

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

F.A.I. ROUTE 90/94: I-90/94 (DAN RYAN EXPRESSWAY)
AT ROOSEVELT RD.
SECTION: 2011-070-1
BRIDGE DECK SEALING & JOINT RECONSTRUCTION
COOK COUNTY

C-91-627-11

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2011-070-1	COOK	27	1
ILLINOIS		CONTRACT NO. 60P89		

* 27+7-1=33

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT IS LOCATED IN THE CITY OF CHICAGO.

TRAFFIC DATA

I-90/94:

2009 ADT = 197,100
POSTED SPEED LIMIT = 55 MPH

ROOSEVELT RD:

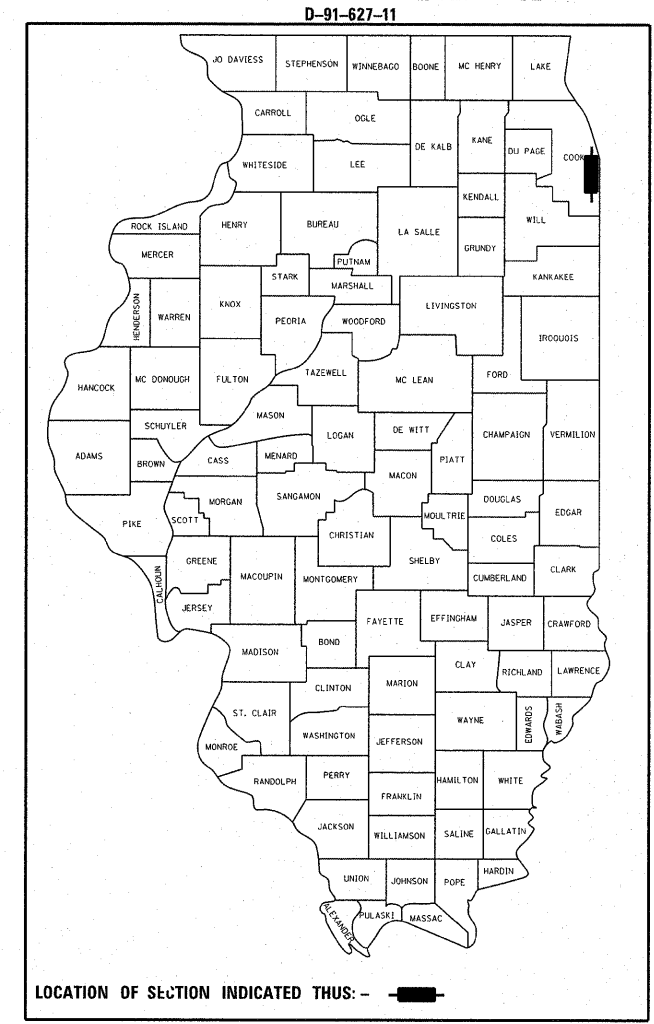
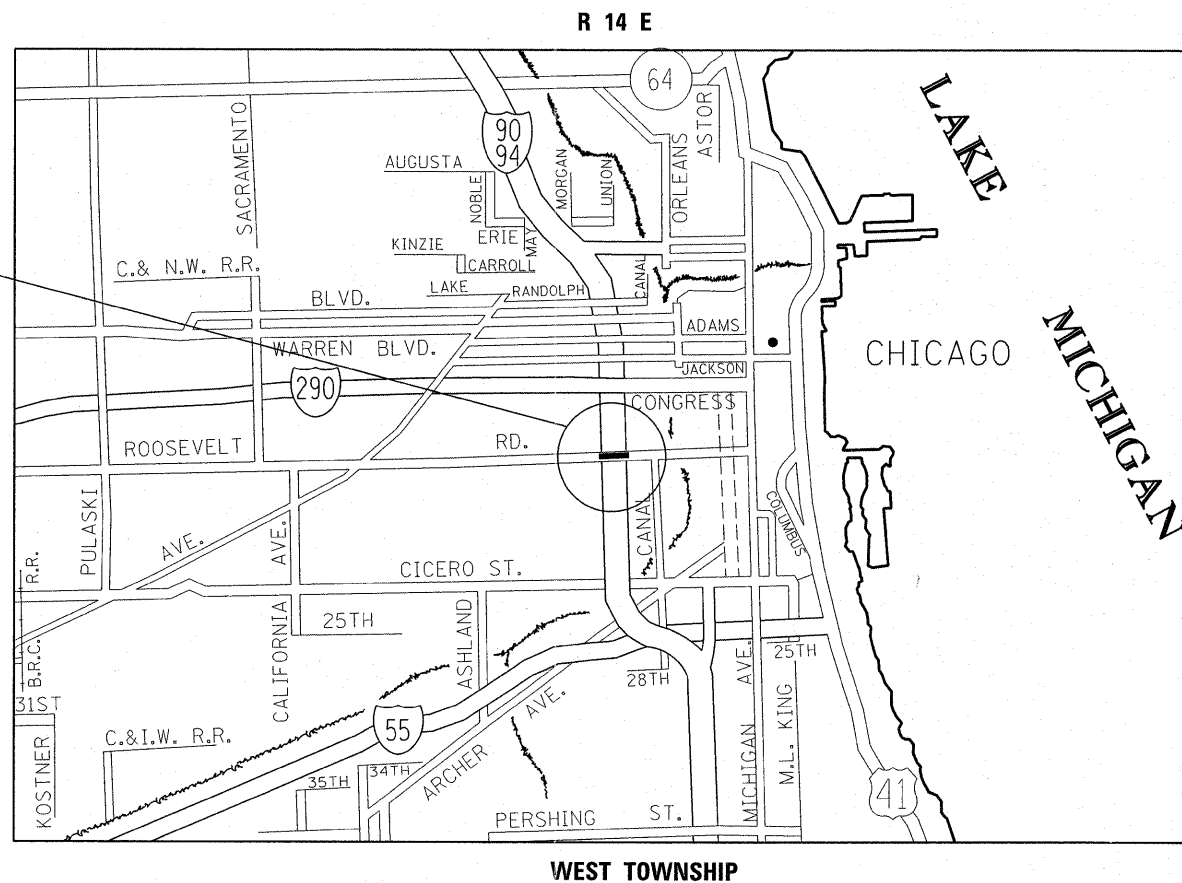
2010 ADT = 28,100
POSTED SPEED LIMIT = 30 MPH

SN 016-0478:
I-90/94 AT ROOSEVELT

C.U.A.N.
CHICAGO UTILITY ALERT NETWORK
(312) 744-7000

PROJECT ENGINEER: BOB BORO (847) 705-4237
PROJECT MANAGER: ISSAM RAYYAN

CONTRACT NO. 60P89



LOCATION OF SECTION INDICATED THIS: - [black rectangle] -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED NOVEMBER 3, 2011

Diane M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

December 9, 2011
Scott E. Stitt P.E.
Acting ENGINEER OF DESIGN AND ENVIRONMENT

December 9, 2011
William R. Frey
Interim DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

GROSS LENGTH = 680.00 FT. = 0.117 MILE
NET LENGTH = 680.00 FT. = 0.117 MILE

INDEX OF SHEETS

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- 20 CITY OF CHICAGO CATCH BASIN, INELT, AND MANHOLE DETAILS (BD-47)
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- 23 TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14)
- 24 TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES (TC-17)
- 25 ARTERIAL ROAD INFORMATION SIGN (TC-22)
- 26-27 CITY OF CHICAGO TYPICAL PAVEMENT MARKINGS (TC-24)

STATE STANDARDS

STANDARD NO.	DESCRIPTION
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701400-05	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701401-06	LANE CLOSURE, FREEWAY/EXPRESSWAY
701606-08	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701801-05	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-02	TRAFFIC CONTROL DEVICES
701411-08	
701446-03	

GENERAL NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "C.U.A.N." (CHICAGO UTILITY ALERT NETWORK) AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED).

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF CHICAGO.

SIDEWALK HANDICAPPED RAMPS SHALL NOT BE CONSTRUCTED DIRECTLY OVER EXISTING OR PROPOSED DRAINAGE STRUCTURES.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY OF CHICAGO AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE CONTRACTOR WILL NOT BE ABLE TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING, EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 FOR ARTERIALS AND (847) 705-4151 FOR EXPRESSWAYS A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

THE ENGINEER SHALL CONTACT PATRICE HARRIS, AREA TRAFFIC FIELD ENGINEER, AT (708) 597-9800 A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM THE BUREAU OF MAINTENANCE BRIDGE INSPECTORS.

EXISTING PAVEMENT MARKINGS SHALL BE TEMPORARILY COVERED PRIOR TO THE APPLICATION OF THE DECK SURFACE TREATMENT, TO PREVENT THE MATERIAL FROM BEING APPLIED TO THE MARKINGS, THE TEMPORARY COVERING SHALL BE REMOVED AFTER APPLICATIONS OF THE DECK SURFACE TREATMENT AND PRIOR TO OPENING TO TRAFFIC. COST IS INCLUDED WITH THE DECK SURFACE TREATMENT SPECIFIED.

PLAN DIMENSIONS AND DETAIL RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS, SUCH VARIATIONS SHALL NOT BE BECAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF THE WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED BASED AT THE UNITE PRICE BID FOR THE WORK.

CONCRETE SUPERSTRUCTURE SHALL HAVE A SEVEN DAY MINIMUM CURE.

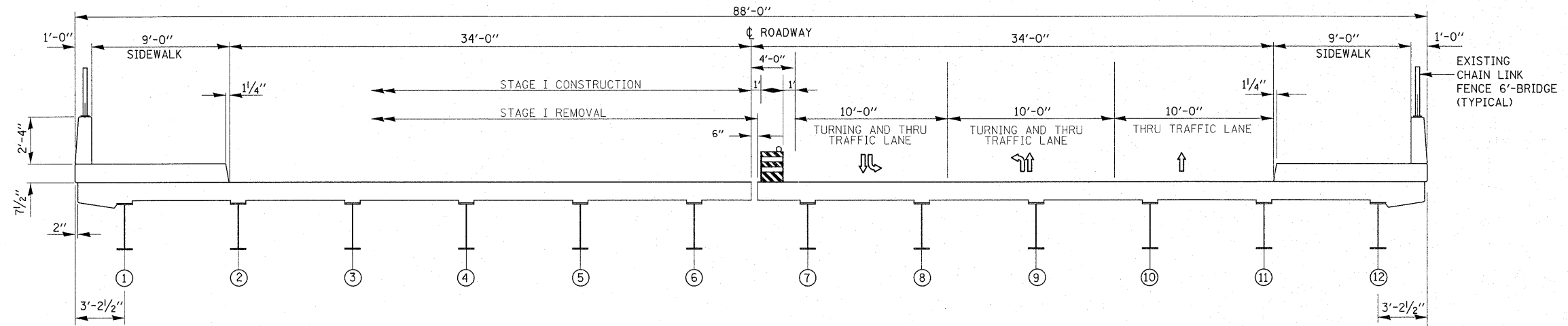
THE "ARTERIAL ROAD INFORMATION SIGN (TC-22)" IS APPLICABLE ONLY TO ARTERIAL ROADS AND SHALL NOT BE APPLIED TO EXPRESSWAYS/TOLLWAYS.

FILE NAME =	USER NAME = patelar	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I- 90/94 (DAN RYAN EXPWY) INDEX OF SHEETS AND STATE STANDARDS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
es:\pw\work\p\patelar\d0281223\01627	l-shr-plan.dgn	DRAWN -	REVISED -			90/94	2011-070-1	COOK	27	2	
	PLOT SCALE = 50,0000' / in.	CHECKED -	REVISED -			SCALE: SHEET NO. OF SHEETS STA. TO STA.		CONTRACT NO. 60PB9			
	PLOT DATE = 11/4/2011	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					

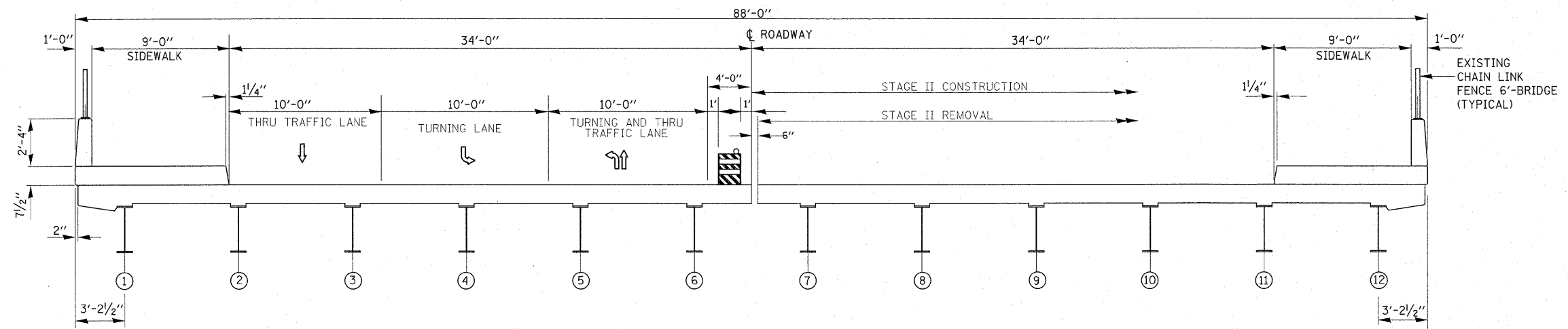
Rev

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0014					CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0014				
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	0.02	0.02					78300100	PAVEMENT MARKING REMOVAL	SO FT	2492	2492				
40600300	AGGREGATE (PRIME COAT)	TON	0.12	0.12					X0326766	CLEAN & RESEAL RELIEF JOINT	FOOT	160	160				
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	5	5					X4400100	PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH)	SO YD	36	36				
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	925	925					X4401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SO YD	22	22				
42400800	DETECTABLE WARNINGS	SO FT	51	51					X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1				
44000600	SIDEWALK REMOVAL	SO FT	925	925					X7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	L SUM	1	1				
50102400	CONCRETE REMOVAL	CU YD	20.7	20.7					X7030025	WET REFLECTIVE TEMPORARY TAPE, TYPE III - LETTERS AND SYMBOLS	SO FT	291.5	291.5				
50157300	PROTECTIVE SHIELD	SO YD	115	115					X7030030	WET REFLECTIVE TEMPORARY TAPE TYPE III, 4 INCH	FOOT	5024	5024				
50300255	CONCRETE SUPERSTRUCTURE	CU YD	20.7	20.7					X7030040	WET REFLECTIVE TEMPORARY TAPE TYPE III, 6 INCH	FOOT	38	38				
50300300	PROTECTIVE COAT	SO YD	2151	2151					X7030050	WET REFLECTIVE TEMPORARY TAPE TYPE III, 12 INCH	FOOT	535	535				
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	940	940					X7030055	WET REFLECTIVE TEMPORARY TAPE TYPE III, 24 INCH	FOOT	88	88				
50800515	BAR SPLICERS	EACH	8	8					Z0001700	APPROACH SLAB REPAIR (FULL DEPTH)	SO YD	6	6				
52000110	PREFORMED JOINT STRIP SEAL	FOOT	180	180					Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	141	141				
60250200	CATCH BASINS TO BE ADJUSTED	EACH	4	4					Z0016200	DECK SLAB REPAIR (PARTIAL)	SO YD	4	4				
60255500	MANHOLES TO BE ADJUSTED	EACH	1	1					Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	51.4	51.4				
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	3	3					X7030045	WET REFLECTIVE TEMPORARY TAPE, TYPE III, 8 INCH	FOOT	640	640				
67100100	MOBILIZATION	L SUM	1	1					B9000200	TEMPORARY TRAFFIC SIGNAL INSTALLATION	L SUM	1	1				
70200100	NIGHTTIME WORK ZONE LIGHTING	L SUM	1	1					Z0073510	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	2	2				
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	3163	3163													
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	66.7	66.7													
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	2018	2018													
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	873	873													
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	950	950													
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	71	71													
* 78008200	POLYUREA PAVEMENT MARKING TYPE I - LETTERS AND SYMBOLS	SO FT	155	155													
* 78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	340	340													
* 78008230	POLYUREA PAVEMENT MARKING TYPE I - LINE 6"	FOOT	272	272													
* 78008250	POLYUREA PAVEMENT MARKING TYPE I - LINE 12"	FOOT	362	362													
* 78008270	POLYUREA PAVEMENT MARKING TYPE I - LINE 24"	FOOT	23	23													

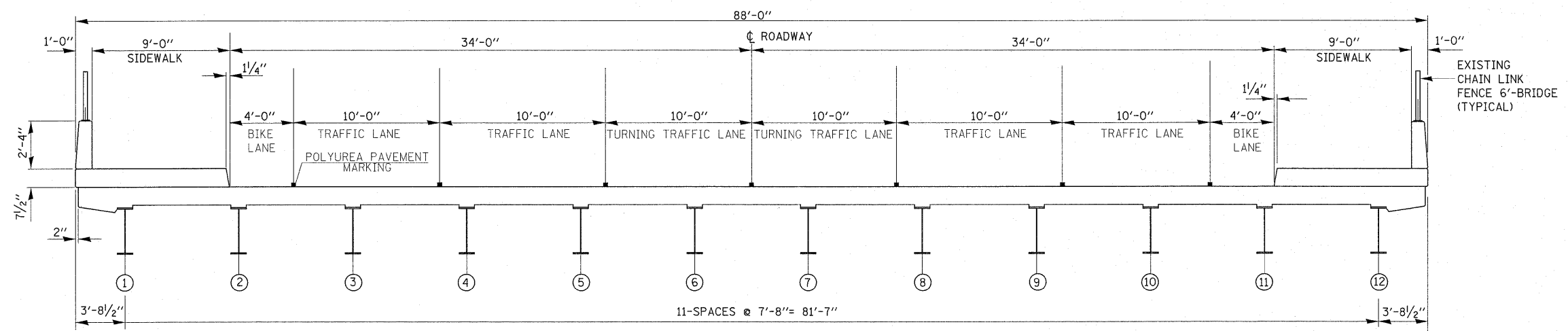
* Specialty Items



STAGE I CONSTRUCTION



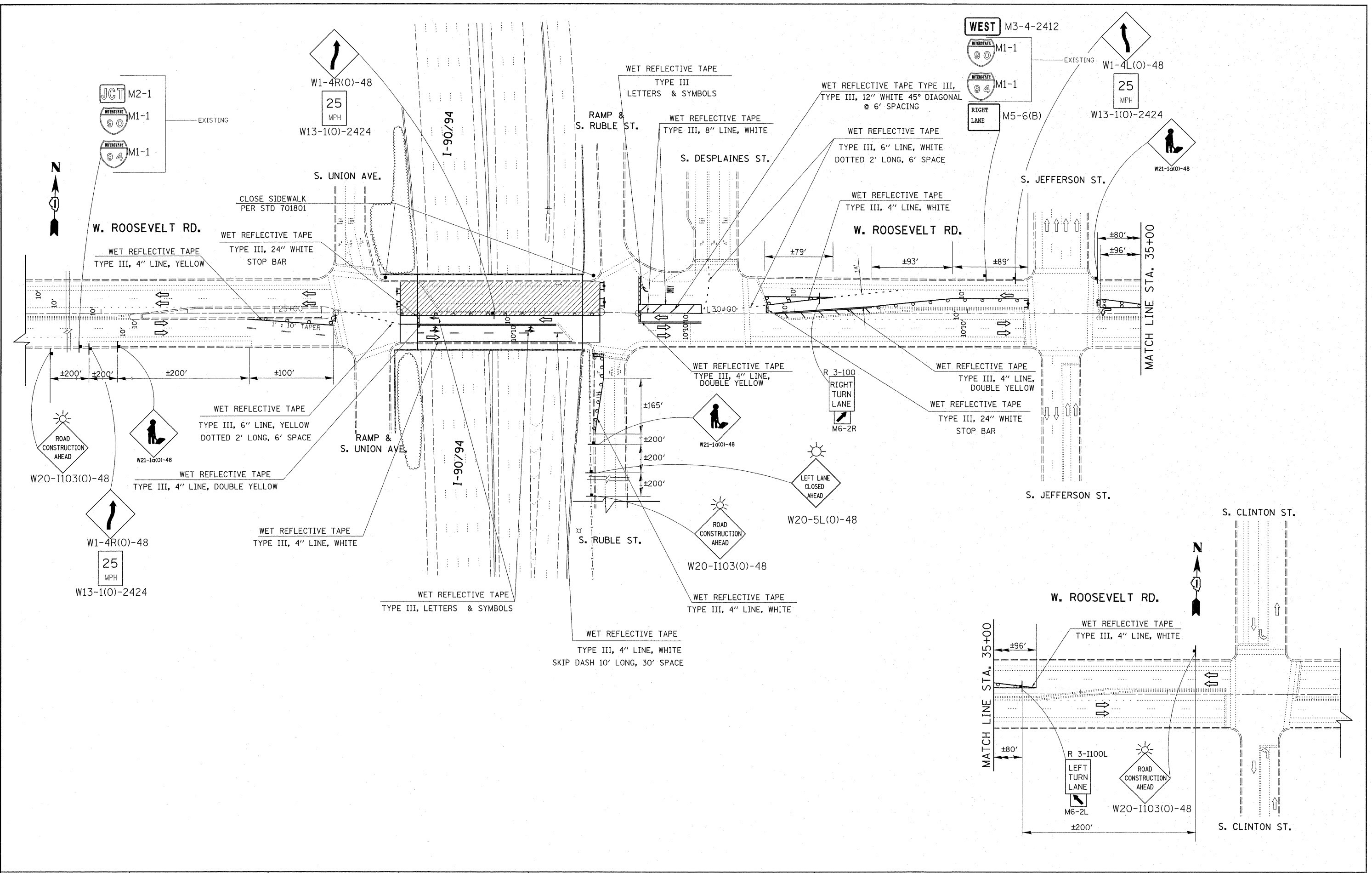
STAGE II CONSTRUCTION



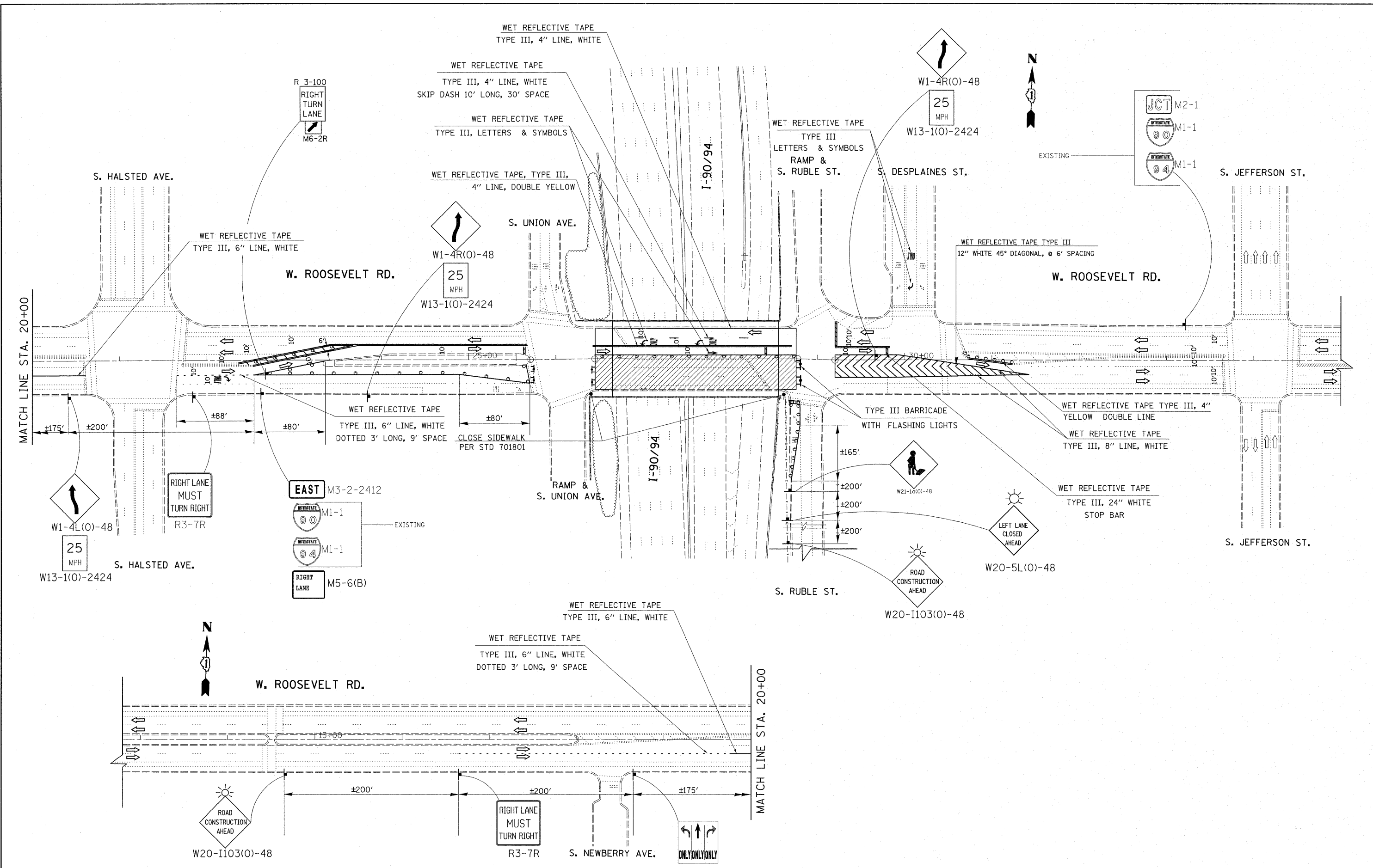
FINAL

ALL VIEWS LOOKING EAST

FILE NAME =	USER NAME = petelar	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-9094 (DAN RYAN EXPRESSWAY) AT ROOSEVELT ROAD TRAFFIC STAGING S.N. 016-0478				F.A.I. RTE. 90/94	SECTION 2011-070-I	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 4
ct:\pw_work\pwidot\petelar\d8281223\0162711-sh-t-plendgn	PLOT SCALE = 50.0000' / in.	DRAWN -	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	CONTRACT NO. 60P89			
	PLOT DATE = 12/21/2011	CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT								
		DATE -	REVISED -										



FILE NAME =	USER NAME = patelar	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	W. ROOSEVELT RD. OVER I-90/94 (DAN RYAN EXPY) TRAFFIC CONTROL PLAN, STAGE I (S.N. 016-0478)	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pwork\pwork\patelar\d0201223\0162711-sh-t-plan.dgn	PLOT SCALE = 50.0000' / in.	DRAWN -	REVISED -			90/94	2011-070-1	COOK	27	5	
	PLOT DATE = 12/28/2011	CHECKED -	REVISED -			CONTRACT NO. 60P89					
		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



FILE NAME =	USER NAME = petalar	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	W. ROOSEVELT RD. OVER I-90/94 (DAN RYAN EXPY) TRAFFIC CONTROL PLAN. STAGE II (S.N. 016-0478)	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pwork\pwidot\petalar\d0281223\161627	11-shr-plan.dgn	DRAWN -	REVISED -			90/94	2011-070-1	COOK	27	6	
	PLOT SCALE = 50.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 60P89					
	PLOT DATE = 12/28/2011	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					
					SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	

PROPOSED PRESENT

- SIGNAL, TRAFFIC 3 SECTION 1-WAY ADJUSTABLE, 12" OR AS NOTED
- SIGNAL, TRAFFIC 3 SECTION 2-WAY ADJUSTABLE, 12" OR AS NOTED
- SIGNAL OPTICALLY PROGRAMMED
- SIGNAL, PEDESTRIAN, DON'T WALK/WALK
- SIGNAL FACE ARROW, 12" COLOR AS NOTED
- SIGNAL FACE, 1 SECTION YELLOW/GREEN ARROW DUAL INDICATION
- PUSH BUTTON, PEDESTRIAN
- SIGN, ILLUMINATED, WITH MESSAGE OR SYMBOL AS INDICATED
- MAST ARM, MONOTUBE, STEEL. SIZE AS INDICATED (SEE DWG. #870)
- MAST ARM, TRUSS, ALUMINUM. SIZE AS INDICATED
- CONTROLLER, TRAFFIC SIGNAL. PEDESTAL OR BASE MOUNTED AS INDICATED
- CONTROLLER, STREET LIGHTING. PEDESTAL OR BASE MOUNTED. (DWG. 876 or 880)
- CONTROLLER, STREET LIGHTING. POLE MOUNTED (DWG. #11940)
- POLE, WOOD. COMMONWEALTH EDISON COMPANY. SERVICE
- POLE, CITY STEEL, ANCHOR BASE, 34'-6", 7 GA., 10" DI A. AND 15" B.C. 24"X7" FND. W/1 1/4" ANCHOR RODS DRG. #818.
- POLE, CITY STEEL, ANCHOR BASE, 34'-6", 3 GA., 10" DIA. AND 15" B.C. 24"X9" FND. W/1 1/4" ANCHOR RODS DRG. #818 (16", 20" or 26" M.A.)
- POLE, CITY STEEL, ANCHOR BASE, 34'-6", 3 GA., 11" DIA. AND 17 1/4" B.C. 30"X9" FND. W/1 1/4" ANCHOR RODS DRG. #816. (30" M.A.)
- POLE, CITY STEEL, ANCHOR BASE 34'-6", 3 GA., 12 1/2" DIA. AND 16 1/2" B.C. 30"X11" FND. W/1 1/2" ANCHOR RODS DRG. #817. (35", 40" or 44" M.A.)
- POLE, CITY STEEL, ANCHOR BASE, 32'-6", 3 GA., 10" DIA., WITH 3 GA. BAL. HSG. BASE AND 17 1/4" B.C. ON 30"X9" FND. W/ 1 1/4" ANCHOR RODS DRG. #816.
- POLE, CITY STEEL, ANCHOR BASE, 20', 27'-6", 29'-6", 7 GA. WITH STEEL BAL. HSG. BASE AND FND. W/10" D. B.C. AND 1" ANCHOR RODS DRG. #716.
- POLE, CITY STEEL, ANCHOR BASE, 20', 27'-6", 29'-6", 3 GA., WITH STEEL BAL. HSG. BASE AND FND. W/10" D. B.C. AND 1" ANCHOR RODS DRG. #719.
- POLE, CITY STEEL, ANCHOR BASE, 20', 27'-6", 29'-6" 7 GA., AND FND. WITH 10" B.C. AND 1" ANCHOR RODS DRG. #11408B.
- POLE, CITY STEEL, ANCHOR BASE, 20', 27'-6", 29'-6" 3 GA., AND FND. WITH 10" B.C. AND 1" ANCHOR RODS DRG. #11408B.
- POLE, CITY STEEL, ANCHOR BASE, 32'-6", 7 GA., AND FND. WITH 11 1/2" B.C. AND 1" ANCHOR RODS DRG. #753.
- POLE, CITY STEEL, ANCHOR BASE, 32'-6", 3 GA., AND FND. WITH 11 1/2" B.C. AND 1" ANCHOR RODS DRG. #753.
- POLE, CITY STEEL, ANCHOR BASE, 32'-6" 7 GA., ALUM. BHB AND FND. WITH 15" B.C. 24"X7" WITH 1" ANCHOR RODS DRG. #691.
- POLE, CITY STEEL, ANCHOR BASE, 32'-6", 3 GA., ALUM. BHB AND FND. WITH 15" B.C. 24"X7" WITH 1" ANCHOR RODS DRG. #691.
- POLE, CITY ALUMINUM, WITH ROUND BAL. HSG. BASE, 25', 28', or 30' ON FND. WITH 14" B.C., ACQUIRED FROM CHICAGO PARK DISTRICT.
- POLE, CITY STEEL, EMBEDDED, 4"X 9"X 35' 7 GA., TAPERED TUBULAR. (DWG. #658)
- POLE, CITY STEEL, EMBEDDED, 4"X 9"X 35' 3 GA., TAPERED TUBULAR. (DWG. #658)
- POLE, CITY STEEL, EMBEDDED. (ACQUIRED FROM CTA)
- COLUMN, ELEVATED STRUCTURE
- POLE, WOOD. (SIZE AS NOTED)
- POLE, FOUNDATION WITH ELBOWS AS INDICATED. (SIZE AS NOTED)
- POLE, ORNAMENTAL OR OTHER, AS INDICATED ON THE PLANS

PROPOSED PRESENT

- HANDHOLE, HEAVY DUTY, 36" I.D. (DWG.#866)
- HANDHOLE, CIRCULAR WITH 24" FRAME & COVER, 30" I.D. (#867)
- MANHOLE, CITY 3'X4'X4' DWG. #729 or 730; 4'X6'X6' DWG. #732 or 733.
- FOUNDATION, CONTROLLER OR PEDESTAL, 13" B.C. 20"X5' (DWG. #709)
- FOUNDATION, TRAFFIC CONTROLLER DWG. #854. F.A. TERMINAL FND. DWG. #11972
- FOUNDATION, TRAFFIC TYPE "P", BASE MOUNT. (DWG. #888)
- FOUNDATION, CONTROLLER STREET LIGHT, SPECIAL, 100A & 200A. (DWG.#876 & # 880)
- FOUNDATION, TRANSCLOSURE; TRANSCLOSURE HOUSING. (DWG.# 583 & #891)
- CONTROLLER, UNDERPASS LIGHTING 120V. & 240V. (DWG. #860 & #861)
- MANHOLE, UTILITY, E=COMMONWEALTH EDISON; T=ILL. BELL TEL.; G=PEOPLES GAS; W=CITY WATER; P=CHGO PARK DISTRICT; CTA=C.T.A.; S=SEWER
- JUNCTION BOX, IN PAVEMENT (DWG. #815)
- DETECTOR LOOP IN PAVEMENT
- CONDUIT or P.V.C., NUMBER, SIZE & TYPE. (AS NOTED)
- CONDUIT or P.V.C. ENCASED IN CONCRETE. (SECTION or NUMBER OF CONDUIT INDICATED)
- LUMINAIRE, H.P.S.V. 400W LAMP, 240V, SEMI-CUTOFF
- LUMINAIRE, H.P.S.V. 400W LAMP, 240V, CUTOFF
- LUMINAIRE, H.P.S.V. 310W LAMP, 240V
- LUMINAIRE, H.P.S.V. 310W LAMP 240V, CUTOFF
- LUMINAIRE, H.P.S.V. 150W LAMP, 240V
- LUMINAIRE, H.P.S.V. 150W LAMP, 120V
- LUMINAIRE, H.P.S.V. 250W LAMP, 120V. (ALLEY LIGHT)
- LUMINAIRE, H.P.S.V. 250W LAMP, 120V
- TERMINAL, CABINET F.A. & P.C.
- FIRE ALARM BOX, MOUNTED
- FIRE ALARM BOX, POLE MOUNTED
- CABLE, TRAFFIC SIGNAL, COMMUNICATION, 1-PAIR #14 SHIELDED, IN CONDUIT
- CABLE, TRAFFIC SIGNAL POWER SUPPLY, 2/C-#4, 600 V. EPR. IN CONDUIT
- CABLE, TRAFFIC SIGNAL POWER SUPPLY, 2 1/C-#2 or #1/0 600V. EPR IN CONDUIT
- CABLE, TRAFFIC SIGNAL POWER SUPPLY, 2/C-#10 or #6, 600V NSRI, IN CONDUIT
- CABLE, TRAFFIC SIGNAL, 7/C-#12 or #14, 600V, EPR IN CONDUIT
- CABLE, TRAFFIC SIGNAL, 10/C-#12 600V. EPR IN CONDUIT
- CABLE, TRAFFIC SIGNAL, 14/C-#14, 600V. EPR IN CONDUIT
- CABLE, TRAFFIC SIGNAL, 19/C-#12 600V, EPR IN CONDUIT
- CABLE, STREET LIGHT, 2 1/C-#6, 600V. RINS IN PARKWAY
- CABLE, STREET LIGHT, 2 1/C-#6, 600V. RINS IN CONDUIT
- CABLE, STREET LIGHT, 2 1/C-#6 EPRN 600V. & 1 1/C-#8 GREEN, TRIPLEXED, IN CONDUIT
- CABLE, STREET LIGHT, 3 1/C-#1/0, or #2/0, or #4, 600V. EPR IN CONDUIT
- WIRE, STREET LIGHT, 2 1/C-#6, HDNS. AERIAL
- WIRE, STREET LIGHT, 2 1/C-#6 & 1 1/C #8, HDNS. AERIAL
- CABLE, STREET LIGHT AERIAL, 3 1/C-#4 or #2 SELF SUPPORTING, 600V EPR
- WIRE, F.A. & P.C. AERIAL, 1/C-#10, NUMERAL DENOTES QUANTITY
- CABLE, F.A. & P.C. AERIAL, W/ MESSENGER #19-(NUMBER OF PAIRS AS INDICATED)
- CABLE, F.A. & P.C. AERIAL, SELF SUPPORTING, #19-(NUMBER OF PAIRS AS INDICATED)
- CABLE, F.A. & P.C., IN CONDUIT, #19-(NUMBER OF PAIRS AS INDICATED)
- DOWNLIGHT ASSEMBLY. (DWG. #850)
- LIGHT, TRAFFIC SAFETY ISLAND
- FLASHING BEACON & DOWNLIGHT

ORNAMENTAL LUMINAIRES

PROPOSED	EXISTING	
		310W PENDANT (240V)
		400W PENDANT (240V)
		250W PENDANT (240V)
		150W ACORN (120V)
		150W ACORN (240V)
		50W ACORN (240V)
		100W ACORN (240V)
		150W GLOBE (240V)
		100W GLOBE (240V)
		50W GLOBE (240V)

C 04-01-02	REVISED/REDRAW	R. POOL/B. I.
B 12-4-01	ADDED ORNAMENTAL SYMBOLS	
A 8-6-96	REDRAWN	
DATE	REVISION	
STANDARD CODE FOR TRAFFIC SIGNALS/ STREET LIGHTING		
CITY OF CHICAGO DEPT. OF STREETS AND SANITATION BUREAU OF ELECTRICITY DIVISION OF ELECTRICAL ENGINEERING		
DRAFTSMAN: R. IVY	CHIEF DRAFTSMAN: R. CARTER	ENGINEER: R. POOL/R.C/W.T.
SUPERVISING ENGINEER:	ELEC. DESIGN ENGR.	DWG. NO.
ENGINEER OF ELECTRICITY:		826
GEN'L. Supt. OF CONSTRUCTION:		
DEPUTY COMMISSIONER:		
SIZE: 22" X 36"	SCALE:	DATE:

FILE NAME: J:\2\9\00\CHDD Sheets\016\099_151.dgn

SINGH
SINGH & ASSOCIATES, INC.
CONSULTING ENGINEERS

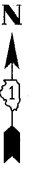
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	DRAWN - VP	REVISED -
PLOT SCALE = 5.0000 "/ in.	CHECKED - IY	REVISED -
PLOT DATE = 11/30/2011	DATE - 11/29/2011	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

W. ROOSEVELT RD. OVER I-90/94 (DAN RYAN EXPY)
CITY OF CHICAGO DOT-DEO
STANDARD DRAWINGS

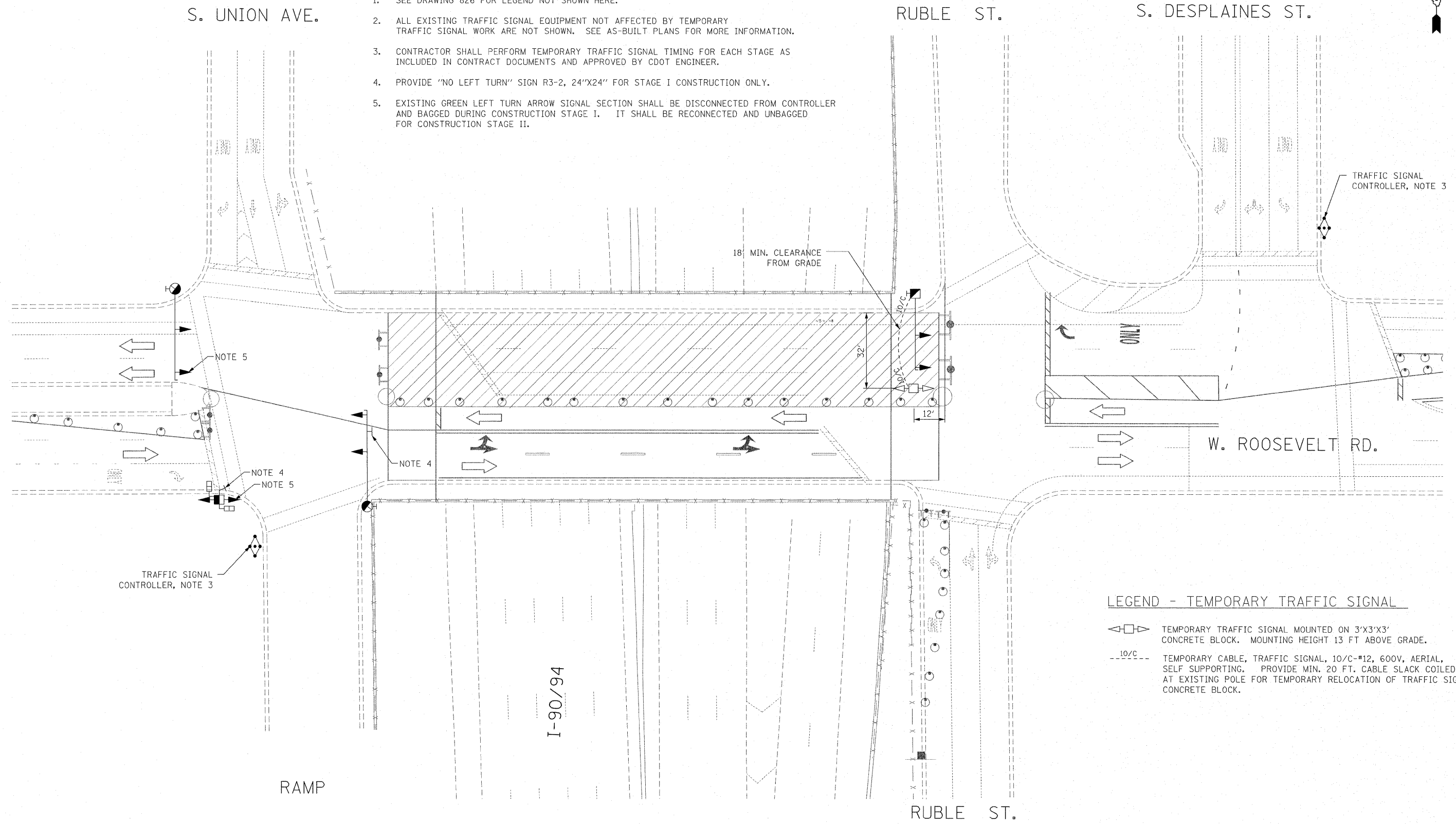
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2011-070-1	COOK	27	6A
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60P89	

SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.



NOTES:

1. SEE DRAWING 826 FOR LEGEND NOT SHOWN HERE.
2. ALL EXISTING TRAFFIC SIGNAL EQUIPMENT NOT AFFECTED BY TEMPORARY TRAFFIC SIGNAL WORK ARE NOT SHOWN. SEE AS-BUILT PLANS FOR MORE INFORMATION.
3. CONTRACTOR SHALL PERFORM TEMPORARY TRAFFIC SIGNAL TIMING FOR EACH STAGE AS INCLUDED IN CONTRACT DOCUMENTS AND APPROVED BY CDOT ENGINEER.
4. PROVIDE "NO LEFT TURN" SIGN R3-2, 24"X24" FOR STAGE I CONSTRUCTION ONLY.
5. EXISTING GREEN LEFT TURN ARROW SIGNAL SECTION SHALL BE DISCONNECTED FROM CONTROLLER AND BAGGED DURING CONSTRUCTION STAGE I. IT SHALL BE RECONNECTED AND UNBAGGED FOR CONSTRUCTION STAGE II.



LEGEND - TEMPORARY TRAFFIC SIGNAL

- TEMPORARY TRAFFIC SIGNAL MOUNTED ON 3'X3'X3' CONCRETE BLOCK. MOUNTING HEIGHT 13 FT ABOVE GRADE.
- TEMPORARY CABLE, TRAFFIC SIGNAL, 10/C-#12, 600V, AERIAL, SELF SUPPORTING. PROVIDE MIN. 20 FT. CABLE SLACK COILED AT EXISTING POLE FOR TEMPORARY RELOCATION OF TRAFFIC SIGNAL CONCRETE BLOCK.

FILE NAME = J:\2011\CDOT\CDOT Sheets\0168899_1TSD.dgn

SINGH 300 W. ADAMS ST.
CHICAGO, IL 60606
SINGH & ASSOCIATES, INC. TEL: (312) 625-0240
CONSULTING ENGINEERS FAX: (312) 625-8449

USER NAME = kpraJapota	DESIGNED - KP	REVISED -
PLOT SCALE = 20.0000' / 1" =	DRAWN - VP	REVISED -
PLOT DATE = 11/30/2011	CHECKED - IY	REVISED -
	DATE - 11/29/2011	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**W. ROOSEVELT RD. OVER I-90/94 (DAN RYAN EXPY)
TEMPORARY TRAFFIC SIGNAL INSTALLATION
STAGE I**

SCALE: 1"=20' SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2011-070-1	COOK	27	63
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60P89	

NOTES:

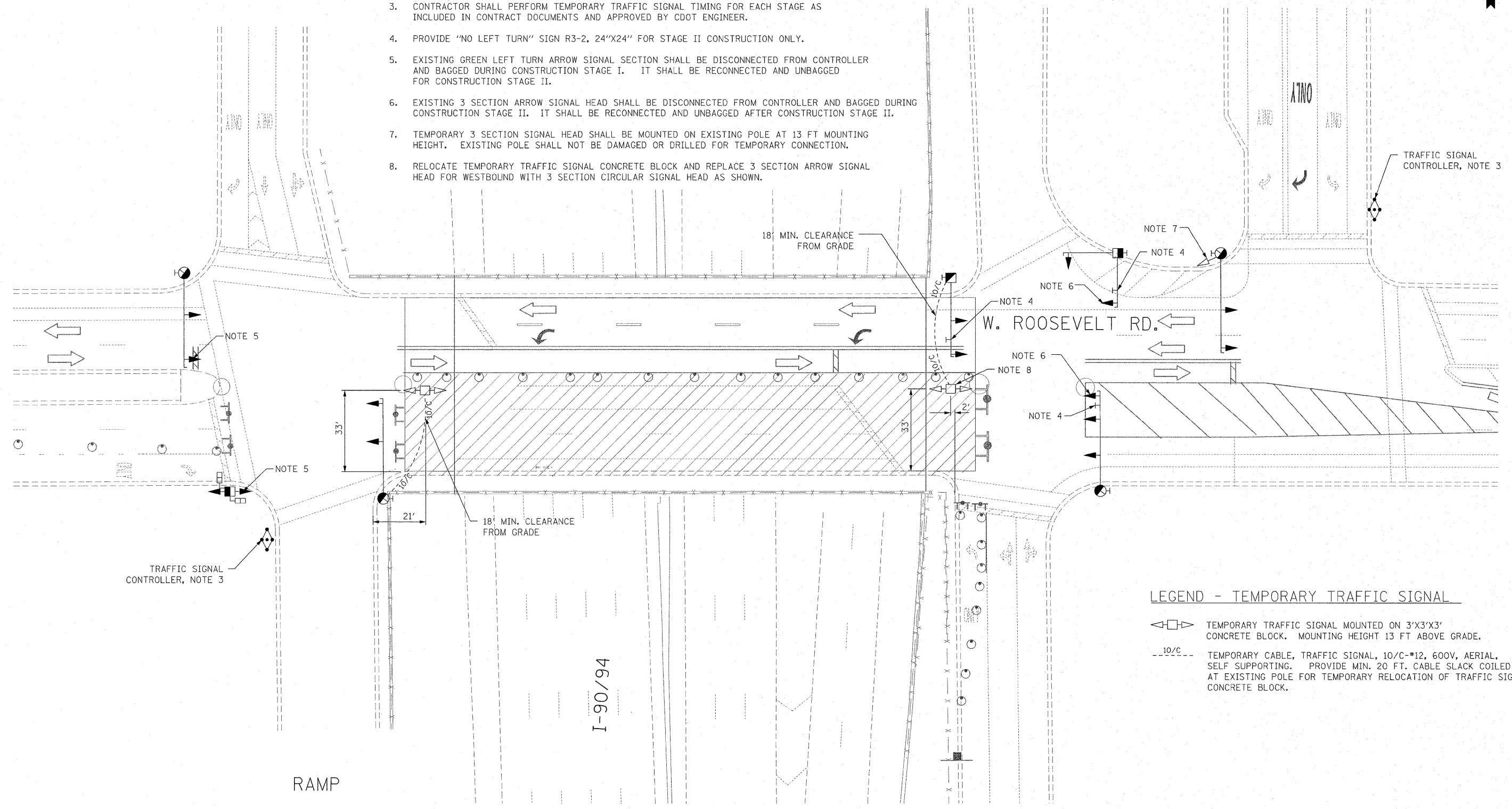
1. SEE DRAWING 826 FOR LEGEND NOT SHOWN HERE.
2. ALL EXISTING TRAFFIC SIGNAL EQUIPMENT NOT AFFECTED BY TEMPORARY TRAFFIC SIGNAL WORK ARE NOT SHOWN. SEE AS-BUILT PLANS FOR MORE INFORMATION.
3. CONTRACTOR SHALL PERFORM TEMPORARY TRAFFIC SIGNAL TIMING FOR EACH STAGE AS INCLUDED IN CONTRACT DOCUMENTS AND APPROVED BY CDOT ENGINEER.
4. PROVIDE "NO LEFT TURN" SIGN R3-2, 24"X24" FOR STAGE II CONSTRUCTION ONLY.
5. EXISTING GREEN LEFT TURN ARROW SIGNAL SECTION SHALL BE DISCONNECTED FROM CONTROLLER AND BAGGED DURING CONSTRUCTION STAGE I. IT SHALL BE RECONNECTED AND UNBAGGED FOR CONSTRUCTION STAGE II.
6. EXISTING 3 SECTION ARROW SIGNAL HEAD SHALL BE DISCONNECTED FROM CONTROLLER AND BAGGED DURING CONSTRUCTION STAGE II. IT SHALL BE RECONNECTED AND UNBAGGED AFTER CONSTRUCTION STAGE II.
7. TEMPORARY 3 SECTION SIGNAL HEAD SHALL BE MOUNTED ON EXISTING POLE AT 13 FT MOUNTING HEIGHT. EXISTING POLE SHALL NOT BE DAMAGED OR DRILLED FOR TEMPORARY CONNECTION.
8. RELOCATE TEMPORARY TRAFFIC SIGNAL CONCRETE BLOCK AND REPLACE 3 SECTION ARROW SIGNAL HEAD FOR WESTBOUND WITH 3 SECTION CIRCULAR SIGNAL HEAD AS SHOWN.



S. UNION AVE.

RUBLE ST.

S. DESPLAINES ST.



LEGEND - TEMPORARY TRAFFIC SIGNAL

- TEMPORARY TRAFFIC SIGNAL MOUNTED ON 3'X3'X3' CONCRETE BLOCK. MOUNTING HEIGHT 13 FT ABOVE GRADE.
- TEMPORARY CABLE, TRAFFIC SIGNAL, 10/C-#12, 600V, AERIAL, SELF SUPPORTING. PROVIDE MIN. 20 FT. CABLE SLACK COILED AT EXISTING POLE FOR TEMPORARY RELOCATION OF TRAFFIC SIGNAL CONCRETE BLOCK.

FILE NAME = J:\299\CDOT\CADD Sheets\1068P89.TSD.dgn

SINGH 300 W. ADAMS ST.
CHICAGO, IL 60606
TEL: (312) 629-0240
FAX: (312) 629-8449
CONSULTING ENGINEERS

USER NAME = vpelegrinas	DESIGNED - KP	REVISED -
PLOT SCALE = 20.0000' / 1" =	DRAWN - VP	REVISED -
PLOT DATE = 12/5/2011	CHECKED - IY	REVISED -
	DATE - 11/29/2011	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**W. ROOSEVELT RD. OVER I-90/94 (DAN RYAN EXPY)
TEMPORARY TRAFFIC SIGNAL INSTALLATION
STAGE II**

SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE. 90/94	SECTION 2011-070-1	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 8C
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60P89	

NOTES:

1. SEE DRAWING 826 FOR LEGEND NOT SHOWN HERE.
2. ALL EXISTING TRAFFIC SIGNAL EQUIPMENT NOT AFFECTED BY TEMPORARY TRAFFIC SIGNAL WORK ARE NOT SHOWN. SEE AS-BUILT PLANS FOR MORE INFORMATION.
3. CONTRACTOR SHALL PERFORM TEMPORARY TRAFFIC SIGNAL TIMING FOR EACH STAGE AS INCLUDED IN CONTRACT DOCUMENTS AND APPROVED BY CDOT ENGINEER.
4. PROVIDE "NO LEFT TURN" SIGN R3-2, 24"x24" FOR STAGE II CONSTRUCTION ONLY.
5. EXISTING GREEN LEFT TURN ARROW SIGNAL SECTION SHALL BE DISCONNECTED FROM CONTROLLER AND BAGGED DURING CONSTRUCTION STAGE I. IT SHALL BE RECONNECTED AND UNBAGGED FOR CONSTRUCTION STAGE II.
6. EXISTING 3 SECTION ARROW SIGNAL HEAD SHALL BE DISCONNECTED FROM CONTROLLER AND BAGGED DURING CONSTRUCTION STAGE II. IT SHALL BE RECONNECTED AND UNBAGGED AFTER CONSTRUCTION STAGE II.
7. TEMPORARY 3 SECTION SIGNAL HEAD SHALL BE MOUNTED ON EXISTING POLE AT 13 FT MOUNTING HEIGHT. EXISTING POLE SHALL NOT BE DAMAGED OR DRILLED FOR TEMPORARY CONNECTION.
8. RELOCATE TEMPORARY TRAFFIC SIGNAL CONCRETE BLOCK AND REPLACE 3 SECTION ARROW SIGNAL HEAD FOR WESTBOUND WITH 3 SECTION CIRCULAR SIGNAL HEAD AS SHOWN.
9. THE TEMPORARY TRAFFIC SIGNAL HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER.
10. EXISTING WIRING AND CONNECTION SHOWN HERE ARE BASED ON AVAILABLE AS-BUILT INFORMATION. CONTRACTOR SHALL VERIFY EXISTING CONDITION IN FIELD PRIOR TO ORDERING MATERIALS.
11. ALL SIGNAL HEADS ARE 3 SECTION WITH CIRCULAR RED, YELLOW AND GREEN SECTIONS, UNLESS NOTED OTHERWISE.
12. PROVIDE "NO LEFT TURN" SIGN R3-2, 24"x24" FOR STAGE I CONSTRUCTION ONLY.
13. EXISTING SIGN SHALL BE DISCONNECTED AT CONTROLLER AND BAGGED FOR CONSTRUCTION STAGE II ONLY.

- (A) R3-2 "NO LEFT TURN", 24"x24", NOTE 12.
- (B) R3-2 "NO LEFT TURN", 24"x24", NOTE 4.
- (C) "LEFT TURN ON ARROW ONLY" SIGN, NOTE 13.

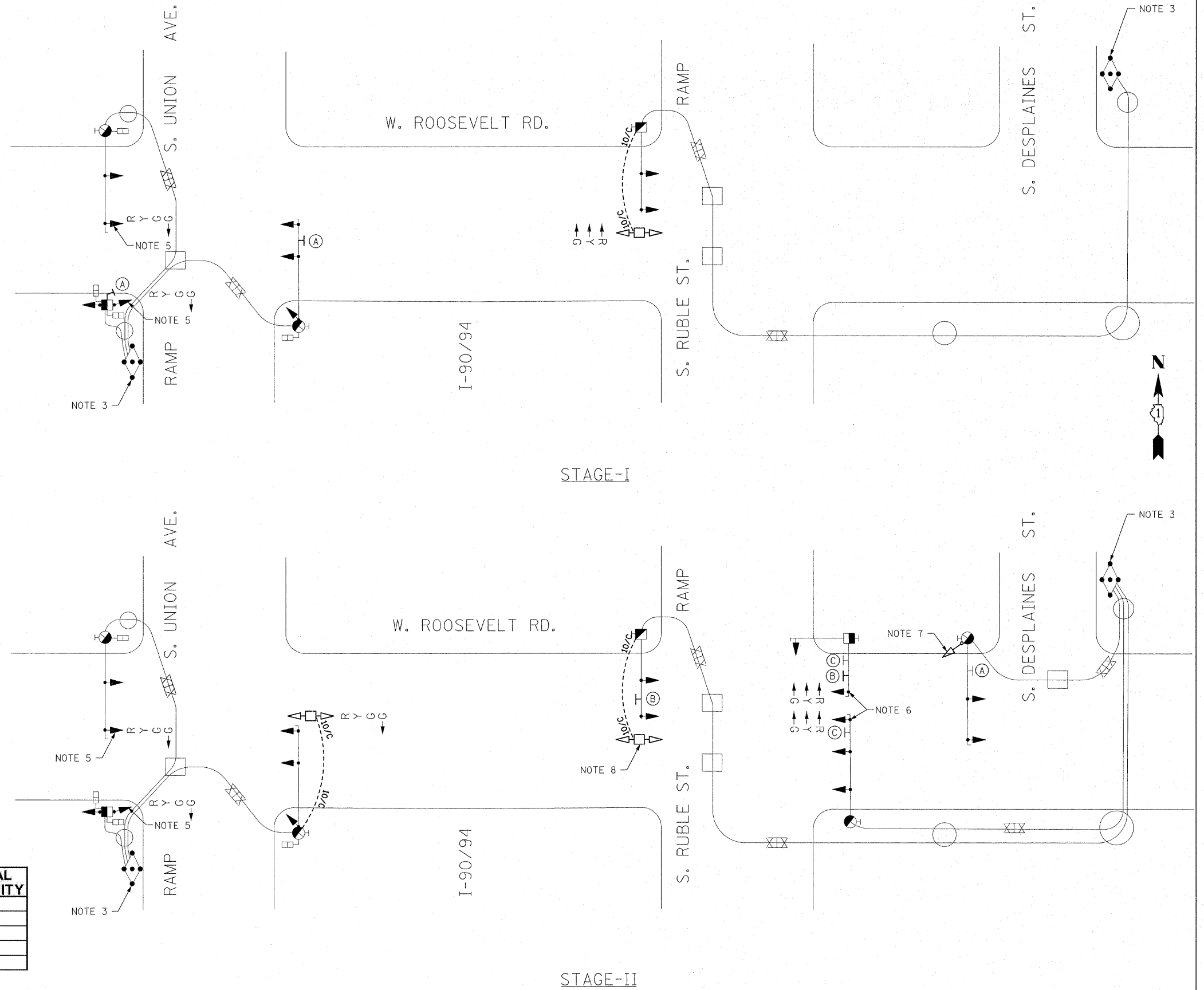
QUANTITY ESTIMATE FOR TEMPORARY TRAFFIC SIGNAL INSTALLATION

QUANTITY	UNIT	DESCRIPTION
200	FOOT	ELECTRIC CABLE AERIAL SUSPENDED, SIGNAL, NO. 12 7C
2	EACH	CONCRETE BLOCK MOUNTED PORTABLE POST
5	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED
1	EACH	SIGNAL HEAD, LED, 1-FACE, 4-SECTION, BRACKET MOUNTED
4	EACH	SIGN, R3-2 "NO LEFT TURN" 24"x24"

SCHEDULE OF QUANTITIES

CODE NO.	ITEM	UNIT	TOTAL QUANTITY
* 89000200	TEMPORARY TRAFFIC SIGNAL INSTALLATION	L SUM	1
* 20073510	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	2

* INDICATES SPECIAL PROVISION



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SINGH
SINGH & ASSOCIATES, INC.
CONSULTING ENGINEERS

300 W. ADAMS ST.
CHICAGO, IL 60606
TEL: (312) 629-0240
FAX: (312) 629-8449

USER NAME = vpalegrmas
DESIGNED - KP
DRAWN - VP
CHECKED - IY
DATE - 11/29/2011

REVISED -
REVISED -
REVISED -
REVISED -

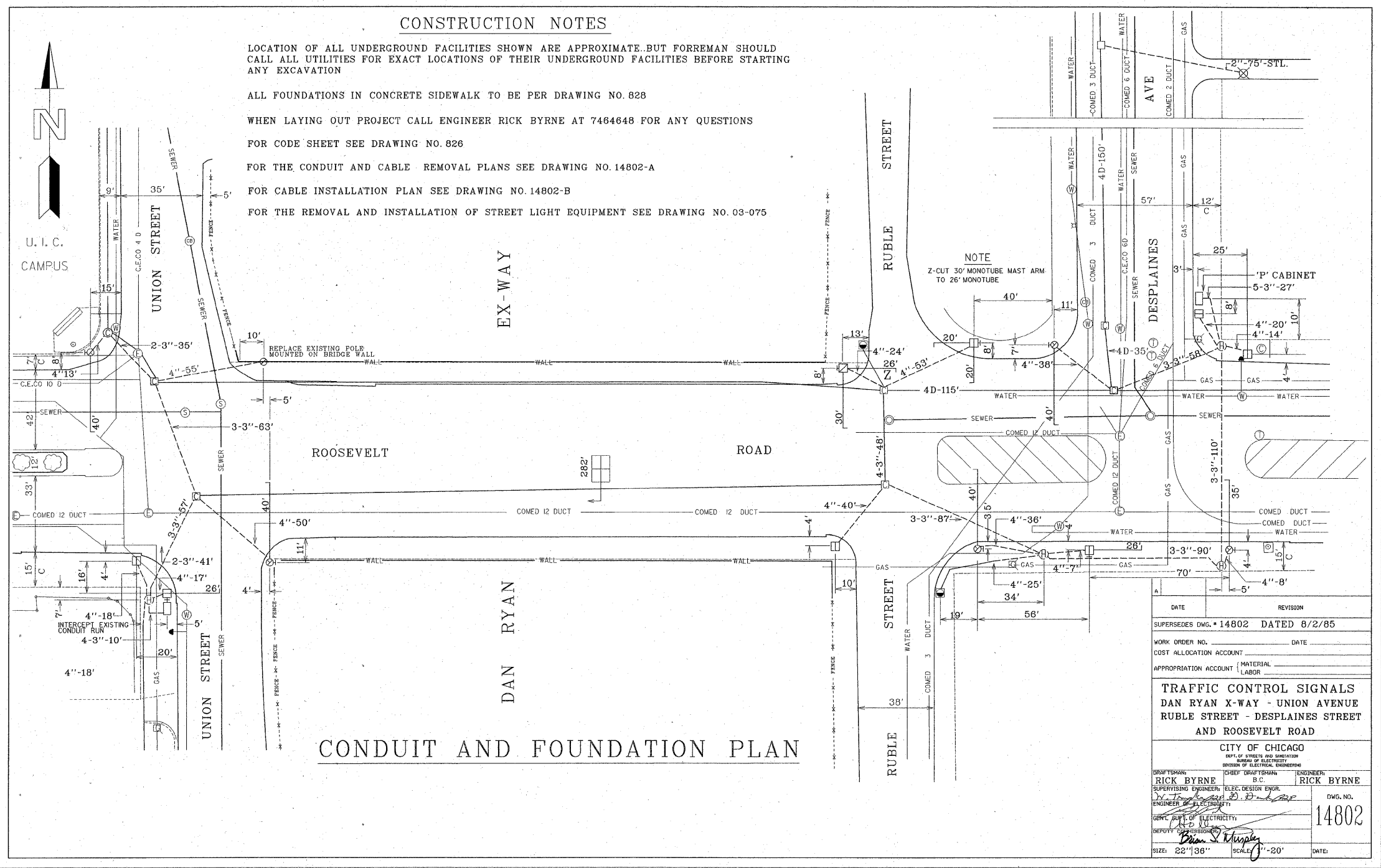
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**W. ROOSEVELT RD. OVER I-90/94 (DAN RYAN EXPY)
TEMPORARY TRAFFIC SIGNAL CABLE DIAGRAM
(STAGES I & II)**

SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2011-070-1	COOK	27	60
CONTRACT NO. 60P89			ILLINOIS FED. AID PROJECT	

FOR REFERENCE ONLY



FILE NAME = J:\2010\CDN\CADD Sheets\0168099_155.dgn

SINGH
SINGH & ASSOCIATES, INC.
CONSULTING ENGINEERS

300 W. ADAMS ST.
CHICAGO, IL 60606
TEL: (312) 629-0240
FAX: (312) 629-8449

USER NAME = kproje@cts	DESIGNED - KP	REVISED -
PLOT SCALE = 5.0000' / in.	DRAWN - VP	REVISED -
PLOT DATE = 11/30/2011	CHECKED - IY	REVISED -
	DATE - 11/29/2011	REVISED -

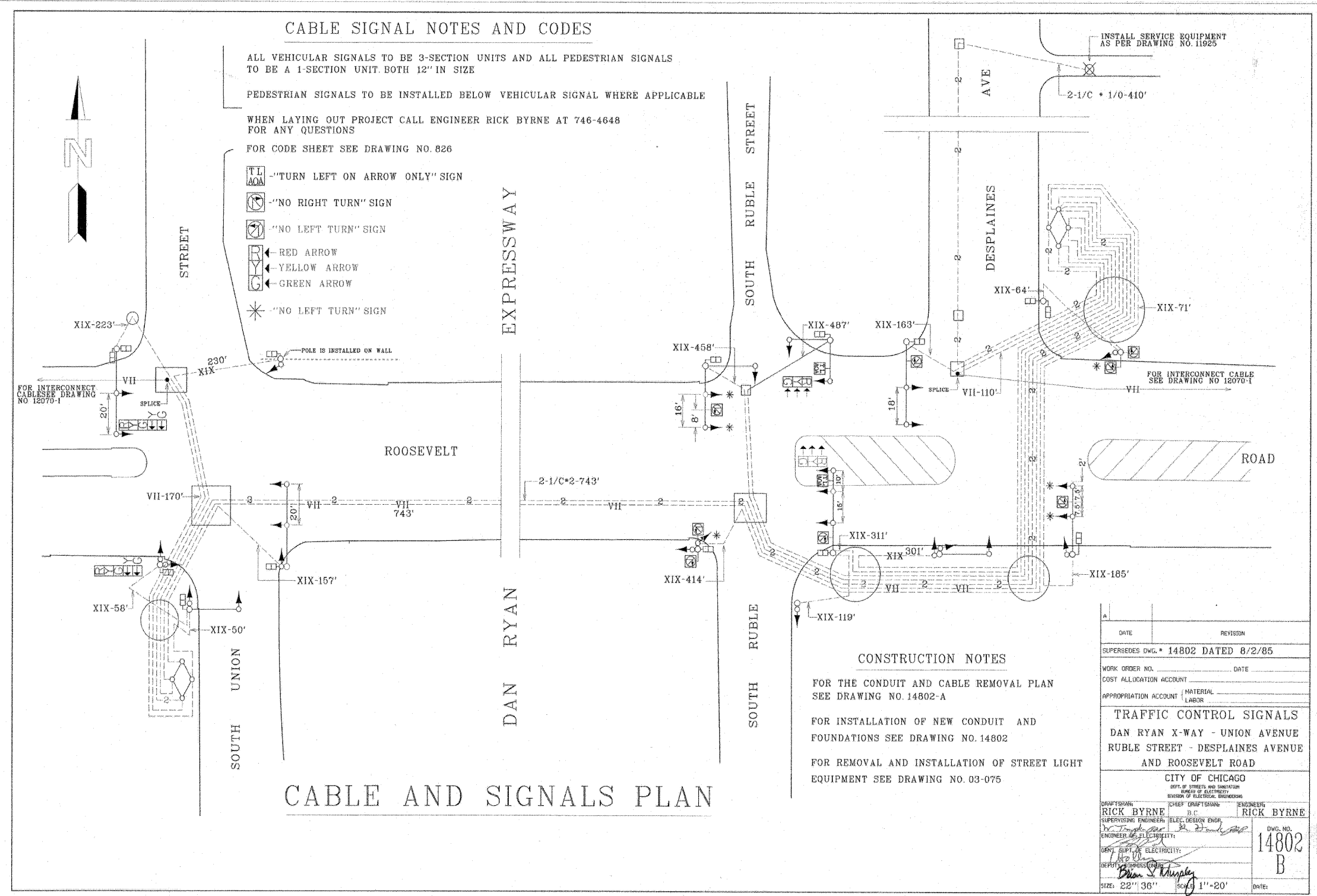
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

W. ROOSEVELT RD. OVER I-90/94 (DAN RYAN EXPY)
AS BUILT CONDUIT AND FOUNDATION PLAN
(FOR REFERENCE ONLY)

SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE. 90/94	SECTION 2011-070-1	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 6E
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60P89	

FOR REFERENCE ONLY



DATE	REVISION
SUPERSEDES DWG. # 14802 DATED 8/2/85	
WORK ORDER NO.	DATE
COST ALLOCATION ACCOUNT	
APPROPRIATION ACCOUNT	MATERIAL
	LABOR
TRAFFIC CONTROL SIGNALS	
DAN RYAN X-WAY - UNION AVENUE	
RUBLE STREET - DESPLAINES AVENUE	
AND ROOSEVELT ROAD	
CITY OF CHICAGO	
DEPT. OF STREETS AND SANITATION	
BUREAU OF ELECTRICITY	
DIVISION OF ELECTRICAL ENGINEERING	
DESIGNED BY RICK BYRNE	CHEF DRAFTSMAN RICK BYRNE
SUPERVISING ENGINEER ELEC. DESIGN ENGR.	ENGINEER ELEC. DESIGN ENGR.
ENGINEER OF ELECTRICITY	DWG. NO. 14802
DEPT. CHIEF OF ELECTRICITY	B
DEPUTY CHIEF OF ELECTRICITY	DATE:
SIZE: 22" x 36"	SCALE: 1" = 20'

FILE NAME = J:\299\CD\ACAD\ Sheets\14802\F1_TSG.dgn

SINGH 300 W. ADAMS ST.
CHICAGO, IL 60606
TEL: (312) 629-0240
FAX: (312) 629-8449
CONSULTING ENGINEERS

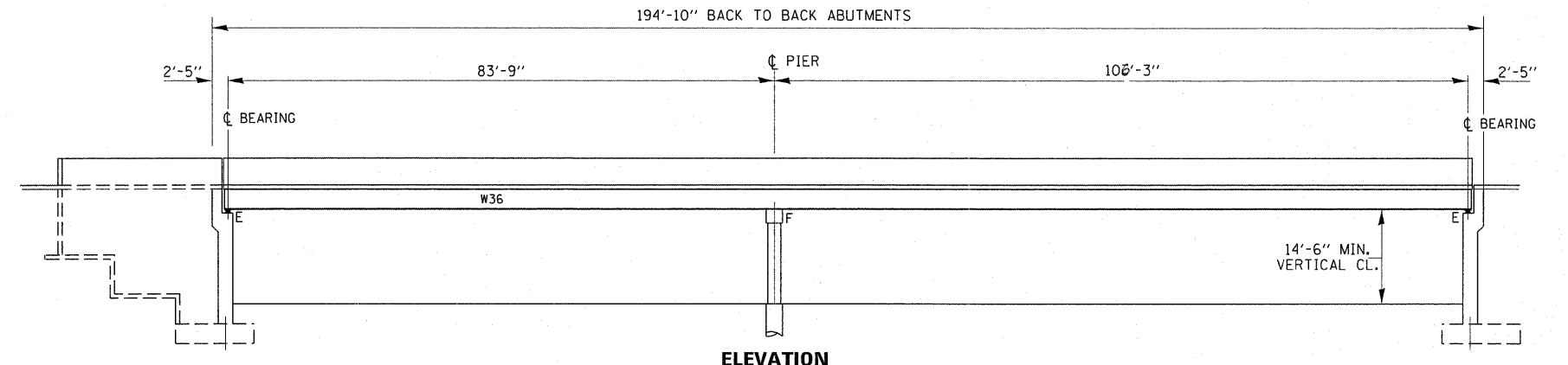
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	DATE - 11/29/2011	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

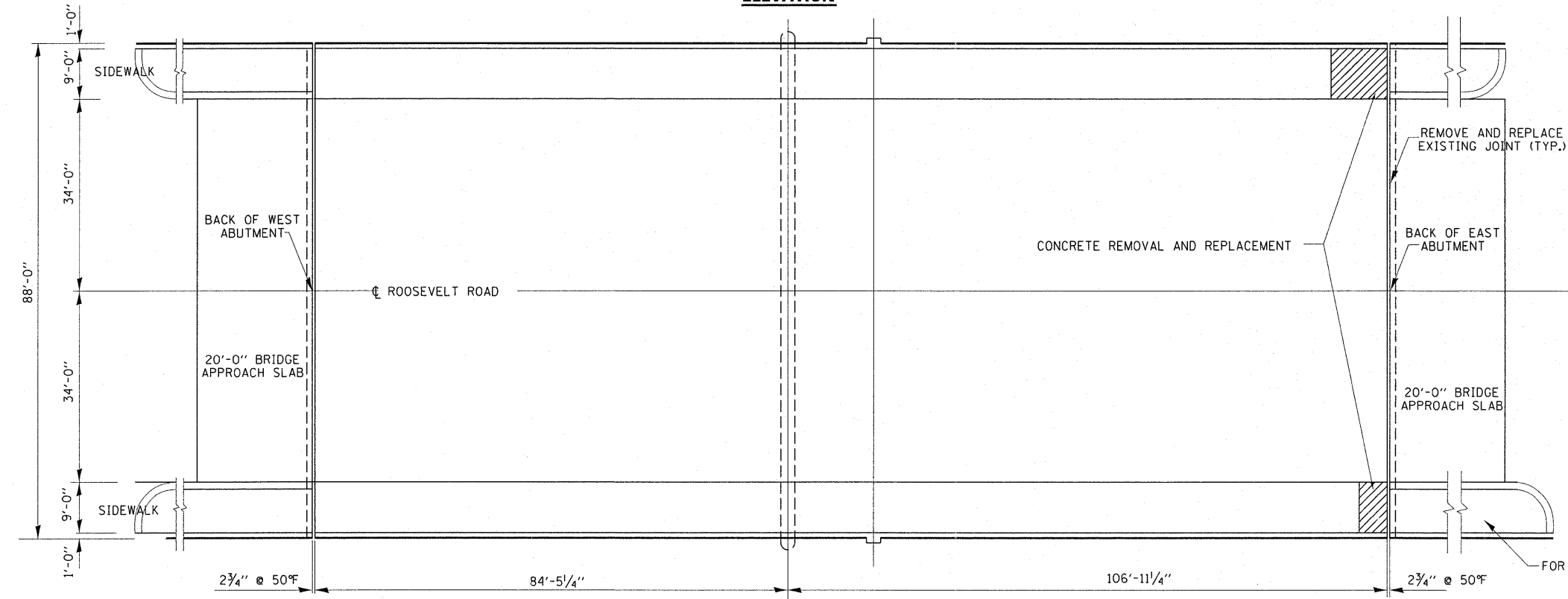
W. ROOSEVELT RD. OVER I-90/94 (DAN RYAN EXPY)
AS BUILT TRAFFIC SIGNAL CABLE PLAN
(FOR REFERENCE ONLY)

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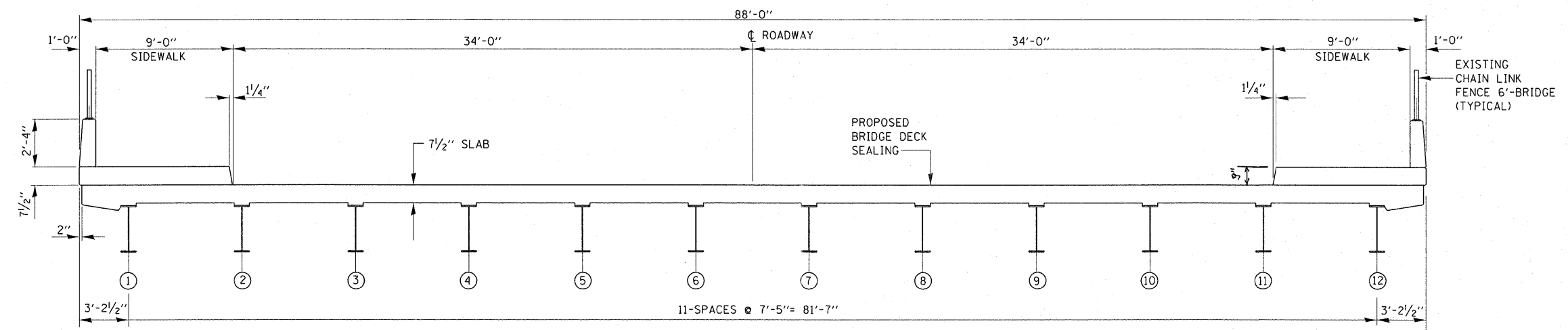
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2011-070-1	COOK	27	6F
			CONTRACT NO. 60P89	
ILLINOIS FED. AID PROJECT				



ELEVATION



PLAN



CROSS SECTION
(LOOKING EAST)

GENERAL NOTES:

PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENT PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF THE WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED BASED AT THE UNIT PRICE BID FOR THE WORK.

ALL STRUCTURAL STEEL SHALL CONFORM TO AASHTO CLASSIFICATION M-270 GR. 36, UNLESS OTHERWISE NOTED.

REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED.

EXISTING REINFORCEMENT BARS EXTENDING INTO THE REMOVAL AREA SHALL BE CLEANED, STRAIGHTENED AND INCORPORATED INTO THE NEW CONSTRUCTION. ANY REINFORCEMENT BARS THAT ARE DAMAGED DURING CONCRETE REMOVAL SHALL BE REPLACED WITH AN APPROVED BAR SPLICER OR ANCHORAGE SYSTEM. COST INCLUDED WITH CONCRETE REMOVAL.

AREA OF DECK REPAIRS SHOWN ARE ESTIMATED. THE ENGINEER SHALL SHOW ACTUAL LOCATIONS OF DECK REPAIRS ON AS-BUILT PLANS.

CONCRETE SUPERSTRUCTURE SHALL HAVE A SEVEN DAY MINIMUM CURE.

REINFORCEMENT BARS SHALL CONFORM TO THE REQUIREMENTS OF ASTM A 706 Gr 60.. SEE SPECIAL PROVISIONS.

JOINT OPENINGS SHALL BE ADJUSTED ACCORDING TO ARTICLE 520.04 OF THE STANDARD SPECIFICATIONS, WHEN THE DECK IS POURED AT AN AMBIENT TEMPERATURE OTHER THAN 50° F.

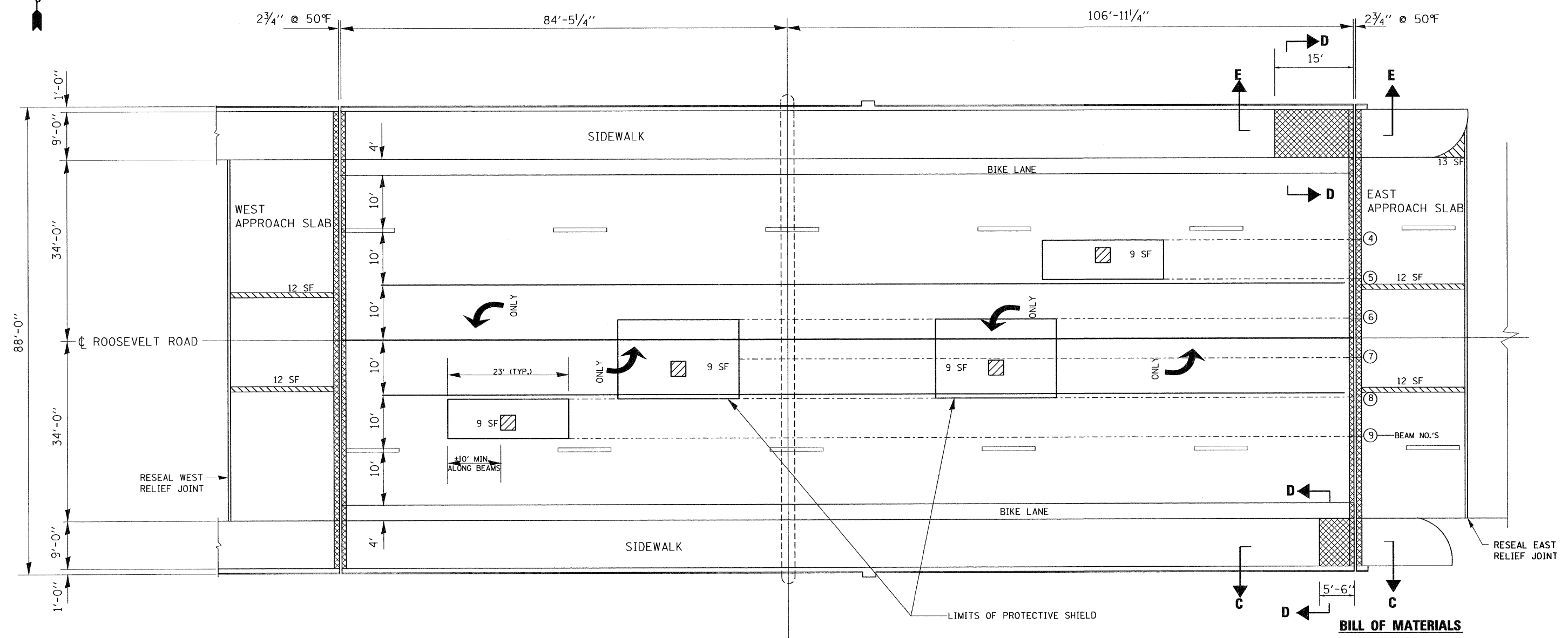
PRIOR TO POURING THE NEW CONCRETE DECK, ALL HEAVY OR LOOSE RUST, LOOSE MILL SCALE, AND OTHER LOOSE OR POTENTIALLY DETRIMENTAL FOREIGN MATERIAL SHALL BE REMOVED FROM THE SURFACES IN CONTACT WITH CONCRETE. TIGHTLY ADHERED PAINT MAY REMAIN UNLESS OTHERWISE NOTED. REMOVAL SHALL BE ACCOMPLISHED BY METHODS THAT WILL NOT DAMAGE THE STEEL AND THE COST WILL BE INCLUDED IN THE PAY ITEM COVERING REMOVAL OF THE EXISTING CONCRETE.

TOTAL BILL OF MATERIALS

ITEM	UNIT	QUANTITY
REINFORCEMENT BARS (EPOXY COATED)	POUND	940
CONCRETE REMOVAL	CU. YD.	20.7
CONCRETE SUPERSTRUCTURE	CU. YD.	20.7
PREFORMED JOINT STRIP SEAL	FOOT	180
DECK SLAB REPAIR (PARTIAL DEPTH)	SO. YD.	4
APPROACH SLAB REPAIR (FULL DEPTH)	SO. YD.	6
PROTECTIVE COAT	SO. YD.	2001
CLEAN & RESEAL RELIEF JOINT	FOOT	160
BAR SPLICERS	EACH	8
PROTECTIVE SHIELD	SO. YD.	115



David Carl Puze 12/13/11
Expires 11/30/12



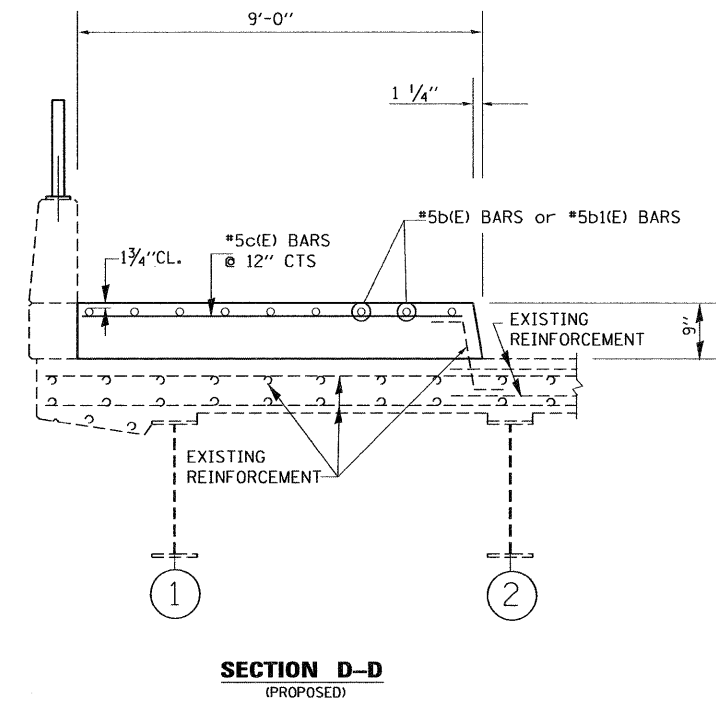
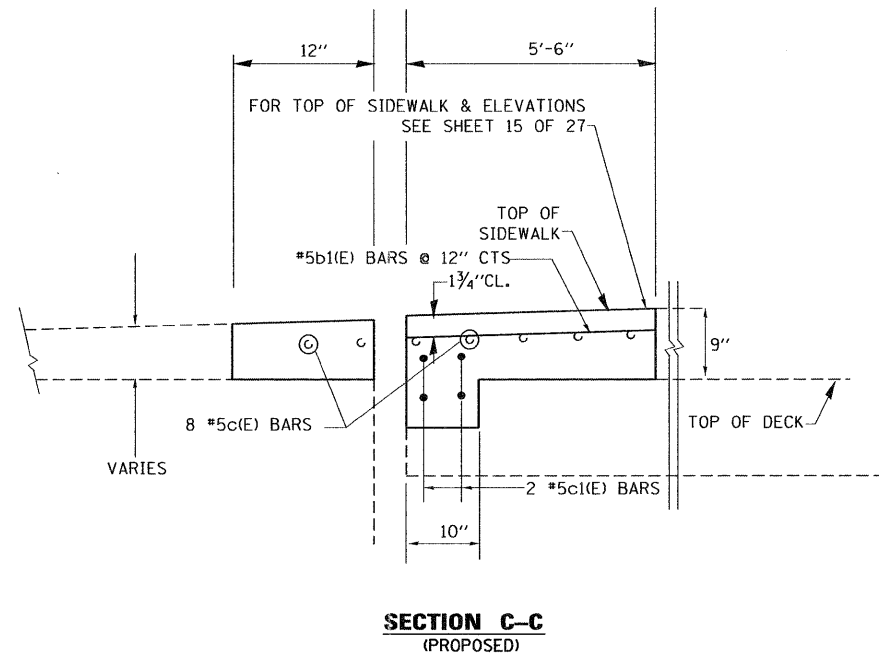
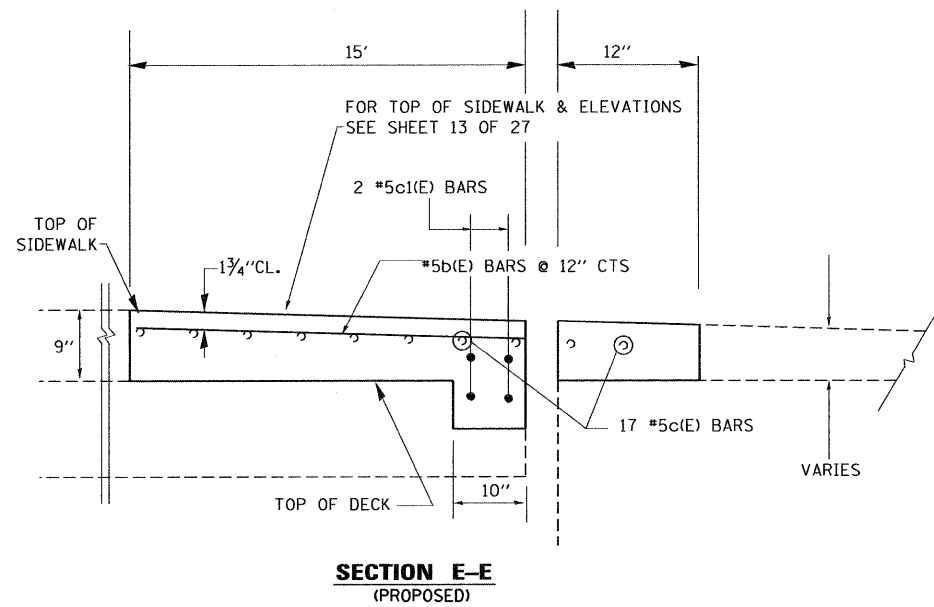
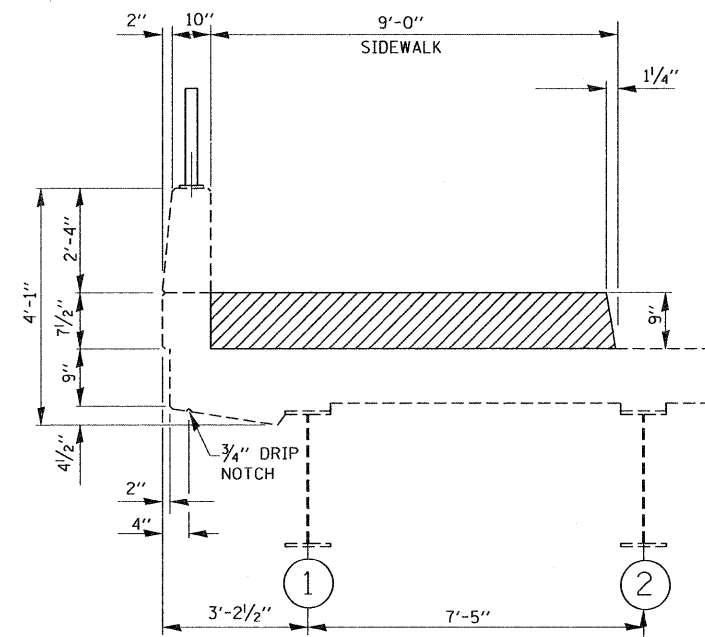
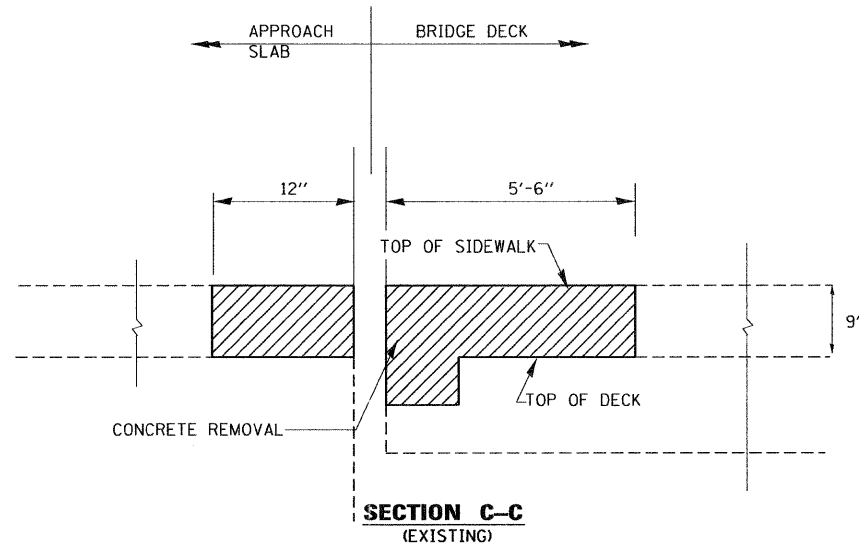
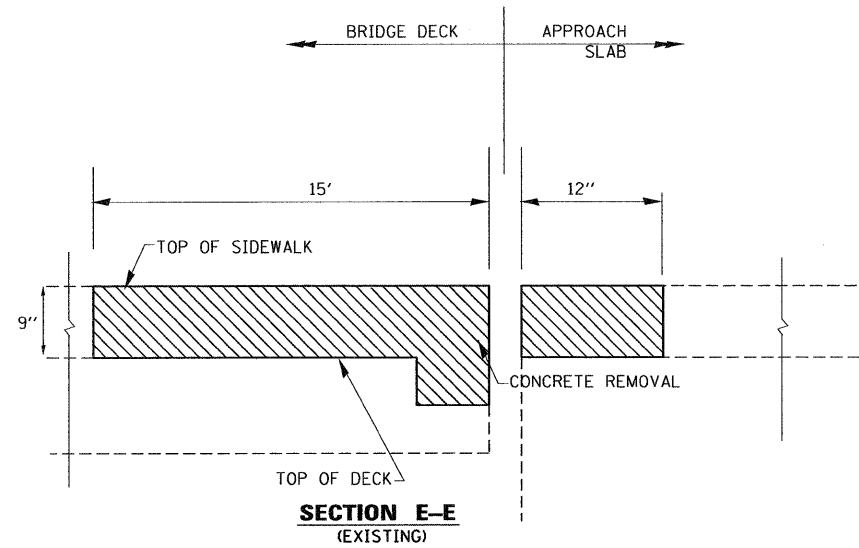
FOR SECTION C-C, D-D, & E-E SEE SHEET 4 OF 6

ITEM	UNIT	QUANTITY
DECK SLAB REPAIR (PARTIAL DEPTH)	SO. YD.	4
APPROACH SLAB REPAIR (FULL DEPTH)	SO. YD.	6
CLEAN & RESEAL RELIEF JOINT	FOOT	160
PROTECTIVE SHIELD	SO. YD.	115
CONCRETE REMOVAL	CU. YD.	20.7
CONCRETE SUPERSTRUCTURE	CU. YD.	20.7

LEGEND:

- DECK SLAB REPAIR (PARTIAL DEPTH)
- APPROACH SLAB REPAIR (FULL DEPTH)
- CONCRETE REMOVAL AND CONCRETE SUPERSTRUCTURE

FILE NAME =	USER NAME = patelar	DESIGNED - ANAND PATEL	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-90/94 (DAN RYAN EXPRESSWAY) AT ROOSEVELT ROAD DECK SLAB AND APPROACH SLAB DETAILS S.N. 016-0478	F.A.I. RTE. 90/94	SECTION 2011-070-1	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 9
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	PLOT DATE = 12/13/2011	CHECKED - ANAND PATEL	REVISED -			ILLINOIS FED. AID PROJECT				
		DATE - 11/02/11	REVISED -							



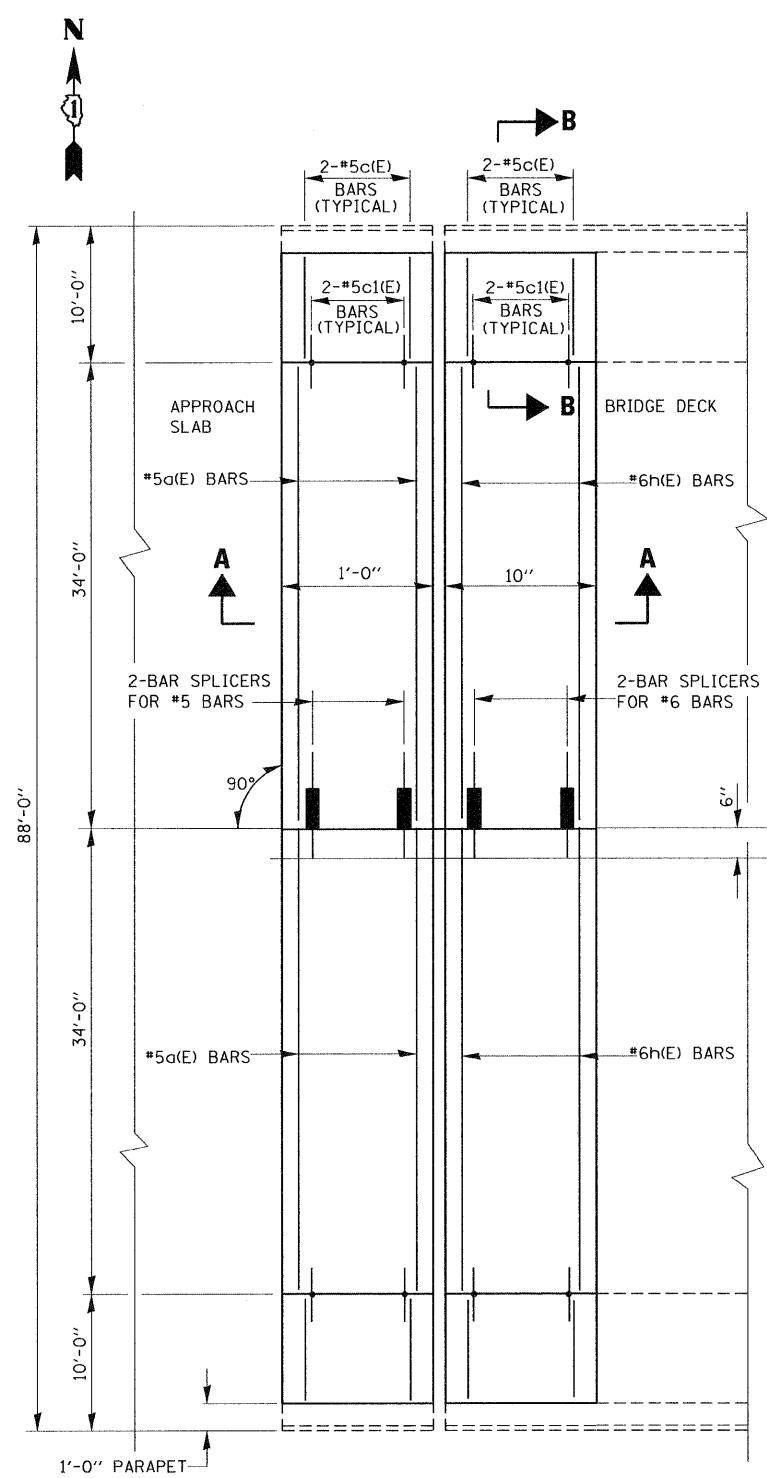
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PLOT SCALE = 50.0000 ' / in.		CHECKED - ANAND PATEL	REVISED -
PLOT DATE = 12/13/2011		DATE - 11/02/11	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

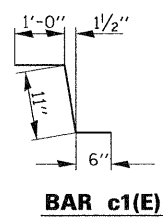
I-90/94 (DAN RYAN EXPRESSWAY) AT ROOSEVELT ROAD
SIDEWALK DETAILS
S.N. 016-0478

F.A.I. RTE. 90/94	SECTION 2011-070-1	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 9A
			CONTRACT NO. 60P89	
ILLINOIS FED. AID PROJECT				

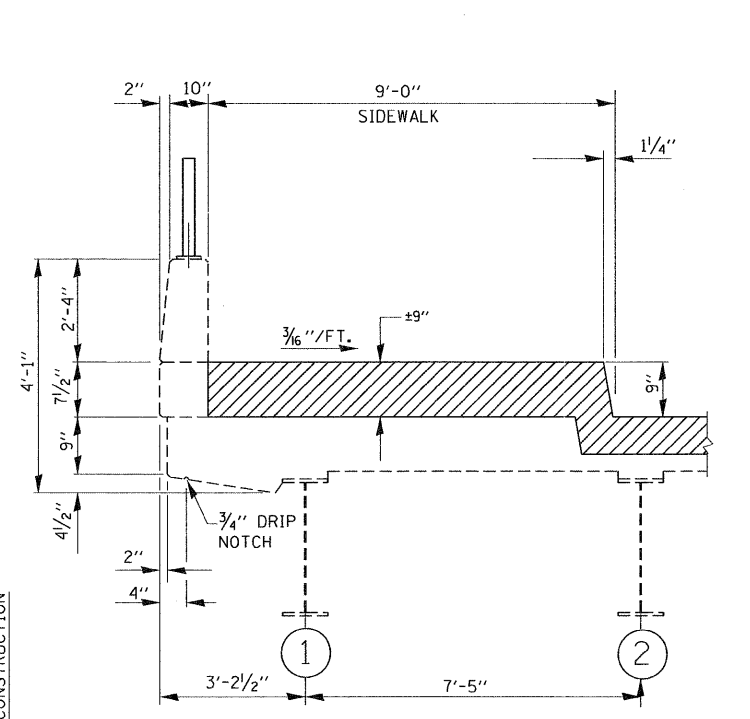
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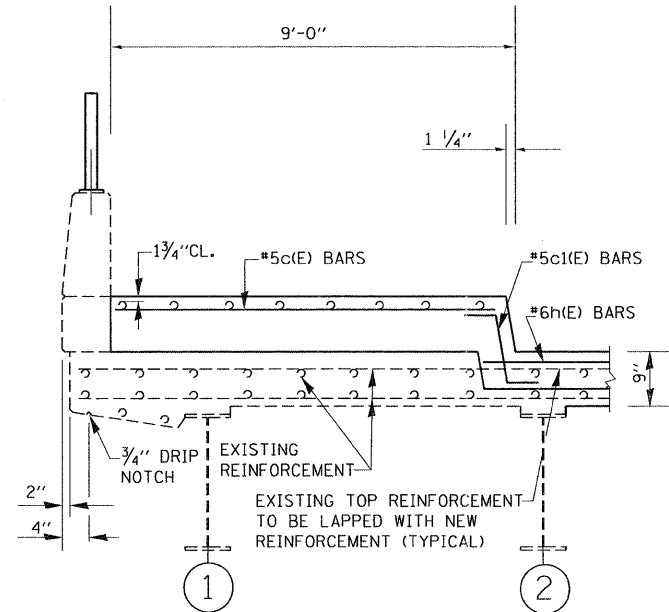
PLAN
WEST ABUTMENT SHOWN
(EAST ABUTMENT SIMILAR BY 180°)



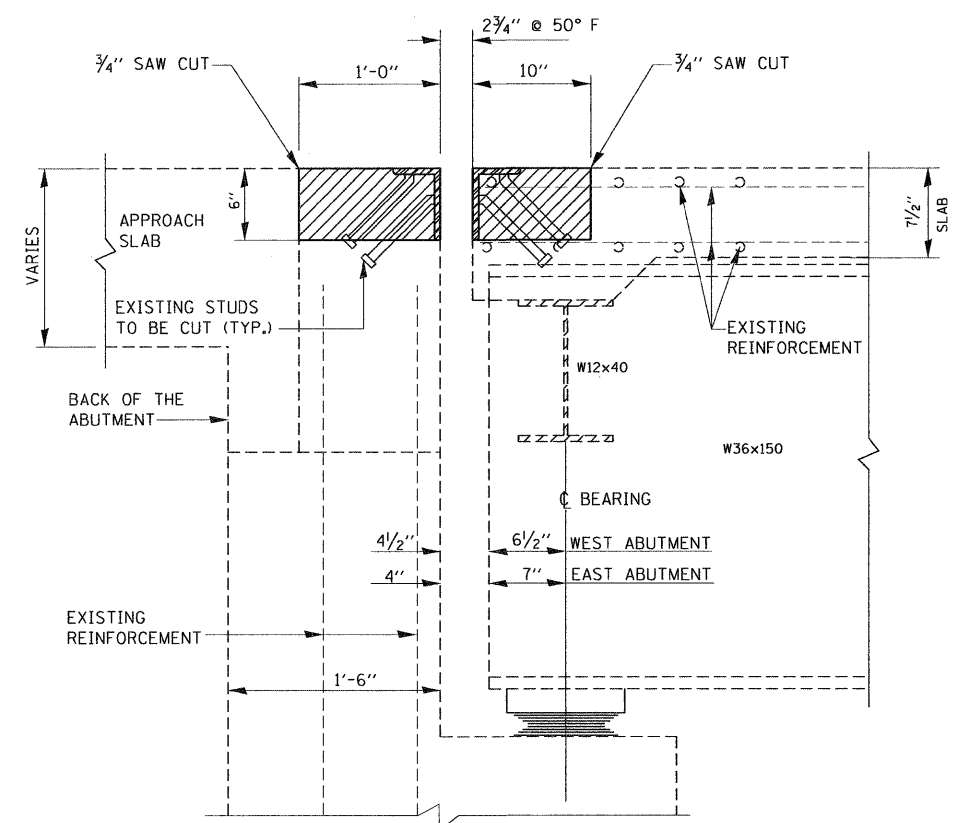
BAR c1(E)



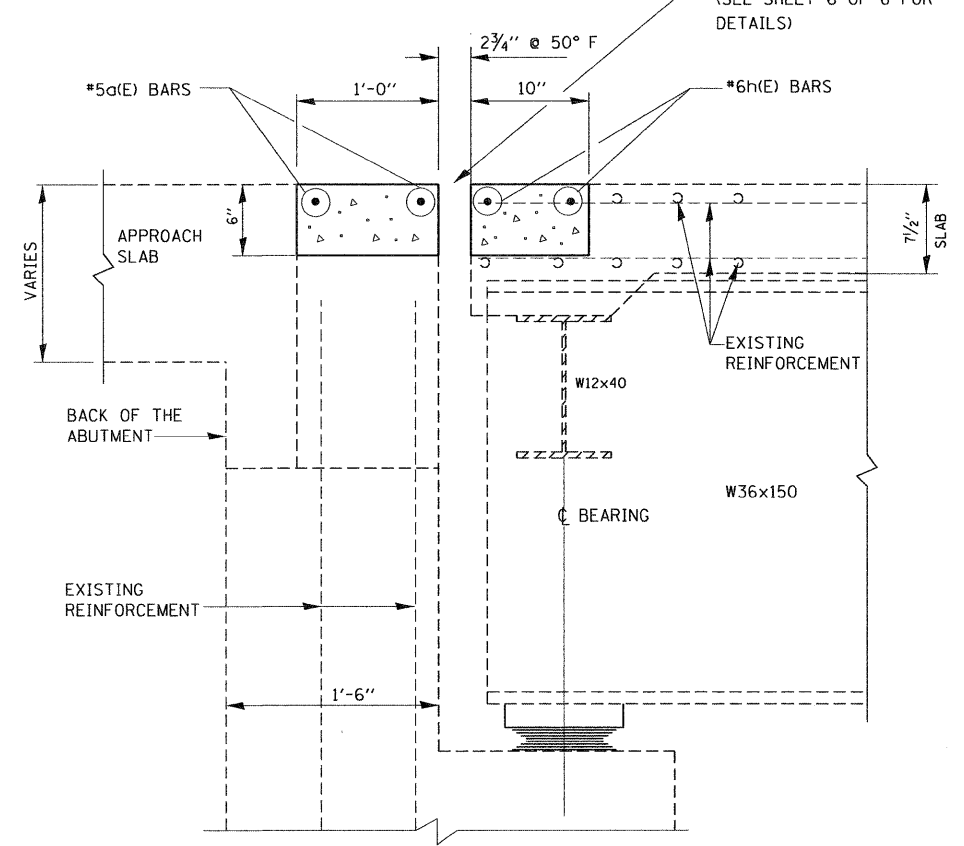
SECTION B-B
(EXISTING)



SECTION B-B
(PROPOSED)



SECTION A-A
(EXISTING)

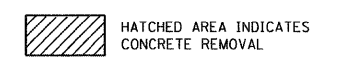


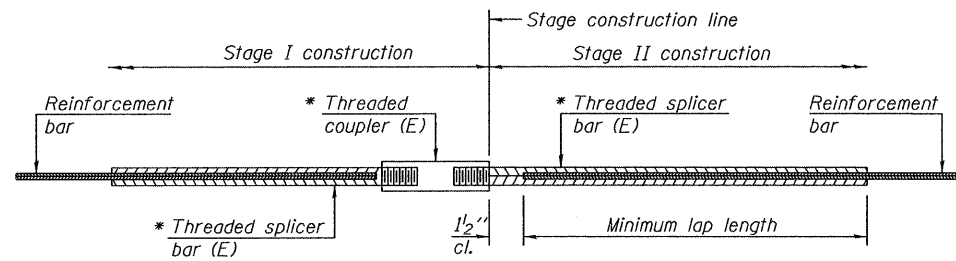
SECTION A-A
(PROPOSED)

BILL OF MATERIALS

BAR	NO.	SIZE	LENGTH	SHAPE	
a(E)	8	#5	33'-9"	—	
h(E)	8	#6	33'-9"	—	
c(E)	37	#5	8'-9"	—	
c1(E)	16	#5	2'-5"	┘	
b(E)	9	#5	14'-8"	—	
b1(E)	9	#5	5'-2"	—	
REINFORCEMENT BARS (EPOXY COATED)				POUND	940
CONCRETE REMOVAL				CU.YD.	20.7
CONCRETE SUPERSTRUCTURE				CU.YD.	20.7
BAR SPLICERS				EACH	8
PROTECTIVE COAT				SQ. YD.	2001

LEGEND:





STANDARD BAR SPLICER ASSEMBLY

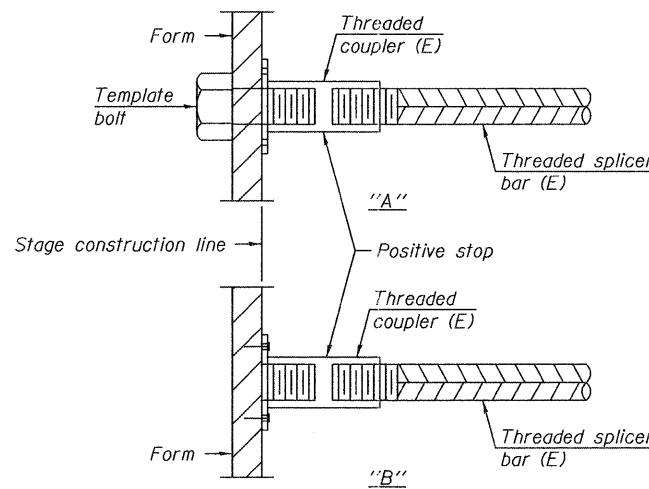
Minimum Lap Lengths					
Bar size to be spliced	Table 1	Table 2	Table 3	Table 4	Table 5
3, 4	1'-5"	1'-11"	2'-1"	2'-4"	2'-3"
5	1'-9"	2'-5"	2'-7"	2'-11"	2'-10"
6	2'-1"	2'-11"	3'-1"	3'-6"	3'-4"
7	2'-9"	3'-10"	4'-2"	4'-8"	4'-6"
8	3'-8"	5'-1"	5'-5"	6'-2"	5'-10"
9	4'-7"	6'-5"	6'-10"	7'-9"	7'-5"

- Table 1: Black bar, 0.8 Class C
- Table 2: Black bar, Top bar lap, 0.8 Class C
- Table 3: Epoxy bar, 0.8 Class C
- Table 4: Epoxy bar, Top bar lap, 0.8 Class C
- Table 5: Epoxy bar, Top bar lap, Class B

Threaded splicer bar length = min. lap length + 1/2" + thread length

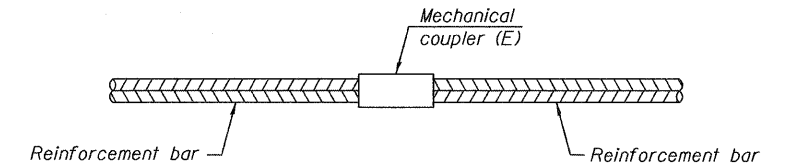
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length
Abutment	#5	4	2'-7"
Deck	#6	4	3'-1"



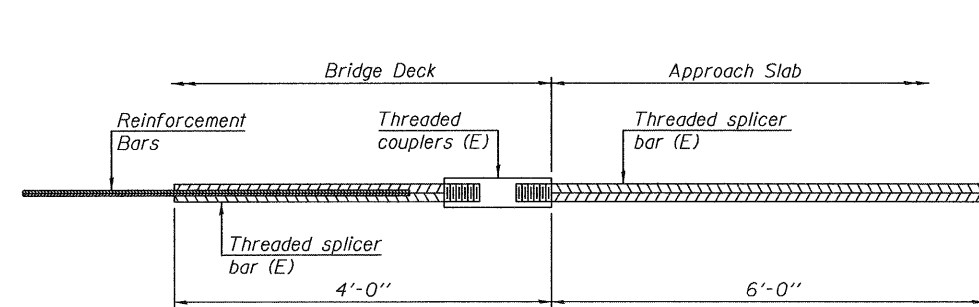
INSTALLATION AND SETTING METHODS

- "A" : Set bar splicer assembly by means of a template bolt.
- "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
- (E) : Indicates epoxy coating.



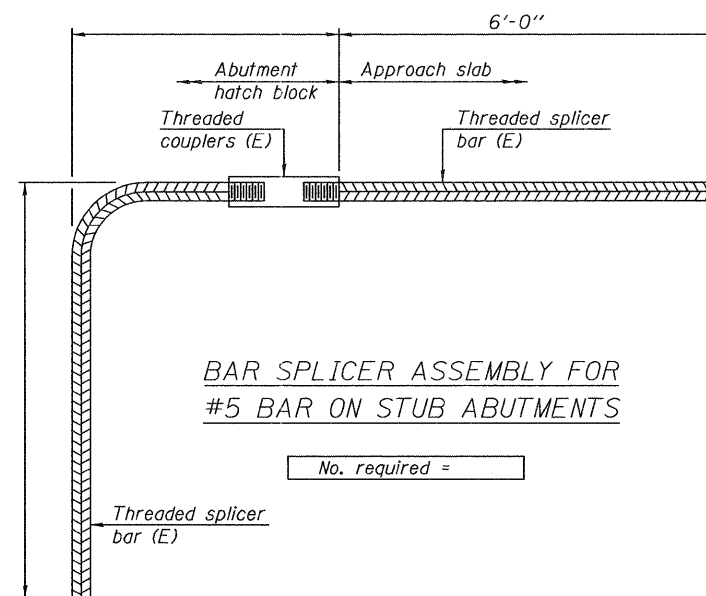
STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



BAR SPLICER ASSEMBLY FOR #5 BAR ON INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

No. required =



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

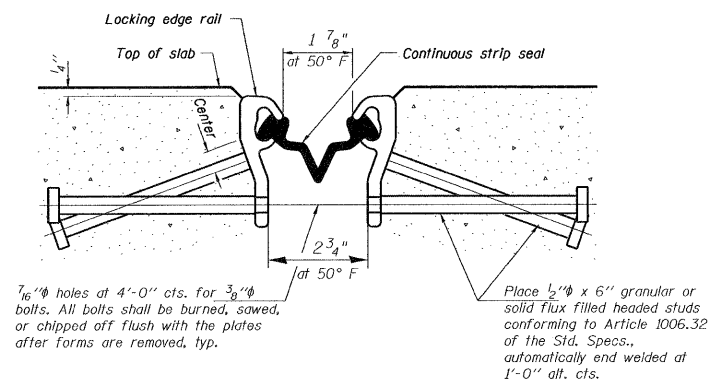
No. required =

NOTES

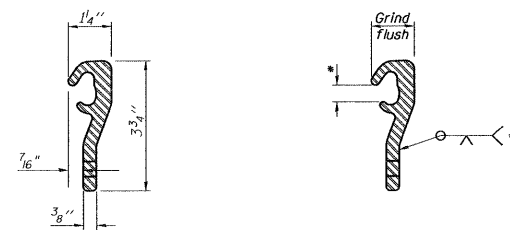
- Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
- All reinforcement shall be lapped and tied to the splicer bars.
- Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
- See special provision for Mechanical Splicers.
- See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1 7-1-10

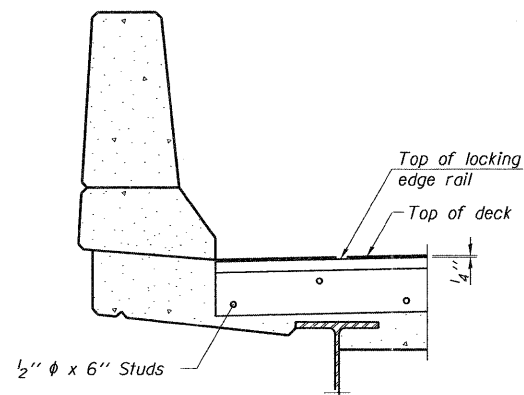
*** FOR END TREATMENT DETAILS REFER TO SHEET 10



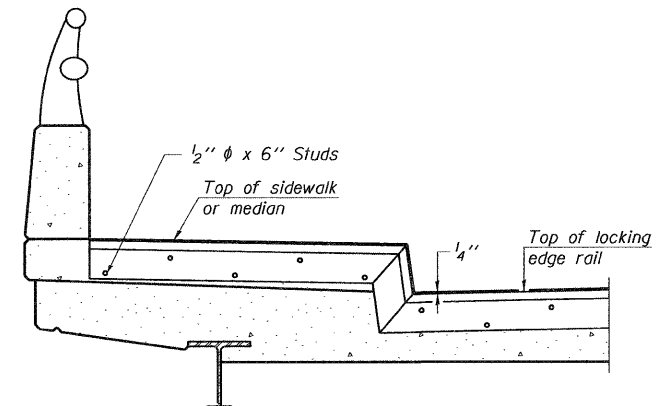
SECTION THRU STRIP SEAL JOINT



LOCKING EDGE RAIL LOCKING EDGE RAIL SPLICE
* Omit weld at seal opening.

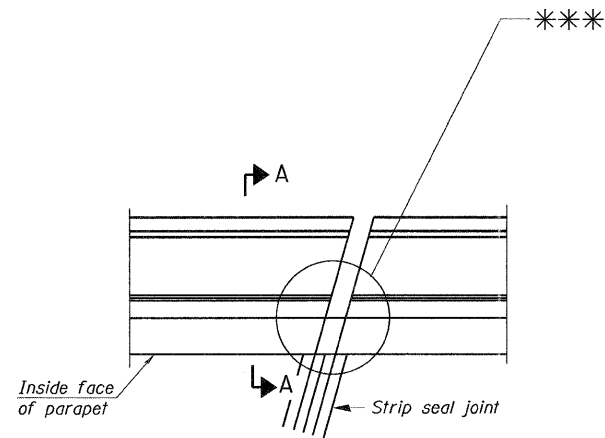


SECTION A-A



TYPICAL END TREATMENT AT SIDEWALK OR MEDIAN

TYPICAL END TREATMENTS



PLAN
(For Skew > 30°)
Showing point Block

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails.

The height and thickness of the Locking Edge Rails shown are minimum dimensions. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed.

The inside of the Locking Edge Rail groove shall be free of weld residue.

Locking Edge Rails may be spliced at slope discontinuities and stage construction joints.

The manufacturer's recommended installation methods shall be followed.

The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

Maximum space between rail segments at stage lines shall be 3/16", sealed with a suitable sealant.

Parapet plates and anchorage studs for skews > 30° included in the cost of Preformed Joint Strip Seal.

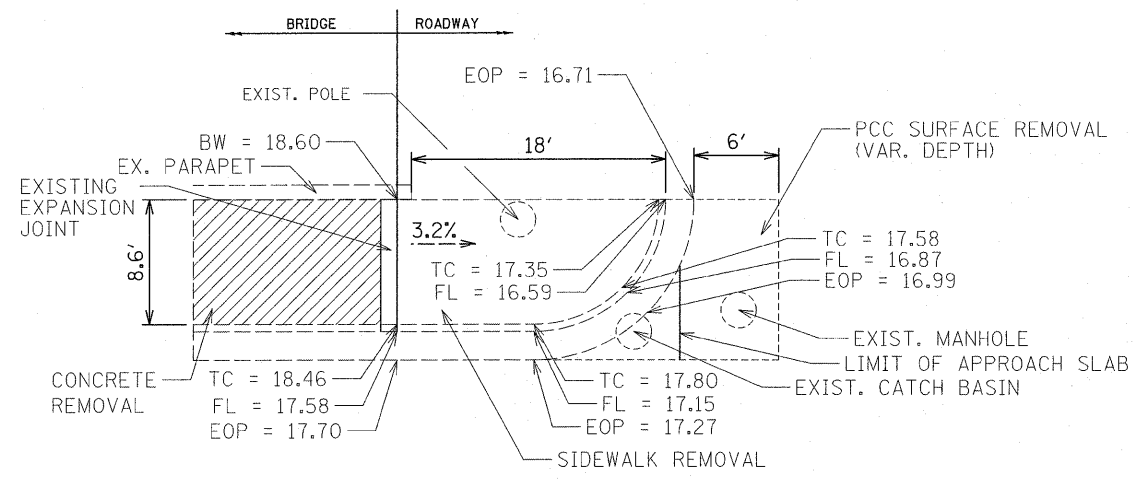
The Inside of the Locking Edge Rail groove shall be free of weld residue.

BILL OF MATERIAL

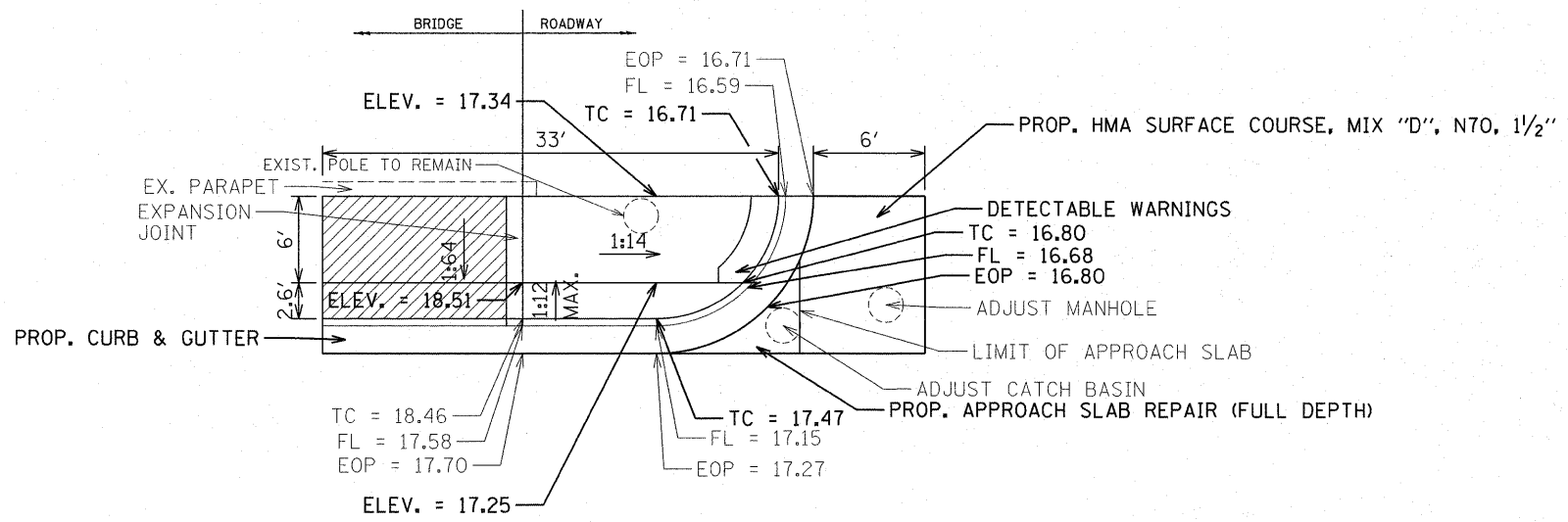
Item	Unit	Total
Preformed Joint Strip Seal	Foot	180

PREFORMED JOINT STRIP SEAL
STRUCTURE NO. 016-0478

FILE NAME =	USER NAME = patelar	DESIGNED - ANAND PATEL	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-9094 (DAN RYAN EXPRESSWAY AT ROOSEVELT ROAD) PREFORMED JOINT STRIP SEAL S.N. 016-0478	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pw\work\pwr\dot\patelar\d0201223\0162711-shr-plan.dgn	DRAWN - MARK V. TINIAKOS	REVISIED -	90/94			2011-070-1	COOK	27	12	
PLOT SCALE = 50.0000' / in.	CHECKED - ANAND PATEL	REVISED -	CONTRACT NO. 60P89							
PLOT DATE = 12/13/2011	DATE - 11/02/11	REVISED -	ILLINOIS FED. AID PROJECT							
					SCALE:	SHEET NO. 6 OF 6 SHEETS		STA.	TO STA.	



EXISTING PLAN



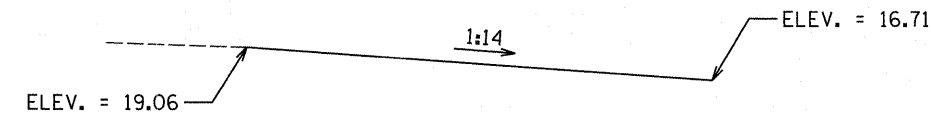
PROPOSED PLAN

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE USE	AIR VOIDS @ NDES
HMA SURFACE COURSE, MIX "D", N70 (IL 9.5 mm)	4% @ 70 Gyr.

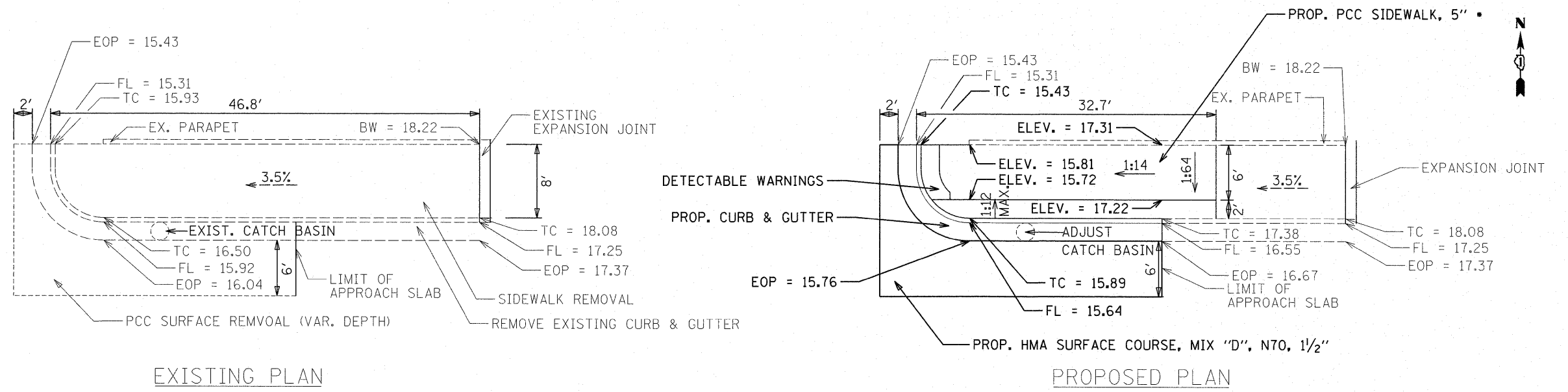
THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQYD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPEICAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.



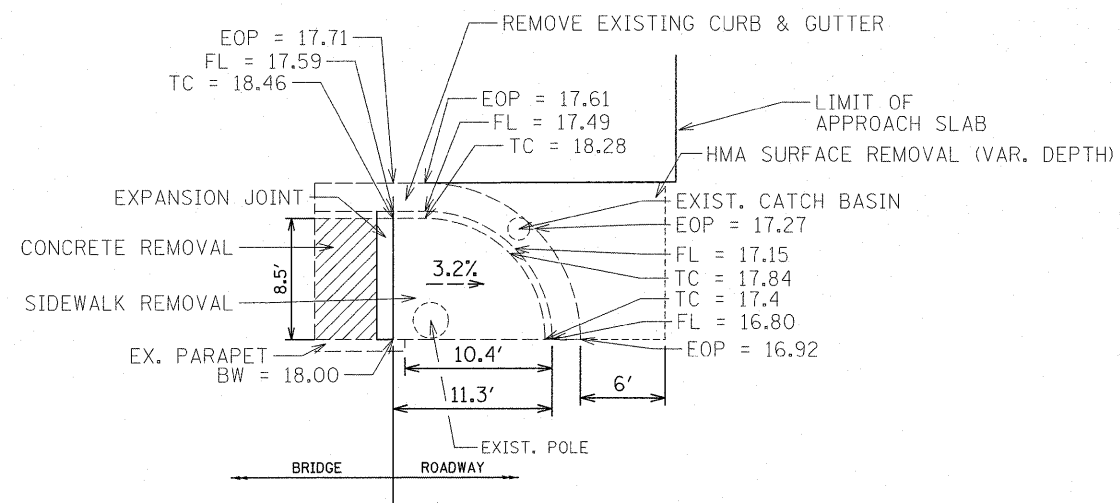
PROFILE

• NOTE: REFER TO BD-17 FOR DETAILS ON PCC SIDEWALK FOR THE CITY OF CHICAGO.

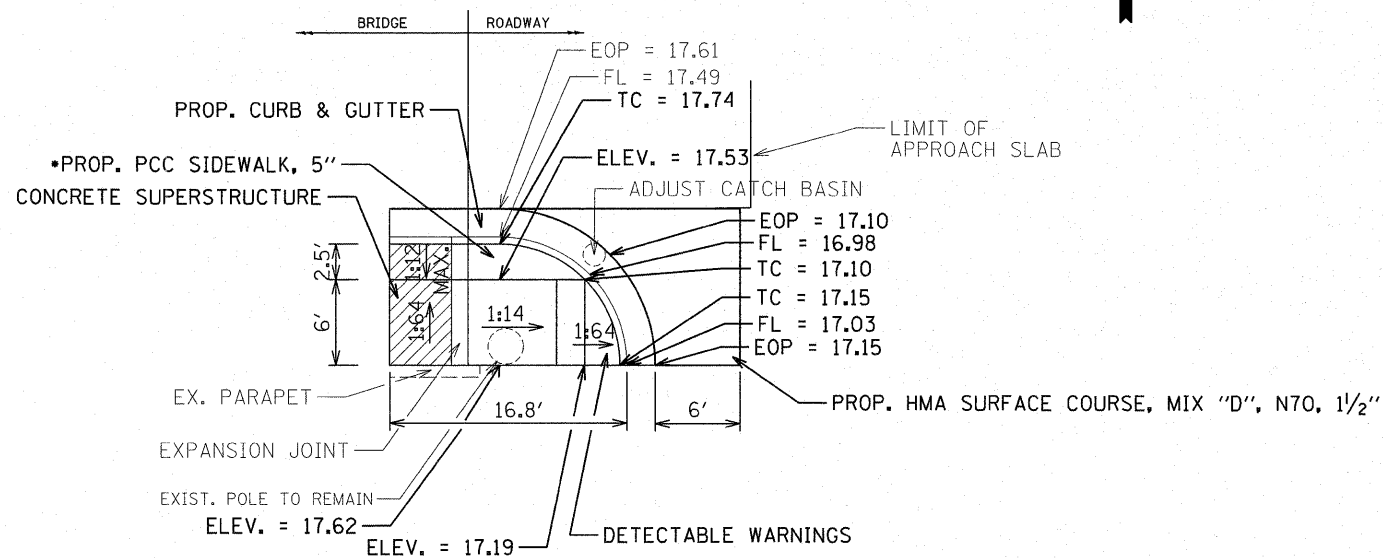


• NOTE: REFER TO BD-17 FOR DETAILS ON PCC SIDEWALK FOR THE CITY OF CHICAGO.

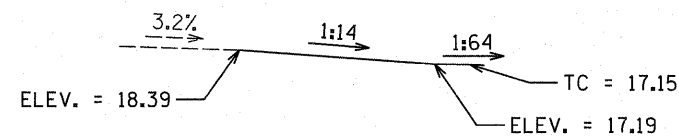
FILE NAME =	USER NAME = patelar	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB RAMP PLANS ROOSEVELT ROAD AT I-90/94 (NORTHWEST CORNER)	F.A.I. RTE. 90/94	SECTION 2011-070-I	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 14		
cr\pw_work\pwsdot\patelar\11\281223\01627	11-shr-plandgn	DRAWN -	REVISED -			SCALE:	SHEET NO. 2 OF 4 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		
	PLOT SCALE = 50.0000' / in.	CHECKED -	REVISED -									
	PLOT DATE = 11/4/2011	DATE -	REVISED -									



EXISTING PLAN



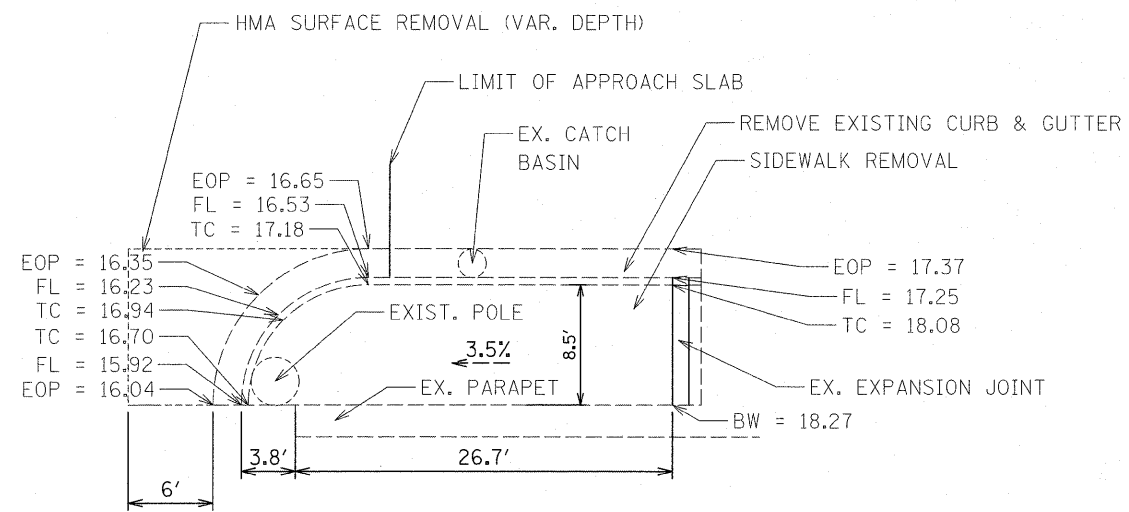
PROPOSED PLAN



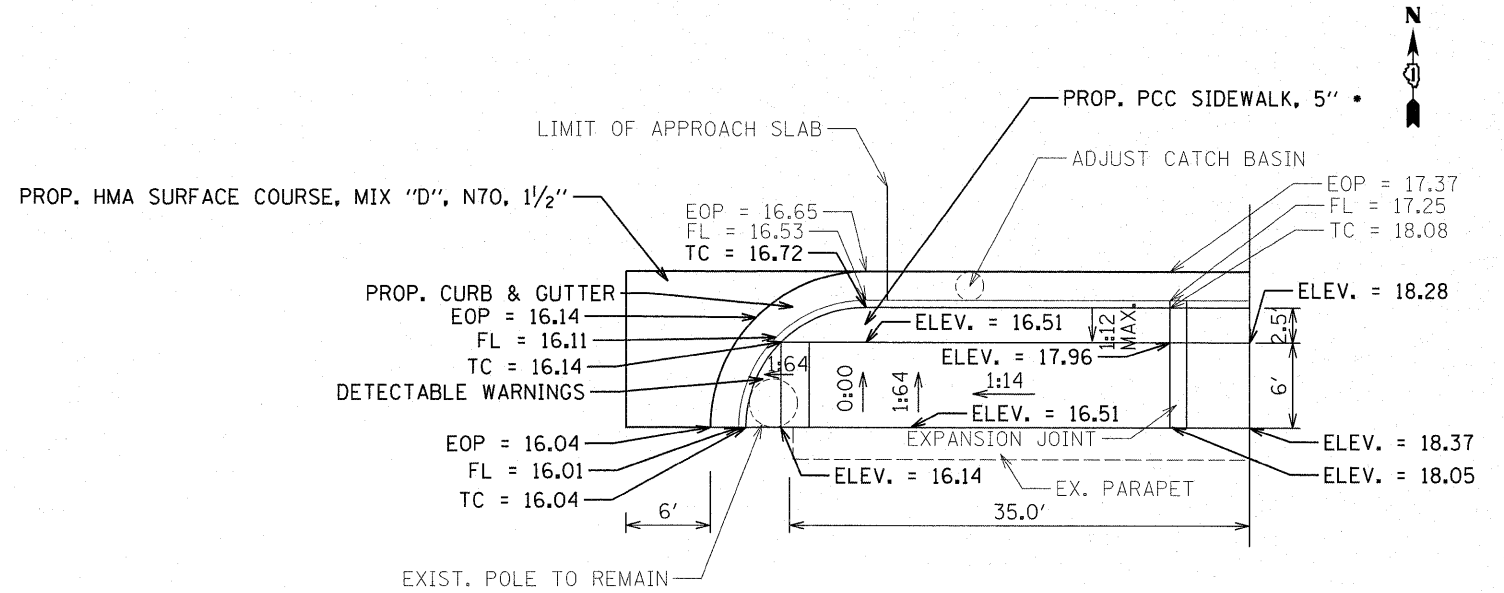
PROFILE

*NOTE: REFER TO BD-17 FOR DETAILS ON PCC SIDEWALK FOR THE CITY OF CHICAGO.

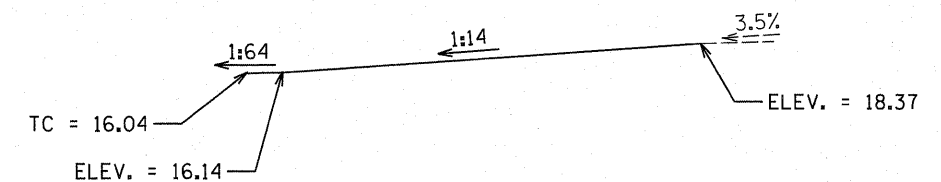
FILE NAME =	USER NAME = poteler	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB RAMP PLANS ROOSEVELT ROAD AT I-90/94 (SOUTHEAST CORNER)		F.A.I. RTE. 90/94	SECTION 2011-070-I	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 15	
ct\pwr_work\pwr\dot\poteler\d0281223\0162711-shr-pln.dgn		DRAWN -	REVISED -		SCALE:	SHEET NO. 3 OF 4 SHEETS	STA.	TO STA.	CONTRACT NO. 60P89			
		PLOT SCALE = 50.0000' / in.	CHECKED -		REVISED -	ILLINOIS FED. AID PROJECT						
		PLOT DATE = 11/4/2011	DATE -		REVISED -							



EXISTING PLAN



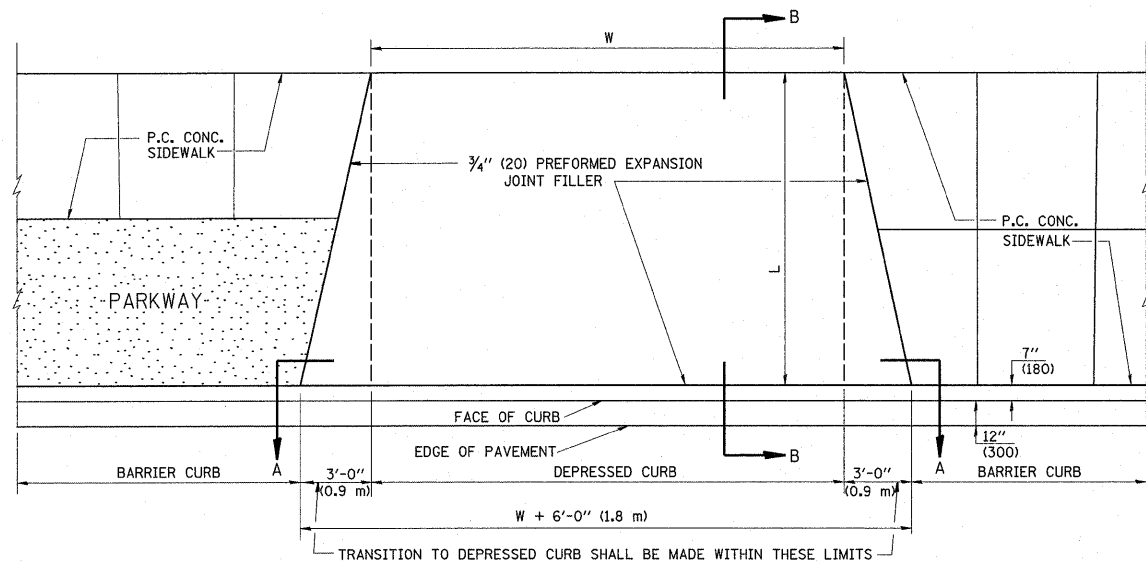
PROPOSED PLAN



PROFILE

*NOTE: REFER TO BD-17 FOR DETAILS ON PCC SIDEWALK FOR THE CITY OF CHICAGO.

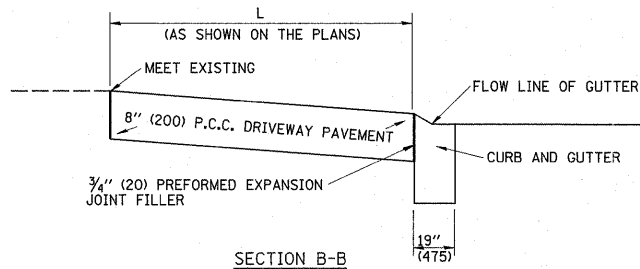
FILE NAME =	USER NAME = petelar	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB RAMP PLANS ROOSEVELT ROAD AT I-90/94 (SOUTHWEST CORNER)	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cr\pv_work\pvidot\petelar\d0281223\DI62711-shr-plan.dgn		DRAWN -	REVISED -			90/94	2011-070-I	COOK	27	16
PLOT SCALE = 50.0000' / 1" =		CHECKED -	REVISED -			SCALE: SHEET NO. 4 OF 4 SHEETS STA. TO STA.		CONTRACT NO. 60P89		
PLOT DATE = 11/4/2011		DATE -	REVISED -			ILLINOIS FED. AID PROJECT				



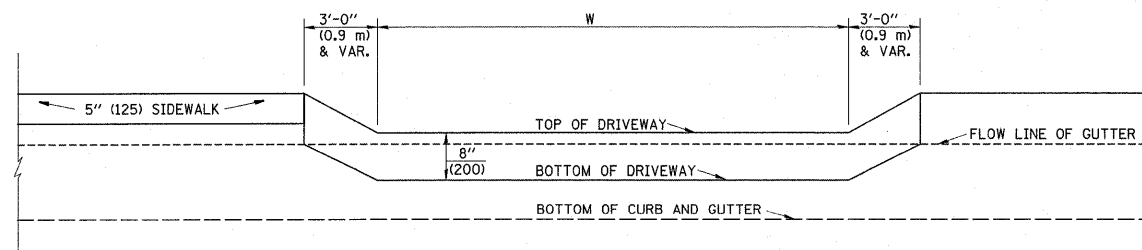
PLAN VIEW

NOTES:

1. EXPANSION JOINTS SHALL BE CONSTRUCTED AS SHOWN ON THE DETAILS FOR P.C.C. SIDEWALK.
2. THE CURB BETWEEN ADJACENT DRIVEWAYS SHALL BE FULL HEIGHT FOR A DISTANCE OF AT LEAST FOUR FEET (1.2 METERS)
3. P.C. CONCRETE DRIVEWAYS SHALL BE CONSTRUCTED AT LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
4. 3/4" (20) PREFORMED EXPANSION JOINTS WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO P.C.C. DRIVEWAY PAVEMENT 8" (200).
5. COMBINATION CONC. CURB AND GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE TRANSITION CURB AND GUTTER.

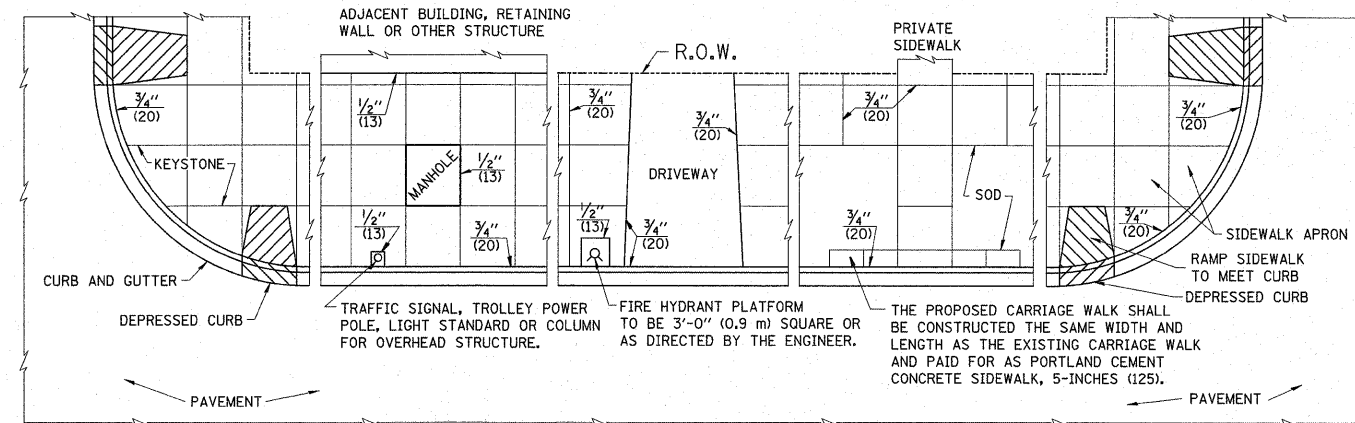


SECTION B-B



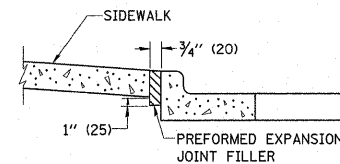
SECTION A-A

P.C.C. DRIVEWAY PAVEMENT DETAIL



NOTES:

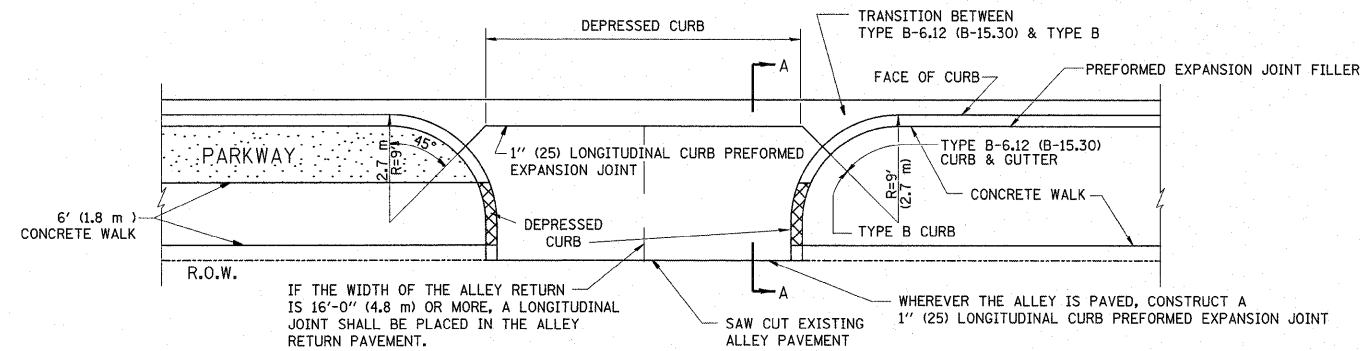
1. ONE-HALF INCH THICK EXPANSION JOINTS SHALL BE PLACED BETWEEN THE SIDEWALK AND ALL STRUCTURES SUCH AS LIGHT STANDARDS, TRAFFIC LIGHT STANDARDS, MANHOLES, WHICH EXTEND THROUGH THE SIDEWALK.
2. 3/4" (20) THICK EXPANSION JOINTS SHALL BE PLACED AT INTERVALS OF NOT MORE THAN 100 FEET (30 METERS) IN THE SIDEWALK. WHERE THE SIDEWALK IS CONSTRUCTED ADJACENT TO PAVEMENT OR CURB HAVING EXPANSION JOINTS, THE EXPANSION JOINTS IN THE SIDEWALK SHALL BE PLACED OPPOSITE THE EXISTING EXPANSION JOINTS AS NEARLY AS PRACTICABLE. EXPANSION JOINTS SHALL ALSO BE PLACED WHERE THE SIDEWALK ABUTS EXISTING SIDEWALKS, BETWEEN DRIVEWAY PAVEMENT AND SIDEWALK, AND BETWEEN SIDEWALK AND CURBS WHERE THE SIDEWALK ABUTS A CURB.



SLOPE FOR SIDEWALK
1" (25) IN 3'-0" (0.9 m) IN CHICAGO

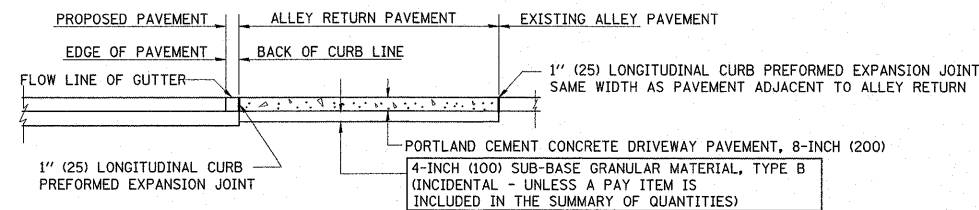
PORTLAND CEMENT CONCRETE SIDEWALK DETAILS

NOTES: NO EXTRA COMPENSATION SHALL BE ALLOWED FOR THE GUTTER FLARE



IF THE WIDTH OF THE ALLEY RETURN IS 16'-0" (4.8 m) OR MORE, A LONGITUDINAL JOINT SHALL BE PLACED IN THE ALLEY RETURN PAVEMENT.

WHEREVER THE ALLEY IS PAVED, CONSTRUCT A 1" (25) LONGITUDINAL CURB PREFORMED EXPANSION JOINT



SECTION A-A

ALLEY RETURN DETAIL

FILE NAME =	USER NAME = gorengoutab	DESIGNED - M. DE YONG	REVISED -
es:\pw_work\pwwork\gorengoutab\d0281223\0162711-shd-plan.dgn		DRAWN -	REVISED -
PLOT SCALE = 50.0000' / 1"		CHECKED -	REVISED -
PLOT DATE = 11/3/2011		DATE - 06-13-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO
DETAILS FOR P.C. CONCRETE DRIVEWAY, ALLEY RETURN AND SIDEWALK

F.A.I. RTE. 90/94	SECTION 2011-070-1	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 17
BD400-03 (BD-17)		CONTRACT NO. 60P89		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

1/4" (5) **

18" (450) MAX.

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

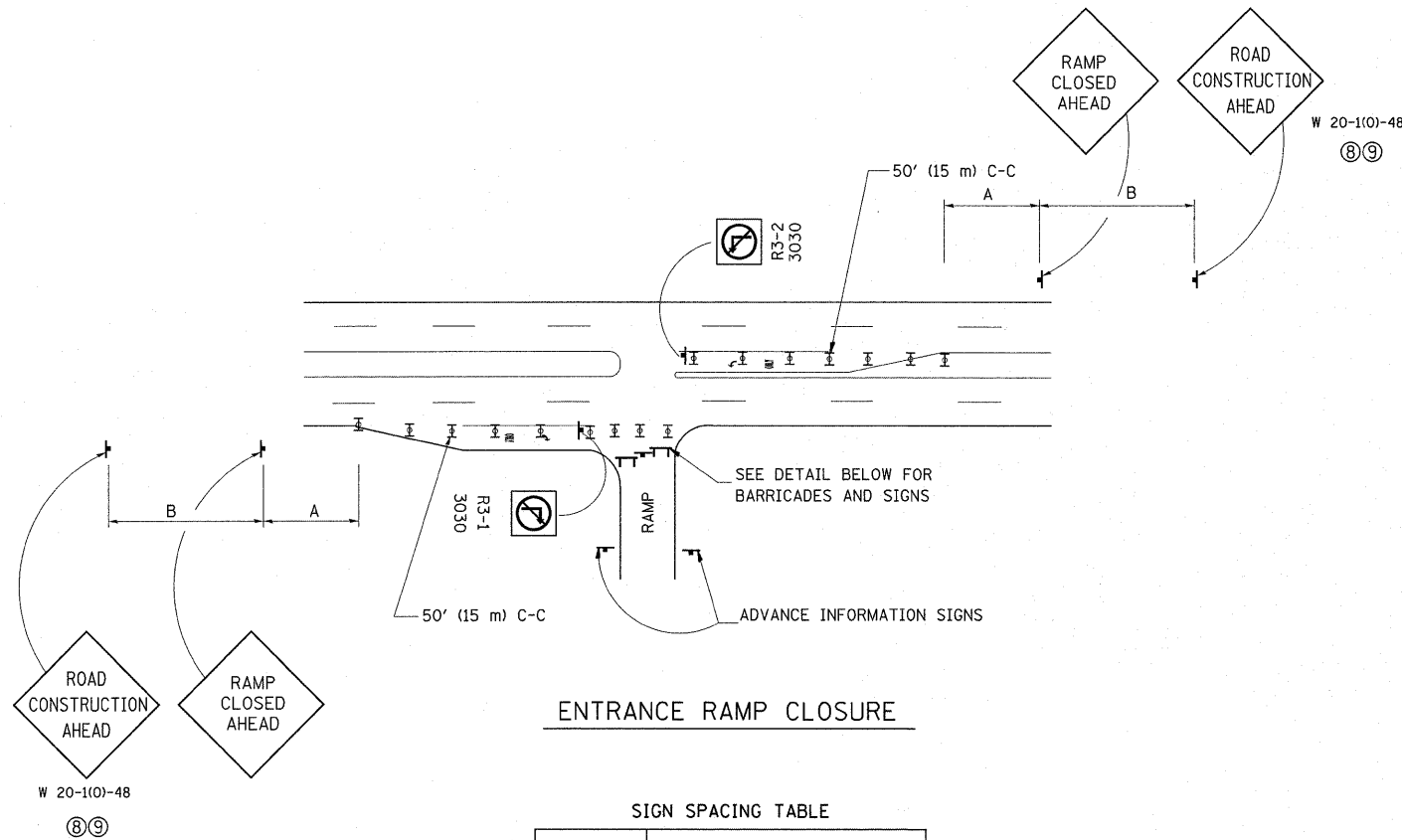
BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = gorengautab	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pw\work\pwwork\gorengautab\d0281223\162711-shr-plen.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97		90/94	2011-070-I	COOK	27	18			
PLOT SCALE = 50.0000 / in.		CHECKED -	REVISED - M. GOMEZ 01-22-01		BD600-06 (BD-24)			CONTRACT NO. 60P89				
PLOT DATE = 11/3/2011		DATE - 03-11-94	REVISED - R. BORO 12-15-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

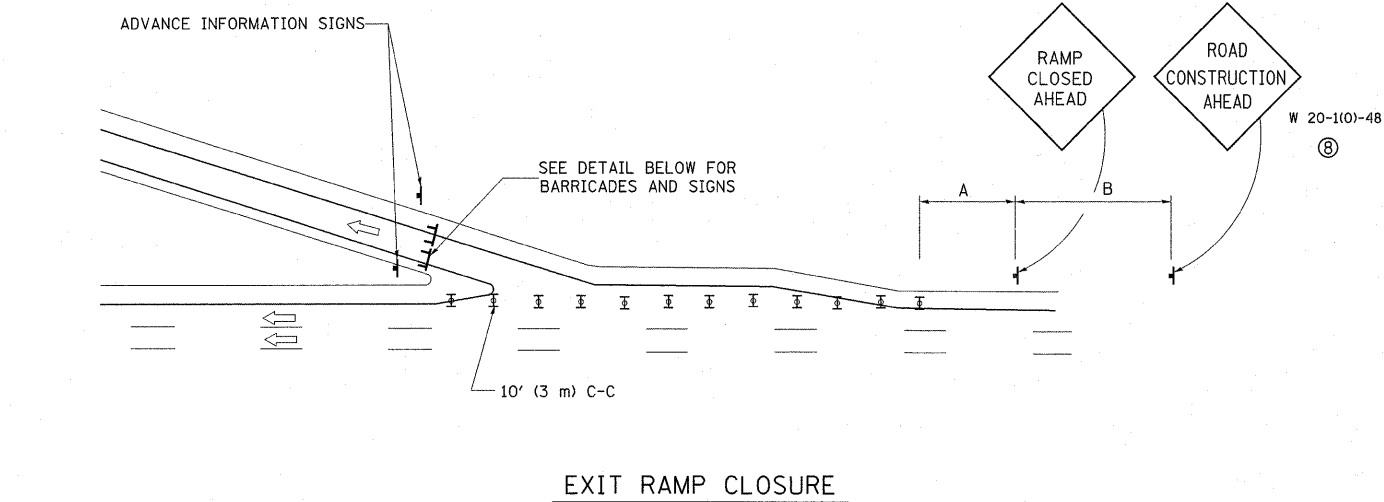


ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY <24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL ≥45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	150' (45 m)	150' (45 m)

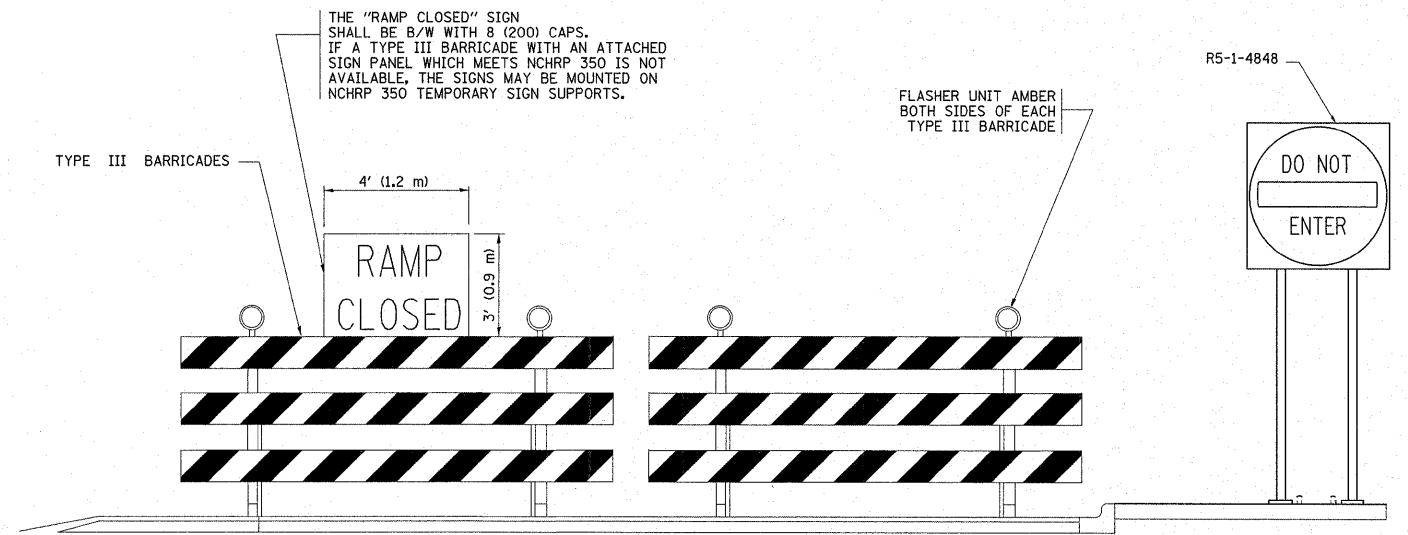
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



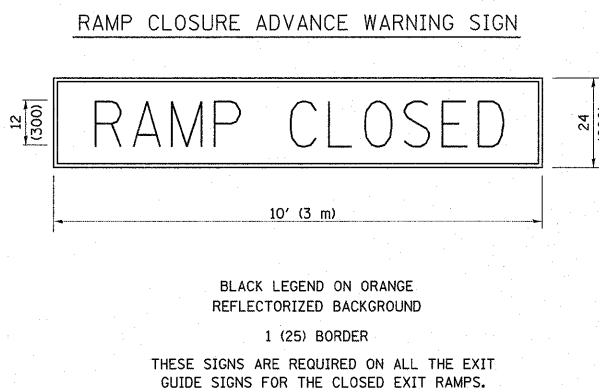
EXIT RAMP CLOSURE

SYMBOLS

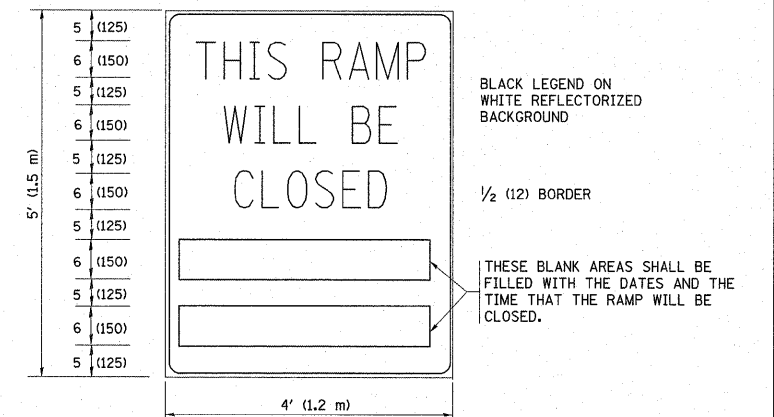
- ▬ TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- ▬ TYPE III BARRICADE WITH FLASHING LIGHT



DETAIL FOR REQUIRED BARRICADES & SIGNS



RAMP CLOSURE ADVANCE INFORMATION SIGN



THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

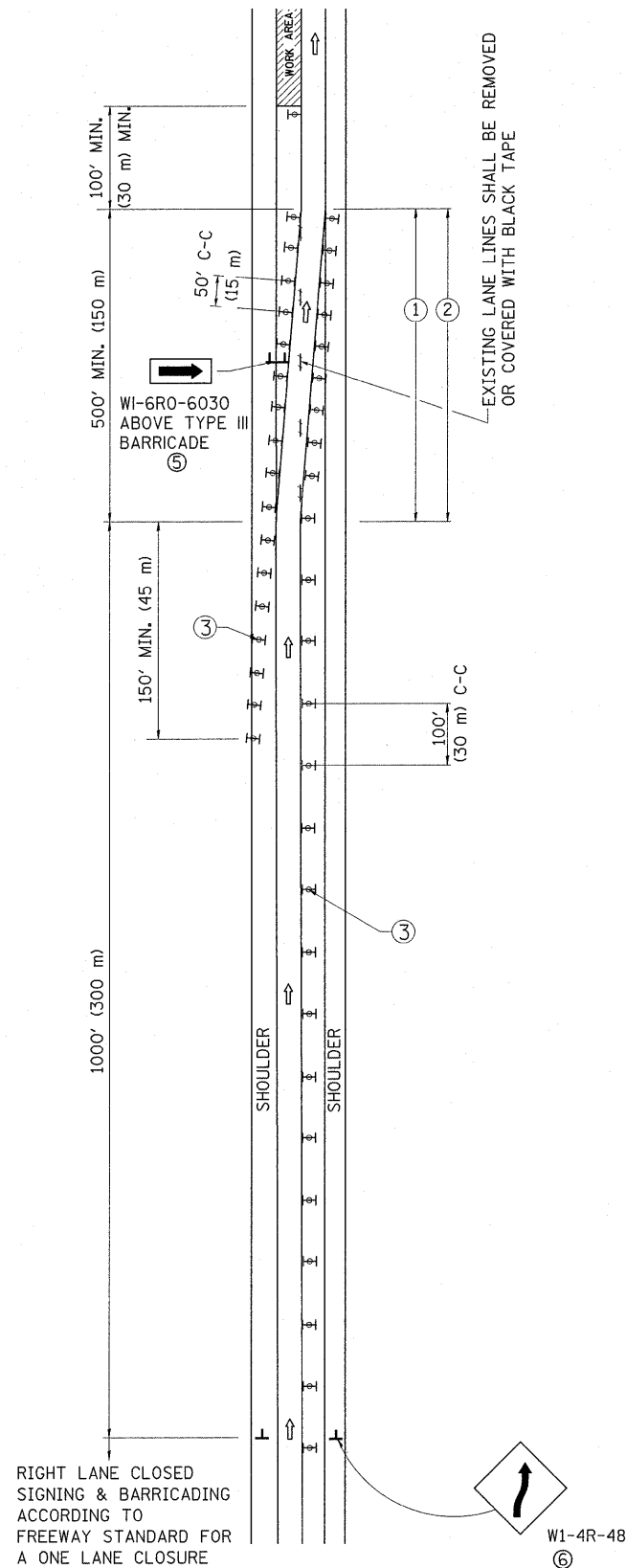
GENERAL NOTES:

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED TWENTY-FOUR (24) HOURS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED ON CLOSURES LESS THAN 24 HOURS IN DURATION.

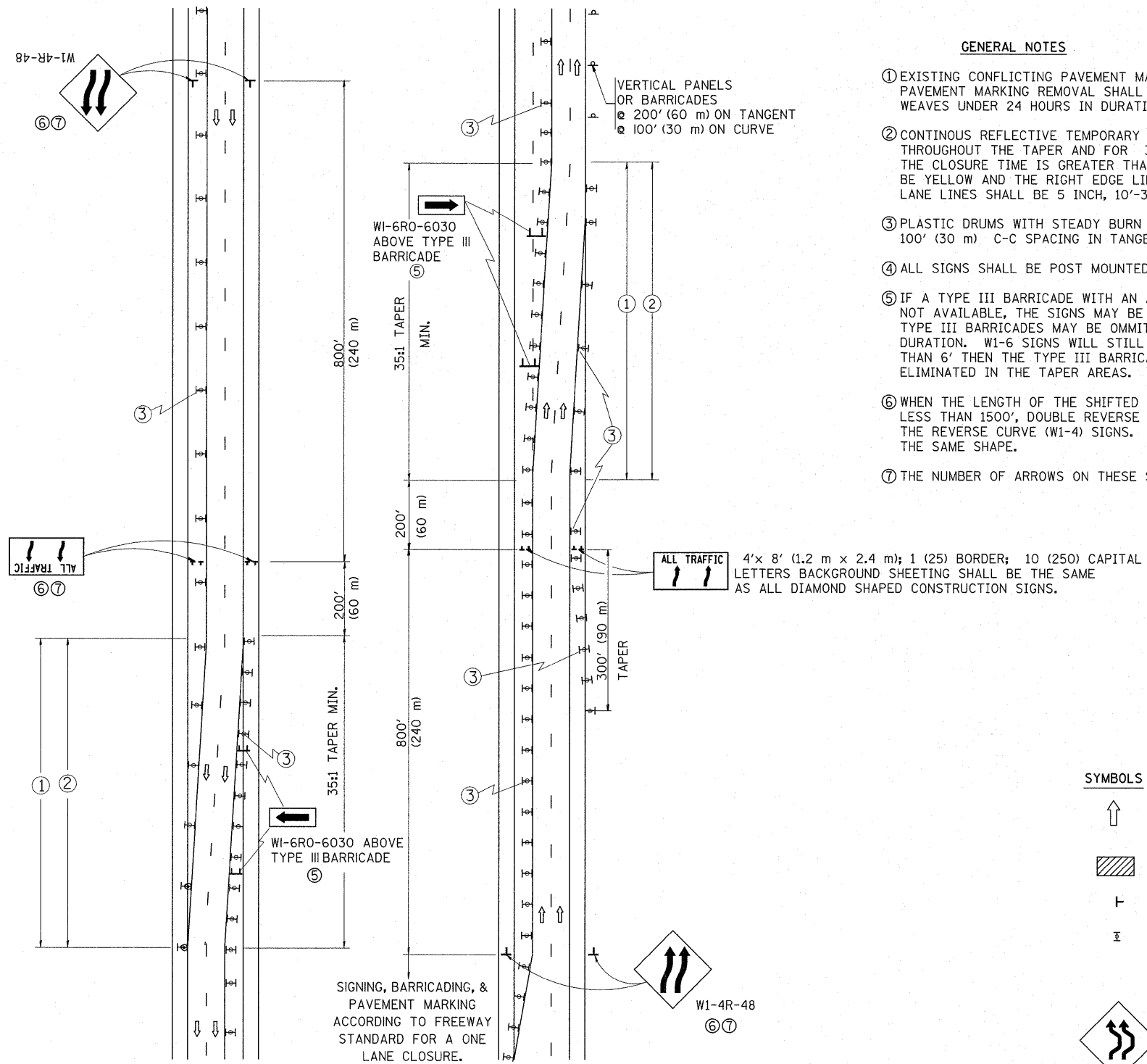
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = gorengautab	DESIGNED - DWS	REVISED - DWS/JAF 12-02	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FREEWAY ENTRANCE AND EXIT RAMP CLOSURE DETAILS			F.A.I. RTE. 90/94	SECTION 2011-070-1	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 21
cs:\pw_work\pwidot\gorengautab\d0281223\162711-sht-pln.dgn	PLOT SCALE = 50.0000 / 1" = 100'	DRAWN -	REVISED - JAF 02-06		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	TC-08		CONTRACT NO. 60P89		
	PLOT DATE = 11/3/2011	CHECKED -	REVISED - SPB 01-07		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
		DATE - 02-83	REVISED - SPB 12-09									

SINGLE LANE WEAVE



MULTI-LANE WEAVE



GENERAL NOTES

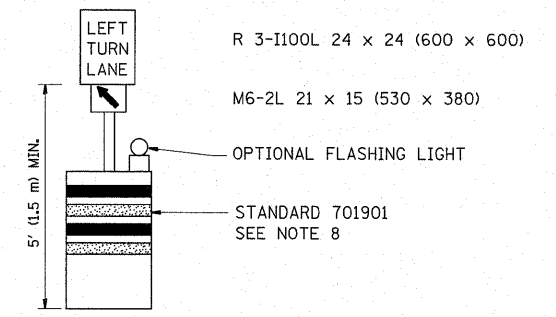
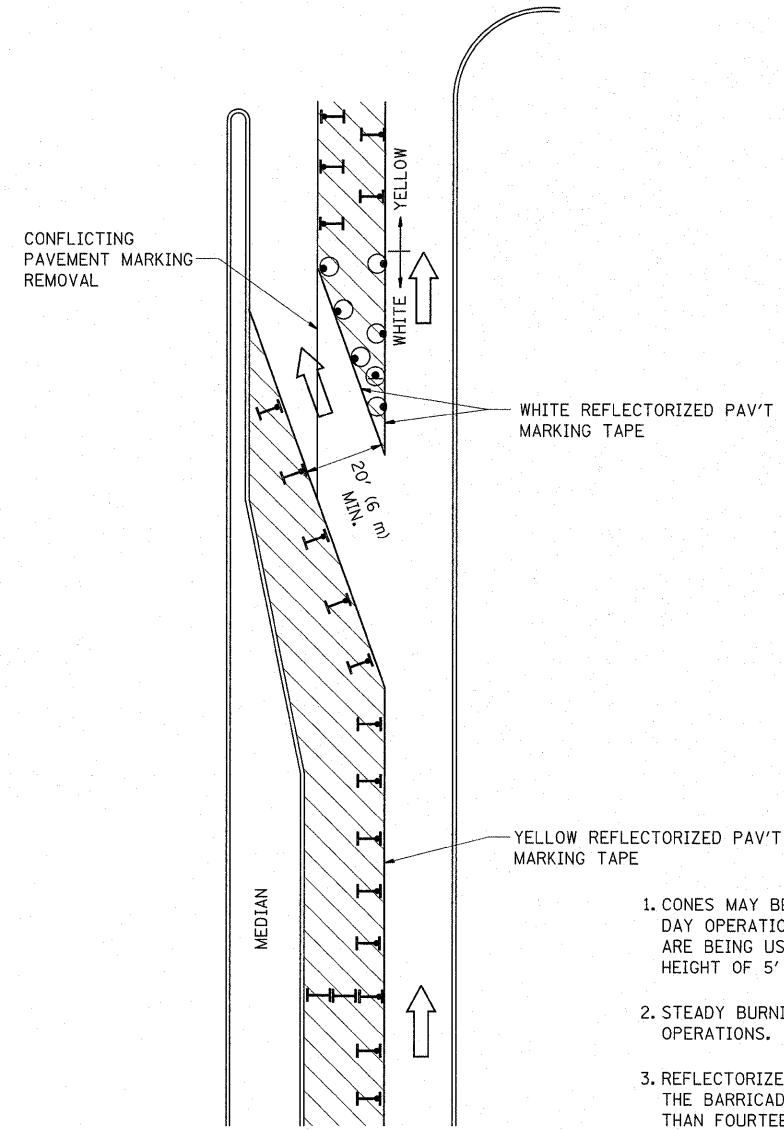
- EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 24 HOURS IN DURATION.
- CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 IS NOT AVAILABLE, THE SIGNS MAY BE MOUNTED ON NCHRP 350 TEMPORARY SIGN SUPPORTS. TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. W1-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
- WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
- THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

SYMBOLS

- DIRECTION OF TRAFFIC
 - WORK AREA
 - SIGN ON PORTABLE OR PERMANENT SUPPORT
 - TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- W24-1-48

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = gorengautab	DESIGNED - DWS	REVISED - JAF 01-03	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE & MULTI-LANE WEAVE			F.A.I. RTE. 90/94	SECTION 2011-070-I	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 22
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	PLOT DATE = 11/3/2011	CHECKED -	REVISED - SPB 01-07		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
		DATE - 02-87	REVISED - SPB 12-09									

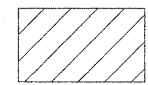







GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHR 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

FILE NAME =	USER NAME = gorengautab	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
cs:\pw_work\pvidot\gorengautab\d0281223\162711-sht-plen.dgn		REVISED - A. HOUSEH 11-07-95	REVISED -
PLOT SCALE = 50,0000' / 1in.		REVISED - A. HOUSEH 10-12-96	REVISED -
PLOT DATE = 11/3/2011		REVISED - T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

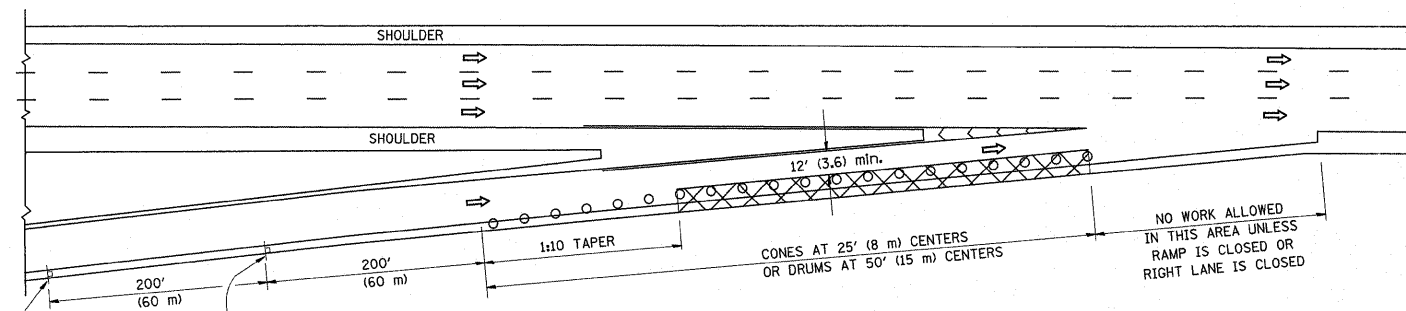
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

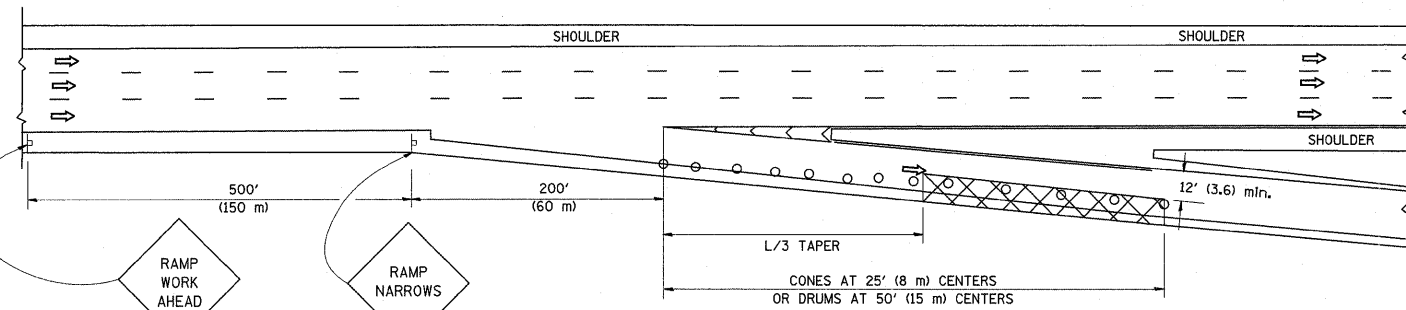
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2011-070-1	COOK	27	23
TC-14		CONTRACT NO. 60P89		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

PARTIAL RAMP CLOSURE DETAILS

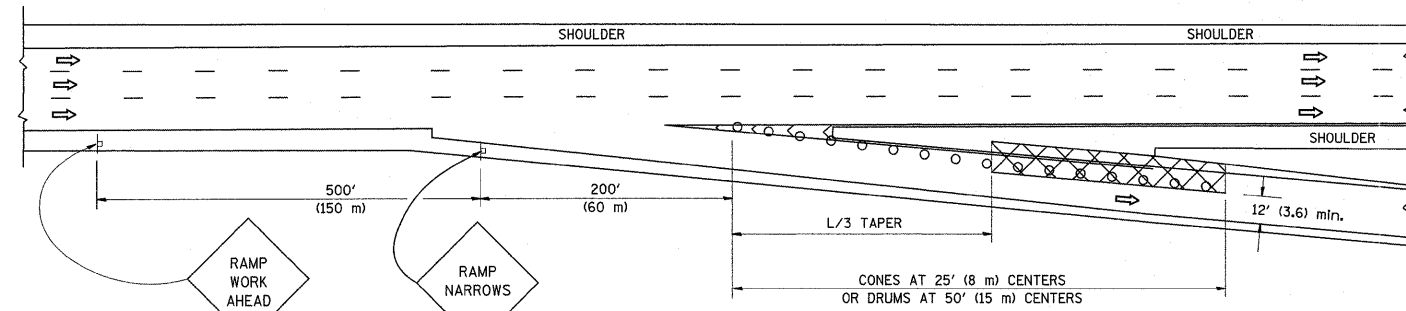
SHOULDER CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

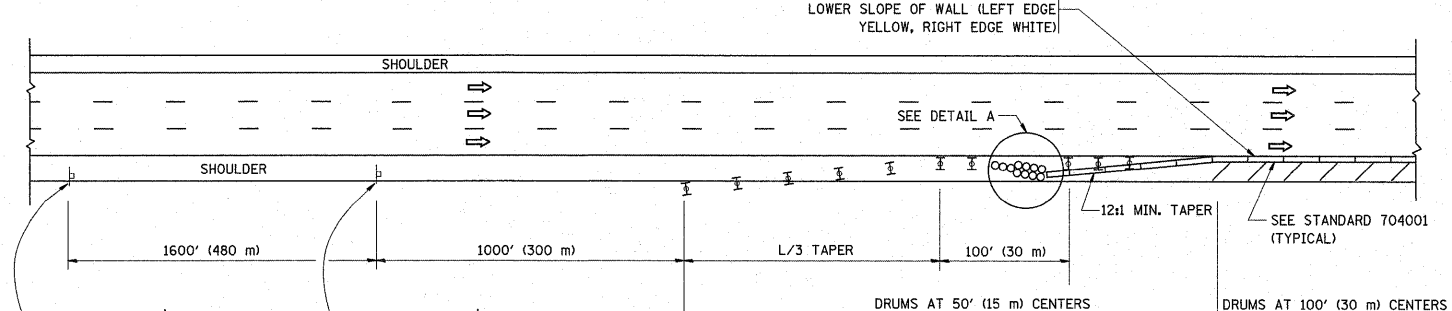
- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE

GENERAL NOTES

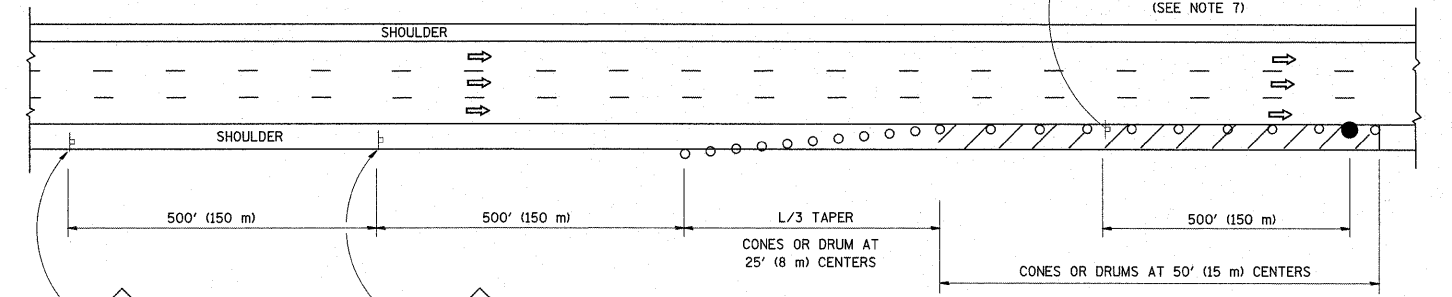
1. THE "L" DISTANCE EQUALS:

SPEED LIMIT	FORMULAS
45 mph (80 km/h)	METRIC ENGLISH
OR GREATER:	$L=0.65(W)(S)$ $L=(W)(S)$

W = WIDTH OF OFFSET IN FEET (METERS)
 S = NORMAL POSTED SPEED MPH (KM/H)
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

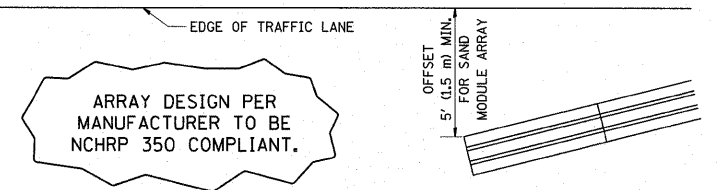


PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

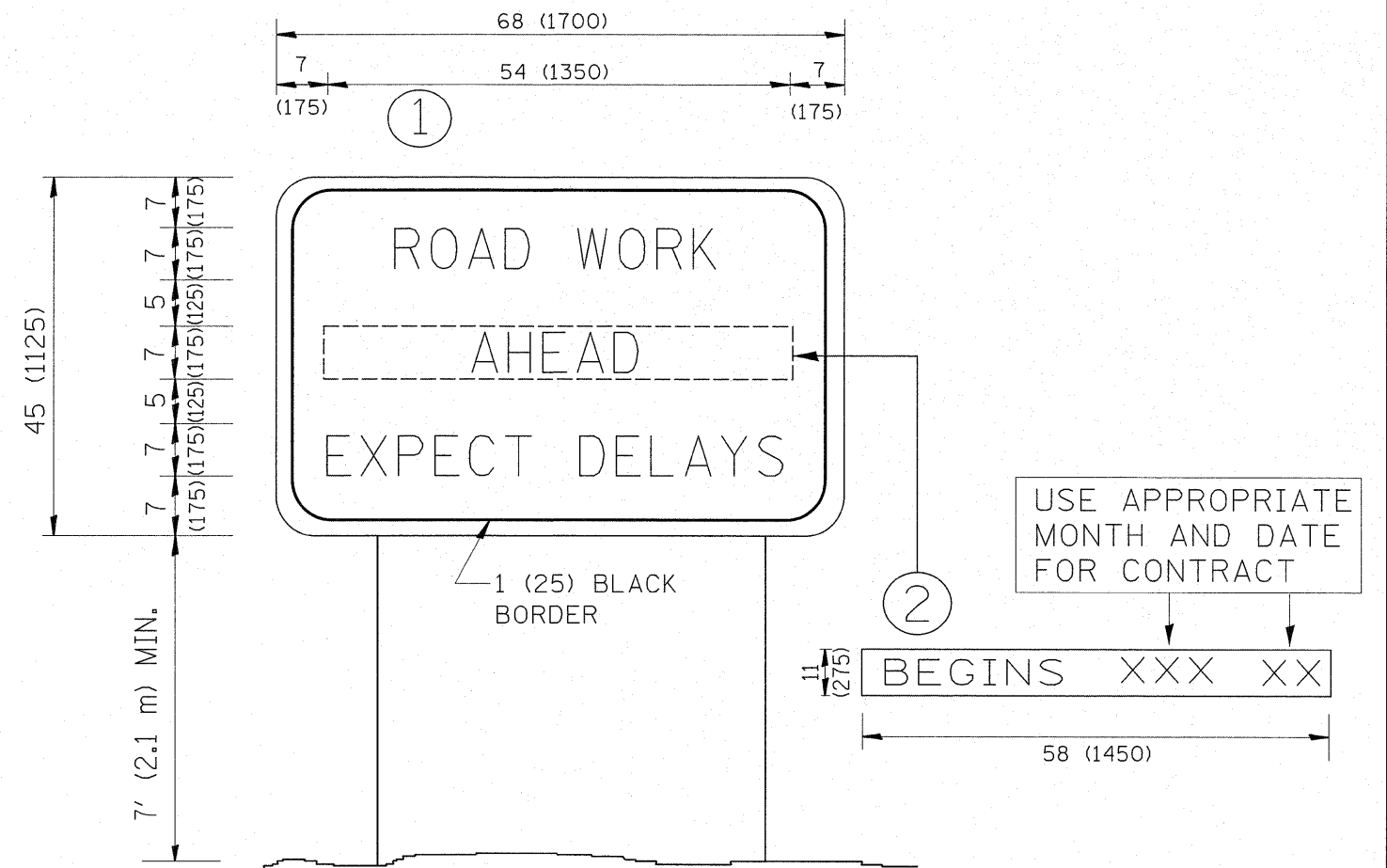
THIS DETAIL IS USED WHERE:
 1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCR OACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



DETAIL "A"
 IMPACT ATTENUATOR, TEMPORARY
 (SEE NOTE 5)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = gorengautab	DESIGNED -	REVISED - 04-03	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cd:\pw_work\pwidot\gorengautab\d0281223\162711-shr-plan.dgn	162711-shr-plan.dgn	DRAWN - D.W.S.	REVISED - J.A.F. 12-06		90/94	2011-070-I	COOK	27	24			
PLOT SCALE = 50.0000' / 1"		CHECKED -	REVISED - S.P.B. 01-07		TC-17			CONTRACT NO. 60P89				
PLOT DATE = 11/3/2011		DATE - 11-96	REVISED - S.P.B. 12-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

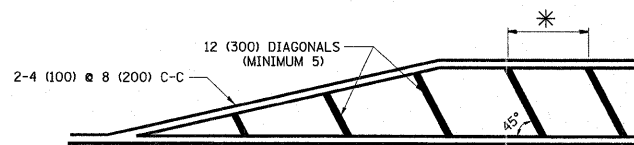
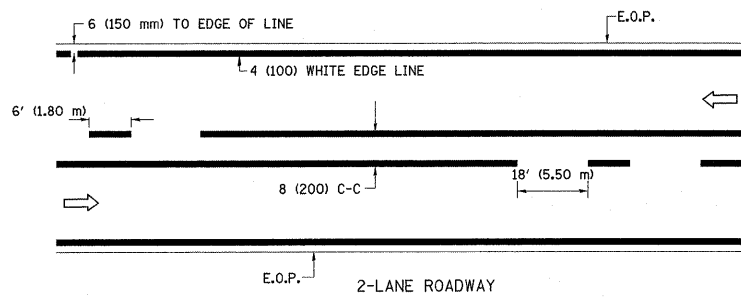
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	PLOT DATE = 11/3/2011	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

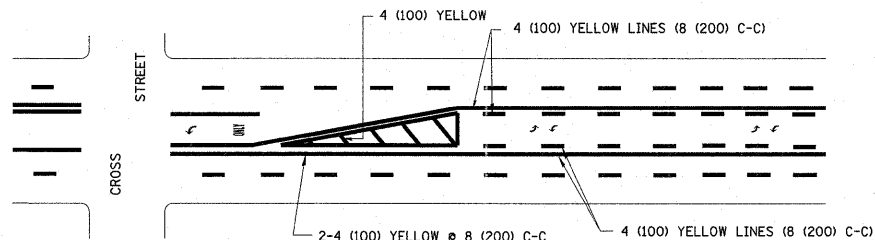
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-22		CONTRACT NO. 60P89		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

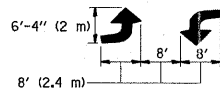


* FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
* DIAGONAL LINE SPACING: 20' (6.1 m) C-C

PAINTED MEDIANS

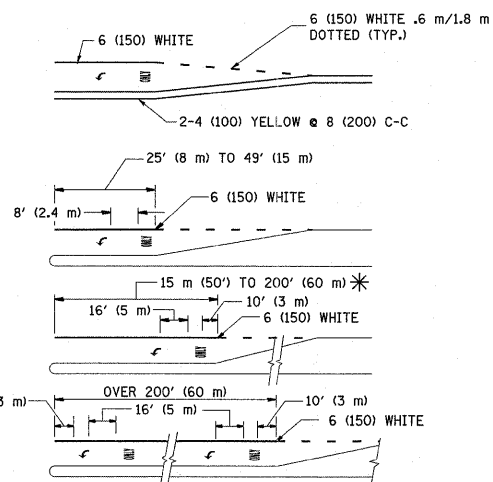


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

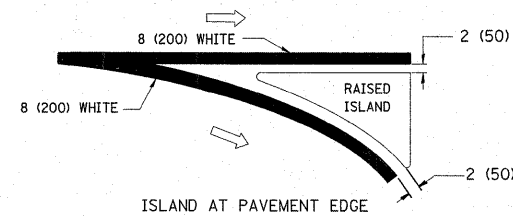
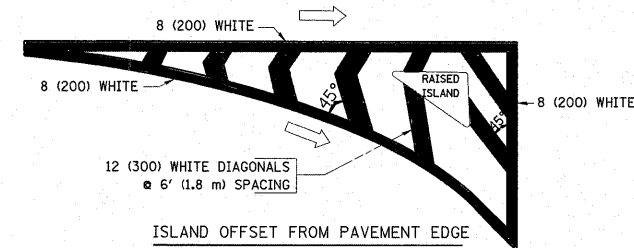


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.8 SQ. FT. (1.47 m²) ONLY AREA = 22.9 SQ. FT. (2.13 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

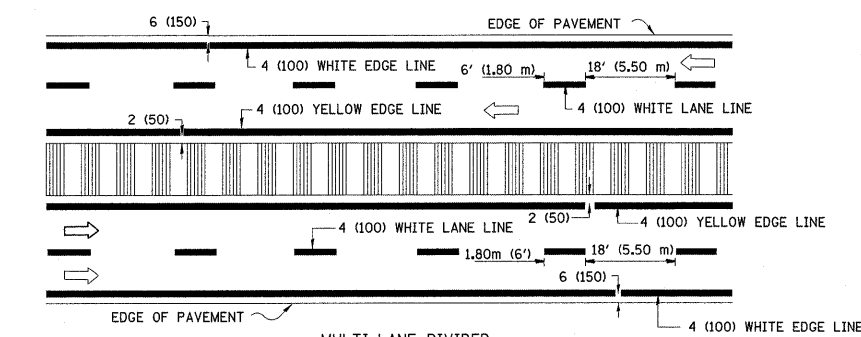
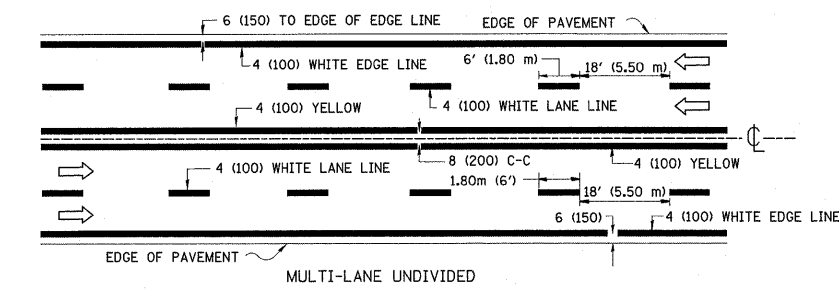


TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	8 (200) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	8 (200) C-C
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	6' (1.8 m) LINE WITH 18' (5.50 m) SPACE FOR SKIP-DASH; 8 (200) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 8 (200) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2'-4" (700) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	8 (200) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 20' (6.1 m) (LESS THAN 30 MPH (50 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)

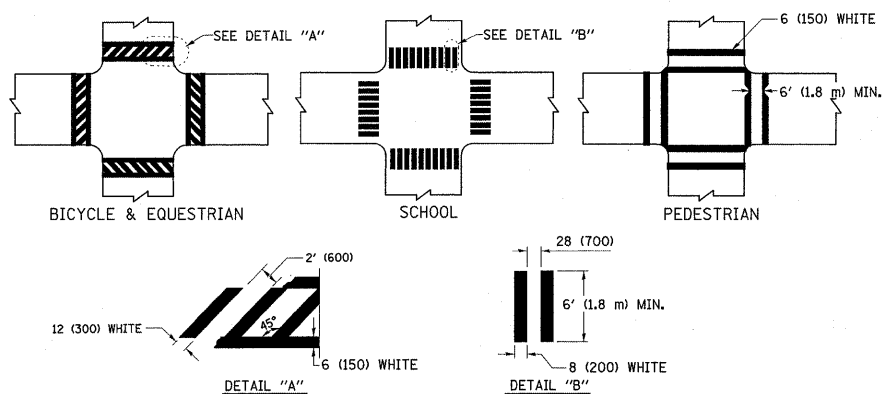
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STREET MARKING STANDARDS, PRINTED BY CITY OF CHICAGO, DEPARTMENT OF TRANSPORTATION, BUREAU OF TRAFFIC.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

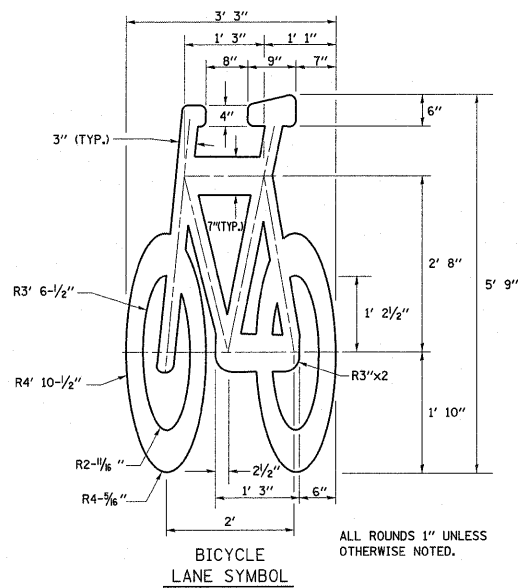
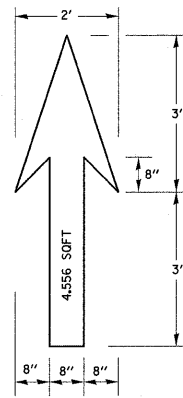
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PLOT SCALE = 50.0000 / 1 in.		CHECKED -	REVISED -
PLOT DATE = 11/3/2011		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET NO. 1 OF 2 SHEETS STA. TO STA.

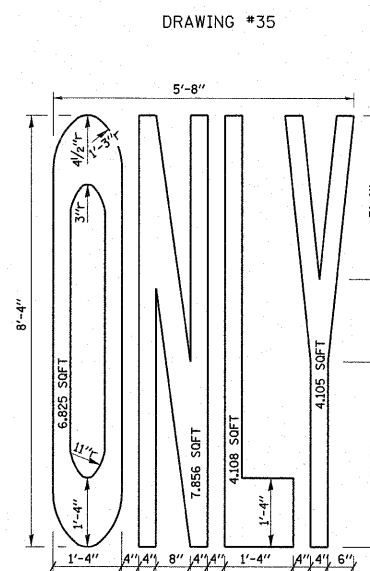
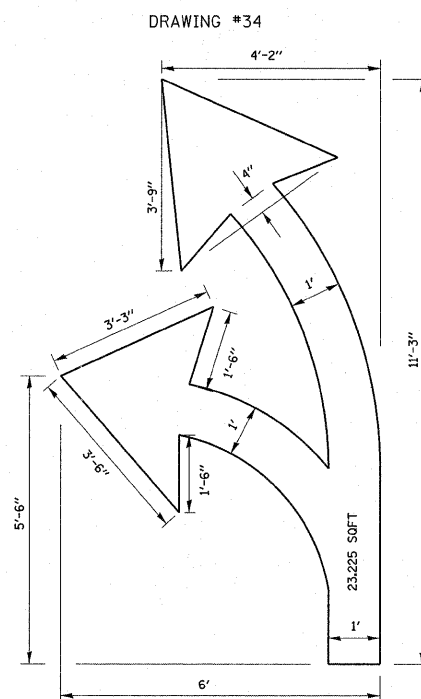
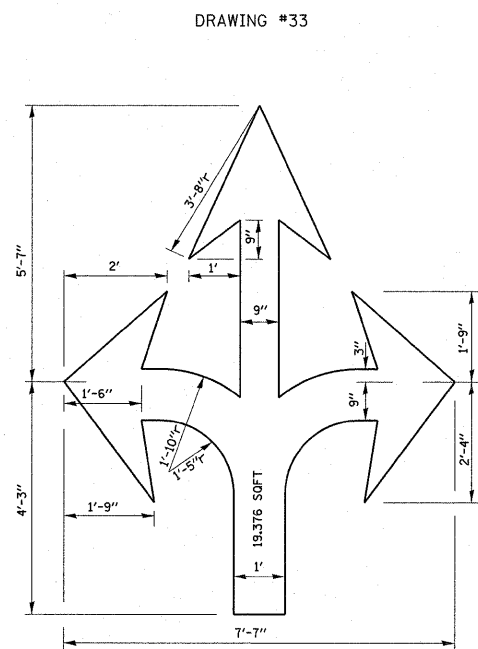
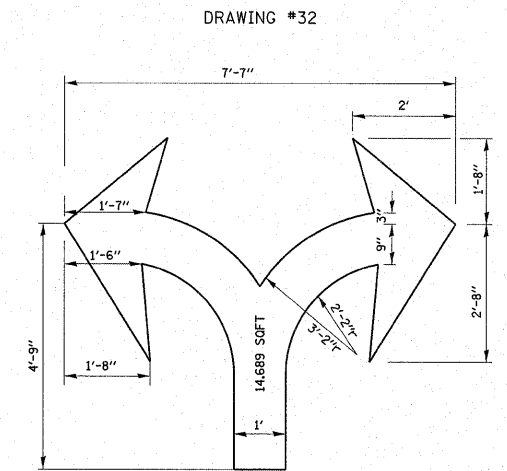
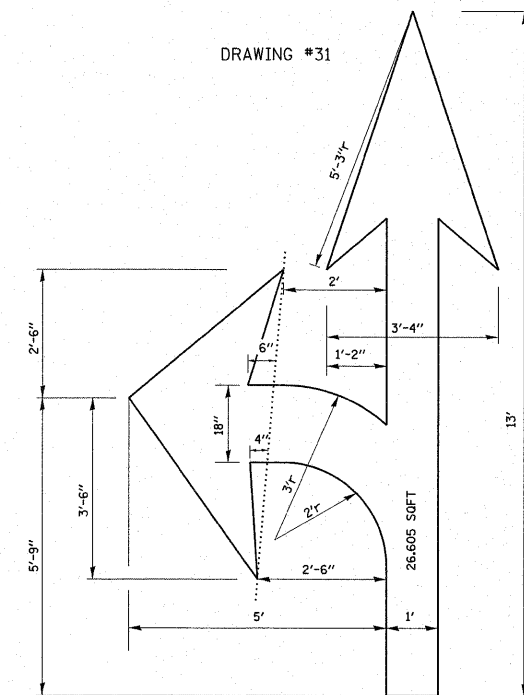
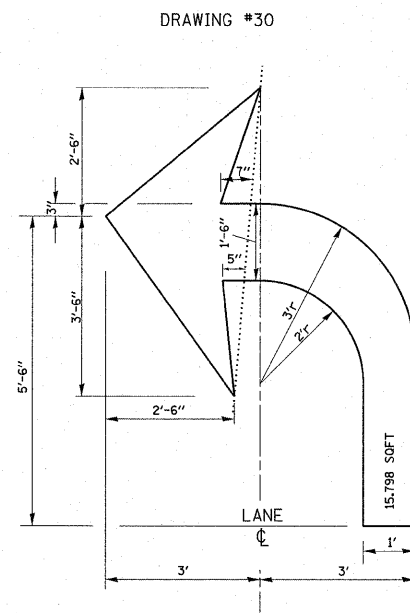
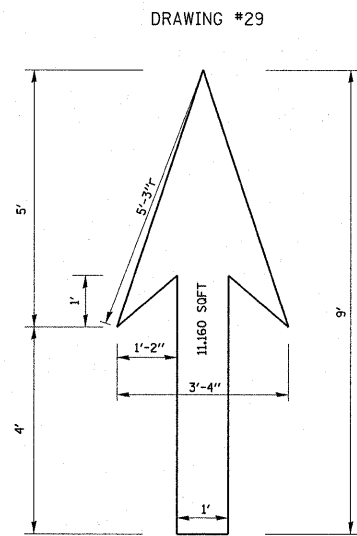
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2011-070-I	COOK	27	26
TC-24		CONTRACT NO. 60P89		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTE:

- 1.) FOR BIKE LANE SYMBOLS ONLY, USE PRE-FORMED THERMOPLASTIC WITH A MINIMUM THICKNESS OF 90 MILS, MINIMUM SKID RESISTANCE VALUE OF 60 BPN, & A MINIMUM INDEX OF REFRACTION OF 1.50.
- 2.) THE RESIDENT ENGINEER SHALL CONTACT MR. BEN GOMBERG AT 312-744-8093 AT LEAST ONE CALENDAR WEEK PRIOR TO INSTALLING BIKE LANE SYMBOLS.

TYPICAL BIKE LANE SYMBOLS
DRAWING #28



NOTE:

ALL MARKINGS SHALL BE SOLID WHITE UNLESS OTHERWISE NOTED IN THE PLANS

FILE NAME =	USER NAME = gorengautab	DESIGNED -	REVISED - T. RAMMACHER 12-07-00
es:\pw_work\pwsdot\gorengautab\d0281223\162711-sht-p1an.dgn		DRAWN -	REVISED -
PLOT SCALE = 50.0000 / 1 in.		CHECKED -	REVISED -
PLOT DATE = 11/3/2011		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A.I. RTE. 90/94	SECTION 2011-070-I	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 27
TC-24		CONTRACT NO. 60P89		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				