

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	(2424&3138)RS-3	COOK	75	1
		ILLINOIS	CONTRACT NO. 62C19	

FOR INDEX OF SHEETS, SEE SHEET NO.

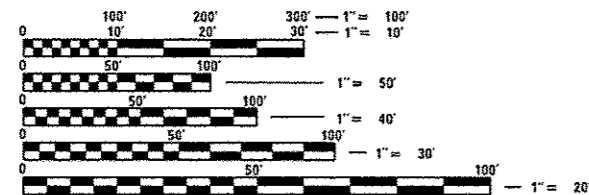
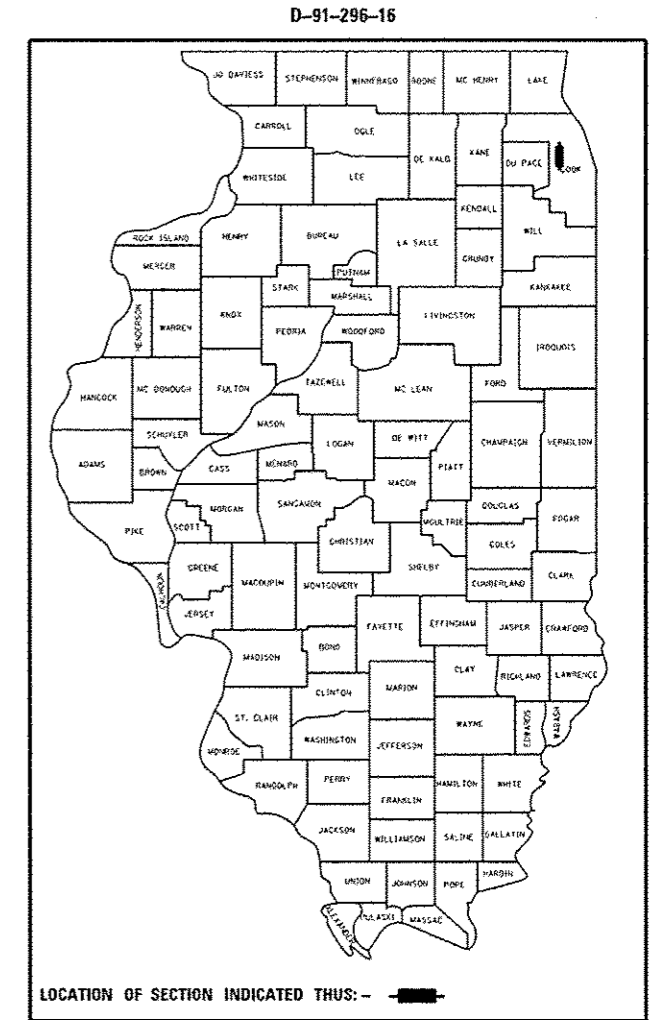
PROJECT IS LOCATED IN THE CITY OF BERWYN, THE TOWNSHIP OF LYONS AND THE VILLAGES OF RIVERSIDE, STICKNEY AND FOREST VIEW.

TRAFFIC DATA:
2015 ADT - 29,100
SPEED LIMIT - 30 MPH

**PROPOSED
HIGHWAY PLANS**

F.A.P. ROUTE 348: IL 43 (HARLEM AVE)
26TH STREET TO I-55
SECTION: (2424&3138)RS-3
PROJECT: ACNHPP-0348(056)
RESURFACING (3P), PEDESTRIAN RAMPS
COOK COUNTY

C-91-296-16

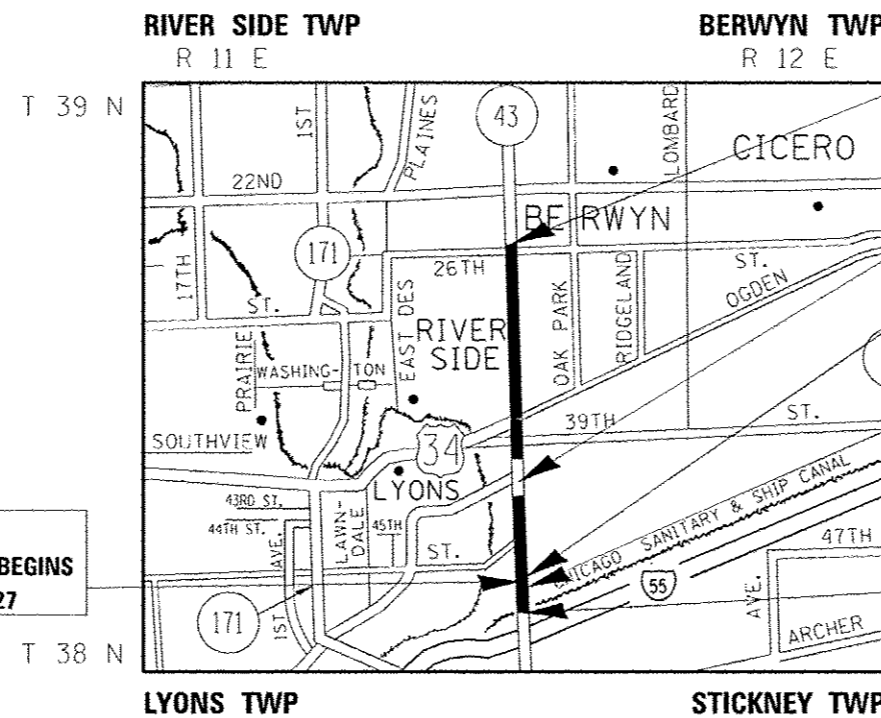


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: DANIEL WILGREEN (847)-705-4240
PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247

CONTRACT NO. 62C19



SB IL 43
PROJECT BEGINS
STA. 36+27

PROJECT ENDS
STA. 580+44

OMISSION
STA 445+50 TO STA 461+46

AH STA. 438+26.64
BK STA. 41+73.94

OMISSION
STA 34+92 TO STA 36+54

NB IL 43
PROJECT BEGINS
STA. 32+30

GROSS LENGTH (ALONG SB IL 43) = 15,161 FT = 2.871 MILES
NET LENGTH (ALONG SB IL 43) = 13,403 FT = 2.538 MILES

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED *October 27, 2016*

John Fortman
REGIONAL ENGINEER

Dee A. Addis, P.E.
ENGINEER OF DESIGN AND ENVIRONMENT

Dee A. Addis
DIRECTOR OF PROGRAM DEVELOPMENT

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OF THE STATE OF ILLINOIS

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STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-09	PERPENDICULAR CURB RAMPS
424011-03	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424021-03	DEPRESSED CORNER FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
604001-04	FRAME AND LIDS TYPE 1
604086-03	FRAME AND GRATE TYPE 23
606001-06	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
606301-04	PC CONCRETE ISLANDS AND MEDIANS
606306-04	CORRUGATED PC CONCRETE MEDIANS
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24' (600 mm) FROM PAVEMENT EDGE
701427-05	LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY, FOR SPEEDS <= 40 MPH
701602-08	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE

STATE STANDARDS (CONTINUED)

STANDARD NO.	DESCRIPTION
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701611-01	URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-06	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
728001-01	TELESCOPING STEEL SIGN SUPPORT
814001-03	HANDHOLES

GENERAL NOTES

NOTE NO.	DESCRIPTION
1	BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION AT 8-1-1 OR (800) 892-0123 FOR FIELD LOCATION OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES (48 HOUR NOTIFICATION IS REQUIRED).
2	10 FEET TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER ITEMS OF WORK TO EXISTING CURBS AND GUTTERS AND CONDITIONS IN THE FIELD UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
3	THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF BERWYN AND THE VILLAGES OF RIVERSIDE, STICKNEY AND FOREST VIEW.
4	IN VARIOUS LOCATIONS THROUGHOUT THE PROJECT, EXISTING ASPHALT OVERLAYS THE EXISTING CONCRETE GUTTER FLAG. REMOVAL OF ASPHALT ON THE GUTTER FLAG SHALL BE INCLUDED IN THE UNIT COST OF HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4 INCH.
5	THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
6	SAW CUTTING OF PAVEMENTS, SIDEWALK, CURB & GUTTER, ETC. SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEM REMOVED.
7	DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
8	THE CONTRACTOR SHALL USE CARE IN REMOVING OR EXCAVATING NEAR ALL EXISTING ITEMS WHICH WILL REMAIN. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
9	WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL, BETWEEN PASSES OF THE MILLING MACHING SHALL NOT EXCEED 1.5 INCHES WHERE THE SPEED LIMIT IS 40 MPH OR LESS. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRAD DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF MILLING IS SLOPED A MINIMUM OF 1:3 (V:H).
10	UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.
11	BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE ALL EXISTING PAVEMENT MARKING LINES AND RAISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKING SHALL BE AS DIRECTED BY THE ENGINEER.

GENERAL NOTES (CONTINUED)

NOTE NO.	DESCRIPTION
12	PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
13	THE CONTRACTOR SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO BEGINNING WORK.
14	THE RESIDENT ENGINEER SHALL CONTACT THE AREA TRAFFIC FIELD ENGINEER, PATRICE HARRIS AT PATRICE.HARRIS@ILLINOIS.GOV AT LEAST TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
15	LOCATIONS OF PAVEMENT PATCHING AND CURB AND GUTTER REMOVAL AND REPLACEMENT WILL BE AS DETERMINED BY THE ENGINEER.
16	BUTT JOINTS WILL BE INSTALLED AT THE END OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE THE "BUTT JOINT AND HMA TAPER DETAILS" INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
17	MATCH EXISTING PAVEMENT AT THE PROJECT LIMITS.
18	ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTORS EXPENSE.
19	THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.
20	THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC OR PRIVATE UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR.
21	THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MIGHT NOT BE SHOWN ON THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR.
22	ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY, PRIOR TO ACCEPTANCE OF THE IMPROVEMENT. ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WILL NOT BE PAID FOR SEPERATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.
23	EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPERATE PAY ITEM HAS BEEN PROVIDED.
24	FOR WORK OUTSIDE THE LIMITS OF THE BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB AND GUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT, SHALL BE EPOXY COATED.
25	CONTRACTOR SHALL MILL & PAVE UP TO R.R. CROSSING PANELS.

SUMMARY OF QUANTITIES			URBAN CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005 80% FED 20% STATE			
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	1098	1098			
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	550	550			
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	550	550			
* 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	6	6			
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	2262	2262			
* 89502376	REBUILD EXISTING HANDHOLE	EACH	17	17			
X4060004	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, N80	TON	6560	6560			
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	LSUM	1	1			
△ X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	500	500			
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	57	57			
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	3126	3126			
△ Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	26	26			
Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	52	52			

SUMMARY OF QUANTITIES			URBAN CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005 80% FED 20% STATE			
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	LSUM	1	1			
∅ Z0076600	TRAINEES	Hour	500	500			
∅ Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	Hour	500	500			

FILE NAME =	USER NAME = curryw	DESIGNED -	REVISED -
OFFICE =	PROJECT =	CHECKED -	REVISED -
PLOT SCALE = 1/4" = 1'		DATE -	REVISED -
PLOT DATE = 10/28/2016			

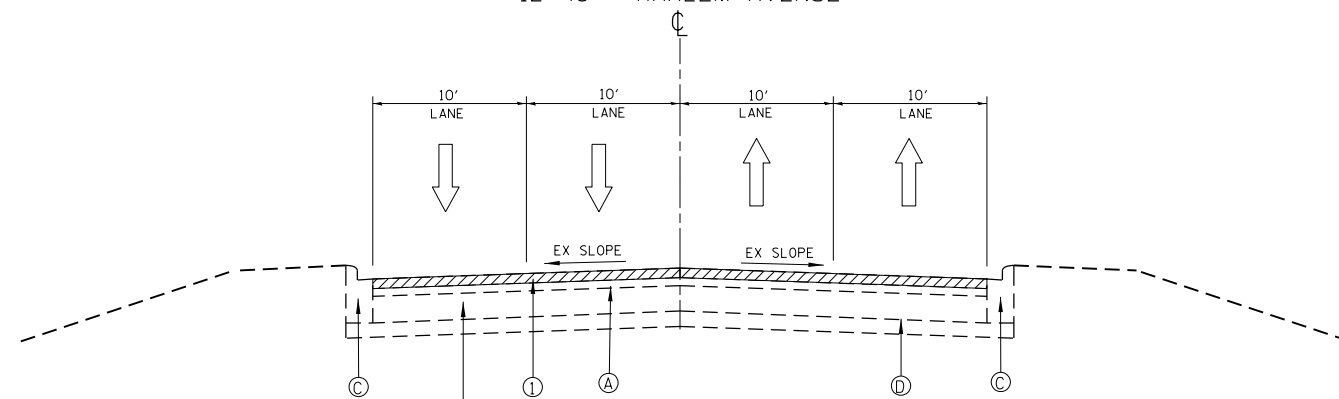
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 43 (HARLEM AVE) - 26th ST TO I-55
SUMMARY OF QUANTITIES
SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
34B	(2424&313)RS-3	COOK	75	5
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

∅ 0042
△ = NON-PARTICIPATING WORK (100% STATE)
* = SPECIALTY ITEMS

IL 43 - HARLEM AVENUE



EXISTING CROSS SECTION
STA. 440+29 TO 47+th ST

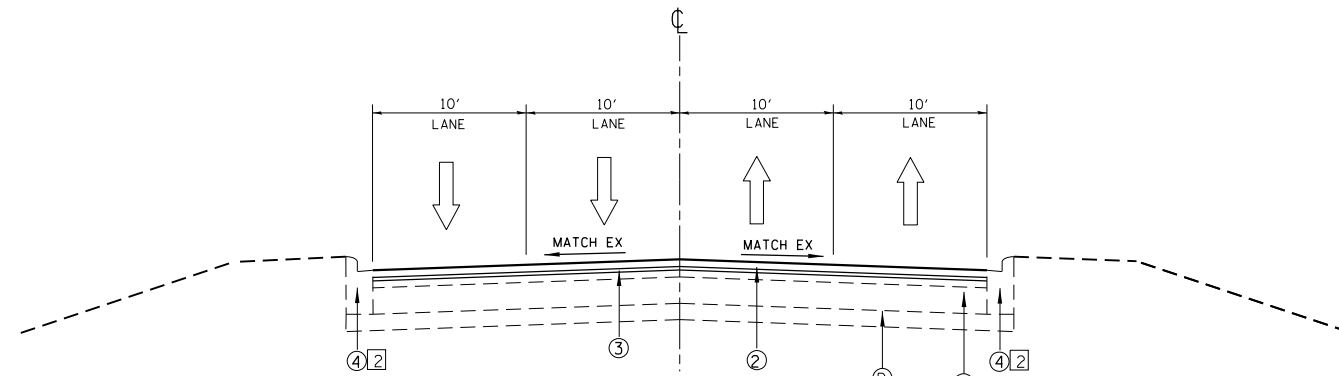
LEGEND EXISTING:

- (A) HOT-MIX ASPHALT SURFACE (BEFORE MILLING), 4 1/2"
- (B) PCC PAVEMENT, 10"
- (C) COMB. CONC. CURB & GUTTER, TYPE B-6.12
- (D) AGGREGATE SUB-BASE
- (E) HMA SHOULDER
- (F) PCC SIDEWALK

NOTES:

THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING

IL 43 - HARLEM AVENUE



PROPOSED CROSS SECTION
STA. 440+29 TO 47+th ST

LEGEND PROPOSED

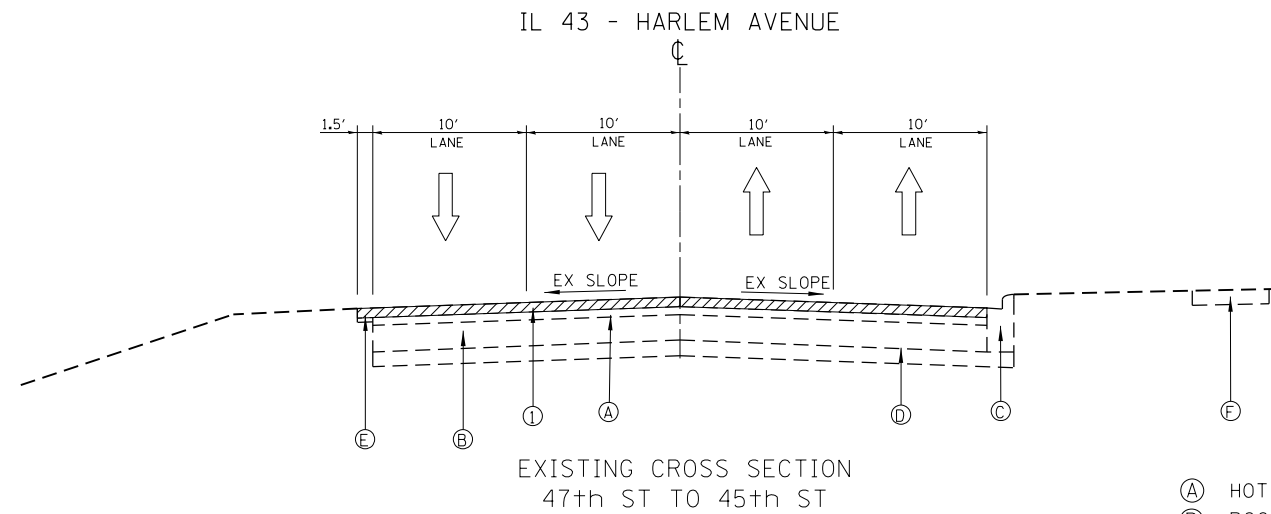
- (1) HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- (2) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA, 9.5, N80, 1 3/4"
- (3) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- (4) COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- (5) HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"

(2) = LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)), WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER/TECHNICIAN.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes	QUALITY MANAGEMENT PROGRAM (QMP)
PAVEMENT RESURFACING		
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA, 9.5, N80	3.5% AT 80 GYR.	PFP
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	3.5% AT 50 GYR.	QCP
DRIVEWAYS		
HMA SURFACE COURSE, MIX D, N 50 (IL 9.5 mm); 2"	4% AT 50 GYR.	QC/OA
HMA BASE COURSE (HMA BINDER IL-19 mm); PE -6", CE - 8"	4% AT 50 GYR.	QC/OA
PATCHING		
CLASS D PATCHES (HMA BINDER IL-19 mm)	4% AT 70 GYR.	QC/OA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/OA); QUALITY CONTROL FOR PERFORMANCE (QCP)		

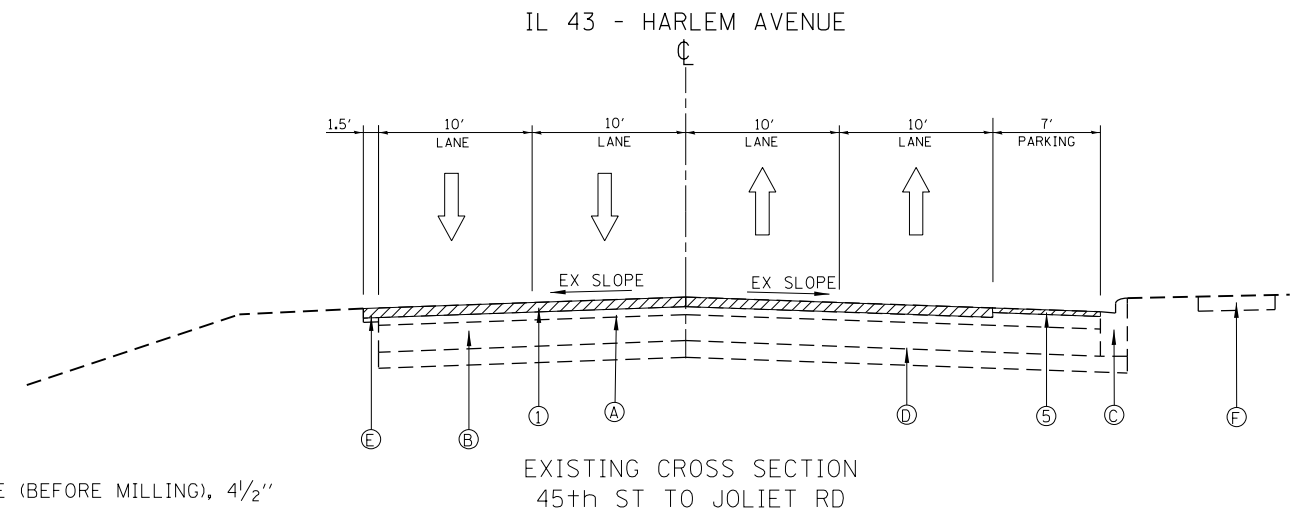
- NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
- NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.
- NOTE 3: QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.



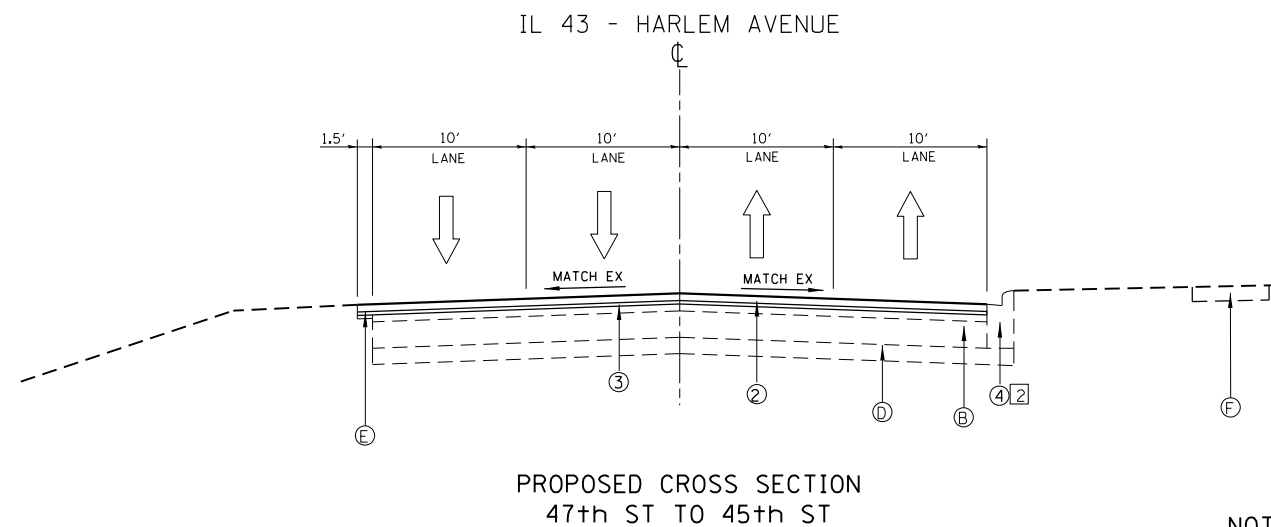
EXISTING CROSS SECTION
47th ST TO 45th ST

LEGND EXISTING:

- Ⓐ HOT-MIX ASPHALT SURFACE (BEFORE MILLING), 4 1/2"
- Ⓑ PCC PAVEMENT, 10"
- Ⓒ COMB. CONC. CURB & GUTTER, TYPE B-6.12
- Ⓓ AGGREGATE SUB-BASE
- Ⓔ HMA SHOULDER
- Ⓕ PCC SIDEWALK



EXISTING CROSS SECTION
45th ST TO JOLIET RD



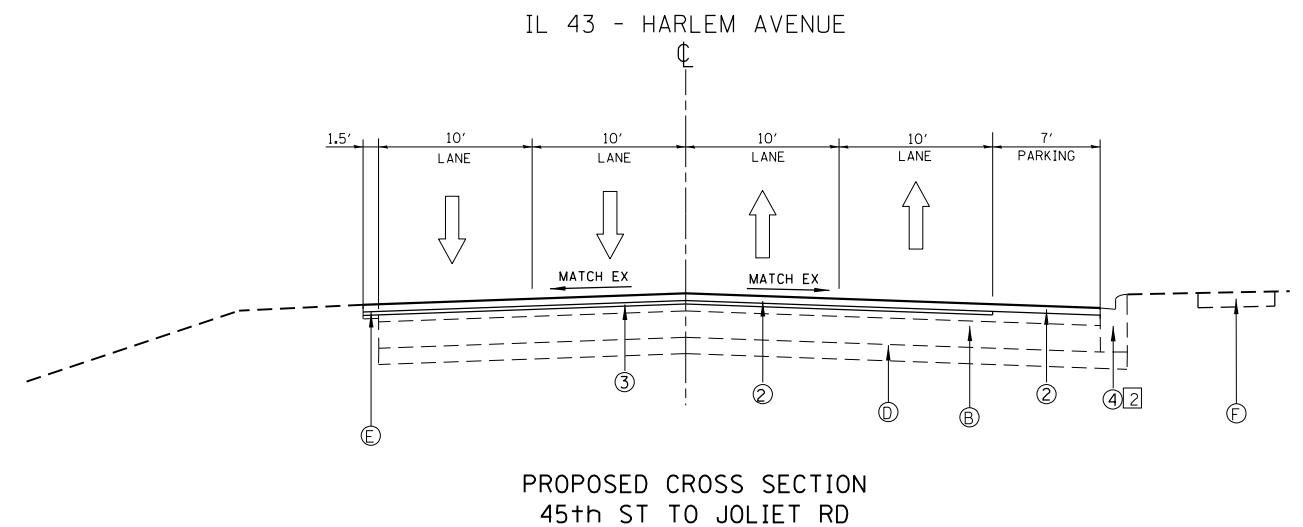
PROPOSED CROSS SECTION
47th ST TO 45th ST

NOTES:

THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING

LEGEND PROPOSED

- ① HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- ② POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA, 9.5, N80, 1 3/4"
- ③ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ④ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- ⑤ HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"



PROPOSED CROSS SECTION
45th ST TO JOLIET RD

② = LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)), WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER/TECHNICIAN.

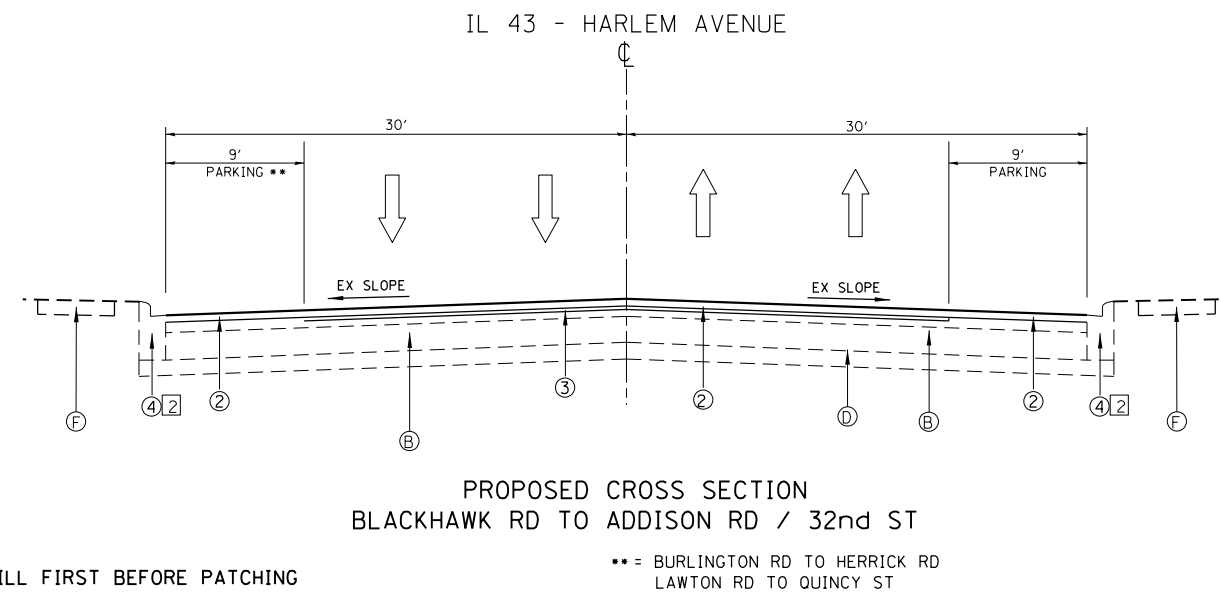
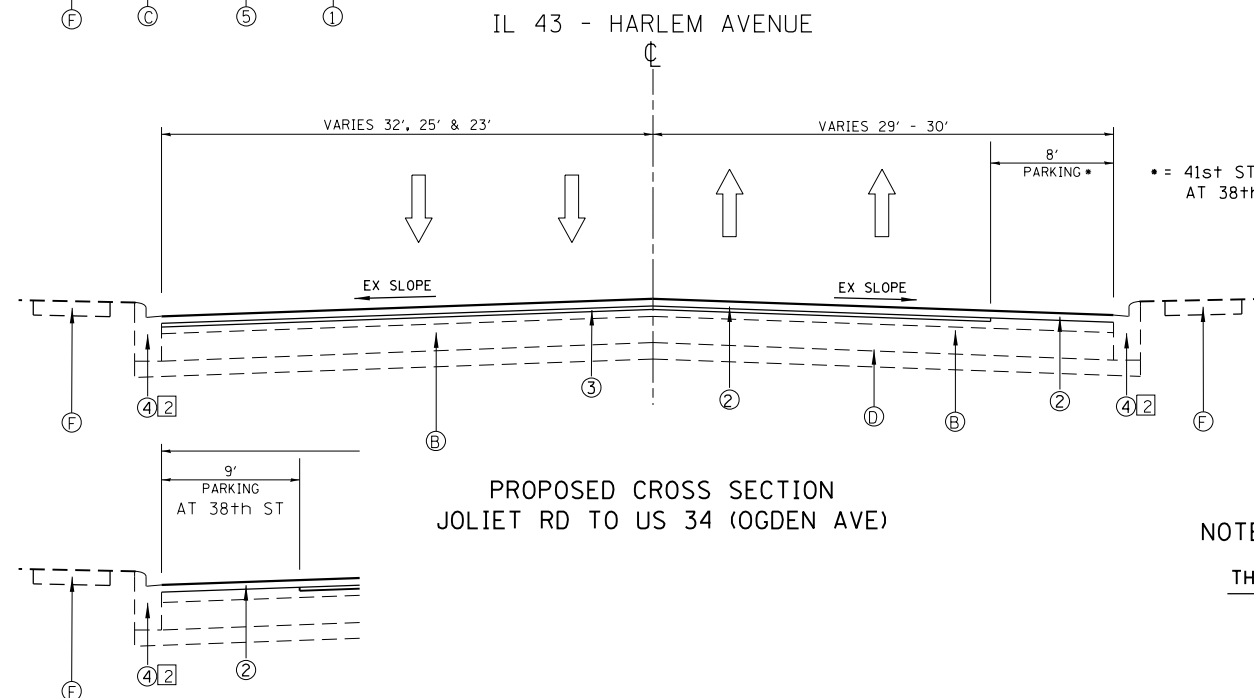
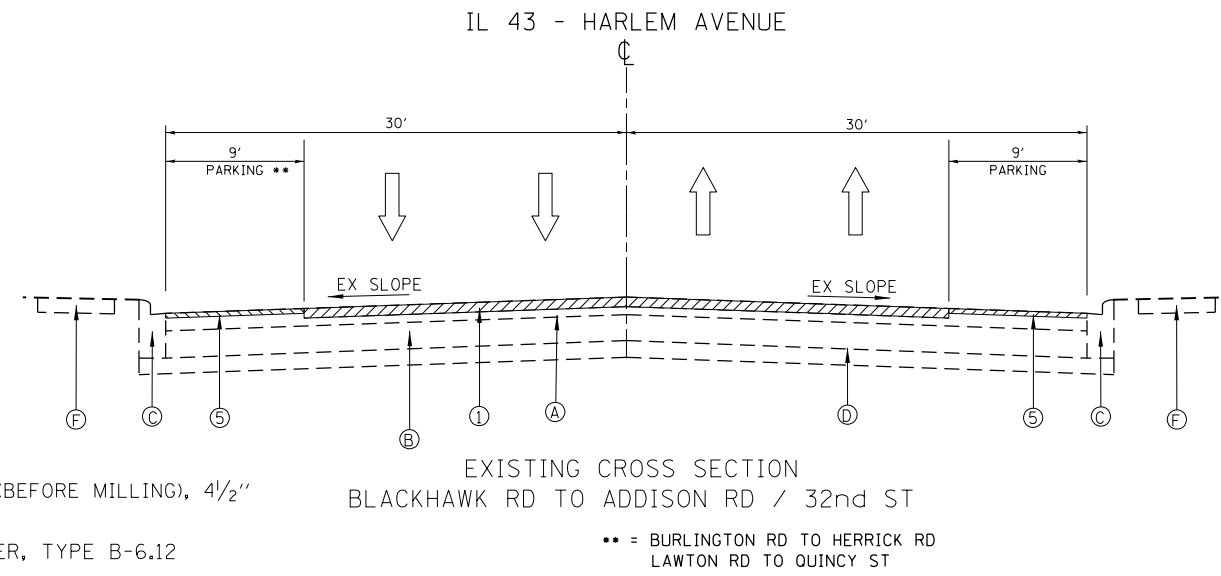
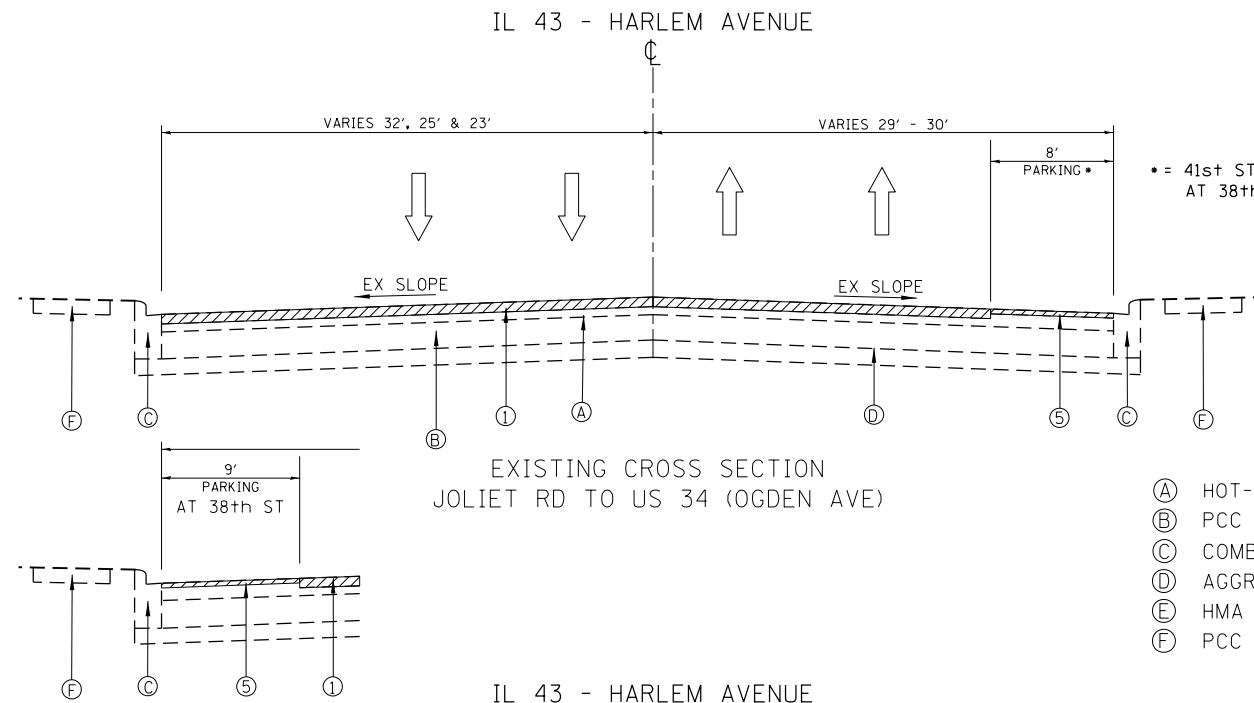
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	PLOT DATE = 10/28/2016	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL 43 (HARLEM AVE) - 26th ST TO I-55
TYPICAL SECTIONS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	(2424&3138)RS-3	COOK	75	7
CONTRACT NO. 62C19			ILLINOIS FED. AID PROJECT	



NOTES:

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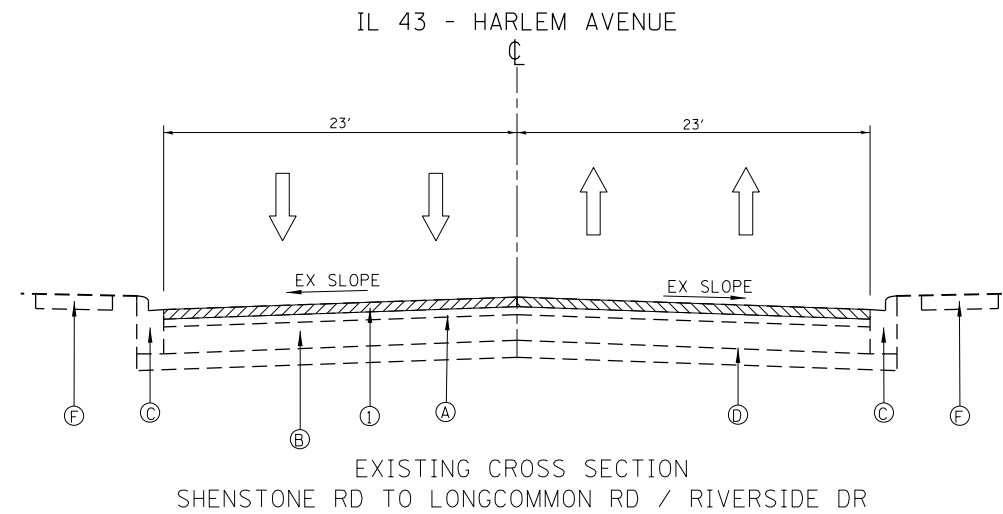
LEGEND PROPOSED

- ① HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- ② POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA, 9.5, N80, 1 3/4"
- ③ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ④ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- ⑤ HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"

② = LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT [OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)], WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER/TECHNICIAN.

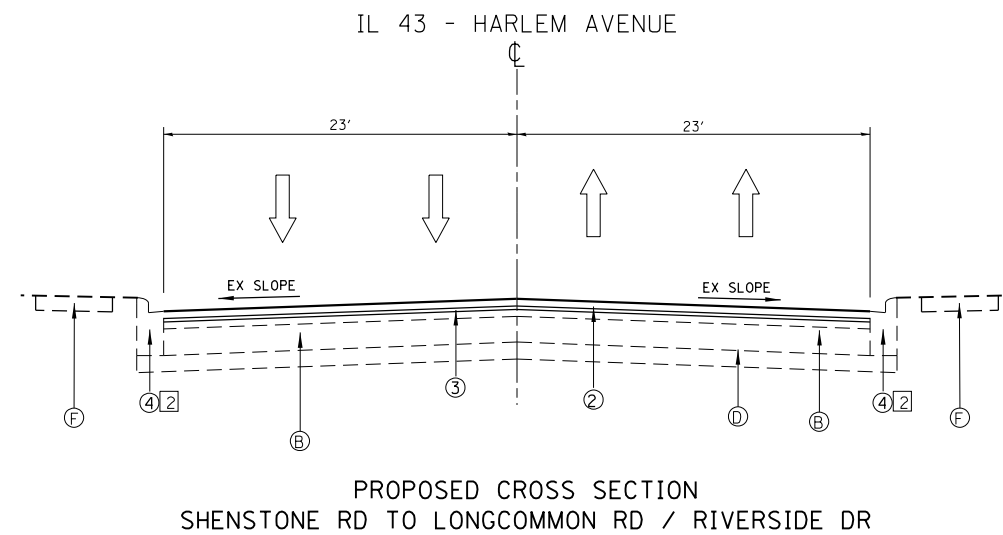
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FILE NAME =	USER NAME = curryw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL 43 (HARLEM AVE) - 26th ST TO I-55 TYPICAL SECTIONS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\IL\084EBIDINTEG\illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\DI29616\Design\DI29616-sh-typical.dgn		CHECKED -	REVISED -			348	(2424&3138)RS-3	COOK	75	8	
Default	PLOT SCALE = 100.0000' / in.	DATE -	REVISED -			CONTRACT NO. 62C19			ILLINOIS FED. AID PROJECT		
	PLOT DATE = 10/28/2016					SCALE:	SHEET	OF	SHEETS	STA.	TO



LEGND EXISTING:

- Ⓐ HOT-MIX ASPHALT SURFACE (BEFORE MILLING), 4 1/2"
- Ⓑ PCC PAVEMENT, 10"
- Ⓒ COMB. CONC. CURB & GUTTER, TYPE B-6.12
- Ⓓ AGGREGATE SUB-BASE
- Ⓔ HMA SHOULDER
- Ⓕ PCC SIDEWALK



NOTES:

THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING

LEGEND PROPOSED

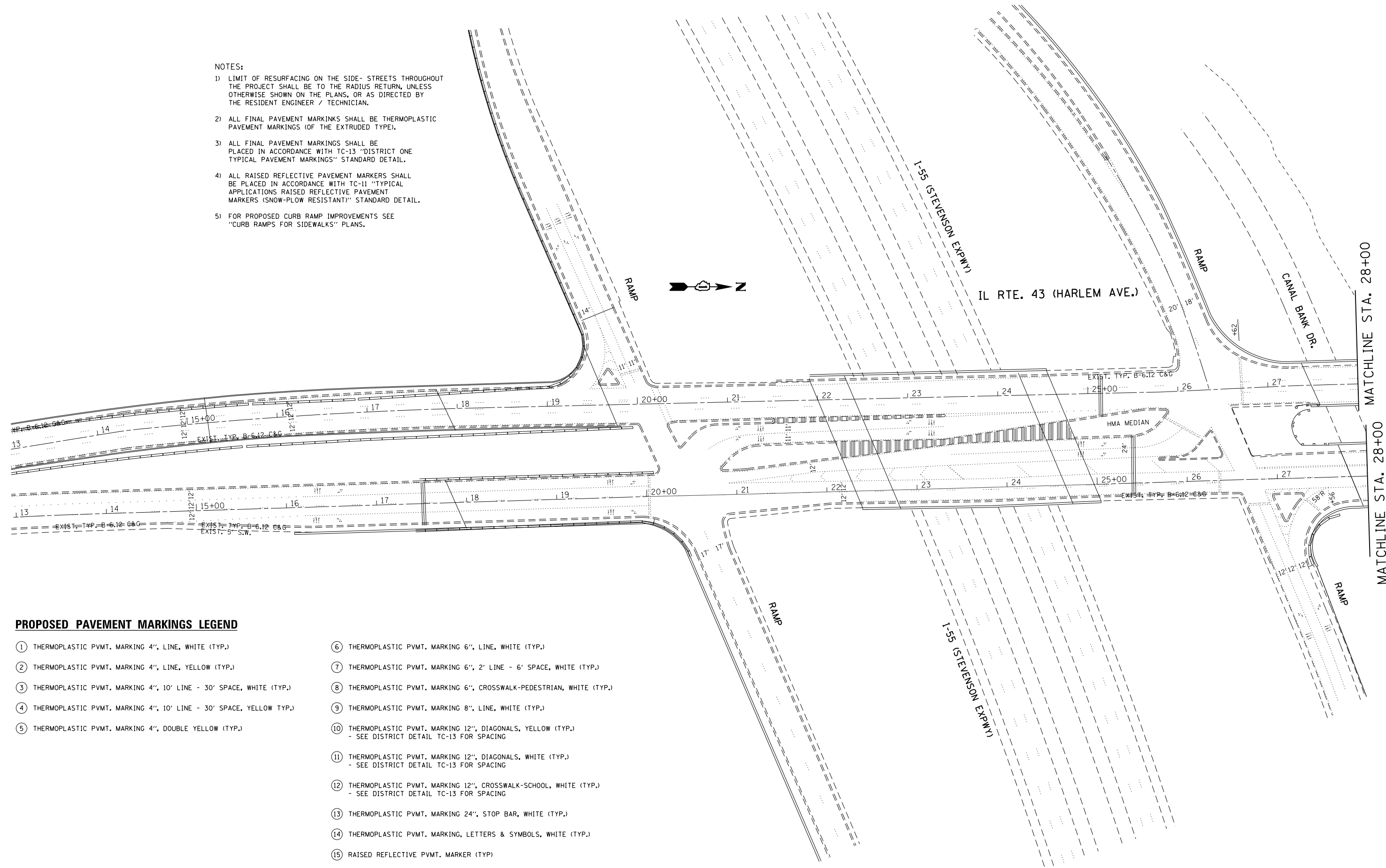
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- ② POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA, 9.5, N80, 1 3/4"
- ③ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ④ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- ⑤ HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"

② = LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)), WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER/TECHNICIAN.

FILE NAME =	USER NAME = curryw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL 43 (HARLEM AVE) - 26th ST TO I-55 TYPICAL SECTIONS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\IL\084EBIDINTEG\illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\DI29616\Design\DI29616-sh-typical.dgn		REVISIONS				348	(2424&3138)RS-3	COOK	75	9	
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 62C19		ILLINOIS FED. AID PROJECT			
	PLOT DATE = 10/28/2016	DATE -	REVISED -			SCALE:	SHEET OF SHEETS	STA. TO STA.			

NOTES:

- 1) LIMIT OF RESURFACING ON THE SIDE- STREETS THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE SHOWN ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER / TECHNICIAN.
- 2) ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC PAVEMENT MARKINGS (OF THE EXTRUDED TYPE).
- 3) ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH TC-13 "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" STANDARD DETAIL.
- 4) ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH TC-11 "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" STANDARD DETAIL.
- 5) FOR PROPOSED CURB RAMP IMPROVEMENTS SEE "CURB RAMPS FOR SIDEWALKS" PLANS.



PROPOSED PAVEMENT MARKINGS LEGEND

- | | |
|--|--|
| ① THERMOPLASTIC PAVT. MARKING 4", LINE, WHITE (TYP.) | ⑥ THERMOPLASTIC PAVT. MARKING 6", LINE, WHITE (TYP.) |
| ② THERMOPLASTIC PAVT. MARKING 4", LINE, YELLOW (TYP.) | ⑦ THERMOPLASTIC PAVT. MARKING 6", 2' LINE - 6' SPACE, WHITE (TYP.) |
| ③ THERMOPLASTIC PAVT. MARKING 4", 10' LINE - 30' SPACE, WHITE (TYP.) | ⑧ THERMOPLASTIC PAVT. MARKING 6", CROSSWALK-PEDESTRIAN, WHITE (TYP.) |
| ④ THERMOPLASTIC PAVT. MARKING 4", 10' LINE - 30' SPACE, YELLOW TYP.) | ⑨ THERMOPLASTIC PAVT. MARKING 8", LINE, WHITE (TYP.) |
| ⑤ THERMOPLASTIC PAVT. MARKING 4", DOUBLE YELLOW (TYP.) | ⑩ THERMOPLASTIC PAVT. MARKING 12", DIAGONALS, YELLOW (TYP.)
- SEE DISTRICT DETAIL TC-13 FOR SPACING |
| | ⑪ THERMOPLASTIC PAVT. MARKING 12", DIAGONALS, WHITE (TYP.)
- SEE DISTRICT DETAIL TC-13 FOR SPACING |
| | ⑫ THERMOPLASTIC PAVT. MARKING 12", CROSSWALK-SCHOOL, WHITE (TYP.)
- SEE DISTRICT DETAIL TC-13 FOR SPACING |
| | ⑬ THERMOPLASTIC PAVT. MARKING 24", STOP BAR, WHITE (TYP.) |
| | ⑭ THERMOPLASTIC PAVT. MARKING, LETTERS & SYMBOLS, WHITE (TYP.) |
| | ⑮ RAISED REFLECTIVE PAVT. MARKER (TYP.) |

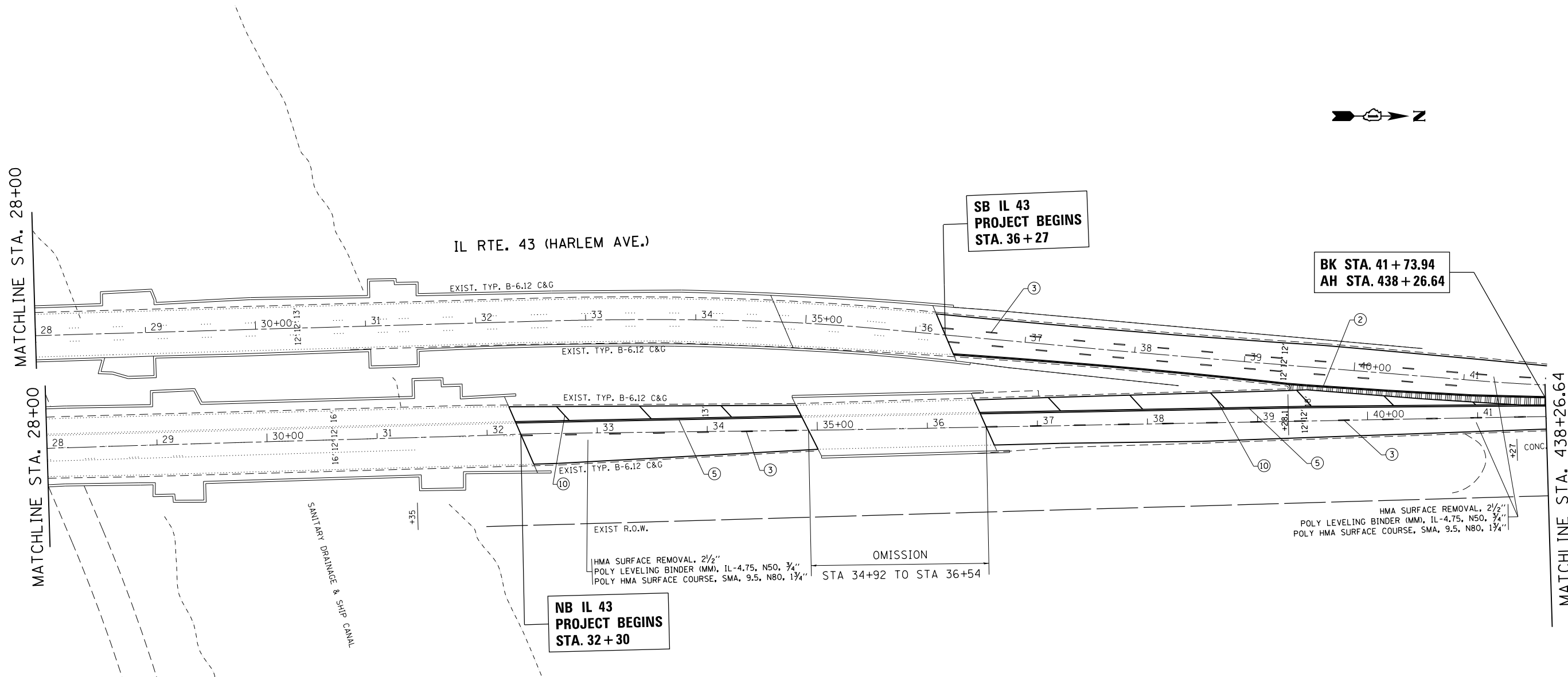
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Default	PLOT DATE = 10/28/2016	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL 43 (HARLEM AVE) - 26th ST TO I-55
ROADWAY PLAN**

STA.: 1''=50' SHEET OF SHEETS STA. 26+51.6 TO STA. 28+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	(2424&3138)RS-3	COOK	75	10
CONTRACT NO. 62C19				
ILLINOIS FED. AID PROJECT				



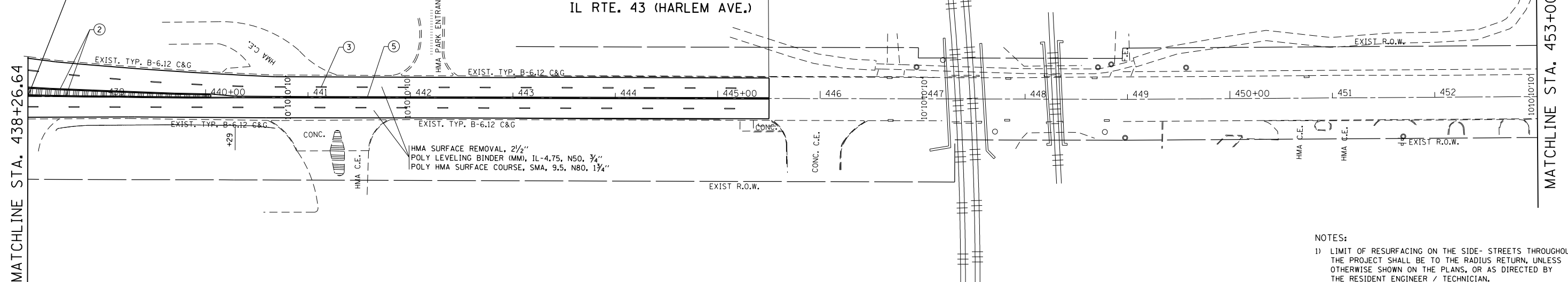
PROPOSED PAVEMENT MARKINGS LEGEND

- ① THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.)
- ② THERMOPLASTIC PVMT. MARKING 4", LINE, YELLOW (TYP.)
- ③ THERMOPLASTIC PVMT. MARKING 4", 10' LINE - 30' SPACE, WHITE (TYP.)
- ④ THERMOPLASTIC PVMT. MARKING 4", 10' LINE - 30' SPACE, YELLOW TYP.)
- ⑤ THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.)
- ⑥ THERMOPLASTIC PVMT. MARKING 6", LINE, WHITE (TYP.)
- ⑦ THERMOPLASTIC PVMT. MARKING 6", 2' LINE - 6' SPACE, WHITE (TYP.)
- ⑧ THERMOPLASTIC PVMT. MARKING 6", CROSSWALK-PEDESTRIAN, WHITE (TYP.)
- ⑨ THERMOPLASTIC PVMT. MARKING 8", LINE, WHITE (TYP.)
- ⑩ THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.) - SEE DISTRICT DETAIL TC-13 FOR SPACING
- ⑪ THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, WHITE (TYP.) - SEE DISTRICT DETAIL TC-13 FOR SPACING
- ⑫ THERMOPLASTIC PVMT. MARKING 12", CROSSWALK-SCHOOL, WHITE (TYP.) - SEE DISTRICT DETAIL TC-13 FOR SPACING
- ⑬ THERMOPLASTIC PVMT. MARKING 24", STOP BAR, WHITE (TYP.)
- ⑭ THERMOPLASTIC PVMT. MARKING, LETTERS & SYMBOLS, WHITE (TYP.)
- ⑮ RAISED REFLECTIVE PVMT. MARKER (TYP.)

- NOTES:**
- 1) LIMIT OF RESURFACING ON THE SIDE- STREETS THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE SHOWN ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER / TECHNICIAN.
 - 2) ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC PAVEMENT MARKINGS (OF THE EXTRUDED TYPE).
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 - 5) FOR PROPOSED CURB RAMP IMPROVEMENTS SEE "CURB RAMP FOR SIDEWALKS" PLANS.

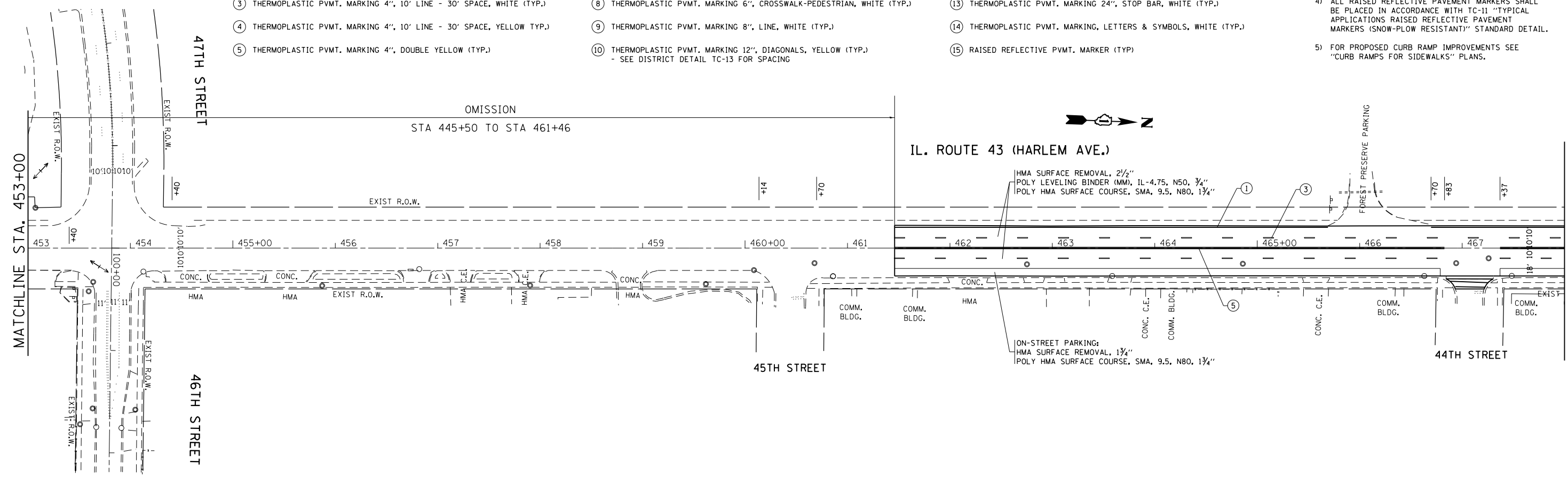
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						348	(2424&3138)RS-3	COOK	75	11	
						CONTRACT NO. 62C19					
Default	PLOT DATE = 10/28/2016	DATE -	REVISED -			STA.: 1"=50'	SHEET OF SHEETS	STA. 28+00	TO STA. 438+26.64		ILLINOIS FED. AID PROJECT

BK STA. 41 + 73.94
AH STA. 438 + 26.64

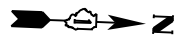


PROPOSED PAVEMENT MARKINGS LEGEND

- | | | | |
|--|--|--|---|
| ① THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.) | ⑥ THERMOPLASTIC PVMT. MARKING 6", LINE, WHITE (TYP.) | ⑪ THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, WHITE (TYP.)
- SEE DISTRICT DETAIL TC-13 FOR SPACING | 4) ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH TC-11 "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" STANDARD DETAIL. |
| ② THERMOPLASTIC PVMT. MARKING 4", LINE, YELLOW (TYP.) | ⑦ THERMOPLASTIC PVMT. MARKING 6", 2' LINE - 6' SPACE, WHITE (TYP.) | ⑫ THERMOPLASTIC PVMT. MARKING 12", CROSSWALK-SCHOOL, WHITE (TYP.)
- SEE DISTRICT DETAIL TC-13 FOR SPACING | 5) FOR PROPOSED CURB RAMP IMPROVEMENTS SEE "CURB RAMP FOR SIDEWALKS" PLANS. |
| ③ THERMOPLASTIC PVMT. MARKING 4", 10' LINE - 30' SPACE, WHITE (TYP.) | ⑧ THERMOPLASTIC PVMT. MARKING 6", CROSSWALK-PEDESTRIAN, WHITE (TYP.) | ⑬ THERMOPLASTIC PVMT. MARKING 24", STOP BAR, WHITE (TYP.) | |
| ④ THERMOPLASTIC PVMT. MARKING 4", 10' LINE - 30' SPACE, YELLOW TYP. | ⑨ THERMOPLASTIC PVMT. MARKING 8", LINE, WHITE (TYP.) | ⑭ THERMOPLASTIC PVMT. MARKING, LETTERS & SYMBOLS, WHITE (TYP.) | |
| ⑤ THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.) | ⑩ THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.)
- SEE DISTRICT DETAIL TC-13 FOR SPACING | ⑮ RAISED REFLECTIVE PVMT. MARKER (TYP.) | |



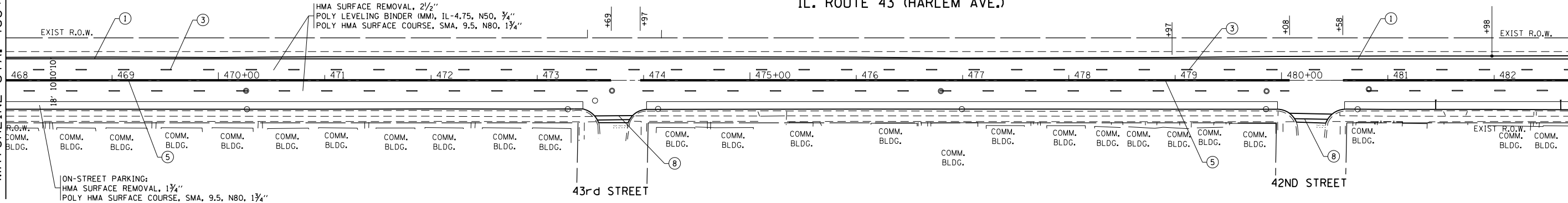
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pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI2966\Meta\Design\DI2966-shr-plan.dgn	PLOT STA. = #STA.#	CHECKED -	REVISED -			348	(2424&3138)RS-3	COOK	75	12	
Default	PLOT DATE = 10/28/2016	DATE -	REVISED -			CONTRACT NO. 62C19					
						ILLINOIS FED. AID PROJECT					



MATCHLINE STA. 468+00

MATCHLINE STA. 483+00

IL. ROUTE 43 (HARLEM AVE.)



PROPOSED PAVEMENT MARKINGS LEGEND

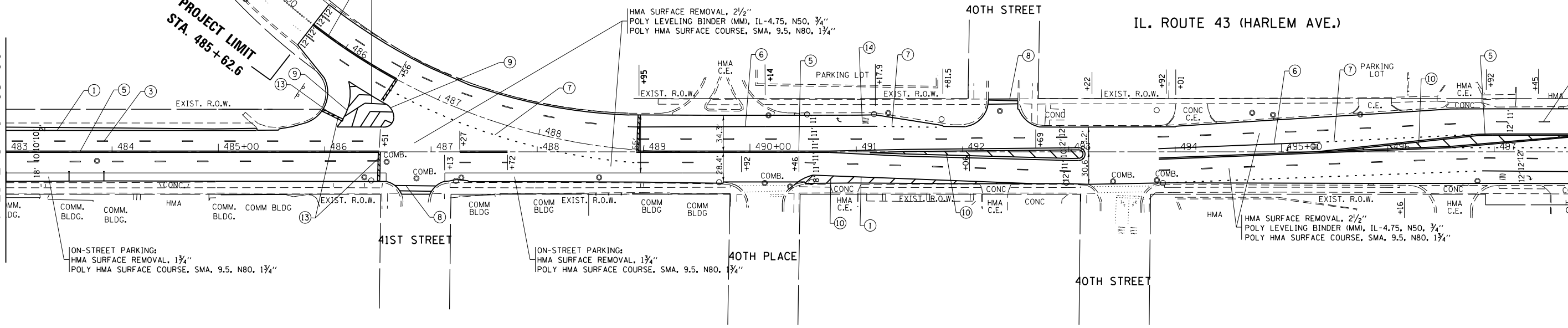
- | | | |
|--|--|--|
| ① THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.) | ⑥ THERMOPLASTIC PVMT. MARKING 6", LINE, WHITE (TYP.) | ⑪ THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, WHITE (TYP.)
- SEE DISTRICT DETAIL TC-13 FOR SPACING |
| ② THERMOPLASTIC PVMT. MARKING 4", LINE, YELLOW (TYP.) | ⑦ THERMOPLASTIC PVMT. MARKING 6", 2' LINE - 6' SPACE, WHITE (TYP.) | ⑫ THERMOPLASTIC PVMT. MARKING 12", CROSSWALK-SCHOOL, WHITE (TYP.)
- SEE DISTRICT DETAIL TC-13 FOR SPACING |
| ③ THERMOPLASTIC PVMT. MARKING 4", 10' LINE - 30' SPACE, WHITE (TYP.) | ⑧ THERMOPLASTIC PVMT. MARKING 6", CROSSWALK-PEDESTRIAN, WHITE (TYP.) | ⑬ THERMOPLASTIC PVMT. MARKING 24", STOP BAR, WHITE (TYP.) |
| ④ THERMOPLASTIC PVMT. MARKING 4", 10' LINE - 30' SPACE, YELLOW TYP.) | ⑨ THERMOPLASTIC PVMT. MARKING 8", LINE, WHITE (TYP.) | ⑭ THERMOPLASTIC PVMT. MARKING, LETTERS & SYMBOLS, WHITE (TYP.) |
| ⑤ THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.) | ⑩ THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.)
- SEE DISTRICT DETAIL TC-13 FOR SPACING | ⑮ RAISED REFLECTIVE PVMT. MARKER (TYP.) |

- NOTES:**
- LIMIT OF RESURFACING ON THE SIDE-STREETS THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE SHOWN ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER / TECHNICIAN.
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MATCHLINE STA. 483+00

MATCHLINE STA. 498+00

IL. ROUTE 43 (HARLEM AVE.)

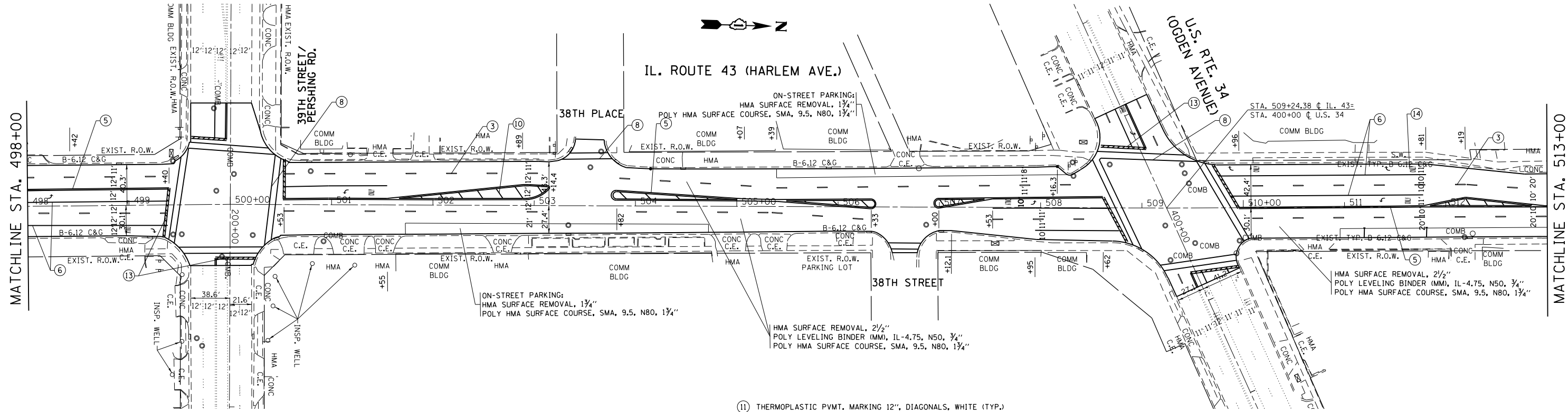


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Default	PLOT DATE = 10/28/2016	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

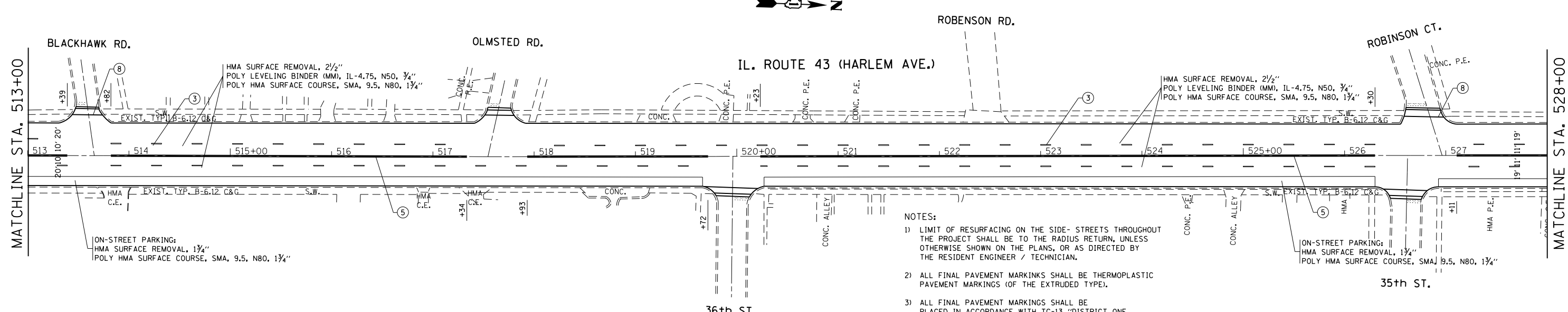
IL 43 (HARLEM AVE) - 26th ST TO I-55 ROADWAY PLAN			
STA.: 1"=50'	SHEET	OF	SHEETS
	STA. 468+00	TO	STA. 498+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	(2424&3138)RS-3	COOK	75	13
CONTRACT NO. 62C19				
ILLINOIS FED. AID PROJECT				



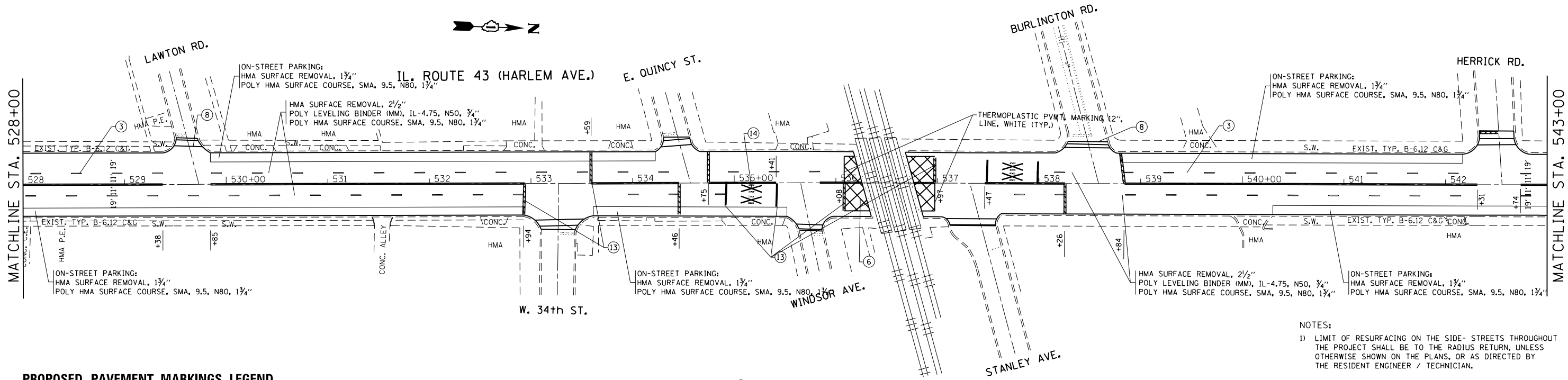
PROPOSED PAVEMENT MARKINGS LEGEND

- ① THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.)
- ② THERMOPLASTIC PVMT. MARKING 4", LINE, YELLOW (TYP.)
- ③ THERMOPLASTIC PVMT. MARKING 4", 10' LINE - 30' SPACE, WHITE (TYP.)
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- ⑦ THERMOPLASTIC PVMT. MARKING 6", 2' LINE - 6' SPACE, WHITE (TYP.)
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- ⑩ THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.)
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- ⑭ THERMOPLASTIC PVMT. MARKING, LETTERS & SYMBOLS, WHITE (TYP.)
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- NOTES:**
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 - FOR PROPOSED CURB RAMP IMPROVEMENTS SEE "CURB RAMP FOR SIDEWALKS" PLANS.

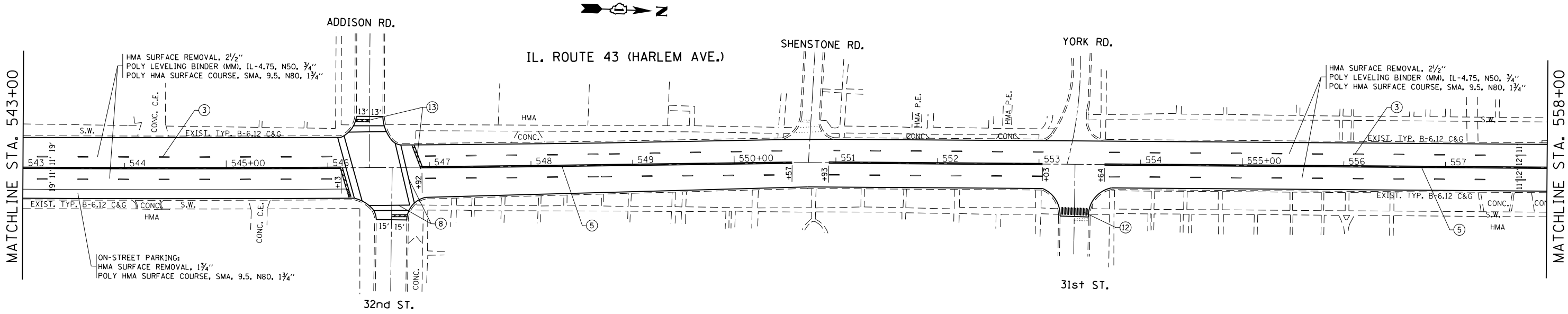
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pw:\IL\084EBID\INTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\DI29616\Design\DI29616-shr-plan.dgn	DRW	CHECKED -	REVISED -			348	(2424&3138)RS-3	COOK	75	14	
Default	PLOT STA. = *STA.*	DATE -	REVISED -			CONTRACT NO. 62C19			ILLINOIS FED. AID PROJECT		
	PLOT DATE = 10/28/2016					STA.: 1"=50'	SHEET OF SHEETS	STA. 498+00 TO STA. 528+00			



PROPOSED PAVEMENT MARKINGS LEGEND

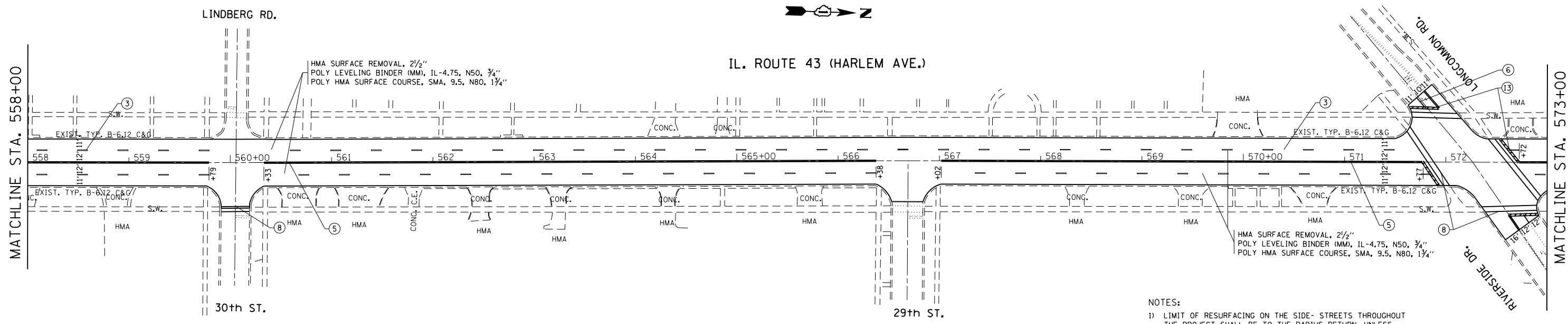
- ① THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.)
- ② THERMOPLASTIC PVMT. MARKING 4", LINE, YELLOW (TYP.)
- ③ THERMOPLASTIC PVMT. MARKING 4", 10' LINE - 30' SPACE, WHITE (TYP.)
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FILE NAME =	USER NAME = currywaw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL 43 (HARLEM AVE) - 26th ST TO I-55 ROADWAY PLAN	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\IL\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI29616\Design\DI29616-shr-plan.dgn		REVISED -	REVISED -			348	(2424&3138)RS-3	COOK	75	15	
Default	PLOT STA. = *STA.*	CHECKED -	REVISED -			CONTRACT NO. 62C19					
	PLOT DATE = 10/28/2016	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					

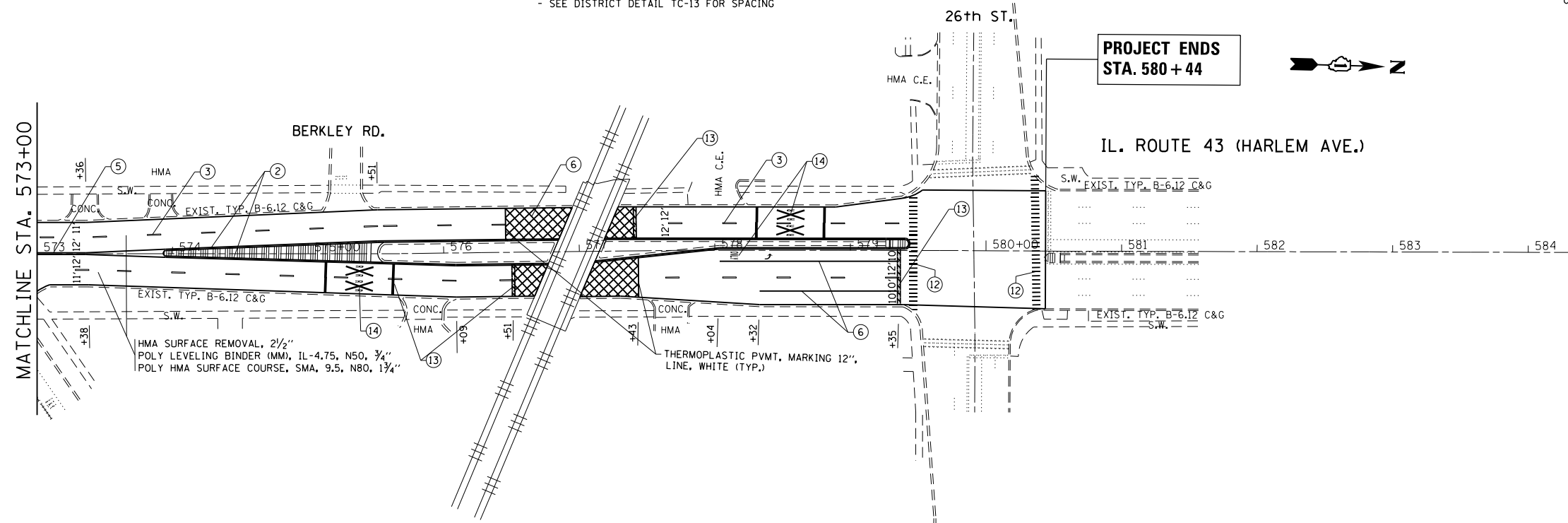
STA.: 1"=50' SHEET OF SHEETS STA. 528+00 TO STA. 558+00



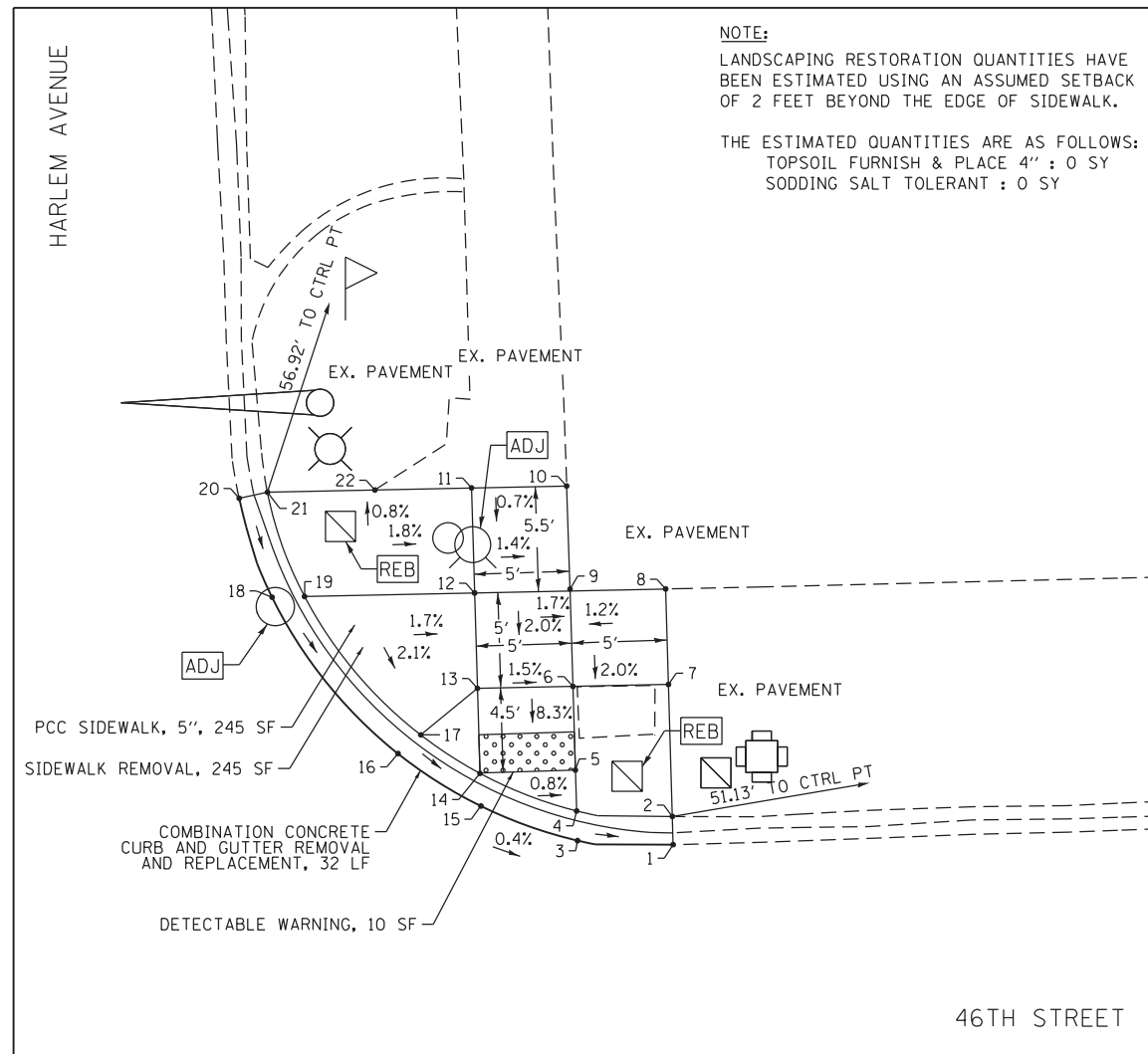
PROPOSED PAVEMENT MARKINGS LEGEND

- ① THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.)
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- ⑭ THERMOPLASTIC PVMT. MARKING, LETTERS & SYMBOLS, WHITE (TYP.)
- ⑮ RAISED REFLECTIVE PVMT. MARKER (TYP.)

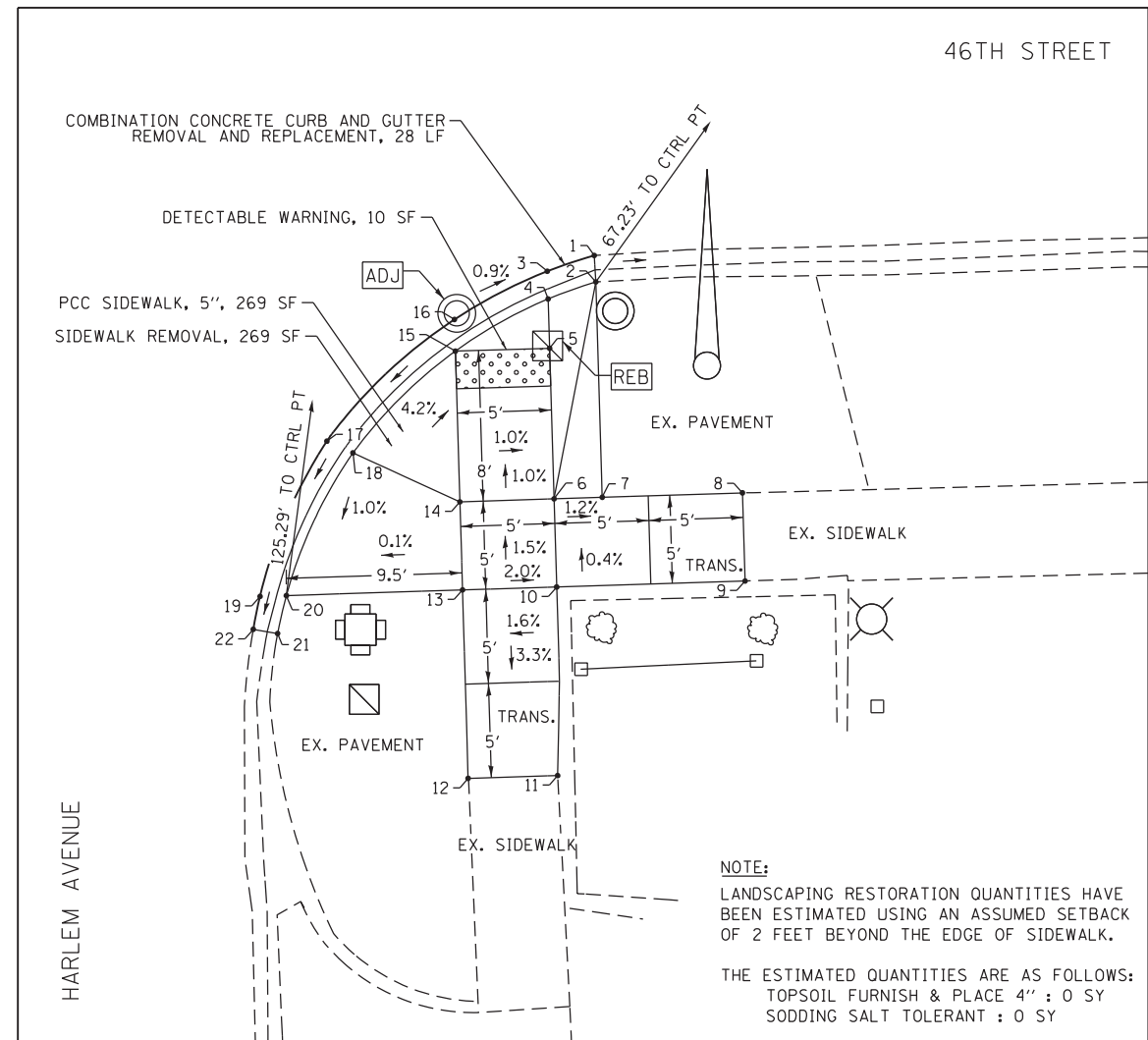
- NOTES:**
- LIMIT OF RESURFACING ON THE SIDE-STREETS THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE SHOWN ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER / TECHNICIAN.
 - ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC PAVEMENT MARKINGS (OF THE EXTRUDED TYPE).
 - ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH TC-13 "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" STANDARD DETAIL.
 - ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH TC-11 "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" STANDARD DETAIL.
 - FOR PROPOSED CURB RAMP IMPROVEMENTS SEE "CURB RAMP FOR SIDEWALKS" PLANS.



FILE NAME =	USER NAME = curryw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL 43 (HARLEM AVE) - 26th ST TO I-55 ROADWAY PLAN	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\IL084EBIDINTEG\illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\DI29616\Design\DI29616-shr-plan.dgn		CHECKED -	REVISED -			348	(2424&3138)RS-3	COOK	75	16	
PLOT STA. = #STA.#		DATE -	REVISED -			CONTRACT NO. 62C19					
Default						ILLINOIS FED. AID PROJECT					



NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 0 SY
 SODDING SALT TOLERANT : 0 SY



NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 0 SY
 SODDING SALT TOLERANT : 0 SY

CONTROL POINTS:
 CUT CROSS ELEV: 592.70
 STA: 454+73.55
 OS: 38.08' RT
 N: 1873350.414
 E: 1129129.665

NORTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 46TH STREET

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	453+99.44	43.32' RT	MATCH EX.
2	454+00.92	43.34' RT	MATCH EX.
3	453+99.85	38.33' RT	591.38
4	454+01.40	38.34' RT	591.37
5	454+03.54	38.36' RT	591.42
6	454+07.93	38.40' LT	591.78
7	454+07.84	43.40' RT	MATCH EX.
8	454+12.84	43.45' RT	MATCH EX.
9	454+12.93	38.45' RT	591.88
10	454+18.43	38.48' RT	MATCH EX.
11	454+18.52	33.50' RT	MATCH EX.
12	454+13.02	33.45' RT	591.96
13	454+08.02	33.40' RT	591.86
14	454+03.57	33.36' RT	591.39
15	454+01.86	33.35' RT	591.40
16	454+04.77	29.12' RT	591.42
17	454+05.70	30.34' RT	591.92
18	454+13.21	22.85' RT	591.73
19	454+13.18	24.55' RT	592.12
20	454+18.45	21.33' RT	MATCH EX.
21	454+18.71	22.82' RT	MATCH EX.
22	454+18.61	28.44' RT	592.07

CONTROL POINTS:
 CUT CROSS ELEV: 592.70
 STA: 454+73.55
 OS: 38.08' RT
 N: 1873350.414
 E: 1129129.665

SOUTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 46TH STREET

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	453+66.51	40.54' RT	MATCH EX.
2	453+65.10	40.57' RT	MATCH EX.
3	453+65.77	38.02' RT	591.07
4	453+64.29	38.01' RT	591.06
5	453+61.67	37.98' RT	591.10
6	453+53.68	37.91' RT	591.18
7	453+53.67	40.47' RT	590.96
8	453+53.61	47.91' RT	MATCH EX.
9	453+48.93	47.87' RT	MATCH EX.
10	453+49.00	37.87' RT	591.26
11	453+39.01	37.54' RT	MATCH EX.
12	453+39.04	32.78' RT	MATCH EX.
13	453+49.04	32.87' RT	591.36
14	453+53.72	32.91' RT	591.19
15	453+61.72	32.98' RT	591.11
16	453+63.40	33.00' RT	591.12
17	453+57.22	25.97' RT	591.00
18	453+56.53	27.34' RT	591.43
19	453+49.12	22.12' RT	590.93
20	453+49.11	23.52' RT	591.35
21	453+47.11	22.98' RT	MATCH EX.
22	453+47.38	21.69' RT	MATCH EX.



USER NAME = WTeng
 DRAWN -
 PLOT SCALE = 10.0000" / in.
 CHECKED - MTC
 PLOT DATE = 10/17/2016
 DATE - 10/17/2016

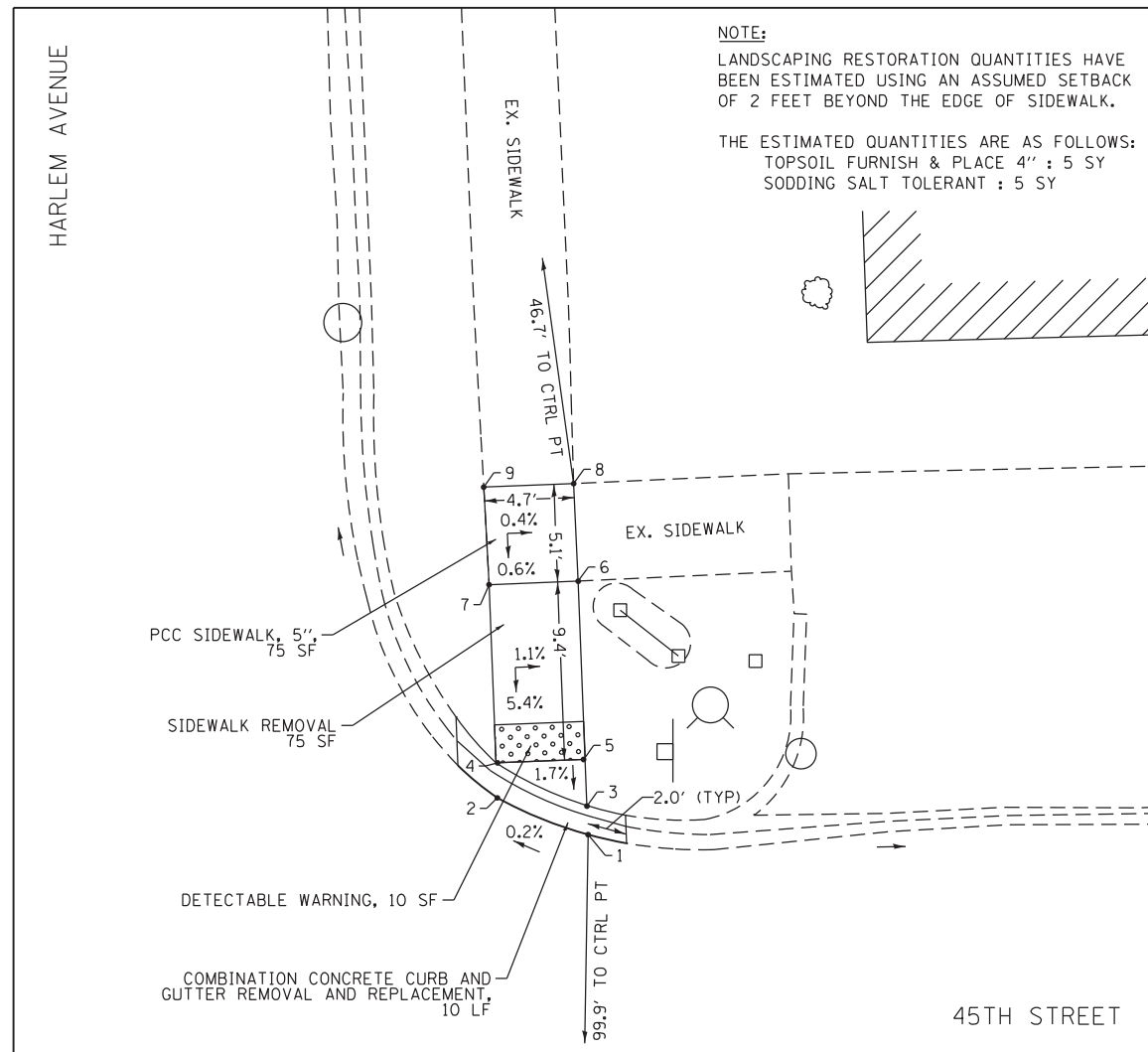
DESIGNED -
 REVISIONS -
 REVISIONS -
 REVISIONS -
 REVISIONS -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

IL ROUTE 43 (HARLEM AVENUE)
 ADA RAMP DETAILS

SCALE: 1" = 5' SHEET 1 OF 38 SHEETS STA. TO STA.

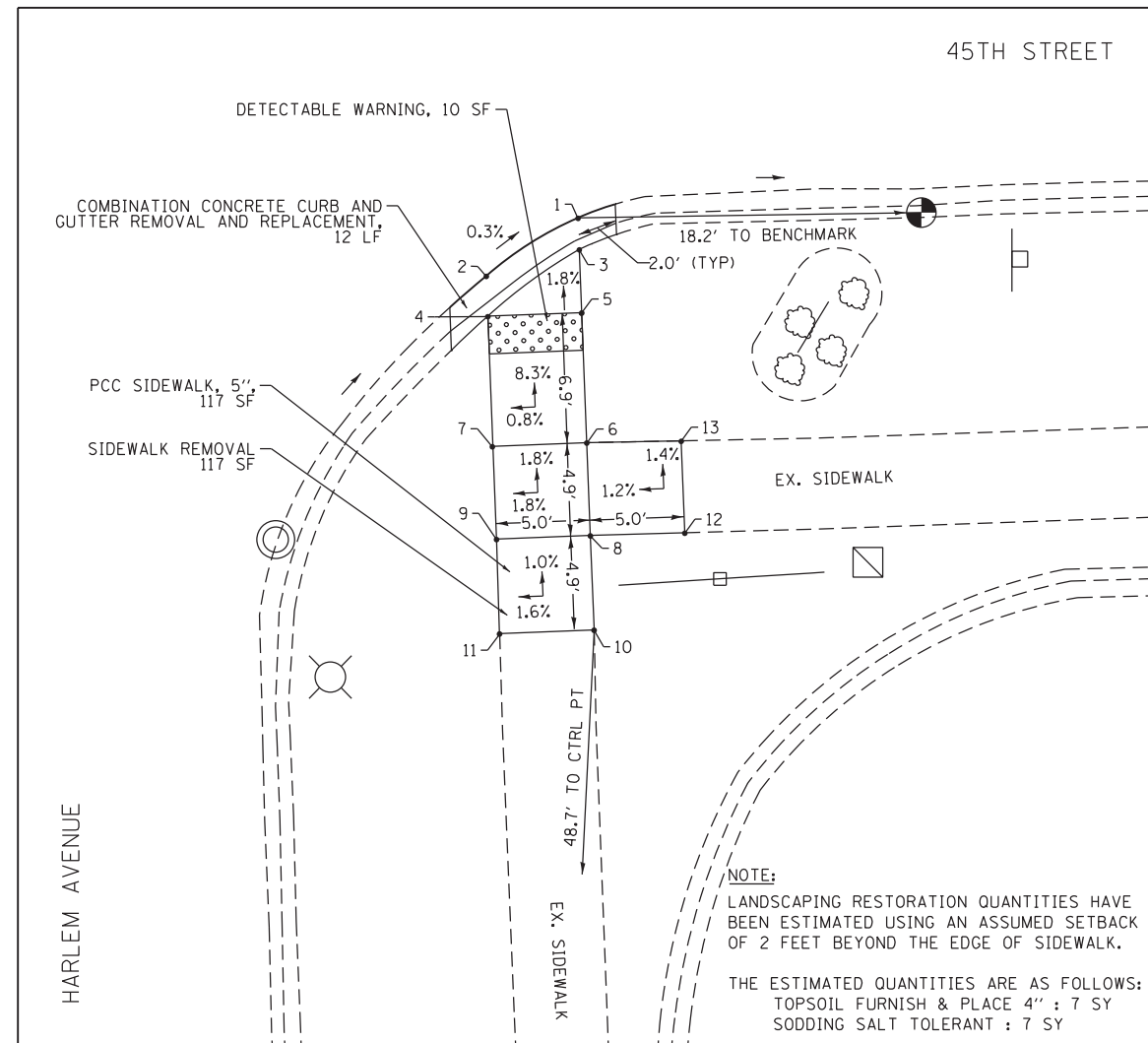
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	(2424 & 3138) RS-3	COOK	75	17
CONTRACT NO. 62C19				
ILLINOIS FED. AID PROJECT				



CONTROL POINTS:
 CUT CROSS CUT CROSS
 ELEV: 597.01 ELEV: 596.55
 STA: 461+23.48 STA: 459+58.82
 OS: 34.82' RT OS: 34.05' RT
 N: 1873999.722 N: 1873835.158
 E: 1129101.201 E: 1129106.818

NORTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 45TH STREET

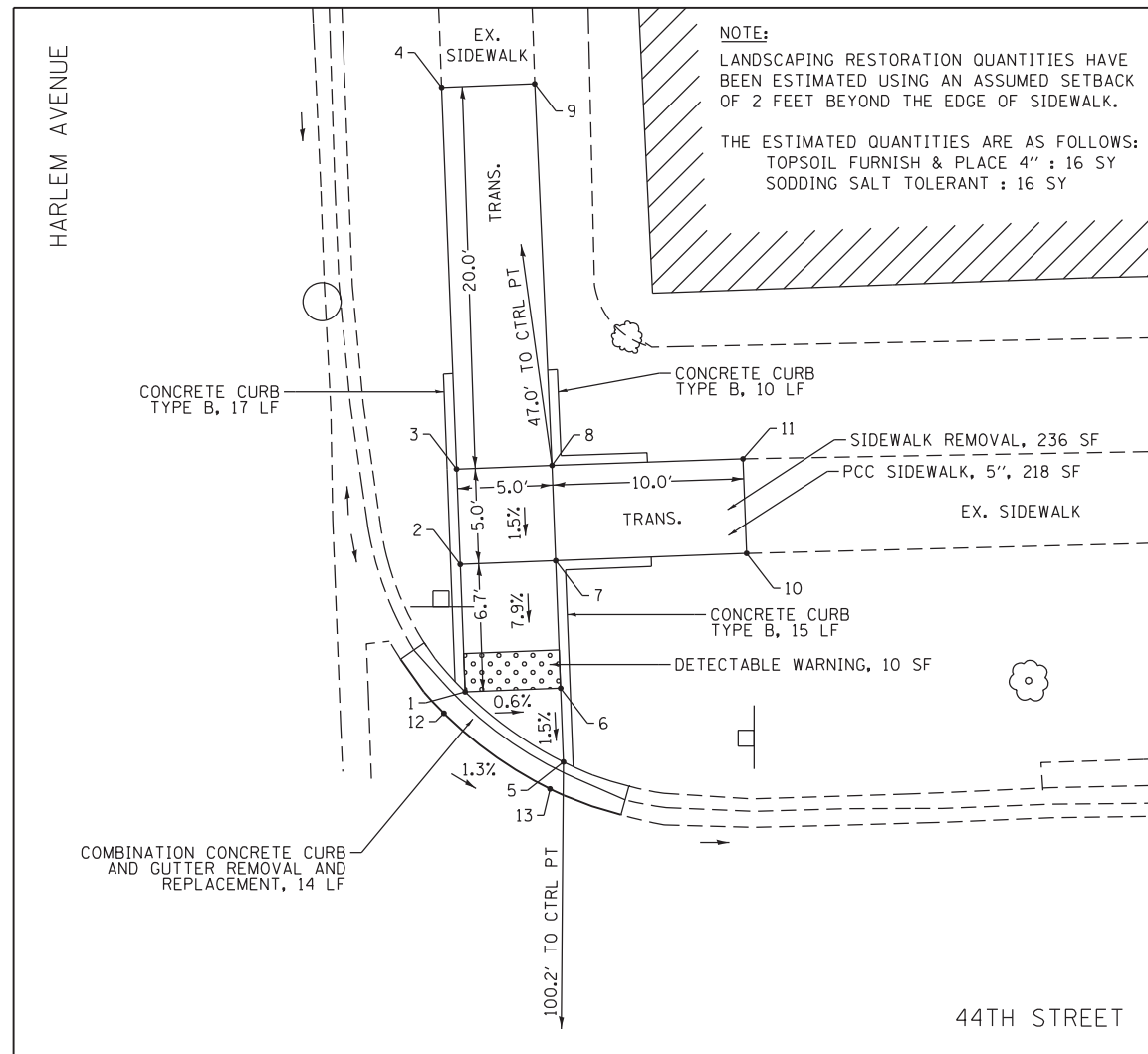
ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	460+58.53	39.51' RT	596.36
2	460+60.64	34.82' RT	596.35
3	460+60.06	39.51' RT	596.35
4	460+62.48	34.82' RT	596.34
5	460+62.48	39.51' RT	596.39
6	460+71.84	39.51' RT	MATCH EX.
7	460+71.84	34.82' RT	MATCH EX.
8	460+76.96	39.47' RT	MATCH EX.
9	460+76.96	34.74' RT	MATCH EX.



CONTROL POINTS:
 BENCHMARK CUT CROSS
 BACK OF CURB ELEV: 596.55
 ELEV: 596.22 STA: 459+58.82
 STA: 460+28.69 OS: 34.05' RT
 OS: 56.47' RT N: 1873835.158
 N: 1873905.850 E: 1129106.818
 E: 1129126.509

SOUTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 45TH STREET

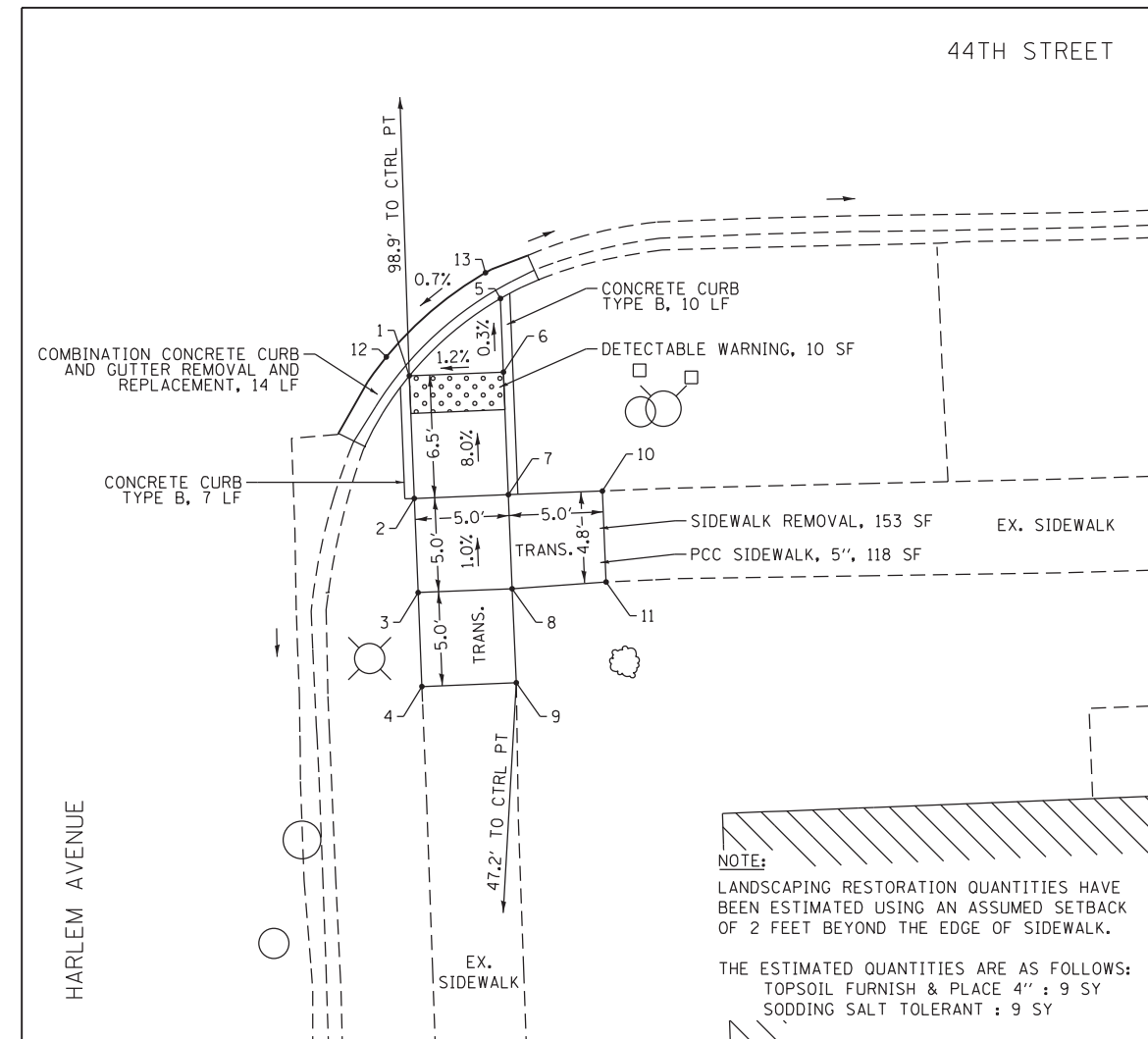
ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	460+29.12	38.30' RT	596.08
2	460+26.24	33.33' RT	596.10
3	460+27.45	38.30' RT	596.07
4	460+24.11	33.33' RT	596.09
5	460+24.11	38.30' RT	596.13
6	460+17.22	38.30' RT	596.70
7	460+17.22	33.33' RT	596.66
8	460+12.30	38.30' RT	596.79
9	460+12.30	33.33' RT	596.75
10	460+07.30	38.31' RT	MATCH EX.
11	460+07.30	33.30' RT	MATCH EX.
12	460+12.25	43.30' RT	MATCH EX.
13	460+17.12	43.30' RT	MATCH EX.



CONTROL POINTS:
 CUT CROSS CUT CROSS
 ELEV: 602.94 ELEV: 600.67
 STA: 467+86.27 STA: 466+23.87
 OS: 34.22' RT OS: 34.29' RT
 N: 1874661.991 N: 1874499.716
 E: 1129074.890 E: 1129081.262

NORTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 44TH STREET

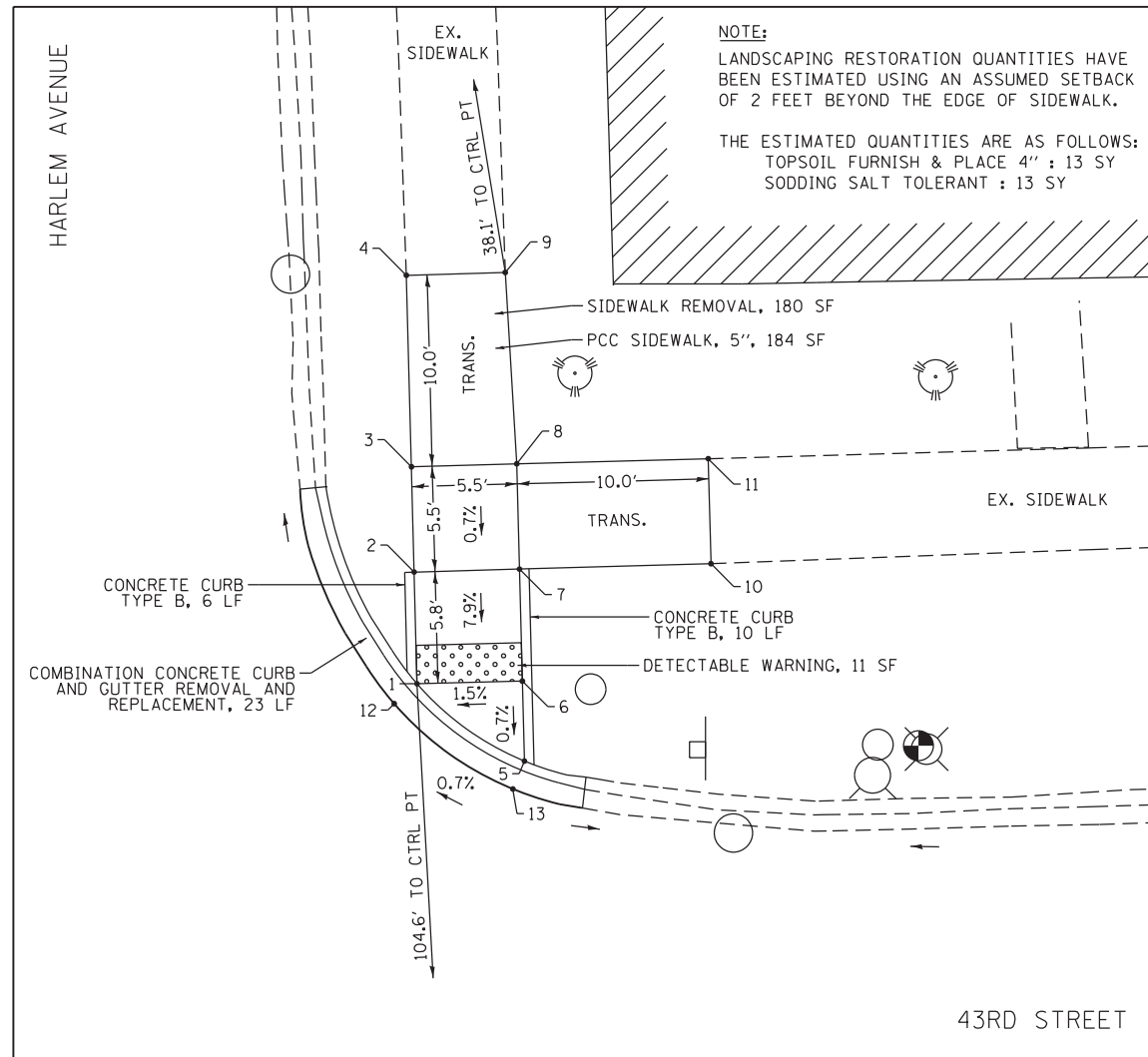
ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	467+27.81	33.95' RT	601.13
2	467+34.48	33.95' RT	601.66
3	467+39.48	33.95' RT	601.73
4	467+59.48	33.94' RT	MATCH EX.
5	467+23.94	38.95' RT	601.04
6	467+27.81	38.95' RT	601.10
7	467+34.48	38.95' RT	601.63
8	467+39.48	38.95' RT	601.70
9	467+59.48	38.82' RT	MATCH EX.
10	467+34.48	48.95' RT	MATCH EX.
11	467+39.44	48.95' RT	MATCH EX.
12	467+26.73	32.79' RT	601.14
13	467+22.55	38.19' RT	601.05



CONTROL POINTS:
 CUT CROSS CUT CROSS
 ELEV: 602.94 ELEV: 600.67
 STA: 467+86.27 STA: 466+23.87
 OS: 34.22' RT OS: 34.29' RT
 N: 1874661.991 N: 1874499.716
 E: 1129074.890 E: 1129081.262

SOUTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 44TH STREET

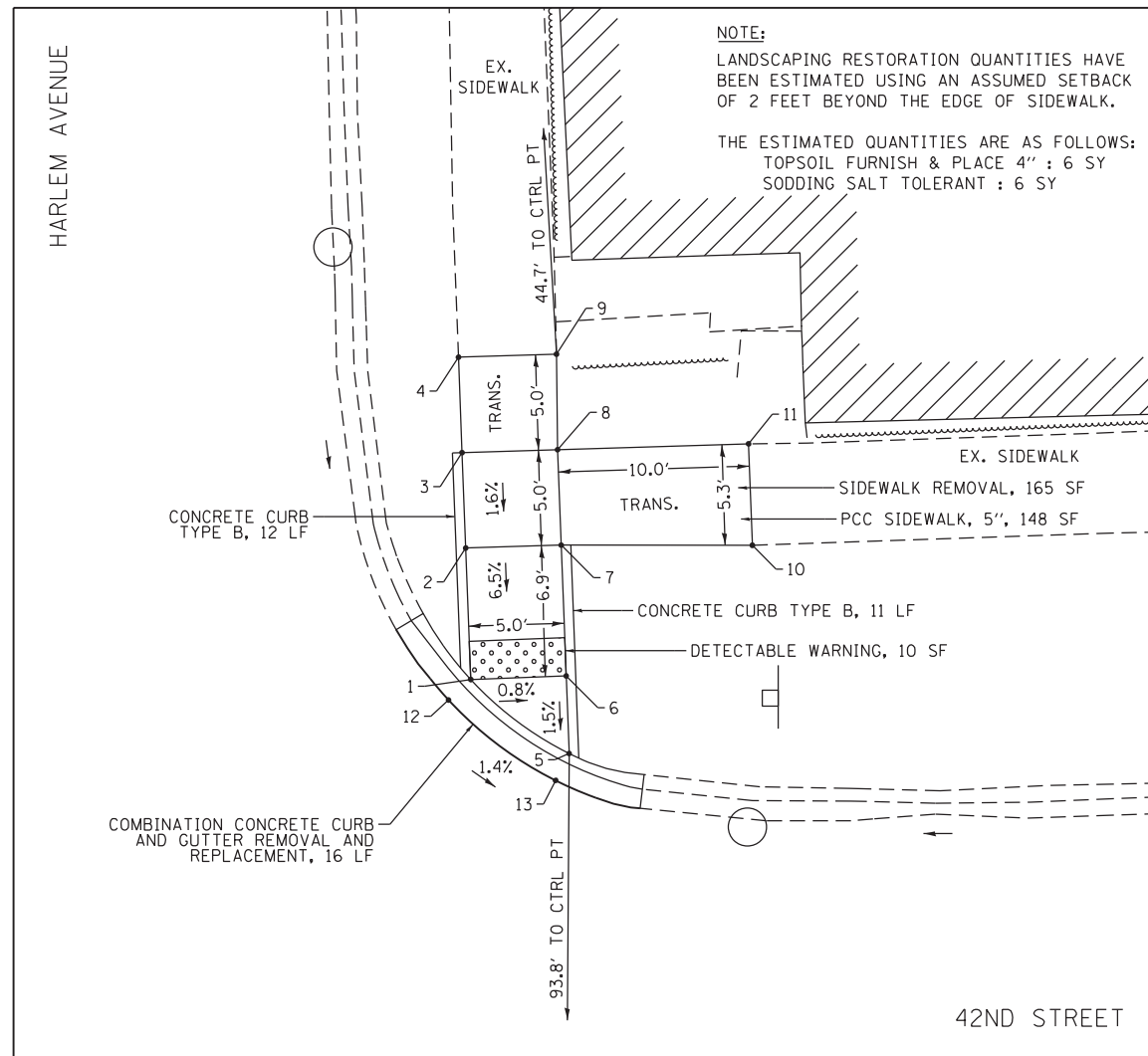
ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	466+87.35	33.77' RT	600.79
2	466+80.84	33.78' RT	601.31
3	466+75.84	33.78' RT	601.36
4	466+70.84	33.79' RT	MATCH EX.
5	466+91.27	38.76' RT	600.84
6	466+87.36	38.77' RT	600.85
7	466+80.84	38.78' RT	601.37
8	466+75.84	38.78' RT	601.42
9	466+70.84	38.81' RT	MATCH EX.
10	466+80.84	43.78' RT	MATCH EX.
11	466+76.01	43.78' RT	MATCH EX.
12	466+88.41	32.59' RT	600.80
13	466+92.67	38.02' RT	600.85



CONTROL POINTS:
 CUT CROSS CUT CROSS
 ELEV: 607.72 ELEV: 608.29
 STA: 474+50.87 STA: 472+87.19
 OS: 33.90' RT OS: 34.76' RT
 N: 1875326.084 N: 1875162.561
 E: 1129048.792 E: 1129055.997

NORTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 43RD STREET

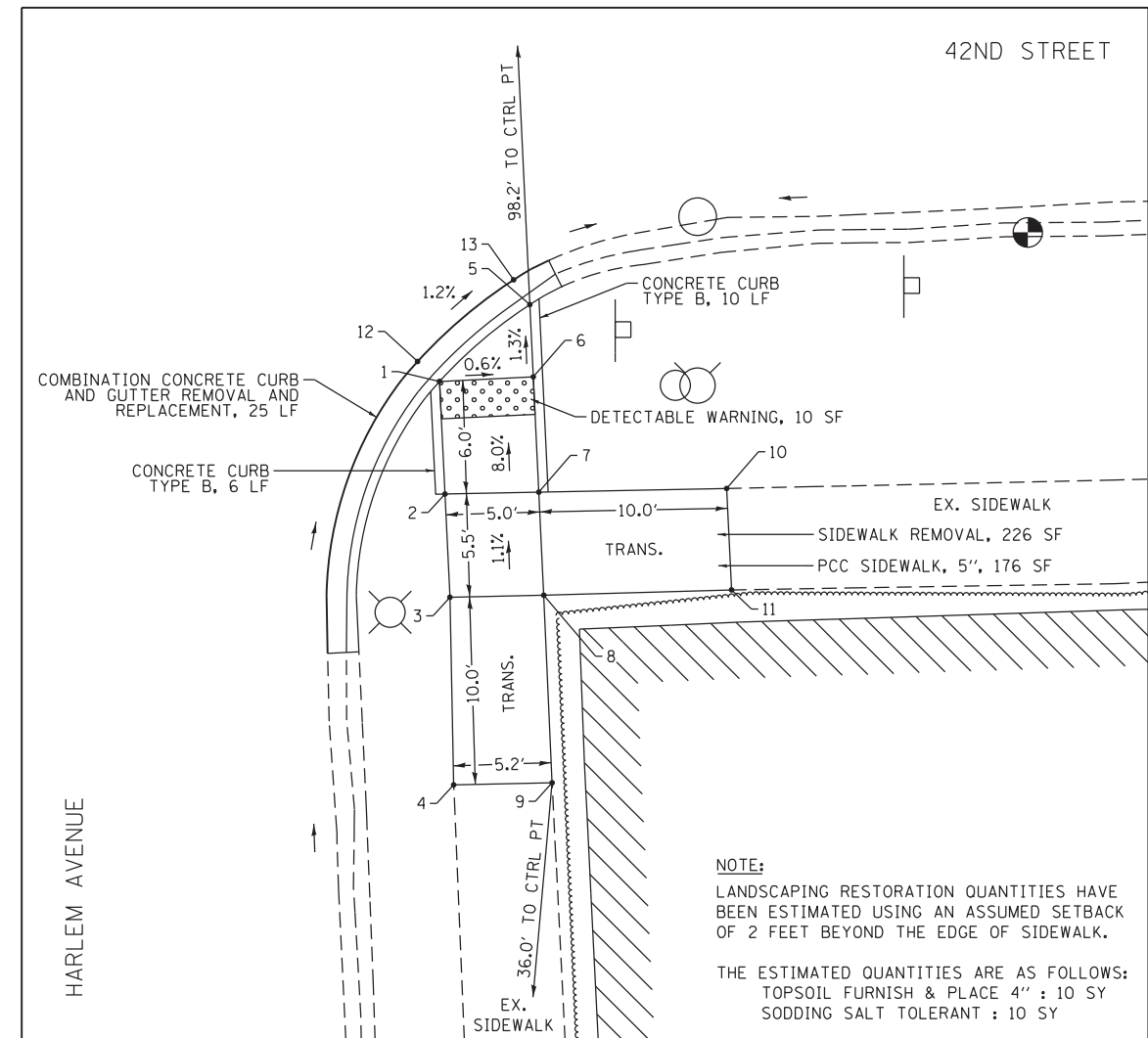
ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	473+91.80	33.14' RT	607.39
2	473+97.64	33.21' RT	607.85
3	474+03.14	33.28' RT	607.89
4	474+13.14	33.41' RT	MATCH EX.
5	473+87.55	38.58' RT	607.44
6	473+91.73	38.64' RT	607.47
7	473+97.58	38.71' RT	607.93
8	474+03.08	38.78' RT	607.97
9	474+13.08	38.57' RT	MATCH EX.
10	473+97.47	48.71' RT	MATCH EX.
11	474+02.96	48.78' RT	MATCH EX.
12	473+90.81	31.91' RT	607.40
13	473+86.12	37.92' RT	607.45



CONTROL POINTS:
 CUT CROSS CUT CROSS
 ELEV: 608.23 ELEV: 608.15
 STA: 481+10.06 STA: 479+50.73
 OS: 37.77' RT OS: 34.23' RT
 N: 1875984.920 N: 1875825.577
 E: 1129027.097 E: 1129029.739

NORTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 42ND STREET

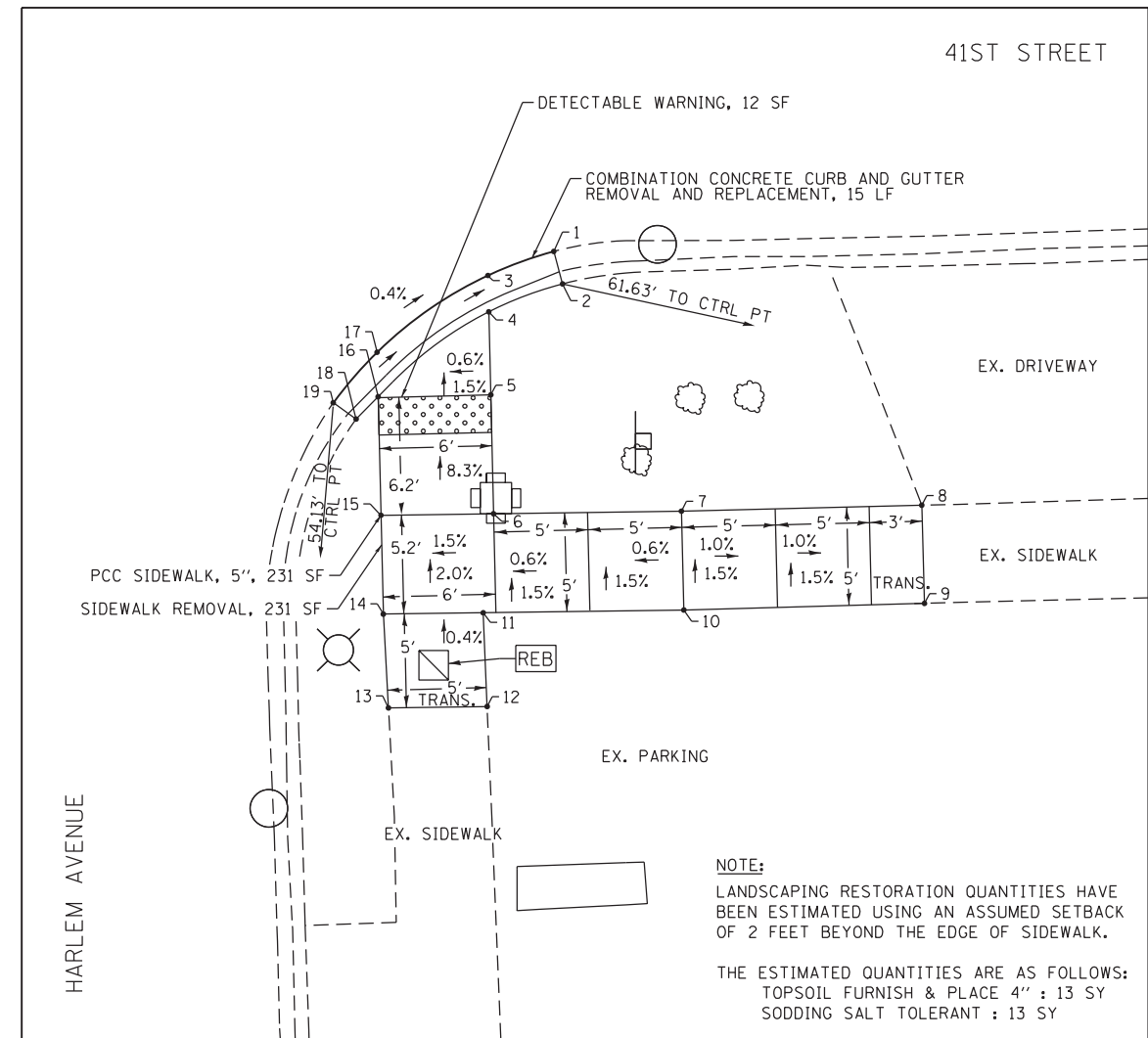
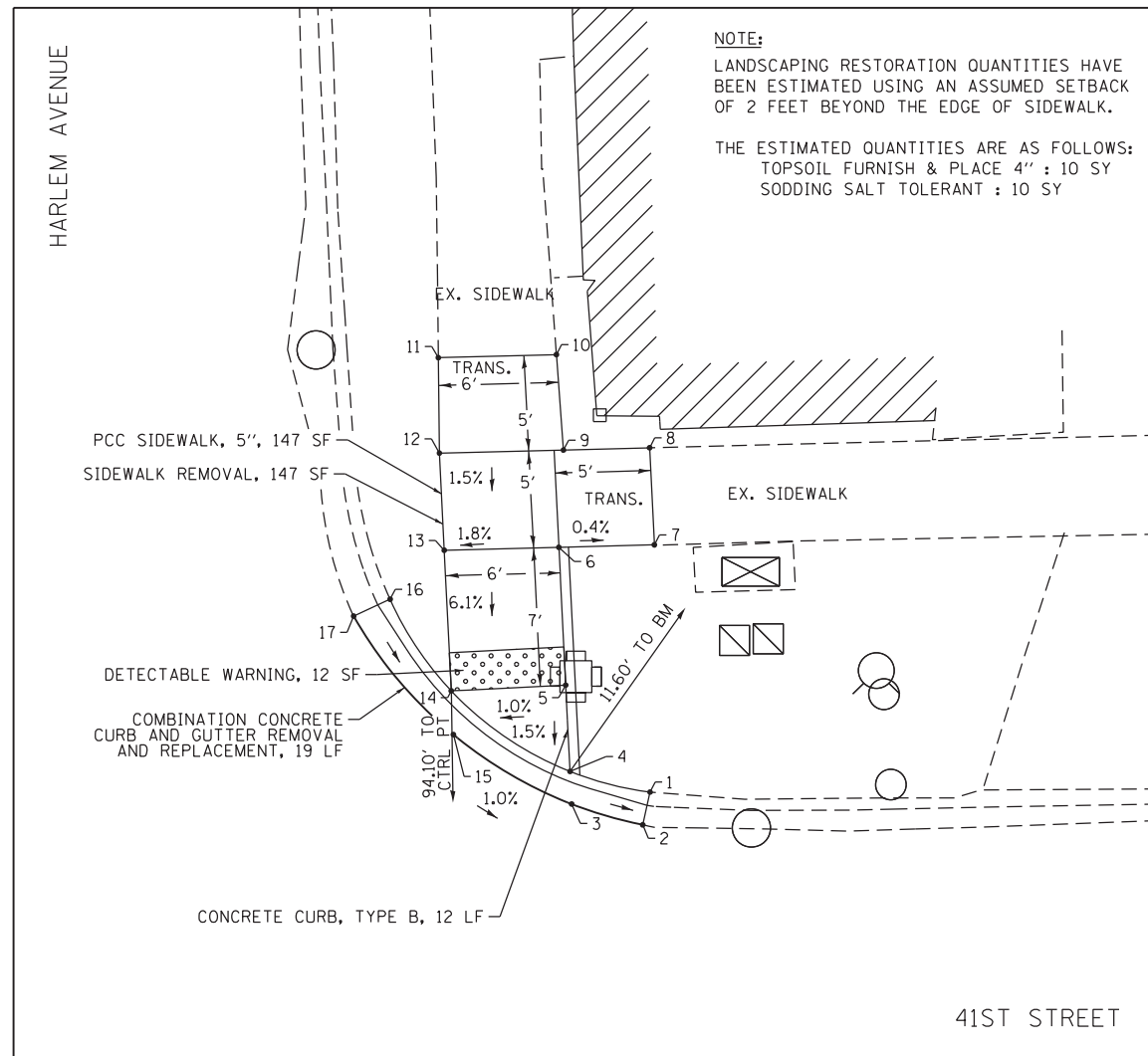
ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	480+48.47	33.46' RT	607.60
2	480+55.36	33.46' RT	608.15
3	480+60.36	33.47' RT	608.23
4	480+65.36	33.47' RT	MATCH EX.
5	480+44.40	38.46' RT	607.50
6	480+48.46	38.46' RT	607.56
7	480+55.32	38.46' RT	608.11
8	480+60.32	38.47' RT	MATCH EX.
9	480+65.32	38.60' RT	MATCH EX.
10	480+54.92	48.46' RT	MATCH EX.
11	480+60.23	48.47' RT	MATCH EX.
12	480+47.44	32.26' RT	607.61
13	480+43.01	37.69' RT	607.51



CONTROL POINTS:
 CUT CROSS CUT CROSS
 ELEV: 608.23 ELEV: 608.15
 STA: 481+10.06 STA: 479+50.73
 OS: 37.77' RT OS: 34.23' RT
 N: 1875984.920 N: 1875825.577
 E: 1129027.097 E: 1129029.739

SOUTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 42ND STREET

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	480+08.00	33.65' RT	607.48
2	480+01.99	33.70' RT	607.96
3	479+96.49	33.75' RT	608.02
4	479+86.49	33.57' RT	MATCH EX.
5	480+11.90	38.61' RT	607.40
6	480+08.05	38.65' RT	607.45
7	480+01.90	38.70' RT	607.93
8	479+96.40	38.76' RT	607.99
9	479+86.40	38.81' RT	MATCH EX.
10	480+01.71	48.71' RT	MATCH EX.
11	479+96.30	48.76' RT	MATCH EX.
12	480+09.11	32.52' RT	607.49
13	480+13.26	37.80' RT	607.41



CONTROL POINTS:
CUT CROSS ELEV: 608.72
STA: 486+11.31
OS: 30.57' RT
N: 1876485.516
E: 1129000.455

CUT CROSS ELEV: 609.21
STA: 487+10.16
OS: 47.00' RT
N: 1876584.930
E: 1129013.045

NORTHEAST CORNER
IL ROUTE 43 (HARLEM AVENUE) AND 41ST STREET

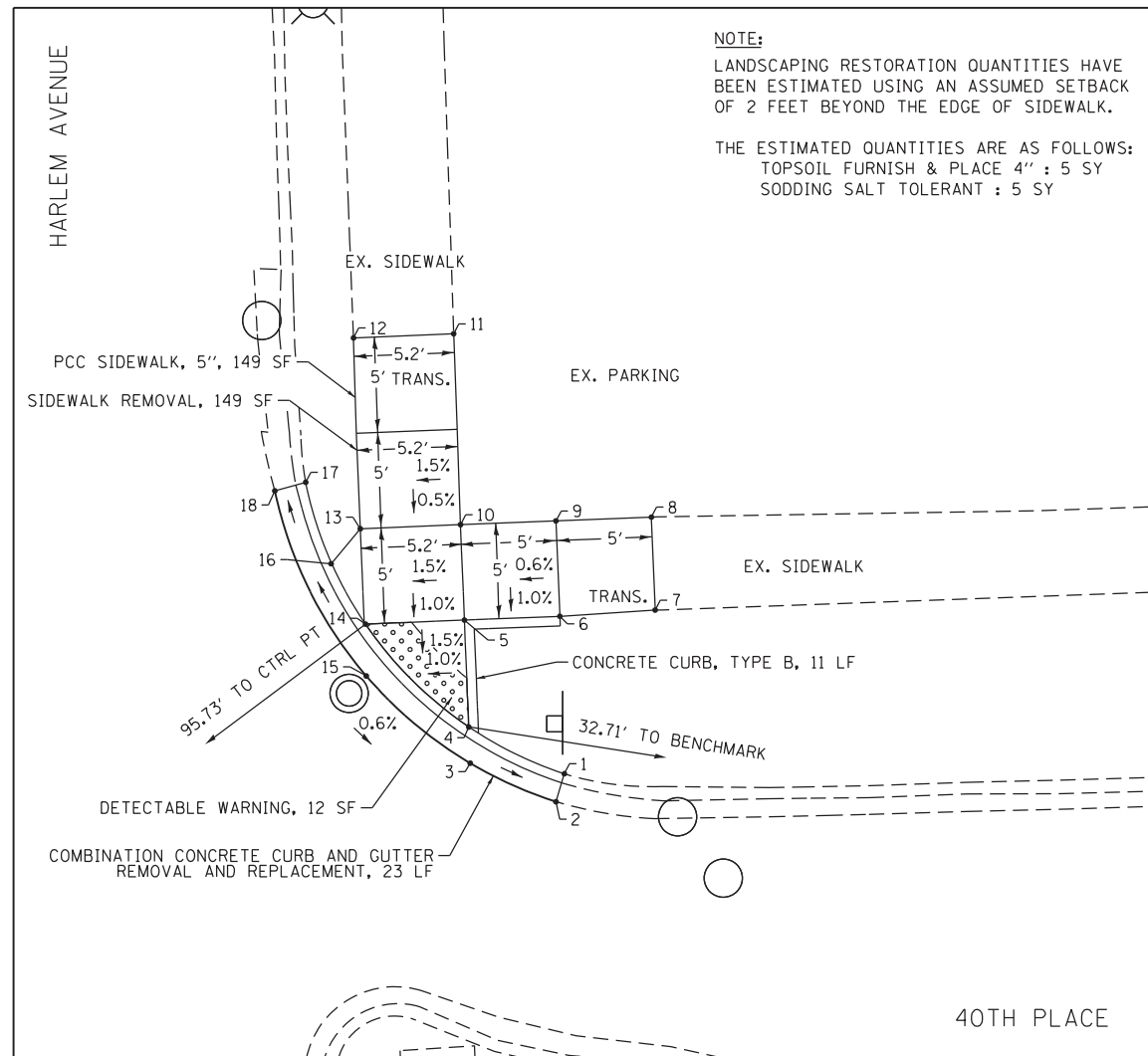
POINT No.:	STATION	OFFSET	ELEV.
1	486+99.65	44.11' RT	MATCH EX.
2	486+97.96	43.66' RT	MATCH EX.
3	486+99.19	39.99' RT	608.22
4	487+00.89	39.97' RT	608.21
5	487+05.42	39.92' RT	608.31
6	487+12.64	39.84' RT	608.82
7	487+12.58	44.84' RT	MATCH EX.
8	487+17.67	44.78' RT	MATCH EX.
9	487+17.73	40.27' RT	608.90
10	487+22.73	40.10' RT	MATCH EX.
11	487+22.81	33.92' RT	MATCH EX.
12	487+17.81	33.78' RT	608.81
13	487+12.72	33.84' RT	608.73
14	487+05.35	33.92' RT	608.28
15	487+03.06	33.94' RT	608.29
16	487+10.27	30.90' RT	MATCH EX.
17	487+09.46	28.96' RT	MATCH EX.

CONTROL POINTS:
CUT CROSS ELEV: 608.72
STA: 486+11.31
OS: 30.57' RT
N: 1876485.516
E: 1129000.455

CUT CROSS ELEV: 608.49
STA: 486+57.63
OS: 100.34' RT
N: 1876534.505
E: 1129068.380

SOUTHEAST CORNER
IL ROUTE 43 (HARLEM AVENUE) AND 41ST STREET

POINT No.:	STATION	OFFSET	ELEV.
1	486+73.04	43.53' RT	MATCH EX.
2	486+71.29	43.93' RT	MATCH EX.
3	486+71.90	39.98' RT	608.17
4	486+69.96	39.95' RT	608.18
5	486+65.53	39.89' RT	608.23
6	486+59.20	39.79' RT	608.76
7	486+58.95	49.79' RT	608.82
8	486+58.77	62.59' RT	MATCH EX.
9	486+53.54	62.53' RT	MATCH EX.
10	486+53.69	49.71' RT	608.89
11	486+53.96	39.03' RT	608.87
12	486+48.95	39.05' RT	MATCH EX.
13	486+49.09	33.79' RT	MATCH EX.
14	486+54.09	33.72' RT	608.79
15	486+59.35	33.79' RT	608.68
16	486+65.62	33.89' RT	608.19
17	486+68.04	33.92' RT	608.20
18	486+64.41	32.82' RT	MATCH EX.
19	486+65.44	31.49' RT	MATCH EX.



NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 5 SY
 SODDING SALT TOLERANT : 5 SY

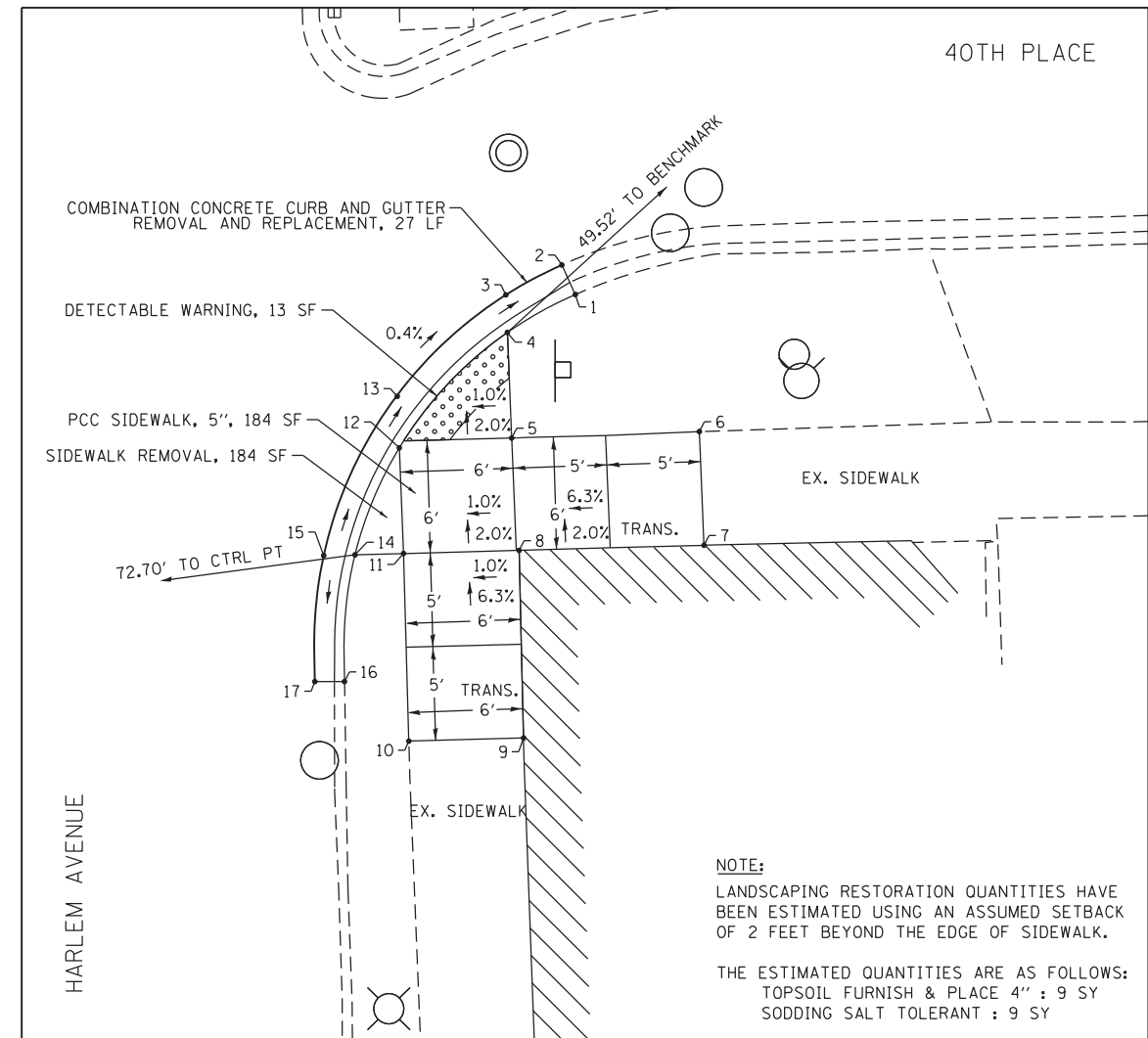


CONTROL POINTS:
 CUT CROSS ELEV: 609.55
 STA: 489+81.80
 OS: 41.69' LT
 N: 1876852.925
 E: 1128913.889

BENCHMARK
 TOP OF CURB ELEV: 608.93
 STA: 490+31.26
 OS: 71.07' RT
 N: 1876906.723
 E: 129024.643

NORTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 40TH PLACE

POINT No.:	STATION	OFFSET	ELEV.
1	490+32.88	43.55' RT	MATCH EX.
2	490+31.43	43.05' RT	MATCH EX.
3	490+33.62	38.65' RT	608.90
4	490+35.53	38.64' RT	608.89
5	490+41.13	38.63' RT	608.98
6	490+41.13	43.63' RT	609.01
7	490+41.26	48.63' RT	MATCH EX.
8	490+46.13	48.62' RT	MATCH EX.
9	490+46.13	43.62' RT	609.06
10	490+46.13	38.63' RT	609.06
11	490+56.13	38.65' RT	MATCH EX.
12	490+56.12	33.41' RT	MATCH EX.
13	490+46.12	33.37' RT	608.98
14	490+41.12	33.38' RT	608.93
15	490+38.40	33.39' RT	608.94
16	490+44.37	31.80' RT	608.95
17	490+48.66	30.61' RT	MATCH EX.
18	490+48.27	28.98' RT	MATCH EX.



NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 9 SY
 SODDING SALT TOLERANT : 9 SY



CONTROL POINTS:
 CUT CROSS ELEV: 609.55
 STA: 489+81.80
 OS: 41.69' LT
 N: 1876852.925
 E: 1128913.889

BENCHMARK
 TOP OF CURB ELEV: 608.93
 STA: 490+31.26
 OS: 71.07' RT
 N: 1876906.723
 E: 129024.643

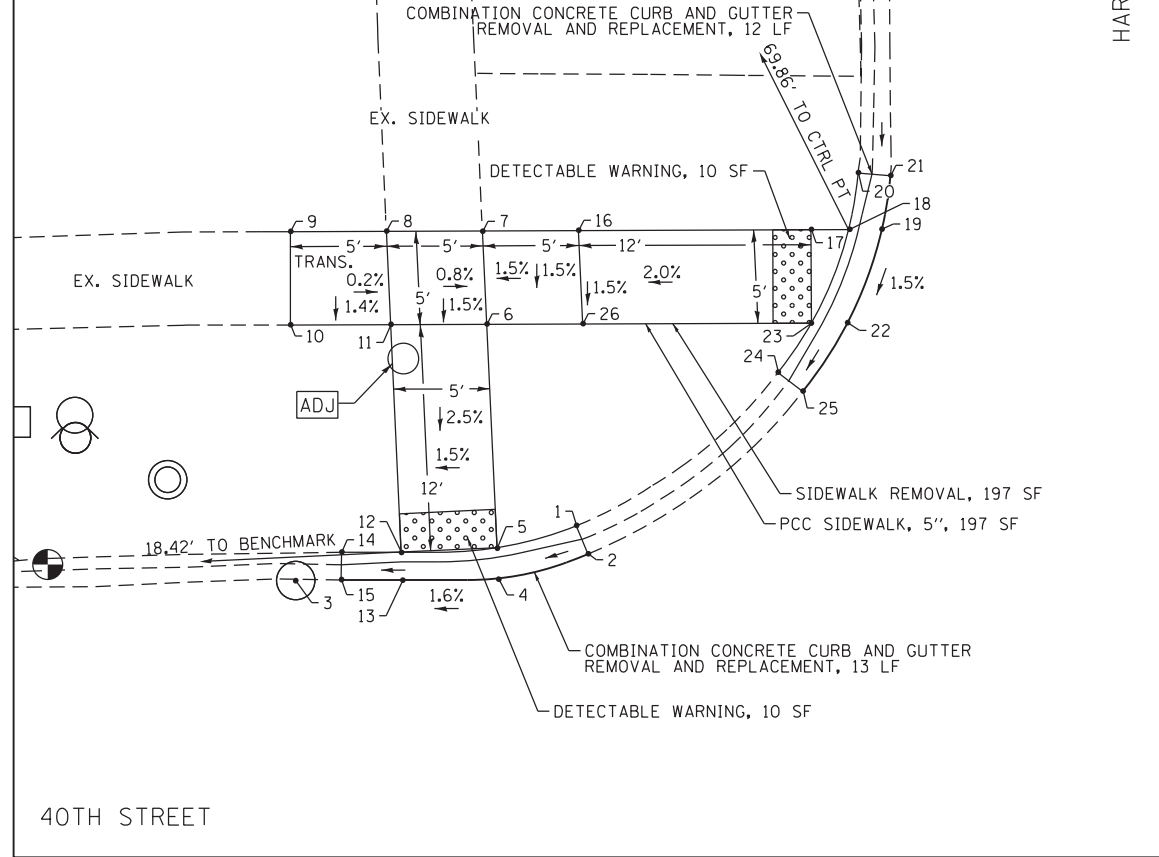
SOUTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 40TH PLACE

POINT No.:	STATION	OFFSET	ELEV.
1	489+95.00	43.17' RT	MATCH EX.
2	489+96.60	42.51' RT	MATCH EX.
3	489+95.13	39.47' RT	608.85
4	489+93.12	39.47' RT	608.84
5	489+87.50	39.49' RT	608.95
6	489+87.45	49.49' RT	MATCH EX.
7	489+81.39	49.50' RT	MATCH EX.
8	489+81.50	39.65' RT	609.07
9	489+71.50	39.50' RT	MATCH EX.
10	489+71.57	33.69' RT	MATCH EX.
11	489+81.57	33.50' RT	609.01
12	489+87.57	33.73' RT	608.89
13	489+90.00	33.48' RT	608.88
14	489+81.60	30.91' RT	609.17
15	489+81.57	29.23' RT	608.90
16	489+74.87	30.09' RT	MATCH EX.
17	489+74.94	28.51' RT	MATCH EX.

NOTE:

LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.

THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 10 SY
 SODDING SALT TOLERANT : 10 SY



40TH STREET

HARLEM AVENUE

CONTROL POINTS:

CUT CROSS	BENCHMARK
ELEV: 608.83	TOP OF CURB
STA: 493+36.47	ELEV: 608.44
OS: 45.73' LT	STA: 492+53.74
N: 1877207.171	OS: 68.02' LT
E: 1128896.088	N: 1877123.636
	E: 1128877.023

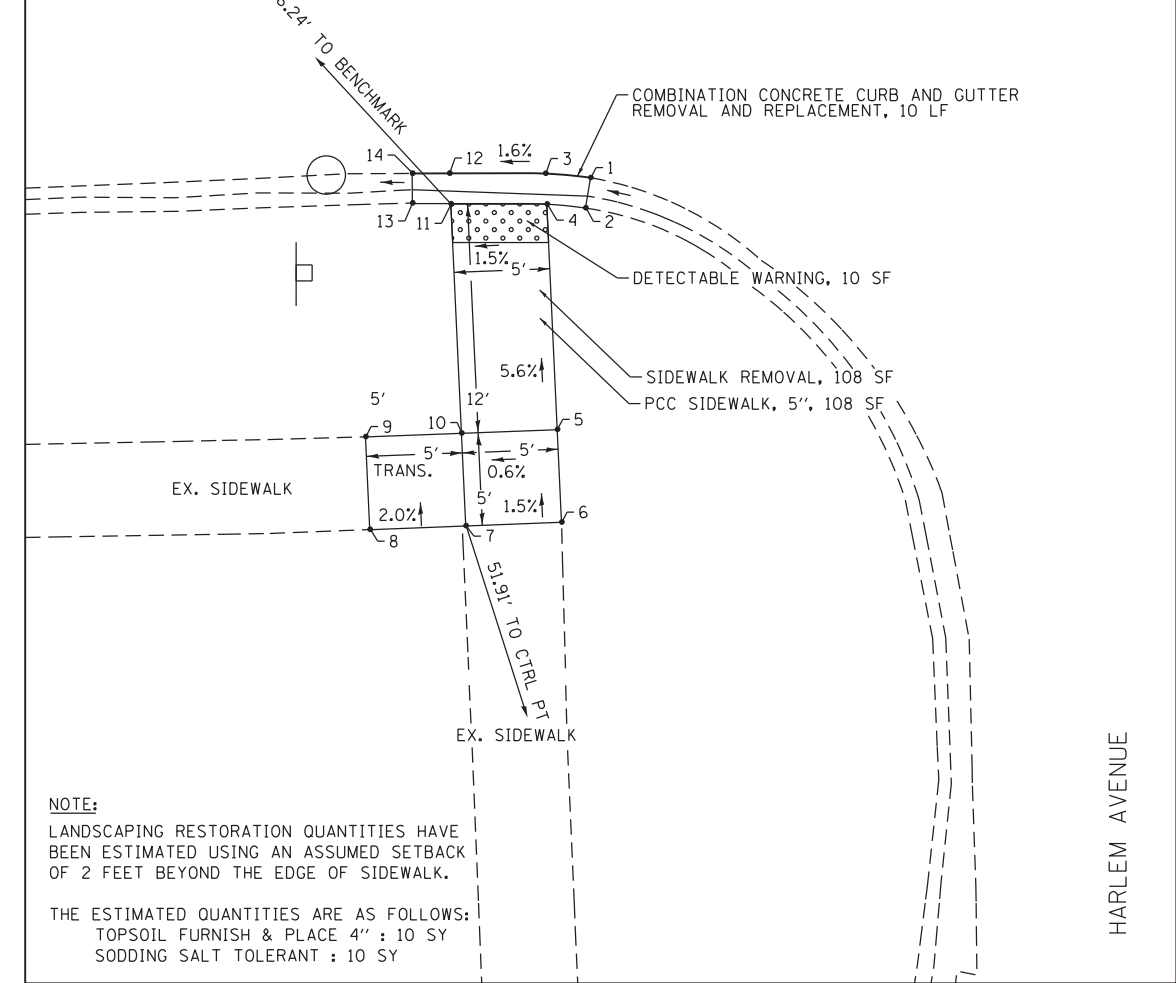
NORTHWEST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 40TH STREET

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	492+54.69	40.42' LT	MATCH EX.
2	492+53.18	39.87' LT	MATCH EX.
3	492+54.45	54.95' LT	608.12
4	492+52.02	44.58' LT	608.42
5	492+53.70	44.60' LT	608.41
6	492+65.36	44.68' LT	608.60
7	492+70.23	44.71' LT	MATCH EX.
8	492+70.40	49.72' LT	MATCH EX.
9	492+70.58	54.72' LT	MATCH EX.
10	492+65.72	54.91' LT	MATCH EX.
11	492+65.53	49.68' LT	608.64
12	492+53.65	49.60' LT	608.33
13	492+52.21	49.58' LT	608.34

NORTHWEST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 40TH STREET

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
14	492+53.77	52.70' LT	MATCH EX.
15	492+52.34	52.78' LT	MATCH EX.
16	492+70.05	39.71' LT	608.75
17	492+69.63	27.60' LT	609.00
18	492+69.56	25.64' LT	608.97
19	492+69.50	23.91' LT	608.98
20	492+72.49	25.04' LT	MATCH EX.
21	492+72.28	23.35' LT	MATCH EX.
22	492+64.70	25.89' LT	608.90
23	492+64.76	27.77' LT	608.89
24	492+62.26	29.61' LT	MATCH EX.
25	492+61.23	28.35' LT	MATCH EX.
26	492+65.18	39.68' LT	608.68

40TH STREET



NOTE:

LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.

THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 10 SY
 SODDING SALT TOLERANT : 10 SY

CONTROL POINTS:

BENCHMARK	CUT CROSS
ELEV: 608.44	ELEV: 609.76
STA: 492+53.74	STA: 491+54.12
OS: 68.02' LT	OS: 45.12' LT
N: 1877123.636	N: 1877024.980
E: 1128877.023	E: 1128903.770

SOUTHWEST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 40TH STREET

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	492+23.73	42.04' LT	MATCH EX.
2	492+22.17	42.37' LT	MATCH EX.
3	492+24.04	44.38' LT	608.42
4	492+22.45	44.37' LT	608.41
5	492+10.69	44.28' LT	609.03
6	492+05.85	44.25' LT	MATCH EX.
7	492+05.85	49.25' LT	MATCH EX.
8	492+05.86	54.25' LT	MATCH EX.
9	492+10.70	54.28' LT	MATCH EX.
10	492+10.69	49.28' LT	609.00
11	492+22.68	49.37' LT	608.33
12	492+24.24	49.38' LT	608.34
13	492+22.75	51.37' LT	MATCH EX.
14	492+24.31	51.32' LT	MATCH EX.



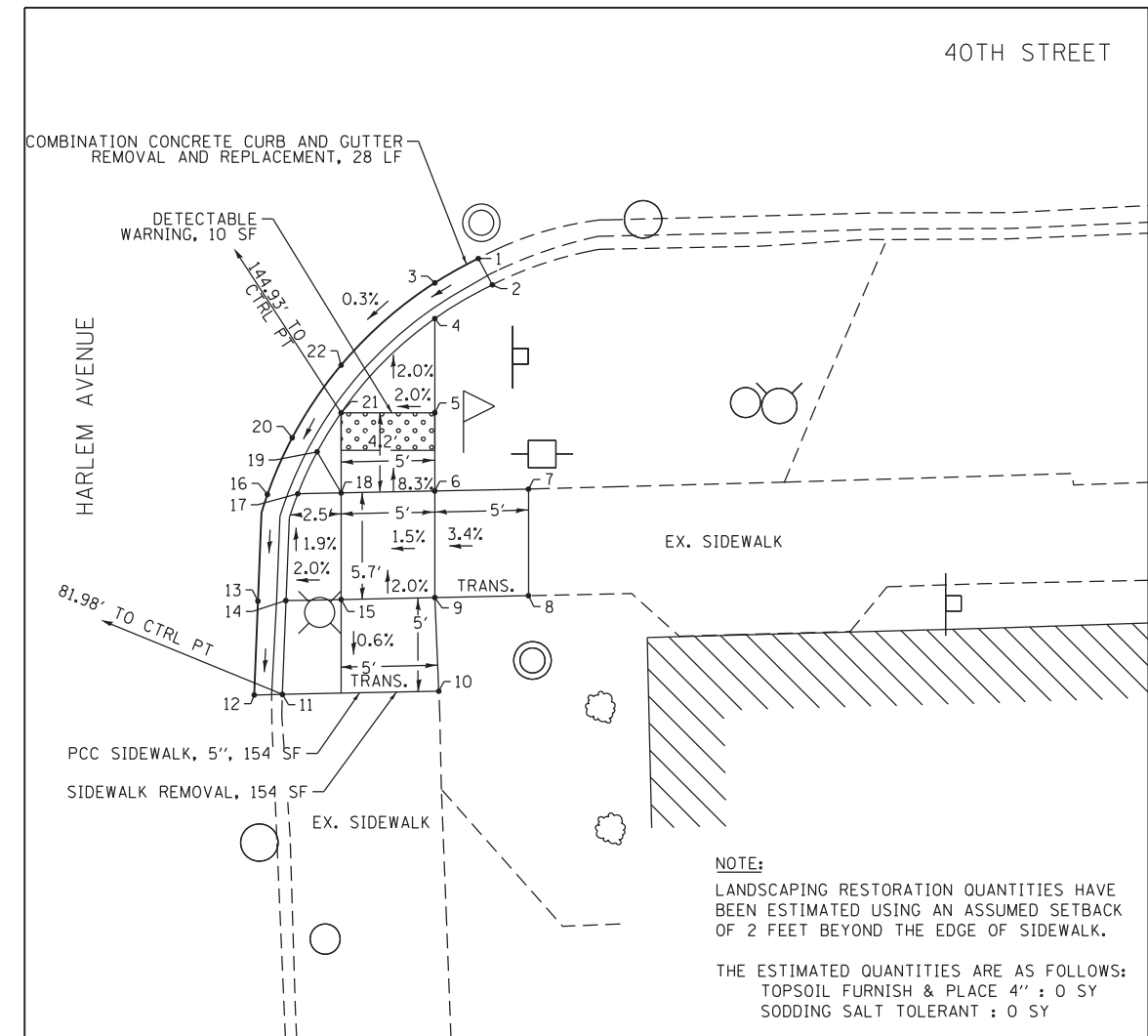
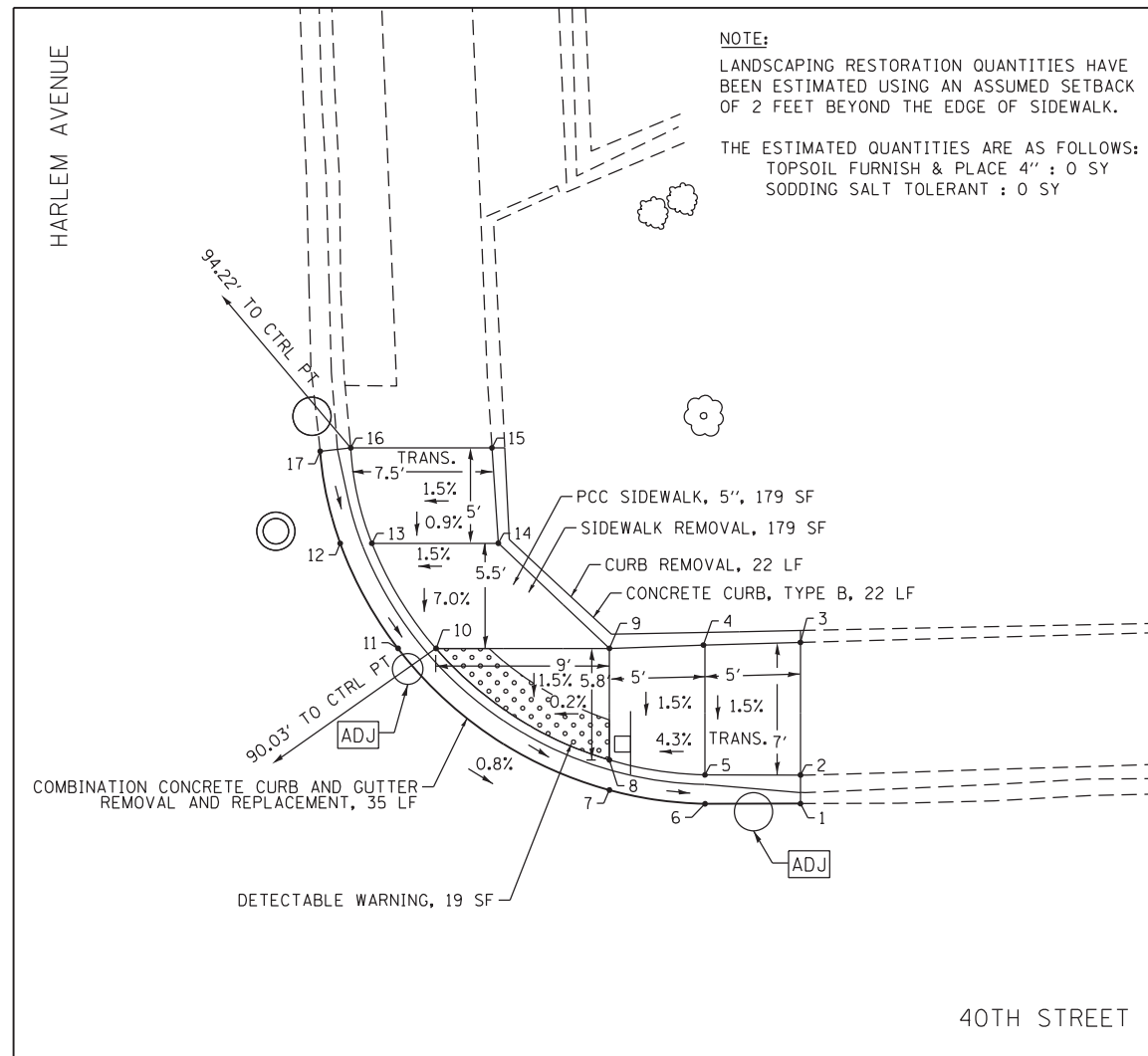
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PLOT SCALE = 10.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 10/17/2016	CHECKED - MTC	REVISED -
	DATE - 10/17/2016	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**IL ROUTE 43 (HARLEM AVENUE)
 ADA RAMP DETAILS**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	(2424 & 3138) RS-3	COOK	75	24
CONTRACT NO. 62C19				
ILLINOIS FED. AID PROJECT				

SCALE: 1" = 5' SHEET 8 OF 38 SHEETS STA. TO STA.



CONTROL POINTS:
CUT CROSS ELEV: 609.01
STA: 494+41.07
OS: 45.88' LT
N: 1877311.693
E: 1128891.888

NORTHEAST CORNER
IL ROUTE 43 (HARLEM AVENUE) AND 40TH STREET

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	493+67.17	53.88' RT	MATCH EX.
2	493+68.66	53.94' RT	MATCH EX.
3	493+75.60	54.21' RT	MATCH EX.
4	493+75.66	49.21' RT	608.45
5	493+68.87	49.94' RT	608.35
6	493+67.35	49.88' RT	608.10
7	493+68.27	43.92' RT	608.15
8	493+69.84	43.98' RT	608.14
9	493+75.68	44.20' RT	608.27
10	493+76.03	35.13' RT	608.25
11	493+76.11	33.14' RT	608.26
12	493+81.72	30.33' RT	608.33
13	493+81.66	31.90' RT	608.70
14	493+81.41	38.61' RT	608.80
15	493+86.41	38.47' RT	MATCH EX.
16	493+86.70	31.08' RT	MATCH EX.
17	493+86.58	29.48' RT	MATCH EX.

CONTROL POINTS:
CUT CROSS ELEV: 609.01
STA: 494+41.07
OS: 45.88' LT
N: 1877311.693
E: 1128891.888

SOUTHEAST CORNER
IL ROUTE 43 (HARLEM AVENUE) AND 40TH STREET

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	493+29.19	41.87' RT	MATCH EX.
2	493+27.76	42.57' RT	MATCH EX.
3	493+27.99	39.50' RT	608.67
4	493+26.08	39.43' RT	608.66
5	493+21.06	39.23' RT	608.76
6	493+16.88	39.07' RT	609.11
7	493+16.79	44.07' RT	MATCH EX.
8	493+11.11	43.85' RT	MATCH EX.
9	493+11.20	38.85' RT	609.23
10	493+06.20	38.87' RT	MATCH EX.
11	493+06.35	30.52' RT	MATCH EX.
12	493+06.38	29.01' RT	MATCH EX.
13	493+11.38	29.40' RT	608.56
14	493+11.35	30.91' RT	609.09
15	493+11.29	33.85' RT	609.15
16	493+17.05	30.13' RT	608.58
17	493+17.02	31.75' RT	608.98
18	493+16.98	34.07' RT	609.03
19	493+19.22	32.88' RT	608.87
20	493+20.02	31.57' RT	608.60
21	493+21.25	34.24' RT	608.66
22	493+23.79	34.33' RT	608.65



USER NAME = WTeng
PLOT SCALE = 10.0000' / in.
PLOT DATE = 10/17/2016

DESIGNED -
DRAWN -
CHECKED - MTC
DATE - 10/17/2016

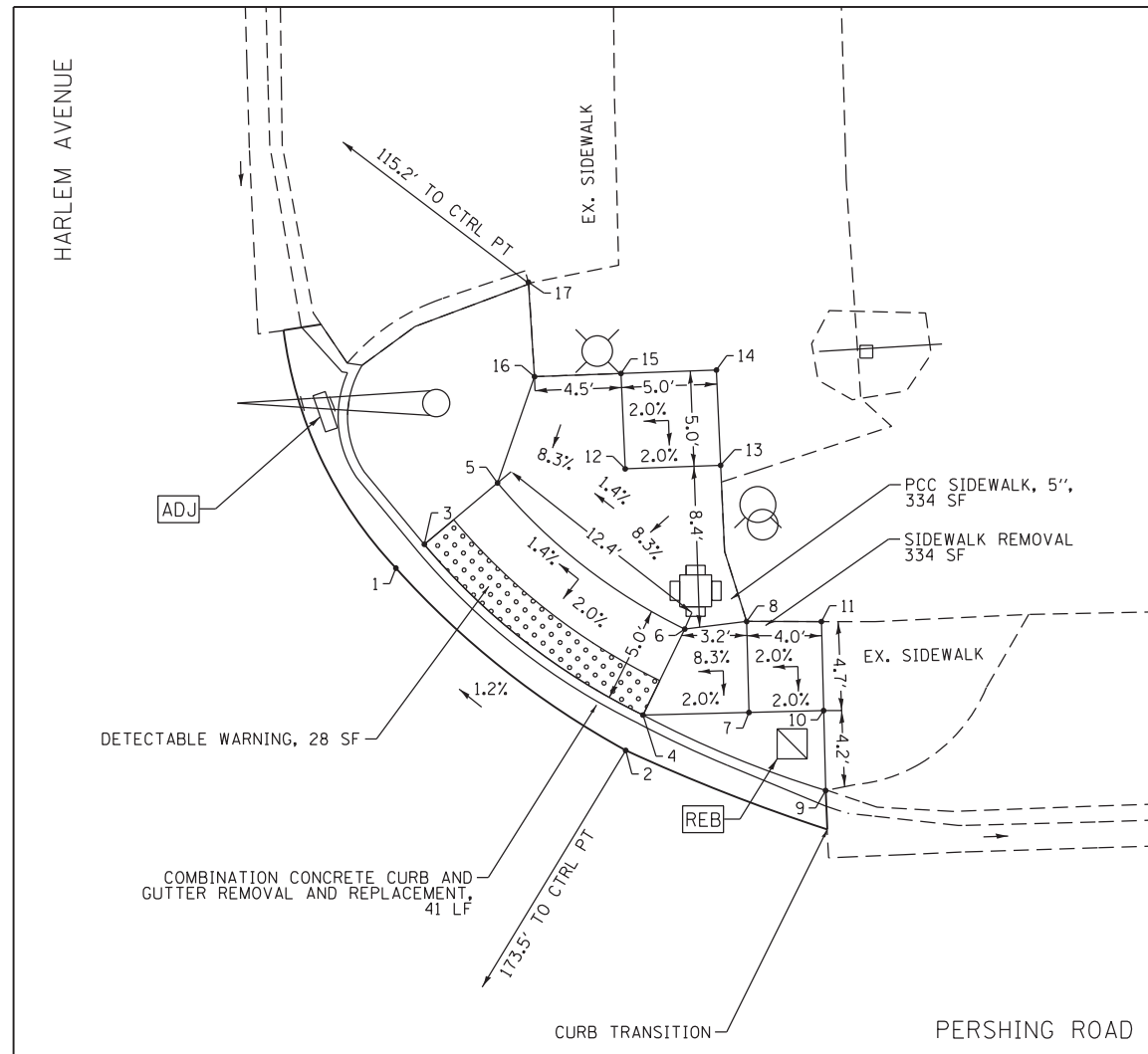
REVISED -
REVISED -
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REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL ROUTE 43 (HARLEM AVENUE)
ADA RAMP DETAILS

SCALE: 1" = 5' SHEET 9 OF 38 SHEETS STA. TO STA.

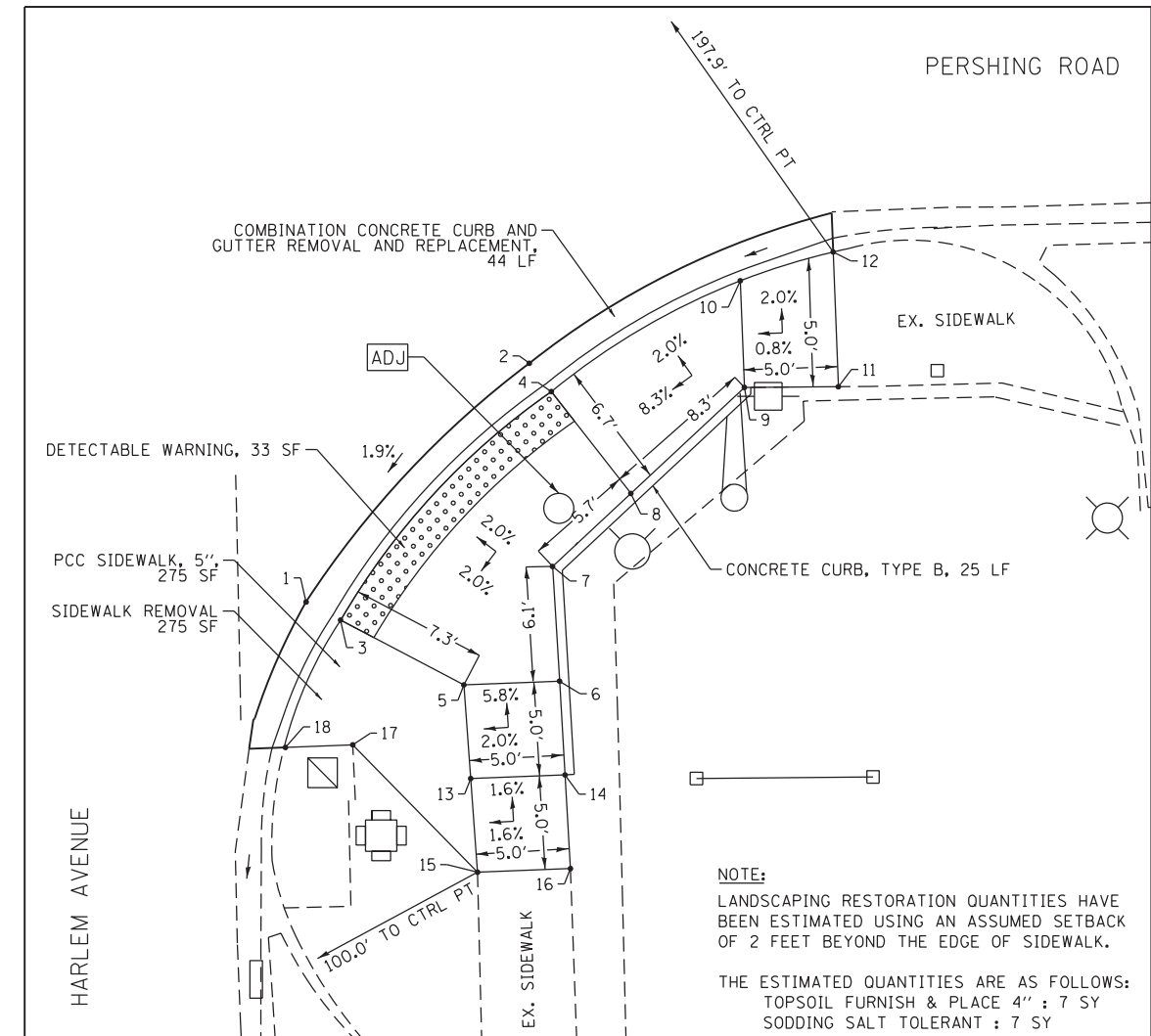
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	(2424 & 3138) RS-3	COOK	75	25
CONTRACT NO. 62C19				
ILLINOIS FED. AID PROJECT				



CONTROL POINTS:
 CUT CROSS ELEV: 608.44 STA: 501+23.71 OS: 45.50' LT N: 1877994.518 E: 1128867.502
 CUT CROSS ELEV: 608.67 STA: 498+81.77 OS: 46.34' LT N: 1877752.037 E: 1128874.336

NORTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND PERSHING ROAD

POINT No.:	STATION	OFFSET	ELEV.
1	500+37.04	37.14' RT	608.09
2	500+27.20	48.93' RT	608.27
3	500+38.25	38.66' RT	608.06
4	500+29.03	49.89' RT	608.24
5	500+41.37	42.57' RT	608.16
6	500+33.48	52.17' RT	608.34
7	500+29.03	55.42' RT	608.60
8	500+33.79	55.42' RT	608.61
9	500+24.84	59.33' RT	MATCH EX.
10	500+29.03	59.33' RT	MATCH EX.
11	500+33.69	59.33' RT	MATCH EX.
12	500+41.94	49.26' RT	608.69
13	500+42.00	54.26' RT	MATCH EX.
14	500+47.00	54.17' RT	MATCH EX.
15	500+46.94	49.17' RT	MATCH EX.
16	500+46.89	44.64' RT	MATCH EX.
17	500+51.74	44.44' RT	MATCH EX.

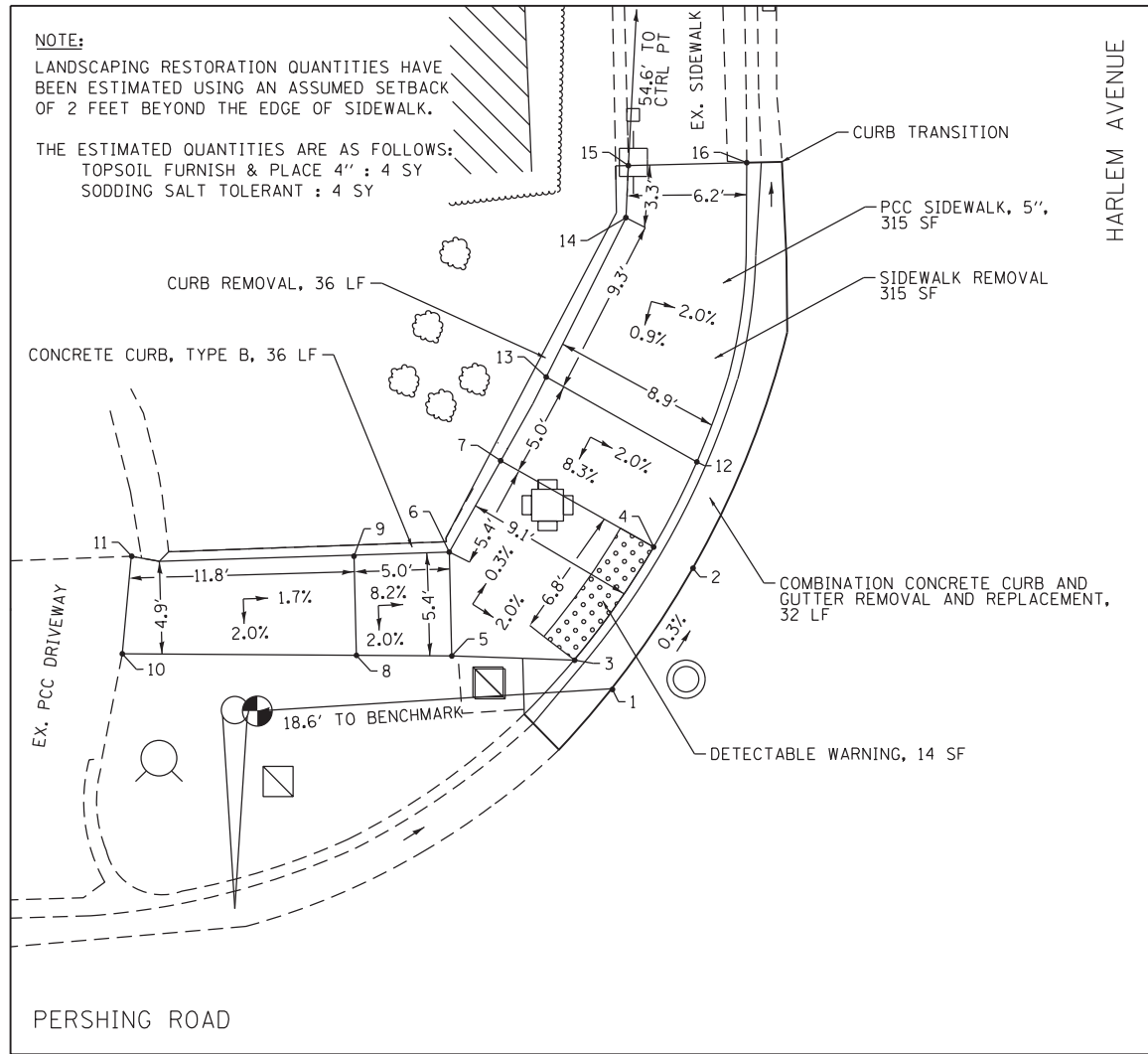


CONTROL POINTS:
 CUT CROSS ELEV: 608.44 STA: 501+23.71 OS: 45.50' LT N: 1877994.518 E: 1128867.502
 CUT CROSS ELEV: 608.67 STA: 498+81.77 OS: 46.34' LT N: 1877752.037 E: 1128874.336

SOUTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND PERSHING ROAD

POINT No.:	STATION	OFFSET	ELEV.
1	499+40.54	34.84' RT	607.90
2	499+52.79	47.21' RT	608.23
3	499+39.51	36.64' RT	607.87
4	499+51.24	48.33' RT	608.21
5	499+35.80	43.06' RT	608.01
6	499+35.80	48.16' RT	608.11
7	499+41.92	48.03' RT	608.20
8	499+45.65	52.34' RT	608.35
9	499+51.06	58.61' RT	609.02
10	499+56.75	58.61' RT	608.91
11	499+50.91	63.61' RT	MATCH EX.
12	499+58.09	63.61' RT	MATCH EX.
13	499+30.80	43.23' RT	608.32
14	499+30.80	48.26' RT	608.40
15	499+25.80	43.40' RT	MATCH EX.
16	499+25.80	48.36' RT	MATCH EX.
17	499+32.84	37.03' RT	MATCH EX.
18	499+32.84	33.36' RT	MATCH EX.

NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 7 SY
 SODDING SALT TOLERANT : 7 SY



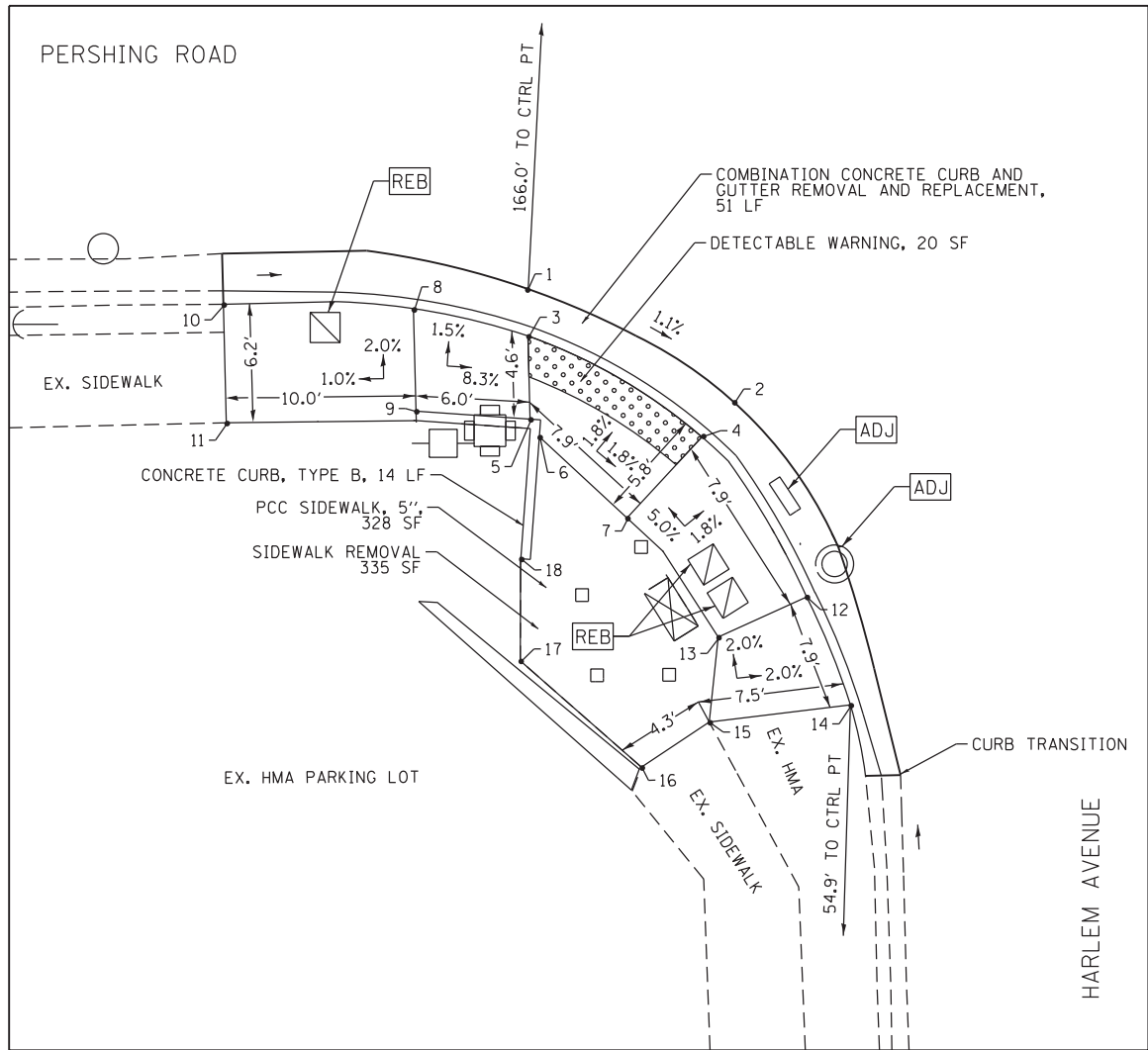
NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 4 SY
 SODDING SALT TOLERANT : 4 SY

CONTROL POINTS:
 CUT CROSS ELEV: 608.44
 STA: 501+23.71
 OS: 45.50' LT
 N: 1877994.518
 E: 1128867.502

BENCHMARK
 MAST ARM FOUNDATION
 ELEV: 608.67
 STA: 500+41.15
 OS: 69.85' LT
 N: 1877911.383
 E: 1128845.202

NORTHWEST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND PERSHING ROAD

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	500+40.86	51.25' LT	607.82
2	500+48.04	46.84' LT	607.80
3	500+43.42	53.17' LT	607.80
4	500+49.24	48.85' LT	607.78
5	500+43.79	59.58' LT	607.90
6	500+49.24	59.58' LT	607.97
7	500+53.96	56.77' LT	607.96
8	500+43.94	64.58' LT	608.28
9	500+49.21	64.58' LT	608.38
10	500+44.33	76.86' LT	MATCH EX.
11	500+49.44	76.27' LT	MATCH EX.
12	500+53.63	46.45' LT	608.19
13	500+58.29	54.27' LT	608.37
14	500+66.55	49.88' LT	MATCH EX.
15	500+69.28	49.69' LT	MATCH EX.
16	500+69.28	43.47' LT	MATCH EX.

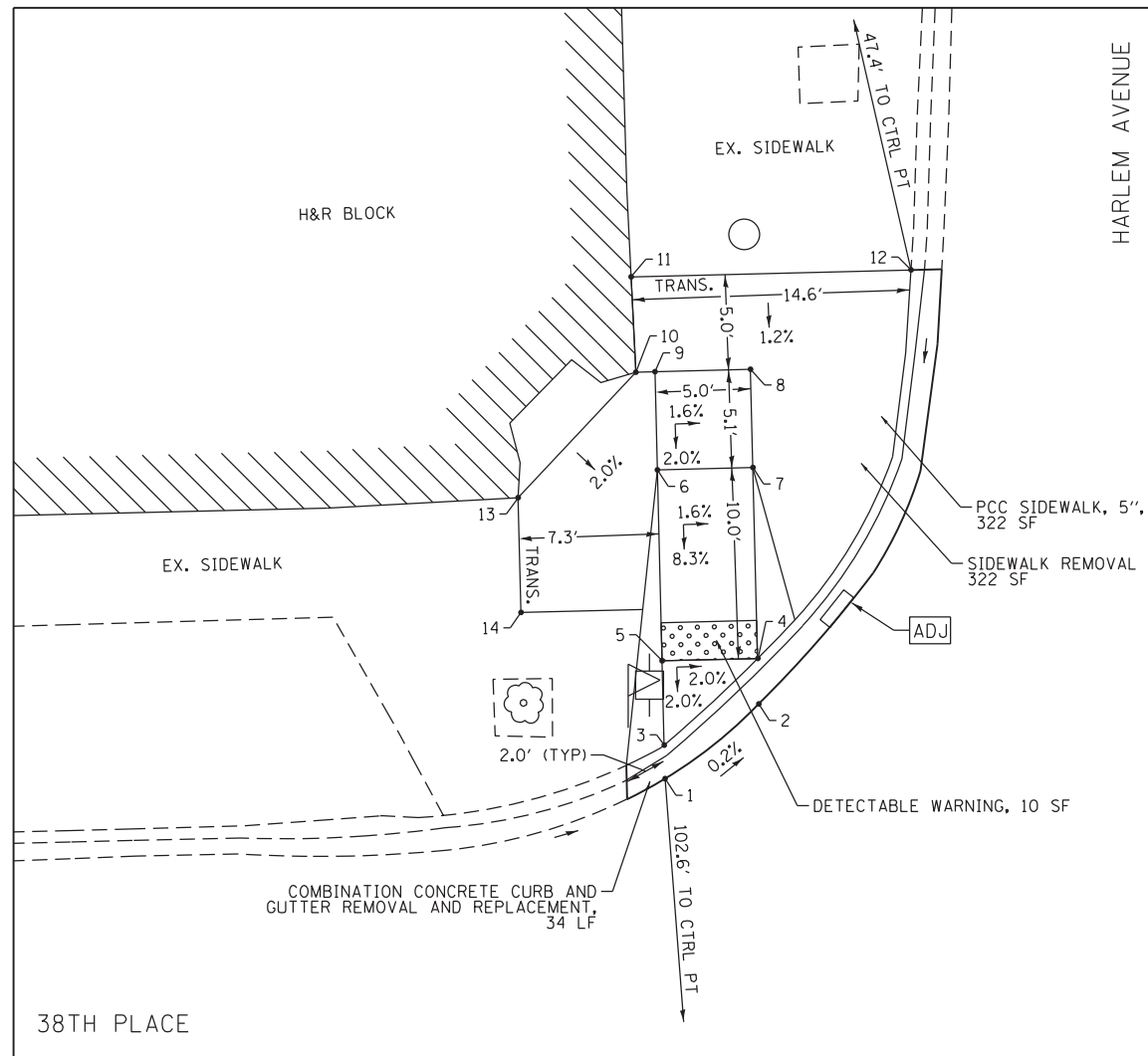


CONTROL POINTS:
 CUT CROSS ELEV: 608.44
 STA: 501+23.71
 OS: 45.50' LT
 N: 1877994.518
 E: 1128867.502

CUT CROSS
 ELEV: 608.67
 STA: 498+81.71
 OS: 46.34' LT
 N: 1877752.037
 E: 1128874.336

SOUTHWEST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND PERSHING ROAD

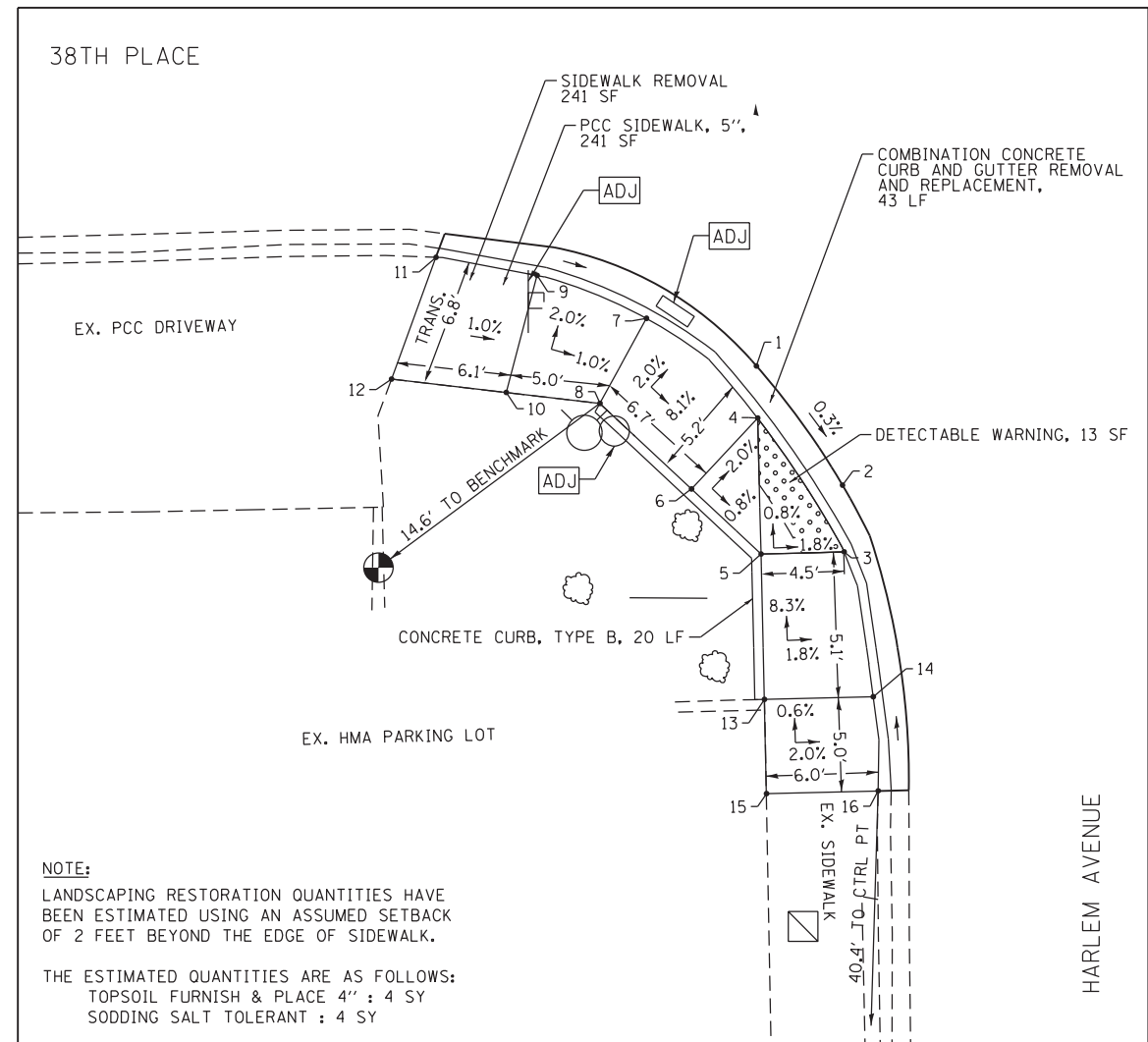
ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	499+59.07	58.73' LT	608.06
2	499+52.71	48.03' LT	607.92
3	499+56.57	58.77' LT	608.04
4	499+50.91	49.83' LT	607.90
5	499+52.19	58.83' LT	608.12
6	499+51.24	58.38' LT	608.12
7	499+46.80	53.93' LT	608.00
8	499+58.25	64.74' LT	608.50
9	499+52.85	64.82' LT	608.59
10	499+58.86	74.74' LT	MATCH EX.
11	499+52.62	74.82' LT	MATCH EX.
12	499+42.28	44.65' LT	608.40
13	499+40.34	49.38' LT	608.46
14	499+36.50	42.56' LT	MATCH EX.
15	499+35.88	50.02' LT	MATCH EX.
16	499+33.64	53.68' LT	MATCH EX.
17	499+39.50	59.84' LT	MATCH EX.
18	499+44.86	59.60' LT	608.88



CONTROL POINTS:
 CUT CROSS ELEV: 608.55
 STA: 504+45.25
 OS: 48.39' LT
 N: 1878315.903
 E: 1128856.656

NORTHWEST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 38TH PLACE

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	503+72.45	52.50' LT	607.67
2	503+76.24	47.50' LT	607.66
3	503+74.21	52.50' LT	607.66
4	503+78.61	47.50' LT	607.65
5	503+78.63	52.50' LT	607.75
6	503+88.63	52.50' LT	608.58
7	503+88.63	47.50' LT	608.50
8	503+93.77	47.50' LT	608.60
9	503+93.77	52.50' LT	608.68
10	503+93.77	53.50' LT	MATCH EX.
11	503+98.77	53.62' LT	MATCH EX.
12	503+98.77	38.97' LT	MATCH EX.
13	503+87.35	59.82' LT	MATCH EX.
14	503+81.34	59.82' LT	MATCH EX.

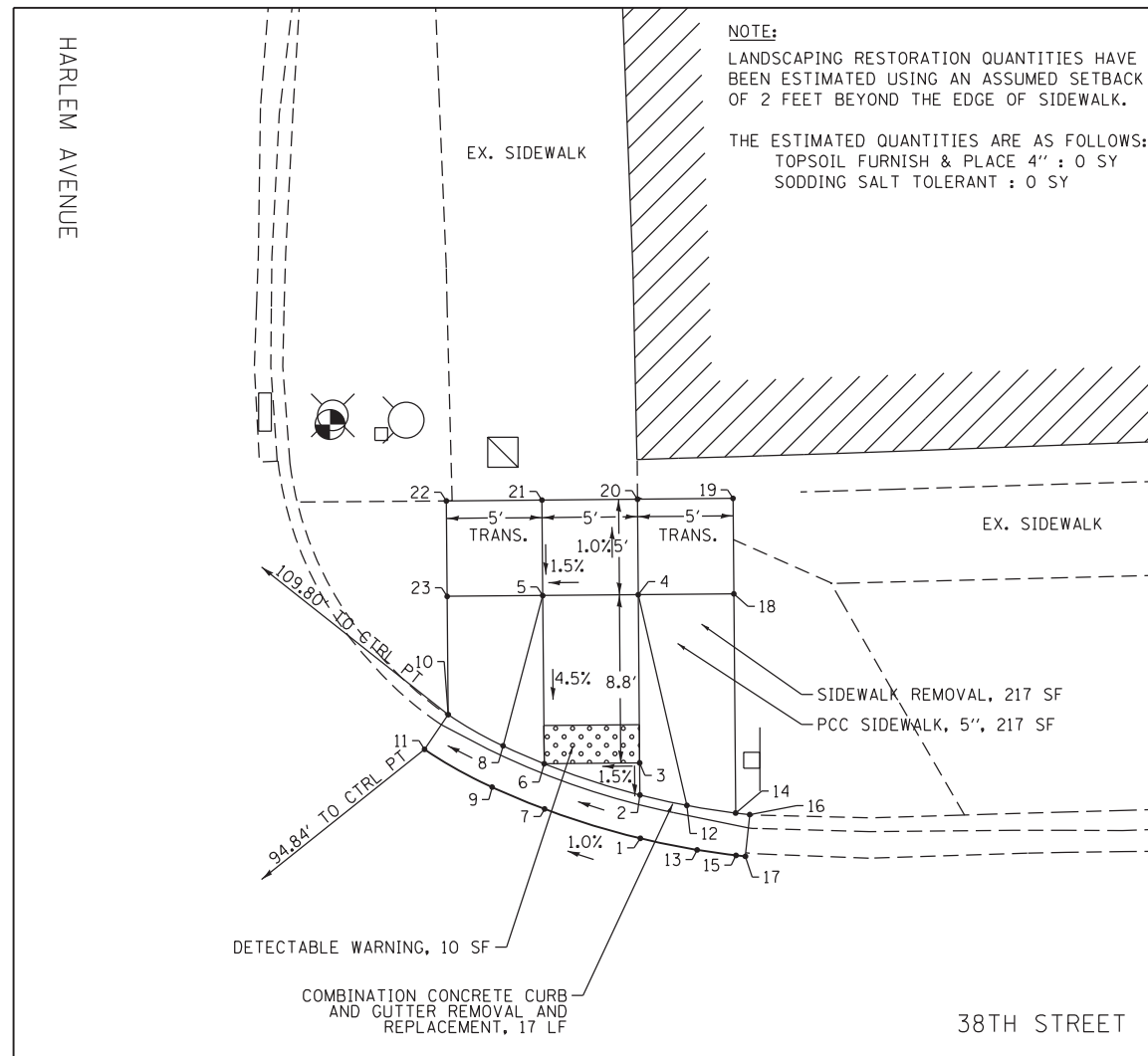


NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 4 SY
 SODDING SALT TOLERANT : 4 SY

CONTROL POINTS:
 BENCHMARK ELEV: 609.22
 STA: 504+45.25
 OS: 71.25' LT
 N: 1878192.975
 E: 1128836.822

SOUTHWEST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 38TH PLACE

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	503+33.07	50.94' LT	607.66
2	503+26.64	46.53' LT	607.64
3	503+23.08	46.53' LT	607.63
4	503+30.30	50.94' LT	607.65
5	503+23.08	50.94' LT	607.71
6	503+26.62	54.55' LT	607.75
7	503+35.73	56.71' LT	608.19
8	503+31.28	59.29' LT	608.29
9	503+38.16	62.46' LT	608.21
10	503+31.98	64.24' LT	608.34
11	503+39.25	67.79' LT	MATCH EX.
12	503+32.85	70.31' LT	MATCH EX.
13	503+15.36	50.96' LT	608.35
14	503+15.36	45.18' LT	608.24
15	503+10.36	50.97' LT	MATCH EX.
16	503+10.36	45.04' LT	MATCH EX.



NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 0 SY
 SODDING SALT TOLERANT : 0 SY

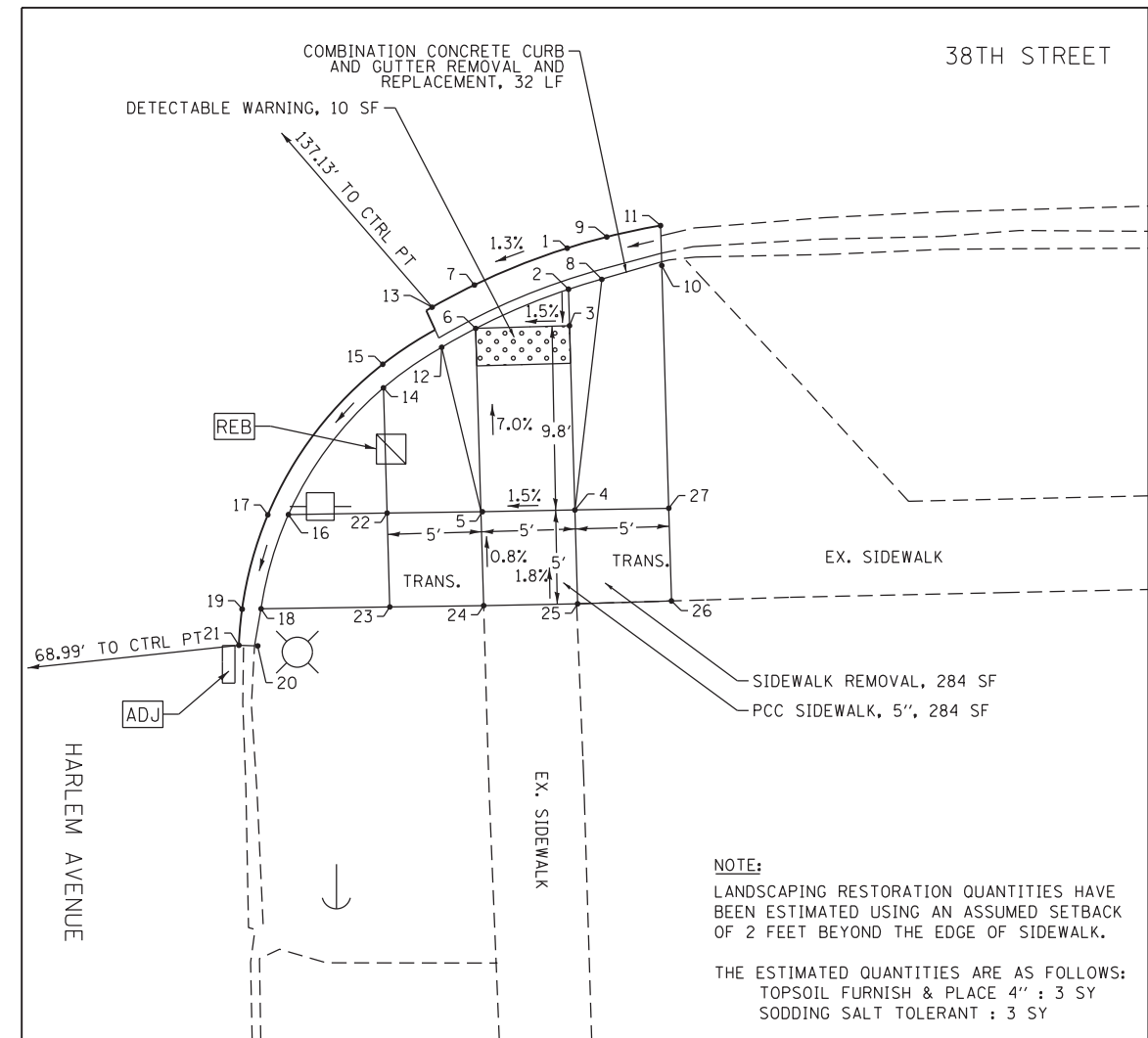
CONTROL POINTS:
 CUT CROSS CUT CROSS
 ELEV: 607.99 ELEV: 608.70
 STA: 506+26.67 STA: 507+56.02
 OS: 43.88' LT OS: 51.45' LT
 N: 1878497.365 N: 1878626.495
 E: 1128856.657 E: 1128845.893

NORTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 38TH STREET

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	506+79.06	42.84' RT	607.79
2	506+81.33	42.88' RT	607.79
3	506+83.01	42.90' RT	607.82
4	506+91.82	43.05' RT	608.22
5	506+91.90	38.05' RT	608.15
6	506+83.09	37.90' RT	607.75
7	506+80.73	37.86' RT	607.74
8	506+84.09	35.77' RT	607.80
9	506+81.94	35.15' RT	607.64
10	506+85.79	32.95' RT	MATCH EX.
11	506+84.00	31.66' RT	MATCH EX.
12	506+80.73	45.32' RT	607.81

NORTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 38TH STREET

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
13	506+78.39	45.79' RT	607.82
14	506+80.27	47.86' RT	608.29
15	506+78.05	47.82' RT	607.98
16	506+80.16	48.59' RT	MATCH EX.
17	506+77.98	48.31' RT	MATCH EX.
18	506+91.74	48.04' RT	MATCH EX.
19	506+96.74	48.13' RT	MATCH EX.
20	506+96.82	43.13' RT	MATCH EX.
21	506+96.90	38.13' RT	MATCH EX.
22	506+96.98	33.13' RT	MATCH EX.
23	506+91.98	33.05' RT	MATCH EX.



CONTROL POINTS:
 CUT CROSS CUT CROSS
 ELEV: 607.99 ELEV: 608.70
 STA: 506+26.67 STA: 507+56.02
 OS: 43.88' LT OS: 51.45' LT
 N: 1878497.365 N: 1878626.495
 E: 1128856.657 E: 1128845.893

SOUTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 38TH STREET

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	506+53.04	42.88' RT	607.87
2	506+50.85	42.89' RT	607.90
3	506+48.91	42.89' RT	607.87
4	506+39.09	42.94' RT	608.56
5	506+39.12	37.94' RT	608.49
6	506+48.89	37.89' RT	607.80
7	506+51.20	37.88' RT	607.80
8	506+51.33	44.67' RT	607.92
9	506+53.58	44.99' RT	607.89
10	506+51.99	47.88' RT	MATCH EX.
11	506+54.12	47.87' RT	MATCH EX.
12	506+47.93	36.06' RT	607.82
13	506+48.86	35.73' RT	607.78
14	506+50.07	35.61' RT	607.82

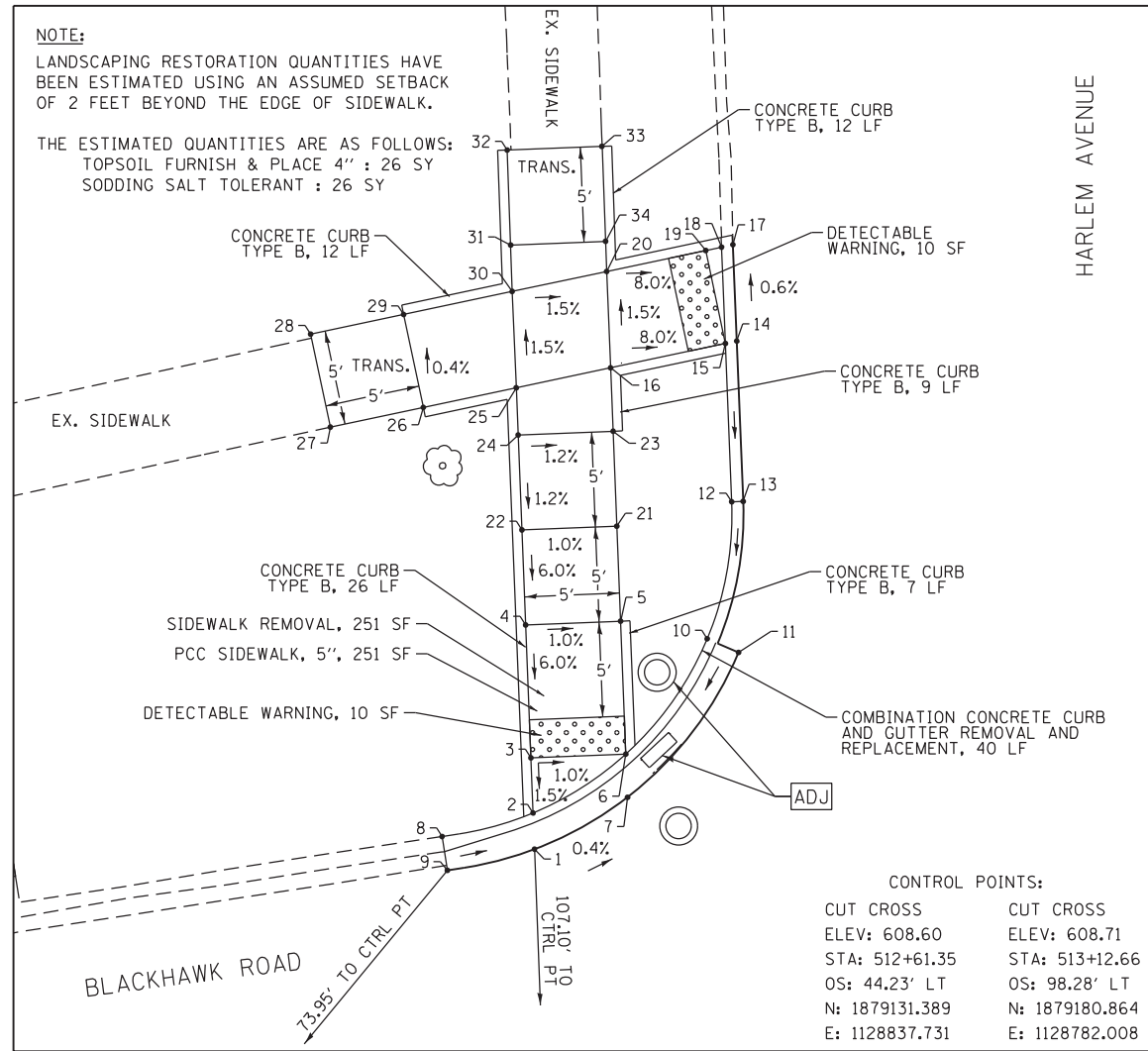
SOUTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 38TH STREET

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
15	506+47.10	32.90' RT	607.55
16	506+39.22	27.68' RT	607.80
17	506+39.23	26.58' RT	607.47
18	506+34.24	26.10' RT	607.88
19	506+34.25	25.09' RT	607.38
20	506+32.26	25.87' RT	MATCH EX.
21	506+32.33	39.7624.88	MATCH EX.
22	506+39.17	32.94' RT	608.39
23	506+34.17	32.96' RT	MATCH EX.
24	506+34.12	37.96' RT	MATCH EX.
25	506+34.09	42.96' RT	MATCH EX.
26	506+34.12	47.96' RT	MATCH EX.
27	506+39.06	47.94' RT	MATCH EX.

NOTE:

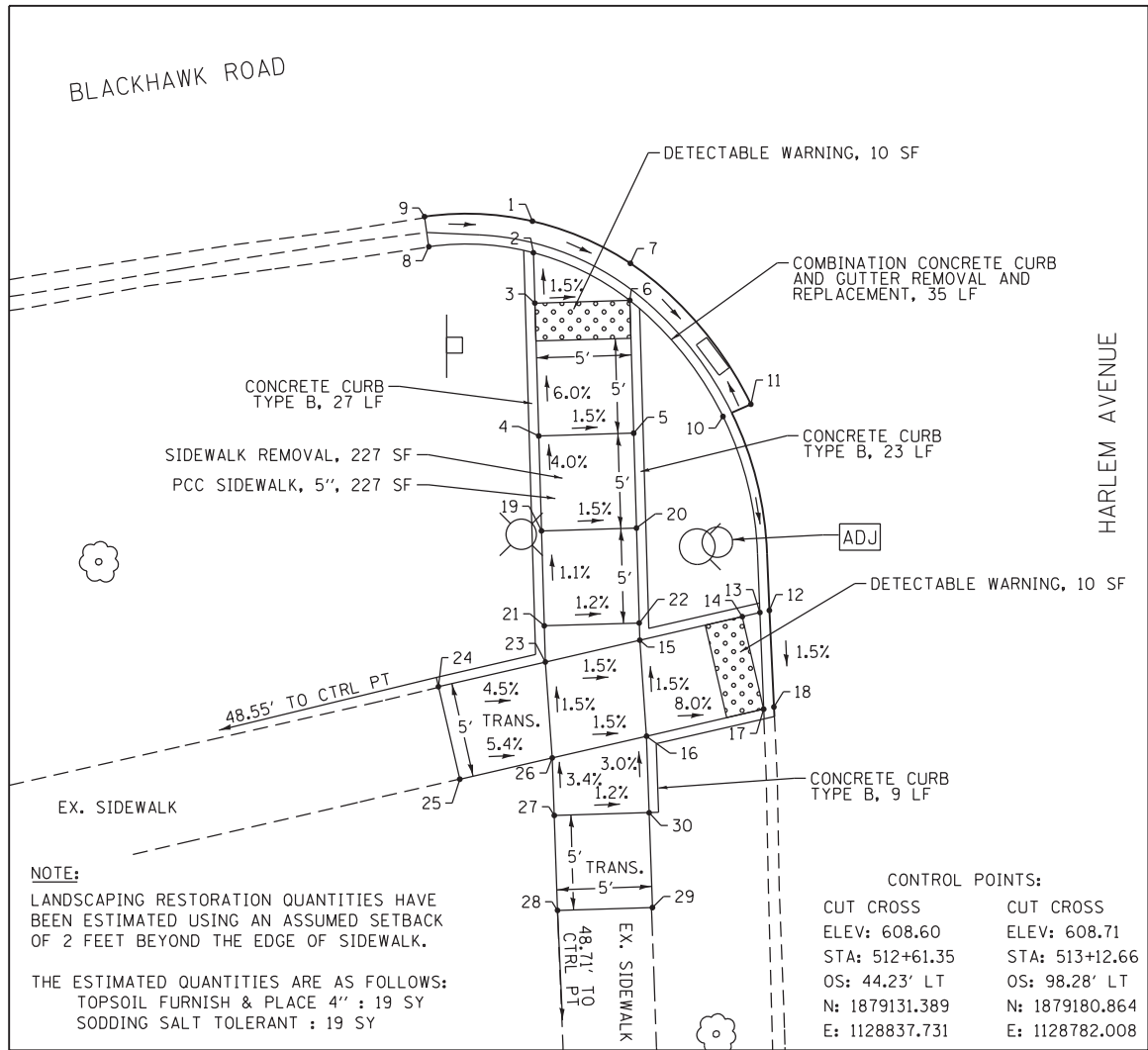
LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.

THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 26 SY
 SODDING SALT TOLERANT : 26 SY



CONTROL POINTS:

CUT CROSS	CUT CROSS
ELEV: 608.60	ELEV: 608.71
STA: 512+61.35	STA: 513+12.66
OS: 44.23' LT	OS: 98.28' LT
N: 1879131.389	N: 1879180.864
E: 1128837.731	E: 1128782.008



CONTROL POINTS:

CUT CROSS	CUT CROSS
ELEV: 608.60	ELEV: 608.71
STA: 512+61.35	STA: 513+12.66
OS: 44.23' LT	OS: 98.28' LT
N: 1879131.389	N: 1879180.864
E: 1128837.731	E: 1128782.008

NORTHWEST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND BLACKHAWK ROAD

ADA RAMP ELEVATION TABLE

POINT No.:	STATION	OFFSET	ELEV.
1	513+69.05	44.72' LT	607.46
2	513+70.96	44.73' LT	607.45
3	513+73.86	44.75' LT	607.49
4	513+80.86	44.79' LT	607.91
5	513+80.89	39.79' LT	607.86
6	513+73.89	39.75' LT	607.44
7	513+71.62	39.74' LT	607.44
8	513+69.88	49.56' LT	MATCH EX.
9	513+68.09	49.34' LT	MATCH EX.
10	513+79.81	35.27' LT	608.03
11	513+79.04	33.64' LT	607.60
12	513+87.18	33.74' LT	608.11
13	513+87.18	33.14' LT	607.65
14	513+95.42	33.19' LT	607.70
15	513+95.31	33.79' LT	607.74
16	513+94.23	39.87' LT	608.23
17	514+00.49	33.23' LT	607.67

NORTHWEST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND BLACKHAWK ROAD

ADA RAMP ELEVATION TABLE

POINT No.:	STATION	OFFSET	ELEV.
18	514+00.38	33.83' LT	607.71
19	514+00.23	34.67' LT	607.72
20	513+99.30	39.92' LT	608.15
21	513+85.89	39.82' LT	608.16
22	513+85.86	44.82' LT	608.21
23	513+90.89	39.85' LT	608.21
24	513+90.86	44.85' LT	608.27
25	513+93.35	44.87' LT	608.30
26	513+92.47	49.79' LT	608.65
27	513+91.60	54.72' LT	MATCH EX.
28	513+96.53	55.59' LT	MATCH EX.
29	513+97.40	50.67' LT	608.63
30	513+98.42	44.91' LT	608.22
31	514+00.86	44.92' LT	608.36
32	514+05.86	44.95' LT	MATCH EX.
33	514+05.89	39.95' LT	MATCH EX.
34	514+00.89	39.92' LT	608.27

SOUTHWEST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND BLACKHAWK ROAD

ADA RAMP ELEVATION TABLE

POINT No.:	STATION	OFFSET	ELEV.
1	513+46.38	44.89' LT	607.45
2	513+44.72	44.89' LT	607.43
3	513+42.07	44.90' LT	607.47
4	513+35.07	44.93' LT	607.89
5	513+35.05	39.93' LT	607.82
6	513+42.05	39.90' LT	607.40
7	513+44.05	39.89' LT	607.40
8	513+45.22	50.39' LT	MATCH EX.
9	513+46.81	50.56' LT	MATCH EX.
10	513+35.77	35.20' LT	607.98
11	513+36.36	33.71' LT	607.67
12	513+25.99	33.10' LT	607.64
13	513+25.89	33.60' LT	607.64
14	513+25.20	34.53' LT	607.65
15	513+24.13	39.97' LT	608.09

SOUTHWEST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND BLACKHAWK ROAD

ADA RAMP ELEVATION TABLE

POINT No.:	STATION	OFFSET	ELEV.
16	513+19.08	39.78' LT	608.17
17	513+20.29	33.57' LT	607.67
18	513+20.39	33.07' LT	607.63
19	513+30.07	44.95' LT	608.09
20	513+30.05	39.95' LT	608.02
21	513+25.07	44.97' LT	608.14
22	513+25.05	39.97' LT	608.08
23	513+23.16	44.98' LT	608.17
24	513+22.04	50.65' LT	MATCH EX.
25	513+17.14	49.693' LT	MATCH EX.
26	513+18.10	44.78' LT	608.25
27	513.15.06	44.78' LT	608.35
28	513+10.06	44.77' LT	MATCH EX.
29	513+10.04	39.77' LT	MATCH EX.
30	513+15.04	39.78' LT	608.29



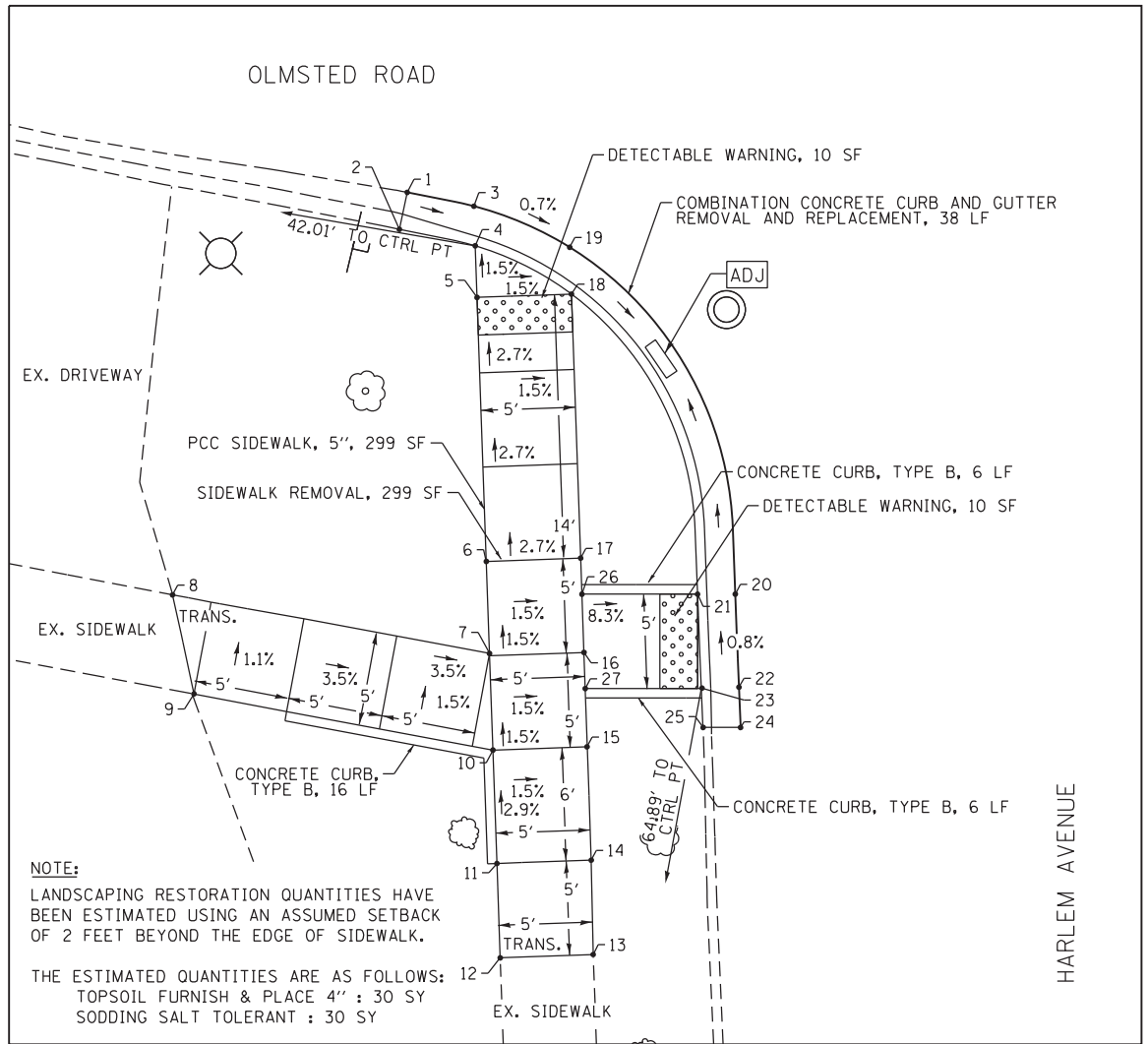
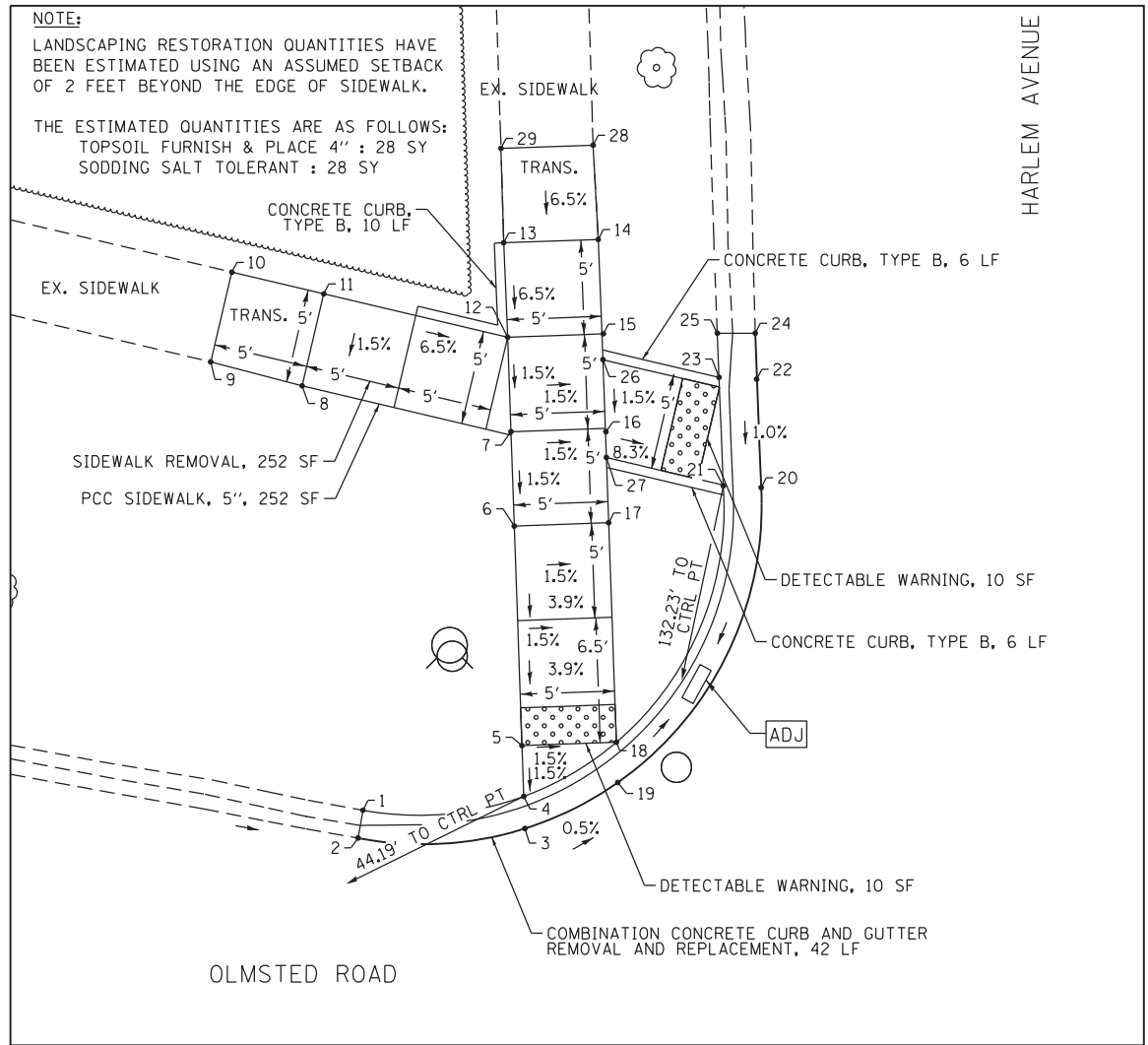
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PLOT DATE = 10/17/2016	CHECKED - MTC	REVISED -
	DATE - 10/17/2016	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**IL ROUTE 43 (HARLEM AVENUE)
 ADA RAMP DETAILS**

SCALE: 1" = 5' SHEET 14 OF 38 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	(2424 & 3138) RS-3	COOK	75	30
CONTRACT NO. 62C19				
ILLINOIS FED. AID PROJECT				



CONTROL POINTS:
CUT CROSS CUT CROSS
ELEV: 608.93 ELEV: 608.72
STA: 516+62.89 STA: 517+61.91
OS: 41.00' LT OS: 85.97' LT
N: 1879532.806 N: 1879629.493
E: 1128827.603 E: 1128778.673

CONTROL POINTS:
CUT CROSS CUT CROSS
ELEV: 608.93 ELEV: 608.72
STA: 516+62.89 STA: 517+61.91
OS: 41.00' LT OS: 85.97' LT
N: 1879532.806 N: 1879629.493
E: 1128827.603 E: 1128778.673

NORTHWEST CORNER
IL ROUTE 43 (HARLEM AVENUE) AND OLMSTED ROAD

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	517+78.61	53.76' LT	MATCH EX.
2	517+77.15	54.07' LT	MATCH EX.
3	517+77.32	45.23' LT	608.19
4	517+79.02	45.22' LT	608.18
5	517+81.71	45.22' LT	608.22
6	517+93.34	45.19' LT	608.65
7	517+98.34	45.18' LT	608.73
8	518+01.17	56.14' LT	609.37
9	518+02.62	60.92' LT	MATCH EX.
10	518+07.32	59.63' LT	MATCH EX.
11	518+05.99	54.81' LT	609.45
12	518+03.34	45.17' LT	608.80
13	518+08.34	45.19' LT	609.12
14	518+08.32	40.18' LT	608.99

NORTHWEST CORNER
IL ROUTE 43 (HARLEM AVENUE) AND OLMSTED ROAD

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
15	518+03.32	40.17' LT	608.73
16	517+98.32	40.18' LT	608.65
17	517+93.32	40.19' LT	608.58
18	517+81.70	40.22' LT	608.15
19	517+79.57	40.22' LT	608.16
20	517+95.78	32.05' LT	608.24
21	517+95.09	34.05' LT	608.23
22	518+01.05	32.06' LT	608.29
23	518+01.12	34.06' LT	608.28
24	518+03.06	32.07' LT	MATCH EX.
25	518+03.13	34.07' LT	MATCH EX.
26	518+01.96	40.17' LT	608.71
27	517+96.78	40.15' LT	608.64
28	518+13.32	40.26' LT	MATCH EX.
29	518+13.34	45.15' LT	MATCH EX.

SOUTHWEST CORNER
IL ROUTE 43 (HARLEM AVENUE) AND OLMSTED ROAD

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	517+54.20	48.78' LT	MATCH EX.
2	517+52.25	49.26' LT	MATCH EX.
3	517+53.34	45.28' LT	608.24
4	517+54.25	45.27' LT	608.23
5	517+48.51	45.27' LT	608.27
6	517+34.52	45.24' LT	608.64
7	517+29.52	45.23' LT	608.71
8	517+33.30	61.91' LT	MATCH EX.
9	517+28.02	60.96' LT	MATCH EX.
10	517+24.52	45.23' LT	608.80
11	517+18.51	45.21' LT	608.93
12	517+13.51	45.21' LT	MATCH EX.
13	517+13.52	40.28' LT	MATCH EX.

SOUTHWEST CORNER
IL ROUTE 43 (HARLEM AVENUE) AND OLMSTED ROAD

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
14	517+18.52	40.21' LT	608.86
15	517+24.53	40.23' LT	608.72
16	517+29.53	40.23' LT	608.63
17	517+32.53	40.24' LT	608.57
18	517+48.52	40.27' LT	608.19
19	517+51.00	40.27' LT	608.20
20	517+32.64	32.12' LT	608.10
21	517+32.41	34.12' LT	608.09
22	517+27.30	32.09' LT	608.14
23	517+27.36	34.09' LT	608.13
24	517+25.28	32.08' LT	MATCH EX.
25	517+25.36	34.08' LT	MATCH EX.
26	517+32.62	40.24' LT	608.60
27	517+27.61	40.23' LT	608.68



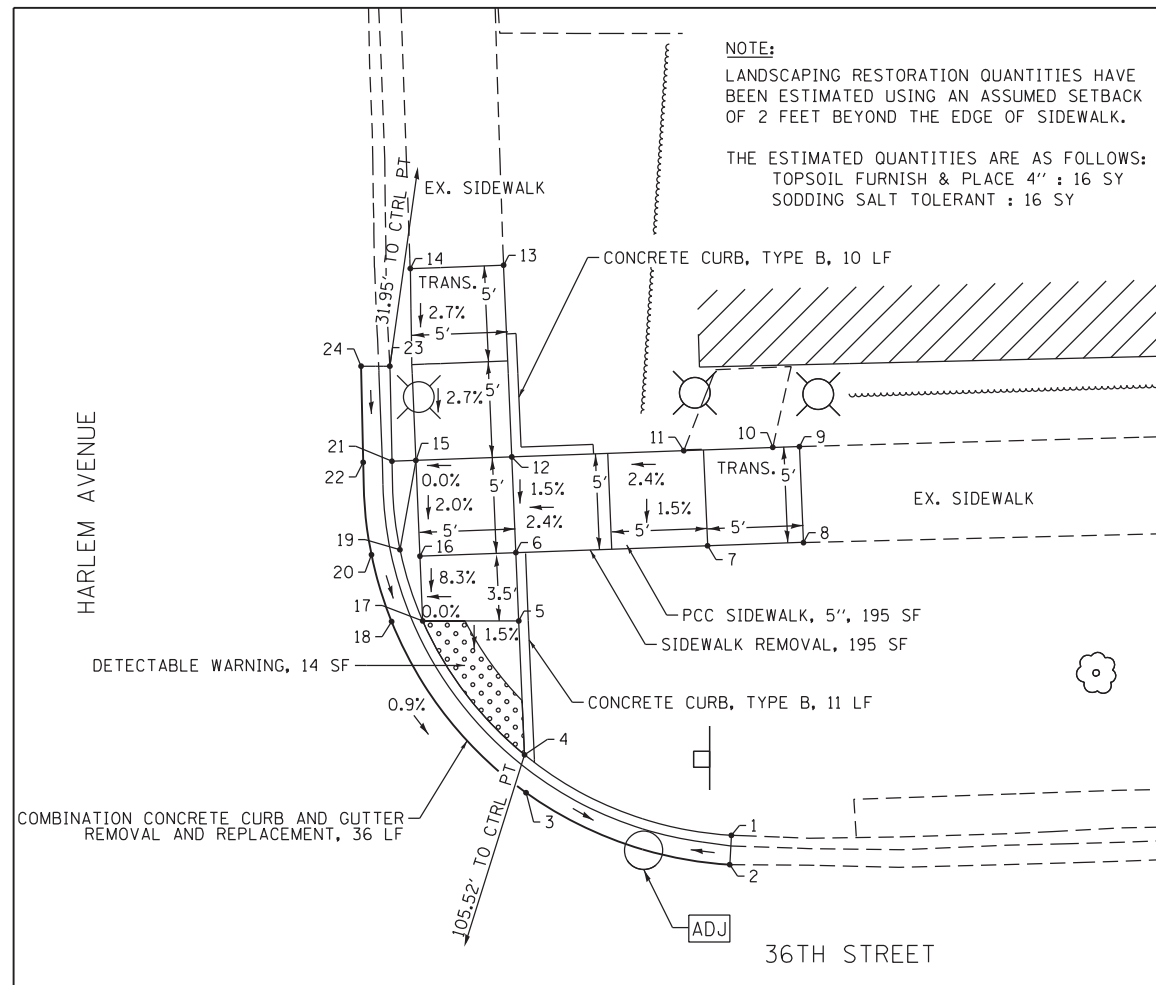
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PLOT DATE = 10/17/2016	DATE - 10/17/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL ROUTE 43 (HARLEM AVENUE)
ADA RAMP DETAILS**

SCALE: 1" = 5' SHEET 15 OF 38 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	(2424 & 3138) RS-3	COOK	75	31
CONTRACT NO. 62C19				
ILLINOIS FED. AID PROJECT				

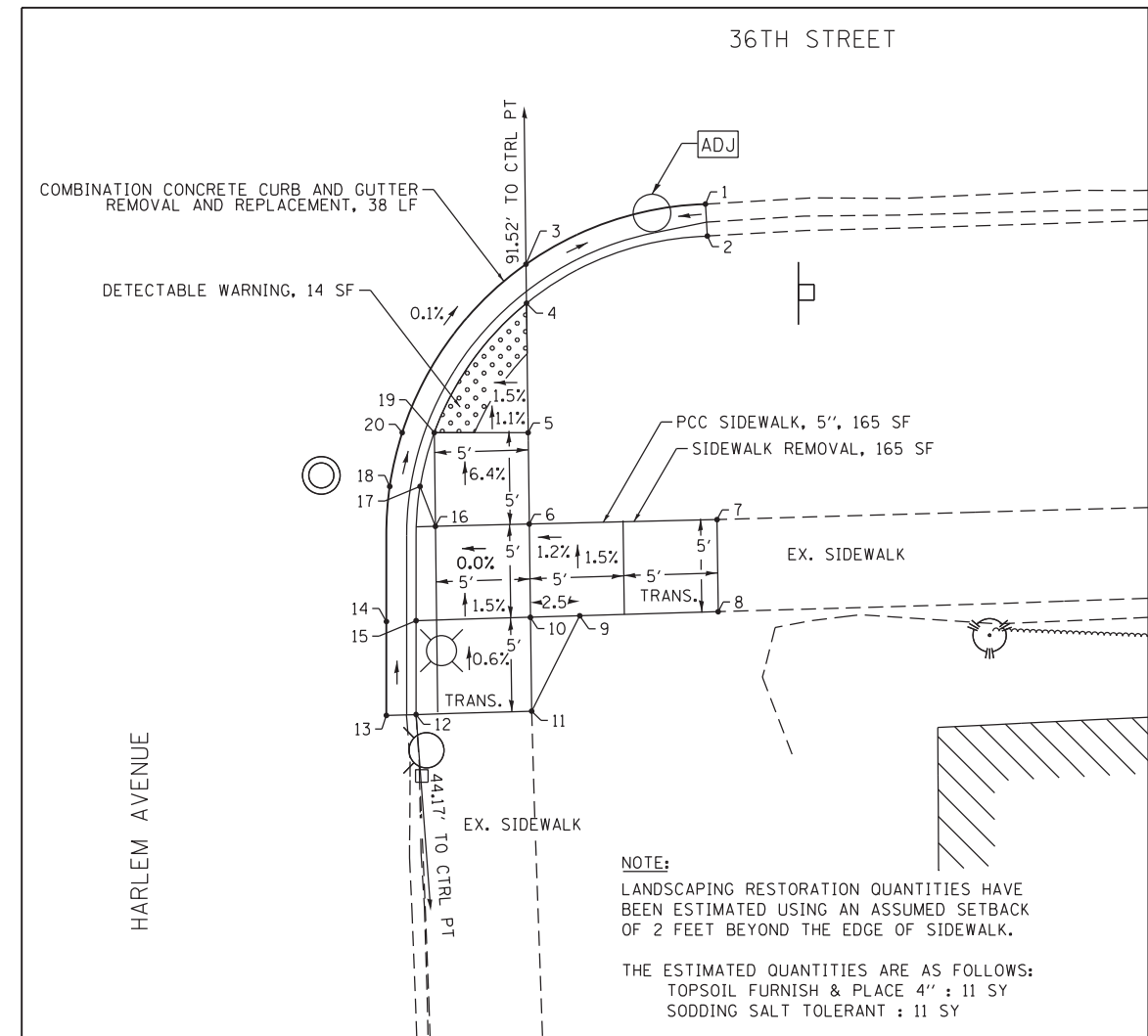


NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 16 SY
 SODDING SALT TOLERANT : 16 SY

CONTROL POINTS:
 CUT CROSS ELEV: 609.5
 STA: 520+67.45
 OS: 37.48' RT
 N: 1879940.364
 E: 1128891.246

NORTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 36TH STREET

POINT No.:	STATION	OFFSET	ELEV.
1	520+11.38	48.78' RT	MATCH EX.
2	520+09.86	48.64' RT	MATCH EX.
3	520+14.00	38.17' RT	608.83
4	520+15.98	38.16' RT	608.82
5	520+22.96	38.12' RT	608.92
6	520+26.52	38.10' RT	609.21
7	520+26.51	48.10' RT	609.47
8	520+26.50	53.10' RT	MATCH EX.
9	520+31.49	53.08' RT	MATCH EX.
10	520+31.50	51.69' RT	609.57
11	520+31.51	47.05' RT	609.54
12	520+31.52	38.08' RT	609.31
13	520+41.52	38.03' RT	MATCH EX.
14	520+41.52	33.16' RT	MATCH EX.
15	520+31.52	33.08' RT	609.31
16	520+26.52	33.10' RT	609.21
17	520+23.15	33.12' RT	608.92
18	520+23.21	31.49' RT	608.93
19	520+26.90	32.07' RT	609.33
20	520+26.69	30.58' RT	608.95
21	520+31.52	31.83' RT	609.31
22	520+31.52	30.33' RT	608.96
23	520+36.48	31.91' RT	MATCH EX.
24	520+36.54	30.41' RT	MATCH EX.

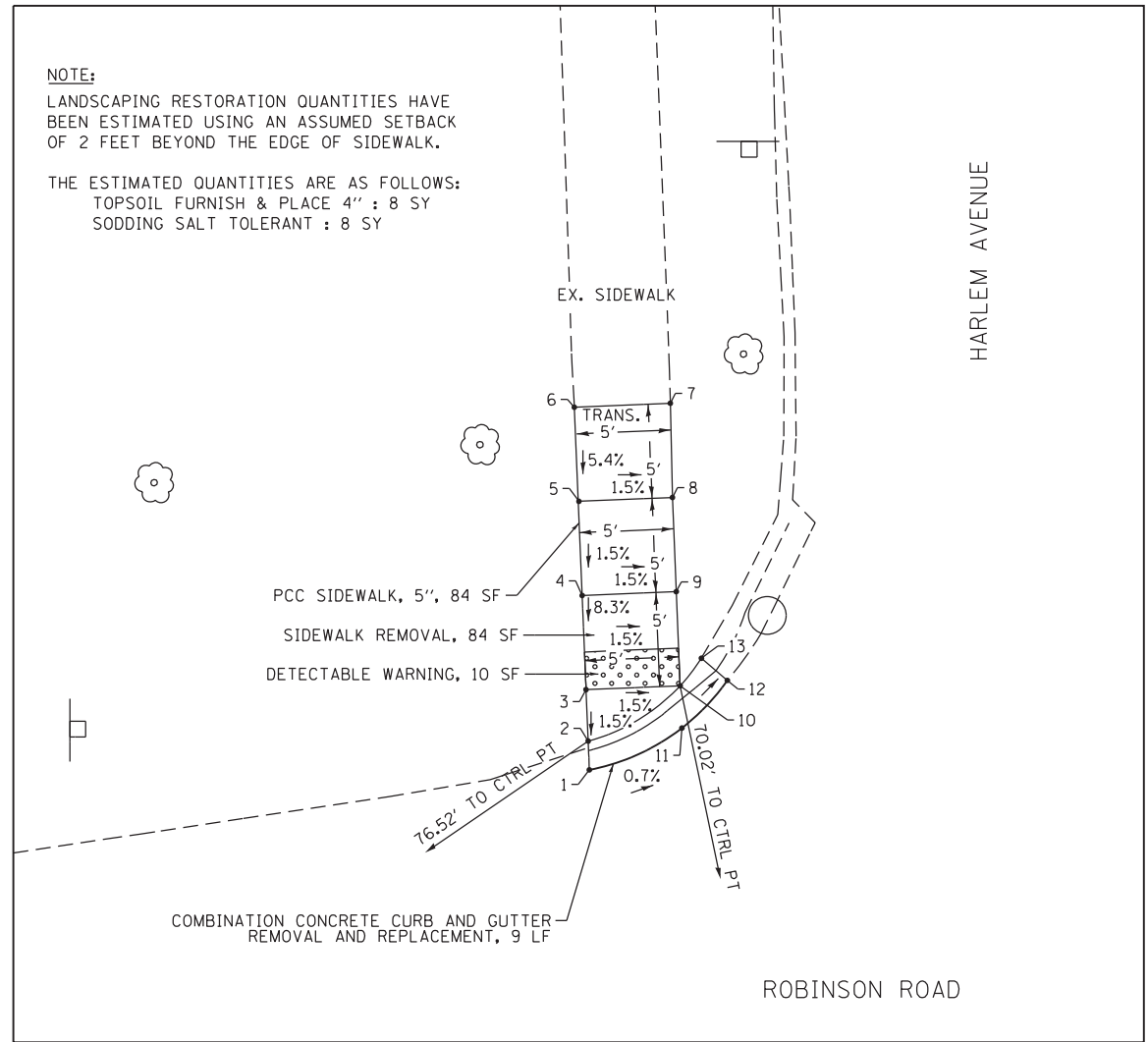


NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 11 SY
 SODDING SALT TOLERANT : 11 SY

CONTROL POINTS:
 CUT CROSS ELEV: 609.5
 STA: 520+67.45
 OS: 37.48' RT
 N: 1879940.364
 E: 1128891.246

SOUTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 36TH STREET

POINT No.:	STATION	OFFSET	ELEV.
1	519+81.35	48.09' RT	MATCH EX.
2	519+79.64	48.13' RT	MATCH EX.
3	519+78.51	38.43' RT	608.92
4	519+76.43	38.38' RT	608.91
5	519+69.54	38.20' RT	608.99
6	519+64.67	38.08' RT	609.31
7	519+64.53	48.08' RT	MATCH EX.
8	519+59.62	47.96' RT	MATCH EX.
9	519+59.68	40.59' RT	609.42
10	519+59.70	37.96' RT	609.38
11	519+54.70	37.83' RT	MATCH EX.
12	519+54.75	31.68' RT	MATCH EX.
13	519+54.77	30.10' RT	MATCH EX.
14	519+59.76	30.28' RT	609.00
15	519+59.75	31.87' RT	609.38
16	519+64.74	33.08' RT	609.31
17	519+66.89	32.35' RT	609.35
18	519+66.95	30.73' RT	608.97
19	519+69.72	33.21' RT	608.92
20	519+69.79	31.50' RT	608.93

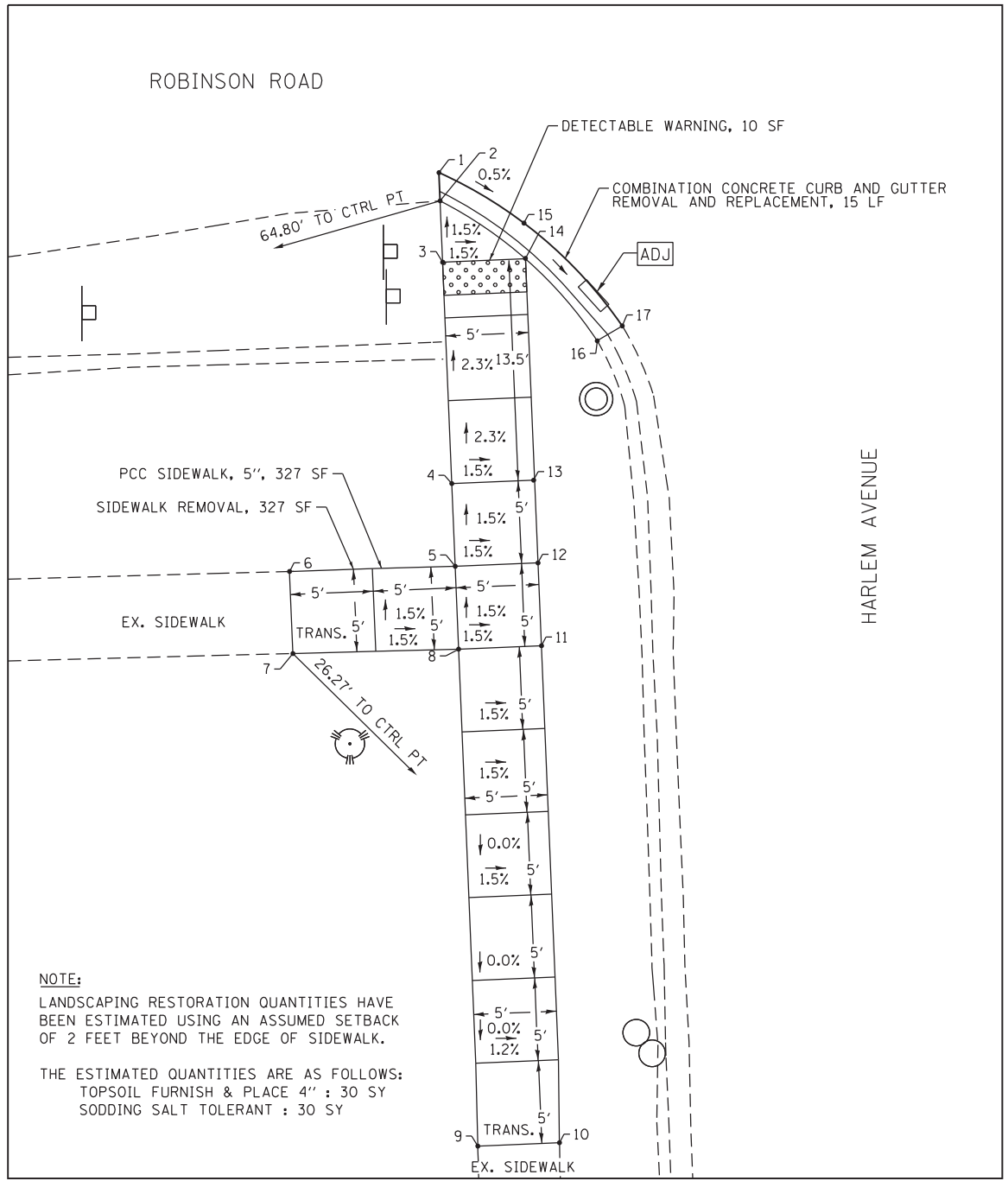


NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 8 SY
 SODDING SALT TOLERANT : 8 SY

CONTROL POINTS:
 CUT CROSS CUT CROSS
 ELEV: 609.41 ELEV: 609.06
 STA: 522+10.98 STA: 521+90.17
 OS: 104.98' LT OS: 32.59' LT
 N: 1880077.969 N: 1880059.815
 E: 1128743.530 E: 1128816.679

NORTHWEST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND ROBINSON ROAD

POINT No.:	STATION	OFFSET	ELEV.
1	522+55.60	43.99' LT	608.71
2	522+57.13	43.99' LT	608.70
3	522+59.86	44.00' LT	608.74
4	522+64.86	44.02' LT	609.16
5	522+69.86	44.04' LT	609.24
6	522+75.46	44.06' LT	MATCH EX.
7	522+74.88	38.96' LT	MATCH EX.
8	522+69.88	39.04' LT	609.17
9	522+64.88	39.02' LT	609.09
10	522+59.88	39.00' LT	608.66
11	522+57.63	38.99' LT	608.67
12	522+60.07	36.49' LT	MATCH EX.
13	522+61.30	37.82' LT	MATCH EX.



NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 30 SY
 SODDING SALT TOLERANT : 30 SY

CONTROL POINTS:
 CUT CROSS CUT CROSS
 ELEV: 609.41 ELEV: 609.06
 STA: 522+10.98 STA: 521+90.17
 OS: 104.98' LT OS: 32.59' LT
 N: 1880077.969 N: 1880059.815
 E: 1128743.530 E: 1128816.679

SOUTHWEST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND ROBINSON ROAD

POINT No.:	STATION	OFFSET	ELEV.
1	522+34.33	43.90' LT	608.70
2	522+32.62	43.90' LT	608.69
3	522+29.92	43.88' LT	608.74
4	522+15.56	43.83' LT	609.05
5	522+10.56	43.81' LT	609.13
6	522+10.62	53.81' LT	MATCH EX.
7	522+05.67	53.79' LT	MATCH EX.
8	522+05.56	43.79' LT	609.21
9	521+75.56	43.76' LT	MATCH EX.

SOUTHWEST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND ROBINSON ROAD

POINT No.:	STATION	OFFSET	ELEV.
10	521+75.58	38.83' LT	MATCH EX.
11	522+05.58	38.79' LT	609.13
12	522+10.58	38.81' LT	609.05
13	522+15.58	38.83' LT	608.97
14	522+28.94	38.88' LT	608.66
15	522+31.09	38.89' LT	608.67
16	522+23.83	34.74' LT	MATCH EX.
17	522+24.67	33.20' LT	MATCH EX.



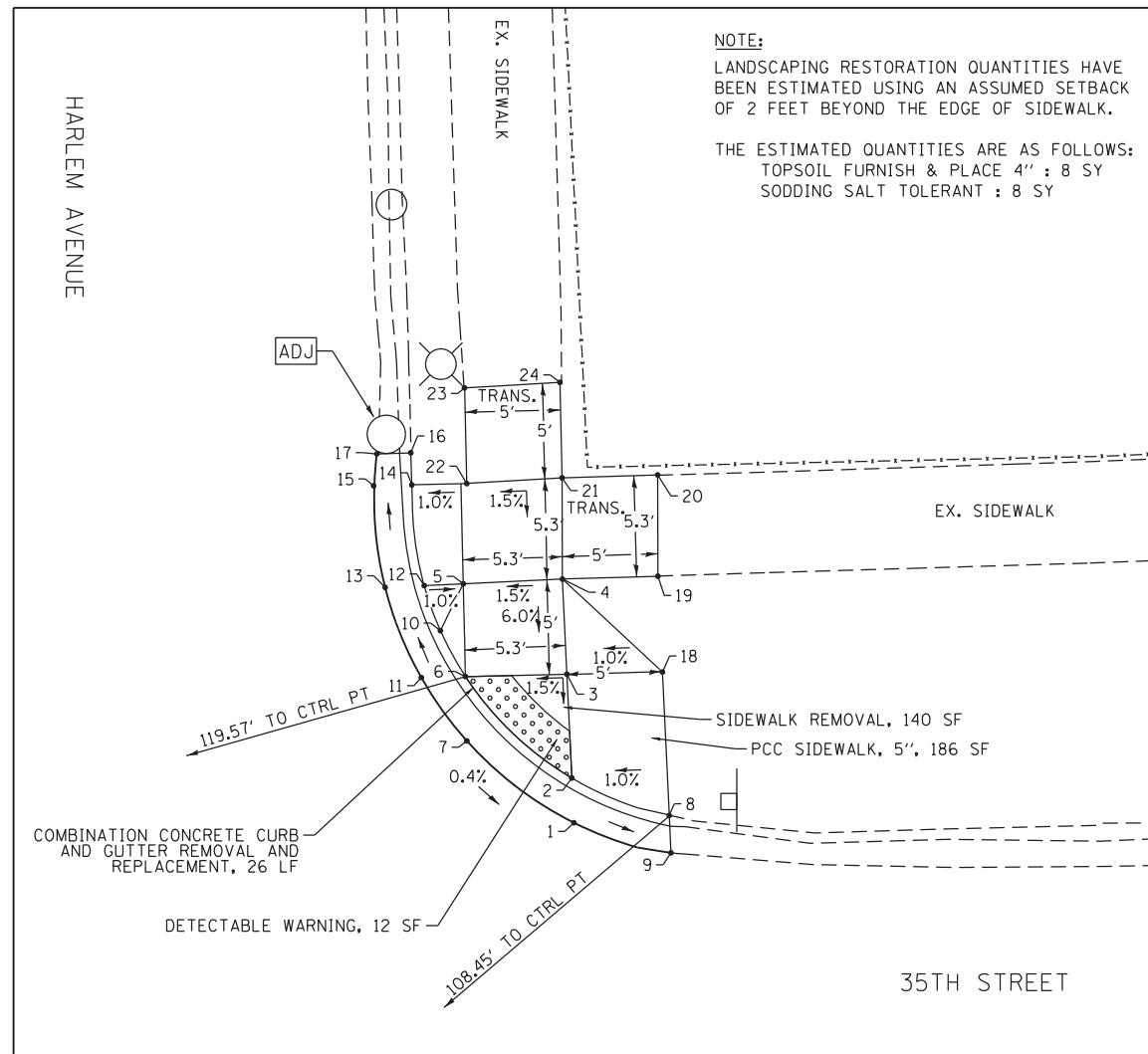
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PLOT DATE = 10/17/2016	DATE - 10/17/2016	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

IL ROUTE 43 (HARLEM AVENUE)
 ADA RAMP DETAILS

SCALE: 1" = 5' SHEET 17 OF 38 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	(2424 & 3138) RS-3	COOK	75	33
CONTRACT NO. 62C19				
ILLINOIS FED. AID PROJECT				



NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 8 SY
 SODDING SALT TOLERANT : 8 SY

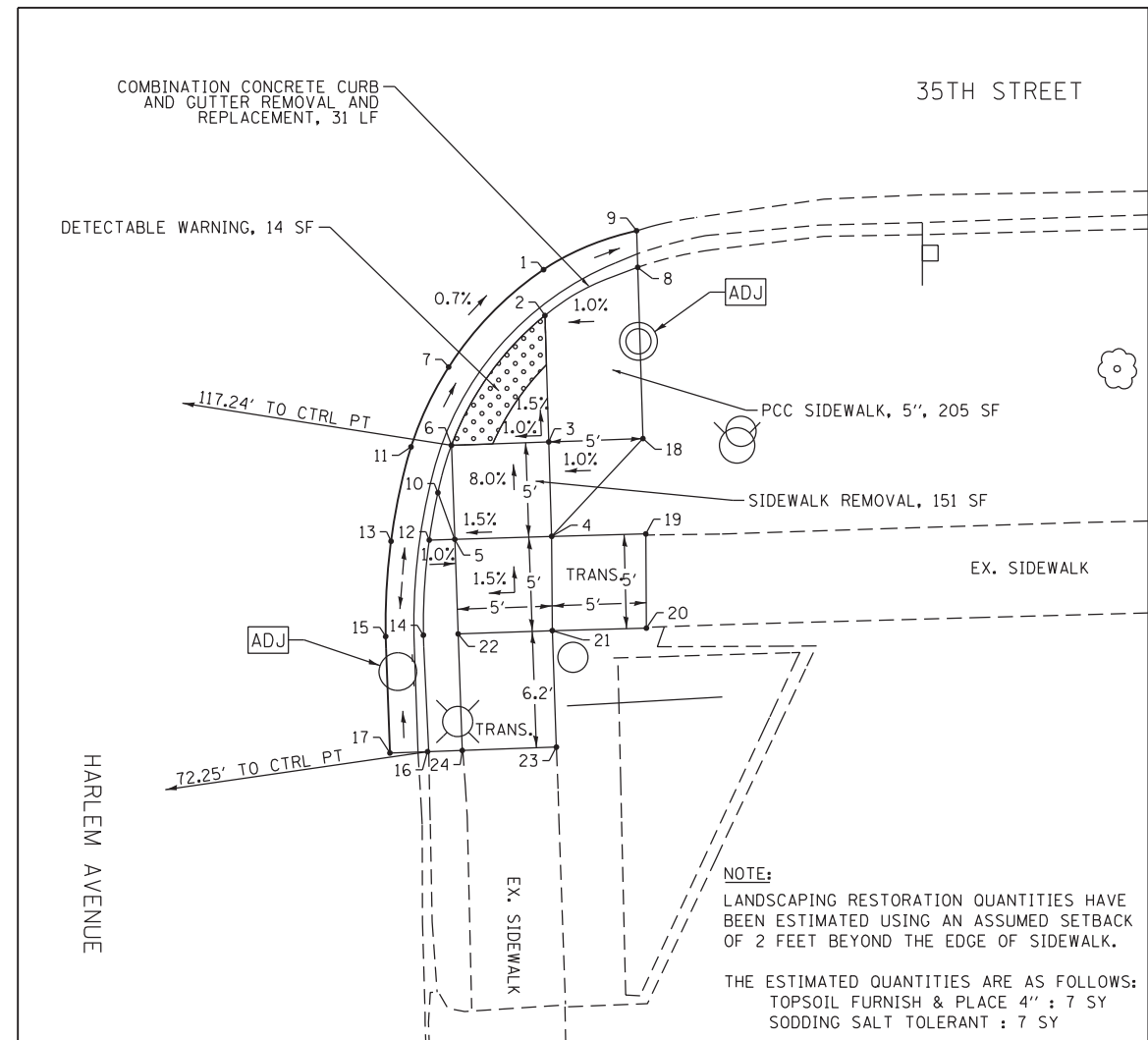
CONTROL POINTS:
 CUT CROSS ELEV: 609.31
 STA: 526+10.63
 OS: 38.57' LT
 N: 1880479.819
 E: 1128794.930

NORTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 35TH STREET

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	526+77.60	41.62' RT	608.45
2	526+79.94	41.60' RT	608.48
3	426+85.38	41.54' RT	608.57
4	526+90.37	41.49' RT	608.86
5	526+90.31	36.30' RT	608.78
6	526+85.46	36.23' RT	608.49
7	526+82.08	36.18' RT	608.48
8	526+77.76	46.62' RT	MATCH EX.
9	526+75.70	46.64' RT	MATCH EX.
10	526+87.90	35.01' RT	608.65
11	526+85.68	33.82' RT	608.51
12	526+90.29	34.25' RT	608.80

NORTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 35TH STREET

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
13	526+90.27	32.20' RT	608.60
14	526+95.59	33.79' RT	608.83
15	526+95.62	31.79' RT	608.49
16	526+97.27	33.80' RT	MATCH EX.
17	526+97.28	31.80' RT	MATCH EX.
18	526+85.31	46.54' RT	608.62
19	526+90.34	46.49' RT	MATCH EX.
20	526+95.64	46.67' RT	MATCH EX.
21	526+95.67	41.67' RT	608.94
22	526+95.55	36.67' RT	608.86
23	527+00.56	36.74' RT	MATCH EX.
24	527+00.67	41.74' RT	MATCH EX.



NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 7 SY
 SODDING SALT TOLERANT : 7 SY

CONTROL POINTS:
 CUT CROSS ELEV: 609.31
 STA: 526+10.63
 OS: 38.57' LT
 N: 1880479.819
 E: 1128794.930

SOUTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 35TH STREET

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	526+43.67	40.37' RT	608.78
2	526+41.23	40.36' RT	608.77
3	526+34.52	40.31' RT	608.87
4	526+29.51	40.27' RT	609.27
5	526+29.54	35.13' RT	609.21
6	526+34.53	35.14' RT	608.81
7	526+38.69	35.15' RT	608.83
8	526+43.75	45.37' RT	MATCH EX.
9	526+45.83	45.39' RT	MATCH EX.
10	526+32.05	34.33' RT	609.02
11	526+35.29	33.29' RT	608.88
12	526+29.55	33.77' RT	609.22

SOUTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 35TH STREET

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
13	526+29.46	31.73' RT	608.82
14	526+24.52	33.27' RT	609.26
15	526+24.52	31.27' RT	608.84
16	526+18.34	33.26' RT	MATCH EX.
17	526+18.34	31.26' RT	MATCH EX.
18	526+34.51	45.31' RT	608.92
19	526+29.45	45.27' RT	MATCH EX.
20	526+24.45	45.12' RT	MATCH EX.
21	526+24.51	40.12' RT	609.34
22	526+24.52	35.12' RT	609.28
23	526+18.32	40.11' RT	MATCH EX.
24	526+18.33	35.11' RT	MATCH EX.

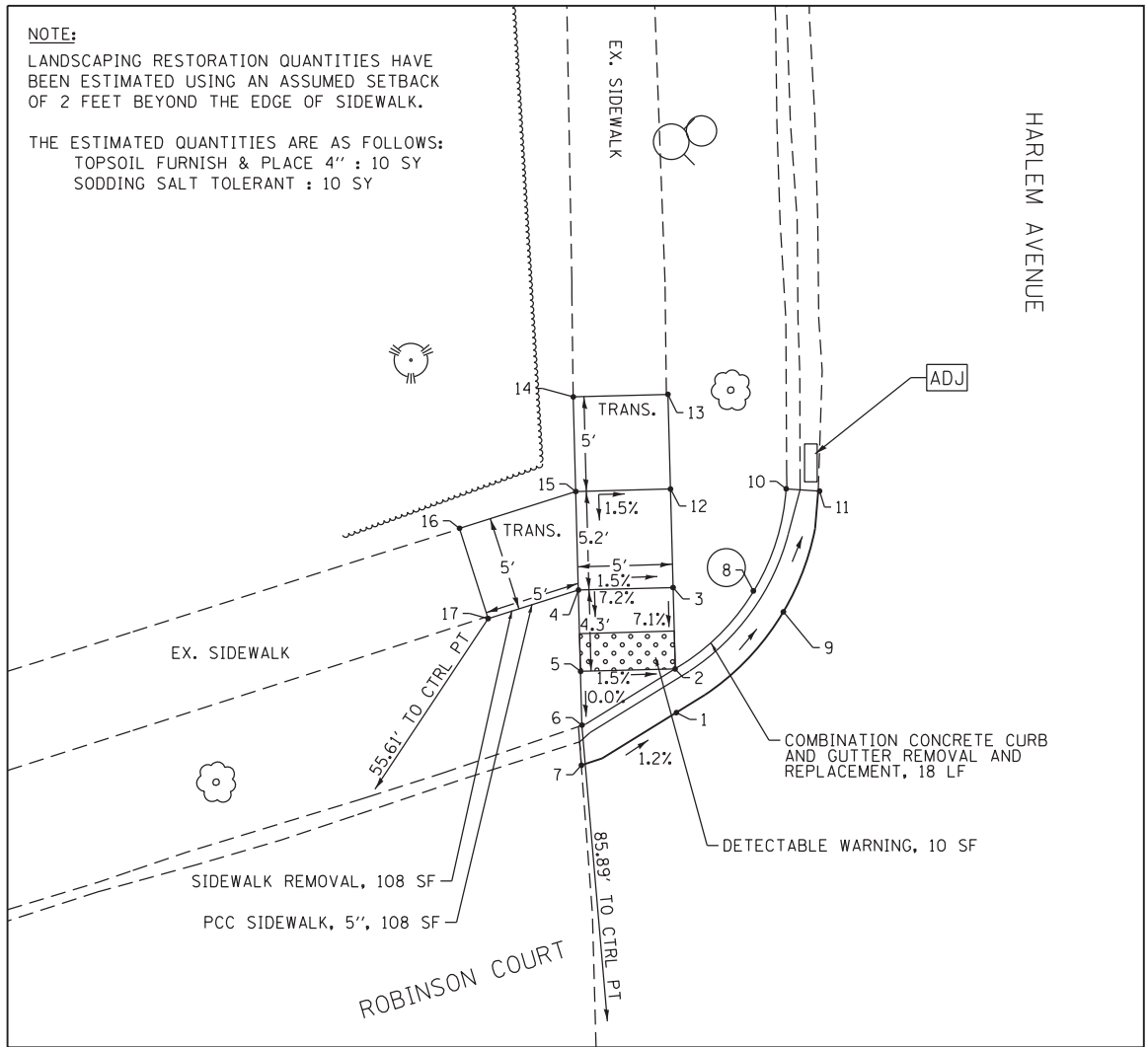


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	DRAWN -	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - MTC	REVISED -
PLOT DATE = 10/17/2016	DATE - 10/17/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL ROUTE 43 (HARLEM AVENUE)			
ADA RAMP DETAILS			
SCALE: 1" = 5'	SHEET 18	OF 38 SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	(2424 & 3138) RS-3	COOK	75	34
CONTRACT NO. 62C19				
ILLINOIS FED. AID PROJECT				

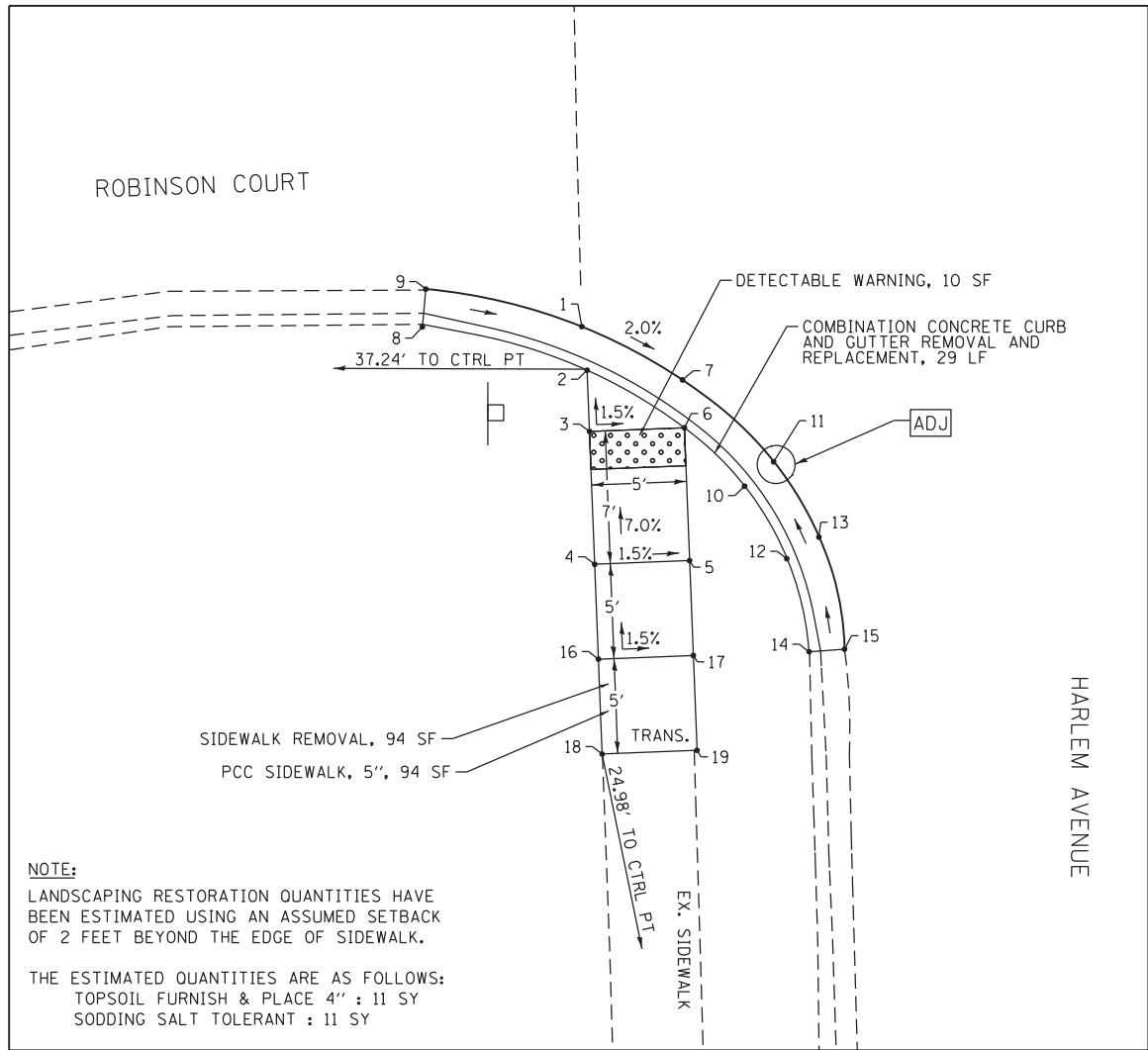


NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 10 SY
 SODDING SALT TOLERANT : 10 SY

CONTROL POINTS:
 CUT CROSS CUT CROSS
 ELEV: 609.31 ELEV: 609.09
 STA: 526+10.63 STA: 526+57.06
 OS: 38.57' LT OS: 79.91' LT
 N: 1880479.819 N: 1880524.674
 E: 1128794.930 E: 1128751.883

NORTHWEST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND ROBINSON COURT

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	526+96.86	37.74' LT	608.91
2	526+99.17	37.72' LT	608.89
3	527+03.47	37.67' LT	609.20
4	527+03.52	42.67' LT	609.27
5	526+99.22	42.72' LT	608.96
6	526+96.38	42.74' LT	608.96
7	526+94.27	42.85' LT	MATCH EX.
8	527+03.14	33.44' LT	609.03
9	527+01.97	31.90' LT	608.60
10	527+08.46	31.51' LT	MATCH EX.
11	527+08.28	29.76' LT	MATCH EX.
12	527+08.67	37.62' LT	609.28
13	527+13.67	37.57' LT	MATCH EX.
14	527+13.73	42.57' LT	MATCH EX.
15	527+08.73	42.62' LT	609.35
16	527+07.01	48.83' LT	MATCH EX.
17	527+02.19	47.49' LT	MATCH EX.



NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 11 SY
 SODDING SALT TOLERANT : 11 SY

CONTROL POINTS:
 CUT CROSS CUT CROSS
 ELEV: 609.31 ELEV: 609.09
 STA: 526+10.63 STA: 526+57.06
 OS: 38.57' LT OS: 79.91' LT
 N: 1880479.819 N: 1880524.674
 E: 1128794.930 E: 1128751.883

SOUTHWEST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND ROBINSON COURT

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	526+57.80	42.89' LT	608.56
2	526+55.50	42.70' LT	608.54
3	526+52.27	42.70' LT	608.49
4	526+45.27	42.69' LT	608.98
5	526+45.27	37.69' LT	608.91
6	526+52.27	37.70' LT	608.42
7	526+54.80	37.70' LT	608.44
8	526+58.10	51.28' LT	MATCH EX.
9	526+60.08	51.03' LT	MATCH EX.
10	526+49.08	34.64' LT	608.89
11	526+50.31	33.07' LT	608.28
12	526+45.18	32.56' LT	609.09
13	526+46.26	30.83' LT	608.32
14	526+40.24	31.58' LT	MATCH EX.
15	526+40.31	29.58' LT	MATCH EX.
16	526+40.27	42.68' LT	609.06
17	526+40.27	37.68' LT	608.99
18	526+35.27	42.67' LT	MATCH EX.
19	526+35.27	37.67' LT	MATCH EX.

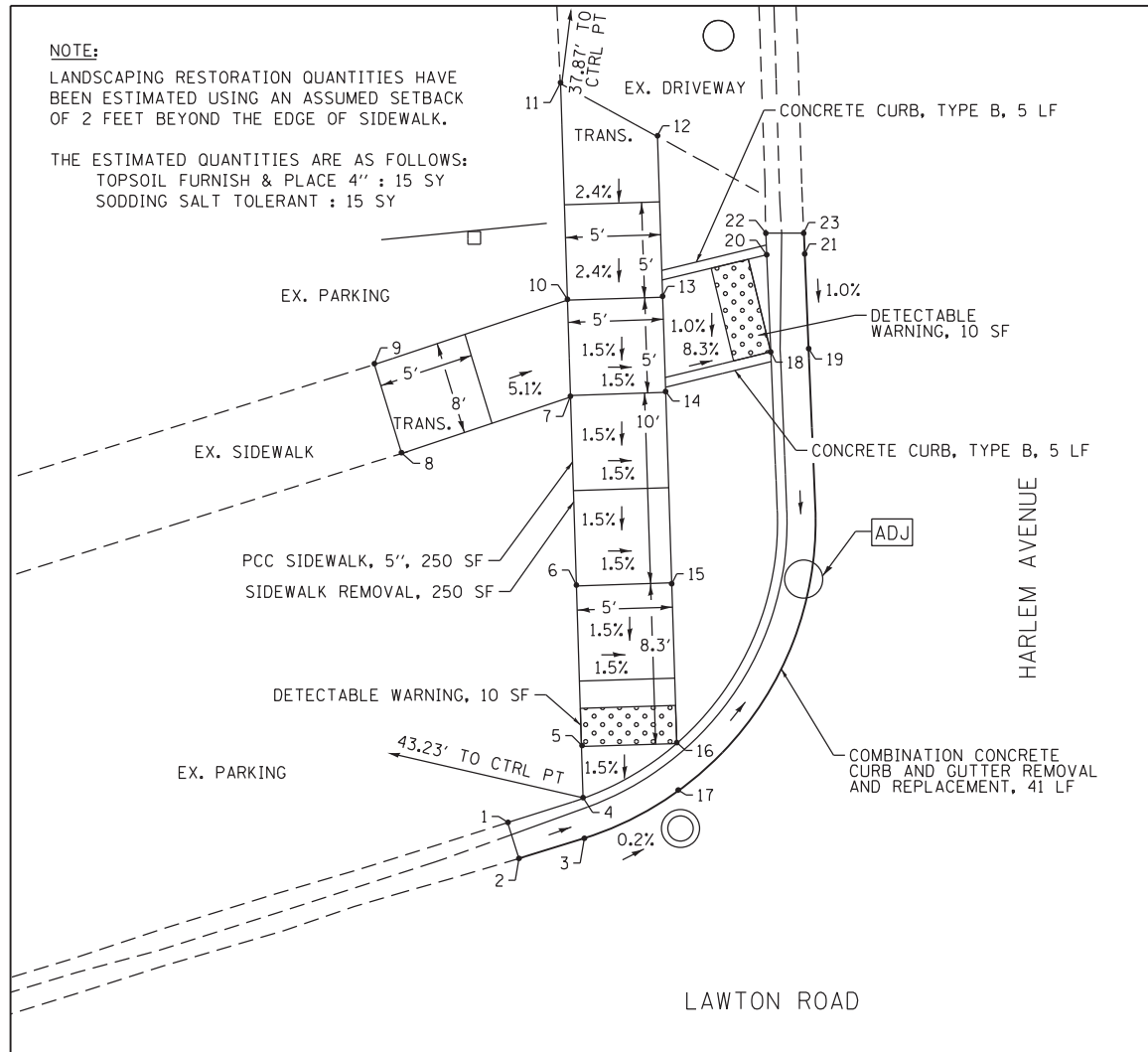


USER NAME = WTeng	DESIGNED -	REVISED -
PLOT SCALE = 10.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 10/17/2016	CHECKED - MTC	REVISED -
	DATE - 10/17/2016	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

IL ROUTE 43 (HARLEM AVENUE) ADA RAMP DETAILS		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: 1" = 5'	SHEET 19 OF 38 SHEETS	STA.	TO STA.	COOK	75	35

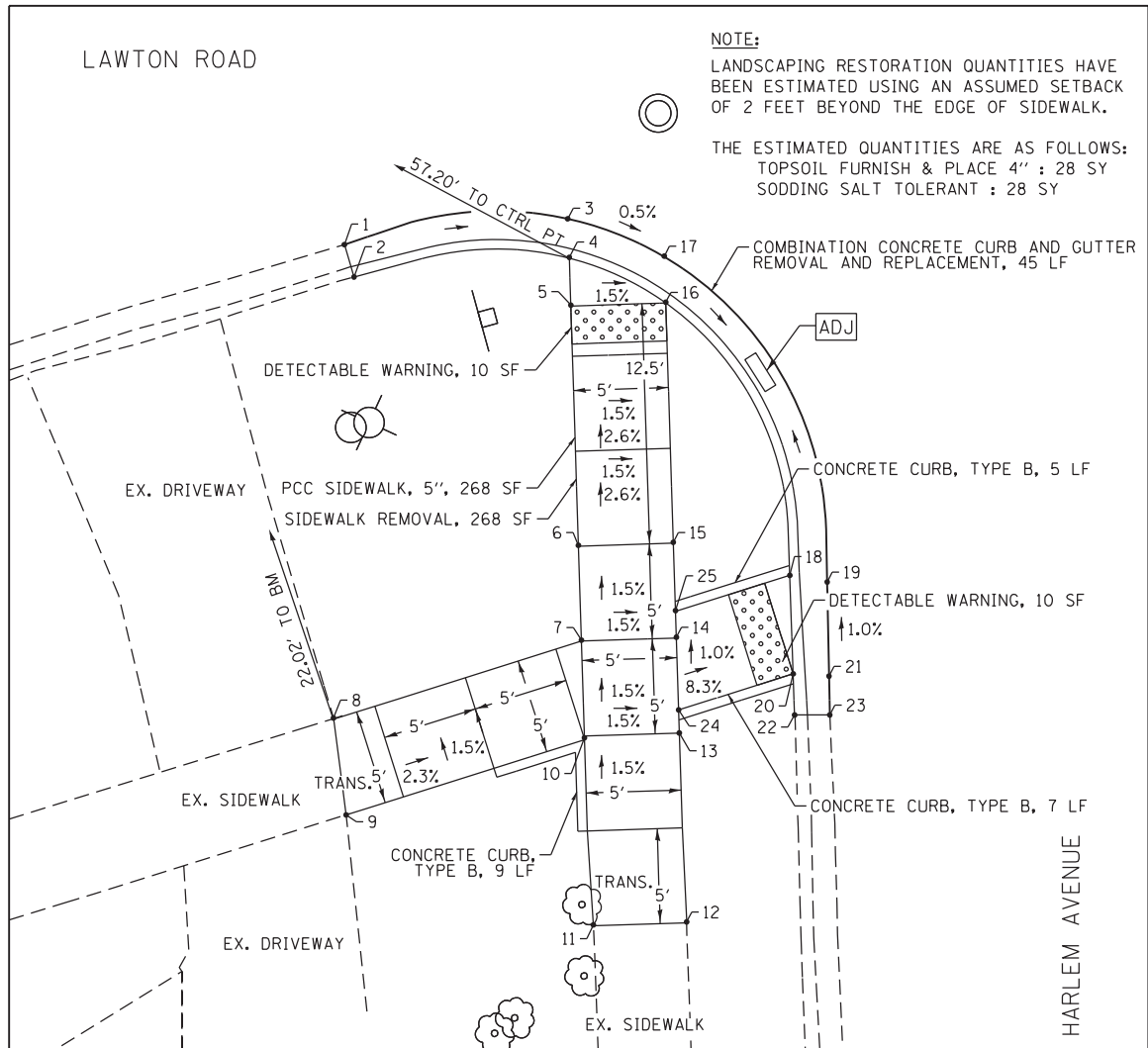
CONTRACT NO. 62C19		ILLINOIS	FED. AID PROJECT
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NOTE:

LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.

THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 15 SY
 SODDING SALT TOLERANT : 15 SY



NOTE:

LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.

THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 28 SY
 SODDING SALT TOLERANT : 28 SY

CONTROL POINTS:

CUT CROSS	ELEV.	STA.	OS:	N:	E:
609.21	609.55	529+86.64	37.52' LT	1880920.083	1128779.790
609.55	609.55	529+86.64	83.66' LT	1880853.947	1128736.042

NORTHWEST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND LAWTON ROAD

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	529+74.83	45.78' LT	MATCH EX.
2	529+72.92	45.25' LT	MATCH EX.
3	529+73.86	41.79' LT	608.59
4	529+75.95	41.77' LT	608.58
5	529+78.67	41.76' LT	608.61
6	529+87.10	41.72' LT	608.78
7	529+97.10	41.67' LT	608.93
8	529+94.38	50.63' LT	MATCH EX.
9	529+99.09	51.88' LT	MATCH EX.
10	530+02.10	41.64' LT	609.01
11	530+13.46	41.58' LT	MATCH EX.
12	530+10.50	36.60' LT	MATCH EX.
13	530+02.08	36.64' LT	608.94
14	529+97.08	36.67' LT	608.86
15	529+87.08	36.72' LT	608.71
16	529+78.65	36.76' LT	608.57
17	529+76.16	36.77' LT	608.58
18	529+99.00	31.08' LT	608.50
19	529+99.13	29.08' LT	608.51
20	530+04.10	31.10' LT	608.55
21	530+04.02	29.11' LT	608.56
22	530+05.23	31.11' LT	MATCH EX.
23	530+05.15	29.11' LT	MATCH EX.

CONTROL POINTS:

CUT CROSS	ELEV.	STA.	OS:	N:	E:
609.55	609.02	529+86.64	83.66' LT	1880853.947	1128736.042
609.02	609.02	529+45.34	59.61' LT	1880813.564	1128761.595

SOUTHWEST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND LAWTON ROAD

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	529+48.68	53.73' LT	MATCH EX.
2	529+46.96	53.29' LT	MATCH EX.
3	529+49.60	41.91' LT	608.61
4	529+7.53	41.92' LT	608.60
5	529+45.01	41.93' LT	608.64
6	529+32.36	42.00' LT	608.97
7	529+27.36	42.02' LT	609.03
8	529+23.72	55.21' LT	MATCH EX.
9	529+18.66	54.75' LT	MATCH EX.
10	529+22.36	42.05' LT	609.10
11	529+12.36	41.95' LT	MATCH EX.
12	529+12.33	37.03' LT	MATCH EX.

SOUTHWEST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND LAWTON ROAD

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
13	529+22.33	37.05' LT	609.21
14	529+27.33	37.02' LT	608.97
15	529+32.33	37.00' LT	608.90
16	529+44.98	36.93' LT	608.57
17	529+47.42	36.92' LT	608.58
18	529+30.02	30.94' LT	608.55
19	529+29.94	28.98' LT	608.56
20	529+25.05	30.95' LT	608.60
21	529+24.99	29.06' LT	608.61
22	529+23.05	30.95' LT	MATCH EX.
23	529+22.99	29.10' LT	MATCH EX.
24	529+23.54	37.04' LT	609.03
25	529+28.74	37.02' LT	608.96



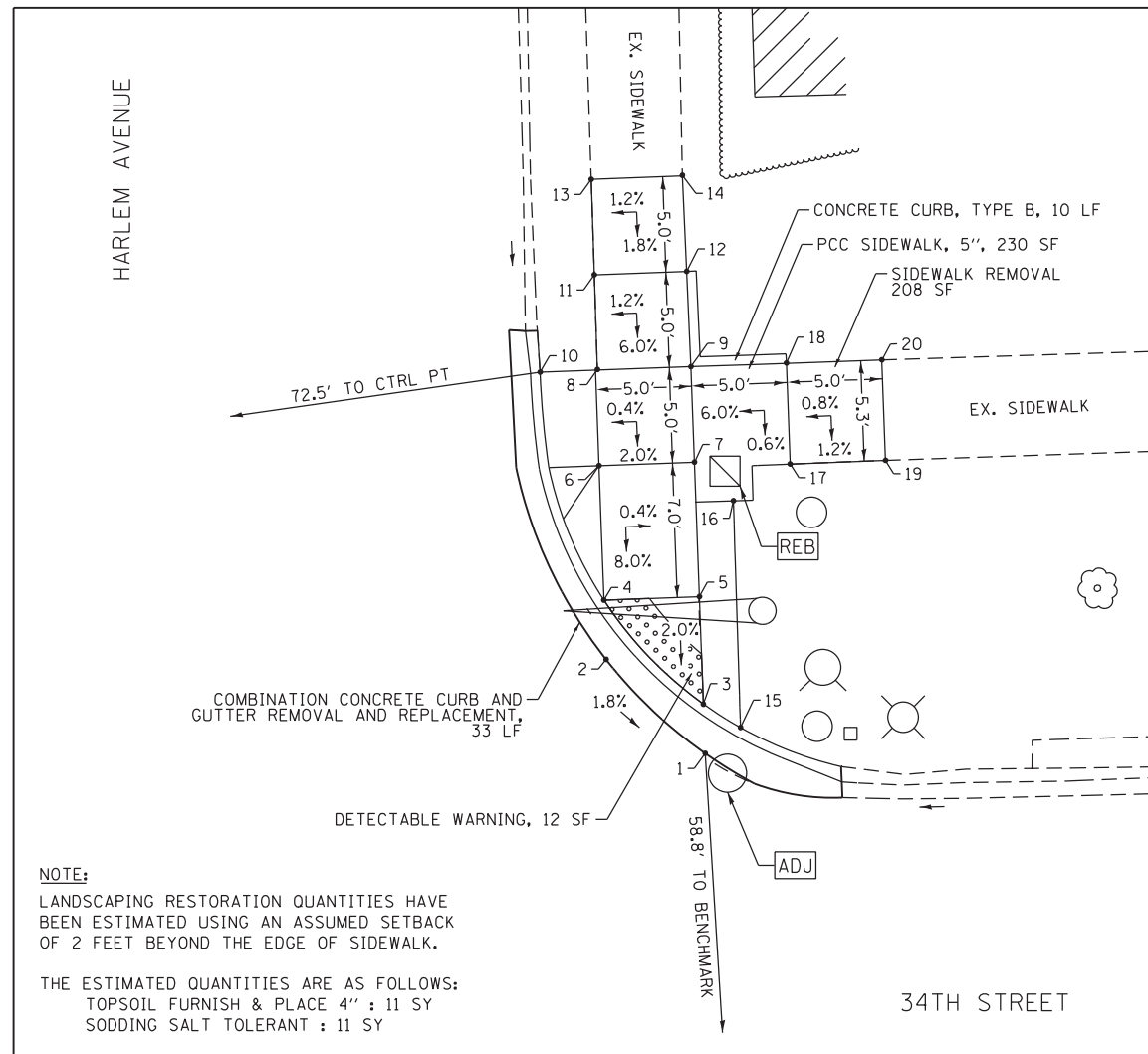
USER NAME = WTeng	DESIGNED -	REVISED -
PLOT SCALE = 10.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 10/17/2016	CHECKED - MTC	REVISED -
	DATE - 10/17/2016	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**IL ROUTE 43 (HARLEM AVENUE)
 ADA RAMP DETAILS**

SCALE: 1" = 5' SHEET 20 OF 38 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	(2424 & 3138) RS-3	COOK	75	36
CONTRACT NO. 62C19				
ILLINOIS FED. AID PROJECT				



NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 11 SY
 SODDING SALT TOLERANT : 11 SY

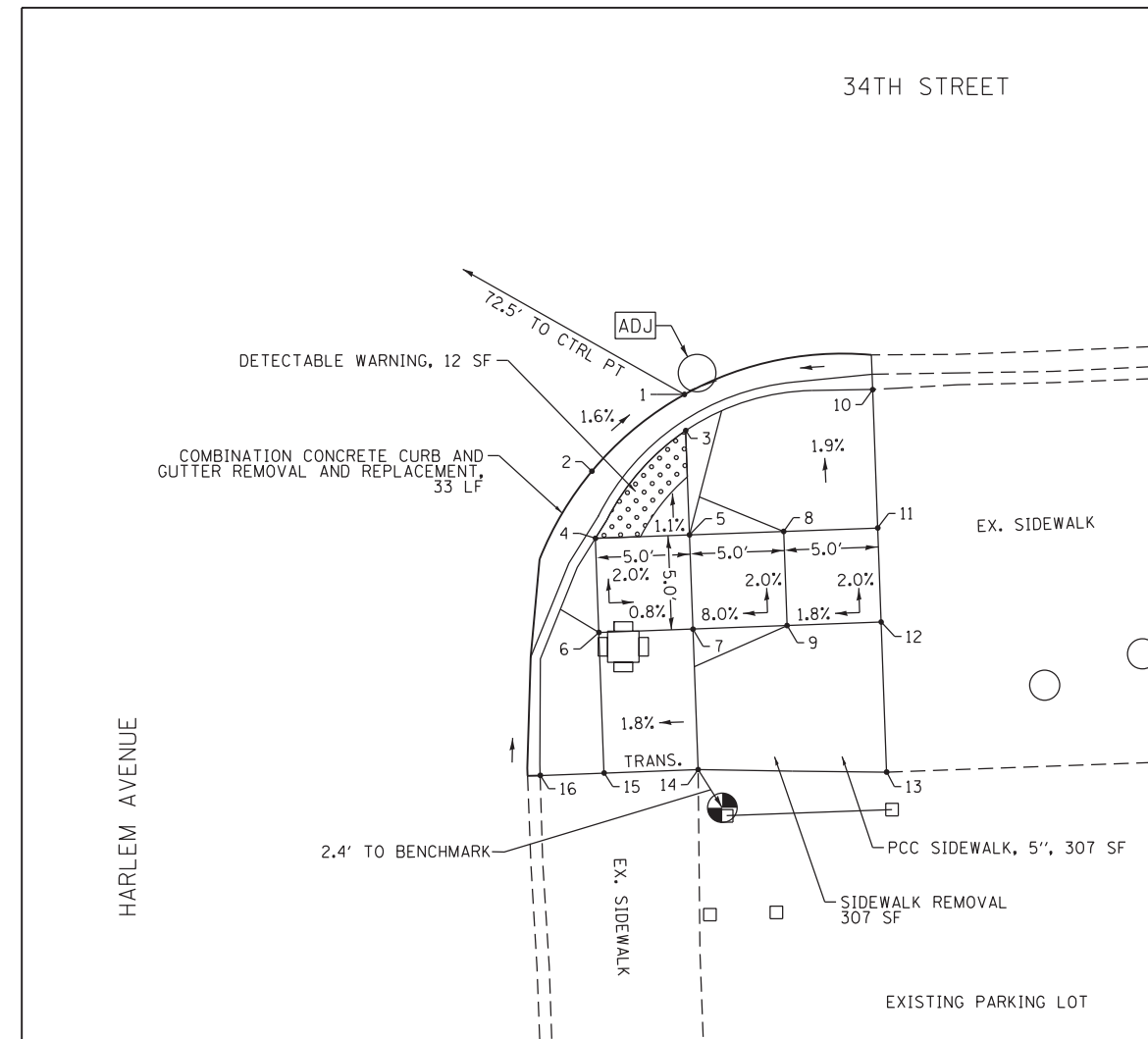
CONTROL POINTS:

BENCHMARK
 CONCRETE SIGN FOUNDATION
 ELEV: 610.38
 STA: 532+86.20
 OS: 44.64' RT
 N: 1881158.011
 E: 1128853.275

CUT CROSS
 ELEV: 610.34
 STA: 533+57.61
 OS: 36.85' LT
 N: 1881226.389
 E: 1128769.217

**NORTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 34TH STREET**

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	533+44.99	43.12' RT	608.90
2	533+50.10	38.13' RT	609.03
3	533+47.59	43.13' RT	608.89
4	533+53.21	38.13' RT	609.02
5	533+53.21	43.13' RT	609.00
6	533+60.25	38.13' RT	609.58
7	533+60.25	43.13' RT	609.56
8	533+65.25	38.13' RT	609.64
9	533+65.25	43.13' RT	609.66
10	533+65.25	35.23' RT	MATCH EX.
11	533+70.25	38.25' RT	609.94
12	533+70.25	43.09' RT	610.00
13	533+75.25	38.27' RT	MATCH EX.
14	533+75.25	43.05' RT	MATCH EX.
15	533+46.28	45.03' RT	MATCH EX.
16	533+58.16	45.09' RT	609.80
17	533+59.97	48.13' RT	609.93
18	533+65.25	48.13' RT	609.96
19	533+59.98	53.13' RT	MATCH EX.
20	533+65.24	53.13' RT	MATCH EX.



CONTROL POINTS:

BENCHMARK
 CONCRETE SIGN FOUNDATION
 ELEV: 610.38
 STA: 532+86.20
 OS: 44.64' RT
 N: 1881158.011
 E: 1128853.275

CUT CROSS
 ELEV: 610.34
 STA: 533+57.61
 OS: 36.85' LT
 N: 1881226.389
 E: 1128769.217

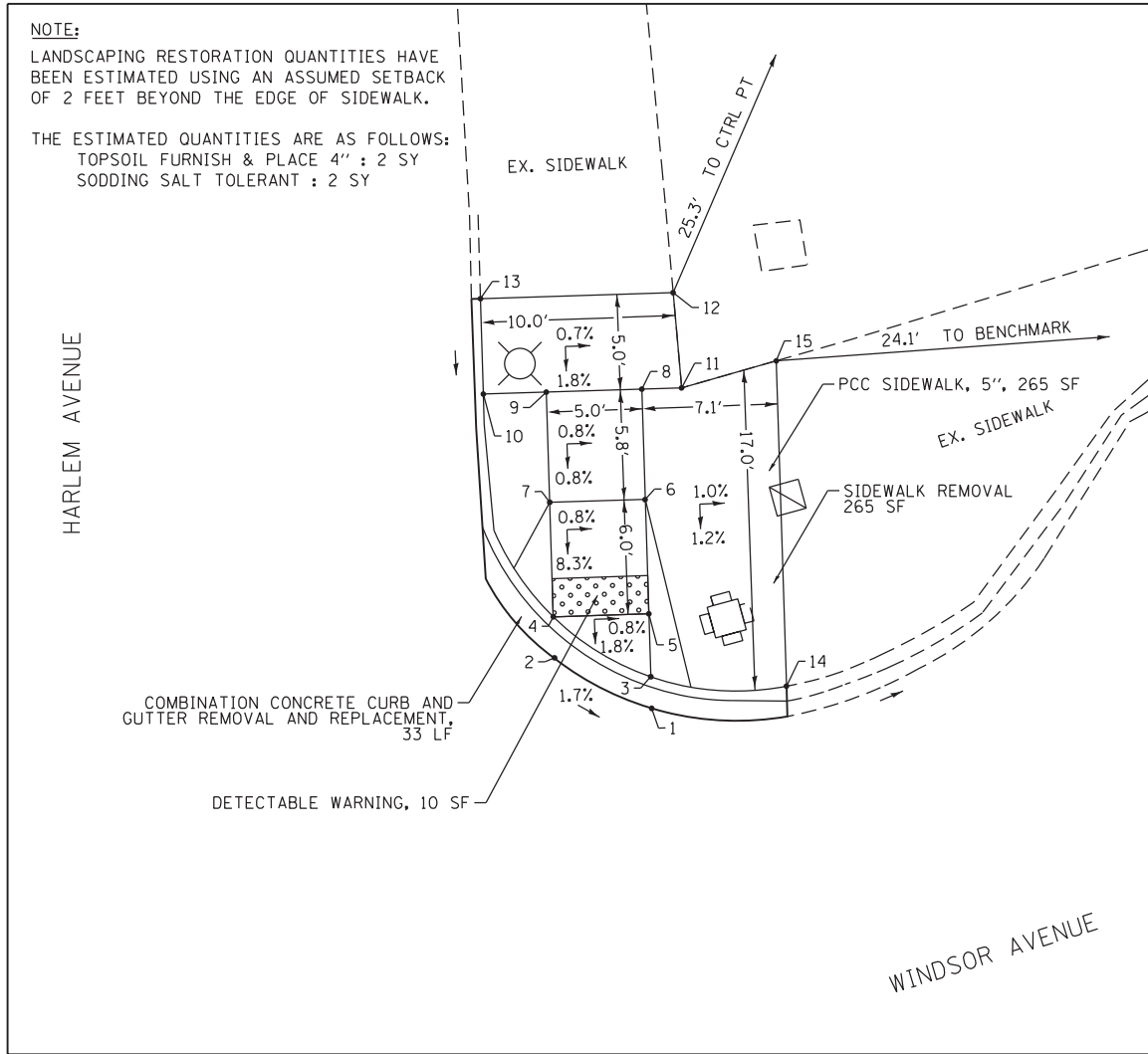
**SOUTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 34TH STREET**

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	533+08.20	43.43' RT	609.15
2	533+04.38	38.43' RT	609.25
3	533+06.28	43.43' RT	609.14
4	533+00.74	38.43' RT	609.24
5	533+00.74	43.43' RT	609.20
6	532+95.74	38.43' RT	609.34
7	532+95.74	43.43' RT	609.30
8	533+00.74	48.43' RT	609.60
9	532+95.74	48.43' RT	609.70
10	533+08.10	53.43' RT	MATCH EX.
11	533+00.74	53.43' RT	MATCH EX.
12	532+95.74	53.43' RT	MATCH EX.
13	532+87.76	53.43' RT	MATCH EX.
14	532+88.26	43.43' RT	MATCH EX.
15	532+88.26	38.43' RT	MATCH EX.
16	532+88.26	35.04' RT	MATCH EX.

NOTE:

LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.

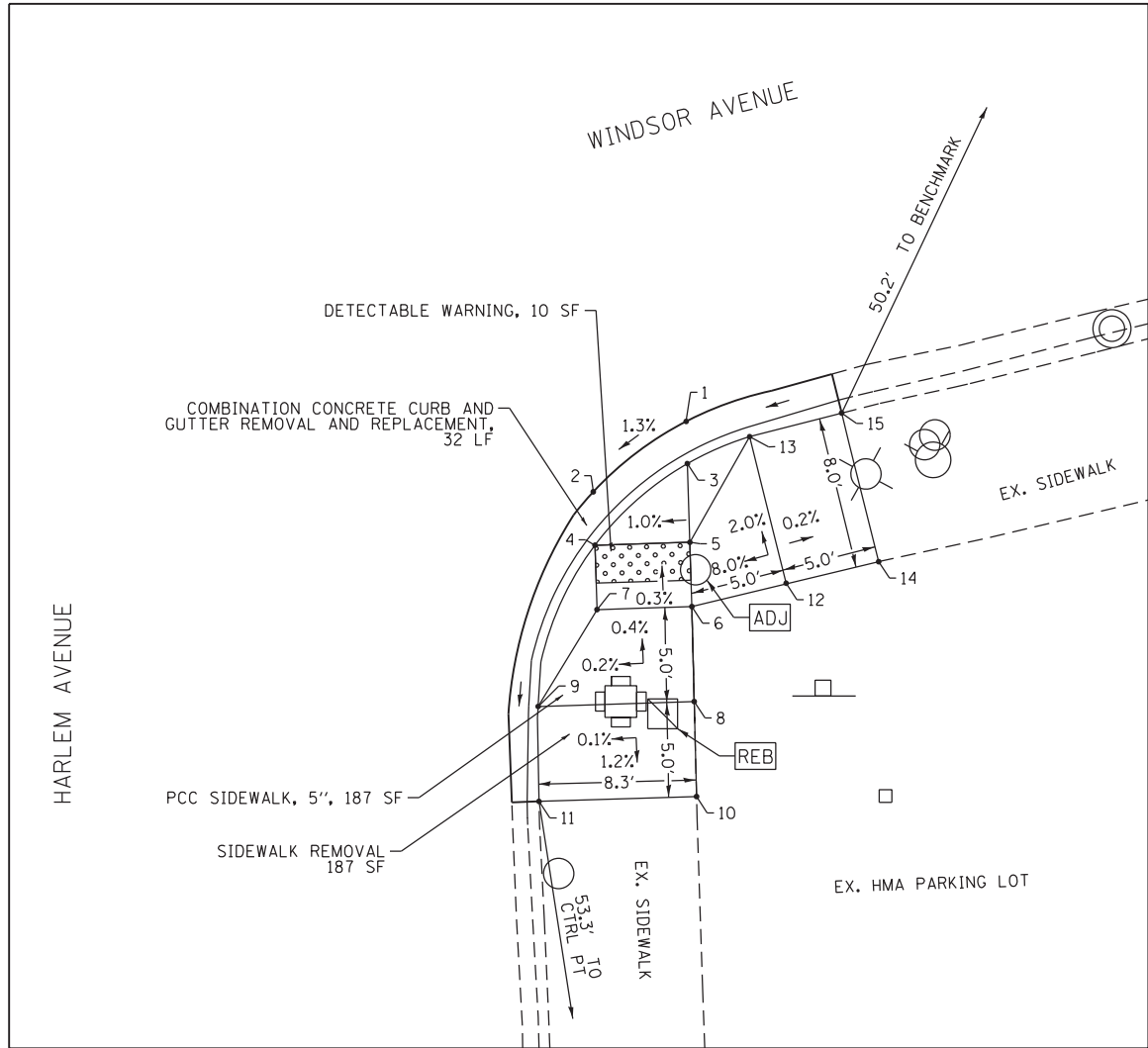
THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 2 SY
 SODDING SALT TOLERANT : 2 SY



CONTROL POINTS:

CUT CROSS ELEV: 610.54 STA: 536+32.37 OS: 56.77' RT N: 1881504.101 E: 1128853.072	BENCHMARK CONC. BACK OF CURB ELEV: 611.74 STA: 536+06.76 OS: 75.36' RT N: 1881479.083 E: 1128872.455	NORTHEAST CORNER IL ROUTE 43 (HARLEM AVENUE) AND WINDSOR AVENUE
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ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	535+87.77	44.23' RT	611.51
2	535+90.55	39.23' RT	611.61
3	535+89.42	44.23' RT	611.50
4	535+92.72	39.23' RT	611.60
5	535+92.72	44.23' RT	611.56
6	535+98.72	44.23' RT	612.05
7	535+98.72	39.23' RT	612.09
8	536+04.49	44.23' RT	612.16
9	536+04.49	39.23' RT	612.20
10	536+04.49	35.93' RT	612.24
11	536+04.49	46.31' RT	MATCH EX.
12	536+09.49	46.02' RT	MATCH EX.
13	536+09.49	35.94' RT	MATCH EX.
14	535+88.71	51.31' RT	MATCH EX.
15	536+05.75	51.31' RT	MATCH EX.



CONTROL POINTS:

BENCHMARK CONC. BACK OF CURB ELEV: 611.74 STA: 536+06.76 OS: 75.36' RT N: 1881479.083 E: 1128872.455	CUT CROSS ELEV: 610.20 STA: 534+89.06 OS: 41.95' RT N: 1881360.642 E: 1128843.149	SOUTHEAST CORNER IL ROUTE 43 (HARLEM AVENUE) AND WINDSOR AVENUE
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ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	535+61.71	44.21' RT	610.34
2	535+58.19	39.19' RT	610.26
3	535+59.49	44.20' RT	610.33
4	535+55.37	39.17' RT	610.25
5	535+55.34	44.17' RT	610.30
6	535+51.95	44.15' RT	MATCH EX.
7	535+51.98	39.15' RT	610.26
8	535+46.95	44.09' RT	MATCH EX.
9	535+47.00	35.87' RT	MATCH EX.
10	535+41.95	44.03' RT	MATCH EX.
11	535+42.00	35.74' RT	MATCH EX.
12	535+52.97	49.17' RT	MATCH EX.
13	535+60.79	47.52' RT	610.52
14	535+53.96	54.07' RT	MATCH EX.
15	535+61.85	52.41' RT	MATCH EX.



USER NAME = WTeng	DESIGNED -	REVISED -
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

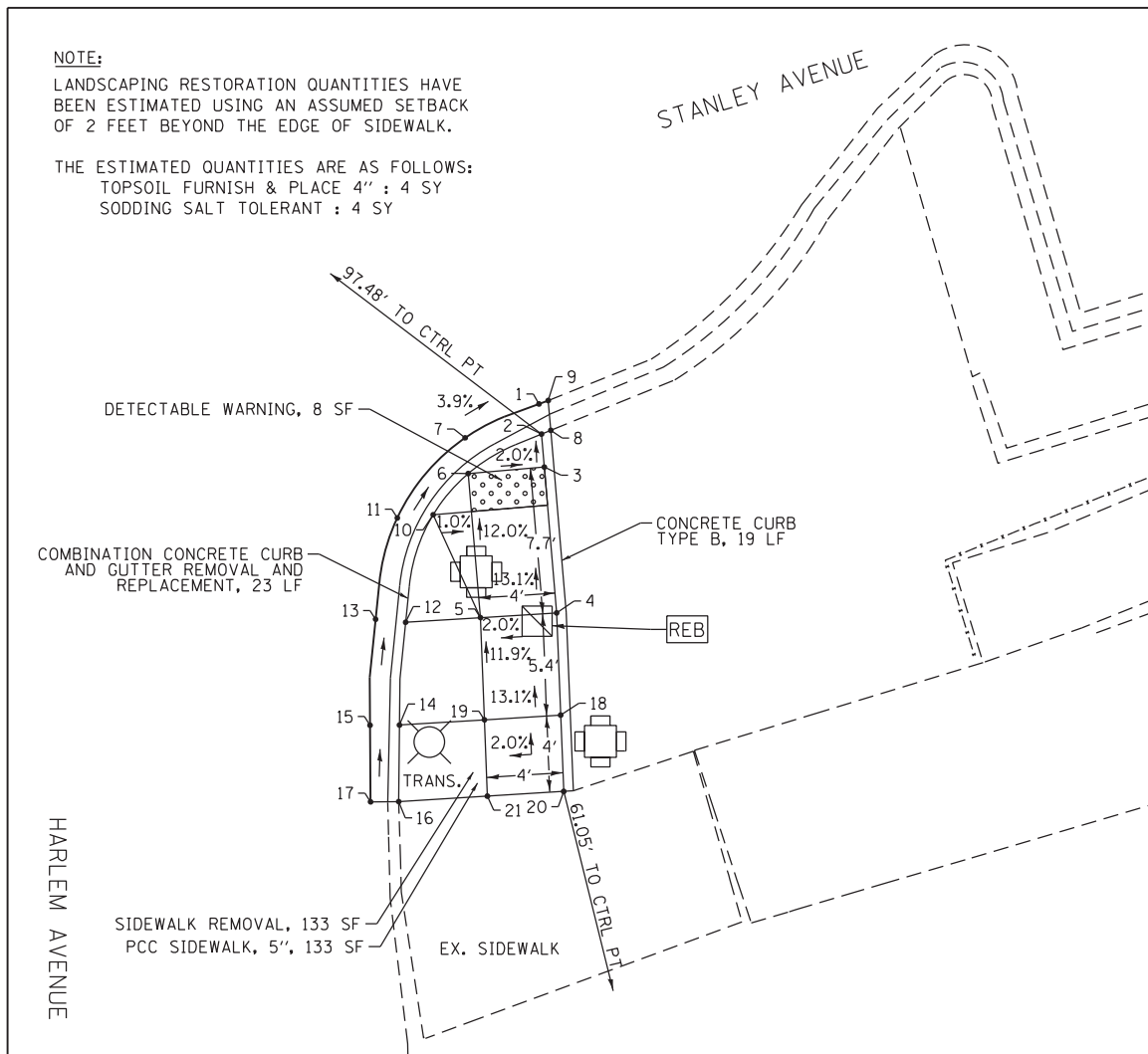
IL ROUTE 43 (HARLEM AVENUE) ADA RAMP DETAILS			
SCALE: 1" = 5'	SHEET 23	OF 38 SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	(2424 & 3138) R5-3	COOK	75	39
CONTRACT NO. 62C19				
ILLINOIS FED. AID PROJECT				

NOTE:

LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.

THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 4 SY
 SODDING SALT TOLERANT : 4 SY



CONTROL POINTS:

CUT CROSS	CUT CROSS
ELEV: 612.81	ELEV: 610.88
STA: 536+32.37	STA: 537+72.16
OS: 56.77' RT	OS: 32.41' LT
N: 1881504.101	N: 1881641.039
E: 1128853.072	E: 1128759.574

**SOUTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND STANLEY AVENUE**

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	537+12.44	43.24' RT	610.98
2	537+10.79	43.33' RT	611.04
3	537+09.06	43.43' RT	611.07
4	537+01.41	43.83' RT	612.07
5	537+01.28	39.84' RT	612.06
6	537+08.84	39.43' RT	611.15
7	537+10.72	39.33' RT	611.15
8	537+10.79	43.33' RT	MATCH EX.
9	537+12.54	43.74' RT	MATCH EX.
10	537+06.74	37.53' RT	611.41
11	537+06.64	35.65' RT	611.41
12	537+01.16	35.92' RT	612.04
13	537+01.37	34.35' RT	611.86
14	536+95.79	35.43' RT	612.68
15	536+95.83	33.89' RT	612.27
16	536+91.79	35.37' RT	MATCH EX.
17	536+91.82	33.79' RT	MATCH EX.
18	536+96.05	43.88' RT	612.77
19	536+95.93	39.88' RT	612.70
20	536+92.05	43.91' RT	MATCH EX.
21	536+91.93	39.91' RT	MATCH EX.

CONTROL POINTS:

CUT CROSS	CUT CROSS
ELEV: 612.81	ELEV: 610.88
STA: 536+32.37	STA: 537+72.16
OS: 56.77' RT	OS: 32.41' LT
N: 1881504.101	N: 1881641.039
E: 1128853.072	E: 1128759.574

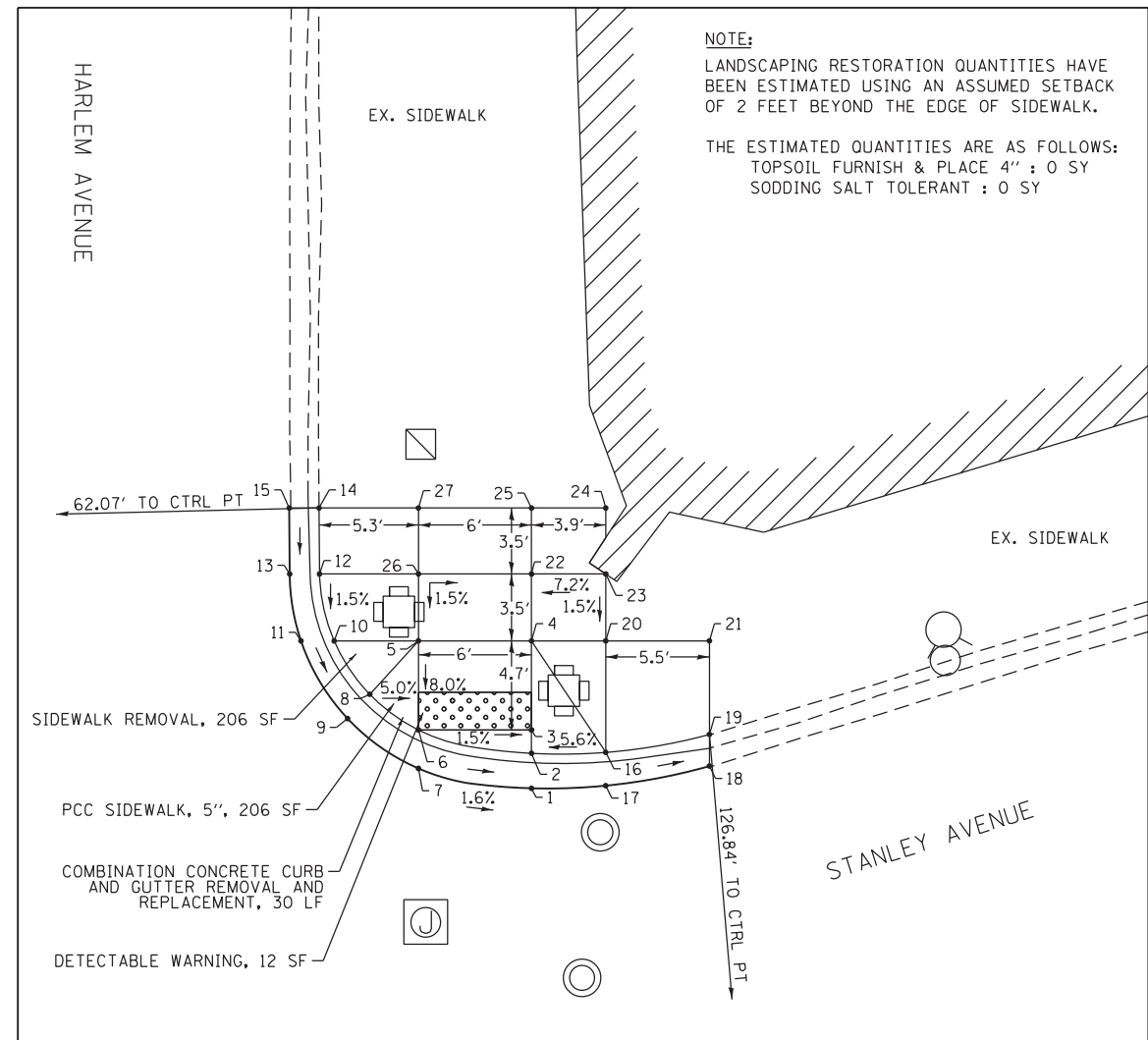
**NORTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND STANLEY AVENUE**

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	537+56.47	40.47' RT	690.80
2	537+58.32	40.53' RT	609.78
3	537+59.56	40.56' RT	609.80
4	537+64.29	40.71' RT	610.18
5	537+64.47	34.72' RT	610.27
6	537+59.75	34.57' RT	609.89
7	537+57.70	34.50' RT	609.90
8	537+61.72	32.04' RT	610.14
9	537+60.47	30.83' RT	610.01
10	537+64.61	30.24' RT	610.27
11	537+64.67	28.49' RT	610.10
12	537+68.18	29.58' RT	610.32
13	537+68.23	28.00' RT	610.13
14	537+71.68	29.66' RT	MATCH EX.

NOTE:

LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.

THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 0 SY
 SODDING SALT TOLERANT : 0 SY



**NORTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND STANLEY AVENUE**

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
15	537+71.73	28.08' RT	MATCH EX.
16	537+58.25	44.46' RT	610.00
17	537+56.48	44.41' RT	609.73
18	537+57.35	49.94' RT	MATCH EX.
19	537+59.03	49.99' RT	MATCH EX.
20	537+64.16	44.65' RT	610.46
21	537+63.99	50.15' RT	MATCH EX.
22	537+67.83	40.82' RT	610.23
23	537+67.71	44.76' RT	610.51
24	537+71.21	44.87' RT	MATCH EX.
25	537+71.33	40.93' RT	MATCH EX.
26	537+68.02	34.83' RT	610.32
27	537+71.52	34.94' RT	MATCH EX.

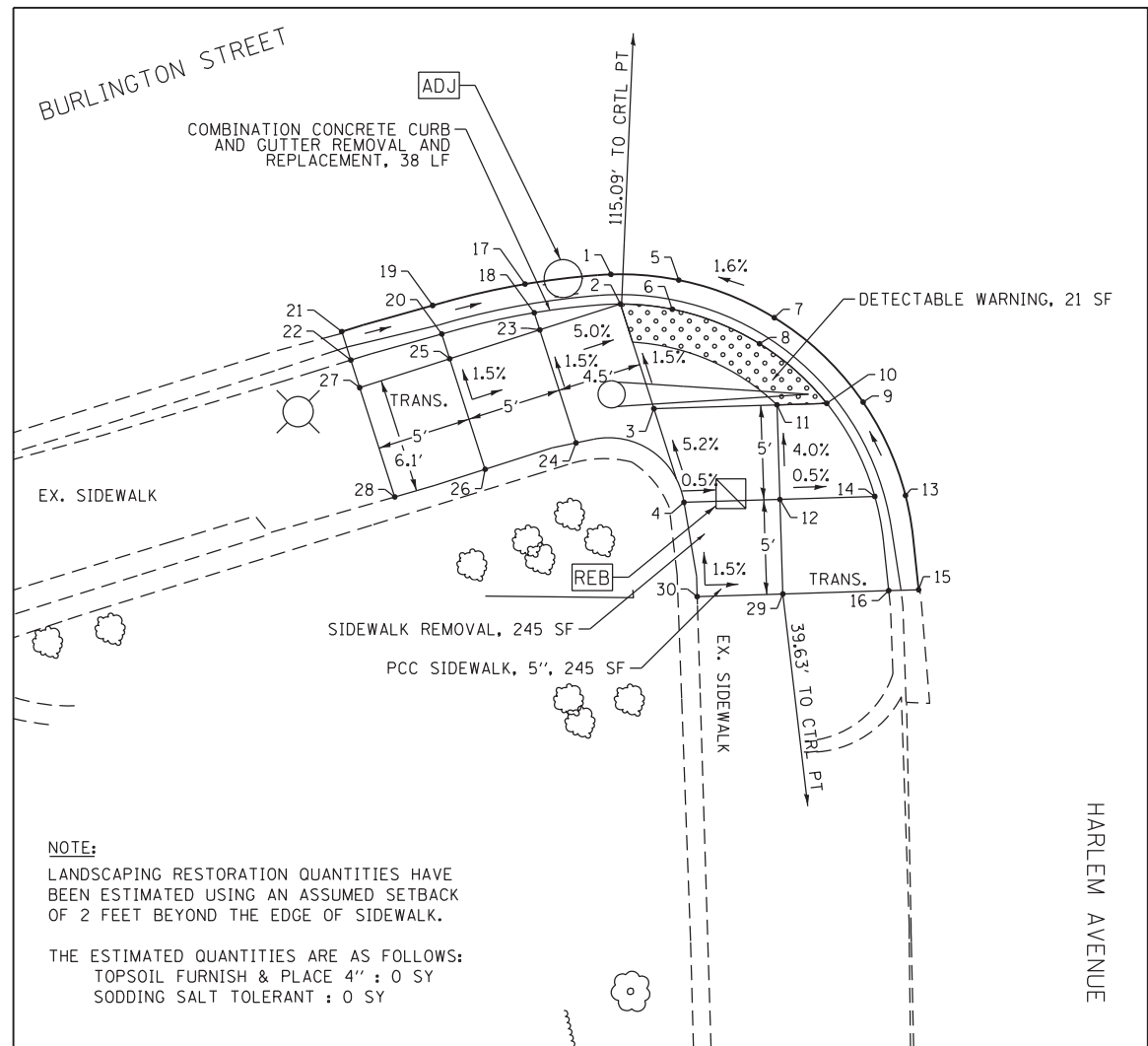


USER NAME = WTeng	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - MTC	REVISED -
PLOT DATE = 10/17/2016	DATE - 10/17/2016	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

IL ROUTE 43 (HARLEM AVENUE) ADA RAMP DETAILS	
SCALE: 1" = 5'	SHEET 24 OF 38 SHEETS
STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	(2424 & 3138) RS-3	COOK	75	40
CONTRACT NO. 62C19				
ILLINOIS FED. AID PROJECT				



NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 0 SY
 SODDING SALT TOLERANT : 0 SY

CONTROL POINTS:

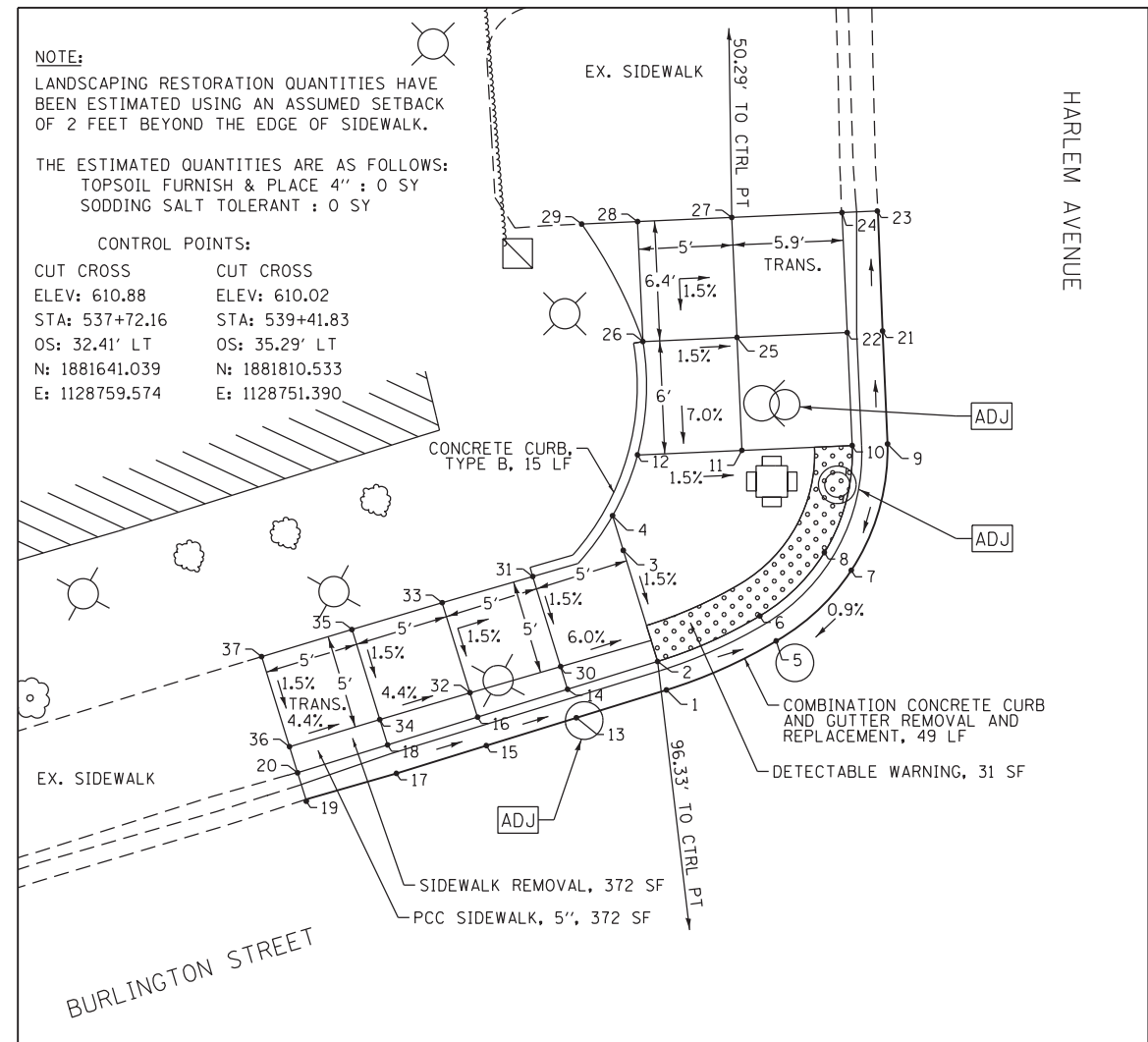
CUT CROSS	CUT CROSS
ELEV: 610.88	ELEV: 610.02
STA: 537+72.16	STA: 539+41.83
OS: 32.41' LT	OS: 35.29' LT
N: 1881641.039	N: 1881810.533
E: 1128759.574	E: 1128751.390

SOUTHWEST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND BURLINGTON STREET

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	538+28.82	44.37' LT	609.87
2	538+27.23	43.92' LT	609.90
3	538+21.65	42.32' LT	609.99
4	538+16.65	40.89' LT	610.26
5	538+28.40	40.79' LT	609.91
6	538+26.87	41.18' LT	609.94
7	538+26.26	35.81' LT	610.00
8	538+24.91	36.63' LT	610.00
9	538+21.64	31.25' LT	609.98
10	538+21.64	33.17' LT	610.02
11	538+21.64	35.80' LT	610.01
12	538+16.64	35.80' LT	610.23
13	538+16.64	29.16' LT	610.03
14	538+16.64	30.79' LT	610.21
15	538+11.64	28.62' LT	MATCH EX.

SOUTHWEST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND BURLINGTON STREET

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
16	538+11.64	30.21' LT	MATCH EX.
17	538+28.45	48.92' LT	609.80
18	538+26.91	48.48' LT	610.11
19	538+27.46	53.84' LT	609.80
20	538+25.94	53.41' LT	610.17
21	538+26.24	58.69' LT	MATCH EX.
22	538+24.27	58.25' LT	MATCH EX.
23	538+26.00	48.22' LT	610.12
24	538+19.97	46.49' LT	610.21
25	538+24.62	53.03' LT	610.19
26	538+18.72	51.34' LT	610.28
27	538+23.25	57.83' LT	MATCH EX.
28	538+17.41	56.16' LT	MATCH EX.
29	538+11.64	35.81' LT	MATCH EX.
30	538+11.65	40.34' LT	MATCH EX.



NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 0 SY
 SODDING SALT TOLERANT : 0 SY

CONTROL POINTS:

CUT CROSS	CUT CROSS
ELEV: 610.88	ELEV: 610.02
STA: 537+72.16	STA: 539+41.83
OS: 32.41' LT	OS: 35.29' LT
N: 1881641.039	N: 1881810.533
E: 1128759.574	E: 1128751.390

NORTHWEST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND BURLINGTON STREET

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	538+66.60	40.37' LT	609.69
2	538+68.12	40.79' LT	609.69
3	538+74.08	42.42' LT	609.78
4	538+75.93	42.93' LT	609.81
5	538+69.02	34.46' LT	609.60
6	538+70.37	35.29' LT	609.60
7	538+72.64	30.36' LT	609.65
8	538+73.62	31.76' LT	609.65
9	538+79.28	28.23' LT	609.69
10	538+79.25	30.11' LT	609.69
11	538+79.18	35.96' LT	609.77
12	538+79.11	41.51' LT	609.86
13	538+65.27	45.19' LT	609.70
14	538+66.80	45.61' LT	609.99
15	538+63.95	50.01' LT	609.70
16	538+65.47	50.43' LT	610.07
17	538+62.62	54.83' LT	609.73
18	538+64.15	55.25' LT	610.29
19	538+61.29	59.65' LT	MATCH EX.

NORTHWEST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND BURLINGTON STREET

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
20	538+62.82	60.07' LT	MATCH EX.
21	538+85.28	28.30' LT	609.56
22	538+85.25	30.18' LT	610.02
23	538+91.65	28.38' LT	MATCH EX.
24	538+91.62	30.26' LT	MATCH EX.
25	538+85.18	36.04' LT	610.19
26	538+85.12	41.04' LT	610.28
27	538+91.55	36.12' LT	MATCH EX.
28	538+91.49	41.12' LT	MATCH EX.
29	538+91.45	44.09' LT	MATCH EX.
30	538+68.02	45.94' LT	610.01
31	538+72.84	47.27' LT	610.08
32	538+66.78	50.79' LT	610.09
33	538+71.60	52.11' LT	610.16
34	538+65.50	55.62' LT	610.31
35	538+70.32	56.95' LT	610.38
36	538+64.22	60.45' LT	MATCH EX.
37	538+69.04	61.78' LT	MATCH EX.



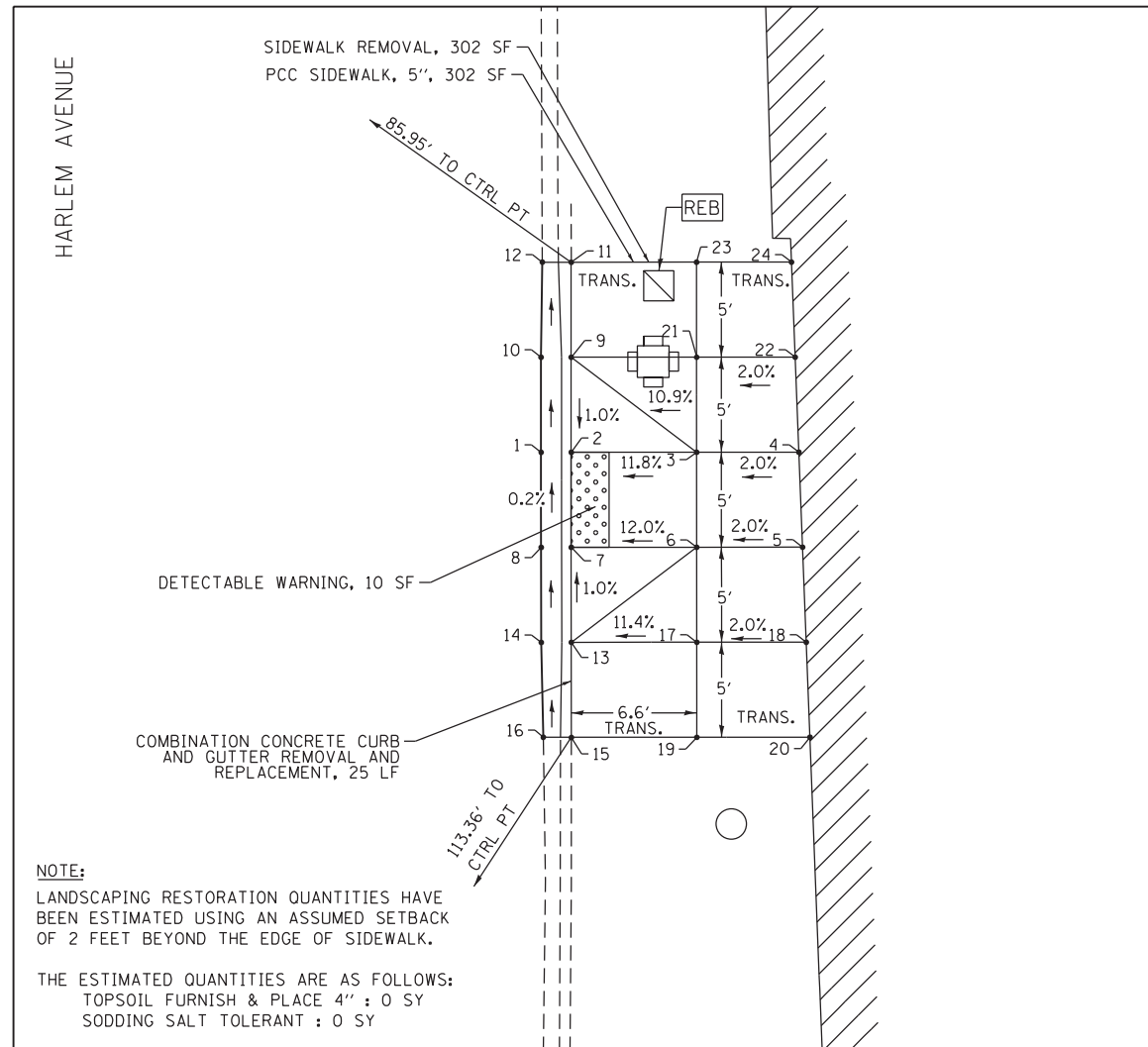
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	DRAWN -	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - MTC	REVISED -
PLOT DATE = 10/17/2016	DATE - 10/17/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL ROUTE 43 (HARLEM AVENUE)
ADA RAMP DETAILS

SCALE: 1" = 5' SHEET 25 OF 38 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	(2424 & 3138) RS-3	COOK	75	41
CONTRACT NO. 62C19				
ILLINOIS FED. AID PROJECT				



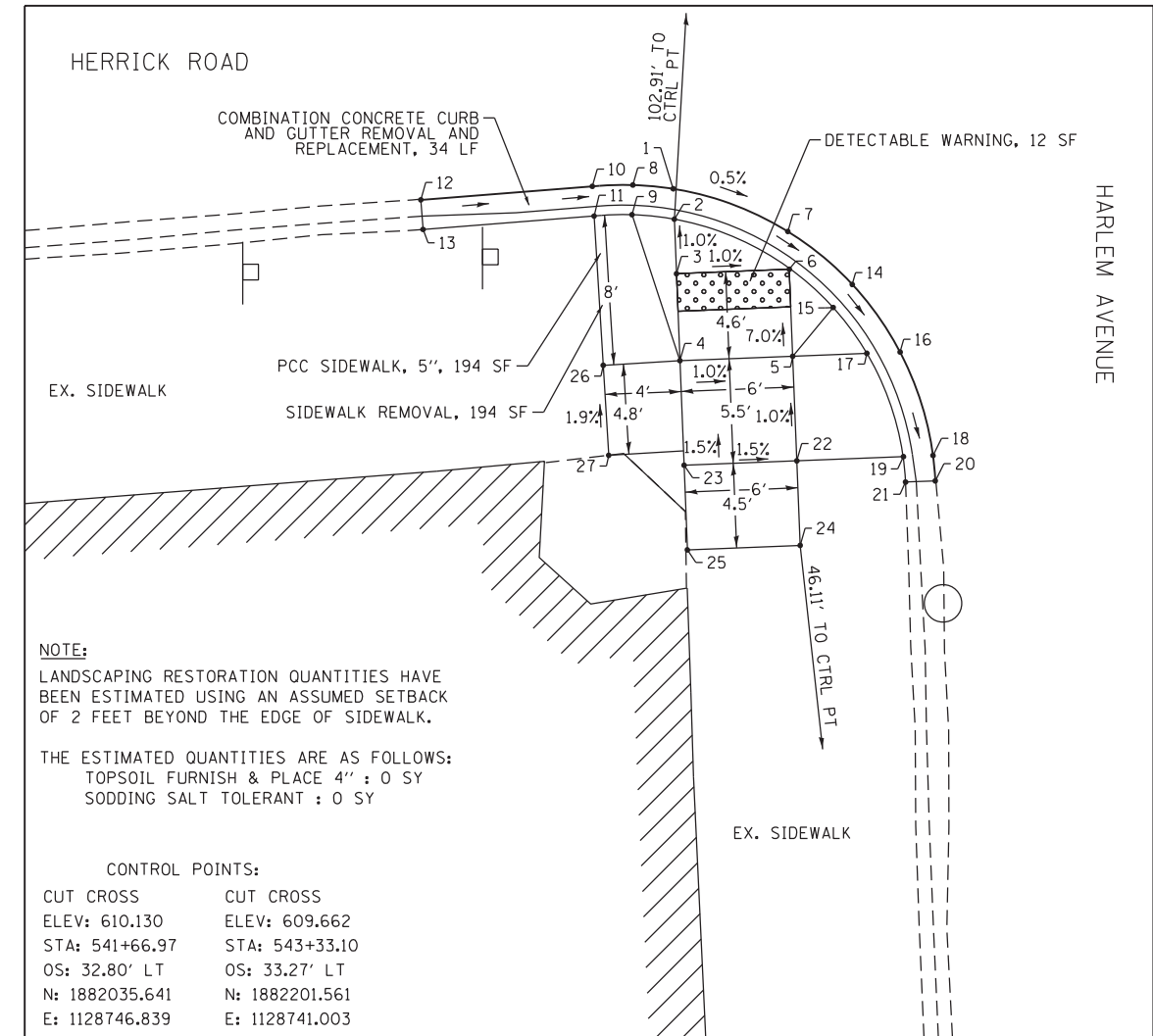
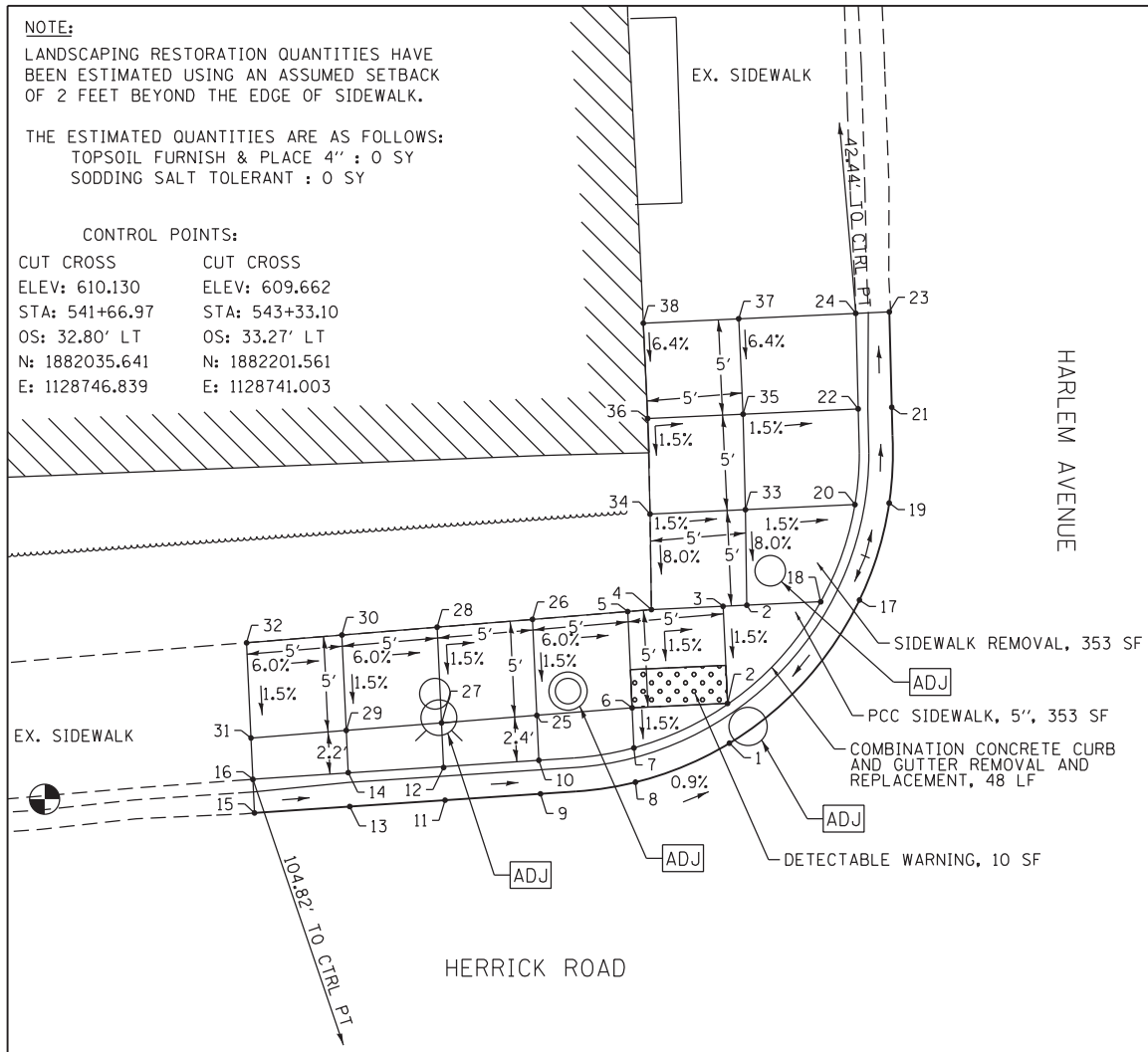
NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 0 SY
 SODDING SALT TOLERANT : 0 SY

CONTROL POINTS:

CUT CROSS	CUT CROSS
ELEV: 610.88	ELEV: 610.02
STA: 537+72.16	STA: 539+41.83
OS: 32.41' LT	OS: 35.29' LT
N: 1881641.039	N: 1881810.533
E: 1128759.574	E: 1128751.390

EAST SIDE
 IL ROUTE 43 (HARLEM AVENUE) AND BURLINGTON STREET

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	538+80.11	31.43' RT	609.73
2	538+80.06	33.01' RT	609.73
3	538+79.86	39.61' RT	610.51
4	538+79.69	44.99' RT	MATCH EX.
5	538+74.69	45.02' RT	MATCH EX.
6	538+74.86	39.46' RT	610.53
7	538+75.06	32.86' RT	609.74
8	538+75.11	31.28' RT	609.73
9	538+85.06	33.17' RT	609.78
10	538+85.11	31.58' RT	609.73
11	538+90.06	33.34' RT	MATCH EX.
12	538+90.10	31.81' RT	MATCH EX.
13	538+70.06	32.71' RT	609.79
14	538+70.11	31.12' RT	MATCH EX.
15	538+65.07	32.55' RT	MATCH EX.
16	538+65.11	31.08' RT	MATCH EX.
17	538+69.86	39.30' RT	610.54
18	538+69.68	45.06' RT	MATCH EX.
19	538+64.86	39.15' RT	MATCH EX.
20	538+64.68	45.10' RT	MATCH EX.
21	538+84.85	39.76' RT	610.50
22	538+84.69	44.95' RT	MATCH EX.
23	538+89.85	39.92' RT	MATCH EX.
24	538+89.70	44.91' RT	MATCH EX.



NORTHWEST CORNER
IL ROUTE 43 (HARLEM AVENUE) AND HERRICK ROAD

POINT No.:	STATION	OFFSET	ELEV.
1	542+68.37	38.42' LT	609.28
2	542+70.41	38.45' LT	609.32
3	542+75.56	38.52' LT	609.40
4	542+75.51	42.27' LT	609.46
5	542+75.45	43.52' LT	609.48
6	542+70.39	43.45' LT	609.40
7	542+68.29	43.42' LT	609.37
8	542+66.49	46.40' LT	609.33
9	542+66.04	48.39' LT	609.47
10	542+67.80	48.41' LT	609.67
11	542+65.87	53.39' LT	609.63
12	542+67.63	53.41 LT	609.75
13	542+65.70	58.39' LT	609.79
14	542+67.48	58.41' LT	610.05
15	542+65.53	63.39' LT	MATCH EX.
16	542+67.29	63.41' LT	MATCH EX.
17	542+75.65	31.37' LT	609.44
18	542+75.63	33.40' LT	609.48
19	542+80.68	29.65' LT	609.44

NORTHWEST CORNER
IL ROUTE 43 (HARLEM AVENUE) AND HERRICK ROAD

POINT No.:	STATION	OFFSET	ELEV.
20	542+80.66	31.44' LT	609.72
21	542+85.69	29.35' LT	609.40
22	542+85.67	31.11' LT	609.79
23	542+90.70	29.31' LT	MATCH EX.
24	542+90.68	31.07' LT	MATCH EX.
25	542+70.16	48.45' LT	609.70
26	542+75.20	48.51' LT	609.78
27	542+69.93	53.44' LT	609.80
28	542+74.96	53.51' LT	609.90
29	542+69.70	58.44' LT	610.10
30	542+74.71	58.51' LT	610.20
31	542+69.46	63.44' LT	MATCH EX.
32	542+74.46	6.50' LT	MATCH EX.
33	542+80.59	37.18' LT	609.80
34	542+80.52	42.18' LT	609.88
35	542+85.59	37.14' LT	609.88
36	542+58.53	42.14' LT	609.96
37	542+90.60	37.20' LT	MATCH EX.
38	542+90.53	42.20' LT	MATCH EX.

SOUTHWEST CORNER
IL ROUTE 43 (HARLEM AVENUE) AND HERRICK ROAD

POINT No.:	STATION	OFFSET	ELEV.
1	542+32.10	42.56' LT	609.81
2	542+30.49	42.54' LT	609.85
3	542+27.58	42.52' LT	609.88
4	542+22.95	42.48' LT	610.20
5	542+23.00	36.48' LT	610.14
6	542+27.69	36.52' LT	609.82
7	542+29.64	36.54' LT	609.78
8	542+32.37	44.69' LT	609.87
9	542+30.79	44.79' LT	609.83
10	542+32.37	46.85' LT	609.85
11	542+30.78	46.80' LT	MATCH EX.
12	542+31.94	55.99' LT	MATCH EX.
13	542+30.36	55.91' LT	MATCH EX.
14	542+26.72	33.20' LT	609.61

SOUTHWEST CORNER
IL ROUTE 43 (HARLEM AVENUE) AND HERRICK ROAD

POINT No.:	STATION	OFFSET	ELEV.
15	542+25.53	34.26' LT	609.98
16	542+23.05	30.77' LT	609.51
17	542+23.03	32.54' LT	610.01
18	542+17.50	29.20' LT	609.29
19	542+17.48	30.77' LT	MATCH EX.
20	542+16.14	29.12' LT	MATCH EX.
21	542+16.13	30.70' LT	MATCH EX.
22	542+17.44	36.44' LT	610.20
23	542+17.39	42.44' LT	610.29
24	542+13.94	36.40' LT	MATCH EX.
25	542+12.89	42.40' LT	MATCH EX.
26	542+22.83	46.57' LT	MATCH EX.
27	542+18.04	46.42' LT	MATCH EX.



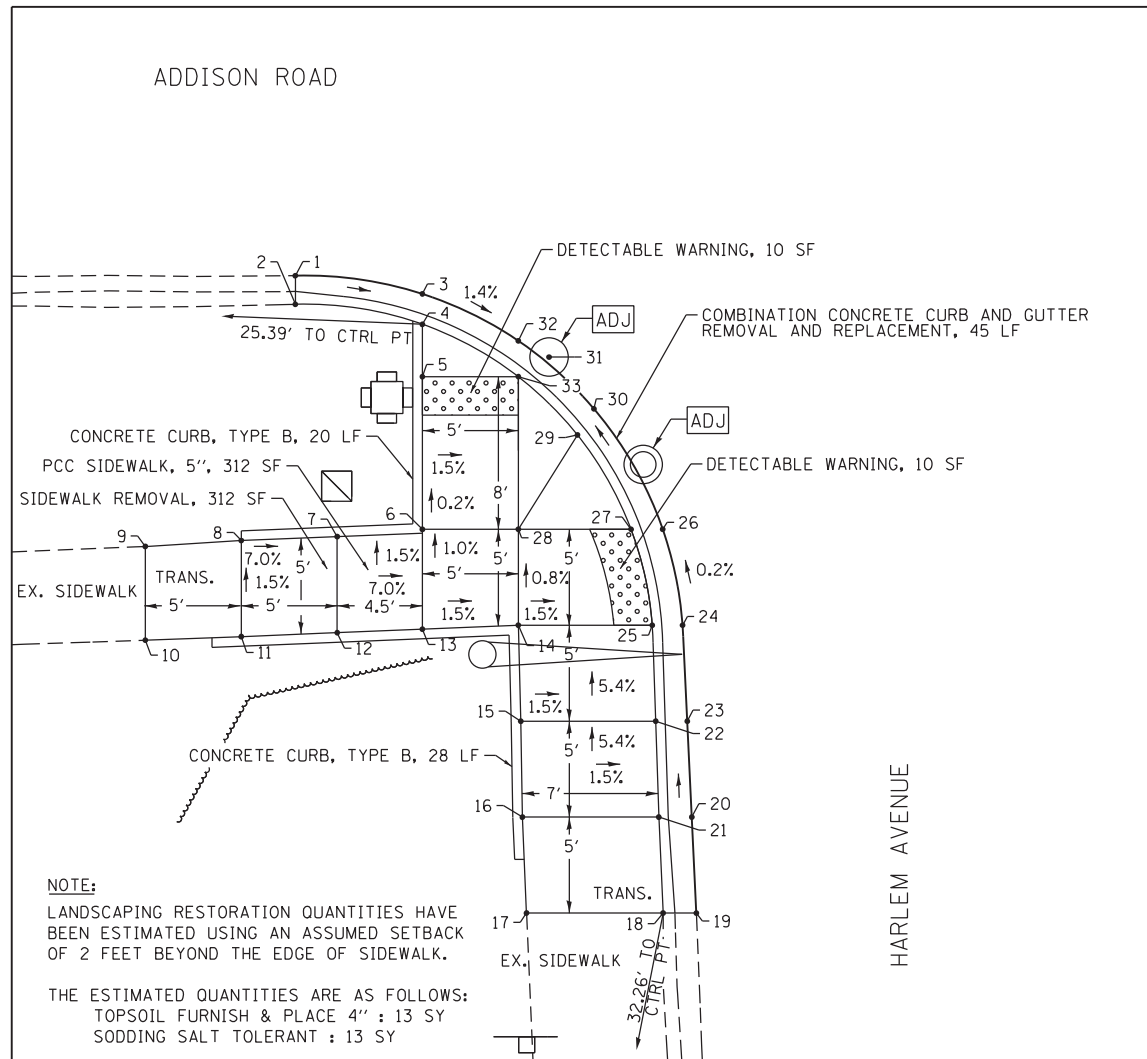
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DESIGNED -
DRAWN -
CHECKED - MTC
DATE - 10/17/2016

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REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL ROUTE 43 (HARLEM AVENUE)
ADA RAMP DETAILS
SCALE: 1" = 5'
SHEET 27 OF 38 SHEETS
STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	(2424 & 3138) RS-3	COOK	75	43
CONTRACT NO. 62C19				
ILLINOIS FED. AID PROJECT				



SOUTHWEST CORNER
IL ROUTE 43 (HARLEM AVENUE) AND ADDISON ROAD

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	546+26.10	47.70' LT	MATCH EX.
2	546+24.61	47.77' LT	MATCH EX.
3	546+24.86	41.13' LT	608.68
4	546+23.28	41.20' LT	608.67
5	546+20.54	41.32' LT	608.68
6	546+12.59	41.67' LT	608.69
7	546+12.40	46.12' LT	609.01
8	546+12.41	51.13' LT	609.37
9	546+12.33	56.14' LT	MATCH EX.
10	546+07.44	56.35' LT	MATCH EX.
11	546+07.41	51.35' LT	609.44
12	546+07.39	46.34' LT	609.09
13	546+07.38	41.90' LT	608.74
14	546+07.36	36.90' LT	608.66
15	546+02.36	36.99' LT	609.20
16	545+97.35	37.12' LT	609.20
17	545+92.34	37.12' LT	MATCH EX.
18	545+92.03	30.01' LT	MATCH EX.
19	545+91.96	28.28' LT	MATCH EX.
20	545+96.97	28.30' LT	609.09
21	545+97.04	30.02' LT	608.63
22	546+02.05	29.96' LT	609.09
23	546+01.98	28.32' LT	608.56
24	546+06.99	28.34' LT	608.55
25	546+07.06	29.92' LT	608.54
26	546+12.03	29.16' LT	608.54
27	546+12.11	30.81' LT	608.53
28	546+12.37	36.68' LT	608.62
29	546+17.15	33.38' LT	609.05
30	546+18.47	32.46' LT	608.53
31	546+21.27	34.68' LT	608.50
32	546+22.19	36.24' LT	608.60
33	546+20.32	36.33' LT	608.59

NOTE:
LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
TOPSOIL FURNISH & PLACE 4" : 13 SY
SODDING SALT TOLERANT : 13 SY

CONTROL POINTS:
CUT CROSS CUT CROSS
ELEV: 609.54 ELEV: 609.19
STA: 546+25.43 STA: 545+56.82
OS: 66.50' LT OS: 32.48' LT
N: 1882491.846 N: 1882424.896
E: 1128696.256 E: 1128733.214

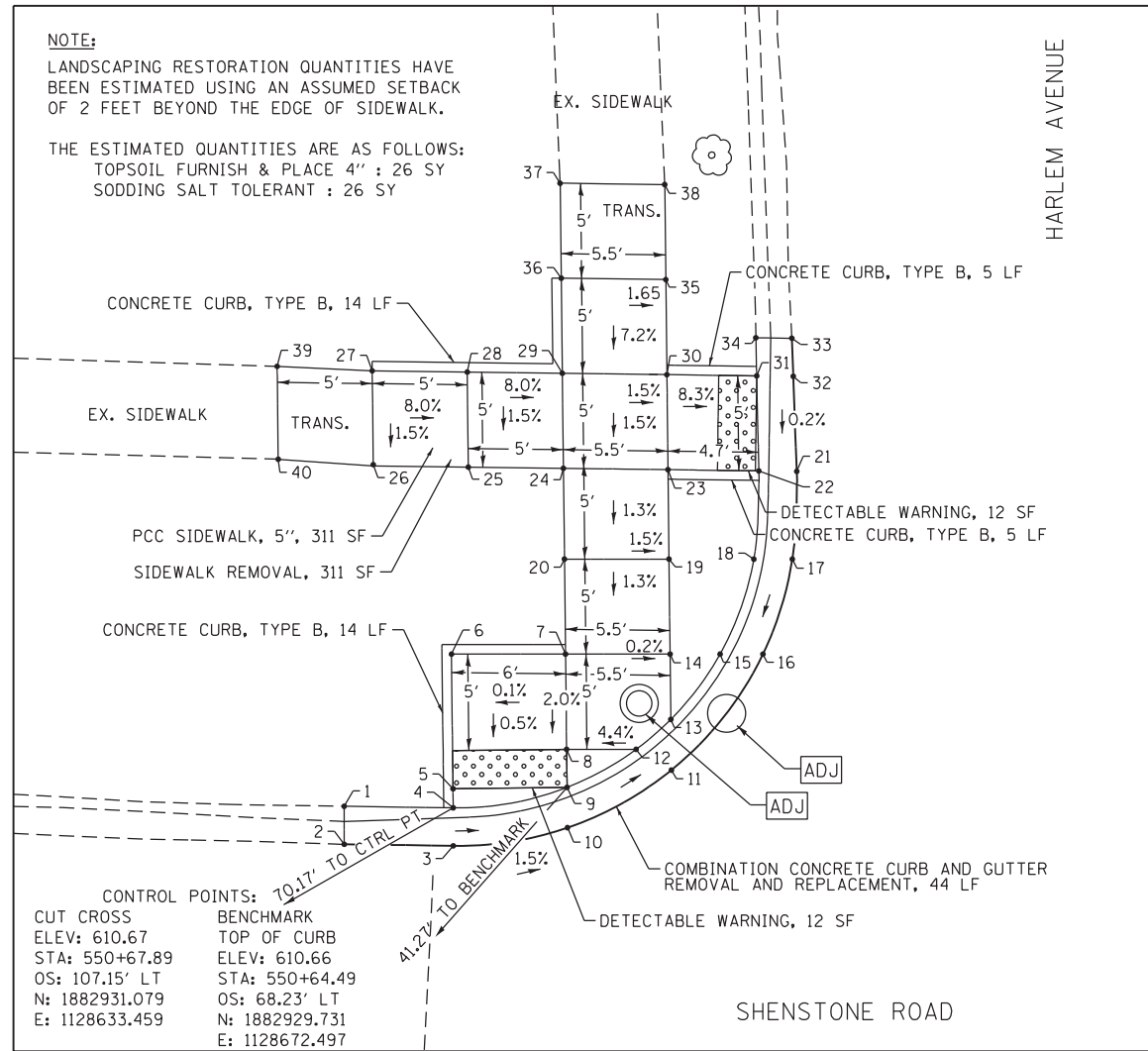


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	DRAWN -	REVISED -
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PLOT DATE = 10/17/2016	DATE - 10/17/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL ROUTE 43 (HARLEM AVENUE)
ADA RAMP DETAILS
SCALE: 1" = 5'
SHEET 28 OF 38 SHEETS
STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	(2424 & 3138) RS-3	COOK	75	44
CONTRACT NO. 62C19				
ILLINOIS FED. AID PROJECT				

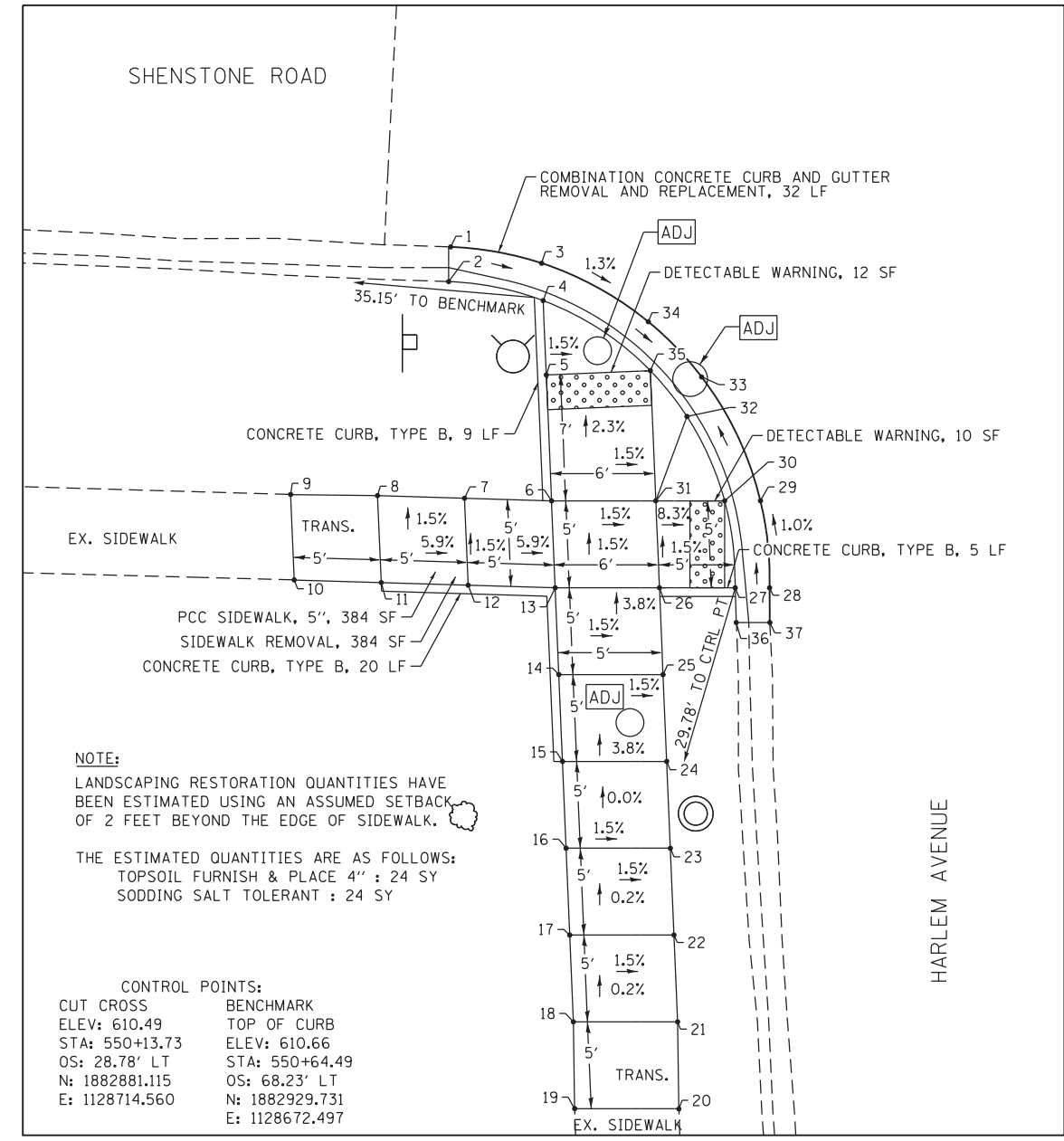


NORTHWEST CORNER
IL ROUTE 43 (HARLEM AVENUE) AND SHENSTONE ROAD

POINT No.:	STATION	OFFSET	ELEV.
1	550+84.91	45.20' LT	MATCH EX.
2	550+82.91	45.27' LT	MATCH EX.
3	550+82.64	39.51' LT	610.17
4	550+84.64	39.44' LT	610.16
5	550+85.65	39.45' LT	610.17
6	550+92.72	39.31' LT	610.20
7	550+92.53	33.31' LT	610.21
8	550+87.52	33.41' LT	610.11
9	550+85.52	33.45' LT	610.07
10	550+83.40	33.50' LT	610.08
11	550+86.26	27.94' LT	609.77
12	550+87.41	29.76' LT	610.26
13	550+88.92	27.88' LT	610.21
14	550+92.36	27.81' LT	610.20
15	550+92.27	25.18' LT	610.27
16	550+92.20	22.93' LT	609.77
17	550+97.15	21.22' LT	609.94
18	550+97.21	23.25' LT	610.44
19	550+97.36	27.71' LT	610.19

NORTHWEST CORNER
IL ROUTE 43 (HARLEM AVENUE) AND SHENSTONE ROAD

POINT No.:	STATION	OFFSET	ELEV.
20	550+97.53	33.21' LT	610.27
21	551+01.75	20.84' LT	609.97
22	551+01.84	22.84' LT	609.94
23	551+02.06	27.61' LT	610.26
24	551+02.31	33.11' LT	610.34
25	551+02.53	38.11' LT	610.74
26	551+02.97	43.10' LT	611.14
27	551+07.76	43.00' LT	611.22
28	551+07.53	38.00' LT	610.82
29	551+07.31	33.01' LT	610.42
30	551+07.06	27.51' LT	610.34
31	551+06.85	22.79' LT	609.95
32	551+06.76	20.87' LT	609.96
33	551+08.76	20.88' LT	MATCH EX.
34	551+08.85	22.77' LT	MATCH EX.
35	551+12.06	27.41' LT	610.70
36	551+12.31	32.90' LT	610.78
37	551+17.31	32.80' LT	MATCH EX.
38	551+17.06	27.31' LT	MATCH EX.
39	551+08.15	47.99' LT	MATCH EX.
40	551+03.26	48.09' LT	MATCH EX.



SOUTHWEST CORNER
IL ROUTE 43 (HARLEM AVENUE) AND SHENSTONE ROAD

POINT No.:	STATION	OFFSET	ELEV.
1	550+63.46	38.64' LT	MATCH EX.
2	550+61.46	38.74' LT	MATCH EX.
3	550+62.24	33.34' LT	610.00
4	550+60.08	33.36' LT	609.99
5	550+55.79	33.41' LT	609.99
6	550+48.54	33.48' LT	610.16
7	550+48.95	38.48' LT	610.59
8	550+49.37	43.48' LT	610.75
9	550+49.69	48.47' LT	MATCH EX.
10	550+44.77	48.53' LT	MATCH EX.
11	550+44.36	43.53' LT	610.82
12	550+43.94	38.53' LT	610.52
13	550+43.53	33.54' LT	610.24
14	550+38.52	33.59' LT	610.43

POINT No.:	STATION	OFFSET	ELEV.
15	550+33.52	33.65' LT	610.62
16	550+28.52	33.70' LT	610.62
17	550+23.51	33.76' LT	610.63
18	550+18.51	33.81' LT	610.64
19	550+13.51	33.99' LT	MATCH EX.
20	550+13.20	28.00' LT	MATCH EX.
21	550+18.19	27.81' LT	610.54
22	550+23.20	27.76' LT	610.54
23	550+28.20	27.70' LT	610.53
24	550+33.20	27.65' LT	610.53
25	550+38.21	27.59' LT	610.34
26	550+43.21	27.54' LT	610.15
27	550+42.98	23.18' LT	609.80
28	550+42.88	21.21' LT	609.80

POINT No.:	STATION	OFFSET	ELEV.
29	550+47.91	21.47' LT	609.75
30	550+48.02	23.53' LT	609.74
31	550+48.23	27.49' LT	610.07
32	550+52.98	25.44' LT	610.06
33	550+55.21	24.48' LT	609.68
34	550+58.55	27.38' LT	609.91
35	550+55.73	27.41' LT	609.90
36	550+40.98	23.23' LT	MATCH EX.
37	550+40.88	21.30' LT	MATCH EX.



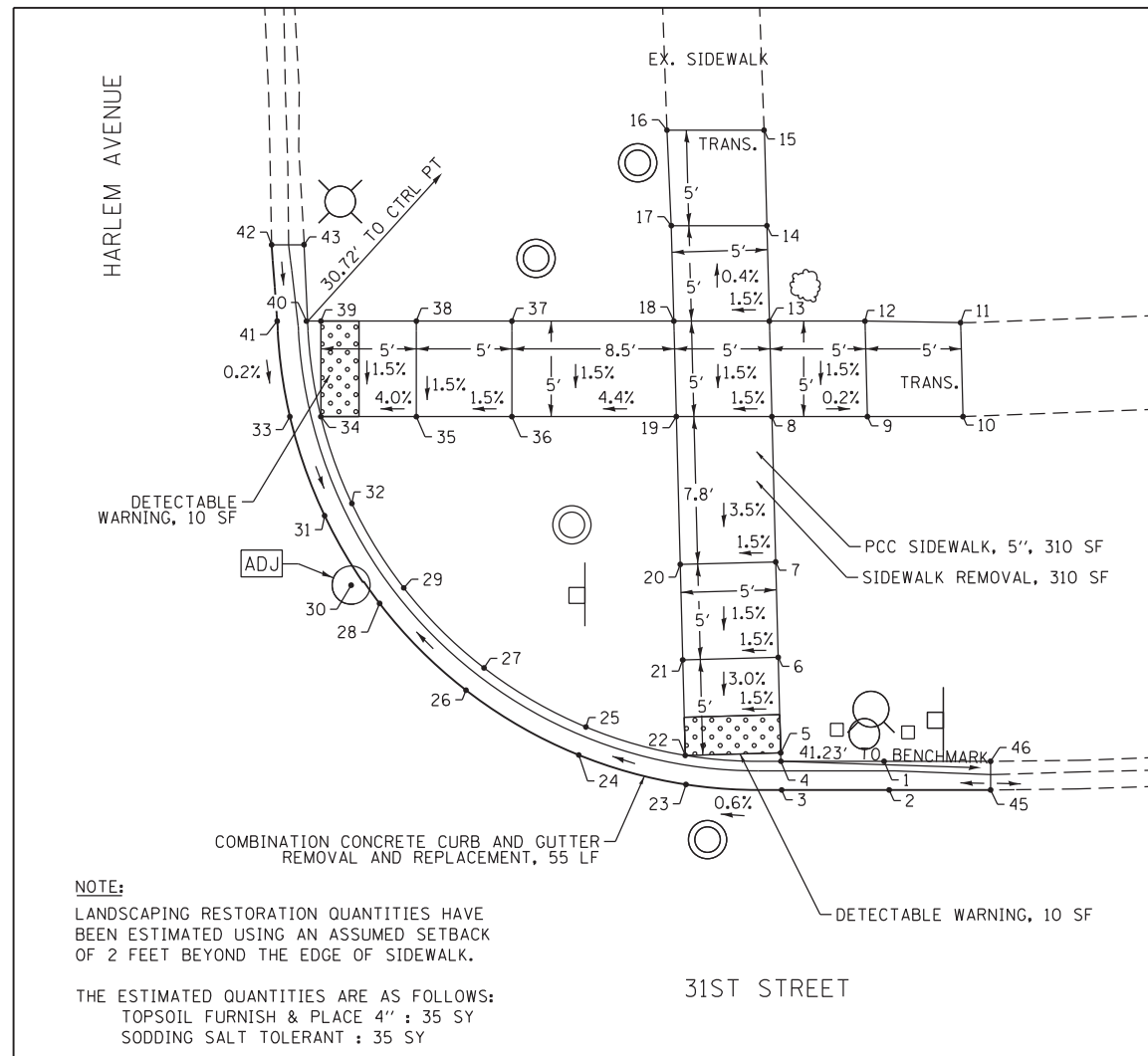
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PLOT SCALE = 10.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 10/17/2016	CHECKED - MTC	REVISED -
	DATE - 10/17/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL ROUTE 43 (HARLEM AVENUE)
ADA RAMP DETAILS

SCALE: 1" = 5' SHEET 30 OF 38 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	(2424 & 3138) RS-3	COOK	75	46
CONTRACT NO. 62C19				
ILLINOIS FED. AID PROJECT				



NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 35 SY
 SODDING SALT TOLERANT : 35 SY

CONTROL POINTS:
 CUT CROSS BENCHMARK
 ELEV: 610.72 TOP OF CURB
 STA: 553+98.05 ELEV: 610.65
 OS: 45.71' RT STA: 553+49.81
 N: 1883268.137 OS: 91.34' RT
 E: 1128775.616 N: 1883221.352
 E: 1128822.681

NORTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 31ST STREET

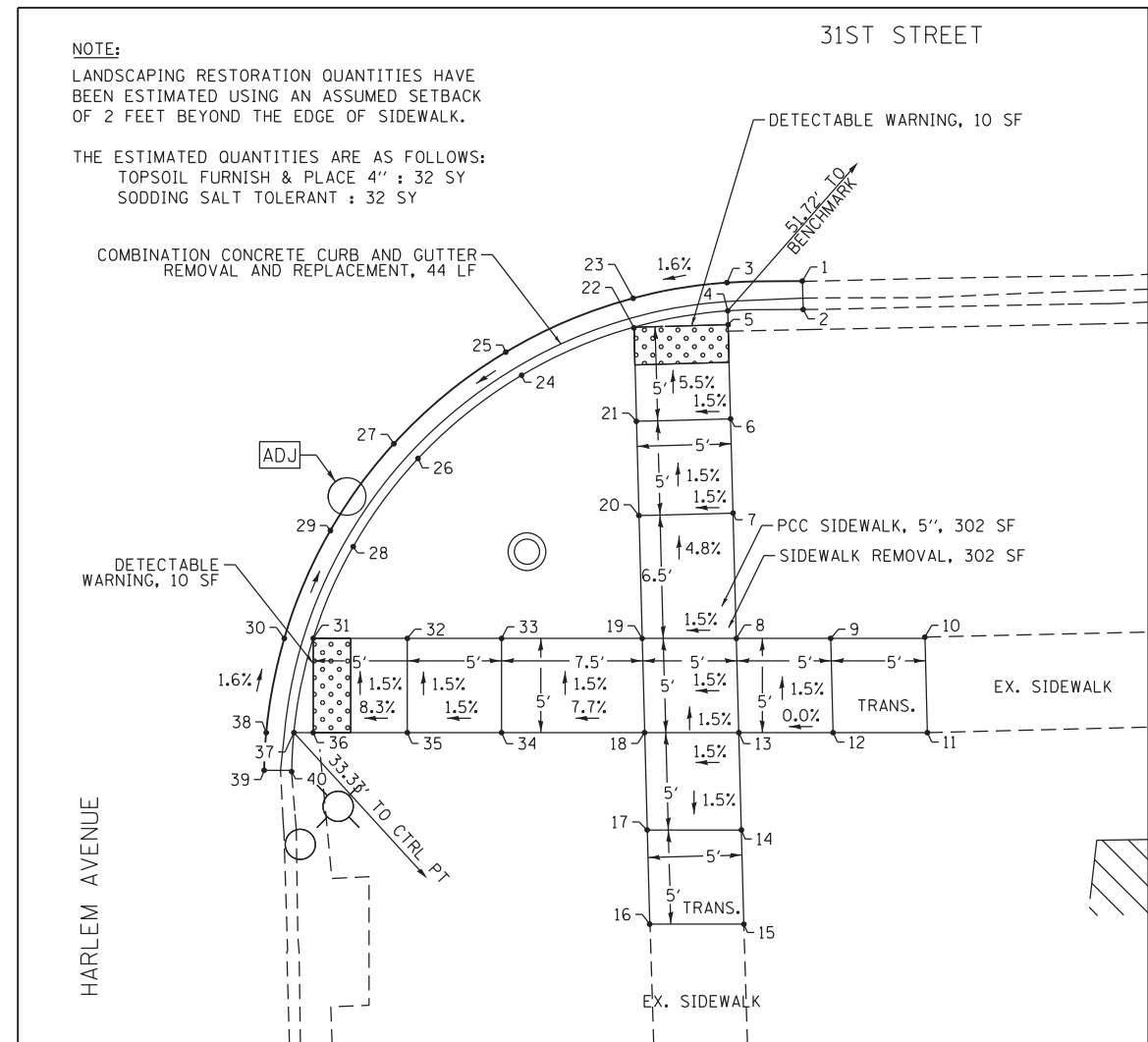
POINT No.:	STATION	OFFSET	ELEV.
1	553+50.52	55.48' RT	610.83
2	553+49.01	55.69' RT	610.56
3	553+49.19	50.07' RT	610.42
4	553+50.69	50.08' RT	610.41
5	553+51.13	50.08' RT	610.42
6	553+56.13	50.11' RT	610.57
7	553+61.13	50.13' RT	610.65
8	553+68.74	50.17' RT	610.92
9	553+68.58	55.17' RT	610.91
10	553+68.43	60.17' RT	MATCH EX.
11	553+73.35	60.20' RT	MATCH EX.
12	553+73.58	55.20' RT	610.99
13	553+73.74	50.20' RT	611.00
14	553+78.74	50.22' RT	610.98
15	553+83.75	50.25' RT	MATCH EX.

NORTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 31ST STREET

POINT No.:	STATION	OFFSET	ELEV.
16	553+83.91	45.15' RT	MATCH EX.
17	553+78.90	45.22' RT	610.90
18	553+73.90	45.20' RT	610.92
19	553+68.90	45.17' RT	610.84
20	553+61.15	45.13' RT	610.56
21	553+56.15	45.11' RT	610.50
22	553+51.15	45.08' RT	610.38
23	553+49.63	45.08' RT	610.39
24	553+51.35	39.51' RT	610.30
25	553+52.80	39.93' RT	610.77
26	553+54.92	33.72' RT	610.18
27	553+56.06	34.70' RT	610.65
28	553+59.61	29.34' RT	610.13
29	553+60.44	30.59' RT	610.87
30	553+60.61	27.88' RT	610.10

NORTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 31ST STREET

POINT No.:	STATION	OFFSET	ELEV.
31	553+64.29	26.61' RT	610.20
32	553+64.89	28.06' RT	610.67
33	553+69.53	24.96' RT	610.25
34	553+69.48	26.58' RT	610.24
35	553+69.32	31.58' RT	610.38
36	553+69.17	36.57' RT	610.47
37	553+74.16	36.73' RT	610.54
38	553+74.32	31.73' RT	610.46
39	553+74.48	26.73' RT	610.26
40	553+74.50	26.05' RT	610.25
41	553+74.55	24.45' RT	610.26
42	553+78.56	24.28' RT	MATCH EX.
43	553+78.50	25.98' RT	MATCH EX.
44	553+50.34	61.05' RT	MATCH EX.
45	553+48.84	61.00' RT	MATCH EX.



NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 32 SY
 SODDING SALT TOLERANT : 32 SY

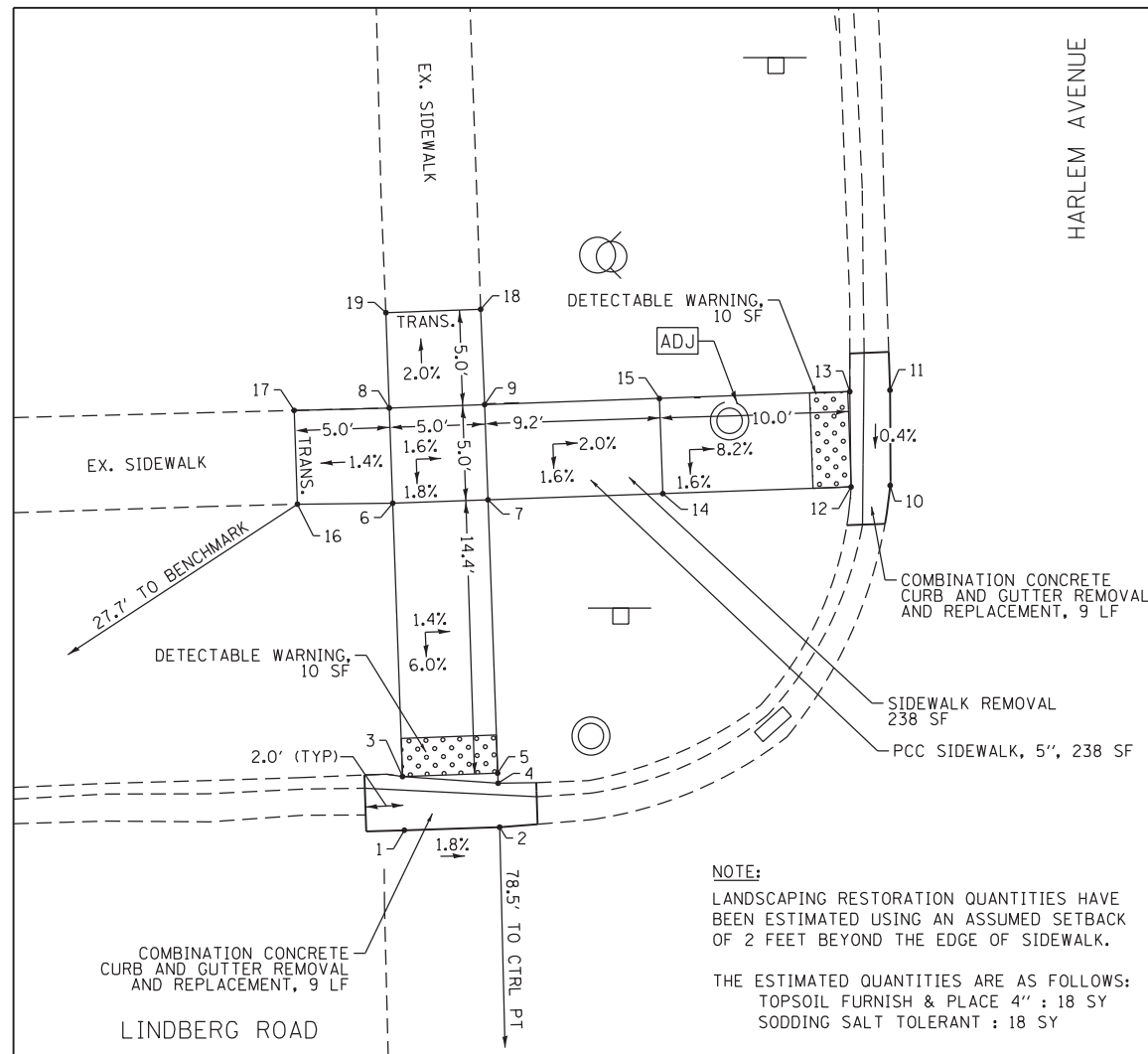
CONTROL POINTS:
 CUT CROSS BENCHMARK
 ELEV: 611.26 TOP OF CURB
 STA: 552+71.01 ELEV: 610.65
 OS: 45.19' RT STA: 553+49.81
 N: 1883141.162 OS: 91.34' RT
 E: 1128779.060 N: 1883221.352
 E: 1128822.681

SOUTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 31ST STREET

POINT No.:	STATION	OFFSET	ELEV.
1	553+20.26	53.92' RT	MATCH EX.
2	553+18.75	53.91' RT	MATCH EX.
3	553+20.30	49.92' RT	610.72
4	553+18.80	49.91' RT	610.71
5	553+18.07	49.91' RT	610.72
6	553+13.07	49.88' RT	610.99
7	553+08.07	49.85' RT	611.06
8	553+01.42	49.81' RT	611.38
9	553+01.26	54.81' RT	611.38
10	553+01.10	59.81' RT	MATCH EX.
11	552+96.09	59.78' RT	MATCH EX.
12	552+96.25	54.78' RT	611.45
13	552+96.41	49.78' RT	611.45
14	552+91.25	49.76' RT	611.40
15	552+86.25	49.73' RT	MATCH EX.
16	552+86.41	44.73' RT	MATCH EX.
17	552+91.41	44.76' RT	611.31
18	552+96.57	44.79' RT	611.38
19	553+01.57	44.81' RT	611.30
20	553+08.10	44.85' RT	610.98

SOUTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND 31ST STREET

POINT No.:	STATION	OFFSET	ELEV.
21	553+13.10	44.88' RT	610.91
22	553+18.10	44.91' RT	610.63
23	553+19.63	44.91' RT	610.64
24	553+15.74	38.86' RT	610.88
25	553+16.99	38.06' RT	610.41
26	553+11.50	33.23' RT	610.78
27	553+12.32	31.97' RT	610.31
28	553+06.92	29.63' RT	610.57
29	553+07.82	28.46' RT	610.10
30	553+02.18	25.83' RT	610.23
31	553+02.13	27.36' RT	610.22
32	553+01.97	32.36' RT	610.64
33	553+01.81	37.36' RT	610.72
34	552+96.82	37.20' RT	610.79
35	552+96.97	32.20' RT	610.72
36	552+97.13	27.20' RT	610.30
37	552+97.17	26.18' RT	610.30
38	552+97.21	24.72' RT	610.31
39	552+95.22	24.54' RT	MATCH EX.
40	552+95.17	26.00' RT	MATCH EX.

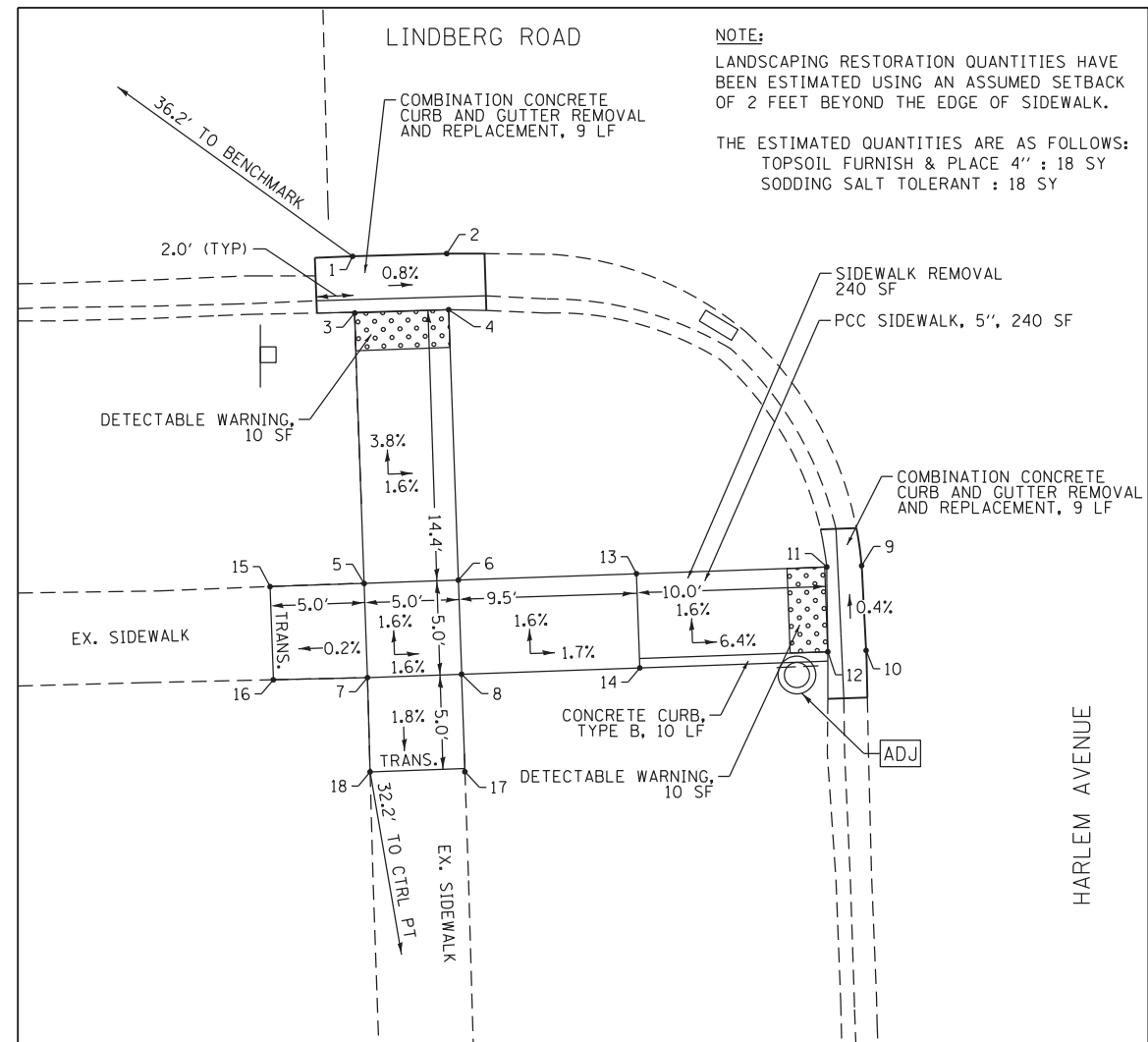


NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 18 SY
 SODDING SALT TOLERANT : 18 SY

CONTROL POINTS:
 CUT CROSS BENCHMARK
 ELEV: 611.69 ELEV: 611.64
 STA: 559+36.88 STA: 560+18.56
 OS: 44.27' LT OS: 77.43' LT
 N: 1883803.877 N: 1883884.143
 E: 1128668.724 E: 1128632.970

ADA RAMP ELEVATION TABLE

POINT No.:	STATION	OFFSET	ELEV.
1	560+15.63	48.73' LT	611.24
2	560+15.61	43.72' LT	611.15
3	560+18.45	48.73' LT	611.23
4	560+17.92	43.73' LT	611.15
5	560+18.45	43.73' LT	611.15
6	560+32.80	48.73' LT	612.10
7	560+32.80	43.73' LT	612.03
8	560+37.80	48.73' LT	612.19
9	560+37.80	43.73' LT	612.11
10	560+32.80	22.61' LT	611.10
11	560+37.80	22.44' LT	611.12
12	560+32.80	24.66' LT	611.09
13	560+37.80	24.54' LT	611.11
14	560+32.80	34.54' LT	611.91
15	560+37.80	34.54' LT	611.93
16	560+32.92	53.73' LT	MATCH EX.
17	560+37.84	53.73' LT	MATCH EX.
18	560+42.80	43.75' LT	MATCH EX.
19	560+42.80	48.73' LT	MATCH EX.

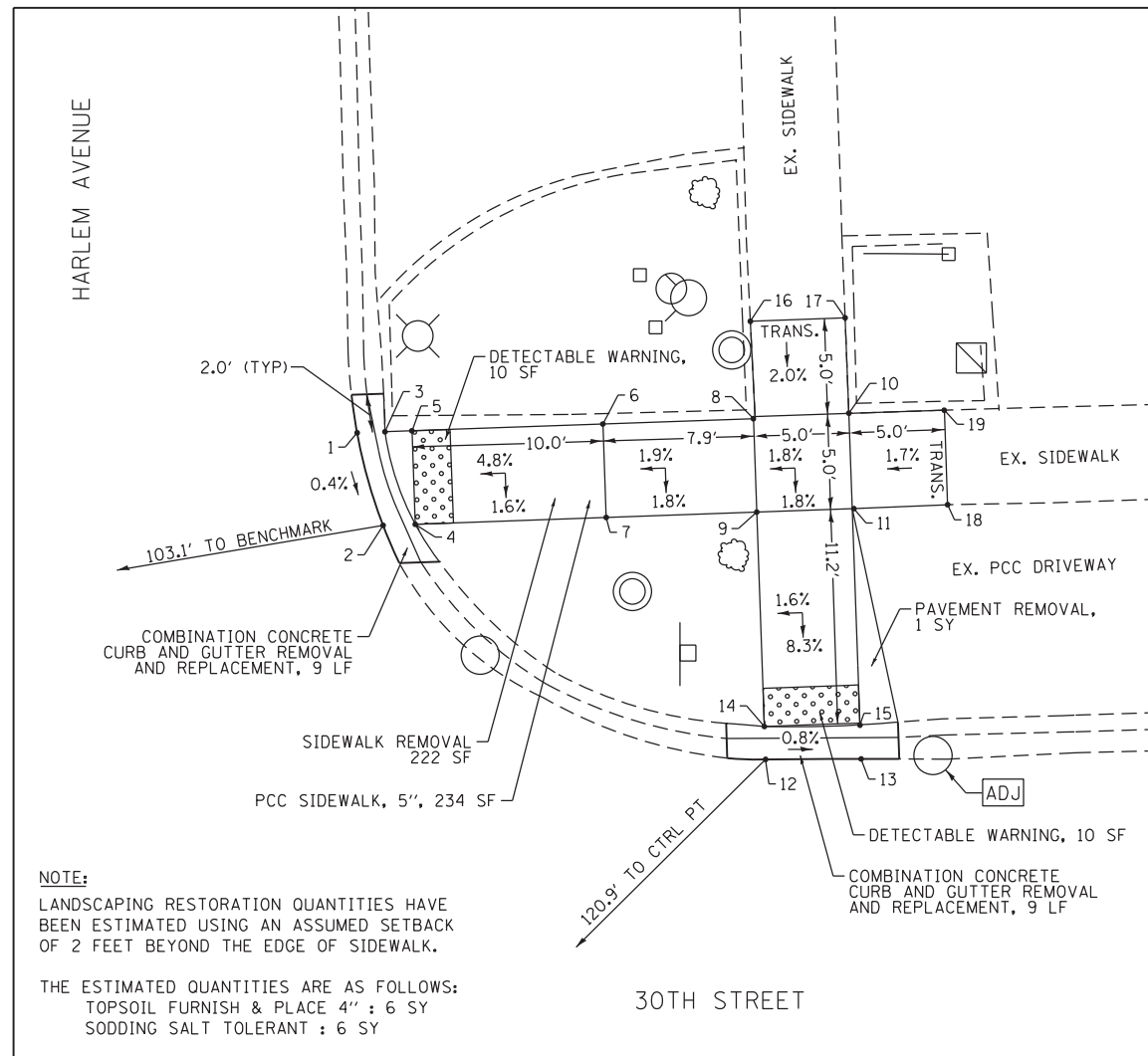


NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 18 SY
 SODDING SALT TOLERANT : 18 SY

CONTROL POINTS:
 BENCHMARK CUT CROSS
 ELEV: 611.64 ELEV: 611.69
 STA: 560+18.56 STA: 559+36.88
 OS: 77.43' LT OS: 44.27' LT
 N: 1883803.877 N: 1883884.143
 E: 1128632.970 E: 1128668.724

ADA RAMP ELEVATION TABLE

POINT No.:	STATION	OFFSET	ELEV.
1	559+96.15	48.79' LT	611.23
2	559+96.14	43.79' LT	611.19
3	559+93.13	48.78' LT	611.21
4	559+93.16	43.78' LT	611.17
5	559+78.78	48.72' LT	611.75
6	559+78.80	43.72' LT	611.67
7	559+73.78	48.70' LT	611.83
8	559+73.80	43.70' LT	611.75
9	559+78.90	22.30' LT	610.93
10	559+74.40	22.21' LT	610.95
11	559+78.89	24.16' LT	610.91
12	559+74.39	24.24' LT	610.94
13	559+78.84	34.26' LT	611.50
14	559+73.84	34.24' LT	611.58
15	559+78.77	53.72' LT	MATCH EX.
16	559+73.83	53.70' LT	MATCH EX.
17	559+68.80	43.69' LT	MATCH EX.
18	559+68.78	48.72' LT	MATCH EX.



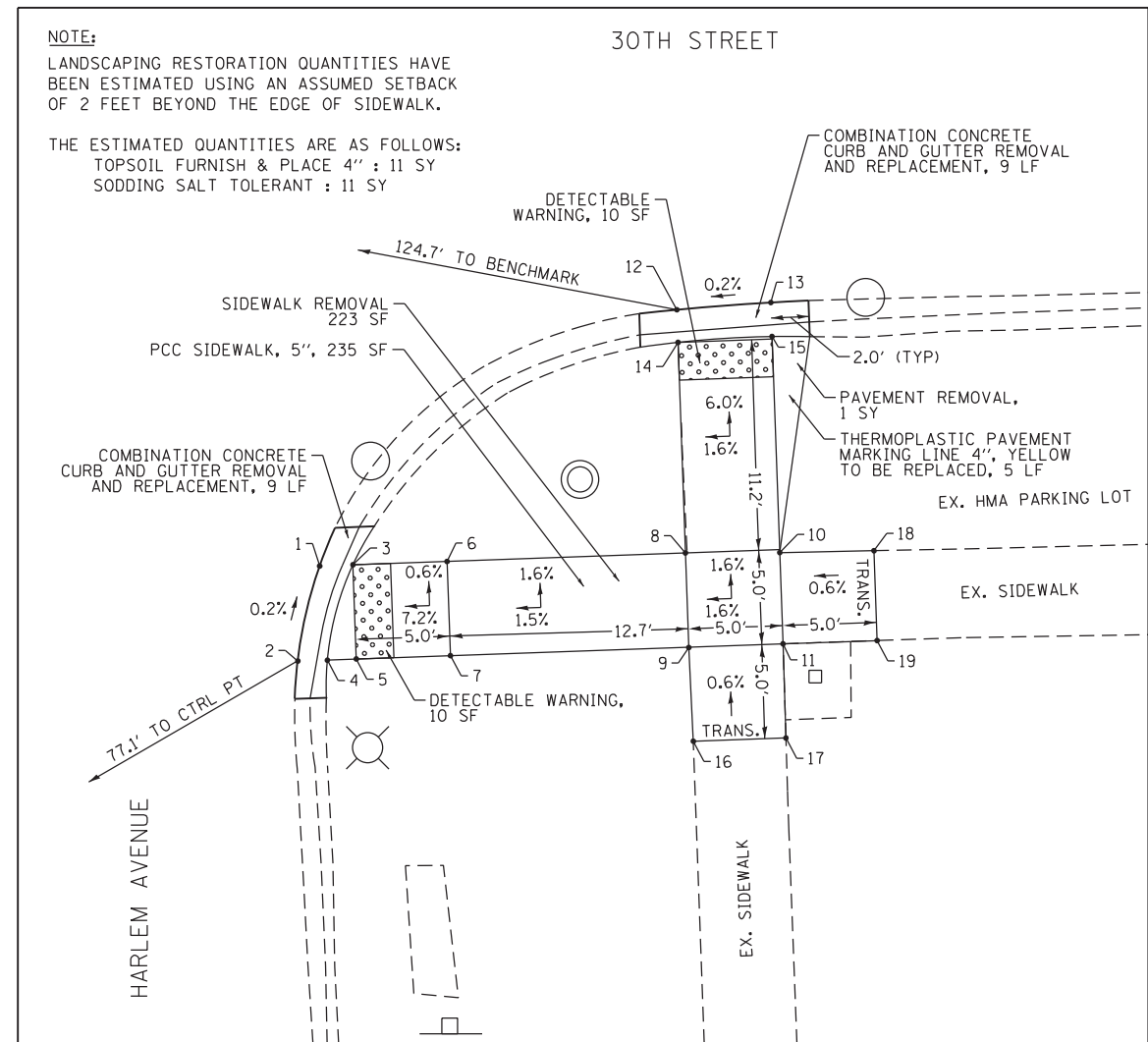
NOTE:
LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.

THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
TOPSOIL FURNISH & PLACE 4" : 6 SY
SODDING SALT TOLERANT : 6 SY

CONTROL POINTS:
CUT CROSS BENCHMARK
ELEV: 611.69 ELEV: 611.64
STA: 559+36.88 STA: 560+18.56
OS: 44.27' LT STA: 560+18.56
N: 1883803.877 OS: 77.43' LT
E: 1128668.724 N: 1883884.143
E: 1128632.970

ADA RAMP ELEVATION TABLE

POINT No.:	STATION	OFFSET	ELEV.
1	560+36.96	23.54' RT	610.92
2	560+32.07	24.73' RT	610.90
3	560+36.96	25.01' RT	610.91
4	560+32.07	26.42' RT	610.89
5	560+36.96	26.42' RT	610.93
6	560+36.96	36.42' RT	611.41
7	560+32.07	36.42' RT	611.33
8	560+36.96	44.31' RT	611.56
9	560+32.07	44.31' RT	611.47
10	560+37.07	49.31' RT	611.65
11	560+32.07	49.31' RT	611.55
12	560+19.11	44.31' RT	610.67
13	560+18.96	49.31' RT	610.63
14	560+20.84	44.31' RT	610.66
15	560+20.74	49.31' RT	610.62
16	560+42.07	44.33' RT	MATCH EX.
17	560+42.07	49.29' RT	MATCH EX.
18	560+32.10	54.31' RT	MATCH EX.
19	560+37.05	54.31' RT	MATCH EX.



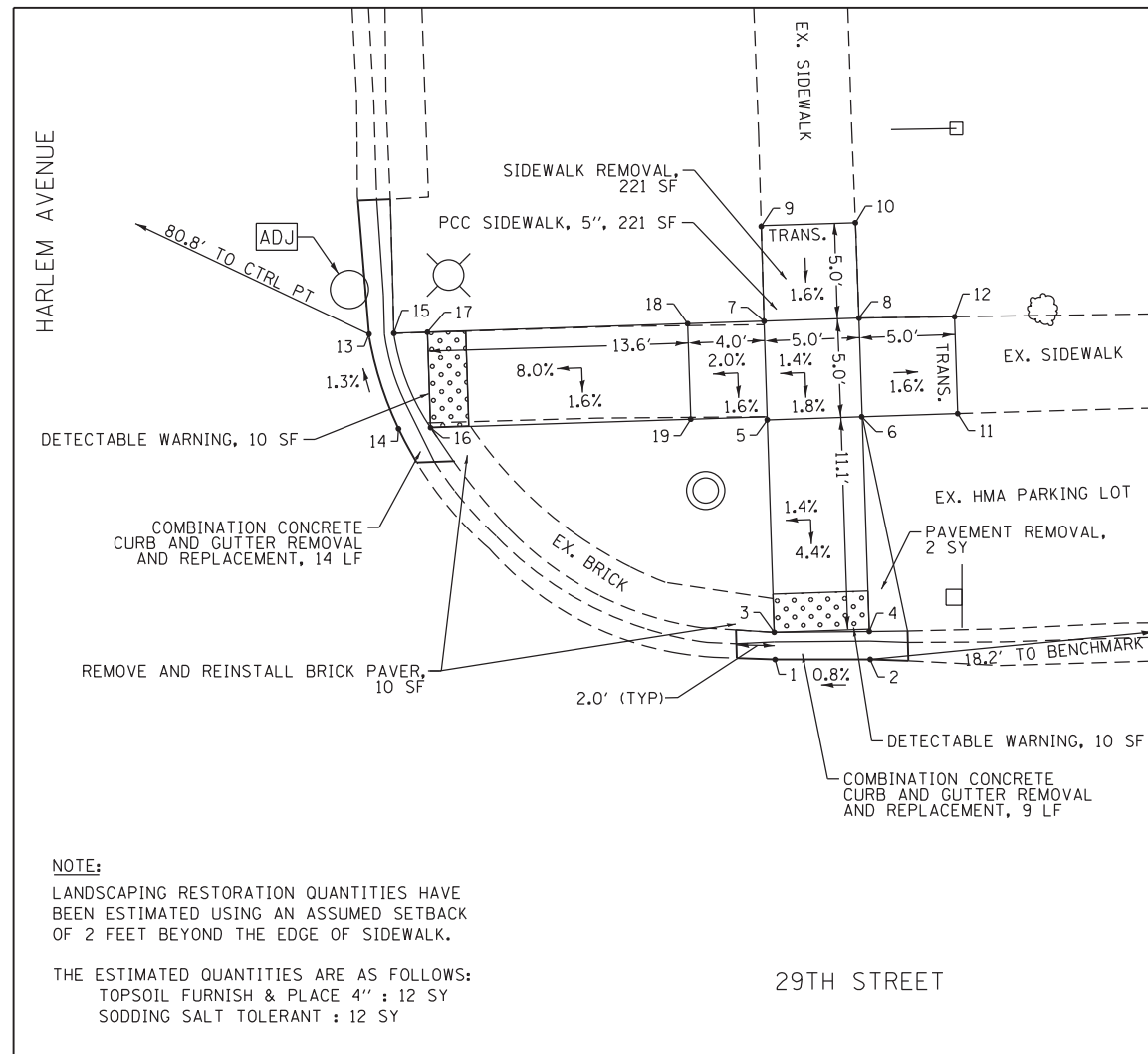
NOTE:
LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.

THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
TOPSOIL FURNISH & PLACE 4" : 11 SY
SODDING SALT TOLERANT : 11 SY

CONTROL POINTS:
BENCHMARK CONC. BACK OF CURB
ELEV: 611.64 ELEV: 611.69
STA: 560+18.56 STA: 559+36.88
OS: 77.43' LT OS: 44.27' LT
N: 1883803.877 N: 1883884.143
E: 1128632.970 E: 1128668.724

ADA RAMP ELEVATION TABLE

POINT No.:	STATION	OFFSET	ELEV.
1	559+78.40	24.94' RT	610.56
2	559+73.39	23.63' RT	610.57
3	559+78.41	26.71' RT	610.55
4	559+73.40	25.20' RT	610.56
5	559+73.40	26.73' RT	610.58
6	559+78.43	31.71' RT	610.91
7	559+73.42	31.73' RT	610.94
8	559+78.48	44.37' RT	611.05
9	559+73.46	44.39' RT	611.13
10	559+78.50	49.40' RT	611.13
11	559+73.50	49.39' RT	611.21
12	559+91.40	44.31' RT	610.46
13	559+91.62	49.31' RT	610.47
14	559+89.66	44.32' RT	610.45
15	559+89.82	49.32' RT	610.46
16	559+68.48	44.47' RT	MATCH EX.
17	559+68.50	49.39' RT	MATCH EX.
18	559+78.29	54.37' RT	MATCH EX.
19	559+73.51	54.39' RT	MATCH EX.

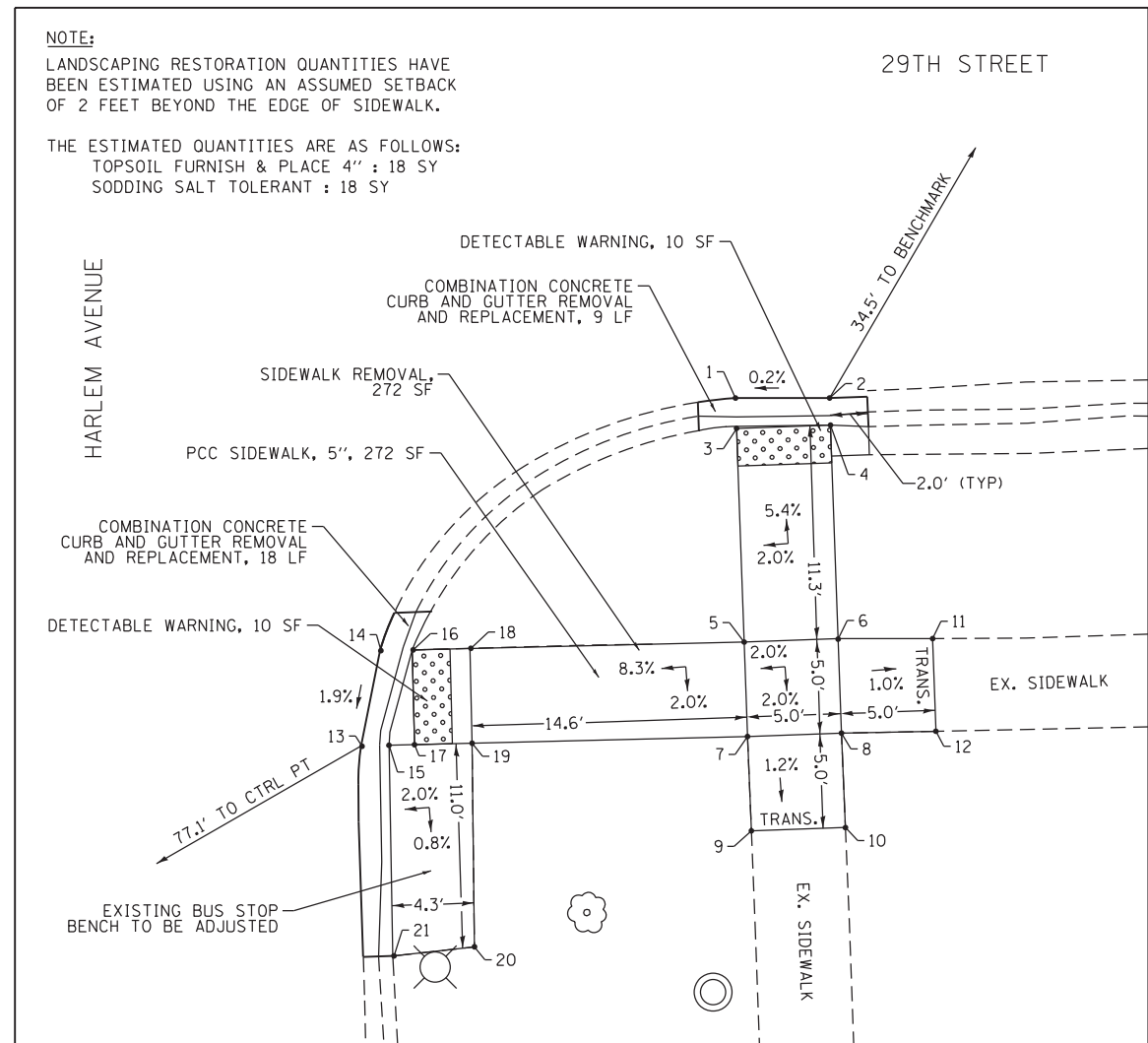


NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 12 SY
 SODDING SALT TOLERANT : 12 SY

CONTROL POINTS:
 CUT CROSS BENCHMARK
 ELEV: 620.52 CONC. BACK OF CURB
 STA: 567+37.39 ELEV: 619.78
 OS: 46.39' LT STA: 566+84.03
 N: 1884603.888 OS: 69.47' RT
 E: 1128638.762 N: 1884554.052
 E: 1128756.177

NORTHEAST CORNER
IL ROUTE 43 (HARLEM AVENUE) AND 29TH STREET

POINT No.:	STATION	OFFSET	ELEV.
1	566+82.97	46.28' RT	619.69
2	566+82.83	51.26' RT	619.73
3	566+84.39	46.28' RT	619.68
4	566+84.29	51.26' RT	619.72
5	566+95.52	46.25' RT	620.14
6	566+95.53	51.23' RT	620.21
7	567+00.69	46.24' RT	620.22
8	567+00.70	51.22' RT	620.31
9	567+05.69	46.25' RT	MATCH EX.
10	567+05.70	51.17' RT	MATCH EX.
11	566+95.54	56.23' RT	MATCH EX.
12	567+00.62	56.22' RT	MATCH EX.
13	567+00.64	25.56' RT	619.03
14	566+95.64	26.95' RT	619.10
15	567+00.63	26.86' RT	619.02
16	566+95.64	28.62' RT	619.09
17	567+00.64	28.61' RT	619.05
18	567+00.68	42.24' RT	620.14
19	566+95.68	42.25' RT	620.06



NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 18 SY
 SODDING SALT TOLERANT : 18 SY

CONTROL POINTS:
 BENCHMARK CONC. BACK OF CURB
 ELEV: 619.78 STA: 566+84.03
 OS: 69.47' RT
 N: 1884554.052
 E: 1128756.177

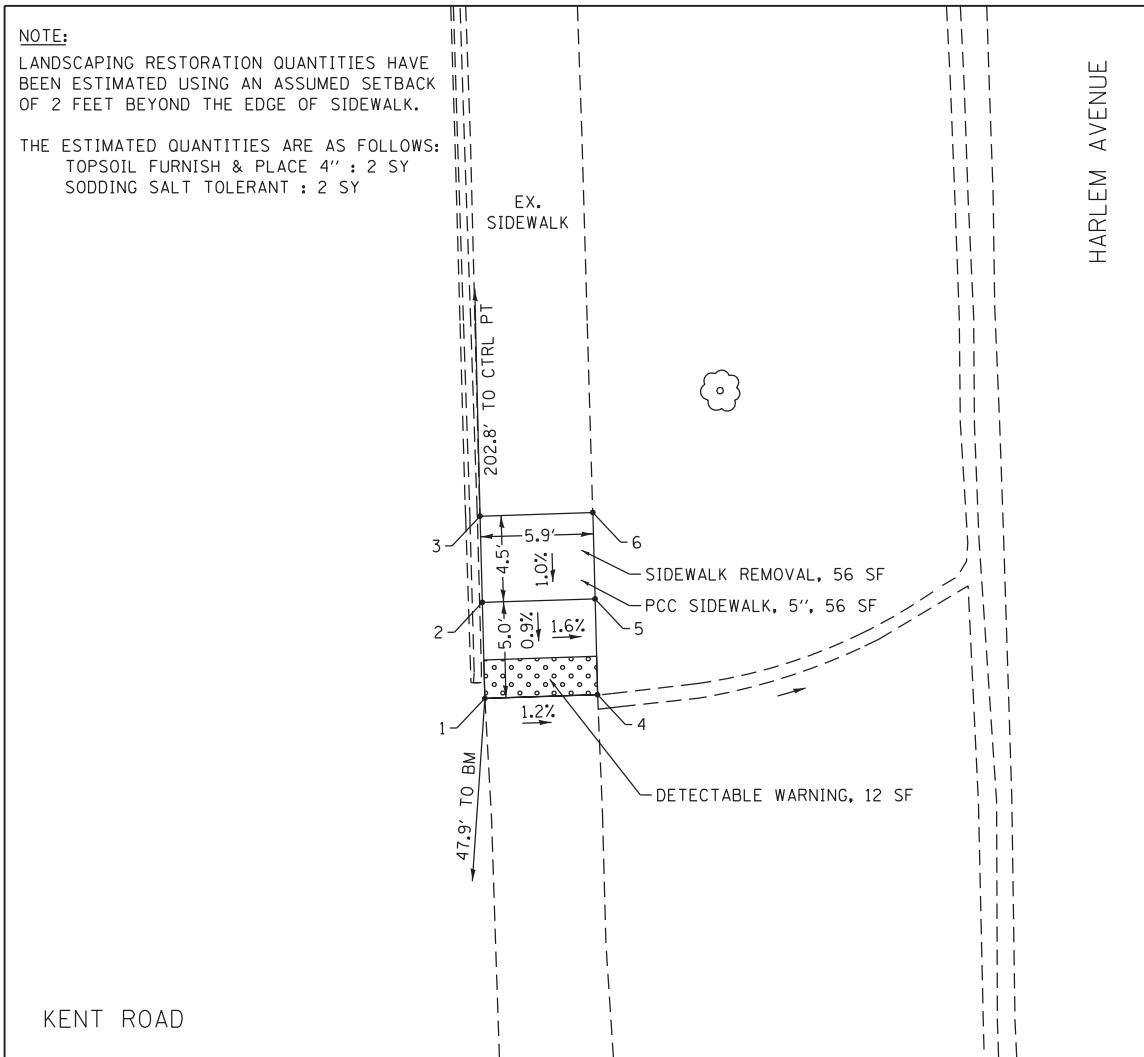
CUT CROSS BENCHMARK
 ELEV: 619.55
 STA: 566+00.96
 OS: 42.52' LT
 N: 1884467.416
 E: 1128647.128

SOUTHEAST CORNER
IL ROUTE 43 (HARLEM AVENUE) AND 29TH STREET

POINT No.:	STATION	OFFSET	ELEV.
1	566+54.64	46.12' RT	619.59
2	566+54.49	51.09' RT	619.60
3	566+53.16	46.12' RT	619.58
4	566+53.06	51.09' RT	619.59
5	566+41.74	46.12' RT	620.10
6	566+41.74	51.09' RT	620.20
7	566+36.74	46.12' RT	620.00
8	566+36.74	51.09' RT	620.10
9	566+31.74	46.14' RT	MATCH EX.
10	566+31.74	51.13' RT	MATCH EX.
11	566+41.57	56.09' RT	MATCH EX.
12	566+36.66	56.09' RT	MATCH EX.
13	566+36.97	25.66' RT	618.72
14	566+41.97	26.83' RT	618.82
15	566+36.95	27.13' RT	618.71
16	566+41.95	28.54' RT	618.81
17	566+36.94	28.48' RT	618.73
18	566+41.93	31.59' RT	618.87
19	566+36.91	31.54' RT	618.79
20	566+26.14	31.28' RT	618.87
21	566+25.79	26.94' RT	MATCH EX.

NOTE:
LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.

THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
TOPSOIL FURNISH & PLACE 4" : 2 SY
SODDING SALT TOLERANT : 2 SY

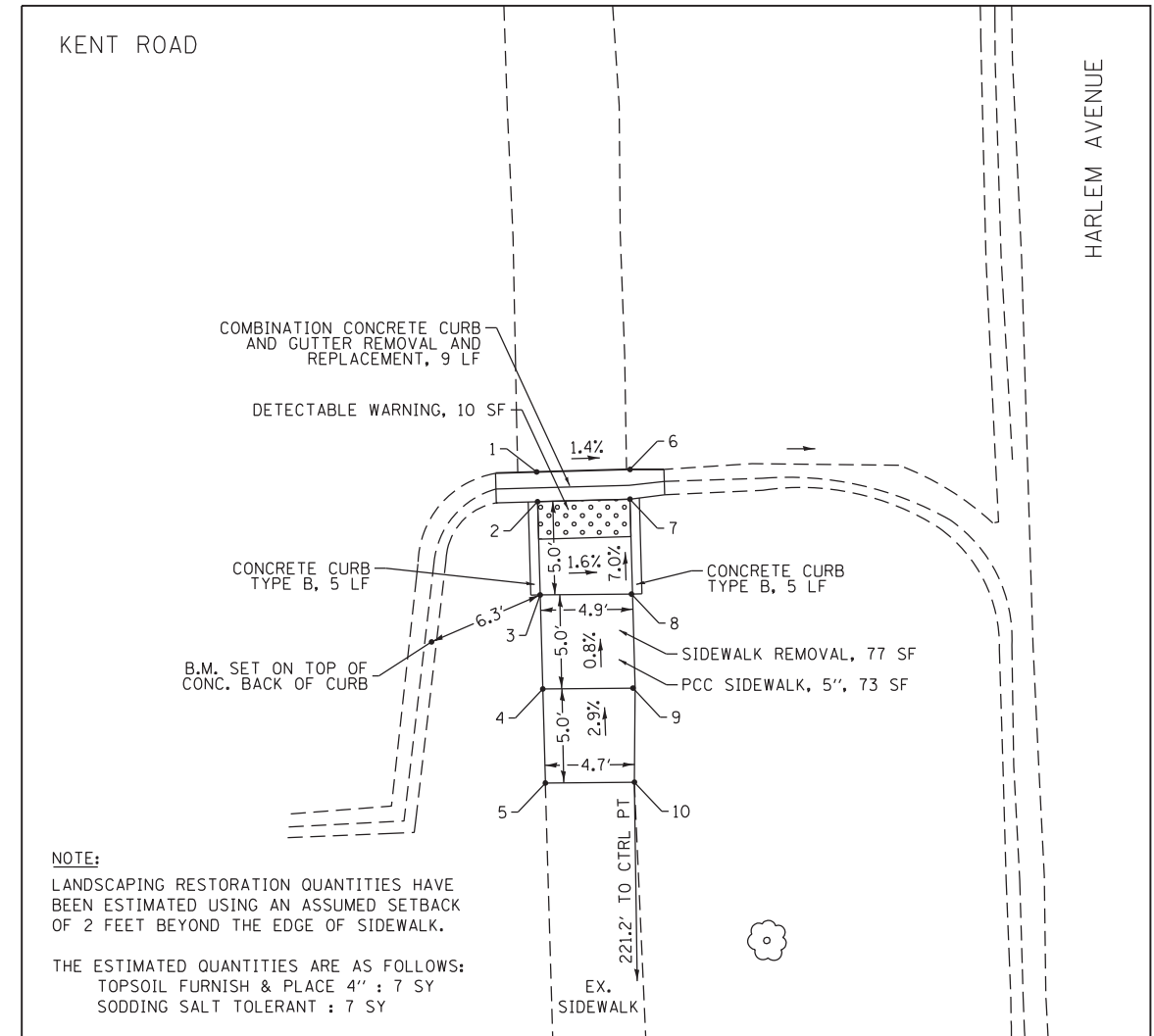


CONTROL POINTS:
CUT CROSS BENCHMARK
ELEV: 620.97 ELEV: 621.48
STA: 572+26.25 STA: 569+66.36
OS: 47.11' LT OS: 53.22' LT
N: 1885092.507 N: 1884832.551
E: 1128623.301 E: 1128625.031

NORTH CROSSING
IL ROUTE 43 (HARLEM AVENUE) AND KENT ROAD

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	570+13.98	48.54' LT	MATCH EX.
2	570+18.98	48.52' LT	620.83
3	570+23.48	48.51' LT	MATCH EX.
4	570+14.00	42.66' LT	MATCH EX.
5	570+19.00	42.64' LT	620.75
6	570+23.50	42.62' LT	MATCH EX.

KENT ROAD



NOTE:
LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.

THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
TOPSOIL FURNISH & PLACE 4" : 7 SY
SODDING SALT TOLERANT : 7 SY

CONTROL POINTS:
BENCHMARK CUT CROSS
ELEV: 621.48 ELEV: 620.52
STA: 569+66.36 STA: 567+37.39
OS: 53.22' LT OS: 46.39' LT
N: 1884832.551 N: 1884603.888
E: 1128625.031 E: 1128638.762

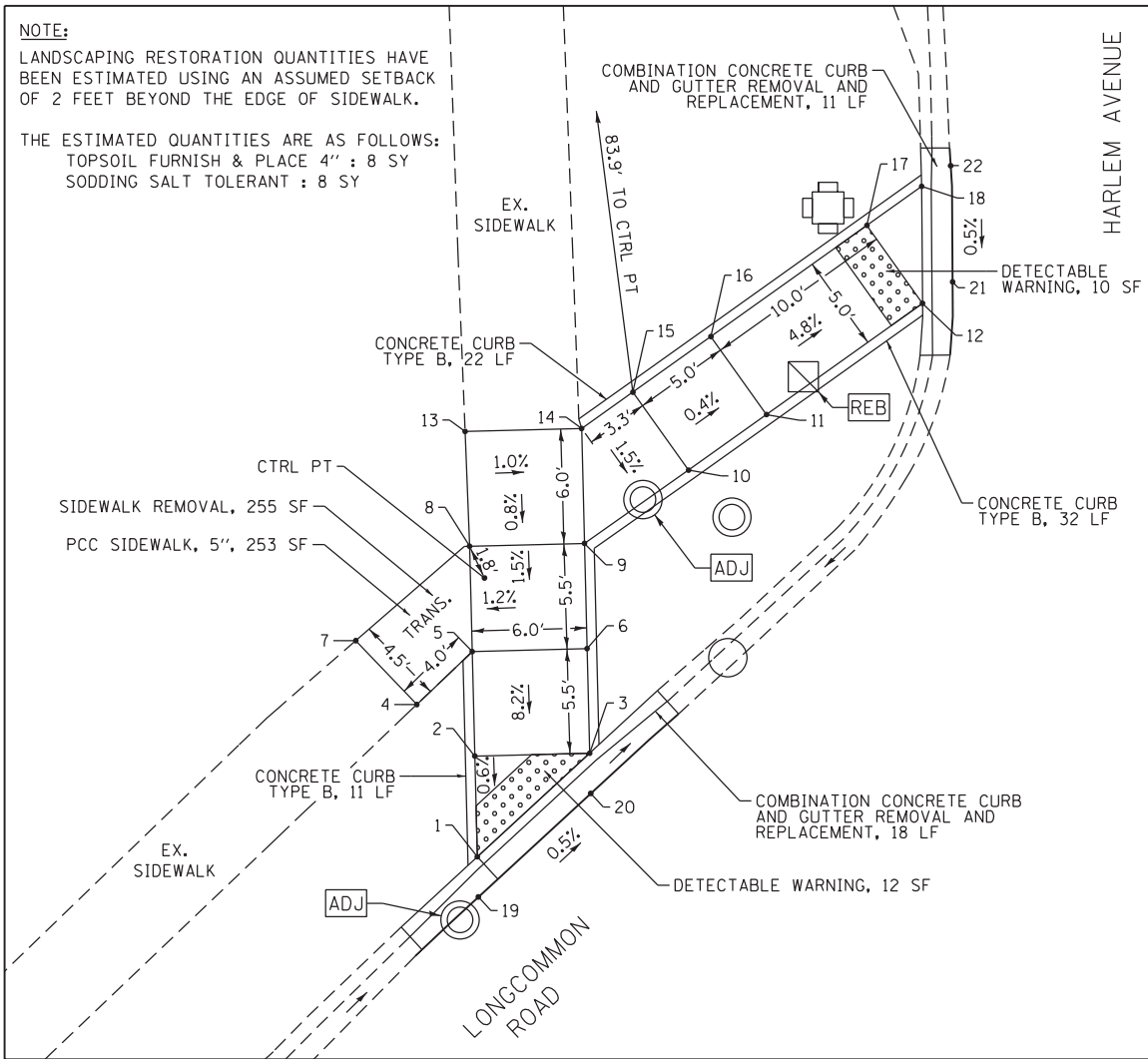
SOUTH CROSSING
IL ROUTE 43 (HARLEM AVENUE) AND KENT ROAD

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	569+75.22	47.36' LT	620.34
2	569+73.64	47.37' LT	620.33
3	569+68.68	47.38' LT	620.68
4	569+63.68	47.39' LT	620.72
5	569+58.68	47.40' LT	MATCH EX.
6	569+75.21	42.40' LT	620.27
7	569+73.63	42.45' LT	620.26
8	569+68.58	42.46' LT	620.61
9	569+63.58	42.47' LT	620.65
10	569+58.58	42.67' LT	MATCH EX.

NOTE:

LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.

THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 8 SY
 SODDING SALT TOLERANT : 8 SY

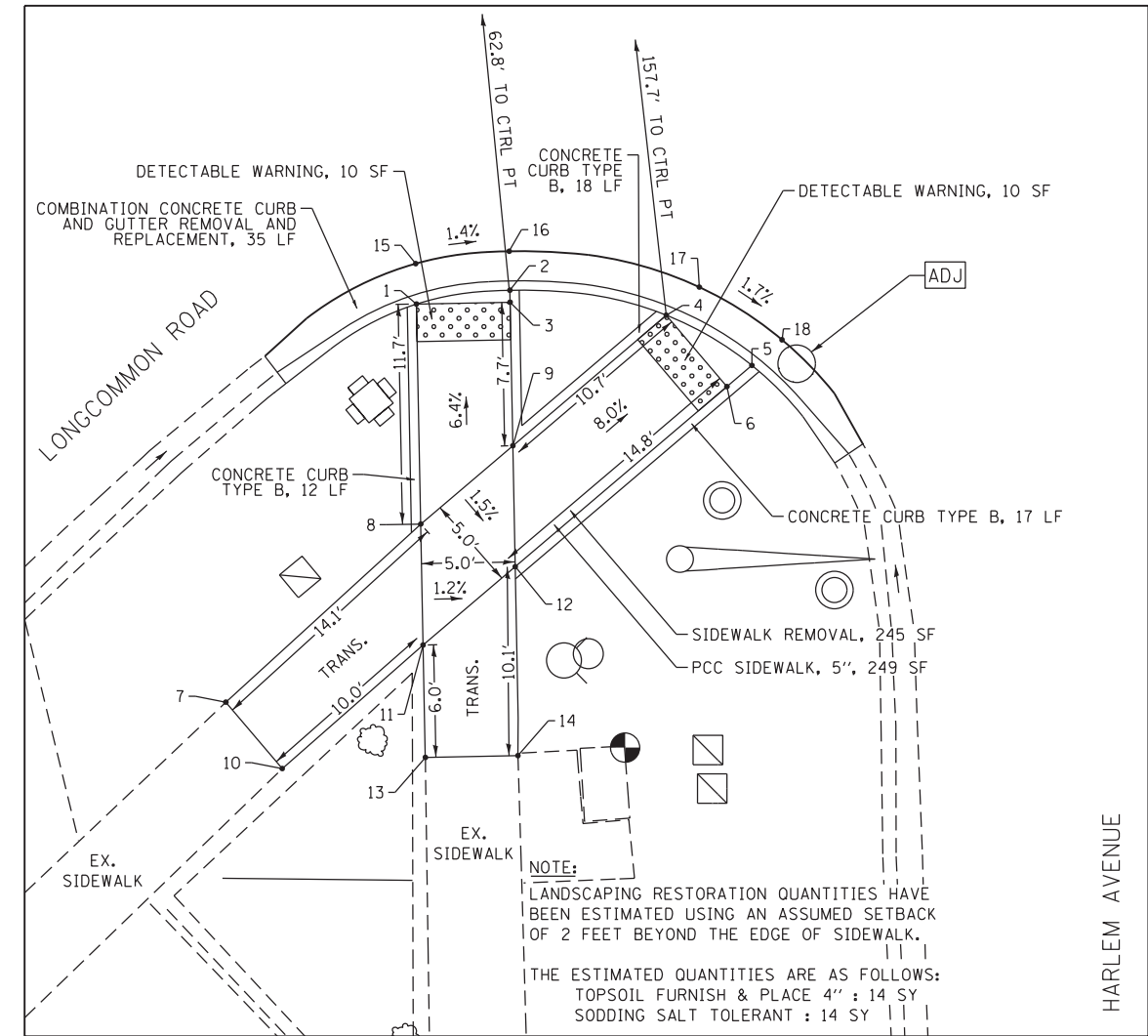


CONTROL POINTS:

CUT CROSS ELEV: 621.10 STA: 573+19.18 OS: 47.36' LT N: 1885185.382 E: 1128620.249	CUT CROSS ELEV: 620.97 STA: 572+26.25 OS: 47.11' LT N: 1885092.507 E: 1128623.301
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**NORTHWEST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND LONGCOMMON ROAD**

POINT No.:	STATION	OFFSET	ELEV.
1	572+11.72	47.92' LT	620.43
2	572+16.98	47.90' LT	620.46
3	572+16.95	41.90' LT	620.39
4	572+19.76	50.84' LT	MATCH EX.
5	572+22.43	47.87' LT	620.91
6	572+22.40	41.87' LT	620.84
7	572+23.19	53.92' LT	MATCH EX.
8	572+27.93	47.84' LT	MATCH EX.
9	572+27.90	41.84' LT	620.92
10	572+31.56	36.29' LT	620.91
11	572+34.34	32.14' LT	620.89
12	572+39.90	23.82' LT	620.41
13	572+33.93	47.89' LT	MATCH EX.
14	572+33.90	41.81' LT	MATCH EX.
15	572+35.72	39.07' LT	620.98
16	572+38.50	34.91' LT	620.96
17	572+44.05	26.60' LT	620.48
18	572+46.01	23.68' LT	620.44
19	572+09.61	47.93' LT	620.44
20	572+14.84	41.91' LT	620.40
21	572+40.97	22.22' LT	620.42
22	572+47.04	22.14' LT	620.45

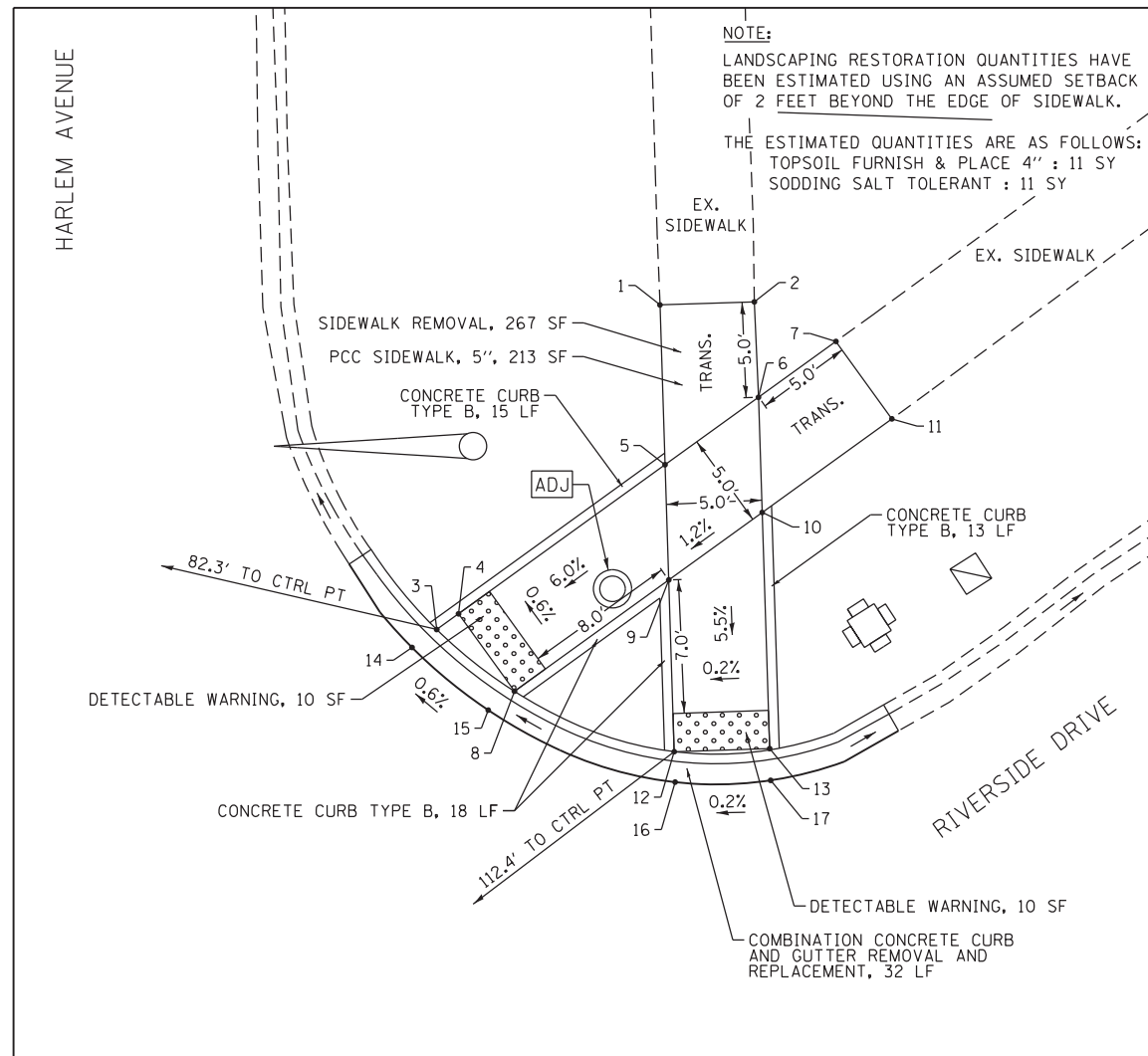


CONTROL POINTS:

CUT CROSS ELEV: 621.10 STA: 573+19.18 OS: 47.36' LT N: 1885185.382 E: 1128620.249	CUT CROSS ELEV: 620.97 STA: 572+26.25 OS: 47.11' LT N: 1885092.507 E: 1128623.301
--	--

**SOUTHWEST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND LONGCOMMON ROAD**

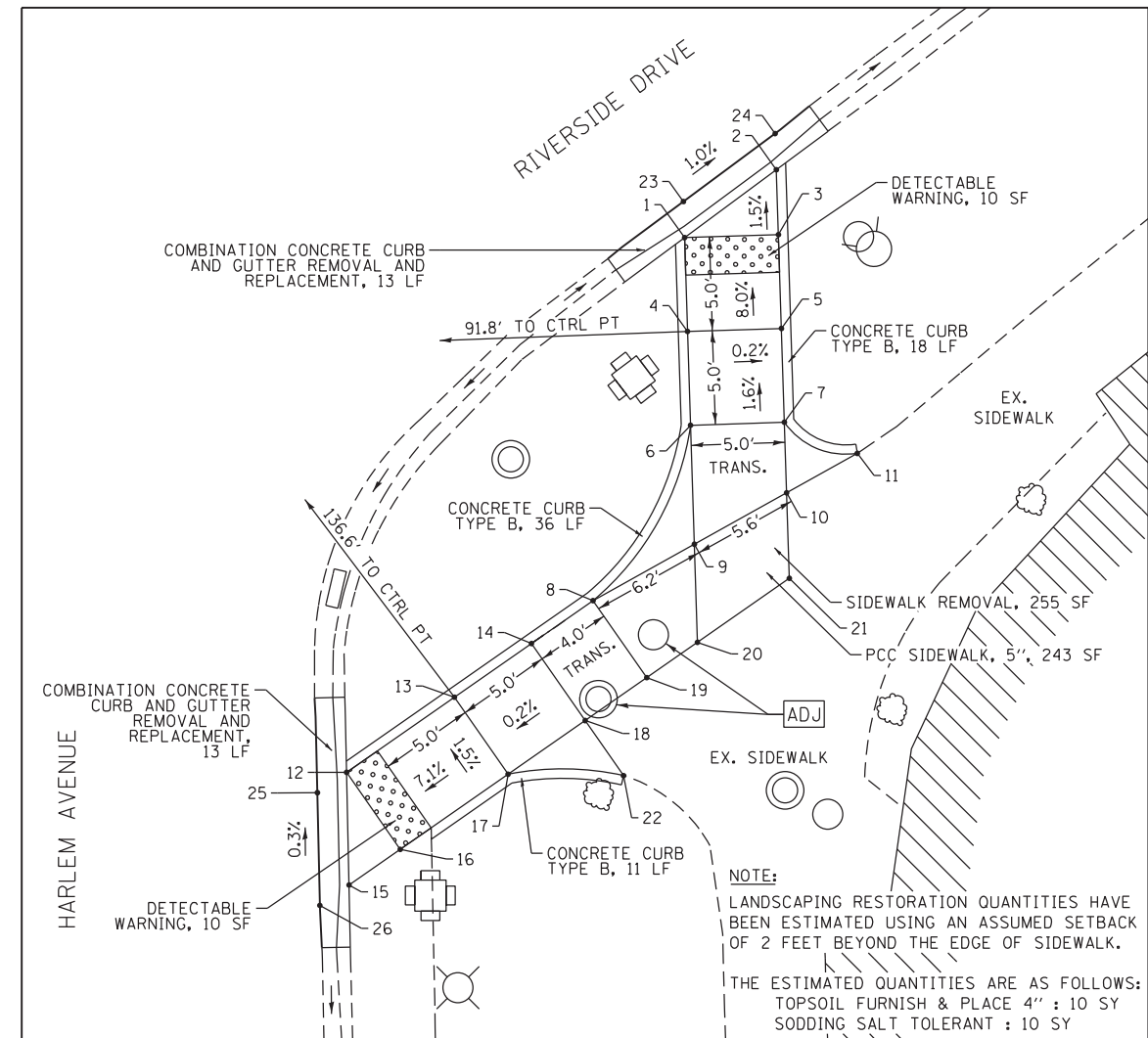
POINT No.:	STATION	OFFSET	ELEV.
1	571+62.99	47.82' LT	620.28
2	571+63.57	42.81' LT	620.21
3	571+62.93	42.82' LT	620.22
4	571+61.99	34.51' LT	619.91
5	571+59.17	30.02' LT	619.82
6	571+58.08	31.39' LT	619.84
7	571+42.05	58.64' LT	MATCH EX.
8	571+51.24	47.95' LT	620.84
9	571+55.27	42.90' LT	620.77
10	571+38.42	55.73' LT	MATCH EX.
11	571+44.78	48.02' LT	620.91
12	571+48.82	42.97' LT	620.84
13	571+38.78	48.07' LT	MATCH EX.
14	571+38.73	43.13' LT	MATCH EX.
15	571+65.14	47.80' LT	620.31
16	571+65.65	42.79' LT	620.24
17	571+63.43	32.70' LT	619.94
18	571+60.49	28.37' LT	619.85



CONTROL POINTS:
 CUT CROSS ELEV: 621.10 STA: 573+19.18 OS: 47.36' LT N: 1885185.382 E: 1128620.249
 CUT CROSS ELEV: 620.97 STA: 572+26.25 OS: 47.11' LT N: 1885092.507 E: 1128623.301

NORTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND RIVERSIDE DRIVE

POINT No.:	STATION	OFFSET	ELEV.
1	573+14.85	44.38' RT	MATCH EX.
2	573+14.86	49.33' RT	MATCH EX.
3	572+98.22	32.20' RT	620.03
4	572+99.00	33.36' RT	620.05
5	573+06.48	44.40' RT	620.65
6	573+09.86	49.39' RT	620.72
7	573+12.67	53.53' RT	MATCH EX.
8	572+94.86	36.16' RT	620.06
9	573+00.45	44.41' RT	620.66
10	573+03.83	49.40' RT	620.73
11	573+08.52	56.34' RT	MATCH EX.
12	572+91.45	44.42' RT	620.12
13	572+91.39	49.42' RT	620.13
14	572+97.32	30.87' RT	620.04
15	572+93.91	34.76' RT	620.07
16	572+89.85	44.43' RT	620.13
17	572+89.79	49.43' RT	620.14



CONTROL POINTS:
 CUT CROSS ELEV: 621.10 STA: 573+19.18 OS: 47.36' LT N: 1885185.382 E: 1128620.249
 CUT CROSS ELEV: 620.97 STA: 572+26.25 OS: 47.11' LT N: 1885092.507 E: 1128623.301

SOUTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND RIVERSIDE DRIVE

POINT No.:	STATION	OFFSET	ELEV.
1	572+31.90	44.68' RT	620.04
2	572+35.37	49.67' RT	619.98
3	572+31.91	49.68' RT	620.03
4	572+26.90	44.68' RT	620.44
5	572+26.91	49.68' RT	620.43
6	572+21.90	44.69' RT	620.52
7	572+21.91	49.69' RT	620.51
8	572+12.72	39.23' RT	620.63
9	572+15.56	44.70' RT	MATCH EX.
10	572+18.15	49.70' RT	MATCH EX.
11	572+20.14	53.52' RT	MATCH EX.
12	572+03.97	25.83' RT	610.17
13	572+07.80	31.69' RT	610.66

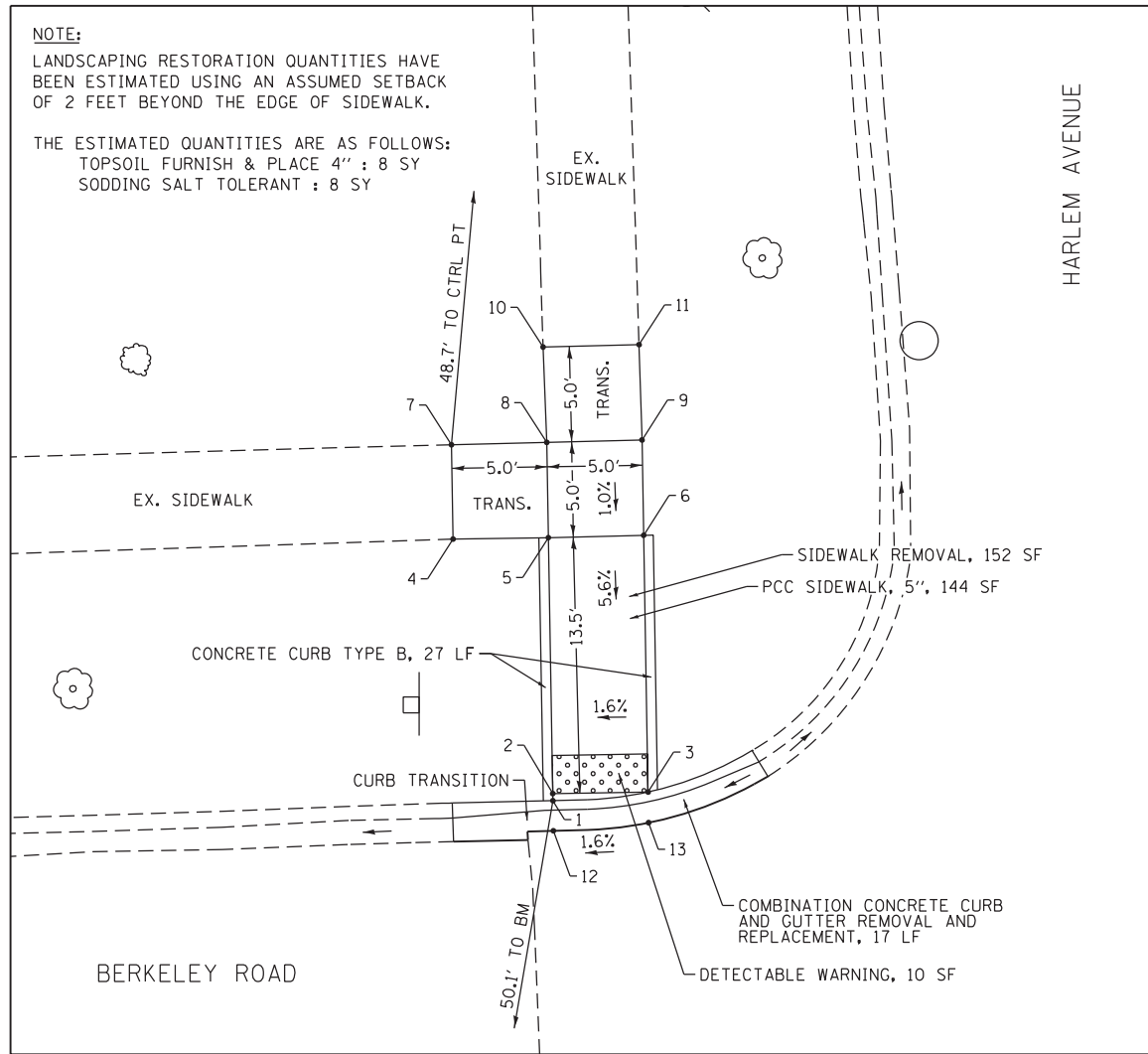
SOUTHEAST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND RIVERSIDE DRIVE

POINT No.:	STATION	OFFSET	ELEV.
14	572+10.53	35.88' RT	610.67
15	571+97.97	25.78' RT	620.19
16	571+99.79	28.56' RT	620.24
17	572+03.61	34.42' RT	620.73
18	572+06.35	38.61' RT	MATCH EX.
19	572+08.53	41.96' RT	MATCH EX.
20	572+10.33	44.71' RT	MATCH EX.
21	572+13.59	49.71' RT	MATCH EX.
22	572+03.35	40.56' RT	MATCH EX.
23	572+33.82	44.67' RT	620.05
24	572+37.29	49.67' RT	619.99
25	572+02.93	24.24' RT	610.18
26	571+96.93	24.19' RT	620.20

NOTE:

LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.

THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 8 SY
 SODDING SALT TOLERANT : 8 SY

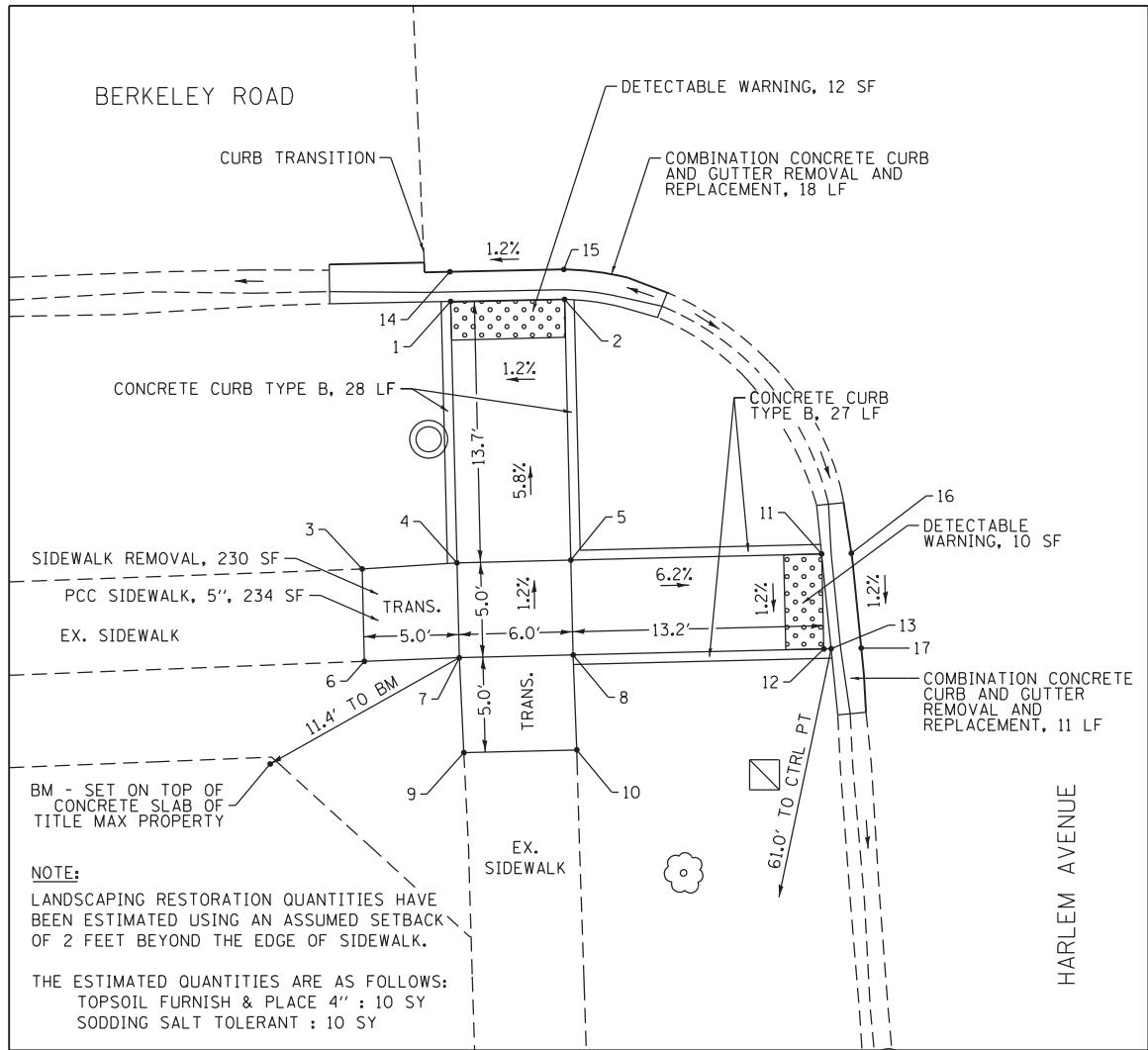


CONTROL POINTS:

CUT CROSS ELEV: 620.64 STA: 576+07.36 OS: 47.78' LT N: 1885472.849 E: 1128610.696	BENCHMARK ELEV: 621.18 STA: 574+90.55 OS: 59.12' LT N: 1885356.322 E: 1128603.329
--	--

**NORTHWEST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND BERKELEY ROAD**

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	575+39.64	49.25' LT	619.64
2	575+40.01	49.24' LT	619.64
3	575+39.94	44.24' LT	619.72
4	575+53.51	54.07' LT	MATCH EX.
5	575+53.44	49.07' LT	620.39
6	575+53.41	44.07' LT	620.42
7	575+58.46	54.01' LT	MATCH EX.
8	575+58.44	49.01' LT	620.44
9	575+58.41	44.01' LT	620.47
10	575+63.44	49.05' LT	MATCH EX.
11	575+63.41	44.01' LT	MATCH EX.
12	575+38.06	49.27' LT	619.65
13	575+38.33	44.27' LT	619.73



CONTROL POINTS:

BENCHMARK ELEV: 621.18 STA: 574+90.55 OS: 59.12' LT N: 1885356.322 E: 1128603.329	CUT CROSS ELEV: 620.76 STA: 574+36.45 OS: 43.78' LT N: 1885302.714 E: 1128620.294
--	--

**SOUTHWEST CORNER
 IL ROUTE 43 (HARLEM AVENUE) AND BERKELEY ROAD**

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	575+14.61	48.88' LT	619.87
2	575+14.54	42.88' LT	619.94
3	575+00.67	53.97' LT	MATCH EX.
4	575+00.84	48.96' LT	620.67
5	575+00.81	42.97' LT	620.74
6	574+95.80	53.99' LT	MATCH EX.
7	574+95.84	48.99' LT	620.73
8	574+95.81	42.99' LT	620.80
9	574+90.87	48.90' LT	MATCH EX.
10	574+90.80	42.95' LT	MATCH EX.
11	575+00.73	29.76' LT	619.92
12	574+95.73	29.79' LT	619.86
13	574+95.72	29.41' LT	619.86
14	575+16.17	48.87' LT	619.88
15	575+16.12	42.87' LT	619.95
16	575+00.72	28.19' LT	619.93
17	574+95.71	27.82' LT	619.87



USER NAME = WTeng	DESIGNED -	REVISED -
PLOT SCALE = 10.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 10/17/2016	CHECKED - MTC	REVISED -
	DATE - 10/17/2016	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**IL ROUTE 43 (HARLEM AVENUE)
 ADA RAMP DETAILS**

SCALE: 1" = 5' SHEET 38 OF 38 SHEETS STA. TO STA.

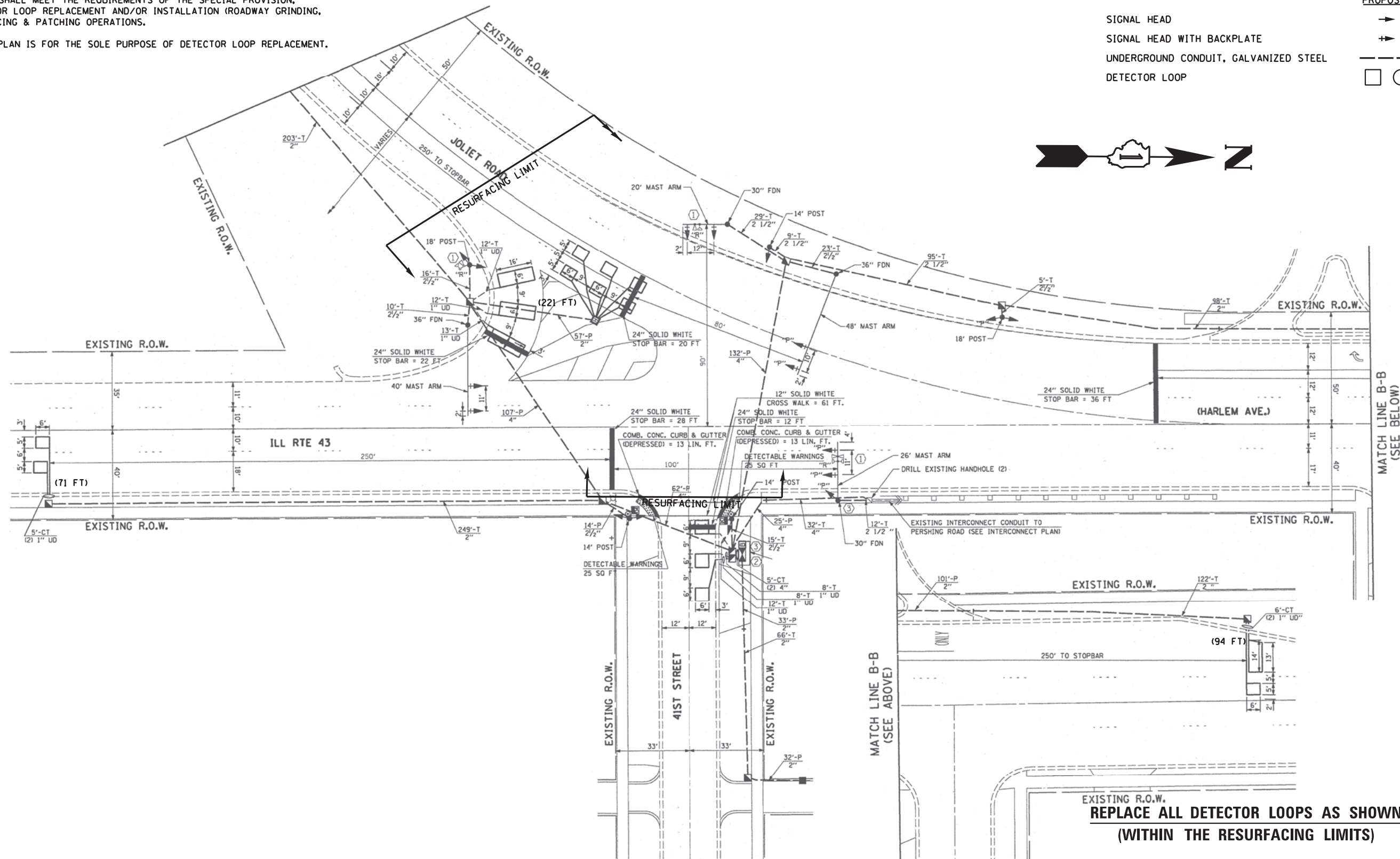
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	(2424 & 3138) RS-3	COOK	75	54
CONTRACT NO. 62C19				
ILLINOIS FED. AID PROJECT				

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

TRAFFIC SIGNAL LEGEND:

	PROPOSED	EXISTING
SIGNAL HEAD	▶	◀
SIGNAL HEAD WITH BACKPLATE	▶◀	◀▶
UNDERGROUND CONDUIT, GALVANIZED STEEL	—	—
DETECTOR LOOP	□ ○	◻ ◯



CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	386	FOOT

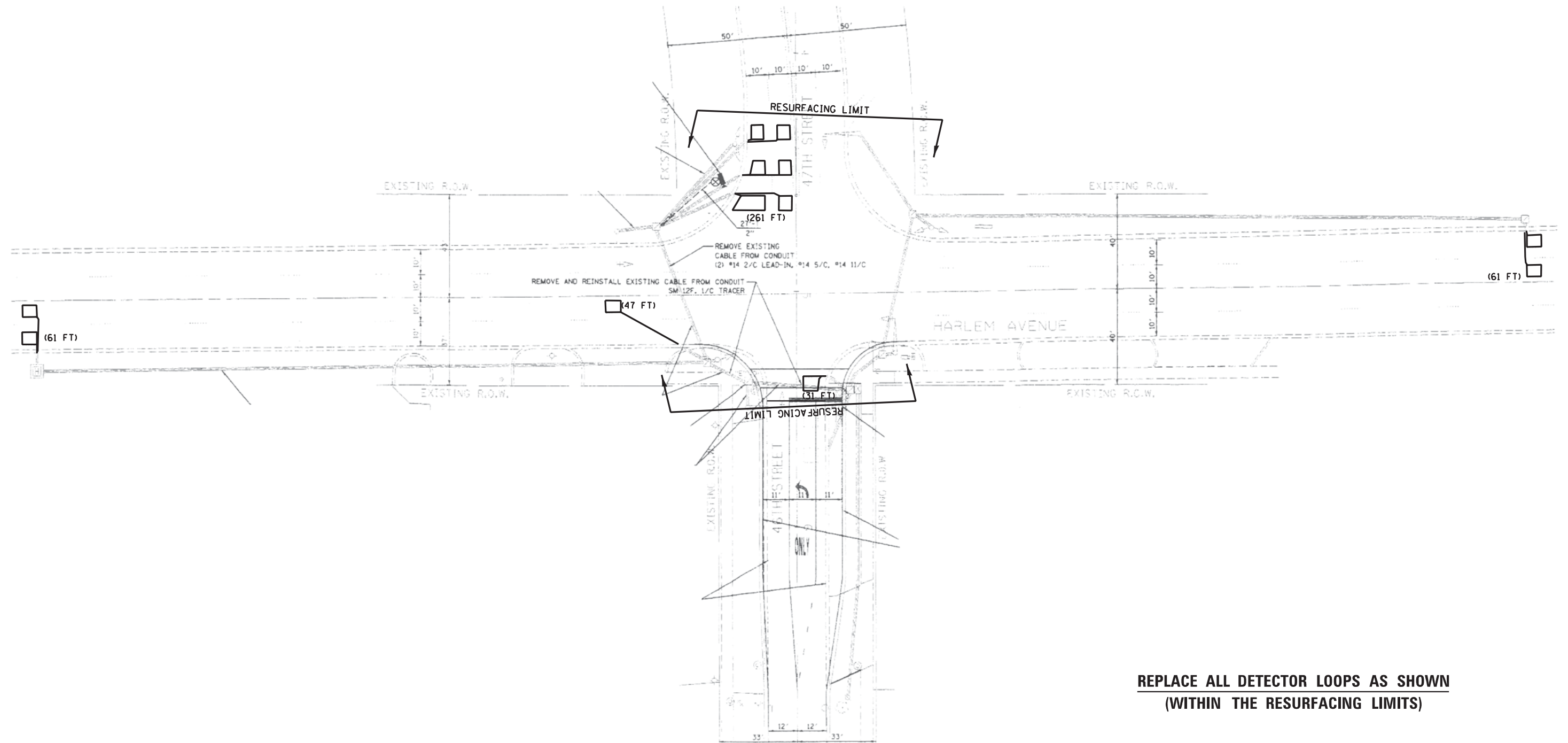
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S:\WP\Design\JC\Detector Loop Details\2016\62C19\DetectorLoopReplacementSheet.Blan	DRAWN 6.dgn	CHECKED - ---	REVISED - ---			398	(2424 & 3138) RS-3	COOK	75	55	
Default	PLOT SCALE = 40.0000' / in.	DATE - ---	REVISED - ---			CONTRACT NO. 62C19					
	PLOT DATE = 8/16/2016					ILLINOIS FED. AID PROJECT					

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

TRAFFIC SIGNAL LEGEND:

	PROPOSED	EXISTING
SIGNAL HEAD	→	↖
SIGNAL HEAD WITH BACKPLATE	→+	↖+
UNDERGROUND CONDUIT, GALVANIZED STEEL	— — —	====
DETECTOR LOOP	□ ○	□ ○



**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	261	FOOT

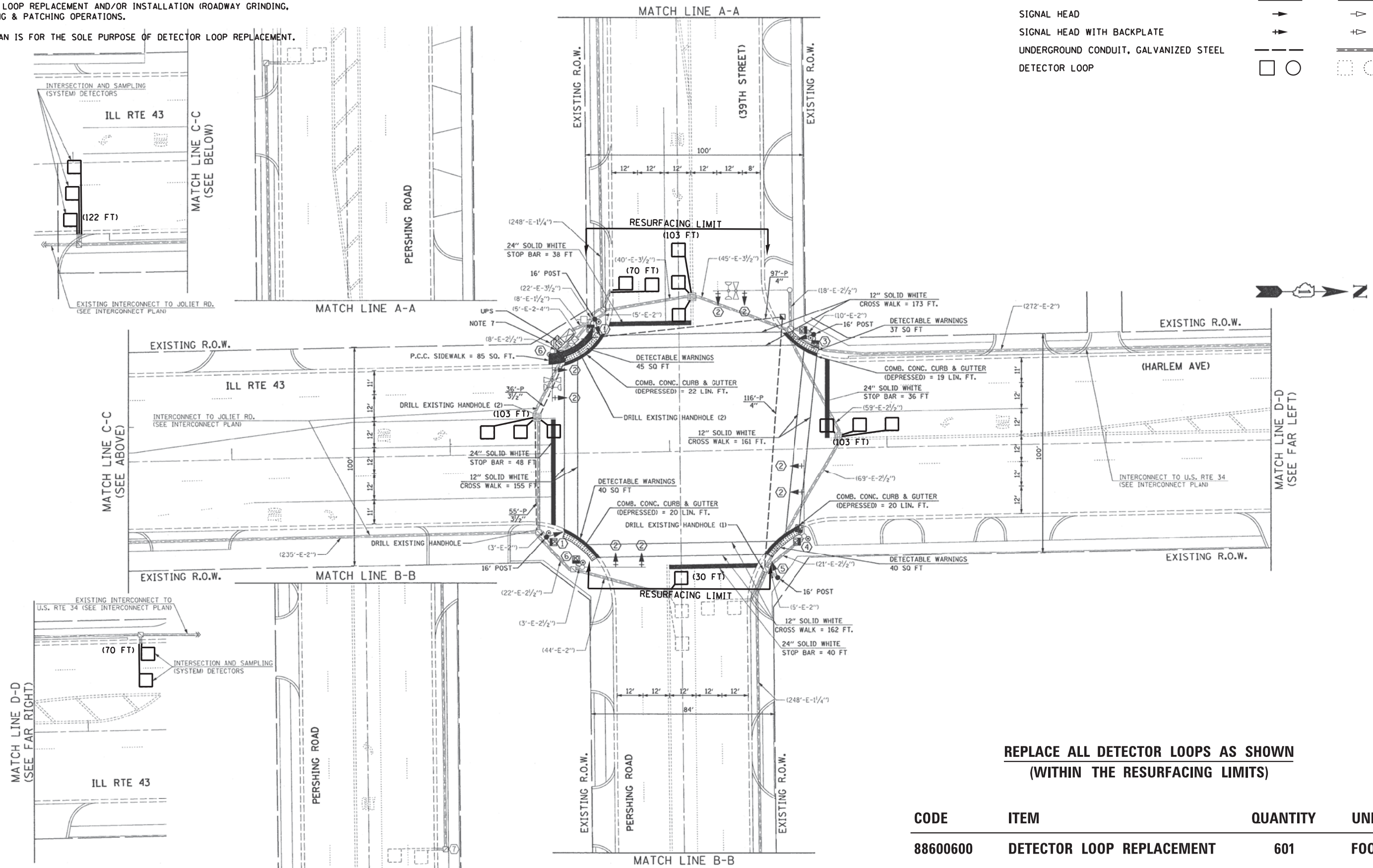
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S:\WP\Design\JC\Detector Loop Details\2016\62C19\DetectorLoopReplacementSheet.Blan	DRAWN 6.dgn	CHECKED - ---	REVISED - ---			348	(2424 & 3138) RS-3	COOK	75	56	
Default	PLOT SCALE = 40.0000' / in.	DATE - ---	REVISED - ---			CONTRACT NO. 62C19			ILLINOIS FED. AID PROJECT		
	PLOT DATE = 8/16/2016					SCALE: _____	SHEET _____	OF _____	SHEETS	STA. _____	TO STA. _____

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

TRAFFIC SIGNAL LEGEND:

	PROPOSED	EXISTING
SIGNAL HEAD	▲	△
SIGNAL HEAD WITH BACKPLATE	▲	△
UNDERGROUND CONDUIT, GALVANIZED STEEL	—	—
DETECTOR LOOP	□ ○	□ ○



**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	601	FOOT

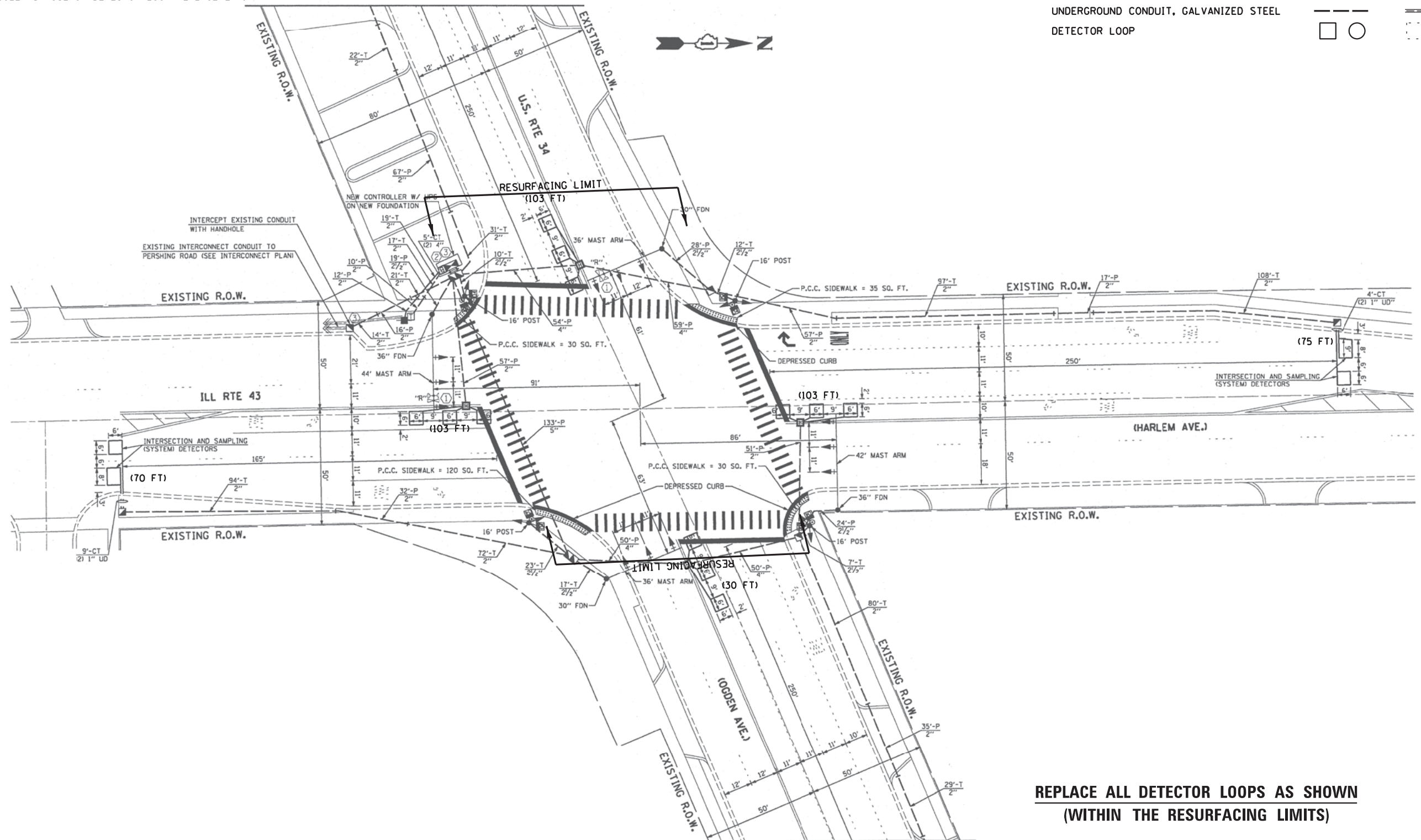
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S:\WP\Design\JC\Detector Loop Details\2016\62C19\DetectorLoopReplacementSheet.Blonc.DRAWN6.dgn-		REVISD -	REVISD -			398	(2424 & 3138) RS-3	COOK	75	57
PLOT SCALE = 40.0000' / in.		CHECKED -	REVISD -			CONTRACT NO. 62C19				
Default		DATE -	REVISD -			ILLINOIS FED. AID PROJECT				

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

TRAFFIC SIGNAL LEGEND:

	PROPOSED	EXISTING
SIGNAL HEAD	➔	➤
SIGNAL HEAD WITH BACKPLATE	➔➤	➤➤
UNDERGROUND CONDUIT, GALVANIZED STEEL	—	—
DETECTOR LOOP	□ ○	□ ○



**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	484	FOOT

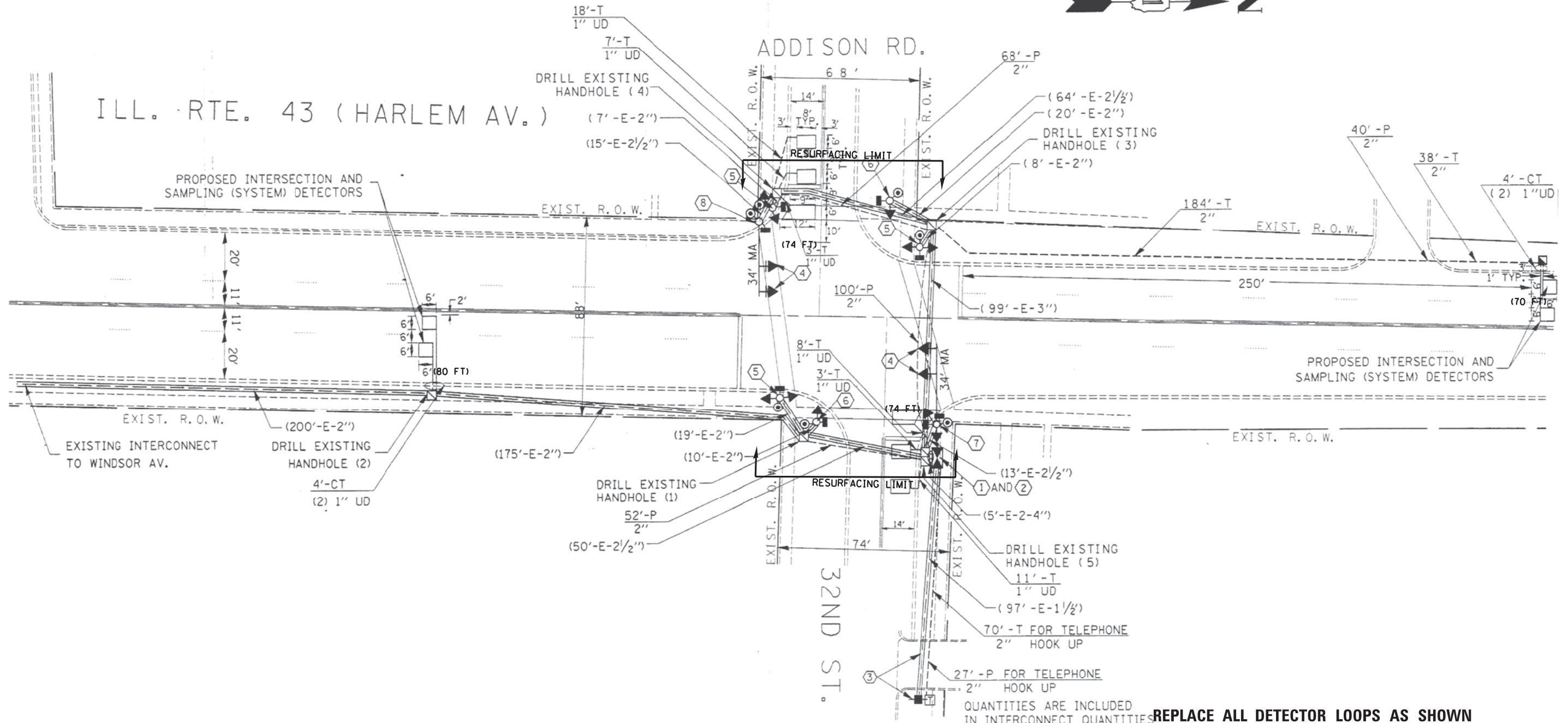
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S:\WP\Design\JC\Detector Loop Details\2016\62C19\DetectorLoopReplacementSheet.Blan	DRAWN 6.dgn	CHECKED - ---	REVISED - ---			SCALE: _____	SHEET _____ OF _____ SHEETS	STA. _____ TO STA. _____	CONTRACT NO. 62C19			
Default	PLOT SCALE = 40.0000' / in.	DATE - ---	REVISED - ---			ILLINOIS FED. AID PROJECT						
	PLOT DATE = 8/16/2016											

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

TRAFFIC SIGNAL LEGEND:

	PROPOSED	EXISTING
SIGNAL HEAD	➔	➤
SIGNAL HEAD WITH BACKPLATE	➔➤	➤➤
UNDERGROUND CONDUIT, GALVANIZED STEEL	— — —	≡ ≡ ≡
DETECTOR LOOP	□ ○	□ ○



QUANTITIES ARE INCLUDED IN INTERCONNECT QUANTITIES **REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	298	FOOT

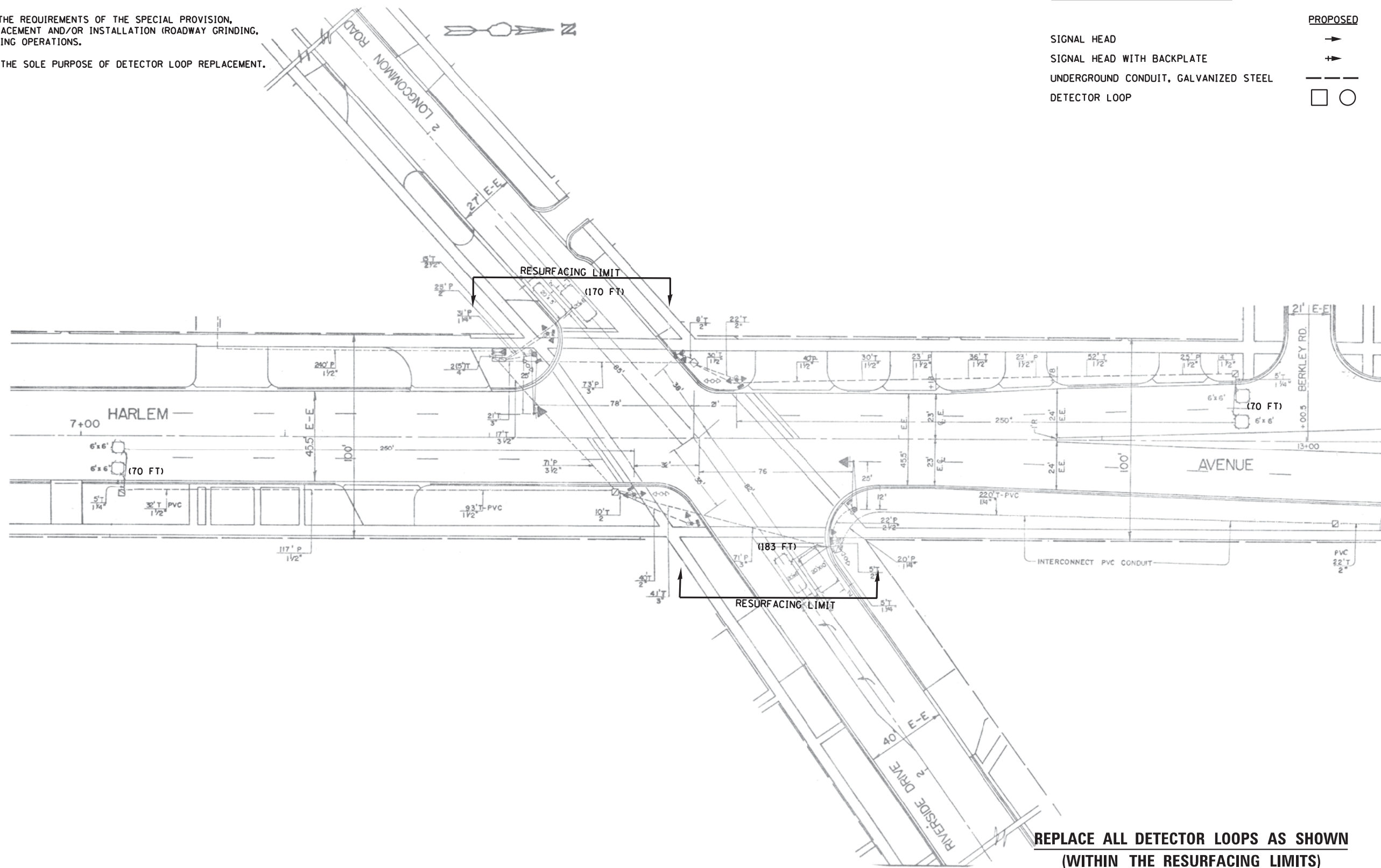
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Default	PLOT SCALE = 40.0000' / in.	CHECKED - ---	REVISED - ---			SCALE: _____ SHEET _____ OF _____ SHEETS STA. _____ TO STA. _____	CONTRACT NO. 62C19				
	PLOT DATE = 8/16/2016	DATE - ---	REVISED - ---			ILLINOIS FED. AID PROJECT					

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

TRAFFIC SIGNAL LEGEND:

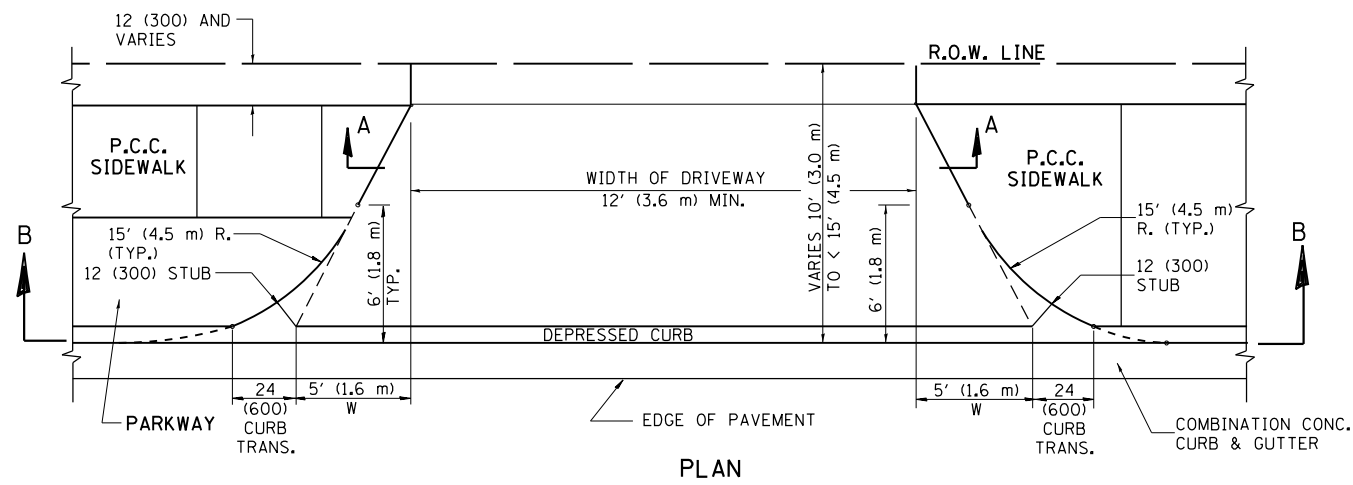
	PROPOSED	EXISTING
SIGNAL HEAD	➔	⚡
SIGNAL HEAD WITH BACKPLATE	➔	⚡
UNDERGROUND CONDUIT, GALVANIZED STEEL	— — —	— — —
DETECTOR LOOP	□ ○	□ ○



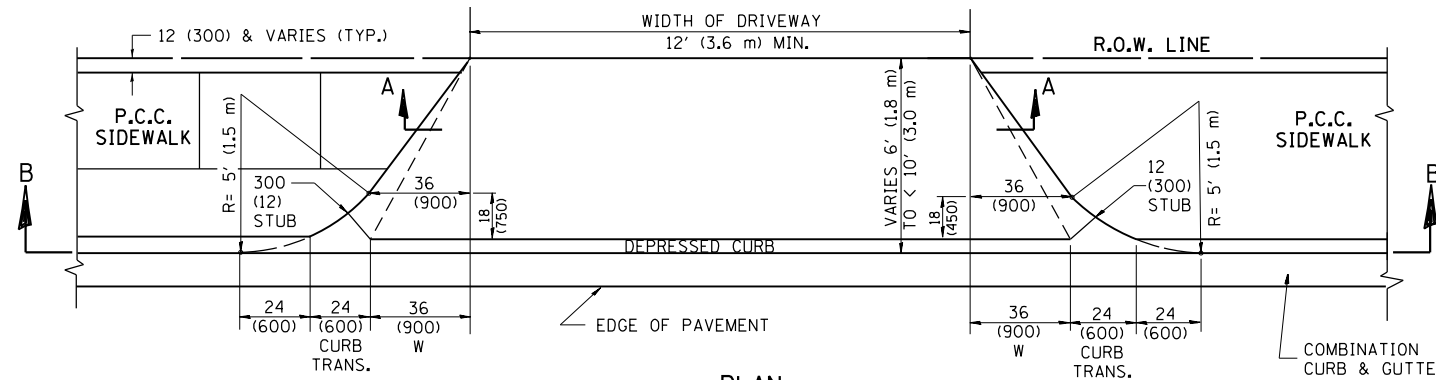
**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	493	FOOT

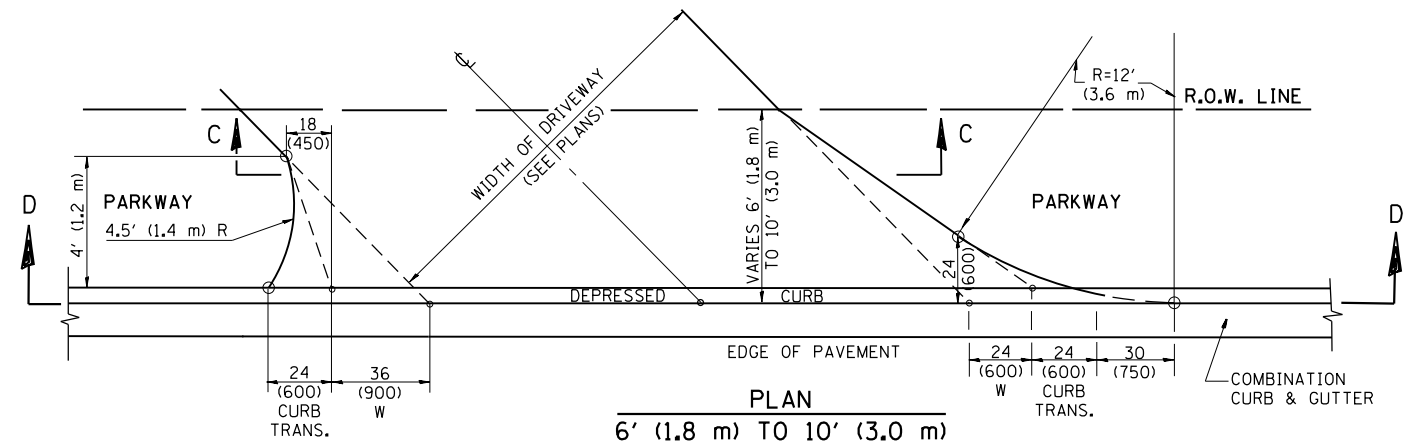
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S:\WP\Design\JC\Detector Loop Details\2016\62C19\DetectorLoopReplacementSheet.Blank.DWG	DESIGNED - ---	REVISED - ---	348			(2424 & 3138) RS-3	COOK	75	60	
PLOT SCALE = 40.0268 ' / in.	CHECKED - ---	REVISED - ---	CONTRACT NO. 62C19							
Default	DATE - ---	REVISED - ---	ILLINOIS FED. AID PROJECT							



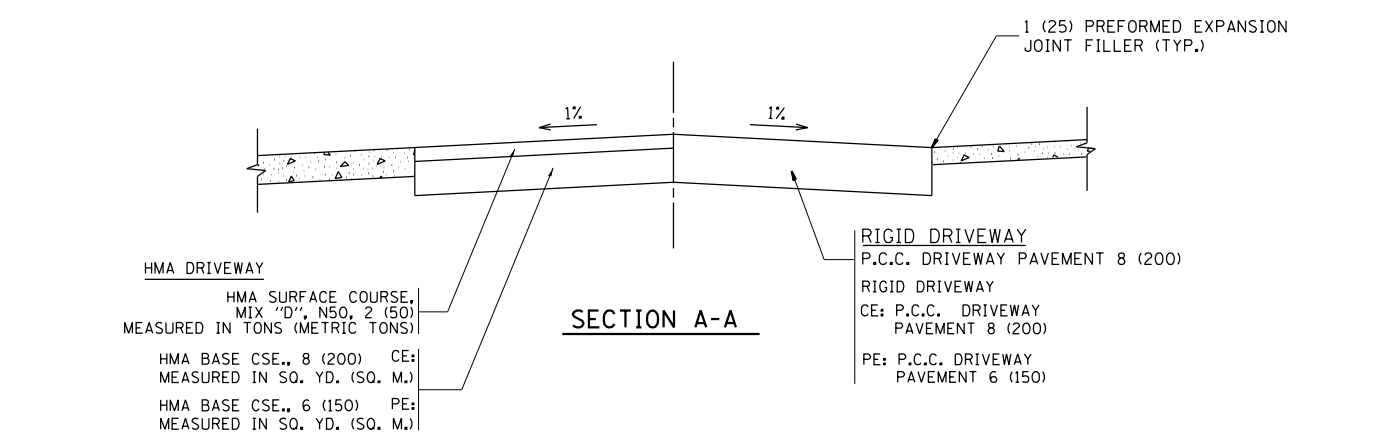
PLAN
10' (3.0 m) TO < 15' (4.5 m)



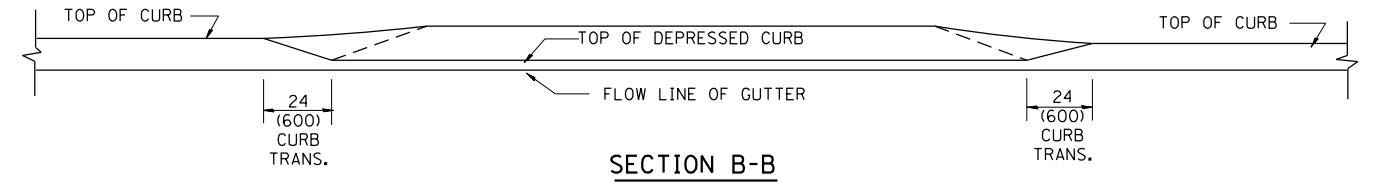
PLAN
6' (1.8 m) TO < 10' (3.0 m)



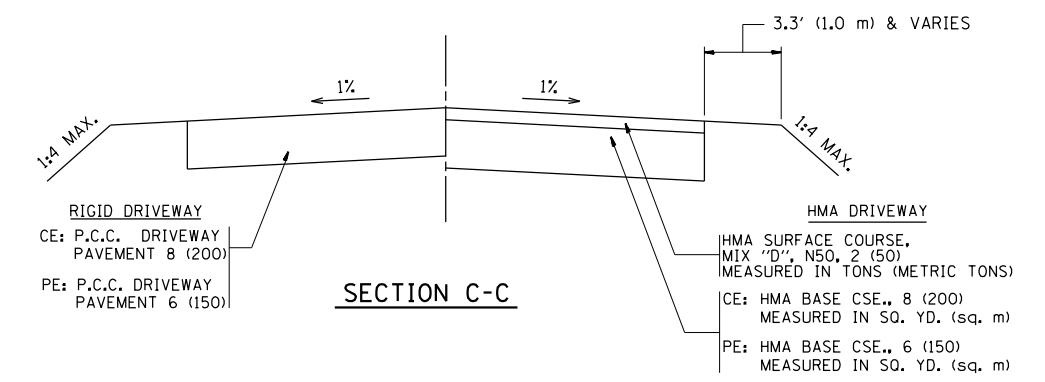
PLAN
6' (1.8 m) TO 10' (3.0 m)



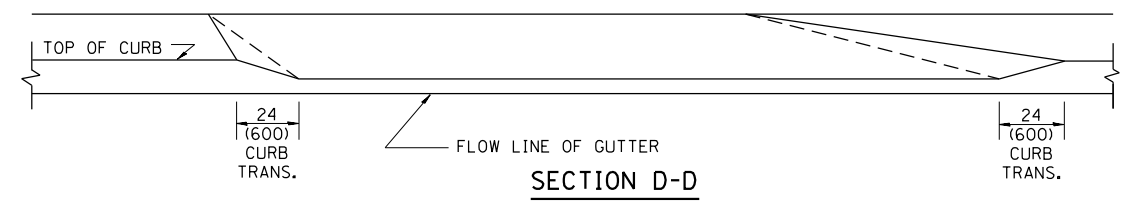
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

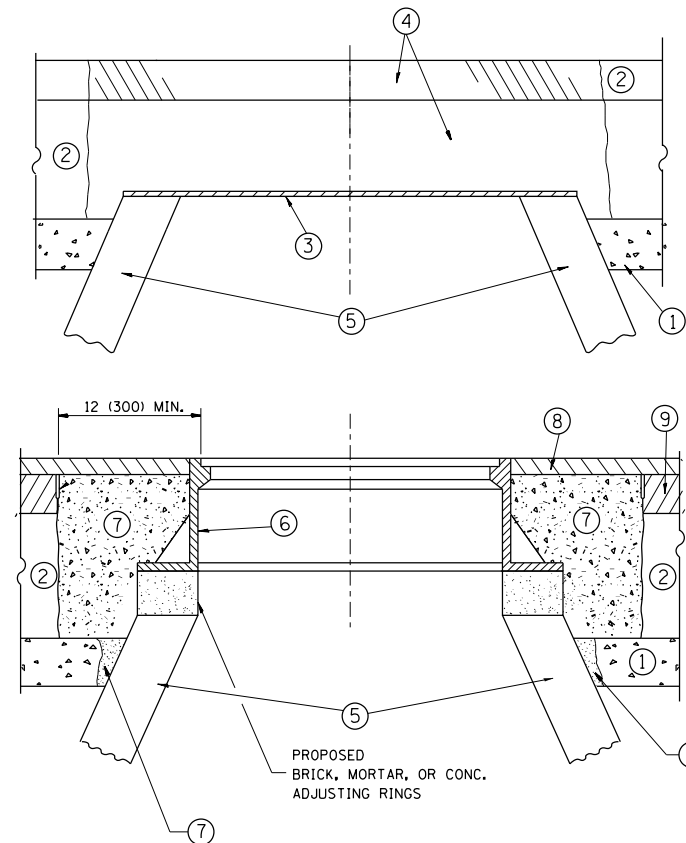
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

FILE NAME =	USER NAME = currywaw	DESIGNED - R. SHAH	REVISED - M. GOMEZ 04-06-01
p:\1\084EBIDINTEG.illinois.gov\PI\DOT\Documents\IDOT Offices\District 1\Projects\DI296\Drawings\Design\Diststd.dgn		CHECKED -	REVISED - P. LaFLEUR 04-15-03
		DATE - 11-06-95	REVISED - R. BORO 01-01-07
			REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS			
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	(2424&3138)R5-3	COOK	75	61
BD400-02 (BD-02)		CONTRACT NO. 62C19		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

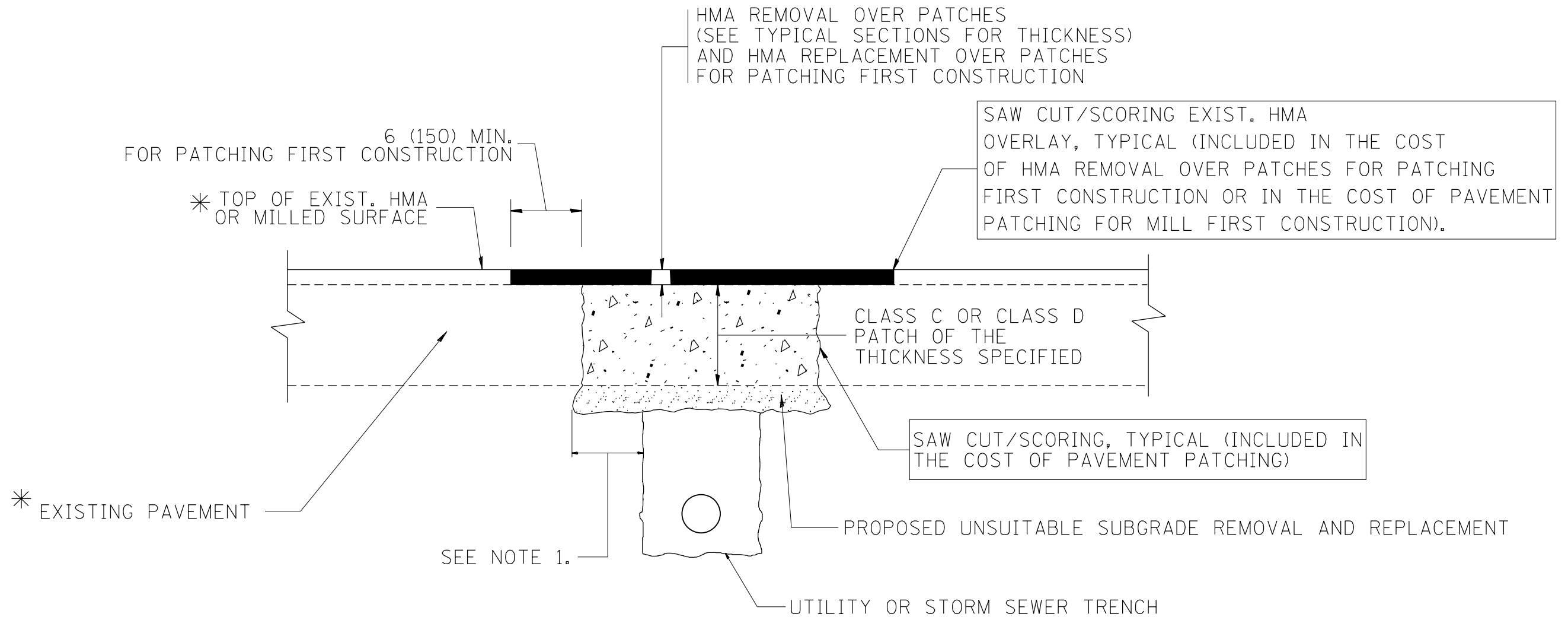
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = curryaw	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
pw:\IL\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI296\BROWNS\Meta\Design\Diststd.dgn			REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 10/28/2016	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	(2424&3138)RS-3	COOK	75	62
BD600-03 (BD-8)		CONTRACT NO. 62C19		
<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</small>				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = curryaw	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98
pw\11084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI296\BROWNS\Design\Diststd.dgn		CHECKED -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0000' / 1in.	DATE - 10-25-94	REVISED - R. BORO 09-04-07
	PLOT DATE = 10/28/2016		REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	(2424&3138)R5-3	COOK	75	63
BD400-04 (BD-22)		CONTRACT NO. 62C19		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

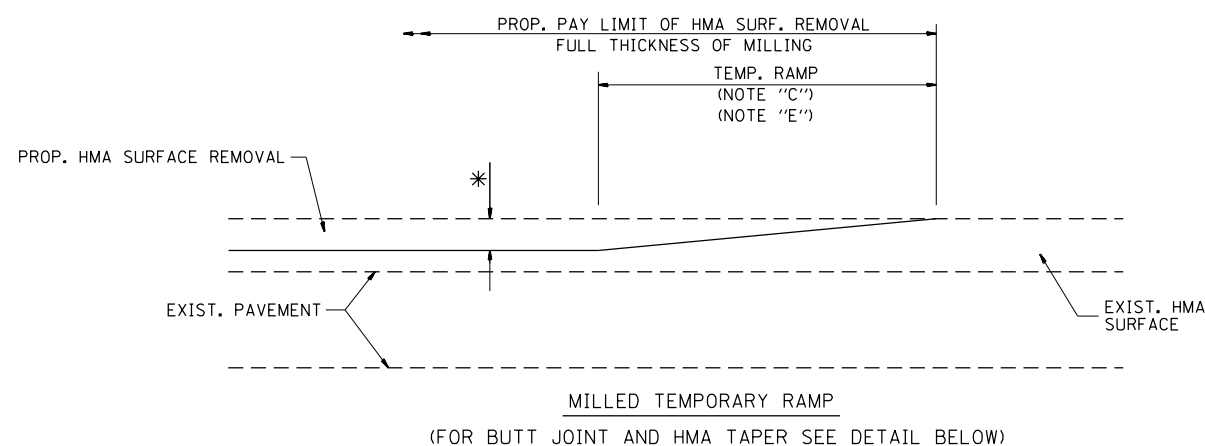
⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

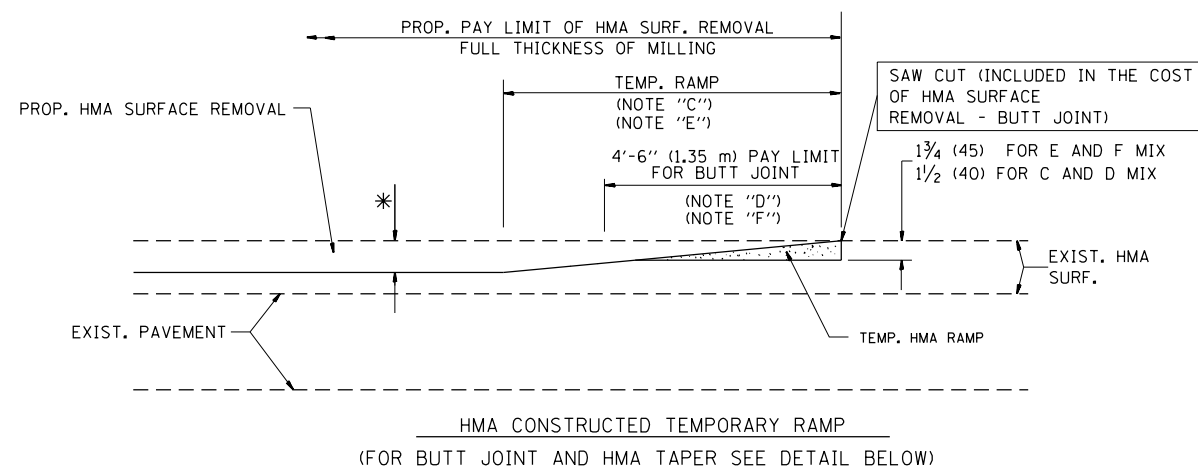
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = curryw	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI296\BROWNS\Meta\Design\Diststd.dgn		REVISED - A. ABBAS 03-21-97	REVISED - M. GOMEZ 01-22-01					348	(2424&3138)RS-3	COOK	75	64
PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 12-15-09			BD600-06 (BD-24)			CONTRACT NO. 62C19				
PLOT DATE = 10/28/2016	DATE - 03-11-94				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

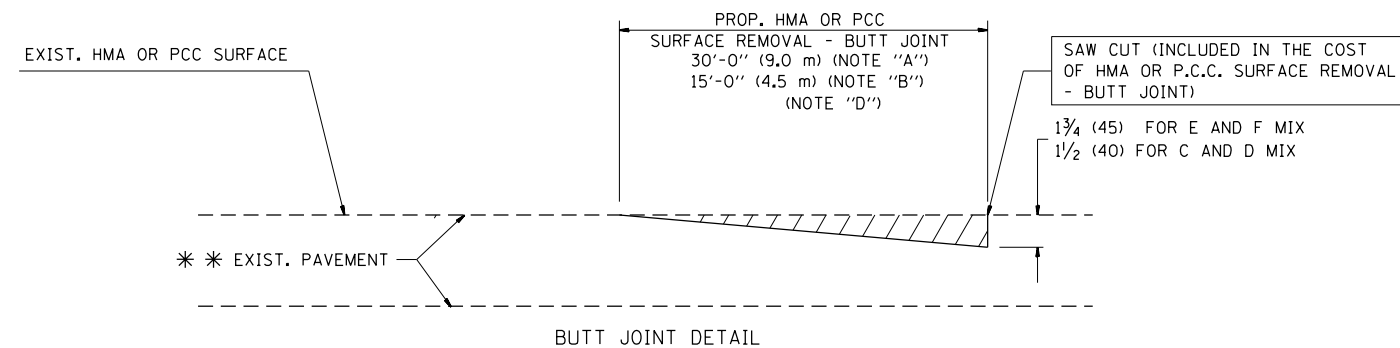


OPTION 1

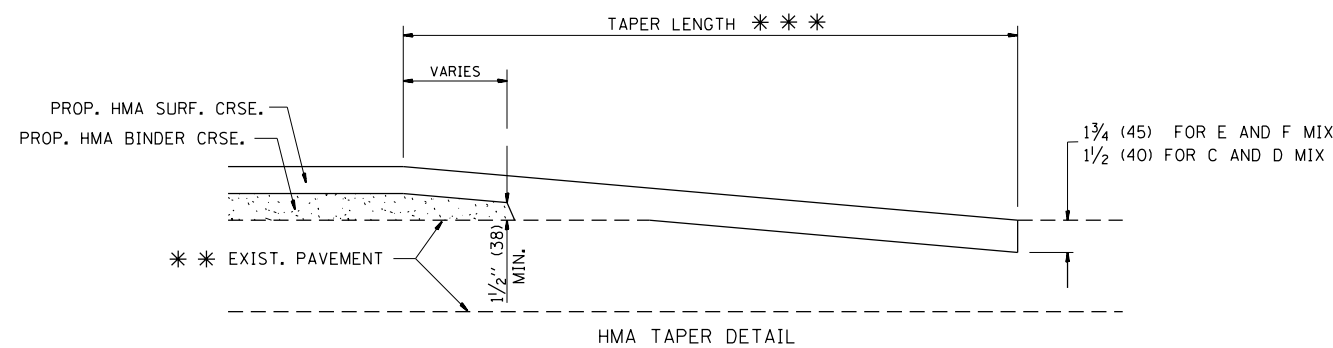


OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

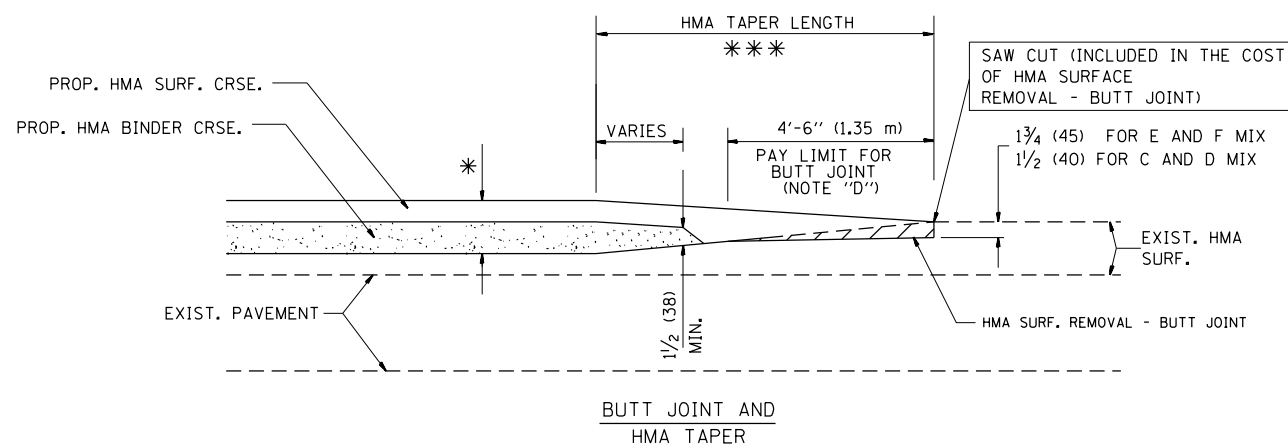
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

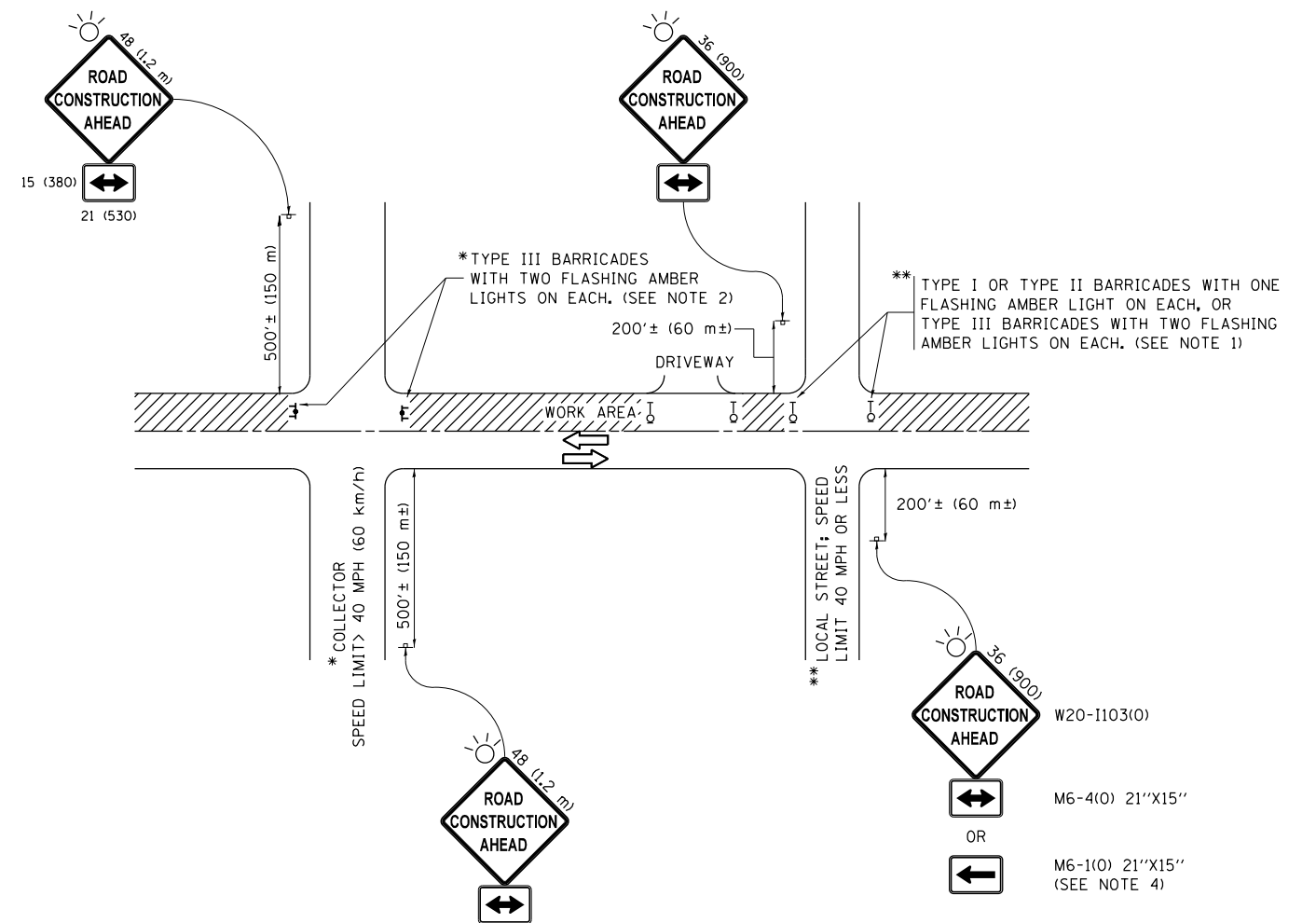
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 10/28/2016	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	(2424&3138)RS-3	COOK	75	65
BD400-05 BD32		CONTRACT NO. 62C19		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

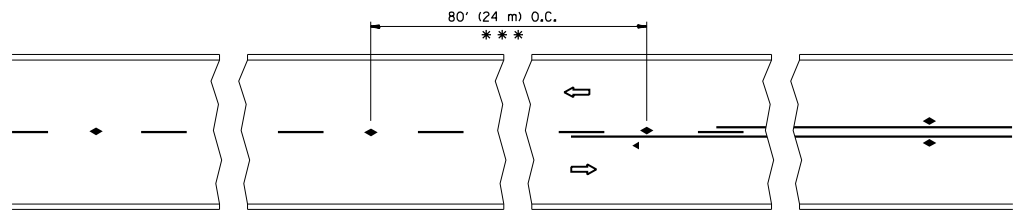
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pw:\IL\084EBIDINTEG\Illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI296\Drawings\Design\Diststd.dgn		CHECKED -	REVISED - T. RAMMACHER 01-06-00
Default	PLOT SCALE = 100.0000' / in.	DATE - 06-89	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 10/28/2016		REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

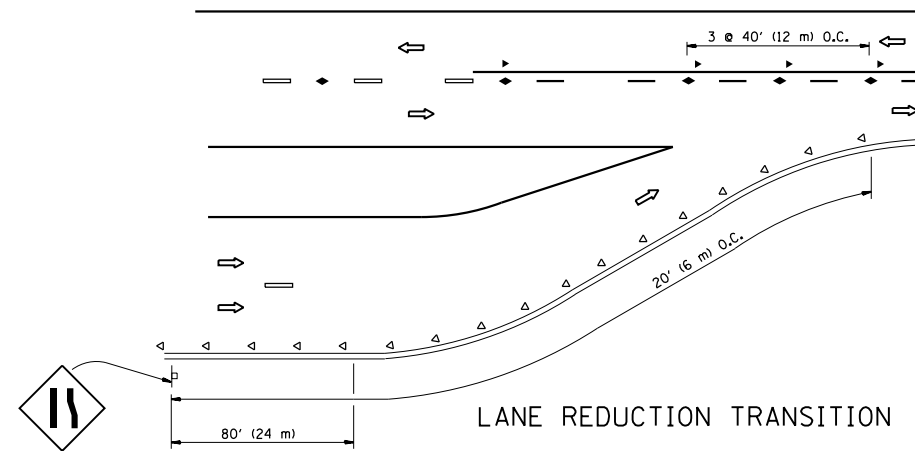
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	(2424&3138)RS-3	COOK	75	66
TC-10			CONTRACT NO. 62C19	
ILLINOIS FED. AID PROJECT				

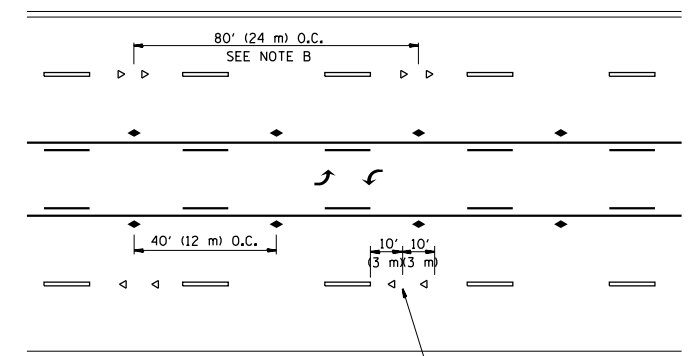


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

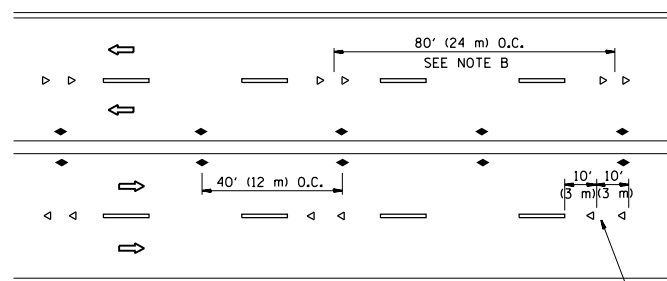
TWO-LANE/TWO-WAY



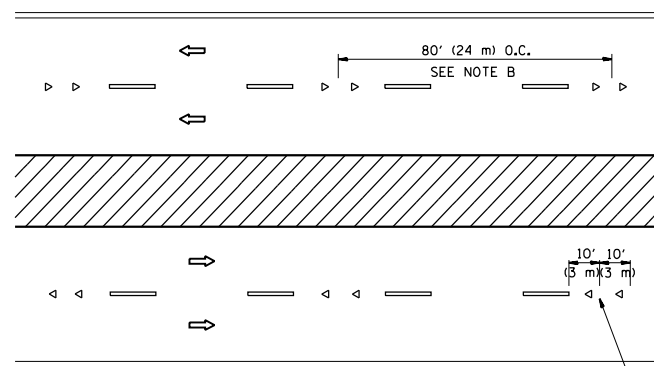
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

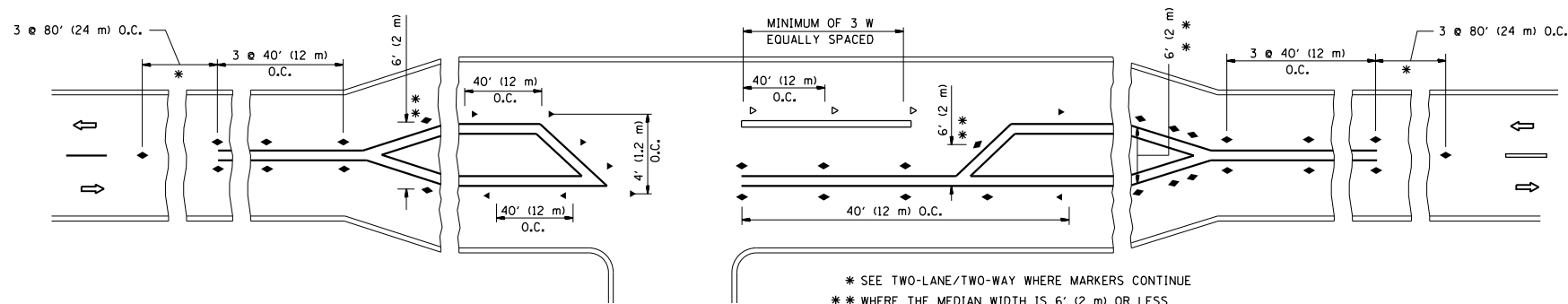
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = currywaw	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
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		DATE -	REVISED - T. RAMMACHER 01-06-00
			REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	(2424&3138)RS-3	COOK	75	67
TC-11		CONTRACT NO. 62C19		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

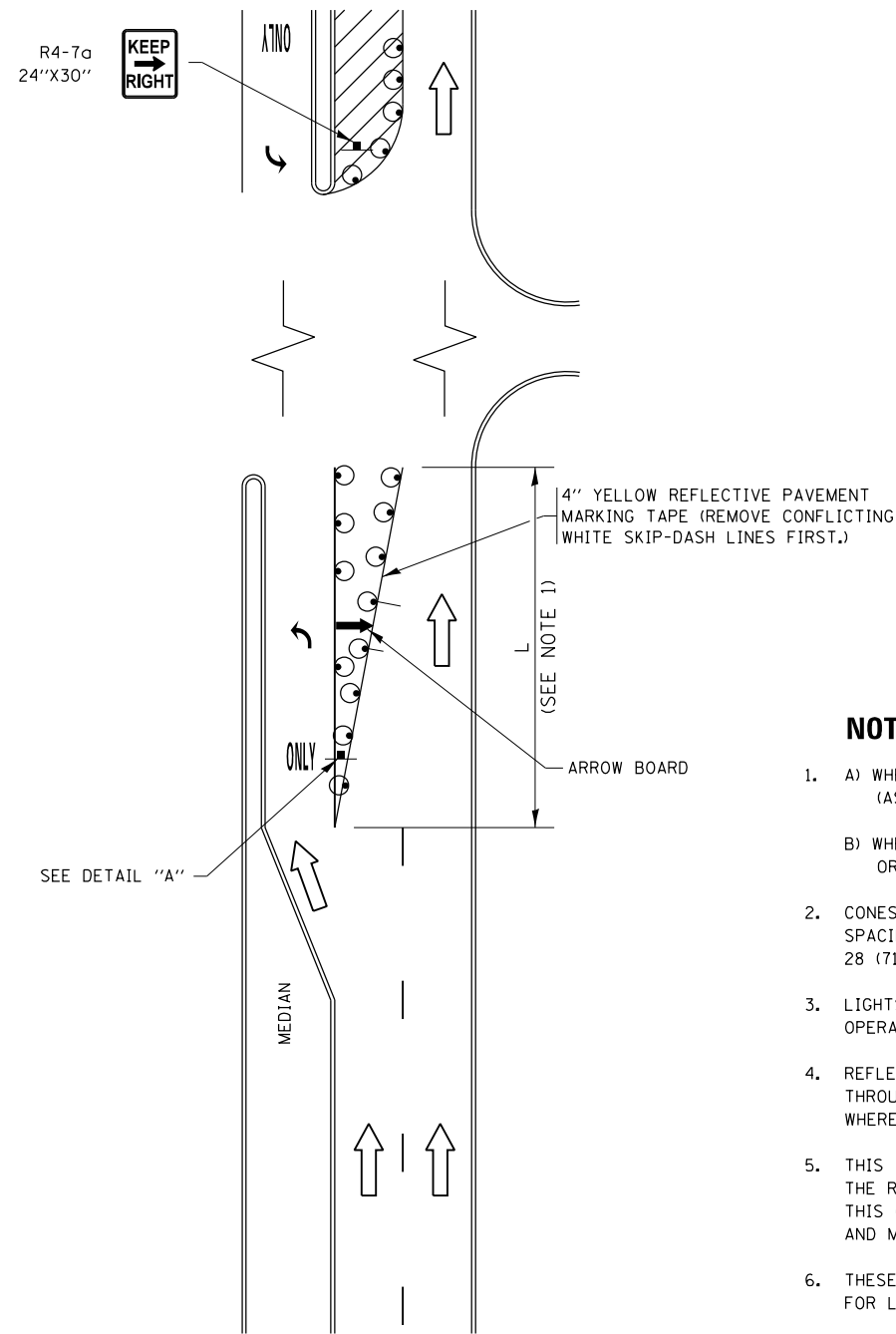


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

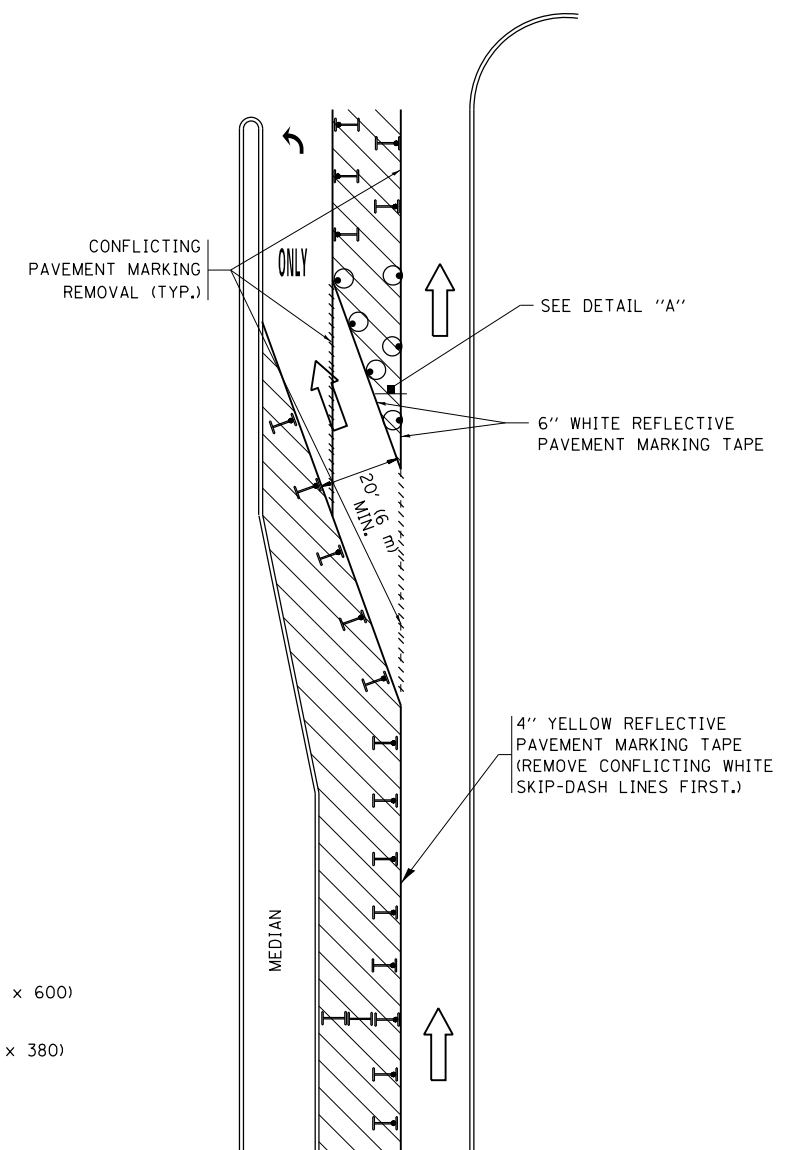


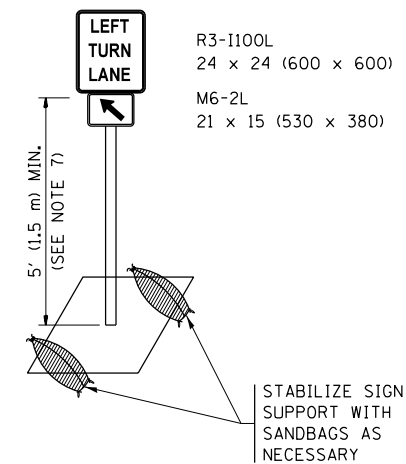
FIGURE 2

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

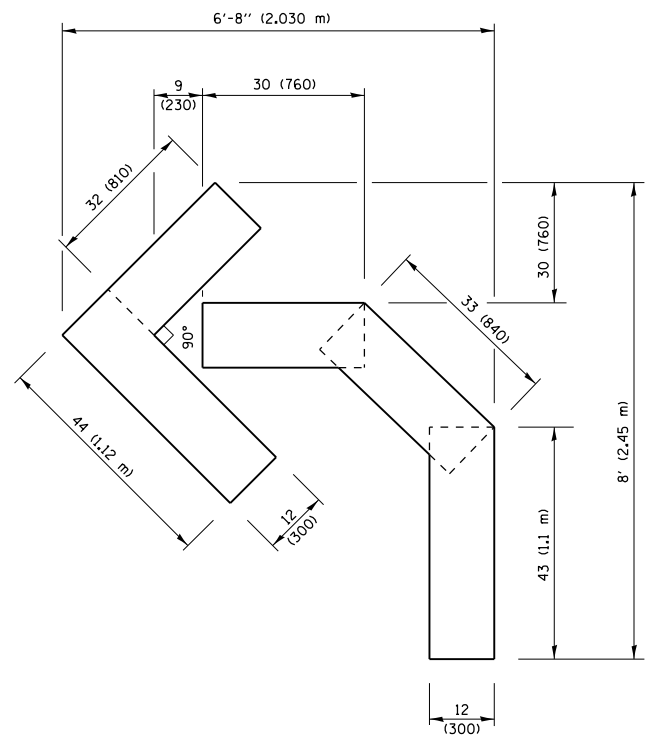
1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



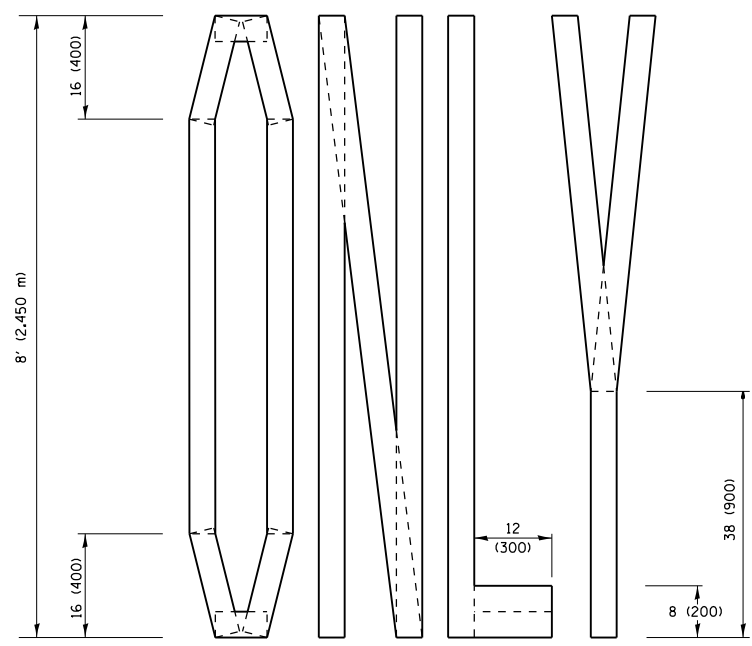
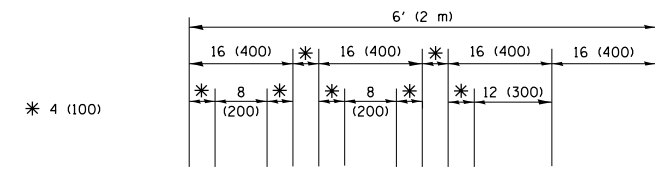
DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

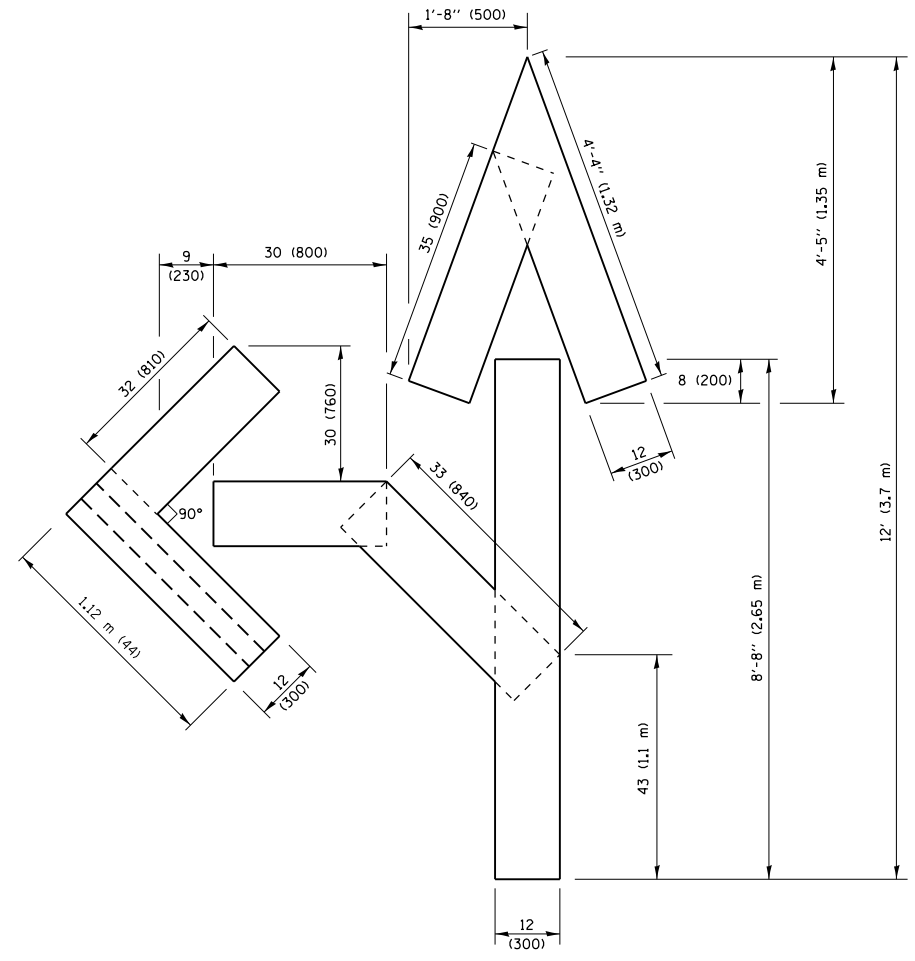
FILE NAME =	USER NAME = curryw	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)	F.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\DI296\Design\HOUSEH 11-07-95	REVISED - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13	REVISED - A. SCHUETZE 07-01-13			348	(242483138)RS-3	COOK	75	69
Default	PLOT SCALE = 100.0000' / in.	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16			TC-14		CONTRACT NO. 62C19		
	PLOT DATE = 10/28/2016	REVISED - T. RAMMACHER 01-06-00	REVISED -			ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

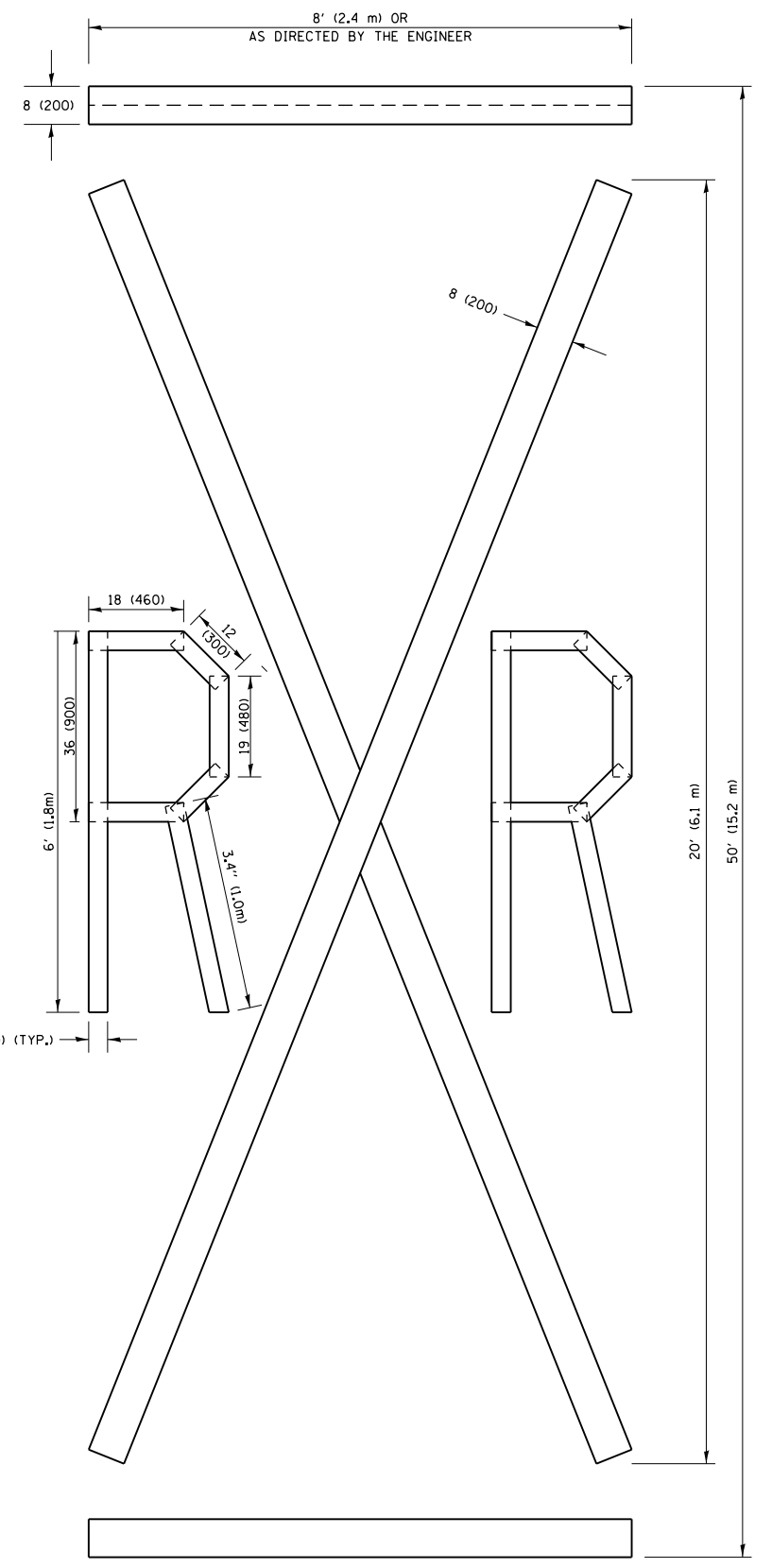


QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

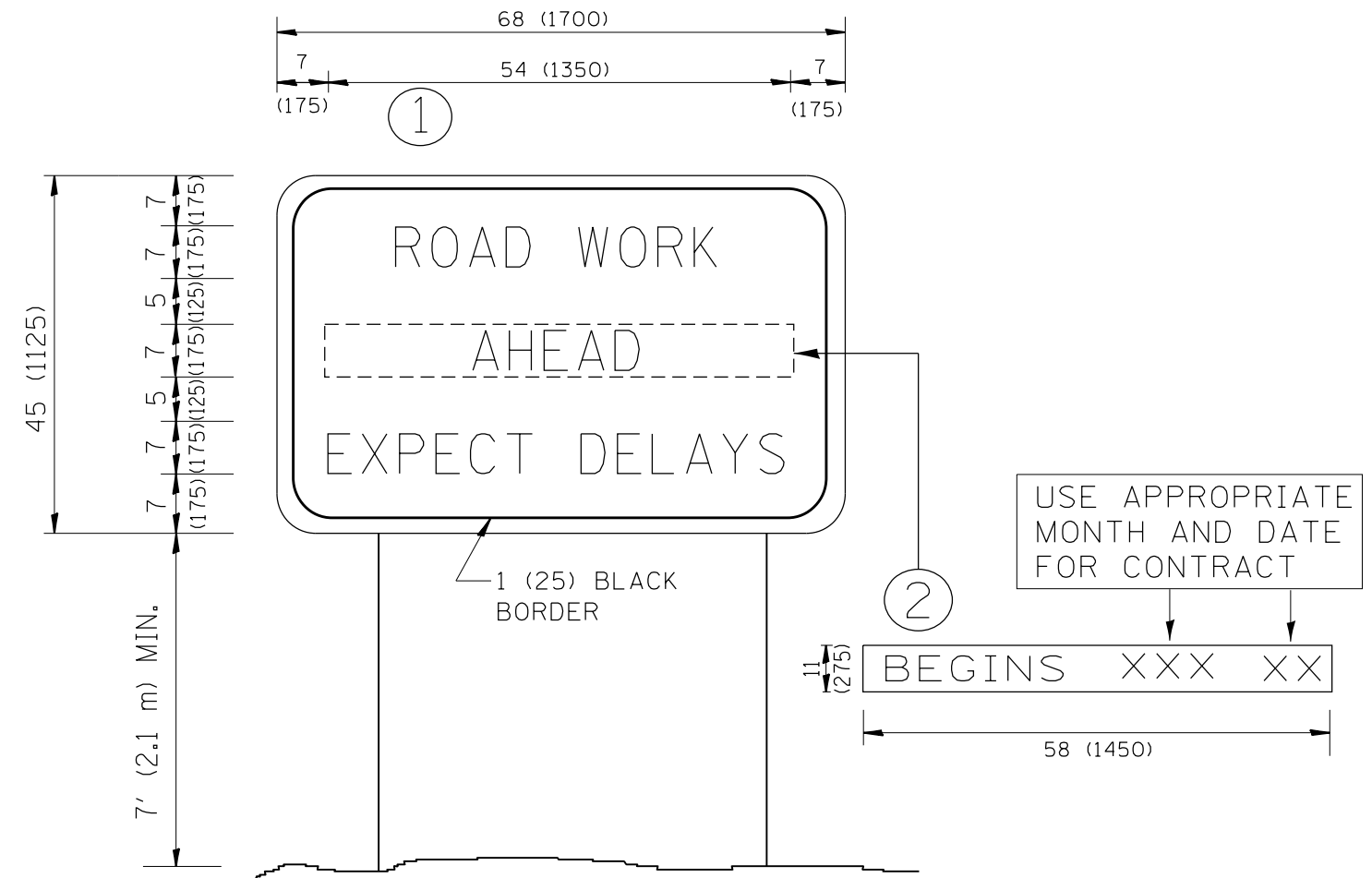
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = currywaw	DESIGNED -	REVISED -T. RAMMACHER 03-02-98
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		CHECKED -	REVISED -E. GOMEZ 08-28-00
		DATE - 09-18-94	REVISED -A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	(2424&3138)RS-3	COOK	75	70
TC-16		CONTRACT NO. 62C19		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

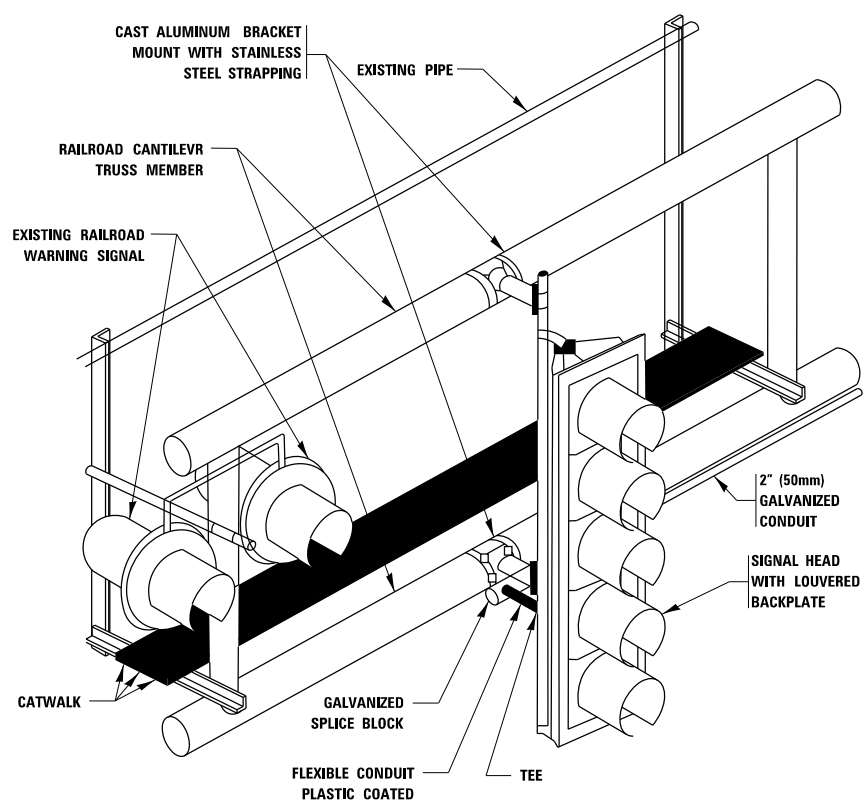


NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = curryw	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11084EBIDINTEG.illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\DI296\Drawings\Design\Diststd.dgn		CHECKED -	REVISED - R. MIRS 12-11-97			348	(2424&3138)RS-3	COOK	75	71
		PLOT SCALE = 100.0000' / 1in.	REVISED - T. RAMMACHER 02-02-99			TC-22		CONTRACT NO. 62C19		
		DATE -	REVISED - C. JUCIUS 01-31-07			SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.
				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						



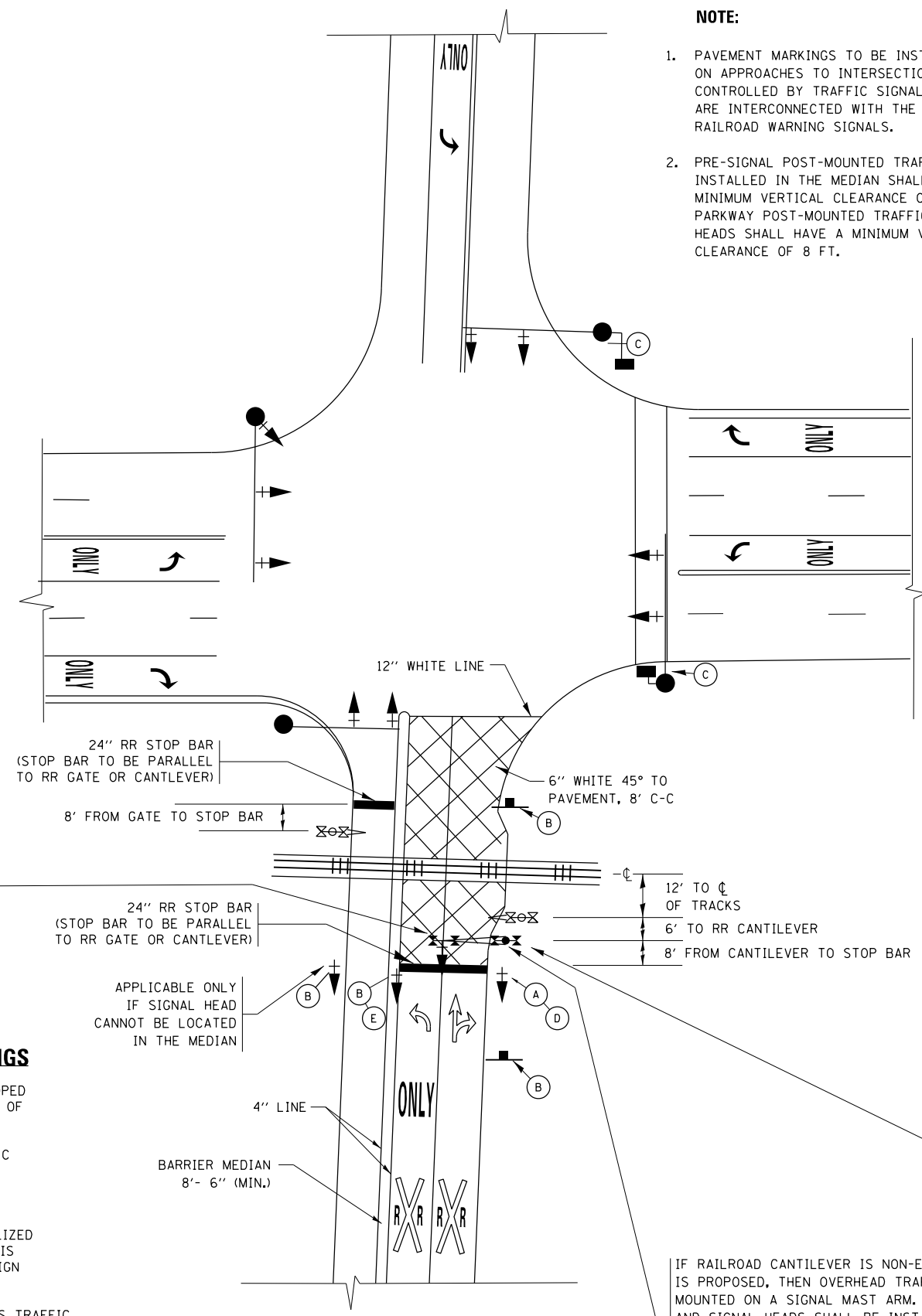
RAILROAD CANTILEVER SIGNAL HEAD MOUNTING
 USE NON-CONDUCTIVE SPACERS BETWEEN THE TRAFFIC SIGNAL EQUIPMENT AND THE RAILROAD CANTILEVER TO PREVENT DISSIMILAR METAL CORROSION
 N.T.S.

SIGNING AND PAVEMENT MARKING AT RAILROAD CROSSINGS

SIGNING AND PAVEMENT MARKING TRAFFIC CONTROL STANDARD (TC-23) HAS BEEN DEVELOPED IN CONSULTATION WITH THE ILLINOIS COMMERCE COMMISSION AND THE U.S. DEPARTMENT OF TRANSPORTATION'S GRADE CROSSING SAFETY TASK FORCE. THIS STANDARD PROVIDES INFORMATION ON UPDATES TO THE PAVEMENT MARKING AND SIGNING DETAILS IN ORDER TO INCORPORATE CHANGES ADOPTED IN THE 2009 NATIONAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICE (MUTCD). THESE NEW DETAILS HAVE BEEN STUDIED AND TESTED BY THE DEPARTMENT AND ACCEPTED BY THE ILLINOIS COMMERCE COMMISSION.

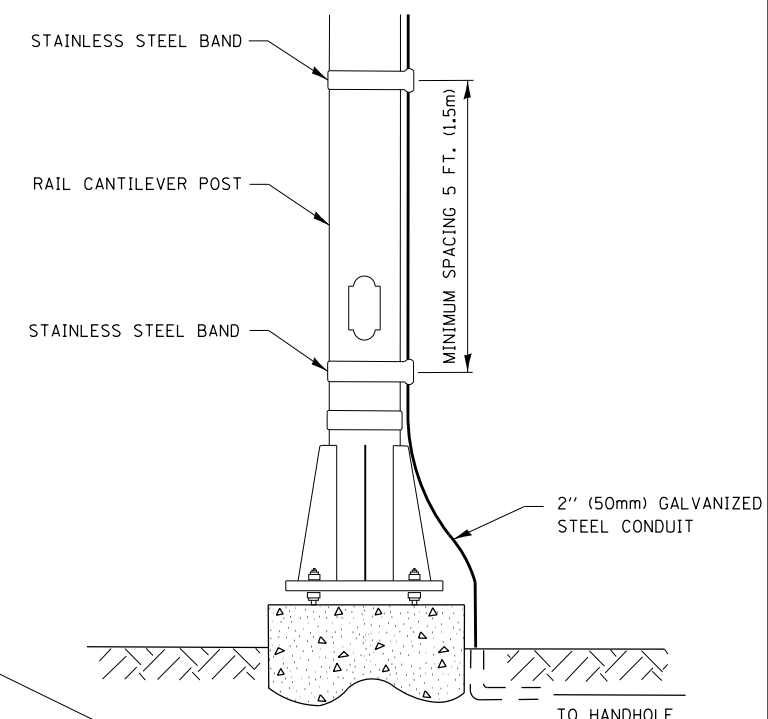
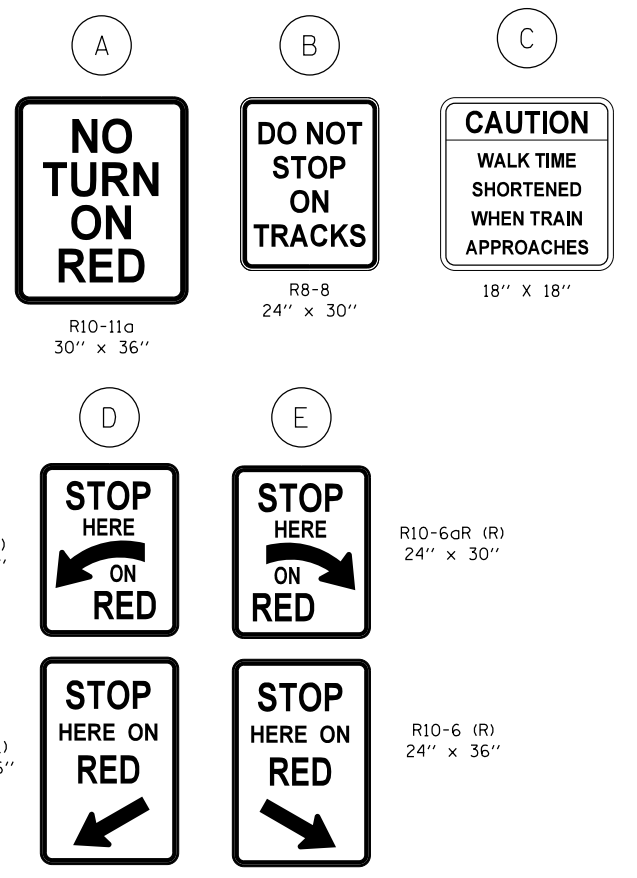
THIS APPLIES TO PROJECTS WHICH INCLUDE RAILROAD INTERCONNECTED TRAFFIC SIGNALS, WITH OR WITHOUT PRE-SIGNALS. THIS STANDARD ALSO APPLIES TO NON-SIGNALIZED INTERSECTIONS THAT ARE WITHIN 81 FEET OF A RAILROAD GRADE CROSSING. THE ILLINOIS SUPPLEMENT TO THE MUTCD SHOULD BE CONSULTED FOR ADDITIONAL INFORMATION ON SIGN REQUIREMENTS AT NON-SIGNALIZED INTERSECTIONS NEAR RAILROAD GRADE CROSSINGS.

THESE DETAILS WILL BE INCLUDED IN A FUTURE UPDATE TO THE BUREAU OF OPERATIONS TRAFFIC POLICIES AND PROCEDURES MANUAL.



SIGNALIZED INTERSECTION WITH NEAR-SIDE TRAFFIC SIGNAL

- NOTE:**
- PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
 - PRE-SIGNAL POST-MOUNTED TRAFFIC SIGNAL HEADS INSTALLED IN THE MEDIAN SHALL HAVE A MINIMUM VERTICAL CLEARANCE OF 4.5 FT. PARKWAY POST-MOUNTED TRAFFIC SIGNAL HEADS SHALL HAVE A MINIMUM VERTICAL CLEARANCE OF 8 FT.



SIGNAL CONDUIT CONNECTION TO RAIL CANTILEVER DETAIL

USE NON-CONDUCTIVE SPACERS BETWEEN THE TRAFFIC SIGNAL EQUIPMENT AND THE RAILROAD CANTILEVER TO PREVENT DISSIMILAR METAL CORROSION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

IF RAILROAD CANTILEVER IS NON-EXISTANT AND NONE IS PROPOSED, THEN OVERHEAD TRAFFIC SIGNAL TO BE MOUNTED ON A SIGNAL MAST ARM. SIGNAL MAST ARM AND SIGNAL HEADS SHALL BE INSTALLED AS CLOSE AS PRACTICABLE TO THE RAILROAD TRACKS WITHOUT OBSTRUCTING ANY RAILROAD WARNING DEVICES. SIGNAL MAST ARM SHALL BE AT LEAST 12 FT. FROM NEAREST RAIL.

FILE NAME =	USER NAME = currywaw	DESIGNED -	REVISED - 02-25-11
pw:\IL\084EBID\INTEG\illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\DI296\Drawings\Design\Diststd.dgn		CHECKED -	REVISED - 04-26-12
Default	PLOT DATE = 10/28/2016	DATE -	REVISED - A.R. 07-11-16

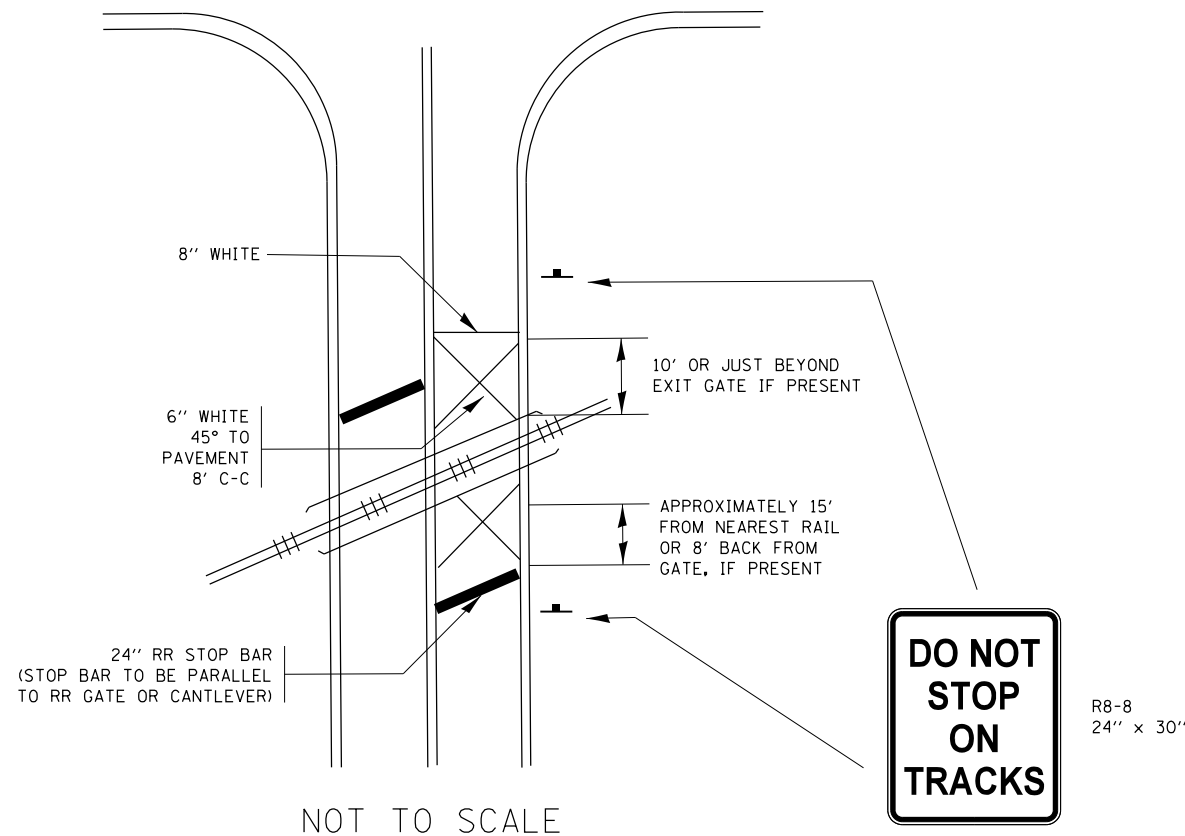
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING
 TREATMENT FOR RAILROAD CROSSINGS**
 SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	(2424&3138)RS-3	COOK	75	72
TC-23			CONTRACT NO. 62C19	
ILLINOIS FED. AID PROJECT				

TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS

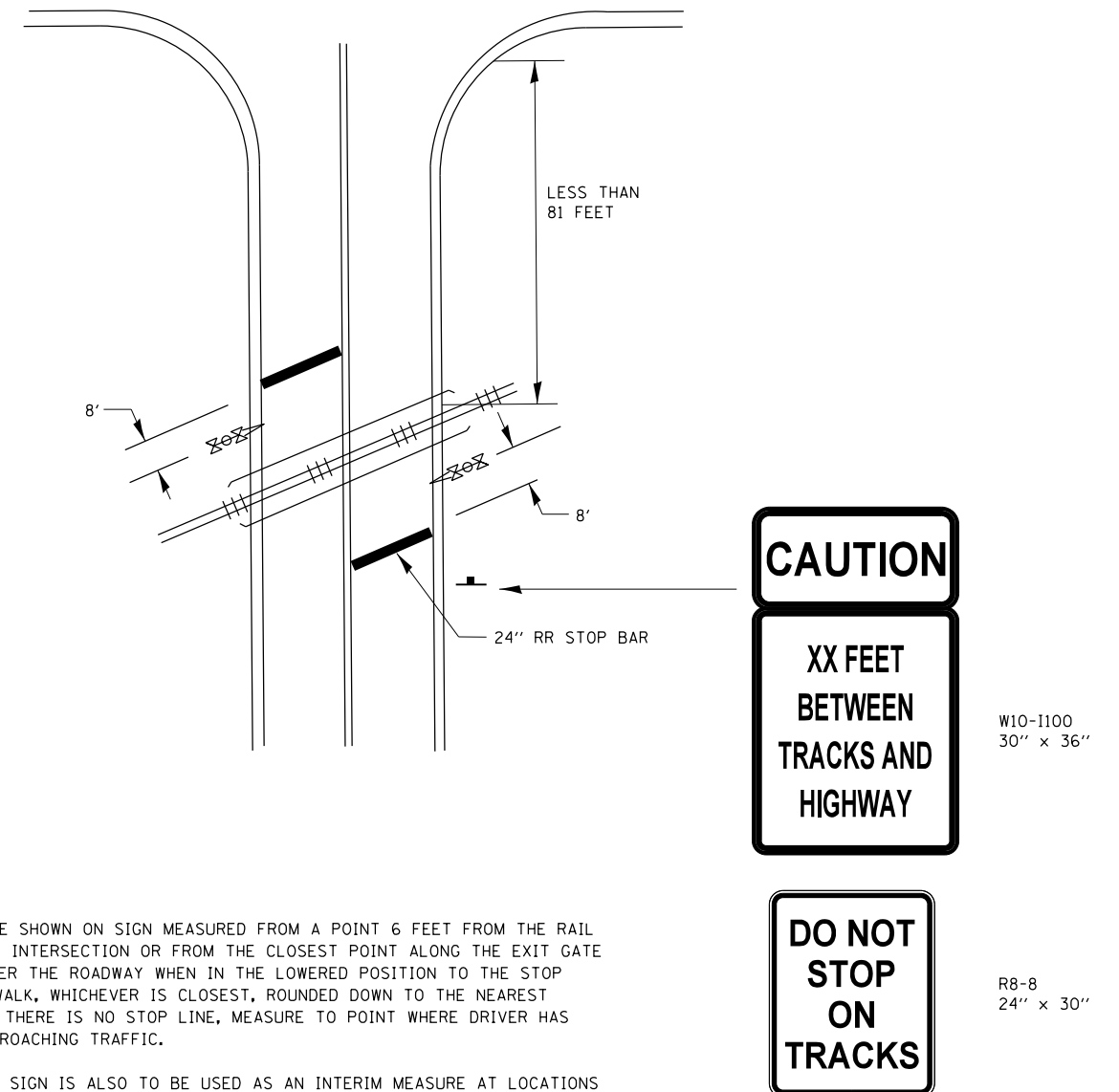
WITH SIGNALIZED INTERSECTION



NOTE:

- PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
- WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED THE PAVEMENT MARKINGS EXTEND TO THE INTERSECTION. (SEE DETAIL FOR PRE-SIGNALS).

WITH NON-SIGNALIZED INTERSECTION 81' OR LESS TO CLOSEST RAIL



NOTE:

- DISTANCE TO BE SHOWN ON SIGN MEASURED FROM A POINT 6 FEET FROM THE RAIL CLOSEST TO THE INTERSECTION OR FROM THE CLOSEST POINT ALONG THE EXIT GATE IF PRESENT OVER THE ROADWAY WHEN IN THE LOWERED POSITION TO THE STOP BAR OR CROSSWALK, WHICHEVER IS CLOSEST, ROUNDED DOWN TO THE NEAREST 5 FEET. WHERE THERE IS NO STOP LINE, MEASURE TO POINT WHERE DRIVER HAS A VIEW OF APPROACHING TRAFFIC.
- THE CLEARANCE SIGN IS ALSO TO BE USED AS AN INTERIM MEASURE AT LOCATIONS WITH INTERCONNECTED INTERSECTION TRAFFIC SIGNALS WHERE IT IS PLANNED TO CHANGE THEM TO NEAR-SIDE SIGNALS AT A FUTURE TIME. IN THIS CASE, THE DISTANCE TO BE SHOWN ON THE SIGN IS MEASURED FROM THE EDGE OF THE STRIPED-OUT AREA INSTEAD OF 6 FEET FROM THE RAIL. THE SIGN IS TO BE REMOVED WHEN THE NEAR-SIDE SIGNALS ARE INSTALLED AND THE PAVEMENT MARKING EXTEND TO THE INTERSECTION.

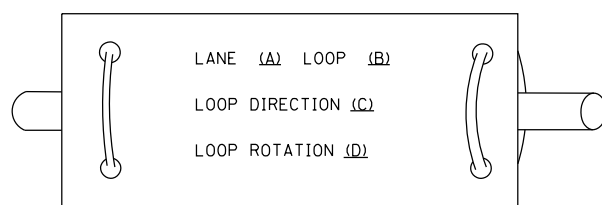
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = curryw	DESIGNED -	REVISED - 02-25-11	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG\illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\1226\BROWN\Meta\Design\Diststd.dgn		CHECKED -	REVISED - 04-26-12			348	(2424&3138)RS-3	COOK	75	73
Default		PLOT SCALE = 100.0000' / in.	REVISED - A.R. 07-11-16			TC-23		CONTRACT NO. 62C19		
		PLOT DATE = 10/28/2016	DATE -			SCALE: NONE	SHEET 2 OF 2 SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT	

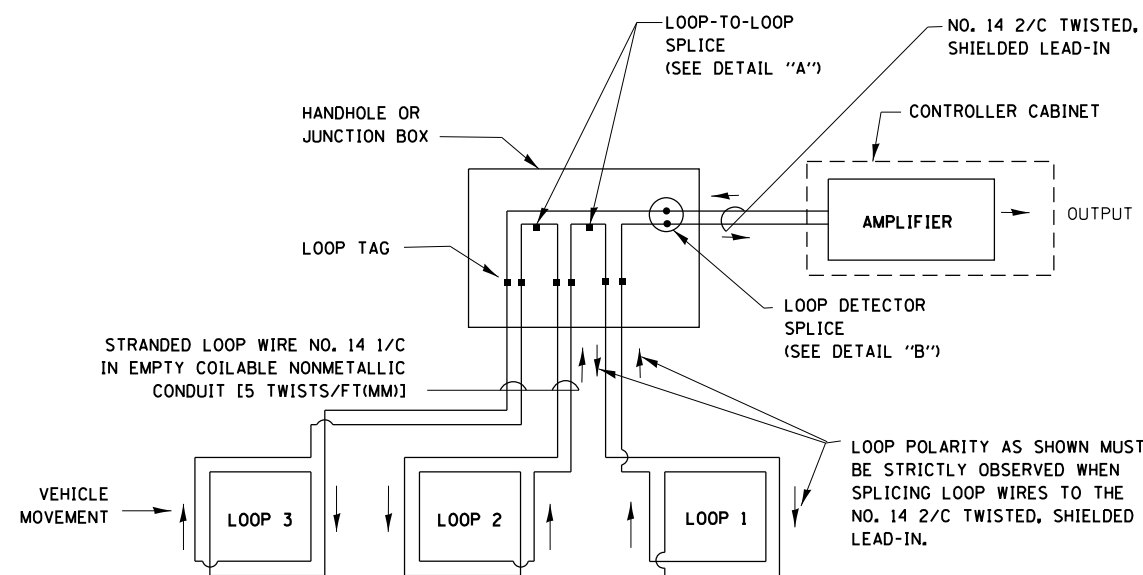
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

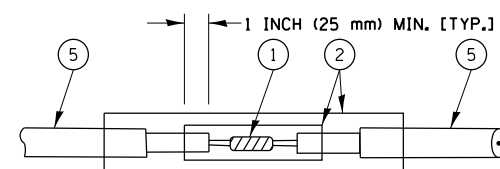


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

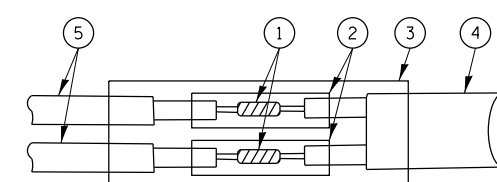


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

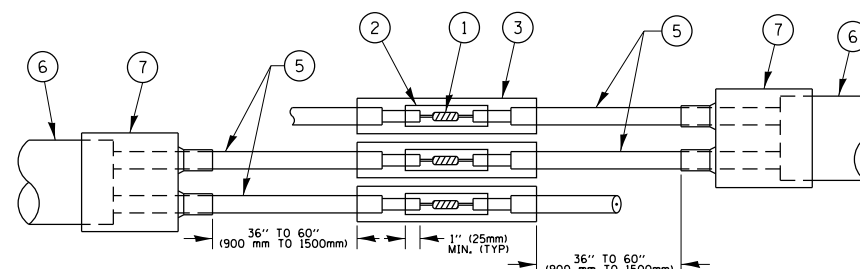


DETAIL "A"
LOOP-TO-LOOP SPLICE

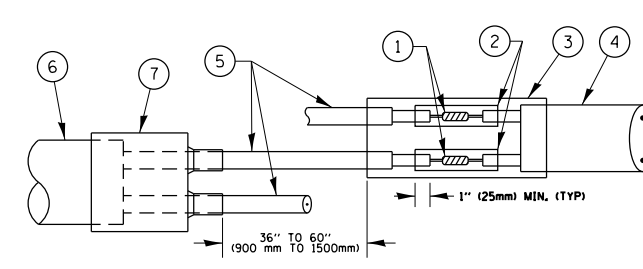


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH, THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

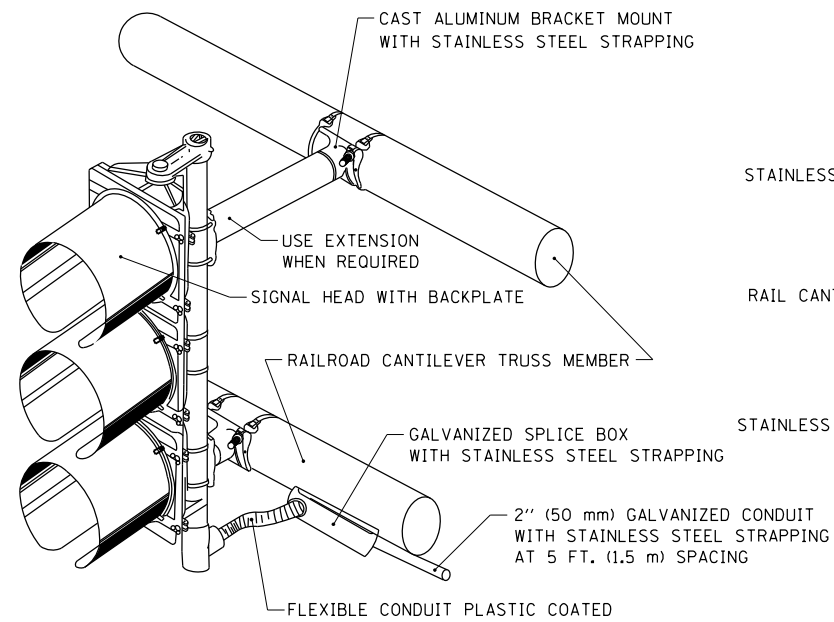
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Default	PLOT DATE = 10/28/2016	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

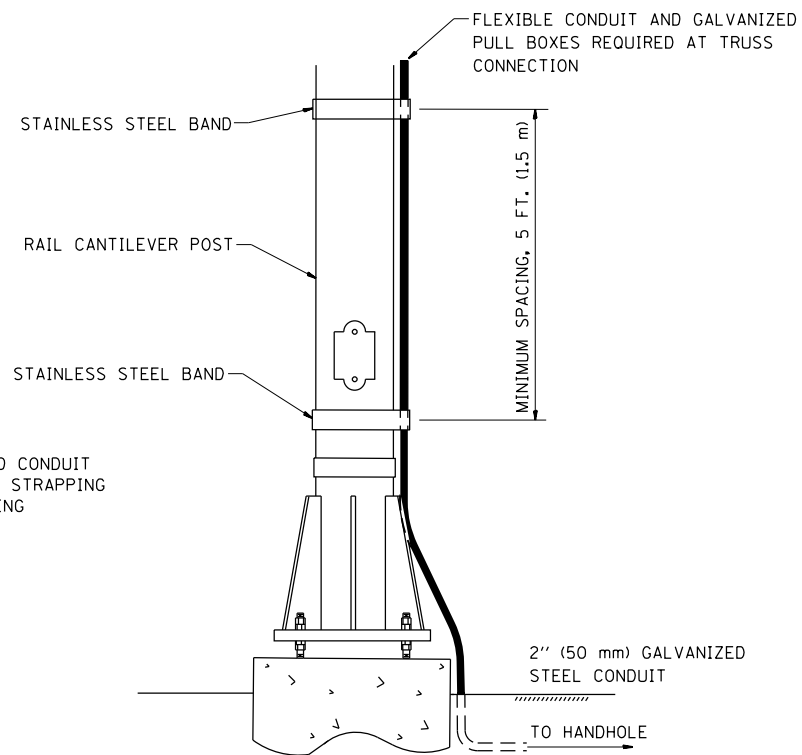
**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET 2 OF 7 SHEETS STA. TO STA.

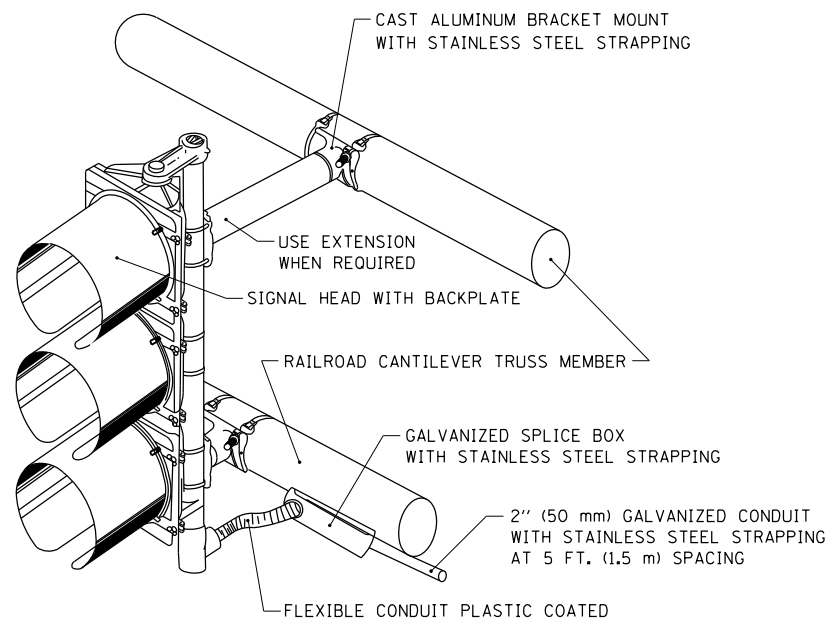
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	(2424&3138)RS-3	COOK	75	74
TS-05		CONTRACT NO.	62C19	
ILLINOIS FED. AID PROJECT				



RAILROAD CANTILEVER SIGNAL HEAD MOUNTING

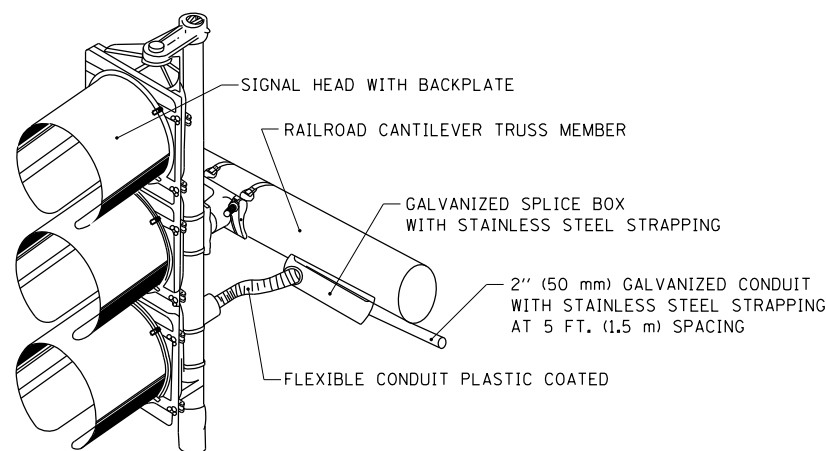


SIGNAL CONDUIT CONNECTION TO RAIL CANTILEVER DETAIL



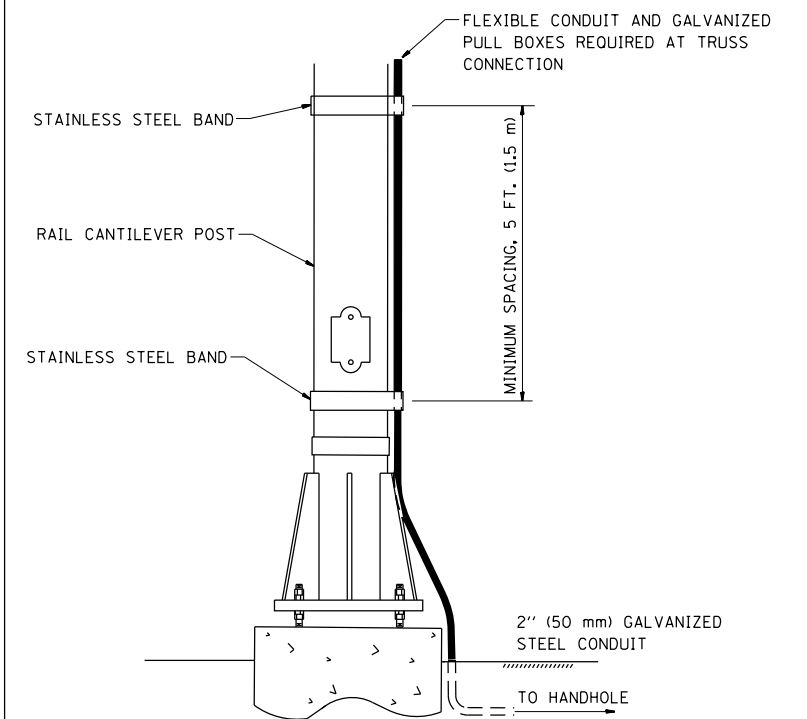
NOTE: USE NONCONDUCTIVE SPACERS BETWEEN THE TRAFFIC SIGNAL EQUIPMENT AND THE RAILROAD CANTILEVER TO PREVENT DISSIMILAR METAL CORRSION.

RAILROAD CANTILEVER SIGNAL HEAD MOUNTING



NOTE: USE NONCONDUCTIVE SPACERS BETWEEN THE TRAFFIC SIGNAL EQUIPMENT AND THE RAILROAD CANTILEVER TO PREVENT DISSIMILAR METAL CORRSION.

RAILROAD CANTILEVER SIGNAL HEAD MOUNTING



NOTE: USE NONCONDUCTIVE SPACERS BETWEEN THE TRAFFIC SIGNAL EQUIPMENT AND THE RAILROAD CANTILEVER TO PREVENT DISSIMILAR METAL CORRSION.

SIGNAL CONDUIT CONNECTION TO RAIL CANTILEVER DETAIL

FILE NAME =	USER NAME = currywaw	DESIGNED -	REVISED -
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PLOT SCALE = 100.0000' / in.	CHECKED -		REVISED -
PLOT DATE = 10/28/2016	DATE -		REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

RAILROAD CANTILEVER SIGNAL HEAD MOUNTING DETAIL			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	(2424&3138)RS-3	COOK	75	75
TS-06		CONTRACT NO.	62C19	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				