LETTING 12A

DUPAGE AIRPORT AUTHORITY WEST CHICAGO, ILLINOIS

DU085

WEST TOWER ROAD WEST TOWER ROAD WANNAY 10-28 TAXIWAY G PROJECT LOCATION PROJECT LOCATION

FOR DUPAGE AIRPORT

PHASE 2: ECHO T-HANGAR
ASPHALT PAVEMENT REHABILITATION

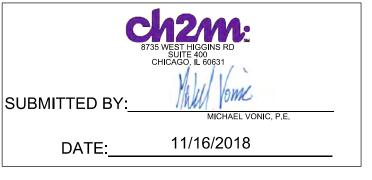
ILLINOIS PROJECT: DPA-4651
A.I.P. PROJECT: 3-17-SBGP-144
IDOT LETTING JANUARY 18, 2019
NOVEMBER 16, 2018

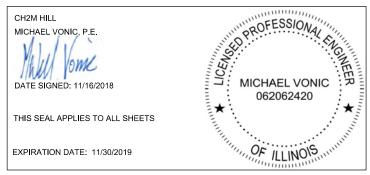
DuPage Airport Authority Ch2m:

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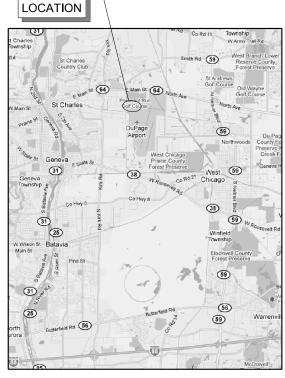




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SITE PLAN

PROJECT



LOCATION MAP

SUMMARY OF QUANTITIES

ITEM NO.	PAY ITEM NO.	DESCRIPTION	UNIT	QUANTITY
1	AR150520	MOBILIZATION	LS	1
2	AR150530	TRAFFIC MAINTENANCE	LS	1
3	AR152511	SUBGRADE REPAIR	SY	400
4	AR156500	TEMPORARY EROSION CONTROL	LS	1
5	AR156520	INLET PROTECTION	EA	15
6	AR201661	CLEAN & SEAL BITUMINOUS CRACKS	LF	284
7	AR401610	BITUMINOUS SURFACE COURSE	TON	1,050
8	AR401650	BITUMINOUS PAVEMENT MILLING	SY	9,610
9	AR401910	REMOVE & REPLACE BIT. PAVEMENT	SY	193
10	AR603510	BITUMINOUS TACK COAT	GAL	1,450
11	AR620520	PAVEMENT MARKING - WATERBORNE	SF	1,481

ABBREVIATIONS:

AGGREGATE BASE

AB ABV A/C AC AGG AGS ALD ALSF ARFF AS ABOVE ACCESS CONTROL ACRES ADJUST AGGREGATE AUXILIARY GAS VALVE AIRFIELD LIGHTING DUCT APPROACH LIGHTING SEQUENCE FLASHERS AIRPORT RESCUE AND FIRE FIGHTING AERIAL SURVEYS ASPHALT
AMERICAN SOCIETY FOR TESTING AND MATERIALS ASTM AUX AUXILIARY AVENUE AIR TRAFFIC CONTROL TOWER AVE. ATCT AXIS OF ROTATION BACK TO BACK **BOULEVARD**

BLVD BV BALL VALVE CL CB CC CED CENTERLINE CATCH BASIN

CENTER TO CENTER
COMBINED / COMMON ELECTRICAL DUCTBANK CI CMH

COMMUNICATION MANHOLE CORRUGATED METAL PIPE COMMONWEALTH EDISON COMED COMM COMMUNICATIONS CONC CONCRETE CUBIC YARD

CSPN CARGO SITE PREP NORTH DUPAGE AIRPORT AUTHORITY

DAA DET DIA. DIP DETAIL

DUCTILE IRON PIPE DISTANCE MEASURING EQUIPMENT DME DEPARTMENT OF AVIATION

EDGE DRAIN ED or E.D. ELECTRIC MANHOLE

EL or ELEV ELEVATION ESVCP EXTRA STRONG VITRIFIED CLAY PIPE

ABBREVIATIONS:

FAA FFM FEDERAL AVIATION ADMINISTRATION FAR FIELD MONITOR

FH FT FIRE HYDRANT

G GRND GRADE GROUND GLIDE SLOPE

HR HV HANGAR ROAD HIGH VOLTAGE

IDOT ILLINOIS DEPARTMENT OF TRANSPORTATION INSTRUMENT LANDING SYSTEM

ILS IE or I.E.

INVERT ELEVATION IDENTIFICATION ID iH INV.

JUNCTION CHAMBER JC JV JOINT VALVE

ΚV KILOVOLT LENGTH OF VERTICAL CURVE/ALGEBRAIC DIFFERENCE IN GRADE K=L/A

LENGTH LUMP SUM LOCALIZER L.S. LOC MAX MAXIMUM

MH MIN MANHOLE MINIMUM MJ MPR MECHANICAL JOINT MOUNT PROSPECT ROAD

NORTH NAVD NORTH AMERICAN VERTICAL DATUM

N.C. N.O. NORMALLY CLOSED NORMALLY OPEN N.I.C. NOT IN CONTRACT

NTS NOT TO SCALE NO NUMBER NORTH PERIMETER ROAD NPR

O.D. OFA OZ **OUTSIDE DIAMETER** OBJECT FREE AREA

ABBREVIATIONS:

PRECISION APPROACH PATH INDICATOR LIGHTS POINT OF CURVATURE
POINT OF COMPOUND CURVATURE PC PCC PORTLAND CEMENT CONCRETE PAVEMENT POINT OF INTERSECTION PCCP P.I.

PL PT PVC PROPERTY LINE
POINT OF TANGENCY POINT OF VERTICAL CURVATURE
POINT OF VERTICAL INTERSECTION PVI PVT PVC PIPE PSI POLYVINYL CHLORIDE PIPE

POUNDS PER SQUARE INCH

REMOVE

R RAD RC RCP RPP RPU RADIUS REINFORCED CONCRETE REINFORCED CONCRETE PIPE REINFORCED POLYPROPYLENE REMOTE PROCESSING UNIT

REV. RGS RD. RSA REVISION RIGID STEEL

ROAD RUNWAY SAFETY AREA R/W RPZ RUNWAY RUNWAY PROTECTION ZONE

SA SCH SD SHT S SQR SANITARY SCHEDULE STORM DRAIN SHEET SOUTH SQUARE S.F. S.Y. STA ST ST SQUARE FEET SQUARE YARD STREET

STORM SEWER T.S. TSA T/W TDZ TYP TANGENT SPIRAL TAXIWAY SAFETY AREA TAXIWAY TOUCHDOWN ZONE

UD or U.D. UNDERDRAIN UNDERDRAIN MANHOLE

U.N.O. UNLESS NOTED OTHERWISE

VERT. VERTICAL VPI W VERTICAL POINT OF INTERSECTION WEST

WEST MASS GRADING WMG YARD

THIS DOCUMENT, AND THE IDEAS AND DESIGNS CH2M HILL AND IS NOT TO BE USED, IN WHOLE SUMMARY OF QUANTITIES AND ABBREVIATIONS CH2MHILL AS SHOWN VERIFY SCALE BAR IS ONE INCH ON

DATE

PROJ

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SHEET

11/16/2018

2 of 11

PLOT TIME: \$PLOTTIME

DPA-4651

G-101

DUPAGE AIRPORT AUTHORITY

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GENERAL NOTES

- 1. THIS PROJECT SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CONTRACT SPECIFICATIONS AND ANY RULES, REGULATIONS, STANDARDS AND SPECIFICATIONS REFERENCED THEREIN. THE PROJECT IS SUBJECT TO INSPECTION BY REPRESENTATIVES OF THE DUPAGE AIRPORT AUTHORITY (DAA).
- 2. THE PROJECT IS TO BE COMPLETED IN A TIMELY MANNER IN ACCORDANCE WITH THE CONTRACTOR'S APPROVED PROJECT SCHEDULE. THE SCHEDULE SHALL BE PROVIDED FOR COMPLETION OF THE WORK AS DESCRIBED IN THE CONTRACT SPECIFICATIONS. THE CONTRACTOR SHALL KEEP DAA INFORMED OF UPDATES TO THE PROJECT SCHEDULE AT ALL TIMES.
- 3. THE CONTRACTOR SHALL COMPLETE THE ENTIRE PROJECT WITHIN THE TIME STATED IN THE CONTRACT.
- 4. CONSTRUCTION AND MAINTENANCE OPERATIONS BY OTHERS MAY OCCUR CONCURRENTLY AND AT TIMES IN THE VICINITY OF CONSTRUCTION ASSOCIATED WITH THIS PROJECT. THE CONTRACTOR SHALL COORDINATE HIS OPERATIONS AND COOPERATE WITH MAINTENANCE CREWS AND OTHER CONTRACTORS. CONTRACTOR COORDINATION WITH APPROPRIATE GOVERNMENT AND UTILITY AGENCIES IS ALSO REQUIRED PRIOR TO AND DURING CONSTRUCTION.
- 5 ACCESS TO THE SITE THE CONTRACTOR'S MAY BE ASKED TO ACCESS THROUGH DESIGNATED SECURITY GATES AS SHOWN ON THE PLANS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL VEHICLES AND PERSONNEL THAT ENTER THROUGH THESE DESIGNATED AIRPORT SECURITY GATES. AIRPORT SECURITY GATES SHALL BE SECURED AT ALL TIMES WHEN NOT IN USE.
- 6. HAUL ROUTES THE CONTRACTOR'S HAUL ROUTES AND CONSTRUCTION ACCESS SHALL BE EXISTING HAUL ROADS WHERE AVAILABLE. ANY DEBRIS (WHETHER CAUSED BY THE CONTRACTOR OR NOT) SHALL BE REMOVED IMMEDIATELY BY THE CONTRACTOR. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS, OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE ROADS USED AS HAUL ROUTES/CONSTRUCTION ACCESS SHALL BE MAINTAINED AND STABILIZED BY THE CONTRACTOR AS REQUIRED TO SUPPORT CONSTRUCTION EQUIPMENT THROUGHOUT THE DURATION OF THIS PROJECT AND SHALL BE RESTORED, AT THE CONTRACTOR'S EXPENSE, TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE/CONSTRUCTION ACCESS UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER. THE BEFORE AND AFTER CONDITION OF ON-SITE HAUL ROUTES/CONSTRUCTION ACCESS SHALL BE JOINTLY INSPECTED AND AGREED UPON BY THE CONTRACTOR AND THE RESIDENT ENGINEER. FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT/REMOVE TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE RESIDENT ENGINEER PRIOR TO COMMENCING THE WORK
- 7. CONTRACTOR'S STAGING AREAS AN AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE AS SHOWN ON THE PLANS. THE CONTRACTOR SHALL PROTECT EXISTING UTILITIES, FENCE, TREES, ETC. WITHIN THE STAGING AREA AND THE SITE SHALL BE RESTORED TO ITS ORIGINAL CONDITION WITHIN THIRTY (30) DAYS OF THE COMPLETION OF USE. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION SHALL BE ALLOWED.
- 8. DISPOSAL AREAS EXCESS SOILS, MILLED ASPHALT, CONCRETE RUBBLE AND UNSUITABLE EXCAVATION MUST BE DISPOSED OF OFFSITE. OFFSITE DISPOSAL AREAS SHALL BE IN ACCORDANCE WITH LOCAL, STATE AND FEDERAL LAWS.
- 9. SAFETY THE CONTRACTOR SHALL CONDUCT ALL ACTIVITIES IN A SAFE MANNER AS SPECIFIED BY LOCAL, STATE AND FEDERAL LAWS
- 10. PROTECTION AND REPAIR OF EXISTING UTILITIES THE CONTRACTOR SHALL VERIFY ALL UTILITIES, BY VISUAL, ELECTRICAL AND BY HAND EXCAVATION OR OTHER METHODS IN COORDINATION WITH ALL UTILITY COMPANIES AND DAA FACILITIES, PRIOR TO BEGINNING ANY CONSTRUCTION OPERATIONS. ANY AND ALL CONFLICTS OF EXISTING UTILITIES BY THE PROPOSED IMPROVEMENTS SHALL BE RESOLVED WITH THE RESIDENT ENGINEER PRIOR TO BEGINNING THE CONSTRUCTION ACTIVITIES. ANY REPAIRS TO EXISTING UTILITIES DAMAGED BY CONTRACTOR ACTIVITIES SHALL BE COMPLETED IMMEDIATELY BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER. ALL SUCH REPAIRS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS AND SHALL BE AT THE CONTRACTOR'S EXPENSE. IF FAA CABLES ARE DAMAGED, REPAIRS SHALL BE DONE IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF AN FAA REPRESENTATIVE. THE FAA MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS, IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS AT NO ADDITIONAL EXPENSE TO THE

- 11 CONSTRUCTION LIMITS ALL CONTRACTOR VEHICLES AND TRAFFIC (LINI ESS OTHERWISE AUTHORIZED) SHALL REMAIN WITHIN THE DESIGNATED STAGING AREA CONSTRUCTION LIMITS AND HAUL ROUTES.
- 12. THE CONTRACTOR SHALL OBTAIN ALL PERMITS AND LICENSES REQUIRED FOR THE CONSTRUCTION WORK. REFER TO THE SPECIAL CONDITIONS AND GENERAL CONDITIONS SECTIONS OF THE CONTRACT DOCUMENTS
- 13. EXISTING TOPOGRAPHIC. UNDERGROUND AND ABOVE GROUND UTILITY DATA FOR THIS PROJECT WERE OBTAINED FROM HISTORICAL DATA, DESIGN RECORDS, DAA REFERENCES AND PREVIOUS PROJECT "AS-BUILTS". CONTRACT IS RESPONSIBLE FOR VERIFYING ALL UTILITY INFORMATION.
- 14. IF ANY UNUSUAL ODORS, SOIL STAINS OR BURIED WASTES ARE ENCOUNTERED, STOP WORK IMMEDIATELY AND NOTIFY THE OWNER. AND/OR RESIDENT ENGINEER
- 15. WORK PERFORMED UNDER THIS CONTRACT WILL NOT BE CONSIDERED COMPLETE UNTIL FINAL ACCEPTANCE OF THE PROJECT BY THE OWNER AND RECEIPT AND APPROVAL OF THE FOLLOWING DOCUMENTS:
- A) CONTRACTOR'S WAIVER AND RELEASE OF LIEN.
- B) ABSOLUTE BILL OF SALE.
- C) CONTRACTOR'S LETTER OF WARRANTY (I.E., LETTER AGREEMENT).
- D) CONTRACTOR AS-BUILTS.
- E) FINAL PUNCH LIST COMPLETED AND ACCEPTED BY THE RESIDENT ENGINEER.
- F) TRANSFER OF OWNER WARRANTY
- 16. IN ACCORDANCE WITH THE CONTRACT DOCUMENTS, PREVAILING WAGE RATES SHALL BE POSTED AT THE CONTRACTOR STAGING AREA IN A WEATHERPROOF ENCLOSURE AND COPIES SHALL BE SUBMITTED TO THE RESIDENT ENGINEER AND THE OWNER.

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- 17 IF BITUMINOUS PAVEMENT LOCATIONS, AFTER BEING MILLED OR DURING THE MILLING PROCESS BEGIN TO FAIL UNDER CONSTRUCTION TRAFFIC. THE RESIDENT ENGINEER WILL DESIGNATE AREAS OF COMPLETE PAVEMENT STRUCTURE UNDERCUT AND FULL DEPTH PAVEMENT RECONSTRUCTION REPLACEMENT WITH A NEW PAVEMENT SECTION AS SHOWN IN THE PLANS. IF ANY UNSUITABLE MATERIALS ARE DISCOVERED DURING THE MILLING PROCESS PER THE RESIDENT ENGINEER, IT SHALL BE UNDERCUT AND REMOVED BY THE CONTRACTOR AND REPLACED IN ACCORDANCE WITH THE PLAN.
- 18. CONTRACTOR SHALL GRADE AND COMPACT ALL EXISTING AGGREGATE BASES TO THE SATISFACTION OF THE RESIDENT ENGINEER PRIOR TO PAVING. (INCIDENTAL TO THE
- 19. CONTRACTOR SHALL CONSTRUCT ALL IMPROVEMENTS TO DRAIN OFF OF NEWLY CONSTRUCTED PAVEMENTS. ANY AREAS OF NEW PAVEMENT THAT ARE FOUND TO NOT DRAIN PROPERLY BY THE RESIDENT ENGINEER SHALL BE REMOVED AND REPLACED AT THE CONTRACTORS EXPENSE
- 20. THE AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT. COORDINATION OF WORK WITH THE DAA, FAA, IDOT AND LOCAL ATCT REPRESENTATIVES IS MANDATORY TO MINIMIZE IMPACTS ON AIRPORT OPERATIONS.
- 21. THE PAVEMENT SURFACE SHALL BE CLEAN AND DRY PRIOR TO MARKING.

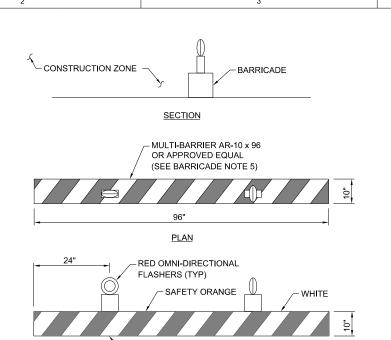
GENERAL PROJECT DESCRIPTION

- 1. THE WORK GENERALLY CONSISTS OF:
 - MILLING OF EXISTING PAVEMENT AREA AS SHOWN IN THE PROJECT DOCUMENTS
 - DISPOSE OF EXISTING ASPHALT MILLINGS
 - PLACEMENT OF BITUMINOUS SURFACE COURSE
 - OTHER WORK ITEMS AS SHOWN IN THE PROJECT DOCUMENTS

SECURITY NOTES

- 1. THE CONTRACTOR SHALL DESIGNATE TO THE RESIDENT ENGINEER IN WRITING THE NAME OF ITS 'CONTRACTOR SECURITY OFFICER' (C.S.O.). THE C.S.O. SHALL REPRESENT THE CONTRACTOR ON THE SECURITY REQUIREMENTS FOR THE CONTRACT. IT IS THE CONTRACTORS RESPONSIBILITY TO FAMILIARIZE HIMSELF/HERSELF WITH THE VARIOUS ASPECTS OF FAR 49 CFR PART 1542: "AIRPORT SECURITY", ADMINISTERED BY THE TRANSPORTATION SECURITY ADMINISTRATION (TSA). ANY VIOLATION OF CFR PART 1542 BY THE CONTRACTOR AND ANY SUBSEQUENT FINES IMPOSED DUE TO THE VIOLATION SHALL BE THE RESPONSIBILITY OF
- 2. CONTRACTOR PERSONNEL SECURITY ORIENTATION: THE C.S.O. SHALL BE RESPONSIBLE FOR BRIEFING ALL CONTRACTOR PERSONNEL ON THESE REQUIREMENTS AND PROVISIONS ADOPTED BY THE DUPAGE AIRPORT AUTHORITY (DAA). ALL NEW CONTRACTOR EMPLOYEES SHALL BE BRIEFED ON THESE REQUIREMENTS PRIOR TO WORKING IN THE CONSTRUCTION
- 3. ACCESS TO SITE: THE AIRPORT WILL NOT PROVIDE AIRFIELD OPERATIONS AREA ESCORTS. CONTRACTOR'S ACCESS TO THE SITE SHALL BE AS SHOWN ON THE SAFETY PLAN OR AS DIRECTED BY THE RESIDENT ENGINEER. NO OTHER ACCESS POINT SHALL BE ALLOWED UNLESS APPROVED BY THE RESIDENT ENGINEER. IF GATE IS TO BE LEFT UNLOCKED. CONTRACTOR SHALL PROVIDE GATE GUARDS FOR THE DURATION OF THE UNLOCKED CONDITION. ALL CONTRACTOR TRAFFIC AUTHORIZED TO ENTER THE SITE SHALL BE EXPERIENCED IN THE ROUTE OR ESCORTED BY THE CONTRACTOR PERSONNEL. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TRAFFIC CONTROL TO AND FROM THE ACCESS GATE TO THE SITE. A CONTRACTOR'S FLAGMAN OR TRAFFIC CONTROL PERSON SHALL MONITOR AND COORDINATE ALL CONTRACTOR TRAFFIC AT THE ACCESS GATE. THE CONTRACTOR SHALL NOT PERMIT ANY UNAUTHORIZED CONSTRUCTION PERSONNEL OR TRAFFIC ON THE AIRPORT OPERATIONS AREA (AOA). DIRECTIONAL SIGNING ALONG THE DELIVERY ROUTE TO THE STORAGE AREA OR WORK SITE SHALL BE AS DIRECTED BY THE RESIDENT ENGINEER.
- 4. CONSTRUCTION AREA LIMITS: THE LIMITS OF CONSTRUCTION, MATERIAL STORAGE AREAS, PARKING AREA AND OTHER AREAS DEFINED FOR THE CONTRACTOR'S EXCLUSIVE USE DURING CONSTRUCTION SHALL BE MARKED AND LIGHTED. THE CONTRACTOR SHALL ERECT AND MAINTAIN FENCING AROUND THE PERIMETER OF THESE AREAS AND VISIBLE DEVICES FOR DAY/NIGHT USE. TEMPORARY BARRICADES, FLAGGING AND FLASHING WARNING LIGHTS WILL BE REQUIRED AT CRITICAL ACCESS POINTS. THE TYPE OF MARKING AND WARNING DEVICES SHALL BE APPROVED BY THE RESIDENT ENGINEER AND AIRPORT OPERATIONS
- 5. CONTRACTOR SHALL PROVIDE OPERATIONS AND THE RESIDENT ENGINEER A CURRENT LIST OF COMPANIES AUTHORIZED TO CONDUCT WORK ON THE AIRPORT. CONTRACTOR EMPLOYEE PERSONAL VEHICLES SHALL BE RESTRICTED TO THE CONTRACTORS EMPLOYEE PARKING AREA AND ARE NOT ALLOWED ON THE AIRFIELD AT ANY TIME.

## STATE OF THE CONTRACT # BU085 CHICAGO, IL. 60631			. 0	JULAGE	AUTH			
## SUTTE 400 CHICAGO, IL 60831 PHASE 2: ECHO T-HANGAR ASPHALT PAVEMENT REHABILITATION CONTRACT #DU085 NERAL NOTES ###						APVD		
### SUITE 400 CHICAGO, IL 60631 PHASE 2: ECHO THANGAR ASPHALT PAVEMENT REHABILITATION NO. DATE PARE CONTRACT #BU085 DEGN MFMA NERAL NOTES R330F MFMA T 1/1/16/2018 R53UED FOR BID R61K MFMA DR R61K MW MM MM MM MM MM MM MM MM M						В		
### SUITE 400 CHICAGO, IL 60631 PHASE 2: ECHO T-HANGAR ASPHALT PAVEMENT REHABILITATION CONTRACT #DU085 DSGN MEMA MEMA NERAL NOTES ###################################							APVD	- AW
SUITE 400 CHICAGO, IL 60631 PHASE 2: ECHO T-HANGAR ASPHALT PAVEMENT REHABILITATION CONTRACT #DU085 DSGN MEMAN NERAL NOTES R35 W. HIGGINS ROAD SUITE 400 CHICAGO, IL 60631 1 11/16/2018 NO. DATE DSGN MEMAN					ISSUED FOR BID	REVISION	CHK	Z Y
WHILL® SUITE 400 CHICAGO, IL 60631 PHASE 2: ECHO THANGAR ASPHALT PHASE 2: ECHO THANGAR ASPHALT NERAL NOTES NO. DATE DSGN							2	
WHILL® SUITE 400 CHICAGO, IL 60631 PHASE 2: ECHO T-HANGAR ASPHALT PAVEMENT REHABILITATION NO. DSGN					/16/2018	DATE	<u>I</u>	ME/MV
B735 W. HIGGINS ROAD SUITE 400 CHICAGO, IL 60631 CHICAGO, IL 60631 PHASE 2: ECHO T-HANGAR ASPHALT PAVEMENT REHABILITATION CONTRACT #BU085	+				-		SGN	
MHILL NERAL NOTES	8735 W. HIGGINS RO/ SUITE 400	CHICAGO, IL 60631			PHASE 2: ECHO T-HAN	PAVEMEN I KEHA		
			GENERAL NOTES					
DATE 11/16/2018			_					
PROJ DPA-4651 DWG G-102			_	_				



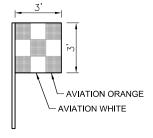


ELEVATION

6" TO 12" WIDE STRIPES AT 45° ANGLE

BARRICADE NOTES:

- 1. ALL AIRFIELD PAVEMENT CLOSURES SHALL BE TEMPORARY CLOSURES AS DESCRIBED IN FAA AC 150/5370-2G, "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION." THE CONTRACTOR SHALL COMPLY WITH ALL THE REQUIREMENTS SET FORTH IN THE CONTRACT DOCUMENTS REGARDING AIRFIELD CLOSURES, SAFETY, AND SECURITY.
- 2. PRIOR TO REOPENING CLOSED AIRFIELD PAVEMENTS, THE ENTIRE SAFETY AREA MUST MEET FAA CRITERIA UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER.
- 3. FLASHERS TO BE BATTERY OPERATED AND/OR SOLAR POWERED. LENS TO BE RED AND BE ABLE TO ROTATE 90°.
- 4. FACING OF LOW-PROFILE BARRICADE TO BE COVERED WITH REFLECTIVE MATERIAL.
- 5. LOW-PROFILE BARRICADES TO BE PLACED END TO END ALONG OPERATIONAL PAVEMENT, ADJACENT TO CONSTRUCTION, AS DIRECTED BY THE
- 6. FLASHERS SHALL BE SECURED TO THE BARRICADES, PER MANUFACTURER'S INSTRUCTIONS. ALTERNATE FLASHER LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
- 7. LOW-PROFILE BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT'S COMPONENTS, AND WEIGHTED OR STURDILY ATTACHED TO THE SURFACE. IF AFFIXED TO THE SURFACE, THE BARRICADE MUST BE FRANGIBLE AT GRADE LEVEL OR AS LOW AS POSSIBLE, NOT TO EXCEED 3 INCHES ABOVE THE GROUND.
- 8. THE CONTRACTOR SHALL PROVIDE BARRICADES WITH RED FLASHING BATTERY OPERATED LIGHTS AS SHOWN IN GOOD WORKING ORDER. CONTRACTOR SHALL MAINTAIN ALL BARRICADES AND ENSURE THEY ARE IN WORKING ORDER TWENTY-FOUR (24) HOURS A DAY, FOR THE DURATION OF THE PROJECT. THE BARRICADE LIGHTING SHALL BE CHECKED NIGHTLY BY THE CONTRACTOR.
- 9. THE LIGHTS SHALL BE OPERATIONAL AT EACH BARRICADE LOCATION AT ALL TIMES. ANY LIGHTS FOUND NON-OPERATIONAL SHALL BE REPAIRED IMMEDIATELY WITH EMERGENCY CONTACT NUMBER 24/7.
- 10. ALL BARRICADES SHALL BE CHECKED VISUALLY FOR SIGNS OF WEAR AND TEAR ON A DAILY BASIS AND SHALL BE REPAINTED OR REPLACED WHEN DEEMED APPROPRIATE BY THE RESIDENT ENGINEER. THE CONDITIONS OF LIGHTING UNITS SHALL BE CHECKED DAILY. ALL LIGHT FIXTURES SHALL BE VERIFIED IN OPERATING CONDITION AND GOOD WORKING ORDER BY THE CONTRACTOR ON A DAILY BASIS, BEFORE THE CONTRACTOR CEASES OPERATIONS FOR THE DAY.



VEHICLE FLAG

DUPAGE AIRPORT AUTHORITY ΒY THIS DOCUMENT, AND THE IDEAS AND DESIGNS CH2M HILL AND IS NOT TO BE USED, IN WHOLE CH2MHILL AS SHOWN VERIFY SCALE BAR IS ONE INCH ON DATE 11/16/2018 PROJ DPA-4651 DWG G-104 SHEET 5 OF 11

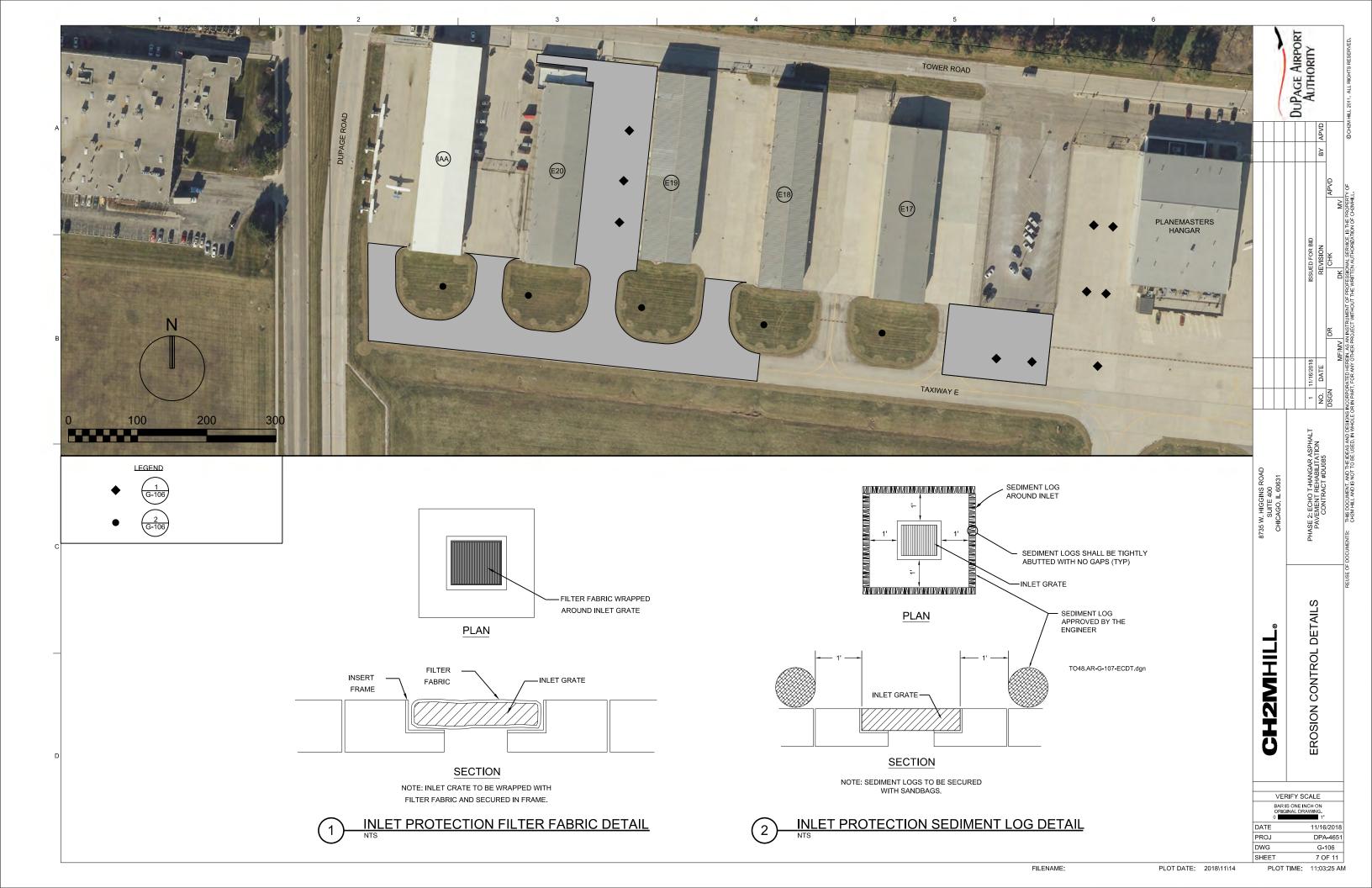
EROSION CONTROL NOTES

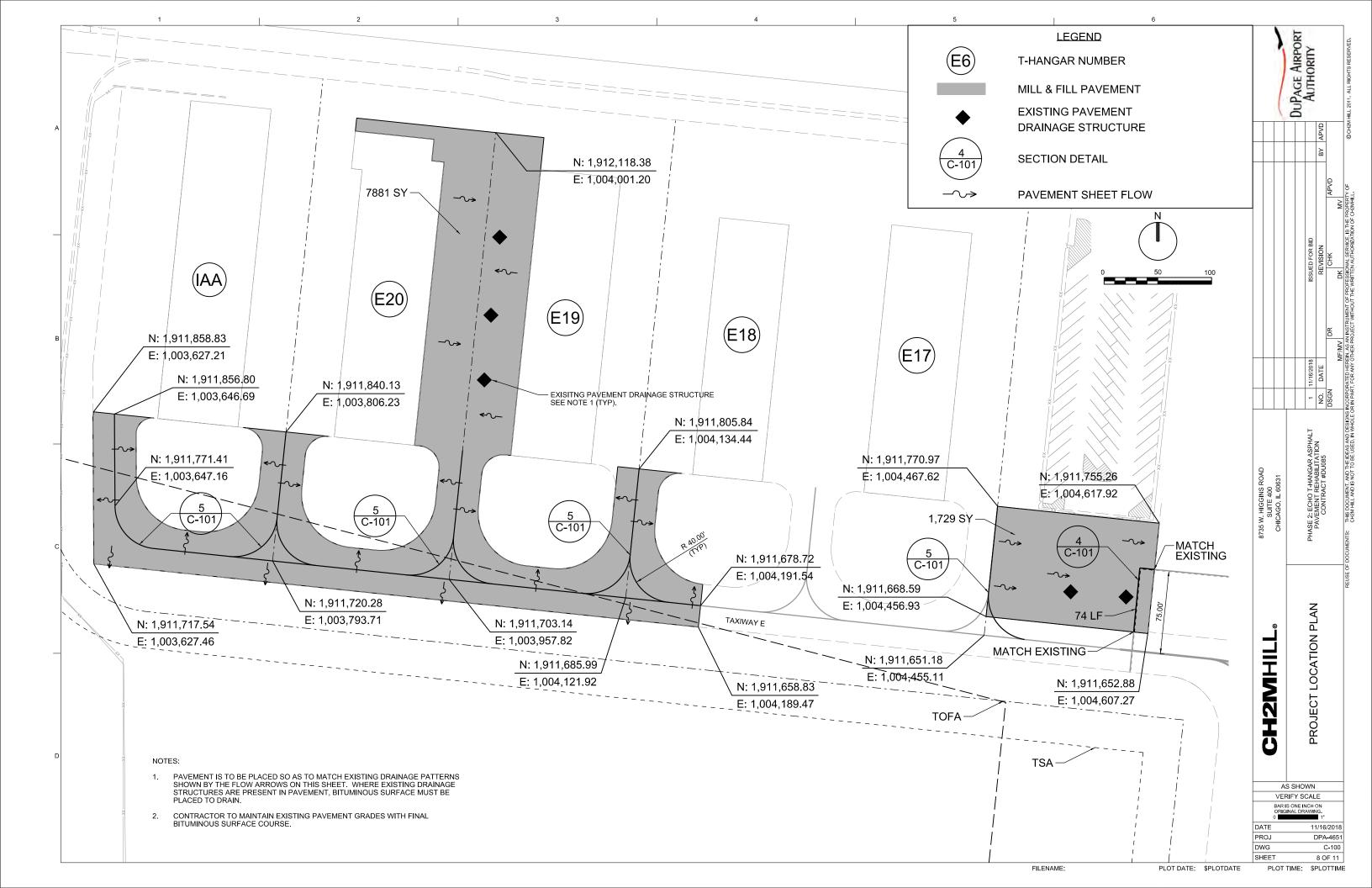
- THE EROSION CONTROL SHOWN ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED AS DIRECTED BY THE RESIDENT ENGINEER. ALL ADDITIONAL MEASURES MUST BE IN PLACE WITHIN 3 DAYS OF DISTURBANCE.
- THE CONTRACTOR MUST CLEAN UP, GRADE THE WORK AREA AS THE PROJECT PROGRESSES, AND INSTALL EROSION PROTECTION TO ELIMINATE THE CONCENTRATION OF RUNOFF, OR MUST INSTALL APPROPRIATE SEDIMENT CONTROL DEVICES TO TRAP SEDIMENT. PAVEMENT MUST BE CLEANED DAILY OR AS NECESSARY TO REMOVE EARTHEN MATERIAL TO THE SATISFACTION OF THE RESIDENT
- ALL EROSION CONTROL MEASURES MUST BE KEPT OPERATIONAL AND MAINTAINED CONTINUOUSLY THROUGHOUT THE PERIOD OF LAND DISTURBANCE OR UNTIL CONTRACT IS COMPLETED.
- THE CONTRACTOR MUST MAKE INSPECTIONS A MINIMUM OF ONCE EVERY SEVEN DAYS OF ALL EROSION CONTROL MEASURES. AN ADDITIONAL INSPECTION OF EROSION CONTROL MEASURES MUST BE MADE WITHIN TWENTY-FOUR (24) HOURS OF A 0.5-INCH OR GREATER RAINFALL OR EQUIVALENT SNOWFALL.
- THE CONTRACTOR MUST KEEP A WRITTEN REPORT SUMMARIZING THE REQUIRED INSPECTION EACH TIME AN INSPECTION TAKES PLACE. THE REPORTS MUST BE KEPT AT THE SITE DURING CONSTRUCTION.
- THE CONTRACTOR MUST COOPERATE WITH THE DAA REPRESENTATIVES WHO WILL MAKE SITE VISITS TO REVIEW COMPLIANCE WITH THE PLAN IN THE FIELD AND AUDIT THE CONTRACTOR LOGS AND RECORDS REQUIRED BY THE PERMIT.
- 7. ALL STORM SEWER INLET STRUCTURES MUST BE PROTECTED WITH STORM SEWER INLET PROTECTION (I.E. INLET FILTERS) PER INLET PROTECTION DETAILS IN THE PLANS.
- 8. THE CONTRACTOR MUST MAINTAIN AND PRESERVE ANY EXISTING SUB-SURFACE DRAINAGE SYSTEMS.
- CONSTRUCTION ACTIVITIES MUST BE SCHEDULED TO MINIMIZE THE TIME SOIL IS EXPOSED AND UNPROTECTED. IN NO CASE WILL THE EXISTING VEGETATION BE DESTROYED, REMOVED, OR DISTURBED MORE THAN FOURTEEN (14) DAYS PRIOR TO THE
- 10. GRAVELED ROADS, ACCESS DRIVES, PARKING AREAS OF SUFFICIENT WIDTH AND LENGTH, AND VEHICLE WASH DOWN FACILITIES IF NECESSARY, MUST BE PROVIDED TO PREVENT THE DEPOSIT OF SOIL FROM BEING TRACKED ONTO PUBLIC OR PRIVATE ROADWAYS. ANY SOIL REACHING PUBLIC OR PRIVATE ROADWAY MUST BE REMOVED IMMEDIATELY.
- 11. STOCK PILES OF SOIL MUST NOT BE LOCATED IN FLOOD PLAINS, RIPARIAN AREAS (VEGETATED FLOOD PLAINS), WETLANDS AND WATERS OF THE U.S. UNLESS OTHERWISE AUTHORIZED BY THE RESIDENT ENGINEER. IF A STOCKPILE IS TO REMAIN IN PLACE FOR MORE THAN THREE DAYS, PERIMETER EROSION BARRIER MUST BE PROVIDED. IF THE STOCKPILE IS INACTIVE FOR MORE THAN 14 DAYS, SOIL STABILIZATION MUST BE PROVIDED BY THE 7TH DAY AFTER ACTIVITY HAS STOPPED. THE COST OF THIS WORK WILL BE INCLUDED IN THE TEMPORARY EROSION CONTROL PAY ITEM, AR156500.
- 12. INLET PROTECTION SHALL BE APPLIED TO ALL INLETS WITHIN THE PROJECT AREA IN THE VICINITY OF ACTIVE CONSTRUCTION.
- 13. INLET PROTECTION METHOD SHALL BE APPROVED BY THE RESIDENT ENGINEER.
- 14. INLET PROTECTION PLACEMENT SHALL BE INSPECTED AND APPROVED DAILY BY THE RESIDENT ENGINEER, AND THE CONTRACTOR SHALL MAINTAIN THE INLET PROTECTION TO THE SATISFACTION OF THE RESIDENT ENGINEER DAILY.
- 15. COST OF PLACEMENT AND MAINTENANCE OF INLET PROTECTION SHALL BE INCIDENTAL TO PAY ITEM NO. AR156520, INLET PROTECTION.

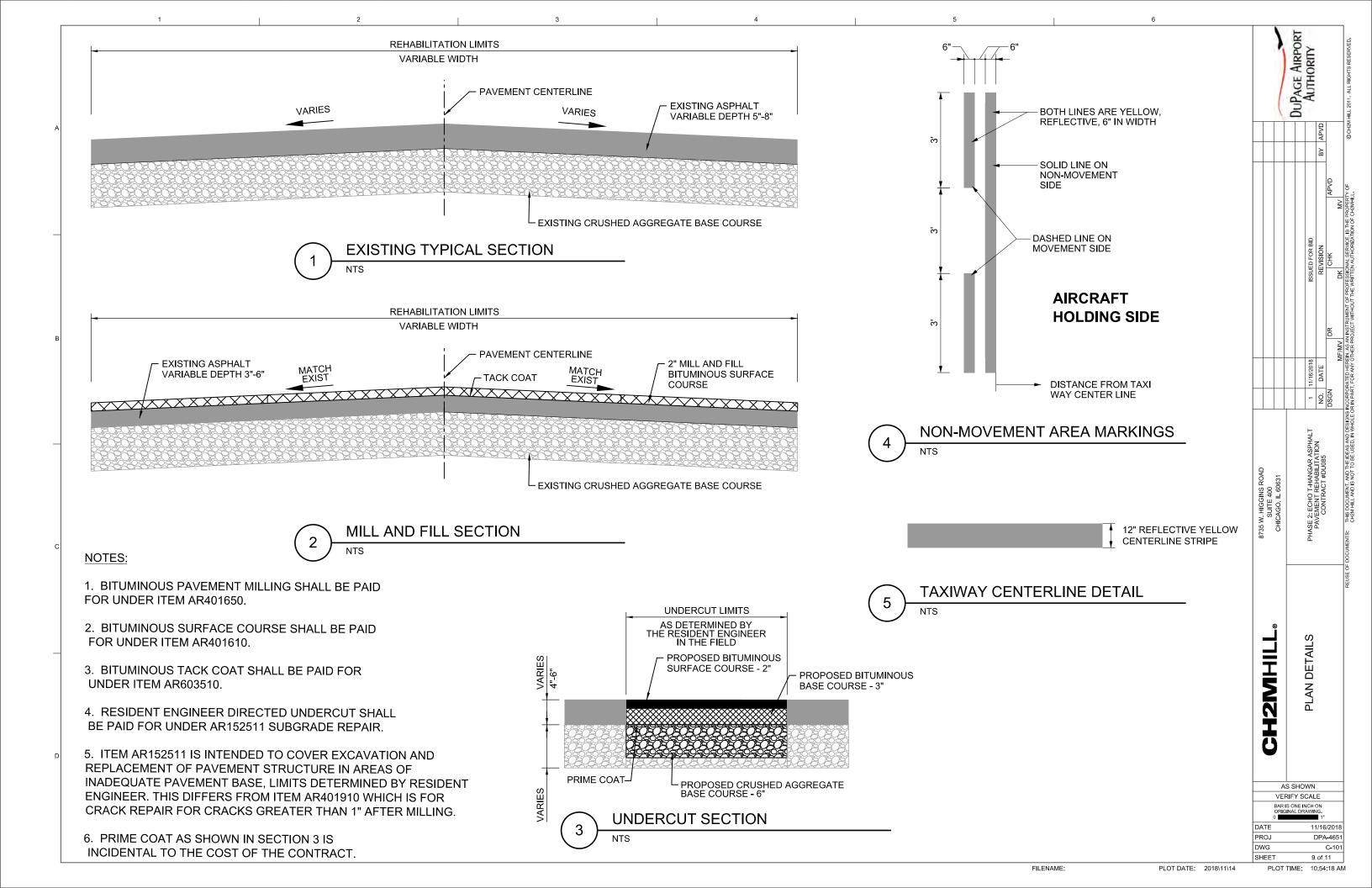
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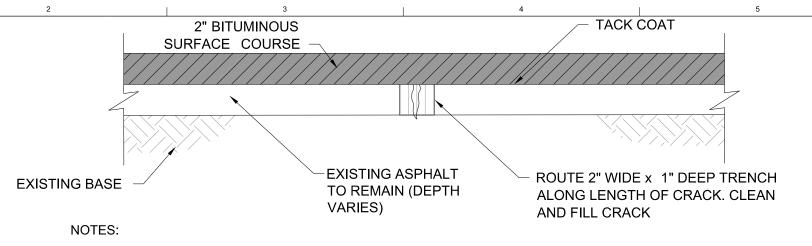
EROSION CONTROL NOTES

	VERIFY SCALE
	BAR IS ONE INCH ON ORIGINAL DRAWING. 0 1"
DATE	11/16/2018
PROJ	DPA-4651
DWG	G-105
SHEE	T 6 OF 11



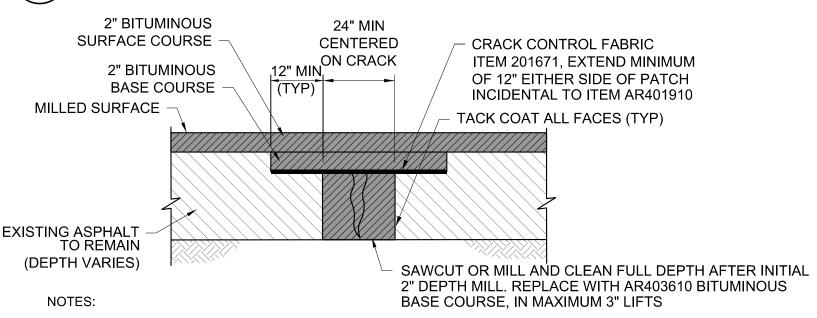






- 1. CRACKS SHALL BE FIELD LOCATED BY THE RESIDENT ENGINEER.
- 2. PAYMENT FOR CLEAN & SEAL BITUMINOUS CRACKS SHALL BE PER LINEAR FOOT UNDER ITEM AR201661, AND SHALL INCLUDE ALL WORK SHOWN IN THIS DETAIL BELOW THE LEVEL OF THE INITIAL 2" MILL. THIS INCLUDES SAW CUTTING, DETAIL MILLING, CLEANING, ROUTING, AND CRACK SEALANT. BITUMINOUS SURFACE COURSE WITH BITUMINOUS TACK COAT SHALL BE PAID FOR SEPARATELY UNDER AR401610 AND AR603510 RESPECTIVELY.

CLEAN & SEAL BITUMINOUS CRACKS - CRACKS GREATER THAN 1/2" WIDE BUT LESS THAN 1" WIDE AFTER MILL



- 1. CRACKS SHALL BE FIELD LOCATED BY THE RESIDENT ENGINEER.
- 2. PAYMENT FOR REMOVE & REPLACE BITUMINOUS PAVEMENT SHALL BE ONLY AT CRACKS GREATER THEN 1" AFTER MILL AND MEASURED PER SQUARE YARD UNDER ITEM AR401910, AND SHALL INCLUDE ALL WORK SHOWN IN THIS DETAIL BELOW THE LEVEL OF THE INITIAL 2" MILL EXCEPT FOR BITUMINOUS SURFACE COURSE. THIS INCLUDES SAW CUTTING, DETAIL MILLING, CLEANING AND CRACK CONTROL FABRIC. BITUMINOUS SURFACE COURSE WITH BITUMINOUS TACK COAT SHALL BE PAID FOR SEPARATELY UNDER AR401610 AND AR603510 RESPECTIVELY.
- 3. ITEM AR401910 IS INTENDED TO COVER CRACK REPAIR FOR CRACKS GREATER THAN 1" AFTER MILLING. THIS DIFFERS FROM ITEM AR152511 WHICH IS INTENDED TO COVER EXCAVATION AND REPLACEMENT OF PAVEMENT STRUCTURE IN AREAS OF INADEQUATE PAVEMENT BASE, LIMITS DETERMINED BY RESIDENT ENGINEER.

REMOVE & REPLACE BITUMINOUS PAVEMENT - CRACKS GREATER THAN 1" AFTER MILL

FILENAME:

PLOT DATE: 2018\11\14

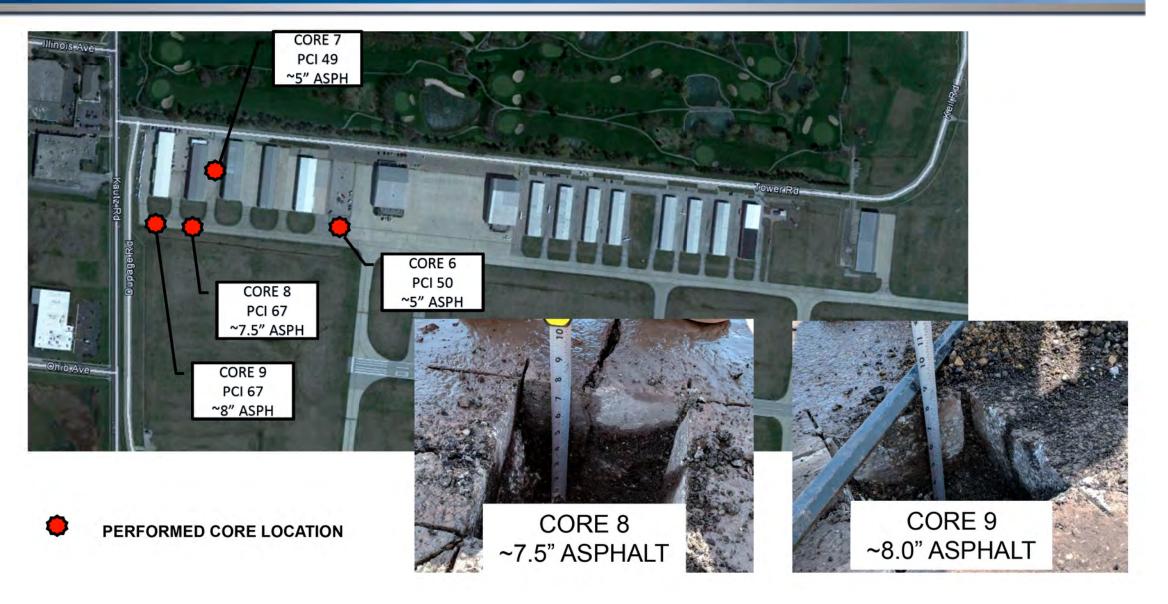
DWG C-102 SHEET PLOT TIME: 10:55:28 AM

11/16/2018

AS SHOWN VERIFY SCALE

DUPAGE AIRPORT AUTHORITY

Existing Pavement Cores



EXISTING PAVEMENT CORE DATA - FOR INFORMATION ONLY

CH2MHILL.