

HORIZONTAL AND VERTICAL CONTROL POINTS						
NO.	DESCRIPTION	STATION	OFFSET	NORTHING	NORTHING	ELEV.
1	CENTER INLET GRATE	168+07	300' RT.	1,196,841.74	2,007,378.99	757.33
2	CENTER INLET GRATE	172+33	282' RT.	1,197,267.18	2,007,361.18	756.01
3	REBAR W/ALUMINUM CAP	176+00	302' LT.	1,197,634.47	2,006,777.32	-
4	REBAR W/CAP	176+01	409' RT.	1,197,635.44	2,007,487.47	-
5	REBAR W/CAP	186+00	305' LT.	1,198,634.46	2,006,773.84	-
6	REBAR W/CAP	185+95	354' RT.	1,198,629.42	2,007,432.76	-
7	RR SPIKE IN TREE	185+95	657' RT.	1,198,629.42	2,007,735.76	742.64

SCOPE OF WORK

THE PROJECT SCOPE OF WORK CONSISTS OF THE REHABILITATION OF THE CONCRETE RUNWAY PAVEMENTS BY MEANS OF BITUMINOUS PATCHING, PAVEMENT REPAIR AND A POROUS FRICTION COURSE OVERLAY HAVING A MINIMUM THICKNESS OF 0.10-FT (1.2 INCHES). THE SCOPE OF THIS PROJECT IS LIMITED TO THE AREAS IDENTIFIED ON THE CONSTRUCTION PLANS AND DOES NOT INCLUDE THE ENTIRE RUNWAY.

AIRPORT SECURITY NOTE

AIRPORT SECURITY WILL BE MAINTAINED AT ALL TIMES. THE CONTRACTOR WILL CLOSE AND LOCK THE EXISTING GATE TO THE HAUL ROUTE AT THE END OF EACH WORKING DAY.

UTILITY NOTE

THE CONTRACTOR SHALL CONTACT ALL UTILITY COMPANIES AND AGENCIES WHICH HAVE LINES OR CONDUITS IN THE PROPOSED WORK AREA. ALL LINES AND CONDUITS SHALL BE LOCATED AND IDENTIFIED FOR DEPTH BEFORE ANY EXCAVATION BEGINS. THE CONTRACTOR WILL CALL J.U.L.I.E. (1-800-892-0123) TO ACCOMPLISH THE ABOVE. IT IS THE CONTRACTOR'S RESPONSIBILITY TO IDENTIFY ALL UNDERGROUND NON-JULIE UTILITIES LOCATED WITHIN THE PROPOSED CONSTRUCTION LIMITS. THESE UNDERGROUND IMPROVEMENTS WILL BE LOCATED AT THE CONTRACTOR'S OWN EXPENSE PRIOR TO THE START OF CONSTRUCTION ACTIVITIES.

HEIGHT OF CONSTRUCTION EQUIPMENT

THE MAXIMUM ANTICIPATED HEIGHT OF THE CONSTRUCTION EQUIPMENT WILL BE 25 FEET. THE TALLEST EQUIPMENT IS EXPECTED TO BE A SEMI TRUCK WITH TRAILER IN THE UP POSITION.

HAUL ROUTE AND EQUIPMENT PARKING

THE CONTRACTOR WILL USE THE DESIGNATED HAUL ROUTE AND PARKING AREA AS SHOWN ON THIS SAFETY PLAN. THE PROPOSED PARKING AREA WILL BE 200' X 200'. THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN THE PROPOSED HAUL ROUTE AND PARKING AREA THROUGHOUT THE COURSE OF THE PROJECT. ANY AREAS DAMAGED OUTSIDE OF THESE AREAS WILL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE. AT THE CONCLUSION OF THE PROJECT THE CONTRACTOR WILL GRADE AND REPAIR THE HAUL ROAD AND GRADE, FERTILIZE, SEED, AND MULCH THE PARKING AREA AS NEEDED TO RESTORE IT TO ITS' PRE CONSTRUCTION STATE TO THE SATISFACTION OF THE RESIDENT ENGINEER. MAINTENANCE AND RESTORATION OF THE HAUL ROUTE AND PARKING AREA WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE CONTRACTOR IS RESPONSIBLE FOR ACQUIRING ALL PERTINENT PERMITS AND APPROVAL TO USE THE TOWNSHIP ROADS.

CONTRACTOR RESPONSIBILITIES

THE CONTRACTOR'S EQUIPMENT PARKING AND STORAGE AREA WILL BE AS SHOWN ON THIS SHEET. THE CONTRACTOR'S EMPLOYEES WILL PARK THEIR VEHICLES IN THIS AREA. ONLY CONTRACTOR VEHICLES NECESSARY FOR CONSTRUCTION WILL BE ALLOWED OUTSIDE THIS AREA.

THE CONTRACTOR AND HIS EMPLOYEES WILL BE RESTRICTED TO THE WORK AREA AND ALL OTHER AREAS OF THE AIRPORT ARE "OFF LIMITS" TO THEM.

THE CONTRACTOR SHALL KEEP ONE RUNWAY OPEN AT ALL TIMES AND MAINTAIN CONTINUOUS TAXIWAY ACCESS TO ALL HANGARS AND ADMINISTRATIVE AREAS.

ALL WORK PERFORMED SHALL BE DONE IN AN ORDERLY AND EFFECTIVE MANNER TO MINIMIZE RUNWAY CLOSURE.

NO TRENCHES OR HOLES WILL REMAIN OPEN OVERNIGHT UNLESS APPROVED BY THE AIRPORT DIRECTOR.

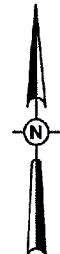
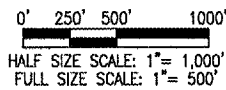
THE CONTRACTOR IS REQUIRED TO LIMIT THE USE OF CONSTRUCTION EQUIPMENT ON THE EXISTING PAVEMENTS. ONLY EQUIPMENT NEEDED TO COMPLETE THE SPECIFIC WORK ON THE EXISTING PAVEMENT WILL BE PERMITTED. THE CONTRACTOR IS RESPONSIBLE FOR REPAIRING ANY DAMAGE TO EXISTING PAVEMENTS CAUSED BY HIS PERSONNEL OR EQUIPMENT.

ENGINEER'S FIELD OFFICE

NO ENGINEER'S FIELD OFFICE WILL BE USED ON THIS PROJECT.

J.U.L.I.E. INFORMATION

COUNTY: ADAMS
CITY: QUINCY
TOWNSHIP: CLIMER
SECTION NO.: 34
ADDRESS: QUINCY REGIONAL AIRPORT - BALDWIN FIELD
1645 HIGHWAY 104
QUINCY, ILLINOIS 62301



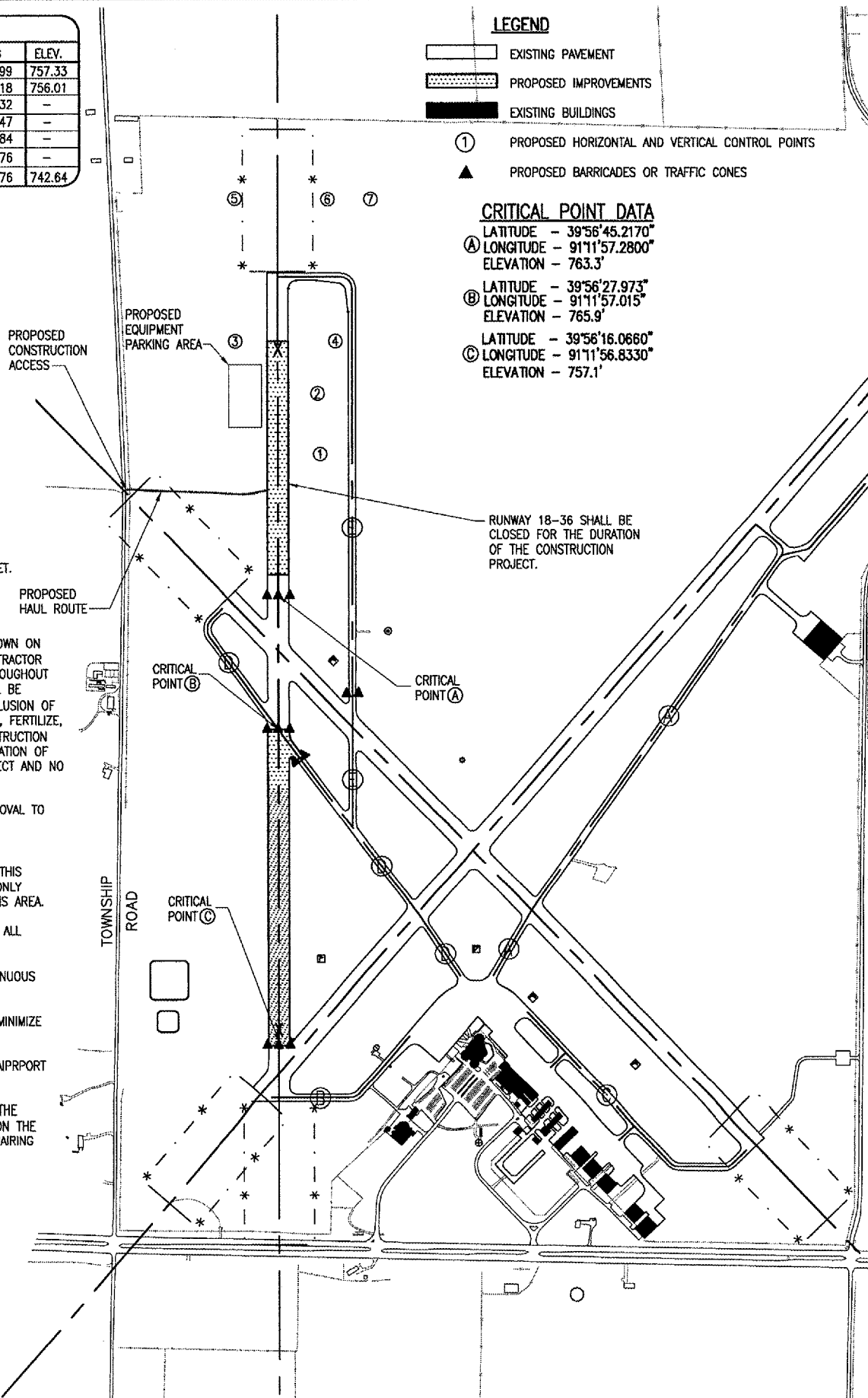
LEGEND

- EXISTING PAVEMENT
- PROPOSED IMPROVEMENTS
- EXISTING BUILDINGS

- PROPOSED HORIZONTAL AND VERTICAL CONTROL POINTS
- PROPOSED BARRICADES OR TRAFFIC CONES

CRITICAL POINT DATA

- LATITUDE - 39°56'45.2170"
- A LONGITUDE - 91°11'57.2800"
- ELEVATION - 763.3'
- LATITUDE - 39°56'27.973"
- B LONGITUDE - 91°11'57.015"
- ELEVATION - 765.9'
- LATITUDE - 39°56'16.0660"
- C LONGITUDE - 91°11'56.8330"
- ELEVATION - 757.1'



RUNWAY AND TAXIWAY CLOSURE

A RUNWAY MUST BE CLOSED ANYTIME CONSTRUCTION PERSONNEL OR EQUIPMENT ARE WITHIN 250-FT OF THE RUNWAY'S CENTERLINE. RUNWAY 18-36 SHALL BE CLOSED FOR THE DURATION OF THE PROJECT. RUNWAY 4-22 WILL REQUIRE CLOSURE. AT LEAST ONE RUNWAY MUST BE OPEN AT ALL TIMES DURING THE DAY AND TWO RUNWAYS OPEN OVER NIGHT.

WHEN CROSSING ACTIVE RUNWAYS, THE CONTRACTOR IS REQUIRED TO USE RADIO CONTACT WITH THE AIRPORT UNICOM. CROSSING ACTIVE RUNWAYS SHALL BE COORDINATED WITH THE RESIDENT ENGINEER. THE CONTRACTOR SHALL COORDINATE HIS CONSTRUCTION SCHEDULE TO LIMIT THE CROSSING OF ACTIVE RUNWAYS TO A MINIMUM. THE CONTRACTOR'S PERSONNEL WILL NOT BE ALLOWED TO FREELY CROSS THE RUNWAYS UNLESS THE RUNWAY IS CLOSED.

A TAXIWAY MUST BE CLOSED AT ANYTIME CONSTRUCTION PERSONNEL OR EQUIPMENT ARE WITHIN 72-FT OF A TAXIWAY CENTERLINE. VARIOUS TAXIWAY CLOSURE WILL BE REQUIRED THROUGHOUT THE PROJECT.

RUNWAY CLOSURE REQUIRES PLACEMENT OF YELLOW CROSSES OVER THE DESIGNATION NUMERALS AS DETAILED ON THIS SAFETY PLAN. BARRICADES AND TRAFFIC CONES SHALL BE PLACED AS SHOWN AND AS DIRECTED BY THE RESIDENT ENGINEER. BARRICADES MUST BE EQUIPPED WITH RED FLASHING LIGHTS.

PLACEMENT, MAINTENANCE, AND REMOVAL OF BARRICADES AND TRAFFIC CONES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION ALLOWED.

CROSSES MAY BE PLACED OVER THE NUMERALS OR OFF THE RUNWAY END AS DETAILED.

THE CONTRACTOR IS REQUIRED TO PLACE THE RUNWAY CLOSURE CROSS ON RUNWAY NUMERALS AS DETAILED WHEN RUNWAY CLOSURE IS NECESSARY.

RUNWAY CLOSURE PROCEDURES:

- * CONTACT THE AIRPORT MANAGER OR HIS ASSIGNED REPRESENTATIVE
- * ISSUANCE OF NOTAM BY THE AIRPORT MANAGER OR HIS ASSIGNED REPRESENTATIVE.
- * PLACEMENT OF CROSSES (SEE DETAIL THIS SHEET)
- * PLACEMENT OF LIGHTED BARRICADES. ONLY AT THE TIME THAT ALL OF THE ABOVE ARE COMPLETED MAY ANY CONSTRUCTION OPERATIONS WITHIN 250 FT. OF THE AFFECTED RUNWAY CENTERLINE BEGIN.

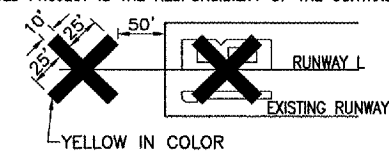
RUNWAY REOPENING PROCEDURES:

- * REMOVE CROSSES
- * REMOVE LIGHTED BARRICADES
- * NOTIFY THE AIRPORT MANAGER OR HIS REPRESENTATIVE TO CANCEL THE NOTAM.
- * CANCELLATION OF THE NOTAM. A CLOSED RUNWAY WILL NOT BE REOPENED UNTIL ALL EQUIPMENT AND WORK ARE FURTHER THAN 250 FT. FROM THE AFFECTED RUNWAY CENTERLINE.

THE PROPOSED CONSTRUCTION AREA WILL BE BLOCKED BY BARRICADES AND TRAFFIC CONES AS SHOWN ON THIS SAFETY PLAN. THE CONTRACTOR IS RESPONSIBLE FOR PLACING, MAINTAINING, RELOCATING, AND REMOVING BARRICADES, TRAFFIC CONES, AND SIGNAGE NECESSARY TO SUCCESSFULLY AND SAFELY CLOSE A RUNWAY AND/OR A TAXIWAY. THE CONTRACTOR MUST POSSESS ENOUGH BARRICADES AND TRAFFIC CONES TO CLOSE THE CONSTRUCTION AREA IN ACCORDANCE WITH THE CONSTRUCTION DRAWINGS AND THE SPECIAL PROVISIONS.

THE CONTRACTOR IS REQUIRED TO NOTIFY THE AIRPORT MANAGER, THROUGH THE RESIDENT ENGINEER, SEVEN (7) DAYS IN ADVANCE OF COMMENCEMENT OF WORK. IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROPERLY MARK AND BARRICADE THE CLOSED CONSTRUCTION AREA AND TO REMOVE THE BARRICADES AND CONES WHEN THE WORK IS COMPLETED AND THE CONCRETE HAS CURED. IT IS THE AIRPORT MANAGER'S RESPONSIBILITY TO ISSUE ALL NOTAM'S REQUIRED THROUGHOUT THE CONSTRUCTION OF THE PROJECT.

TRAFFIC CONTROL IN ALL FACETS OF THE PROPOSED PROJECT IS THE RESPONSIBILITY OF THE CONTRACTOR.



DETAIL OF CROSS FOR CLOSED RUNWAY

"NOT TO SCALE"

NOTE:

COST OF CONSTRUCTING, PLACING, MAINTAINING AND REMOVING CROSSES WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. THE CROSSES WILL BE YELLOW IN COLOR AND SHALL BE MADE OF A SUITABLE MATERIAL AS APPROVED BY THE AIRPORT MANAGER. THE CROSSES WILL BE PLACED OVER THE NUMERALS AND SECURED IN A MANNER APPROVED BY THE MANAGER. THE PROPOSED CROSSES WILL BE PLACED EACH DAY THE RUNWAY IS CLOSED AND REMOVED WHEN THE RUNWAY IS RE-OPENED. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE PLACEMENT AND REMOVAL OF THE CROSSES. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

PROPOSED SAFETY PLAN

GENERAL - THE QUINCY REGIONAL AIRPORT IS COMPRISED OF THREE PAVED RUNWAYS AND THEIR ASSOCIATED TAXIWAY SYSTEM.

IDENTIFICATION - WHEN THE CONTRACTORS VEHICLES AND EQUIPMENT ARE ON THE AIRPORT THEY SHALL BE PROPERLY MARKED WITH THREE (3') FOOT SQUARE CHECKERED FLAGS (INTERNATIONAL ORANGE AND WHITE). THE CONTRACTOR WILL ALSO PROVIDE WORKERS WITH SOME TYPE OF TAG OR GARMENT TO IDENTIFY THE PERSON AS BEING PART OF THE CONSTRUCTION CREW.

RADIO CONTROL - THE CONTRACTOR WILL BE REQUIRED TO BE IN TWO-WAY RADIO CONTACT (123.00 MHz.) WITH THE AIRPORT UNICOM. THIS WILL KEEP THE CONTRACTOR IN CONSTANT CONTACT WITH THE QUINCY REGIONAL AIRPORT AND ENABLE THE AIRPORT TO IMMEDIATELY CONTACT THE CONTRACTOR IN CASE OF AN AERONAUTIC EMERGENCY THAT WOULD REQUIRE ACTION BY THE CONTRACTOR AND/OR HIS PERSONNEL.

EROSION CONTROL

THIS PROJECT WILL NOT DISTURB MORE THAN 1 ACRE OF LAND, THEREFORE A N.P.D.E.S. PERMIT WILL NOT BE REQUIRED. A STORM WATER POLLUTION PREVENTION PLAN IS NOT INCLUDED IN THESE CONSTRUCTION PLANS.

REVISION	DATE	BY

QUINCY REGIONAL AIRPORT
BALDWIN FIELD
ADAMS COUNTY, ILLINOIS

IL PROJ. UN-3336 A.I.P. PROJ.: 3-17-0085-XX

Project No. 82405CRK0	DATE 01/17/04	6/14/05
Revision R-003357.DWG	DATE 6/14/05	6/24/05
Scale 1"=500'	DATE	
LAYOUT DAK	DATE	
DRAWN DAK	DATE	
REVIEWED JDW	DATE	

Hanson Professional Services, Inc.
1525 South Sixth Street
Springfield, Illinois 62705-2886
Offices Nationwide

REHABILITATE
RUNWAY 18-36

PROPOSED
SAFETY
PLAN

3
3 of 13 sheets

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PAVEMENT PREPARATION

BITUMINOUS SURFACE COURSE

THE EXISTING CONCRETE SHALL BE PREPARED PRIOR TO PAVING THE POROUS FRICTION COURSE OVERLAY. THE CONTRACTOR SHALL REMOVE ALL LOOSE MATERIAL AND DEBRIS FROM SPALLS, SCALING, POP-OUTS, AND CRACKS BY MEANS OF HIGH PRESSURE AIR OR WATER. THE MATERIAL AND DEBRIS SHALL BE REMOVED FROM THE AIRPORT PROPERTY BY THE CONTRACTOR.

ONCE LOOSE MATERIAL IS REMOVED AND THE PARTIAL DEPTH SPALLS AND PATCH AREAS ARE CLEANED TO THE SATISFACTION OF THE RESIDENT ENGINEER, A BITUMINOUS TACK COAT SHALL BE APPLIED TO THE SIDES AND BOTTOM AND A BITUMINOUS SURFACE COURSE SHALL BE PLACED AND COMPACTED SO THAT THE FINAL SURFACE OF THE PATCH IS FLUSH WITH THE SURROUNDING PAVEMENT.

PLAN QUANTITY OF BITUMINOUS SURFACE COURSE WAS CALCULATED ANTICIPATING 1,555 S.Y. OF REPAIR AREA AT A MAXIMUM DEPTH OF 2 INCHES.

ALL WORK, LABOR, EQUIPMENT, AND MATERIALS ASSOCIATED WITH THE REMOVAL OF LOOSE MATERIAL, PREPARATION AND CLEANING, BITUMINOUS TACK COAT AND BITUMINOUS SURFACE COURSE REQUIRED FOR THE PATCH AREAS SHALL BE INCLUDED IN THE CONTRACT BID PRICE FOR ITEM AR401610 BITUMINOUS SURFACE COURSE PER TON.

BUTT JOINT CONSTRUCTION

BITUMINOUS AND PC CONCRETE PAVEMENT MILLING SHALL BE REQUIRED AT THE LOCATIONS IDENTIFIED ON THESE CONSTRUCTION PLANS TO CONSTRUCT A BUTT JOINT FOR THE PROPOSED POROUS FRICTION COURSE MATERIAL TO MATCH THE EXISTING PAVEMENT GRADE. MILLING DEPTHS VARY FROM 0.10-FT (1.2 INCHES) AT THE JOINT AND TAPER TO ZERO AT THE LOCATION PROVIDED ON THESE PLANS. BUTT JOINT CONSTRUCTION SHALL BE COMPLETED IN ACCORDANCE WITH THE SPECIAL PROVISIONS.

PAYMENT WILL BE MADE UNDER ITEM AR401655 BUTT JOINT CONSTRUCTION PER SQUARE YARD.

CONCRETE SURFACE CLEANING

CONCRETE SURFACE CLEANING SHALL CONSIST OF THE REMOVAL OF EXCESSIVE OLD JOINT SEALANT FROM THE CONCRETE SURFACE ADJACENT TO SOME OF THE EXISTING JOINTS USING A SANDBLASTING OPERATION IN ACCORDANCE WITH THE SPECIAL PROVISIONS. THESE LOCATIONS ARE NOT UNIFORM AND ARE APPROXIMATELY THREE INCHES WIDE ON EITHER SIDE OF THE EXISTING JOINT. JOINTS REQUIRING SURFACE CLEANING WILL BE IDENTIFIED IN FIELD BY THE RESIDENT ENGINEER. THE CONTRACTOR SHALL TAKE PRECAUTION TO REMOVE ONLY EXCESS JOINT MATERIAL OUTSIDE THE JOINT AND AVOID REMOVING MATERIAL FROM WITHIN THE JOINT RESERVOIR.

PAYMENT WILL BE MADE UNDER ITEM AR800571 CONCRETE SURFACE CLEANING PER LINEAR FOOT.

SURFACE POPOUT REPAIR

SURFACE POPOUT REPAIR SHALL CONSIST OF THE CLEANING AND FILLING OF SMALL HOLES OR CAVITIES IN THE CONCRETE SURFACE THAT HAVE RESULTED FROM ENTRAPMENT OF AIR BUBBLES, UNSOUND AGGREGATE OR A CONTAMINATING MATERIAL SUCH AS MUD, WOOD OR GLASS. REPAIR METHODS SHALL BE IN ACCORDANCE WITH THE SPECIAL PROVISIONS. THE HOLES ARE LOCATED SPORADICALLY THROUGHOUT THE PROJECT LIMITS AND TYPICALLY DO NOT EXCEED 3/4 INCH IN DIAMETER. HOWEVER, EXTRA PAYMENT WILL NOT BE CONSIDERED FOR POPOUT REPAIR HOLES EXCEEDING 3/4 INCH DIAMETER.

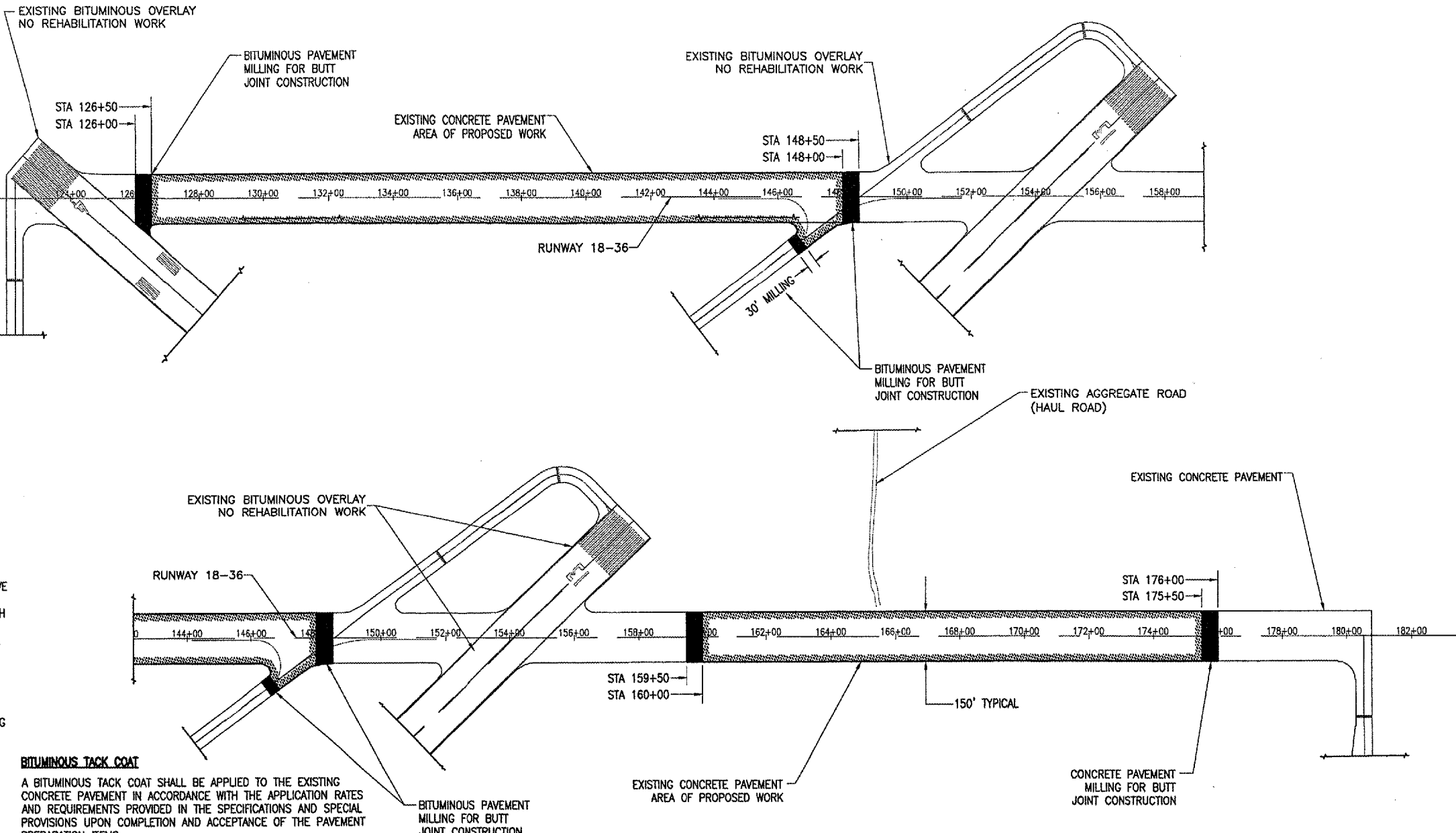
PAYMENT WILL BE MADE UNDER ITEM AR800572 SURFACE POPOUT REPAIR PER EACH.

PAVEMENT REMOVAL & REPLACEMENT

PAVEMENT REMOVAL AND REPLACEMENT SHALL CONSIST OF THE REMOVAL AND REPLACEMENT OF PAVEMENT SLABS THAT HAVE STRUCTURAL FAILURE OR RANDOM TRANSVERSE OR LONGITUDINAL CRACKS THAT EXTEND THROUGH THE DEPTH OF THE CONCRETE PAVEMENT. THE PAVEMENTS SHALL BE REMOVED FULL-DEPTH AND REPLACED IN ACCORDANCE WITH THE SPECIAL PROVISIONS. THE REPAIR METHOD WILL REQUIRE REMOVAL AND REPLACING COMPLETE AND PORTIONS OF CONCRETE SLABS TO THE BOTTOM OF THE PAVEMENT TO RESTORE THE DETERIORATED AREAS. THE DETERIORATED CONCRETE WILL BE REMOVED, THE PATCH AREA PREPARED, PATCH MATERIAL PLACED, DOWELS INSTALLED AND THE JOINT SYSTEM WILL BE REFORMED IN ACCORDANCE WITH THE SPECIAL PROVISIONS.

THE RESIDENT ENGINEER WILL IDENTIFY THE PAVEMENTS REQUIRING REMOVAL AND REPLACEMENT IN THE FIELD PRIOR TO THE START OF CONSTRUCTION.

THE CONTRACTOR SHALL REFER TO THE SPECIAL PROVISIONS FOR CONSTRUCTION METHODS AND REQUIREMENTS OF THE PAVEMENT REMOVAL AND REPLACEMENT. THESE PAVEMENT AREAS SHALL BE PAID UNDER ITEM AR800569 PAVEMENT REMOVAL & REPLACEMENT PER SQUARE YARD.



BITUMINOUS TACK COAT

A BITUMINOUS TACK COAT SHALL BE APPLIED TO THE EXISTING CONCRETE PAVEMENT IN ACCORDANCE WITH THE APPLICATION RATES AND REQUIREMENTS PROVIDED IN THE SPECIFICATIONS AND SPECIAL PROVISIONS UPON COMPLETION AND ACCEPTANCE OF THE PAVEMENT PREPARATION ITEMS.

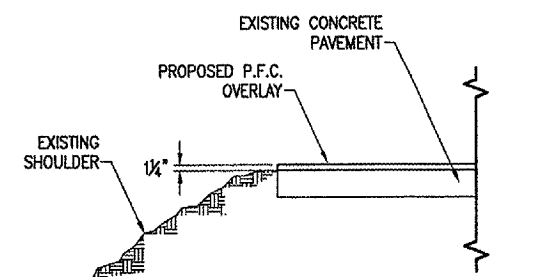
BITUMINOUS TACK COAT FOR THIS APPLICATION SHALL BE PAID UNDER ITEM ARB03510 BITUMINOUS TACK COAT PER GALLON.

POROUS FRICTION COURSE

ONCE THE EXISTING CONCRETE PAVEMENT IS PREPARED TO THE SATISFACTION OF THE RESIDENT ENGINEER, A POLYMER MODIFIED POROUS FRICTION COURSE SHALL BE APPLIED IN ACCORDANCE WITH THE SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS. THE NOMINAL COMPACTED THICKNESS OF THE POROUS FRICTION COURSE SHALL BE A MINIMUM OF 0.10-FT (1.2 INCHES).

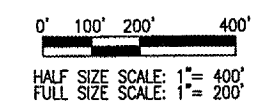
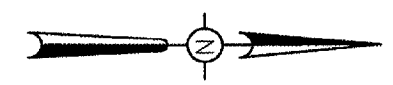
THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 1-800-892-0123.



SHOULDER WORK

EXISTING SHOULDER IS CURRENTLY AT THE ELEVATION OF THE EXISTING PAVEMENT. THE PROPOSED POROUS FRICTION COURSE OVERLAY WILL ALLOW FOR 1/4" DROP FROM EDGE OF RUNWAY PAVEMENT TO EARTH SHOULDER. THEREFORE NO SHOULDER WORK WILL BE REQUIRED.



LEGEND

- PROPOSED POROUS FRICTION COURSE OVERLAY
- EXISTING PAVEMENT
- PROPOSED MILLING AND P.F.C. OVERLAY

DATE	REVISION	BY

QUINCY REGIONAL AIRPORT
BALDWIN FIELD
ADAMS COUNTY, ILLINOIS

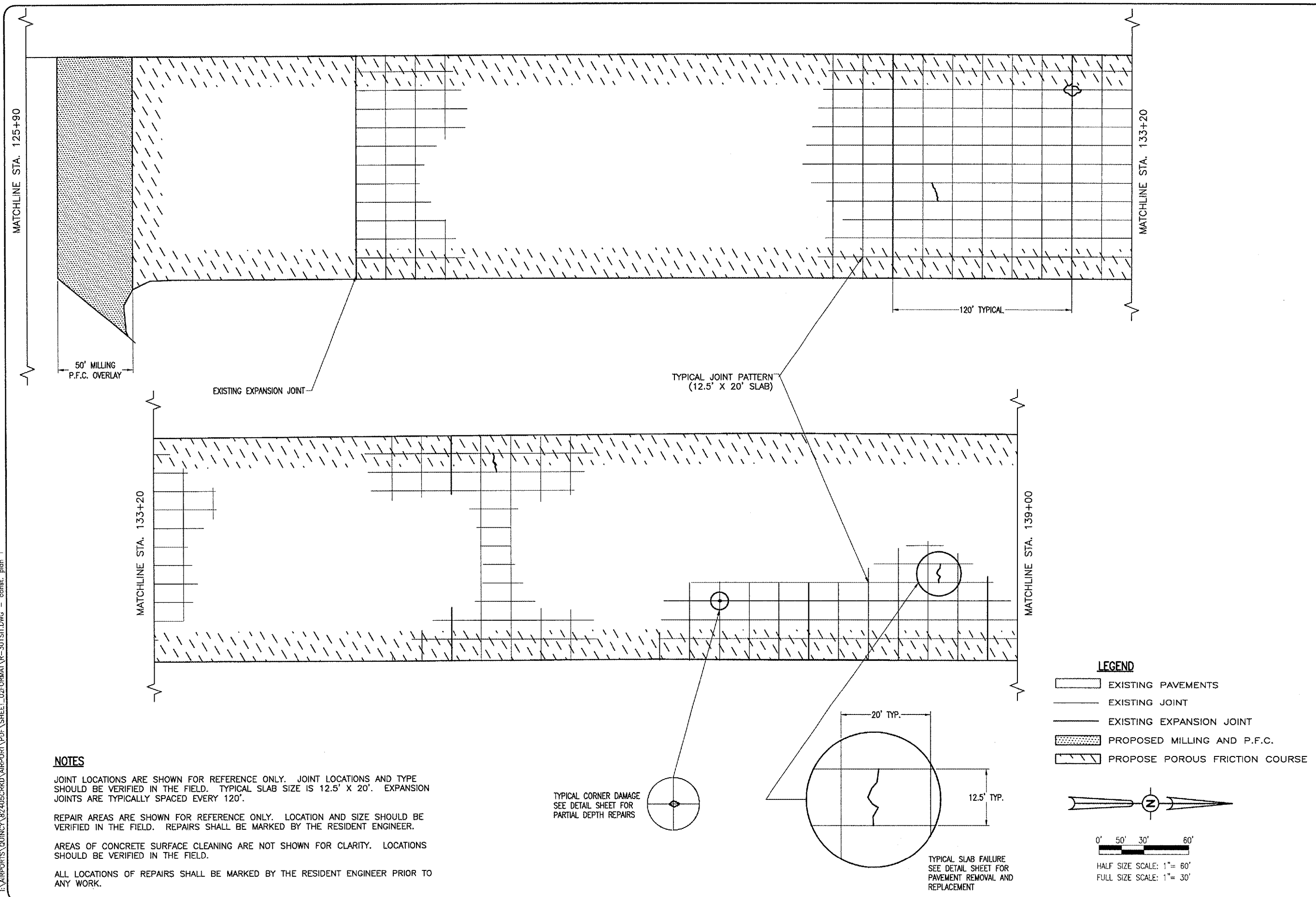


UPPER SHEET NO.	06/16/05
DWG. NO.	06/20/05
DATE	06/25/05
DESIGNED BY	DAK
CHECKED BY	DAK
DATE	06/01/05
APPROVED BY	JDW

HANSON
Professional Services Inc.
1325 South Stom Street
Springfield, Illinois 62703-2886
Chicago, Illinois

REHABILITATE
RUNWAY 18-36
EXISTING
SITE
PLAN

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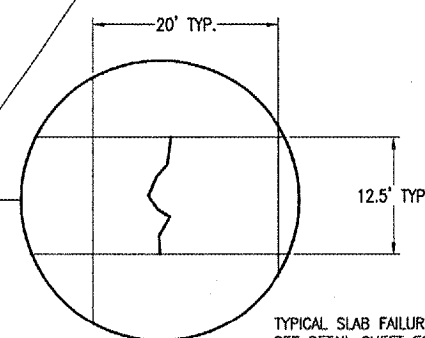
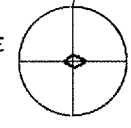
JOINT LOCATIONS ARE SHOWN FOR REFERENCE ONLY. JOINT LOCATIONS AND TYPE SHOULD BE VERIFIED IN THE FIELD. TYPICAL SLAB SIZE IS 12.5' X 20'. EXPANSION JOINTS ARE TYPICALLY SPACED EVERY 120'.

REPAIR AREAS ARE SHOWN FOR REFERENCE ONLY. LOCATION AND SIZE SHOULD BE VERIFIED IN THE FIELD. REPAIRS SHALL BE MARKED BY THE RESIDENT ENGINEER.

AREAS OF CONCRETE SURFACE CLEANING ARE NOT SHOWN FOR CLARITY. LOCATIONS SHOULD BE VERIFIED IN THE FIELD.

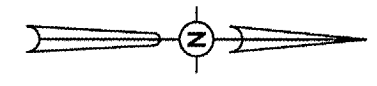
ALL LOCATIONS OF REPAIRS SHALL BE MARKED BY THE RESIDENT ENGINEER PRIOR TO ANY WORK.

TYPICAL CORNER DAMAGE
 SEE DETAIL SHEET FOR
 PARTIAL DEPTH REPAIRS



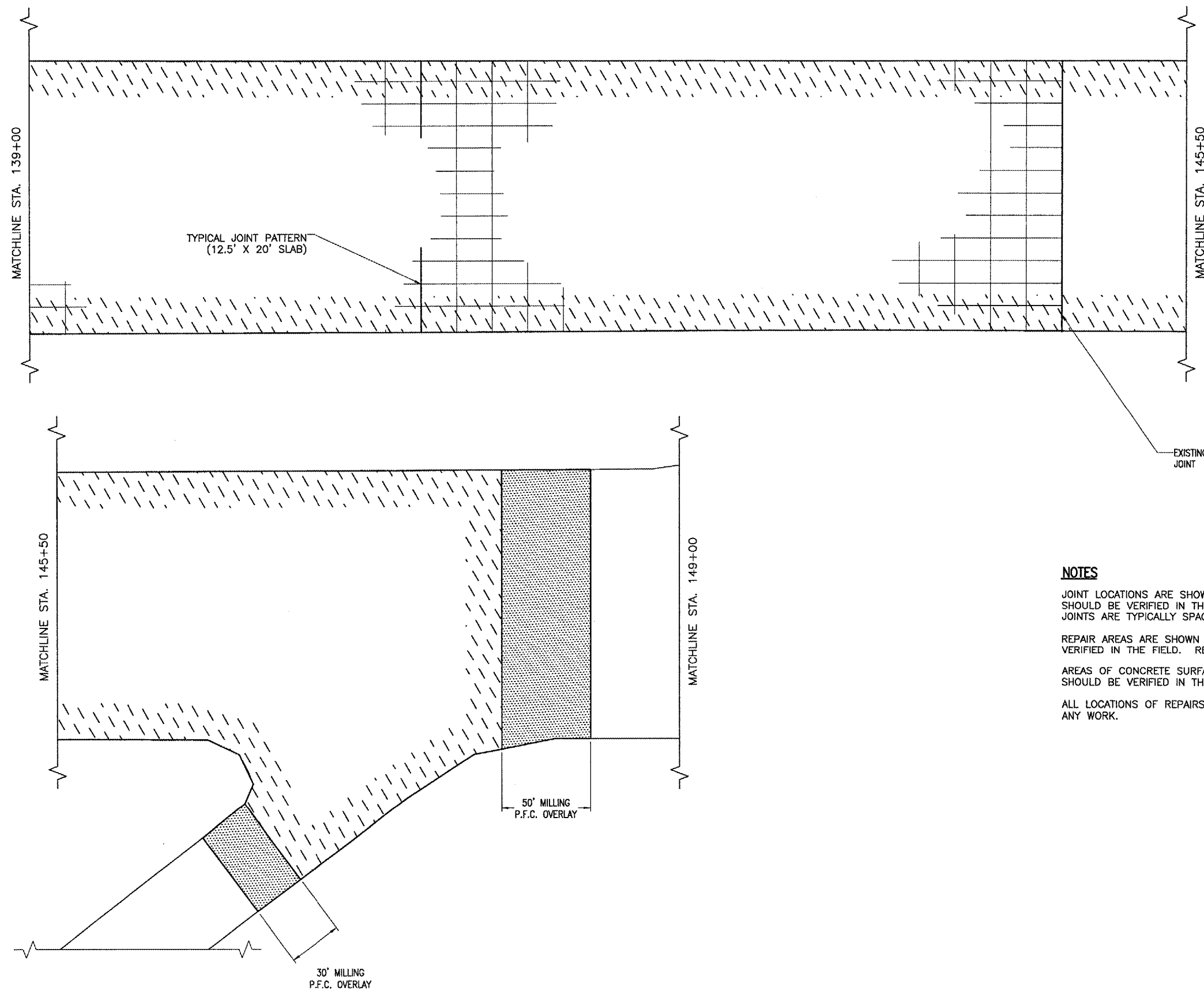
TYPICAL SLAB FAILURE
 SEE DETAIL SHEET FOR
 PAVEMENT REMOVAL AND
 REPLACEMENT

- LEGEND**
- EXISTING PAVEMENTS
 - EXISTING JOINT
 - EXISTING EXPANSION JOINT
 - PROPOSED MILLING AND P.F.C.
 - PROPOSE POROUS FRICTION COURSE



0' 50' 30' 60'
 HALF SIZE SCALE: 1" = 60'
 FULL SIZE SCALE: 1" = 30'

<p>QUINCY REGIONAL AIRPORT BALDWIN FIELD ADAMS COUNTY, ILLINOIS</p>		DATE REVISION	BY
ILL. PROJECT NO. 82405CRKD DRAWING NO. RUBBER STAMP (DIMENAMES) SCALE 1"=XX' DATE XX/XX/XX		ILL. PROJ.: UN-3336 A.I.P. PROJ.: 3-17-0085-XX	
LAYOUT DRAWN REVIEWED	XXX XXX XXX	XX/XX/XX XX/XX/XX XX/XX/XX	XX/XX/XX XX/XX/XX XX/XX/XX
Hanson Professional Services Inc. 1525 South Sixth Street Springfield, Illinois 62705-2896 Offices Nationwide		REHABILITATE RUNWAY 18-36 CONSTRUCTION PLAN STA 125+90 TO STA 139+00	
<p style="font-size: 2em; font-weight: bold;">5</p> 5 of 13 sheets			



NOTES

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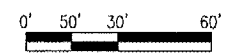
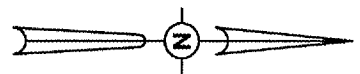
REPAIR AREAS ARE SHOWN FOR REFERENCE ONLY. LOCATION AND SIZE SHOULD BE VERIFIED IN THE FIELD. REPAIRS SHALL BE MARKED BY THE RESIDENT ENGINEER.

AREAS OF CONCRETE SURFACE CLEANING ARE NOT SHOWN FOR CLARITY. LOCATIONS SHOULD BE VERIFIED IN THE FIELD.

ALL LOCATIONS OF REPAIRS SHALL BE MARKED BY THE RESIDENT ENGINEER PRIOR TO ANY WORK.

LEGEND

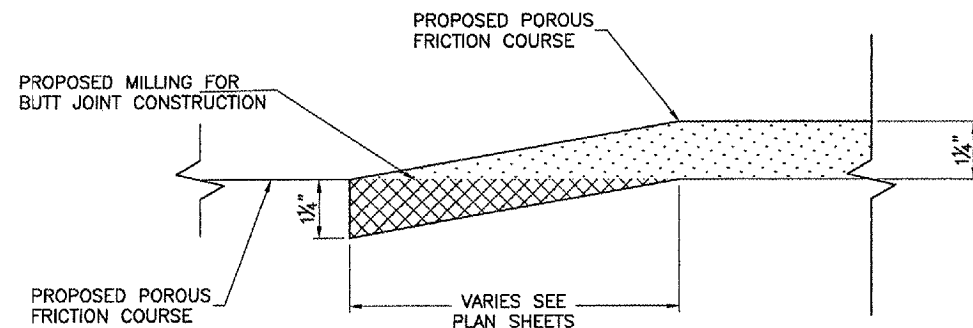
- EXISTING PAVEMENTS
- EXISTING JOINT
- EXISTING EXPANSION JOINT
- PROPOSED MILLING AND P.F.C.
- PROPOSE POROUS FRICTION COURSE



HALF SIZE SCALE: 1" = 60'
 FULL SIZE SCALE: 1" = 30'

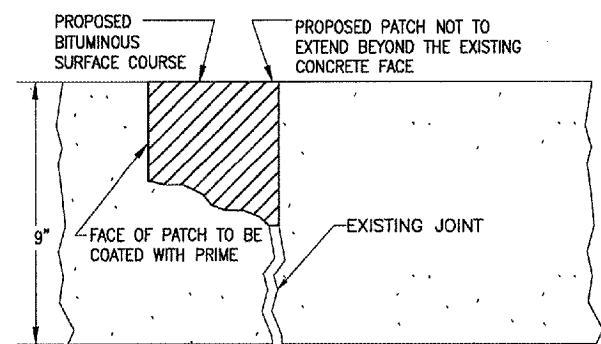
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DATE	REVISION	BY	 QUINCY REGIONAL AIRPORT BALDWIN FIELD ADAMS COUNTY, ILLINOIS	I.L. PROJ.: UN-3336 A.I.P. PROJ.: 3-17-0085-XX
I.L. Project No. 82405CRKD Designer: UPPER & GETWAR, D/W/G/N/A/M/E/S Scale: 1" = 30' Date: XX/XX/XX			LAYOUT XXX XX/XX/XX DRAWN XXX XX/XX/XX REVIEWED XXX XX/XX/XX	
Hanson Professional Services, Inc. 1525 South Sixth Street Springfield, Illinois 62703-2686 Offices Nationwide				
REHABILITATE RUNWAY 18-36		CONSTRUCTION PLAN STA 139+00 TO STA 149+00		
6				
6 of 13 sheets				



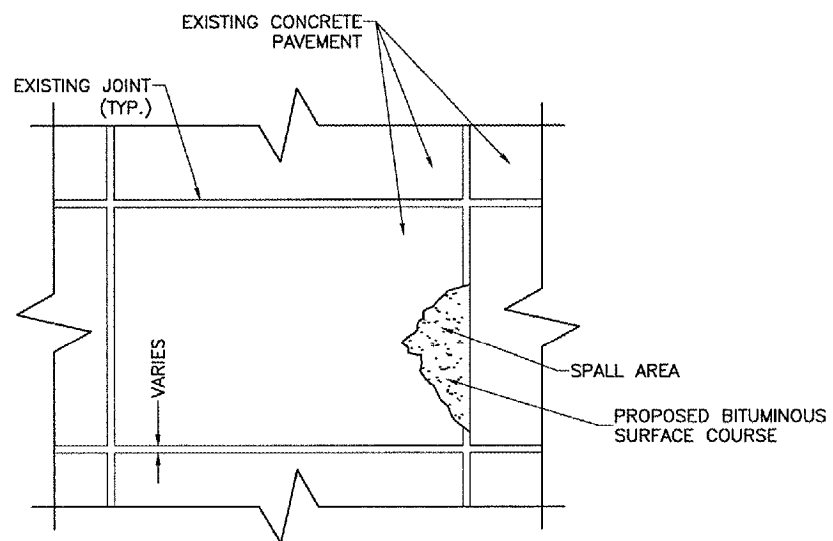
MILLING FOR BUTT JOINT

SECTION VIEW
NOT TO SCALE



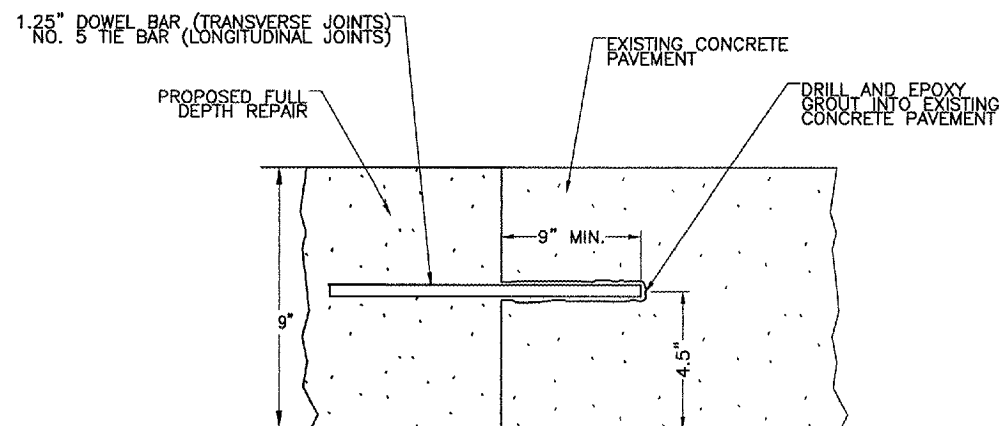
PARTIAL DEPTH SPALL REPAIR

SECTION VIEW
NOT TO SCALE



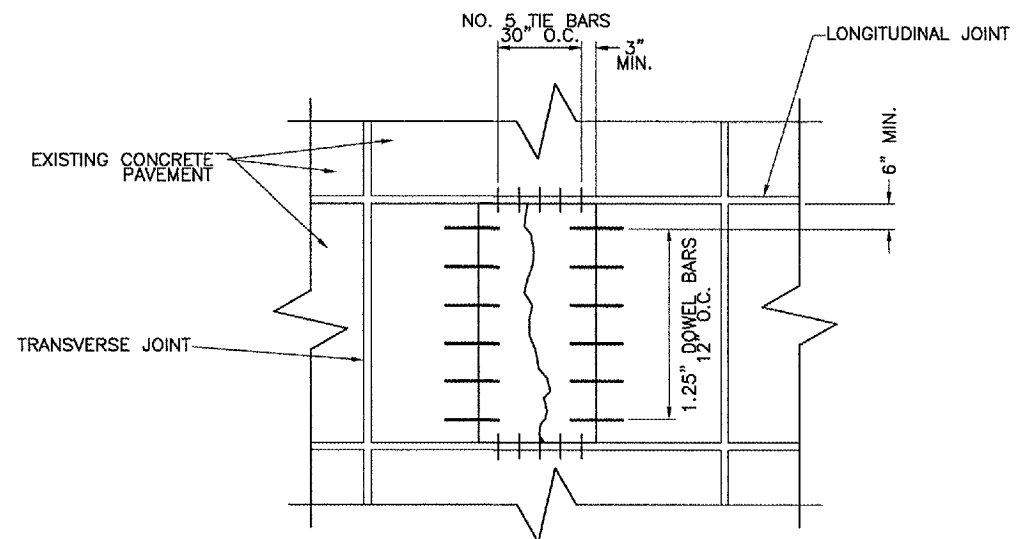
PARTIAL DEPTH SPALL REPAIR

PLAN VIEW
NOT TO SCALE



PAVEMENT REMOVAL & REPLACEMENT

SECTION VIEW
NOT TO SCALE



PAVEMENT REMOVAL & REPLACEMENT

PLAN VIEW
NOT TO SCALE

IF THE REMOVAL AND REPLACEMENT REQUIRES RECONSTRUCTION OF AN EXISTING EXPANSION JOINT, THE JOINT SHALL BE CONSTRUCTED TO THE DIMENSIONS AND MATERIALS TO MATCH THE EXISTING CONDITIONS.

GENERAL NOTE:

REFER TO THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS, CONSTRUCTION METHODS AND MATERIAL SPECIFICATIONS ASSOCIATED WITH THESE DETAILS.

REVISION	DATE	BY

QUINCY REGIONAL AIRPORT
BALDWIN FIELD
ADAMS COUNTY, ILLINOIS

IL PROJ.: UN-3336 A.I.P. PROJ.: 3-17-0085-XX

Project No.	82405CRKO	DATE	6/22/05
Drawn	DNA	DATE	6/28/05
Scale	DNA	DATE	7/1/05
DATE	6/22/05	LAYOUT	DAK
		DRAWN	DAK
		REVIEWED	JDW

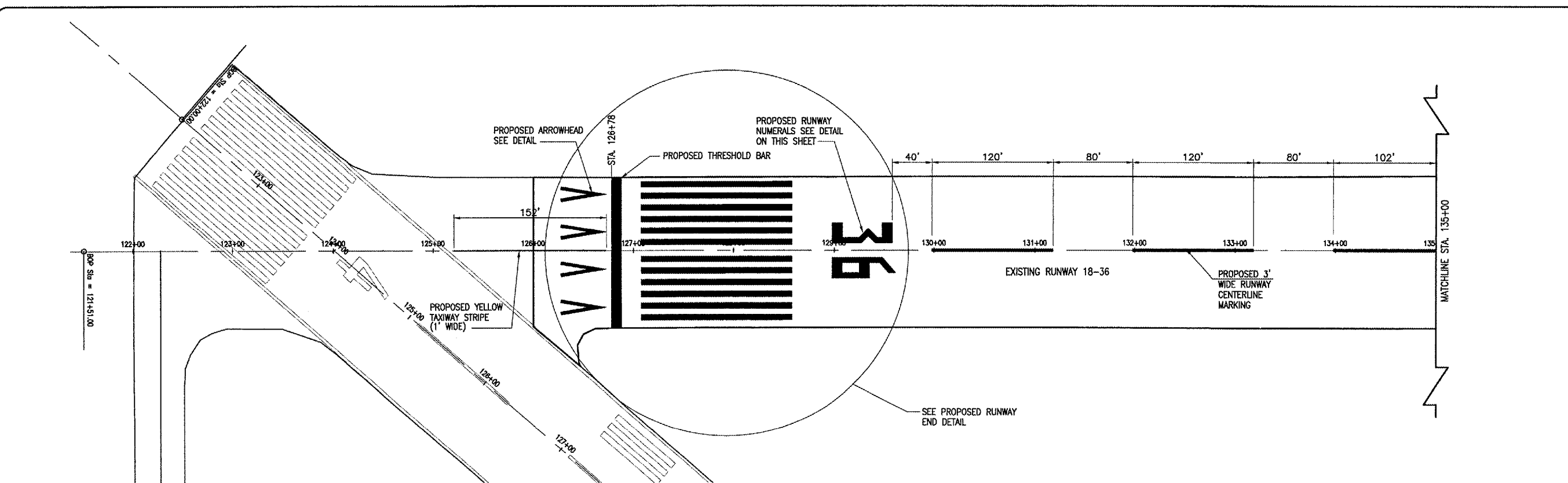
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Springfield, Illinois 62703-2886
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REHABILITATE
RUNWAY 18-36

CONSTRUCTION
DETAILS

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RUNWAY MARKING QUANTITIES				
DESCRIPTION	UNIT AREA	NUMBER REQUIRED	TOTAL AREA WH (S.F.)	TOTAL AREA BLK (S.F.)
RUNWAY 18-36 CENTERLINE STRIPE (120 FT)	360	22	7,920	
RUNWAY 18-36 CENTERLINE STRIPE (120 FT)	124	22		2,728
RUNWAY 18-36 CENTERLINE STRIPE (97 FT)	291	1	291	
RUNWAY 18-36 CENTERLINE STRIPE (97 FT)	101	1		101
RUNWAY 18-36 CENTERLINE STRIPE (93 FT)	279	1	279	
RUNWAY 18-36 CENTERLINE STRIPE (93 FT)	97	1		97
THRESHOLD (RUNWAY END)	862.5	24	20,700	
THRESHOLD (RUNWAY END)	156.75	24		3,762
AIMING POINT MARKING	4,500	4	18,000	
AIMING POINT MARKING	181	4		724
THRESHOLD BAR	1,500	1	1,500	
THRESHOLD BAR	150	1		150
NUMERAL 3 (OF NUMERAL 36)	634	1	634	
NUMERAL 3 (OF NUMERAL 36)	102	1		102
NUMERAL 6 (OF NUMERAL 36)	712	1	712	
NUMERAL 6 (OF NUMERAL 36)	114	1		114
NUMERAL 1 (OF NUMERAL 18)	318	1	318	
NUMERAL 1 (OF NUMERAL 18)	68	1		68
NUMERAL 8 (OF NUMERAL 18)	875.5	1	876	
NUMERAL 8 (OF NUMERAL 18)	132.5	1		132.5
		TOTAL	51,230	7,979
			YELLOW	BLACK
ARROWHEADS	220	4	880	
ARROWHEADS	296	4		304
HOLDING POSITION (TAXIWAY E)	152	1	152	
HOLDING POSITION (TAXIWAY E) (BLACK)	400	1		400
CENTERLINE (TAXIWAY D)	619	1	619	
CENTERLINE (TAXIWAY E)	1,359	1	1,359	
CENTERLINE (SOUTH 36 END)	154	1	154	
CENTERLINES (BLACK BORDER)	2,132	1		2,132
		TOTAL	26,161	2,217
			TOTAL (WHITE)	51,230
			TOTAL (YELLOW)	3,161
			TOTAL (BLACK)	10,815
			TOTAL MARKING	65,206

PROPOSED PAVEMENT MARKING - RUNWAY

THE PROPOSED PAVEMENT MARKING SHALL CONSIST OF RUNWAY 18-36 THRESHOLD STRIPES, THRESHOLD BAR, CENTERLINE STRIPES, AIMING POINT MARKING, ARROWHEADS, AND NUMERALS 1, 8, 3, AND 6. IN ACCORDANCE WITH THE DETAILS.

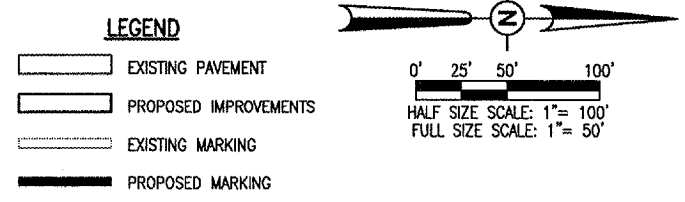
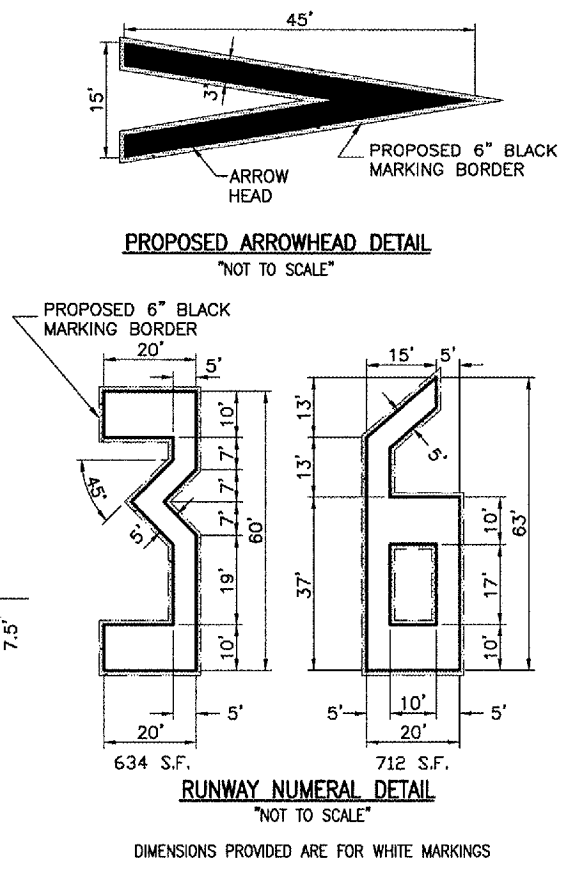
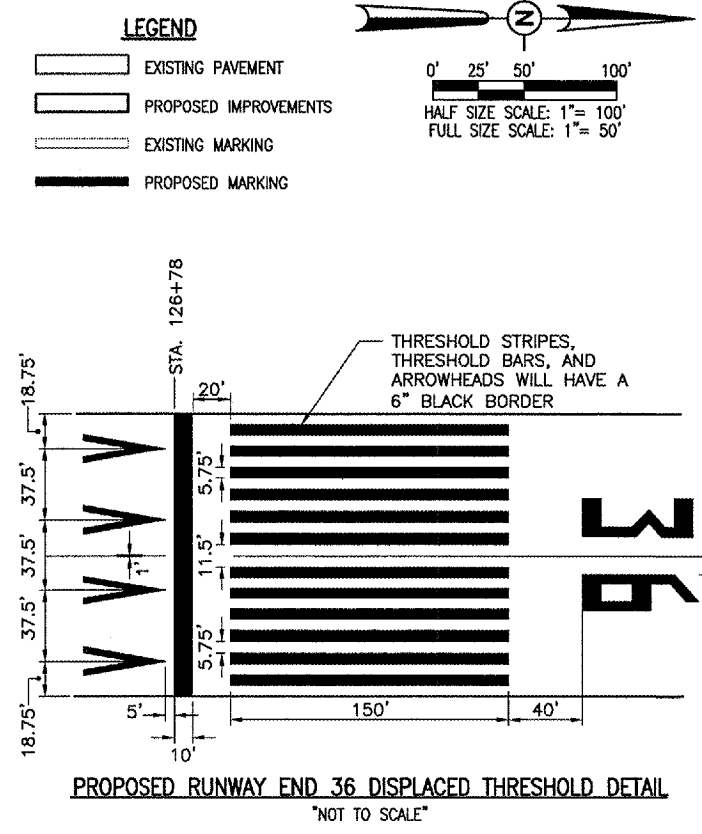
ALL RUNWAY MARKING SHALL BE SOLID AND WHITE IN COLOR AND OUTLINED WITH A SIX INCH WIDE BLACK BORDER.

ALL WHITE AND BLACK MARKING SHALL REQUIRE TWO COATS OF PAINT.

REFLECTIVE MEDIA SHALL BE REQUIRED IN BOTH COATS OF WHITE PAINT. BLACK PAINT DOES NOT REQUIRE REFLECTIVE MEDIA.

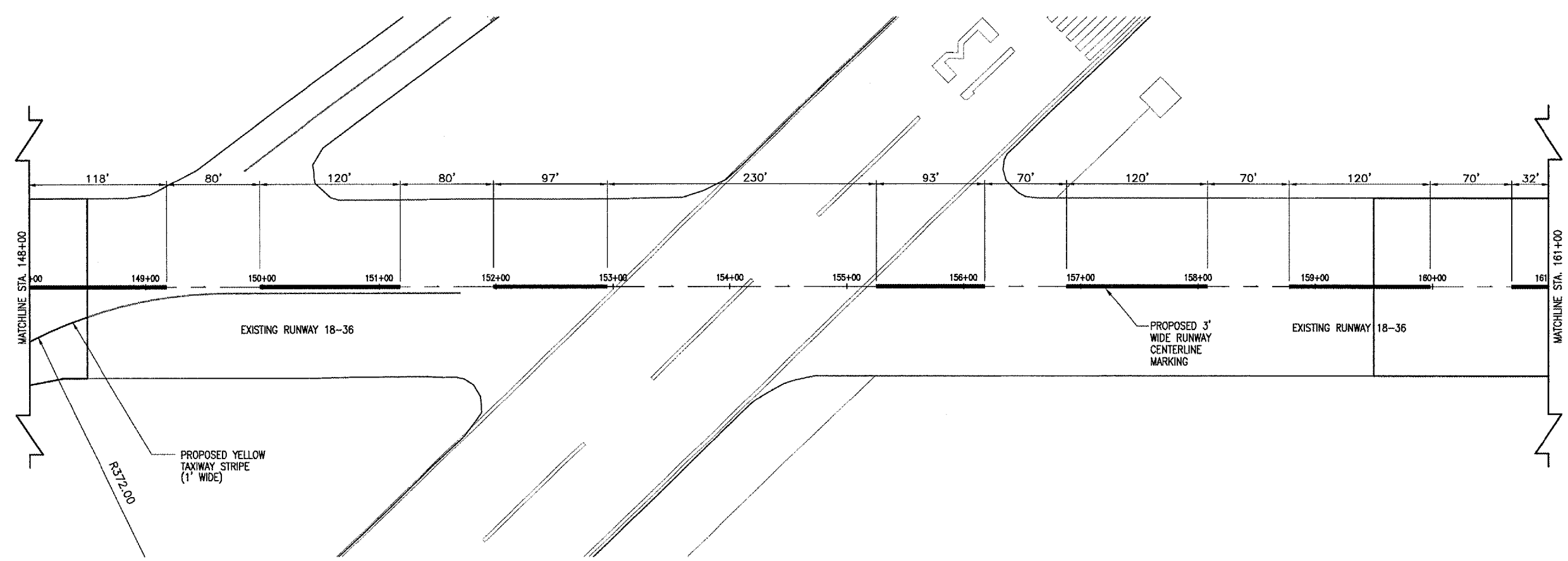
CUT-OFF SHEETS OR SHINGLES ARE REQUIRED TO INSURE STRAIGHT EDGES.

PROPOSED MARKING SHALL BE PAID FOR UNDER ITEMS AR620520 "PAVEMENT MARKING - WATERBORNE" AND AR620525 "PAVEMENT MARKING - BLACK BORDER" RESPECTIVELY, PER SQUARE FOOT.

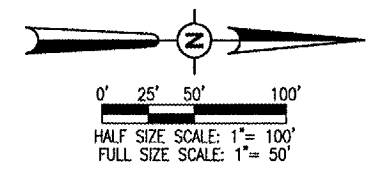


BY	
REVISION	
DATE	16/30/05
	ADD MARKING
QUINCY REGIONAL AIRPORT BALDWIN FIELD ADAMS COUNTY, ILLINOIS	
ILL. PROJ.: UN-3336 A.I.P. PROJ.: 3-17-0085-XX	
FILE PROJECT NO. B2405CRK FILENAME: R-151MRK.DWG SCALE: 1"=50' DATE: 4/28/03	LAYOUT: RAW DRAWN: JRH REVIEWED: JOW
HANSON Hanson Professional Services Inc. 1625 South Sixth Street Springfield, Illinois 62703-2888 Office: Natick, MA	
REHABILITATE RUNWAY 18-36 PROPOSED MARKING PLAN STA. 122+00 TO STA. 135+00	
9 9 of 13 sheets	

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- LEGEND**
- EXISTING PAVEMENT
 - PROPOSED IMPROVEMENTS
 - EXISTING MARKING
 - PROPOSED MARKING



DATE	REVISION	BY
6/30/05	ADD MARKING	

QUINCY REGIONAL AIRPORT
 BALDWIN FIELD
 ADAMS COUNTY, ILLINOIS

IL. PROJ.: JUN-3336 A.I.P. PROJ.: 3-17-0085-XX

ILL. Project No. B2405CRKD Drawings R-15.11MRK.DWG Scale 1"=50' Date 4/28/03	LAYOUT DRAWN JRH REVIEWED JDW	4/28/03 6/22/03 7/29/03
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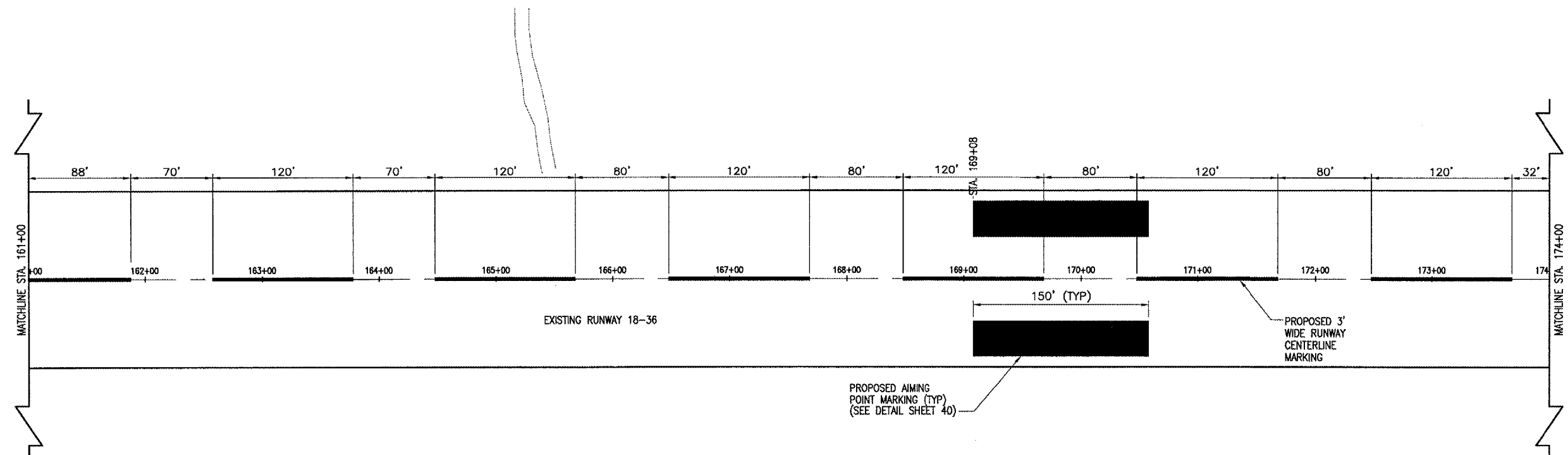
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 Springfield, Illinois 62703-2886
 Offices Nationwide

REHABILITATE
 RUNWAY 18-36

PROPOSED
 MARKING PLAN

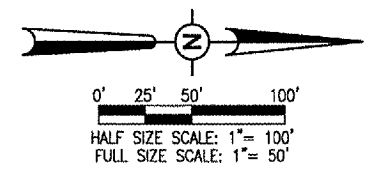
STA. 148+00 TO STA. 161+00

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LEGEND

- EXISTING PAVEMENT
- PROPOSED IMPROVEMENTS
- EXISTING MARKING
- PROPOSED MARKING



DATE	REVISION	BY
6/30/05	ADD MARKING	DAK

QUINCY REGIONAL AIRPORT
 BALDWIN FIELD
 ADAMS COUNTY, ILLINOIS

IL. PROJ.: UN-3336 A.I.P. PROJ.: 3-17-0085-XX

FILE PROJECT No. 82405CRKD	FILE PROJECT No. 82405CRKD
FILE PROJECT No. R-151MRK.DWG	FILE PROJECT No. R-151MRK.DWG
SCALE 1"=50'	SCALE 1"=50'
DATE 6/22/03	DATE 6/22/03
LAYOUT JRH	LAYOUT JRH
DRAWN JRH	DRAWN JRH
REVIEWED JDW	REVIEWED JDW
DATE 6/22/03	DATE 6/22/03

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REHABILITATE
 RUNWAY 18-36

PROPOSED
 MARKING PLAN
 STA. 161+00 TO STA. 174+00

