01-19-2024 LETTING ITEM 072

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	1
	ILLINOIS	CONTRACT NO. 62T87		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED
HIGHWAY PLANS

FAP ROUTE 368: PULASKI ROAD
COLUMBUS AVE TO US 12 (95TH STREET)
SECTION: FAP 0368 22 RS
PROJECT: NHPP–4STD(087)
SMART OVERLAY, ADA IMPROVEMENTS
COOK COUNTY

C–91–322–22

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THIS PROJECT IS LOCATED IN CITIES OF HOMETOWN AND CHICAGO, AND THE VILLAGES OF EVERGREEN PARK AND OAKLAWN

TRAFFIC DATA:

PULASKI ROAD (OTHER PRINCIPLE ARTERIAL):

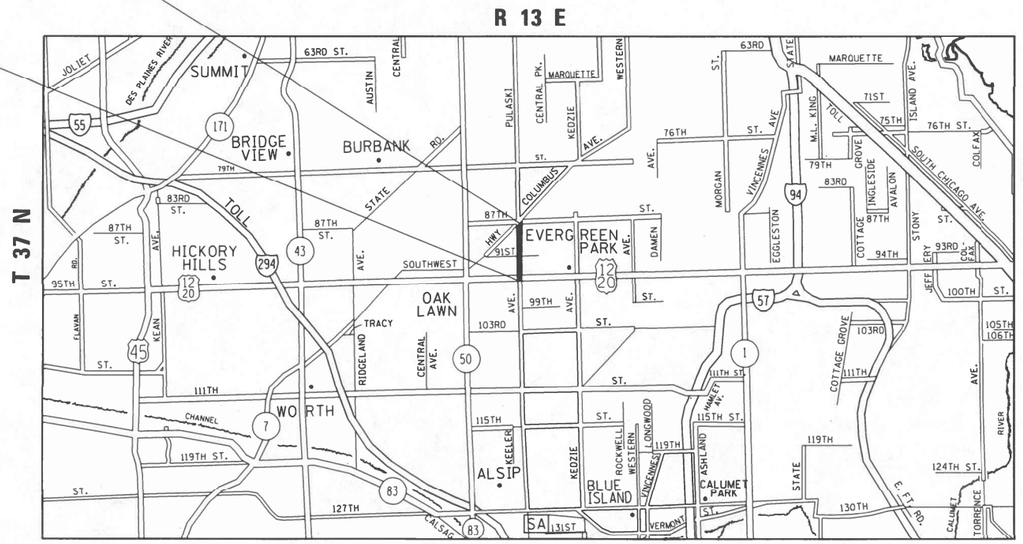
COLUMBUS AVE TO 87TH ST:
ADT (2022) = 19,900
SPEED LIMIT = 30 MPH

87TH ST TO 93RD ST:
ADT (2022) = 20,700
SPEED LIMIT = 40 MPH

93RD ST TO 95TH ST:
ADT (2022) = 20,700
SPEED LIMIT = 35 MPH

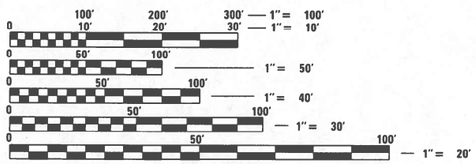
PROJECT ENDS
STA. 68 + 81

PROJECT BEGINS
STA. 14 + 45



WORTH TOWNSHIP

GROSS LENGTH = NET LENGTH = 5,436 FT. = 1.030 MILES



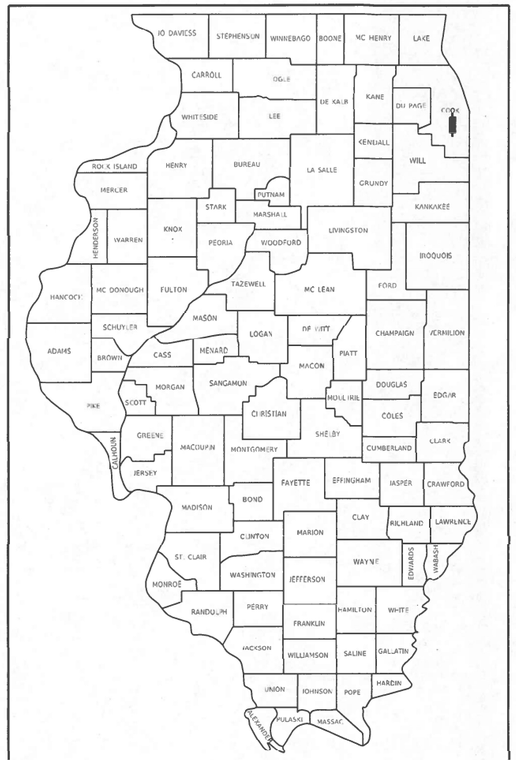
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS
1–800–892–0123
OR 811

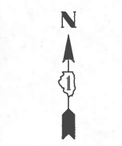
PROJECT ENGINEER: VESELIN VELICHKOV (847) 705–4432
PROJECT MANAGER: FAWAD AQUEEL

CONTRACT NO. 62T87

D–91–268–22



LOCATION OF SECTION INDICATED THUS: –



LOCATION MAP
(NOT TO SCALE)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED October 13 20 23
REGIONAL ENGINEER
December 8, 2023
ENGINEER OF DESIGN AND ENVIRONMENT
December 8, 2023
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

INDEX OF SHEETS

STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-11	PERPENDICULAR CURB RAMPS FOR SIDEWALK
424011-04	CORNER PARALLEL CURB RAMPS FOR SIDEWALK
424026-03	ENTRANCE / ALLEY PEDESTRIAN CROSSINGS
442201-03	CLASS C AND D PATCHES
602001-02	CATCH BASIN TYPE A
604001-05	FRAME AND LIDS, TYPE 1
606001-08	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701101-05	OFF-ROAD OPERATIONS, MULTILANE, 15' (4.5 M) TO 24" (600 MM) FROM PAVEMENT EDGE
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS ≤ 40 MPH
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701602-10	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-09	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS
781001-04	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
814001-03	HANDHOLES
857001-01	STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
873001-02	TRAFFIC SIGNAL GROUNDING & BONDING
878001-11	CONCRETE FOUNDATION DETAILS
880006-01	TRAFFIC SIGNAL MOUNTING DETAILS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUT FOR DETECTION LOOPS

GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES (48 HOURS NOTIFICATION REQUIRED).
2. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE UTILITY COMPANIES, CITY OF HOMETOWN, CITY OF CHICAGO, VILLAGE OF EVERGREEN PARK, AND VILLAGE OF OAKLAWN.
3. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSIONS FROM THE DEPARTMENT.
4. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
5. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.
6. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MININMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
7. THE RESIDENT ENGINEER SHALL CONTACT MR. FADI SULTAN, AREA TRAFFIC FIELD ENGINEER, VIA EMAIL AT PATRICE.HARRIS@ILLINOIS.GOV A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKING.
8. IT IS THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE A FIELD LABORATORY FOR USE FOR ANY ON-SITE TESTING BY THE ENVIRONMENTAL FIRM. NO TESTING OF ANY KIND, CONTAMINATED OR NON-CONTAMINATED, FLUID OR SOLID SHALL BE PERMITTED IN THE ENGINEER'S FIELD OFFICE.
9. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
10. ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
11. ALL PAVEMENT PATCHING, CURB AND GUTTER REMOVAL AND REPLACEMENT, DRAINAGE ADJUSTMENT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.
12. ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
13. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING LIMITS (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
14. FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
15. TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTERS AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.

GENERAL NOTES CONTINUE ON SHEET NO. 3

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	DRAWN -	REVISED -	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	
PLOT DATE = 10/24/2023	DATE -	REVISED -	

INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES
PULASKI ROAD (COLUMBUS AVENUE TO US 12 (95TH STREET))
SCALE: | SHEET OF SHEETS | STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	2
		CONTRACT NO. 62T87		
	ILLINOIS	FED. AID PROJECT		

GENERAL NOTES (CONTINUED)

16. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT NOT EXCEED 1 1/2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER. A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MIN. 1:3 (V:H).

17. LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.

18. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO THE ARTICLE 194.04 OF THE STANDARD UNLESS A SEPERATE PAY ITEM HAS BEEN PROVIDED.

19. PROPOSED SIDEWALK RAMPS SHALL CONFORM TO CURRENT ADA REQUIREMENTS AND APPLICABLE STATE HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER.

20. THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION.

21. OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS.

22. CONTACT THE IDOT ROADSIDE DEVELOPMENT UNIT AT 847-705-4171 AT LEAST 2 WEEKS PRIOR TO THE BEGINNING LANDSCAPE AND FORESTRY WORK FOR LAYOUT

23. ALL MILLED SURFACES SHALL BE A UNIFORM CROSS SLOPE PER LANE AND FREE OF RIDGES BETWEEN PASSES. ANY DEVIATIONS SHALL BE CORRECTED AT NO COST TO THE DEPARTMENT.

24. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

25. ALL CRACK ROUTING & CRACK SEALING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER. CRACK ROUTING SHALL BE OMITTED WHERE VEHICLE DETECTOR LOOPS & RAISED REFLECTIVE PAVEMENT MARKERS ARE PRESENT IN THE PAVEMENT.

26. MATERIALS RESULTING FROM THE ROUTING OF CRACKS IN THE EXISTING PAVEMENT MAY BE PLACED, SHAPED AND COMPACTED TO THE SATISFACTION OF THE ENGINEER ALONG EXISTING AGGREGATE SHOULDERS ADJACENT TO THE PAVEMENT. ALL MATERIALS RESULTING FROM THE ROUTING OF CRACKS IN PAVEMENTS WITHOUT AGGREGATE SHOULDERS AND SURPLUS MATERIALS RESULTING FROM THE ROUTING OF CRACKS IN PAVEMENTS WITH AGGREGATE SHOULDERS, WHERE ALL MATERIALS ARE NOT PLACED ALONG EXISTING AGGREGATE SHOULDERS, SHALL BE DISPOSED OF AS SPECIFIED IN ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS.

27. MODIFIED URETHANE PAVEMENT MARKING SHALL BE PLACED IMMEDIATELY AFTER C & J SEALER TEMPERATURE HAS COOLED DOWN AND DOES NOT AFFECT THE APPLICATION OF THE MODIFIED URETHANE PAVEMENT MARKING.

28. DURING CONSTRUCTION OPERATIONS, IF ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, THE MATERIAL SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATION, ALL UTILITY STRUCTURES SHALL BE FREE FROM DUST AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

29. TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE III TAPE SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES
PULASKI ROAD (COLUMBUS AVENUE TO US 12 (95TH STREET))

SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	3
		CONTRACT NO. 62T87		
	ILLINOIS	FED. AID PROJECT		

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		80% FED 20% STATE ROADWAY 0005 URBAN	100% STATE ROADWAY 005 URBAN	80% FED 20% STATE TRAFFIC SIGNALS 0021			
20200100	EARTH EXCAVATION	CU YD	45	45					
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	107	107					
25200110	SODDING, SALT TOLERANT	SQ YD	107	107					
25200200	SUPPLEMENTAL WATERING	UNIT	1.1	1.1					
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	16202	16202					
40600370	LONGITUDINAL JOINT SEALANT	FOOT	21407	21407					
40600400	MIXTURE FOR CRACKS, JOINTS, AND	TON	54	54					
	FLANGEWAYS								
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT	SQ YD	252	252					
	JOINT								
40601005	HOT-MIX ASPHALT REPLACEMENT OVER	TON	370	370					
	PATCHES								
40605026	POLYMERIZED HOT-MIX ASPHALT SURFACE	TON	3536	3536					
	COURSE, STONE MATRIX ASPHALT, 9.5, MIX								
	"F", N80								
42001300	PROTECTIVE COAT	SQ YD	666	666					
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY	SQ YD	7	7					
	PAVEMENT, 6 INCH								

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		80% FED 20% STATE ROADWAY 0005 URBAN	100% STATE ROADWAY 005 URBAN	80% FED 20% STATE TRAFFIC SIGNALS 0021			
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5	SQ FT	4360	4360					
	INCH								
42400800	DETECTABLE WARNINGS	SQ FT	240	240					
44000156	HOT-MIX ASPHALT SURFACE REMOVAL, 1	SQ YD	36004	36004					
	3/4"								
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	7	7					
44000600	SIDEWALK REMOVAL	SQ FT	4360	4360					
44002216	HOT-MIX ASPHALT REMOVAL OVER PATCHES,	SQ YD	1650	1650					
	4"								
44003100	MEDIAN REMOVAL	SQ FT	737	737					
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SQ YD	100	100					
44201757	CLASS D PATCHES, TYPE III, 9 INCH	SQ YD	252	252					
44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SQ YD	1350	1350					
44201789	CLASS D PATCHES, TYPE II, 12 INCH	SQ YD	14	14					
44201796	CLASS D PATCHES, TYPE IV, 12 INCH	SQ YD	69	69					
45100100	CRACK ROUTING (PAVEMENT)	FOOT	3489	3489					
45100200	CRACK FILLING	POUND	997	997					

✱ = SPECIALTY ITEMS

△ = NON-PARTICIPATING WORK (100% STATE)

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
PULASKI ROAD (COLUMBUS AVE TO US 12 (95TH STREET))

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	4
		CONTRACT NO. 62T87		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

REV-SEP

	SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
	CODE NO	ITEM	UNIT		80% FED 20% STATE ROADWAY 0005 URBAN	100% STATE ROADWAY 005 URBAN	80% FED 20% STATE TRAFFIC SIGNALS 0021			
	60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	1	1					
	60255700	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1	EACH	1	1					
		FRAME, OPEN LID								
	60257900	MANHOLES TO BE RECONSTRUCTED	EACH	1	1					
	60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	2	2					
	60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	3	3					
	60404950	FRAMES AND GRATES, TYPE 24	EACH	1	1					
	60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	1	1					
	60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	1	1					
*	66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	45	45					
*	66900530	SOIL DISPOSAL ANALYSIS	EACH	3	3					
*	66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION	L SUM	1	1					
		PLAN								
*	66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION	L SUM	1	1					
		REPORT								
*	66901006	REGULATED SUBSTANCES MONITORING	CAL DA	8	8					
	67100100	MOBILIZATION	L SUM	1	1					

	SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
	CODE NO	ITEM	UNIT		80% FED 20% STATE ROADWAY 0005 URBAN	100% STATE ROADWAY 005 URBAN	80% FED 20% STATE TRAFFIC SIGNALS 0021			
	70102625	TRAFFIC CONTROL AND PROTECTION,	L SUM	1	1					
		STANDARD 701606								
	70102630	TRAFFIC CONTROL AND PROTECTION,	L SUM	1	1					
		STANDARD 701601								
	70102632	TRAFFIC CONTROL AND PROTECTION,	L SUM	1	1					
		STANDARD 701602								
	70102635	TRAFFIC CONTROL AND PROTECTION,	L SUM	1	1					
		STANDARD 701701								
	70102640	TRAFFIC CONTROL AND PROTECTION,	L SUM	1	1					
		STANDARD 701801								
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	7615	7615					
	70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	4652	4652					
	70300211	TEMPORARY PAVEMENT MARKING LETTERS AND	SQ FT	929	929					
		SYMBOLS - PAINT								
	70300221	TEMPORARY PAVEMENT MARKING - LINE 4"-	FOOT	15214	15214					
		PAINT								
	70300241	TEMPORARY PAVEMENT MARKING - LINE 6"-	FOOT	2399	2399					
		PAINT								
	70300261	TEMPORARY PAVEMENT MARKING - LINE 12"-	FOOT	364	364					
		PAINT								

* = SPECIALTY ITEMS

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	5
		CONTRACT NO. 62T87		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

	SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
	CODE NO	ITEM	UNIT		80% FED 20% STATE ROADWAY 0005 URBAN	100% STATE ROADWAY 005 URBAN	80% FED 20% STATE TRAFFIC SIGNALS 0021			
	70300281	TEMPORARY PAVEMENT MARKING - LINE 24"-	FOOT	494	494					
		PAINT								
	70306120	TEMPORARY PAVEMENT MARKING - LINE 4" -	FOOT	3803	3803					
		TYPE III TAPE								
*	78000100	THERMOPLASTIC PAVEMENT MARKING -	SQ FT	929	929					
		LETTERS AND SYMBOLS								
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE	FOOT	15214	15214					
		4"								
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE	FOOT	2399	2399					
		6"								
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE	FOOT	364	364					
		12"								
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE	FOOT	494	494					
		24"								
*	78009000	MODIFIED URETHANE PAVEMENT MARKING -	SQ FT	329	329					
		LETTERS AND SYMBOLS								
*	78009004	MODIFIED URETHANE PAVEMENT MARKING -	FOOT	772	772					
		LINE 4"								
*	78009006	MODIFIED URETHANE PAVEMENT MARKING -	FOOT	592	592					
		LINE 6"								

	SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
	CODE NO	ITEM	UNIT		80% FED 20% STATE ROADWAY 0005 URBAN	100% STATE ROADWAY 005 URBAN	80% FED 20% STATE TRAFFIC SIGNALS 0021			
*	78009012	MODIFIED URETHANE PAVEMENT MARKING -	FOOT	55	55					
		LINE 12"								
*	78009024	MODIFIED URETHANE PAVEMENT MARKING -	FOOT	312	312					
		LINE 24"								
*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	520	520					
	78300200	RAISED REFLECTIVE PAVEMENT MARKER	EACH	520	520					
		REMOVAL								
	78300202	PAVEMENT MARKING REMOVAL - WATER	SQ FT	8552	8552					
		BLASTING								
*	81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL,	FOOT	20			20			
		2" DIA.								
*	85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL	EACH	3	1		2			
		INSTALLATION								
*	87301125	ELECTRIC CABLE IN CONDUIT, SIGNAL NO.	FOOT	766			766			
		12 3C								
*	87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO.	FOOT	30			30			
		14 2C								
*	87900200	DRILL EXISTING HANDHOLE	EACH	1			1			
*	88600100	DETECTOR LOOP, TYPE I	FOOT	709			709			

* = SPECIALTY ITEMS

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	6
		CONTRACT NO. 62T87		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

	SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
	CODE NO	ITEM	UNIT		80% FED 20% STATE ROADWAY 0005 URBAN	100% STATE ROADWAY 005 URBAN	80% FED 20% STATE TRAFFIC SIGNALS 0021			
*	89502210	MODIFY EXISTING CONTROLLER CABINET	EACH	2			2			
*	89502375	REMOVE EXISTING TRAFFIC SIGNAL	EACH	2			2			
		EQUIPMENT								
*	89502376	REBUILD EXISTING HANDHOLE	EACH	4	4					
	X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1					
*	X1400378	PEDESTRIAN SIGNAL POST, 5 FT.	EACH	1			1			
	X4400501	COMBINATION CURB AND GUTTER REMOVAL AND	FOOT	248	248					
		REPLACEMENT LESS THAN OR EQUAL TO 10								
		FEET								
	X4400503	COMBINATION CURB AND GUTTER REMOVAL AND	FOOT	511	511					
		REPLACEMENT GREATER THAN 10 FEET								
△	X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	90		90				
	X6030310	FRAMES AND LIDS TO BE ADJUSTED	EACH	39	39					
		(SPECIAL)								
	X6700407	ENGINEER'S FIELD OFFICE, TYPE A (D1)	CAL MO	12	12					
*	X8140238	REBUILD EXISTING DOUBLE HANDHOLE	EACH	2	2					
*	X8760200	ACCESSIBLE PEDESTRIAN SIGNALS	EACH	16			16			

	SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
	CODE NO	ITEM	UNIT		80% FED 20% STATE ROADWAY 0005 URBAN	100% STATE ROADWAY 005 URBAN	80% FED 20% STATE TRAFFIC SIGNALS 0021			
*	X8780012	CONCRETE FOUNDATION, TYPE A 12-INCH	FOOT	4			4			
		DIAMETER								
△	Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	6		6				
	Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	154.2	154.2					
*	Z0033044	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL	EACH	2			2			
		1								
Ø	Z0076600	TRAINEES	HOURS	500	500					
Ø	Z0076604	TRAINEES - TRAINING PROGRAM GRADUATE	HOURS	500	500					

Ø 0042

* = SPECIALTY ITEMS

△ = NON-PARTICIPATING WORK (100% STATE)

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		DRAWN -	REVISED -
	PLOT SCALE = 100.0000 ' / In.	CHECKED -	REVISED -
	PLOT DATE = 10/24/2023	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	7
		CONTRACT NO. 62T87		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

REV-SEP

LEGEND:

1. EXIST. HOT-MIX ASPHALT, 4'' AND VARIES
2. EXIST. P.C.C. PAVEMENT, 9'' AND VARIES
3. EXIST. COMB. CONC. CURB AND GUTTER
4. PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 1.75''
5. PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX ''F'', N80, 1.75''

✳ CURB AND GUTTER REMOVAL AND REPLACEMENT (LOCATION AS DIRECTED BY THE ENGINEER)

NOTES:

(1) THE CONTRACTOR SHALL PATCH FIRST BEFORE MILLING

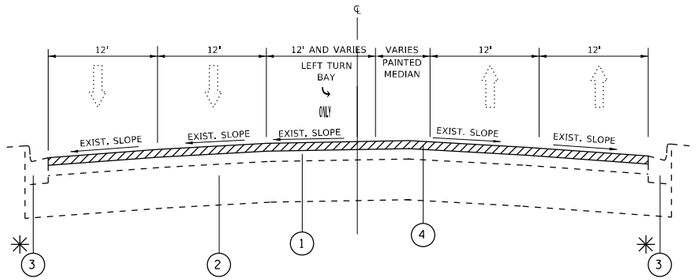
(2) THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE MILLED SURFACE.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE TYPE	AIR VOIDS (%) @ NDES	
RESURFACING MAINLINE:		
POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX ''F'', N80, 1.75''	3.5% @ 80 GYR.	QCP
HOT-MIX ASPHALT PATCHING:		
CLASS D PATCH (HMA BINDER IL-19 mm), 9'', 12''	4% @ 70 GYR.	QC/QA
HOT-MIX ASPHALT REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm), 4''	4% @ 70 GYR.	QC/QA

QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP)

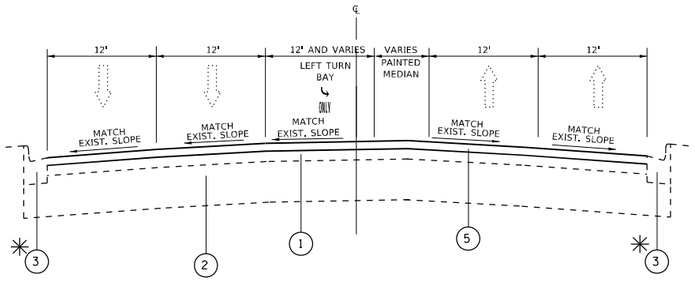
NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN

NOTE 2: THE ''AC TYPE'' FOR POLYMERIZED HMA MIXES SHALL BE ''SBS/SBR PG 76-22'' AND FOR NON-POLYMERIZED HMA THE ''AC TYPE'' SHALL BE ''PG 64-22'' UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS



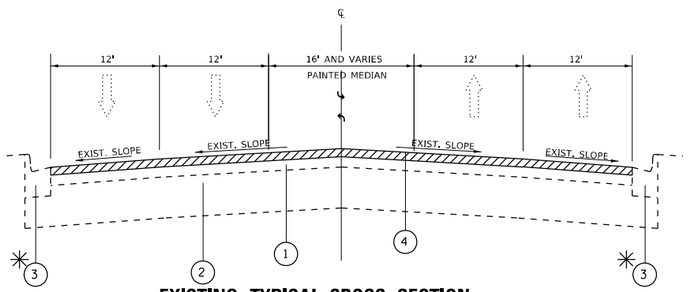
EXISTING TYPICAL CROSS SECTION

STA. 14+45 TO STA. 17+26
STA. 27+80 TO STA. 30+78



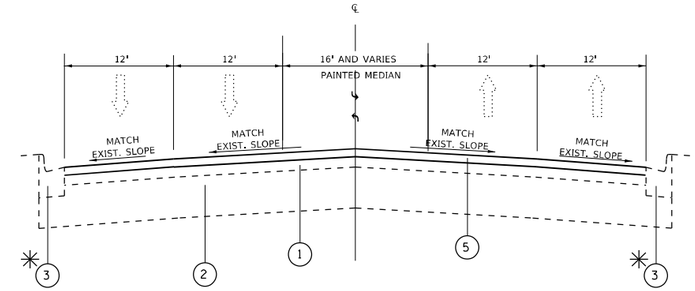
PROPOSED TYPICAL CROSS SECTION

STA. 14+45 TO STA. 17+26
STA. 27+80 TO STA. 30+78



EXISTING TYPICAL CROSS SECTION

STA. 17+26 TO STA. 23+99
STA. 30+78 TO STA. 39+18



PROPOSED TYPICAL CROSS SECTION

STA. 17+26 TO STA. 23+99
STA. 30+78 TO STA. 39+18

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	DRAWN -	REVISED -	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	
PLOT DATE = 10/23/2023	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
PULASKI ROAD (COLUMBUS AVENUE TO US 12 (95TH STREET))

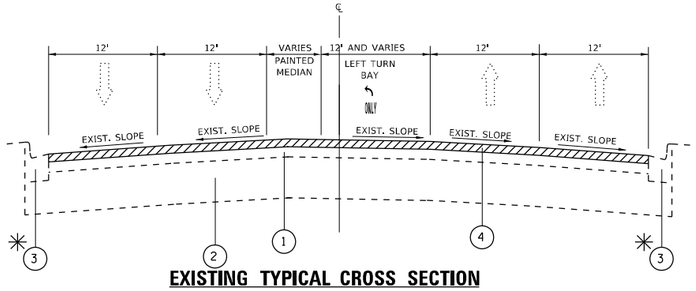
SCALE: | SHEET OF SHEETS | STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	8
		CONTRACT NO. 62T87		
	ILLINOIS	FED. AID PROJECT		

LEGEND:

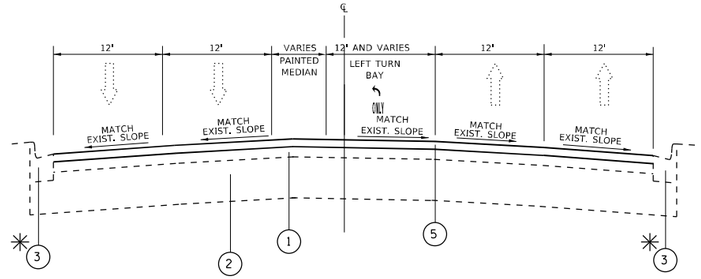
1. EXIST. HOT-MIX ASPHALT, 4" AND VARIES
2. EXIST. P.C.C. PAVEMENT, 9" AND VARIES
3. EXIST. COMB. CONC. CURB AND GUTTER
4. PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 1.75"
5. PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5. MIX "F", N80, 1.75"

✳ CURB AND GUTTER REMOVAL AND REPLACEMENT (LOCATION AS DIRECTED BY THE ENGINEER)



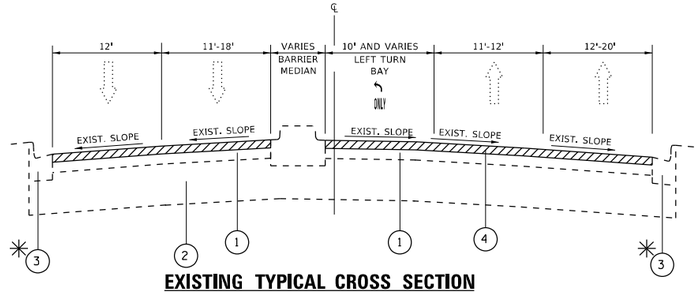
EXISTING TYPICAL CROSS SECTION

STA. 23+99 TO STA. 27+80
STA. 39+18 TO STA. 43+36



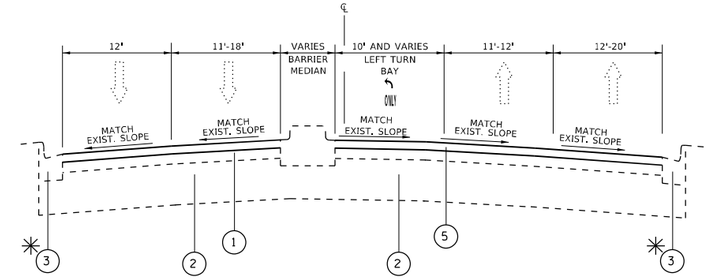
PROPOSED TYPICAL CROSS SECTION

STA. 23+99 TO STA. 27+80
STA. 39+18 TO STA. 43+36



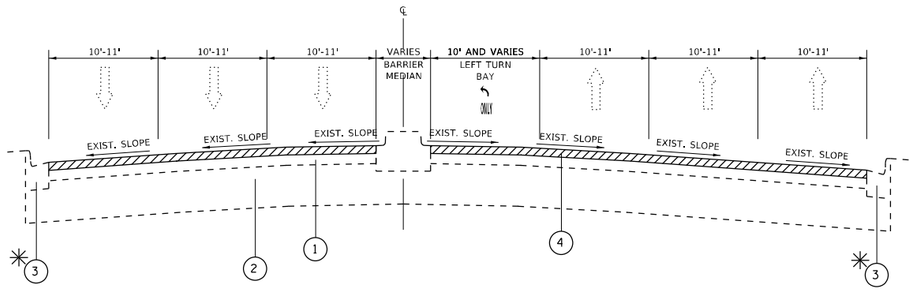
EXISTING TYPICAL CROSS SECTION

STA. 43+36 TO STA. 58+51



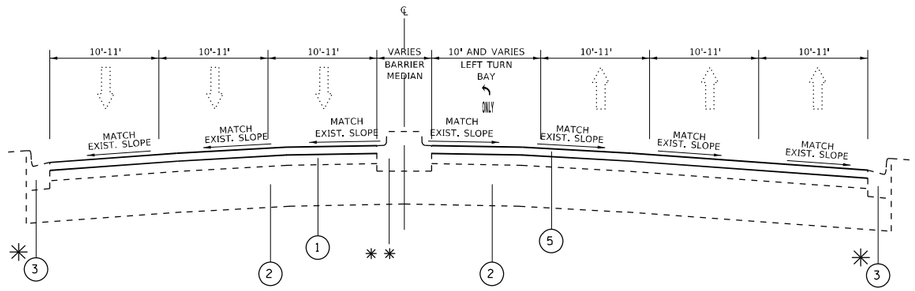
PROPOSED TYPICAL CROSS SECTION

STA. 43+36 TO STA. 58+51



EXISTING TYPICAL CROSS SECTION

STA. 58+51 TO STA. 65+40



PROPOSED TYPICAL CROSS SECTION

STA. 58+51 TO STA. 65+40

✳ ✳ PROP. LEFT TURN LANE STORAGE EXTENSION: STA. 63+53 TO STA. 64+69

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	DRAWN -	REVISED -	
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PLOT DATE = 10/24/2023	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

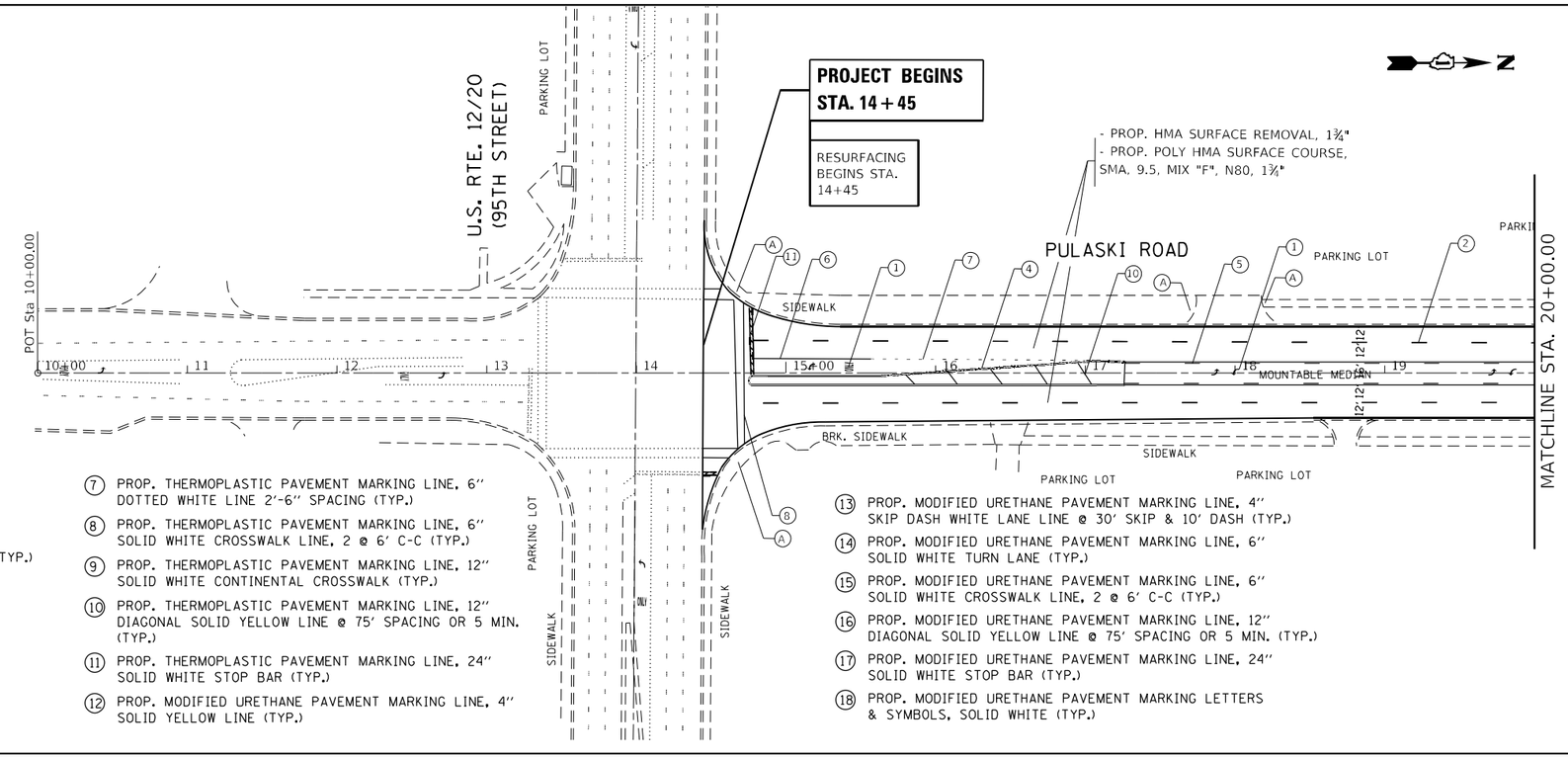
TYPICAL SECTIONS
PULASKI ROAD (COLUMBUS AVENUE TO US 12 (95TH STREET))

SCALE: | SHEET OF SHEETS | STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	9
		CONTRACT NO. 62T87		
	ILLINOIS FED. AID PROJECT			

SCHEDULE OF QUANTITIES - ADA IMPROVEMENT

STATE ROUTE	CROSS STREET	CORNER	20200100	21101615	25200110	25200200	42001300	42400200	42400800	44000600	60255700	85000200	89500400	89502376	X4400501	X4400503	X8140238
			EARTH EXACAVATION	TOPSOIL FURNISH AND PLACE, 4"	SODDING, SALT TOLERANT	SUPPLEMENTAL WATERING	PROTECTIVE COAT	PORTLAND CEMENT CONCRETE SIDEWALK, 5INCH	DETECTABLE WARNINGS	SIDEWALK REMOVAL	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	RELOCATE EXISTING PEDESTRIAN PUSH-BUTTON	REBUILD EXISTING HANDHOLE	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT LESS THAN OR EQUAL TO 10 FEET	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT GREATER THAN 10 FEET	REBUILD EXISTING DOUBLE HANDHOLE
			CU YD	SQ YD	SQ YD	UNIT	SQ YD	SQ FT	SQ FT	SQ FT	EACH	EACH	EACH	EACH	FOOT	FOOT	EACH
Pulaski Road	95th Street	NE	4.3	0.0	0.0	0.00	60.2	425.0	30.0	425.0		1.0	1.0		19.5	19.5	1.0
		NW	3.0	0.0	0.0	0.00	46.3	300.0	30.0	300.0			1.0	1.0	19.5	19.5	
	Jewel Osco (South Entrance)	NW	2.00	2.7	2.7	0.03	26.2	200.0	10.0	200.0					6.0	6.0	
		SW	2.25	3.3	3.3	0.03	28.7	225.0	10.0	225.0					5.5	5.5	
	94th Street	NE	3.00	4.4	4.4	0.04	44.0	300.0	10.0	300.0					16.0	16.0	
		SE	3.00	4.2	4.2	0.04	38.9	300.0	10.0	300.0					8.3	8.3	
		SW	2.00	1.3	1.3	0.01	26.7	200.0	10.0	200.0					6.8	6.8	
	93rd Street	NE	2.45	3.1	3.1	0.03	37.0	245.0	20.0	245.0	1.0	1.0	1.0	1.0	14.7	14.7	
		NW	3.25	1.3	1.3	0.01	47.1	325.0	20.0	325.0			1.0	1.0	16.5	16.5	
		SE	2.75	2.5	2.5	0.03	40.5	275.0	20.0	275.0			1.0	1.0	14.9	14.9	
		SW	2.45	3.8	3.8	0.04	38.3	245.0	20.0	245.0			1.0		16.6	16.6	
	92nd Street	NE	2.50	3.6	3.6	0.04	37.7	250.0	10.0	250.0					15.0	15.0	
		SE	3.75	1.3	1.3	0.01	45.8	375.0	10.0	375.0					6.3	6.3	
	91st Place	NW	2.25	1.9	1.9	0.02	33.9	225.0	10.0	225.0					13.4	13.4	
		SW	2.25	4.0	4.0	0.04	44.8	225.0	10.0	225.0					29.7	29.7	
	91st Street	SE	2.45	1.0	1.0	0.01	38.5	245.0	10.0	245.0		1.0			17.0	17.0	1.0
TOTAL			45	39	39	0.40	635	4360	240	4360	1	3	6	4	225	225	2

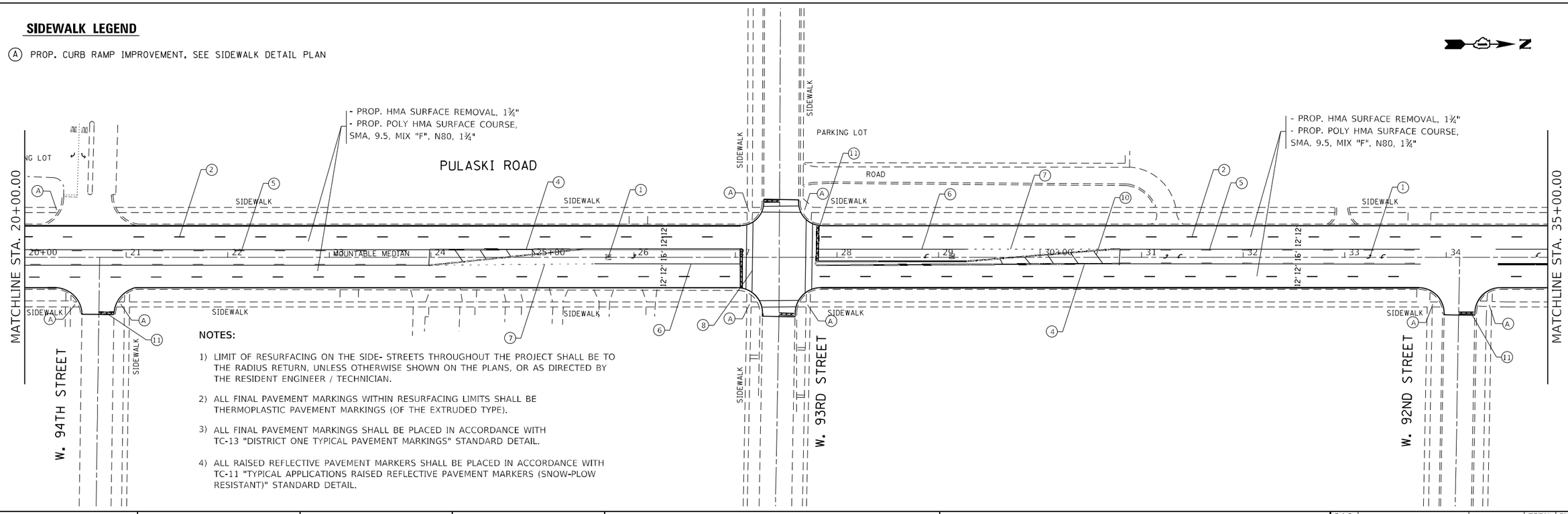


PAVEMENT MARKING LEGEND

1. PROP. THERMOPLASTIC PAVEMENT MARKING LETTERS & SYMBOLS, SOLID WHITE (TYP.)
2. PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" SKIP DASH WHITE LANE LINE @ 30' SKIP & 10' DASH (TYP.)
3. PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" SOLID YELLOW LINE (TYP.)
4. PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" DOUBLE SOLID YELLOW LINE, 2 @ 11" C-C (TYP.)
5. PROP. THERMOPLASTIC PAVEMENT MARKING LINES, 4" SOLID YELLOW LINE @ 5½ C-C FROM 4" SKIP-DASH YELLOW LINE @ 30' SKIP & 10' DASH (TYP.)
6. PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" SOLID WHITE LINE (TYP.)
7. PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" DOTTED WHITE LINE 2'-6" SPACING (TYP.)
8. PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" SOLID WHITE CROSSWALK LINE, 2 @ 6' C-C (TYP.)
9. PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" SOLID WHITE CONTINENTAL CROSSWALK (TYP.)
10. PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" DIAGONAL SOLID YELLOW LINE @ 75' SPACING OR 5 MIN. (TYP.)
11. PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 24" SOLID WHITE STOP BAR (TYP.)
12. PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 4" SOLID YELLOW LINE (TYP.)
13. PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 4" SKIP DASH WHITE LANE LINE @ 30' SKIP & 10' DASH (TYP.)
14. PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 6" SOLID WHITE TURN LANE (TYP.)
15. PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 6" SOLID WHITE CROSSWALK LINE, 2 @ 6' C-C (TYP.)
16. PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 12" DIAGONAL SOLID YELLOW LINE @ 75' SPACING OR 5 MIN. (TYP.)
17. PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 24" SOLID WHITE STOP BAR (TYP.)
18. PROP. MODIFIED URETHANE PAVEMENT MARKING LETTERS & SYMBOLS, SOLID WHITE (TYP.)

SIDEWALK LEGEND

(A) PROP. CURB RAMP IMPROVEMENT, SEE SIDEWALK DETAIL PLAN



NOTES:

1) LIMIT OF RESURFACING ON THE SIDE- STREETS THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE SHOWN ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER / TECHNICIAN.
2) ALL FINAL PAVEMENT MARKINGS WITHIN RESURFACING LIMITS SHALL BE THERMOPLASTIC PAVEMENT MARKINGS (OF THE EXTRUDED TYPE).
3) ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH TC-13 "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" STANDARD DETAIL.
4) ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH TC-11 "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" STANDARD DETAIL.

USER NAME = AYA.ELkhatib	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 10/23/2023	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY AND PAVEMENT MARKING PLAN
PULASKI ROAD (COLUMBUS AVE TO US 12 (95TH STREET))

SCALE: 1"=50' | SHEET OF SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	11
		CONTRACT NO. 62T87		
	ILLINOIS	FED. AID PROJECT		

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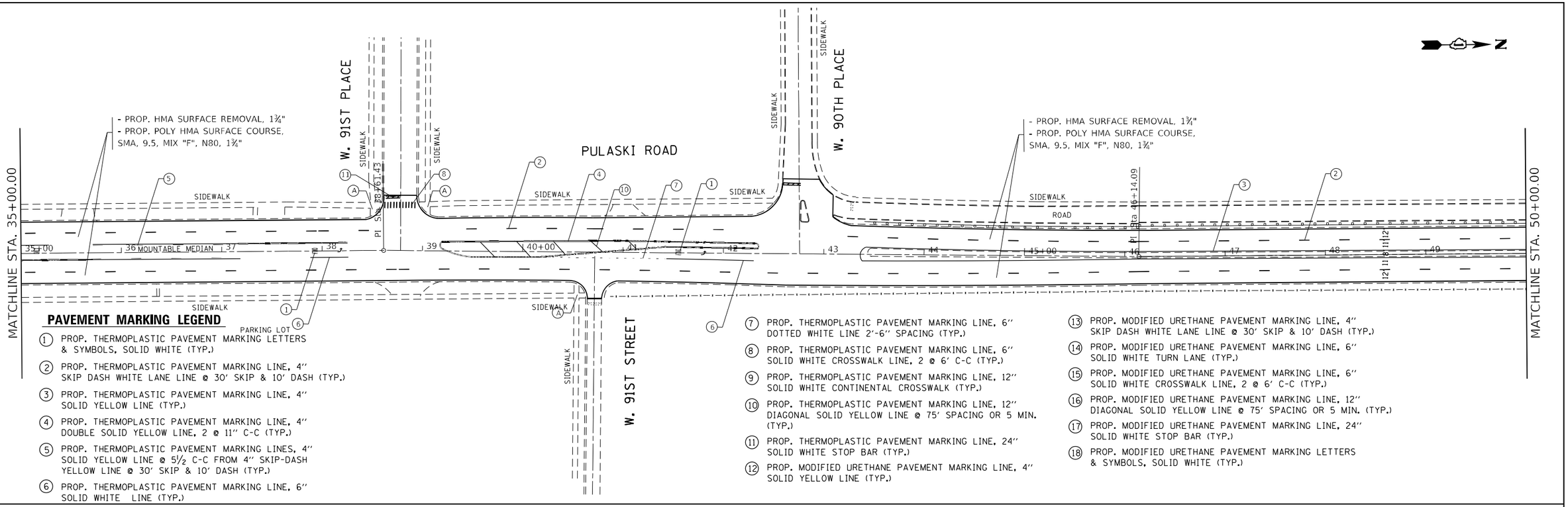

PAVEMENT MARKING LEGEND

1. PROP. THERMOPLASTIC PAVEMENT MARKING LETTERS & SYMBOLS, SOLID WHITE (TYP.)
2. PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" SKIP DASH WHITE LANE LINE @ 30' SKIP & 10' DASH (TYP.)
3. PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" SOLID YELLOW LINE (TYP.)
4. PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" DOUBLE SOLID YELLOW LINE, 2 @ 11" C-C (TYP.)
5. PROP. THERMOPLASTIC PAVEMENT MARKING LINES, 4" SOLID YELLOW LINE @ 5½ C-C FROM 4" SKIP-DASH YELLOW LINE @ 30' SKIP & 10' DASH (TYP.)
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7. PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" DOTTED WHITE LINE 2'-6" SPACING (TYP.)
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9. PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" SOLID WHITE CONTINENTAL CROSSWALK (TYP.)
10. PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" DIAGONAL SOLID YELLOW LINE @ 75' SPACING OR 5 MIN. (TYP.)
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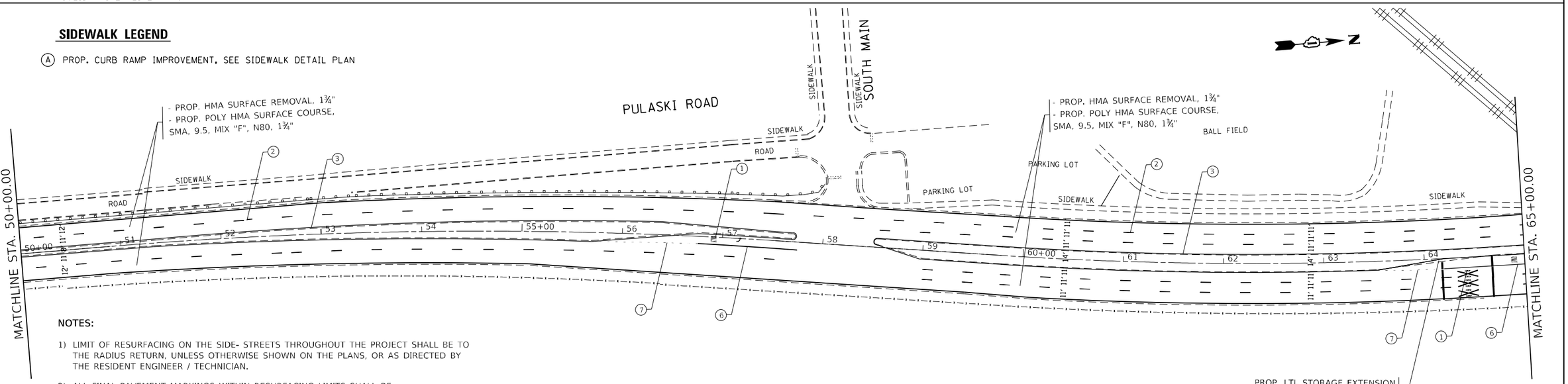
17. PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 24" SOLID WHITE STOP BAR (TYP.)
18. PROP. MODIFIED URETHANE PAVEMENT MARKING LETTERS & SYMBOLS, SOLID WHITE (TYP.)

SIDEWALK LEGEND

(A) PROP. CURB RAMP IMPROVEMENT, SEE SIDEWALK DETAIL PLAN



PROP. LTL STORAGE EXTENSION
- MEDIAN REMOVAL
- CLASS D PATCHES, TYPE II, 12"
- PROP. POLY HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 1¾"

NOTES:

1) LIMIT OF RESURFACING ON THE SIDE- STREETS THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE SHOWN ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER / TECHNICIAN.
2) ALL FINAL PAVEMENT MARKINGS WITHIN RESURFACING LIMITS SHALL BE THERMOPLASTIC PAVEMENT MARKINGS (OF THE EXTRUDED TYPE).
3) ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH TC-13 "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" STANDARD DETAIL.
4) ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH TC-11 "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" STANDARD DETAIL.

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	DRAWN -	REVISED -
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 10/24/2023	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY AND PAVEMENT MARKING PLAN
PULASKI ROAD (COLUMBUS AVE TO US 12 (95TH STREET))

SCALE: 1"=50' | SHEET OF SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	12
		CONTRACT NO. 62T87		
	ILLINOIS	FED. AID PROJECT		

MODEL: Default
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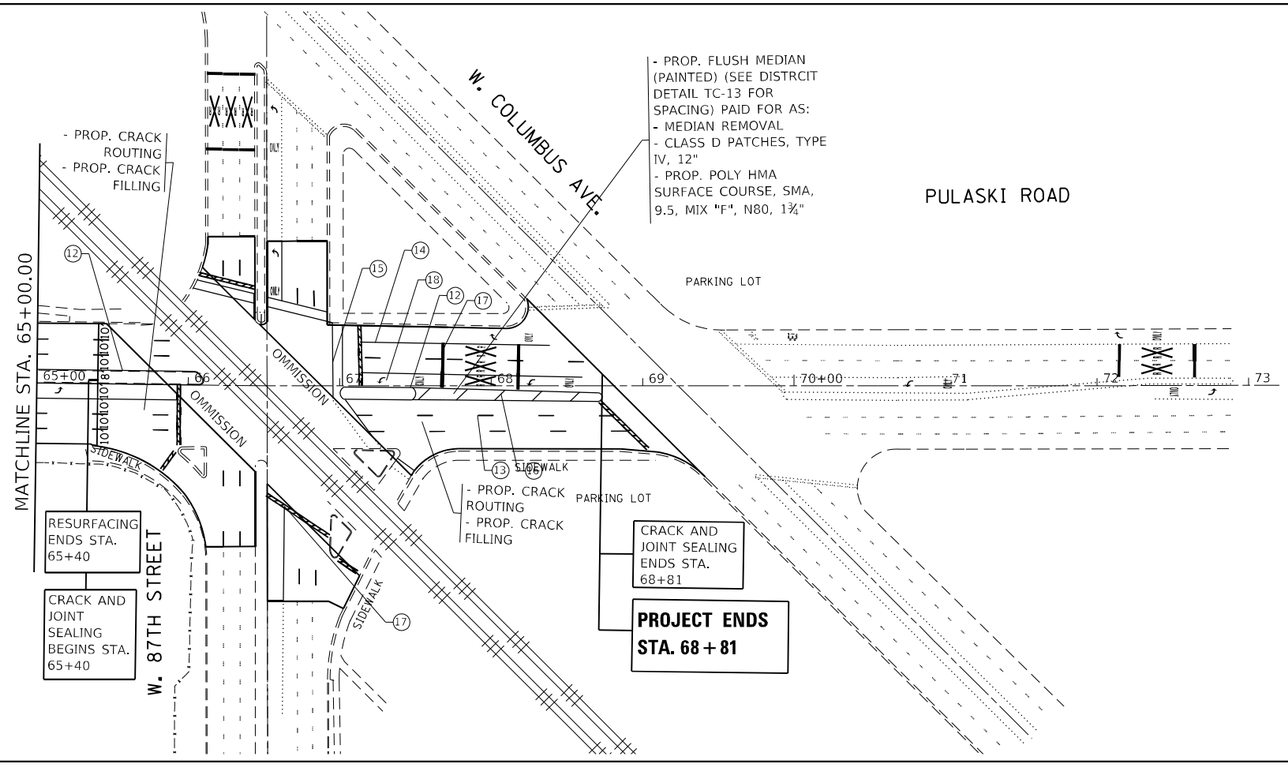


PAVEMENT MARKING LEGEND

1. PROP. THERMOPLASTIC PAVEMENT MARKING LETTERS & SYMBOLS, SOLID WHITE (TYP.)
2. PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" SKIP DASH WHITE LANE LINE @ 30' SKIP & 10' DASH (TYP.)
3. PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" SOLID YELLOW LINE (TYP.)
4. PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" DOUBLE SOLID YELLOW LINE, 2 @ 11" C-C (TYP.)
5. PROP. THERMOPLASTIC PAVEMENT MARKING LINES, 4" SOLID YELLOW LINE @ 5½ C-C FROM 4" SKIP-DASH YELLOW LINE @ 30' SKIP & 10' DASH (TYP.)
6. PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" SOLID WHITE LINE (TYP.)
7. PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" DOTTED WHITE LINE 2'-6" SPACING (TYP.)
8. PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" SOLID WHITE CROSSWALK LINE, 2 @ 6' C-C (TYP.)
9. PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" SOLID WHITE CONTINENTAL CROSSWALK (TYP.)
10. PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" DIAGONAL SOLID YELLOW LINE @ 75' SPACING OR 5 MIN. (TYP.)
11. PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 24" SOLID WHITE STOP BAR (TYP.)
12. PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 4" SOLID YELLOW LINE (TYP.)
13. PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 4" SKIP DASH WHITE LANE LINE @ 30' SKIP & 10' DASH (TYP.)
14. PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 6" SOLID WHITE TURN LANE (TYP.)
15. PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 6" SOLID WHITE CROSSWALK LINE, 2 @ 6' C-C (TYP.)
16. PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 12" DIAGONAL SOLID YELLOW LINE @ 75' SPACING OR 5 MIN. (TYP.)
17. PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 24" SOLID WHITE STOP BAR (TYP.)
18. PROP. MODIFIED URETHANE PAVEMENT MARKING LETTERS & SYMBOLS, SOLID WHITE (TYP.)

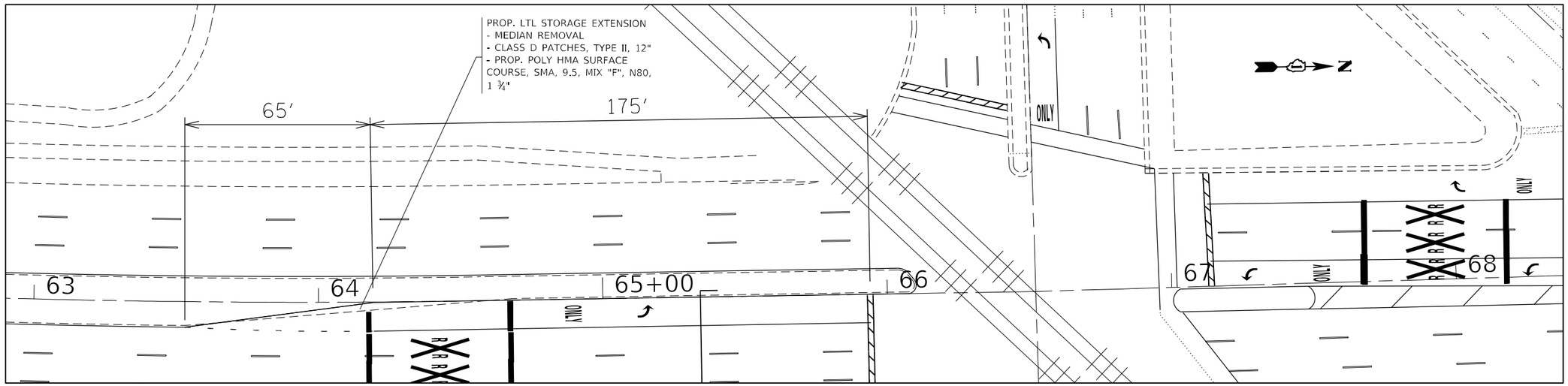
SIDEWALK LEGEND

A. PROP. CURB RAMP IMPROVEMENT, SEE SIDEWALK DETAIL PLAN

NOTES:

1) LIMIT OF RESURFACING ON THE SIDE- STREETS THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE SHOWN ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER / TECHNICIAN.
2) ALL FINAL PAVEMENT MARKINGS WITHIN RESURFACING LIMITS SHALL BE THERMOPLASTIC PAVEMENT MARKINGS (OF THE EXTRUDED TYPE).
3) ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH TC-13 "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" STANDARD DETAIL.
4) ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH TC-11 "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" STANDARD DETAIL.

PROPOSED LEFT TURN LANE STORAGE EXTENSION – DETAIL



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	DRAWN -	REVISED -	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	
PLOT DATE = 10/24/2023	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY AND PAVEMENT MARKING PLAN
PULASKI ROAD (COLUMBUS AVE TO US 12 (95TH STREET))

SCALE: 1"=50' | SHEET OF SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	13
		CONTRACT NO. 62T87		
	ILLINOIS	FED. AID PROJECT		

MODEL: Default
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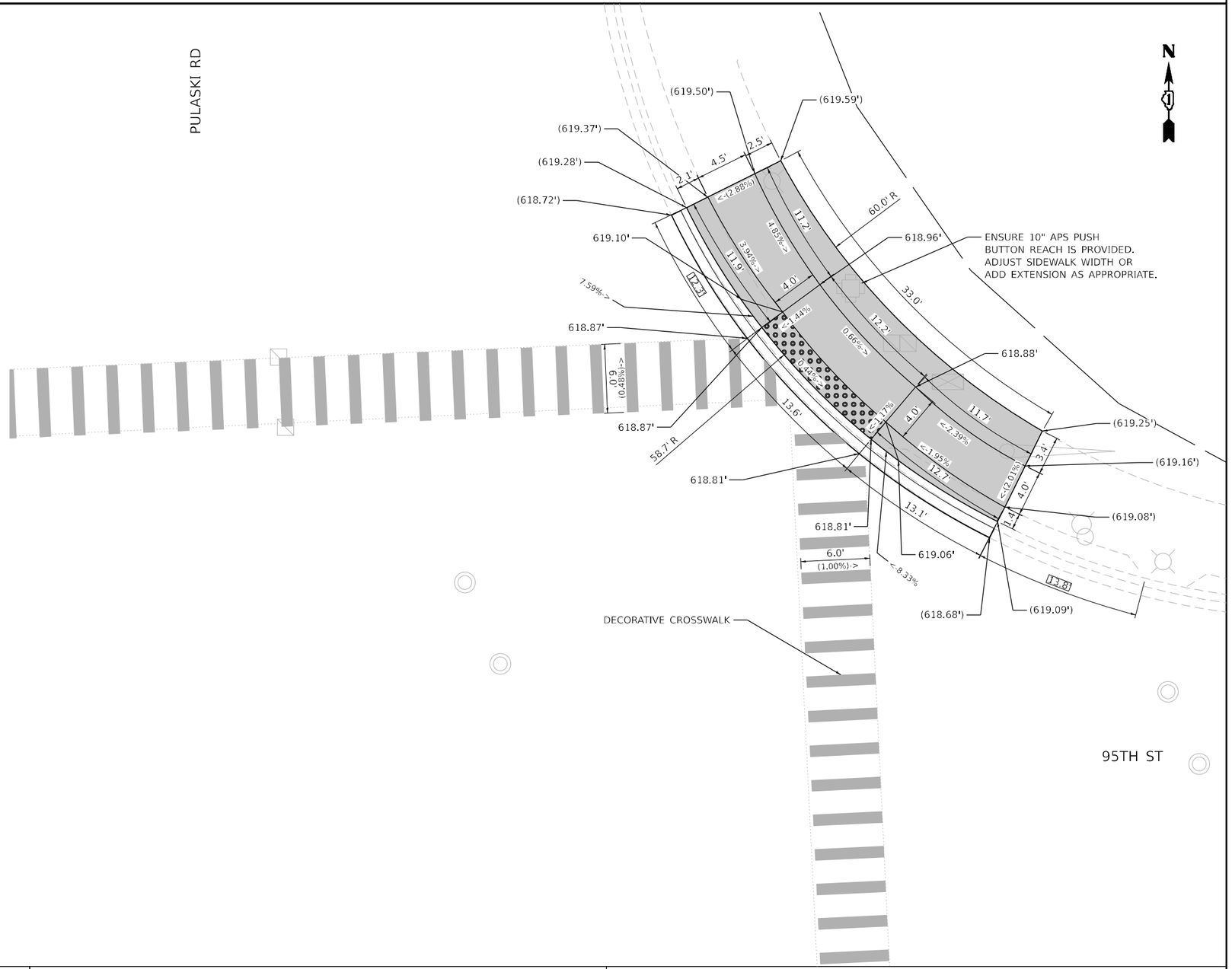
ADA DETAILS FOR PHASE 1 PROJECTS



REFERENCE BENCHMARK : ELEV. 619.000

BENCHMARK : CUT CROSS ON WALK

LOCATION : NW CORNER OF 95TH ST AND PULASKI ROAD

LEGEND

XX.X'	EXISTING LENGTH
═══	PROPOSED SIDE CURB
( )	EXISTING ELEVATION/SLOPE
	PROPOSED SIDEWALK
	DETECTABLE WARNINGS
	SIDEWALK REMOVAL REPLACE W/ TOPSOIL & SOD

INTERSECTION 60857

MODEL: $MODELNAME$
FILE NAME: $FILEL$

CERA

USER NAME = $USER$	DESIGNED - DE	REVISED -
	DRAWN - DE	REVISED -
PLOT SCALE = $SCALE$	CHECKED -	REVISED -
PLOT DATE = $DATE$	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAIL PLAN
US 20/12 (95TH ST) AND PULASKI RD – OAK LAWN

SCALE: 1" = 5' | SHEET OF SHEETS | STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	14
		CONTRACT NO. 62T87		
		ILLINOIS	FED. AID PROJECT	

ADA DETAILS FOR PHASE 1 PROJECTS

REFERENCE BENCHMARK : ELEV. 619.000

BENCHMARK : CUT CROSS ON WALK

LOCATION : NW CORNER OF 95TH ST AND PULASKI ROAD

LEGEND

XX.X'	EXISTING LENGTH
———	PROPOSED SIDE CURB
( )	EXISTING ELEVATION/SLOPE
	PROPOSED SIDEWALK
	DETECTABLE WARNINGS
	SIDEWALK REMOVAL REPLACE W/ TOPSOIL & SOD

INTERSECTION 60857

MODEL: $MODELNAME$
FILE NAME: $FILEL$

CERA

USER NAME = $USER$	DESIGNED - DE	REVISED -
	DRAWN - DE	REVISED -
PLOT SCALE = $SCALE$	CHECKED -	REVISED -
PLOT DATE = $DATE$	DATE -	REVISED -

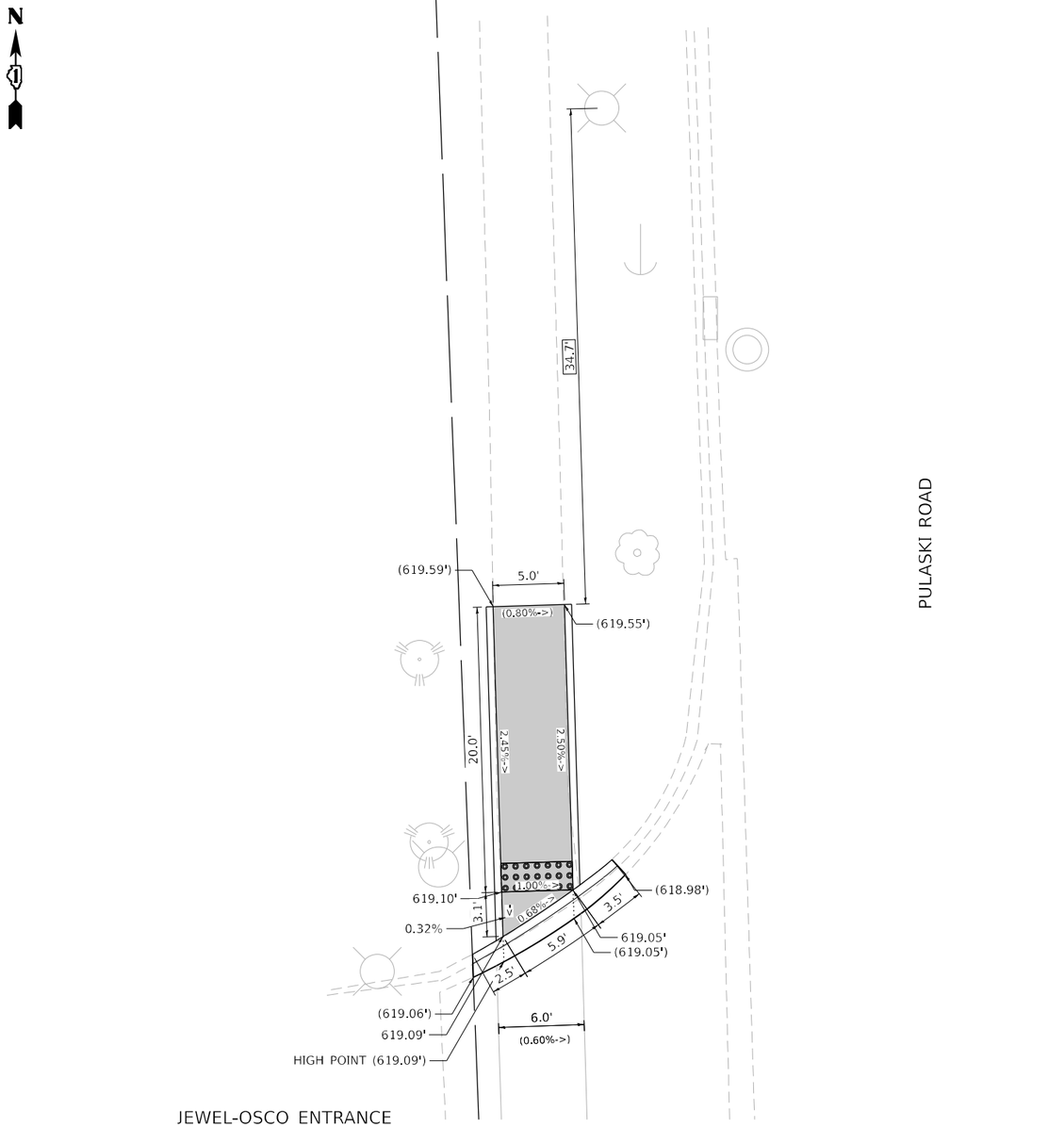
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

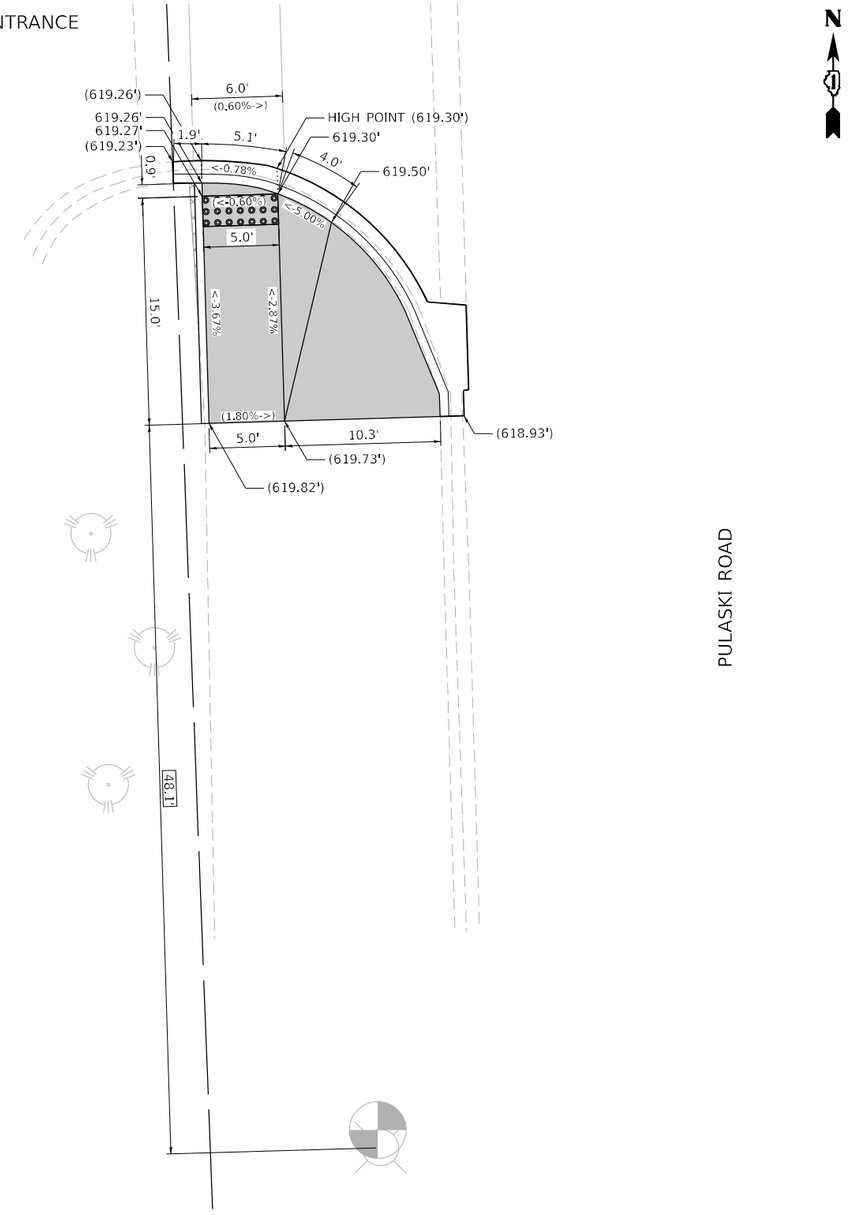
ADA RAMP DETAIL PLAN
US 20\12 (95TH ST) AND PULASKI RD – OAK LAWN

SCALE: 1" = 5' | SHEET OF SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	15
		CONTRACT NO. 62T87		
		ILLINOIS	FED. AID PROJECT	

ADA DETAILS FOR PHASE 1 PROJECTS





REFERENCE BENCHMARK : ELEV. 619.584

BENCHMARK : CONTROL POINT CROSS ON CONCRETE LIGHT POLE FOUNDATION

LOCATION : 63.4' S OF CORNER AT SW CORNER
OF PULASKI ROAD AND JEWEL-OSCO ENTRANCE

LEGEND

XX.X'	EXISTING LENGTH
═══	PROPOSED SIDE CURB
( )	EXISTING ELEVATION/SLOPE
	PROPOSED SIDEWALK
	DETECTABLE WARNINGS
	SIDEWALK REMOVAL REPLACE W/ TOPSOIL & SOD

INTERSECTION 61360

USER NAME = $USER$	DESIGNED - JMM	REVISED -
	DRAWN - SVJ	REVISED -
PLOT SCALE = $SCALE$	CHECKED - SMS	REVISED -
PLOT DATE = $DATE$	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAIL PLAN
PULASKI ROAD AND JEWEL–OSCO ENTRANCE

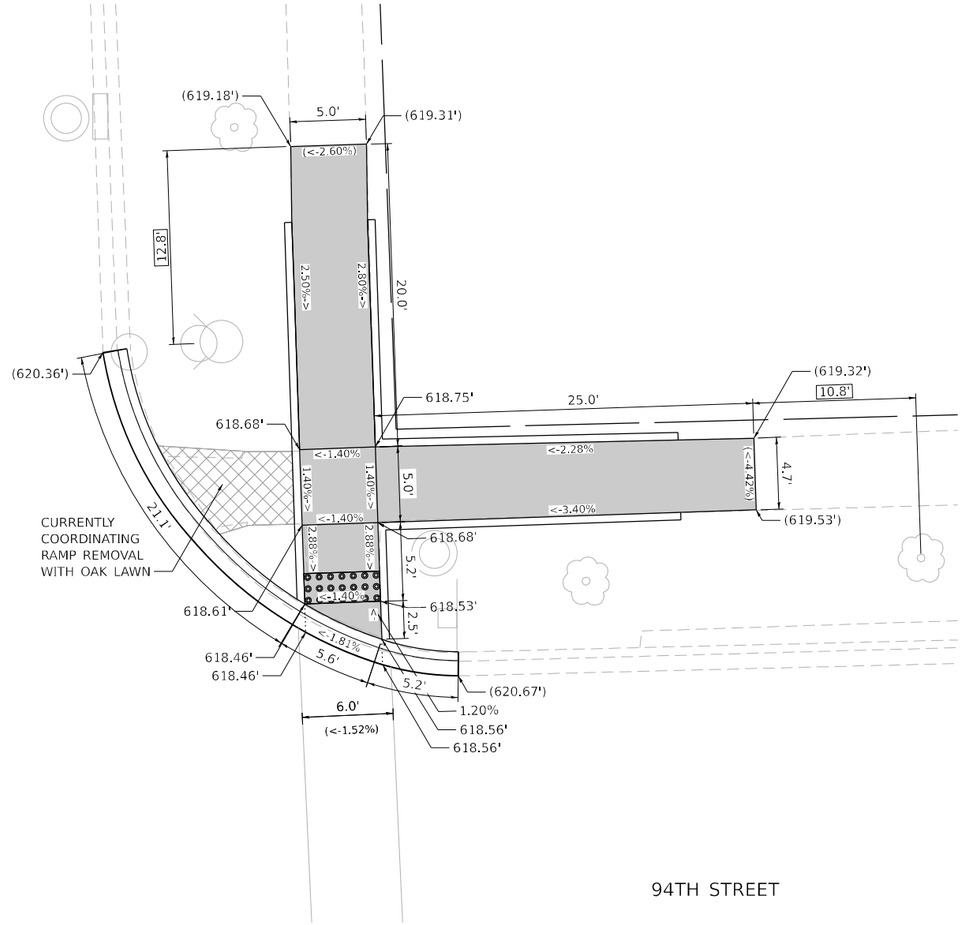
SCALE: 1" = 5' | SHEET OF SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	16
		CONTRACT NO. 62T87		
		ILLINOIS	FED. AID PROJECT	

MODEL: $MODELNAME$
FILE NAME: $FILE$

ADA DETAILS FOR PHASE 1 PROJECTS

PULASKI ROAD



REFERENCE BENCHMARK : ELEV. 619.533

BENCHMARK : CONTROL POINT CROSS ON CONCRETE LIGHT POLE FOUNDATION

LOCATION : 26.7' S OF CORNER AT SW CORNER
OF PULASKI ROAD AND 94TH STREET

LEGEND

XX.X'	EXISTING LENGTH
———	PROPOSED SIDE CURB
( )	EXISTING ELEVATION/SLOPE
	PROPOSED SIDEWALK
	DETECTABLE WARNINGS
	SIDEWALK REMOVAL REPLACE W/ TOPSOIL & SOD

INTERSECTION 61359

MODEL: $MODELNAME$
FILE NAME: $FILE$

SEA

USER NAME = $USER$	DESIGNED - JMM	REVISED -	
	DRAWN - SVJ	REVISED -	
PLOT SCALE = $SCALE$	CHECKED - SMS	REVISED -	
PLOT DATE = $DATE$	DATE -	REVISED -	

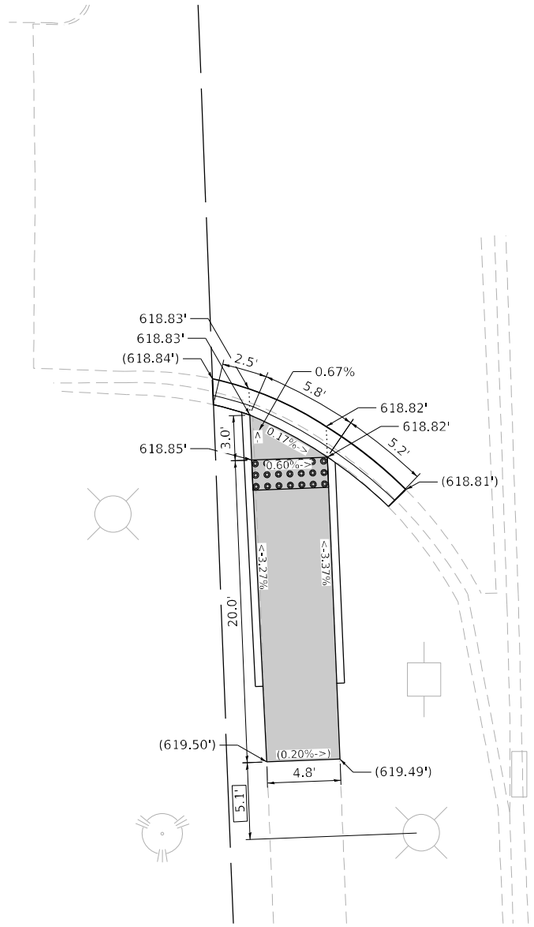
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

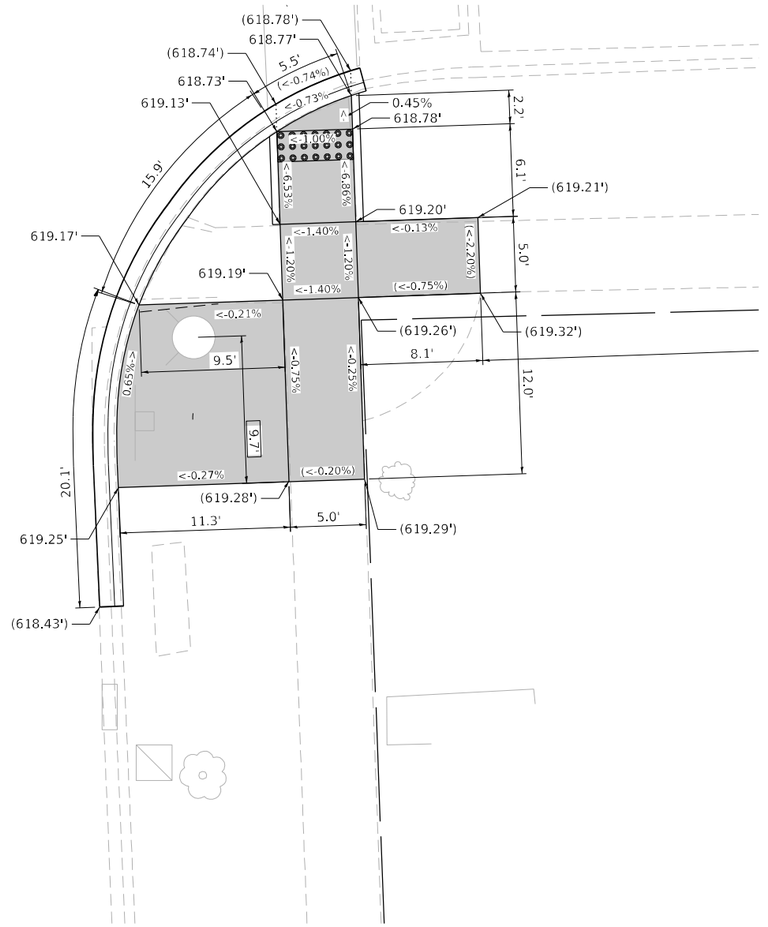
ADA RAMP DETAIL PLAN
PULASKI ROAD AND 94TH ST

SCALE: 1" = 5' | SHEET OF SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	17
		CONTRACT NO. 62T87		
	ILLINOIS	FED. AID PROJECT		

ADA DETAILS FOR PHASE 1 PROJECTS





PULASKI ROAD

REFERENCE BENCHMARK : ELEV. 619.533

BENCHMARK : CONTROL POINT CROSS ON CONCRETE LIGHT POLE FOUNDATION

LOCATION : 26.7' S OF CORNER AT SW CORNER
OF PULASKI ROAD AND 94TH STREET

LEGEND

XX.X'	EXISTING LENGTH
———	PROPOSED SIDE CURB
( )	EXISTING ELEVATION/SLOPE
	PROPOSED SIDEWALK
	DETECTABLE WARNINGS
	SIDEWALK REMOVAL REPLACE W/ TOPSOIL & SOD

INTERSECTION 61359

MODEL: $MODELNAME$
FILE NAME: $FILE$

SEA

USER NAME = $USER$	DESIGNED - JMM	REVISED -
	DRAWN - SVJ	REVISED -
PLOT SCALE = $SCALE$	CHECKED - SMS	REVISED -
PLOT DATE = $DATE$	DATE -	REVISED -

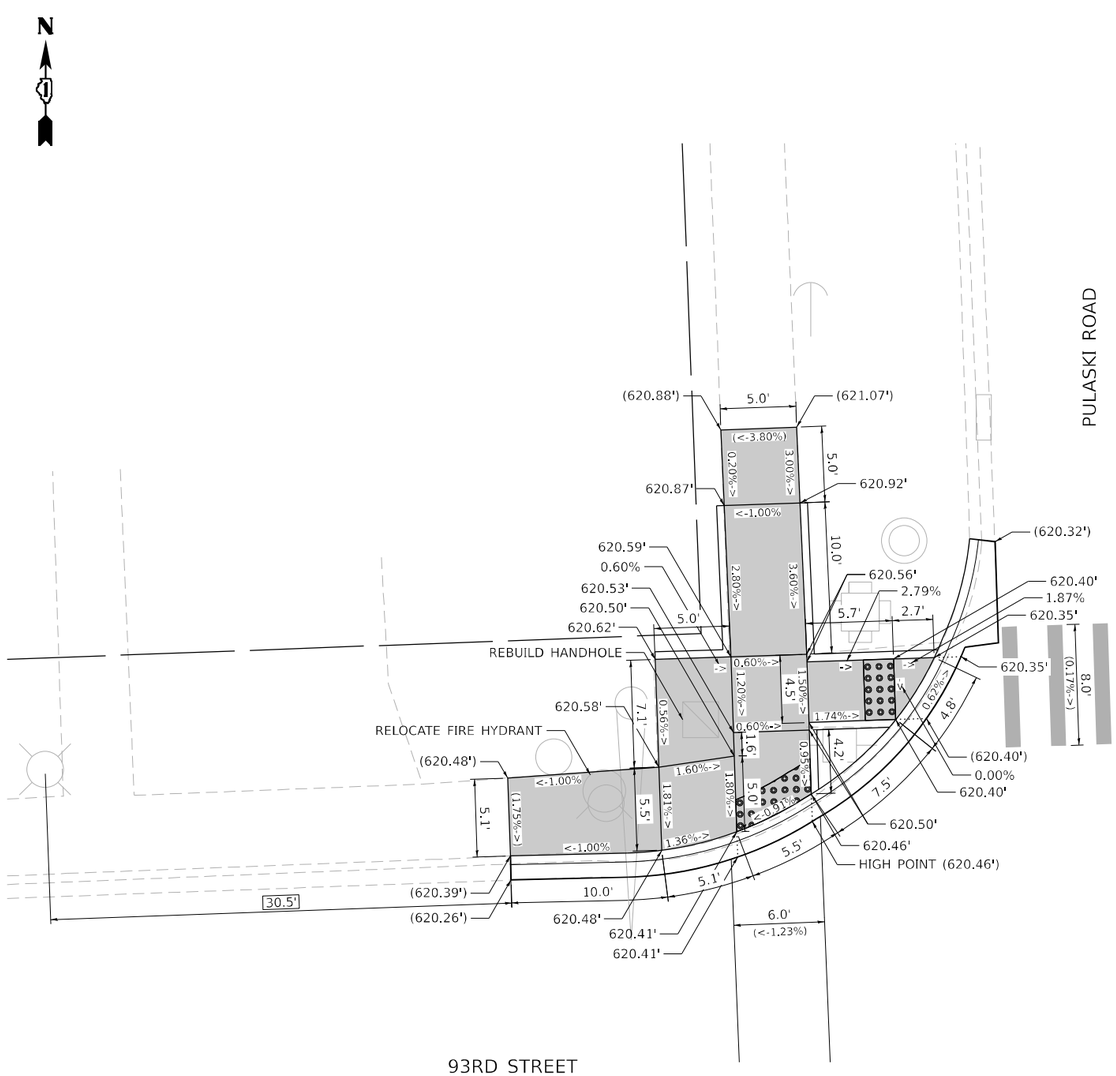
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

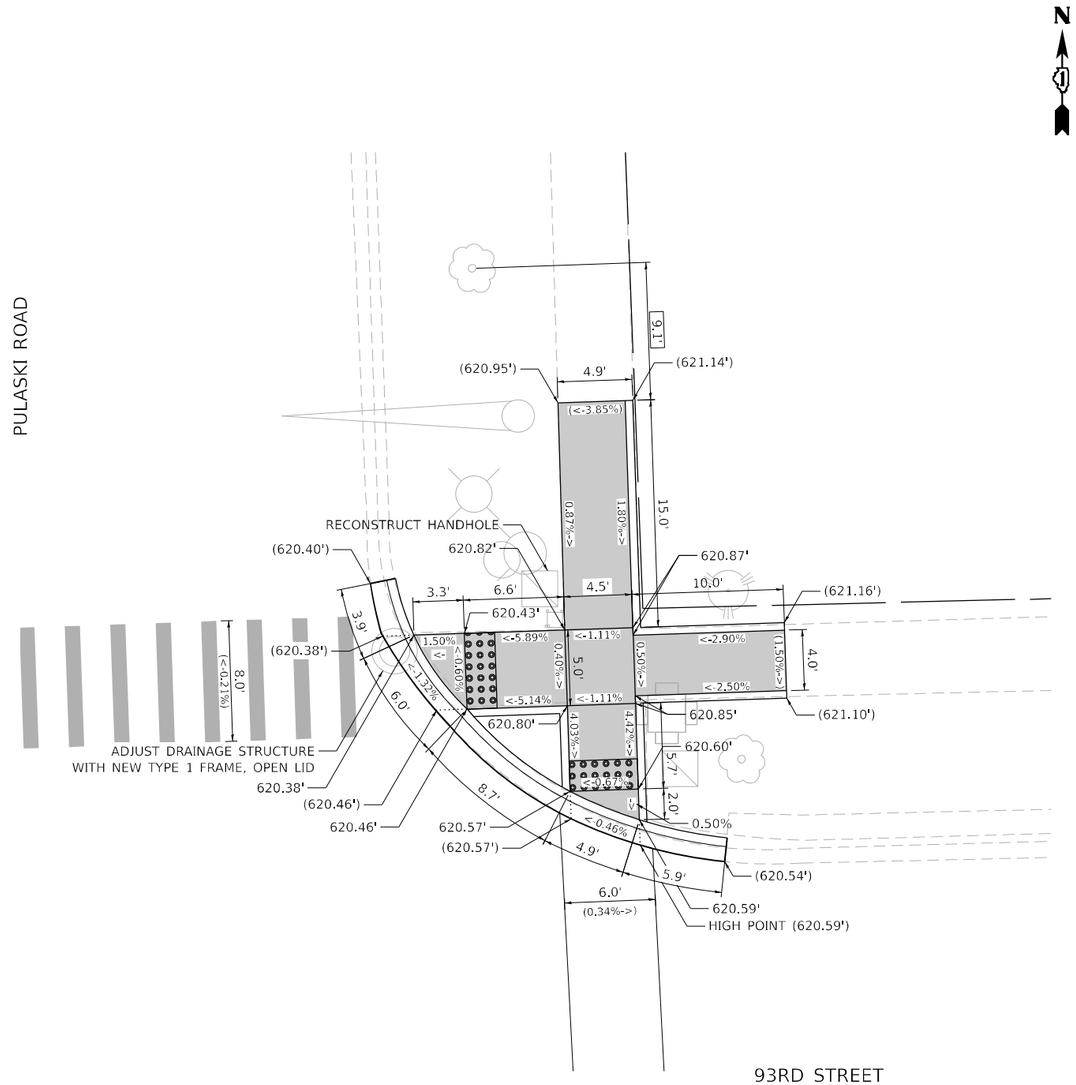
ADA RAMP DETAIL PLAN
PULASKI ROAD AND 94TH ST – OAK LAWN

SCALE: 1" = 5' | SHEET OF SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	18
		CONTRACT NO. 62T87		
		ILLINOIS	FED. AID PROJECT	

ADA DETAILS FOR PHASE 1 PROJECTS





REFERENCE BENCHMARK : ELEV. 621.21

BENCHMARK : CONTROL POINT CROSS ON CONCRETE CONTROLLER PLATFORM

LOCATION : 20.3' W OF CORNER AT SW CORNER
OF PULASKI ROAD AND 93RD STREET

LEGEND

XX.X'	EXISTING LENGTH
═══	PROPOSED SIDE CURB
( )	EXISTING ELEVATION/SLOPE
	PROPOSED SIDEWALK
	DETECTABLE WARNINGS
	SIDEWALK REMOVAL REPLACE W/ TOPSOIL & SOD

INTERSECTION 61358

MODEL: $MODELNAME$
FILE NAME: $FILE$

SEB

USER NAME = $USER$	DESIGNED - JMM	REVISED -
	DRAWN - SVJ	REVISED -
PLOT SCALE = $SCALE$	CHECKED - SMS	REVISED -
PLOT DATE = $DATE$	DATE -	REVISED -

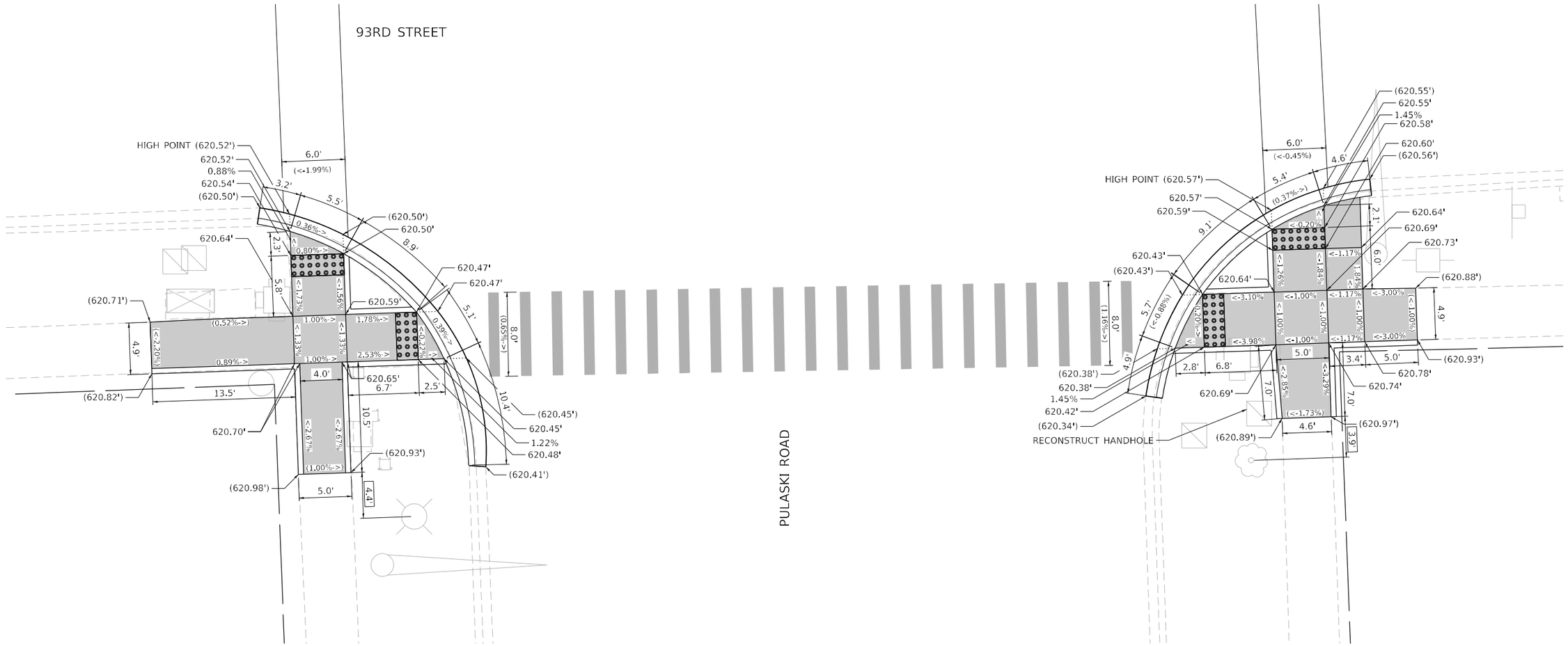
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAIL PLAN
PULASKI ROAD AND 93RD ST

SCALE: 1" = 5' | SHEET OF SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	19
		CONTRACT NO. 62T87		
		ILLINOIS	FED. AID PROJECT	

ADA DETAILS FOR PHASE 1 PROJECTS



REFERENCE BENCHMARK : ELEV. 621.21

BENCHMARK : CONTROL POINT CROSS ON CONCRETE CONTROLLER PLATFORM

LOCATION : 20.3' W OF CORNER AT SW CORNER
OF PULASKI ROAD AND 93RD STREET

LEGEND

XX.X'	EXISTING LENGTH
	PROPOSED SIDE CURB
( )	EXISTING ELEVATION/SLOPE
	PROPOSED SIDEWALK
	DETECTABLE WARNINGS
	SIDEWALK REMOVAL REPLACE W/ TOPSOIL & SOD

INTERSECTION 61358

MODEL: $MODELNAME$
FILE NAME: $FILE$

SEB

USER NAME = $USER$	DESIGNED - JMM	REVISED -	
	DRAWN - SVJ	REVISED -	
PLOT SCALE = $SCALE$	CHECKED - SMS	REVISED -	
PLOT DATE = $DATE$	DATE -	REVISED -	

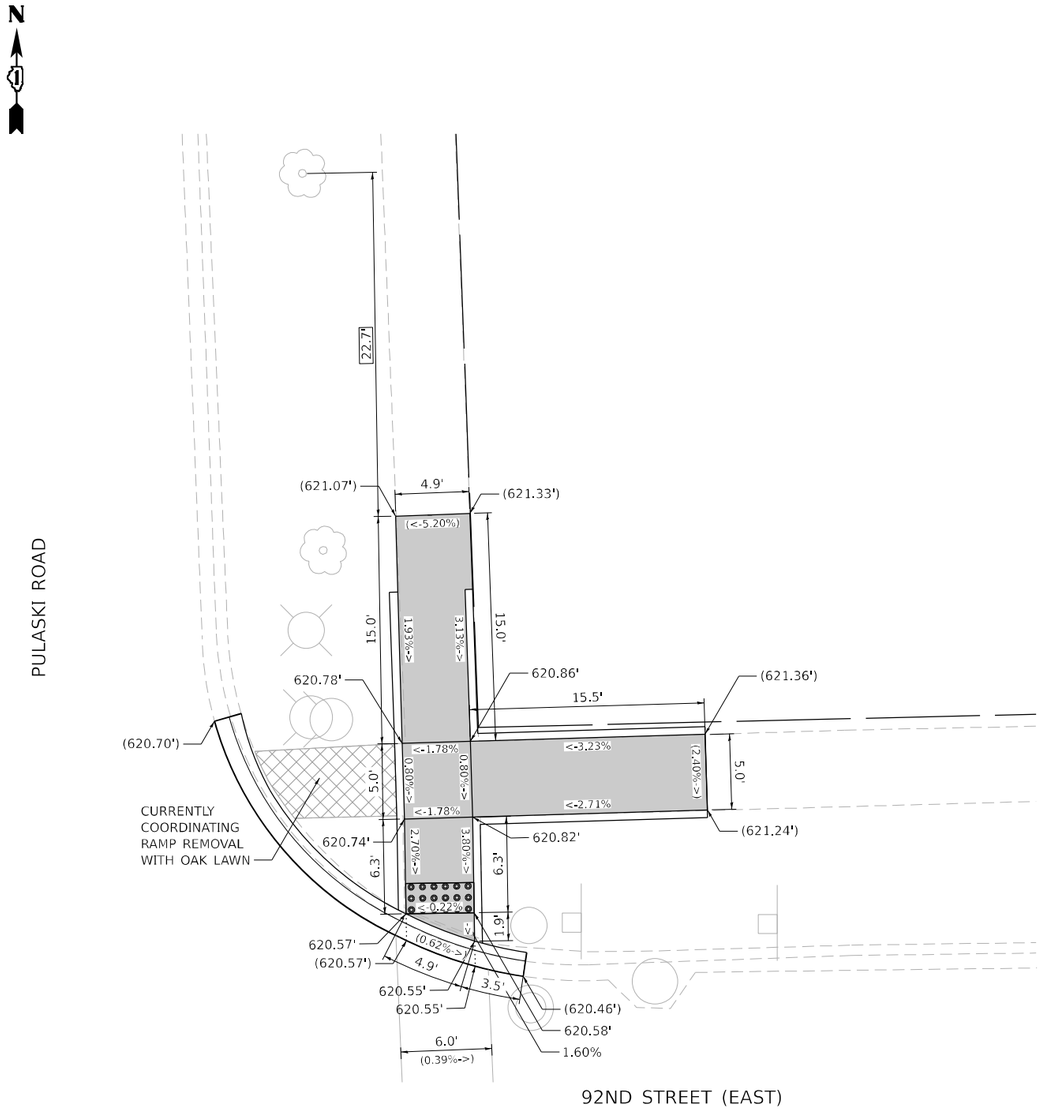
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

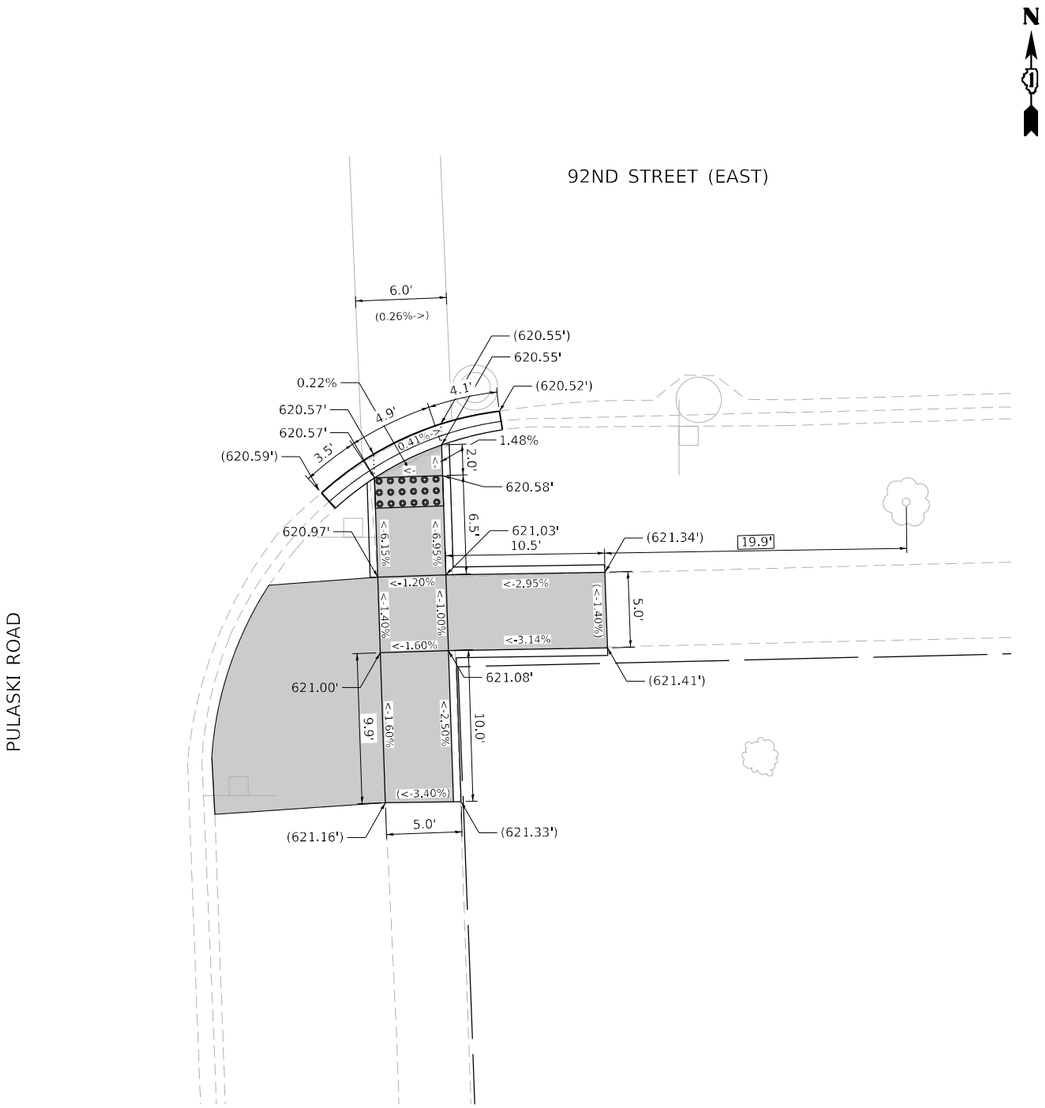
ADA RAMP DETAIL PLAN
PULASKI ROAD AND 93RD ST

SCALE: 1" = 5' | SHEET OF SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	20
		CONTRACT NO. 62T87		
		ILLINOIS	FED. AID PROJECT	

ADA DETAILS FOR PHASE 1 PROJECTS





REFERENCE BENCHMARK : ELEV. 621.641

BENCHMARK : CONCRETE LIGHT POLE FOUNDATION

LOCATION : 7.2' N OF CORNER AT NE CORNER
OF PULASKI ROAD AND 92ND STREET

LEGEND

XX.X'	EXISTING LENGTH
	PROPOSED SIDE CURB
( )	EXISTING ELEVATION/SLOPE
	PROPOSED SIDEWALK
	DETECTABLE WARNINGS
	SIDEWALK REMOVAL REPLACE W/ TOPSOIL & SOD

INTERSECTION 61357

MODEL: $MODELNAME$
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PLOT DATE = $DATE$	DATE -	REVISED -

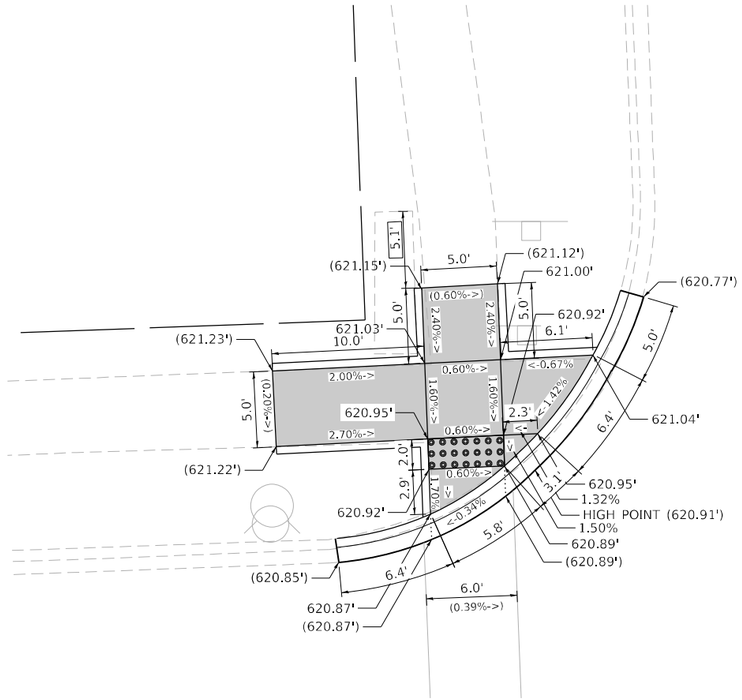
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAIL PLAN
PULASKI ROAD AND 92ND ST (EAST)

SCALE: 1" = 5' | SHEET OF SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	21
		CONTRACT NO. 62T87		
	ILLINOIS	FED. AID PROJECT		

ADA DETAILS FOR PHASE 1 PROJECTS



91ST PLACE

PULASKI ROAD

N

REFERENCE BENCHMARK : ELEV. 621.518

BENCHMARK : CONTROL POINT CROSS ON WALK

LOCATION : 40.8' S OF CORNER AT SE CORNER
OF PULASKI ROAD AND 91ST STREET

LEGEND

XX.X'	EXISTING LENGTH
═══	PROPOSED SIDE CURB
( )	EXISTING ELEVATION/SLOPE
	PROPOSED SIDEWALK
	DETECTABLE WARNINGS
	SIDEWALK REMOVAL REPLACE W/ TOPSOIL & SOD

INTERSECTION 61356

MODEL: $MODELNAME$
FILE NAME: $FILE$

SEB

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	DRAWN - SVJ	REVISED -	
PLOT SCALE = $SCALE$	CHECKED - SMS	REVISED -	
PLOT DATE = $DATE$	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAIL PLAN
PULASKI ROAD AND 91ST PL

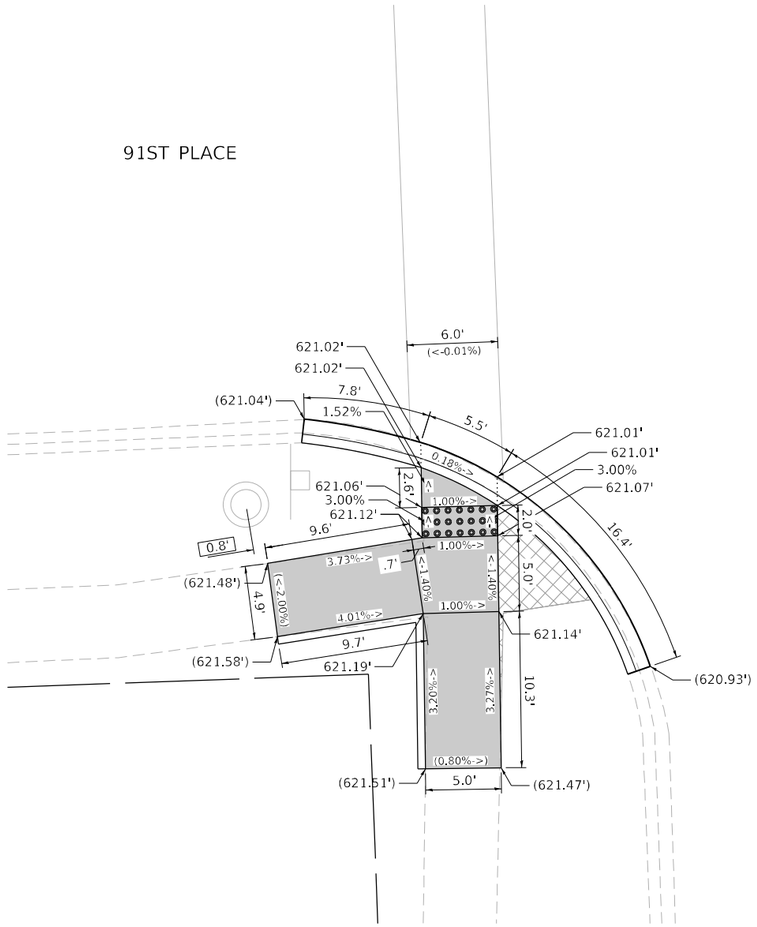
SCALE: 1" = 5' | SHEET OF SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	22
		CONTRACT NO. 62T87		
	ILLINOIS FED. AID PROJECT			

ADA DETAILS FOR PHASE 1 PROJECTS

N

91ST PLACE



PULASKI ROAD

REFERENCE BENCHMARK : ELEV. 621.518

BENCHMARK : CONTROL POINT CROSS ON WALK

LOCATION : 40.8' S OF CORNER AT SE CORNER
OF PULASKI ROAD AND 91ST STREET

LEGEND

XX.X'	EXISTING LENGTH
═══	PROPOSED SIDE CURB
( )	EXISTING ELEVATION/SLOPE
	PROPOSED SIDEWALK
	DETECTABLE WARNINGS
	SIDEWALK REMOVAL REPLACE W/ TOPSOIL & SOD

INTERSECTION 61356

MODEL: $MODELNAME$
FILE NAME: $FILE1$

SEA

USER NAME = $USER$	DESIGNED - JMM	REVISED -
	DRAWN - SVJ	REVISED -
PLOT SCALE = $SCALE$	CHECKED - SMS	REVISED -
PLOT DATE = $DATE$	DATE -	REVISED -

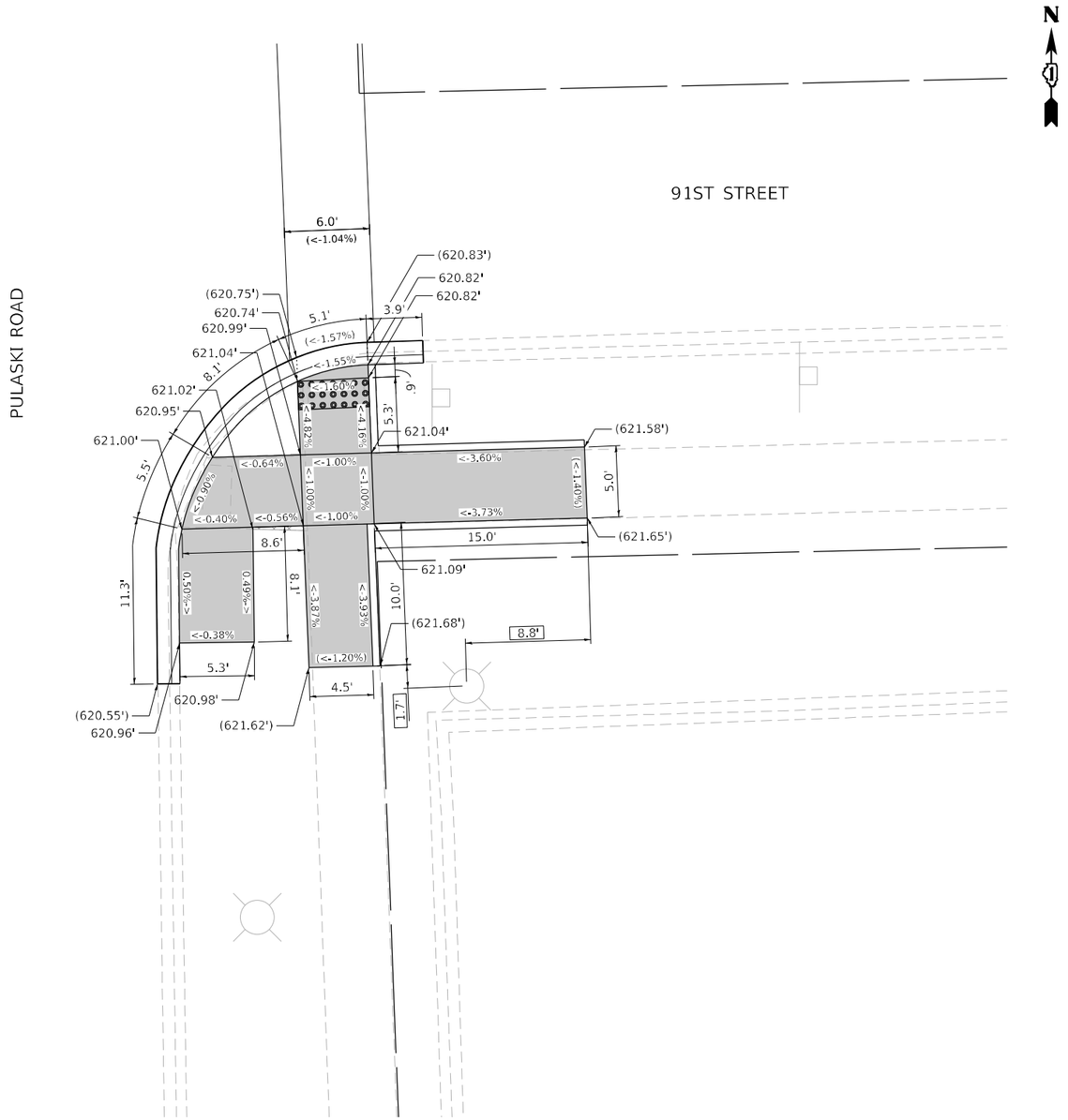
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAIL PLAN
PULASKI ROAD AND 91ST PL

SCALE: 1" = 5' | SHEET OF SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	23
		CONTRACT NO. 62T87		
		ILLINOIS	FED. AID PROJECT	

ADA DETAILS FOR PHASE 1 PROJECTS



REFERENCE BENCHMARK : ELEV. 621.746

BENCHMARK : CONTROL POINT CROSS ON CONCRETE LIGHT POLE FOUNDATION

LOCATION : 40.2' S OF CORNER AT SE CORNER
OF PULASKI ROAD AND 91ST STREET

LEGEND

XX.X'	EXISTING LENGTH
———	PROPOSED SIDE CURB
( )	EXISTING ELEVATION/SLOPE
	PROPOSED SIDEWALK
	DETECTABLE WARNINGS
	SIDEWALK REMOVAL REPLACE W/ TOPSOIL & SOD

INTERSECTION 64085

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FILE NAME: $FILEL$

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	DRAWN - SVJ	REVISED -
PLOT SCALE = $SCALE$	CHECKED - SMS	REVISED -
PLOT DATE = $DATE$	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAIL PLAN
PULASKI ROAD AND 91ST ST – OAK LAWN

SCALE: 1" = 5' | SHEET OF SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	24
		CONTRACT NO. 62T87		
		ILLINOIS	FED. AID PROJECT	

TRAFFIC SIGNAL LEGEND

(NOT TO SCALE)

ITEM	EXISTING	PROPOSED
CONTROLLER CABINET		
COMMUNICATION CABINET	CC	CC
MASTER CONTROLLER	MC	MC
MASTER MASTER CONTROLLER	MMC	MMC
UNINTERRUPTABLE POWER SUPPLY		
SERVICE INSTALLATION -(P) POLE MOUNTED	P	P
SERVICE INSTALLATION -(G) GROUND MOUNTED -(GM) GROUND MOUNTED METERED	G GM	G GM
TELEPHONE CONNECTION	ET	T
STEEL MAST ARM ASSEMBLY AND POLE		
ALUMINUM MAST ARM ASSEMBLY AND POLE		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE		
SIGNAL POST -(BM) BARREL MOUNTED - TEMPORARY		BM
WOOD POLE		
GUY WIRE		
SIGNAL HEAD		
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD OPTICALLY PROGRAMMED	P P	P P
FLASHER INSTALLATION -(FS) SOLAR POWERED	F FS F FS	F FS F FS
PEDESTRIAN SIGNAL HEAD		
PEDESTRIAN PUSH BUTTON -(APS) ACCESSIBLE PEDESTRIAN PUSH BUTTON	APS	APS
RADAR DETECTION SENSOR	R	R
VIDEO DETECTION CAMERA	V	V
RADAR/VIDEO DETECTION ZONE		
PAN, TILT, ZOOM (PTZ) CAMERA	PTZ	PTZ
EMERGENCY VEHICLE LIGHT DETECTOR		
CONFIMATION BEACON		
WIRELESS INTERCONNECT		
WIRELESS INTERCONNECT RADIO REPEATER	ERR	RR

ITEM	EXISTING	PROPOSED
HANDHOLE -SQUARE -ROUND		
HEAVY DUTY HANDHOLE -SQUARE -ROUND	H	H
DOUBLE HANDHOLE		
JUNCTION BOX	J	J
RAILROAD CANTILEVER MAST ARM		
RAILROAD FLASHING SIGNAL		
RAILROAD CROSSING GATE		
RAILROAD CROSSBUCK		
RAILROAD CONTROLLER CABINET	RR	RR
UNDERGROUND CONDUIT (UC), GALVANIZED STEEL		
TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE		
SYSTEM ITEM	S	SP
INTERSECTION ITEM	I	IP
REMOVE ITEM		R
RELOCATE ITEM		RL
ABANDON ITEM		A
CONTROLLER CABINET AND FOUNDATION TO BE REMOVED		RCF
MAST ARM POLE AND FOUNDATION TO BE REMOVED		RMF
SIGNAL POST AND FOUNDATION TO BE REMOVED		RPF
DETECTOR LOOP, TYPE I		
PREFORMED DETECTOR LOOP	P P	P P
SAMPLING (SYSTEM) DETECTOR	S S	S S
INTERSECTION AND SAMPLING (SYSTEM) DETECTOR	IS IS	IS IS
QUEUE AND SAMPLING (SYSTEM) DETECTOR	QS QS	QS QS
WIRELESS DETECTOR SENSOR	W	W
WIRELESS ACCESS POINT		

ITEM	EXISTING	PROPOSED
SIGNAL HEAD -(P) PROGRAMMABLE SIGNAL HEAD	R Y G ◄Y ◄G (P)	R Y G ◄Y ◄G (P)
SIGNAL HEAD WITH BACKPLATE -(P) PROGRAMMABLE SIGNAL HEAD -(RB) RETROREFLECTIVE BACKPLATE	R Y G ◄Y ◄G (P) (RB)	R Y G ◄Y ◄G (P) (RB)
PEDESTRIAN SIGNAL HEAD AT RAILROAD INTERSECTIONS		
PEDESTRIAN SIGNAL HEAD WITH COUNTDOWN TIMER	C D	C D
ILLUMINATED SIGN "NO LEFT TURN"/"NO RIGHT TURN"		
NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED	5	5
GROUND CABLE IN CONDUIT, NO. 6 SOLID COPPER (GREEN)	1#6	1#6
ELECTRIC CABLE IN CONDUIT, TRACER NO. 14 1/C	1	1
COAXIAL CABLE	C	C
VENDOR CABLE	V	V
COPPER INTERCONNECT CABLE, NO. 18, 3 PAIR TWISTED, SHIELDED	6#18	6#18
FIBER OPTIC CABLE -NO. 62.5/125, MM12F -NO. 62.5/125, MM12F SM12F -NO. 62.5/125, MM12F SM24F	12F 24F 36F	12F 24F 36F
GROUND ROD -(C) CONTROLLER -(M) MAST ARM -(P) POST -(S) SERVICE	C M P S	C M P S

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET 1 OF 7 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	25
	TS-05	CONTRACT NO. 62T87		
	ILLINOIS	FED. AID PROJECT		

LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.

2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.

3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.

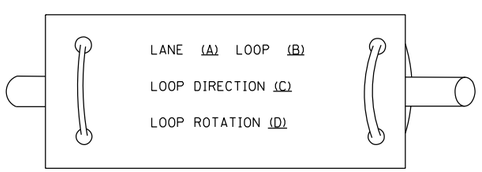
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.

5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.

6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.

7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

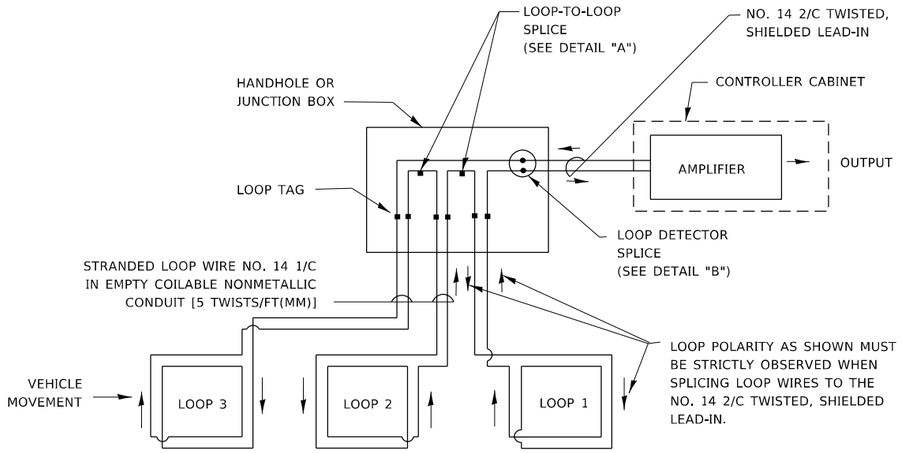


A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY

B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.

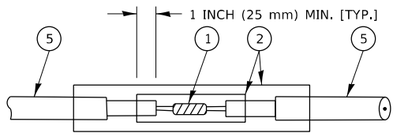
C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".

D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

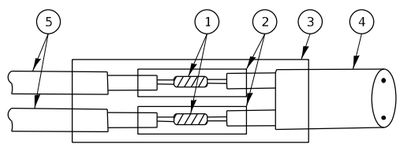


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES. SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE.
- THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

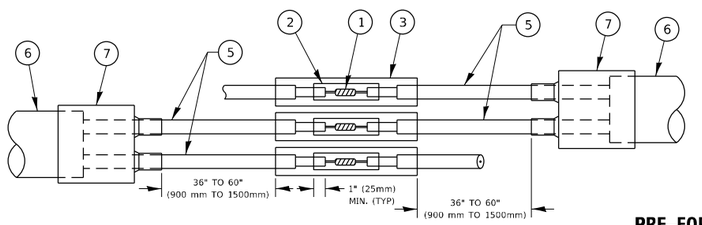


DETAIL "A"
LOOP-TO-LOOP SPLICE



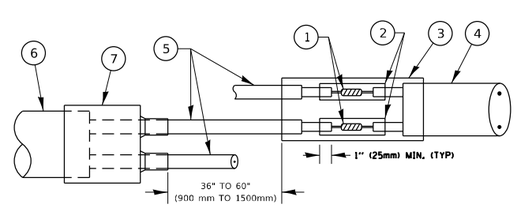
DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE

PRE-FORMED LOOP



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

(1) WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.

(2) WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.

(3) WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.

(4) NO. 14 2/C TWISTED, SHIELDED CABLE.

(5) LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE. PRE-FORMED LOOP

(6) XL POLYOLEFIN 2 CONDUCTOR

(7) BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

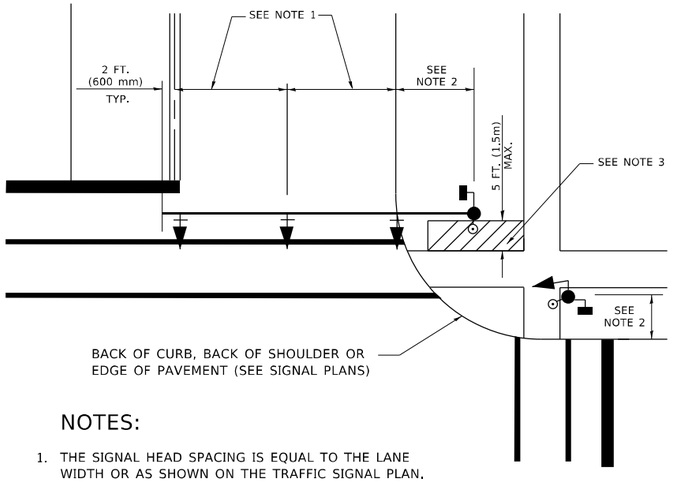
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET 2 OF 7 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	26
	TS-05	CONTRACT NO. 62T87		
	ILLINOIS FED. AID PROJECT			

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

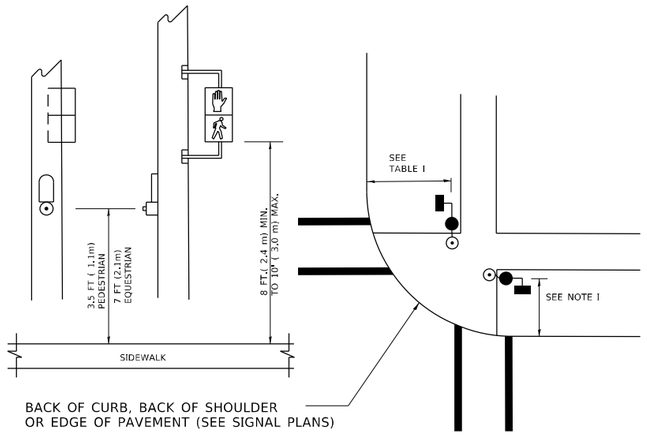
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

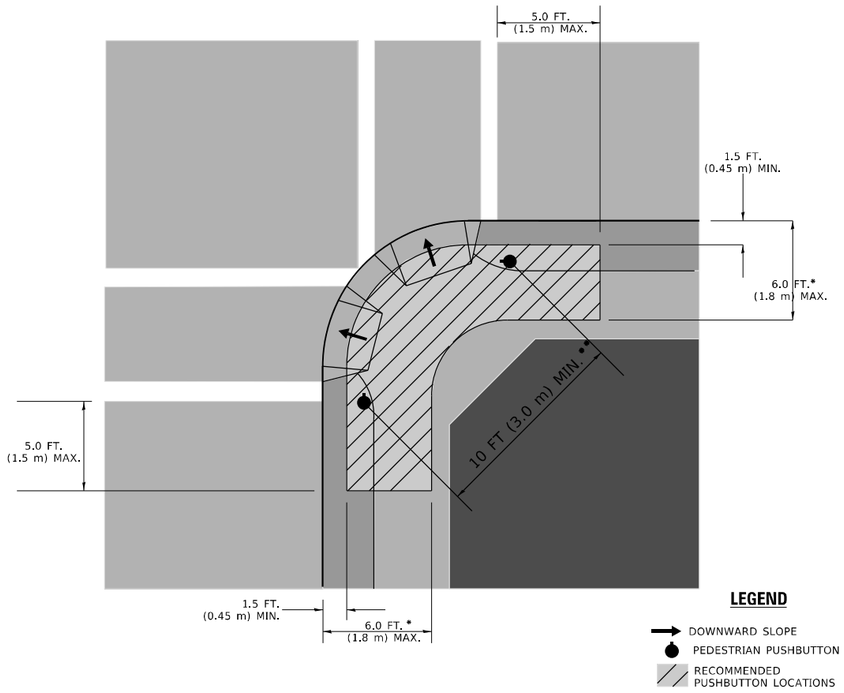
PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



* WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.

** WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TOTHE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

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PLOT DATE = 10/23/2023	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET 3 OF 7 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	27
	TS-05	CONTRACT NO. 62T87		
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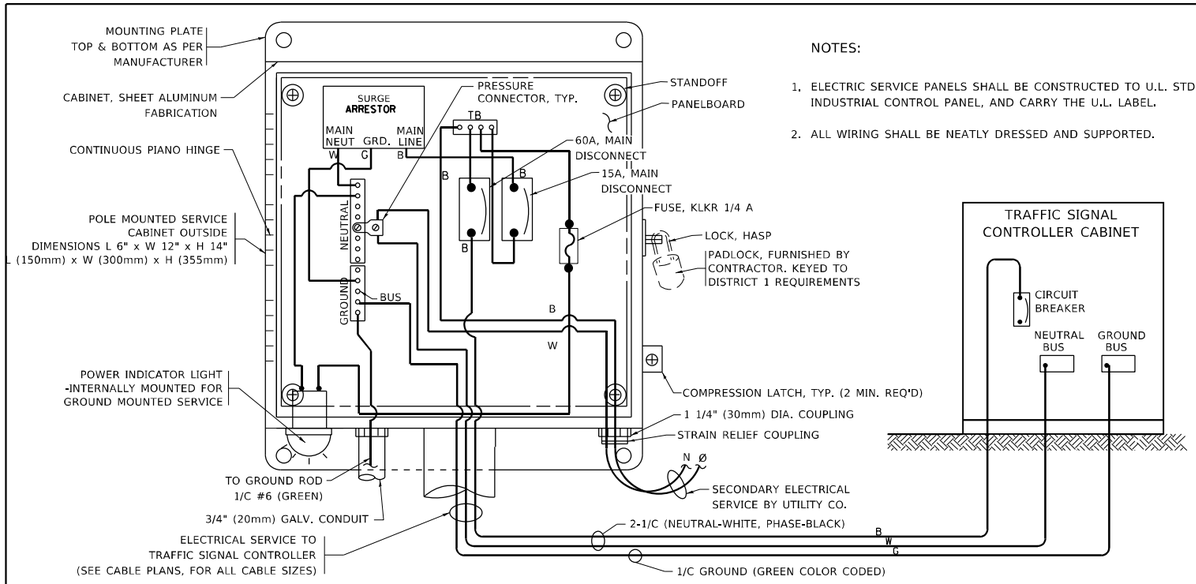


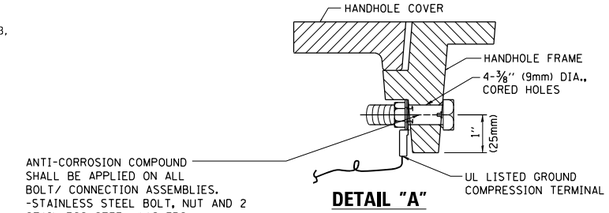
NOTES:

1. ELECTRIC SERVICE PANELS SHALL BE CONSTRUCTED TO U.L. STD 508, INDUSTRIAL CONTROL PANEL, AND CARRY THE U.L. LABEL.
2. ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.

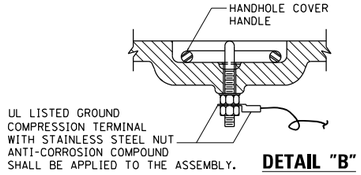
ELECTRICAL SERVICE – PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)

SERVICE INSTALLATION POLE MOUNT (SHOWN)

(NOT TO SCALE)



DETAIL "A"

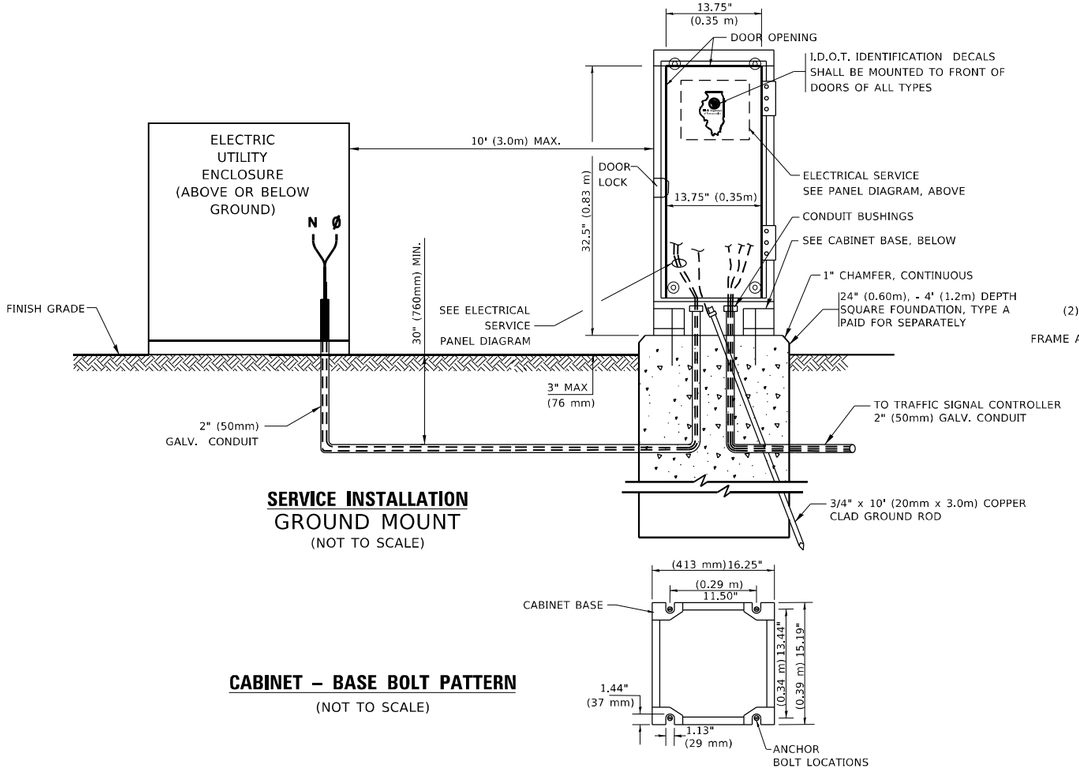


DETAIL "B"

NOTES:

GROUNDING SYSTEM

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC,). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



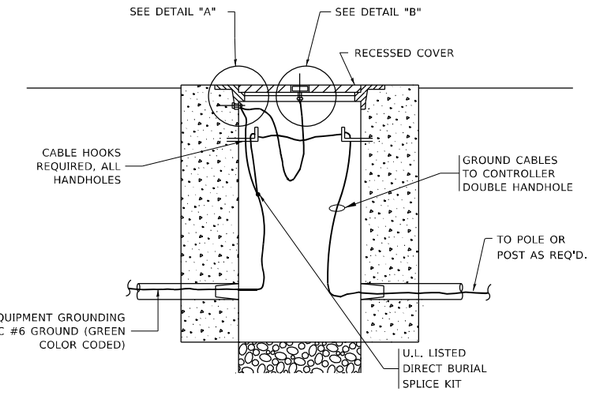
SERVICE INSTALLATION

GROUND MOUNT

(NOT TO SCALE)

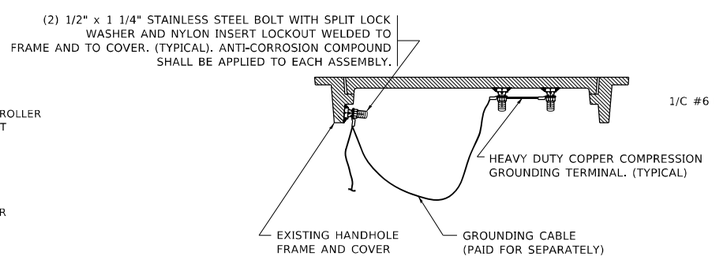
CABINET – BASE BOLT PATTERN

(NOT TO SCALE)



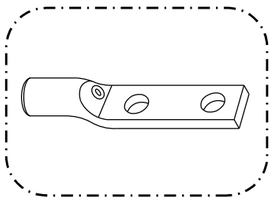
HANDHOLE COVER & FRAME – GROUNDING DETAIL

(NOT TO SCALE)

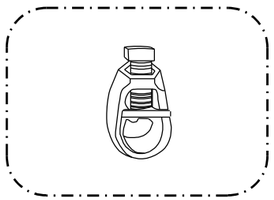


EXISTING HANDHOLE COVER & FRAME – GROUNDING DETAIL

(NOT TO SCALE)



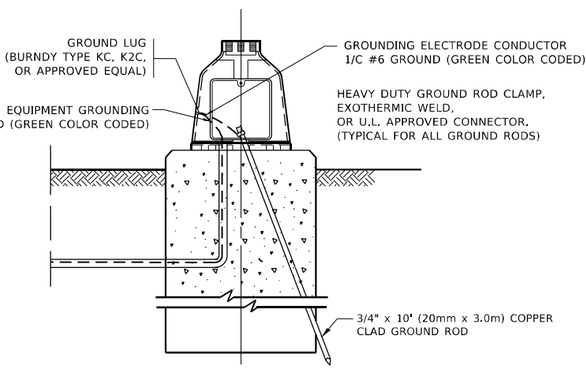
HEAVY-DUTY COMPRESSION TERMINAL (BURNDY TYPE YGHA OR APPROVED EQUAL)



¾" (20mm) HEAVY-DUTY GROUND ROD CLAMP (BURNDY TYPE GRC OR APPROVED EQUAL)

NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



MAST ARM POLE /POST–GROUNDING DETAIL

(NOT TO SCALE)

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PLOT DATE = 10/23/2023	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET 4 OF 7 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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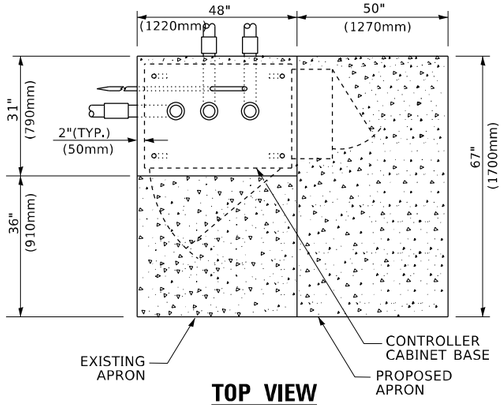
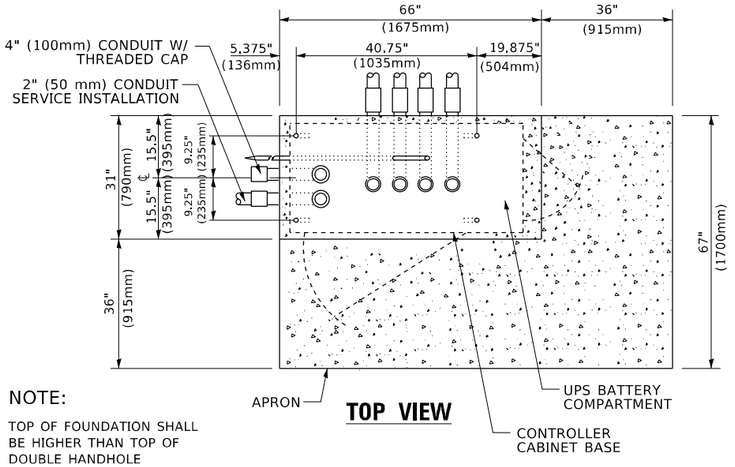
	TS-05	CONTRACT NO. 62T87		
	ILLINOIS FED. AID PROJECT			



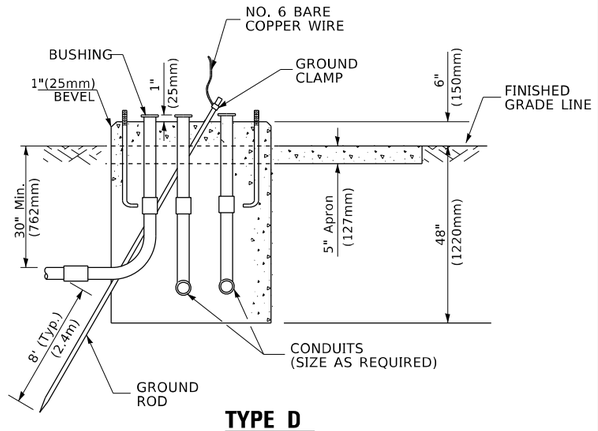
TOP VIEW



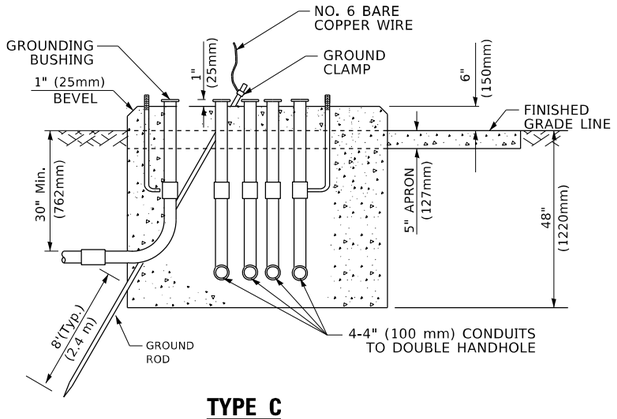
NOTE:

TOP OF FOUNDATION SHALL BE HIGHER THAN TOP OF DOUBLE HANDHOLE

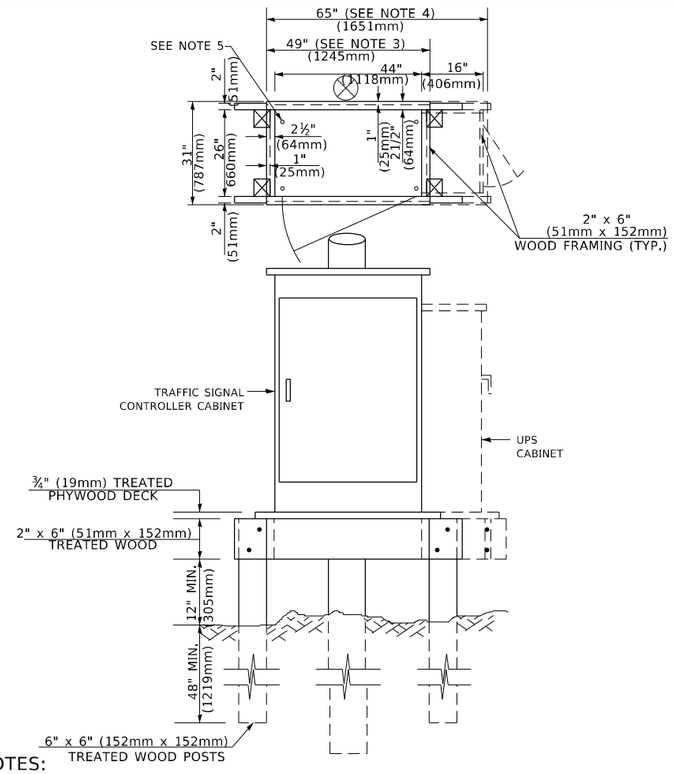
TOP VIEW



TYPE D
FOR GROUND MOUNTED CONTROLLER CABINET AND UPS BATTERY CABINET



TYPE C
FOR GROUND MOUNTED SUPER P (TYPE IV) AND SUPER R (TYPE V) CONTROLLER CABINETS



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION..

TEMPORARY SIGNAL CONTROLLER WOOD SUPPORT PLATFORM

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

Mast Arm Length	① Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations
4. For mast arm assemblies with dual arms refer to state standard 878001..

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET 5 OF 7 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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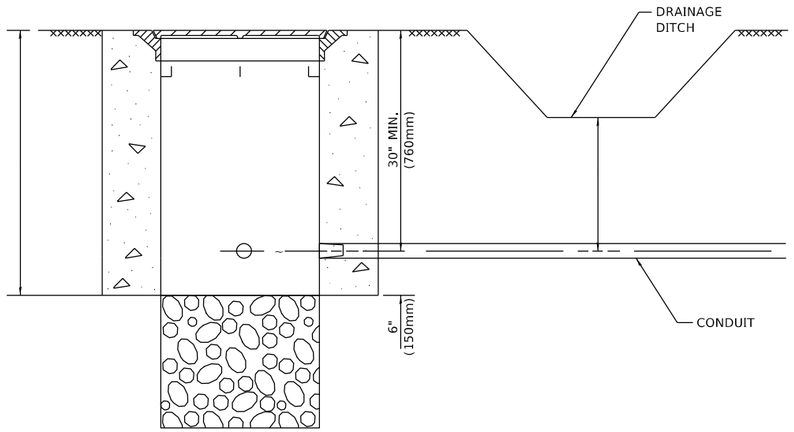
	TS-05	CONTRACT NO. 62T87		
	ILLINOIS	FED. AID PROJECT		

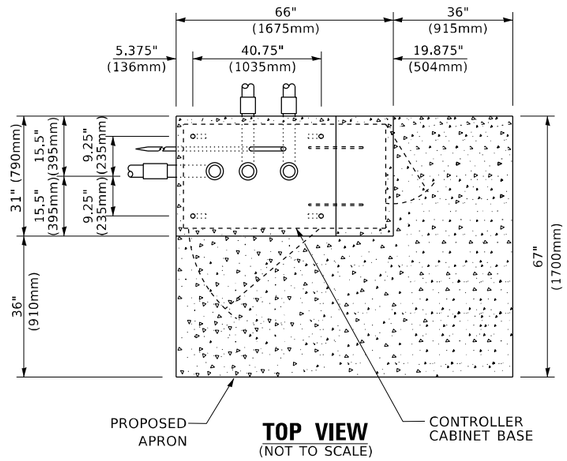


NOTES:

1. CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
2. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
3. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

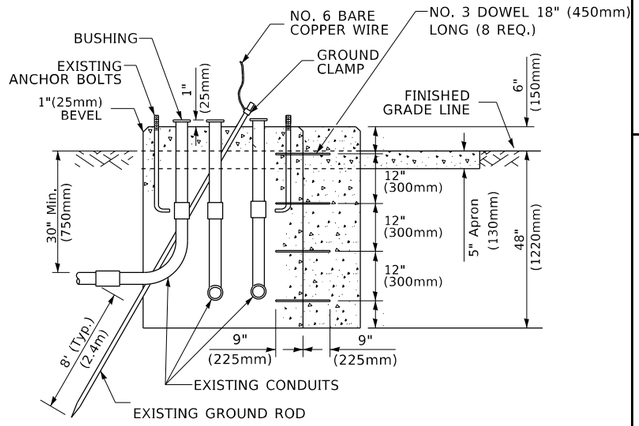
HANDHOLE WITH MINIMUM CONDUIT DEPTH

(NOT TO SCALE)



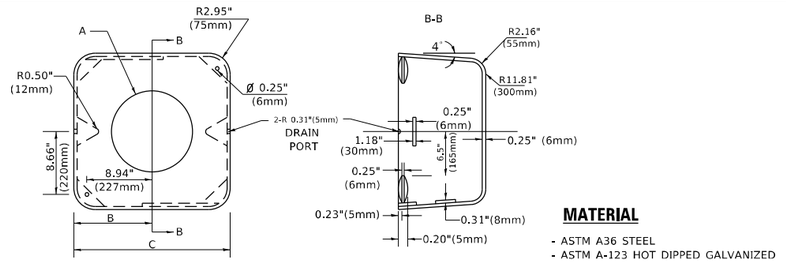
TOP VIEW

(NOT TO SCALE)



MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION

(NOT TO SCALE)



MATERIAL

- ASTM A36 STEEL
- ASTM A-123 HOT DIPPED GALVANIZED

A	B	C	HEIGHT	WEIGHT
VARIES	9.5"(241mm)	19"(483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIES	10.75"(273mm)	21.5"(546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIES	13.0"(330mm)	26"(660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIES	18.5"(470mm)	37"(940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

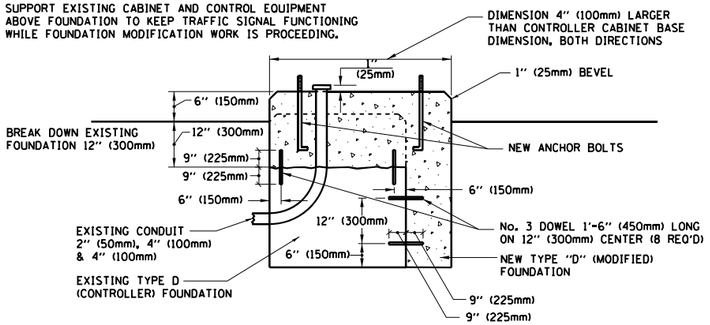
SHROUD

NOTES:

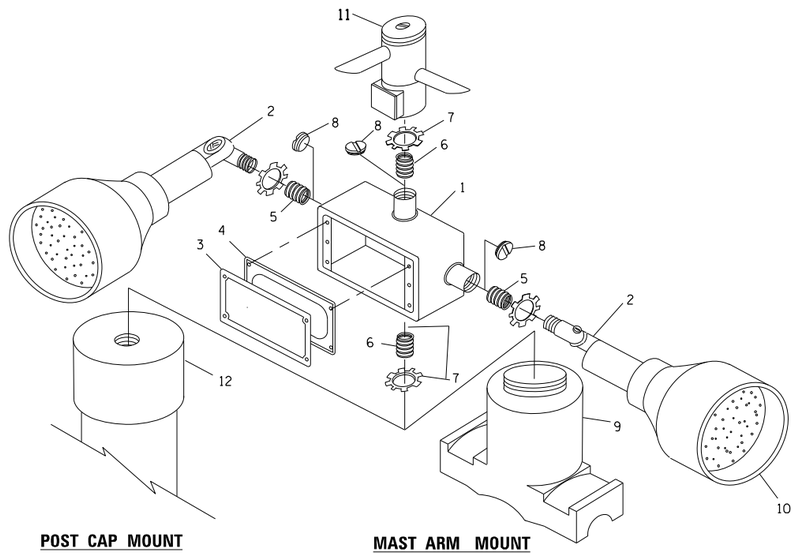
1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
2. THE SUPPLIER SHALL VERIFIED THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

NOTE:

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION



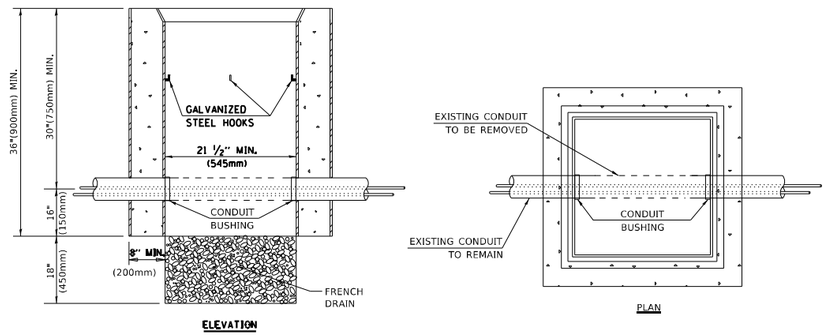
POST CAP MOUNT **MAST ARM MOUNT**

EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL

ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	¾"(19 mm) CLOSE NIPPLE
7	¾"(19 mm) LOCKNUT
8	¾"(19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

NOTES:

1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
   ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
   ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4 "(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



ELEVATION PLAN

NOTES:

1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET 6 OF 7 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	30
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	ILLINOIS FED. AID PROJECT			

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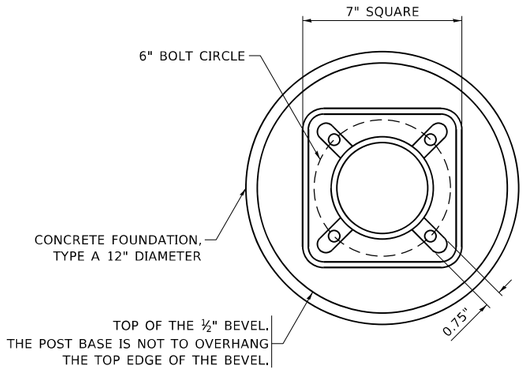
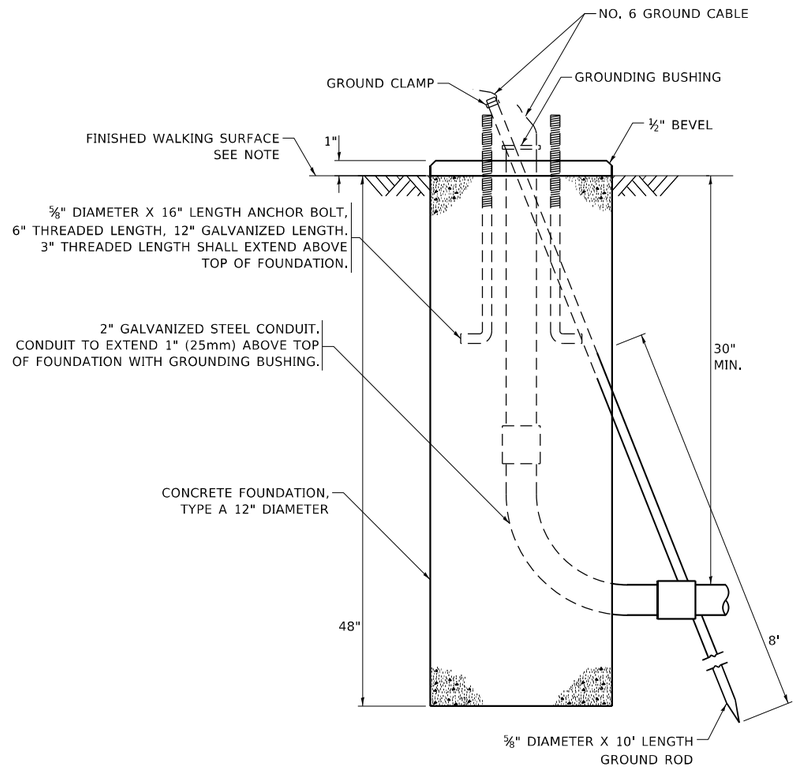


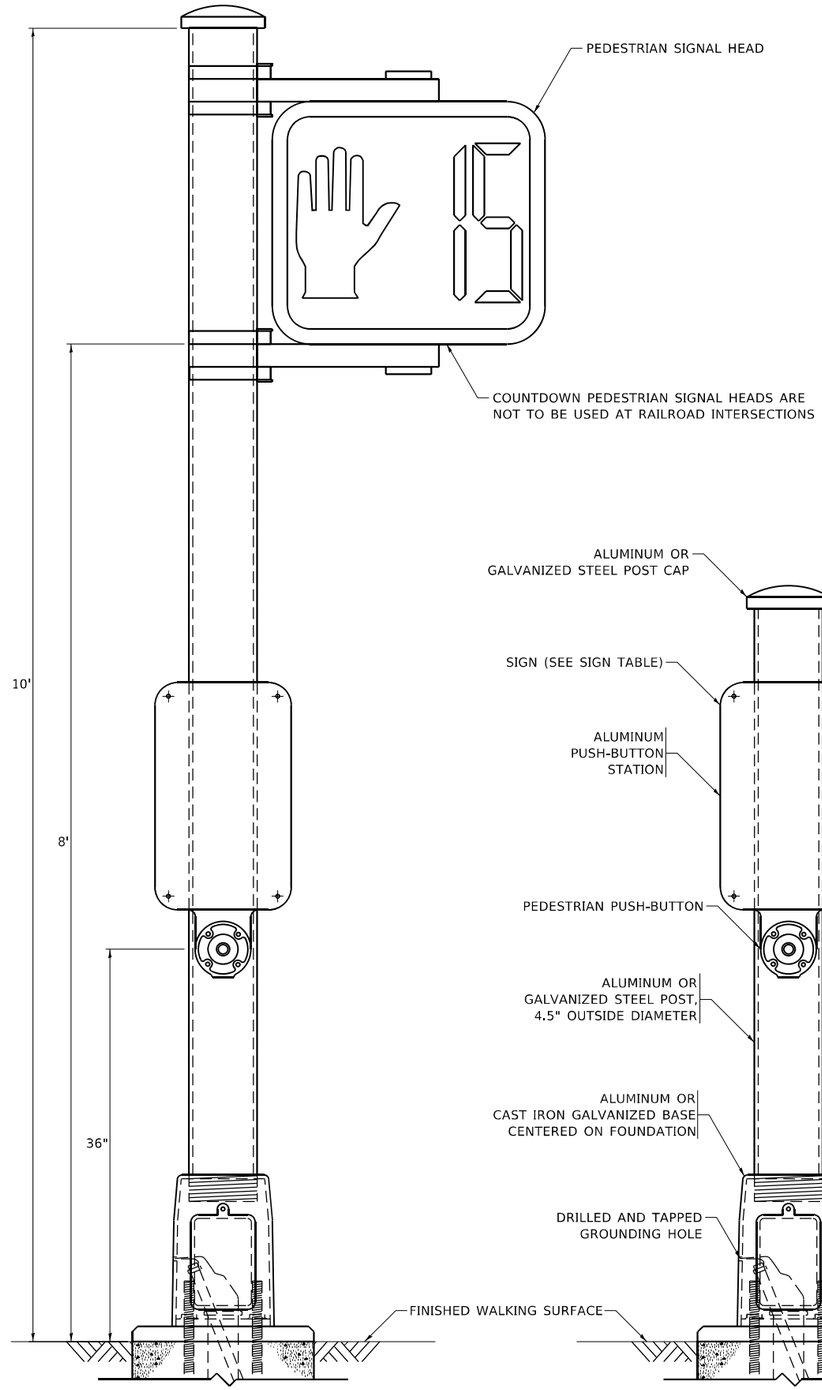
BOLT PATTERN

NOTE:

1. IF THE PEDESTRIAN SIGNAL POST FOUNDATION IS INSTALLED WITHIN OR BEHIND A BARRIER CURB, THE TOP OF THE FOUNDATION SHALL BE INSTALLED FLUSH WITH THE TOP OF THE BARRIER CURB.



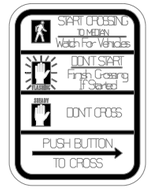
CONCRETE FOUNDATION, TYPE A 12–INCH DIAMETER



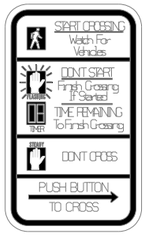
PEDESTRIAN SIGNAL POST, 10 FT.

PEDESTRIAN SIGNAL POST, 5 FT.

R10–3b



R10–3d



R10–3e

SIGN TABLE

SIGN	DIMENSIONS
R10-3b (RAILROAD ONLY)	9" X 12"
R10-3d (RAILROAD ONLY)	9" X 12"
R10-3e	9" X 12"

NOTES:

1. THE SIGN PANELS SHALL BE TYPE AP SHEETING.
2. THE ARROW ON SIGNS FOR PUSH-BUTTONS SERVING TWO DIRECTIONS ON THE SAME PHASE SHALL BE BI-DIRECTIONAL.
3. THE SIGN FOR DUAL-CALL PUSH-BUTTONS SHALL HAVE NO ARROW.

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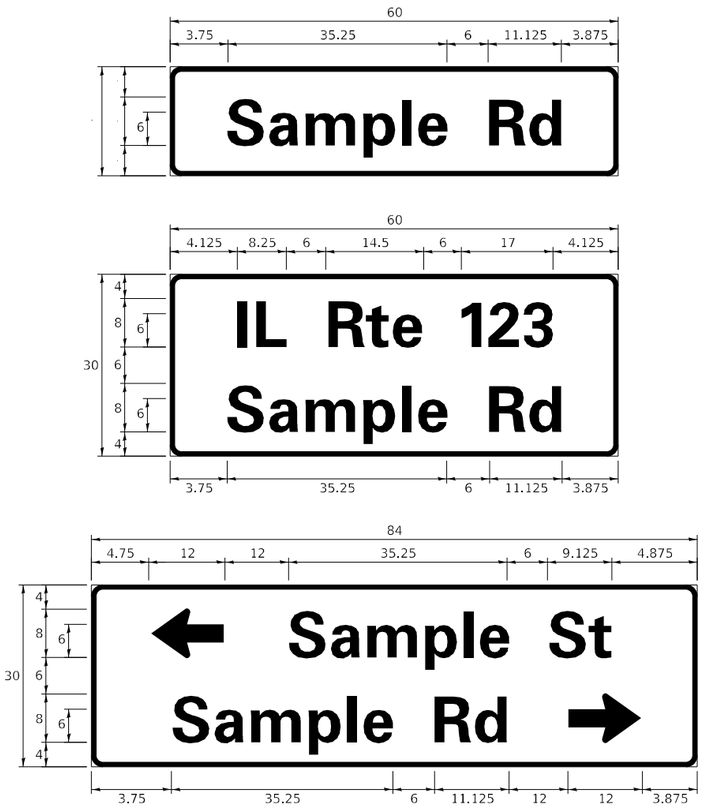
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET 7 OF 7 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	31
	TS–05	CONTRACT NO. 62T87		
		ILLINOIS FED. AID PROJECT		

SIGN PANEL – TYPE 1 OR TYPE 2



DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY. REQUIRED
D OR C	-	1 OR 2	ZZ	-

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COMMON STREET NAME ABBREVIATIONS AND WIDTHS

NAME	ABBREVATION	WIDTH (INCH)	
		SERIES "C"	SERIES "D"
AVENUE	Ave	15.000	18.250
BOULEVARD	Blvd	17.125	20.000
CIRCLE	Cir	11.125	13.000
COURT	Ct	8.250	9.625
DRIVE	Dr	8.625	10.125
HIGHWAY	Hwy	18.375	22.000
ILLINOIS	IL	7.000	8.250
LANE	Ln	9.125	10.750
PARKWAY	Pkwy	23.375	27.375
PLACE	Pl	7.125	7.750
ROAD	Rd	9.625	11.125
ROUTE	Rte	12.625	14.500
STREET	St	8.000	9.125
TERRACE	Ter	12.625	14.625
TRAIL	Tr	7.750	9.125
UNITED STATES	US	10.375	12.250

GENERAL NOTES

1. WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 877001, 877002, 877006, 877011 AND 877012, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 8'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
2. ALL SIGNS SHALL CONSIST OF A WHITE LEGEND AND BORDER (TYPE ZZ SHEETING) ON A GREEN BACKGROUND (TYPE ZZ SHEETING)
3. THE SIGN LENGTH SHALL BE IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHALL NOT EXCEED 8'-0". ALL BORDERS IF POSSIBLE, BUT MAY BE REDUCED TO 5" WHEN SPACING IS CRITICAL. A MINIMUM OF 2-1/2" SHALL BE INCLUDED BETWEEN THE WORD AND THE RIGHT AND LEFT EDGES OF THE SIGN.
4. A PREFERRED METHOD FOR THE SIGN DESIGN IS TO USE SERIES "D" LETTER ON A ONE-LINE SIGN 18" IN HEIGHT AND A MAXIMUM OF 8'-0" IN WIDTH. IF SERIES "D" DOES NOT FIT ON A 8"-0" SIGN, THEN SERIES "C" SHOULD BE TRIED. IF SERIES "C" DOES NOT FIT ON A 8'-0" SIGN, A 30" HIGH TWO-LINE SIGN CAN BE USED. THE CROSSROAD DESIGNATION AS TO STREET, AVENUE, ETC. SHOULD BE SPELLED OUT ON THE SECOND LINE, IF THE ABBREVIATION CANNOT FIT ON THE FIRST LINE.
5. LED ILLUMINATED STREET NAME SIGNS CAN BE USED IN PLACE OF REGULAR SIGN PANELS BUT ANY SPECIAL WORDING AND SYMBOLOGY MUST BE APPROVED BY THE DEPARTMENT. GENERAL DESIGN REQUIREMENT AS LISTED ABOVE (COLOR, FONT, SIZE, ETC.) MUST BE FOLLOWED.
6. SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS.

LOCAL SUPPLIERS:

- J.O. HERBERT COMPANY, INC
  MIDLOTHIAN, VA
- WESTERN REMAC, INC.
  WOODRIDGE, IL

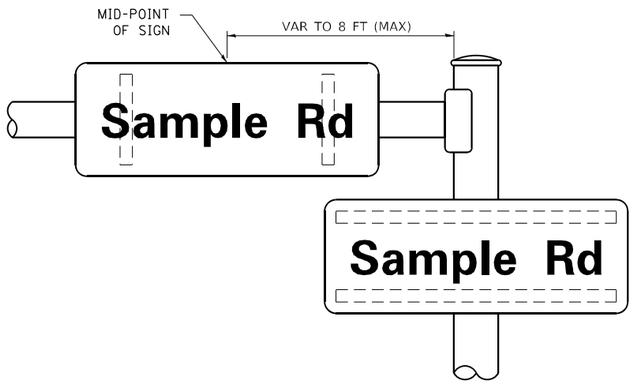
PARTS LISTING:

SIGN CHANNEL	PART #HPN053 (MED. CHANNEL)
SIGN SCREWS	1/4" x 14 x 1" H.W.H. #3 SELF TAPPING WITH NEOPRENE WASHER
BRACKETS	PART #HPN034 (UNIVERSAL) CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING

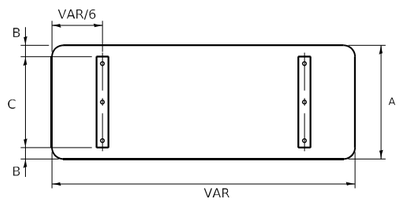
OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BRACKET OF THE ABOVE PRODUCT.

MOUNTING LOCATION

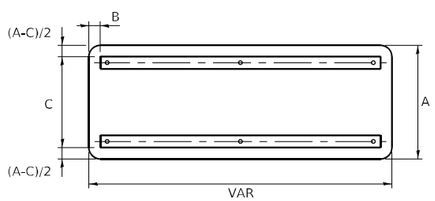
ARM OR POLE MOUNTED



SUPPORTING CHANNELS



A	B	C
18"	2"	14"
30"	2"	24"



A	B	C
18"	2"	12"
30"	2"	22"

STANDARD ALPHABETS SPACING CHART

(8") UPPER CASE AND (6") LOWER CASE

FHWA SERIES "C"				FHWA SERIES "D"			
CHARACTER	LEFT SPACING (INCH)	WIDTH (INCH)	RIGHT SPACING (INCH)	CHARACTER	LEFT SPACING (INCH)	WIDTH (INCH)	RIGHT SPACING (INCH)
A	0.240	5.122	0.240	A	0.240	6.804	0.240
B	0.880	4.482	0.480	B	0.960	5.446	0.400
C	0.720	4.482	0.720	C	0.800	5.446	0.800
D	0.880	4.482	0.720	D	0.960	5.446	0.800
E	0.880	4.082	0.480	E	0.960	4.962	0.400
F	0.880	4.082	0.240	F	0.960	4.962	0.240
G	0.720	4.482	0.720	G	0.800	5.446	0.800
H	0.880	4.482	0.880	H	0.960	5.446	0.960
I	0.880	1.120	0.880	I	0.960	1.280	0.960
J	0.240	4.082	0.880	J	0.240	5.122	0.960
K	0.880	4.482	0.480	K	0.960	5.604	0.400
L	0.880	4.082	0.240	L	0.960	4.962	0.240
M	0.880	5.284	0.880	M	0.960	6.244	0.960
N	0.880	4.482	0.880	N	0.960	5.446	0.960
O	0.720	4.722	0.720	O	0.800	5.684	0.800
P	0.880	4.482	0.720	P	0.960	5.446	0.240
Q	0.720	4.722	0.720	Q	0.800	5.684	0.800
R	0.880	4.482	0.480	R	0.960	5.446	0.400
S	0.480	4.482	0.480	S	0.400	5.446	0.400
T	0.240	4.082	0.240	T	0.240	4.962	0.240
U	0.880	4.482	0.880	U	0.960	5.446	0.960
V	0.240	4.962	0.240	V	0.240	6.084	0.240
W	0.240	6.084	0.240	W	0.240	7.124	0.240
X	0.240	4.722	0.240	X	0.400	5.446	0.400
Y	0.240	5.122	0.240	Y	0.240	6.884	0.240
Z	0.480	4.482	0.480	Z	0.400	5.446	0.400
a	0.320	3.842	0.640	a	0.400	4.562	0.720
b	0.720	4.082	0.480	b	0.800	4.802	0.480
c	0.480	4.002	0.240	c	0.480	4.722	0.240
d	0.480	4.082	0.720	d	0.480	4.802	0.800
e	0.480	4.082	0.320	e	0.480	4.722	0.320
f	0.320	2.480	0.160	f	0.320	2.882	0.160
g	0.480	4.082	0.720	g	0.480	4.802	0.800
h	0.720	4.082	0.640	h	0.800	4.722	0.720
i	0.720	1.120	0.720	i	0.800	1.280	0.800
j	0.000	2.320	0.720	j	0.000	2.642	0.800
k	0.720	4.322	0.160	k	0.800	5.122	0.160
l	0.720	1.120	0.720	l	0.800	1.280	0.800
m	0.720	6.724	0.640	m	0.800	7.926	0.720
n	0.720	4.082	0.640	n	0.800	4.722	0.720
o	0.480	4.082	0.480	o	0.480	4.882	0.480
p	0.720	4.082	0.480	p	0.800	4.802	0.480
q	0.480	4.082	0.720	q	0.480	4.802	0.800
r	0.720	2.642	0.160	r	0.800	3.042	0.160
s	0.320	3.362	0.240	s	0.320	3.762	0.240
t	0.080	2.882	0.080	t	0.080	3.202	0.080
u	0.640	4.082	0.720	u	0.720	4.722	0.800
v	0.160	4.722	0.160	v	0.160	5.684	0.160
w	0.160	7.524	0.160	w	0.160	9.046	0.160
x	0.000	5.202	0.000	x	0.000	6.244	0.000
y	0.160	4.962	0.160	y	0.160	6.004	0.160
z	0.240	3.362	0.240	z	0.240	4.002	0.240
1	0.720	1.680	0.880	1	0.800	2.000	0.960
2	0.480	4.482	0.480	2	0.800	5.446	0.800
3	0.480	4.482	0.480	3	1.440	5.446	0.800
4	0.240	4.962	0.720	4	0.160	6.004	0.960
5	0.480	4.482	0.480	5	0.800	5.446	0.800
6	0.720	4.482	0.720	6	0.800	5.446	0.800
7	0.240	4.482	0.720	7	0.560	5.446	0.560
8	0.480	4.482	0.480	8	0.800	5.446	0.800
9	0.480	4.482	0.480	9	0.800	5.446	0.800
0	0.720	4.722	0.720	0	0.800	5.684	0.800
-	0.240	2.802	0.240	-	0.240	2.802	0.240

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	DRAWN - LP	REVISED -	
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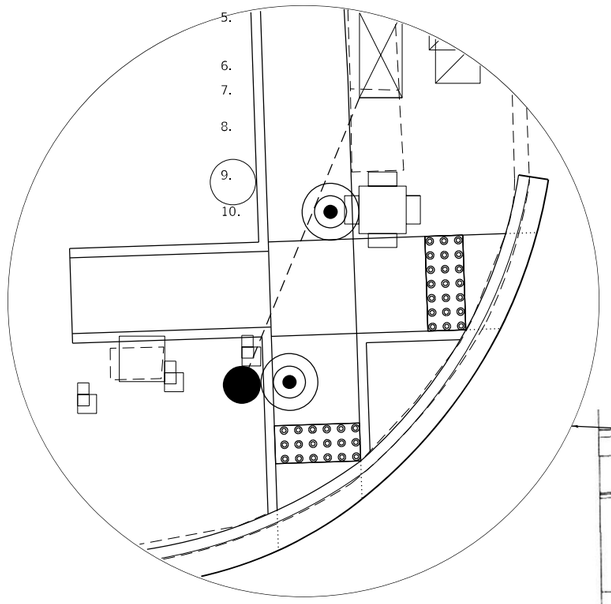
PLOT DATE = 10/23/2023	DATE - 10/01/2014	REVISED -	

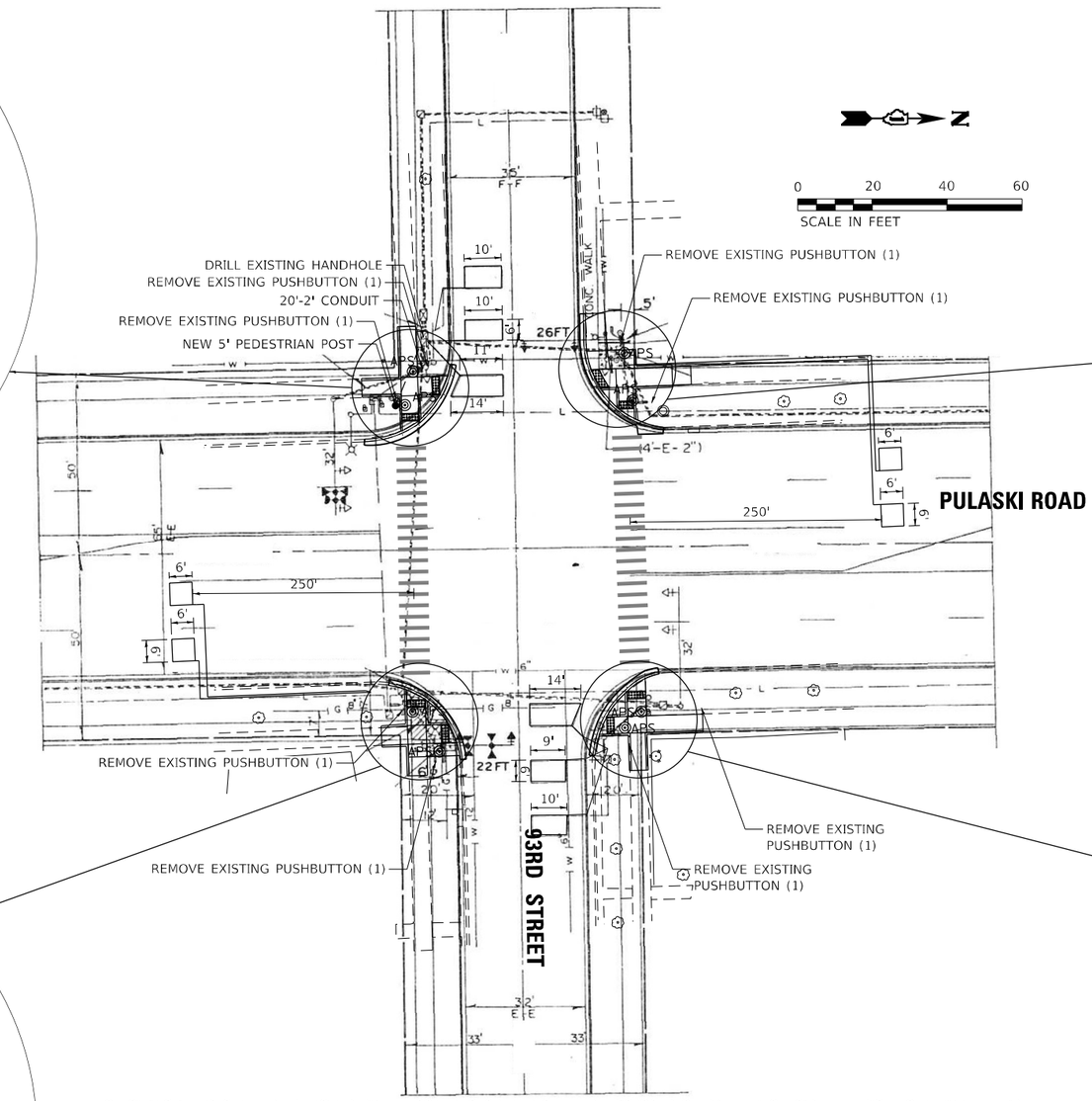
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

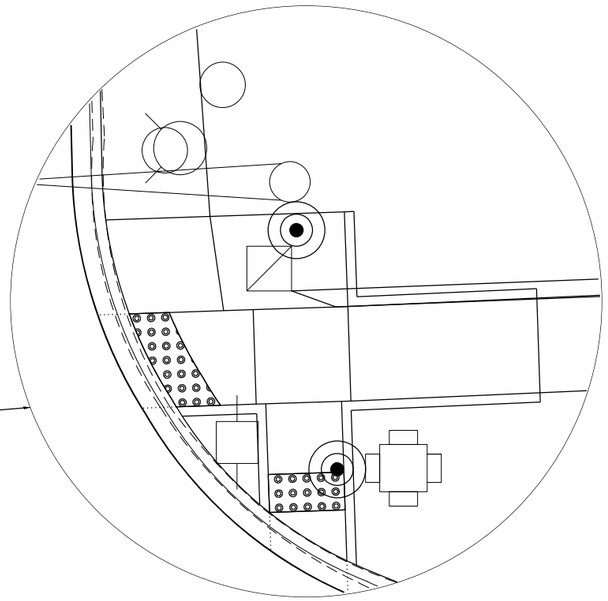
DISTRICT ONE
MAST ARM MOUNTED STREET NAME SIGNS

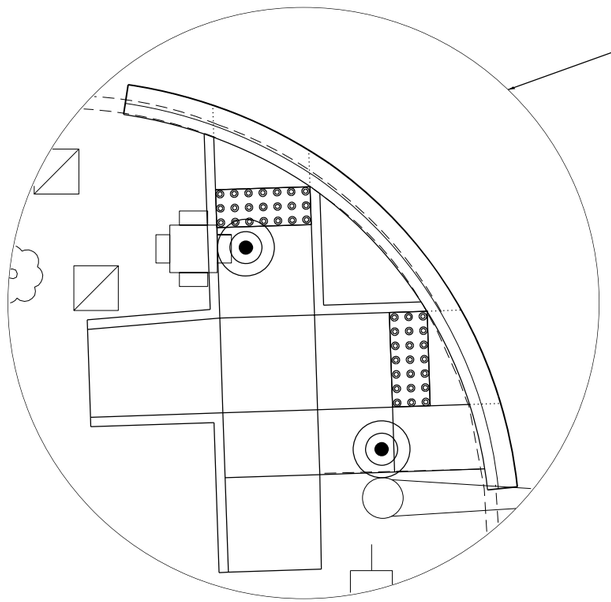
SCALE: | SHEET OF SHEETS | STA. | TO STA.

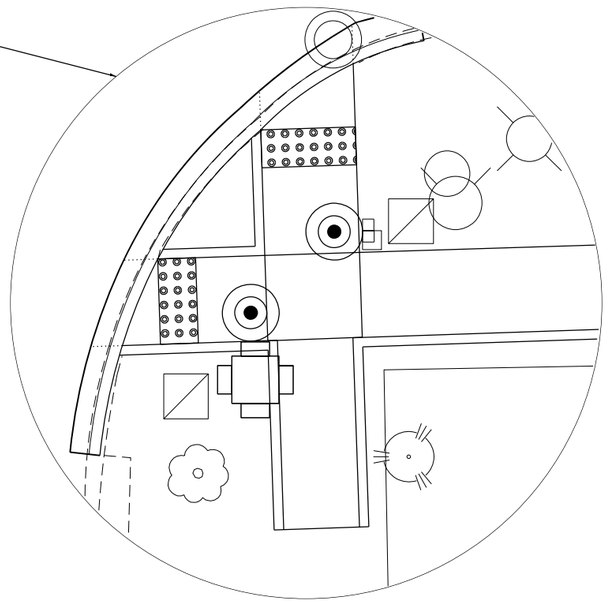
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	32
	TS-02	CONTRACT NO. 62T87		
	ILLINOIS FED. AID PROJECT			











THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVAL EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

4 EACH PEDESTRIAN PUSH-BUTTON

ALL EXISTING PEDESTRIAN PUSH-BUTTONS SHALL BE REMOVED.

CONTRACTOR SHALL PERFORM SIGNAL WORK IN SUCH A WAY AS TO MINIMIZE THE TIME THE EXISTING PEDESTRIAN EQUIPMENT IS INOPERATIONAL.

CONTRACTOR SHALL CONFIRM THE FINAL LOCATION OF THE PEDESTRIAN EQUIPMENT BEFORE INSTALLATION WITH THE TRAFFIC SIGNAL AREA ENGINEER BEFORE INSTALLATION.

DIMENSIONS SHOWN ARE FROM CENTER OF POST TO BACK OF CURB AND ARE APPROXIMATE AND PLACEMENT OF THE POST SHALL BE VERIFIED WITH THE ENGINEER PRIOR TO THE START OF WORK

APS SHALL BE PLACED PARALLEL TO THE CORRESPONDING CROSS WALK.

STATIONING SHOWN ON THE TRAFFIC SIGNAL PLAN ARE FROM RECORD DRAWINGS AND MAY NOT MATCH OTHER PLANS.

WHERE TRAFFIC SIGNAL EQUIPMENT WAS REMOVED AND THE EXISTING CABLE IS NOT BEING REUSED, THE EXISTING ELECTRICAL CABLE SHALL BE REMOVED FROM CONDUIT.

THE CONTRACTOR SHALL UTILIZE THE EXISTING UNIDUCT FOR THE PROPOSED LOOP DETECTORS.

PUSH-BUTTON EXTENSIONS ARE REQUIRED WHERE PEDESTRIAN PUSH-BUTTON LOCATIONS/INSTALLATIONS DO NOT MEET THE REQUIREMENTS OF THE MUTCD OR THE INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILD AND FACILITIES". THE PUSH-BUTTON EXTENSIONS SHALL BE INCLUDED IN THE COST OF THE ACCESSIBLE PEDESTRIAN SIGNALS.

USER: mcormicr
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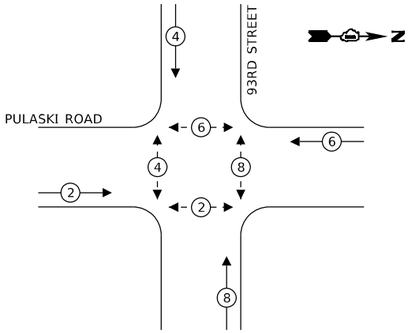
USER NAME = $USERNAME$	DESIGNED - Designed By	REVISED - $REV1DATE$ Rev1 By	**STATE OF ILLINOIS**	**TRAFFIC SIGNAL PLAN**	F.A.P RTE. / SECTION / COUNTY / TOTAL SHEETS / SHEET NO.
PEN TABLE = $PENTBL$	DRAWN - Drawn By	REVISED - $REV2DATE$ Rev2 By	**DEPARTMENT OF TRANSPORTATION**	**PULASKI ROAD AT 93RD STREET**	368 / FAP 0368 22 RS / COOK / 53 / 33
PLOT SCALE = 40.0000 ' / in.	CHECKED - Checked By	REVISED - $REV3DATE$ Rev3 By		SCALE: 40.0000 ' / in. SHEET Set Sheet OF Set TOTAL SHEETS STA. Station From TO STA. Station To	CONTRACT NO. 62T87
PLOT DATE = 10/20/2023 12:14:10 PM	DATE - $DATE-SUBMIT$	REVISED - $REV4DATE$ Rev4 By			ILLINOIS / FED. AID PROJECT

Long Section Number — Multiple County Names

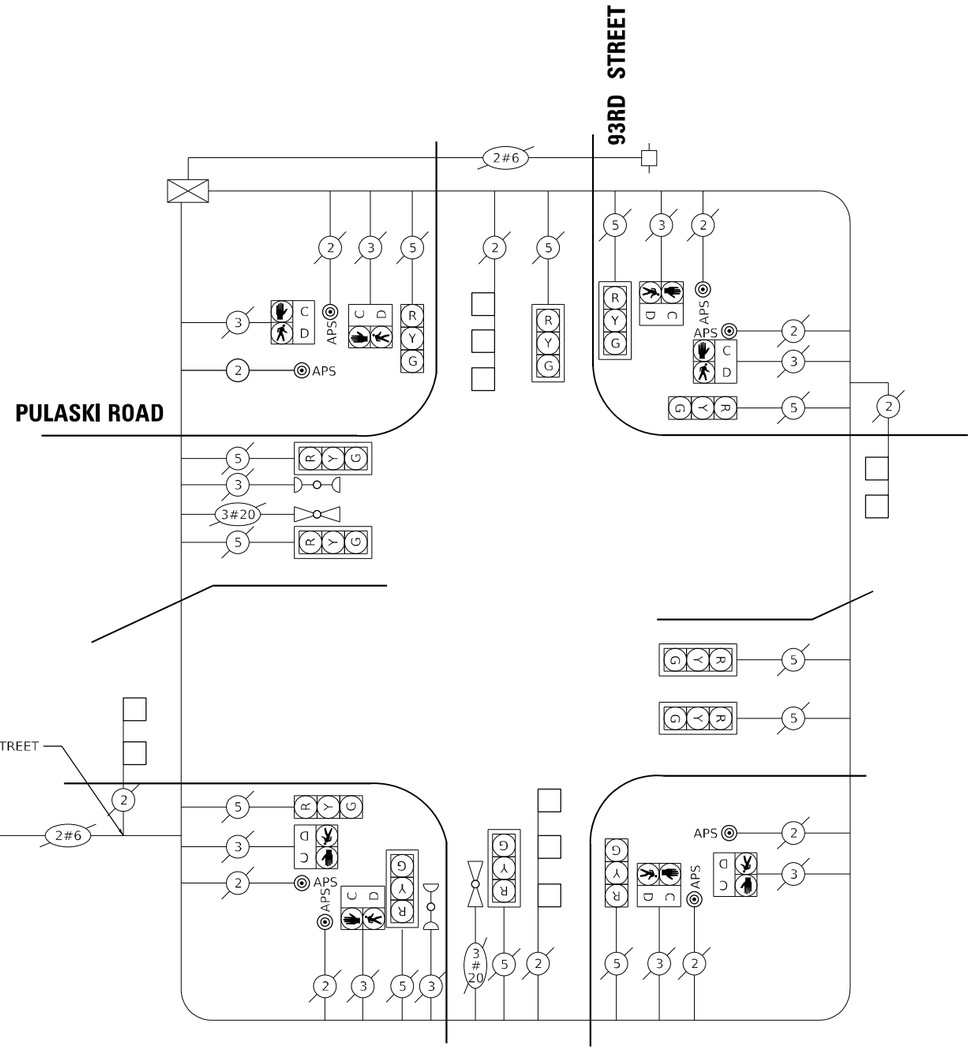
EXISTING CONTROLLER SEQUENCE



EXISTING PHASE DESIGNATION DIAGRAM

LEGEND:

- PROTECTED PHASE
- PROTECTED/PERMITTED PHASE
- PEDESTRIAN PHASE
- OL — OVERLAP



CABLE PLAN
(NOT TO SCALE)

ITEM DESCRIPTION	UNIT	TOTAL QTY
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	20
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	30
DETECTOR LOOP, TYPE I	FOOT	483
MODIFY EXISTING CONTROLLER CABINET	EACH	1
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
PEDESTRIAN SIGNAL POST, 5 FT.	EACH	4
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	8
CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	FOOT	4
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1
DRILL EXISTING HAND HOLE	EACH	1

TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

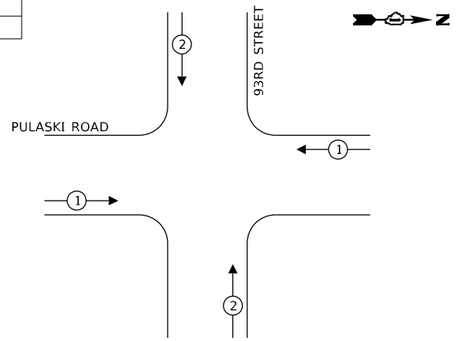
TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	12	11	50	66
(YELLOW)	12	20	5	12
(GREEN)	12	12	45	64.6
PERMISSIVE ARROW	-	10	10	-
PED. SIGNAL	8	20	100	80.0
CONTROLLER	1	100	100	100.0
UPS	-	25	100	-
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	-	-	-
			TOTAL =	322.6

ENERGY COSTS TO:

VILLAGE OF EVERGREEN PARK
9418 S. KEDZIE AVE
EVERGREEN PARK, IL 60805

ENERGY SUPPLY: CONTACT: NEW BUSINESS DEPARTMENT
PHONE: 866-639-3532
COMPANY: COMMONWEALTH EDISON
ACCOUNT NUMBER: ---

EXISTING EMERGENCY VEHICLE PREEMPTION SEQUENCE



EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	1	2
MOVEMENT	⟶ ⟵	↓ ↓

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CABLE PLAN AND PHASE DESIGNATION DIAGRAM
PULASKI ROAD AT 93RD STREET

SCALE: 40.0000 ' / in. SHEET Set Sheet OF Set Total SHEETS STA. Station From TO STA. Station To

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	34
		CONTRACT NO. 62T87		
		ILLINOIS	FED. AID PROJECT	

NOTE:

1. APS SHALL BE PLACED PARALLEL TO THE CORRESPONDING CROSS WALK.
2. STATIONING SHOWN ON THE TRAFFIC SIGNAL PLAN ARE FROM RECORD DRAWINGS AND MAY NOT MATCH OTHER PLANS.
3. WHERE TRAFFIC SIGNAL EQUIPMENT WAS REMOVED AND THE EXISTING CABLE IS NOT BEING REUSED, THE EXISTING ELECTRICAL CABLE SHALL BE REMOVED FROM CONDUIT.
4. THE CONTRACTOR SHALL UTILIZE THE EXISTING UNIDUCT FOR THE PROPOSED LOOP DETECTORS.
5. PUSH-BUTTON EXTENSIONS ARE REQUIRED WHERE PEDESTRIAN PUSH-BUTTON LOCATIONS/INSTALLATIONS DO NOT MEET THE REQUIREMENTS OF THE MUTCD OR THE INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILD AND FACILITIES". THE PUSH-BUTTON EXTENSIONS SHALL BE INCLUDED IN THE COST OF THE ACCESSIBLE PEDESTRIAN SIGNALS.
6. THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVAL EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

   4 EACH PEDESTRIAN PUSH BUTTON
7. ALL EXISTING PEDESTRIAN PUSH-BUTTONS SHALL BE REMOVED.
8. CONTRACTOR SHALL PERFORM SIGNAL WORK IN SUCH A WAY AS TO MINIMIZE THE TIME THE EXISTING PEDESTRIAN EQUIPMENT IS INOPERATIONAL.
9. CONTRACTOR SHALL CONFIRM THE FINAL LOCATION OF THE PEDESTRIAN EQUIPMENT BEFORE INSTALLATION WITH THE TRAFFIC SIGNAL AREA ENGINEER BEFORE INSTALLATION.
10. DIMENSIONS SHOWN ARE FROM CENTER OF POST TO BACK OF CURB AND ARE APPROXIMATE AND PLACEMENT OF THE POST SHALL BE VERIFIED WITH THE ENGINEER PRIOR TO THE START OF WORK.

N

0 20 40 60
SCALE IN FEET

PULASKI ROAD

95TH STREET

REMOVE EXISTING PUSHBUTTON (1)

SAMPLING LOOPS

APS

DSGN ENGINEER = $PRELIMNAME$	DESIGNED - Designed By	REVISED - $REV1DATE$	Rev1 By	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC SIGNAL PLAN PULASKI ROAD AT 95TH STREET	F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PEN TABLE = $PENTBL$	DRAWN - Drawn By	REVISED - $REV2DATE$	Rev2 By			368	FAP 0368 22 RS	COOK	53	35
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Long Section Number Multiple County Names

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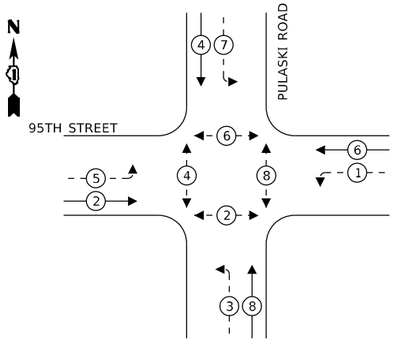
EXISTING CONTROLLER SEQUENCE



LEGEND:

PROTECTED PHASE

PEDESTRIAN PHASE

EXISTING PHASE DESIGNATION DIAGRAM

ITEM DESCRIPTION	UNIT	TOTAL QTY
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	765.5
DETECTOR LOOP, TYPE I	FOOT	226
MODIFY EXISTING CONTROLLER CABINET	EACH	1
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	8
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1

TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

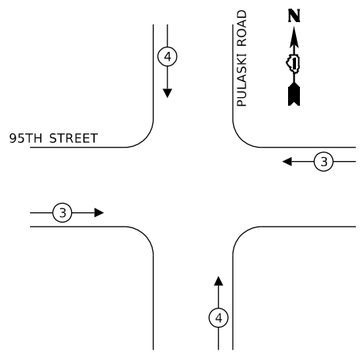
TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	14	11	50	77
(YELLOW)	14	20	5	14
(GREEN)	14	12	45	75.6
PERMISSIVE ARROW	16	10	10	160.0
PED. SIGNAL	8	20	100	80.0
CONTROLLER	1	100	100	100.0
UPS	-	25	100	-
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	-	-	-
			TOTAL =	506.6

ENERGY COSTS TO:

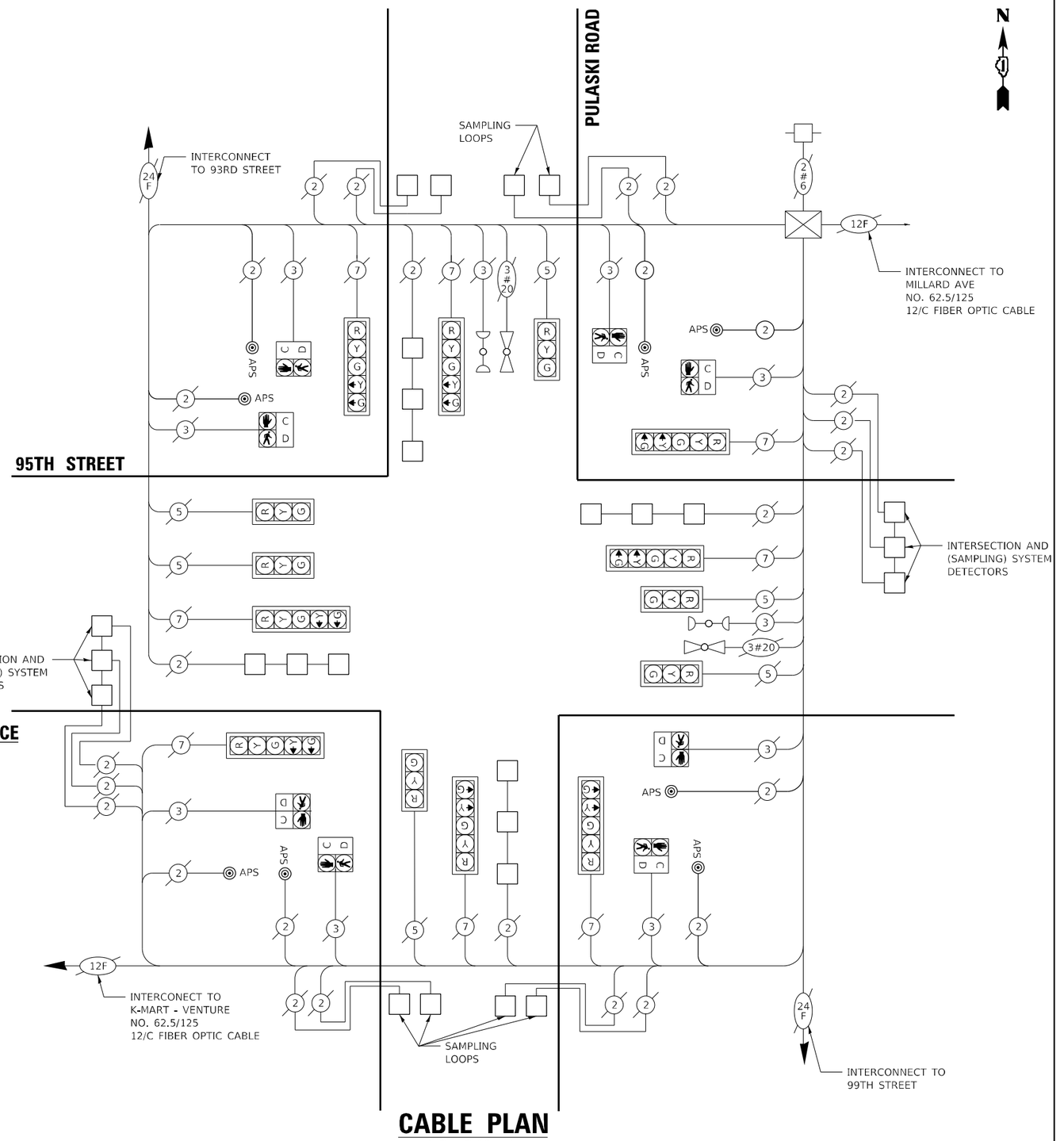
VILLAGE OF OAK LAWN
9446 RAYMOND AVE #1
OAK LAWN, IL 60453

ENERGY SUPPLY: CONTACT: NEW BUSINESS DEPTARTMENT
PHONE: 866-639-3532
COMPANY: COMMONWEALTH EDISON
ACCOUNT NUMBER: ---

EXISTING EMERGENCY PREEMPTIVE SEQUENCE



EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	→ ←	↓ ↑



CABLE PLAN

(NOT TO SCALE)

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USER NAME = $USERNAME$	DESIGNED - Designed By	REVISED - $REV1DATE$ Rev1 By	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CABLE PLAN AND PHASE DESIGNATION DIAGRAM 95TH STREET AT PULASKI ROAD
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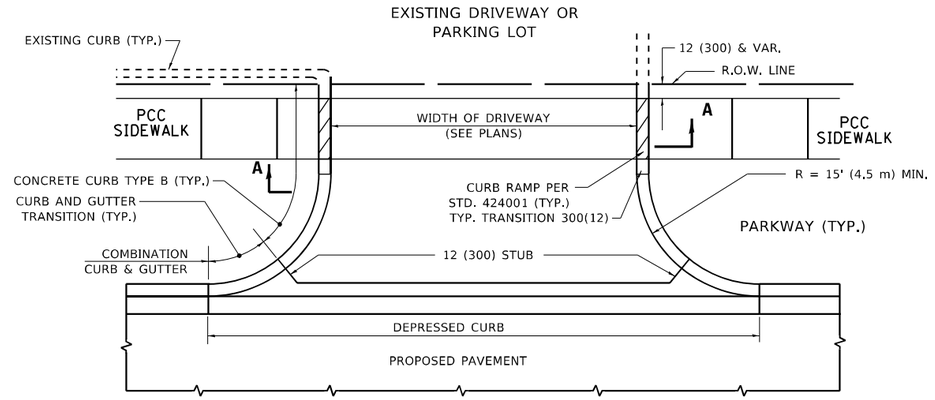
PLOT DATE = 10/20/2023 12:14:25 PM	DATE - $DATE-SUBMIT$	REVISED - $REV4DATE$ Rev4 By		

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	36
		CONTRACT NO. 62T87		
		ILLINOIS	FED. AID PROJECT	

Long Section Number — Multiple County Names



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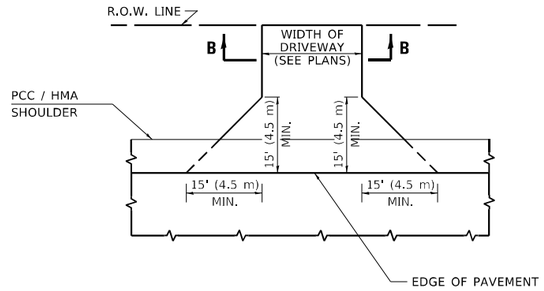
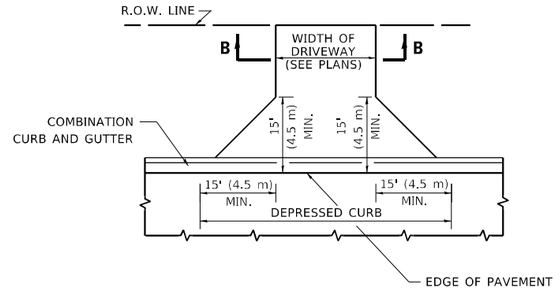
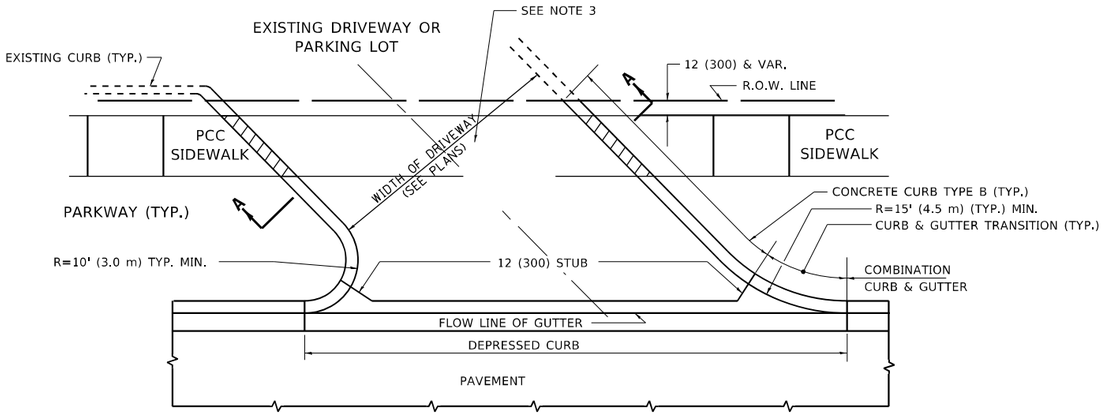


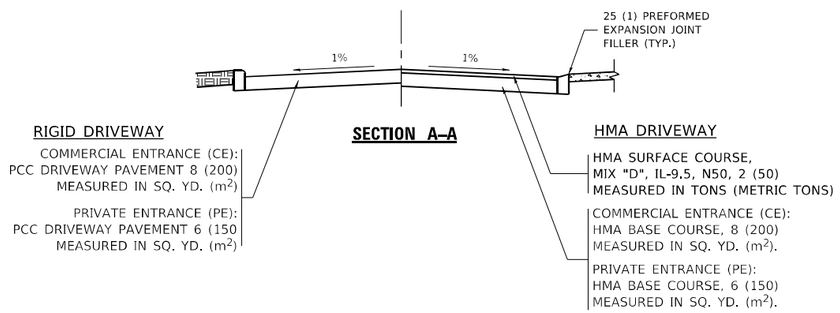
ADJACENT TO PCC /HMA SHOULDER

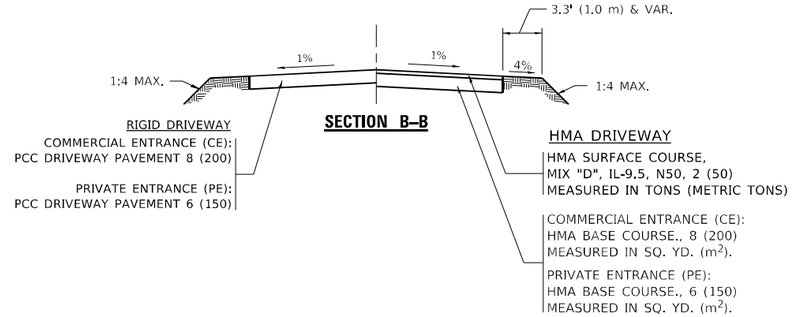


ADJACENT TO CURB AND GUTTER



WITH CONCRETE CURB, TYPE B





GENERAL NOTES

1. DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

2. COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

USER NAME = AYA.Elkhatib	DESIGNED - R. SHAH	REVISED - R. BORO 06-11-08	
	DRAWN -	REVISED - R. BORO 09-06-11	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - K. SMITH 08-28-19	
PLOT DATE = 10/23/2023	DATE - 11-04-95	REVISED - K. SMITH 11-18-22	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS – DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER ≥15'(4.5m)

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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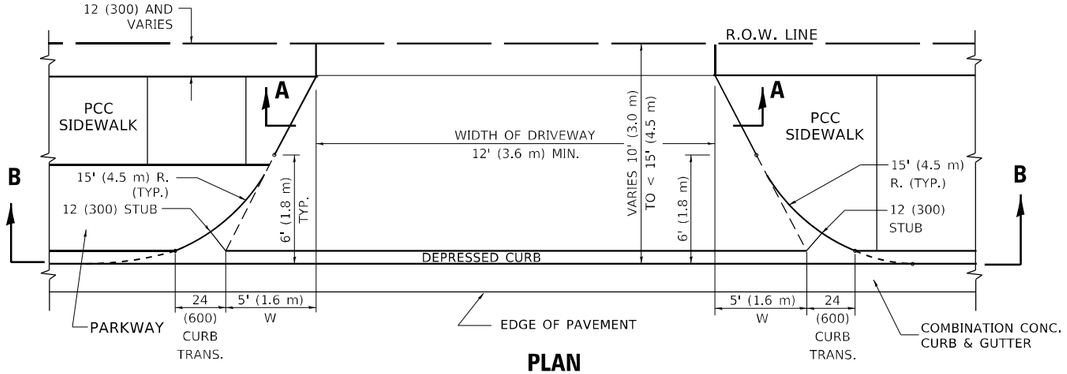
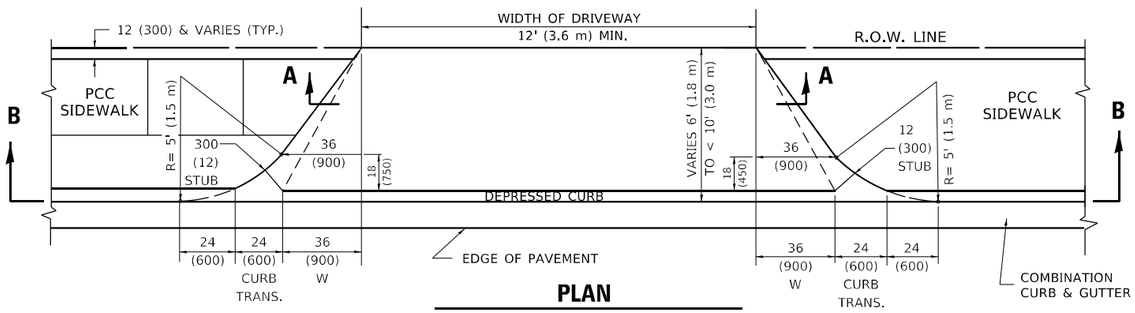
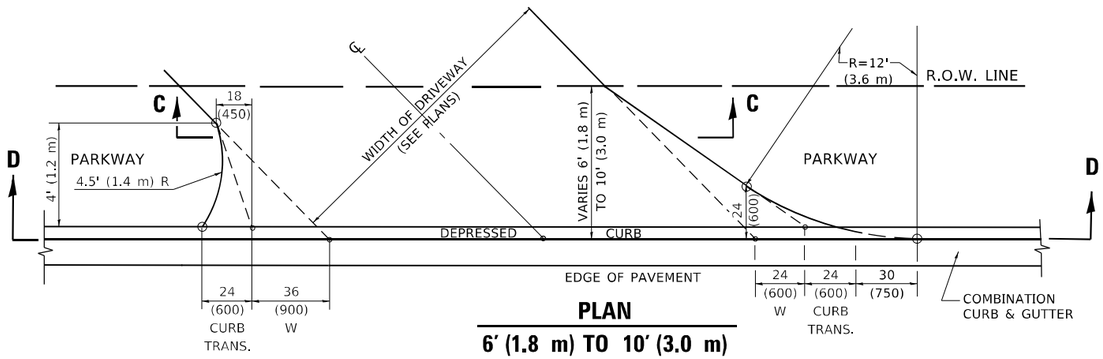
	BD400-01 (BD-01)	CONTRACT NO. 62T87		
	ILLINOIS	FED. AID PROJECT		



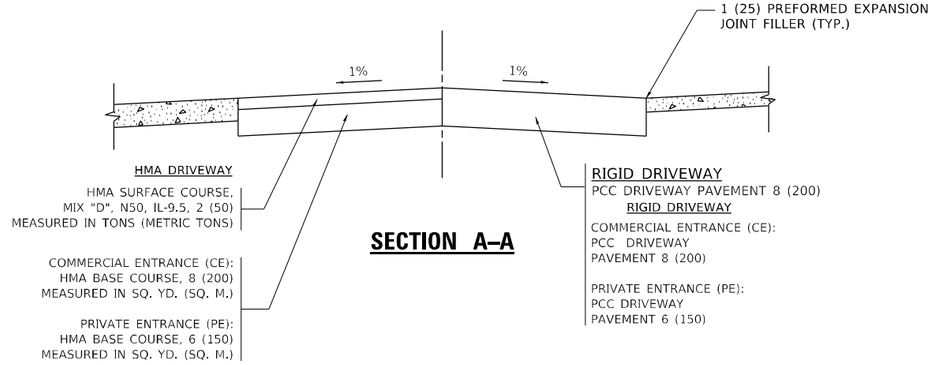
PLAN
10' (3.0 m) TO < 15' (4.5 m)



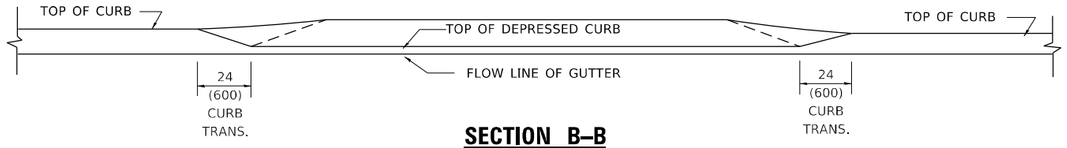
PLAN
6' (1.8 m) TO < 10' (3.0 m)



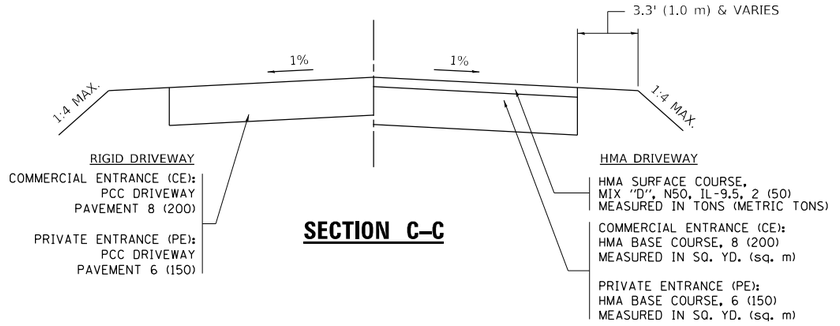
PLAN
6' (1.8 m) TO 10' (3.0 m)



SECTION A–A



SECTION B–B



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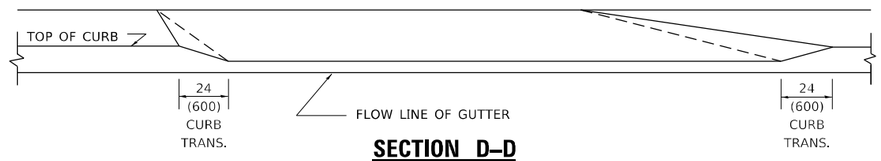

SECTION D–D

GENERAL NOTES

1. DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.
2. WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE PCC SIDEWALK SHALL EXTEND TO THE BACK OF CURB.
3. "W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

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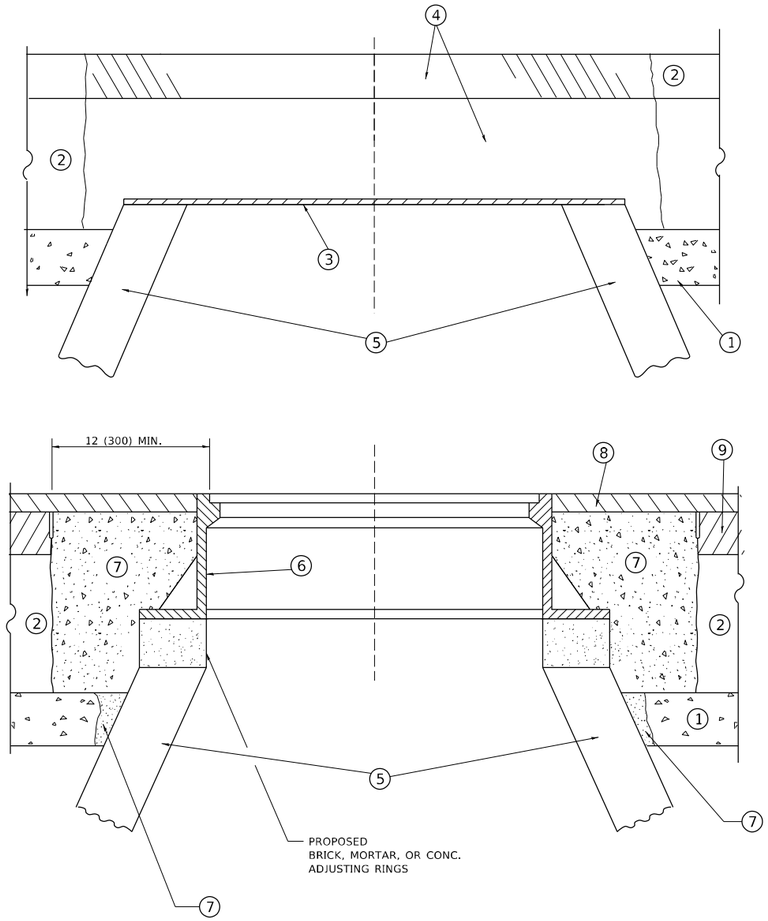
USER NAME = AYA.ELkhatib	DESIGNED - R. SHAH	REVISED - R. BORO 01-01-07	
	DRAWN -	REVISED - R. BORO 09-06-11	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - K. SMITH 08-27-19	
PLOT DATE = 10/23/2023	DATE - 11-06-95	REVISED - K. SMITH 11-18-22	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5m)

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	38
	BD400–02 (BD–02)	CONTRACT NO. 62T87		
	ILLINOIS FED. AID PROJECT			



DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

NOTES

1. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

2. IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

3. CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

4. THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

5. THE CONTRACTOR SHALL REMOVE ALL TRAFFIC CONTROL DEVICES BY THE END OF EACH WORK SHIFT.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.

B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.

C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.

D) BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 3 (80) HMA TO REMAIN AFTER MILLING).

STAGE 2 (AFTER PAVEMENT MILLING)

A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.

B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.

C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-2* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

1. SUB-BASE GRANULAR MATERIAL
2. EXISTING PAVEMENT
3. 36 (900) DIAMETER METAL PLATE
4. PROPOSED CRUSHED STONE AND HMA SURFACE MIX
5. EXISTING STRUCTURE
6. FRAME AND LID (SEE NOTES)
7. CLASS PP-2* CONCRETE
8. PROPOSED HMA SURFACE COURSE
9. PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

1. REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

2. THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

3. NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

4. WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

MODEL: Default
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USER NAME = AYA.ELkhatib	DESIGNED - R. SHAH	REVISED - R. BORO 03-09-11	
	DRAWN -	REVISED - R. BORO 12-06-11	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - K. SMITH 11-18-22	
PLOT DATE = 10/23/2023	DATE - 10-25-94	REVISED - K. SMITH 09-15-23	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

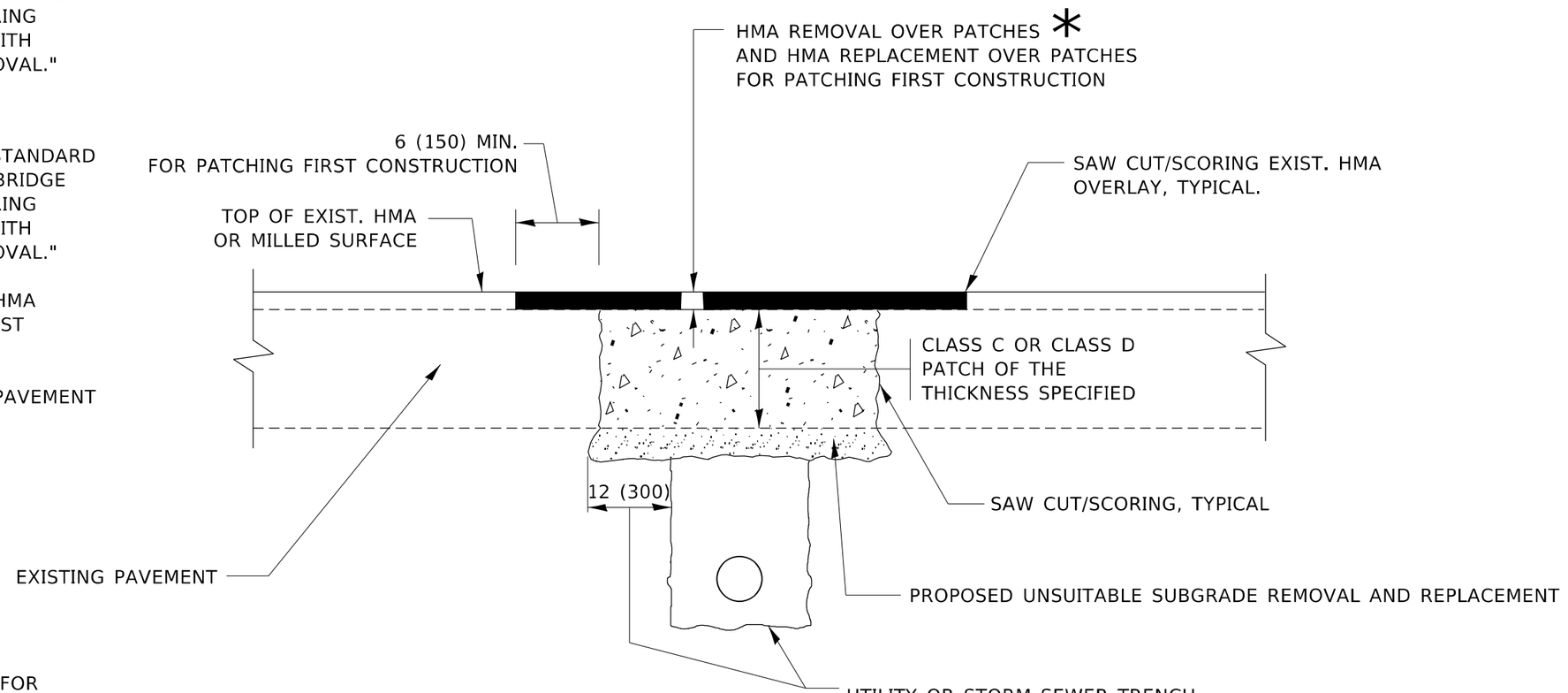
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	39
	BD600-03 (BD-08)	CONTRACT NO. 62T87		
	ILLINOIS FED. AID PROJECT			

METHOD OF MEASUREMENT

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

BASIS OF PAYMENT

1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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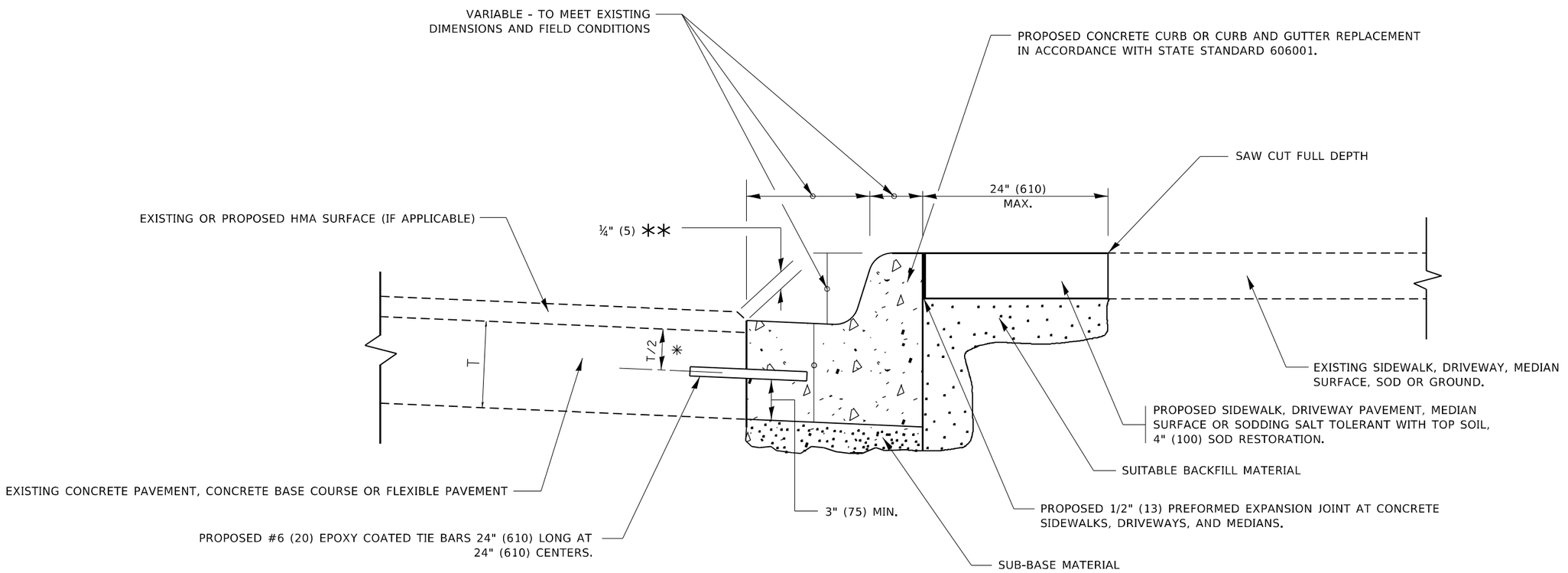
USER NAME = AYA.ELkhatib	DESIGNED - R. SHAH	REVISED - R. BORO 01-01-07	**STATE OF ILLINOIS** **DEPARTMENT OF TRANSPORTATION**	**PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT**	
	DRAWN -	REVISED - R. BORO 09-04-07			
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - K. ENG 10-27-08			
PLOT DATE = 10/23/2023	DATE - 10-25-94	REVISED - K. SMITH 11-18-22		SCALE: NONE	SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	40
	BD400-04 (BD-22)	CONTRACT NO. 62T87		
	ILLINOIS FED. AID PROJECT			



* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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USER NAME = AYA.Elkhatib	DESIGNED - A. HOUSEH	REVISED - A. ABBAS 03-21-97	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**	**CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT**	
	DRAWN -	REVISED - M. GOMEZ 01-22-01			
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - R. BORO 12-15-09		SCALE: NONE	SHEET 1 OF 1 SHEETS STA. TO STA.
PLOT DATE = 10/23/2023	DATE - 03-11-94	REVISED - K. SMITH 07-11-19			

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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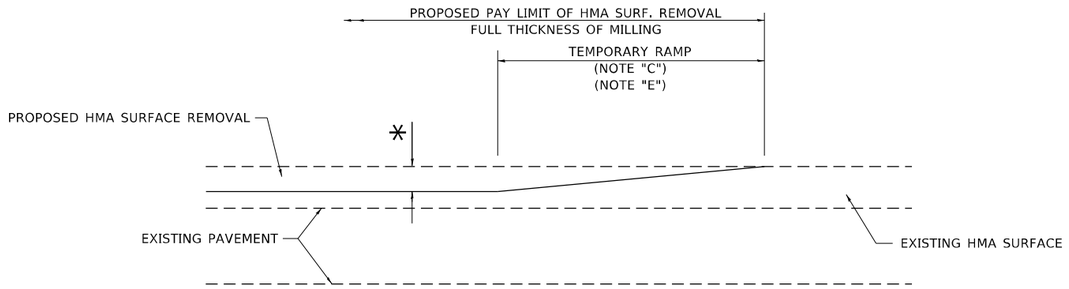
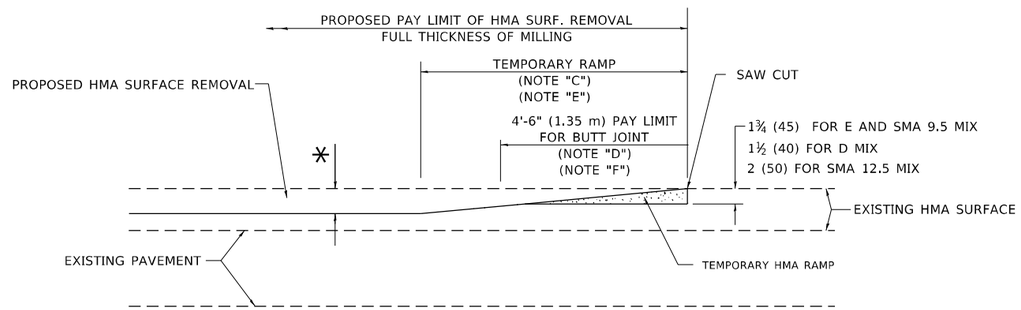
	BD600-06 (BD-24)	CONTRACT NO. 62T87		
	ILLINOIS FED. AID PROJECT			



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

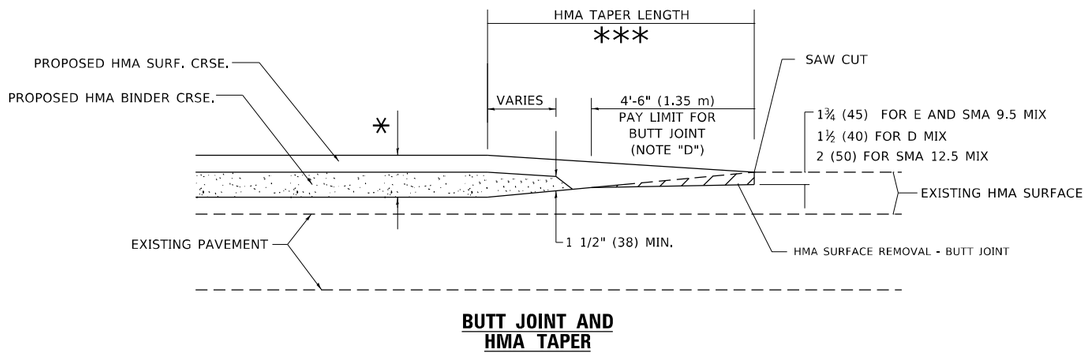


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

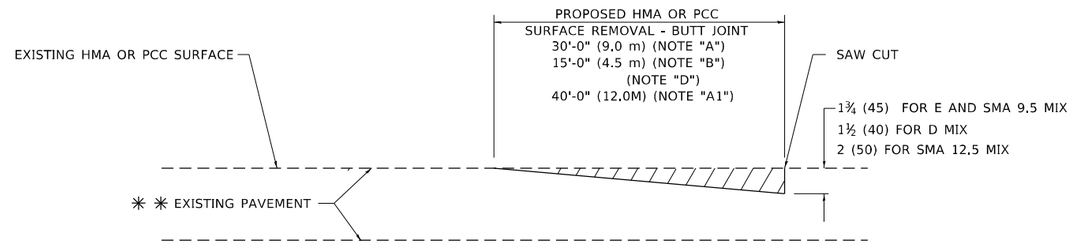
OPTION 2

TYPICAL TEMPORARY RAMP

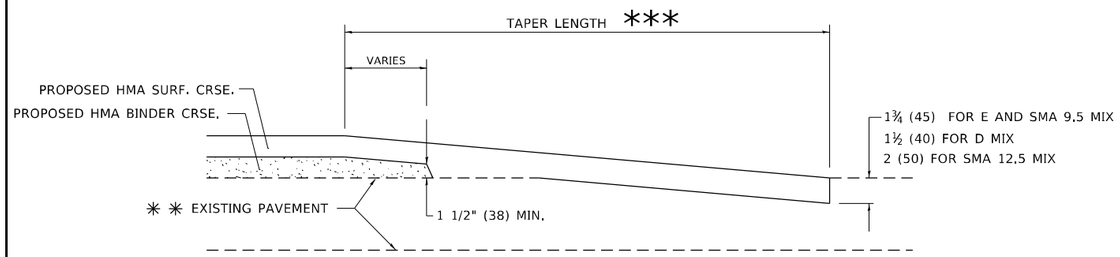


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

GENERAL NOTES

A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.

A1. INTERSTATES

B. MINOR SIDE ROADS.

C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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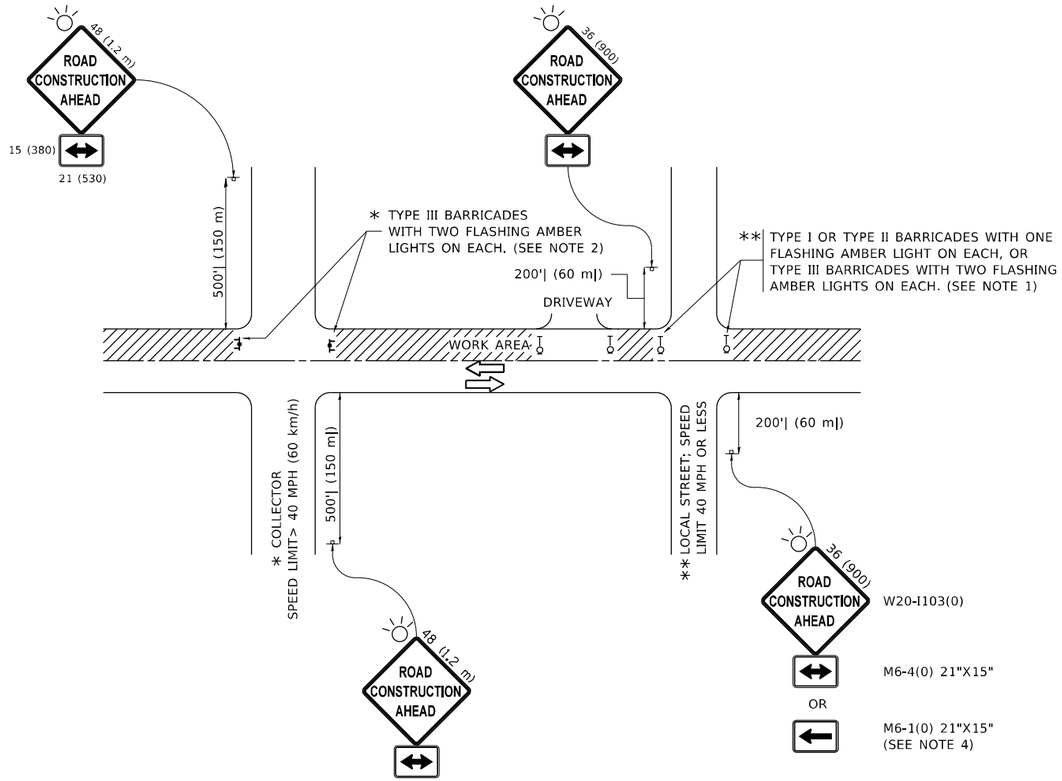
USER NAME = AYA.ELkhatib	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97	
	DRAWN -	REVISED - M. GOMEZ 04-06-01	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - R. BORO 01-01-07	
PLOT DATE = 10/23/2023	DATE - 06-13-90	REVISED - K. SMITH 11-18-22	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	42
	BD400-05 BD-32	CONTRACT NO. 62T87		
	ILLINOIS FED. AID PROJECT			



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = AYA.Elkhatib	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
	DRAWN -	REVISED - T. RAMMACHER 01-06-00
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
PLOT DATE = 10/23/2023	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	43
	TC-10	CONTRACT NO. 62T87		
	ILLINOIS FED. AID PROJECT			

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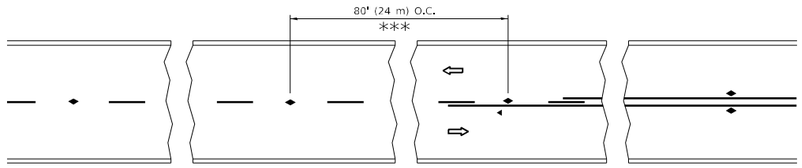
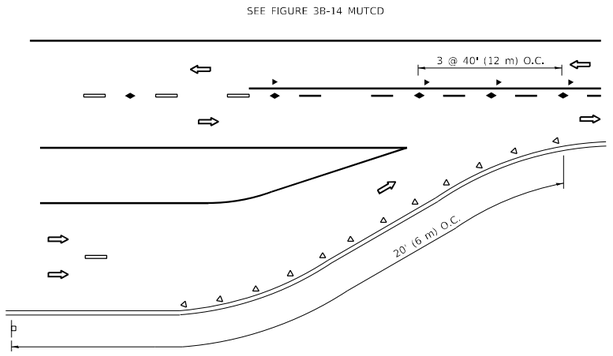


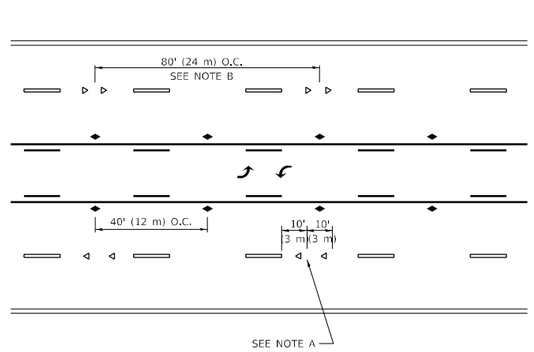
*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

TWO–LANE/TWO–WAY

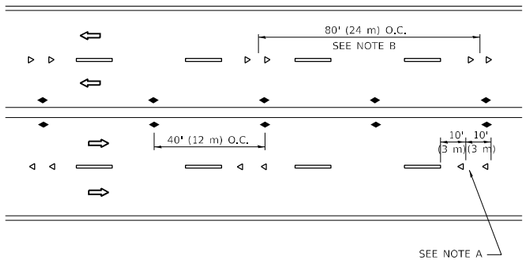
SEE FIGURE 3B-14 MUTCD



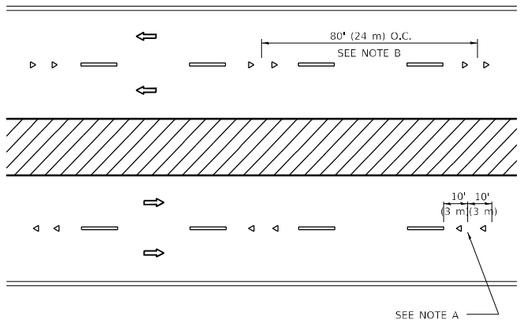
LANE REDUCTION TRANSITION



TWO–WAY LEFT TURN



MULTI–LANE/UNDIVIDED



MULTI–LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

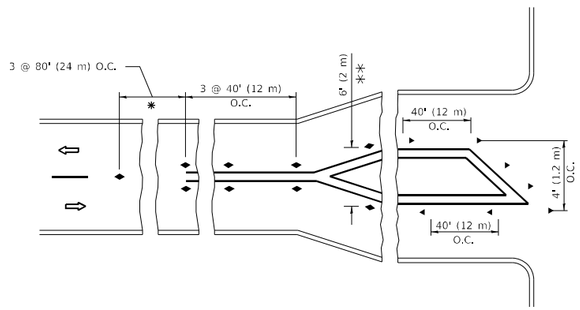
SYMBOLS

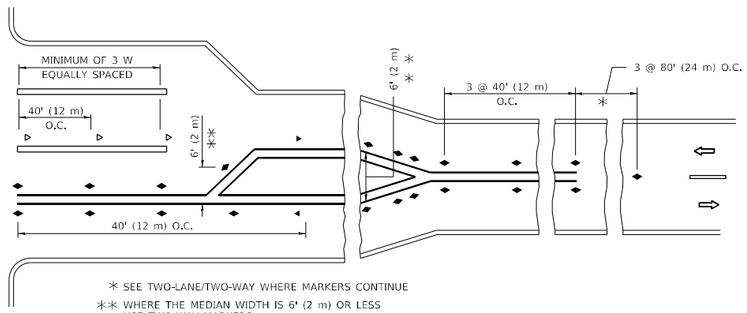
- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

LANE MARKER NOTES

A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.





* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE

** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

TURN LANES

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default
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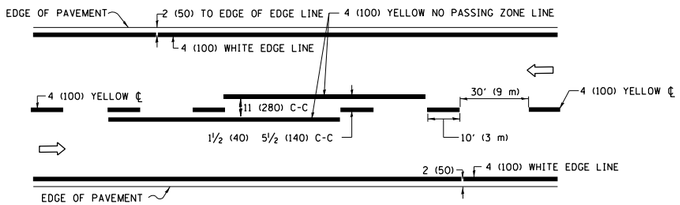
USER NAME = AYA.ELkhatib	DESIGNED -	REVISED - T. RAMMACHER 03-12-99	
	DRAWN -	REVISED - T. RAMMACHER 01-06-00	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - C. JUCIUS 09-09-09	
PLOT DATE = 10/23/2023	DATE -	REVISED - C. JUCIUS 07-01-13	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

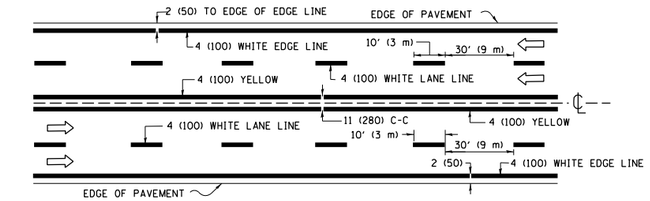
TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW–PLOW RESISTANT)

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

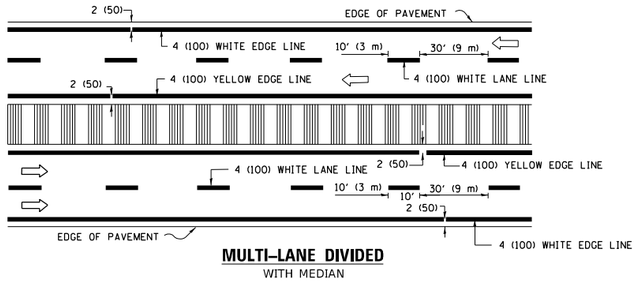
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	44
	TC-11	CONTRACT NO. 62T87		
	ILLINOIS	FED. AID PROJECT		



2–LANE ROADWAY

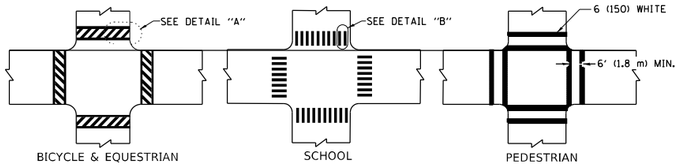


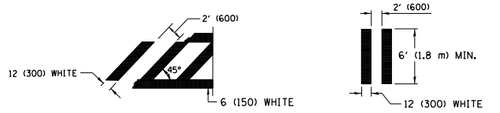
MULTI–LANE UNDIVIDED



MULTI–LANE DIVIDED
WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

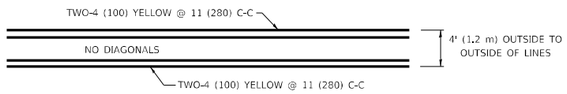




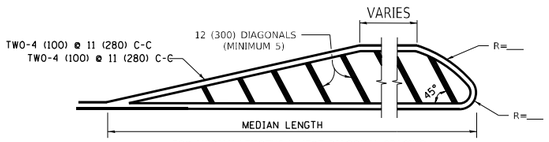
DETAIL "A" **DETAIL "B"**

TYPICAL CROSSWALK MARKING

✳ MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



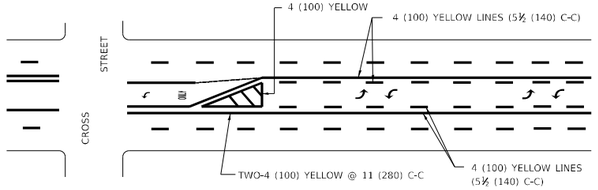
4' (1.2 m) WIDE MEDIANS ONLY



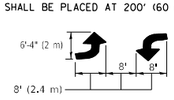
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

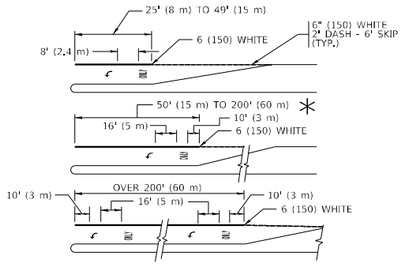


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO–WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

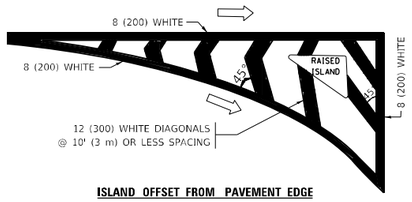


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) **ONLY** AREA = 20.8 SQ. FT. (1.9 m ²

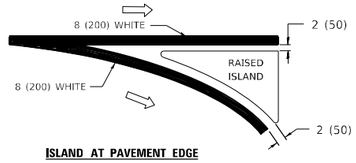
✳ TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

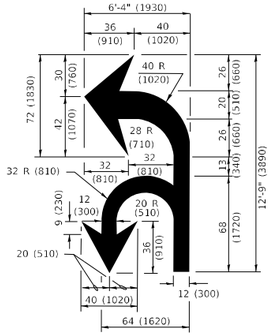


ISLAND OFFSET FROM PAVEMENT EDGE

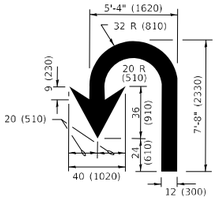


ISLAND AT PAVEMENT EDGE

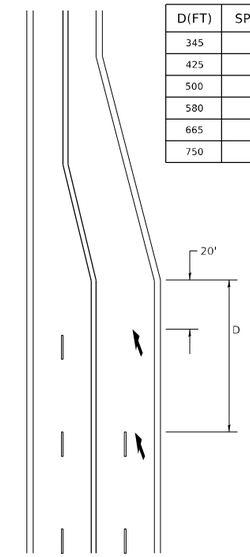
TYPICAL ISLAND MARKING



COMBINATION LEFT AND U–TURN



U–TURN



D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

LANE REDUCTION TRANSITION

✳ LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH: 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" IS 6' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ² EACH "X"=54.0 SQ. FT. (5.0 m ²
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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USER NAME = AYA.ELkhatib	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09	
	DRAWN -	REVISED - C. JUCIUS 07-01-13	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - C. JUCIUS 12-21-15	
PLOT DATE = 10/23/2023	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE | SHEET 1 OF 2 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	45
	TC–13	CONTRACT NO. 62T87		
	ILLINOIS FED. AID PROJECT			

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

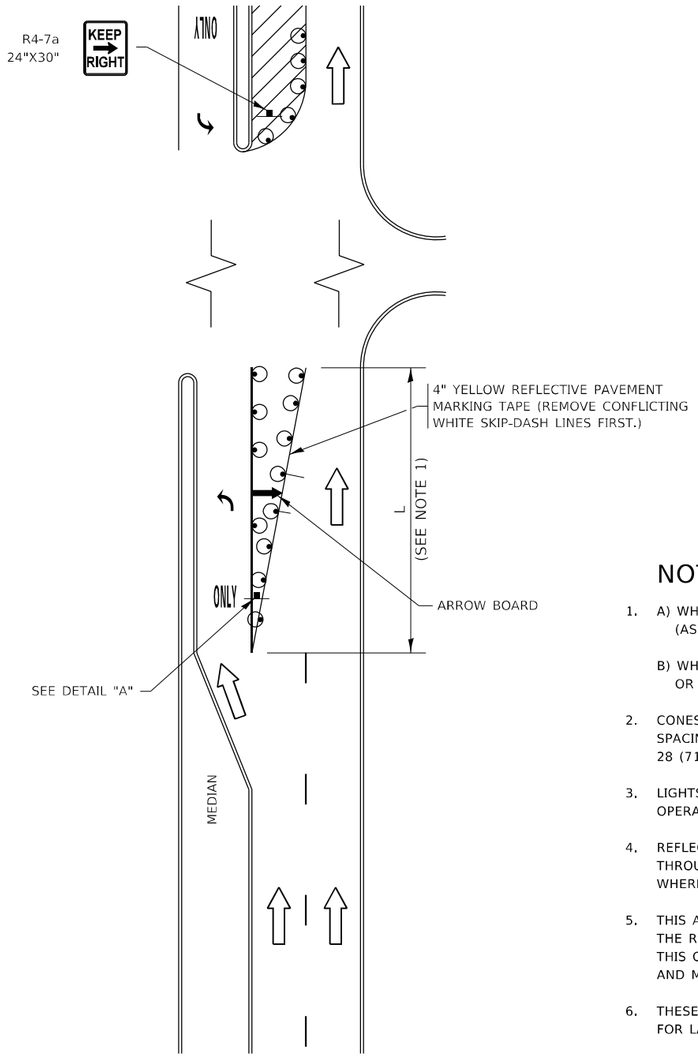


FIGURE 1

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

1. A) WHEN "L" IS ≤ THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.

   B) WHEN "L" IS > THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.

2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.

3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.

4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.

5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.

6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.

7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.

8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

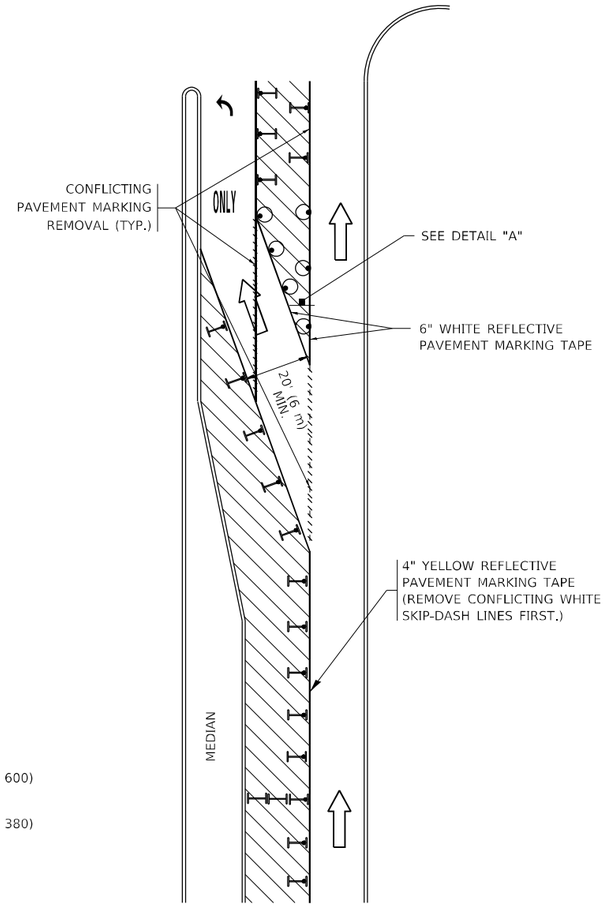
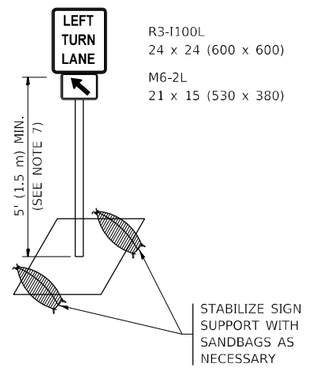


FIGURE 2



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

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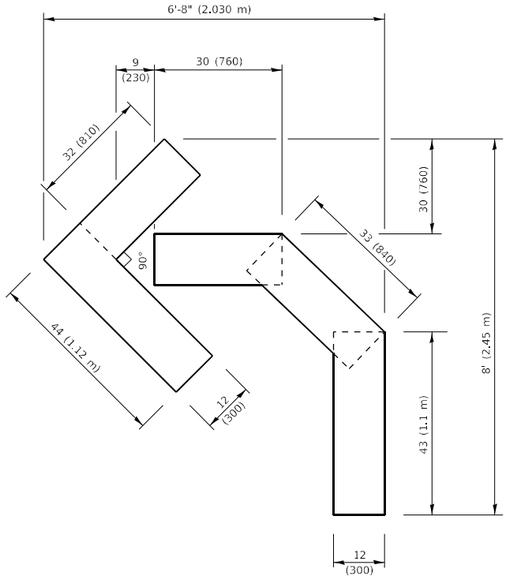
	DRAWN - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13	
PLOT SCALE = 100.0000 ' / in.	CHECKED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16	
PLOT DATE = 10/23/2023	DATE - T. RAMMACHER 01-06-00	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)

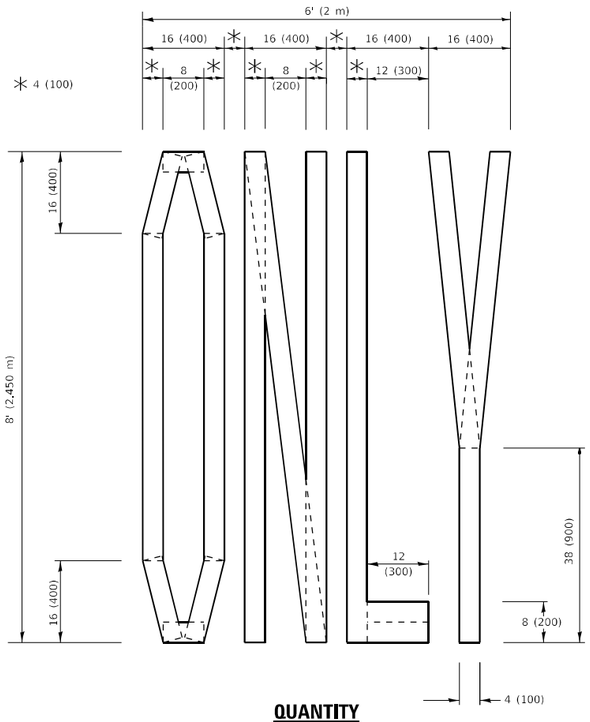
SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	46
	TC-14	CONTRACT NO. 62T87		
	ILLINOIS	FED. AID PROJECT		



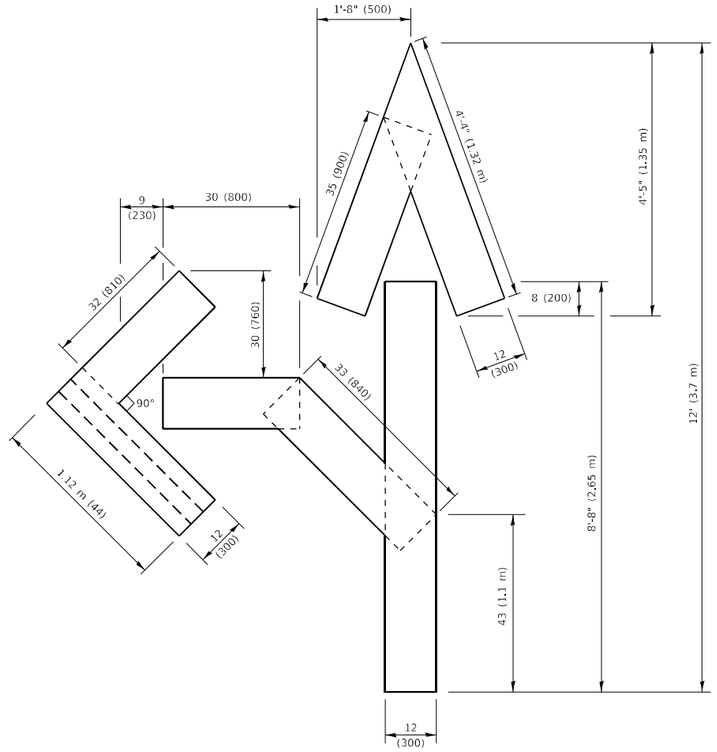
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

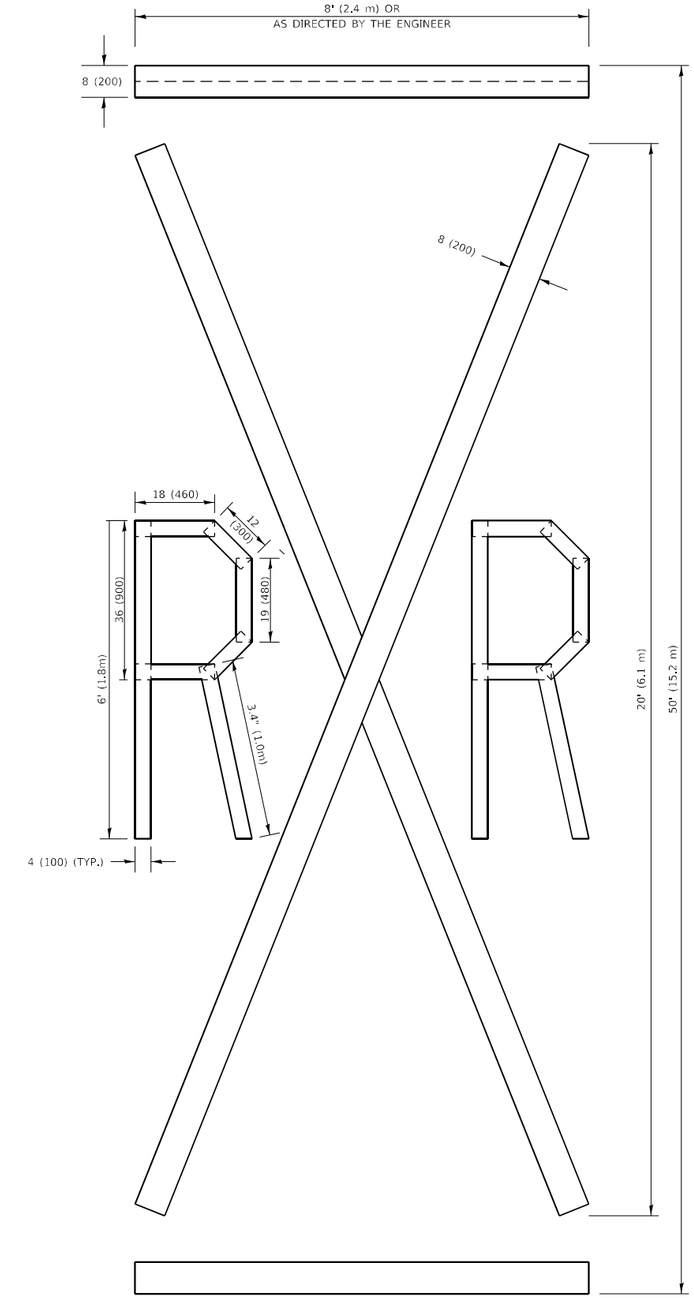


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = AYA.ELkhatib	DESIGNED -	REVISED - T. RAMMACHER 03-02-98	
	DRAWN -	REVISED - E. GOMEZ 08-28-00	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - E. GOMEZ 08-28-00	
PLOT DATE = 10/23/2023	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16	

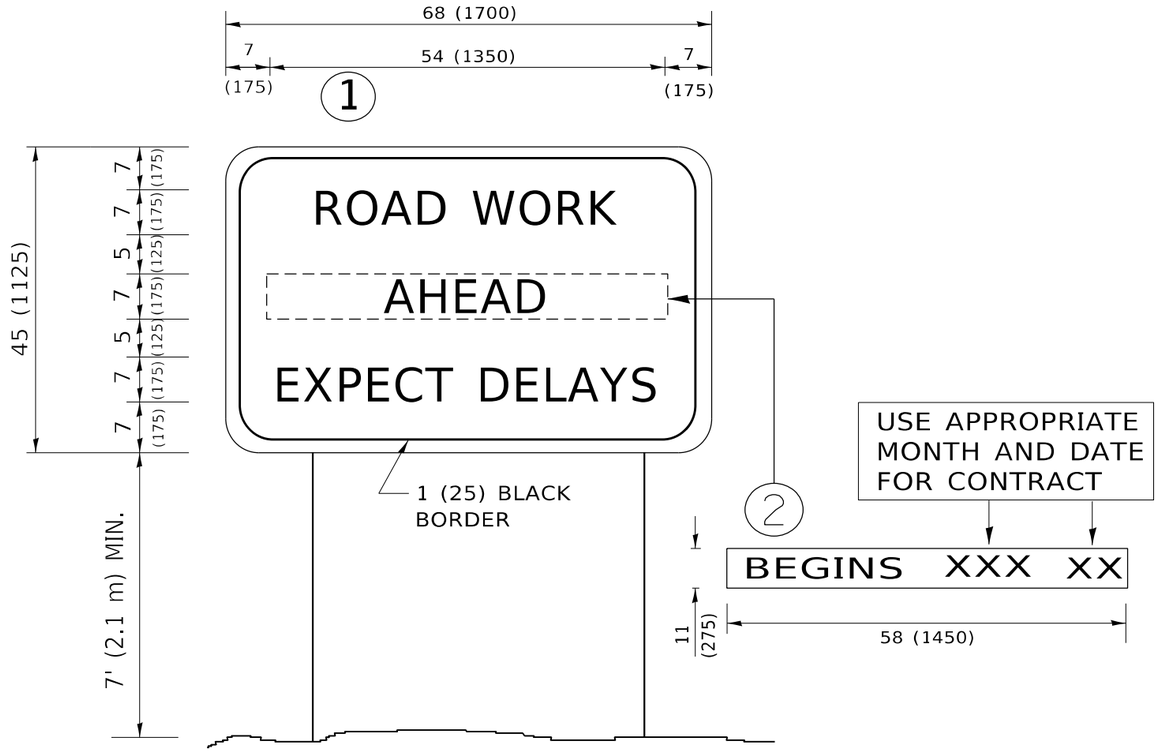
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	47
	TC-16	CONTRACT NO. 62T87		
	ILLINOIS FED. AID PROJECT			

MODEL: Default
FILE NAME: pw:\\dot-pw.bentley.com:PWDOTDocs.ments\\DOT Offices\District 1\Projects\126822\CADData\Design\DistStd.dgn



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

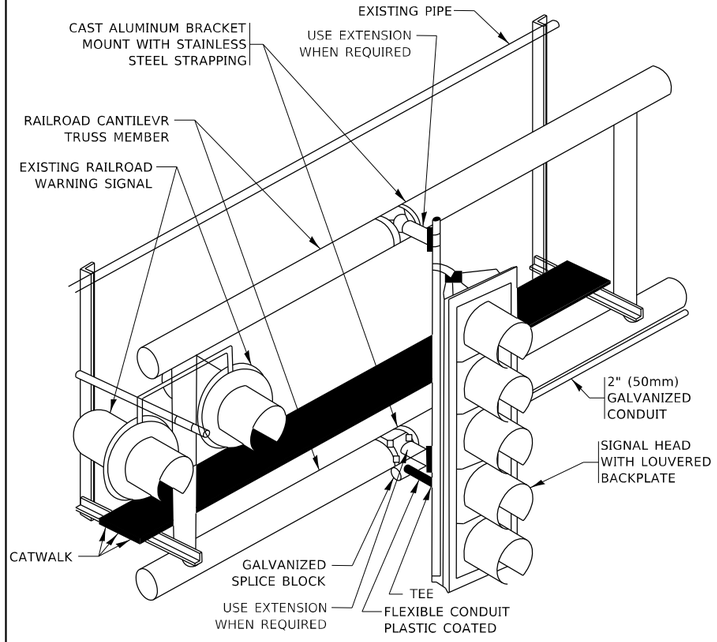
USER NAME = AYA.Elkhatib	DESIGNED -	REVISED -	R. MIRS 09-15-97
	DRAWN -	REVISED -	R. MIRS 12-11-97
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	T. RAMMACHER 02-02-99
PLOT DATE = 10/23/2023	DATE -	REVISED -	C. JUCIUS 01-31-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD INFORMATION SIGN

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	48
	TC-22	CONTRACT NO. 62T87		
	ILLINOIS FED. AID PROJECT			



RAILROAD CANTILEVER SIGNAL HEAD MOUNTING

USE NON-CONDUCTIVE SPACERS BETWEEN THE TRAFFIC SIGNAL EQUIPMENT AND THE RAILROAD CANTILEVER TO PREVENT DISSIMILAR METAL CORROSION

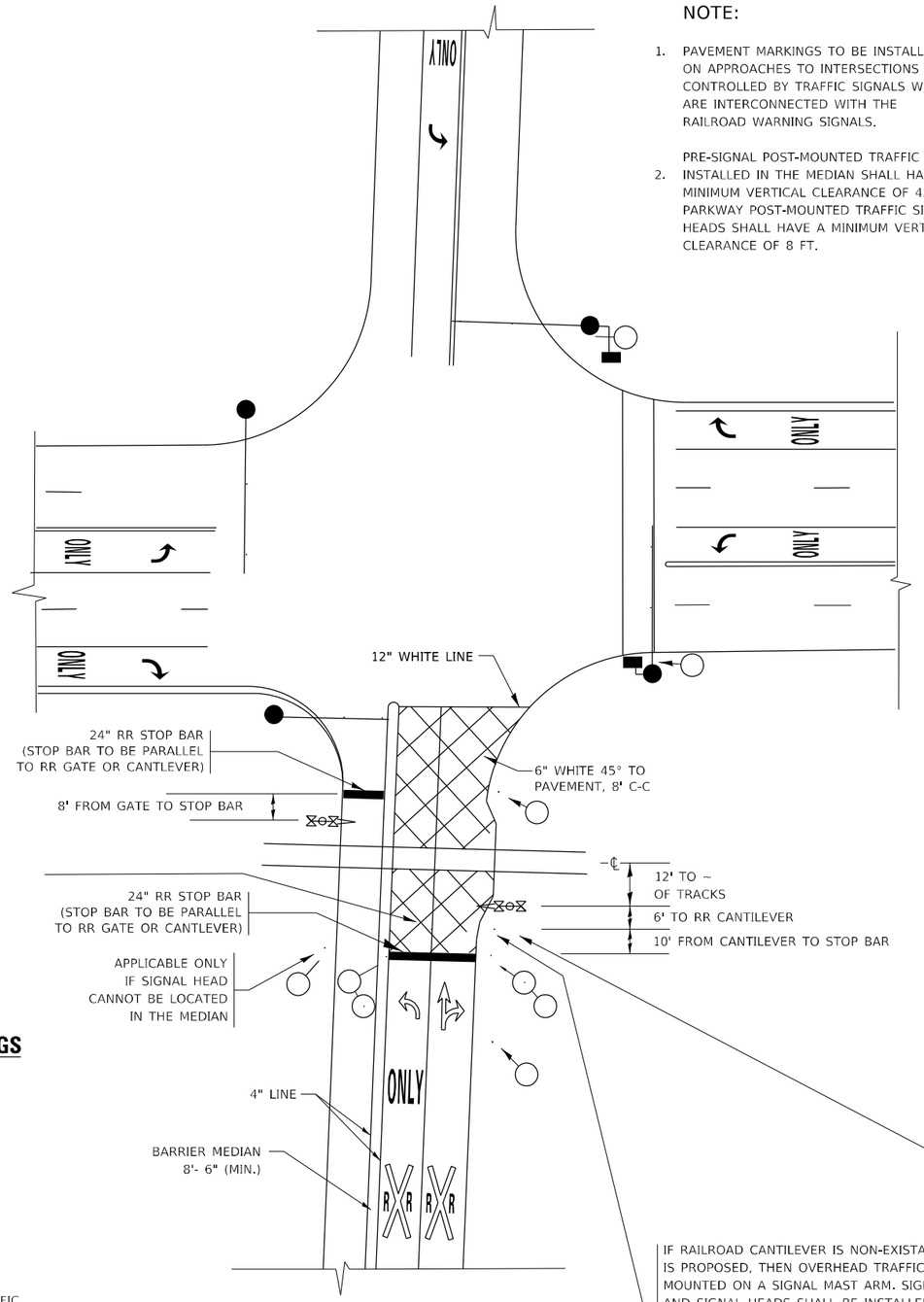
N.T.S.

SIGNING AND PAVEMENT MARKING AT RAILROAD CROSSINGS

SIGNING AND PAVEMENT MARKING TRAFFIC CONTROL STANDARD (TC-23) HAS BEEN DEVELOPED IN CONSULTATION WITH THE ILLINOIS COMMERCE COMMISSION AND THE U.S. DEPARTMENT OF TRANSPORTATION'S GRADE CROSSING SAFETY TASK FORCE. THIS STANDARD PROVIDES INFORMATION ON UPDATES TO THE PAVEMENT MARKING AND SIGNING DETAILS IN ORDER TO INCORPORATE CHANGES ADOPTED IN THE 2009 NATIONAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICE (MUTCD). THESE NEW DETAILS HAVE BEEN STUDIED AND TESTED BY THE DEPARTMENT AND ACCEPTED BY THE ILLINOIS COMMERCE COMMISSION.

THIS APPLIES TO PROJECTS WHICH INCLUDE RAILROAD INTERCONNECTED TRAFFIC SIGNALS, WITH OR WITHOUT PRE-SIGNALS. THIS STANDARD ALSO APPLIES TO NON-SIGNALIZED INTERSECTIONS THAT ARE WITHIN 81 FEET OF A RAILROAD GRADE CROSSING. THE ILLINOIS SUPPLEMENT TO THE MUTCD SHOULD BE CONSULTED FOR ADDITIONAL INFORMATION ON SIGN REQUIREMENTS AT NON-SIGNALIZED INTERSECTIONS NEAR RAILROAD GRADE CROSSINGS.

THESE DETAILS WILL BE INCLUDED IN A FUTURE UPDATE TO THE BUREAU OF OPERATIONS TRAFFIC POLICIES AND PROCEDURES MANUAL.

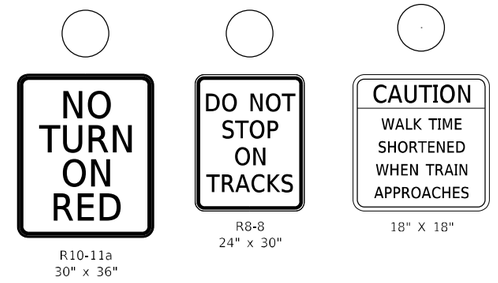


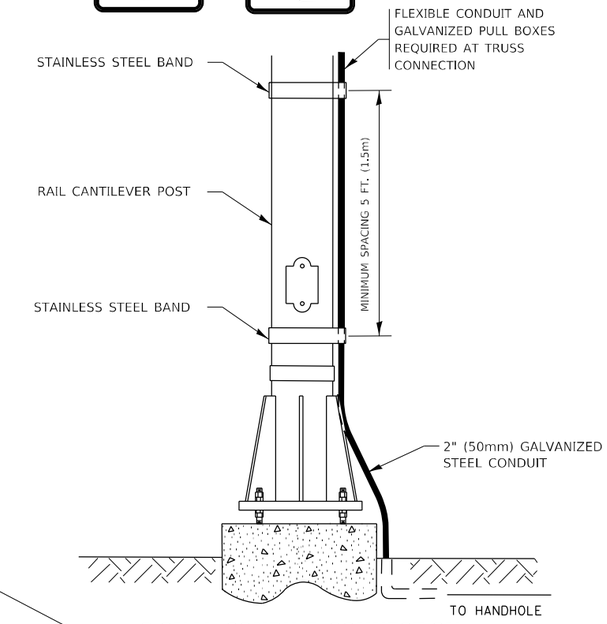
SIGNALIZED INTERSECTION WITH NEAR–SIDE TRAFFIC SIGNAL

NOTE:

1. PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
2. PRE-SIGNAL POST-MOUNTED TRAFFIC SIGNAL HEADS INSTALLED IN THE MEDIAN SHALL HAVE A MINIMUM VERTICAL CLEARANCE OF 4.5 FT. PARKWAY POST-MOUNTED TRAFFIC SIGNAL HEADS SHALL HAVE A MINIMUM VERTICAL CLEARANCE OF 8 FT.

IF RAILROAD CANTILEVER IS NON-EXISTANT AND NONE IS PROPOSED, THEN OVERHEAD TRAFFIC SIGNAL TO BE MOUNTED ON A SIGNAL MAST ARM. SIGNAL MAST ARM AND SIGNAL HEADS SHALL BE INSTALLED AS CLOSE AS PRACTICABLE TO THE RAILROAD TRACKS WITHOUT OBSTRUCTING ANY RAILROAD WARNING DEVICES. SIGNAL MAST ARM SHALL BE AT LEAST 12 FT. FROM NEAREST RAIL.





SIGNAL CONDUIT CONNECTION TO RAIL CANTILEVER DETAIL

USE NON-CONDUCTIVE SPACERS BETWEEN THE TRAFFIC SIGNAL EQUIPMENT AND THE RAILROAD CANTILEVER TO PREVENT DISSIMILAR METAL CORROSION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

USER NAME = AYA.ELkhatib	DESIGNED -	REVISED -	02-25-11
	DRAWN -	REVISED -	04-26-12
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	A.R. 07-11-16
PLOT DATE = 10/23/2023	DATE -	REVISED -	D.G. 8-22-19

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS

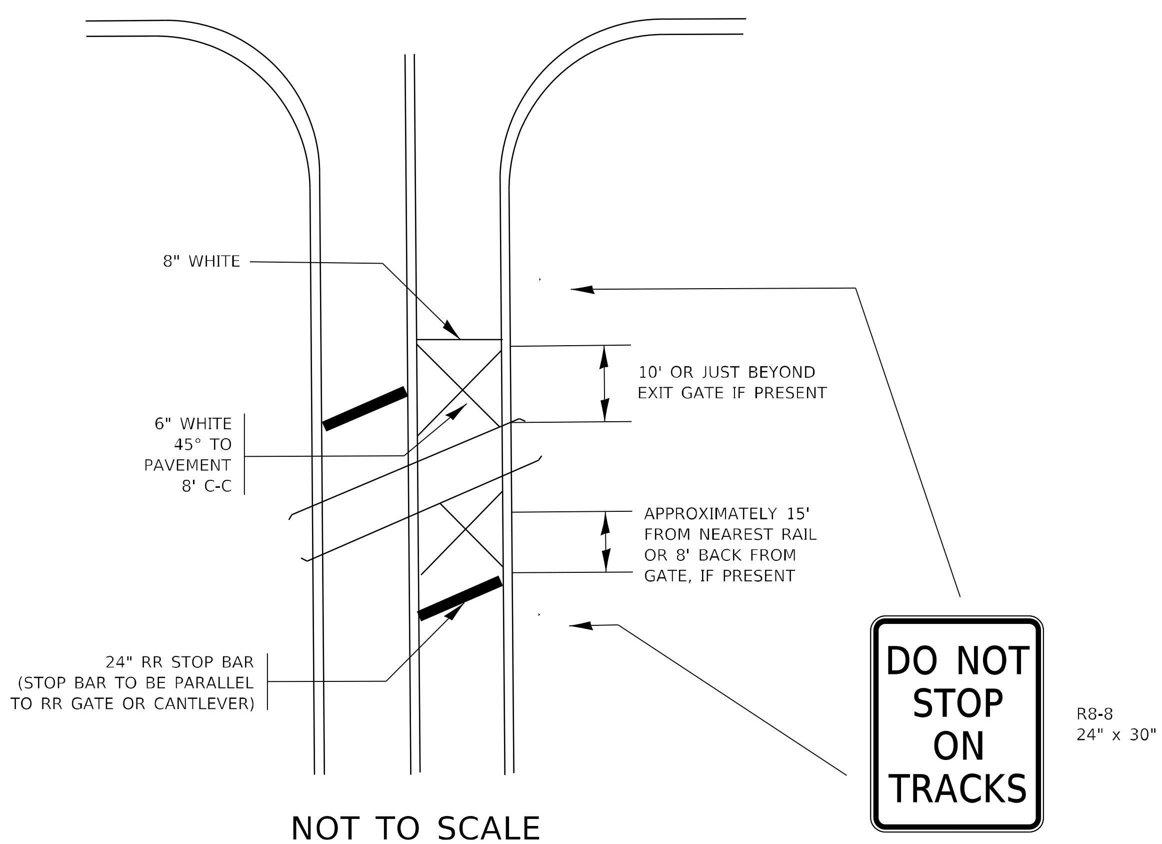
SCALE: NONE | SHEET 1 OF 2 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	49
	TC–23	CONTRACT NO. 62T87		
	ILLINOIS FED. AID PROJECT			

MODEL: Default
FILE NAME: ave:\\dot-sw.bentley.com:PWIDOT Documents\IDOT Offices\District 1\Projects\D1262822\CADData\Design\Dist1Std.dgn

TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS

WITH SIGNALIZED INTERSECTION



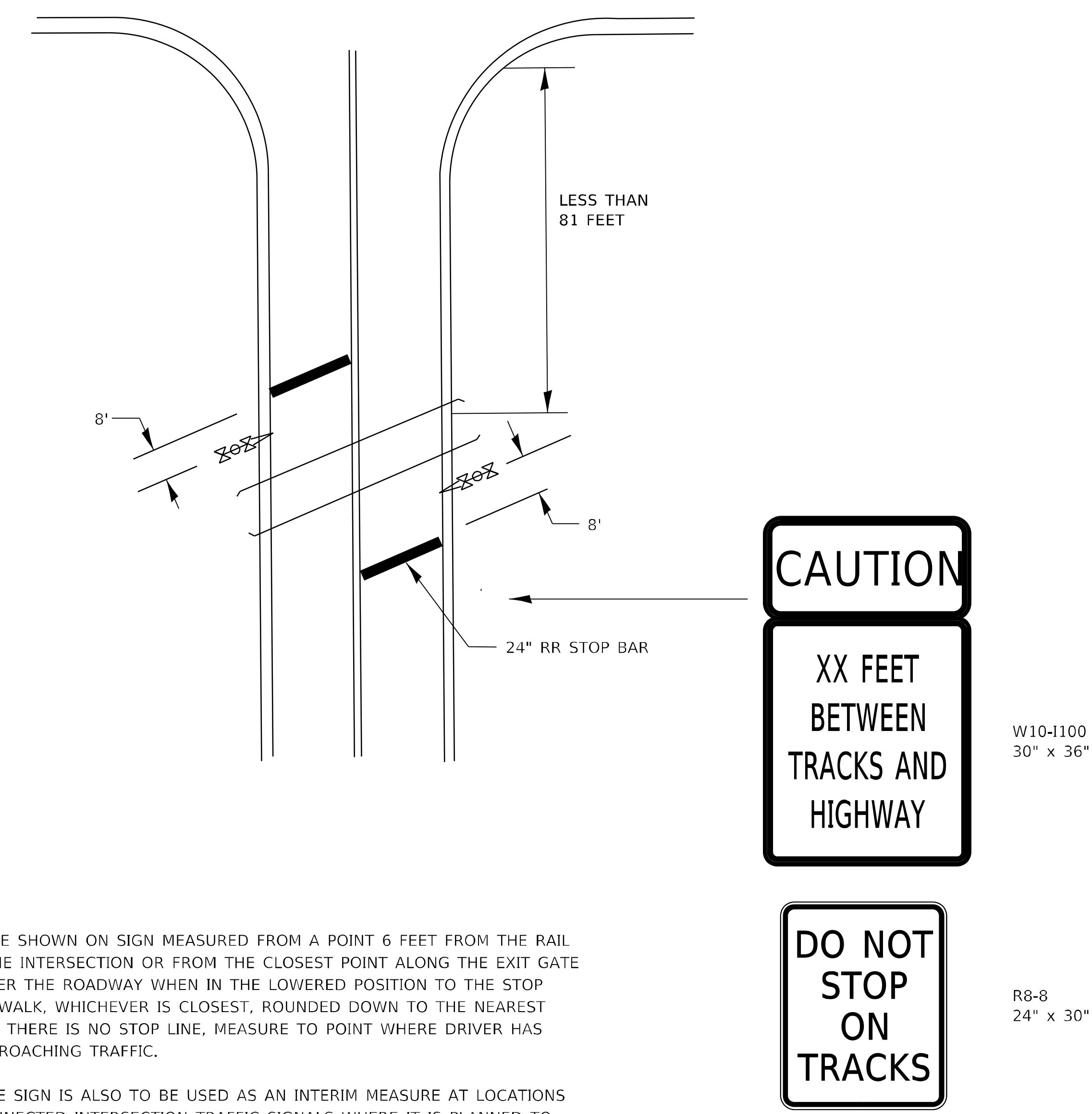
NOT TO SCALE

NOTE:

1. PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
2. WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED THE PAVEMENT MARKINGS EXTEND TO THE INTERSECTION. (SEE DETAIL FOR PRE-SIGNALS).

WITH NON–SIGNALIZED INTERSECTION

81' OR LESS TO CLOSEST RAIL



NOTE:

1. DISTANCE TO BE SHOWN ON SIGN MEASURED FROM A POINT 6 FEET FROM THE RAIL CLOSEST TO THE INTERSECTION OR FROM THE CLOSEST POINT ALONG THE EXIT GATE IF PRESENT OVER THE ROADWAY WHEN IN THE LOWERED POSITION TO THE STOP BAR OR CROSSWALK, WHICHEVER IS CLOSEST, ROUNDED DOWN TO THE NEAREST 5 FEET. WHERE THERE IS NO STOP LINE, MEASURE TO POINT WHERE DRIVER HAS A VIEW OF APPROACHING TRAFFIC.
2. THE CLEARANCE SIGN IS ALSO TO BE USED AS AN INTERIM MEASURE AT LOCATIONS WITH INTERCONNECTED INTERSECTION TRAFFIC SIGNALS WHERE IT IS PLANNED TO CHANGE THEM TO NEAR-SIDE SIGNALS AT A FUTURE TIME. IN THIS CASE, THE DISTANCE TO BE SHOWN ON THE SIGN IS MEASURED FROM THE EDGE OF THE STRIPED-OUT AREA INSTEAD OF 6 FEET FROM THE RAIL. THE SIGN IS TO BE REMOVED WHEN THE NEAR-SIDE SIGNALS ARE INSTALLED AND THE PAVEMENT MARKING EXTEND TO THE INTERSECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

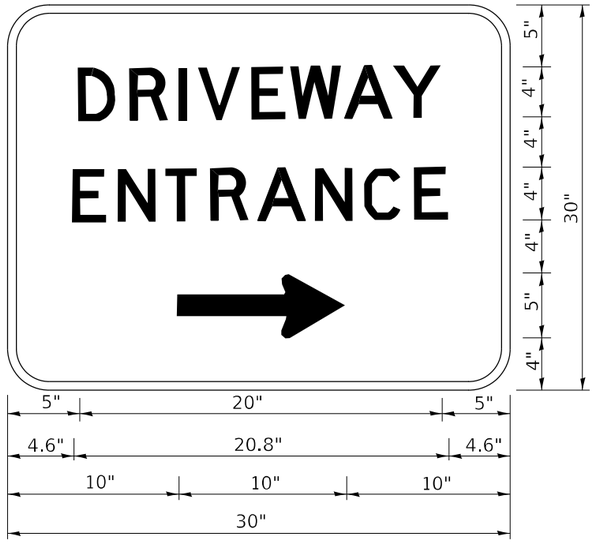
USER NAME = AYA.ELkhatib	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
	DRAWN -	REVISED -	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	
PLOT DATE = 10/23/2023	DATE -	REVISED -	

TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS

SCALE: NONE | SHEET 2 OF 2 SHEETS | STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	50
	TC–23	CONTRACT NO. 62T87		
	ILLINOIS FED. AID PROJECT			

MODEL: Default
FILE NAME: pw:\\ildot-pw.bentley.com:PWIDOT\Documents\IDOT Offices\District 1\Projects\D126822\CADData\Design\DistStd.dgn



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
"DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

USER NAME = AYA.ELkhatib	DESIGNED -	REVISED - C. JUCIUS 02-15-07
	DRAWN -	REVISED -
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 10/23/2023	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

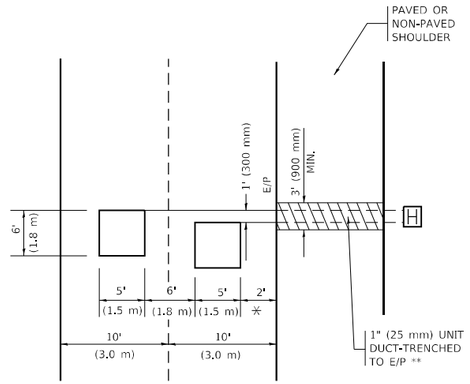
DRIVEWAY ENTRANCE SIGNING

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	51
	TC-26	CONTRACT NO. 62T87		
	ILLINOIS FED. AID PROJECT			

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



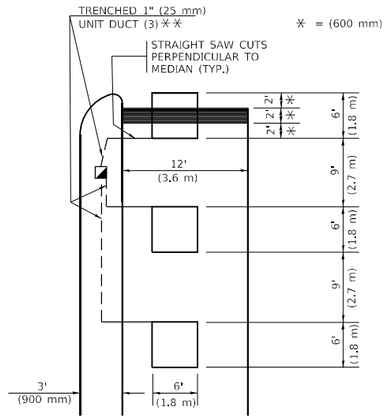
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS

VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH (PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



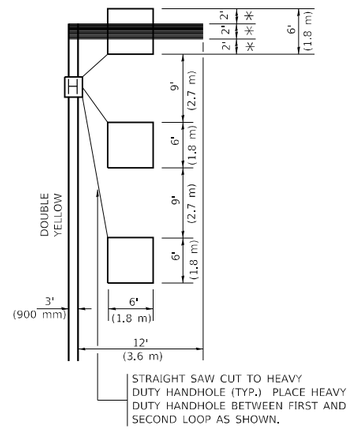
** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS

VOLUME DENSITY (''FAR OUT'' DETECTION) ON SAME APPROACH (PROTECTED / PERMITTED LEFT TURN PHASING)

* = (600 mm)



NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

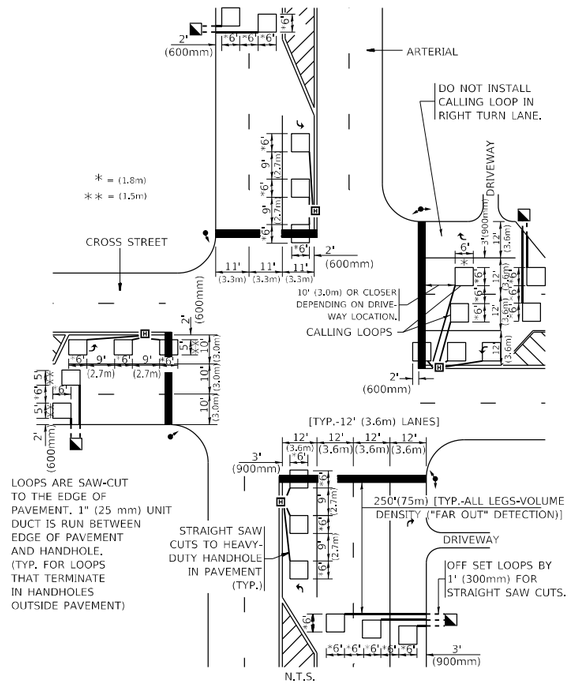
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

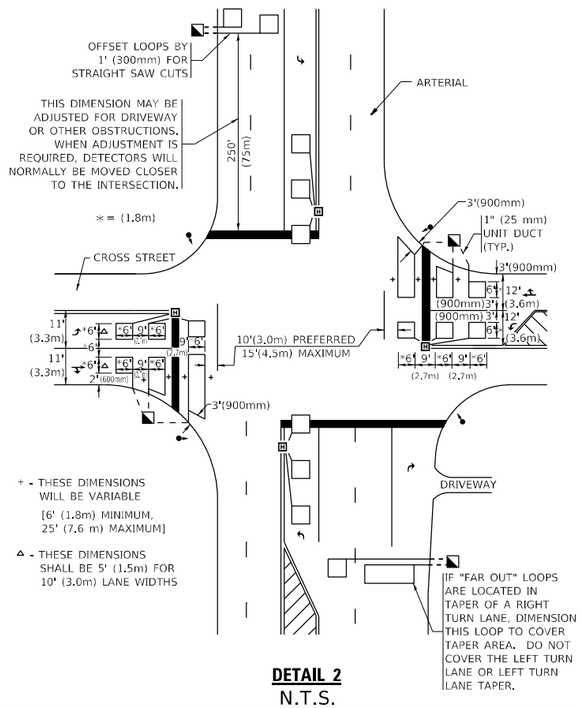
THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

USER NAME = AYA.ELkhatib	DESIGNED -		REVISED -
	DRAWN -		REVISED -
PLOT SCALE = 100.0000 ' / in.	CHECKED -	R.K.F.	REVISED -
PLOT DATE = 10/23/2023	DATE -		REVISED -

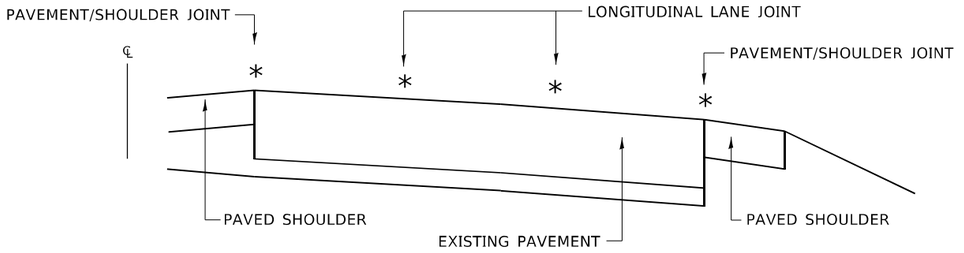
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 – DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

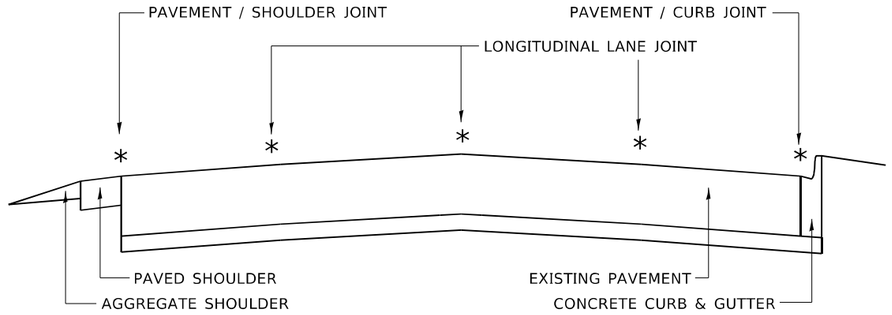
SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	52
	TS-07	CONTRACT NO. 62T87		
	ILLINOIS FED. AID PROJECT			

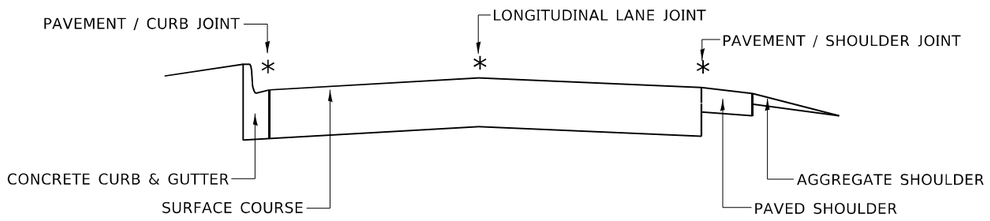
MODEL: Default
FILE NAME: ow:\\Idot-ow.bentley.com:PWIDOT Documents\IDOT Office of District 1\Projects\D1262372\CADData\Design\Dist Std.dgn



MULTI-LANE DIVIDED PAVEMENT



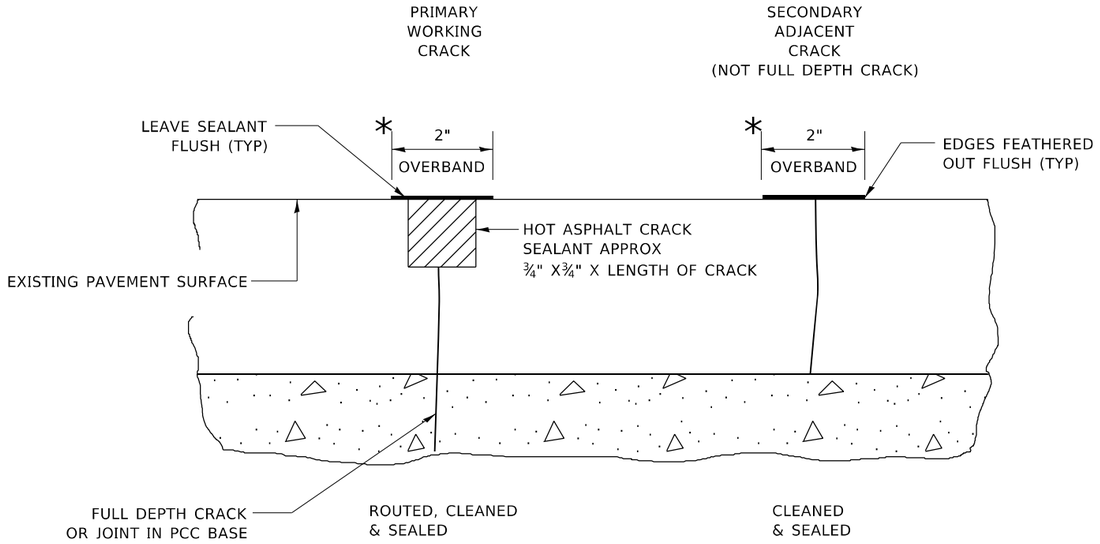
MULTI-LANE UNDIVIDED PAVEMENT



TWO-LANE PAVEMENT

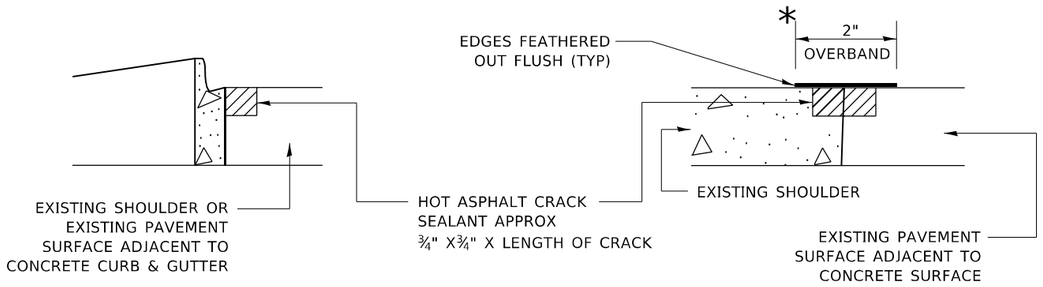
* PROPOSED LONGITUDINAL CRACK ROUTING (PAVEMENT) & CRACK FILLING LOCATIONS

GENERAL EXISTING TYPICAL SECTIONS (APPLIES TO HMA AND PCC PAVEMENTS)



* IN ALL LOCATIONS WHERE THE LONGITUDINAL CRACK CONFLICTS WITH EXISTING PAVEMENT MARKINGS (EITHER EDGE LINE OR LANE LINE) THE CRACK SHALL BE ROUTED & THE RESULTING RESERVOIR FILLED WITH SEALANT. ALL DAMAGES TO EXISTING RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE REPLACED AT THE EXPENSE OF THE CONTRACTOR. ALL EXISTING PAVEMENT MARKINGS WITHIN THE PROJECT LIMITS SHALL BE RESTORED WITH MODIFIED URETHANE PAVEMENT MARKINGS.

LONGITUDINAL JOINT



CRACK & JOINT SEALING DETAIL

USER NAME = AYA.ELkhatib	DESIGNED - F. Aqueel / A. Midy	REVISED -	
	DRAWN -	REVISED -	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	
PLOT DATE = 10/23/2023	DATE - 10/08/2020	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CRACK & JOINT SEALING DETAIL (PD-11)

SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
368	FAP 0368 22 RS	COOK	53	53
	PD-11	CONTRACT NO. 62T87		
	ILLINOIS FED. AID PROJECT			

MODEL: Default
FILE NAME: pw:\\dotapw.bentley.com:PWDOT\Documents\IDOT Offices\District 1\Projects\D1262222\CADData\Design\Xkt5fd.dgn