

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0358	FAP 0358 22 RS	COOK	71	1
		ILLINOIS	CONTRACT NO. 62T58	

D-91-259-22



PROPOSED
HIGHWAY PLANS

FAP 0358: US 6 / IL 83 (TORRENCE AVE)
HARDING AVE TO 173RD ST
SECTION FAP 0358 22 RS
PROJECT NHPP-TLNK(736)
SMART OVERLAY AND ADA IMPROVEMENTS
COOK COUNTY

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THE IMPROVEMENT IS LOCATED IN
CALUMET CITY AND LANSING.

TRAFFIC DATA:

TORRENCE AVE /IL 83

STA 13+20 - 87+00

ADT = 22200

STA 87+00 - 127+00

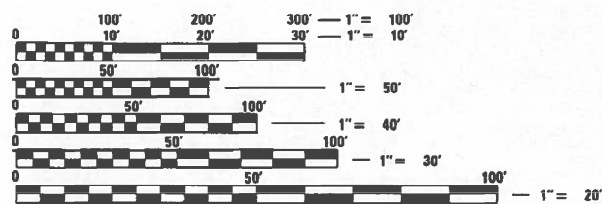
ADT = 18200

STA 127+00 - STA 172+02

ADT = 17700

STA 13+20 - 172+02

POSTED SPEED LIMIT = 40 MPH



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS
1-800-892-0123
OR 811

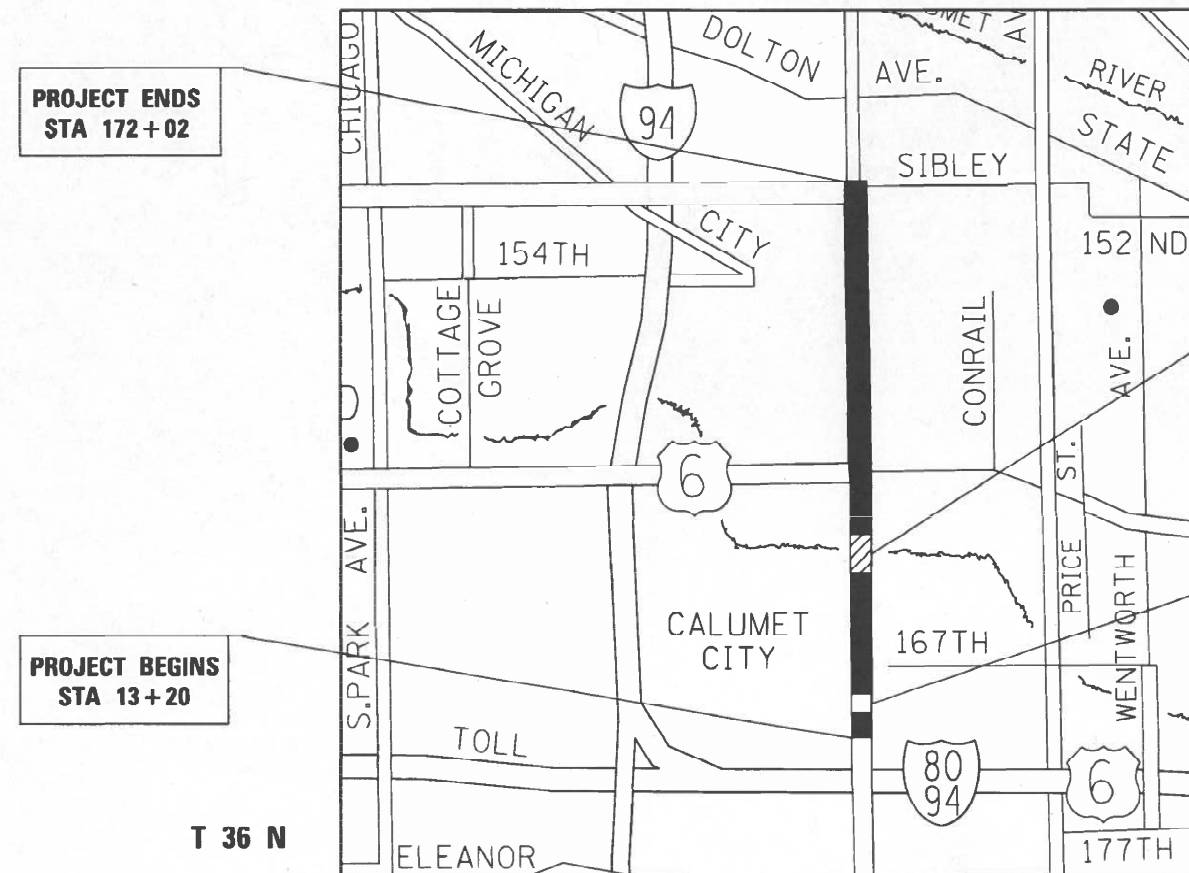
PROJECT ENGINEER LUKASZ POCIECHA (847) 705-4255

PROJECT MANAGER FAWAD AQUEEL

CONTRACT NO. 62T58

C-91-312-22

R 15 E



PROJECT ENDS
STA 172+02

PROJECT BEGINS
STA 13+20

RESURFACING OMISSION AT
STA 57+70 - 66+21

OMISSION AT
STA 28+00 - 38+06

T 36 N

THORNTON TOWNSHIP

GROSS LENGTH = 15,882 FT. = 3.00 MILE

NET LENGTH = 14,876 FT. = 2.82 MILE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED October 16, 2023

Jose Rivas
REGIONAL ENGINEER

December 8, 2023

Steph A. Etk
ENGINEER OF DESIGN AND ENVIRONMENT

December 8, 2023

Steph McAnis
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

HIGHWAY STANDARDS

GENERAL NOTES

SHEET NO.	DESCRIPTION
1	COVER SHEET
2-3	INDEX OF SHEETS, HIGHWAY STANDARDS, AND GENERAL NOTES
4-8	SUMMARY OF QUANTITIES
8	ADA SCHEDULE OF QUANTITIES
9-11	TYPICAL SECTION
11-16	ROADWAY AND PAVEMENT MARKING PLANS
17-30	ADA RAMP DESIGNS AND STANDARDS
31-52	TRAFFIC SIGNALS
53	DETAILS FOR FRAMES AND LIDS ADJUSTMENTS WITH MILLING (BD-08)
54	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)
55	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)
56	BUTT JOINT AND HMA TAPER DETAILS (BD-32)
57	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)
58	TYPICAL APPLICATION FOR RAISED REFLECTIVE PAVEMENT MARKERS (TC-11)
59	DISTRICT 1 TYPICAL PAVEMENT MARKING (TC-13)
60	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TC-14)
61	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS (TC-16)
62	ARTERIAL ROAD INFORMATION SIGN (TC-22)
63	DISTRICT ONE - MAST ARM MOUNTED STREET NAME SIGNS (TS-02)
64-70	DISTRICT ONE - STANDARD TRAFFIC SIGNAL DESIGN DETAILS (TS-05)
71	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-07)

STANDARD NO.	DESCRIPTION
000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-11	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424011-04	CORNER PARALLEL RAMPS FOR SIDEWALKS
424021-06	DEPRESSED CORNER RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
604001-05	FRAMES AND LIDS TYPE 1
604056-04	FRAME AND GRATE TYPE 11
604091-05	FRAME AND GRATE TYPE 24
606001-08	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
606301-04	PC CONCRETE ISLAND AND MEDIAN
642006-01	SHOULDER RUMBLE STRIPS, 8 IN.
701001-02	OFF ROAD OPERATIONS, 2L, 2W, MORE THAN 15' (4.5m) AWAY
701006-05	OFF ROAD OPERATIONS, 2L, 2W, 15' (4.5m) TO 24" (600mm) FROM PAVEMENT EDGE
701011-04	OFF ROAD MOVING OPERATIONS, 2L, 2W, DAILY
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' (4.5 M) TO 24" (600 MM) FROM PAVEMENT EDGE
701106-02	OFF-RD OPERATIONS, MULTILANE, MORE THAN 15' (4.5 M) AWAY
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS DAY ONLY
701427-05	LANE CLOSURE, MULTIPLANE, TINTERMITTENT OR MOVING OPER. FOR SPEED < OR = 40 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701602-10	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701611-01	URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-09	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS
886001-01	DETECTOR LOOP INSTALLATIONS

1. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE (or TOLLWAY) PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT (or ISTHA).
2. BUTT JOINTS SHALL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE 1/2 BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS 1/2 SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
3. TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN.
4. SIDEWALK REMOVAL AND P.C.C. SIDEWALK LOCATIONS SHALL BE DETERMINED BY THE ENGINEER.
5. DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
6. IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
7. FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
8. STORM SEWER CONSTRUCTED UNDER THE ROADWAY SHALL BE BACKFILLED ACCORDING TO METHOD 1 OF ARTICLE 550.07 OF THE STANDARD SPECIFICATIONS.
9. THE CONTRACTOR SHALL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF PLATED STRUCTURES BY STATION AND OFFSET LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT.
10. THE CONTRACTOR SHALL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR SHALL DELIVER THE RECORD TO THE ENGINEER.
11. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
12. FOR WORK OUTSIDE THE LIMITS OF BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB AND GUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT, SHALL BE EPOXY COATED, UNLESS NOTED ON THE PLAN.
13. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA,ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

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USER NAME = Sher.Khan	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES US 6/IL 83 TORRENCE AVE (HARDING AVE TO 173RD ST)	F.A.P. RTE.:	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
DRAWN -	REVISED -	0358			FAP 0358 22 RS	COOK	71	2		
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 62T58					
PLOT DATE = 11/18/2023	DATE -	REVISED -			SCALE:	SHEET	OF	SHEETS	STA.	TO
					ILLINOIS FED. AID PROJECT					

GENERAL NOTES

- 17. CONTRACT MUST CONTACT ROADSIDE DEVELOPMENT UNIT AT (847) 705-4171 AT LEAST 2 WEEKS PRIOR TO FORESTRY WORK TO IDENTIFY & MARK TREES TO SAVE WITHIN SELECTIVE CLEARING AREAS, TO IDENTIFY LOCATIONS FOR BASAL TREATMENT, AND TO MARK THE TREES FOR TREE CARE ITEMS.

- 18. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE NOTIFICATION OF ALL EMERGENCY SERVICES, SCHOOL DISTRICTS, I.D.O.T.S COMMUNICATIONS CENTER, SPRINGFIELD TRUCK PERMIT SECTION AND OTHER AGENCIES AFFECTED BY THE CLOSURE. THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR POSTING SIGNS THAT WILL INDICATE THE DATES THE CLOSURE WILL BE IN PLACE.

- 19. PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.

- 20. THE ARTERIAL ROAD INFORMATION SIGN (TC-22) IS APPLICABLE ONLY TO ARTERIAL ROADS AND SHALL NOT BE APPLIED TO EXPRESSWAYS/TOLLWAYS.

- 21. OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS.

- 22. TWO WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS ON IDOT ARTERIAL ROADS, THE ENGINEER SHALL CONTACT PATRICE HARRIS, AREA TRAFFIC FIELD TECHNICIAN AT PATRICE.HARRIS@ILLINOIS.GOV.

- 23. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD, FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKING SHALL BE AS DIRECTED BY THE ENGINEER.

- 24. THE "ROAD CONSTRUCTION AHEAD" SIGNS SHALL REMAIN INSTALLED UNTIL THE COMPLETION OF THE PROJECT OR WHEN NO ROADWAY HAZARDS REMAIN IN THE WORK ZONE.

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USER NAME = Sher,Khan	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,0000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 10/20/2023	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES
US 6/IL 83 TORRENCE AVE (HARDING AVE TO 173RD ST)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0358	FAP 0358 22 RS	COOK	71	3
			CONTRACT NO. 62T58	
		ILLINOIS	FED. AID PROJECT	

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		ROADWAY 80% FED 20% STATE 0005	100% STATE 0005	TRAFFIC SIGNAL 80% FED 20% STATE 0021			
20200100	EARTH EXCAVATION	CU YD	175	175					
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	110	110					
25200110	SODDING, SALT TOLERANT	SO YD	110	110					
25200200	SUPPLEMENTAL WATERING	UNIT	1	1					
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	50045	50045					
40600370	LONGITUDINAL JOINT SEALANT	FOOT	55215	55215					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	167	167					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	804	804					
40605026	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F" N80	TON	10899	10899					
42001300	PROTECTIVE COAT	SO YD	600	600					
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SO YD	30	30					
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	3839	3748		91			
42400410	PORTLAND CEMENT CONCRETE SIDEWALK 8 INCH	SQ FT	500	500					
42400800	DETECTABLE WARNINGS	SO FT	351	351					

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		ROADWAY 80% FED 20% STATE 0005	100% STATE 0005	TRAFFIC SIGNAL 80% FED 20% STATE 0021			
44000156	HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"	SO YD	111212	111212					
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	30	30					
44000600	SIDEWALK REMOVAL	SO FT	3748	3748					
44201777	CLASS D PATCHES, TYPE II, 11 INCH	SO YD	2900	2900					
44201781	CLASS D PATCHES, TYPE III, 11 INCH	SO YD	300	300					
44201783	CLASS D PATCHES, TYPE IV, 11 INCH	SO YD	870	870					
60255500	MANHOLES TO BE ADJUSTED	EACH	16	16					
60260100	INLETS TO BE ADJUSTED	EACH	16	16					
60266600	VALVE BOXES TO BE ADJUSTED	EACH	3	3					
60619600	CONCRETE MEDIAN, TYPE SB-6.12	SO FT	144	144					
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	38	38					
66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	105	105					
66900530	SOIL DISPOSAL ANALYSIS	EACH	4	4					
66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	LSUM	1	1					
66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	LSUM	1	1					
66901006	REGULATED SUBSTANCES MONITORING	CAL DA	8	8					

* = SPECIALTY ITEMS

FILE NAME =	USER NAME = Sher.Khan	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US 6/IL 83 TORRENCE AVE (HARDING AVE TO 173RD ST) SUMMARY OF QUANTITIES	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw\N\dot-pw.Bentley.com\PW\DOT\Documents\1\DOT - Office\District\Projects\DI25922\CADD\Design\DI25922-st-300.DWG		DRAWN -	REVISED -			0358	FAP 0358 22 RS	COOK	71	4	
PLOT SCALE = 100,0000' / 1in		CHECKED -	REVISED -			CONTRACT NO. 62T58					
PLOT DATE = 10/20/2023		DATE -	REVISED -			SCALE: SHEET NO. OF SHEETS STA. TO STA. FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE							
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	ROADWAY 80% FED 20% STATE 0005	100% STATE 0005	TRAFFIC SIGNAL 80% FED 20% STATE 0021				CODE NO	ITEM	UNIT	TOTAL QUANTITIES	ROADWAY 80% FED 20% STATE 0005	100% STATE 0005	TRAFFIC SIGNAL 80% FED 20% STATE 0021				
67100100	MOBILIZATION	L SUM	1	1						70300251	TEMPORARY PAVEMENT MARKING - LINE 8" -	FOOT	980	980						
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1							PAINT									
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1						70300261	TEMPORARY PAVEMENT MARKING - LINE 12" -	FOOT	3627	3627						
											PAINT									
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1						70300281	TEMPORARY PAVEMENT MARKING - LINE 24" -	FOOT	1674	1674						
											PAINT									
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	1						70306120	TEMPORARY PAVEMENT MARKING - LINE 4" -	FOOT	14156	14156						
70102634	TRAFFIC CONTROL AND PROTECTION, STANDARD 701611	L SUM	1	1							TYE III TAPE									
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1						* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	1559	1559						
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1						* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	48309	48309						
70300100	SHORT TERM PAVEMENT MARKING	FOOT	33849	33849						* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	8787	8787						
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	11283	11283						* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	980	980						
70300211	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - PAINT	SO FT	1559	1559						* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	3627	3627						
70300221	TEMPORARY PAVEMENT MARKING - LINE 4" - PAINT	FOOT	48309	48309						* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	1674	1674						
70300241	TEMPORARY PAVEMENT MARKING - LINE 6" - PAINT	FOOT	8787	8787						* 78004635	PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - LINE 7"	FOOT	420	420						
										* 78009000	MODIFIED URETHANE PAVEMENT MARKINGS - LETTERS AND SYMBOLS	SO FT	194	194						

* = SPECIALTY ITEMS

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE						SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	ROADWAY 80% FED 20% STATE 0005	100% STATE 0005	TRAFFIC SIGNAL 80% FED 20% STATE 0021				CODE NO	ITEM	UNIT	TOTAL QUANTITIES	ROADWAY 80% FED 20% STATE 0005	100% STATE 0005	TRAFFIC SIGNAL 80% FED 20% STATE 0021			
* 78009004	MODIFIED URETHANE PAVEMENT MARKINGS - LINE 4"	FOOT	3061	3061						* 87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	487			487			
* 78009006	MODIFIED URETHANE PAVEMENT MARKINGS - LINE 6"	FOOT	709	709						* 87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	207			207			
* 78011040	GROOVING FOR RECESSED PAVEMENT MARKING 8"	FOOT	420	420						* 87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO.61C	FOOT	278			278			
* 78009012	MODIFIED URETHANE PAVEMENT MARKINGS - LINE 12"	FOOT	204	204						* 87900200	DRILL EXISTING HANDHOLE	EACH	8			8			
* 78009024	MODIFIED URETHANE PAVEMENT MARKINGS - LINE 24"	FOOT	87	87						* 88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNED WITH COUNTDOWN TIMER	EACH	4			4			
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1500	1500						* 88500100	INDUCTIVE LOOP DETECTOR	EACH	2			2			
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	1500	1500						* 88600100	DETECTOR LOOP, TYPE 1	FOOT	5212			5212			
78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SO FT	1753	1753						* 89502200	MODIFY EXISTING CONTROLLER	EACH	1			1			
* 81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	224			224				* 89502300	REMOVE EXISTING CABLE FROM CONDUIT	FOOT	1079			1079			
* 81400200	HEAVY-DUTY HANDHOLE	EACH	2			2				* 89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	5			5			
* 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	7			7				* 89502376	REBUILD EXISTING HANDHOLE	EACH	7	3		4			
* 87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	2320			2320				X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1					
										* X1400367	PEDESTRIAN SIGNAL POST, 10 FT.	EACH	2			2			* = SPECIALTY ITEMS
										* X1400378	PEDESTRIAN SIGNAL POST, 5 FT.	EACH	7			7			

FILE NAME =	USER NAME = Sher.Khan	DESIGNED -	REVISED -
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		DATE -	REVISED -
PLOT SCALE = 100.0000' / 1in			
PLOT DATE = 10/20/2023			

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**US 6/IL 83 TORRENCE AVE (HARDING AVE TO 173RD ST)
SUMMARY OF QUANTITIES**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0358	FAP 0358 22 RS	COOK	71	6
CONTRACT NO. 62T58				
SCALE: SHEET NO. OF SHEETS STA. TO STA. FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		ROADWAY 80% FED 20% STATE 0005	100% STATE 0005	TRAFFIC SIGNAL 80% FED 20% STATE 0021			CODE NO	ITEM		UNIT	ROADWAY 80% FED 20% STATE 0005	100% STATE 0005	TRAFFIC SIGNAL 80% FED 20% STATE 0021	
X4400501	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT LESS THAN OR EQUAL 10 FEET	FOOT	150	150												
X4400503	COMBINATION CONCRETE CURB REMOVAL AND REPLACEMENT GREATER THAN 10 FEET	FOOT	13100	13100												
X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	2160		2160											
* X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	53	53												
X6700407	ENGINEER'S FIELD OFFICE TYPE A (D1)	CAL MO	12	12												
* X8760200	ACCESSIBLE PEDESTRIAN SIGNALS	EACH	26		26											
* X8780012	CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	FOOT	36		36											
* X8860105	DETECTOR LOOP REPLACEMENT	FOOT	3432		3432											
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	108		108											
Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	103	103												
* Z0033044	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1		1											
Ø Z0076600	TRAINEES	HOURS	500	500												
Ø Z0076604	TRAINEES	HOURS	500	500												

* = SPECIALTY ITEMS

Ø 0042

FILE NAME =	USER NAME = Sher.Khan	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US 6/IL 83 TORRENCE AVE (HARDING AVE TO 173RD ST) SUMMARY OF QUANTITIES	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\N\dot-pw\benley.com\PW\DOT\Documents\DOT - Office\District\Projects\DI25922\CADD\Design\DI25922-st-300.DWG		DRAWN -	REVISED -			0358	FAP 0358 22 RS	COOK	71	7	
PLOT SCALE = 100,0000' / 1in		CHECKED -	REVISED -			CONTRACT NO. 62T58		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			
PLOT DATE = 10/20/2023		DATE -	REVISED -			SCALE:	SHEET NO.	OF	SHEETS	STA.	TO

ID	LOCATION	CORNER	20200100 EARTH EXCAVATION	21101615 TOPSOIL FURNISH AND PLACE, 4"	25200110 SODDING, SALT TOLERANT	25200200 SUPPLEMENTAL WATERING	42001300 PROTECTIVE COAT	42400200 PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	42400800 DETECTABLE WARNINGS	44000600 SIDEWALK REMOVAL	60255500 MANHOLES TO BE ADJUSTED	88800100 PEDESTRIAN PUSH-BUTTON	89502376 REBUILD EXISTING HANDHOLE	X4400503 COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT GREATER THAN 10 FEET
			CU YD	SQ YD	SQ YD	UNIT	SQ YD	SQ FT	SQ FT	SQ YD	EACH	EACH	EACH	FOOT
	Torrence Ave													
1	173rd St	NW	2.7	5.00	5.00		108.00	878	26	878			1.0	37
2	173rd St	NE	1.2	0.00	0.00		45.00	402	20					0
3	173rd St	SW	1.2	10.00	10.00		59.00	402	14	402				50
4	Pulaski Rd	NE	0.9	7.00	7.00		36.00	282	20	282	2.0			15
5	Pulaski Rd	SE	0.6	0.00	0.00		32.00	201	24	201	1.0		1.0	32
6	153rd St	NW	0.4	0.00	0.00		24.00	144	16	144				26
7	153rd St	NE	0.5	6.00	6.00		22.00	156	16	156	1.0			17
8	Wilson Ave	NE	0.3	0.00	0.00		16.00	96	13	96				17
9	Sibley Blvd.	NW	1.2	10.00	10.00		50.00	387	22	387			1.0	24
SUB-TOTAL =			9.1	38.0	38.0	0.0	392.0	2948.0	171.0	2546.0	4.0	0.0	3.0	218

** NOTE: ADA SCHEDULE OF QUANTITIES IS FOR PROPOSED ADA RAMP DESIGN ONLY.
QUANTITIES FOR CORNERS UPDATED WITH PERPENDICULAR CURB RAMP DETAILS
WILL BE DETERMINED BY THE ENGINEER IN THE FIELD.

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	DRAWN -	REVISED -
PLOT SCALE = 100,0000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 11/18/2023	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

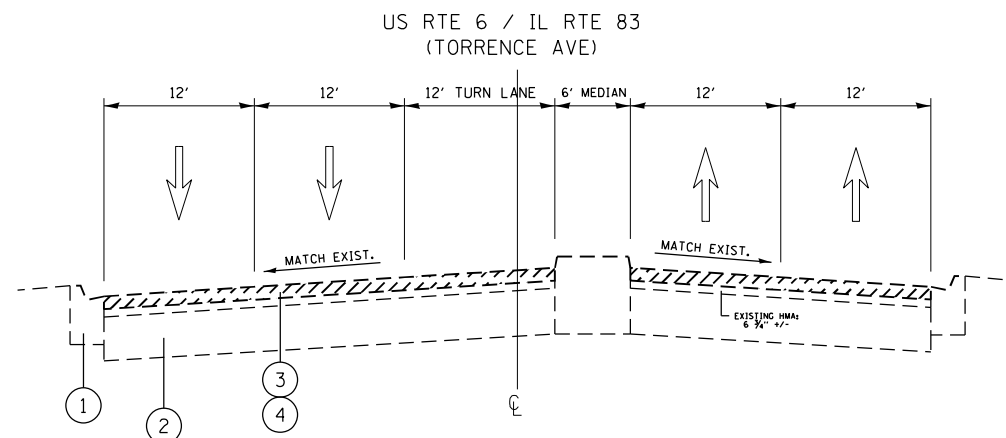
**US 61L 83 TORRENCE AVE (HARDING AVE TO 173RD ST)
SCHEDULE OF QUANTITIES**

SCALE: SHEET OF SHEETS STA. TO STA.

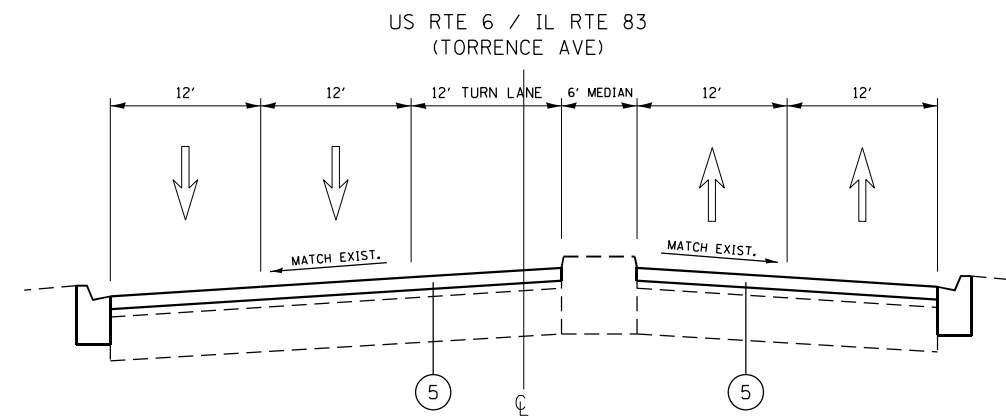
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0358	FAP 0358 22 RS	COOK	71	8
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T58	

LEGEND

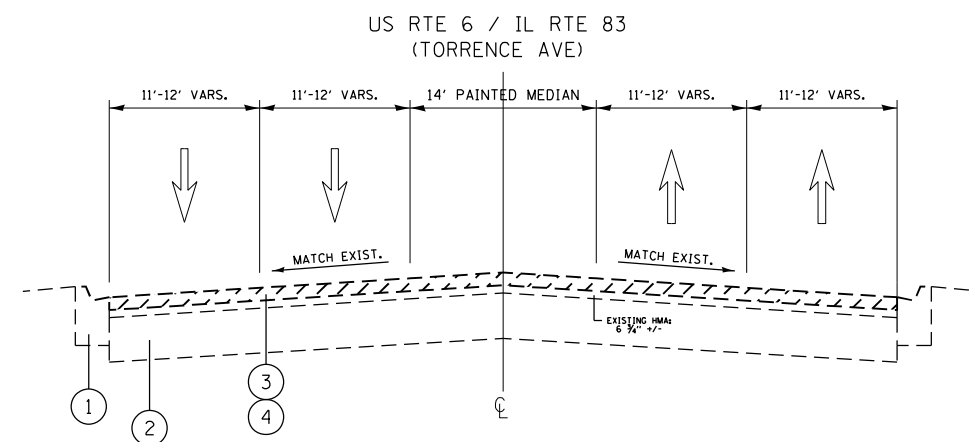
1. EXISTING COMBINATION CONCRETE CURB AND GUTTER
2. EXISTING PCC BASE COURSE (VARS. BETWEEN 9" - 11")
3. PROPOSED HMA SURFACE REMOVAL, 1 3/4"
4. PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80; 1 3/4"
5. PROPOSED COMBINATION CURB AND GUTTER (LOCATIONS TO BE DETERMINED BY THE RESIDENT ENGINEER)



EXISTING TYPICAL SECTION
STA 13+20 - STA 23+00

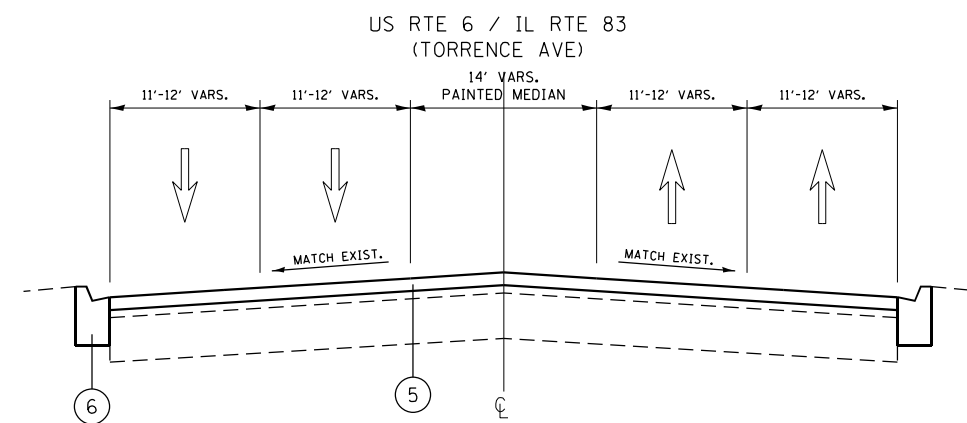


PRROPOSED TYPICAL SECTION
STA 13+20 - STA 23+00



EXISTING TYPICAL SECTION
STA 23+00 - STA 28+00
STA 38+06 - STA 39+69
STA 49+56 - STA 57+70
STA 92+86 - STA 146+00
STA 159+00 - STA 172+02

*OMISSION STA 28+00 - STA 38+06, RESURFACING OMISSION STA 57+70 - STA 66+21



PRROPOSED TYPICAL SECTION
STA 23+00 - STA 28+00
STA 38+06 - STA 39+69
STA 49+56 - STA 57+70
STA 92+86 - STA 146+00
STA 159+00 - STA 172+02

*OMISSION STA 28+00 - STA 38+06, RESURFACING OMISSION STA 57+70 - STA 66+21

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE TYPE	AIR VOIDS(%) @ Ndes	
HMA PAVEMENT		
POLY HMA SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80; 1 3/4"	3.5% @ 80 GYR.	PFP
PATCHING		
CLASS D PATCHES (HMA BINDER IL-19 MM)	4% @ 70 GYR.	QC/OA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/OA); QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PFP)		

NOTE

- NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTATIES IS 112 LBS/SQ YD/IN.
- NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONIS.
- NOTE 3: THE CONTRACTOR SHALL MILL BEFORE PATCHING.
- NOTE 4: THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE MILLED SURFACE

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PLOT DATE = 11/18/2023	CHECKED -	REVISED -
	DATE -	REVISED -

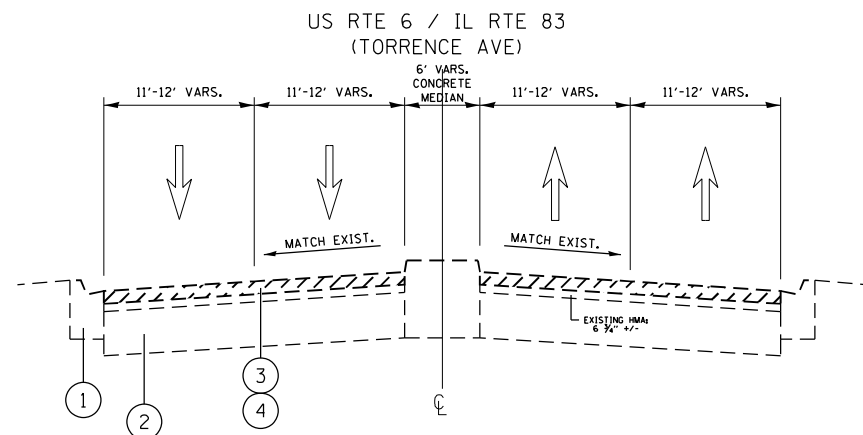
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTION			
US 6/IL 83 TORRENCE AVE (HARDING AVE TO 173RD ST)			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

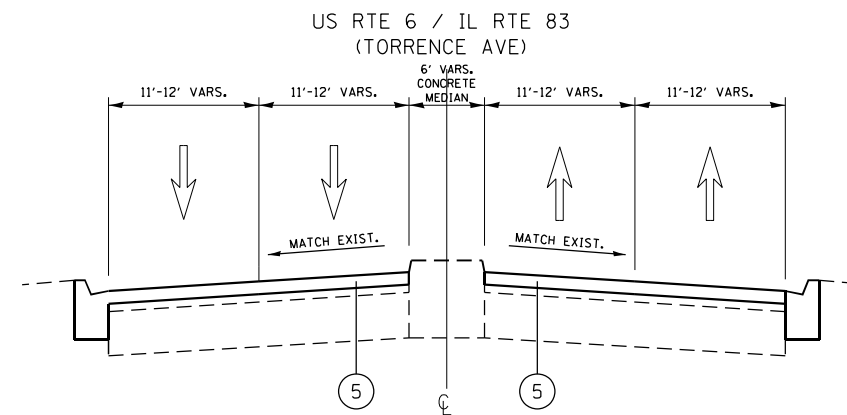
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0358	FAP 0358 22 RS	COOK	71	9
CONTRACT NO. 62T58				
ILLINOIS FED. AID PROJECT				

LEGEND

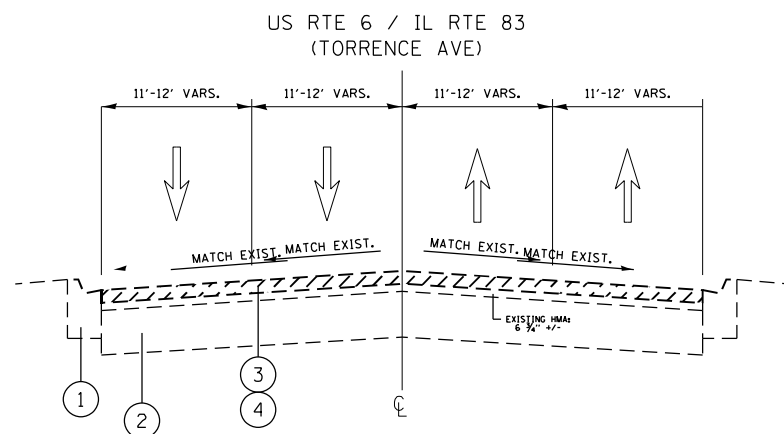
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|--|---|
| <ol style="list-style-type: none"> 1. EXISTING COMBINATION CONCRETE CURB AND GUTTER 2. EXISTING PCC BASE COURSE (VARS. BETWEEN 9" - 11") | <ol style="list-style-type: none"> 3. PROPOSED HMA SURFACE REMOVAL, 1 3/4" 4. PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80; 1 3/4" 5. PROPOSED COMBINATION CURB AND GUTTER (LOCATIONS TO BE DETERMINED BY THE RESIDENT ENGINEER) |
|--|---|



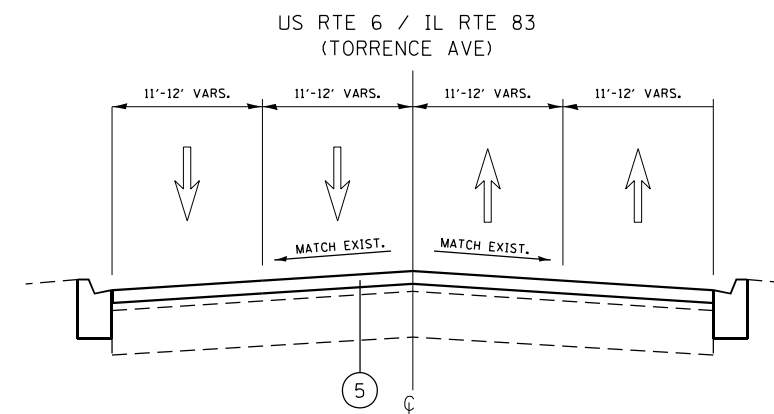
EXISTING TYPICAL SECTION
STA 39+69 - STA 49+56
STA 66+21 - STA 92+86



PROPOSED TYPICAL SECTION
STA 39+69 - STA 49+56
STA 66+21 - STA 92+86



EXISTING TYPICAL SECTION
STA 146+00 - STA 159+00



PROPOSED TYPICAL SECTION
STA 146+00 - STA 159+00

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	DRAWN -	REVISED -	
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PLOT DATE = 11/18/2023	DATE -	REVISED -	

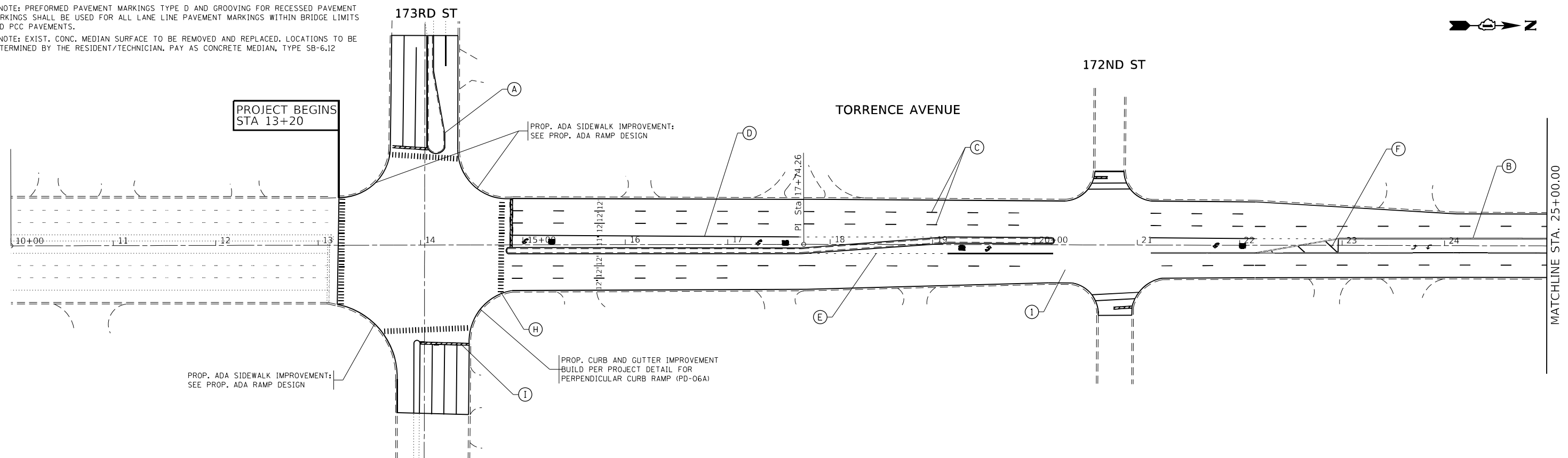
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTION
US 6/IL 83 TORRENCE AVE (HARDING AVE TO 173RD ST)**

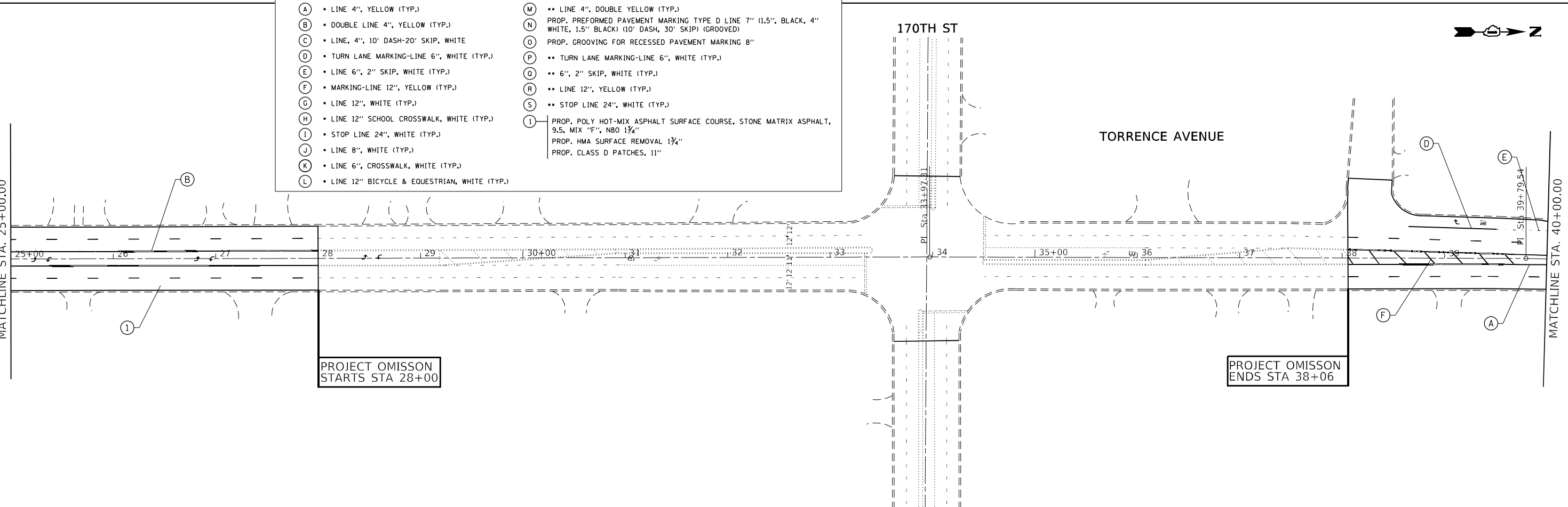
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0358	FAP 0358 22 RS	COOK	71	10
CONTRACT NO. 62T58				
ILLINOIS FED. AID PROJECT				

••NOTE: PREFORMED PAVEMENT MARKINGS TYPE D AND GROOVING FOR RECESSED PAVEMENT MARKINGS SHALL BE USED FOR ALL LANE LINE PAVEMENT MARKINGS WITHIN BRIDGE LIMITS AND PCC PAVEMENTS.
 ••NOTE: EXIST. CONC. MEDIAN SURFACE TO BE REMOVED AND REPLACED. LOCATIONS TO BE DETERMINED BY THE RESIDENT/TECHNICIAN. PAY AS CONCRETE MEDIAN, TYPE SB-6.12



PAVEMENT MARKING LEGEND	
•• = THERMOPLASTIC PAVEMENT MARKING -	
••• = MODIFIED URETHANE PAVEMENT MARKING -	
(A)	• LINE 4", YELLOW (TYP.)
(B)	•• DOUBLE LINE 4", YELLOW (TYP.)
(C)	• LINE 4", 10' DASH-20" SKIP, WHITE
(D)	• TURN LANE MARKING-LINE 6", WHITE (TYP.)
(E)	• LINE 6", 2" SKIP, WHITE (TYP.)
(F)	• MARKING-LINE 12", YELLOW (TYP.)
(G)	• LINE 12", WHITE (TYP.)
(H)	• LINE 12" SCHOOL CROSSWALK, WHITE (TYP.)
(I)	• STOP LINE 24", WHITE (TYP.)
(J)	• LINE 8", WHITE (TYP.)
(K)	• LINE 6", CROSSWALK, WHITE (TYP.)
(L)	• LINE 12" BICYCLE & EQUESTRIAN, WHITE (TYP.)
(M)	•• LINE 4", DOUBLE YELLOW (TYP.)
(N)	•• PROP. PREFORMED PAVEMENT MARKING TYPE D LINE 7" (1.5", BLACK, 4" WHITE, 1.5" BLACK) (10" DASH, 30" SKIP) (GROOVED)
(O)	• PROP. GROOVING FOR RECESSED PAVEMENT MARKING 8"
(P)	•• TURN LANE MARKING-LINE 6", WHITE (TYP.)
(Q)	•• 6", 2" SKIP, WHITE (TYP.)
(R)	•• LINE 12", YELLOW (TYP.)
(S)	•• STOP LINE 24", WHITE (TYP.)
(I)	• PROP. POLY HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80 1 3/4" • PROP. HMA SURFACE REMOVAL 1 3/4" • PROP. CLASS D PATCHES, 11"



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PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED -
PLOT DATE = 11/18/2023	CHECKED -	REVISED -
	DATE -	REVISED -

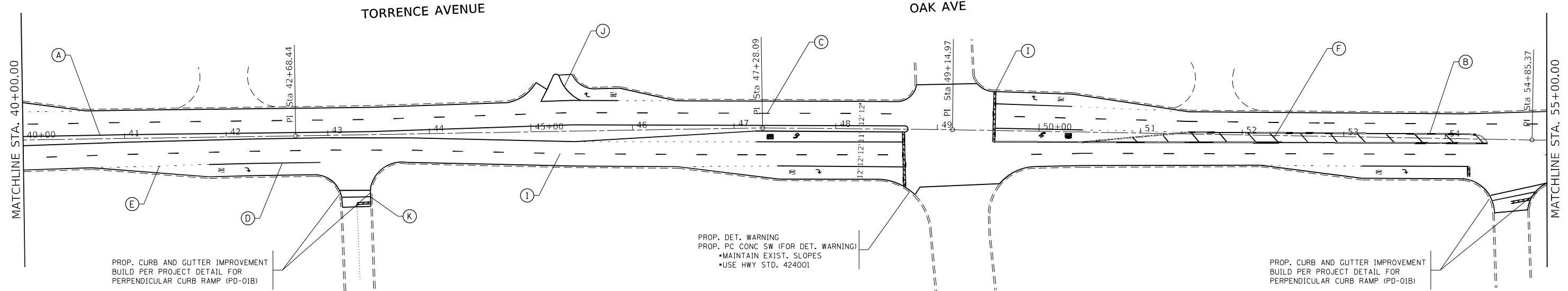
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

ROADWAY PLAN	
TORRENCE AVE (173RD ST TO HARDING AVE)	
SCALE: 1"=50'	SHEET OF SHEETS STA. TO STA.

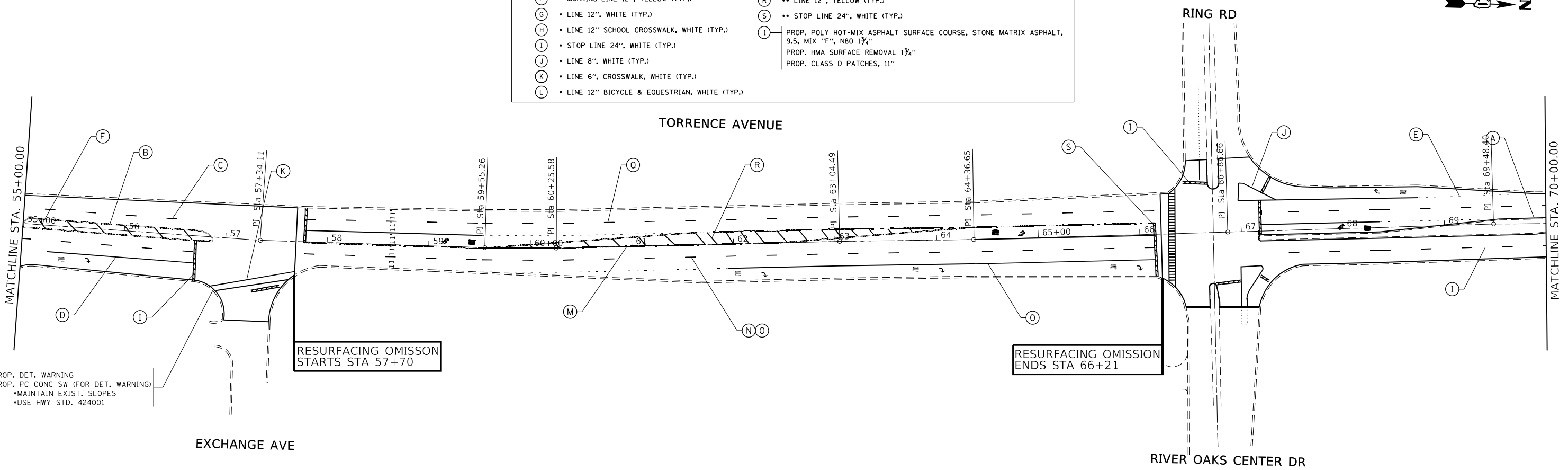
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0358	FAP 0358 22 RS	COOK	71	11
CONTRACT NO. 62T58				
ILLINOIS FED. AID PROJECT				

••NOTE: PREFORMED PAVEMENT MARKINGS TYPE D AND GROOVING FOR RECESSED PAVEMENT MARKINGS SHALL BE USED FOR ALL LANE LINE PAVEMENT MARKINGS WITHIN BRIDGE LIMITS AND PCC PAVEMENTS.

••NOTE: EXIST. CONC. MEDIAN SURFACE TO BE REMOVED AND REPLACED. LOCATIONS TO BE DETERMINED BY THE RESIDENT/TECHNICIAN. PAY AS CONCRETE MEDIAN, TYPE SB-6.12



PAVEMENT MARKING LEGEND	
• = THERMOPLASTIC PAVEMENT MARKING	
•• = MODIFIED URETHANE PAVEMENT MARKING	
(A) • LINE 4", YELLOW (TYP.)	(M) •• LINE 4", DOUBLE YELLOW (TYP.)
(B) • DOUBLE LINE 4", YELLOW (TYP.)	(N) PROP. PREFORMED PAVEMENT MARKING TYPE D LINE 7" (1.5", BLACK, 4" WHITE, 1.5" BLACK) (10' DASH, 30' SKIP) (GROOVED)
(C) • LINE, 4", 10' DASH-20' SKIP, WHITE	(O) PROP. GROOVING FOR RECESSED PAVEMENT MARKING 8"
(D) • TURN LANE MARKING-LINE 6", WHITE (TYP.)	(P) •• TURN LANE MARKING-LINE 6", WHITE (TYP.)
(E) • LINE 6", 2" SKIP, WHITE (TYP.)	(Q) •• 6", 2" SKIP, WHITE (TYP.)
(F) • MARKING-LINE 12", YELLOW (TYP.)	(R) •• LINE 12", YELLOW (TYP.)
(G) • LINE 12", WHITE (TYP.)	(S) •• STOP LINE 24", WHITE (TYP.)
(H) • LINE 12" SCHOOL CROSSWALK, WHITE (TYP.)	(I) PROP. POLY HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80 1 3/4"
(J) • STOP LINE 24", WHITE (TYP.)	PROP. HMA SURFACE REMOVAL 1 3/4"
(K) • LINE 8", WHITE (TYP.)	PROP. CLASS D PATCHES, 11"
(L) • LINE 6", CROSSWALK, WHITE (TYP.)	
(I) • LINE 12" BICYCLE & EQUESTRIAN, WHITE (TYP.)	



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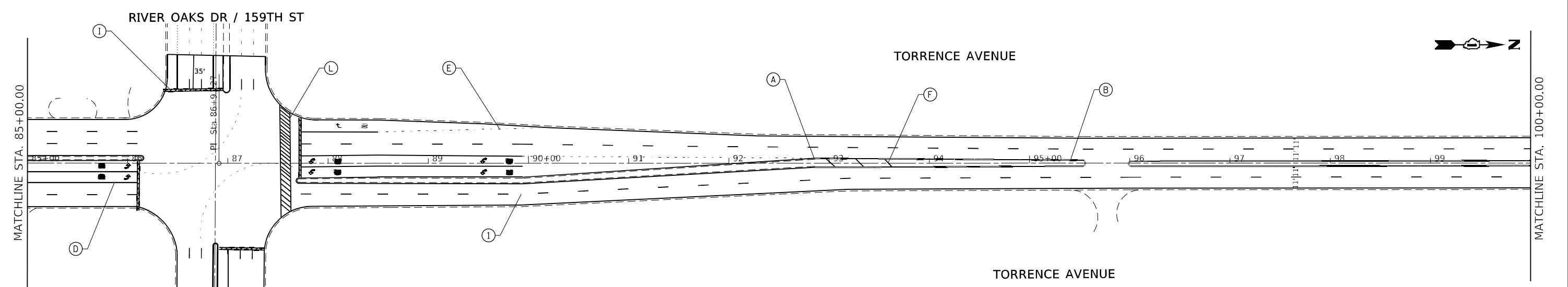
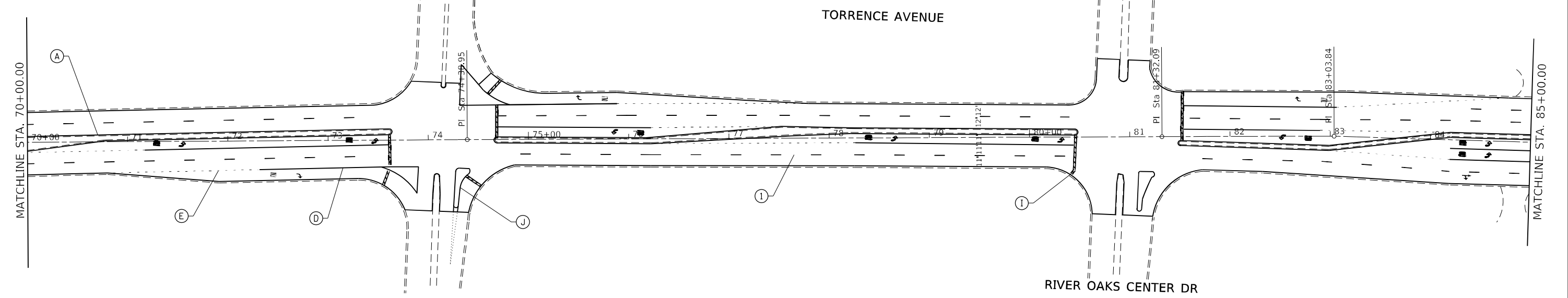
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PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED -
PLOT DATE = 11/18/2023	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY PLAN			
TORRENCE AVE (173RD ST TO HARDING AVE)			
SCALE: 1"=50'	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE. 0358	SECTION FAP 0358 22 RS	COUNTY COOK	TOTAL SHEETS 71	SHEET NO. 12
			CONTRACT NO. 62T58	
		ILLINOIS FED. AID PROJECT		

••NOTE: PREFORMED PAVEMENT MARKINGS TYPE D AND GROOVING FOR RECESSED PAVEMENT MARKINGS SHALL BE USED FOR ALL LANE LINE PAVEMENT MARKINGS WITHIN BRIDGE LIMITS AND PCC PAVEMENTS.
 ••NOTE: EXIST. CONC. MEDIAN SURFACE TO BE REMOVED AND REPLACED. LOCATIONS TO BE DETERMINED BY THE RESIDENT/TECHNICIAN. PAY AS CONCRETE MEDIAN, TYPE SB-6.12



PAVEMENT MARKING LEGEND

•• THERMOPLASTIC PAVEMENT MARKING -		•• MODIFIED URETHANE PAVEMENT MARKING -	
(A)	• LINE 4", YELLOW (TYP.)	(M)	•• LINE 4", DOUBLE YELLOW (TYP.)
(B)	• DOUBLE LINE 4", YELLOW (TYP.)	(N)	PROP. PREFORMED PAVEMENT MARKING TYPE D LINE 7" (1.5", BLACK, 4" WHITE, 1.5" BLACK) (10' DASH, 30' SKIP) (GROOVED)
(C)	• LINE, 4", 10' DASH-20' SKIP, WHITE	(O)	PROP. GROOVING FOR RECESSED PAVEMENT MARKING 8"
(D)	• TURN LANE MARKING-LINE 6", WHITE (TYP.)	(P)	•• TURN LANE MARKING-LINE 6", WHITE (TYP.)
(E)	• LINE 6", 2" SKIP, WHITE (TYP.)	(Q)	•• 6", 2" SKIP, WHITE (TYP.)
(F)	• MARKING-LINE 12", YELLOW (TYP.)	(R)	•• LINE 12", YELLOW (TYP.)
(G)	• LINE 12", WHITE (TYP.)	(S)	•• STOP LINE 24", WHITE (TYP.)
(H)	• LINE 12" SCHOOL CROSSWALK, WHITE (TYP.)	(I)	PROP. POLY HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80 1 3/4" PROP. HMA SURFACE REMOVAL 1 3/4" PROP. CLASS D PATCHES, 11"
(J)	• STOP LINE 24", WHITE (TYP.)		
(K)	• LINE 8", WHITE (TYP.)		
(L)	• LINE 6", CROSSWALK, WHITE (TYP.)		
(I)	• LINE 12" BICYCLE & EQUESTRIAN, WHITE (TYP.)		

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	DATE -	REVISED -

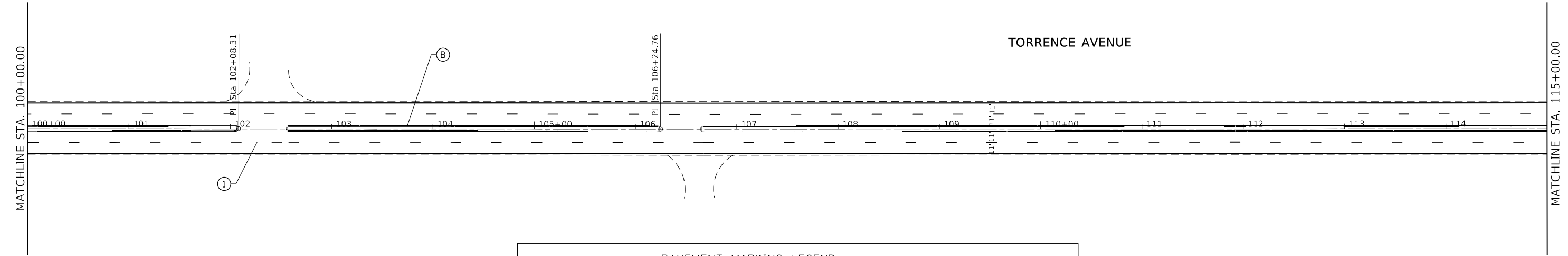
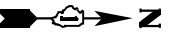
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY PLAN			
TORRENCE AVE (173RD ST TO HARDING AVE)			
SCALE: 1"=50'	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0358	FAP 0358 22 RS	COOK	71	13
CONTRACT NO. 62T58				
ILLINOIS FED. AID PROJECT				

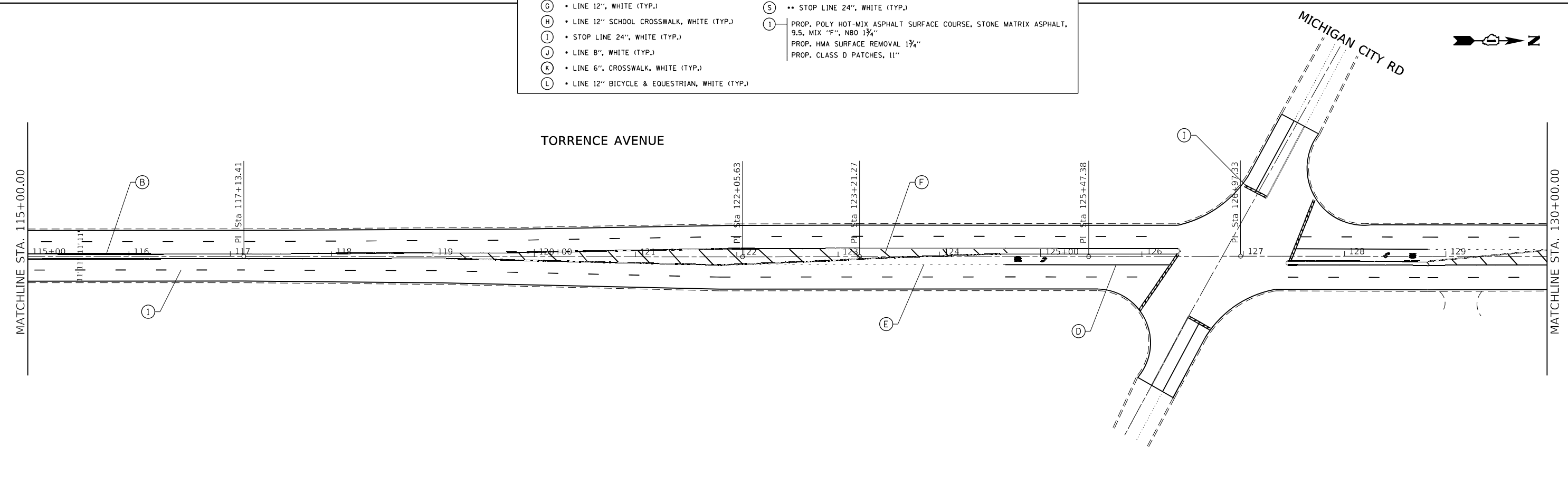
••NOTE: PREFORMED PAVEMENT MARKINGS TYPE D AND GROOVING FOR RECESSED PAVEMENT MARKINGS SHALL BE USED FOR ALL LANE LINE PAVEMENT MARKINGS WITHIN BRIDGE LIMITS AND PCC PAVEMENTS.

••NOTE: EXIST. CONC. MEDIAN SURFACE TO BE REMOVED AND REPLACED. LOCATIONS TO BE DETERMINED BY THE RESIDENT/TECHNICIAN. PAY AS CONCRETE MEDIAN, TYPE SB-6.12



PAVEMENT MARKING LEGEND

- = THERMOPLASTIC PAVEMENT MARKING -
- = MODIFIED URETHANE PAVEMENT MARKING -
- (A) • LINE 4", YELLOW (TYP.)
- (B) • DOUBLE LINE 4", YELLOW (TYP.)
- (C) • LINE, 4", 10' DASH-20' SKIP, WHITE
- (D) • TURN LANE MARKING-LINE 6", WHITE (TYP.)
- (E) • LINE 6", 2" SKIP, WHITE (TYP.)
- (F) • MARKING-LINE 12", YELLOW (TYP.)
- (G) • LINE 12", WHITE (TYP.)
- (H) • LINE 12" SCHOOL CROSSWALK, WHITE (TYP.)
- (I) • STOP LINE 24", WHITE (TYP.)
- (J) • LINE 8", WHITE (TYP.)
- (K) • LINE 6", CROSSWALK, WHITE (TYP.)
- (L) • LINE 12" BICYCLE & EQUESTRIAN, WHITE (TYP.)
- (M) •• LINE 4", DOUBLE YELLOW (TYP.)
- (N) PROP. PREFORMED PAVEMENT MARKING TYPE D LINE 7" (1.5", BLACK, 4" WHITE, 1.5" BLACK) (10' DASH, 30' SKIP) (GROOVED)
- (O) PROP. GROOVING FOR RECESSED PAVEMENT MARKING 8"
- (P) •• TURN LANE MARKING-LINE 6", WHITE (TYP.)
- (Q) •• 6", 2" SKIP, WHITE (TYP.)
- (R) •• LINE 12", YELLOW (TYP.)
- (S) •• STOP LINE 24", WHITE (TYP.)
- (I) PROP. POLY HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80 1 3/4"
- PROP. HMA SURFACE REMOVAL 1 3/4"
- PROP. CLASS D PATCHES, 11"



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DRAWN -	REVISED -	
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 11/18/2023	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

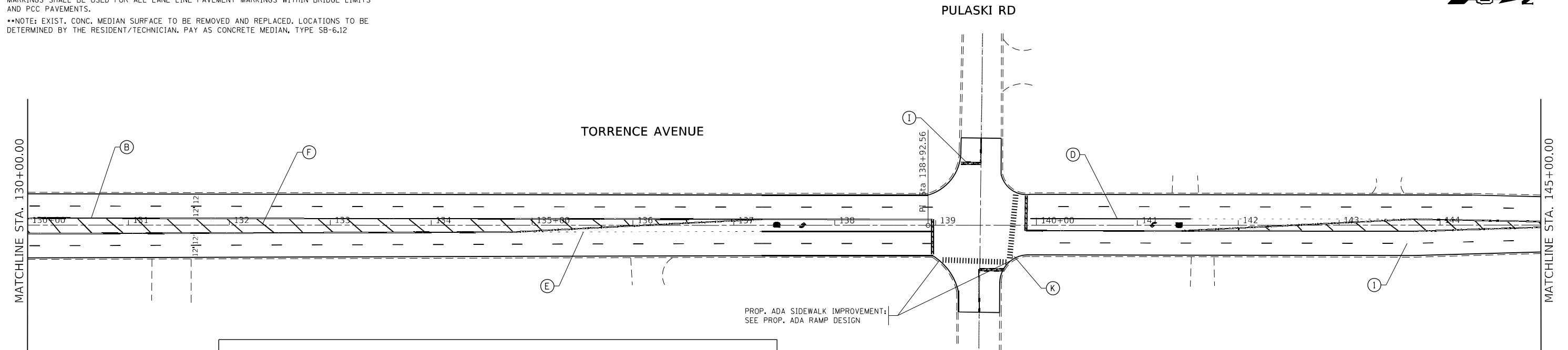
**ROADWAY PLAN
TORRENCE AVE (173RD ST TO HARDING AVE)**

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0358	FAP 0358 22 RS	COOK	71	14
CONTRACT NO. 62T58			ILLINOIS FED. AID PROJECT	

••NOTE: PREFORMED PAVEMENT MARKINGS TYPE D AND GROOVING FOR RECESSED PAVEMENT MARKINGS SHALL BE USED FOR ALL LANE LINE PAVEMENT MARKINGS WITHIN BRIDGE LIMITS AND PCC PAVEMENTS.

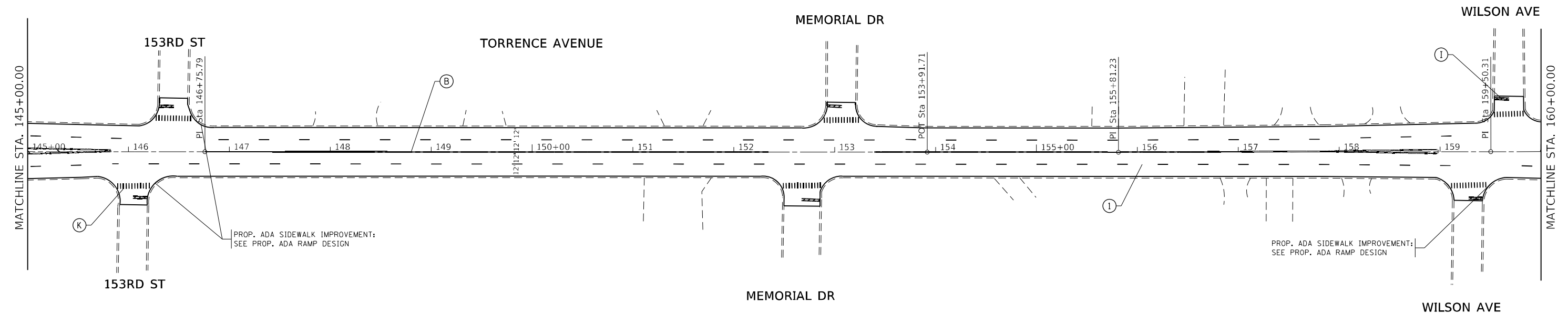
••NOTE: EXIST. CONC. MEDIAN SURFACE TO BE REMOVED AND REPLACED. LOCATIONS TO BE DETERMINED BY THE RESIDENT/TECHNICIAN. PAY AS CONCRETE MEDIAN, TYPE SB-6.12



PAVEMENT MARKING LEGEND

- THERMOPLASTIC PAVEMENT MARKING -
- MODIFIED URETHANE PAVEMENT MARKING -
- (A) • LINE 4", YELLOW (TYP.)
- (B) • DOUBLE LINE 4", YELLOW (TYP.)
- (C) • LINE, 4", 10' DASH-20' SKIP, WHITE
- (D) • TURN LANE MARKING-LINE 6", WHITE (TYP.)
- (E) • LINE 6", 2" SKIP, WHITE (TYP.)
- (F) • MARKING-LINE 12", YELLOW (TYP.)
- (G) • LINE 12", WHITE (TYP.)
- (H) • LINE 12" SCHOOL CROSSWALK, WHITE (TYP.)
- (I) • STOP LINE 24", WHITE (TYP.)
- (J) • LINE 8", WHITE (TYP.)
- (K) • LINE 6", CROSSWALK, WHITE (TYP.)
- (L) • LINE 12" BICYCLE & EQUESTRIAN, WHITE (TYP.)
- (M) •• LINE 4", DOUBLE YELLOW (TYP.)
- (N) PROP. PREFORMED PAVEMENT MARKING TYPE D LINE 7" (1.5", BLACK, 4" WHITE, 1.5" BLACK) (10' DASH, 30' SKIP) (GROOVED)
- (O) PROP. GROOVING FOR RECESSED PAVEMENT MARKING 8"
- (P) •• TURN LANE MARKING-LINE 6", WHITE (TYP.)
- (Q) •• 6", 2" DASH - 6' SKIP, WHITE (TYP.)
- (R) •• LINE 12", YELLOW (TYP.)
- (S) •• STOP LINE 24", WHITE (TYP.)
- (I) PROP. POLY HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80 1 3/4"
- PROP. HMA SURFACE REMOVAL 1 3/4"
- PROP. CLASS D PATCHES, 11"

PROP. ADA SIDEWALK IMPROVEMENT:
SEE PROP. ADA RAMP DESIGN



PROP. ADA SIDEWALK IMPROVEMENT:
SEE PROP. ADA RAMP DESIGN

PROP. ADA SIDEWALK IMPROVEMENT:
SEE PROP. ADA RAMP DESIGN

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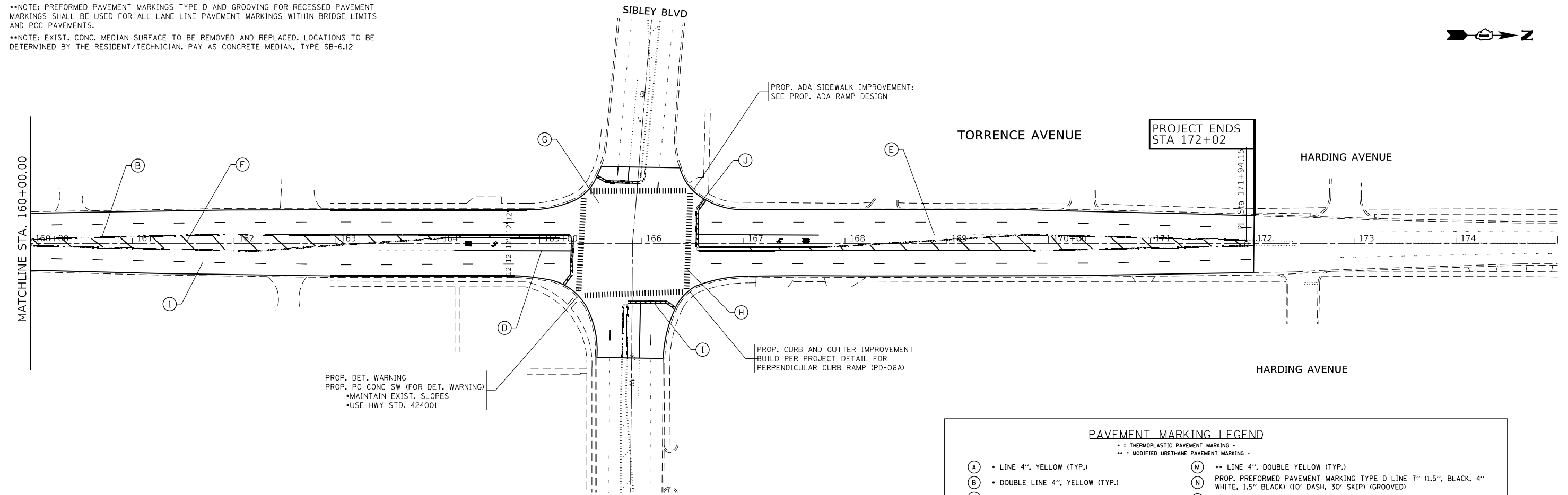
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY PLAN			
TORRENCE AVE (173RD ST TO HARDING AVE)			
SCALE: 1"=50'	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0358	FAP 0358 22 RS	COOK	71	15
CONTRACT NO. 62T58				
ILLINOIS FED. AID PROJECT				

••NOTE: PREFORMED PAVEMENT MARKINGS TYPE D AND GROOVING FOR RECESSED PAVEMENT MARKINGS SHALL BE USED FOR ALL LANE LINE PAVEMENT MARKINGS WITHIN BRIDGE LIMITS AND PCC PAVEMENTS.

••NOTE: EXIST. CONC. MEDIAN SURFACE TO BE REMOVED AND REPLACED. LOCATIONS TO BE DETERMINED BY THE RESIDENT/TECHNICIAN. PAY AS CONCRETE MEDIAN, TYPE SB-6.12



PROP. DET. WARNING
 PROP. PC CONC SW (FOR DET. WARNING)
 •MAINTAIN EXIST. SLOPES
 •USE HWY STD. 424001

PROP. ADA SIDEWALK IMPROVEMENT:
 SEE PROP. ADA RAMP DESIGN

PROP. CURB AND GUTTER IMPROVEMENT
 BUILD PER PROJECT DETAIL FOR
 PERPENDICULAR CURB RAMP (PD-06A)

PAVEMENT MARKING LEGEND

- = THERMOPLASTIC PAVEMENT MARKING
- = MODIFIED URETHANE PAVEMENT MARKING
- (A) • LINE 4", YELLOW (TYP.)
- (B) • DOUBLE LINE 4", YELLOW (TYP.)
- (C) • LINE, 4", 10' DASH-20' SKIP, WHITE
- (D) • TURN LANE MARKING-LINE 6", WHITE (TYP.)
- (E) • LINE 6", 2" SKIP, WHITE (TYP.)
- (F) • MARKING-LINE 12", YELLOW (TYP.)
- (G) • LINE 12", WHITE (TYP.)
- (H) • LINE 12" SCHOOL CROSSWALK, WHITE (TYP.)
- (I) • STOP LINE 24", WHITE (TYP.)
- (J) • LINE 8", WHITE (TYP.)
- (K) • LINE 6", CROSSWALK, WHITE (TYP.)
- (L) • LINE 12" BICYCLE & EQUESTRIAN, WHITE (TYP.)
- (M) •• LINE 4", DOUBLE YELLOW (TYP.)
- (N) PROP. PREFORMED PAVEMENT MARKING TYPE D LINE 7" (1.5", BLACK, 4" WHITE, 1.5" BLACK) (10' DASH, 30' SKIP) (GROOVED)
- (O) PROP. GROOVING FOR RECESSED PAVEMENT MARKING 8"
- (P) •• TURN LANE MARKING-LINE 6", WHITE (TYP.)
- (Q) •• 6", 2" SKIP, WHITE (TYP.)
- (R) •• LINE 12", YELLOW (TYP.)
- (S) •• STOP LINE 24", WHITE (TYP.)
- (T) PROP. POLY HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80 1 3/4"
- (U) PROP. HMA SURFACE REMOVAL 1 3/4"
- (V) PROP. CLASS D PATCHES, 11"

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	DRAWN -	REVISED -
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PLOT DATE = 11/18/2023	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN
 TORRENCE AVE (173RD ST TO HARDING AVE)**

SCALE: 1"=50'

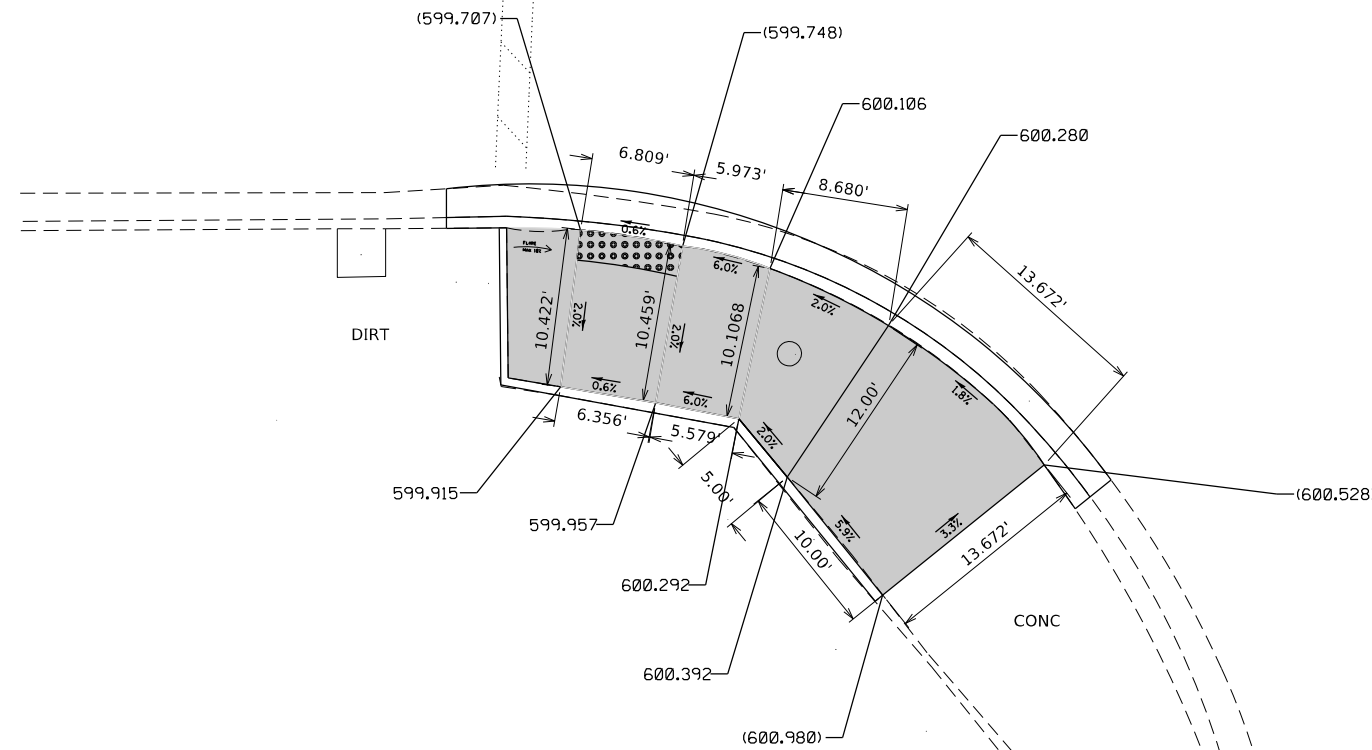
SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 0358	SECTION FAP 0358 22 RS	COUNTY COOK	TOTAL SHEETS 71	SHEET NO. 16
				CONTRACT NO. 62T58
ILLINOIS FED. AID PROJECT				

173RD ST.



TORRENCE AVE.



DESIGNER NOTES

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
- 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
- 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
- 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

BENCHMARK: CUT SQUARE ON S.W. CORNER ON TRAFFIC SIGNAL CONTROL BOX AT SE CORNER OF 173RD ST AND TORRENCE AVE EL. 600.881'

LEGEND

- PROPOSED SIDE CURB
- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- 2) SEE ADA SCHEDULE FOR QUANTITIES
- * MATCH EXISTING SIDEWALK WIDTH

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	DRAWN -	REVISED -
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PLOT DATE = 10/20/2023	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

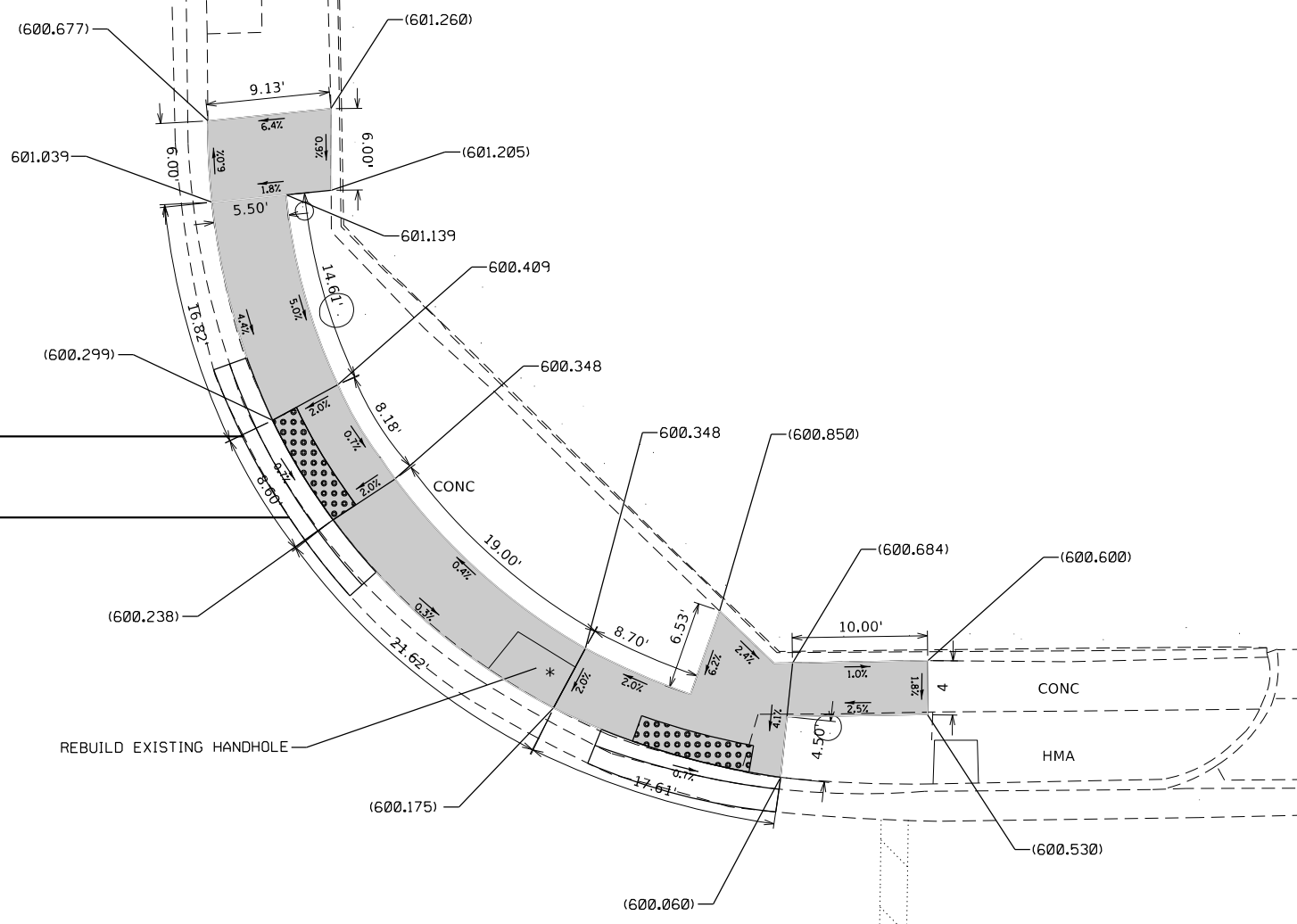
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SOUTH WEST CORNER OF TORRENCE AVE & 173RD ST			
SCALE:	SHEET	OF	SHEETS
	STA.	TO	STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	FAP 0358 22 RS	COOK	71	17
CONTRACT NO. 62T58				
ILLINOIS FED. AID PROJECT				

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TORRENCE AVE.

173RD ST.



DESIGNER NOTES

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
- 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
- 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
- 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

BENCHMARK: CUT SQUARE ON S.W. CORNER ON TRAFFIC SIGNAL CONTROL BOX AT SE CORNER OF 173RD ST AND TORRENCE AVE EL. 600.881'

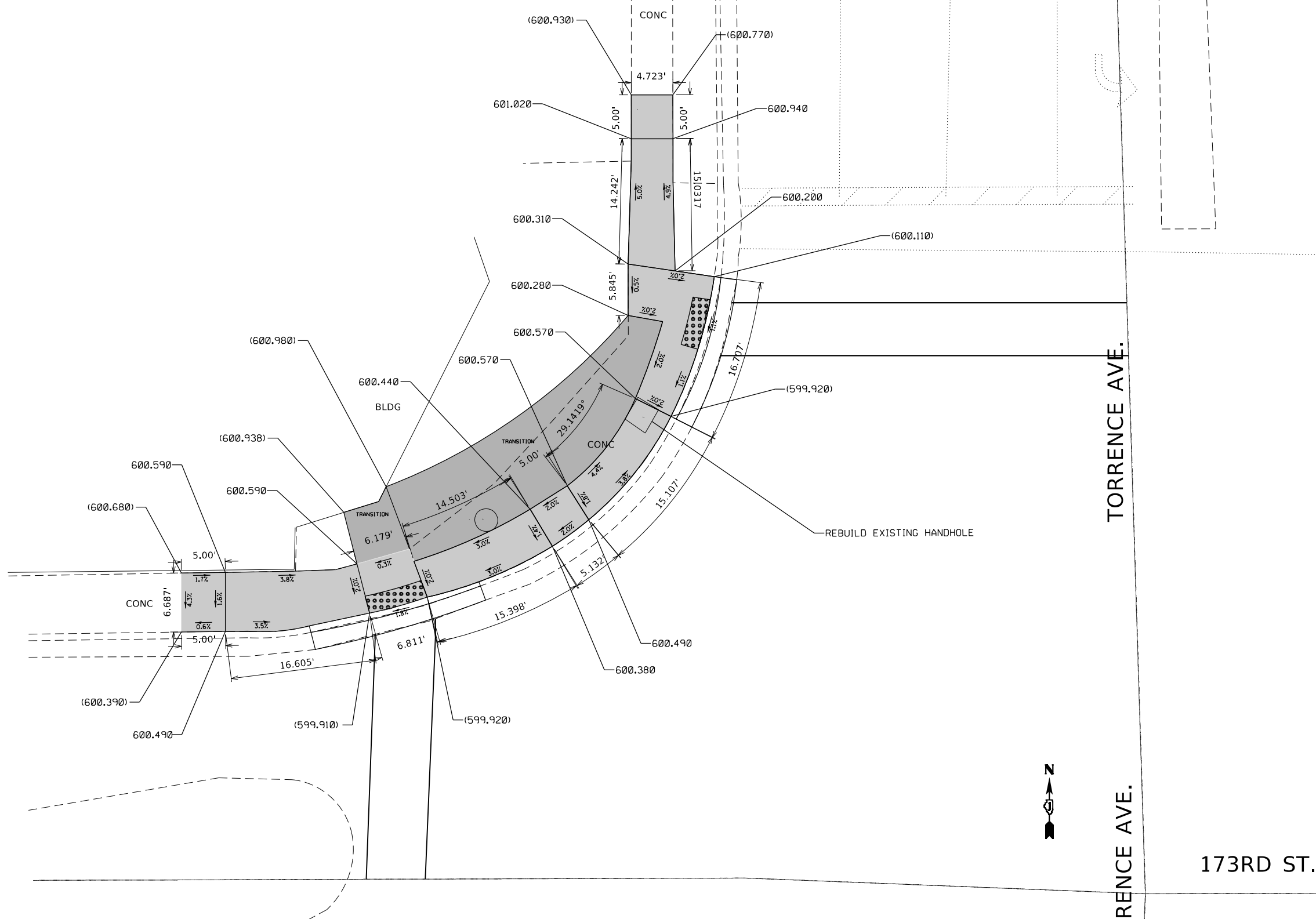
LEGEND

- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- PROPOSED SIDE CURB

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- 2) SEE ADA SCHEDULE FOR QUANTITIES
- * MATCH EXISTING SIDEWALK WIDTH

	USER NAME = Sher,Khan	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED ADA RAMP DESIGN NORTH EAST CORNER OF TORRENCE AVE & 173RD ST			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT DATE = 10/20/2023	DATE -	REVISED -						CONTRACT NO. 62T58			
								ILLINOIS FED. AID PROJECT				



DESIGNER NOTES

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
- 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
- 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
- 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

BENCHMARK: CUT SQUARE ON S.W. CORNER ON TRAFFIC SIGNAL CONTROL BOX AT SE CORNER OF 173RD ST AND TORRENCE AVE EL. 600.881'

LEGEND

- PROPOSED SIDE CURB
- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- 2) SEE ADA SCHEDULE FOR QUANTITIES
- * MATCH EXISTING SIDEWALK WIDTH

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PLOT SCALE = 12,6500' / in.	DRAWN -	REVISED -
PLOT DATE = 10/20/2023	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

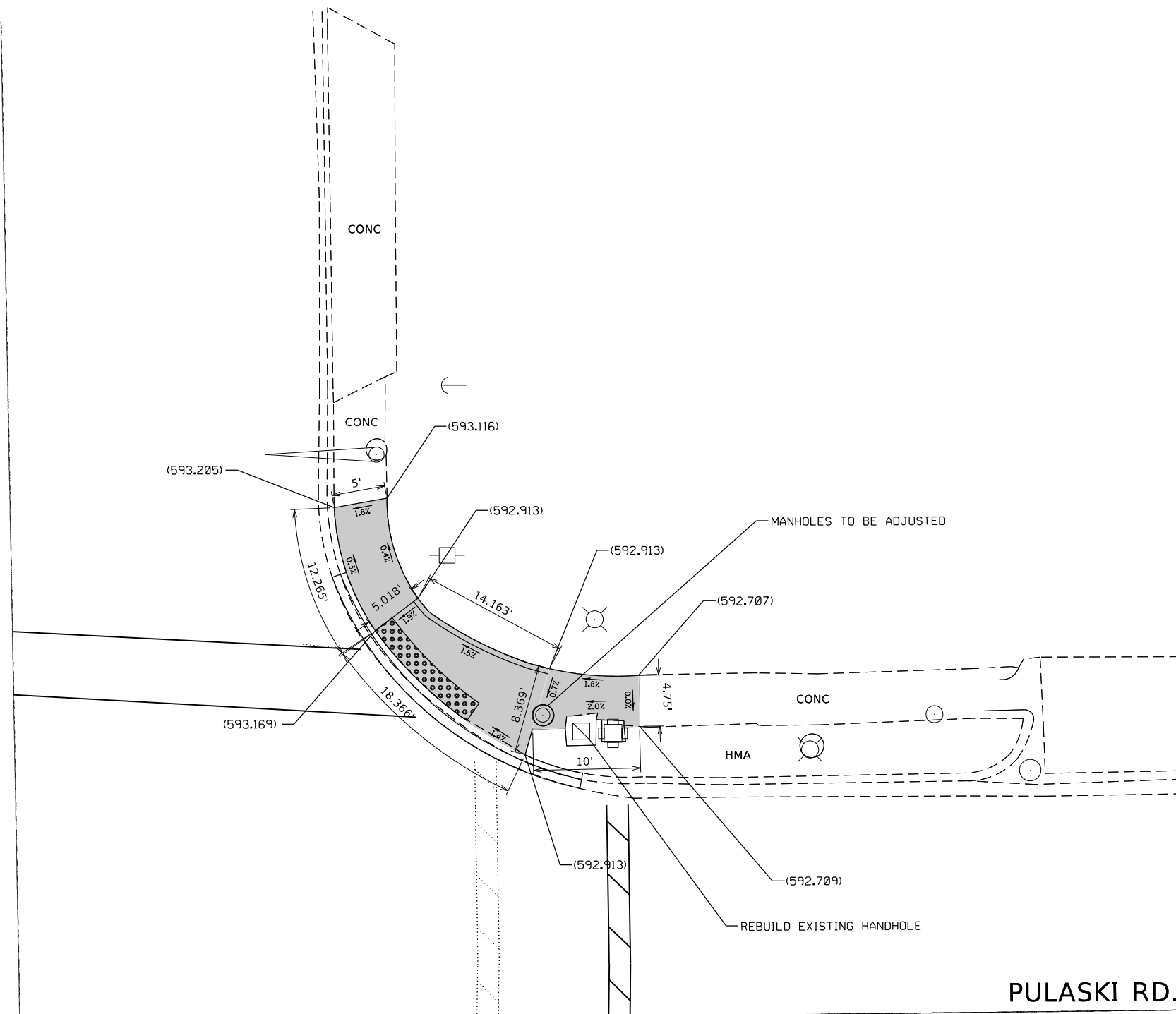
PROPOSED ADA RAMP DESIGN
NORTH WEST CORNER OF TORRENCE AVE & 173RD ST

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	FAP 0358 22 RS	COOK	71	19
			CONTRACT NO. 62T58	
		ILLINOIS FED. AID PROJECT		







TORRENCE AVE.



- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
- 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
- 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
- 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

BENCHMARK: N.W. CORNER ON THE X OF THE TRAFFIC SIGNAL HANDHOLE EL. 596.330'

LEGEND

-  PROPOSED SIDE CURB
-  EXIST. GRASS
-  PROPOSED SIDEWALK
-  DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
 - 2) SEE ADA SCHEDULE FOR QUANTITIES
- * MATCH EXISTING SIDEWALK WIDTH

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PLOT DATE = 10/20/2023	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PROPOSED ADA RAMP DESIGN
SOUTH WEST CORNER OF TORRENCE AVE & 173RD ST

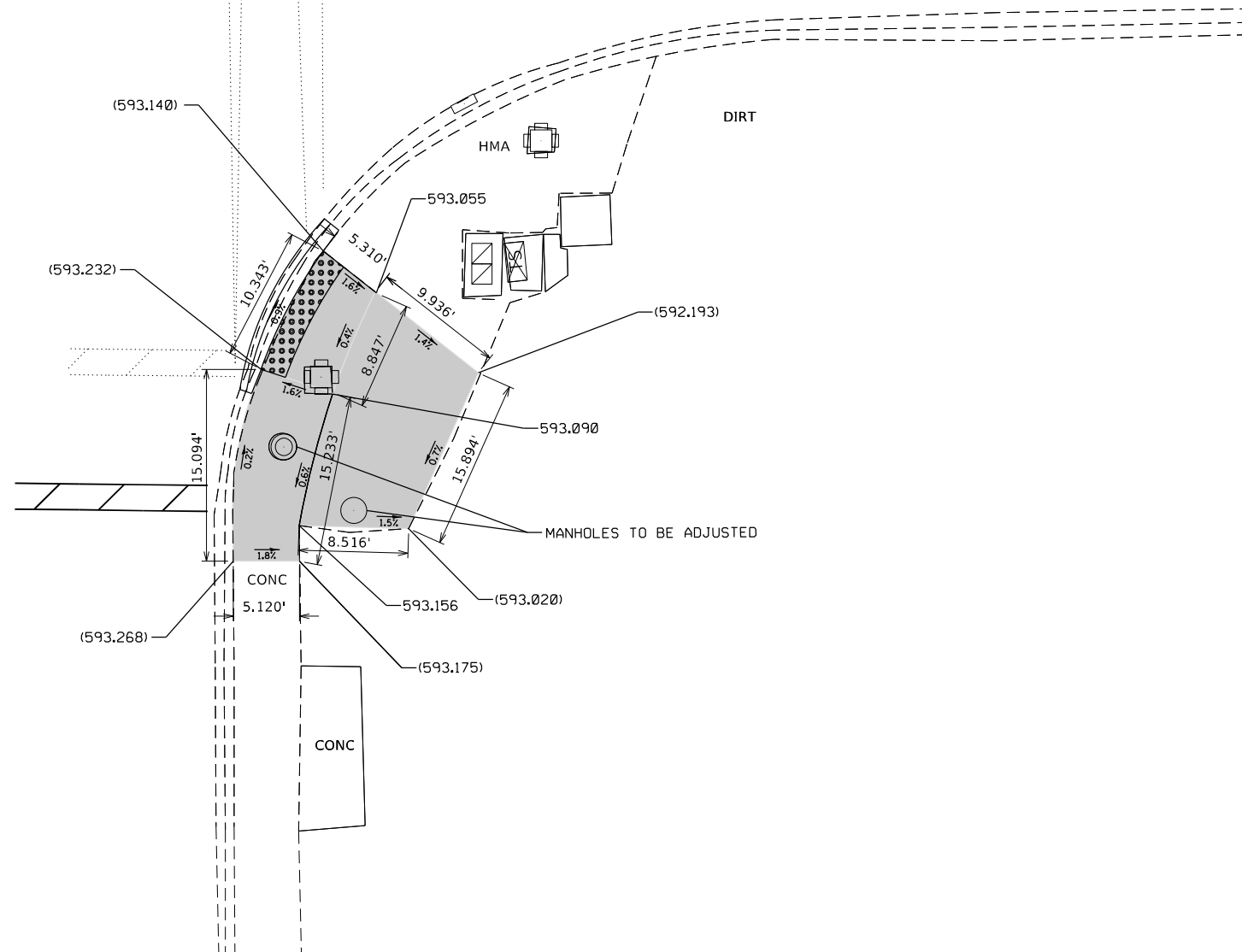
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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			CONTRACT NO. 62T58	
		ILLINOIS	FED. AID PROJECT	



TORRENCE AVE.





PULASKI RD.



DESIGNER NOTES

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
- 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
- 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
- 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

LEGEND

-  PROPOSED SIDE CURB
-  EXIST. GRASS
-  PROPOSED SIDEWALK
-  DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
 - 2) SEE ADA SCHEDULE FOR QUANTITIES
- * MATCH EXISTING SIDEWALK WIDTH

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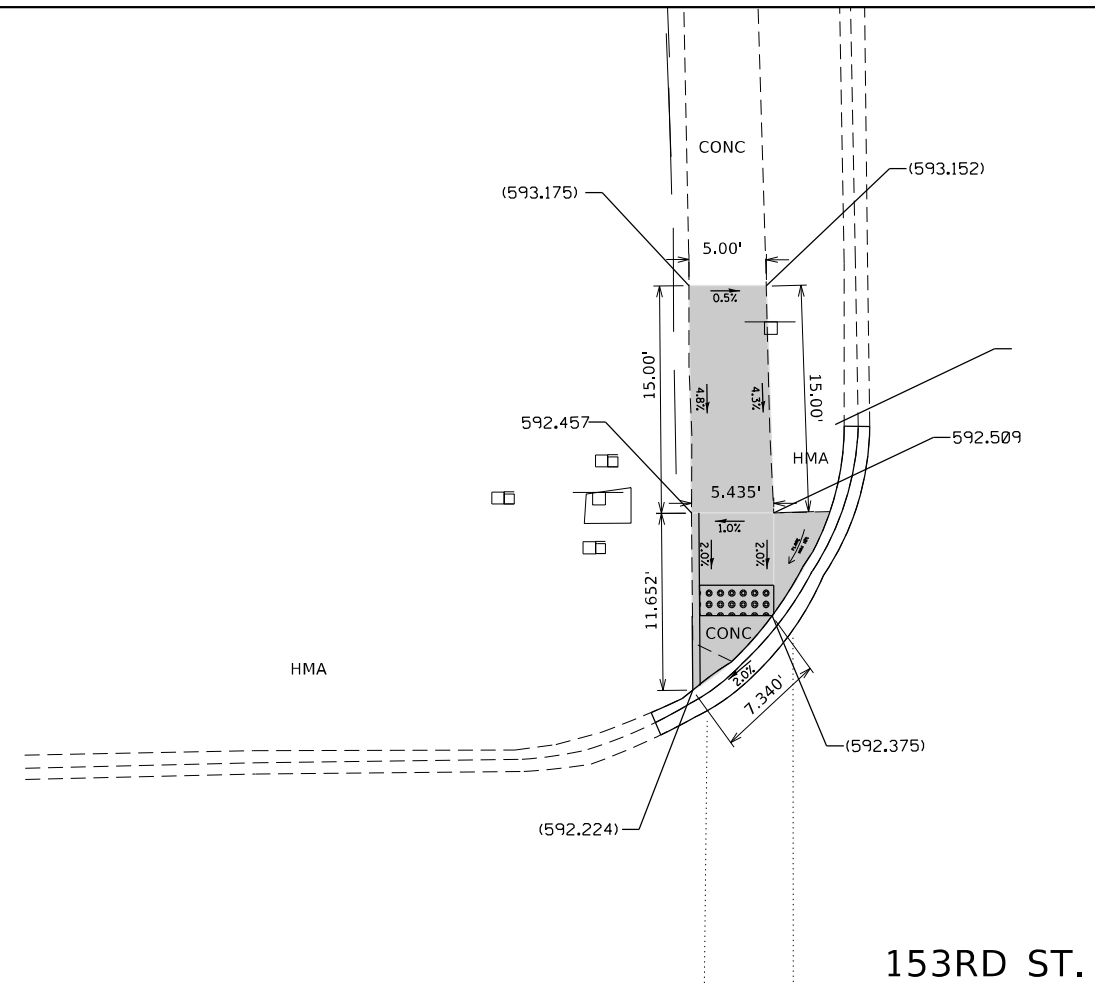
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	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED ADA RAMP DESIGN
SOUTH EAST CORNER OF TORRENCE AVE & PULASKI RD

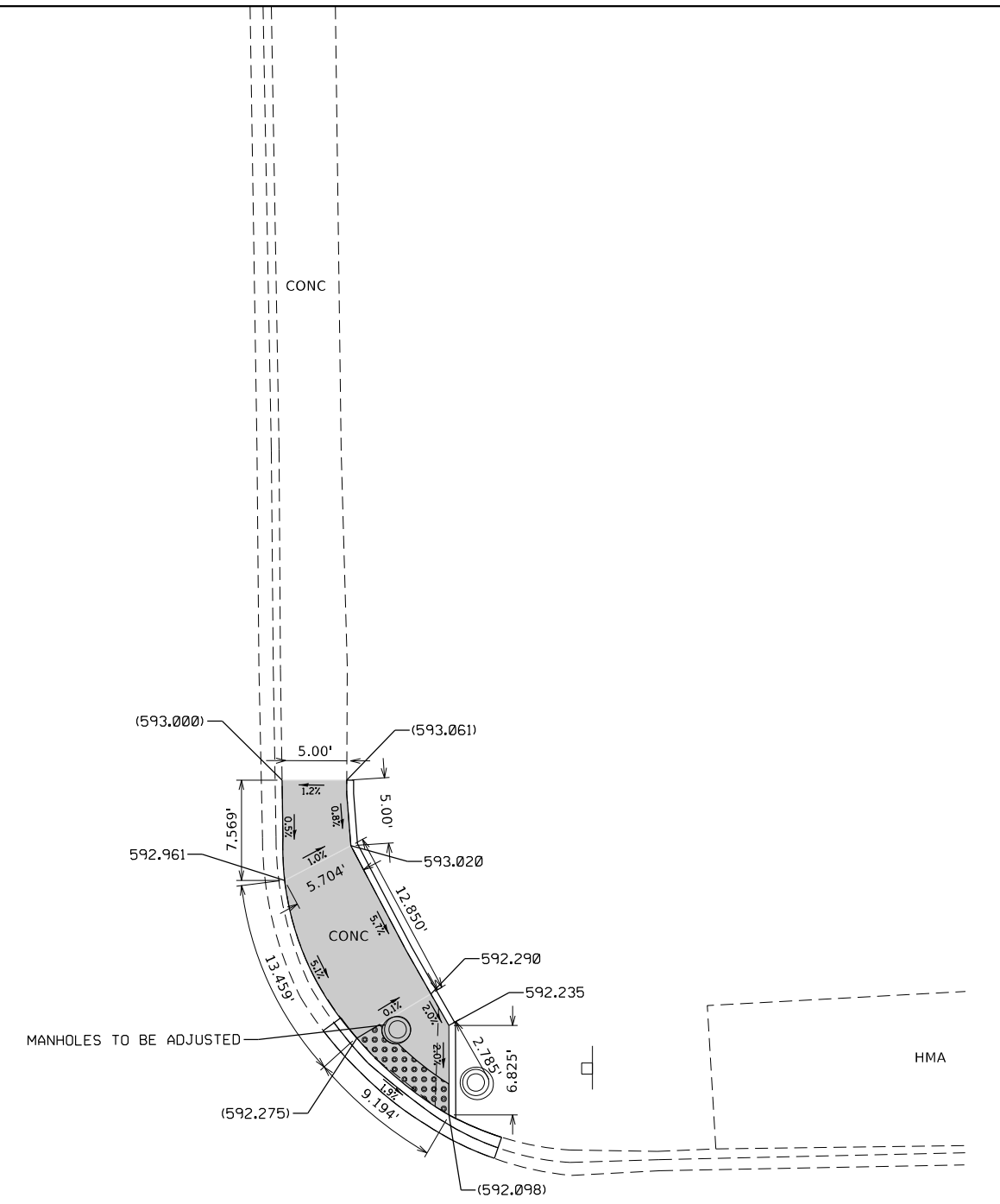
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	FAP 0358 22 RS	COOK	71	21
			CONTRACT NO. 62T58	
		ILLINOIS	FED. AID PROJECT	



TORRENCE AVE.

153RD ST.



153RD ST.

DESIGNER NOTES

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
- 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
- 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
- 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

BENCHMARK: S.W CORNER AT THE ATT&T PAD EL. 591.287'

LEGEND

- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- PROPOSED SIDE CURB

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
 - 2) SEE ADA SCHEDULE FOR QUANTITIES
- * MATCH EXISTING SIDEWALK WIDTH

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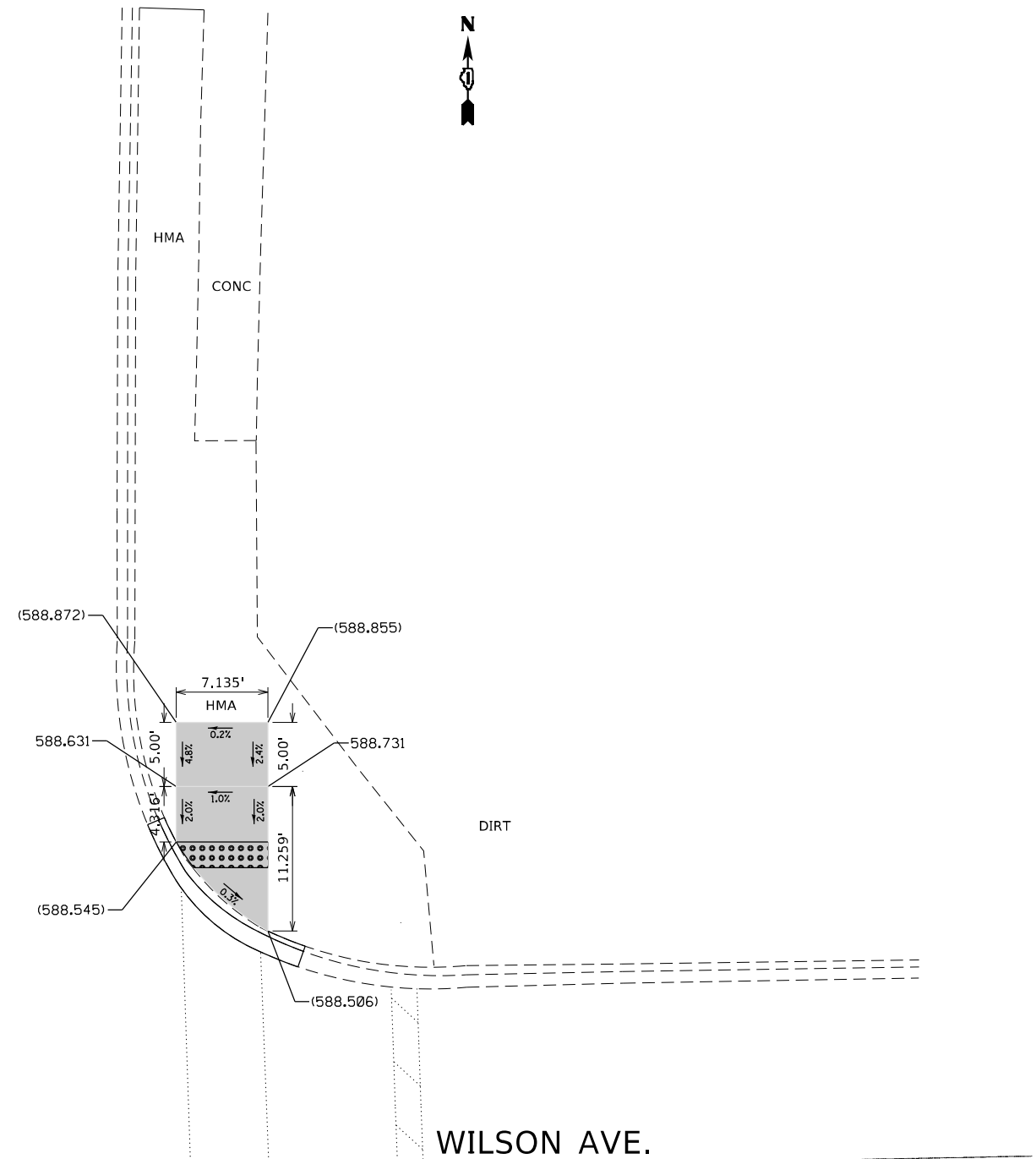
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PROPOSED ADA RAMP DESIGN			
NORTH WEST AND EAST CORNER OF TORRENCE AVE & 153RD ST			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	FAP 0358 22 RS	COOK	71	22
			CONTRACT NO. 62T58	
		ILLINOIS FED. AID PROJECT		

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TORRENCE AVE.



DESIGNER NOTES

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
- 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
- 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
- 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

BENCHMARK: N.E CORNER OF THE NORTH BOLT AT THE HANDHOLE EL. 590.360'

LEGEND

- PROPOSED SIDE CURB
- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
 - 2) SEE ADA SCHEDULE FOR QUANTITIES
- * MATCH EXISTING SIDEWALK WIDTH

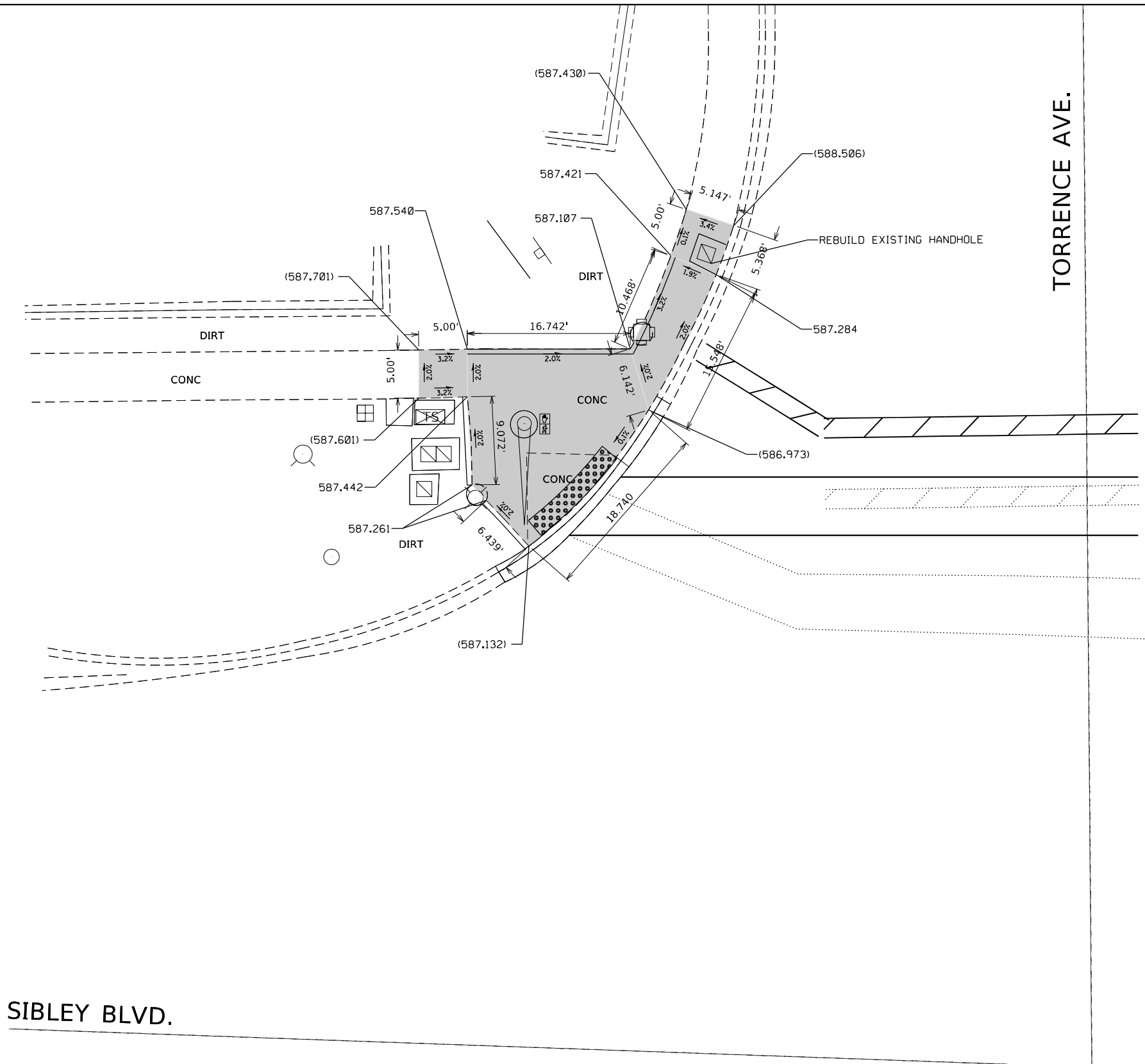
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PROPOSED ADA RAMP DESIGN
NORTH EAST CORNER OF TORRENCE AVE & WILSON AVE

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	FAP 0358 22 RS	COOK	71	23
			CONTRACT NO. 62T58	
		ILLINOIS	FED. AID PROJECT	



SIBLEY BLVD.

TORRENCE AVE.

DESIGNER NOTES

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
- 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
- 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
- 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

BENCHMARK: N.E CORNER AT THE ON THE EAST FACE OF THE LIGHTPOLE EL. 587.299

LEGEND

- PROPOSED SIDE CURB
- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- 2) SEE ADA SCHEDULE FOR QUANTITIES
- * MATCH EXISTING SIDEWALK WIDTH

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PLOT DATE = 10/20/2023	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

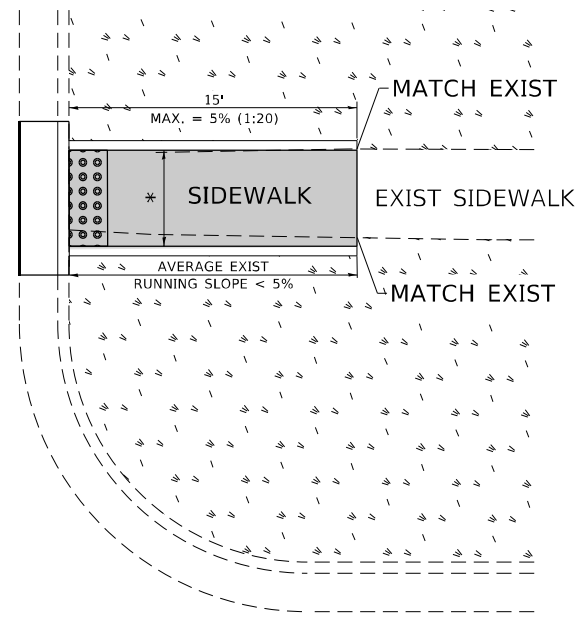
PROPOSED ADA RAMP DESIGN
NORTH WEST CORNER OF TORRENCE AVE & SIBLEY BLVD

SCALE: SHEET OF SHEETS STA. TO STA.

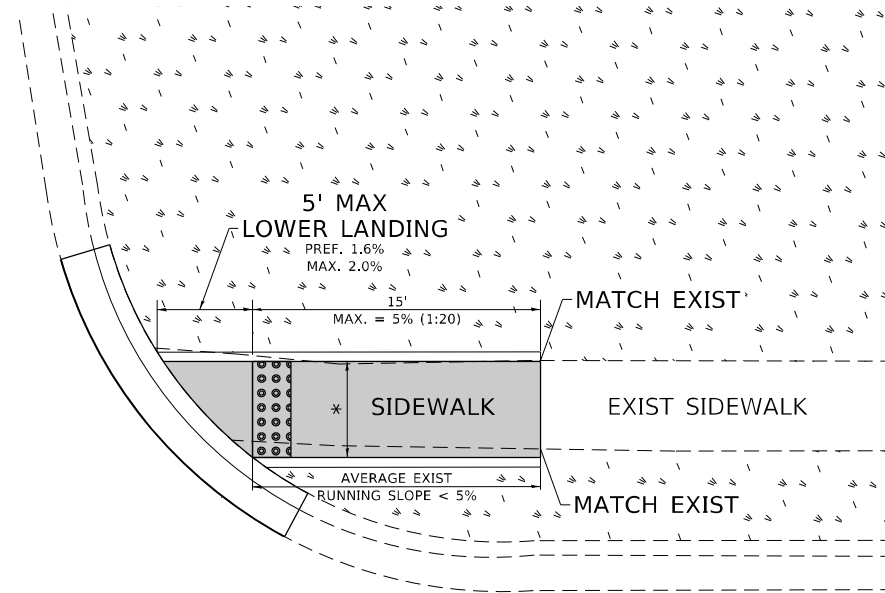
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	FAP 0358 22 RS	COOK	71	24
			CONTRACT NO. 62T58	
		ILLINOIS FED. AID PROJECT		

ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ EXIST. 5% OR LESS RUN. SLOPE

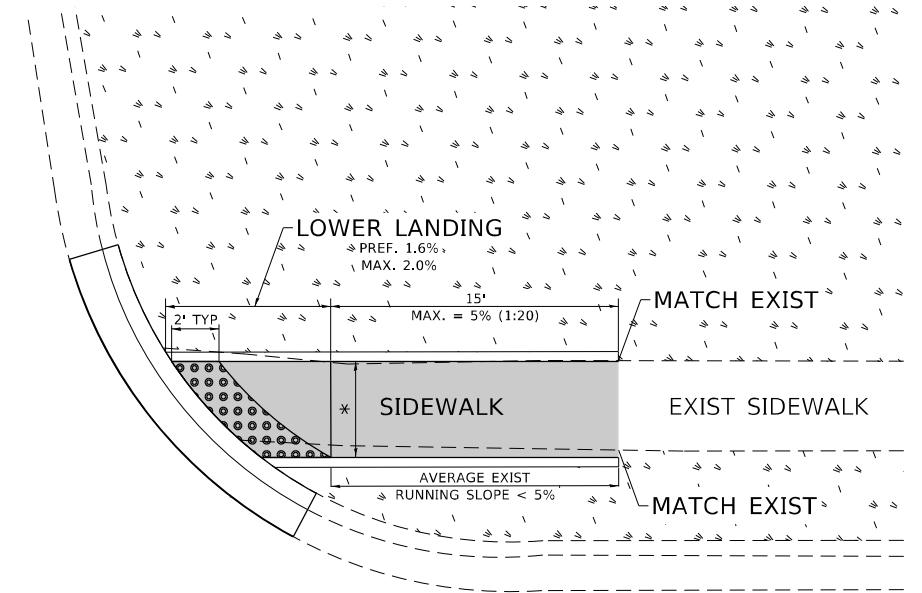
PD-01A



PD-01B



PD-01C



LEGEND

- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- PROPOSED SIDE CURB

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

MODEL: Default
FILE NAME: \\ultra-cw-beadley.com\PI\DOT\Documents\DOT Office\District 11\Project\1125922\CADD\DATA\Design\BRTStu.dgn

USER NAME = Sher,Khan	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/20/2023	DATE - 10/02/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

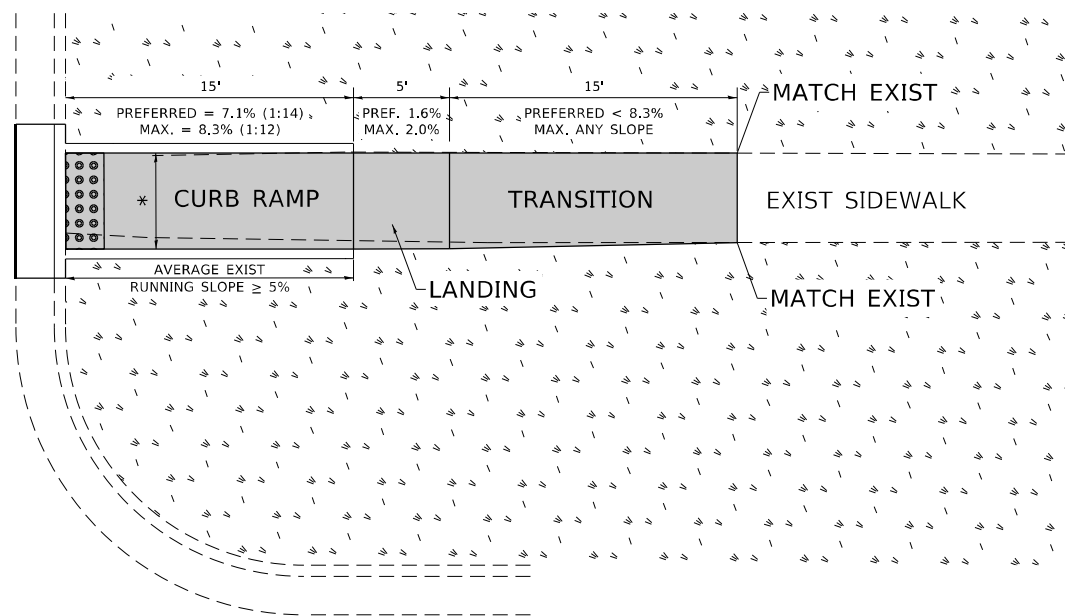
**PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS
(PD-01)**

SCALE: NONE SHEET OF SHEETS STA. TO STA.

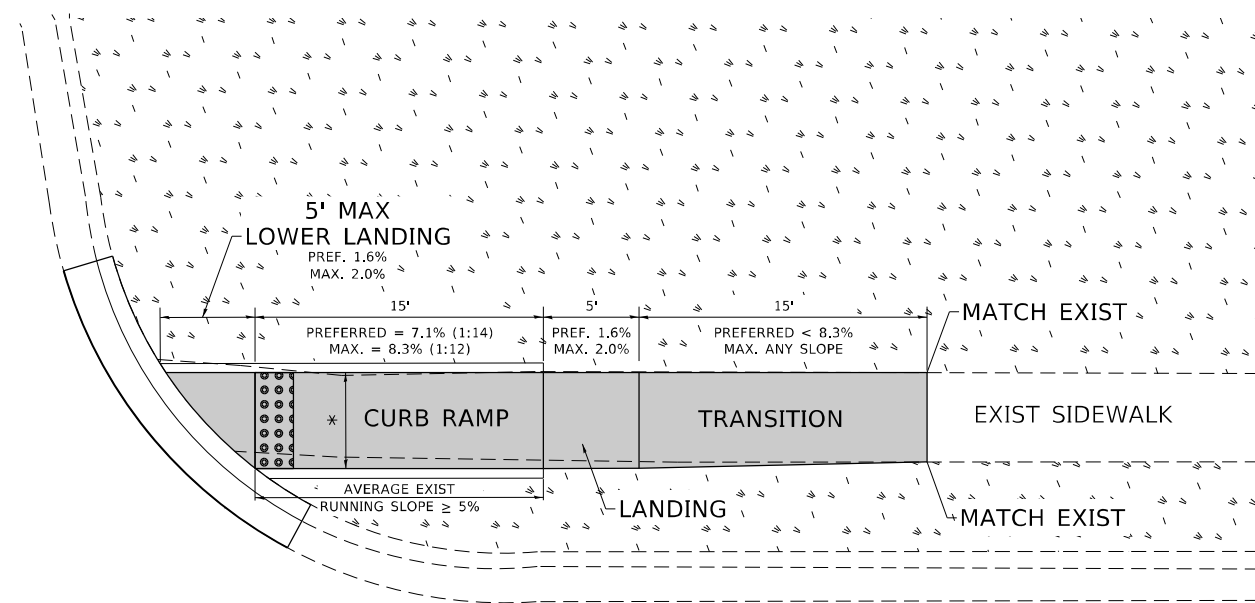
F.A.P. RTE. 0358	SECTION FAP 0358 22 RS PD-01	COUNTY COOK	TOTAL SHEETS 71	SHEET NO. 25
ILLINOIS			FED. AID PROJECT	
			CONTRACT NO. 62T58	

ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ EXIST. 5% OR GREATER RUN. SLOPE

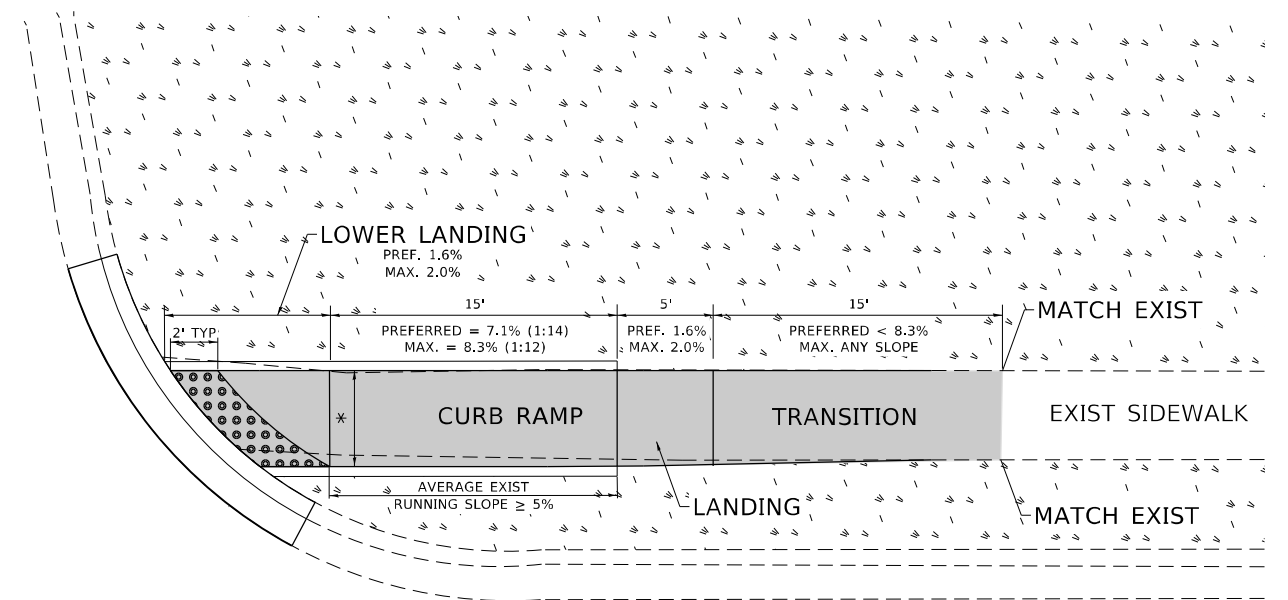
PD-02A



PD-02B



PD-02C



LEGEND

- EXIST. GRASS
- PROPOSED SIDEWALK
- PROPOSED SIDE CURB
- DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

MODEL: Default
FILE NAME: \\sldetaw.bentley.com\FW\DOT\Documents\DOT Office\Dir\drt_1\Project\1125922\CADD\Drawings\Drawings\PD-02.dgn

USER NAME = Sher,Khan	DESIGNED -	REVISED -
PLOT SCALE = 100,0000' / in.	DRAWN - R. LEDEZMA	REVISED -
PLOT DATE = 10/20/2023	CHECKED -	REVISED -
	DATE - 10/02/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

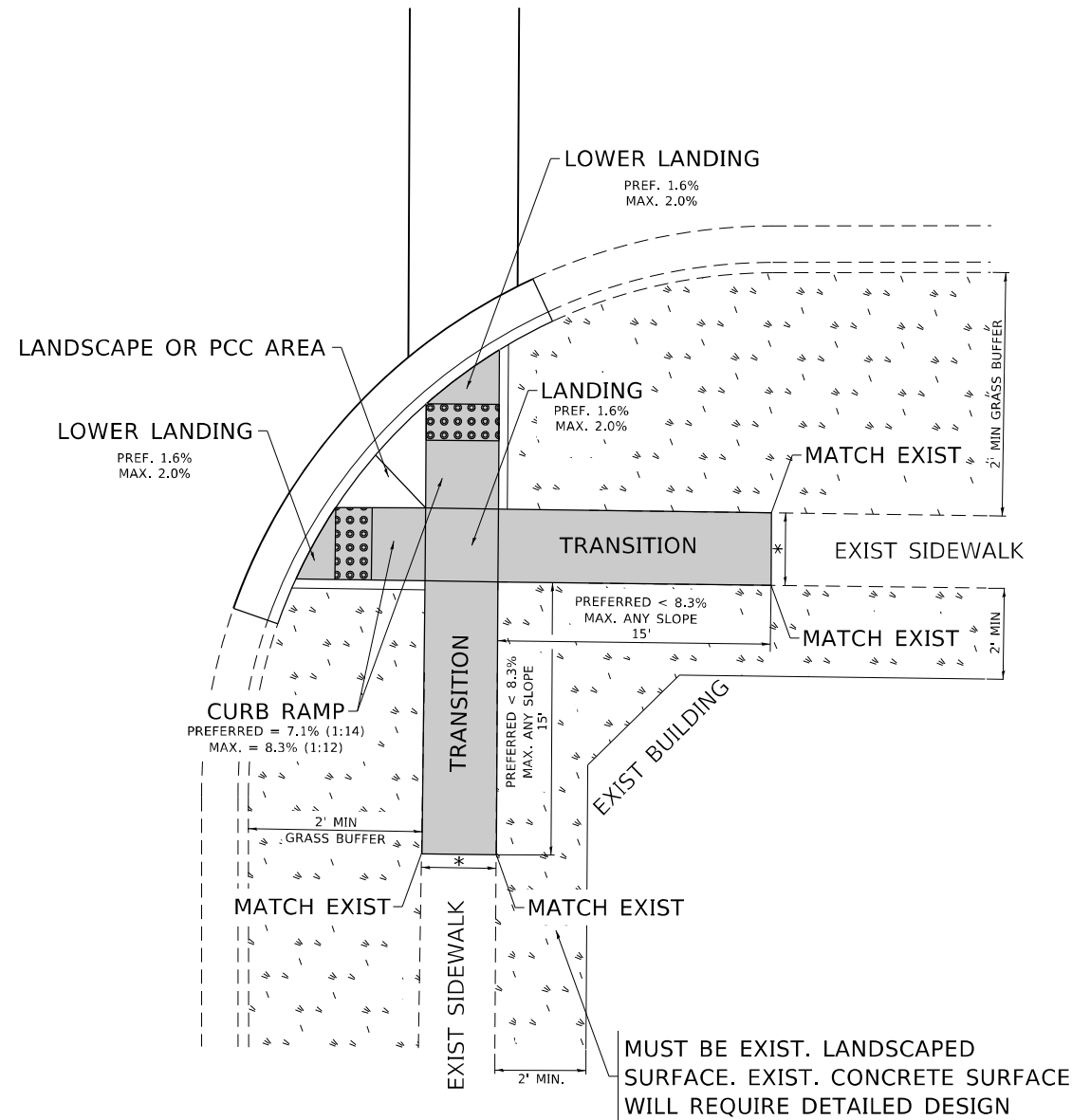
**PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS
(PD-02)**

SCALE: NONE SHEET OF SHEETS STA. TO STA.

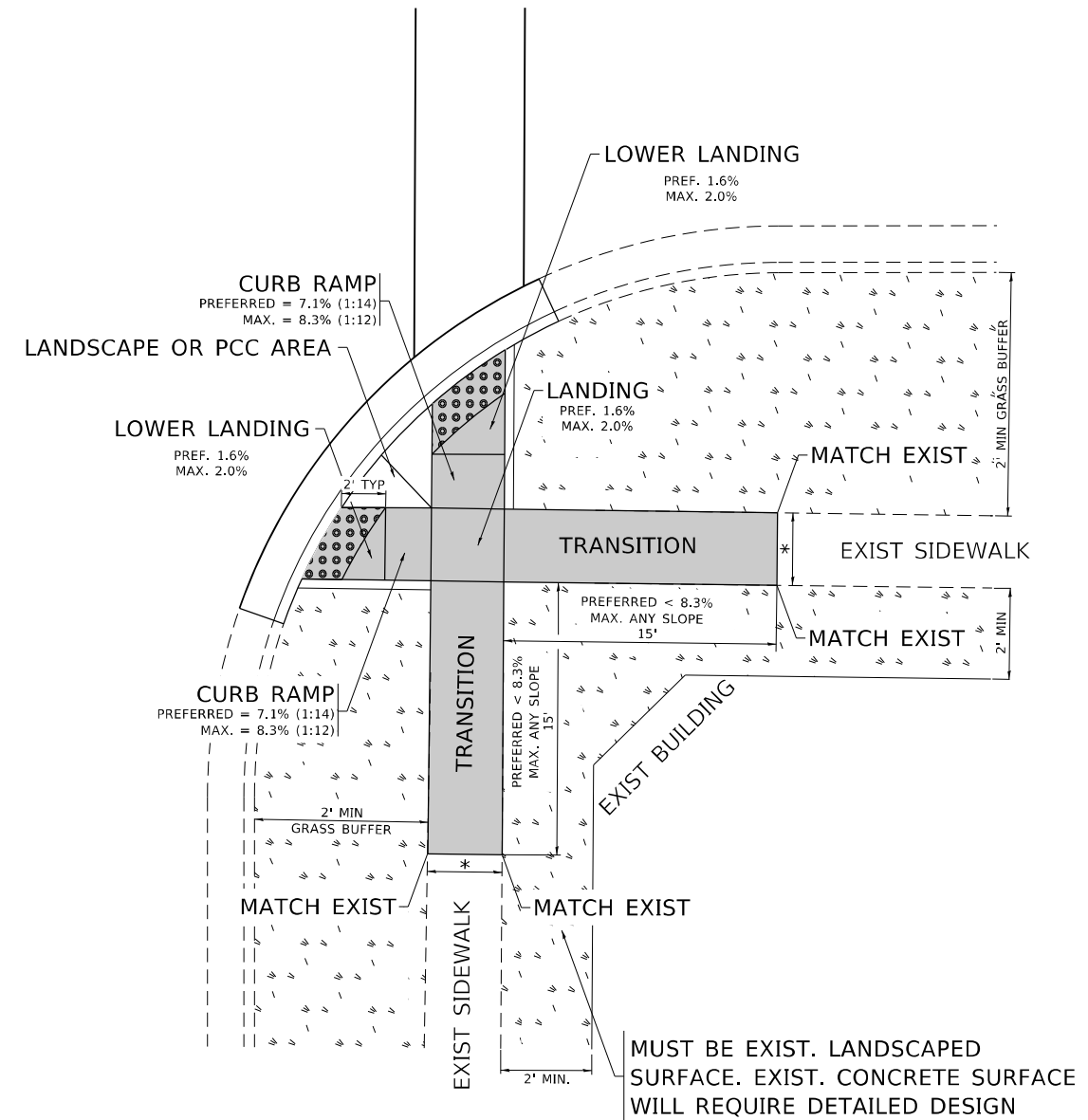
F.A.P. RTE. 0358	SECTION FAP 0358 22 RS	COUNTY COOK	TOTAL SHEETS 71	SHEET NO. 26
PD-02		CONTRACT NO. 62T58		
ILLINOIS FED. AID PROJECT				

ADA DETAIL FOR DOUBLE PERPENDICULAR CURB RAMPS

PD-03A



PD-03B



LEGEND

- PROPOSED SIDE CURB
- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

MODEL: Default
FILE NAME: p:\project-aw-beadley.com\PI\DOT\Documents\DOT Office\District 11\Project\1125922\CADD\Drawings\Design\BRTS\03A.dgn

USER NAME = Sher,Khan	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/20/2023	DATE - 10/02/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

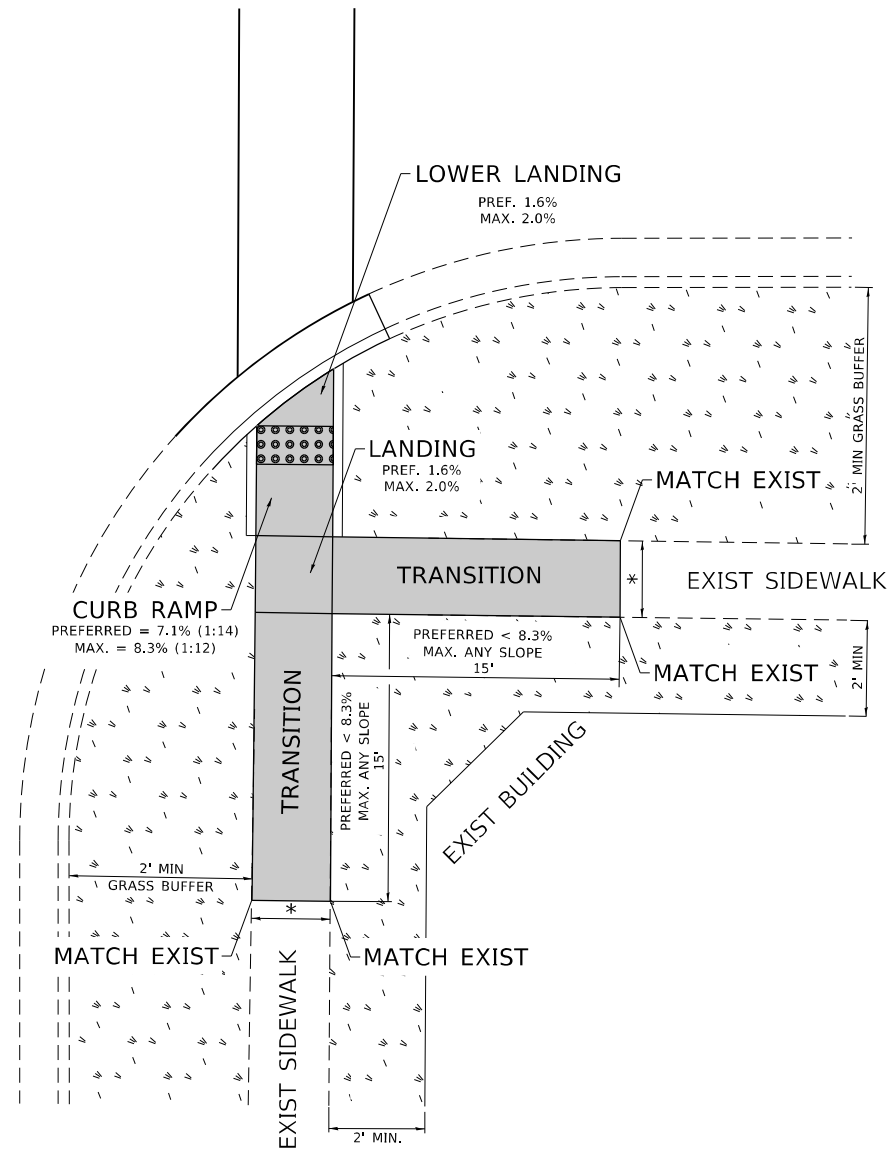
**PROJECT DETAIL FOR DOUBLE PERPENDICULAR CURB RAMPS
(PD-03)**

SCALE: NONE SHEET OF SHEETS STA. TO STA.

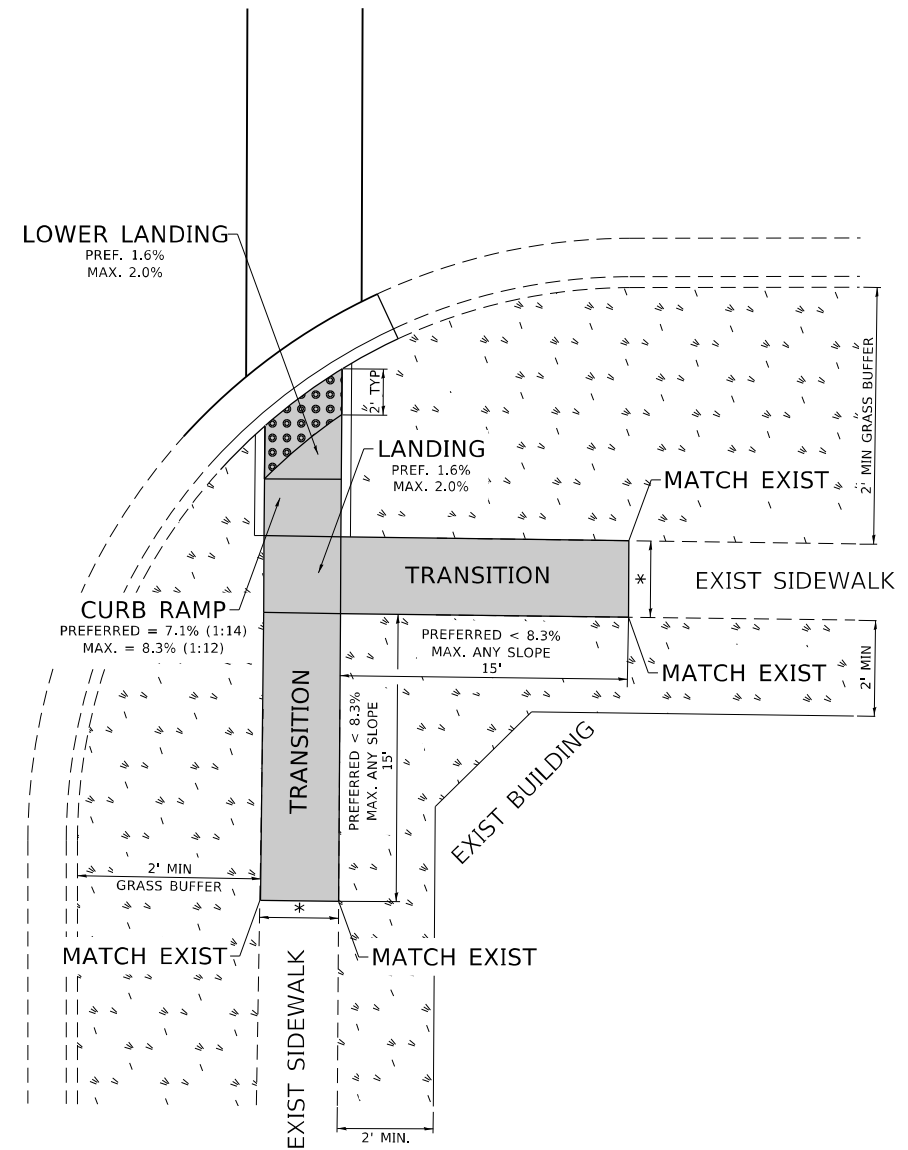
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0358	FAP 0358 22 RS	COOK	71	27
PD-03		CONTRACT NO. 62T58		
ILLINOIS FED. AID PROJECT				

ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ TURNING SPACE

PD-04A



PD-04B



LEGEND

- EXIST. GRASS
- PROPOSED SIDE CURB
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

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USER NAME = Sher,Khan	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/20/2023	DATE - 10/02/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

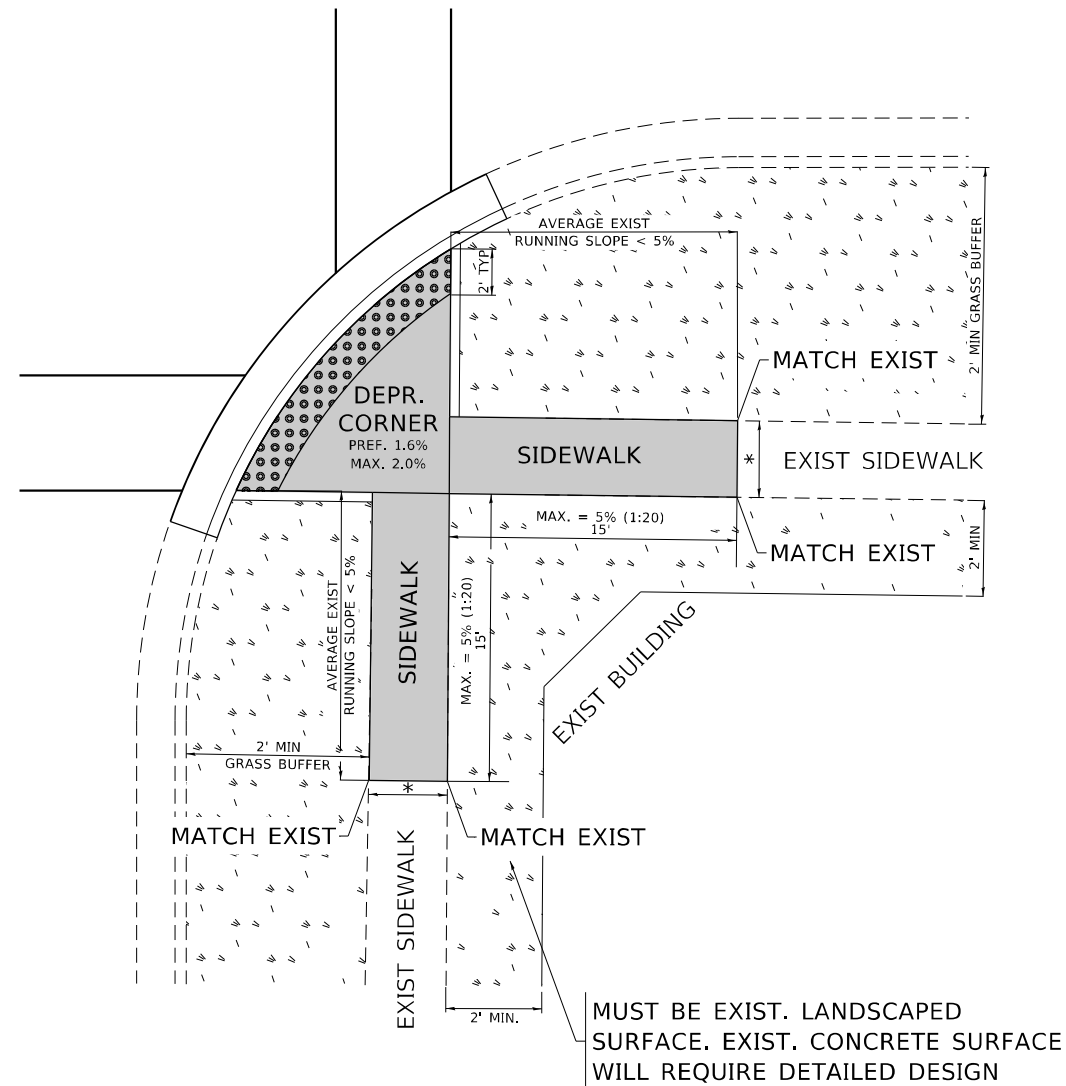
**PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS WITH
TURNING SPACE (PD-04)**

SCALE: NONE SHEET OF SHEETS STA. TO STA.

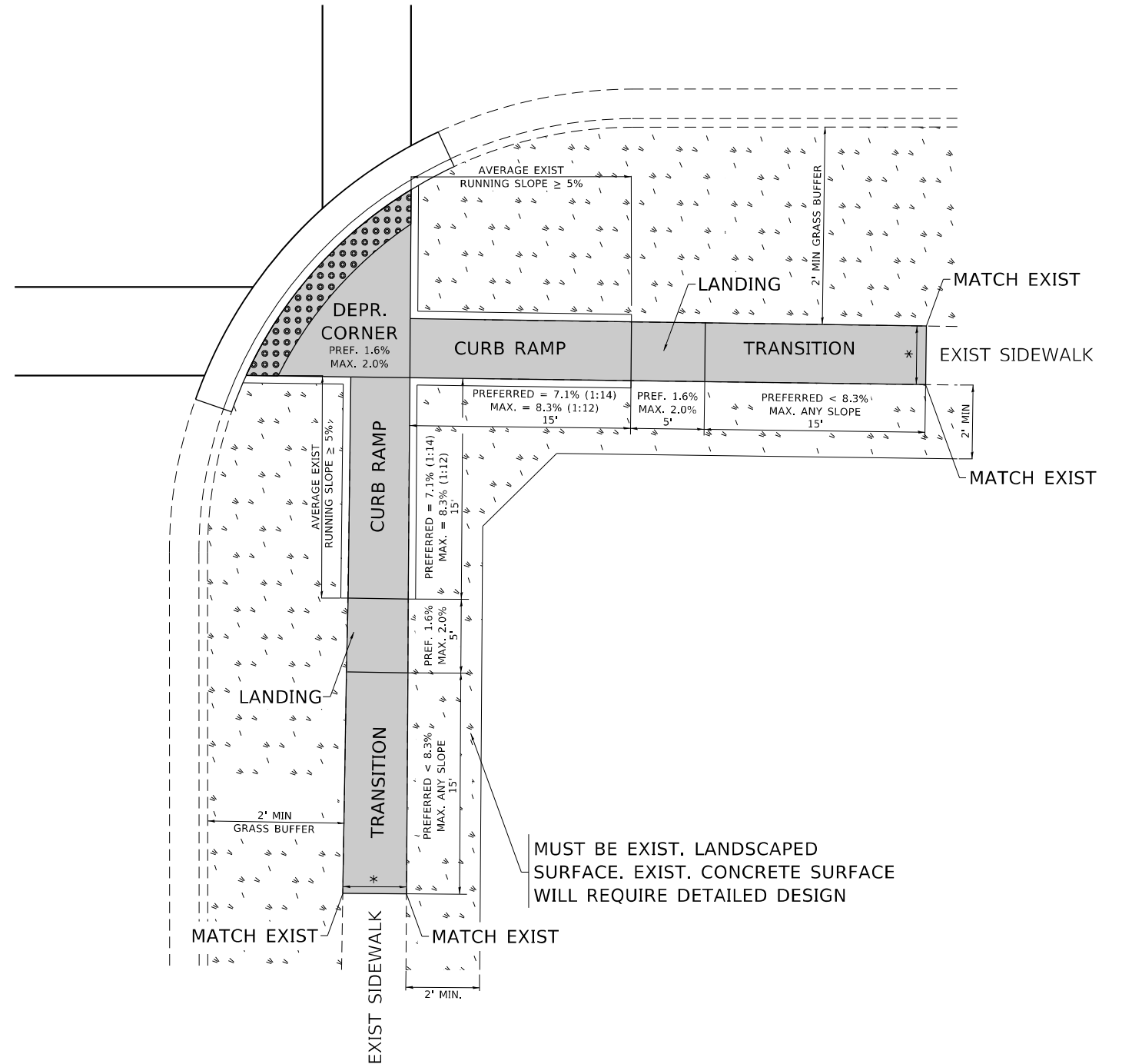
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0358	FAP 0358 22 RS	COOK	71	28
PD-04		CONTRACT NO. 62T58		
		ILLINOIS FED. AID PROJECT		

ADA DETAIL FOR DEPRESSED CORNER CURB RAMPS

PD-05A



PD-05B



LEGEND

- PROPOSED SIDE CURB
- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

MODEL: Default
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USER NAME = Sher,Khan	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/20/2023	DATE - 10/02/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

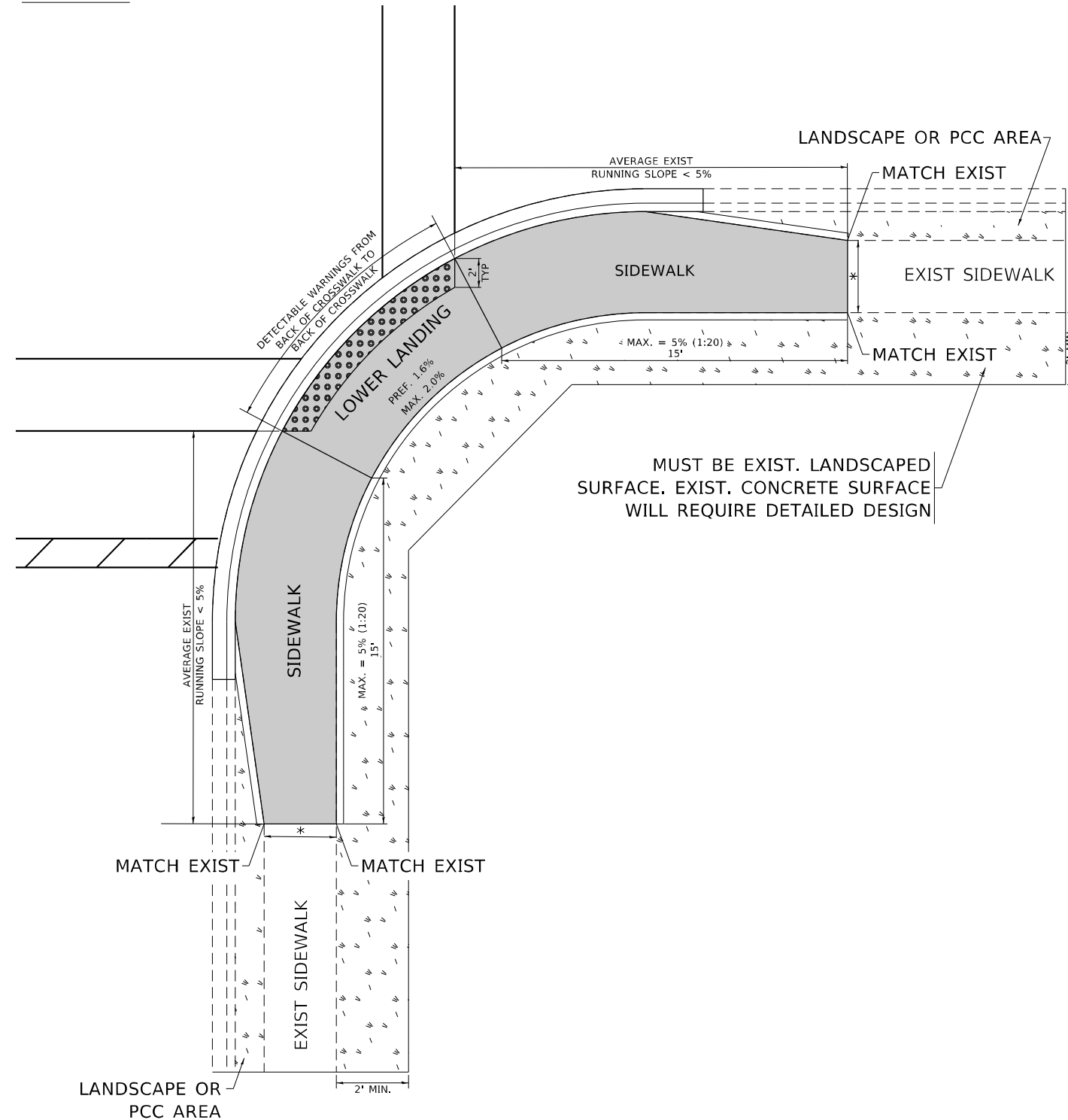
**PROJECT DETAIL FOR DEPRESSED CORNER CURB RAMPS
(PD-05)**

SCALE: NONE SHEET OF SHEETS STA. TO STA.

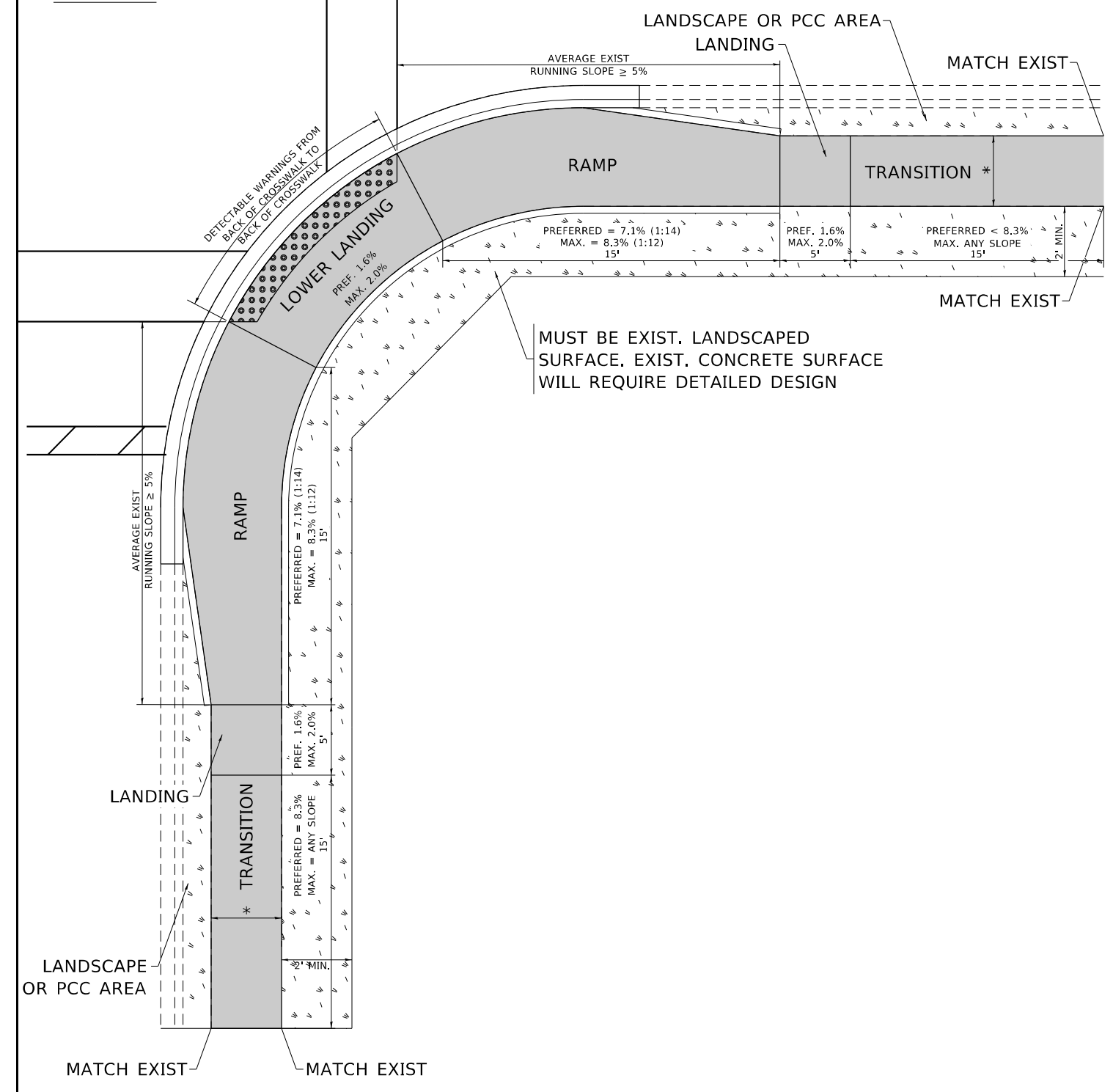
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0358	FAP 0358 22 RS	COOK	71	29
PD-05		CONTRACT NO. 62T58		
ILLINOIS FED. AID PROJECT				

ADA DETAIL FOR PARALLEL CURB RAMPS ADJACENT TO LANDSCAPING

PD-06A



PD-06B



LEGEND

- EXIST. GRASS
- PROPOSED SIDE CURB
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

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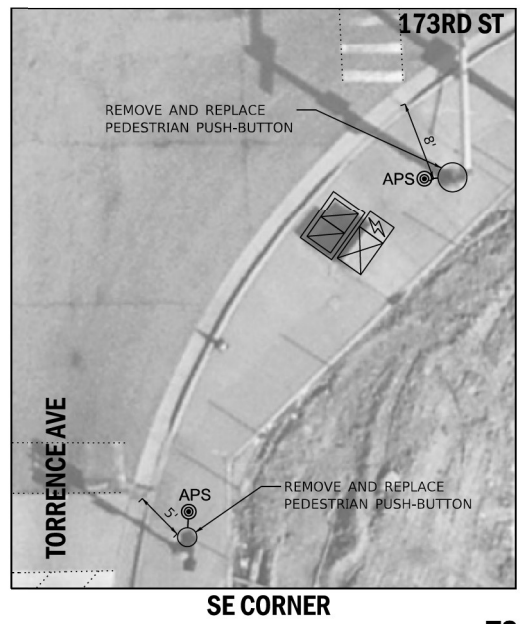
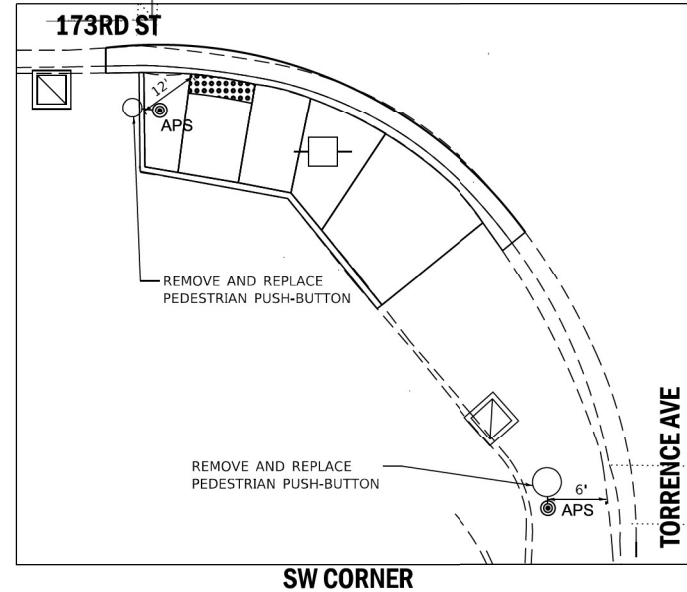
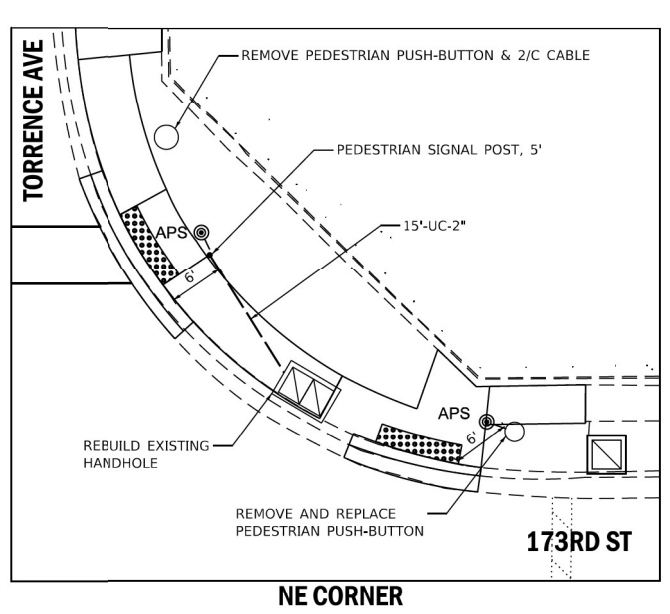
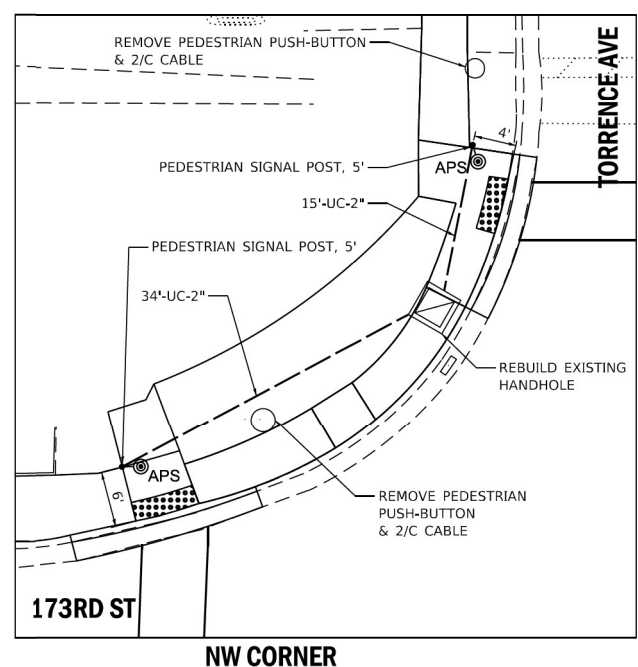
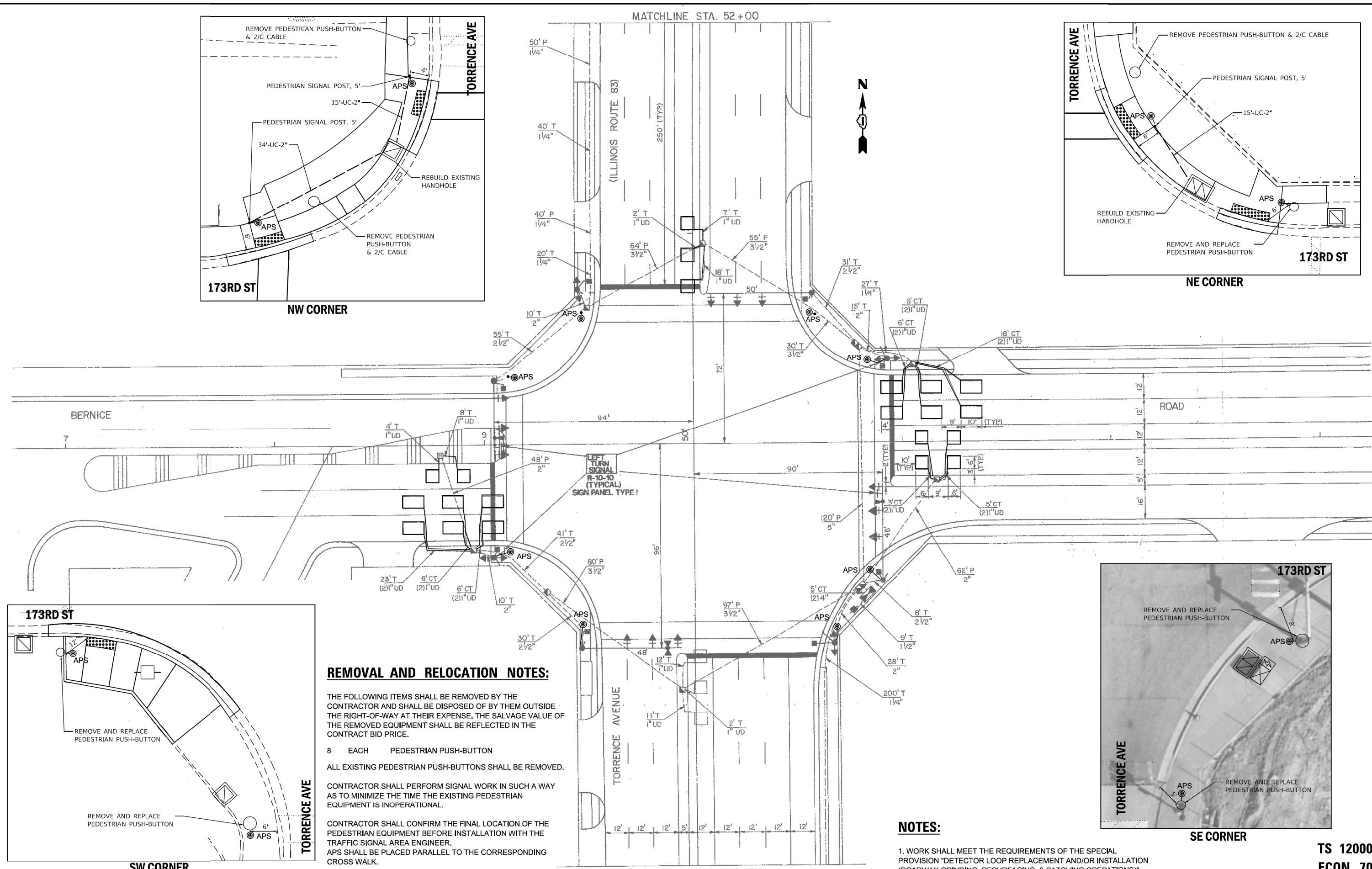
USER NAME = Sher,Khan	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/20/2023	DATE - 10/02/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROJECT DETAIL FOR PARALLEL CURB RAMPS
(PD-06)**

SCALE: NONE SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 0358	SECTION FAP 0358 22 RS	COUNTY COOK	TOTAL SHEETS 71	SHEET NO. 30
PD-06		CONTRACT NO. 62T58		
ILLINOIS FED. AID PROJECT				



REMOVAL AND RELOCATION NOTES:

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

8 EACH PEDESTRIAN PUSH-BUTTON

ALL EXISTING PEDESTRIAN PUSH-BUTTONS SHALL BE REMOVED.

CONTRACTOR SHALL PERFORM SIGNAL WORK IN SUCH A WAY AS TO MINIMIZE THE TIME THE EXISTING PEDESTRIAN EQUIPMENT IS INOPERATIONAL.

CONTRACTOR SHALL CONFIRM THE FINAL LOCATION OF THE PEDESTRIAN EQUIPMENT BEFORE INSTALLATION WITH THE TRAFFIC SIGNAL AREA ENGINEER.

APS SHALL BE PLACED PARALLEL TO THE CORRESPONDING CROSS WALK.

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, & PATCHING OPERATIONS)".

**TS 12000
ECON 70**

Sam Schwartz
200 S. WACKER
SUITE 1400
CHICAGO, IL 60606
TEL: (773) 305-0800

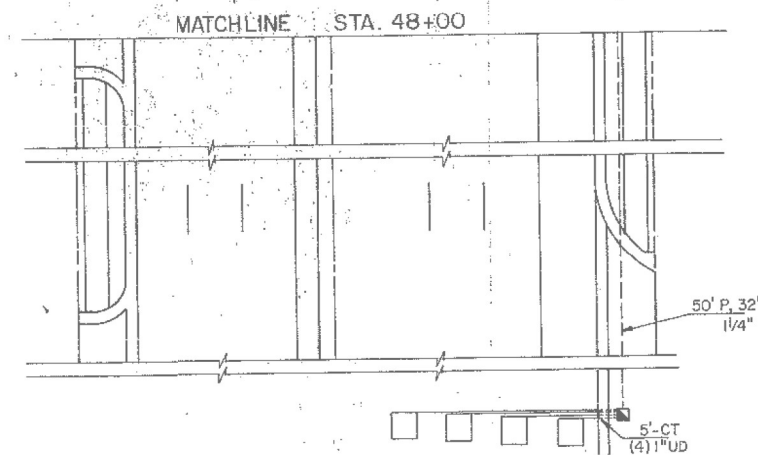
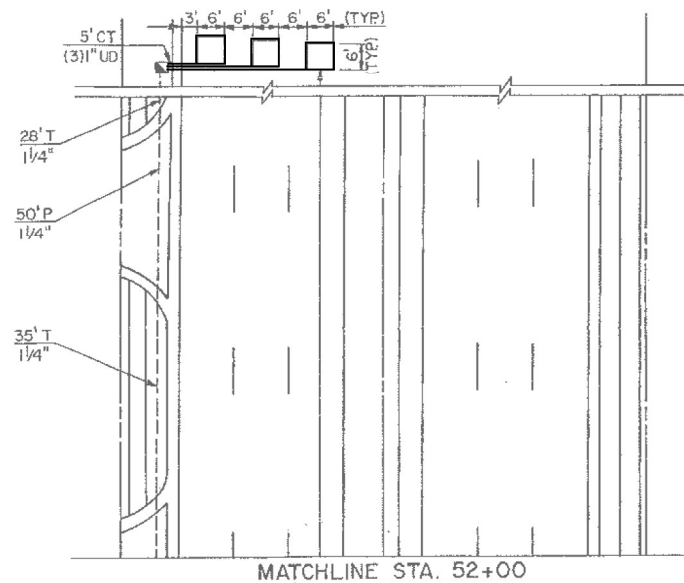
USER NAME = erica.salutz	DESIGNED - ERICA SALUTZ	REVISED -
	DRAWN - KATHLEEN MURPHY	REVISED -
PLOT SCALE = 40,0000' / 1"	CHECKED - PETER WOJTKIEWICZ	REVISED -
PLOT DATE = 10/20/2023	DATE - 10/19/2023	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC SIGNAL MODERNIZATION PLAN (SHEET 1 OF 2)
IL RTE 83 (TORRENCE AVE) AT 173RD ST / BERNICE RD**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 358	SECTION FAP 0358 22 RS	COUNTY COOK	TOTAL SHEETS 71	SHEET NO. 31
ILLINOIS			FED. AID PROJECT	



SCHEDULE OF QUANTITIES

ITEM DESCRIPTION	UNITS	TOTAL QTY
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	64
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	888
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	82
DETECTOR LOOP, TYPE I	FOOT	1024
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	920
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REBUILD EXISTING HANDHOLE	EACH	2
PEDESTRIAN SIGNAL POST, 5 FT.	EACH	3
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	8
CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	FOOT	12

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, & PATCHING OPERATIONS)".

TS SHT NO. 10

FILE NAME: M:\2019\3_Civil\18-03-2500 DOT_PTB_190 - Various Traffic Signal Design\Graphics\W0_2-0-03_62R00 & 62T10 & 62T58\2T58CAD\62T58 Sheet 02_Torrence at 173rd.dgn

Sam Schwartz
200 S. WACKER
SUITE 1400
CHICAGO, IL 60606
TEL: (773) 305-0800

USER NAME = erica.salutz	DESIGNED - ERICA SALUTZ	REVISED -
PLOT SCALE = 40,0000' / 1"	DRAWN - KATHLEEN MURPHY	REVISED -
PLOT DATE = 10/20/2023	CHECKED - PETER WOJTKIEWICZ	REVISED -
	DATE - 10/19/2023	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

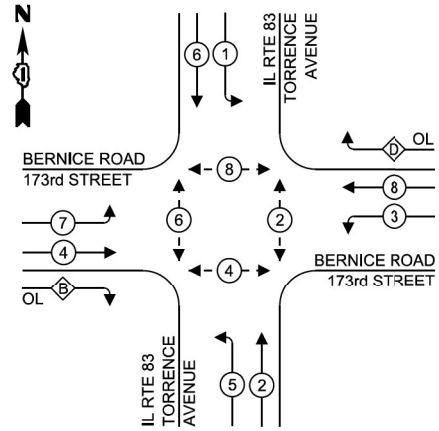
**TRAFFIC SIGNAL MODERNIZATION PLAN (SHEET 2 OF 2)
IL RTE 83 (TORRENCE AVE) AT 173RD ST / BERNICE RD**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
358	FAP 0358 22 RS	COOK	71	32
			CONTRACT NO. 62T58	
		ILLINOIS	FED. AID PROJECT	

**TS 12000
ECON 70**

PROPOSED CONTROLLER SEQUENCE



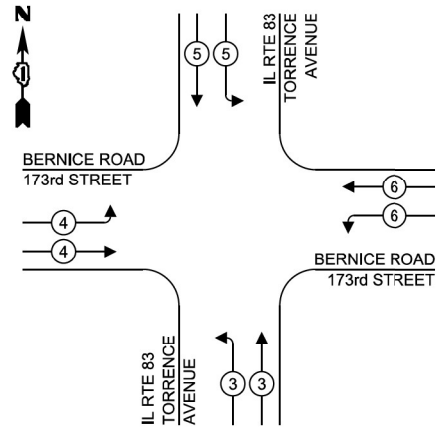
LEGEND:

- ← (circle with star) → PROTECTED PHASE
- ← (circle with star) - - → PROTECTED/PERMITTED PHASE
- ← (circle with star) → PEDESTRIAN PHASE
- ← (circle with star) → OVERLAP

RIGHT TURN OVERLAP PHASE DESIGNATION:

OVERLAP LETTER	PERMISSIVE PHASE	PROTECTED PHASE
B =	4 +	5
D =	8 +	1

PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE



FILE NAME: M:\2019\3_Civil\1803-4500 DOT FTS 180 - Various Traffic Signal Design\Graphics\1803-4500 2-4-03 62110 & 62118 & 62158 & 62159 & 62160 & 62161 & 62162 & 62163 & 62164 & 62165 & 62166 & 62167 & 62168 & 62169 & 62170 & 62171 & 62172 & 62173 & 62174 & 62175 & 62176 & 62177 & 62178 & 62179 & 62180 & 62181 & 62182 & 62183 & 62184 & 62185 & 62186 & 62187 & 62188 & 62189 & 62190 & 62191 & 62192 & 62193 & 62194 & 62195 & 62196 & 62197 & 62198 & 62199 & 62200 & 62201 & 62202 & 62203 & 62204 & 62205 & 62206 & 62207 & 62208 & 62209 & 62210 & 62211 & 62212 & 62213 & 62214 & 62215 & 62216 & 62217 & 62218 & 62219 & 62220 & 62221 & 62222 & 62223 & 62224 & 62225 & 62226 & 62227 & 62228 & 62229 & 62230 & 62231 & 62232 & 62233 & 62234 & 62235 & 62236 & 62237 & 62238 & 62239 & 62240 & 62241 & 62242 & 62243 & 62244 & 62245 & 62246 & 62247 & 62248 & 62249 & 62250 & 62251 & 62252 & 62253 & 62254 & 62255 & 62256 & 62257 & 62258 & 62259 & 62260 & 62261 & 62262 & 62263 & 62264 & 62265 & 62266 & 62267 & 62268 & 62269 & 62270 & 62271 & 62272 & 62273 & 62274 & 62275 & 62276 & 62277 & 62278 & 62279 & 62280 & 62281 & 62282 & 62283 & 62284 & 62285 & 62286 & 62287 & 62288 & 62289 & 62290 & 62291 & 62292 & 62293 & 62294 & 62295 & 62296 & 62297 & 62298 & 62299 & 62300 & 62301 & 62302 & 62303 & 62304 & 62305 & 62306 & 62307 & 62308 & 62309 & 62310 & 62311 & 62312 & 62313 & 62314 & 62315 & 62316 & 62317 & 62318 & 62319 & 62320 & 62321 & 62322 & 62323 & 62324 & 62325 & 62326 & 62327 & 62328 & 62329 & 62330 & 62331 & 62332 & 62333 & 62334 & 62335 & 62336 & 62337 & 62338 & 62339 & 62340 & 62341 & 62342 & 62343 & 62344 & 62345 & 62346 & 62347 & 62348 & 62349 & 62350 & 62351 & 62352 & 62353 & 62354 & 62355 & 62356 & 62357 & 62358 & 62359 & 62360 & 62361 & 62362 & 62363 & 62364 & 62365 & 62366 & 62367 & 62368 & 62369 & 62370 & 62371 & 62372 & 62373 & 62374 & 62375 & 62376 & 62377 & 62378 & 62379 & 62380 & 62381 & 62382 & 62383 & 62384 & 62385 & 62386 & 62387 & 62388 & 62389 & 62390 & 62391 & 62392 & 62393 & 62394 & 62395 & 62396 & 62397 & 62398 & 62399 & 62400 & 62401 & 62402 & 62403 & 62404 & 62405 & 62406 & 62407 & 62408 & 62409 & 62410 & 62411 & 62412 & 62413 & 62414 & 62415 & 62416 & 62417 & 62418 & 62419 & 62420 & 62421 & 62422 & 62423 & 62424 & 62425 & 62426 & 62427 & 62428 & 62429 & 62430 & 62431 & 62432 & 62433 & 62434 & 62435 & 62436 & 62437 & 62438 & 62439 & 62440 & 62441 & 62442 & 62443 & 62444 & 62445 & 62446 & 62447 & 62448 & 62449 & 62450 & 62451 & 62452 & 62453 & 62454 & 62455 & 62456 & 62457 & 62458 & 62459 & 62460 & 62461 & 62462 & 62463 & 62464 & 62465 & 62466 & 62467 & 62468 & 62469 & 62470 & 62471 & 62472 & 62473 & 62474 & 62475 & 62476 & 62477 & 62478 & 62479 & 62480 & 62481 & 62482 & 62483 & 62484 & 62485 & 62486 & 62487 & 62488 & 62489 & 62490 & 62491 & 62492 & 62493 & 62494 & 62495 & 62496 & 62497 & 62498 & 62499 & 62500

TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	TOTAL WATTAGE
SIGNAL HEAD			
3 - SECTION	16	11	176
4 - SECTION	0	14	0
5 - SECTION	4	13	52
PROGRAMMABLE SIGNALS			
3 - SECTION	0	22	0
4 - SECTION	0	32	0
5 - SECTION	0	28	0
PED. SIGNAL	8	15	120
CONTROLLER	1	150	150
MASTER CONTROLLER	0	100	0
UPS	1	25	25
DETECTION RADAR	-	20	-
VIDEO	-	20	-
BLANK-OUT SIGN	-	25	-
NETWORK SWITCH II OR III	-	35	-
CELLULAR MODEM	-	15	-
TOTAL UPS SIZING =			523
UPS CHARGING	1	225	225
BATTERY HEATER MAT	1	180	180
CABINET HEATER	1	200	200
FLASHER	1	15	15
LED STREET NAME SIGN	-	120	-
LUMINAIRE	-	240	-
TOTAL SERVICE WIRE SIZING =			1143

ENERGY COSTS TO:
VILLAGE OF LANSING
 3141 RIDGE ROAD
 LANSING, IL 60438

ENERGY SUPPLY:
 CONTACT: NEW BUSINESS DEPT
 PHONE: 866-639-3532
 COMPANY: COMED
 ACCOUNT NUMBER: 62350-62018

TS SHT NO. 11

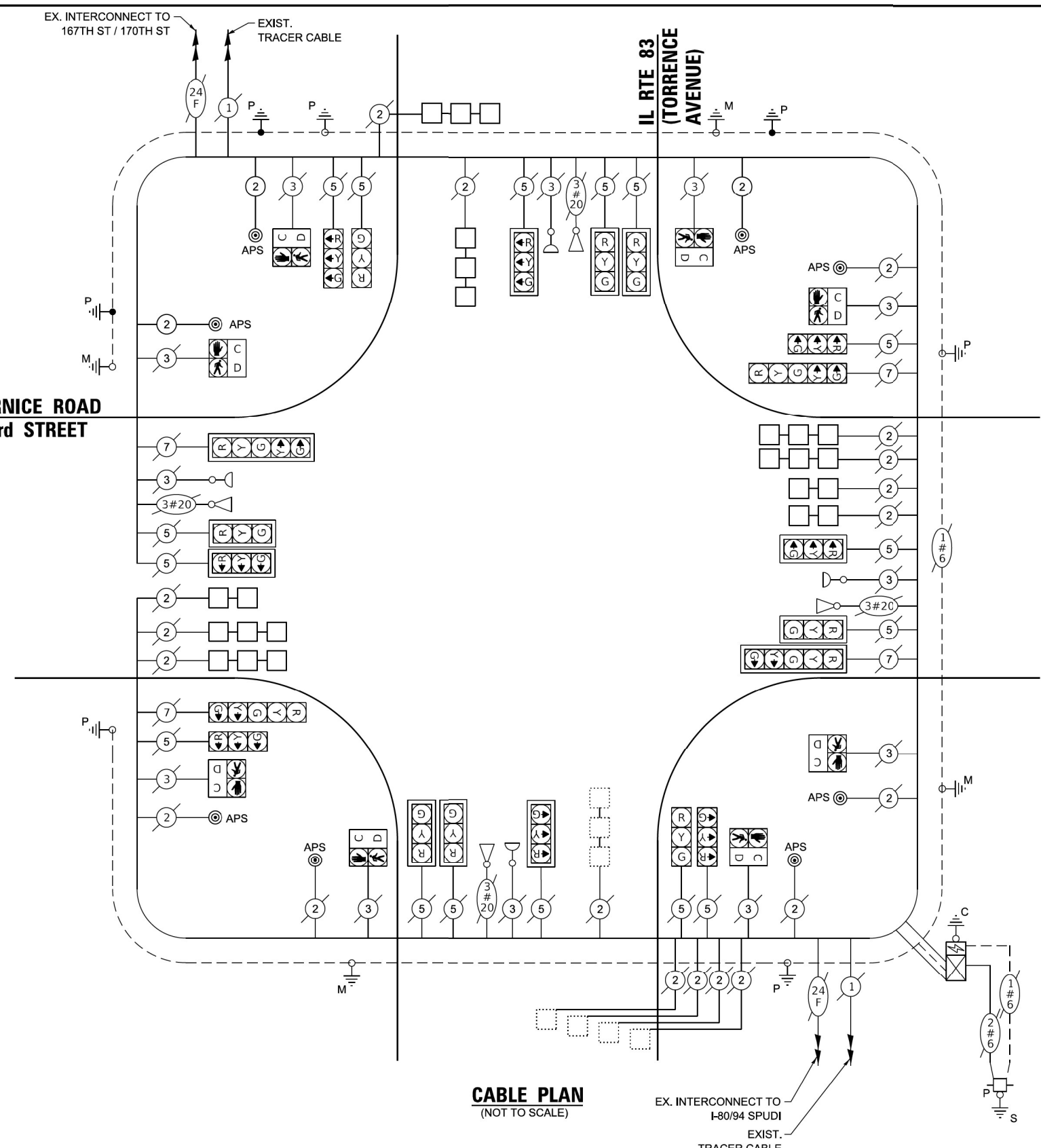
Sam Schwartz
 200 S. WACKER
 SUITE 1400
 CHICAGO, IL 60606
 TEL: (773) 305-0800

USER NAME = erica.salutz	DESIGNED - ERICA SALUTZ	REVISED -
PLOT SCALE = 40,0000' / 1"	DRAWN - KATHLEEN MURPHY	REVISED -
PLOT DATE = 10/20/2023	CHECKED - PETER WOJTKIEWICZ	REVISED -
	DATE - 10/19/2023	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CABLE PLAN, PHASE DESIGNATION DIAGRAM,
 AND EMERGENCY VEHICLE PREEMPTION SEQUENCE
 IL RTE 83 (TORRENCE AVE) AT 173RD ST / BERNICE RD**

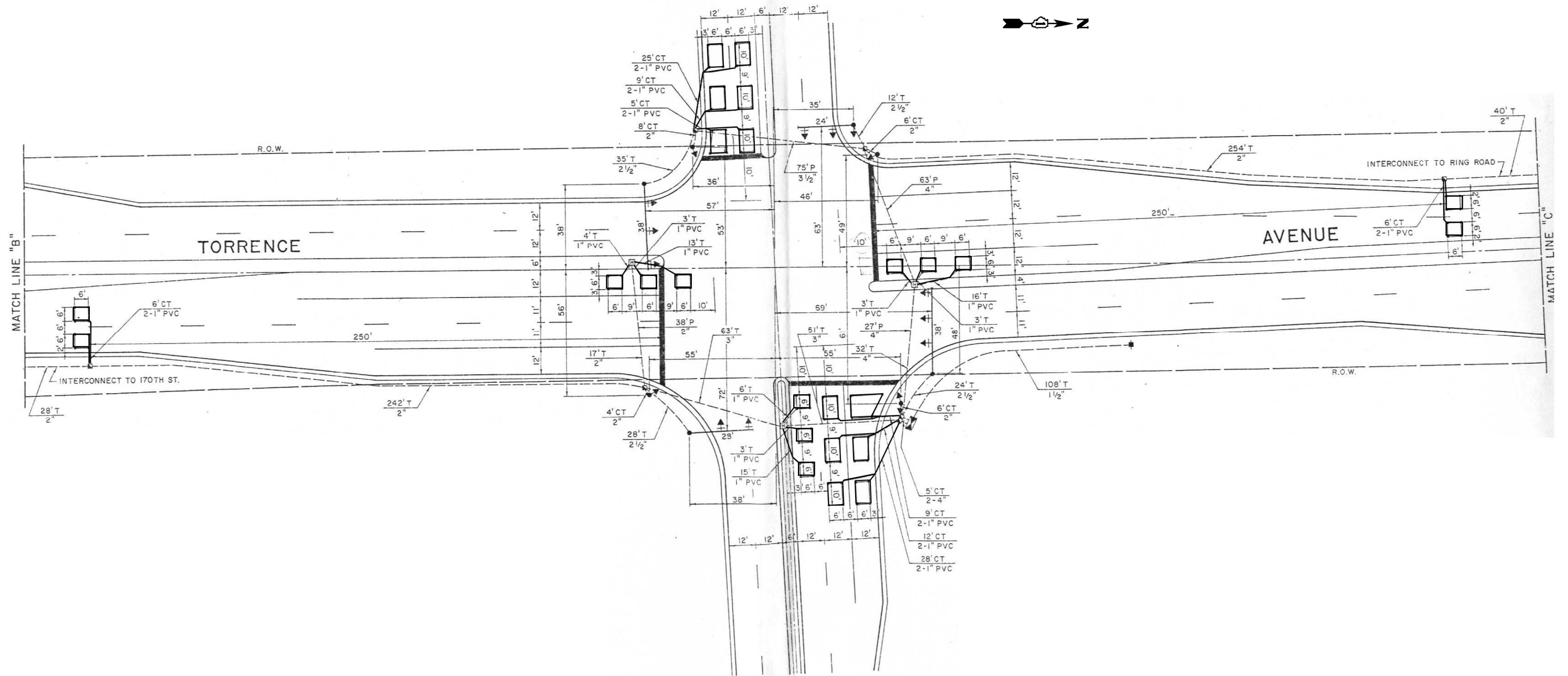
F.A.P. RTE. 358	SECTION FAP 0358 22 RS	COUNTY COOK	TOTAL SHEETS 71	SHEET NO. 33
CONTRACT NO. 62T58				
ILLINOIS FED. AID PROJECT				



CABLE PLAN
 (NOT TO SCALE)



**TS 12000
 ECON 70**



NOTES:

- 1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, & PATCHING OPERATIONS)".

Sam Schwartz
 200 S. WACKER
 SUITE 1400
 CHICAGO, IL 60606
 TEL: (773) 305-0800

USER NAME = erica.salutz	DESIGNED - ERICA SALUTZ	REVISED -
	DRAWN - KATHLEEN MURPHY	REVISED -
PLOT SCALE = 40,0000' / 1"	CHECKED - PETER WOJTKIEWICZ	REVISED -
PLOT DATE = 10/20/2023	DATE - 10/19/2023	REVISED -

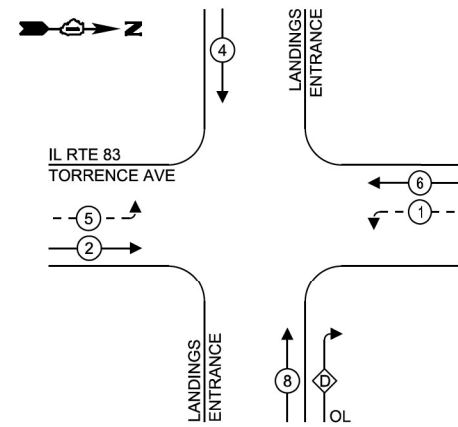
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**TRAFFIC SIGNAL MODERNIZATION PLAN
 IL RTE 83 (TORRENCE AVE) AT LANDINGS SHOPPING CENTER ENTRANCE**

F.A.P. RTE. 358	SECTION FAP 0358 22 RS	COUNTY COOK	TOTAL SHEETS 71	SHEET NO. 34
			CONTRACT NO. 62T58	
ILLINOIS FED. AID PROJECT				

**TS 11350
 ECON 49**

PROPOSED CONTROLLER SEQUENCE



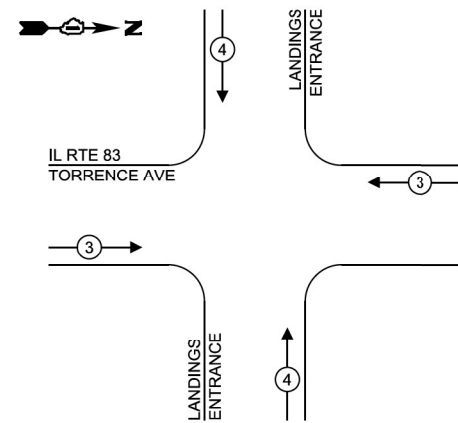
LEGEND:

- ⊙ PROTECTED PHASE
- ⊙- PROTECTED/PERMITTED PHASE
- ⊙- PEDESTRIAN PHASE
- ⊙ OL OVERLAP

RIGHT TURN OVERLAP PHASE DESIGNATION:

OVERLAP LETTER	PERMISSIVE PHASE	PROTECTED PHASE
D	8	1

PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE



FILE NAME: M:\2018\3_Civil\8245500\DOT_FTB_180 - Various Traffic Signal Design\Graphics\23-013_62110_6_621358218CAD\62118_Sheet_023_Torrence at Landings.dgn

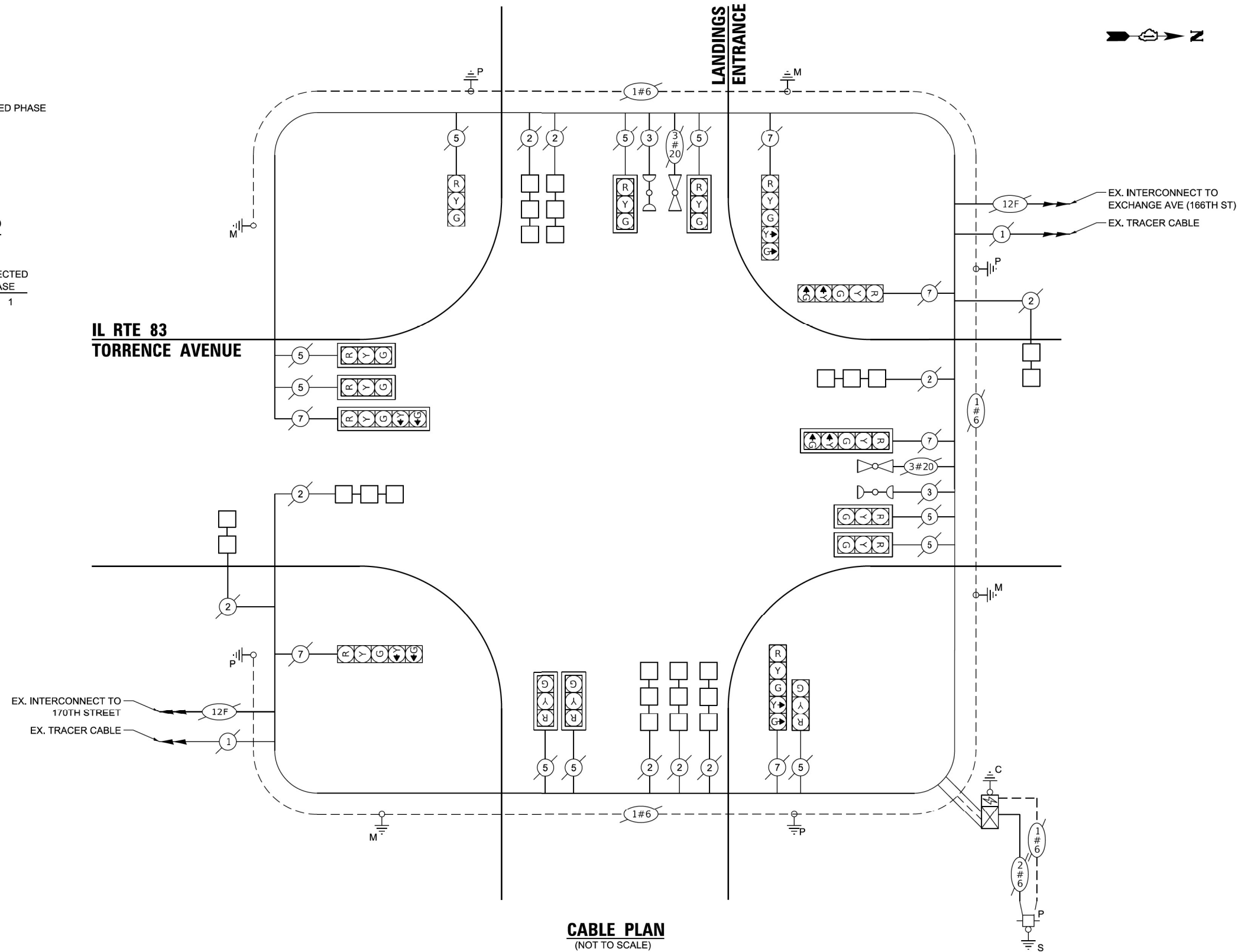
TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	TOTAL WATTAGE
SIGNAL HEAD			
3 - SECTION	10	11	110
4 - SECTION	0	14	0
5 - SECTION	6	13	78
PROGRAMMABLE SIGNALS			
3 - SECTION	0	22	0
4 - SECTION	0	32	0
5 - SECTION	0	28	0
PED. SIGNAL			
CONTROLLER	1	150	150
MASTER CONTROLLER	0	100	0
UPS	1	25	25
DETECTION			
RADAR	-	20	-
VIDEO	-	20	-
BLANK-OUT SIGN			
NETWORK SWITCH II OR III	-	35	-
CELLULAR MODEM	-	15	-
TOTAL UPS SIZING =			363
UPS CHARGING	1	225	225
BATTERY HEATER MAT	1	180	180
CABINET HEATER	1	200	200
FLASHER	1	15	15
LED STREET NAME SIGN	-	120	-
LUMINAIRE	-	240	-
TOTAL SERVICE WIRE SIZING =			983

ENERGY COSTS TO:
DEMETRIOS DELLAPORTAS
 204 PULASKI ROAD
 CALUMET CITY, IL 60409

ENERGY SUPPLY:
 CONTACT: NEW BUSINESS DEPT
 PHONE: 866-639-3532
 COMPANY: COMED
 ACCOUNT NUMBER: 62350-62018

IL RTE 83 TORRENCE AVENUE



CABLE PLAN
(NOT TO SCALE)

SCHEDULE OF QUANTITIES

ITEM DESCRIPTION	UNITS	TOTAL QTY
DETECTOR LOOP REPLACEMENT	FOOT	969

TS SHT NO. 13

Sam Schwartz
 200 S. WACKER
 SUITE 1400
 CHICAGO, IL 60606
 TEL: (773) 305-0800

USER NAME = erica.salutz	DESIGNED - ERICA SALUTZ	REVISED -
PLOT SCALE = 40,0000' / ft.	DRAWN - KATHLEEN MURPHY	REVISED -
PLOT DATE = 10/20/2023	CHECKED - PETER WOJTKIEWICZ	REVISED -
	DATE - 10/19/2023	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

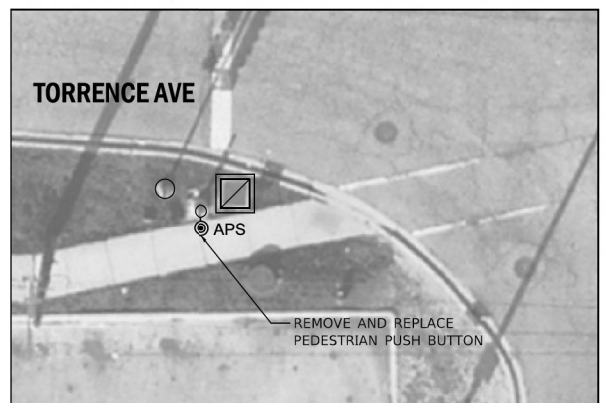
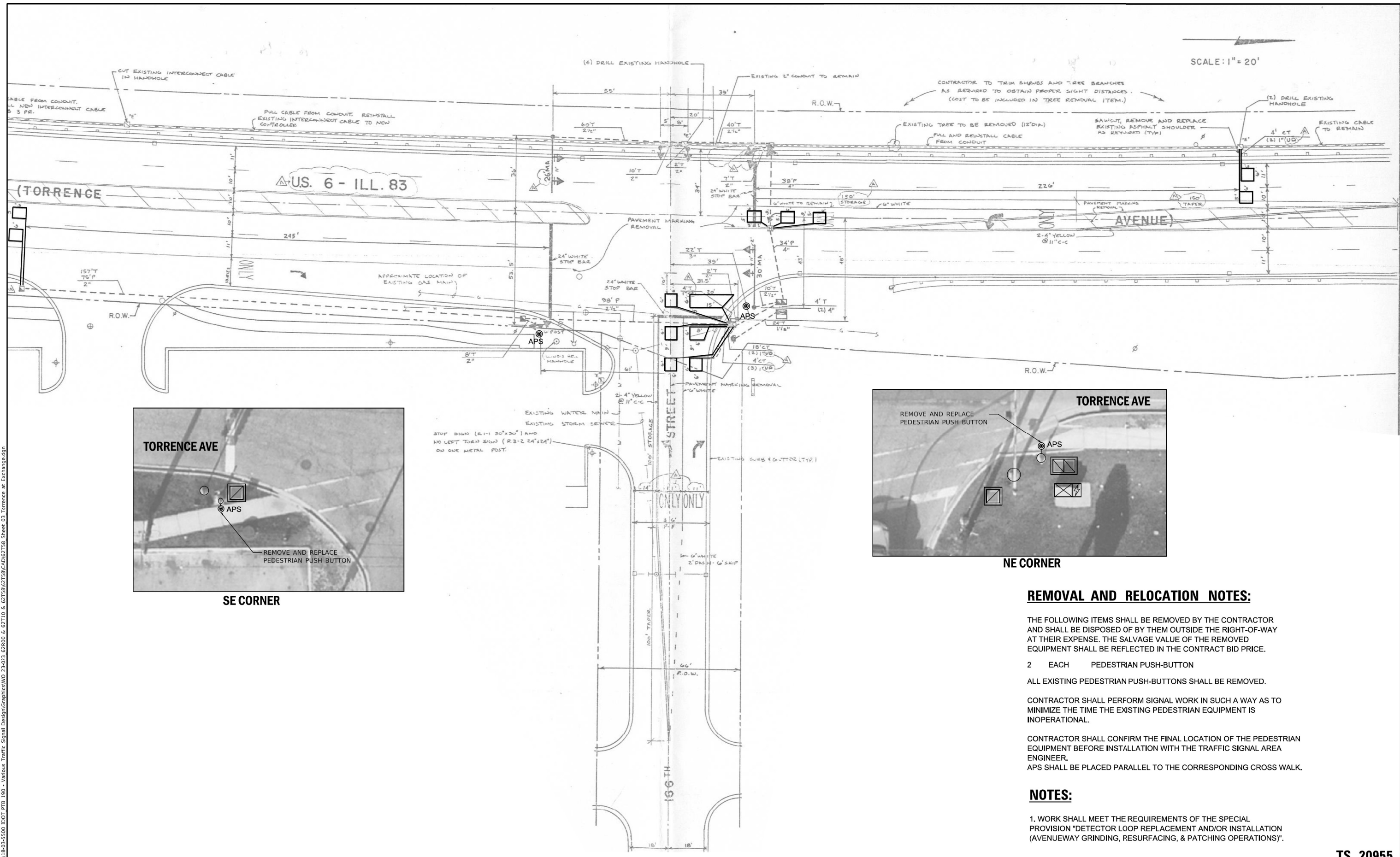
**CABLE PLAN, PHASE DESIGNATION DIAGRAM,
 AND EMERGENCY VEHICLE PREEMPTION SEQUENCE
 IL RTE 83 (TORRENCE AVE) AT LANDINGS SHOPPING CENTER ENTRANCE**

SCALE: SHEET OF SHEETS STA. TO STA.

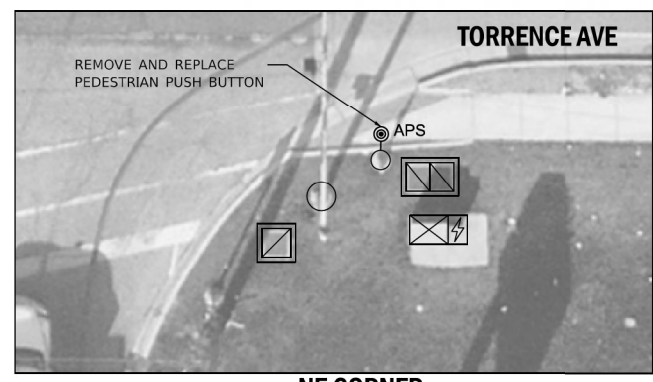
F.A.P. RTE. 358	SECTION FAP 0358 22 RS	COUNTY COOK	TOTAL SHEETS 71	SHEET NO. 35
CONTRACT NO. 62T58				
ILLINOIS FED. AID PROJECT				

**TS 11350
 ECON 49**

SCALE: 1" = 20'



SE CORNER



NE CORNER

REMOVAL AND RELOCATION NOTES:

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 2 EACH PEDESTRIAN PUSH-BUTTON

ALL EXISTING PEDESTRIAN PUSH-BUTTONS SHALL BE REMOVED.

CONTRACTOR SHALL PERFORM SIGNAL WORK IN SUCH A WAY AS TO MINIMIZE THE TIME THE EXISTING PEDESTRIAN EQUIPMENT IS INOPERATIONAL.

CONTRACTOR SHALL CONFIRM THE FINAL LOCATION OF THE PEDESTRIAN EQUIPMENT BEFORE INSTALLATION WITH THE TRAFFIC SIGNAL AREA ENGINEER. APS SHALL BE PLACED PARALLEL TO THE CORRESPONDING CROSS WALK.

NOTES:

- 1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (AVENUEWAY GRINDING, RESURFACING, & PATCHING OPERATIONS)".

TS SHT NO. 14

Sam Schwartz
 200 S. WACKER
 SUITE 1400
 CHICAGO, IL 60606
 TEL: (773) 305-0800

USER NAME = erica.salutz	DESIGNED - ERICA SALUTZ	REVISED -
	DRAWN - KATHLEEN MURPHY	REVISED -
PLOT SCALE = 40,0000' / 1"	CHECKED - PETER WOJTKIEWICZ	REVISED -
PLOT DATE = 10/20/2023	DATE - 10/19/2023	REVISED -

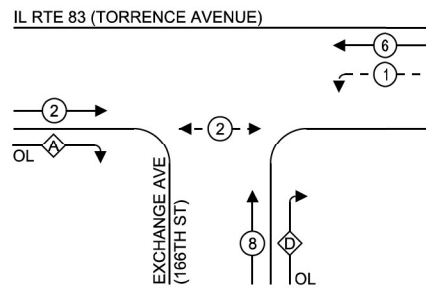
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**TRAFFIC SIGNAL MODERNIZATION PLAN
 IL 83 (TORRENCE AVE) AT EXCHANGE AVE (166TH ST)**

F.A.P. RTE. 358	SECTION FAP 0358 22 RS	COUNTY COOK	TOTAL SHEETS 71	SHEET NO. 36
CONTRACT NO. 62T58				ILLINOIS FED. AID PROJECT

**TS 20955
 ECON 49**

PROPOSED CONTROLLER SEQUENCE



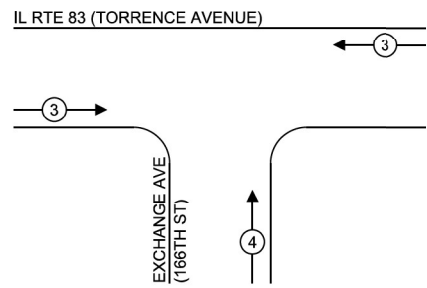
LEGEND:

- ← (⊛) → PROTECTED PHASE
- ← (⊛) - - PROTECTED/PERMITTED PHASE
- ← (⊛) → PEDESTRIAN PHASE
- ← (⊛) → OVERLAP

RIGHT TURN OVERLAP PHASE DESIGNATION:

OVERLAP LETTER	PERMISSIVE PHASE	PROTECTED PHASE
A	= 2 +	8
D	= 8 +	1

PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE



FILE NAME: M:\2019\3_Civil\18-03-2500 DOT FTS 180 - Various Traffic Signal Design\Graphics\2-2-013 62110 & 62158\218CAD\62158 Sheet 03 Torrence at Exchange.dgn

TS SHT NO. 15

TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	TOTAL WATTAGE
SIGNAL HEAD			
3 - SECTION	8	11	88
4 - SECTION	0	14	0
5 - SECTION	4	13	52
PROGRAMMABLE SIGNALS			
3 - SECTION	0	22	0
4 - SECTION	0	32	0
5 - SECTION	0	28	0
PED. SIGNAL CONTROLLER	2	15	30
MASTER CONTROLLER	1	150	150
UPS	0	100	0
DETECTION RADAR	1	25	25
VIDEO	-	20	-
BLANK-OUT SIGN	-	20	-
NETWORK SWITCH II OR III	-	25	-
CELLULAR MODEM	-	35	-
	-	15	-
TOTAL UPS SIZING =			348
UPS CHARGING	1	225	225
BATTERY HEATER MAT	1	180	180
CABINET HEATER	1	200	200
FLASHER	1	15	15
LED STREET NAME SIGN	-	120	-
LUMINAIRE	-	240	-
TOTAL SERVICE WIRE SIZING =			965

ENERGY COSTS TO:
FIRST NATIONAL REALTY

ENERGY SUPPLY:
 CONTACT: NEW BUSINESS DEPT
 PHONE: 866-639-3532
 COMPANY: COMED
 ACCOUNT NUMBER: 62350-62018

Sam Schwartz
 200 S. WACKER SUITE 1400 CHICAGO, IL 60606 TEL: (773) 305-0800

USER NAME = erica.salutz	DESIGNED - ERICA SALUTZ	REVISED -
PLOT SCALE = 40,0000' / ft.	DRAWN - KATHLEEN MURPHY	REVISED -
PLOT DATE = 10/20/2023	CHECKED - PETER WOJTKIEWICZ	REVISED -
	DATE - 10/19/2023	REVISED -

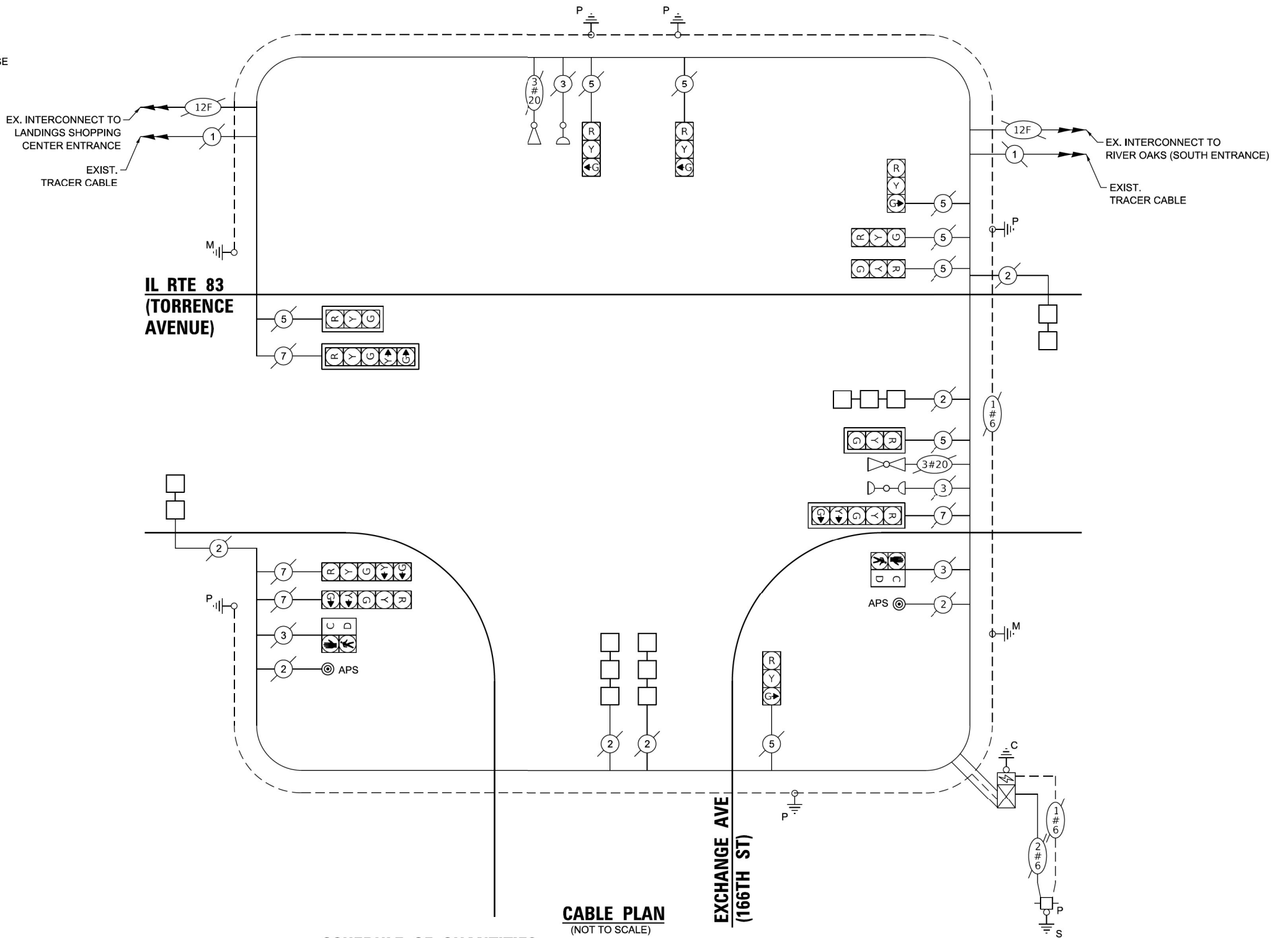
SCHEDULE OF QUANTITIES

ITEM DESCRIPTION	UNITS	TOTAL QTY
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
DETECTOR LOOP, TYPE I	FOOT	541
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	2

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

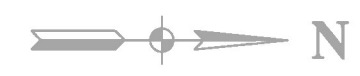
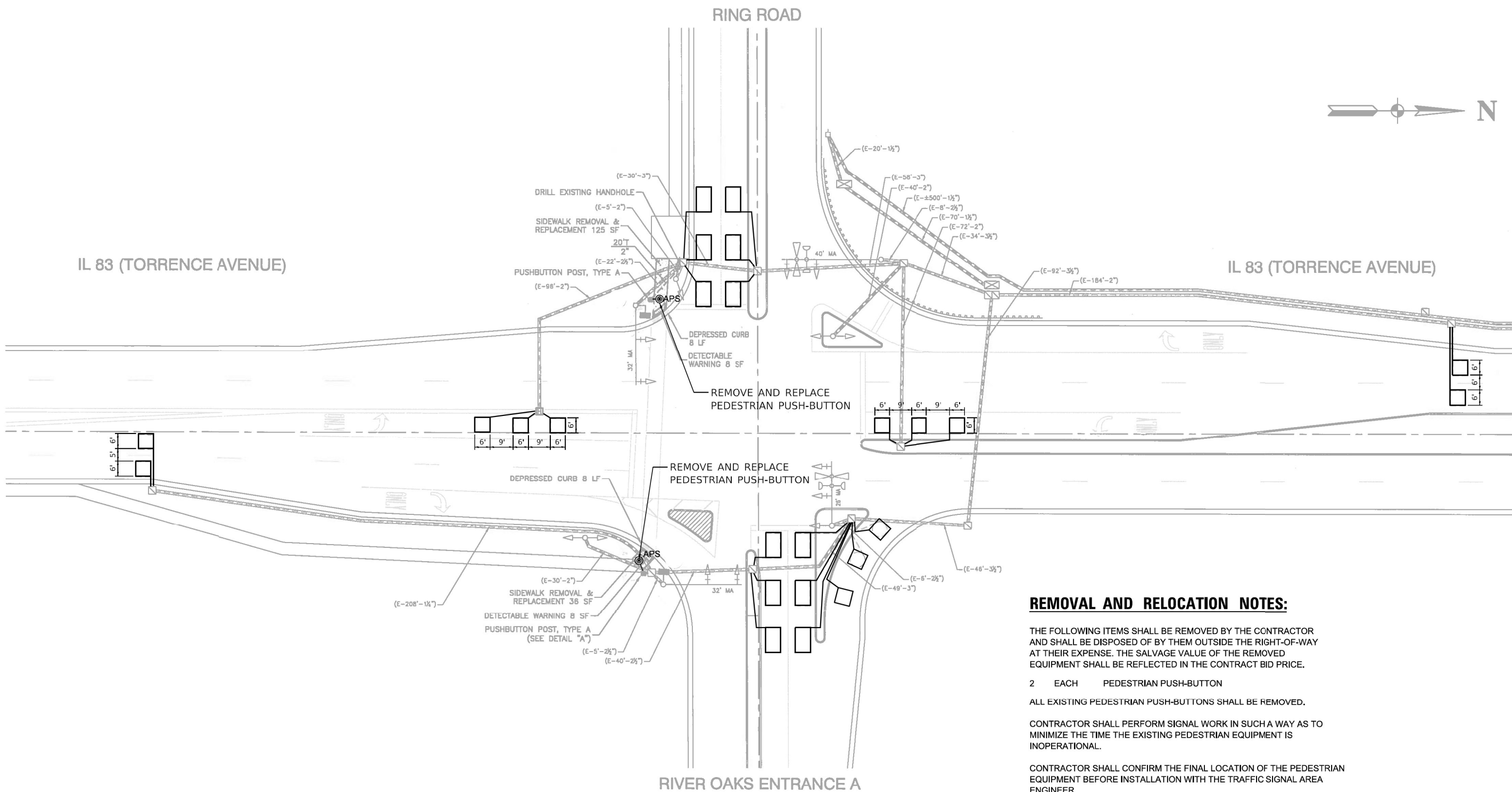
**CABLE PLAN, PHASE DESIGNATION DIAGRAM, EMERGENCY VEHICLE PREEMPTION SEQUENCE, AND SUMMARY OF QUANTITIES
 IL 83 (TORRENCE AVE) AT EXCHANGE AVE (166TH ST)**

F.A.P. RTE. 358	SECTION FAP 0358 22 RS	COUNTY COOK	TOTAL SHEETS 71	SHEET NO. 37
CONTRACT NO. 62T58				
ILLINOIS FED. AID PROJECT				



CABLE PLAN
(NOT TO SCALE)

**TS 20955
 ECON 49**



REMOVAL AND RELOCATION NOTES:

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

2 EACH PEDESTRIAN PUSH-BUTTON

ALL EXISTING PEDESTRIAN PUSH-BUTTONS SHALL BE REMOVED.

CONTRACTOR SHALL PERFORM SIGNAL WORK IN SUCH A WAY AS TO MINIMIZE THE TIME THE EXISTING PEDESTRIAN EQUIPMENT IS INOPERATIONAL.

CONTRACTOR SHALL CONFIRM THE FINAL LOCATION OF THE PEDESTRIAN EQUIPMENT BEFORE INSTALLATION WITH THE TRAFFIC SIGNAL AREA ENGINEER.

APS SHALL BE PLACED PARALLEL TO THE CORRESPONDING CROSS WALK.

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, & PATCHING OPERATIONS)".

Sam Schwartz
200 S. WACKER SUITE 1400 CHICAGO, IL 60606 TEL: (773) 305-0800

USER NAME = erica.salutz	DESIGNED - ERICA SALUTZ	REVISED -
	DRAWN - KATHLEEN MURPHY	REVISED -
PLOT SCALE = 40,0000' / 1"	CHECKED - PETER WOJTKIEWICZ	REVISED -
PLOT DATE = 10/20/2023	DATE - 10/19/2023	REVISED -

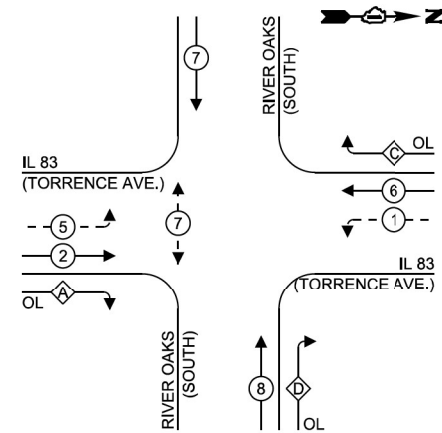
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

**TRAFFIC SIGNAL MODERNIZATION PLAN
IL 83 (TORRENCE AVE) AT RIVER OAKS CENTER DR (SOUTH ENTRANCE)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 358	SECTION FAP 0358 22 RS	COUNTY COOK	TOTAL SHEETS 71	SHEET NO. 38
			CONTRACT NO. 62T58	
		ILLINOIS FED. AID PROJECT		

PROPOSED CONTROLLER SEQUENCE



LEGEND:

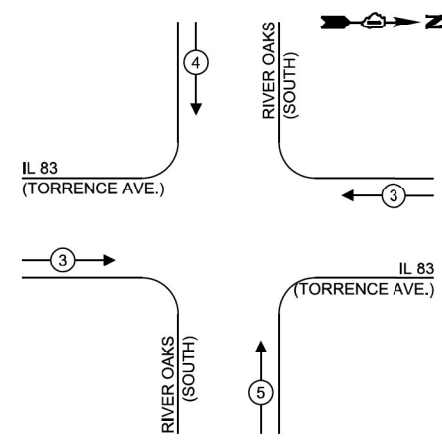
- ← (⊙) → PROTECTED PHASE
- ← (⊙) - - → PROTECTED/PERMITTED PHASE
- ← (⊙) → PEDESTRIAN PHASE
- ← (⊙) → OVERLAP

RIGHT TURN OVERLAP

PHASE DESIGNATION:

OVERLAP LETTER	PERMISSIVE PHASE	PROTECTED PHASE
A	= 2 + 8	7
C	= 6 + 7	3
D	= 8 + 1	5

PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE



FILE NAME: M:\2018\3_Civil\18034500_DDOT_FTB_180 - Various Traffic Signal Design\Graphics\18034500_23-013_62110_6_63158275R\CAD\62118_Sheet_04_Torrence at River Oaks-South.dgn

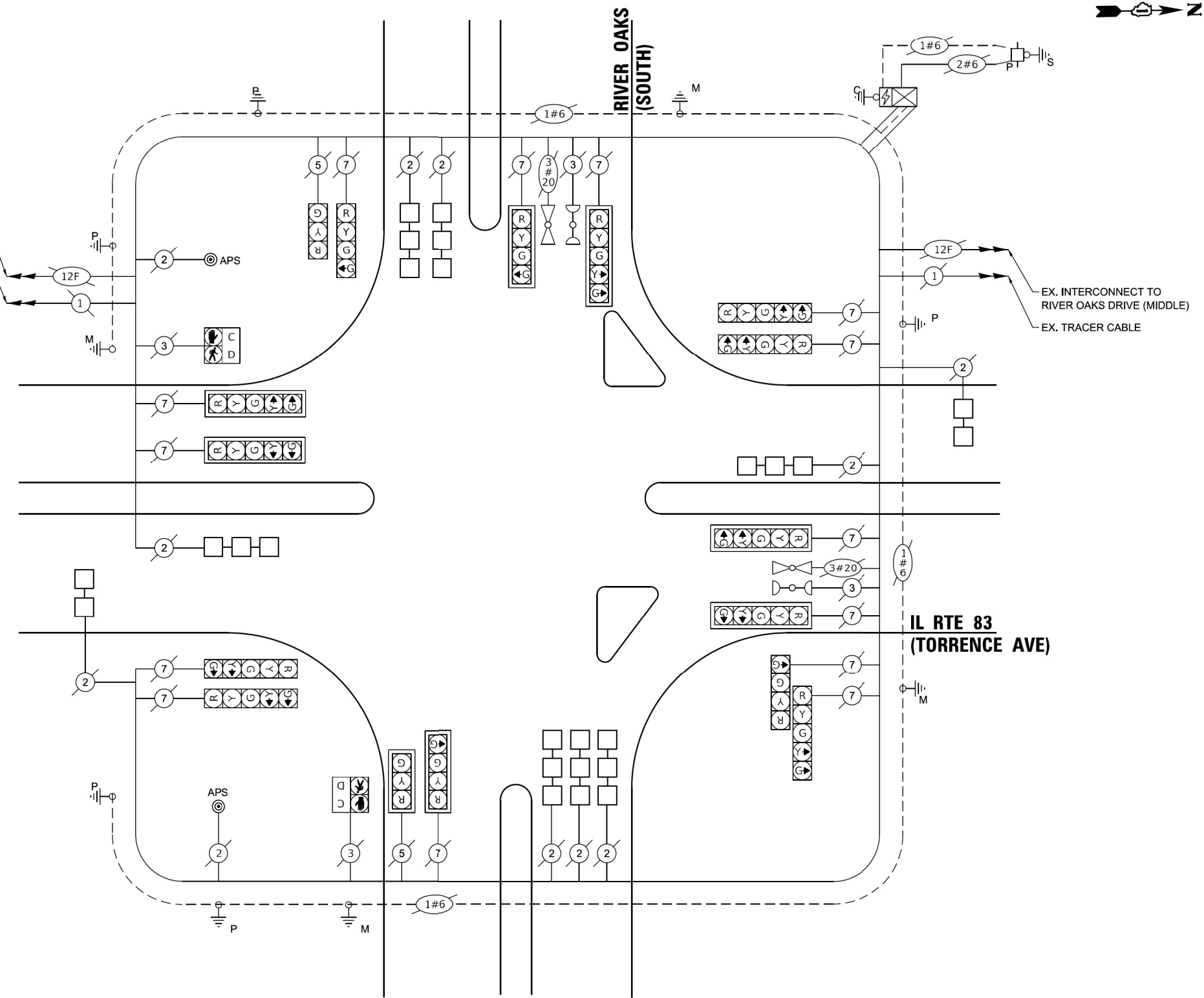
TS SHT NO. 17

TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

	NO. OF LAMPS	LED WATTAGE	TOTAL WATTAGE
SIGNAL HEAD			
3 - SECTION	2	11	22
4 - SECTION	4	14	56
5 - SECTION	10	13	130
PROGRAMMABLE SIGNALS			
3 - SECTION	0	22	0
4 - SECTION	0	32	0
5 - SECTION	0	28	0
PED. SIGNAL	2	15	30
CONTROLLER	1	150	150
MASTER CONTROLLER	0	100	0
UPS	1	25	25
DETECTION			
RADAR	-	20	-
VIDEO	-	20	-
BLANK-OUT SIGN			
NETWORK SWITCH II OR III	-	35	-
CELLULAR MODEM	-	15	-
TOTAL UPS SIZING =			413
UPS CHARGING	1	225	225
BATTERY HEATER MAT	1	180	180
CABINET HEATER	1	200	200
FLASHER	1	15	15
LED STREET NAME SIGN	-	120	-
LUMINAIRE	-	240	-
TOTAL SERVICE WIRE SIZING =			1033

ENERGY COSTS TO:
CALUMET CITY
 204 PULASKI ROAD
 CALUMET CITY, IL 60409

ENERGY SUPPLY:
 CONTACT: NEW BUSINESS DEPT
 PHONE: 866-639-3532
 COMPANY: COMED
 ACCOUNT NUMBER: 07730-19122



CABLE PLAN
(NOT TO SCALE)

SCHEDULE OF QUANTITIES

ITEM DESCRIPTION	UNITS	TOTAL QTY
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
DETECTOR LOOP, TYPE I	FOOT	1048
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	2

TS 345
ECON 49

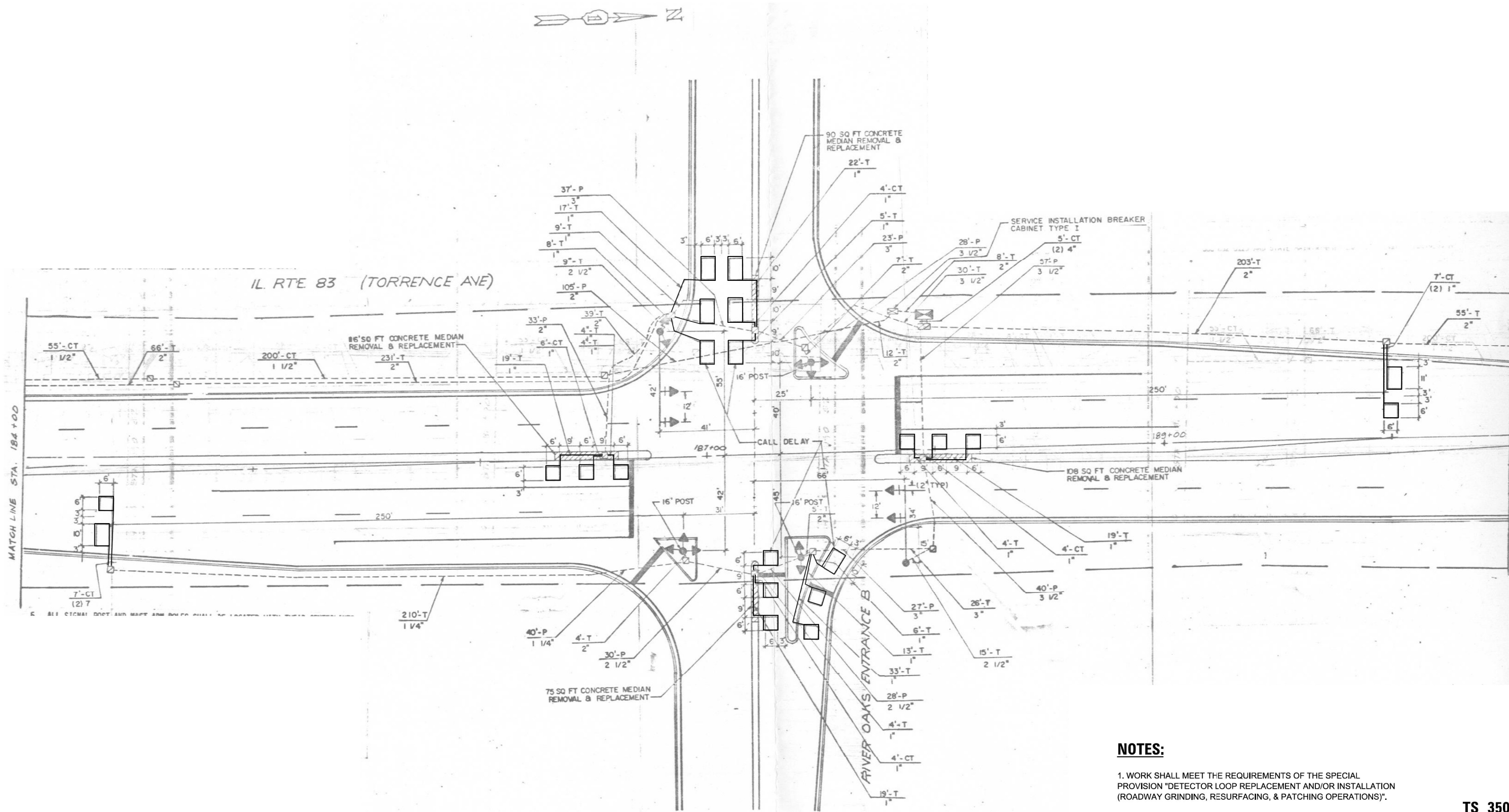
Sam Schwartz
 200 S. WACKER
 SUITE 1400
 CHICAGO, IL 60606
 TEL: (773) 305-0800

USER NAME = erica.salutz	DESIGNED - ERICA SALUTZ	REVISED -
PLOT SCALE = 40,0000' / ft.	DRAWN - KATHLEEN MURPHY	REVISED -
PLOT DATE = 10/20/2023	CHECKED - PETER WOJTKIEWICZ	REVISED -
	DATE - 10/19/2023	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CABLE PLAN, PHASE DESIGNATION DIAGRAM, EMERGENCY VEHICLE PREEMPTION SEQUENCE, AND SUMMARY OF QUANTITIES
IL 83 (TORRENCE AVE) AT RIVER OAKS CENTER DR (SOUTH ENTRANCE)

F.A.P. RTE. 358	SECTION FAP 0358 22 RS	COUNTY COOK	TOTAL SHEETS 71	SHEET NO. 39
CONTRACT NO. 62T58			ILLINOIS FED. AID PROJECT	



NOTES:

- 1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, & PATCHING OPERATIONS)".

TS 350
ECON 49

Sam Schwartz
200 S. WACKER
SUITE 1400
CHICAGO, IL 60606
TEL (773) 305-0800

USER NAME = erica.salutz	DESIGNED - ERICA SALUTZ	REVISED -
	DRAWN - KATHLEEN MURPHY	REVISED -
PLOT SCALE = 40,0000 * / 1/4"	CHECKED - PETER WOJTKIEWICZ	REVISED -
PLOT DATE = 10/20/2023	DATE - 10/19/2023	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

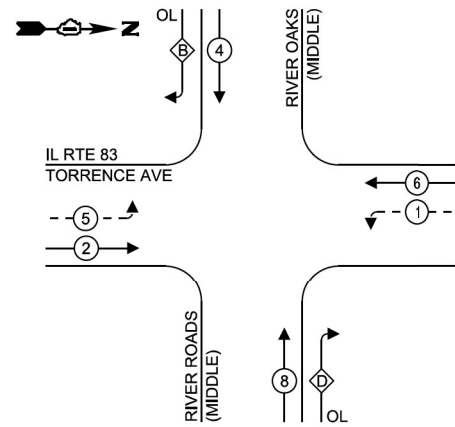
TRAFFIC SIGNAL MODERNIZATION PLAN
IL 83 (TORRENCE AVE) AT RIVER OAKS CENTER DR (MIDDLE ENTRANCE)

SCALE: SHEET OF SHEETS STA. TO STA.

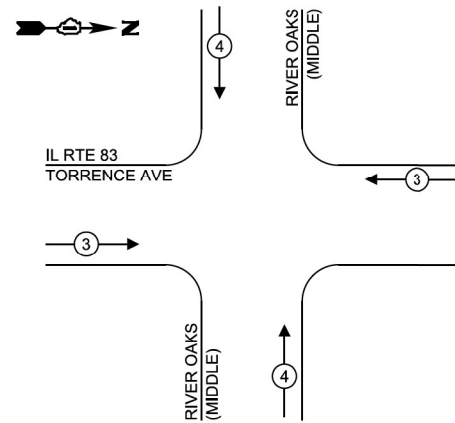
F.A.P. RTE. 358	SECTION FAP 0358 22 RS	COUNTY COOK	TOTAL SHEETS 71	SHEET NO. 40
ILLINOIS			FED. AID PROJECT	

CONTRACT NO. 62T58

PROPOSED CONTROLLER SEQUENCE



PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE



LEGEND:

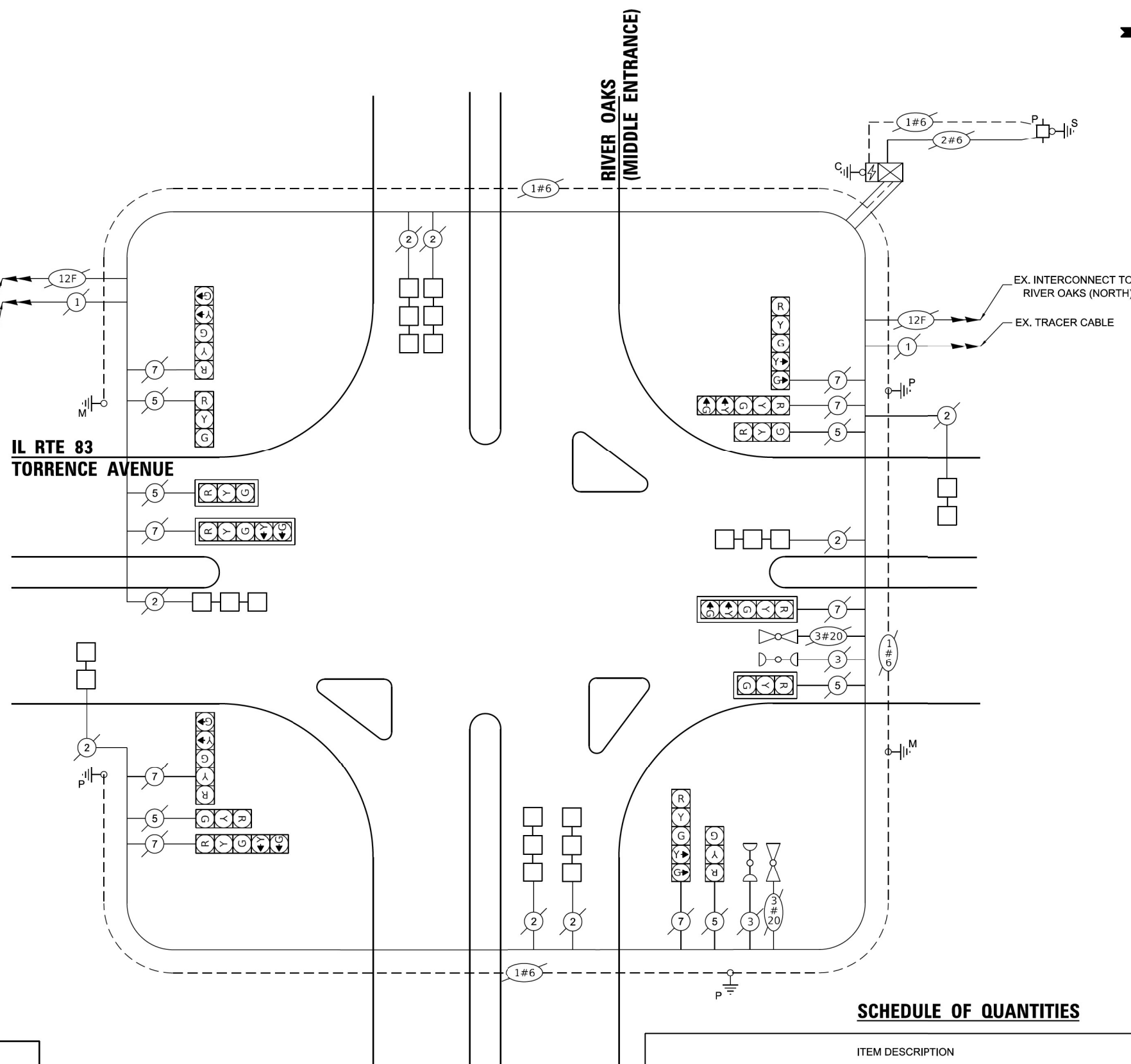
- ← * → PROTECTED PHASE
- ← - * - → PROTECTED/PERMITTED PHASE
- ← * → PEDESTRIAN PHASE
- ← * → OVERLAP

RIGHT TURN OVERLAP PHASE DESIGNATION:

OVERLAP LETTER	PERMISSIVE PHASE	PROTECTED PHASE
B	= 4	+ 5
D	= 8	+ 1

EX. INTERCONNECT TO RIVER OAKS (SOUTH)
EX. TRACER CABLE

IL RTE 83 TORRENCE AVENUE



CABLE PLAN
(NOT TO SCALE)

SCHEDULE OF QUANTITIES

ITEM DESCRIPTION	UNITS	TOTAL QTY
DETECTOR LOOP REPLACEMENT	FOOT	903

FILE NAME: M:\2018\3_Civil\8434500_DOT_FTB_180 - Various Traffic Signal Design\Graphics\23-013_62700 & 62710 & 62758\23B\CAD\62718_Sheet_05_Torrence at River Oaks-middle.dgn

TS SHT NO. 19

TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	TOTAL WATTAGE
SIGNAL HEAD			
3 - SECTION	6	11	66
4 - SECTION	0	14	0
5 - SECTION	8	13	104
PROGRAMMABLE SIGNALS			
3 - SECTION	0	22	0
4 - SECTION	0	32	0
5 - SECTION	0	28	0
PED. SIGNAL	0	15	0
CONTROLLER	1	150	150
MASTER CONTROLLER	0	100	0
UPS	1	25	25
DETECTION RADAR	-	20	-
VIDEO	-	20	-
BLANK-OUT SIGN	-	25	-
NETWORK SWITCH II OR III	-	35	-
CELLULAR MODEM	-	15	-
TOTAL UPS SIZING =			345
UPS CHARGING	1	225	225
BATTERY HEATER MAT	1	180	180
CABINET HEATER	1	200	200
FLASHER	1	15	15
LED STREET NAME SIGN	-	120	-
LUMINAIRE	-	240	-
TOTAL SERVICE WIRE SIZING =			965

ENERGY COSTS TO:
CALUMET CITY
204 PULASKI ROAD
CALUMET CITY, IL 60409

ENERGY SUPPLY:
CONTACT: NEW BUSINESS DEPT
PHONE: 866-639-3532
COMPANY: COMED
ACCOUNT NUMBER: 07730-19122

Sam Schwartz
200 S. WACKER
SUITE 1400
CHICAGO, IL 60606
TEL: (773) 305-0800

USER NAME = erica.salutz	DESIGNED - ERICA SALUTZ	REVISED -
PLOT SCALE = 40,0000' / ft.	DRAWN - KATHLEEN MURPHY	REVISED -
PLOT DATE = 10/20/2023	CHECKED - PETER WOJTKIEWICZ	REVISED -
	DATE - 10/19/2023	REVISED -

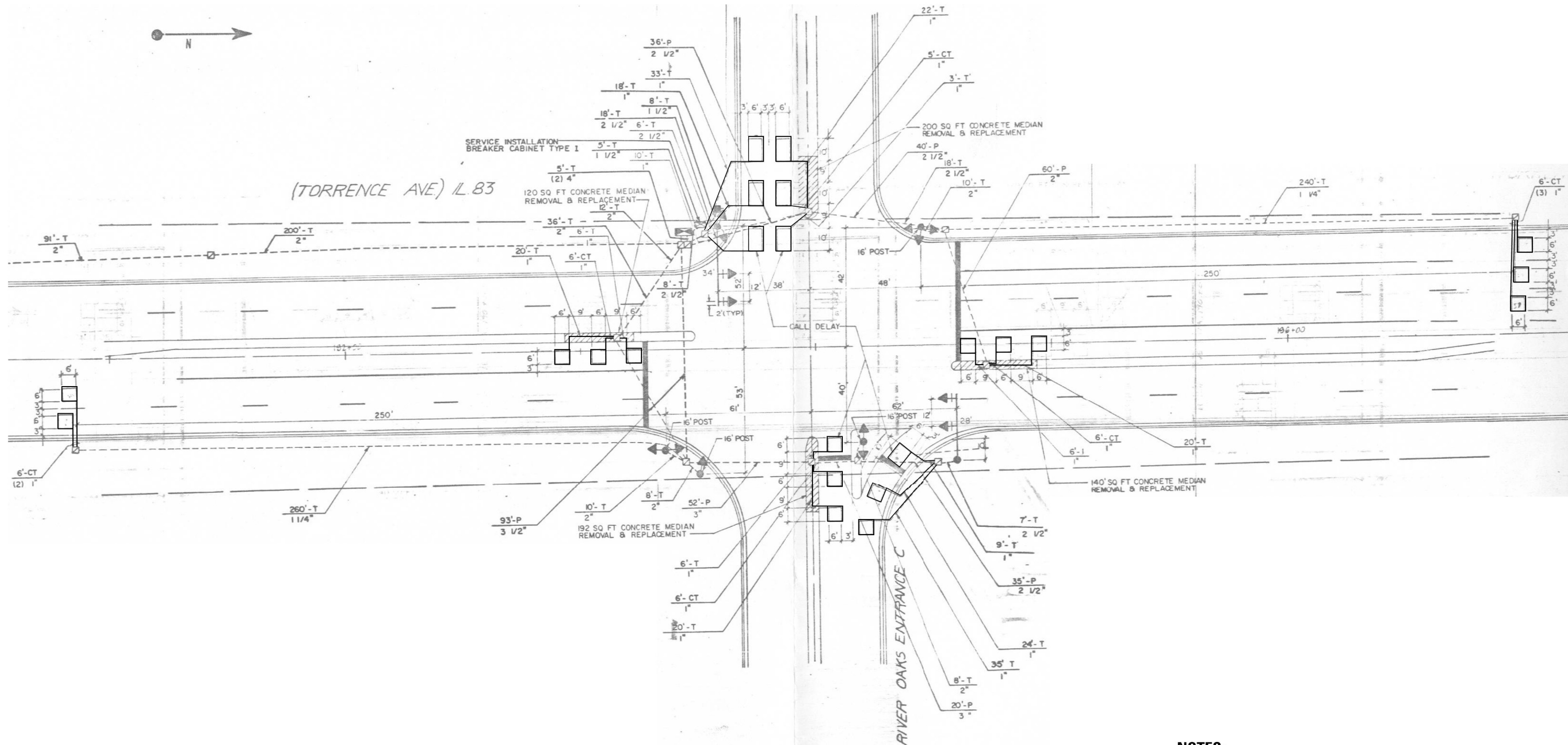
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CABLE PLAN, PHASE DESIGNATION DIAGRAM, EMERGENCY VEHICLE
PREEMPTION SEQUENCE, AND SUMMARY OF QUANTITIES
IL 83 (TORRENCE AVE) AT RIVER OAKS CENTER DR (MIDDLE ENTRANCE)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 358	SECTION FAP 0358 22 RS	COUNTY COOK	TOTAL SHEETS 71	SHEET NO. 41
CONTRACT NO. 62T58				
ILLINOIS FED. AID PROJECT				

**TS 350
ECON 49**



NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, & PATCHING OPERATIONS)".

**TS 355
ECON 49**

Sam Schwartz
200 S. WACKER
SUITE 1400
CHICAGO, IL 60606
TEL: (773) 305-0800

USER NAME = erica.salutz	DESIGNED - ERICA SALUTZ	REVISED -
	DRAWN - KATHLEEN MURPHY	REVISED -
PLOT SCALE = 40,0000' / 1"	CHECKED - PETER WOJTKIEWICZ	REVISED -
PLOT DATE = 10/20/2023	DATE - 10/19/2023	REVISED -

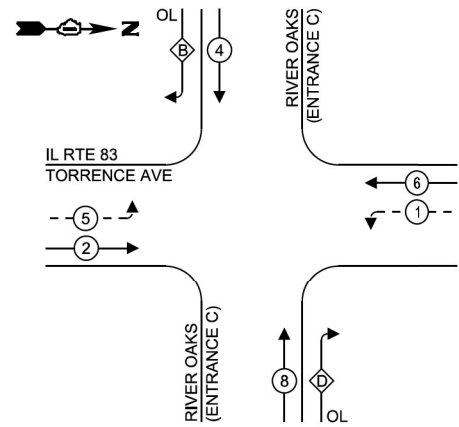
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC SIGNAL MODERNIZATION PLAN
IL 83 (TORRENCE AVE) AT RIVER OAKS CENTER DR (NORTH ENTRANCE)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
358	FAP 0358 22 RS	COOK	71	42
CONTRACT NO. 62T58				
ILLINOIS FED. AID PROJECT				

PROPOSED CONTROLLER SEQUENCE



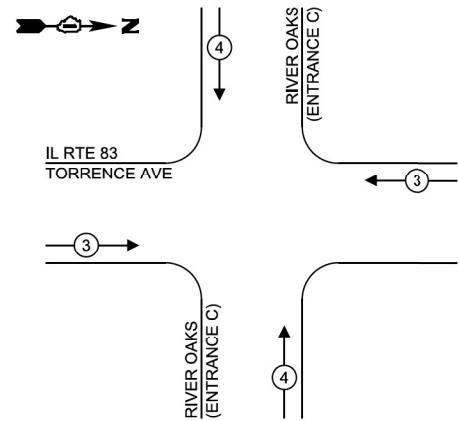
LEGEND:

- ← * → PROTECTED PHASE
- ← * - * → PROTECTED/PERMITTED PHASE
- ← * → PEDESTRIAN PHASE
- ← * → OL OVERLAP

RIGHT TURN OVERLAP PHASE DESIGNATION:

OVERLAP LETTER	PERMISSIVE PHASE	PROTECTED PHASE
B	= 4 +	5
D	= 8 +	1

PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE



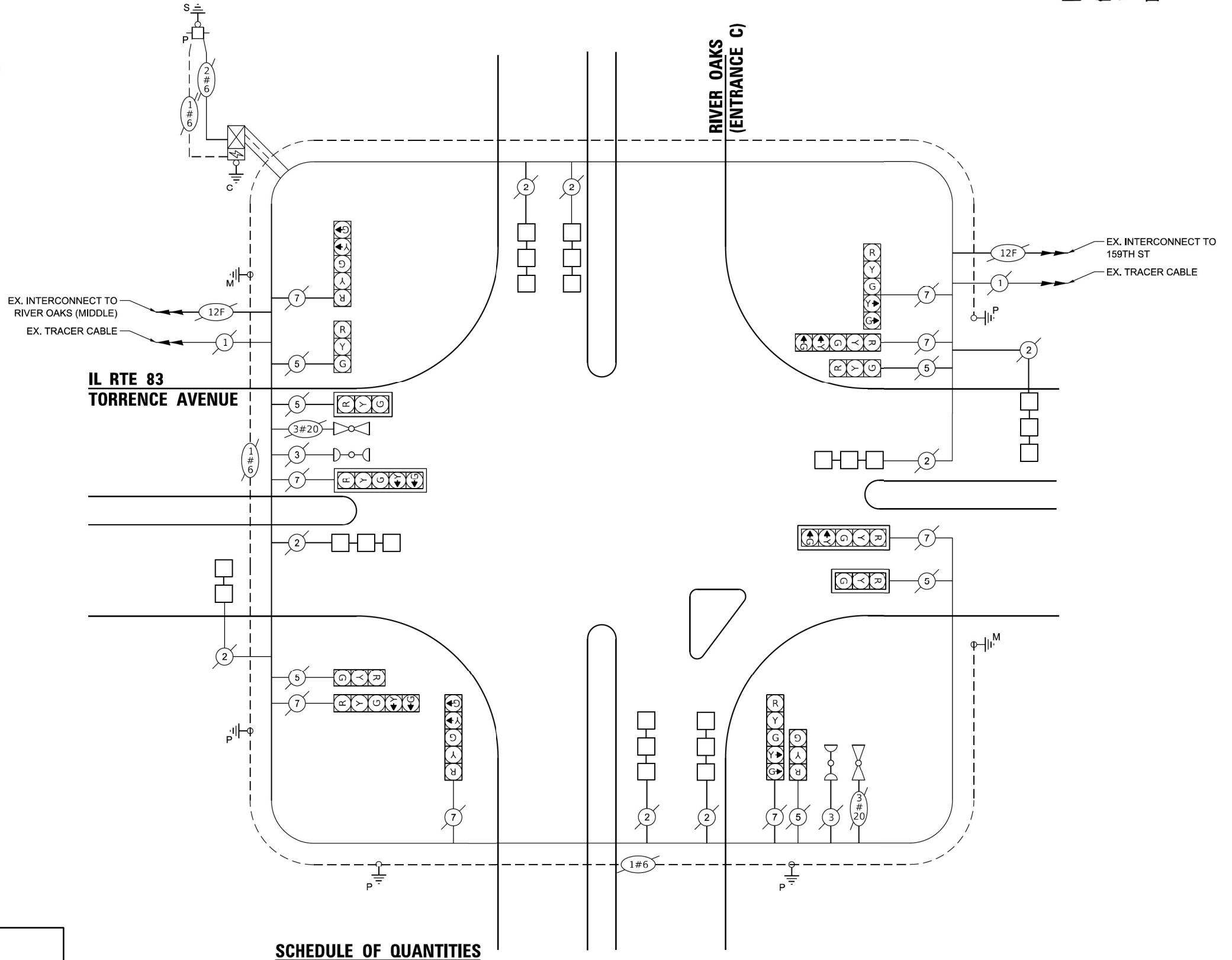
FILE NAME: M:\2019\3_Civil\8034500\DOT\FIT_180 - Various Traffic Signal Design\Graphics\23-013_62700 & 62710 & 62758\23B\CAD\62718 Sheet 06 Torrence at River Oaks-North.dgn

TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	TOTAL WATTAGE
SIGNAL HEAD			
3 - SECTION	6	11	66
4 - SECTION	0	14	0
5 - SECTION	8	13	104
PROGRAMMABLE SIGNALS			
3 - SECTION	0	22	0
4 - SECTION	0	32	0
5 - SECTION	0	28	0
PED. SIGNAL	0	15	0
CONTROLLER	1	150	150
MASTER CONTROLLER	0	100	0
UPS	1	25	25
DETECTION RADAR	-	20	-
VIDEO	-	20	-
BLANK-OUT SIGN	-	25	-
NETWORK SWITCH II OR III	-	35	-
CELLULAR MODEM	-	15	-
TOTAL UPS SIZING =			345
UPS CHARGING	1	225	225
BATTERY HEATER MAT	1	180	180
CABINET HEATER	1	200	200
FLASHER	1	15	15
LED STREET NAME SIGN	-	120	-
LUMINAIRE	-	240	-
TOTAL SERVICE WIRE SIZING =			965

ENERGY COSTS TO:
CALUMET CITY
 204 PULASKI ROAD
 CALUMET CITY, IL 60409

ENERGY SUPPLY:
 CONTACT: NEW BUSINESS DEPT
 PHONE: 866-639-3532
 COMPANY: COMED
 ACCOUNT NUMBER: 07730-19122



SCHEDULE OF QUANTITIES

ITEM DESCRIPTION	UNITS	TOTAL QTY
DETECTOR LOOP REPLACEMENT	FOOT	982

CABLE PLAN
(NOT TO SCALE)

TS SHT NO. 21

Sam Schwartz
 200 S. WACKER SUITE 1400
 CHICAGO, IL 60606
 TEL: (773) 305-0800

USER NAME = erica.salutz	DESIGNED - ERICA SALUTZ	REVISED -
PLOT SCALE = 40.0000" / ft.	DRAWN - KATHLEEN MURPHY	REVISED -
PLOT DATE = 10/20/2023	CHECKED - PETER WOJTKIEWICZ	REVISED -
	DATE - 10/19/2023	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

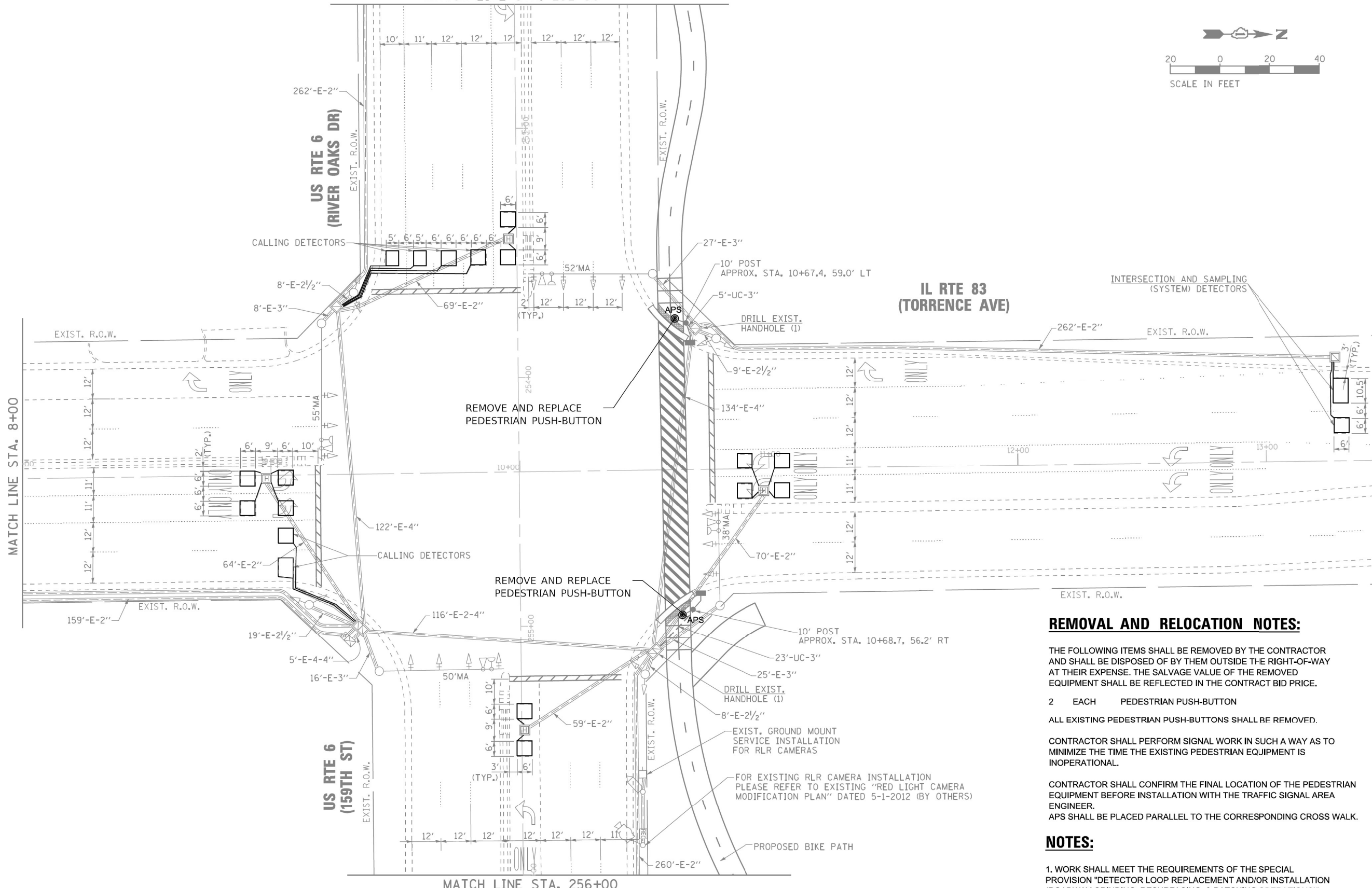
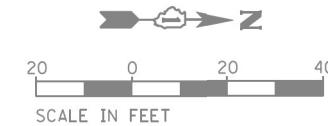
CABLE PLAN, PHASE DESIGNATION DIAGRAM, EMERGENCY VEHICLE PREEMPTION SEQUENCE, AND SUMMARY OF QUANTITIES
IL 83 (TORRENCE AVE) AT RIVER OAKS CENTER DR (NORTH ENTRANCE)

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 358	SECTION FAP 0358 22 RS	COUNTY COOK	TOTAL SHEETS 71	SHEET NO. 43
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T58	

TS 355
ECON 49

MATCH LINE STA. 252+50



REMOVAL AND RELOCATION NOTES:

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 2 EACH PEDESTRIAN PUSH-BUTTON
- ALL EXISTING PEDESTRIAN PUSH-BUTTONS SHALL BE REMOVED.

CONTRACTOR SHALL PERFORM SIGNAL WORK IN SUCH A WAY AS TO MINIMIZE THE TIME THE EXISTING PEDESTRIAN EQUIPMENT IS INOPERATIONAL.

CONTRACTOR SHALL CONFIRM THE FINAL LOCATION OF THE PEDESTRIAN EQUIPMENT BEFORE INSTALLATION WITH THE TRAFFIC SIGNAL AREA ENGINEER. APS SHALL BE PLACED PARALLEL TO THE CORRESPONDING CROSS WALK.

NOTES:

- 1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, & PATCHING OPERATIONS)".

TS SHT NO. 22

TS 175
ECON 49

Sam Schwartz
200 S. WACKER
SUITE 1400
CHICAGO, IL 60606
TEL: (773) 305-0800

USER NAME = erica.salutz	DESIGNED - ERICA SALUTZ	REVISED -
	DRAWN - KATHLEEN MURPHY	REVISED -
PLOT SCALE = 40,0000' / 1"	CHECKED - PETER WOJTKIEWICZ	REVISED -
PLOT DATE = 10/20/2023	DATE - 10/19/2023	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

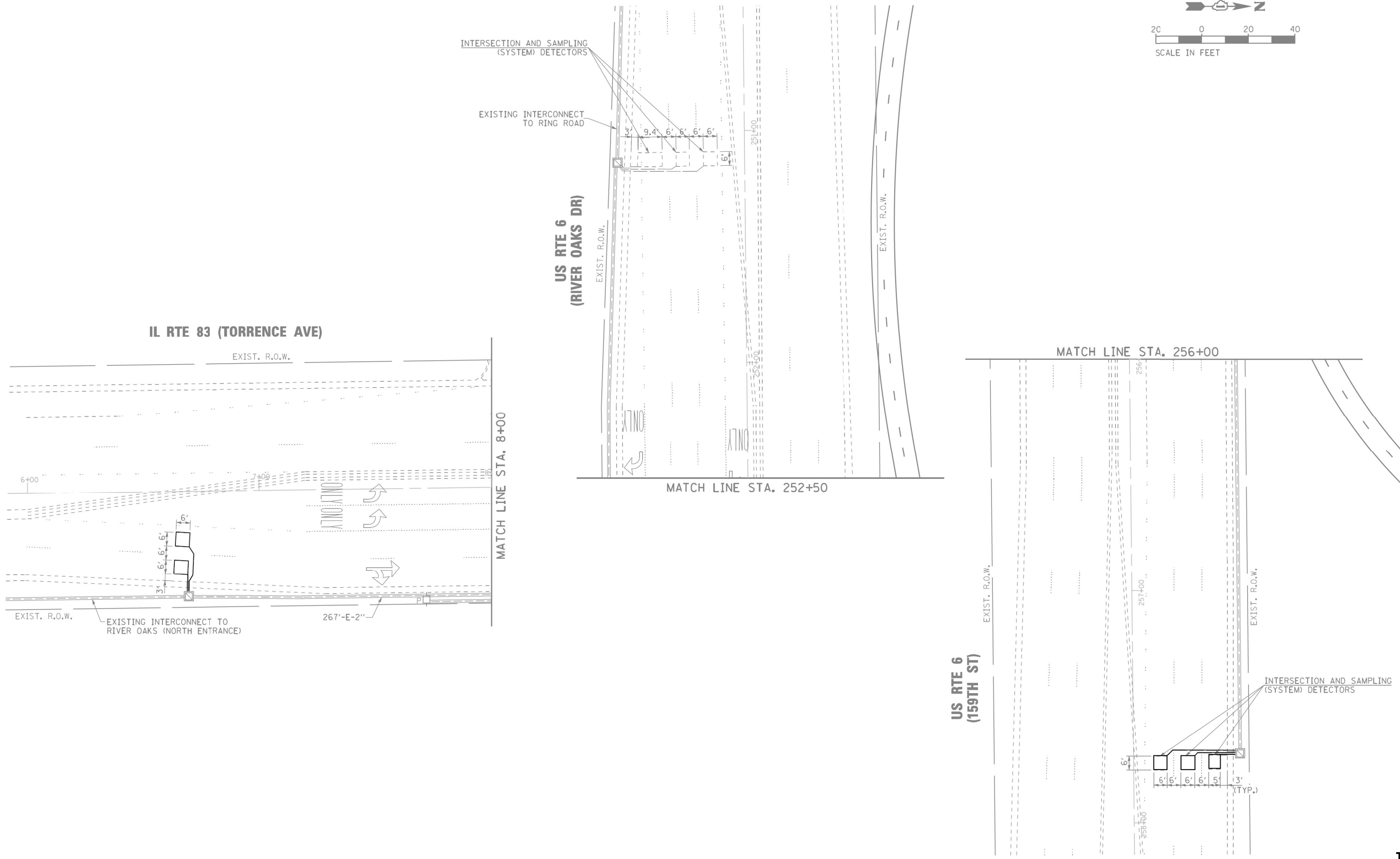
**TRAFFIC SIGNAL MODERNIZATION PLAN (SHEET 1 OF 2)
US 6 (TORRENCE AVE) AT 159TH ST (RIVER OAKS DR)**

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
358	FAP 0358 22 RS	COOK	71	44
CONTRACT NO. 62T58				
ILLINOIS FED. AID PROJECT				

FILE NAME: M:\2019\3_Civil\18-03-5500 DOT_PTB_190 - Various Traffic Signal Design\Graphics\W0_23-013_62T000 & 62T110 & 62T58E2T58CAD\62T58 Sheet 07 Torrence at 159th-River Oaks.dwg

FILE NAME: M:\2019\3_Civil\18-03-5500 DOT FTB 190 - Various Traffic Signal Design\Graphics\23-03-62700 & 62110 & 62158\23B\CAD\62158 Sheet 07 Torrence at 159th-River Oaks.dgn



Sam Schwartz
200 S. WACKER
SUITE 1400
CHICAGO, IL 60606
TEL: (773) 305-0800

USER NAME = erica.salutz	DESIGNED - ERICA SALUTZ	REVISED -
	DRAWN - KATHLEEN MURPHY	REVISED -
PLOT SCALE = 40,0000 * / ft.	CHECKED - PETER WOJTKIEWICZ	REVISED -
PLOT DATE = 10/20/2023	DATE - 10/19/2023	REVISED -

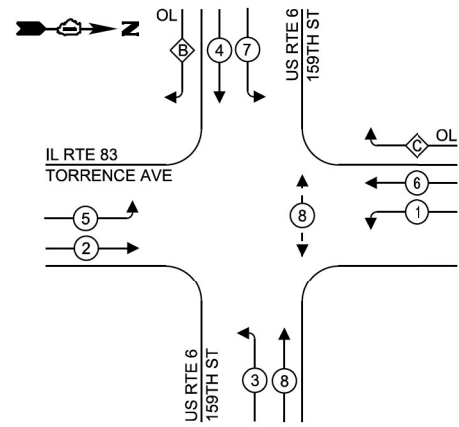
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL MODERNIZATION PLAN (SHEET 2 OF 2)
US 6 (TORRENCE AVE) AT 159TH ST (RIVER OAKS DR)

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
--------	-------	----	--------	------	----	------

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
358	FAP 0358 22 RS	COOK	71	45
CONTRACT NO. 62T58				
ILLINOIS FED. AID PROJECT				

PROPOSED CONTROLLER SEQUENCE



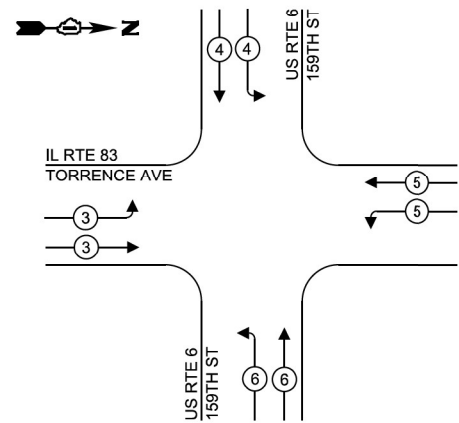
LEGEND:

- ← * → PROTECTED PHASE
- ← - * → PROTECTED/PERMITTED PHASE
- ← * → PEDESTRIAN PHASE
- ← * → OVERLAP

RIGHT TURN OVERLAP PHASE DESIGNATION:

OVERLAP LETTER	PERMISSIVE PHASE	PROTECTED PHASE
B	= 4 +	5
C	= 6 +	7

PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE



FILE NAME: M:\2018\3_Civil\8034500_DDOT_FTB_180 - Various Traffic Signal Design\Graphics\2-2-03_62110_6_6215827B7C4D6E17B_Sheet_07_Torrence at 159th-River Oaks.dwg

TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	TOTAL WATTAGE
SIGNAL HEAD			
3 - SECTION	18	11	198
4 - SECTION	0	14	0
5 - SECTION	4	13	52
PROGRAMMABLE SIGNALS			
3 - SECTION	0	22	0
4 - SECTION	0	32	0
5 - SECTION	0	28	0
PED. SIGNAL	2	15	30
CONTROLLER	1	150	150
MASTER CONTROLLER	1	100	100
UPS	1	25	25
DETECTION RADAR	-	20	-
VIDEO	-	20	-
BLANK-OUT SIGN	-	25	-
NETWORK SWITCH II OR III	-	35	-
CELLULAR MODEM	-	15	-
TOTAL UPS SIZING =			555
UPS CHARGING	1	225	225
BATTERY HEATER MAT	1	180	180
CABINET HEATER	1	200	200
FLASHER	1	15	15
LED STREET NAME SIGN	-	120	-
LUMINAIRE	-	240	-
TOTAL SERVICE WIRE SIZING =			1175

SCHEDULE OF QUANTITIES

ITEM DESCRIPTION	UNITS	TOTAL QTY
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
DETECTOR LOOP, TYPE I	FOOT	1027
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	2

ENERGY COSTS TO:
CALUMET CITY
 204 PULASKI ROAD
 CALUMET CITY, IL 60409

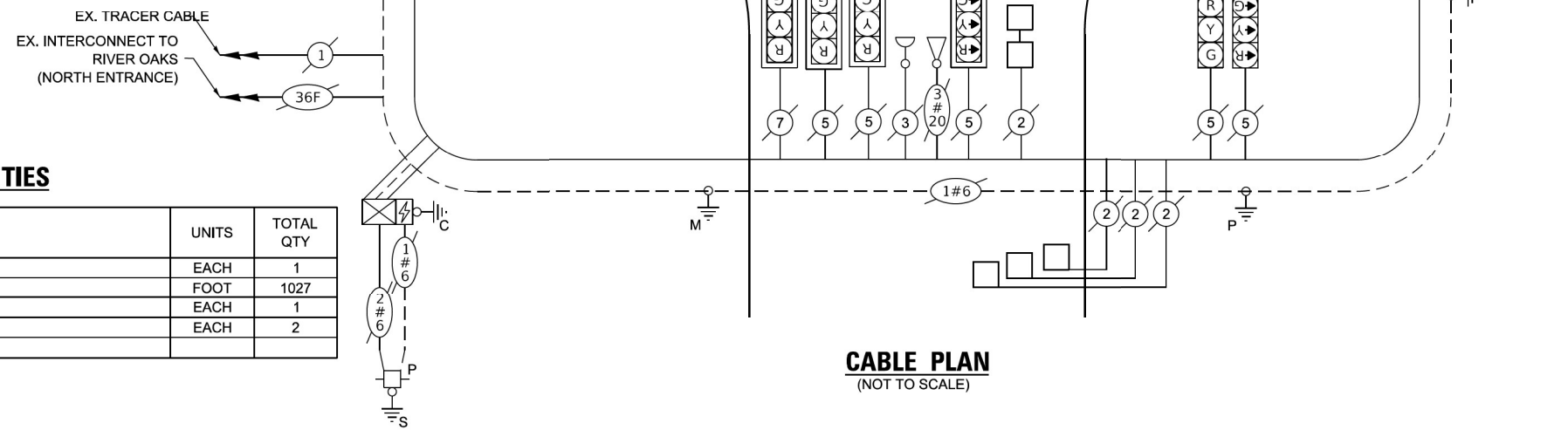
ENERGY SUPPLY:

CONTACT: NEW BUSINESS DEPT
 PHONE: 866-639-3532
 COMPANY: COMED
 ACCOUNT NUMBER: 07730-19122

EX. TRACER CABLE
 EX. INTERCONNECT TO RIVER OAKS (NORTH ENTRANCE)

IL RTE 83 (TORRENCE AVE)

US RTE 6 (159TH ST)



TS SHT NO. 24

Sam Schwartz
 200 S. WACKER SUITE 1400 CHICAGO, IL 60606 TEL: (773) 305-0800

USER NAME = erica.salutz	DESIGNED - ERICA SALUTZ	REVISED -
PLOT SCALE = 40,0000' / 1"	DRAWN - KATHLEEN MURPHY	REVISED -
PLOT DATE = 10/20/2023	CHECKED - PETER WOJTKIEWICZ	REVISED -
	DATE - 10/19/2023	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

**CABLE PLAN, PHASE DESIGNATION DIAGRAM, EMERGENCY VEHICLE PREEMPTION SEQUENCE, AND SUMMARY OF QUANTITIES
 IL RTE 83 (TORRENCE AVE) AT US RTE 6 (159TH ST)**

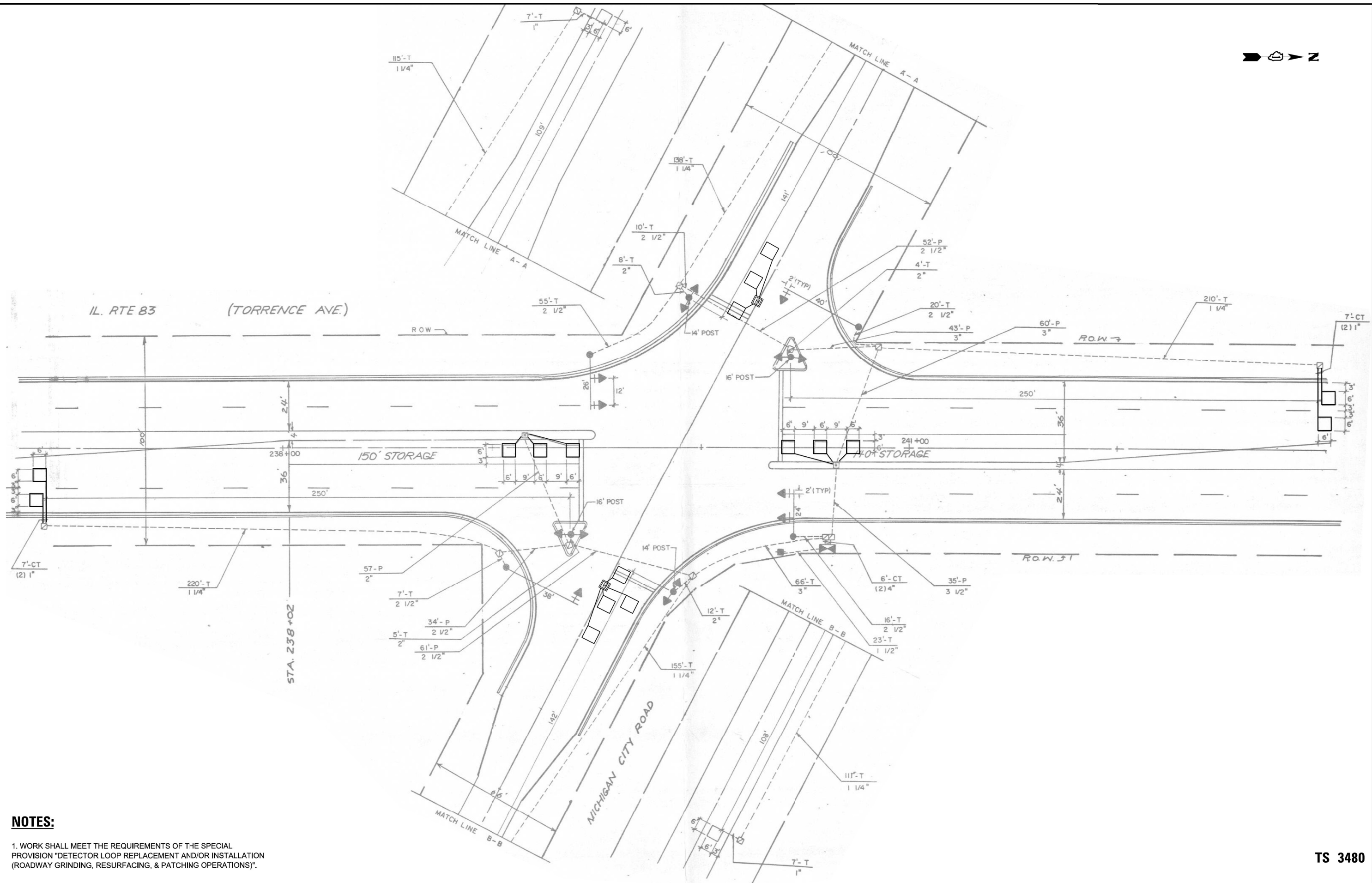
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 358	SECTION FAP 0358 22 RS	COUNTY COOK	TOTAL SHEETS 71	SHEET NO. 46
CONTRACT NO. 62T58				
ILLINOIS FED. AID PROJECT				

**TS 175
 ECON 49**

TS SHT NO. 25

FILE NAME: M:\2019\3_Civil\18-03-2500 DOT FTB 190 - Various Traffic Signal Design\Graphics\23-03-62700 & 62110 & 62158\23B\CAD\62158 Sheet 08 Torrence at Michigan City.dgn



NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, & PATCHING OPERATIONS)".

TS 3480

Sam Schwartz
 200 S. WACKER
 SUITE 1400
 CHICAGO, IL 60606
 TEL: (773) 305-0800

USER NAME = erica.salutz	DESIGNED - ERICA SALUTZ	REVISED -
	DRAWN - KATHLEEN MURPHY	REVISED -
PLOT SCALE = 40,0000' / 1"	CHECKED - PETER WOJTKIEWICZ	REVISED -
PLOT DATE = 10/20/2023	DATE - 10/19/2023	REVISED -

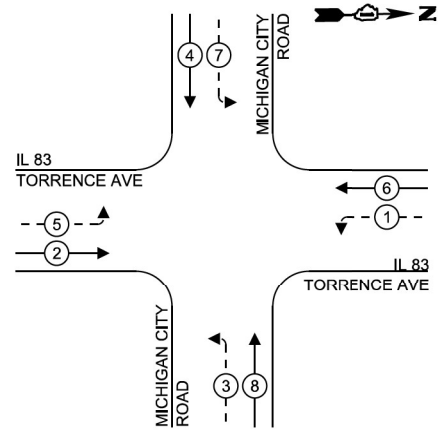
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**TRAFFIC SIGNAL MODERNIZATION PLAN
 IL 83 (TORRENCE AVE) AT MICHIGAN CITY**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 358	SECTION FAP 0358 22 RS	COUNTY COOK	TOTAL SHEETS 71	SHEET NO. 47
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T58	

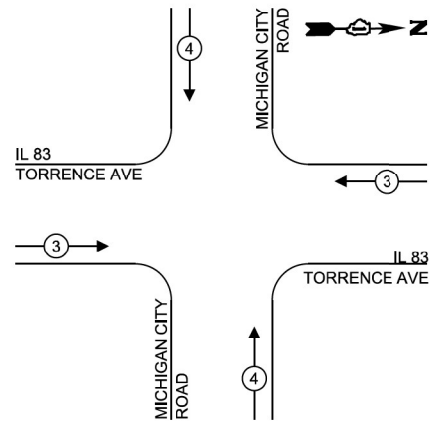
PROPOSED CONTROLLER SEQUENCE



LEGEND:

- ← (circle with asterisk) → PROTECTED PHASE
- ← (circle with asterisk and dashed line) → PROTECTED/PERMITTED PHASE
- ← (circle with asterisk and arrow) → PEDESTRIAN PHASE
- ← (diamond with asterisk) → OVERLAP

PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE



TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	TOTAL WATTAGE
SIGNAL HEAD			
3 - SECTION	4	11	44
4 - SECTION	0	14	0
5 - SECTION	8	13	104
PROGRAMMABLE SIGNALS			
3 - SECTION	0	22	0
4 - SECTION	0	32	0
5 - SECTION	0	28	0
PED. SIGNAL			
CONTROLLER	1	150	150
MASTER CONTROLLER	0	100	0
UPS	1	25	25
DETECTION RADAR	-	20	-
VIDEO	-	20	-
BLANK-OUT SIGN	-	25	-
NETWORK SWITCH II OR III	-	35	-
CELLULAR MODEM	-	15	-
TOTAL UPS SIZING =			323
UPS CHARGING	1	225	225
BATTERY HEATER MAT	1	180	180
CABINET HEATER	1	200	200
FLASHER	1	15	15
LED STREET NAME SIGN	-	120	-
LUMINAIRE	-	240	-
TOTAL SERVICE WIRE SIZING =			943

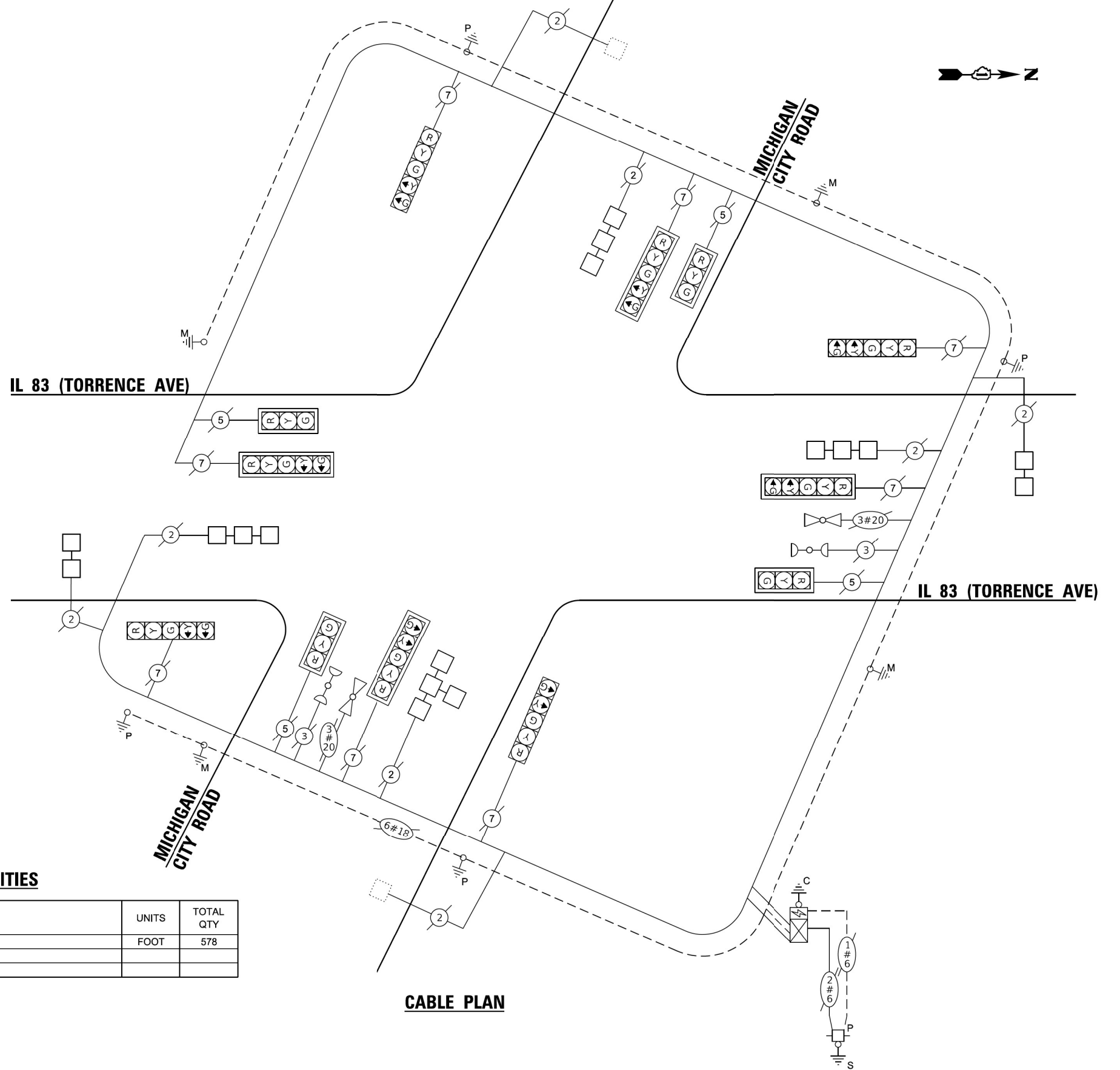
SCHEDULE OF QUANTITIES

ITEM DESCRIPTION	UNITS	TOTAL QTY
DETECTOR LOOP REPLACEMENT	FOOT	578

ENERGY COSTS TO:
ILLINOIS DEPARTMENT OF TRANSPORTATION
 201 WEST CENTER COURT
 SCHAUMBURG, IL 60196

ENERGY SUPPLY:
 CONTACT: NEW BUSINESS DEPT
 PHONE: 866-639-3532
 COMPANY: COMED
 ACCOUNT NUMBER: -

IL 83 (TORRENCE AVE)



CABLE PLAN

TS 3480

TS SHT NO. 26

Sam Schwartz
 200 S. WACKER
 SUITE 1400
 CHICAGO, IL 60606
 TEL: (773) 305-0800

USER NAME = erica.salutz	DESIGNED - ERICA SALUTZ	REVISED -
PLOT SCALE = 40,0000' / 1"	DRAWN - KATHLEEN MURPHY	REVISED -
PLOT DATE = 10/20/2023	CHECKED - PETER WOJTKIEWICZ	REVISED -
	DATE - 10/19/2023	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CABLE PLAN, PHASE DESIGNATION DIAGRAM, EMERGENCY VEHICLE
 PREEMPTION SEQUENCE, AND SUMMARY OF QUANTITIES
 IL 83 (TORRENCE AVE) AT MICHIGAN CITY**

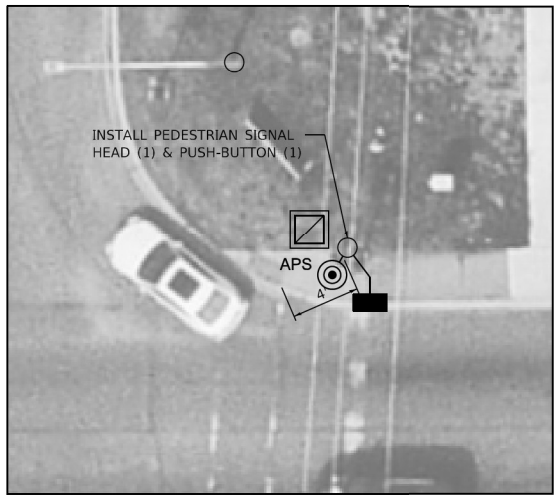
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 358	SECTION FAP 0358 22 RS	COUNTY COOK	TOTAL SHEETS 71	SHEET NO. 48
CONTRACT NO. 62T58				
ILLINOIS FED. AID PROJECT				

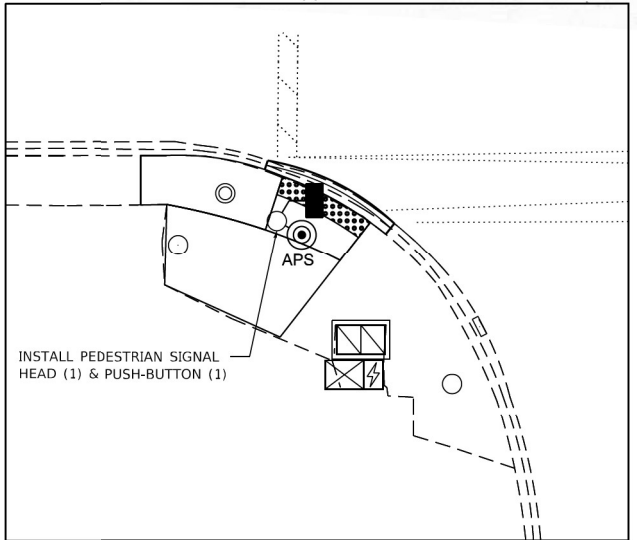
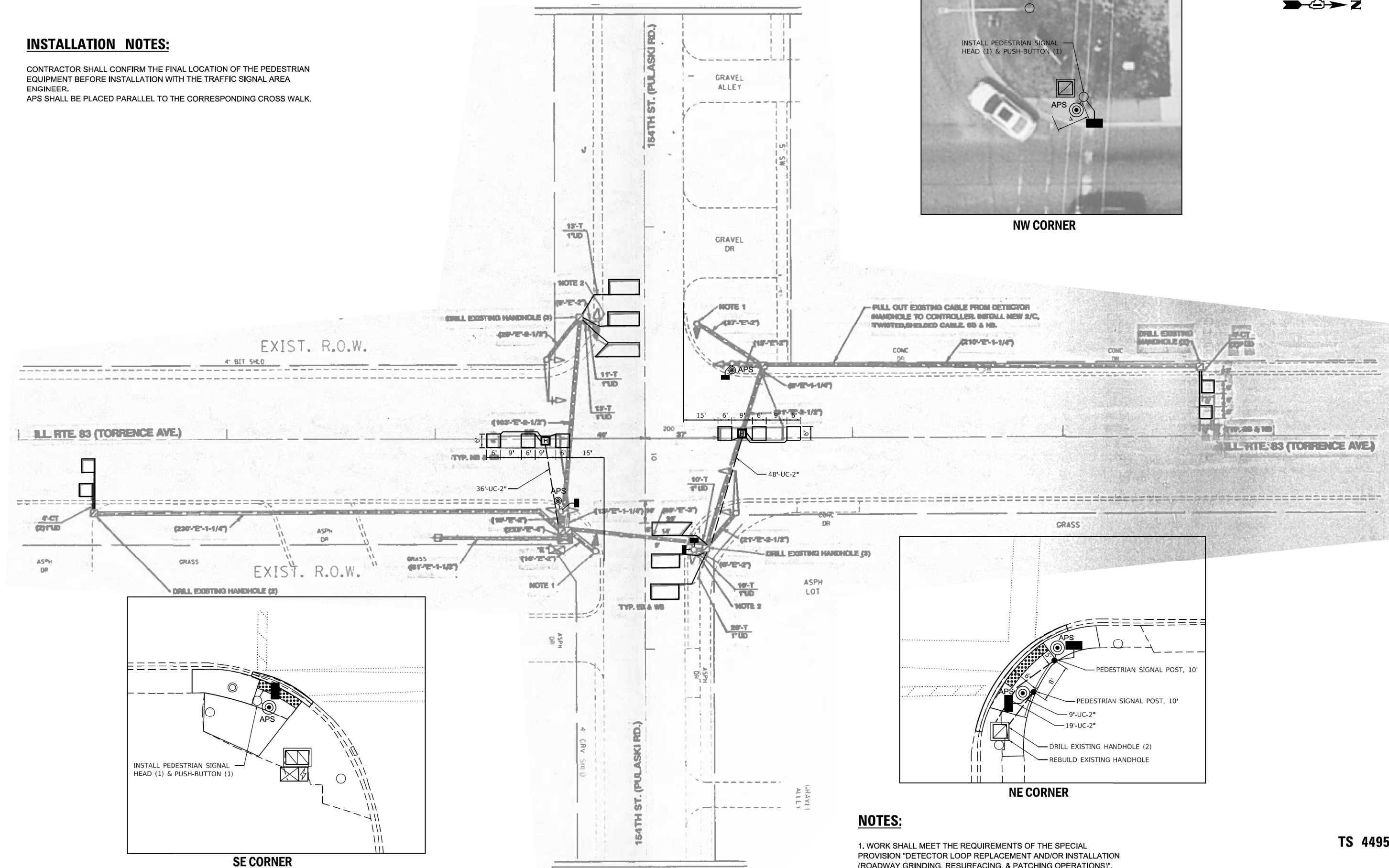
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INSTALLATION NOTES:

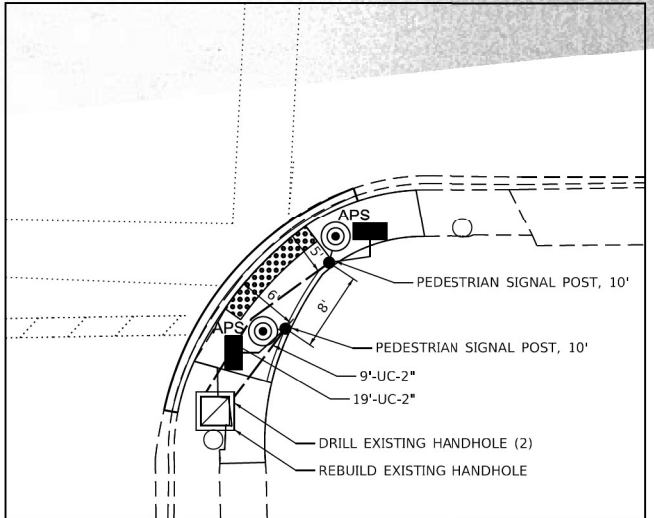
CONTRACTOR SHALL CONFIRM THE FINAL LOCATION OF THE PEDESTRIAN EQUIPMENT BEFORE INSTALLATION WITH THE TRAFFIC SIGNAL AREA ENGINEER.
 APS SHALL BE PLACED PARALLEL TO THE CORRESPONDING CROSS WALK.



NW CORNER



SE CORNER



NE CORNER

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, & PATCHING OPERATIONS)".

TS 4495

TS SHT NO. 27

Sam Schwartz
 200 S. WACKER
 SUITE 1400
 CHICAGO, IL 60606
 TEL: (773) 305-0800

USER NAME = erica.salutz	DESIGNED - ERICA SALUTZ	REVISED -
PLOT SCALE = 40,0000' / 1"	DRAWN - KATHLEEN MURPHY	REVISED -
PLOT DATE = 10/20/2023	CHECKED - PETER WOJTKIEWICZ	REVISED -
	DATE - 10/19/2023	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

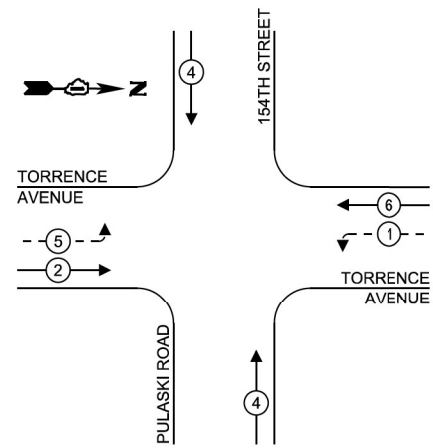
**TRAFFIC SIGNAL MODERNIZATION PLAN
 IL 83 (TORRENCE AVE) AT 154TH (PULASKI RD)**

SCALE: SHEET OF SHEETS STA. TO STA.

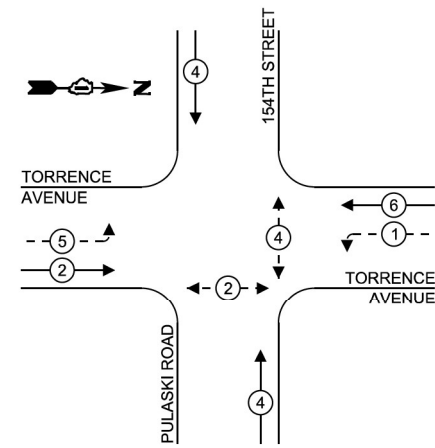
F.A.P. RTE. 358	SECTION FAP 0358 22 RS	COUNTY COOK	TOTAL SHEETS 71	SHEET NO. 49
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T58	

FILE NAME: M:\2019\3_Civil\803-2500 DOT FTB 190 - Various Traffic Signal Design\Graphics\W0 2-013 62R00 & 62T10 & 62T58\27B\CAD\62T58 Sheet 09 Torrence at 154th-Pulaski.dgn

EXISTING CONTROLLER SEQUENCE



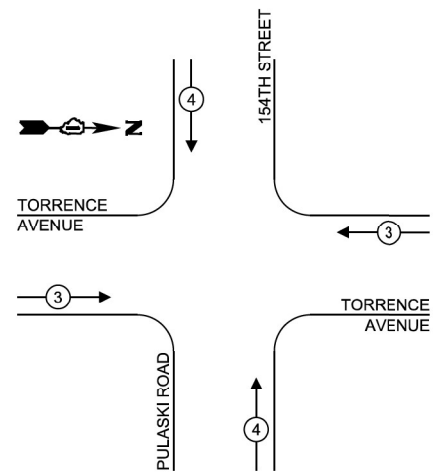
PROPOSED CONTROLLER SEQUENCE



LEGEND:

- ← ⊙ → PROTECTED PHASE
- ← ⊙ - - ⊙ → PROTECTED/PERMITTED PHASE
- ← ⊙ → PEDESTRIAN PHASE
- ← ⊙ OL → OVERLAP

**PROPOSED EMERGENCY VEHICLE
PREEMPTION SEQUENCE**



SCHEDULE OF QUANTITIES

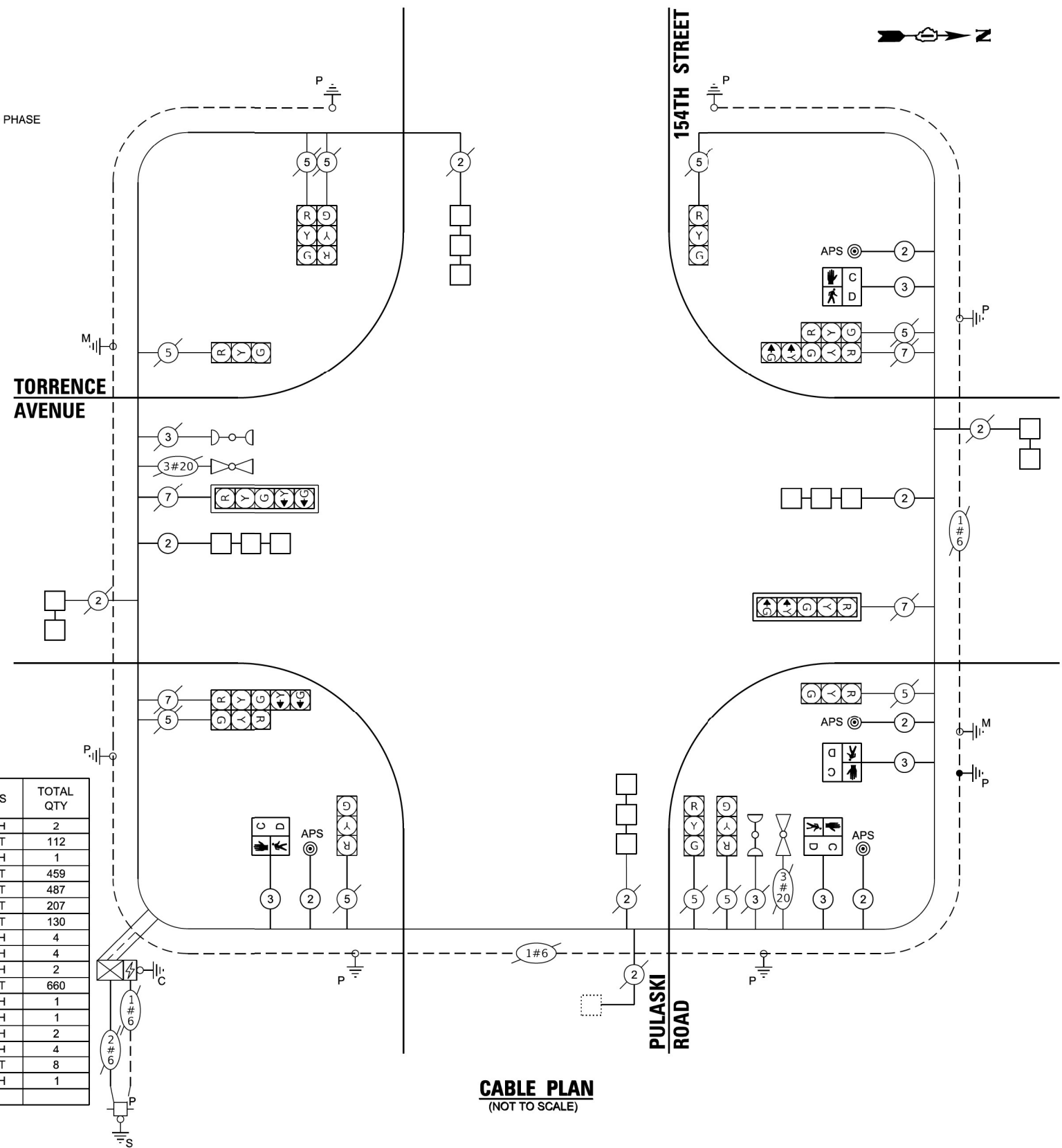
ITEM DESCRIPTION	UNITS	TOTAL QTY
HEAVY-DUTY HANDHOLE	EACH	2
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	112
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	459
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	487
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	207
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	130
DRILL EXISTING HANDHOLE	EACH	4
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	4
INDUCTIVE LOOP DETECTOR	EACH	2
DETECTOR LOOP, TYPE I	FOOT	660
MODIFY EXISTING CONTROLLER	EACH	1
REBUILD EXISTING HANDHOLE	EACH	1
PEDESTRIAN SIGNAL POST, 10 FT.	EACH	2
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	4
CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	FOOT	8
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1

ENERGY COSTS TO:
CALUMET CITY
 204 PULASKI ROAD
 CALUMET CITY, IL 60409

ENERGY SUPPLY:
 CONTACT: NEW BUSINESS DEPT
 PHONE: 866-639-3532
 COMPANY: COMED
 ACCOUNT NUMBER: 07730-19122

**TRAFFIC SIGNAL
ELECTRICAL SERVICE REQUIREMENTS**

TYPE	NO. OF LAMPS	LED WATTAGE	TOTAL WATTAGE
SIGNAL HEAD			
3 - SECTION	10	11	110
4 - SECTION	0	14	0
5 - SECTION	4	13	52
PROGRAMMABLE SIGNALS			
3 - SECTION	0	22	0
4 - SECTION	0	32	0
5 - SECTION	0	28	0
PED. SIGNAL			
CONTROLLER	1	150	150
MASTER CONTROLLER	0	100	0
UPS			
DETECTION RADAR	-	20	-
VIDEO	-	20	-
BLANK-OUT SIGN	-	25	-
NETWORK SWITCH II OR III	-	35	-
CELLULAR MODEM	-	15	-
TOTAL UPS SIZING =			397
UPS CHARGING	1	225	225
BATTERY HEATER MAT	1	180	180
CABINET HEATER	1	200	200
FLASHER	1	15	15
LED STREET NAME SIGN	-	120	-
LUMINAIRE	-	240	-
TOTAL SERVICE WIRE SIZING =			1017



CABLE PLAN
(NOT TO SCALE)

TS SHT NO. 28

Sam Schwartz
 200 S. WACKER
 SUITE 1400
 CHICAGO, IL 60606
 TEL: (773) 305-0800

USER NAME = erica.salutz
DESIGNED - ERICA SALUTZ
DRAWN - KATHLEEN MURPHY
CHECKED - PETER WOJTKIEWICZ
DATE - 10/19/2023
REVISIONS
REVISED -
REVISED -
REVISED -
REVISED -

SCALE = 40,0000' / 1"
DATE = 10/20/2023

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CABLE PLAN, PHASE DESIGNATION DIAGRAM, EMERGENCY VEHICLE
PREEMPTION SEQUENCE, AND SUMMARY OF QUANTITIES
IL 83 (TORRENCE AVE) AT 154TH (PULASKI RD)**

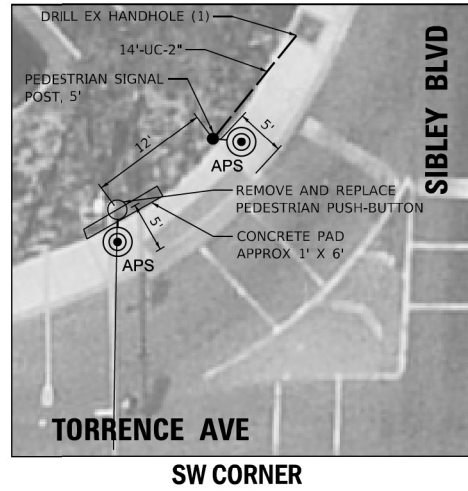
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 358	SECTION FAP 0358 22 RS	COUNTY COOK	TOTAL SHEETS 71	SHEET NO. 50
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T58	

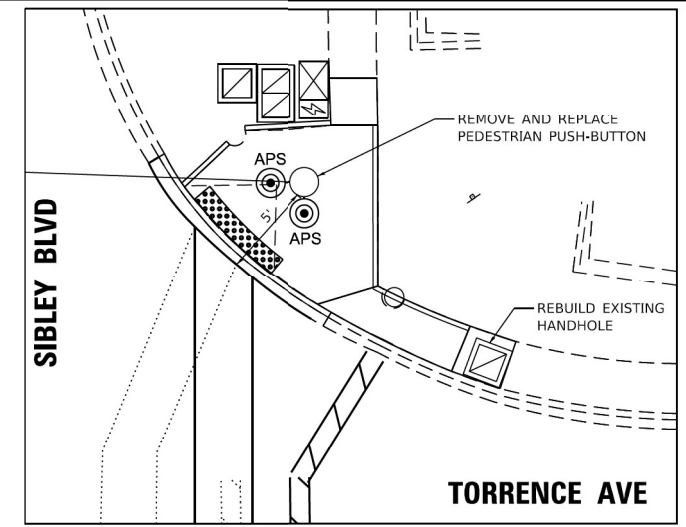
TS 4495

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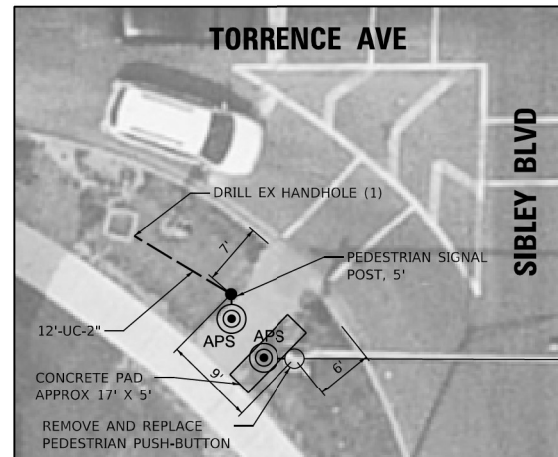
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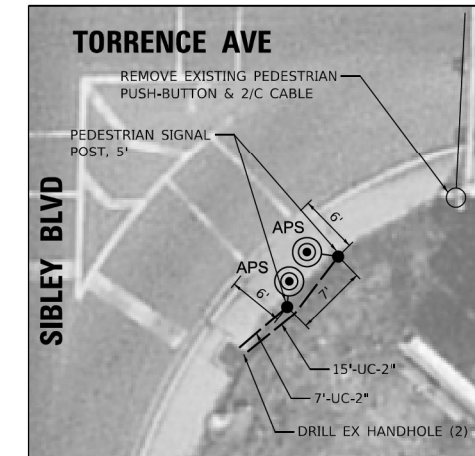
TORRENCE AVE
SW CORNER



SIBLEY BLVD
TORRENCE AVE
NW CORNER



TORRENCE AVE
SE CORNER



TORRENCE AVE
SIBLEY BLVD
NE CORNER

REMOVAL AND RELOCATION NOTES:

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

4 EACH PEDESTRIAN PUSH-BUTTON

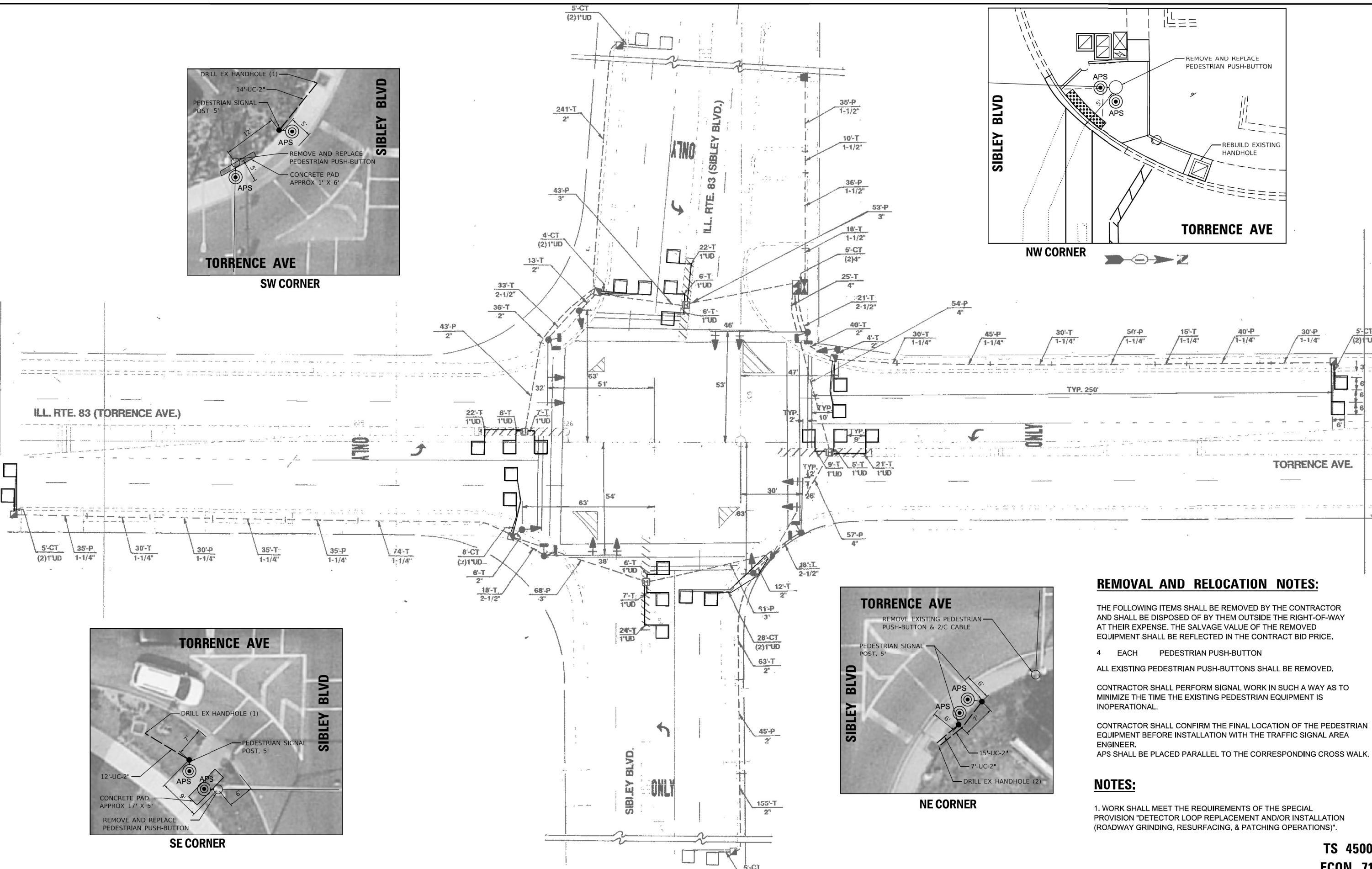
ALL EXISTING PEDESTRIAN PUSH-BUTTONS SHALL BE REMOVED.

CONTRACTOR SHALL PERFORM SIGNAL WORK IN SUCH A WAY AS TO MINIMIZE THE TIME THE EXISTING PEDESTRIAN EQUIPMENT IS INOPERATIONAL.

CONTRACTOR SHALL CONFIRM THE FINAL LOCATION OF THE PEDESTRIAN EQUIPMENT BEFORE INSTALLATION WITH THE TRAFFIC SIGNAL AREA ENGINEER. APS SHALL BE PLACED PARALLEL TO THE CORRESPONDING CROSS WALK.

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, & PATCHING OPERATIONS)".



Sam Schwartz
200 S. WACKER
SUITE 1400
CHICAGO, IL 60606
TEL: (773) 305-0800

USER NAME = erica.salutz	DESIGNED - ERICA SALUTZ	REVISED -
PLOT SCALE = 40.0000' / 1"	DRAWN - KATHLEEN MURPHY	REVISED -
PLOT DATE = 10/20/2023	CHECKED - PETER WOJTKIEWICZ	REVISED -
	DATE - 10/19/2023	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

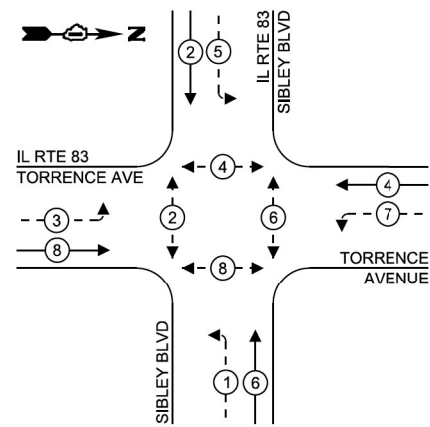
TRAFFIC SIGNAL MODERNIZATION PLAN
US 6 (TORRENCE AVE) AT SIBLEY BLVD

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
358	FAP 0358 22 RS	COOK	71	51
CONTRACT NO. 62T58				
ILLINOIS FED. AID PROJECT				

TS 4500
ECON 71

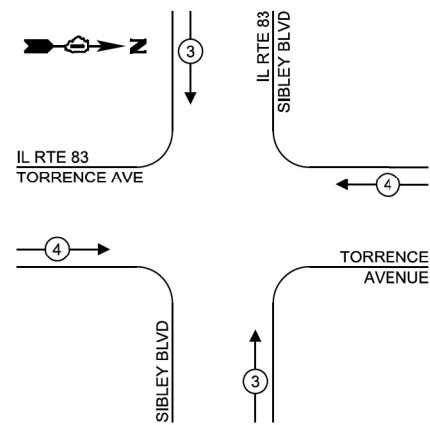
PROPOSED CONTROLLER SEQUENCE



LEGEND:

- ← * → PROTECTED PHASE
- ← - * - → PROTECTED/PERMITTED PHASE
- ← * → PEDESTRIAN PHASE
- ← * → OL OVERLAP

PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE



FILE NAME: M:\2018\3_Civil\8034500_DDOT_FTB_180 - Various Traffic Signal Design\Graphics\2-24-03_62100 & 62110 & 62158\218CAD\62158 Sheet 10 Torrence at Sibley.dgn

TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

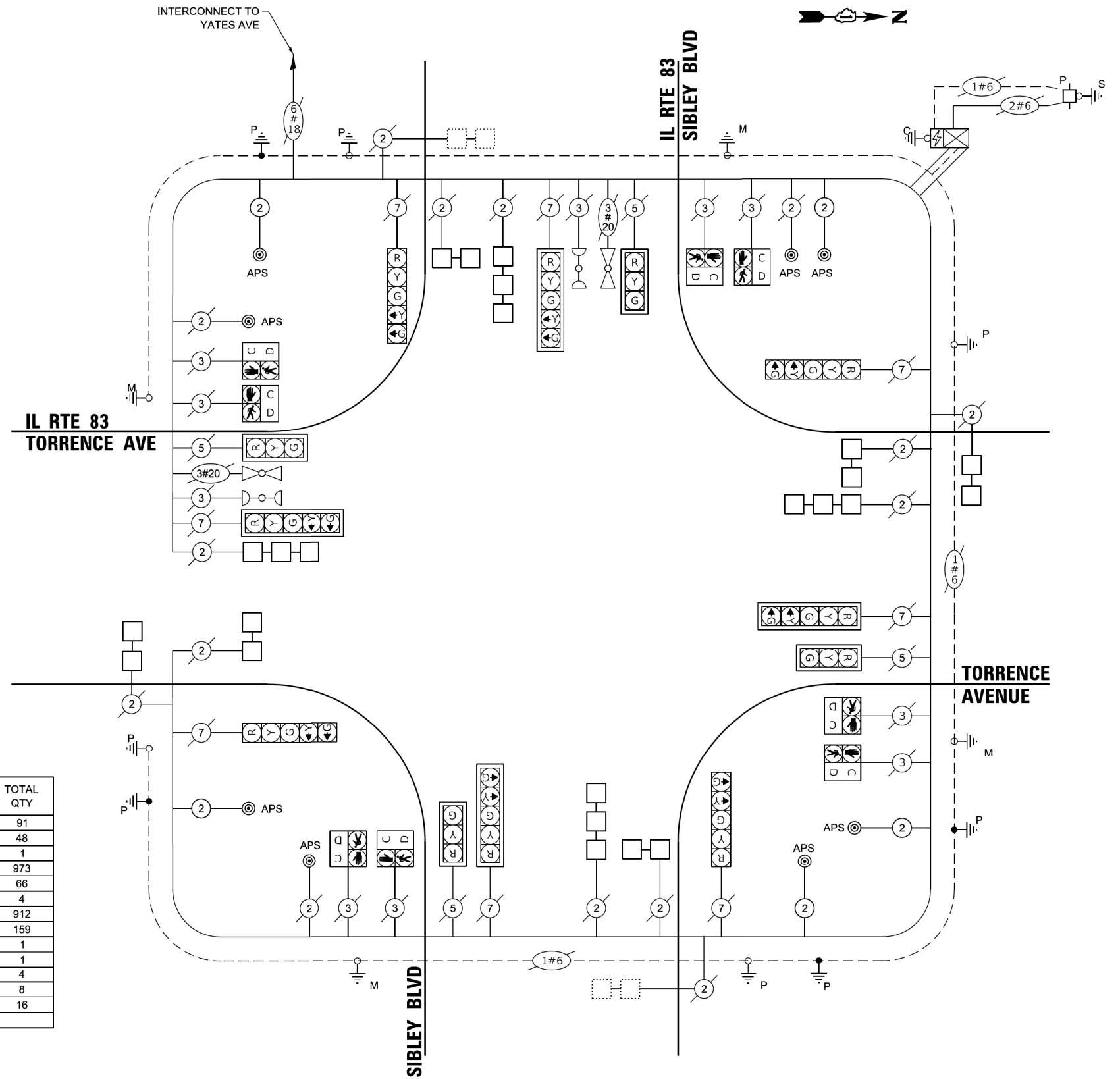
TYPE	NO. OF LAMPS	LED WATTAGE	TOTAL WATTAGE
SIGNAL HEAD			
3 - SECTION	4	11	44
4 - SECTION	0	14	0
5 - SECTION	8	13	104
PROGRAMMABLE SIGNALS			
3 - SECTION	0	22	0
4 - SECTION	0	32	0
5 - SECTION	0	28	0
PED. SIGNAL			
CONTROLLER	1	150	150
MASTER CONTROLLER	0	100	0
UPS	1	25	25
DETECTION			
RADAR	-	20	-
VIDEO	-	20	-
BLANK-OUT SIGN			
NETWORK SWITCH II OR III	-	35	-
CELLULAR MODEM	-	15	-
TOTAL UPS SIZING =			443
UPS CHARGING	1	225	225
BATTERY HEATER MAT	1	180	180
CABINET HEATER	1	200	200
FLASHER	1	15	15
LED STREET NAME SIGN	-	120	-
LUMINAIRE	-	240	-
TOTAL SERVICE WIRE SIZING =			1063

SCHEDULE OF QUANTITIES

ITEM DESCRIPTION	UNITS	TOTAL QTY
PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	91
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	48
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	973
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	66
DRILL EXISTING HANDHOLE	EACH	4
DETECTOR LOOP, TYPE I	FOOT	912
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	159
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REBUILD EXISTING HANDHOLE	EACH	1
PEDESTRIAN SIGNAL POST, 5 FT.	EACH	4
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	8
CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	FOOT	16

ENERGY COSTS TO:
CALUMET CITY
 204 PULASKI ROAD
 CALUMET CITY, IL 60409

ENERGY SUPPLY:
 CONTACT: NEW BUSINESS DEPT
 PHONE: 866-639-3532
 COMPANY: COMED
 ACCOUNT NUMBER: 07730-19122



CABLE PLAN
(NOT TO SCALE)

TS SHT NO. 30

Sam Schwartz
 200 S. WACKER
 SUITE 1400
 CHICAGO, IL 60606
 TEL: (773) 305-0800

USER NAME = erica.salutz	DESIGNED - ERICA SALUTZ	REVISED -
PLOT SCALE = 40,0000 * / ft.	DRAWN - KATHLEEN MURPHY	REVISED -
PLOT DATE = 10/20/2023	CHECKED - PETER WOJKIEWICZ	REVISED -
	DATE - 10/19/2023	REVISED -

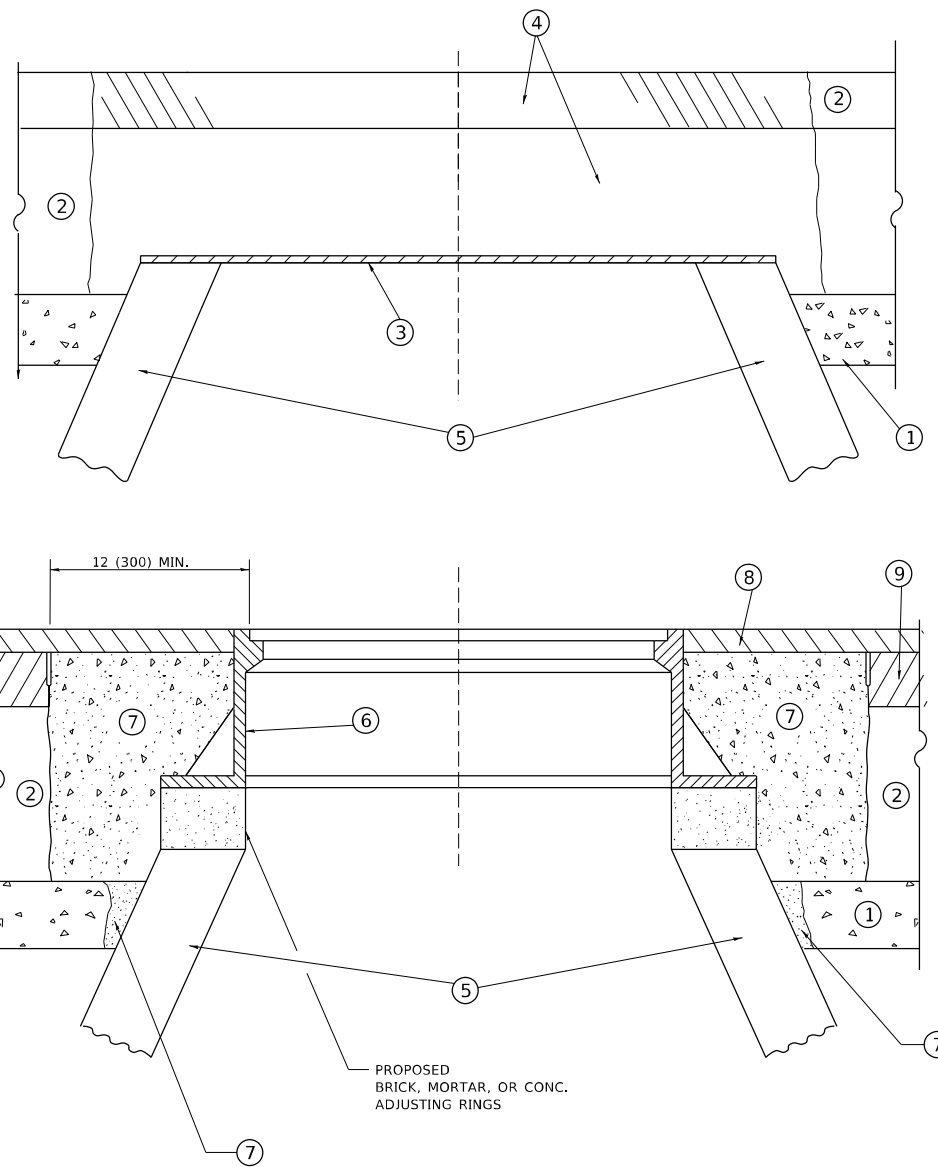
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CABLE PLAN, PHASE DESIGNATION DIAGRAM, EMERGENCY VEHICLE PREEMPTION SEQUENCE, AND SUMMARY OF QUANTITIES
US 6 (TORRENCE AVE) AT SIBLEY BLVD

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 358	SECTION FAP 0358 22 RS	COUNTY COOK	TOTAL SHEETS 71	SHEET NO. 52
CONTRACT NO. 62T58				
ILLINOIS FED. AID PROJECT				

TS 4500
ECON 71



**DETAILS FOR FRAMES AND LIDS ADJUSTMENT
WITH MILLING**

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 3 (80) HMA TO REMAIN AFTER MILLING).

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-2* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-2* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

- 1. REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
- 2. THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
- 3. NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
- 4. WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

NOTES

- 1. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- 2. IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
- 3. CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
- 4. THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
- 5. THE CONTRACTOR SHALL REMOVE ALL TRAFFIC CONTROL DEVICES BY THE END OF EACH WORK SHIFT.

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

USER NAME = Sher,Khan	DESIGNED - R. SHAH	REVISED - R. BORO 03-09-11
	DRAWN -	REVISED - R. BORO 12-06-11
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED - K. SMITH 11-18-22
PLOT DATE = 10/20/2023	DATE - 10-25-94	REVISED - K. SMITH 09-15-23

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0358	FAP 0358 22 RS	COOK	71	53
BD600-03 (BD-08)			CONTRACT NO. 62T58	
ILLINOIS FED. AID PROJECT				

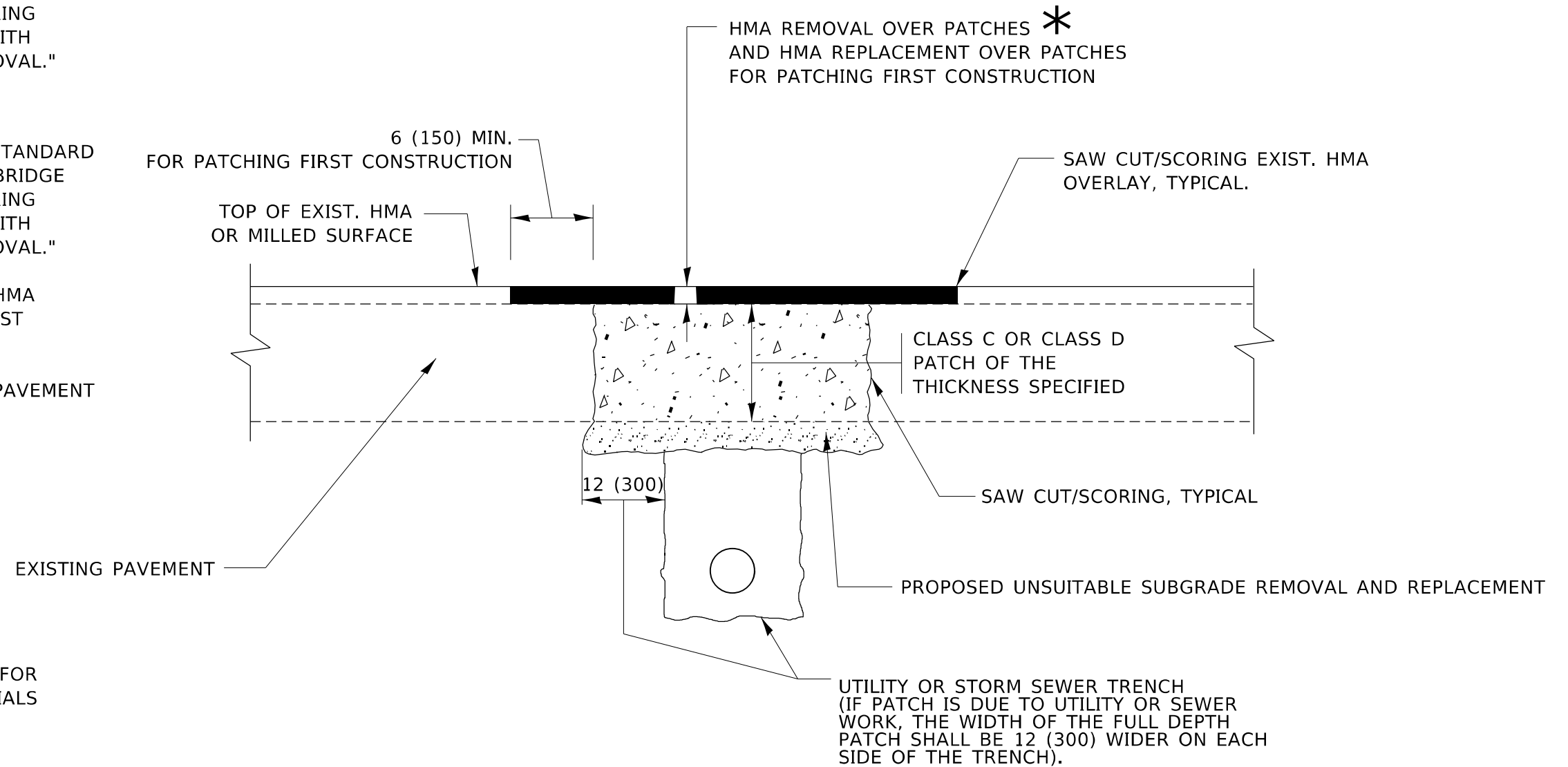
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METHOD OF MEASUREMENT

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

BASIS OF PAYMENT

1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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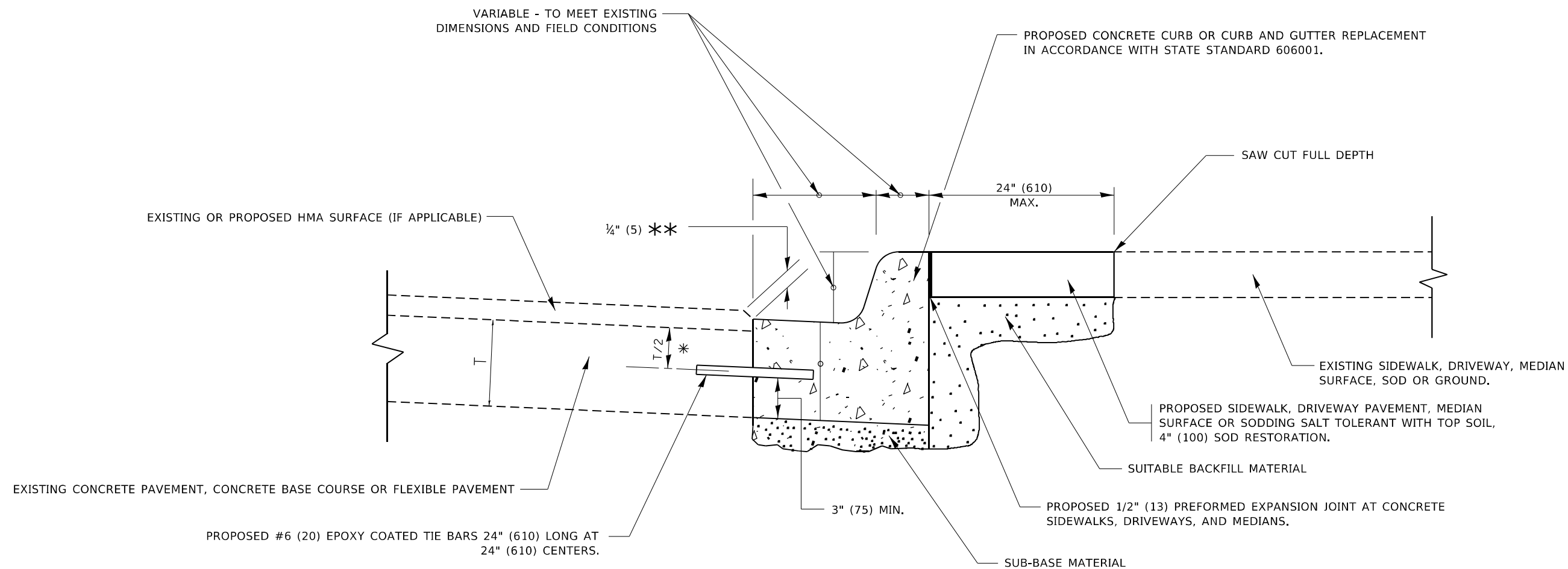
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	DRAWN -	REVISED - R. BORO 09-04-07
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED - K. ENG 10-27-08
PLOT DATE = 10/20/2023	DATE - 10-25-94	REVISED - K. SMITH 11-18-22

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0358	FAP 0358 22 RS	COOK	71	54
BD400-04 (BD-22)		CONTRACT NO. 62T58		
ILLINOIS FED. AID PROJECT				



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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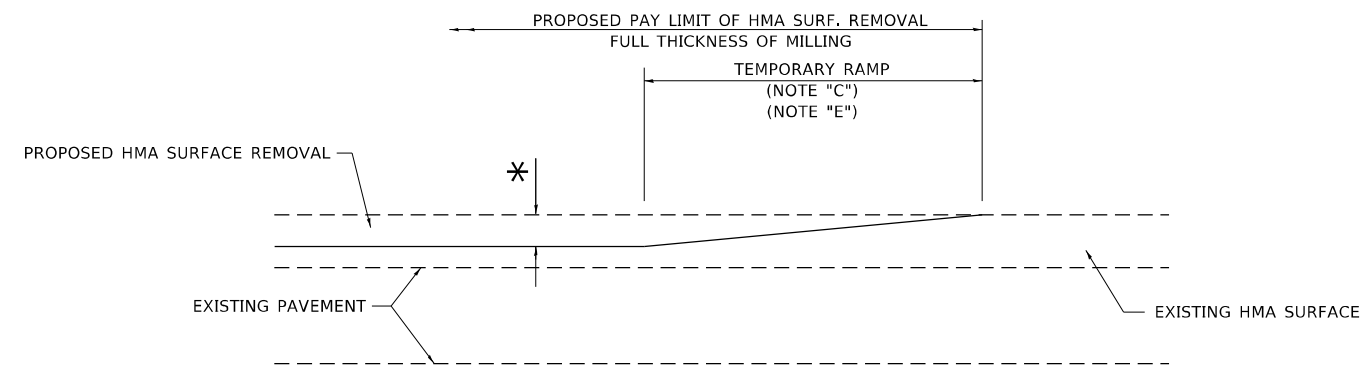
USER NAME = Sher,Khan	DESIGNED - A. HOUSEH	REVISED - A. ABBAS 03-21-97
	DRAWN -	REVISED - M. GOMEZ 01-22-01
PLOT SCALE = 100,0000 ' / in.	CHECKED -	REVISED - R. BORO 12-15-09
PLOT DATE = 10/20/2023	DATE - 03-11-94	REVISED - K. SMITH 07-11-19

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CURB OR CURB AND GUTTER
REMOVAL AND REPLACEMENT**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

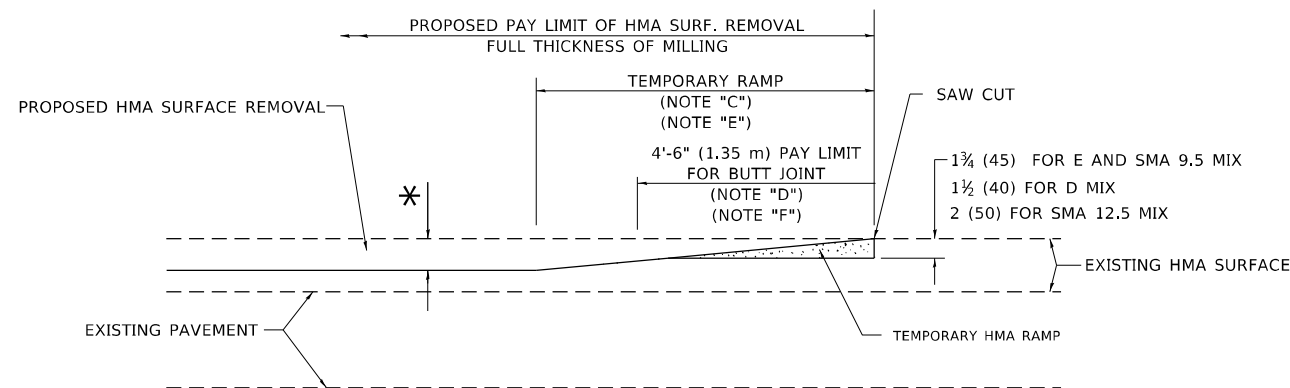
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0358	FAP 0358 22 RS	COOK	71	55
BD600-06 (BD-24)		CONTRACT NO. 62T58		
ILLINOIS FED. AID PROJECT				



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

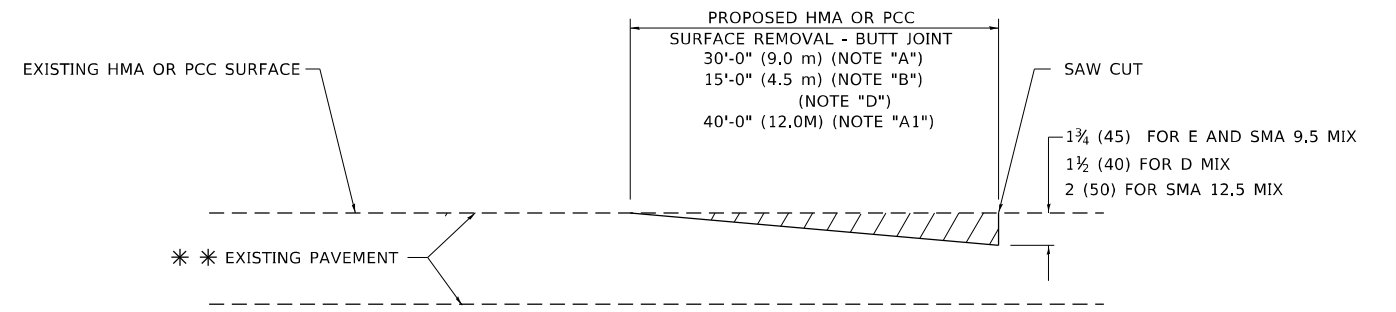


HMA CONSTRUCTED TEMPORARY RAMP

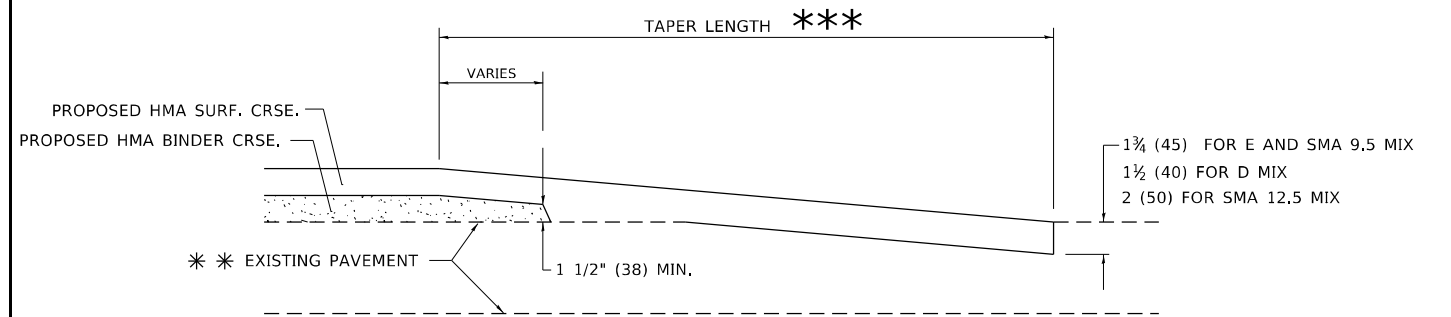
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



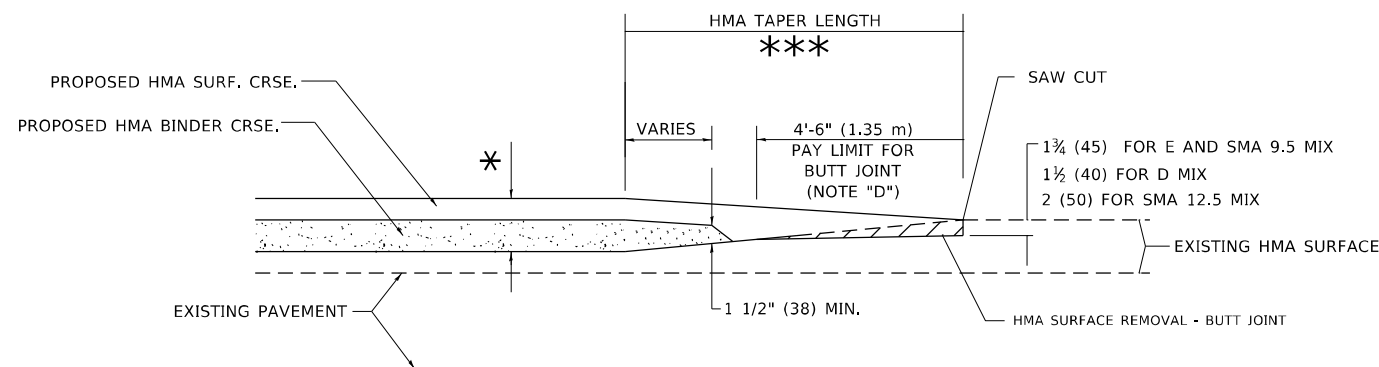
BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

GENERAL NOTES

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

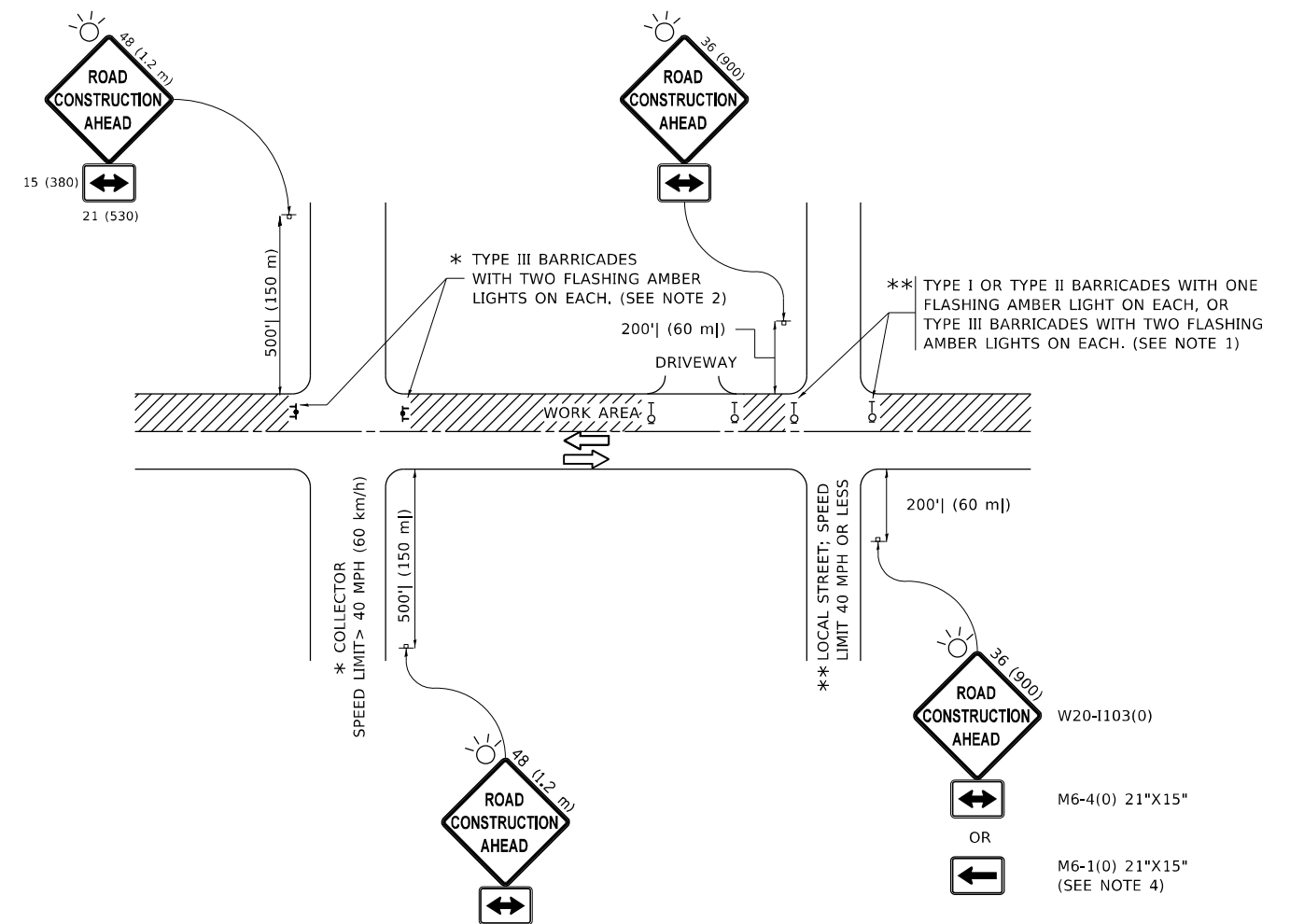
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DATE: 10/20/2023

USER NAME = Sher,Khan	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97
PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED - M. GOMEZ 04-06-01
PLOT DATE = 10/20/2023	CHECKED -	REVISED - R. BORO 01-01-07
	DATE - 06-13-90	REVISED - K. SMITH 11-18-22

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT AND HMA TAPER DETAILS			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0358	FAP 0358 22 RS	COOK	71	56
BD400-05 BD-32		CONTRACT NO. 62T58		
ILLINOIS FED. AID PROJECT				



NOTES:

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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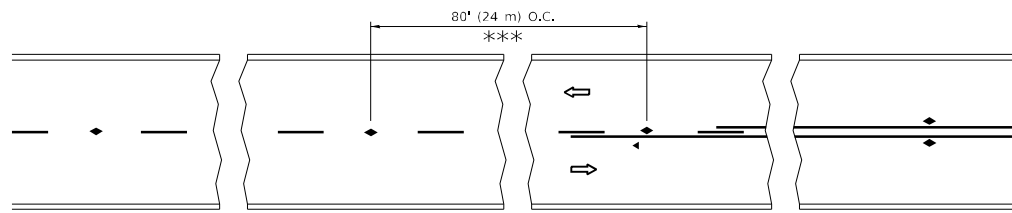
USER NAME = Sher,Khan	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
PLOT SCALE = 100,0000 ' / in.	CHECKED -	REVISED - T. RAMMACHER 01-06-00
PLOT DATE = 10/20/2023	DATE - 06-89	REVISED - A. SCHUETZE 07-01-13
		REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

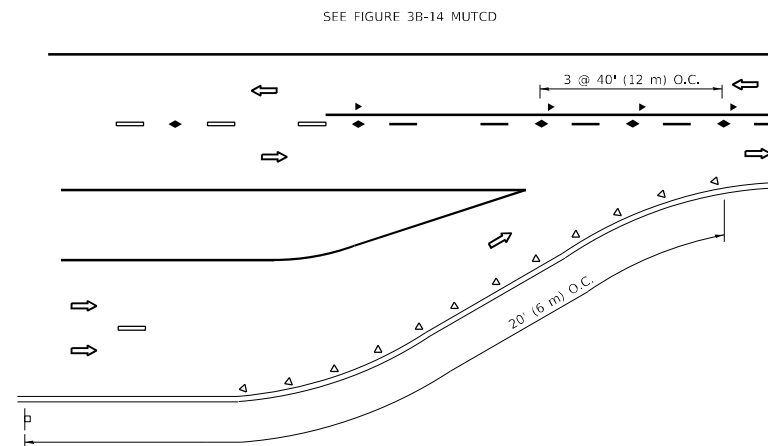
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0358	FAP 0358 22 RS	COOK	71	57
TC-10			CONTRACT NO. 62T58	
ILLINOIS FED. AID PROJECT				

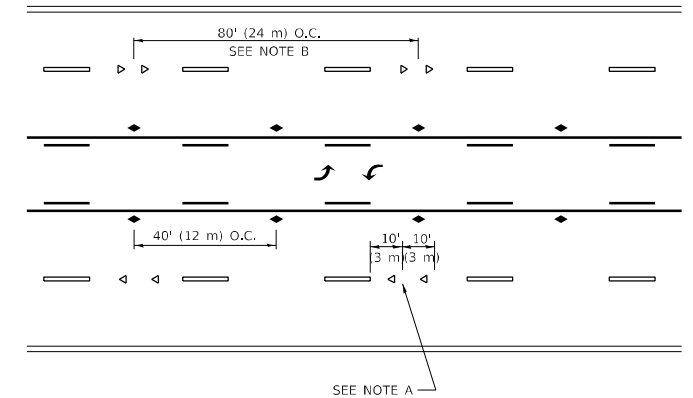


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

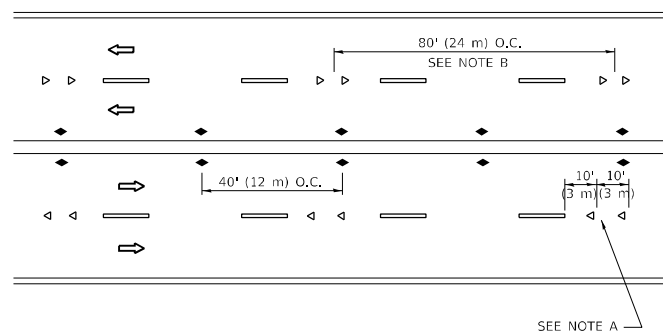
TWO-LANE/TWO-WAY



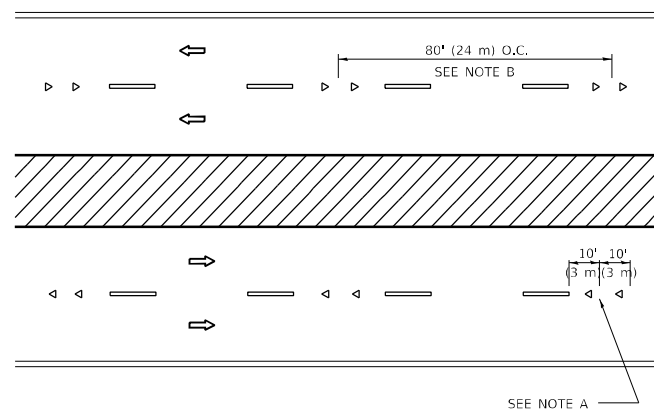
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

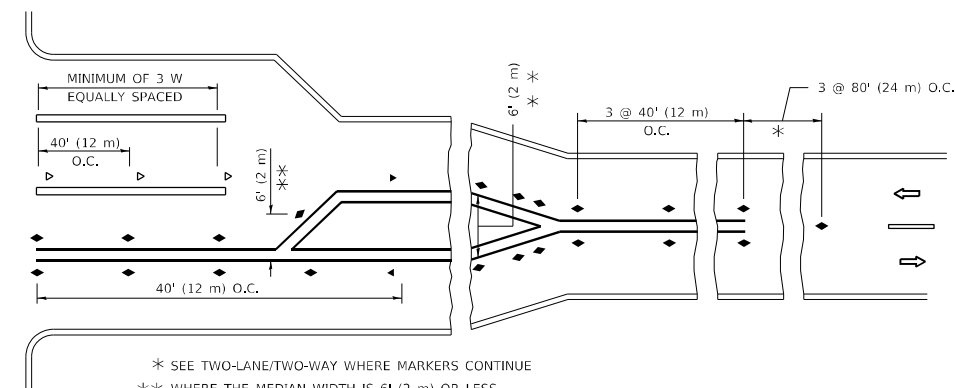
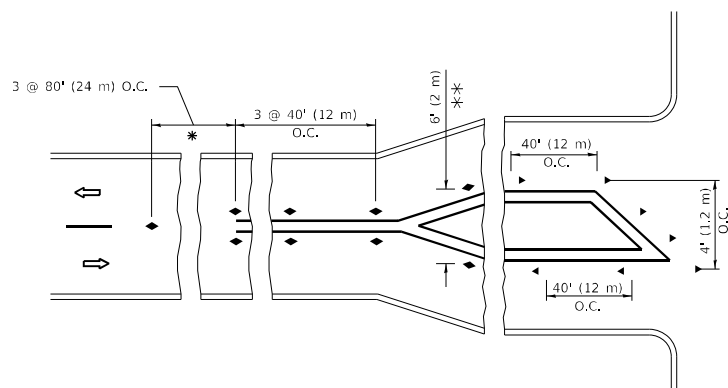
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



TURN LANES

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

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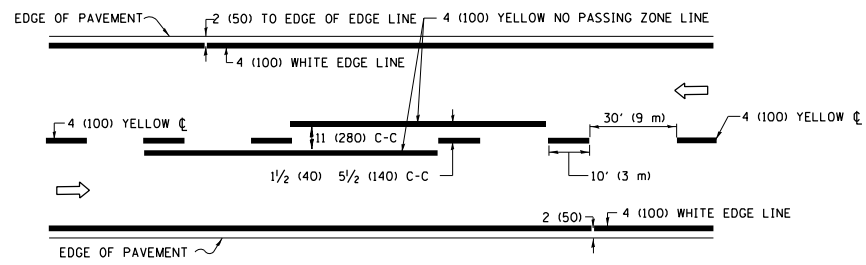
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	DRAWN -	REVISED - T. RAMMACHER 01-06-00
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PLOT DATE = 10/20/2023	DATE -	REVISED - C. JUCIUS 07-01-13

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

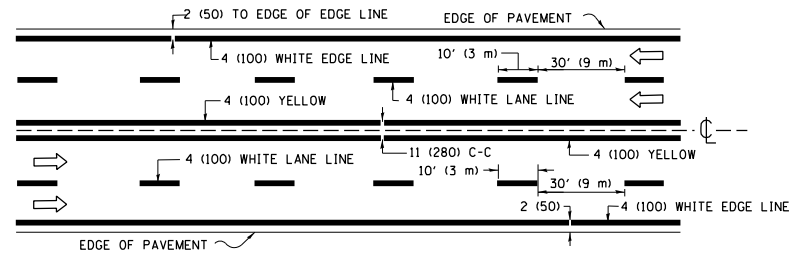
**TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

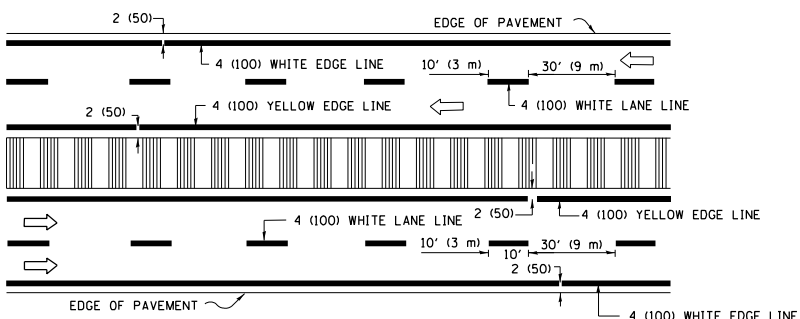
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0358	FAP 0358 22 RS	COOK	71	58
TC-11			CONTRACT NO. 62T58	
ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

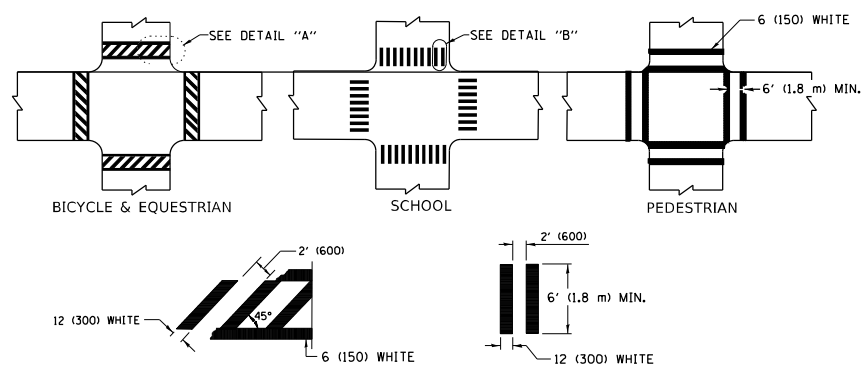


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

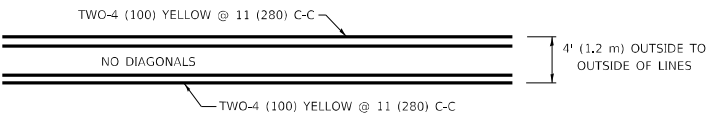


DETAIL "A"

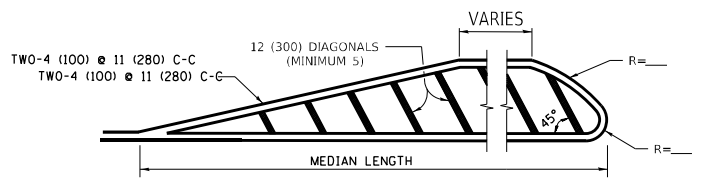
DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



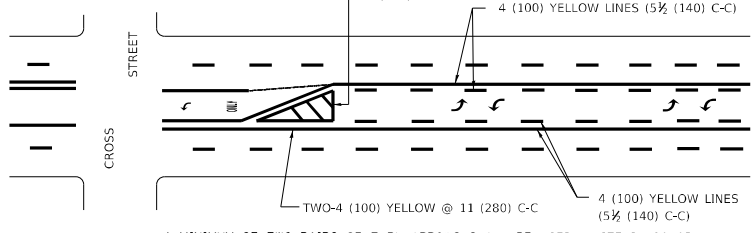
4' (1.2 m) WIDE MEDIANS ONLY



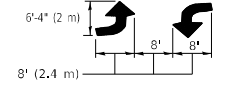
MEDIANS OVER 4' (1.2 m) WIDE

FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

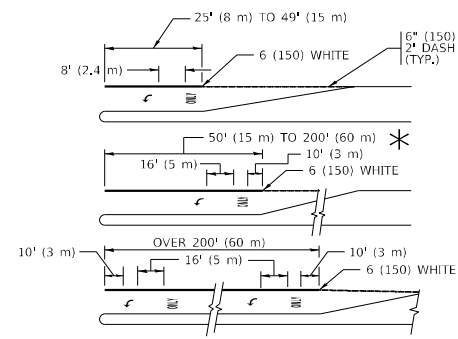


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

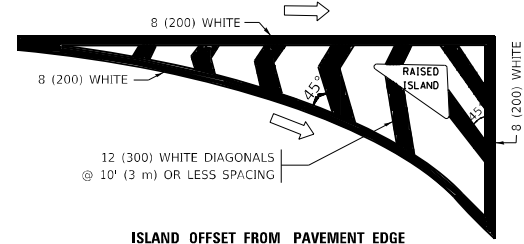


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

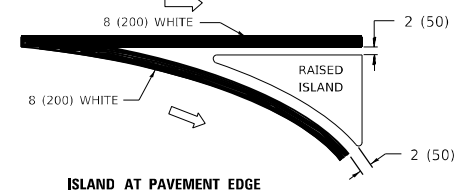
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

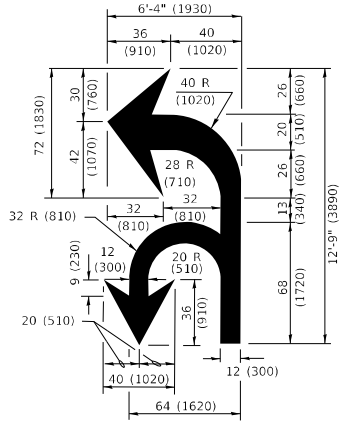


ISLAND OFFSET FROM PAVEMENT EDGE

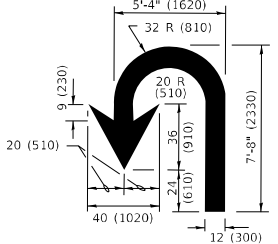


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" IS 8' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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USER NAME	DESIGNED	REVISION
= Sher,Khan	- EVERS	- C. JUCIUS 09-09-09
	-	- C. JUCIUS 07-01-13
	-	- C. JUCIUS 12-21-15
	-	- C. JUCIUS 04-12-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE TYPICAL PAVEMENT MARKINGS	
SCALE: NONE	SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0358	FAP 0358 22 RS	COOK	71	59
TC-13		CONTRACT NO. 62T58		
		ILLINOIS FED. AID PROJECT		

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

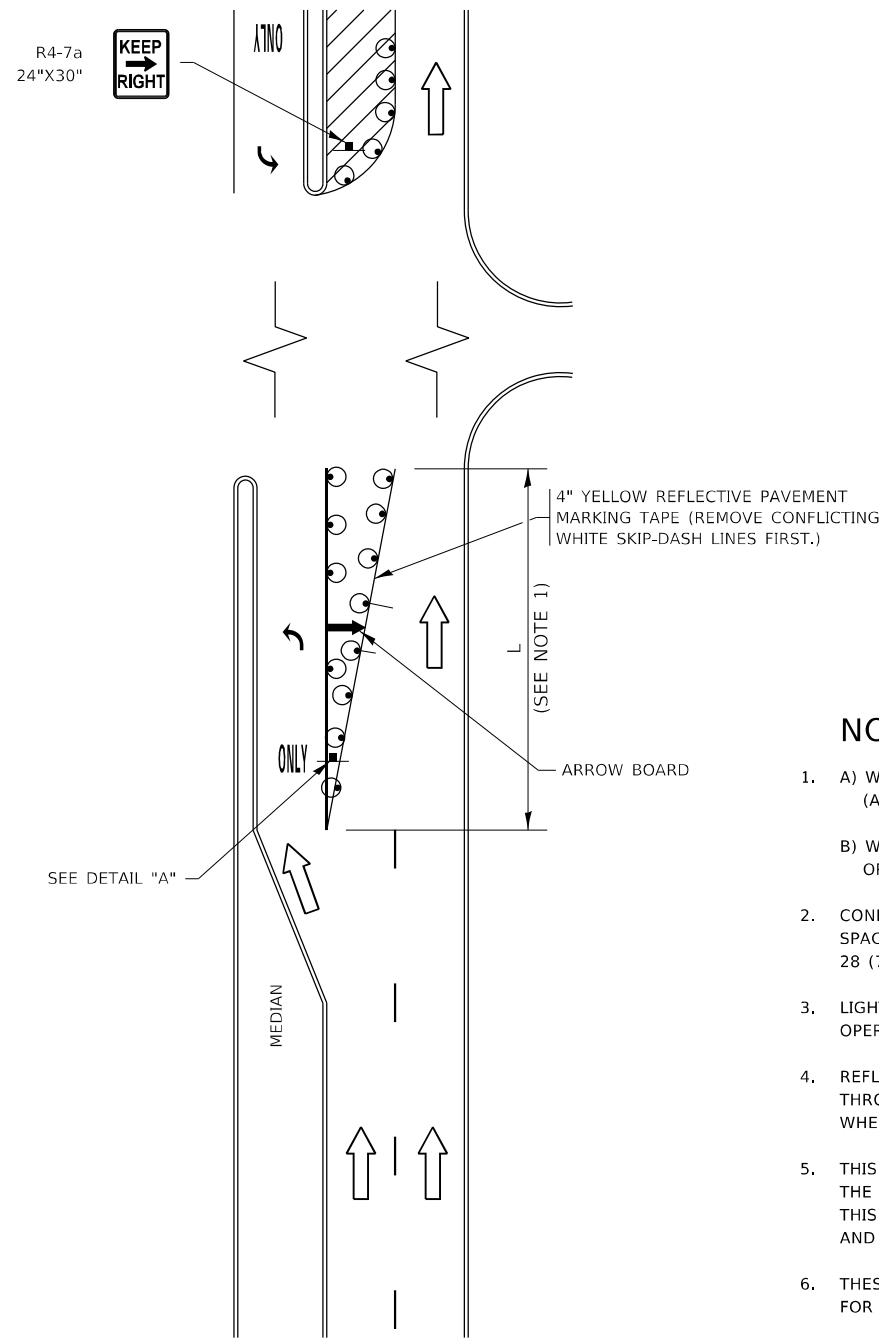


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

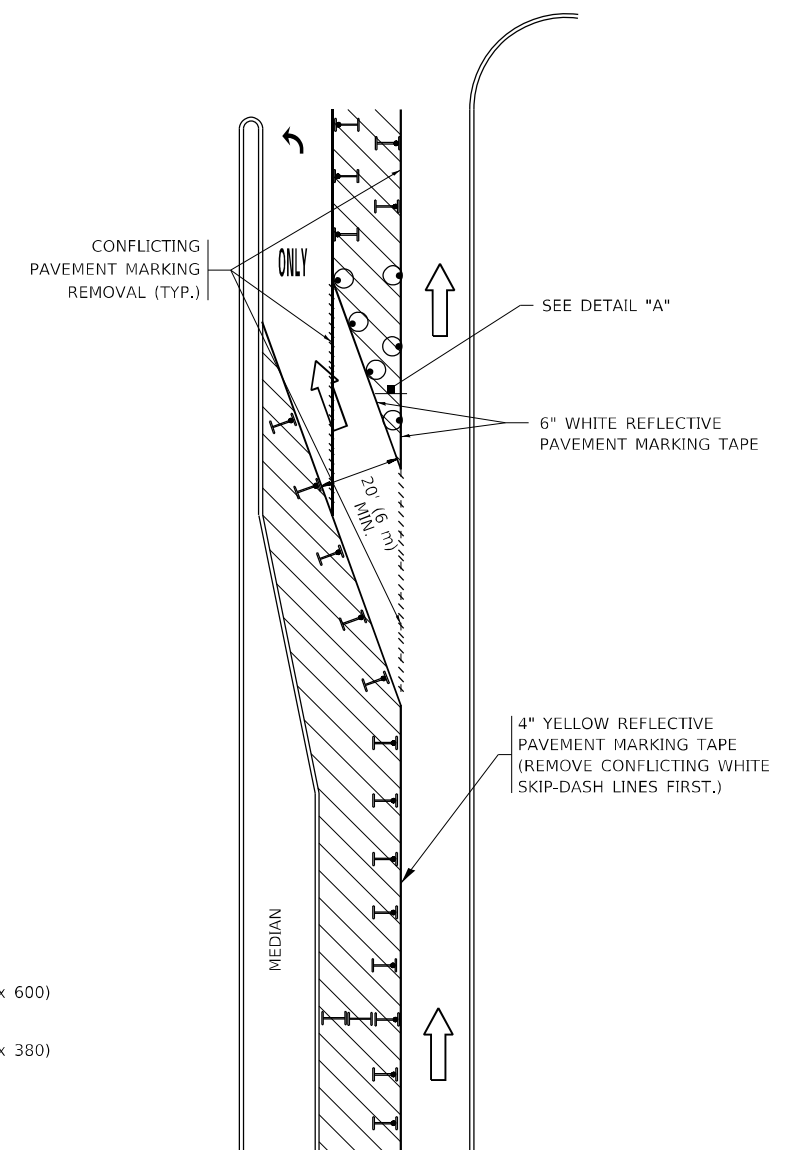


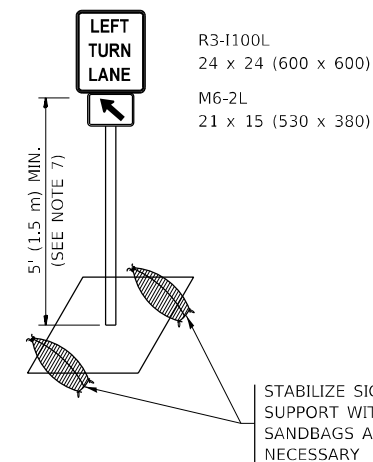
FIGURE 2

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

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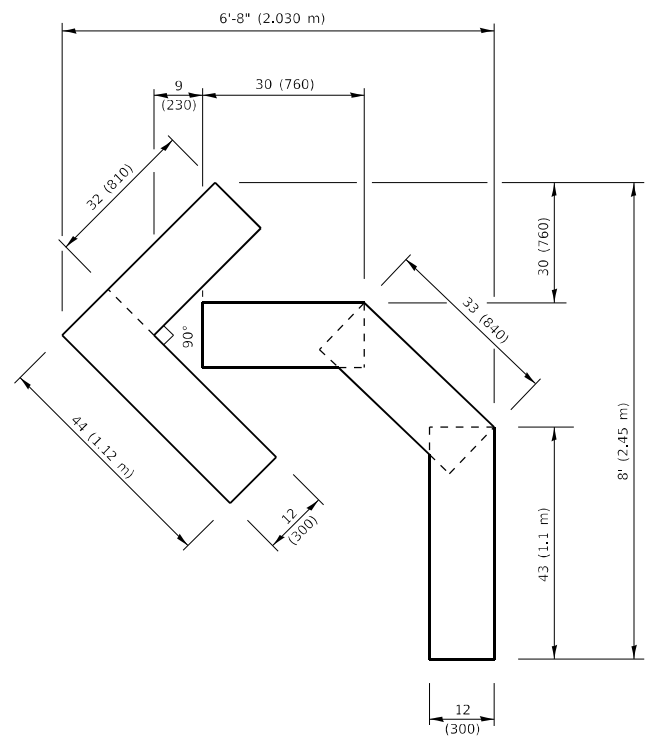
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	DRAWN - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13
PLOT SCALE = 100,0000' / in.	CHECKED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16
PLOT DATE = 10/20/2023	DATE - T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

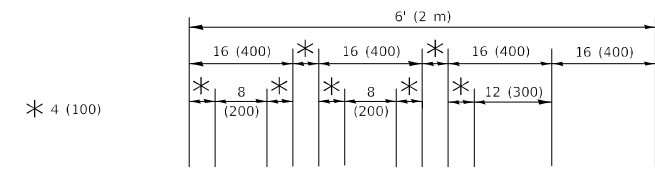
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-14		CONTRACT NO. 62T58		
ILLINOIS FED. AID PROJECT				



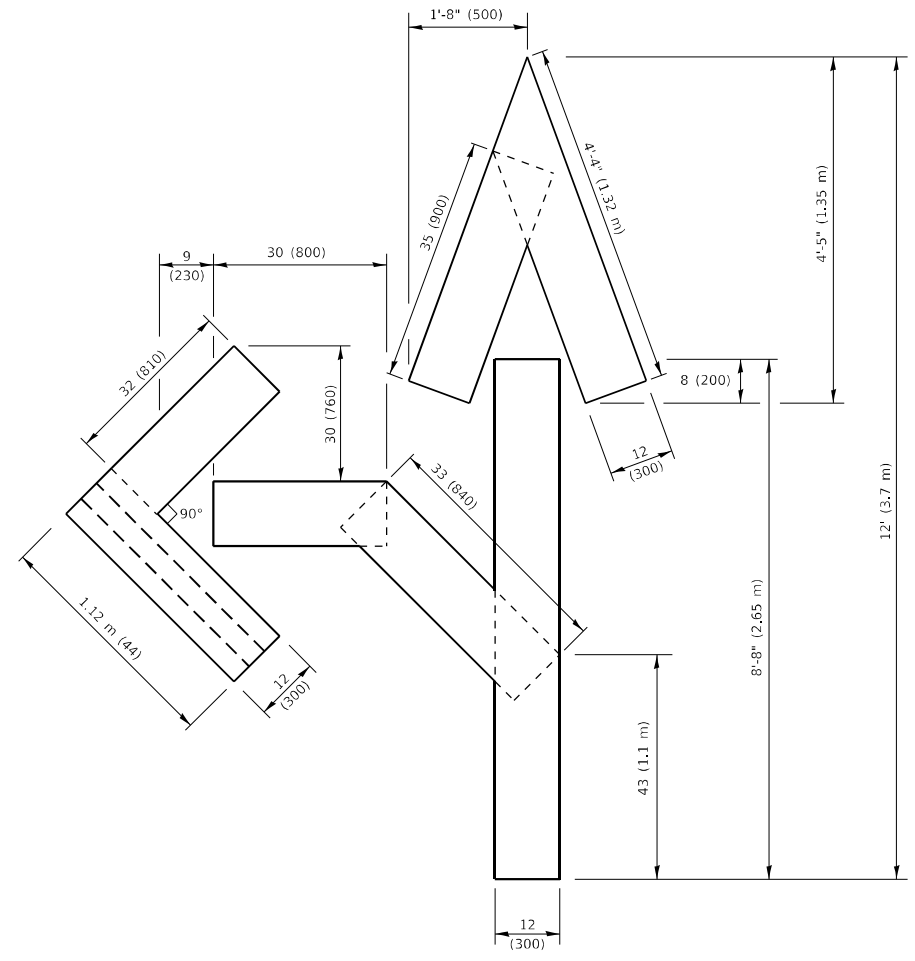
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

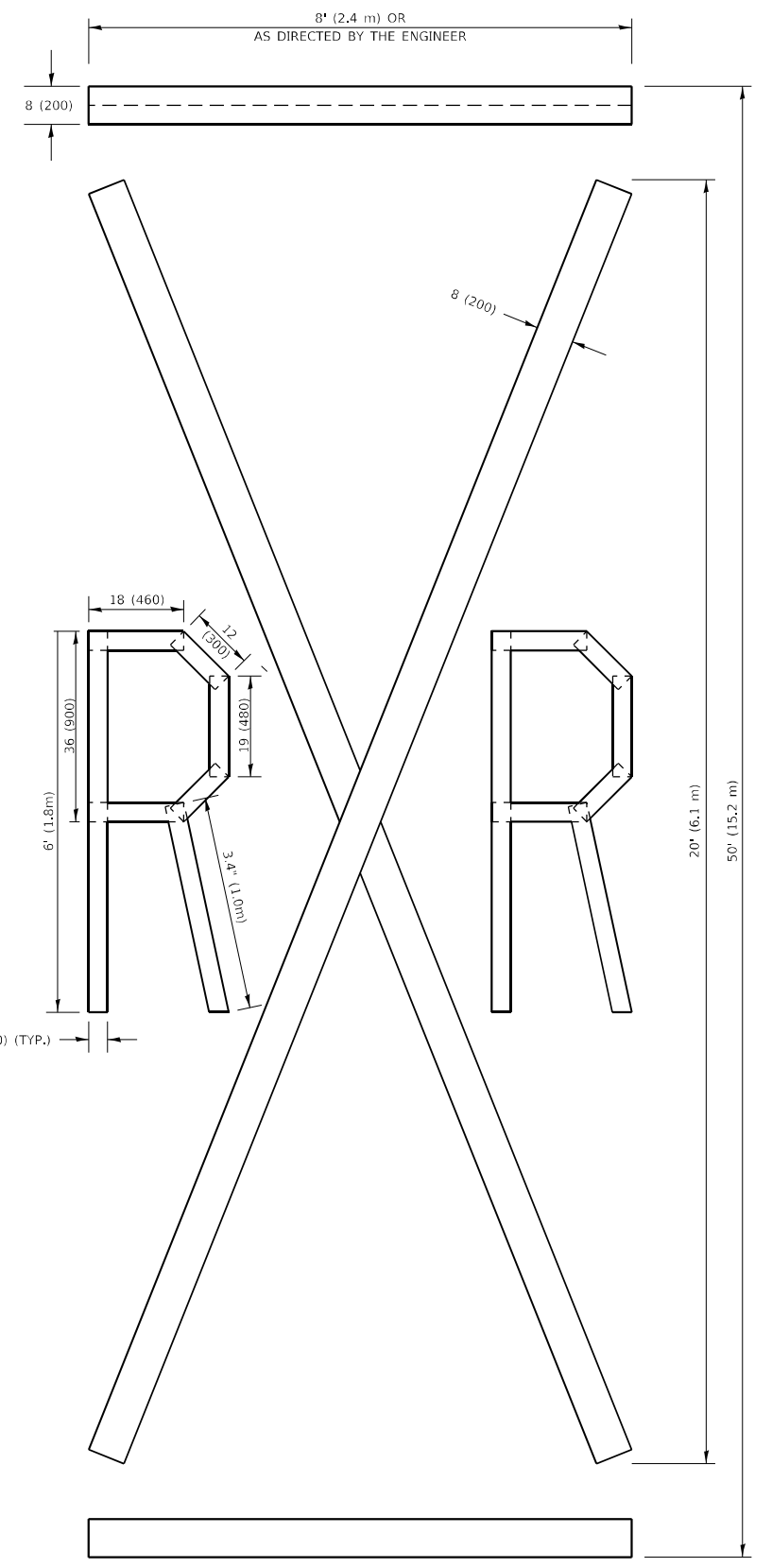


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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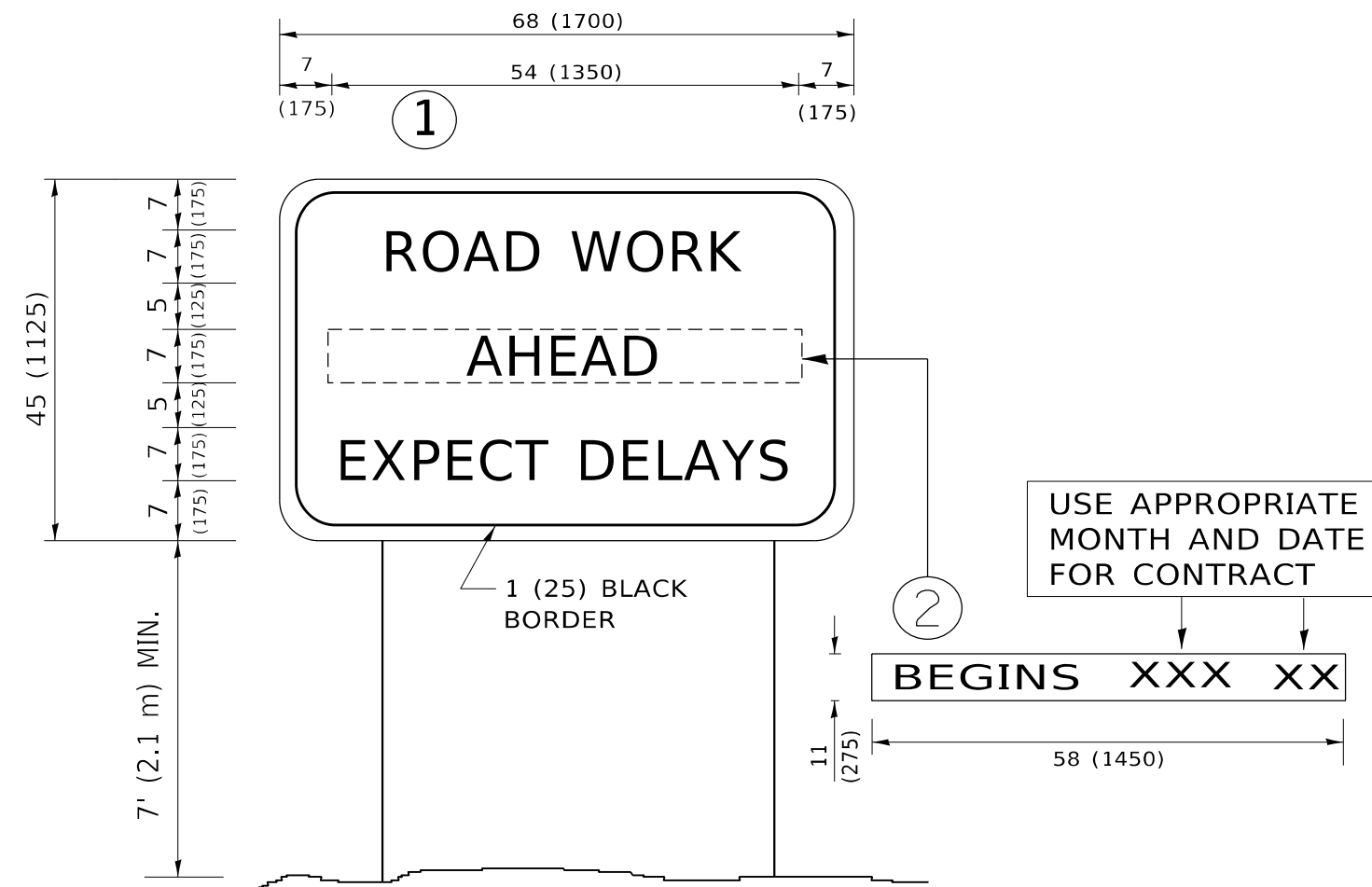
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PLOT DATE = 10/20/2023	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE. 0358	SECTION FAP 0358 22 RS	COUNTY COOK	TOTAL SHEETS 71	SHEET NO. 61
TC-16		CONTRACT NO. 62T58		
ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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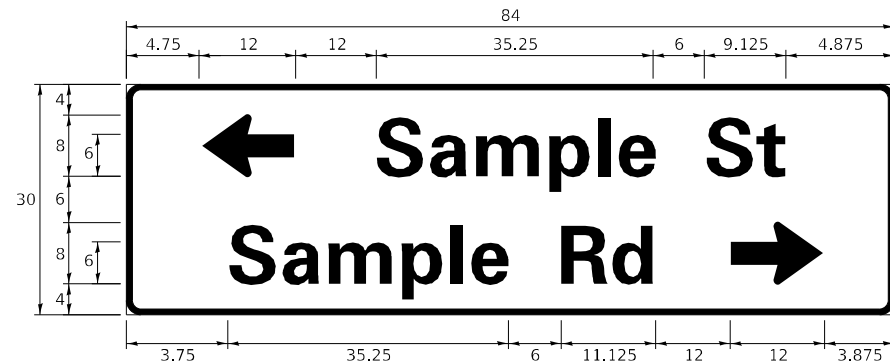
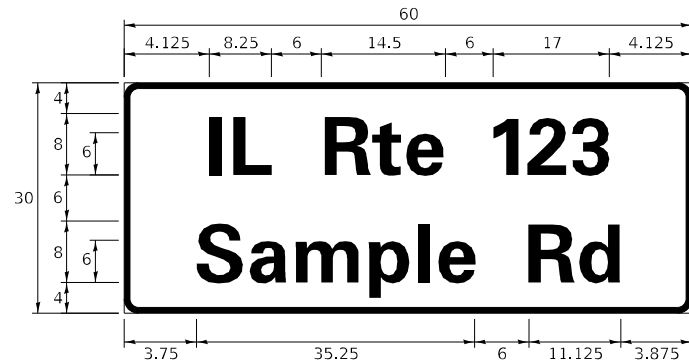
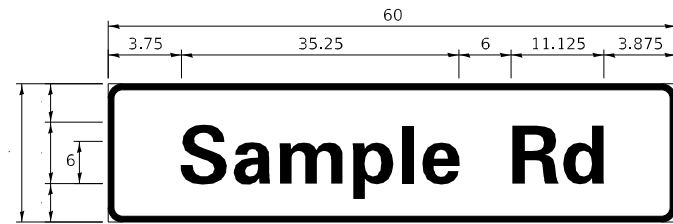
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0358	FAP 0358 22 RS	COOK	71	62
TC-22		CONTRACT NO. 62T58		
ILLINOIS FED. AID PROJECT				

SIGN PANEL – TYPE 1 OR TYPE 2



DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY. REQUIRED
D OR C	-	1 OR 2	ZZ	-

COMMON STREET NAME ABBREVIATIONS AND WIDTHS

NAME	ABBREVIATION	WIDTH (INCH)	
		SERIES "C"	SERIES "D"
AVENUE	Ave	15.000	18.250
BOULEVARD	Blvd	17.125	20.000
CIRCLE	Cir	11.125	13.000
COURT	Ct	8.250	9.625
DRIVE	Dr	8.625	10.125
HIGHWAY	Hwy	18.375	22.000
ILLINOIS	IL	7.000	8.250
LANE	Ln	9.125	10.750
PARKWAY	Pkwy	23.375	27.375
PLACE	Pl	7.125	7.750
ROAD	Rd	9.625	11.125
ROUTE	Rte	12.625	14.500
STREET	St	8.000	9.125
TERRACE	Ter	12.625	14.625
TRAIL	Tr	7.750	9.125
UNITED STATES	US	10.375	12.250

GENERAL NOTES

- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 877001, 877002, 877006, 877011 AND 877012, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 8'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
- ALL SIGNS SHALL CONSIST OF A WHITE LEGEND AND BORDER (TYPE ZZ SHEETING) ON A GREEN BACKGROUND (TYPE ZZ SHEETING)
- THE SIGN LENGTH SHALL BE IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHALL NOT EXCEED 8'-0". ALL BORDERS IF POSSIBLE, BUT MAY BE REDUCED TO 5" WHEN SPACING IS CRITICAL. A MINIMUM OF 2-1/2" SHALL BE INCLUDED BETWEEN THE WORD AND THE RIGHT AND LEFT EDGES OF THE SIGN.
- A PREFERRED METHOD FOR THE SIGN DESIGN IS TO USE SERIES "D" LETTER ON A ONE-LINE SIGN 18" IN HEIGHT AND A MAXIMUM OF 8'-0" IN WIDTH. IF SERIES "D" DOES NOT FIT ON A 8'-0" SIGN, THEN SERIES "C" SHOULD BE TRIED. IF SERIES "C" DOES NOT FIT ON A 8'-0" SIGN, A 30" HIGH TWO-LINE SIGN CAN BE USED. THE CROSSROAD DESIGNATION AS TO STREET, AVENUE, ETC. SHOULD BE SPELLED OUT ON THE SECOND LINE, IF THE ABBREVIATION CANNOT FIT ON THE FIRST LINE.
- LED ILLUMINATED STREET NAME SIGNS CAN BE USED IN PLACE OF REGULAR SIGN PANELS BUT ANY SPECIAL WORDING AND SYMBOLOGY MUST BE APPROVED BY THE DEPARTMENT. GENERAL DESIGN REQUIREMENT AS LISTED ABOVE (COLOR, FONT, SIZE, ETC.) MUST BE FOLLOWED.
- SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS.

LOCAL SUPPLIERS:

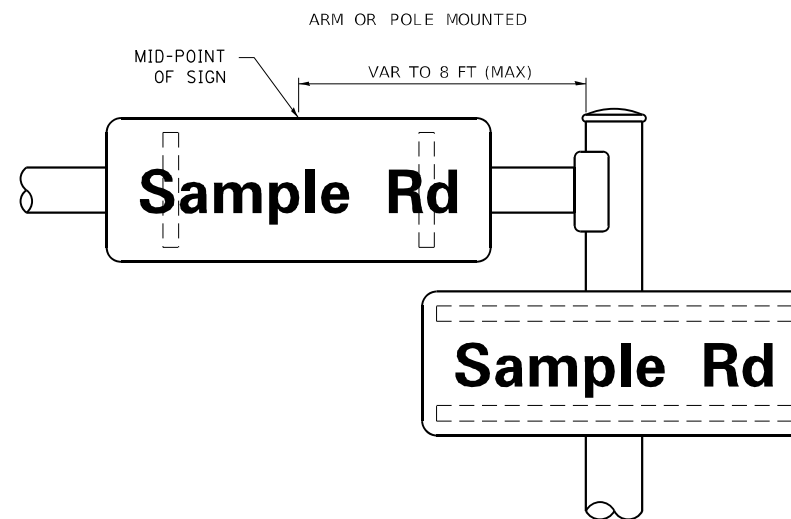
- J.O. HERBERT COMPANY, INC
MIDLOTHIAN, VA
- WESTERN REMAC, INC.
WOODRIDGE, IL

PARTS LISTING:

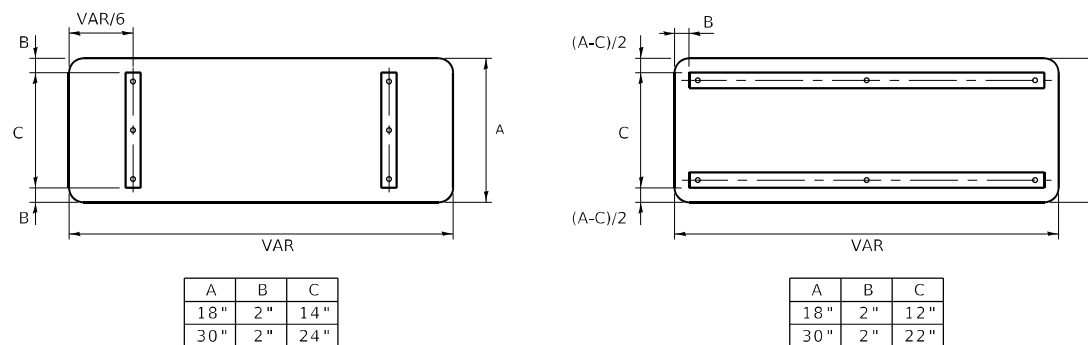
- SIGN CHANNEL PART #HPN053 (MED. CHANNEL)
- SIGN SCREWS 1/4" x 14 x 1" H.W.H. #3
- BRACKETS SELF TAPPING WITH NEOPRENE WASHER PART #HPN034 (UNIVERSAL)
- CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING

OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BACKET OF THE ABOVE PRODUCT.

MOUNTING LOCATION



SUPPORTING CHANNELS



STANDARD ALPHABETS SPACING CHART

(8") UPPER CASE AND (6") LOWER CASE

FHWA SERIES "C"				FHWA SERIES "D"			
CHARACTER	LEFT SPACING (INCH)	WIDTH (INCH)	RIGHT SPACING (INCH)	CHARACTER	LEFT SPACING (INCH)	WIDTH (INCH)	RIGHT SPACING (INCH)
A	0.240	5.122	0.240	A	0.240	6.804	0.240
B	0.880	4.482	0.480	B	0.960	5.446	0.400
C	0.720	4.482	0.720	C	0.800	5.446	0.800
D	0.880	4.482	0.720	D	0.960	5.446	0.800
E	0.880	4.082	0.480	E	0.960	4.962	0.400
F	0.880	4.082	0.240	F	0.960	4.962	0.240
G	0.720	4.482	0.720	G	0.800	5.446	0.800
H	0.880	4.482	0.880	H	0.960	5.446	0.960
I	0.880	1.120	0.880	I	0.960	1.280	0.960
J	0.240	4.082	0.880	J	0.240	5.122	0.960
K	0.880	4.482	0.480	K	0.960	5.604	0.400
L	0.880	4.082	0.240	L	0.960	4.962	0.240
M	0.880	5.284	0.880	M	0.960	6.244	0.960
N	0.880	4.482	0.880	N	0.960	5.446	0.960
O	0.720	4.722	0.720	O	0.800	5.684	0.800
P	0.880	4.482	0.720	P	0.960	5.446	0.240
Q	0.720	4.722	0.720	Q	0.800	5.684	0.800
R	0.880	4.482	0.480	R	0.960	5.446	0.400
S	0.480	4.482	0.480	S	0.400	5.446	0.400
T	0.240	4.082	0.240	T	0.240	4.962	0.240
U	0.880	4.482	0.880	U	0.960	5.446	0.960
V	0.240	4.962	0.240	V	0.240	6.084	0.240
W	0.240	6.084	0.240	W	0.240	7.124	0.240
X	0.240	4.722	0.240	X	0.400	5.446	0.400
Y	0.240	5.122	0.240	Y	0.240	6.884	0.240
Z	0.480	4.482	0.480	Z	0.400	5.446	0.400
a	0.320	3.842	0.640	a	0.400	4.562	0.720
b	0.720	4.082	0.480	b	0.800	4.802	0.480
c	0.480	4.002	0.240	c	0.480	4.722	0.240
d	0.480	4.082	0.720	d	0.480	4.802	0.800
e	0.480	4.082	0.320	e	0.480	4.722	0.320
f	0.320	2.480	0.160	f	0.320	2.882	0.160
g	0.480	4.082	0.720	g	0.480	4.802	0.800
h	0.720	4.082	0.640	h	0.800	4.722	0.720
i	0.720	1.120	0.720	i	0.800	1.280	0.800
j	0.000	2.320	0.720	j	0.000	2.642	0.800
k	0.720	4.322	0.160	k	0.800	5.122	0.160
l	0.720	1.120	0.720	l	0.800	1.280	0.800
m	0.720	6.724	0.640	m	0.800	7.926	0.720
n	0.720	4.082	0.640	n	0.800	4.722	0.720
o	0.480	4.082	0.480	o	0.480	4.882	0.480
p	0.720	4.082	0.480	p	0.800	4.802	0.480
q	0.480	4.082	0.720	q	0.480	4.802	0.800
r	0.720	2.642	0.160	r	0.800	3.042	0.160
s	0.320	3.362	0.240	s	0.320	3.762	0.240
t	0.080	2.882	0.080	t	0.080	3.202	0.080
u	0.640	4.082	0.720	u	0.720	4.722	0.800
v	0.160	4.722	0.160	v	0.160	5.684	0.160
w	0.160	7.524	0.160	w	0.160	9.046	0.160
x	0.000	5.202	0.000	x	0.000	6.244	0.000
y	0.160	4.962	0.160	y	0.160	6.004	0.160
z	0.240	3.362	0.240	z	0.240	4.002	0.240
1	0.720	1.680	0.880	1	0.800	2.000	0.960
2	0.480	4.482	0.480	2	0.800	5.446	0.800
3	0.480	4.482	0.480	3	1.440	5.446	0.800
4	0.240	4.962	0.720	4	0.160	6.004	0.960
5	0.480	4.482	0.480	5	0.800	5.446	0.800
6	0.720	4.482	0.720	6	0.800	5.446	0.800
7	0.240	4.482	0.720	7	0.560	5.446	0.560
8	0.480	4.482	0.480	8	0.800	5.446	0.800
9	0.480	4.482	0.480	9	0.800	5.446	0.800
0	0.720	4.722	0.720	0	0.800	5.684	0.800
-	0.240	2.802	0.240	-	0.240	2.802	0.240

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
MAST ARM MOUNTED STREET NAME SIGNS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 0358	SECTION FAP 0358 22 RS	COUNTY COOK	TOTAL SHEETS 71	SHEET NO. 63
TS-02		CONTRACT NO. 62T58		
ILLINOIS FED. AID PROJECT				

TRAFFIC SIGNAL LEGEND

(NOT TO SCALE)

ITEM	EXISTING	PROPOSED	ITEM	EXISTING	PROPOSED	ITEM	EXISTING	PROPOSED
CONTROLLER CABINET			HANDHOLE -SQUARE -ROUND			SIGNAL HEAD -(P) PROGRAMMABLE SIGNAL HEAD		
COMMUNICATION CABINET			HEAVY DUTY HANDHOLE -SQUARE -ROUND			SIGNAL HEAD WITH BACKPLATE -(P) PROGRAMMABLE SIGNAL HEAD -(RB) RETROREFLECTIVE BACKPLATE		
MASTER CONTROLLER			DOUBLE HANDHOLE			PEDESTRIAN SIGNAL HEAD AT RAILROAD INTERSECTIONS		
MASTER MASTER CONTROLLER			JUNCTION BOX			PEDESTRIAN SIGNAL HEAD WITH COUNTDOWN TIMER		
UNINTERRUPTABLE POWER SUPPLY			RAILROAD CANTILEVER MAST ARM			ILLUMINATED SIGN "NO LEFT TURN"/"NO RIGHT TURN"		
SERVICE INSTALLATION -(P) POLE MOUNTED			RAILROAD FLASHING SIGNAL			NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE. ALL DETECTOR LOOP CABLE TO BE SHIELDED		
SERVICE INSTALLATION -(G) GROUND MOUNTED -(GM) GROUND MOUNTED METERED			RAILROAD CROSSING GATE			GROUND CABLE IN CONDUIT, NO. 6 SOLID COPPER (GREEN)		
TELEPHONE CONNECTION			RAILROAD CROSSBUCK			ELECTRIC CABLE IN CONDUIT, TRACER NO. 14 1/C		
STEEL MAST ARM ASSEMBLY AND POLE			RAILROAD CONTROLLER CABINET			COAXIAL CABLE		
ALUMINUM MAST ARM ASSEMBLY AND POLE			UNDERGROUND CONDUIT (UC), GALVANIZED STEEL			VENDOR CABLE		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE			TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE			COPPER INTERCONNECT CABLE, NO. 18, 3 PAIR TWISTED, SHIELDED		
SIGNAL POST -(BM) BARREL MOUNTED - TEMPORARY			SYSTEM ITEM			FIBER OPTIC CABLE -NO. 62.5/125, MM12F -NO. 62.5/125, MM12F SM12F -NO. 62.5/125, MM12F SM24F		
WOOD POLE			INTERSECTION ITEM			GROUND ROD -(C) CONTROLLER -(M) MAST ARM -(P) POST -(S) SERVICE		
GUY WIRE			REMOVE ITEM					
SIGNAL HEAD			RELOCATE ITEM					
SIGNAL HEAD WITH BACKPLATE			ABANDON ITEM					
SIGNAL HEAD OPTICALLY PROGRAMMED			CONTROLLER CABINET AND FOUNDATION TO BE REMOVED					
FLASHER INSTALLATION -(FS) SOLAR POWERED			MAST ARM POLE AND FOUNDATION TO BE REMOVED					
PEDESTRIAN SIGNAL HEAD			SIGNAL POST AND FOUNDATION TO BE REMOVED					
PEDESTRIAN PUSH BUTTON -(APS) ACCESSIBLE PEDESTRIAN PUSH BUTTON			DETECTOR LOOP, TYPE I					
RADAR DETECTION SENSOR			PREFORMED DETECTOR LOOP					
VIDEO DETECTION CAMERA			SAMPLING (SYSTEM) DETECTOR					
RADAR/VIDEO DETECTION ZONE			INTERSECTION AND SAMPLING (SYSTEM) DETECTOR					
PAN, TILT, ZOOM (PTZ) CAMERA			QUEUE AND SAMPLING (SYSTEM) DETECTOR					
EMERGENCY VEHICLE LIGHT DETECTOR			WIRELESS DETECTOR SENSOR					
CONFIRMATION BEACON			WIRELESS ACCESS POINT					
WIRELESS INTERCONNECT								
WIRELESS INTERCONNECT RADIO REPEATER								

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

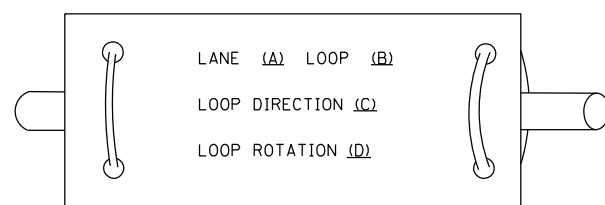
SCALE: NONE SHEET 1 OF 7 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0358	FAP 0358 22 RS	COOK	71	64
TS-05		CONTRACT NO. 62T58		
ILLINOIS FED. AID PROJECT				

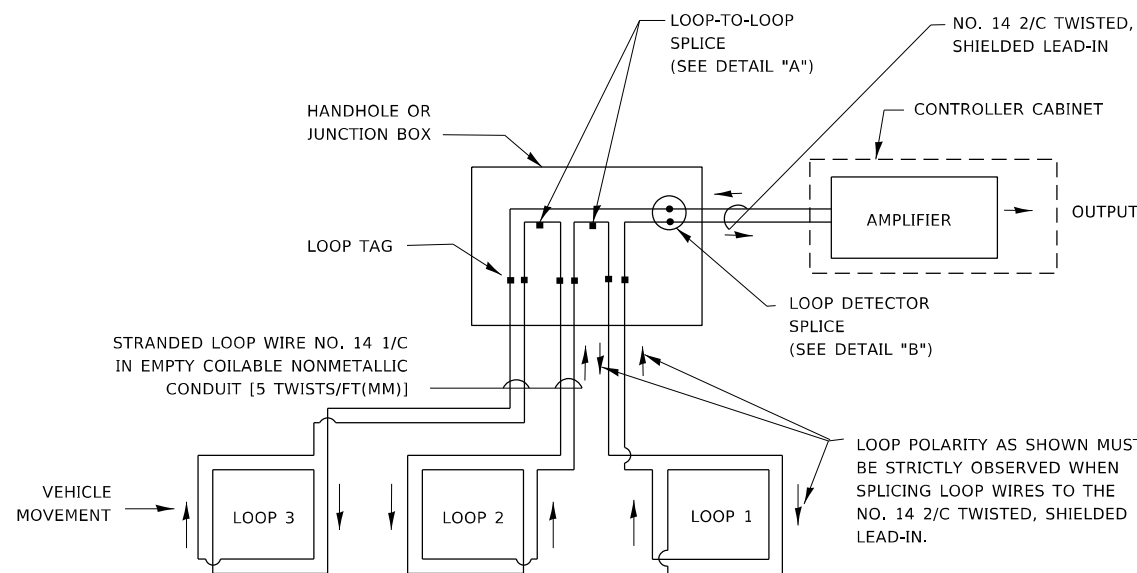
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

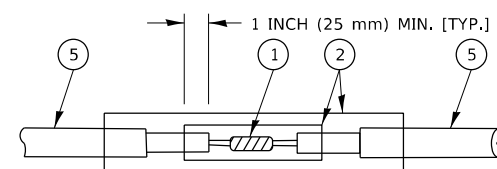


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

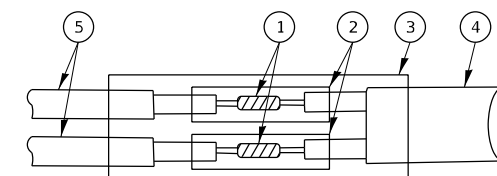


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES. SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm), IF IN CONCRETE.
- THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

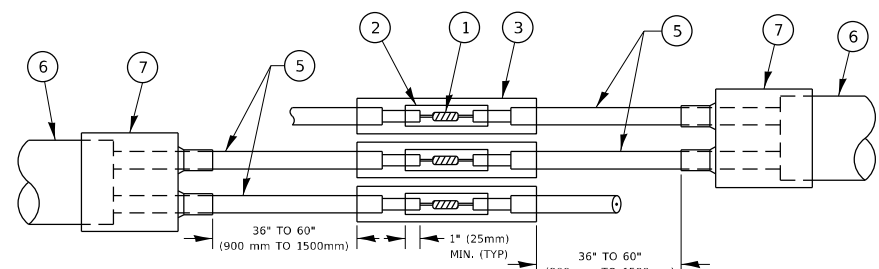


DETAIL "A"
LOOP-TO-LOOP SPLICE

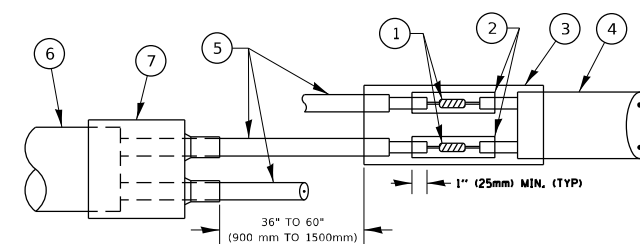


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

PREFORMED LOOP

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE. PREFORMED LOOP
- 6 XL POLYOLEFIN 2 CONDUCTOR
- 7 BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

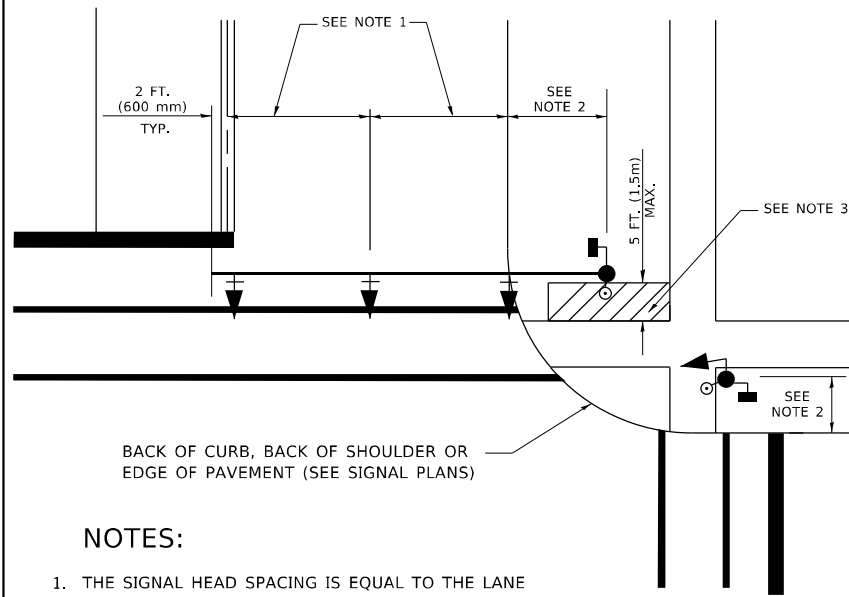
**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET 2 OF 7 SHEETS STA. TO STA.

F.A.P. RTE. 0358	SECTION FAP 0358 22 RS	COUNTY COOK	TOTAL SHEETS 71	SHEET NO. 65
TS-05		CONTRACT NO. 62T58		
ILLINOIS		FED. AID PROJECT		

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.

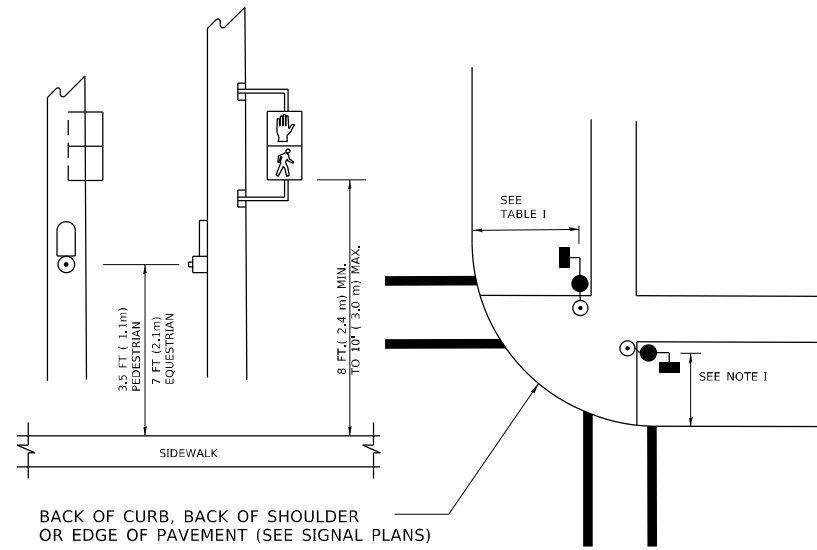


BACK OF CURB, BACK OF SHOULDER OR EDGE OF PAVEMENT (SEE SIGNAL PLANS)

NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST

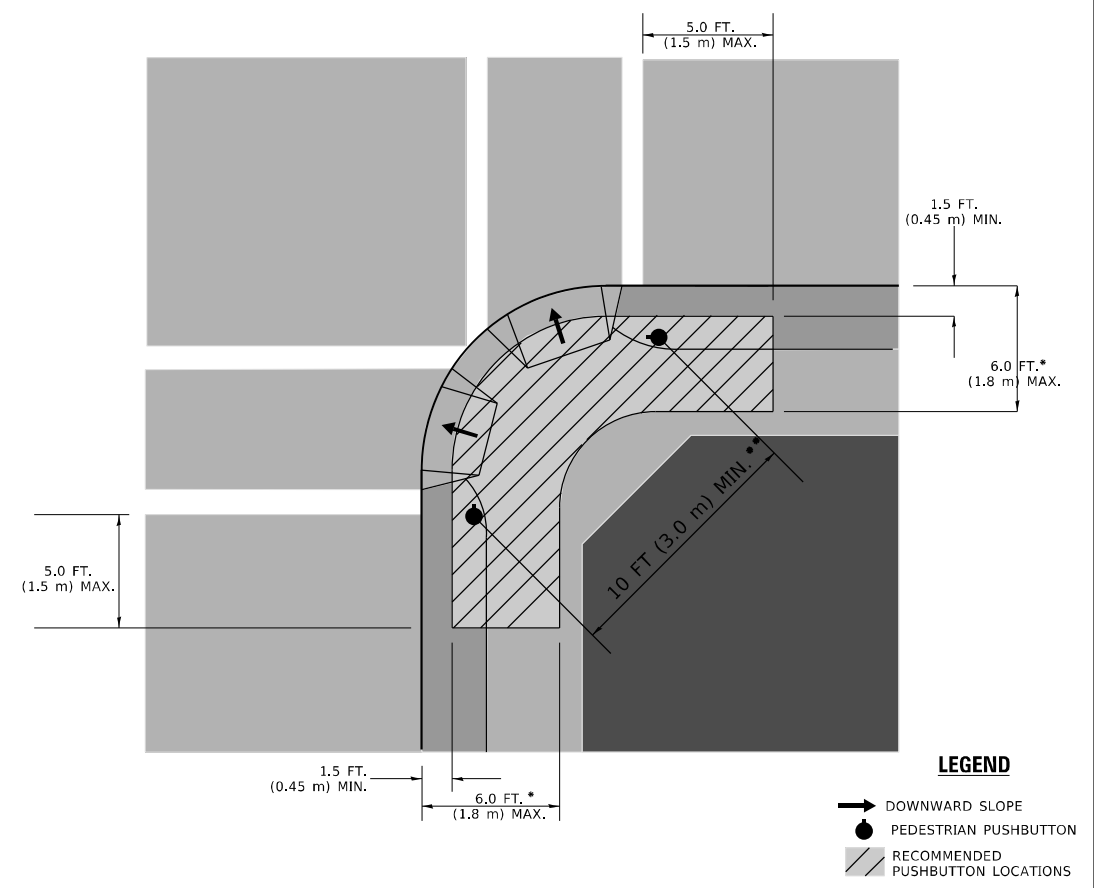


BACK OF CURB, BACK OF SHOULDER OR EDGE OF PAVEMENT (SEE SIGNAL PLANS)

NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



LEGEND

- DOWNWARD SLOPE
- PEDESTRIAN PUSHBUTTON
- ▨ RECOMMENDED PUSHBUTTON LOCATIONS

- * WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- ** WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

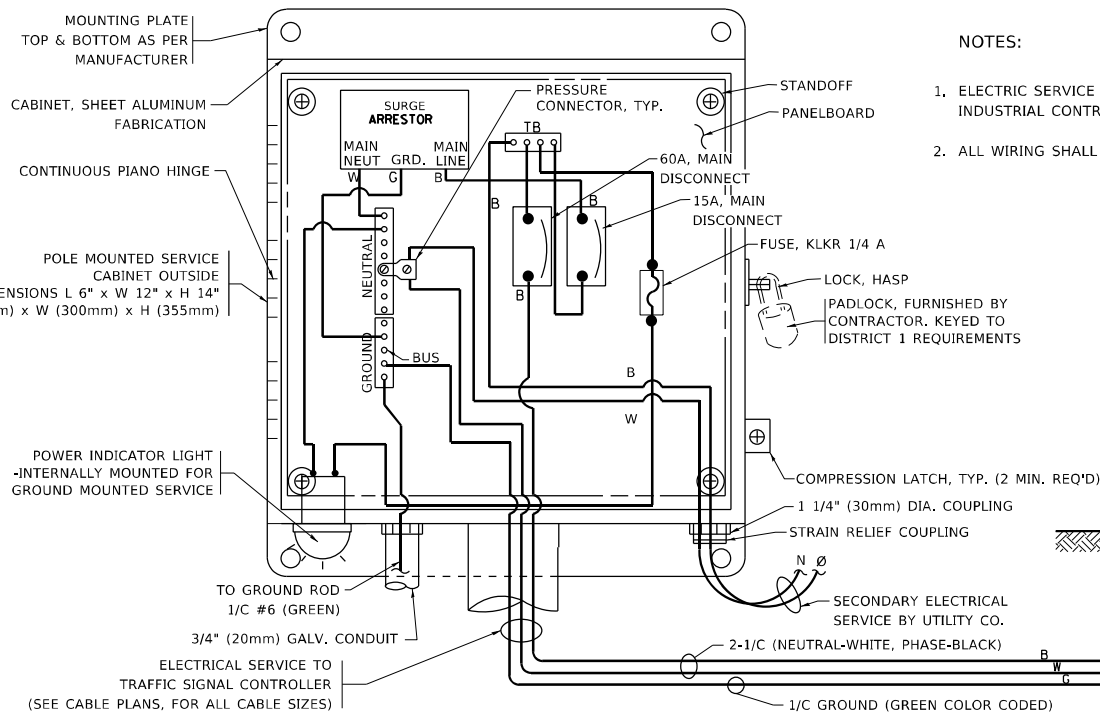
TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

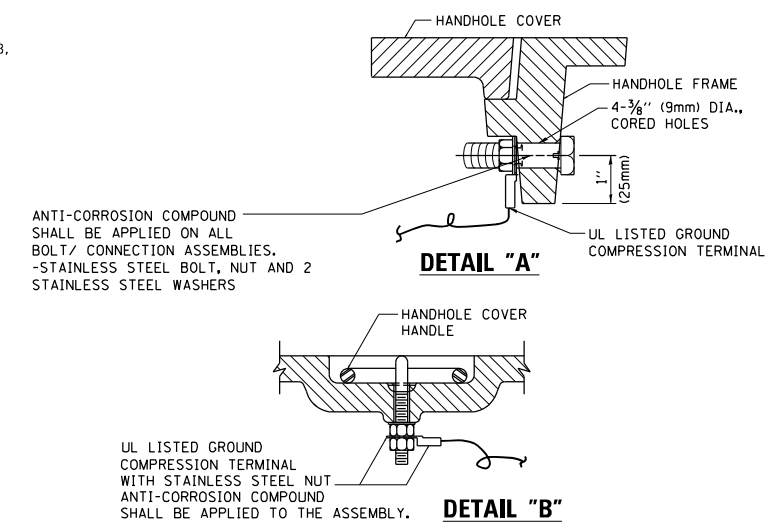
NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

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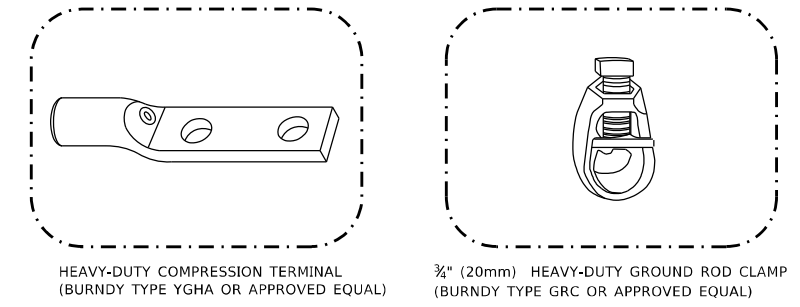


ELECTRICAL SERVICE – PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
 (NOT TO SCALE)



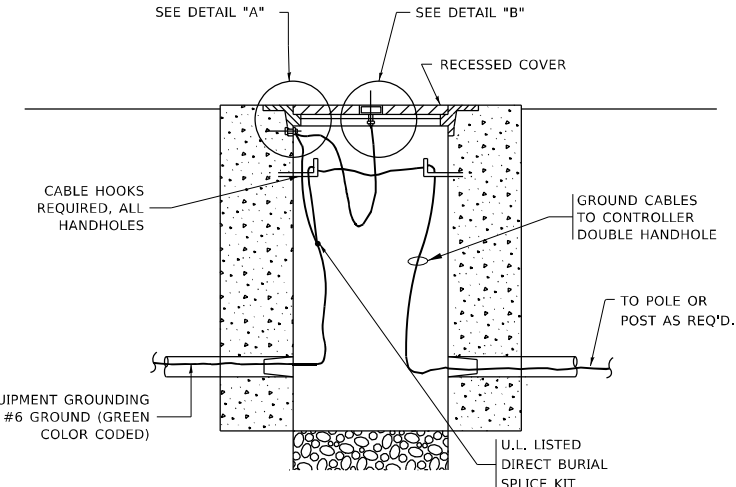
NOTES:
GROUNDING SYSTEM

- THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
- THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
- ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
- THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



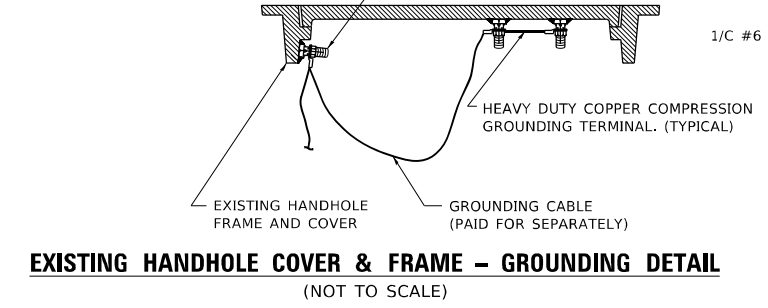
NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES
 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES
 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES.
 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.

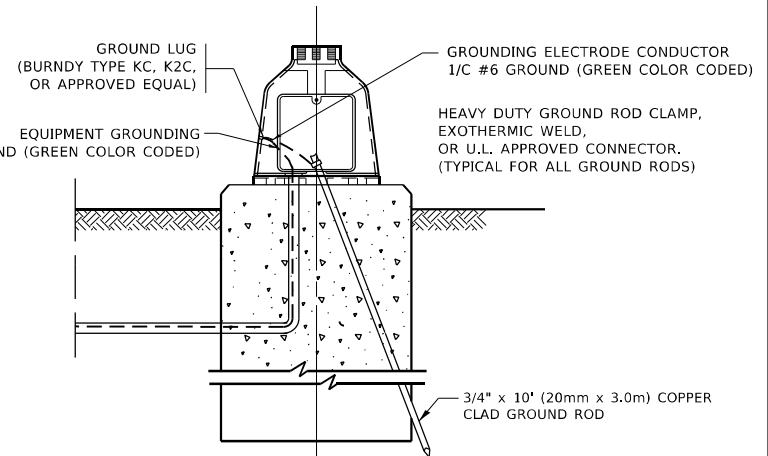


HANDHOLE COVER & FRAME – GROUNDING DETAIL
 (NOT TO SCALE)

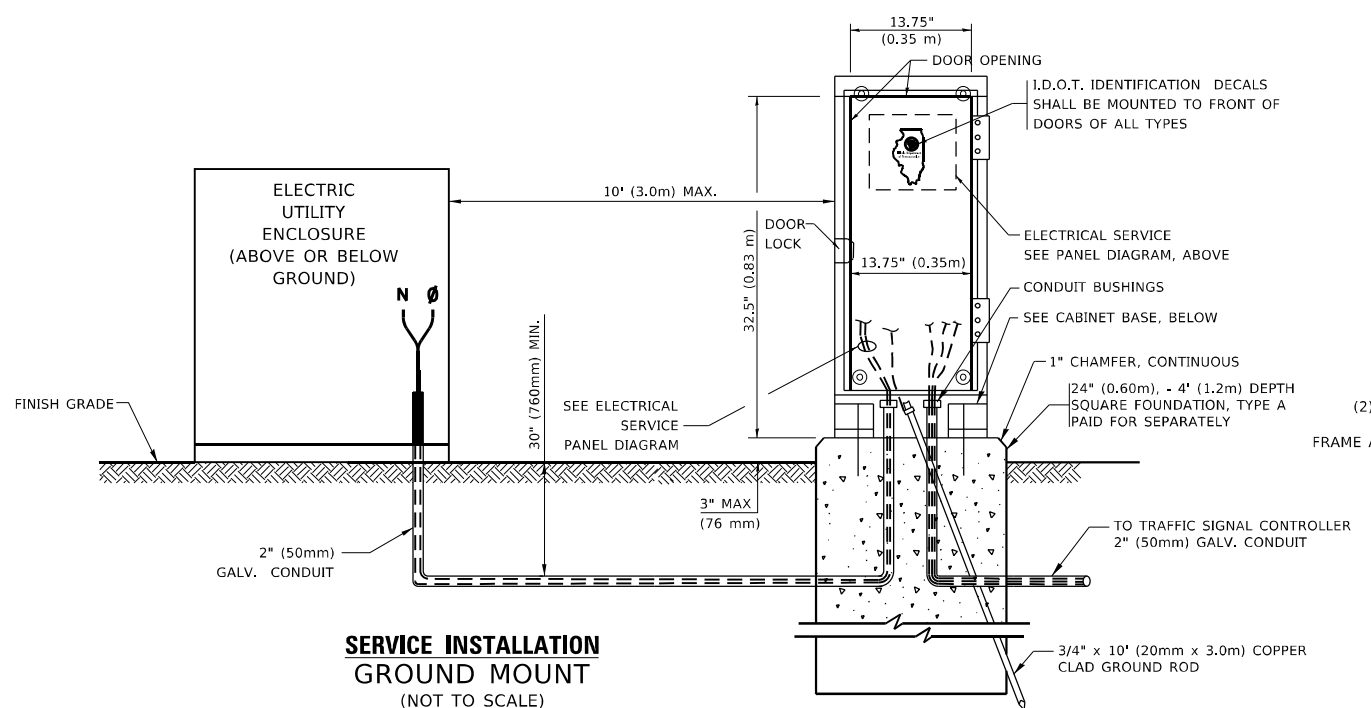
(2) 1/2" x 1 1/4" STAINLESS STEEL BOLT WITH SPLIT LOCK WASHER AND NYLON INSERT LOCKOUT WELDED TO FRAME AND TO COVER. (TYPICAL). ANTI-CORROSION COMPOUND SHALL BE APPLIED TO EACH ASSEMBLY.



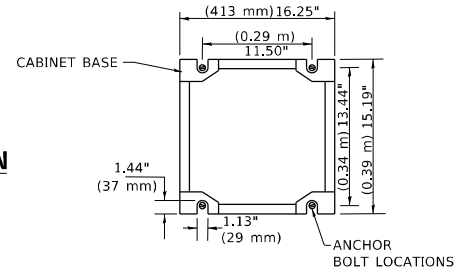
EXISTING HANDHOLE COVER & FRAME – GROUNDING DETAIL
 (NOT TO SCALE)



MAST ARM POLE /POST-GROUNDING DETAIL
 (NOT TO SCALE)



SERVICE INSTALLATION GROUND MOUNT
 (NOT TO SCALE)



CABINET – BASE BOLT PATTERN
 (NOT TO SCALE)

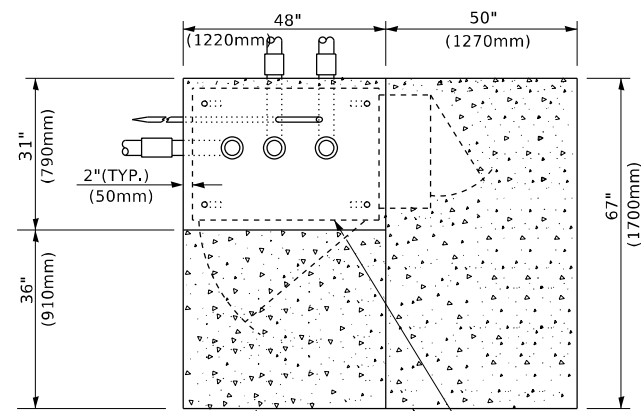
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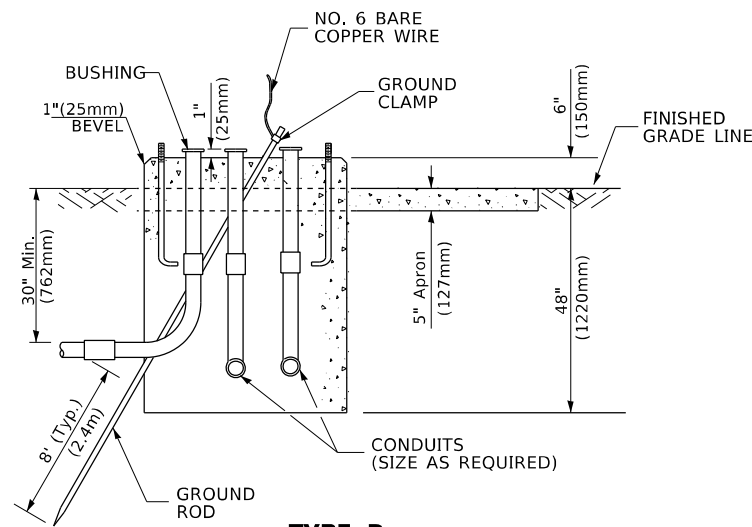
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE			
STANDARD TRAFFIC SIGNAL DESIGN DETAILS			
SCALE: NONE	SHEET 4	OF 7 SHEETS	STA. TO STA.

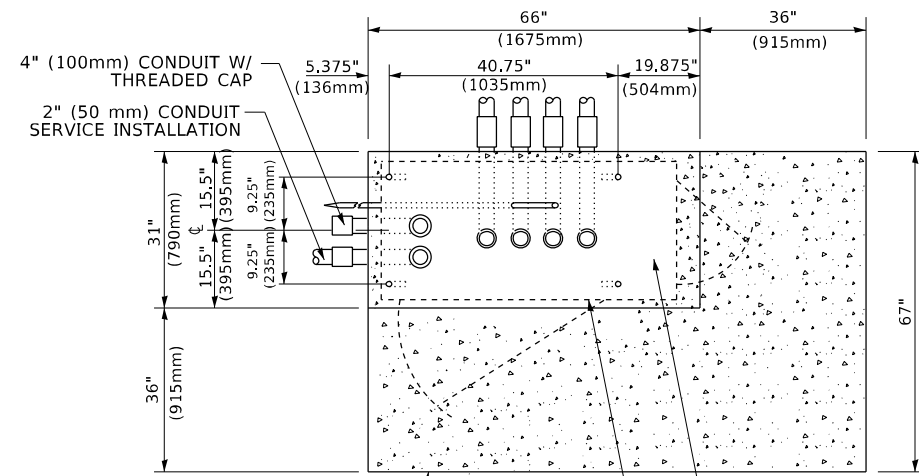
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TS-05		CONTRACT NO. 62T58		
ILLINOIS FED. AID PROJECT				



TOP VIEW



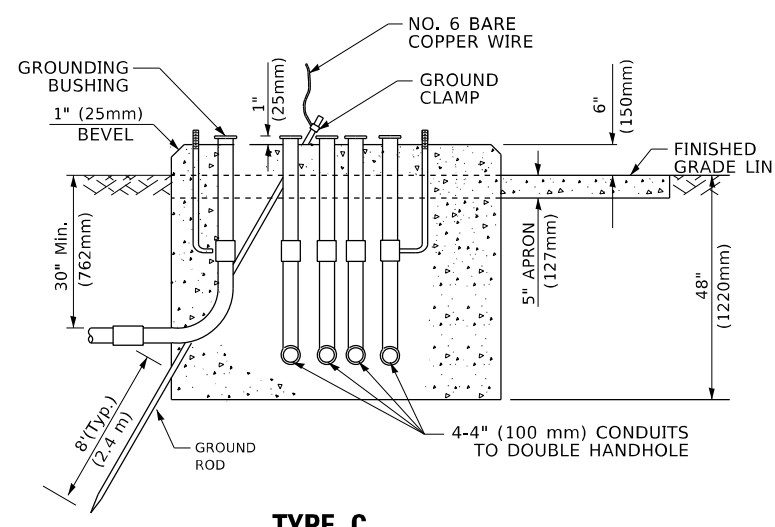
**TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET**



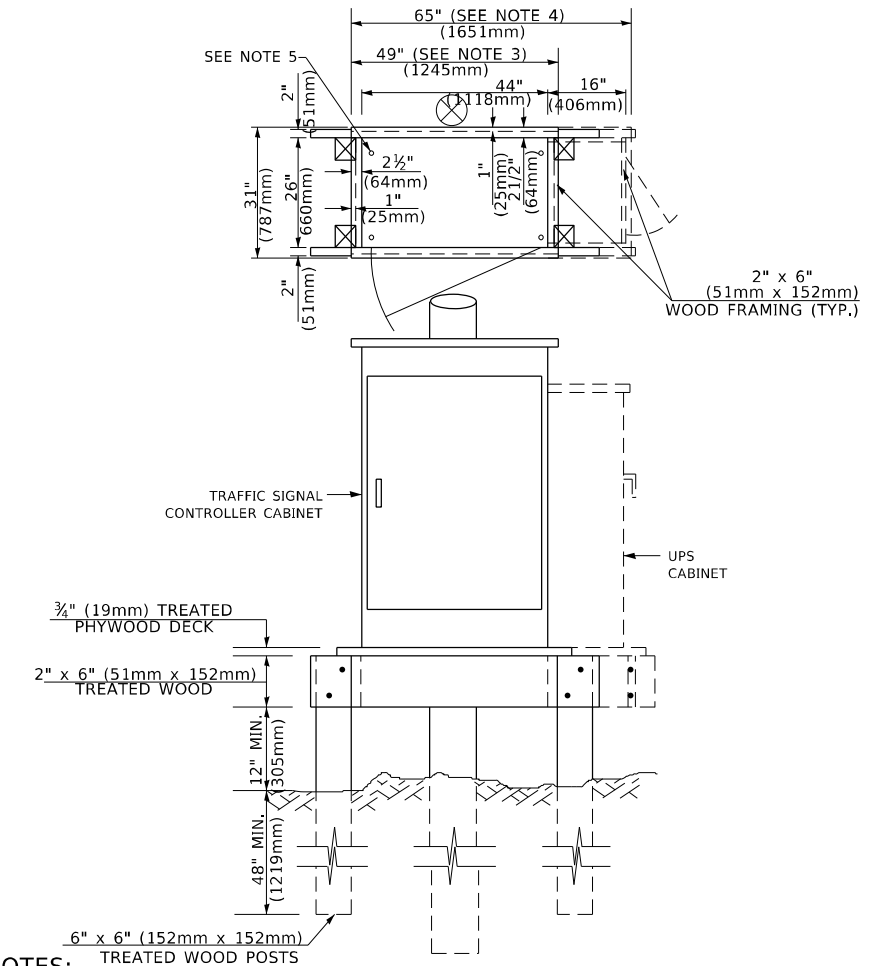
TOP VIEW

NOTE:

TOP OF FOUNDATION SHALL BE HIGHER THAN TOP OF DOUBLE HANDHOLE



**TYPE C
FOR GROUND MOUNTED
SUPER P (TYPE IV) AND SUPER R (TYPE V)
CONTROLLER CABINETS**



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION..

**TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

MAST ARM LENGTH	① FOUNDATION DEPTH	FOUNDATION DIAMETER	SPIRAL DIAMETER	QUANTITY OF REBARS	SIZE OF REBARS
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	11'-0" (3.4 m)	36" (900mm)	24" (600mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 55' (16.8 m) and less than 65' (19.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 75' (22.9 m) and up to 85' (25.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average unconfined compressive strength (qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
4. For mast arm assemblies with dual arms refer to state standard 878001..

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

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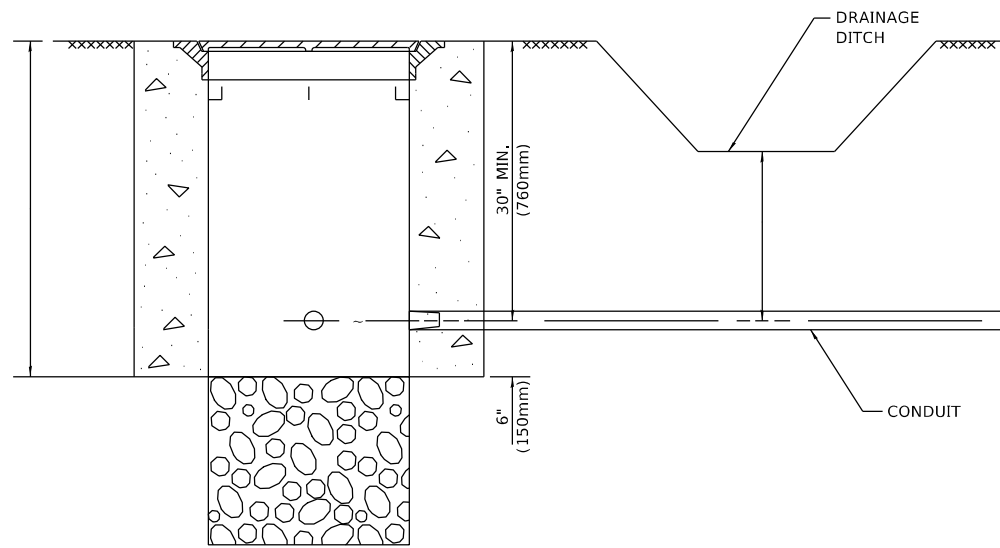
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	DRAWN -	REVISED -
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PLOT DATE = 10/20/2023	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET 5 OF 7 SHEETS STA. TO STA.

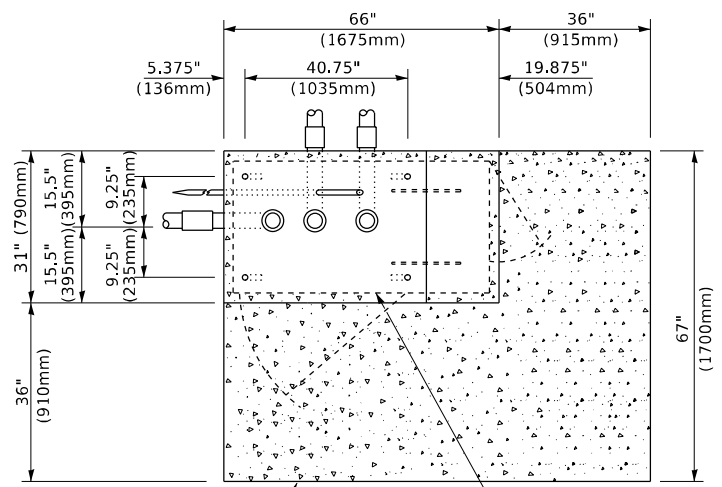
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TS-05		CONTRACT NO. 62T58		
ILLINOIS FED. AID PROJECT				



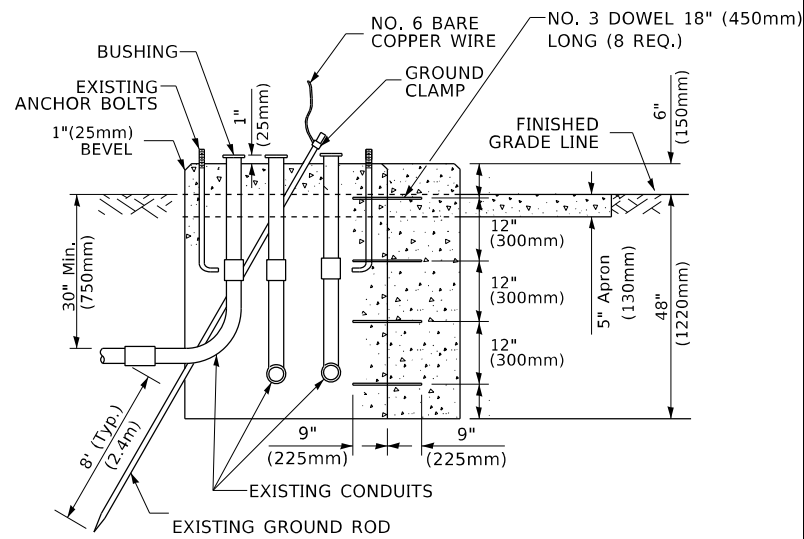
NOTES:

1. CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
2. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
3. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

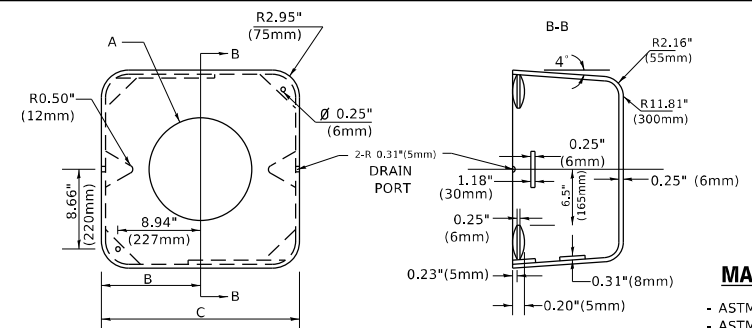
HANDHOLE WITH MINIMUM CONDUIT DEPTH
(NOT TO SCALE)



TOP VIEW
(NOT TO SCALE)



MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION
(NOT TO SCALE)



MATERIAL
- ASTM A36 STEEL
- ASTM A-123 HOT DIPPED GALVANIZED

A	B	C	HEIGHT	WEIGHT
VARIABLES	9.5"(241mm)	19"(483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIABLES	10.75"(273mm)	21.5"(546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIABLES	13.0"(330mm)	26"(660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIABLES	18.5"(470mm)	37"(940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

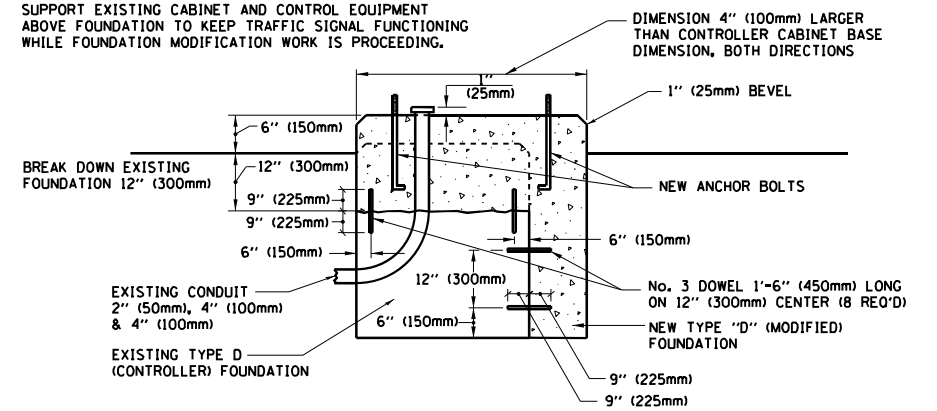
SHROUD

NOTES:

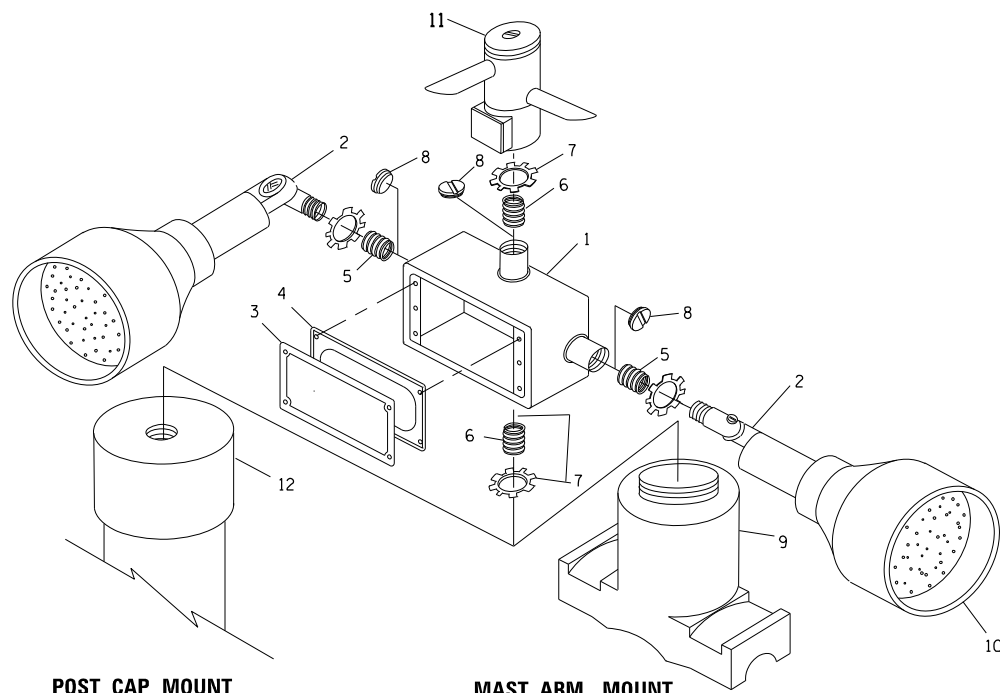
1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
2. THE SUPPLIER SHALL VERIFY THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

NOTE:

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION



POST CAP MOUNT

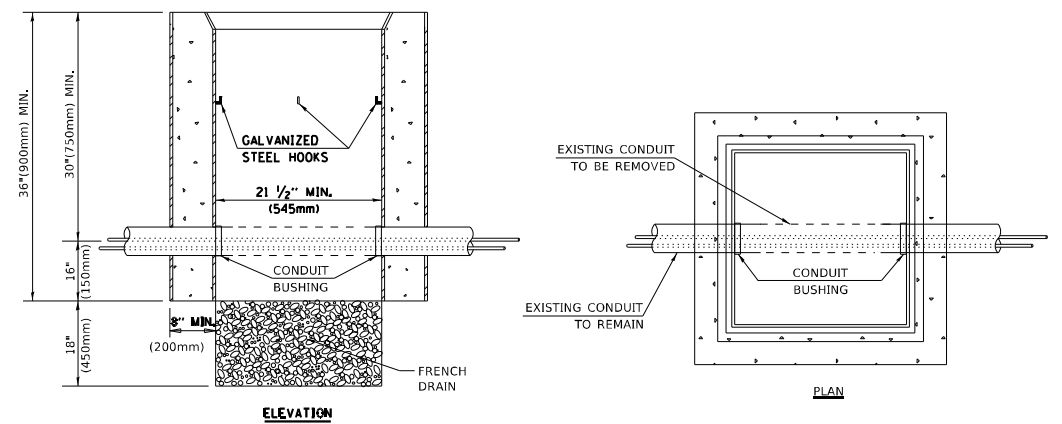
MAST ARM MOUNT

EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL

ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0,000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4" (19 mm) CLOSE NIPPLE
7	3/4" (19 mm) LOCKNUT
8	3/4" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

NOTES:

1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



NOTES:

1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

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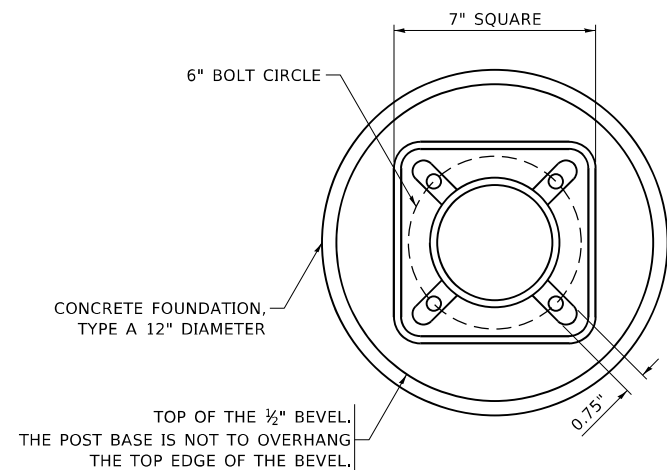
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PLOT DATE = 10/20/2023	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE SHEET 6 OF 7 SHEETS STA. TO STA.

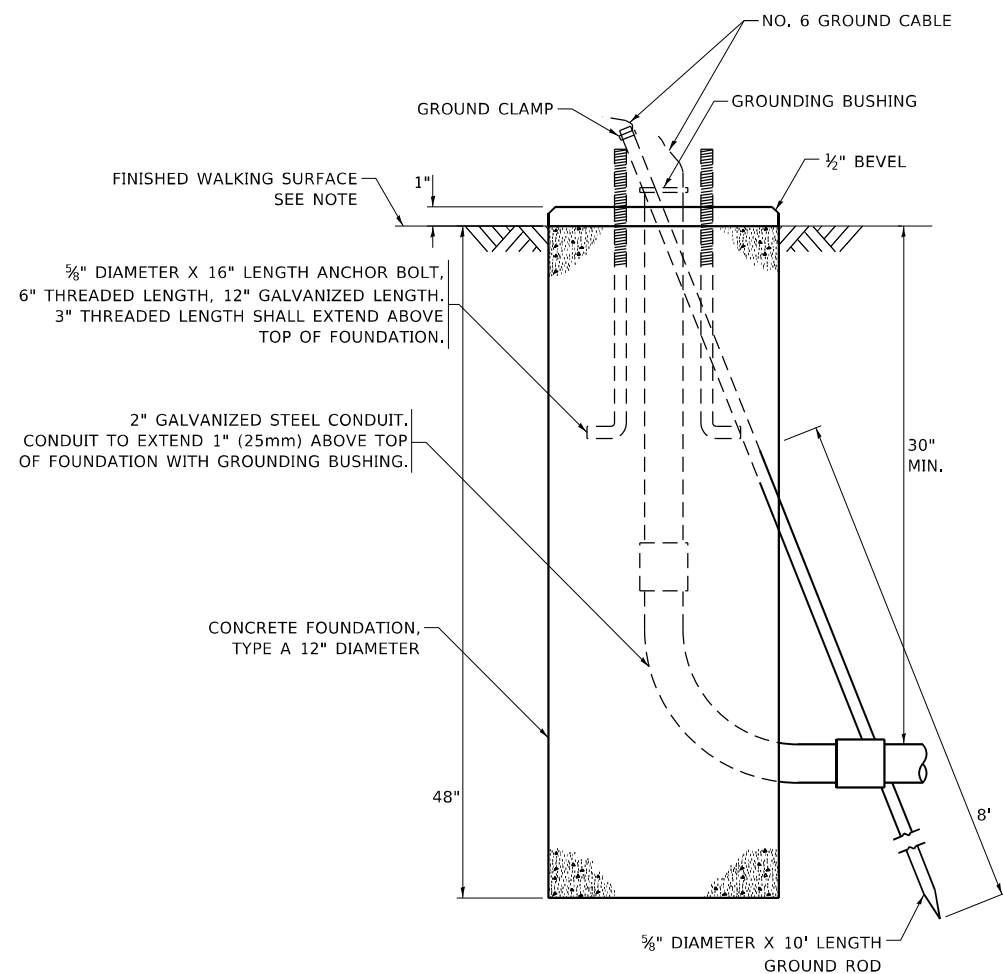
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TS-05		CONTRACT NO. 62T58		
ILLINOIS FED. AID PROJECT				



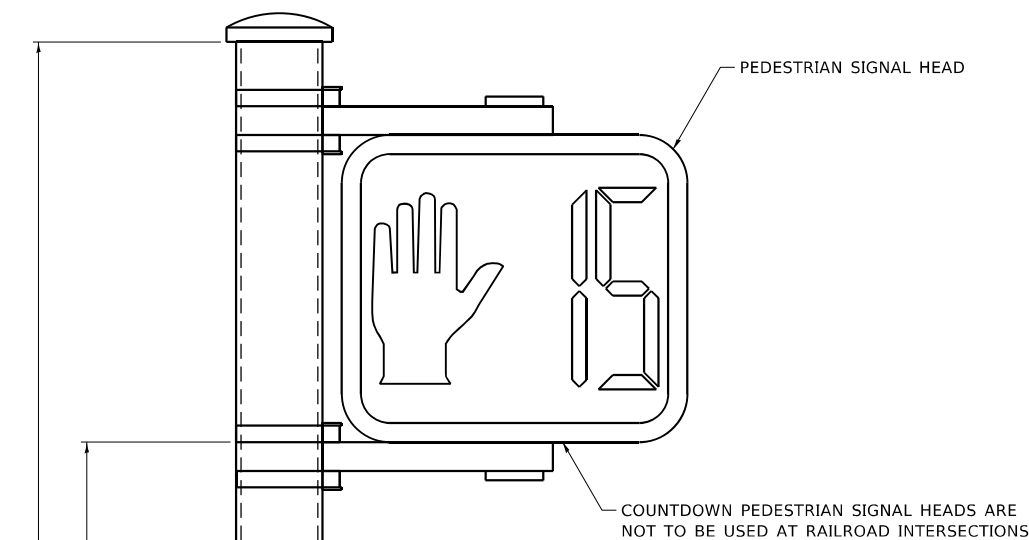
BOLT PATTERN

NOTE:

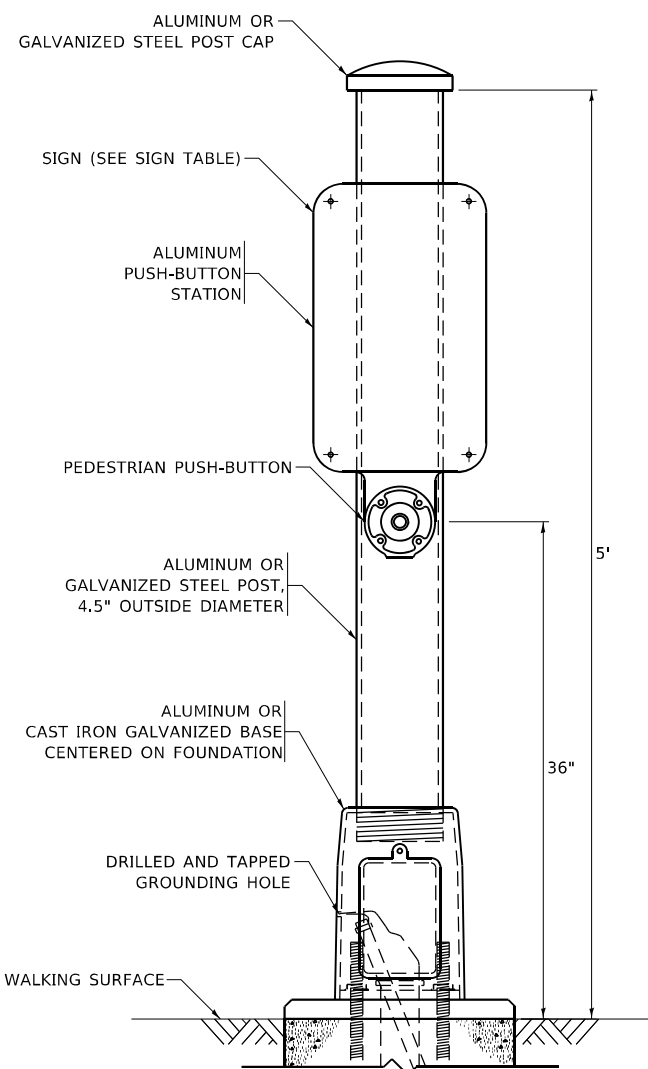
1. IF THE PEDESTRIAN SIGNAL POST FOUNDATION IS INSTALLED WITHIN OR BEHIND A BARRIER CURB, THE TOP OF THE FOUNDATION SHALL BE INSTALLED FLUSH WITH THE TOP OF THE BARRIER CURB.



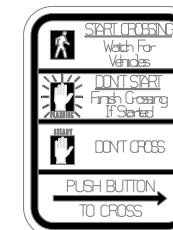
**CONCRETE FOUNDATION,
TYPE A 12-INCH DIAMETER**



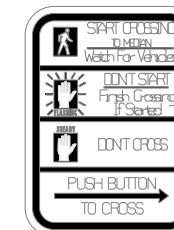
PEDESTRIAN SIGNAL POST, 10 FT.



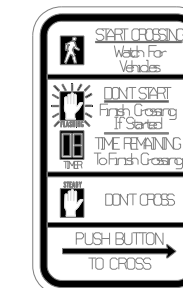
PEDESTRIAN SIGNAL POST, 5 FT.



R10-3b



R10-3d



R10-3e

SIGN TABLE

SIGN	DIMENSIONS
R10-3b (RAILROAD ONLY)	9" X 12"
R10-3d (RAILROAD ONLY)	9" X 12"
R10-3e	9" X 12"

NOTES:

1. THE SIGN PANELS SHALL BE TYPE AP SHEETING.
2. THE ARROW ON SIGNS FOR PUSH-BUTTONS SERVING TWO DIRECTIONS ON THE SAME PHASE SHALL BE BI-DIRECTIONAL.
3. THE SIGN FOR DUAL-CALL PUSH-BUTTONS SHALL HAVE NO ARROW.

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	DATE - 10-15-2018	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

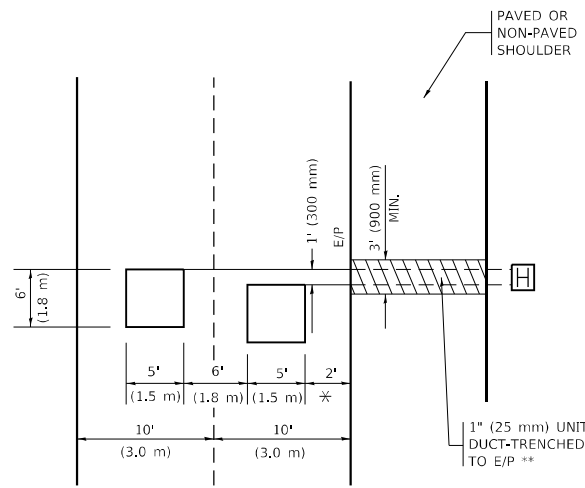
**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET 7 OF 7 SHEETS STA. TO STA.

F.A.P. RTE. 0358	SECTION FAP 0358 22 RS	COUNTY COOK	TOTAL SHEETS 71	SHEET NO. 70
TS-05		CONTRACT NO. 62T58		
ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

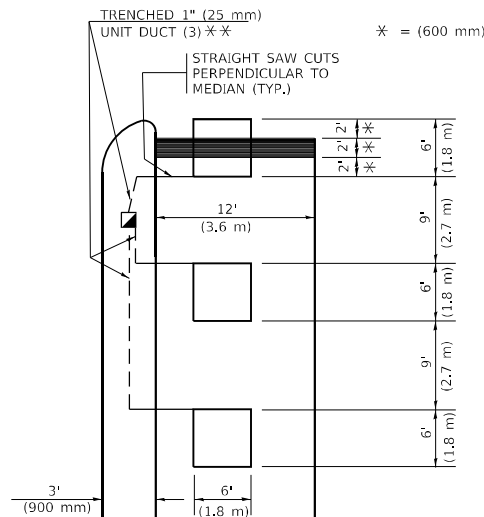


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

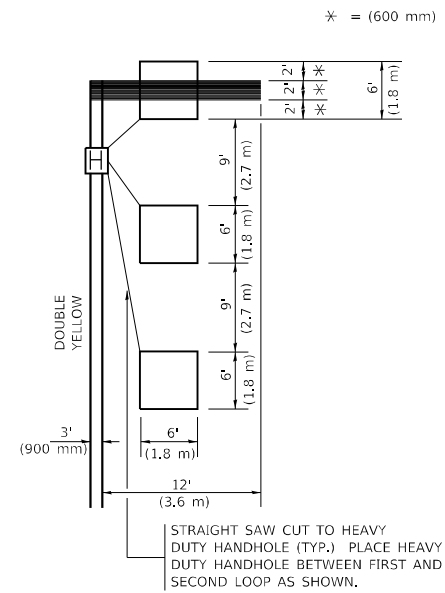


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



* = (600 mm)

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

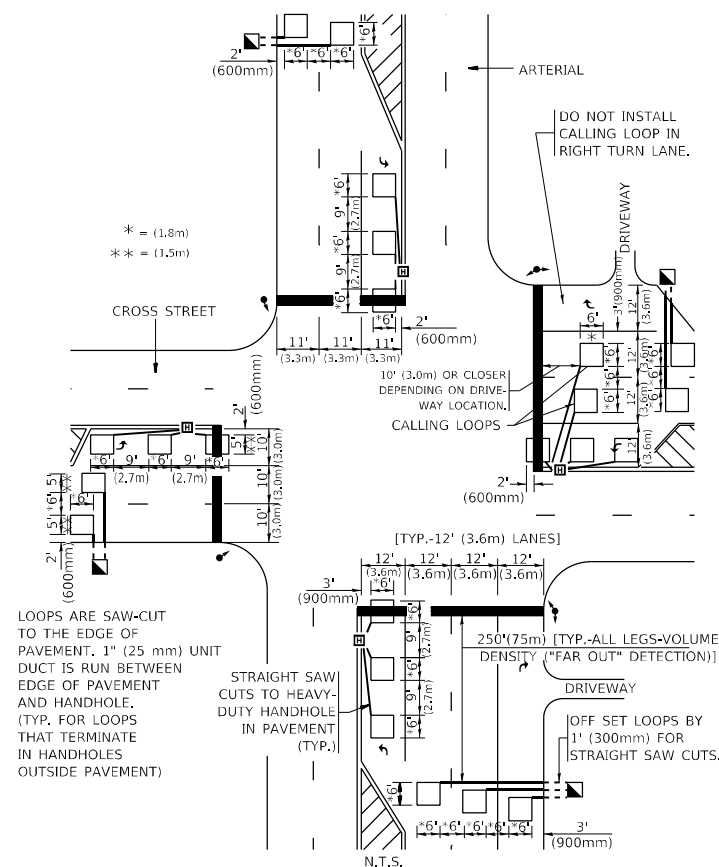
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)



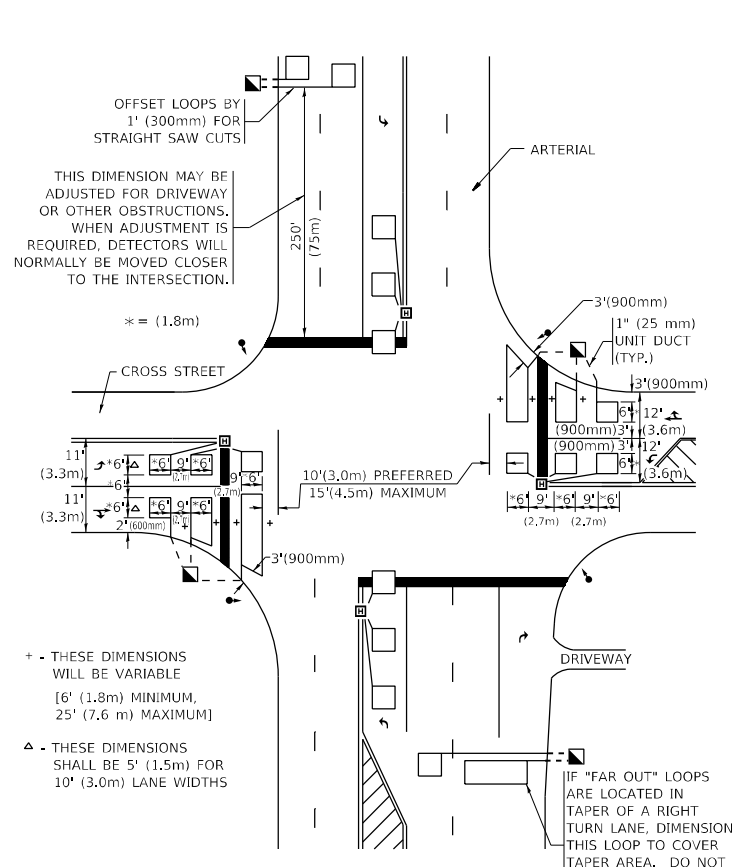
* = (1.8m)
** = (1.5m)

LOOPS ARE SAW-CUT TO THE EDGE OF PAVEMENT. 1" (25 mm) UNIT DUCT IS RUN BETWEEN EDGE OF PAVEMENT AND HANDHOLE. (TYP. FOR LOOPS THAT TERMINATE IN HANDHOLES OUTSIDE PAVEMENT)

STRAIGHT SAW CUTS TO HEAVY-DUTY HANDHOLE IN PAVEMENT (TYP.)

DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



THIS DIMENSION MAY BE ADJUSTED FOR DRIVEWAY OR OTHER OBSTRUCTIONS. WHEN ADJUSTMENT IS REQUIRED, DETECTORS WILL NORMALLY BE MOVED CLOSER TO THE INTERSECTION.

+ - THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]
△ - THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

IF "FAR OUT" LOOPS ARE LOCATED IN TAPER OF A RIGHT TURN LANE, DIMENSION THIS LOOP TO COVER TAPER AREA. DO NOT COVER THE LEFT TURN LANE OR LEFT TURN LANE TAPER.

DETAIL 2
N.T.S.

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