

SN 041-0077  
ORIGINAL CONST.

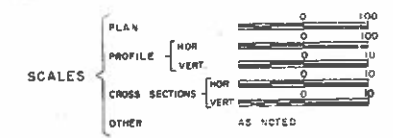
STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS AND BUILDINGS  
DIVISION OF HIGHWAYS  
PLANS FOR PROPOSED  
FEDERAL AID INTERSTATE HIGHWAY

PROJECT NO.	SEC.	COUNTY	SHEET	TOTAL
F.A.I. 1-64-3(1)262	41-6HVB-1	JEFFERSON	28	1

INDEX OF SHEETS

1 TITLE SHEET  
2 TYPICAL SECTION FAI-64  
3 SUMMARY OF QUANTITIES, SCHEDULES AND SIGNATURES & GENERAL NOTES  
4-5 PLAN AND PROFILE: FAI-64 STA. 2005+00 TO STA. 2065+00  
6-25 BRIDGE PLANS  
26-28 CROSS SECTIONS FAI-64 STA. 2028+00 TO STA. 2038+00

STANDARDS REQUIRED  
2298-2, 2799-1, 2300, 2302-1, 2303, 2, 2307-2  
1608-3, 2113-1, 2153-8, 2051, 2198-8, 2230-5,  
2231-5



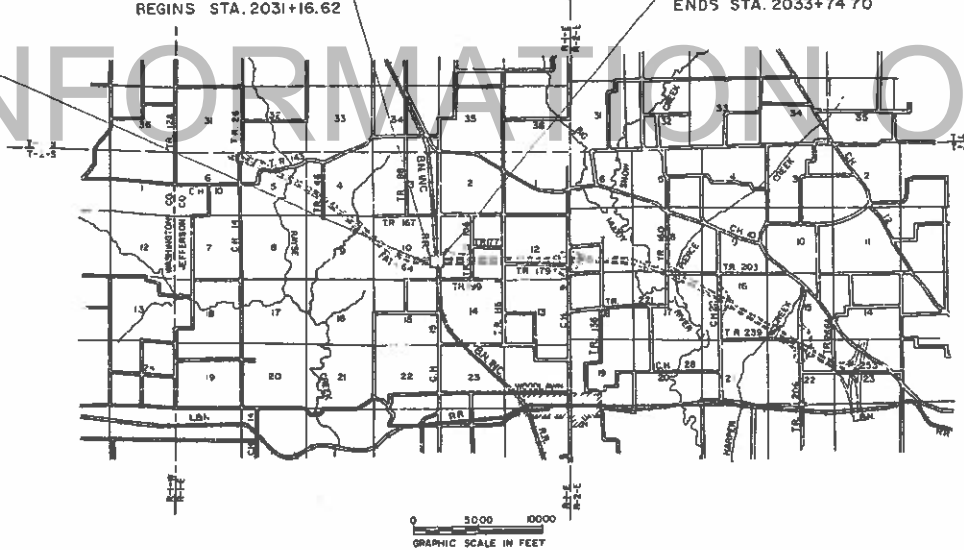
FAI ROUTE 64  
SECTION 41-6HVB-1  
PROJECT I-IG-64-3 (1)262  
JEFFERSON COUNTY



FAI ROUTE 64 SECTION 41-6HVB-1  
PROJECT I-IG-64-3(1)262  
INCLUDES THE CONSTRUCTION OF  
DUAL 4-SPAN CONTINUOUS WF BEAM  
STRUCTURES CARRYING FAI ROUTE 64  
OVER THE B.N. INC. R.R. AND OVER CH-15  
(2 SPANS @ 48'-0" 2 SPANS @ 78'-0" V2')

PROJECT I-IG-64-3(1)262  
SECTION 41-6HVB-1  
REGINS STA. 2031+16.62

PROJ. I-IG-64-3(1)262  
SECTION 41-6HVB-1  
ENDS STA. 2033+74.70



DESIGN DESIGNATION  
I-64 MAINLINE 1830(90) B-1 77 (PCC-20)

NET LENGTH OF PROJECT = 258.08 FT. = 0.049 MILES

ROAD PLANS PREPARED BY  
WESTENHOFF AND N'WICK, INC.  
CONSULTING ENGINEERS  
CHICAGO, ILLINOIS

SUBMITTED *Edward S. Newman*  
ILLINOIS P.E. # 4417

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS AND BUILDINGS  
DIVISION OF HIGHWAYS

APPROVED  
DATE

APPROVED  
DATE

DEPARTMENT OF TRANSPORTATION  
FEDERAL HIGHWAY ADMINISTRATION

APPROVED  
DATE

JEFFERSON COUNTY SECTION 41-6HVB-1 F.A. ROUTE 64



ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 64	41-6HVB-1	JEFFERSON	24	3
FED ROAD DIST NO 2 ILLINOIS PROJ				

### SUMMARY OF QUANTITIES

FAI ROUTE 64  
SECTION 41-6 HVB-1  
JEFFERSON COUNTY

IG FUNDS = 59.939 %  
I FUNDS = 40.061 %

CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY	
			CONSTRUCTION TYPE CODE Y 571	CE 58
201001	TREE REMOVAL (6 TO 15 INCH DIAMETER)	IN DIA	20	
202001	EARTH EXCAVATION	CU YD	1,502	
204001	BORROW EXCAVATION	CU YD	104,099	
502001	CLASS A EXCAVATION FOR STRUCTURES	CU YD	630	
503002	CLASS X CONCRETE HEADWALLS	CU YD	4.5	
503004	PROTECTIVE COAT	SQ YD	2,710	
504003	CLASS X CONCRETE	CU YD	1,262.6	
507030	FURNISHING AND ERECTING STRUCTURAL STEEL	L SUM	1	
507025	STUD SHEAR CONNECTORS	EACH	3,744	
508005	ALUMINUM RAILING	LIN FT	1,020	
511131	PIPE CULVERTS, TYPE 2 RCCP 36"	LIN FT	224	
512001	REINFORCEMENT BARS	POUND	237,950	
513005	FURNISHING CRESOATED PILES 20 1/2 TO 38 FEET	LIN FT	552	
513012	FURNISHING STEEL PILES 8BP36	LIN FT	4,853	
513022	DRIVING TIMBER PILES	LIN FT	552	
513026	DRIVING STEEL PILES	LIN FT	4,853	
513032	TEST PILE STEEL 8BP36	EACH	1	
514001	NAME PLATES	EACH	2	
618001	SLOPE WALL 4 INCH	SQ YD	2,267	
646001	ENGINEER'S FIELD OFFICE, TYPE A	EACH	1	1
210029	BRIDGE SEAT SEALANT	L SUM	1	
X21100	TRAINING	HOURS	1000	

### SCHEDULE OF CLASS X CONCRETE

LOCATION	CLASS X CONCRETE		REINFORCEMENT
	BRIDGE CU. YDS.	HDWL. CU. YDS.	BAR# LBS.
STA 2233+30 TO RT		4.5	130
BRIDGE STRUCTURES	1,262.6		237,950
TOTAL	1,262.6	4.5	237,950

### SCHEDULE OF TREE REMOVAL QUANTITIES

STATION	OFFSET	6-15 IN. DIA.
2031+50	27' LT	10
2031+30	9' RT	10
TOTAL		20

SIGNATURES  
FAI ROUTE 64, SEC. 41-6HVB-1  
JEFFERSON COUNTY

EXAMINED April 14 1970  
EXAMINED April 21 1970  
EXAMINED April 19 1970  
EXAMINED A 1970  
EXAMINED April 20 1970  
EXAMINED April 15 1970  
EXAMINED April 24 1970

H. O. Coe  
DISTRICT ENGINEER OF DESIGN  
T. H. W. [unclear]  
DISTRICT ENGINEER OF CONSTRUCTION  
[unclear]  
DISTRICT ENGINEER OF MAINTENANCE  
[unclear]  
DISTRICT ENGINEER OF PLANNING  
E. R. [unclear]  
DISTRICT ENGINEER OF TRAFFIC  
John D. [unclear]  
DISTRICT ENGINEER OF RIGHT OF WAY  
[unclear]  
DISTRICT ENGINEER

#### GENERAL NOTES

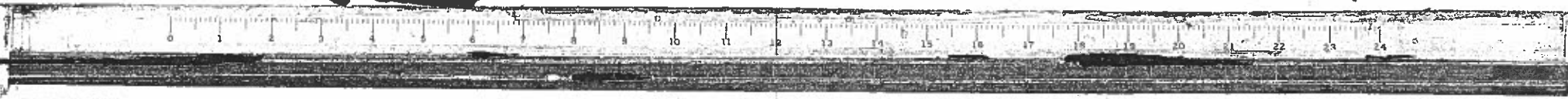
THIS SECTION SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE PLANS STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, ADOPTED AUGUST 1, 1968, THE MANUAL OF GREEN TRAFFIC CONTROL, ADOPTED NOVEMBER 1964, AND THE SPECIAL PROVISIONS.  
THE WORK ENCLOSED IN SECTION 41-6HVB-1 CONSISTS OF THE FABRICATION, FURNISHING AND COMPLETE CONSTRUCTION OF DUAL 4-SPAN CONTINUOUS W/ BEAM BRIDGES (2-SPANS AT 48'-8" AND 2-SPANS AT 78'-0") CROSSING P.A.L. ROUTE 64 OVER THE B.H. INC. R.R. AND C.H. 15. THE CONTRACTOR OF BRIDGE ABUTMENT COSES AND OTHER WORK NECESSARY TO COMPLETE THE SECTION.  
SOIL EXCAVATION NECESSARY FOR THE CONSTRUCTION OF THIS SECTION SHALL BE FURNISHED BY THE CONTRACTOR FOR THIS SECTION FROM LOCATIONS APPROVED BY THE ENGINEER.  
WHERE SECTION OR SUB-SECTION MARKING ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MARKINGS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKS AND MONUMENTS UNTIL THE OWNER, AN AUTHORIZED SURVEYOR, OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION.  
PROTECTIVE CURB SHALL BE APPLIED TO ALL PARAPETS, BELIEGE DECKS, CURBS, PARAPET WALLS, AND CONCRETE MEDIANS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.  
ONE (1) 6" DIA. PILE TEST SHALL BE ERECTED AT THE LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.  
THE CONTRACTOR FOR THIS SECTION SHALL CONSTRUCT THE DRAINAGE TO THE GRADEING LINE DRAW ON THE X-SECTION.

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

SUMMARY OF QUANTITIES,  
CLASS X CONCRETE AND  
TREE REMOVAL SCHEDULES,  
AND SIGNATURES

WESTENHOFF AND NOVICK, INC.  
CONSULTING ENGINEERS

Rev. 12-18-70



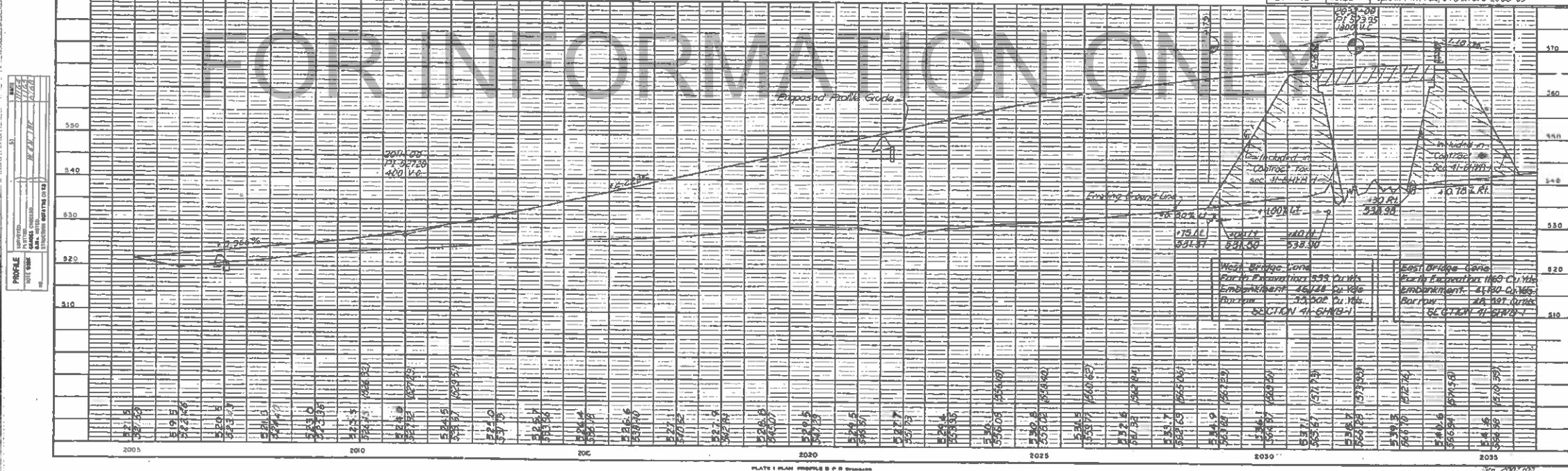
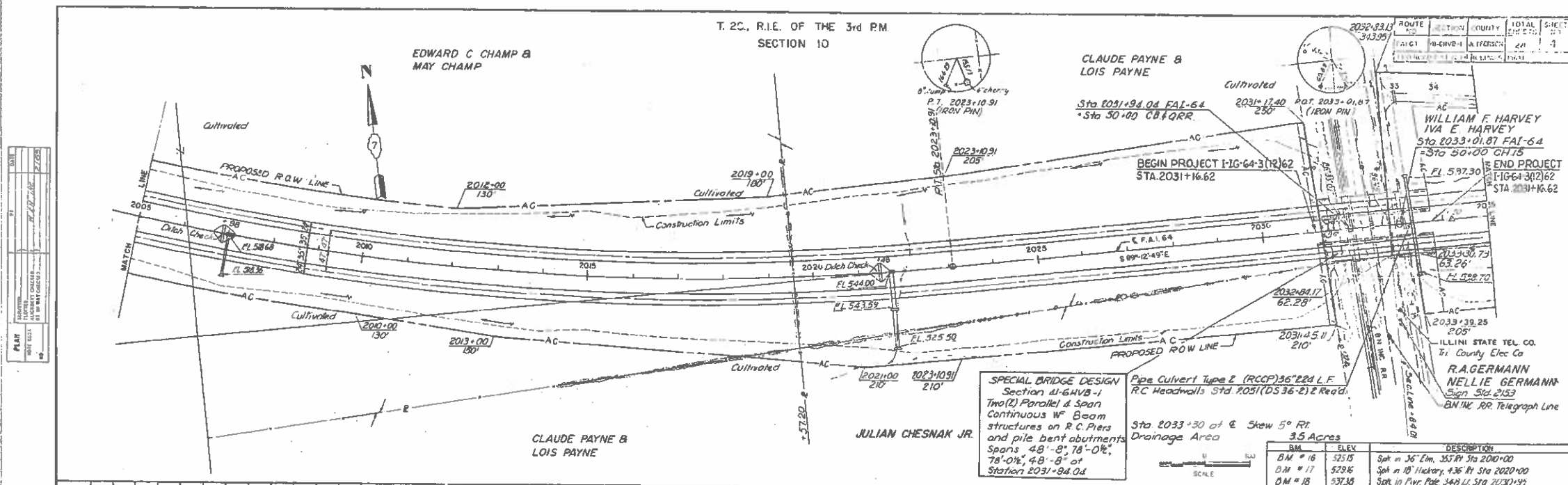
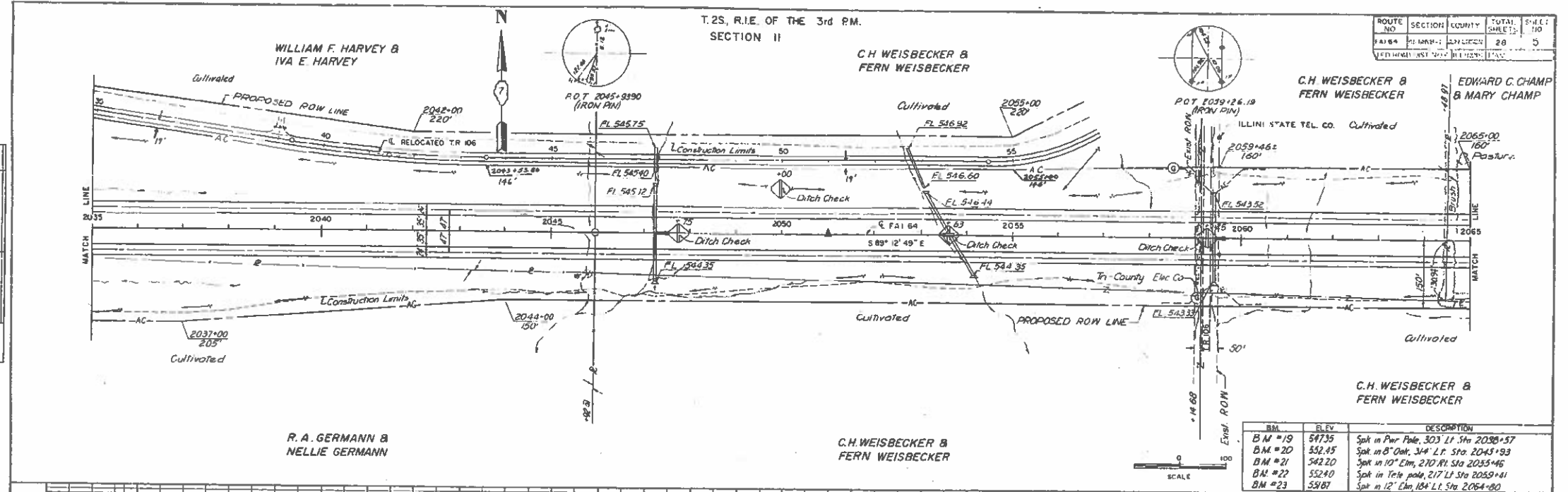


PLATE 1 PLAN PROFILE & P & R DRAWING  
DATE SETTING IN. 05/20/00

DATE	2/27/00
BY	J.P.K.
CHECKED	J.P.K.
APPROVED	J.P.K.
PROJECT	RELOCATED TR 106
SECTION	SECTION II

DATE	2/27/00
BY	J.P.K.
CHECKED	J.P.K.
APPROVED	J.P.K.
PROJECT	RELOCATED TR 106
SECTION	SECTION II



ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAH 84	SECTION II	ILLINOIS	28	5

B.M.	ELEV.	DESCRIPTION
B.M. #19	547.39	Spk in Pine Pole, 303' L.F. Sta. 2038+57
B.M. #20	552.45	Spk in 8" Oak, 34' L.F. Sta. 2045+93
B.M. #21	542.20	Spk in 10" Elm, 210' L.F. Sta. 2055+46
B.M. #22	552.40	Spk in Telephone, 217' L.F. Sta. 2059+41
B.M. #23	551.87	Spk in 12" Elm, 184' L.F. Sta. 2064+80

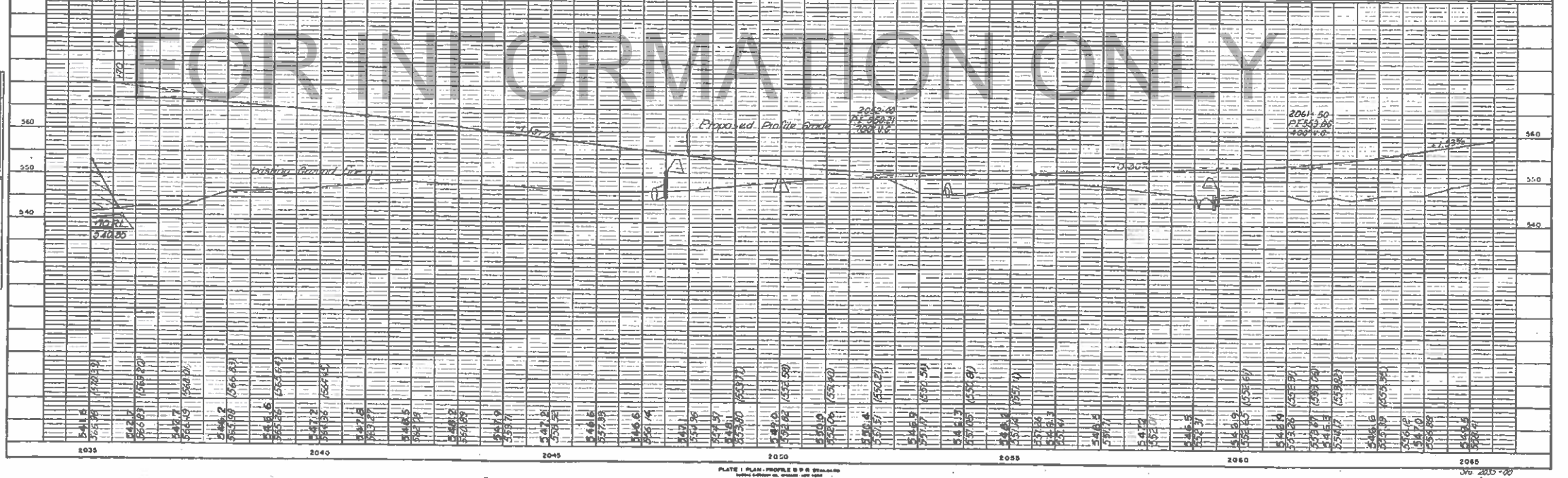


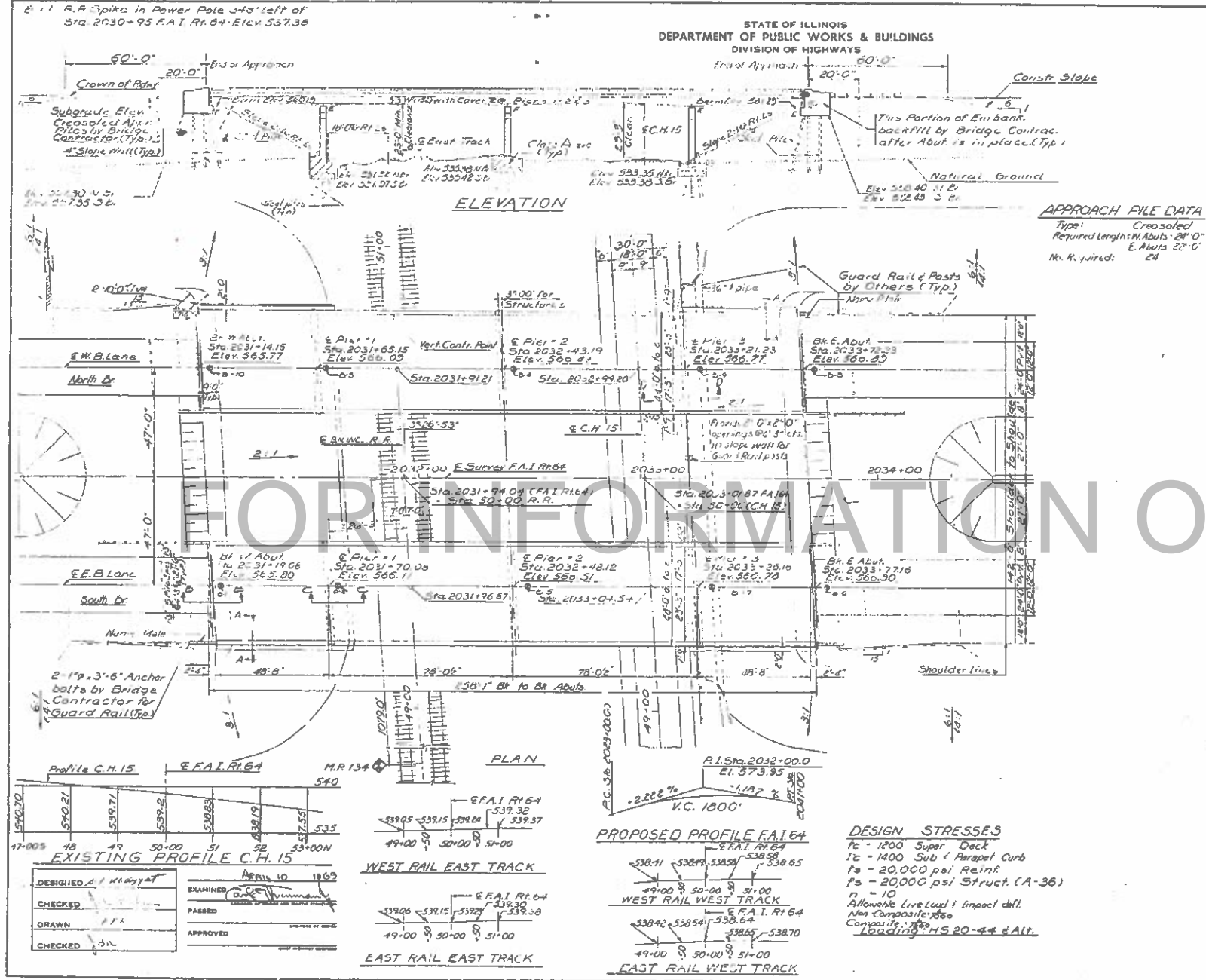
PLATE I PLAN - PROFILE OF ROAD

Jan 2005-00

FOR INFORMATION ONLY

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

PROJECT NO.	21	DATE	28	SHEET NO.	6
BY	JEFFERSON	BY		DATE	
CHECKED		DATE		BY	



**GENERAL NOTES**

All reinforcement bars shall be topped 24 diameters unless otherwise shown.

Calculated weight of structural steel = 379410 Lbs.

Field welding of construction accessories will not be permitted to the bottom flange of beams or girders nor to the top flange for a distance equal to one-fourth the span length each way from the pier supports. Field welding in other areas will be permitted only when approved by the Engineer.

Anchor bolts shall be set before fastening diaphragms over supports.

Slope walls shall be reinforced with welded wire fabric 6" x 6" mesh, weighing 58# per 100 sq ft.

The embankment configuration shown shall be the minimum embankment that must be constructed prior to construction of the abutments.

The concrete rail section above the roadway construction part of the top of the slab shall be constructed of Class X Concrete, except the aggregates shall conform to the requirements of Standard Concrete.

The Contractor shall drive A Steel test piles in a permanent location at the W of FAI R164 Bridge, East of East South's Bridge. Test piles shall be 12" dia. x 18' long bridge as directed by the Engineer before commencing the remainder of pile.

The basic Lead Silica Chromate 1 pint gal. shall be used for shop and field painting of structural steel.

Fasteners shall be high strength bolts. Bolts 3/4" & over shall be 3/4" & unless otherwise noted.

Diaphragm connections may be accepted to shop welding subject to approval by the Engineer.

Class A Excavation for structures includes excavation for slope wall.

**APPROACH PILE DATA**

Type: Cressed

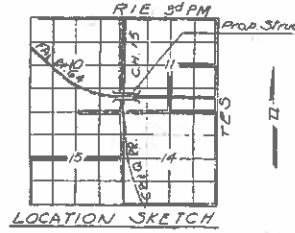
Required Length: W. Abut. 28' 0"

E. Abut. 22' 0"

No. R. joined: 24

STATION 2031+94.04  
BUILT 1970 BY  
STATE OF ILLINOIS  
FAI R164 SEC 41-6HVB  
FA. PROJ. I-16-64-3(12)  
LOADING H520HLT.

Name Photo  
(See S161 2/15-7)

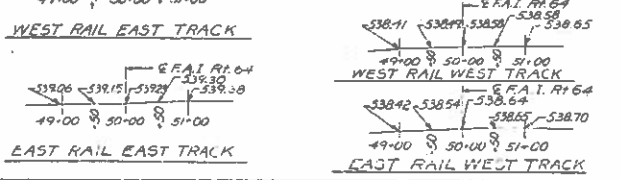


GENERAL PLAN & ELEVATION  
PROJECT: I-16-64-3(12)162  
FAI R164 over BN INC. R.R. & C.H. 15.  
FAI R164 - SECTION 41-6HVB-1  
JEFFERSON CO.  
STA. 2031 + 94.04

**DESIGN STRESSES**

$f_c$  - 1200 Super Deck  
 $f_c$  - 1400 Sub & Parapet Curb  
 $f_s$  - 20,000 psi Reinf.  
 $f_s$  - 20,000 psi Struct. (A-36)  
 $n$  - 10  
Allowable Live Load & Impact defl.  
Non Composite Deck  
Composite Deck  
LOADING H520-44 & ALT.

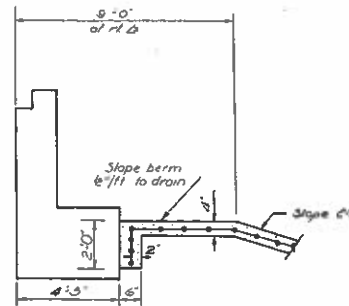
DESIGNED BY	W. J. WAGYAT	EXAMINED	APRIL 10 1969
CHECKED		PASSED	
DRAWN	P. T. T.	APPROVED	
CHECKED			



FOR INFORMATION ONLY

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

PROJECT NO.	SECTION	SHEET NO.	TOTAL SHEETS
41-6HYB-1	JEFFERSON 2B	7	20



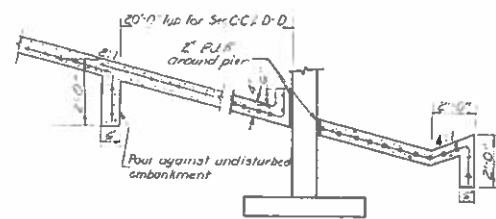
SECTION B-B

FOR INFORMATION ONLY

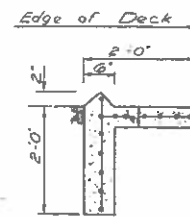
TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Class A Excav for Struct	Cu yds		630	630
Class 2 Concrete	Cu yds	677.4	555.3	1232.7
Protective Coat	Sq yds			2710
Structural Steel	L.S.			1
Aluminum Siding	Lin. Ft.	1020		1020
Reinforcement Bars	Lbs	165330	65470	230800
Creosoted Piles (4" to 36")	Lin. Ft.			550
Steel Piles (ODP36)	Lin. Ft.		1853	1853
Steel Piles Steel (ODP36)	Lbs		4	4
Alumg Piles	Lbs		2	2
Slope Wall (4")	Sq yds			2267
Bridge Seal Sealant	L.S.		1	1
Stud Shear Connectors	Ea.	3744		3744

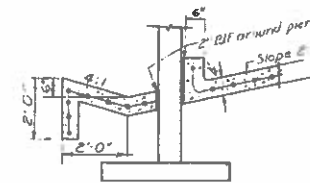
\* At Abutments only.



SECTION C-C



SECTION A-A



SECTION D-D

DESIGNED <i>A. H. [Signature]</i>	EXAMINED <i>[Signature]</i>
CHECKED <i>[Signature]</i>	APPROVED <i>[Signature]</i>
DRAWN <i>[Signature]</i>	
CHECKED <i>[Signature]</i>	

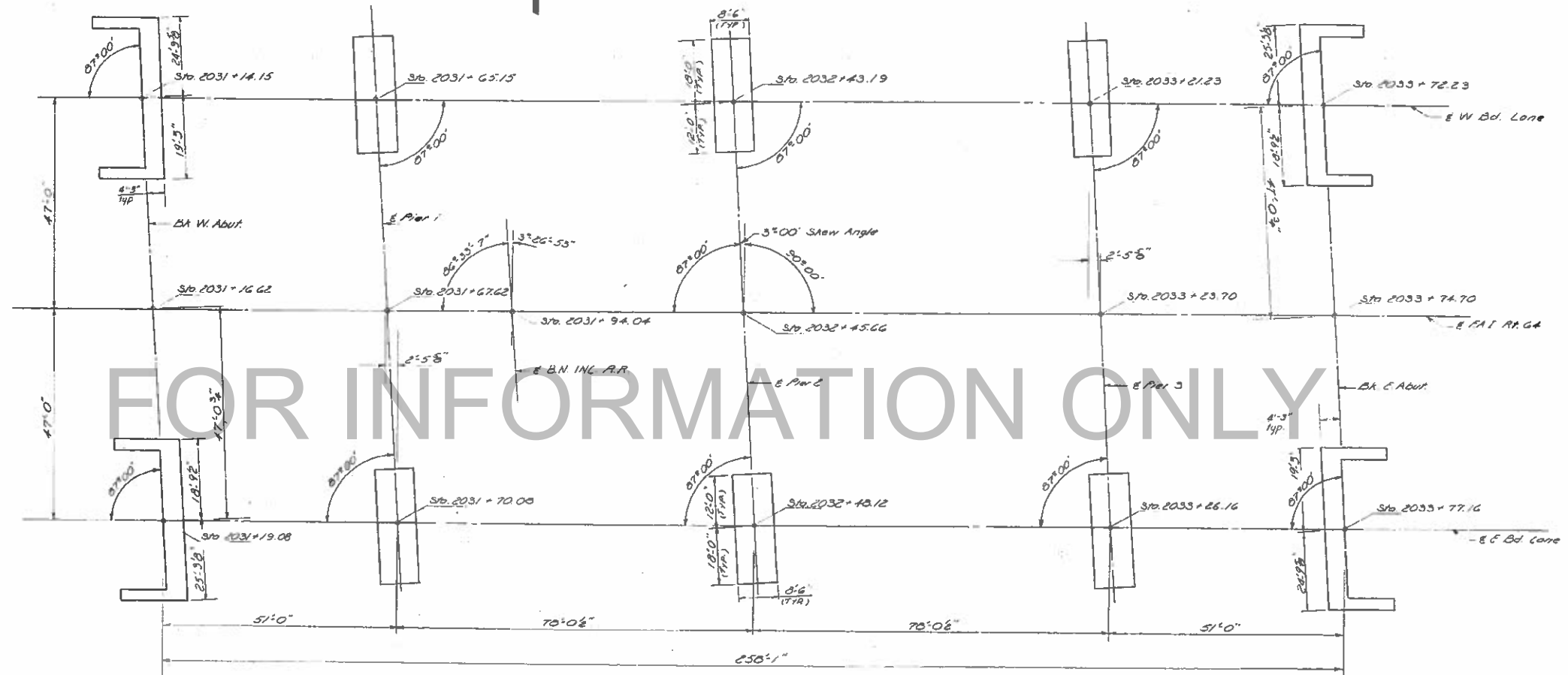
GENERAL PLAN & ELEVATION  
DETAILS

PROJECT: I-IG 6A-3(12)62  
F.A.I.R.164 over B.N. INC. R.R. & C.H.15  
F.A.I.R.164 SECTION 41-6HYB-1

JEFFERSON CO.  
STA. 2031 + 94.04

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

DATE	BY	QUANTITY	NO. SHEETS	SHEET NO.
4-10-69	JEFFERSON	28	8	3
PROJECT		SHEET NO.		
JEFFERSON		28		



FOR INFORMATION ONLY

PLAN

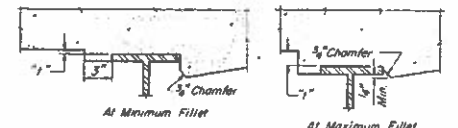
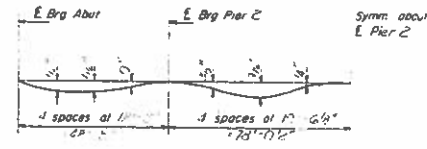
DESIGNED	A. J. H. H. H. H. H.	DATE	APRIL 10 1969
CHECKED	B. H. H. H. H.	EXAMINED	[Signature]
DRAWN	C. H. H. H. H.	PASSED	[Signature]
CHECKED	D. H. H. H. H.	APPROVED	[Signature]

FOOTING LAYOUT  
P.A.I. R.R.G.A. - SEC. 41 - G.H.V.B.-1  
JEFFERSON COUNTY  
STA. 2031+94.04



STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

JEFFERSON 28 5  
SHEETS



Girder 1

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk W Abut	203114.988	15.958	565.502	565.502
E Brg West Abut	203117.320	15.958	565.511	565.511
A	203122.320	15.958	565.568	565.568
B	203127.320	15.958	565.625	565.625
C	203132.320	15.958	565.682	565.682
D	203137.320	15.958	565.739	565.739
E Pier 1	203145.950	15.958	565.823	565.823
E	203175.980	15.958	565.880	565.880
F	203185.980	15.958	565.937	565.937
G	203195.980	15.958	565.994	565.994
H	203205.980	15.958	566.051	566.051
I	203215.980	15.958	566.108	566.108
J	203225.980	15.958	566.165	566.165
K	203235.980	15.958	566.222	566.222
E Pier 2	203243.620	15.958	566.279	566.279
L	203253.620	15.958	566.336	566.336
M	203263.620	15.958	566.393	566.393
N	203273.620	15.958	566.450	566.450
O	203283.620	15.958	566.507	566.507
P	203293.620	15.958	566.564	566.564
Q	203303.620	15.958	566.621	566.621
R	203313.620	15.958	566.678	566.678
E Pier 3	203321.270	15.958	566.735	566.735
S	203331.270	15.958	566.792	566.792
T	203341.270	15.958	566.849	566.849
U	203351.270	15.958	566.906	566.906
V	203361.270	15.958	566.963	566.963
E Brg East Abut	203370.734	15.958	566.972	566.972
Bk East Abut	203372.070	15.958	566.972	566.972

Girder 2

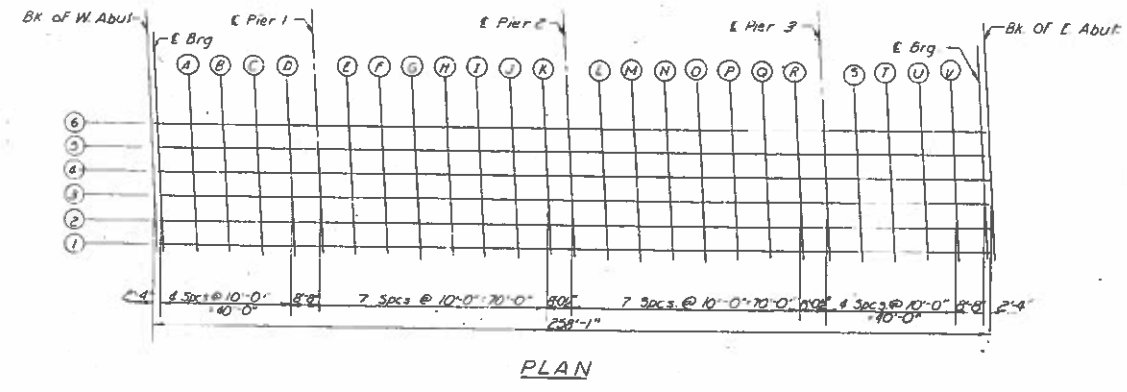
Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk W Abut	203114.988	15.958	565.502	565.502
E Brg West Abut	203117.320	15.958	565.511	565.511
A	203122.320	15.958	565.568	565.568
B	203127.320	15.958	565.625	565.625
C	203132.320	15.958	565.682	565.682
D	203137.320	15.958	565.739	565.739
E Pier 1	203145.950	15.958	565.823	565.823
E	203175.980	15.958	565.880	565.880
F	203185.980	15.958	565.937	565.937
G	203195.980	15.958	565.994	565.994
H	203205.980	15.958	566.051	566.051
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J	203225.980	15.958	566.165	566.165
K	203235.980	15.958	566.222	566.222
E Pier 2	203243.620	15.958	566.279	566.279
L	203253.620	15.958	566.336	566.336
M	203263.620	15.958	566.393	566.393
N	203273.620	15.958	566.450	566.450
O	203283.620	15.958	566.507	566.507
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Q	203303.620	15.958	566.621	566.621
R	203313.620	15.958	566.678	566.678
E Pier 3	203321.270	15.958	566.735	566.735
S	203331.270	15.958	566.792	566.792
T	203341.270	15.958	566.849	566.849
U	203351.270	15.958	566.906	566.906
V	203361.270	15.958	566.963	566.963
E Brg East Abut	203370.734	15.958	566.972	566.972
Bk East Abut	203372.070	15.958	566.972	566.972

Girder 3

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk W Abut	203114.988	15.958	565.502	565.502
E Brg West Abut	203117.320	15.958	565.511	565.511
A	203122.320	15.958	565.568	565.568
B	203127.320	15.958	565.625	565.625
C	203132.320	15.958	565.682	565.682
D	203137.320	15.958	565.739	565.739
E Pier 1	203145.950	15.958	565.823	565.823
E	203175.980	15.958	565.880	565.880
F	203185.980	15.958	565.937	565.937
G	203195.980	15.958	565.994	565.994
H	203205.980	15.958	566.051	566.051
I	203215.980	15.958	566.108	566.108
J	203225.980	15.958	566.165	566.165
K	203235.980	15.958	566.222	566.222
E Pier 2	203243.620	15.958	566.279	566.279
L	203253.620	15.958	566.336	566.336
M	203263.620	15.958	566.393	566.393
N	203273.620	15.958	566.450	566.450
O	203283.620	15.958	566.507	566.507
P	203293.620	15.958	566.564	566.564
Q	203303.620	15.958	566.621	566.621
R	203313.620	15.958	566.678	566.678
E Pier 3	203321.270	15.958	566.735	566.735
S	203331.270	15.958	566.792	566.792
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U	203351.270	15.958	566.906	566.906
V	203361.270	15.958	566.963	566.963
E Brg East Abut	203370.734	15.958	566.972	566.972
Bk East Abut	203372.070	15.958	566.972	566.972

E. W. Bd. Lane

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk W Abut	203114.988	15.958	565.502	565.502
E Brg West Abut	203117.320	15.958	565.511	565.511
A	203122.320	15.958	565.568	565.568
B	203127.320	15.958	565.625	565.625
C	203132.320	15.958	565.682	565.682
D	203137.320	15.958	565.739	565.739
E Pier 1	203145.950	15.958	565.823	565.823
E	203175.980	15.958	565.880	565.880
F	203185.980	15.958	565.937	565.937
G	203195.980	15.958	565.994	565.994
H	203205.980	15.958	566.051	566.051
I	203215.980	15.958	566.108	566.108
J	203225.980	15.958	566.165	566.165
K	203235.980	15.958	566.222	566.222
E Pier 2	203243.620	15.958	566.279	566.279
L	203253.620	15.958	566.336	566.336
M	203263.620	15.958	566.393	566.393
N	203273.620	15.958	566.450	566.450
O	203283.620	15.958	566.507	566.507
P	203293.620	15.958	566.564	566.564
Q	203303.620	15.958	566.621	566.621
R	203313.620	15.958	566.678	566.678
E Pier 3	203321.270	15.958	566.735	566.735
S	203331.270	15.958	566.792	566.792
T	203341.270	15.958	566.849	566.849
U	203351.270	15.958	566.906	566.906
V	203361.270	15.958	566.963	566.963
E Brg East Abut	203370.734	15.958	566.972	566.972
Bk East Abut	203372.070	15.958	566.972	566.972



DESIGNED A. I. Maysa  
CHECKED B. H. O.  
DRAWN P. G. Barnett B.K.R.  
E-5 8-1-65

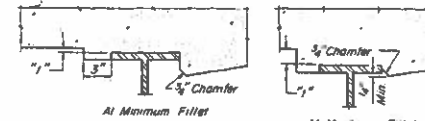
EXAMINED  
PASSED  
APPROVED

APR 10 1965

TOP OF SLAB ELEVATION  
NORTH BRIDGE  
FAIRFAX SECTION 416HVB-1  
JEFFERSON COUNTY  
STA. 2031+94.04



STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS



PROJECT NO.	416	SECTION	2B	SHEET NO.	10
DATE	4/10/65	PROJECT	JEFFERSON	SHEETS	

To determine "f" After all structural steel has been erected, elevations of the top flanges of the beams shall be taken at intervals shown below. These elevations subtracted from the Theoretical Grade Elevations Adjusted for Dead Load Deflection shown below, minus slab thickness, equals the fillet heights "f" above top flange of beams.

FILLET HEIGHTS

Girder 4

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut	203113.298	-8.792	505.052	505.052
E Brg West Abut	203118.121	-8.792	505.052	505.052
	203120.127	-8.792	505.710	505.710
	203130.127	-8.792	505.800	505.810
	203140.127	-8.792	505.807	505.810
E Pier 1	203150.127	-8.792	505.920	505.928
	203164.794	-8.792	505.980	505.980
	203174.288	-8.792	506.217	506.232
	203184.794	-8.792	506.093	506.125
E Pier 2	203194.794	-8.792	506.140	506.185
	203204.288	-8.792	506.149	506.202
	203214.794	-8.792	506.240	506.281
	203224.288	-8.792	506.400	506.415
	203234.794	-8.792	506.312	506.352
	203244.288	-8.792	506.270	506.374
	203252.436	-8.792	506.421	506.432
	203262.436	-8.792	506.401	506.405
	203272.436	-8.792	506.300	506.320
	203282.436	-8.792	506.327	506.382
E Pier 3	203292.436	-8.792	506.312	506.405
	203302.436	-8.792	506.400	506.415
	203312.436	-8.792	506.637	506.685
	203320.437	-8.792	506.661	506.681
E Brg East Abut	203330.437	-8.792	506.689	506.689
	203340.437	-8.792	506.715	506.715
	203350.437	-8.792	506.741	506.745
	203360.437	-8.792	506.741	506.745
Bk East Abut	203371.437	-8.792	506.782	506.782

Girder 5

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut	203113.297	-14.375	505.504	505.514
E Brg West Abut	203119.730	-14.375	505.590	505.590
	203125.730	-14.375	505.658	505.611
	203135.730	-14.375	505.678	505.677
	203145.730	-14.375	505.728	505.717
E Pier 1	203155.730	-14.375	505.750	505.750
	203164.787	-14.375	505.857	505.857
	203174.797	-14.375	505.904	505.919
	203184.797	-14.375	505.920	505.950
E Pier 2	203194.797	-14.375	506.019	506.032
	203204.797	-14.375	506.040	506.110
	203214.797	-14.375	506.115	506.134
	203224.797	-14.375	506.183	506.185
	203234.797	-14.375	506.210	506.219
	203242.430	-14.375	506.245	506.245
	203252.430	-14.375	506.288	506.301
	203262.430	-14.375	506.329	506.353
	203272.430	-14.375	506.369	506.403
	203282.430	-14.375	506.405	506.520
E Pier 3	203292.430	-14.375	506.440	506.477
	203302.430	-14.375	506.474	506.501
	203312.430	-14.375	506.505	506.517
	203320.440	-14.375	506.529	506.525
E Brg East Abut	203330.440	-14.375	506.557	506.557
	203340.440	-14.375	506.588	506.588
	203350.440	-14.375	506.607	506.613
	203360.440	-14.375	506.625	506.634
Bk East Abut	203371.440	-14.375	506.642	506.652

Girder 6

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut	203112.999	-21.950	505.501	505.501
E Brg West Abut	203118.333	-21.950	505.579	505.579
	203124.333	-21.950	505.688	505.631
	203130.333	-21.950	505.711	505.717
	203140.333	-21.950	505.574	505.577
E Pier 1	203150.333	-21.950	505.635	505.635
	203164.999	-21.950	505.687	505.687
	203174.999	-21.950	505.744	505.739
	203184.999	-21.950	505.800	505.830
E Pier 2	203194.999	-21.950	505.838	505.864
	203204.599	-21.950	505.904	505.930
	203214.999	-21.950	505.958	505.989
	203224.999	-21.950	506.008	506.025
	203234.999	-21.950	506.050	506.060
	203242.041	-21.950	506.088	506.088
	203252.041	-21.950	506.149	506.141
	203264.041	-21.950	506.170	506.153
	203274.041	-21.950	506.205	506.244
	203284.041	-21.950	506.240	506.291
E Pier 3	203294.041	-21.950	506.281	506.318
	203304.041	-21.950	506.313	506.346
	203314.041	-21.950	506.346	506.398
	203320.045	-21.950	506.370	506.370
E Brg East Abut	203330.045	-21.950	506.388	506.388
	203340.045	-21.950	506.424	506.418
	203350.045	-21.950	506.445	506.454
	203360.045	-21.950	506.471	506.475
Bk East Abut	203371.045	-21.950	506.489	506.489

DESIGNED A. V. Knappe  
CHECKED J. S. [Signature]  
DRAWN P. G. Barnett BKR  
CHECKED J. S. [Signature]  
E-S 8-1-65

APRIL 10 1965  
EXAMINED [Signature]  
PASSED [Signature]  
APPROVED [Signature]

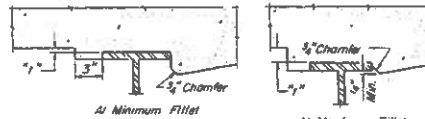
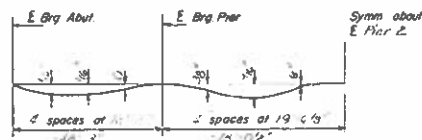
For Plan see sheet #4

TOP OF SLAB ELEVATION  
NORTH BRIDGE  
FBI RT 64 SECTION 41-611V8-1  
JEFFERSON COUNTY  
STA 2031+94.04



STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

PROJECT NO. 1-64 CHVB-1 JEFFERSON 28 11 SHEET NO. 6  
OF 11 SHEETS



To determine "f". After all structural steel has been erected, elevations of the top flanges of the beams shall be taken at intervals shown below. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection" shown below, minus slab thickness, equals the fillet height "f" above top flange of beams.

FILLET HEIGHTS

DEAD LOAD DEFLECTION DIAGRAM

(Includes weight of concrete only)  
Note: The above deflections are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections as shown below.

Girder 1				
Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
BK W Abut	203110+211	±1.956	505.417	505.417
E Brg West Abut	203112+304	±1.956	505.433	505.433
	203112+308	±1.956	505.439	505.439
	203112+304	±1.956	505.445	505.445
	203112+308	±1.956	505.451	505.451
E Pier 1	203117+441	±1.956	505.733	505.733
	203118+431	±1.956	505.749	505.749
	203119+421	±1.956	505.765	505.765
	203120+411	±1.956	505.781	505.781
	203121+401	±1.956	505.797	505.797
	203122+391	±1.956	505.813	505.813
	203123+381	±1.956	505.829	505.829
	203124+371	±1.956	505.845	505.845
E Pier 2	203128+272	±1.956	506.121	506.121
	203129+262	±1.956	506.137	506.137
	203130+252	±1.956	506.153	506.153
	203131+242	±1.956	506.169	506.169
	203132+232	±1.956	506.185	506.185
	203133+222	±1.956	506.201	506.201
	203134+212	±1.956	506.217	506.217
	203135+202	±1.956	506.233	506.233
E Pier 3	203141+114	±1.956	506.354	506.354
	203142+114	±1.956	506.370	506.370
	203143+114	±1.956	506.386	506.386
	203144+114	±1.956	506.402	506.402
E Brg East Abut	203175+003	±1.956	506.502	506.502
	203176+014	±1.956	506.510	506.510

Girder 2				
Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
BK W Abut	203110+211	±1.956	505.572	505.572
E Brg West Abut	203112+304	±1.956	505.788	505.788
	203112+308	±1.956	505.794	505.794
	203112+304	±1.956	505.800	505.800
	203112+308	±1.956	505.806	505.806
E Pier 1	203117+441	±1.956	505.889	505.889
	203118+431	±1.956	505.905	505.905
	203119+421	±1.956	505.921	505.921
	203120+411	±1.956	505.937	505.937
	203121+401	±1.956	505.953	505.953
	203122+391	±1.956	505.969	505.969
	203123+381	±1.956	505.985	505.985
	203124+371	±1.956	506.001	506.001
E Pier 2	203128+272	±1.956	506.277	506.277
	203129+262	±1.956	506.293	506.293
	203130+252	±1.956	506.309	506.309
	203131+242	±1.956	506.325	506.325
	203132+232	±1.956	506.341	506.341
	203133+222	±1.956	506.357	506.357
	203134+212	±1.956	506.373	506.373
	203135+202	±1.956	506.389	506.389
E Pier 3	203141+114	±1.956	506.500	506.500
	203142+114	±1.956	506.516	506.516
	203143+114	±1.956	506.532	506.532
	203144+114	±1.956	506.548	506.548
E Brg East Abut	203175+003	±1.956	506.607	506.607
	203176+014	±1.956	506.613	506.613

Girder 3				
Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
BK W Abut	203110+211	±1.956	505.701	505.701
E Brg West Abut	203112+304	±1.956	505.716	505.716
	203112+308	±1.956	505.722	505.722
	203112+304	±1.956	505.728	505.728
	203112+308	±1.956	505.734	505.734
E Pier 1	203117+441	±1.956	506.017	506.017
	203118+431	±1.956	506.033	506.033
	203119+421	±1.956	506.049	506.049
	203120+411	±1.956	506.065	506.065
	203121+401	±1.956	506.081	506.081
	203122+391	±1.956	506.097	506.097
	203123+381	±1.956	506.113	506.113
	203124+371	±1.956	506.129	506.129
E Pier 2	203128+272	±1.956	506.406	506.406
	203129+262	±1.956	506.422	506.422
	203130+252	±1.956	506.438	506.438
	203131+242	±1.956	506.454	506.454
	203132+232	±1.956	506.470	506.470
	203133+222	±1.956	506.486	506.486
	203134+212	±1.956	506.502	506.502
	203135+202	±1.956	506.518	506.518
E Pier 3	203141+114	±1.956	506.680	506.680
	203142+114	±1.956	506.696	506.696
	203143+114	±1.956	506.712	506.712
	203144+114	±1.956	506.728	506.728
E Brg East Abut	203175+003	±1.956	506.793	506.793
	203176+014	±1.956	506.809	506.809

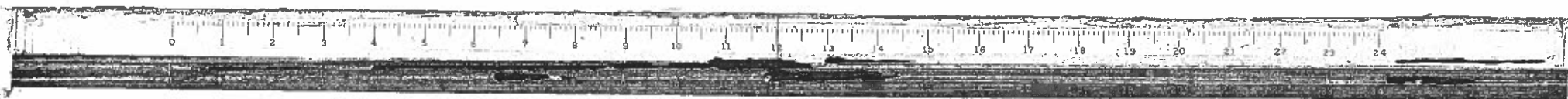
E. E. Bd. Lane				
Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
BK W Abut	203110+211	-0.000	505.604	505.604
E Brg West Abut	203112+304	-0.000	505.620	505.620
	203112+308	-0.000	505.626	505.626
	203112+304	-0.000	505.632	505.632
	203112+308	-0.000	505.638	505.638
E Pier 1	203117+441	-0.000	506.121	506.121
	203118+431	-0.000	506.137	506.137
	203119+421	-0.000	506.153	506.153
	203120+411	-0.000	506.169	506.169
	203121+401	-0.000	506.185	506.185
	203122+391	-0.000	506.201	506.201
	203123+381	-0.000	506.217	506.217
	203124+371	-0.000	506.233	506.233
E Pier 2	203128+272	-0.000	506.511	506.511
	203129+262	-0.000	506.527	506.527
	203130+252	-0.000	506.543	506.543
	203131+242	-0.000	506.559	506.559
	203132+232	-0.000	506.575	506.575
	203133+222	-0.000	506.591	506.591
	203134+212	-0.000	506.607	506.607
	203135+202	-0.000	506.623	506.623
E Pier 3	203141+114	-0.000	506.705	506.705
	203142+114	-0.000	506.721	506.721
	203143+114	-0.000	506.737	506.737
	203144+114	-0.000	506.753	506.753
E Brg East Abut	203175+003	-0.000	506.896	506.896
	203176+014	-0.000	506.902	506.902

DESIGNED: A. V. HANNA  
CHECKED: B. W. HANNA  
DRAWN: P. G. Barnett BRR  
CHECKED: J. R. HANNA  
E-5 8-65

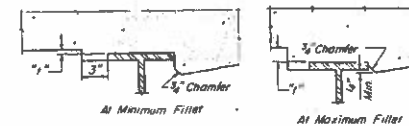
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EXAMINED: [Signature]  
PASSED: [Signature]  
APPROVED: [Signature]

For Plan see sheet #4

TOP OF SLAB ELEVATION  
SOUTH BRIDGE  
EAL RT. 64 SECTION 41-64VB-1  
JEFFERSON COUNTY  
STA. 2031+94.04



STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS



PROJECT NO.	28	12	SHEET NO. 7
SECTION	JEFFERSON		
DATE	28	12	

To determine "f" After all structural steel has been erected, elevations of the top flange of the beams shall be taken at intervals shown below. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection" shown below, minus slab thickness, equals the fillet heights "f" above top flange of beams.

FILLET HEIGHTS

Girder 4

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
At N Abut	203118.839	-0.792	363.292	363.292
E Brg West Abut	203122.322	-0.792	363.601	363.601
	203121.292	-0.792	363.612	363.612
	203120.262	-0.792	363.623	363.623
	203119.232	-0.792	363.634	363.634
E Pier 1	203170.039	-0.792	366.105	366.105
	203170.039	-0.792	366.105	366.105
	203170.039	-0.792	366.105	366.105
	203170.039	-0.792	366.105	366.105
	203170.039	-0.792	366.105	366.105
	203170.039	-0.792	366.105	366.105
	203170.039	-0.792	366.105	366.105
	203170.039	-0.792	366.105	366.105
E Pier 2	203248.080	-0.792	366.498	366.498
	203248.080	-0.792	366.498	366.498
	203248.080	-0.792	366.498	366.498
	203248.080	-0.792	366.498	366.498
	203248.080	-0.792	366.498	366.498
	203248.080	-0.792	366.498	366.498
	203248.080	-0.792	366.498	366.498
	203248.080	-0.792	366.498	366.498
E Pier 3	203316.122	-0.792	366.774	366.774
	203316.122	-0.792	366.774	366.774
	203316.122	-0.792	366.774	366.774
	203316.122	-0.792	366.774	366.774
	203316.122	-0.792	366.774	366.774
E Brg East Abut	203374.269	-0.792	368.660	368.660
BK East Abut	203381.144	-0.792	368.690	368.690

Girder 5

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
BK W Abut	203118.839	-0.375	363.671	363.671
E Brg West Abut	203120.974	-0.375	363.660	363.660
	203120.974	-0.375	363.671	363.671
	203120.974	-0.375	363.682	363.682
	203120.974	-0.375	363.693	363.693
E Pier 1	203169.841	-0.375	363.388	363.388
	203170.039	-0.375	363.388	363.388
	203170.039	-0.375	363.388	363.388
	203170.039	-0.375	363.388	363.388
	203170.039	-0.375	363.388	363.388
	203170.039	-0.375	363.388	363.388
	203170.039	-0.375	363.388	363.388
	203170.039	-0.375	363.388	363.388
	203170.039	-0.375	363.388	363.388
	203170.039	-0.375	363.388	363.388
	203170.039	-0.375	363.388	363.388
E Pier 2	203247.883	-0.375	366.376	366.376
	203247.883	-0.375	366.376	366.376
	203247.883	-0.375	366.376	366.376
	203247.883	-0.375	366.376	366.376
	203247.883	-0.375	366.376	366.376
	203247.883	-0.375	366.376	366.376
	203247.883	-0.375	366.376	366.376
	203247.883	-0.375	366.376	366.376
E Pier 3	203317.224	-0.375	366.661	366.661
	203317.224	-0.375	366.661	366.661
	203317.224	-0.375	366.661	366.661
	203317.224	-0.375	366.661	366.661
	203317.224	-0.375	366.661	366.661
E Brg East Abut	203374.269	-0.375	368.766	368.766
BK East Abut	203381.144	-0.375	368.771	368.771

Girder 6

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
BK W Abut	203118.244	-15.928	363.349	363.349
E Brg West Abut	203120.577	-15.928	363.345	363.345
	203120.577	-15.928	363.345	363.345
	203120.577	-15.928	363.345	363.345
	203120.577	-15.928	363.345	363.345
E Pier 1	203169.244	-15.928	363.647	363.647
	203170.039	-15.928	363.647	363.647
	203170.039	-15.928	363.647	363.647
	203170.039	-15.928	363.647	363.647
	203170.039	-15.928	363.647	363.647
	203170.039	-15.928	363.647	363.647
	203170.039	-15.928	363.647	363.647
	203170.039	-15.928	363.647	363.647
E Pier 2	203247.285	-15.928	366.237	366.237
	203247.285	-15.928	366.237	366.237
	203247.285	-15.928	366.237	366.237
	203247.285	-15.928	366.237	366.237
	203247.285	-15.928	366.237	366.237
	203247.285	-15.928	366.237	366.237
	203247.285	-15.928	366.237	366.237
	203247.285	-15.928	366.237	366.237
E Pier 3	203316.322	-15.928	366.513	366.513
	203316.322	-15.928	366.513	366.513
	203316.322	-15.928	366.513	366.513
	203316.322	-15.928	366.513	366.513
	203316.322	-15.928	366.513	366.513
E Brg East Abut	203374.269	-15.928	368.627	368.627
BK East Abut	203381.144	-15.928	368.631	368.631

DESIGNED: A. V. [Signature]  
CHECKED: [Signature]  
DRAWN: P. E. Barnett BFR  
CHECKED: [Signature]  
E-S 8-65

APR 10 1965  
EXAMINED: [Signature]  
PASSED: [Signature]  
APPROVED: [Signature]

For Plan see sheet # 4

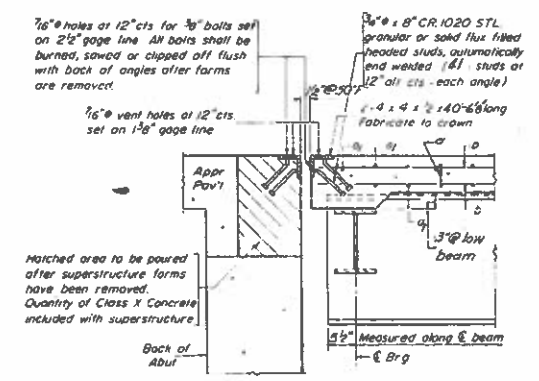
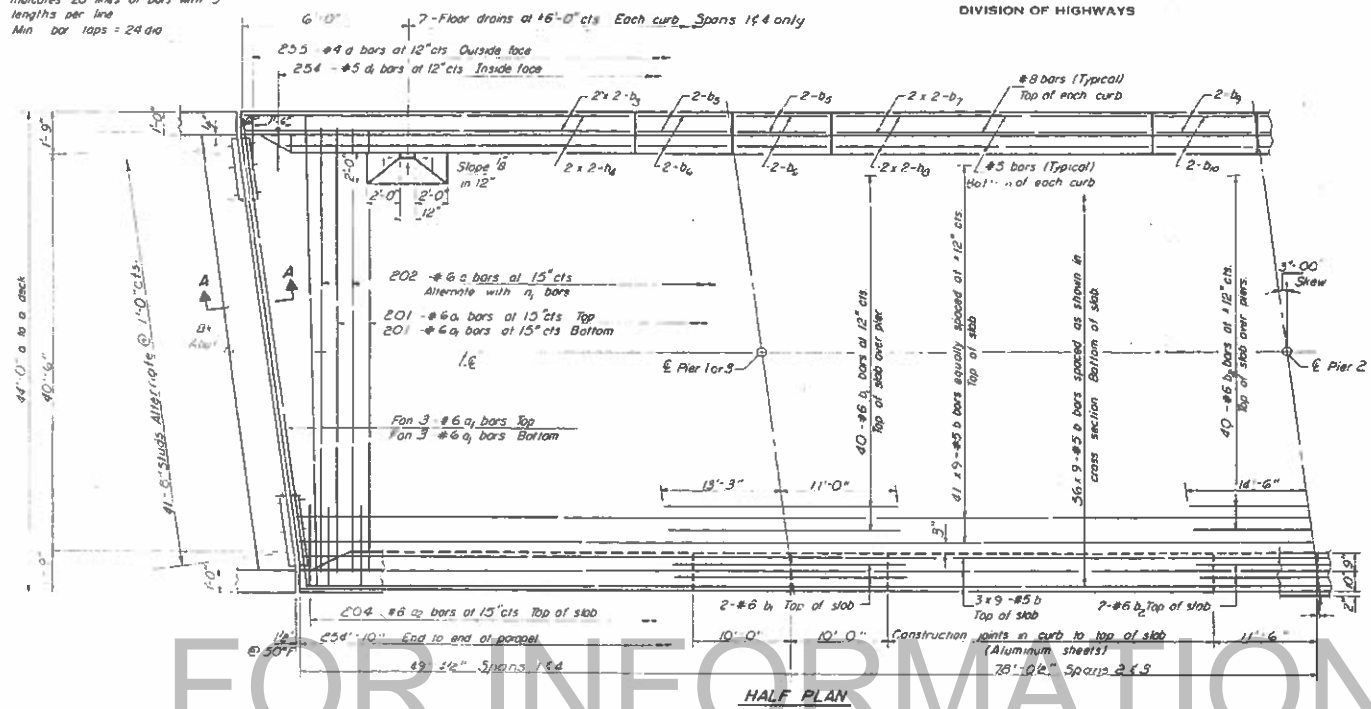
TOP OF SLAB ELEVATION  
SOUTH BRIDGE  
FAI RT 64 SECTION 41-GHYB-1  
JEFFERSON COUNTY  
STA. 2031794.04



STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

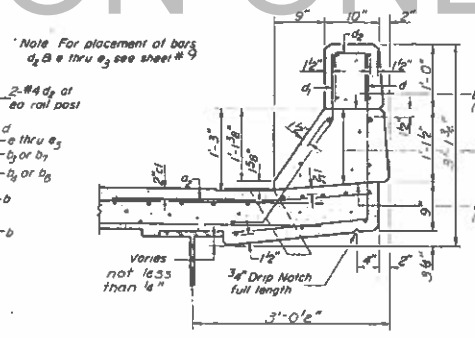
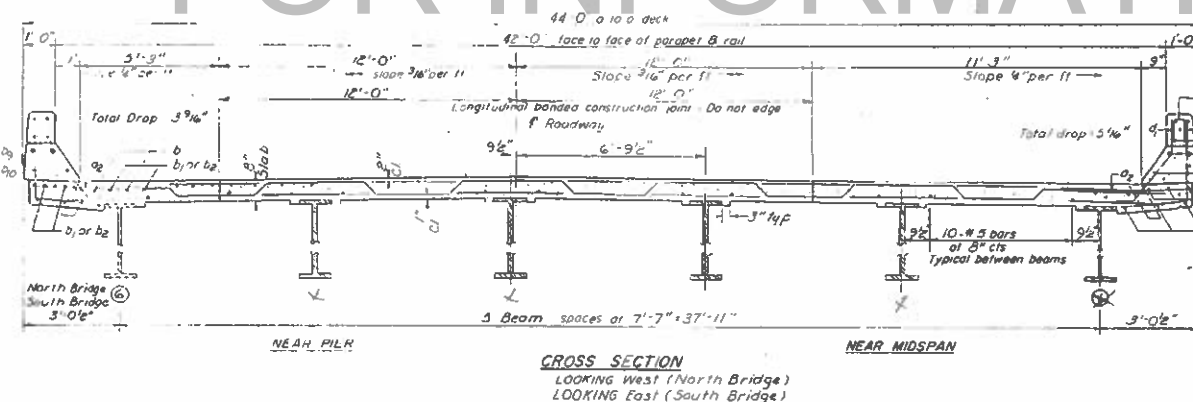
PROJECT NO.	28	13	SHEET NO. 8
CONTRACT NO.	JEFFERSON	28	70 SHEETS

Note  
Bars indicated thus 20 x 3-#5 etc  
indicates 20 lines of bars with 3  
lengths per line  
Min. bar laps = 24 dia



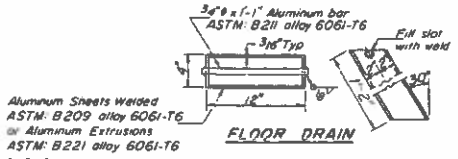
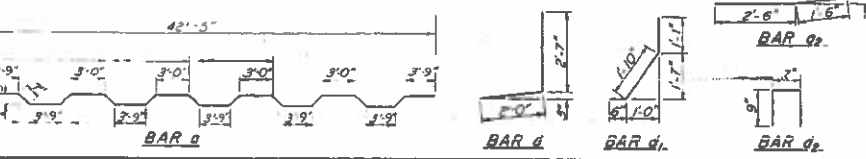
SECTION A-A  
TWO BRIDGES  
BILL OF MATERIAL

Bar	No	Size	Length	Shape
a	401	#6	44'-11"	
a	228	#6	72'-6"	
a	8	#6	4'-0"	
b	185d	#5	29'-0"	
b	176	#6	24'-3"	
b	88	#6	29'-0"	
b	39	#4	20'-0"	
b	30	#3	20'-3"	
b	37	#3	9'-9"	
b	32	#3	9'-9"	
b	32	#3	29'-3"	
b	30	#3	25'-9"	
b	16	#3	11'-3"	
b	16	#3	11'-3"	
Reinforcement Bars				Lbs. 166650
Class X Concrete				Cu Yds. 644.6
Stud Shear Connectors				Eo. 3784



CURB SECTION  
Cast of Aluminum Drains and Sheets  
shall be incidental to Class X Concrete

DESIGNED A. V. HANNAY  
CHECKED [Signature]  
DRAWN [Signature]  
I-4106-R (115) 3-1-68

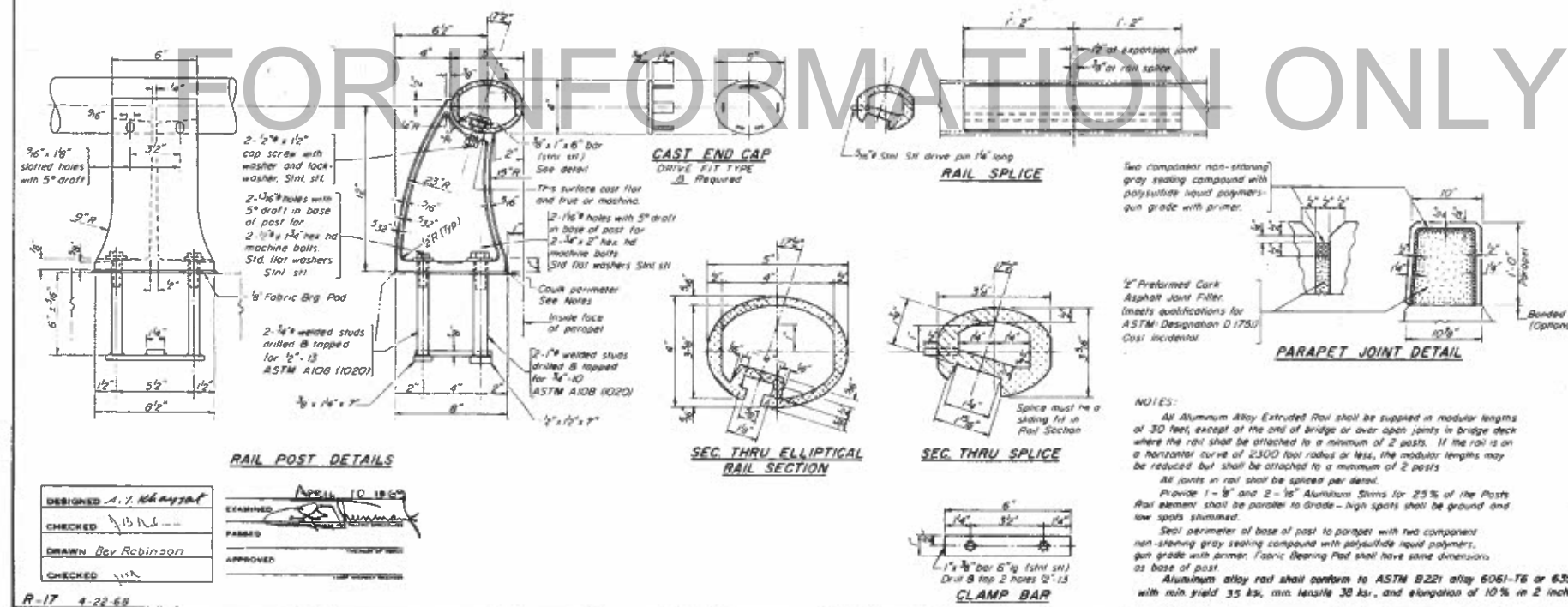


The lengths and quantities of longitudinal reinforcement and Class X Concrete in parapets are not included in above quantities. See sheet 7.  
SUPERSTRUCTURE  
NORTH & SOUTH BRIDGES  
I-4106-R SEC. 61-6HVB-1  
JEFFERSON COUNTY  
STA 2031+94.04





ELEVATION



DESIGNED *A. J. Knappe*  
CHECKED *B. N. ...*  
DRAWN *Ray Robinson*  
CHECKED *...*

APRIL 10 1969  
EXAMINED  
FORWARDED  
APPROVED

**PARAPETS & RAILS  
BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
#1	12	#4	17'-9"	
#2	24	#4	9'-9"	
#3	36	#4	18'-7"	
#4	32	#4	11'-9"	
Reinforcement Bars				Lbs 2680
Class X Concrete				Cu. Yds. 32.5
Aluminum Railing				Lbs 1060

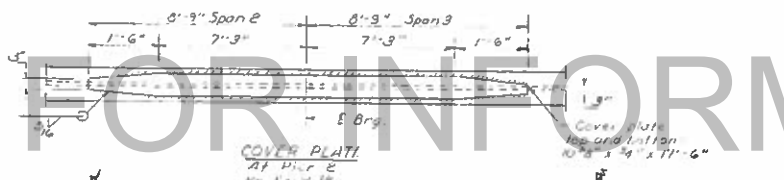
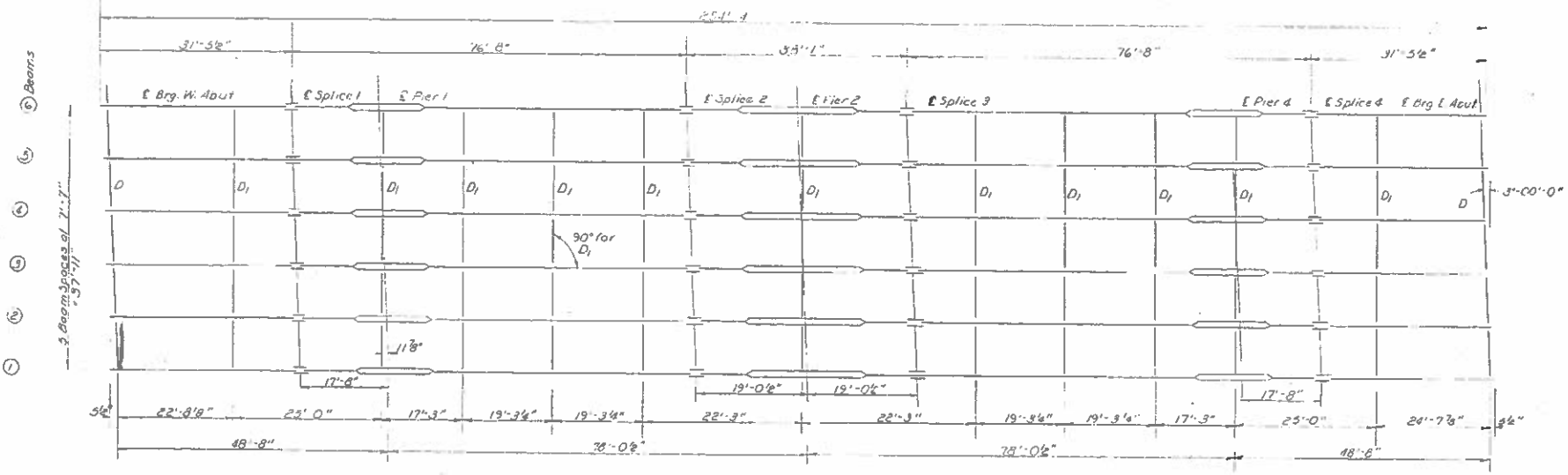
**ALUMINUM RAILING**  
FAI RT64 SEC 41 6116-1  
JEFFERSON COUNTY  
STA 2031-54.04

**NOTES:**  
All Aluminum Alloy Extruded Rail shall be supplied in modular lengths of 30 feet, except at the end of bridge or over open joints in bridge deck where the rail shall be attached to a minimum of 2 posts. If the rail is on a horizontal curve of 2300 foot radius or less, the modular lengths may be reduced but shall be attached to a minimum of 2 posts.  
All joints in rail shall be spaced per detail.  
Provide 1-#4 and 2-#6 Aluminum Straps for 25% of the Posts. Rail element shall be parallel to Grade - high spots shall be ground and low spots shimmed.  
Seal perimeter of base of post to parapet with two component non-staining gray sealing compound with polysulfide liquid polymer, gun grade with primer. Fabric Bearing Pad shall have same dimensions as base of post.  
Aluminum alloy rail shall conform to ASTM B221 alloy 6061-T6 or 6351-T5 with min yield 35 ksi, min tensile 38 ksi, and elongation of 10% in 2 inches.

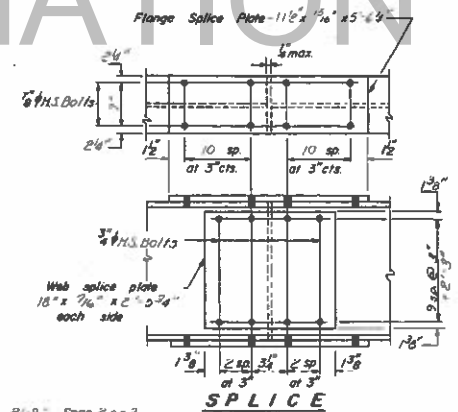
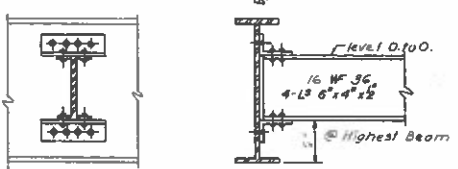
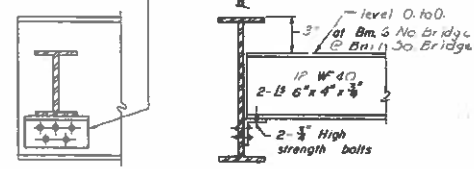
$\cos 3^\circ = 0.9986, x = 7.593'$

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

Project No.	41-64	Section	JEFFERSON	Sheet No.	28	Total Sheets	15
Job Name	JEFFERSON COUNTY						



In lieu of shop rivets,  
angles may be attached  
in the shop with 3/4" High  
strength bolts



TOP OF FLANGE ELEVATION

	Span 1	Span 2	Span 3	Pier 4	Pier 5	Pier 6
E Brg. W. Abut	264.87	265.01	265.12	265.03	264.89	264.73
E Splice 1	264.80	264.85	264.84	264.75	264.71	264.52
E Brg. Pier 1	264.09	263.83	263.38	262.89	262.11	261.55
E Splice 2	263.49	263.57	263.68	263.59	263.43	263.29
E Brg. Pier 2	262.57	262.60	262.76	262.67	262.51	262.37
E Splice 3	261.57	261.73	261.64	261.73	261.67	261.49
E Brg. Pier 3	260.77	260.91	260.02	261.93	261.79	261.63
E Splice 4	260.02	260.96	260.07	260.98	261.84	260.69
E Brg. E. Abut	260.97	260.71	260.82	260.3	260.99	260.83

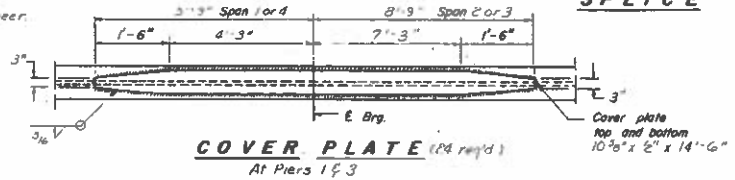
SOUTH BRIDGE

E Brg. W. Abut	262.78	262.93	263.06	263.14	263.03	262.89
E Splice 1	262.90	263.05	263.18	263.28	263.10	263.01
E Brg. Pier 1	262.00	262.72	263.28	263.38	263.21	263.11
E Splice 2	263.33	263.48	263.61	263.71	263.58	263.44
E Brg. Pier 2	263.41	263.36	263.49	263.79	263.68	263.52
E Splice 3	263.49	263.64	263.77	263.87	263.74	263.60
E Brg. Pier 3	263.55	263.87	263.98	264.04	263.91	263.77
E Splice 4	263.71	263.86	263.99	264.09	263.96	263.82
E Brg. E. Abut	263.66	263.61	263.74	263.84	263.71	263.57

\* For fabrication only

DESIGNED A. T. Minyard  
CHECKED B. J. Robinson  
DRAWN V. Schaeffer  
CHECKED P. H.

EXAMINED APRIL 10, 1965  
PASSED  
APPROVED

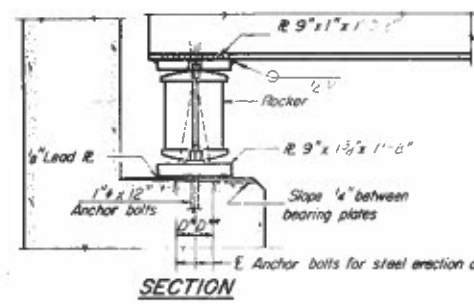


STRUCTURAL STEEL  
FAI RT 64 SEC 41-GHVB-1  
JEFFERSON COUNTY  
STA. 2031+94.04

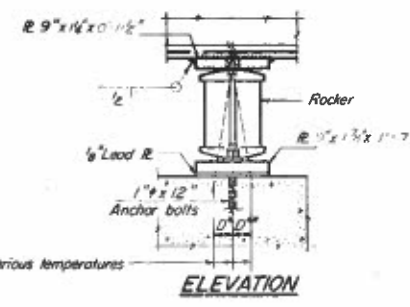
I-2-CD 9-1-65

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

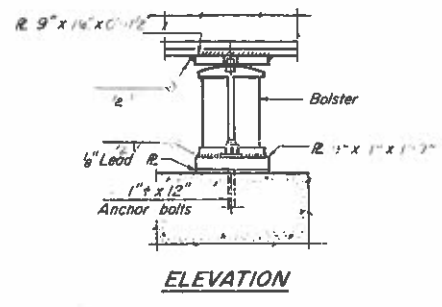
PROJECT NO.	41-64	SECTION	JEFFERSON 28	POST	16	SHEET NO. 11
DATE	10-1-65	BY		SCALE		20 SHEETS



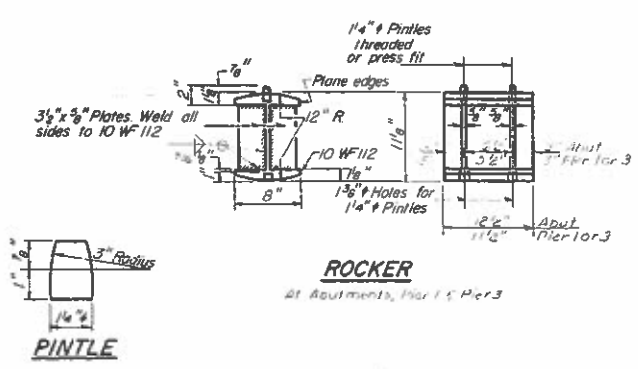
SECTION



ELEVATION

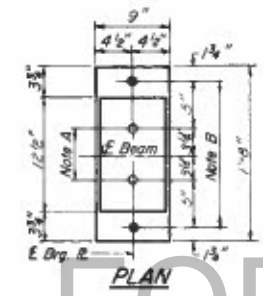


ELEVATION



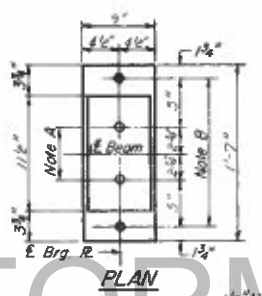
ROCKER

PINTLE



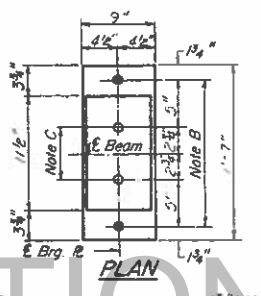
PLAN AT ABUTMENT

NOTE A  
1 1/2" Holes 1" deep in top R for pintles. Thread or press fit pintles into bottom R.



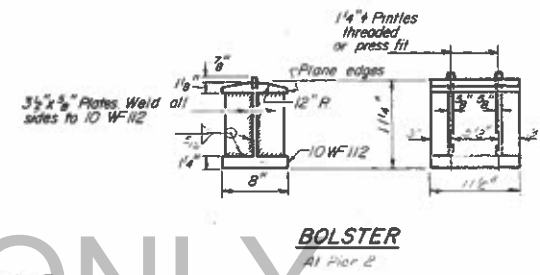
PLAN AT PIER 1

NOTE B  
1 1/2" Holes for 1" anchor bolts. 2 1/2" x 2 1/2" x 3/4" R washers under nut.



PLAN AT PIER 2

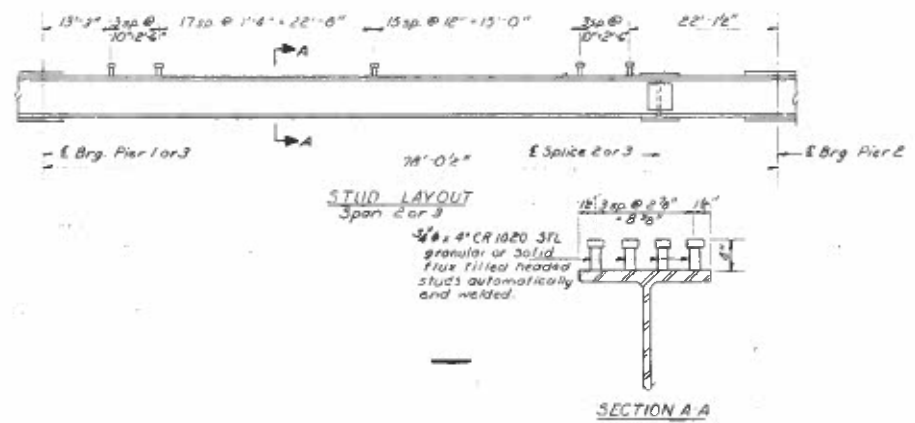
NOTE C  
1 1/2" Holes 1" deep in top R only for 1/4" pintles.



BOLSTER

NOTES ON SETTING OF ANCHOR BOLTS AT EXP. BRGS.

- a) D\* (Side of brg. away from fixed brg.)  
D\* = 1/8" per each 100' of expansion for every 15" fall below the normal temp. of 50°F
- D\*\* (Side of brg. toward fixed brg.)  
D\*\* = 1/8" per each 100' of expansion for every 15" rise above the normal temp. of 50°F
- b) After beams have been erected and dimensions D\* or D\*\* determined, holes shall be drilled and anchor bolts shall be grouted in place. All fixed anchor bolts may be built into the masonry.



STUD LAYOUT

SECTION A-A

TABLE OF MOMENTS & REACTIONS  
(Interior Beams)

	Moments (Kip-ft)		Reactions (Kips)	
	Pier 1	Pier 2	Abut.	Pier 1/Pier 2
D.L.	103.8	407.8	249.6	48.38
S.D.L.	39.6	102.9	97.2	4.90
I	32.3	319.5	341.8	37.97
Imp.	94.0	85.8	183.8	14.63
Total	169.7	926.0	1175.6	105.88

\*Symm. about E. Pier 2

PROPERTIES

STEEL SECTION	
I <sub>x</sub>	8147.6 in <sup>4</sup>
S <sub>T<sub>x</sub></sub>	486.4 in <sup>3</sup>
S <sub>C<sub>x</sub></sub>	485.2 in <sup>3</sup>
COMPOSITE SECTION	
I <sub>c</sub>	1756.7 in <sup>4</sup>
S <sub>T<sub>c</sub></sub>	4475.1 in <sup>3</sup>
S <sub>B<sub>c</sub></sub>	679.7 in <sup>3</sup>

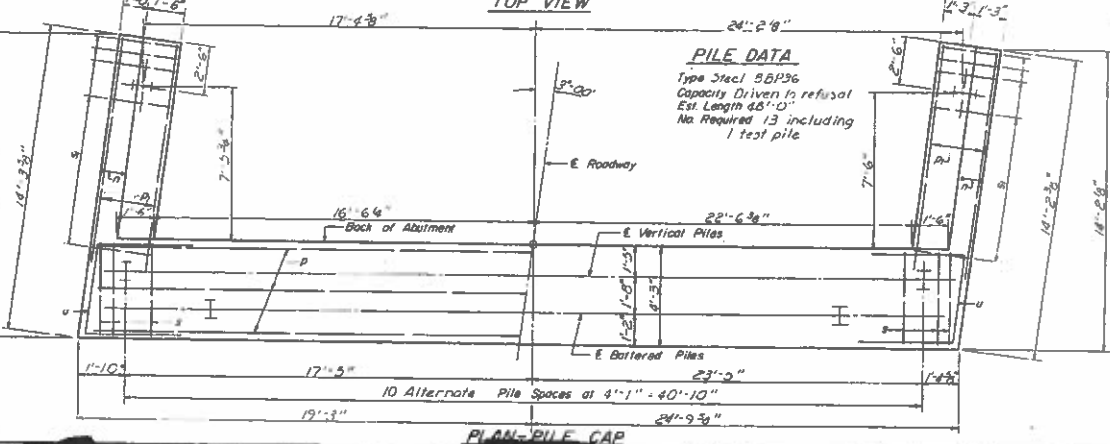
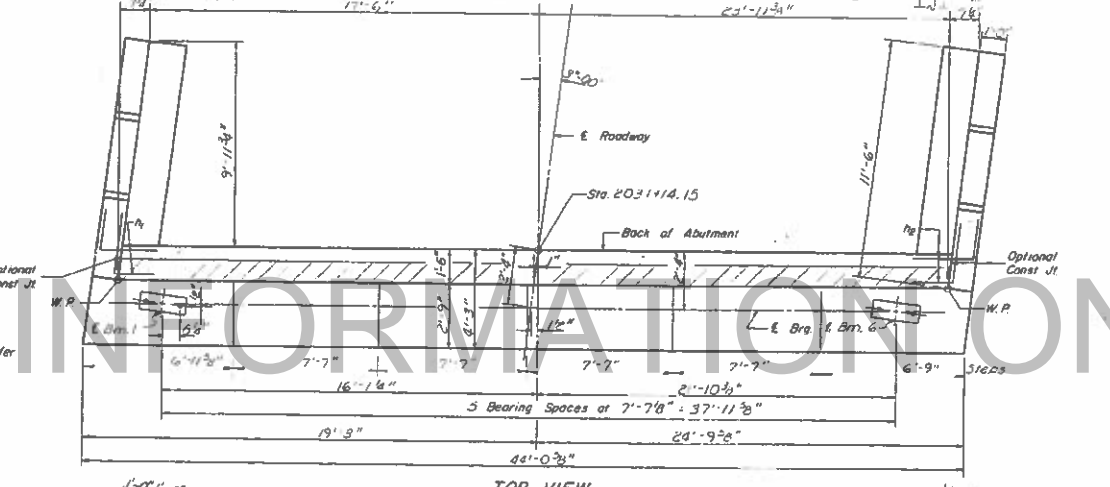
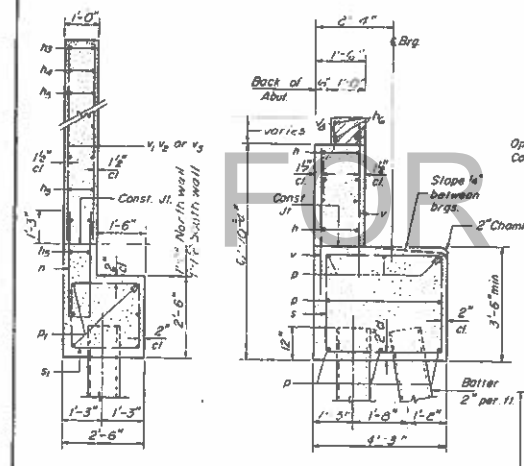
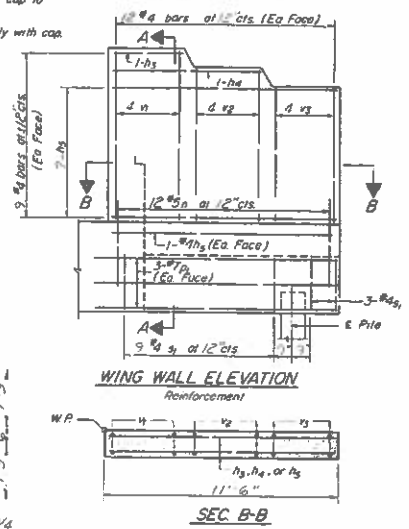
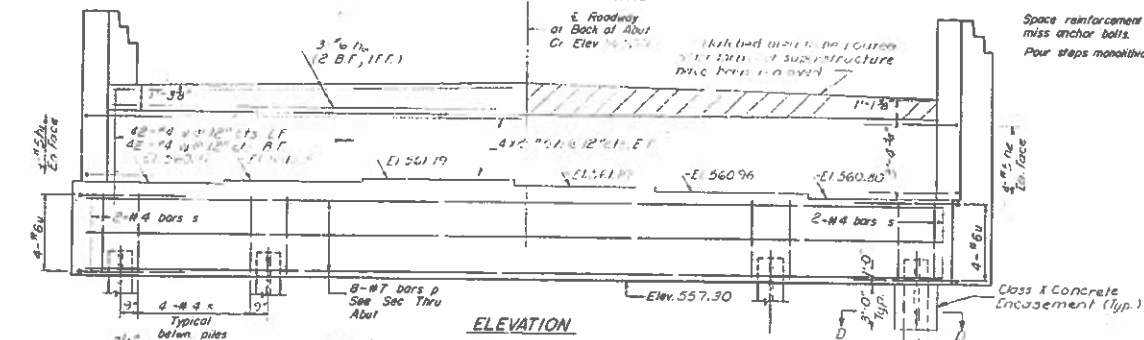
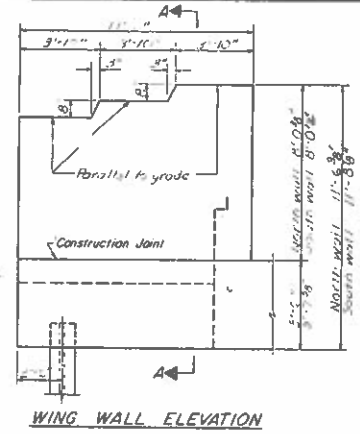
STRUCTURAL STEEL  
FAT. RT. GA. SEC. 41-HV6-1  
JEFFERSON COUNTY  
STA 2031+24.04

DESIGNED: J.V. HAASVOLD  
CHECKED: J.B. HARRIS  
DRAWN: P.G. BARNETT  
APPROVED: [Signature]

I-2-B 9-1-65

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

PROJECT NO.	41-64	SECTION	JEFFERSON 28	SHEET NO.	17
DATE	1-1-66	BY	...	DATE	...



**PILE DATA**  
Type Steel SBP36  
Capacity Driven to refusal  
Est. Length 48'-0"  
No. Required 13 including 1 test pile

ONE ABUTMENT  
BILL OF MATERIAL

Bar	No	Size	Length	Shape
n	40	#5	21'-6"	L
n	5	#5	9'-0"	L
h <sub>2</sub>	4	#4	9'-0"	L
h <sub>4</sub>	4	#4	9'-0"	L
h <sub>6</sub>	5	#4	11'-0"	L
h <sub>8</sub>	3	#4	4'-3"	L
u	20	#5	21'-6"	L
p	5	#7	11'-0"	L
p	12	#7	11'-0"	L
s	44	#4	18'-11"	DD
s	24	#4	9'-0"	DD
u	5	#5	9'-0"	L
v	24	#4	5'-8"	L
w	16	#4	7'-9"	L
w	16	#4	7'-1"	L
w	16	#4	6'-5"	L
u <sub>2</sub>	22	#4	3'-3"	L
Class X Concrete	Cu Yds	44.7		
Reinforcement Bars	Lbs	3440		
Steel Piles SBP36	Lin Ft	576		
Test Piles steel (SBP36)	Eq.	1		

WEST ABUTMENT  
NORTH BRIDGE  
FAI RT 64 SEC 41-64MB/1  
JEFFERSON COUNTY  
STA 2031-94.04

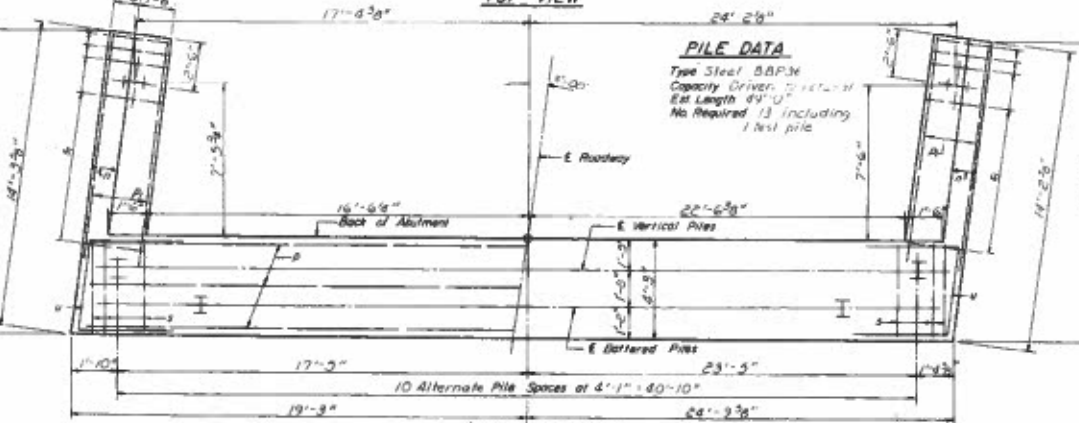
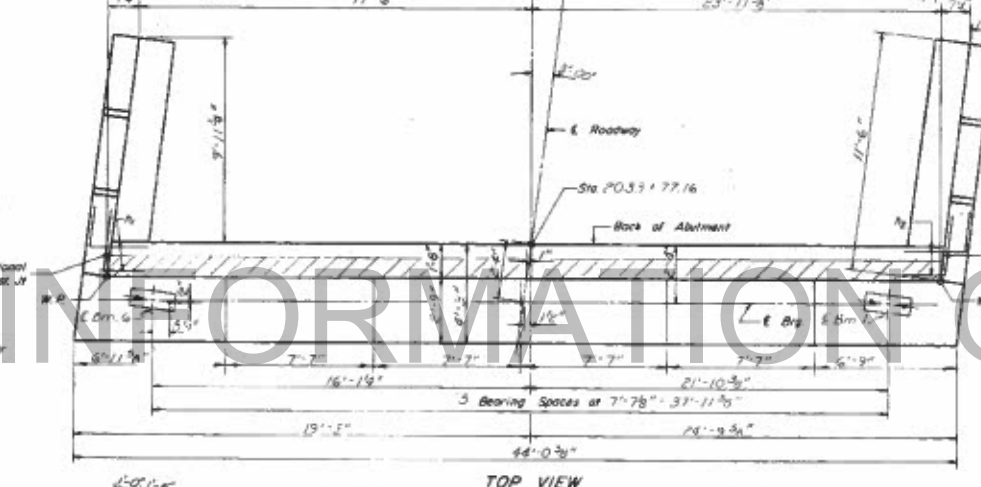
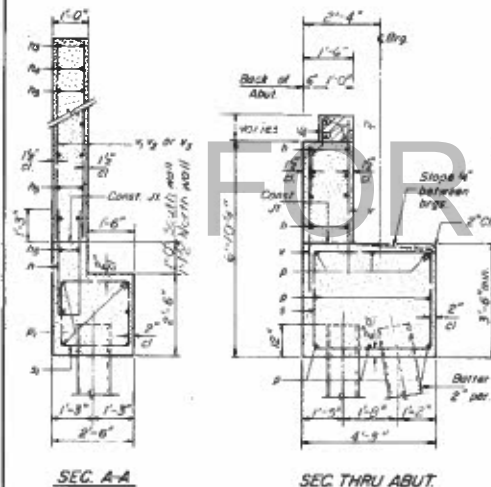
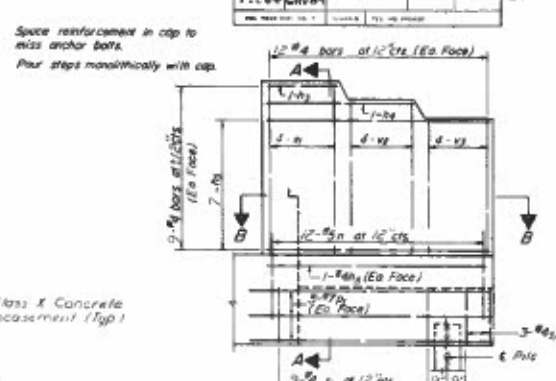
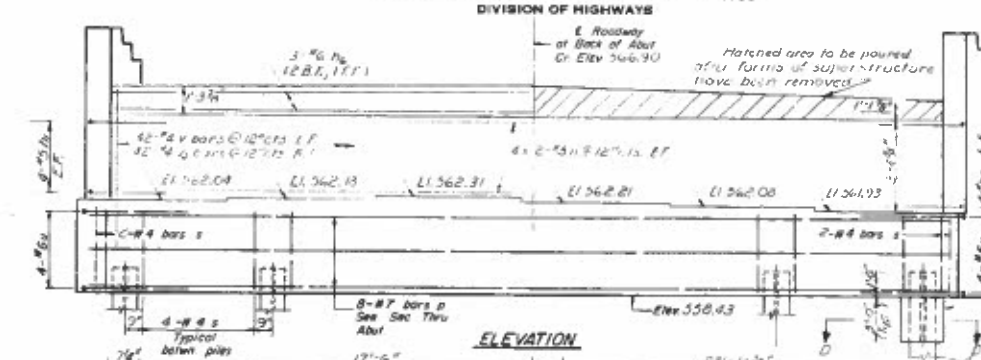
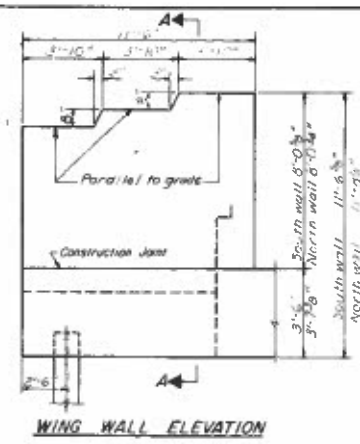
DESIGNED A. J. Whaypa  
CHECKED S. G. Perchow  
DRAWN P. J. ...  
APRIL 10 1966  
EXAMINED ...  
PASSED ...  
APPROVED ...

A-9-R (1°-14°) 2-1-66



STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

PROJECT: JEFFERSON RD  
SHEET NO. 13  
DATE: 2-1-65



ONE ABUTMENT  
BILL OF MATERIAL

Bar No	Size	Length	Shape
n	#5	21'-0"	
n1	#5	21'-0"	L
n2	#5	21'-0"	L
n3	#5	21'-0"	L
n4	#5	21'-0"	L
n5	#5	21'-0"	L
n6	#5	21'-0"	L
p	#5	21'-0"	L
p1	#5	21'-0"	L
p2	#5	21'-0"	L
p3	#5	21'-0"	L
p4	#5	21'-0"	L
p5	#5	21'-0"	L
p6	#5	21'-0"	L
u	#8	9'-0"	J
v	#8	5'-0"	
v1	#8	5'-0"	
v2	#8	5'-0"	
v3	#8	5'-0"	
v4	#8	5'-0"	
v5	#8	5'-0"	
v6	#8	5'-0"	
CLASS X CONCRETE	Cu Yds	44.8	
REINFORCEMENT BARS	Lbs	3680	
STEEL PILES (A.P.P. 36)	Lbs Ft.	308	
TEST PILES (A.P.P. 36)	Eq.	1	

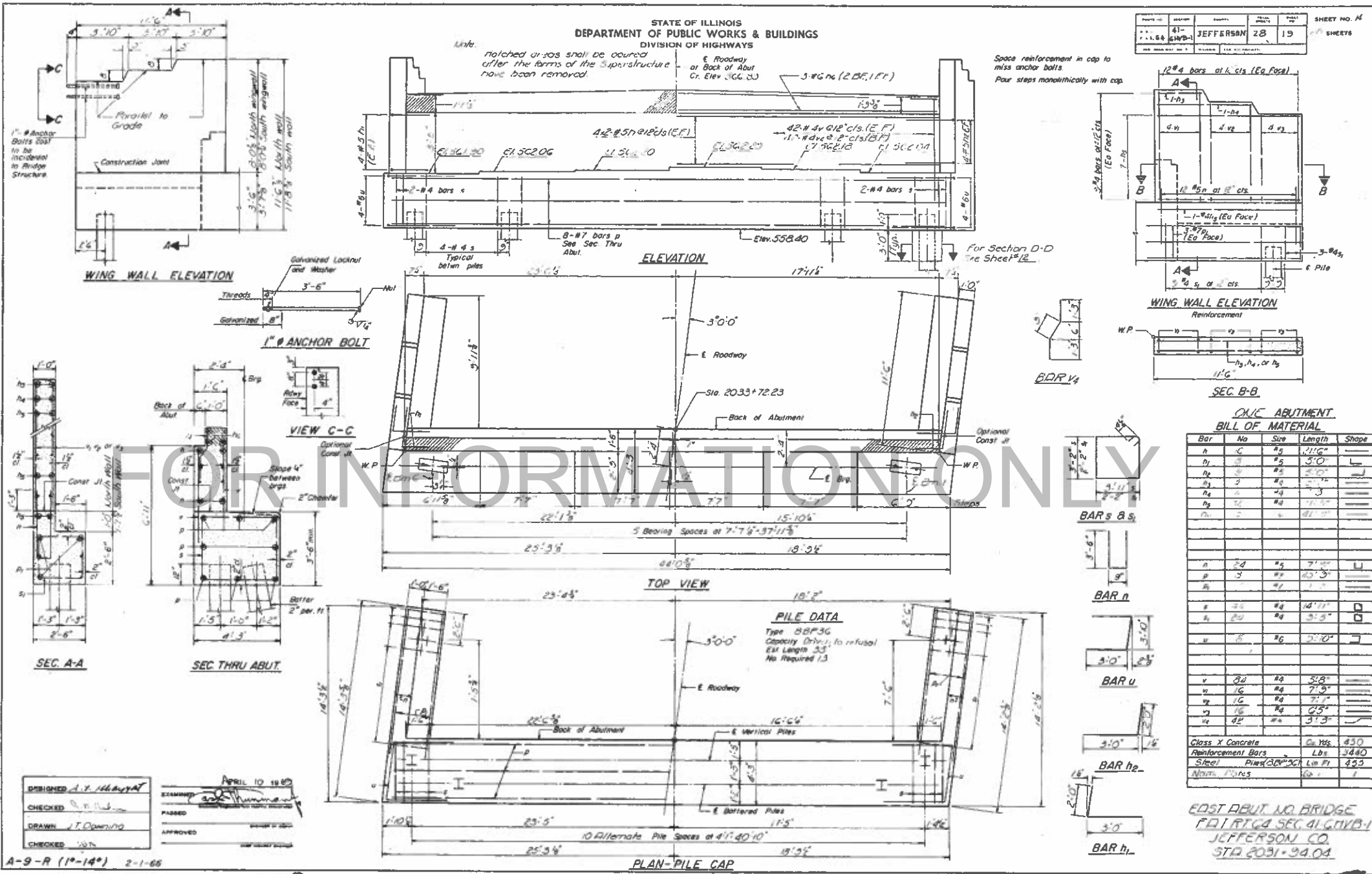
EAST ABUTMENT  
SOUTH BRIDGE  
FAI RT 64 SEC 41 64VB-1  
JEFFERSON COUNTY  
STA. 2031+94.04

DESIGNED: A. V. McLaughlin  
CHECKED: J. S. [Signature]  
DRAWN: B. J. Robinson  
APPROVED: [Signature]  
DATE: APRIL 10 1965

A-9-R (1'-14") 2-1-65

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

PROJECT NO.	41-104	SECTION	JEFFERSON 28	DATE	19
SHEET NO. 4					

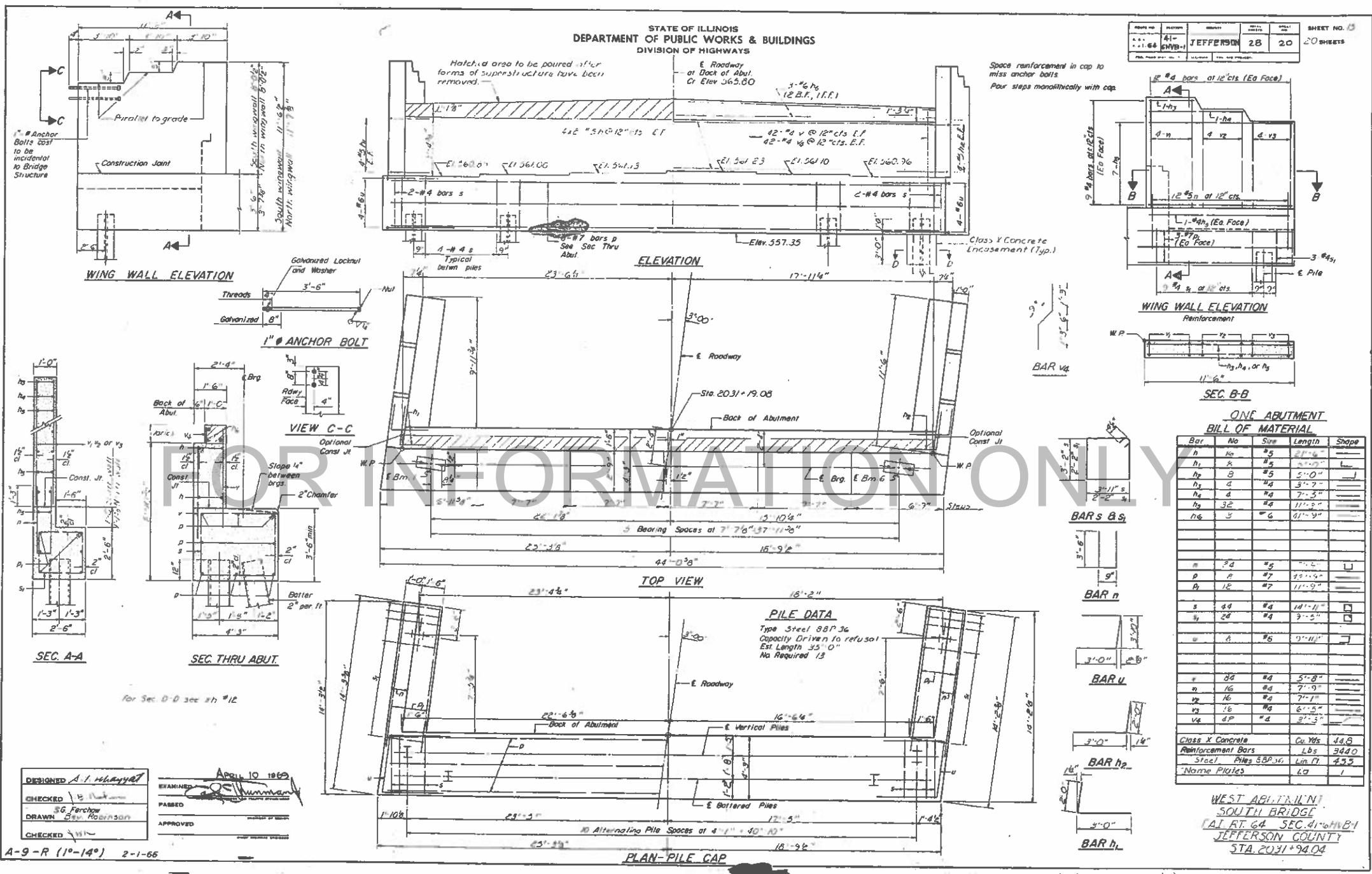


FOR INFORMATION ONLY



STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

PROJECT NO.	SECTION	DATE	SHEET NO.
41-164	ENR-1	JEFFERSON 2B	20
			20 SHEETS



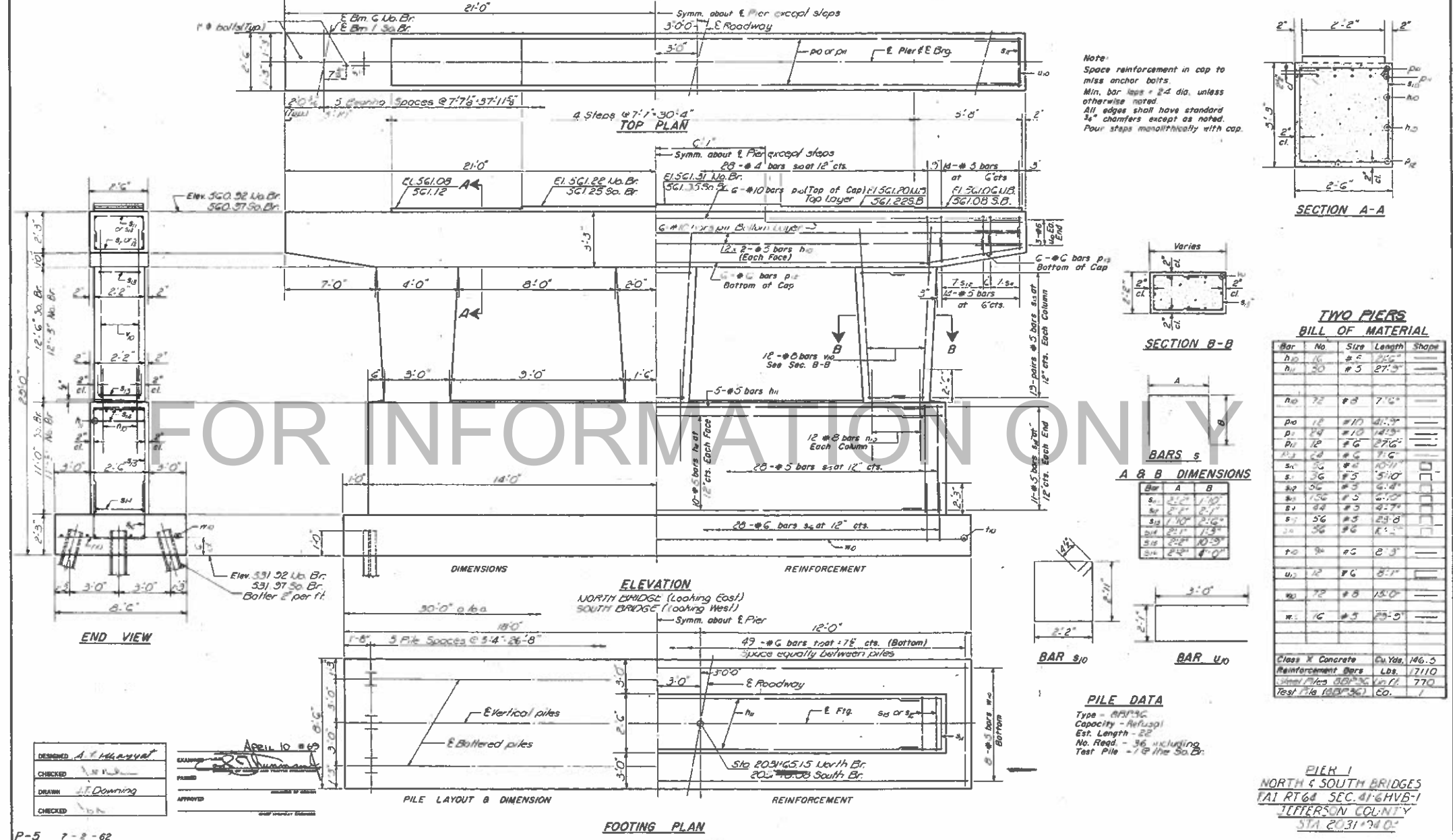
DESIGNED A. I. HAYES  
CHECKED B. [Signature]  
DRAWN G. W. ROBINSON  
CHECKED [Signature]

EXAMINED APRIL 10 1965  
PASSED [Signature]  
APPROVED [Signature]

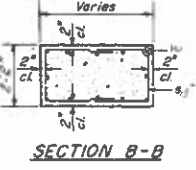
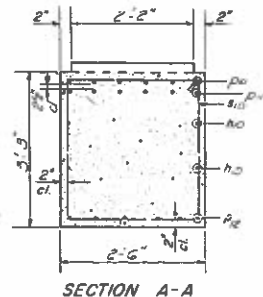
A-9-R (10-14) 2-1-65

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

PROJECT NO.	SECTION	COUNTY	POST MILE	SHEET NO.
1-14-64	6184-4	JEFFERSON	38	21
DESIGNED BY	DRAWN BY	CHECKED BY	DATE	



Note:  
Space reinforcement in cap to miss anchor bolts.  
Min. bar size = 2 dia, unless otherwise noted.  
All edges shall have standard 3/4" chamfers except as noted.  
Four steps monolithically with cap.



**BARS s**

Bar No.	Size	Length	Shape
h <sub>10</sub>	#10	27'-5"	
h <sub>12</sub>	#12	27'-5"	
h <sub>14</sub>	#14	27'-5"	
h <sub>16</sub>	#16	27'-5"	
h <sub>18</sub>	#18	27'-5"	
h <sub>20</sub>	#20	27'-5"	
h <sub>22</sub>	#22	27'-5"	
h <sub>24</sub>	#24	27'-5"	
h <sub>26</sub>	#26	27'-5"	
h <sub>28</sub>	#28	27'-5"	
h <sub>30</sub>	#30	27'-5"	
h <sub>32</sub>	#32	27'-5"	
h <sub>34</sub>	#34	27'-5"	
h <sub>36</sub>	#36	27'-5"	
h <sub>38</sub>	#38	27'-5"	
h <sub>40</sub>	#40	27'-5"	
h <sub>42</sub>	#42	27'-5"	
h <sub>44</sub>	#44	27'-5"	
h <sub>46</sub>	#46	27'-5"	
h <sub>48</sub>	#48	27'-5"	
h <sub>50</sub>	#50	27'-5"	

**TWO PIERS**  
**BILL OF MATERIAL**

Bar No.	No.	Size	Length	Shape
h <sub>10</sub>	10	#5	25'-5"	
h <sub>12</sub>	20	#5	27'-5"	
h <sub>14</sub>	22	#8	7'-0"	
h <sub>16</sub>	12	#10	41'-5"	
h <sub>18</sub>	24	#10	14'-5"	
h <sub>20</sub>	12	#6	27'-5"	
h <sub>22</sub>	24	#6	7'-6"	
h <sub>24</sub>	30	#4	10'-11"	
h <sub>26</sub>	36	#5	5'-10"	
h <sub>28</sub>	36	#5	6'-10"	
h <sub>30</sub>	156	#5	6'-10"	
h <sub>32</sub>	84	#5	4'-7"	
h <sub>34</sub>	56	#5	23'-8"	
h <sub>36</sub>	24	#6	8'-5"	
h <sub>38</sub>	36	#6	8'-5"	
h <sub>40</sub>	12	#6	8'-5"	
h <sub>42</sub>	72	#8	15'-0"	
h <sub>44</sub>	16	#5	25'-5"	
h <sub>46</sub>	12	#6	8'-5"	
h <sub>48</sub>	72	#8	15'-0"	
h <sub>50</sub>	16	#5	25'-5"	

Class X Concrete	Cu Yds.	146.5
Reinforcement Bars	Lbs.	1710
Form (Plan)	Sq. Ft.	770
Test Pile (BDP-36)	Co.	1

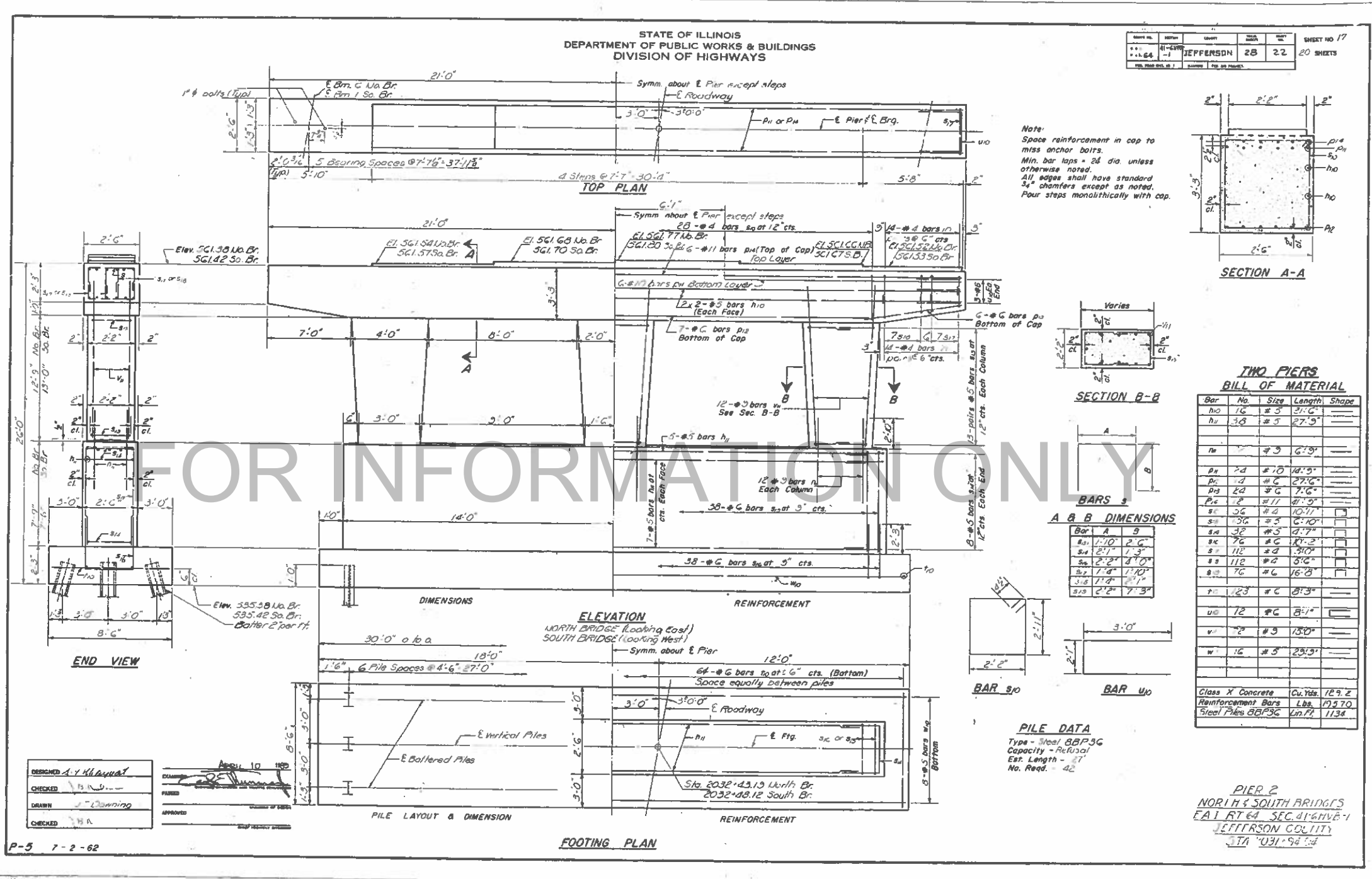
**PILE DATA**

Type - BDP-36  
Capacity - Refusal  
Est. Length - 22'  
No. Req. - 36 including  
Test Pile - 1 @ the So Br.

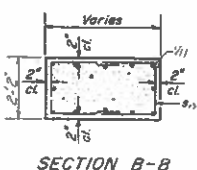
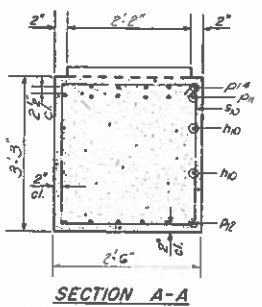
PIER 1  
NORTH & SOUTH BRIDGES  
TAI RT 64 SEC. 41-6HVB-1  
JEFFERSON COUNTY  
STA 2031+74.0'

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

PROJECT NO.	41-4378	CONTRACT NO.	JEFFERSON 28	SHEET NO.	22	TOTAL SHEETS	20
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Note:  
Space reinforcement in cap to miss anchor bolts.  
Min. bar laps = 24 dia unless otherwise noted.  
All edges shall have standard 3/4" chamfers except as noted.  
Pour steps monolithically with cap.



TWO PIERS  
BILL OF MATERIAL

Bar No.	Size	Length	Shape
h0	#5	31'-0"	—
h1	#5	27'-5"	—
h2	#5	6'-9"	—
h3	#10	14'-5"	—
h4	#6	27'-6"	—
h5	#6	7'-6"	—
h6	#11	4'-5"	—
h7	#4	10'-11"	—
h8	#5	6'-10"	—
h9	#5	4'-7"	—
h10	#6	11'-2"	—
h11	#6	3'-0"	—
h12	#6	3'-6"	—
h13	#6	16'-8"	—
h14	#6	8'-3"	—
h15	#6	8'-1"	—
h16	#5	13'-0"	—
h17	#5	23'-3"	—
h18	#5	23'-3"	—

A & B DIMENSIONS

Bar	A	B
h1	1'-10"	2'-0"
h2	2'-1"	1'-3"
h3	2'-2"	4'-0"
h4	1'-4"	1'-10"
h5	1'-8"	1'-11"
h6	2'-2"	7'-3"

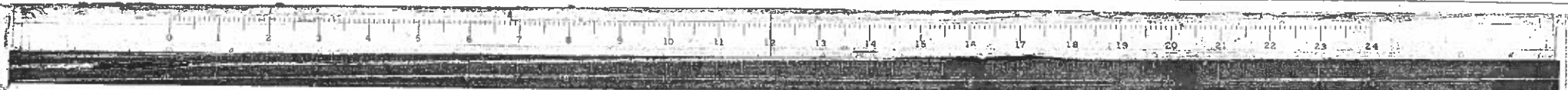
PILE DATA  
Type - Steel 88P36  
Capacity - Refusal  
Exp. Length - 27'  
No. Reqd. - 42

PIER 2  
NORTH & SOUTH BRIDGES  
EAT RT 64 SEC. 31-61N-E-1  
JEFFERSON COUNTY  
STA 2031+94.5

DESIGNED BY: A. V. KILPATRICK  
CHECKED BY: B. D. ...  
DRAWN BY: J. ...  
CHECKED BY: ...

APPROVED BY: ...  
DATE: APRIL 10 1962

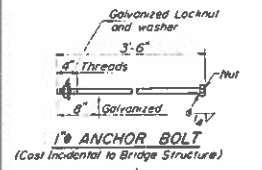
P-5 7-2-62



STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

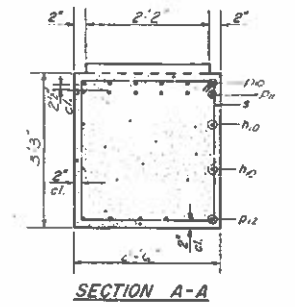
PROJECT NO.	SECTION	COUNTY	DATE	SHEET NO.
11-64	JEFFERSON	28	23	20 SHEETS

Note  
Bolts are to be placed nearest  
face toward approaching traffic  
at each end of pier

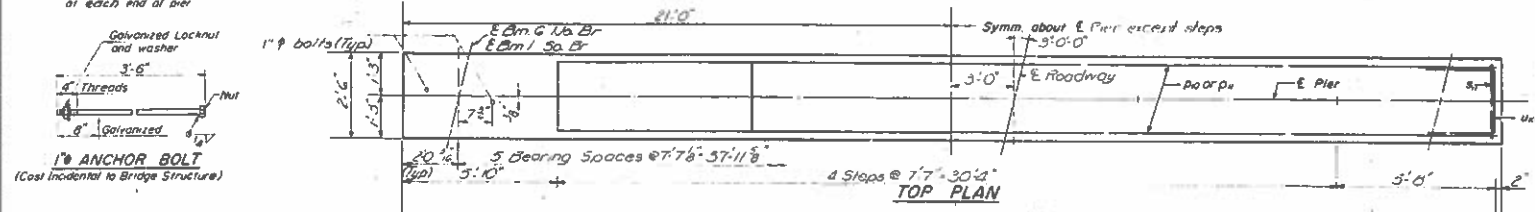


1" ANCHOR BOLT  
(Cost incidental to Bridge Structure)

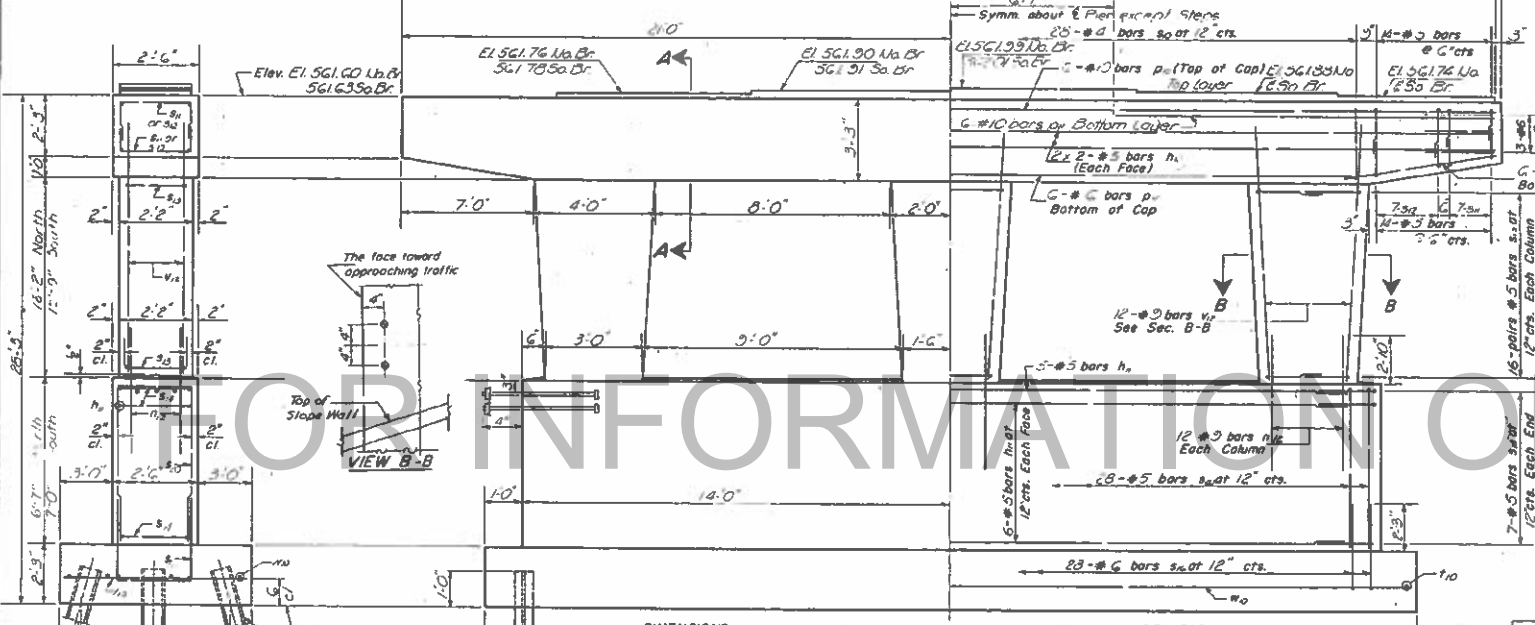
Note  
Space reinforcement in cap to  
miss anchor bolts.  
Min. bar laps = 24 dia. unless  
otherwise noted.  
All edges shall have standard  
3/8" chamfers except as noted.  
Pour steps monolithically with cap.



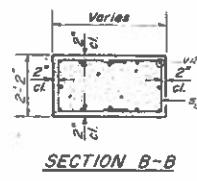
SECTION A-A



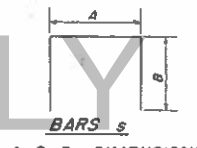
TOP PLAN



ELEVATION



SECTION B-B

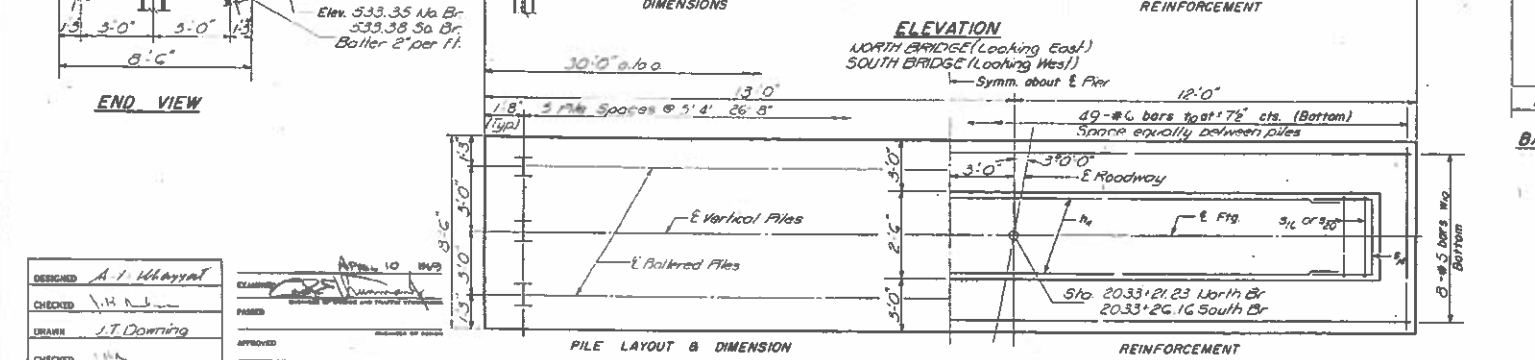


A & B DIMENSIONS

Bar	A	B
s1	2'2"	1'0"
s2	2'2"	2'1"
s3	1'10"	2'6"
s4	2'7"	1'3"
s5	2'2"	4'0"
s6	2'2"	6'8"

TWO PIERS  
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h1	16	#5	27'6"	
h2	34	#5	27'9"	
h3	72	#5	6'5"	
P10	12	#10	81'3"	
P11	24	#10	14'5"	
P12	12	#6	27'6"	
C1	20	#6	7'0"	
s1	36	#4	10'11"	
s2	36	#5	5'10"	
s3	36	#5	6'8"	
s4	192	#5	4'10"	
s5	28	#5	8'7"	
s6	36	#6	10'2"	
s7	36	#5	14'10"	
P13	98	#6	81'5"	
u1	12	#6	8'1"	
u2	72	#5	18'6"	
u3	16	#5	25'3"	
Class X Concrete				Cu. Yds. 130.0
Reinforcement Bars				Lbs. 18030
Steel Piles BBR3C				Lin. Ft. 875
Test Pile (BBP3C)				Co. 1



PILE LAYOUT & DIMENSION

FOOTING PLAN

DESIGNED	A. I. Klayman	APPROVED	[Signature]
CHECKED	[Signature]	PASSED	[Signature]
DRAWN	J. T. Downing	APPROVED	[Signature]
CHECKED	[Signature]		

PILE DATA  
Type - BBR3C  
Capacity - Refusal  
Est. Length - 25'  
No. Read. - 3% including  
Test Piles - 1 @ North Bridge

PIER 3  
NORTH & SOUTH BRIDGES  
FAI RT 64 SEC. 11 & 12-1  
JEFFERSON COUNTY  
STA 2031+74.04

P-5 7-2-62