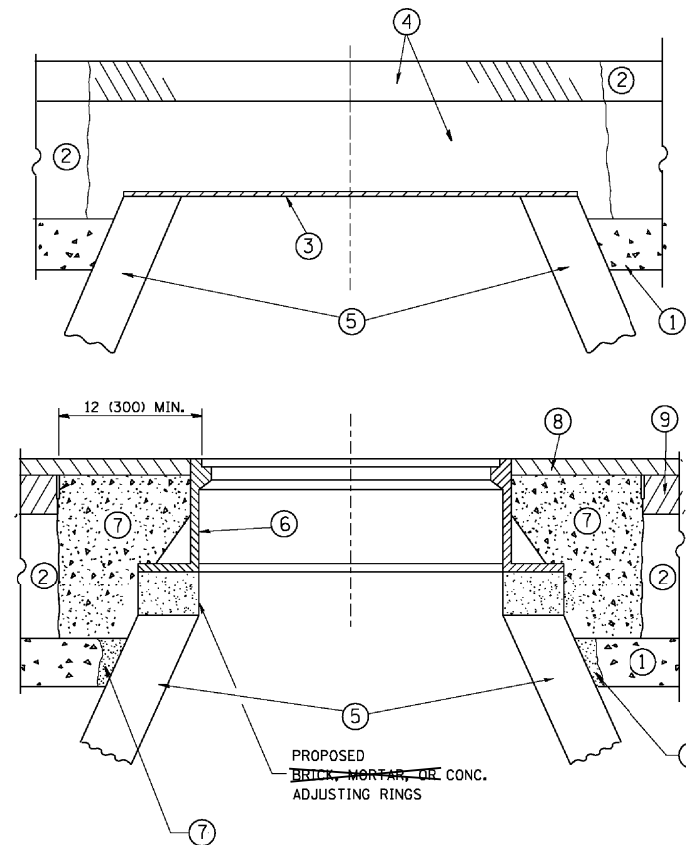
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	PLOT DATE = 1/4/2008	DATE - 07-25-90	REVISED - R. SHAH 06-12-96

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAIL OF STORM SEWER
CONNECTION TO EXISTING SEWER**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1375	18-00065-00-RS	DUPAGE	29	17
BD500-01 (BD-7)			CONTRACT NO. 61F30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

VILLAGE OF CAROL STREAM CASTINGS ARE THE PROPERTY OF THE VILLAGE AND THE CONTRACTOR SHALL NOTIFY THE VILLAGE FOR REMOVAL AND DISPOSITION OF THE CASTINGS

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

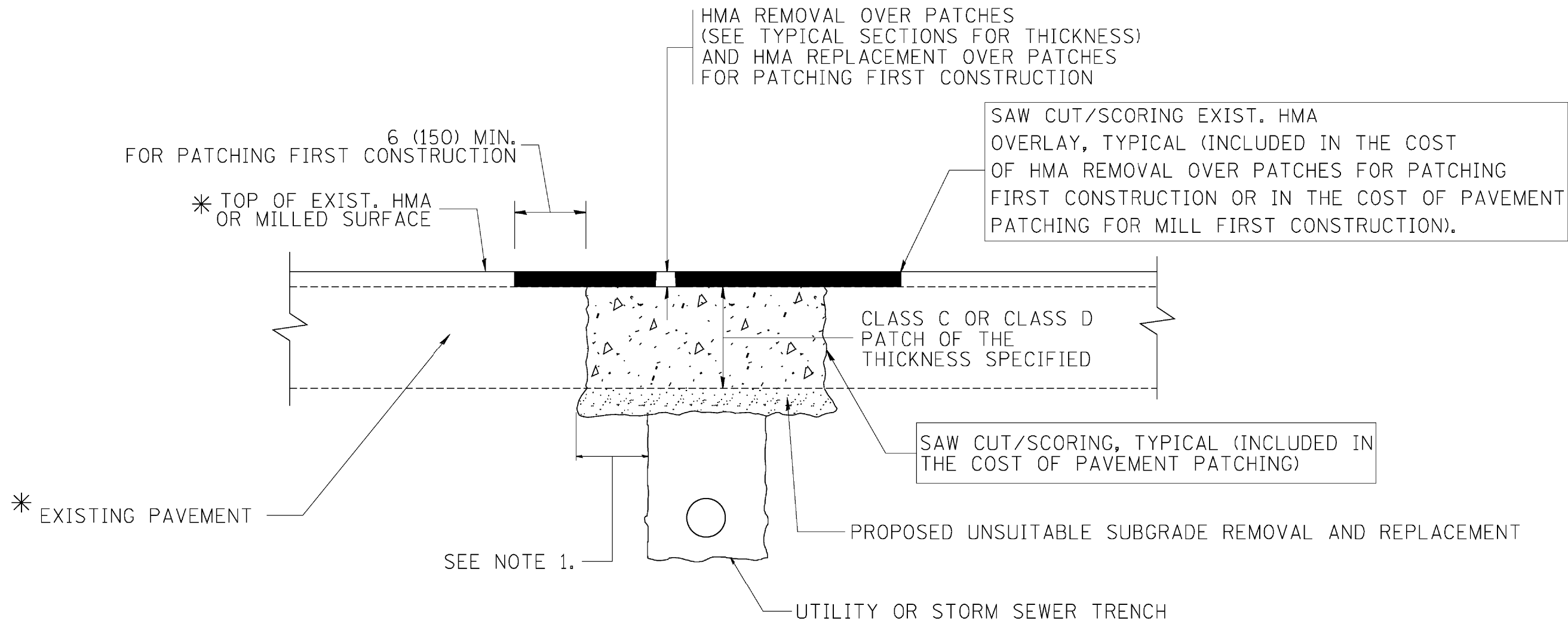
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1375	18-00065-00-RS	DUPAGE	29	18
BD600-03 (BD-8)		CONTRACT NO. 61F30		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

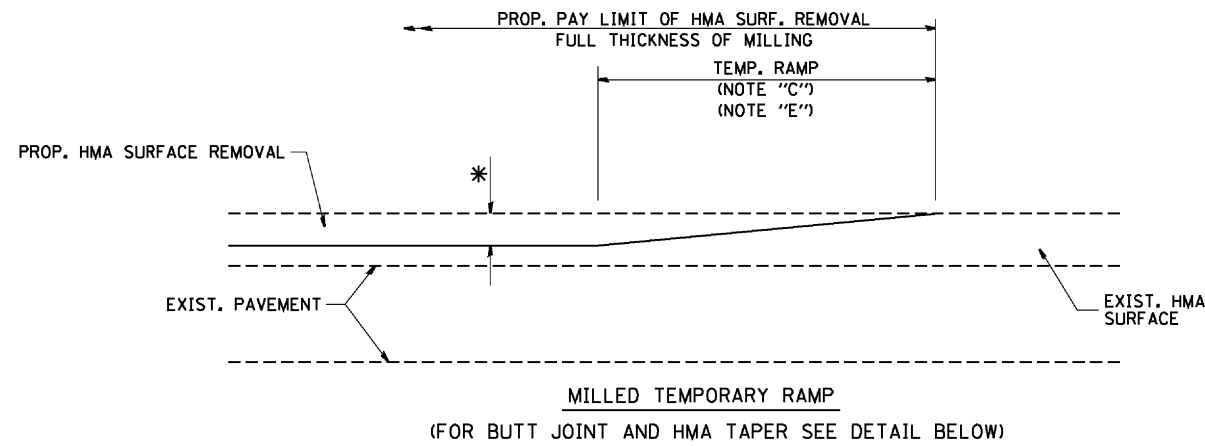
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

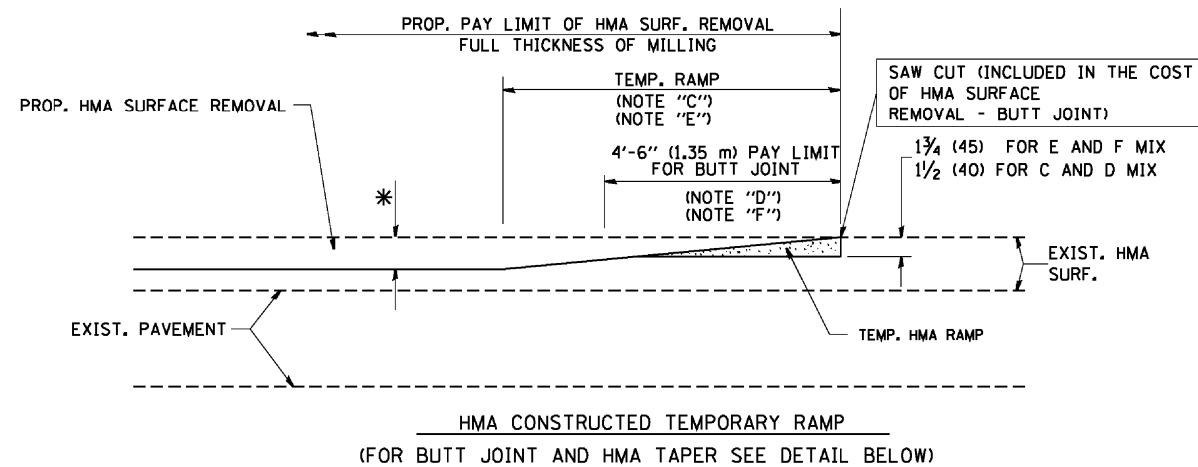
**PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

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BD400-04 (BD-22)			CONTRACT NO. 61F30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

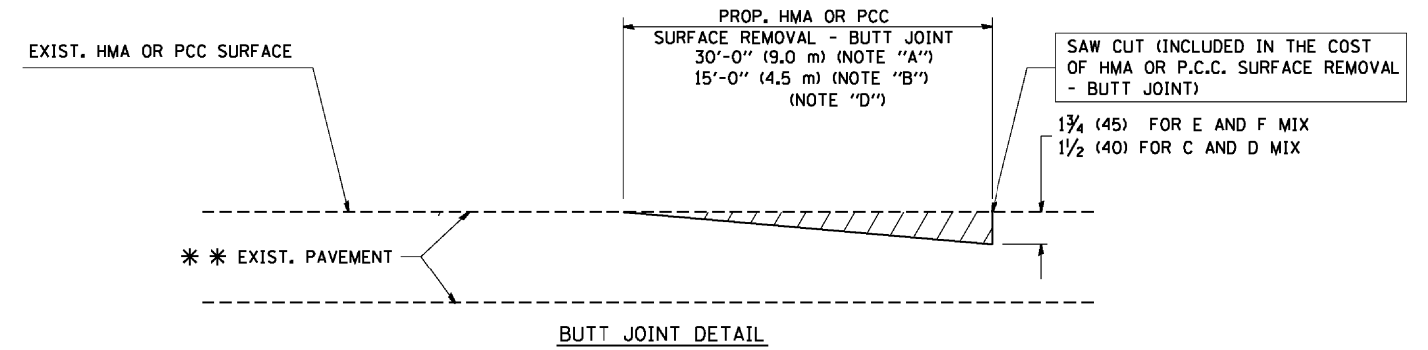


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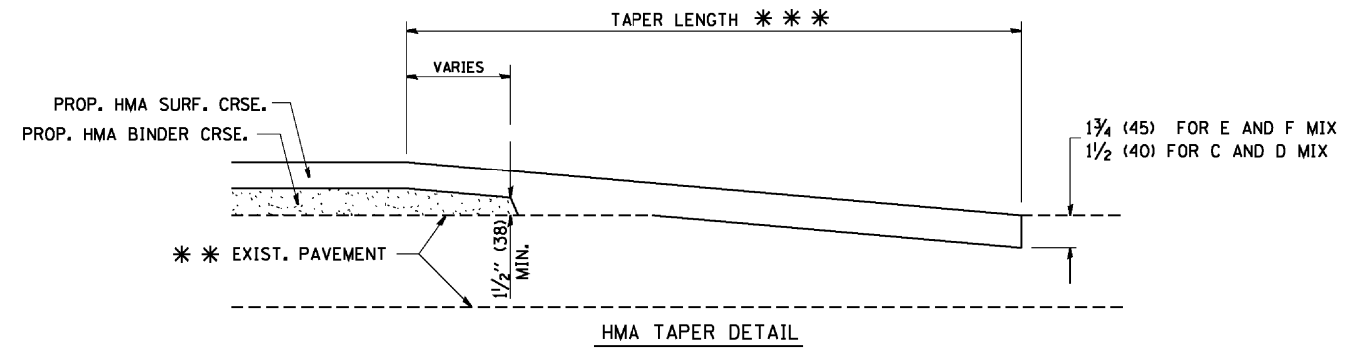


OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

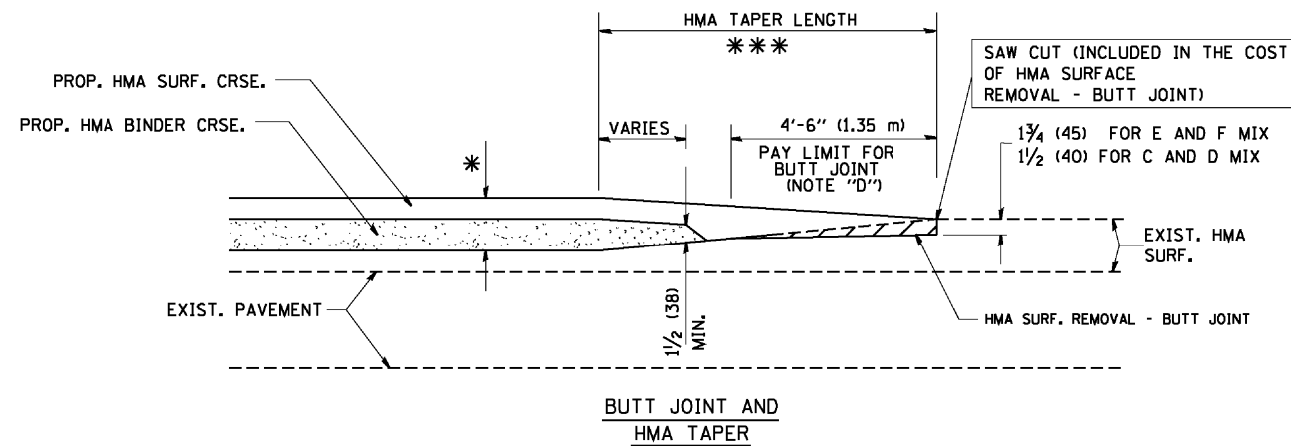
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

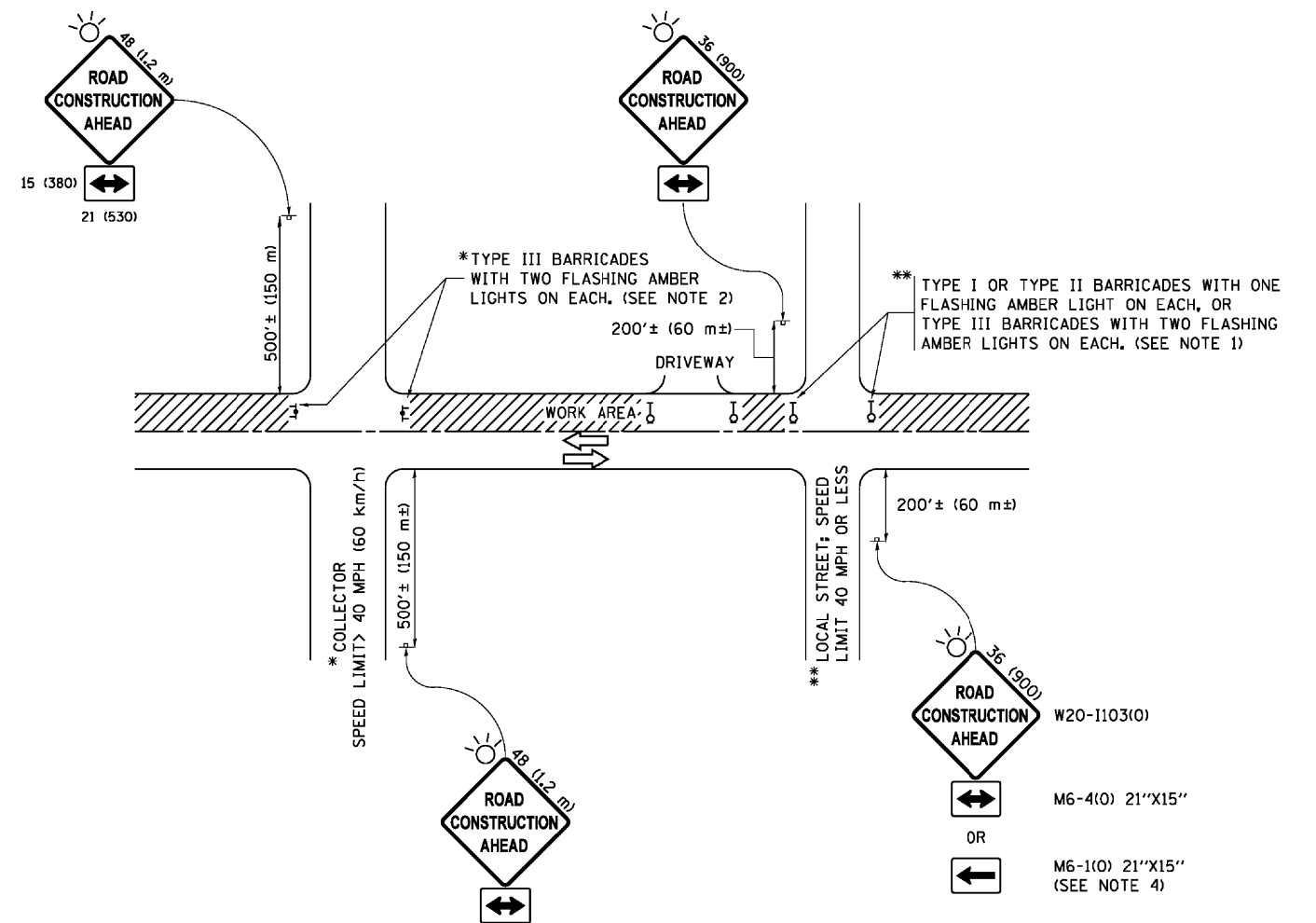
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	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1375	18-00065-00-RS	DUPAGE	29	20
BD400-05 BD32			CONTRACT NO. 61F30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

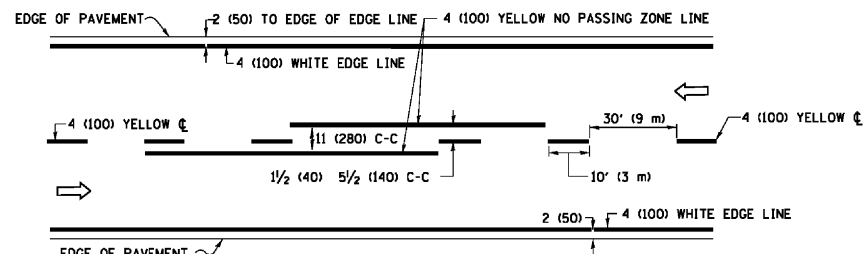
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

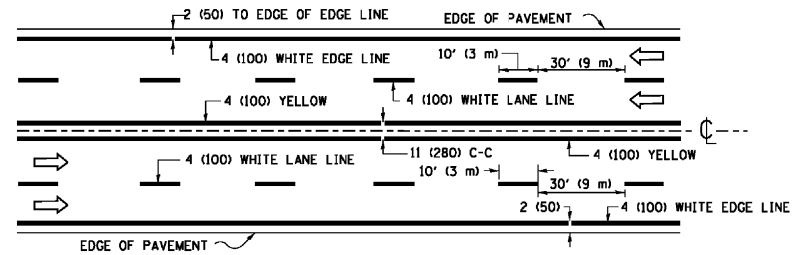
**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

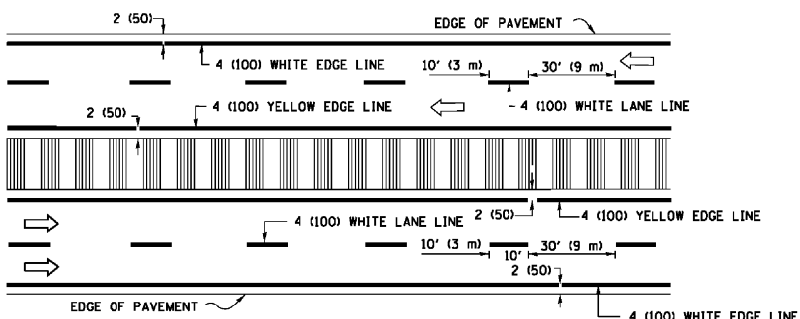
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1375	18-00065-00-RS	DUPAGE	29	21
TC-10			CONTRACT NO. 61F30	
ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

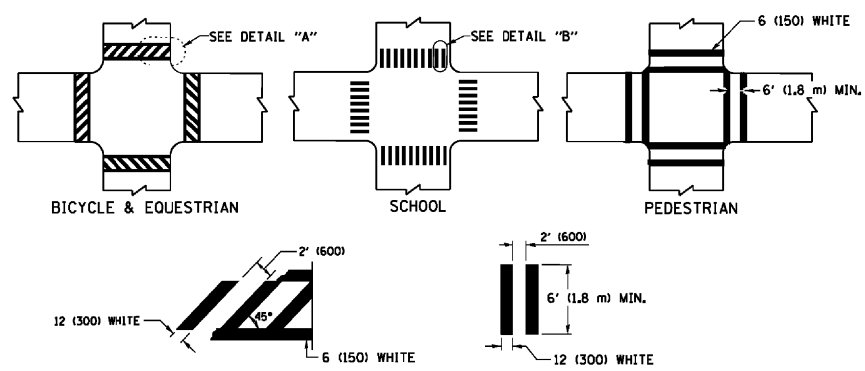


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

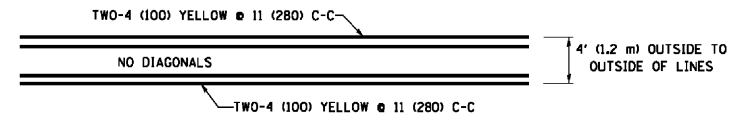


DETAIL "A"

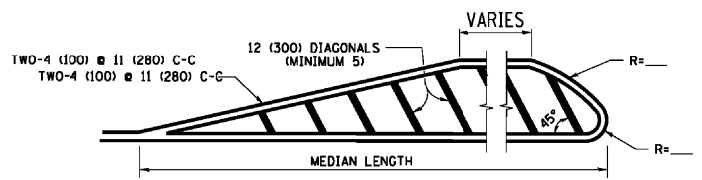
DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

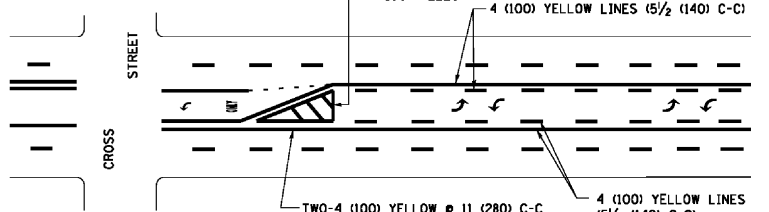


4' (1.2 m) WIDE MEDIANS ONLY



DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

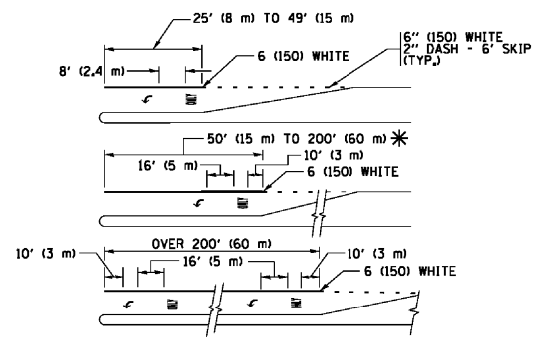
MEDIANS OVER 4' (1.2 m) WIDE



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

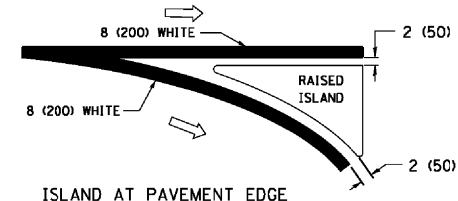
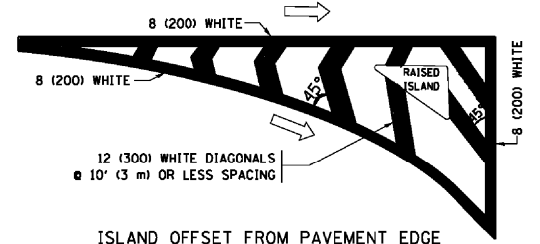


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 * AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

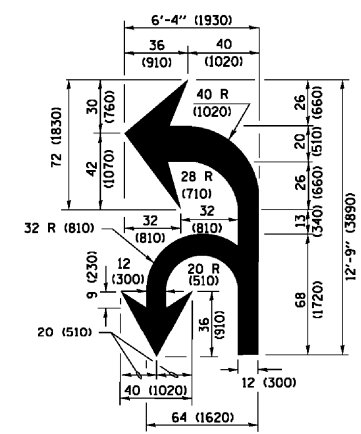
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

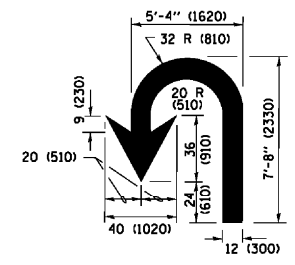
TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS; 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "RR"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

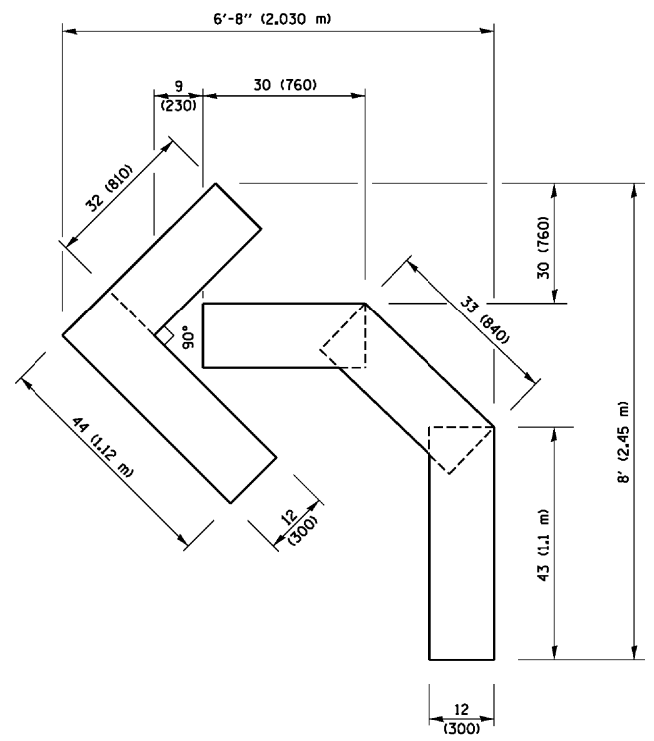
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	PLOT DATE = 4/13/2016		REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

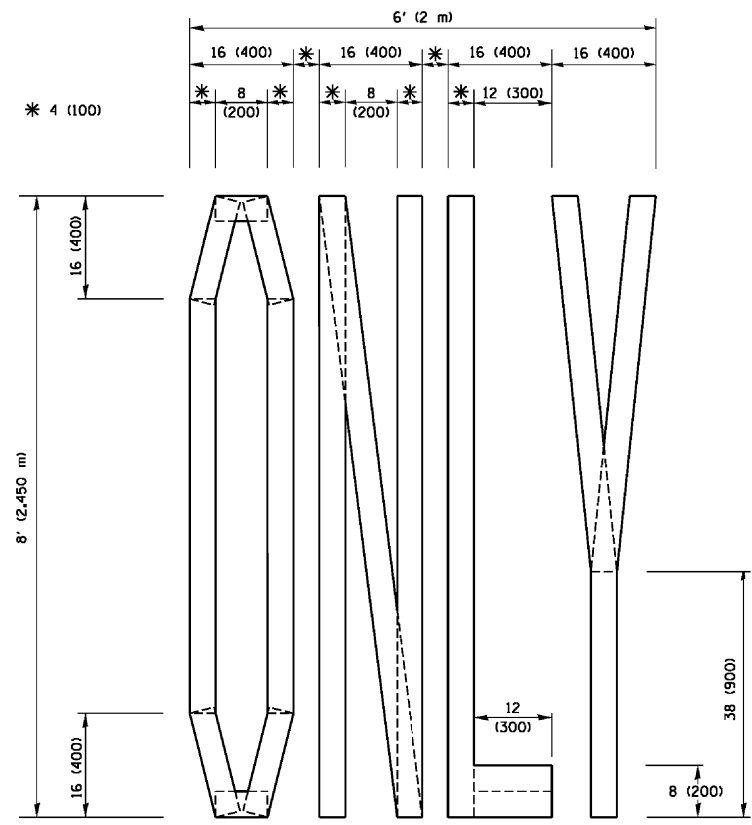
**DISTRICT ONE
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

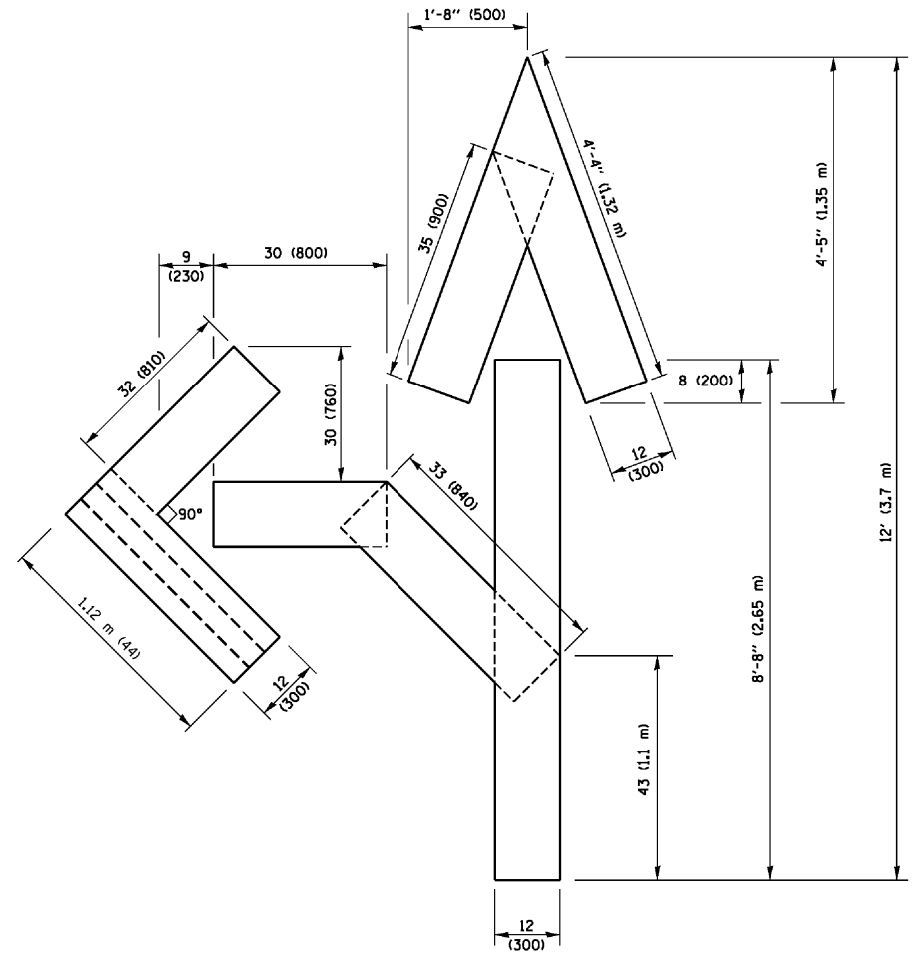
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1375	18-00065-00-RS	DUPAGE	29	22
TC-13		CONTRACT NO. 61F30		
ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

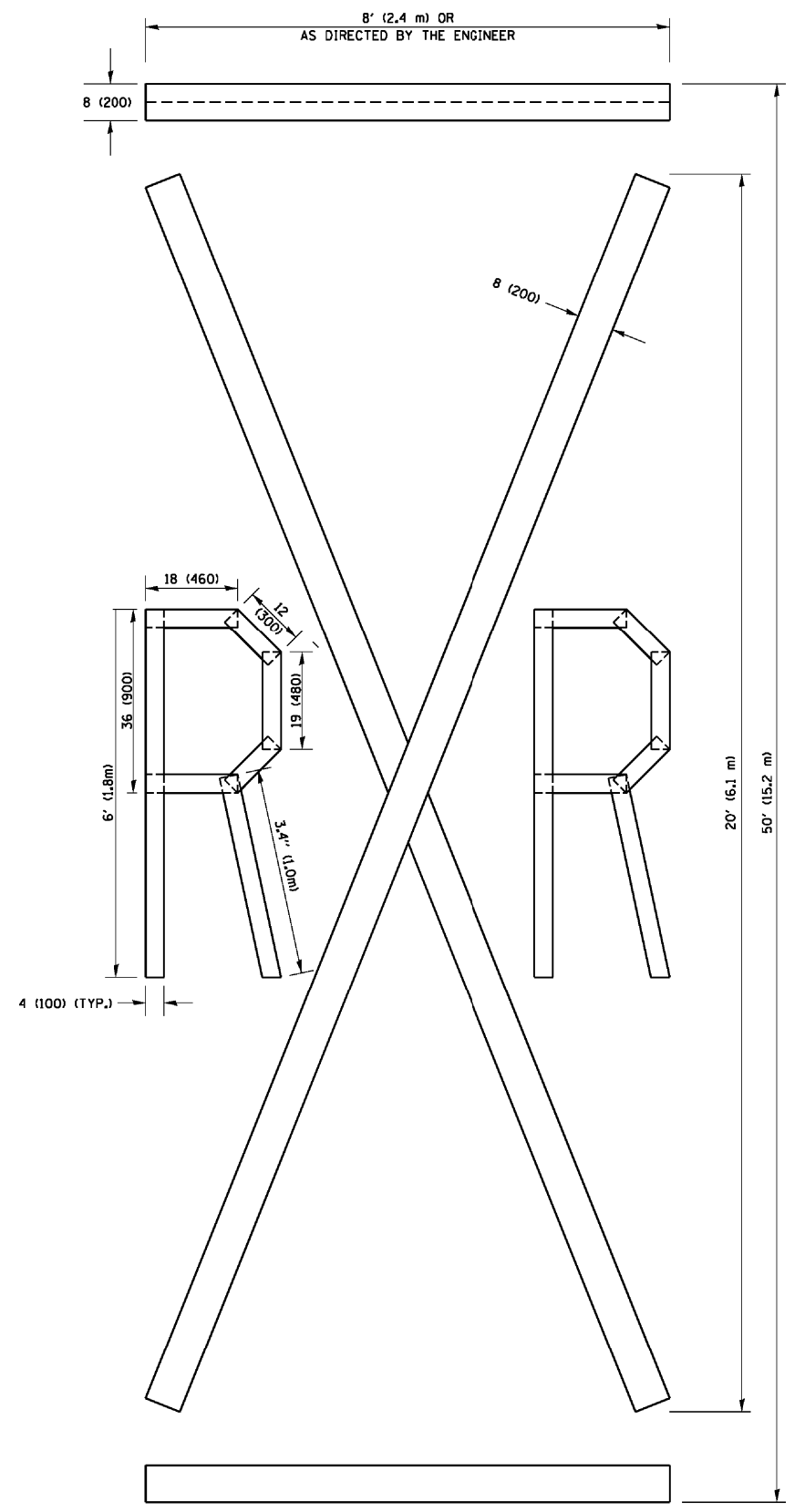


QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

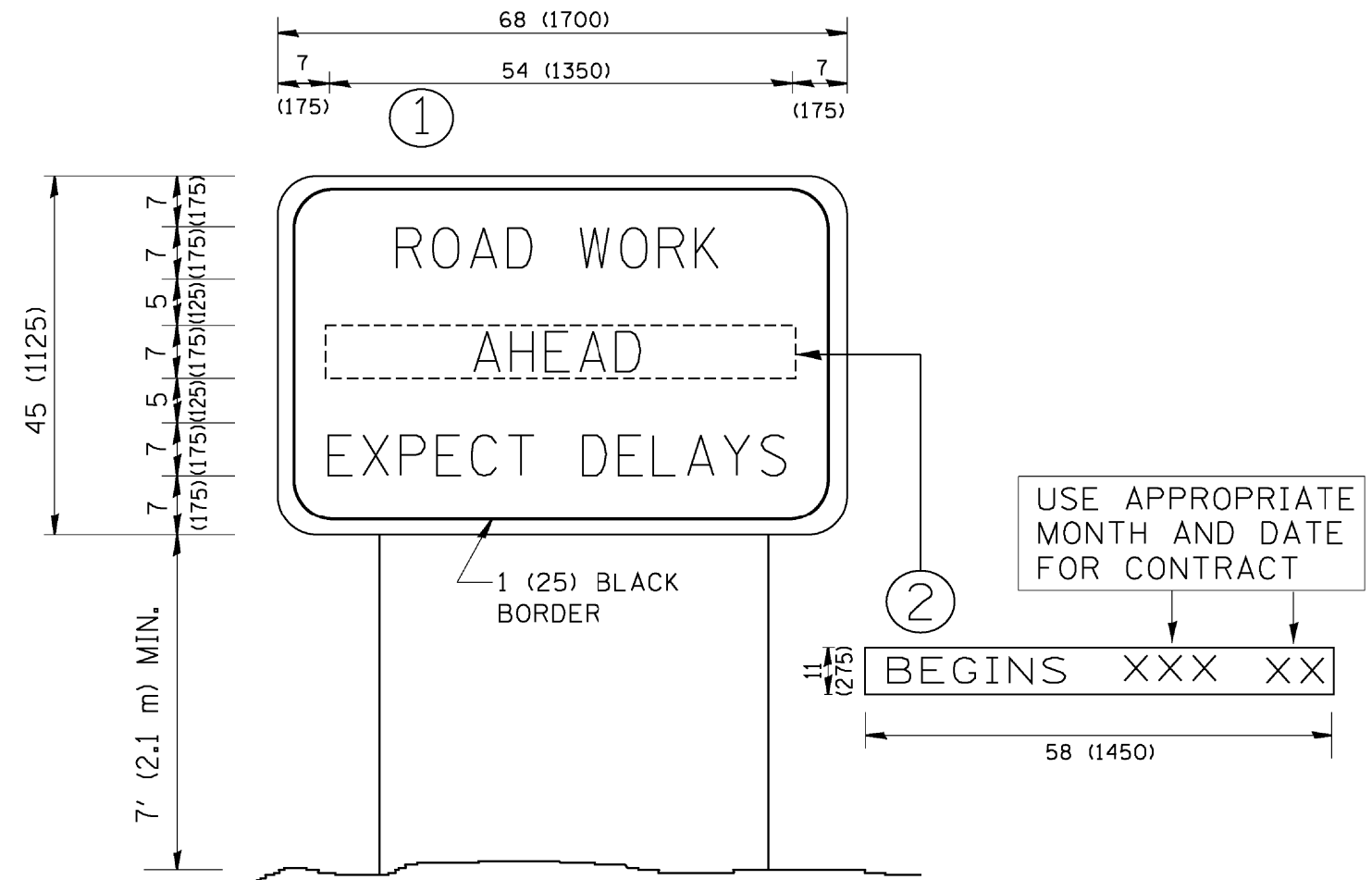
All dimensions are in inches (millimeters) unless otherwise shown.

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PLOT SCALE = 50.0000' / in.		DATE -	REVISED - E. GOMEZ 08-28-00
PLOT DATE = 9/15/2016			REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1375	18-00065-00-RS	DUPAGE	29	23
TC-16			CONTRACT NO. 61F30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

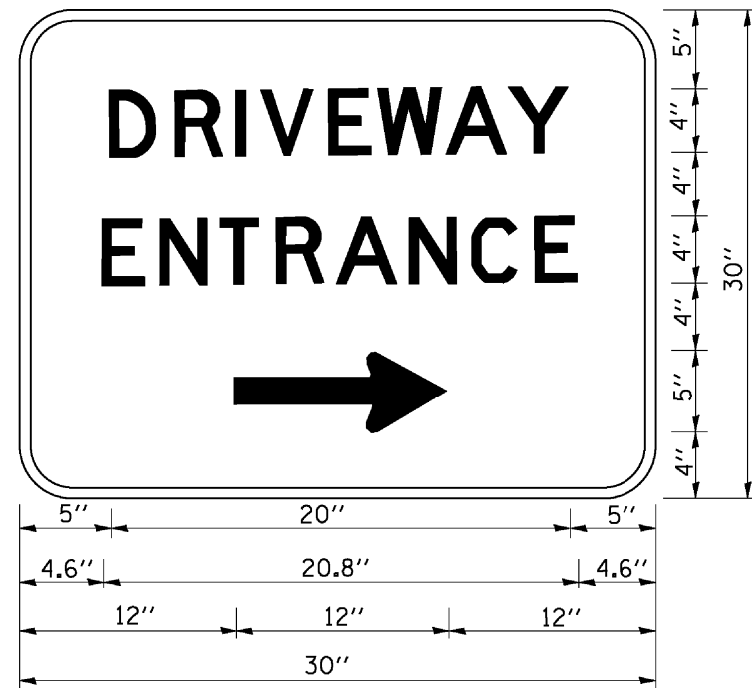
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		REVISOR - CHECKED -	REVISOR - CHECKED -
	PLOT SCALE = 50.000' / IN. PLOT DATE = 1/4/2008	DATE -	REVISOR - CHECKED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1375	18-00065-00-RS	DUPAGE	29	24
TC-22			CONTRACT NO. 61F30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME = W:\diststd\22x34\tc26.dgn	USER NAME = gegl1aenobt	DESIGNED -	REVISED - C. JUCIUS 02-15-07
		DRAWN -	REVISED -
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 1/4/2008	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DRIVEWAY ENTRANCE SIGNING

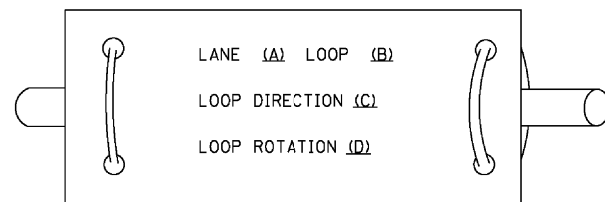
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-26			CONTRACT NO. 61F30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

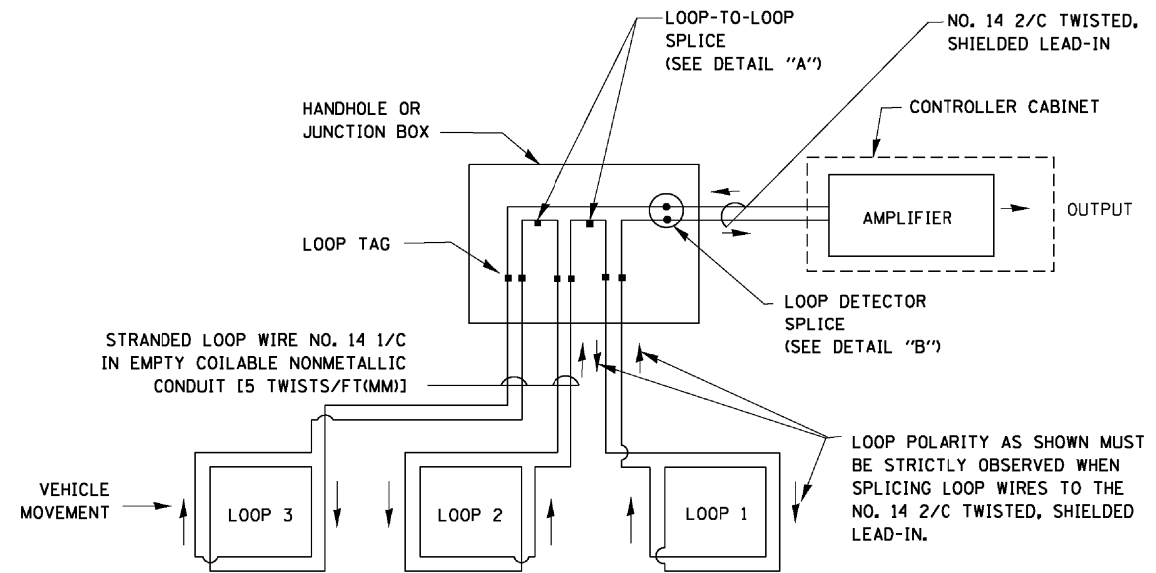
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

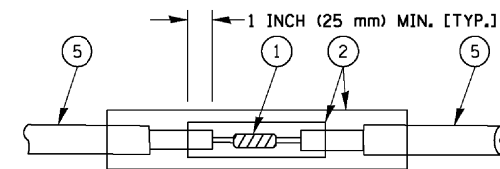


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

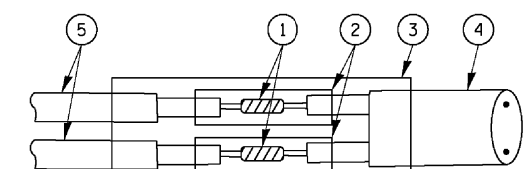


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

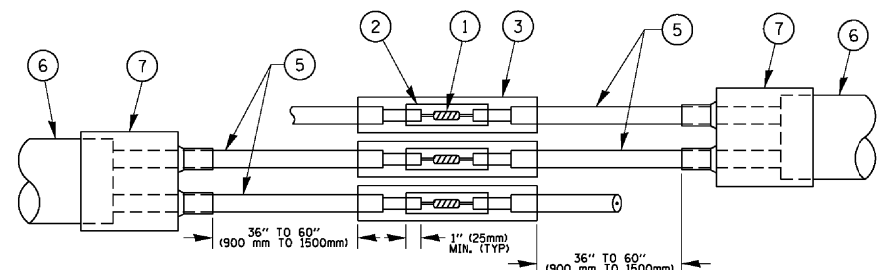


DETAIL "A"
LOOP-TO-LOOP SPLICE

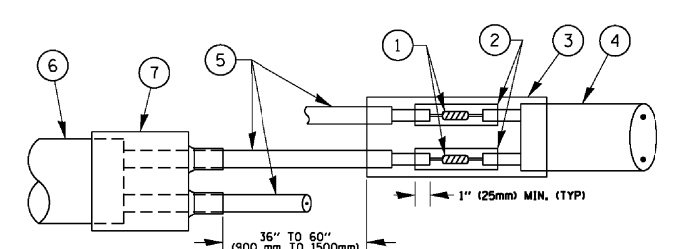


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

PREFORMED LOOP

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PREFORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

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PLOT DATE = 1/13/2014		DATE - 10-28-09	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

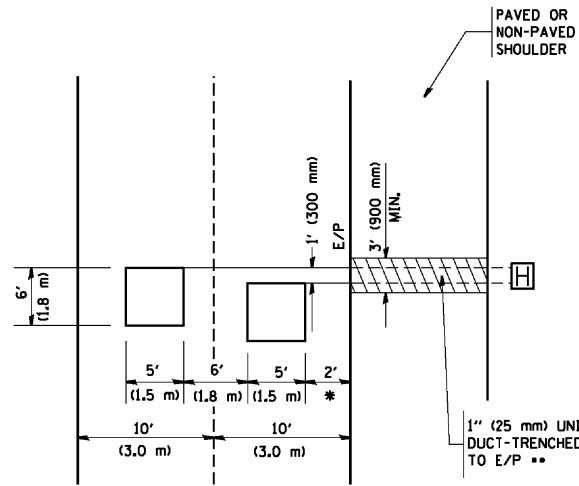
**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET NO. 1 of 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1375	18-00065-00-RS	DUPAGE	29	26
TS-05		CONTRACT NO. 61F30		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



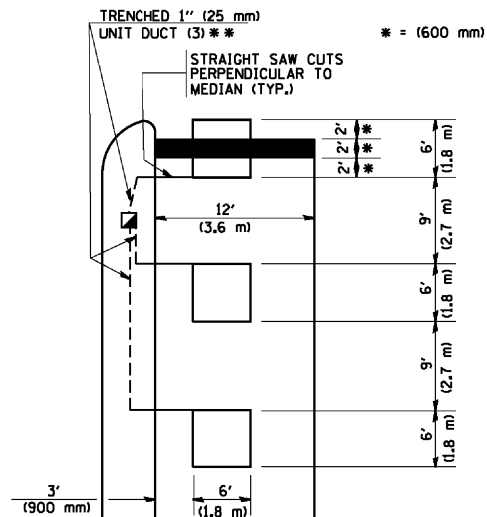
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

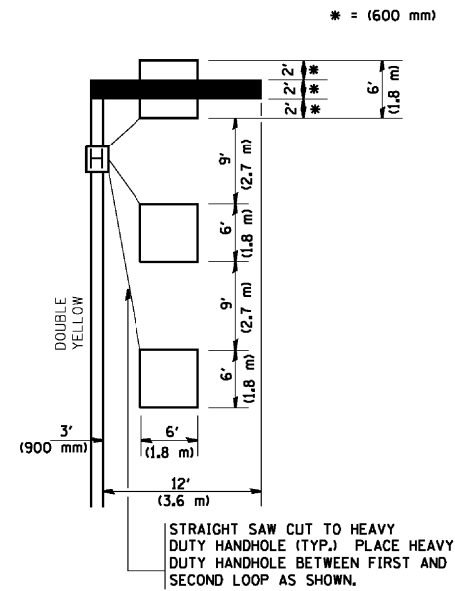


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)



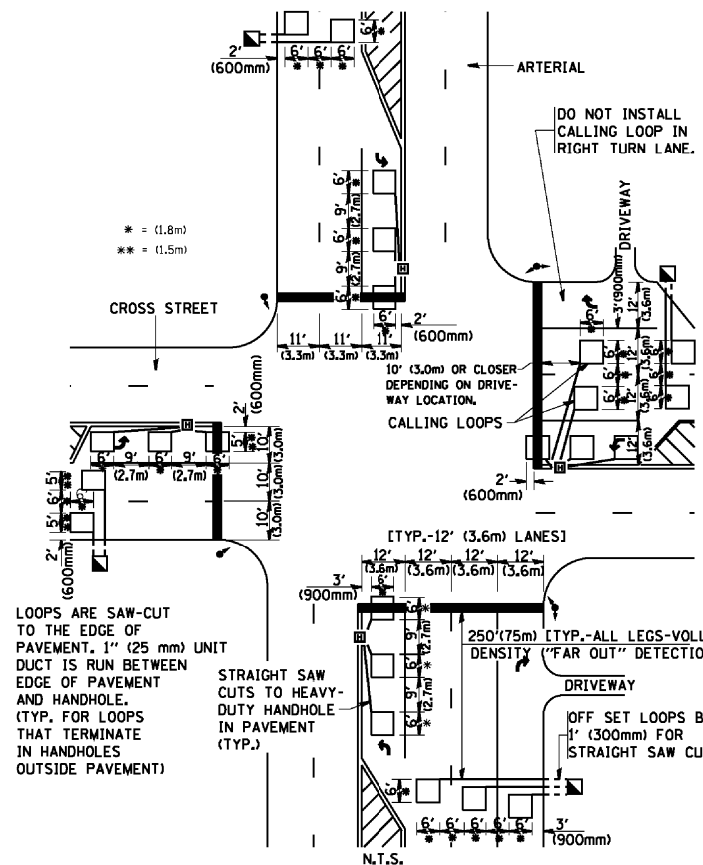
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

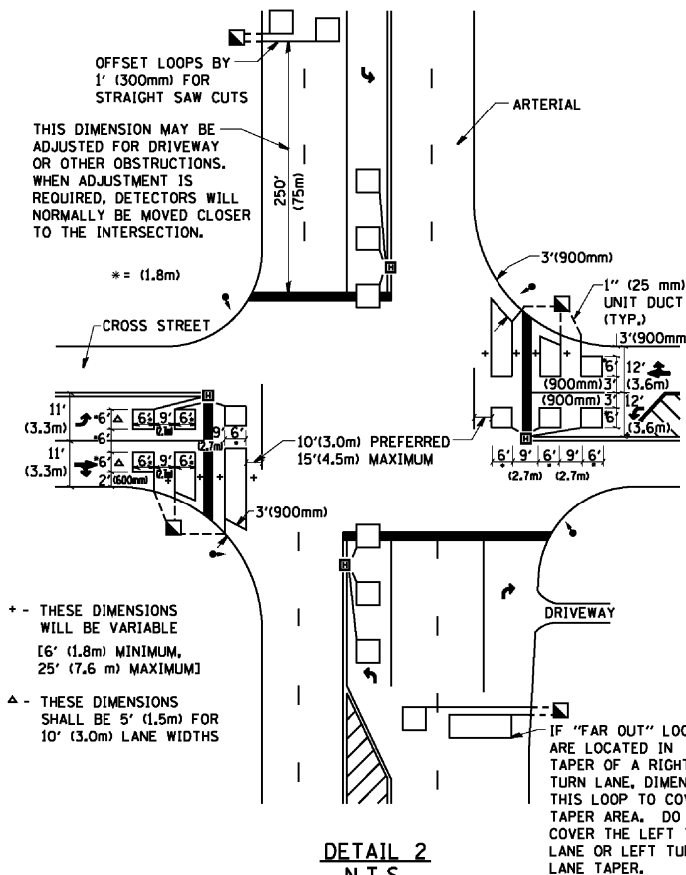
- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2
N.T.S.**

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

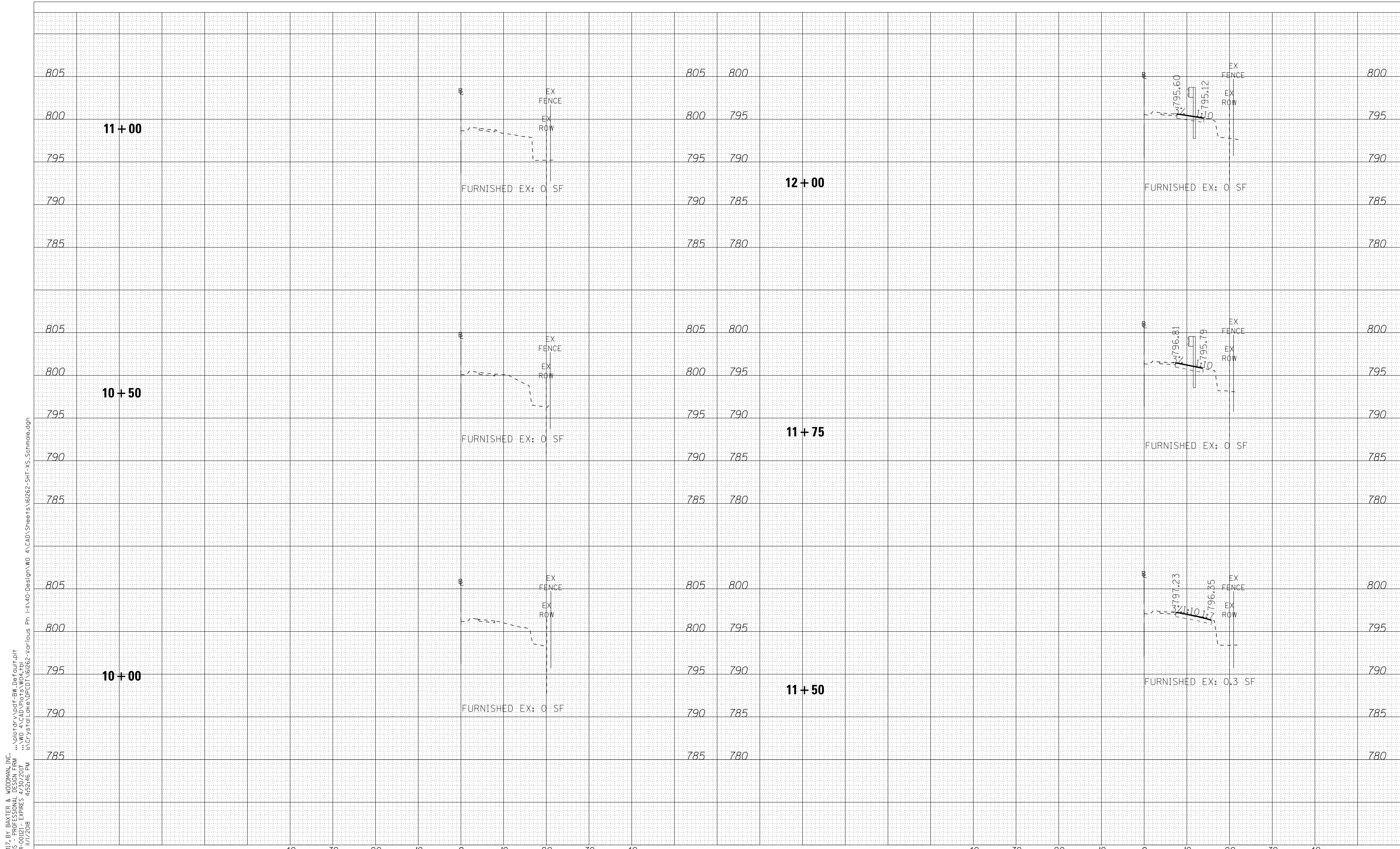
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = W:\distad\22x34\ts07.dgn	USER NAME = geglanoht	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
	PLOT SCALE = 50.0000' / IN.	CHECKED - R.K.F.	REVISED -			1375	18-00065-00-RS	DUPAGE	29	27	
	PLOT DATE = 1/4/2008	DATE -	REVISED -			TS-07		CONTRACT NO. 61F30			
						SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.	



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DESIGNED -	AMM	REVISED -	
DRAWN -	CJC	REVISED -	
CHECKED -	DTH	REVISED -	
DATE -	11/01/18	FILE -	161262-SHT-XS_Schmale.dgn

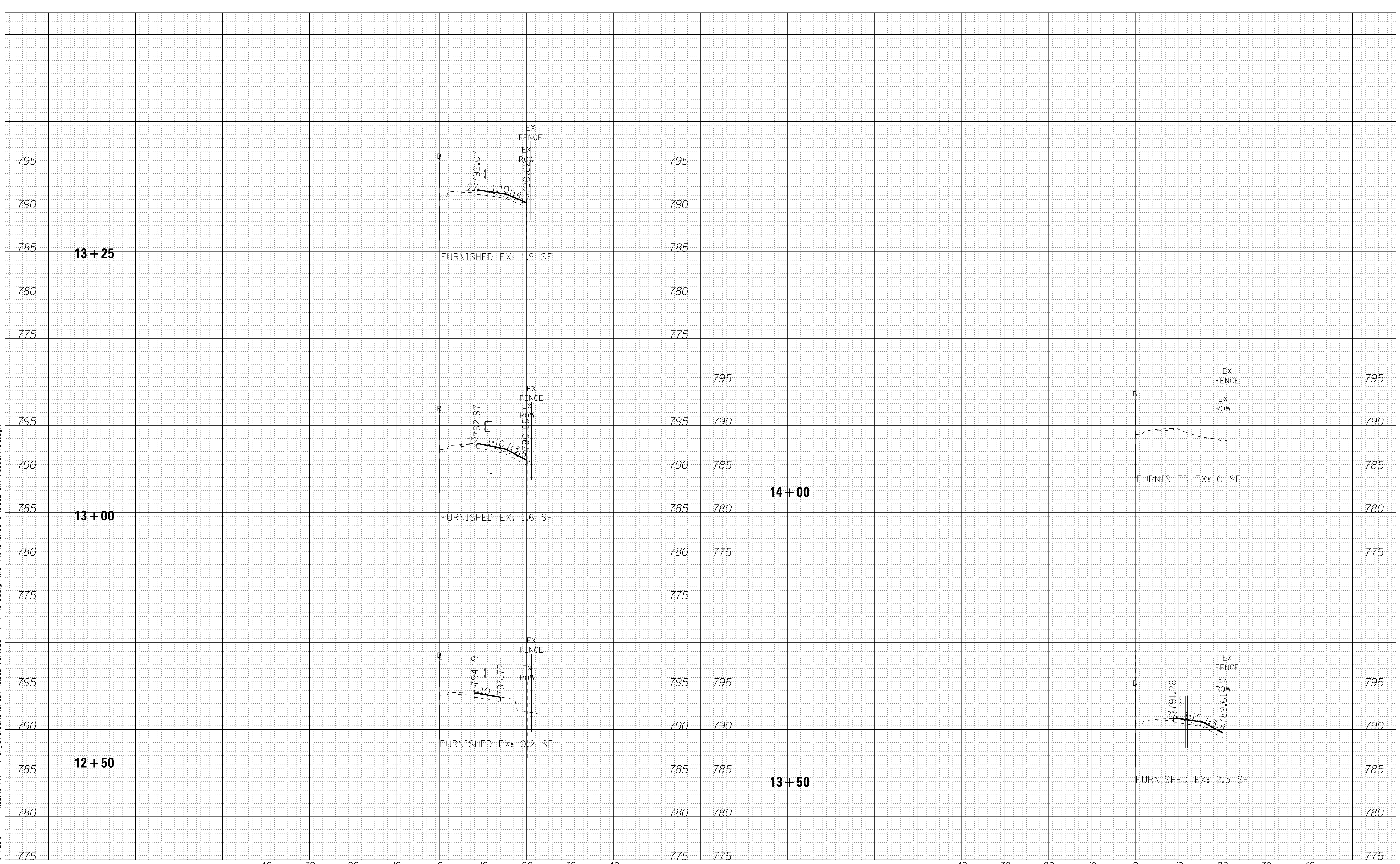
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**LIES ROAD RESURFACING
CROSS SECTIONS**

SCALE: H: 1"=10' V: 1"=5' STA. 10+00 TO STA. 12+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1375	18-00065-00-RS	DUPAGE	29	28
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 61F30	

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DESIGNED -	AMM	REVISED -	
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

LIES ROAD RESURFACING
CROSS SECTIONS

SCALE: H: 1"=10' V: 1"=5'

STA. 12+50 TO STA. 14+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1375	18-00065-00-RS	DUPAGE	29	29
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 61F30	