

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		SANGAMON	85	1
		ILLINOIS	CONTRACT NO. 72J83	

*F.A.I. 55, F.A.I. 72, F.A.P. 666
** (84-4, 84-3, 84-9, 110) RS

D-96-047-17



TRAFFIC COUNT

ADT= 45,600 (2015)
PV= 34,930 (76.6%)
SU= 2,964 (6.5%)
MU= 7,706 (16.9%)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED *October 20 2017*
Jeffrey M. South REGIONAL ENGINEER
Nov 30 2017
Marcus M. Addis ENGINEER OF DESIGN AND ENVIRONMENT
Nov 30 2017
Paula DIRECTOR OF PROGRAM DEVELOPMENT

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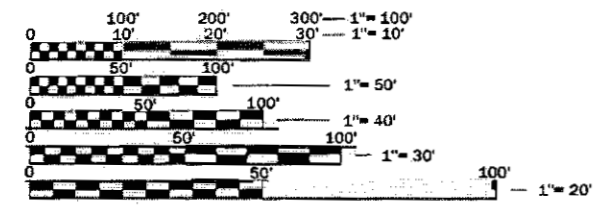
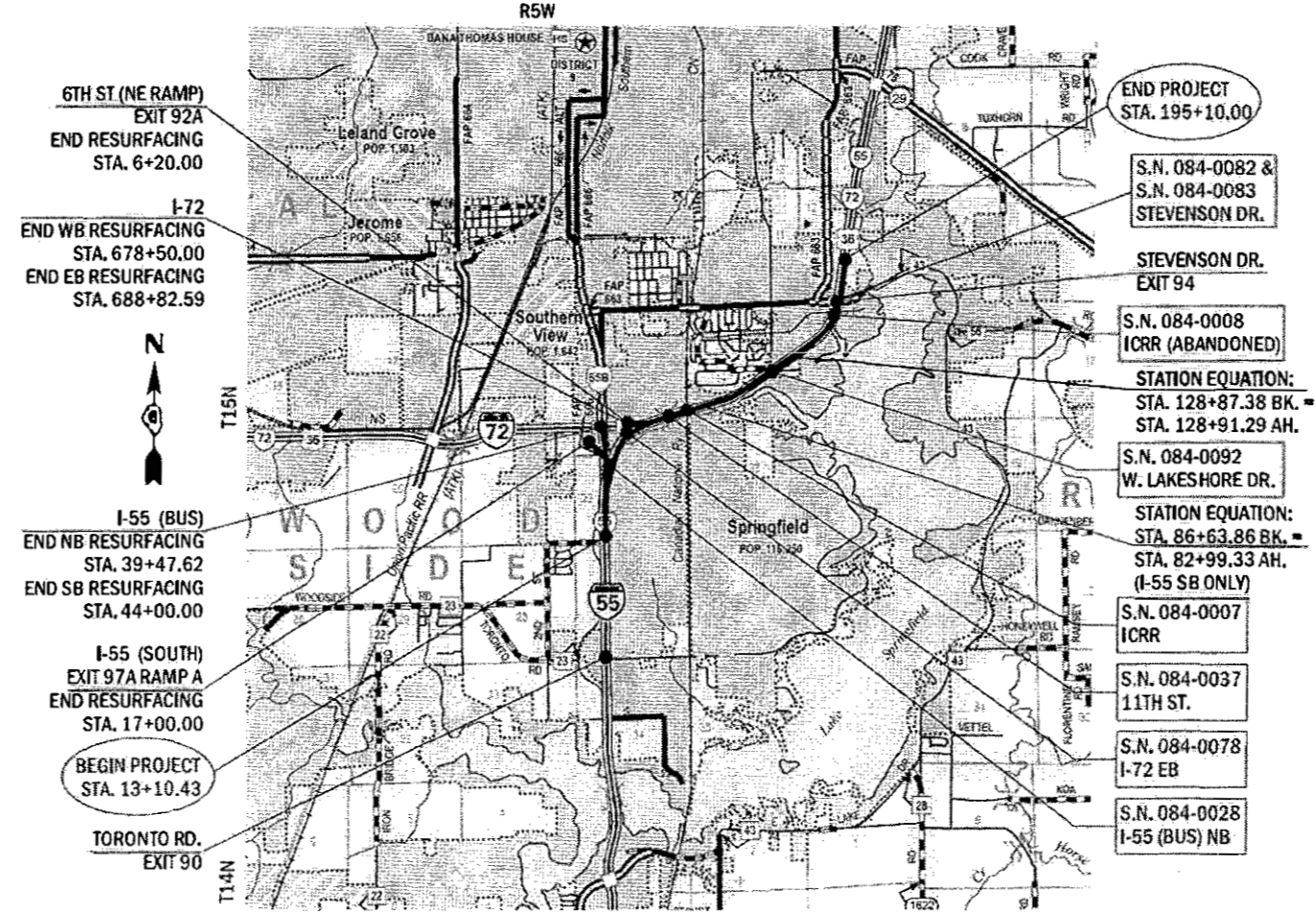
STANDARDS

000001-06	631031-15	701446-09
001001-02	631033-07	701451-05
001006	635001-02	701456-05
280001-07	642001-02	701901-07
442101-08	701400-09	704001-08
482001-02	701401-11	725001-01
542201-02	701406-11	780001-05
630001-12	701411-09	781001-04
631011-10	701426-09	782006
631026-06	701428-01	

**PROPOSED
HIGHWAY PLANS**

VARIOUS ROUTES
SECTION (84-4, 84-3, 84-9, 110) RS
PROJECT: NHPP-R4FF(212)
INTERSTATE RESURFACING
SANGAMON COUNTY

C-96-047-17



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: JAY EDWARDS (217) 785-0597
PROJECT MANAGER: JARED MARTIN (217) 524-0064

CONTRACT NO. 72J83

LOCATION MAP

GROSS LENGTH = 18,195.66 FT. = 3.446 MILES
NET LENGTH = 18,195.66 FT. = 3.446 MILES

GENERAL NOTES

- THE GRADING AND SHAPING OF THE DITCHES SHALL BE DONE IN SUCH A MANNER TO ESTABLISH POSITIVE FLOW BETWEEN DRAINAGE STRUCTURES OR BETWEEN THE EXISTING ELEVATION AT THE UPSTREAM END OF THE GRADING TO THE DOWNSTREAM DRAINAGE STRUCTURE.
- CONTRACTOR SHALL PROVIDE ADEQUATE DRAINAGE AND EROSION PROTECTION FOR THE DURATION OF THIS PROJECT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY FROM CONSTRUCTION OPERATIONS AS OUTLINED IN ARTICLE 107.31 OF THE STANDARD SPECIFICATIONS. THE JULIE NUMBER IS 800-892-0123. A MINIMUM OF FORTY-EIGHT HOURS ADVANCE NOTICE IS REQUIRED. ANY DAMAGE TO THE UNDERGROUND FACILITIES, CAUSED BY THE CONTRACTOR SHALL BE REPAIRED TO THE SATISFACTION OF THE DEPARTMENT AT THE CONTRACTOR'S EXPENSE, INCLUDING TEMPORARY REPAIRS WHICH MAY BE REQUIRED TO KEEP THE FACILITY OPERATIONAL WHILE MATERIAL IS BEING OBTAINED TO MAKE PERMANENT REPAIRS.
- THE LOCATIONS OF THOSE BURIED AND ABOVEGROUND UTILITIES SHOWN ARE APPROXIMATE, ARE SHOWN FOR CONTRACTOR INFORMATIONAL USE ONLY, AND ARE NOT TO BE REFERENCED FOR CONSTRUCTION PURPOSES. THE IMPLIED PRESENCE OR ABSENCE OF UTILITIES IS NOT TO BE CONSTRUED BY THE OWNER, ENGINEER, CONTRACTOR, OR SUBCONTRACTORS TO BE AN ACCURATE AND COMPLETE REPRESENTATION OF UTILITIES THAT MAY OR MAY NOT EXIST ON THE CONSTRUCTION SITE. BURIED AND ABOVEGROUND UTILITY LOCATION, IDENTIFICATION, AND MARKING ARE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. REROUTING, DISCONNECTION, PROTECTION, ETC. OF ANY UTILITIES MUST BE COORDINATED BETWEEN THE CONTRACTOR, UTILITY COMPANY, AND OWNER. SITE SAFETY, INCLUDING THE AVOIDANCE OF HAZARDS ASSOCIATED WITH BURIED AND ABOVEGROUND UTILITIES, REMAINS THE SOLE RESPONSIBILITY OF THE CONTRACTOR.
- THE THICKNESS OF HOT-MIX ASPHALT SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE BITUMINOUS MIXTURE IS PLACED.
- FOR STABILIZATION, ALL TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF FOUR SANDBAGS PER BARRICADE.
- WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL MONUMENTS UNTIL AN AUTHORIZED SURVEYOR OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR HAVING AN AUTHORIZED SURVEYOR RE-ESTABLISH ANY SECTION OR SUBSECTION MONUMENTS DESTROYED BY HIS OPERATIONS.
- ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUBNUMBER LISTED ON THE INDEX OF SHEETS OR THE COPY OF THE STANDARD INCLUDED IN THESE PLANS.
- IN ADDITION TO FIELD SURVEYS AND AERIAL SURVEYS, PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING FACILITIES HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO CONSTRUCTION VARIATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY SUCH DIMENSIONS AND DETAILS IN THE FIELD. SUCH VARIATIONS SHALL NOT BE A CAUSE FOR ADDITIONAL COMPENSATION DUE TO A CHANGE IN THE SCOPE OF THE WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.
- GUARDRAIL REMOVAL WILL INCLUDE THE REMOVAL OF ANY CONCRETE ENCASUREMENTS OF THE POSTS AND ANY ADDITIONAL RAILING LOCATED AT THE BOTTOM OF THE POSTS.
- ANY REFERENCES OR CALL-OUTS IN THESE PLANS TO "BITUMINOUS CONCRETE" SHALL BE INTERPRETED TO MEAN "HOT-MIX ASPHALT".

THE FOLLOWING RATES OF APPLICATION HAVE BEEN USED TO CALCULATE THE PLAN QUANTITIES:

BITUMINOUS MATERIALS (TACK COAT)	0.025 ILB/SQ. FT. (ON PAVEMENT)	NITROGEN FERTILIZER NUTRIENT	90 LBS./ACRE
BITUMINOUS MATERIALS (PRIME COAT)	0.05 ILB/SQ. FT. (ON AGGREGATE)	PHOSPHORUS FERTILIZER NUTRIENT	90 LBS./ACRE
HOT-MIX ASPHALT SURFACE/BINDER/BASE	0.056 TON/SQ. YD./IN	POTASSIUM FERTILIZER NUTRIENT	90 LBS./ACRE
AGGREGATE MATERIAL	2.05 TON/CU. YD.	AGRICULTURAL GROUND LIMESTONE	2.0 TON/ACRE

- UNLESS DIRECTED BY THE ENGINEER, PAVEMENT MARKING LINES SHALL NOT BE LAID DIRECTLY OVER A LONGITUDINAL CRACK OR JOINT NOR OVER A TAR OR ASPHALT PAINTED LINE. THE EDGE OF A CENTERLINE OR LANE LINE SHALL BE OFFSET A MINIMUM DISTANCE OR 2" FROM A LONGITUDINAL CRACK OR JOINT. EDGE LINES SHALL BE APPROXIMATELY 2" FROM THE EDGE LINE OF PAVEMENT. SEE SECTION 780 OF THE STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS.
- THE DISTRICT BUREAU OF OPERATIONS SHALL BE NOTIFIED AT LEAST 14 DAYS PRIOR TO PLACEMENT OF FINAL PAVEMENT MARKINGS (PH 217-785-7314).
- THE ENGINEER SHALL BE THE SOLE JUDGE CONCERNING CURING TIME FOR THE VARIOUS BITUMINOUS LIFTS.
- EVERY TREE SHALL BE SAVED IF POSSIBLE. THE ENGINEER IN THE FIELD WILL VERIFY AND MARK ALL TREES REQUIRED TO BE REMOVED. SHOULD THE ENGINEER'S DECISION INCREASE OR DECREASE THE QUANTITIES OF WORK TO BE PERFORMED FROM THE PLANS, THE CONTRACTOR SHALL ACCEPT PAYMENT AS STATED IN ARTICLE 104.02 OF THE STANDARD SPECIFICATIONS. TREES OUTSIDE THE LIMITS OF CONSTRUCTION SHALL NOT BE DISTURBED UNLESS DESIGNATED BY THE ENGINEER.
- RAISED REFLECTIVE PAVEMENT MARKERS INSTALLED INTO THE ORIGINAL CONCRETE PAVEMENT MAY NOT HAVE BEEN REMOVED WHEN THE PAVEMENT WAS RESURFACED. ANY MARKERS FOUND IN THE ORIGINAL CONCRETE WILL BE REMOVED AND PAID FOR AS RAISED REFLECTIVE PAVEMENT MARKER REMOVAL.

MIXTURE REQUIREMENTS

MIXTURE NUMBER	1	2	3
MIXTURE USE(S)	INTERSTATE AND RAMP POLYMER HMA BINDER	INTERSTATE AND RAMP POLYMER HMA SURFACE	MEDIAN CROSS-OVER, INTERSTATE AND RAMP SHOULDER LIFT
AC/PG:	SBS PG 70-22	SBS PG 76-22	PG 64-22
DESIGN AIR VOIDS	4.0% @ N90	4.0% @ N90	4.0% @ N50
MIXTURE COMPOSITION: (GRADATION MIXTURE)	IL 19.0	IL 9.5	IL 9.5
FRICTION AGGREGATE	N/A	MIX "E"	MIX "C"
QUALITY MANAGEMENT	PFP*	PFP	QC/OA
SUBLOT SIZE	1000 TON	1000 TON	N/A

*WHERE TWO LIFTS OF BINDER ARE REQUIRED TO BE PLACED DURING ONE NIGHT OF PAVING, CORES ARE NOT REQUIRED TO BE COMPLETED ON THE FIRST LIFT PRIOR TO PLACING THE SECOND LIFT. THE DENSITY OF BOTH LIFTS WILL BE OBTAINED FROM CORES EXTENDING THROUGH BOTH LIFTS OBTAINED AFTER COMPLETION OF THE SECOND LIFT. THE CORES WILL BE SEPARATED AT THE LIFT INTERFACE PRIOR TO TESTING.

BINDER LIFTS SHALL BE NO LESS THAN 2.25 INCHES AND SHALL NOT EXCEED 4 INCHES.

COMMITMENTS

THE RESIDENT ENGINEER SHALL CONTACT STUDIES AND PLANS CONCERNING ANY MAJOR PLAN CHANGE, TO MAKE SURE NO PREVIOUS COMMITMENTS (NOT LISTED) WERE MADE AFFECTING THE DESIGN AND TO ALLOW IMPROVED DESIGN FOR FUTURE PROJECTS.

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DISTRICT SIX	
EXAMINED <i>October 4th</i> 20 17	
<i>John C. [Signature]</i>	
OPERATIONS ENGINEER	
EXAMINED <i>Oct 10th</i> 20 17	
<i>Susan [Signature]</i>	
PROJECT IMPLEMENTATION ENGINEER	
EXAMINED <i>October 5</i> 20 17	
<i>Jeffrey P. [Signature]</i>	
PROGRAM DEVELOPMENT ENGINEER	

*F.A.I. 55, F.A.I. 72, F.A.P. 666 **84-1, 84-3, 84-9, 110) RS

SUMMARY OF QUANTITIES				6-60082-0000 & 6-60082-0100						
				90% FEDERAL 10% STATE ROADWAY	90% FEDERAL 10% STATE ROADWAY	90% FEDERAL 10% STATE ROADWAY	90% FEDERAL 10% STATE ROADWAY	90% FEDERAL 10% STATE ROADWAY	90% FEDERAL 10% STATE ROADWAY	90% FEDERAL 10% STATE ROADWAY
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	0004 URBAN	0047 S.N. 084-0008	0047 S.N. 084-0028	0047 S.N. 084-0078	0047 S.N. 084-0082	0047 S.N. 084-0083	0047 S.N. 084-0092
21400100	GRADING AND SHAPING DITCHES	FOOT	3,130	3,130	0	0	0	0	0	0
28000305	TEMPORARY DITCH CHECKS	FOOT	156.5	156.5	0	0	0	0	0	0
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	165,172	165,172	0	0	0	0	0	0
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	826	826	0	0	0	0	0	0
40600990	TEMPORARY RAMP	SQ YD	780	780	0	0	0	0	0	0
40603240	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90	TON	33,915	33,915	0	0	0	0	0	0
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	37	37	0	0	0	0	0	0
40603570	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N90	TON	12,474	12,474	0	0	0	0	0	0
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	6,488	6,488	0	0	0	0	0	0
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	68,045	68,045	0	0	0	0	0	0
44001980	CONCRETE BARRIER REMOVAL	FOOT	25	25	0	0	0	0	0	0
44201043	CLASS B PATCHES, TYPE II, 16 INCH	SQ YD	1,447	1,447	0	0	0	0	0	0
44201047	CLASS B PATCHES, TYPE III, 16 INCH	SQ YD	380	380	0	0	0	0	0	0
44201048	CLASS B PATCHES, TYPE IV, 16 INCH	SQ YD	173	173	0	0	0	0	0	0

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	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES			
SCALE:	SHEET	OF	SHEETS
	STA.	TO	STA.

F.A.* RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**	SANGAMON	85	3
			CONTRACT NO. 72183	
ILLINOIS FED. AID PROJECT				

SUMMARY OF QUANTITIES				6-60082-0000 & 6-60082-0100						
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	90% FEDERAL 10% STATE ROADWAY	90% FEDERAL 10% STATE ROADWAY	90% FEDERAL 10% STATE ROADWAY	90% FEDERAL 10% STATE ROADWAY	90% FEDERAL 10% STATE ROADWAY	90% FEDERAL 10% STATE ROADWAY	90% FEDERAL 10% STATE ROADWAY
				0004	0047	0047	0047	0047	0047	0047
				URBAN	S.N. 084-0008	S.N. 084-0028	S.N. 084-0078	S.N. 084-0082	S.N. 084-0083	S.N. 084-0092
44201299	DOWEL BARS 1 1/2"	EACH	3,448	3,448	0	0	0	0	0	0
44213200	SAW CUTS	FOOT	11,388	11,388	0	0	0	0	0	0
44213204	TIE BARS 3/4"	EACH	120	120	0	0	0	0	0	0
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	3,372	3,372	0	0	0	0	0	0
48203100	HOT-MIX ASPHALT SHOULDERS	TON	7,848	7,848	0	0	0	0	0	0
50100100	REMOVAL OF EXISTING STRUCTURES	EACH	1	1	0	0	0	0	0	0
50300225	CONCRETE STRUCTURES	CU YD	79.3	0	31.3	9.9	14.4	7.2	7.2	9.3
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	5,692	0	1,530	890	1,100	760	760	652
52000030	PREFORMED JOINT SEAL 2 1/2"	FOOT	36	0	0	0	0	36	0	0
54215424	CAST-IN-PLACE REINFORCED CONCRETE END SECTIONS 24"	EACH	1	1	0	0	0	0	0	0
61100605	MISCELLANEOUS CONCRETE	CU YD	17	17	0	0	0	0	0	0
* 63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	13,756	13,756	0	0	0	0	0	0
* 63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	14	14	0	0	0	0	0	0
* 63100070	TRAFFIC BARRIER TERMINAL, TYPE 5	EACH	5	5	0	0	0	0	0	0

* SPECIALTY ITEM

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PLOT DATE = 10/20/2017	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: SHEET OF SHEETS STA. TO STA. F.A. * RTE. SECTION COUNTY TOTAL SHEETS SHEET NO. SANGAMON 85 4 ILLINOIS FED. AID PROJECT

TEMPORARY PAVEMENT MARKING - LINE 5''						
LOCATION	INSIDE EDGE LINE YELLOW (FOOT)	OUTSIDE EDGE LINE WHITE (FOOT)	CENTERLINE SKIP DASH WHITE (FOOT)	TOTAL LENGTH (FOOT)		
STATION TO STATION						
I-55 MAINLINE NORTHBOUND						
STA. 13+10.43 TO STA. 24+04.00	1094	1094	547	2734		
STA. 24+04.00 TO STA. 29+78.00	574	574	144	1292		
STA. 29+78.00 TO STA. 63+73.00	3395	3395	849	7639		
STA. 63+73.00 TO STA. 69+20.00	547	547	137	1231		
STA. 69+20.00 TO STA. 91+07.00	2187	2187	1094	5468		
STA. 91+07.00 TO STA. 128+87.38	3780	3780	945	8506		
STATION EQUATION STA. 128+87.38 BK = STA. 128+91.29 AH						
STA. 128+91.29 TO STA. 171+00.00	4209	4209	1052	9470		
STA. 171+00.00 TO STA. 175+65.22	465	465	116	1047		
STA. 175+65.22 TO STA. 182+68.00	703	703	176	1581		
STA. 182+68.00 TO STA. 187+55.51	488	488	122	1097		
STA. 187+55.51 TO STA. 195+10.00	754	754	189	1698		
SOUTHBOUND						
STA. 13+10.43 TO STA. 31+22.00	1812	1812	906	4529		
STA. 31+22.00 TO STA. 34+94.00	372	372	93	837		
STA. 34+94.00 TO STA. 62+08.00	2714	2714	679	6107		
STA. 62+08.00 TO STA. 71+82.00	974	974	244	2192		
STA. 71+82.00 TO STA. 86+63.86	1482	1482	741	3705		
STATION EQUATION STA. 86+63.86 BK = STA. 82+99.33 AH						
STA. 82+99.33 TO STA. 128+87.38	4588	4588	1147	10323		
STATION EQUATION STA. 128+87.38 BK = STA. 128+91.29 AH						
STA. 128+91.29 TO STA. 153+27.68	2436	2436	609	5482		
STA. 153+27.68 TO STA. 169+12.19	1585	1585	396	3565		
STA. 169+12.19 TO STA. 195+10.00	2598	2598	649	5845		
I-72 MAINLINE EASTBOUND						
STA. 688+82.59 TO STA. 694+78.17	596	596	149	1340		
STA. 694+78.17 TO STA. 699+21.04	443	443	111	996		
WESTBOUND						
STA. 678+50.00 TO STA. 684+15.00	565	565	141	1271		
STA. 684+15.00 TO STA. 688+97.00	482	482	121	1085		
STA. 688+97.00 TO STA. 698+72.69	976	976	244	2195		
I-55 BUSINESS LOOP NORTHBOUND						
STA. 24+04.00 TO STA. 29+78.00	574	574	144	1292		
STA. 29+78.00 TO STA. 39+47.62	970	970	242	2182		
SOUTHBOUND						
STA. 31+22.00 TO STA. 34+94.00	372	372	93	837		
STA. 34+94.00 TO STA. 44+00.00	906	906	227	2039		
STA. 34+94.00 TO STA. 37+45.43	251	251	63	566		
STA. 41+30.80 TO STA. 44+00.00	269	269	67	606		
SIXTH STREET INTERCHANGE RAMP A						
STA. 17+00.00 TO STA. 29+93.00	1293	1293	0	2586		
NE RAMP						
STA. 3+14.60 TO STA. 6+20.00	305	305	0	611		
STEVENSON DRIVE RAMP A						
STA. 0+00 TO STA. 3+02.00	302	302	0	604		
STA. 3+02.00 TO STA. 6+69.00	367	367	0	734		
STA. 6+69.00 TO STA. 16+82.00	1013	1013	0	2026		
RAMP B						
STA. 1+16.00 TO STA. 7+50.00	634	634	0	1268		
STA. 7+50.00 TO STA. 12+19.00	469	469	0	938		
RAMP C						
STA. 4+76.00 TO STA. 7+21.00	245	245	0	490		
STA. 7+21.00 TO STA. 9+44.00	223	223	0	446		
STA. 9+44.00 TO STA. 18+27.00	883	883	0	1766		
RAMP D						
STA. 0+98.00 TO STA. 11+21.00	1023	1023	0	2046		
STA. 11+21.00 TO STA. 16+08.00	487	487	0	974		
TOTAL	50,404	50,404	12,433	113,241		

DELINEATOR SYSTEM								
LOCATION	STATION TO STATION	SPACING (FOOT)	IN CURVE (EACH)	ADVANCE (EACH)	BEYOND (EACH)	TOTAL (EACH)		
I-55 MAINLINE NORTHBOUND								
RT	STA 15+55	MEDIAN CROSSOVER	-	-	3	-	3	
RT	STA 99+02 TO STA 109+05		150	7	3	3	13	
LT	STA 117+40	MEDIAN CROSSOVER	-	-	3	-	3	
SOUTHBOUND								
RT	STA 117+40	MEDIAN CROSSOVER	-	-	3	-	3	
LT	STA 34+95 TO STA 41+39		175	4	-	-	4	
LT	STA 45+24 TO STA 51+90		175	4	-	-	4	
I-72 MAINLINE EASTBOUND								
RT	STA 688+91 TO STA 694+78		150	4	-	-	4	
SIXTH STREET INTERCHANGE RAMP A								
LT	STA 22+28 TO STA 26+12		70	6	3	-	9	
RT	STA 28+28 TO STA 29+93		100	2	-	-	2	
NE RAMP								
RT	STA 0+00 TO STA 6+63		100	7	3	3	13	
STEVENSON DRIVE RAMP A								
RT	STA 0+00 TO STA 6+53		100	7	3	3	13	
LT	STA 6+68 TO STA 11+14		75	6	-	3	9	
RT	STA 13+50 TO STA 16+82		40	9	3	3	15	
RAMP B								
LT	STA 2+39 TO STA 7+56		30	18	3	-	21	
RAMP C								
LT	STA 13+00 TO STA 17+03		35	12	-	3	15	
RAMP D								
RT	STA 2+49 TO STA 5+38		50	6	3	3	12	
LT	STA 7+60 TO STA 11+24		55	7	3	-	10	
RT	STA 11+92 TO STA 16+08		100	5	3	3	11	
							TOTAL	164

TEMPORARY RAMPS			
LOCATION	LENGTH	WIDTH	AREA (SQ YD)
I-55 MAINLINE SOUTHBOUND			
STA 13+10.43	15.00	50.0	83.33
STA 195+10.00	15.00	38.0	63.33
NORTHBOUND			
STA 13+10.43	15.00	50.0	83.33
STA 195+10.00	15.00	38.0	63.33
I-72 MAINLINE EASTBOUND			
STA 688+82.59	15.00	38.0	63.33
WESTBOUND			
STA 678+50.00	15.00	38.0	63.33
I-55 BUSINESS LOOP NORTHBOUND			
STA 39+47.62	15.00	38.0	63.33
SOUTHBOUND			
STA 44+00.00	15.00	38.0	63.33
SIXTH STREET INTERCHANGE RAMP A			
STA 17+00.00	12.50	28.0	38.89
NE RAMP			
STA 6+20.00	12.50	28.0	38.89
STEVENSON DRIVE RAMP A			
STA 16+82.00	12.50	28.0	38.89
RAMP B			
STA 1+16.00	12.50	28.0	38.89
RAMP C			
STA 18+27.00	12.50	28.0	38.89
RAMP D			
STA 0+98.00	12.50	28.0	38.89
TOTAL			780

PREFORMED PLASTIC PAVEMENT MARKING, TYPE B -INLAID- LETTERS AND SYMBOLS		
LOCATION	DESCRIPTION	QUANTITY (SQ. FT)
I-55 MAINLINE NORTHBOUND		
STA. 82+98.00	LANE REDUCTION ARROW	41.5
STA. 88+01.00	LANE REDUCTION ARROW	41.5
STA. 91+25.00	LANE REDUCTION ARROW	41.5
STEVENSON DRIVE RAMP A		
STA. 13+48.00	LEFT ARROW	15.6
STA. 13+48.00	RIGHT ARROW	15.6
STA. 15+00.00	LEFT ARROW	15.6
STA. 15+00.00	RIGHT ARROW	15.6
STA. 15+84.00	LEFT ARROW	15.6
STA. 15+84.00	RIGHT ARROW	15.6
STA. 16+68.00	LEFT ARROW	15.6
STA. 16+68.00	RIGHT ARROW	15.6
RAMP B		
STA. 14+80.00	LANE REDUCTION ARROW	15.6
STA. 17+25.00	LANE REDUCTION ARROW	15.6
STA. 19+69.00	LANE REDUCTION ARROW	15.6
STA. 22+13.00	LANE REDUCTION ARROW	15.6
RAMP C		
STA. 15+93.00	COMB LEFT/THROUGH ARROW	26.00
STA. 15+93.00	RIGHT ARROW	15.60
STA. 17+28.00	COMB LEFT/THROUGH ARROW	26.00
STA. 17+28.00	RIGHT ARROW	15.60
RAMP D		
STA. 1+00.00	THROUGH ARROW	11.50
TOTAL		406.4

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
**	**	SANGAMON	85	11
			CONTRACT NO. 72J83	

*F.A.I. 55, F.A.I. 72, F.A.P. 666 ** (84-4, 84-3, 84-9, 110) RS

URETHANE PAVEMENT MARKING - LINE 5''						
LOCATION	INSIDE EDGE LINE YELLOW (FOOT)	OUTSIDE EDGE LINE WHITE (FOOT)	DOTTED EXTENSION WHITE (FOOT)	TOTAL LENGTH (FOOT)		
I-55 MAINLINE NORTHBOUND						
STA. 13+10.43 TO STA. 24+04.00	1094	1094		2187		
STA. 24+04.00 TO STA. 29+78.00	0	574		574		
STA. 29+78.00 TO STA. 63+73.00	3395	3395		6790		
STA. 63+73.00 TO STA. 69+20.00	0	547		547		
STA. 69+20.00 TO STA. 91+07.00	2187	2187		4374		
STA. 91+07.00 TO STA. 128+87.38	3780	3780		7561		
STATION EQUATION STA. 128+87.38 BK = STA. 128+91.29 AH						
STA. 128+91.29 TO STA. 171+00.00	4209	4209		8417		
STA. 171+00.00 TO STA. 175+65.22	465	0		465		
STA. 175+65.22 TO STA. 182+68.00	703	703		1406		
STA. 182+68.00 TO STA. 187+55.51	488	0		488		
STA. 187+55.51 TO STA. 195+10.00	754	754		1509		
SOUTHBOUND						
STA. 13+10.43 TO STA. 31+22.00	1812	1812		3623		
STA. 31+22.00 TO STA. 34+94.00	372	0		372		
STA. 34+94.00 TO STA. 62+08.00	2714	2714		5428		
STA. 62+08.00 TO STA. 71+82.00	974	0		974		
STA. 71+82.00 TO STA. 83+68.36	1186	1186		2373		
STATION EQUATION STA. 86+63.86 BK = STA. 82+99.33 AH						
STA. 82+99.33 TO STA. 128+87.38	4588	4588		9176		
STATION EQUATION STA. 128+87.38 BK = STA. 128+91.29 AH						
STA. 128+91.29 TO STA. 153+27.68	2436	2436		4873		
STA. 153+27.68 TO STA. 169+12.19	1585	1585		3169		
STA. 169+12.19 TO STA. 195+10.00	2598	2598		5196		
I-72 MAINLINE EASTBOUND						
STA. 688+82.59 TO STA. 694+78.17	596	596		1191		
STA. 694+78.17 TO STA. 699+21.04	443	0		443		
WESTBOUND						
STA. 678+50.00 TO STA. 684+15.00	565	565		1130		
STA. 684+15.00 TO STA. 688+97.00	482	0		482		
STA. 688+97.00 TO STA. 698+72.69	0	976		976		
I-55 BUSINESS LOOP NORTHBOUND						
STA. 24+04.00 TO STA. 29+78.00	574	0		574		
STA. 29+78.00 TO STA. 39+47.62	970	970		1939		
SOUTHBOUND						
STA. 31+22.00 TO STA. 34+94.00	0	372		372		
STA. 34+94.00 TO STA. 44+00.00	906	0		906		
STA. 34+94.00 TO STA. 37+45.43	0	251		251		
STA. 41+30.80 TO STA. 44+00.00	0	269		269		
SIXTH STREET INTERCHANGE RAMP A						
STA. 29+93.00 TO STA. 17+00.00	1293	1293		2586		
NE RAMP						
STA. 3+14.60 TO STA. 6+20.00	0	305		305		
STEVENSON DRIVE RAMP A						
STA. 0+00 TO STA. 3+02.00	0	302	302	604		
STA. 3+02.00 TO STA. 6+69.00	0	367		367		
STA. 6+69.00 TO STA. 16+82.00	1013	1013		2026		
RAMP B						
STA. 1+16.00 TO STA. 7+50.00	634	634		1268		
STA. 7+50.00 TO STA. 12+19.00	0	469		469		
STA. 12+19.00 TO STA. 27+69.00			1550	1550		
RAMP C						
STA. 4+76.00 TO STA. 7+21.00	0	245		245		
STA. 7+21.00 TO STA. 9+44.00	0	223		223		
STA. 9+44.00 TO STA. 18+27.00	883	883		1766		
RAMP D						
STA. 0+98.00 TO STA. 11+21.00	1023	1023		2046		
STA. 11+21.00 TO STA. 16+08.00	0	487		487		
TOTAL	44,720.4	45,405	1,852	91,977		

SHOULDER RUMBLE STRIP, 16''		
LOCATION	STATION TO STATION	LENGTH (FOOT)
I-55 MAINLINE NORTHBOUND OUTSIDE SHOULDER		
STA. 13+10.43 TO STA. 128+87.38		11576.95
STATION EQUATION STA. 128+87.38 BK = STA. 128+91.29 AH		
STA. 128+91.29 TO STA. 171+00.60		4209.31
STA. 175+82.00 TO STA. 182+68.00		686.00
STA. 194+76.00 TO STA. 195+10.00		34.00
NORTHBOUND INSIDE SHOULDER		
STA. 13+10.43 TO STA. 27+77.96		1467.53
STA. 27+77.96 TO STA. 63+75.00		3597.04
STA. 63+75.00 TO STA. 128+87.38		6512.38
STATION EQUATION STA. 128+87.38 BK = STA. 128+91.29 AH		
STA. 128+91.29 TO STA. 195+10.00		6618.71
SOUTHBOUND OUTSIDE SHOULDER		
STA. 13+10.43 TO STA. 31+22.04		1811.61
STA. 34+95.41 TO STA. 62+07.00		2711.59
STA. 64+19.00 TO STA. 86+63.86		2244.86
STATION EQUATION STA. 86+63.86 BK = STA. 82+99.33 AH		
STA. 82+99.33 TO STA. 128+87.38		4588.05
STATION EQUATION STA. 128+87.38 BK = STA. 128+91.29 AH		
STA. 128+91.29 TO STA. 163+18.00		3426.71
STA. 174+02.00 TO STA. 181+27.00		725.00
STA. 187+95.91 TO STA. 195+10.00		714.09
SOUTHBOUND INSIDE SHOULDER		
STA. 13+10.43 TO STA. 86+63.86		7353.43
STATION EQUATION STA. 86+63.86 BK = STA. 82+99.33 AH		
STA. 82+99.33 TO STA. 128+87.38		4588.05
STATION EQUATION STA. 128+87.38 BK = STA. 128+91.29 AH		
STA. 128+91.29 TO STA. 195+10.00		6618.71
I-72 MAINLINE EASTBOUND OUTSIDE SHOULDER		
STA. 688+82.59 TO STA. 694+78.17		595.58
EASTBOUND INSIDE SHOULDER		
STA. 689+73.00 TO STA. 694+78.17		505.17
WESTBOUND OUTSIDE SHOULDER		
STA. 678+50.00 TO STA. 688+95.90		1045.90
WESTBOUND INSIDE SHOULDER		
STA. 678+50.00 TO STA. 688+95.90		1045.90
TOTAL		72,676.57

GROOVING FOR RECESSED PAVEMENT MARKING - 6''				
LOCATION	INSIDE EDGE LINE YELLOW (FOOT)	OUTSIDE EDGE LINE WHITE (FOOT)	TOTAL LENGTH (FOOT)	
I-55 MAINLINE NORTHBOUND				
STA. 13+10.43 TO STA. 24+04.00	1094	1094	2187	
STA. 24+04.00 TO STA. 29+78.00	0	574	574	
STA. 29+78.00 TO STA. 63+73.00	3395	3395	6790	
STA. 63+73.00 TO STA. 69+20.00	0	547	547	
STA. 69+20.00 TO STA. 91+07.00	2187	2187	4374	
STA. 91+07.00 TO STA. 128+87.38	3780	3780	7561	
STATION EQUATION STA. 128+87.38 BK = STA. 128+91.29 AH				
STA. 128+91.29 TO STA. 171+00.00	4209	4209	8417	
STA. 171+00.00 TO STA. 175+65.22	465	0	465	
STA. 175+65.22 TO STA. 182+68.00	703	703	1406	
STA. 182+68.00 TO STA. 187+55.51	488	0	488	
STA. 187+55.51 TO STA. 195+00.00	744	744	1489	
SOUTHBOUND				
STA. 13+10.43 TO STA. 31+22.00	1812	1812	3623	
STA. 31+22.00 TO STA. 34+94.00	372	0	372	
STA. 34+94.00 TO STA. 62+08.00	2714	2714	5428	
STA. 62+08.00 TO STA. 71+82.00	974	0	974	
STA. 71+82.00 TO STA. 86+63.86	1482	1482	2964	
STATION EQUATION STA. 86+63.86 BK = STA. 82+99.33 AH				
STA. 82+99.33 TO STA. 128+87.38	4588	4588	9176	
STATION EQUATION STA. 128+87.38 BK = STA. 128+91.29 AH				
STA. 128+91.29 TO STA. 153+27.68	2436	2436	4873	
STA. 153+27.68 TO STA. 169+12.19	1585	1585	3169	
STA. 169+12.19 TO STA. 195+00.00	2588	2588	5176	
I-72 MAINLINE EASTBOUND				
STA. 688+82.59 TO STA. 694+78.17	596	596	1191	
STA. 694+78.17 TO STA. 699+21.04	443	0	443	
WESTBOUND				
STA. 678+50.00 TO STA. 684+15.00	115	115	230	
STA. 684+15.00 TO STA. 688+97.00	482	0	482	
STA. 688+97.00 TO STA. 698+72.69	0	976	976	
I-55 BUSINESS LOOP NORTHBOUND				
STA. 24+04.00 TO STA. 29+78.00	574	0	574	
STA. 29+78.00 TO STA. 39+47.62	970	970	1939	
SOUTHBOUND				
STA. 31+22.00 TO STA. 34+94.00	0	372	372	
STA. 34+94.00 TO STA. 44+00.00	906	0	906	
STA. 34+94.00 TO STA. 37+45.43	0	251	251	
STA. 41+30.80 TO STA. 44+00.00	0	269	269	
SIXTH STREET INTERCHANGE RAMP A				
STA. 29+93.00 TO STA. 17+00.00	1293	1293	2586	
NE RAMP				
STA. 3+14.60 TO STA. 6+20.00	0	305	305	
STEVENSON DRIVE RAMP A				
STA. 0+00 TO STA. 3+02.00	0	302	302	
STA. 3+02.00 TO STA. 6+69.00	0	367	367	
STA. 6+69.00 TO STA. 16+82.00	1013	1013	2026	
RAMP B				
STA. 1+16.00 TO STA. 7+50.00	634	634	1268	
STA. 7+50.00 TO STA. 12+19.00	0	469	469	
STA. 12+19.00 TO STA. 27+69.00			1550	
RAMP C				
STA. 4+76.00 TO STA. 7+21.00	0	245	245	
STA. 7+21.00 TO STA. 9+44.00	0	223	223	
STA. 9+44.00 TO STA. 18+27.00	883	883	1766	
RAMP D				
STA. 0+98.00 TO STA. 11+21.00	1023	1023	2046	
STA. 11+21.00 TO STA. 16+08.00	0	487	487	
TOTAL	44,546	45,230	89,776	

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USER NAME = coxte	DESIGNED -	REVISED -
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PLOT DATE = 10/20/2017	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	ILLINOIS	FED. AID PROJECT
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	**	SANGAMON	85	12
CONTRACT NO. 72J83				

*F.A.I. 55, F.A.I. 72, F.A.P. 666 ** (84-4, 84-3, 84-9, 110) RS

SHORT TERM PAVEMENT MARKING, 5"						
LOCATION STATION TO STATION	INSIDE EDGE LINE YELLOW (FOOT)	OUTSIDE EDGE LINE WHITE (FOOT)	CENTERLINE SKIP DASH WHITE (FOOT)	TOTAL LENGTH (FOOT)		
I-55 MAINLINE NORTHBOUND						
STA. 13+10.43 TO STA. 24+04.00	87.49	87.49	109.36	284		
STA. 24+04.00 TO STA. 29+77.00	45.84	45.84	57.30	149		
STA. 29+77.00 TO STA. 63+75.00	271.84	271.84	339.80	883		
STA. 63+75.00 TO STA. 69+20.00	43.60	43.60	54.50	142		
STA. 69+20.00 TO STA. 128+87.38	477.39	477.39	596.74	1552		
STATION EQUATION STA. 128+87.38 BK = STA. 128+91.29 AH						
STA. 128+91.29 TO STA. 171+00.60	336.74	336.74	420.93	1094		
STA. 171+00.60 TO STA. 175+81.00	38.43	38.43	48.04	125		
STA. 175+81.00 TO STA. 182+68.00	54.96	54.96	68.70	179		
STA. 182+68.00 TO STA. 194+76.00	96.64	96.64	120.80	314		
STA. 194+76.00 TO STA. 195+10.00	2.72	2.72	3.40	9		
SOUTHBOUND						
STA. 13+10.43 TO STA. 31+22.04	144.93	144.93	181.16	471		
STA. 31+22.04 TO STA. 34+95.41	29.87	29.87	37.34	97		
STA. 34+95.41 TO STA. 62+07.00	216.93	216.93	271.16	705		
STA. 62+07.00 TO STA. 71+82.00	78.00	78.00	97.50	254		
STA. 71+82.00 TO STA. 86+63.86	118.55	118.55	148.19	385		
STATION EQUATION STA. 86+63.86 BK = STA. 82+99.33 AH						
STA. 82+99.33 TO STA. 128+87.38	367.04	367.04	458.81	1193		
STATION EQUATION STA. 128+87.38 BK = STA. 128+91.29 AH						
STA. 128+91.29 TO STA. 163+18.00	274.14	274.14	342.67	891		
STA. 163+18.00 TO STA. 174+02.00	86.72	86.72	108.40	282		
STA. 174+02.00 TO STA. 181+27.00	58.00	58.00	72.50	189		
STA. 181+27.00 TO STA. 187+95.91	53.51	53.51	66.89	174		
STA. 187+95.91 TO STA. 195+10.00	57.13	57.13	71.41	186		
I-72 MAINLINE EASTBOUND						
STA. 688+82.59 TO STA. 694+78.00	47.63	47.63	59.54	155		
STA. 694+78.00 TO STA. 698+72.70	31.58	31.58	39.47	103		
WESTBOUND						
STA. 678+50.00 TO STA. 684+19.00	45.52	45.52	56.90	148		
STA. 684+19.00 TO STA. 688+95.00	38.08	38.08	47.60	124		
I-55 BUSINESS LOOP NORTHBOUND						
STA. 29+73.82 TO STA. 39+47.62	77.90	77.90	97.38	253		
SOUTHBOUND						
STA. 31+22.00 TO STA. 44+00.00	102.24	102.24	127.80	332		
SIXTH STREET INTERCHANGE RAMP A						
STA. 17+00.00 TO STA. 29+93.00	103.44	103.44	129.30	336		
NE RAMP						
STA. 0+00 TO STA. 6+20.00	49.60	49.60	62.00	161		
STEVENS ON DRIVE RAMP A						
STA. 0+00 TO STA. 16+82.00	134.56	134.56	168.20	437		
RAMP B						
STA. 1+16.00 TO STA. 27+69.00	212.24	212.24	265.30	690		
RAMP C						
STA. 4+76.00 TO STA. 18+27.00	108.08	108.08	135.10	351		
RAMP D						
STA. 0+98.00 TO STA. 16+08.00	120.80	120.80	151.00	393		
TOTAL		4,012	4,012	5,015	13,039	

NOTE : 2-APPLICATIONS FOR EDGE LINE
1-APPLICATION FOR CENTERLINE
HAVE BEEN CALCULATED

SHORT TERM PAVEMENT MARKING REMOVAL					
LOCATION STATION TO STATION	INSIDE EDGE LINE YELLOW (FOOT)	OUTSIDE EDGE LINE WHITE (FOOT)	TOTAL AREA (SQ. FT)		
I-55 MAINLINE NORTHBOUND					
STA. 13+10.43 TO STA. 24+04.00	43.74	43.74	36		
STA. 24+04.00 TO STA. 29+77.00	22.92	22.92	19		
STA. 29+77.00 TO STA. 63+75.00	135.92	135.92	113		
STA. 63+75.00 TO STA. 69+20.00	21.80	21.80	18		
STA. 69+20.00 TO STA. 128+87.38	238.70	238.70	199		
STATION EQUATION STA. 128+87.38 BK = STA. 128+91.29 AH					
STA. 128+91.29 TO STA. 171+00.60	168.37	168.37	140		
STA. 171+00.60 TO STA. 175+81.00	19.22	19.22	16		
STA. 175+81.00 TO STA. 182+68.00	27.48	27.48	23		
STA. 182+68.00 TO STA. 194+76.00	48.32	48.32	40		
STA. 194+76.00 TO STA. 195+10.00	1.36	1.36	1		
SOUTHBOUND					
STA. 13+10.43 TO STA. 31+22.04	72.46	72.46	60		
STA. 31+22.04 TO STA. 34+95.41	14.93	14.93	12		
STA. 34+95.41 TO STA. 62+07.00	108.46	108.46	90		
STA. 62+07.00 TO STA. 71+82.00	39.00	39.00	33		
STA. 71+82.00 TO STA. 86+63.86	59.27	59.27	49		
STATION EQUATION STA. 86+63.86 BK = STA. 82+99.33 AH					
STA. 82+99.33 TO STA. 128+87.38	183.52	183.52	153		
STATION EQUATION STA. 128+87.38 BK = STA. 128+91.29 AH					
STA. 128+91.29 TO STA. 163+18.00	137.07	137.07	114		
STA. 163+18.00 TO STA. 174+02.00	43.36	43.36	36		
STA. 174+02.00 TO STA. 181+27.00	29.00	29.00	24		
STA. 181+27.00 TO STA. 187+95.91	26.76	26.76	22		
STA. 187+95.91 TO STA. 195+10.00	28.56	28.56	24		
I - 72 MAINLINE EASTBOUND					
STA. 688+82.59 TO STA. 694+78.17	23.82	23.82	20		
STA. 694+78.17 TO STA. 698+72.70	15.78	15.78	13		
WESTBOUND					
STA. 678+50.00 TO STA. 684+19.00	22.76	22.76	19		
STA. 684+19.00 TO STA. 688+95.90	19.08	19.08	16		
I - 55 BUSINESS LOOP NORTHBOUND					
STA. 29+73.82 TO STA. 39+47.62	38.95	38.95	32		
SOUTHBOUND					
STA. 31+22.00 TO STA. 44+00.00	51.12	51.12	43		
SIXTH STREET INTERCHANGE RAMP A					
STA. 17+00.00 TO STA. 29+93.00	51.72	51.72	43		
NE RAMP					
STA. 0+00 TO STA. 6+20.00	24.80	24.80	21		
STEVENS ON DRIVE RAMP A					
STA. 0+00 TO STA. 16+82.00	67.28	67.28	56		
RAMP B					
STA. 1+16.00 TO STA. 27+69.36	106.13	106.13	88		
RAMP C					
STA. 4+76.00 TO STA. 18+27.00	54.04	54.04	45		
RAMP D					
STA. 0+98.00 TO STA. 16+08.00	60.40	60.40	50		
			1,671.77		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. ⁺ RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
**	**	SANGAMON	85	13
CONTRACT NO. 72J83				
ILLINOIS FED. AID PROJECT				

*F.A.I. 55, F.A.I. 72, F.A.P. 666 ** (84-4, 84-3, 84-9, 110) RS

RAISED REFLECTIVE PAVEMENT MARKER										
LOCATION	LENGTH	LANE	COLOR	NUMBER OF	NUMBER OF	SPACING	TOTAL			
STATION TO STATION	(FOOT)	LOCATION		LINES	REFLECTORS	(FOOT)	(EACH)			
I-55 MAINLINE										
NORTHBOUND										
STA. 13+10.43 TO STA. 24+04.00	1093.57	CENTERLINE	CRYSTAL	2	2	80	56			
STA. 24+04.00 TO STA. 69+20.00	4516.00	CENTERLINE	CRYSTAL	1	2	80	114			
STA. 29+77.00 TO STA. 63+75.00	3398.00	INSIDE EOP	AMBER	1	1	40	86			
STA. 69+20.00 TO STA. 91+07.00	2187.00	CENTERLINE	CRYSTAL	2	2	80	110			
STA. 91+07.00 TO STA. 128+87.38	3780.38	CENTERLINE	CRYSTAL	1	2	80	96			
STATION EQUATION STA. 128+87.38 BK = STA. 128+91.29 AH										
STA. 128+91.29 TO STA. 195+10.00	6618.71	CENTERLINE	CRYSTAL	1	2	80	166			
SOUTHBOUND										
STA. 13+10.43 TO STA. 31+22.04	1811.61	CENTERLINE	CRYSTAL	2	2	80	92			
STA. 31+22.04 TO STA. 71+82.00	4059.96	CENTERLINE	CRYSTAL	1	2	80	102			
STA. 34+95.41 TO STA. 62+07.00	2711.59	OUTSIDE EOP	AMBER	1	1	40	69			
STA. 71+82.00 TO STA. 83+88.00	1206.00	CENTERLINE	CRYSTAL	2	2	80	61			
STA. 83+88.00 TO STA. 86+63.86	275.86	CENTERLINE	CRYSTAL	1	2	80	8			
STATION EQUATION STA. 86+63.86 BK = STA. 82+99.33 AH										
STA. 82+99.33 TO STA. 128+87.38	4588.05	CENTERLINE	CRYSTAL	1	2	80	116			
STATION EQUATION STA. 128+87.38 BK = STA. 128+91.29 AH										
STA. 128+91.29 TO STA. 195+10.00	6618.71	CENTERLINE	CRYSTAL	1	2	80	166			
I-72 MAINLINE										
EASTBOUND										
STA. 688+82.59 TO STA. 694+78.17	595.58	CENTERLINE	CRYSTAL	1	2	80	16			
STA. 694+78.17 TO STA. 698+72.70	394.53	CENTERLINE	CRYSTAL	1	2	80	11			
WESTBOUND										
STA. 678+50.00 TO STA. 688+95.90	1045.90	CENTERLINE	CRYSTAL	1	2	80	27			
I -55 BUSINESS LOOP										
NORTHBOUND										
STA. 29+73.82 TO STA. 39+47.62	973.80	CENTERLINE	CRYSTAL	1	2	80	25			
SOUTHBOUND										
STA. 44+00.00 TO STA. 63+50.00	1950.00	CENTERLINE	CRYSTAL	1	2	80	50			
SIXTH STREET INTERCHANGE										
RAMP A										
STA. 17+00.00 TO STA. 29+93.00	1293.00	OUTSIDE EOP	CRYSTAL	1	1	20	66			
STA. 29+93.00 TO STA. 26+13.00	380.0	GORE (RAMP)	CRYSTAL	1	1	40	11			
NE RAMP										
STA. 0+.00 TO STA. 6+00.00	600.00	OUTSIDE EOP	CRYSTAL	1	1	20	31			
STA. 3+14.00 TO STA. 6+00.00	286.00	GORE (RAMP)	CRYSTAL	1	1	40	8			
STEVENSON DRIVE										
RAMP A										
STA. 0+.00 TO STA. 6+68.00	668.00	OUTSIDE EOP	CRYSTAL	1	1	20	34			
STA. 3+02.00 TO STA. 6+68.00	366.00	GORE (RAMP)	CRYSTAL	1	1	40	10			
STA. 6+68.00 TO STA. 16+82.00	1014.00	INSIDE EOP	AMBER	1	1	40	26			
RAMP C										
STA. 4+76.00 TO STA. 18+27.00	1351.00	OUTSIDE EOP	CRYSTAL	1	1	20	69			
STA. 4+76.00 TO STA. 9+44.00	468.00	GORE (RAMP)	CRYSTAL	1	1	40	13			
STA. 9+44.00 TO STA. 18+27.00	883.00	INSIDE EOP	AMBER	1	1	40	23			
							TOTAL	1,662		

PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID-LINE 8"			LENGTH (FEET)	
LOCATION	STATION TO STATION			
NB I-55 / NB BL-55 DIVERGENCE				
STA. 24+04.00 TO STA. 29+78.00			1148.00	
SB I-55 / SB BL-55 CONVERGENCE				
STA. 31+22.00 TO STA. 34+94.00			744.00	
NB I-55 / EB I-72 CONVERGENCE				
STA. 63+73.00 TO STA. 69+20.00			1094.00	
SB I-55 / WB I-72 DIVERGENCE				
STA. 62+08.00 TO STA. 71+82.00			1948.00	
WB I-72 / NE RAMP				
STA. 688+97.00 TO STA. 698+71.82			1949.64	
SIXTH STREET INTERCHANGE				
RAMP A				
STA. 26+13.00 TO STA. 29+93.00			760.00	
NE RAMP				
STA. 3+14.60 TO STA. 6+20.00			610.80	
STEVENSON DRIVE				
RAMP A				
STA. 3+02.00 TO STA. 6+69.00			734.00	
RAMP B				
STA. 7+50.00 TO STA. 12+19.00			938.00	
RAMP C				
STA. 4+76.00 TO STA. 7+21.00			245.00	
STA. 7+21.00 TO STA. 9+44.00			446.00	
RAMP D				
STA. 11+21.00 TO STA. 16+08.00			974.00	
			TOTAL	11,591.44

PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"				NUMBER OF LINES	LENGTH (FOOT)
LOCATION	STATION TO STATION				
I-55 MAINLINE					
NORTHBOUND					
STA. 13+10.43 TO STA. 24+04.00			2		283.39
STA. 24+04.00 TO STA. 69+20.00			1		1139.00
STA. 69+20.00 TO STA. 91+70.00			2		572.50
STA. 91+70.00 TO STA. 128+87.38			2		939.35
STATION EQUATION STA. 128+87.38 BK = STA. 128+91.29 AH					
STA. 128+91.29 TO STA. 195+10.00			1		1664.68
SOUTHBOUND					
STA. 13+10.43 TO STA. 31+22.00			2		462.89
STA. 31+22.00 TO STA. 71+82.00			1		1025.00
STA. 71+82.00 TO STA. 86+63.86			2		380.47
STATION EQUATION STA. 86+63.86 BK = STA. 82+99.33 AH					
STA. 82+99.33 TO STA. 128+87.38			2		1157.01
STATION EQUATION STA. 128+87.38 BK = STA. 128+91.29 AH					
STA. 128+91.29 TO STA. 195+10.00			1		1664.68
I-72 MAINLINE					
EASTBOUND					
STA. 688+82.59 TO STA. 699+21.09			1		269.63
WESTBOUND					
STA. 678+50.00 TO STA. 698+72.69			1		515.67
I-55 BUSINESS LOOP					
NORTHBOUND					
STA. 24+04.00 TO STA. 39+47.62			1		395.91
SOUTHBOUND					
STA. 31+22.00 TO STA. 44+00.00			1		329.50
				TOTAL	10,799.67

MODEL - Default
 FILE - AutoCAD
 PROJECT - 10/20/2017
 USER - coxte
 DESIGNED -
 DRAWN -
 CHECKED -
 DATE -
 REVISIONS -
 REVISED -
 REVISED -
 REVISED -
 REVISED -
 STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 SCHEDULE OF QUANTITIES
 SCALE: SHEET OF SHEETS STA. TO STA.
 COUNTY SANGAMON
 CONTRACT NO. 72J83
 ILLINOIS FED. AID PROJECT
 *F.A.I. 55, F.A.I. 72, F.A.P. 666 *(84-4, 84-3, 84-9, 110) RS

USER NAME = coxte	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/20/2017	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCALE:		SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS	FED. AID PROJECT

MILLING SCHEDULE												
LOCATION STATION TO STATION	LENGTH (FOOT)	PAVEMENT WIDTH (FOOT)	OUTSIDE SHLDR WIDTH (FOOT)	MEDIAN SHLDR WIDTH (FOOT)	RAMP WIDTH (FOOT)	OBSERVED AVG. MAINLINE HMA THICKNESS (INCH)	HMA SURF. REM BUTT-JOINT (SQ. YD)	HMA SURF. REM VARIABLE DEPTH (SQ. YD)	HMA SURF. REM 1.5" (SQ. YD)	SHLDR HMA SURF. REM 2" (SQ. YD)		
I-72 MAINLINE												
EASTBOUND												
STA. 688+82.59 TO STA. 688+92.59	10.00	24	10	6	-	7.75	44.44	-	-	-		
STA. 688+92.59 TO STA. 694+78.17	585.58	24	10 TO 16	6	-	7.75	-	1561.55	-	1236.22		
STA. 694+78.17 TO STA. 695+89.88	111.71	24	10	6	-	7.75	-	297.89	-	235.83		
WESTBOUND												
STA. 678+50.00 TO STA. 678+60.00	10.00	24	8	4	-	6.25	40.00	-	-	-		
STA. 678+60.00 TO STA. 688+95.90	1035.90	24	8	4	-	6.25	-	2762.40	-	1381.20		
I-55 BUSINESS LOOP												
NORTHBOUND												
STA. 27+75.83 TO STA. 39+37.62	1161.79	24	4 TO 8	4	-	7	-	3098.11	-	1290.88		
STA. 39+37.62 TO STA. 39+47.62	10.00	24	8	4	-	7	40.00	-	-	-		
SOUTHBOUND												
STA. 31+21.97 TO STA. 36+46.22	524.25	28 to 40	4	4	-	3.75	-	1980.50	-	466.00		
STA. 36+46.22 TO STA. 43+95.00	748.78	40 to 24	4	0 TO 8	-	3.75	-	-	-	-		
STA. 43+95.00 TO STA. 44+00.00	5.00	24	4	8	-	3.75	22.22	-	-	-		
SIXTH STREET INTERCHANGE												
RAMP A												
STA. 17+00.00 TO STA. 17+50.00	50.00	-	6	4	16	-	144.44	-	-	-		
STA. 17+50.00 TO STA. 26+13.00	863.00	-	6	4	16	-	-	1534.22	-	958.89		
STA. 26+13.00 TO STA. 29+93.00	380.00	-	0	4	16	-	-	675.56	-	168.89		
GORE AREA (MEASURED IN CAD)	-	VARIES	VARIES	VARIES	-	-	-	98.00	-	218.67		
NE RAMP												
STA. 0+.00 TO STA. 6+10.00	610.00	-	6	4	16	-	-	1084.44	-	677.78		
STA. 6+10.00 TO STA. 6+20.00	10.00	-	6	4	16	-	28.89	-	-	-		
GORE AREA (MEASURED IN CAD)	-	VARIES	VARIES	VARIES	-	-	-	269.11	-	224.00		
STEVENSON DRIVE												
RAMP A												
STA. 0+.00 TO STA. 12+29.60	1229.60	-	8	4	16	-	-	2185.96	-	1639.47		
STA. 12+29.60 TO STA. 13+49.60	120.00	-	8 to 4	4	20	-	-	266.67	-	133.33		
STA. 13+49.60 TO STA. 14+63.86	114.26	-	4	4	24	-	-	304.69	-	101.56		
STA. 14+63.86 TO STA. 16+57.00	193.14	-	4	4	24	-	-	515.04	-	171.68		
STA. 16+57.00 TO STA. 16+82.00	25.00	-	4	4	24	-	88.89	-	-	-		
GORE AREA (MEASURED IN CAD)	-	VARIES	VARIES	VARIES	-	-	-	248.11	-	206.44		
RAMP B												
STA. 1+16.00 TO STA. 1+41.00	25.00	-	8	4	16	-	77.78	-	-	-		
STA. 1+41.00 TO STA. 12+19.26	1078.26	-	8	4	16	-	-	1916.91	-	1437.68		
STA. 12+19.26 TO STA. 22+19.95	1000.69	-	8	0	12	-	-	-	1334.25	889.50		
STA. 22+19.95 TO STA. 27+69.36	549.41	-	8	0	12 TO 1	-	-	-	396.80	488.36		
GORE AREA (MEASURED IN CAD)	-	VARIES	VARIES	VARIES	-	-	-	91.44	-	104.22		
RAMP C												
STA. 0+.00 TO STA. 2+75.77	275.77	-	0 TO 6	0	1 TO 12	-	-	-	199.17	91.92		
STA. 2+75.77 TO STA. 6+83.67	407.90	-	0 TO 6	0	12	-	-	-	543.87	135.97		
STA. 6+83.67 TO STA. 9+43.11	259.44	-	8	4	12 TO 16	-	-	403.57	-	345.92		
STA. 9+43.11 TO STA. 14+72.79	529.68	-	8	4	16	-	-	941.65	-	706.24		
STA. 14+72.79 TO STA. 18+02.00	329.21	-	4	4	20.5	-	-	749.87	-	292.63		
STA. 18+02.00 TO STA. 18+27.00	25.00	-	4	4	20.5	-	79.17	-	-	-		
GORE AREA (MEASURED IN CAD)	-	VARIES	VARIES	VARIES	-	-	-	185.22	-	112.56		
RAMP D												
STA. 0+98.00 TO STA. 1+23.00	25.00	-	8	4	16	-	77.78	-	-	-		
STA. 1+23.00 TO STA. 11+21.00	998.00	-	8	4	16	-	-	1774.22	-	1330.67		
STA. 11+21.00 TO STA. 16+08.00	487.00	-	8	0	16	-	-	865.78	-	432.89		
GORE AREA (MEASURED IN CAD)	-	VARIES	VARIES	VARIES	-	-	-	90.22	-	266.78		
							SUB TOTAL - 2	643.61	23,901.13	2,474.08	15,746.18	
							SUB TOTAL - 1	182.22	113,431.33	4,013.33	52,066.49	
							TOTAL	825.83	137,332.46	6,487.42	67,812.67	

MODEL: Default
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USER NAME = coxte	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,0000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 10/20/2017	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE:	SHEET	OF	SHEETS	STA.	TO STA.
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**	SANGAMON	85	16
CONTRACT NO. 72J83				
ILLINOIS FED. AID PROJECT				

*F.A.I. 55, F.A.I. 72, F.A.P. 666 ** (84-4, 84-3, 84-9, 110) RS

GUARDRAIL ITEMS														
LOCATION STATION TO STATION		GUARDRAIL REMOVAL (FOOT)	SPBGR TY A 6' POST (FOOT)	GUARDRAIL REFLECTORS TY A (EACH)	TERMINAL MARKERS (EACH)	TYPE 1 SPL TANG TERMINAL (EACH)	TRAFF BAR TY 2 TERMINAL (EACH)	TRAFF BAR TY 5 TERMINAL (EACH)	TRAFF BAR TY 6 TERMINAL (EACH)	TRAFF BAR TY 6B TERMINAL (EACH)	TRAFF BAR TY 6B (SPECIAL) (EACH)			
I-55 MAINLINE														
NORTHBOUND														
STA. 23+05.00 TO STA. 23+55.00	RT	50.0	-	1	1	1	-	-	-	-	-			
STA. 23+55.00 TO STA. 24+17.50	RT	62.5	62.5	2	-	-	-	-	-	-	-			
STA. 24+17.50 TO STA. 24+30.00	RT	12.5	-	1	-	-	1	-	-	-	-			
STA. 58+41.00 TO STA. 58+91.00	RT	50.0	-	1	1	1	-	-	-	-	-			
STA. 58+91.00 TO STA. 61+16.50	RT	225.5	225.0	3	-	-	-	-	-	-	-			
STA. 61+16.50 TO STA. 61+29.00	RT	12.5	-	1	-	-	1	-	-	-	-			
STA. 63+73.00 TO STA. 82+94.34	LT	1921.3	1921.3	9	-	-	-	-	-	-	-			
STA. 82+94.34 TO STA. 83+46.23	LT	51.9	-	1	-	-	-	-	-	1	-			
STA. 83+61.26 TO STA. 84+00.65	LT	39.4	-	3	-	-	-	-	1	-	-			
STA. 92+50.00 TO STA. 92+63.00	LT	12.5	-	1	-	-	1	-	-	-	-			
STA. 80+56.61 TO STA. 81+06.61	RT	50.0	-	1	1	1	-	-	-	-	-			
STA. 81+06.61 TO STA. 82+94.11	RT	187.5	187.5	2	-	-	-	-	-	-	-			
STA. 82+94.11 TO STA. 83+46.00	RT	51.9	-	1	-	-	-	-	-	1	-			
STA. 121+46.96 TO STA. 121+96.96	RT	50.0	-	1	1	1	-	-	-	-	-			
STA. 121+96.96 TO STA. 126+46.96	RT	450.0	450.0	5	-	-	-	-	-	-	-			
STA. 126+46.96 TO STA. 126+98.85	RT	51.9	-	1	-	-	-	-	-	1	-			
STA. 127+30.00 TO STA. 127+67.50	LT	37.5	-	-	-	-	-	-	-	-	-			
STA. 156+20.29 TO STA. 156+70.29	RT	50.0	-	1	1	1	-	-	-	-	-			
STA. 156+70.29 TO STA. 166+57.79	RT	987.5	987.5	6	-	-	-	-	-	-	-			
STA. 166+57.79 TO STA. 167+09.67	RT	51.9	-	1	-	-	-	-	-	1	-			
STA. 167+19.70 TO STA. 167+33.00	RT	13.3	-	1	-	-	-	1	-	-	-			
STA. 167+33.00 TO STA. 171+47.61	RT	414.6	414.6	5	-	-	-	-	-	-	-			
STA. 171+47.61 TO STA. 171+99.50	RT	51.9	-	1	-	-	-	-	-	1	-			
STA. 172+03.36 TO STA. 172+44.63	RT	41.3	-	1	-	-	-	-	-	-	1			
STA. 172+47.70 TO STA. 172+61.00	RT	13.3	-	1	-	-	-	1	-	-	-			
STA. 172+61.00 TO STA. 172+98.50	RT	37.5	37.5	2	-	-	-	-	-	-	-			
STA. 172+98.50 TO STA. 173+11.00	RT	12.5	-	1	-	-	1	-	-	-	-			
SOUTHBOUND														
STA. 24+18.00 TO STA. 24+30.50	RT	12.5	-	1	-	-	1	-	-	-	-			
STA. 24+30.50 TO STA. 25+05.50	RT	75.0	75.0	2	-	-	-	-	-	-	-			
STA. 24+05.50 TO STA. 24+55.50	RT	50.0	-	1	1	1	-	-	-	-	-			
STA. 40+99.00 TO STA. 41+11.50	RT	12.5	-	1	-	-	1	-	-	-	-			
STA. 41+11.50 TO STA. 45+24.00	RT	412.5	412.5	5	-	-	-	-	-	-	-			
STA. 45+24.00 TO STA. 45+74.00	RT	50.0	-	1	1	1	-	-	-	-	-			
STA. 42+39.30 TO STA. 42+91.19	LT	51.9	-	1	-	-	-	-	-	1	-			
STA. 42+91.19 TO STA. 45+16.19	LT	225.0	225.0	3	-	-	-	-	-	-	-			
STA. 45+16.19 TO STA. 45+66.19	LT	50.0	-	1	1	1	-	-	-	-	-			
STA. 58+76.48 TO STA. 59+28.37	LT	51.9	-	1	-	-	-	-	-	1	-			
STA. 59+28.37 TO STA. 60+90.87	LT	162.5	162.5	2	-	-	-	-	-	-	-			
STA. 60+90.87 TO STA. 61+40.87	LT	50.0	-	1	1	1	-	-	-	-	-			
STA. 58+88.00 TO STA. 59+00.50	RT	12.5	-	1	-	-	1	-	-	-	-			
STA. 59+00.50 TO STA. 62+13.00	RT	312.5	312.5	4	-	-	-	-	-	-	-			
STA. 62+13.00 TO STA. 62+63.00	RT	50.0	-	1	1	1	-	-	-	-	-			
STA. 69+97.50 TO STA. 70+10.00	RT	12.5	-	1	-	-	1	-	-	-	-			
STA. 70+10.00 TO STA. 86+63.86	RT	1653.9	1653.9	8	-	-	-	-	-	-	-			
STATION EQUATION STA. 86+63.86 BK = STA. 82+99.33 AH														
SUB TOTAL - 1		8,235.8	7,127.3	89	10	10	8	2	1	7	1			

MODEL: Default
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USER NAME = coxte	DESIGNED -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/20/2017	DATE -	REVISED -

DRAWN -	REVISED -
CHECKED -	REVISED -
DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.* RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**	SANGAMON	85	19
CONTRACT NO. 72J83				
ILLINOIS FED. AID PROJECT				

*F.A.I. 55, F.A.I. 72, F.A.P. 666 ** (84-4, 84-3, 84-9, 110) RS

GUARDRAIL ITEMS														
LOCATION STATION TO STATION		GUARDRAIL REMOVAL (FOOT)	SPBGR TY A 6' POST (FOOT)	GUARDRAIL REFLECTORS TY A (EACH)	TERMINAL MARKERS (EACH)	TYPE 1 SPL TANG TERMINAL (EACH)	TRAFF BAR TY 2 TERMINAL (EACH)	TRAFF BAR TY 5 TERMINAL (EACH)	TRAFF BAR TY 6 TERMINAL (EACH)	TRAFF BAR TY 6B TERMINAL (EACH)	TRAFF BAR TY 6B (SPECIAL) (EACH)			
SOUTHBOUND														
STA. 82+99.33 TO STA. 83+07.89	RT	8.6	8.6	-	-	-	-	-	-	-	-			
STA. 83+07.89 TO STA. 83+59.78	RT	51.9	-	1	-	-	-	-	-	1	-			
STA. 83+62.15 TO STA. 84+01.54	RT	39.4	-	1	-	-	-	-	1	-	-			
STA. 84+01.54 TO STA. 92+64.04	RT	862.5	862.5	11	-	-	-	-	-	-	-			
STA. 92+64.04 TO STA. 93+14.04	RT	50.0	-	1	1	1	-	-	-	-	-			
STA. 71+60.00 TO STA. 71+72.50	LT	12.5	-	1	-	-	1	-	-	-	-			
STA. 71+72.50 TO STA. 74+85.00	LT	312.5	312.5	4	-	-	-	-	-	-	-			
STA. 74+85.00 TO STA. 75+35.00	LT	50.0	-	1	1	1	-	-	-	-	-			
STA. 83+62.37 TO STA. 84+14.26	LT	51.9	-	1	-	-	-	-	-	1	-			
STA. 84+14.26 TO STA. 88+39.26	LT	425.0	425.0	5	-	-	-	-	-	-	-			
STA. 88+39.26 TO STA. 88+89.26	LT	50.0	-	1	1	1	-	-	-	-	-			
STA. 124+57.75 TO STA. 124+70.25	LT	12.5	-	1	-	-	1	-	-	-	-			
STA. 124+70.25 TO STA. 127+45.25	LT	275.0	275.0	3	-	-	-	-	-	-	-			
STA. 127+45.25 TO STA. 127+58.55	LT	13.3	-	1	-	-	-	1	-	-	-			
STA. 127+72.98 TO STA. 128+24.87	LT	51.9	-	1	-	-	-	-	-	1	-			
STA. 128+24.87 TO STA. 130+74.87	LT	250.0	250.0	3	-	-	-	-	-	-	-			
STA. 130+74.87 TO STA. 131+24.87	LT	50.0	-	1	1	1	-	-	-	-	-			
STA. 127+30.00 TO STA. 127+67.50	RT	37.5	-	-	-	-	-	-	-	-	-			
STA. 153+46.24 TO STA. 153+58.74	LT	12.5	-	1	-	-	1	-	-	-	-			
STA. 153+58.74 TO STA. 166+21.24	LT	1262.5	1262.5	8	-	-	-	-	-	-	-			
STA. 166+21.24 TO STA. 166+34.54	LT	13.3	-	1	-	-	-	1	-	-	-			
STA. 166+44.26 TO STA. 166+96.15	LT	51.9	-	1	-	-	-	-	-	1	-			
STA. 166+96.15 TO STA. 171+36.94	LT	440.8	440.8	5	-	-	-	-	-	-	-			
STA. 171+36.94 TO STA. 171+50.24	LT	13.3	-	1	-	-	-	1	-	-	-			
STA. 171+55.55 TO STA. 171+96.82	LT	41.3	-	1	-	-	-	-	-	-	1			
STA. 172+01.91 TO STA. 172+53.80	LT	51.9	-	1	-	-	-	-	-	1	-			
STA. 172+53.80 TO STA. 173+66.30	LT	112.5	112.5	2	-	-	-	-	-	-	-			
STA. 173+66.30 TO STA. 174+16.30	LT	50.0	-	1	1	1	-	-	-	-	-			
STA. 171+55.00 TO STA. 171+67.50	RT	12.5	-	1	-	-	1	-	-	-	-			
STA. 171+67.50 TO STA. 173+17.50	RT	150.0	150.0	2	-	-	-	-	-	-	-			
STA. 173+17.50 TO STA. 173+67.50	RT	50.0	-	1	1	1	-	-	-	-	-			
SUB TOTAL - 2		4,866.9	4,099.4	63	6	6	4	3	1	5	1			

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PLOT DATE = 10/20/2017	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES			
SCALE:	SHEET	OF	SHEETS

F.A.* RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**	SANGAMON	85	20
CONTRACT NO. 72J83				
			ILLINOIS	FED. AID PROJECT

*F.A.J. 55, F.A.J. 72, F.A.P. 666 ** (84-4, 84-3, 84-9, 110) RS

GUARDRAIL ITEMS															
LOCATION STATION TO STATION		GUARDRAIL REMOVAL (FOOT)	SPBGR TY A 6' POST (FOOT)	GUARDRAIL REFLECTORS TY A (EACH)	TERMINAL MARKERS (EACH)	TYPE 1 SPL TANG TERMINAL (EACH)	TRAFF BAR TY 2 TERMINAL (EACH)	TRAFF BAR TY 5 TERMINAL (EACH)	TRAFF BAR TY 6 TERMINAL (EACH)	TRAFF BAR TY 6B TERMINAL (EACH)	TRAFF BAR TY 6B (SPECIAL) (EACH)				
I-72 MAINLINE															
EASTBOUND															
STA. 687+81.89 TO STA. 688+21.28	RT	39.4	-	1	-	-	-	-	1	-	-				
STA. 688+21.28 TO STA. 694+71.28	RT	650.0	650.0	8	-	-	-	-	-	-	-				
STA. 694+71.28 TO STA. 694+83.78	RT	12.5	-	1	-	-	1	-	-	-	-				
STA. 688+54.02 TO STA. 688+93.42	LT	39.4	-	1	-	-	-	-	1	-	-				
STA. 688+93.42 TO STA. 694+73.08	LT	579.7	579.7	7	-	-	-	-	-	-	-				
WESTBOUND															
STA. 686+48.61 TO STA. 686+98.61	RT	50.0	-	1	1	1	-	-	-	-	-				
STA. 686+98.61 TO STA. 688+11.11	RT	112.5	112.5	2	-	-	-	-	-	-	-				
STA. 688+11.11 TO STA. 688+23.61	RT	12.5	-	1	-	-	-	-	-	-	-				
I-55 BUSINESS LOOP															
NORTHBOUND															
STA. 33+29.10 TO STA. 33+79.10	RT	50.0	-	1	1	1	-	-	-	-	-				
STA. 33+79.10 TO STA. 40+04.10	RT	625.0	625.0	8	-	-	-	-	-	-	-				
STA. 40+04.10 TO STA. 40+43.50	RT	39.4	-	1	-	-	-	-	1	-	-				
STA. 36+34.89 TO STA. 36+84.89	LT	50.0	-	1	1	1	-	-	-	-	-				
STA. 36+84.89 TO STA. 39+47.39	LT	262.5	262.5	3	-	-	-	-	-	-	-				
STA. 39+47.39 TO STA. 39+86.79	LT	39.4	-	1	-	-	-	-	1	-	-				
SIXTH STREET INTERCHANGE															
NE RAMP															
STA. 0+37.00 TO STA. 0+87.00	RT	50.0	-	1	1	1	-	-	-	-	-				
STA. 1+36.00 TO STA. 4+36.00	RT	300.0	300.0	4	-	-	-	-	-	-	-				
STA. 4+36.00 TO STA. 4+48.50	RT	12.5	-	1	-	-	1	-	-	-	-				
SUB TOTAL - 3		2,924.8	2,529.7	43	4	4	2	0	4	0	0				
SUB TOTAL - 2		4,866.9	4,099.4	63	6	6	4	3	1	5	1				
SUB TOTAL - 1		8,235.8	7,127.3	89	10	10	8	2	1	7	1				
TOTAL		16,027.4	13,756.4	195	20	20	14	5	6	12	2				

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PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/20/2017	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.* RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**	SANGAMON	85	21
			CONTRACT NO. 72J83	
		ILLINOIS	FED. AID PROJECT	

*F.A.I. 55, F.A.I. 72, F.A.P. 666 ** (84-4, 84-3, 84-9, 110) RS

PATCHING SCHEDULE					CLASS B, 16 INCH			SAW CUTS (FT)	TIE BARS 3/4" (EA.)	DOWELS (EA.)
STATION	LANE	LENGTH	WIDTH	AREA	TY II	TY III	TY IV			
	DL/ML/PL	(FT)	(FT)	(S.Y.)	(S.Y.)	(S.Y.)	(S.Y.)			
SOUTHBOUND										
161+00	SBPL	6	12	8.0	8.0	--	--	60.0	--	20.0
157+00	SBPL	6	12	8.0	8.0	--	--	60.0	--	20.0
151+00	SBPL	8	12	10.7	10.7	--	--	64.0	--	20.0
137+00	SBPL	10	12	13.3	13.3	--	--	68.0	--	20.0
135+00	SBPL	15	12	20.0	--	20.0	--	78.0	--	20.0
133+00	SBPL	6	12	8.0	8.0	--	--	60.0	--	20.0
129+00	SBPL	12	12	16.0	--	16.0	--	72.0	--	20.0
128+40	SBPL	8	12	10.7	10.7	--	--	64.0	--	20.0
128+00	SBPL	8	12	10.7	10.7	--	--	64.0	--	20.0
120+00	SBPL	6	12	8.0	8.0	--	--	60.0	--	20.0
115+00	SBPL	6	12	8.0	8.0	--	--	60.0	--	20.0
102+00	SBPL	8	12	10.7	10.7	--	--	64.0	--	20.0
100+00	SBPL	6	12	8.0	8.0	--	--	60.0	--	20.0
98+00	SBPL	8	12	10.7	10.7	--	--	64.0	--	20.0
56+50	SBPL	6	12	8.0	8.0	--	--	60.0	--	20.0
34+40	SBPL	8	12	10.7	10.7	--	--	64.0	--	20.0
28+00	SBPL	6	12	8.0	8.0	--	--	60.0	--	20.0
I-72 MAINLINE										
EASTBOUND										
699+00	EBDL	6	12	8.0	8.0	--	--	60.0	--	20.0
700+00	EBDL	6	12	8.0	8.0	--	--	60.0	--	20.0
700+00	EBPL	6	12	8.0	8.0	--	--	60.0	--	20.0
703+00	EBPL	6	12	8.0	8.0	--	--	60.0	--	20.0
705+00	EBPL	6	12	8.0	8.0	--	--	60.0	--	20.0
WESTBOUND										
707+50	WBDL	10	12	13.3	13.3	--	--	68.0	--	20.0
706+00	WBDL	6	12	8.0	8.0	--	--	60.0	--	20.0
698+50	WBDL	6	12	8.0	8.0	--	--	60.0	--	20.0
697+00	WBDL	6	12	8.0	8.0	--	--	60.0	--	20.0
690+00	WBDL	6	12	8.0	8.0	--	--	60.0	--	20.0
685+00	WBDL	15	12	20.0	--	20.0	--	78.0	--	20.0
683+50	WBDL	6	12	8.0	8.0	--	--	60.0	--	20.0
683+00	WBDL	6	12	8.0	8.0	--	--	60.0	--	20.0
698+50	WBPL	6	12	8.0	8.0	--	--	60.0	--	20.0
697+00	WBPL	6	12	8.0	8.0	--	--	60.0	--	20.0
690+00	WBPL	6	12	8.0	8.0	--	--	60.0	--	20.0
I-55 BUSINESS LOOP										
NORTHBOUND										
30+00	NBDL	6	12	8.0	8.0	--	--	60.0	--	20.0
34+70	NBDL	6	12	8.0	8.0	--	--	60.0	--	20.0
30+00	NBPL	6	12	8.0	8.0	--	--	60.0	--	20.0
34+70	NBPL	6	12	8.0	8.0	--	--	60.0	--	20.0
SOUTHBOUND										
37+50	SBDL	12	12	16.0	--	16.0	--	72.0	--	20.0
36+50	SBDL	6	12	8.0	8.0	--	--	60.0	--	20.0
33+50	SBDL	6	12	8.0	8.0	--	--	60.0	--	20.0
28+50	SBDL	8	12	10.7	10.7	--	--	64.0	--	20.0
37+50	SBPL	12	12	16.0	--	16.0	--	72.0	--	20.0
33+50	SBPL	6	12	8.0	8.0	--	--	60.0	--	20.0
31+00	SBPL	6	12	8.0	8.0	--	--	60.0	--	20.0
28+50	SBPL	8	12	10.7	10.7	--	--	64.0	--	20.0
SUB TOTAL-3					352.0	88.0	0.0	2,820.0	0.0	900.0

PATCHING SCHEDULE					CLASS B, 16 INCH			SAW CUTS (FT)	TIE BARS 3/4" (EA.)	DOWELS (EA.)
STATION	LANE	LENGTH	WIDTH	AREA	TY II	TY III	TY IV			
	DL/ML/PL	(FT)	(FT)	(S.Y.)	(S.Y.)	(S.Y.)	(S.Y.)			
SIXTH STREET INTERCHANGE										
RAMP "A" - I-72 EB TO I-55 SB										
	NONE			--	--	--	--	--	--	--
"NE RAMP" - I-55 SB TO NB 6th STREET.										
	NONE			--	--	--	--	--	--	--
STEVENSON DRIVE										
RAMP "A" - I-55 SB TO STEVENSON DRIVE										
4+50	OFF RAMP	6	16	10.7	10.7	--	--	82.0	--	14.0
RAMP "B" - STEVENSON DRIVE TO I-55 SB										
	ON RAMP	NONE		--	--	--	--	--	--	--
RAMP "C" - I-55 NB TO STEVENSON DRIVE										
15+00	OFF RAMP	6	16	10.7	10.7	--	--	82.0	--	14.0
RAMP "D" - STEVENSON DRIVE TO I-55 NB										
	ON RAMP	NONE		--	--	--	--	--	--	--
SUB TOTAL - 4					21.3	0.0	0.0	164.0	0.0	28.0
SUB TOTAL - 3					352.0	88.0	0.0	2,820.0	0.0	900.0
SUB TOTAL - 2					520.0	126.7	86.7	4,124.0	59.0	1,240.0
SUB TOTAL - 1					553.3	165.3	86.7	4,280.0	61.0	1,280.0
TOTAL					1,446.7	380.0	173.3	11,388.0	120.0	3,448.0

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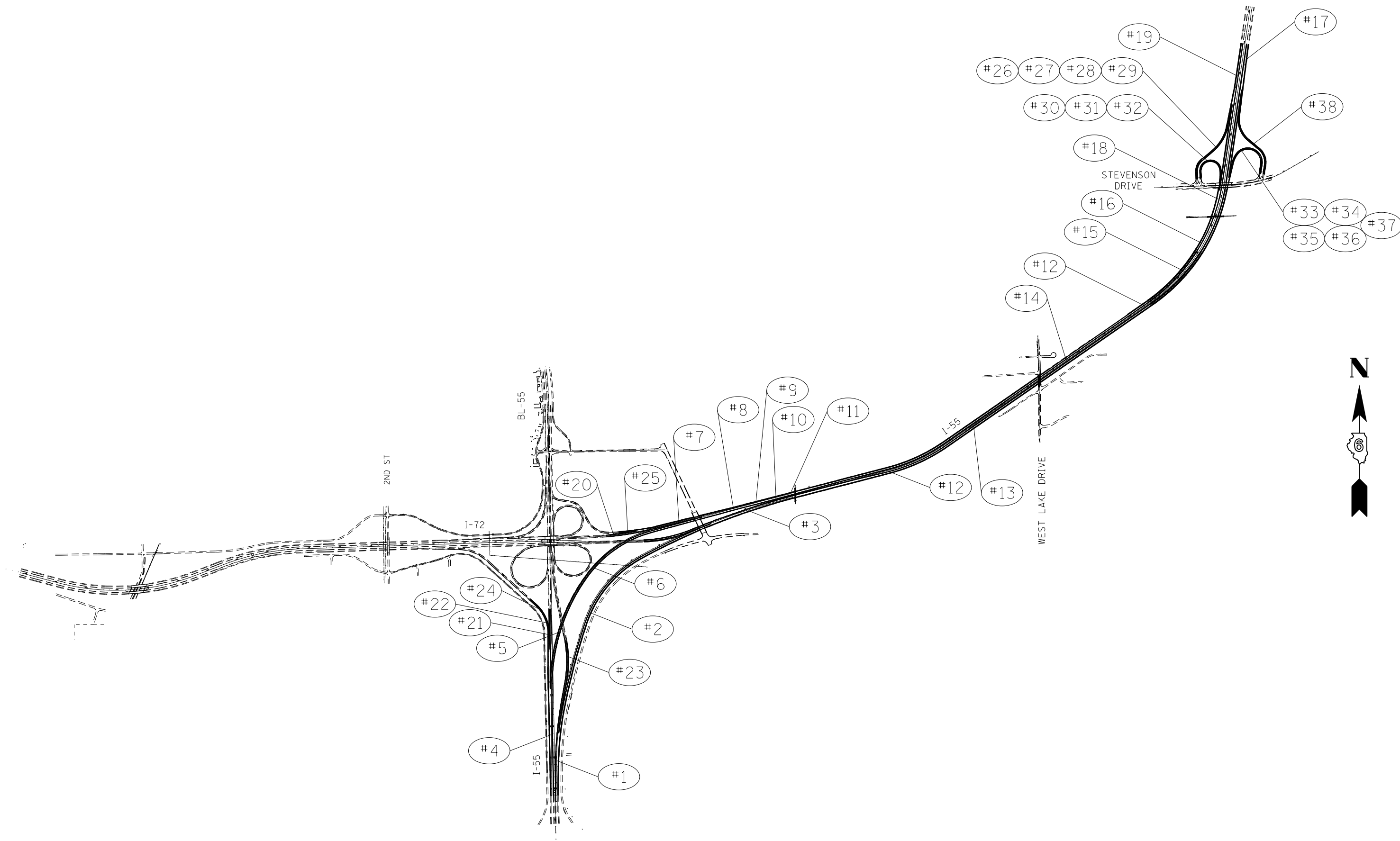
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE:	SHEET	OF	SHEETS	STA.	TO STA.
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F.A.* RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**	SANGAMON	85	23
CONTRACT NO. 72J83				
		ILLINOIS	FED. AID PROJECT	

*F.A.I. 55, F.A.I. 72, F.A.P. 666 ** (84-4, 84-3, 84-9, 110) RS



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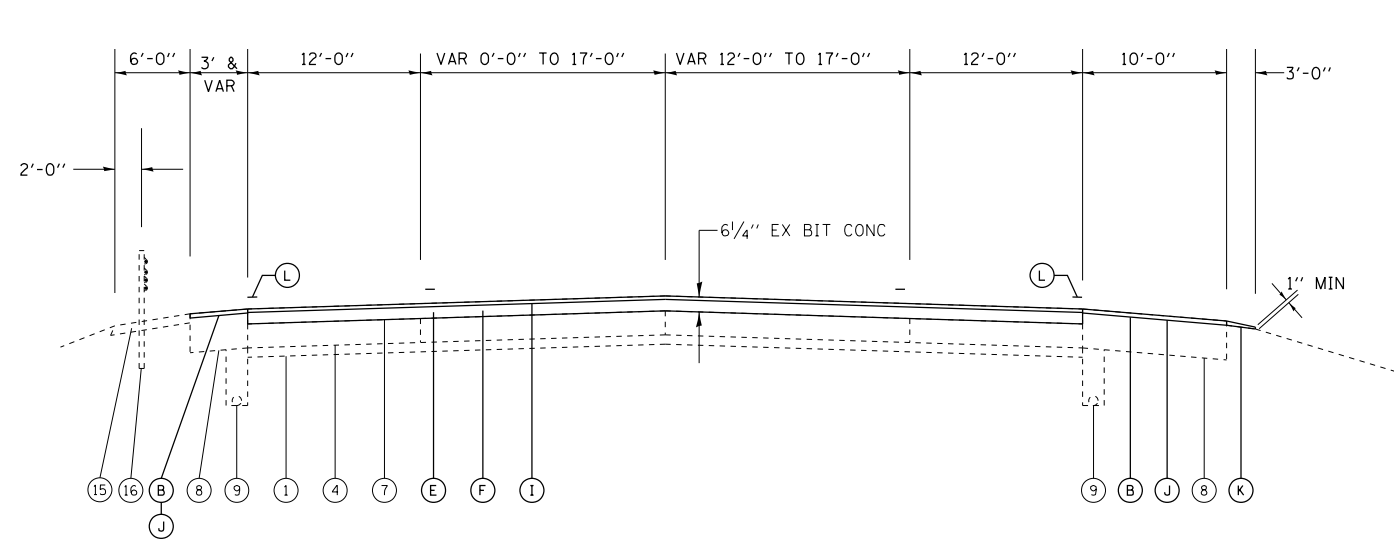
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

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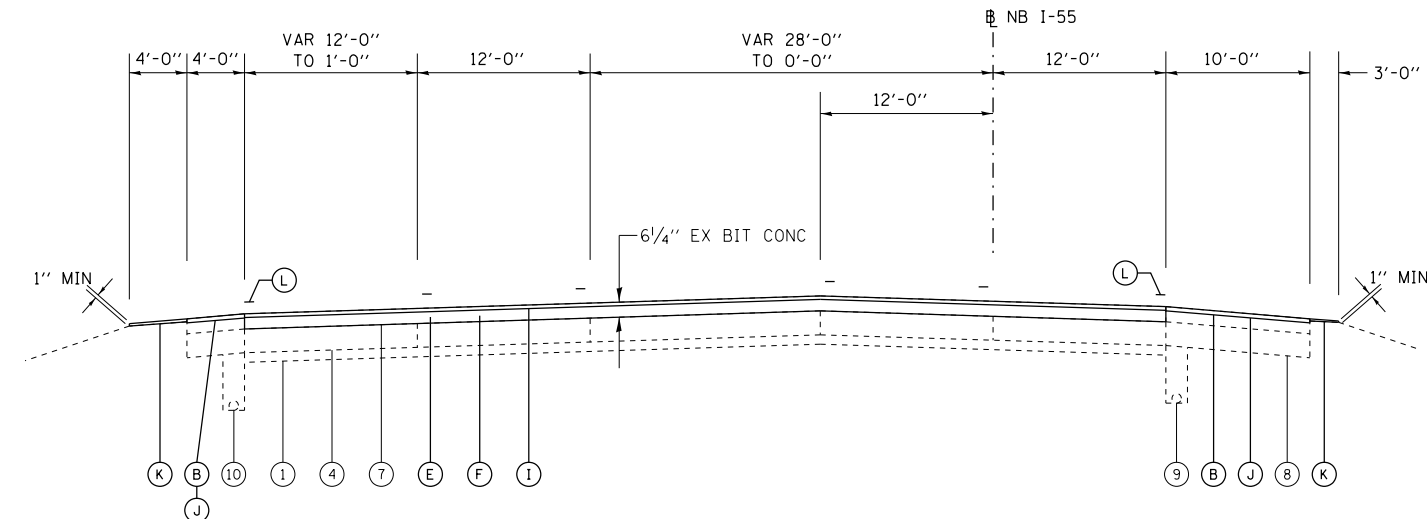
SCALE:	SHEET	OF	SHEETS	STA.	TO STA.
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**	SANGAMON	85	24
CONTRACT NO. 72J83				
ILLINOIS FED. AID PROJECT				

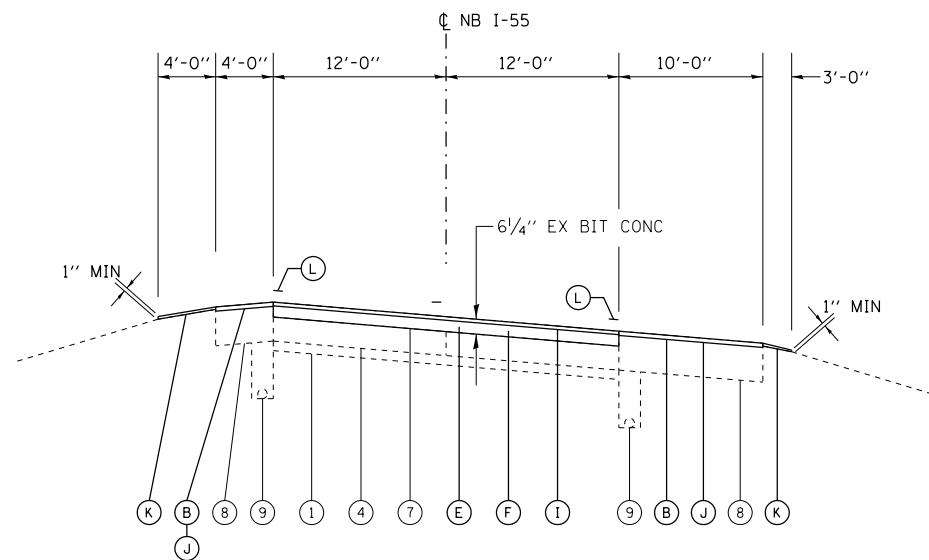
*F.A.I. 55, F.A.I. 72, F.A.P. 666 ** (84-4, 84-3, 84-9, 110) RS



TYPICAL #1
NORTHBOUND FAI 55
STA. 13+10.43 TO STA. 30+16.00



TYPICAL #3
NORTHBOUND FAI 55
STA. 64+66.59 TO STA. 98+11.00



TYPICAL #2
NORTHBOUND FAI 55
STA. 30+16.00 TO STA. 64+66.59

PAVED SHOULDER SLOPE
4.0% CROSS SLOPE IS DESIRABLE ON NORMAL CROWN SECTIONS

ON THE HIGH SIDE OF SUPERELEVATED CURVES, SLOPE THE SHOULDER SO THAT THE ALGEBRETIC DIFFERENCE BETWEEN THE PAVEMENT AND SHOULDERS ARE NOT GREATER THAN 8%.

ON THE LOW SIDE OF SUPERELEVATED CURVES, SLOPE THE SHOULDER 4% UNLESS THE SUPER ELEVATED SECTION IS GREATER THAN 4% THEN MATCH SUPERELEVATED SLOPE

PAVEMENT SLOPE
CROSS SLOPE IN CROWN SECTIONS: SHALL BE 1.5%

SUPER ELEVATED SECTIONS: SEE SUPER ELEVATION DETAILS FOR MORE INFORMATION

LEGEND

①	EX STABILIZED SUB-BASE 4"	Ⓐ	PR HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
②	EX AGGREGATE SUB-BASE 4"	Ⓑ	PR HOT-MIX ASPHALT SURFACE REMOVAL, 2"
③	EX SUB-BASE GRANULAR MATERIAL 12"	Ⓒ	PR HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4" AVG.
④	EX PCC PAVEMENT 10"	Ⓓ	PR HOT-MIX ASPHALT SURFACE REMOVAL, 7" AVG.
⑤	EX PCC BASE COURSE 10"	Ⓔ	PR HOT-MIX ASPHALT SURFACE REMOVAL, 6 1/4" AVG.
⑥	EX HMA BASE COURSE 10 1/4"	Ⓕ	PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, 1L-19.0, N90 4 3/4"
⑦	EX BIT SURFACE COURSE (THICK VARIES)	Ⓖ	PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, 1L-19.0, N90 2 1/4"
⑧	EX BITUMINOUS SHOULDERS (THICK VARIES)	Ⓗ	PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, 1L-19.0, N90 5 1/2"
⑨	EX PIPE UNDERDRAINS 4"	Ⓘ	PR POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N90 1 1/2"
⑩	EX PIPE UNDERDRAINS 6"	Ⓝ	PR HOT-MIX ASPHALT SHOULDERS, 2"
⑪	EX BITUMINOUS CONCRETE PAVEMENT 18"	Ⓚ	PR AGGREGATE WEDGE SHOULDER, TYPE B
⑫	EX PCC BASE COURSE 9"	Ⓛ	PR PAVEMENT MARKING
⑬	EX PCC PAVEMENT 8" STD REINFORCEMENT		
⑭	EX PCC PAVEMENT 10" STD REINFORCEMENT		
⑮	EX HOT-MIX ASPHALT SHOULDERS, 4"		
⑯	EX CABLE MEDIAN BARRIER		

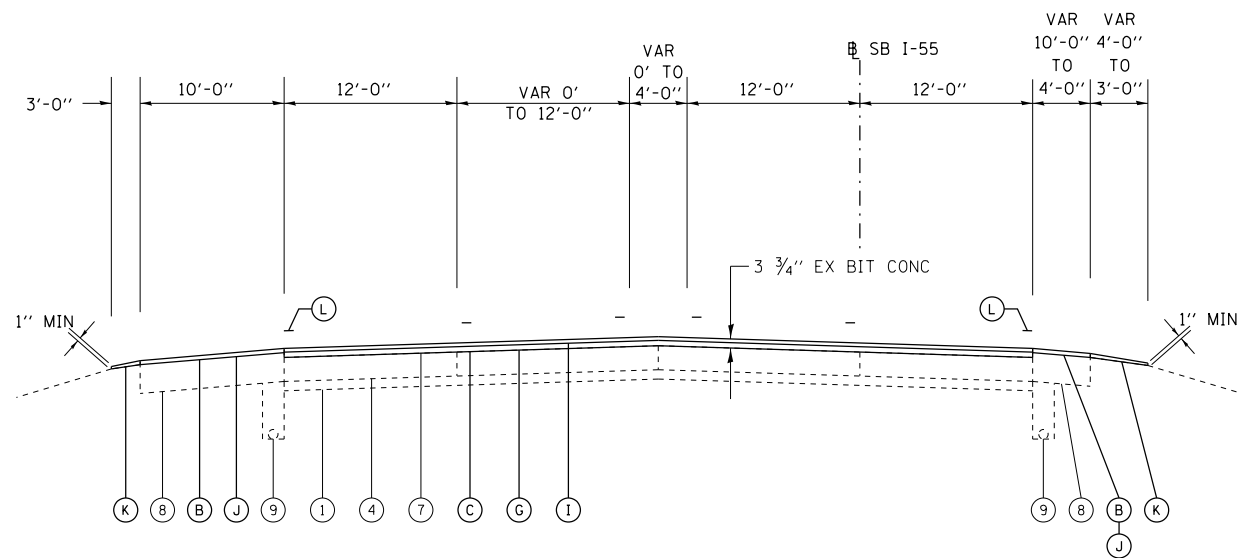
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

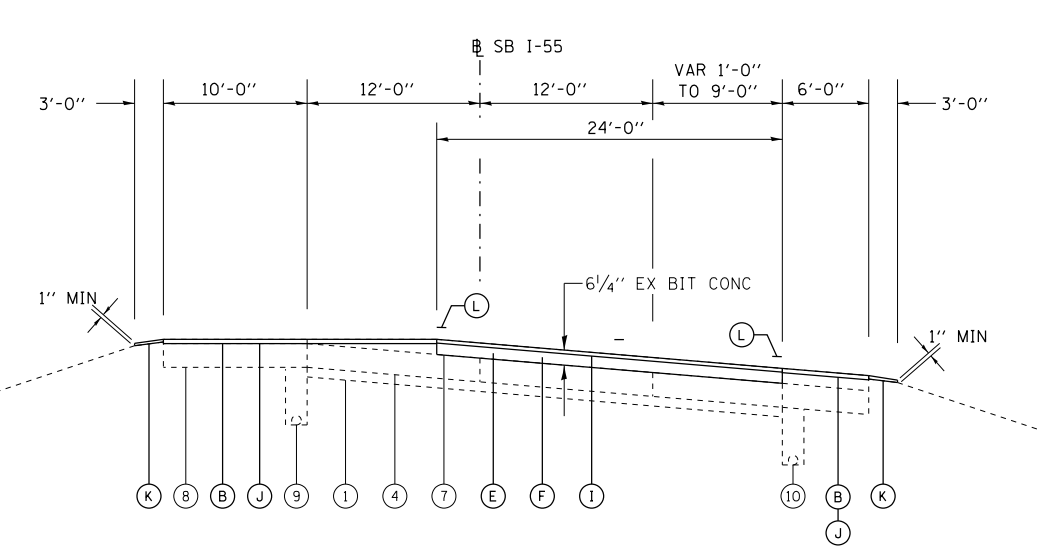
TYPICAL SECTIONS

SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
							**	SANGAMON	85	25
CONTRACT NO. 72J83										
ILLINOIS FED. AID PROJECT										

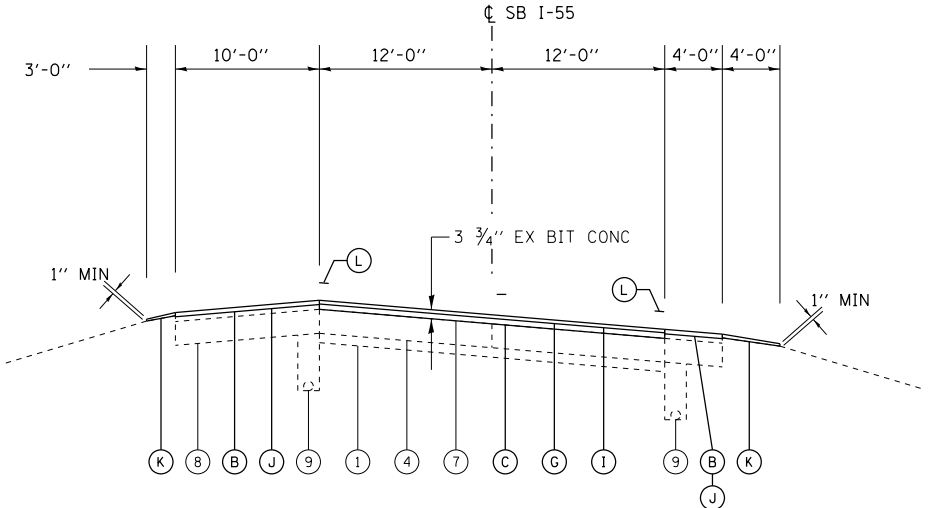
*F.A.I. 55, F.A.I. 72, F.A.P. 666 *(84-4, 84-3, 84-9, 110) RS



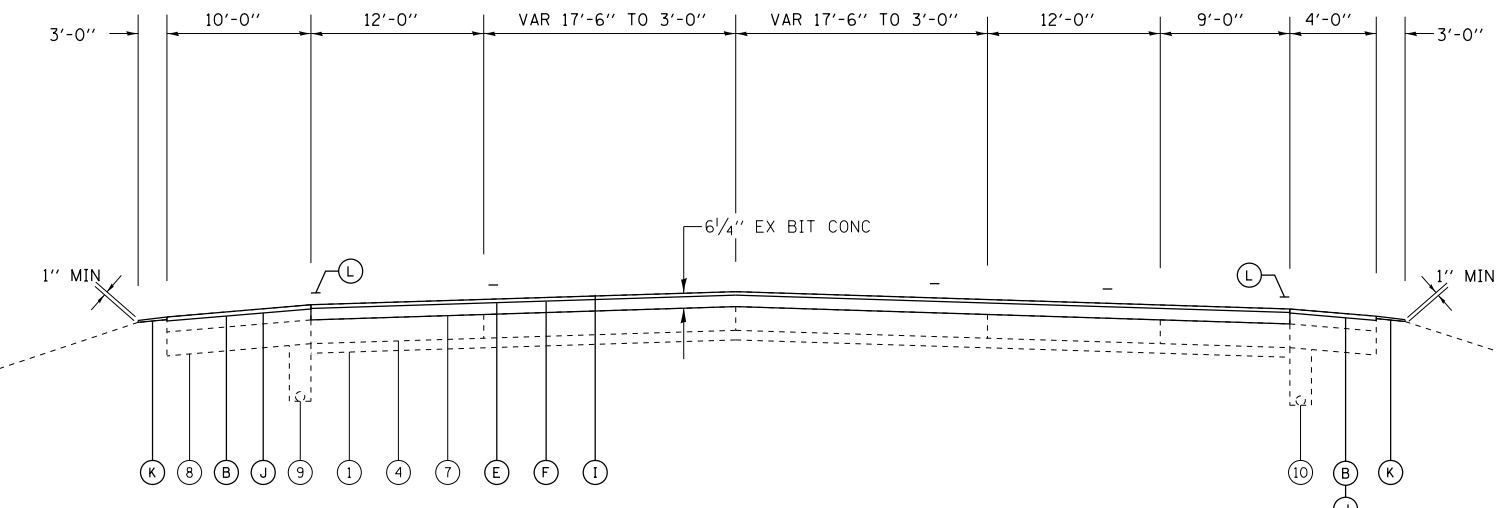
TYPICAL #4
SOUTHBOUND FAI 55
STA. 13+10.43 TO STA. 31+21.67



TYPICAL #6
SOUTHBOUND FAI 55
STA. 50+00.00 TO STA. 64+32.03 * 5" UNDER SN 084-0078
SEE DETAIL



TYPICAL #5
SOUTHBOUND FAI 55
STA. 31+21.67 TO STA. 50+00.00



TYPICAL #7
SOUTHBOUND FAI 55
STA. 64+32.03 TO STA. 74+87.00

LEGEND

① EX STABILIZED SUB-BASE 4"	Ⓐ PR HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
② EX AGGREGATE SUB-BASE 4"	Ⓑ PR HOT-MIX ASPHALT SURFACE REMOVAL, 2"
③ EX SUB-BASE GRANULAR MATERIAL 12"	Ⓒ PR HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4" AVG.
④ EX PCC PAVEMENT 10"	Ⓓ PR HOT-MIX ASPHALT SURFACE REMOVAL, 7" AVG.
⑤ EX PCC BASE COURSE 10"	Ⓔ PR HOT-MIX ASPHALT SURFACE REMOVAL, 6 1/4" AVG.
⑥ EX HMA BASE COURSE 10 1/4"	Ⓕ PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 4 3/4"
⑦ EX BIT SURFACE COURSE (THICK VARIES)	Ⓖ PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 2 1/4"
⑧ EX BITUMINOUS SHOULDERS (THICK VARIES)	Ⓗ PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 5 1/2"
⑨ EX PIPE UNDERDRAINS 4"	Ⓘ PR POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N90 1 1/2"
⑩ EX PIPE UNDERDRAINS 6"	Ⓝ PR HOT-MIX ASPHALT SHOULDERS, 2"
⑪ EX BITUMINOUS CONCRETE PAVEMENT 18"	Ⓚ PR AGGREGATE WEDGE SHOULDER, TYPE B
⑫ EX PCC BASE COURSE 9"	Ⓛ PR PAVEMENT MARKING
⑬ EX PCC PAVEMENT 8" STD REINFORCEMENT	
⑭ EX PCC PAVEMENT 10" STD REINFORCEMENT	
⑮ EX HOT-MIX ASPHALT SHOULDERS, 4"	
⑯ EX CABLE MEDIAN BARRIER	

PAVED SHOULDER SLOPE
4.0% CROSS SLOPE IS DESIRABLE ON NORMAL CROWN SECTIONS

ON THE HIGH SIDE OF SUPERELEVATED CURVES, SLOPE THE SHOULDER SO THAT THE ALGEBRETIC DIFFERENCE BETWEEN THE PAVEMENT AND SHOULDERS ARE NOT GREATER THAN 8%

ON THE LOW SIDE OF SUPERELEVATED CURVES, SLOPE THE SHOULDER 4% UNLESS THE SUPER ELEVATED SECTION IS GREATER THAN 4% THEN MATCH SUPERELEVATED SLOPE

PAVEMENT SLOPE
CROSS SLOPE IN CROWN SECTIONS: SHALL BE 1.5%

SUPER ELEVATED SECTIONS: SEE SUPER ELEVATION DETAILS FOR MORE INFORMATION

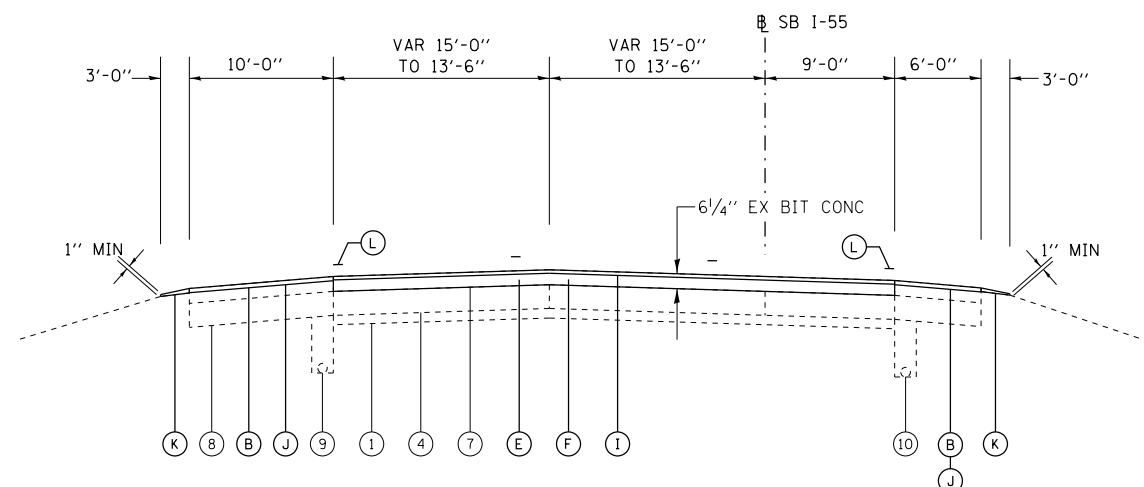
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

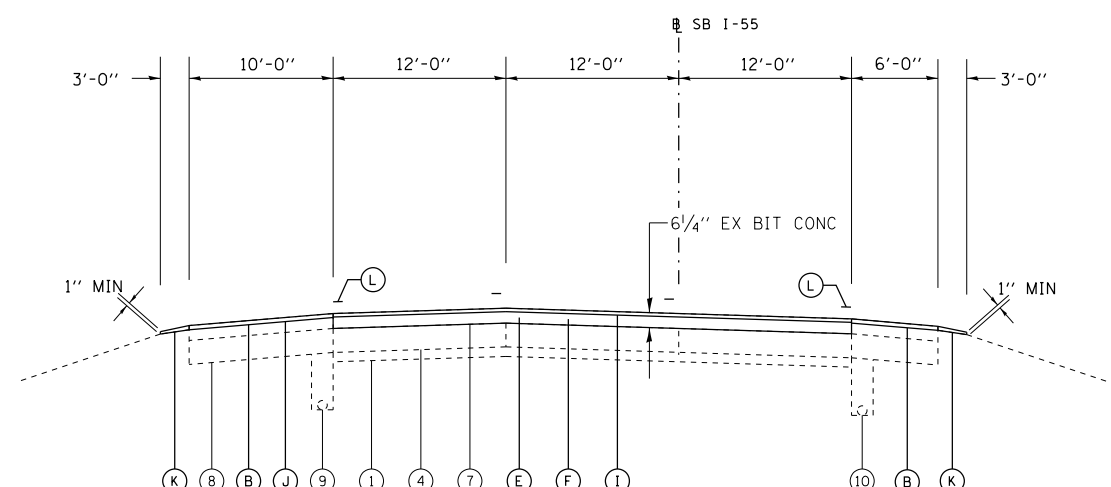
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SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	F.A. * RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
							**	SANGAMON	85	26
CONTRACT NO. 72J83										
ILLINOIS FED. AID PROJECT										

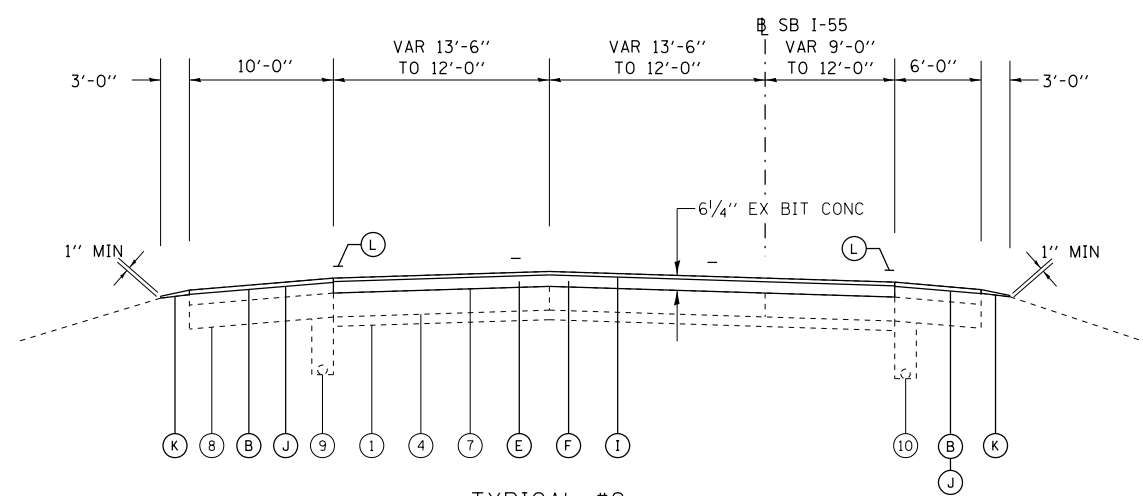
*F.A.I. 55, F.A.I. 72, F.A.P. 666 ** (84-4, 84-3, 84-9, 110) RS



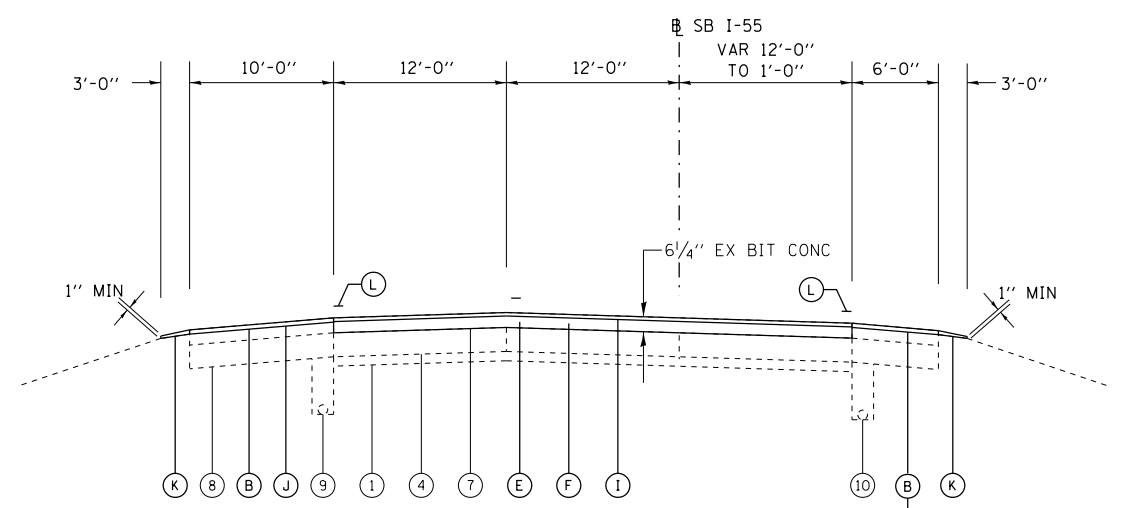
TYPICAL #8
SOUTHBOUND FAI 55
STA. 74+87.00 TO STA. 77+88.00



TYPICAL #10
SOUTHBOUND FAI 55
STA. 80+88.00 TO STA. 83+88.00



TYPICAL #9
SOUTHBOUND FAI 55
STA. 77+88.00 TO STA. 80+88.00



TYPICAL #11
SOUTHBOUND FAI 55
STA. 83+88.00 TO STA. 86+63.86
STATION EQUATION: STA. 86+63.86 BK = STA. 82+99.33 AH
STA. 82+99.33 TO STA. 90+23.61

LEGEND

① EX STABILIZED SUB-BASE 4"	Ⓐ PR HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
② EX AGGREGATE SUB-BASE 4"	Ⓑ PR HOT-MIX ASPHALT SURFACE REMOVAL, 2"
③ EX SUB-BASE GRANULAR MATERIAL 12"	Ⓒ PR HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4" AVG.
④ EX PCC PAVEMENT 10"	Ⓓ PR HOT-MIX ASPHALT SURFACE REMOVAL, 7" AVG.
⑤ EX PCC BASE COURSE 10"	Ⓔ PR HOT-MIX ASPHALT SURFACE REMOVAL, 6 1/4" AVG.
⑥ EX HMA BASE COURSE 10 1/4"	Ⓕ PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 4 3/4"
⑦ EX BIT SURFACE COURSE (THICK VARIES)	Ⓖ PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 2 1/4"
⑧ EX BITUMINOUS SHOULDERS (THICK VARIES)	Ⓗ PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 5 1/2"
⑨ EX PIPE UNDERDRAINS 4"	Ⓘ PR POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N90 1 1/2"
⑩ EX PIPE UNDERDRAINS 6"	Ⓝ PR HOT-MIX ASPHALT SHOULDERS, 2"
⑪ EX BITUMINOUS CONCRETE PAVEMENT 18"	Ⓚ PR AGGREGATE WEDGE SHOULDER, TYPE B
⑫ EX PCC BASE COURSE 9"	Ⓛ PR PAVEMENT MARKING
⑬ EX PCC PAVEMENT 8" STD REINFORCEMENT	
⑭ EX PCC PAVEMENT 10" STD REINFORCEMENT	
⑮ EX HOT-MIX ASPHALT SHOULDERS, 4"	
⑯ EX CABLE MEDIAN BARRIER	

PAVED SHOULDER SLOPE
4.0% CROSS SLOPE IS DESIRABLE ON NORMAL CROWN SECTIONS

ON THE HIGH SIDE OF SUPERELEVATED CURVES, SLOPE THE SHOULDER SO THAT THE ALGEBRETIC DIFFERENCE BETWEEN THE PAVEMENT AND SHOULDERS ARE NOT GREATER THAN 8%

ON THE LOW SIDE OF SUPERELEVATED CURVES, SLOPE THE SHOULDER 4% UNLESS THE SUPER ELEVATED SECTION IS GREATER THAN 4% THEN MATCH SUPERELEVATED SLOPE

PAVEMENT SLOPE
CROSS SLOPE IN CROWN SECTIONS: SHALL BE 1.5%

SUPER ELEVATED SECTIONS: SEE SUPER ELEVATION DETAILS FOR MORE INFORMATION

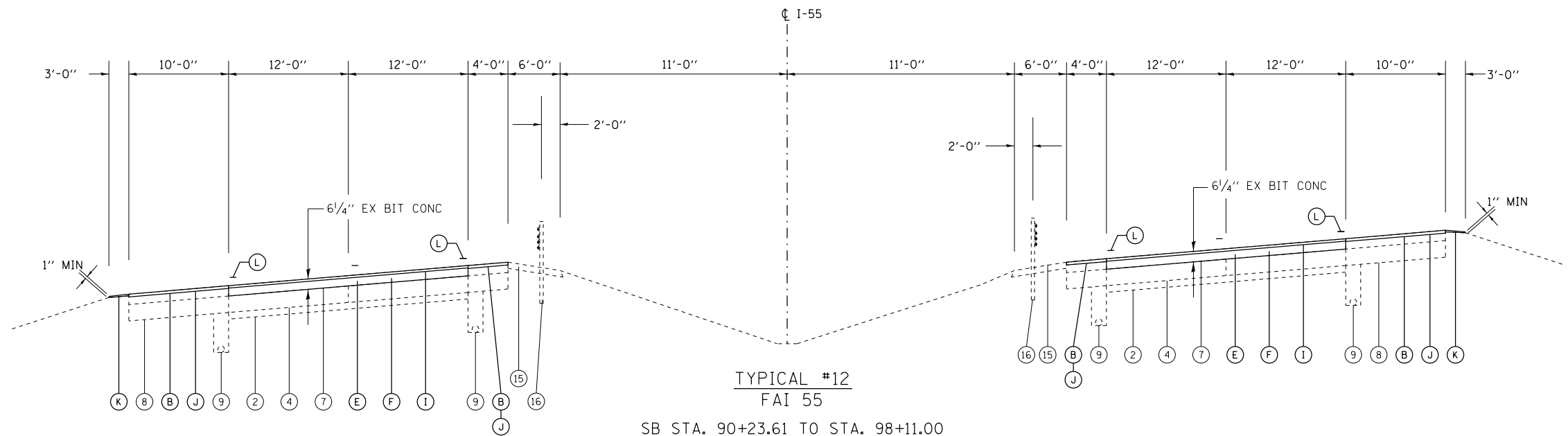
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

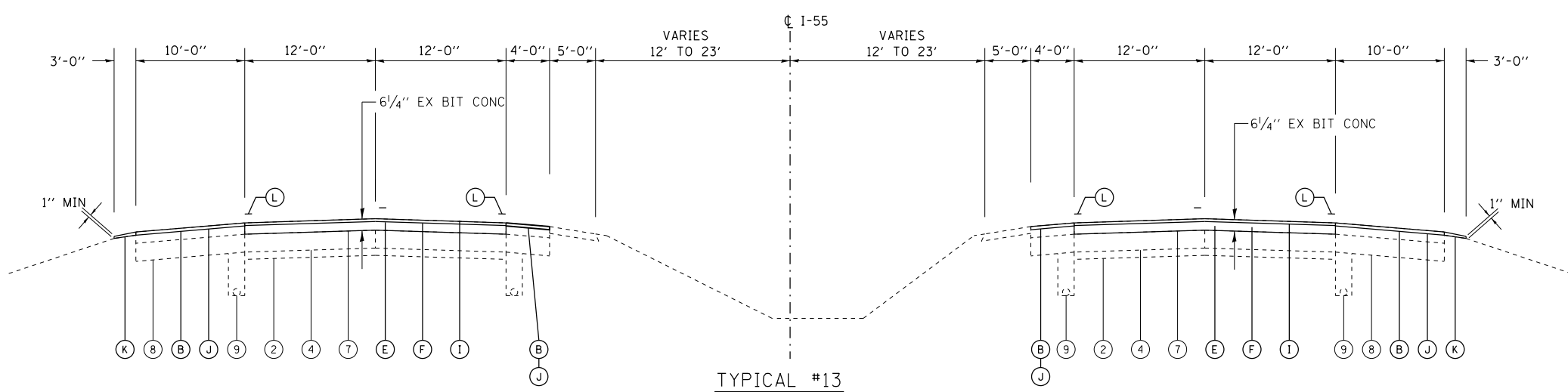
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	PLOT DATE = 10/20/2017		

SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	F.A. * RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
							**	SANGAMON	85	27
CONTRACT NO. 72J83										
ILLINOIS FED. AID PROJECT										

*F.A.I. 55, F.A.I. 72, F.A.P. 666 ** (84-4, 84-3, 84-9, 110) RS



TYPICAL #12
FAI 55
SB STA. 90+23.61 TO STA. 98+11.00
STA. 98+11.00 TO STA. 111+00.00
SB STA. 146+49.00 TO STA. 148+44.42
NB STA. 147+55.00 TO STA. 148+44.42



TYPICAL #13
FAI 55
STA. 111+00.00 TO STA. 117+43.62

LEGEND

① EX STABILIZED SUB-BASE 4"	Ⓐ PR HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
② EX AGGREGATE SUB-BASE 4"	Ⓑ PR HOT-MIX ASPHALT SURFACE REMOVAL, 2"
③ EX SUB-BASE GRANULAR MATERIAL 12"	Ⓒ PR HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4" AVG.
④ EX PCC PAVEMENT 10"	Ⓓ PR HOT-MIX ASPHALT SURFACE REMOVAL, 7" AVG.
⑤ EX PCC BASE COURSE 10"	Ⓔ PR HOT-MIX ASPHALT SURFACE REMOVAL, 6 1/4" AVG.
⑥ EX HMA BASE COURSE 10 1/4"	Ⓕ PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 4 3/4"
⑦ EX BIT SURFACE COURSE (THICK VARIES)	Ⓖ PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 2 1/4"
⑧ EX BITUMINOUS SHOULDERS (THICK VARIES)	Ⓗ PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 5 1/2"
⑨ EX PIPE UNDERDRAINS 4"	Ⓘ PR POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N90 1 1/2"
⑩ EX PIPE UNDERDRAINS 6"	Ⓝ PR HOT-MIX ASPHALT SHOULDERS, 2"
⑪ EX BITUMINOUS CONCRETE PAVEMENT 18"	Ⓚ PR AGGREGATE WEDGE SHOULDER, TYPE B
⑫ EX PCC BASE COURSE 9"	Ⓛ PR PAVEMENT MARKING
⑬ EX PCC PAVEMENT 8" STD REINFORCEMENT	
⑭ EX PCC PAVEMENT 10" STD REINFORCEMENT	
⑮ EX HOT-MIX ASPHALT SHOULDERS, 4"	
⑯ EX CABLE MEDIAN BARRIER	

PAVED SHOULDER SLOPE
4.0% CROSS SLOPE IS DESIRABLE ON NORMAL CROWN SECTIONS

ON THE HIGH SIDE OF SUPERELEVATED CURVES, SLOPE THE SHOULDER SO THAT THE ALGEBRETIC DIFFERENCE BETWEEN THE PAVEMENT AND SHOULDERS ARE NOT GREATER THAN 8%

ON THE LOW SIDE OF SUPERELEVATED CURVES, SLOPE THE SHOULDER 4% UNLESS THE SUPER ELEVATED SECTION IS GREATER THAN 4% THEN MATCH SUPERELEVATED SLOPE

PAVEMENT SLOPE
CROSS SLOPE IN CROWN SECTIONS: SHALL BE 1.5%

SUPER ELEVATED SECTIONS: SEE SUPER ELEVATION DETAILS FOR MORE INFORMATION

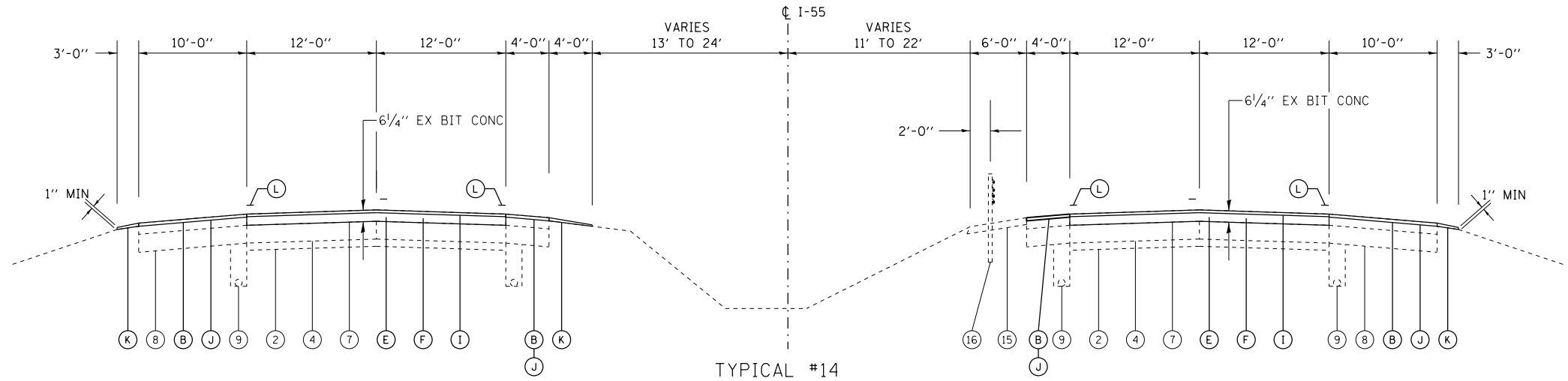
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**	SANGAMON	85	28
CONTRACT NO. 72J83				
ILLINOIS FED. AID PROJECT				

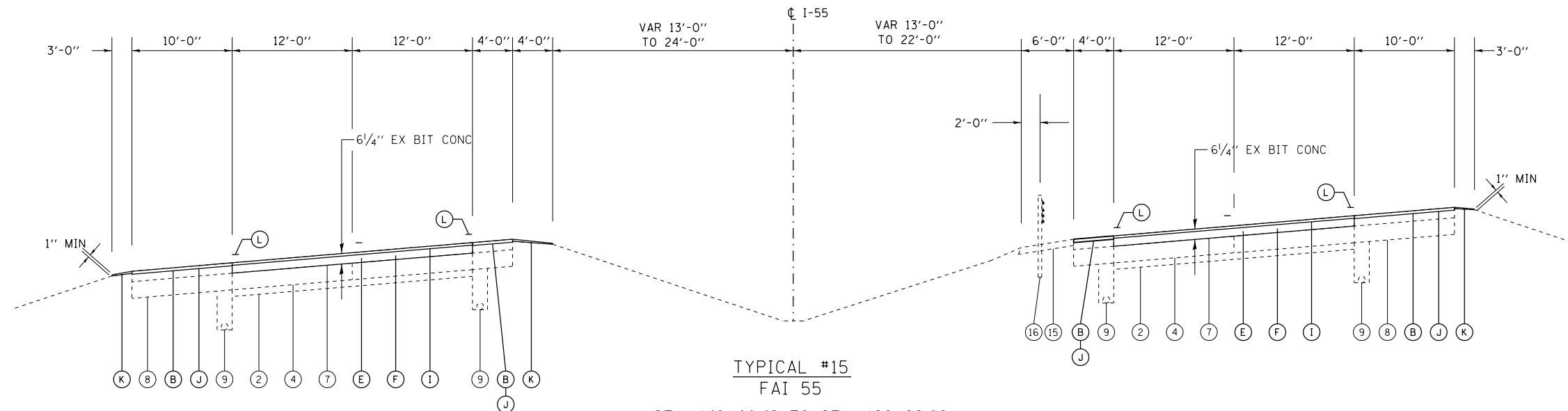
*F.A.I. 55, F.A.I. 72, F.A.P. 666 ** (84-4, 84-3, 84-9, 110) RS



SB STA. 117+43.62 TO STA. 128+87.38
 STATION EQUATION: STA. 128+87.38 BK = STA. 128+91.29 AH
 SB STA. 128+91.29 TO STA. 146+49.00

NB STA. 117+43.62 TO STA. 128+87.38
 STATION EQUATION: STA. 128+87.38 BK = STA. 128+91.29 AH
 NB STA. 128+91.29 TO STA. 147+55.00

TYPICAL #14
FAI 55



TYPICAL #15
FAI 55
STA. 148+44.42 TO STA. 160+69.89

LEGEND

① EX STABILIZED SUB-BASE 4"	Ⓐ PR HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
② EX AGGREGATE SUB-BASE 4"	Ⓑ PR HOT-MIX ASPHALT SURFACE REMOVAL, 2"
③ EX SUB-BASE GRANULAR MATERIAL 12"	Ⓒ PR HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4" AVG.
④ EX PCC PAVEMENT 10"	Ⓓ PR HOT-MIX ASPHALT SURFACE REMOVAL, 7" AVG.
⑤ EX PCC BASE COURSE 10"	Ⓔ PR HOT-MIX ASPHALT SURFACE REMOVAL, 6 1/4" AVG.
⑥ EX HMA BASE COURSE 10 1/4"	Ⓕ PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 4 3/4"
⑦ EX BIT SURFACE COURSE (THICK VARIES)	Ⓖ PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 2 1/4"
⑧ EX BITUMINOUS SHOULDERS (THICK VARIES)	Ⓗ PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 5 1/2"
⑨ EX PIPE UNDERDRAINS 4"	Ⓘ PR POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N90 1 1/2"
⑩ EX PIPE UNDERDRAINS 6"	Ⓝ PR HOT-MIX ASPHALT SHOULDERS, 2"
⑪ EX BITUMINOUS CONCRETE PAVEMENT 18"	Ⓚ PR AGGREGATE WEDGE SHOULDER, TYPE B
⑫ EX PCC BASE COURSE 9"	Ⓛ PR PAVEMENT MARKING
⑬ EX PCC PAVEMENT 8" STD REINFORCEMENT	
⑭ EX PCC PAVEMENT 10" STD REINFORCEMENT	
⑮ EX HOT-MIX ASPHALT SHOULDERS, 4"	
⑯ EX CABLE MEDIAN BARRIER	

PAVED SHOULDER SLOPE
 4.0% CROSS SLOPE IS DESIRABLE ON NORMAL CROWN SECTIONS

ON THE HIGH SIDE OF SUPERELEVATED CURVES, SLOPE THE SHOULDER SO THAT THE ALGEBRETIC DIFFERENCE BETWEEN THE PAVEMENT AND SHOULDERS ARE NOT GREATER THAN 8%

ON THE LOW SIDE OF SUPERELEVATED CURVES, SLOPE THE SHOULDER 4% UNLESS THE SUPER ELEVATED SECTION IS GREATER THAN 4% THEN MATCH SUPERELEVATED SLOPE

PAVEMENT SLOPE
 CROSS SLOPE IN CROWN SECTIONS: SHALL BE 1.5%

SUPER ELEVATED SECTIONS: SEE SUPER ELEVATION DETAILS FOR MORE INFORMATION

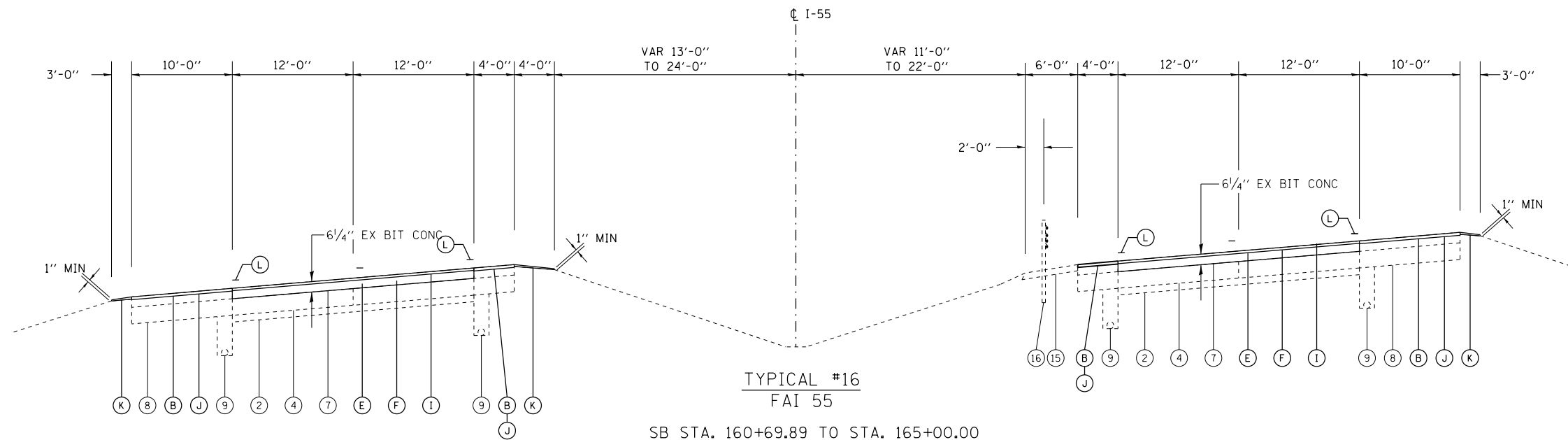
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

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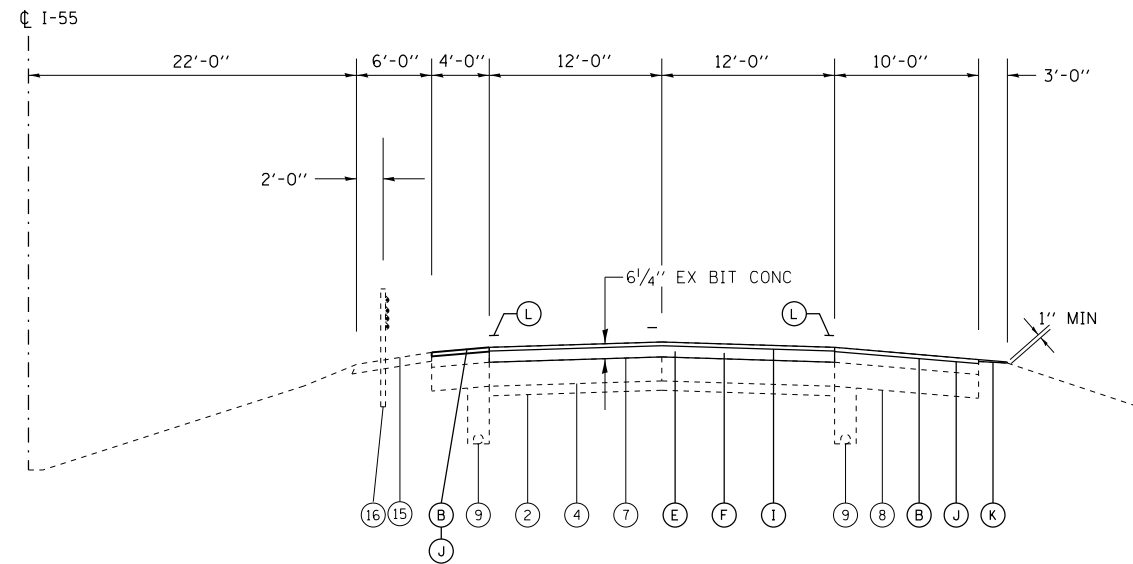
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						**		SANGAMON	85	29
						CONTRACT NO. 72J83				
						ILLINOIS FED. AID PROJECT				

*F.A.I. 55, F.A.I. 72, F.A.P. 666 **84-4, 84-3, 84-9, 110) RS



TYPICAL #16
FAI 55

SB STA. 160+69.89 TO STA. 165+00.00
NB STA. 160+69.89 TO STA. 173+71.00



TYPICAL #17
FAI 55

NB STA. 173+71.00 TO STA. 195+10.00

LEGEND

① EX STABILIZED SUB-BASE 4"	Ⓐ PR HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
② EX AGGREGATE SUB-BASE 4"	Ⓑ PR HOT-MIX ASPHALT SURFACE REMOVAL, 2"
③ EX SUB-BASE GRANULAR MATERIAL 12"	Ⓒ PR HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4" AVG.
④ EX PCC PAVEMENT 10"	Ⓓ PR HOT-MIX ASPHALT SURFACE REMOVAL, 7" AVG.
⑤ EX PCC BASE COURSE 10"	Ⓔ PR HOT-MIX ASPHALT SURFACE REMOVAL, 6 1/4" AVG.
⑥ EX HMA BASE COURSE 10 1/4"	Ⓕ PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 4 3/4"
⑦ EX BIT SURFACE COURSE (THICK VARIES)	Ⓖ PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 2 1/4"
⑧ EX BITUMINOUS SHOULDERS (THICK VARIES)	Ⓗ PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 5 1/2"
⑨ EX PIPE UNDERDRAINS 4"	Ⓘ PR POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N90 1 1/2"
⑩ EX PIPE UNDERDRAINS 6"	Ⓝ PR HOT-MIX ASPHALT SHOULDERS, 2"
⑪ EX BITUMINOUS CONCRETE PAVEMENT 18"	Ⓚ PR AGGREGATE WEDGE SHOULDER, TYPE B
⑫ EX PCC BASE COURSE 9"	Ⓛ PR PAVEMENT MARKING
⑬ EX PCC PAVEMENT 8" STD REINFORCEMENT	
⑭ EX PCC PAVEMENT 10" STD REINFORCEMENT	
⑮ EX HOT-MIX ASPHALT SHOULDERS, 4"	
⑯ EX CABLE MEDIAN BARRIER	

PAVED SHOULDER SLOPE
4.0% CROSS SLOPE IS DESIRABLE ON NORMAL CROWN SECTIONS

ON THE HIGH SIDE OF SUPERELEVATED CURVES, SLOPE THE SHOULDER SO THAT THE ALGEBRETIC DIFFERENCE BETWEEN THE PAVEMENT AND SHOULDERS ARE NOT GREATER THAN 8%

ON THE LOW SIDE OF SUPERELEVATED CURVES, SLOPE THE SHOULDER 4% UNLESS THE SUPER ELEVATED SECTION IS GREATER THAN 4% THEN MATCH SUPERELEVATED SLOPE

PAVEMENT SLOPE
CROSS SLOPE IN CROWN SECTIONS: SHALL BE 1.5%

SUPER ELEVATED SECTIONS: SEE SUPER ELEVATION DETAILS FOR MORE INFORMATION

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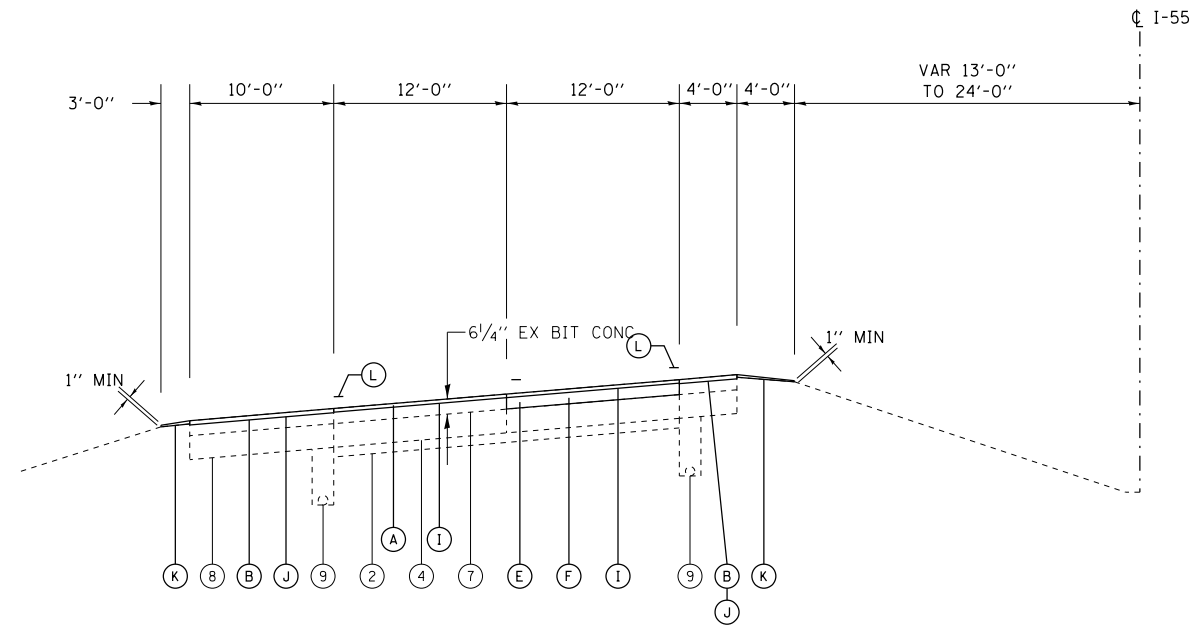
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

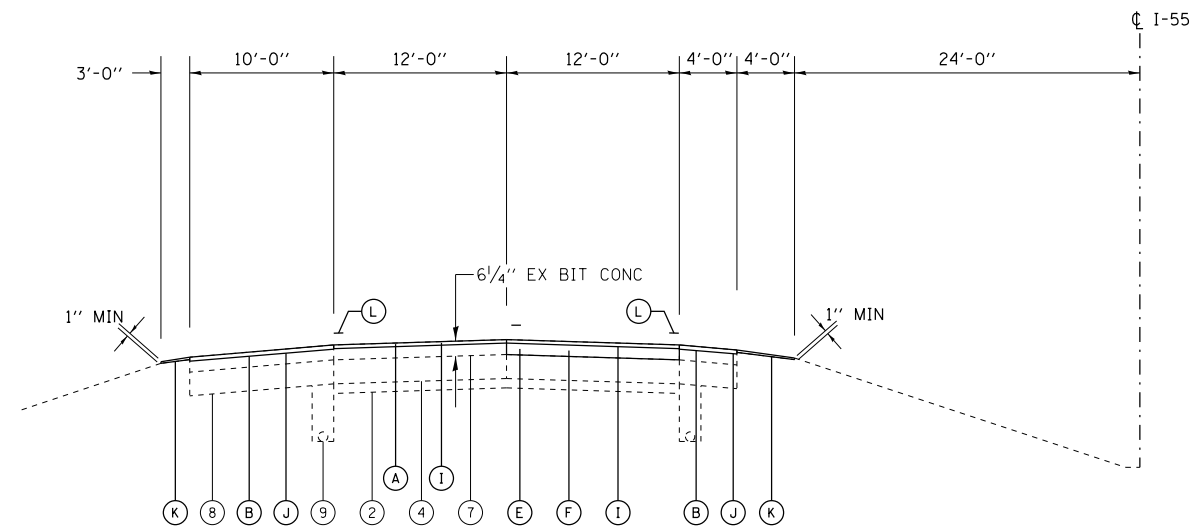
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**	SANGAMON	85	30
CONTRACT NO. 72J83				
ILLINOIS FED. AID PROJECT				

*F.A.I. 55, F.A.I. 72, F.A.P. 666 ** (84-4, 84-3, 84-9, 110) RS



TYPICAL #18
FAI 55
SB STA. 165+00.00 TO STA. 174+36.00



TYPICAL #19
FAI 55
SB STA 174+36.00 TO STA 195+10.00

PAVED SHOULDER SLOPE
4.0% CROSS SLOPE IS DESIRABLE ON NORMAL CROWN SECTIONS

ON THE HIGH SIDE OF SUPERELEVATED CURVES, SLOPE THE SHOULDER SO THAT THE ALGEBRETIC DIFFERENCE BETWEEN THE PAVEMENT AND SHOULDERS ARE NOT GREATER THAN 8%.

ON THE LOW SIDE OF SUPERELEVATED CURVES, SLOPE THE SHOULDER 4% UNLESS THE SUPER ELEVATED SECTION IS GREATER THAN 4% THEN MATCH SUPERELEVATED SLOPE

PAVEMENT SLOPE
CROSS SLOPE IN CROWN SECTIONS: SHALL BE 1.5%
SUPER ELEVATED SECTIONS: SEE SUPER ELEVATION DETAILS FOR MORE INFORMATION

LEGEND

①	EX STABILIZED SUB-BASE 4"	Ⓐ	PR HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
②	EX AGGREGATE SUB-BASE 4"	Ⓑ	PR HOT-MIX ASPHALT SURFACE REMOVAL, 2"
③	EX SUB-BASE GRANULAR MATERIAL 12"	Ⓒ	PR HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4" AVG.
④	EX PCC PAVEMENT 10"	Ⓓ	PR HOT-MIX ASPHALT SURFACE REMOVAL, 7" AVG.
⑤	EX PCC BASE COURSE 10"	Ⓔ	PR HOT-MIX ASPHALT SURFACE REMOVAL, 6 1/4" AVG.
⑥	EX HMA BASE COURSE 10 1/4"	Ⓕ	PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 4 3/4"
⑦	EX BIT SURFACE COURSE (THICK VARIES)	Ⓖ	PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 2 1/4"
⑧	EX BITUMINOUS SHOULDERS (THICK VARIES)	Ⓗ	PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 5 1/2"
⑨	EX PIPE UNDERDRAINS 4"	Ⓘ	PR POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N90 1 1/2"
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⑪	EX BITUMINOUS CONCRETE PAVEMENT 18"	Ⓚ	PR AGGREGATE WEDGE SHOULDER, TYPE B
⑫	EX PCC BASE COURSE 9"	Ⓛ	PR PAVEMENT MARKING
⑬	EX PCC PAVEMENT 8" STD REINFORCEMENT		
⑭	EX PCC PAVEMENT 10" STD REINFORCEMENT		
⑮	EX HOT-MIX ASPHALT SHOULDERS, 4"		
⑯	EX CABLE MEDIAN BARRIER		

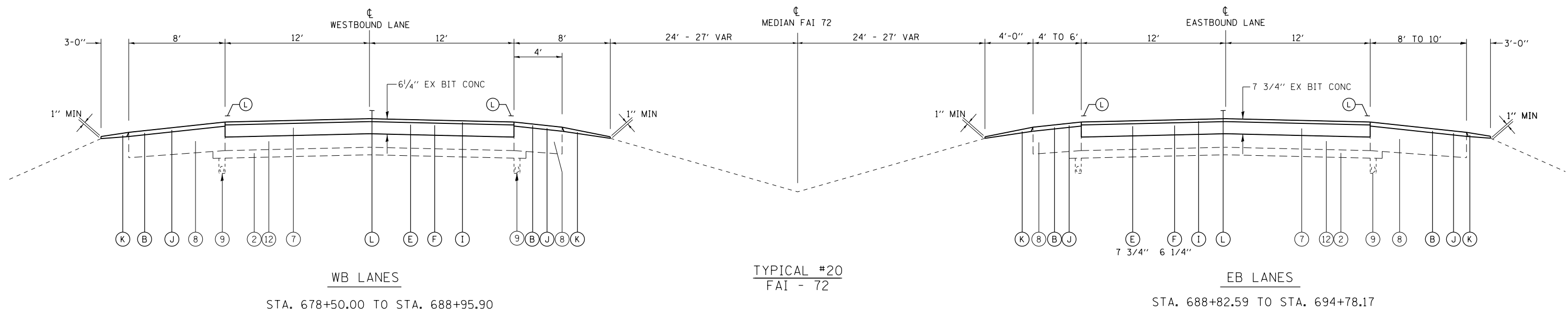
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**	SANGAMON	85	31
CONTRACT NO. 72J83				
ILLINOIS FED. AID PROJECT				



WB LANES
STA. 678+50.00 TO STA. 688+95.90

TYPICAL #20
FAI - 72

EB LANES
STA. 688+82.59 TO STA. 694+78.17

PAVED SHOULDER SLOPE
4.0% CROSS SLOPE IS DESIRABLE ON NORMAL CROWN SECTIONS
ON THE HIGH SIDE OF SUPERELEVATED CURVES, SLOPE THE SHOULDER SO THAT THE ALGEBRETIC DIFFERENCE BETWEEN THE PAVEMENT AND SHOULDERS ARE NOT GREATER THAN 8%
ON THE LOW SIDE OF SUPERELEVATED CURVES, SLOPE THE SHOULDER 4% UNLESS THE SUPER ELEVATED SECTION IS GREATER THAN 4% THEN MATCH SUPERELEVATED SLOPE

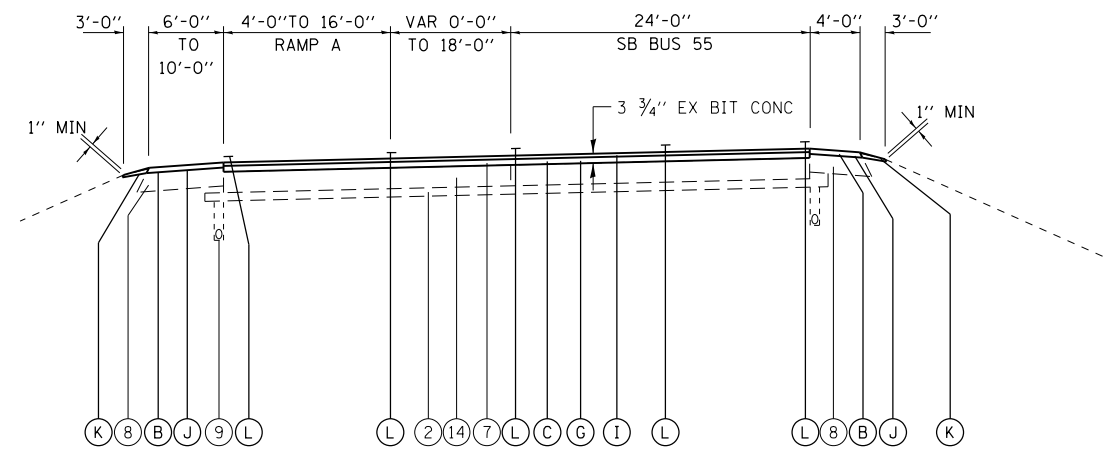
PAVEMENT SLOPE
CROSS SLOPE IN CROWN SECTIONS: SHALL BE 1.5%
SUPER ELEVATED SECTIONS: SEE SUPER ELEVATION DETAILS FOR MORE INFORMATION

LEGEND

- | | |
|--|--|
| ① EX STABILIZED SUB-BASE 4" | Ⓐ PR HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2" |
| ② EX AGGREGATE SUB-BASE 4" | Ⓑ PR HOT-MIX ASPHALT SURFACE REMOVAL, 2" |
| ③ EX SUB-BASE GRANULAR MATERIAL 12" | Ⓒ PR HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4" AVG. |
| ④ EX PCC PAVEMENT 10" | Ⓓ PR HOT-MIX ASPHALT SURFACE REMOVAL, 7" AVG. |
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| ⑥ EX HMA BASE COURSE 10 1/4" | Ⓕ PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, 1L-19.0, N90 4 3/4" |
| ⑦ EX BIT SURFACE COURSE (THICK VARIES) | Ⓖ PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, 1L-19.0, N90 2 1/4" |
| ⑧ EX BITUMINOUS SHOULDERS (THICK VARIES) | Ⓗ PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, 1L-19.0, N90 5 1/2" |
| ⑨ EX PIPE UNDERDRAINS 4" | Ⓘ PR POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N90 1 1/2" |
| ⑩ EX PIPE UNDERDRAINS 6" | Ⓝ PR HOT-MIX ASPHALT SHOULDERS, 2" |
| ⑪ EX BITUMINOUS CONCRETE PAVEMENT 18" | Ⓚ PR AGGREGATE WEDGE SHOULDER, TYPE B |
| ⑫ EX PCC BASE COURSE 9" | Ⓛ PR PAVEMENT MARKING |
| ⑬ EX PCC PAVEMENT 8" STD REINFORCEMENT | |
| ⑭ EX PCC PAVEMENT 10" STD REINFORCEMENT | |
| ⑮ EX HOT-MIX ASPHALT SHOULDERS, 4" | |
| ⑯ EX CABLE MEDIAN BARRIER | |

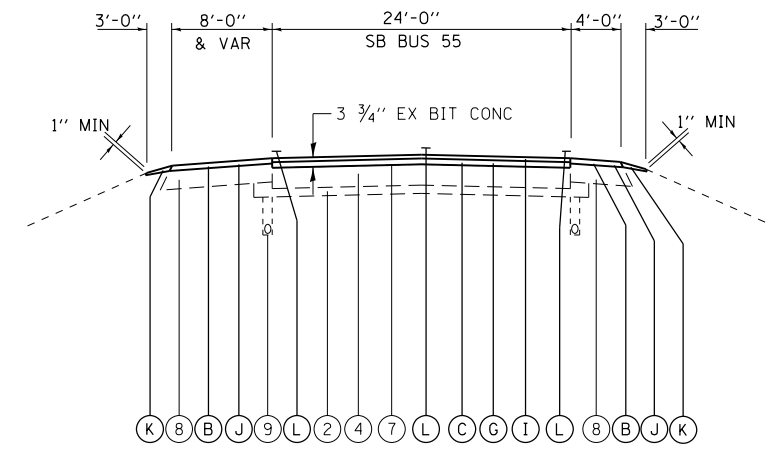
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	PLOT DATE = 10/20/2017							ILLINOIS FED. AID PROJECT				

*F.A.I. 55, F.A.I. 72, F.A.P. 666 ** (84-4, 84-3, 84-9, 110) RS



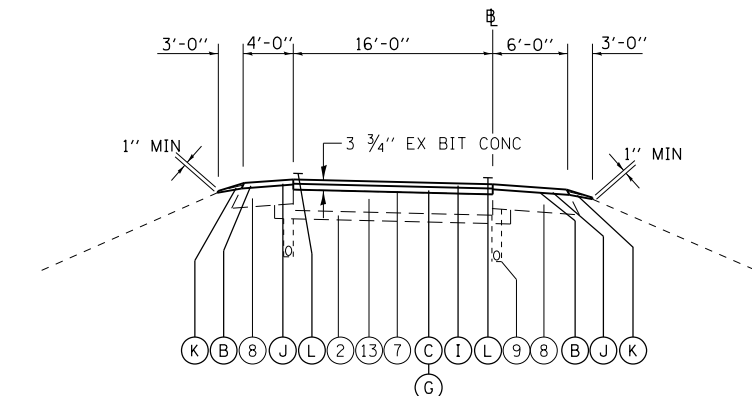
TYPICAL #21
SB BL-55

STA. 31+21.97 TO STA. 41+32.34

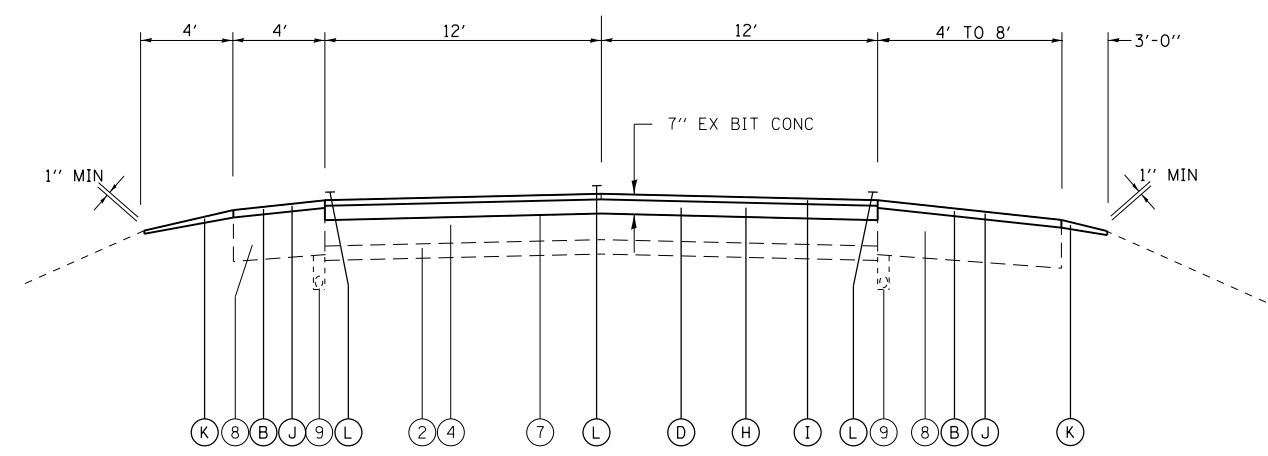


TYPICAL #22
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STA. 41+32.34 TO STA. 44+00.00

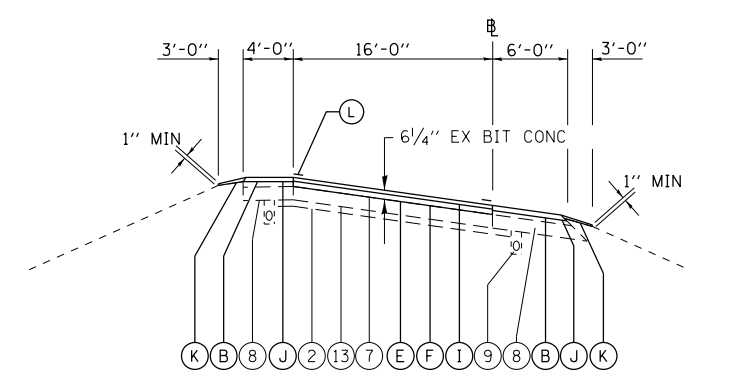


TYPICAL #24
6TH STREET INTERCHANGE
RAMP A STA. 0+00 TO STA. 31+65.18



TYPICAL #23
NB BL-55

STA. 29+73.82 TO STA. 39+47.62



TYPICAL #25
6TH STREET INTERCHANGE
NE RAMP STA. 0+00.00 TO STA. 6+20.00

PAVED SHOULDER SLOPE
4.0% CROSS SLOPE IS DESIRABLE ON NORMAL CROWN SECTIONS

ON THE HIGH SIDE OF SUPERELEVATED CURVES, SLOPE THE SHOULDER SO THAT THE ALGEBRETIC DIFFERENCE BETWEEN THE PAVEMENT AND SHOULDERS ARE NOT GREATER THAN 8%.

ON THE LOW SIDE OF SUPERELEVATED CURVES, SLOPE THE SHOULDER 4% UNLESS THE SUPER ELEVATED SECTION IS GREATER THAN 4% THEN MATCH SUPERELEVATED SLOPE

PAVEMENT SLOPE
CROSS SLOPE IN CROWN SECTIONS: SHALL BE 1.5%

SUPER ELEVATED SECTIONS: SEE SUPER ELEVATION DETAILS FOR MORE INFORMATION

LEGEND

① EX STABILIZED SUB-BASE 4"	Ⓐ PR HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
② EX AGGREGATE SUB-BASE 4"	Ⓑ PR HOT-MIX ASPHALT SURFACE REMOVAL, 2"
③ EX SUB-BASE GRANULAR MATERIAL 12"	Ⓒ PR HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4" AVG.
④ EX PCC PAVEMENT 10"	Ⓓ PR HOT-MIX ASPHALT SURFACE REMOVAL, 7" AVG.
⑤ EX PCC BASE COURSE 10"	Ⓔ PR HOT-MIX ASPHALT SURFACE REMOVAL, 6 1/4" AVG.
⑥ EX HMA BASE COURSE 10 1/4"	Ⓕ PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, 1L-19.0, N90 4 3/4"
⑦ EX BIT SURFACE COURSE (THICK VARIES)	Ⓖ PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, 1L-19.0, N90 2 1/4"
⑧ EX BITUMINOUS SHOULDERS (THICK VARIES)	Ⓗ PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, 1L-19.0, N90 5 1/2"
⑨ EX PIPE UNDERDRAINS 4"	Ⓘ PR POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N90 1 1/2"
⑩ EX PIPE UNDERDRAINS 6"	Ⓝ PR HOT-MIX ASPHALT SHOULDERS, 2"
⑪ EX BITUMINOUS CONCRETE PAVEMENT 18"	Ⓚ PR AGGREGATE WEDGE SHOULDER, TYPE B
⑫ EX PCC BASE COURSE 9"	Ⓛ PR PAVEMENT MARKING
⑬ EX PCC PAVEMENT 8" STD REINFORCEMENT	
⑭ EX PCC PAVEMENT 10" STD REINFORCEMENT	
⑮ EX HOT-MIX ASPHALT SHOULDERS, 4"	
⑯ EX CABLE MEDIAN BARRIER	

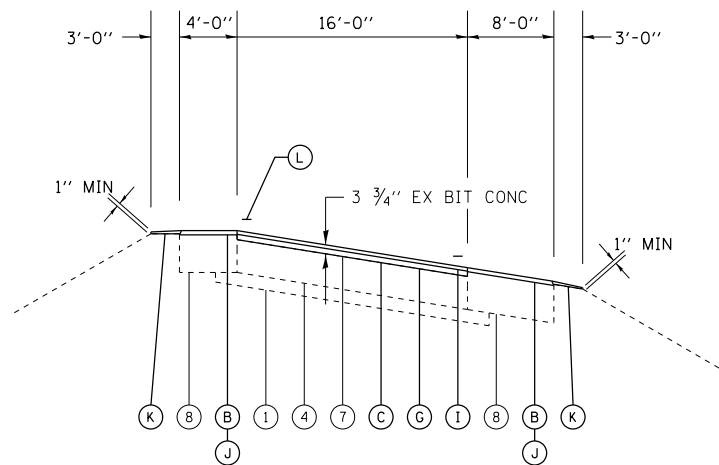
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

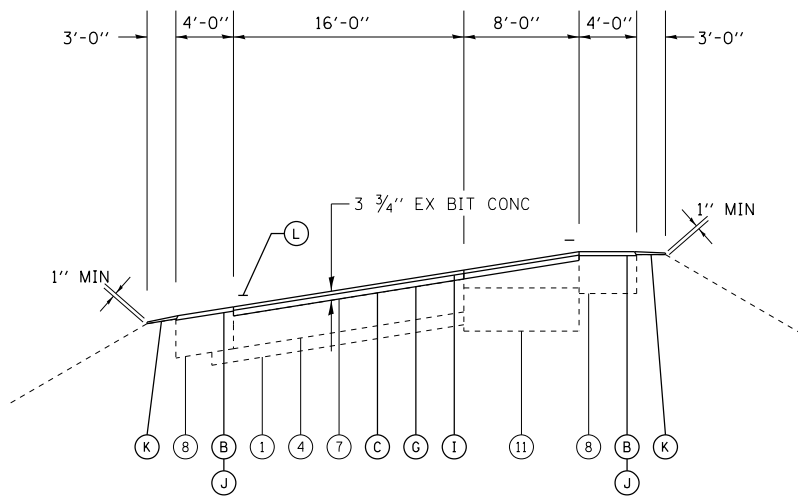
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										ILLINOIS FED. AID PROJECT	

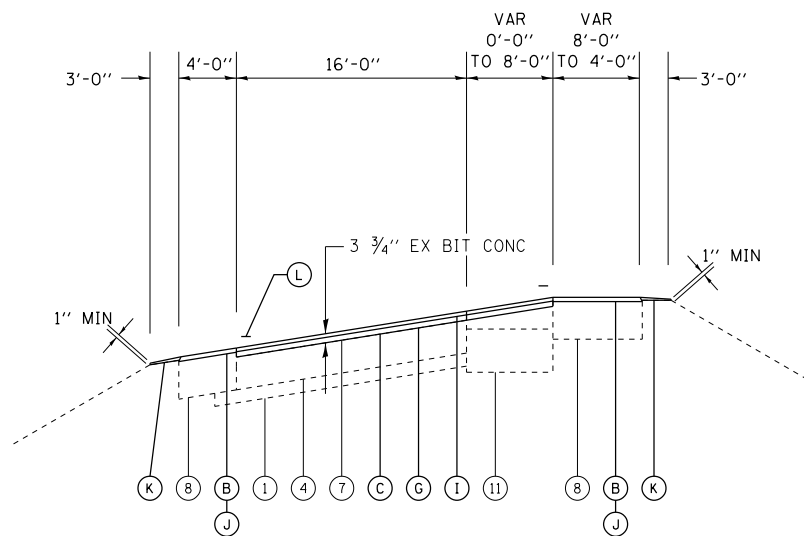
*F.A.I. 55, F.A.I. 72, F.A.P. 666 *(84-4, 84-3, 84-9, 110) RS



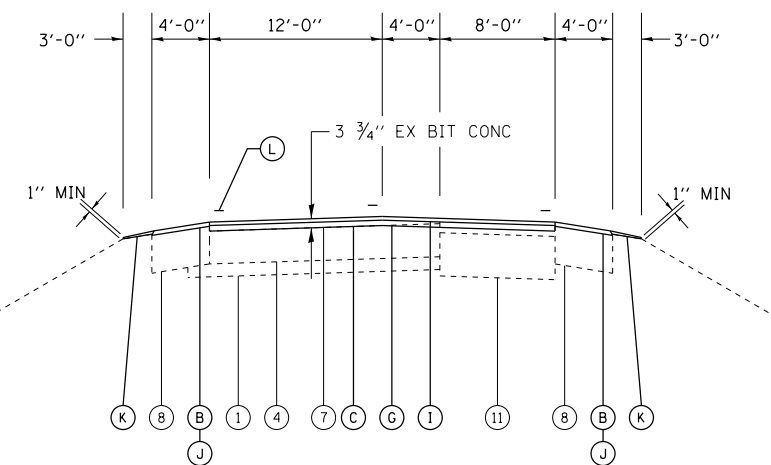
TYPICAL #26
STEVENSON DRIVE RAMPS
RAMP A STA. 0+00 TO STA. 12+29.60



TYPICAL #28
STEVENSON DRIVE RAMPS
RAMP A STA. 13+49.60 TO STA. 14+63.86



TYPICAL #27
STEVENSON DRIVE RAMPS
RAMP A STA. 12+29.60 TO STA. 13+49.60



TYPICAL #29
STEVENSON DRIVE RAMPS
RAMP A STA. 14+63.86 TO STA. 16+82

PAVED SHOULDER SLOPE
4.0% CROSS SLOPE IS DESIRABLE ON NORMAL CROWN SECTIONS

ON THE HIGH SIDE OF SUPERELEVATED CURVES, SLOPE THE SHOULDER SO THAT THE ALGEBRETIC DIFFERENCE BETWEEN THE PAVEMENT AND SHOULDERS ARE NOT GREATER THAN 8%.

ON THE LOW SIDE OF SUPERELEVATED CURVES, SLOPE THE SHOULDER 4% UNLESS THE SUPER ELEVATED SECTION IS GREATER THAN 4% THEN MATCH SUPERELEVATED SLOPE

PAVEMENT SLOPE
CROSS SLOPE IN CROWN SECTIONS: SHALL BE 1.5%

SUPER ELEVATED SECTIONS: SEE SUPER ELEVATION DETAILS FOR MORE INFORMATION

LEGEND

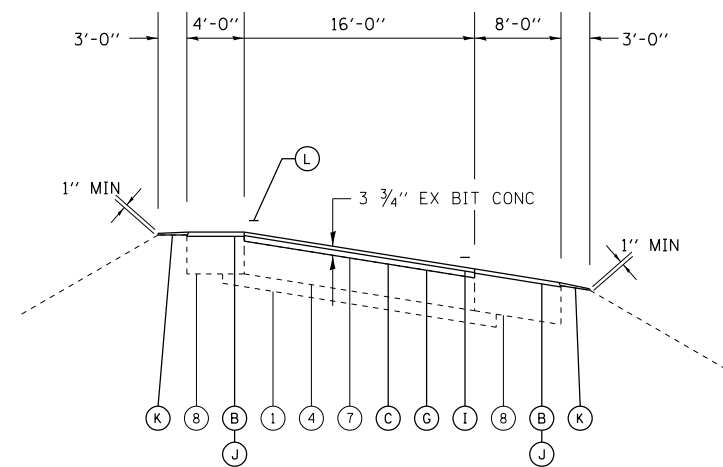
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②	EX AGGREGATE SUB-BASE 4"	Ⓑ	PR HOT-MIX ASPHALT SURFACE REMOVAL, 2"
③	EX SUB-BASE GRANULAR MATERIAL 12"	Ⓒ	PR HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4" AVG.
④	EX PCC PAVEMENT 10"	Ⓓ	PR HOT-MIX ASPHALT SURFACE REMOVAL, 7" AVG.
⑤	EX PCC BASE COURSE 10"	Ⓔ	PR HOT-MIX ASPHALT SURFACE REMOVAL, 6 1/4" AVG.
⑥	EX HMA BASE COURSE 10 1/4"	Ⓕ	PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 4 3/4"
⑦	EX BIT SURFACE COURSE (THICK VARIES)	Ⓖ	PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 2 1/4"
⑧	EX BITUMINOUS SHOULDERS (THICK VARIES)	Ⓗ	PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 5 1/2"
⑨	EX PIPE UNDERDRAINS 4"	Ⓘ	PR POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N90 1 1/2"
⑩	EX PIPE UNDERDRAINS 6"	Ⓝ	PR HOT-MIX ASPHALT SHOULDERS, 2"
⑪	EX BITUMINOUS CONCRETE PAVEMENT 18"	Ⓚ	PR AGGREGATE WEDGE SHOULDER, TYPE B
⑫	EX PCC BASE COURSE 9"	Ⓛ	PR PAVEMENT MARKING
⑬	EX PCC PAVEMENT 8" STD REINFORCEMENT		
⑭	EX PCC PAVEMENT 10" STD REINFORCEMENT		
⑮	EX HOT-MIX ASPHALT SHOULDERS, 4"		
⑯	EX CABLE MEDIAN BARRIER		

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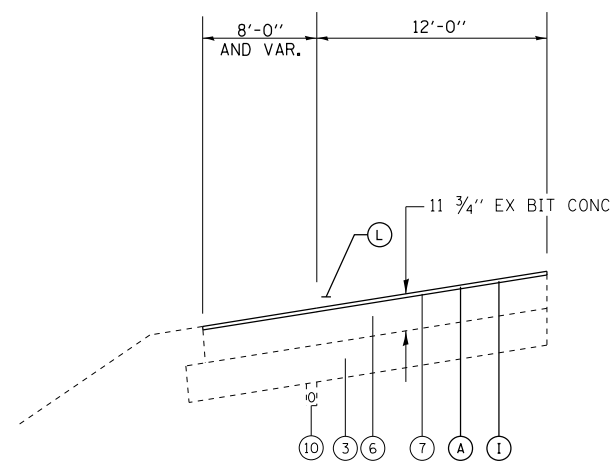
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

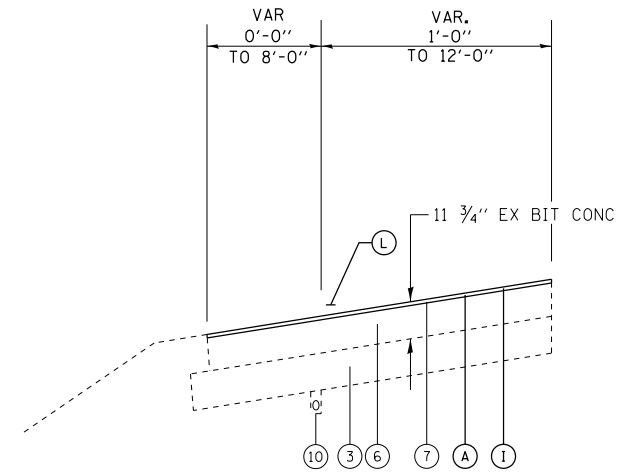
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							**	SANGAMON	85	34
ILLINOIS FED. AID PROJECT CONTRACT NO. 72J83										



TYPICAL #30
STEVENSON DRIVE RAMPS
RAMP B STA. 1+16 TO STA. 12+19.26



TYPICAL #31
STEVENSON DRIVE RAMPS
RAMP B STA. 12+19.26 TO STA. 22+19.95



TYPICAL #32
STEVENSON DRIVE RAMPS
RAMP B STA. 22+19.95 TO STA. 27+69.36

PAVED SHOULDER SLOPE
4.0% CROSS SLOPE IS DESIRABLE ON NORMAL CROWN SECTIONS

ON THE HIGH SIDE OF SUPERELEVATED CURVES, SLOPE THE SHOULDER SO THAT THE ALGEBRETIC DIFFERENCE BETWEEN THE PAVEMENT AND SHOULDERS ARE NOT GREATER THAN 8%

ON THE LOW SIDE OF SUPERELEVATED CURVES, SLOPE THE SHOULDER 4% UNLESS THE SUPER ELEVATED SECTION IS GREATER THAN 4% THEN MATCH SUPERELEVATED SLOPE

PAVEMENT SLOPE
CROSS SLOPE IN CROWN SECTIONS: SHALL BE 1.5%

SUPER ELEVATED SECTIONS: SEE SUPER ELEVATION DETAILS FOR MORE INFORMATION

LEGEND

① EX STABILIZED SUB-BASE 4"	Ⓐ PR HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
② EX AGGREGATE SUB-BASE 4"	Ⓑ PR HOT-MIX ASPHALT SURFACE REMOVAL, 2"
③ EX SUB-BASE GRANULAR MATERIAL 12"	Ⓒ PR HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4" AVG.
④ EX PCC PAVEMENT 10"	Ⓓ PR HOT-MIX ASPHALT SURFACE REMOVAL, 7" AVG.
⑤ EX PCC BASE COURSE 10"	Ⓔ PR HOT-MIX ASPHALT SURFACE REMOVAL, 6 1/4" AVG.
⑥ EX HMA BASE COURSE 10 1/4"	Ⓕ PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 4 3/4"
⑦ EX BIT SURFACE COURSE (THICK VARIES)	Ⓖ PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 2 1/4"
⑧ EX BITUMINOUS SHOULDERS (THICK VARIES)	Ⓗ PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 5 1/2"
⑨ EX PIPE UNDERDRAINS 4"	Ⓘ PR POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N90 1 1/2"
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⑪ EX BITUMINOUS CONCRETE PAVEMENT 18"	Ⓚ PR AGGREGATE WEDGE SHOULDER, TYPE B
⑫ EX PCC BASE COURSE 9"	Ⓛ PR PAVEMENT MARKING
⑬ EX PCC PAVEMENT 8" STD REINFORCEMENT	
⑭ EX PCC PAVEMENT 10" STD REINFORCEMENT	
⑮ EX HOT-MIX ASPHALT SHOULDERS, 4"	
⑯ EX CABLE MEDIAN BARRIER	

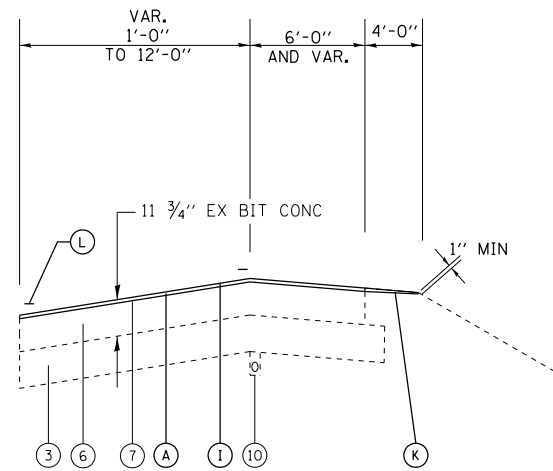
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

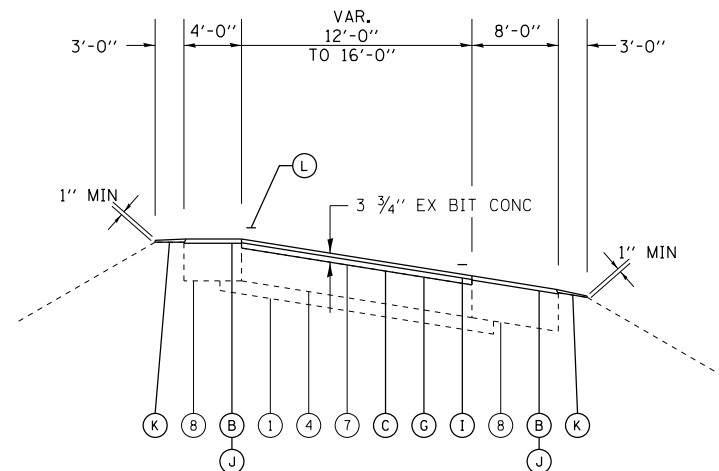
TYPICAL SECTIONS

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							**	SANGAMON	85	35
								CONTRACT NO. 72J83		
								ILLINOIS FED. AID PROJECT		

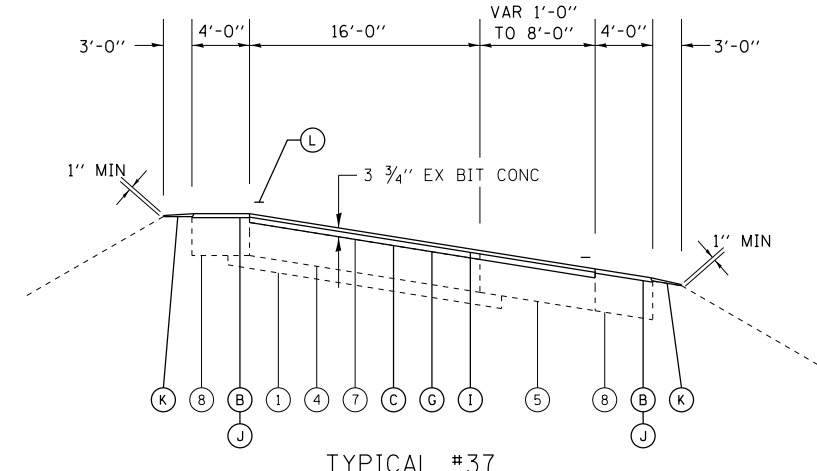
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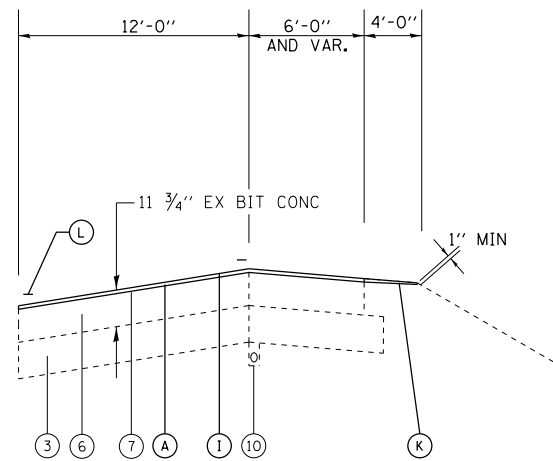
TYPICAL #33
STEVENSON DRIVE RAMPS
RAMP C STA. 0+00.00 TO STA. 2+75.77



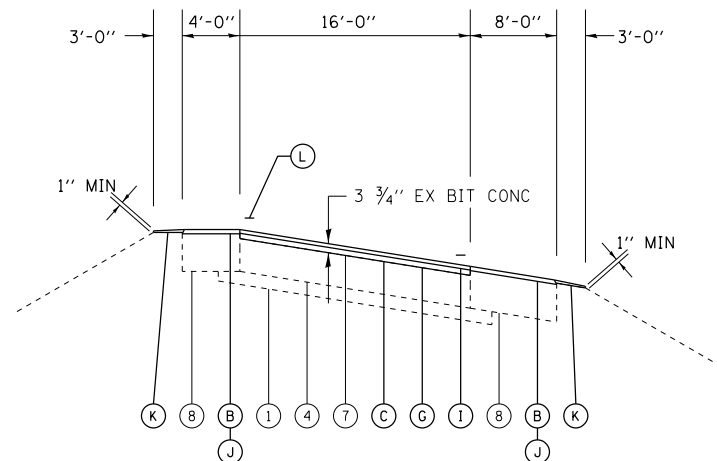
TYPICAL #35
STEVENSON DRIVE RAMPS
RAMP C STA. 6+83.67 TO STA. 9+43.11



TYPICAL #37
STEVENSON DRIVE RAMPS
RAMP C STA. 14+72.79 TO STA. 18+27.00



TYPICAL #34
STEVENSON DRIVE RAMPS
RAMP C STA. 2+75.77 TO STA. 6+83.67



TYPICAL #36
STEVENSON DRIVE RAMPS
RAMP C STA. 9+43.11 TO STA. 14+72.79

PAVED SHOULDER SLOPE
4.0% CROSS SLOPE IS DESIRABLE ON NORMAL CROWN SECTIONS

ON THE HIGH SIDE OF SUPERELEVATED CURVES, SLOPE THE SHOULDER SO THAT THE ALGEBRETIC DIFFERENCE BETWEEN THE PAVEMENT AND SHOULDERS ARE NOT GREATER THAN 8%

ON THE LOW SIDE OF SUPERELEVATED CURVES, SLOPE THE SHOULDER 4% UNLESS THE SUPER ELEVATED SECTION IS GREATER THAN 4% THEN MATCH SUPERELEVATED SLOPE

PAVEMENT SLOPE
CROSS SLOPE IN CROWN SECTIONS: SHALL BE 1.5%

SUPER ELEVATED SECTIONS: SEE SUPER ELEVATION DETAILS FOR MORE INFORMATION

LEGEND

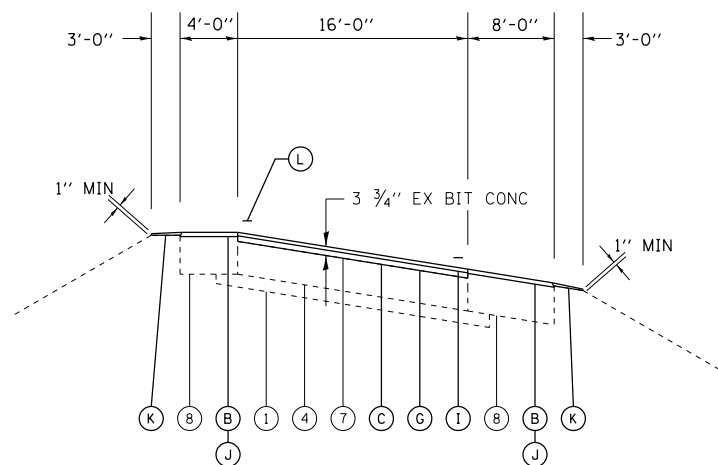
①	EX STABILIZED SUB-BASE 4"	Ⓐ	PR HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
②	EX AGGREGATE SUB-BASE 4"	Ⓑ	PR HOT-MIX ASPHALT SURFACE REMOVAL, 2"
③	EX SUB-BASE GRANULAR MATERIAL 12"	Ⓒ	PR HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4" AVG.
④	EX PCC PAVEMENT 10"	Ⓓ	PR HOT-MIX ASPHALT SURFACE REMOVAL, 7" AVG.
⑤	EX PCC BASE COURSE 10"	Ⓔ	PR HOT-MIX ASPHALT SURFACE REMOVAL, 6 1/4" AVG.
⑥	EX HMA BASE COURSE 10 1/4"	Ⓕ	PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 4 3/4"
⑦	EX BIT SURFACE COURSE (THICK VARIES)	Ⓖ	PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 2 1/4"
⑧	EX BITUMINOUS SHOULDERS (THICK VARIES)	Ⓗ	PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 5 1/2"
⑨	EX PIPE UNDERDRAINS 4"	Ⓘ	PR POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N90 1 1/2"
⑩	EX PIPE UNDERDRAINS 6"	Ⓝ	PR HOT-MIX ASPHALT SHOULDERS, 2"
⑪	EX BITUMINOUS CONCRETE PAVEMENT 18"	Ⓚ	PR AGGREGATE WEDGE SHOULDER, TYPE B
⑫	EX PCC BASE COURSE 9"	Ⓛ	PR PAVEMENT MARKING
⑬	EX PCC PAVEMENT 8" STD REINFORCEMENT		
⑭	EX PCC PAVEMENT 10" STD REINFORCEMENT		
⑮	EX HOT-MIX ASPHALT SHOULDERS, 4"		
⑯	EX CABLE MEDIAN BARRIER		

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
							**	SANGAMON	85	36
								CONTRACT NO. 72J83		
								ILLINOIS FED. AID PROJECT		



TYPICAL #38
 STEVENSON DRIVE RAMPS
 RAMP D STA. 0+98.00 TO STA. 23+29.60

PAVED SHOULDER SLOPE
 4.0% CROSS SLOPE IS DESIRABLE ON NORMAL CROWN SECTIONS
 ON THE HIGH SIDE OF SUPERELEVATED CURVES, SLOPE THE SHOULDER SO THAT THE ALGEBRETIC DIFFERENCE BETWEEN THE PAVEMENT AND SHOULDERS ARE NOT GREATER THAN 8%
 ON THE LOW SIDE OF SUPERELEVATED CURVES, SLOPE THE SHOULDER 4% UNLESS THE SUPER ELEVATED SECTION IS GREATER THAN 4% THEN MATCH SUPERELEVATED SLOPE

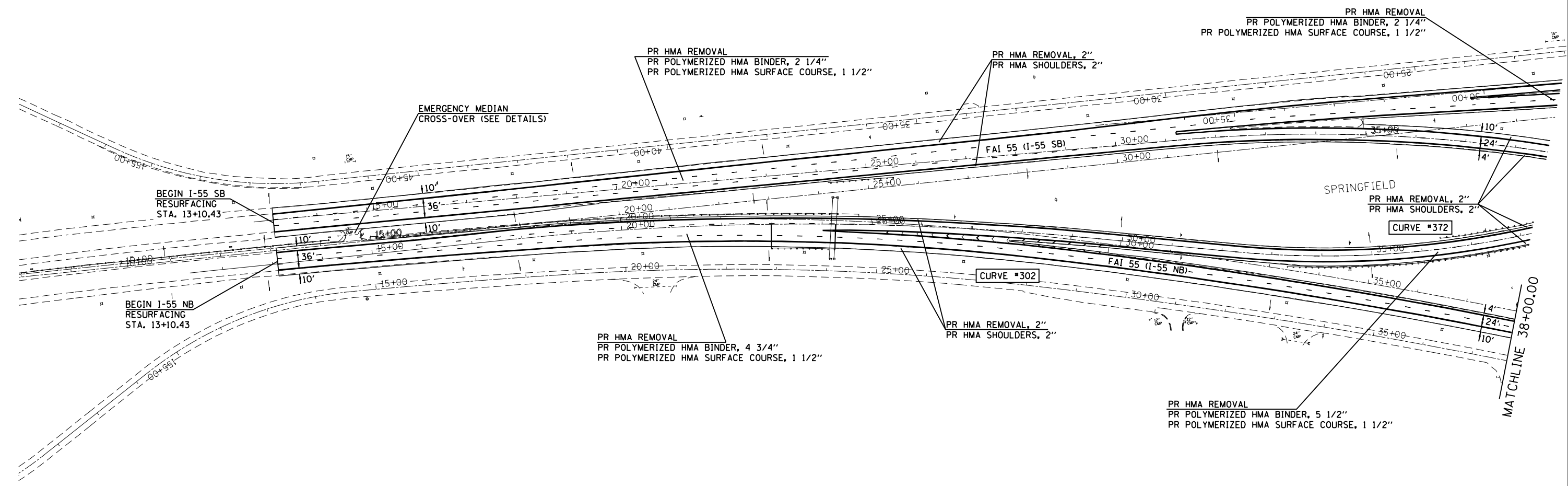
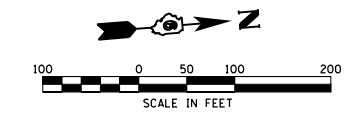
PAVEMENT SLOPE
 CROSS SLOPE IN CROWN SECTIONS: SHALL BE 1.5%
 SUPER ELEVATED SECTIONS: SEE SUPER ELEVATION DETAILS FOR MORE INFORMATION

LEGEND

①	EX STABILIZED SUB-BASE 4"	Ⓐ	PR HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
②	EX AGGREGATE SUB-BASE 4"	Ⓑ	PR HOT-MIX ASPHALT SURFACE REMOVAL, 2"
③	EX SUB-BASE GRANULAR MATERIAL 12"	Ⓒ	PR HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4" AVG.
④	EX PCC PAVEMENT 10"	Ⓓ	PR HOT-MIX ASPHALT SURFACE REMOVAL, 7" AVG.
⑤	EX PCC BASE COURSE 10"	Ⓔ	PR HOT-MIX ASPHALT SURFACE REMOVAL, 6 1/4" AVG.
⑥	EX HMA BASE COURSE 10 1/4"	Ⓕ	PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 4 3/4"
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⑧	EX BITUMINOUS SHOULDERS (THICK VARIES)	Ⓗ	PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 5 1/2"
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⑮	EX HOT-MIX ASPHALT SHOULDERS, 4"		
⑯	EX HOT-MIX ASPHALT SHOULDERS, 4"		
⑰	EX CABLE MEDIAN BARRIER		

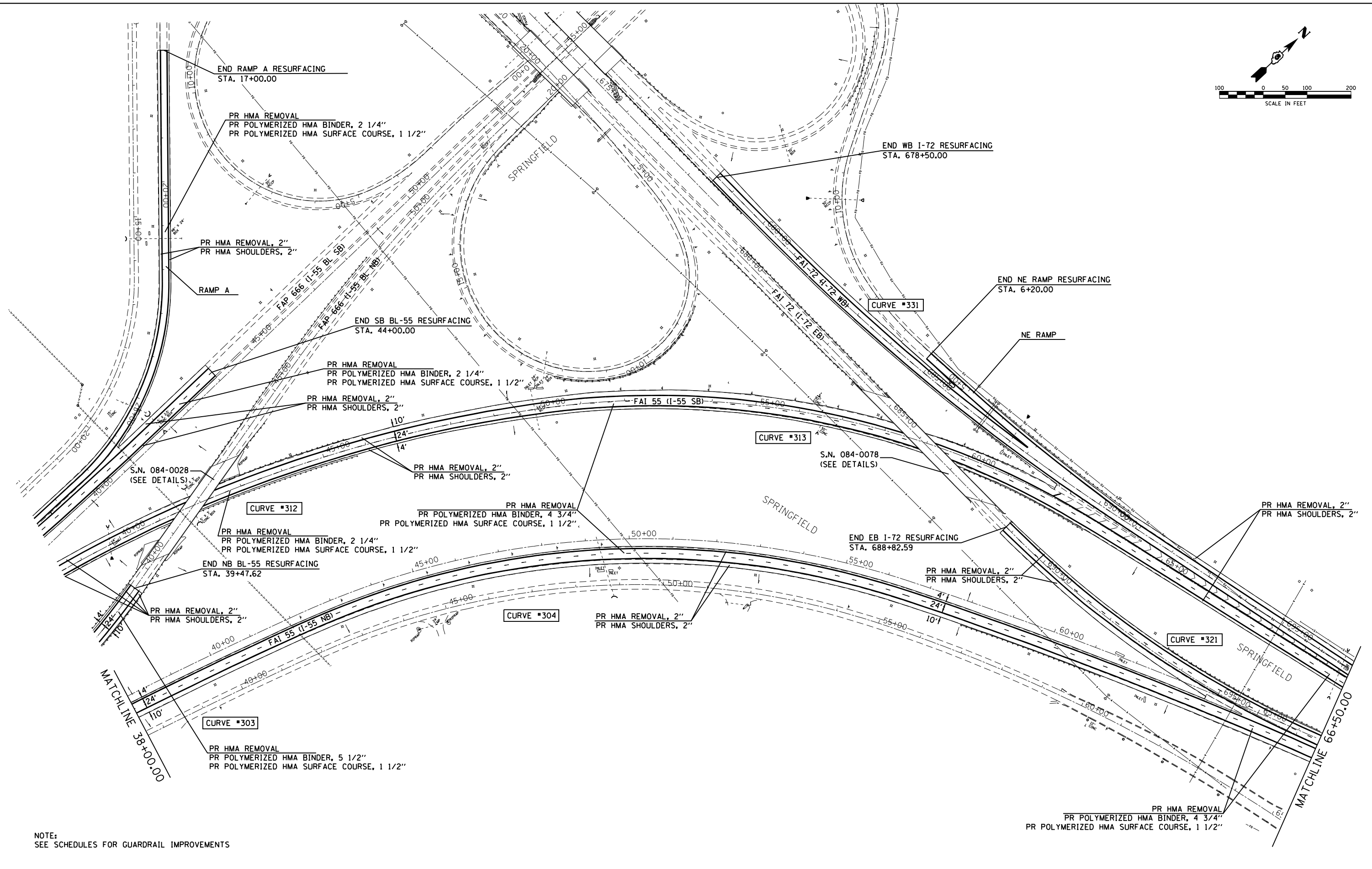
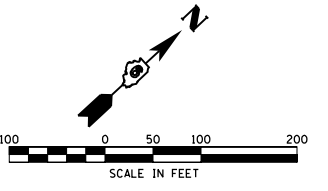
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Default		DATE -	REVISED -								SANGAMON	85	37
											CONTRACT NO. 72J83		

*F.A.I. 55, F.A.I. 72, F.A.P. 666 *(84-4, 84-3, 84-9, 110) RS



NOTE:
SEE SCHEDULES FOR GUARDRAIL IMPROVEMENTS

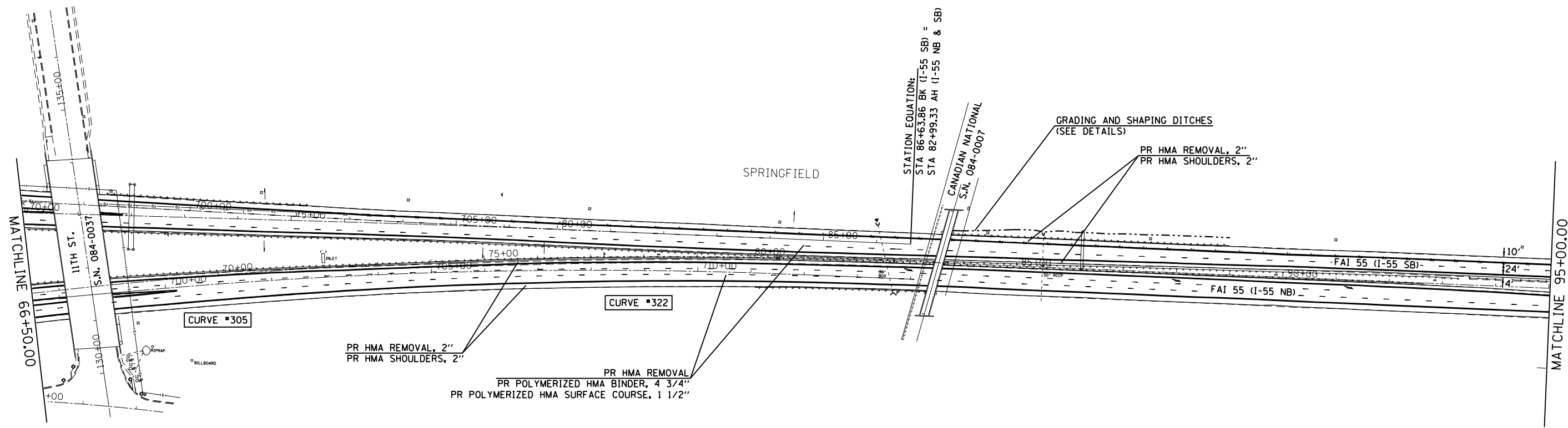
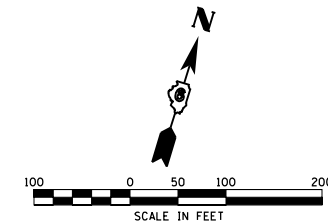
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Default		CHECKED -	REVISED -								CONTRACT NO. 72J83			
		DATE -	REVISED -								*F.A.I. 55, F.A.I. 72, F.A.P. 666 *(84-4, 84-3, 84-9, 110) RS			



NOTE:
SEE SCHEDULES FOR GUARDRAIL IMPROVEMENTS

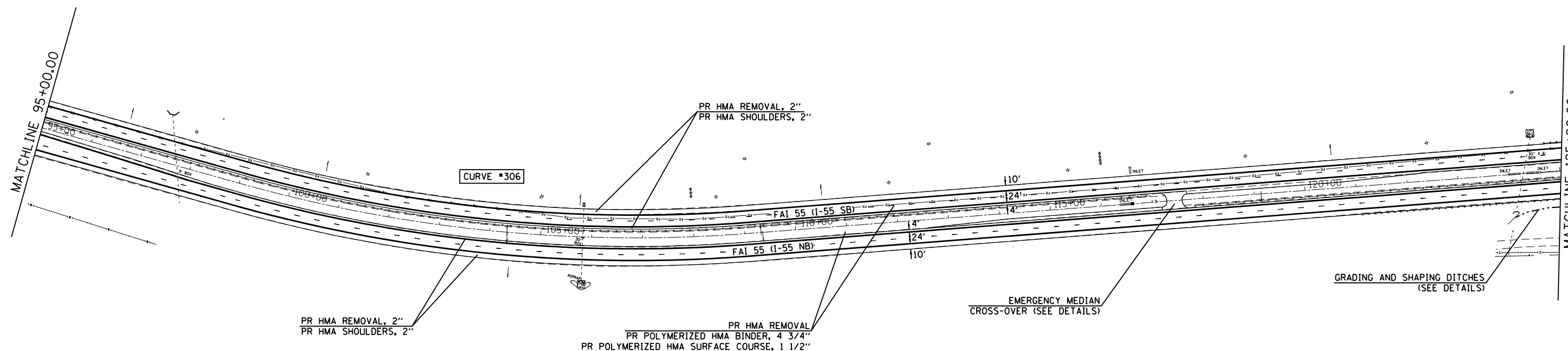
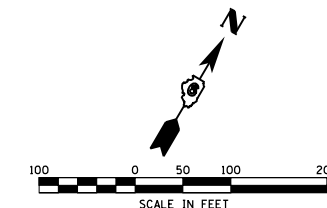
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Default		DATE -	REVISED -								SANGAMON	85	39
											CONTRACT NO. 72J83		

*F.A.I. 55, F.A.I. 72, F.A.P. 666 ** (84-4, 84-3, 84-9, 110) RS



NOTE:
SEE SCHEDULES FOR GUARDRAIL IMPROVEMENTS

FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN SHEET			F.A. * RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
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Default	PLOT SCALE = 200.0000' / in.	DATE -	REVISED -		CONTRACT NO. 72J83									
	PLOT DATE = 10/20/2017				*F.A.I. 55, F.A.I. 72, F.A.P. 666 *(84-4, 84-3, 84-9, 110) RS									



NOTE:
SEE SCHEDULES FOR GUARDRAIL IMPROVEMENTS

FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -
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	PLOT DATE = 10/20/2017	DATE -	REVISED -

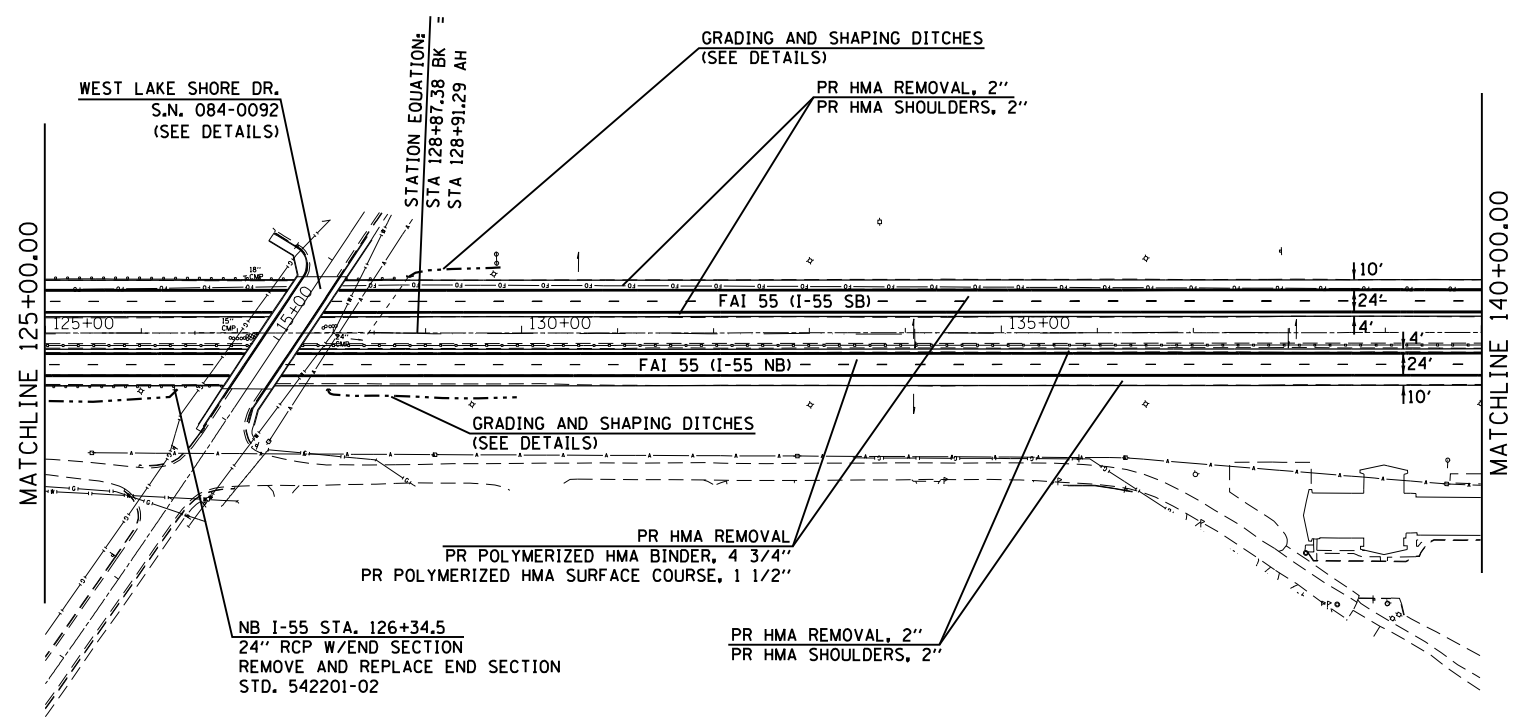
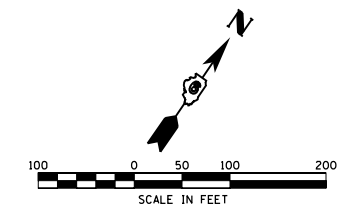
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN SHEET

SCALE: SHEET OF SHEETS STA. TO STA.

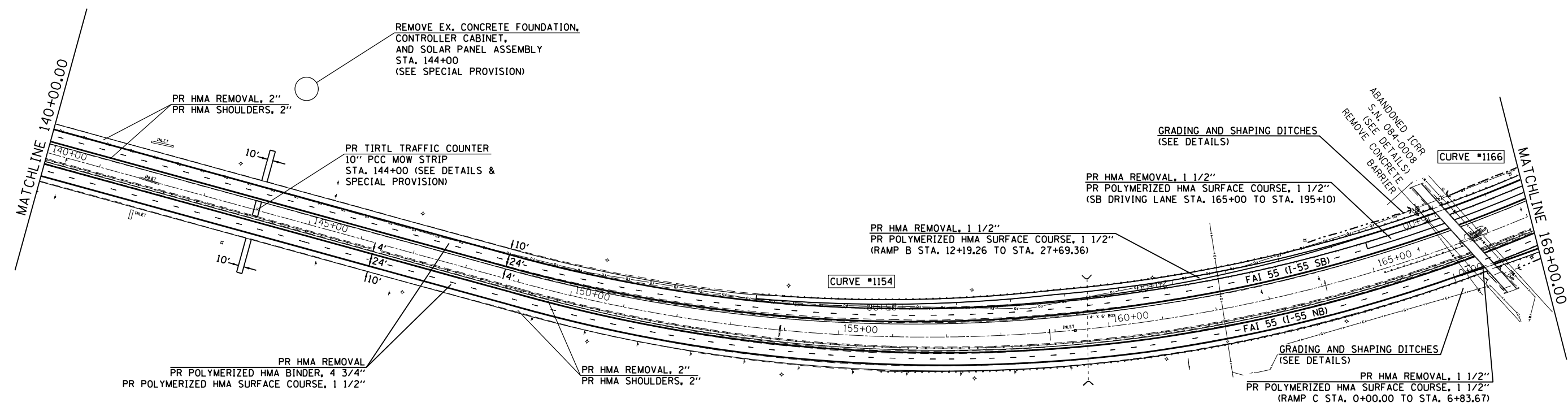
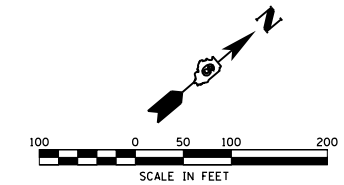
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*	**	SANGAMON	85	41
CONTRACT NO. 72J83				
ILLINOIS FED. AID PROJECT				

*F.A.I. 55, F.A.I. 72, F.A.P. 666 ** (84-4, 84-3, 84-9, 110) RS



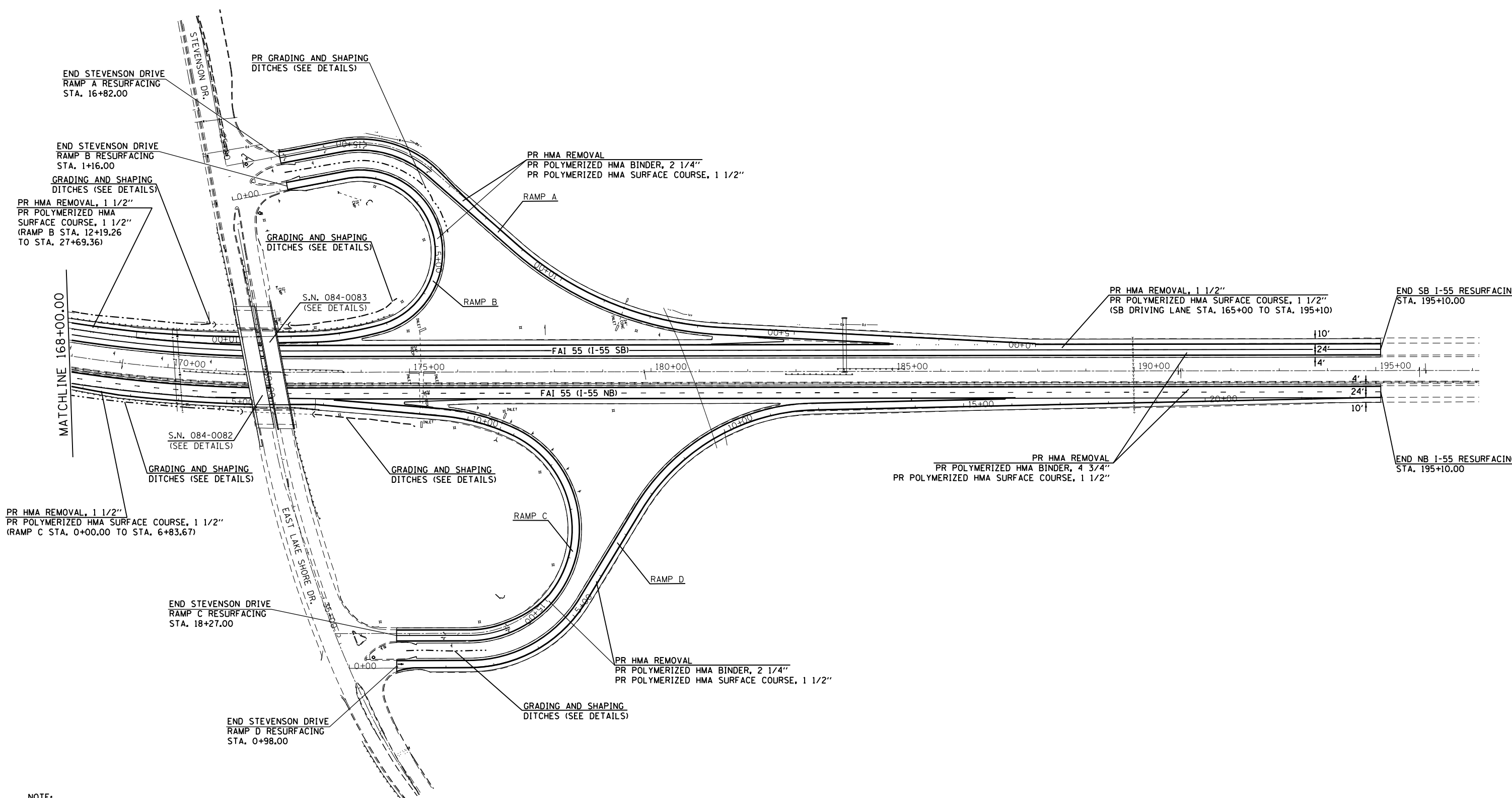
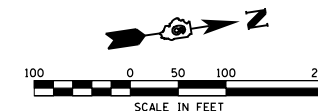
NOTE:
SEE SCHEDULES FOR GUARDRAIL IMPROVEMENTS

FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN SHEET			F.A. * RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT DATE = 10/20/2017	DATE -	REVISED -		*F.A.I. 55, F.A.I. 72, F.A.P. 666 ** (84-4, 84-3, 84-9, 110) RS							



NOTE:
SEE SCHEDULES FOR GUARDRAIL IMPROVEMENTS

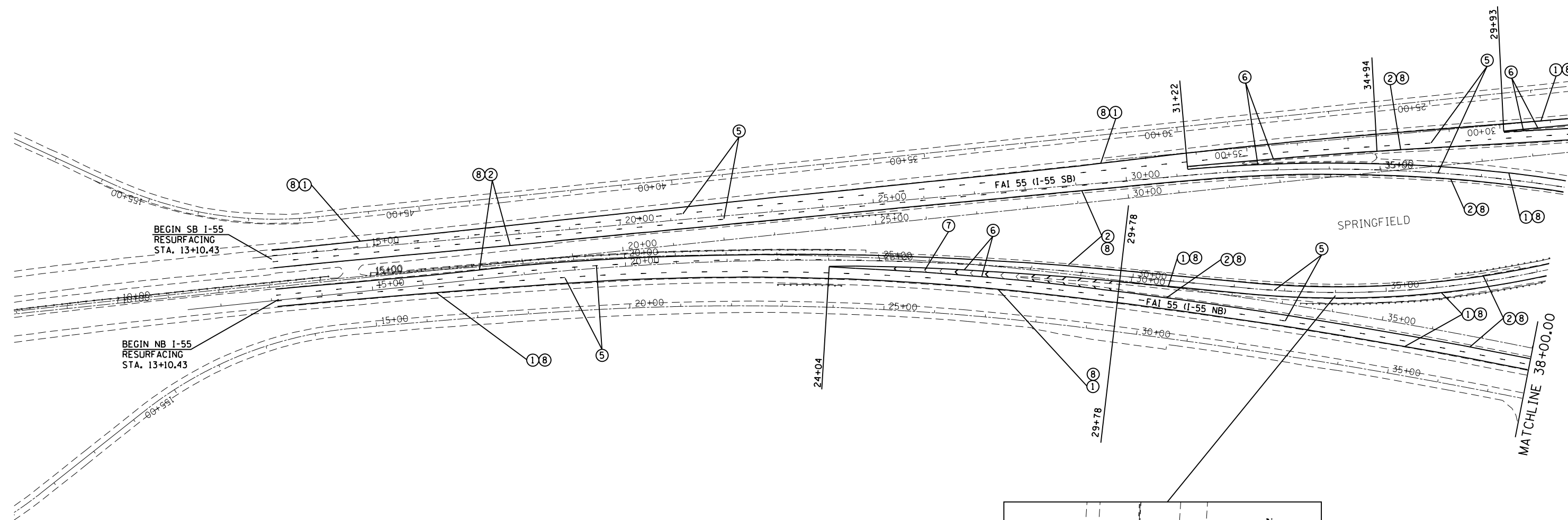
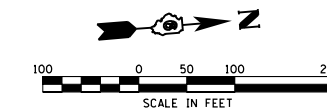
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Default	PLOT SCALE = 200.0000 1/in.	CHECKED -	REVISED -		CONTRACT NO. 72J83								
	PLOT DATE = 10/20/2017	DATE -	REVISED -		*F.A.I. 55, F.A.I. 72, F.A.P. 666 *(84-4, 84-3, 84-9, 110) RS								



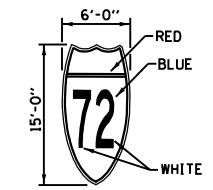
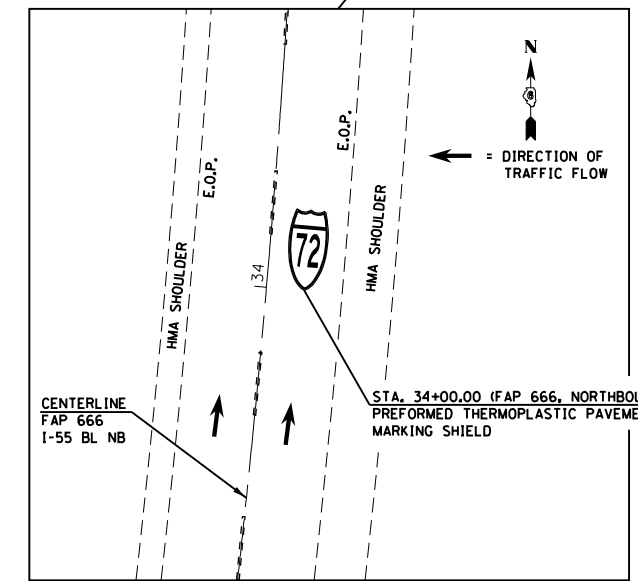
NOTE:
SEE SCHEDULES FOR GUARDRAIL IMPROVEMENTS

FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN SHEET			F.A. * RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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Default		DATE -	REVISED -								SANGAMON	85	44
											CONTRACT NO.	72J83	

*F.A.I. 55, F.A.I. 72, F.A.P. 666 ** (84-4, 84-3, 84-9, 110) RS

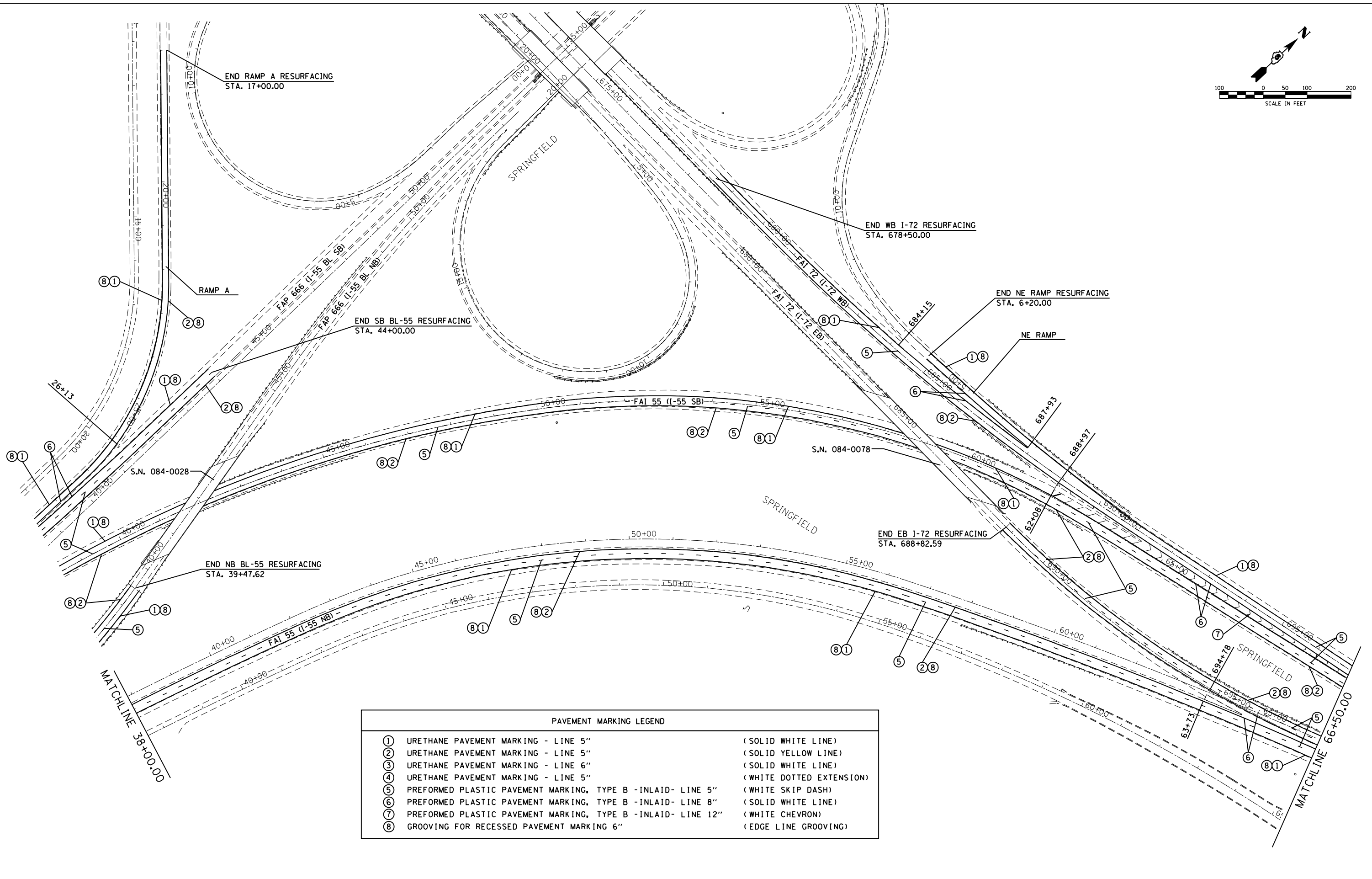
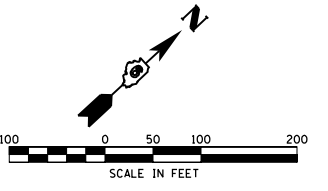


PAVEMENT MARKING LEGEND		
①	URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
②	URETHANE PAVEMENT MARKING - LINE 5"	(SOLID YELLOW LINE)
③	URETHANE PAVEMENT MARKING - LINE 6"	(SOLID WHITE LINE)
④	URETHANE PAVEMENT MARKING - LINE 5"	(WHITE DOTTED EXTENSION)
⑤	PERFORMED PLASTIC PAVEMENT MARKING, TYPE B -INLAID- LINE 5"	(WHITE SKIP DASH)
⑥	PERFORMED PLASTIC PAVEMENT MARKING, TYPE B -INLAID- LINE 8"	(SOLID WHITE LINE)
⑦	PERFORMED PLASTIC PAVEMENT MARKING, TYPE B -INLAID- LINE 12"	(WHITE CHEVRON)
⑧	GROOVING FOR RECESSED PAVEMENT MARKING 6"	(EDGE LINE GROOVING)

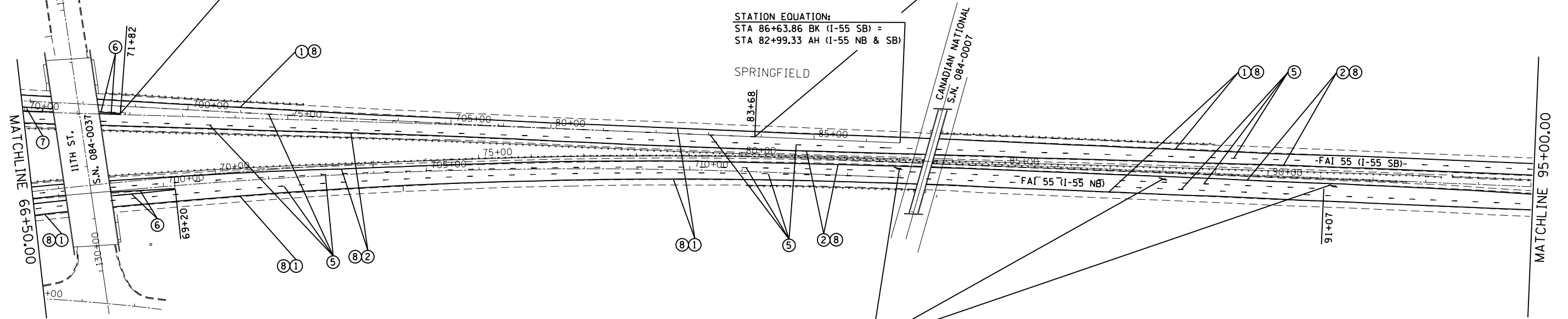
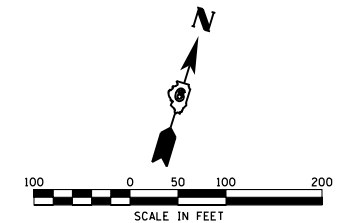
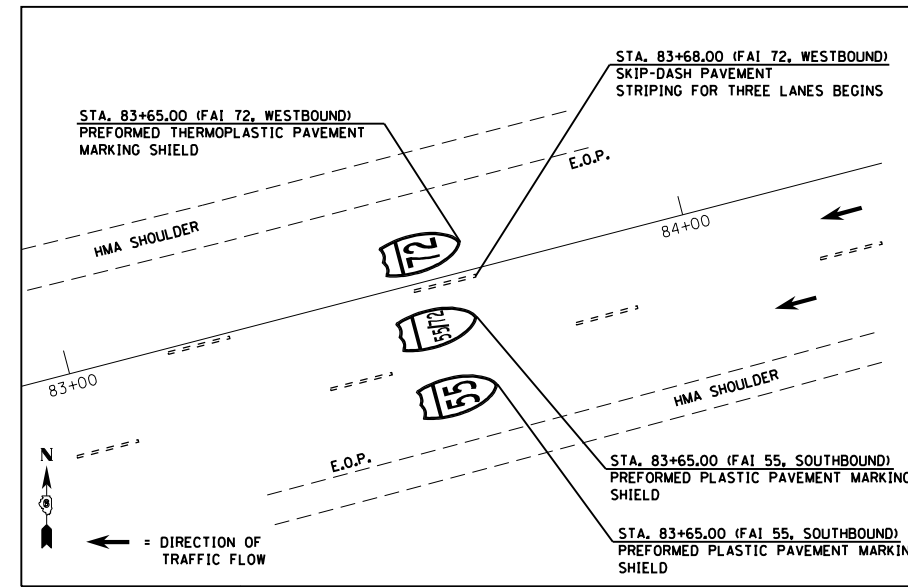
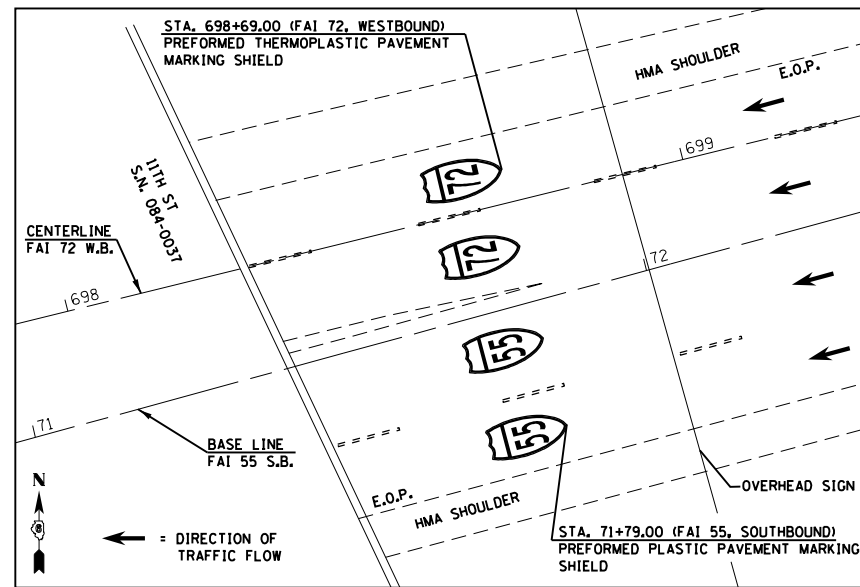


SHIELD DIMENSIONS

NOTES:
SEE THE SPECIAL PROVISIONS FOR FURTHER DETAILS ON THE MATERIALS AND INSTALLATION OF PROPOSED PERFORMED THERMOPLASTIC PAVEMENT MARKING SHIELDS.

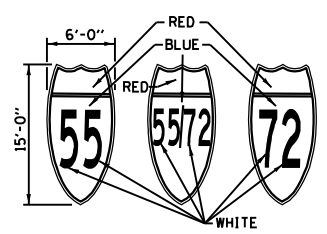


PAVEMENT MARKING LEGEND	
①	URETHANE PAVEMENT MARKING - LINE 5" (SOLID WHITE LINE)
②	URETHANE PAVEMENT MARKING - LINE 5" (SOLID YELLOW LINE)
③	URETHANE PAVEMENT MARKING - LINE 6" (SOLID WHITE LINE)
④	URETHANE PAVEMENT MARKING - LINE 5" (WHITE DOTTED EXTENSION)
⑤	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B -INLAID- LINE 5" (WHITE SKIP DASH)
⑥	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B -INLAID- LINE 8" (SOLID WHITE LINE)
⑦	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B -INLAID- LINE 12" (WHITE CHEVRON)
⑧	GROOVING FOR RECESSED PAVEMENT MARKING 6" (EDGE LINE GROOVING)



STATION EQUATION:
 STA 86+63.86 BK (I-55 SB) =
 STA 82+99.33 AH (I-55 NB & SB)

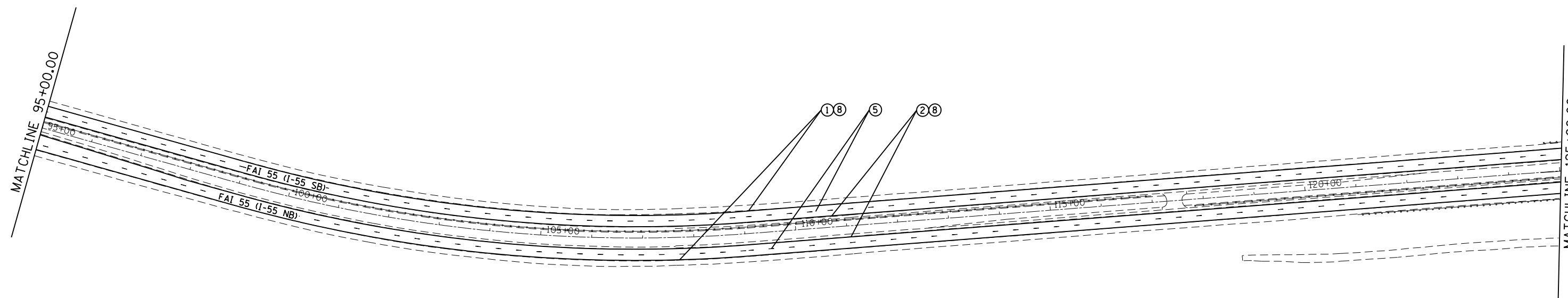
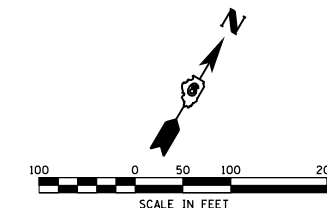
PREFORMED PLASTIC PAVEMENT MARKING TYPE B (INLAID)- LETTERS AND SYMBOLS (WHITE)
 LANE REDUCTION ARROW
 STA. 82+98
 STA. 88+01
 STA. 91+25



SHIELD DIMENSIONS

NOTES:
 SEE THE SPECIAL PROVISIONS FOR FURTHER DETAILS ON THE MATERIALS AND INSTALLATION OF PROPOSED PREFORMED THERMOPLASTIC PAVEMENT MARKING SHIELDS.

PAVEMENT MARKING LEGEND		
①	URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
②	URETHANE PAVEMENT MARKING - LINE 5"	(SOLID YELLOW LINE)
③	URETHANE PAVEMENT MARKING - LINE 6"	(SOLID WHITE LINE)
④	URETHANE PAVEMENT MARKING - LINE 5"	(WHITE DOTTED EXTENSION)
⑤	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B -INLAID- LINE 5"	(WHITE SKIP DASH)
⑥	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B -INLAID- LINE 8"	(SOLID WHITE LINE)
⑦	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B -INLAID- LINE 12"	(WHITE CHEVRON)
⑧	GROOVING FOR RECESSED PAVEMENT MARKING 6"	(EDGE LINE GROOVING)



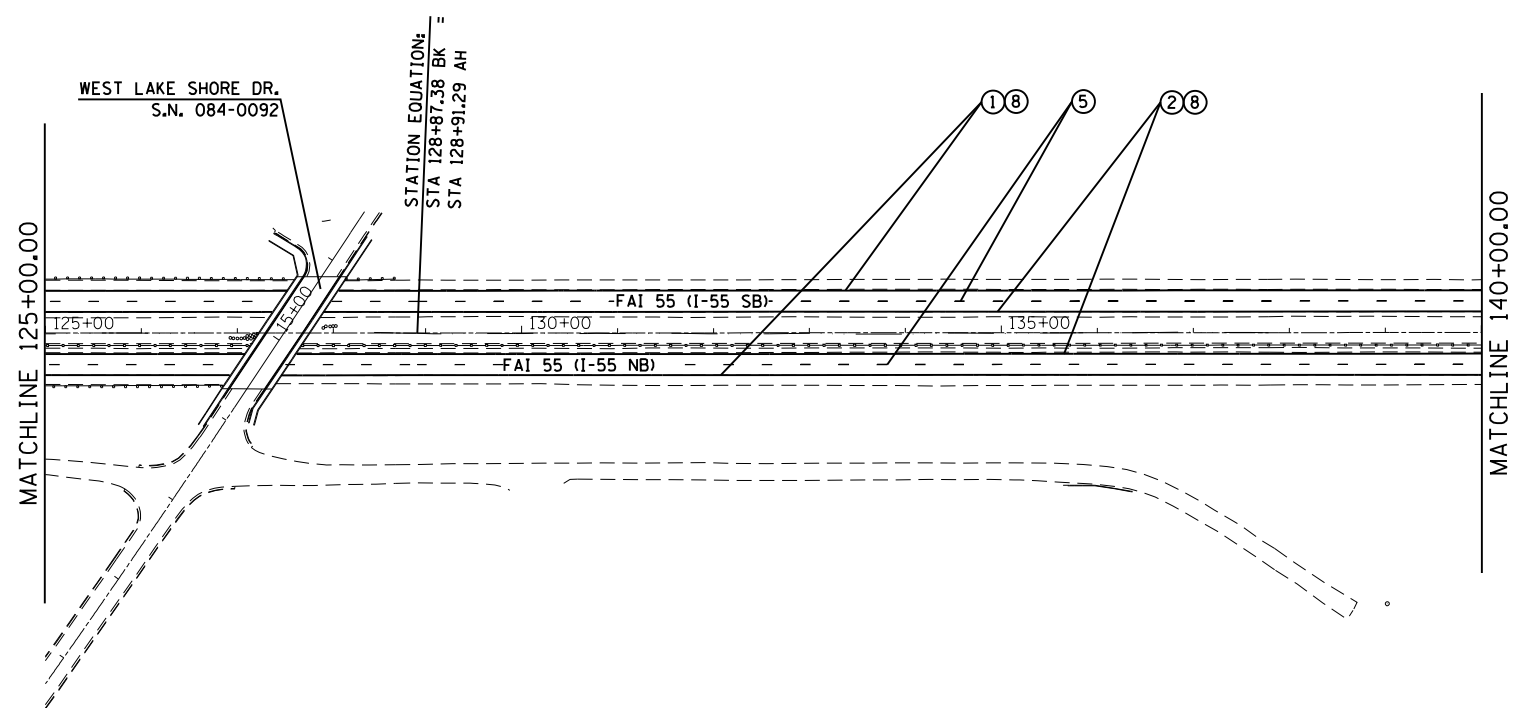
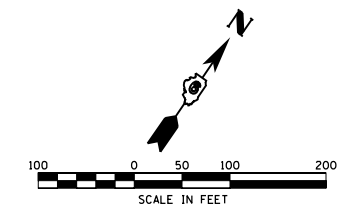
PAVEMENT MARKING LEGEND		
①	URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
②	URETHANE PAVEMENT MARKING - LINE 5"	(SOLID YELLOW LINE)
③	URETHANE PAVEMENT MARKING - LINE 6"	(SOLID WHITE LINE)
④	URETHANE PAVEMENT MARKING - LINE 5"	(WHITE DOTTED EXTENSION)
⑤	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B -INLAID- LINE 5"	(WHITE SKIP DASH)
⑥	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B -INLAID- LINE 8"	(SOLID WHITE LINE)
⑦	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B -INLAID- LINE 12"	(WHITE CHEVRON)
⑧	GROOVING FOR RECESSED PAVEMENT MARKING 6"	(EDGE LINE GROOVING)

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Default	PLOT SCALE = 200.0000 "/in.	CHECKED -	REVISOR -
	PLOT DATE = 10/20/2017	DATE -	REVISION -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING				F.A. * RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	ILLINOIS	FED. AID PROJECT	CONTRACT NO. 72J83
							85	48

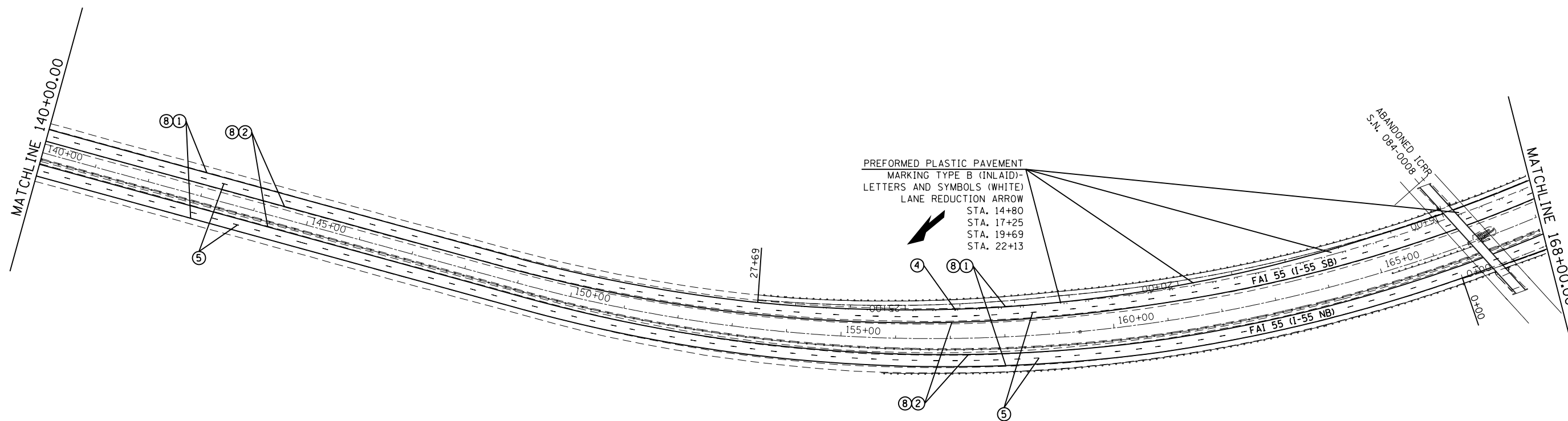
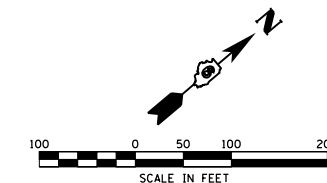
*F.A.I. 55, F.A.I. 72, F.A.P. 666 **(84-4, 84-3, 84-9, 110) RS



PAVEMENT MARKING LEGEND		
①	URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
②	URETHANE PAVEMENT MARKING - LINE 5"	(SOLID YELLOW LINE)
③	URETHANE PAVEMENT MARKING - LINE 6"	(SOLID WHITE LINE)
④	URETHANE PAVEMENT MARKING - LINE 5"	(WHITE DOTTED EXTENSION)
⑤	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B -INLAID- LINE 5"	(WHITE SKIP DASH)
⑥	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B -INLAID- LINE 8"	(SOLID WHITE LINE)
⑦	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B -INLAID- LINE 12"	(WHITE CHEVRON)
⑧	GROOVING FOR RECESSED PAVEMENT MARKING 6"	(EDGE LINE GROOVING)

FILE NAME =	USER NAME = martinj	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING			F.A. * RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
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Default	PLOT SCALE = 200.0000' / in.	DATE -	REVISED -								CONTRACT NO. 72J83			
	PLOT DATE = 10/20/2017													

*F.A.I. 55, F.A.I. 72, F.A.P. 666 **(84-4, 84-3, 84-9, 110) RS



PAVEMENT MARKING LEGEND

①	URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
②	URETHANE PAVEMENT MARKING - LINE 5"	(SOLID YELLOW LINE)
③	URETHANE PAVEMENT MARKING - LINE 6"	(SOLID WHITE LINE)
④	URETHANE PAVEMENT MARKING - LINE 5"	(WHITE DOTTED EXTENSION)
⑤	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B -INLAID- LINE 5"	(WHITE SKIP DASH)
⑥	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B -INLAID- LINE 8"	(SOLID WHITE LINE)
⑦	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B -INLAID- LINE 12"	(WHITE CHEVRON)
⑧	GROOVING FOR RECESSED PAVEMENT MARKING 6"	(EDGE LINE GROOVING)

FILE NAME =	USER NAME = martinj	DESIGNED -	REVISED -
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Default	PLOT SCALE = 200.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 10/20/2017	DATE -	REVISED -

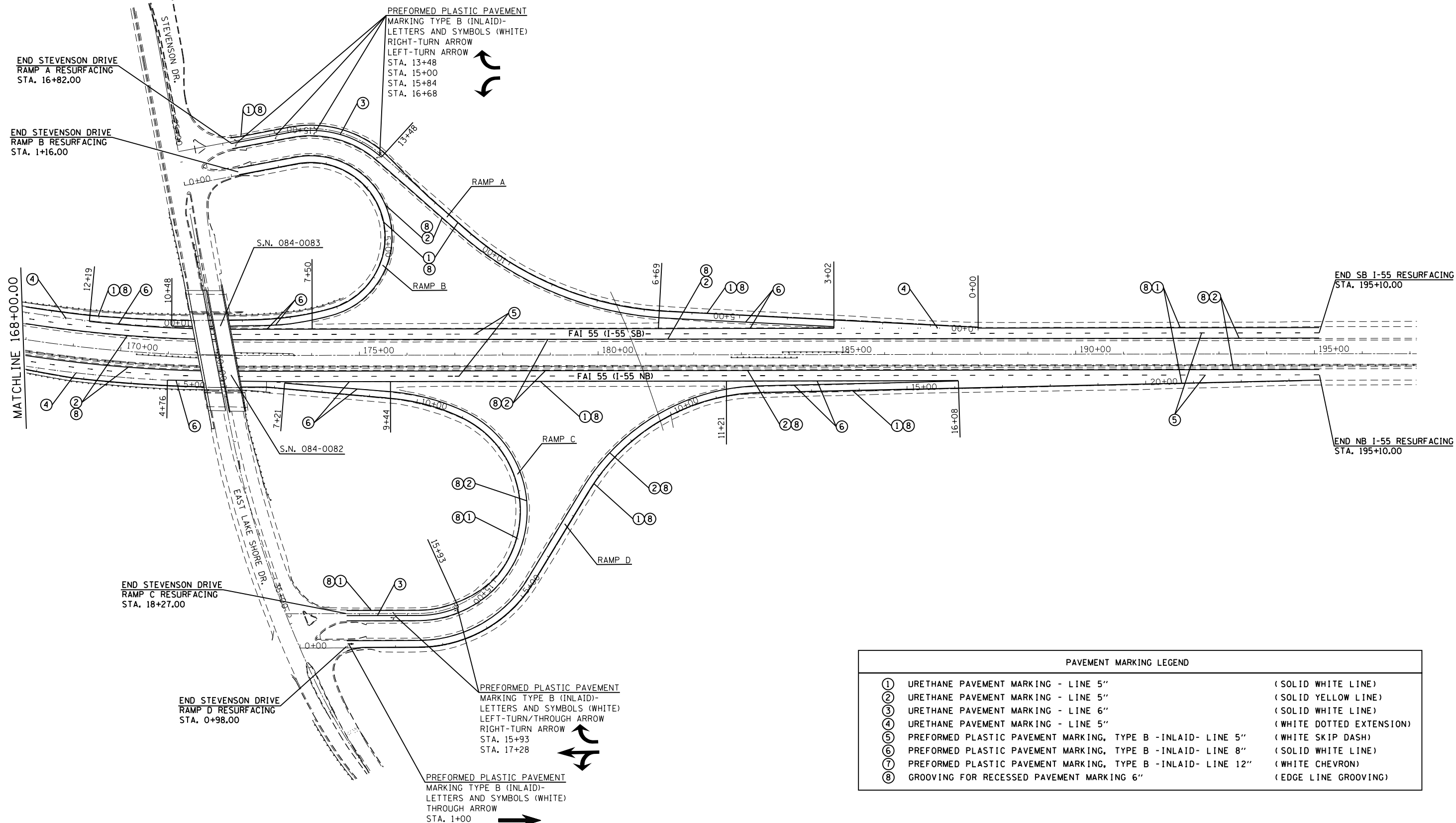
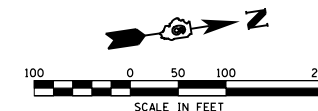
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. * RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**	SANGAMON	85	50
			CONTRACT NO. 72J83	
		ILLINOIS	FED. AID PROJECT	

*F.A.I. 55, F.A.I. 72, F.A.P. 666 **(84-4, 84-3, 84-9, 110) RS



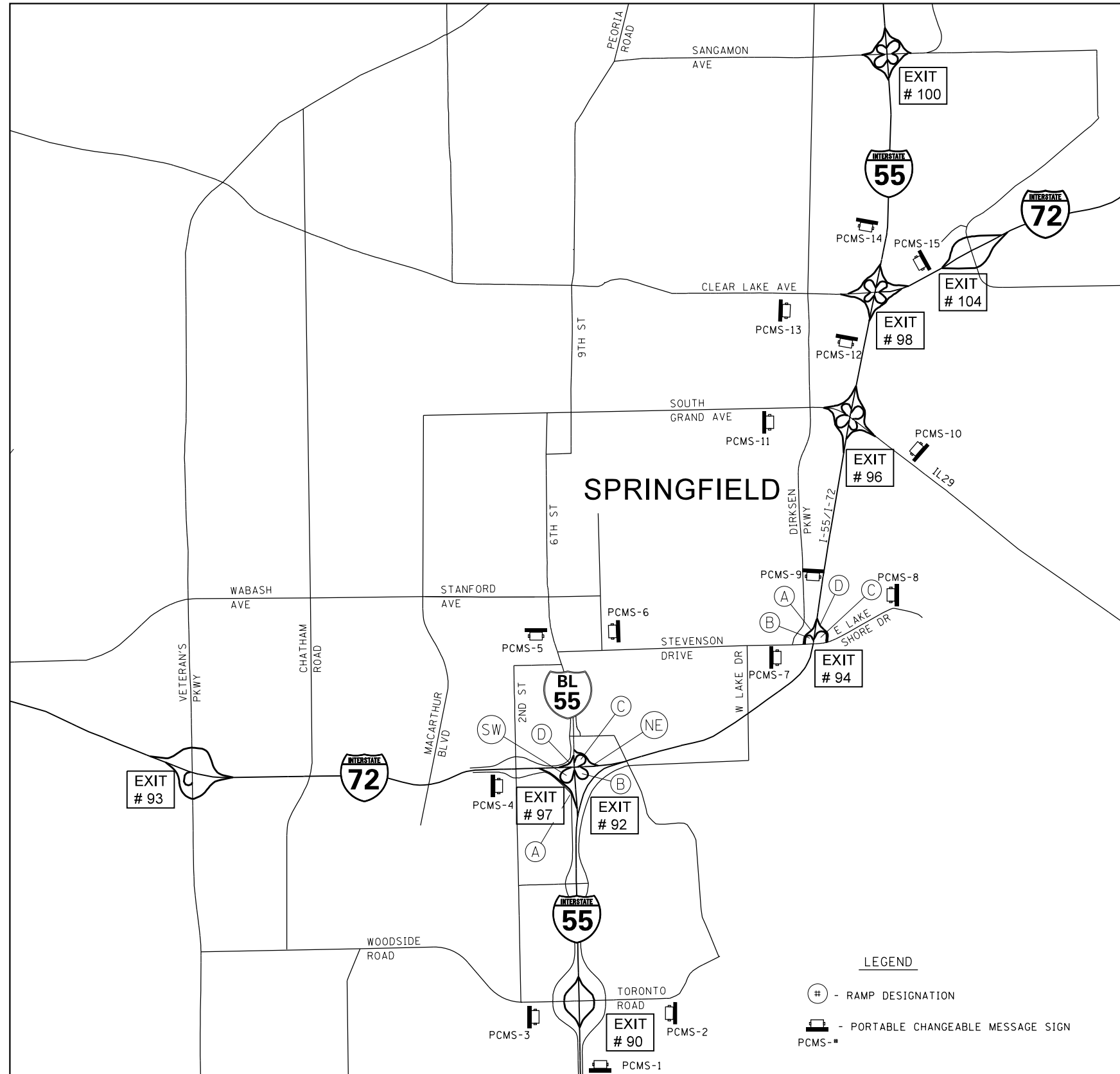
PAVEMENT MARKING LEGEND		
①	URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
②	URETHANE PAVEMENT MARKING - LINE 5"	(SOLID YELLOW LINE)
③	URETHANE PAVEMENT MARKING - LINE 6"	(SOLID WHITE LINE)
④	URETHANE PAVEMENT MARKING - LINE 5"	(WHITE DOTTED EXTENSION)
⑤	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B -INLAID- LINE 5"	(WHITE SKIP DASH)
⑥	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B -INLAID- LINE 8"	(SOLID WHITE LINE)
⑦	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B -INLAID- LINE 12"	(WHITE CHEVRON)
⑧	GROOVING FOR RECESSED PAVEMENT MARKING 6"	(EDGE LINE GROOVING)

FILE NAME =	USER NAME = martinj	DESIGNED -	REVISED -
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PLOT SCALE = 200.0000' / in.		DATE -	REVISED -
PLOT DATE = 10/20/2017			

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING				F.A. * RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	ILLINOIS	FED. AID PROJECT	
						SANGAMON	85	51
							CONTRACT NO. 72J83	

*F.A.I. 55, F.A.I. 72, F.A.P. 666 ** (84-4, 84-3, 84-9, 110) RS



NOTE:
 ALL MESSAGE SIGN SHALL BY PROTECTED
 BY THREE, TYPE III BARRICADES, DRUMS, OR
 VERTICAL BARRICADES AT 50' CENTERS.

FINAL LOCATIONS OF MESSAGE SIGNS SHALL
 BE DETERMINED BY THE ENGINEER

FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -
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DATE = 10/20/2017	DATE -	REVISED -	REVISED -

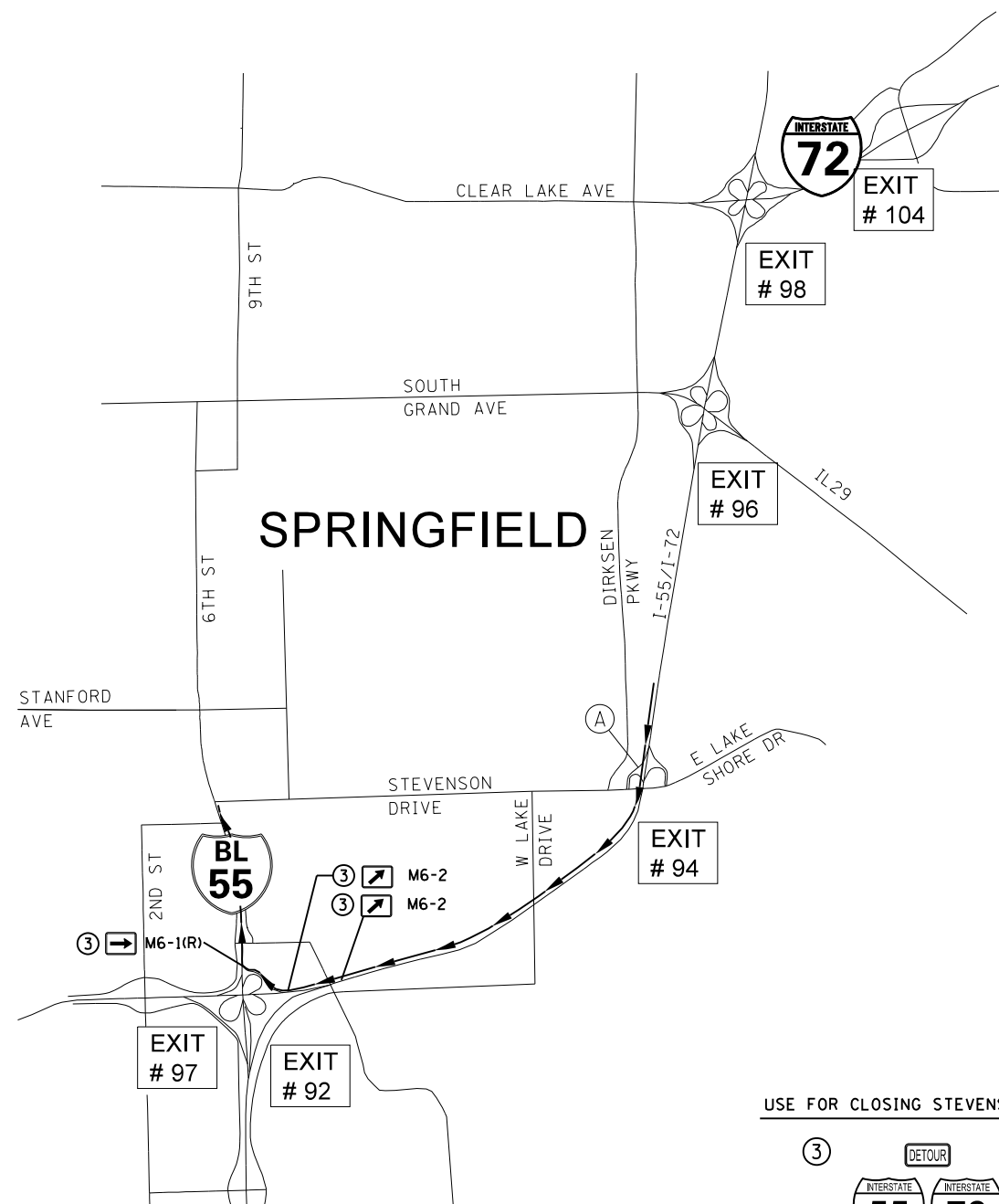
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

MESSAGE SIGN PLACEMENT SHEET			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

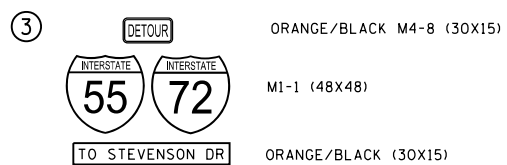
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**	SANGAMON	85	52
CONTRACT NO. 72J83				
ILLINOIS			FED. AID PROJECT	

*F.A.I. 55, F.A.I. 72, F.A.P. 666 ** (84-4, 84-3, 84-9, 110) RS

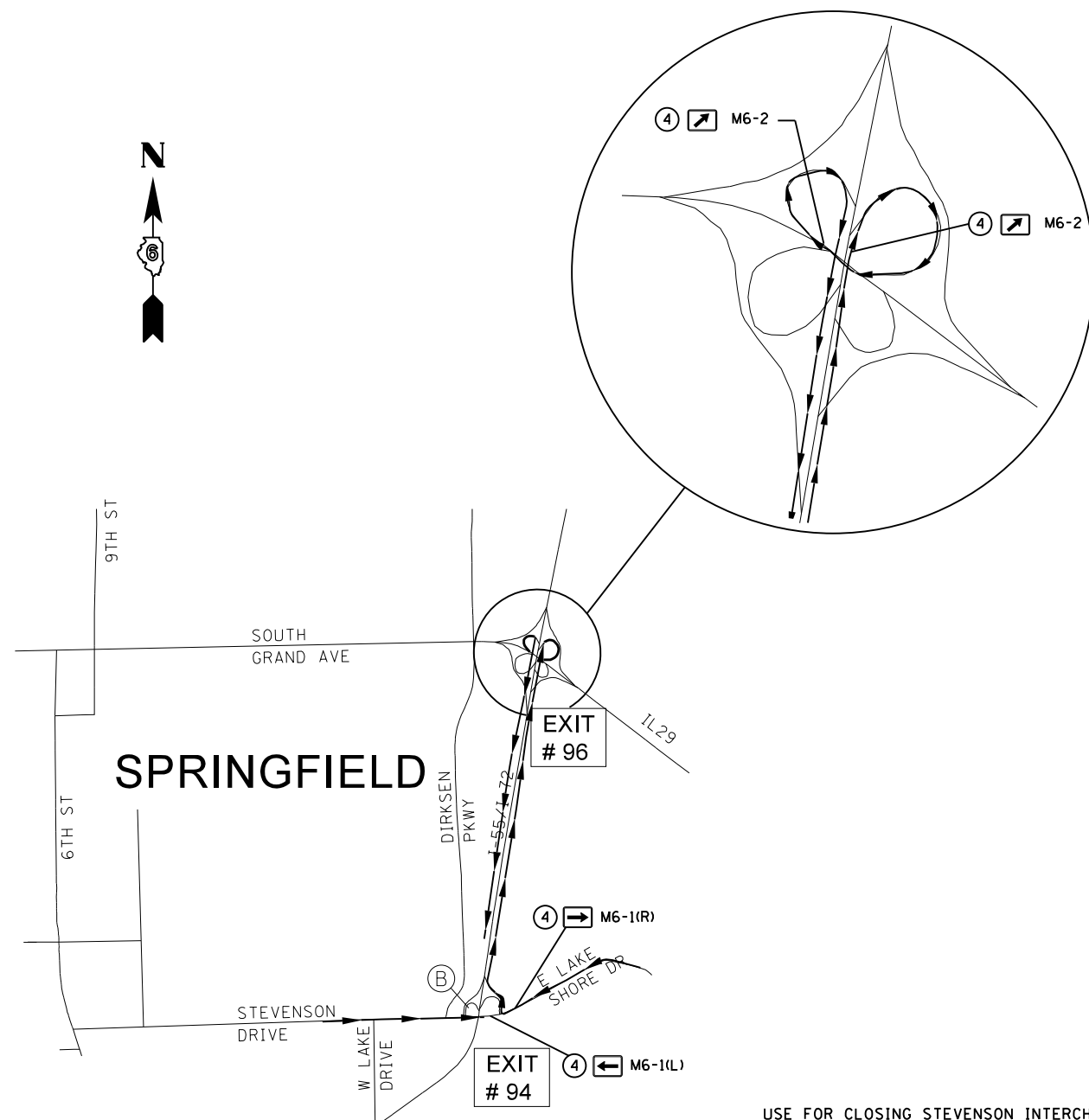
SB I-55 TO STEVENSON DR



USE FOR CLOSING STEVENSON DR INTERCHANGE RAMP A



STEVENSON DR / EAST LAKE SHORE DR TO SB I-55



USE FOR CLOSING STEVENSON INTERCHANGE RAMP B



FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -
pw:\IL04EB\INTEG\Illinois.gov\PWIDOT\Documents\DOT\Offices\District 6\Projects\0672\83\CADData\CADsheets\0672\83\Drawn\Drawn.dgn		DRWN	REVISED -
Default	PLOT SCALE = 70,0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 10/20/2017	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

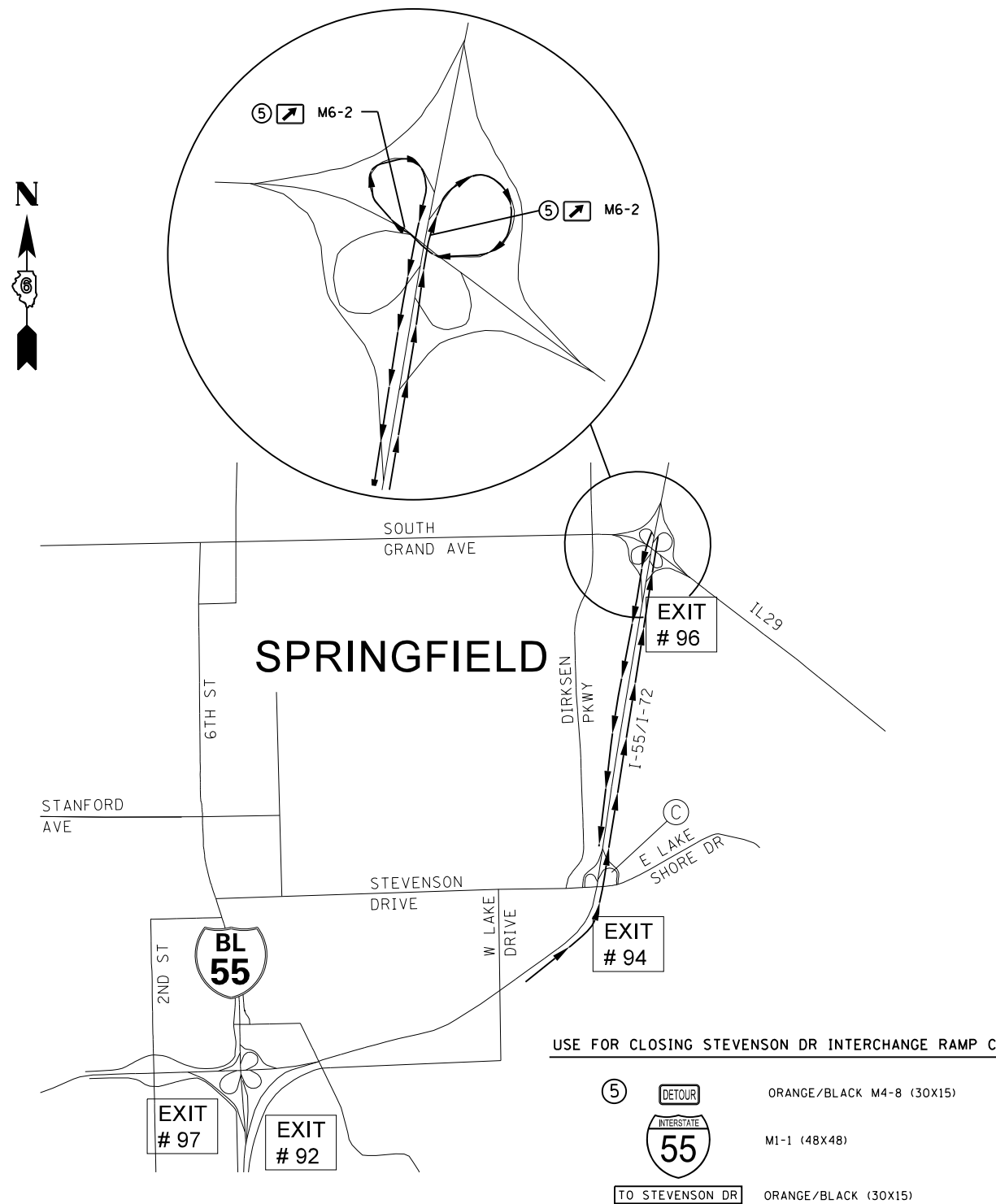
DETOUR PLAN SHEET

SCALE: SHEET OF SHEETS STA. TO STA.

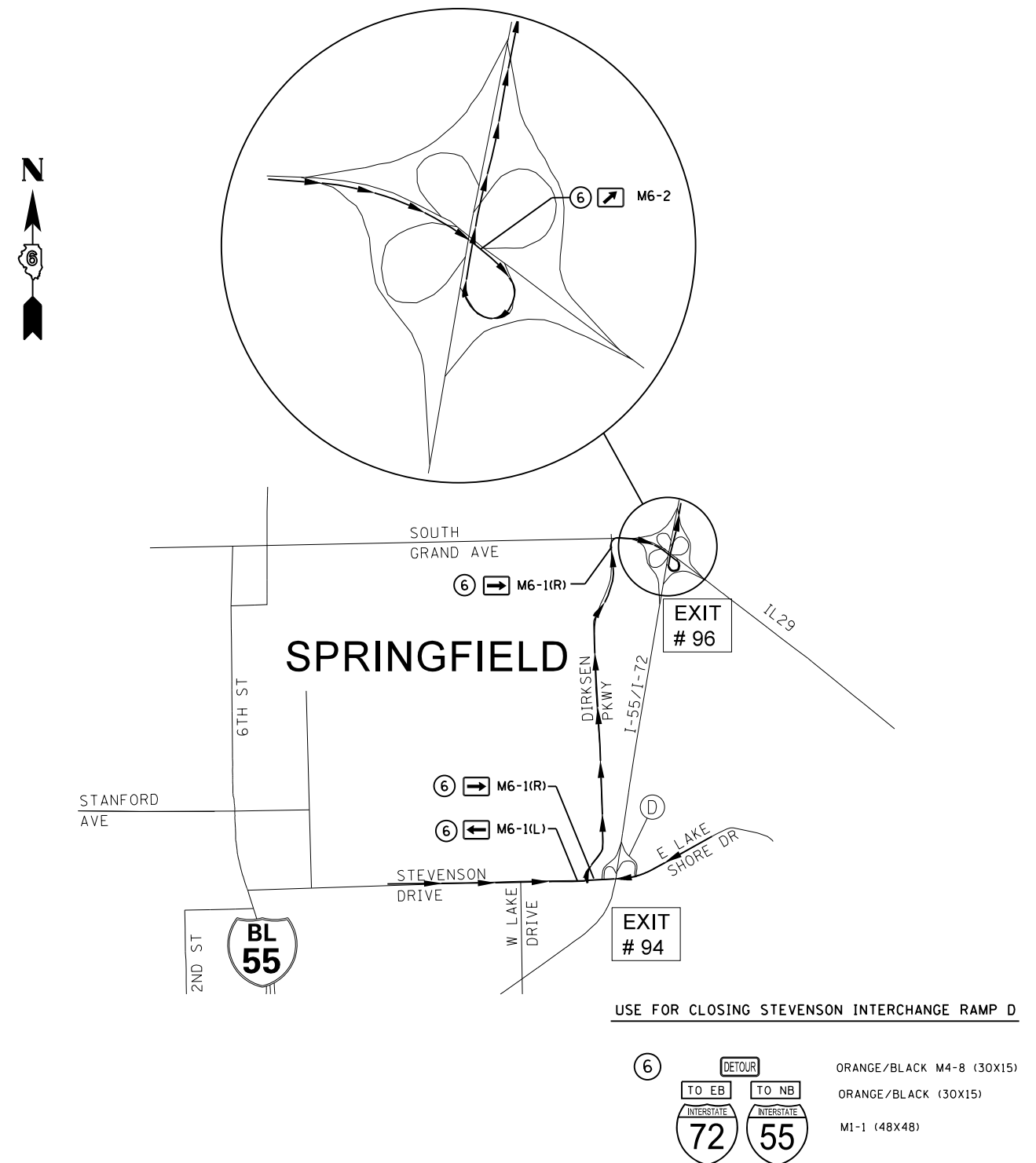
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	**	SANGAMON	85	53
CONTRACT NO. 72J83				
ILLINOIS FED. AID PROJECT				

*F.A.I. 55, F.A.I. 72, F.A.P. 666 ** (84-4, 84-3, 84-9, 110) RS

NB I-55 TO STEVENSON DR



STEVENSON DR / EAST LAKE SHORE DR TO NB I-55 / EB I-72



FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -
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PLOT DATE = 10/20/2017	DATE -	REVISOR -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETOUR PLAN SHEET

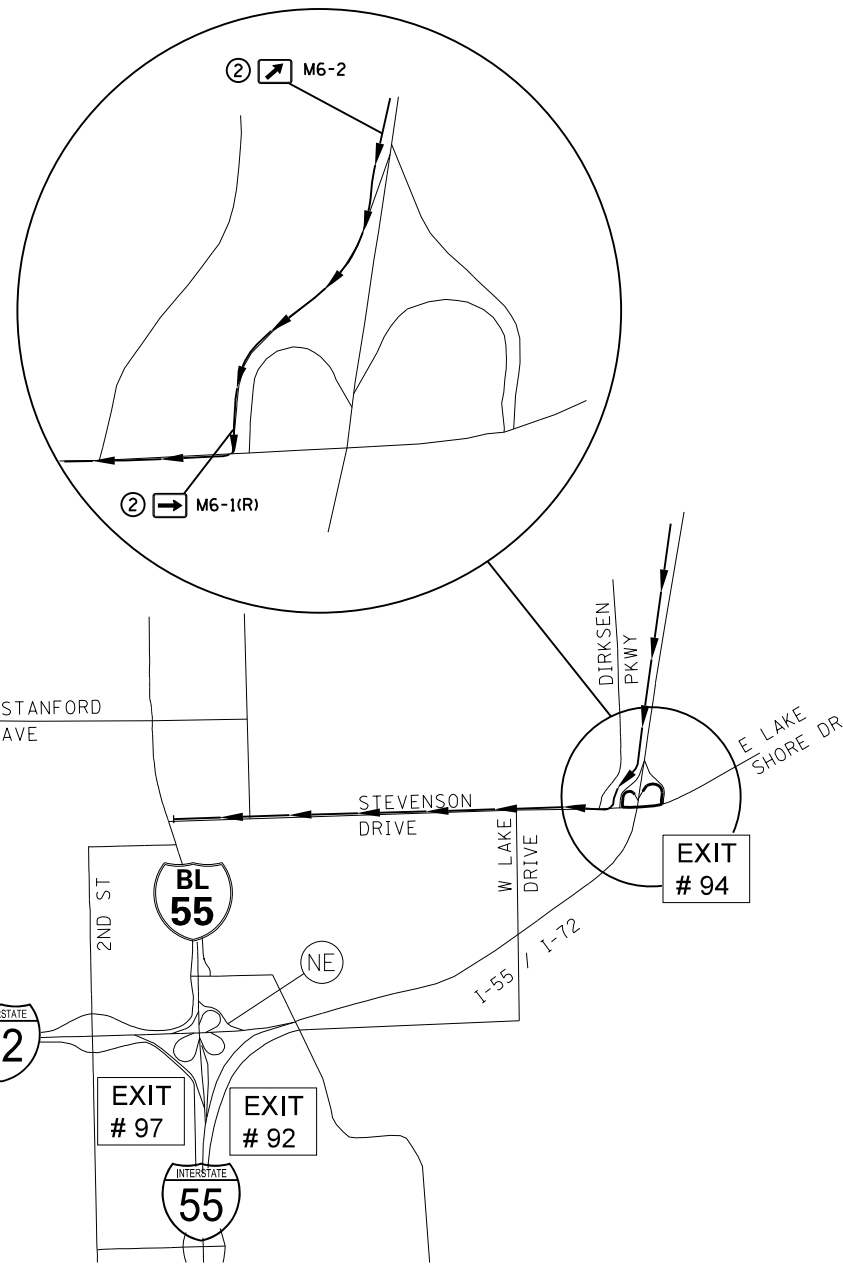
SCALE:	SHEET	OF	SHEETS	STA.	TO STA.
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F.A. * RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	**	SANGAMON	85	54
CONTRACT NO. 72J83				
ILLINOIS FED. AID PROJECT				

*F.A.I. 55, F.A.I. 72, F.A.P. 666 ** (84-4, 84-3, 84-9, 110) RS

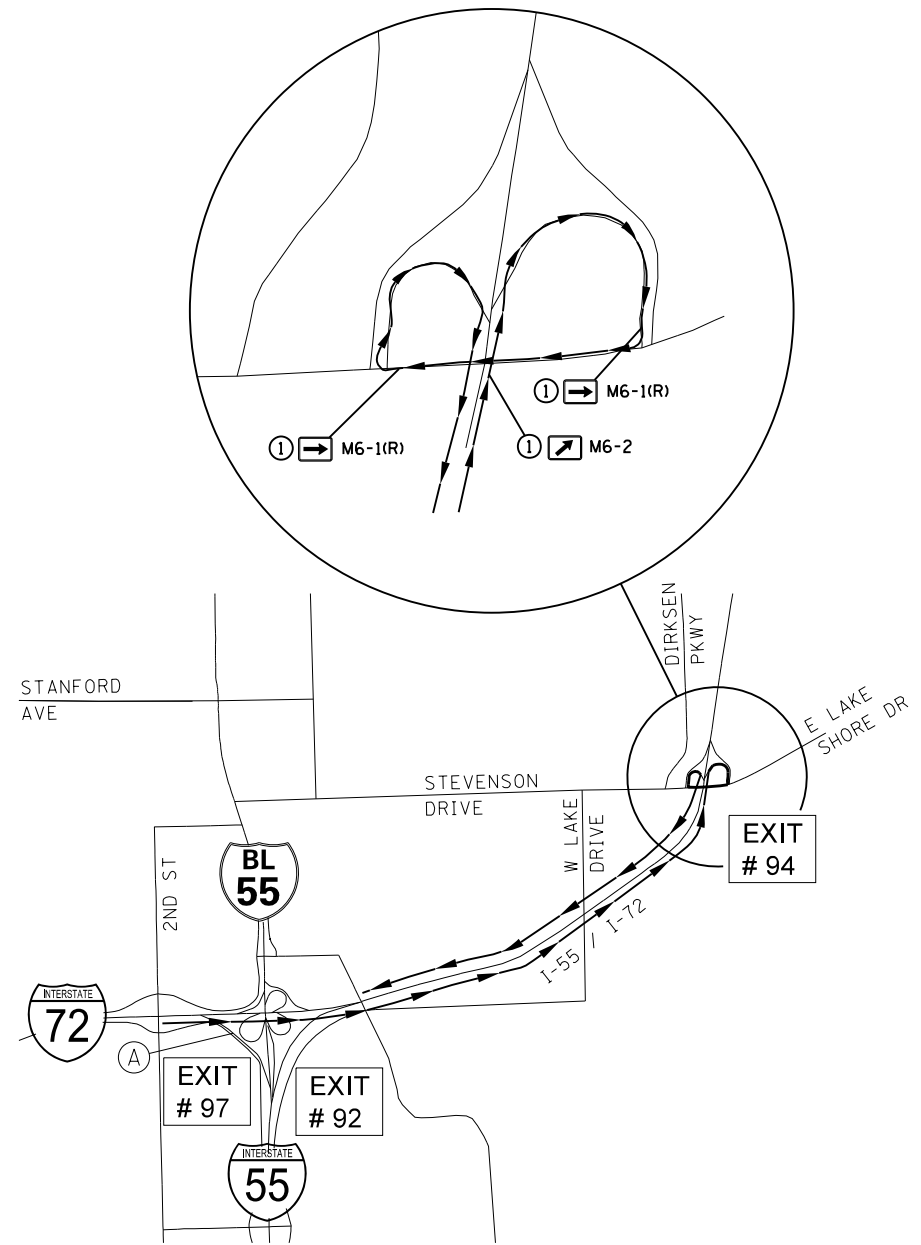
SB I-55 TO 6TH ST (BL-55)

EB I-72 TO SB I-55



USE FOR CLOSING SIXTH STREET INTERCHANGE NE RAMP

- ② ORANGE/BLACK M4-8 (30X15)
- MI-1 (48X48)
- ORANGE/BLACK (30X15)



USE FOR CLOSING SIXTH STREET INTERCHANGE RAMP A

- ① ORANGE/BLACK M4-8 (30X15)
- MI-1 (48X48)
- ORANGE/BLACK

FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -
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Default	DATE -	REVISED -	REVISED -
	PLOT DATE = 10/20/2017		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETOUR PLAN SHEET

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. * RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	**	SANGAMON	85	55
CONTRACT NO. 72J83				
		ILLINOIS	FED. AID PROJECT	

*F.A.I. 55, F.A.I. 72, F.A.P. 666 ** (84-4, 84-3, 84-9, 110) RS

I-55 SOUTHBOUND

EXIST. CURVE 312 PI STA. = 41+17.61 Δ = 35° 45' 56" (RT) D = 1° 52' 12" R = 3,064.16' T = 988.68' L = 1,912.73' E = 155.55' e = 2.3% T.R. = 50' S.E. RUN = 103.5' P.C. STA. = 31+28.93 P.T. STA. = 50+41.66	EXIST. CURVE 313 PI STA. = 57+22.27 Δ = 39° 13' 36" (RT) D = 2° 59' 59" R = 1,909.97' T = 680.61' L = 1,307.63' E = 117.64' e = 5.5% T.R. = 39' S.E. RUN = 247.5' P.C. STA. = 50+41.66 P.T. STA. = 63+49.29
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I-55 NORTHBOUND

EXIST. CURVE 302 PI STA. = 23+66.80 Δ = 10° 35' 36" (RT) D = 1° 04' 16" R = 5,349.67' T = 495.96' L = 989.09' E = 22.94' e = 3.0% T.R. = 48' S.E. RUN = 144' P.C. STA. = 18+70.84 P.T. STA. = 28+59.93	EXIST. CURVE 303 PI STA. = 35+40.78 Δ = 6° 46' 37" (RT) D = 0° 29' 54" R = 11,498.99' T = 680.85' L = 1,360.11' E = 20.14' e = 3.2% T.R. = N/A S.E. RUN = N/A P.C. STA. = 28+59.93 P.T. STA. = 42+20.04	EXIST. CURVE 304 PI STA. = 49+70.81 Δ = 42° 54' 18" (RT) D = 2° 59' 56" R = 1,910.58' T = 750.77' L = 1,430.71' E = 142.22' e = 5.8% T.R. = N/A S.E. RUN = N/A P.C. STA. = 42+20.04 P.T. STA. = 56+50.75	EXIST. CURVE 305 PI STA. = 69+81.99 Δ = 14° 19' 53" (RT) D = 0° 32' 28" R = 10,588.90' T = 1,331.24' L = 2,648.58' E = 83.35' e = 2.5% T.R. = 48' S.E. RUN = 112.5' P.C. STA. = 56+50.75 P.T. STA. = 82+99.33	EXIST. CURVE 306 PI STA. = 104+08.71 Δ = 20° 06' 45" (LT) D = 2° 00' 16" R = 2,858.36' T = 506.90' L = 1,003.37' E = 44.60' e = 4.6% T.R. = 40' S.E. RUN = 207' P.C. STA. = 99+01.81 P.T. STA. = 109+05.18	EXIST. CURVE 1154 PI STA. = 154+92.02 Δ = 23° 26' 35" (LT) D = 1° 59' 59" R = 2,865.16' T = 594.47' L = 1,172.31' E = 61.02' e = 4.4% T.R. = 41' S.E. RUN = 198' P.C. STA. = 148+97.55 P.T. STA. = 160+69.85	EXIST. CURVE 1166 PI STA. = 166+64.29 Δ = 23° 28' 36" (LT) D = 2° 00' 10" R = 2,860.82' T = 594.44' L = 1,172.20' E = 61.11' e = 3.2% T.R. = 44' S.E. RUN = 144' P.C. STA. = 160+69.85 P.T. STA. = 172+42.05
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I-72 EASTBOUND

EXIST. CURVE 321 PI STA. = 693+04.84 Δ = 20° 22' 59" (LT) D = 2° 29' 16" R = 2,302.98' T = 414.02' L = 819.29' E = 36.92' e = 4.9% T.R. = 40' S.E. RUN = 220.5' P.C. STA. = 688+90.82 P.T. STA. = 697+10.11	EXIST. CURVE 322 PI STA. = 705+75.64 Δ = 7° 31' 01" (RT) D = 0° 36' 31" R = 9,412.89' T = 618.36' L = 1,234.95' E = 20.29' e = 2.0% T.R. = 54' S.E. RUN = 90' P.C. STA. = 699+57.28 P.T. STA. = 711+92.22
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I-72 WESTBOUND

EXIST. CURVE 331 PI STA. = 685+31.89 Δ = 12° 09' 43" (LT) D = 1° 11' 59" R = 4,775.83' T = 508.78' L = 1,013.74' E = 27.02' e = 2.2% T.R. = 51' S.E. RUN = 99' P.C. STA. = 680+23.11 P.T. STA. = 690+36.85	EXIST. CURVE 332 PI STA. = 705+20.93 Δ = 0° 42' 09" (LT) D = 0° 08' 14" R = 41,758.11' T = 256.00' L = 511.99' E = 0.78' e = ----- P.C. STA. = 702+64.93 P.T. STA. = 707+76.93
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I-72 RAMP A

EXIST. CURVE 524 PI STA. = 24+93.07 Δ = 47° 20' 26" (RT) D = 9° 27' 37" R = 605.64' T = 265.48' L = 500.41' E = 55.63' e = ----- P.C. STA. = 22+27.59 P.T. STA. = 27+28.00
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I-55 BUS NORTHBOUND

EXIST. CURVE 371 PI STA. = 23+47.00 Δ = 11° 22' 09" (RT) D = 1° 00' 00" R = 5,730.07' T = 570.38' L = 1,137.01' E = 28.32' e = 3.0% T.R. = 45' S.E. RUN = 135' P.C. STA. = 17+76.62 P.T. STA. = 29+13.63	EXIST. CURVE 372 PI STA. = 35+96.32 Δ = 21° 59' 35" (LT) D = 3° 00' 18" R = 1,906.67' T = 370.50' L = 731.88' E = 35.66' e = 4.6% T.R. = 40' S.E. RUN = 207' P.C. STA. = 32+25.82 P.T. STA. = 39+57.70
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STEVENS DRIVE RAMP A

EXIST. CURVE 101 PI STA. = 14+59.88 Δ = 52° 00' 07" (LT) D = 25° 20' 45" R = 226.06' T = 110.26' L = 205.17' E = 25.46' e = ----- P.C. STA. = 13+49.62 P.T. STA. = 15+54.79	EXIST. CURVE 102 PI STA. = 8+92.63 Δ = 38° 21' 00" (RT) D = 8° 18' 53" R = 689.09' T = 239.63' L = 461.23' E = 40.48' e = ----- P.C. STA. = 6+53.00 P.T. STA. = 11+14.23
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STEVENS DRIVE RAMP B

EXIST. CURVE 115 PI STA. = 10+91.95 Δ = 7° 24' 58" (RT) D = 2° 54' 31" R = 1,969.78' T = 127.66' L = 254.96' E = 4.13' e = ----- P.C. STA. = 9+64.29 P.T. STA. = 12+19.26	EXIST. CURVE 116 PI STA. = 16+41.27 Δ = 17° 11' 19" (RT) D = 2° 03' 07" R = 2,792.32' T = 422.01' L = 837.69' E = 31.71' e = ----- P.C. STA. = 12+19.26 P.T. STA. = 20+56.95	EXIST. CURVE 117 PI STA. = 21+38.47 Δ = 3° 20' 22" (RT) D = 2° 02' 55" R = 2,796.64' T = 81.52' L = 163.00' E = 1.19' e = ----- P.C. STA. = 20+56.95 P.T. STA. = 22+19.95	EXIST. CURVE 118 PI STA. = 24+95.57 Δ = 11° 26' 34" (RT) D = 2° 04' 58" R = 2,750.95' T = 275.62' L = 549.41' E = 13.77' e = ----- P.C. STA. = 22+19.95 P.T. STA. = 27+69.36	EXIST. CURVE R103 PI STA. = 40+50.18 Δ = 175° 20' 31" (RT) D = 36° 57' 54" R = 155.00' T = 3,811.01' L = 474.35' E = 3,659.16' e = ----- P.C. STA. = 2+39.17 P.T. STA. = 7+13.52	EXIST. CURVE R104 PI STA. = 7+94.39 Δ = 14° 09' 44" (RT) D = 8° 48' 02" R = 651.04' T = 80.87' L = 160.92' E = 5.00' e = ----- P.C. STA. = 7+13.52 P.T. STA. = 8+74.44
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STEVENS DRIVE RAMP C

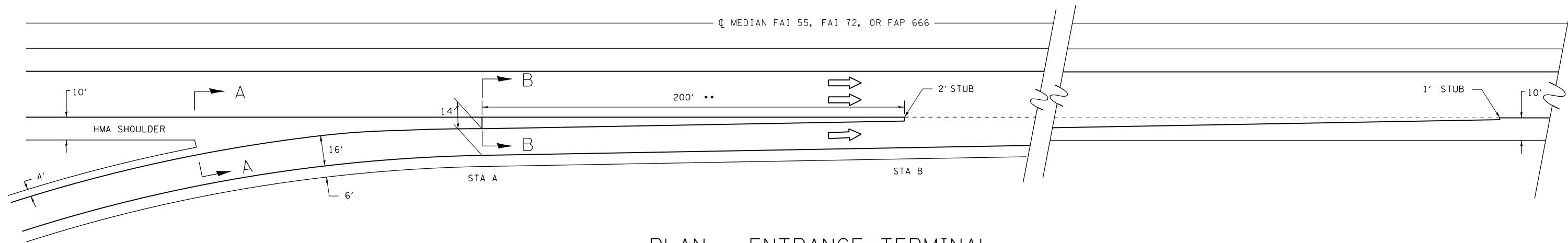
EXIST. CURVE R106 PI STA. = 10+21.21 Δ = 20° 33' 56" (RT) D = 13° 18' 34" R = 430.49' T = 78.10' L = 154.52' E = 7.03' e = ----- P.C. STA. = 9+43.11 P.T. STA. = 10+97.63	EXIST. CURVE R107 PI STA. = 20+57.82 Δ = 154° 43' 25" (RT) D = 26° 36' 43" R = 215.30' T = 960.19' L = 581.40' E = 768.73' e = ----- P.C. STA. = 10+97.63 P.T. STA. = 16+79.04	EXIST. CURVE 121 PI STA. = 1+37.96 Δ = 4° 34' 55" (LT) D = 1° 39' 41" R = 3,448.46' T = 137.96' L = 275.77' E = 2.76' e = ----- P.C. STA. = 0+00.00 P.T. STA. = 2+75.77	EXIST. CURVE 122 PI STA. = 4+48.16 Δ = 6° 44' 20" (LT) D = 1° 57' 25" R = 2,927.92' T = 172.38' L = 344.37' E = 5.07' e = ----- P.C. STA. = 2+75.77 P.T. STA. = 6+20.14
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STEVENS DRIVE RAMP D

EXIST. CURVE 108 PI STA. = 4+07.79 Δ = 58° 32' 56" (LT) D = 20° 14' 36" R = 283.04' T = 158.67' L = 289.23' E = 41.44' e = ----- P.C. STA. = 2+49.12 P.T. STA. = 5+38.35	EXIST. CURVE 109 PI STA. = 9+96.44 Δ = 57° 30' 21" (RT) D = 13° 18' 44" R = 430.40' T = 236.16' L = 431.98' E = 60.53' e = ----- P.C. STA. = 7+60.28 P.T. STA. = 11+92.26
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6TH STREET NE RAMP

EXIST. CURVE 341 PI STA. = 3+31.58 Δ = 5° 02' 15" (RT) D = 1° 30' 20" R = 3,805.84' T = 167.41' L = 334.61' E = 3.68' e = ----- P.C. STA. = 1+64.17 P.T. STA. = 4+98.78	EXIST. CURVE 342 PI STA. = 7+53.52 Δ = 14° 54' 58" (RT) D = 8° 17' 11" R = 691.44' T = 90.51' L = 180.00' E = 5.90' e = ----- P.C. STA. = 6+63.01 P.T. STA. = 8+43.01
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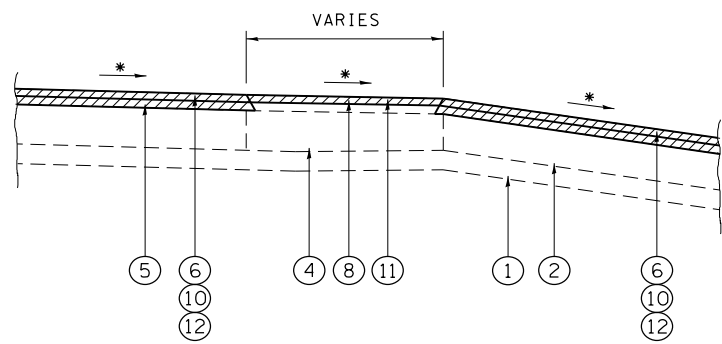


PLAN - ENTRANCE TERMINAL

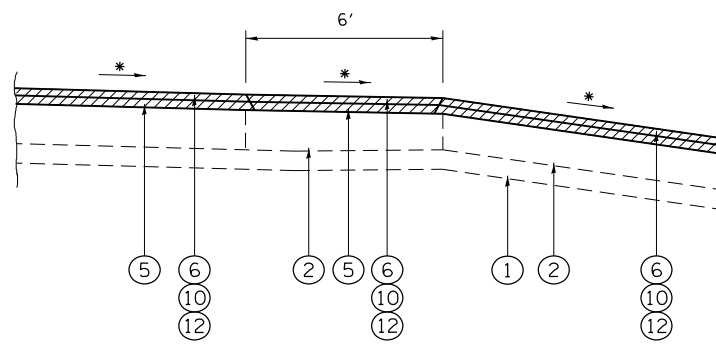
INTERCHANGE	RAMP	ADJACENT MAINLINE	STA A		STA B	
			RAMP	MAINLINE	RAMP	MAINLINE
SIXTH ST	A	SB BL-55	28+28	39+13	30+28	37+13
STEVENSON DR	B	SB I-55	8+46	173+10	10+48	171+04
STEVENSON DR	D	NB I-55	13+92	185+39	15+87	187+33

LEGEND

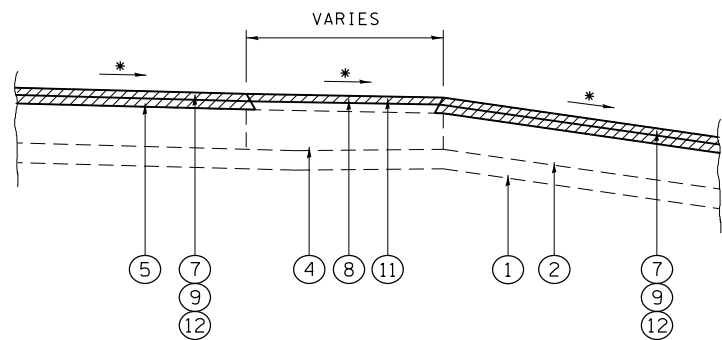
- 1 EX STABILIZED SUB-BASE
- 2 EX PCC BASE COURSE
- 3 EX PCC PAVEMENT
- 4 EX HOT-MIX ASPHALT SHOULDER
- 5 EX HOT-MIX ASPHALT
- 6 PR HOT-MIX ASPHALT REMOVAL, 6 1/4" AVG.
- 7 PR HOT-MIX ASPHALT REMOVAL, 3 3/4" AVG.
- 8 PR HOT-MIX ASPHALT REMOVAL, 2"
- 9 PR HOT-MIX ASPHALT BINDER 2 1/4"
- 10 PR HOT-MIX ASPHALT BINDER 4 3/4"
- 11 PR HOT-MIX ASPHALT SHOULDER 2"
- 12 PR HOT-MIX ASPHALT SURFACE 1 1/2"



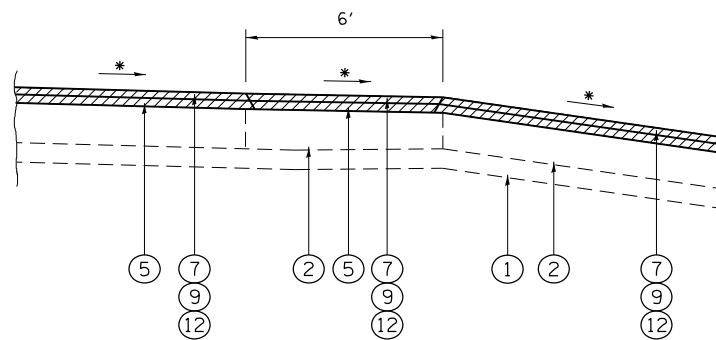
SECTION A-A
STEVENSON DR.



SECTION B-B
STEVENSON DR.



SECTION A-A
SIXTH ST.



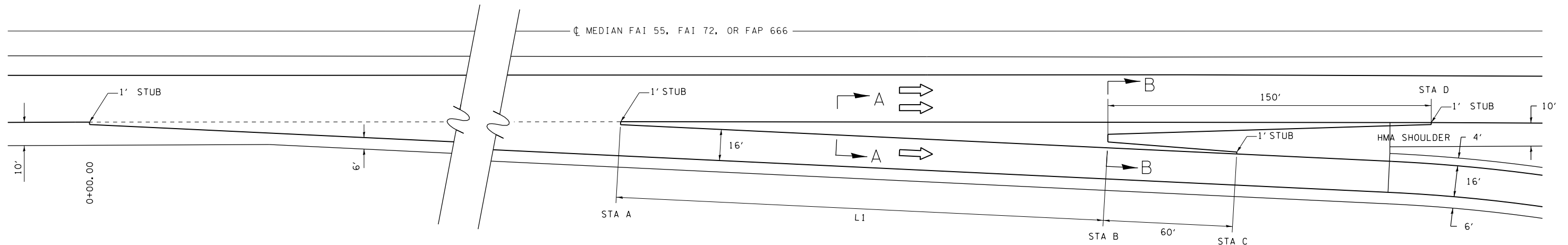
SECTION B-B
SIXTH ST.

NOTE:

I-55 SBDL AT STEVENSON DRIVE
HMA REMOVAL IS 1 1/2",
HMA SURFACE IS 1 1/2", AND NO BINDER

* MATCH EXIST SLOPE

STEVENSON DRIVE, RAMP B
STA. 12+19.26 TO STA. 27+69.36
HMA REMOVAL IS 1 1/2",
HMA SURFACE IS 1 1/2", AND NO BINDER

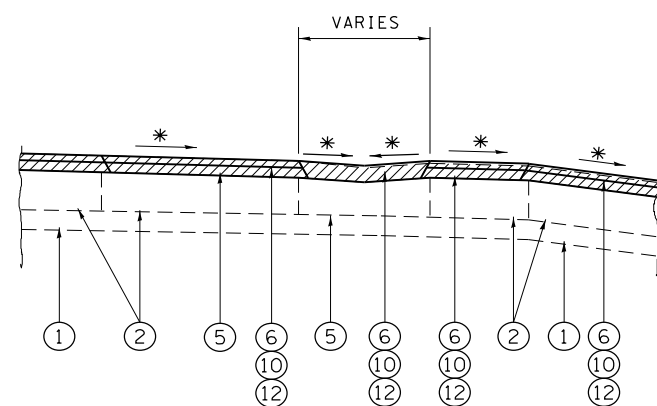


PLAN - EXIT TERMINAL

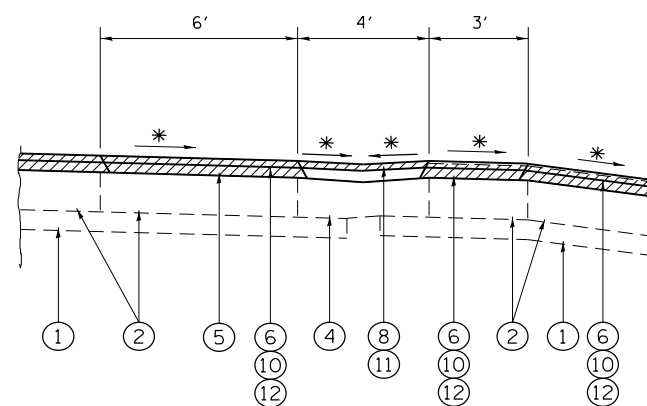
INTERCHANGE	RAMP	ADJACENT MAINLINE	STA A		STA B		STA C	STA D	L1 (FEET)
			RAMP	MAINLINE	RAMP	MAINLINE	RAMP	MAINLINE	
SIXTH ST	NE RAMP	WB I-72	3+03	688+03	5+30	685+75	5+90	684+25	227
STEVENSON DR	A	SB I-55	2+94	185+02	5+21	182+75	5+81	181+25	227
STEVENSON DR	C	NB I-55	7+20	173+43	8+67	174+90	9+27	176+40	147

LEGEND

- 1 EX STABILIZED SUB-BASE
- 2 EX PCC BASE COURSE
- 3 EX PCC PAVEMENT
- 4 EX HOT-MIX ASPHALT SHOULDER
- 5 EX HOT-MIX ASPHALT
- 6 PR HOT-MIX ASPHALT REMOVAL, 6 1/4" AVG.
- 7 PR HOT-MIX ASPHALT REMOVAL, 3 3/4" AVG.
- 8 PR HOT-MIX ASPHALT REMOVAL, 2"
- 9 PR HOT-MIX ASPHALT BINDER 2 1/4"
- 10 PR HOT-MIX ASPHALT BINDER 4 3/4"
- 11 PR HOT-MIX ASPHALT SHOULDER 2"
- 12 PR HOT-MIX ASPHALT SURFACE 1 1/2"



SECTION A-A



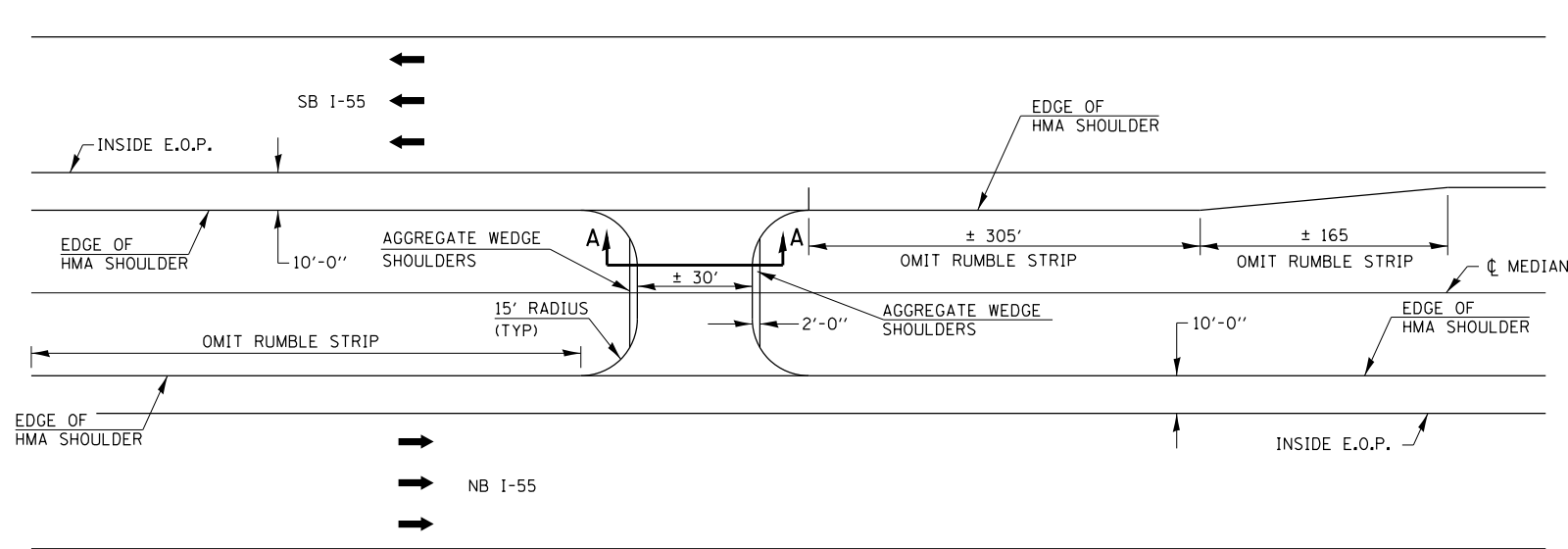
SECTION B-B

NOTE:
 I-55 SBDL AT STEVENSON DRIVE
 HMA REMOVAL IS 1 1/2",
 HMA SURFACE IS 1 1/2", AND NO BINDER

* MATCH EXIST SLOPE

STEVENSON DRIVE, RAMP C
 STA. 0+00 TO STA. 6+83.67
 HMA SURFACE REMOVAL IS 1 1/2",
 HMA SURFACE IS 1 1/2", AND NO BINDER

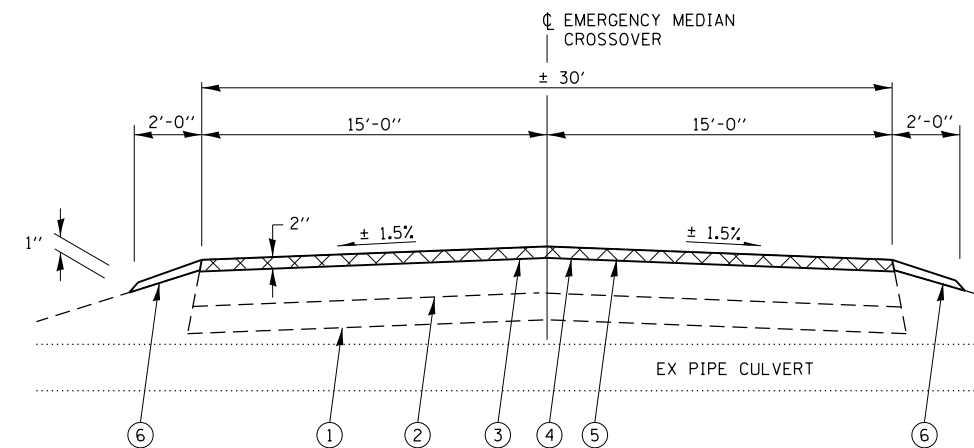
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pw:\IL084EB\INTEG\Illinois.gov\PWIDOT\Documents\DOT Offices\District 6\Projects\D672J83\CADData\CADsheets\D672J83\Drawings.dwg		REVISIONS	REVISIONS		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	ILLINOIS	FED. AID PROJECT	
Default	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISIONS								SANGAMON	85	58
	PLOT DATE = 10/20/2017	DATE -	REVISIONS								CONTRACT NO. 72J83		



**EMERGENCY MEDIAN CROSSOVER
I-55 STA. 15+55
PLAN VIEW**

EMERGENCY CROSSOVER IMPROVEMENT STA. 15+55

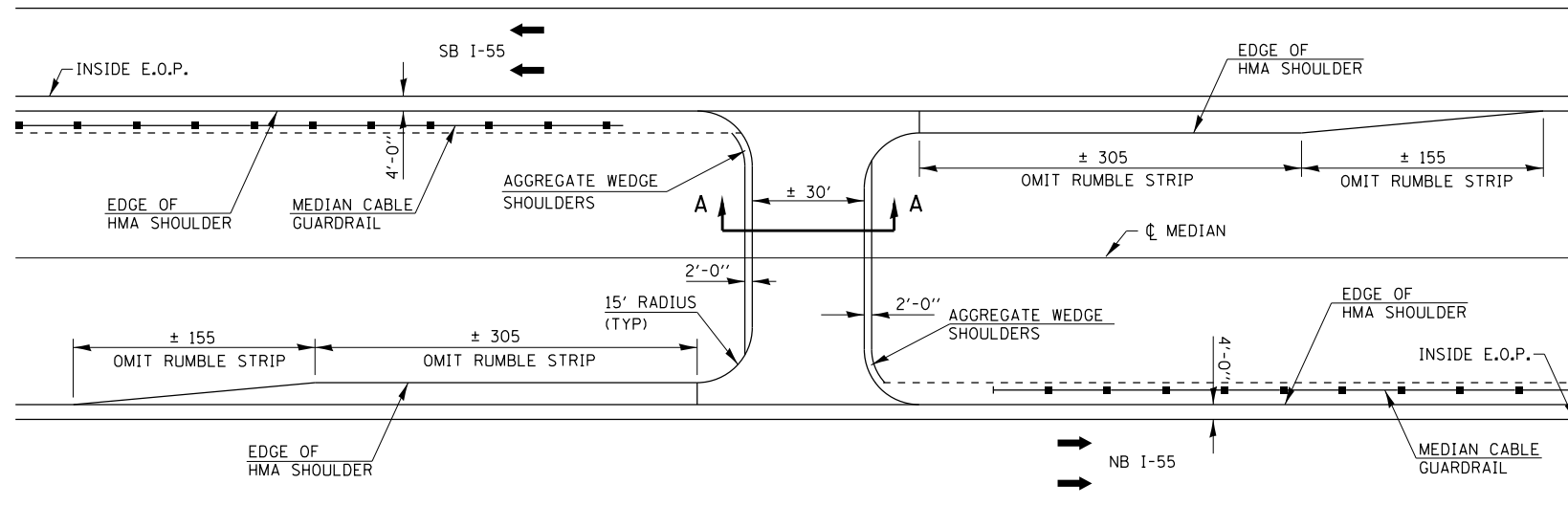
LOCATION	HMA SURF	HMA SURF CSE	AGG WEDGE SHLDRS.
	REM 2"	MIX C N50	TYPE B
SO YD	TON	TON	TON
I-55 STA 15+55	90.0	10.08	2.50
TOTAL	90.0	10.08	2.50



SECTION A - A

LEGEND

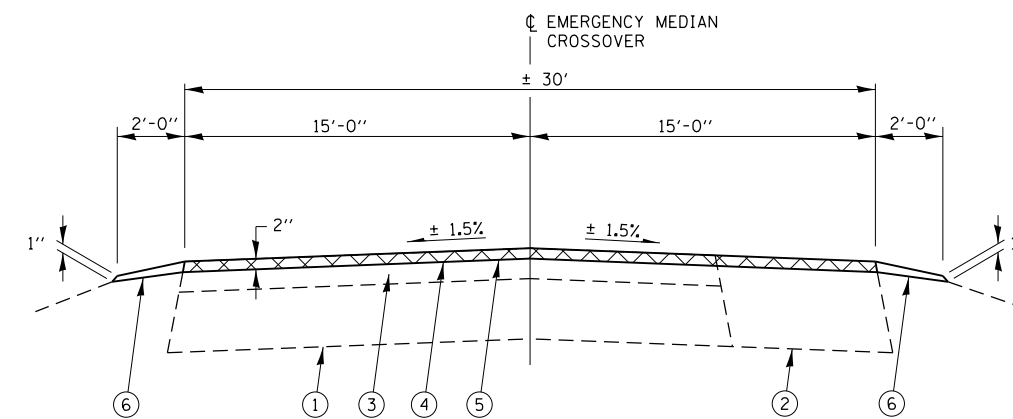
- ① EX AGGREGATE BASE COURSE
- ② EX HOT-MIX ASPHALT BASE COURSE, 8"
- ③ EX HOT-MIX ASPHALT OVERLAY
- ④ PR HOT-MIX ASPHALT SURFACE REMOVAL 2"
- ⑤ PR HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 2"
- ⑥ PR AGGREGATE WEDGE SHOULDERS



**EMERGENCY MEDIAN CROSSOVER
I-55 STA. 117+45
PLAN VIEW**

EMERGENCY CROSSOVER IMPROVEMENT STA. 117+45

LOCATION	HMA SURF	HMA SURF CSE	AGG WEDGE SHLDRS.
	REM 2"	MIX C N50	TYPE B
SO YD	TON	TON	TON
I-55 STA 117+45	142.1	26.58	2.50
TOTAL	142.1	26.58	2.50



SECTION A - A

LEGEND

- ① EX AGGREGATE BASE COURSE
- ② EX HOT-MIX ASPHALT BASE COURSE, 8"
- ③ EX HOT-MIX ASPHALT OVERLAY
- ④ PR HOT-MIX ASPHALT SURFACE REMOVAL 2"
- ⑤ PR HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 2"
- ⑥ PR AGGREGATE WEDGE SHOULDERS

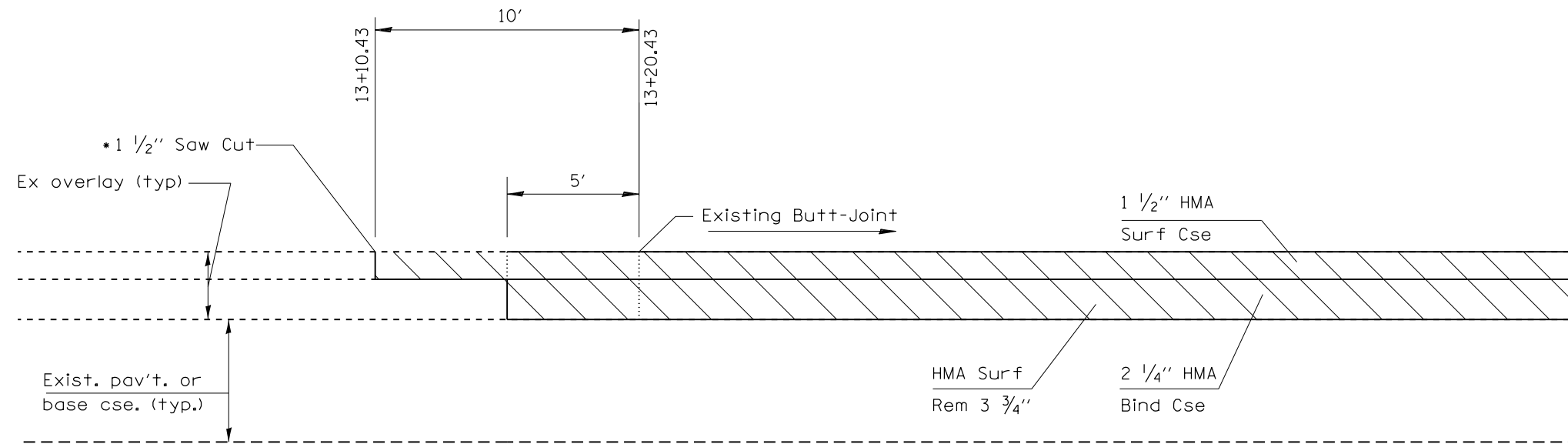
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 10/20/2017	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

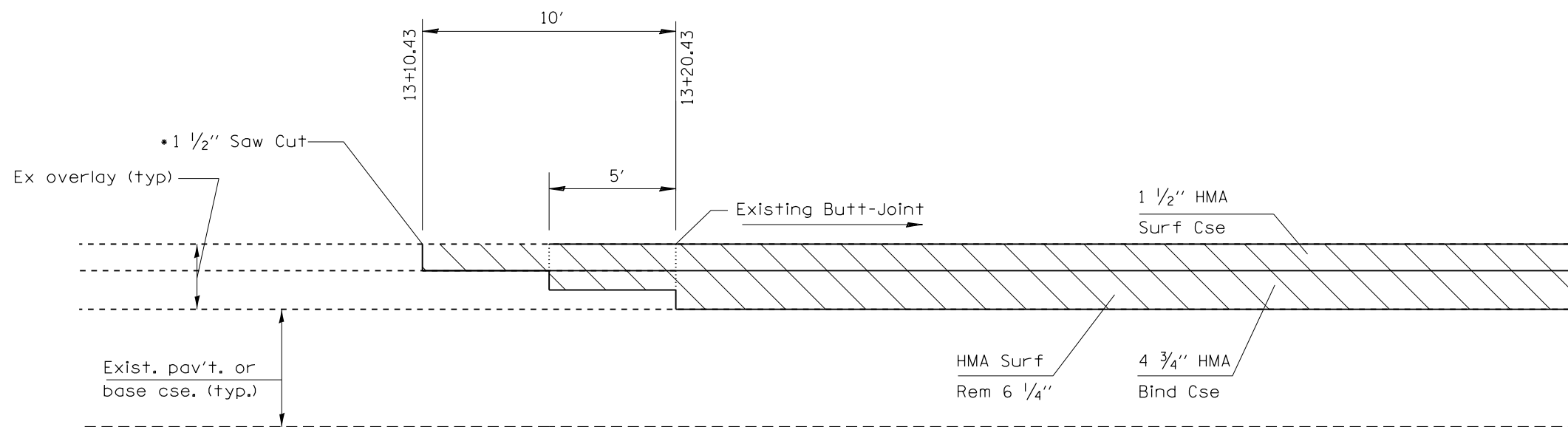
EMERGENCY CROSS OVER DETAIL

SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	F.A. * RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
							**	SANGAMON	85	59
								CONTRACT NO. 72J83		
								ILLINOIS FED. AID PROJECT		

*F.A.I. 55, F.A.I. 72, F.A.P. 666 ** (84-4, 84-3, 84-9, 110) RS



I-55 SOUTHBOUND
 STA 13+10.43 TO STA. 13+20.43

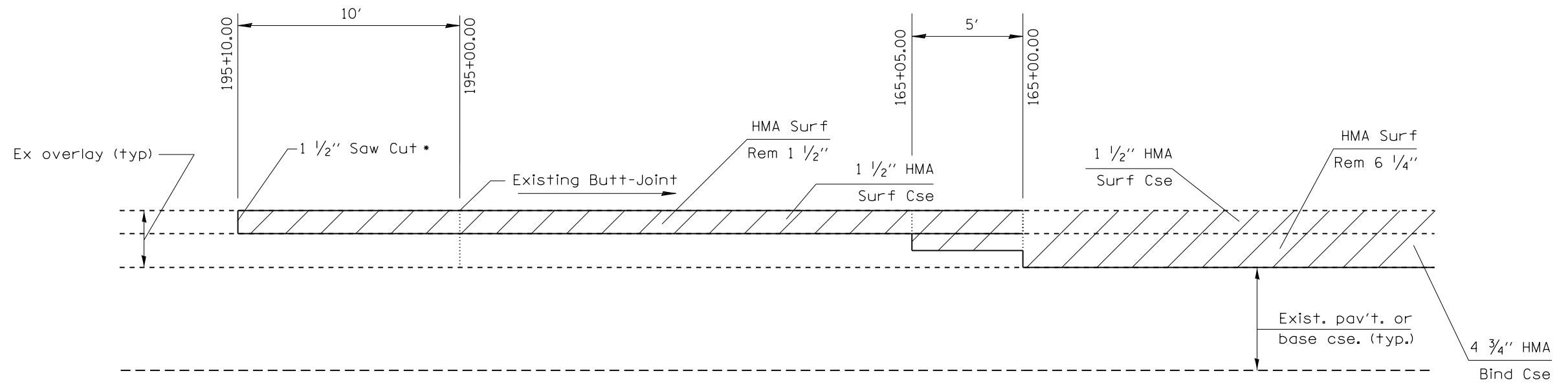


I-55 NORTHBOUND
 STA 13+10.43 TO STA. 13+20.43

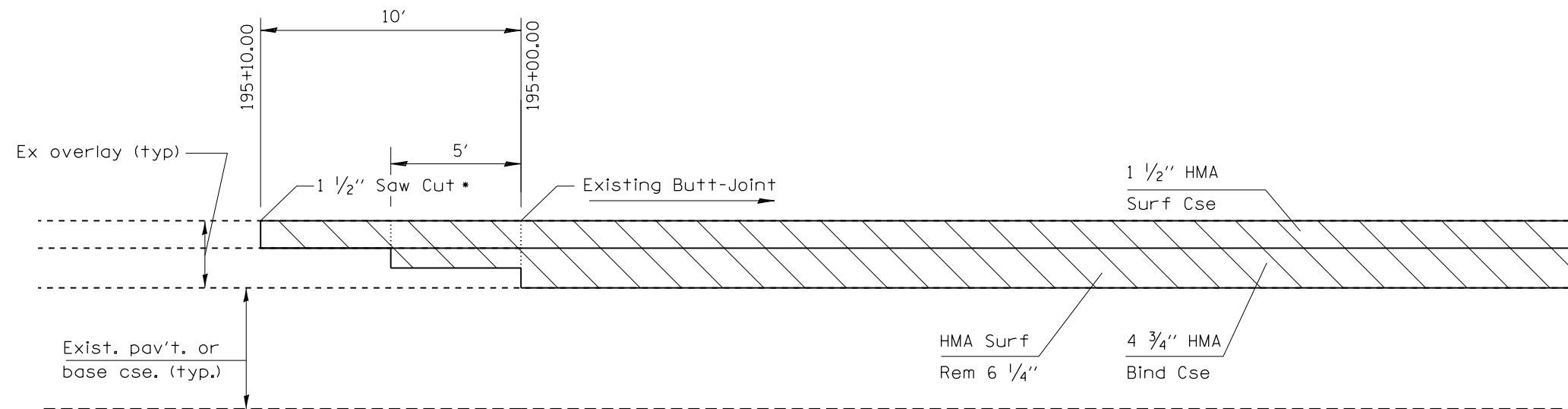
* ALL SAW CUTS ARE INCLUDED IN THE COST OF HMA SURFACE REMOVAL

FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT-JOINT AND TRANSITION DETAILS			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL04EB\INTEG\Illinois.gov\PWIDOT\Documents\DOT Offices\District 6\Projects\0672\83\CADD\Drawings\CADsheets\0672\83\0672-83-0672.dwg	DATE	CHECKED -	REVISED -					*	**	SANGAMON	85	60
Default	PLOT SCALE = 100.0000' / in.	DATE	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	CONTRACT NO. 72J83	
	PLOT DATE = 10/20/2017	DATE	REVISED -								ILLINOIS FED. AID PROJECT	

*F.A.I. 55, F.A.I. 72, F.A.P. 666 ** (84-4, 84-3, 84-9, 110) RS



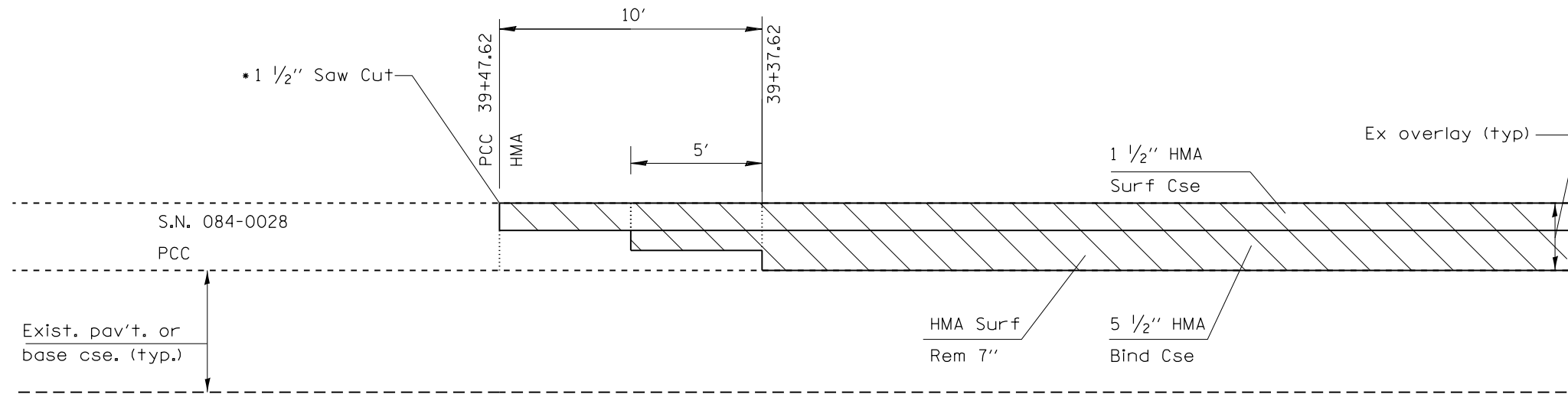
I-55 SOUTHBOUND DRIVING LANE
STA. 165+00.00 TO STA. 195+10.00



I-55 NORTHBOUND & SOUTHBOUND PASSING LANE
STA. 195+00.00 TO STA. 195+10.00

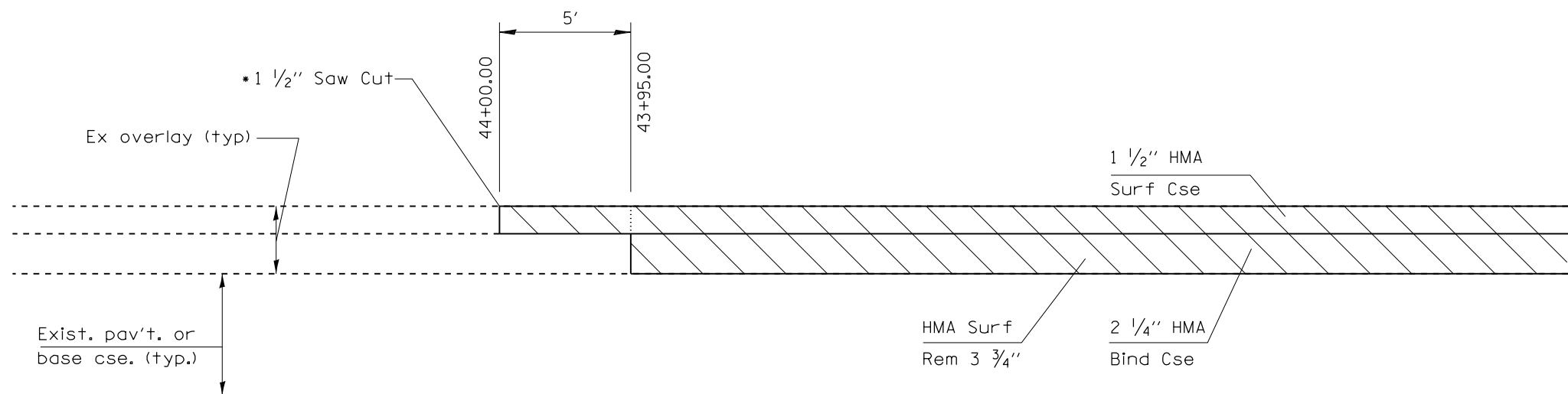
* ALL SAW CUTS ARE INCLUDED IN
THE COST OF HMA SURFACE REMOVAL

FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT-JOINT AND TRANSITION DETAILS			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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Default	PLOT DATE = 10/20/2017	DATE -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.			ILLINOIS FED. AID PROJECT				
								CONTRACT NO. 72J83				



I-55 BUSINESS LOOP NORTHBOUND

STA. 39+37.62 TO STA 39+47.62

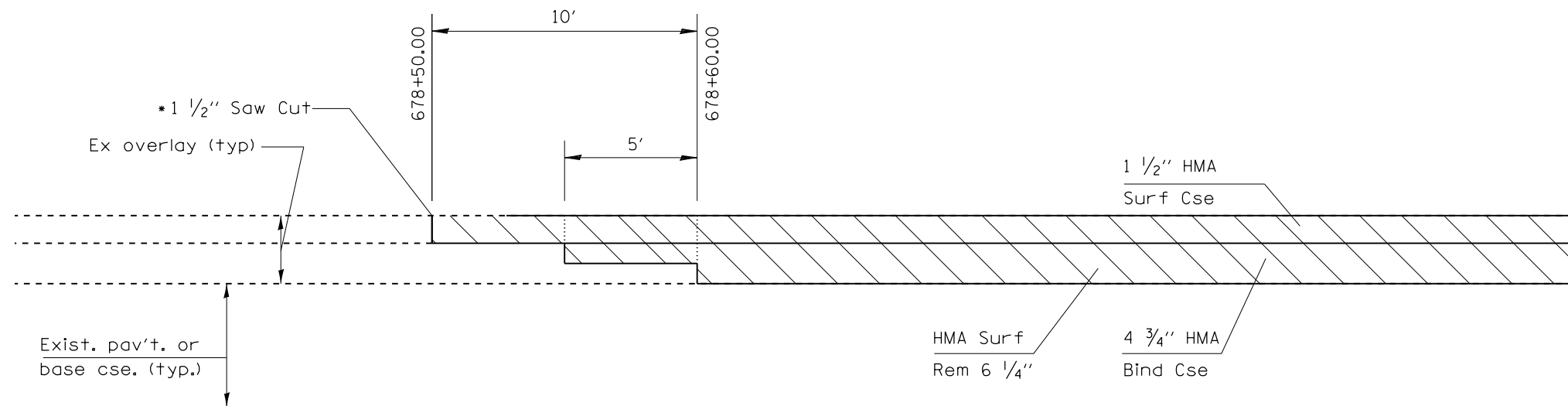


I-55 BUSINESS LOOP SOUTHBOUND

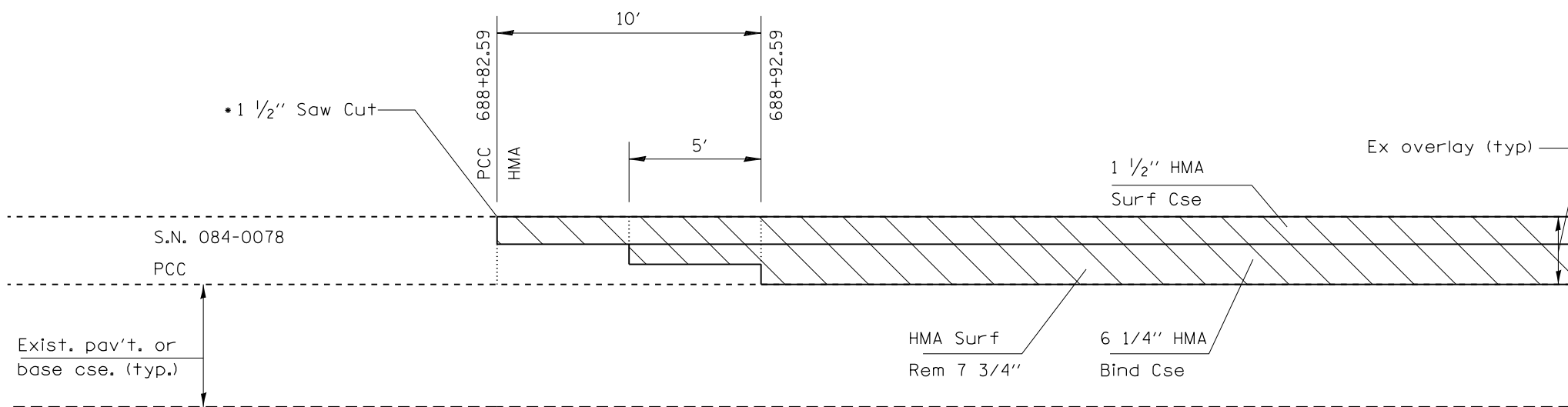
STA. 43+95.00 TO STA 44+00.00

* ALL SAW CUTS ARE INCLUDED IN THE COST OF HMA SURFACE REMOVAL

FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT-JOINT AND TRANSITION DETAILS			F.A. * RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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Default	PLT DATE = 10/20/2017	DATE -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.			ILLINOIS FED. AID PROJECT		CONTRACT NO. 72J83		
					*F.A.I. 55, F.A.I. 72, F.A.P. 666 ** (84-4, 84-3, 84-9, 110) RS							



I-72 WESTBOUND
STA. 678+50.00 TO STA. 678+60.00

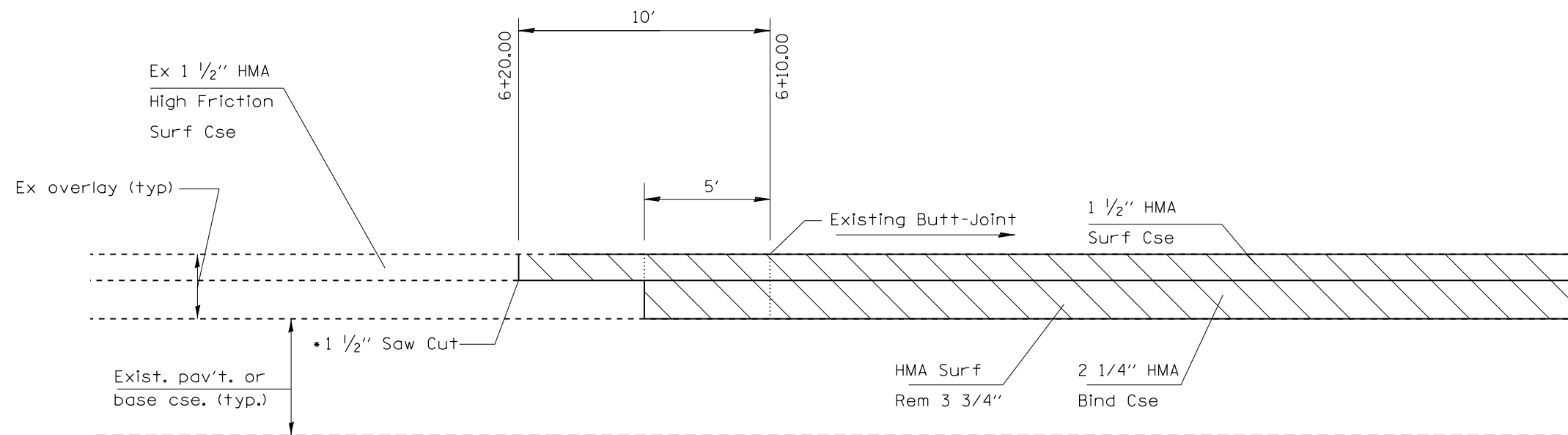


I-72 EASTBOUND
STA. 688+82.59 TO STA. 688+92.59

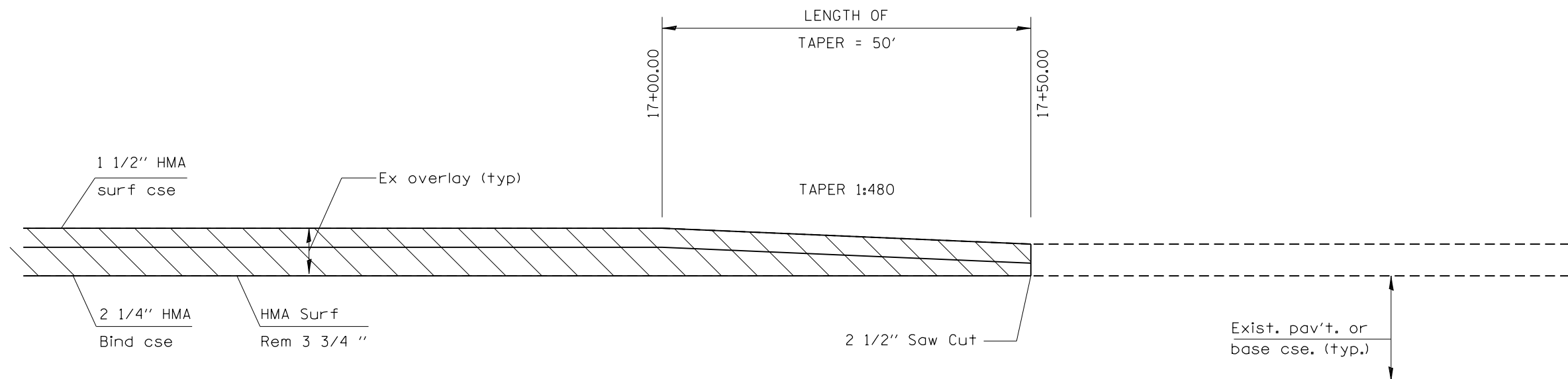
* ALL SAW CUTS ARE INCLUDED IN THE COST OF HMA SURFACE REMOVAL

FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT-JOINT AND TRANSITION DETAILS			F.A. * RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL04EB\INTEG\Illinois.gov\PWIDOT\Documents\DOT Offices\District 6\Projects\0672\83\CADData\CADsheets\0672\83\0672-83-0672.dwg	PLotted	CHECKED -	REVISED -					*	**	SANGAMON	85	63
Default	PLOT SCALE = 100,0000' / in.	DATE -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 72J83				
	PLOT DATE = 10/20/2017							ILLINOIS FED. AID PROJECT				

*F.A.I. 55, F.A.I. 72, F.A.P. 666 ** (84-4, 84-3, 84-9, 110) RS



6TH ST. INTERCHANGE
NE RAMP STA 6+00.00 TO STA. 6+20.00

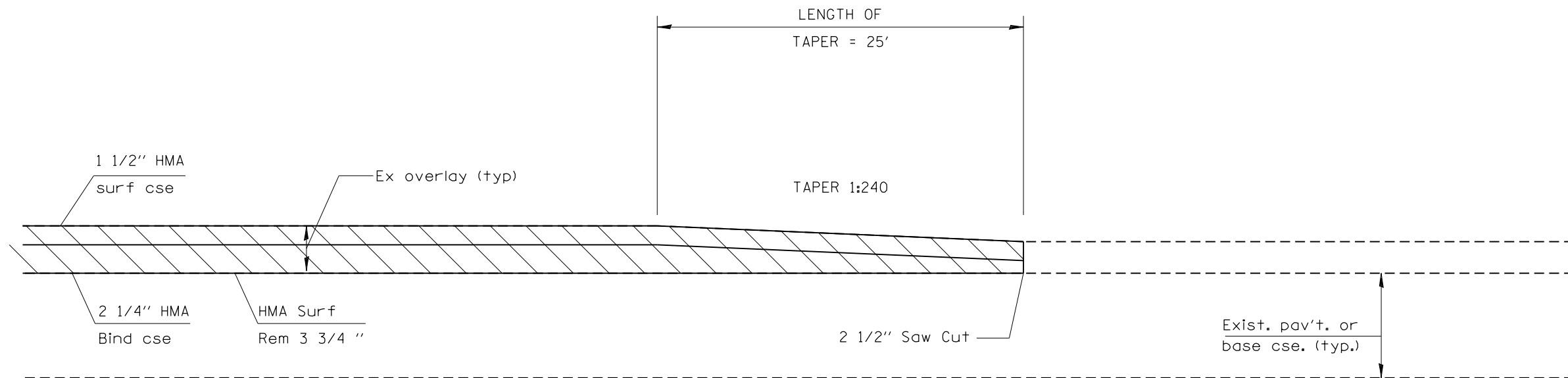


6TH ST. INTERCHANGE
RAMP A STA 17+00.00 TO STA. 17+50.00

* ALL SAW CUTS ARE INCLUDED IN THE COST OF HMA SURFACE REMOVAL

FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT-JOINT AND TRANSITION DETAILS			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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Default	PLOT SCALE = 100.0000 "/in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 72J83				
	PLOT DATE = 10/20/2017	DATE -	REVISED -					ILLINOIS FED. AID PROJECT				

*F.A.I. 55, F.A.I. 72, F.A.P. 666 ** (84-4, 84-3, 84-9, 110) RS



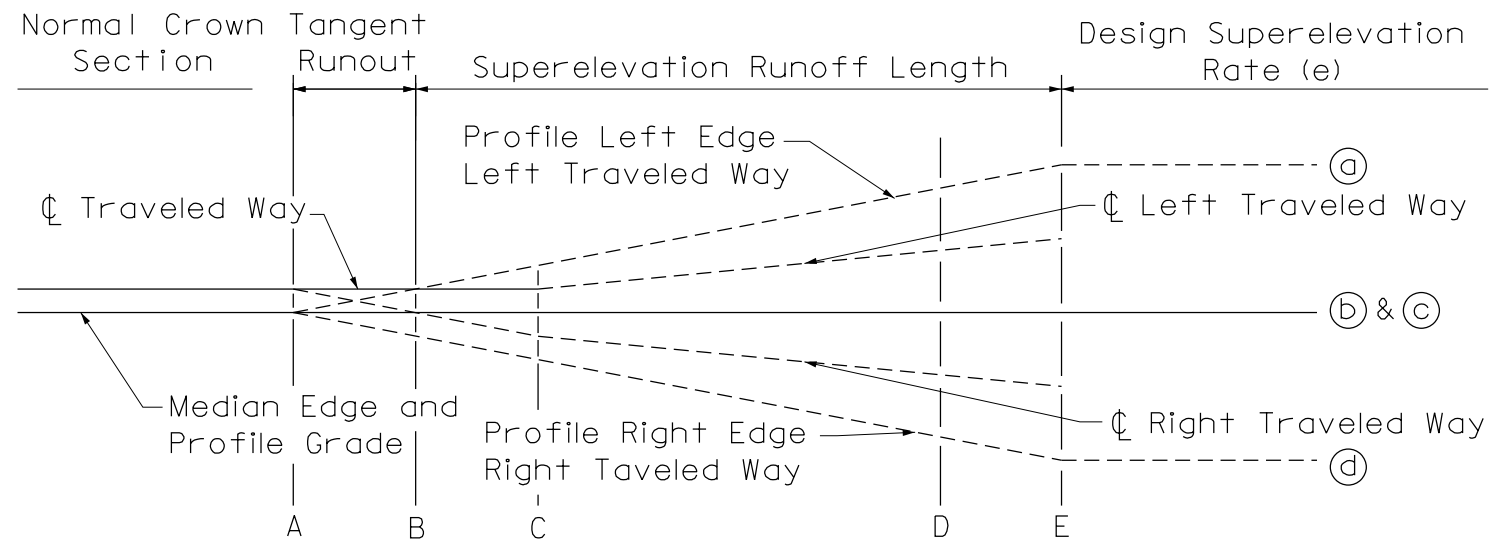
STEVENSON DRIVE INTERCHANGE

- RAMP A STA 16+57.00 TO STA. 16+82.00
- RAMP B STA 1+16.00 TO STA. 1+41.00
- RAMP C STA 18+02.00 TO STA. 18+27.00
- RAMP D STA 0+98.00 TO STA. 1+23.00

* ALL SAW CUTS ARE INCLUDED IN THE COST OF HMA SURFACE REMOVAL

FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT-JOINT AND TRANSITION DETAILS			F.A. * RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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Default	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 72J83				
	PLOT DATE = 10/20/2017	DATE -	REVISED -					ILLINOIS FED. AID PROJECT				

*F.A.I. 55, F.A.I. 72, F.A.P. 666 **(84-4, 84-3, 84-9, 110) RS



AXIS OF ROTATION ABOUT MEDIAN EDGES OF TRAVELED WAY

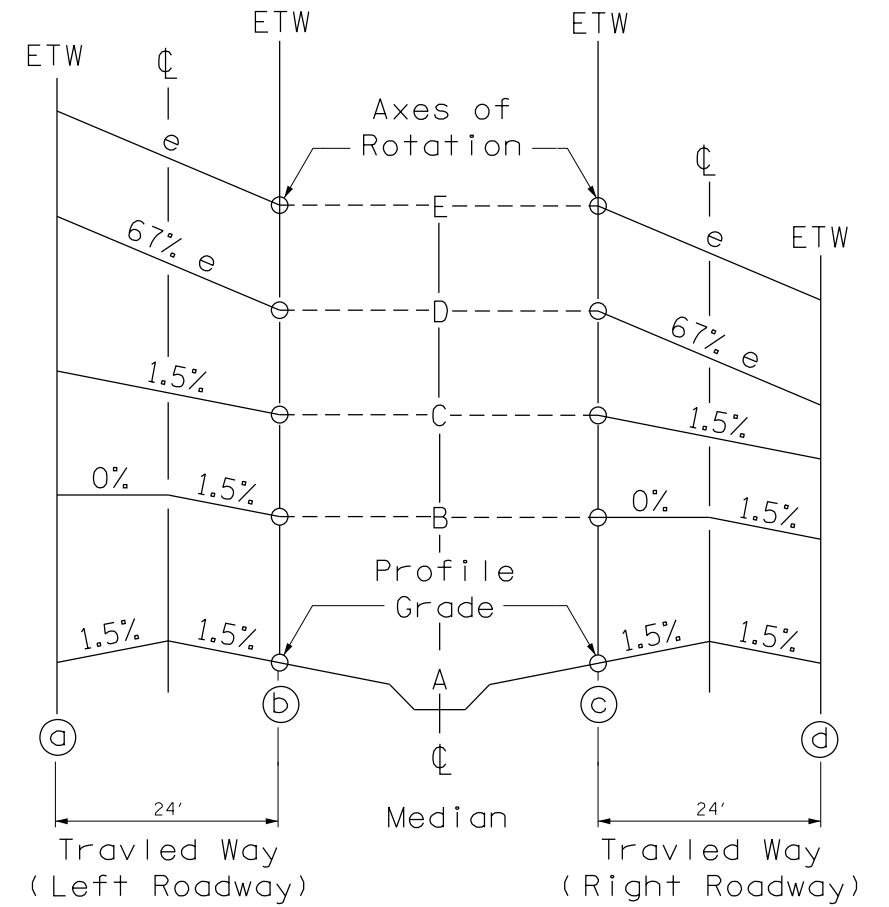
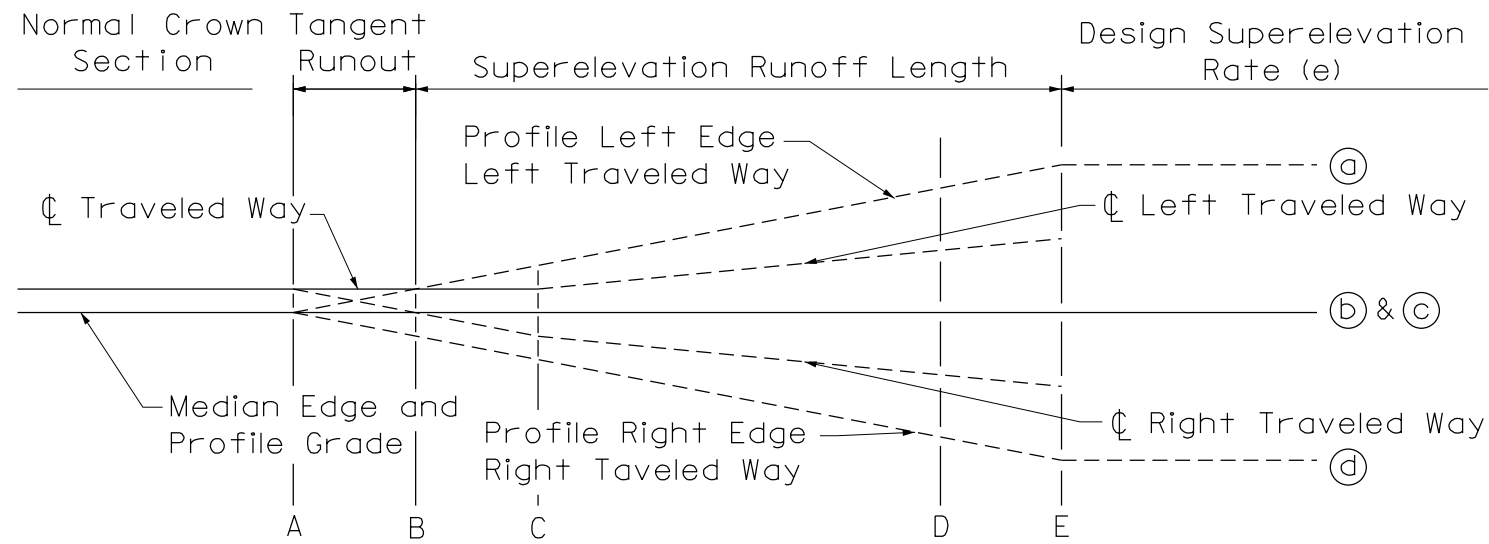


TABLE OF SUPERELEVATION BREAK POINT LOCATIONS							
CURVE NO.	e	A	B	C	D	E	TRANSITION
302	3.0%	17+26.84	17+74.84	18+22.84	18+70.84	19+18.84	TRANS. IN
		N/A	N/A	N/A	N/A	28+11.93	TRANS. OUT
303	3.3%	N/A	N/A	N/A	N/A	28+59.93	TRANS. IN
		N/A	N/A	N/A	N/A	40+91.00	TRANS. OUT
304	5.8%	N/A	N/A	N/A	N/A	42+20.04	TRANS. IN
		N/A	N/A	N/A	N/A	55+56.08	TRANS. OUT
305	2.5%	N/A	N/A	N/A	N/A	57+35.00	TRANS. IN
		84+22.00	83+77.33	83+26.00	82+99.33	82+60.33	TRANS. OUT
306	4.6%	97+23.81	97+63.81	98+03.81	99+01.81	99+70.81	TRANS. IN
		110+83.18	110+43.18	110+03.18	109+05.18	108+36.18	TRANS. OUT
312	2.3%	30+10.00	30+60.00	31+10.00	31+28.93	31+63.00	TRANS. IN
		N/A	N/A	N/A	N/A	49+58.00	TRANS. OUT
313	5.5%	N/A	N/A	N/A	N/A	50+41.66	TRANS. IN
		65+52.29	65+14.00	64+75.00	63+49.29	62+65.29	TRANS. OUT
1154	4.4%	147+24.55	147+65.55	148+06.55	148+97.55	149+63.55	TRANS. IN
		N/A	N/A	N/A	N/A	160+03.85	TRANS. OUT
1166	3.2%	N/A	N/A	N/A	N/A	160+69.85	TRANS. IN
		173+82.13	173+38.05	172+93.97	172+42.05	171+94.05	TRANS. OUT

*MATCH EXISTING SUPERELEVATIONS



AXIS OF ROTATION ABOUT MEDIAN EDGES OF TRAVELED WAY

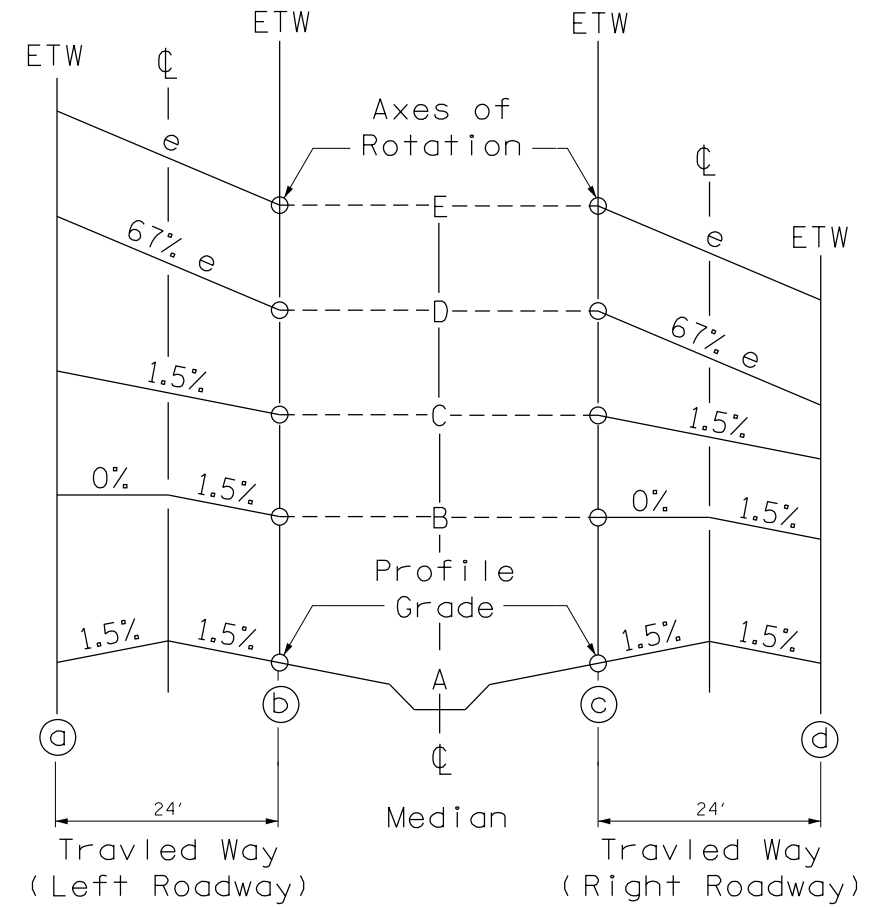
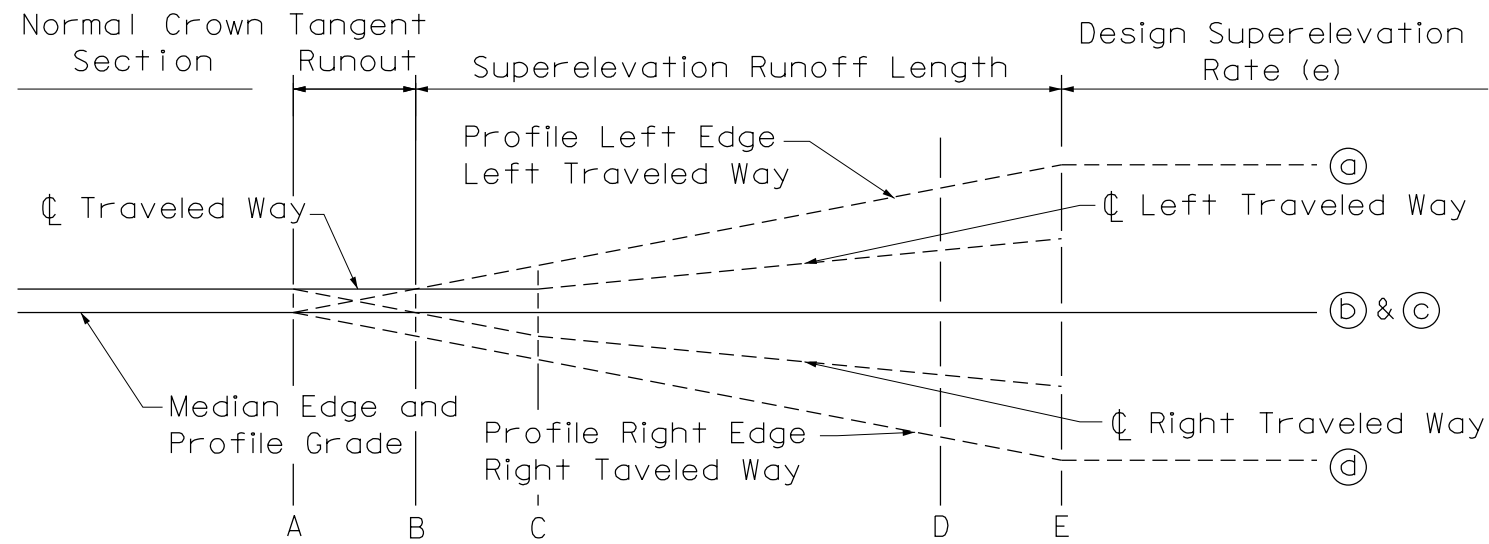


TABLE OF SUPERELEVATION BREAK POINT LOCATIONS							
CURVE NO.	e	A	B	C	D	E	TRANSITION
321	4.9%	687+03.49	687+43.49	687+83.49	688+90.82	689+64.49	TRANS. IN
		N/A	N/A	697+97.00	696+90.00	696+16.00	TRANS. OUT
322	2.0%	N/A	N/A	699+57.00	699+64.00	699+93.00	TRANS. IN
		713+06.22	712+52.22	711+98.22	711+92.22	711+62.22	TRANS. OUT
331	2.2%	679+06.11	679+57.11	680+08.11	680+23.11	680+56.11	TRANS. IN
		691+53.85	691+02.85	690+51.85	690+36.85	690+03.85	TRANS. OUT

*MATCH EXISTING SUPERELEVATIONS



AXIS OF ROTATION ABOUT MEDIAN EDGES OF TRAVELED WAY

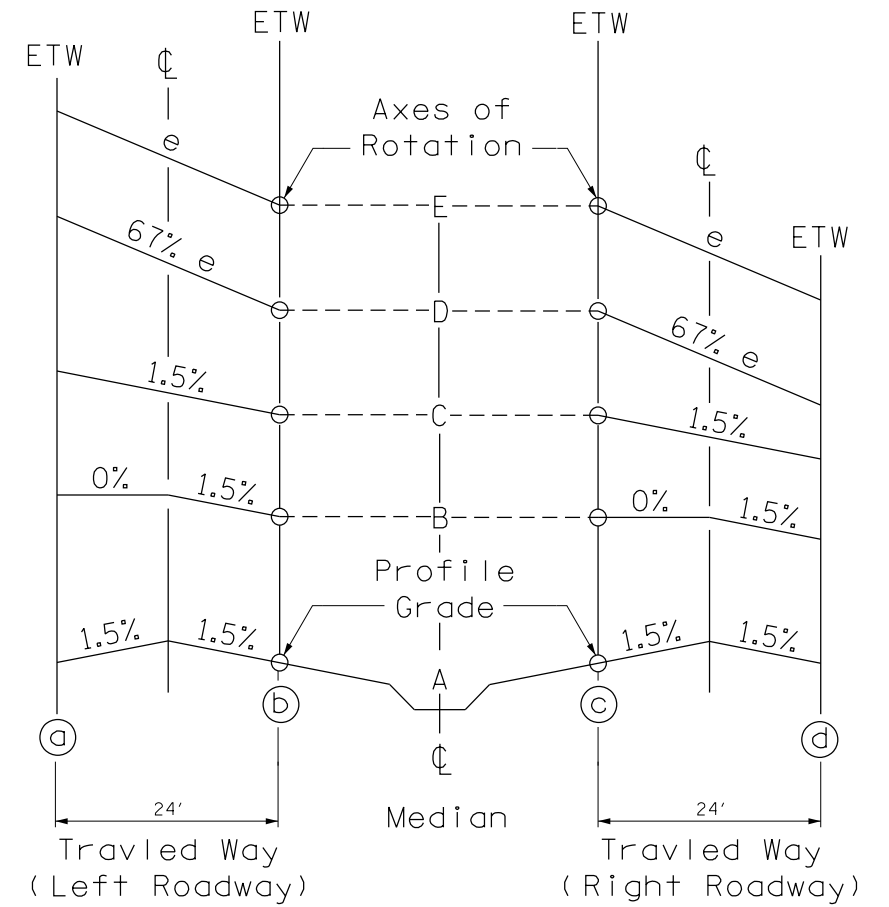
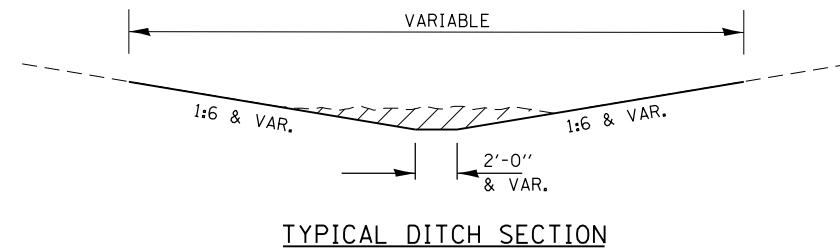


TABLE OF SUPERELEVATION BREAK POINT LOCATIONS							
CURVE NO.	e	A	B	C	D	E	TRANSITION
371	3.0%	16+41.62	16+86.62	17+31.62	17+76.62	18+21.62	TRANS. IN
		30+47.50	30+03.63	29+58.63	29+13.63	28+68.63	TRANS. OUT
372	4.6%	30+47.50	30+87.82	31+28.14	32+25.82	32+94.82	TRANS. IN
		41+36.02	40+95.70	40+55.38	39+57.70	38+88.70	TRANS. OUT

*MATCH EXISTING SUPERELEVATIONS



SURPLUS MATERIAL SHALL BE DISPOSED OF OUTSIDE THE RIGHT OF WAY AS SPECIFIED IN ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS.

EROSION CONTROL MEASURES SHALL BE IMPLEMENTED AS FOLLOWS:

TEMPORARY DITCH CHECKS AT 200 FOOT CENTERS BEGINNING AT THE DOWNSTREAM END OF THE DISTURBED AREA.

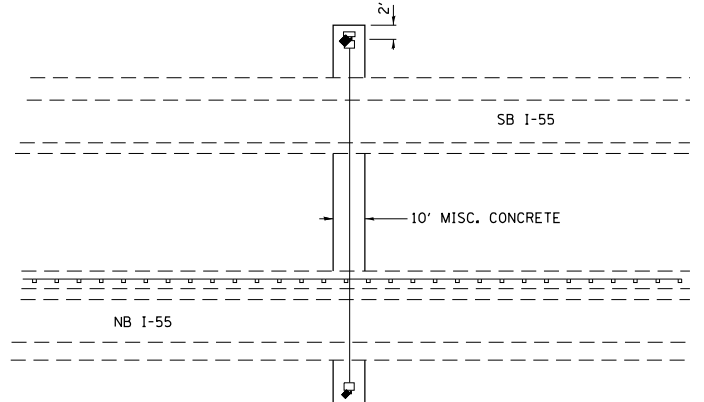
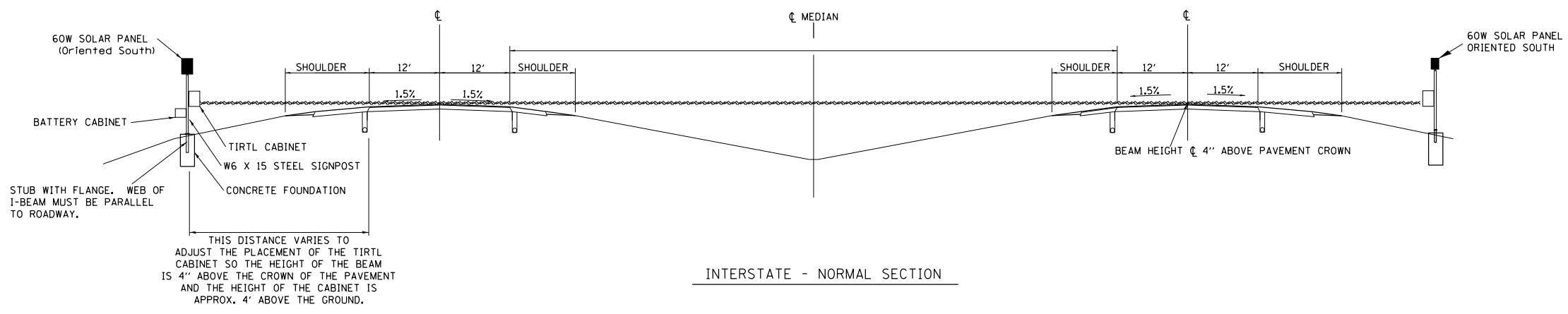
INLET PROTECTION AT THE UPSTREAM END OF ALL PIPE CULVERTS AND BOX CULVERTS RECEIVING FLOW FROM THE RE-GRADED DITCH.

TEMPORARY SEEDING AS REQUIRED BY THE SPECIAL PROVISION FOR TEMPORARY EROSION CONTROL SEEDING.

SEEDING AND INLET & PIPE PROTECTION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF GRADING & SHAPING DITCHES.

LOCATION				GRADING & SHAPING DITCHES (FOOT)	TEMPORARY DITCH CHECKS (FOOT)
I-55 MAINLINE					
STA 83+78	TO	STA 88+98	LT	520	26.0
STA 124+05	TO	STA 126+35	RT	230	11.5
STA 127+95	TO	STA 129+95	RT	200	10.0
STA 128+19	TO	STA 129+79	LT	160	8.0
STEVENSON DRIVE RAMPS					
RAMP B					
STA 1+16	TO	STA 4+76	LT	360	18
STA 6+55	TO	STA 9+05	RT	250	12.5
STA 10+56	TO	STA 14+66	RT	410	20.5
STA 15+40	TO	STA 17+40	RT	200	10.0
RAMP C					
STA 1+15	TO	STA 5+35	RT	420	21
STA 6+77	TO	STA 8+97	RT	220	11
STA 16+47	TO	STA 18+07	LT	160	8
TOTAL				3,130	156.5

FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GRADING AND SHAPING DITCHES			F.A. * RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL084EB\INTEG\Illinois.gov\PWIDOT\Documents\DOT Offices\District 6\Projects\0672\83\CADD\CAD\Drawings\0672\83\0672-83-01.dwg	PLotted	CHECKED -	REVISED -					*	**	SANGAMON	85	69
Default	PLOT SCALE = 100,0000' / in.	DATE -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 72J83				
	PLOT DATE = 10/20/2017							ILLINOIS FED. AID PROJECT				



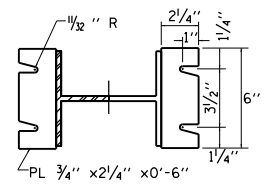
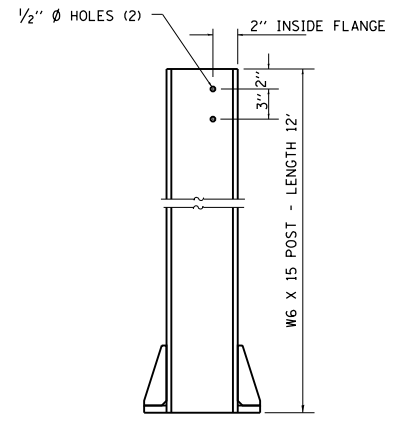
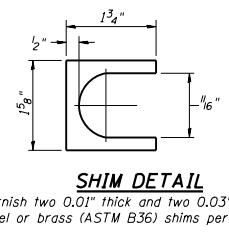
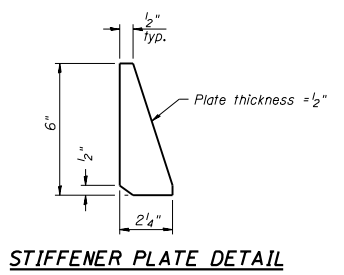
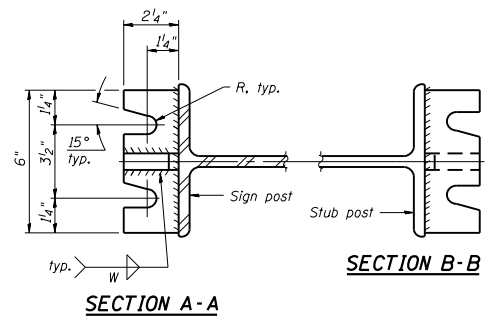
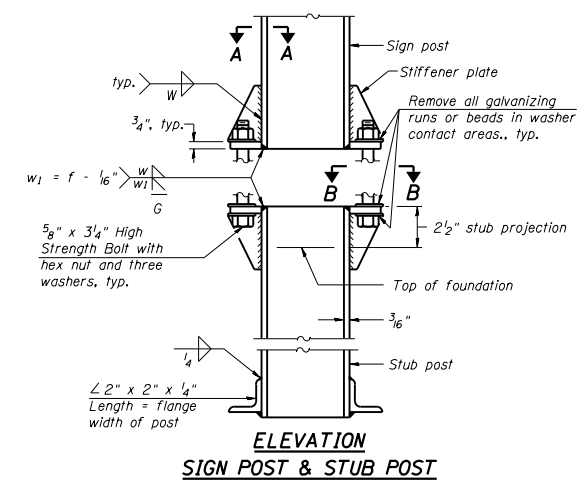
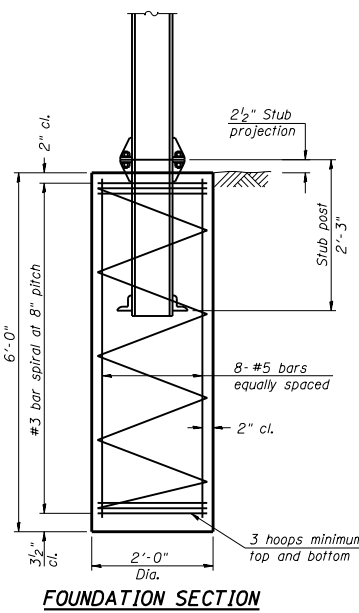
LOCATION: I-55 STA. 144+00.00

NOTES:

1. SYSTEM CONSISTS OF TWO W6 X 15 I-BEAM SIGNPOSTS WITH CONCRETE FOUNDATION AND A FLANGE WITH BREAKAWAY BOLTS
2. THE I-BEAMS ARE 12' LONG AND DRILLED AS PER THE I-BEAM DETAIL TO ACCOMODATE A PIPE TO ALLOW THE MOUNTING OF THE SOLAR PANEL.
3. THE WEB OF THE I-BEAM IS TO BE PLACED PARALLEL TO THE LANES.
4. THE TWO POSTS MUST BE PLACED DIRECTLY ACROSS FROM EACH OTHER AND PERPENDICULAR TO THE LANES.
5. TWO POSTS ARE REQUIRED LOCATED AS PER THE ENGINEER AND DRAWING.
6. THE CABINETS ARE ATTACHED TO THE I-BEAM USING 8" GALVANIZED J-BOLTS.
7. THE BOTTOM OF THE CABINET HEIGHT MUST BE NO LOWER THAN 4' ABOVE THE GROUND AND BE ABLE TO VIEW ACROSS ALL LANES AT THE CROWN OF THE PAVEMENT.
8. THE IDOT OFFICE OF PLANNING AND PROGRAMMING DATA MANAGEMENT LAB (RAMON TAYLOR or RICH MARX 217-782-2065) SHALL BE NOTIFIED TWO WEEKS PRIOR TO THE LAYOUT AND SHALL BE PRESENT DURING THE PLACEMENT OF THE POST FOUNDATIONS.

TIRTL TRAFFIC COUNTER SCHEDULE OF QUANTITIES (FOR INFORMATION ONLY)

CODE NUMBER	ITEM	UNIT	QUANTITY
61100605	MISCELLANEOUS CONCRETE	CU YD	17
72700100	STRUCTURAL STEEL SIGN SUPPORT - BREAKAWAY	POUND	520
73400100	CONCRETE FOUNDATIONS	CU YD	1.4
X0323388	TRAFFIC COUNTER SYSTEM (INCLUDES BELOW)	EACH	1
TIRTL TRAFFIC DATA COLLECTION SYSTEM INCLUDES:			
	Sierra Wireless GX-450 Mobile 4G XLTE Gateway EVDO-Verizon	EACH	1
	Panorama Low Profile LTE MiMo LGAM-BC3G-26-3SP Antenna	EACH	1
	TIRTL CABINET 15" X 27" X 16"	EACH	2
	BATTERY CABINET 16.5" X 16" X 11.5" BBA1M w/ *2 Police Lock	EACH	1
	SOLAR PANEL 60 W 12 VDC with Bracket	EACH	2
	TIRTL TRAFFIC COUNTER	EACH	1
	SUNSAVER SOLAR CHARGE REGULATOR 10 AMP 12 VDC	EACH	2
	BATTERY - 33 AH ABSORBED ELECTROLYTE 8" X 5.5" X 7" (DEEP CYCLE)	EACH	1
	BATTERY -104AH ABSORBED ELECTROLYTE 10" X 6" X 8" (DEEP CYCLE)	EACH	1



GENERAL NOTES

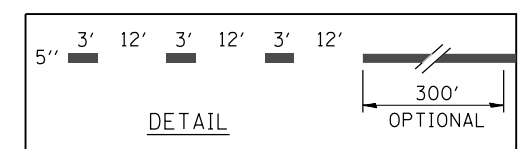
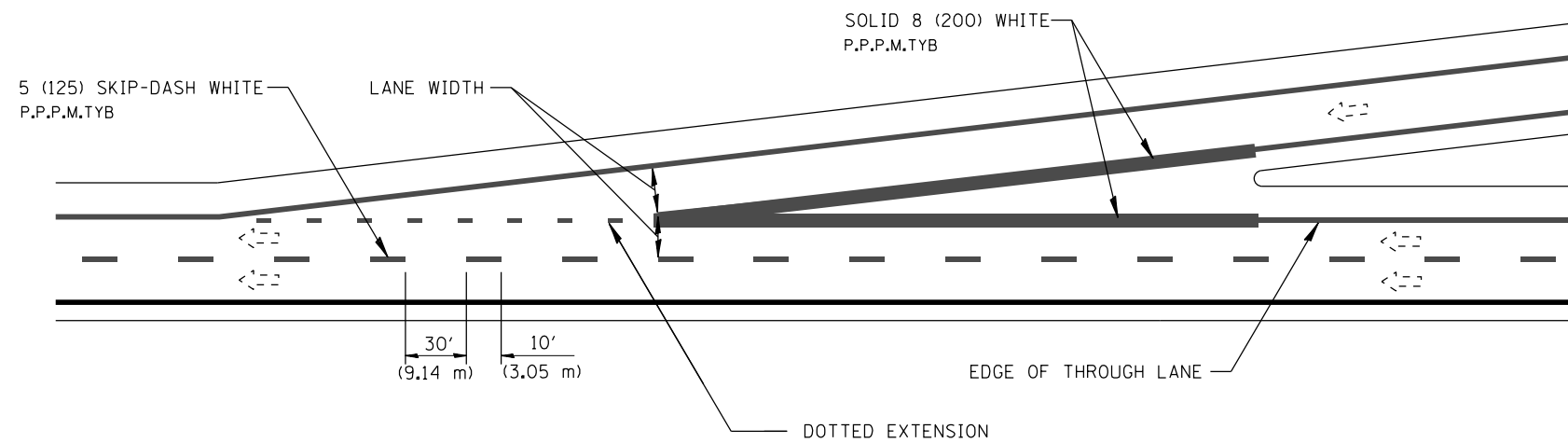
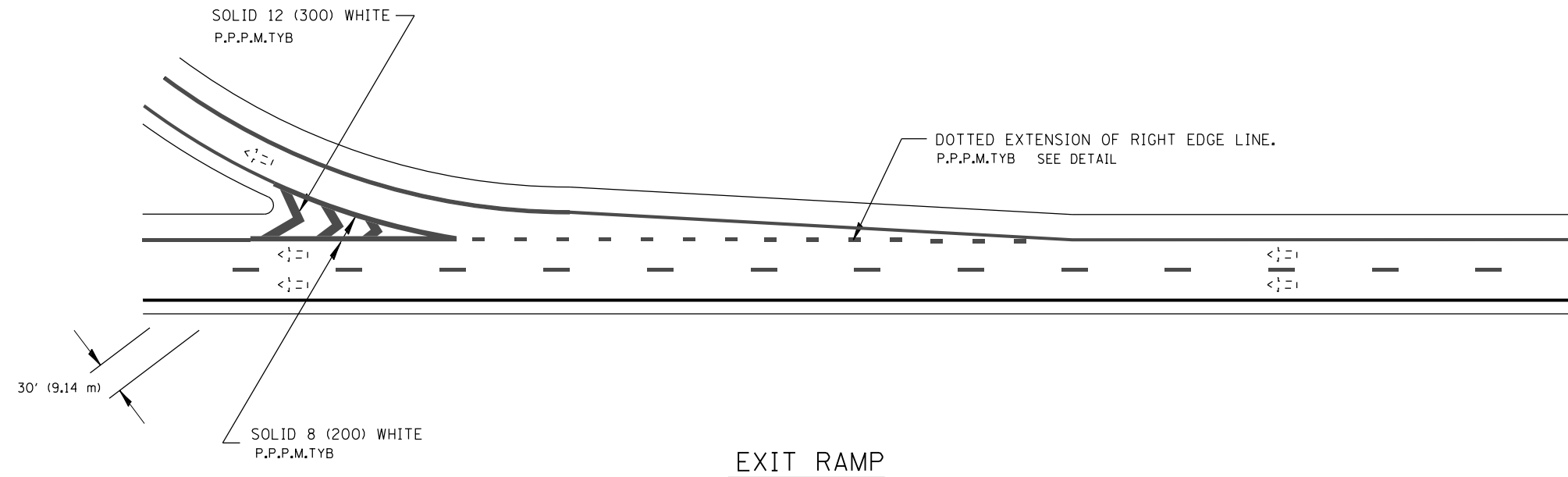
Posts shall be plumbed by using shims with post-to-stub post connection bolts snug tight only. Final tightening of all High Strength Bolts shall be in accordance with Article 727.05 and threads at the junction of the bolt and nut shall be burred or center punched to prevent the nut from loosening.

LOADING: 80 m.p.h. wind with 30% gust factor, normal to sign.

DESIGN STRESSES:
 Structural steel - 20,000 p.s.i.
 Reinforcing steel - 20,000 p.s.i.
 Concrete - 1,400 p.s.i.
 Footing soil pressure - 2,000 p.s.f.

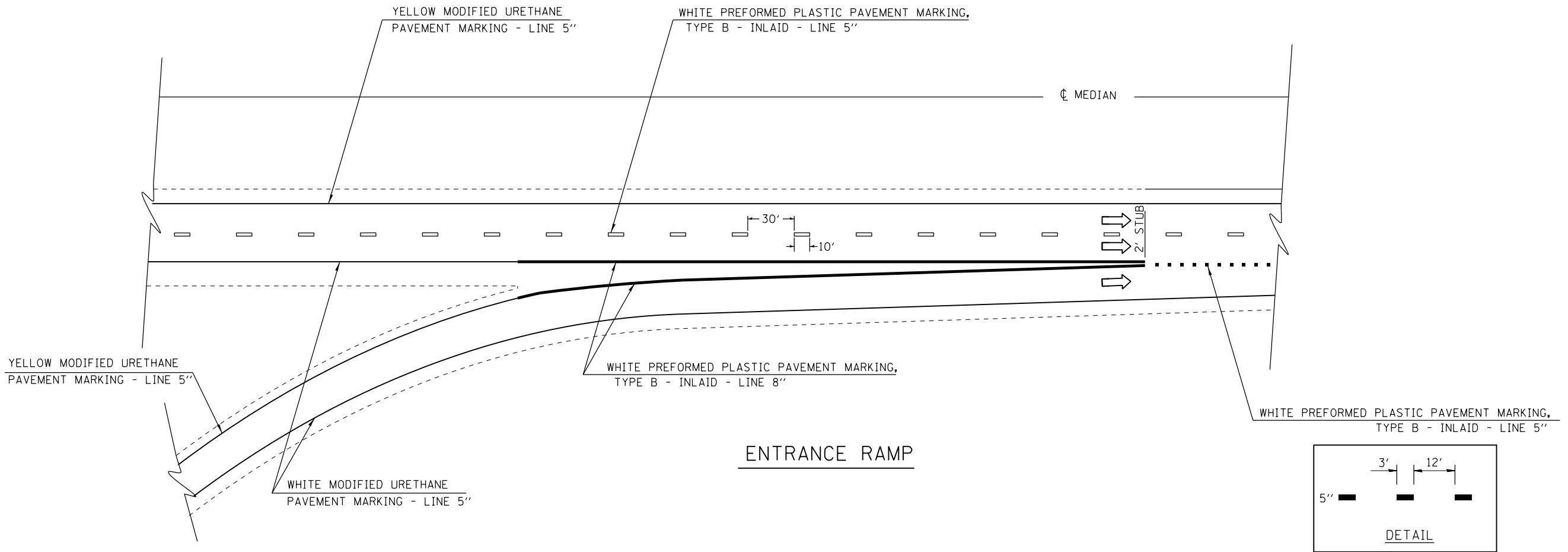
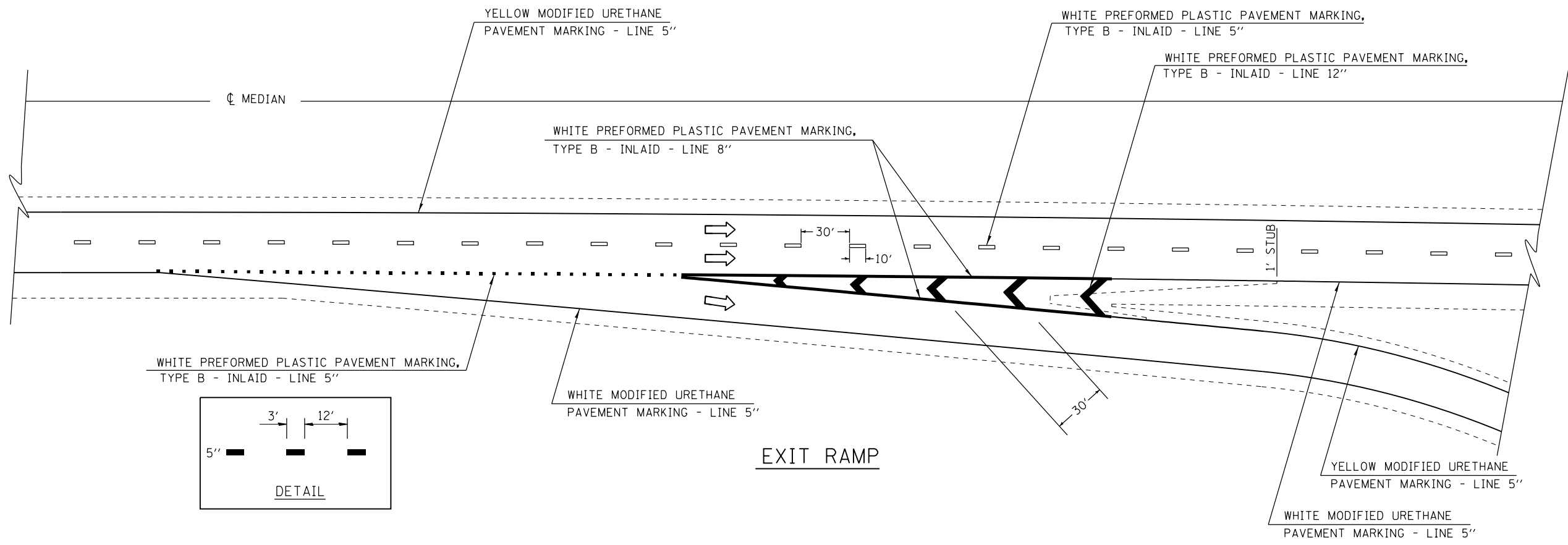
After fabrication, the post and upper 6", min. of the stub post shall be hot-dip galvanized in accordance with AASHTO M111. All bolts, nuts and washers shall be hot-dip galvanized in accordance with AASHTO M232.

BREAK-AWAY WIDE FLANGE STEEL POST DETAILS



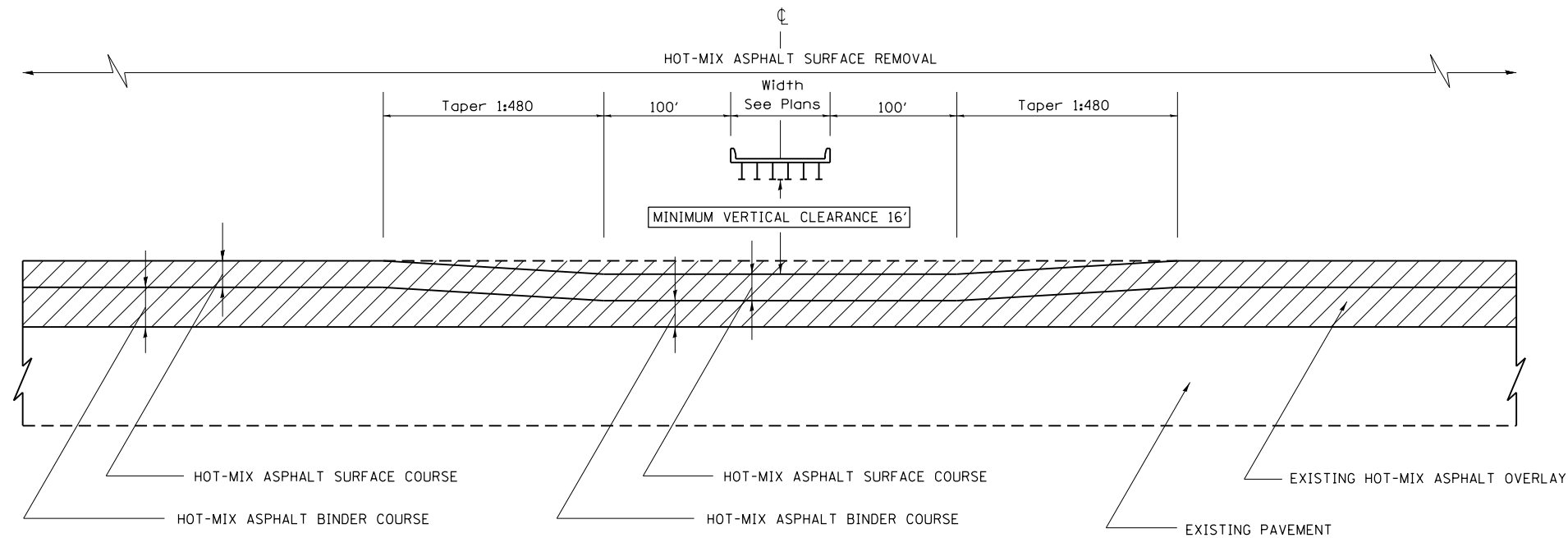
FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL PAVEMENT MARKING DETAILS			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL084EB\INTEG\Illinois.gov\PWIDOT\Documents\DOT Offices\District 6\Projects\D672J83\CADData\CADsheets\D672J83\Drawings.dwg		CHECKED -	REVISED -						**	SANGAMON	85	71
Default	PLOT SCALE = 100.0000 "/in.	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	ILLINOIS	FED. AID PROJECT
	PLOT DATE = 10/20/2017										CONTRACT NO.	72J83

*F.A.I. 55, F.A.I. 72, F.A.P. 666 **(84-4, 84-3, 84-9, 110) RS



FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL PAVEMENT MARKING DETAILS			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL084EB\INTEG\Illinois.gov\PWIDOT\Documents\DOT Offices\District 6\Projects\D672J83\CADData\CADsheets\D672J83\Drawings.dwg		CHECKED -	REVISED -						**	SANGAMON	85	72
Default	PLOT SCALE = 100.0000' / in.	DATE -	REVISED -		SCALE:	SHEET	OF SHEETS	STA.	TO STA.	ILLINOIS	FED. AID PROJECT	CONTRACT NO. 72J83
	PLOT DATE = 10/20/2017											

*F.A.I. 55, F.A.I. 72, F.A.P. 666 **(84-4, 84-3, 84-9, 110) RS



OVERHEAD STRUCTURE RAMPING DETAIL

 - HOT-MIX ASPHALT SURFACE REMOVAL

NOTES:

HOT-MIX ASPHALT RESURFACING OF THE EXISTING PAVEMENT AND SHOULDERS SHALL BE DONE IN A MANNER THAT MAINTAINS A MINIMUM VERTICAL CLEARANCE OF 16 FEET AT ALL OVERHEAD STRUCTURES. THE MINIMUM VERTICAL CLEARANCE SHALL BE MAINTAINED FROM OUTSIDE TO OUTSIDE OF THE PAVED SHOULDERS.

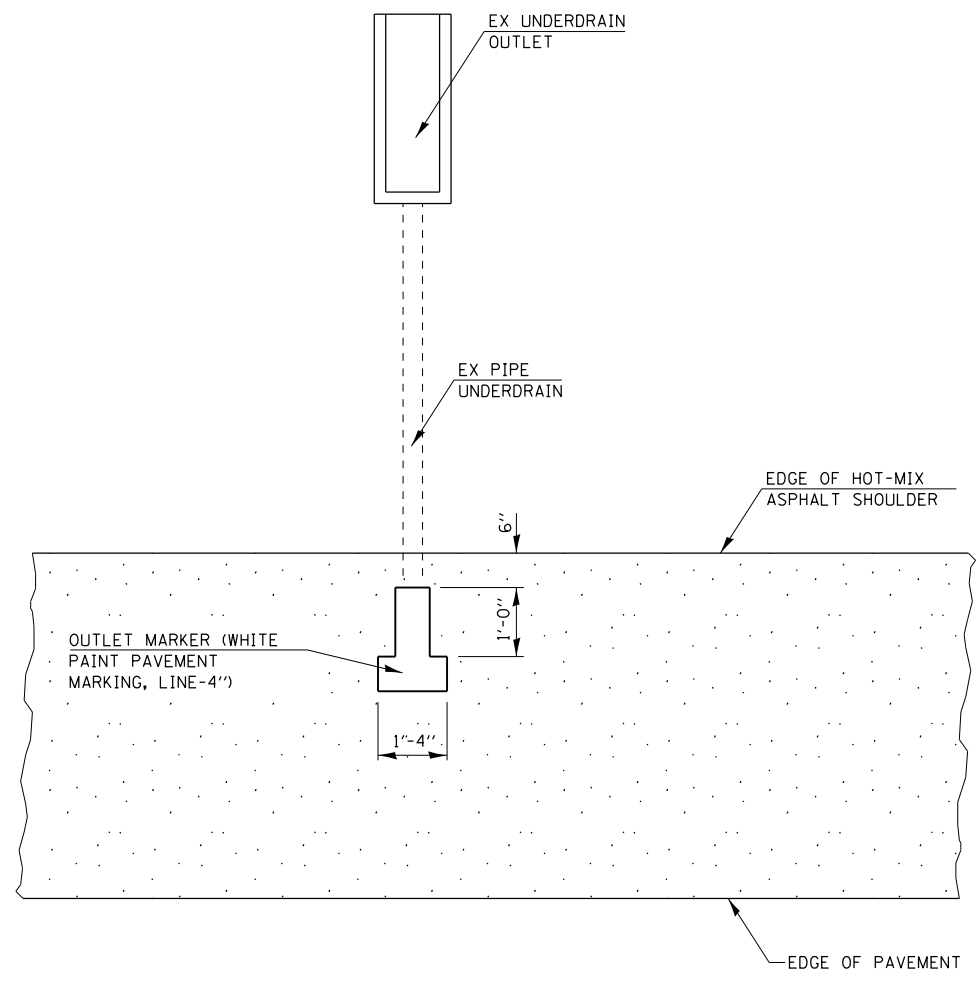
PRIOR TO THE START OF RESURFACING OPERATIONS, THE CONTRACTOR, IN THE PRESENCE OF THE ENGINEER, SHALL MEASURE AND DOCUMENT THE EXISTING VERTICAL CLEARANCE AT ALL OVERHEAD STRUCTURES. MEASUREMENTS SHALL BE TAKEN AT THE OUTSIDE EDGES OF THE PAVED SHOULDERS, AT THE EDGE OF EACH LANE, AND AT ANY SPLICE PLATES OVER THE SHOULDERS OR PAVEMENT. IF NECESSARY, THE ENGINEER SHALL MAKE ADJUSTMENTS TO THE RESURFACING THICKNESS SHOWN IN THE PLANS TO MAINTAIN THE REQUIRED MINIMUM VERTICAL CLEARANCE.

FOLLOWING PLACEMENT OF THE SURFACE COURSE AND HMA SHOULDERS, THE CONTRACTOR, IN THE PRESENCE OF THE ENGINEER, SHALL MEASURE AND DOCUMENT THE VERTICAL CLEARANCE AS DESCRIBED ABOVE. IF THE MINIMUM VERTICAL CLEARANCE IS LESS THAN 16 FEET, THE CONTRACTOR, AT HIS OWN EXPENSE, SHALL REMOVE AND REPLACE THE HMA SURFACE COURSE AS DIRECTED BY THE ENGINEER.

THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE VARIOUS HOT-MIX ASPHALT ITEMS.

HOT-MIX ASPHALT SHOULDERS SHALL BE RAMPED IN THE SAME MANNER AS THE HMA BINDER AND SURFACE COURSES.

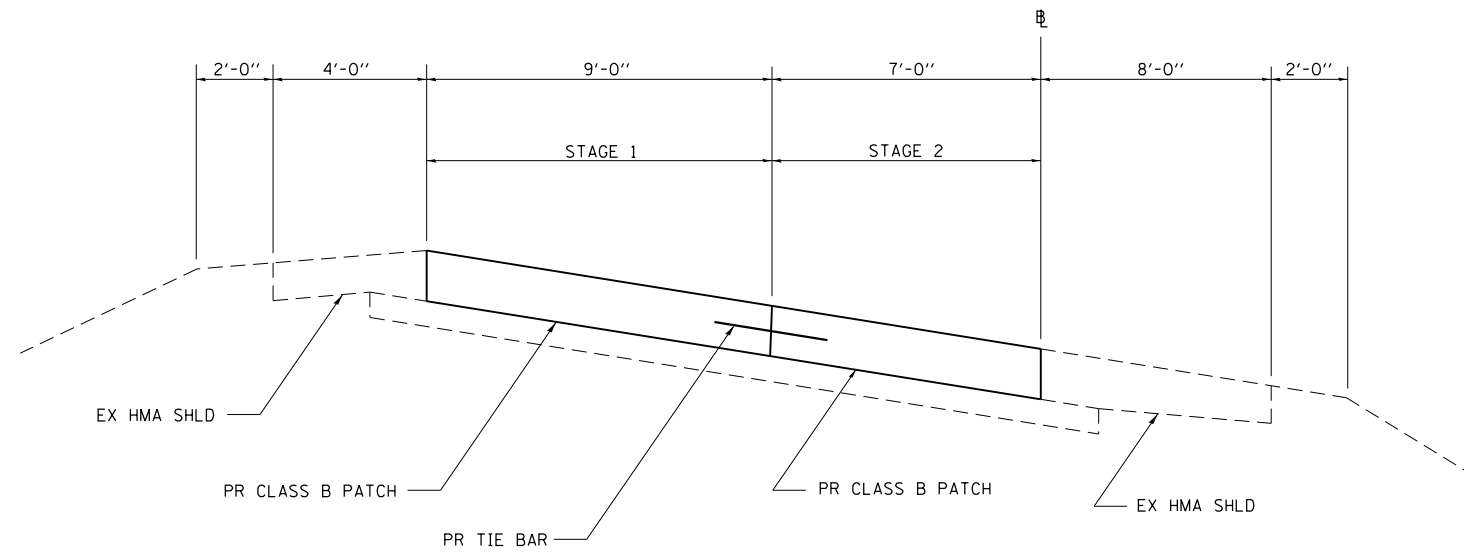
FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	OVERHEAD STRUCTURE RAMPING DETAIL			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL084EB\INTEG\Illinois.gov\PWIDOT\Documents\DOT Offices\District 6\Projects\0672\83\CADData\CADsheets\0672\DRAWN\11.dwg	DESIGNED -	REVISED -	REVISED -					*	**	SANGAMON	85	73
Default	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.			ILLINOIS FED. AID PROJECT				
	PLOT DATE = 10/20/2017	DATE -	REVISED -					CONTRACT NO. 72J83				



PLAN FOR UNDERDRAIN OUTLET MARKER

FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	UNDERDRAIN OUTLET MARKER DETAIL			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL084EB\INTEG\Illinois.gov\PWIDOT\Documents\DOT Offices\District 6\Projects\0672\83\CADData\CADsheets\0672\83\0672_83.dwg	DRWN	REVISIONS	REVISIONS					*	**	SANGAMON	85	74
Default	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 72J83				
	PLOT DATE = 10/20/2017	DATE -	REVISED -					ILLINOIS FED. AID PROJECT				

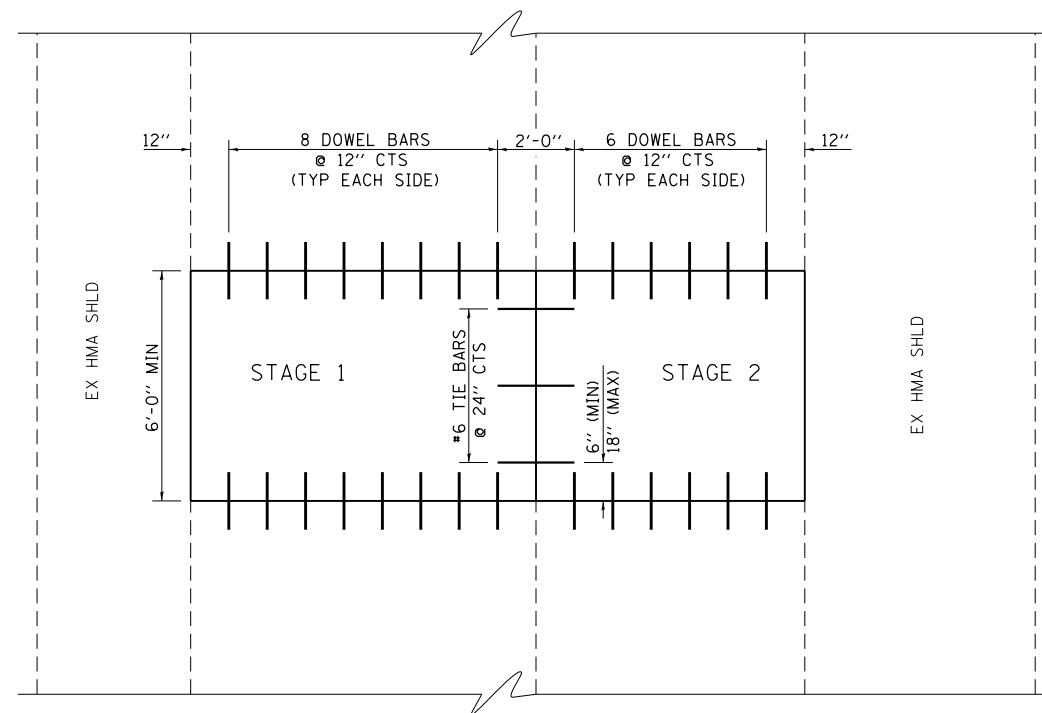
*F.A.I. 55, F.A.I. 72, F.A.P. 666 **(84-4, 84-3, 84-9, 110) RS



NOTE:

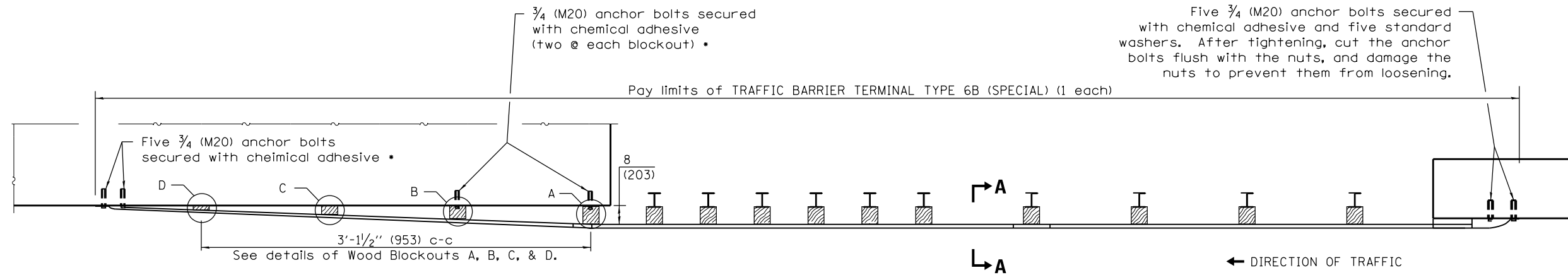
1. THIS DETAIL IS TO BE USED IN CONJUNCTION WITH STANDARD 442101.
2. TIE BARS SHALL BE NO. 6 DEFORMED BARS AT 24" CENTERS WHICH SHALL BE DRILLED AND GROUTED ACCORDING TO ARTICLE 420.05(b) OF THE STANDARD SPECIFICATIONS.

TYPICAL SECTION - CLASS B PATCH ON RAMPS



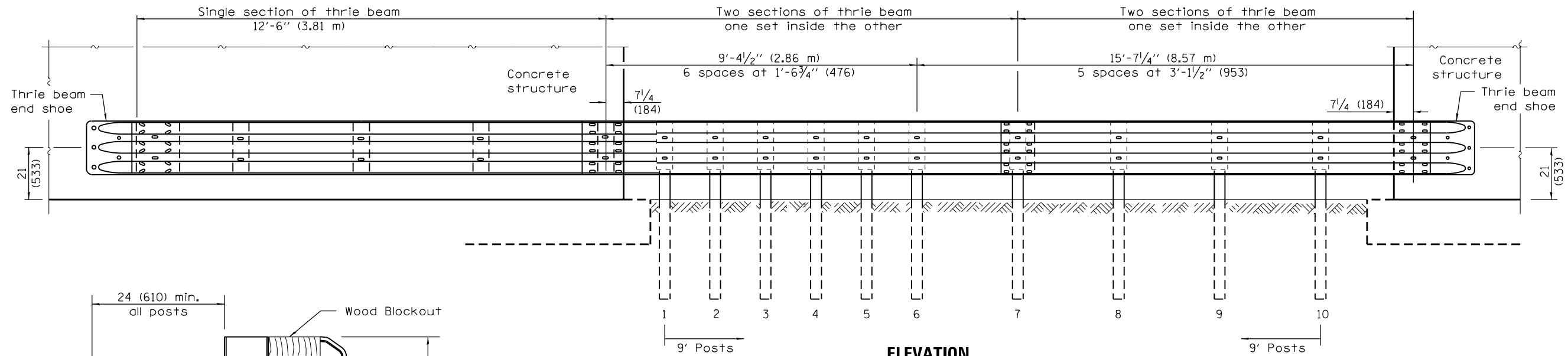
PLAN - CLASS B PATCH ON RAMPS

FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CLASS B PATCHING RAMP DETAIL			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL04EB\INTEG\Illinois.gov\PWIDOT\Documents\DOT Offices\District 6\Projects\0672\83\CADData\CADsheets\0672\83\DRW\1111.dwg	DATE	REVISION	DATE					*	**	SANGAMON	85	75
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISION	DATE		CONTRACT NO. 72J83			ILLINOIS	FED. AID PROJECT			
Default	DATE = 10/20/2017	REVISION	DATE		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.		

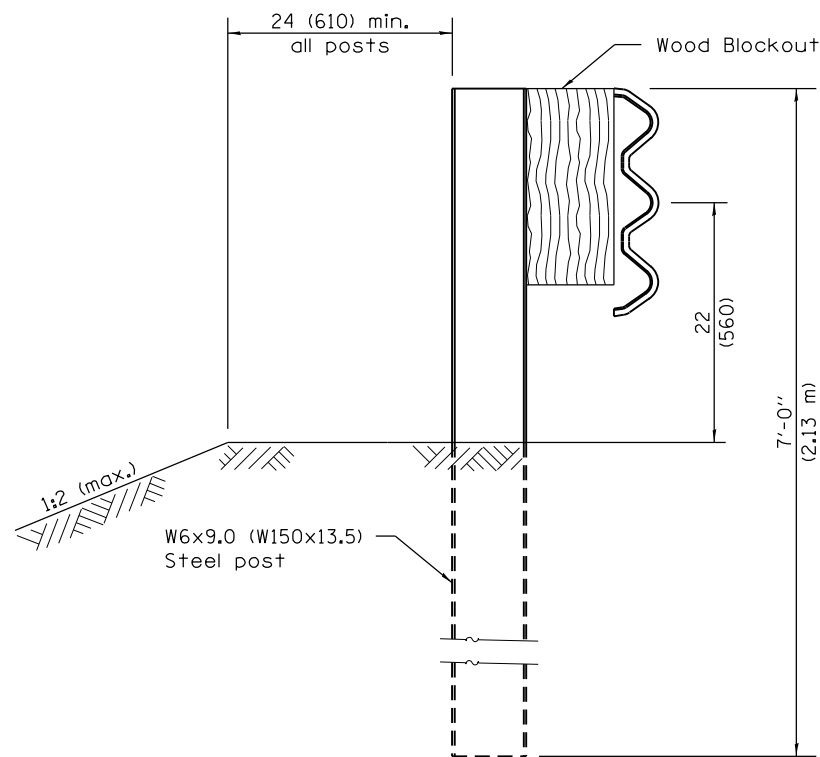


- * With standard washers. After tightening, cut the anchor bolts flush with the nuts and damage the nuts to prevent them from loosening.

PLAN



ELEVATION

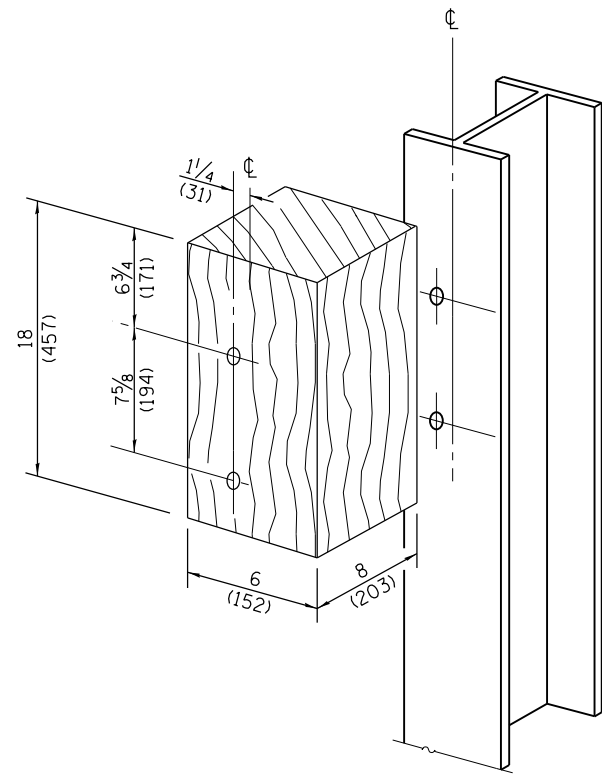


SECTION A-A

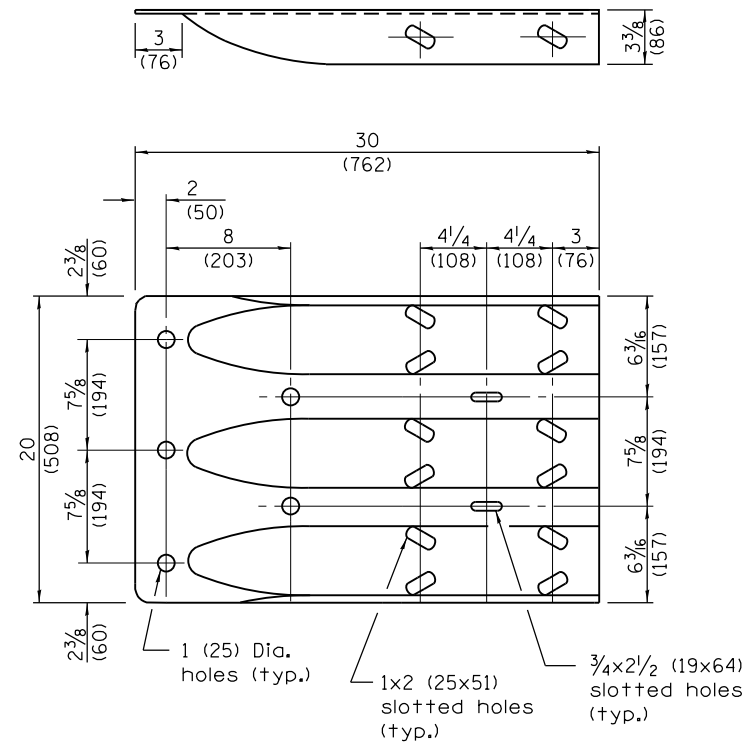
GENERAL NOTES

1. See Standard 630001 for details of guardrail not shown.
2. Bolt hole slots shall be as shown on Standard 630001 except that: Some holes will need to be field drilled or punched, do not elongate existing bolt slots in w-beam. Keep new bolt hole slots at least 3' 1-1/2" from existing bolt hole slots.
3. Post spacing shall be maintained at common values according to Standard 631033. Use odd lengths in increments of 3' 1-1/2" as needed, avoid field drilling splice bolt holes.
4. Thrie beam rail shall be bolted to block-out at all posts.
5. Odd panels shall be shop fabricated.
6. To avoid subsurface conflicts, double block-outs may be used.
7. All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).
8. All dimensions are in inches (millimeters) unless otherwise shown.

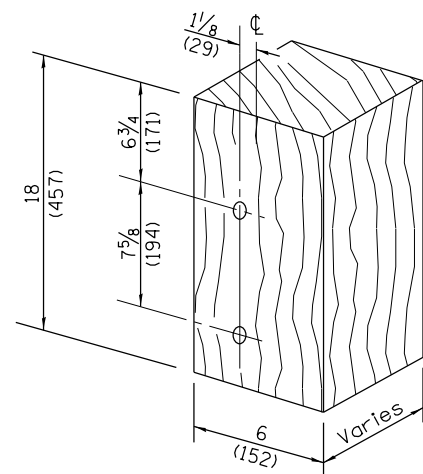
FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC BARRIER TERMINAL TYPE 6B (SPECIAL)				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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Default	PLOT SCALE = 100.0000 "/in.	DATE -	REVISED -								CONTRACT NO. 72J83		
	PLOT DATE = 10/20/2017										ILLINOIS FED. AID PROJECT		



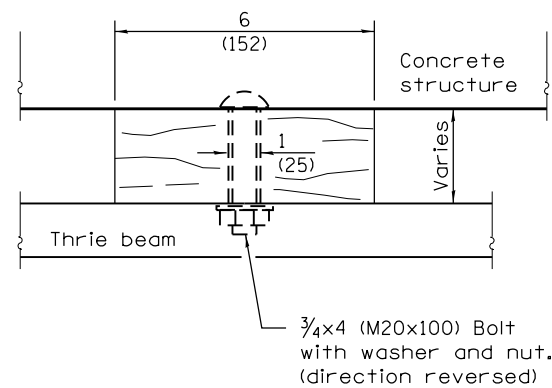
POSTS 1-10 WOOD BLOCKOUT DETAIL



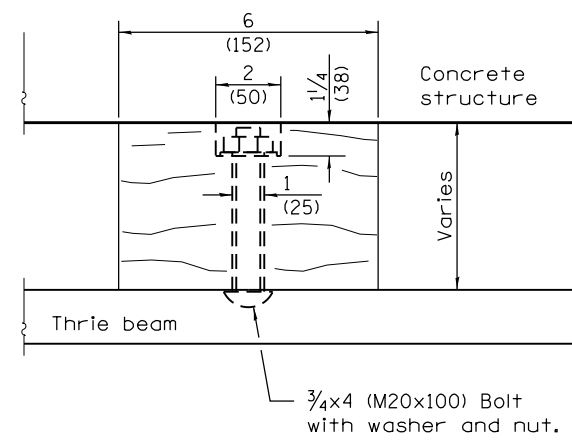
THRIE BEAM END SHOE DETAIL



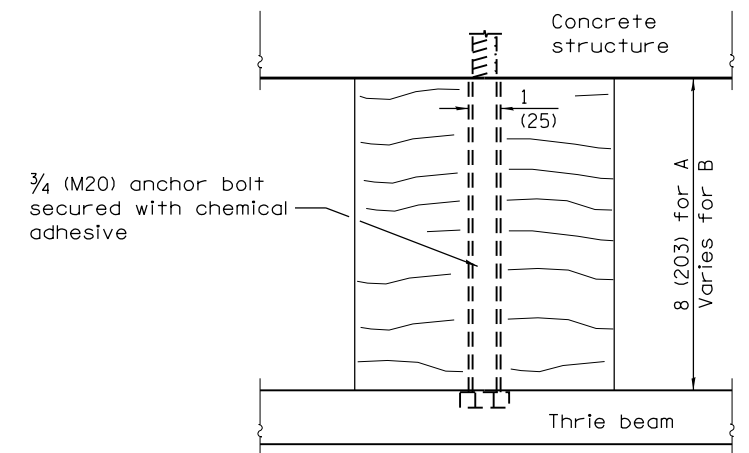
**MODIFIED THICKNESS DETAIL
WOOD BLOCKOUTS A, B, C, & D**



WOOD BLOCKOUT D

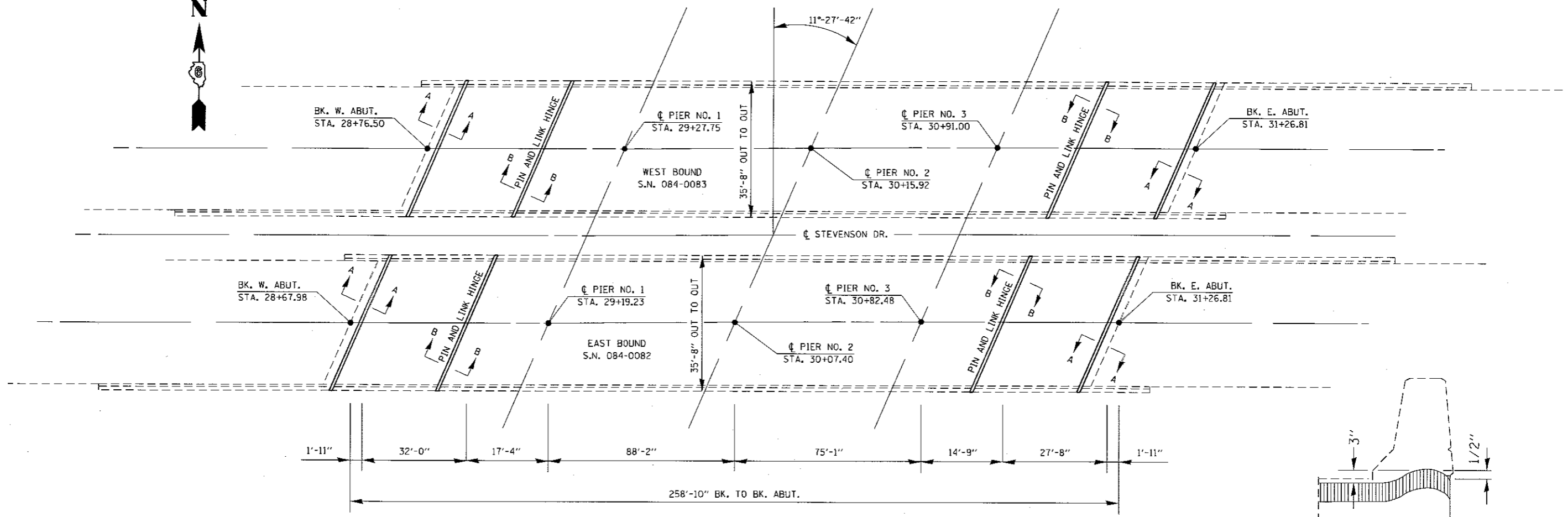


WOOD BLOCKOUT C

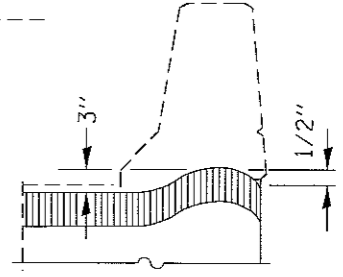


WOOD BLOCKOUT A & B

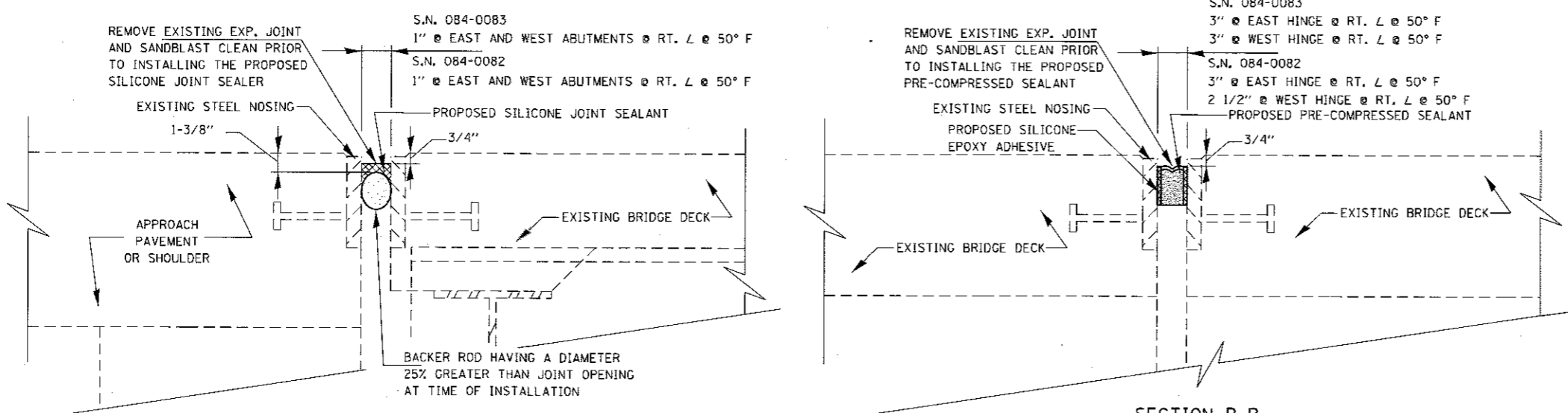
FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC BARRIER TERMINAL TYPE 6B (SPECIAL)			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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Default	PLOT SCALE = 100.0000 /in.	DATE -	REVISED -					SCALE: SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 72J83	
	PLOT DATE = 10/20/2017							ILLINOIS FED. AID PROJECT			*F.A.I. 55, F.A.I. 72, F.A.P. 666 ** (84-4, 84-3, 84-9, 110) RS	



PLAN
S.N. 084-0082 & S.N. 084-0083



TYPICAL END OF SEAL TREATMENT



SECTION A-A
SILICONE JOINT DETAIL

SECTION B-B
PRE-COMPRESSED, SELF EXPANDING SEALER DETAIL

NOTES:

1. PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN THE SCOPE OF WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.
2. WHEN REQUIRED, THE PRE-COMPRESSED PREFORMED JOINT SEAL SHALL BE INSTALLED PER THE MANUFACTURERS RECOMMENDATIONS AND THE ACCOMPANYING SPECIAL PROVISIONS.
3. CONTRACTOR SHALL FIELD VERIFY JOINT WIDTHS PRIOR TO ORDERING MATERIAL.

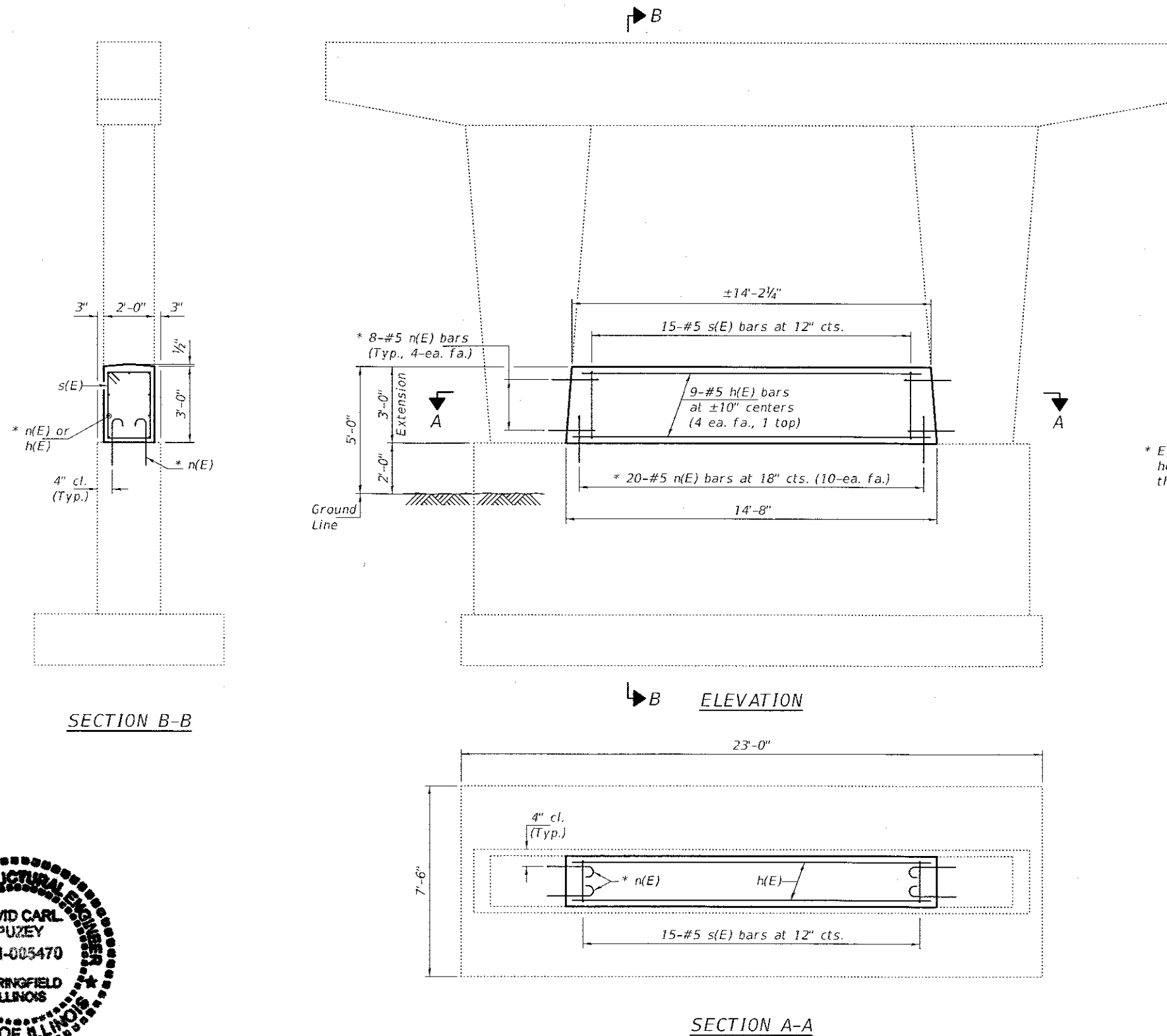
BILL OF MATERIAL

ITEM	UNIT	TOTAL
SILICONE JOINT SEALER, 1"	FOOT	144
PREFORMED JOINT SEAL, 2 1/2"	FOOT	36
PREFORMED JOINT SEAL, 3"	FOOT	108

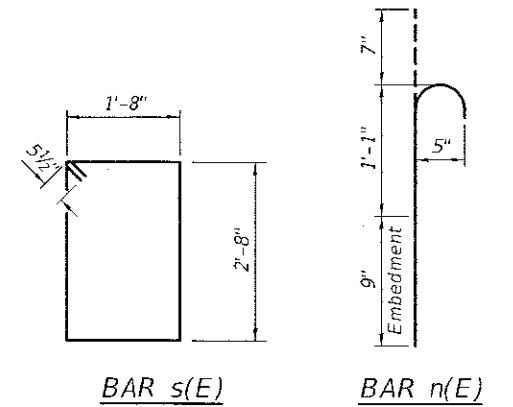
*F.A.I. 55, F.A.I. 72, F.A.P. 666 **84-4, 84-3, 84-9, 110: RS

NOTES

The cost of epoxy grouting threaded rods shall be included with Reinforcement Bars, Epoxy Coated.
 Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
 Reinforcement bars designated (E) shall be epoxy coated.

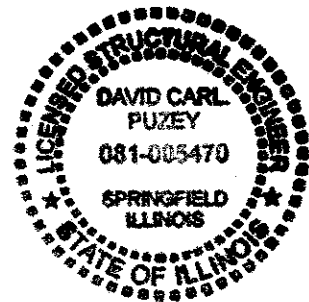


* Epoxy grout n(E) bars in 9" min. holes according to Article 584 of the Standard Specifications.



BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h(E)	18	#5	13'-10"	—
n(E)	72	#5	2'-5"	U
s(E)	30	#5	9'-7"	□
Concrete Structures			Cu. Yd.	6.5
Reinforcement Bars, Epoxy Coated			Pound	740



EXPIRES 11-30-2018

DESIGNED	<i>Victor H. Kelly</i>	EXAMINED	<i>Tina A. Alt</i>	DATE	NOVEMBER 15, 2017
CHECKED	<i>Kyle M. Steffen</i>	PASSED	<i>David Carl Puzey</i>	REVISIONS	
DRAWN	Kyle M. Steffen	ENGINEER OF BRIDGES AND STRUCTURES			
CHECKED	<i>VH</i>				

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

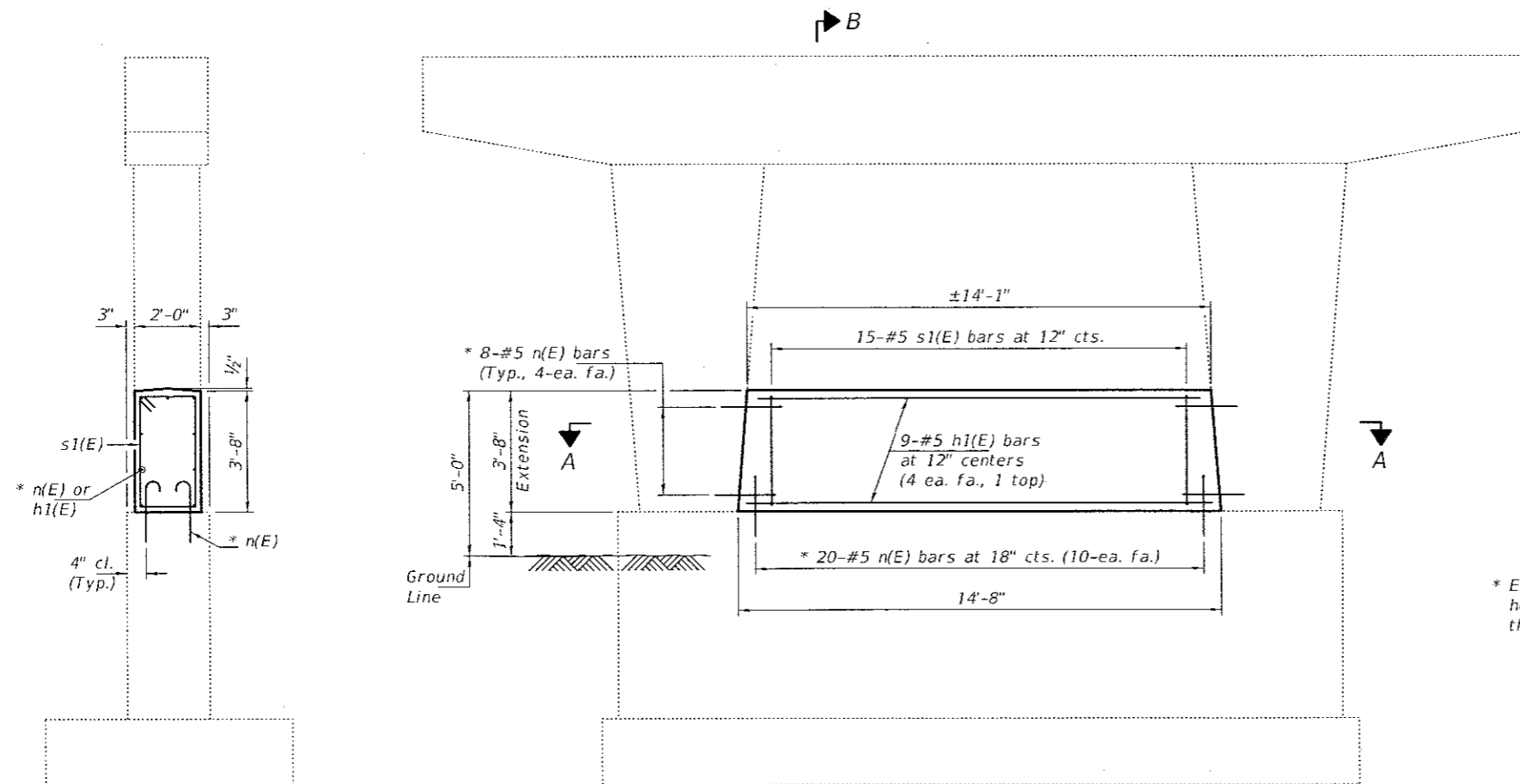
PIER 1 CRASHWALL EXTENSION DETAILS
 SN 084-0082 (E.B.) & -0083 (W.B.)

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(84-4, 84-3, 84-9, 110)RS	SANGAMON	85	79
CONTRACT NO. 72J83			ILLINOIS FED. AID PROJECT	

SHEET NO. 2 OF 3 SHEETS

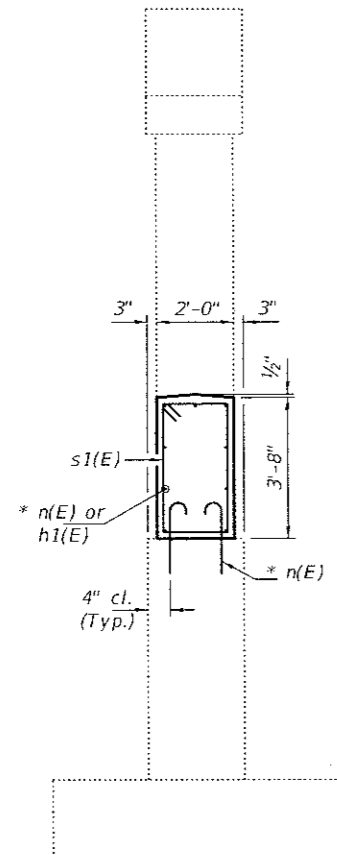
NOTES

The cost of epoxy grouting threaded rods shall be included with Reinforcement Bars, Epoxy Coated.
 Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
 Reinforcement bars designated (E) shall be epoxy coated.

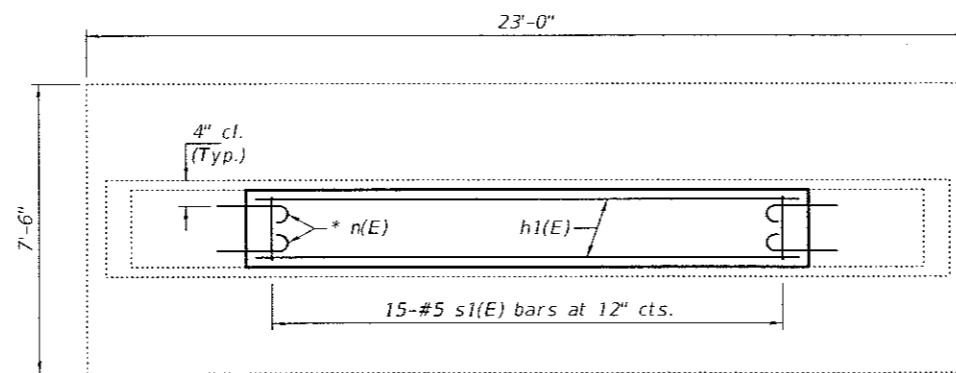


* Epoxy grout n(E) bars in 9" min. holes according to Article 584 of the Standard Specifications.

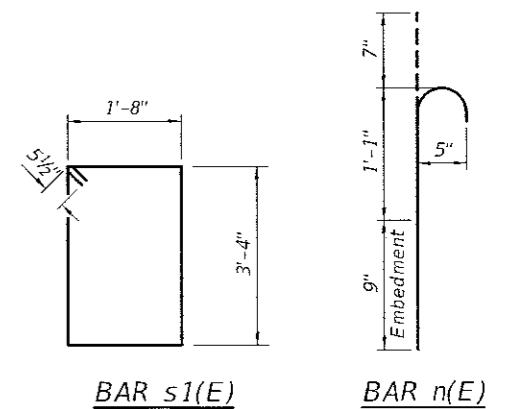
SECTION B-B



ELEVATION



SECTION A-A



BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h1(E)	18	#5	13'-9"	—
n(E)	72	#5	2'-5"	U
s1(E)	30	#5	10'-11"	□
Concrete Structures			Cu. Yd.	7.9
Reinforcement Bars, Epoxy Coated			Pound	780

DESIGNED - VHV	EXAMINED - <i>Timothy A. McGee</i>	DATE - NOVEMBER 15, 2017
CHECKED - DAB	ENGINEER OF STRUCTURAL SERVICES	
DRAWN - Kyle M. Steffen	PASSED - <i>Carl Perry</i>	REVISED
CHECKED - VHV DAB	ENGINEER OF BRIDGES AND STRUCTURES	REVISED

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

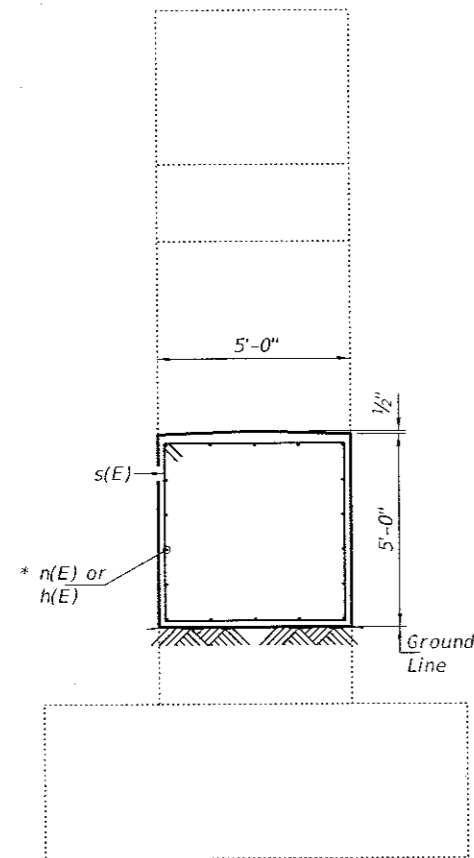
**PIER 3 CRASHWALL EXTENSION DETAILS
 SN 084-0082 (E.B.) & -0083 (W.B.)**

SHEET NO. 3 OF 3 SHEETS

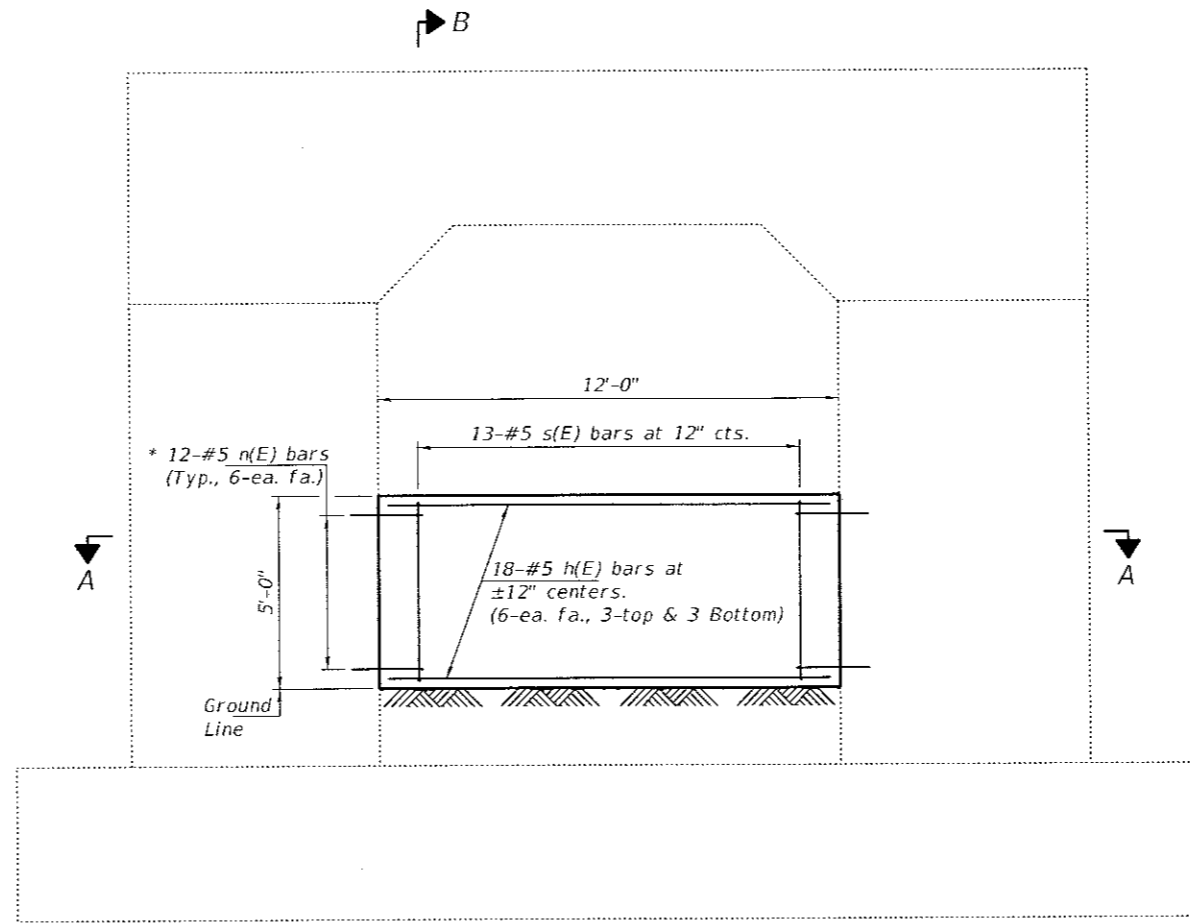
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(84-4, 84-3, 84-9, 110)RS	SANGAMON	85	80
			CONTRACT NO. 72J83	
ILLINOIS FED. AID PROJECT				

NOTES

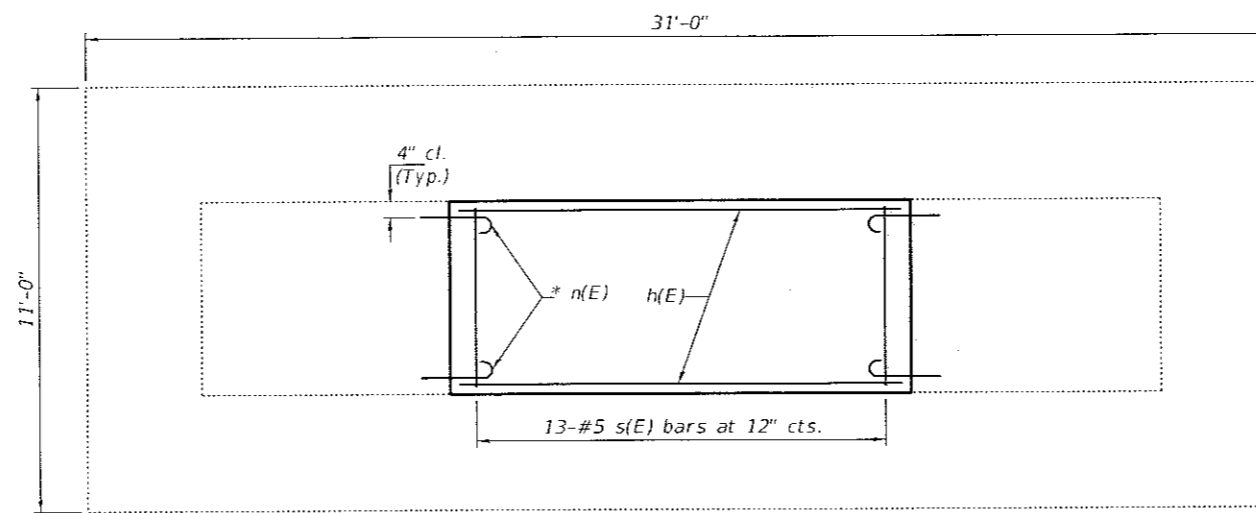
The cost of epoxy grouting threaded rods shall be included with Reinforcement Bars, Epoxy Coated.
 Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
 Reinforcement bars designated (E) shall be epoxy coated.



SECTION B-B

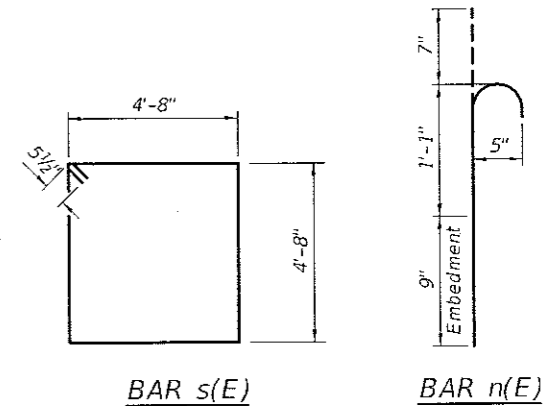


ELEVATION



SECTION A-A

* Epoxy grout n(E) bars in 9" min. holes according to Article 584 of the Standard Specifications.

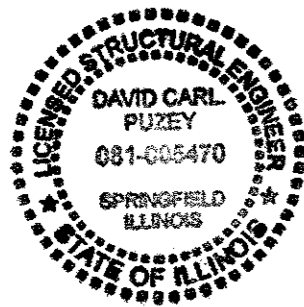


BAR s(E)

BAR n(E)

**BILL OF MATERIAL
(TWO PIERS)**

Bar	No.	Size	Length	Shape
h(E)	36	#5	11'-8"	—
n(E)	48	#5	2'-5"	⌋
s(E)	26	#5	19'-7"	□
Concrete Structures			Cu. Yd.	22.3
Reinforcement Bars, Epoxy Coated			Pound	1090



EXPIRES 11-30-2018

DESIGNED - <i>David K. Puzey</i>	EXAMINED - <i>Tim A. Alt</i>	DATE - NOVEMBER 15, 2017
CHECKED - <i>Kyle M. Steffen</i>	PASSED - <i>David K. Puzey</i>	REVISER
DRAWN - <i>Kyle M. Steffen</i>	REVISER	REVISER
CHECKED - <i>V. K. U.</i>	REVISER	REVISER

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PIERS 1 AND 3 CRASHWALL EXTENSION DETAILS
SN 084-0008**

SHEET NO. 1 OF 2 SHEETS

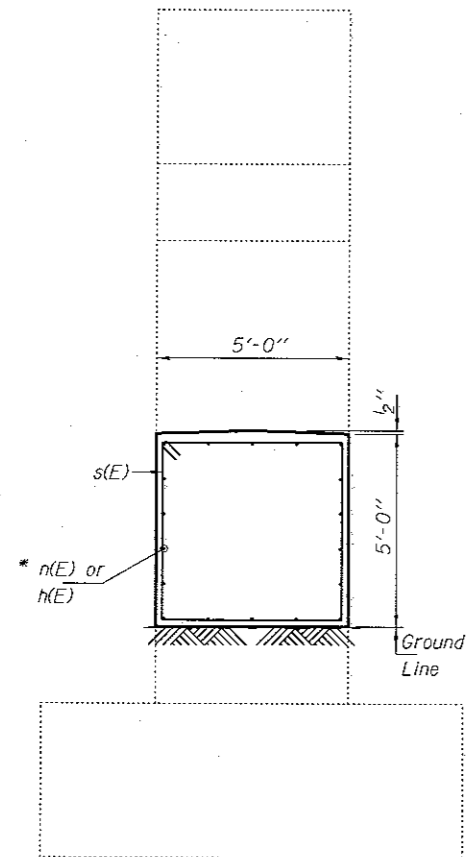
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	184-4, 84-3, 84-9, 110RS	SANGAMON	85	81
CONTRACT NO. 72J83			ILLINOIS FED. AID PROJECT	

GENERAL NOTES

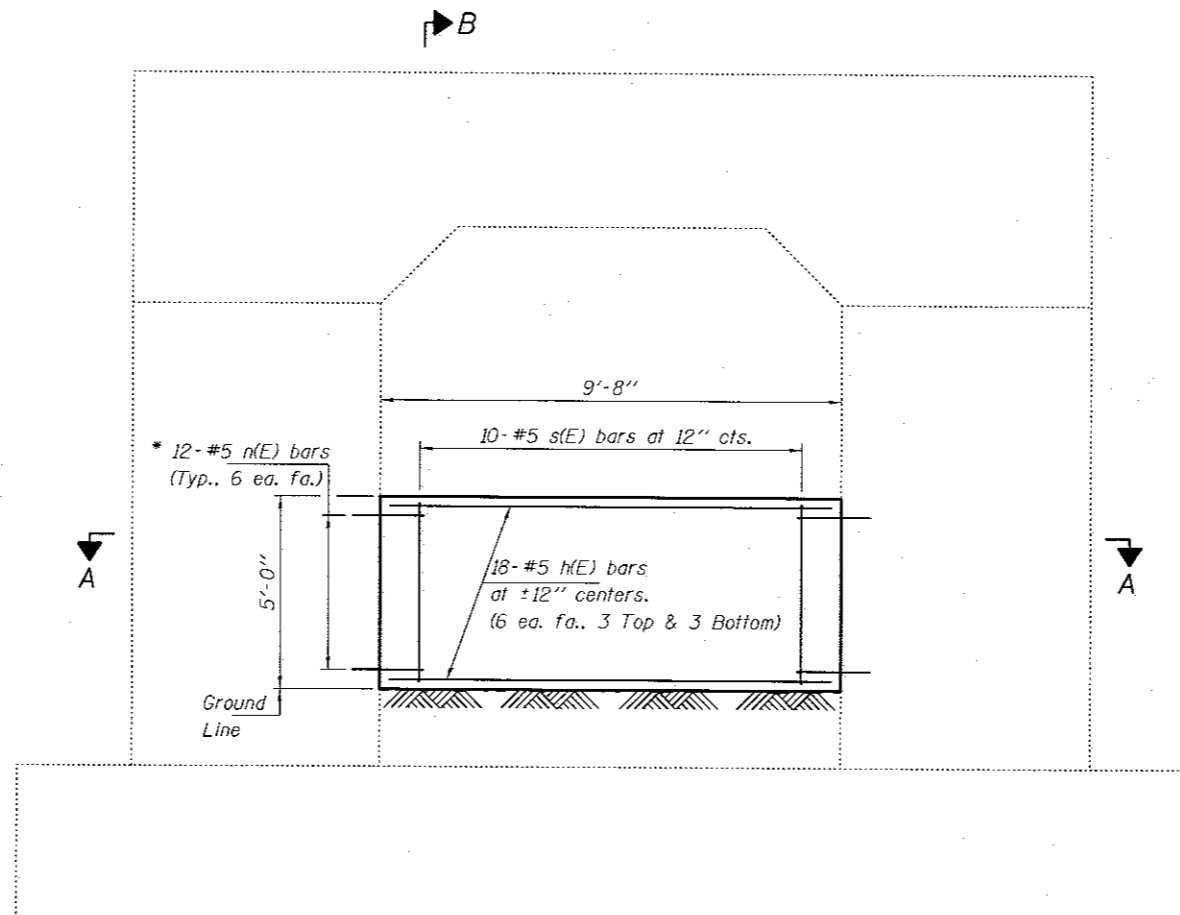
The cost of epoxy grouting threaded rods shall be included with Reinforcement Bars, Epoxy Coated.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work. Reinforcement bars designated (E) shall be epoxy coated.

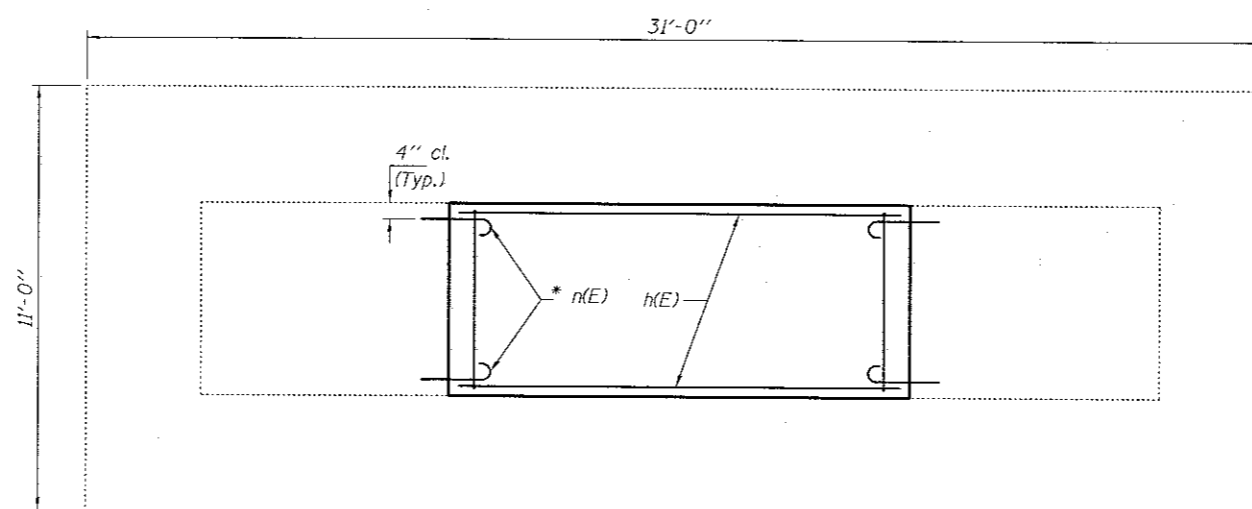
* Epoxy grout n(E) bars in 9" min. holes according to Article 584 of the Standard Specifications.



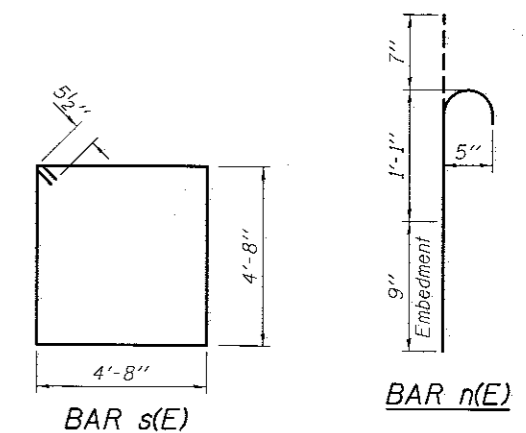
SECTION B-B



ELEVATION



SECTION A-A



TOTAL BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h(E)	18	#5	9'-4"	—
n(E)	24	#5	2'-5"	┌
s(E)	10	#5	19'-7"	□
Concrete Structures			Cu. Yd.	9.0
Reinforcement Bars, Epoxy Coated			Pound	440

DESIGNED <i>VHV</i>	EXAMINED <i>Timothy A. Alcott</i>	DATE <i>NOVEMBER 15, 2017</i>
CHECKED <i>DAB</i>	PASSED <i>Carl Perry</i>	REVISOR
DRAWN <i>baliva</i>	ENGINEER OF BRIDGES AND STRUCTURES	REVISOR
CHECKED <i>VHV DAB</i>		

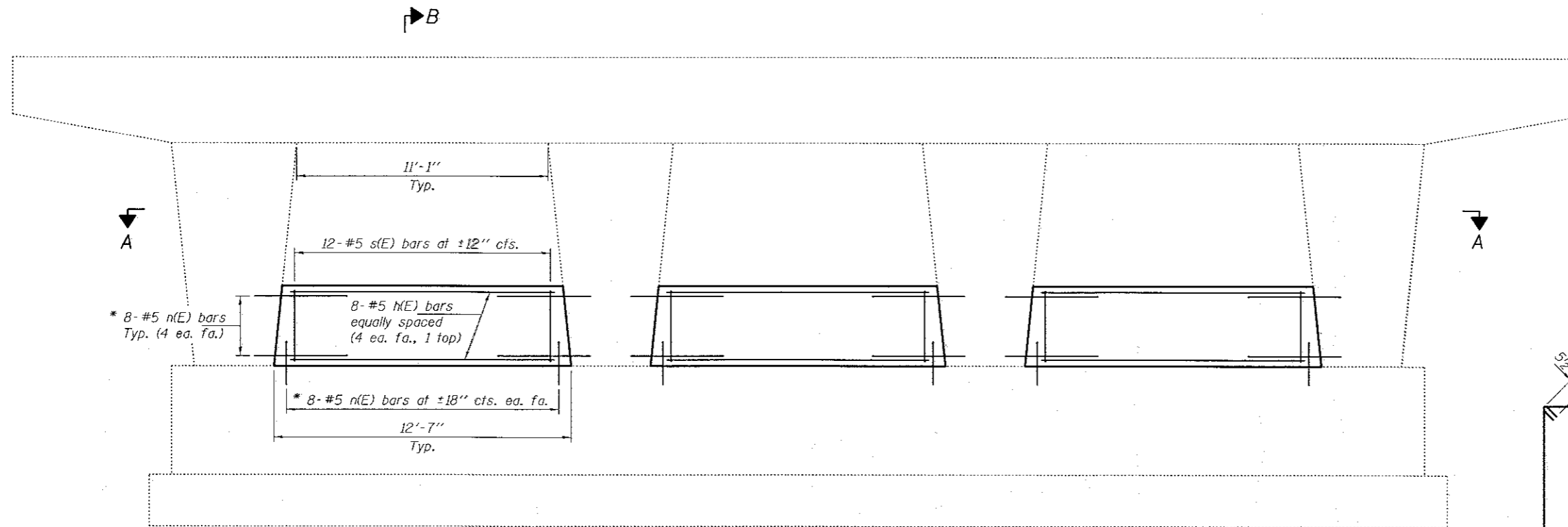
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER 2 CRASHWALL EXTENSION
SN 084-0008

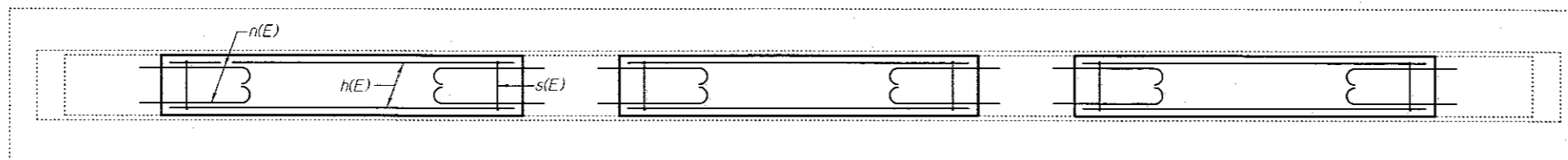
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	084-4, 84-3, 84-9, 110RS	SANGAMON	85	82
CONTRACT NO. 72J83			ILLINOIS FED. AID PROJECT	

GENERAL NOTES

The cost of epoxy grouting threaded rods shall be included with Reinforcement Bars, Epoxy Coated.
 Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work. Reinforcement bars designated (E) shall be epoxy coated.



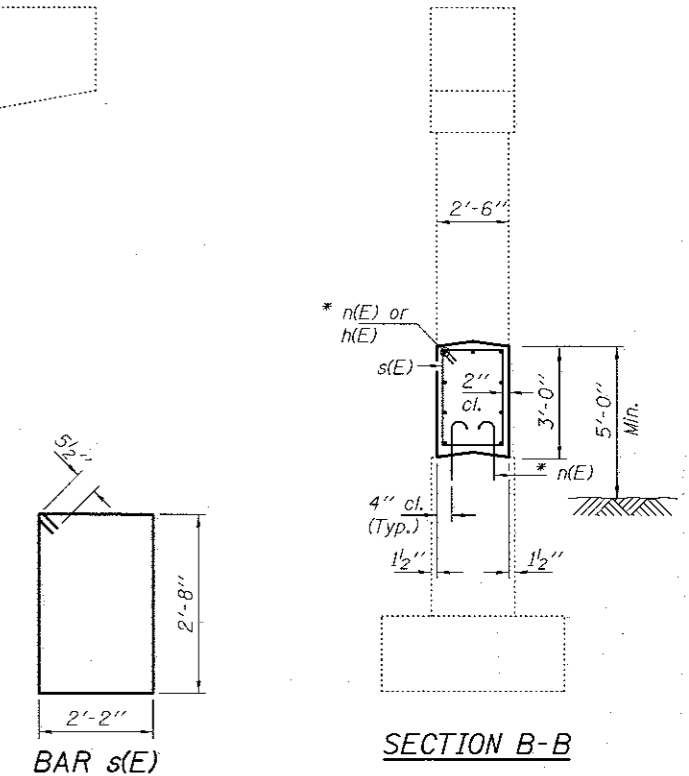
ELEVATION



SECTION A-A



EXPIRES 11-30-2018



* Epoxy grout n(E) bars in 9" min. holes according to Article 584 of the Standard Specifications.

TOTAL BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h(E)	24	#5	10'-9"	—
n(E)	96	#5	2'-5"	U
s(E)	36	#5	10'-1"	□
Concrete Structures			Cu. Yd.	9.9
Reinforcement Bars, Epoxy Coated			Pound	890

DESIGNED <i>Vic Fort-Holmes</i>	EXAMINED <i>Tony A. [Signature]</i>	DATE NOVEMBER 15, 2017
CHECKED <i>[Signature]</i>	PASSED <i>[Signature]</i>	REVISIONS
DRAWN <i>baliva</i>	REVISIONS	
CHECKED <i>VKW</i>		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

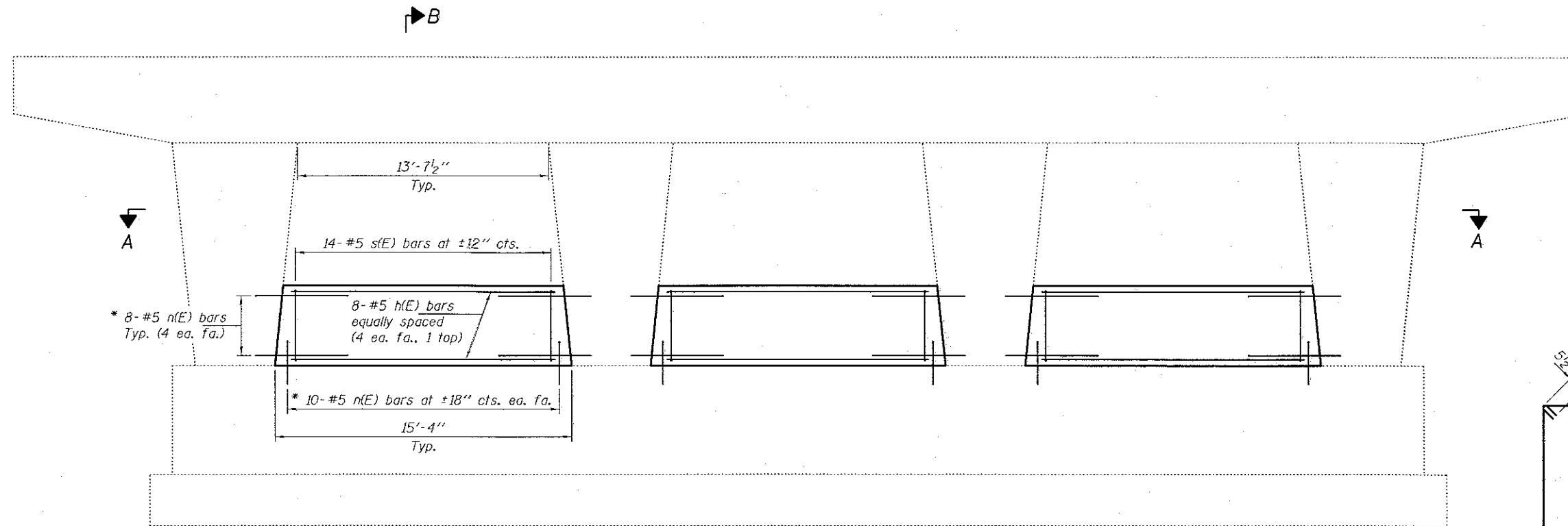
**PIER 2 CRASHWALL EXTENSION
SN 084-0028**

SHEET NO. 1 OF 1 SHEETS

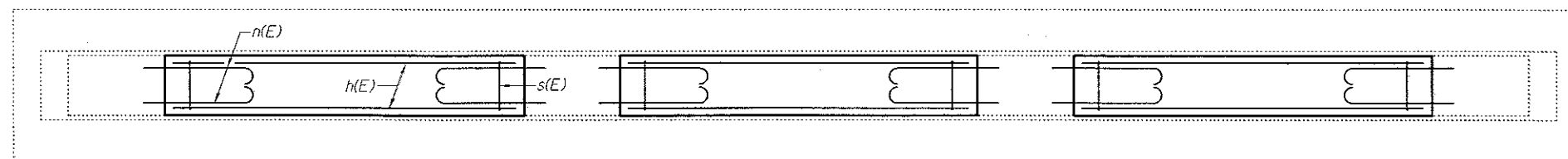
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(84-4, 84-3, 84-9, 110IRS)	SANGAMON	85	83
				CONTRACT NO. 72J83
ILLINOIS FED. AID PROJECT				

GENERAL NOTES

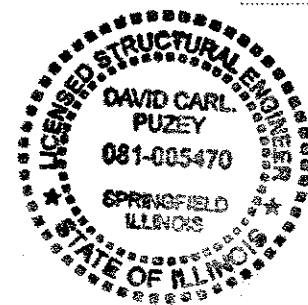
The cost of epoxy grouting threaded rods shall be included with Reinforcement Bars, Epoxy Coated.
 Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
 Reinforcement bars designated (E) shall be epoxy coated.



ELEVATION



SECTION A-A



EXPIRES 11-30-2018

DESIGNED <i>Nickolas H. Veliz</i>	EXAMINED <i>Timothy A. Duda</i>	DATE NOVEMBER 15, 2017
CHECKED <i>...</i>	PASSED <i>...</i>	REVISOR
DRAWN <i>baliva</i>	REVISOR	REVISOR
CHECKED <i>VHV</i>		

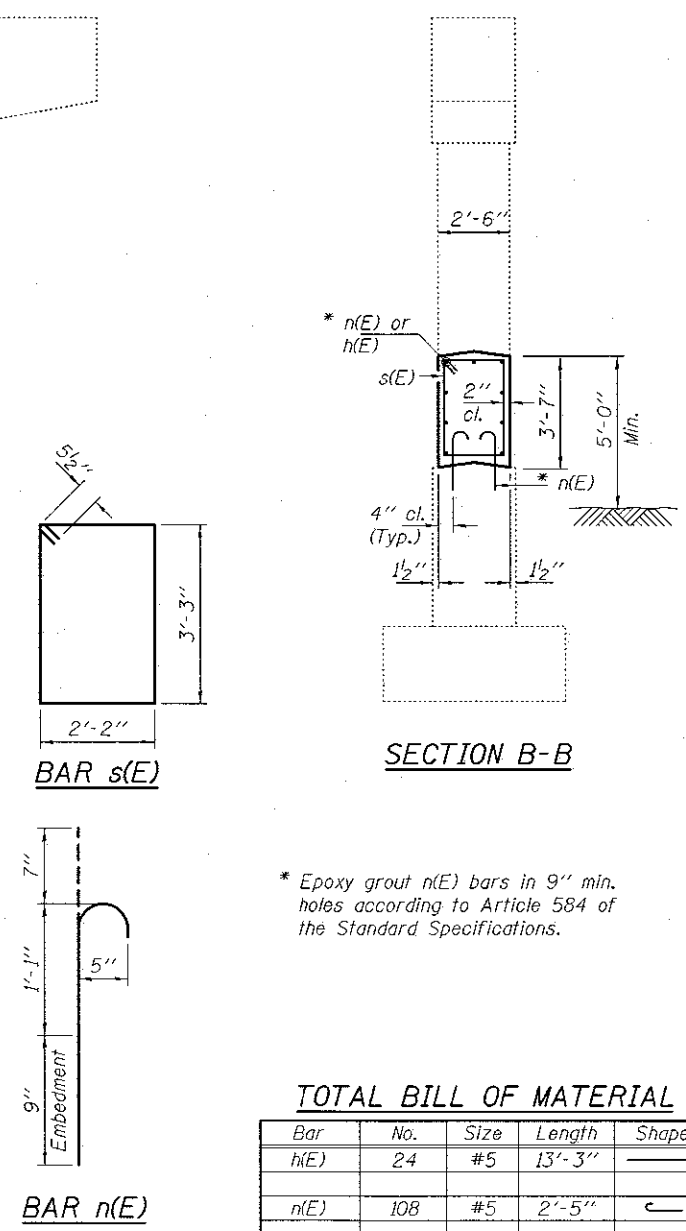
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PIER 1 CRASHWALL EXTENSION
SN 084-0078**

SHEET NO. 1 OF 1 SHEETS

TOTAL BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h(E)	24	#5	13'-3"	—
n(E)	108	#5	2'-5"	—
s(E)	42	#5	11'-3"	□
Concrete Structures			Cu. Yd.	14.4
Reinforcement Bars, Epoxy Coated			Pound	1100

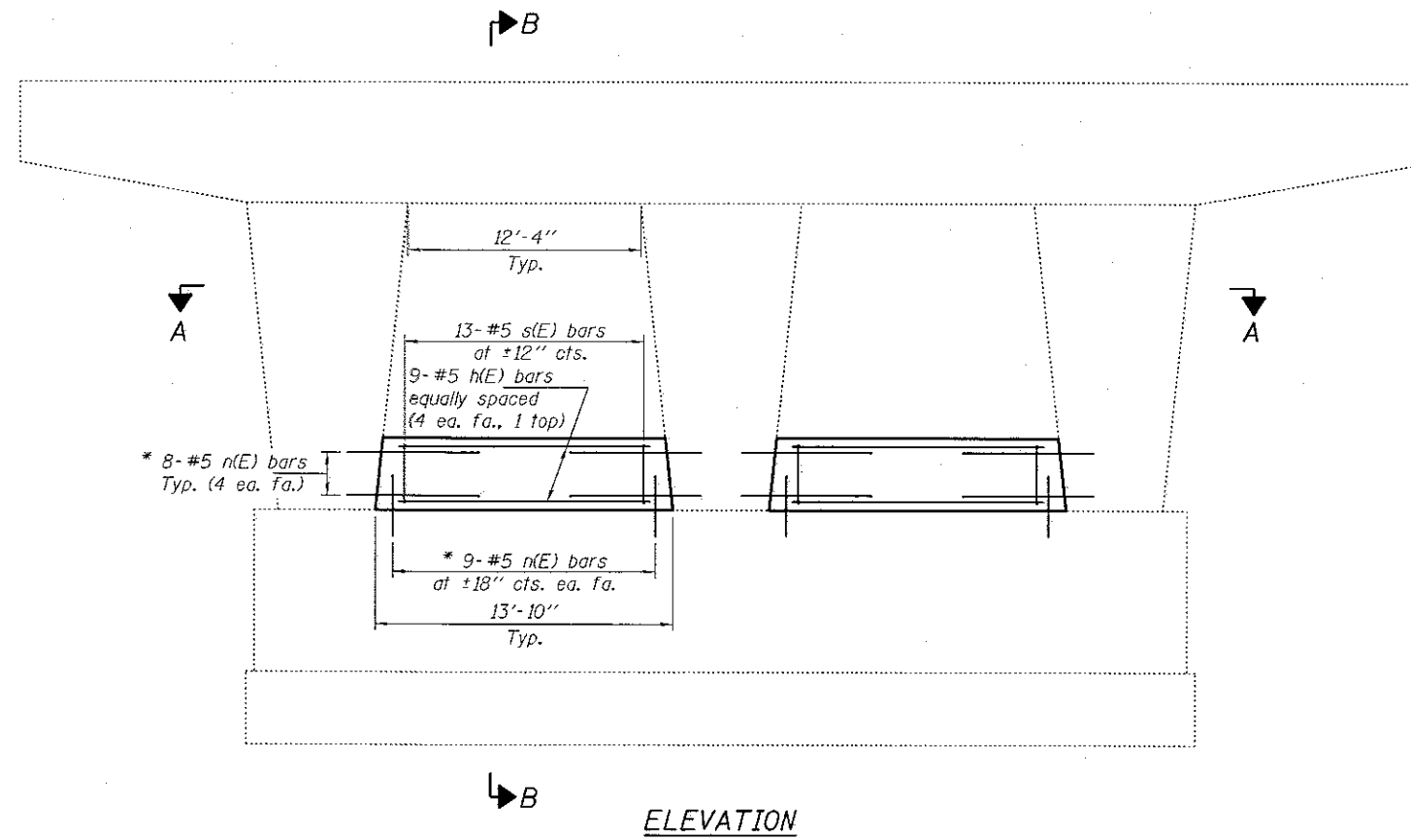


* Epoxy grout n(E) bars in 9" min. holes according to Article 584 of the Standard Specifications.

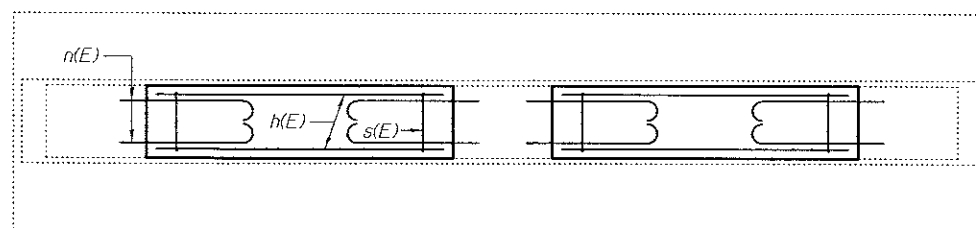
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(84-4, 84-3, 84-9, 110IRS)	SANGAMON	85	84
				CONTRACT NO. 72J83
ILLINOIS FED. AID PROJECT				

GENERAL NOTES

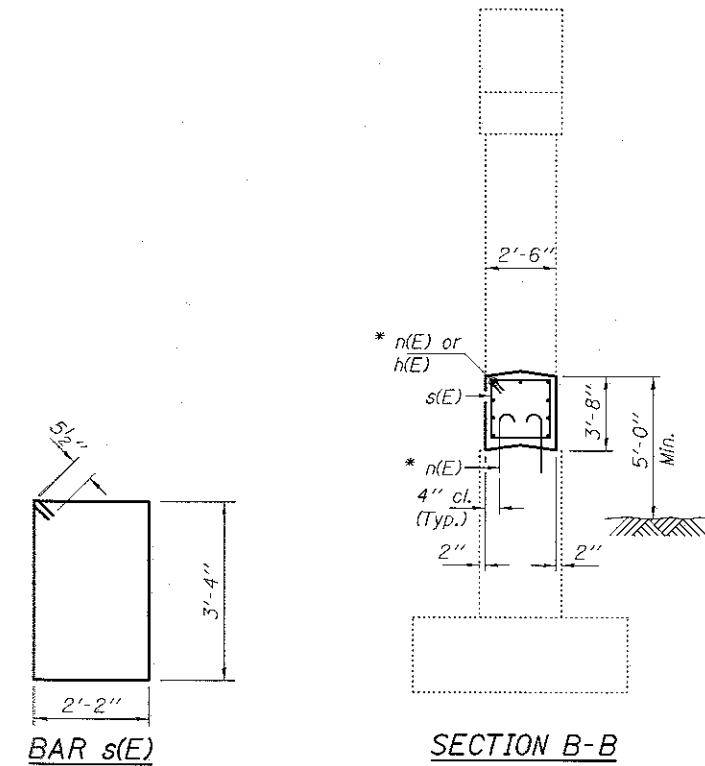
The cost of epoxy grouting threaded rods shall be included with Reinforcement Bars, Epoxy Coated.
 Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
 Reinforcement bars designated (E) shall be epoxy coated.



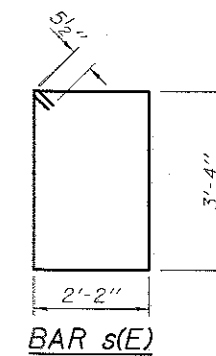
ELEVATION



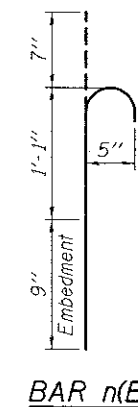
SECTION A-A



SECTION B-B



BAR s(E)

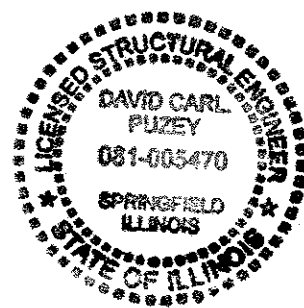


BAR n(E)

* Epoxy grout n(E) bars in 9" min. holes according to Article 584 of the Standard Specifications.

TOTAL BILL OF MATERIAL

Bar	No.	Size	Length	Shape	
n(E)	18	#5	8'-5"	—	
n(E)	68	#5	2'-5"	U	
s(E)	26	#5	11'-11"	□	
Concrete Structures				Cu. Yd.	9.3
Reinforcement Bars, Epoxy Coated				Pound	652



EXPIRES 11-30-2018

DESIGNED: <i>Victor H. Veliz</i>	EXAMINED: <i>Tony A. Doherty</i>	DATE: NOVEMBER 15, 2017
CHECKED: <i>[Signature]</i>	PASSED: <i>[Signature]</i>	REVISOR: _____
DRAWN: <i>baliva</i>	ENGINEER OF BRIDGES AND STRUCTURES	REVISOR: _____
CHECKED: <i>[Signature]</i>		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER 1 CRASHWALL EXTENSION
SN 084-0092

SHEET NO. 1 OF 1 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(84-4, 84-3, 84-9, 110)RS	SANGAMON	85	85
			CONTRACT NO. 72JB3	
ILLINOIS FED. AID PROJECT				