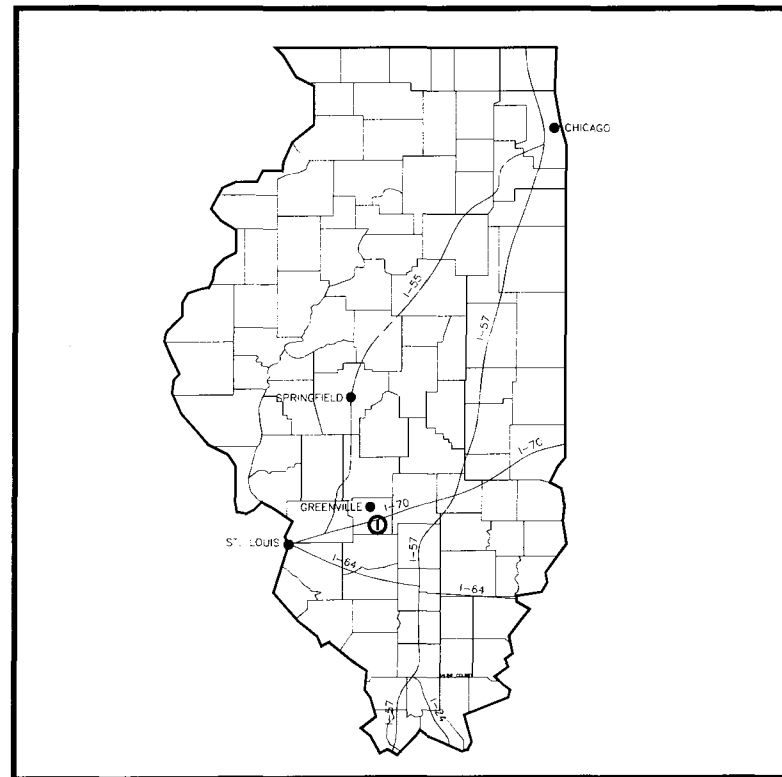


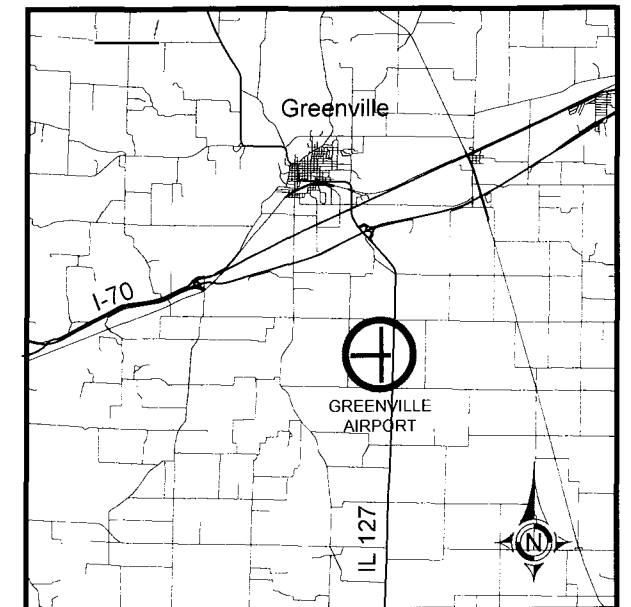
LETTING ITEM NO. 03A
JANUARY 19, 2018 LETTING

CONSTRUCTION PLANS FOR GREENVILLE AIRPORT REHABILITATE AIRCRAFT PARKING APRON DESIGNATED R1

GR008
TOTAL SHEETS: 13



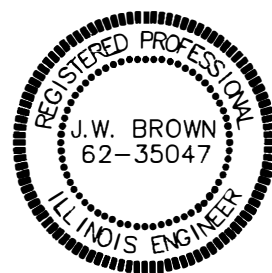
LOCATION MAP



VICINITY MAP

FINAL SUBMITTAL SEPTEMBER 28, 2017
ILLINOIS PROJECT NUMBER: GRE-4561
SBG PROJECT NUMBER: 3-17-SBGP-120/133/139

GREENVILLE, ILLINOIS
BOND COUNTY



28 Sep 2017 - 8:45am X:\2016\16114\ac16114 Cover.dwg: Layout Tab 'Layout1'

DESIGN INFORMATION

- CRITICAL AIRCRAFT = CESSNA CITATION III
- AIRCRAFT APPROACH CATEGORY (AAC) = B
- AIRPLANE DESIGN GROUP (ADG) = II
- TAXIWAY DESIGN GROUP (TDG) = 2
- DEPARTURE WEIGHT = 22,000 LBS.

BROWN AND ROBERTS, INC.
CONSULTING ENGINEER
PRESIDENT

SUBMITTED BY: *Jim W. Brown*
JIM W. BROWN

DATE SUBMITTED: 9/28/17

LICENSE NUMBER: 184-002518

LICENSE EXPIRATION DATE: APRIL 30, 2019

PLANS PREPARED BY:



BROWN AND ROBERTS, INC.
1 WESTRIDGE ROAD
HARRISBURG, IL. 62946
(618) 252-8111

GREENVILLE AIRPORT AUTHORITY
CHAIRMAN

APPROVED BY: *Byron V. Vothkamp* 9/28/17
DATE

SECRETARY

ATTESTED BY: *Randee Vesel* 9/28/17
DATE

SUMMARY OF QUANTITIES			
ITEM NUMBER	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY
AR110501	1-WAY CONC. ENCASED DUCT	L.F.	90
AR150520	MOBILIZATION	L. S.	1
AR152511	SUBGRADE REPAIR	S.Y.	450
AR162504	CLASS E FENCE 4'	L.F.	410
AR162606	CLASS E GATE - 6'	EACH	4
AR162620	CLASS E GATE - 20'	EACH	1
AR162900	REMOVE CLASS E FENCE	L.F.	340
AR162910	REMOVE CLASS E GATE	EACH	5
AR201670	CRACK CONTROL FABRIC	S.Y.	110
AR401613	BIT. SURF. CSE. - METHOD I, SUPERPAVE	TON	525
AR401650	BITUMINOUS PAVEMENT MILLING	S.Y.	4470
AR403613	BIT. BASE CSE. - METHOD I, SUPERPAVE	TON	525
AR510510	TIE DOWN	EACH	29
AR510900	REMOVE TIE DOWN	EACH	29
AR602510	BITUMINOUS PRIME COAT	GAL.	230
AR603510	BITUMINOUS TACK COAT	GAL.	1570
AR620520	PAVEMENT MARKING - WATERBORNE	S.F.	420

INDEX OF SHEETS	
SHEET NUMBER	DESCRIPTION
1	COVER SHEET
2	SUMMARY OF QUANTITIES & INDEX OF SHEETS
3	CONSTRUCTION SAFETY & PHASING PLAN
4	TYPICAL SECTIONS
5	APRON PAVING PLAN
6	TIE DOWN REMOVAL PLAN
7	TIE DOWN PLAN
8	TIE DOWN DETAILS
9	FENCE DETAILS 1
10	FENCE DETAILS 2
11	APRON CROSS SECTIONS 1
12	APRON CROSS SECTIONS 2
13	APRON CROSS SECTIONS 3

SCOPE OF WORK

THE PROJECT SCOPE CONSISTS OF THE REHABILITATION OF AIRCRAFT PARKING APRON R1 ALONG WITH OTHER NECESSARY AND RELATED WORK.

PROPOSED SAFETY PLAN

GENERAL- THE GREENVILLE AIRPORT CURRENTLY HAS A TURF RUNWAY 9-27 WHICH IS 2822 FT. x 250 FT. AND A PAVED RUNWAY 18-36 WHICH IS 4001 FT. x 75 FT.

IT IS ANTICIPATED THAT BOTH RUNWAYS WILL REMAIN OPEN FOR THE DURATION OF THE PROJECT, AS NO CONSTRUCTION ACTIVITIES ASSOCIATED WITH THIS PROJECT WILL BE WITHIN 200' OF THE RUNWAY CENTERLINES. ANY WORK WITHIN 200' OF THE CENTERLINE OF A RUNWAY WILL REQUIRE CLOSURE OF THAT RUNWAY.

THE CONTRACTOR SHALL SUBMIT A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) CONFIRMING COMPLIANCE WITH THE CONSTRUCTION SAFETY PHASING PLAN (CSPP) PRIOR TO THE ISSUANCE OF THE NOTICE TO PROCEED AS SPECIFIED IN FAA AC 150/5370-2.

CONTRACTOR'S RESPONSIBILITIES

IDENTIFICATION- THE CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE PROPERLY MARKED WITH 3-FOOT SQUARE INTERNATIONAL ORANGE AND WHITE CHECKERED FLAGS ANYTIME THEY ARE ON AIRPORT PROPERTY.

THE CONTRACTOR AND HIS EMPLOYEES SHALL BE RESTRICTED TO THE WORK AREA.

EQUIPMENT PARKING AND STORAGE- THE CONTRACTOR'S EQUIPMENT PARKING, STORAGE, AND EMPLOYEE PARKING WILL BE AT THE LOCATION SHOWN ON THIS SHEET. ONLY CONTRACTOR VEHICLES AND EQUIPMENT REQUIRED FOR CONSTRUCTION WILL BE ALLOWED OUTSIDE THIS AREA.

BARRICADES AND TRAFFIC CONES- IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN BARRICADES AND TRAFFIC CONES AS REQUIRED AND AS DIRECTED BY THE RESIDENT ENGINEER. BARRICADES, THEIR MAINTENANCE, PLACEMENT, AND REMOVAL WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

ANY WORK WITHIN 65.5' OF A TAXIWAY OR 57.5' OF A TAXILANE CENTERLINE SHALL REQUIRE CLOSURE OF THAT TAXIWAY OR TAXILANE USING LIGHTED BARRICADES AS SPECIFIED. NO WORK SHALL OCCUR WITHIN 65.5' OF THE CENTERLINE OF AN ACTIVE TAXIWAY OR 57.5' OF AN ACTIVE TAXILANE.

THE CONTRACTOR WILL BE RESPONSIBLE FOR REPAIRING ANY DAMAGE TO EXISTING PAVEMENTS CAUSED BY HIS PERSONNEL OR EQUIPMENT.

HAUL ROUTE AND EQUIPMENT PARKING

THE CONTRACTOR WILL USE THE DESIGNATED HAUL ROUTE AND EQUIPMENT PARKING AREA SHOWN ON THIS SAFETY PLAN. THE PROPOSED EQUIPMENT PARKING AREA WILL BE APPROXIMATELY 70-FT BY 150-FT. THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN THE PROPOSED HAUL ROUTE AND PARKING AREA THROUGHOUT THE COURSE OF THE PROJECT. AT THE CONCLUSION OF THE PROJECT, ALL AREAS DISTURBED WILL BE RESTORED AS NEEDED TO ITS ORIGINAL STATE. RESTORATION OF THE HAUL ROUTE AND EQUIPMENT PARKING AREA WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

UTILITY NOTE

THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING ALL UTILITY COMPANIES AND ORGANIZATIONS THAT HAVE LINES OR CONDUITS IN THE PROPOSED WORK AREA. ALL LINES AND CONDUITS SHALL BE LOCATED AND IDENTIFIED FOR DEPTH BEFORE ANY EXCAVATION BEGINS. THE CONTRACTOR SHALL CALL JULIE (1-800-892-0123) TO ACCOMPLISH THESE REQUIREMENTS. THE CONTRACTOR IS RESPONSIBLE FOR IDENTIFYING ALL NON-JULIE UTILITIES LOCATED WITHIN THE PROPOSED CONSTRUCTION LIMITS. THESE UTILITIES ARE TO BE LOCATED PRIOR TO THE START OF CONSTRUCTION.

J.U.L.I.E. INFORMATION

COUNTY.....BOND
 CITY.....GREENVILLE (4 MI SOUTHEAST)
 TOWNSHIP.....CENTRAL
 SECTION NO.....36
 NEAREST MAJOR ROAD INTERSECTION...ILLINOIS RT. 127 & SKY LANE
 AIRPORT ADDRESS...GREENVILLE AIRPORT
 1374 SKY LANE
 GREENVILLE, IL 62246

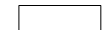





HEIGHT OF CONSTRUCTION EQUIPMENT

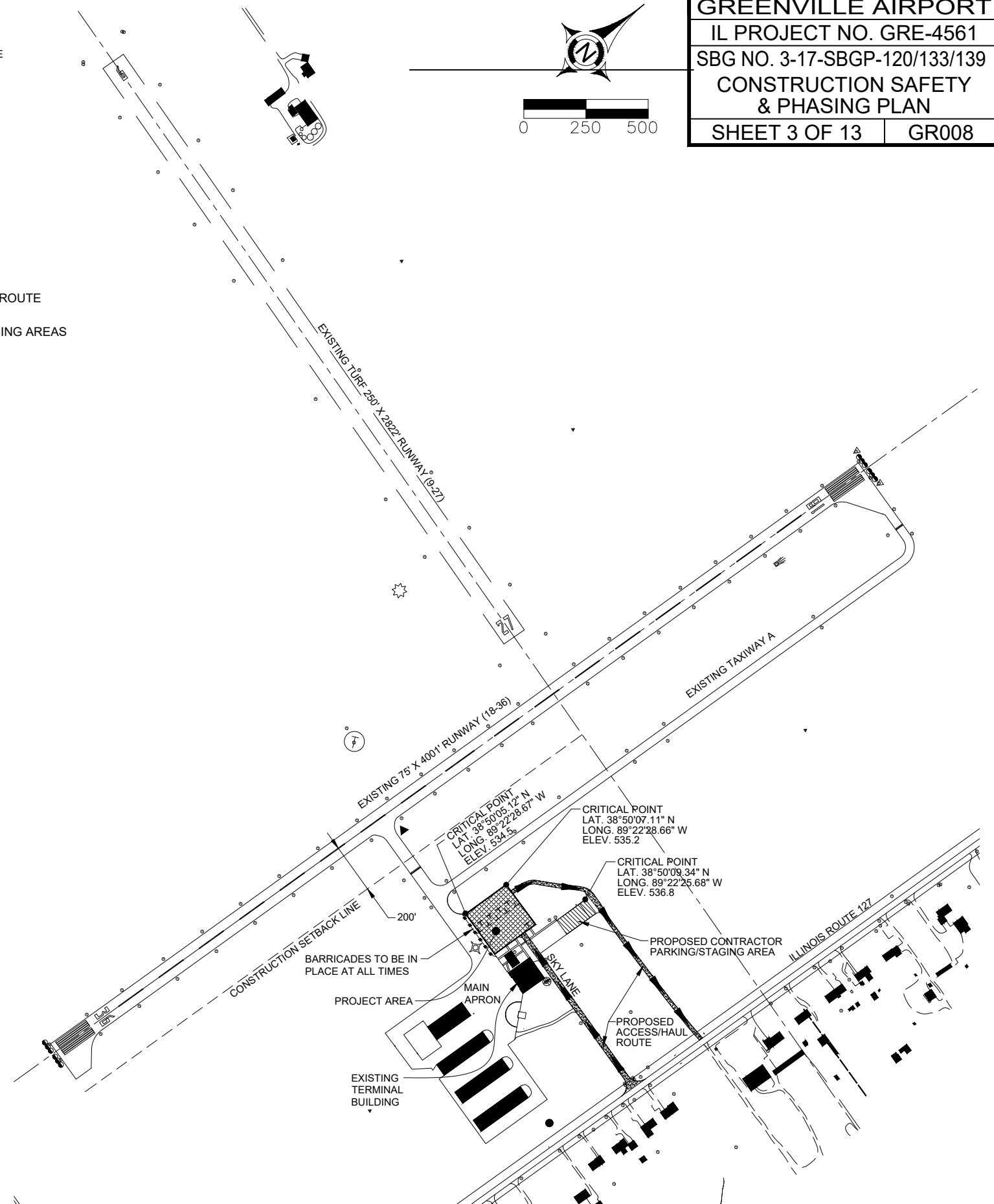
THE MAXIMUM ANTICIPATED HEIGHT OF THE CONSTRUCTION EQUIPMENT IS 20 FEET. THE TALLEST EQUIPMENT IS EXPECTED TO BE A SEMI-TRUCK WITH TRAILER IN THE UP POSITION.

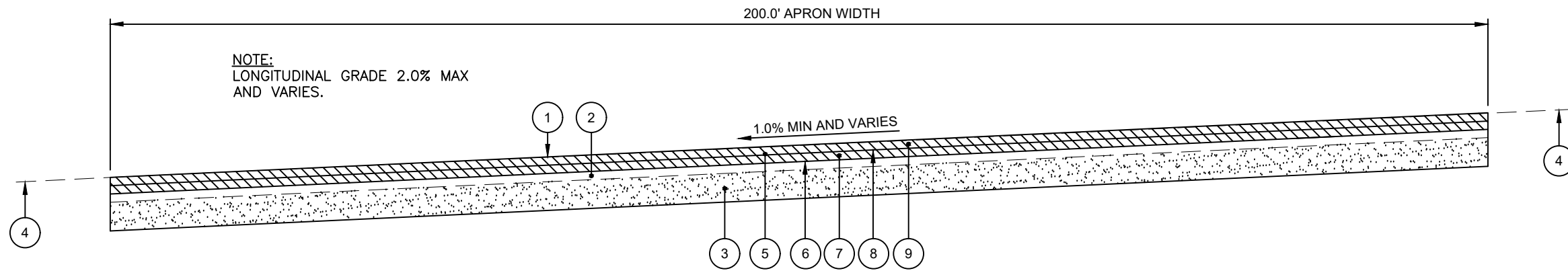
AIRPORT SECURITY

AIRPORT SECURITY WILL BE MAINTAINED AT ALL TIMES. THE PROPOSED HAUL ROUTE SHOWN ON THIS SAFETY PLAN IS THE ONLY ACCESS CONTRACTOR EQUIPMENT AND PERSONNEL WILL BE ALLOWED TO USE. THE CONTRACTOR SHALL PROVIDE BARRICADES AT THIS ACCESS AND ENSURE THE BARRICADES ARE IN PLACE AT THE END OF EACH WORKING DAY.

LEGEND

-  EXISTING PAVEMENT
-  PROPOSED PAVEMENT IMPROVEMENTS
-  EXISTING BUILDINGS
-  PROPOSED CONTRACTOR ACCESS AND HAUL ROUTE
-  PROPOSED CONTRACTOR PARKING AND STAGING AREAS
-  BARRICADES



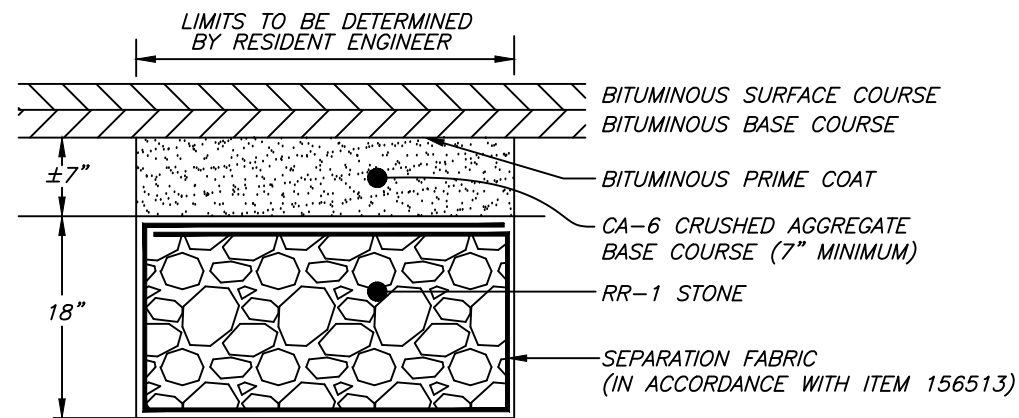


TYPICAL SECTION (EAST - WEST)

NO SCALE

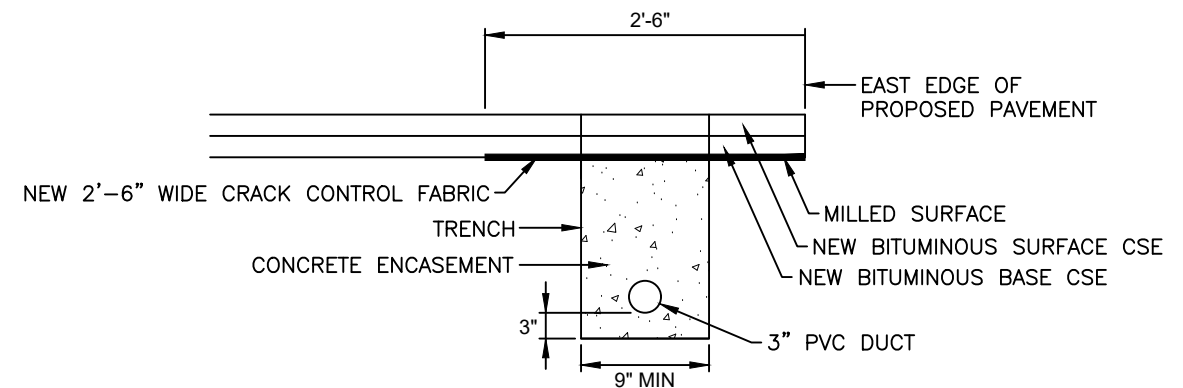
LEGEND

- | | | | |
|---|--|---|---|
| 1 | EXISTING BITUMINOUS SURFACE | 6 | AR603510 - BITUMINOUS TACK COAT |
| 2 | EXISTING BITUMINOUS BASE TO REMAIN | 7 | AR403613 - BITUMINOUS BASE COURSE - METHOD I, SUPERPAVE (2") |
| 3 | EXISTING AGGREGATE BASE TO REMAIN | 8 | AR603510 - BITUMINOUS TACK COAT |
| 4 | EXISTING TURF SHOULDER | 9 | AR401613 - BITUMINOUS SURFACE COURSE - METHOD I, SUPERPAVE (2") |
| 5 | AR401650 - BITUMINOUS PAVEMENT MILLING | | |



SUBGRADE REPAIR

NO SCALE



1-WAY CONCRETE ENCASED DUCT NOTES:

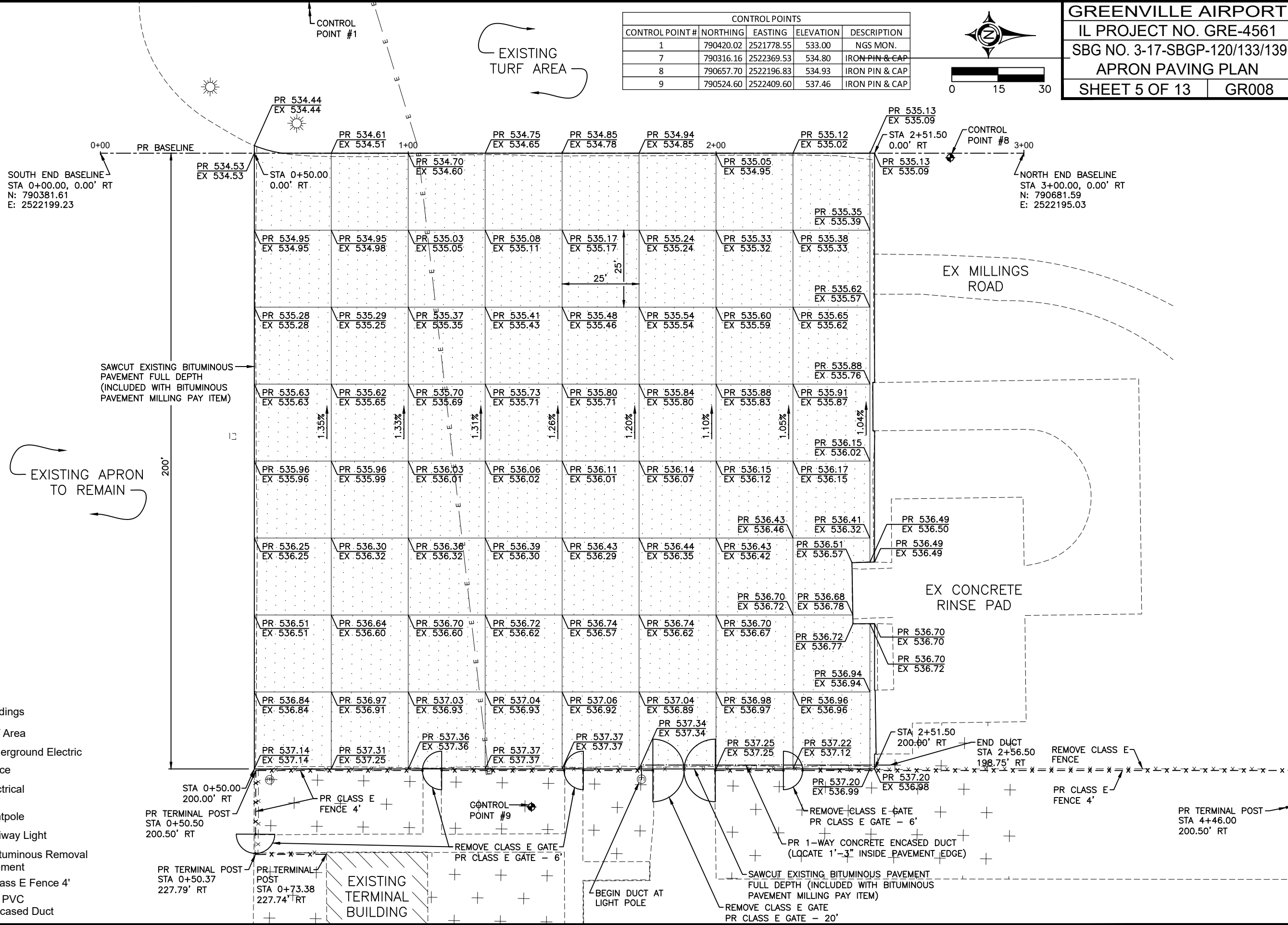
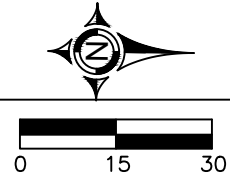
1. PAVEMENT REMOVAL, EXCAVATION AND DISPOSAL, AND BACKFILL SHALL BE INCIDENTAL TO 1-WAY CONCRETE ENCASED DUCT PAY ITEM.
2. BITUMINOUS SURFACE AND BASE COURSES AND CRACK CONTROL FABRIC TO BE PAID FOR SEPARATELY.
3. DUCT SHALL BE BURIED 18" MINIMUM DEPTH TO PROPOSED GRADE.

1-WAY CONCRETE ENCASED DUCT DETAIL

NO SCALE

GREENVILLE AIRPORT
IL PROJECT NO. GRE-4561
SBG NO. 3-17-SBGP-120/133/139
APRON PAVING PLAN
SHEET 5 OF 13 | **GR008**

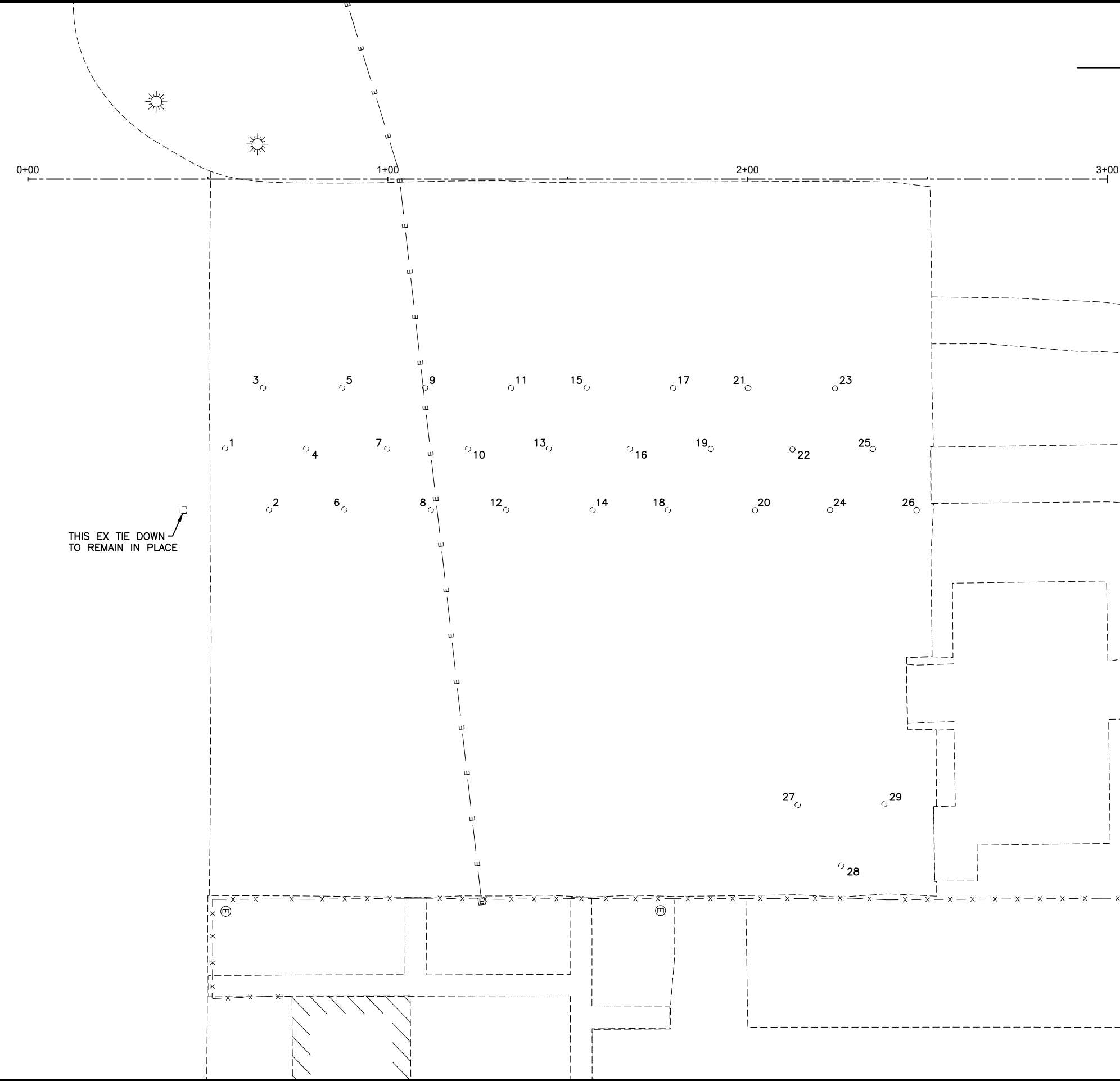
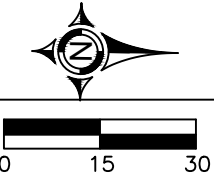
CONTROL POINTS				
CONTROL POINT #	NORTHING	EASTING	ELEVATION	DESCRIPTION
1	790420.02	2521778.55	533.00	NGS MON.
7	790316.16	2522369.53	534.80	IRON PIN & CAP
8	790657.70	2522196.83	534.93	IRON PIN & CAP
9	790524.60	2522409.60	537.46	IRON PIN & CAP



LEGEND

- Existing Buildings
- Existing Turf Area
- Existing Underground Electric
- Existing Fence
- Existing Electrical Pull Box
- Existing Lightpole
- Existing Taxiway Light
- Proposed Bituminous Removal and Replacement
- Proposed Class E Fence 4'
- Proposed 3" PVC Concrete Encased Duct

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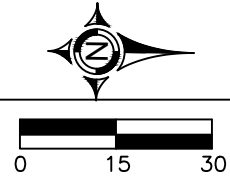
TIE DOWN REMOVAL SCHEDULE		
TIE DOWN #	STATION	OFFSET
* 1	0+55	75' RT
* 2	0+67	92' RT
* 3	0+65.5	58' RT
* 4	0+77.5	75' RT
** 5	0+87.5	58' RT
* 6	0+88	92' RT
* 7	1+00	75' RT
* 8	1+12	92' RT
* 9	1+10.5	58' RT
* 10	1+22.5	75' RT
* 11	1+34.5	58' RT
* 12	1+33	92' RT
* 13	1+45	75' RT
* 14	1+57	92' RT
* 15	1+55.5	58' RT
* 16	1+67.5	75' RT
* 17	1+79.5	58' RT
* 18	1+78	92' RT
* 19	1+90	75' RT
* 20	2+02	92' RT
* 21	2+00.5	58' RT
* 22	2+12.5	75' RT
* 23	2+24.5	58' RT
** 24	0+68	173' RT
** 25	0+80	190' RT
** 26	0+92	173' RT
** 27	1+18	173' RT
** 28	1+30	190' RT
** 29	1+42	173' RT

* EXISTING TIE DOWN TO BE REMOVED AND REPLACED WITH NEW TIE DOWN IN SAME LOCATION.

** EXISTING TIE DOWN TO BE REMOVED AND PERMANENTLY BACKFILLED WITH COMPACTED CA-6.

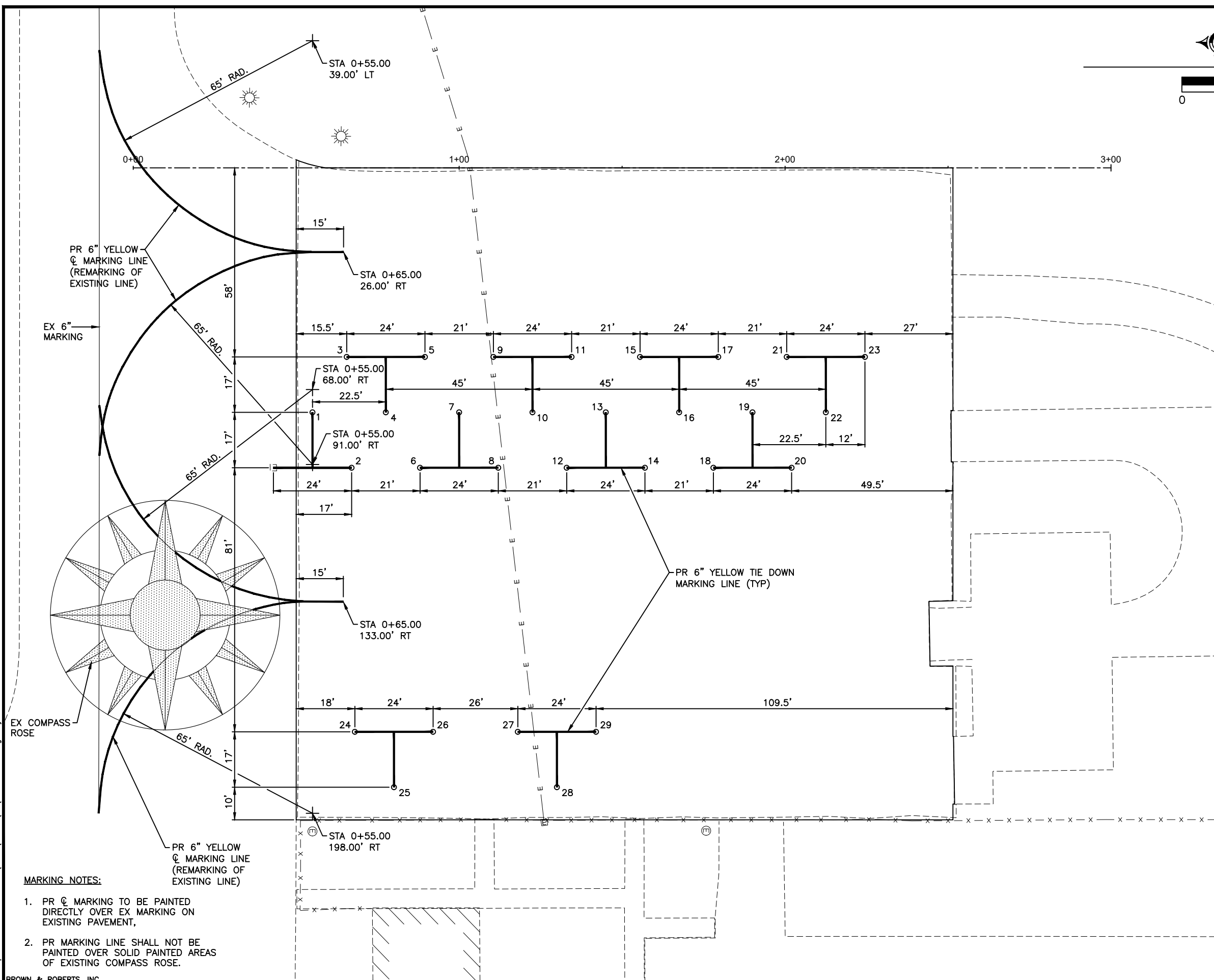
THIS EX TIE DOWN TO REMAIN IN PLACE

GREENVILLE AIRPORT
IL PROJECT NO. GRE-4561
SBG NO. 3-17-SBGP-120/133/139
TIE DOWN PLAN
SHEET 7 OF 13 **GR008**



PROPOSED TIE DOWN SCHEDULE				
TIE DOWN #	STATION	OFFSET	NORTHING	EASTING
* 1	0+55.00	75.00' RT	790437.66	2522273.45
* 2	0+67.00	92.00' RT	790449.90	2522290.28
* 3	0+65.50	58.00' RT	790447.92	2522256.31
* 4	0+77.50	75.00' RT	790460.16	2522273.14
5	0+89.50	58.00' RT	790471.92	2522255.97
* 6	0+88.00	92.00' RT	790470.89	2522289.99
* 7	1+00.00	75.00' RT	790482.65	2522272.82
* 8	1+12.00	92.00' RT	790494.89	2522289.65
* 9	1+10.50	58.00' RT	790492.92	2522255.68
* 10	1+22.50	75.00' RT	790505.15	2522272.51
* 11	1+34.50	58.00' RT	790516.91	2522255.34
* 12	1+33.00	92.00' RT	790515.89	2522289.36
* 13	1+45.00	75.00' RT	790527.65	2522272.19
* 14	1+57.00	92.00' RT	790539.89	2522289.02
* 15	1+55.50	58.00' RT	790537.91	2522255.05
* 16	1+67.50	75.00' RT	790550.15	2522271.88
* 17	1+79.50	58.00' RT	790561.91	2522254.71
* 18	1+78.00	92.00' RT	790560.88	2522288.73
* 19	1+90.00	75.00' RT	790572.65	2522271.56
* 20	2+02.00	92.00' RT	790584.88	2522288.39
* 21	2+00.50	58.00' RT	790582.91	2522254.42
* 22	2+12.50	75.00' RT	790595.14	2522271.25
* 23	2+24.50	58.00' RT	790606.90	2522254.08
24	0+68.00	173.00' RT	790452.03	2522371.26
25	0+80.00	190.00' RT	790464.27	2522388.09
26	0+92.00	173.00' RT	790476.03	2522370.92
27	1+18.00	173.00' RT	790502.02	2522370.56
28	1+30.00	190.00' RT	790514.26	2522387.39
29	1+42.00	173.00' RT	790526.02	2522370.22

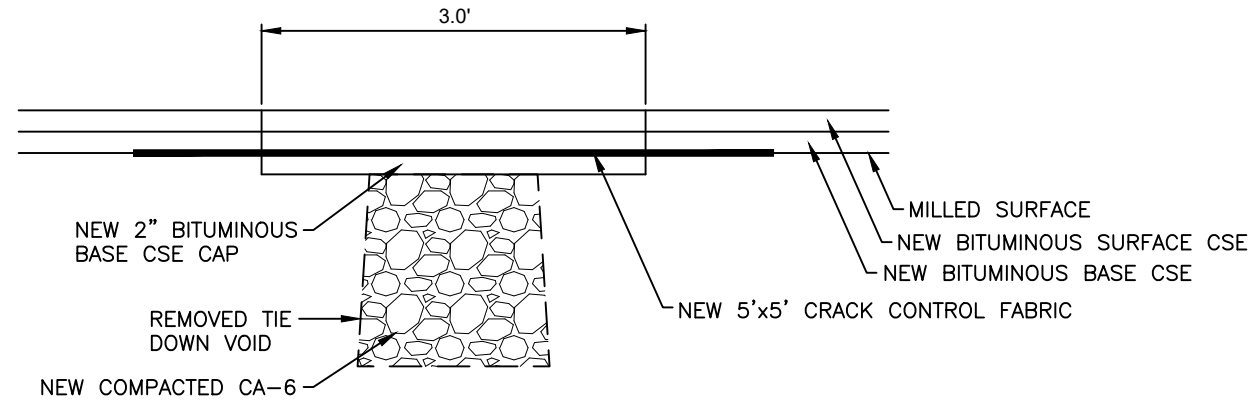
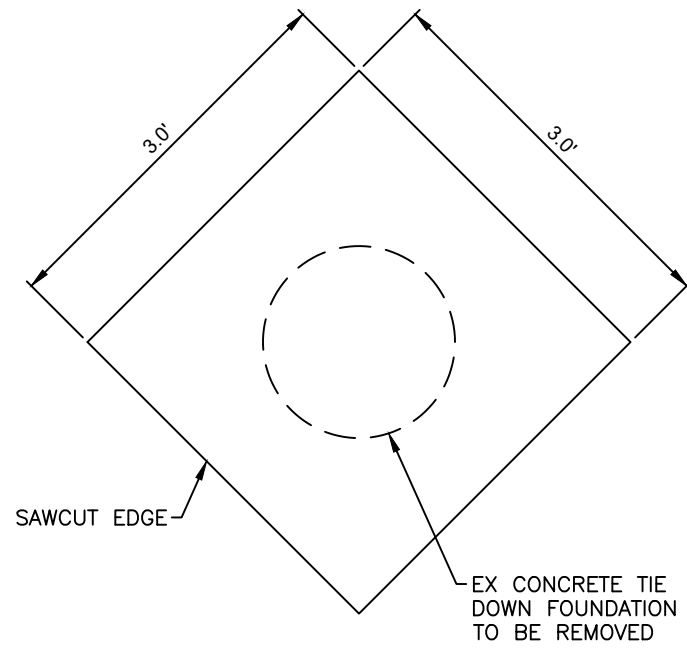
* NEW TIE DOWN WILL BE IN SAME LOCATION AS EXISTING TIE DOWN.



MARKING NOTES:

1. PR CL MARKING TO BE PAINTED DIRECTLY OVER EX MARKING ON EXISTING PAVEMENT,
2. PR MARKING LINE SHALL NOT BE PAINTED OVER SOLID PAINTED AREAS OF EXISTING COMPASS ROSE.

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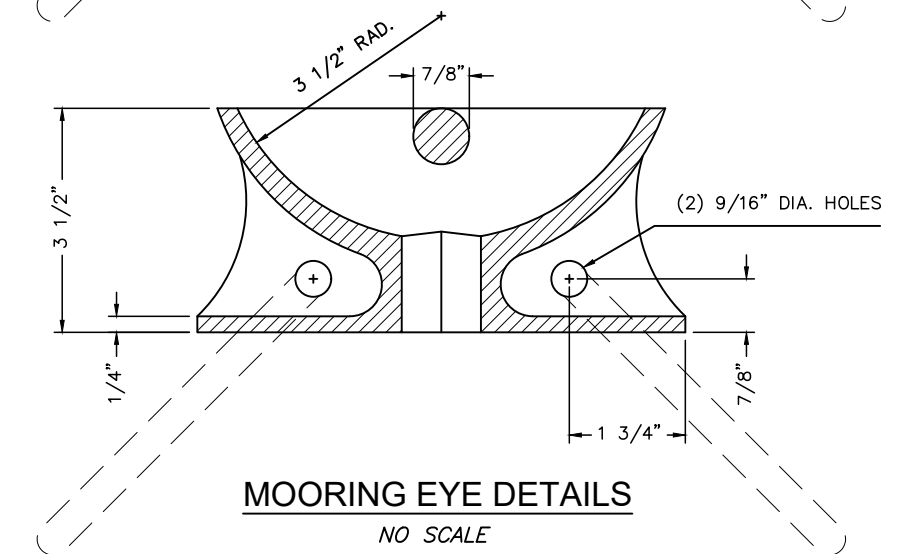
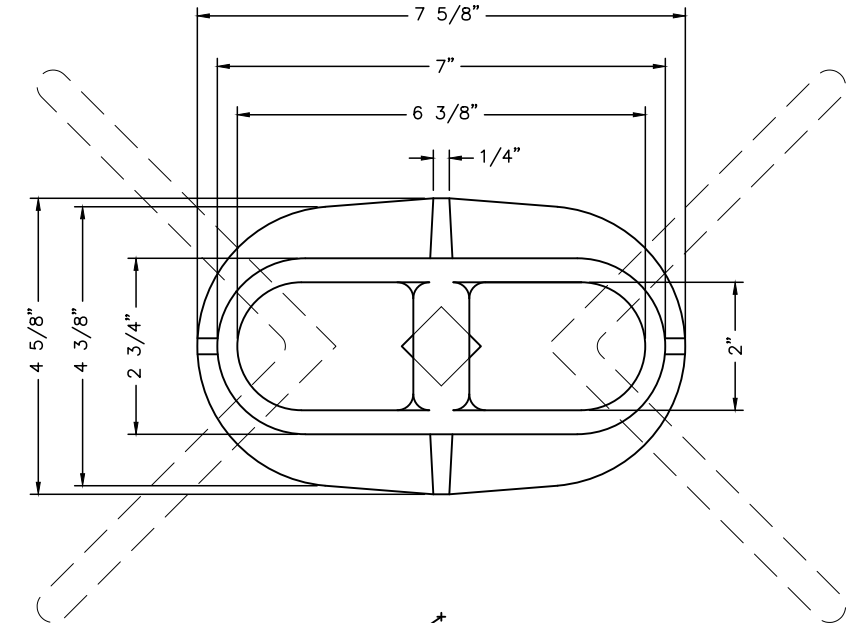


TIE DOWN REMOVAL/BACKFILL NOTES:

1. SAWCUT, PAVEMENT REMOVAL, EXCAVATION AND DISPOSAL, AND BACKFILL SHALL BE INCIDENTAL TO REMOVE TIE DOWN PAY ITEM.
2. BITUMINOUS SURFACE AND BASE COURSES AND CRACK CONTROL FABRIC TO BE PAID FOR SEPARATELY.
3. WHERE NEW TIE DOWNS WILL BE IN SAME LOCATION AS EXISTING TIE DOWNS, CONTRACTOR MAY BACKFILL VOID FROM TIE DOWN REMOVAL WITH SAND OR OTHER MATERIALS APPROVED BY THE RESIDENT ENGINEER IN LIEU OF COMPACTED CA-6.

TIE DOWN REMOVAL/BACKFILL DETAIL

NO SCALE

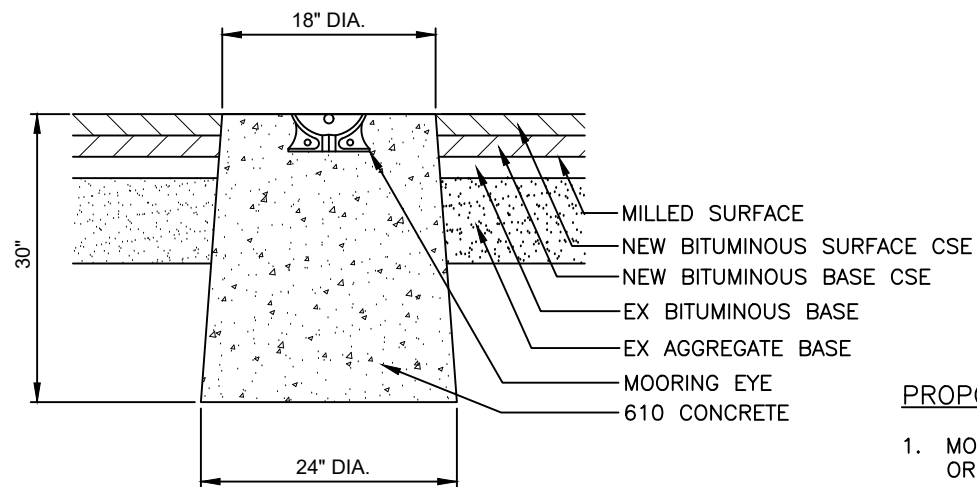
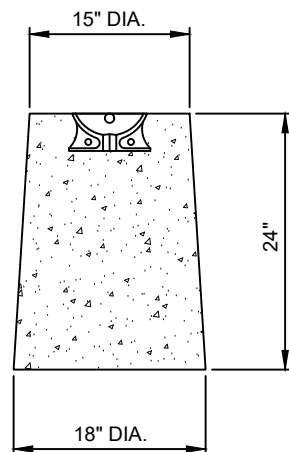


PROPOSED TIE DOWN/MOORING EYE NOTES:

1. MOORING EYE CASTINGS SHALL BE NEENAH R-3490, EJCO 00599300, OR APPROVED EQUIVALENT.
2. ANCHOR RODS SHALL BE #4 DEFORMED REBAR, 24" LONG AND SHALL BE BENT DOWNWARD AT 45°.
3. CONCRETE TIE DOWNS SHALL BE INSTALLED AFTER PAVING OPERATIONS ARE COMPLETE BY CORING THROUGH NEW PAVEMENT.
4. PROPOSED CONCRETE TIE DOWN FOUNDATIONS SHALL BE POURED AGAINST NATURAL UNDISTURBED EARTH. CARE SHALL BE TAKEN BY CONTRACTOR NOT TO OVER-EXCAVATE DURING REMOVAL OPERATIONS.

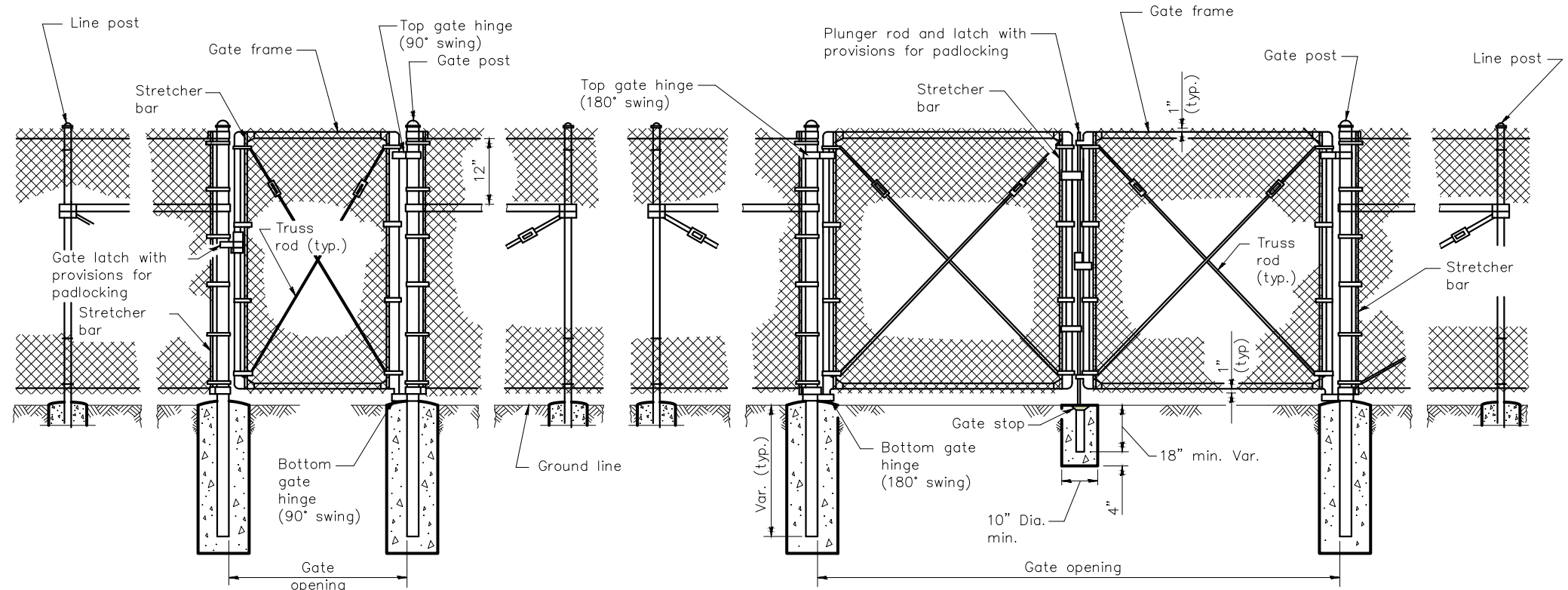
EXISTING TIE DOWN DETAIL

NO SCALE



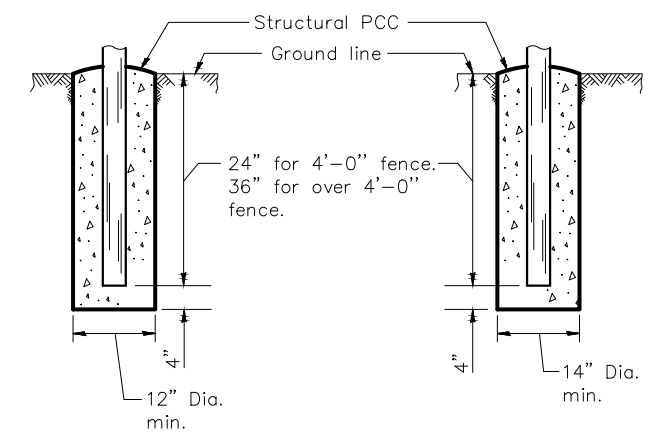
PROPOSED TIE DOWN DETAIL

NO SCALE



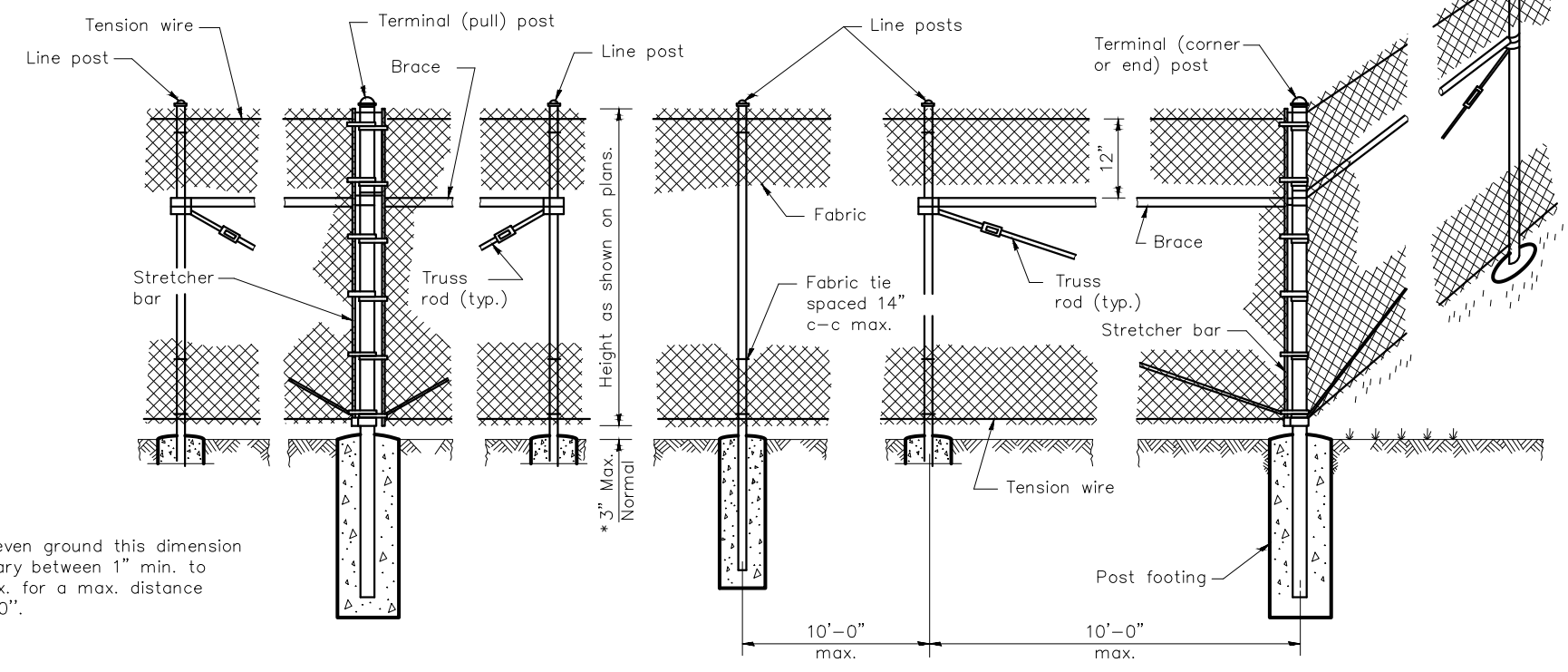
PEDESTRIAN GATE ARRANGEMENT

VEHICLE GATE ARRANGEMENT



FOOTING FOR LINE POST

FOOTING FOR GATE & TERMINAL POST



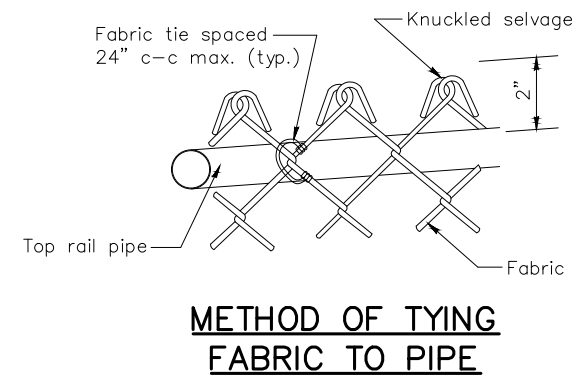
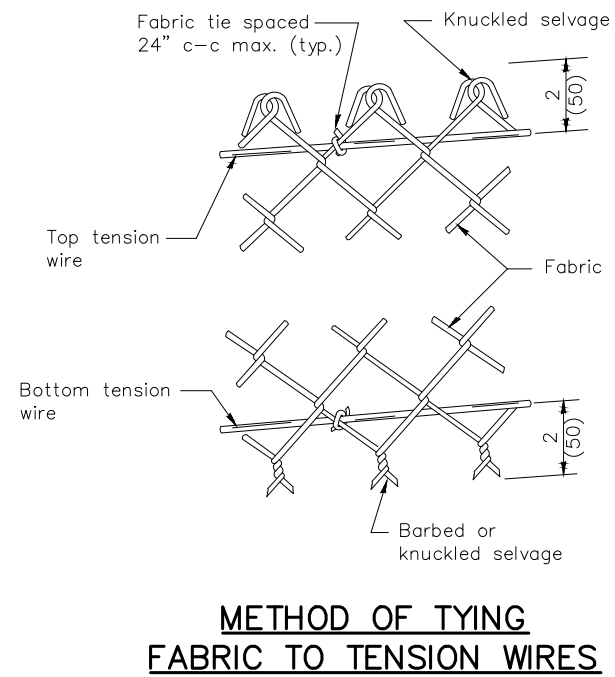
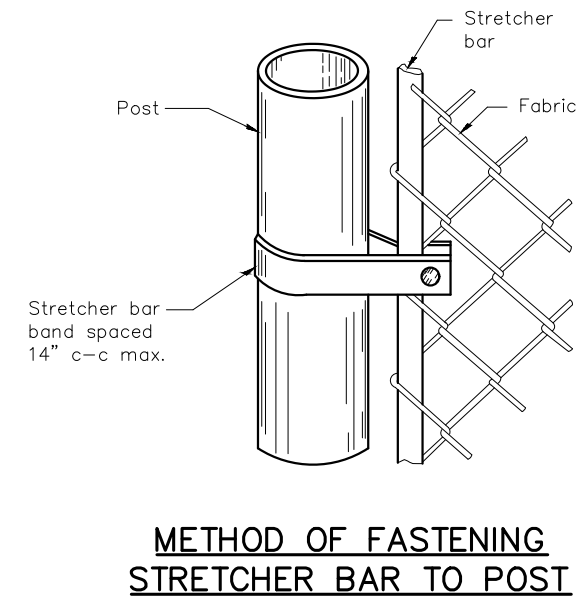
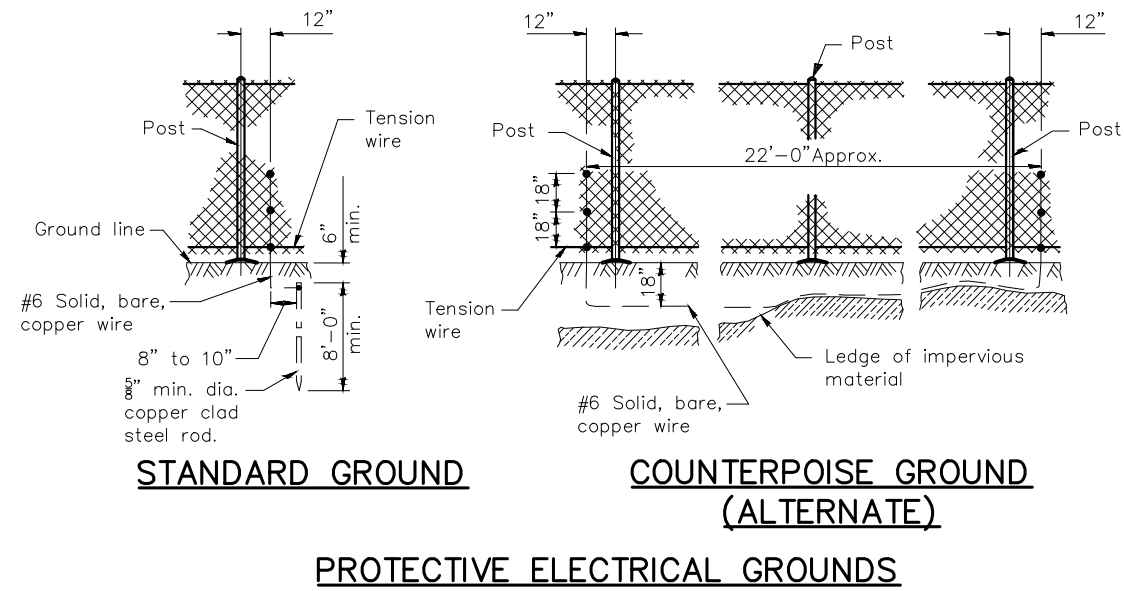
PULL POST ARRANGEMENT

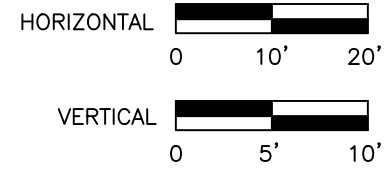
LINE POST ARRANGEMENT

CORNER OR END POST ARRANGEMENT

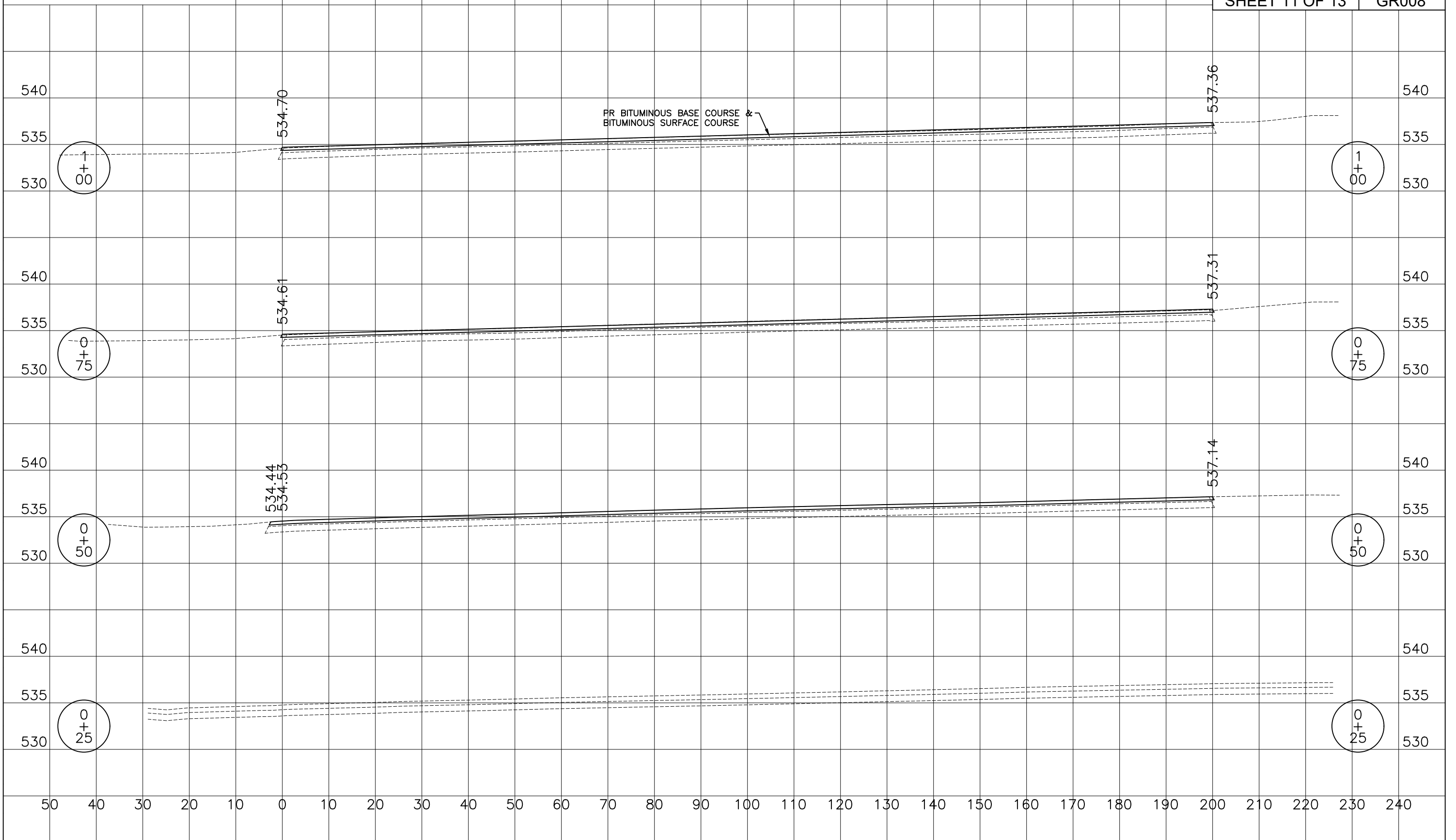
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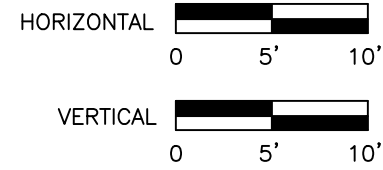
1. PULL POSTS SHALL BE PLACED AT LOCATIONS DETERMINED BY THE ENGINEER. THEY SHALL BE PLACED AT 660' INTERVALS BETWEEN POSTS TO WHICH THE ENDS OF THE FABRIC ARE CLAMPED OR MIDWAY BETWEEN SUCH POSTS WHEN THE DISTANCE IS LESS THAN 1320' AND GREATER THAN 660'.
2. CONTINUOUS FENCE SHALL BE GROUNDED AT INTERVALS NOT EXCEEDING 500 FT. EXCEPT THERE SHALL BE A GROUND NOT EXCEEDING 100 FT FROM GATE IN EACH SECTION OF THE FENCE ADJACENT TO THE GATE.
3. FENCE UNDER POWER LINES SHALL BE GROUNDED BY THREE GROUNDS, ONE DIRECTLY UNDER THE CROSSING AND ONE ON EACH SIDE 25 TO 50 FT AWAY. A SINGLE GROUND SHALL BE LOCATED DIRECTLY UNDER EACH TELEPHONE WIRE OR CABLE CROSSING.
4. THE COUNTERPOISE SHALL BE USED ONLY WHERE IT IS IMPOSSIBLE TO DRIVE A GROUND ROD BECAUSE OF IMPERVIOUS EARTH STRUCTURES.
5. THE GROUND WIRE SHALL BE CONNECTED TO THE FABRIC AND THE GROUND ROD BY A MECHANICAL CLAMP OF CAST BRONZE BODY AND BRONZE OR STAINLESS STEEL BOLTS AND WASHERS. WHEN A TENSION WIRE IS REQUIRED, THE BOTTOM CONNECTION OF THE GROUND WIRE SHALL BE MADE TO THE TENSION WIRE.
6. ALL PROPOSED CLASS E FENCE SHALL MEET THE REQUIREMENTS OF IDOT STANDARD 664001 LATEST REVISION.



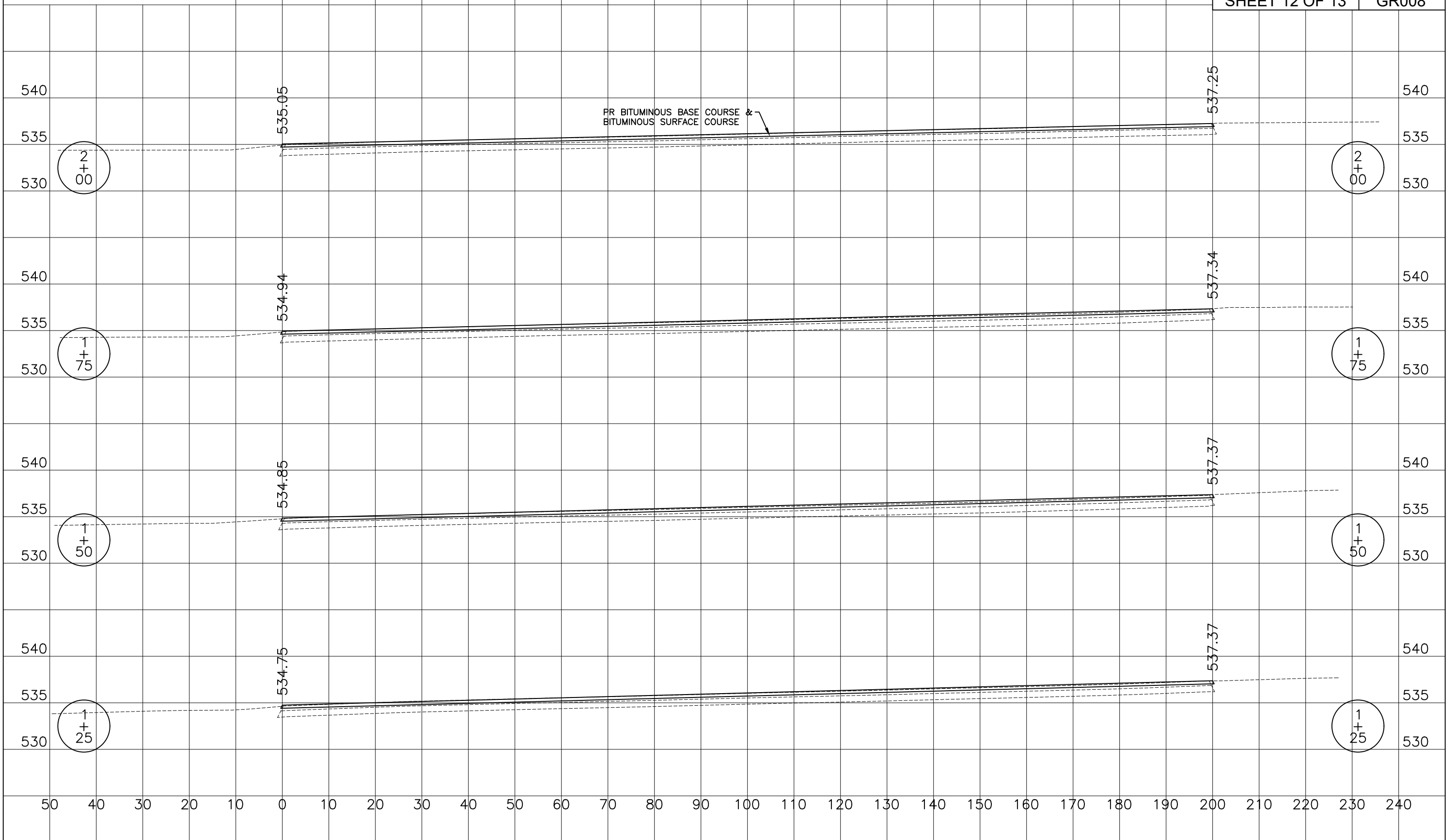


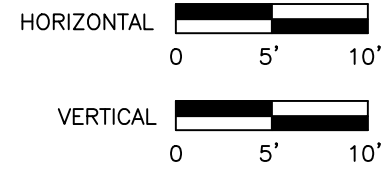
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