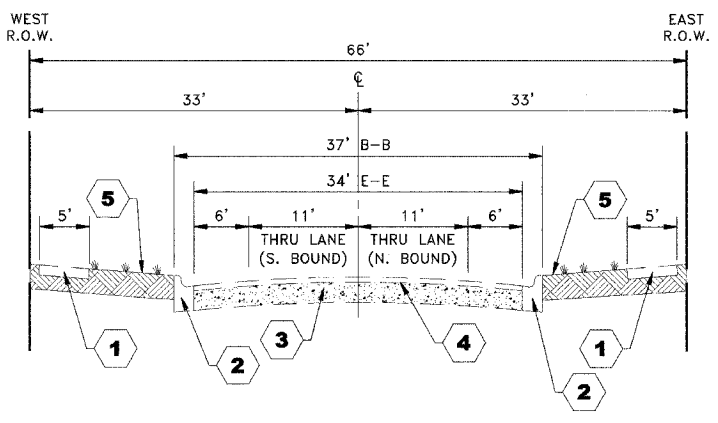
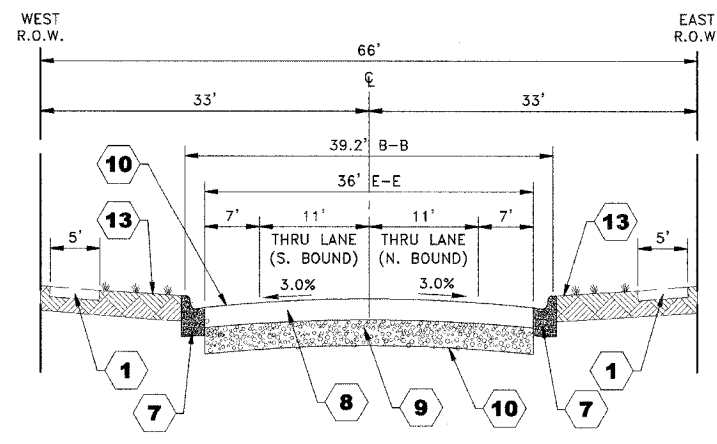


**TYPICAL CROSS SECTION LEGEND**

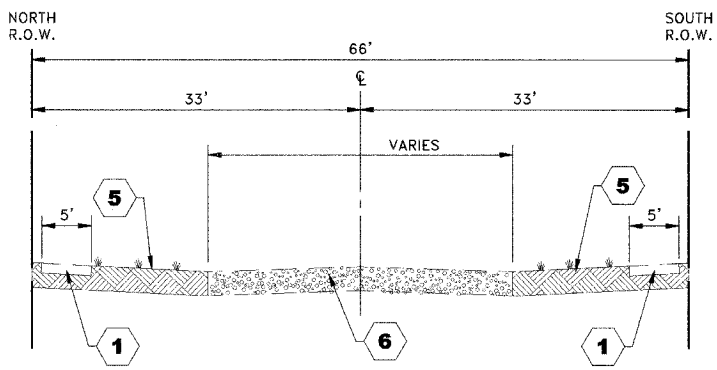
- | EXISTING |  | PROPOSED |  |
|----------|--|----------|--|
| 1        | PORTLAND CEMENT CONCRETE SIDEWALK, 5"                      | 7        | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12        |
| 2        | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12          | 8        | BITUMINOUS CONCRETE PAVEMENT (FULL-DEPTH), SUPERPAVE 9"  |
| 3        | PORTLAND CEMENT CONCRETE BASE COURSE, 5" - 9 1/2"          | 9        | AGGREGATE BASE COURSE, TYPE B, 12"                       |
| 4        | BITUMINOUS CONCRETE BINDER AND SURFACE COURSE, 3" - 7 1/2" | 10       | GEOTECHNICAL FABRIC FOR GROUND STABILIZATION             |
| 5        | SODDED PARKWAY   | 11       | BITUMINOUS CONCRETE PAVEMENT (FULL-DEPTH), SUPERPAVE, 6" |
| 6        | AGGREGATE PAVEMENT   | 12       | AGGREGATE BASE COURSE, TYPE B, 6"                        |
|          |  | 13       | SODDED PARKWAY WITH 4" TOPSOIL                           |



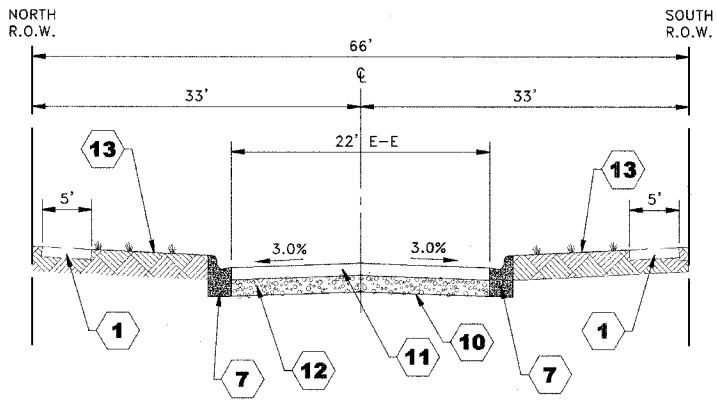
**EXISTING TYPICAL CROSS SECTION**  
PRAIRIE AVENUE  
(WASHINGTON AVENUE TO 31ST STREET)



**PROPOSED TYPICAL CROSS SECTION**  
PRAIRIE AVENUE  
(WASHINGTON AVENUE TO 31ST STREET)



**EXISTING TYPICAL CROSS SECTION**  
MONROE AVENUE, JACKSON AVENUE, GARFIELD AVENUE  
(EAST OF PRAIRIE AVENUE)



**PROPOSED TYPICAL CROSS SECTION**  
MONROE AVENUE, JACKSON AVENUE, GARFIELD AVENUE  
(EAST OF PRAIRIE AVENUE)

BITUMINOUS MIXTURE REQUIREMENTS			
ITEM	AC TYPE	VOIDS	RAP%
BITUMINOUS CONCRETE PAVEMENT (FULL-DEPTH), SUPERPAVE, 9" BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N50 (7")	PG 58-22	4% @50 GYR	25
BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX D, N50 (2")	PG 64-22	4% @50 GYR	15
BITUMINOUS CONCRETE PAVEMENT (FULL-DEPTH), SUPERPAVE, 6" BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N50 (4")	PG 58-22	4% @50 GYR	25
BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX D, N50 (2")	PG 64-22	4% @50 GYR	15
BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX D, N50	PG 64-22	4% @50 GYR	15
BITUMINOUS MIXTURE FOR PATCHING POT HOLES (HOT MIX)	PG 64-22	4% @50 GYR	15

\* THE UNIT WEIGHT USED TO CALCULATE ALL BITUMINOUS SURFACE MIXTURES IS 112 LBS/SQYD/IN.

PAVEMENT CORE SUMMARY						
BORE	STATION	OFFSET	LEFT/RIGHT	BITUMINOUS	CONC. BASE	TOTAL
PC-13	23+99	14'	RIGHT	3"	9 1/2"	12 1/2"
PC-14	26+16	5.5'	RIGHT	6"	6"	12"
PC-15	27+85	5'	LEFT	6"	7"	13"
PC-16	29+50	12.5'	LEFT	5"	NONE	5"
PC-17	31+21	13'	RIGHT	4"	6"	10"
PC-18	32+81	9'	RIGHT	4 1/2"	NONE	4 1/2"
PC-19	34+50	10'	LEFT	8"	8"	16"
PC-20	36+16	14'	LEFT	5 1/2"	NONE	5 1/2"
PC-21	37+83	13'	RIGHT	5 1/2"	NONE	5 1/2"
PC-22	39+51	3.5'	RIGHT	3 1/2"	5 1/2"	8 3/4"
PC-23	41+18	5.5'	LEFT	7 1/2"	5"	12 1/2"
PC-24	42+82	13'	LEFT	4"	NONE	4"
PC-25	44+50	14'	RIGHT	7 1/2"	7"	14 1/2"
PC-26	46+17	4.5'	RIGHT	7 1/2"	NONE	7 1/2"
PC-27	47+85	4'	LEFT	6"	6"	12"
PC-28	49+51	13'	LEFT	5 1/2"	NONE	5 1/2"

**POROUS GRANULAR EMBANKMENT SUBGRADE LOCATIONS PER SOIL REPORT**

LOCATION	EAST SIDE		WEST SIDE	
	WIDTH	DEPTH	WIDTH	DEPTH
27+00 TO 31+40	18'	6"	18'	6"
33+50 TO 37+85	18'	6"	18'	6"

NOTE:  
ACTUAL LOCATIONS, WIDTHS, AND DEPTHS TO BE DETERMINED BY ENGINEER IN THE FIELD.

**PROJECT STAGING AND LOCAL ACCESS**

- DURING THE CONSTRUCTION OF THE PROJECT, PRAIRIE AVENUE WILL BE CLOSED TO THROUGH TRAFFIC FROM WASHINGTON AVENUE TO 31ST STREET.
- LOCAL ACCESS WILL BE PROVIDED AT MONROE AVENUE, JACKSON AVENUE, AND GARFIELD AVENUE.
- THE CONTRACTOR WILL MAINTAIN ACCESS TO THE INTERSECTIONS OF MONROE AVENUE, JACKSON AVENUE, AND GARFIELD AVENUE AT ALL TIMES INCLUDING DURING THE RECONSTRUCTION OF THE PAVEMENT SO THAT RESIDENTS WILL HAVE ACCESS TO THE ALLEY BEHIND THE HOUSES ON THE EAST SIDE OF PRAIRIE AVENUE.
- WHILE THE STORM SEWER, COMBINED SEWER, AND CONCRETE WORK IS BEING PERFORMED, ON-STREET PARKING WILL BE ALLOWED ON BOTH SIDES OF THE STREET AND 2 THROUGH LANES TO PERMIT LOCAL ACCESS WILL BE MAINTAINED DURING NON-WORKING HOURS.
- DURING THE INSTALLATION OF THE STORM SEWER AND COMBINED SEWER, ALL TRENCHES SHALL BE SAW-CUT FULL-DEPTH TO ALLOW FOR THE NEAT REMOVAL OF THE EXISTING PAVEMENT.
- DURING THE INSTALLATION OF THE CURB AND GUTTER, THE EXISTING PAVEMENT SHALL REMAIN IN PLACE. THE PAVEMENT SHALL BE SAW-CUT FULL-DEPTH 6" FROM THE EDGE OF PAVEMENT TO ALLOW FOR THE NEAT REMOVAL OF THE EXISTING CURB AND GUTTER.
- ALL REQUIRED SAW-CUTTING WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE PERTINENT INDIVIDUAL PAY ITEMS IN THE CONTRACT.
- THE TRENCHES FOR THE STORM SEWER LATERALS AND THE COMBINED SEWER REPAIRS WILL BE BACKFILLED TO THE TOP OF THE EXISTING PAVEMENT TO PROVIDE LOCAL ACCESS.
- THE TRENCH ALONG THE EAST SIDE OF PRAIRIE AVENUE FOR THE STORM SEWER WILL BE BACKFILLED TO THE TOP OF THE EXISTING PAVEMENT TO PROVIDE ACCESS AT ALL DRIVEWAYS, ALLEY RETURNS, INTERSECTIONS AND AS DIRECTED BY THE ENGINEER.
- THE EXCAVATION FOR THE PROPOSED PAVEMENT SHALL BEGIN AFTER THE PLACEMENT OF THE PROPOSED CONCRETE CURB AND GUTTER, DRIVEWAYS AND SIDEWALKS.