

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
321		PIKE	69	1
		ILLINOIS	CONTRACT NO. 72E43	

• 104RS2,BR3;105RS7,BR3,4;106RS6

D-96-071-11



LOCATION OF SECTION INDICATED THUS: - [shaded box] -

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# STATE OF ILLINOIS 1-20-2012 LETTING ITEM 085

## DEPARTMENT OF TRANSPORTATION

### DIVISION OF HIGHWAYS

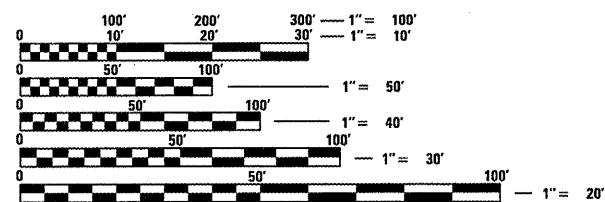
# PROPOSED HIGHWAY PLANS

FAP ROUTE 321 (US 54)  
 SECTION 104RS2,BR3;105RS7,BR3,4;106RS6  
 PROJECT NHF-0321(014)  
 SMART RESURFACING, BRIDGE REPAIRS  
 PIKE COUNTY

C-96-071-11

## IDOT HIGHWAY STANDARDS

000001-06	701006-03	701321-12
442201-03	701011-02	701326-04
606006-02	701201-04	701501-06
635006-03	701301-04	701901-02
635011-02	701306-03	780001-03
701001-02	701311-03	781001-03



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
 1-800-892-0123  
 OR 811

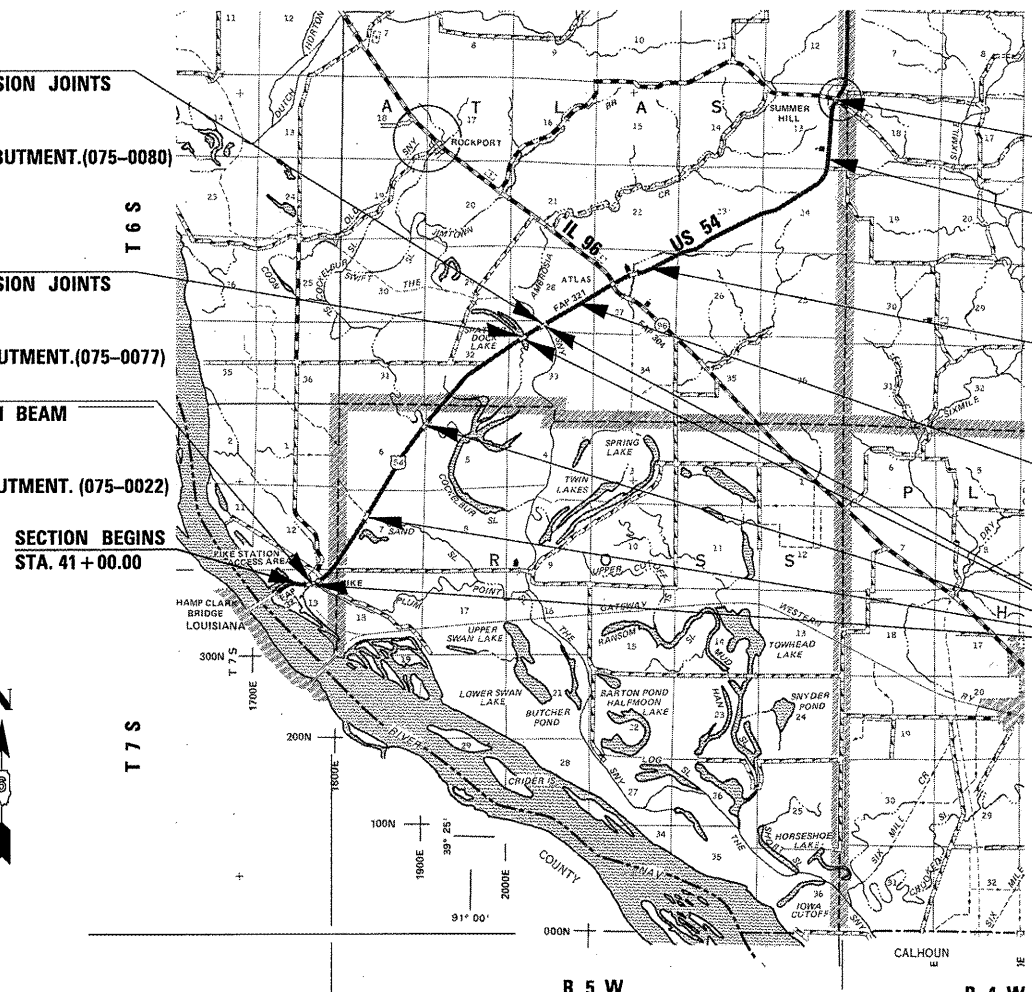
SENIOR TEAM LEADER: MARK DUST (217)-785-0597  
 TEAM ENGINEER: MARK DUST (217)-785-0597

CONTRACT NO. 72E43

STA. 274 + 40  
 REPLACEMENT OF EXPANSION JOINTS WITH STRIP SEAL  
 BRIDGE WIDTH: 46'-0"  
 151'-10" BK TO BACK ABUTMENT. (075-0080)

STA. 258 + 50  
 REPLACEMENT OF EXPANSION JOINTS WITH STRIP SEAL  
 BRIDGE WIDTH: 46'-0"  
 137'-8" BK TO BACK ABUTMENT. (075-0077)

STA. 49 + 51.00  
 REPLACEMENT OF NORTH BEAM AND BRIDGE RAIL  
 BRIDGE WITH: 45'-0"  
 33'-0" BK TO BK OF ABUTMENT. (075-0022)



SECTION ENDS  
 STA. 552 + 34.00

STATION EQUATION  
 STA. 508 + 94.98 BK. =  
 STA. 509 + 81.78 AHD

STATION EQUATION  
 STA. 365 + 23.40 BK. =  
 STA. 365 + 30.93 AHD

STATION EQUATION  
 STA. 303 + 40.90 BK. =  
 STA. 325 + 00.00 AHD

BRIDGE PAVING OMISSION  
 STA. 257 + 81.67 TO 259 + 18.33  
 STA. 273 + 65.39 TO 275 + 16.22  
 STA. 172 + 84.25 TO 173 + 39.75  
 STA. 104 + 29.25 TO 104 + 72.75  
 STA. 49 + 35.00 TO 49 + 67.00

ADT = 2200 (2009)  
 PV = 1804 (82.0%)  
 SU = 117 (5.3%)  
 MU = 279 (12.7%)

GROSS LENGTH = 488 + 80.57 FT. = 9.258 MILE  
 NET LENGTH = 484 + 62.08 FT. = 9.178 MILE  
 HIGHWAY CLASSIFICATION = OTHER PRINCIPAL ARTERIAL

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
 DIVISION OF HIGHWAYS

SUBMITTED Oct 31 20 11

Ray Z. Dalk  
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

December 9 20 11  
Scott E. Stitt P.E.  
 acting ENGINEER OF DESIGN AND ENVIRONMENT

December 9 20 11  
William R. Freyler  
 DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY  
 OF THE STATE OF ILLINOIS

## GENERAL NOTES

- 1 ALL ELEVATIONS SHOWN IN THE PLANS ARE U. S. G. S. MEAN SEA LEVEL DATUM.
- 2 ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS SHALL BE INTERPRETED TO BE THE LATEST STANDARDS OF THE DEPARTMENT AS INCLUDED IN THE BACK OF THE PLANS.
- 3 THE THICKNESS OF HOT-MIX ASPHALT SHOWN ON THE PLANS IS THE NORMAL THICKNESS. DEVIATIONS FROM THE NORMAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HOT-MIX ASPHALT IS PLACED.
- 4 SEEDING WILL NOT BE PERMITTED AT ANY TIME WHEN THE GROUND IS FROZEN, WET OR IN AN UNTILLABLE CONDITION. AREAS TO BE SEEDED SHALL BE DETERMINED BY THE ENGINEER.
- 5 EXISTING PAVEMENT DAMAGED DUE TO THE CONTRACTOR'S OPERATIONS, AND NOT OTHERWISE NECESSARY TO REPLACE, SHALL BE REPLACED AT THE EXPENSE OF THE CONTRACTOR.
- 6 THE LOCATIONS OF THOSE BURIED AND ABOVE GROUND UTILITIES SHOWN ARE APPROXIMATE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY FROM CONSTRUCTION OPERATIONS AS OUTLINE IN ARTICLE 107.26 OF THE STANDARD SPECIFICATIONS. THE J.U.L.I.E. NUMBER IS 800-892-0123. A MINIMUM OF FORTY-EIGHT HOURS ADVANCED NOTICE IS REQUIRED.
- 7 THE EXISTING ROAD SIGNS THAT INTERFERE WITH CONSTRUCTION SHALL BE RELOCATED AS DIRECTED BY THE ENGINEER. AFTER THE CONSTRUCTION IS COMPLETED, THE CONTRACTOR WILL REPLACE THE SIGNS AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- 8 ABANDONED UNDERGROUND UTILITIES THAT CONFLICT WITH CONSTRUCTION SHALL BE DISPOSED OUTSIDE THE LIMITS OF RIGHT-OF-WAY ACCORDING TO ARTICLE 202.03 OF THE STANDARDS SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF EARTH EXCAVATION AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- 9 WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED THE DEPARTMENT SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKERS AND MONUMENTS UNTIL THE OWNER AND AUTHORIZED SURVEYOR OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION. IF THE ENGINEER DECIDES TO HAVE THE CONTRACTOR RESET THE MONUMENT, THIS WORK WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04.
- 10 NO PASSING ZONES SHALL BE FIELD VERIFIED BY OPERATIONS, DIST. 6, (217)785-5312 14 DAYS PRIOR TO FINAL PAVEMENT MARKINGS.

11 THE FOLLOWING APPLICATION RATES WERE USED FOR QUANTITY CALCULATIONS.

AGGREGATE ITEMS	2.05 TON / CU YD
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	0.056 TON / sq. yd. •inch
INCIDENTAL BITUMINOUS SURFACE	0.056 TON / sq. yd. •inch
AGGREGATE (PRIME COAT)	0.002 TON / sq. yd
BITUMINOUS MATERIAL (PRIME COAT) (ON BITUMINOUS)	0.00038 TON / sq. yd
AGGREGATE SHOULDERS TYPE B	2.05 TON / CU YD
RIPRAP	1.5 TON / CU YD

12 THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT.

MIXTURE USE(S):	HMA SURFACE	HMA INCIDENTAL SURFACE
PG	PG 64-22	PG 64-22
DESIGN AIR VOIDS	4.0% @ N DESIGN = 50	4.0% @ N DESIGN = 50
MIXTURE COMPOSITION (GRADATION MIXTURE)	IL 9.5 OR 12.5	9.5 OR 12.5
FRICTION AGGREGATE	MIX "C"	MIX "C"

## COMMITMENTS

1. THE RESIDENT ENGINEER SHALL CONTACT STUDIES & PLANS CONCERNING ANY MAJOR PLAN CHANGE, TO MAKE SURE NO PREVIOUS COMMITMENTS (NOT LISTED) WERE MADE AFFECTING THE DESIGN AND ALLOW AN IMPROVED DESIGN FOR FUTURE PROJECTS.

EXAMINED <u>Oct 20</u> 20 <u>11</u> <i>Jimmy</i>
PROJECT IMPLEMENTATION ENGINEER
EXAMINED <u>Oct 25</u> 20 <u>11</u> <i>ZRM</i>
PROGRAM DEVELOPMENT ENGINEER
DISTRICT SIX
EXAMINED <u>October 6</u> 20 <u>11</u> <i>Carl Walker</i>
OPERATIONS ENGINEER

• 104RS2, BR3, 105RS7, BR3, 4106RS6

**ILLINOIS DEPARTMENT OF TRANSPORTATION  
SUMMARY OF QUANTITIES**

LOCATION OF WORK				CONSTRUCTION TYPE CODE		
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY NHS 80% FED / 20% STATE 0005	BRIDGE REPAIRS NHS 80% FED / 20% STATE 0014 075-0022, -0077, -0080	
20300100	CHANNEL EXCAVATION	CU YD	27	27		
28100707	STONE DUMPED RIPRAP, CLASS A4	SQ YD	17	17		
28100709	STONE DUMPED RIPRAP, CLASS A5	SQ YD	334	334		
28100807	STONE DUMPED RIPRAP, CLASS A4	TON	14	14		
28200200	FILTER FABRIC	SQ YD	360	360		
40200800	AGGREGATE SURFACE COURSE, TYPE B	TON	133	133		
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	67	67		
40600300	AGGREGATE (PRIME COAT)	TON	349	349		
40600895	CONSTRUCTING TEST STRIP	EACH	1	1		
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	4,433	4,433		
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SQ YD	71	71		
40600990	TEMPORARY RAMP	SQ YD	352	352		
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX " C ", N50	TON	14,552	14,552		
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	666	666		
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1-1/2"	SQ YD	169,522	169,522		
44000300	CURB REMOVAL	FOOT	500	500		
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	9	9		
44200144	PAVEMENT PATCHING, TYPE II, 12 INCH	SQ YD	473	473		
44200148	PAVEMENT PATCHING, TYPE III, 12 INCH	SQ YD	100	100		
48101200	AGGREGATE SHOULDERS, TYPE B	TON	3,229	3,229		
50102400	CONCRETE REMOVAL	CU YD	30.3		30.3	
50105220	PIPE CULVERT REMOVAL	FOOT	63	63		
50300255	CONCRETE SUPERSTRUCTURE	CU YD	30.1		30.1	
50300300	PROTECTIVE COAT	SQ YD	69		69	
50400105	PRECAST CONCRETE BRIDGE SLAB	SQ FT	120.0		120.0	

• 104RS2, BR3; 105RS7, BR3, 4; 106RS6

FILE NAME :	USER NAME : sparksgu	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwwork\pwwork\sparksgu\d0265393\067	BE*3*skt-500.dgn	DRAWN -	REVISED -					321	-	PIKE	69	3
	PLOT SCALE : 179.8534 ' / in.	CHECKED -	REVISED -		CONTRACT NO. 72E43			ILLINOIS FED. AID PROJECT				
	PLOT DATE : Oct-31-2011 03:46:22PM	DATE -	REVISED -		SCALE:	SHEET NO. 1 OF 3 SHEETS	STA.	TO STA.				

**ILLINOIS DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

LOCATION OF WORK				CONSTRUCTION TYPE CODE		
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY NHS 80% FED / 20% STATE 0005	BRIDGE REPAIRS NHS 80% FED / 20% STATE 0014 075-0022, -0077, -0080	
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	3,500		3500	
50800515	BAR SPLICERS	EACH	56		56	
50900205	STEEL RAILING, TYPE S1	FOOT	64		64	
52000110	PREFORMED JOINT STRIP SEAL	FOOT	183		183	
54248510	CONCRETE COLLAR	CU YD	2	2		
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	60	60		
550A0070	STORM SEWERS, CLASS A, TYPE 1 15"	FOOT	70	70		
59300100	CONTROLLED LOW-STRENGTH MATERIAL	CU YD	40	40		
60237000	INLETS, TYPE A, TYPE 15 FRAME AND LID	EACH	2	2		
60600095	CLASS SI CONCRETE (OUTLET)	CU YD	3	3		
60600605	CONCRETE CURB, TYPE B	FOOT	500	500		
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	9	9		
*63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	1	1		
63304385	TRAFFIC BARRIER TERMINAL REMOVAL, TYPE 1	EACH	1	1		
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	8	8		
67100100	MOBILIZATION	L SUM	1	1		
70100405	TRAFFIC CONTROL AND PROTECTION, STANDARD 701321	EACH	3		3	
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1		
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1		
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	5	5		
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	3		3	
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	8,888	8,888		
70300230	TEMPORARY PAVEMENT MARKING - LINE 5"	FOOT	126,556	126,556		
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	90	90		

\*SPECIALTY ITEM

• 104RS2,BR3;105RS7,BR3,4;106RS6

FILE NAME :	USER NAME : sparksgv	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwork\pvidot\sparksgv\d0265393\067243-ph-500.dgn	PLT SCALE : 174.5634' / in.	DRAWN -	REVISED -					321	*		69	4
	PLT DATE : Oct-31-2011 03:46:23PM	CHECKED -	REVISED -		SCALE:	SHEET NO. 1 OF 3 SHEETS	STA.	TO STA.	CONTRACT NO. 72E43			
		DATE -	REVISED -						ILLINOIS FED. AID PROJECT			



**ILLINOIS DEPARTMENT OF TRANSPORTATION  
SUMMARY OF QUANTITIES**

LOCATION OF WORK				CONSTRUCTION TYPE CODE		
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY NHS 80% FED / 20% STATE 0005	BRIDGE REPAIRS NHS 80% FED / 20% STATE 0014 075-0022, -0077, -0080	
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	1,852	1,852		
70400100	TEMPORARY CONCRETE BARRIER	FOOT	1,000		1,000	
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	1,000		1,000	
*78001120	PAINT PAVEMENT MARKING - LINE 5"	FOOT	126,556	126,556		
*78004280	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	FOOT	90	90		
*78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	620.0	620		
*78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	1	1		
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	620	620		
X0324744	REMOVAL OF EXISTING PRECAST CONCRETE UNITS	SQ FT	120		120	
X5015225	PIPE CULVERT REMOVAL (SPECIAL)	FOOT	12	12		
X5030275	CONCRETE WEARING SURFACE REMOVAL	SQ YD	1.8		1.8	
X5030305	CONCRETE WEARING SURFACE, 5"	SQ YD	15.1		15.1	
<del>50800530</del>	MECHANICAL SPLICERS	EACH	33		33.0	
*X7200201	WIDTH RESTRICTION SIGNING	LSUM	1		1	
*Z0030260	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	6		6	
*Z0030332	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE NARROW), TEST LEVEL 3	EACH	6		6	

\*SPECIALTY ITEM

• 104RS2, BR3; 105RS7, BR3, 4; 106RS6

FILE NAME =	USER NAME = sparksgn	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
en:\pwork\puidot\sparksgn\d0265393\0672E43-shc-500.dgn	DRAWN -	REVISED -	321						PIKE	69	5	
PLOT SCALE = 1/4" = 1'-0"	CHECKED -	REVISED -	CONTRACT NO. 72E43									
PLOT DATE = Oct-31-2011 03:46:23PM	DATE	REVISED -	SCALE:		SHEET NO. 1 OF 3 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT				

**HMA PAVEMENT SCHEDULE**

SECTION		LENGTH	ROADWAY WIDTH	HMA SHOULDER WIDTH	AGGRAGATE SHOULDER WIDTH	ROADWAY AREA	HMA SHOULDER AREA	HMA SURF. REM. - BUTT JOINT	TEMPORARY RAMP	AGG. SHOULDER TYPE B	HMA SURFACE REMOVAL. 3/4" VAR. DEPTH	HMA SURFACE REMOVAL. 1-1/2"	HMA SHOULDER 6"	BIT. (P. C.)	AGG. (P. C.)	HMA SURFACE COURSE, MIX. "C", N50	INCIDENTAL HMA SURFACING
"FROM" STATION	"TO" STATION	( RT/LT)	FOOT	FT	FT	FT.	SO. FT.	SO. FT.	SO. YD.	SO. YD.	TON	SO. YD.	SO. YD.	TON	TON	TON	TON
41+00.00	49+35.00		835.00	24	8	8	20040	6680	107	18	63.4	2969		1.13	5.94	249.39	
<b>BRIDGE OMISSION</b>														0.00	0.00	0.00	
49+67.00	51+11.15		144.15	24	8	8	3460	1153	107	18	10.9	513		0.19	1.03	43.05	
51+11.15	52+63.15		152.00	24	8	8	3648	1216			11.5	540		0.21	1.08	45.40	
52+63.15	63+26.27		1063.12	24	8	8	25515	8505			80.7	3780		1.44	7.56	317.52	
63+26.27	104+29.25		4102.98	24	8	8	98472	32824	107	18	311.5	14588		5.54	29.18	1225.42	
<b>BRIDGE OMISSION</b>														0.00	0.00	0.00	
104+72.75	172+84.25		6811.50	24	8	8	163476	54492	213	36	517.2	24219		9.20	48.44	2034.37	
<b>BRIDGE OMISSION</b>														0.00	0.00	0.00	
173+39.75	224+50.00		5110.25	24	8	8	122646	40882	107	18	388.0	18170		6.90	36.34	1526.26	
224+50.00	225+85.00		135.00	24	8	8	3240	1080			10.3	480		0.18	0.96	40.32	
225+85.00	244+50.00		1865.00	24	8	8	44760	14920			141.6	6631		2.52	13.26	557.01	
244+50.00	257+81.67		1331.67	24	8	8	31960	10653	107	18	101.1	4735		1.80	9.47	397.73	
<b>BRIDGE OMISSION</b>														0.00	0.00	0.00	
259+18.33	273+65.39		1447.06	24	8	8	34729	11576	213	36	109.9	5145		1.96	10.29	432.19	
<b>BRIDGE OMISSION</b>														0.00	0.00	0.00	
275+16.22	275+39.22		23.00	24	8	8	552	184	107	18	1.7	82		0.03	0.16	6.87	
275+39.22	303+40.90		2801.68	24	8	8	67240	22413			212.7	9962		3.79	19.92	836.77	
303+40.90BK	325+00.00AHD	STATION EQUATION												0.00	0.00	0.00	
325+00.00	340+00.00		1500.00	24	8	8	36000	12000			113.9	5333		2.03	10.67	448.00	
340+00.00	340+32.28		32.28	26	4	4	839	129			1.2	108		0.04	0.22	9.04	
340+32.28	341+71.00		138.72	24	4		3329	555				432		0.16	0.86	36.25	
341+71.00	344+51.00		280.00	24			6720					747		0.28	1.49	62.72	
344+51.00	346+60.67		209.67	24	6		5032	1258				699		0.27	1.40	58.71	
346+60.67	350+60.00	RT	399.33	12			4792					532		0.20	1.06	44.72	
346+60.67	351+54.84	LT	494.17	12			5930					659		0.25	1.32	55.35	
350+60.00	351+54.84	RT	94.84	12	7		1138	664				200		0.08	0.40	16.82	
351+54.84	359+81.61		826.77	24	14	12	19842	11575			94.2	3491		1.33	6.98	293.23	
359+81.61	360+57.07		75.46	24	12	12	1811	906			8.6	302		0.11	0.60	25.35	

• 104RS2, BR3; 105RS7, BR3, 4; 106RS6

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SCHEDULE OF QUANTITIES</b>			F.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\id\sparksgw\0265393\067E43-sht-schedule.dgn	DRAWN -	REVISED -	321					•	PIKE	69	6	
PLOT SCALE = 106.7353' / in.	CHECKED -	REVISED -	CONTRACT NO. 72E43									
PLOT DATE = Oct-31-2011 03:50:38PM	DATE -	REVISED -	SCALE:		SHEET NO. 1 OF 8 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT				

**HMA PAVEMENT SCHEDULE**

SECTION	LENGTH	ROADWAY WIDTH	HMA SHOULDER WIDTH	AGGRAGATE SHOULDER WIDTH	ROADWAY AREA	HMA SHOULDER AREA	HMA SURF. REM. - BUTT JOINT	TEMPORARY RAMP	AGG. SHOULDER TYPE B	HMA SURFACE REMOVAL. 3/4" VAR. DEPTH	HMA SURFACE REMOVAL. 1-1/2"	HMA SHOULDER 6"	BIT. (P. C.)	AGG. (P. C.)	HMA SURFACE COURSE, MIX. "C", N50	INCIDENTAL HMA SURFACING
360+57.07	365+23.40		466.33	24	14	12	11192	6529		53.1	1969		0.75	3.94	165.39	
365+23.40BK	365+30.93AHD	STATION EQUATION											0.00	0.00	0.00	
365+30.93	380+28.86		1497.93	24	14	12		20971		170.6	6325		2.40	12.65	531.27	
380+28.86	388+75.00		846.14	24	8	6	20307	6769		48.2	3008		1.14	6.02	252.71	
388+75.00	391+00.00		225.00	22	3	6	4950	675		12.8	625		0.24	1.25	52.50	
391+00.00	401+22.00		1022.00	24	3	6	24528	3066		58.2	3066		1.17	6.13	257.54	
401+22.00	408+70.00	RT	748.00	12	1.5	3	8976	1122		21.3	1122		0.43	2.24	94.25	
401+22.00	408+70.00	LT	748.00	12	1.75		8976	1309			1143		0.43	2.29	95.99	
408+70.00	410+75.00	RT	205.00	11	1.5	3	2255	308		5.8	285		0.11	0.57	23.92	
408+70.00	410+75.00	LT	205.00	11	1.17		2255	240			277		0.11	0.55	23.29	
410+75.00	411+00.00		25.00	22	3	6	550	75		1.4	69		0.03	0.14	5.83	
411+00.00	415+47.00	LT	447.00	11	1.5	3	4917	671		12.7	621		0.24	1.24	52.15	
411+00.00	415+47.00	RT	447.00	11	1.17		4917	523			604		0.23	1.21	50.77	
415+47.00	424+45.00		898.00	24	2.5		21552	2245			2644		1.00	5.29	222.11	
424+45.00	430+00.00		555.00	22	3	6	12210	1665		31.6	1542		0.59	3.08	129.50	
430+00.00	440+14.40		1014.40	24	2.5		24346	2536		0.0	2987		1.14	5.97	250.89	
440+14.40	443+00.00		285.60	22	3	6	6283	857		16.3	793		0.30	1.59	66.64	
443+00.00	454+00.00		1100.00	24	8	6	26400	8800		62.6	3911		1.49	7.82	328.53	
454+00.00	492+00.00		3800.00	24	8	6	91200	30400		216.4	13511		5.13	27.02	1134.93	
492+00.00	505+55.77		1355.77	24	8	6	32538	10846		77.2	4821		1.83	9.64	404.92	
505+55.77	508+94.98		339.21	24	8	6	8141	2714		19.3	1206		0.46	2.41	101.31	
508+94.98BK	509+81.78AHD	STATION EQUATION											0.00	0.00	0.00	
509+81.78	511+32.00		150.22	24	8	6	3605	1202		8.6	534		0.20	1.07	44.87	
511+32.00	523+35.00		1203.00	24	8	6	28872	9624		68.5	4277		1.63	8.55	359.30	
523+35.00	524+02.00		67.00	24	5	6	1608	335		3.8	216		0.08	0.43	18.13	
524+02.00	543+62.64		1960.64	24	2	6	47055	3921		111.6	5664		2.15	11.33	475.78	
543+62.64	549+16.13		553.49	24	2	6	13284	1107		31.5	1599		0.61	3.20	134.31	
549+16.13	552+34.00		317.87	24	2	6	7629	636	87	14	18.1	918	0.35	1.84	77.14	
<b>TOTALS =</b>					<b>1,159,670</b>	<b>352,810</b>	<b>1,154</b>	<b>193</b>	<b>3,229</b>		<b>168,054</b>		<b>64</b>	<b>337</b>	<b>14,117</b>	

• 104RS2, BR3; 105RS7, BR3, 4; 106RS6

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SCHEDULE OF QUANTITIES</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pwork\pwork\sparksgw\0265393\067E43-sht-schedule.dgn	DRAWN -	REVISED -	321					•	PIKE	69	7	
PLOT SCALE = 106.7353' / in.	CHECKED -	REVISED -	CONTRACT NO. 72E43									
PLOT DATE = Oct-31-2011 03:50:38PM	DATE -	REVISED -	SCALE:		SHEET NO. 2 OF 8 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT				

**HMA SIDE ROAD IMPROVEMENT SCHEDULE**

SECTION			LENGTH	SIDE ROAD WIDTH	HMA SHOULDER WIDTH	HMA SHOULDER AREA	DRIVEWAY AREA	TOTAL AREA	HMA SURF. REM.-BUTT JOINT	TEMPORARY RAMP	AGG. SHOULDER TYPE B	HMA SURFACE REMOVAL VAR. DEPTH	HMA SURFACE REMOVAL 1 1/2"	HMA SHOULDER 6"	BIT. (P.C.)	AGG. (P.C.)	HMA SURFACE COURSE MIX. "C".N50	INCIDENTAL HMA SURFACING	
LOCATION	STATION	( RT/LT)	FOOT	FT	FT	SQ. FT.	SQ. FT.	SQ. FT.	SQ. YD.	SQ. YD.	TON	SQ. YD.	SQ. YD.	SQ. YD.	TON	TON	TON	TON	
TR 441	51+11.15	RT	10.00	20	4	48	228	276	31	11					0.01	0.06		2.6	
TR 110 (265TH AVE)	52+63.15	LT	10.00	20	4	48	228	276	31	11					0.01	0.06		2.6	
TR 417 (140TH AVE)	63+26.27	RT	10.00	30	4	48	288	336	37	17					0.01	0.07		3.1	
TR 391 (165TH AVE)	224+50.00	LT	10.00	20	4	48	228	276	31	11					0.01	0.06		2.6	
TR 391 (165TH AVE)	226+08.00	LT	10.00	30	4	48	288	336	37	17					0.01	0.07		3.1	
TR 184 (BLACK OAK RD)	505+55.77	RT	10.00	20	4	48	228	276	31	11					0.01	0.06		2.6	
TR 343	552+00.00	RT	10.00	12	1	72	297	369	41	7					0.02	0.08		3.4	
TR 341 (JIMTOWN RD)	552+00.00	LT	10.00	12	1	72	297	369	41	7					0.02	0.08		3.4	
FAP 304(ILL 96)	345+57.25	LT	CAD CALCULATION					5853.9	138	22			512			0.25	1.30		54.6
FAP 304 (ILL 96)	345+57.25	RT	CAD CALCULATION					4622.77	134	22			379			0.20	1.03		43.1
TR 646 (327TH ST)	543+62.24	LT	CAD CALCULATION					1046.95	116	12						0.04	0.23		9.8
TR 645	549+16.13	LT	CAD CALCULATION					630.23	70	11						0.03	0.14		5.9
HISTORIC MARKER TURNOUT	150+75.00	RT	SPECIAL SECTION					5184					576			0.22	1.15		48.4
<b>TOTALS =</b>								<b>19,852</b>	<b>739</b>	<b>159</b>			<b>1,468</b>		<b>1</b>	<b>5</b>		<b>186</b>	

• 104RS2,BR3;105RS7,BR3,4;106RS6

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SCHEDULE OF QUANTITIES</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pwork\pwork\sparksgw\0265393\0672E43-sht-schedule.dgn	DRAWN -	REVISED -	321					•	PIKE	69	8	
PLOT SCALE = 525.0000' / in.	CHECKED -	REVISED -	CONTRACT NO. 72E43									
PLOT DATE = Oct-31-2011 03:50:39PM	DATE -	REVISED -	SCALE:		SHEET NO. 3 OF 8 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT				

**ENTRANCE IMPROVEMENT SCHEDULE FOR RURAL / URBAN "PPP" PROJECT**

LOCATION	TYPE OF ENTRANCE	EX MATERIAL TYPE DRIVE	EX MATERIAL TYPE APRON	LENGTH (FROM EDGE OF PVT/ BIT SHLD TO LIMITS OF IMPROVEMENT)	WIDTH	SHOULDER WIDTH	DRIVEWAY AREA	MAILBOX TURNOUT AREA	TOTAL AREA	PROPOSED HMA THICKNESS	HOT-MIX ASPHALT SURF. REM. - BUTT JOINT	PCC SURF. REM. - BUTT JOINT	PREP. OF BASE	AGG. SURFACE COURSE, TYPE B	BIT (PC)	AGG (PC)	INCIDENTAL HOT-MIX ASPHALT SURFACING
(LT / RT) STA	(FE/PE/CE/MB) (RURAL/URBAN)	(EARTH/AGG./O&C/BIT./PCC)	(EARTH/AGG./O&C/BIT./P.C.C)	FOOT	FOOT	FOOT	SQ. FT.	SQ. FT.	SQ. FT.	INCH	SQ. YD.	SQ. YD.	SQ. YD.	TON	TON	TON	TON
RT STA. 43+30	PE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
RT STA. 44+85	PE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
RT STA. 48+30	PE / RURAL	AGG	BIT	8	14	4	128	100	228	1.5	25.3			1	0.01	0.05	3.00
RT STA. 52+20	PE / RURAL	AGG	BIT	8	14	4	128	0	128	1.5	14.2			1	0.01	0.03	2.00
RT STA. 53+20	PE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
LT STA. 60+50	PE / RURAL	AGG	BIT	8	18	4	144	0	144	1.5	16.0			1	0.01	0.03	2.00
LT STA. 62+40	PE / RURAL	AGG	BIT	8	22	4	160	0	160	1.5	17.8			1	0.01	0.04	2.00
LT STA. 63+25	PE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
LT STA 64+45	PE / RURAL	AGG	BIT	8	24	4	168	0	168	1.5	18.7			2	0.01	0.04	2.00
LT STA. 65+20	PE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
LT STA. 65+70	PE / RURAL	AGG	BIT	8	18	4	144	84	228	1.5	25.3			1	0.01	0.05	3.00
RT STA. 66+15	PE / RURAL	AGG	BIT	8	16	4	136	100	236	1.5	26.2			1	0.01	0.05	3.00
LT STA. 67+30	PE / RURAL	AGG	BIT	8	24	4	168	0	168	1.5	18.7			2	0.01	0.04	2.00
RT STA. 70+50	MB / RURAL	BIT	BIT				0	336	336	1.5	37.3			0	0.01	0.07	4.00
LT STA. 70+85	PE / RURAL	AGG	BIT	8	14	4	128	0	128	1.5	14.2			1	0.01	0.03	2.00
LT STA. 73+20	PE / RURAL	AGG	BIT	8	12	4	120	168	288	1.5	32.0			1	0.01	0.06	3.00
LT STA. 74+50	PE / RURAL	AGG	BIT	8	14	4	128	0	128	1.5	14.2			1	0.01	0.03	2.00
LT STA. 74+80	PE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
RT STA. 122+70	PE / RURAL	AGG	BIT	8	14	4	128	0	128	1.5	14.2			1	0.01	0.03	2.00
LT STA. 122+80	FE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
RT STA. 123+75	PE / RURAL	AGG	BIT	8	24	4	168	100	268	1.5	29.8			2	0.01	0.06	3.00
LT STA. 130+85	PE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
RT STA. 130+85	FE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
LT STA. 148+65	FE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
RT STA. 159+90	FE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
LT STA. 159+95	FE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
LT STA. 167+40	FE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
RT STA. 167+90	MB / RURAL	BIT	BIT				0	336	336	1.5	37.3			0	0.01	0.07	4.00
LT STA. 168+30	PE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
RT STA. 170+20	FE / RURAL	AGG	BIT	8	14	4	128	0	128	1.5	14.2			1	0.01	0.03	2.00
RT STA. 171+20	PE / RURAL	AGG	BIT	8	12	4	120	100	220	1.5	24.4			1	0.01	0.05	3.00
LT STA. 172+00	PE / RURAL	BIT	BIT	8	24	4	168	0	168	1.5	18.7			0	0.01	0.04	2.00
LT STA. 175+70	PE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
LT STA. 176+15	PE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00

• 104RS2,BR3;105RS7,BR3,4;106RS6

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SCHEDULE OF QUANTITIES</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\id\sparksgw\10265393\067E43-sht-schedule.dgn	DRAWN -	REVISED -	321					•	PIKE	69	9	
PLOT SCALE = 106.7353' / in.	CHECKED -	REVISED -	CONTRACT NO. 72E43									
PLOT DATE = Oct-31-2011 03:50:39PM	DATE -	REVISED -	SCALE:		SHEET NO. 4 OF 8 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT				

**ENTRANCE IMPROVEMENT SCHEDULE FOR RURAL / URBAN "PPP" PROJECT**

LOCATION	TYPE OF ENTRANCE	EX MATERIAL TYPE DRIVE	EX MATERIAL TYPE APRON	LENGTH (FROM EDGE OF PVT/ BIT SHLD TO LIMITS OF IMPROVEMENT)	WIDTH	SHOULDER WIDTH	DRIVEWAY AREA	MAILBOX TURNOUT AREA	TOTAL AREA	PROPOSED HMA THICKNESS	HOT-MIX ASPHALT SURF. REM. BUTT JOINT	PCC SURF. REM. -BUTT JOINT	PREP. OF BASE	AGG. SURFACE COURSE, TYPE B	BIT (PC)	AGG (PC)	INCIDENTAL HOT-MIX ASPHALT SURFACING
(LT / RT) STA	(FE/PE/CE/MB) (RURAL/URBAN)	(EARTH/AGG./O&C/BIT./PCC)	(EARTH/AGG./O&C/BIT./P.C.C)	FOOT	FOOT	FOOT	SQ. FT.	SQ. FT.	SQ. FT.	INCH	SQ. YD.	SQ. YD.	SQ. YD.	TON	TON	TON	TON
LT STA. 180+10	FE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
RT STA. 180+60	FE / RURAL	AGG	BIT	8	12	4	120	113	233	1.5	25.9			1	0.01	0.05	3.00
LT STA. 181+70	PE / RURAL	AGG	BIT	8	24	4	168	0	168	1.5	18.7			2	0.01	0.04	2.00
RT STA. 190+40	PE / RURAL	AGG	BIT	8	24	4	168	84	252	1.5	28.0			2	0.01	0.06	3.00
LT STA. 192+60	FE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
LT STA. 200+75	FE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
RT STA. 200+75	FE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
LT STA. 207+18	FE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
RT STA. 208+00	PE / RURAL	AGG	BIT	8	14	4	128	100	228	1.5	25.3			1	0.01	0.05	3.00
RT STA. 209+85	PE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
LT STA. 217+00	FE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
RT STA. 217+00	FE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
RT STA. 226+85	PE / RURAL	AGG	BIT	8	16	4	136	100	236	1.5	26.2			1	0.01	0.05	3.00
LT STA. 255+00	PE / RURAL	AGG	BIT	8	24	4	168	0	168	1.5	18.7			2	0.01	0.04	2.00
RT STA. 255+10	PE / RURAL	AGG	BIT	8	24	4	168	0	168	1.5	18.7			2	0.01	0.04	2.00
RT STA. 267+20	FE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
RT STA 272+95	FE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
RT STA. 277+60	PE / RURAL	AGG	BIT	8	20	4	152	100	252	1.5	28.0			1	0.01	0.06	3.00
RT STA. 281+80	PE / RURAL	AGG	BIT	8	20	4	152	0	152	1.5	16.9			1	0.01	0.03	2.00
LT STA. 282+85	FE / RURAL	AGG	BIT	8	24	4	168	0	168	1.5	18.7			1	0.01	0.04	2.00
LT STA. 292+80	FE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
RT STA. 295+55	PE / RURAL	AGG	BIT	8	24	4	168	100	268	1.5	29.8			2	0.01	0.06	3.00
RT STA 297+30	PE / RURAL	AGG	BIT	8	24	4	168	0	168	1.5	18.7			2	0.01	0.04	2.00
RT STA. 298+80	PE / RURAL	AGG	BIT	8	16	4	136	0	136	1.5	15.1			1	0.01	0.03	2.00
RT STA. 341+05	CE / RURAL	AGG	BIT	10	35	0	550	203	753	1.5	83.6			1	0.03	0.17	8.00
LT STA. 341+65	PE / RURAL	AGG	AGG	8	24	0	368	0	368	3.5	0.0			2	0.02	0.08	9.00
LT STA. 343+80	CE / RURAL	AGG	BIT	10	24	0	440	0	440	1.5	48.9			1	0.02	0.10	5.00
LT STA. 344+75	CE / RURAL	PCC	PCC	10	35	0	550	0	550	1.5	0.0	61.1		0	0.02	0.12	6.00
LT STA. 346+40	PE / RURAL	AGG	AGG	8	14	0	288	0	288	3.5	0.0			1	0.01	0.06	7.00
LT STA. 347+55	PE / RURAL	AGG	AGG	8	12	0	272	0	272	3.5	0.0			1	0.01	0.06	6.00
RT STA. 348+10	MB / RURAL	BIT	BIT				0	346	346	1.5	38.4			0	0.01	0.08	4.00
LT STA. 349+20	CE / RURAL	AGG	AGG	10	24	0	440	0	440	8	0.0			1	0.02	0.10	22.00
LT STA. 349+65	CE / RURAL	AGG	AGG	10	24	0	440	0	440	8	0.0			1	0.02	0.10	22.00
RT STA. 351+30	PE / RURAL	AGG	PCC	8	12	5	87	0	87	1.5	0.0	9.7		1	0.00	0.02	1.00

• 104RS2, BR3; 105RS7, BR3, 4; 106RS6

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SCHEDULE OF QUANTITIES</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\idot\sparksgw\10265393\067E43-sht-schedule.dgn	DRAWN -	REVISED -	321					*	PIKE	69	10	
PLOT SCALE = 106.7353' / in.	CHECKED -	REVISED -	CONTRACT NO. 72E43									
PLOT DATE = Oct-31-2011 03:58:40PM	DATE -	REVISED -	SCALE:		SHEET NO. 5 OF 8 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT				



**ENTRANCE IMPROVEMENT SCHEDULE FOR RURAL / URBAN "PPP" PROJECT**

LOCATION	TYPE OF ENTRANCE	EX MATERIAL TYPE DRIVE	EX MATERIAL TYPE APRON	LENGTH (FROM EDGE OF PVT/ BIT SHLD TO LIMITS OF IMPROVEMENT)	WIDTH	SHOULDER WIDTH	DRIVEWAY AREA	MAILBOX TURNOUT AREA	TOTAL AREA	PROPOSED HMA THICKNESS	HOT-MIX ASPHALT SURF. REM. - BUTT JOINT	PCC SURF. REM. - BUTT JOINT	PREP. OF BASE	AGG. SURFACE COURSE, TYPE B	BIT (PC)	AGG (PC)	INCIDENTAL HOT-MIX ASPHALT SURFACING
(LT / RT) STA	(FE/PE/CE/MB) (RURAL/URBAN)	(EARTH/AGG./O&C/BIT./PCC)	(EARTH/AGG./O&C/BIT./P.C.C)	FOOT	FOOT	FT	SQ. FT.	SQ. FT.	SQ. FT.	INCH	SQ. YD.	SQ. YD.	SQ. YD.	TON	TON	TON	TON
RT STA. 352+90	PE / RURAL	AGG	AGG	8	12	4	120	0	120	3.5	0.0			1	0.01	0.03	3.00
RT STA. 354+80	PE / RURAL	AGG	AGG	8	12	4	120	0	120	3.5	0.0			1	0.01	0.03	3.00
RT STA. 357+25	PE / RURAL	AGG	AGG	8	16	4	136	0	136	3.5	0.0			1	0.01	0.03	3.00
RT STA. 358+50	CE / RURAL	AGG	AGG	10	24	4	240	0	240	8	0.0			1	0.01	0.05	12.00
RT STA 359+10	MB / RURAL	BIT	BIT				0	336	336	1.5	37.3			0	0.01	0.07	4.00
LT STA. 359+35	CE / RURAL	AGG	AGG	10	24	4	240	0	240	8	0.0			1	0.01	0.05	12.00
RT STA. 360+40	PE / RURAL	AGG	BIT	8	16	4	136	84	220	1.5	24.4			1	0.01	0.05	3.00
RT STA. 364+00	PE / RURAL	AGG	BIT	8	16	4	136	0	136	1.5	15.1			1	0.01	0.03	2.00
LT STA. 365+70	CE / RURAL	AGG	AGG	10	24	4	240	0	240	8	0.0			1	0.01	0.05	12.00
LT STA. 367+00	CE / RURAL	AGG	BIT	10	24	4	240	0	240	1.5	26.7			1	0.01	0.05	3.00
RT STA. 367+10	PE / RURAL	AGG	AGG	8	14	4	128	0	128	3.5	0.0			1	0.01	0.03	3.00
RT STA. 380+60	PE / RURAL	AGG	AGG	8	18	4	144	84	228	3.5	0.0			1	0.01	0.05	5.00
LT STA. 383+50	PE / RURAL	AGG	BIT	8	20	4	152	100	252	1.5	28.0			1	0.01	0.06	3.00
RT STA. 384+05	PE / RURAL	AGG	BIT	8	12	4	120	84	204	1.5	22.7			1	0.01	0.05	2.00
LT STA. 384+15	PE / RURAL	AGG	BIT	8	14	4	128	84	212	1.5	23.6			1	0.01	0.05	2.00
RT STA. 385+65	MB / RURAL	AGG	AGG				0	336	336	3.5	0.0			0	0.01	0.07	8.00
LT STA. 385+65	PE / RURAL	AGG	AGG	8	12	4	120	84	204	3.5	0.0			1	0.01	0.05	5.00
LT STA. 386+30	PE / RURAL	AGG	AGG	8	12	4	120	0	120	3.5	0.0			1	0.01	0.03	3.00
RT STA. 386+80	MB / RURAL	AGG	AGG				0	336	336	3.5	0.0			0	0.01	0.07	8.00
LT STA. 387+55	PE / RURAL	AGG	AGG	8	12	4	120	0	120	3.5	0.0			1	0.01	0.03	3.00
LT STA. 388+00	PE / RURAL	AGG	BIT	8	12	4	120	168	288	1.5	32.0			1	0.01	0.06	3.00
LT STA. 391+25	PE / RURAL	AGG	BIT	8	12	1.5	211	0	211	1.5	23.5			1	0.01	0.05	2.00
RT STA. 399+70	PE / RURAL	AGG	BIT	8	24	1.5	289	129	418	1.5	46.4			2	0.02	0.09	4.00
RT STA. 400+70	PE / RURAL	AGG	BIT	8	20	1.5	263	155	418	1.5	46.4			1	0.02	0.09	4.00
LT STA. 409+20	PE / RURAL	AGG	AGG	8	24	1	315	0	315	3.5	0.0			2	0.01	0.07	7.00
RT STA. 412+30	CE / RURAL	AGG	AGG	10	24	1	387	0	387	8	0.0			1	0.02	0.09	20.00
LT STA. 438+00	PE / RURAL	AGG	AGG	8	12	1.75	202	0	202	3.5	0.0			1	0.01	0.04	5.00
RT STA. 441+00	MB / RURAL	BIT	BIT				0	336	336	1.5	37.3			0	0.01	0.07	4.00
LT STA. 442+15	PE / RURAL	AGG	BIT	8	12	1.5	211	0	211	1.5	23.5			1	0.01	0.05	2.00
LT STA. 443+30	PE / RURAL	AGG	AGG	8	12	4	120	0	120	3.5	0.0			1	0.01	0.03	3.00
RT STA. 447+80	PE / RURAL	AGG	AGG	8	12	4	120	0	120	3.5	0.0			1	0.01	0.03	3.00
RT STA. 451+55	PE / RURAL	AGG	BIT	8	22	4	160	0	160	1.5	17.8			1	0.01	0.04	2.00
LT STA. 452+70	PE / RURAL	AGG	BIT	8	16	4	136	168	304	1.5	33.8			1	0.01	0.07	3.00
RT STA. 454+55	PE / RURAL	AGG	BIT	8	18	4	144	0	144	1.5	16.0			1	0.01	0.03	2.00

• 104RS2, BR3; 105RS7, BR3, 4; 106RS6

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SCHEDULE OF QUANTITIES</b>			F.A.P. RFE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pwork\pwork\sparksgw\10265393\0672E43-sht-schedule.dgn	DRAWN -	REVISED -	321					•	PIKE	69	11	
PLOT SCALE = 106.7353' / in.	CHECKED -	REVISED -	CONTRACT NO. 72E43									
PLOT DATE = Oct-31-2011 03:58:40PM	DATE -	REVISED -	SCALE:		SHEET NO. 6 OF 8 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT				

**ENTRANCE IMPROVEMENT SCHEDULE FOR RURAL / URBAN "PPP" PROJECT**

LOCATION	TYPE OF ENTRANCE	EX MATERIAL TYPE DRIVE	EX MATERIAL TYPE APRON	LENGTH (FROM EDGE OF PVT/ BIT SHLD TO LIMITS OF IMPROVEMENT)	WIDTH	SHOULDER WIDTH	DRIVEWAY AREA	MAILBOX TURNOUT AREA	TOTAL AREA	PROPOSED HMA THICKNESS	HOT-MIX ASPHALT SURF. REM. - BUTT JOINT	PCC SURF. REM. - BUTT JOINT	PREP. OF BASE	AGG. SURFACE COURSE, TYPE B	BIT (PC)	AGG (PC)	INCIDENTAL HOT-MIX ASPHALT SURFACING
(LT / RT) STA	(FE/PE/CE/MB) (RURAL/URBAN)	(EARTH/AGG./O&C/BIT./PCC)	(EARTH/AGG./O&C/BIT./P.C.C)	FOOT	FOOT	FOOT	SQ. FT.	SQ. FT.	SQ. FT.	INCH	SQ. YD.	SQ. YD.	SQ. YD.	TON	TON	TON	TON
RT STA. 456+55	PE / RURAL	AGG	BIT	8	18	4	144	100	244	1.5	27.1			1	0.01	0.05	3.00
RT STA. 465+48	PE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
LT STA. 470+45	PE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
RT STA. 470+45	FE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
LT STA. 482+25	MB / RURAL	BIT	BIT				0	336	336	1.5	37.3			0	0.01	0.07	4.00
LT STA. 496+95	FE / RURAL	AGG	BIT	8	12	4	120	113	233	1.5	25.9			1	0.01	0.05	3.00
RT STA. 497+00	PE / RURAL	AGG	BIT	8	20	4	152	0	152	1.5	16.9			1	0.01	0.03	2.00
LT STA. 501+70	FE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
LT STA. 504+25	PE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
LT STA. 505+50	PE / RURAL	AGG	AGG	8	12	4	120	100	220	3.5	0.0			1	0.01	0.05	5.00
LT STA. 506+65	PE / RURAL	PCC	BIT	8	24	4	168	0	168	1.5	18.7			0	0.01	0.04	2.00
LT STA. 508+85	FE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
RT STA 508+85	PE / RURAL	AGG	BIT	8	22	4	160	0	160	1.5	17.8			1	0.01	0.04	2.00
RT STA. 511+05	PE / RURAL	AGG	BIT	8	12	4	120	0	120	1.5	13.3			1	0.01	0.03	2.00
RT STA. 511+80	PE / RURAL	BIT	BIT	8	24	4	168	0	168	1.5	18.7			0	0.01	0.04	2.00
RT STA. 512+45	PE / RURAL	BIT	BIT	8	24	4	168	0	168	1.5	18.7			0	0.01	0.04	2.00
RT STA. 513+10	PE / RURAL	BIT	BIT	8	24	4	168	0	168	1.5	18.7			0	0.01	0.04	2.00
RT STA. 514+30	PE / RURAL	BIT	BIT	8	22	4	160	84	244	1.5	27.1			0	0.01	0.05	3.00
LT STA. 519+90	PE / RURAL	AGG	BIT	8	24	4	168	168	336	1.5	37.3			2	0.01	0.07	4.00
RT STA. 525+29	FE / RURAL	AGG	AGG	3	12	1	0	49	49					1	0.00	0.01	0.00
RT STA. 529+32	PE / RURAL	AGG	BIT	8	16	1	259	137	396	1.5	43.9			1	0.02	0.09	4.00
LT STA. 532+73	FE / RURAL	AGG	AGG	3	12	1								1			
RT STA. 532+73	FE / RURAL	AGG	AGG	3	12	1								1			
LT STA. 533+89	FE / RURAL	AGG	AGG	3	12	1								1			
RT STA. 533+89	FE / RURAL	AGG	BIT	8	12	1	231	0	231	1.5	25.7			1	0.01	0.05	3.00
RT STA. 536+24	PE / RURAL	AGG	BIT	8	16	1	259	137	396	1.5	43.9			1	0.02	0.09	4.00
RT STA. 537+46	PE / RURAL	AGG	BIT	8	16	1	259	137	396	1.5	43.9			1	0.02	0.09	4.00
RT STA. 538+61	PE / RURAL	AGG	BIT	8	16	1	259	137	396	1.5	43.9			1	0.02	0.09	4.00
RT STA. 539+29	PE / RURAL	AGG	BIT	8	16	1	259	137	396	1.5	43.9			1	0.02	0.09	4.00
LT STA. 539+54	PE / RURAL	AGG	BIT	8	12	1	231	294	525	1.5	58.3			1	0.02	0.12	5.00
RT STA. 540+75	PE / RURAL	BIT	BIT	8	16	1	259	165	424	1.5	47.1			0	0.02	0.09	4.00
RT STA. 547+12	PE / RURAL	AGG	BIT	8	12	1	231	0	231	1.5	25.7			1	0.01	0.05	3.00
RT STA. 549+12	CE / RURAL	AGG	BIT	10	35	1	486	187	673	1.5	74.8			1	0.03	0.15	7.00
LT STA. 550+00	PE / RURAL	AGG	BIT	8	12	1	231	294	525	1.5	58.3			1	0.02	0.12	5.00
<b>TOTALS</b>							<b>21,758</b>	<b>7,524</b>	<b>29,282</b>	<b>273</b>	<b>2,540</b>	<b>71</b>	<b>0</b>	<b>133</b>	<b>2</b>	<b>7</b>	<b>480</b>

• 104RS2,BR3;105RS7,BR3,4;106RS6

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SCHEDULE OF QUANTITIES</b>			F.A.P. RFE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\idot\sparksgw\10265393\0672E43-sht-schedule.dgn	DRAWN -	REVISED -	321					•	PIKE	69	12	
PLOT SCALE = 106.7353' / in.	CHECKED -	REVISED -	CONTRACT NO. 72E43									
PLOT DATE = Oct-31-2011 03:58:41PM	DATE -	REVISED -	SCALE:		SHEET NO. 7 OF 8 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT				

**PAVEMENT MARKING SCHEDULE**

LOCATION		LENGTH FOOT	PAINT PAVEMENT MARKING-LINE, 5"				TEMPORARY PAVEMENT MARKING-LINE, 5"				PREFORMED PLASTIC PAVEMENT MARKING, TY B INLAID -LINE 24"	TEMPORARY PAVEMENT MARKING - LINE 24"	SHORT-TERM PAVEMENT MARKING (2 APPLICATIONS)	WORK ZONE PAVEMENT MARKING REMOVAL	RAISED REFLECTIVE PAVEMENT MARKER EACH	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL EACH
			SOLID WHITE LINE FOOT	SOLID YELLOW LINE, LEFT FOOT	SOLID YELLOW LINE, RIGHT FOOT	DASHED YELLOW CENTER LINE FOOT	SOLID WHITE FOOT	SOLID YELLOW LINE, LEFT FOOT	SOLID YELLOW LINE, RIGHT FOOT	DASHED YELLOW CENTER LINE FOOT						
STATION	STATION	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	SQ FT	EACH	EACH
41+00.00	47+50.00	650	1300	650	650			1300	650	650				120	25	9
47+50.00	72+74.00	2524	5048	2524		630		5048	2524		630			456	95	32
72+74.00	303+40.90	23067	46134			5770		46134		5770				4192	873	289
303+40.90BK	325+00.00AHD															
325+00.00	346+12.00	2112	4224			530		4224		530				384	80	27
346+12.00	347+05.00	93	186	93		20		186	93	20				16	3	2
347+05.00	365+23.40	1818	3637			450		3637		450				328	68	23
365+23.40BK	365+30.93AHD															
365+30.93	380+70.00	1539	3078			380		3078		380				280	58	20
380+70.00	393+00.00	1230	2460		1230	310		2460		1230	310			224	47	16
393+00.00	416+65.00	2365	4730	2365	2365			4730	2365	2365				432	90	30
416+65.00	423+82.00	717	1434	717		180		1434	717	180				128	27	9
423+82.00	425+84.00	202	404	202	202			404	202	202				40	8	3
425+84.00	434+62.00	878	1756		878	220		1756		878	220			160	33	11
434+62.00	440+20.00	558	1116	558	558			1116	558	558				104	22	7
440+20.00	448+82.00	862	1724	862		220		1724	862	220				160	33	11
448+82.00	484+15.00	3533	7066			880		7066		880				640	133	45
484+15.00	495+18.00	1103	2206		1103	280		2206		1103	280			200	42	14
495+18.00	506+68.00	1150	2300	1150	1150			2300	1150	1150				208	43	15
506+68.00	508+94.98	227	454	227		60		454	227	60				40	8	3
508+94.98BK	509+80.78AHD															
509+80.78	512+90.00	309	618	309		80		618	309	80				56	12	4
512+90.00	552+34.00	3944	7888			990		7888		990				720	150	50
STOPBARS @ ATLAS INTERSECTION												90	90			
<b>TOTALS=</b>			97763	9657	8136	11000	97763	9657	8136	11000						
			<b>126,556</b>				<b>126,556</b>				<b>90</b>	<b>90</b>	<b>8,888</b>	<b>1,852</b>	<b>620</b>	<b>620</b>

**TOTALS=**

**126,556**

**126,556**

**90**

**90**

**8,888**

**1,852**

**620**

**620**

**DRAINAGE SCHEDULE**

LOCATION	PIPE CULVERT REMOVAL FOOT	STORM SEWER, CLASS A, TYPE 1, 12" FOOT	STORM SEWER, CLASS A, TYPE 1, 15" FOOT	CLASS SI CONCRETE (OUTLET) CU. YD.	INLETS, TYPE A, TYPE 15 FRAME AND LID EACH	CONTROLLED LOW-STRENGTH MATERIAL CU. YD.	CONCRETE COLLAR CU. YD.	PIPE CULVERT REMOVAL (SPECIAL) FOOT	PAVEMENT PATCHING, TYPE II, 12 INCH SQ YD
IL 96/ US 54	63	57	63		2	7.6			23
US 54, LT 347+06				3					
LT +/- 481+00							2	12	
<b>TOTALS=</b>	<b>63</b>	<b>57</b>	<b>63</b>	<b>3</b>	<b>2</b>	<b>7.6</b>	<b>2</b>	<b>12</b>	<b>23</b>

**GUARDRAIL SCHEDULE**

LOCATION		TRAFFIC BARRIER TERMINAL REMOVAL, TYPE 1 EACH	TRAFFIC BARRIER TERMINAL, TYPE 1 SPECIAL (TANGENT) EACH	TERMINAL MARKERS DIRECT APPLIED EACH
STATION	LT/ RT			
45+25.00	RT	1	1	1
<b>TOTALS =</b>		<b>1</b>	<b>1</b>	<b>1</b>

**SUPERELEVATION HMA  
SCHEDULE**

CURVE NO	HMA SURFACE COURSE, MIX "C". N50 TON
PSBA2	9.94
PSBA3	10.99
300	13.69
301	259.37
302	21.55
303	88.68
403	8.42
PSBA8	19.15
PSBA9	6.21
<b>TOTALS=</b>	<b>438</b>

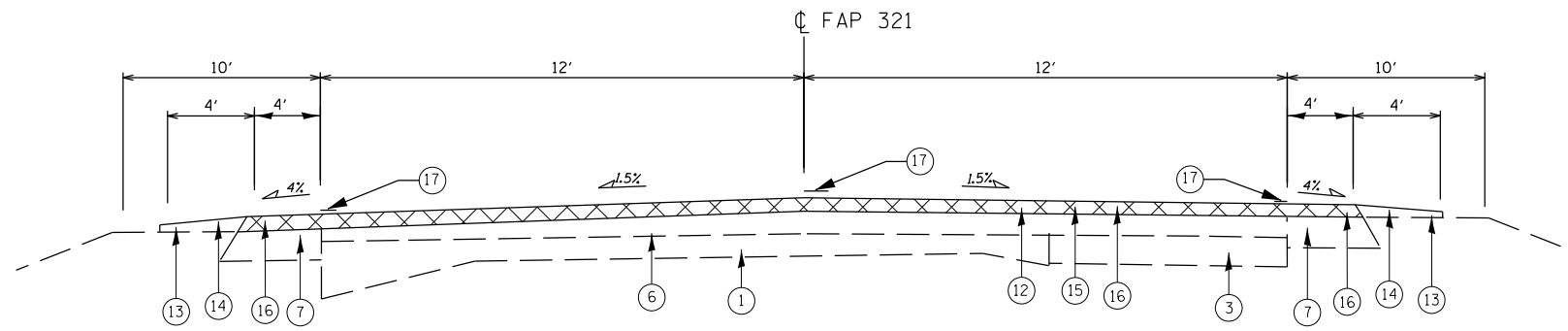
• 104RS2, BR3; 105RS7, BR3, 4; 106RS6

**RIPRAP /CHANNEL EXCAVATION SCHEDULE**

LOCATION	STONE DUMPED RIPRAP, CLASS A5 SQ YD	STONE DUMPED RIPRAP, CLASS A4 SQ YD	STONE DUMPED RIPRAP, CLASS A4 TON	FILTER FABRIC SQ YD	CHANNEL EXCAVATION CU YD
STA. 429+50 LT	166.67			166.67	
STA. 429+50 RT	166.67			166.67	
STA. 346+82.00		16.82			
STA. 410+00.00 (SN =075-7162 )			14.00	9.00	27
<b>TOTALS=</b>	<b>334.00</b>	<b>17.00</b>	<b>14.00</b>	<b>343.00</b>	<b>27.00</b>

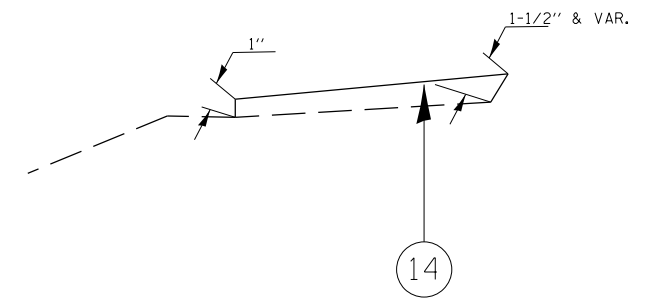
**CURB AND GUTTER SCHEDULE**

LOCATION	COMBINATION CURB AND GUTTER, REMOVAL FOOT	CURB REMOVAL FOOT	COMBINATION CURB AND GUTTER, TYPE B6, 24 FOOT	CONCRETE CURB, TYPE B FOOT
LT 459+00 to 464+00		500		500
RT 345+22 (US-54 And ILL-9)	9		9	
<b>TOTALS =</b>	<b>9</b>	<b>500</b>	<b>9</b>	<b>500</b>

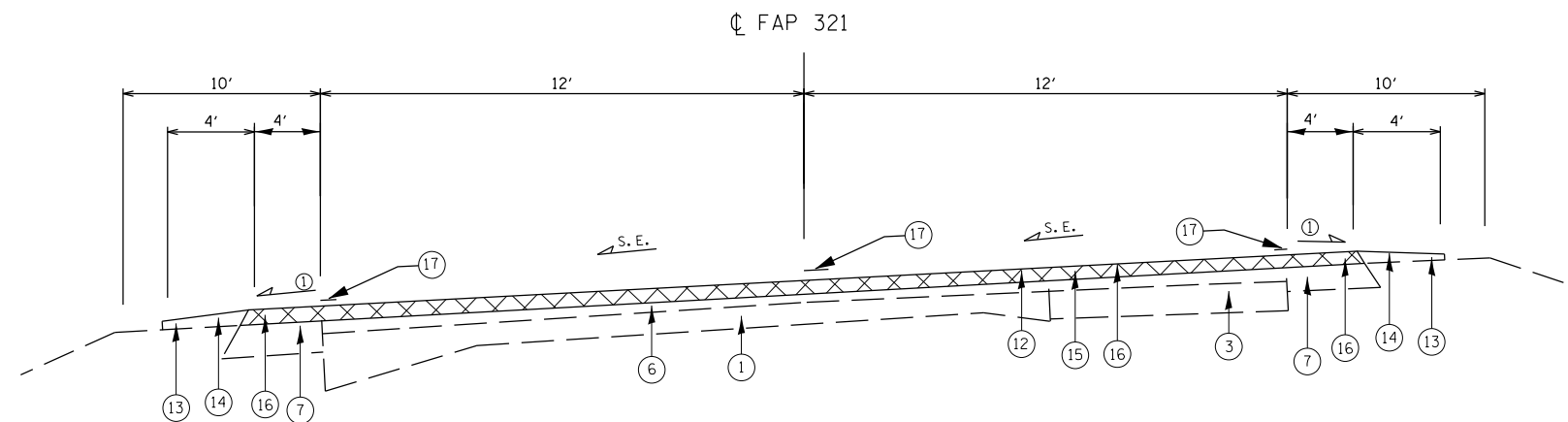


STA. 39+23.81 TO 49+35.00  
 STA. 49+67.00 TO 104+29.25 \*  
 STA. 104+72.75 TO 172+84.25  
 STA. 173+39.75 TO 244+50.00  
 STA. 275+39.22 TO 340+00.00

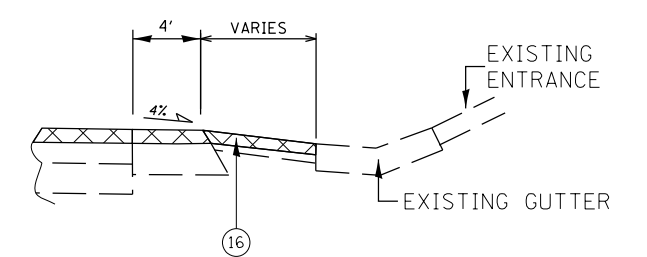
STA. EQ. 303+40.90 BK. =  
 STA. EQ. 325+00.00 AH.



AGGREGATE SHOULDER DETAIL



COMPOUND CURVE  
 STA. 43+07.77 TO 44+82.97 ( S.E. TRANS. NC TO 5.8%)  
 STA. 44+82.97 TO 54+71.70 ( FULL S.E. 5.8%)  
 STA. 54+71.70 TO 54+86.10 ( S.E. TRANS. 5.8% TO 5.2% )  
 STA. 54+86.10 TO 65+55.99 ( FULL S.E. 5.2% )  
 STA. 65+55.99 TO 67+16.79 ( S.E. TRANS. 5.2% TO NC )



\* TYPE A GUTTER LT STA 57+50.00 TO 65+14.00

BRIDGE PAVING OMISSION  
 STA. 49+35.00 TO 49+67.00  
 STA. 104+29.25 TO 104+72.75  
 STA. 172+84.25 TO 173+39.75

EXISTING LEGEND

1. EXISTING PCC PAVEMENT (9''-6''-9'')
2. EXISTING PCC COURSE, 12''
3. EXISTING 8'' P.C.C. BASE COURSE
4. EXISTING 3/4 '' SMOOTH BAR
5. EXISTING 1/2 '' BAR
6. EXISTING HMA SURFACE COURSE OVERLAY,VAR
7. EXISTING 8'' STABLIZED SHOULDER
8. EXISTING CONCRETE CURB AND GUTTER
9. EXISTING CONCRETE GUTTER
10. EXISTING SUB-BASE GRANULAR
11. EXISTING LEVELING BINDER, 3/4 - 1 1/2
12. EXISTING HMA SURFACE COURSE, 1 1/2
13. EXISTING AGGREGATE SHOULDER, TYPE B

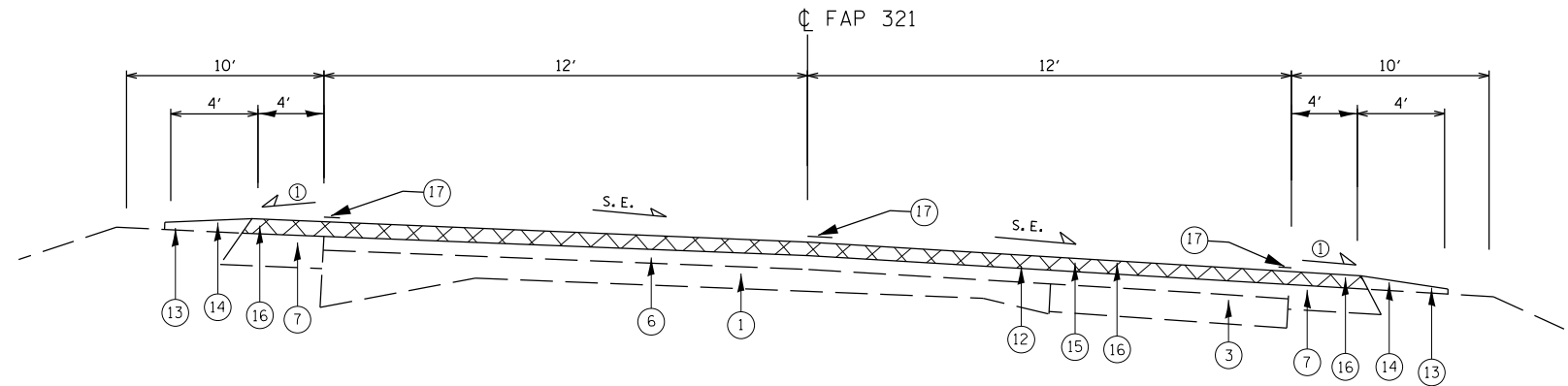
PROPOSED LEGEND

14. PROPOSED AGGREGATE SHOULDER, TYPE B
15. PROPOSED HMA SURFACE COURSE REMOVAL, 1-1/2''
16. PROPOSED HMA SURFACE COURSE MIXTURE C, N50 1-1/2''
17. PROPOSED PAVEMENT MARKING-LINE 5''

- NOTES: 1. IF SUPERELEVATION EXIST THE SHOULDER SLOPES ON THE HIGH SIDE MAXIMUM BREAK-OVER SHOULD BE NO GREATER THAN 8% AND ON THE LOW SIDE SAME AS SUPERELEVATION IF OVER 4%.
2. FOR LIMITS OF SUPERELEVATION SEE SUPERELEVATION TRANSITION DETAIL FOR TWO LANE HIGHWAY

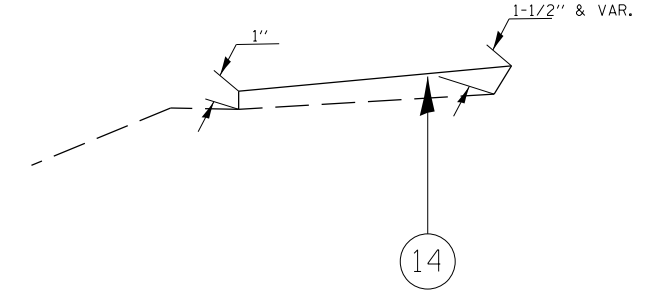
• 104RS2,BR3;105RS7,BR3,4;106RS6

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>US 54 TYPICAL SECTIONS</b>			F.A.P. RTE. 321	SECTION *	COUNTY PIKE	TOTAL SHEETS 69	SHEET NO. 14
ci:\pw\work\p\dot\sparksgw\10265393\0672E43-sht-typical.dgn		DRAWN -	REVISED -		SCALE:	SHEET NO. 1 OF 10 SHEETS	STA. _____ TO STA. _____	CONTRACT NO. 72E43				
		CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT							
		DATE -	REVISED -									

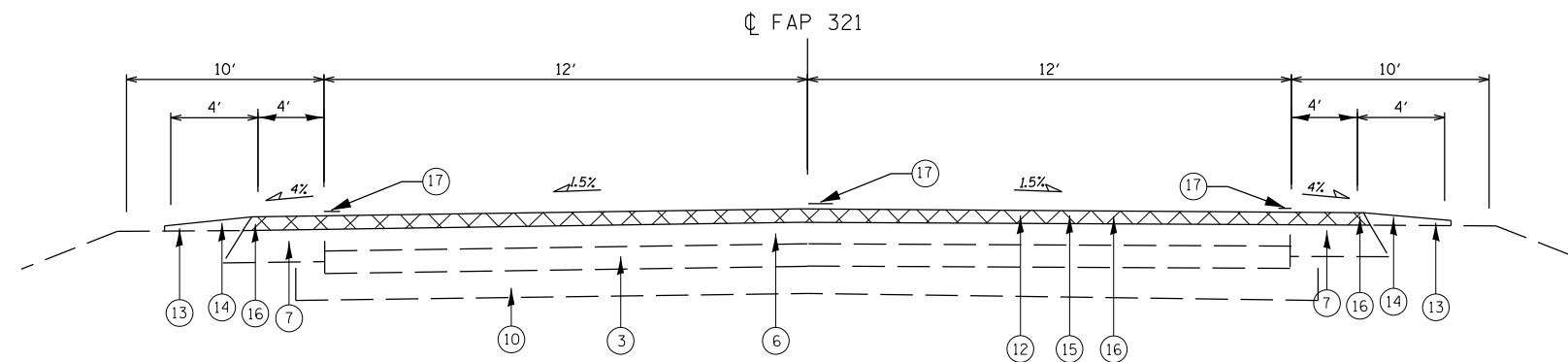


STA. 151+58.99 TO 152+78.87 ( S.E. TRANS. NC TO 3.0%)  
 STA. 152+78.87 TO 163+50.47 ( FULL S.E. 3.0%)  
 STA. 163+50.47 TO 164+70.35 ( S.E. TRANS. 3.0% TO NC )

STA. 214+12.26 TO 216+06.73 ( S.E. TRANS. NC TO 5.8%)  
 STA. 216+06.73 TO 225+56.17 ( FULL S.E. 5.8%)  
 STA. 225+56.17 TO 227+50.64 ( S.E. TRANS. 5.8% TO NC )



AGGREGATE SHOULDER DETAIL



STA. 244+50.00 TO 257+81.67  
 STA. 259+18.33 TO 273+65.39  
 STA. 275+16.22 TO 275+39.22

BRIDGE PAVING OMISSION  
 STA. 257+81.67 TO 259+18.33  
 STA. 273+65.39 TO 275+16.22

EXISTING LEGEND

1. EXISTING PCC PAVEMENT (9''-6''-9'')
2. EXISTING PCC COURSE, 12''
3. EXISTING 8'' P.C.C. BASE COURSE
4. EXISTING 3/4 '' SMOOTH BAR
5. EXISTING 1/2 '' BAR
6. EXISTING HMA SURFACE COURSE OVERLAY,VAR
7. EXISTING 8'' STABLIZED SHOULDER
8. EXISTING CONCRETE CURB AND GUTTER
9. EXISTING CONCRETE GUTTER
10. EXISTING SUB-BASE GRANULAR
11. EXISTING LEVELING BINDER, 3/4 - 1 1/2
12. EXISTING HMA SURFACE COURSE, 1 1/2
13. EXISTING AGGREGATE SHOULDER, TYPE B

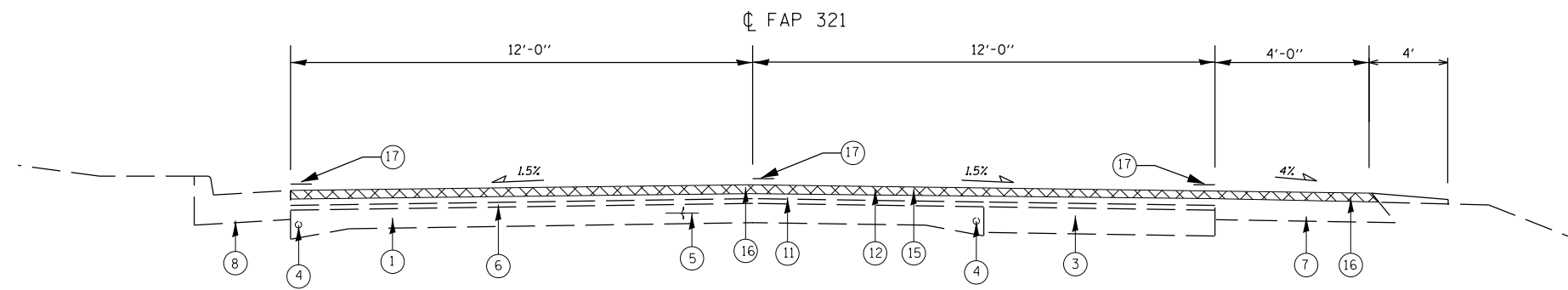
PROPOSED LEGEND

14. PROPOSED AGGREGATE SHOULDER, TYPE B
15. PROPOSED HMA SURFACE COURSE REMOVAL, 1-1/2''
16. PROPOSED HMA SURFACE COURSE MIXTURE C, N50 1-1/2''
17. PROPOSED PAVEMENT MARKING-LINE 5''

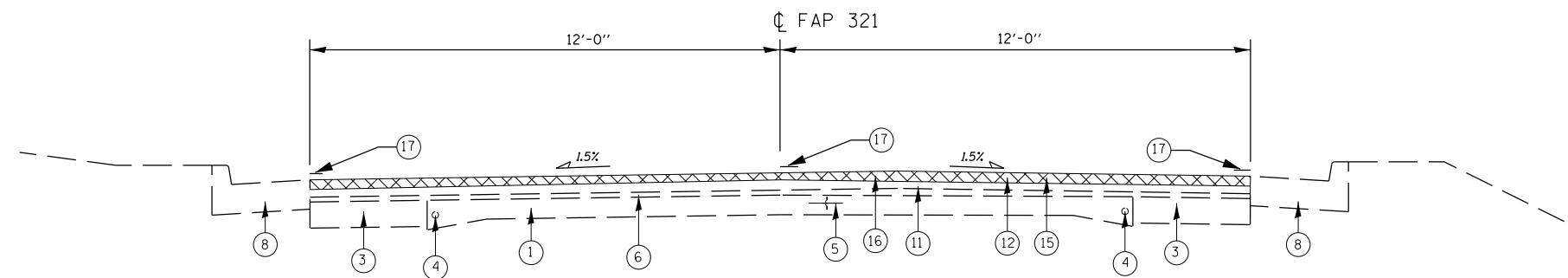
- NOTES: 1. IF SUPERELEVATION EXIST THE SHOULDER SLOPES ON THE HIGH SIDE MAXIMUM BREAK-OVER SHOULD BE NO GREATER THAN 8% AND ON THE LOW SIDE SAME AS SUPERELEVATION IF OVER 4%
2. FOR LIMITS OF SUPERELEVATION SEE SUPERELEVATION TRANSITION DETAIL FOR TWO LANE HIGHWAY

• 104RS2,BR3;105RS7,BR3,4;106RS6

FILE NAME = c:\pwork\pwork\sparksgw\10265393\0672E43-sht-typical.dgn	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>US 54 TYPICAL SECTIONS</b>			F.A.P. RTE. 321	SECTION *	COUNTY PIKE	TOTAL SHEETS 69	SHEET NO. 15
	PLOT SCALE = 4.2352 '' / in.	CHECKED -	REVISED -		SCALE:	SHEET NO. 2 OF 10 SHEETS	STA.	TO STA.	CONTRACT NO. 72E43			
PLOT DATE = Oct-31-2011 03:50:53PM	DATE -	REVISED -	REVISED -	ILLINOIS FED. AID PROJECT								



STA. 340+00.00 TO 341+71.00



STA. 341+71.00 TO 344+51.00  
 RT. STA. 346+60.67 TO 350+60.00  
 LT. STA. 346+60.67 TO 351+54.84

EXISTING LEGEND

1. EXISTING PCC PAVEMENT (9"-6"-9")
2. EXISTING PCC COURSE, 12"
3. EXISTING 8" P.C.C. BASE COURSE
4. EXISTING 3/4 " SMOOTH BAR
5. EXISTING 1/2 " BAR
6. EXISTING HMA SURFACE COURSE OVERLAY,VAR
7. EXISTING 8" STABLIZED SHOULDER
8. EXISTING CONCRETE CURB AND GUTTER
9. EXISTING CONCRETE GUTTER
10. EXISTING SUB-BASE GRANULAR
11. EXISTING LEVELING BINDER, 3/4 - 1 1/2
12. EXISTING HMA SURFACE COURSE, 1 1/2
13. EXISTNG AGGREGATE SHOULDER, TYPE B

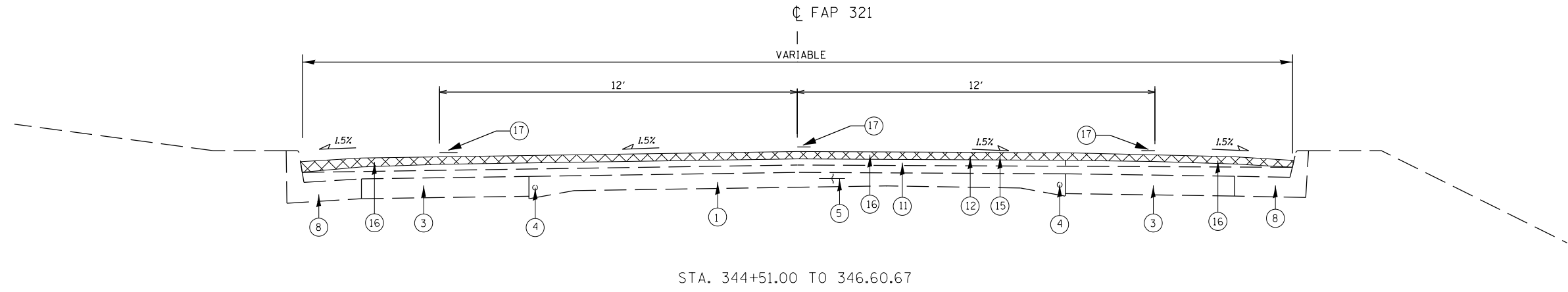
PROPOSED LEGEND

14. PROPOSED AGGREGATE SHOULDER, TYPE B
15. PROPOSED HMA SURFACE COURSE REMOVAL, 1-1/2"
16. PROPOSED HMA SURFACE COURSE MIXTURE C, N50 1-1/2"
17. PROPOSED PAVEMENT MARKING-LINE 5"

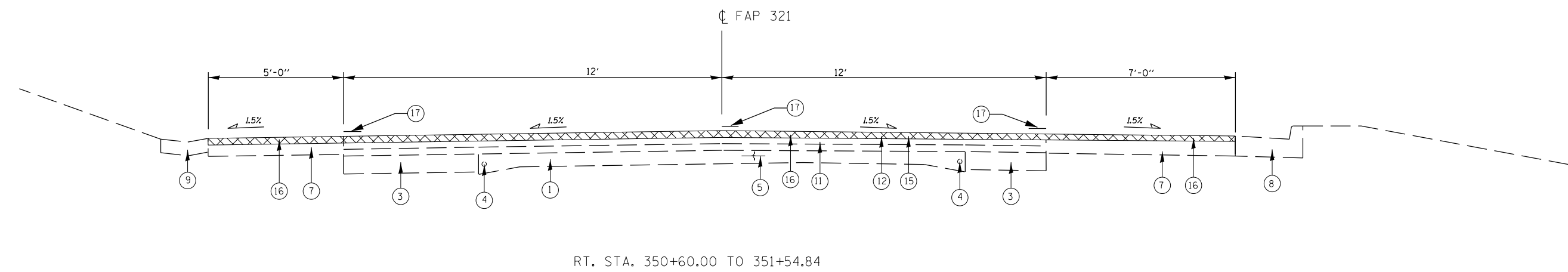
• 104RS2,BR3;105RS7,BR3,4;106RS6

FILE NAME = c:\pwwork\pwwork\sparksgw\10265393\0672E43-sht-typical.dgn	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>US 54 TYPICAL SECTIONS</b>			F.A.P. RTE. 321	SECTION *	COUNTY PIKE	TOTAL SHEETS 69	SHEET NO. 16
	PLOT SCALE = 4.2352 ' / in.	CHECKED -	REVISED -		SCALE:	SHEET NO. 3 OF 10 SHEETS	STA.	TO STA.	CONTRACT NO. 72E43			
PLOT DATE = Oct-31-2011 03:50:53PM	DATE -	REVISED -	REVISED -	ILLINOIS FED. AID PROJECT								





STA. 344+51.00 TO 346.60.67



RT. STA. 350+60.00 TO 351+54.84

EXISTING LEGEND

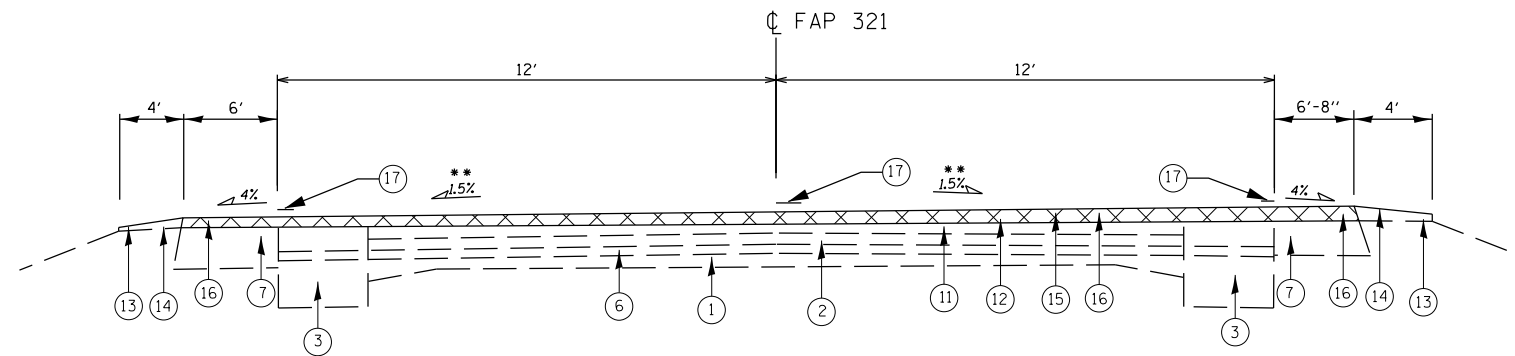
1. EXISTING PCC PAVEMENT (9''-6''-9'')
2. EXISTING PCC COURSE, 12''
3. EXISTING 8'' P.C.C. BASE COURSE
4. EXISTING 3/4 '' SMOOTH BAR
5. EXISTING 1/2 '' BAR
6. EXISTING HMA SURFACE COURSE OVERLAY,VAR
7. EXISTING 8'' STABILIZED SHOULDER
8. EXISTING CONCRETE CURB AND GUTTER
9. EXISTING CONCRETE GUTTER
10. EXISTING SUB-BASE GRANULAR
11. EXISTING LEVELING BINDER, 3/4 - 1 1/2
12. EXISTING HMA SURFACE COURSE, 1 1/2
13. EXISTING AGGREGATE SHOULDER, TYPE B

PROPOSED LEGEND

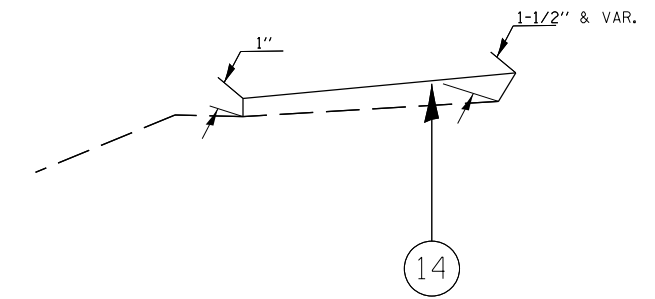
14. PROPOSED AGGREGATE SHOULDER, TYPE B
15. PROPOSED HMA SURFACE COURSE REMOVAL, 1-1/2''
16. PROPOSED HMA SURFACE COURSE MIXTURE C, N50 1-1/2''
17. PROPOSED PAVEMENT MARKING-LINE 5''

• 104RS2,BR3;105RS7,BR3,4;106RS6

FILE NAME = c:\pwork\pwork\sparksgw\0265393\067E43-sht-typical.dgn	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>US 54 TYPICAL SECTIONS</b>			F.A.P. RTE. 321	SECTION *	COUNTY PIKE	TOTAL SHEETS 69	SHEET NO. 17
	PLOT SCALE = 4.2352 ' / in.	CHECKED -	REVISED -		SCALE:	SHEET NO. 4 OF 10 SHEETS	STA. TO STA.	CONTRACT NO. 72E43				
PLOT DATE = Oct-31-2011 03:50:54PM	DATE -	REVISED -	REVISED -		ILLINOIS FED. AID PROJECT							

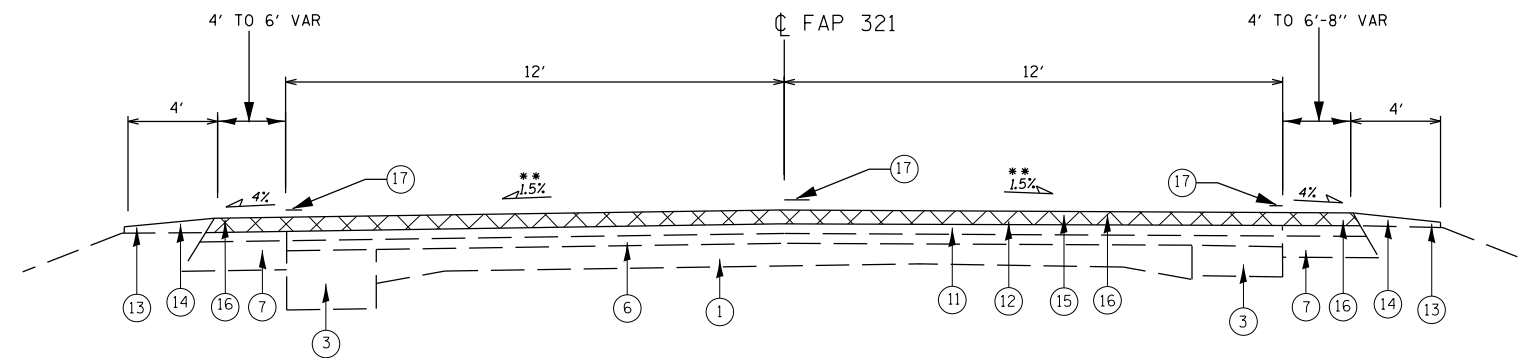


STA. 359+81.61 TO 360+57.07



AGGREGATE SHOULDER DETAIL

\*\* NOTE 1.5% SLOPE EXCEPT USE PROP. S.E. RATE SHOWN ON THE PLAN SHEET, SUPERELEVATION TRANSITION DETAILS, AND REVERSE CURVE TRANSITION DETAIL



STA. 351+54.84 TO 359+81.61  
STA. 360+57.07 TO 380+28.86

EXISTING LEGEND

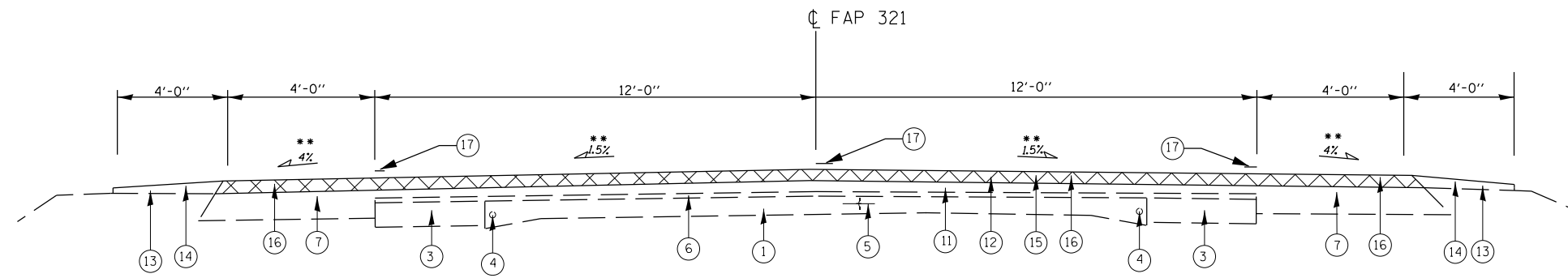
1. EXISTING PCC PAVEMENT (9'-6"-9')
2. EXISTING PCC COURSE, 12"
3. EXISTING 8" P.C.C. BASE COURSE
4. EXISTING 3/4 " SMOOTH BAR
5. EXISTING 1/2 " BAR
6. EXISTING HMA SURFACE COURSE OVERLAY,VAR
7. EXISTING 8" STABILIZED SHOULDER
8. EXISTING CONCRETE CURB AND GUTTER
9. EXISTING CONCRETE GUTTER
10. EXISTING SUB-BASE GRANULAR
11. EXISTING LEVELING BINDER, 3/4 - 1 1/2
12. EXISTING HMA SURFACE COURSE, 1 1/2
13. EXISTING AGGREGATE SHOULDER, TYPE B

PROPOSED LEGEND

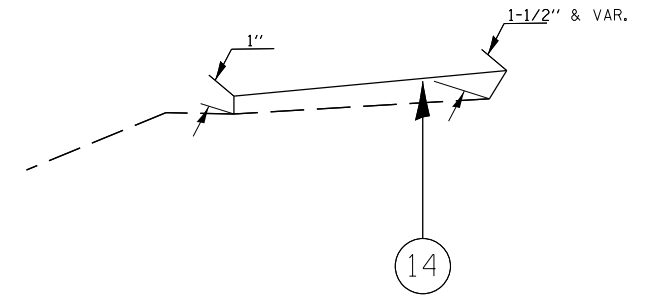
14. PROPOSED AGGREGATE SHOULDER, TYPE B
15. PROPOSED HMA SURFACE COURSE REMOVAL, 1-1/2"
16. PROPOSED HMA SURFACE COURSE MIXTURE C, N50 1-1/2"
17. PROPOSED PAVEMENT MARKING-LINE 5"

• 104RS2,BR3;105RS7,BR3,4;106RS6

FILE NAME = c:\pwwork\pwwork\sparksgw\10265393\0672E43-sht-typicol.dgn	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>US 54 TYPICAL SECTIONS</b>			F.A.P. RTE. 321	SECTION •	COUNTY PIKE	TOTAL SHEETS 69	SHEET NO. 18
	PLOT SCALE = 4.2352 ' / in.	CHECKED -	REVISED -		SCALE:	SHEET NO. 5 OF 10 SHEETS	STA. _____ TO STA. _____	CONTRACT NO. 72E43				
PLOT DATE = Oct-31-2011 03:50:54PM	DATE -	REVISED -	REVISED -		ILLINOIS FED. AID PROJECT							

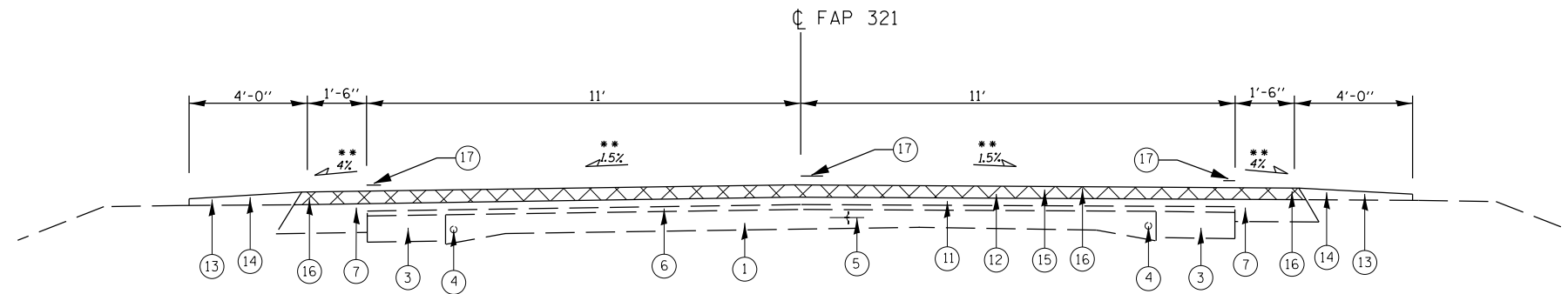


STA. 380+28.86 TO 388+75.00  
 STA. 443+00.00 TO 454+00.00  
 STA. 511+32.00 TO 523+35.00



AGGREGATE SHOULDER DETAIL

\*\* NOTE 1.5% SLOPE EXCEPT USE PROP. S.E. RATE SHOWN ON THE PLAN SHEET, SUPERELEVATION TRANSITION DETAILS, AND REVERSE CURVE TRANSITION DETAIL



STA. 388+75.00 TO 391+00.00  
 RT. STA. 408+70.00 TO 410+75.00  
 STA. 410+75.00 TO 411+00.00  
 LT. STA. 411+00.00 TO 415+47.00  
 STA. 424+45.00 TO 430+00.00  
 STA. 440+14.40 TO 443+00.00

EXISTING LEGEND

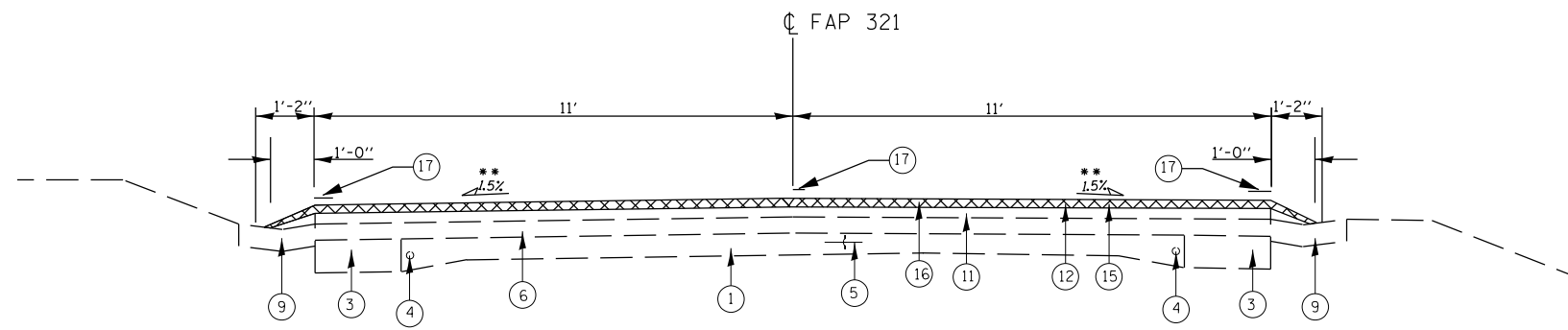
1. EXISTING PCC PAVEMENT (9'-6"-9')
2. EXISTING PCC COURSE, 12"
3. EXISTING 8" P.C.C. BASE COURSE
4. EXISTING 3/4" SMOOTH BAR
5. EXISTING 1/2" BAR
6. EXISTING HMA SURFACE COURSE OVERLAY, VAR
7. EXISTING 8" STABILIZED SHOULDER
8. EXISTING CONCRETE CURB AND GUTTER
9. EXISTING CONCRETE GUTTER
10. EXISTING SUB-BASE GRANULAR
11. EXISTING LEVELING BINDER, 3/4 - 1 1/2
12. EXISTING HMA SURFACE COURSE, 1 1/2
13. EXISTING AGGREGATE SHOULDER, TYPE B

PROPOSED LEGEND

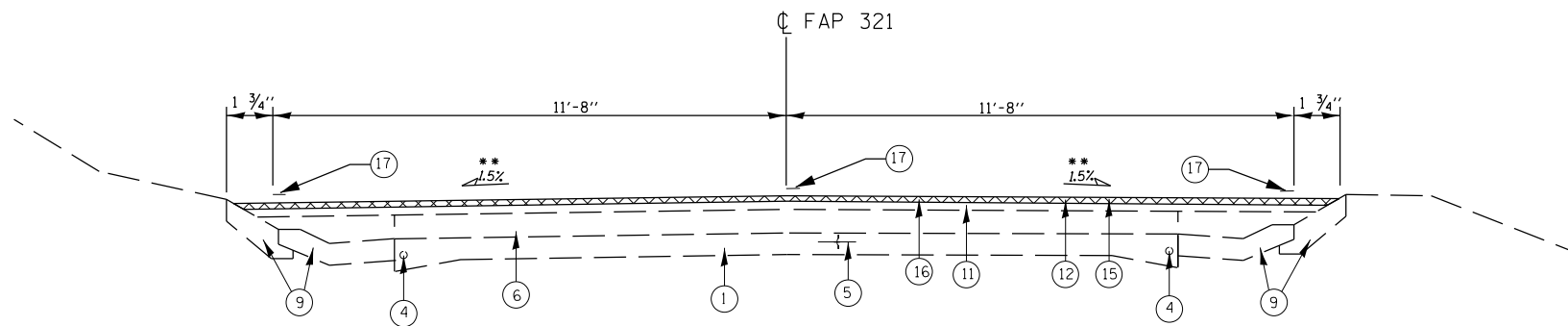
14. PROPOSED AGGREGATE SHOULDER, TYPE B
15. PROPOSED HMA SURFACE COURSE REMOVAL, 1-1/2"
16. PROPOSED HMA SURFACE COURSE MIXTURE C, N50 1-1/2"
17. PROPOSED PAVEMENT MARKING-LINE 5"

• 104RS2,BR3;105RS7,BR3,4;106RS6

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>US 54 TYPICAL SECTIONS</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ci:\pw\work\p\dot\sparksgw\10265393\0672E43-sht-typicol.dgn		DRAWN -	REVISED -		321	*	PIKE	69	19	<b>CONTRACT NO. 72E43</b>		
	PLOT SCALE = 4.2352' / in.	CHECKED -	REVISED -		SCALE:	SHEET NO. 6 OF 10 SHEETS	STA.	TO STA.		ILLINOIS FED. AID PROJECT		
	PLOT DATE = Oct-31-2011 03:50:54PM	DATE -	REVISED -									



LT. STA. 408+70.00 TO 410+75.00  
 RT. STA. 411+00.00 TO 415+47.00



LT. STA. 401+22.00 TO 408+70.00  
 STA. 415+47.00 TO 424+45.00  
 STA. 430+00.00 TO 440+14.40

\*\* NOTE 1.5% SLOPE EXCEPT USE PROP. S.E. RATE SHOWN ON THE PLAN SHEET, SUPERELEVATION TRANSITION DETAILS, AND REVERSE CURVE TRANSITION DETAIL

EXISTING LEGEND

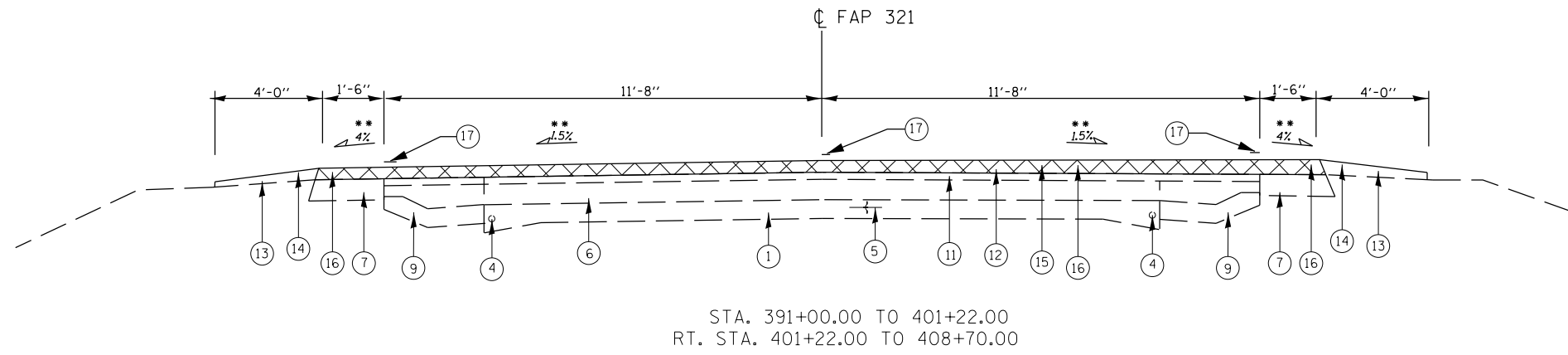
1. EXISTING PCC PAVEMENT (9''-6''-9'')
2. EXISTING PCC COURSE, 12''
3. EXISTING 8'' P.C.C. BASE COURSE
4. EXISTING 3/4 '' SMOOTH BAR
5. EXISTING 1/2 '' BAR
6. EXISTING HMA SURFACE COURSE OVERLAY,VAR
7. EXISTING 8'' STABILIZED SHOULDER
8. EXISTING CONCRETE CURB AND GUTTER
9. EXISTING CONCRETE GUTTER
10. EXISTING SUB-BASE GRANULAR
11. EXISTING LEVELING BINDER, 3/4 - 1 1/2
12. EXISTING HMA SURFACE COURSE, 1 1/2
13. EXISTING AGGREGATE SHOULDER, TYPE B

PROPOSED LEGEND

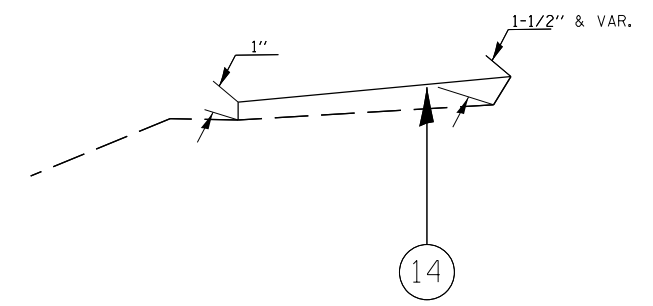
14. PROPOSED AGGREGATE SHOULDER, TYPE B
15. PROPOSED HMA SURFACE COURSE REMOVAL, 1-1/2''
16. PROPOSED HMA SURFACE COURSE MIXTURE C, N50 1-1/2''
17. PROPOSED PAVEMENT MARKING-LINE 5''

• 104RS2,BR3;105RS7,BR3,4;106RS6

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	PLOT SCALE = 4.2352 ' / in.	CHECKED -	REVISED -		SCALE:	SHEET NO. 7 OF 10 SHEETS	STA.	TO STA.	CONTRACT NO. 72E43			
PLOT DATE = Oct-31-2011 03:50:55PM	DATE -	REVISED -	REVISED -	ILLINOIS FED. AID PROJECT								

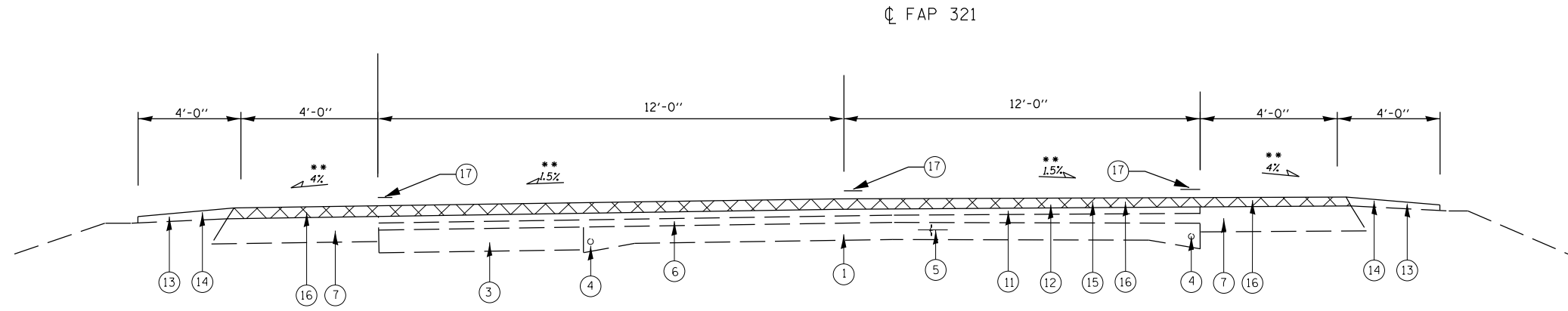


STA. 391+00.00 TO 401+22.00  
RT. STA. 401+22.00 TO 408+70.00



AGGREGATE SHOULDER DETAIL

\*\* NOTE 1.5% SLOPE EXCEPT USE PROP. S.E. RATE SHOWN ON THE PLAN SHEET



STA. 454+00.00 TO 492+00.00

EXISTING LEGEND

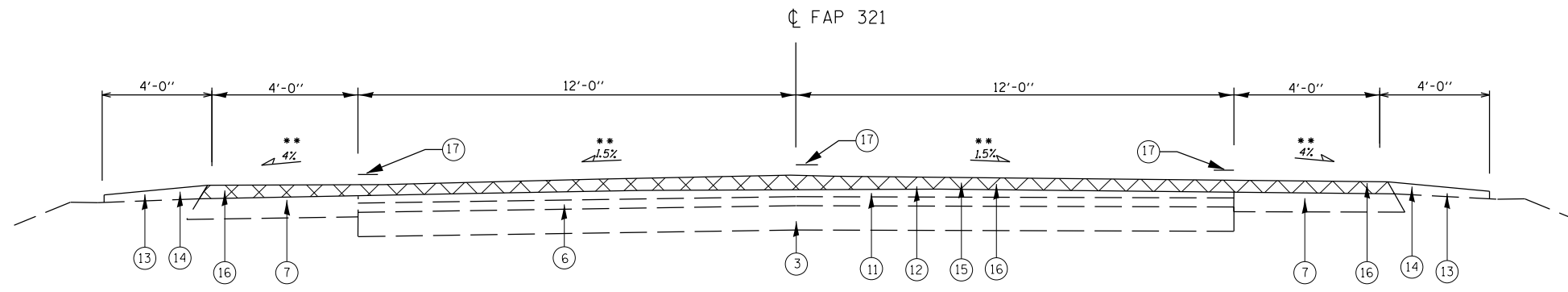
1. EXISTING PCC PAVEMENT (9"-6"-9")
2. EXISTING PCC COURSE, 12"
3. EXISTING 8" P.C.C. BASE COURSE
4. EXISTING 3/4" SMOOTH BAR
5. EXISTING 1/2" BAR
6. EXISTING HMA SURFACE COURSE OVERLAY, VAR
7. EXISTING 8" STABILIZED SHOULDER
8. EXISTING CONCRETE CURB AND GUTTER
9. EXISTING CONCRETE GUTTER
10. EXISTING SUB-BASE GRANULAR
11. EXISTING LEVELING BINDER, 3/4 - 1 1/2
12. EXISTING HMA SURFACE COURSE, 1 1/2
13. EXISTING AGGREGATE SHOULDER, TYPE B

PROPOSED LEGEND

14. PROPOSED AGGREGATE SHOULDER, TYPE B
15. PROPOSED HMA SURFACE COURSE REMOVAL, 1-1/2"
16. PROPOSED HMA SURFACE COURSE MIXTURE C, N50 1-1/2"
17. PROPOSED PAVEMENT MARKING-LINE 5"

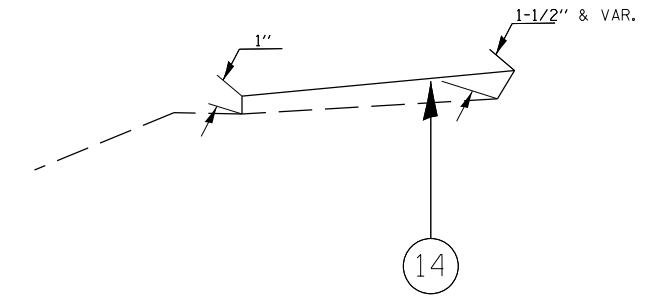
• 104RS2, BR3; 105RS7, BR3, 4; 106RS6

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	PLOT SCALE = 4.2352' / in.	CHECKED -	REVISED -		SCALE:	SHEET NO. 8 OF 10 SHEETS	STA.	TO STA.	CONTRACT NO. 72E43			
PLOT DATE = Oct-31-2011 03:51:33PM	DATE -	REVISED -	REVISED -	ILLINOIS FED. AID PROJECT								



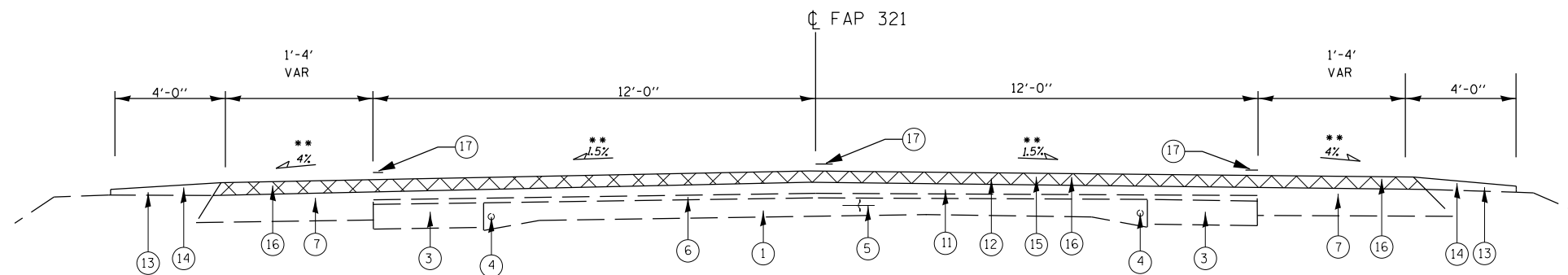
STA. 492+00.00 TO 511+32.00

STA. EQ. 508+94.98 BK. =  
STA. EQ. 509+81.78 AH.



AGGREGATE SHOULDER DETAIL

\*\* NOTE 1.5% SLOPE EXCEPT USE PROP. S.E. RATE SHOWN ON THE PLAN SHEET, SUPERELEVATION TRANSITION DETAILS, AND REVERSE CURVE TRANSITION DETAIL



STA. 523+35.00 TO 524+02.00

EXISTING LEGEND

1. EXISTING PCC PAVEMENT (9''-6''-9'')
2. EXISTING PCC COURSE, 12''
3. EXISTING 8'' P.C.C. BASE COURSE
4. EXISTING 3/4 '' SMOOTH BAR
5. EXISTING 1/2 '' BAR
6. EXISTING HMA SURFACE COURSE OVERLAY,VAR
7. EXISTING 8'' STABLIZED SHOULDER
8. EXISTING CONCRETE CURB AND GUTTER
9. EXISTING CONCRETE GUTTER
10. EXISTING SUB-BASE GRANULAR
11. EXISTING LEVELING BINDER, 3/4 - 1 1/2
12. EXISTING HMA SURFACE COURSE, 1 1/2
13. EXISTING AGGREGATE SHOULDER, TYPE B

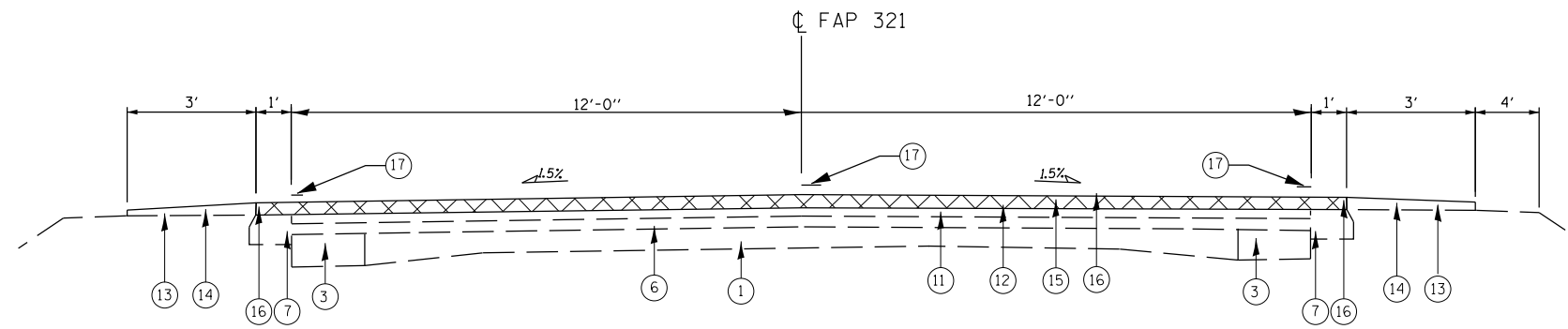
PROPOSED LEGEND

14. PROPOSED AGGREGATE SHOULDER, TYPE B
15. PROPOSED HMA SURFACE COURSE REMOVAL, 1-1/2''
16. PROPOSED HMA SURFACE COURSE MIXTURE C, N50 1-1/2''
17. PROPOSED PAVEMENT MARKING-LINE 5''

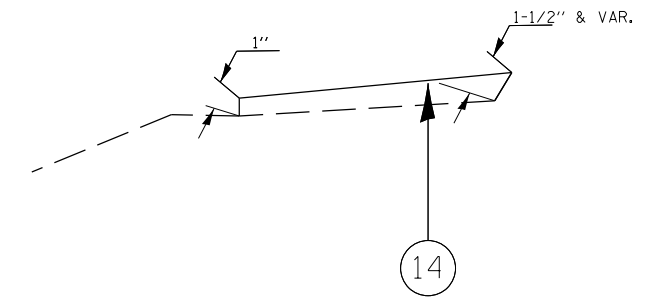
• 104RS2,BR3;105RS7,BR3,4;106RS6

FILE NAME = c:\pwork\pwork\sparksgw\10265393\0672E43-sht-typical.dgn	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>US 54 TYPICAL SECTIONS</b>			F.A.P. RTE. 321	SECTION .	COUNTY PIKE	TOTAL SHEETS 69	SHEET NO. 22
	PLOT SCALE = 4.2352' / in.	CHECKED -	REVISED -		SCALE:	SHEET NO. 9 OF 10 SHEETS	STA.	TO STA.	CONTRACT NO. 72E43			
PLOT DATE = Oct-31-2011 03:51:33PM	DATE -	REVISED -	REVISED -	ILLINOIS FED. AID PROJECT								

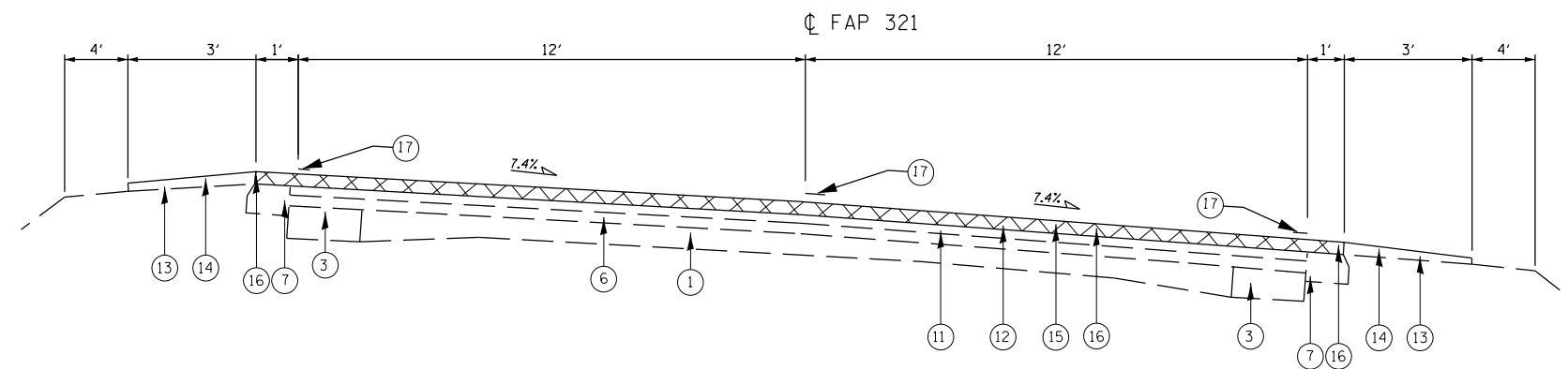




STA. 524+02.00 TO 539+99.71  
 STA. 552+07.08 TO 552+20.00



AGGREGATE SHOULDER DETAIL



STA. 539+78.99 TO 541+76.57 (S.E. TRANS. NC TO 7.4%)  
 STA. 541+76.57 TO 550+30.22 (FULL S.E. 7.4%)  
 STA. 550+30.22 TO 552+27.80 (S.E. TRANS. 7.4% TO NC)

EXISTING LEGEND

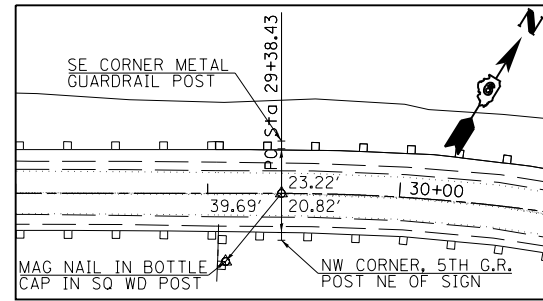
1. EXISTING PCC PAVEMENT (9'-6"-9")
2. EXISTING PCC COURSE, 12"
3. EXISTING 8" P.C.C. BASE COURSE
4. EXISTING 3/4" SMOOTH BAR
5. EXISTING 1/2" BAR
6. EXISTING HMA SURFACE COURSE OVERLAY,VAR
7. EXISTING 8" STABILIZED SHOULDER
8. EXISTING CONCRETE CURB AND GUTTER
9. EXISTING CONCRETE GUTTER
10. EXISTING SUB-BASE GRANULAR
11. EXISTING LEVELING BINDER, 3/4 - 1 1/2
12. EXISTING HMA SURFACE COURSE, 1 1/2
13. EXISTING AGGREGATE SHOULDER, TYPE B

PROPOSED LEGEND

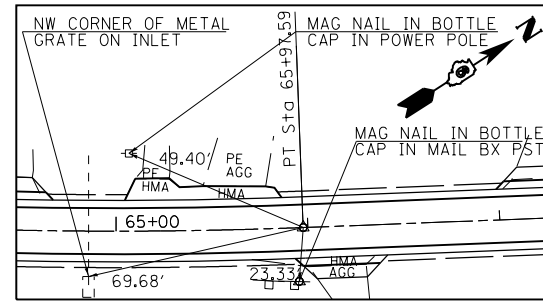
14. PROPOSED AGGREGATE SHOULDER, TYPE B
15. PROPOSED HMA SURFACE COURSE REMOVAL, 1-1/2"
16. PROPOSED HMA SURFACE COURSE MIXTURE C, N50 1-1/2"
17. PROPOSED PAVEMENT MARKING-LINE 5"

• 104RS2,BR3;105RS7,BR3,4;106RS6

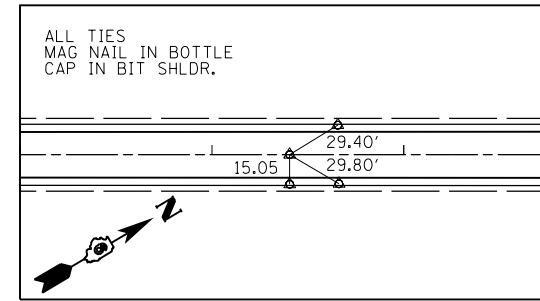
FILE NAME =	USER NAME = sparksgw	DESIGNED - Mark Dust	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>US 54 TYPICAL SECTIONS</b>			F.A.P. RTE. 321	SECTION *	COUNTY PIKE	TOTAL SHEETS 69	SHEET NO. 23
ei:\pw\work\p\dot\sparksgw\10265393\0672E43-sht-typical.dgn		DRAWN -	REVISED -		SCALE:	SHEET NO. 10 OF 10 SHEETS	STA.	TO STA.	CONTRACT NO. 72E43			
		CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT							
		DATE -	REVISED -									



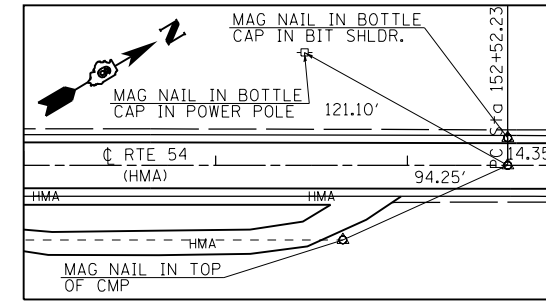
#85501 (MAG NAIL) IN BOTTLE CAP IN SQUARE WOOD POST



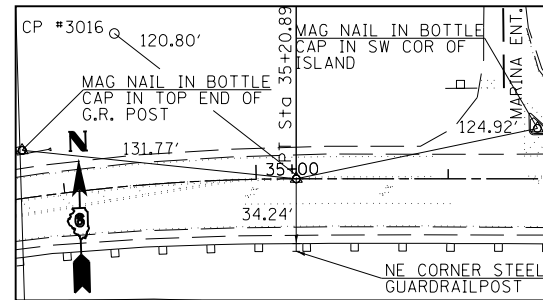
#85509 (MAG NAIL) IN BOTTLE CAP IN PWR POLE (MAG NAIL) IN BOTTLE CAP MAIL BOX POST NW MEDAL GRATE ON INLET



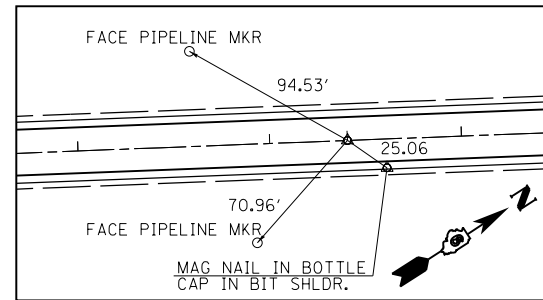
#99604 POT 112+40.54 ALL TIES (MAG NAIL) IN BOTTLE CAP IN BIT. SHLDR.



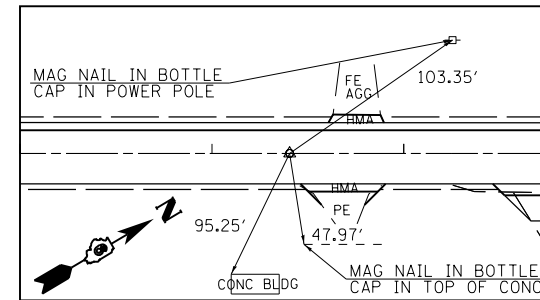
#85512 (MAG NAIL) IN BOTTLE CAP IN BIT SHLDR. (MAG NAIL) IN BOTTLE CAP ON PWR POLE (MAG NAIL) IN TOP OF CMP



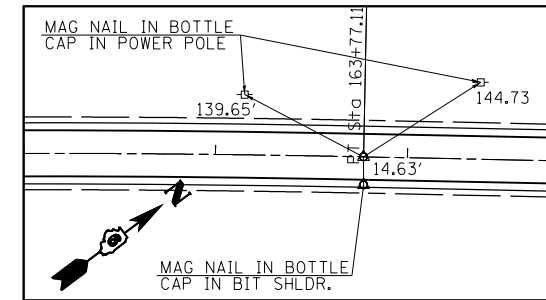
#85502 (MAG NAIL) IN BOTTLE CAP IN TOP END OF G.R. (MAG NAIL) IN BOTTLE CAP IN SW COR. OF ISLAND



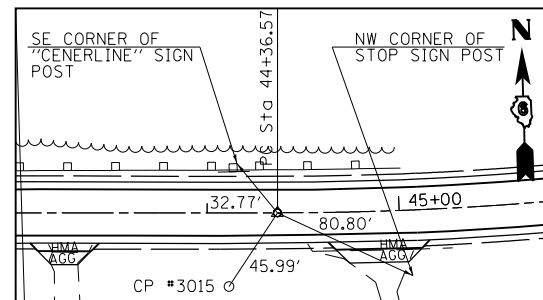
#99600 POT 82+40.51 (MAG NAIL) IN BOTTLE CAP IN BIT SHLDR.



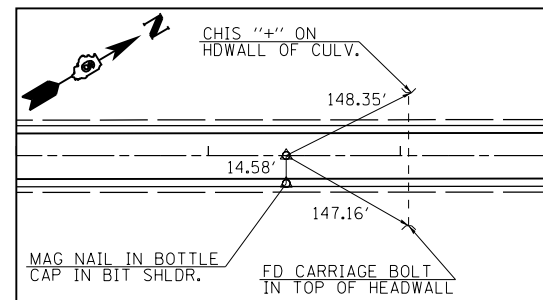
#99605 POT 122+40.51 (MAG NAIL) IN BOTTLE CAP IN PWR POLE (MAG NAIL) IN BOTTLE CAP IN TOP OF CONC.



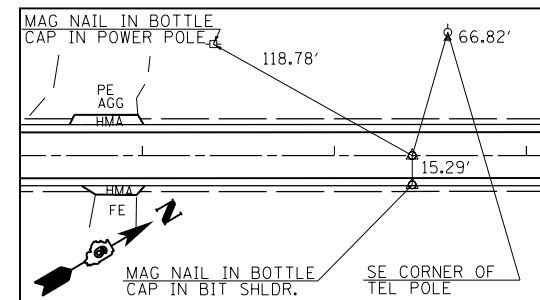
#85514 (MAG NAIL) IN BOTTLE CAP IN BIT SHLDR. (MAG NAIL) IN BOTTLE CAP ON PWR POLE



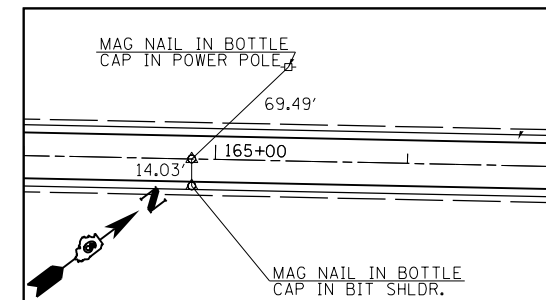
#85504 S.E. CORNER OF "CENTERLINE" SIGN POST NW CORNER OF STOP SIGN POST



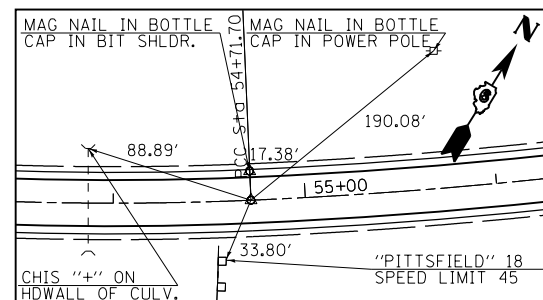
#99602 POT 92+40.60 (MAG NAIL) IN BOTTLE CAP IN BIT. SHLDR. CHIS "+" ON HEADWALL OF CULVERT FD CARRIAGE BOLT IN TOP OF HEADWALL



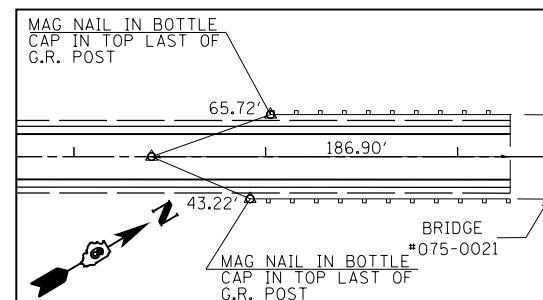
#99606 POT 132+40.56 (MAG NAIL) IN BOTTLE CAP IN PWR POLE (MAG NAIL) IN BOTTLE CAP IN BIT. SHLDR.



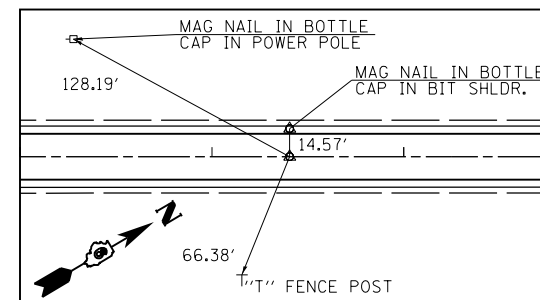
#99608 STA 164+87.61 (MAG NAIL) IN BOTTLE CAP IN PWR POLE (MAG NAIL) IN BOTTLE CAP ON BIT. SHLDR.



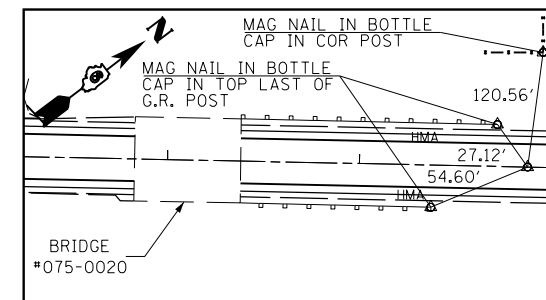
#85506 (MAG NAIL) IN BOTTLE CAP IN PWR POLE (MAG NAIL) IN BOTTLE CAP IN BIT. SHLDR. CHIS "+" ON HEADWALL OF CULVERT



#99603 STA. 102+40.52 (MAG NAIL) IN BOTTLE CAP IN TOP LAST G.R. POST (MAG NAIL) IN BOTTLE CAP IN TOP LAST G.R. POST



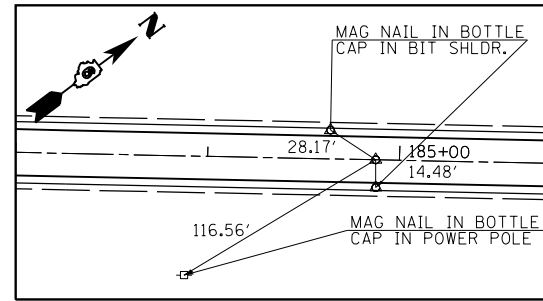
#99607 POT 142+40.50 (MAG NAIL) IN BOTTLE CAP IN PWR POLE (MAG NAIL) IN BOTTLE CAP IN BIT. SHLDR.



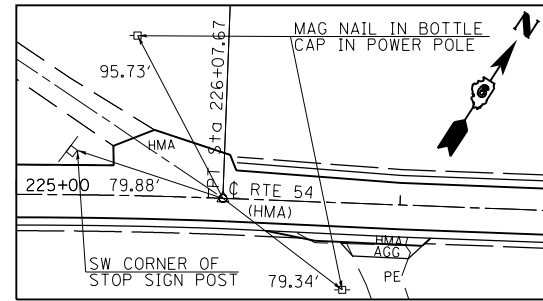
#99609 STA 174+87.62 (MAG NAIL) IN BOTTLE CAP IN TOP LAST GR POST (MAG NAIL) IN BOTTLE CAP COR POST

• 104RS2, BR3; 105RS7, BR3, 4; 106RS6

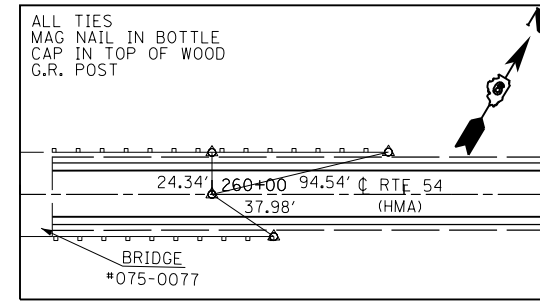
FILE NAME =	USER NAME = sparksgw	DESIGNED - JWC	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ALIGNMENT TIES</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ei:\pw\work\p1dot\sparksgw\10265393\067E43-sht-ATB.dgn		DRAWN - JWC	REVISED -					321	.	PIKE	69	24
PLOT SCALE = 100.0000' / in.		CHECKED - MSD	REVISED -		SCALE: 50			SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 72E43	
PLOT DATE = Oct-31-2011 03:51:22PM		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							



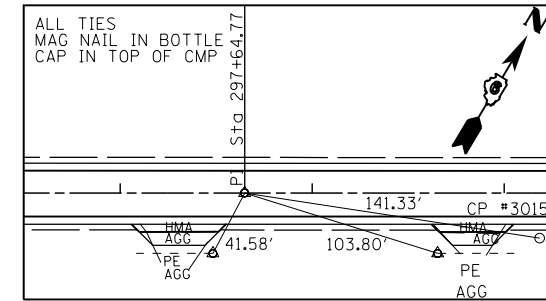
#99610 STA 184+87.63 (MAG NAIL) IN BOTTLE CAP IN PWR POLE  
(MAG NAIL) IN BOTTLE CAP ON BIT. SHLDR.



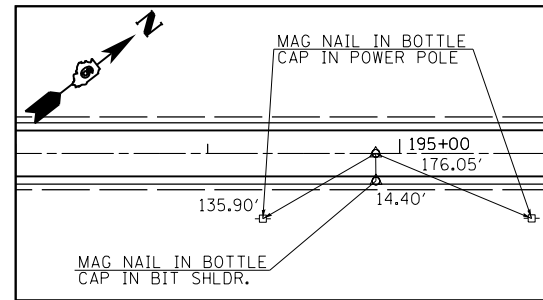
#85518 (MAG NAIL) IN BOTTLE CAP IN PWR POLE  
S. CORNER OF STOP SIGN POST



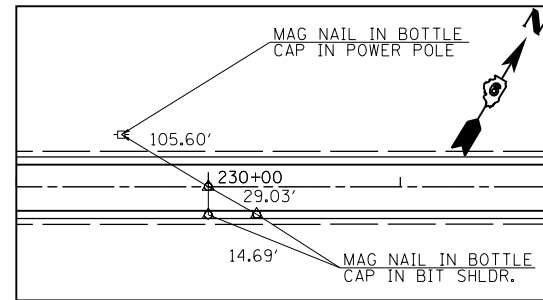
#99623 POT 260+00.00 ALL TIES (MAG NAIL) IN BOTTLE CAP IN TOP WOOD OF G.R. POST



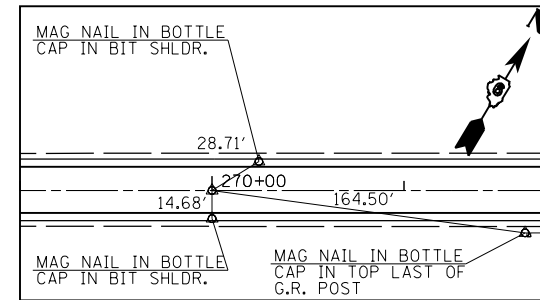
#85519 ALL TIES (MAG NAIL) IN BOTTLE CAP IN TOP OF CMP



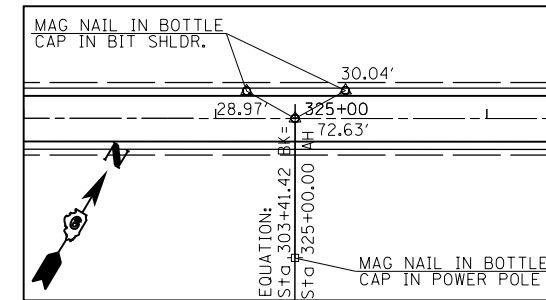
#99610 STA 194+87.60 (MAG NAIL) IN BOTTLE CAP IN PWR POLE  
(MAG NAIL) IN BOTTLE CAP ON BIT. SHLDR.



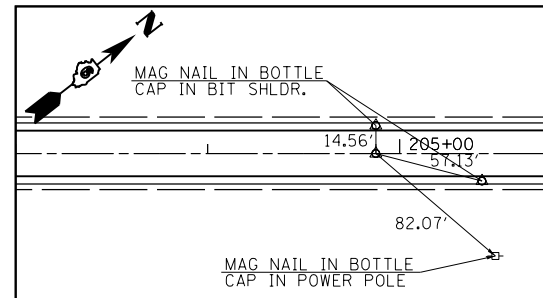
#99635 POT 230+00.00 (MAG NAIL) IN BOTTLE CAP IN PWR POLE  
(MAG NAIL) IN BOTTLE CAP ON BIT. SHLDR.



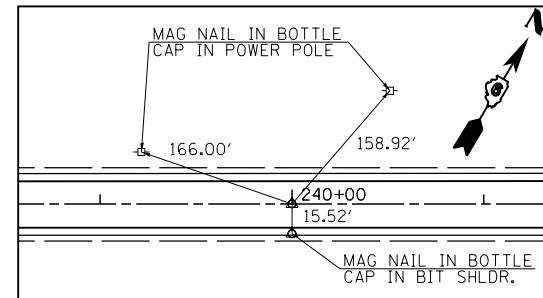
#99622 POT 270+00.00 (MAG NAIL) IN BOTTLE CAP ON BIT. SHLDR  
(MAG NAIL) IN TOP LAST OF G.R. POST



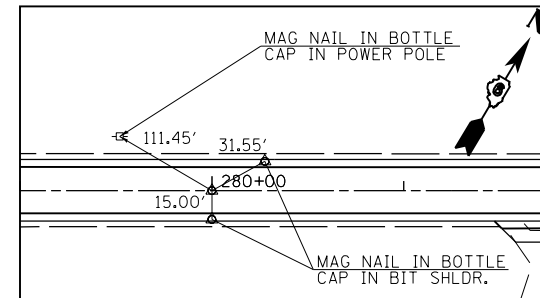
#85524 (MAG NAIL) IN BOTTLE CAP IN BIT SHLDR.  
(MAG NAIL) IN BOTTLE CAP ON PWR POLE



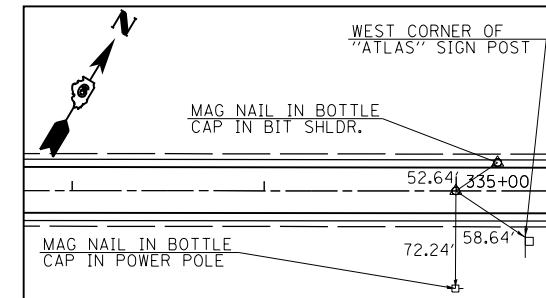
#99612 STA 204+87.61 (MAG NAIL) IN BOTTLE CAP IN PWR POLE  
(MAG NAIL) IN BOTTLE CAP ON BIT. SHLDR.



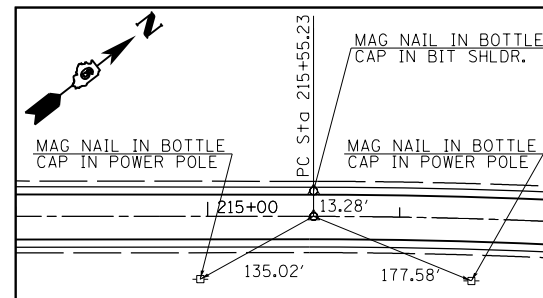
#99625 POT 240+00.00 (MAG NAIL) IN BOTTLE CAP IN PWR POLE  
(MAG NAIL) IN BOTTLE CAP ON BIT. SHLDR.



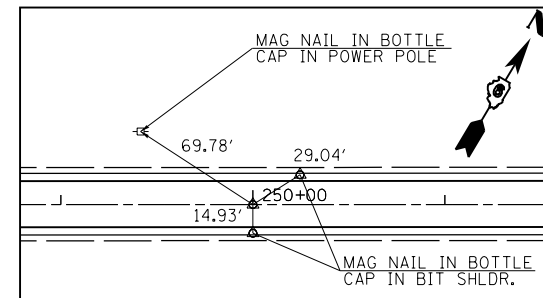
#99621 POT 280+00.00 (MAG NAIL) IN BOTTLE CAP IN PWR POLE  
(MAG NAIL) IN BOTTLE CAP ON BIT. SHLDR.



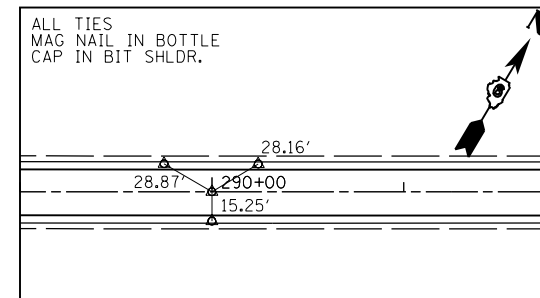
#95530 STA 280+00.00 (MAG NAIL) IN BOTTLE CAP IN PWR POLE  
(MAG NAIL) IN BOTTLE CAP ON BIT. SHLDR.  
WEST COR OF "ATLAS" SIGN



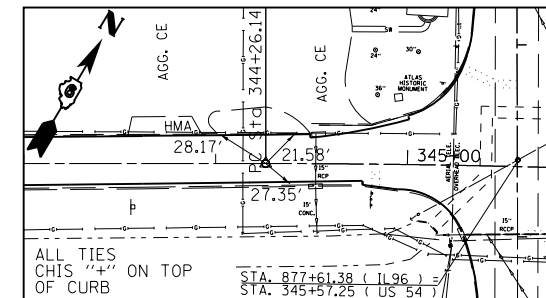
#99616 (MAG NAIL) IN BOTTLE CAP IN PWR POLE  
(MAG NAIL) IN BOTTLE CAP ON BIT. SHLDR.



#99624 POT 250+00.00 (MAG NAIL) IN BOTTLE CAP IN PWR POLE  
(MAG NAIL) IN BOTTLE CAP ON BIT. SHLDR.



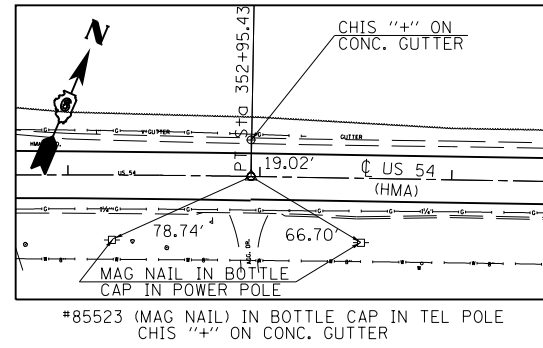
#99620 POT 290+00.00 ALL TIES (MAG NAIL) IN BOTTLE CAP IN BIT. SHLDR.



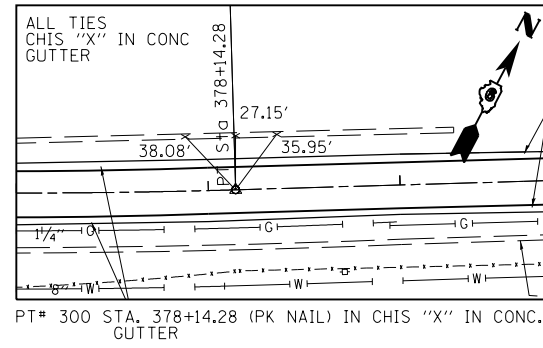
#99623 ALL TIES CHIS "+" ON TOP OF CURB

104RS2, BR3; 105RS7, BR3, 4; 106RS6

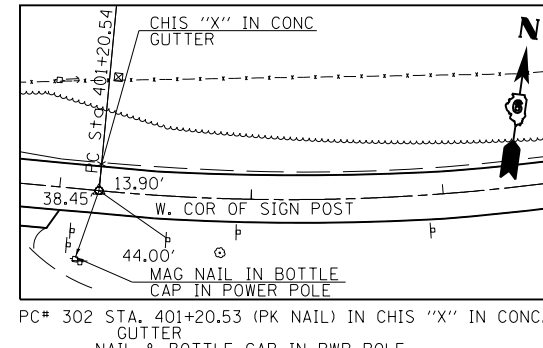
FILE NAME =	USER NAME = sparksgw	DESIGNED - JWC	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ALIGNMENT TIES</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\d01dot\sparksgw\10265393\0672E43-sht-ATB.dgn	DRAWN - JWC	REVISED -	321					.	PIKE	69	25	
PLOT SCALE = 100.0000' / in.	CHECKED - MSD	REVISED -	CONTRACT NO. 72E43									
PLOT DATE = Oct-31-2011 03:51:23PM	DATE -	REVISED -	ILLINOIS FED. AID PROJECT									
					SCALE: 50	SHEET	OF	SHEETS	STA.	TO	STA.	



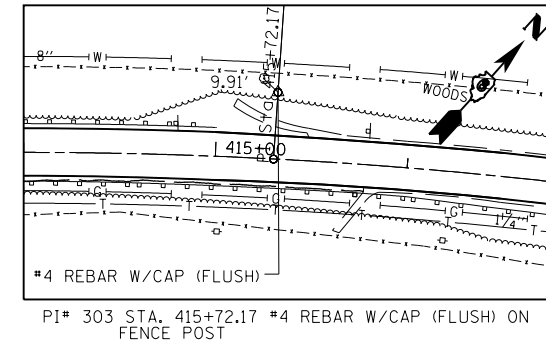
PC# 302 STA. 352+95.43 (PK NAIL) IN CHIS "+" IN CONC. GUTTER  
MAG NAIL IN BOTTLE CAP IN POWER POLE



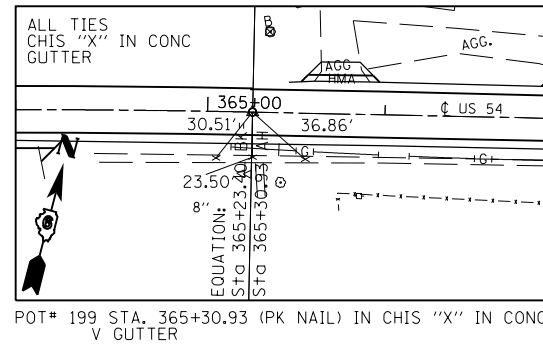
PT# 300 STA. 378+14.28 (PK NAIL) IN CHIS "+" IN CONC. GUTTER



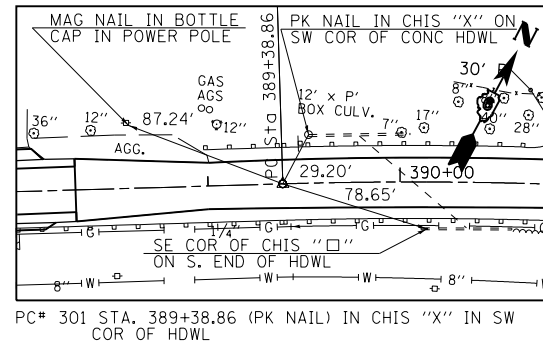
PC# 302 STA. 401+20.53 (PK NAIL) IN CHIS "+" IN CONC. GUTTER  
NAIL & BOTTLE CAP IN PWR POLE



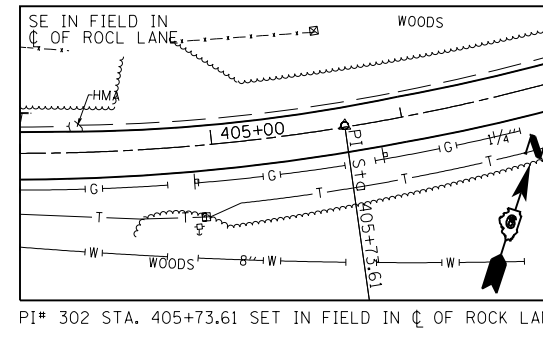
PI# 303 STA. 415+72.17 #4 REBAR W/CAP (FLUSH) ON FENCE POST



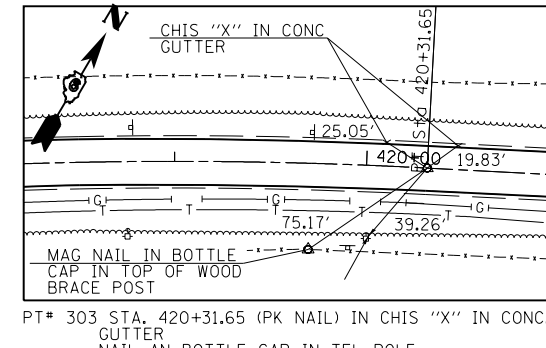
POT# 199 STA. 365+30.93 (PK NAIL) IN CHIS "+" IN CONC. V GUTTER



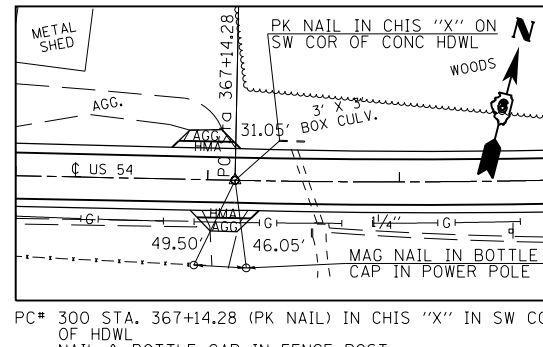
PC# 301 STA. 389+38.86 (PK NAIL) IN CHIS "+" IN SW COR OF HDWL



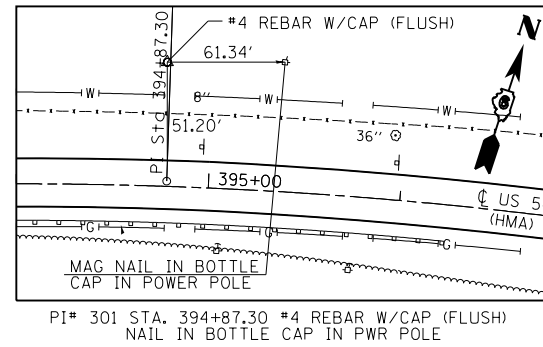
PI# 302 STA. 405+73.61 SET IN FIELD IN C OF ROCK LANE



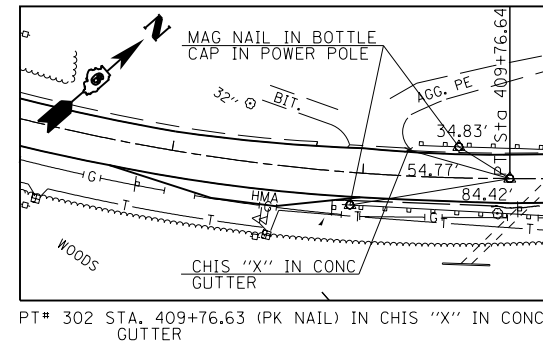
PT# 303 STA. 420+31.65 (PK NAIL) IN CHIS "+" IN CONC. GUTTER  
NAIL IN BOTTLE CAP IN TEL POLE  
NAIL IN BOTTLE CAP ON TOP OF WOOD BRACE POST



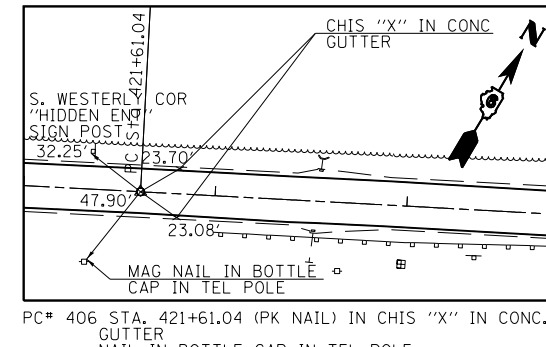
PC# 300 STA. 367+14.28 (PK NAIL) IN CHIS "+" IN SW COR OF HDWL  
NAIL & BOTTLE CAP IN FENCE POST



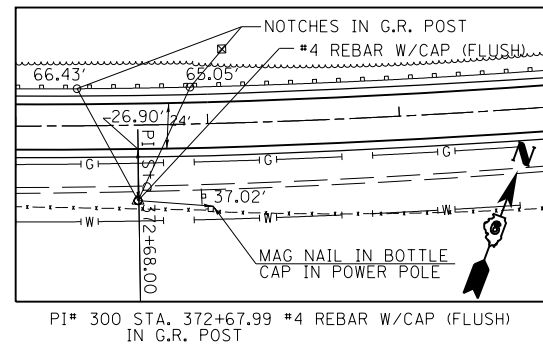
PI# 301 STA. 394+87.30 #4 REBAR W/CAP (FLUSH) NAIL IN BOTTLE CAP IN PWR POLE



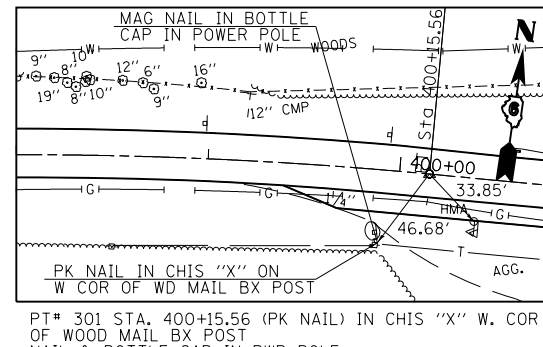
PT# 302 STA. 409+76.63 (PK NAIL) IN CHIS "+" IN CONC. GUTTER



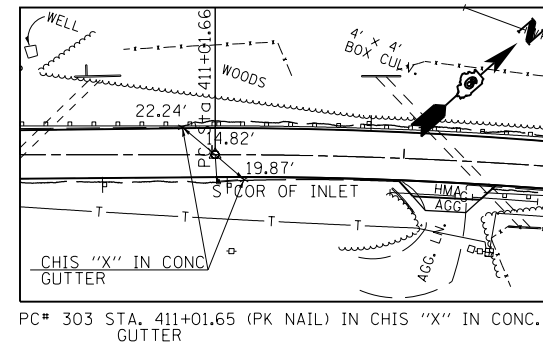
PC# 406 STA. 421+61.04 (PK NAIL) IN CHIS "+" IN CONC. GUTTER  
NAIL IN BOTTLE CAP IN TEL POLE



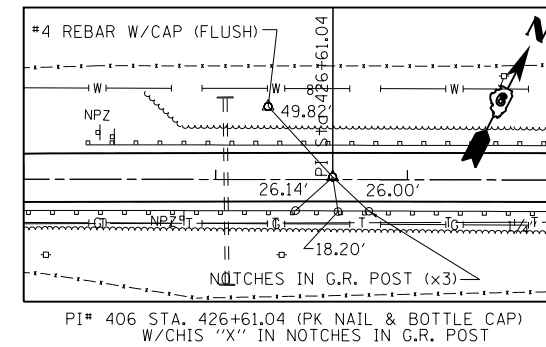
PI# 300 STA. 372+67.99 #4 REBAR W/CAP (FLUSH) IN G.R. POST



PT# 301 STA. 400+15.56 (PK NAIL) IN CHIS "+" W. COR OF WOOD MAIL BX POST  
NAIL & BOTTLE CAP IN PWR POLE



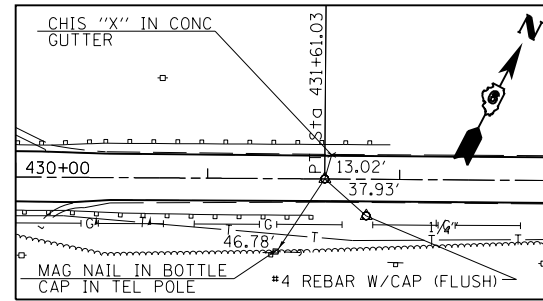
PC# 303 STA. 411+01.65 (PK NAIL) IN CHIS "+" IN CONC. GUTTER



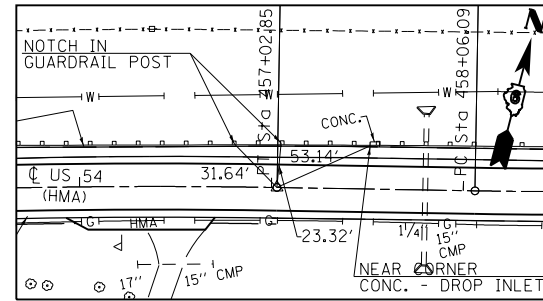
PI# 406 STA. 426+61.04 (PK NAIL & BOTTLE CAP) W/CHIS "+" IN NOTCHES IN G.R. POST

104RS2, BR3; 105RS7, BR3, 4; 106RS6

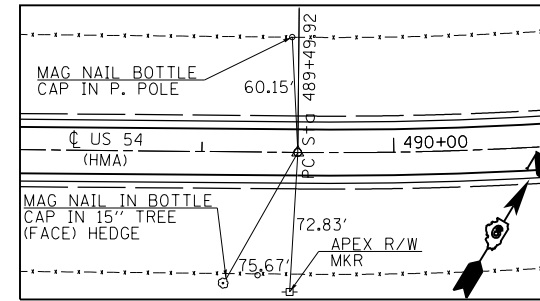
FILE NAME =	USER NAME = sparksgw	DESIGNED - JWC	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ALIGNMENT TIES</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pwork\p\idot\sparksgw\10265393\067E43-sht-ATB.dgn	DRAWN - JWC	REVISED -	321					.	PIKE	69	26	
PLOT SCALE = 100.0000' / in.	CHECKED - MSD	REVISED -	CONTRACT NO. 72E43									
PLOT DATE = Oct-31-2011 03:51:23PM	DATE -	REVISED -	ILLINOIS FED. AID PROJECT									
					SCALE: 50	SHEET	OF	SHEETS	STA.	TO	STA.	



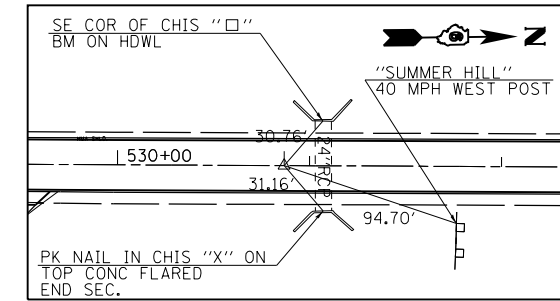
PT# 406 STA. 431+61.03 (PK NAIL & BOTTLE CAP) IN CHIS "X" IN CONC. GUTTER & TEL POLE #4 REBAR W/CAP (FLUSH) TOPO #2055



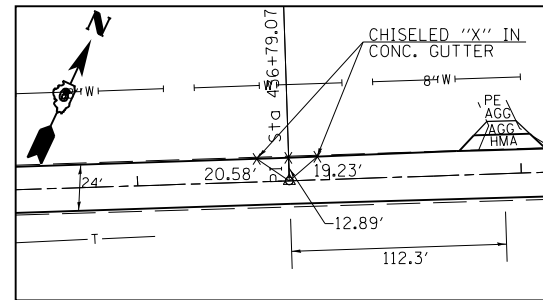
P.T. 403 457+02.853 PK N&BC W/CHSLD. "X"



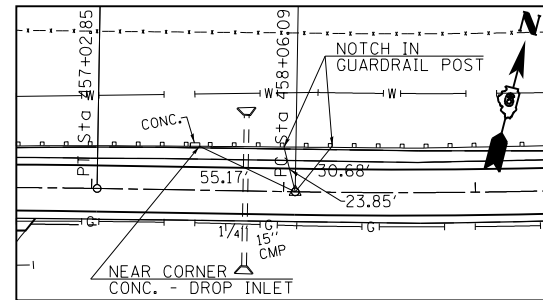
#85609 PC #20 - 489+49.92



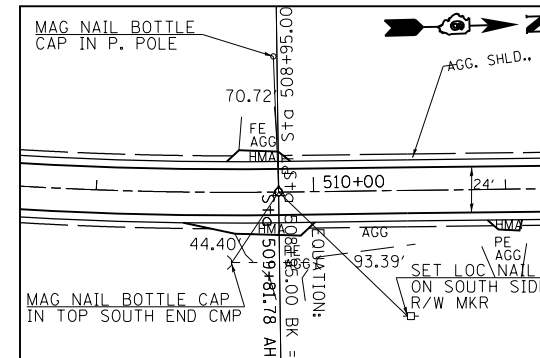
#99634 530+86.78



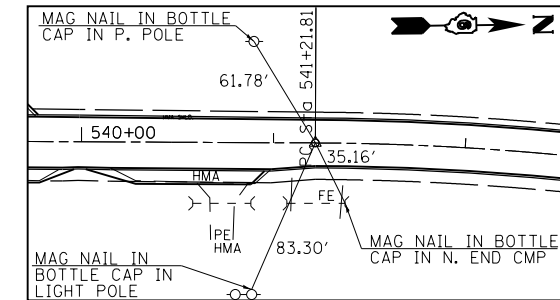
P.I. 405 436+79.070 PK N & BK BK WITH CHSLD. "X"



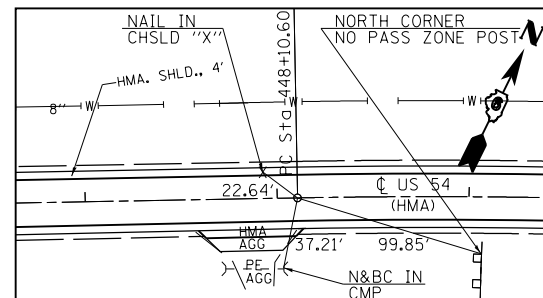
PC 402 458+06.089 N&BC W/CHSLD. "X"



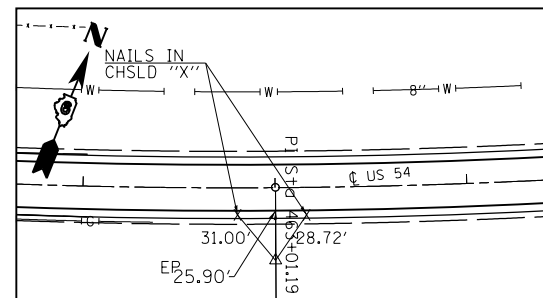
#85611 PT #20 - 508+95.00 BK 509+81.78 AH



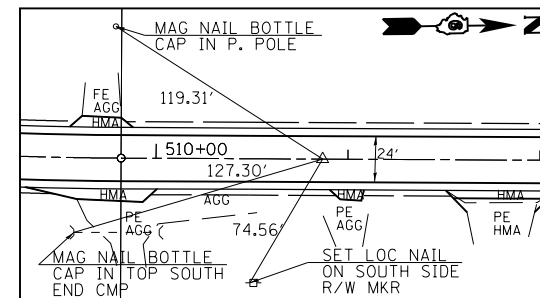
#85604 PC - 541+21.81



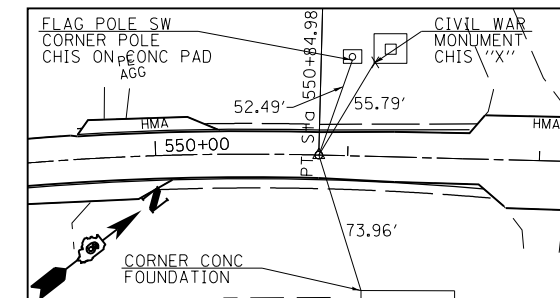
P.C. 403 448+10.603



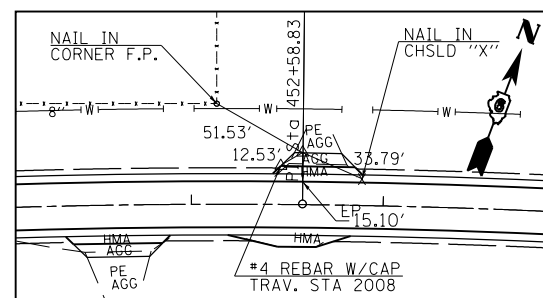
P.I. 402 463+01.189 #4 REBAR W/CAP BURIED 1"



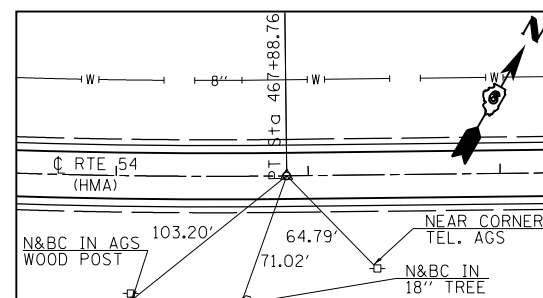
#99611 510+86.78



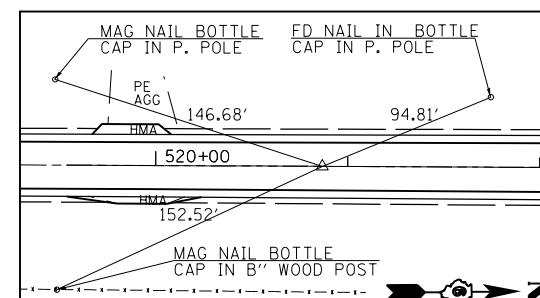
#85602 PT - 550+84.98



P.I. 403 452+58.830 #4 REBAR W/CAP BURIED 1"



P.T. 402 467+88.763 PK N&BC W/CHSLD "X"



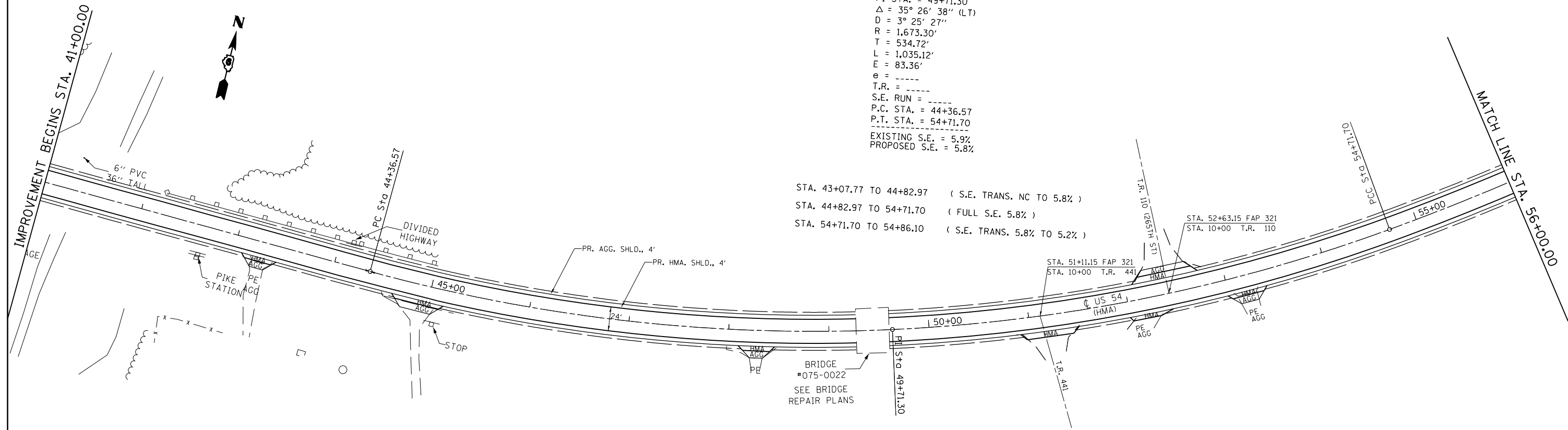
#99633 520+86.78

• 104RS2, BR3; 105RS7, BR3, 4; 106RS6

FILE NAME = c:\pwork\pwork\sparksgw\10265393\067E43-sht-ATB.dgn	USER NAME = sparksgw	DESIGNED - JWC	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ALIGNMENT TIES</b>			F.A.P. RTE. 321	SECTION .	COUNTY PIKE	TOTAL SHEETS 69	SHEET NO. 27
	PLOT SCALE = 100.0000' / in.	CHECKED - MSD	REVISED -					SCALE: 50	SHEET OF SHEETS	STA. TO STA.	CONTRACT NO. 72E43	
PLOT DATE = Oct-31-2011 03:51:24PM	DATE -	DATE -	REVISED -	ILLINOIS FED. AID PROJECT								

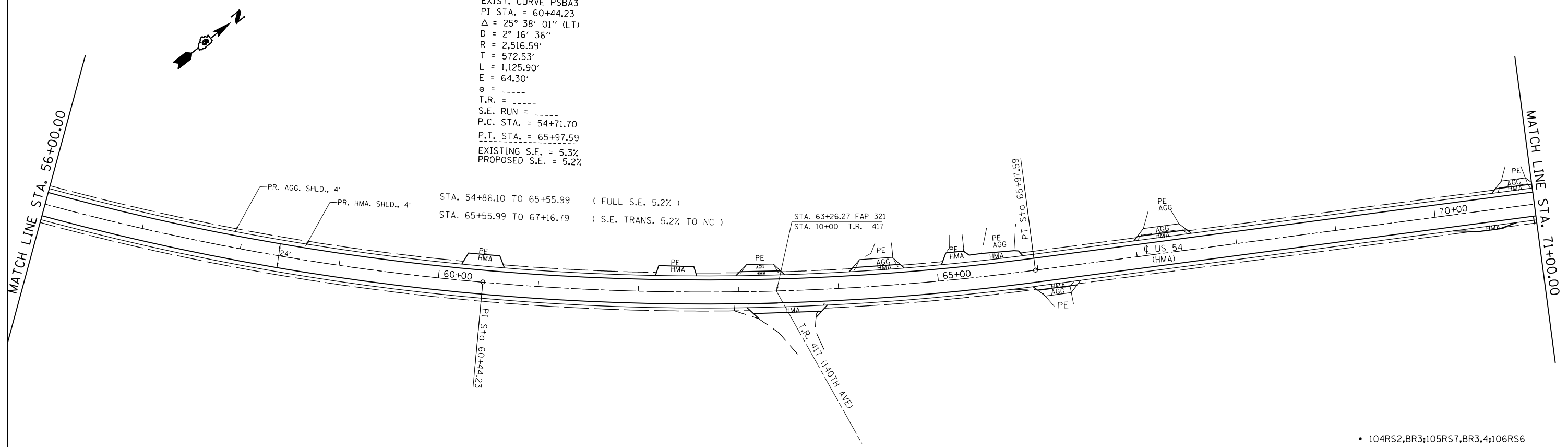
EXIST. CURVE PSBA2  
 PI STA. = 49+71.30  
 $\Delta = 35^\circ 26' 38''$  (LT)  
 $D = 3^\circ 25' 27''$   
 $R = 1,673.30'$   
 $T = 534.72'$   
 $L = 1,035.12'$   
 $E = 83.36'$   
 $e = \text{-----}$   
 T.R. = -----  
 S.E. RUN = -----  
 P.C. STA. = 44+36.57  
 P.T. STA. = 54+71.70  
 EXISTING S.E. = 5.9%  
 PROPOSED S.E. = 5.8%

STA. 43+07.77 TO 44+82.97 ( S.E. TRANS. NC TO 5.8% )  
 STA. 44+82.97 TO 54+71.70 ( FULL S.E. 5.8% )  
 STA. 54+71.70 TO 54+86.10 ( S.E. TRANS. 5.8% TO 5.2% )



EXIST. CURVE PSBA3  
 PI STA. = 60+44.23  
 $\Delta = 25^\circ 38' 01''$  (LT)  
 $D = 2^\circ 16' 36''$   
 $R = 2,516.59'$   
 $T = 572.53'$   
 $L = 1,125.90'$   
 $E = 64.30'$   
 $e = \text{-----}$   
 T.R. = -----  
 S.E. RUN = -----  
 P.C. STA. = 54+71.70  
 P.T. STA. = 65+97.59  
 EXISTING S.E. = 5.3%  
 PROPOSED S.E. = 5.2%

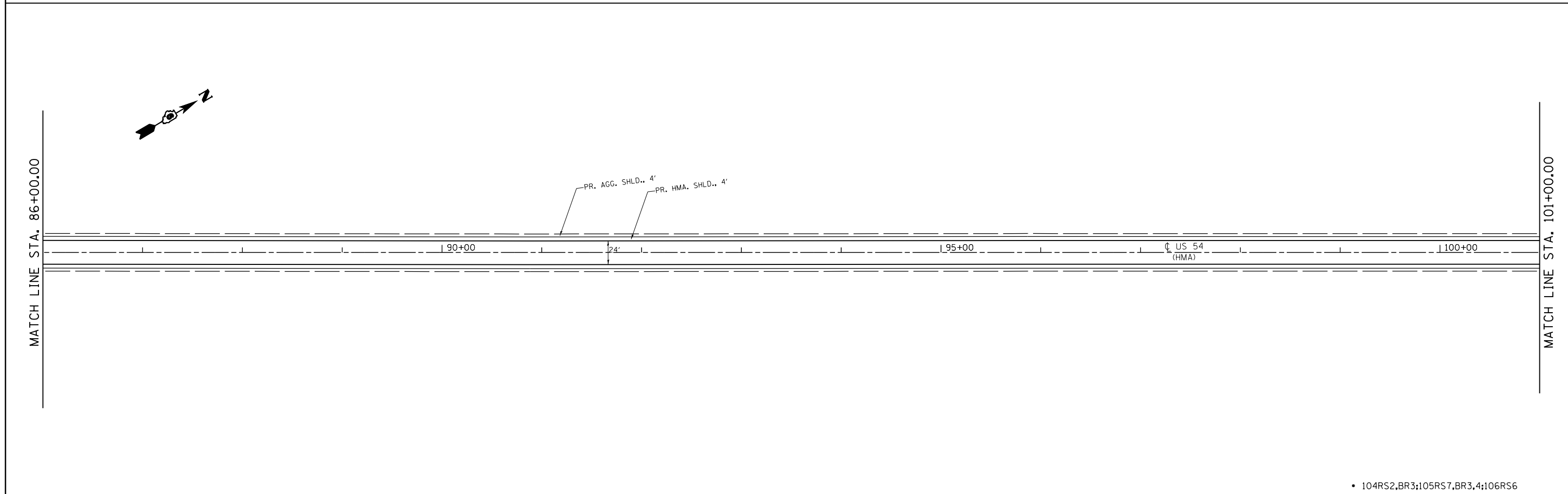
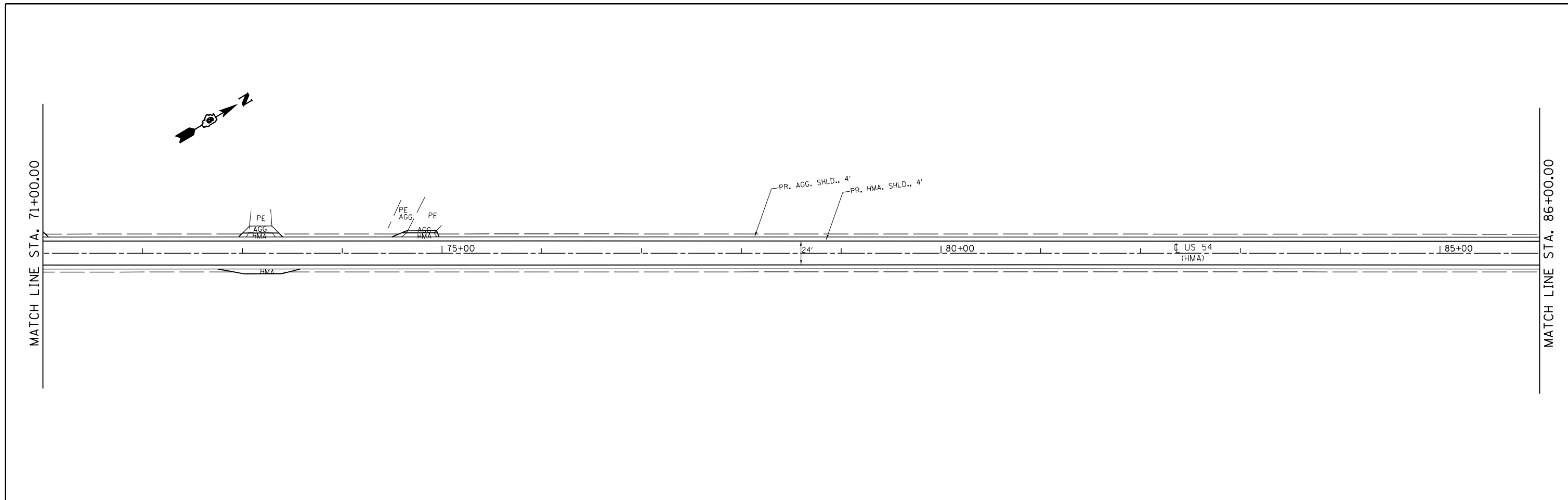
STA. 54+86.10 TO 65+55.99 ( FULL S.E. 5.2% )  
 STA. 65+55.99 TO 67+16.79 ( S.E. TRANS. 5.2% TO NC )



• 104RS2,BR3;105RS7,BR3,4;106RS6

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PLAN SHEET</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwwork\pwwork\sparksgw\10265393\PLAN.dgn		DRAWN -	REVISED -		321	•	PIKE	69	28			
PLOT SCALE = 100.0000' / in.		CHECKED -	REVISED -		CONTRACT NO. 72E43							
PLOT DATE = Oct-31-2011 03:51:33PM		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							
				SCALE: 1"=100'	SHEET NO. 1 OF 17 SHEETS	STA. 41+00.00 TO STA. 71+00.00						





• 104RS2,BR3;105RS7,BR3,4;106RS6

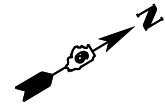
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ei:\pw\work\p\idot\sparksgw\10265393\PLAN\dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = Oct-31-2011 03:51:34PM	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PLAN SHEET**

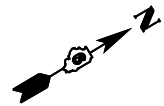
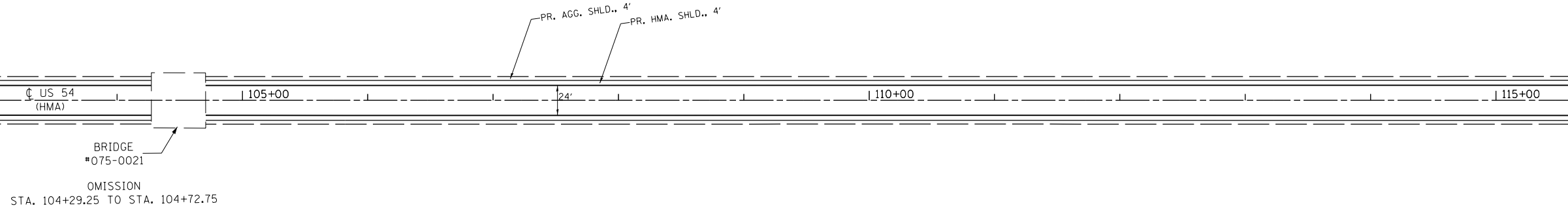
SCALE: 1"=100'    SHEET NO. 2 OF 17 SHEETS    STA. 71+00.00 TO STA. 101+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
321	•	PIKE	69	29
				<b>CONTRACT NO. 72E43</b>
ILLINOIS FED. AID PROJECT				



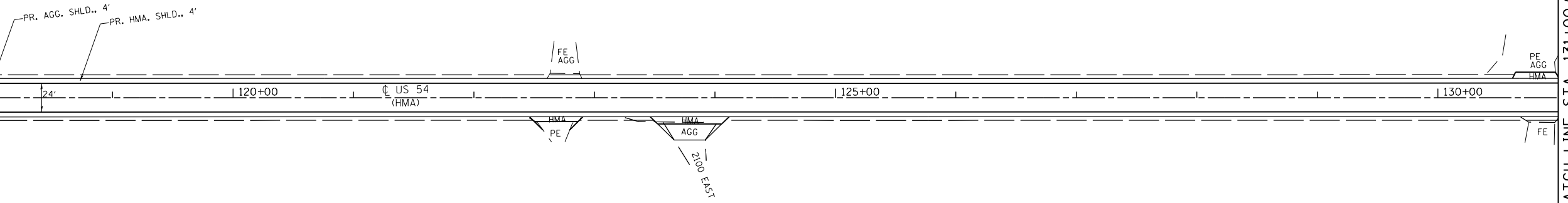
MATCH LINE STA. 101+00.00

MATCH LINE STA. 116+00.00



MATCH LINE STA. 116+00.00

MATCH LINE STA. 131+00.00



• 104RS2,BR3;105RS7,BR3,4;106RS6

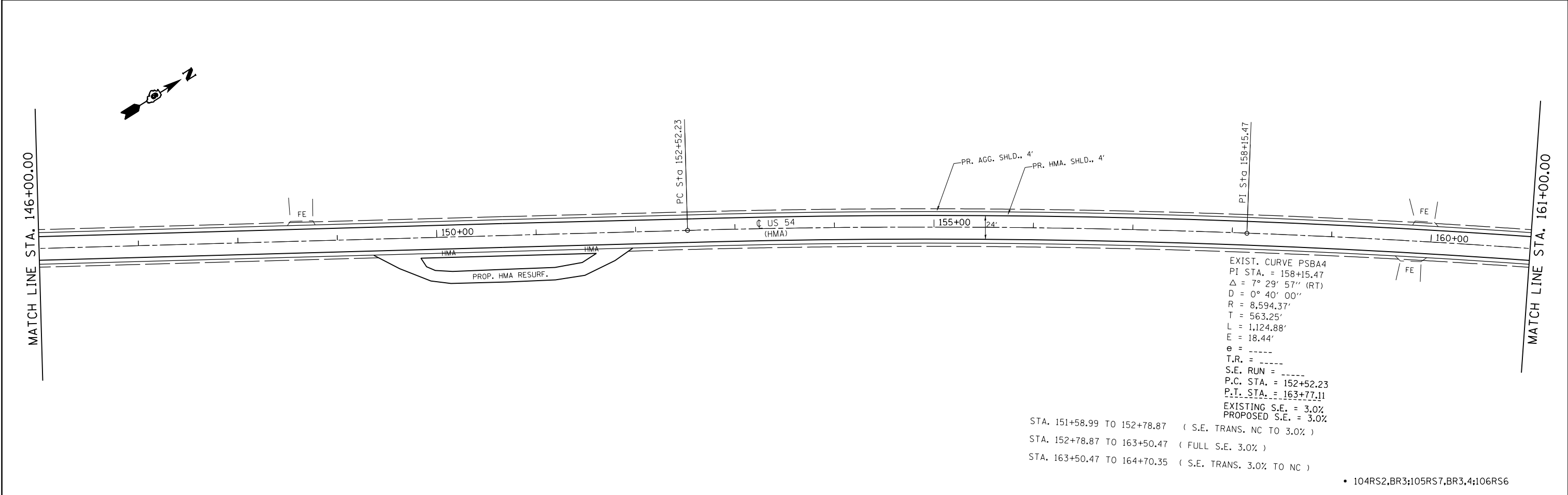
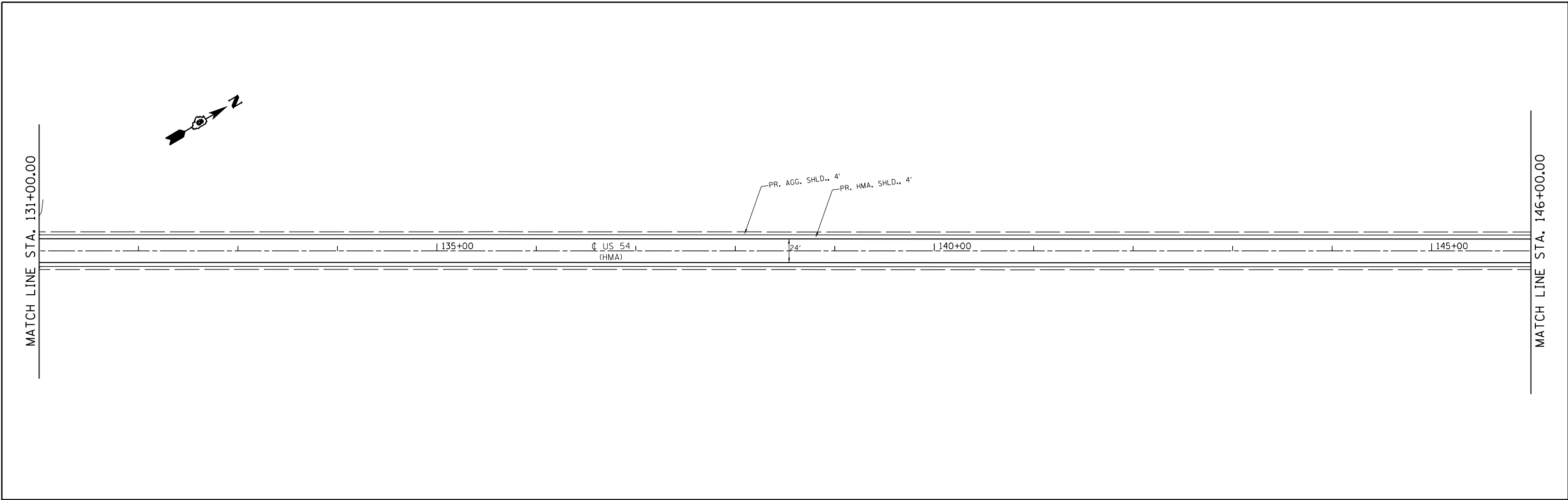
FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -
c:\pwwork\pwwork\sparksgw\10265393\PLAN.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = Oct-31-2011 03:51:34PM	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PLAN SHEET**

SCALE: 1"=100'      SHEET NO. 3 OF 17 SHEETS      STA. 101+00.00 TO STA. 131+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
321	•	PIKE	69	30
				<b>CONTRACT NO. 72E43</b>
ILLINOIS FED. AID PROJECT				



EXIST. CURVE PSBA4  
 PI STA. = 158+15.47  
 $\Delta = 7^\circ 29' 57''$  (RT)  
 $D = 0^\circ 40' 00''$   
 $R = 8,594.37'$   
 $T = 563.25'$   
 $L = 1,124.88'$   
 $E = 18.44'$   
 $e =$  -----  
 $T.R. =$  -----  
 $S.E. RUN =$  -----  
 $P.C. STA. = 152+52.23$   
 $P.T. STA. = 163+77.11$   
 EXISTING S.E. = 3.0%  
 PROPOSED S.E. = 3.0%

STA. 151+58.99 TO 152+78.87 ( S.E. TRANS. NC TO 3.0% )  
 STA. 152+78.87 TO 163+50.47 ( FULL S.E. 3.0% )  
 STA. 163+50.47 TO 164+70.35 ( S.E. TRANS. 3.0% TO NC )

• 104RS2,BR3;105RS7,BR3,4;106RS6

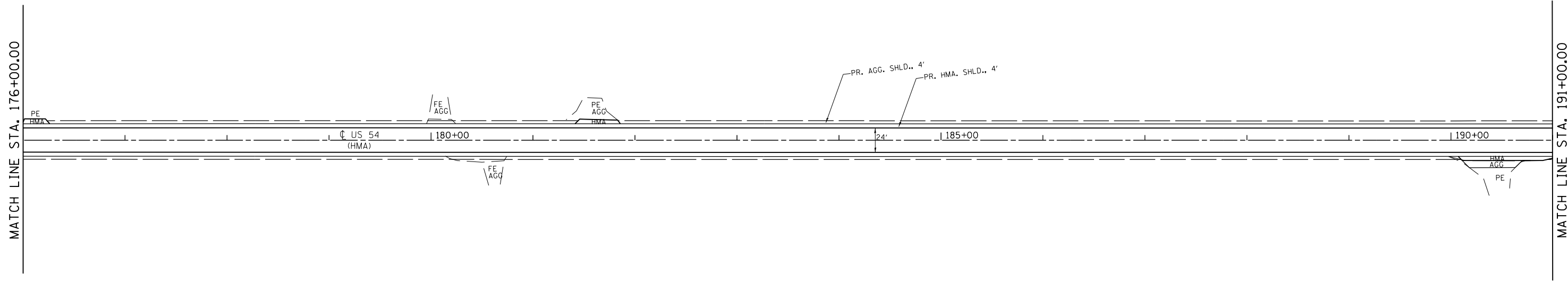
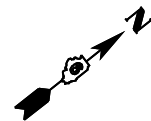
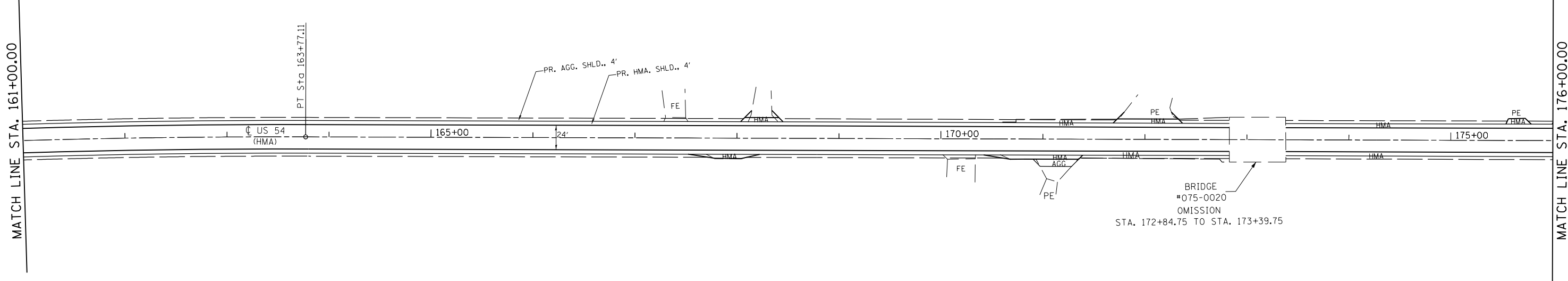
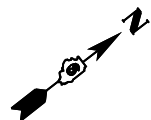
FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = Oct-31-2011 03:51:34PM	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**PLAN SHEET**

SCALE: 1"=100'    SHEET NO. 4 OF 17 SHEETS    STA. 131+00.00 TO STA. 161+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
321	•	PIKE	69	31
CONTRACT NO. 72E43				
ILLINOIS FED. AID PROJECT				



• 104RS2,BR3;105RS7,BR3,4;106RS6

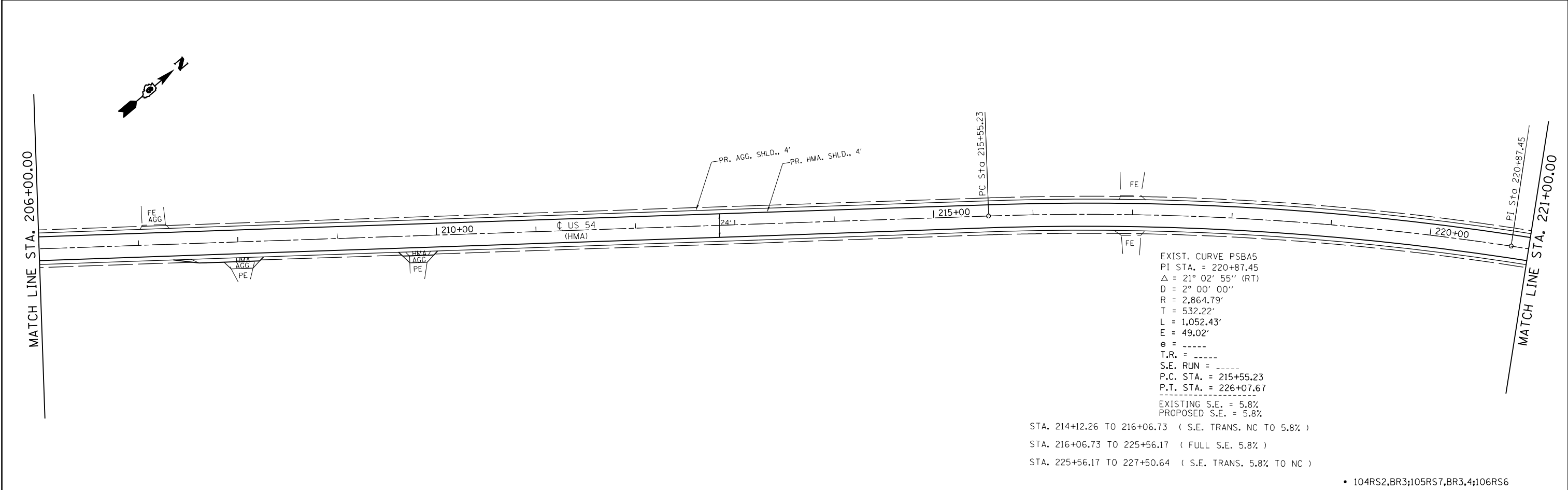
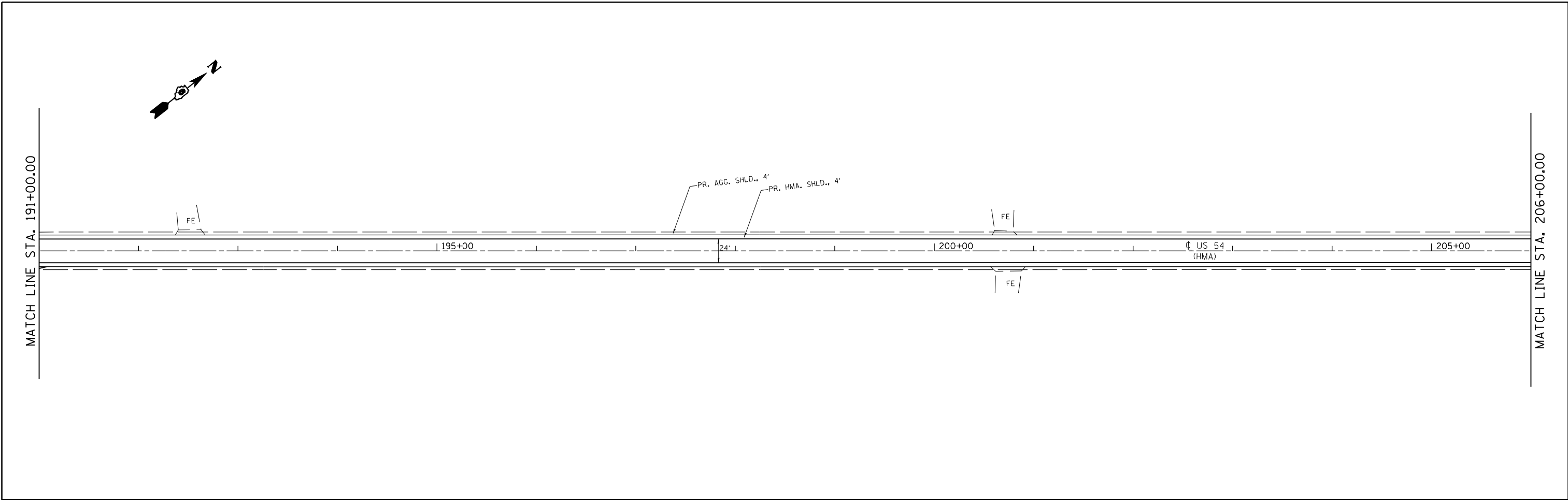
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c:\pwwork\pwwork\sparksgw\10265393\PLAN.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = Oct-31-2011 03:51:35PM	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PLAN SHEET**

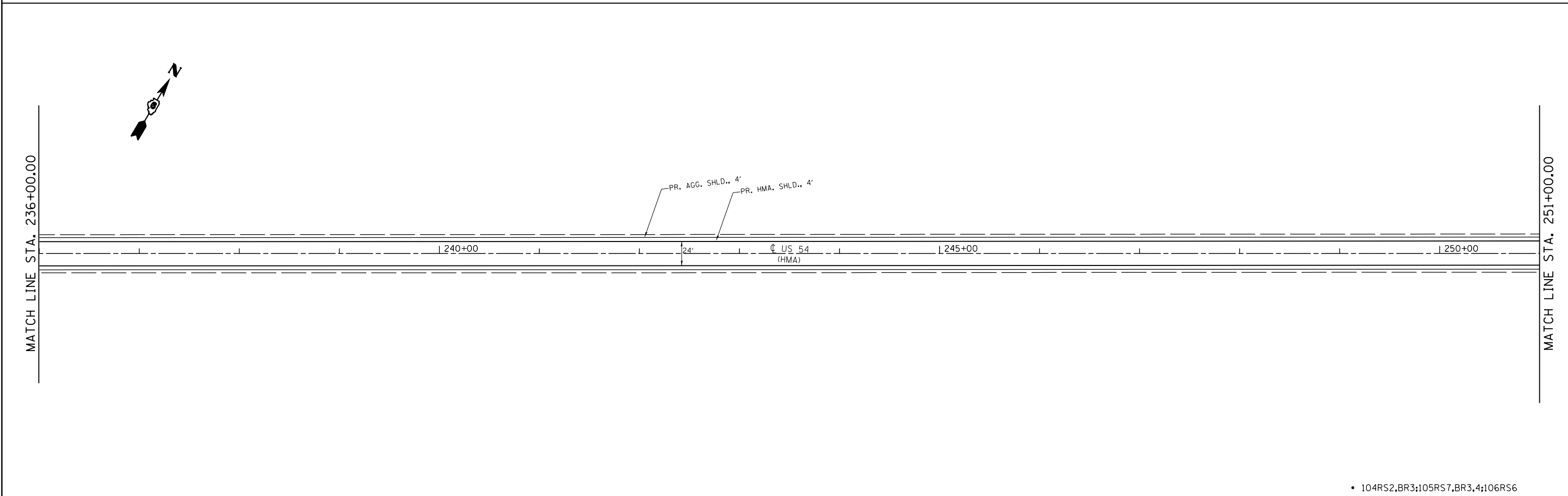
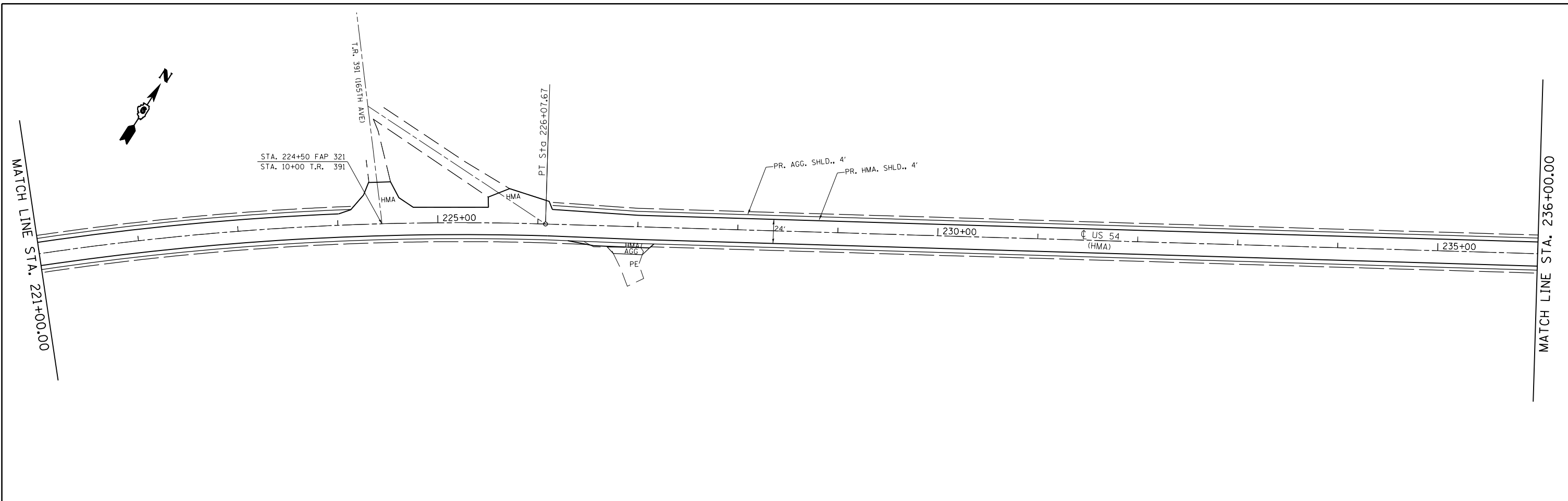
SCALE: 1"=100'    SHEET NO. 5 OF 17 SHEETS    STA. 161+00.00 TO STA. 191+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
321	•	PIKE	69	32
<b>CONTRACT NO. 72E43</b>				
ILLINOIS FED. AID PROJECT				



FILE NAME = c:\pwwork\pwwork\sparksgw\10265393\PLAN.dgn	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PLAN SHEET</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		321		PIKE	69	33			
PLOT DATE = Oct-31-2011 03:51:35PM	DATE -	REVISED -	REVISED -		SCALE: 1"=100'    SHEET NO. 6 OF 17 SHEETS    STA. 191+00.00 TO STA. 221+00.00			CONTRACT NO. 72E43				
					ILLINOIS FED. AID PROJECT							

• 104RS2,BR3;105RS7,BR3,4;106RS6



• 104RS2,BR3;105RS7,BR3,4;106RS6

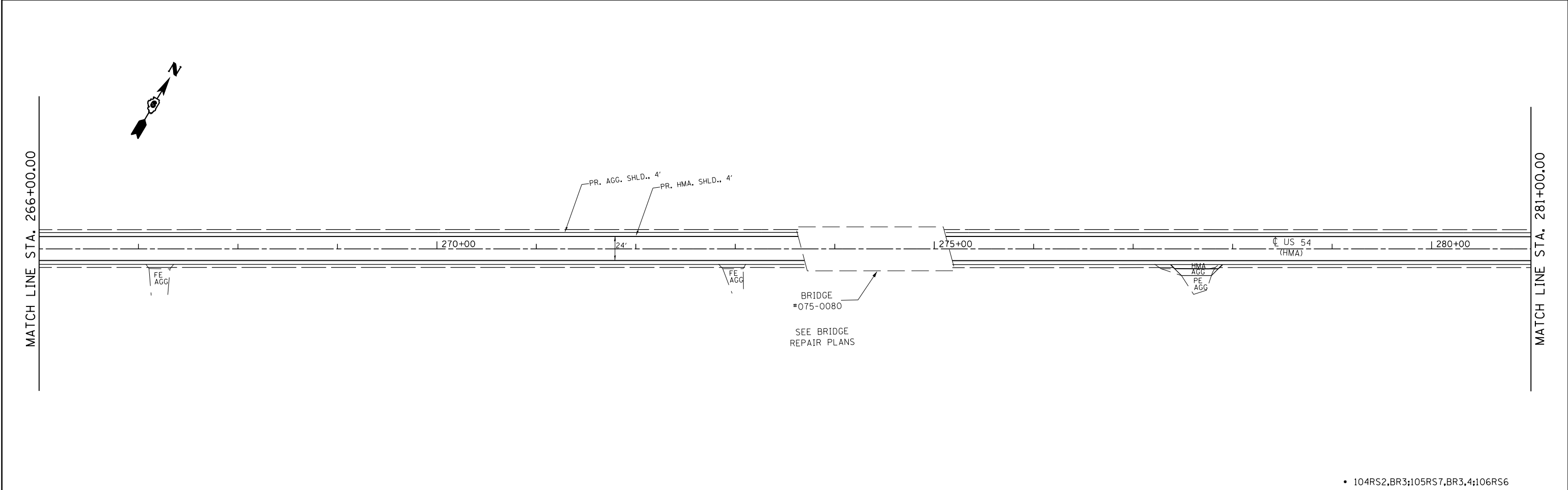
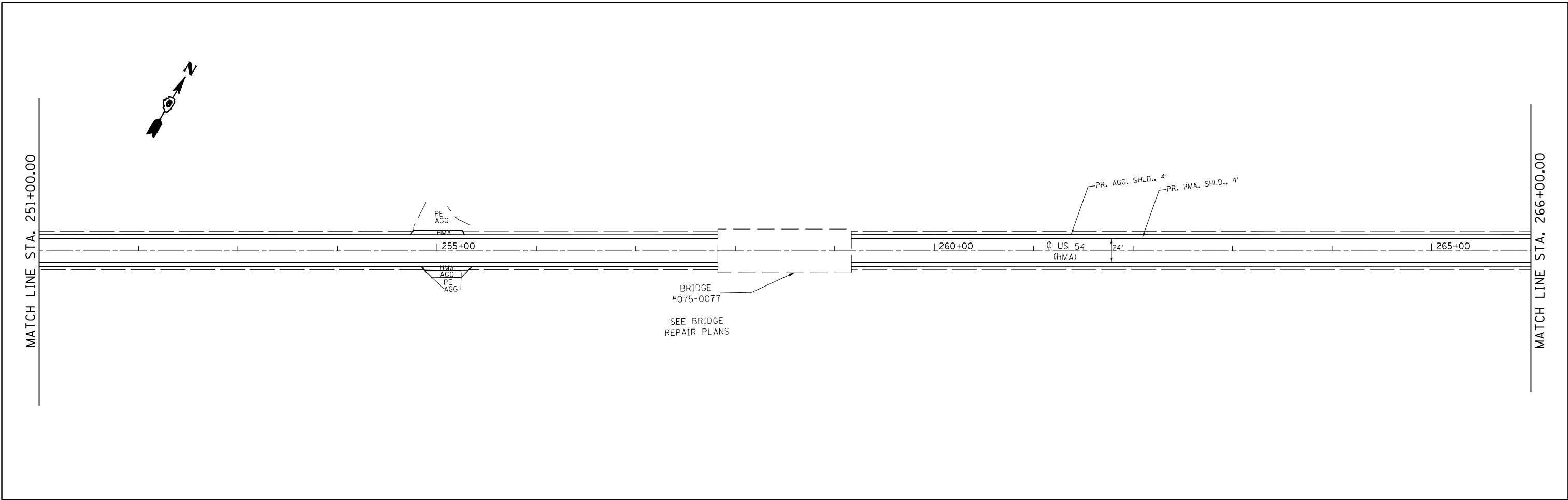
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ei:\pw\work\p\id\sparksgw\10265393\PLAN\dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = Oct-31-2011 03:51:36PM	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PLAN SHEET**

SCALE: 1"=100'    SHEET NO. 7 OF 17 SHEETS    STA. 221+00.00 TO STA. 251+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
321	•	PIKE	69	34
				<b>CONTRACT NO. 72E43</b>
ILLINOIS FED. AID PROJECT				



• 104RS2,BR3;105RS7,BR3,4;106RS6

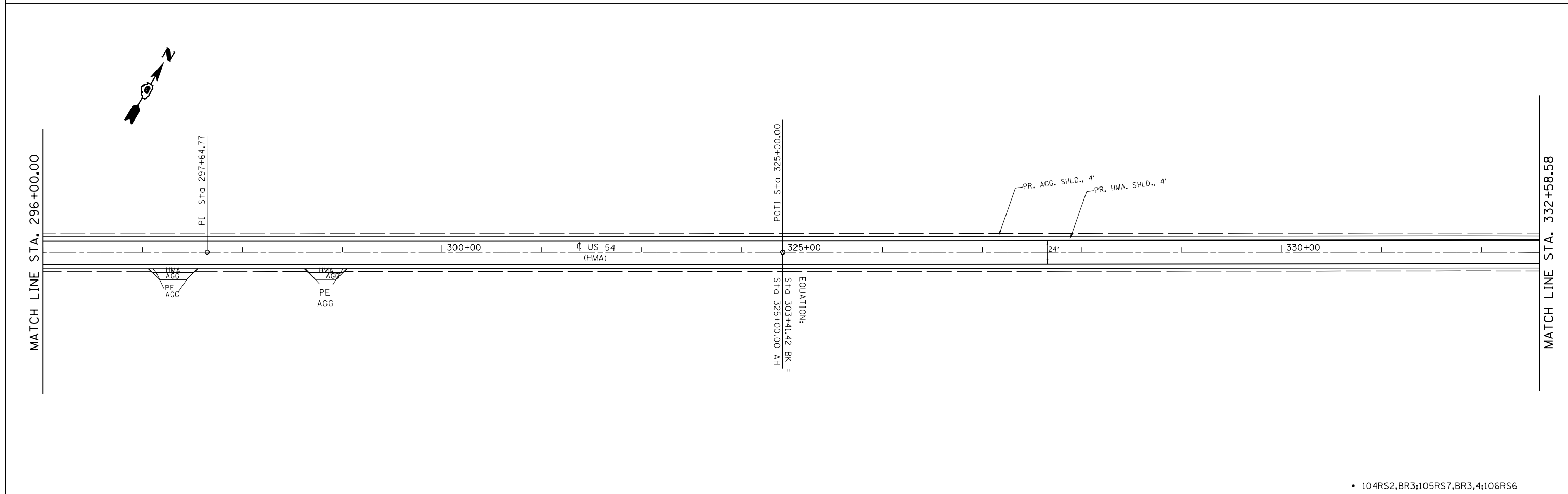
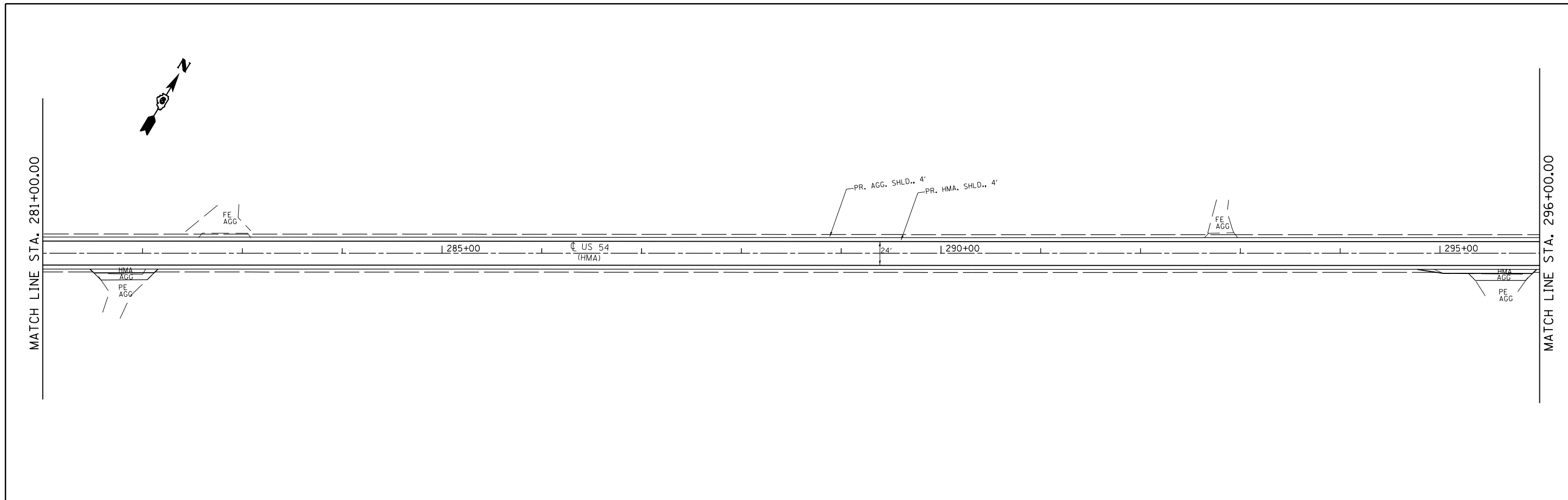
FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = Oct-31-2011 03:51:36PM	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PLAN SHEET**

SCALE: 1"=100'    SHEET NO. 8 OF 17 SHEETS    STA. 251+00.00 TO STA. 281+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
321	•	PIKE	69	35
CONTRACT NO. 72E43				
ILLINOIS FED. AID PROJECT				



• 104RS2,BR3;105RS7,BR3,4;106RS6

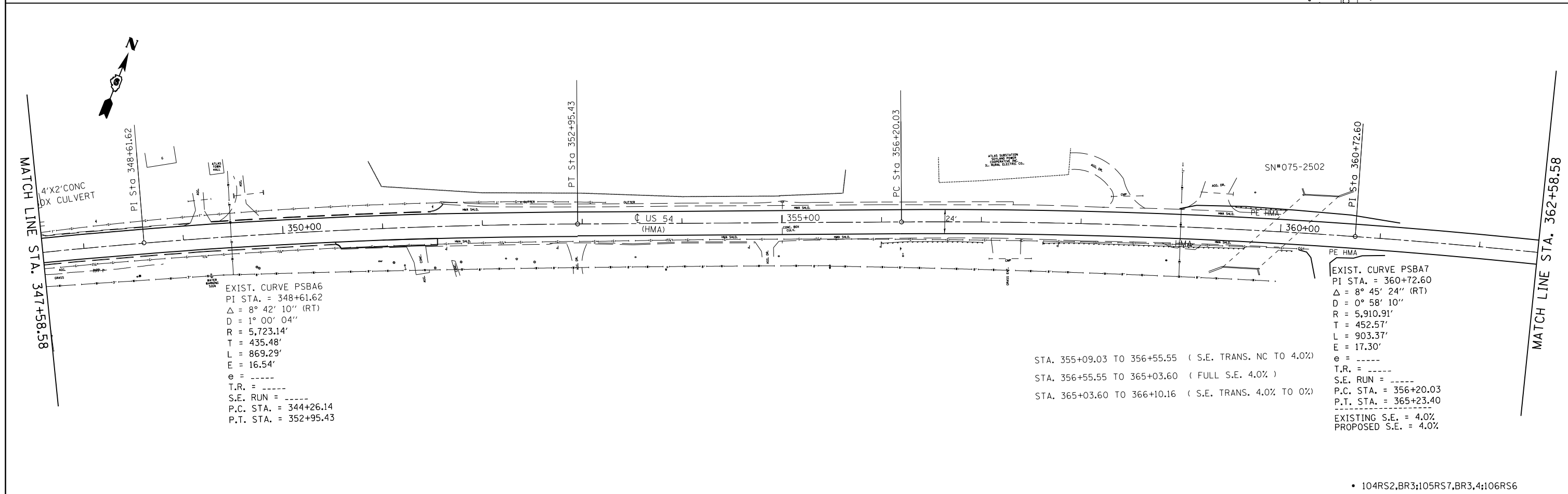
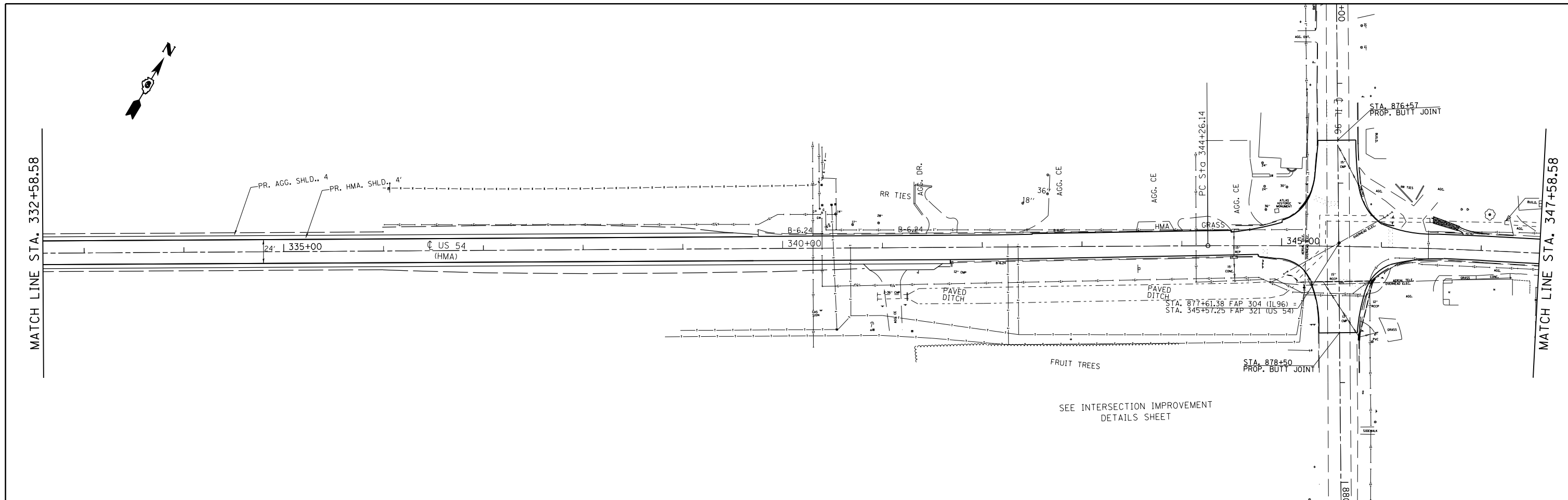
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = Oct-31-2011 03:51:37PM	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>PLAN SHEET</b>		
SCALE: 1"=100'	SHEET NO. 9 OF 17 SHEETS	STA. 281+00.00 TO STA. 332+85.58

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
321	•	PIKE	69	36
CONTRACT NO. 72E43				
ILLINOIS FED. AID PROJECT				





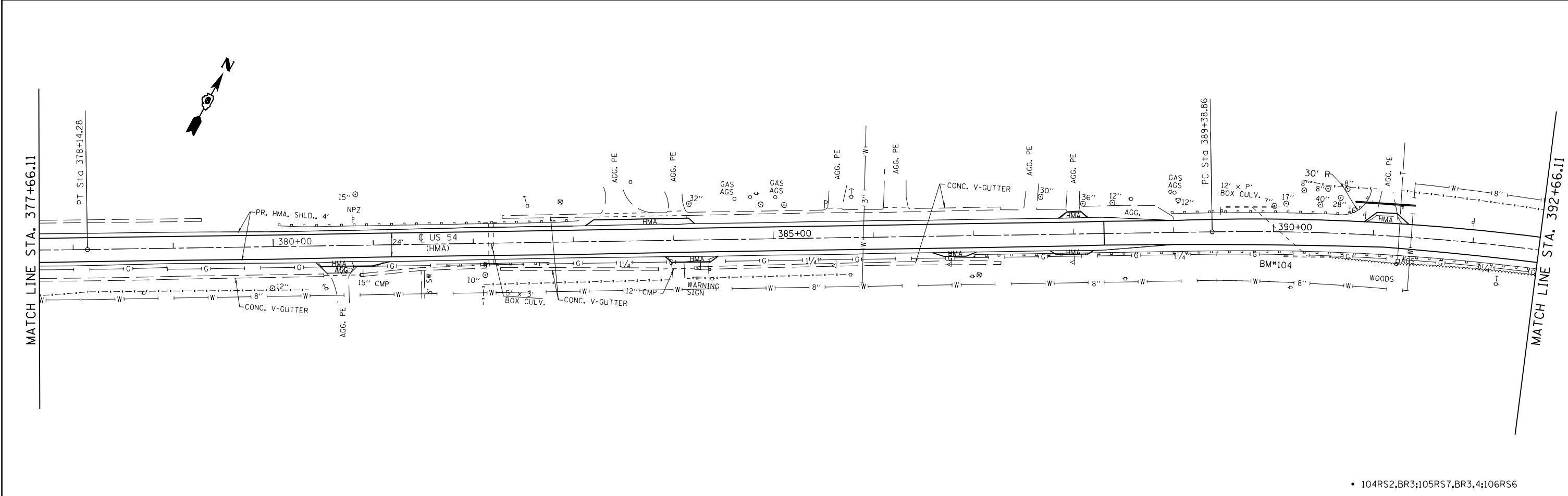
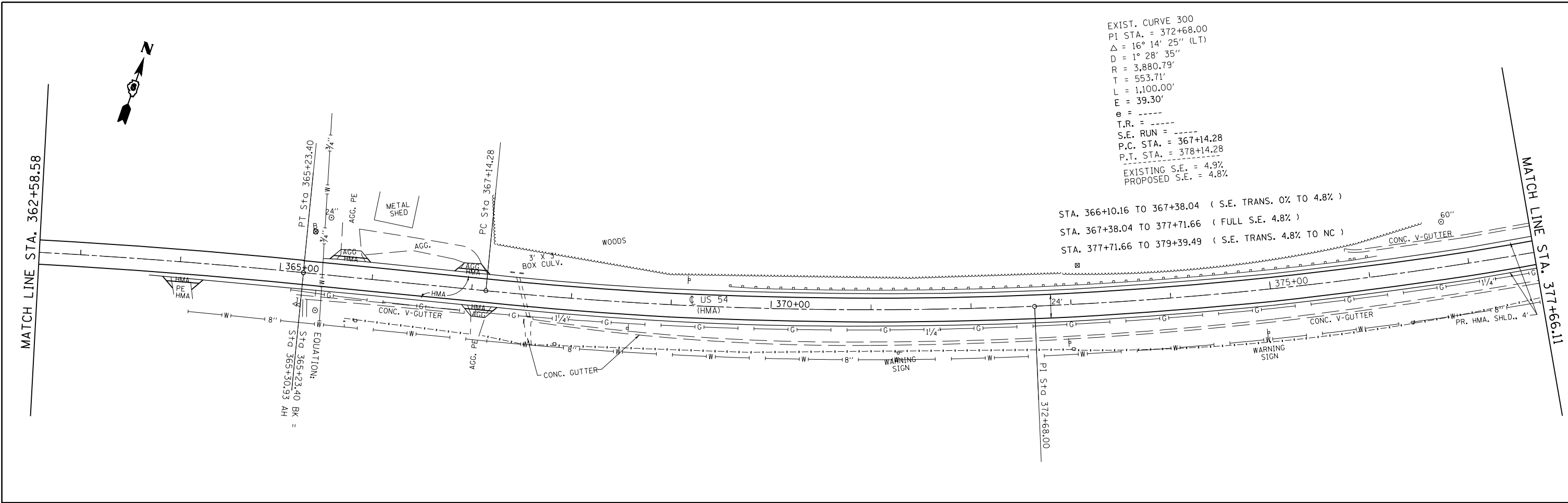
EXIST. CURVE PSBA6  
 PI STA. = 348+61.62  
 $\Delta = 8^{\circ} 42' 10''$  (RT)  
 $D = 1^{\circ} 00' 04''$   
 $R = 5,723.14'$   
 $T = 435.48'$   
 $L = 869.29'$   
 $E = 16.54'$   
 $e = \text{-----}$   
 $T.R. = \text{-----}$   
 $S.E. \text{ RUN} = \text{-----}$   
 $P.C. \text{ STA.} = 344+26.14$   
 $P.T. \text{ STA.} = 352+95.43$

STA. 355+09.03 TO 356+55.55 ( S.E. TRANS. NC TO 4.0% )  
 STA. 356+55.55 TO 365+03.60 ( FULL S.E. 4.0% )  
 STA. 365+03.60 TO 366+10.16 ( S.E. TRANS. 4.0% TO 0% )

EXIST. CURVE PSBA7  
 PI STA. = 360+72.60  
 $\Delta = 8^{\circ} 45' 24''$  (RT)  
 $D = 0^{\circ} 58' 10''$   
 $R = 5,910.91'$   
 $T = 452.57'$   
 $L = 903.37'$   
 $E = 17.30'$   
 $e = \text{-----}$   
 $T.R. = \text{-----}$   
 $S.E. \text{ RUN} = \text{-----}$   
 $P.C. \text{ STA.} = 356+20.03$   
 $P.T. \text{ STA.} = 365+23.40$   
 EXISTING S.E. = 4.0%  
 PROPOSED S.E. = 4.0%

• 104RS2,BR3;105RS7,BR3,4;106RS6

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PLAN SHEET</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwwork\pwwork\sparksgw\10265393\PLAN1.dgn		DRAWN -	REVISED -					321	.	PIKE	69	37
PLOT SCALE = 100.0000' / in.		CHECKED -	REVISED -		CONTRACT NO. 72E43							
PLOT DATE = Oct-31-2011 03:51:37PM		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							
				SCALE: 1"=100'			SHEET NO. 10 OF 17 SHEETS		STA. 332+58.58 TO STA. 362+58.58			



• 104RS2,BR3;105RS7,BR3,4;106RS6

FILE NAME = c:\pwwork\pwwork\sparks\10265393\PLAN.dgn	USER NAME = sparks	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PLAN SHEET</b>			F.A.P. R.T.E. = 321	SECTION =	COUNTY = PIKE	TOTAL SHEETS = 69	SHEET NO. = 38
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -	REVISED -		SCALE: 1"=100'	SHEET NO. 11 OF 17 SHEETS	STA. 362+58.58 TO STA. 392+66.11	CONTRACT NO. 72E43				
PLOT DATE = Oct-31-2011 03:51:38PM	DATE -	REVISED -	REVISED -		ILLINOIS FED. AID PROJECT							

EXIST. CURVE 302  
 PI STA. = 405+73.61  
 $\Delta = 46^\circ 23' 22''$  (LT)  
 D = 5° 25' 07"  
 R = 1,057.37'  
 T = 453.08'  
 L = 856.10'  
 E = 92.98'  
 e = -----  
 T.R. = -----  
 S.E. RUN = -----  
 P.C. STA. = 401+20.54  
 P.T. STA. = 409+76.64  
 -----  
 EXISTING S.E. = 7.3%  
 PROPOSED S.E. = 8.0%

EXIST. CURVE 301  
 PI STA. = 394+87.30  
 $\Delta = 26^\circ 52' 19''$  (RT)  
 D = 2° 29' 45"  
 R = 2,295.72'  
 T = 548.44'  
 L = 1,076.70'  
 E = 64.60'  
 e = -----  
 T.R. = -----  
 S.E. RUN = -----  
 P.C. STA. = 389+38.86  
 P.T. STA. = 400+15.56  
 -----  
 EXISTING S.E. = 2.7%  
 PROPOSED S.E. = 5.2%

STA. 388+06.55 TO 389+85.04 ( S.E. TRANS. NC TO 5.2% )  
 STA. 389+85.04 TO 399+21.03 ( FULL S.E. 5.2% )  
 STA. 399+21.03 TO 400+59.56 ( S.E. TRANS. 5.2% TO 0% )

STA. 400+59.56 TO 402+51.56 ( S.E. TRANS. 0% TO 8.0% )  
 STA. 402+51.56 TO 408+58.45 ( FULL S.E. 8.0% )  
 STA. 408+58.45 TO 410+50.45 ( S.E. TRANS. 8.0% TO 0% )

EXIST. CURVE 303  
 PI STA. = 415+72.17  
 $\Delta = 21^\circ 27' 53''$  (RT)  
 D = 2° 18' 29"  
 R = 2,482.46'  
 T = 470.51'  
 L = 930.00'  
 E = 44.20'  
 e = -----  
 T.R. = -----  
 S.E. RUN = -----  
 P.C. STA. = 411+01.66  
 P.T. STA. = 420+31.65  
 -----  
 EXISTING S.E. = 3.2%  
 PROPOSED S.E. = 5.0%

STA. 410+50.45 TO 411+83.65 ( S.E. TRANS. 0% TO 5.0% )  
 STA. 411+83.65 TO 419+87.25 ( FULL S.E. 5.0% )  
 STA. 419+87.25 TO 421+60.41 ( S.E. TRANS. 5.0% TO NC )

• 104RS2,BR3;105RS7,BR3,4;106RS6

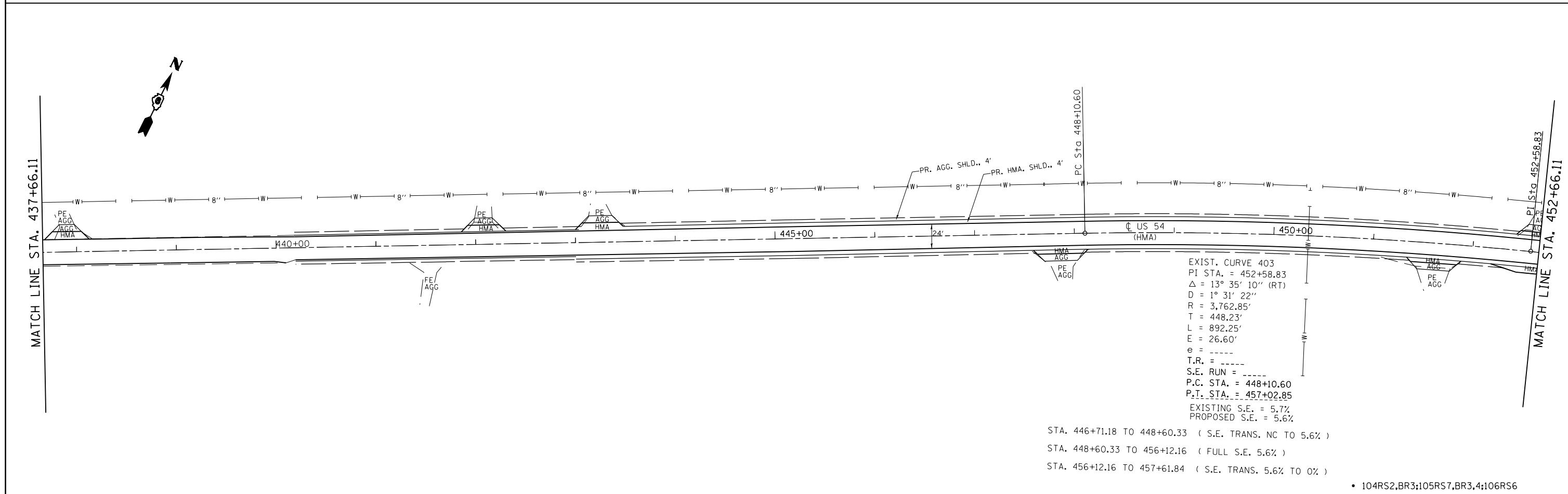
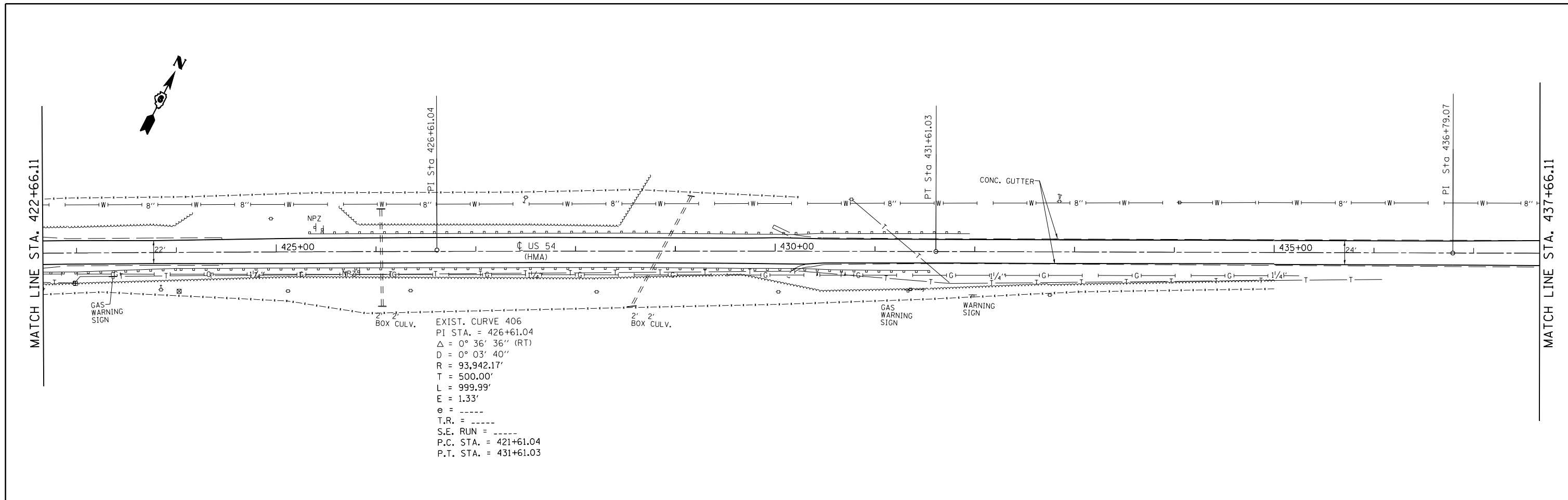
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	PLOT DATE = Oct-31-2011 03:51:38PM	DATE -	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

PLAN SHEET

SCALE: 1"=100' SHEET NO. 12 OF 17 SHEETS STA. 392+66.11 TO STA. 422+66.11

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
321	•	PIKE	69	39
CONTRACT NO. 72E43				
ILLINOIS FED. AID PROJECT				

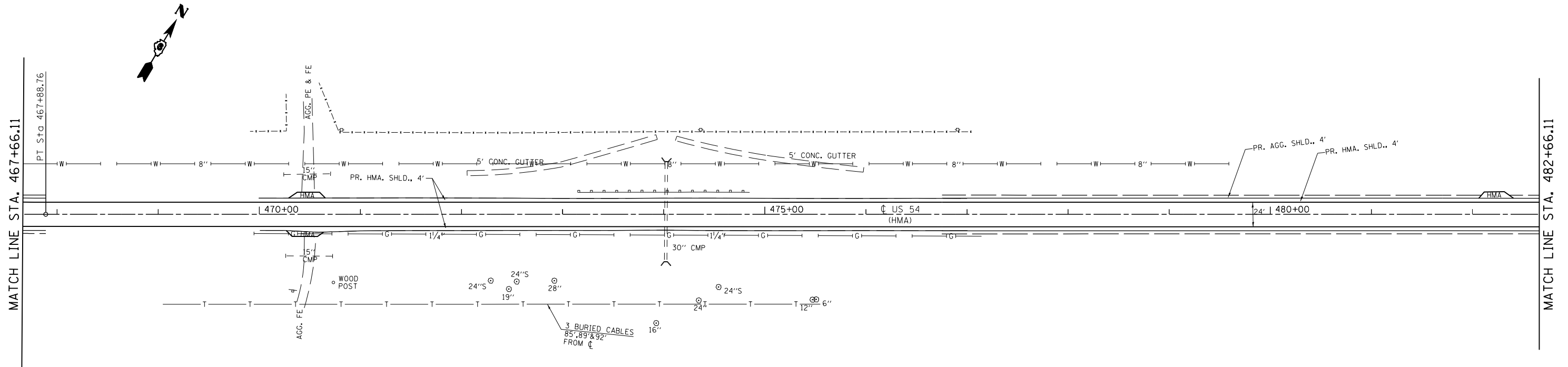
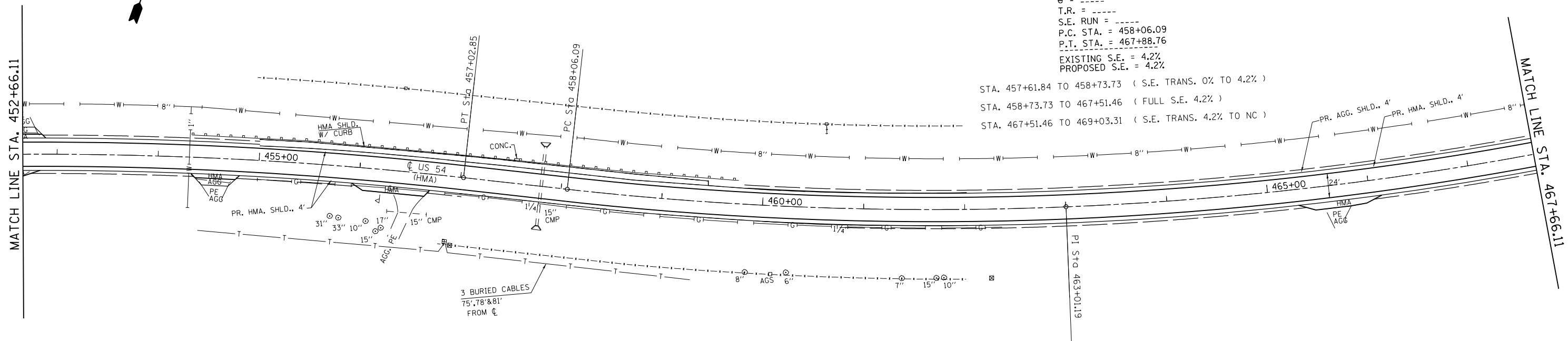


FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PLAN SHEET</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwwork\pwwork\sparksgw\10265393\PLAN.dgn		DRAWN -	REVISED -		321	.	PIKE	69	40			
PLOT SCALE = 100.0000' / in.		CHECKED -	REVISED -		<b>CONTRACT NO. 72E43</b>							
PLOT DATE = Oct-31-2011 03:51:39PM		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							
				SCALE: 1"=100'	SHEET NO. 13 OF 17 SHEETS	STA. 422+66.11 TO STA. 452+66.11						

• 104RS2,BR3;105RS7,BR3,4;106RS6

EXIST. CURVE 402  
 PI STA. = 463+01.19  
 $\Delta = 17^\circ 17' 23''$  (LT)  
 $D = 1^\circ 45' 34''$   
 $R = 3,256.45'$   
 $T = 495.10'$   
 $L = 982.67'$   
 $E = 37.42'$   
 $e = \text{-----}$   
 $T.R. = \text{-----}$   
 $S.E. RUN = \text{-----}$   
 P.C. STA. = 458+06.09  
 P.T. STA. = 467+88.76  
 EXISTING S.E. = 4.2%  
 PROPOSED S.E. = 4.2%

STA. 457+61.84 TO 458+73.73 ( S.E. TRANS. 0% TO 4.2% )  
 STA. 458+73.73 TO 467+51.46 ( FULL S.E. 4.2% )  
 STA. 467+51.46 TO 469+03.31 ( S.E. TRANS. 4.2% TO NC )



• 104RS2,BR3;105RS7,BR3,4;106RS6

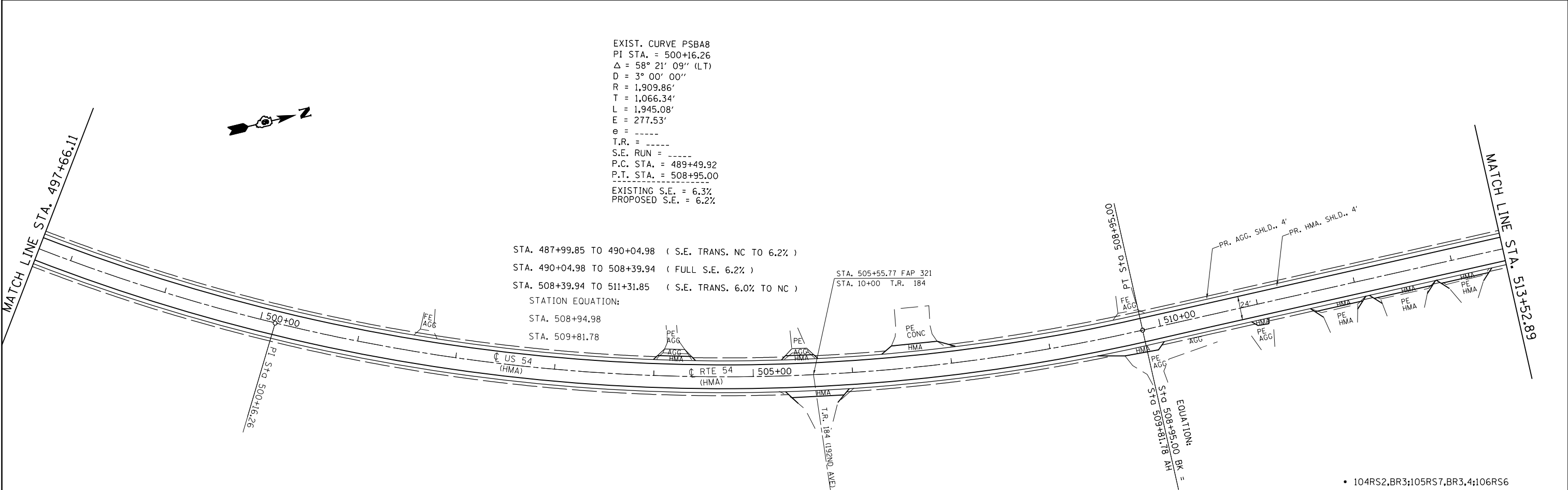
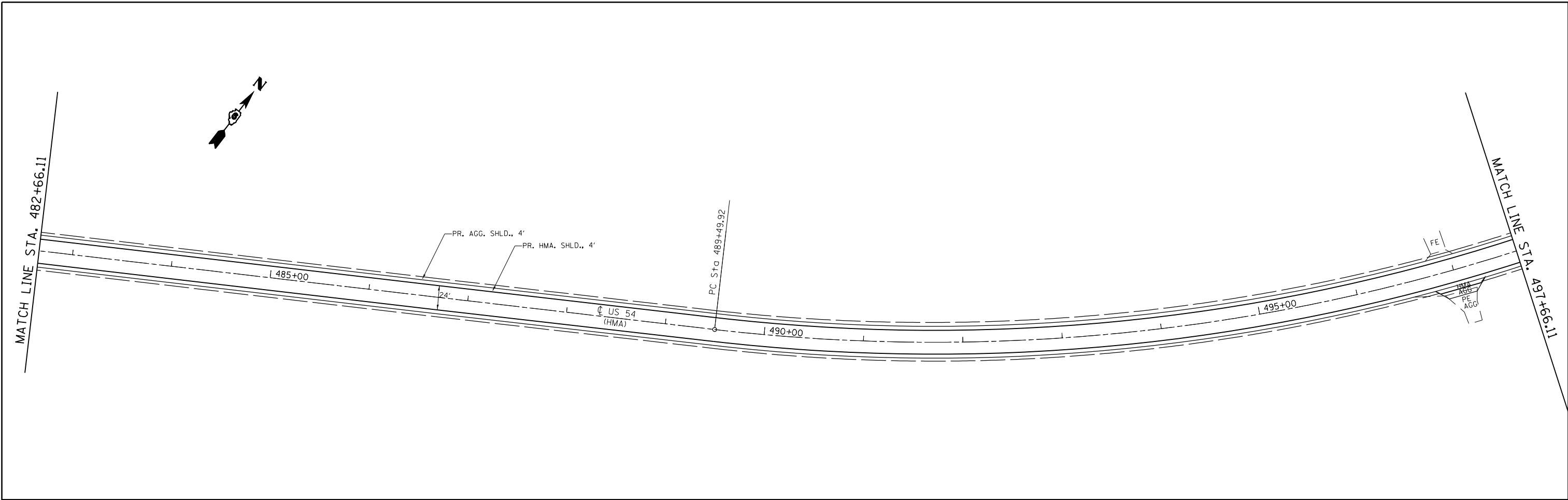
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		CHECKED -	REVISED -
		DATE -	REVISED -
	PLOT DATE = Oct-31-2011 03:51:39PM		

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**PLAN SHEET**

SCALE: 1"=100'    SHEET NO. 14 OF 17 SHEETS    STA. 452+66.11 TO STA. 482+66.11

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
321	•	PIKE	69	41
CONTRACT NO. 72E43				
ILLINOIS FED. AID PROJECT				



EXIST. CURVE PSBA8  
 PI STA. = 500+16.26  
 $\Delta$  = 58° 21' 09" (LT)  
 D = 3° 00' 00"  
 R = 1,909.86'  
 T = 1,066.34'  
 L = 1,945.08'  
 E = 277.53'  
 e = -----  
 T.R. = -----  
 S.E. RUN = -----  
 P.C. STA. = 489+49.92  
 P.T. STA. = 508+95.00  
 -----  
 EXISTING S.E. = 6.3%  
 PROPOSED S.E. = 6.2%

STA. 487+99.85 TO 490+04.98 ( S.E. TRANS. NC TO 6.2% )  
 STA. 490+04.98 TO 508+39.94 ( FULL S.E. 6.2% )  
 STA. 508+39.94 TO 511+31.85 ( S.E. TRANS. 6.0% TO NC )  
 STATION EQUATION:  
 STA. 508+94.98  
 STA. 509+81.78

STA. 505+55.77 FAP 321  
 STA. 10+00 T.R. 184

EQUATION:  
 STA. 508+95.00 BK =  
 STA. 509+81.78 AH

• 104RS2,BR3;105RS7,BR3,4;106RS6

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PLOT DATE = Oct-31-2011 03:51:40PM	DATE -	REVISED -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**PLAN SHEET**

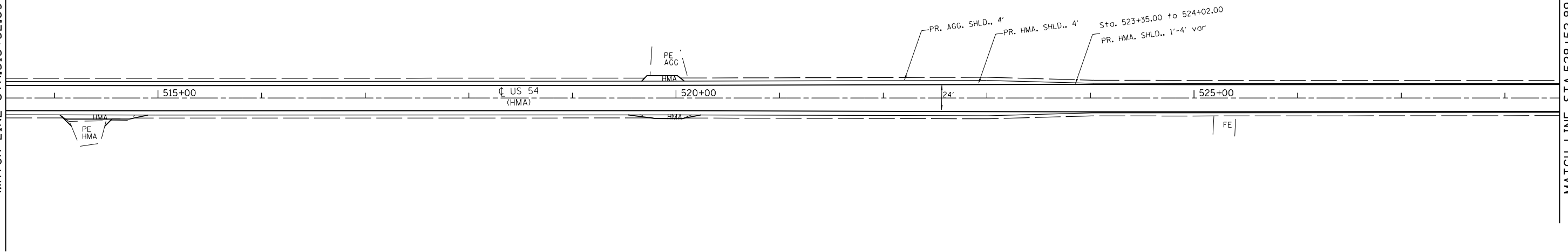
SCALE: 1"=100'    SHEET NO. 15 OF 17 SHEETS    STA. 482+66.11 TO STA. 513+52.89

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
321	•	PIKE	69	42
CONTRACT NO. 72E43				
ILLINOIS FED. AID PROJECT				



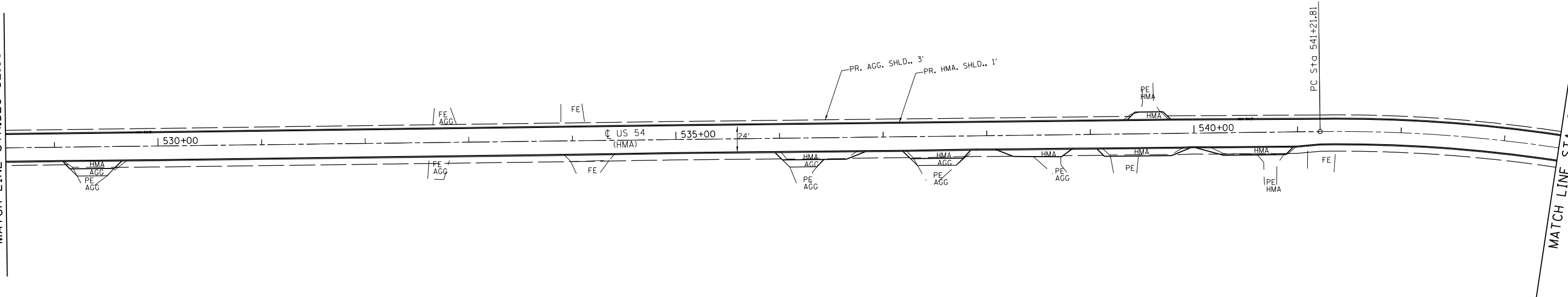
MATCH LINE STA. 513+52.89

MATCH LINE STA. 528+52.89



MATCH LINE STA. 528+52.89

MATCH LINE STA. 543+52.89



• 104RS2,BR3;105RS7,BR3,4;106RS6

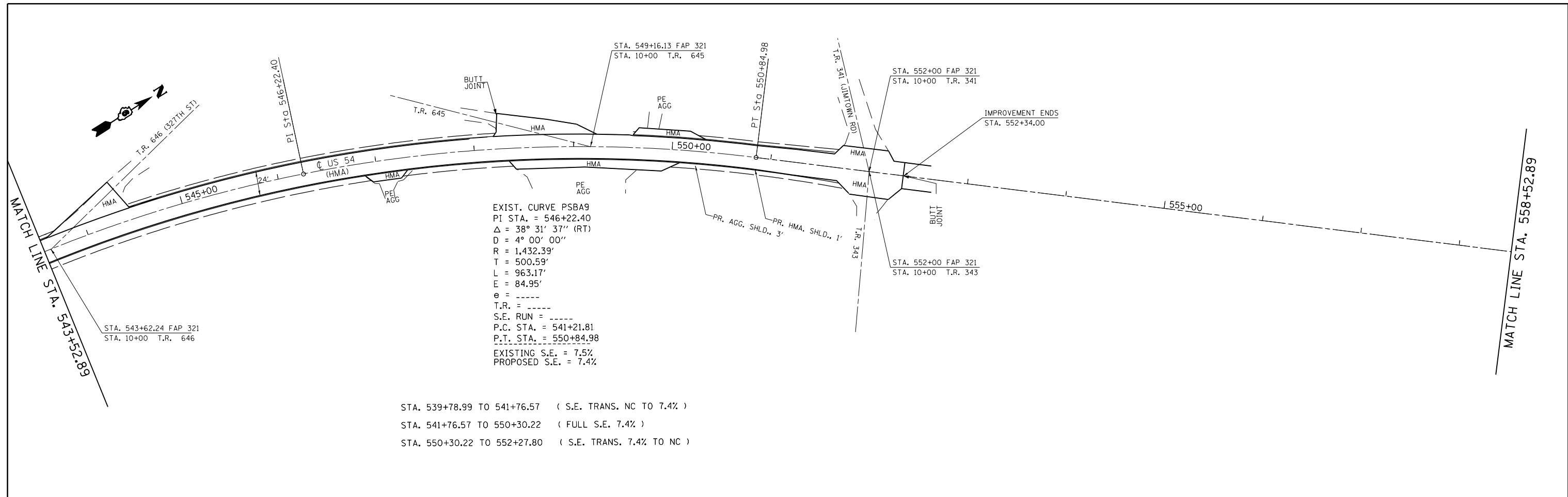
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	PLOT DATE = Oct-31-2011 03:51:40PM	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PLAN SHEET**

SCALE: 1"=100'      SHEET NO. 16 OF 17 SHEETS      STA. 513+52.89 TO STA. 543+52.89

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
321	•	PIKE	69	43
CONTRACT NO. 72E43				
ILLINOIS FED. AID PROJECT				



• 104RS2,BR3;105RS7,BR3,4;106RS6

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -
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PLOT DATE = Oct-31-2011 03:51:41PM		DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PLAN SHEET**

SCALE: 1"=100' SHEET NO. 17 OF 17 SHEETS STA. 543+52.89 TO STA. 558+52.89

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
321	•	PIKE	69	44
CONTRACT NO. 72E43				
ILLINOIS FED. AID PROJECT				



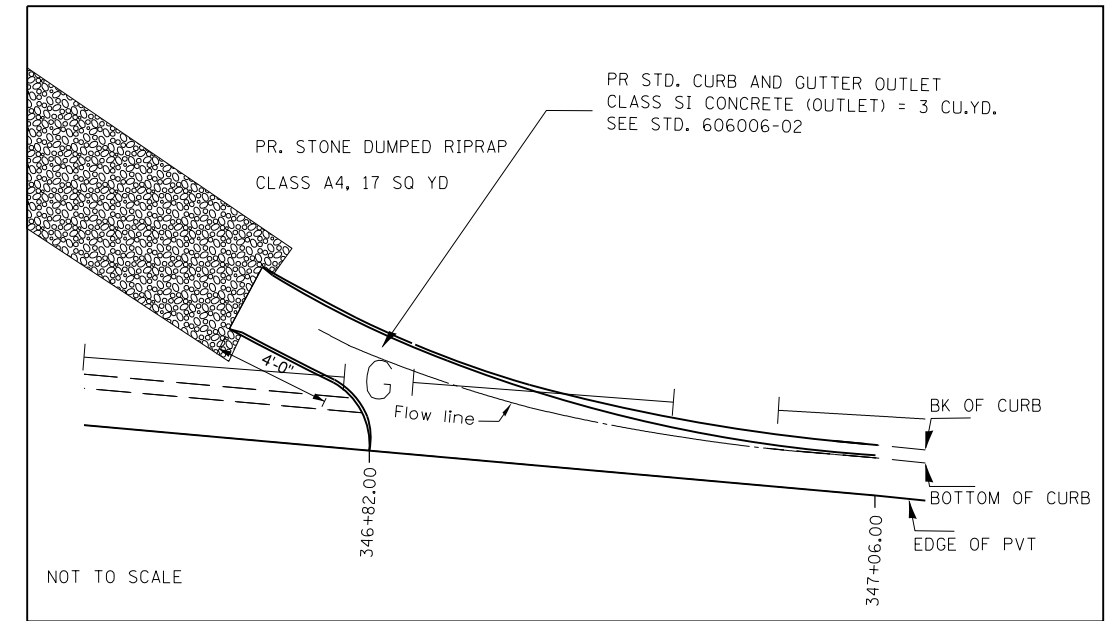
Ⓐ STA. 877+99, 27' RT ( IL 96 )  
 INLET, TYPE A, TYPE 15 FRAME AND LID, 1 EACH  
 TOP GRATE ELEV = 484.20  
 FLOWLINE NORTHEAST = 480.75'

Ⓣ STORM SEWERS, CLASS A, TYPE 1 15", 57'  
 USFL = 481.31' DSFL = 481.06'  
 SLOPE = 0.44%

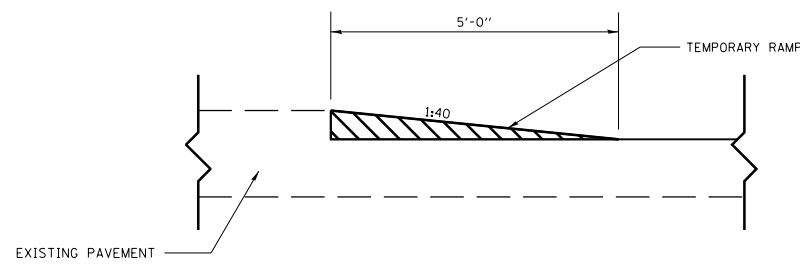
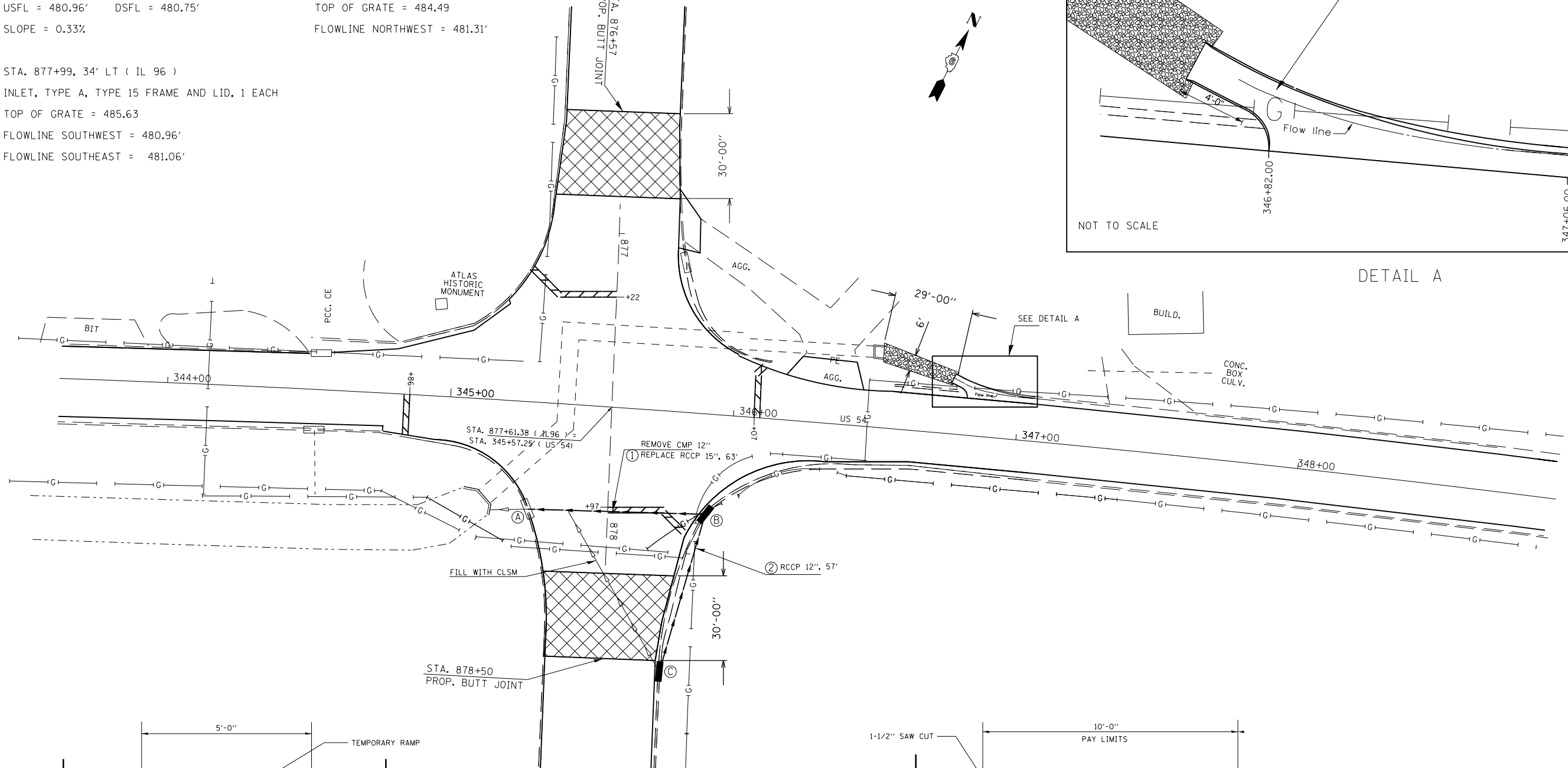
Ⓛ STORM SEWERS, CLASS A, TYPE 1 15", 63'  
 USFL = 480.96' DSFL = 480.75'  
 SLOPE = 0.33%

Ⓒ STA. 878+55, 21' LT ( IL 96 )  
 INLET, TYPE A, TYPE 15 FRAME AND LID, 1 EACH  
 TOP OF GRATE = 484.49  
 FLOWLINE NORTHWEST = 481.31'

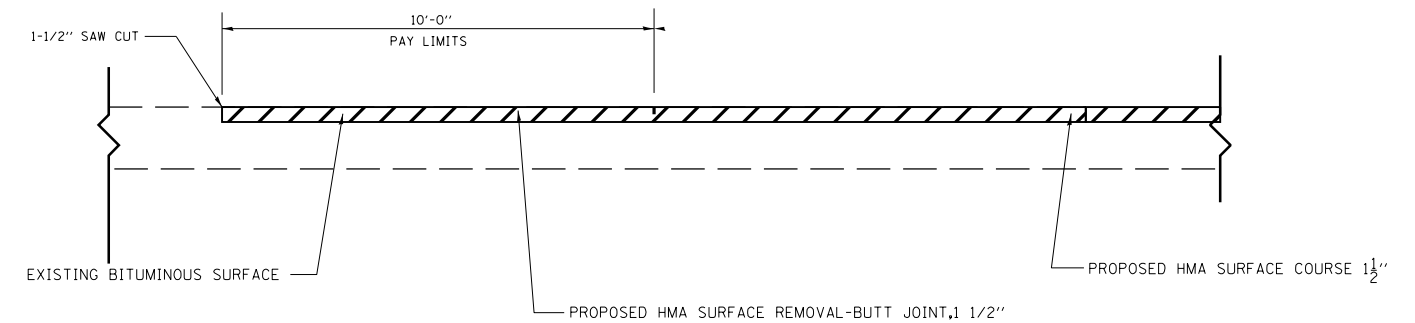
Ⓑ STA. 877+99, 34' LT ( IL 96 )  
 INLET, TYPE A, TYPE 15 FRAME AND LID, 1 EACH  
 TOP OF GRATE = 485.63  
 FLOWLINE SOUTHWEST = 480.96'  
 FLOWLINE SOUTHEAST = 481.06'



DETAIL A



TEMPORARY RAMP DETAILS



BUTT JOINT DETAILS

• 104RS2, BR3; 105RS7, BR3, 4; 106RS6

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PLOT DATE = Oct-31-2011 03:51:48PM		DATE -	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

US 54 AT IL 96  
 AND MISC. DETAILS

SCALE: SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
321	•	PIKE	69	45
CONTRACT NO. 72E43				
ILLINOIS FED. AID PROJECT				



**CURVE NO: PSBA7**

<i>Entrance Curve</i>	
<i>Point</i>	<i>Sta.</i>
A	Sta. 355+09.03
B	Sta. 355+48.99
C	Sta. 355+88.95
D	Sta. 356+20.03
E	Sta. 356+55.55

<i>Exit Curve</i>	
<i>Point</i>	<i>Sta.</i>
1	Sta. 365+03.60
2	Sta. 365+38.77
3	Sta. 366+10.16

**CURVE NO: PSBA7**

<i>Entrance Curve</i>	
<i>Point</i>	<i>Sta.</i>
3	Sta. 366+10.16
4	Sta. 366+95.84
5	Sta. 367+38.04

<i>Exit Curve</i>	
<i>Point</i>	<i>Sta.</i>
E	Sta. 377+71.66
D	Sta. 378+14.28
C	Sta. 378+59.57
B	Sta. 378+99.53
A	Sta. 379+39.49

**CURVE NO: 301**

<i>Entrance Curve</i>	
<i>Point</i>	<i>Sta.</i>
A	Sta. 388+06.55
B	Sta. 388+46.51
C	Sta. 388+86.47
D	Sta. 389+38.86
E	Sta. 389+85.04

<i>Exit Curve</i>	
<i>Point</i>	<i>Sta.</i>
1	Sta. 399+21.03
2	Sta. 399+66.74
3	Sta. 400+59.56

**CURVE NO: 302**

<i>Entrance Curve</i>	
<i>Point</i>	<i>Sta.</i>
3	Sta. 400+59.56
4	Sta. 401+88.20
5	Sta. 402+51.56

<i>Exit Curve</i>	
<i>Point</i>	<i>Sta.</i>
1	Sta. 408+58.45
2	Sta. 409+21.81
3	Sta. 410+50.45

**CURVE NO: 303**

<i>Entrance Curve</i>	
<i>Point</i>	<i>Sta.</i>
3	Sta. 410+50.45
4	Sta. 411+39.70
5	Sta. 411+83.65

<i>Exit Curve</i>	
<i>Point</i>	<i>Sta.</i>
E	Sta. 419+87.25
D	Sta. 420+31.65
C	Sta. 420+80.49
B	Sta. 421+20.45
A	Sta. 421+60.41

**CURVE NO: 403**

<i>Entrance Curve</i>	
<i>Point</i>	<i>Sta.</i>
A	Sta. 446+71.18
B	Sta. 447+11.14
C	Sta. 447+51.10
D	Sta. 448+10.60
E	Sta. 448+60.33

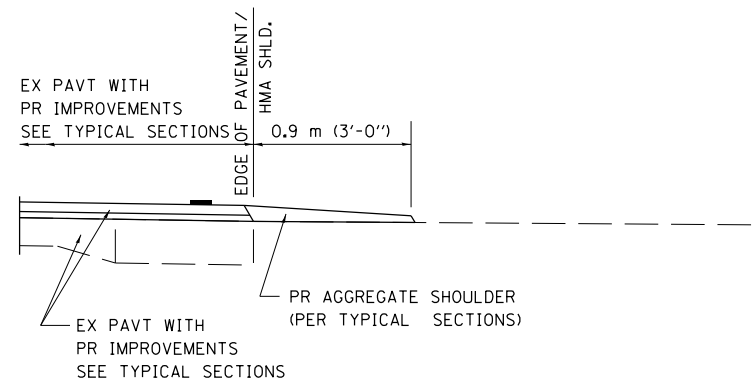
<i>Exit Curve</i>	
<i>Point</i>	<i>Sta.</i>
1	Sta. 456+12.66
2	Sta. 456+61.89
3	Sta. 457+61.84

**CURVE NO: 402**

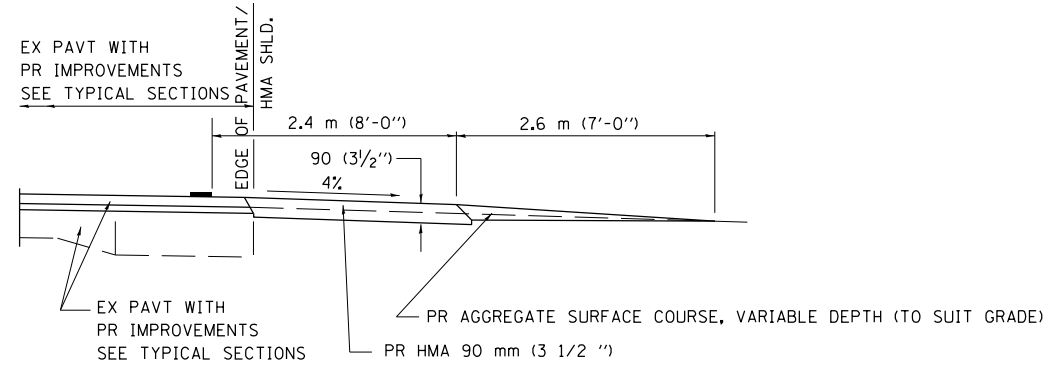
<i>Entrance Curve</i>	
<i>Point</i>	<i>Sta.</i>
3	Sta. 457+61.84
4	Sta. 458+36.81
5	Sta. 458+73.73

<i>Exit Curve</i>	
<i>Point</i>	<i>Sta.</i>
E	Sta. 467+51.46
D	Sta. 467+88.76
C	Sta. 468+23.39
B	Sta. 468+63.35
A	Sta. 469+03.31

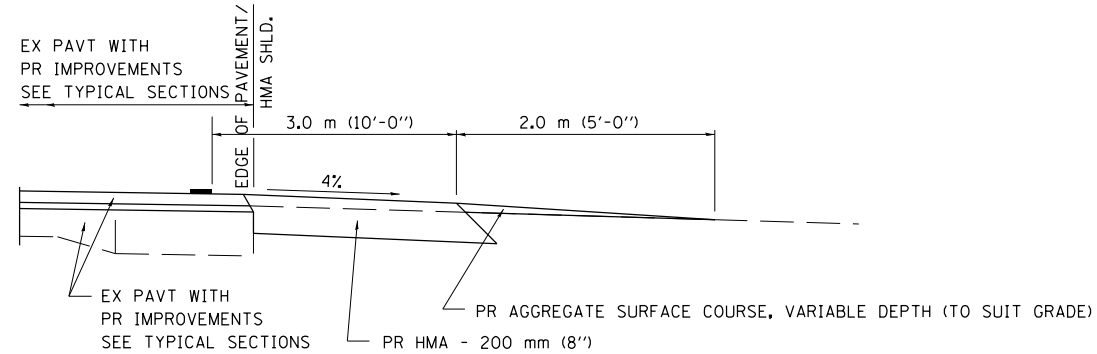
• 104RS2, BR3; 105RS7, BR3, 4; 106RS6



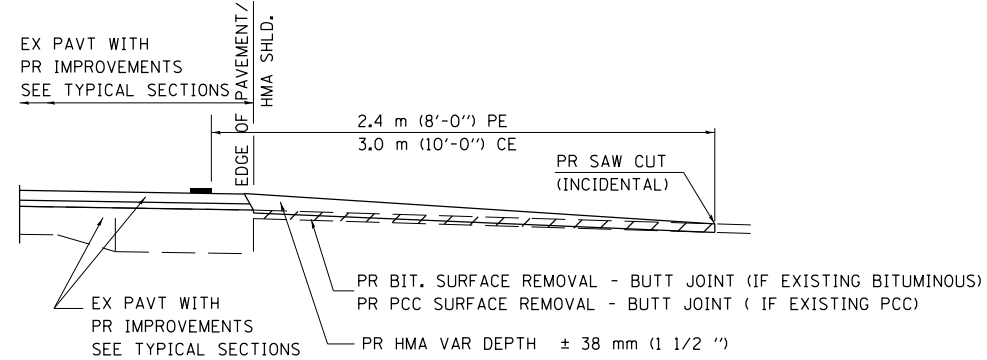
**SECTION A-A FOR EX EARTH/ AGGREGATE FE**



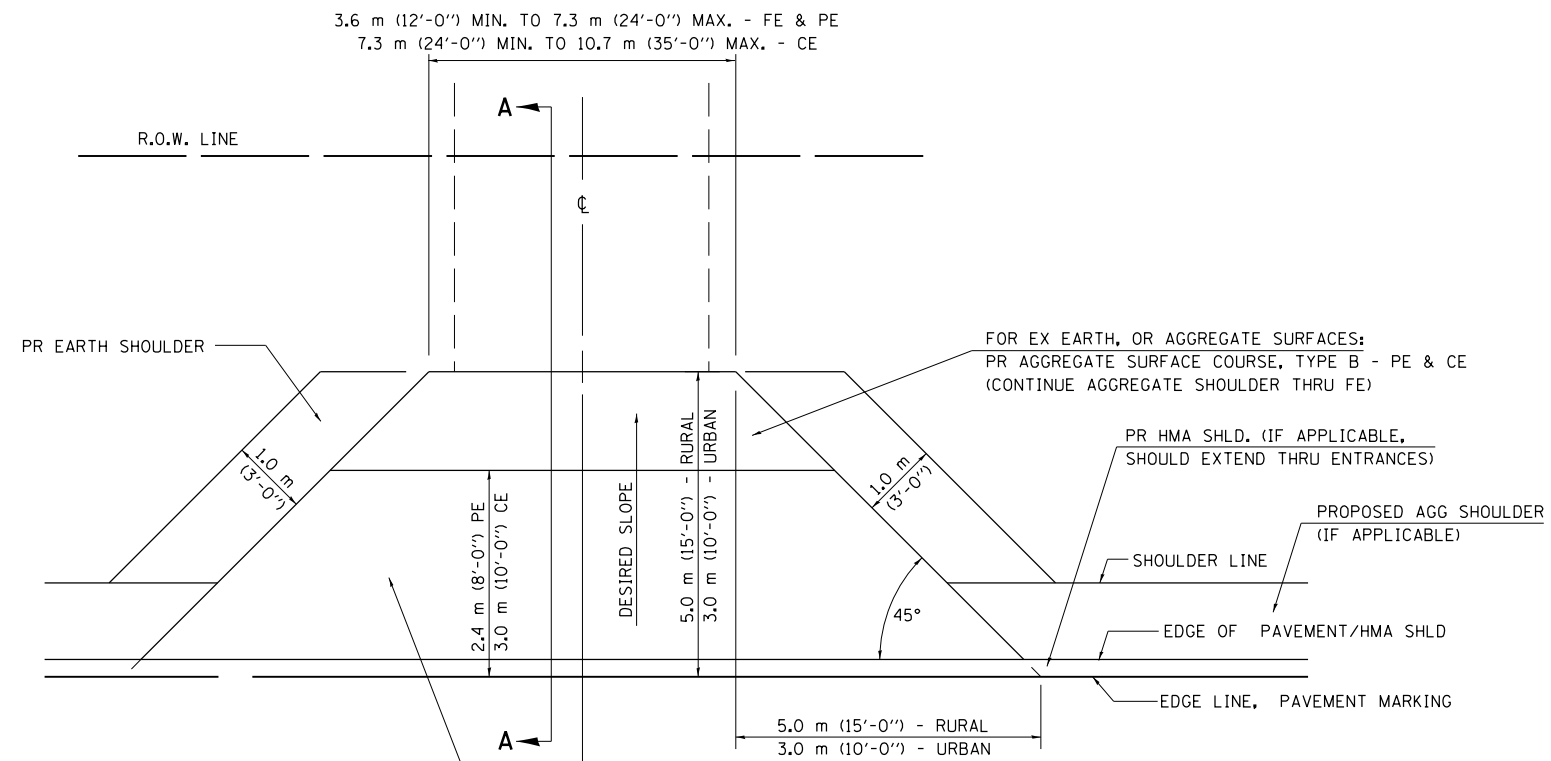
**SECTION A-A FOR EX EARTH/AGGREGATE PE**



**SECTION A-A FOR EX EARTH/AGGREGATE CE & SIDE ROAD**



**SECTION A-A FOR EX BITUMINOUS/ PC CONCRETE PE, CE & SIDE ROAD**



FOR EX EARTH OR AGGREGATE SURFACES:  
 PR BIT SURFACE REMOVAL (IF APPLICABLE)  
 PR AGGREGATE SHOULDER THRU - FE  
 PR BITUMINOUS CONCRETE 90 mm (3 1/2") - PE  
 PR BITUMINOUS CONCRETE 200mm (8") - CE

FOR EX BITUMINOUS CONCRETE SURFACES:  
 PR BITUMINOUS SURFACE REMOVAL-BUTT JOINT

FOR EX PCC SURFACES:  
 PR PCC SURFACE REMOVAL-BUTT JOINT

**GENERAL NOTES:**

THE RESIDENT ENGINEER WILL DETERMINE THE EXACT TYPE OF IMPROVEMENT TO BE COMPLETED FOR ALL ENTRANCES, SIDEROADS AND MAILBOX TURNOUTS ON THIS PROJECT.

THE PLAN DETAILS AND SCHEDULES SHOULD BE USED AS A GUIDE FOR THE ENGINEER TO IMPLEMENT THE FINAL DESIGN. THE ENGINEER MAY DECIDE TO SALVAGE PORTIONS OF THE EXISTING ENTRANCE PAVEMENT STRUCTURE; THEREFORE, REDUCING PAY ITEM QUANTITIES. NO ADDITIONAL PAYMENT WILL BE ALLOWED FOR THIS REDUCTION IN QUANTITIES.

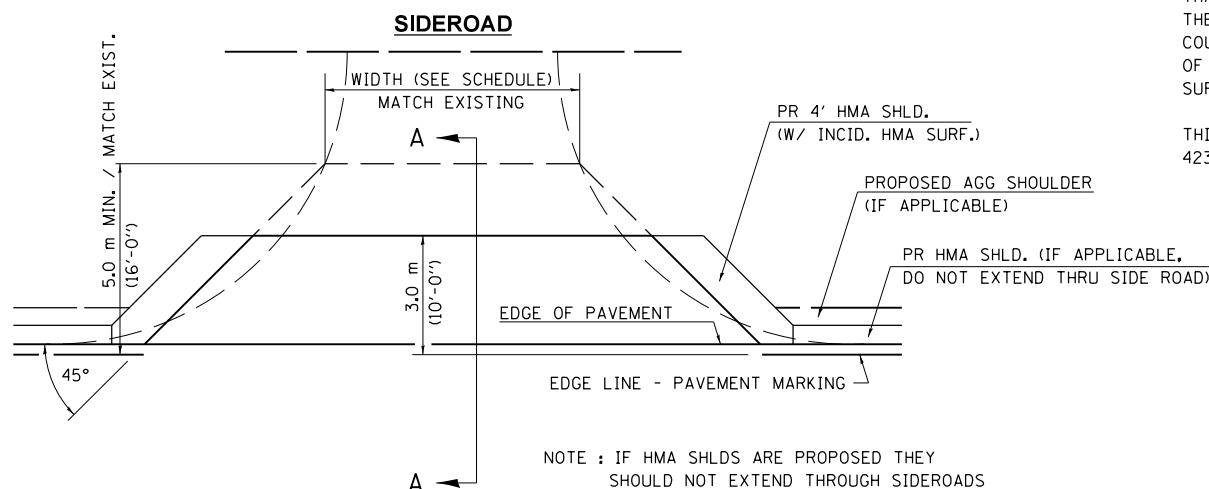
ANY WORK THE ENGINEER REQUIRES WHICH IS NOT COVERED BY A PAY ITEM CONTAINED IN THE PLANS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

BITUMINOUS CONCRETE REQUIRED TO CONSTRUCT THE ENTRANCES SHALL BE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 406 AND 408 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER.

WHEN THE BITUMINOUS CONCRETE PROPOSED FOR THE IMPROVEMENT IS THICKER THAN 75 mm (3 INCHES) AND REQUIRE PLACEMENT IN MORE THAN ONE LIFT. THE BOTTOM LIFT(S) SHALL MEET THE REQUIREMENTS OF BITUMINOUS BASE COURSE IN SECTION 406 OF THE STANDARD SPECIFICATIONS AND THE TOP LIFT OF 50 mm (2 INCHES) SHALL MEET THE REQUIREMENTS OF BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE.

THIS WORK WILL BE PAID FOR IN ACCORDANCE WITH SECTIONS 351, 358, 408, 423 AND 440 OF THE STANDARD SPECIFICATIONS.

ALL DIMENSIONS ARE IN MILLIMETERS ( INCHES ) UNLESS OTHERWISE SHOWN.



NOTE : IF HMA SHLDS ARE PROPOSED THEY SHOULD NOT EXTEND THROUGH SIDEROADS

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	PLOT DATE = Oct-31-2011 03:52:03PM	DATE - FEBRUARY 23, 1999	REVISED -

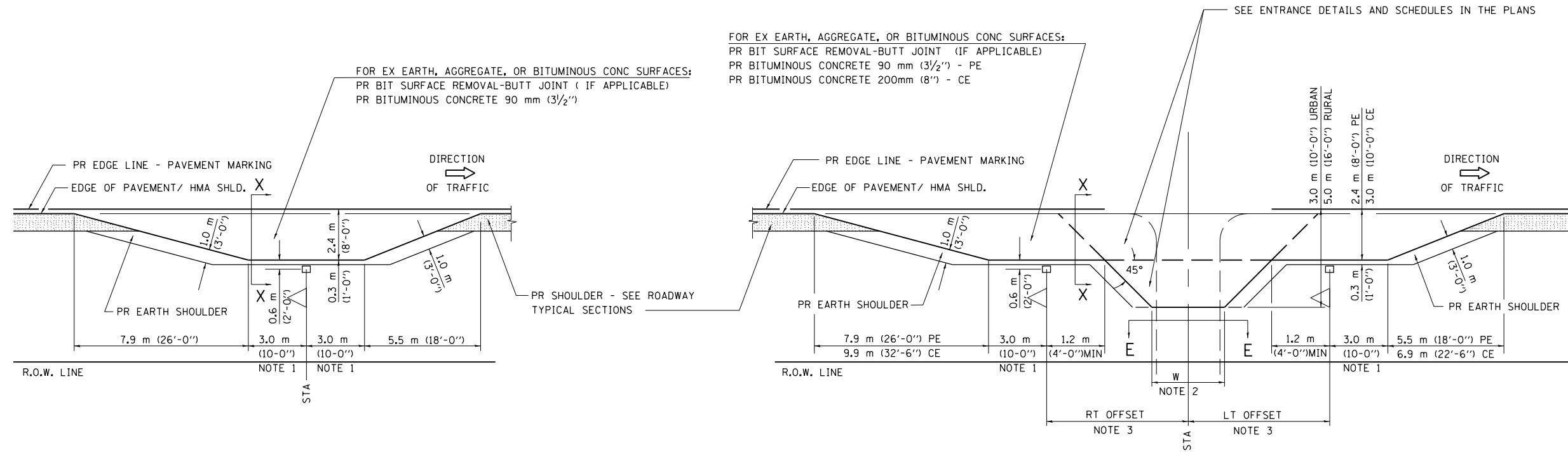
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**DIST.6 DETAILS FOR RURAL/URBAN ENT., MAILBOX  
 TURNOUT & SIDEROADS W/O CONC. GUTTER (3P-PROJ.)**

SCALE: SHEET NO. 1 OF 2 SHEETS STA. TO STA.

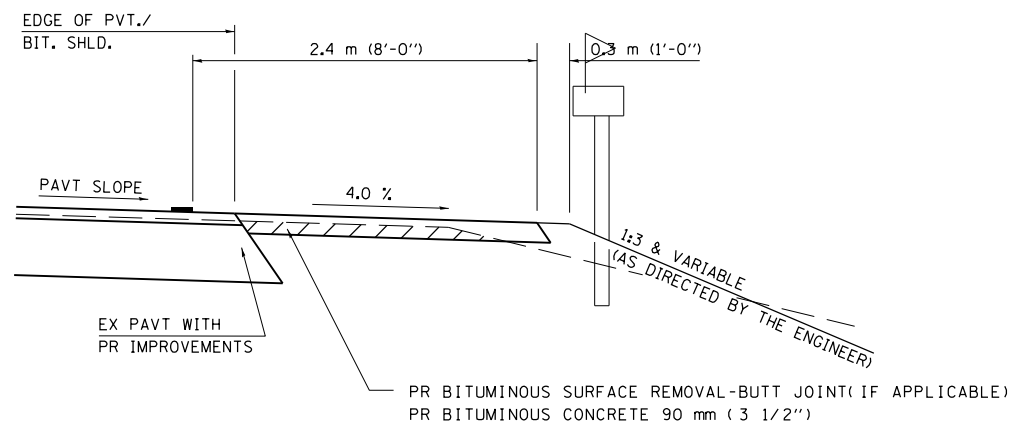
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
321	104RS-2	PIKE	69	48
CONTRACT NO. 72E43				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

## DETAILS OF MAILBOX TURNOUTS



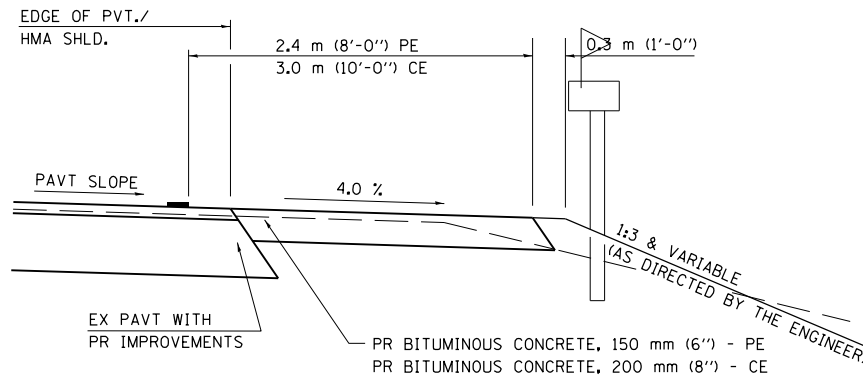
**PLAN - MAILBOX TURNOUTS**

**PLAN - COMBINED MAILBOX TURNOUT WITH TRAILING OR LEADING ENTRANCE**



**SECTION X-X THRU MAILBOX TURNOUT  
ALSO APPLIES TO MAILBOX TURNOUTS COMBINED WITH  
EX EARTH, AGGREGATE, OR BITUMINOUS PE & FE**

( DETAIL APPLIES WHEN M.B. TURNOUT DOES NOT EXIST.  
IF EXISTING, TREAT SAME AS ENTRANCE. )



**SECTION X-X THRU MAILBOX TURNOUT  
COMBINED WITH EX BITUMINOUS CONC & PC CONC PE & CE**

( DETAIL APPLIES WHEN M.B. TURNOUT DOES NOT EXIST.  
IF EXISTING, TREAT SAME AS ENTRANCE. )

- NOTE 1 IF MORE THAN ONE MAILBOX IS PRESENT, DIMENSION FROM CENTER OF END MAILBOX.
- NOTE 2 FOR ENTRANCE LAYOUT DIMENSIONS AND SECTIONS A-A & E-E REFER TO THE SCHEDULES IN THE PLANS.
- NOTE 3 BOTH LT OR RT OFFSETS FOR MAILBOX SHOWN USE OFFSET DIMENSION PER SCHEDULE AND REFER TO LAYOUT SHOWN ON THE PLAN.

ALL DIMENSIONS ARE IN MILLIMETERS ( INCHES )  
UNLESS OTHERWISE SHOWN.

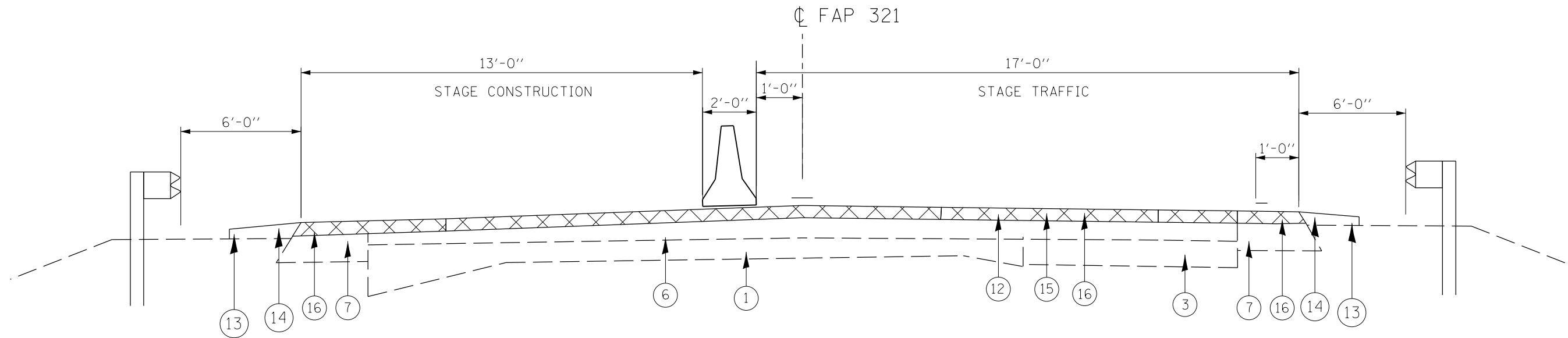
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	PLOT DATE = Oct-31-2011 03:52:03PM	DATE - FEBRUARY 23, 1999	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DIST. 6 DETAILS FOR RURAL/URBAN ENT., MAILBOX  
TURNOUT & SIDEROADS W/O CONC. GUTTER (3P-PROJ.)**

SCALE: SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
321	104RS-2	PIKE	69	49
CONTRACT NO. 72E43				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



STAGE CONSTRUCTION DETAILS - ROADWAY SECTION

EXISTING LEGEND

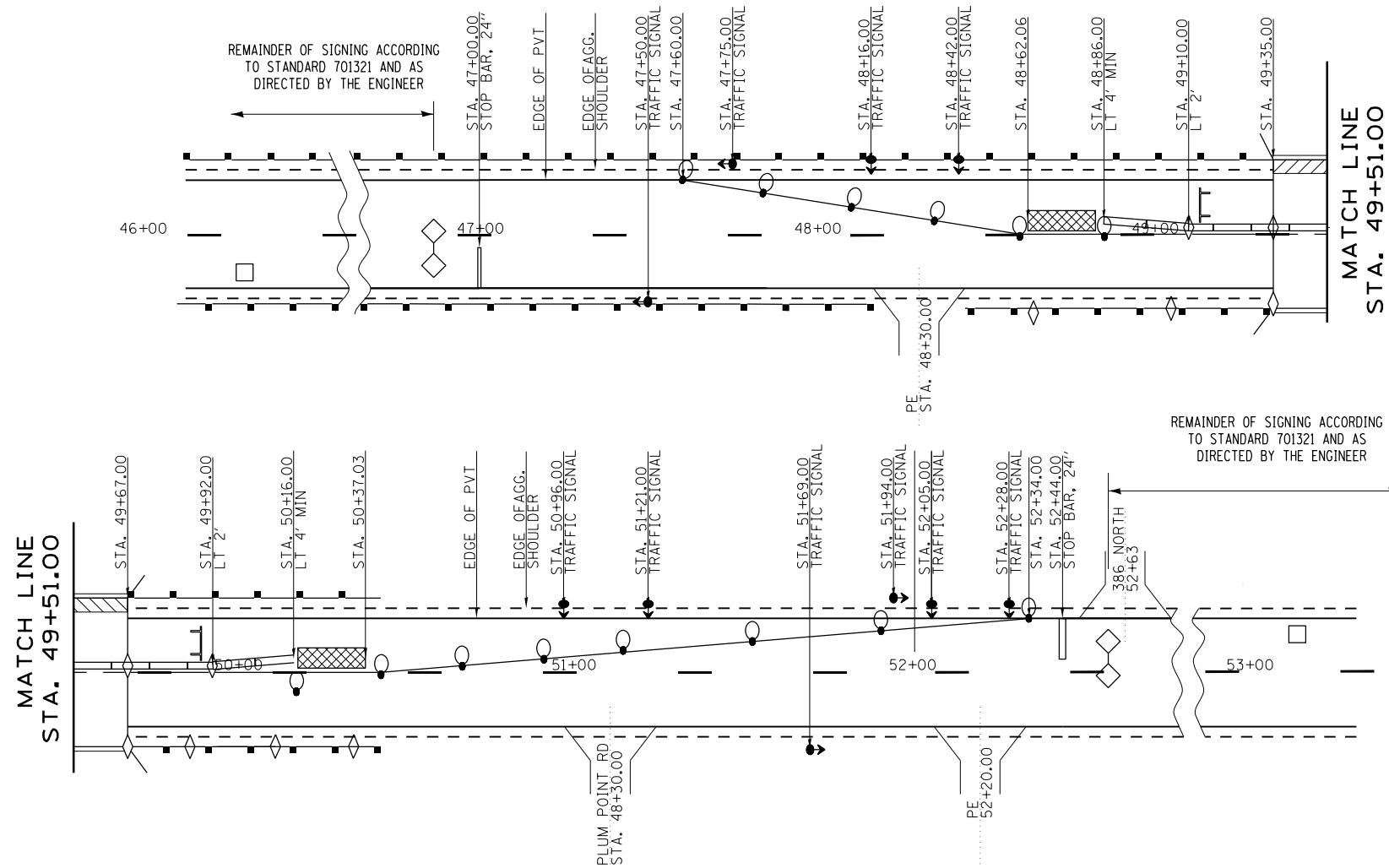
1. EXISTING PCC PAVEMENT
2. EXISTING PCC COURSE 1'
3. EXISTING 8" P.C.C. BASE COURSE WIDENING
4. EXISTING 3/4 " SMOOTH BAR
5. EXISTING 1/2 " BAR
6. EXISTING HMA SURFACE COURSE OVERLAY
7. EXISTING 8" STABILIZED SHOULDER
8. EXISTING CONCRETE CURB AND GUTTER
9. EXISTING CONCRETE GUTTER
10. EXISTING SUB-BASE GRANULAR
11. EXISTING LEVELING BINDER
12. EXISTING HMA SURFACE COURSE
13. EXISTING AGGREGATE SHOULDER, TYPE B

PROPOSED LEGEND

14. PROPOSED AGGREGATE SHOULDER, TYPE B
15. PROPOSED HMA SURFACE COURSE REMOVAL, 1-1/2"
16. PROPOSED HMA SURFACE COURSE MIXTURE C, N50 1-1/2"
17. PROPOSED HMA SHOULDER
18. PROPOSED PAVEMENT MARKING-LINE 5"








• 104RS2,BR3;105RS7,BR3,4;106RS6

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PLOT DATE = Oct-31-2011 03:52:11PM		DATE -	REVISED -		SCALE:	SHEET NO. 1 OF 10 SHEETS	STA. _____ TO STA. _____	ILLINOIS FED. AID PROJECT				



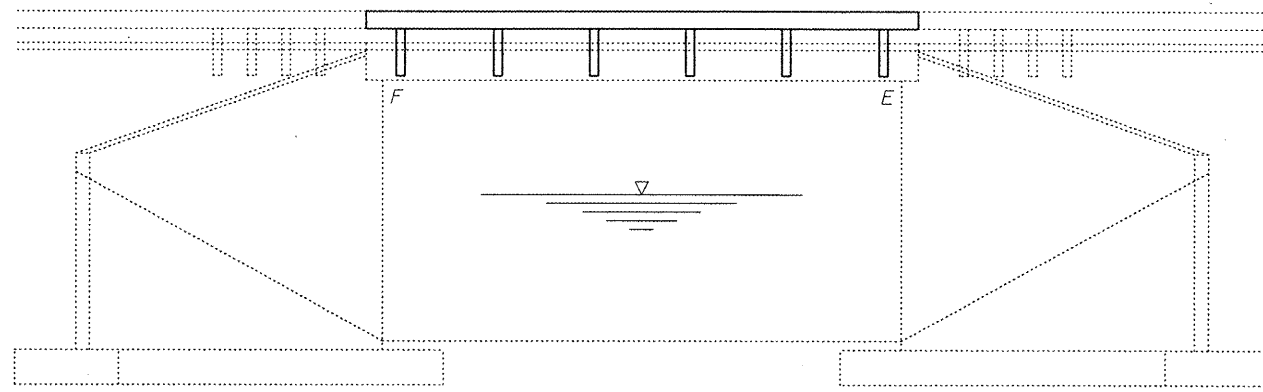
STAGE I STAGING DETAILS

**SYMBOLS**

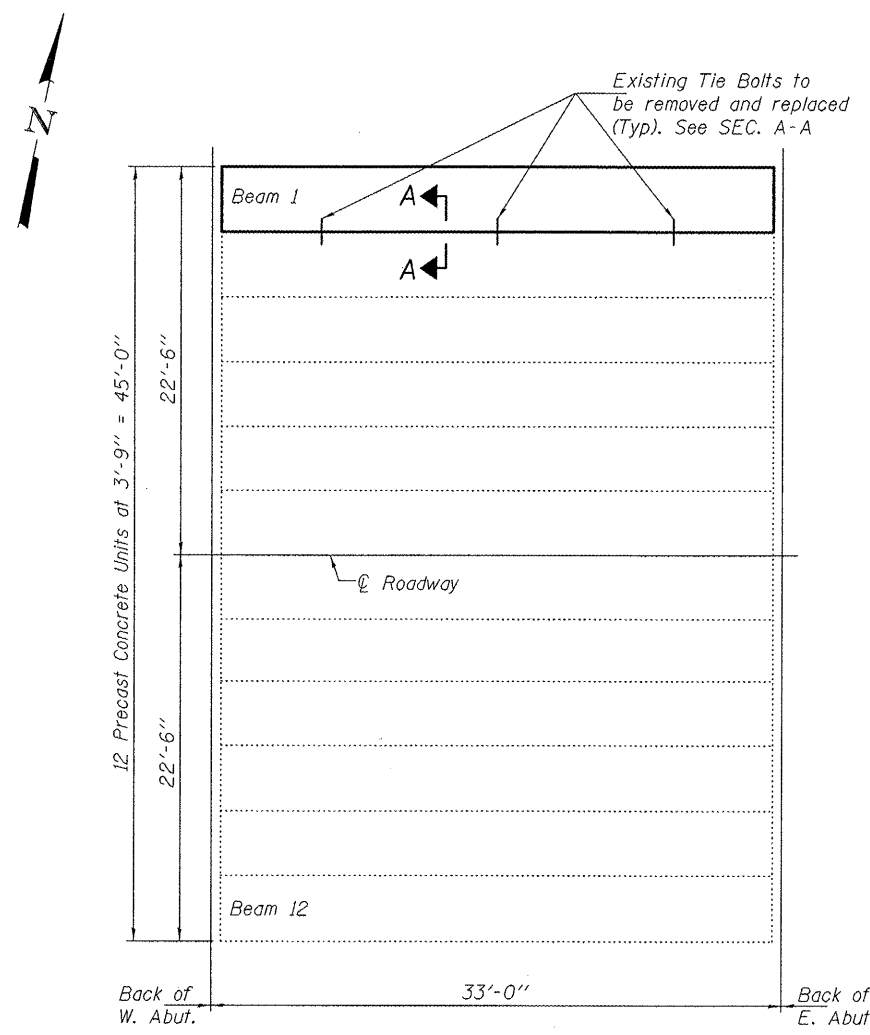
-  Drum with steady burning bi-directional light
-  Temporary concrete barrier
-  Work area
-  Type III barricade
-  Traffic signal
-  Detector loops
-  Impact attenuator
-  Crystal, bidirectional barrier wall/guardrail marker

• 104RS2,BR3;105RS7,BR3,4;106RS6

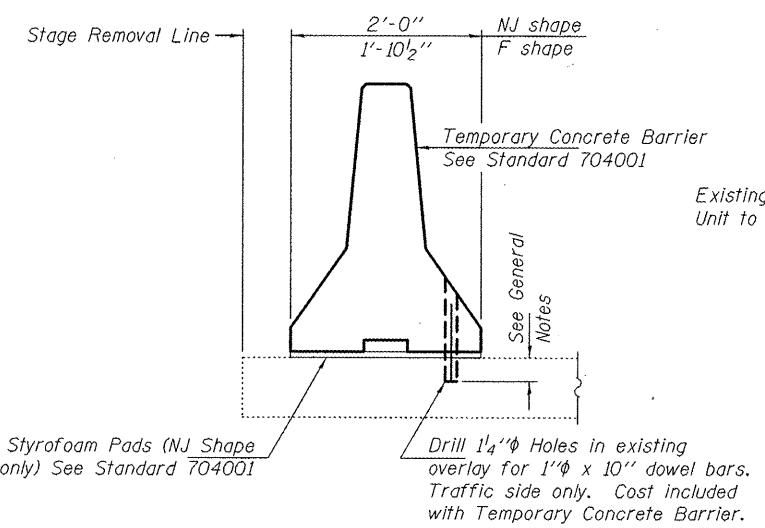
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	PLOT SCALE = 48:0.3640 '1' / in.	CHECKED -	REVISED -		SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	CONTRACT NO. 72E43				
PLOT DATE = Oct-31-2011 03:52:12PM	DATE -	REVISED -	REVISED -	ILLINOIS FED. AID PROJECT								



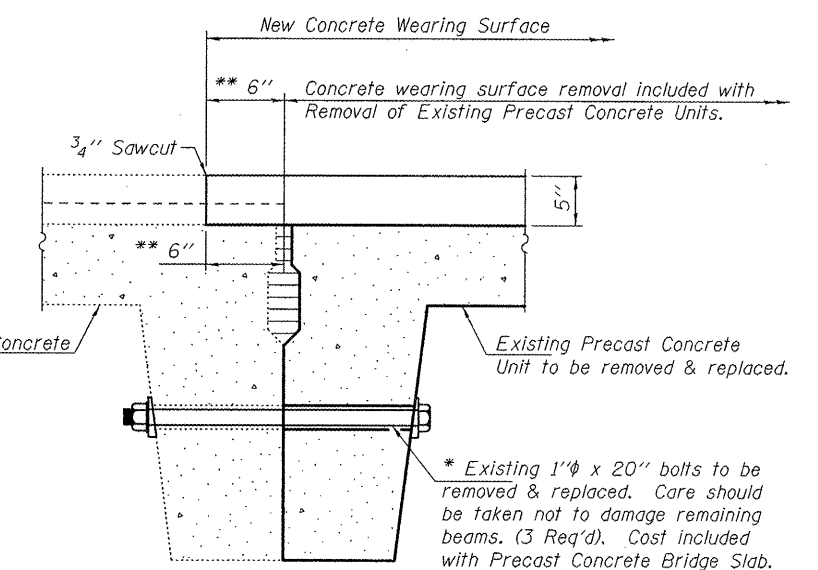
**ELEVATION**



**PLAN**



**SECTION THRU SLAB**



**SECTION A-A**

- \* New bolts shall only be snug tightened after placement.
- \*\* Limits of Concrete Wearing Surface Removal.
- \*\*\* Existing reinforcement to extend 6" min. beyond removal line.

**DESIGN STRESSES**

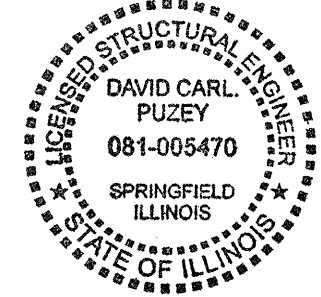
**PC UNITS**  
 $f_c = 1,800$  psi  
 $f'_c = 4,500$  psi  
 $f_s = 20,000$  psi (Reinforcement)  
 $n = 8$

**FIELD UNITS**  
 $f'_c = 3,500$  psi  
 $f_y = 60,000$  psi (Reinforcement)

**DESIGN SPECIFICATIONS**  
 2002 AASHTO Bridge Design Specifications

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	QUANTITY
Precast Concrete Bridge Slab	Sq. Ft.	120
Removal of Existing Precast Concrete Units	Sq. Ft.	120
Concrete Wearing Surface Removal	Sq. Yd.	1.8
Concrete Wearing Surface, 5"	Sq. Yd.	15.1
Reinforcement Bars, Epoxy Coated	Pound	190
Mechanical Splicers	Each	33
Steel Railing, Type S1	Foot	64



EXPIRES 11-30-2012

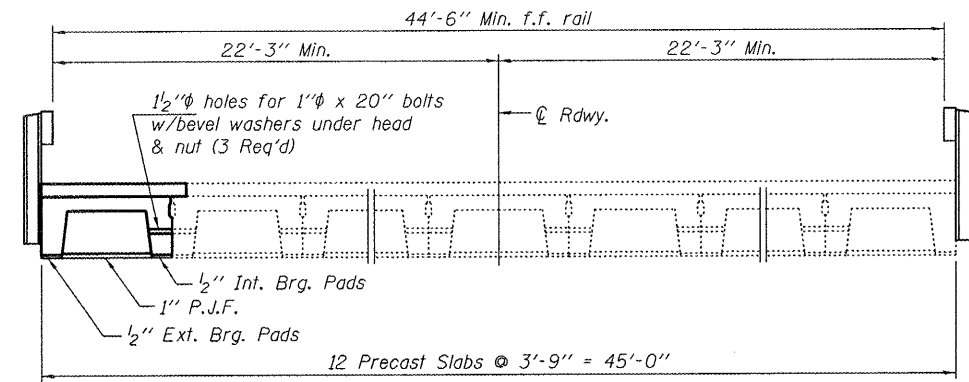
DESIGNED: <i>Ally Vancey</i>	EXAMINED: <i>July F. J...</i>	DATE: NOVEMBER 28, 2011
CHECKED: <i>Mered Christman</i>	PASSED: <i>David Carl Puzey</i>	REVISED:
DRAWN: <i>ballva</i>	REVISED:	
CHECKED: <i>ADY</i>		

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

PLAN AND ELEVATION  
 FAP 321 OVER PLUM POINT SLOUGH  
 SN 075-0022

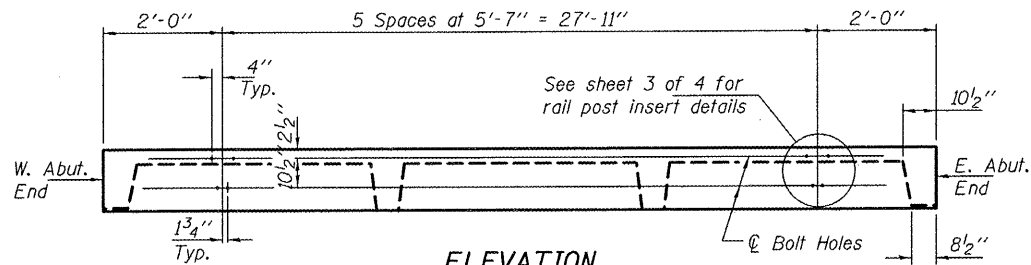
F.A.P. RTE. 321	SECTION 104RS2, BR3, 105RS7, BR3, 4; 106RS6	COUNTY PIKE	TOTAL SHEETS 69	SHEET NO. 52
CONTRACT NO. T2E43			ILLINOIS FED. AID PROJECT	





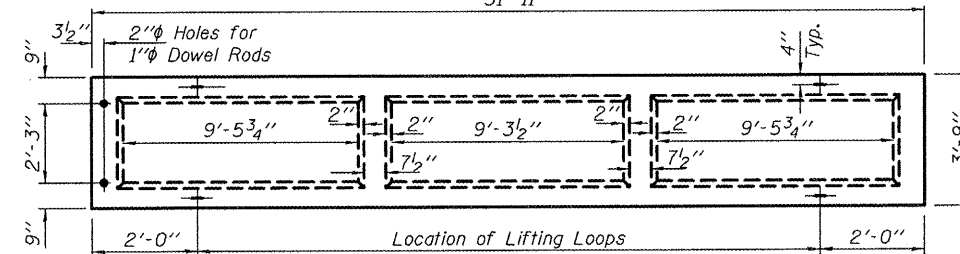
**HALF CROSS SECTION**  
(Looking East)

**HALF END ELEVATION**  
(Looking East)

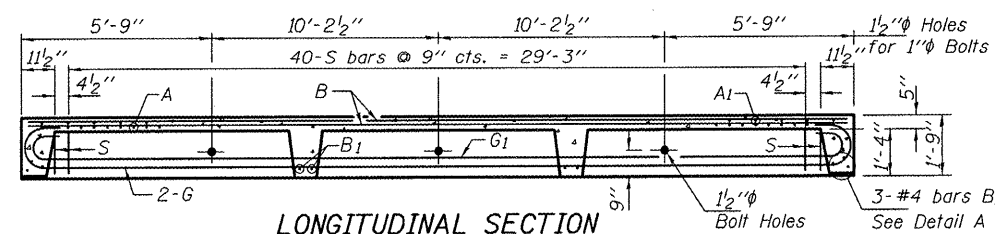


**ELEVATION**

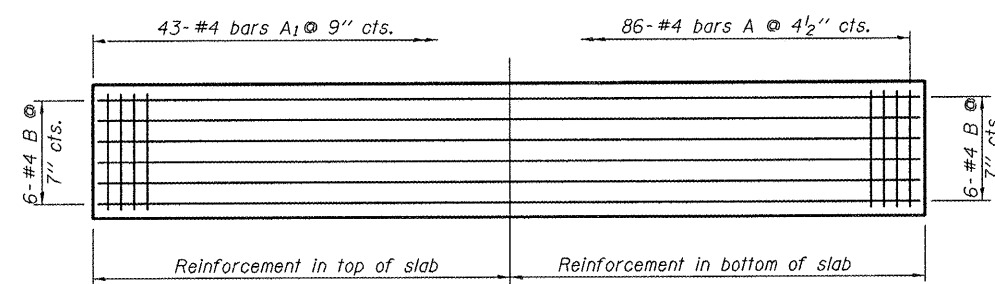
Showing rail post spacing  
31'-11"



**TYPICAL PLAN OF BEAM**

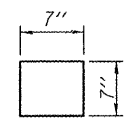


**LONGITUDINAL SECTION**

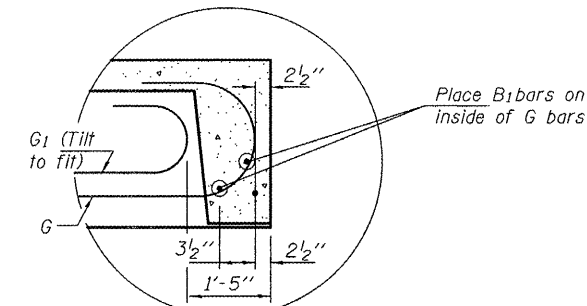


**PLAN**

Showing Slab Reinforcement

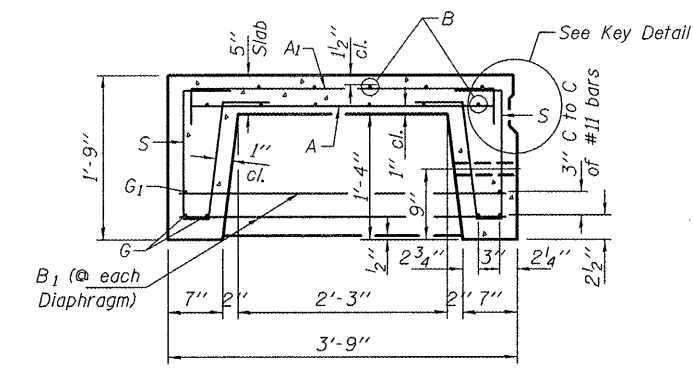


**BEARING PADS**  
4-1/2" Fabric Pads Req'd.

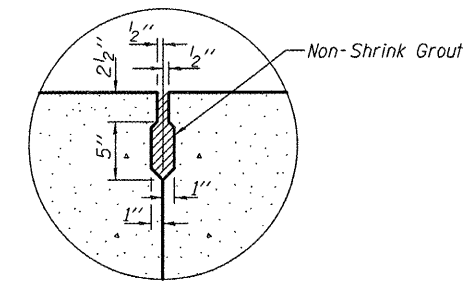


**DETAIL A**

Note:  
The surface of the member shall not deviate more than 1/1200 of the full length of the member from a straight line connecting the two end points on the member's surface. (See Special Provision Precast Concrete Structures)

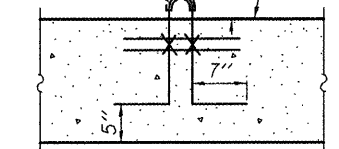


**TYPICAL SECTION THRU BEAM**



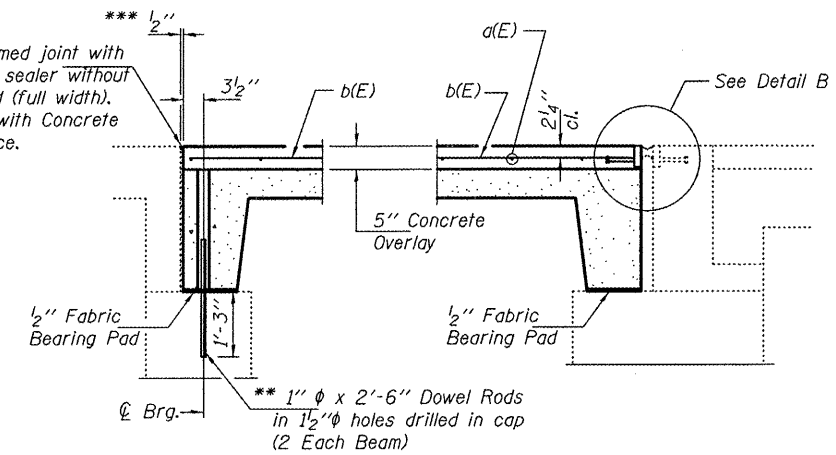
**KEY DETAIL**

3-1/2" Strands/Loop, 2 Ea. End, Ea. Beam. Loop shall be burned off after beams have been erected. Strands shall conform to the requirements of AASHTO M 203.



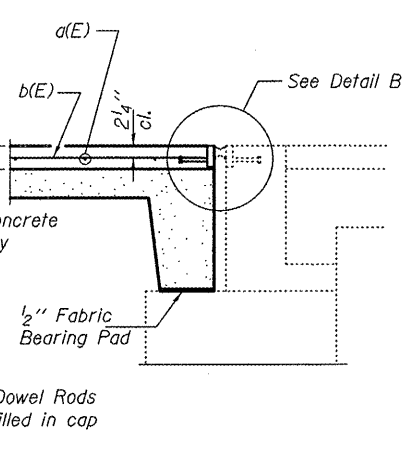
**LIFTING LOOP**

Approved alternate may be substituted for the above.



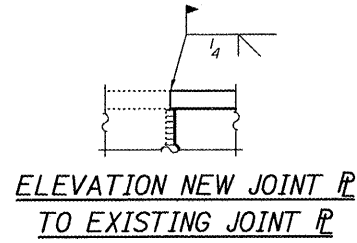
**TYPICAL SECTION AT W. ABUT.**

\*\*\* Dowel Rods to be grouted after beams are in place and allowed to cure (Min. 24 hrs.) prior to grouting the shear keys.



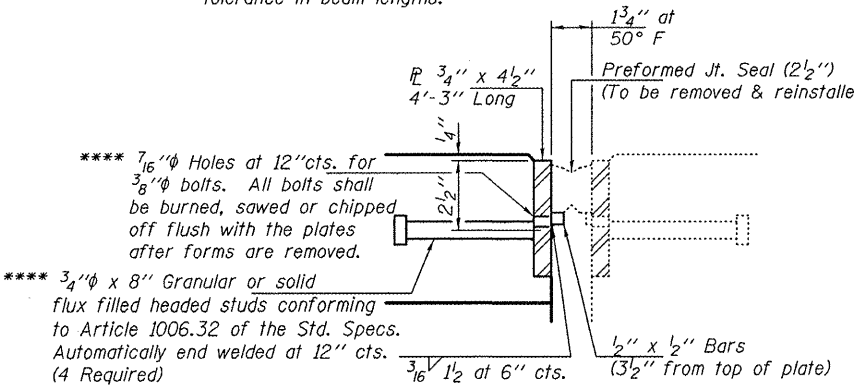
**TYPICAL SECTION AT E. ABUT.**

\*\*\* 1/2" joint shall be packed with a very dry mix of 2:1 sand and P.C. Mortar. 1/2" dimension may vary plus or minus to accommodate tolerance in beam lengths.



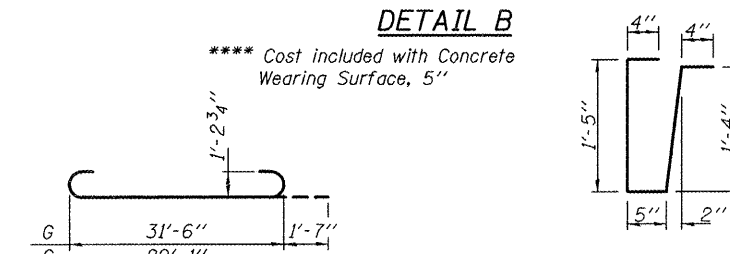
**ELEVATION NEW JOINT R TO EXISTING JOINT R**

**BAR A1**

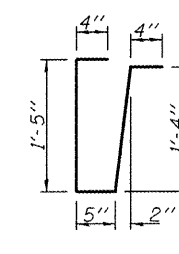


**DETAIL B**

\*\*\*\* Cost included with Concrete Wearing Surface, 5"



**BARS G & G1**



**BAR S**

**\* BILL OF REINFORCEMENT BARS FOR ONE BEAM**

Bar	No.	Size	Length	Shape
A	86	#4	3'-3"	—
A1	43	#4	3'-11"	□
B	12	#4	31'-6"	—
B1	10	#4	3'-6"	—
G	4	#11	34'-8"	—
G1	2	#11	32'-3"	—
S	80	#3	3'-10"	U

**\* ESTIMATED QUANTITIES FOR ONE BEAM**

	CONCRETE CU. YDS.	RE-BARS POUNDS
One Unit	4.2	1770

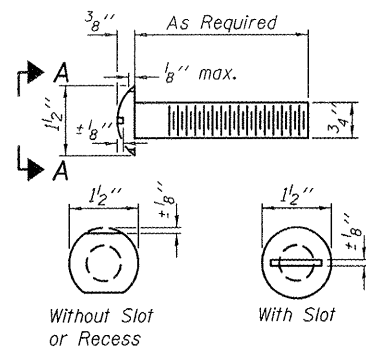
\* For information of suppliers of Precast Slab Units only.

DESIGNED ADY	EXAMINED	DATE NOVEMBER 28, 2011
CHECKED MKC	PASSED	REVISED
DRAWN baliva		REVISED
CHECKED ADY MKC		

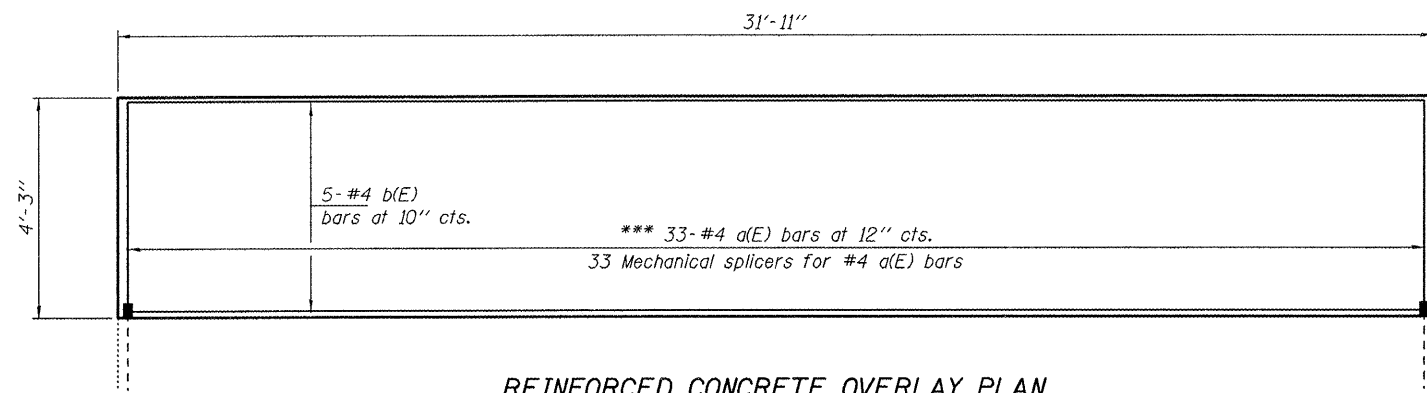
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

REPAIR DETAILS  
SN 075-0022  
SHEET NO. 2 OF 4 SHEETS

F.A.P. RTE. 321	SECTION 104RS2, BR3, 10RSR7, BR3, 4; 10RSR6	COUNTY PIKE	TOTAL SHEETS 69	SHEET NO. 53
CONTRACT NO. 72E43			ILLINOIS FED. AID PROJECT	



VIEW A-A  
ROUND HEAD BOLT



REINFORCED CONCRETE OVERLAY PLAN

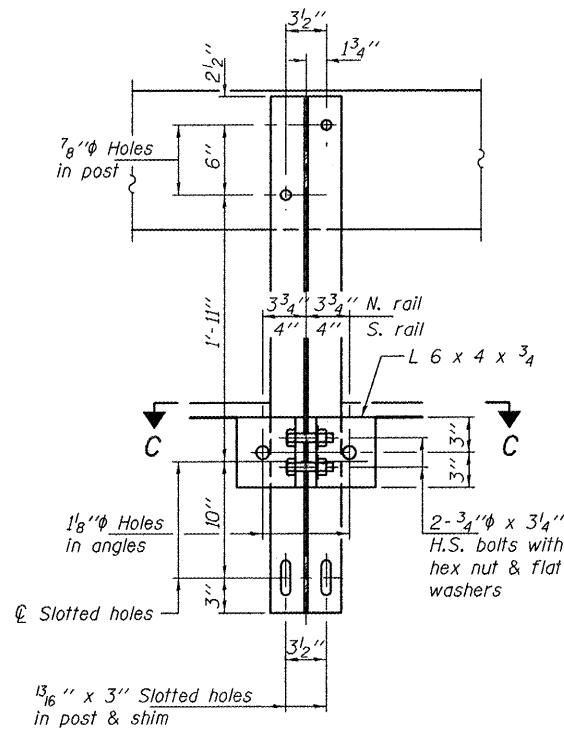
**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a(E)	33	#4	3'-11"	
b(E)	5	#4	31'-7"	
Concrete Wearing Surface, 5"		Sq. Yd.	15.1	
Reinforcement Bars, Epoxy Coated		Pound	190	

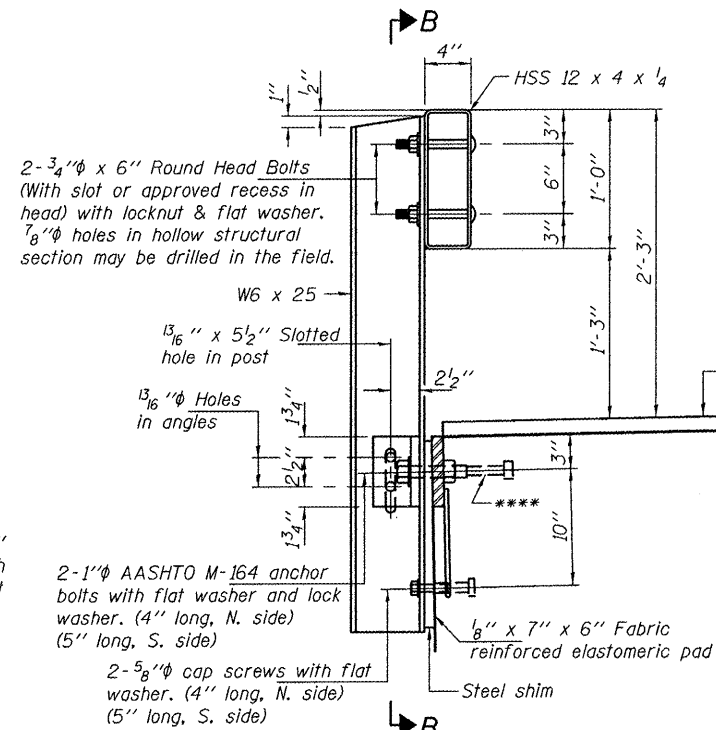
\*\*\* Attach to existing reinforcement with mechanical splicers. Existing reinforcement to extend 6" (min.) into the removal area to allow attachment of the mechanical splicers.

Notes:

All field drilled holes shall be coated with an approved zinc rich paint before erection. For multi-span bridges, sufficient 1/4" x 6" x 1'-2" galvanized steel shims shall be provided to align rail between adjacent spans. Cost included with Steel Railing, Type S-1. All steel rail elements shall be galvanized according to Article 509.05 of the Standard Specifications.

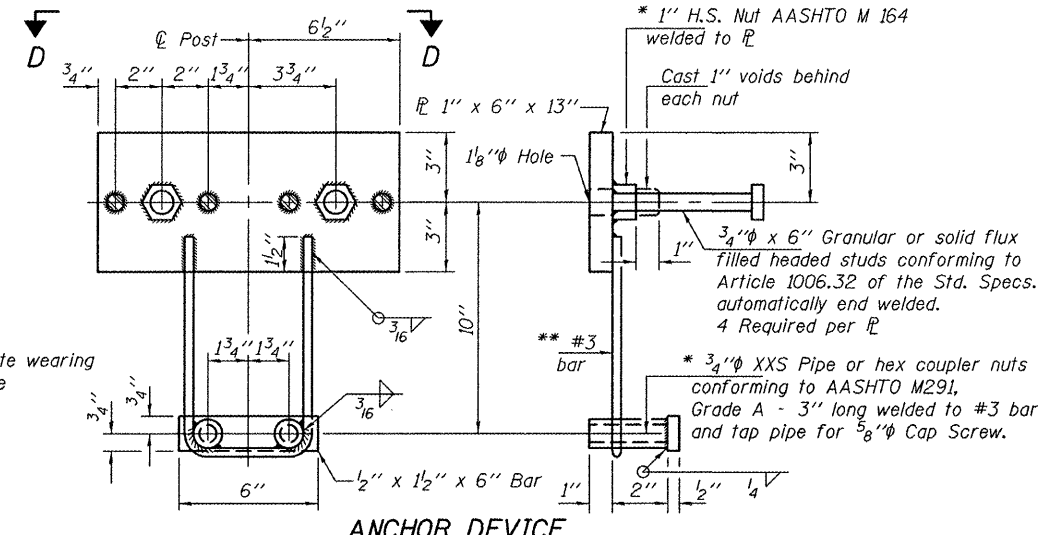


SECTION B-B



SECTION AT RAILING POST

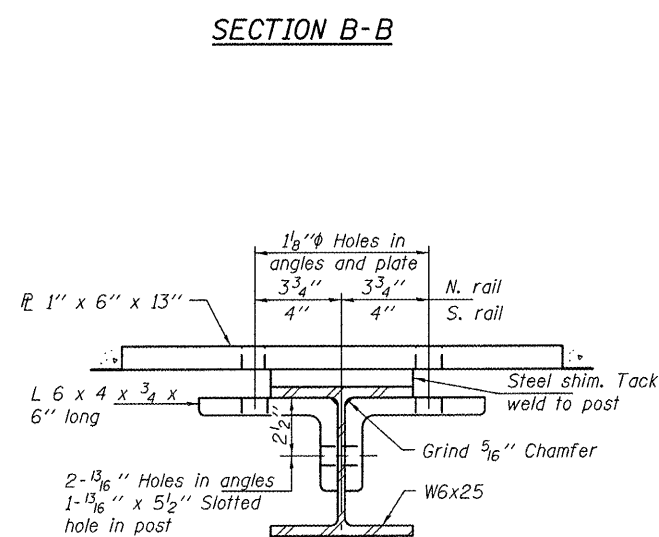
\*\*\*\* The studs of the anchor devices shall be placed below the top reinforcement bars and the outermost longitudinal reinforcement bar shall be placed directly above the studs of the rail post anchor device.



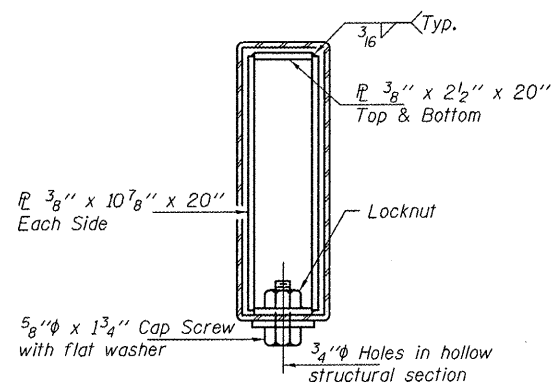
ANCHOR DEVICE

\* Threaded areas shall be plugged or blocked off during casting of beam.

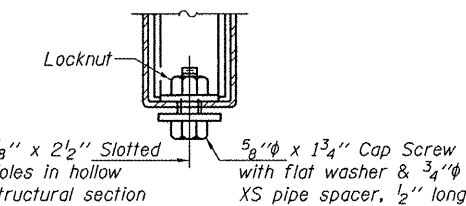
\*\* Whenever the lower insert assemblies interfere with strand locations, the #3 bars shall be cut and adjusted in order to allow raising or lowering of the lower inserts. Maximum adjustment not to exceed 1/2".



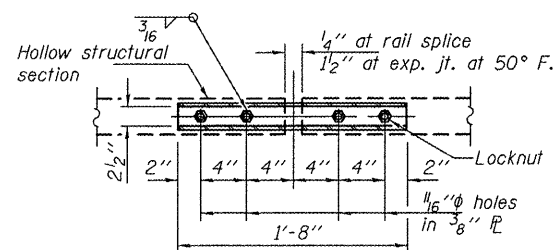
SECTION C-C



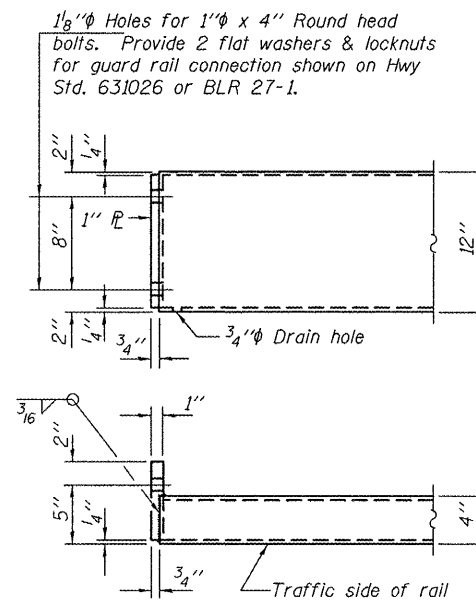
SECTIONS AT RAIL SPLICE



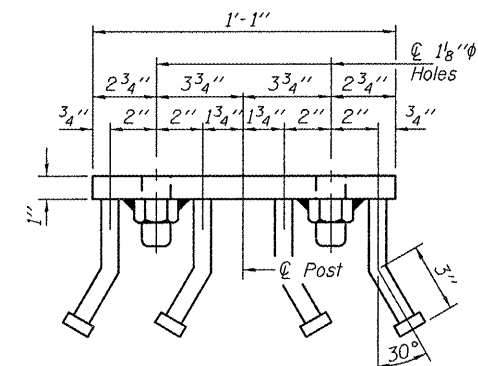
RAIL SPLICE CONNECTION AT EXPANSION JT.



PLAN-BOTT. SPLICE TYPICAL



END OF RAIL DETAILS



VIEW D-D

**BILL OF MATERIAL**

Item	Unit	Quantity
Steel Railing, Type S1	Foot	64

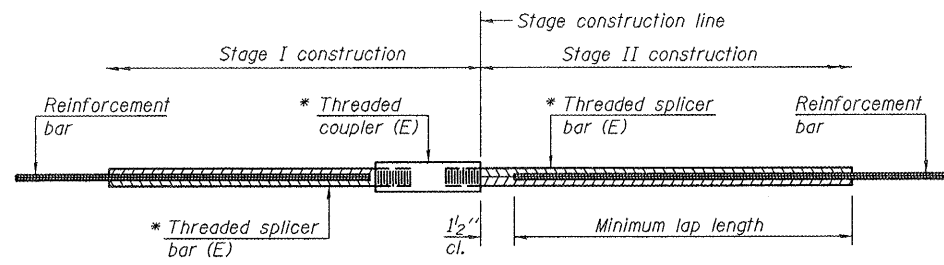
DESIGNED ADY	EXAMINED	DATE NOVEMBER 28, 2011
CHECKED MKC	PASSED	
DRAWN baliva		
CHECKED ADY MKC		

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

REPAIR DETAILS  
SN 075-0022

SHEET NO. 3 OF 4 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
321	104RS2, BR3, 10SR57, BR3, 41 10RS56	PIKE	69	54
CONTRACT NO. 72E43			ILLINOIS FED. AID PROJECT	



**STANDARD BAR SPLICER ASSEMBLY**

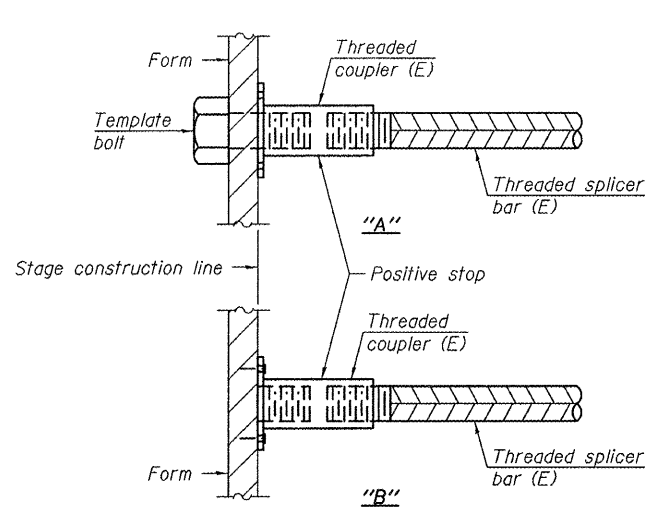
Bar size to be spliced	Minimum Lap Lengths				
	Table 1	Table 2	Table 3	Table 4	Table 5
3, 4	1'-5"	1'-11"	2'-1"	2'-4"	2'-3"
5	1'-9"	2'-5"	2'-7"	2'-11"	2'-10"
6	2'-1"	2'-11"	3'-1"	3'-6"	3'-4"
7	2'-9"	3'-10"	4'-2"	4'-8"	4'-6"
8	3'-8"	5'-1"	5'-5"	6'-2"	5'-10"
9	4'-7"	6'-5"	6'-10"	7'-9"	7'-5"

- Table 1: Black bar, 0.8 Class C
- Table 2: Black bar, Top bar lap, 0.8 Class C
- Table 3: Epoxy bar, 0.8 Class C
- Table 4: Epoxy bar, Top bar lap, 0.8 Class C
- Table 5: Epoxy bar, Top bar lap, Class B

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

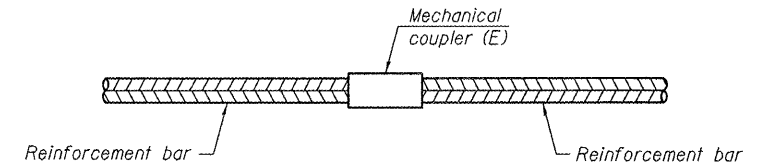
\* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length



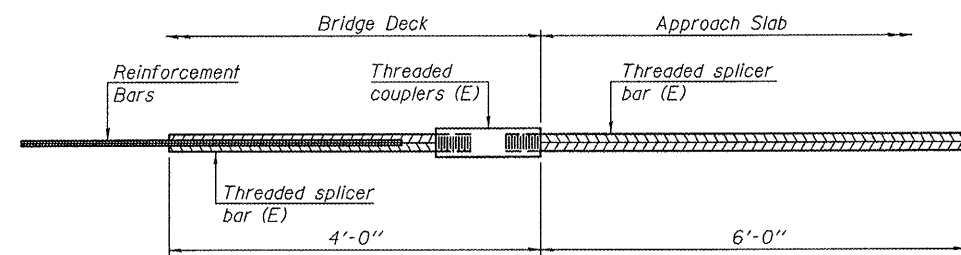
**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.  
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.  
 (E) : Indicates epoxy coating.



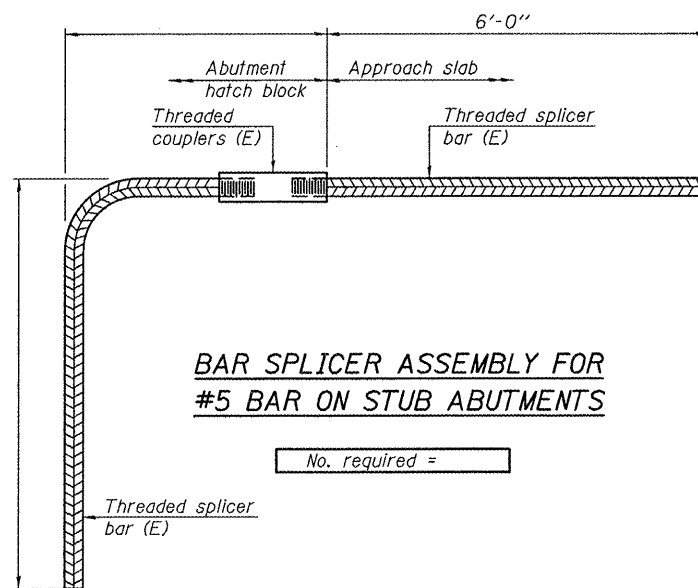
**STANDARD MECHANICAL SPLICER**

Location	Bar size	No. assemblies required
Conc. Wearing Surface	#4	33



**BAR SPLICER ASSEMBLY FOR #5 BAR ON INTEGRAL OR SEMI-INTEGRAL ABUTMENTS**

No. required =



**BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS**

No. required =

**NOTES**

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.  
 All reinforcement shall be lapped and tied to the splicer bars.  
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.  
 See special provision for Mechanical Splicers.  
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

7-1-10

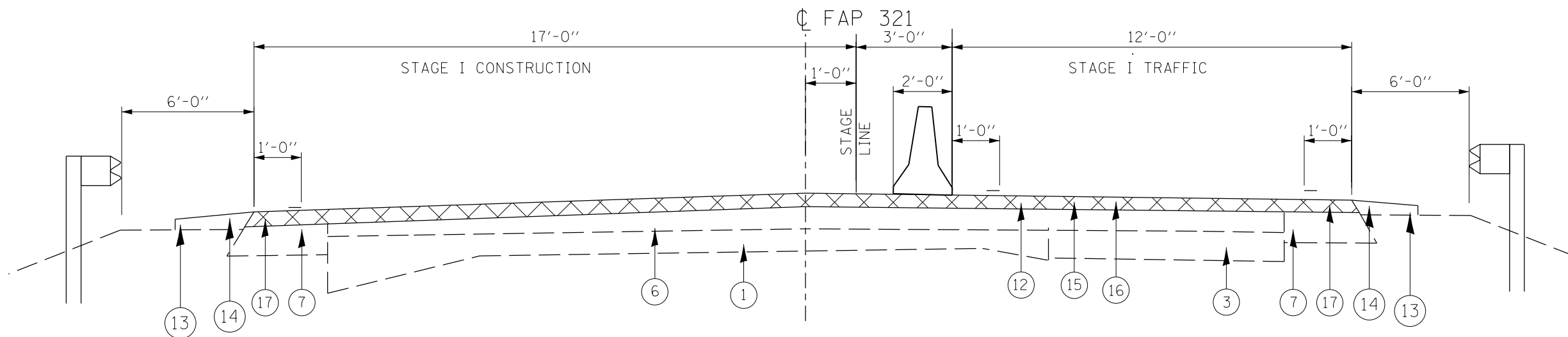
DESIGNED ADY	EXAMINED	DATE NOVEMBER 28, 2011
CHECKED MKC	PASSED	REVISOR
DRAWN baliva	ACTING ENGINEER OF STRUCTURAL SERVICES	REVISOR
CHECKED ADY MKC	ACTING ENGINEER OF BRIDGES AND STRUCTURES	REVISOR

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

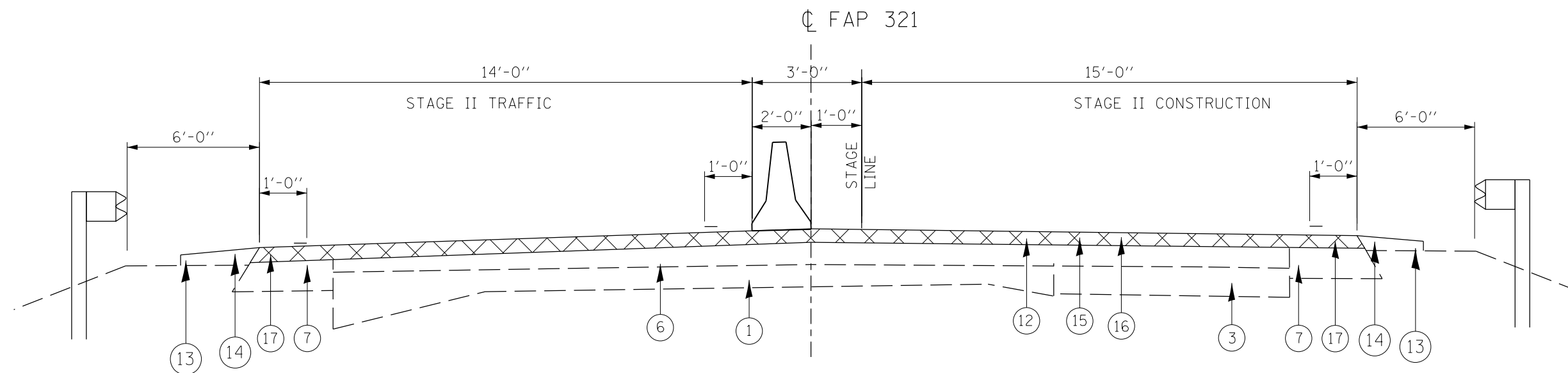
BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS  
SN 075-0022

SHEET NO. 4 OF 4 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
321	104RS2, BR3; 105RS7, BR3, 4; 106RS6	PIKE	69	55
CONTRACT NO. 72E43				
ILLINOIS FED. AID PROJECT				



STAGE I CONSTRUCTION DETAILS - ROADWAY SECTION



STAGE II CONSTRUCTION DETAILS - ROADWAY SECTION

EXISTING LEGEND

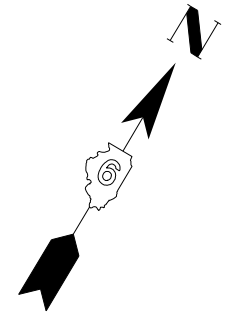
1. EXISTING PCC PAVEMENT
2. EXISTING PCC COURSE 1'
3. EXISTING 8" P.C.C. BASE COURSE WIDENING
4. EXISTING 3/4" SMOOTH BAR
5. EXISTING 1/2" BAR
6. EXISTING HMA SURFACE COURSE OVERLAY
7. EXISTING 8" STABILIZED SHOULDER
8. EXISTING CONCRETE CURB AND GUTTER
9. EXISTING CONCRETE GUTTER
10. EXISTING SUB-BASE GRANULAR
11. EXISTING LEVELING BINDER
12. EXISTING HMA SURFACE COURSE
13. EXISTING AGGREGATE SHOULDER, TYPE B

PROPOSED LEGEND

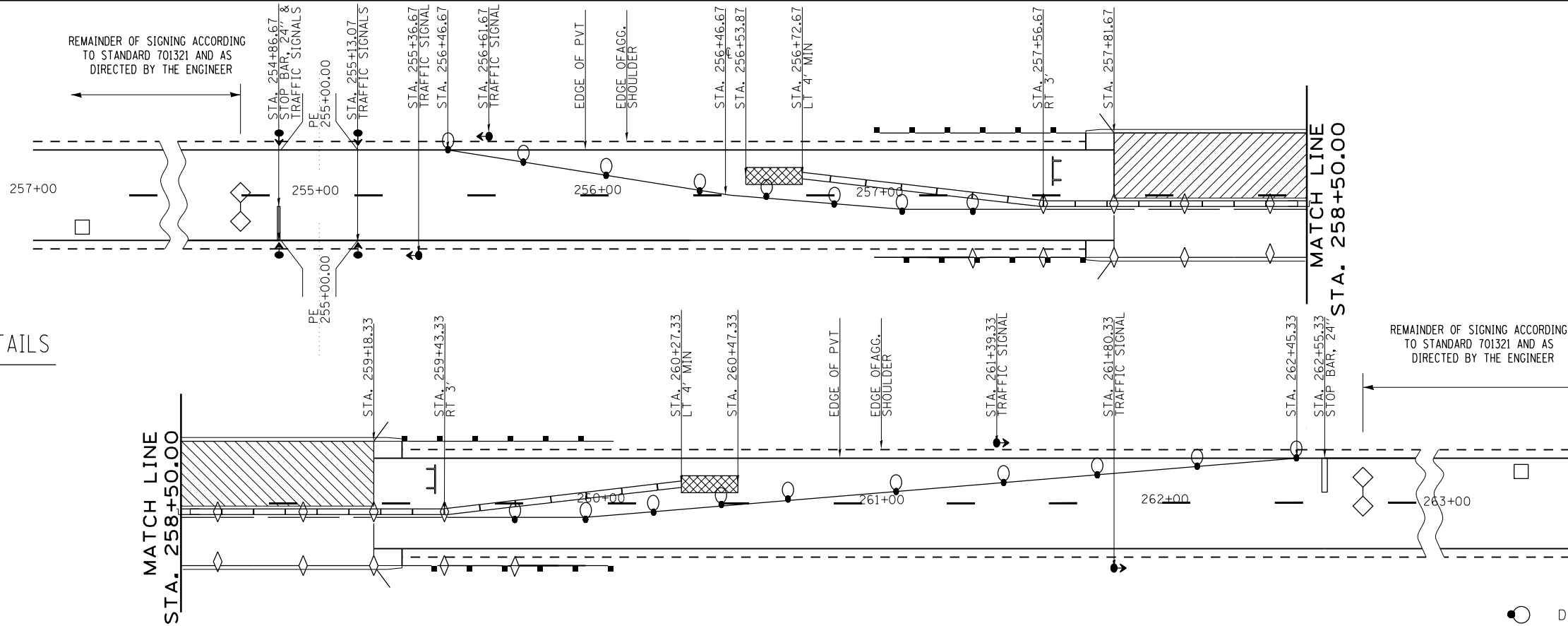
14. PROPOSED AGGREGATE SHOULDER, TYPE B
15. PROPOSED HMA SURFACE COURSE REMOVAL, 1-1/2"
16. PROPOSED HMA SURFACE COURSE MIXTURE C, N50 1-1/2"
17. PROPOSED HMA SHOULDER
18. PROPOSED PAVEMENT MARKING-LINE 5"

• 104RS2,BR3;105RS7,BR3,4;106RS6

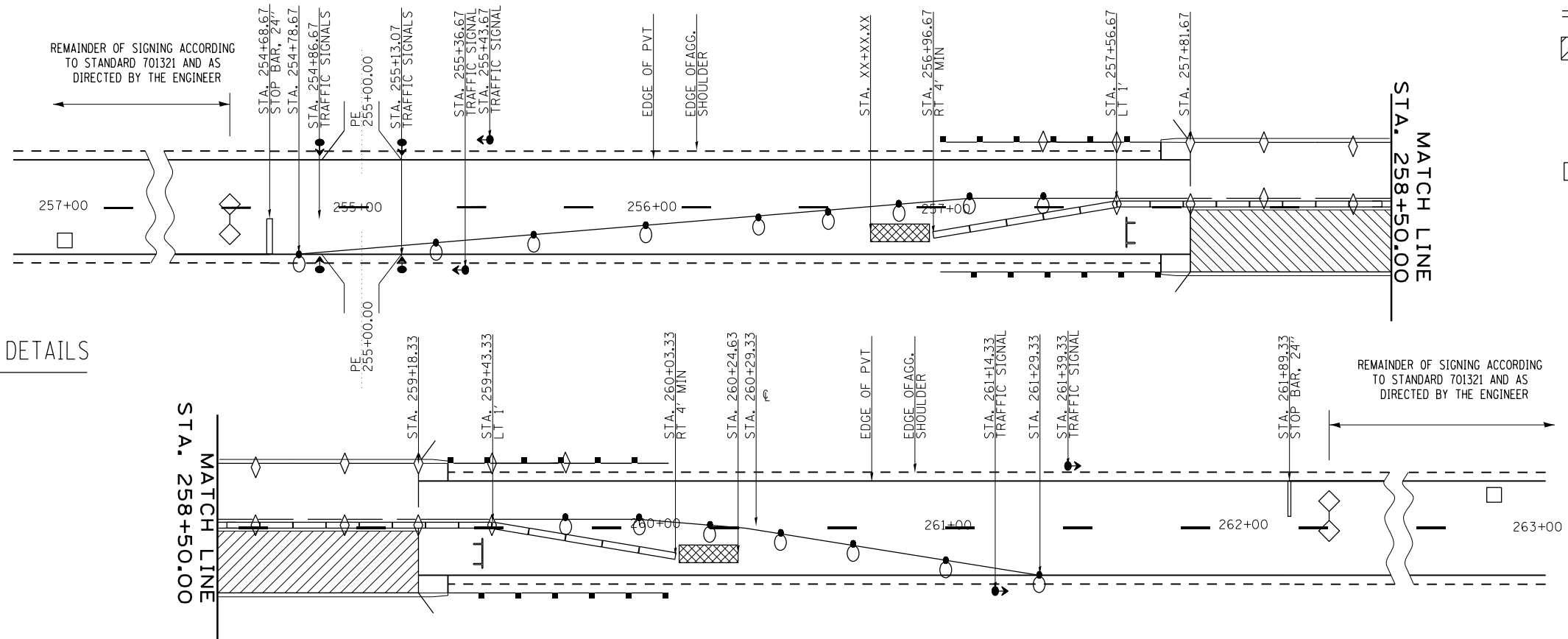
FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STAGING DETAILS TYPICAL SECTIONS SN 075-0077</b>			F.A.P. RTE. 321	SECTION *	COUNTY PIKE	TOTAL SHEETS 69	SHEET NO. 56
c:\pwork\work\pwork\sparksgw\10265393\STAGING.dgn		DRAWN -	REVISED -		SCALE:	SHEET NO. 1 OF 10 SHEETS	STA. _____ TO STA. _____	CONTRACT NO. 72E43				
		PLOT SCALE = 48:0.6997' / in.	CHECKED -		REVISED -	ILLINOIS FED. AID PROJECT						
		PLOT DATE = Oct-31-2011 03:52:24PM	DATE -		REVISED -							



STAGE I STAGING DETAILS



STAGE II STAGING DETAILS



**SYMBOLS**

- Drum with steady burning bi-directional light
- Temporary concrete barrier
- Work area
- Type III barricade
- Traffic signal
- Detector loops
- Impact attenuator
- Crystal, bidirectional barrier wall/guardrail marker

• 104RS2, BR3; 105RS7, BR3, 4; 106RS6

FILE NAME = c:\pwwork\p1dot\sparksgw\10265393\STAGING.dgn	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STAGING DETAILS SN 075-0077</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 48:0.3640 '1' / in.	CHECKED -	REVISED -		321	•	PIKE	69	57			
PLOT DATE = Oct-31-2011 03:52:25PM	DATE -	REVISED -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.			CONTRACT NO. 72E43				
							ILLINOIS FED. AID PROJECT					

**GENERAL NOTES**

All structural steel shall conform to AASHTO Classification M-270 Gr. 36, unless otherwise noted.

Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions.

Reinforcement bars designated (E) shall be epoxy coated.

Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

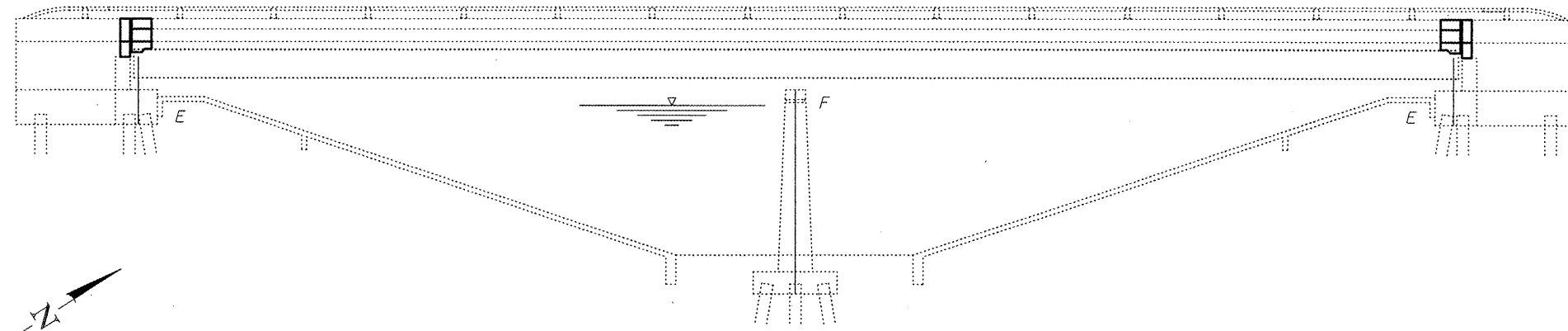
Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

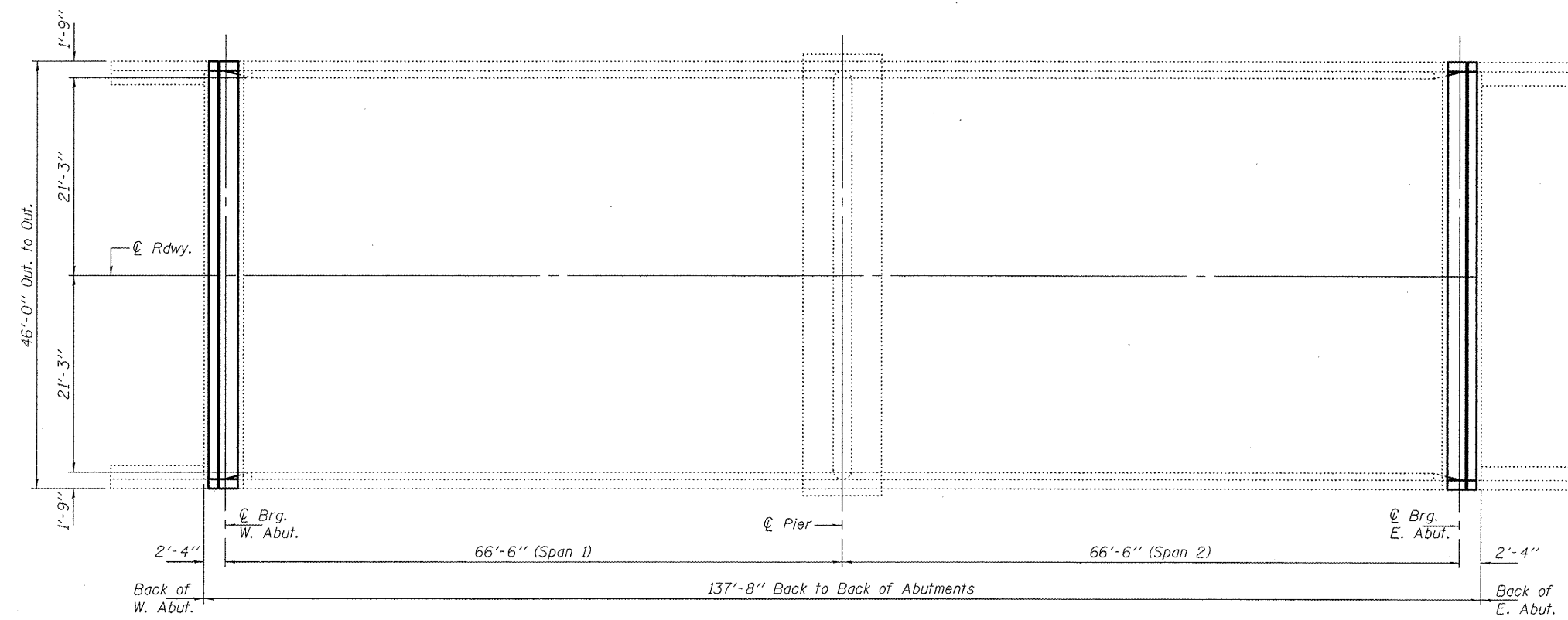
Joint openings shall be adjusted according to Article 520.04 of the Standard Specifications when the deck is poured at an ambient temperature other than 50° F.

The deck surface shall have its final finish tined according to Article 420.09(e)(1) of the Standard Specifications. Cost included with Concrete Superstructures.

The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.



**ELEVATION**



**PLAN**

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	QUANTITY
Concrete Removal	Cu. Yd.	14.8
Concrete Superstructure	Cu. Yd.	14.7
Preformed Joint Strip Seal	Foot	90
Reinforcement Bars, Epoxy Coated	Pound	1630
Bar Splicers	Each	28
* Protective Coat	Sq. Yd.	33.3

\* New Concrete Areas Only.



Expires: November 30, 2012

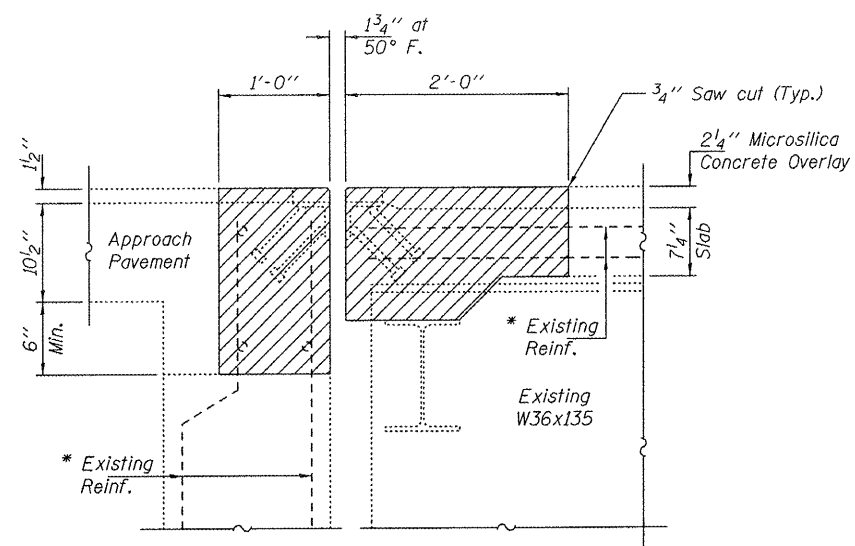
DESIGNED - <i>[Signature]</i>	EXAMINED - <i>[Signature]</i>	DATE - NOVEMBER 28, 2011
CHECKED - <i>[Signature]</i>	PASSED - <i>[Signature]</i>	
DRAWN - Kyle W. Steffen		
CHECKED - <i>[Signature]</i>		

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**GENERAL PLAN & ELEVATION  
U.S. 54 OVER RELOCATED SNY RIVER  
SN 075-0077**

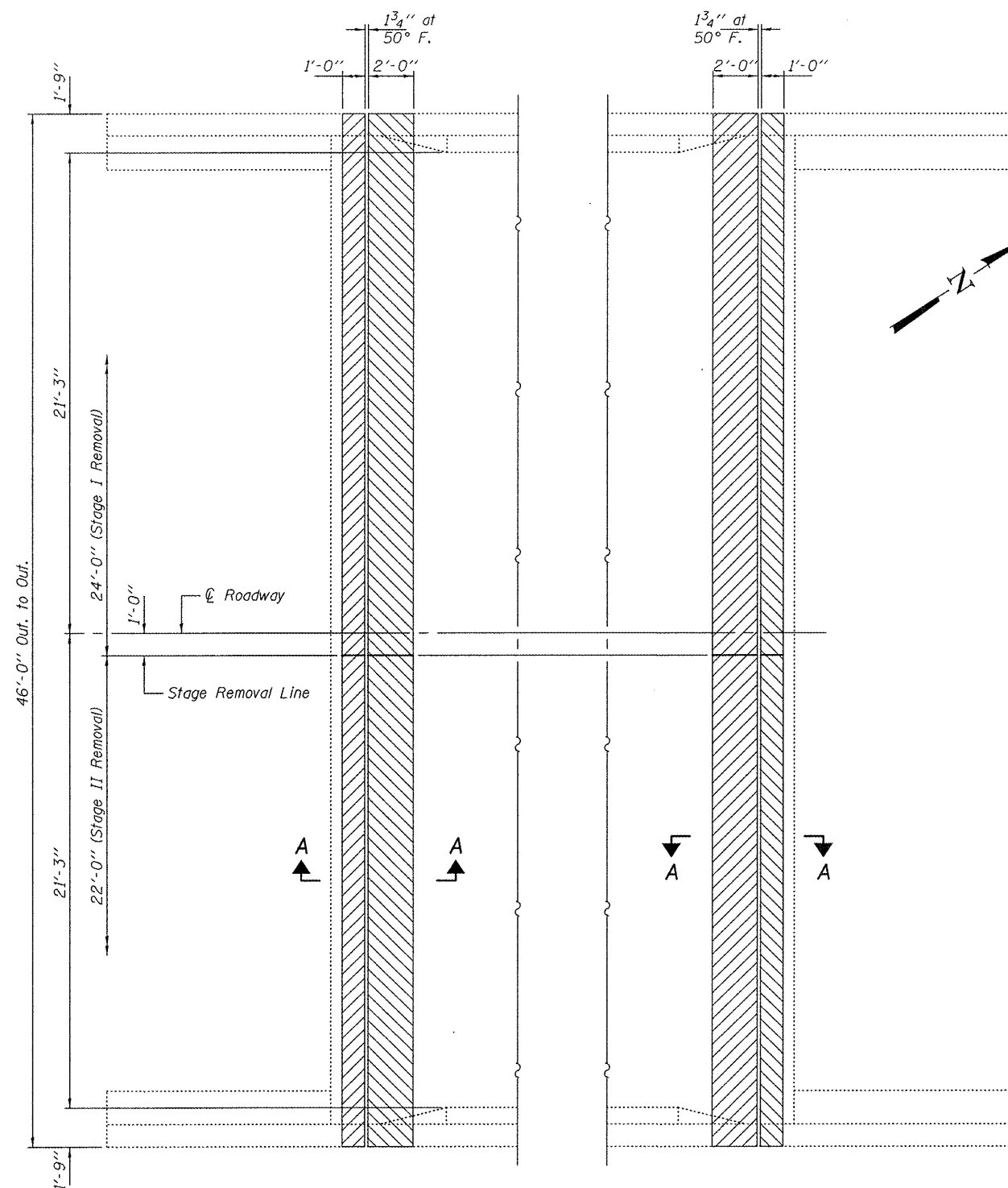
SHEET NO. 1 OF 5 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
321	104RS2, BR3;	PIKE	69	58
	105RS7, BR3, 4; 106RS6		CONTRACT NO. 72E43	
ILLINOIS FED. AID PROJECT				



**SECTION A-A**

\* Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.



**CONCRETE REMOVAL PLAN**

DESIGNED - IJL  
 CHECKED - ADY  
 DRAWN - Kyle M. Steffen  
 CHECKED - IJL ADY

EXAMINED *Joanne F. J. [Signature]*  
 ACTING ENGINEER OF STRUCTURAL SERVICES  
 PASSED *[Signature]*  
 ACTING ENGINEER OF BRIDGES AND STRUCTURES

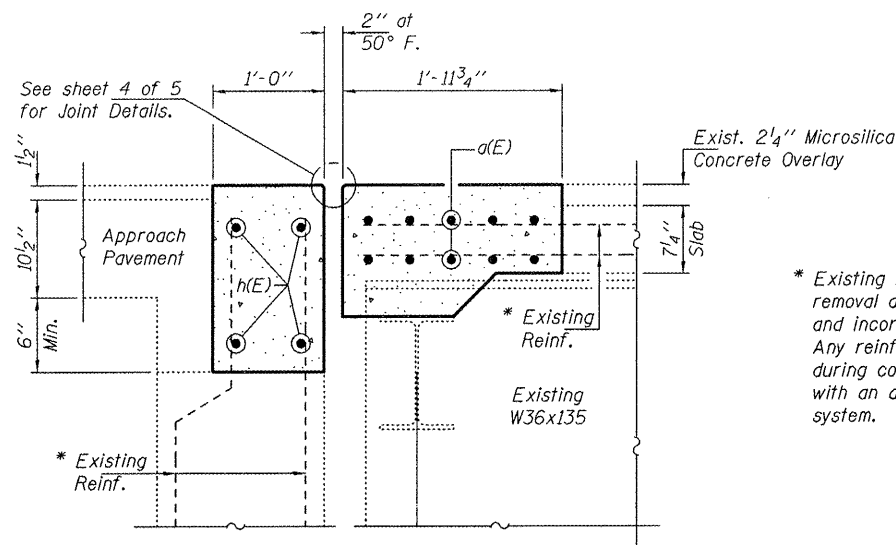
DATE - NOVEMBER 28, 2011

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**CONCRETE REMOVAL DETAILS  
 SN 075-0077**

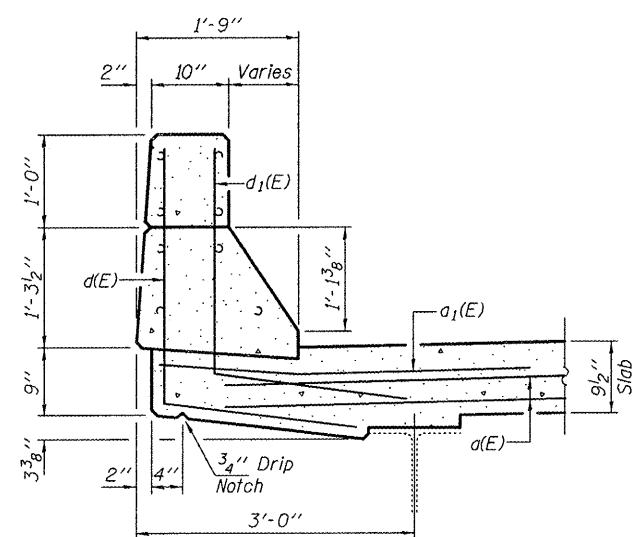
SHEET NO. 2 OF 5 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
321	104RS2, BR3;	PIKE	69	59
	105RS7, BR3, 4; 106RS6			
CONTRACT NO. 72E43				
ILLINOIS FED. AID PROJECT				

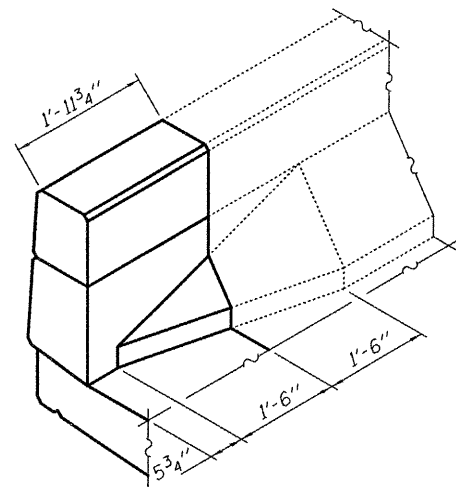


\* Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

**SECTION B-B**

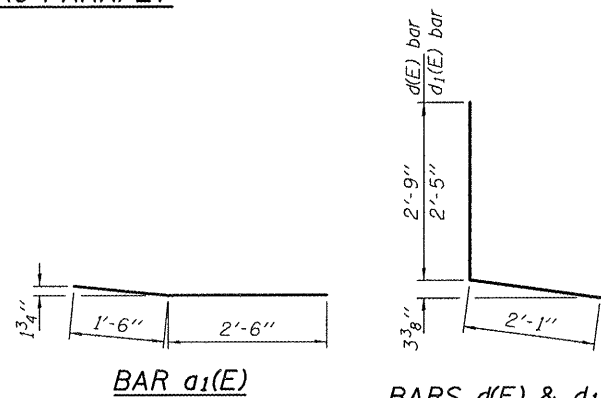


**SECTION THRU PARAPET**



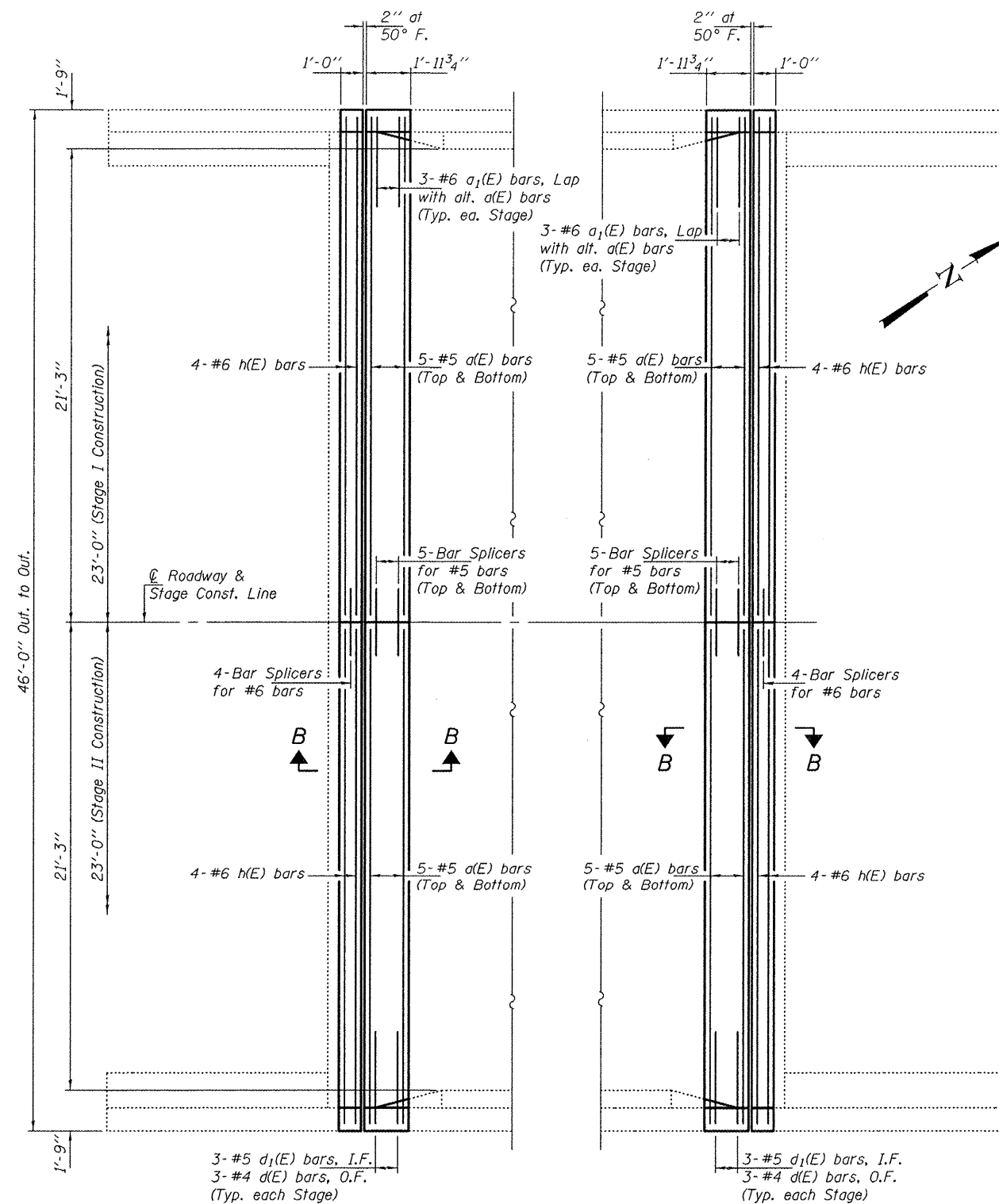
**ISOMETRIC PARAPET END DETAILS**

Hatch Block, Railing, and Expansion Joint not shown for clarity.



**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a(E)	40	#5	22'-0"	—
a1(E)	12	#6	4'-0"	—
d(E)	12	#4	4'-10"	L
d1(E)	12	#5	4'-6"	L
h(E)	16	#6	22'-8"	—
Concrete Removal		Cu. Yd.	14.8	
Concrete Superstructure		Cu. Yd.	14.7	
Reinforcement Bars, Epoxy Coated		Pound	1630	



**CONCRETE REPLACEMENT PLAN**

DESIGNED - IJL  
 CHECKED - ADY  
 DRAWN - Kyle M. Steffen  
 CHECKED - IJL ADY

EXAMINED *James F. [Signature]*  
 ACTING ENGINEER OF STRUCTURAL SERVICES

PASSED *[Signature]*  
 ACTING ENGINEER OF BRIDGES AND STRUCTURES

DATE - NOVEMBER 28, 2011

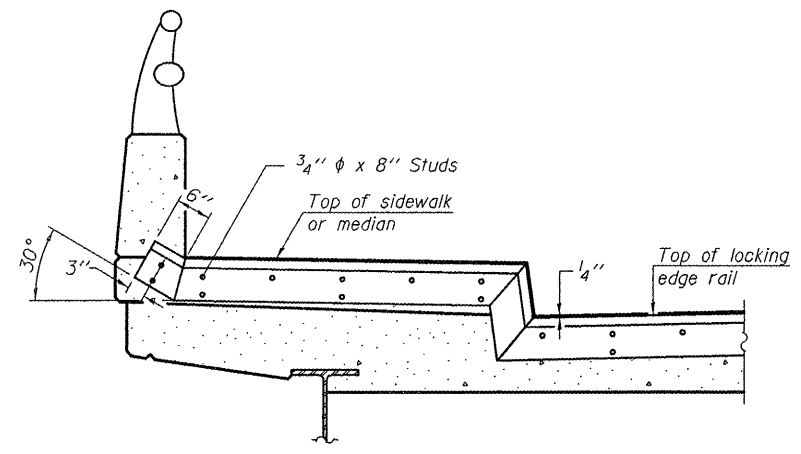
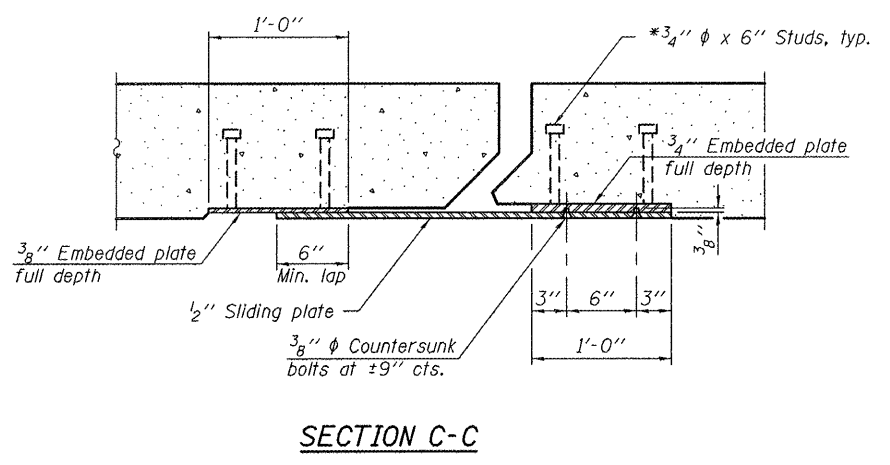
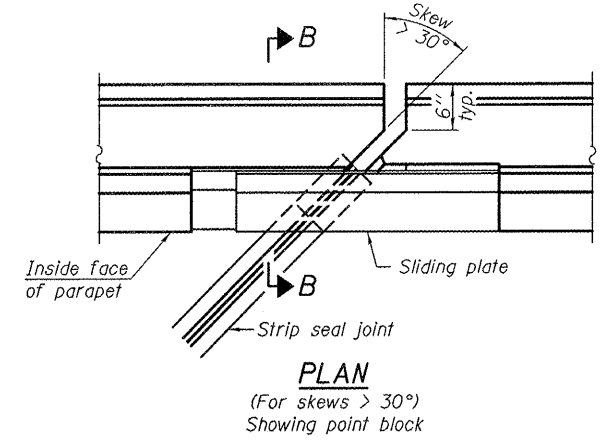
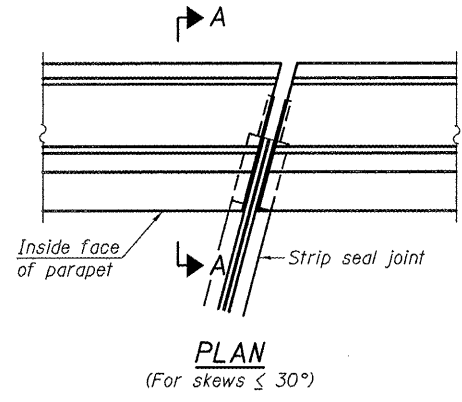
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

CONCRETE REPLACEMENT DETAILS  
 SN 075-0077

SHEET NO. 3 OF 5 SHEETS

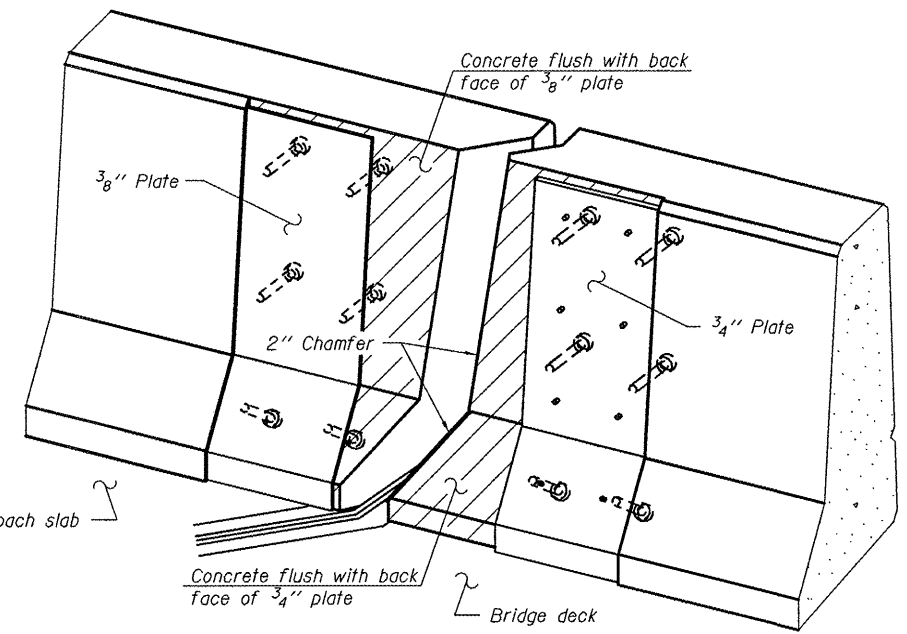
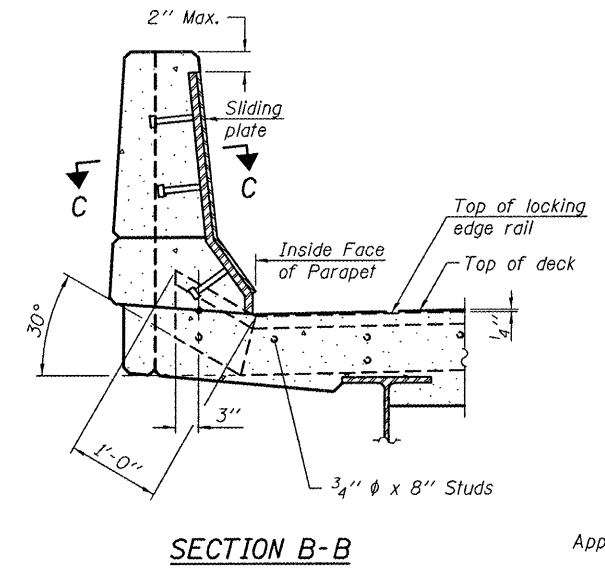
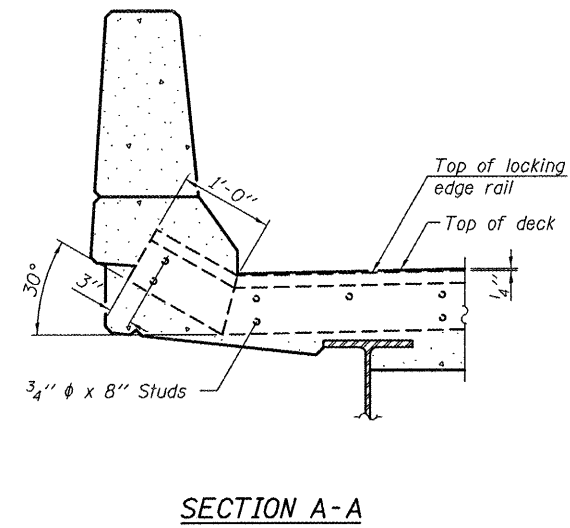
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
321	104RS2, BR3;	PIKE	69	60
	105RS7, BR3, 4; 106RS6			
CONTRACT NO. 72E43				
ILLINOIS FED. AID PROJECT				





**TYPICAL END TREATMENT AT SIDEWALK OR MEDIAN**

Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.



**Notes:**

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

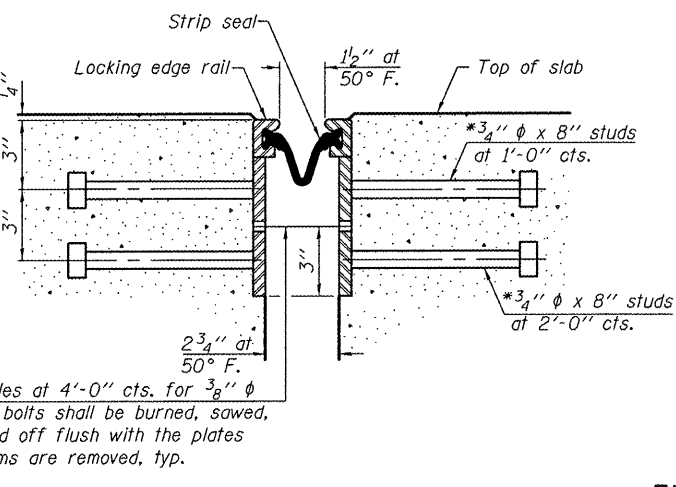
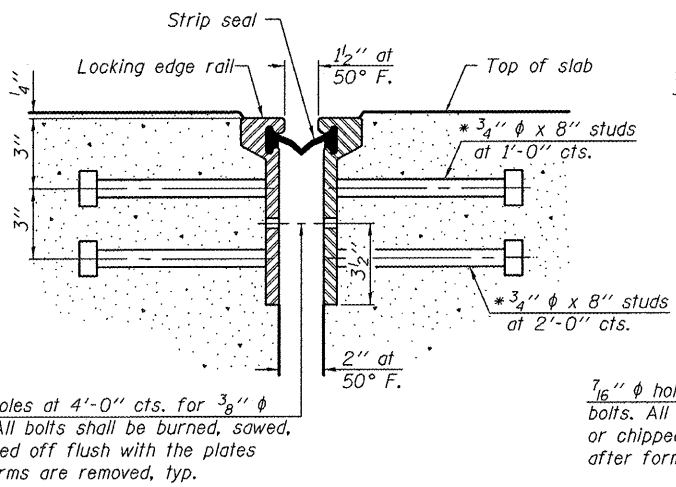
The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities.

The manufacturer's recommended installation methods shall be followed.

The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications. Maximum space between rail segments at stage lines shall be 3/16", sealed with a suitable sealant.

Parapet plates and anchorage studs for skews  $> 30^\circ$  included in the cost of Preformed Joint Strip Seal.



7/16"  $\phi$  holes at 4'-0" cts. for 3/8"  $\phi$  bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

7/16"  $\phi$  holes at 4'-0" cts. for 3/8"  $\phi$  bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

**ROLLED EXTRUDED RAIL**

**WELDED RAIL**

**LOCKING EDGE RAIL SPLICE**

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

**BILL OF MATERIAL**

Item	Unit	Total
Preformed Joint Strip Seal	Foot	90

\* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

EJ-SSJ

7-1-10

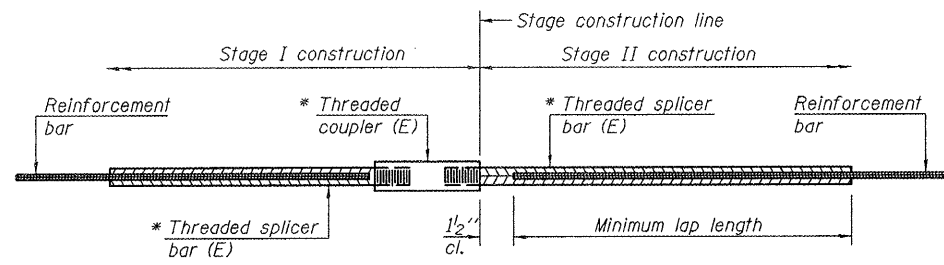
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CHECKED - ADY	PASSED - <i>[Signature]</i>	
DRAWN - Kyle M. Steffen	ACTING ENGINEER OF STRUCTURAL SERVICES	
CHECKED - IJL ADY	ACTING ENGINEER OF BRIDGES AND STRUCTURES	

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL DETAILS  
SN 075-0077

SHEET NO. 4 OF 5 SHEETS

F.A.P. RTE. 321	SECTION 104RS2, BR3; 105RS7, BR3, 4; 106RS6	COUNTY PIKE	TOTAL SHEETS 69	SHEET NO. 61
			CONTRACT NO. 72E43	
[ILLINOIS] FED. AID PROJECT				



**STANDARD BAR SPLICER ASSEMBLY**

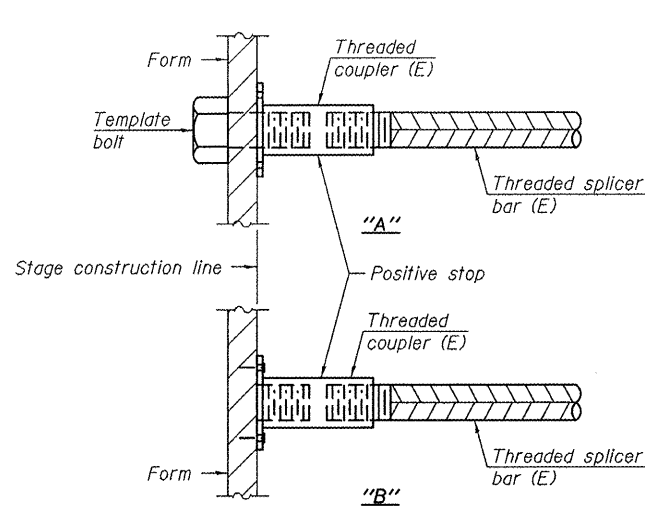
Bar size to be spliced	Minimum Lap Lengths				
	Table 1	Table 2	Table 3	Table 4	Table 5
3, 4	1'-5"	1'-11"	2'-1"	2'-4"	2'-3"
5	1'-9"	2'-5"	2'-7"	2'-11"	2'-10"
6	2'-1"	2'-11"	3'-1"	3'-6"	3'-4"
7	2'-9"	3'-10"	4'-2"	4'-8"	4'-6"
8	3'-8"	5'-1"	5'-5"	6'-2"	5'-10"
9	4'-7"	6'-5"	6'-10"	7'-9"	7'-5"

- Table 1: Black bar, 0.8 Class C
- Table 2: Black bar, Top bar lap, 0.8 Class C
- Table 3: Epoxy bar, 0.8 Class C
- Table 4: Epoxy bar, Top bar lap, 0.8 Class C
- Table 5: Epoxy bar, Top bar lap, Class B

Threaded splicer bar length = min. lap length + 1/2" + thread length

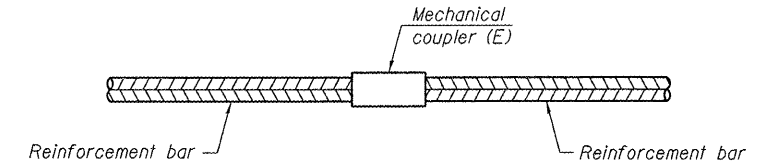
\* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length
Deck	#5	20	3
Abutments	#6	8	3



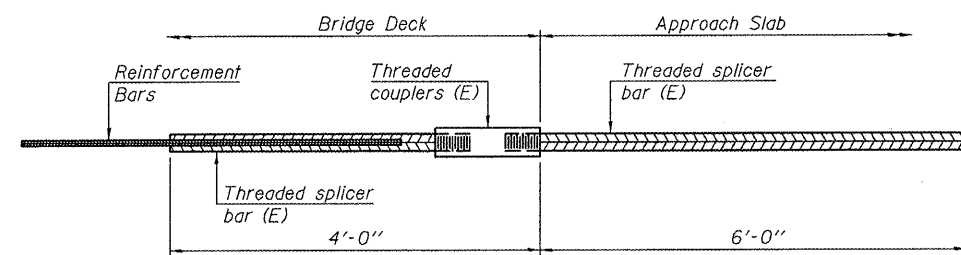
**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.  
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.  
 (E) : Indicates epoxy coating.



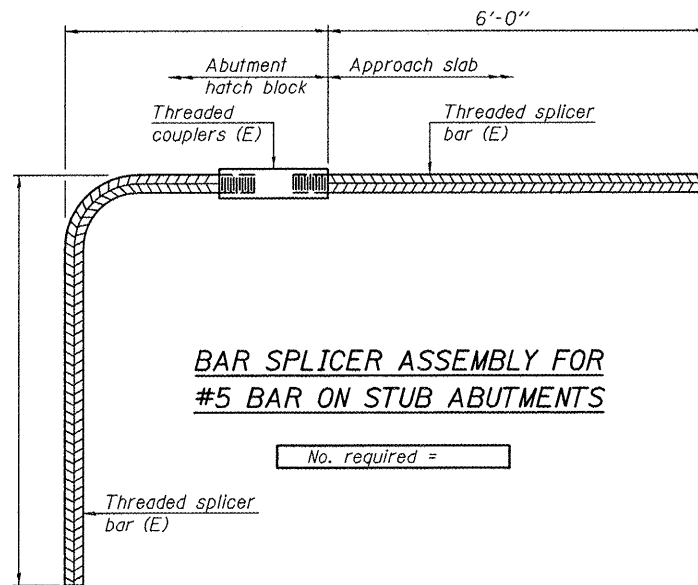
**STANDARD MECHANICAL SPLICER**

Location	Bar size	No. assemblies required



**BAR SPLICER ASSEMBLY FOR #5 BAR ON INTEGRAL OR SEMI-INTEGRAL ABUTMENTS**

No. required =



**BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS**

No. required =

**NOTES**

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.  
 All reinforcement shall be lapped and tied to the splicer bars.  
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.  
 See special provision for Mechanical Splicers.  
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

7-1-10

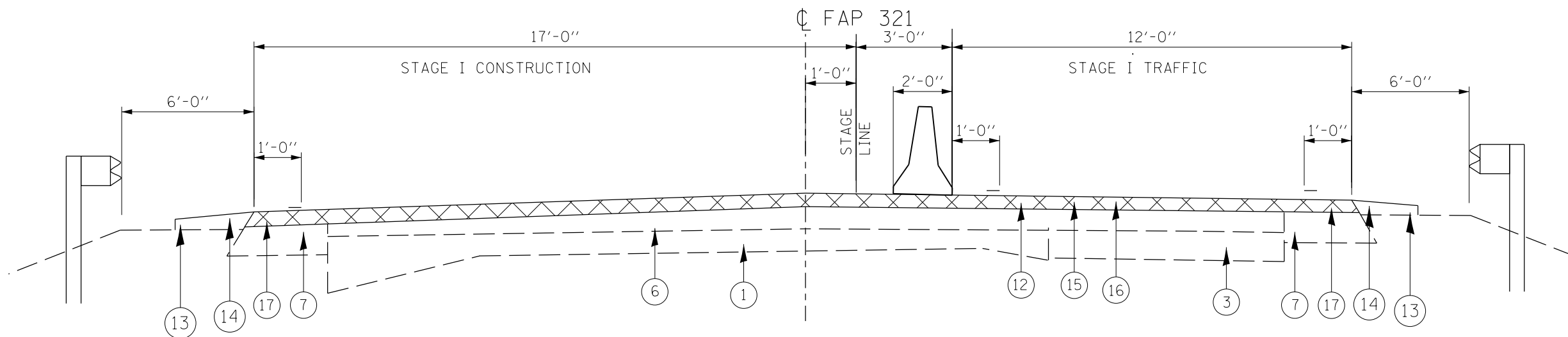
DESIGNED - IJL	EXAMINED - <i>Jayne F. J...</i>	DATE - NOVEMBER 28, 2011
CHECKED - ADY	ACTING ENGINEER OF STRUCTURAL SERVICES	
DRAWN - Kyle M. Steffen	PASSED - <i>Kyle M. Steffen</i>	
CHECKED - IJL ADY	ACTING ENGINEER OF BRIDGES AND STRUCTURES	

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

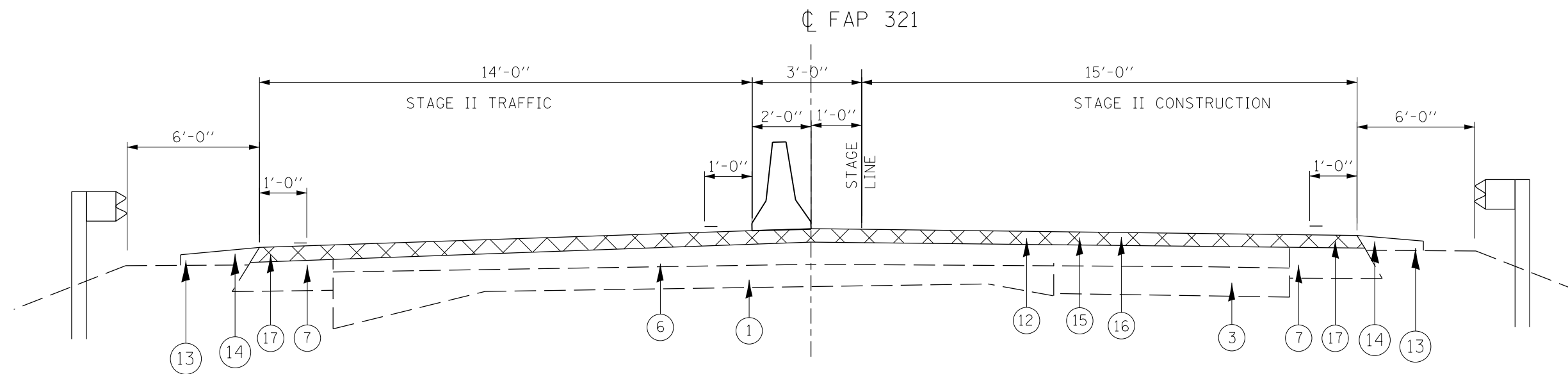
BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS  
SN 075-0077

SHEET NO. 5 OF 5 SHEETS

F.A.P. RTE. 321	SECTION 104RS2, BR3;	COUNTY PIKE	TOTAL SHEETS 69	SHEET NO. 62
	105RS7, BR3, 4; 106RS6		CONTRACT NO. 72E43	
ILLINOIS FED. AID PROJECT				



STAGE I CONSTRUCTION DETAILS - ROADWAY SECTION



STAGE II CONSTRUCTION DETAILS - ROADWAY SECTION

EXISTING LEGEND

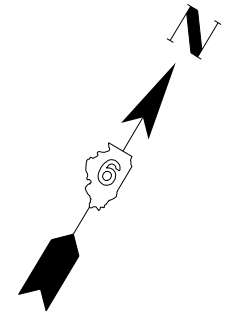
1. EXISTING PCC PAVEMENT
2. EXISTING PCC COURSE 1'
3. EXISTING 8" P.C.C. BASE COURSE WIDENING
4. EXISTING 3/4" SMOOTH BAR
5. EXISTING 1/2" BAR
6. EXISTING HMA SURFACE COURSE OVERLAY
7. EXISTING 8" STABILIZED SHOULDER
8. EXISTING CONCRETE CURB AND GUTTER
9. EXISTING CONCRETE GUTTER
10. EXISTING SUB-BASE GRANULAR
11. EXISTING LEVELING BINDER
12. EXISTING HMA SURFACE COURSE
13. EXISTING AGGREGATE SHOULDER, TYPE B

PROPOSED LEGEND

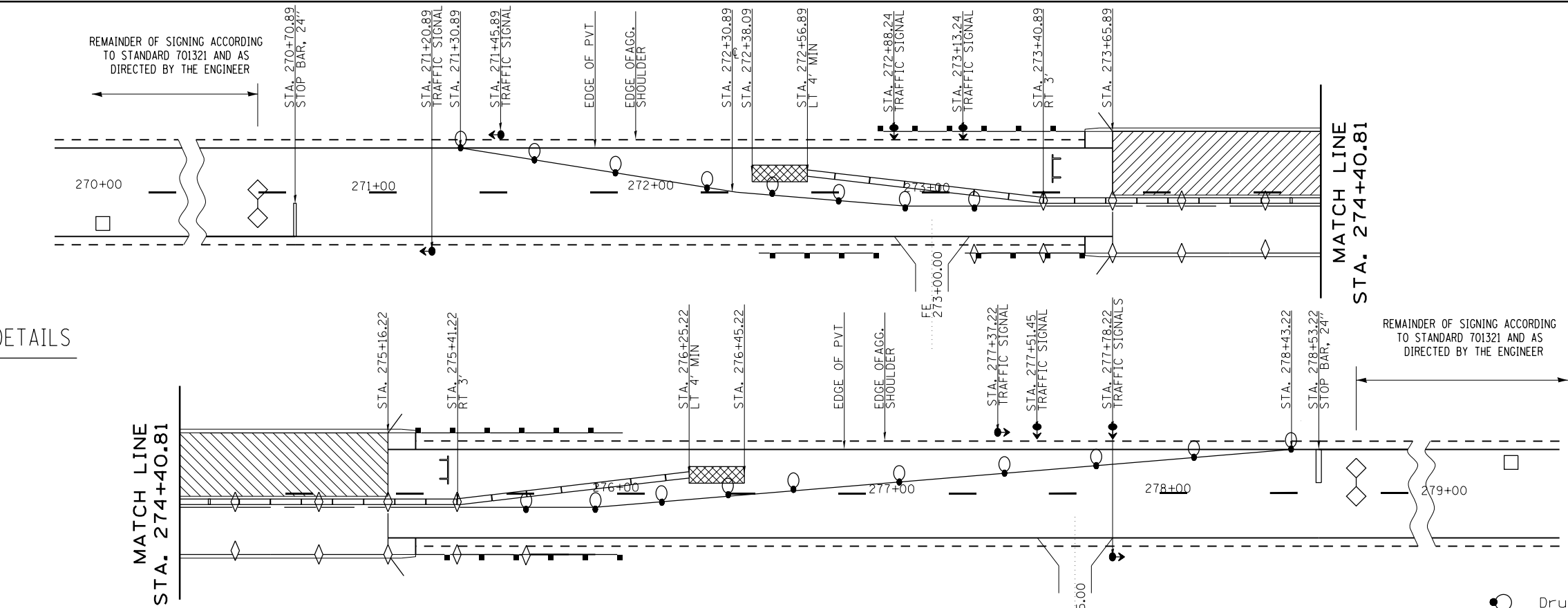
14. PROPOSED AGGREGATE SHOULDER, TYPE B
15. PROPOSED HMA SURFACE COURSE REMOVAL, 1-1/2"
16. PROPOSED HMA SURFACE COURSE MIXTURE C, N50 1-1/2"
17. PROPOSED HMA SHOULDER
18. PROPOSED PAVEMENT MARKING-LINE 5"

• 104RS2, BR3; 105RS7, BR3, 4; 106RS6

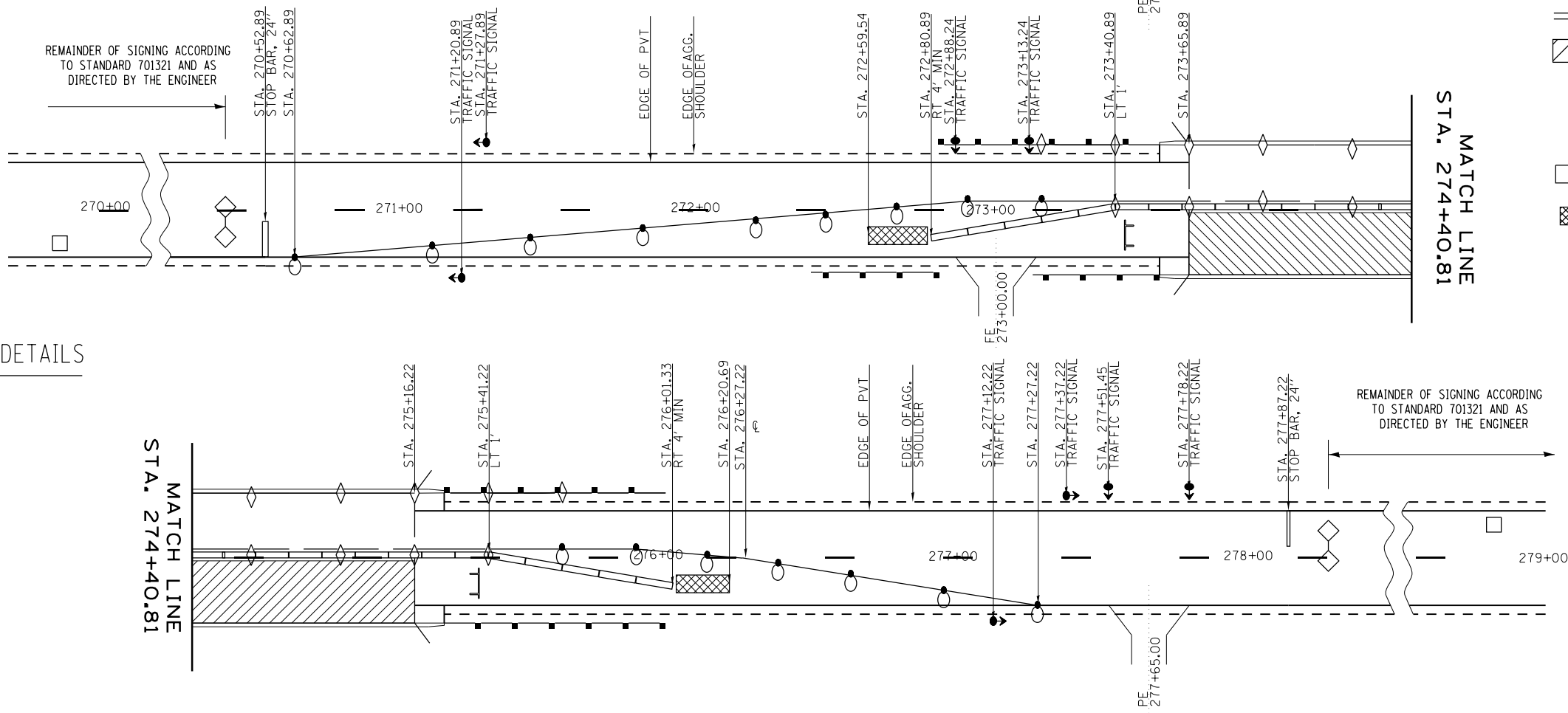
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c:\pwork\work\pwork\sparksgw\10265393\STAGING.dgn		DRAWN -	REVISED -		SCALE:	SHEET NO. 1 OF 10 SHEETS	STA. .... TO STA. ....	CONTRACT NO. 72E43				
		CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT							
		DATE -	REVISED -									



STAGE I STAGING DETAILS



STAGE II STAGING DETAILS



**SYMBOLS**

- Drum with steady burning bi-directional light
- Temporary concrete barrier
- Work area
- Type III barricade
- Traffic signal
- Detector loops
- Impact attenuator
- Crystal, bidirectional barrier wall/guardrail marker

• 104RS2,BR3;105RS7,BR3,4;106RS6

FILE NAME = c:\pwwork\pwwork\sparksgw\10265393\STAGING.dgn	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STAGING DETAILS SN 075-0080</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 48:0.3640 '1' / in.	CHECKED -	REVISED -		321	•	PIKE	69	64	CONTRACT NO. 72E43		
PLOT DATE = Oct-31-2011 03:52:39PM	DATE -	REVISED -	REVISED -	SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT			

**GENERAL NOTES**

All structural steel shall conform to AASHTO Classification M-270 Gr. 36, unless otherwise noted.

Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions.

Reinforcement bars designated (E) shall be epoxy coated.

Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

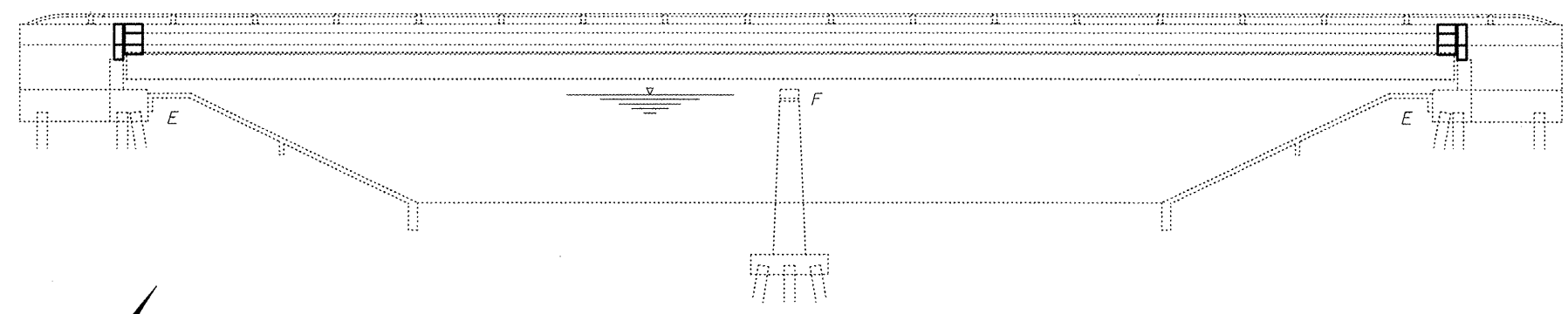
Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

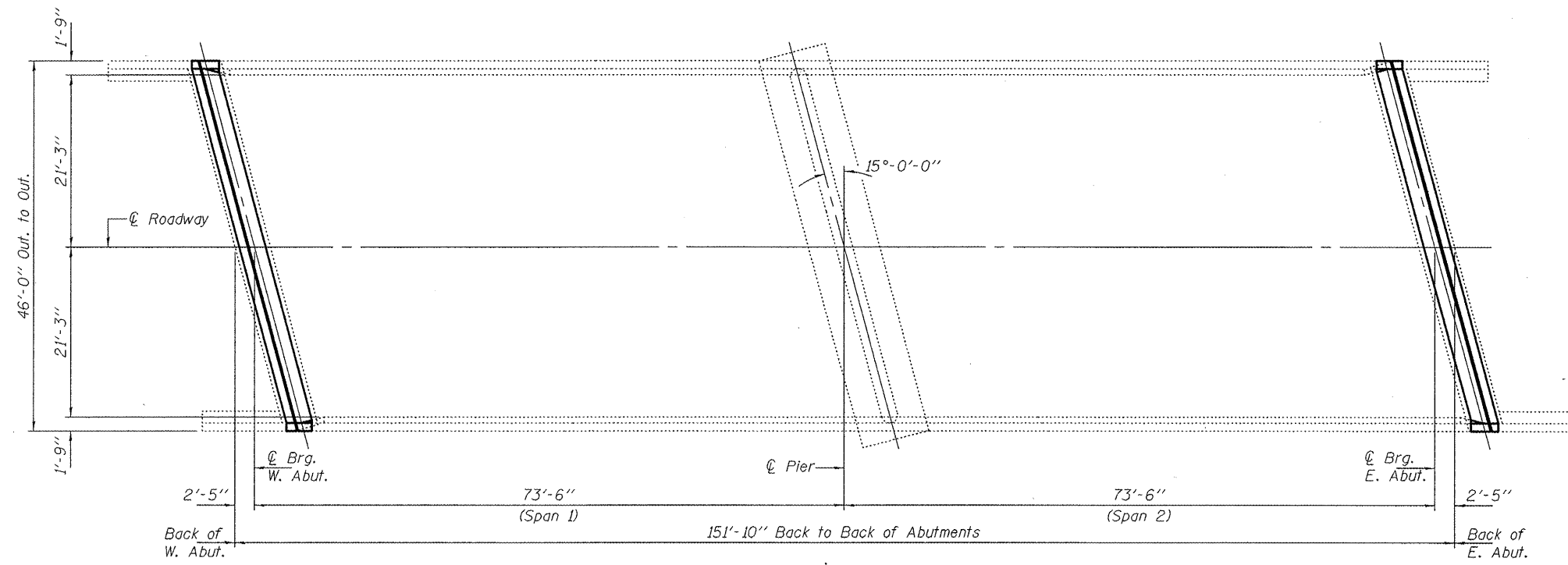
Joint openings shall be adjusted according to Article 520.04 of the Standard Specifications when the deck is poured at an ambient temperature other than 50° F.

The deck surface shall have its final finish tined according to Article 420.09(e)(1) of the Standard Specifications. Cost included with Concrete Superstructures.

The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.



**ELEVATION**



**PLAN**



Expires: November 30, 2012

DESIGNED - <i>John J. Lopez</i>	EXAMINED - <i>David Carl Puzey</i>	DATE - NOVEMBER 28, 2011
CHECKED - <i>Kyle M. Steffen</i>	PASSED - <i>David Carl Puzey</i>	
DRAWN - <i>IJL ADY</i>		

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**GENERAL PLAN & ELEVATION  
U.S. ROUTE 54 OVER AMBROSIA CREEK  
SN 075-0080**

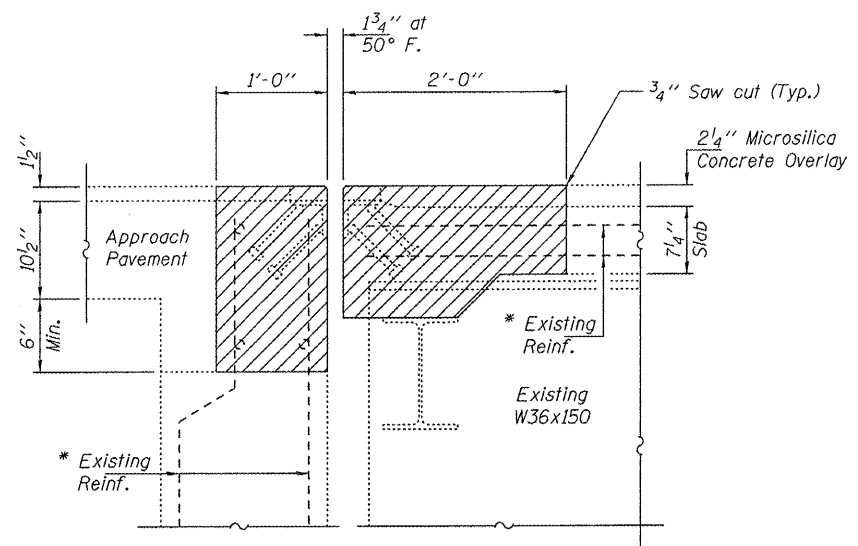
SHEET NO. 1 OF 5 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
321	104RS2, BR3;	PIKE	69	65
	105RS7, BR3, 4; 106RS6		CONTRACT NO. 72E43	
ILLINOIS FED. AID PROJECT				

**TOTAL BILL OF MATERIAL**

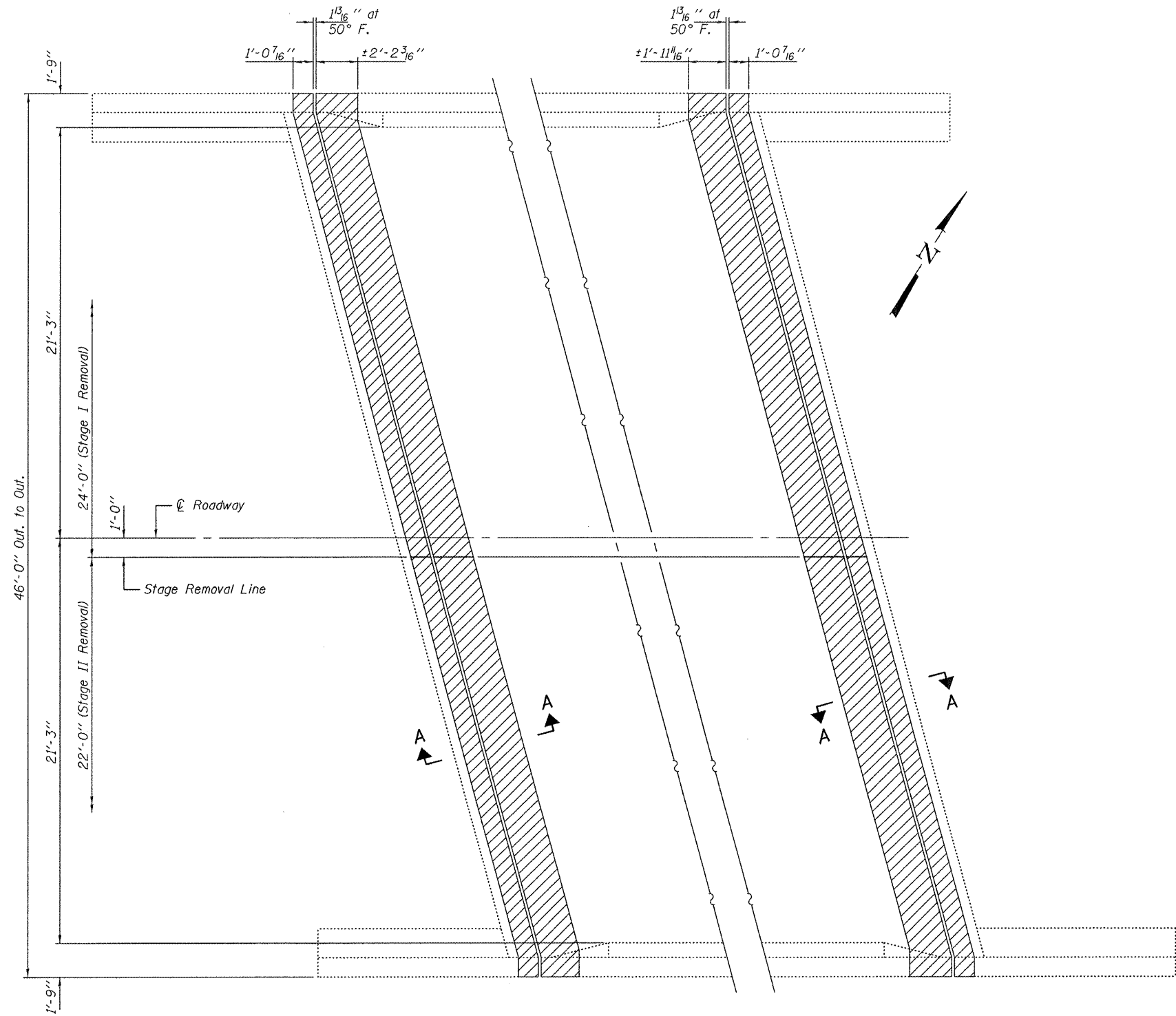
ITEM	UNIT	QUANTITY
Concrete Removal	Cu. Yd.	15.5
Concrete Superstructure	Cu. Yd.	15.4
Preformed Joint Strip Seal	Foot	93
Reinforcement Bars, Epoxy Coated	Pound	1680
Bar Splicers	Each	28
* Protective Coat	Sq. Yd.	35

\* New Concrete Areas Only.



**SECTION A-A**

\* Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.



**CONCRETE REMOVAL PLAN**

DESIGNED - IJL	EXAMINED - <i>Joanne F. [Signature]</i>	DATE - NOVEMBER 28, 2011
CHECKED - ADY	ACTING ENGINEER OF STRUCTURAL SERVICES	
DRAWN - Kyle M. Steffen	PASSED - <i>[Signature]</i>	
CHECKED - IJL ADY	ACTING ENGINEER OF BRIDGES AND STRUCTURES	

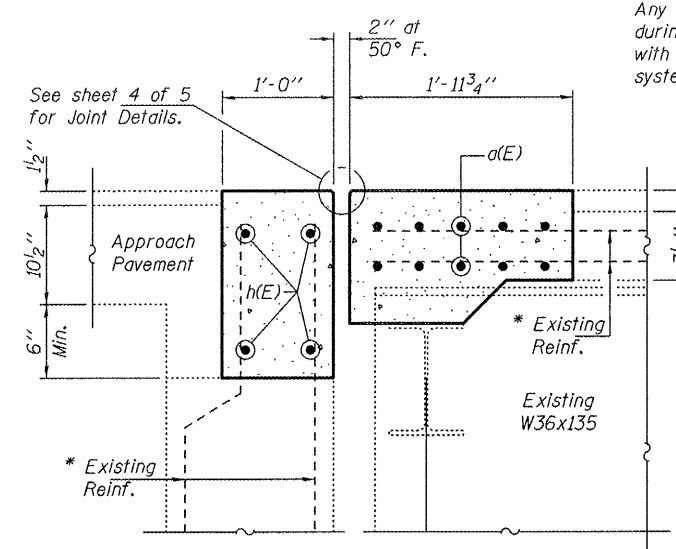
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CONCRETE REMOVAL DETAILS  
SN 075-0080**

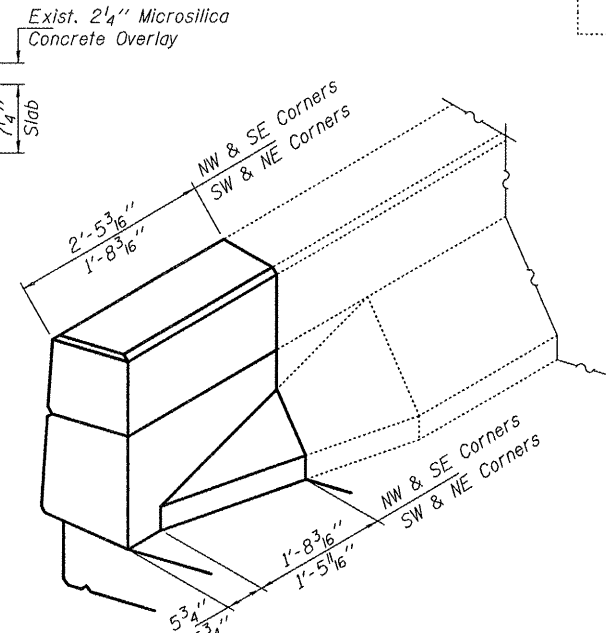
SHEET NO. 2 OF 5 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
321	104RS2, BR3;	PIKE	69	66
	105RS7, BR3, 4; 106RS6			
CONTRACT NO. 72E43				
ILLINOIS FED. AID PROJECT				

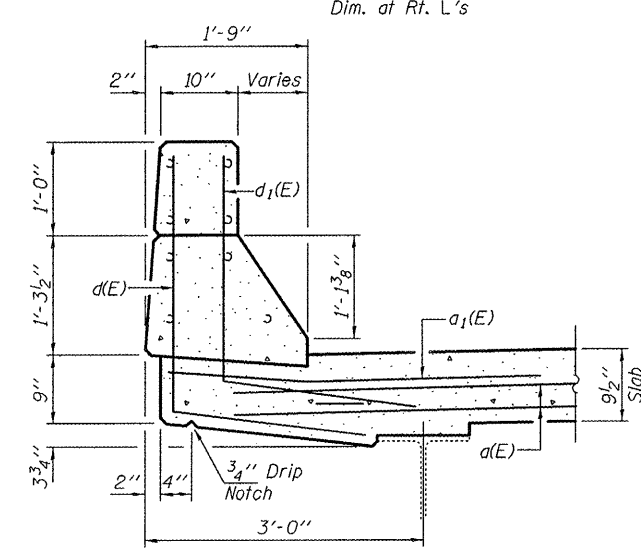
\* Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.



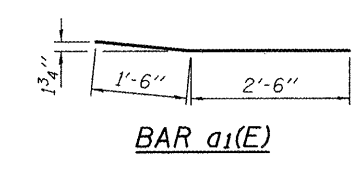
**SECTION B-B**  
Dim. at Rt. L's



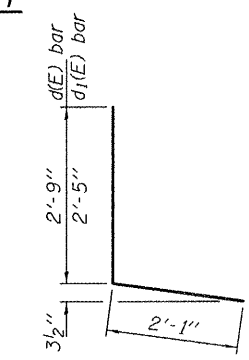
**ISOMETRIC PARAPET END DETAILS**  
Hatch Block, Railing, and Expansion Joint not shown for clarity.



**SECTION THRU PARAPET**



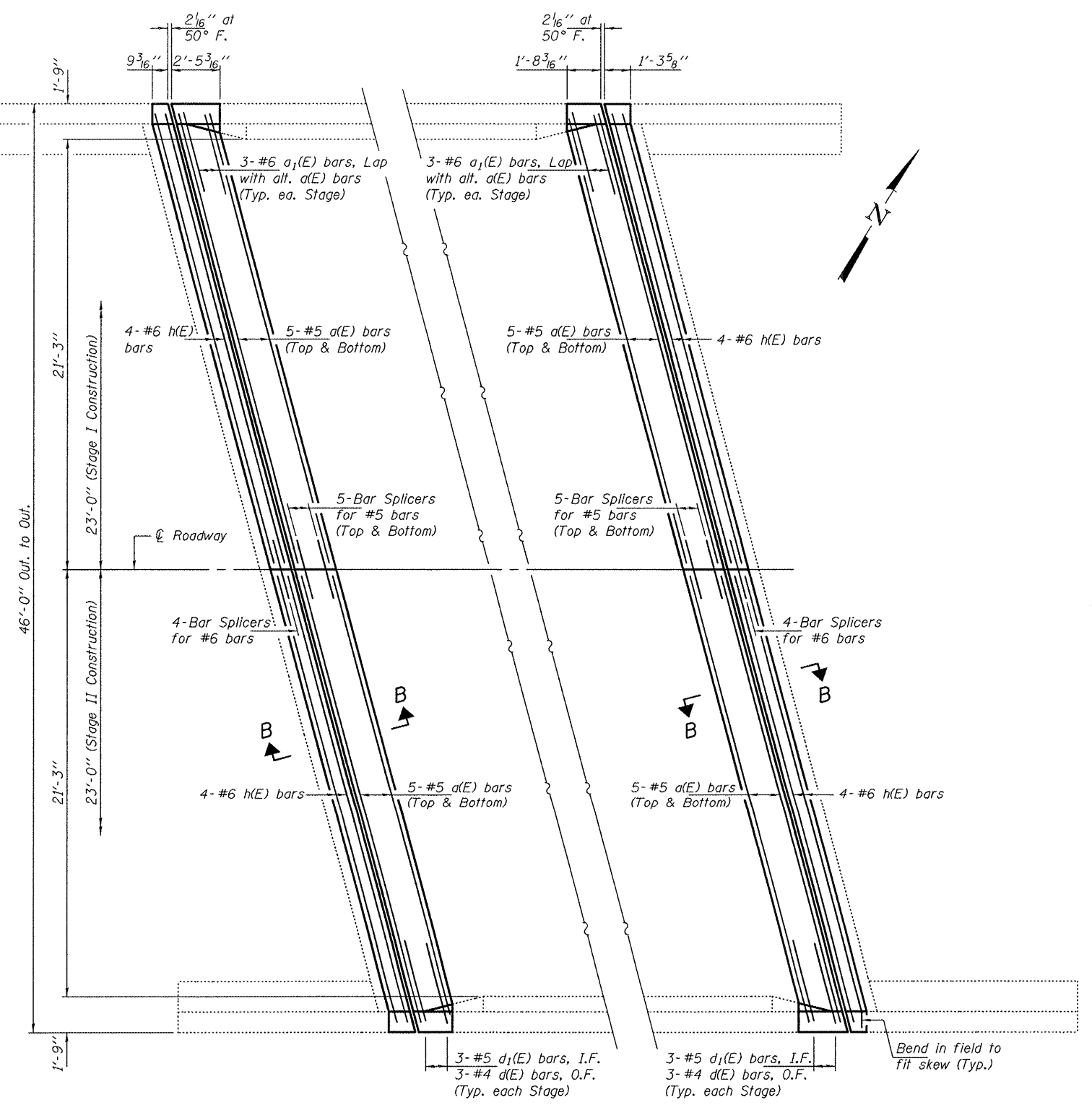
**BAR a1(E)**



**BARS d(E) & d1(E)**

**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
d(E)	40	#5	22'-9"	—
a1(E)	12	#6	4'-0"	—
d1(E)	12	#4	4'-10"	L
d1(E)	12	#5	4'-6"	L
h(E)	16	#6	23'-5"	—
Concrete Removal			Cu. Yd.	15.5
Concrete Superstructure			Cu. Yd.	15.4
Reinforcement Bars, Epoxy Coated			Pound	1680



**CONCRETE REPLACEMENT PLAN**

DESIGNED - IJL  
 CHECKED - ADY  
 DRAWN - Kyle M. Steffen  
 CHECKED - IJL ADY

EXAMINED  
 PASSED  
 ACTING ENGINEER OF STRUCTURAL SERVICES  
 ACTING ENGINEER OF BRIDGES AND STRUCTURES

DATE - NOVEMBER 28, 2011

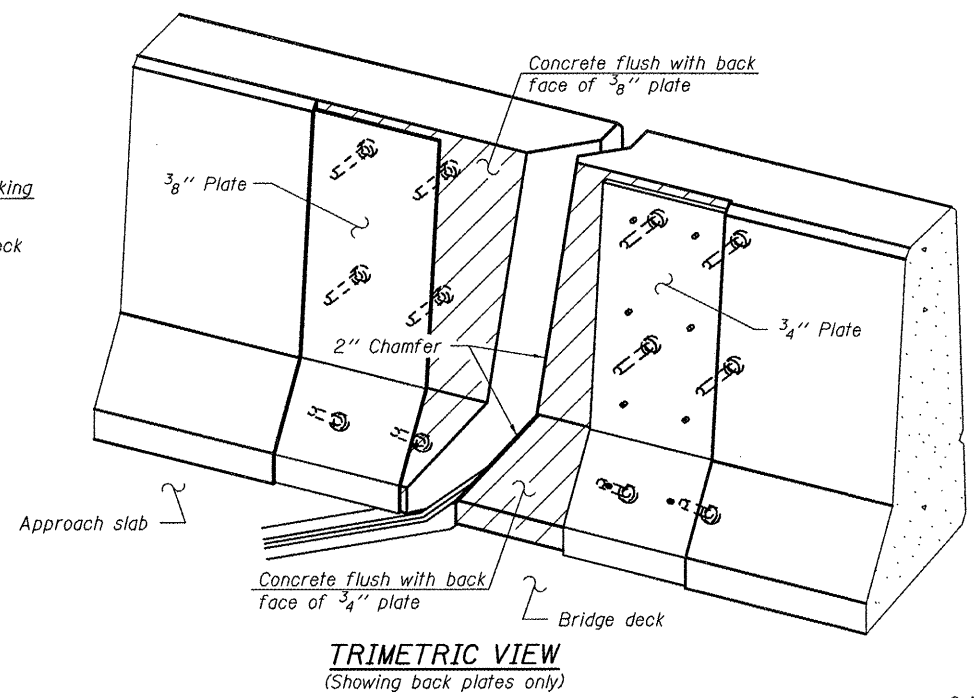
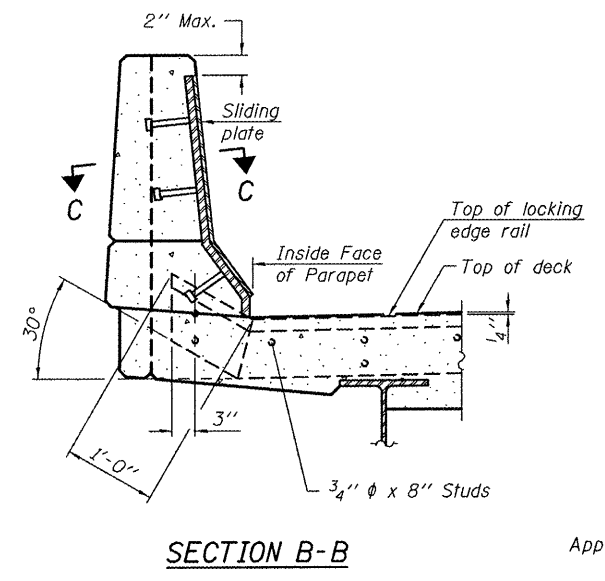
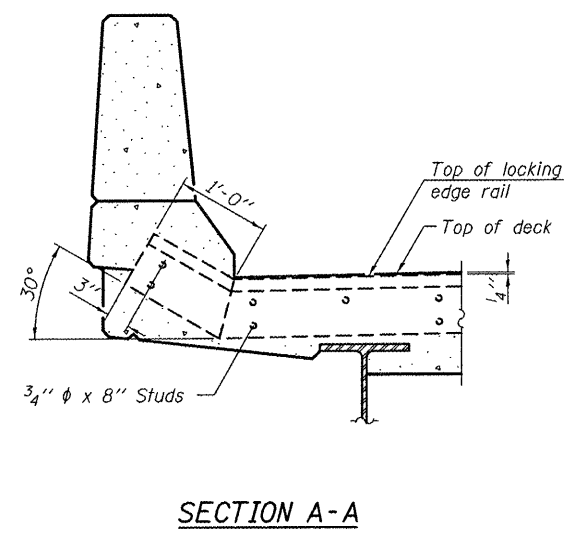
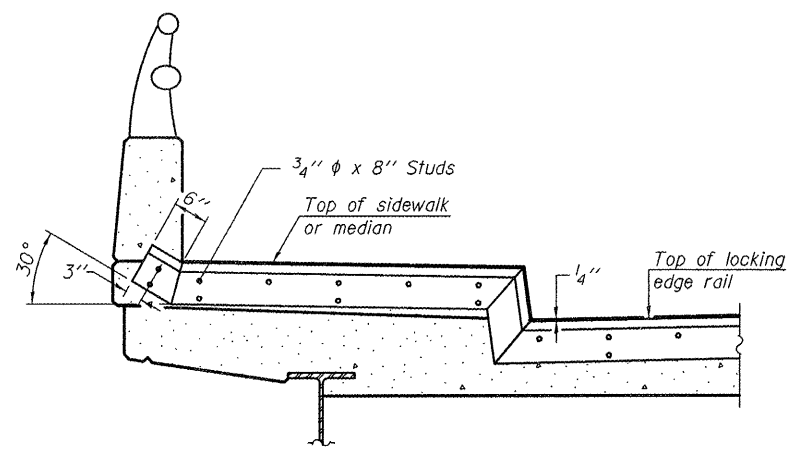
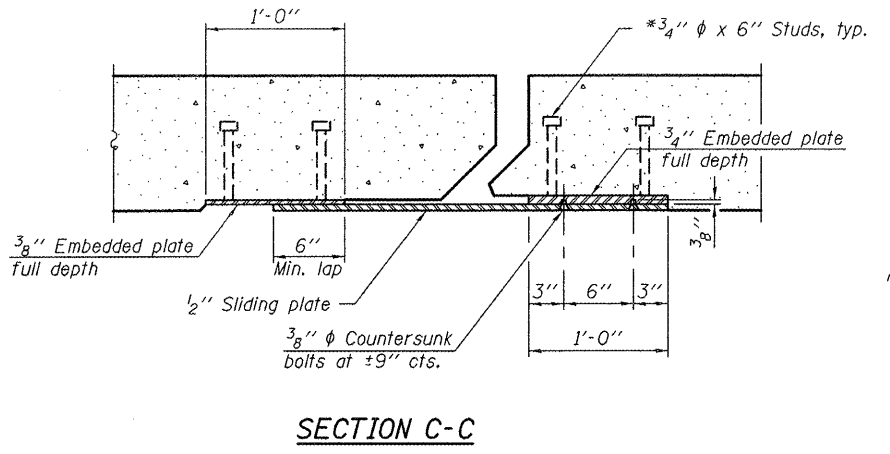
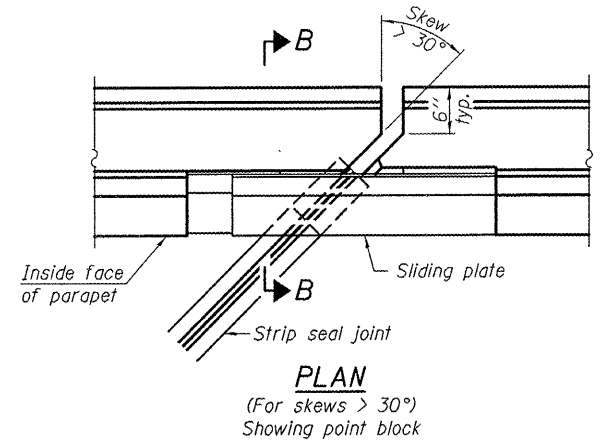
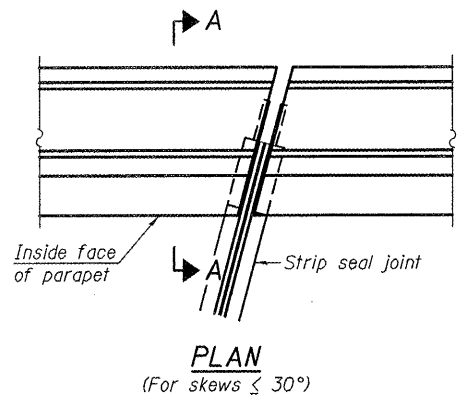
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

CONCRETE REPLACEMENT DETAILS  
 SN 075-0080

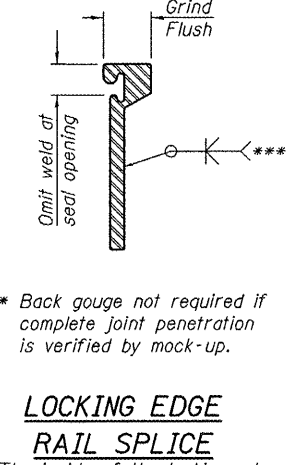
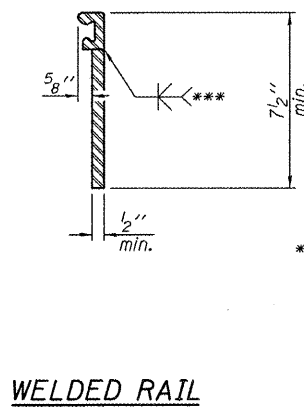
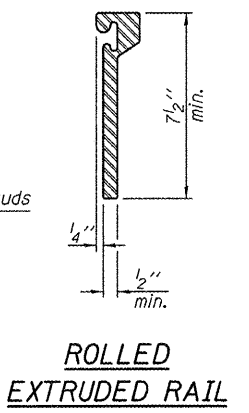
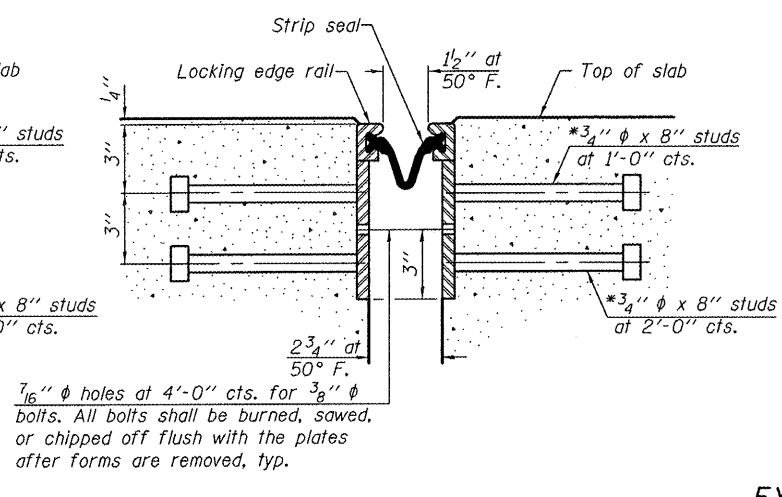
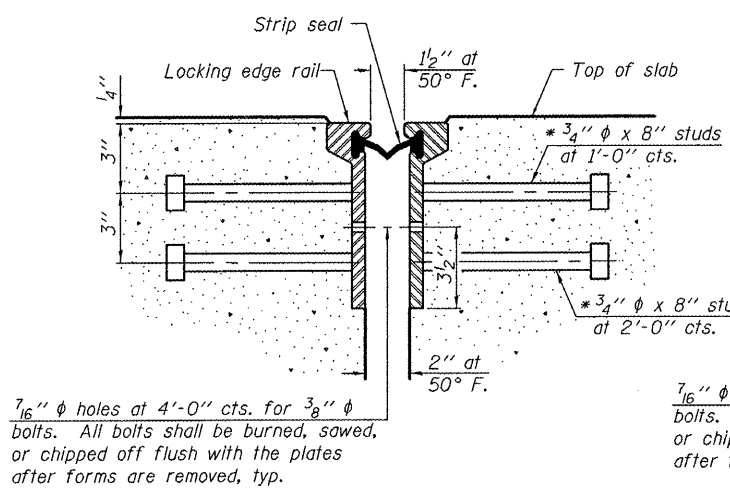
SHEET NO. 3 OF 5 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
321	104RS2, BR3;	PIKE	69	67
	105RS7, BR3, 4; 106RS6		CONTRACT NO. 72E43	
ILLINOIS FED. AID PROJECT				





**Notes:**  
The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.  
The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities.  
The manufacturer's recommended installation methods shall be followed.  
The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.  
All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications. Maximum space between rail segments at stage lines shall be 3/16", sealed with a suitable sealant.  
Parapet plates and anchorage studs for skews  $> 30^\circ$  included in the cost of Preformed Joint Strip Seal.



**SECTION THRU ROLLED RAIL JOINT**

**SECTION THRU WELDED RAIL JOINT**

**ROLLED EXTRUDED RAIL**

**WELDED RAIL**

**LOCKING EDGE RAIL SPLICE**

7/16" phi holes at 4'-0" cts. for 3/8" phi bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

7/16" phi holes at 4'-0" cts. for 3/8" phi bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

\* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

EJ-SSJ

7-1-10

DESIGNED - IJL	EXAMINED - <i>James F. Joffe</i>	DATE - NOVEMBER 28, 2011
CHECKED - ADY	ACTING ENGINEER OF STRUCTURAL SERVICES	
DRAWN - Kyle M. Steffen	PASSED - <i>Carl P. ...</i>	
CHECKED - IJL ADY	ACTING ENGINEER OF BRIDGES AND STRUCTURES	

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL DETAILS  
SN 075-0080

LOCKING EDGE RAILS

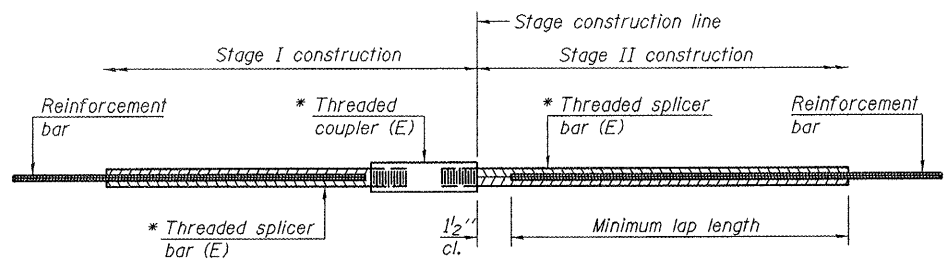
BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	93

F.A.P. RTE. 321	SECTION 104RS2, BR3; 105RS7, BR3, 4; 106RS6	COUNTY PIKE	TOTAL SHEETS 69	SHEET NO. 68
ILLINOIS FED. AID PROJECT			CONTRACT NO. 72E43	

SHEET NO. 4 OF 5 SHEETS





**STANDARD BAR SPLICER ASSEMBLY**

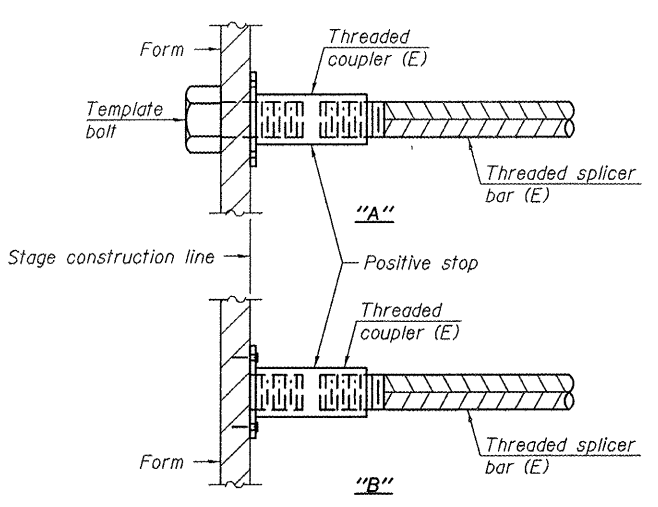
Bar size to be spliced	Minimum Lap Lengths				
	Table 1	Table 2	Table 3	Table 4	Table 5
3, 4	1'-5"	1'-11"	2'-1"	2'-4"	2'-3"
5	1'-9"	2'-5"	2'-7"	2'-11"	2'-10"
6	2'-1"	2'-11"	3'-1"	3'-6"	3'-4"
7	2'-9"	3'-10"	4'-2"	4'-8"	4'-6"
8	3'-8"	5'-1"	5'-5"	6'-2"	5'-10"
9	4'-7"	6'-5"	6'-10"	7'-9"	7'-5"

- Table 1: Black bar, 0.8 Class C
- Table 2: Black bar, Top bar lap, 0.8 Class C
- Table 3: Epoxy bar, 0.8 Class C
- Table 4: Epoxy bar, Top bar lap, 0.8 Class C
- Table 5: Epoxy bar, Top bar lap, Class B

Threaded splicer bar length = min. lap length + 1/2" + thread length

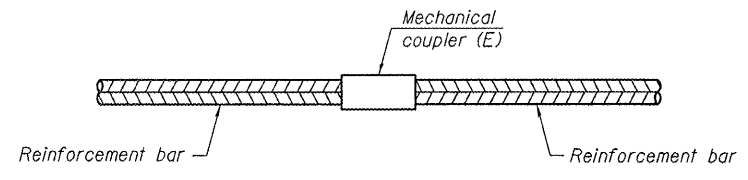
\* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length
Deck	#5	20	3
Abutments	#6	8	3



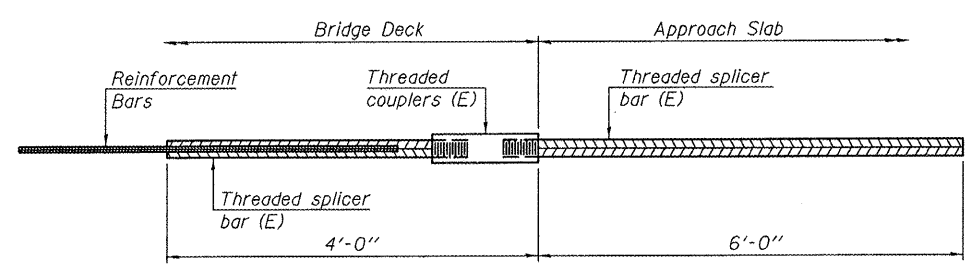
**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.  
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.  
 (E) : Indicates epoxy coating.



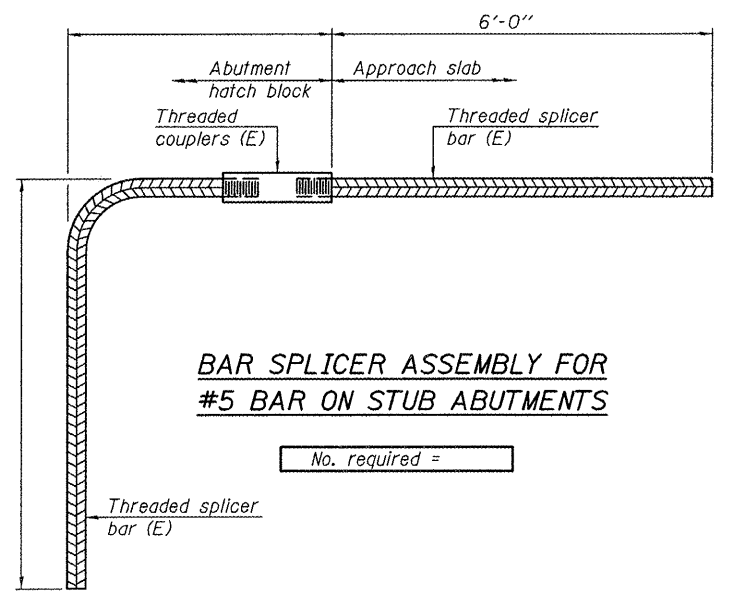
**STANDARD MECHANICAL SPLICER**

Location	Bar size	No. assemblies required



**BAR SPLICER ASSEMBLY FOR #5 BAR ON INTEGRAL OR SEMI-INTEGRAL ABUTMENTS**

No. required =



**BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS**

No. required =

**NOTES**

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.  
 All reinforcement shall be lapped and tied to the splicer bars.  
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.  
 See special provision for Mechanical Splicers.  
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

7-1-10

DESIGNED - IJL	EXAMINED - <i>Joanne F. J...</i>	DATE - NOVEMBER 28, 2011
CHECKED - ADY	PASSED - <i>J. Carl...</i>	
DRAWN - Kyle M. Steffen		
CHECKED - IJL ADY		

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS  
 SN 075-0080  
 SHEET NO. 5 OF 5 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
321	104RS2, BR3;	PIKE	69	69
	105RS7, BR3, 4; 106RS6			
CONTRACT NO. 72E43				
ILLINOIS FED. AID PROJECT				