

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	97-3B-1-2	WHITE	15	1

D-97-015-06

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

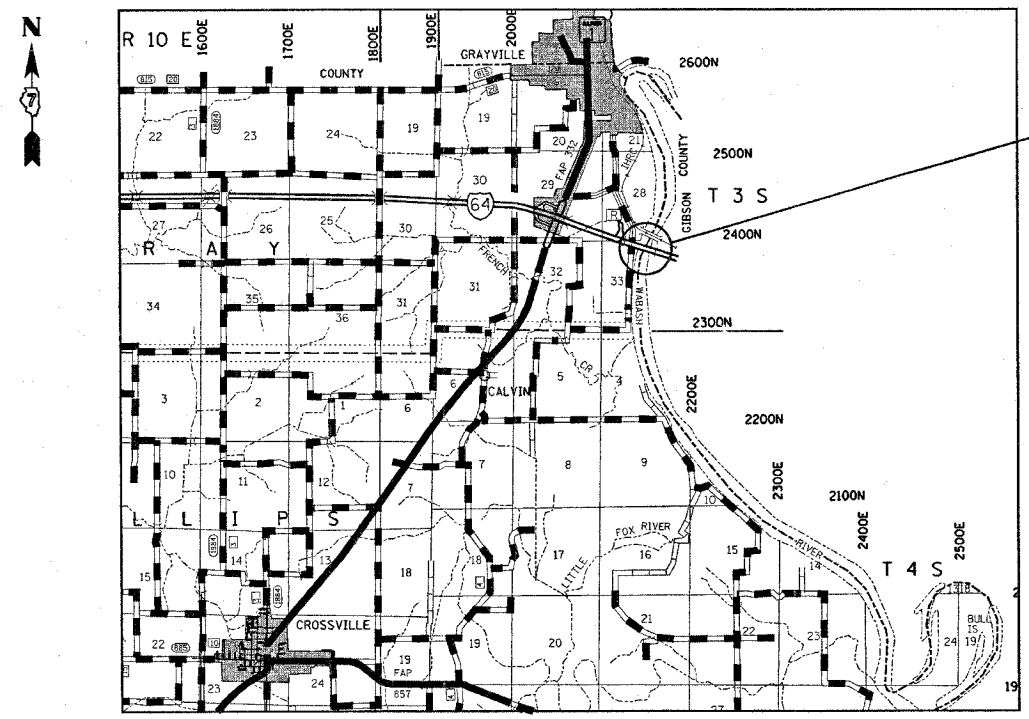
**PROPOSED
HIGHWAY PLANS**

FAI ROUTE 64 (I-64)
SCOUR MITIGATION
WHITE COUNTY, ILLINOIS
POSEY COUNTY, INDIANA

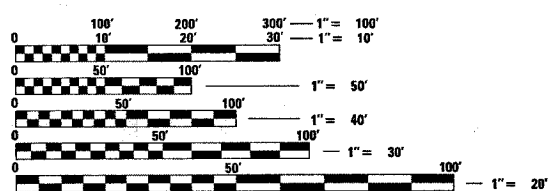
C-97-036-06

SECTION 97-3B-I-2

FOR INDEX OF SHEETS, SEE SHEET NO. 2



LOCATION MAP



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123

2003 ADT=11,380

GROSS LENGTH = 2705.25 FEET = 0.512 MILES
NET LENGTH = 2705.25 FEET = 0.512 MILES

PROJECT ENGINEER : MIKE ALLEN
SQUAD LEADER : BRIAN LEWIS
DESIGNER :
TELEPHONE : 217/342-3951 EX 316

CONTRACT NO. 74142

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED Dec 7, 2005
Christ H. Reed
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

December 9, 2005
Mike Allen
ENGINEER OF DESIGN AND ENVIRONMENT

December 9, 2005
Eric Horn
DEPUTY DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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OF THE STATE OF ILLINOIS**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	97-3B-1-2	WHITE	15	2
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

INDEX OF SHEETS

SHEET NO.	TITLE
1	COVER SHEET
2	INDEX OF SHEETS, GENERAL NOTES
3	SUMMARY OF QUANTITIES
4-5	LOCATION MAP, PLAN VIEW
6-14	DEBRIS REMOVAL
15A.	SHEET PILING AND RIPRAP DETAILS

THE FOLLOWING STANDARDS ARE PART OF THESE PLANS AND ARE INCLUDED FOLLOWING SHEET NUMBER 15:

000001-04 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS

GENERAL NOTES

THIS SECTION SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE PLANS, THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2002; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" INDICATED ON THE CHECK SHEET, AND THE "SPECIAL PROVISIONS" INCLUDED IN THE PROPOSAL.

THE PROPOSED PROJECT IS LOCATED ON INTERSTATE 64 AT THE WABASH RIVER CROSSING.

THE WORK INCLUDED IN THIS SECTION CONSISTS OF CONSTRUCTING SHEET PILING FILLED WITH RIPRAP AROUND FOUR PIERS AND PLACING RIPRAP AROUND TWO OTHER PIERS OF THE TWO INTERSTATE BRIDGES, DEBRIS REMOVAL FROM THE RIVER IN THE VICINITY OF THE INTERSTATE AND RAILROAD BRIDGES, AND ALL OTHER WORK NECESSARY TO COMPLETE THIS SECTION.

TREES SHALL BE PRESERVED THROUGHOUT THIS SECTION AS DIRECTED BY THE ENGINEER EXCEPT FOR A 100' WIDE STRIP ADJACENT TO AND PARALLELING THE INTERSTATE RIGHT OF WAY. TREES MAY BE REMOVED FROM THE AFOREMENTIONED AREA AS NEEDED FOR THE CONTRACTOR'S OPERATIONS. ANY TREES REMOVED SHALL BE REPLACED BY AND DIVIDED EQUALLY AMONG THE SPECIES LISTED IN THE SUMMARY OF QUANTITIES. DISTURBED AREAS SHALL BE RETURNED TO ORIGINAL LINES AND GRADES AT THE COMPLETION OF THE PROJECT.

STRUCTURAL STEEL SALVAGED FROM THE RIVER FROM THE TWO FAILED RAILROAD TRUSSES SHALL REMAIN THE PROPERTY OF THE RAILROAD OWNER AND SHALL BE STOCKPILED ON THE RAILROAD RIGHT OF WAY.

ACCESS TO THE PROJECT SHALL BE FROM THE SOUTH ON THE TOWNSHIP ROAD SYSTEM AS SHOWN ON SHEET NUMBER 4 OF THE PLANS UNLESS PERMISSION IS OBTAINED FROM THE ROAD COMMISSIONER TO USE THE NORTHERN ACCESS.

THE LOCATIONS AND/OR DEPTHS OF UNDERGROUND UTILITIES SHOWN HAVE BEEN TAKEN FROM INFORMATION FURNISHED BY THE UTILITY OWNERS AND MUST BE CONSIDERED APPROXIMATE. FIELD MARKINGS OF FACILITIES IN CRITICAL AREAS MAY BE OBTAINED BY PROVIDING A MINIMUM OF 96 HOURS ADVANCE NOTICE THROUGH THE J.U.L.I.E. SYSTEM BY CALLING 800-892-0123.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS & GENERAL NOTES

SCALE: VERT.
DATE HORIZ.

DRAWN BY
CHECKED BY

Rev.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	97-3B-1-2	WHITE	15	3
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE 3FY-2A 50% STATE 50% INDIANA
CODE NO	ITEM	UNIT		
X0325212	DEBRIS REMOVAL (SPECIAL)	CAL DA	20	20
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	270	270
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	200	200
28100809	STONE DUMPED RIPRAP, CLASS A5	TON	5050	5050
28100811	STONE DUMPED RIPRAP, CLASS A6	TON	2400	2400
50200300	COFFERDAM EXCAVATION	CU YD	360	360
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	8	8
67100100	MOBILIZATION	L SUM	1	1
* A2002316	TREE, BETULA NIGRA (RIVER BIRCH), 2" CALIPER, BALLED AND BURLAPPED	EACH	14	14
* A2005816	TREE, PLATANUS OCCIDENTALIS (SYCAMORE), 2" CALIPER, BALLED AND BURLAPPED	EACH	14	14
* A2007616	TREE, TAXODIUM DISTICHUM (COMMON BALD CYPRESS), 2" CALIPER, BALLED AND BURLAPPED	EACH	14	14
X5121800	PERMANENT STEEL SHEET PILING	SQ FT	10430	10430
Z0015500	DEBRIS REMOVAL	L SUM	1	1

*SPECIALTY ITEMS

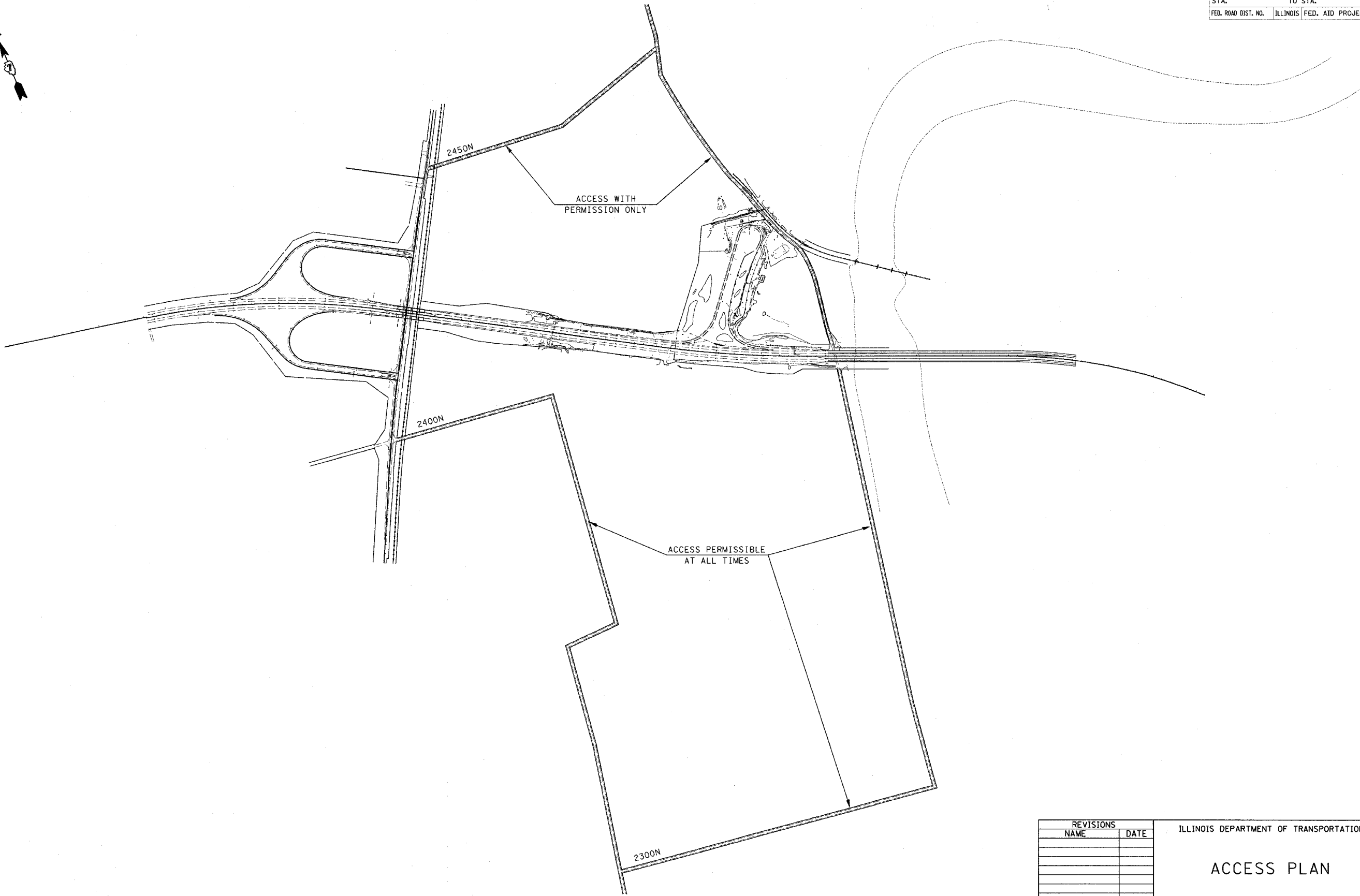
PLOT DATE = 12/2/2005
 FILE NAME = 74142.dwg
 PLOT SCALE = 50.0000 / IN.
 USER NAME = bscrltpr

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		SUMMARY OF QUANTITIES

SCALE: VERT. DATE: HORIZ. DRAWN BY: CHECKED BY:

CONTRACT NO. 74142

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	97-3B-1-2	WHITE	15	4
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



REVISIONS	
NAME	DATE

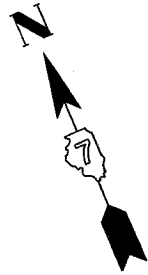
ILLINOIS DEPARTMENT OF TRANSPORTATION

ACCESS PLAN

SCALE: VERT. DRAWN BY
 DATE HORIZ. CHECKED BY

PLOT DATE = 12/6/2005
 FILE NAME = C:\pwworkspace\74142\atl\atl1066.dgn
 PLOT SCALE = 1/8" = 1'-0"
 USER NAME = shwartz

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	97-3B-1-2	WHITE	15	5
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



4" GAS MAIN (STEEL) 400 PSI
RUNS ALONG EAST SIDE OF ROAD

50' 50'

TEMPORARY EASEMENT AREA
BOUNDARIES:
NORTH = RAILROAD R.O.W.
SOUTH = INTERSTATE R.O.W.
EAST = RIVER
WEST = TOWNSHIP ROAD R.O.W.

WABASH RIVER

4" GAS MAIN (STEEL) 400 PSI
RUNS ALONG EAST SIDE OF ROAD

AREA WHERE TREE
REMOVAL IS PERMITTED

5860 5865 5870 5875 5880 5885 5890

PLOT DATE = 12/8/2005
PLOT SCALE = 1"=100'
USER NAME =

REVISIONS	
NAME	DATE

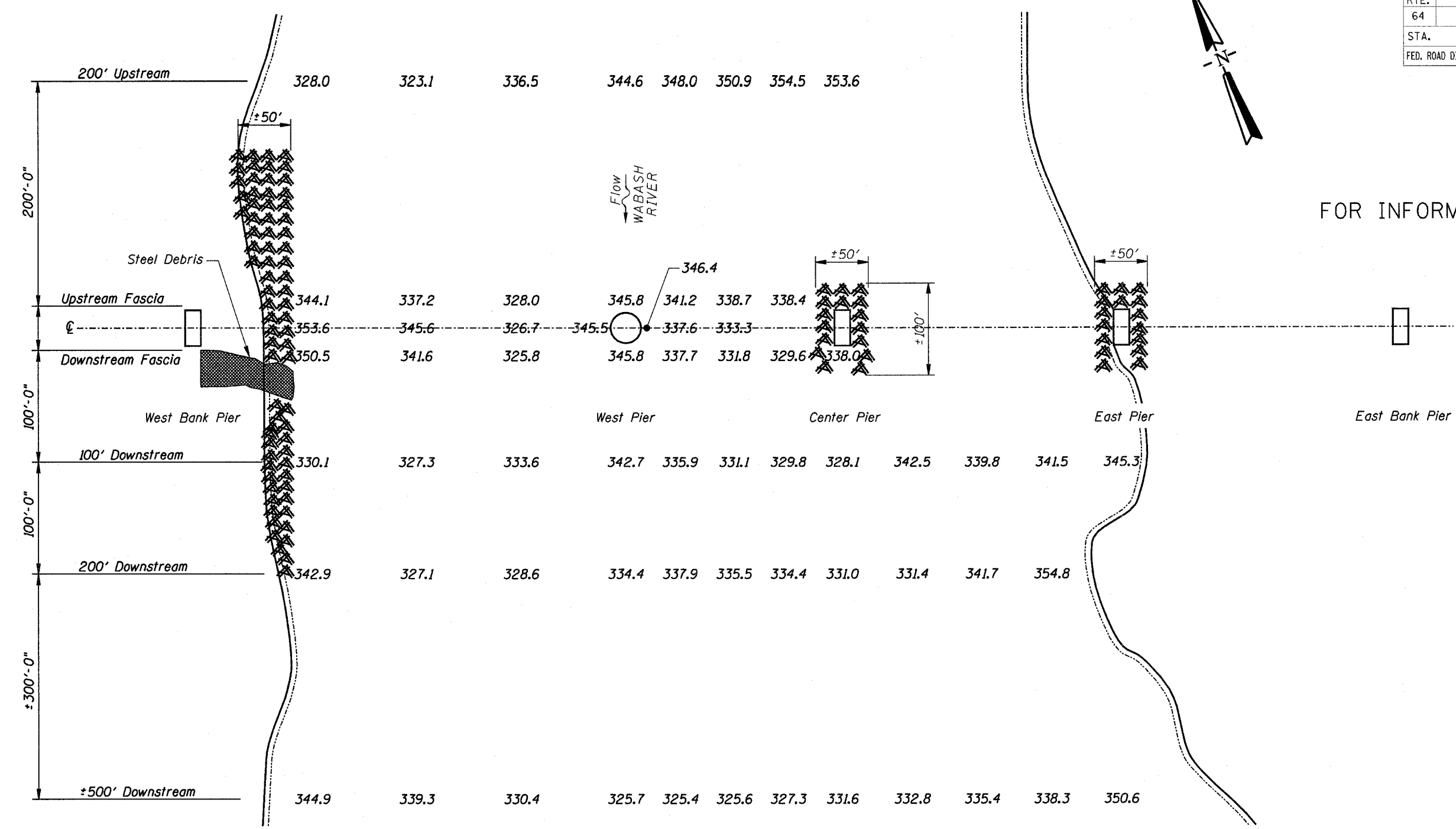
ILLINOIS DEPARTMENT OF TRANSPORTATION

PLAN VIEW

SCALE: VERT.
HORIZ.
DATE

DRAWN BY
CHECKED BY

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	97-3B-1-2	WHITE	15	6
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



FOR INFORMATION ONLY

PLAN

General Notes:

1. At the time of the inspection, the waterline was located approximately 14.4 feet below the bottom of the pier beam at Eastbound Pier 3 of (S.N. 097-0003). This corresponds to a waterline elevation of 358.1 feet, based on the 1966 design plans of (Str. Nos. 097-0003 & 097-0004).
2. Soundings indicate the channel bottom elevation at the time of inspection and are measured in feet.
3. Soundings were taken parallel to the bridge fascias, along the centerline of the structure, as well as 200 feet upstream and downstream of the bridge and 100 feet & 500 feet downstream of the structure.
4. The water velocity of the Wabash River at the time of inspection was approximately 1.0 feet per second.

LEGEND:

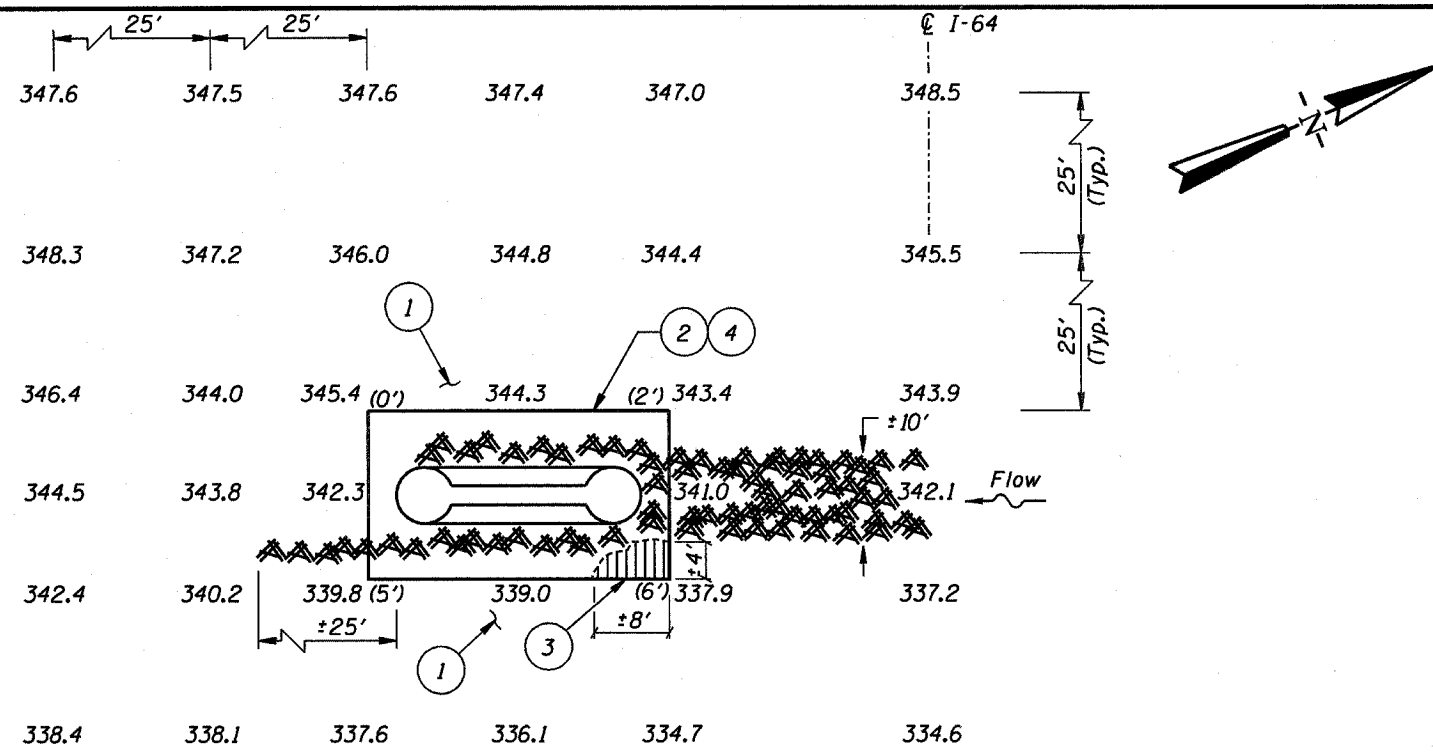
334.4	Channel Bottom Elevation
	Timber Debris
	Steel Debris

**ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**RAILROAD BRIDGE OVER
WABASH RIVER**

Drawn By: DR	 123 North Wacker Drive Suite 300 Chicago, IL 60606 (312) 794-9300 www.collinsengr.com ILLINOIS PROFESSIONAL DESIGN FIRM LICENSE NO. 184-000993	Date: JULY, 2005
Checked By: JEO		Scale: 1"=100'
Code: 4195RRBridge		Figure No.: 3

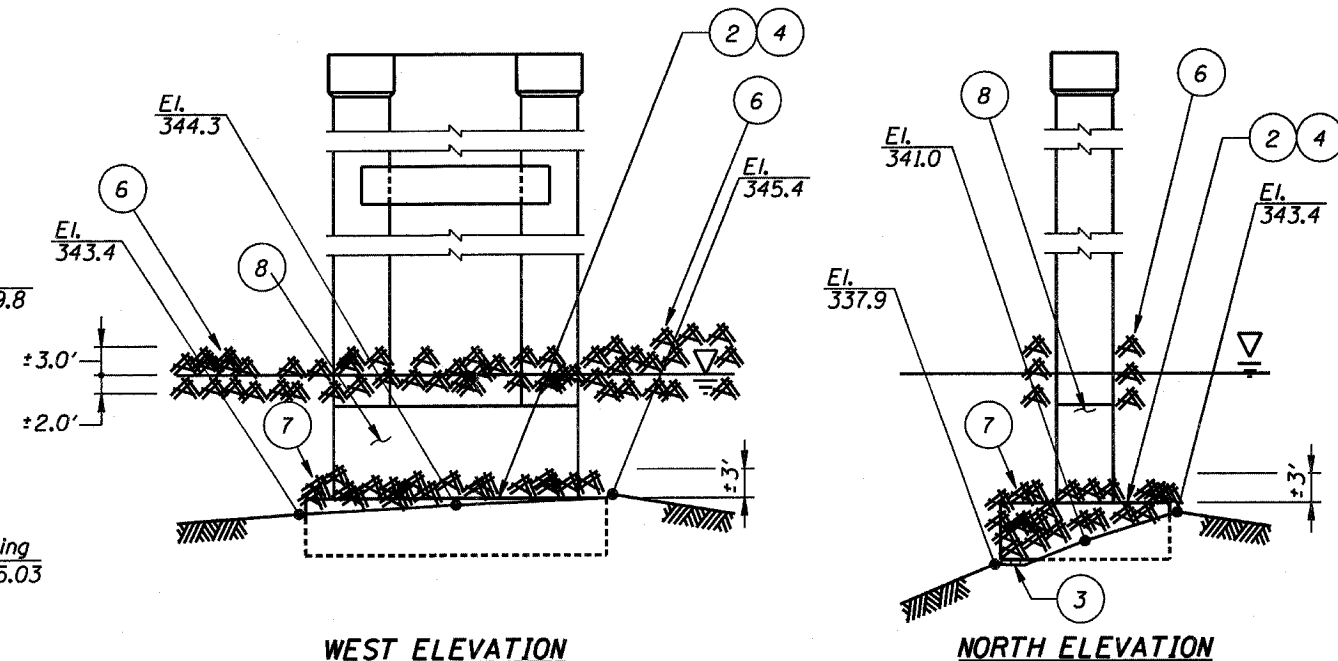
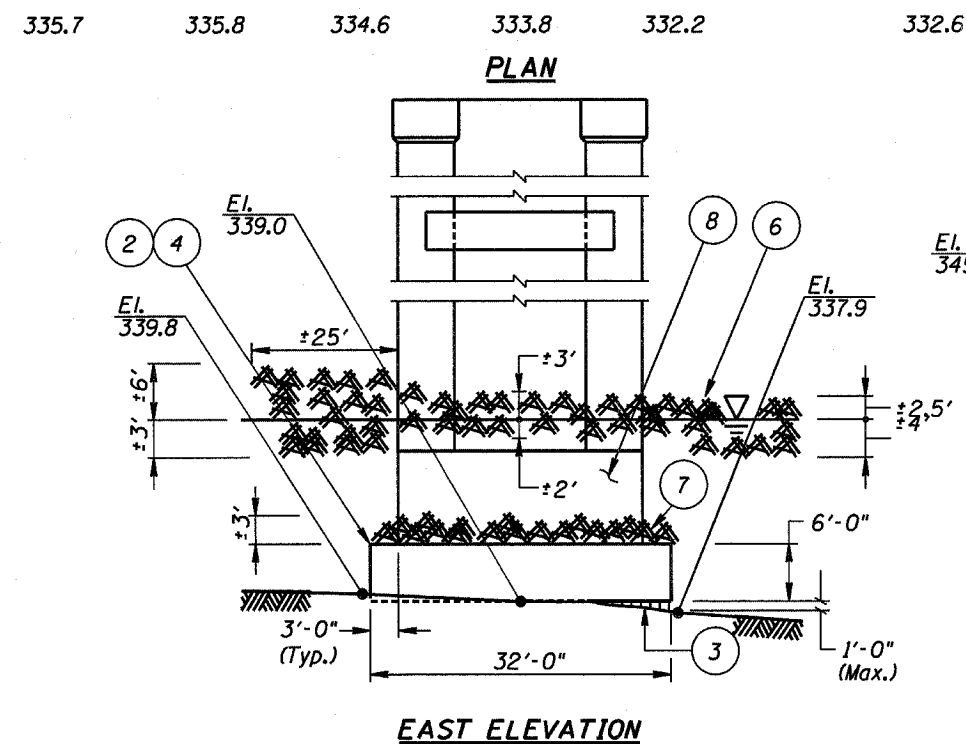
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	97-3B-I-2	WHITE	15	7
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



Inspection Notes:

- ① The channel bottom material typically consisted of sandstone, which was fragmented along the upstream nose of the pier.
- ② Footing exposure, with vertical limits as shown.
- ③ Footing undermined, creating a 1-foot vertical cavity with up to 4 feet of penetration.
- ④ Timber formwork covered approximately 90 percent of the vertical concrete footing surface.
- ⑤ Minor area of concrete section loss located 6 feet above the waterline. The area measured 16 inches wide and 4 inches high, with up to 4 inches of penetration.
- ⑥ Heavy accumulation of timber debris, consisting of trees measuring up to 3 feet in diameter with associated branches.
- ⑦ Heavy accumulation of timber debris, consisting of trees measuring up to 1.5 feet in diameter with associated branches.
- ⑧ No damage below waterline.

FOR INFORMATION ONLY



LEGEND:

- 558.0 Channel Bottom Elevation
- ④ Indicates Inspection Note Number
- Pier Founded on Rock
- Channel Bottom Material
- (2') Vertical Exposure of Footing
- Indicates Undermined Area

General Notes:

1. At the time of the inspection, the waterline was located approximately 14.4 feet below the bottom of the pier beam at Eastbound Pier 3. This corresponds to a waterline elevation of 358.1 feet, based on the 1966 design plans.
2. Soundings indicate the channel bottom elevation at the time of inspection and are measured in feet.

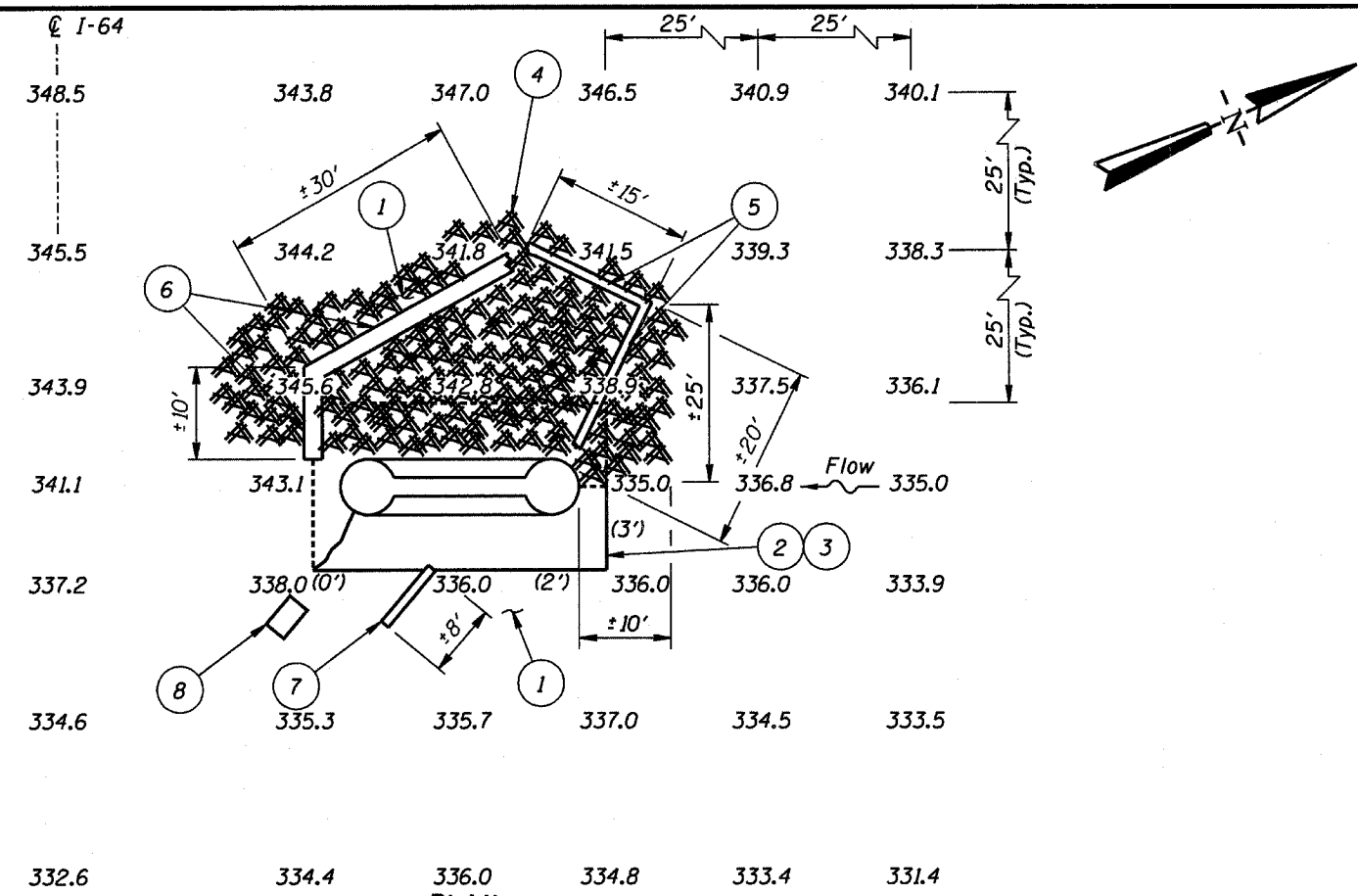
* Elevation per design drawing provided by IDOT.

ILLINOIS DEPARTMENT OF TRANSPORTATION

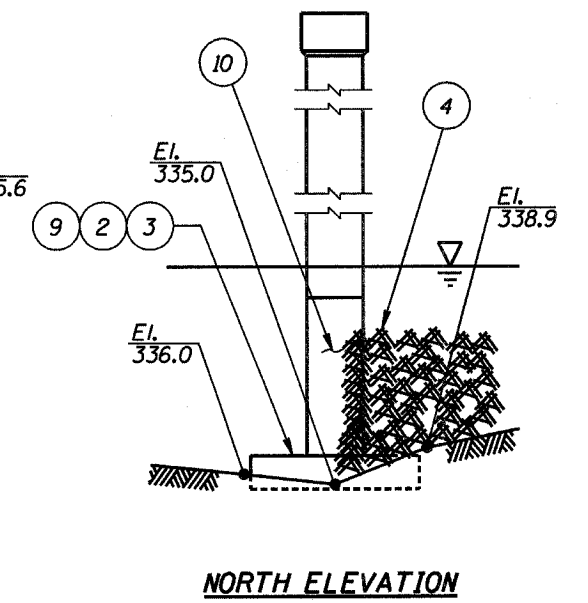
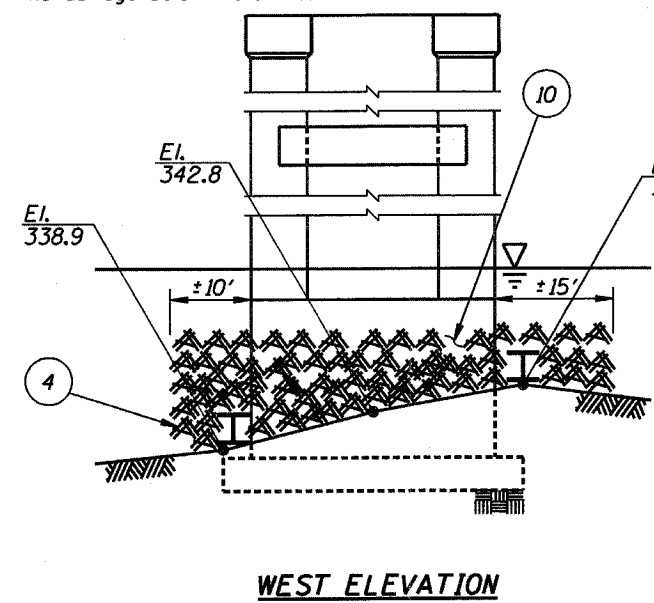
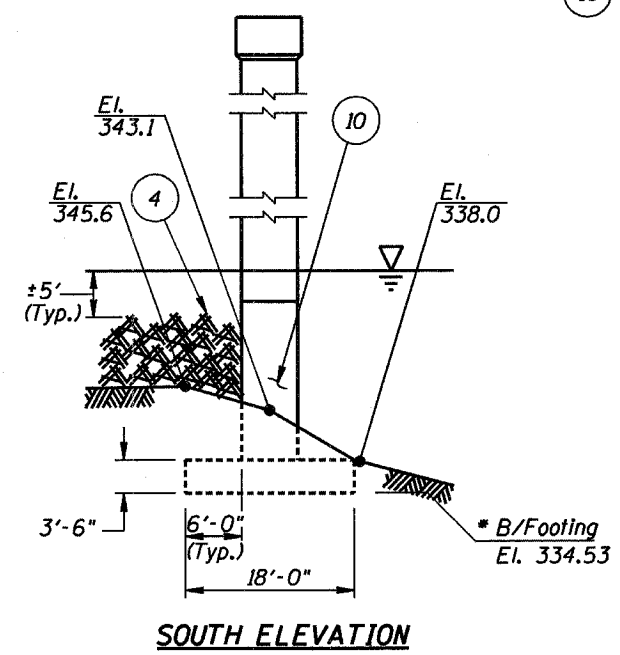
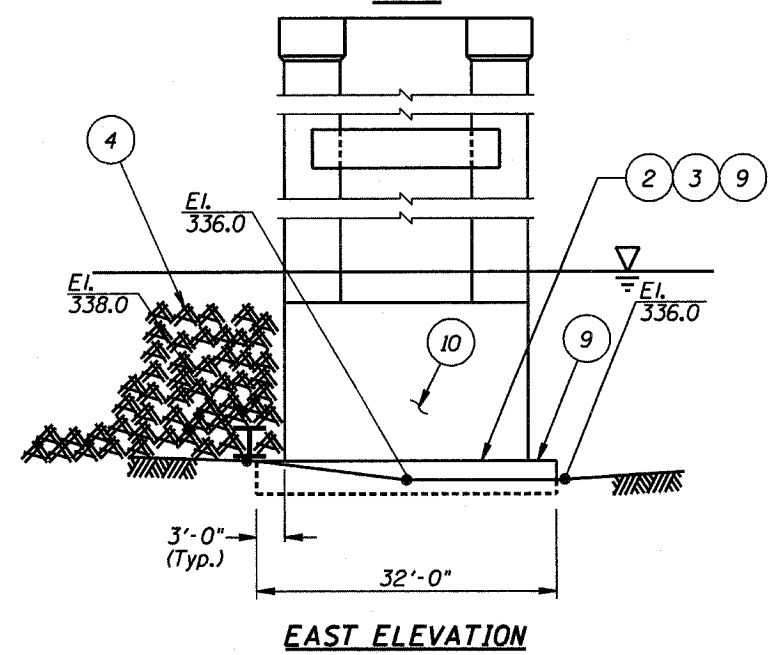
I-64 OVER
WABASH RIVER
STRUCTURE NUMBER: 097-0003
EASTBOUND PIER 3

Drawn By: PRH/DR	COLLINS ENGINEERS 123 North Wacker Drive Suite 300 Chicago, IL 60606 (312) 704-9300 www.collinsengr.com ILLINOIS PROFESSIONAL DESIGN FIRM LICENSE NO. 184-000993	Date: JULY, 2005
Checked By: JEO		Scale: 1"=20'
Code: 419503-04		Figure No.: 14

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	97-3B-1-2	WHITE	15	8
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



- Inspection Notes:**
- 1 The channel bottom material typically consisted of fragmented sandstone.
 - 2 Footing exposure, with vertical limits as shown. Due to heavy timber and steel debris accumulations, the footing could not be detected along the west fascia or western half of the upstream nose. In the area where the footing was exposed, no undermining was detected.
 - 3 Timber formwork covered approximately 90 percent of the vertical concrete footing surface.
 - 4 Heavy accumulation of timber debris, consisting of trees measuring up to 3 feet in diameter and random railroad ties.
 - 5 Steel debris, consisting of two steel beams connected by a gusset plate. The beams measured 15 feet and 20 feet in length, and were approximately 1 foot deep. Small circular ribs were noted along the steel beams, indicating riveted fasteners consistent with railroad truss construction. Due to the heavy accumulations of timber debris covering portions of these beams, exact measurements could not be obtained. While the locations of the observed beams were noted, additional steel debris may intermixed below the large drift accumulation.
 - 6 Steel debris, consisting of two steel beams connected by a gusset plate. The beams measured 10 feet and 30 feet in length, and were approximately 2 feet deep. Small circular ribs were noted along the steel beams, indicating riveted fasteners consistent with railroad truss construction. Due to the heavy accumulations of timber debris covering portions of these beams, exact measurements could not be obtained. While the locations of the observed beams were noted, additional steel debris may intermixed below the large drift accumulation.
 - 7 Steel debris, consisting of an 8-foot long steel beam measuring approximately 1 foot in depth.
 - 8 Steel debris, consisting of a 55-gallon drum located along the channel bottom.
 - 9 No change in exposure condition from June 2005 inspection.
 - 10 No damage below waterline.



- General Notes:**
1. At the time of the inspection, the waterline was located approximately 14.4 feet below the bottom of the pier beam at Eastbound Pier 3. This corresponds to a waterline elevation of 358.1 feet, based on the 1966 design plans.
 2. Soundings indicate the channel bottom elevation at the time of inspection and are measured in feet.

* Elevation per design drawing provided by IDOT.

- LEGEND:**
- 558.0 Channel Bottom Elevation
 - ④ Indicates Inspection Note Number
 - ▨ Pier Founded on Rock
 - ▨ Channel Bottom Material
 - (2') Vertical Exposure of Footing

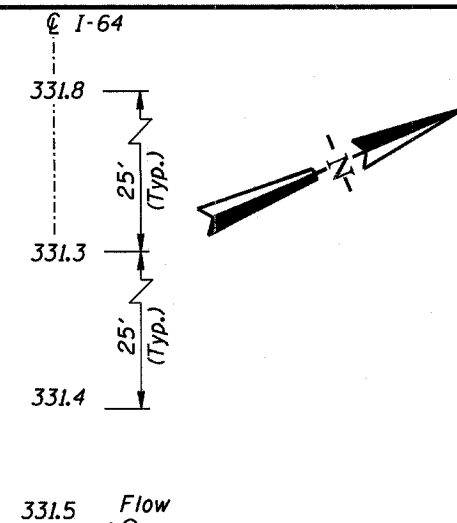
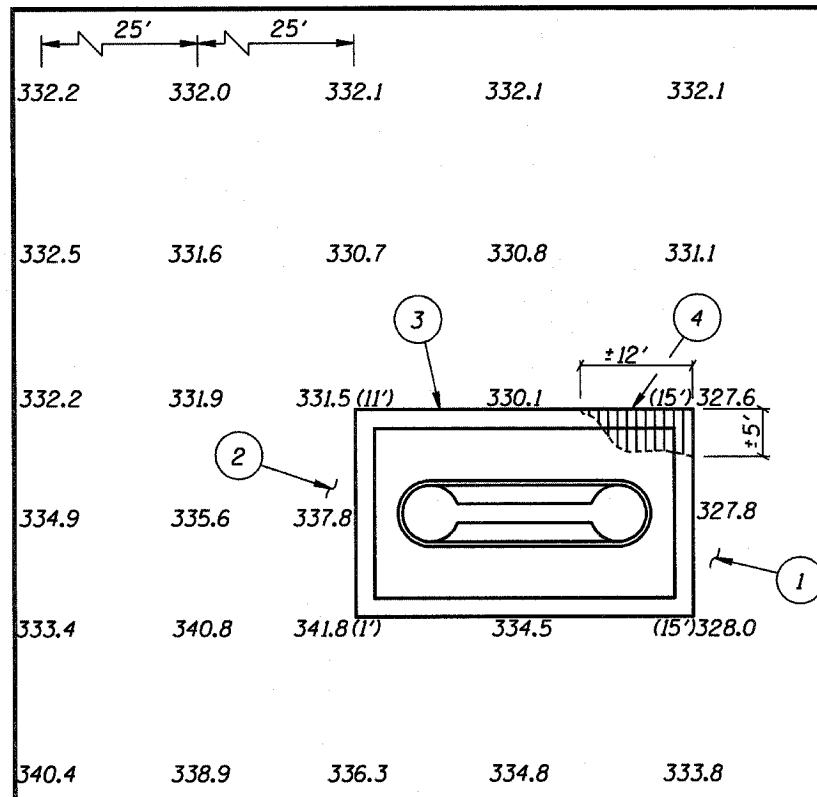
FOR INFORMATION ONLY

ILLINOIS DEPARTMENT OF TRANSPORTATION

I-64 OVER
WABASH RIVER
STRUCTURE NUMBER: 097-0004
WESTBOUND PIER 3

Drawn By: PRH/DR	COLLINS ENGINEERS <small>123 North Wacker Drive Suite 300 Chicago, IL 60606 (312) 704-9300 www.collinsengr.com</small>	Date: JULY, 2005
Checked By: JEO		Scale: 1"=20'
Code: 419503-04		Figure No.: 15

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	97-3B-I-2	WHITE	15	9
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



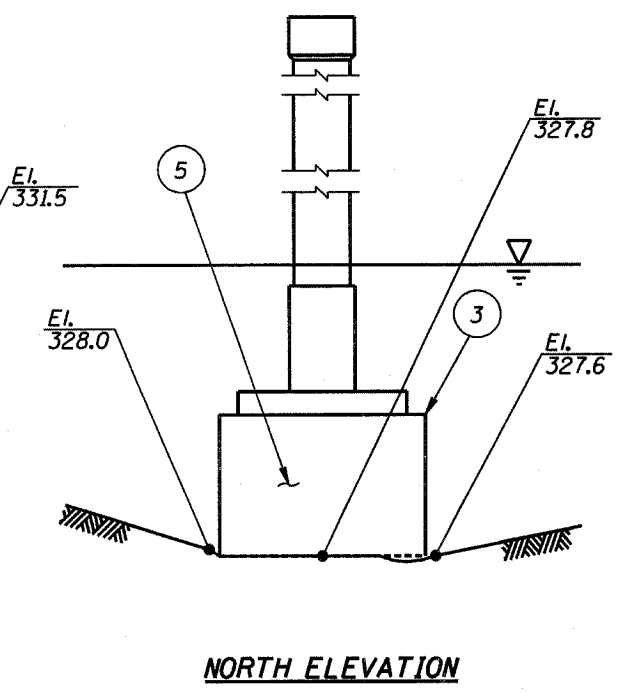
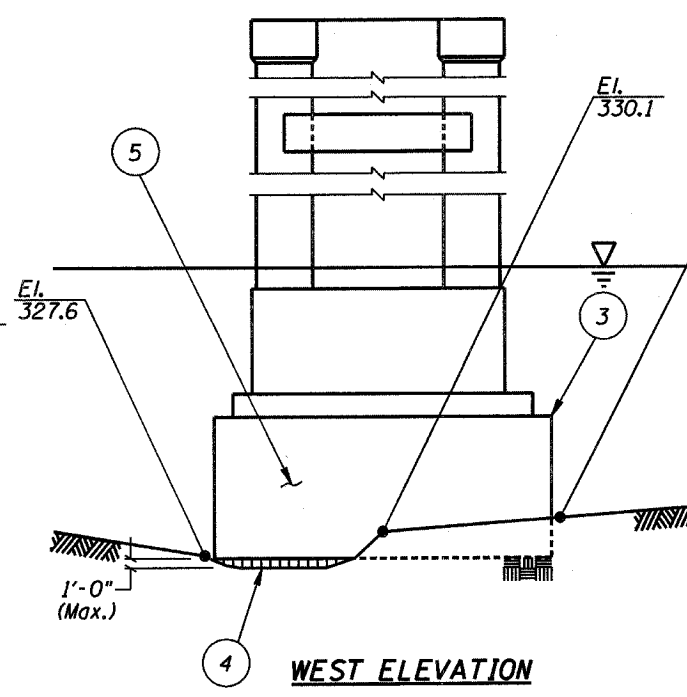
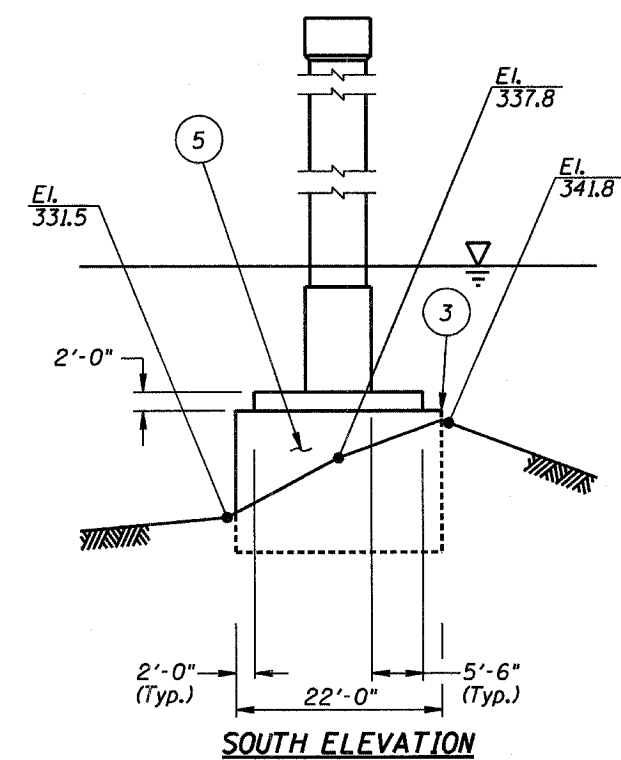
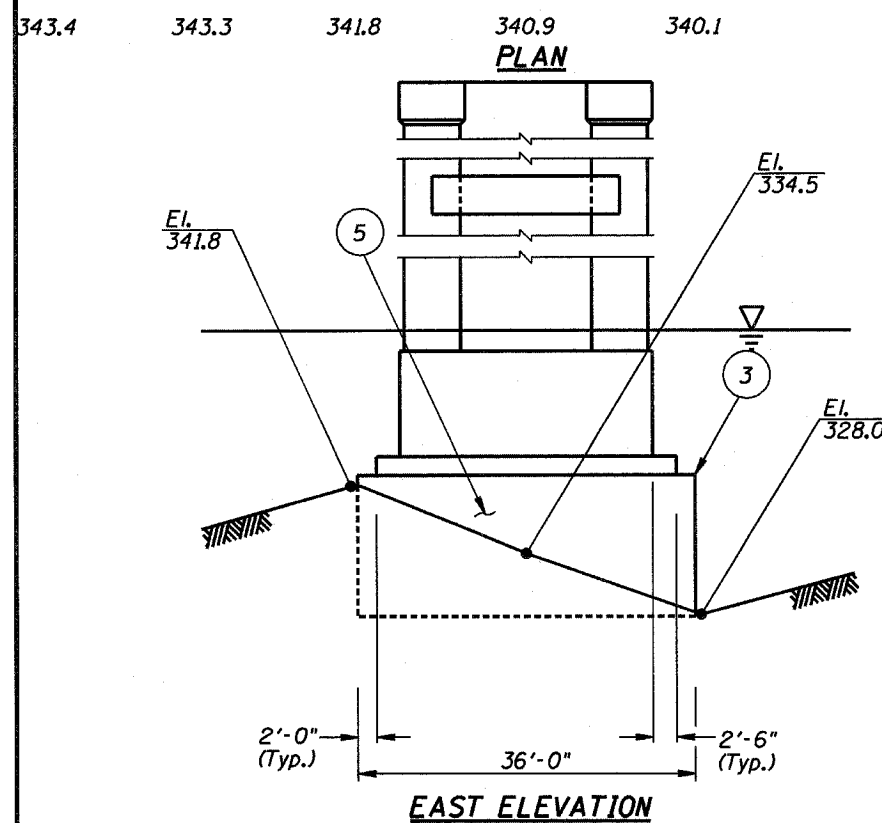
Inspection Notes:

- 1 The channel bottom material typically consisted of fragmented sandstone.
- 2 The channel bottom material typically consisted of loose sand, with up to 1.5 feet of probe rod penetration.
- 3 Seal exposure, with vertical limits as shown.
- 4 Seal undermined, creating a 1-foot vertical cavity with up to 4 feet of penetration. The bottom concrete surface of the undermined seal was typically rough and loose.
- 5 No damage below waterline.

FOR INFORMATION ONLY

General Notes:

1. At the time of the inspection, the waterline was located approximately 14.4 feet below the bottom of the pier beam at Eastbound Pier 3. This corresponds to a waterline elevation of 358.1 feet, based on the 1966 design plans.
2. Soundings indicate the channel bottom elevation at the time of inspection and are measured in feet.



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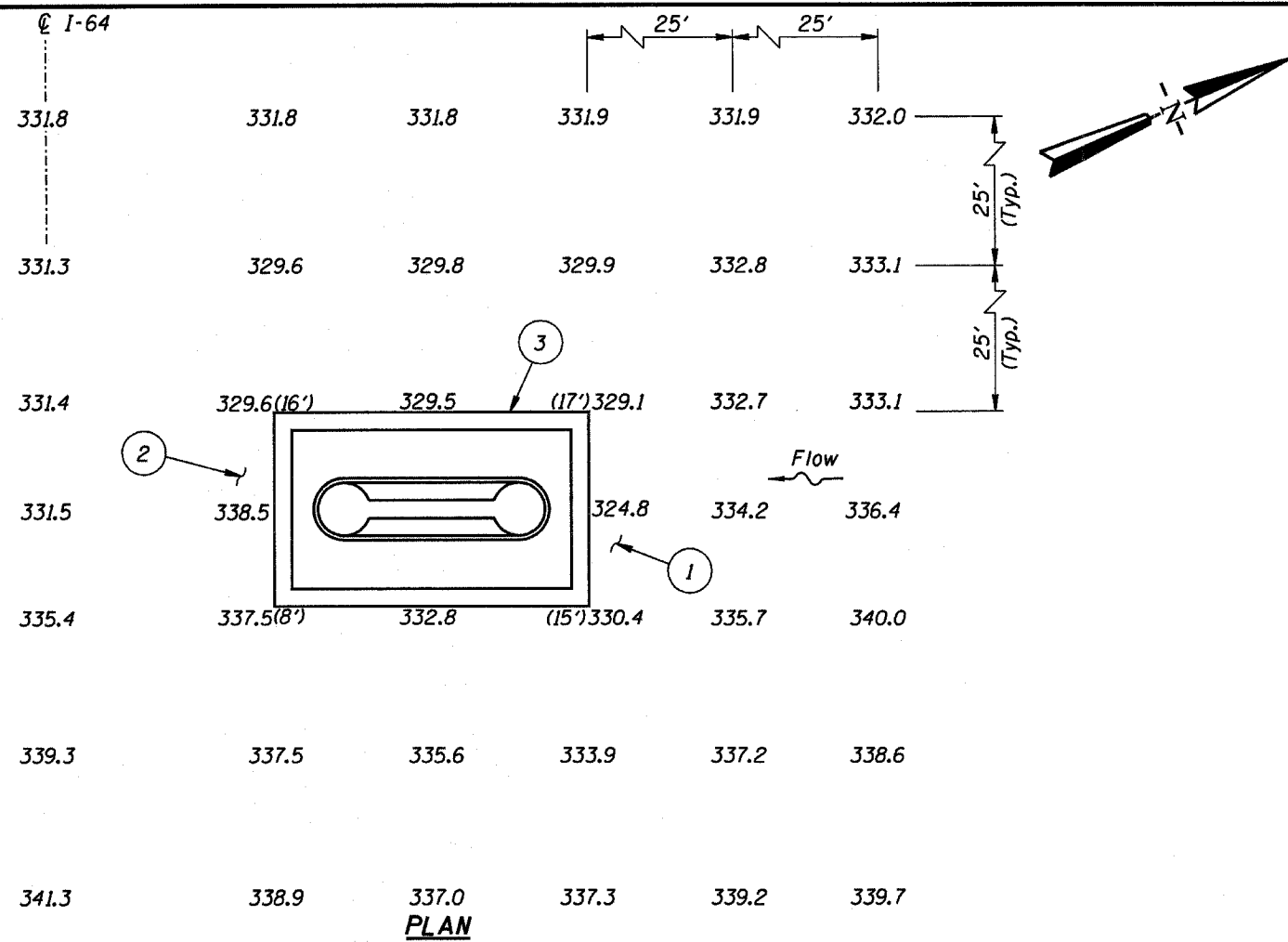
- 558.0 Channel Bottom Elevation
- 4 Indicates Inspection Note Number
- ▒ Pier Founded on Rock
- ▒ Channel Bottom Material
- (2') Vertical Exposure of Seal
- ▒ Indicates Undermined Area

**ILLINOIS
DEPARTMENT OF TRANSPORTATION**

I-64 OVER
WABASH RIVER
STRUCTURE NUMBER: 097-0003
EASTBOUND PIER 4

Drawn By: PRH/DR	COLLINS ENGINEERS <small>123 North Wacker Drive Suite 300 Chicago, IL 60606 (312) 704-9300 www.collinseng.com</small>	Date: JULY, 2005
Checked By: JEO		Scale: 1"=20'
Code: 419503-04		Figure No.: 16

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	97-3B-1-2	WHITE	15	10
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	



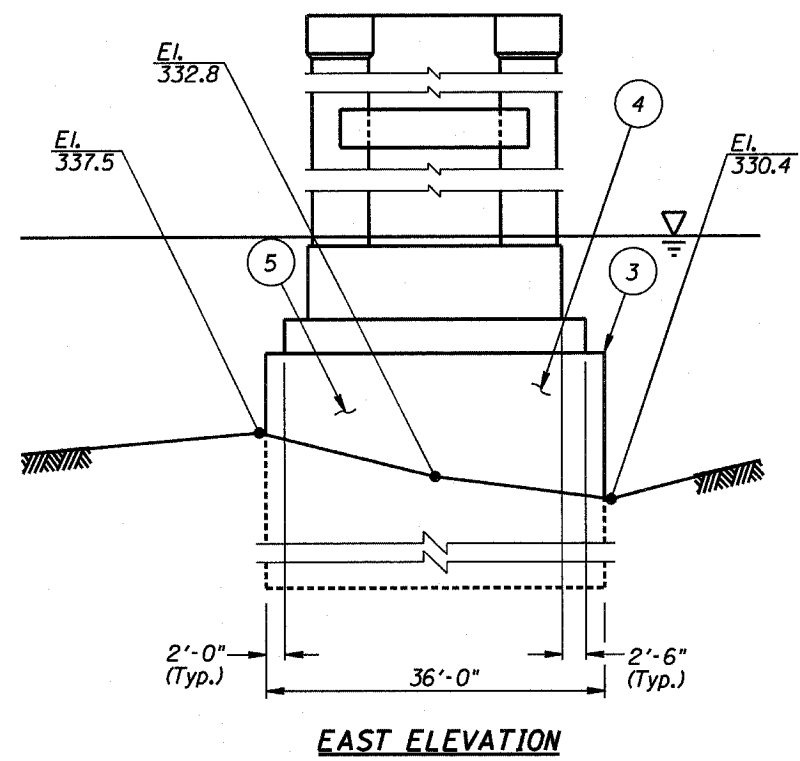
Inspection Notes:

- ① The channel bottom material typically consisted of loose sand and gravel, with up to 2 feet of probe rod penetration.
- ② The channel bottom material typically consisted of loose gravel, with up to 1 foot of probe rod penetration.
- ③ Seal exposure, with vertical limits as shown. No seal undermining detected.
- ④ Band of heavy concrete scale around pier perimeter, measuring 8 inches wide, located 4 feet below the top of the seal. The scale had a typical penetration of 6 inches and a maximum penetration of 1 foot.
- ⑤ No damage below waterline.

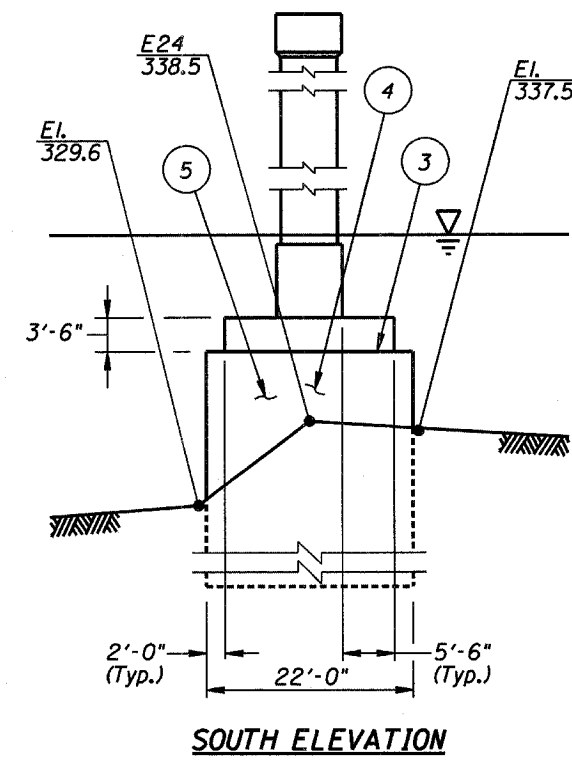
General Notes:

1. At the time of the inspection, the waterline was located approximately 14.4 feet below the bottom of the pier beam at Eastbound Pier 3. This corresponds to a waterline elevation of 358.1 feet, based on the 1966 design plans.
2. Soundings indicate the channel bottom elevation at the time of inspection and are measured in feet.

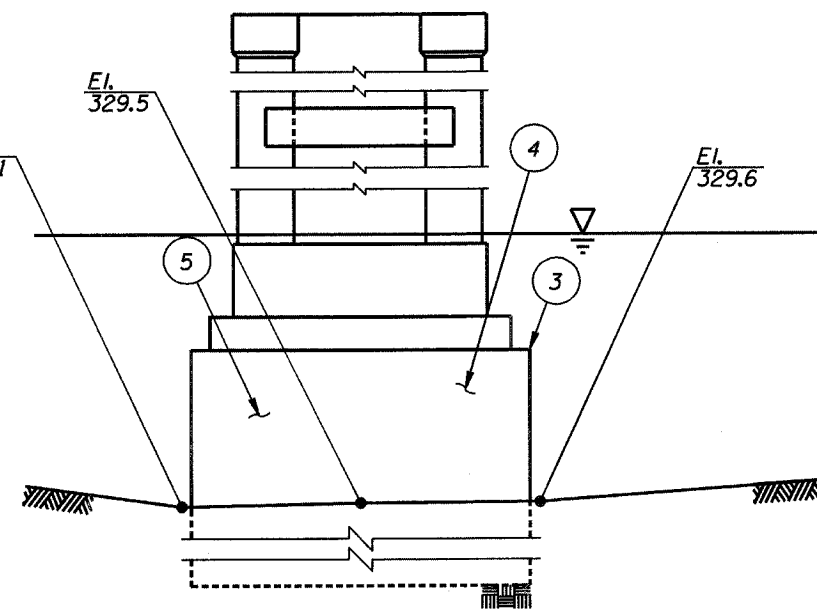
FOR INFORMATION ONLY



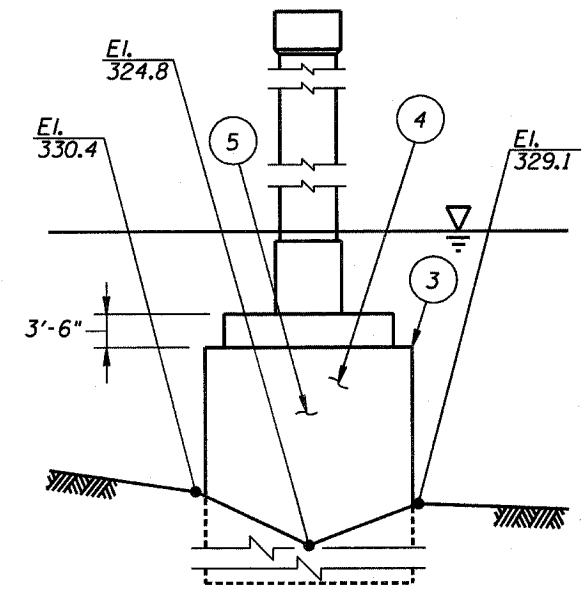
EAST ELEVATION



SOUTH ELEVATION



WEST ELEVATION



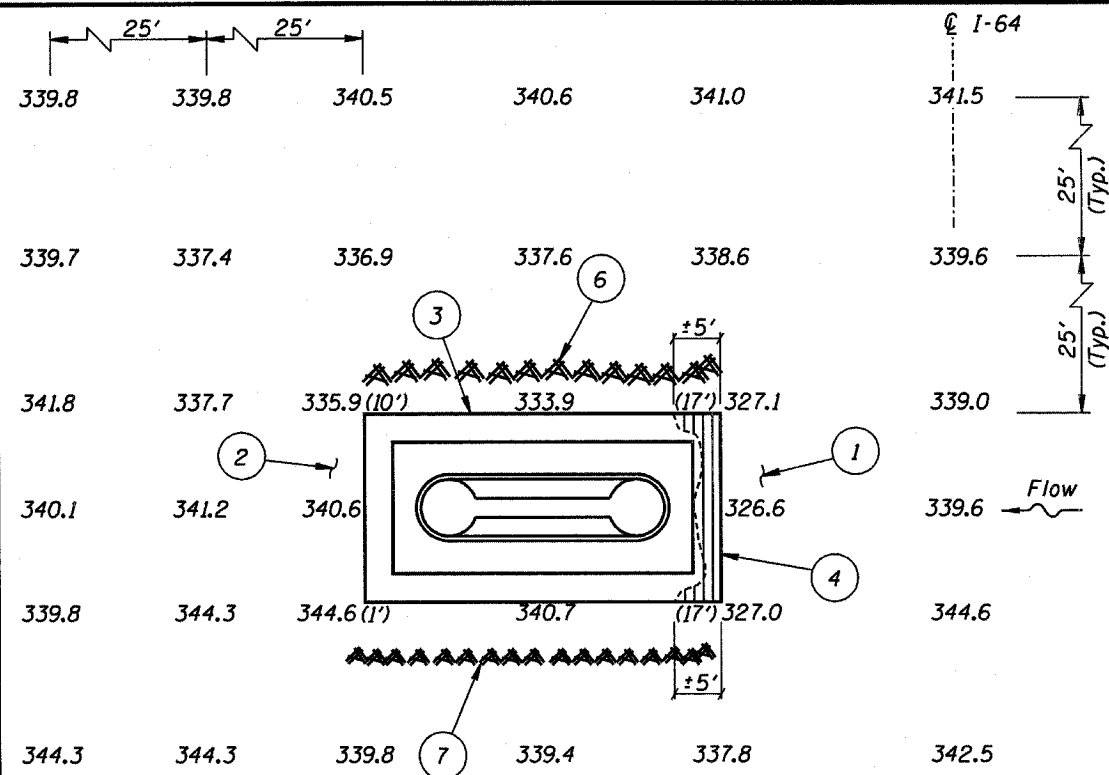
NORTH ELEVATION

LEGEND:

- 558.0 Channel Bottom Elevation
- ④ Indicates Inspection Note Number
- Pier Founded on Rock
- Channel Bottom Material
- (2') Vertical Exposure of Seal

ILLINOIS DEPARTMENT OF TRANSPORTATION		
I-64 OVER WABASH RIVER STRUCTURE NUMBER: 097-0004 WESTBOUND PIER 4		
Drawn By: PRH/DR	COLLINS ENGINEERS <small>123 North Wacker Drive Suite 300 Chicago, IL 60606 (312) 704-9300 www.collinsgr.com</small>	Date: JULY, 2005
Checked By: JEO		Scale: 1"=20'
Code: 419503-04		Figure No.: 17

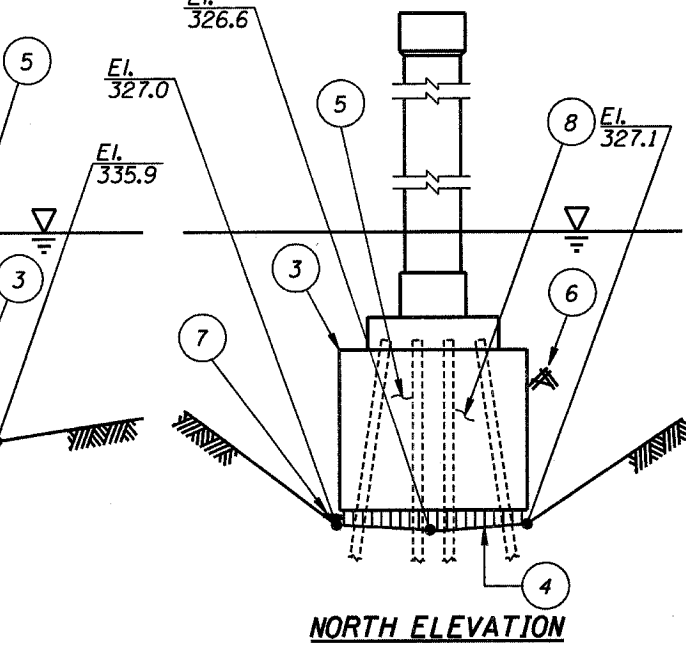
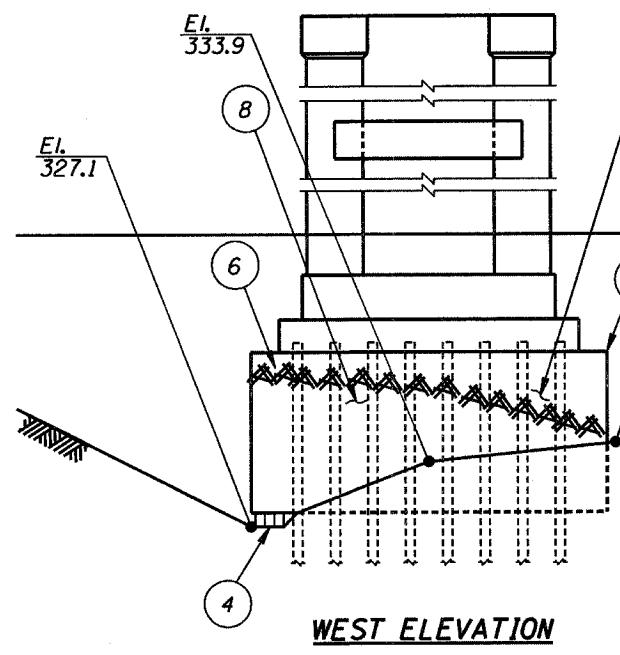
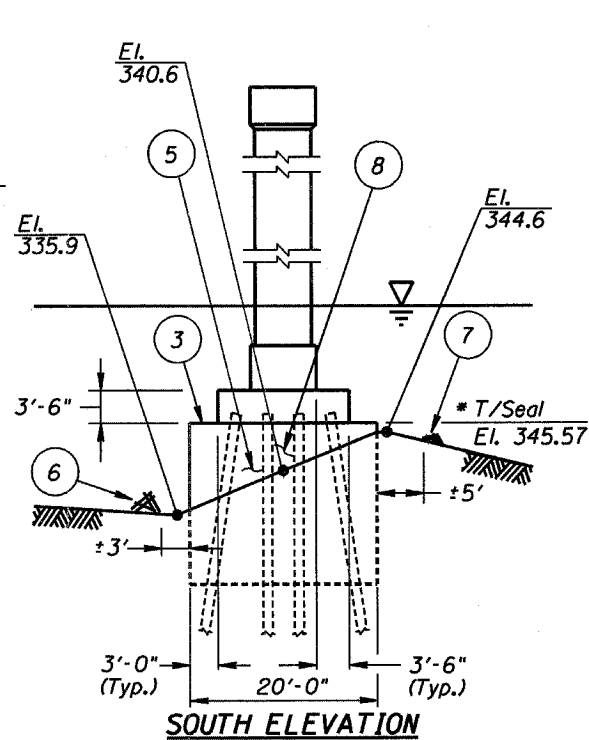
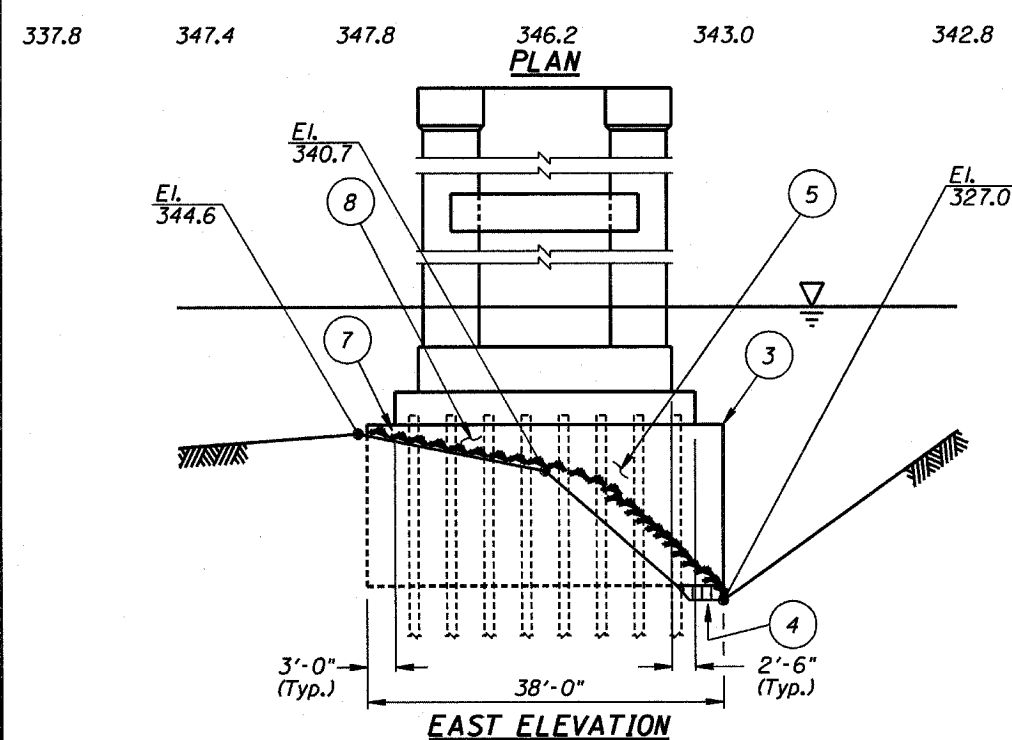
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	97-3B-1-2	WHITE	15	11
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



June 9 Inspection Notes:

- 1 The channel bottom material typically consisted of loose sand and gravel, with up to 1 foot of probe rod penetration.
- 2 The channel bottom material typically consisted of loose sand and gravel, with up to 2 feet of probe rod penetration.
- 3 Seal exposure, with vertical limits as shown.
- 4 Seal undermined, creating a 2-foot vertical cavity with up to 5 feet of penetration. The bottom concrete surface of the undermined seal was typically rough and loose.
- 5 Band of heavy concrete scale around pier perimeter, measuring 8 inches wide, located 5 feet below the top of the seal. The scale had a typical penetration of 6 inches and a maximum penetration of 1 foot.
- 6 Moderate accumulation of timber debris, consisting of a single log along the channel bottom, measuring up to 3 feet in diameter.
- 7 Moderate accumulation of timber debris, consisting of a single log along the channel bottom, measuring up to 1 foot in diameter.
- 8 No damage below waterline.

FOR INFORMATION ONLY



General Notes:

1. At the time of the inspection, the waterline was located approximately 14.4 feet below the bottom of the pier beam at Eastbound Pier 3. This corresponds to a waterline elevation of 358.1 feet, based on the 1966 design plans.
2. Soundings indicate the channel bottom elevation at the time of inspection and are measured in feet.

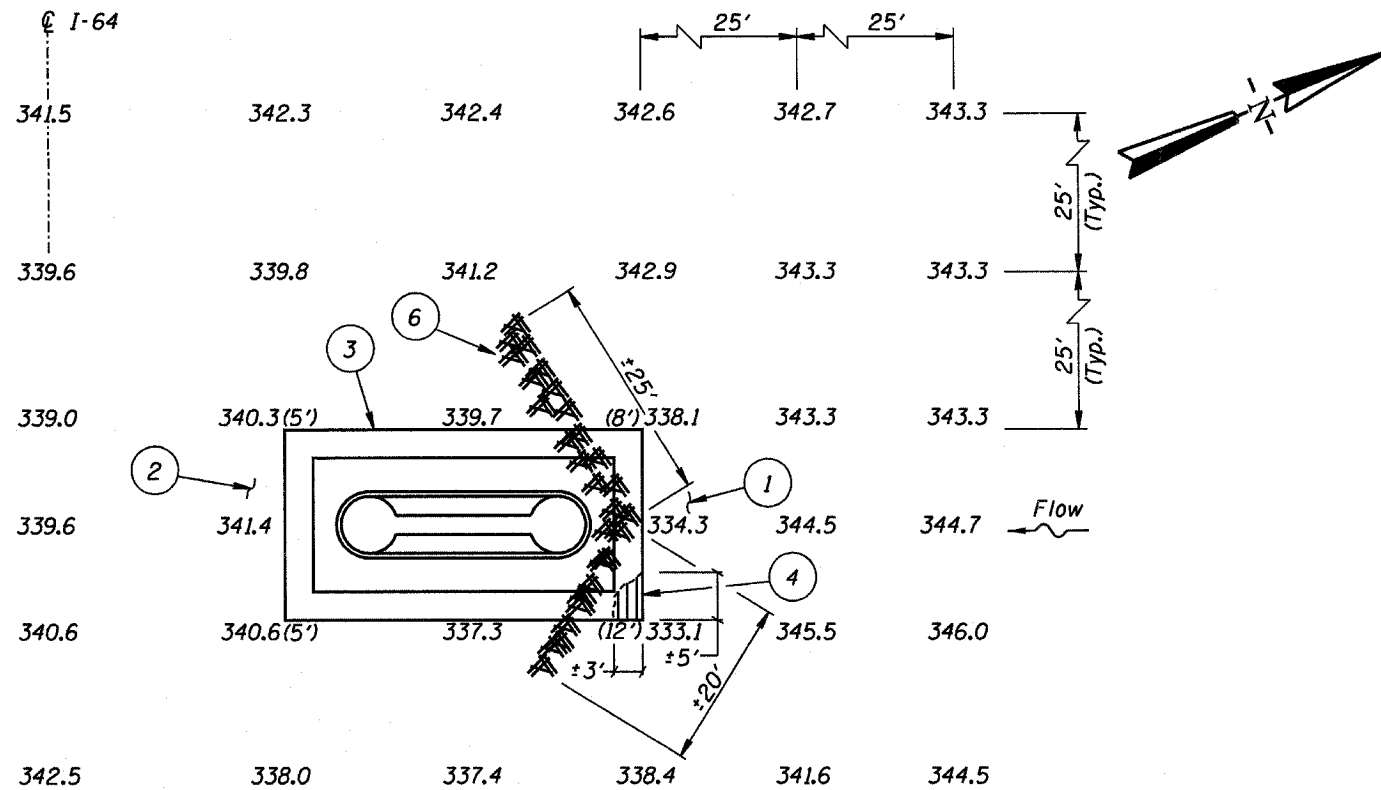
* Elevation per design drawing provided by IDOT.

LEGEND:

- 558.0 Channel Bottom Elevation
- 4 Indicates Inspection Note Number
- Channel Bottom Material
- (-2') Vertical Exposure of Seal
- Indicates Undermined Area

ILLINOIS DEPARTMENT OF TRANSPORTATION	
I-64 OVER WABASH RIVER STRUCTURE NUMBER: 097-0003 EASTBOUND PIER 5	
Drawn By: PRH/DR	Checked By: JEO
Code: 419503-04	Code: 419503-04
COLLINS ENGINEERS	
123 North Wacker Drive Suite 300 Chicago, IL 60606 (312) 704-9300 www.collinseng.com	
Date: JULY, 2005	Scale: 1"=20'
Figure No.: 18	

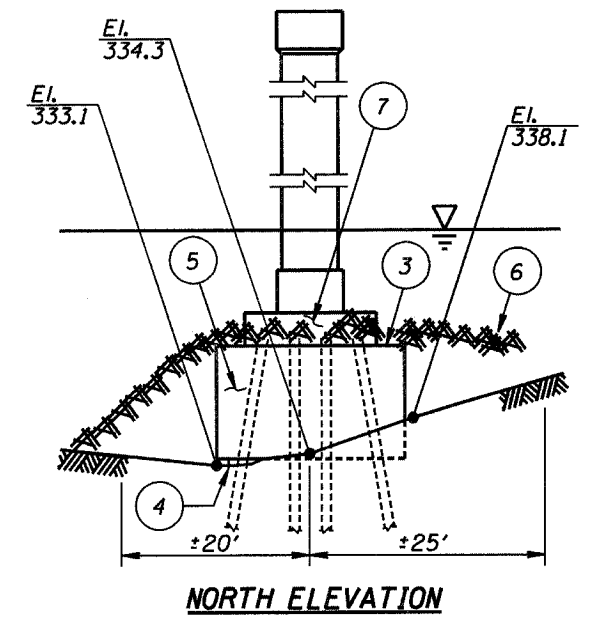
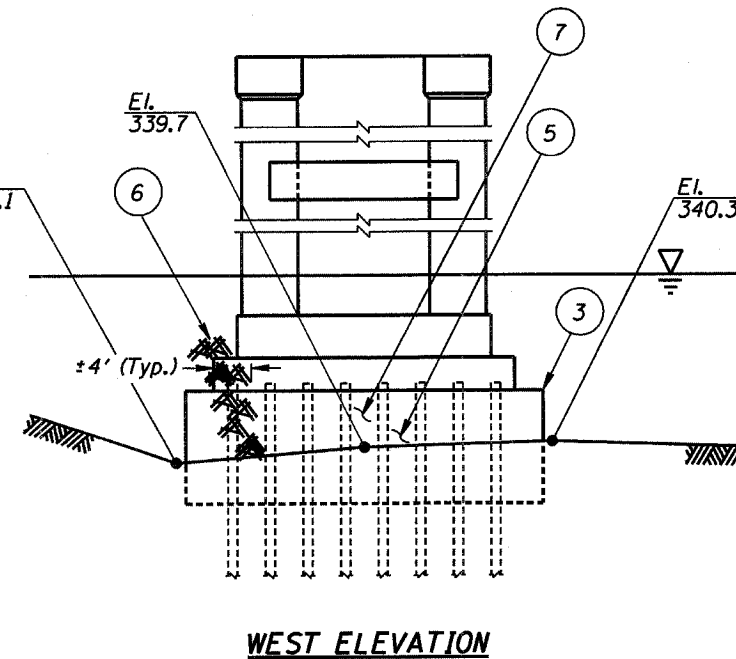
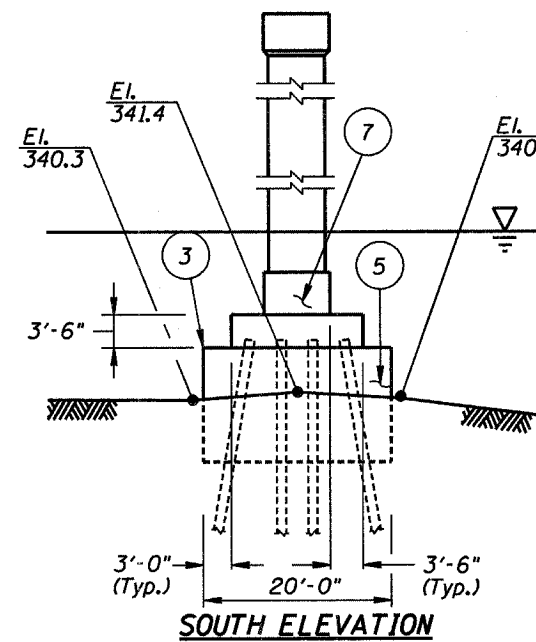
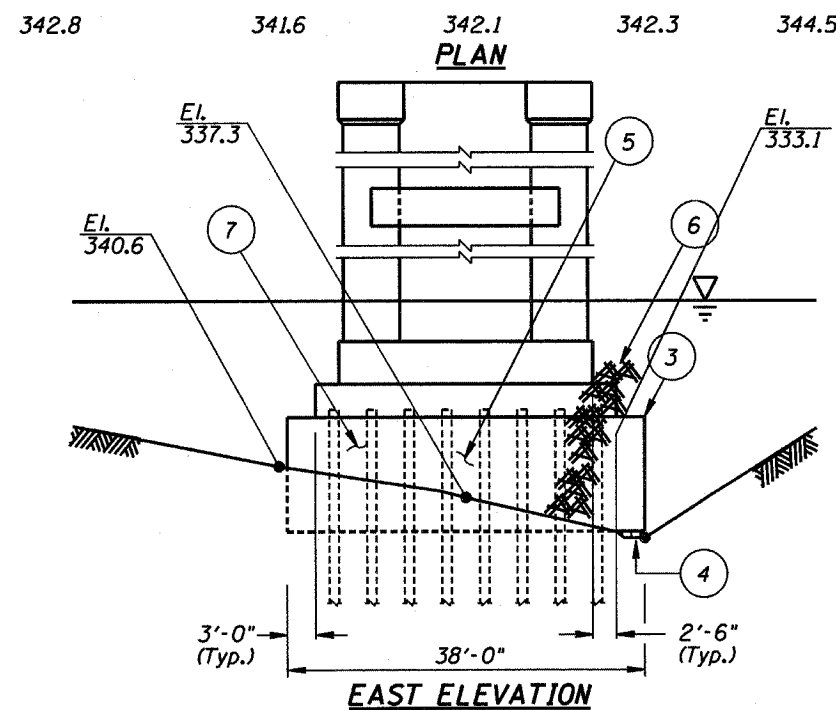
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	97-3B-I-2	WHITE	15	12
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



June 9 Inspection Notes:

- 1 The channel bottom material typically consisted of loose sand and gravel, with up to 1 foot of probe rod penetration.
- 2 The channel bottom material typically consisted of loose sand and gravel, with up to 2 feet of probe rod penetration.
- 3 Seal exposure, with vertical limits as shown.
- 4 Seal undermined, creating a 1-foot vertical cavity with up to 4 feet of penetration. The bottom concrete surface of the undermined seal was typically rough and loose.
- 5 Band of heavy concrete scale around pier perimeter, measuring 8 inches wide, located 5 feet below the top of the seal. The scale had a typical penetration of 6 inches and a maximum penetration of 1 foot.
- 6 Moderate accumulation of timber debris, consisting of a tree measuring up to 3 feet in diameter with associated branches.
- 7 No damage below waterline.

FOR INFORMATION ONLY



General Notes:

1. At the time of the inspection, the waterline was located approximately 14.4 feet below the bottom of the pier beam at Eastbound Pier 3. This corresponds to a waterline elevation of 358.1 feet, based on the 1966 design plans.
2. Soundings indicate the channel bottom elevation at the time of inspection and are measured in feet.

LEGEND:

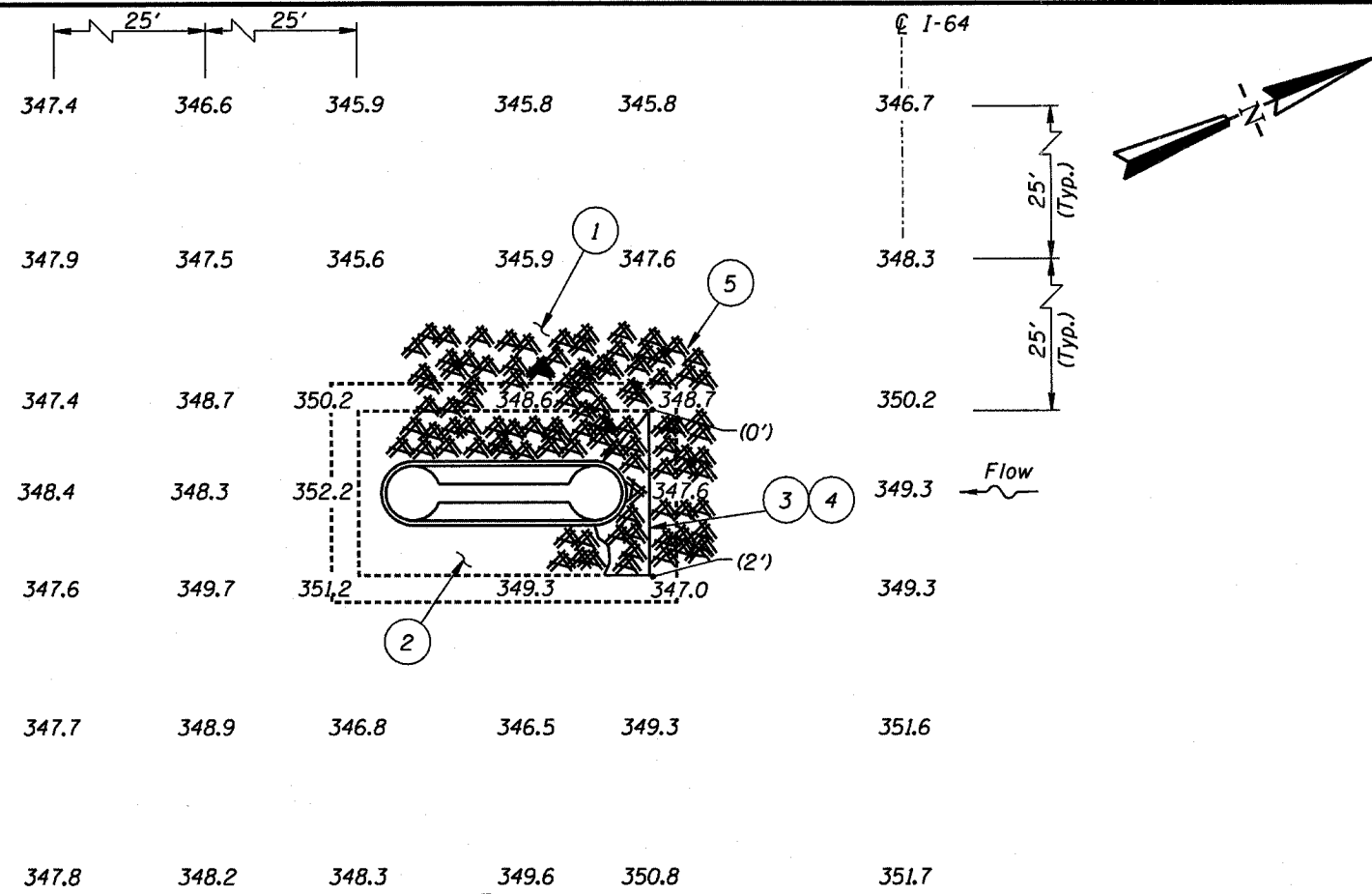
- 558.0 Channel Bottom Elevation
- 4 Indicates Inspection Note Number
- Channel Bottom Material
- (-2') Vertical Exposure of Seal
- Indicates Undermined Area

ILLINOIS DEPARTMENT OF TRANSPORTATION

I-64 OVER WABASH RIVER
STRUCTURE NUMBER: 097-0004
WESTBOUND PIER 5

Drawn By: PRH/DR	COLLINS ENGINEERS 123 North Wacker Drive Suite 300 Chicago, IL 60606 (312) 704-9300 www.collinsengr.com ILLINOIS PROFESSIONAL DESIGN FIRM LICENSE NO. 184-00993	Date: JULY, 2005
Checked By: JEO		Scale: 1"=20'
Code: 419503-04		Figure No.: 19

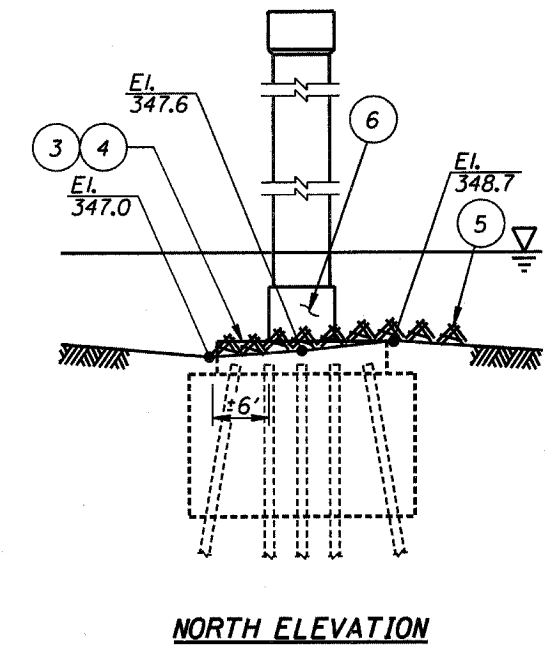
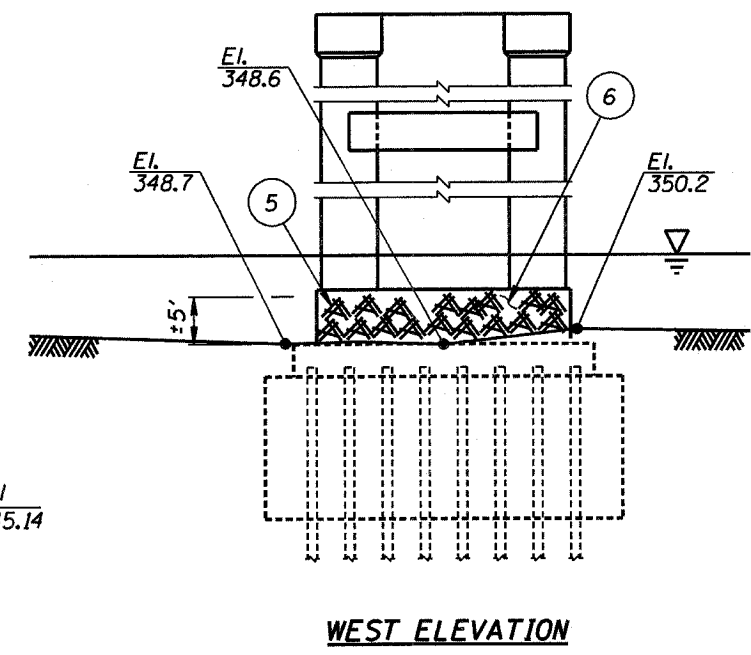
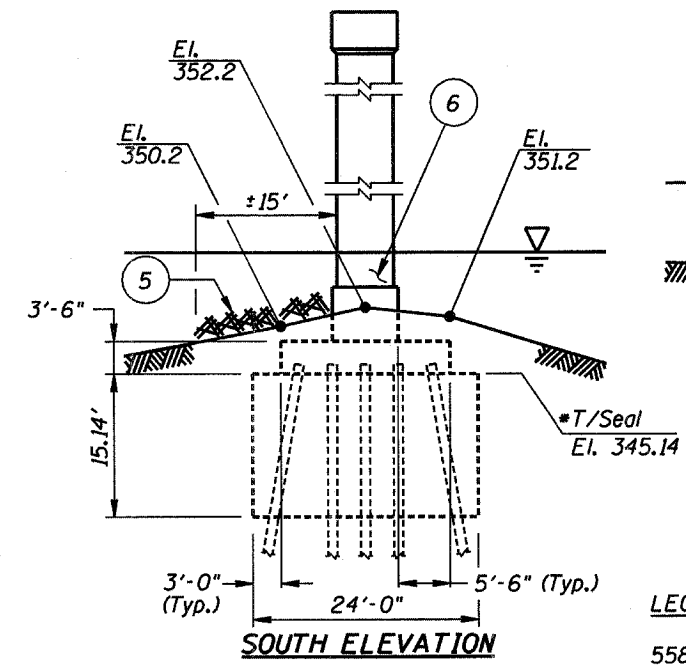
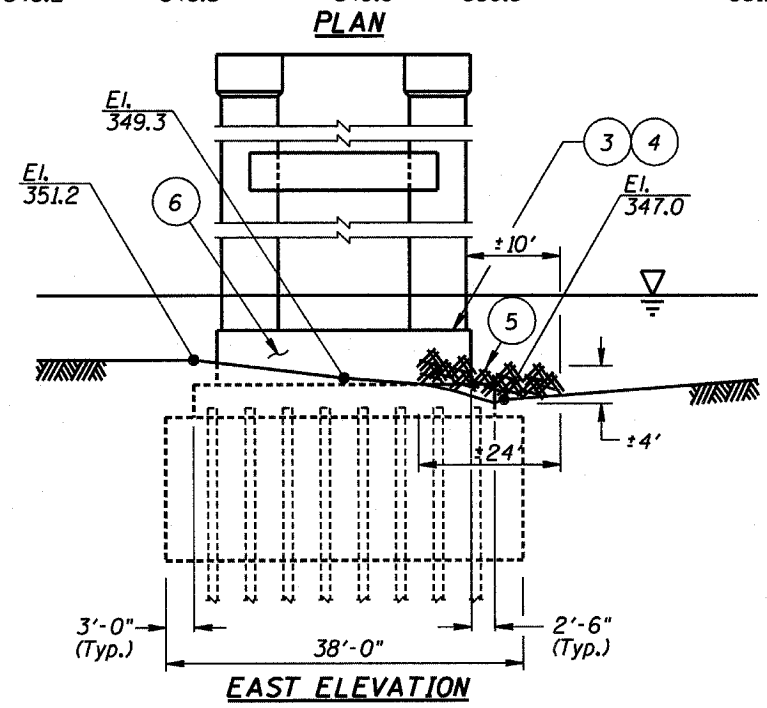
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	97-3B-I-2	WHITE	15	13
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



June 9 Inspection Notes:

- ① The channel bottom material typically consisted of loose sandy silt, with up to 1 foot of probe rod penetration.
- ② The channel bottom material typically consisted of soft silt, with up to 3 feet of probe rod penetration.
- ③ Footing exposure, with vertical limits as shown. Due to heavy timber debris accumulations, the footing could not be detected along the west fascia. In the area where the footing was exposed, no undermining was detected.
- ④ Timber formwork covered approximately 90 percent of the vertical concrete footing surface.
- ⑤ Heavy accumulation of timber debris, consisting of trees measuring up to 3 feet in diameter with associated branches.
- ⑥ No damage below waterline.

FOR INFORMATION ONLY



General Notes:

1. At the time of the inspection, the waterline was located approximately 14.4 feet below the bottom of the pier beam at Eastbound Pier 3. This corresponds to a waterline elevation of 358.1 feet, based on the 1966 design plans.
2. Soundings indicate the channel bottom elevation at the time of inspection and are measured in feet.

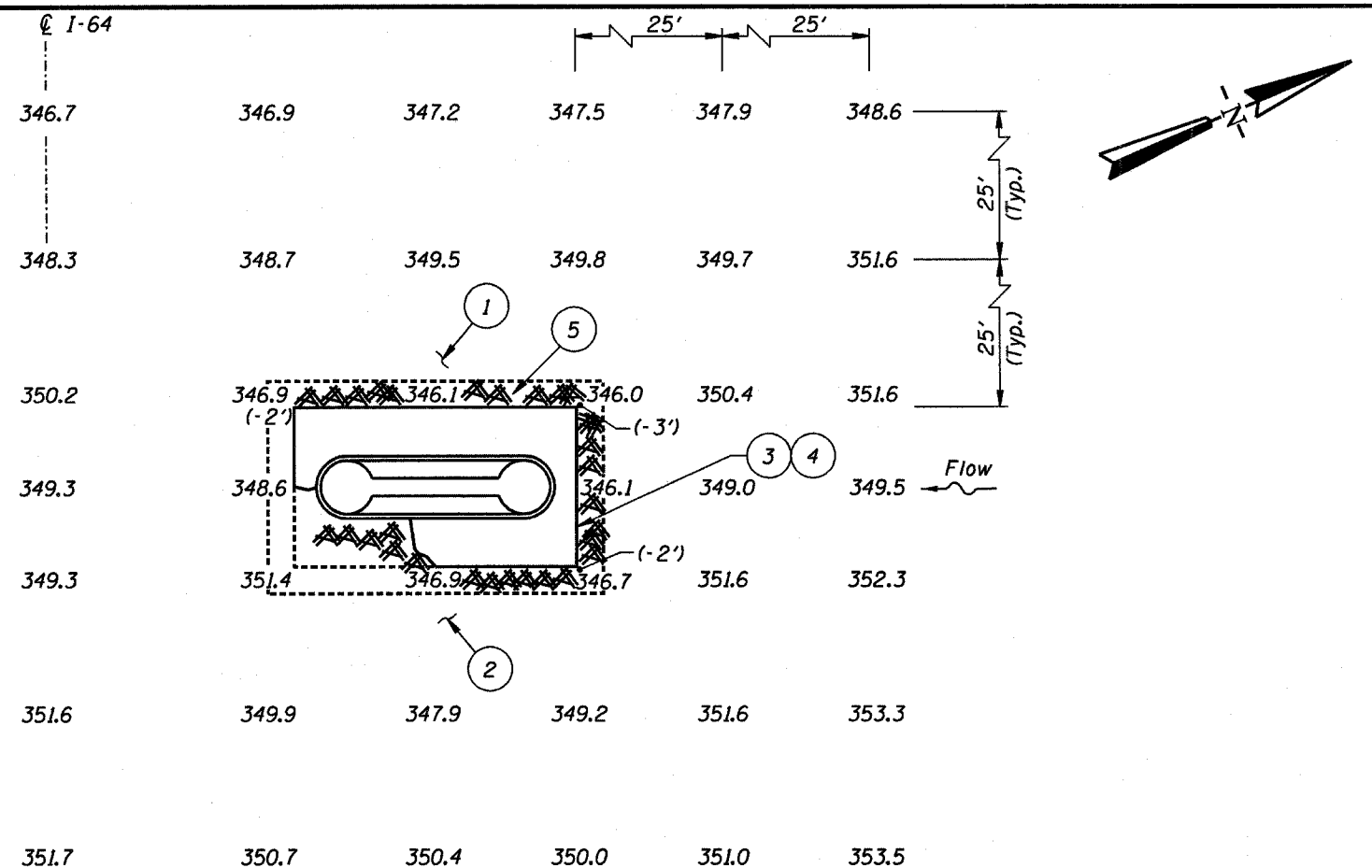
* Elevation per design drawing provided by IDOT.

LEGEND:

- 558.0 Channel Bottom Elevation
- ④ Indicates Inspection Note Number
- Pier Founded on Rock
- Channel Bottom Material
- (2') Vertical Exposure of Footing

ILLINOIS DEPARTMENT OF TRANSPORTATION		
I-64 OVER WABASH RIVER STRUCTURE NUMBER: 097-0003 EASTBOUND PIER 6		
Drawn By: PRH/DR	COLLINS ENGINEERS <small>123 North Wacker Drive Suite 300 Chicago, IL 60606 (312) 764-9300 www.collinsengr.com ILLINOIS PROFESSIONAL DESIGN FIRM LICENSE NO. 184-000993</small>	Date: JULY, 2005
Checked By: JEO		Scale: 1"=20'
Code: 419503-04		Figure No.: 20

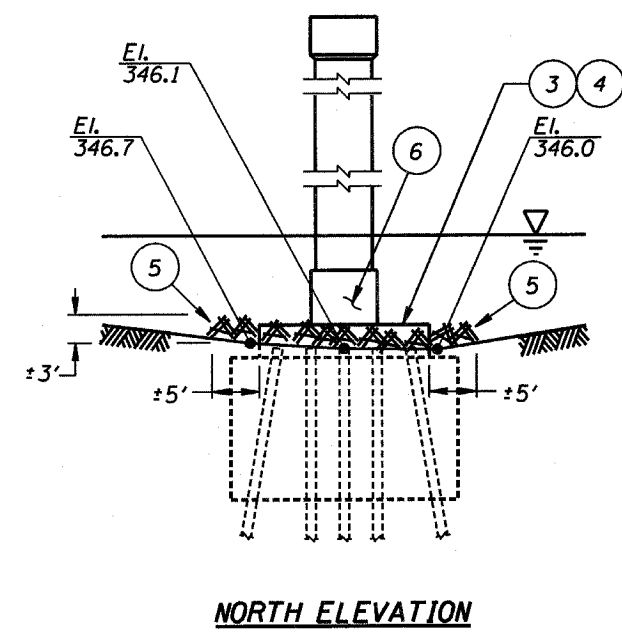
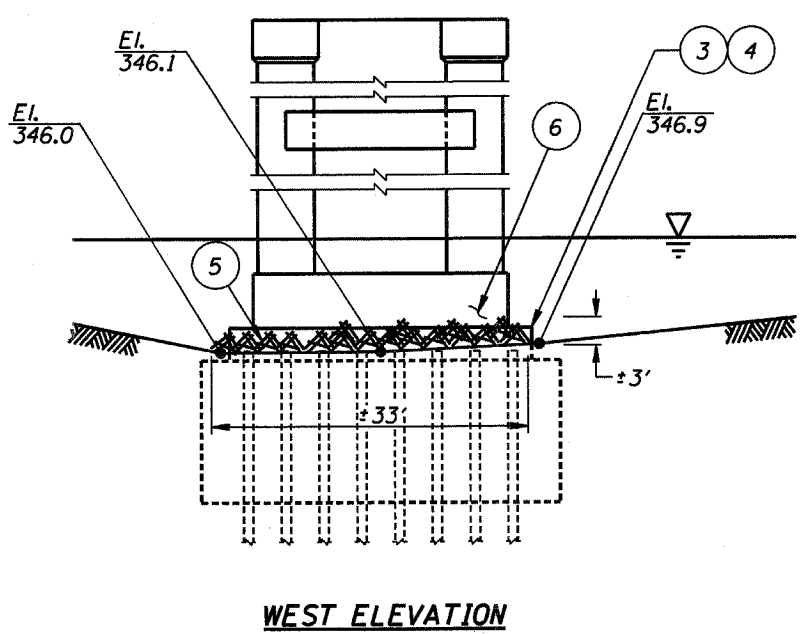
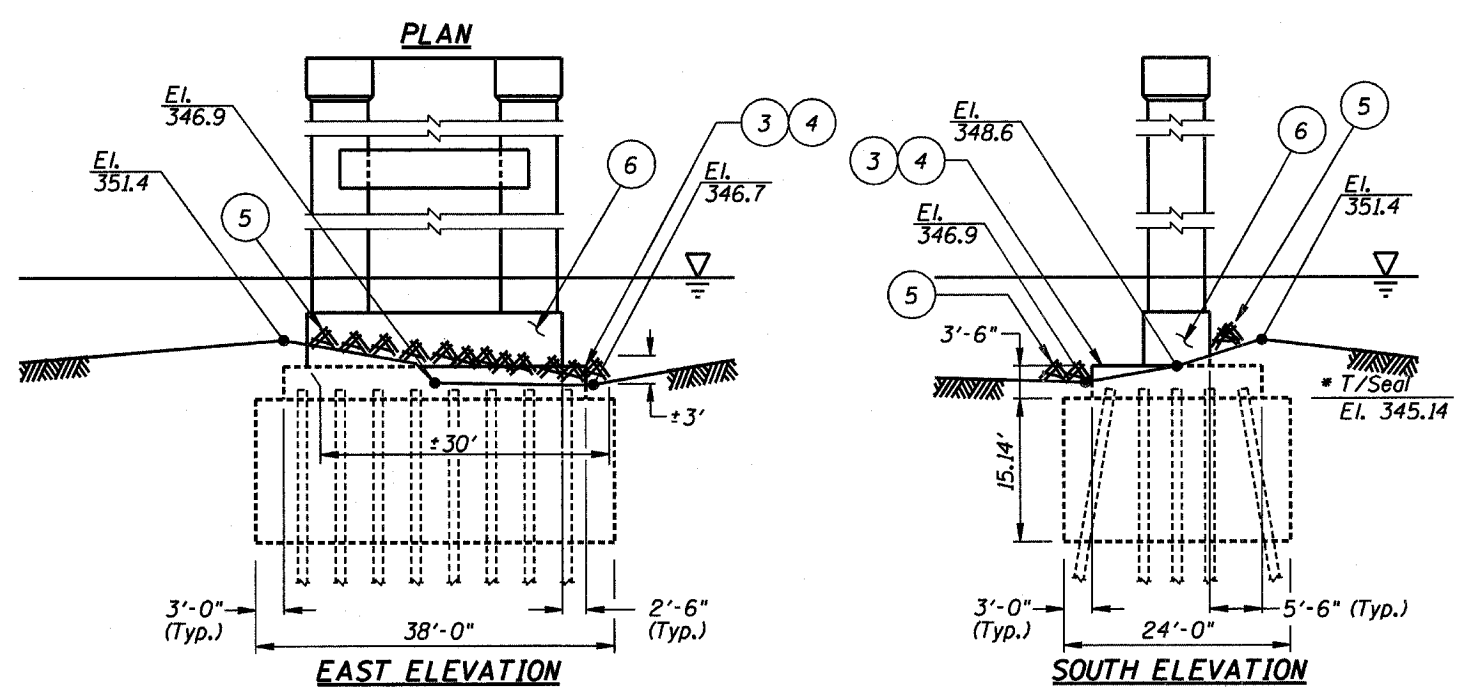
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	97-3B-I-2	WHITE	15	14
STA. TO STA.				
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



June 9 Inspection Notes:

- ① The channel bottom material typically consisted of loose sandy silt, with up to 1.5 feet of probe rod penetration.
- ② The channel bottom material typically consisted of soft silt, with up to 3 feet of probe rod penetration.
- ③ Footing exposure, with vertical limits as shown. In the area where the footing was exposed, no undermining was detected.
- ④ Timber formwork covered approximately 90 percent of the vertical concrete footing surface.
- ⑤ Heavy accumulation of timber debris, consisting of trees measuring up to 2 feet in diameter with associated branches.
- ⑥ No damage below waterline.

FOR INFORMATION ONLY



- LEGEND:**
- 558.0 Channel Bottom Elevation
 - ④ Indicates Inspection Note Number
 - Pier Founded on Rock
 - Channel Bottom Material
 - (2') Vertical Exposure of Footing

General Notes:

1. At the time of the inspection, the waterline was located approximately 14.4 feet below the bottom of the pier beam at Eastbound Pier 3. This corresponds to a waterline elevation of 358.1 feet, based on the 1966 design plans.
2. Soundings indicate the channel bottom elevation at the time of inspection and are measured in feet.

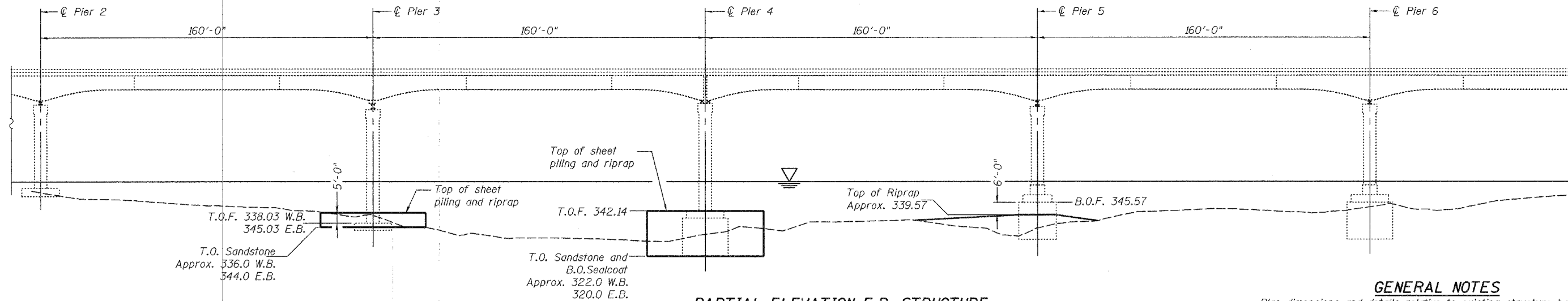
* Elevation per design drawing provided by IDOT.

ILLINOIS DEPARTMENT OF TRANSPORTATION		
I-64 OVER WABASH RIVER STRUCTURE NUMBER: 097-0004 WESTBOUND PIER 6		
Drawn By: PRH/DR	COLLINS ENGINEERS <small>123 North Wacker Drive Suite 300 Chicago, IL 60606 (312) 704-9300 www.collinsengr.com ILLINOIS PROFESSIONAL DESIGN FIRM LICENSE NO. 184-000993</small>	Date: JULY, 2005
Checked By: JEO		Scale: 1"=20'
Code: 419503-04		Figure No.: 21

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 1 2 SHEETS
I-64		White	15	15	
FED. AID DIST. NO. 7		ILLINOIS		FED. AID PROJECT-	

Contract Number: 7442



GENERAL NOTES

Plan dimensions and details relative to existing structure have been taken from existing plans and are subject to nominal construction variations. It shall be the Contractor's responsibility to verify such dimensions and details in the field and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in the scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

At no time will de-watering be allowed inside the sheet piling. Class A5 riprap shall be placed inside the sheet piling to the top of the sheeting at Pier 3 E.B. and W.B. The riprap shall be placed after removal of the riverbed material inside the sheeting down to sandstone. Removal of the material shall be done by pumping or other means that will not damage the sandstone. Cost of removal of riverbed material to be paid for as Cofferdam Excavation.

Class A5 riprap shall be placed inside the sheet piling to the top of the sheeting at Pier 4 E.B. and W.B. Excavation of riverbed material is not required prior to placement of the riprap inside the sheeting. Class A4 riprap may be used in lieu of the A5 riprap except for the top 10'-0".

The Contractor shall exercise care when placing riprap adjacent to the piers. Any damage done to the structure shall be repaired at the Contractor's expense.

Debris shall be removed from around the piers prior to placement of the sheeting and riprap.

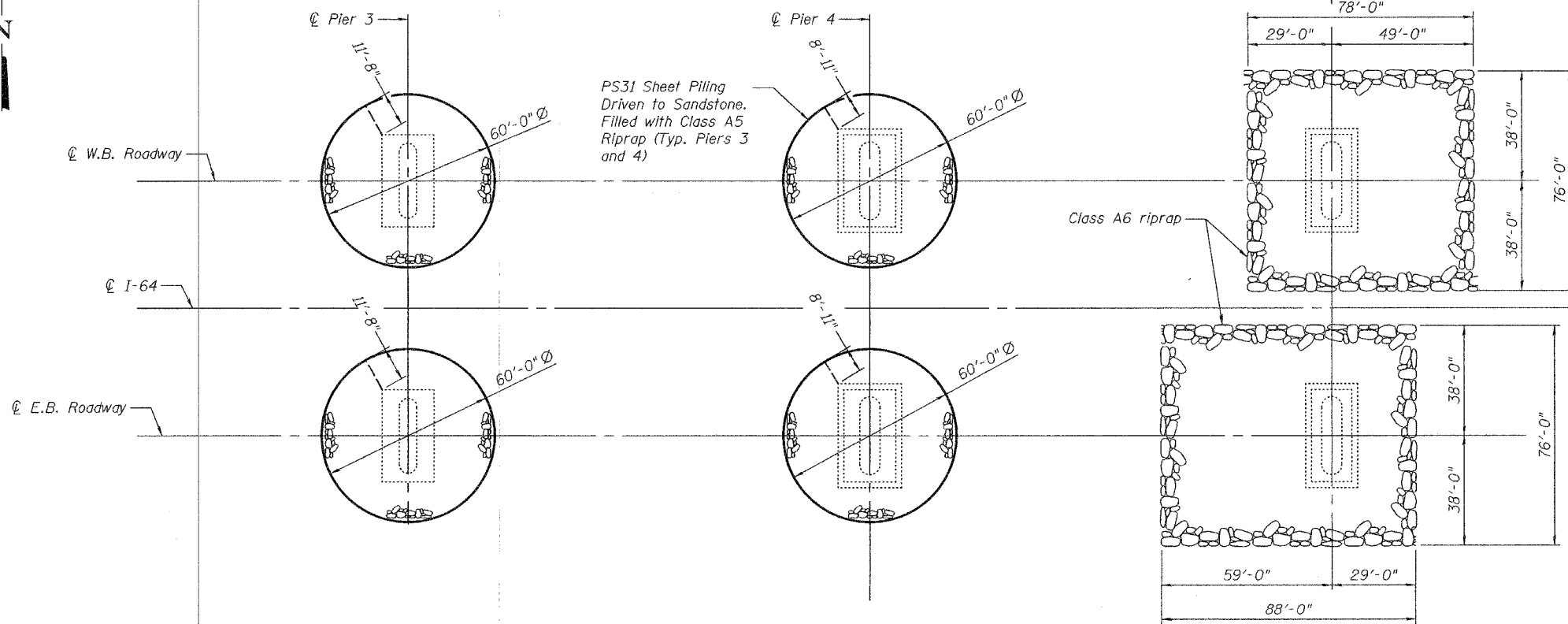
Priority of Construction:

- Pier 3 E.B.
- Pier 4 E.B.
- Pier 3 W.B.
- Pier 4 W.B.
- Pier 5 E.B. and W.B.

PIER CAP ELEVATION IS 392.53 AT PIER 3, 393.64 AT PIER 4, AND 391.57 AT PIER 5.

TOTAL BILL OF MATERIAL

ITEM	UNIT	QUANTITY
* Permanent Steel Sheet Piling	Sq. Ft.	10,430
Stone Dumped Riprap, Class A5	Ton	5,050
Stone Dumped Riprap, Class A6	Ton	2,400
Cofferdam Excavation	Cu. Yd.	360
* Debris Removal	L.S.	1
* Debris Removal (Special)	Cal. Day	20



DESIGNED	<i>Dr. Carl Pappas</i>
CHECKED	<i>[Signature]</i>
DRAWN	<i>[Signature]</i>
CHECKED	<i>DCP / [Signature]</i>

December 7, 2005
 EXAMINED *John A. Morris, P.E.*
 ENGINEER OF STRUCTURAL SERVICES
 PASSED *Ralph E. Adams*
 ENGINEER OF BRIDGES AND STRUCTURES



Expires: November 30, 2006

PARTIAL PLAN

* See special provisions.

PARTIAL PLAN AND ELEVATION
I-64 / WABASH RIVER
WHITE COUNTY
SN 097-0003 & 0004

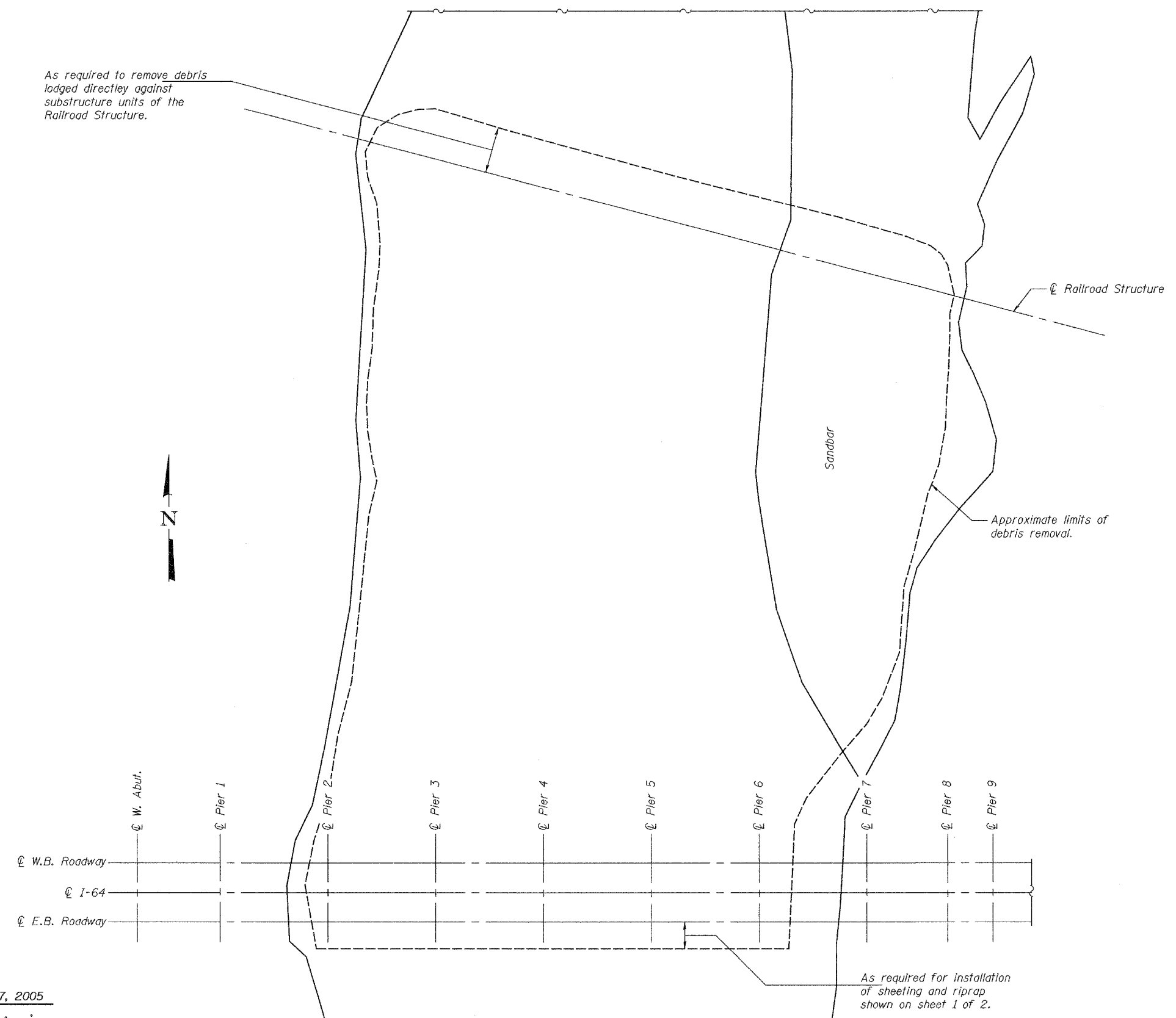
Rev.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-64		White	15	15A
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-		

SHEET NO. 2
2 SHEETS

Contract Number: 74442



DESIGNED	D.C.P.
CHECKED	M.A.C.
DRAWN	Drew Christopher
CHECKED	D.C.P. M.A.C.

December 7, 2005
 EXAMINED *John A. Morris*
 ENGINEER OF STRUCTURAL SERVICES
 PASSED *Ralph E. Anderson*
 ENGINEER OF BRIDGES AND STRUCTURES

DEBRIS REMOVAL OVERALL PLAN
I-64 / WABASH RIVER
WHITE COUNTY
SN 097-0003 & 0004