

CONSTRUCTION PLANS FOR CASEY MUNICIPAL AIRPORT

CASEY, CLARK COUNTY, ILLINOIS

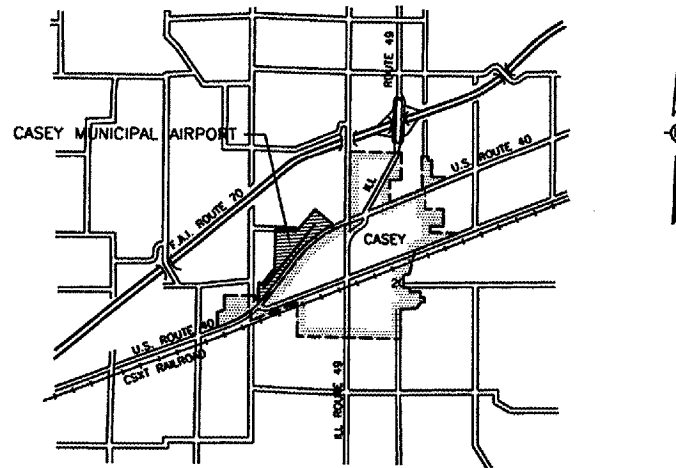
APPLY POROUS FRICTION COURSE AND MARK RUNWAY 4-22

| SUMMARY OF QUANTITIES | | | | |
|-----------------------|--------------------------------|----------|-----------------|-------------------|
| ITEM NO. | DESCRIPTION | UNIT | AS BID QUANTITY | AS BUILT QUANTITY |
| AR150530 | TRAFFIC MAINTENANCE | L. SUM | 1 | |
| AR152480 | SHOULDER ADJUSTMENT | SQ. YD. | 617 | |
| AR201660 | BITUMINOUS CRACK REPAIR | LIN. FT. | 17,600 | |
| AR201666 | BASE COURSE CRACK REPAIR | SQ. YD. | 466 | |
| AR201910 | REMOVE & REPLACE BIT. PAVEMENT | SQ. YD. | 95 | |
| AR401110 | PAVEMENT PREPARATION | L. SUM | 1 | |
| AR401655 | BUTT JOINT CONSTRUCTION | SQ. YD. | 363 | |
| AR402622 | POROUS FRICTION COURSE, 0.10' | SQ. YD. | 35,110 | |
| AR603510 | BITUMINOUS TACK COAT | GAL. | 10,533 | |
| AR620520 | PAVEMENT MARKING-WATERBORNE | SQ. FT. | 28,099 | |

ILLINOIS PROJECT NO. 1H8-3625
AIP PROJECT NO. 3-17-0014-B8

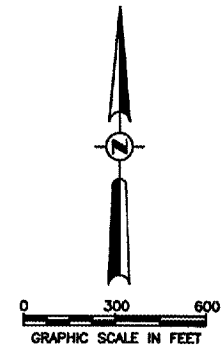
DECEMBER 15, 2006

| INDEX OF SHEETS | |
|-----------------|--------------------------------------|
| NO. | DESCRIPTION |
| 1 | COVER SHEET |
| 2 | IMPROVEMENT AND SAFETY PLAN |
| 3 | TYPICAL SECTIONS AND SPECIAL DETAILS |
| 4-5 | RUNWAY 4-22 PLAN AND PROFILE |
| 6 | RUNWAY MARKING LAYOUT |



VICINITY MAP

| | | |
|---|---|--|
| <p>EXPIRES 11/30/07 ENGINEER'S SEAL</p> | PLANS PREPARED BY : HUTCHISON ENGINEERING, INC. JACKSONVILLE, ILLINOIS | CITY OF CASEY, ILLINOIS |
| | SUBMITTED <i>Doug J. Hutchison</i> DATE <i>November 15, 2006</i> | APPROVED <i>Scott C. ...</i> MAYOR DATE <i>December 4, 2006</i> |



LEGEND

- APPLY POROUS FRICTION COURSE AND MARK RUNWAY 4-22
- CONTRACTOR'S HAUL/ACCESS ROUTE

ROUTE OF ACCESS TO AIRPORT DURING CONSTRUCTION TO APPLY POROUS FRICTION COURSE ON RUNWAY 4-22

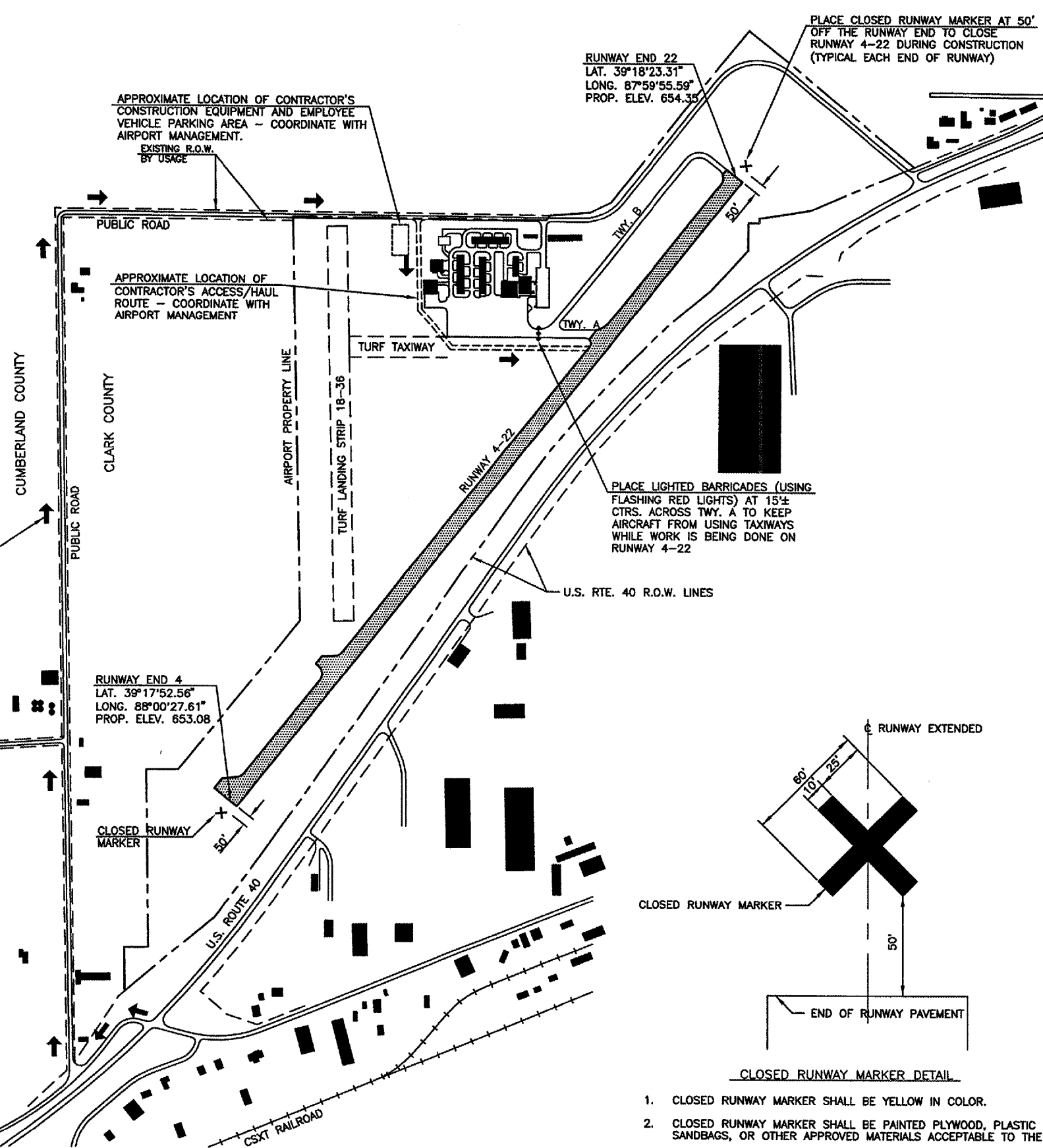
GENERAL NOTES

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY FROM CONSTRUCTION OPERATIONS. PRIOR TO STARTING WORK, THE CONTRACTOR SHALL CONTACT J.U.L.I.E. (JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS) PHONE 1-800-892-0123 TO ARRANGE FOR LOCATION OF UTILITIES IN THE WORK AREA. A MINIMUM OF FORTY-EIGHT HOURS ADVANCE NOTICE IS REQUIRED FOR NON-EMERGENCY WORK.

COUNTY -- CLARK
 CITY -- CASEY
 TOWNSHIP -- T. 10 N., R. 14 W. 2nd P.M. (CASEY)
 SECTION NO. -- 19

2. AVAILABLE RECORDS INDICATE THAT THE FOLLOWING UTILITIES ARE LOCATED WITHIN THE GENERAL AREA OF THIS IMPROVEMENT :

TELEPHONE : ALLTEL ILLINOIS, INC.
 WATER : CITY OF CASEY, ILLINOIS - UTILITIES DEPARTMENT
 ELECTRICAL : CITY OF CASEY, ILLINOIS - UTILITIES DEPARTMENT



RUNWAY END 22
 LAT. 39°18'23.31"
 LONG. 87°59'55.59"
 PROP. ELEV. 654.35

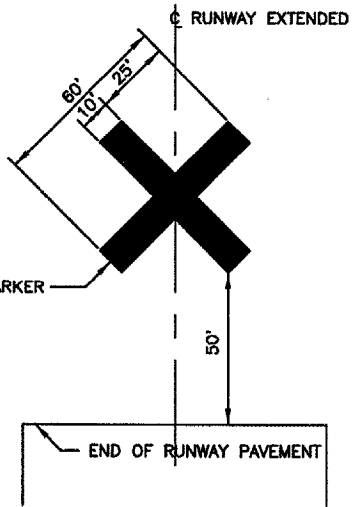
APPROXIMATE LOCATION OF CONTRACTOR'S CONSTRUCTION EQUIPMENT AND EMPLOYEE VEHICLE PARKING AREA - COORDINATE WITH AIRPORT MANAGEMENT. EXISTING R.O.W. BY USAGE

APPROXIMATE LOCATION OF CONTRACTOR'S ACCESS/HAUL ROUTE - COORDINATE WITH AIRPORT MANAGEMENT

PLACE CLOSED RUNWAY MARKER AT 50' OFF THE RUNWAY END TO CLOSE RUNWAY 4-22 DURING CONSTRUCTION (TYPICAL EACH END OF RUNWAY)

PLACE LIGHTED BARRICADES (USING FLASHING RED LIGHTS) AT 15± CTRS. ACROSS TWY. A TO KEEP AIRCRAFT FROM USING TAXWAYS WHILE WORK IS BEING DONE ON RUNWAY 4-22

RUNWAY END 4
 LAT. 39°17'52.56"
 LONG. 88°00'27.61"
 PROP. ELEV. 653.08



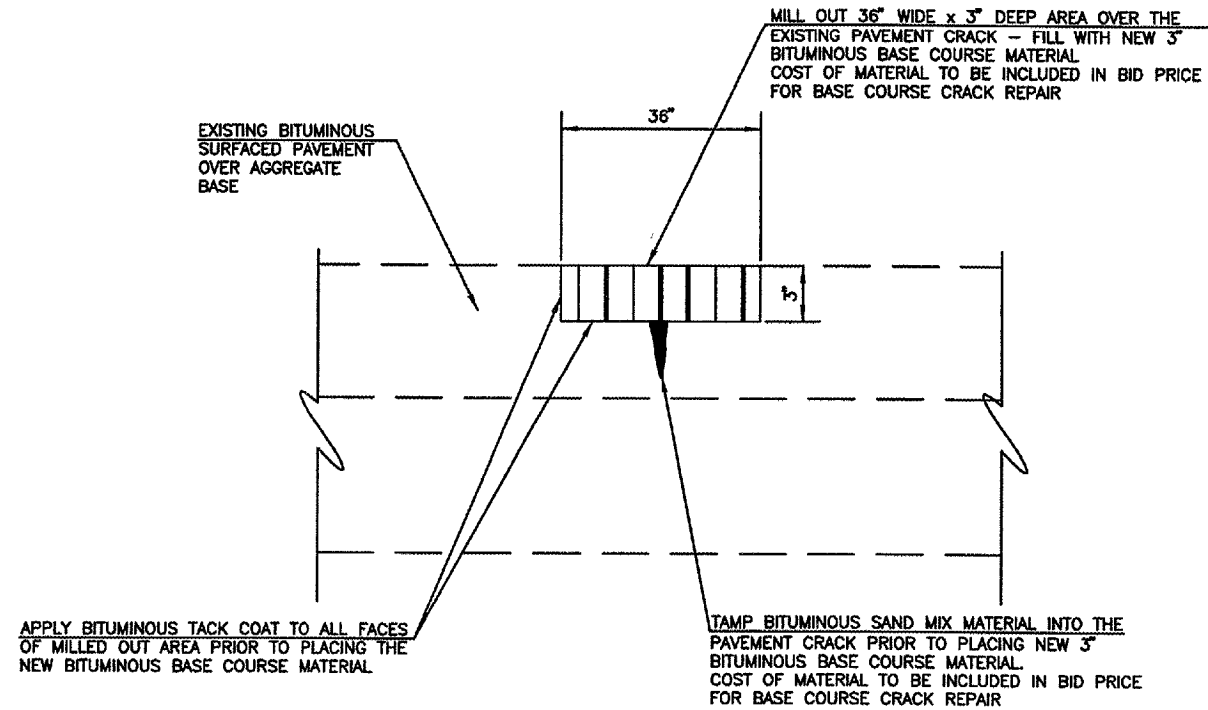
CLOSED RUNWAY MARKER DETAIL

1. CLOSED RUNWAY MARKER SHALL BE YELLOW IN COLOR.
2. CLOSED RUNWAY MARKER SHALL BE PAINTED PLYWOOD, PLASTIC SHEETING WEIGHTED DOWN WITH SANDBAGS, OR OTHER APPROVED MATERIALS ACCEPTABLE TO THE ENGINEER.
3. CONTRACTOR SHALL MAINTAIN MARKERS AS DIRECTED BY THE RESIDENT ENGINEER.
4. THE COST OF FURNISHING, PLACING, MAINTAINING, AND REMOVING CLOSED RUNWAY MARKERS SHALL BE PAID FOR UNDER ITEM AR150530 TRAFFIC MAINTENANCE.

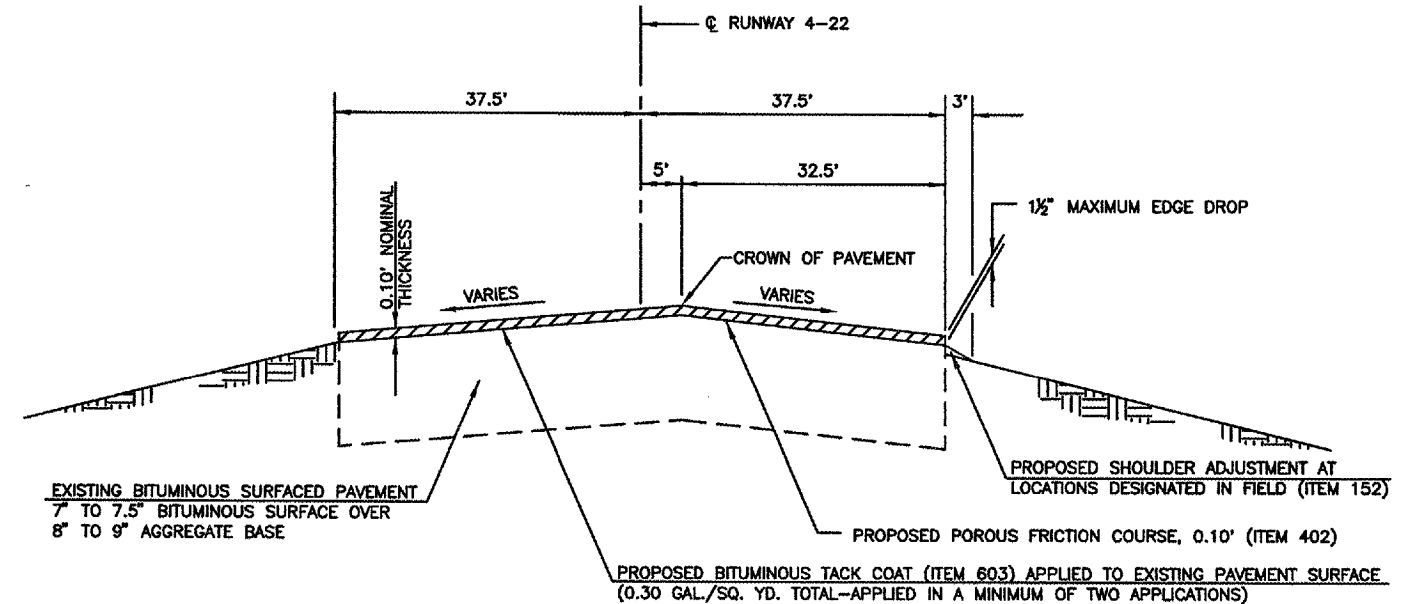
CONSTRUCTION PROCEDURE NOTES

1. ALL CONTRACTOR EMPLOYEES WILL PARK THEIR PERSONAL VEHICLES IN THE AREA DESIGNATED BY THE AIRPORT MANAGER FOR USE AS A VEHICLE PARKING AREA. THE CONTRACTOR WILL BE RESPONSIBLE FOR TRANSPORTING EMPLOYEES TO THE PROPOSED CONSTRUCTION SITE. ONLY AUTHORIZED CONTRACTOR VEHICLES WILL BE ALLOWED ON THE AIR OPERATIONS AREA OF THE AIRPORT AND ON THE CONSTRUCTION SITE.
2. THE CONTRACTOR SHALL FURNISH ALL EMPLOYEES WITH SOME TYPE OF TAG OR GARMENT TO IDENTIFY THEM AS BEING PART OF THE CONSTRUCTION CREW.
3. FLAGS WILL BE REQUIRED ON ALL CONTRACTOR'S VEHICLES AND EQUIPMENT USED ON THE AIRPORT OPERATIONS AREA AND CONSTRUCTION SITE. THE FLAGS SHALL BE THREE (3) FOOT SQUARE CHECKERED FLAGS (INTERNATIONAL ORANGE AND WHITE) DISPLAYED IN FULL VIEW ABOVE THE VEHICLE.
4. THE CONTRACTOR WILL USE THE DESIGNATED HAUL ROUTE AND EQUIPMENT STORAGE AREA SHOWN ON THIS SHEET. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE DESIGNATED HAUL ROUTE AND EQUIPMENT STORAGE AREA FOR THE DURATION OF THE PROJECT. ANY DAMAGE TO EXISTING PAVEMENTS USED TO HAUL MATERIAL TO THE CONSTRUCTION SITE WILL BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE TO THE SATISFACTION OF THE AIRPORT MANAGER AND THE RESIDENT ENGINEER.
5. THE CONTRACTOR SHALL FURNISH, MAINTAIN, AND REMOVE ALL BARRICADES REQUIRED TO DELINEATE THE WORK AREA AND KEEP AIRCRAFT FROM ENCROACHING INTO SAID WORK AREA. THIS REQUIREMENT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
6. CONSTRUCTION ACTIVITY CONTROL AND OPERATION AREA PROTECTION SHALL BE ACCOMPLISHED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR NO. 150/5370-2E.
7. THE MAXIMUM CONSTRUCTION EQUIPMENT HEIGHT UTILIZED ON THE AIRPORT FOR THIS PROJECT SHALL BE 30'.
8. THIS PROJECT CONSISTS OF WORK TO APPLY POROUS FRICTION COURSE AND MARK RUNWAY 4-22.
9. THE CONTRACTOR SHALL COORDINATE A METHOD OF DISABLING THE RUNWAY 4-22 EDGE LIGHTS WITH THE AIRPORT MANAGEMENT SO AS TO KEEP THE LIGHTS INOPERABLE WHILE THE RUNWAY IS CLOSED TO AIR TRAFFIC. THE COST OF DISABLING AND RE-ENERGIZING THE EDGE LIGHT CIRCUITS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
10. THIS PROJECT WILL DISTURB LESS THAN 1 ACRE OF LAND.

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|---|--|
| CASEY MUNICIPAL AIRPORT CASEY, ILLINOIS | |
| IMPROVEMENT AND SAFETY PLAN | |
| HUTCHISON ENGINEERING, INC. JACKSONVILLE, ILLINOIS | |
| DRAWN BY: T.J.D. DATE: NOVEMBER, 2006 | ILL. PROJ. NO. 118-3625 AP PROJ. NO. 3-17-0014-88 |



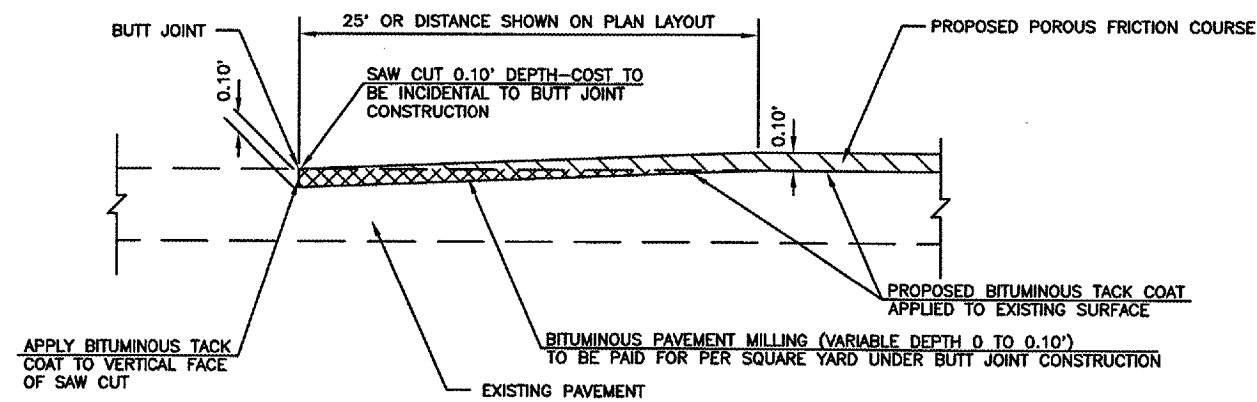
BASE COURSE CRACK REPAIR DETAIL
NOT TO SCALE



RUNWAY 4-22 TYPICAL SECTION
NOT TO SCALE

GENERAL NOTES

1. OPEN PAVING JOINTS AND/OR CRACKS IN THE EXISTING SURFACE OF THE PAVEMENT OVER 3/8" IN WIDTH SHALL BE DESIGNATED BY THE PROJECT ENGINEER IN THE FIELD TO BE REPAIRED PRIOR TO PLACING THE POROUS FRICTION COURSE. THIS WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE SPECIAL PROVISIONS FOR BITUMINOUS CRACK REPAIR.
2. THE QUANTITIES FOR CRACK REPAIR HAVE BEEN ESTIMATED BY MEASURING CRACK REPAIR IN SELECTED REPRESENTATIVE LOCATIONS ON THE PAVEMENT. THESE QUANTITIES MAY BE INCREASED OR REDUCED AT THE TIME OF CONSTRUCTION BASED UPON THE CRACK WORK THAT IS MARKED OUT BY THE RESIDENT ENGINEER AFTER FIELD INSPECTION OF THE PAVEMENT CRACKS.
3. ANY SOD WHICH HAS ENCRONCHED UPON THE PAVEMENT EDGE SHALL BE BLADED BACK PRIOR TO THE PROPOSED POROUS FRICTION COURSE APPLICATION. THE ENTIRE PAVEMENT SURFACE SHALL BE SWEEPED TO REMOVE ALL LOOSE MATERIALS BEFORE THE APPLICATION OF BITUMINOUS TACK COAT. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR PAVEMENT PREPARATION.
4. ANY DAMAGE TO THE EXISTING SOD BEYOND THE EDGE OF PAVEMENT RESULTING FROM THE BLADING OF THE SOD OFF THE PAVEMENT SURFACE SHALL BE FILLED WITH DIRT, GRADED, FERTILIZED AND SEEDED TO RESTORE THE SOD SHOULDERS TO THEIR ORIGINAL CONDITION. ANY LOCALLY AVAILABLE 12-12-12 FERTILIZER MAY BE USED APPLIED AT A RATE OF 1 POUND OF FERTILIZER PER EACH 160 SQUARE FEET OF DISTURBED OR RESEEDING AREA. SEED USED SHALL BE ALTA FESCUE OR KENTUCKY 31, APPLIED AT THE RATE OF 1 POUND OF PURE LIVE SEED PER EACH 436 SQUARE FEET OF RESEEDING AREA. THIS WORK WILL NOT BE PAID FOR SEPARATELY AND SHALL BE CONSIDERED INCIDENTAL TO PAVEMENT PREPARATION.
5. A BITUMINOUS TACK COAT SHALL BE APPLIED TO THE EXISTING PAVEMENT PRIOR TO PLACING THE POROUS FRICTION COURSE. THE APPLICATION RATE SHOWN ON THE TYPICAL SECTION WAS USED TO CALCULATE THE PLAN QUANTITY. THE EXACT AMOUNT OF BITUMINOUS TACK COAT TO BE USED SHALL BE DETERMINED BY THE PROJECT ENGINEER AT THE TIME OF CONSTRUCTION.
6. THE POROUS FRICTION COURSE SHALL BE ABUTTED TO THE EXISTING PAVEMENT AT INTERSECTING TAXIWAYS AS SHOWN ON THE BITUMINOUS BUTT JOINT DETAIL. THE EXISTING PAVEMENT CONFINED WITHIN THE BITUMINOUS PAVEMENT MILLING AREA WILL BE REMOVED IN ACCORDANCE WITH THE DETAIL FROM 0 TO 0.10' DEPTH AT THE BUTT JOINT LOCATION. FILLING OF THE MILLED AREA WILL BE MADE USING POROUS FRICTION COURSE MIX AND WILL BE PAID FOR PER SQUARE YARD FOR POROUS FRICTION COURSE.
7. AT LOCATIONS WHERE THE EDGE DROP FROM THE FINISHED SURFACE TO THE EXISTING TURF SHOULDER EXCEEDS 1/2", THE SHOULDER SHALL BE ADJUSTED AS SHOWN IN THE TYPICAL SECTION USING SOIL OBTAINED OFF SITE THAT IS FREE OF HEAVY SOD, DECAYED VEGETATIVE MATTER, RUBBISH, AGGREGATE AND OTHER UNSUITABLE MATERIAL. THE SHOULDER ADJUSTMENT AREAS SHALL BE FERTILIZED AND SEEDED AND THE COST OF THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR SHOULDER ADJUSTMENT.
8. ALL AREAS DISTURBED BY THE CONTRACTOR INCLUDING THE CONTRACTOR'S PARKING AREA, HAUL ROADS, ETC. SHALL BE REGRADED AND RESEEDED AND/OR RESTORED TO THEIR ORIGINAL CONDITION. THIS RESTORATION SHALL BE ACCOMPLISHED BY THE CONTRACTOR AT HIS OWN EXPENSE UPON COMPLETION OF THIS IMPROVEMENT PROJECT.

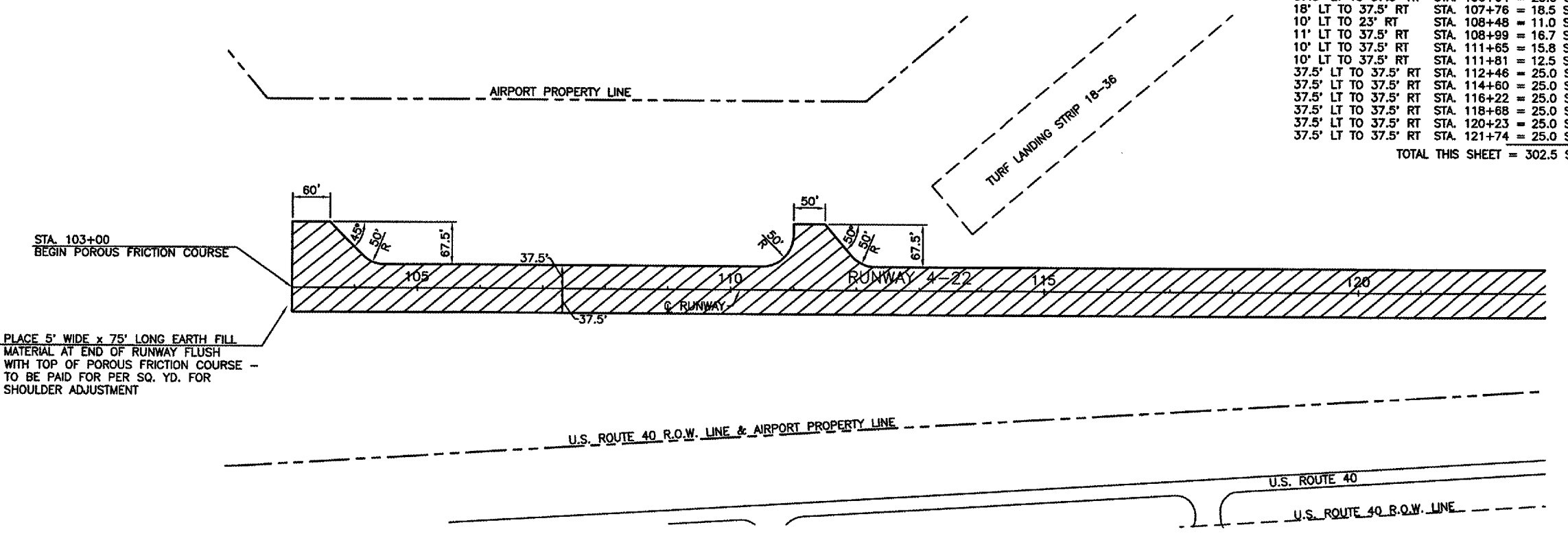
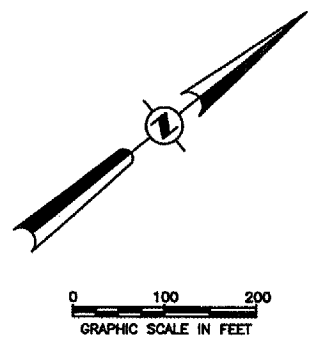


BITUMINOUS BUTT JOINT CONSTRUCTION DETAIL
NOT TO SCALE

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|---|--|
| CASEY MUNICIPAL AIRPORT CASEY, ILLINOIS | |
| TYPICAL SECTIONS AND SPECIAL DETAILS | |
| HUTCHISON ENGINEERING, INC. JACKSONVILLE, ILLINOIS | |
| DRAWN BY: T.J.D. DATE: NOVEMBER, 2006 | ILL. PROJ. NO. 118-3625 AP PROJ. NO. 3-17-0014-BB |

BASE COURSE CRACK REPAIR

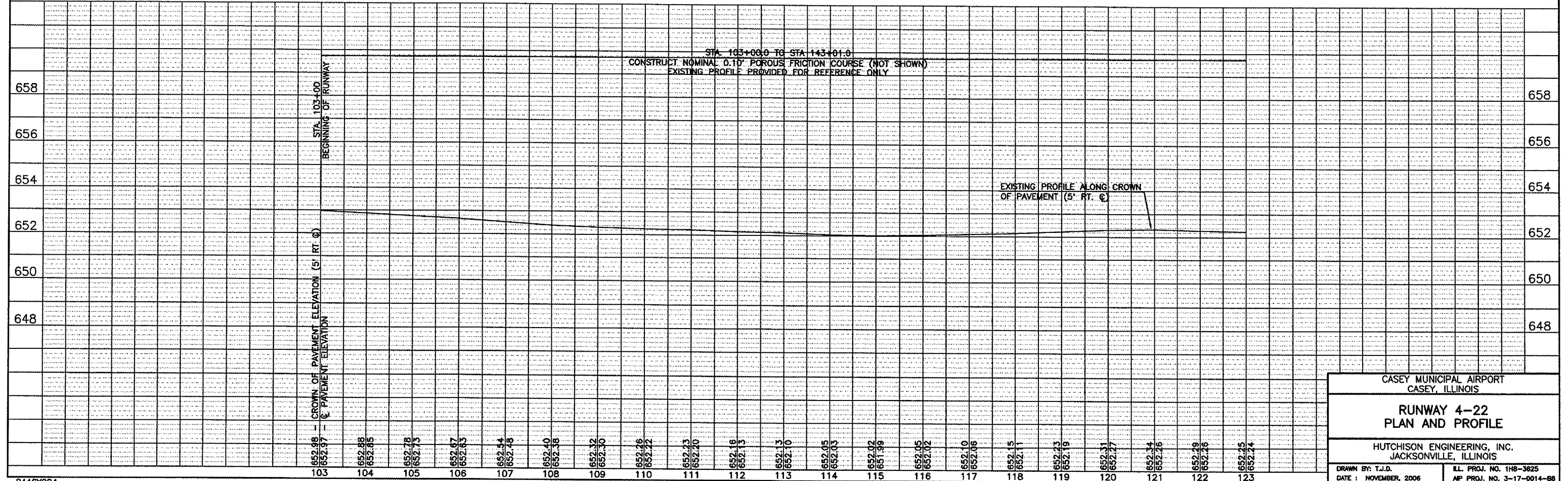
| | | |
|---------------------------|-------------|----------------------|
| 15' LT TO 22' RT | STA. 103+90 | = 12.3 SQ. YD. |
| 17' LT TO 30' RT | STA. 104+45 | = 15.7 SQ. YD. |
| 37.5' LT TO 37.5' RT | STA. 105+33 | = 25.0 SQ. YD. |
| 37.5' LT TO 37.5' RT | STA. 106+64 | = 25.0 SQ. YD. |
| 18' LT TO 37.5' RT | STA. 107+76 | = 18.5 SQ. YD. |
| 10' LT TO 23' RT | STA. 108+48 | = 11.0 SQ. YD. |
| 11' LT TO 37.5' RT | STA. 108+99 | = 16.7 SQ. YD. |
| 10' LT TO 37.5' RT | STA. 111+65 | = 15.8 SQ. YD. |
| 10' LT TO 37.5' RT | STA. 111+81 | = 12.5 SQ. YD. |
| 37.5' LT TO 37.5' RT | STA. 112+46 | = 25.0 SQ. YD. |
| 37.5' LT TO 37.5' RT | STA. 114+60 | = 25.0 SQ. YD. |
| 37.5' LT TO 37.5' RT | STA. 116+22 | = 25.0 SQ. YD. |
| 37.5' LT TO 37.5' RT | STA. 118+88 | = 25.0 SQ. YD. |
| 37.5' LT TO 37.5' RT | STA. 120+23 | = 25.0 SQ. YD. |
| 37.5' LT TO 37.5' RT | STA. 121+74 | = 25.0 SQ. YD. |
| TOTAL THIS SHEET = | | 302.5 SQ. YD. |



PLACE 5' WIDE x 75' LONG EARTH FILL MATERIAL AT END OF RUNWAY FLUSH WITH TOP OF POROUS FRICTION COURSE - TO BE PAID FOR PER SQ. YD. FOR SHOULDER ADJUSTMENT

LEGEND

- POROUS FRICTION COURSE, 0.10'
- BITUMINOUS PAVEMENT MILLING FOR BUTT JOINT CONSTRUCTION



CASEY MUNICIPAL AIRPORT
CASEY, ILLINOIS

**RUNWAY 4-22
PLAN AND PROFILE**

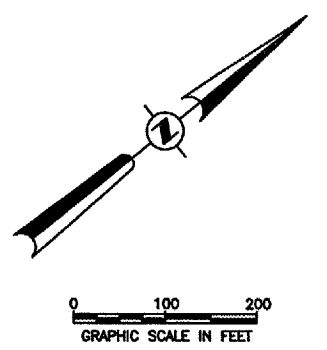
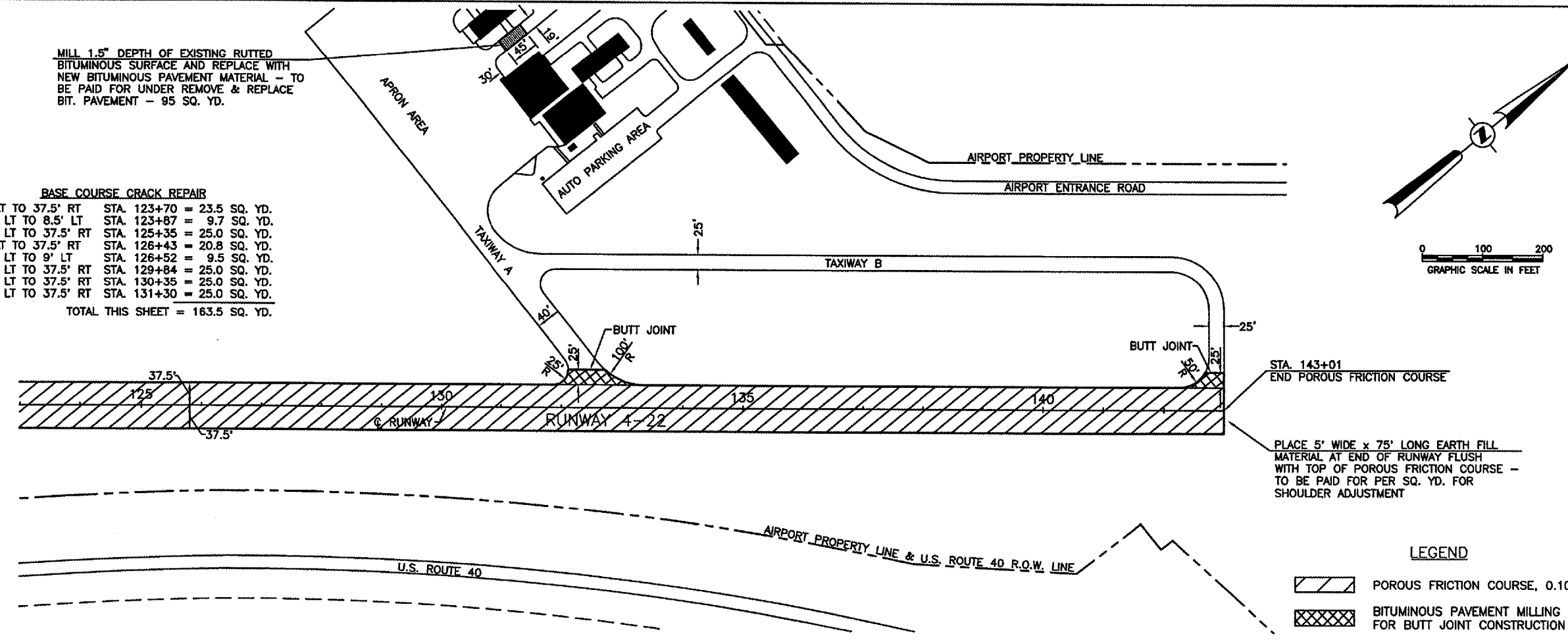
HUTCHISON ENGINEERING, INC.
JACKSONVILLE, ILLINOIS

| | |
|----------------------|---------------------------|
| DRAWN BY: T.J.D. | ILL. PROJ. NO. 118-3625 |
| DATE: NOVEMBER, 2006 | AP PROJ. NO. 3-17-0014-B8 |

MILL 1.5" DEPTH OF EXISTING RUTTED BITUMINOUS SURFACE AND REPLACE WITH NEW BITUMINOUS PAVEMENT MATERIAL - TO BE PAID FOR UNDER REMOVE & REPLACE BIT. PAVEMENT - 95 SQ. YD.

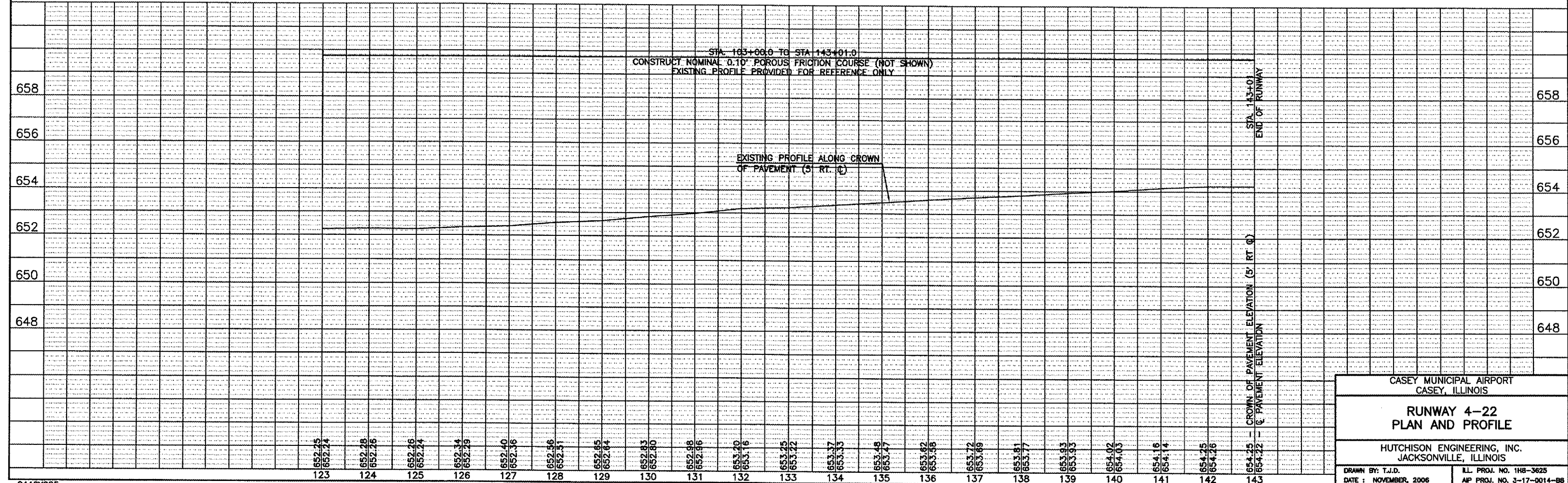
BASE COURSE CRACK REPAIR

| | | |
|---|---------------|--------------|
| 33' LT TO 37.5' RT | STA. 123+70 = | 23.5 SQ. YD. |
| 37.5' LT TO 8.5' LT | STA. 123+87 = | 9.7 SQ. YD. |
| 37.5' LT TO 37.5' RT | STA. 125+35 = | 25.0 SQ. YD. |
| 25' LT TO 37.5' RT | STA. 126+43 = | 20.8 SQ. YD. |
| 37.5' LT TO 9' LT | STA. 126+52 = | 9.5 SQ. YD. |
| 37.5' LT TO 37.5' RT | STA. 129+84 = | 25.0 SQ. YD. |
| 37.5' LT TO 37.5' RT | STA. 130+35 = | 25.0 SQ. YD. |
| 37.5' LT TO 37.5' RT | STA. 131+30 = | 25.0 SQ. YD. |
| TOTAL THIS SHEET = 163.5 SQ. YD. | | |



LEGEND

- POROUS FRICTION COURSE, 0.10'
- BITUMINOUS PAVEMENT MILLING FOR BUTT JOINT CONSTRUCTION

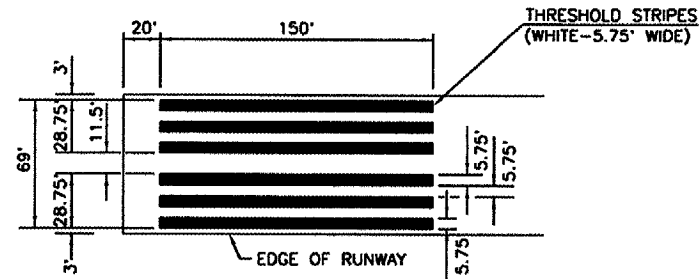


CASEY MUNICIPAL AIRPORT
CASEY, ILLINOIS

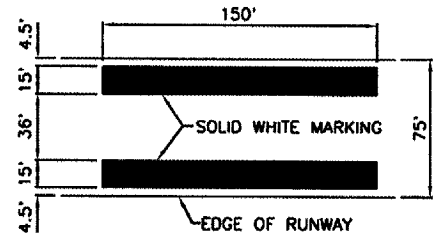
**RUNWAY 4-22
PLAN AND PROFILE**

HUTCHISON ENGINEERING, INC.
JACKSONVILLE, ILLINOIS

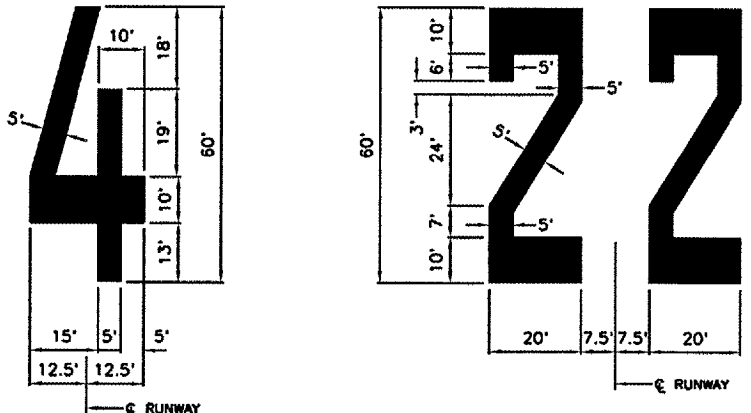
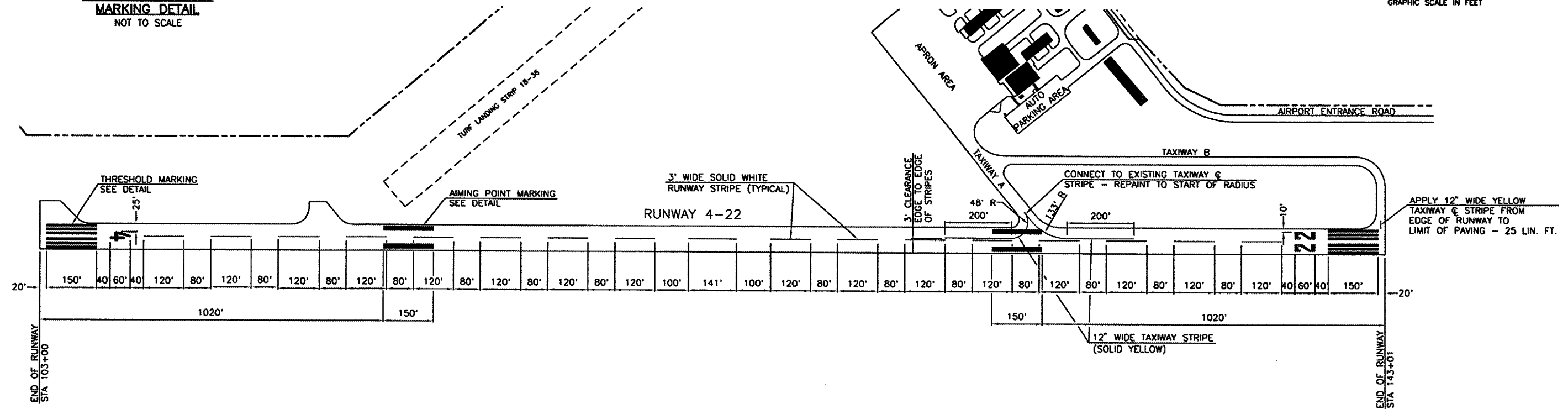
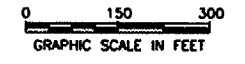
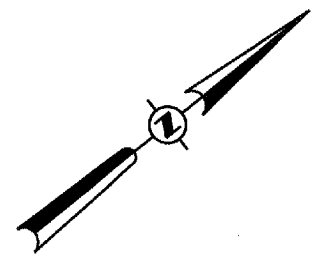
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RUNWAY THRESHOLD MARKING DETAIL
NOT TO SCALE



AIMING POINT MARKING DETAIL
NOT TO SCALE



NUMERAL DETAILS
NOT TO SCALE

MARKING SUMMARY

| DESCRIPTION | UNIT AREA SQ. FT. | NUMBER REQ'D | WHITE | YELLOW |
|---------------------------------------|----------------------|-----------------|------------------|------------------|
| | | | PAINT SQ. FT. | PAINT SQ. FT. |
| NUMERAL "2" | 652 | 2 | 1,304 | |
| NUMERAL "4" | 602 | 1 | 602 | |
| THRESHOLD MARKING | 5,175 | 2 | 10,350 | |
| AIMING POINT MARKING | 4,500 | 2 | 9,000 | |
| RUNWAY CENTERLINE STRIPES (120' LONG) | 360 | 16 | 5,760 | |
| RUNWAY CENTERLINE STRIPES (141' LONG) | 423 | 1 | 423 | |
| TAXIWAY CENTERLINE STRIPES | 660 | | | 660 |
| TOTALS | | | 27,439 | 660 |

NOTES

- ALL RUNWAY MARKING SHALL BE SOLID MARKING AND WHITE IN COLOR.
- ALL TAXIWAY MARKING SHALL BE SOLID MARKING AND YELLOW IN COLOR.
- ALL PAVEMENT MARKING SHALL CONSIST OF TWO APPLICATIONS OF WATERBORNE PAINT AND ONE APPLICATION OF TYPE B GLASS BEADS (REFLECTIVE MEDIA). PAINT SHALL BE APPLIED AT THE RATE OF 115 SQ. FT./GAL. MAXIMUM PER APPLICATION. GLASS BEADS SHALL BE APPLIED AT THE RATE OF 7 LBS./GAL. MINIMUM.

CASEY MUNICIPAL AIRPORT
CASEY, ILLINOIS

MARKING LAYOUT

HUTCHISON ENGINEERING, INC.
JACKSONVILLE, ILLINOIS

DRAWN BY: T.J.D. ILL. PROJ. NO. 118-3625
DATE: NOVEMBER, 2006 AIP PROJ. NO. 3-17-0014-B8