

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	2017-048BP	COOK	26	1
ILLINOIS			CONTRACT NO. 62F80	

# PROPOSED HIGHWAY PLANS

FOR INDEX OF SHEETS, SEE SHEET NO. 2

IMPROVEMENTS ARE LOCATED IN THE VILLAGES OF HODGKINS, WILLOW SPRINGS, CICERO, AND THE CITY OF CHICAGO.

VARIOUS ROUTES  
VARIOUS LOCATIONS  
SECTION: 2017-048BP  
PROJECT: STP-NHPP-OJZO (838)  
BRIDGE PAINTING  
COOK COUNTY  
C-91-051-18

**TRAFFIC DATA:**

IL 50 AT RR-BNSF  
2015 ADT = 33,000  
SPEED = 35MPH

26th ST. AT RR-BNSF  
2014 ADT = 9,600  
SPEED = 30MPH

IL 50 (NORTH OF I-55)  
AT RR-BNSF  
2015 ADT = 56,600  
SPEED = 35MPH

**LOCATION 4**

SN 016-0008  
RR-BNSF (0.6 mi E 45)  
AT I-55 EB

**LOCATION 5**

SN 016-0009  
RR-BNSF (0.6 mi E 45)  
AT I-55 WB

**LOCATION 1**

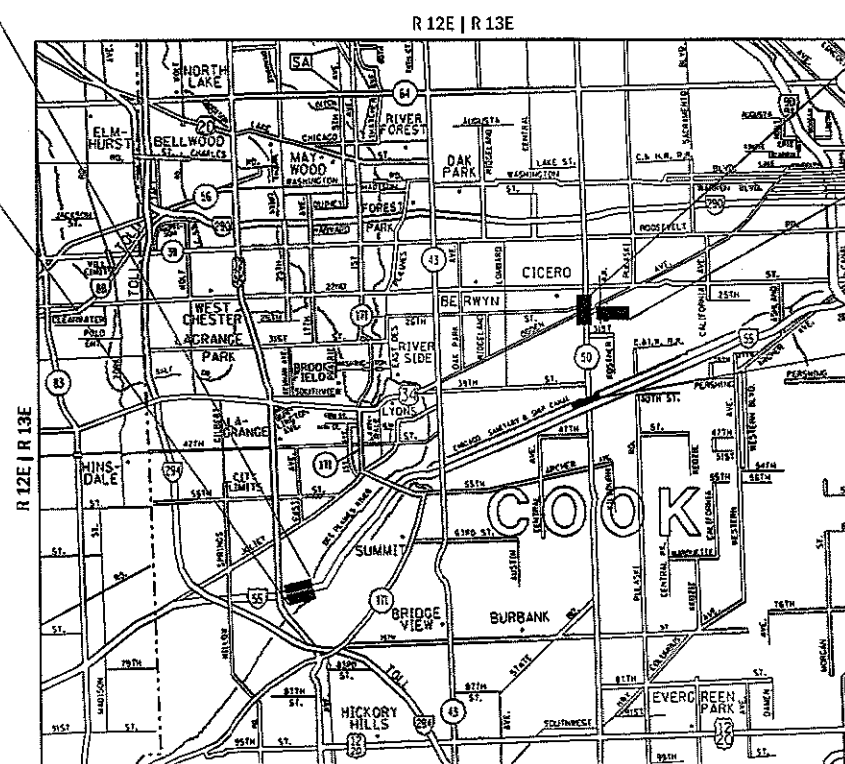
SN 016-0408  
IL 50 AT RR-BNSF

**LOCATION 2**

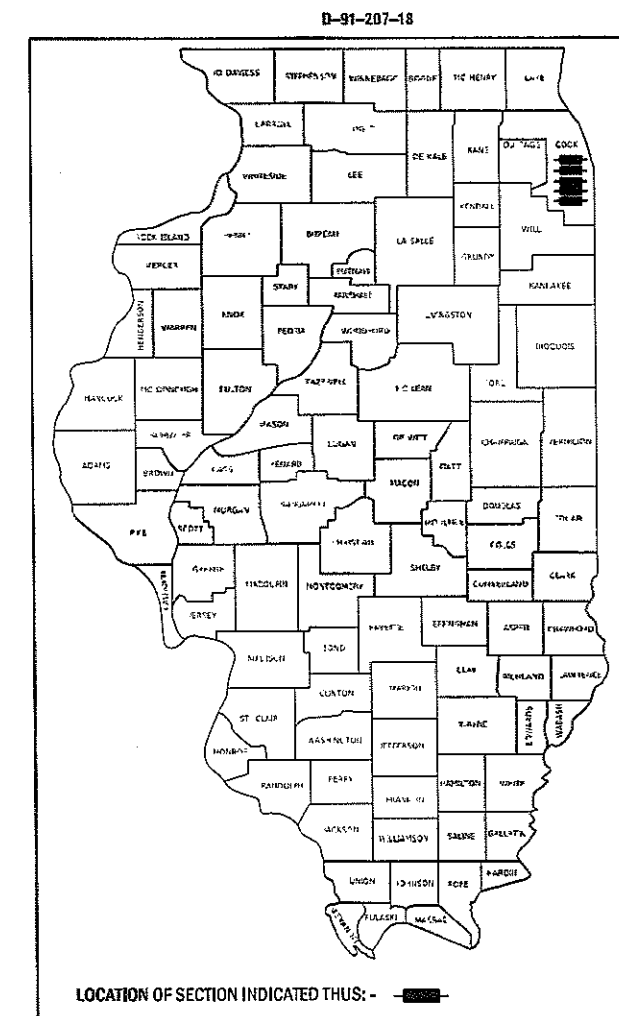
SN 016-0409  
26th ST. AT RR-BNSF

**LOCATION 3**

SN 016-0412  
IL 50 (NORTH I-55) AT  
RR-BNSF



LYONS, CICERO, AND CHICAGO CITY TOWNSHIPS



LOCATION OF SECTION INDICATED THUS: - [black box] -

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

C.U.A.N.  
CHICAGO UTILITY ALERT NETWORK  
(312) 744-7000

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED October 16, 2017

*Anthony J. Gungy* / CAB  
REGIONAL ENGINEER

NOV 30 2017  
*Naureen M. Abdizadeh*  
ENGINEER OF DESIGN AND ENVIRONMENT

NOV 30 2017  
*Amel* 2  
DIRECTOR OF PROGRAM DEVELOPMENT

PROJECT ENGINEER: J. ALAIN MIDY (847) 221-3056  
PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247

CONTRACT NO. 62F80

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

## GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. (48 HOUR NOTIFICATION IS REQUIRED).
2. BEFORE STARTING ANY EXCAVATION IN THE CITY OF CHICAGO, THE CONTRACTOR SHALL CALL "C.U.A.N." (CHICAGO UTILITY ALERT NETWORK) AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. (48 HOUR NOTIFICATION IS REQUIRED).
3. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, THE VILLAGES OF HODGKINS, WILLOW SPRINGS, CICERO, AND THE CITY OF CHICAGO.
4. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROTECT AND PRESERVE THE EXISTING BRIDGE LIGHTING AT ANY LOCATIONS THAT LIGHTING IS ENCOUNTERED ADJACENT TO AN AREA TO BE CLEANED AND PAINTED.
5. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL THE DIMENSIONS & CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION & ORDERING OF MATERIALS.
6. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
7. THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM THE BUREAU OF MAINTENANCE BRIDGE INSPECTORS.
8. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 FOR ARTERIALS AND (847) 705-4151 FOR EXPRESSWAYS A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
9. TREE CLEARING FOR PAINTING ACCESS SHALL BE DONE ACCORDING TO ARTICLE 201.10
10. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
11. THE "ARTERIAL ROAD INFORMATION SIGN (TC-22)" IS APPLICABLE ONLY TO ARTERIAL ROADS AND SHALL NOT BE APPLIED TO EXPRESSWAYS.
12. PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF THE WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED BASED AT THE UNIT PRICE BID FOR THE WORK.
13. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
14. THE CONTRACTOR SHALL REQUEST AND GAIN APPROVAL FROM THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S EXPRESSWAY TRAFFIC OPERATIONS ENGINEER AT (847-705-4155 OR 4155) OR [WWW.IDOT.ILS.GOV](http://www.idot.ils.gov) AT LEAST (24) HOURS IN ADVANCE OF ALL DAILY LANE, AND RAMP AND SHOULDER CLOSURES.

## GENERAL NOTES FOR PAINT

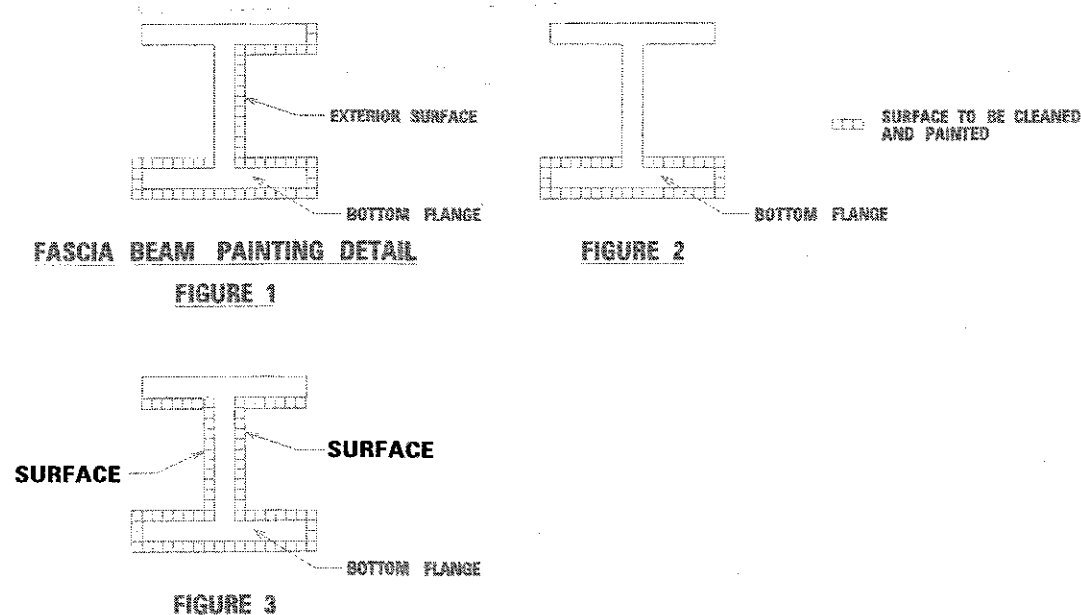
1. CLEANING AND PAINTING OF THE EXISTING STRUCTURAL STEEL SHALL BE AS SPECIFIED IN THE SPECIAL PROVISION FOR "CLEANING AND PAINTING EXISTING STEEL STRUCTURES".
2. THE AREAS DESIGNATED SHALL BE CLEANED PER NEAR WHITE BLAST CLEANING - SSPC-SP10. THE AREAS DESIGNATED SHALL BE PAINTED ACCORDING TO THE REQUIREMENTS OF PAINT SYSTEM 1 -OZ/E/U. THE COLOR OF THE FINAL FINISH COAT FOR ALL INTERIOR STEEL SURFACES SHALL BE GRAY, MUNSELL NO 5B 7/1. THE COLOR OF THE FINAL FINISH COAT FOR EXTERIOR AND BOTTOM FLANGE OF THE FASCIA BEAMS SHALL BE REDDISH BROWN, MUNSELL NO 2.5YR 3/4.
3. A MINIMUM OF TWO (2) AIR MONITORS FOR LOCATION 1 (SN 016-0408) AND A MINIMUM OF ONE (1) AIR MONITOR FOR EACH OTHER STRUCTURES WILL BE REQUIRED TO MONITOR ABRASIVE BLASTING OPERATIONS. SEE SPECIAL PROVISIONS FOR "CONTAINMENT AND DISPOSAL OF LEAD PAINT CLEANING RESIDUES."
4. ALL ITEMS (SUCH AS, BUT NOT LIMITED TO: CONDUITS, BRACKETS AND DECK DRAINS) ATTACHED TO OUTSIDE OF THE FASCIA BEAMS SHOULD BE CLEANED AND PAINTED.
5. LOCATION 1 (SN 016-0408), AND 2 (SN 016-0409): THE EXTERIOR SURFACES AND TOP AND BOTTOM OF THE BOTTOM FLANGE OF THE FASCIA BEAMS (SEE FIGURE 1); TOP AND BOTTOM FLANGE OF INTERIOR BEAMS (SEE FIGURE 2); ALL LOWER LATERAL BRACINGS AND BEARINGS SHALL BE CLEANED PER NEAR WHITE BLAST CLEANING SSPC-SP10.
6. LOCATION 3 (SN 016-0412): ALL STEEL PLATES AT DECK LEVEL (SIDE PATH), END FLOORBEAMS, BEARINGS AND EXTERIOR AND BOTTOM FLANGE OF FASCIA GIRDERS SHALL BE CLEANED PER NEAR WHITE BLAST CLEANING SSPC-SP10.
7. FOR LOCATION 4 (SN 016-0008), AND 5 (SN 016-0009): ALL BEAMS, BEARINGS AND OTHER STRUCTURAL STEEL WITHIN FIVE (5) FT. (MEASURED ALONG THE BEAM) OF EITHER SIDE OF ALL EXPANSION DECK JOINTS, SHALL BE CLEANED PER NEAR WHITE BLAST CLEANING SSPC-SP10 (SEE FIGURE 3). IN ADDITION FOR LOCATION 4 (SN 016-0008): ENTIRE BEAMS 9 AND 16 SHALL BE CLEANED PER NEAR WHITE BLAST CLEANING SSPC-SP10; FOR LOCATION 5 (SN 016-0009): ENTIRE BEAMS 2 AND 8 SHALL BE CLEANED PER NEAR WHITE BLAST CLEANING SSPC-SP10 (SEE FIGURE 3). ALSO BOTTOM FLANGE OF INTERIOR BEAMS 10 THROUGH 15 FOR LOCATION 4 (SN 016-0008) AND BOTTOM FLANGE OF INTERIOR BEAMS 3 THROUGH 7 FOR LOCATION 5 (SN 016-0009) OF SPANS 1 AND 3 SHALL BE CLEANED PER NEAR WHITE BLAST CLEANING SSPC-SP10 (SEE FIGURE 2).

## INDEX OF SHEETS

1. TITLE SHEET
2. INDEX OF SHEETS, STATE STANDARDS, GENERAL NOTES AND GENERAL PAINT NOTES
3. SUMMARY OF QUANTITIES
- 4-8. LOCATION 1 (SN 016-0408)
- 9-11. LOCATION 2 (SN 016-0409)
- 12-15. LOCATION 3 (SN 016-0412)
- 16-18. LOCATION 4 (SN 016-0008)
- 19-21. LOCATION 5 (SN 016-0009)
22. ENTRANCE AND EXIT RAMP CLOSURE DETAILS (TC-08)
23. TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)
24. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14)
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26. ARTERIAL ROAD INFORMATION SIGN (TC-22)

## STATE STANDARDS

STANDARD NO.	DESCRIPTION
701006-05	OFF-ROAD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701106-02	OFF-ROAD OPERATIONS, MULTILANE, MORE THAN 15' AWAY
701011-04	OFF-ROAD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701101-05	OFF-ROAD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS- DAY ONLY
701400-09	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701401-11	LANE CLOSURE, FREEWAY/EXPRESSWAY
701411-09	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS > 45 MPH
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS < 40 MPH
701428-01	TRAFFIC CONTROL, SETUP AND REMOVAL, FREEWAY/EXPRESSWAY
701446-09	TWO LANE CLOSURE, FREEWAY/EXPRESSWAY
701502-08	URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-07	TRAFFIC CONTROL DEVICES



FILE NAME =	USER NAME = eaintvlah	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>INDEX OF SHEETS, STATE STANDARDS, GENERAL NOTES AND GENERAL PAINT NOTES</b>	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	Documents\IDOT Offices\District 1\Projects\01287\Drawings\Design\0128718-sht-plan.dgn	CHECKED -	REVISED -			VAR.	2017-048BP	COOK	26	2
	PLT SCALE = 100,0000 / 1"	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.
	PLT DATE = 11/16/2017									
										ILLINOIS FED. AID PROJECT CONTRACT NO. 62F80

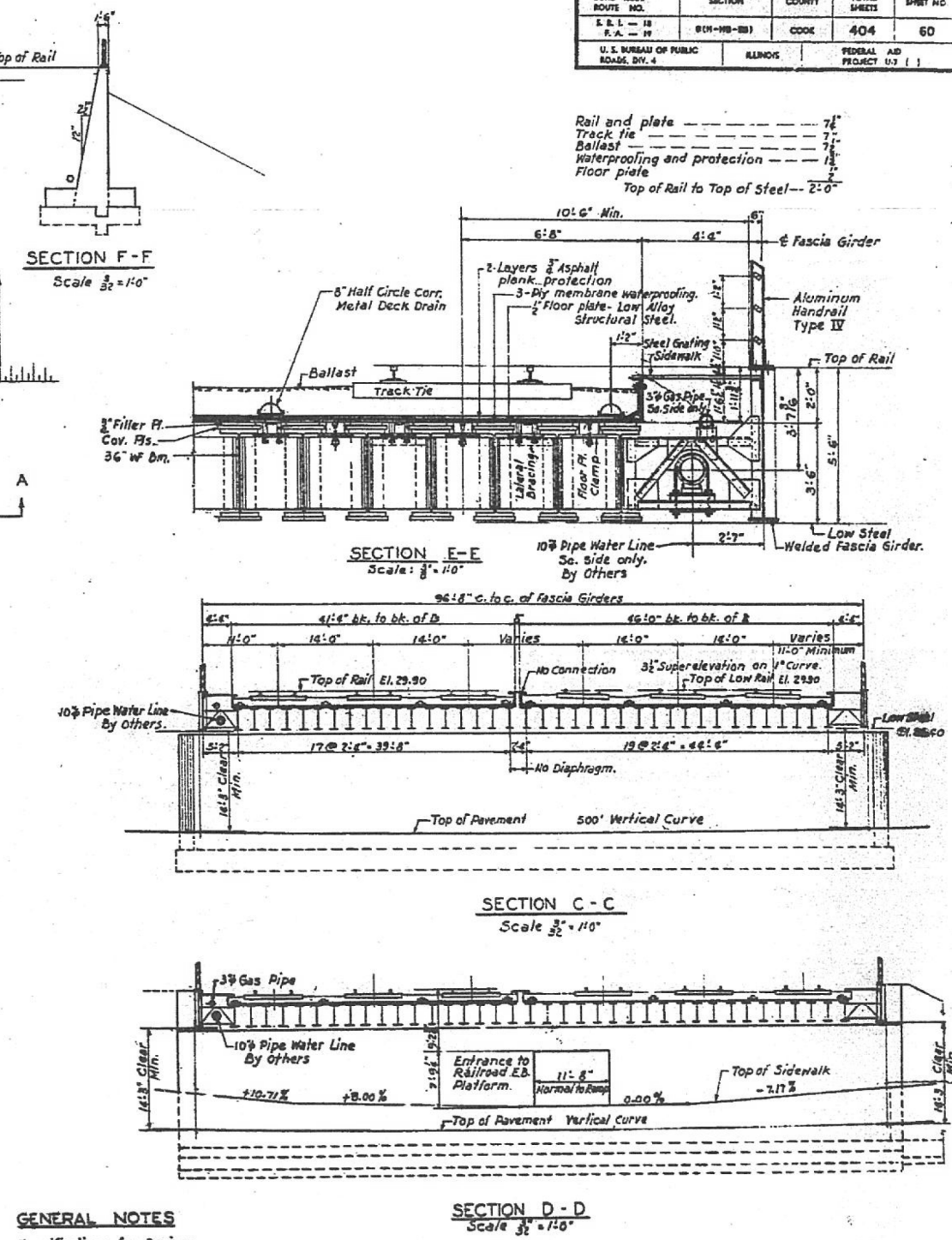
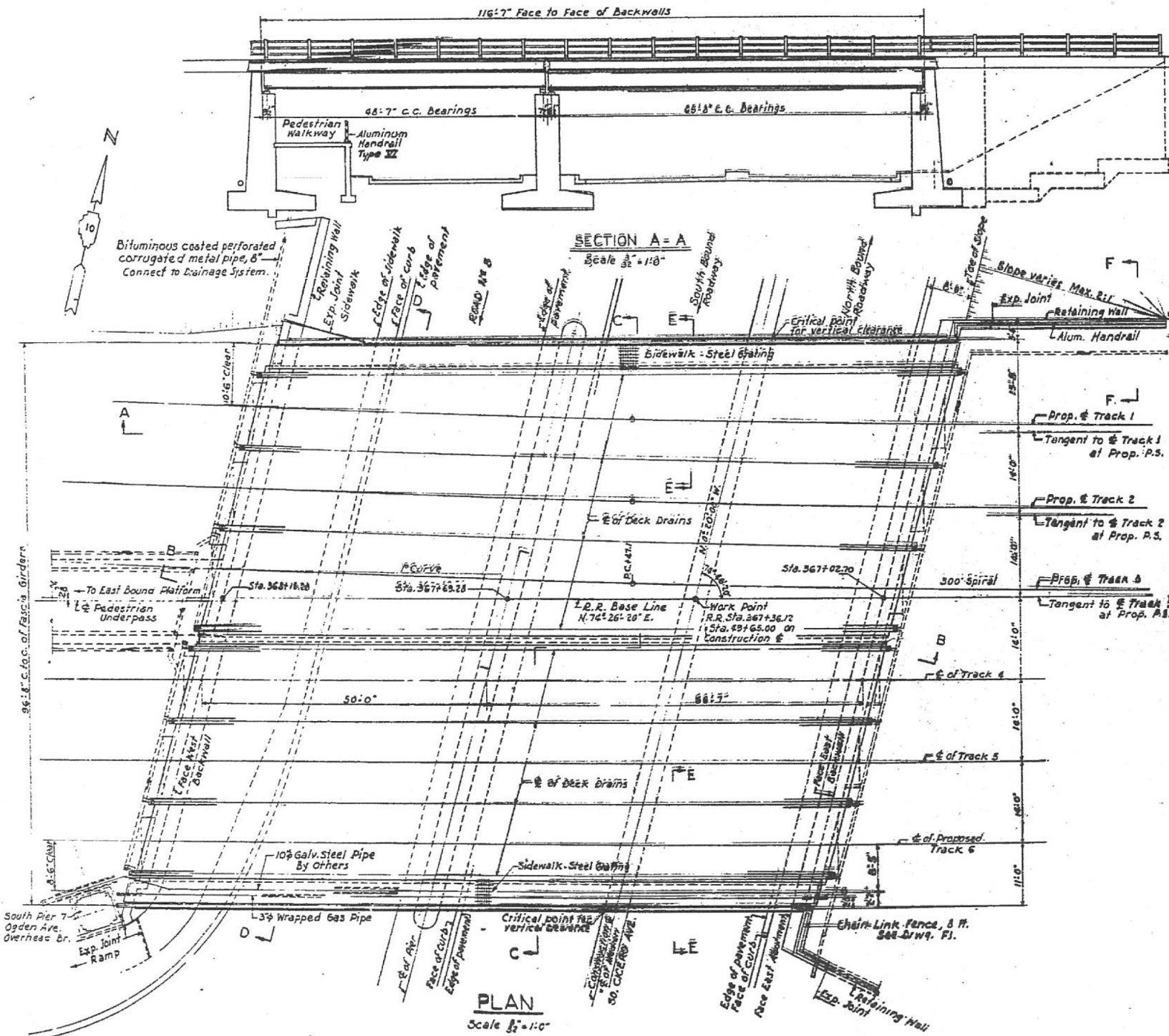
REV

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0047 BRIDGE 1 016-0408 80% NHPP 20% STATE	0047 BRIDGE 2 016-0409 80% STP 20% STATE	0047 BRIDGE 3 016-0412 80% NHPP 20% STATE	0047 BRIDGE 4 016-0008 80% NHPP 20% STATE	0047 BRIDGE 5 016-0009 80% NHPP 20% STATE	CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0047 BRIDGE 1 016-0408 80% NHPP 20% STATE	0047 BRIDGE 2 016-0409 80% STP 20% STATE	0047 BRIDGE 3 016-0412 80% NHPP 20% STATE	0047 BRIDGE 4 016-0008 80% NHPP 20% STATE	0047 BRIDGE 5 016-0009 80% NHPP 20% STATE
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	10	2	2	2	2	2	Z0010503	CLEANING AND PAINTING STEEL BRIDGE NO.	LSUM	1			1		
										3							
67100100	MOBILIZATION	LSUM	1	0.2	0.2	0.2	0.2	0.2	Z0010504	CLEANING AND PAINTING STEEL BRIDGE NO.	LSUM	1				1	
70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	LSUM	1		1				Z0010505	CLEANING AND PAINTING STEEL BRIDGE NO.	LSUM	1					1
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	LSUM	1	0.5		0.5			Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	128.5	25.7	25.7	25.7	25.7	25.7
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	LSUM	1	0.5		0.5			Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	LSUM	1	0.2	0.2	0.2	0.2	0.2
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	LSUM	1	0.2	0.2	0.2	0.2	0.2	Ø Z0076600	TRAINEES	HR	500	500				
70200100	NIGHTTIME WORK ZONE LIGHTING	LSUM	1	0.2	0.2	0.2	0.2	0.2	Ø Z0076604	TRAINEES - TRAINING PROGRAM GRADUATE	HR	500	500				
X7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	LSUM	1				0.5	0.5									
Z0007112	CONTAINMENT AND DISPOSAL OF LEAD PAINT CLEANING RESIDUES	LSUM	1	0.2	0.2	0.2	0.2	0.2									
Z0010501	CLEANING AND PAINTING STEEL BRIDGE NO. 1	LSUM	1	1													
Z0010502	CLEANING AND PAINTING STEEL BRIDGE NO. 2	LSUM	1		1												

Ø 0042

# FOR INFORMATION ONLY

BOND ISSUE ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
E. & L. - 18 P. A. - 19	8(11-19B-88)	COOK	404	60
U. S. BUREAU OF PUBLIC ROADS, DIV. 4			FEDERAL AID PROJECT	U. S. 1



### GENERAL NOTES

**Specifications for Design:**  
 American Railway Engineering Association Manual 1957.  
 Superstructure - Chapter 15. Substructure - Chapter 6, Concrete  $f_c = 3000$ .  
 Live Load - Cooper E72 with impact for Diesel Locomotive.

**Specifications for Material and Workmanship:**  
 A.R.E.A. for Structural Steel and Membrane Waterproofing.  
 Std. Specs. for Road and Bridge Construction, State of Illinois,  
 Dept. of Public Works and Buildings, Division of Highways, dated  
 Jan. 2, 1958, for concrete and all other material, except  
 those covered by Special Provisions.

**Foundation:** Very stiff gray clay.  
 Design load - 2.5 tons maximum per sq. ft. without longitudinal  
 and lateral forces, and 3.5 tons maximum per sq. ft. with  
 longitudinal and lateral forces.

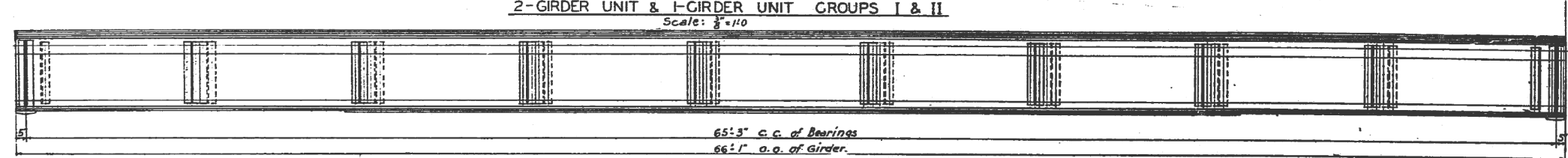
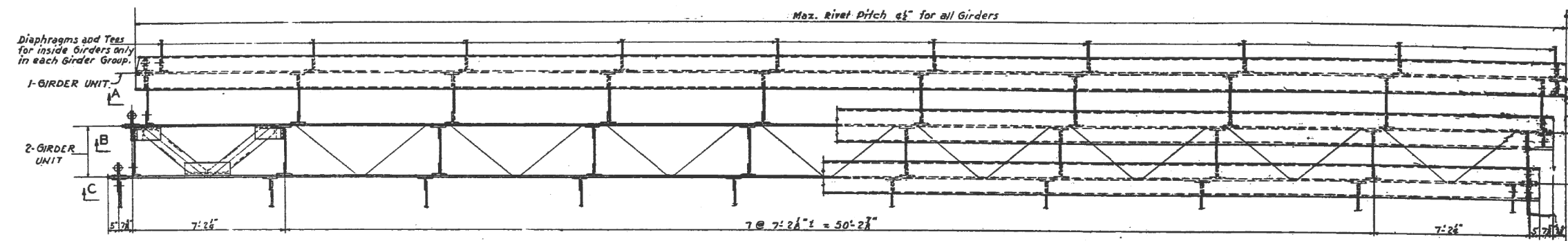
<b>ILLINOIS DIVISION OF HIGHWAYS</b>	
OGDEN AVENUE AND CICERO AVENUE INTERCHANGE	
<b>C.B. &amp; Q.R.R. BRIDGE OVER CICERO AVE. GENERAL PLAN</b>	
DESIGNED BY <b>M.M. VEGARA</b>	SCALE AS SHOWN DEC. 5, 1956
DRAWN BY <b>M.M. VEGARA</b>	SHEET NO. <b>S12</b>
CHECKED BY <b>W.E. SPADE</b>	
IN CHARGE <b>W.B. HORN</b>	
PROJECT MGR. <b>L.H. BROWN</b>	
APPROVED <b>L.H. BRIAN</b>	



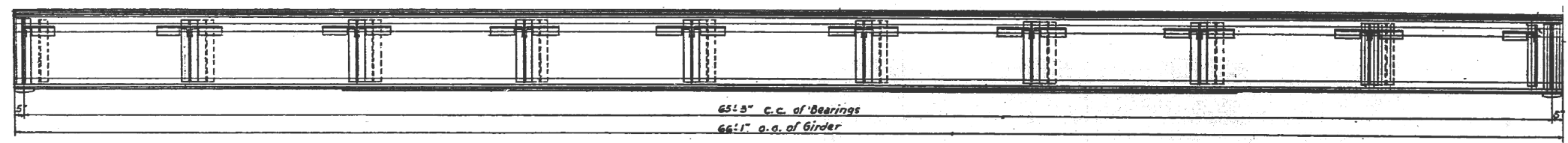


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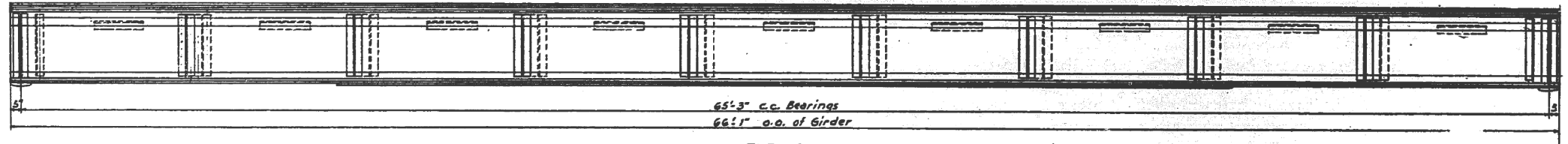
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S. & L. - 18 P. A. - 17	081-081	COOK	404	62
U. S. BUREAU OF PUBLIC ROADS, DIV. 4		ILLINOIS	FEDERAL AID PROJECT (4-1)	



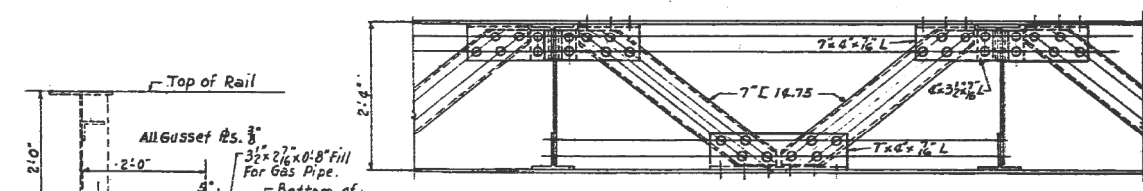
ELEVATION A-A



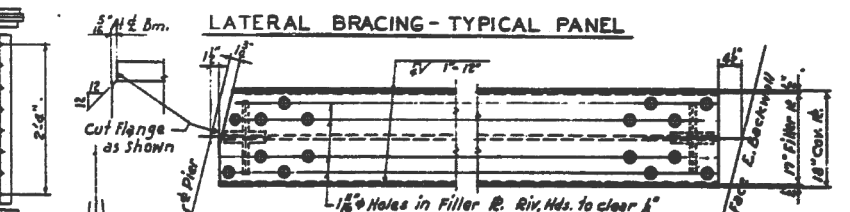
ELEVATION B-B



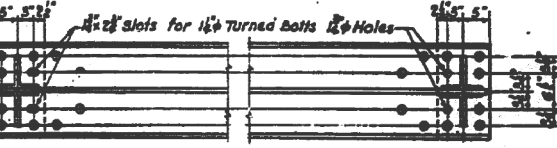
ELEVATION C-C



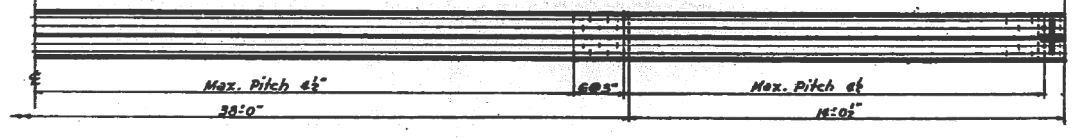
CROSS FRAME  
Scale: 3/8"=1'-0"



TOP FLANGE - TYPICAL



BOTTOM FLANGE - TYPICAL  
Scale: 3/8"=1'-0"



BOTTOM FLANGE - ALL GIRDERS

MAXIMUM MOMENTS AND SHEARS GIRDERS IN GROUP I - TRACKS 1, 2 & 3		
MOMENTS IN FT-KIP	SEC. MO. IN IN-LB	SHEARS IN KIPS
DEAD LOAD 265		26.8
LIVE LOAD 907		62.3
IMPACT 457		31.3
CEN. FORCE 513		21.8
TOTAL 2261	1836 NET	150.4
PROVIDED: GR. SEC. MO. = 2172 NET SEC. MO. = 1836		
DEAD LOAD PER FT. OF GIRDER = 1.06 KIP IMPACT = 80.4 %		
CEN. FORCE = 34.8 %		
L/D = 19.57		

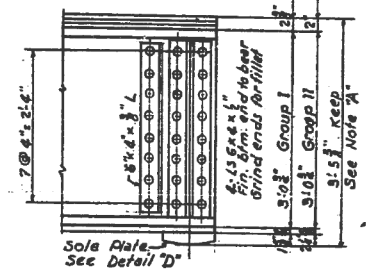
MAXIMUM MOMENTS AND SHEARS GIRDERS IN GROUP II - TRACKS 4, 5 & 6		
MOMENTS IN FT-KIP	SEC. MO. IN IN-LB	SHEARS IN KIPS
DEAD LOAD 259		22.6
LIVE LOAD 907		62.3
IMPACT 457		31.3
TOTAL 1697	1836 NET	150.4
PROVIDED: GR. SEC. MO. = 1862 NET SEC. MO. = 1836		
DEAD LOAD PER FT. OF GIRDER = 1.00 KIP IMPACT = 80.4 %		
L/D = 19.55		

**MAIN MATERIAL FOR ONE GIRDER  
GROUP I TRACKS 1, 2 & 3**

- 1-36 W F 300 x 66 1/2"
- 1-8 1/2" x 18 1/2" x 58' 0"
- 1-8 1/2" x 18 1/2" x 58' 0"
- 1-Top Cov. R. 18 1/2" Full length.
- 1-Top Cov. R. 18 1/2" Full length.
- 1-Top Fill R. 17 1/2" Full length.

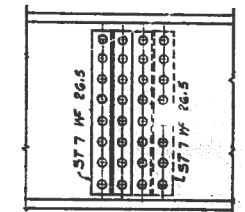
**MAIN MATERIAL FOR ONE GIRDER  
GROUP II TRACKS 4, 5 & 6**

- 1-36 W F 300 x 66 1/2"
- 1-8 1/2" x 18 1/2" x 58' 0"
- 1-8 1/2" x 18 1/2" x 58' 0"
- 1-Top Cov. R. 18 1/2" Full length.
- 1-Top Cov. R. 18 1/2" Full length.
- 1-Top Fill R. 17 1/2" Full length.

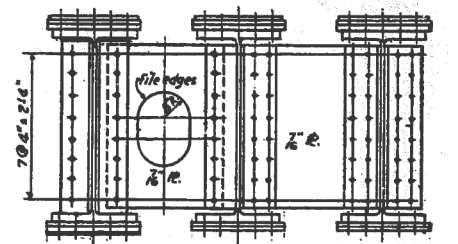


GIRDER END DETAILS  
Scale: 3/8"=1'-0"

Note A: Place Sole Plate to keep exact height of girder.



INT. DIAPHRAGM CONN  
Scale: 3/8"=1'-0"



END DIAPHRAGM INTERMEDIATE DIAPHRAGM  
Scale: 3/8"=1'-0"

**NOTES**

Material: Structural Steel per Specifications given on Drawing No. 957-3C-S12.

Rivets: 3/4"

Holes: 3/4" except as shown and noted.

Painting: See Drawing No. S15.

3/4" High Strength Bolts where required on Cross Frames.

**ILLINOIS DIVISION OF HIGHWAYS**

**OGDEN AVENUE AND CICERO AVENUE INTERCHANGE**

**C.B. & Q.R.R. BRIDGE OVER CICERO AVE. GIRDERS - EAST SPAN**

DE LOURE, GUTHRIE & COMPANY ENGINEERS CHICAGO ILLINOIS

SCALE AS SHOWN ON SHEET 957-3C S 14

FILE NAME =	USER NAME = s01nwlch	DESIGNED -	REVISED -
pw\1\1084EBID\INTEG\11\illinois.gov\PIW001\Documents\1007 Offices\District 1\Projects\DI207\Drawings\Design\DI20718-sht-plen.dgn		CHECKED -	REVISED -
Default	PLOT DATE = 10/20/2017	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

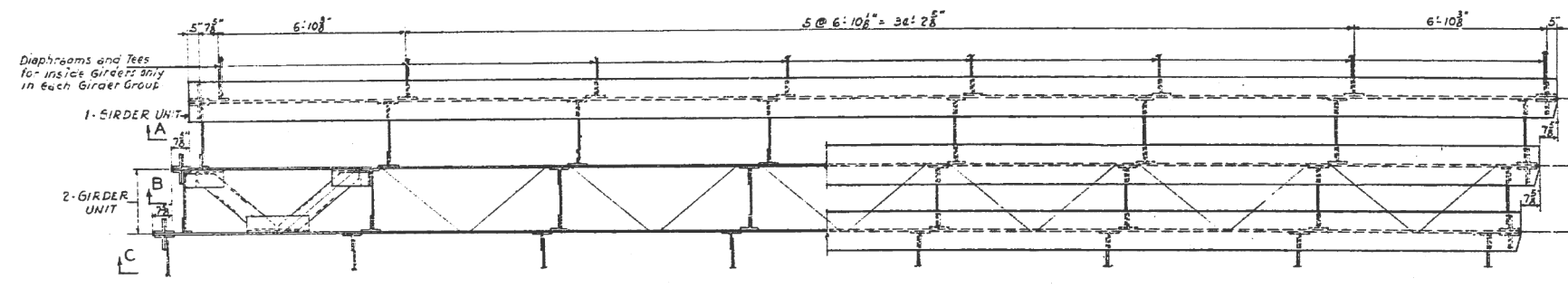
**LOCATION 1 (SN 016-0408)  
GIRDERS - EAST SPAN**

SCALE: SHEET 3 OF 5 SHEETS STA. TO STA.

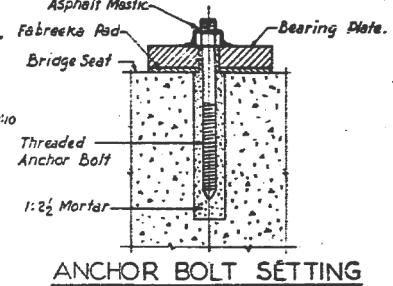
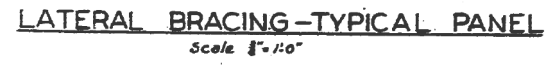
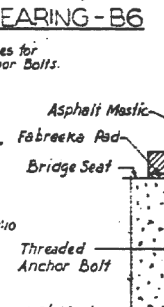
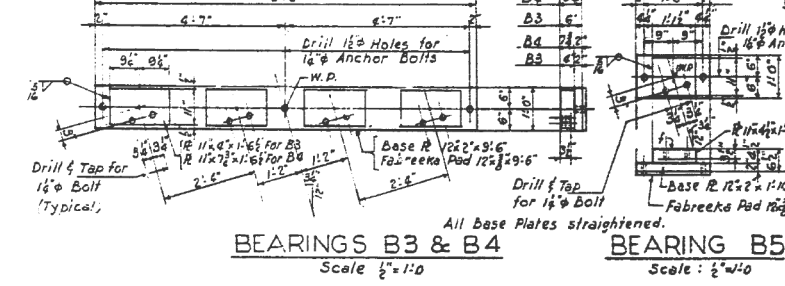
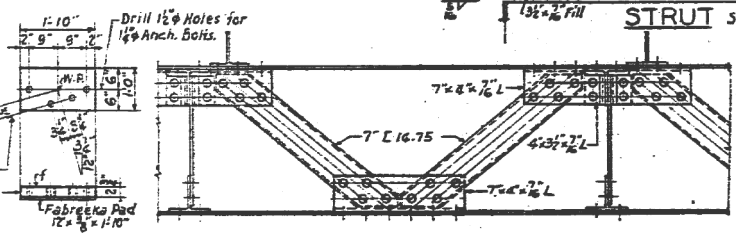
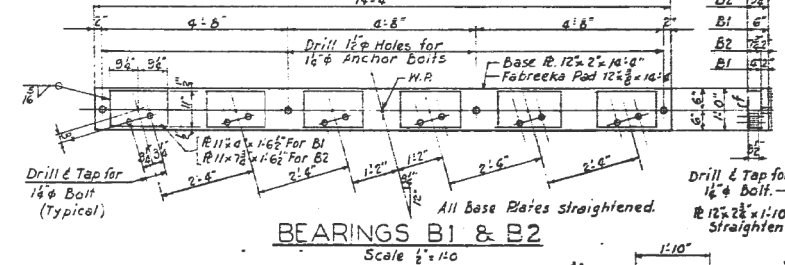
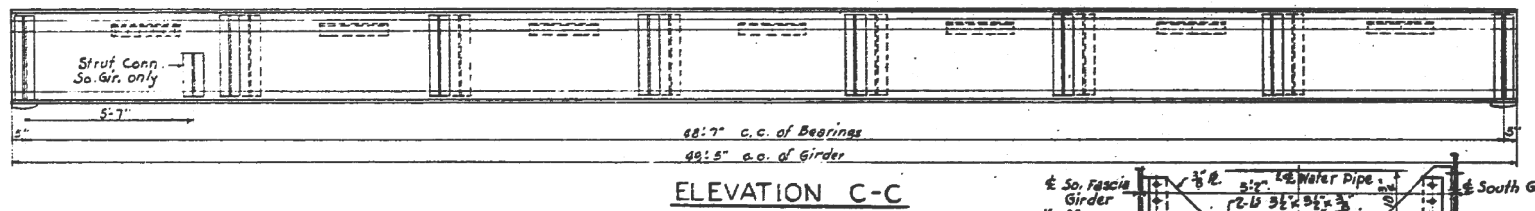
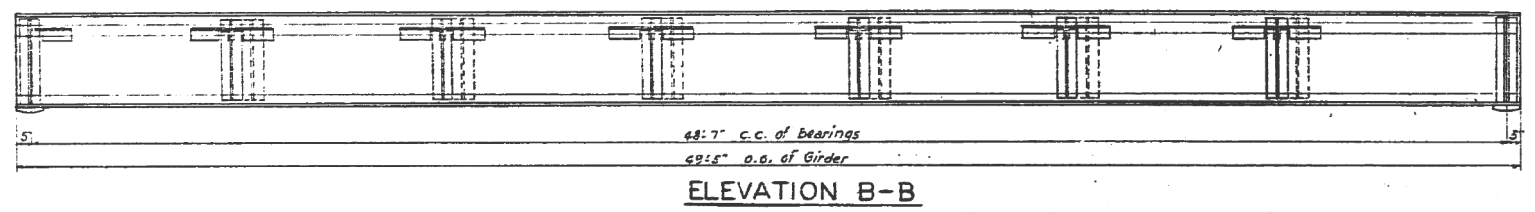
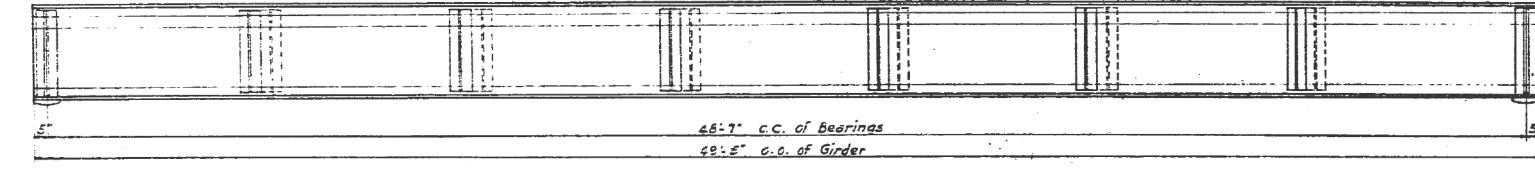
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2017-048BP	COOK	26	6
			CONTRACT NO. 62F80	
ILLINOIS FED. AID PROJECT				

# FOR INFORMATION ONLY

BOND ISSUE ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
S. E. L. - 18 F. A. - 19	041-18-001	COOK	404	63
U. S. BUREAU OF PUBLIC ROADS, DIV. 4			FEDERAL AID PROJECT U. S. 1	

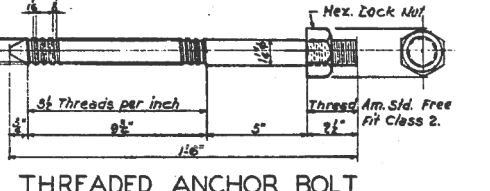
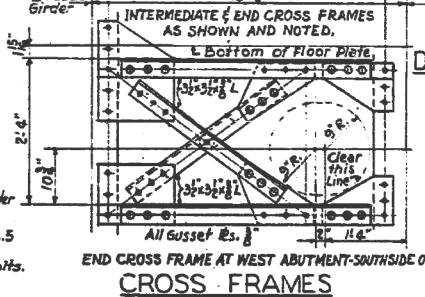
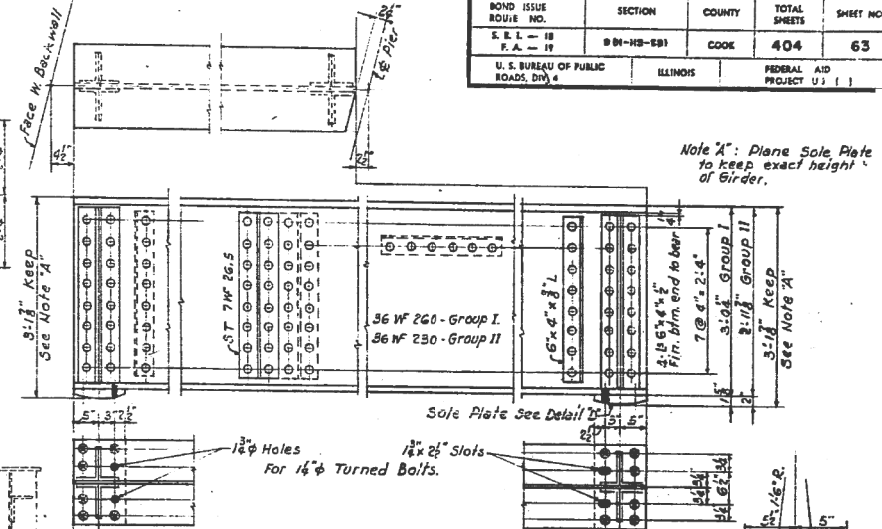


2-GIRDER UNIT AND 1-GIRDER UNIT  
Scale: 3/8" = 1'-0"



Notes for setting threaded anchor bolts.

Immediately before setting anchor bolt saturate the concrete surface interior of hole with water and remove all excess moisture. Fill hole with sufficient stiff cement-sand mortar (1:2 mix) so that when anchor is in final position the mortar will be flush with the top of bearing plate. Screw anchor bolt down until nut bears on bearing plate. Paint nut and anchor projection with asphalt mastic.



MAXIMUM MOMENTS AND SHEARS  
GIRDERS IN GROUP I - TRACKS 1, 2 & 3

MOMENTS IN FT.-KIPS	SEC. MOD. IN IN <sup>3</sup>	SHEARS IN KIPS
DEAD LOAD 243		19.9
LIVE LOAD 542		51.3
IMPACT 293		27.7
CEN. FORCE 187		17.7
TOTAL 1265	892 NET	116.6
PROVIDED: GR. SEC. MOD. = 951	NET SEC. MOD. = 920	
DEAD LOAD PER FT. OF GIRDER = 0.82 KIP	IMPACT = 54.0%	
CEN. FORCE = 34.5%		
L/D = 16.1		

MAXIMUM MOMENTS AND SHEARS  
GIRDERS IN GROUP II - TRACKS 4, 5 & 6

MOMENTS IN FT.-KIPS	SEC. MOD. IN IN <sup>3</sup>	SHEARS IN KIPS
DEAD LOAD 222		18.3
LIVE LOAD 542		51.3
IMPACT 293		27.7
TOTAL 1057	752 NET	97.3
PROVIDED: GR. SEC. MOD. = 635	NET SEC. MOD. = 806	
DEAD LOAD PER FT. OF GIRDER = 6.75 KIP	IMPACT = 54.0%	
L/D = 16.25		

NOTES

Material: Structural Steel per Specifications given on Drawing No. 957-3C-S12.

Rivets: 3/8"

Holes: 1/8" except as shown and noted.

Field Paint: After erection all structural steel shall be cleaned and spot painted with one coat of red lead paint, except surface to be waterproofed. The steel shall then be painted two complete coats of aluminum paint.

3/8" High Strength Bolts where required at Cross Frames and Strut.

ILLINOIS DIVISION OF HIGHWAYS

OGDEN AVENUE AND CICERO AVENUE INTERCHANGE

C.B. & Q.R.R. BRIDGE  
OVER CICERO AVE.  
GIRDERS - WEST SPAN

BE LEWIS, GATHER & COMPANY ENGINEERS CHICAGO ILLINOIS

SCALE AS SHOWN

DRAWING NO. 957-3C

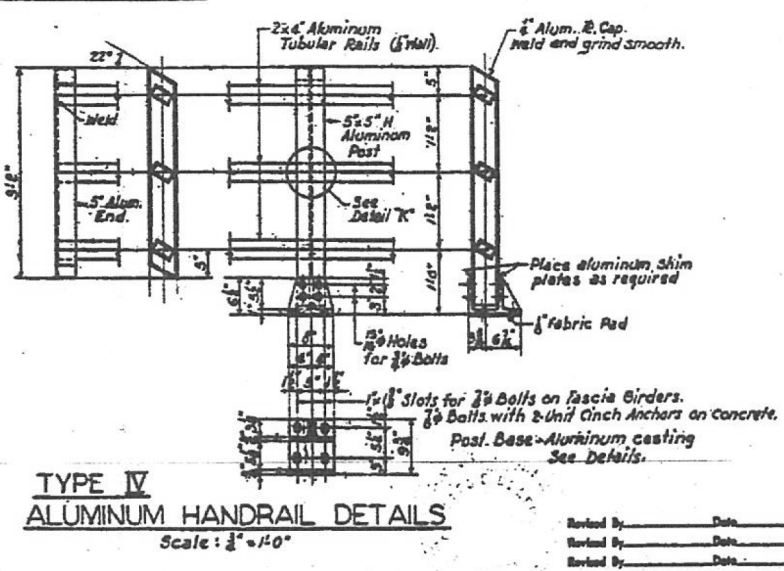
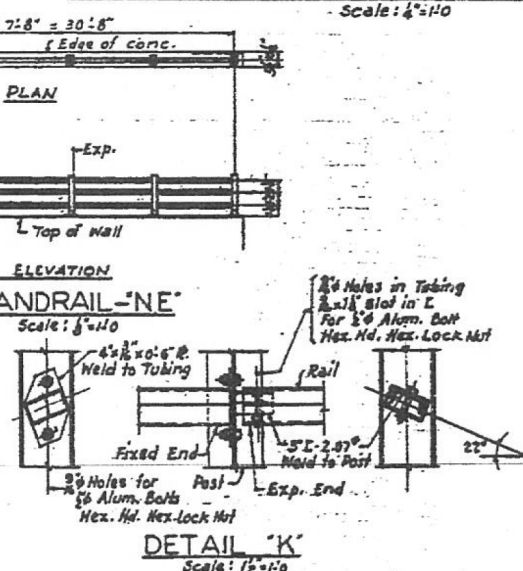
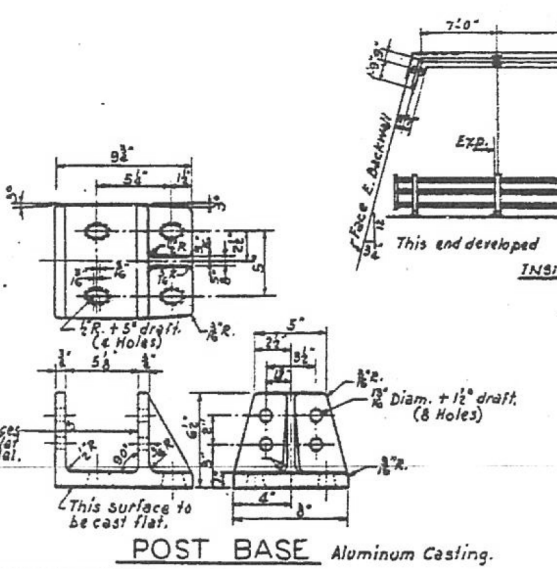
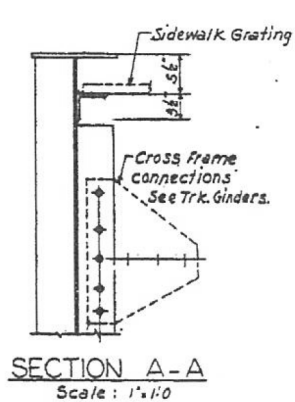
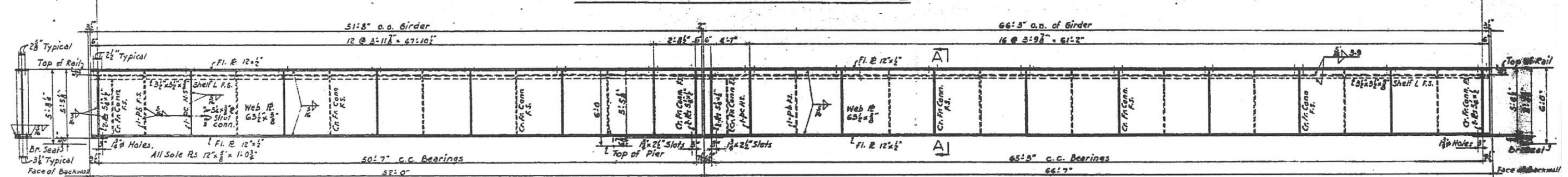
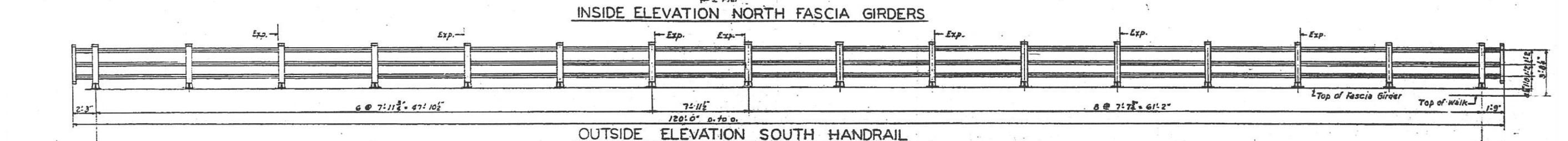
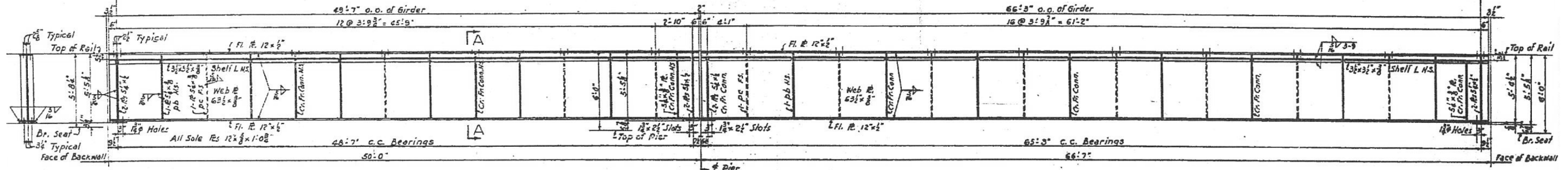
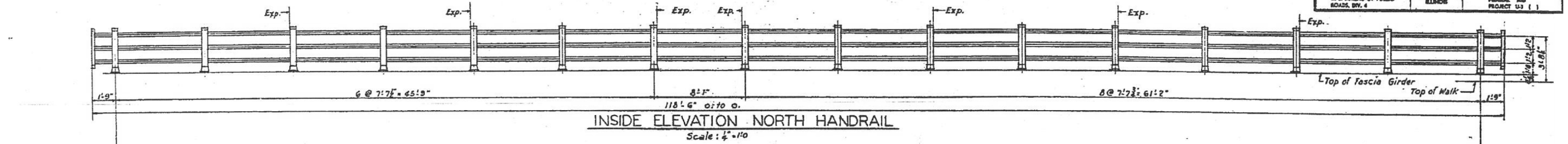
DEC. 3, 1921

S 15



# FOR INFORMATION ONLY

BOND ISSUE ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
E. & L. - 11 P. A. - 11	9 01-08-081	COOK	404	65
U. S. BUREAU OF PUBLIC ROADS, DIV. 4		ILLINOIS	FEDERAL AID PROJECT U3 ( )	



**NOTES**  
 Material: Structural Steel for Fascia Girders.  
 Aluminum for Handrails.  
 Holes: 1/8" except as shown and noted.  
 High Strength Steel Bolts 3/4" to be Used  
 For all Field Connections on Welded Plate  
 Girders Unless Otherwise Noted.

**ILLINOIS DIVISION OF HIGHWAYS**  
 OGDEN AVENUE AND CICERO AVENUE INTERCHANGE  
**C. B. & O.R.R. BRIDGE**  
 OVER CICERO AVE.  
**FASCIA GIRDERS-HANDRAILS**

DESIGNED BY: M. M. VERGARA  
 CHECKED BY: W. E. SPADE  
 IN CHARGE: W. G. HORN  
 APPROVED: L. M. BROWN

DATE: 10/20/2017



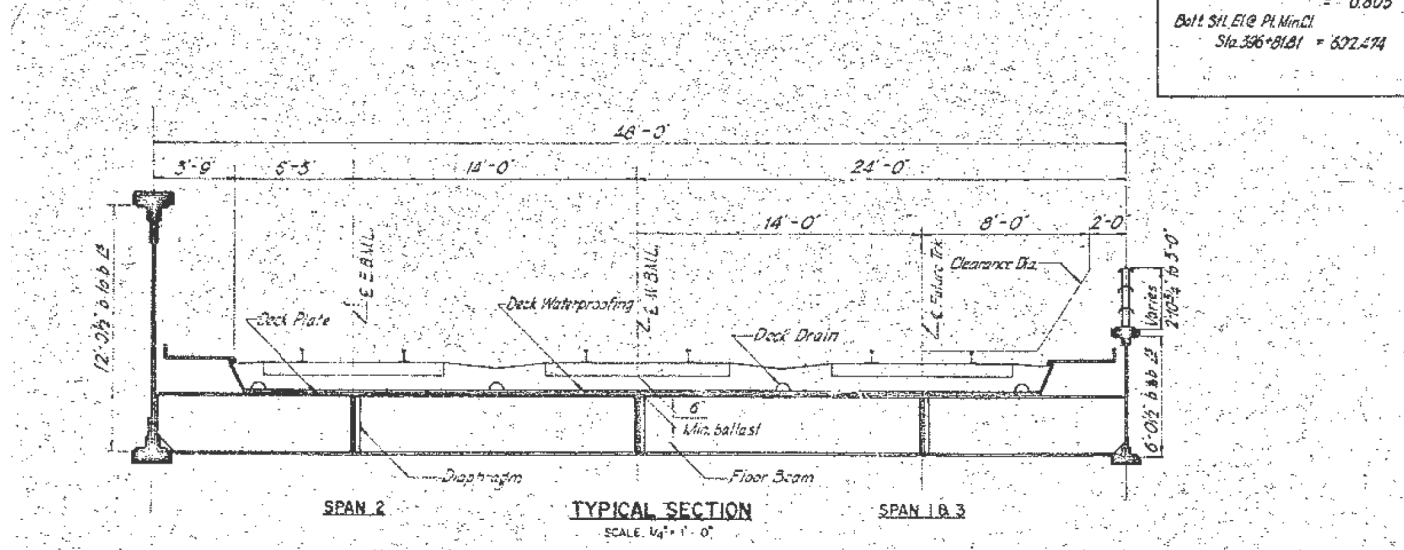
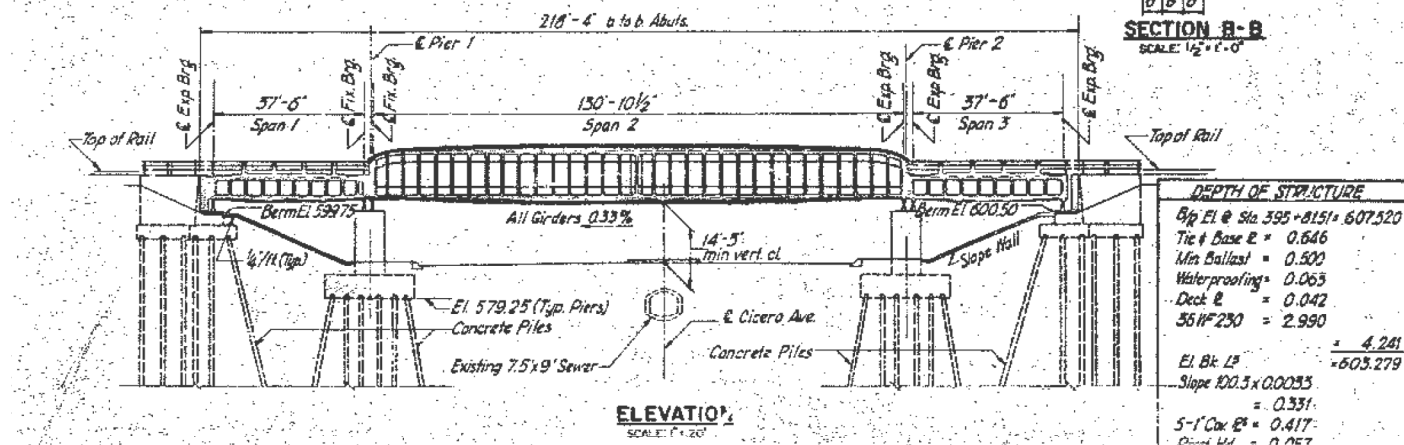
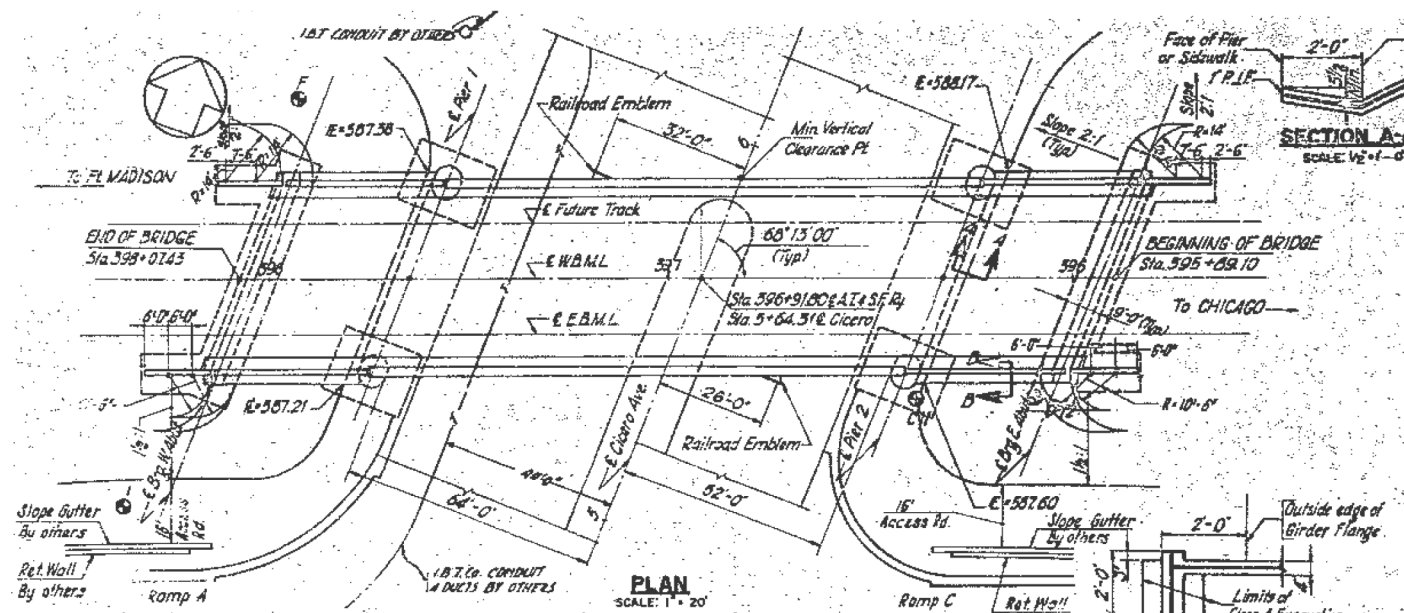






# FOR INFORMATION ONLY

FILE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
65	112-672-58	COOK	27	3



**GENERAL NOTES:**

1. Slope wall shall be reinforced with welded wire fabric 6x6 mesh, #4 wires weighing 56#/per 100 sq ft.
2. All reinforcement bars shall be lapped 20 diameters unless otherwise shown.
3. Class X concrete shall be used throughout.
4. Backs of abutments & wingwalls above footings, where not exposed shall be waterproofed with one primer coat and two coats of hot asphalt.

**STRUCTURAL STEEL:**

1. Rivet material shall conform to A.S.T.M. designation A141, containing 0.2% copper. Rivet sizes shall be as follows, unless otherwise noted:  
Span 2, 1 1/8\"/>
- 2. All field connections shall be made with high strength bolts. For sizes see Structural Steel, Note 1.
- 3. Except as otherwise provided all structural steel shall receive one shop coat of red lead paint and two field coats of aluminum paint. See article 56.1 to 56.5 inclusive of the Standard Specifications.
- 4. Structural Steel shall conform to the following specifications:  
Span 2 main material; A.R.E.A. High Strength Steel containing 0.2% copper.  
All other; A.S.T.M. designation A-36 containing 0.2% copper.

**SUBSTRUCTURE:**

1. The contractor shall drive four concrete test piles, one at each abutment and one at each pier in a permanent location, as directed by the engineer before ordering the remainder of the piles.
2. Concrete piles at abutments shall be driven in holes precored through the embankment. Concrete piles at piers shall be driven in holes precored to elevation 574. This work shall be done in accordance with article 60.9 (c) of the Standard Specifications.

**SPECIFICATIONS:**

1. Where ever in these plans reference is made to the 'Standard Specifications', it is understood to include the 'Supplemental Specifications' effective April 2, 1962.
2. Two signs conforming to Std 2160-f shall be erected at locations as described in Special Provisions.

**DESIGN LOADS AND STRESSES**

**SPECIFICATIONS:**  
American Railway Engineering Association

**DESIGN LOAD:**  
Cooper E 65, Steam Impact.

**DESIGN SPEED:**  
90 M.P.H.

**DESIGN STRESSES:**  
Structural Steel

A.R.E.A. High Strength Steel f = 27,000 psi  
 ASTM A-36 f = 20,000 psi  
 Reinforcement Steel f = 20,000 psi  
 Concrete f<sub>c</sub> = 3,500 psi  
 f<sub>t</sub> = 1,400 psi  
 = 1,000 psi (with earth pressure)  
 f<sub>v</sub> = 75 psi  
 n = 10

Concrete Piles Abutments 35 ton capacity  
 Piers 50 ton capacity

**NOTES:**

1. Tracks, track fittings, ties and ballast by others.
2. Borings indicated thus: ⊕
3. The following quantities are included in the quantity of Special Excavation shown:  
Removal of existing Railway Embankments = 6,200 c.y.  
Removal of ballast on Temporary Detour = 1,330

TOTAL BILL OF MATERIAL				
ITEM	UNIT	SUB.	SUPER.	TOTAL
Special Excavation	Cu. Yd.	11,880		11,880
Embankment	Cu. Yd.	6,200		6,200
Porous Granular Embankment	Cu. Yd.	310		310
Removal of Existing Structures	L. Sum			1
Class A Excavation for Structures	Cu. Yd.	1,850		1,850
Deck Drain, 8 Inch Half Round	Lin. Ft.		860	860
Class X Concrete	Cu. Yd.	812.5		812.5
Deck Waterproofing	Sq. Ft.		8,500	8,500
Furnishing and Erecting Structural Steel	Pound		2,109,600	2,109,600
Furnishing and Erecting Metal Handrail	Lin. Ft.		229	229
Reinforcement Bars	Pound	56,228		56,228
Furnishing Untreated Piles 30.1 - 45 Feet	Lin. Ft.	2,380		2,380
Test Pile Timber	Each	2		2
Driving Timber Piles	Lin. Ft.	2,380		2,380
Driving Concrete Piles	Lin. Ft.	8,070		8,070
Furnishing Concrete Piles	Lin. Ft.	8,070		8,070
Test Pile Concrete	Each	4		4
Name Plates	Each	2		2
Corrugated Metal Pipe B'	Lin. Ft.	150		150
Perforated Corrugated Metal Pipe B'	Lin. Ft.	160		160
Storm Sewers, Type 1 10"	Lin. Ft.	51		51
Inlets, Type A, Type B Grate	Each	2		2
Slope Wall 4 Inch	Sq. Yd.	460		460
Temporary Bridge Complete	Each			1
Rigid Steel Conduit 2 1/2"	Lin. Ft.	15		15
Railroad Emblems	Each		2	2
Railroad Protective Services	L. Sum			1

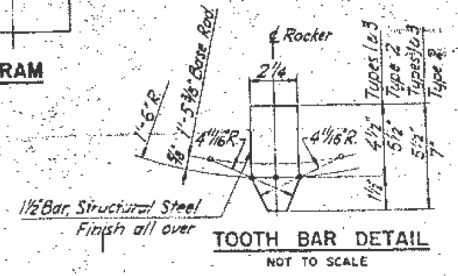
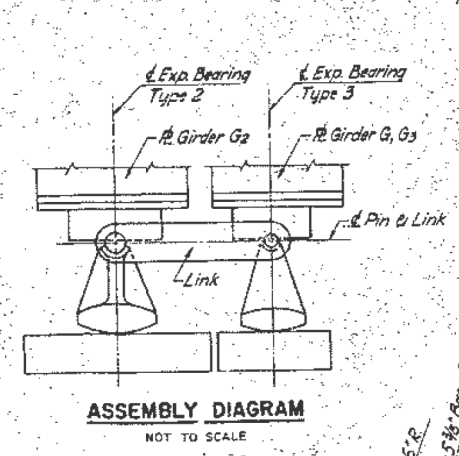
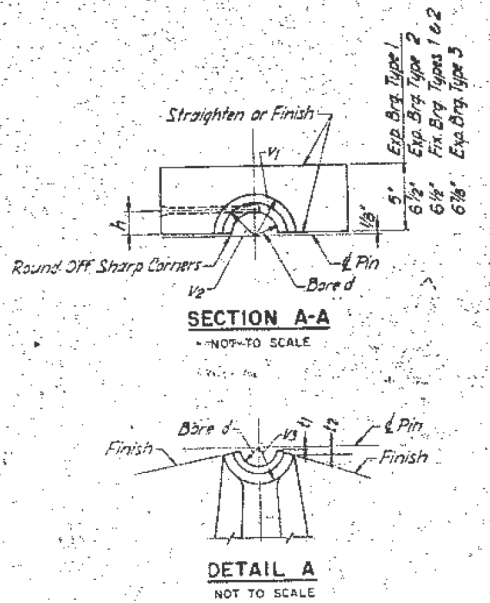
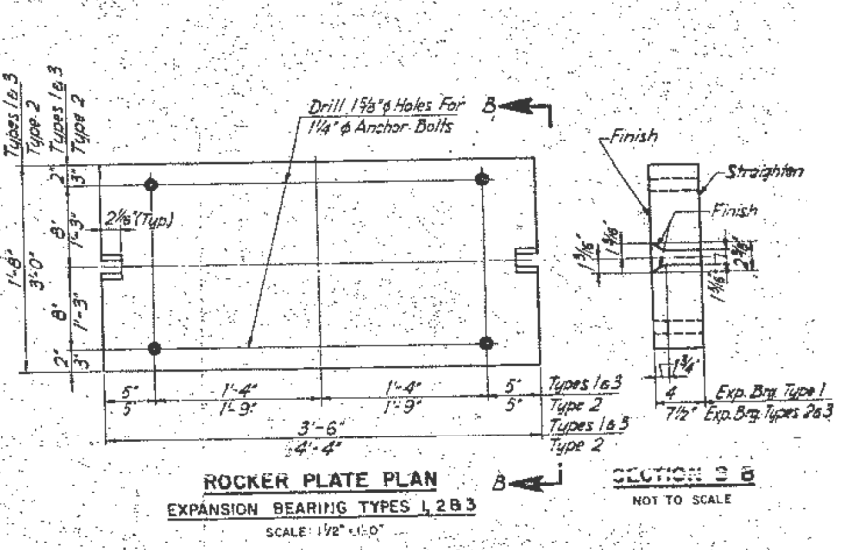
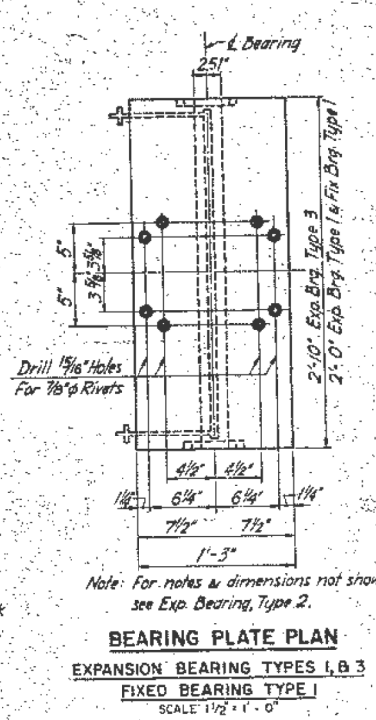
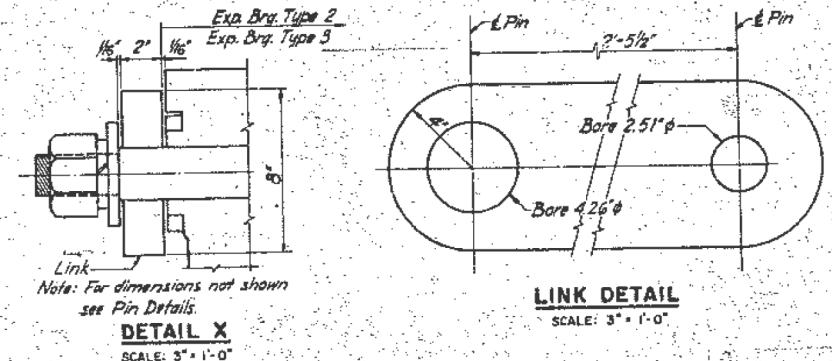
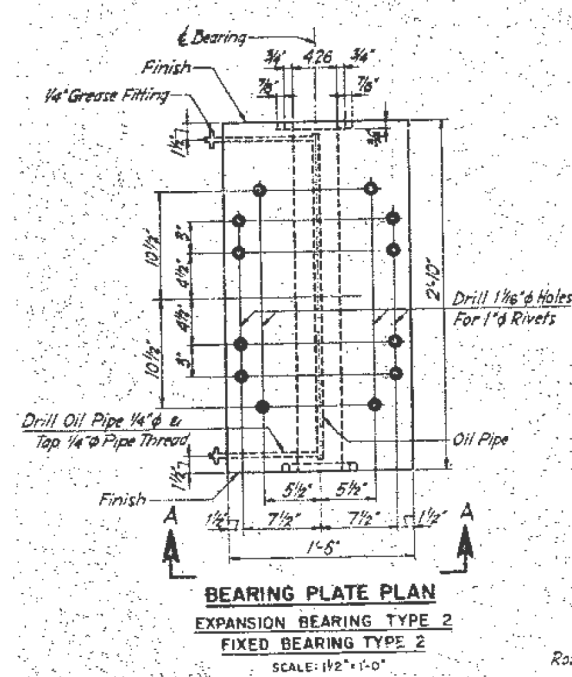
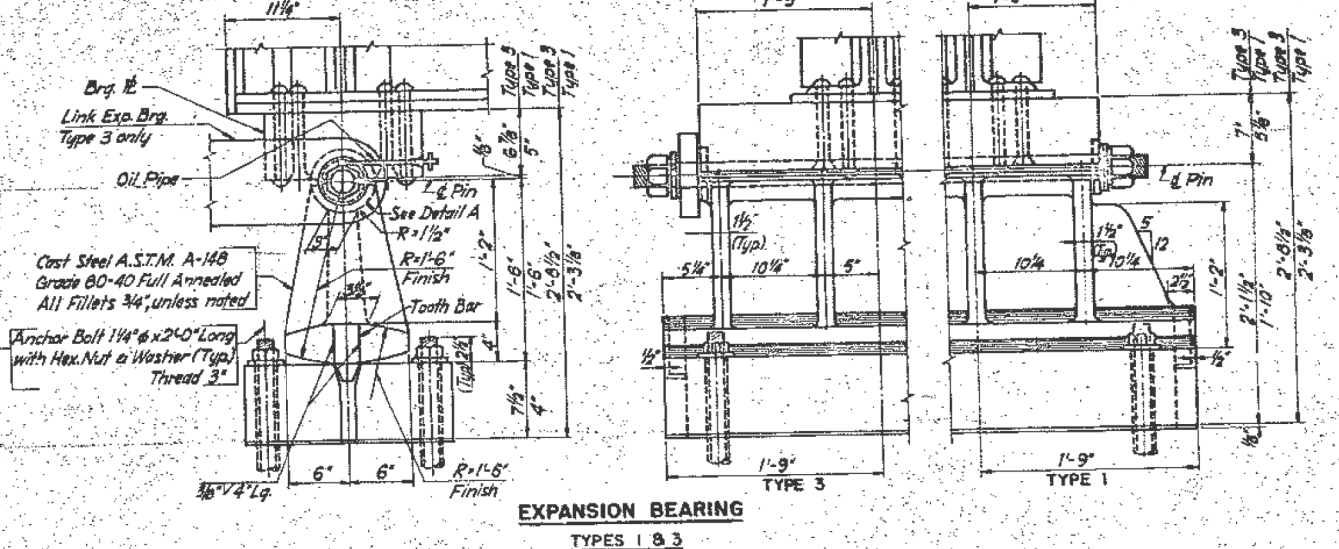
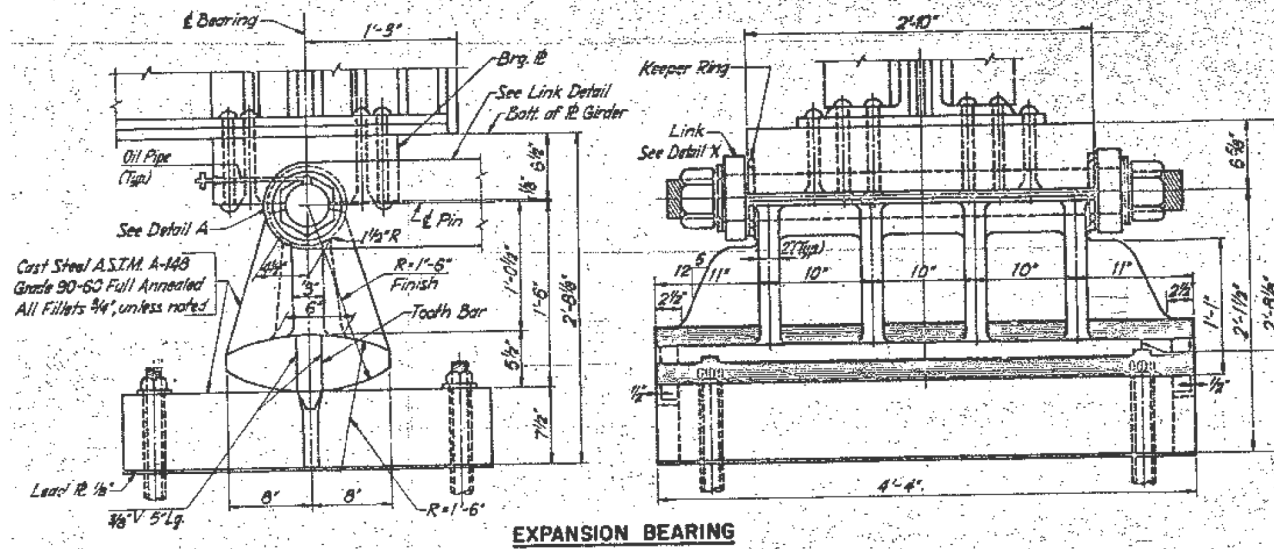
ILLINOIS DIVISION OF HIGHWAYS  
 SOUTHWEST EXPRESSWAY  
 A.T. & S.F. RY. - CICERO AVE. UNDERPASS  
**GENERAL PLAN**  
 SCALE: HORIZ. AS SHOWN VERT. AS SHOWN  
 DATE: JUNE, 1963 DRAWN BY: [Signature] CHECKED BY: A. Houser





# FOR INFORMATION ONLY

P.L. NO.	SECTION	EXPRESSWAY	TOTAL SHEETS	SHEET NO.
55	102-672-SB	SOUTHWEST	27	11
STA.	TO STA.			
FED. ROAD DIST. NO. 7	ALIGNED	FED. AID PROJECT		



	2 1/2" Pin	4 1/4" Pin
d	2.51"	4.26"
v <sub>1</sub>	2 7/8"	3 3/4"
v <sub>2</sub>	2"	2 3/8"
v <sub>3</sub>	3"	3 7/8"
h	1 1/2"	2 3/8"
s	5 3/2"	3 3/8"
t	1 3/2"	3 3/8"

**NOTES:**  
1. For Bearing Notes, Location Plan and Pin Details see Sheet

ILLINOIS DIVISION OF HIGHWAYS  
SOUTHWEST EXPRESSWAY  
A.T. & S.F.R.Y. - CICERO AVE. UNDERPASS

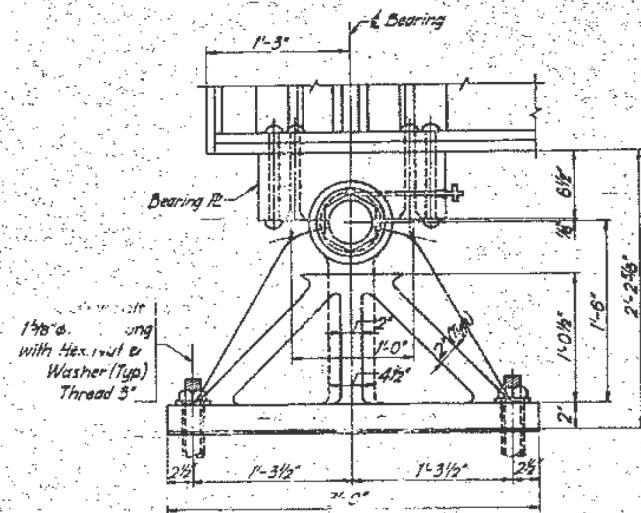
**EXPANSION BEARINGS**

SCALE: HORIZ. AS SHOWN VERT. AS SHOWN  
DATE: JUNE, 1963

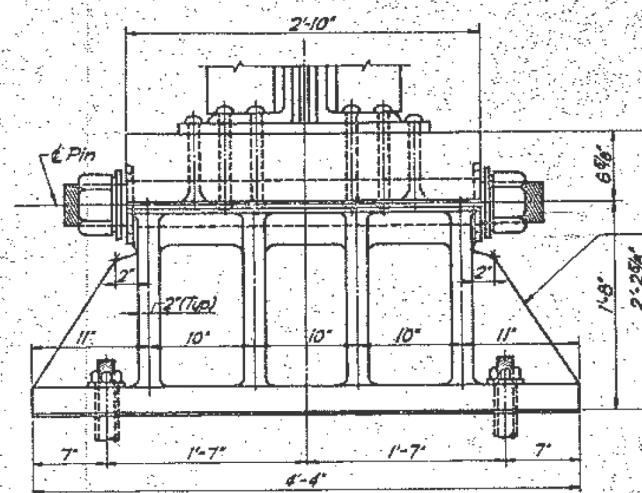
DRAWN BY: H. Harris  
CHECKED BY: C. George

# FOR INFORMATION ONLY

F.A. SHEET NO.	SECTION	STATIONING	TOTAL SHEETS	SHEET NO.
55	102-672-58	SOUTHWEST	27	12
STA.		TO STA.		
FOR ROAD DIST. NO. 7				



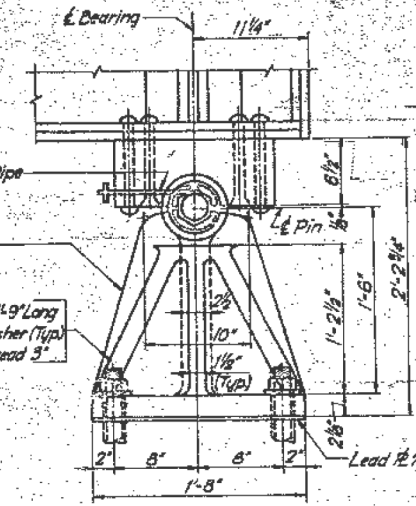
**FIXED BEARING  
TYPE 2**  
SCALE: 1/2" = 1'-0"



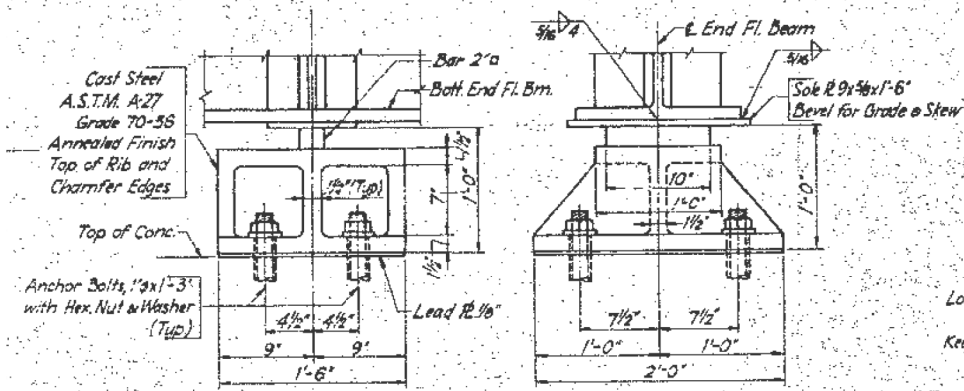
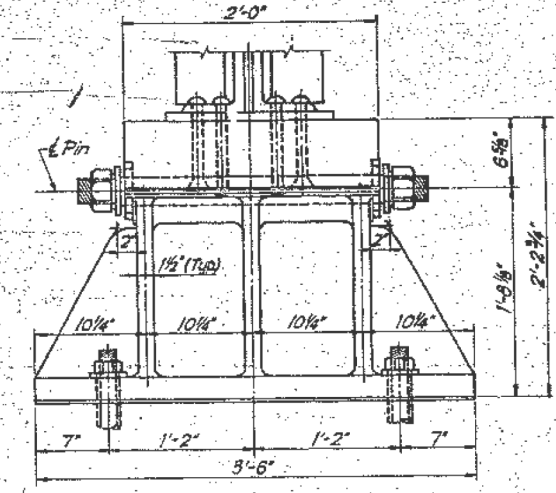
Note: For Notes & Dimensions not shown see Fixed Bearing, Type 1.

ASTM A-27, Grade 65-55, Full Annealed. All Fillets 3/4" unless noted.

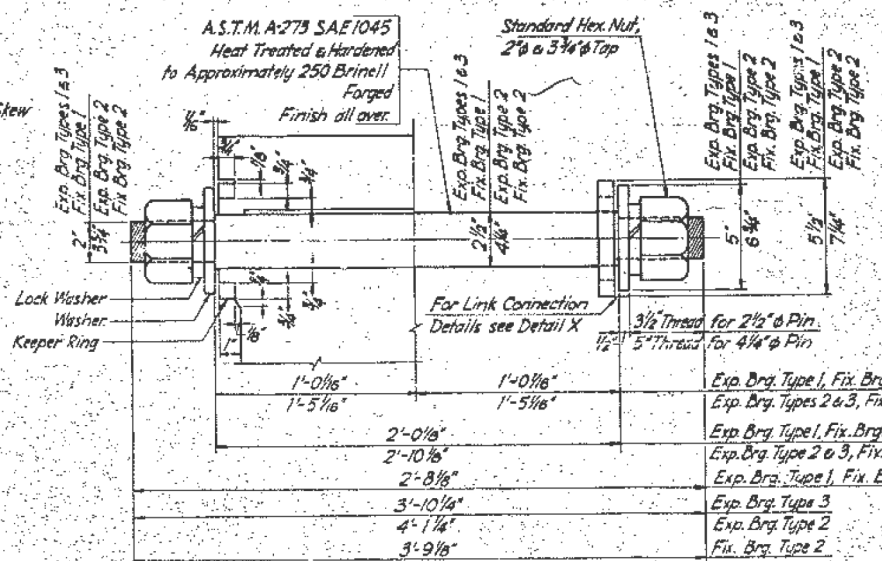
Anchor Bolt 1 1/4" x 1'-9" Long with Hex. Nut & Washer (Top) Thread 3"



**FIXED BEARING  
TYPE 1**  
SCALE: 1/2" = 1'-0"



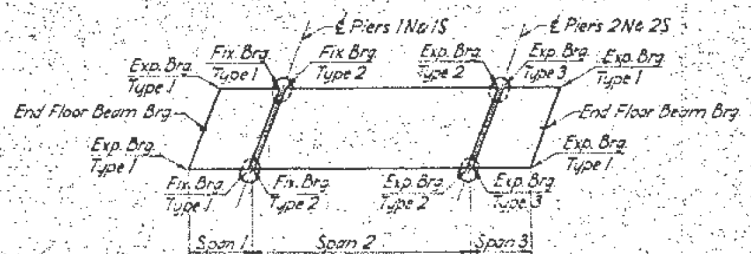
**END FLOOR BEAM BEARING**  
SCALE: 1/2" = 1'-0"



**PIN DETAILS**  
SCALE: 3" = 1'-0"

**NOTES:**

1. Maximum bearing on concrete, 1050 p.s.i.
2. All finished surfaces of pins, rollers and fixed castings, in contact with pins, shall be finished smooth to a roughness not to exceed 63 micra inches.
3. All material shall conform to A.S.T.M. A-36 with 0.2% copper added, except as noted.
4. All casting fillets shall be 3/4" radius, except as noted.
5. For Detail X see sheet 11



**LOCATION PLAN**  
NOT TO SCALE

ILLINOIS DIVISION OF HIGHWAYS  
SOUTHWEST EXPRESSWAY  
AT & S.E. RY. - CICERO AVE. UNDERPASS

**FIXED BEARINGS**

SCALE: HORIZ. AS SHOWN VERT. AS SHOWN  
DATE: JUNE, 1963 DRAWN BY: J. H. ... CHECKED BY: C. ...

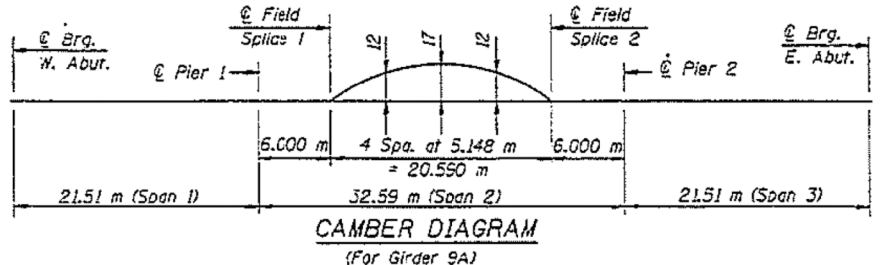
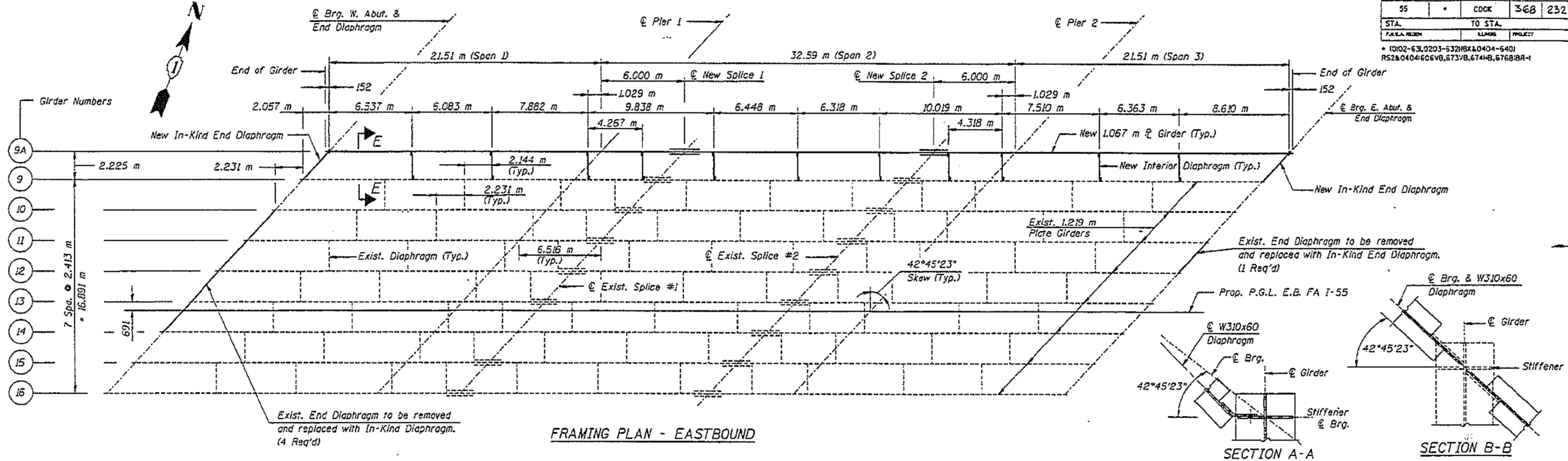




# FOR INFORMATION ONLY

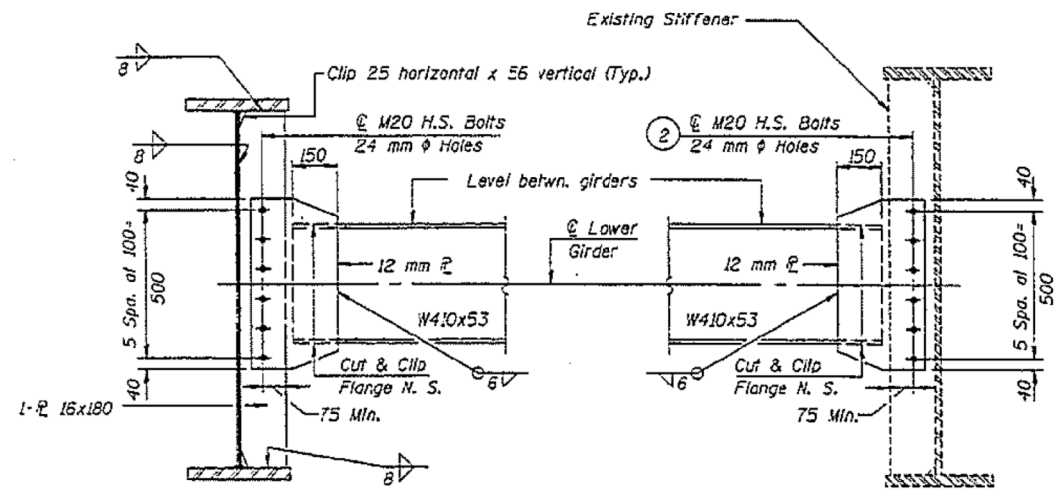
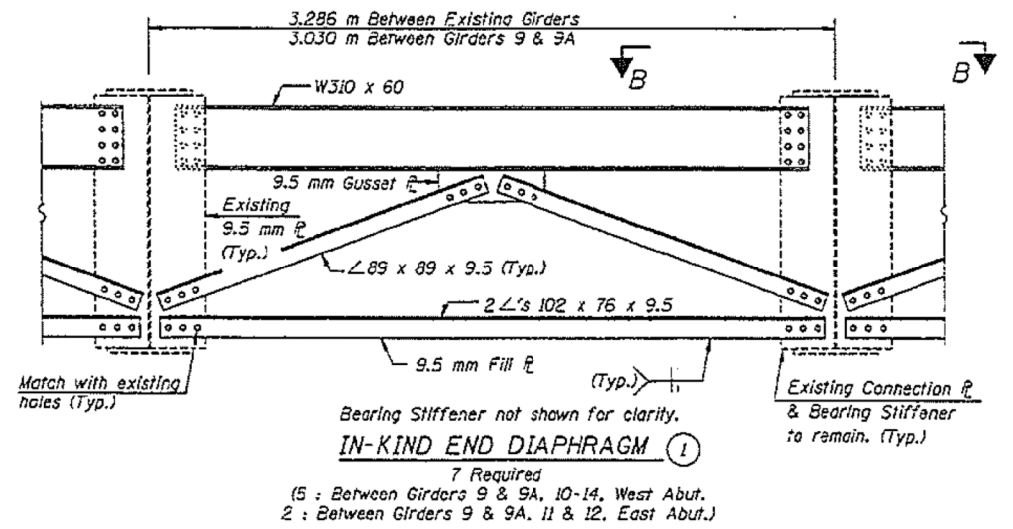
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55		COOK	368	232
STA.	TO STA.			
F.A.R. NO.	CLAS.	PROJECT		

\* 10102-63,0203-632HBK&0404-640  
RS2&0404(606VB,673VB,674HB,676B)BR-I



**BILL OF MATERIAL**

ITEM	UNIT	QTY.
Structural Steel Removal	kg	1670
Structural Steel Repair	kg	1513



Note: Two hardened washers shall be required over all 24 mm  $\phi$  holes.

- For In-Kind End Diaphragm field drill 26 mm  $\phi$  holes to match with existing holes. (Typ.) Cost is included in "Structural Steel Repair."
- Holes shall be drilled in existing bearing and intermediate stiffeners of Girder 9 in the field.

Work this sheet with sheet SCE-12 of 33.

ILLINOIS DEPARTMENT OF TRANSPORTATION  
FA I-55 OVER  
BNSF RAILWAY SEPARATION  
FRAMING & DIAPHRAGM DETAILS

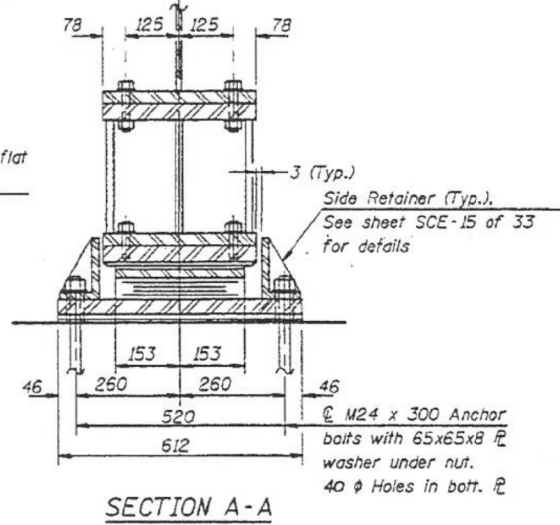
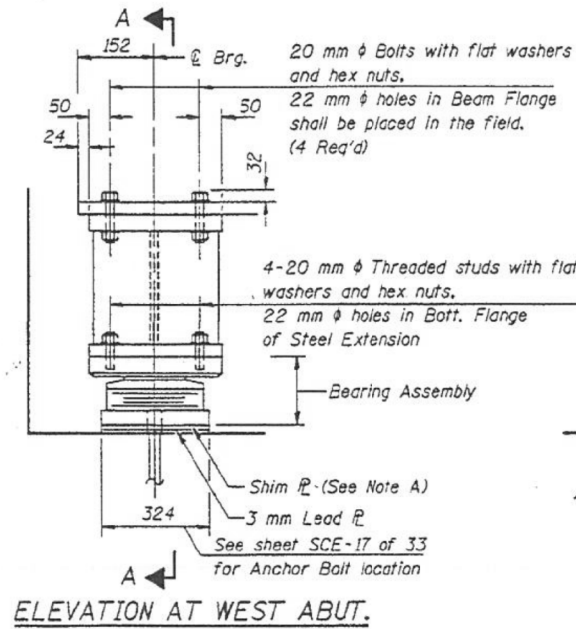
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RS2&0404(606VB,673VB,674HB,676B)BR-I  
COOK CO., IL. STA. 15+04L.71  
DATE: AUGUST 20, 1997

**BOYER**  
ENGINEERING, LTD.  
100 E. DuSable Ave. Suite 4  
Lombard, Illinois 60148  
TEL: 630-251-7575

**ab** **alfred benesch & company**  
CONSULTING ENGINEERS  
100 NORTH MICHIGAN AVENUE, CHICAGO, ILLINOIS 60611  
JOB NO. 2375

# FOR INFORMATION ONLY

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	*	COOK	308	234
STA.		TO STA.		
F.A. REGION		ILLINOIS PROJECT		
* (10102-631,0203-632)BK&O404-640 RS2&O404(606)VB,673VB,674HB,676B)BR-1				



**SHIM TABLE TYPE I**

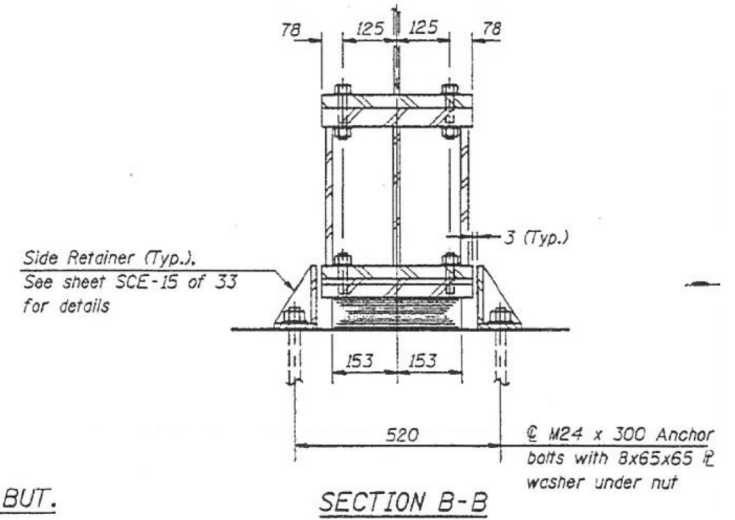
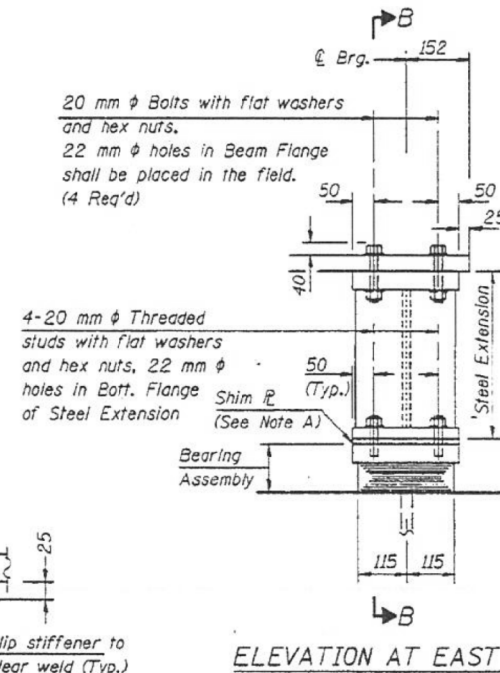
No. Shims	Thickness (mm)			
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8	8	8	8	8

Pre-drill Holes to Match Bearing Plates.

**SHIM TABLE TYPE II**

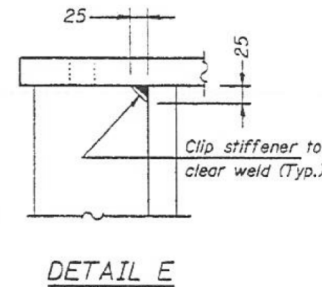
No. Shims	Thickness (mm)			
	3	6	9	12
8	8	8	8	8

Pre-drill Holes to Match Bearing Plates.

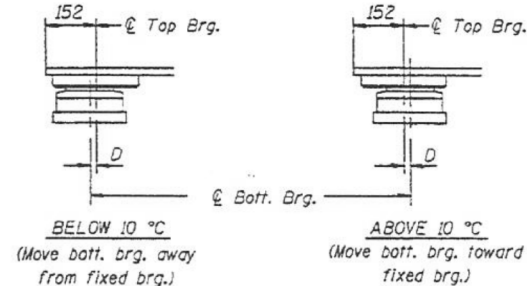
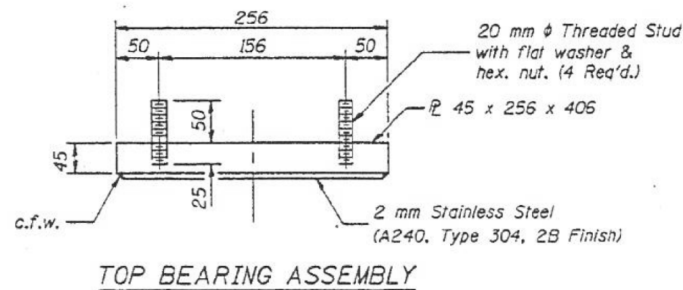


**TYPE II TFE ELASTOMERIC EXP. BRG.**  
(West Abutment: Girders 9-16)

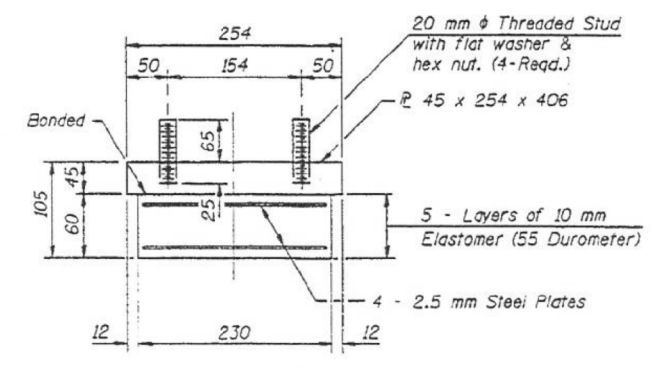
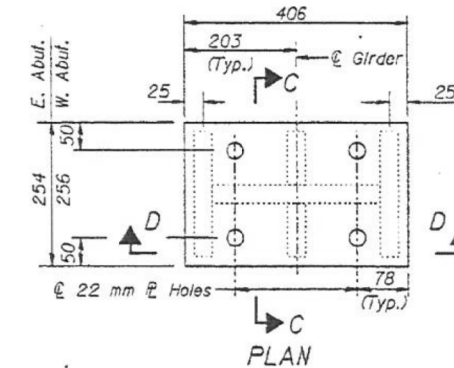
Note A  
Shim as Required to Maintain Existing Bottom of girder Elevation.  
(See Shim Table)



**TYPE I ELASTOMERIC EXP. BRG.**  
(East Abutment: Girders 9-16)



D = 1 mm per each 10 m of expansion for every 8  $^{\circ}$ C temp. change from the normal temp. of 10  $^{\circ}$ C.

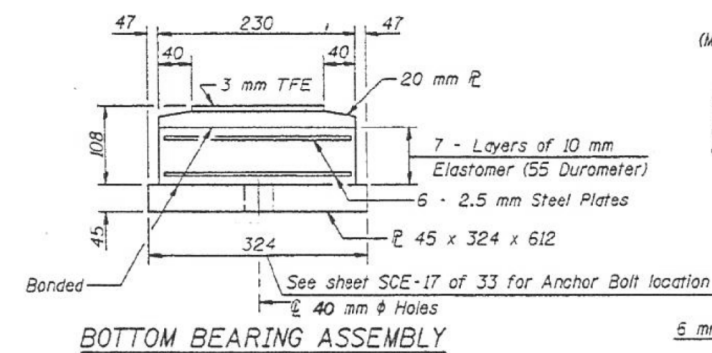


Note: Shim plates shall not be placed under Bearing Assembly.

**BILL OF MATERIAL**

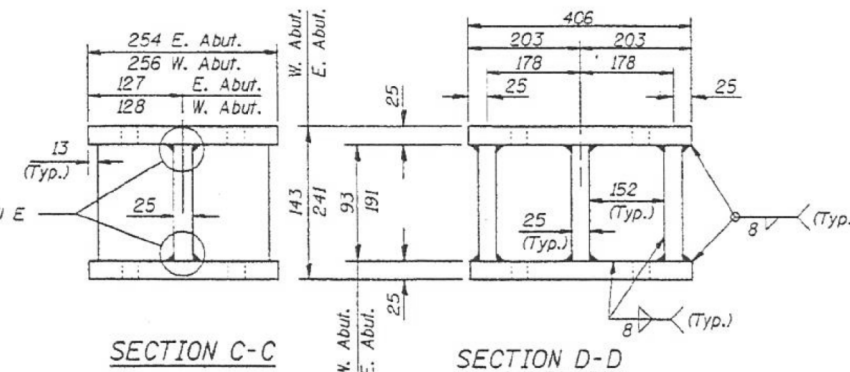
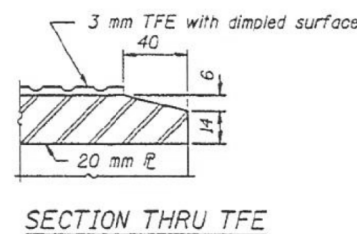
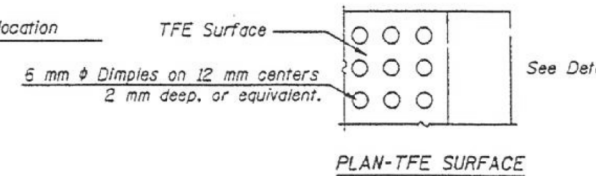
Item	Unit	Total
Elastomeric Bearing Assembly, Type I	Each	8
Elastomeric Bearing Assembly, Type II	Each	8

Work this sheet with sheets SCE-14 & 15 of 33.  
See sheet SCE-26 of 33 for Anchor Bolt Details.



Note: The 3 mm TFE sheet shall be bonded directly to the top steel plate with a two-component, medium viscosity epoxy resin, conforming to the requirements of the Federal Specification MMM-A-134, Type I. The bond agent shall be applied on the full area of the contact surfaces.

Bonding of 3 mm TFE sheet during vulcanizing process will be permitted provided the process and method of adjusting assembly height is approved by the Engineer.



**STEEL EXTENSION DETAIL**  
(For Existing Girders 9-16)

**BOYER**  
ENGINEERING, LTD.  
100 E. DuSable Ave. Suite 4  
Springfield, Illinois 62701 (314) 225-1998

**ab** dlfred benesch & company  
CONSULTING ENGINEERS  
208 NORTH MICHIGAN AVENUE, CHICAGO ILLINOIS 60601  
JOB NO. 2975

ILLINOIS DEPARTMENT OF TRANSPORTATION  
FA I-55 OVER  
BNSF RAILWAY SEPARATION  
BEARING DETAILS  
ABUTMENTS

SN: 016-0008(EB) SECTION: (10102-631,0203-632)BK&O404-640  
RS2&O404(606)VB,673VB,674HB,676B)BR-1  
COOK CO., IL. STA. 15+041.71  
DATE: AUGUST 20, 1997

FILE NAME	USER NAME	DESIGNED	REVISED
p:\11\084EBID\INTEG\11\Illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI207\Drawings\Design\DI20718-sht-plen.dgn	saintwlich	-	-
		CHECKED	REVISIED
		DATE	REVISIED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

LOCATION 4 (SN 016-0008)  
STRUCTURAL STEEL BEARINGS

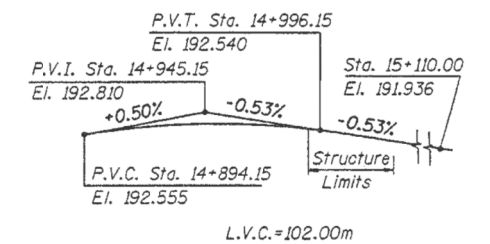
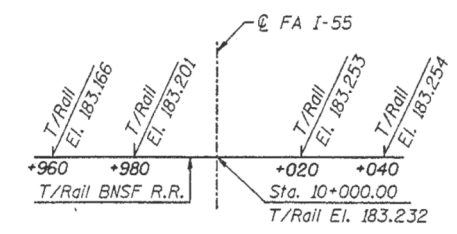
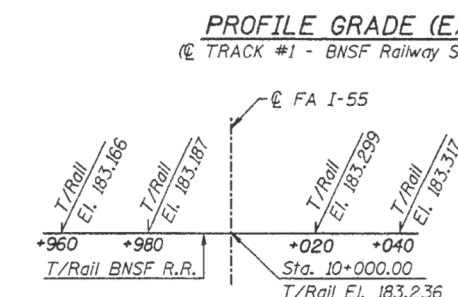
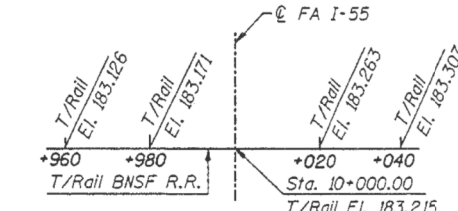
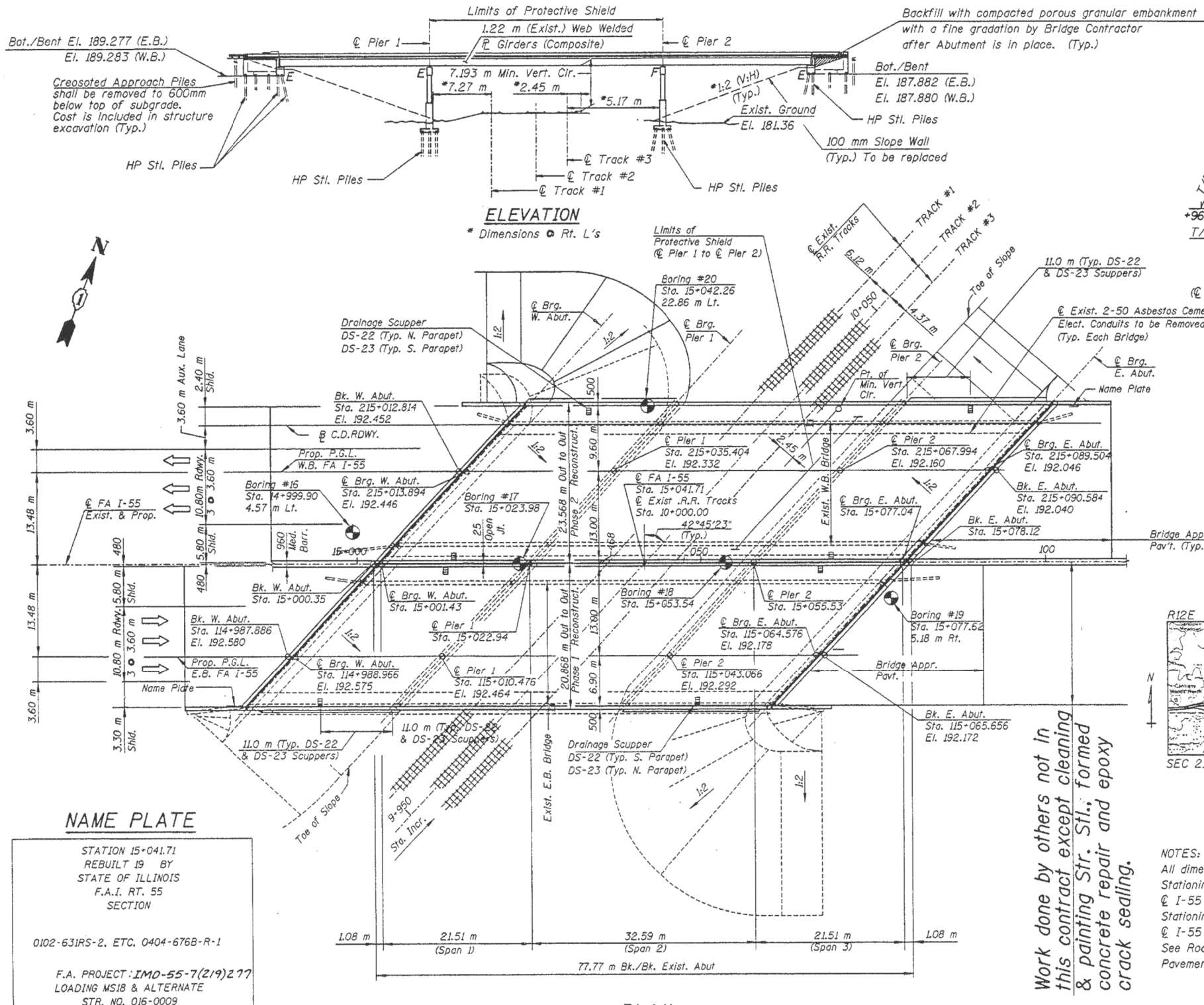
SCALE: SHEET 3 OF 3 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2017-048BP	COOK	26	18
CONTRACT NO. 62F80				
ILLINOIS FED. AID PROJECT				

# FOR INFORMATION ONLY

T.B.M. - Chisled square at S.E. Corner of East Abutment of W.B. FA I-55 over BNSF Railway access road. El. 192.436

Existing Structures: No. 016-0008 (E.B.) and No. 016-0009 (W.B.) built as FA I-55 under section 0404-606-VB, project IG-55-7(84) 274 in 1964. Both structures are Three-Span continuous. Non-composite, Welded Plate Girder Bridges with reinforced concrete decks (77.77 m Bk. to Bk. Abuts. & 18.12 m O. to O. deck). The substructures consist of Pile Bent Abutments and Multiple Column Piers supported by Piles. Existing structures are to be widened and the deck removed and replaced. Traffic shall be maintained by the adjacent E.B. bridge during construction.



PROP. PROFILE GRADE  
(W.B. FA I-55)

### DESIGN SPECIFICATIONS

1992 AASHTO with 1993, 1994 & 1995 Interims.

### LOADING MS18 & ALTERNATE

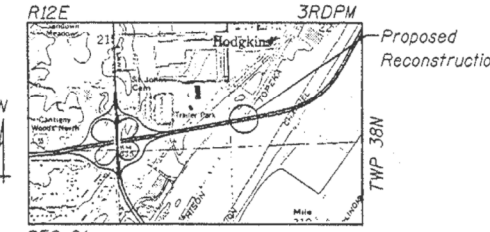
Allow 1.2kN/m<sup>2</sup> for future wearing surface.

### DESIGN STRESSES

NEW CONSTRUCTION	OLD CONSTRUCTION
f <sub>c</sub> = 24 MPa	f <sub>c</sub> = 9.65 MPa
f <sub>y</sub> = 400 MPa (Reinf.)	V <sub>c</sub> = 0.52 MPa (Figs.)
f <sub>s</sub> = 138 MPa (Str. Stl.)	f <sub>y</sub> = 138 MPa (Reinf.)
	f <sub>s</sub> = 138 MPa (Str. Stl.)
	n = 10

### SEISMIC DATA

Seismic Performance Category (SPC)=A  
Bedrock Acceleration Coefficient (A)=0.04g  
Site Coefficient (S)=1.0



LOCATION MAP

Work done by others not in this contract except cleaning & painting Str. Stl. formed concrete repair and epoxy crack sealing.

NOTES:  
All dimensions are in millimeters (mm) except as noted.  
Stationing Along P.G.L. W.B. Equals Stationing of @ I-55 plus 200,000.  
Stationing Along P.G.L. E.B. Equals Stationing of @ I-55 plus 100,000.  
See Roadway Plans for Bridge Approach Pavement Details.

WESTBOUND CONTRACT ONLY  
EASTBOUND FOR INFORMATION ONLY

ILLINOIS DEPARTMENT OF TRANSPORTATION  
FA I-55 OVER  
BNSF RAILWAY SEPARATION  
GENERAL PLAN AND ELEVATION

SN: 016-0009(WB) SECTION: 0102-63IRS-2, ETC, 0404-676B-R-1  
COOK CO., IL. STA. 15+041.71  
DATE: JUNE 29, 1999

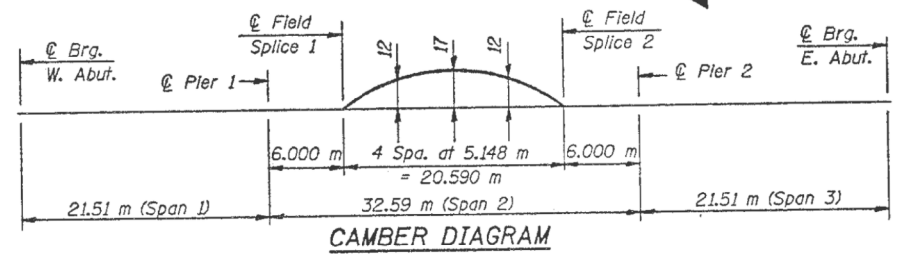
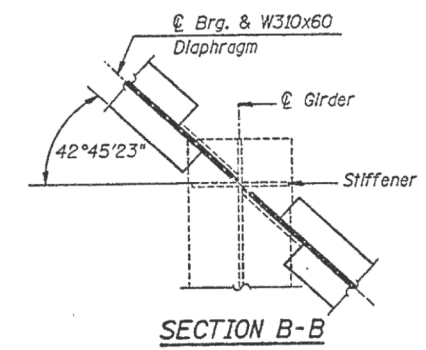
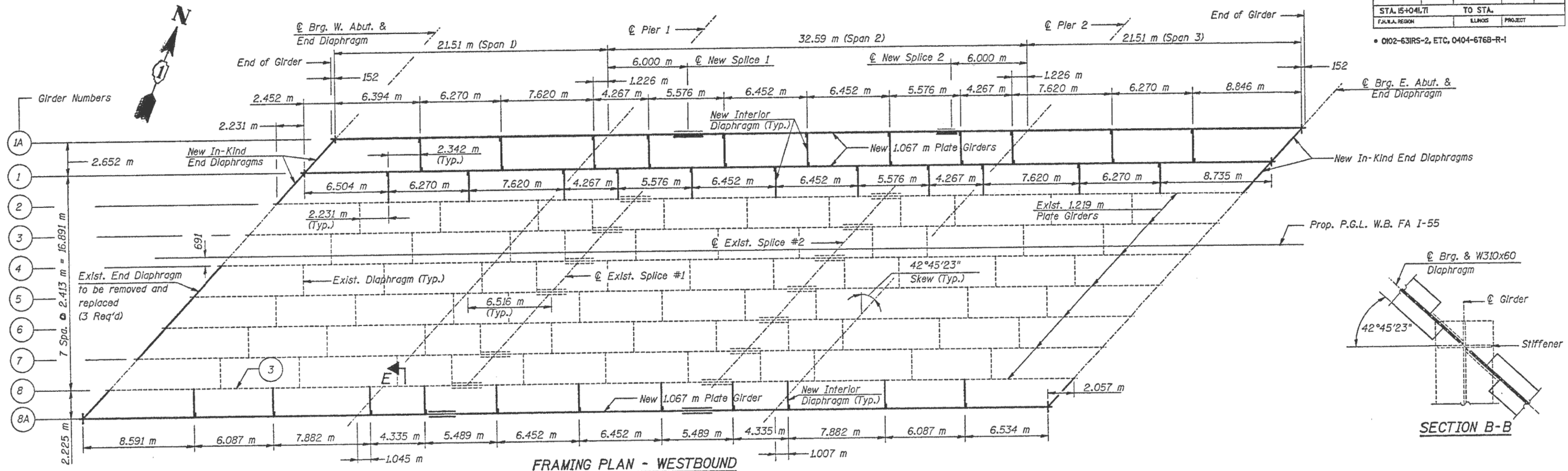
**BOYER**  
ENGINEERING, LTD.  
alfred benesch & company  
CONSULTING ENGINEERS

FILE NAME =	USER NAME = sornitwich	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	LOCATION 5 (SN 016-0009) PLAN AND PROFILE	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw\11084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI207\Drawings\Design\DI20718-sht-plan.dgn		CHECKED -	REVISED -			VAR.	2017-048BP	COOK	26	19	
Default	PLOT SCALE = 100.0000' / in.	DATE -	REVISED -			CONTRACT NO. 62F80					
	PLOT DATE = 11/13/2017					SCALE:	SHEET 1 OF 3 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	

# FOR INFORMATION ONLY

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55		COOK	594	337
STA. 15+04.71		TO STA.		
F.A.R.A. REGION		ILLINOIS PROJECT		

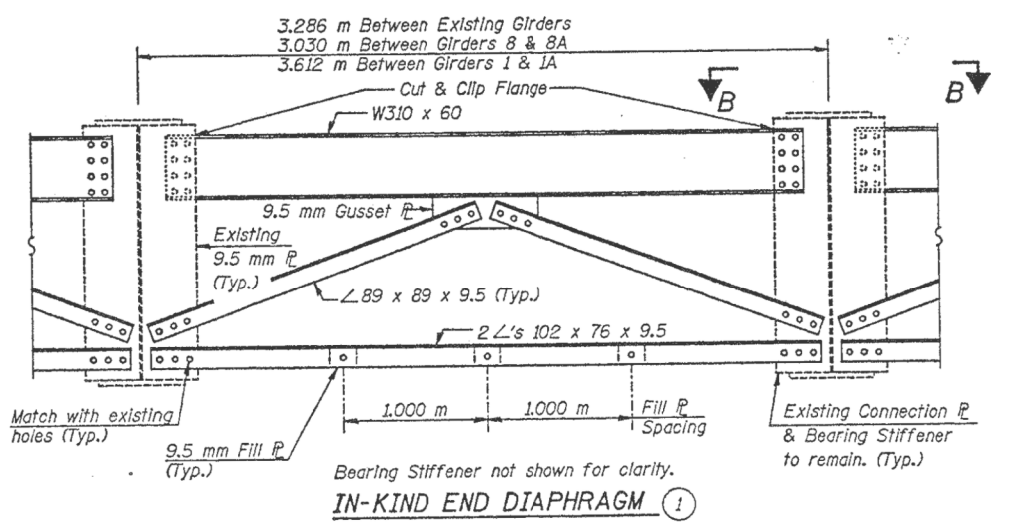
• 0102-63IRS-2, ETC, 0404-676B-R-1



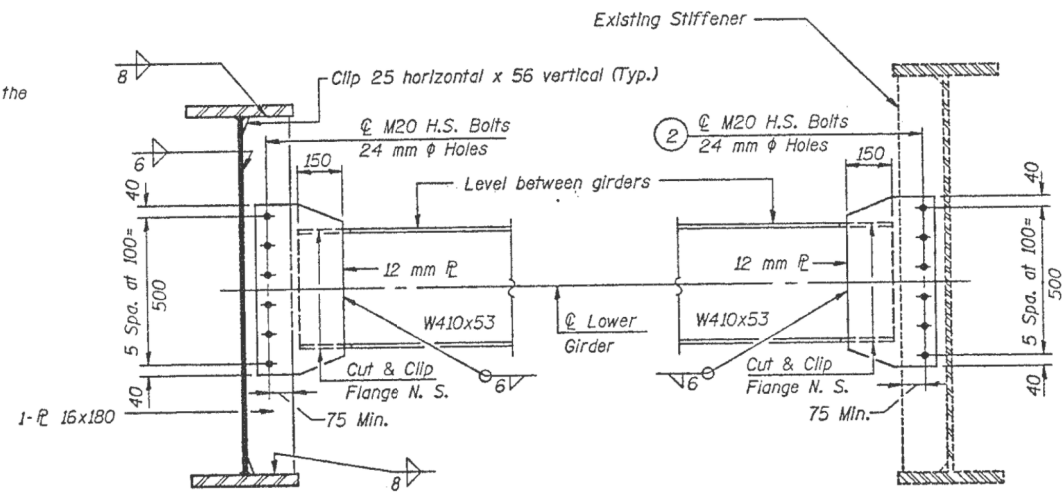
**BILL OF MATERIAL**

ITEM	UNIT	QTY.
Structural Steel Removal	kg	25,285

\*\* Structural Steel Removal includes end diaphragm replacements and girder 1 removal including adjacent diaphragms. See special provision for Structural Steel Removal



3 After the 20mm  $\phi$  stud bolts used to support the floor drains along the existing median fascia girder are removed and before the cleaning and painting operations begin, the remaining 24mm  $\phi$  holes shall be filled with M22 high strength bolts and tightened. The contractor shall verify hole diameters remaining in web and adjust bolt diameter accordingly. The cost shall be included with "Removal of Existing Concrete Deck."



**SECTION E-E**  
**NEW INTERIOR DIAPHRAGM**  
33 Required

- 1 For In-Kind End Diaphragm field drill 26 mm  $\phi$  holes to match with existing holes. (Typ.) Cost is included in "Furnishing and Erecting Structural Steel".
- 2 Holes shall be drilled in existing bearing and intermediate stiffeners of Girders 2 & 8 in the field.

ILLINOIS DEPARTMENT OF TRANSPORTATION  
FA I-55 OVER  
BNSF RAILWAY SEPARATION  
FRAMING & DIAPHRAGM DETAILS

SN: 016-0009(WB) SECTION: 0102-63IRS-2, ETC, 0404-676B-R-1  
COOK CO., IL.

STA. 15+04.71  
DATE: JUNE 29, 1999

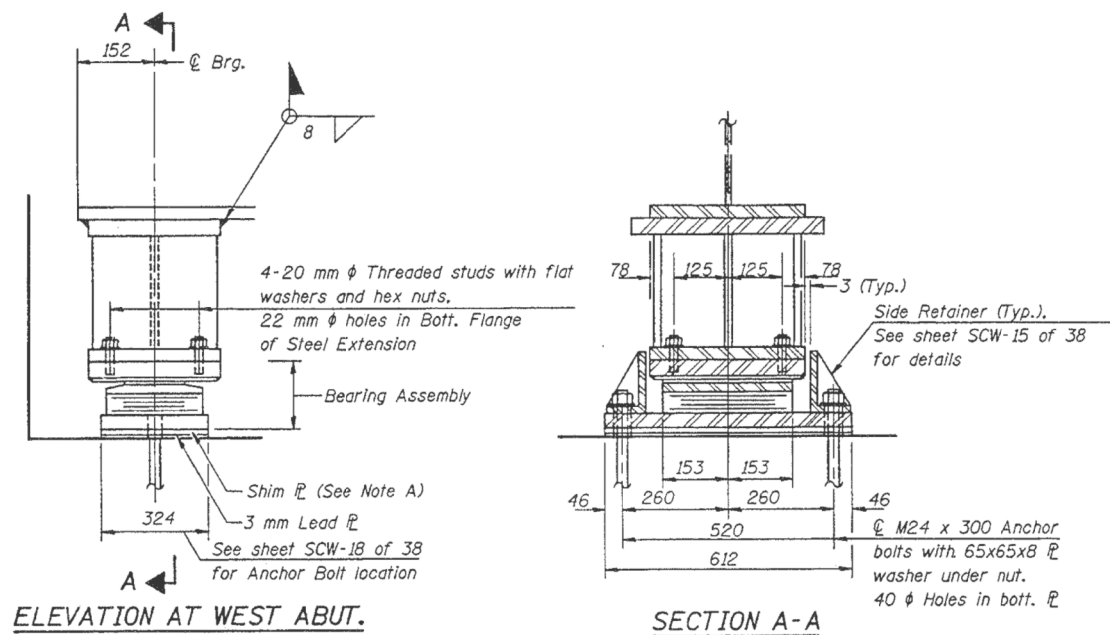
**BOYER**  
ENGINEERING, LTD.  
CONSULTING ENGINEERS  
305 NORTH MICHIGAN AVENUE, CHICAGO ILLINOIS 60604  
JOB NO. 2375



# FOR INFORMATION ONLY

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	•	COOK	594	340
STA.		TO STA.		
FALSA REGION		ILLINOIS	PROJECT	

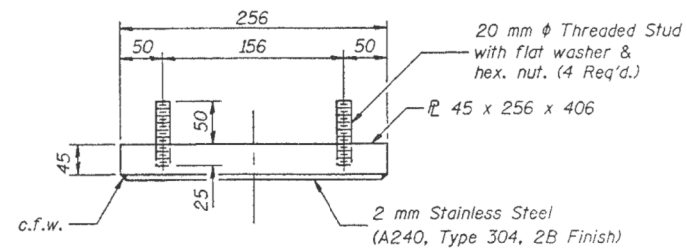
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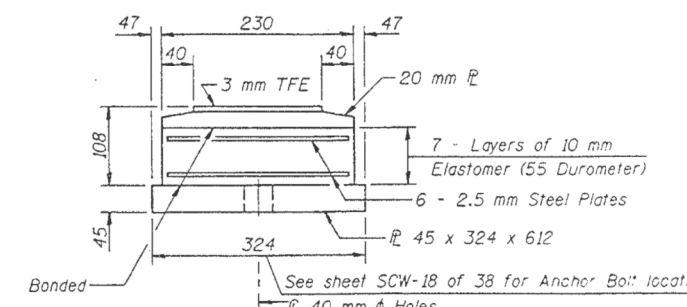
## TYPE II TFE ELASTOMERIC EXP. BRG.

(West Abutment: Girders 2-8)

**Note A**  
Shim as Required to Maintain Existing Bottom of girder Elevation.  
(See Shim Table)

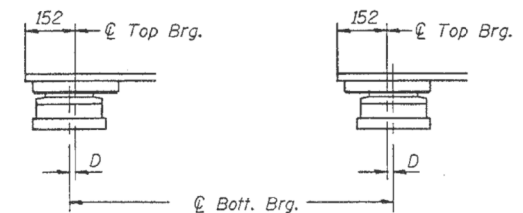


## TOP BEARING ASSEMBLY



Note: The 3 mm TFE sheet shall be bonded directly to the top steel plate with a two-component, medium viscosity epoxy resin, conforming to the requirements of the Federal Specification MMM-A-134, Type I. The bond agent shall be applied on the full area of the contact surfaces.

Bonding of 3 mm TFE sheet during vulcanizing process will be permitted provided the process and method of adjusting assembly height is approved by the Engineer.

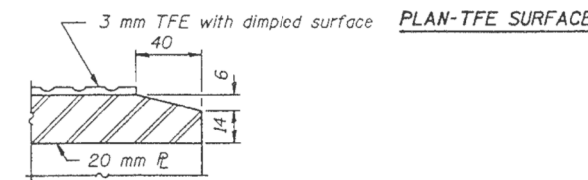
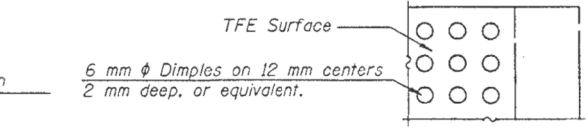


BELOW 10  $\bar{r}$ C  
(Move bott. brg. away from fixed brg.)

ABOVE 10  $\bar{r}$ C  
(Move bott. brg. toward fixed brg.)

**SETTING ANCHOR BOLTS AT EXP. BRG.**

D = 1 mm per each 10 m of expansion for every 8  $\bar{r}$ C temp. change from the normal temp. of 10  $\bar{r}$ C.



## SHIM TABLE TYPE I

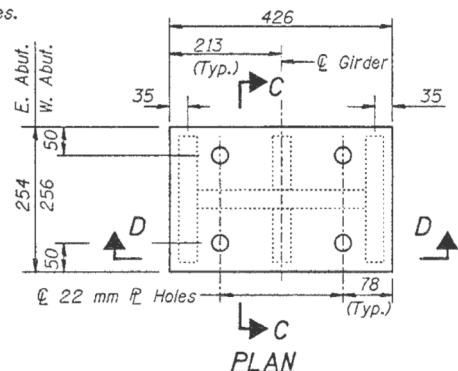
	Thickness (mm)			
	3	6	9	12
No. Shims	8	8	8	8

Pre-drill Holes to Match Bearing Plates.

## SHIM TABLE TYPE II

	Thickness (mm)			
	3	6	9	12
No. Shims	8	8	8	8

Pre-drill Holes to Match Bearing Plates.

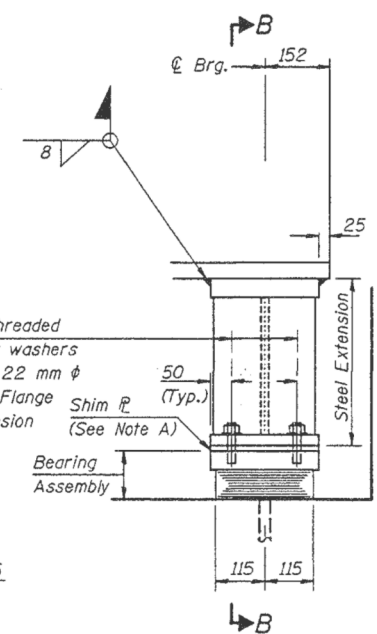


## STEEL EXTENSION DETAIL

(For Existing Girders 2-8)

## JACK AND REMOVE EXISTING BEARINGS PROCEDURE

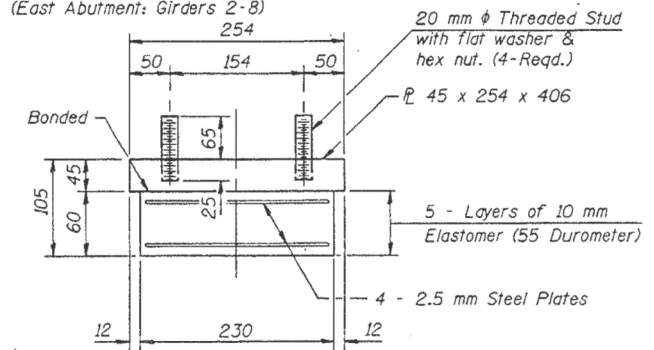
- The contractor shall submit for approval by the Engineer plans for jacking prior to commencing any work at the bearings. At the abutments, Dead Load = 20 kN without concrete deck and jack capacity = 35 kN. At Pier 1, Dead Load = 88 kN without concrete deck and jack capacity = 155 kN.
- Jacking and removing existing bearings shall be done after deck removal is completed and before the new deck is poured.
- All beams at each abutment may be lifted simultaneously, or if lifted individually, the maximum lift shall be 3mm. Simultaneous jacking shall be limited to a maximum lift of 6mm.
- The new elastomeric bearings shall be placed and the jacks shall be lowered before the new deck is poured.



## ELEVATION AT EAST ABUT.

## TYPE I ELASTOMERIC EXP. BRG.

(East Abutment: Girders 2-8)



## BEARING ASSEMBLY

Note: Shim plates shall not be placed under Bearing Assembly.

## BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly, Type I	Each	7
Elastomeric Bearing Assembly, Type II	Each	7
Jack and Remove Existing Bearings	Each	14

Work this sheet with sheets SCW-14 & 15 of 38. See sheet SCW-31 of 38 for Anchor Bolt Details.

ILLINOIS DEPARTMENT OF TRANSPORTATION  
FA I-55 OVER  
BNSF RAILWAY SEPARATION  
BEARING DETAILS - ABUTMENTS

SN: 016-0009(WB) SECTION: 0102-63IRS-2, ETC, 0404-676B-R-1  
COOK CO., IL.

STA. 15+041.71  
DATE: JUNE 29, 1999

FILE NAME =	USER NAME = sornitwch	DESIGNED -	REVISED -
pw\11084EBIDINTEG.illinois.gov\PIWIDOTDocuments\DOT Offices\District 1\Projects\DI20718\Design\DI20718-sht-plen.dgn		REVISOR -	REVISOR -
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISOR -
	PLOT DATE = 11/13/2017	DATE -	REVISOR -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

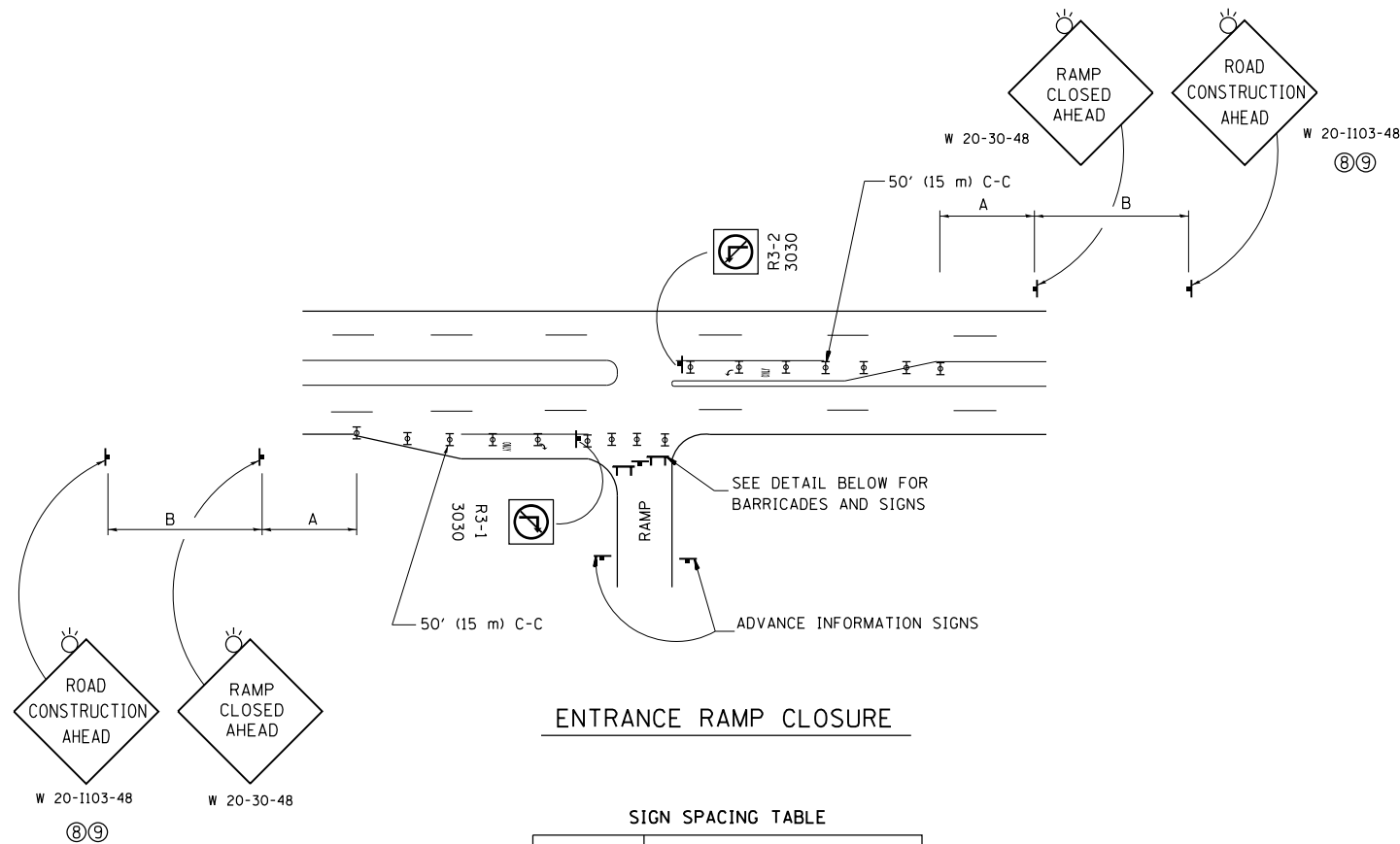
LOCATION 5 (SN 016-0009)  
STRUCTURAL STEEL BEARINGS

SCALE: SHEET 3 OF 3 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2017-048BP	COOK	26	21
CONTRACT NO. 62F80				
ILLINOIS FED. AID PROJECT				

**BOYER**  
ENGINEERING, LTD.  
100 E. Des Plaines Ave., Suite 4  
Springfield, Illinois 62707 (618) 229-7900

**ab** alfred benesch & company  
CONSULTING ENGINEERS  
208 NORTH MICHIGAN AVENUE, CHICAGO ILLINOIS 60601  
JOB NO. 2975

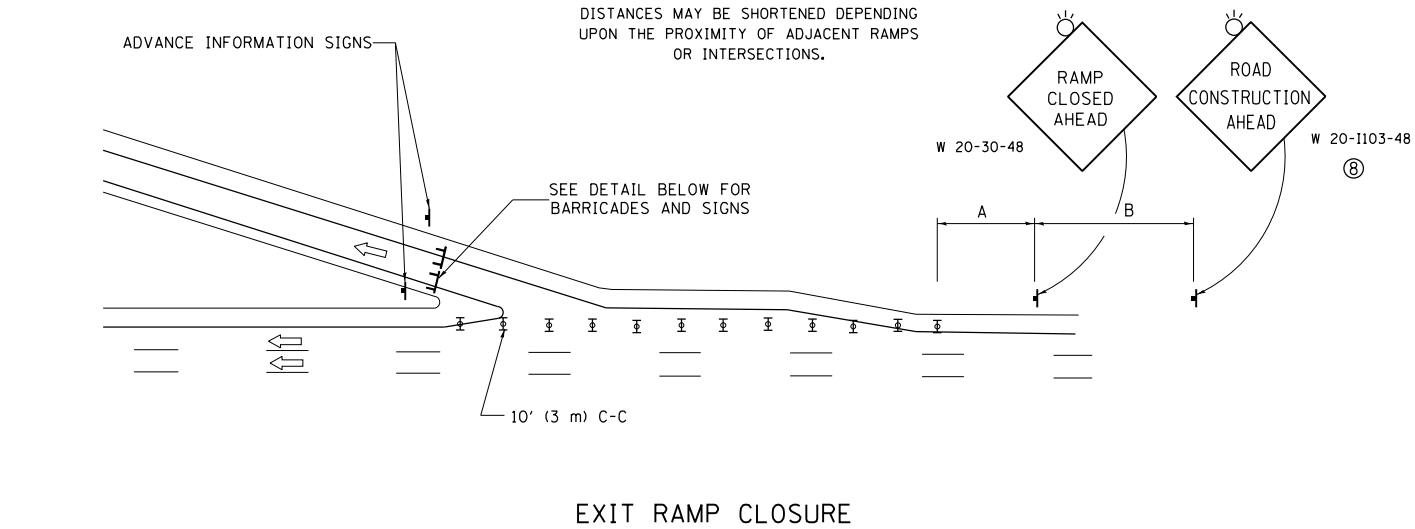


**ENTRANCE RAMP CLOSURE**

**SIGN SPACING TABLE**

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY <24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

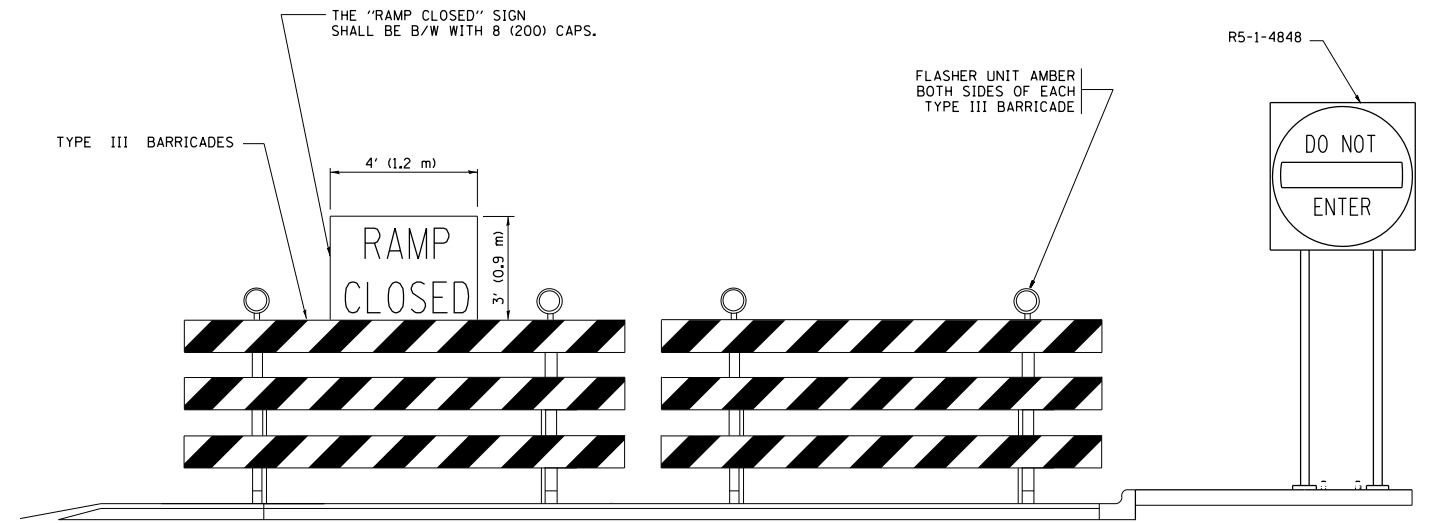
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



**EXIT RAMP CLOSURE**

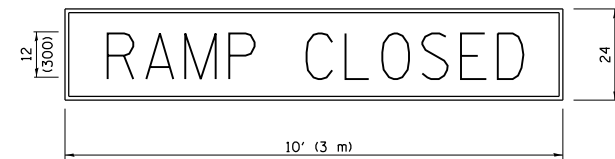
**SYMBOLS**

- ⊥ TYPE II BARRICADE OR DRUM WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- ⊓ TYPE III BARRICADE WITH 2 FLASHING LIGHTS



**DETAIL FOR REQUIRED BARRICADES & SIGNS**

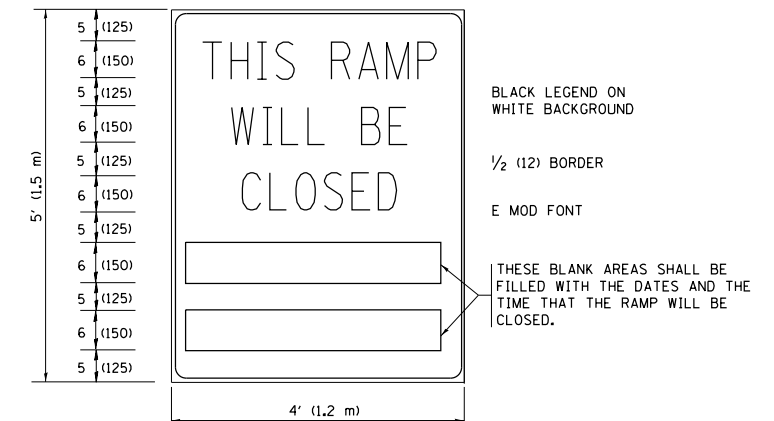
**RAMP CLOSURE ADVANCE WARNING SIGN**



BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY  
E MOD FONT  
1 (25) BORDER

THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.

**RAMP CLOSURE ADVANCE INFORMATION SIGN**



BLACK LEGEND ON WHITE BACKGROUND

1/2 (12) BORDER

E MOD FONT

THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

**GENERAL NOTES:**

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

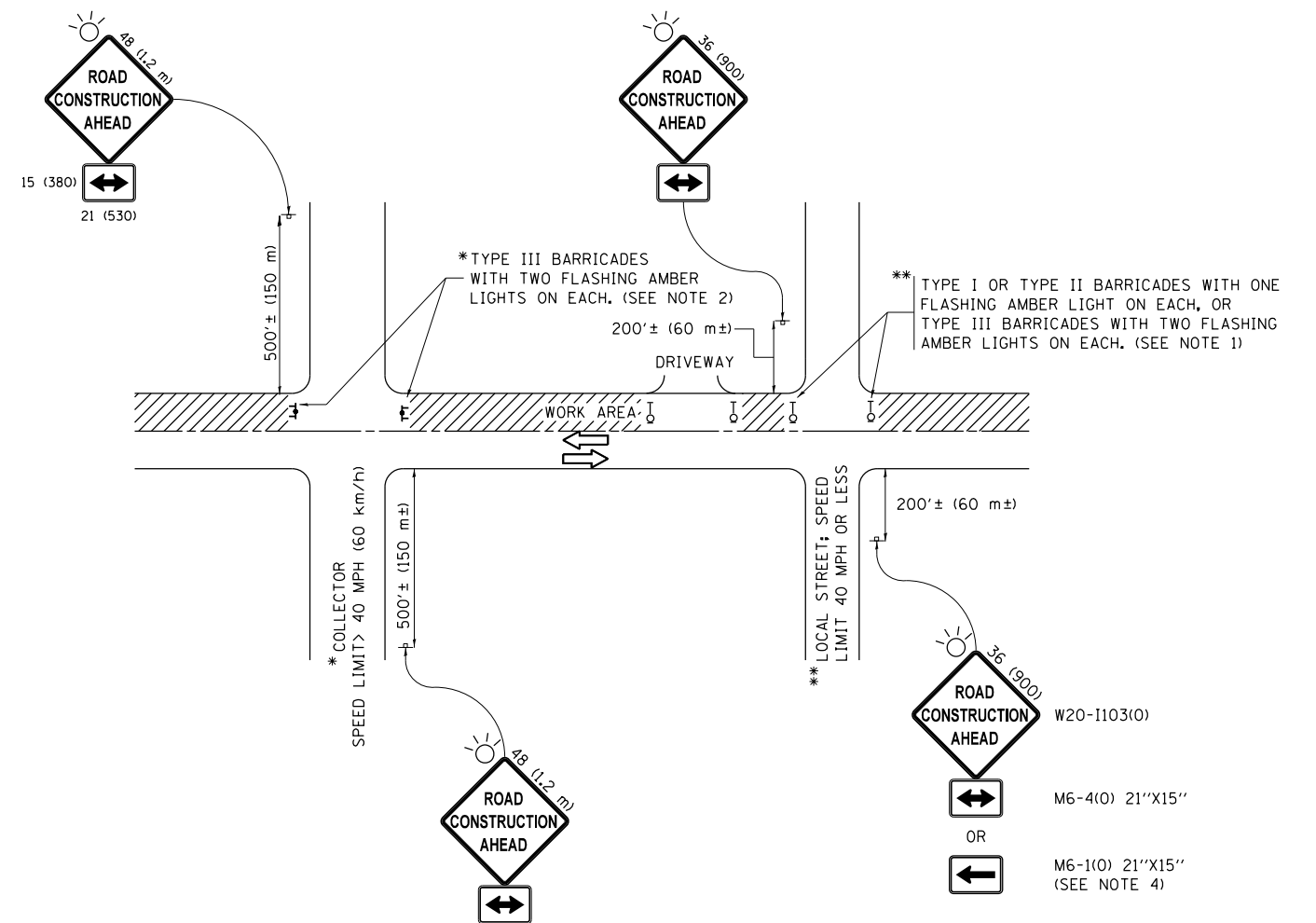
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		DATE - 02-83	REVISED - SPB 12-09
			REVISED - MD 06-13

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ENTRANCE AND EXIT RAMP  
CLOSURE DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	2017-048BP	COOK	26	22
	TC-08	CONTRACT NO.	62F80	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = s01ntwlch	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI2017\Drawings\Design\DistStd.dgn			REVISED - T. RAMMACHER 01-06-00
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 10/20/2017	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

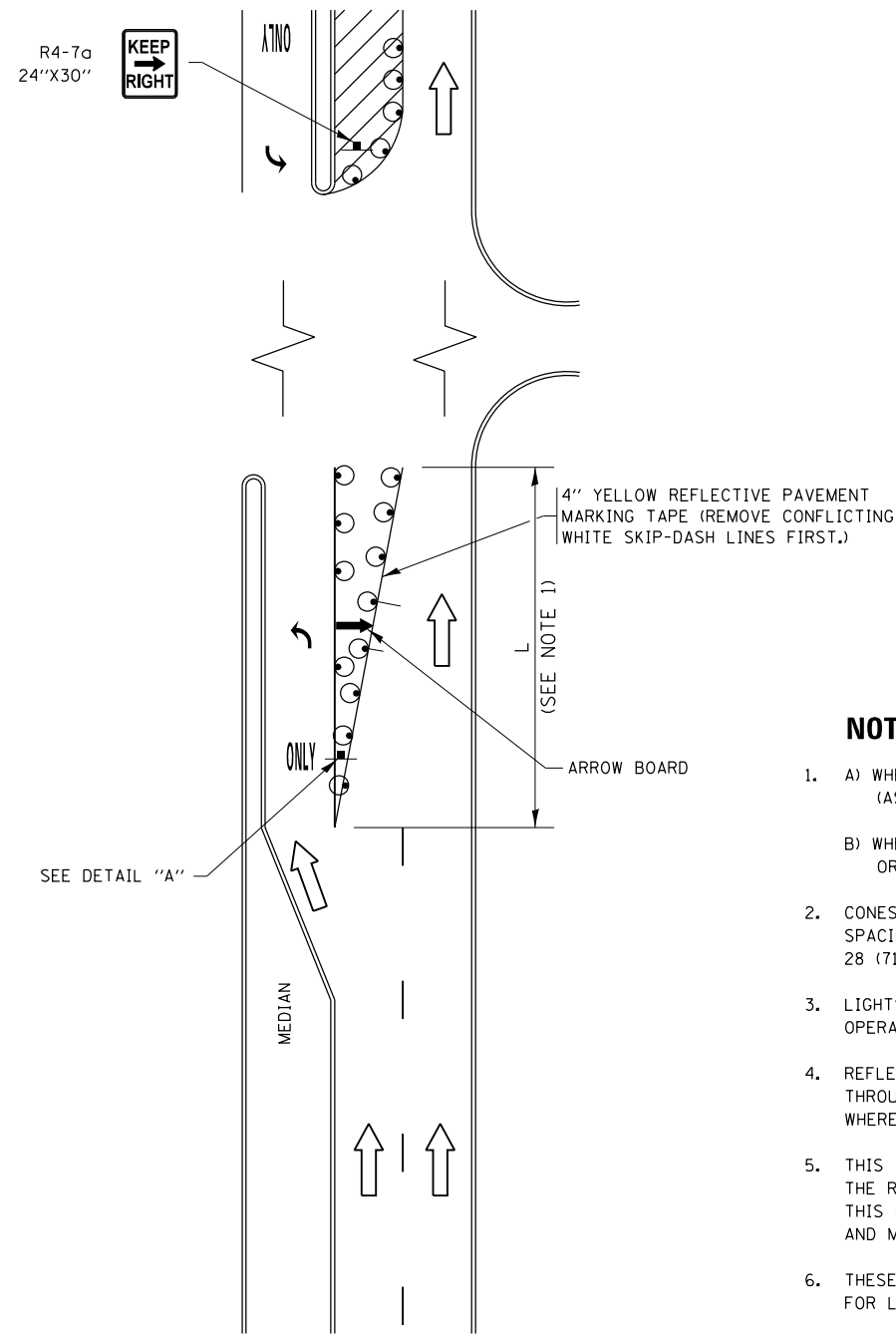
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

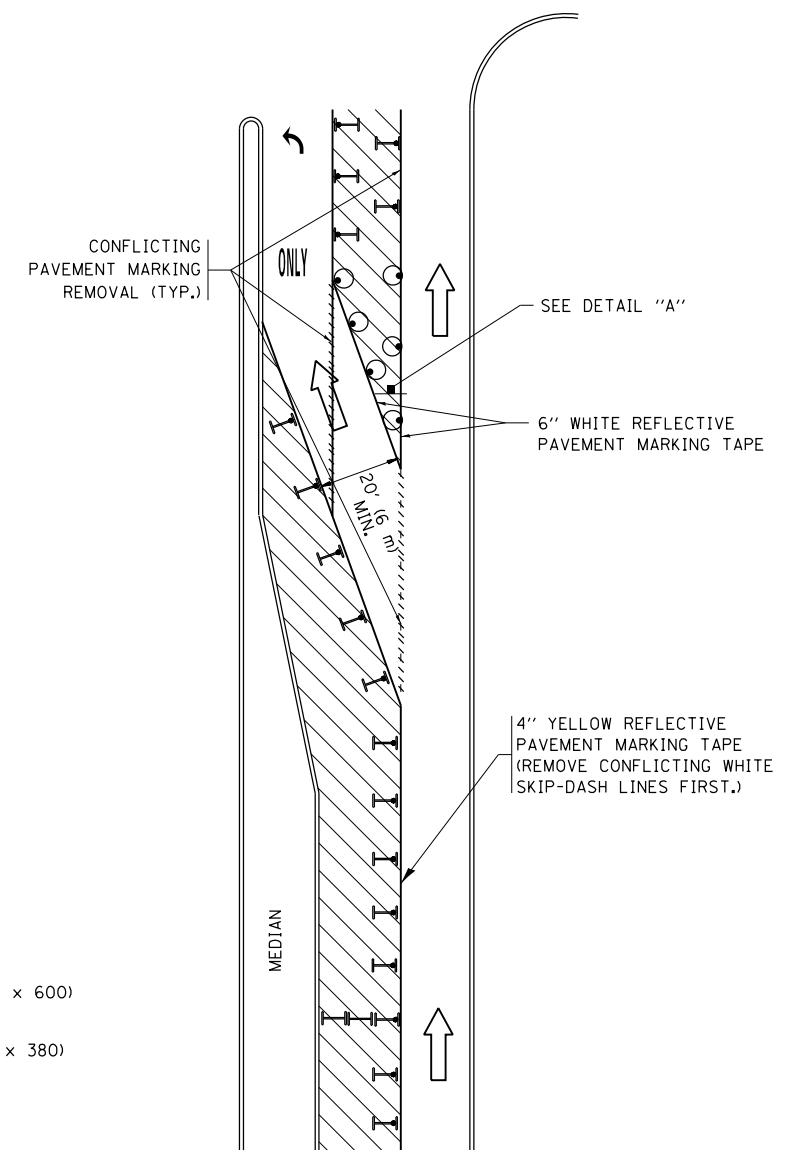
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	2017-048BP	COOK	26	23
	TC-10	CONTRACT NO.	62F80	
ILLINOIS FED. AID PROJECT				

# TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER



**FIGURE 1**

# TURN BAY ENTRANCE WITHIN A LANE CLOSURE



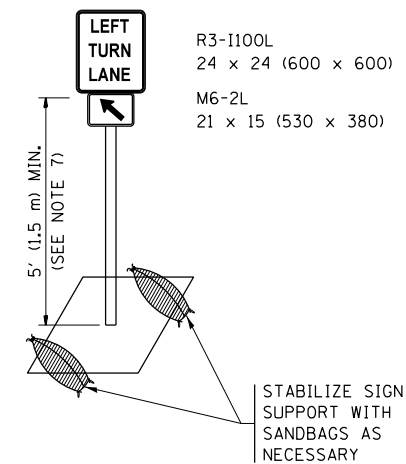
**FIGURE 2**

## LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

## NOTES:

1. A) WHEN "L" IS  $\leq$  THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.  
B) WHEN "L" IS  $>$  THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



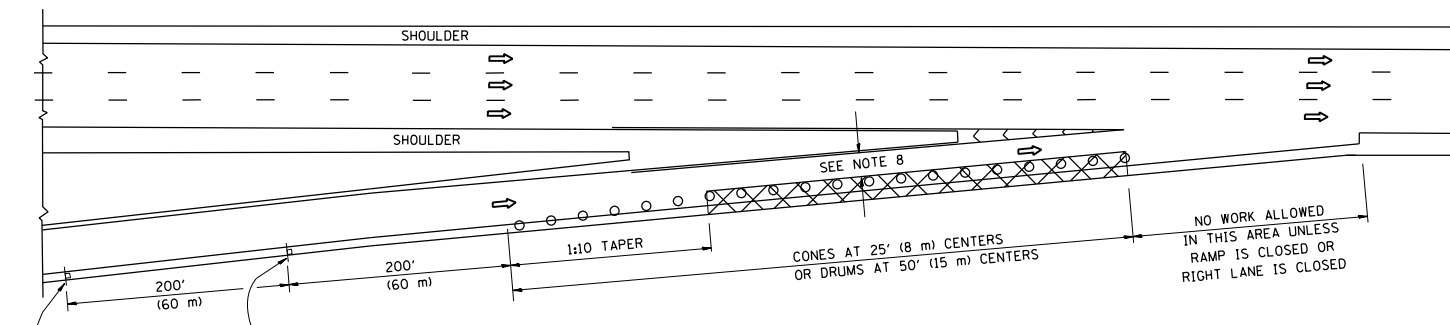
**DETAIL A**

All dimensions are in inches (millimeters) unless otherwise shown.

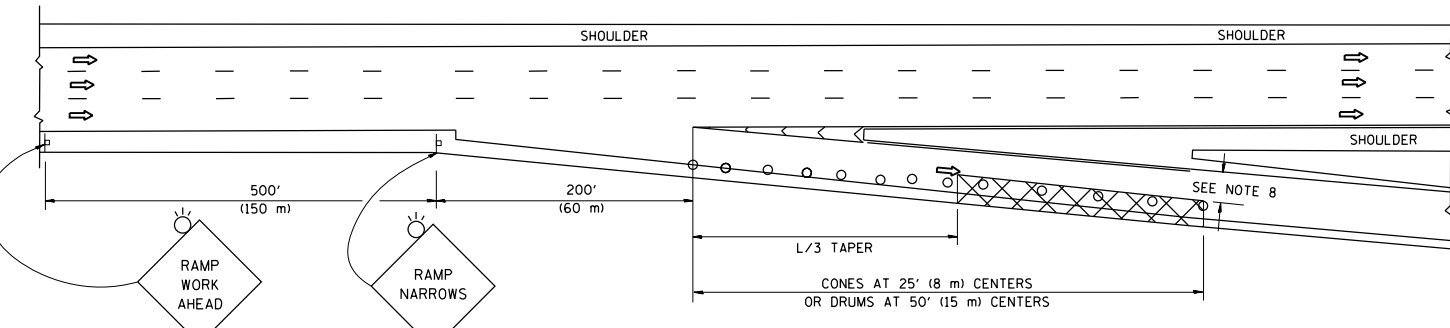
FILE NAME =	USER NAME = sornvilch	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default		REVISED - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13					VAR	2017-048BP	COOK	26	24
	PLOT SCALE = 100.0000' / in.	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16		TC-14			CONTRACT NO.	62F80			
	PLOT DATE = 10/20/2017	REVISED - T. RAMMACHER 01-06-00	REVISED -		SCALE: NONE	SHEET 1	OF 1 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		



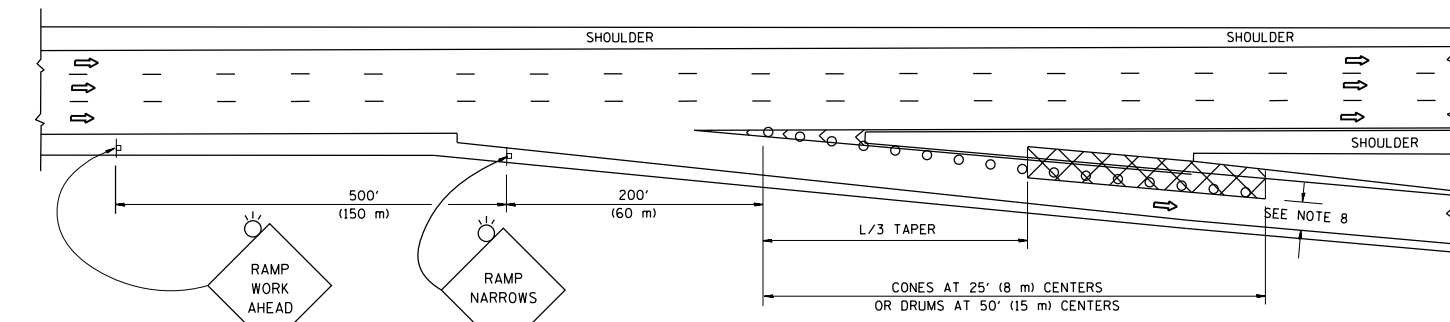
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

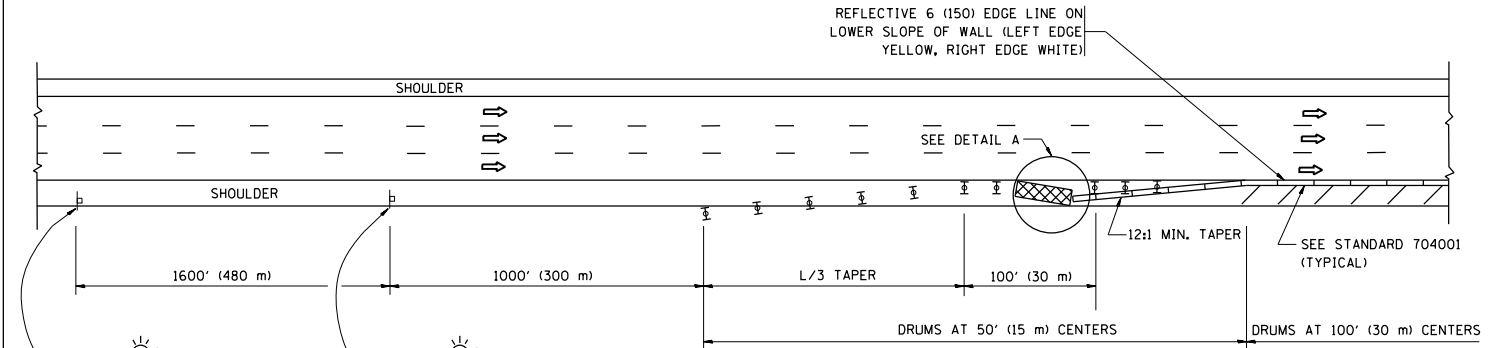
- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

GENERAL NOTES

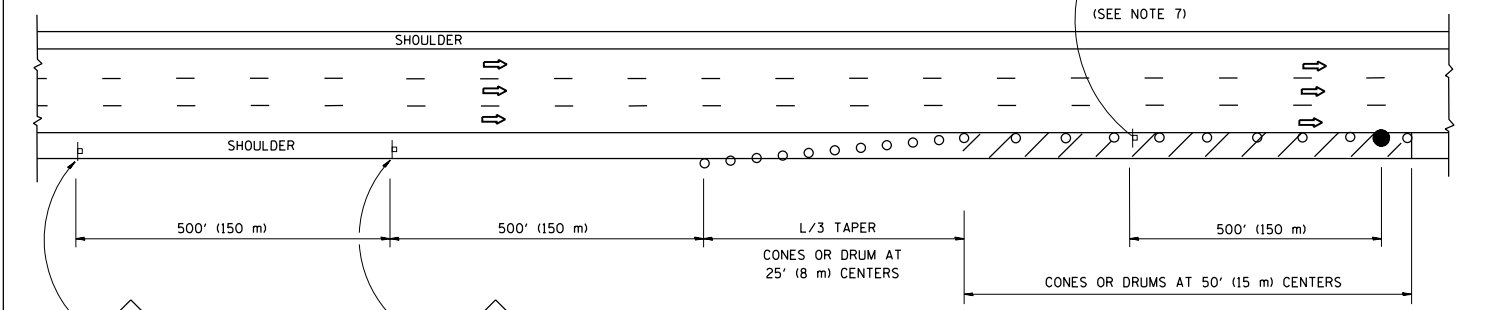
1. THE "L" DISTANCE EQUALS:
 

SPEED LIMIT	FORMULAS
45 mph (80 km/h)	METRIC ENGLISH
OR GREATER:	$L=0.65(W)(S)$ $L=(W)(S)$
W = WIDTH OF OFFSET IN FEET (METERS)	
S = NORMAL POSTED SPEED MPH (KM/H)	
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.
5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
  - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
  - b. THE WORK ACTIVITY REQUIRES FREQUENT ENCROACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
8. 12' MIN. WIDTH TANGENT SECTION  
16' MIN. WIDTH CURVE SECTION.

SHOULDER CLOSURE DETAILS

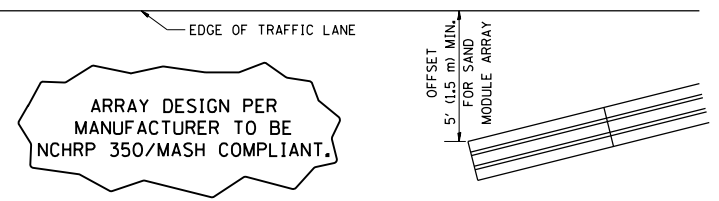


PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

THIS DETAIL IS USED WHERE:  
1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCROACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



DETAIL "A"  
IMPACT ATTENUATOR, TEMPORARY  
(SEE NOTE 5)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

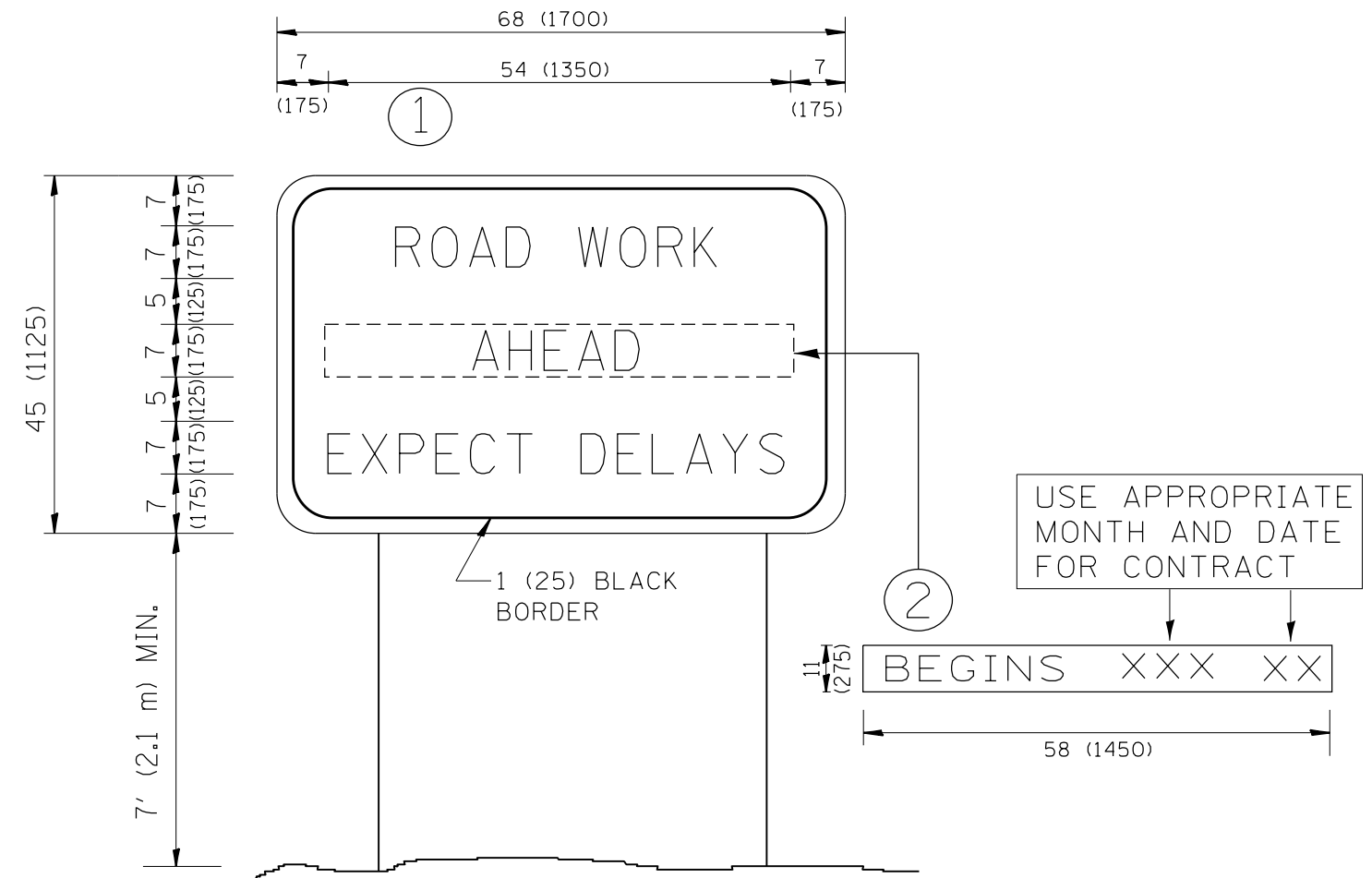
FILE NAME =	USER NAME = sornitwich	DESIGNED -	REVISED - J.A.F. 12-06
pw\1\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI207\Drawings\Design\DWG\Std.dgn		REVISOR -	REVISED - S.P.B. 01-07
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - S.P.B. 12-09
	PLOT DATE = 10/20/2017	DATE -	REVISED - M.D. 06-13

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY  
SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	2017-048BP	COOK	26	25
	TC-17	CONTRACT NO.	62F80	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = saintwlch	DESIGNED -	REVISED - R. MIRS 09-15-97	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ARTERIAL ROAD INFORMATION SIGN</b>	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI207\Drawings\Design\DistStd.dgn			REVISED - R. MIRS 12-11-97			VAR	2017-048BP	COOK	26	26
		CHECKED -	REVISED - T. RAMMACHER 02-02-99			TC-22		CONTRACT NO.		62F80
		DATE -	REVISED - C. JUCIUS 01-31-07			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT