

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.U. R.T.E. 3565	SECTION (46-2)RS	COUNTY COOK	TOTAL SHEETS 49	SHEET NO. 1
ILLINOIS			CONTRACT NO. 60Y87	

PROPOSED HIGHWAY PLANS

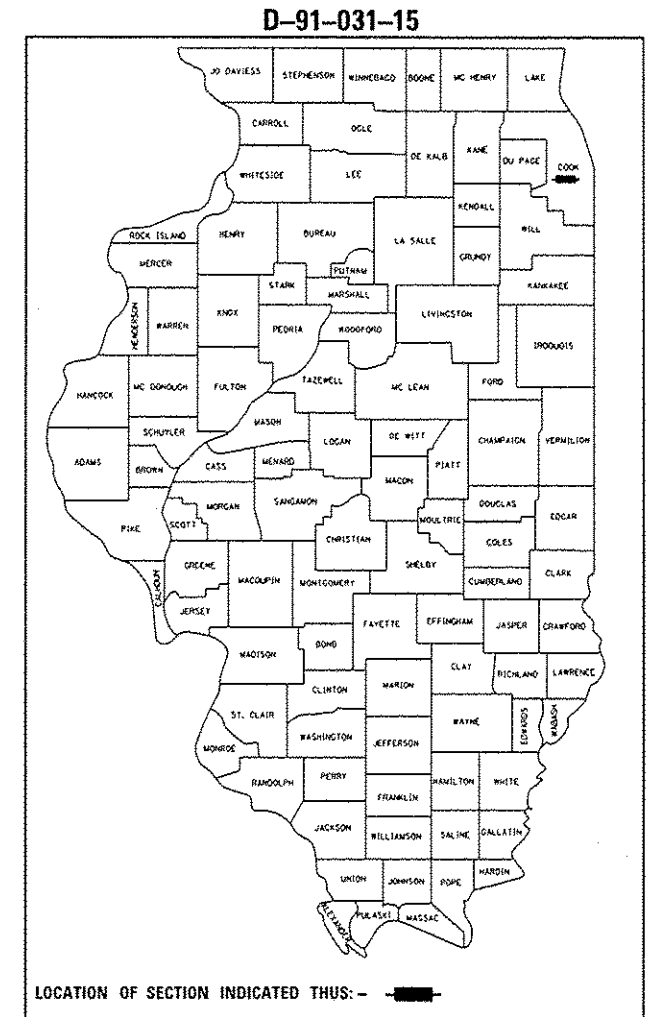
FAU ROUTE 3565 : IL-171 (ARCHER ROAD)
FROM 55TH PLACE TO 63RD STREET
SECTION (46-2)RS
PROJECT ACM-3565(007)
RESURFACING (3P)
COOK COUNTY

FOR INDEX OF SHEETS, SEE SHEET NO.

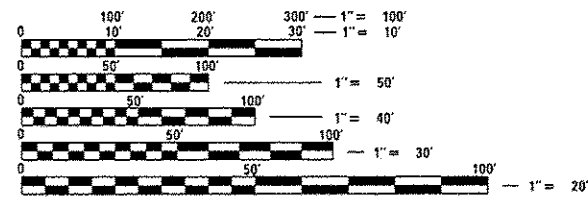
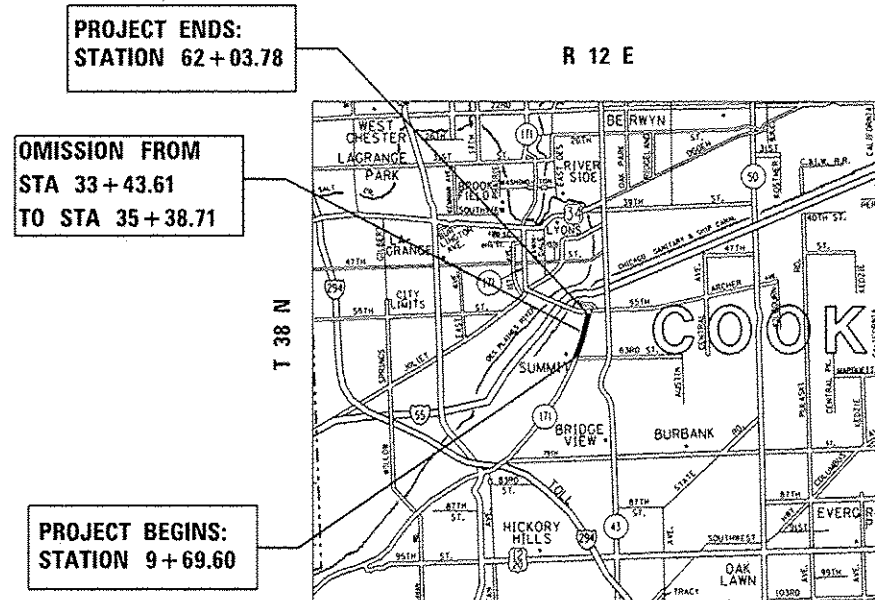
PROJECT IS LOCATED IN THE VILLAGE OF SUMMIT

TRAFFIC DATA:

IL ROUTE 171:
2015 ADT = 21,100
SPEED LIMIT = 35 MPH



C-91-031-15



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER JENPAI CHANG (847)705-4432
PROJECT MANAGER FAWAD AQUEEL (847)705-4247

GROSS LENGTH = 5,150 FT. = 0.975 MILE
NET LENGTH = 4,955 FT. = 0.938 MILE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED *October 27, 2016*

John F. ...
REGIONAL ENGINEER

Maureen M. Addis
ENGINEER OF DESIGN AND ENVIRONMENT

...
DIRECTOR OF PROGRAM DEVELOPMENT

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

CONTRACT NO. 60Y87

INDEX OF SHEETS

STATE STANDARDS:

GENERAL NOTES:

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES
3	SUMMARY OF QUANTITIES
6	TYPICAL SECTIONS
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11	DETECTOR LOOP REPLACEMENT PLANS
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45	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)
46	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)
47	BUTT JOINT AND HMA TAPER DETAILS (BD-32)
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49	DISTRICT 1 DETECTOR LOOP INSTALLATION (TS-07) ✓

000001-06	TYPICAL SYMBOLS, ABBREVIATIONS AND PATTERNS
442201-03	CLASS C AND D PATCHES
606001-06	CONCRETE CURB AND COMBINATION CONCRETE CURB AND GUTTER
701427-05	LANE CLOSURE, MULTILANE INTERMITTENT OR MOVING OPERATION FOR SPEEDS < 40 MPH
701602-08	URBAN LANE CLOSURE MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701611-01	URBAN HALF ROAD CLOSURE MOUNTABLE MEDIAN MULTILANE, 2W WITH
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK CORNER OR CROSSWALK CLOSURE
701901-06	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOURS NOTIFICATION REQUIRED).
- 10 FEET (3 METER) TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURB AND GUTTERS AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN.
- THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGE OF LOCKPORT, ROMEOVILLE AND LEMONT.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H) WITH WRITTEN APPROVAL FROM THE ENGINEER. A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- THE RESIDENT ENGINEER SHALL CONTACT MS. CORA MATHIS, AREA TRAFFIC FIELD ENGINEER AT (815) 485-6475 A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKING.
- ALL PAVEMENT PATCHING AND COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
- THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PROIR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.

URBAN

URBAN

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE	
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	FED 80 STATE 20 0005
20200100	EARTH EXCAVATION	CU YD	140	140
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	227	227
25200110	SODDING, SALT TOLERANT	SO YD	227	227
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	24161	24161
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	21	21
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), 1L-4.75, N50	TON	1567	1567
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	128	128
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SO YD	53	53
40603565	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70	TON	3653	3653
42001300	PROTECTIVE COAT	SO YD	1744	1744
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	11435	11435
42400800	DETECTABLE WARNINGS	SO FT	767	767
44000100	PAVEMENT REMOVAL	SO YD	33	33

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE	
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	FED 80 STATE 20 0005
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SO YD	3087	3087
44000161	HOT-MIX ASPHALT SURFACE REMOVAL, 3"	SO YD	11060	11060
44000600	SIDEWALK REMOVAL	SQ FT	11210	11210
44201761	CLASS D PATCHES, TYPE I, 10 INCH	SO YD	15	15
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SO YD	104	104
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SO YD	80	80
44201835	CLASS D PATCHES, TYPE I, 16 INCH	SO YD	10	10
44201839	CLASS D PATCHES, TYPE II, 16 INCH	SO YD	48	48
44201843	CLASS D PATCHES, TYPE III, 16 INCH	SO YD	27	27
50104000	BRIDGE RAIL REMOVAL	FOOT	13	13
50901750	PARAPET RAILING	FOOT	23	23
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	2	2
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	31	31
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	47	47
60600605	CONCRETE CURB, TYPE B	FOOT	640	640
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	140	140
		*SPECIALTY ITEM		

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FILE NAME :	USER NAME :	DESIGNED :	REVISED :
PROJECT :	PROJECT :	CHECKED :	REVISED :
DATE :	DATE :	DATE :	DATE :

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

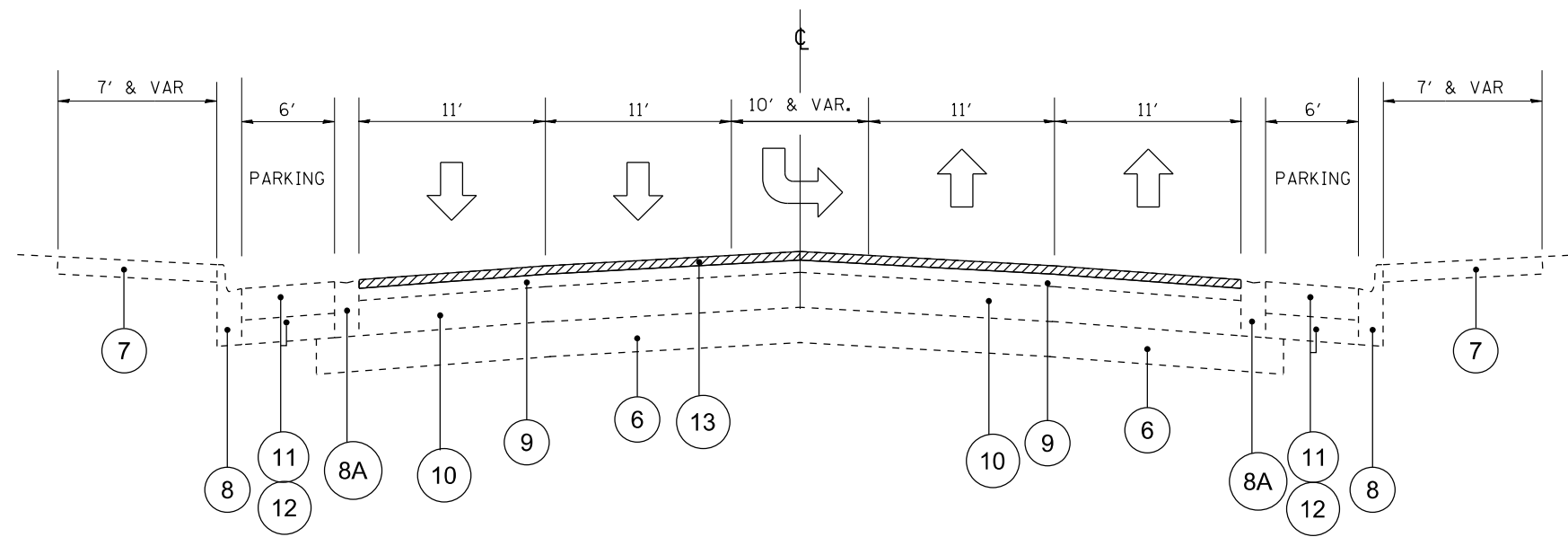
IL-171 (55TH PL TO 63RD ST) SUMMARY OF QUANTITIES

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3565	146-21RS	COOK	49	3
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60Y87	

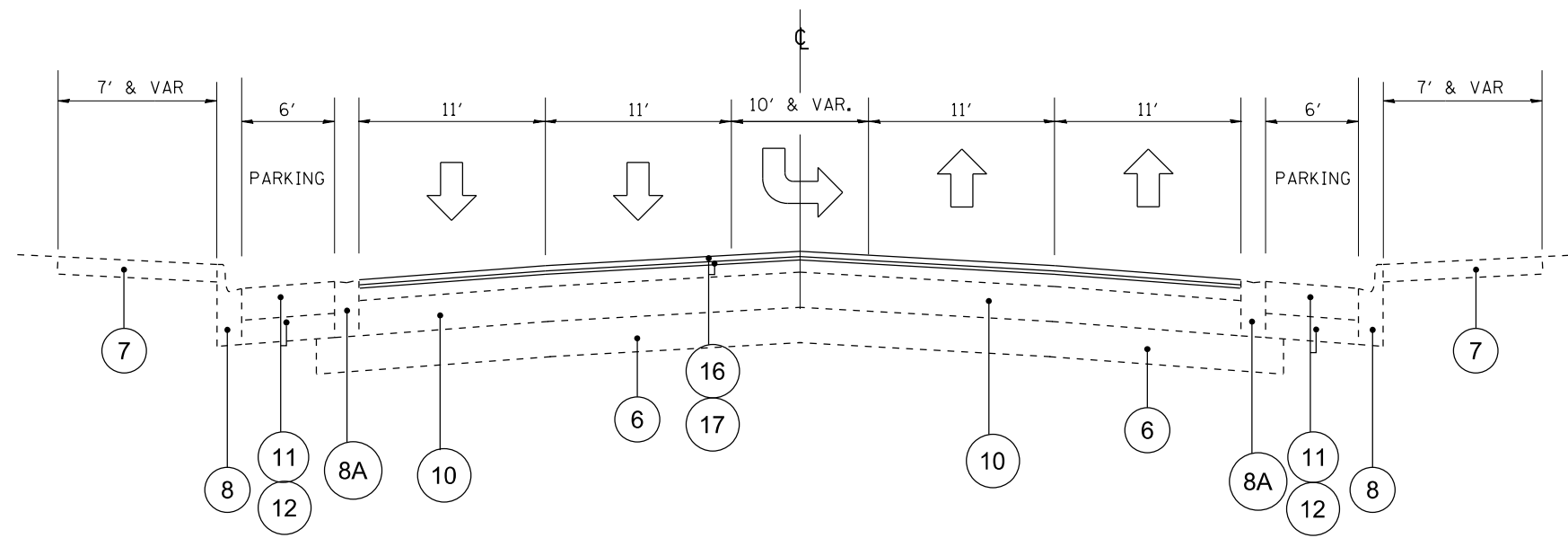
SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	FED 80 STATE 20 0005					CODE NO	ITEM	UNIT	TOTAL QUANTITIES	FED 80 STATE 20 0005					
* 66900450	SPECIAL WASTE PLANS AND REPORTS	LSUM	1	1					* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	291	291					
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	3	3					* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	12743	12743					
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	3	3					* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	3023	3023					
67100100	MOBILIZATION	LSUM	1	1					* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	2785	2785					
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	LSUM	1	1					* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	610	610					
70102634	TRAFFIC CONTROL AND PROTECTION, STANDARD 701611	LSUM	1	1					* 78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	222	222					
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	LSUM	1	1					* 78008230	POLYUREA PAVEMENT MARKING TYPE I - LINE 6"	FOOT	82	82					
70300100	SHORT TERM PAVEMENT MARKING	FOOT	2764	2764					* 78008270	POLYUREA PAVEMENT MARKING TYPE I - LINE 24"	FOOT	35	35					
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	50 FT	461	461					* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	455	455					
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	50 FT	291	291					78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	455	455					
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	12743	12743					* 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	17	17					
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	3023	3023														
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	2785	2785														
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	610	610														

*SPECIALTY ITEM

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STA 9+69.60 TO STA 13+65.61

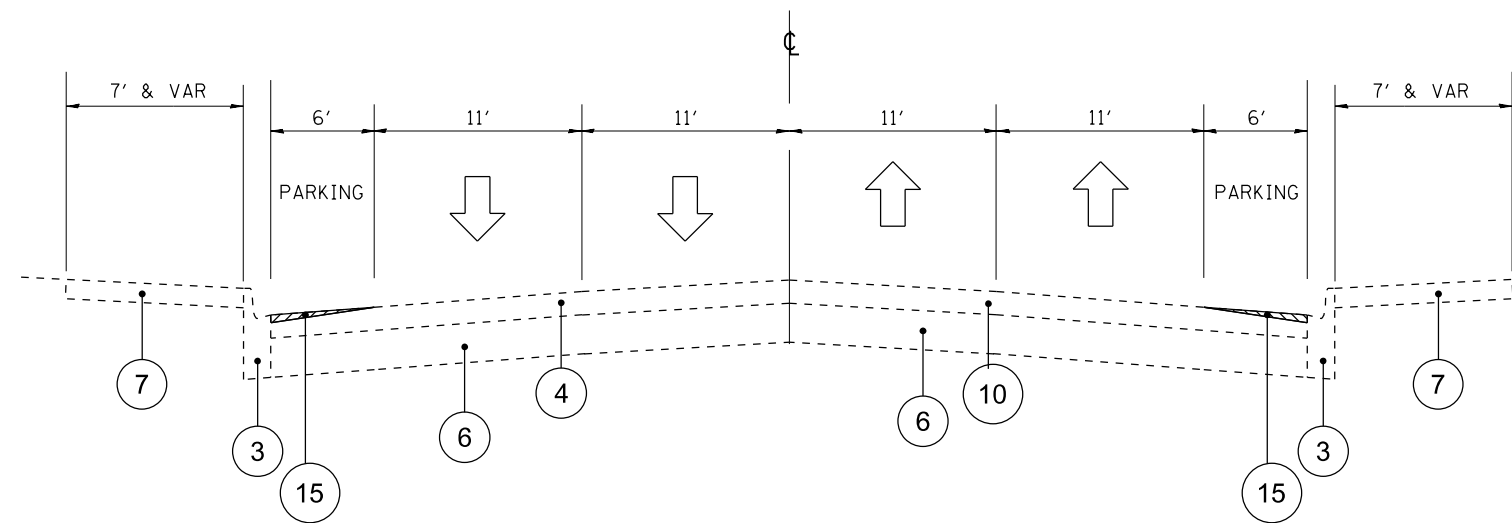


STA 9+69.60 TO STA 13+65.61

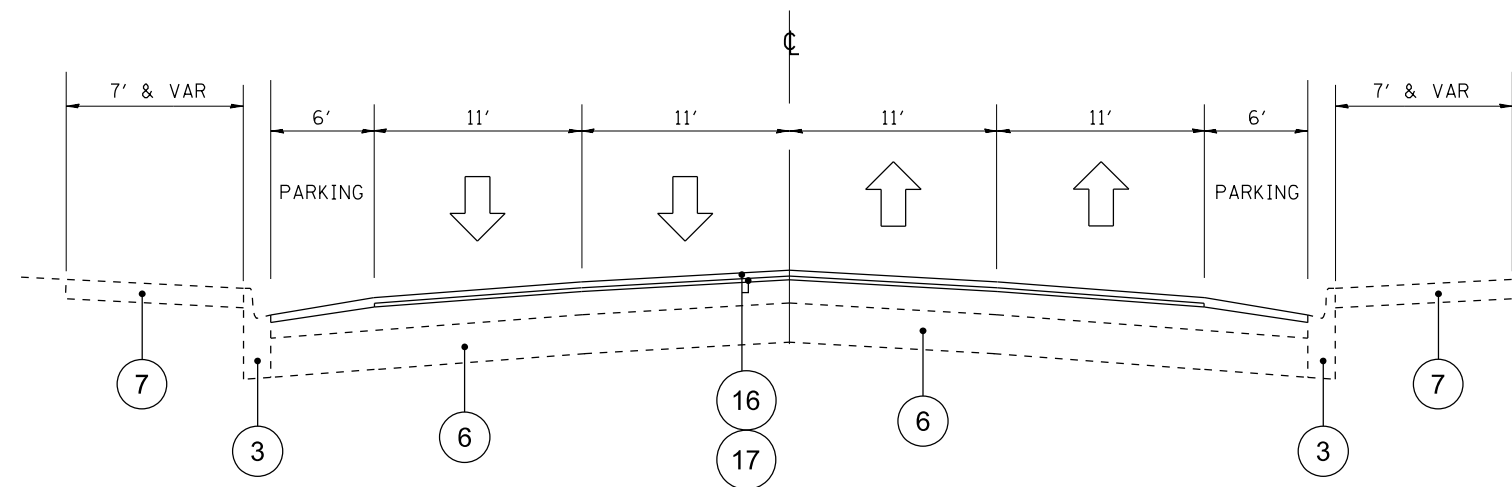
NOTE: MILLING TO BE DONE BEFORE PATCHING

- ① EXIST. SUB-BASE GRANULAR MATERIAL, 4"
- ② EXIST. PCC BASE COURSE, 10"
- ③ EXIST. PCC COMB. CURB & GUTTER, B.6-24
- ④ EXIST. PCC PAVEMENT, 10"
- ⑤ EXIST. HMA SURFACE COURSE, 3"
- ⑥ EXIST. AGGREGATE SUBGRADE, 12"
- ⑦ EXIST. PCC SIDEWALK, 5"
- ⑧ EXIST. PCC COMB. CURB & GUTTER, B.6-12
- ⑧A EXIST. PCC GUTTER FLAG
- ⑨ EXIST. HMA SURFACE COURSE, 7"
- ⑩ EXIST. PCC PAVEMENT, 9"
- ⑪ EXIST. PCC PARKING LANE
- ⑫ EXIST. PARKING LANE SUBBASE
- ⑬ PROP. HMA SURFACE REMOVAL, 2½"
- ⑭ PROP. HMA SURFACE REMOVAL, 3"
- ⑮ PROP. PCC PAVEMENT MILLING, VARIABLE DEPTH
- ⑯ PROP. POLY. HMA SURF COURSE, MIX "E", N70, 1¾"
- ⑰ PROP. POLY LEVELING BINDER, (MM) IL-4.75, N50, ¾"
- ⑱ PROP. POLY LEVELING BINDER, (MM) IL-4.75, N50, 1¼"

FILE NAME =	USER NAME = ledezmar	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTIONS IL. 171 (S. ARCHER ROAD) FROM 55TH PLACE TO 63RD STREET	F.A.U. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
p:\IL\084EBIDINTEG.illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\DI03115\Drawings\Design\DI03115-sh-typical.dgn						REVISED -	3565	(46-2)RS	COOK	49	6
PLOT SCALE = 100.0000' / 1in.						REVISED -	CONTRACT NO. 60Y87				
Default						REVISED -	ILLINOIS FED. AID PROJECT				
PLOT DATE = 10/31/2016				DATE -	SCALE: 1" = 50'	SHEET OF SHEETS		STA.	TO STA.		



STA 13+65.61 TO STA 33+44
STA 35+39 TO STA 48+41



STA 13+65.61 TO STA 33+44
STA 35+39 TO STA 48+41

NOTE: MILLING TO BE DONE BEFORE PATCHING

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE TYPE	AIR VOIDS (%) @ NDES	
RESURFACING:		
POLY. HMA SURFACE COURSE MIX "E", N70, 1 3/4" (IL-9.5mm)	4% @ 70 GYR.	OCP
POLY. LEVELING BINDER (MM) IL-4.75, N50, (3/4" FROM STA 9+69.60 TO STA 13+65.61) (1/4" FROM STA 48+41 TO STA 62+04)	3.5% @ 50 GYR.	OCP
HOT-MIX ASPHALT OVERLAY:		
POLY. HMA SURFACE COURSE MIX "E", N70, 1 3/4" (IL-9.5mm)	4% @ 70 GYR.	OCP
POLY. LEVELING BINDER (MM) IL-4.75, N50, 3/4"	3.5% @ 50 GYR.	OCP
HOT-MIX ASPHALT PATCHING		
CLASS D PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR.	QC/OA
QMP DESIGNATION QUALITY CONTROL/QUALITY ASSURANCE (QC/OA) QUALITY CONTROL FOR PERFORMANCE (OCP)		

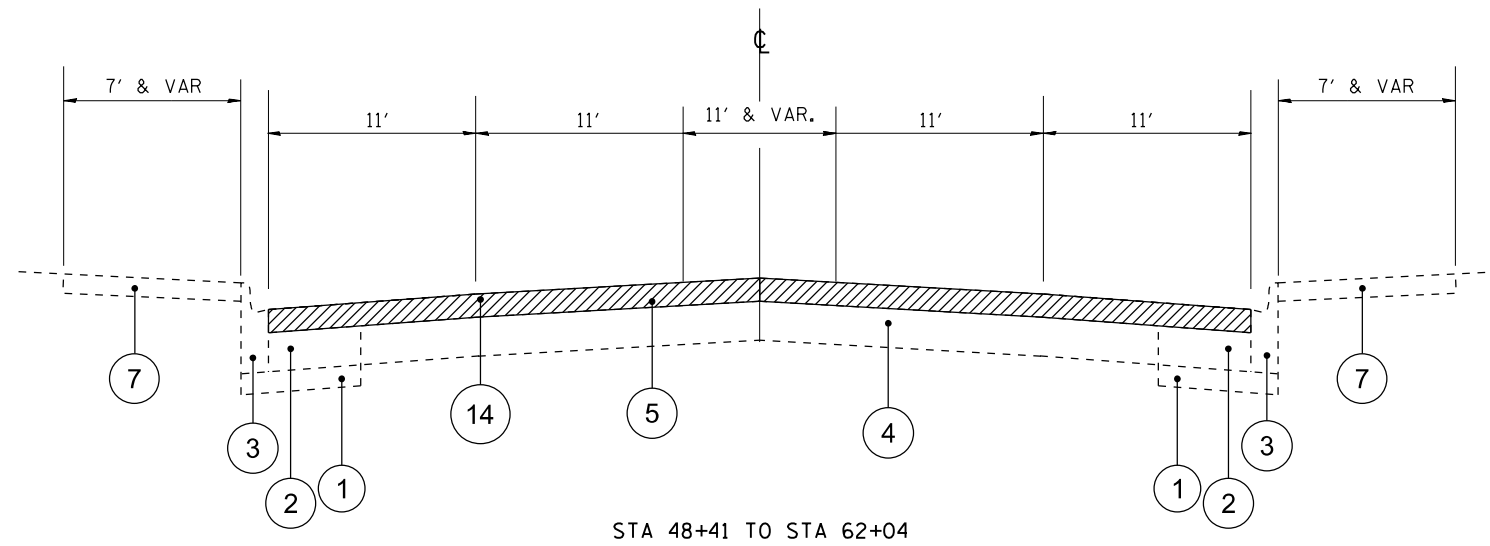
THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 POUND PER SQUARE YARD-INCH

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS / SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

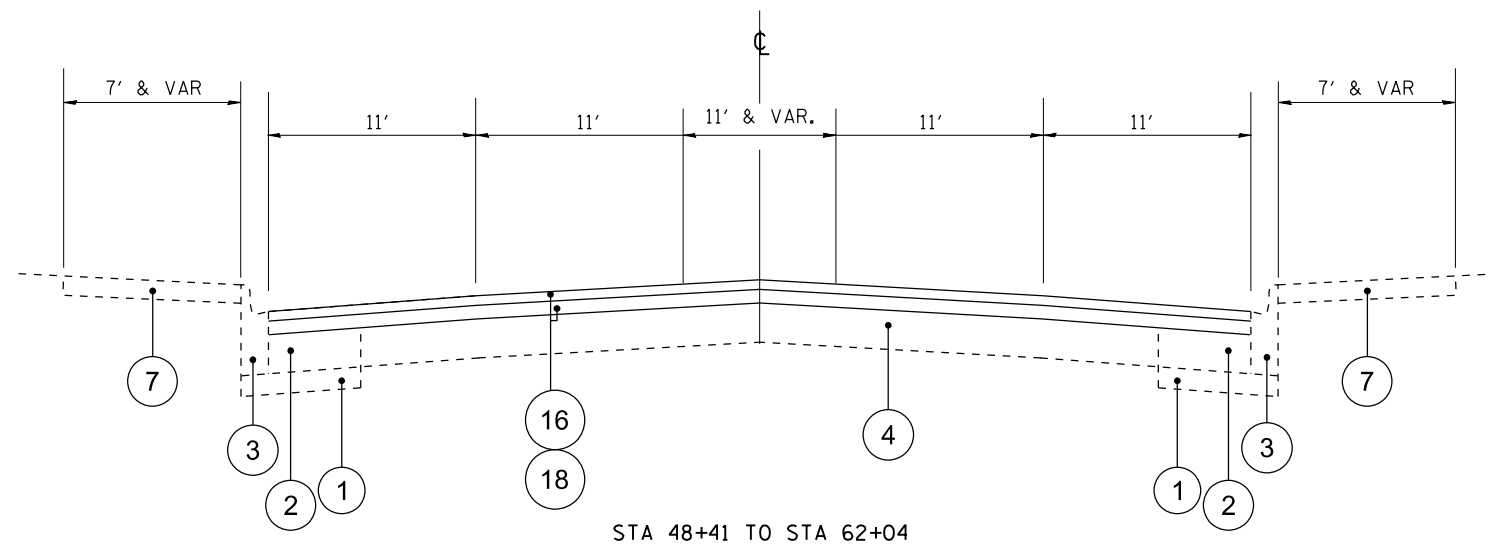
"FOR USE OF RECYCLED MATERIALS SEE DISTRICT ONE SPECIAL PROVISIONS".

QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

- ① EXIST. SUB-BASE GRANULAR MATERIAL, 4"
- ② EXIST. PCC BASE COURSE, 10"
- ③ EXIST. PCC COMB. CURB & GUTTER, B.6-24
- ④ EXIST. PCC PAVEMENT, 10"
- ⑤ EXIST. HMA SURFACE COURSE, 3"
- ⑥ EXIST. AGGREGATE SUBGRADE, 12"
- ⑦ EXIST. PCC SIDEWALK, 5"
- ⑧ EXIST. PCC COMB. CURB & GUTTER, B.6-12
- ⑧A EXIST. PCC GUTTER FLAG
- ⑨ EXIST. HMA SURFACE COURSE, 7"
- ⑩ EXIST. PCC PAVEMENT, 9"
- ⑪ EXIST. PCC PARKING LANE
- ⑫ EXIST. PARKING LANE SUBBASE
- ⑬ PROP. HMA SURFACE REMOVAL, 2 1/2"
- ⑭ PROP. HMA SURFACE REMOVAL, 3"
- ⑮ PROP. PCC PAVEMENT MILLING, VARIABLE DEPTH
- ⑯ PROP. POLY. HMA SURF COURSE, MIX "E", N70, 1 3/4"
- ⑰ PROP. POLY LEVELING BINDER, (MM) IL-4.75, N50, 3/4"
- ⑱ PROP. POLY LEVELING BINDER, (MM) IL-4.75, N50, 1 1/4"



STA 48+41 TO STA 62+04



STA 48+41 TO STA 62+04

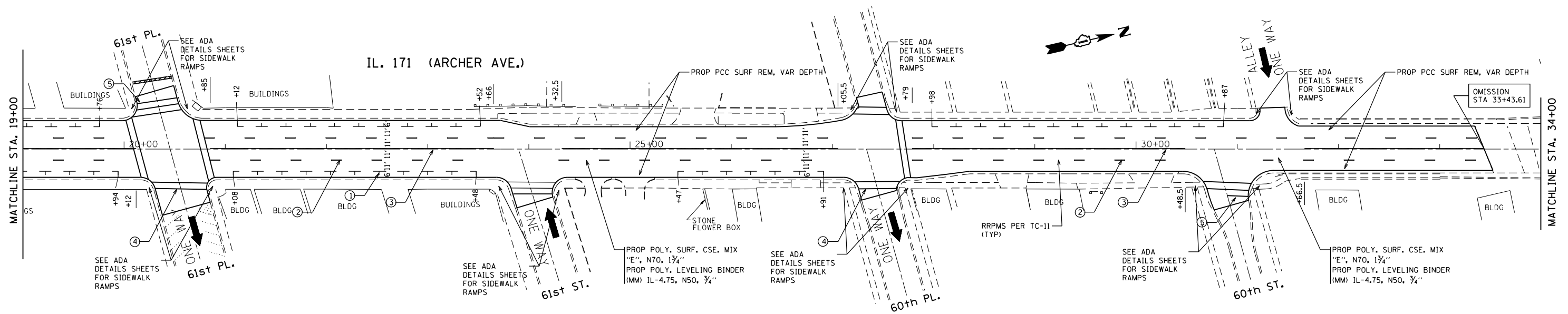
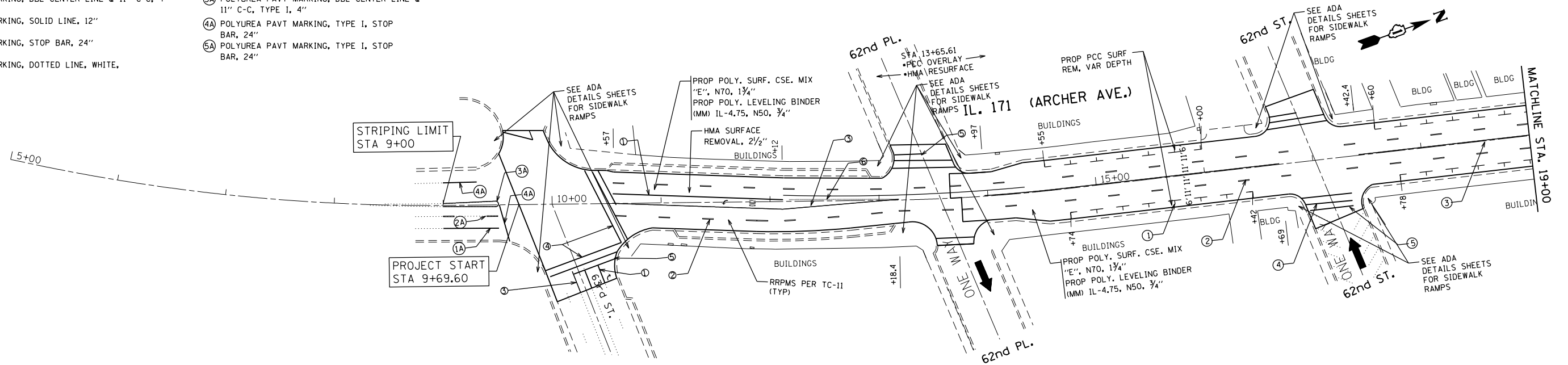
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NOTE: MILLING TO BE DONE BEFORE PATCHING

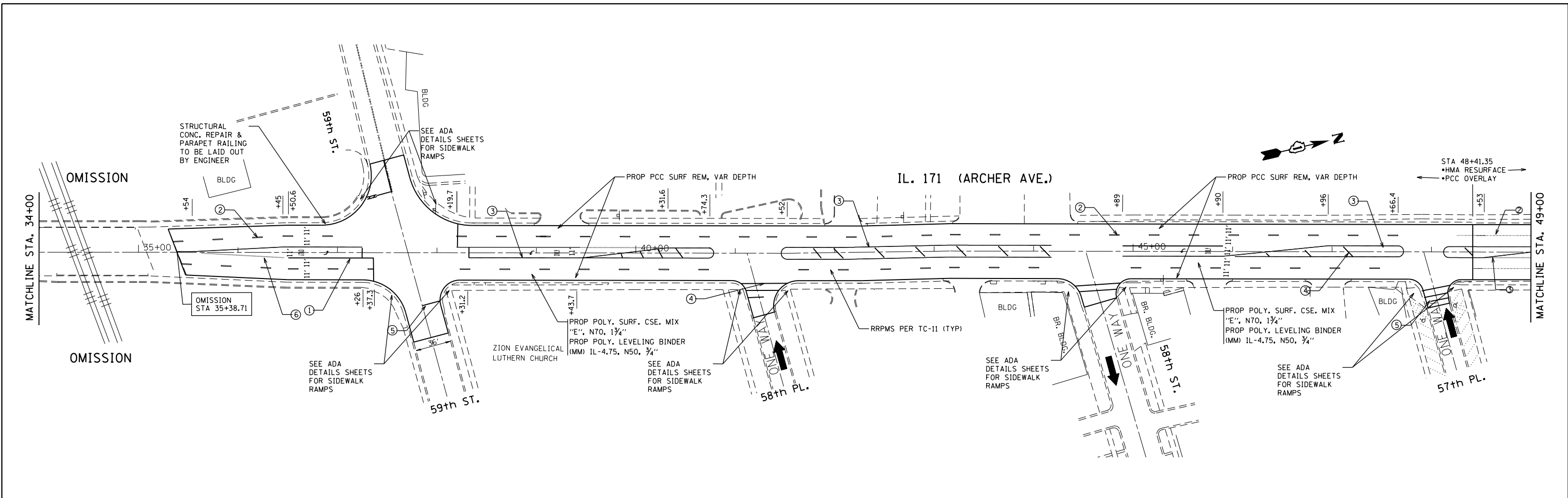
FILE NAME =	USER NAME = ledezmar	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTIONS			F.A.U. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\IL\084EBIDINTEG\illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\DI03115\Drawings\Design\DI03115-shr-typical.dgn		CHECKED -	REVISED -		IL 171 (S. ARCHER ROAD) FROM 55TH PLACE TO 63RD STREET			3565	(46-2)RS	COOK	49	8	
Default	PLOT SCALE = 100.0000' / in.	DATE -	REVISED -		SCALE: 1" = 50'	SHEET	OF	SHEETS	STA.	TO	STA.	CONTRACT NO. 60Y87	
	PLOT DATE = 10/31/2016				ILLINOIS FED. AID PROJECT								

PAVEMENT MARKINGS LEGEND

- ① THPL PAVT MARKING, SOLID LINE, 6"
- ② THPL PAVT MARKING, SKIP DASH, 4"
- ③ THPL PAVT MARKING, DBL CENTER LINE @ 11" C-C, 4"
- ④ THPL PAVT MARKING, SOLID LINE, 12"
- ⑤ THPL PAVT MARKING, STOP BAR, 24"
- ⑥ THPL PAVT MARKING, DOTTED LINE, WHITE, 6"
- ①A POLYUREA PAVT MARKING, TYPE I, SOLID LINE, 6"
- ②A POLYUREA PAVT MARKING, TYPE I, SKIP DASH, 4"
- ③A POLYUREA PAVT MARKING, DBL CENTER LINE @ 11" C-C, TYPE I, 4"
- ④A POLYUREA PAVT MARKING, TYPE I, STOP BAR, 24"
- ⑤A POLYUREA PAVT MARKING, TYPE I, STOP BAR, 24"

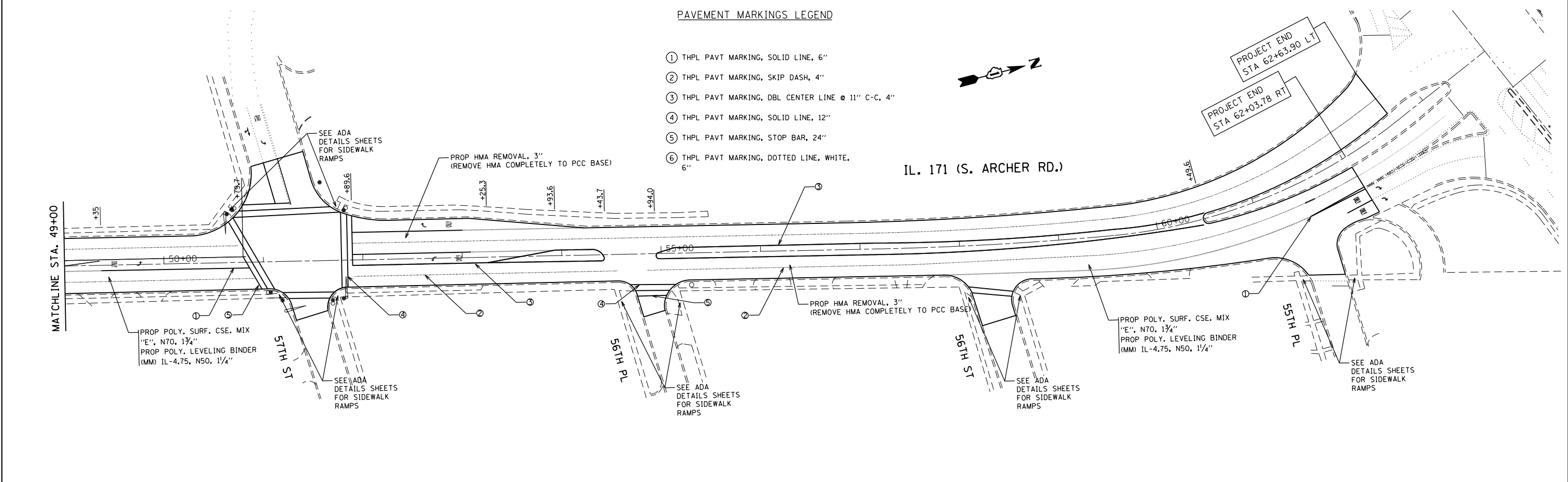


FILE NAME =	USER NAME = ledeznorm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL. ROUTE 171 (57th PL to 63rd PL.) ROADWAY PLANS	F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\IL\084EBIDINTEG\illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\DI03115\Drawings\Design\DI03115-shr-plan.dgn		DRAWN -	REVISED -			3565	(46-2)RS	COOK	49	9	
Default	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -			CONTRACT NO. 60Y87					
	PLOT DATE = 10/31/2016	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



PAVEMENT MARKINGS LEGEND

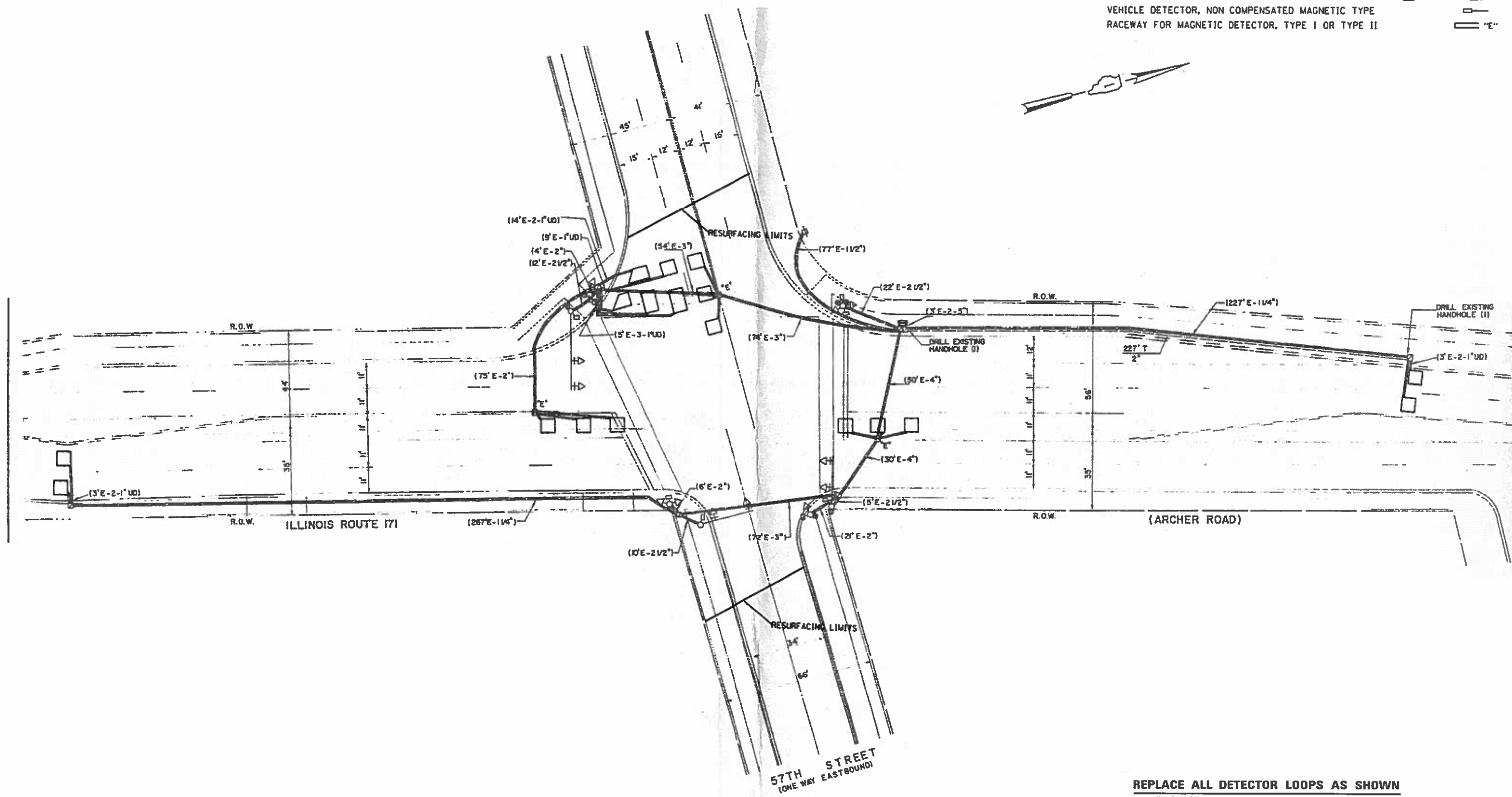
- ① THPL PAVT MARKING, SOLID LINE, 6"
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- ④ THPL PAVT MARKING, SOLID LINE, 12"
- ⑤ THPL PAVT MARKING, STOP BAR, 24"
- ⑥ THPL PAVT MARKING, DOTTED LINE, WHITE, 6"



FILE NAME =	USER NAME = ldezmarm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL. ROUTE 171 (57th PL to 63rd PL) ROADWAY PLANS			F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG\illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\DI03115\Drawings\Design\DI03115-shr-plan.dgn		CHECKED -	REVISED -					3565	(46-2)RS	COOK	49	10
PLOT SCALE = 100.0000' / 1"		DATE -	REVISED -		CONTRACT NO. 60Y87			ILLINOIS FED. AID PROJECT				
Default					SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD		
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED		
DETECTOR LOOP		
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		



**THIS PLAN IS FOR THE SOLE PURPOSE OF
DETECTOR LOOP REPLACEMENTS ONLY**

**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	QUANTITY	UNIT	ITEM
88600700	540	FOOT	PREFORMED DETECTOR LOOP

FILE NAME *	USER NAME *	DESIGNED -	REVISED -
S:\WP\Design\JC\Detector Loop Details\NE	curryja	YB7\IL 171 at 57th St\Detector Loop.dgn	REVISED -
		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

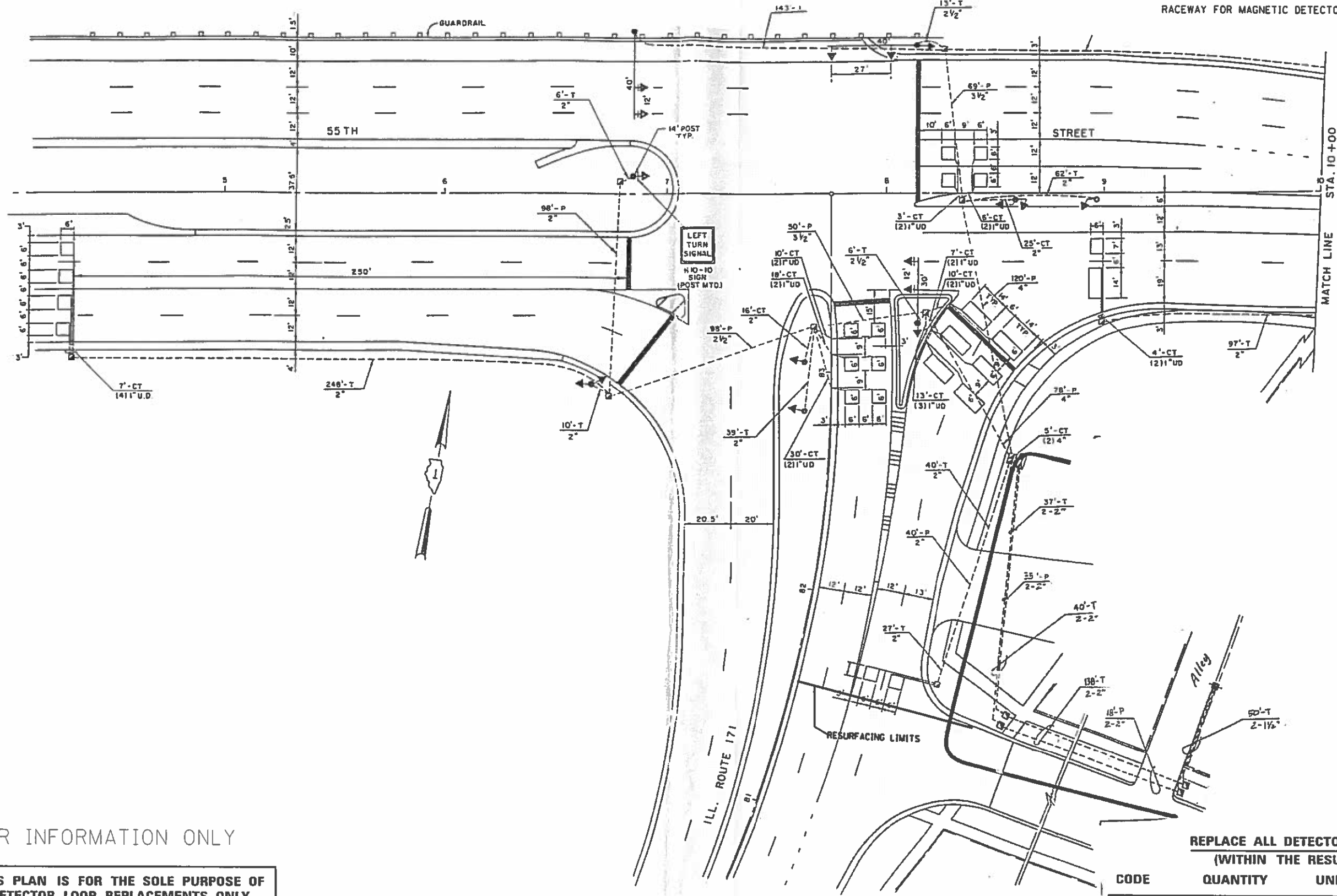
**DISTRICT ONE - DETECTOR LOOP REPLACEMENT
IL 171 (ARCHER RD) AND 57TH STREET**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3565	146-21R5	COOK	49	11
FED. ROAD DIST. NO. ILLINOIS/FED. AID PROJECT			CONTRACT NO. 60YB7	

SCALE: NTS SHEET NO. OF SHEETS STA. TO STA.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD		
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED		
DETECTOR LOOP		
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		



FOR INFORMATION ONLY

**THIS PLAN IS FOR THE SOLE PURPOSE OF
DETECTOR LOOP REPLACEMENTS ONLY**

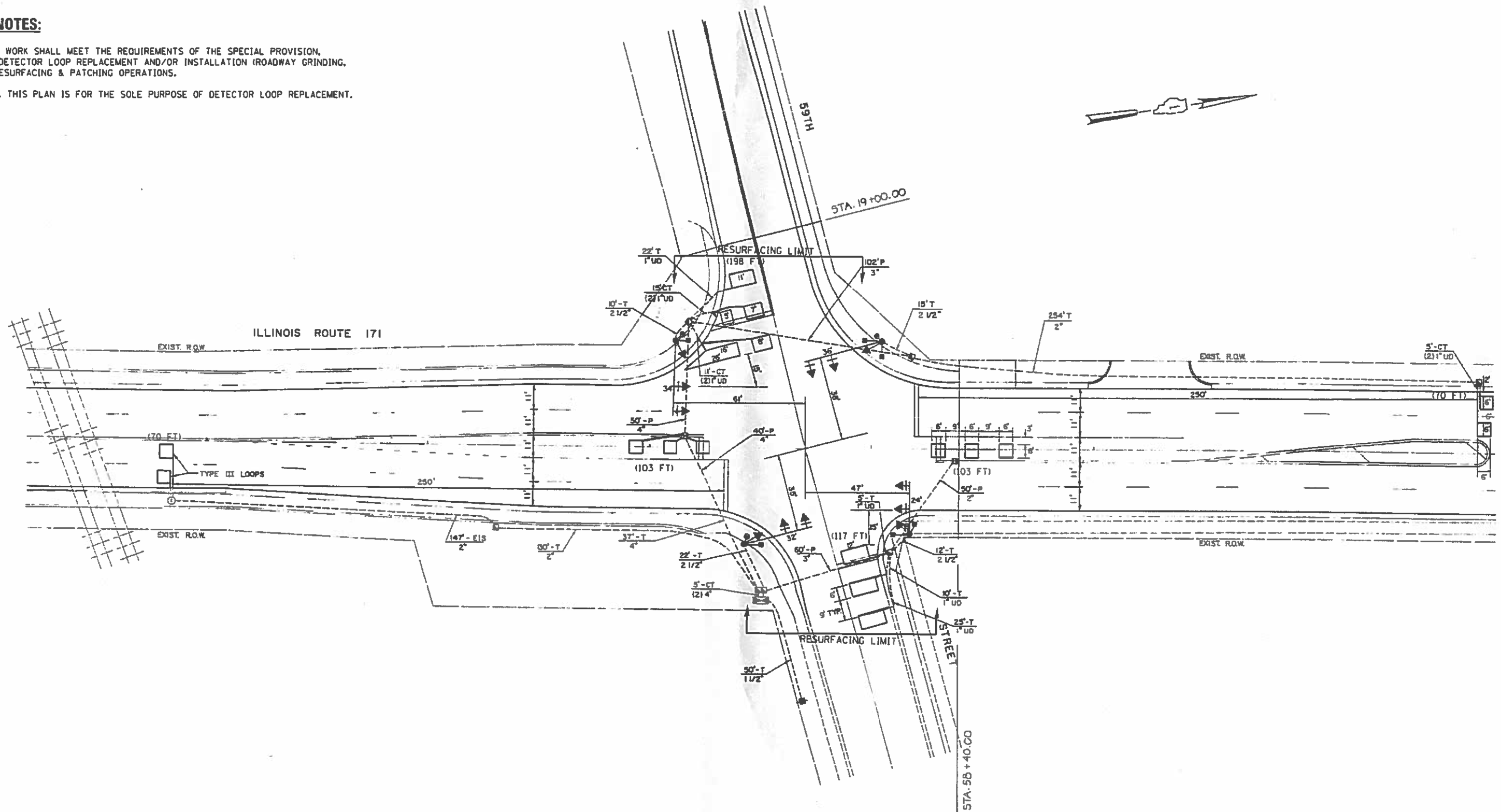
**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	QUANTITY	UNIT	ITEM
B8600700	FOOT	PREFORMED DETECTOR LOOP	

FILE NAME * S:\WP\Design\JCN\Detector Loop Details\6	USER NAME * curryjo	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE - DETECTOR LOOP REPLACEMENT IL 171 (ARCHER RD) AND 55TH STREET	F.A.U. RTE. 3565	SECTION 146-21R5	COUNTY COOK	TOTAL SHEETS 49	SHEET NO. 12	
	PLOT SCALE * 1/8"=1'-0"	DRAWN -	REVISED -			SCALE: NTS	SHEET NO. OF SHEETS	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO. 60YB7	
	PLOT DATE * 1/12/2015	CHECKED -	REVISED -			STA.	TO STA.				
		DATE	REVISED								

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

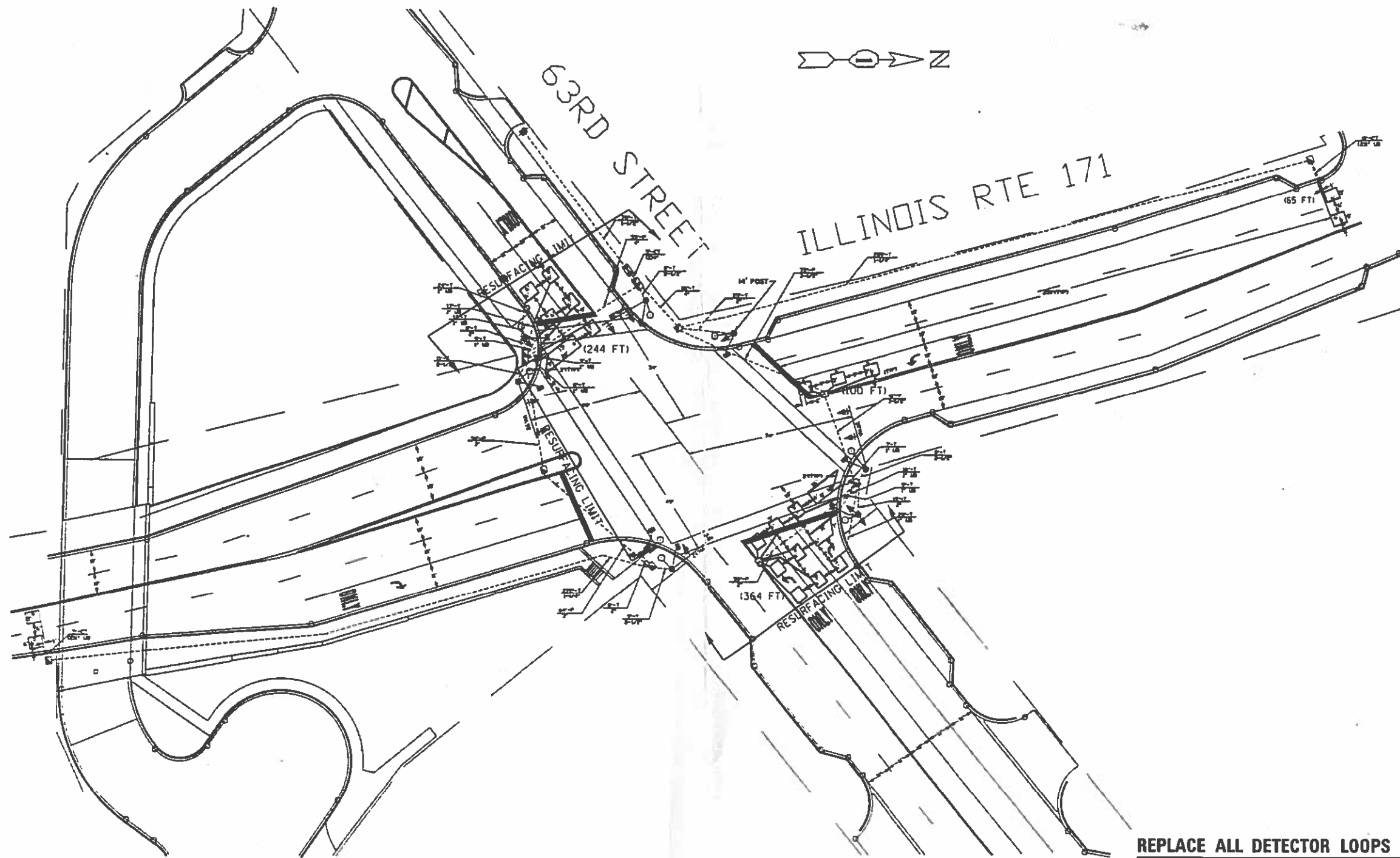


**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	661	FOOT

NOTES:

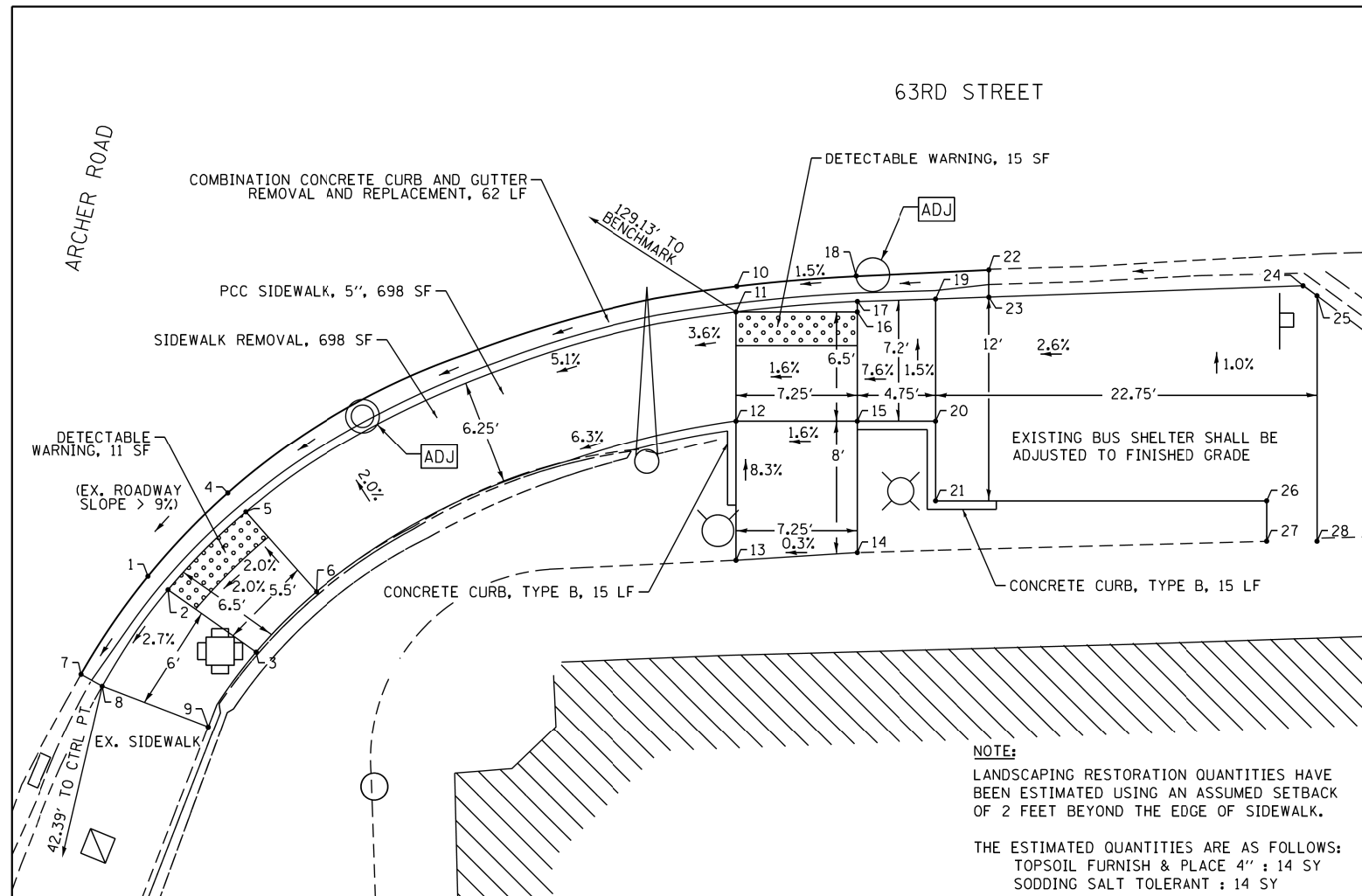
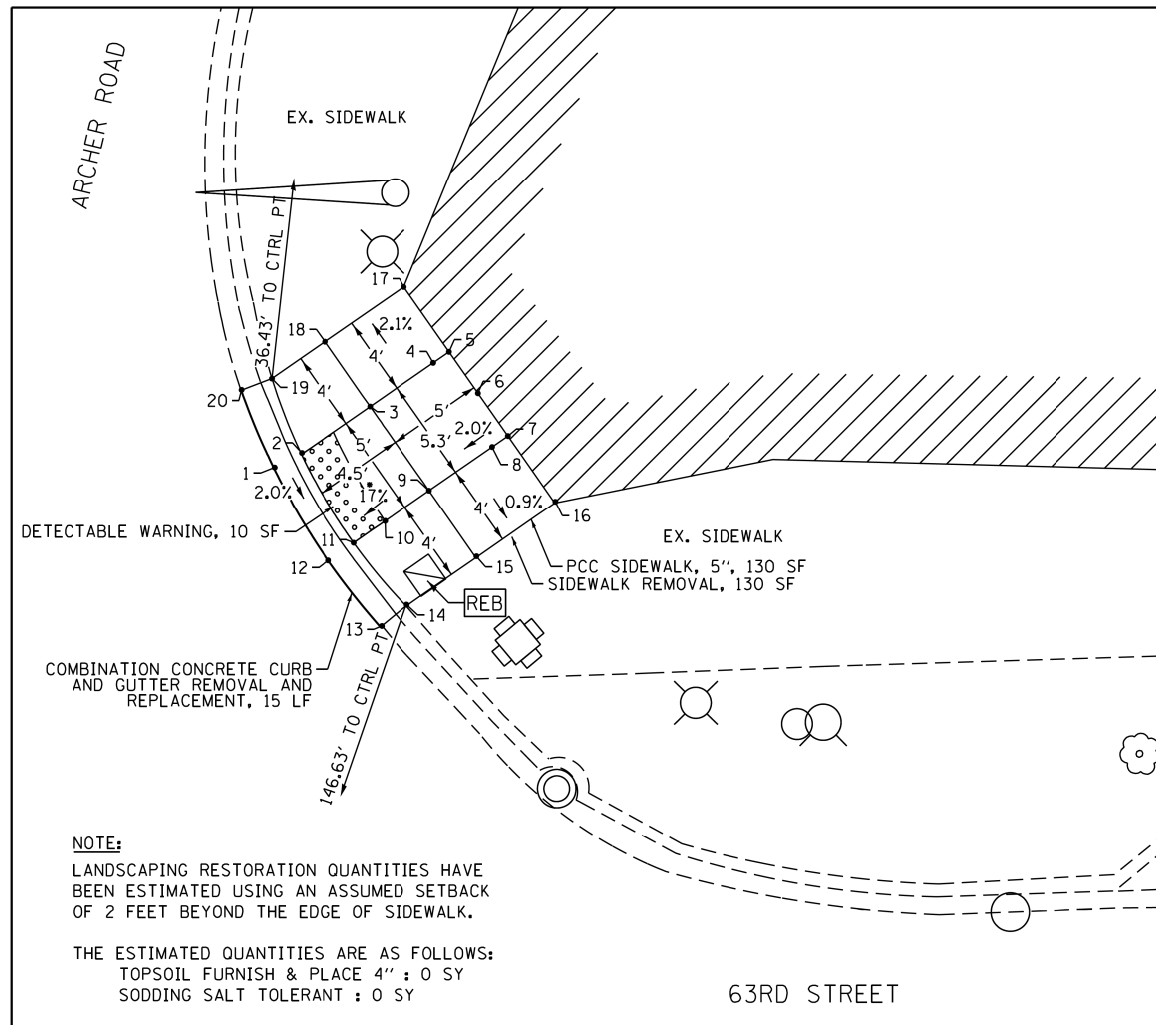
1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	773	FOOT

FILE NAME: S:\MP\Design\JC\Detector Loop Details 2	USER NAME: jcurry	DESIGNED: ---	REVISED: ---	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETECTOR LOOP REPLACEMENT PLAN IL 171 AT 63RD ST.				F.A.U. RTE.:	SECTION:	COUNTY:	TOTAL SHEETS:	SHEET NO.:
	16 6078 Detector Loop Replacement Sheet, Blar	DRAWN: E.g.m	REVISED: ---		3565	146-21RS	COOK	49	14				
	PLOT SCALE: 40.0000 1/4 in.	CHECKED: ---	REVISED: ---		SCALE: SHEET OF SHEETS STA. TO STA.				CONTRACT NO. 60Y87				
Default	PLOT DATE: 10/13/2016	DATE: ---	REVISED: ---		ILLINOIS FED. AID PROJECT								



CONTROL POINTS:
CUT CROSS ELEV: 612.54
STA: 9+17.67
OS: 43.20' RT
N: 1861672.780
E: 1125899.742

NORTHEAST CORNER
ARCHER ROAD AND 63RD STREET

POINT No.:	STATION	OFFSET	ELEV.
1	10+68.73	35.08' RT	612.80
2	10+70.04	36.04' RT	612.79
3	10+73.57	38.61' RT	613.55
4	10+76.80	40.97' RT	613.63
5	10+77.61	41.56' RT	613.65
6	10+76.04	43.73' RT	613.67
7	10+74.47	45.89' RT	613.69
8	10+73.66	45.30' RT	613.67
9	10+70.44	42.94' RT	613.59
10	10+68.19	41.30' RT	612.83
11	10+66.58	40.12' RT	612.80
12	10+65.27	39.17' RT	612.81
13	10+62.88	42.88' RT	MATCH EX.
14	10+64.37	43.79' RT	MATCH EX.
15	10+67.94	46.40' RT	MATCH EX.
16	10+71.98	49.35' RT	MATCH EX.
17	10+80.02	38.23' RT	MATCH EX.
18	10+75.98	35.28' RT	MATCH EX.
19	10+73.26	33.29' RT	MATCH EX.
20	10+72.17	31.98' RT	MATCH EX.

• IN ORDER TO MATCH INTO EXISTING GRADES WITHOUT THE USE OF STEPS, RAMP IS DESIGNED TO BE OUT OF COMPLIANCE. (REFER TO SUBMITTED BDE FORM 3101 FOR MORE INFORMATION)

CONTROL POINTS:
CUT CROSS ELEV: 612.54
STA: 9+17.67
OS: 43.20' RT
N: 1861672.780
E: 1125899.742

SOUTHEAST CORNER
ARCHER ROAD AND 63RD STREET

POINT No.:	STATION	OFFSET	ELEV.
1	9+66.37	35.45' RT	609.05
2	9+65.97	36.83' RT	609.06
3	9+64.18	42.98' RT	609.60
4	9+72.62	38.33' RT	609.53
5	9+71.86	39.68' RT	609.54
6	9+68.75	45.20' RT	609.67
7	9+59.53	33.60' RT	MATCH EX.
8	9+59.27	35.01' RT	MATCH EX.
9	9+59.04	41.76' RT	MATCH EX.
10	9+94.14	62.92' RT	611.28
11	9+92.71	63.41' RT	611.27
12	9+86.58	65.54' RT	611.41
13	9+78.73	68.26' RT	MATCH EX.
14	9+81.52	74.92' RT	MATCH EX.

SOUTHEAST CORNER
ARCHER ROAD AND 63RD STREET

POINT No.:	STATION	OFFSET	ELEV.
15	9+88.94	72.34' RT	611.52
16	9+95.07	70.22' RT	611.39
17	9+95.65	70.01' RT	611.38
18	9+97.07	69.52' RT	611.39
19	9+97.33	74.40' RT	611.74
20	9+90.48	76.78' RT	611.85
21	9+85.96	78.35' RT	611.92
22	10+00.01	76.84' RT	MATCH EX.
23	9+98.48	77.37' RT	MATCH EX.
24	10+05.26	94.85' RT	MATCH EX.
25	10+04.97	95.83' RT	MATCH EX.
26	9+92.44	97.00' RT	612.43
27	9+90.17	97.79' RT	MATCH EX.
28	9+91.15	100.62' RT	MATCH EX.



USER NAME = WTeng
DESIGNED -
DRAWN -
PLOT SCALE = 10.0000' / in.
PLOT DATE = 10/17/2016

DESIGNED -
DRAWN -
CHECKED - MTC
DATE - 09/05/2016

REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

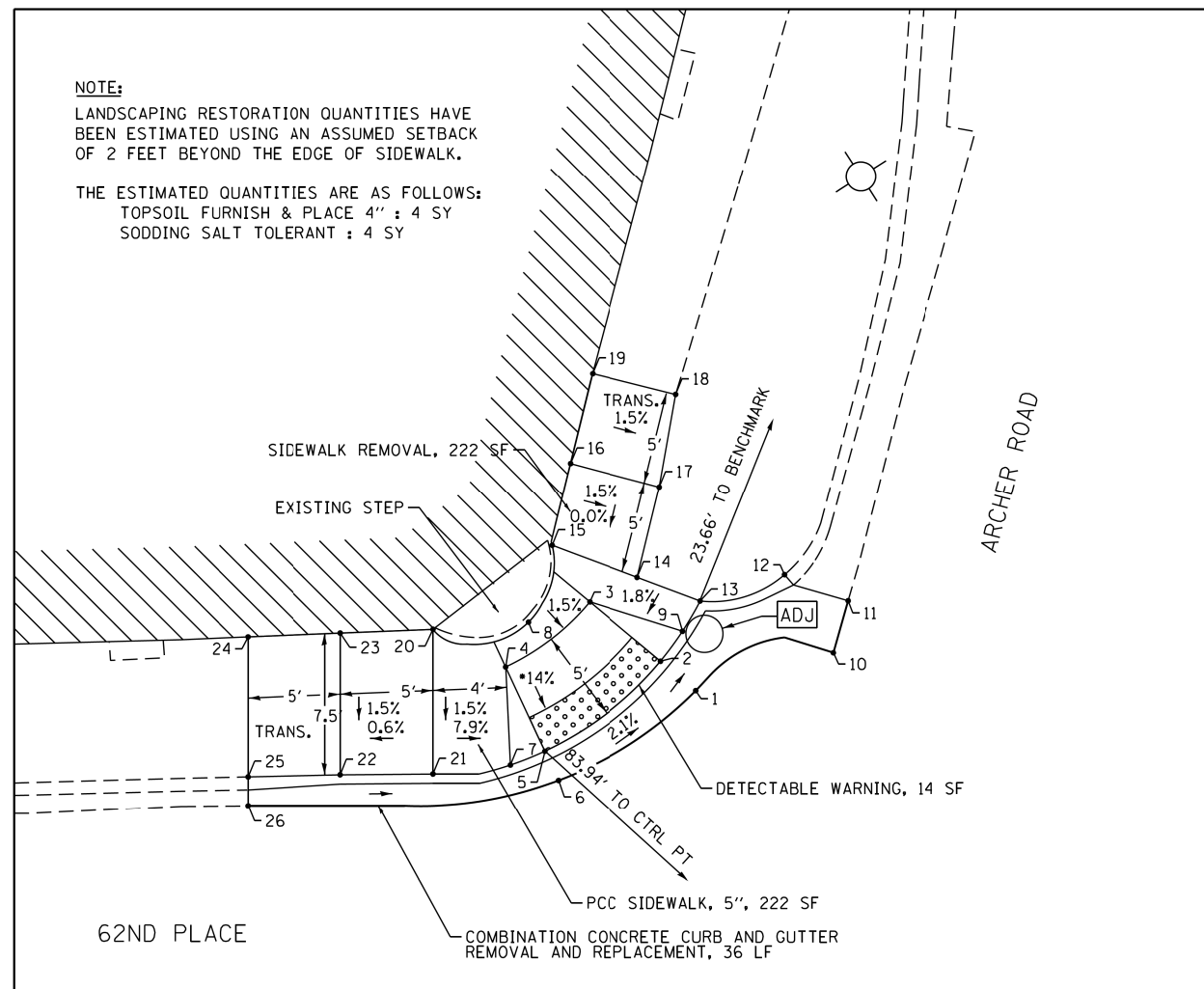
ARCHER ROAD
ADA RAMP DETAILS
SCALE: 1" = 5'
SHEET 2 OF 23 SHEETS
STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3565	(46-2)RS	COOK	49	16
CONTRACT NO. 60Y87				
ILLINOIS FED. AID PROJECT				

NOTE:

LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.

THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 4 SY
 SODDING SALT TOLERANT : 4 SY



CONTROL POINTS:
 BENCHMARK LIGHTPOLE FND. ELEV: 613.32 STA: 14+06.08 OS: 26.18' LT N: 1862156.364 E: 1125990.806
 CUT CROSS ELEV: 613.08 STA: 13+15.28 OS: 28.40' RT N: 1862053.457 E: 1126016.120

• IN ORDER TO MATCH INTO EXISTING GRADES WITHOUT THE USE OF STEPS, RAMP IS DESIGNED TO BE OUT OF COMPLIANCE. (REFER TO SUBMITTED BDE FORM 3101 FOR MORE INFORMATION)

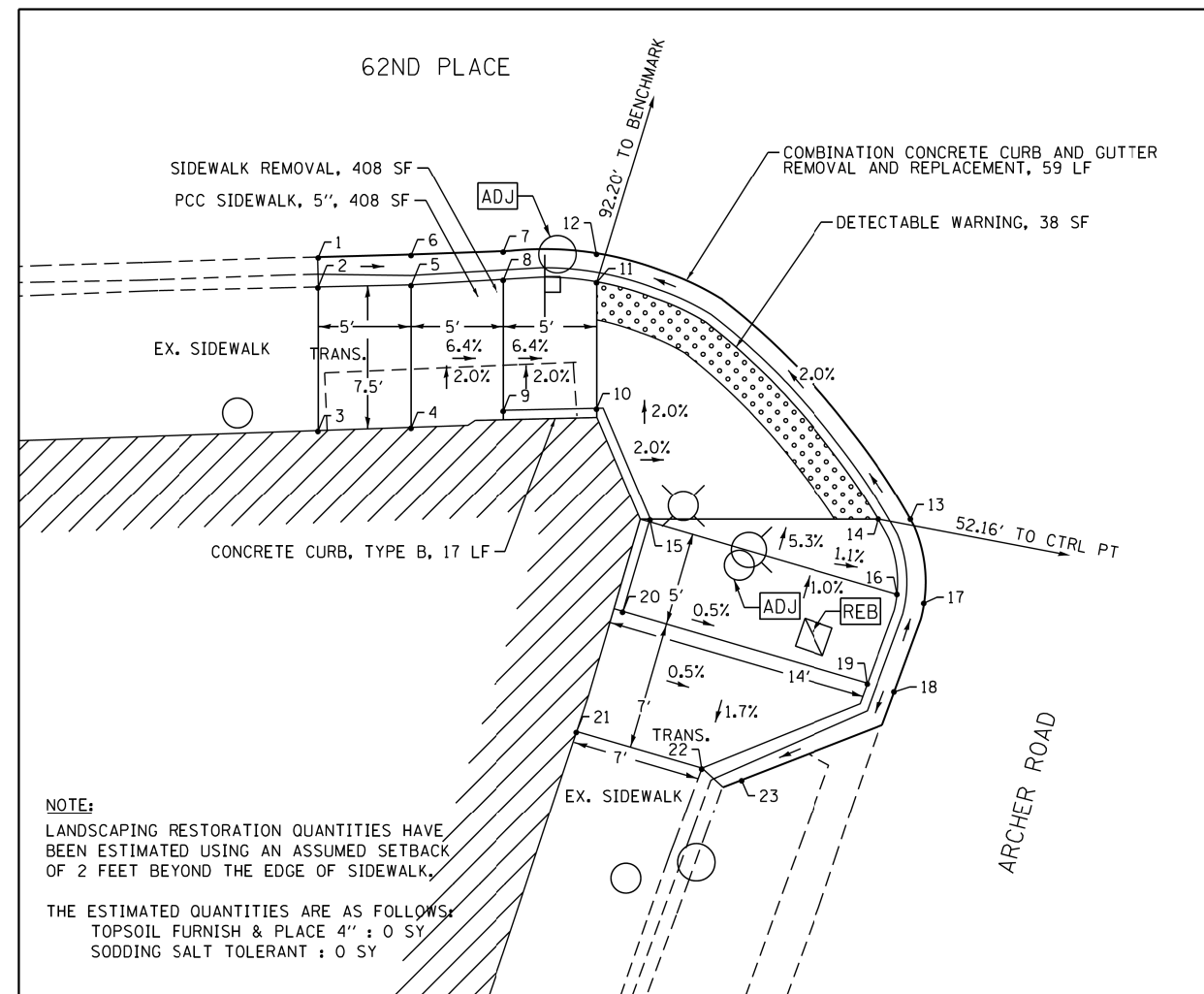
NORTHWEST CORNER
 ARCHER ROAD AND 62ND PLACE

POINT No.:	STATION	OFFSET	ELEV.
1	13+77.25	26.96' LT	612.42
2	13+78.24	29.25' LT	612.41
3	13+80.24	33.85' LT	613.28
4	13+75.49	37.20' LT	613.29
5	13+71.68	33.87' LT	612.58
6	13+70.35	32.71' LT	612.57
7	13+70.40	35.43' LT	613.02
8	13+78.22	36.71' LT	MATCH EX.
9	13+80.16	28.56' LT	612.85
10	13+81.37	20.43' LT	MATCH EX.
11	13+84.32	20.45' LT	MATCH EX.
12	13+84.71	24.14' LT	MATCH EX.
13	13+82.02	28.11' LT	612.92

NORTHWEST CORNER
 ARCHER ROAD AND 62ND PLACE

POINT No.:	STATION	OFFSET	ELEV.
14	13+82.28	31.74' LT	613.22
15	13+82.82	36.77' LT	MATCH EX.
16	13+87.28	36.96' LT	MATCH EX.
17	13+87.41	31.97' LT	613.22
18	13+92.57	32.50' LT	MATCH EX.
19	13+92.39	37.15' LT	MATCH EX.
20	13+76.33	41.52' LT	MATCH EX.
21	13+68.69	39.28' LT	613.36
22	13+67.16	44.05' LT	613.35
23	13+74.66	46.25' LT	MATCH EX.
24	13+72.98	50.98' LT	MATCH EX.
25	13+65.54	48.80' LT	MATCH EX.
26	13+64.00	48.34' LT	MATCH EX.

62ND PLACE



NOTE:

LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.

THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 0 SY
 SODDING SALT TOLERANT : 0 SY

CONTROL POINTS:
 BENCHMARK LIGHTPOLE FND. ELEV: 613.32 STA: 14+06.08 OS: 26.18' LT N: 1862156.364 E: 1125990.806
 CUT CROSS ELEV: 613.08 STA: 13+15.28 OS: 28.40' RT N: 1862053.457 E: 1126016.120

SOUTHWEST CORNER
 ARCHER ROAD AND 62ND PLACE

POINT No.:	STATION	OFFSET	ELEV.
1	13+09.49	56.06' LT	MATCH EX.
2	13+07.93	55.50' LT	MATCH EX.
3	13+00.46	52.96' LT	MATCH EX.
4	13+02.35	48.32' LT	613.29
5	13+09.73	50.81' LT	613.13
6	13+11.30	51.34' LT	612.62
7	13+13.12	46.65' LT	612.53
8	13+11.68	46.18' LT	612.81
9	13+04.86	43.89' LT	612.97
10	13+06.65	39.21' LT	612.65
11	13+13.21	41.40' LT	612.51
12	13+14.69	41.89' LT	612.52
13	13+06.50	21.29' LT	612.82
14	13+05.92	22.96' LT	612.81
15	13+01.88	34.55' LT	613.07
16	13+02.41	20.68' LT	613.05
17	13+02.49	19.17' LT	612.82
18	12+97.49	19.12' LT	612.83
19	12+97.41	26.62' LT	613.10
20	12+96.73	34.31' LT	613.17
21	12+89.84	34.54' LT	MATCH EX.
22	12+90.18	27.55' LT	MATCH EX.
23	12+90.30	25.33' LT	MATCH EX.



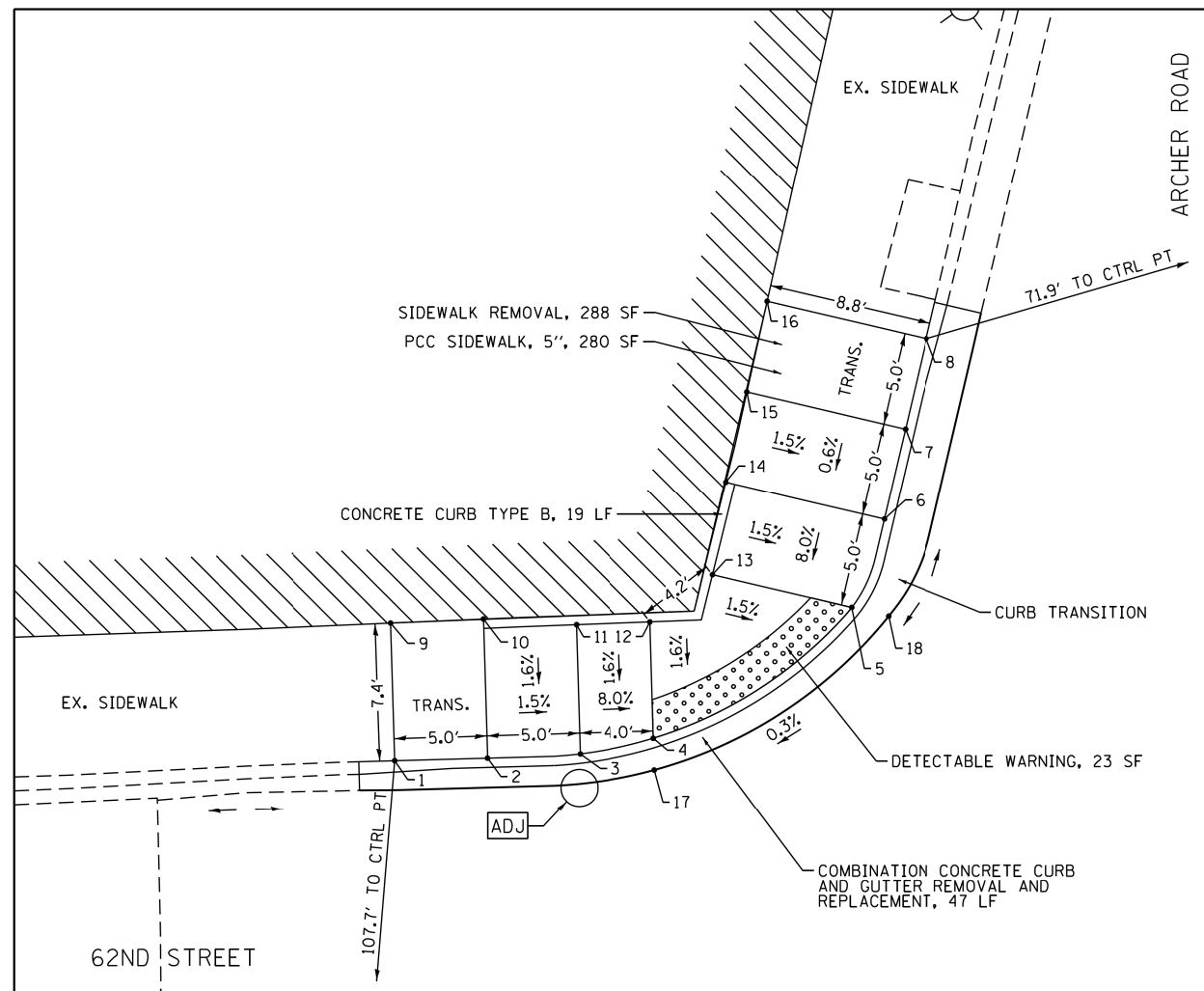
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PLOT SCALE = 10.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 10/17/2016	CHECKED - MTC	REVISED -
	DATE - 09/05/2016	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ARCHER ROAD
 ADA RAMP DETAILS

SCALE: 1" = 5' SHEET 3 OF 23 SHEETS STA. TO STA.

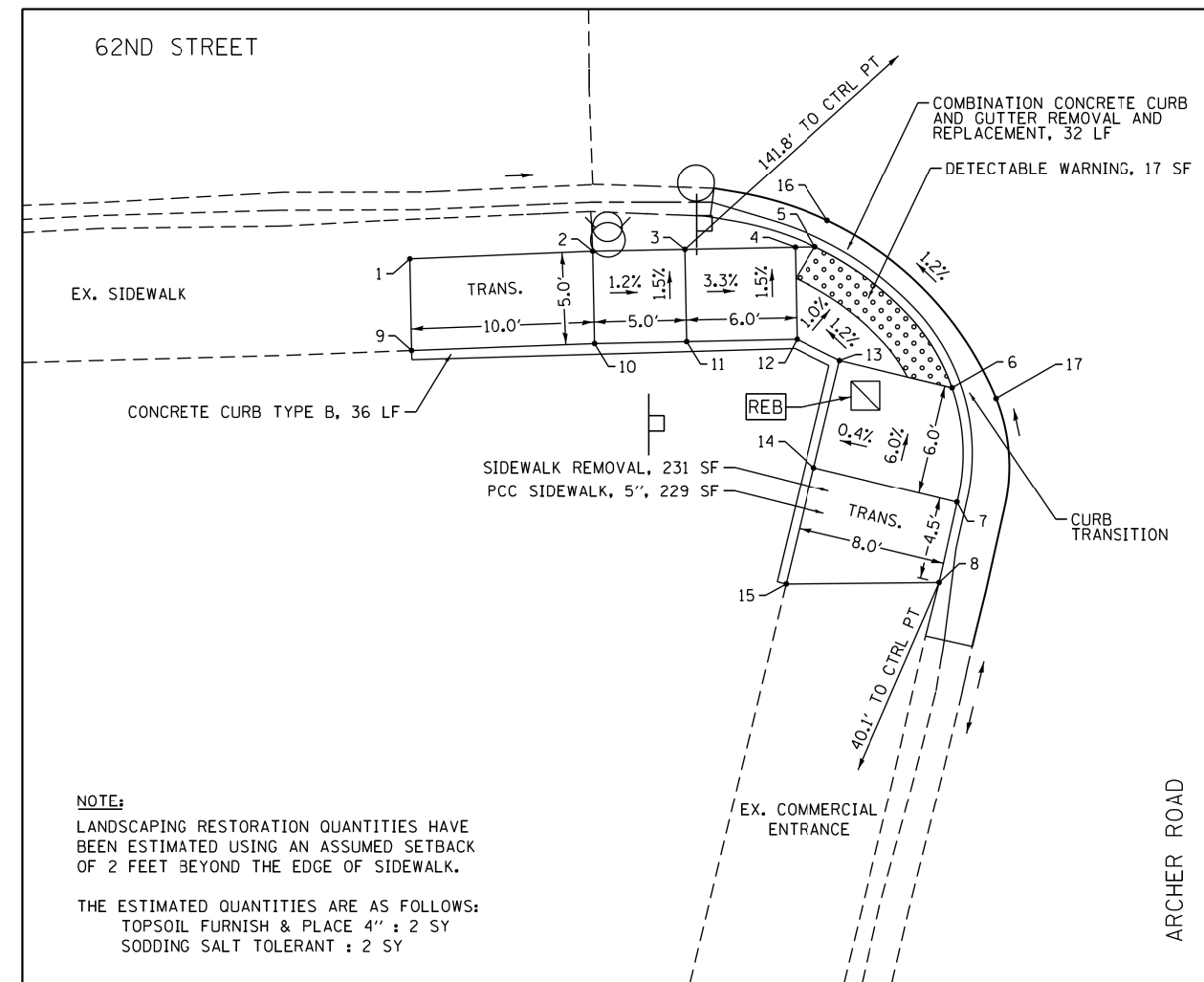
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3565	(46-2)RS	COOK	49	17
CONTRACT NO. 60Y87				
ILLINOIS FED. AID PROJECT				



CONTROL POINTS:
 CUT CROSS ELEV: 612.89 STA: 16+06.95 OS: 36.34' LT N: 1862352.860 E: 1126028.107
 CUT CROSS ELEV: 613.15 STA: 17+77.72 OS: 33.37' RT N: 1862503.038 E: 1126135.199

NORTHWEST CORNER
 ARCHER ROAD AND 62ND STREET

POINT No.	STATION	OFFSET	ELEV.
1	17+13.49	51.95' LT	MATCH EX.
2	17+14.77	47.11' LT	613.08
3	17+16.14	42.30' LT	613.00
4	17+17.88	38.63' LT	612.69
5	17+27.19	29.82' LT	612.73
6	17+32.24	29.19' LT	613.12
7	17+37.24	29.20' LT	613.15
8	17+42.24	29.21' LT	MATCH EX.
9	17+20.65	53.86' LT	MATCH EX.
10	17+22.01	49.05' LT	MATCH EX.
11	17+22.86	44.10' LT	613.11
12	17+23.93	40.25' LT	612.79
13	17+27.18	37.54' LT	612.85
14	17+32.22	37.99' LT	MATCH EX.
15	17+37.22	38.01' LT	MATCH EX.
16	17+42.22	38.06' LT	MATCH EX.
17	17+16.23	38.18' LT	612.70
18	17+27.20	27.77' LT	612.75



NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 2 SY
 SODDING SALT TOLERANT : 2 SY

CONTROL POINTS:
 CUT CROSS ELEV: 612.89 STA: 16+06.95 OS: 36.34' LT N: 1862352.860 E: 1126028.107
 CUT CROSS ELEV: 613.15 STA: 17+77.72 OS: 33.37' RT N: 1862503.038 E: 1126135.199

SOUTHWEST CORNER
 ARCHER ROAD AND 62ND STREET

POINT No.	STATION	OFFSET	ELEV.
1	16+56.91	61.38' LT	MATCH EX.
2	16+59.61	51.75' LT	612.70
3	16+60.86	46.91' LT	612.64
4	16+62.35	41.10' LT	612.44
5	16+62.63	40.04' LT	612.44
6	16+56.90	32.31' LT	612.57
7	16+50.90	32.33' LT	612.93
8	16+45.58	32.36' LT	MATCH EX.
9	16+52.09	60.14' LT	MATCH EX.
10	16+54.76	50.50' LT	612.77
11	16+56.01	45.66' LT	612.71
12	16+57.51	39.85' LT	612.51
13	16+56.92	37.31' LT	612.54
14	16+50.92	37.33' LT	612.90
15	16+44.39	37.37' LT	MATCH EX.
16	16+64.17	39.71' LT	612.45
17	16+56.88	28.54' LT	612.59

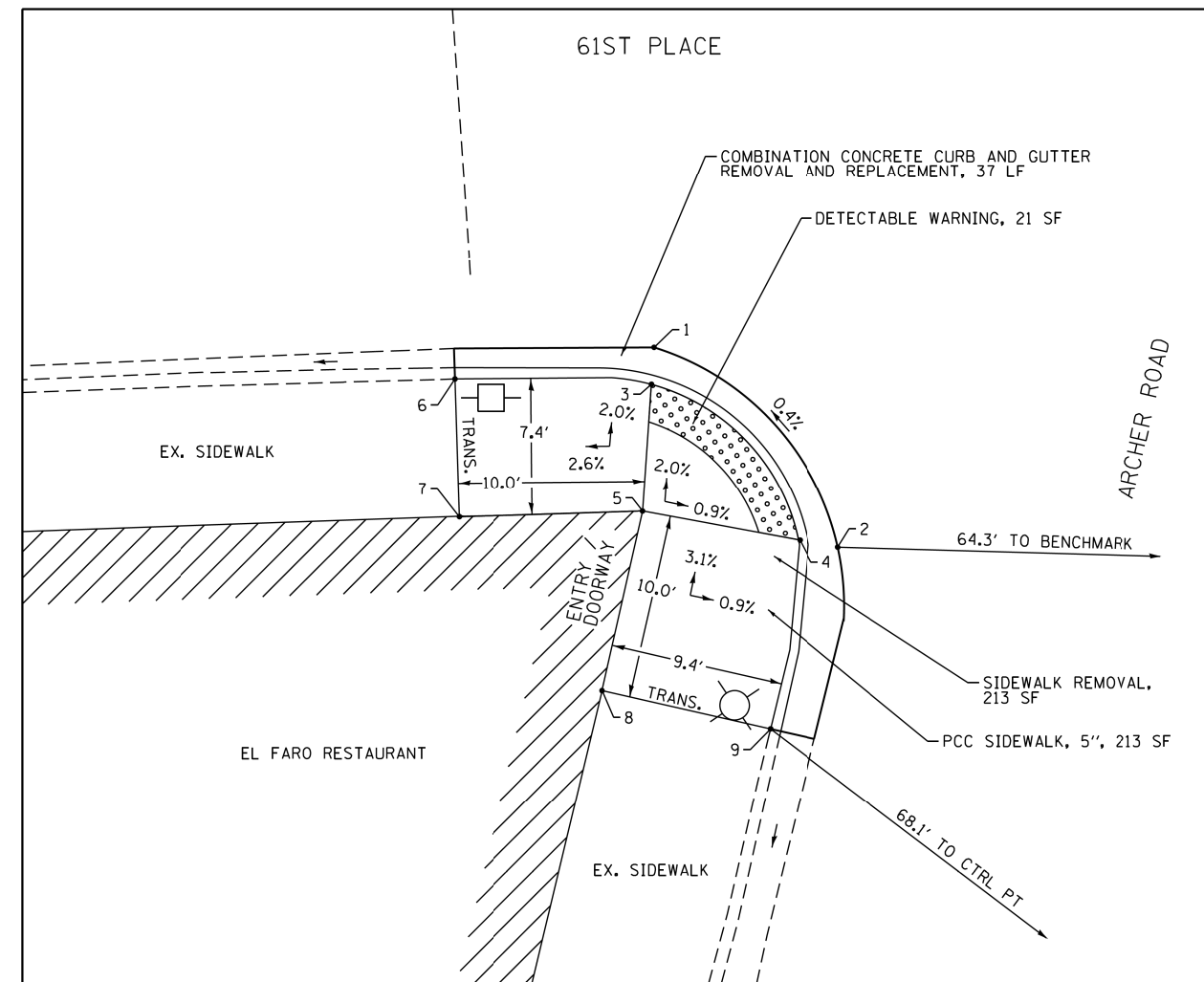
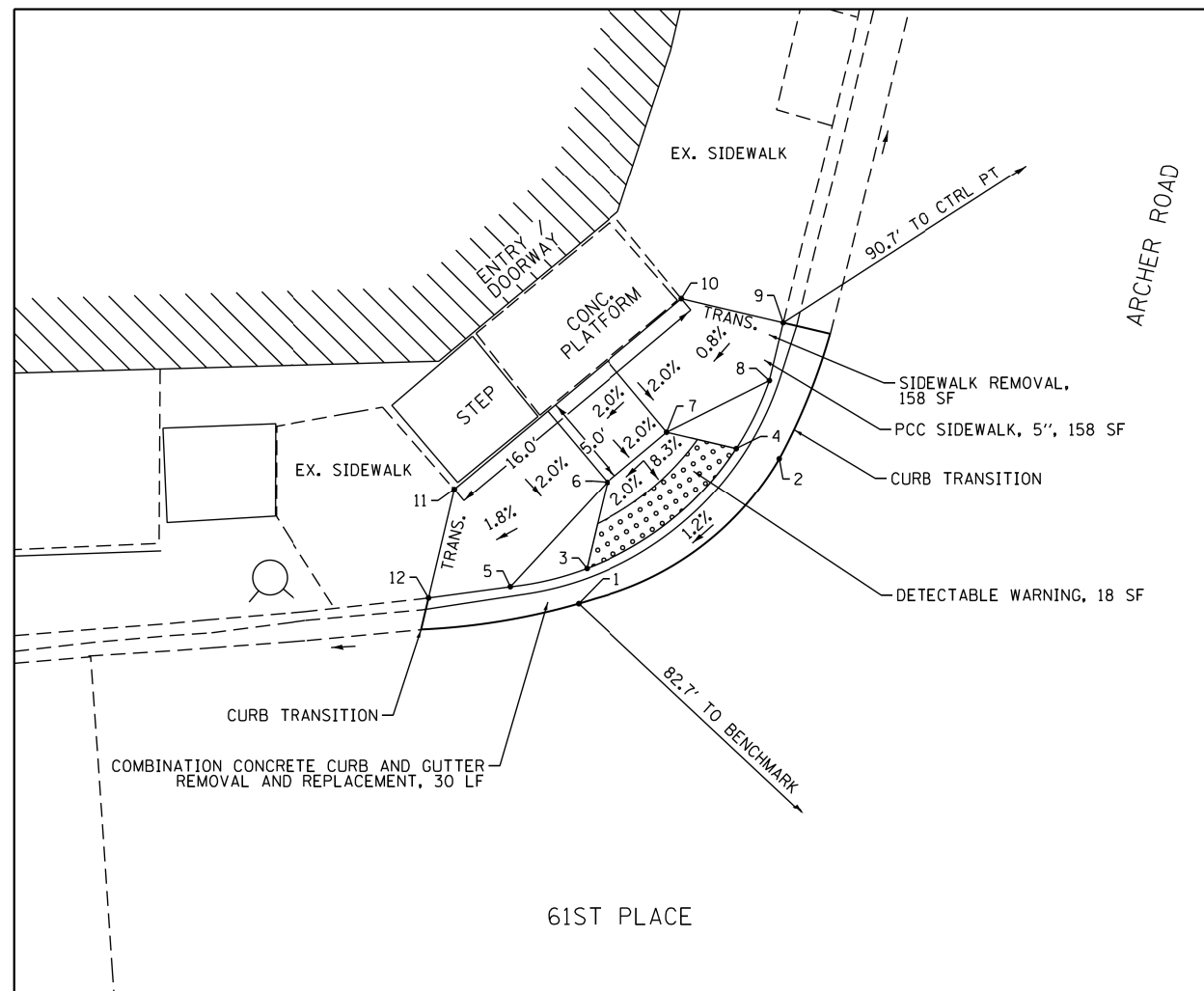


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PLOT SCALE = 10.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 10/17/2016	CHECKED - MTC	REVISED -
	DATE - 09/05/2016	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ARCHER ROAD
 ADA RAMP DETAILS
 SCALE: 1" = 5'
 SHEET 5 OF 23 SHEETS
 STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3565	(46-2)RS	COOK	49	19
CONTRACT NO. 60Y87				
ILLINOIS FED. AID PROJECT				



CONTROL POINTS:
 CUT CROSS ELEV: 613.52
 STA: 21+38.57
 OS: 33.59' RT
 N: 1862854.180
 E: 1126218.335

NORTHWEST CORNER
 ARCHER ROAD AND 61ST PLACE

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	20+55.99	36.67' LT	612.88
2	20+66.12	27.90' LT	613.04
3	20+57.94	36.67' LT	612.85
4	20+66.12	30.29' LT	613.00
5	20+56.04	40.47' LT	613.14
6	20+62.75	36.67' LT	613.24
7	20+66.12	34.15' LT	613.32
8	20+70.09	29.38' LT	613.40
9	20+73.30	29.37' LT	MATCH EX.
10	20+73.30	35.03' LT	MATCH EX.
11	20+60.47	44.61' LT	MATCH EX.
12	20+54.44	44.61' LT	MATCH EX.

CONTROL POINTS:
 BENCHMARK ELEV: 613.69
 STA: 20+15.01
 OS: 35.13' RT
 N: 1862733.570
 E: 1126191.438

SOUTHWEST CORNER
 ARCHER ROAD AND 61ST PLACE

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	20+10.33	40.06' LT	613.00
2	20+02.02	27.80' LT	613.05
3	20+08.34	39.74' LT	613.02
4	20+01.91	30.01' LT	613.07
5	20+01.49	38.62' LT	613.15
6	20+06.16	50.24' LT	MATCH EX.
7	19+98.89	48.27' LT	MATCH EX.
8	19+91.49	38.52' LT	MATCH EX.
9	19+91.58	29.13' LT	MATCH EX.



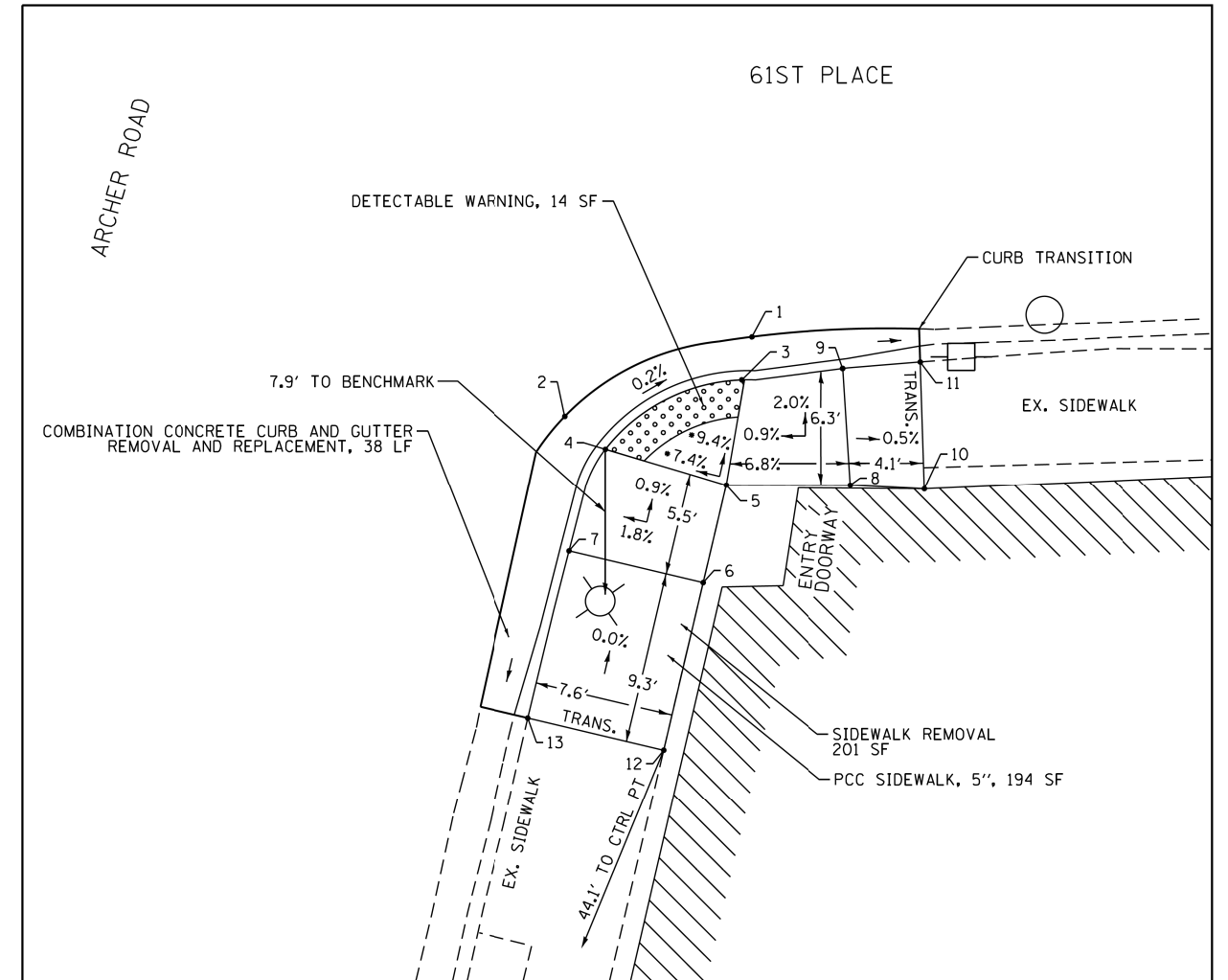
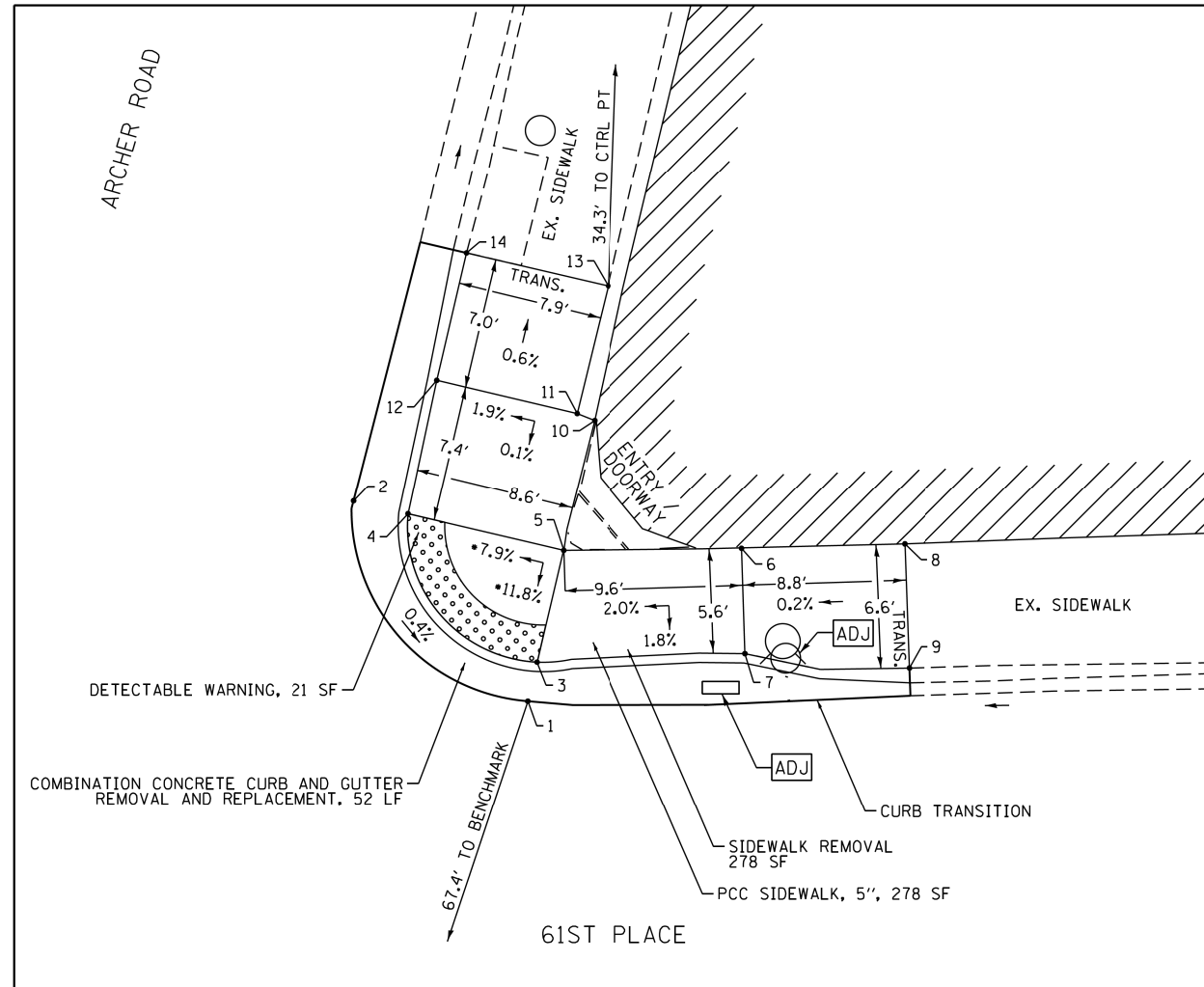
USER NAME = WTeng	DESIGNED -	REVISED -
PLOT SCALE = 10.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 10/17/2016	CHECKED - MTC	REVISED -
	DATE - 09/05/2016	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ARCHER ROAD
 ADA RAMP DETAILS

SCALE: 1" = 5' SHEET 7 OF 23 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3565	(46-2)RS	COOK	49	21
CONTRACT NO. 60Y87				
ILLINOIS FED. AID PROJECT				



CONTROL POINTS:
 CUT CROSS ELEV: 613.52
 STA: 21+38.57
 OS: 33.59' RT
 N: 1862854.180
 E: 1126218.335

BENCHMARK LIGHT POLE FOUNDATION
 ELEV: 613.69
 STA: 20+15.01
 OS: 35.13' RT
 N: 1862733.570
 E: 1126191.438

NORTHEAST CORNER
 ARCHER ROAD AND 61ST PLACE

POINT No.	STATION	OFFSET	ELEV.
1	20+82.11	41.22' RT	613.03
2	20+90.47	29.61' RT	613.08
3	20+84.28	41.22' RT	613.00
4	20+90.46	32.62' RT	613.05
5	20+90.46	41.23' RT	MATCH EX.
6	20+92.80	50.58' RT	613.92
7	20+87.31	52.07' RT	613.81
8	20+95.04	59.11' RT	MATCH EX.
9	20+88.59	60.89' RT	MATCH EX.
10	20+97.69	41.24' RT	MATCH EX.
11	20+97.84	40.24' RT	MATCH EX.
12	20+97.84	32.47' RT	613.52
13	21+04.91	40.30' RT	MATCH EX.
14	21+04.88	32.45' RT	MATCH EX.

• IN ORDER TO MATCH INTO EXISTING GRADES WITHOUT THE USE OF STEPS, RAMP IS DESIGNED TO BE OUT OF COMPLIANCE. (REFER TO SUBMITTED BDE FORM 3101 FOR MORE INFORMATION)

CONTROL POINTS:
 BENCHMARK ELEV: 613.69
 STA: 20+15.01
 OS: 35.13' RT
 N: 1862733.570
 E: 1126191.438

CUT CROSS ELEV: 613.21
 STA: 19+63.96
 OS: 33.15' RT
 N: 1862684.346
 E: 1126177.779

SOUTHEAST CORNER
 ARCHER ROAD AND 61ST PLACE

POINT No.	STATION	OFFSET	ELEV.
1	20+30.47	39.64' RT	613.15
2	20+23.94	30.76' RT	613.17
3	20+28.08	39.64' RT	613.12
4	20+22.71	33.32' RT	613.14
5	20+22.31	40.14' RT	MATCH EX.
6	20+16.86	40.15' RT	MATCH EX.
7	20+16.86	32.67' RT	613.37
8	20+23.86	46.73' RT	MATCH EX.
9	20+29.94	44.87' RT	613.57
10	20+24.59	50.70' RT	MATCH EX.
11	20+31.26	48.90' RT	MATCH EX.
12	20+07.52	40.15' RT	MATCH EX.
13	20+07.52	32.58' RT	MATCH EX.

• IN ORDER TO MATCH INTO EXISTING GRADES WITHOUT THE USE OF STEPS, RAMP IS DESIGNED TO BE OUT OF COMPLIANCE. (REFER TO SUBMITTED BDE FORM 3101 FOR MORE INFORMATION)



USER NAME = WTeng	DESIGNED -	REVISED -
PLOT SCALE = 10.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 10/17/2016	CHECKED - MTC	REVISED -
	DATE - 09/05/2016	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ARCHER ROAD
 ADA RAMP DETAILS

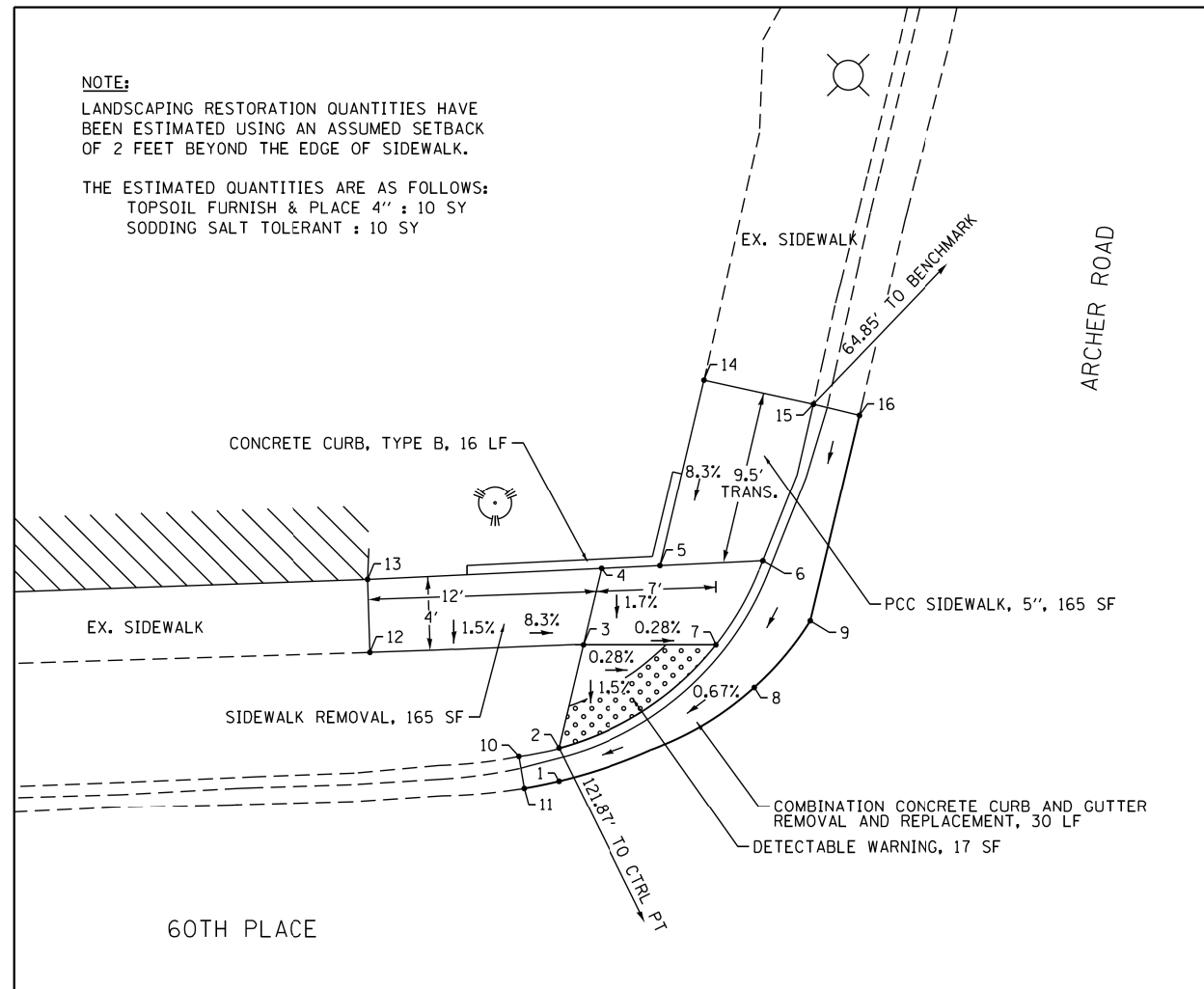
SCALE: 1" = 5' SHEET 8 OF 23 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3565	(46-2)RS	COOK	49	22
CONTRACT NO. 60Y87				
ILLINOIS FED. AID PROJECT				

NOTE:

LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.

THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 10 SY
 SODDING SALT TOLERANT : 10 SY



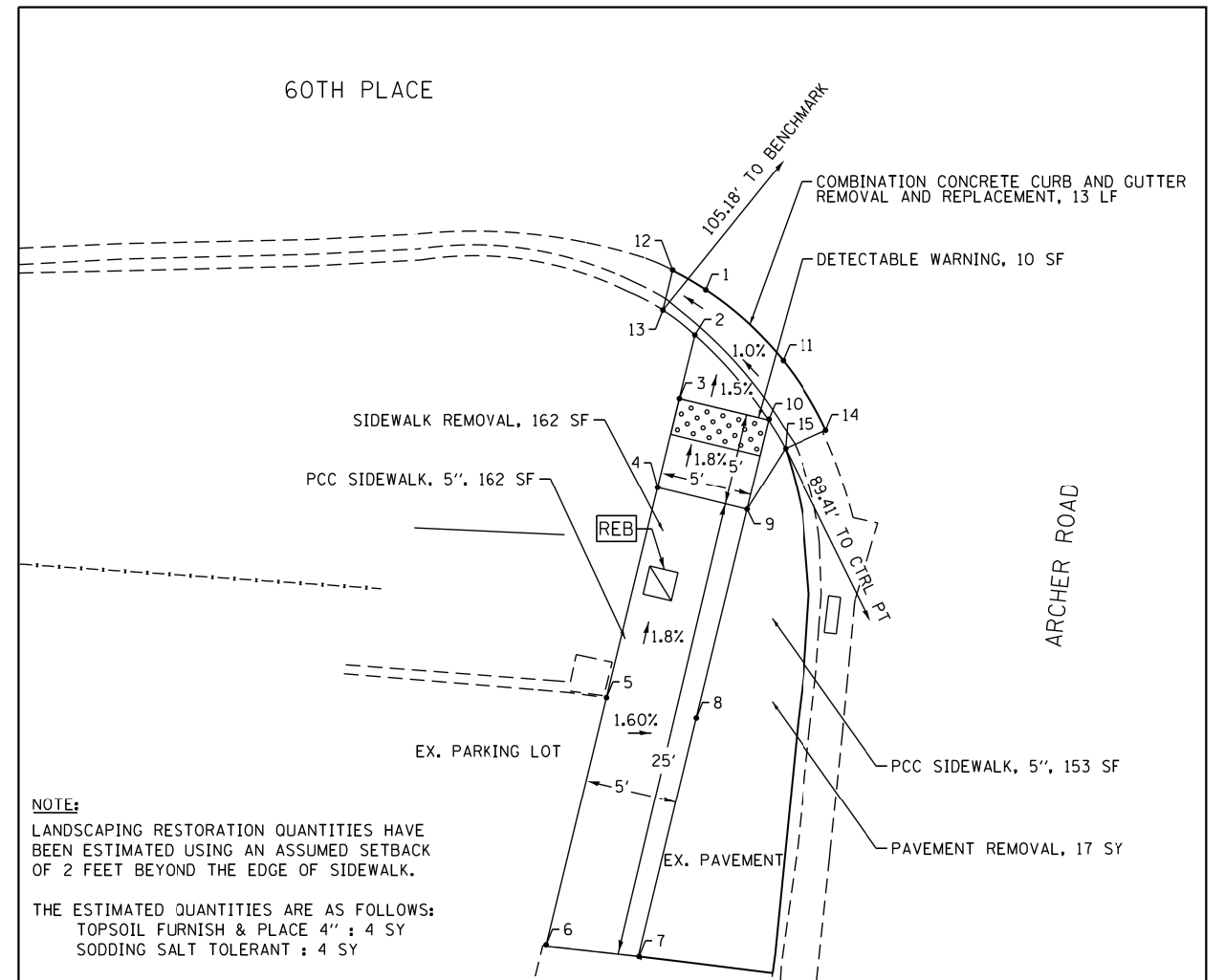
CONTROL POINTS:
 CUT CROSS ELEV: 614.21
 STA: 26+62.50
 OS: 39.46' RT
 N: 1863362.738
 E: 1126344.459

BENCHMARK LIGHTPOLE FND.
 ELEV: 614.13
 STA: 27+96.64
 OS: 32.94' RT
 N: 1863494.784
 E: 1126368.934

**NORTHWEST CORNER
 ARCHER ROAD AND 60TH PLACE**

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	27+54.68	37.79' LT	613.25
2	27+56.39	38.21' LT	613.24
3	27+62.13	38.20' LT	613.33
4	27+66.36	38.20' LT	613.39
5	27+67.24	35.12' LT	613.39
6	27+68.78	29.78' LT	613.39
7	27+63.78	31.21' LT	613.31
8	27+62.03	28.66' LT	613.30
9	27+66.18	26.54' LT	613.33
10	27+55.49	40.21' LT	MATCH EX.
11	27+53.88	39.53' LT	MATCH EX.
12	27+59.10	49.30' LT	MATCH EX.
13	27+62.89	50.32' LT	MATCH EX.
14	27+77.55	35.11' LT	MATCH EX.
15	27+77.64	29.07' LT	MATCH EX.
16	27+77.63	26.51' LT	MATCH EX.

60TH PLACE



NOTE:

LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.

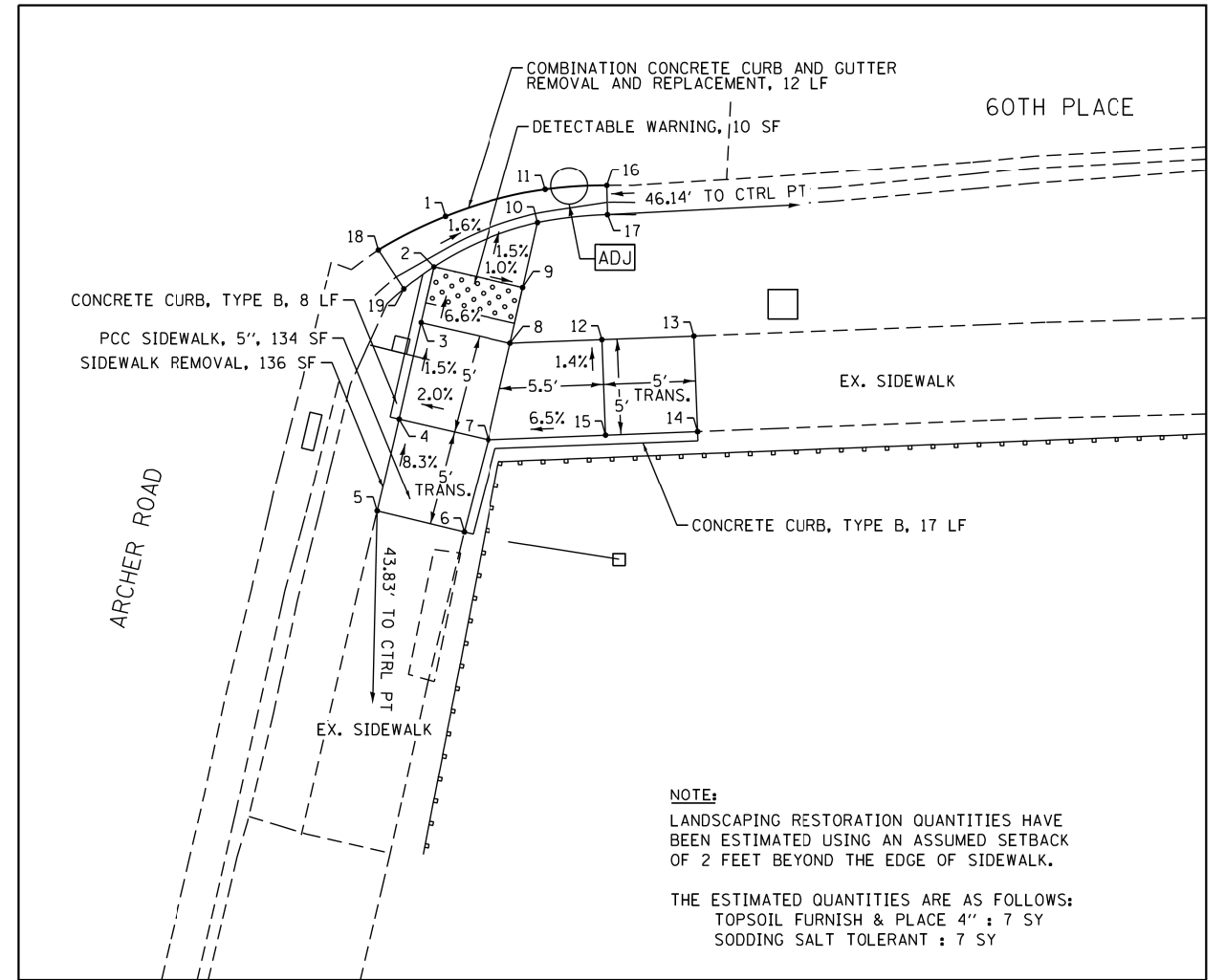
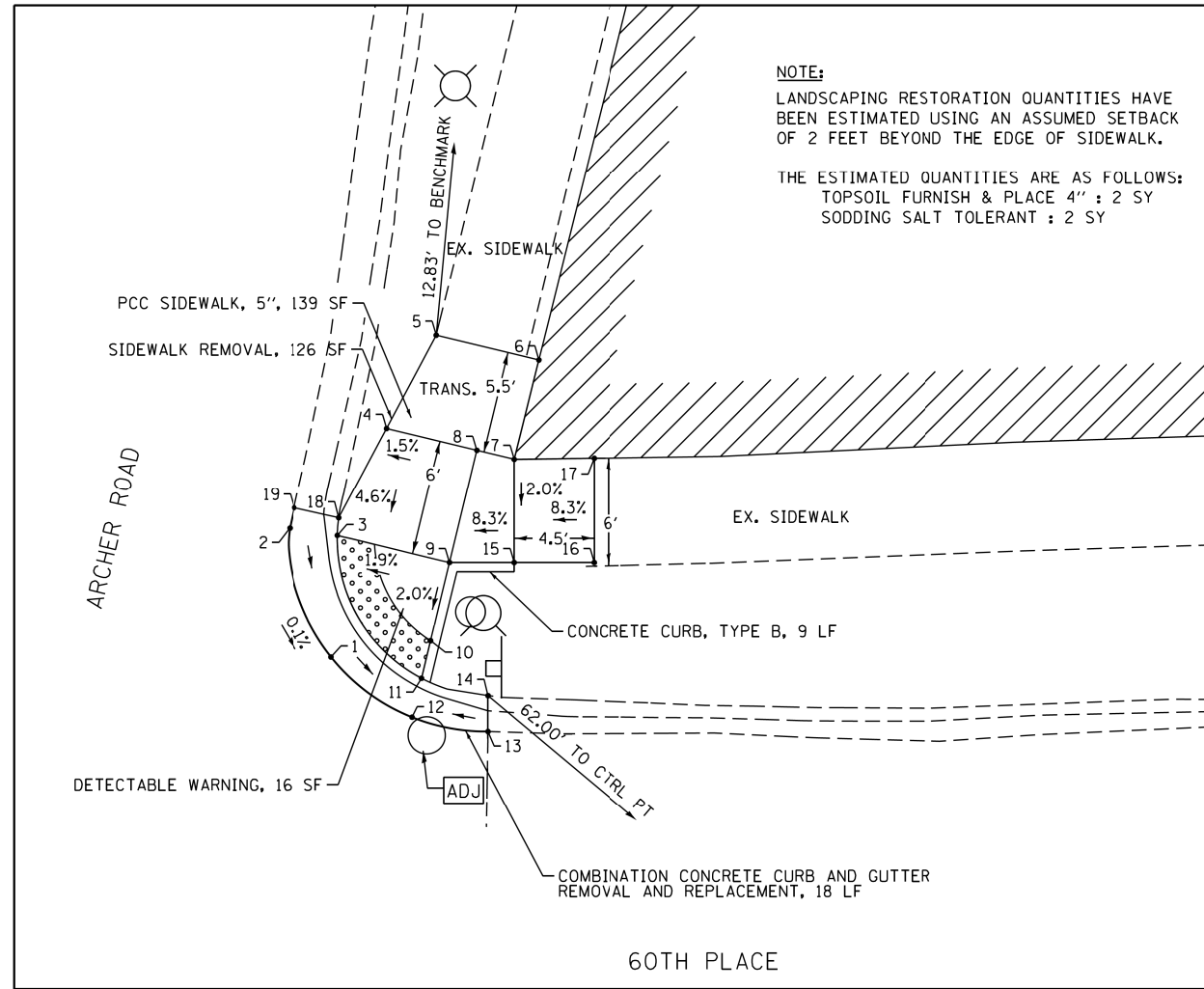
THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 4 SY
 SODDING SALT TOLERANT : 4 SY

CONTROL POINTS:
 CUT CROSS ELEV: 614.21
 STA: 26+62.50
 OS: 39.46' RT
 N: 1863362.738
 E: 1126344.459

BENCHMARK LIGHTPOLE FND.
 ELEV: 614.13
 STA: 27+96.64
 OS: 32.94' RT
 N: 1863494.784
 E: 1126368.934

**SOUTHWEST CORNER
 ARCHER ROAD AND 60TH PLACE**

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	27+22.97	38.56' LT	613.10
2	27+20.45	38.58' LT	613.04
3	27+16.90	38.60' LT	613.09
4	27+11.90	38.64' LT	613.18
5	27+00.25	38.71' LT	613.42
6	26+86.20	38.80' LT	MATCH EX.
7	26+86.81	33.80' LT	MATCH EX.
8	27+00.15	33.71' LT	613.39
9	27+11.86	33.64' LT	613.18
10	27+16.85	33.60' LT	613.10
11	27+20.19	33.58' LT	613.16
12	27+23.57	40.56' LT	613.45
13	27+21.37	40.57' LT	MATCH EX.
14	27+17.06	30.44' LT	MATCH EX.
15	27+15.58	32.35' LT	613.52



CONTROL POINTS:
BENCHMARK ELEV: 614.13
CUT CROSS ELEV: 613.79
LIGHTPOLE FND. STA: 27+96.64
STA: 27+26.07
OS: 89.92' RT
OS: 32.94' RT
N: 1863413.001
N: 1863494.784
E: 1126408.171
E: 1126368.934

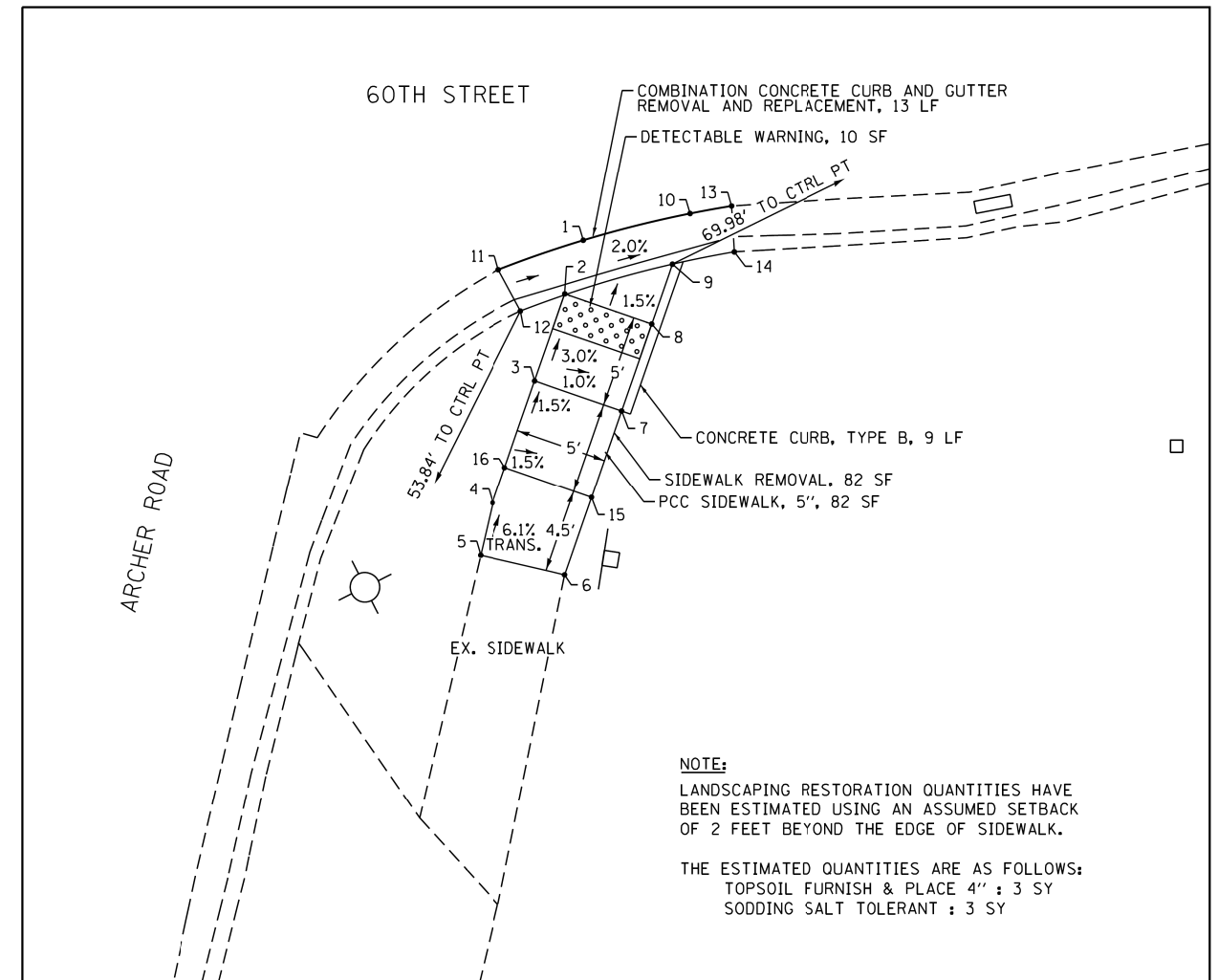
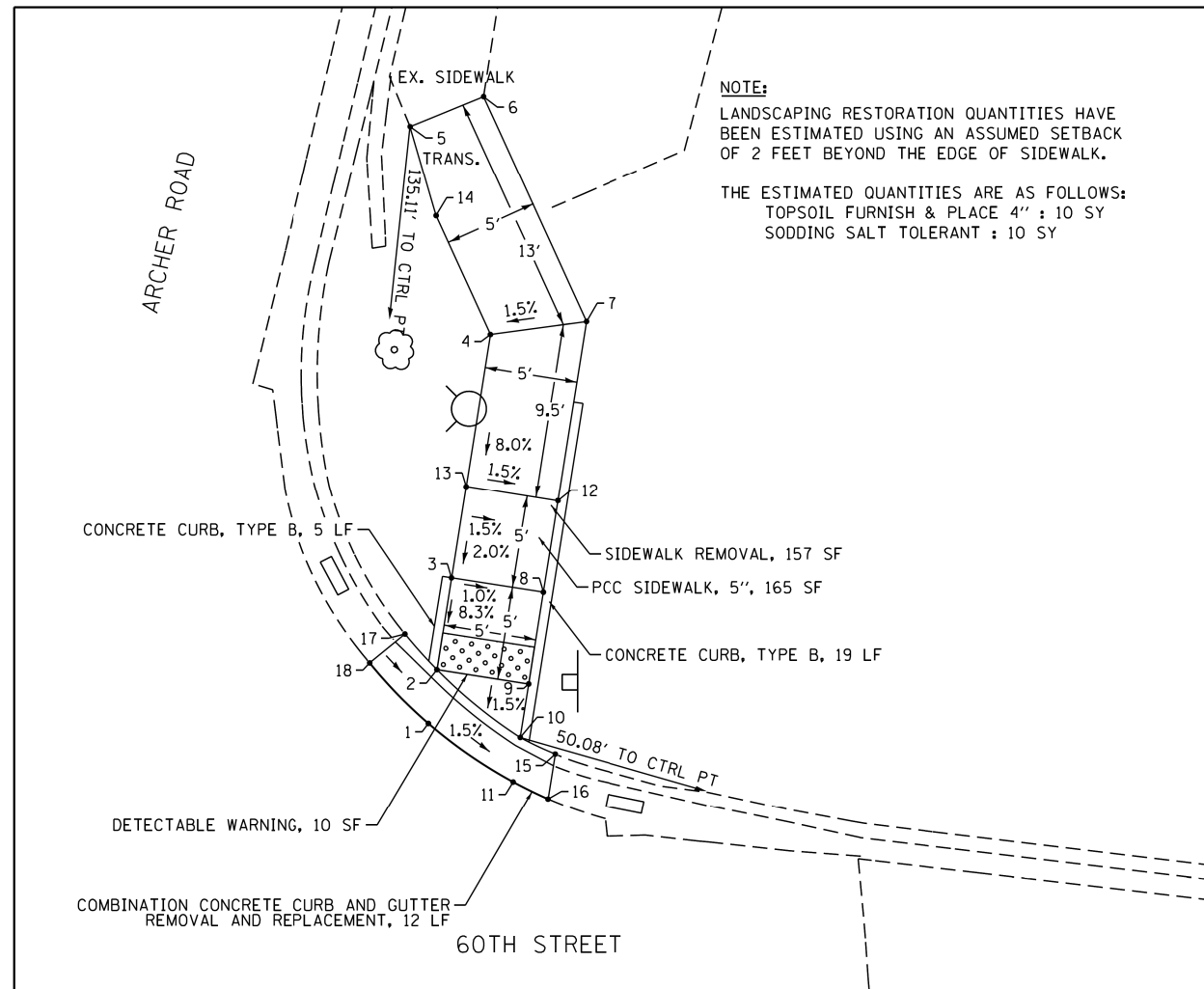
NORTHEAST CORNER
ARCHER ROAD AND 60TH PLACE

POINT No.:	STATION	OFFSET	ELEV.
1	27+65.79	33.56' RT	613.33
2	27+72.01	29.77' RT	613.45
3	27+72.24	32.38' RT	613.45
4	27+78.49	33.63' RT	613.74
5	27+84.00	35.07' RT	MATCH EX.
6	27+83.96	40.76' RT	MATCH EX.
7	27+78.45	40.70' RT	MATCH EX.
8	27+78.47	38.63' RT	613.82
9	27+72.21	38.59' RT	613.57
10	27+67.88	38.57' RT	613.48
11	27+65.80	38.56' RT	613.44
12	27+63.64	38.50' RT	613.44
13	27+63.83	42.70' RT	MATCH EX.
14	27+65.71	42.26' RT	MATCH EX.
15	27+73.01	41.98' RT	613.86
16	27+74.00	46.18' RT	MATCH EX.
17	27+79.50	44.88' RT	MATCH EX.
18	27+73.19	32.24' RT	MATCH EX.
19	27+73.16	29.76' RT	MATCH EX.

CONTROL POINTS:
CUT CROSS ELEV: 614.21
CUT CROSS ELEV: 613.79
STA: 26+62.50
STA: 27+26.07
OS: 39.46' RT
OS: 89.92' RT
N: 1863362.738
N: 1863413.001
E: 1126344.459
E: 1126408.171

SOUTHEAST CORNER
ARCHER ROAD AND 60TH PLACE

POINT No.:	STATION	OFFSET	ELEV.
1	27+22.61	35.20' RT	613.15
2	27+19.79	35.22' RT	613.09
3	27+16.69	35.24' RT	613.29
4	27+11.31	35.28' RT	613.37
5	27+06.14	35.27' RT	MATCH EX.
6	27+06.12	10.15' RT	MATCH EX.
7	27+11.34	40.28' RT	613.47
8	27+16.72	40.24' RT	613.29
9	27+19.82	40.22' RT	613.04
10	27+23.44	40.20' RT	612.99
11	27+25.29	40.19' RT	613.00
12	27+18.05	45.06' RT	613.45
13	27+19.39	49.88' RT	MATCH EX.
14	27+14.39	51.26' RT	MATCH EX.
15	27+13.05	46.45' RT	613.52
16	27+26.27	43.40' RT	MATCH EX.
17	27+24.73	43.80' RT	MATCH EX.
18	27+19.98	32.06' RT	MATCH EX.
19	27+18.25	33.90' RT	MATCH EX.



CONTROL POINTS:
 CUT CROSS CUT CROSS
 ELEV: 613.13 ELEV: 613.63
 STA: 431+15.69 STA: 30+09.16
 OS: 91.86' RT OS: 31.16' RT
 N: 1863791.752 N: 1863702.028
 E: 1126499.599 E: 1126416.045

NORTHEAST CORNER
 ARCHER ROAD AND 60TH STREET

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	31+13.13	36.84' RT	613.11
2	31+16.05	36.62' RT	613.05
3	31+21.04	36.25' RT	613.48
4	31+34.31	35.27' RT	614.21
5	31+44.25	28.48' RT	MATCH EX.
6	31+46.75	31.95' RT	MATCH EX.
7	31+36.18	40.14' RT	614.29
8	31+21.43	41.24' RT	613.41
9	31+16.44	41.61' RT	613.00
10	31+13.53	41.82' RT	612.96
11	31+11.11	42.00' RT	613.02
12	31+26.41	40.87' RT	613.51
13	31+26.03	35.88' RT	613.58
14	31+39.87	30.95' RT	614.77
15	31+13.09	43.86' RT	MATCH EX.
16	31+10.64	44.04' RT	MATCH EX.
17	31+17.52	34.51' RT	MATCH EX.
18	31+15.57	33.02' RT	MATCH EX.

CONTROL POINTS:
 CUT CROSS CUT CROSS
 ELEV: 613.13 ELEV: 613.63
 STA: 431+15.69 STA: 30+09.16
 OS: 91.86' RT OS: 31.16' RT
 N: 1863791.752 N: 1863702.028
 E: 1126499.599 E: 1126416.045

SOUTHEAST CORNER
 ARCHER ROAD AND 60TH STREET

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	30+67.47	36.27' RT	613.33
2	30+64.40	35.95' RT	613.27
3	30+59.43	35.45' RT	613.42
4	30+52.42	34.73' RT	613.63
5	30+49.49	34.74' RT	MATCH EX.
6	30+49.51	39.46' RT	MATCH EX.
7	30+58.92	40.42' RT	613.37
8	30+63.90	40.93' RT	613.22
9	30+67.31	41.28' RT	613.17
10	30+70.22	41.57' RT	613.19
11	30+64.85	32.14' RT	MATCH EX.
12	30+62.94	33.82' RT	MATCH EX.
13	30+71.13	43.68' RT	MATCH EX.
14	30+68.74	44.39' RT	MATCH EX.
15	30+53.95	39.91' RT	613.44
16	30+54.46	34.94' RT	613.51



USER NAME = WTeng	DESIGNED -	REVISED -
PLOT SCALE = 10.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 10/17/2016	CHECKED - MTC	REVISED -
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

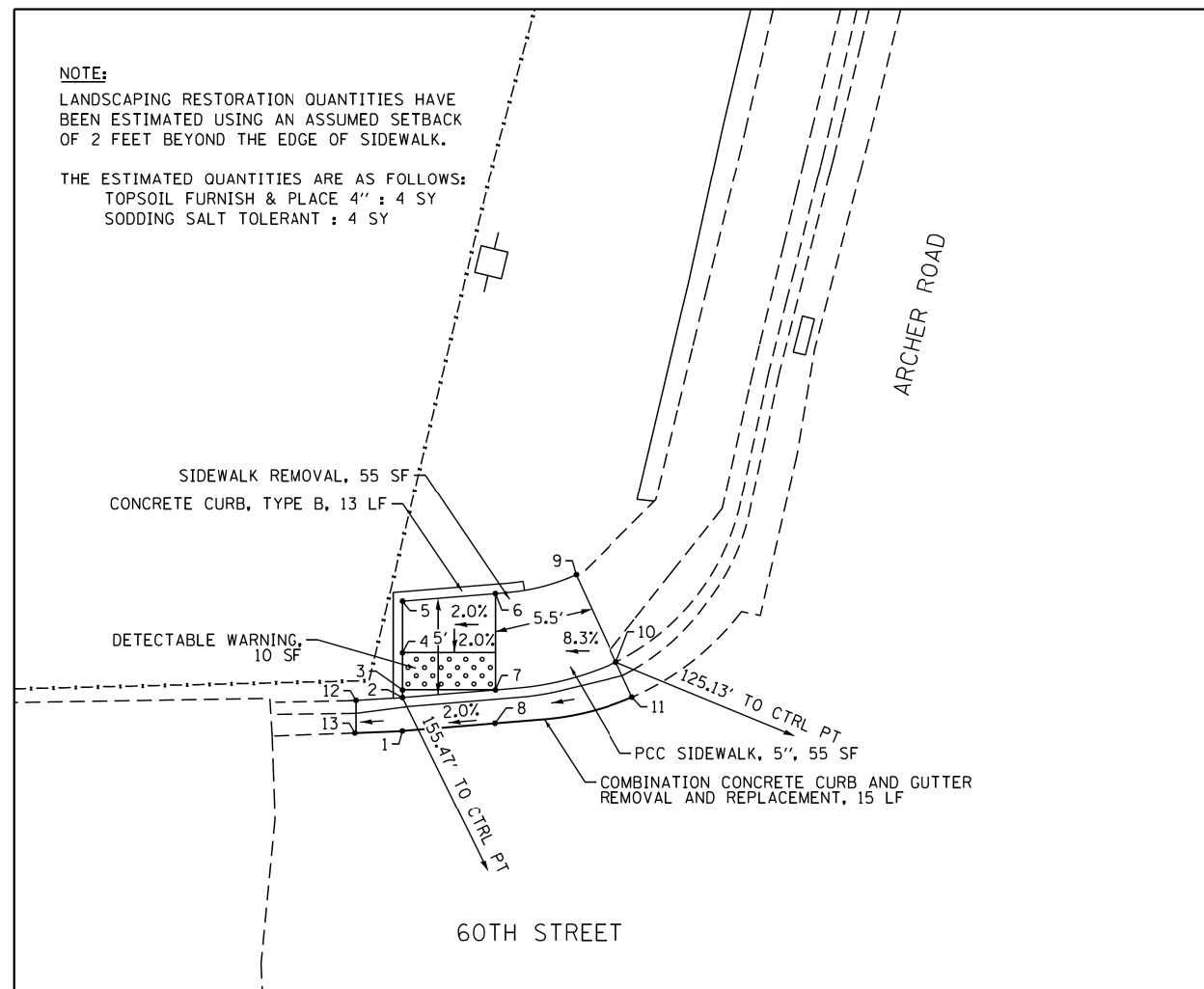
ARCHER ROAD ADA RAMP DETAILS			
SCALE: 1" = 5'	SHEET 12	OF 23 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3565	(46-2)RS	COOK	49	26
CONTRACT NO. 60Y87				
ILLINOIS FED. AID PROJECT				

NOTE:

LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.

THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 4 SY
 SODDING SALT TOLERANT : 4 SY

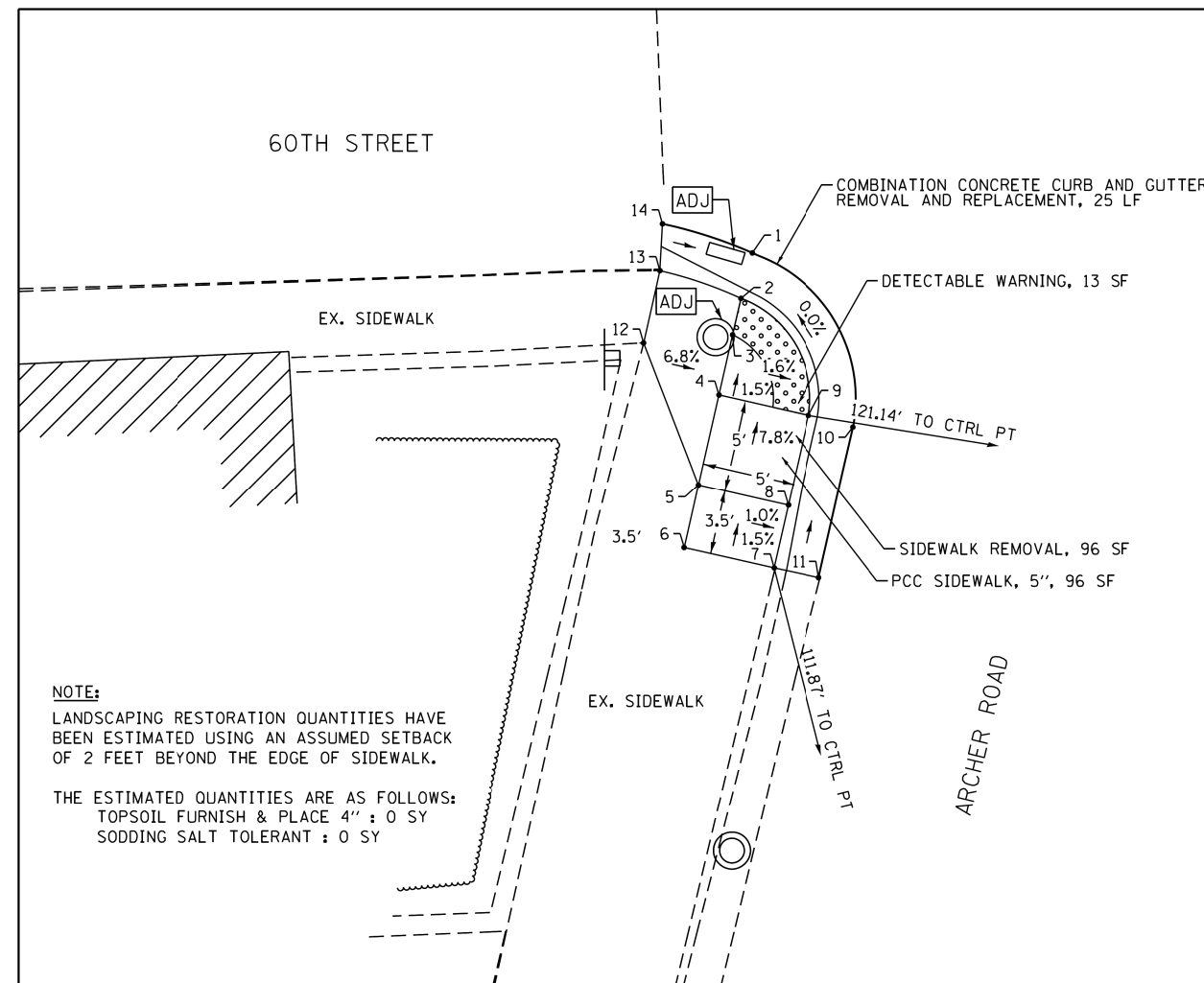


CONTROL POINTS:
 CUT CROSS CUT CROSS
 ELEV: 613.13 ELEV: 616.63
 STA: 31+15.69 STA: 30+09.16
 OS: 91.86' RT OS: 31.16' RT
 N: 1863791.752 N: 1863702.028
 E: 1126499.599 E: 1126416.045

**NORTHWEST CORNER
 ARCHER ROAD AND 60TH STREET**

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	31+46.44	38.03' LT	614.64
2	31+48.19	38.44' LT	614.65
3	31+48.59	38.54' LT	614.66
4	31+50.54	38.99' LT	617.70
5	31+53.24	39.63' LT	617.76
6	31+54.79	33.86' LT	614.86
7	31+49.74	33.67' LT	614.75
8	31+47.99	33.26' LT	617.74
9	31+56.78	30.86' LT	MATCH EX.
10	31+52.69	27.67' LT	MATCH EX.
11	31+51.06	26.39' LT	MATCH EX.
12	31+47.47	40.84' LT	MATCH EX.
13	31+45.76	40.44' LT	MATCH EX.

60TH STREET



NOTE:

LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.

THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 0 SY
 SODDING SALT TOLERANT : 0 SY

CONTROL POINTS:
 CUT CROSS CUT CROSS
 ELEV: 613.13 ELEV: 616.63
 STA: 31+15.69 STA: 30+09.16
 OS: 91.86' RT OS: 31.16' RT
 N: 1863791.752 N: 1863702.028
 E: 1126499.599 E: 1126416.045

**SOUTHWEST CORNER
 ARCHER ROAD AND 60TH STREET**

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	31+19.78	34.17' LT	613.85
2	31+17.25	34.28' LT	613.79
3	31+15.22	34.26' LT	613.82
4	31+11.89	34.23' LT	613.87
5	31+06.87	34.19' LT	614.26
6	31+03.37	34.16' LT	MATCH EX.
7	31+03.42	29.11' LT	MATCH EX.
8	31+06.92	29.19' LT	614.18
9	31+11.93	29.23' LT	613.79
10	31+12.02	26.73' LT	613.85
11	31+03.47	26.66' LT	MATCH EX.
12	31+13.69	38.85' LT	614.21
13	31+17.66	38.88' LT	614.02
14	31+20.20	39.33' LT	MATCH EX.



USER NAME = WTeng	DESIGNED -	REVISED -
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PLOT DATE = 10/17/2016	CHECKED - MTC	REVISED -
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ARCHER ROAD
 ADA RAMP DETAILS**

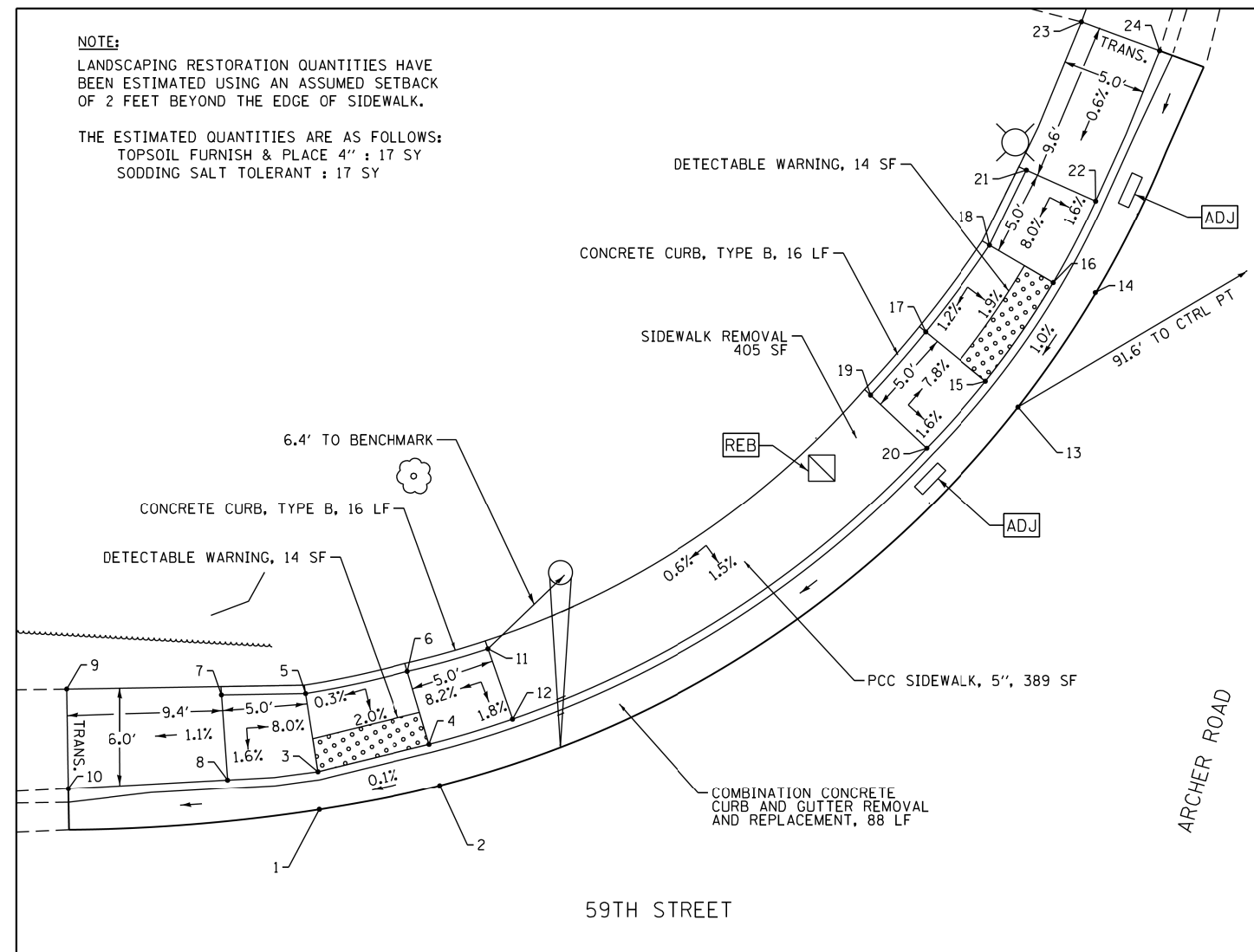
SCALE: 1" = 5' SHEET 13 OF 23 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3565	(46-2)RS	COOK	49	27
CONTRACT NO. 60Y87				
ILLINOIS FED. AID PROJECT				

NOTE:

LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.

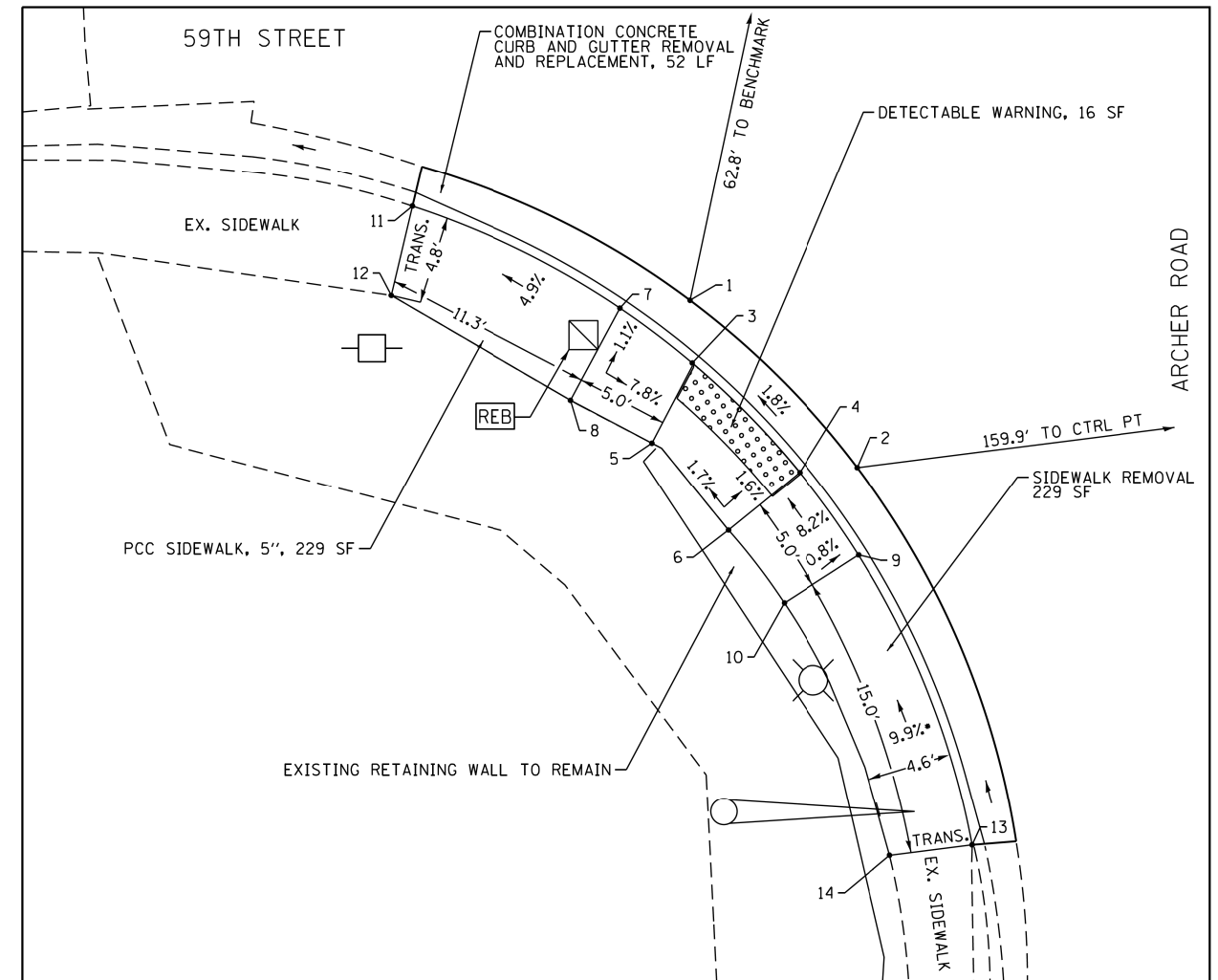
THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 17 SY
 SODDING SALT TOLERANT : 17 SY



CONTROL POINTS:
 CUT CROSS BENCHMARK
 ELEV: 614.86 ELEV: 615.27
 STA: 38+71.93 STA: 37+92.14
 OS: 33.67' RT OS: 56.43' LT
 N: 1864540.964 N: 1864484.167
 E: 1126617.240 E: 1126511.136

NORTHWEST CORNER
 ARCHER ROAD AND 59TH STREET

ADA RAMP ELEVATION TABLE			
POINT No.	STATION	OFFSET	ELEV.
1	37+74.35	67.35' LT	614.41
2	37+78.12	60.80' LT	614.42
3	37+77.24	68.15' LT	614.38
4	37+80.39	62.00' LT	614.39
5	37+81.63	69.95' LT	614.46
6	37+84.35	64.28' LT	614.48
7	37+80.39	74.84' LT	614.86
8	37+75.49	73.30' LT	614.78
9	37+78.58	84.00' LT	MATCH EX.
10	37+72.76	82.52' LT	MATCH EX.
11	37+86.78	59.88' LT	614.89
12	37+83.02	57.48' LT	614.81
13	38+08.30	32.21' LT	614.60
14	38+16.05	29.25' LT	614.64
15	38+09.35	34.47' LT	614.57
16	38+16.05	31.88' LT	614.66
17	38+11.39	38.67' LT	614.66
18	38+17.35	36.11' LT	614.74
19	38+06.94	41.03' LT	615.05
20	38+04.62	37.00' LT	614.98
21	38+22.28	35.01' LT	615.14
22	38+21.50	30.52' LT	615.08
23	38+31.72	33.87' LT	MATCH EX.
24	38+31.12	28.91' LT	MATCH EX.



CONTROL POINTS:
 BENCHMARK CUT CROSS
 ELEV: 615.27 ELEV: 615.13
 STA: 37+92.14 STA: 37+78.82
 OS: 56.43' LT OS: 105.73' RT
 N: 1864484.167 N: 1864433.718
 E: 1126511.136 E: 1126665.825

SOUTHWEST CORNER
 ARCHER ROAD AND 59TH STREET

ADA RAMP ELEVATION TABLE			
POINT No.	STATION	OFFSET	ELEV.
1	37+29.36	54.97' LT	615.58
2	37+22.62	43.98' LT	615.75
3	37+26.07	54.06' LT	615.57
4	37+21.62	47.00' LT	615.72
5	37+21.32	55.19' LT	615.69
6	37+17.66	50.04' LT	615.80
7	37+28.06	58.57' LT	615.24
8	37+22.57	60.03' LT	615.30
9	37+17.98	42.81' LT	616.17
10	37+14.50	46.17' LT	616.21
11	37+30.84	70.88' LT	MATCH EX.
12	37+25.85	70.88' LT	MATCH EX.
13	37+04.05	33.15' LT	MATCH EX.
14	37+02.47	37.38' LT	MATCH EX.

• SLOPE EXCEEDS MAX TO TIE INTO EXISTING BRIDGE SIDEWALK

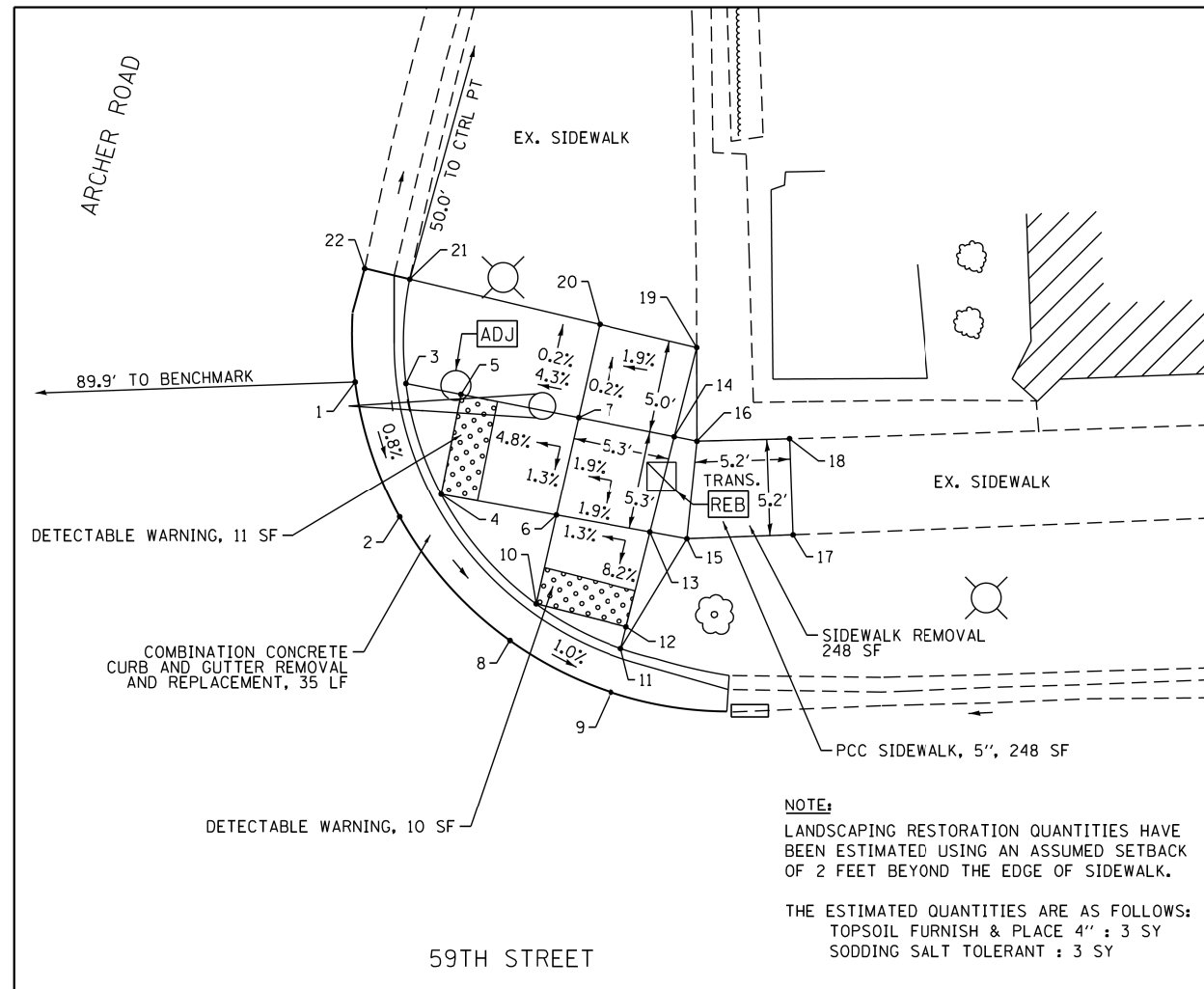


USER NAME = WTeng	DESIGNED -	REVISED -
PLOT SCALE = 10.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 10/17/2016	CHECKED - MTC	REVISED -
	DATE - 09/05/2016	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ARCHER ROAD ADA RAMP DETAILS		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: 1" = 5'		3565	(46-2)RS	COOK	49	28
SHEET 14 OF 23 SHEETS		STA.		TO STA.		

CONTRACT NO. 60Y87					
ILLINOIS FED. AID PROJECT					

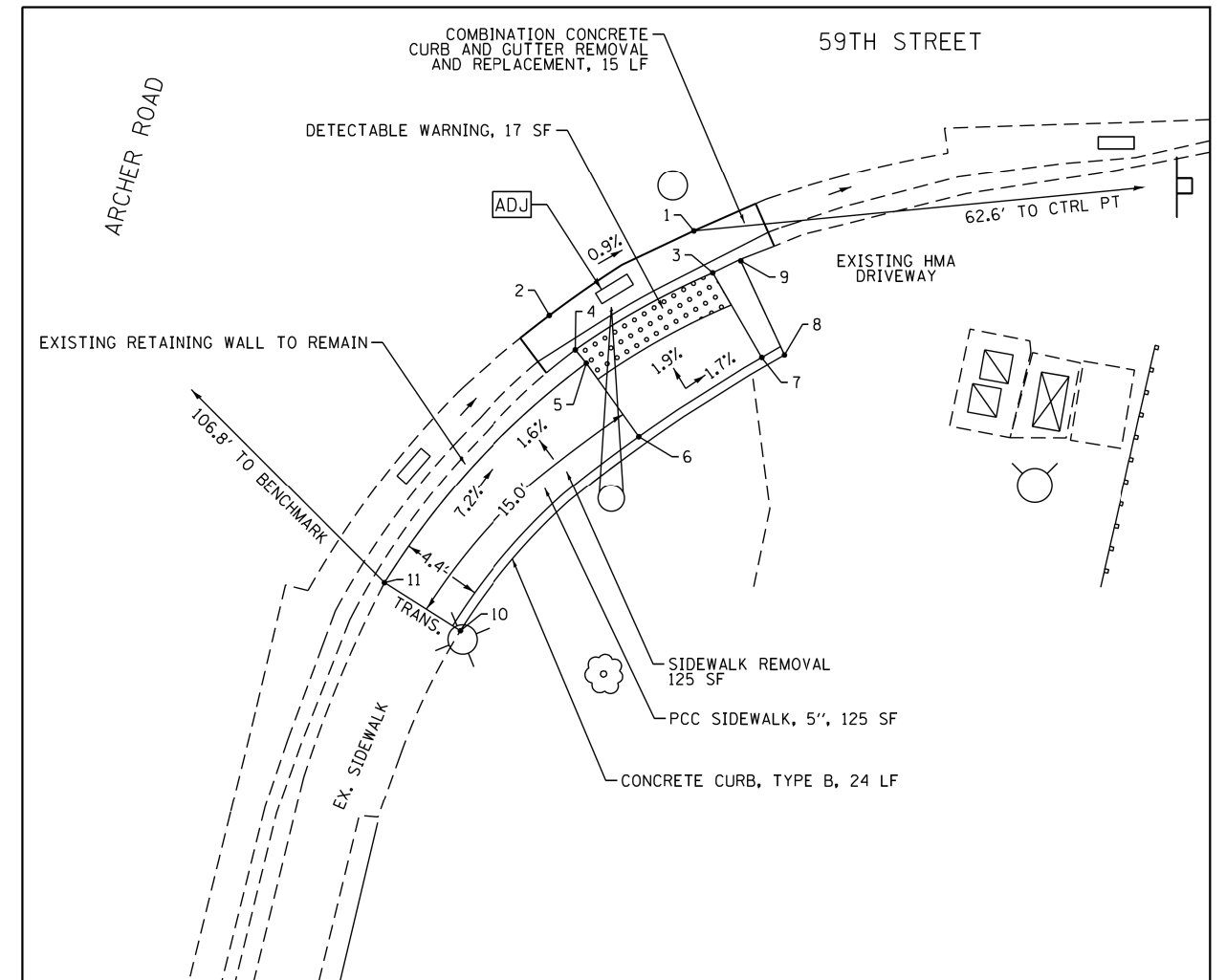


NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 3 SY
 SODDING SALT TOLERANT : 3 SY

CONTROL POINTS:
 CUT CROSS BENCHMARK
 ELEV: 614.86 TRAFFIC MAST ARM
 STA: 38+71.93 ELEV: 615.27
 OS: 33.67' RT STA: 37+92.14
 N: 1864540.964 OS: 56.43' LT
 E: 1126617.240 N: 1864484.167
 E: 1126511.136

NORTHEAST CORNER
 ARCHER ROAD AND 59TH STREET

ADA RAMP ELEVATION TABLE			
POINT No.	STATION	OFFSET	ELEV.
1	38+15.88	30.26' RT	615.03
2	38+09.34	34.27' RT	614.98
3	38+16.44	32.93' RT	615.00
4	38+11.05	36.16' RT	614.95
5	38+16.55	35.95' RT	615.04
6	38+11.39	42.46' RT	615.25
7	38+16.80	42.41' RT	615.32
8	38+04.23	41.58' RT	614.94
9	38+02.79	47.54' RT	614.89
10	38+06.48	42.50' RT	614.91
11	38+05.20	47.52' RT	614.86
12	38+06.40	47.54' RT	614.89
13	38+11.66	47.61' RT	615.32
14	38+17.00	47.69' RT	615.42
15	38+11.77	49.64' RT	615.42
16	38+17.05	48.95' RT	MATCH EX.
17	38+13.30	55.17' RT	MATCH EX.
18	38+18.34	53.48' RT	MATCH EX.
19	38+21.95	47.77' RT	MATCH EX.
20	38+21.95	42.36' RT	MATCH EX.
21	38+21.95	31.83' RT	MATCH EX.
22	38+21.95	29.34' RT	MATCH EX.



CONTROL POINTS:
 BENCHMARK CUT CROSS
 ELEV: 615.27 ELEV: 615.13
 STA: 37+92.14 STA: 37+78.82
 OS: 56.43' LT OS: 105.73' RT
 N: 1864484.167 N: 1864433.718
 E: 1126511.136 E: 1126665.825

SOUTHEAST CORNER
 ARCHER ROAD AND 59TH STREET

ADA RAMP ELEVATION TABLE			
POINT No.	STATION	OFFSET	ELEV.
1	37+58.59	46.45' RT	615.80
2	37+52.31	39.88' RT	615.88
3	37+56.61	47.97' RT	615.77
4	37+50.81	41.69' RT	615.85
5	37+50.25	42.43' RT	615.92
6	37+47.02	46.13' RT	616.00
7	37+52.75	51.63' RT	615.87
8	37+53.56	52.48' RT	MATCH EX.
9	37+57.66	49.25' RT	MATCH EX.
10	37+34.47	39.10' RT	MATCH EX.
11	37+36.05	34.47' RT	MATCH EX.



USER NAME = WTeng
 DESIGNED -
 DRAWN -
 PLOT SCALE = 10.0000' / in.
 PLOT DATE = 10/17/2016

DESIGNED -
 DRAWN -
 CHECKED - MTC
 DATE - 09/05/2016

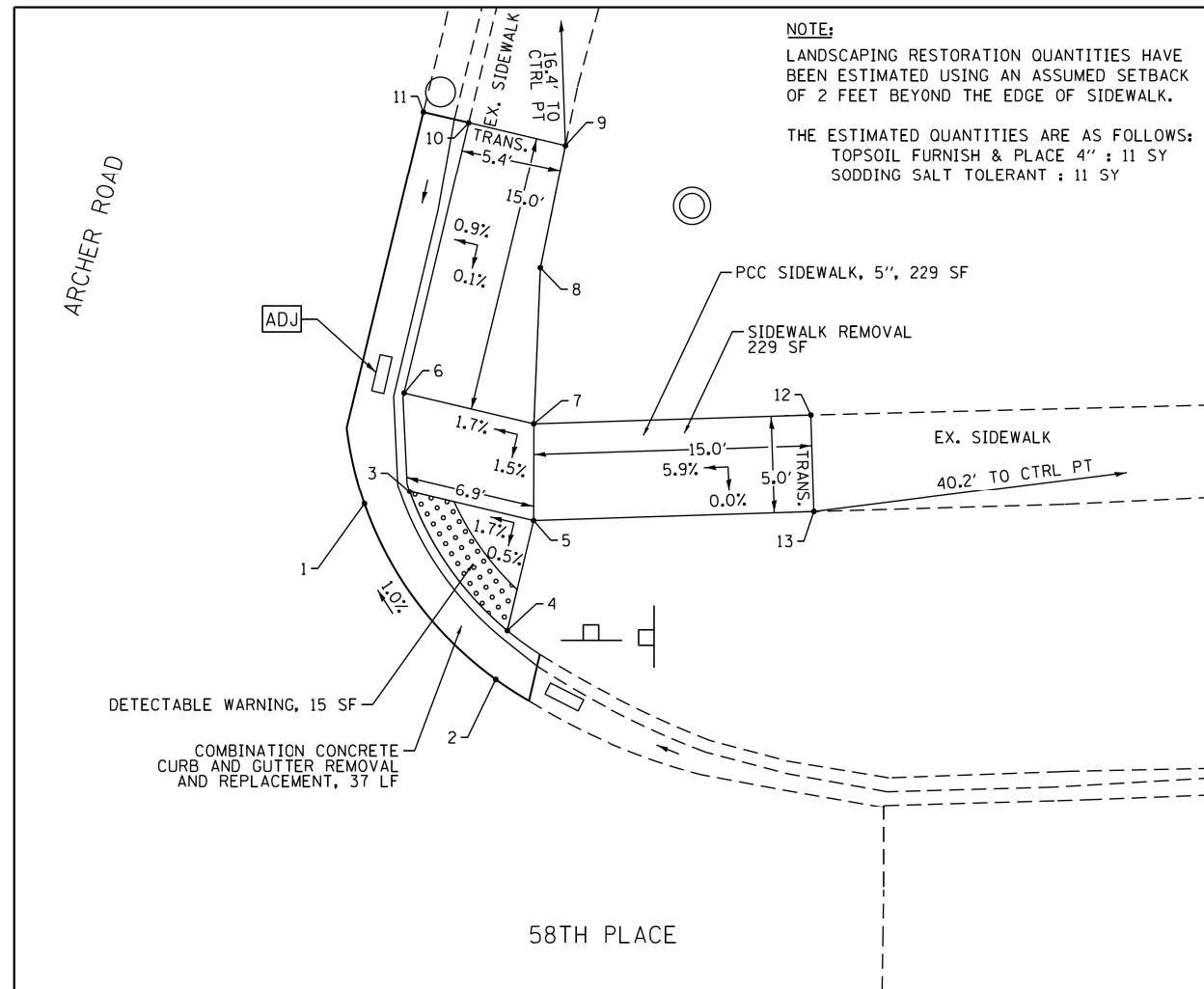
REVISED -
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ARCHER ROAD
 ADA RAMP DETAILS
 SCALE: 1" = 5'
 SHEET 15 OF 23 SHEETS
 STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3565	(46-2)RS	COOK	49	29

CONTRACT NO. 60Y87
 ILLINOIS FED. AID PROJECT

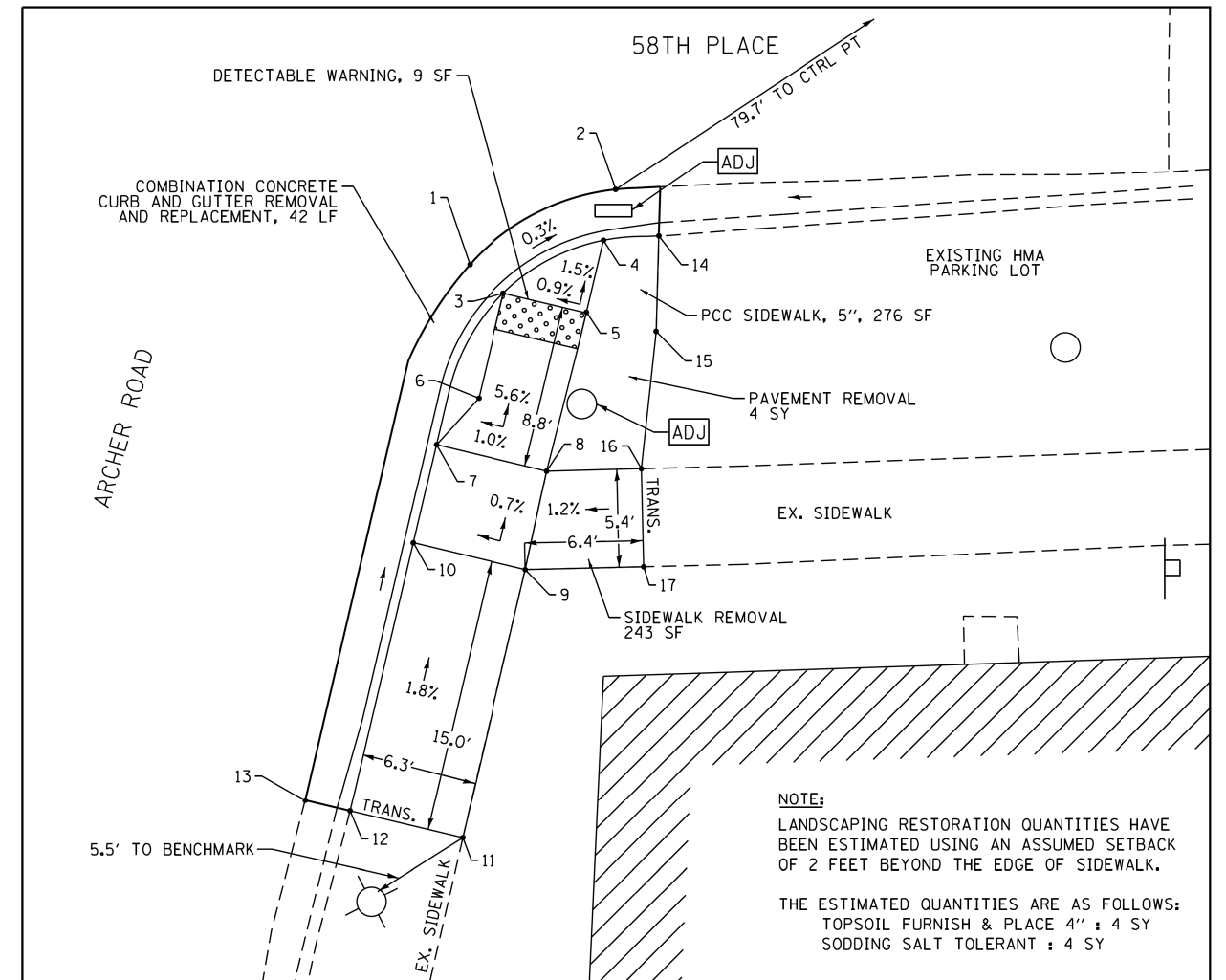


NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 11 SY
 SODDING SALT TOLERANT : 11 SY

CONTROL POINTS:
 CUT CROSS ELEV: 615.48
 STA: 41+66.69
 OS: 92.68' RT
 N: 1864815.543
 E: 1126742.885
 CUT CROSS ELEV: 615.43
 STA: 41+85.42
 OS: 33.35' RT
 N: 1864846.551
 E: 1126689.012

NORTHEAST CORNER
 ARCHER ROAD AND 58TH PLACE

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	41+48.81	31.12' RT	614.11
2	41+41.23	40.19' RT	614.20
3	41+50.02	33.34' RT	614.08
4	41+43.93	40.19' RT	614.17
5	41+50.02	40.19' RT	614.20
6	41+54.64	31.85' RT	614.16
7	41+54.53	39.02' RT	614.28
8	41+62.80	37.55' RT	614.66
9	41+69.56	37.44' RT	MATCH EX.
10	41+69.60	32.09' RT	MATCH EX.
11	41+69.68	29.59' RT	MATCH EX.
12	41+58.23	53.56' RT	MATCH EX.
13	41+53.16	54.85' RT	MATCH EX.



NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 4 SY
 SODDING SALT TOLERANT : 4 SY

CONTROL POINTS:
 BENCHMARK ELEV: 614.66
 STA: 40+69.76
 OS: 34.18' RT
 N: 1864733.315
 E: 1126663.463
 CUT CROSS ELEV: 615.48
 STA: 41+66.69
 OS: 92.68' RT
 N: 1864815.543
 E: 1126742.885

SOUTHEAST CORNER
 ARCHER ROAD AND 58TH PLACE

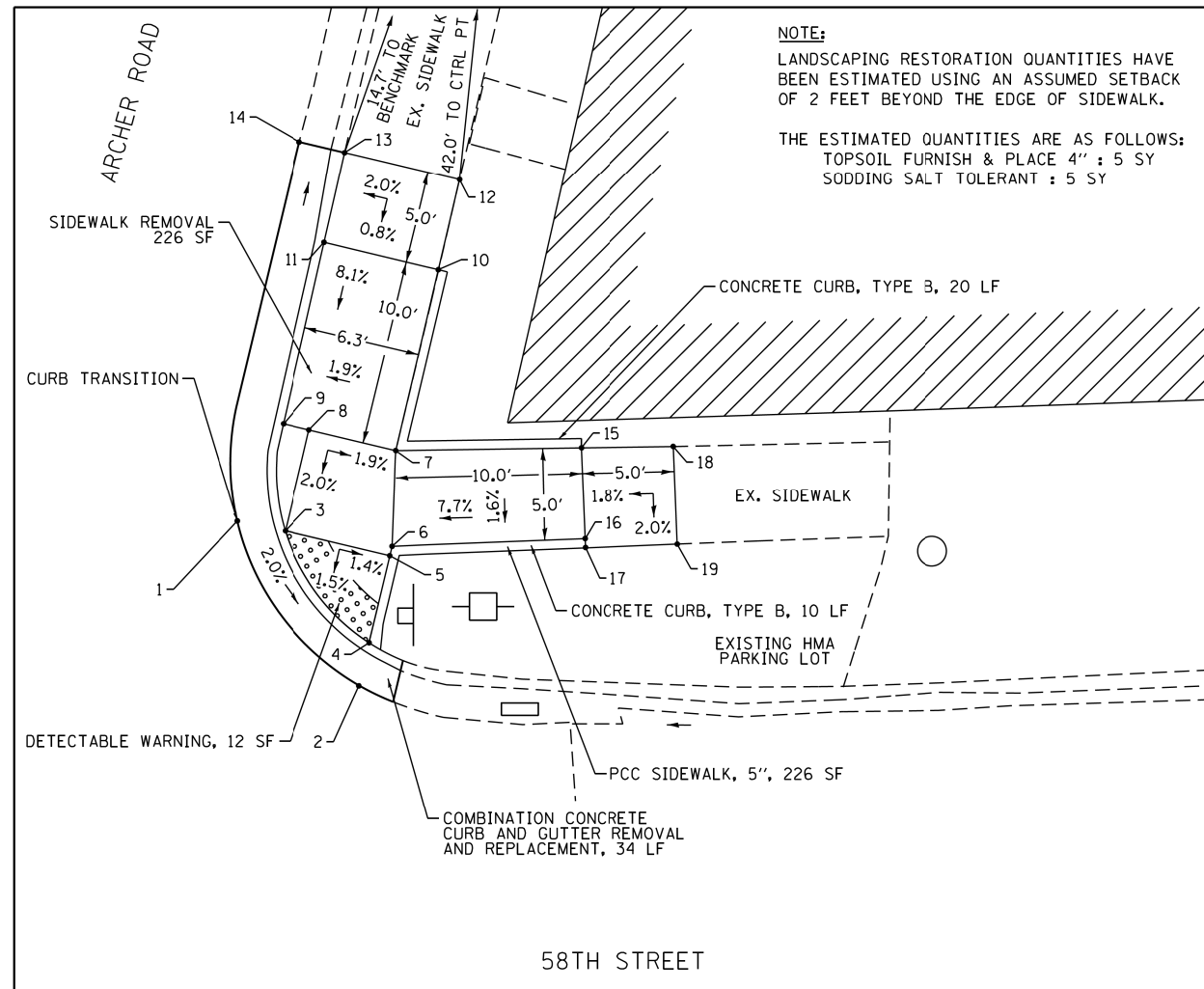
ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	41+04.13	31.19' RT	614.12
2	41+09.94	37.98' RT	614.10
3	41+03.03	33.34' RT	614.09
4	41+07.10	37.98' RT	614.07
5	41+03.03	37.98' RT	614.13
6	40+97.18	33.34' RT	614.58
7	40+94.19	31.67' RT	614.57
8	40+94.19	37.87' RT	614.63
9	40+88.67	37.99' RT	614.67
10	40+88.67	31.68' RT	614.55
11	40+73.67	38.01' RT	MATCH EX.
12	40+73.67	31.71' RT	MATCH EX.
13	40+73.67	29.20' RT	MATCH EX.
14	41+08.02	40.85' RT	MATCH EX.
15	41+02.94	41.90' RT	MATCH EX.
16	40+95.50	42.85' RT	MATCH EX.
17	40+90.30	44.21' RT	MATCH EX.

	USER NAME = WTeng	DESIGNED -	REVISED -
	PLOT SCALE = 10.0000' / in.	DRAWN -	REVISED -
	PLOT DATE = 10/17/2016	CHECKED - MTC	REVISED -
		DATE - 09/05/2016	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ARCHER ROAD ADA RAMP DETAILS			
SCALE: 1" = 5'	SHEET 16	OF 23 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3565	(46-2)RS	COOK	49	30
CONTRACT NO. 60Y87				
ILLINOIS FED. AID PROJECT				

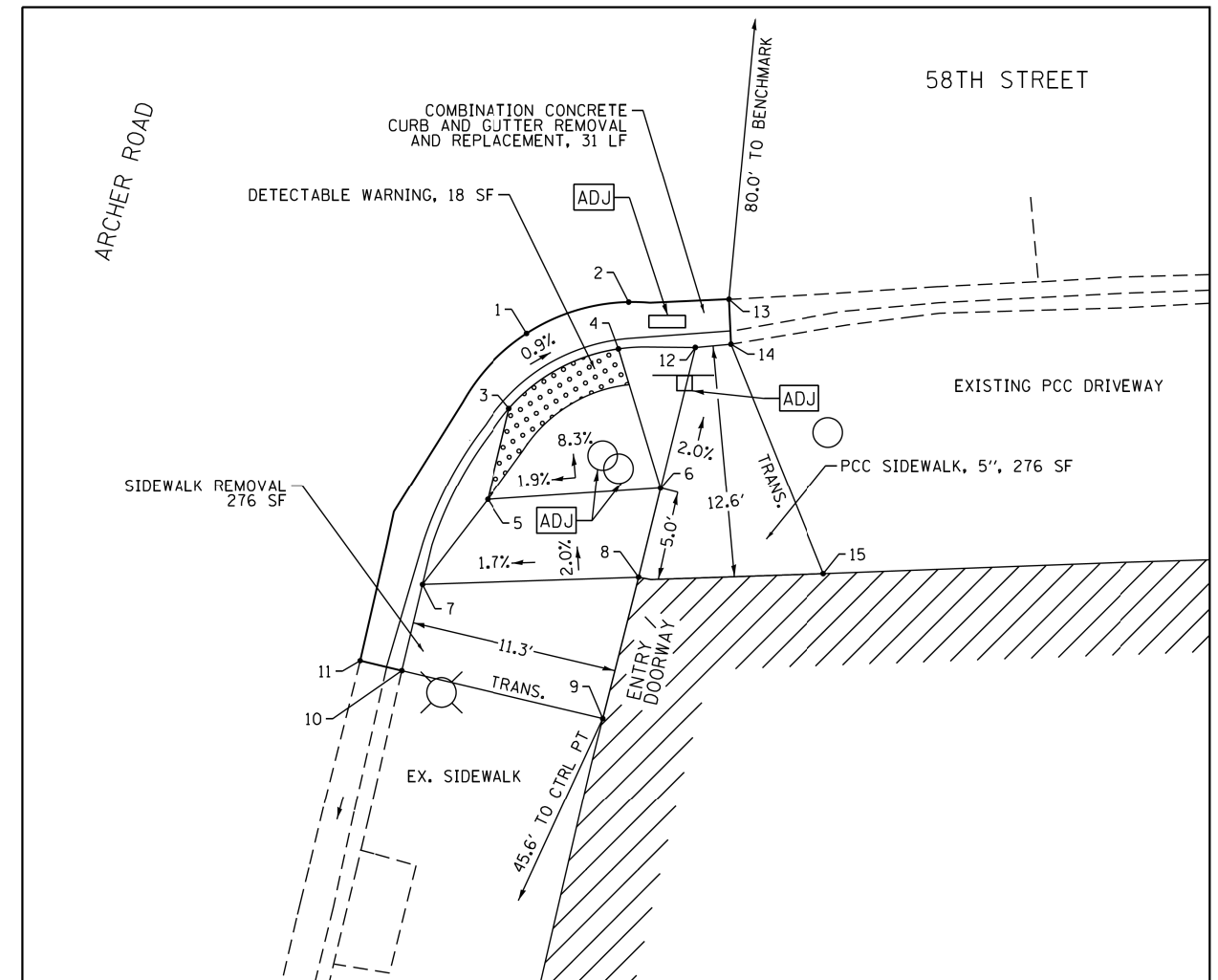


NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 5 SY
 SODDING SALT TOLERANT : 5 SY

CONTROL POINTS:
 BENCHMARK ELEV: 615.45
 LIGHT POLE FOUNDATION STA: 45+24.28
 OS: 33.28' RT
 N: 1865176.306
 E: 1126765.192

NORTHEAST CORNER
 ARCHER ROAD AND 58TH STREET

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	44+88.96	30.72' RT	614.65
2	44+81.85	39.19' RT	614.50
3	44+89.04	33.38' RT	614.62
4	44+84.23	39.19' RT	614.47
5	44+89.04	39.19' RT	614.54
6	44+89.56	39.19' RT	614.55
7	44+94.67	38.18' RT	614.65
8	44+94.67	33.38' RT	614.73
9	44+94.67	31.94' RT	614.88
10	45+04.67	38.16' RT	615.46
11	45+04.68	31.84' RT	615.34
12	45+09.67	38.16' RT	MATCH EX.
13	45+09.67	31.79' RT	MATCH EX.
14	45+09.67	29.27' RT	MATCH EX.
15	44+97.12	47.87' RT	615.42
16	44+92.37	49.21' RT	615.34
17	44+91.89	49.34' RT	MATCH EX.
18	44+98.35	52.72' RT	MATCH EX.
19	44+93.24	54.16' RT	MATCH EX.



CONTROL POINTS:
 BENCHMARK ELEV: 615.45
 LIGHT POLE FOUNDATION STA: 45+24.28
 OS: 33.28' RT
 N: 1865176.306
 E: 1126765.192

SOUTHEAST CORNER
 ARCHER ROAD AND 58TH STREET

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	44+40.66	34.19' RT	614.38
2	44+43.60	39.19' RT	614.27
3	44+36.48	34.19' RT	614.35
4	44+41.00	39.24' RT	614.29
5	44+31.37	34.19' RT	614.77
6	44+34.15	43.22' RT	614.95
7	44+26.03	31.80' RT	614.85
8	44+29.15	43.18' RT	615.05
9	44+21.23	43.06' RT	MATCH EX.
10	44+21.23	31.78' RT	MATCH EX.
11	44+21.23	29.45' RT	MATCH EX.
12	44+42.04	43.28' RT	614.79
13	44+45.01	44.46' RT	MATCH EX.
14	44+42.67	45.12' RT	MATCH EX.
15	44+31.66	52.93' RT	MATCH EX.

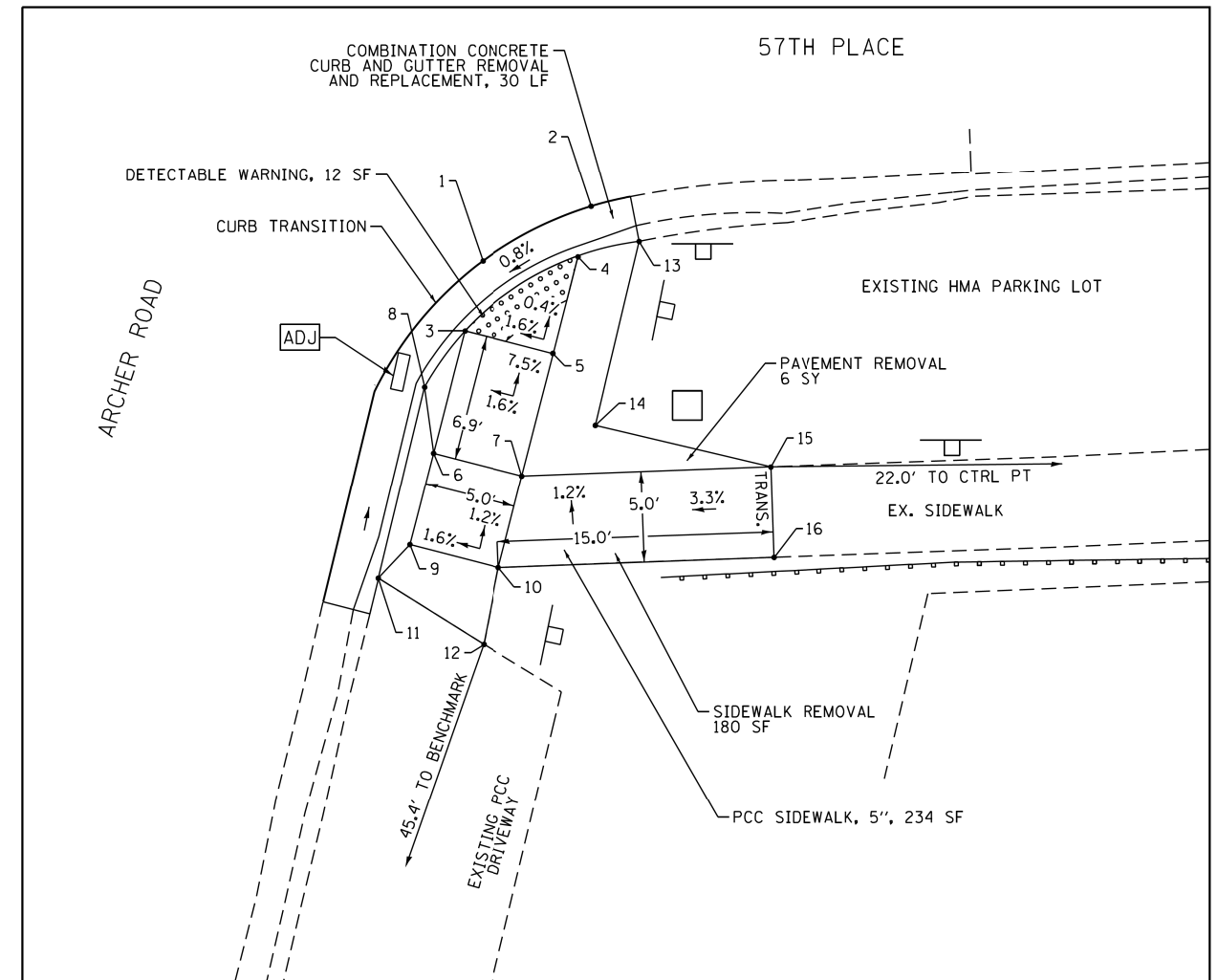
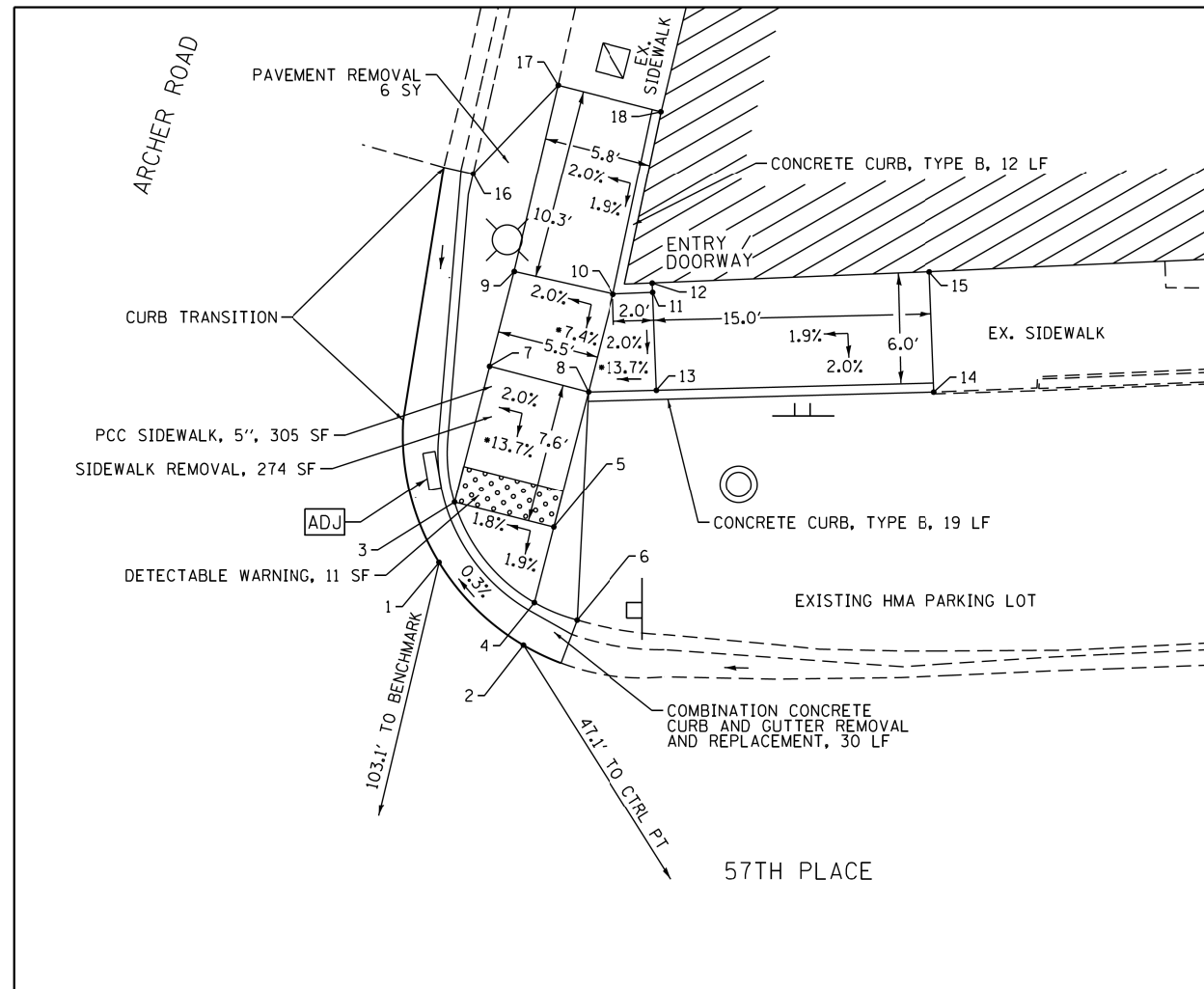


USER NAME = WTeng	DESIGNED -	REVISED -
PLOT SCALE = 10.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 10/17/2016	CHECKED - MTC	REVISED -
	DATE - 09/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARCHER ROAD			
ADA RAMP DETAILS			
SCALE: 1" = 5'	SHEET 17	OF 23 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3565	(46-2)RS	COOK	49	31
CONTRACT NO. 60Y87				
ILLINOIS FED. AID PROJECT				



CONTROL POINTS:
 BENCHMARK ELEV: 615.18
 LIGHT POLE FOUNDATION STA: 47+19.54
 OS: 33.35' RT
 N: 1865366.255
 E: 1126810.391

CUT CROSS ELEV: 615.20
 STA: 47+83.14
 OS: 72.37' RT
 N: 1865419.014
 E: 1126863.060

NORTHEAST CORNER
 ARCHER ROAD AND 57TH PLACE

• IN ORDER TO MATCH INTO EXISTING GRADES WITHOUT THE USE OF STEPS, RAMP IS DESIGNED TO BE OUT OF COMPLIANCE. (REFER TO SUBMITTED BDE FORM 3101 FOR MORE INFORMATION)

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	48+19.30	33.20' RT	614.52
2	48+16.02	38.66' RT	614.54
3	48+22.65	33.26' RT	614.54
4	48+18.38	38.70' RT	614.56
5	48+22.58	38.77' RT	614.64
6	48+18.00	41.16' RT	MATCH EX.
7	48+30.24	33.39' RT	615.56
8	48+30.13	38.90' RT	615.67
9	48+35.51	33.48' RT	615.95
10	48+35.58	38.98' RT	616.07
11	48+36.16	41.04' RT	616.27
12	48+36.64	40.91' RT	616.27
13	48+31.09	42.47' RT	616.16
14	48+34.47	57.10' RT	616.38
15	48+40.72	55.35' RT	616.55
16	48+40.13	30.11' RT	MATCH EX.
17	48+45.86	33.45' RT	MATCH EX.
18	48+45.76	39.22' RT	MATCH EX.

CONTROL POINTS:
 BENCHMARK ELEV: 615.45
 LIGHT POLE FOUNDATION STA: 45+24.28
 OS: 33.28' RT
 N: 1865176.306
 E: 1126765.192

CUT CROSS ELEV: 615.20
 STA: 47+83.14
 OS: 72.37' RT
 N: 1865419.014
 E: 1126863.060

SOUTHEAST CORNER
 ARCHER ROAD AND 57TH PLACE

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	47+85.04	33.22' RT	614.63
2	47+89.30	38.27' RT	614.71
3	47+81.11	33.14' RT	614.62
4	47+86.52	38.22' RT	614.68
5	47+81.03	38.13' RT	614.70
6	47+74.24	33.02' RT	615.14
7	47+74.15	38.02' RT	615.22
8	47+77.63	31.71' RT	614.90
9	47+69.08	32.92' RT	615.22
10	47+69.00	37.93' RT	MATCH EX.
11	47+66.90	31.69' RT	MATCH EX.
12	47+64.70	38.11' RT	MATCH EX.
13	47+88.04	41.25' RT	MATCH EX.
14	47+77.78	41.26' RT	MATCH EX.
15	47+77.79	51.06' RT	MATCH EX.
16	47+73.02	52.38' RT	MATCH EX.



USER NAME = WTeng	DESIGNED -	REVISED -
PLOT SCALE = 10.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 10/17/2016	CHECKED - MTC	REVISED -
	DATE - 09/05/2016	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ARCHER ROAD
 ADA RAMP DETAILS

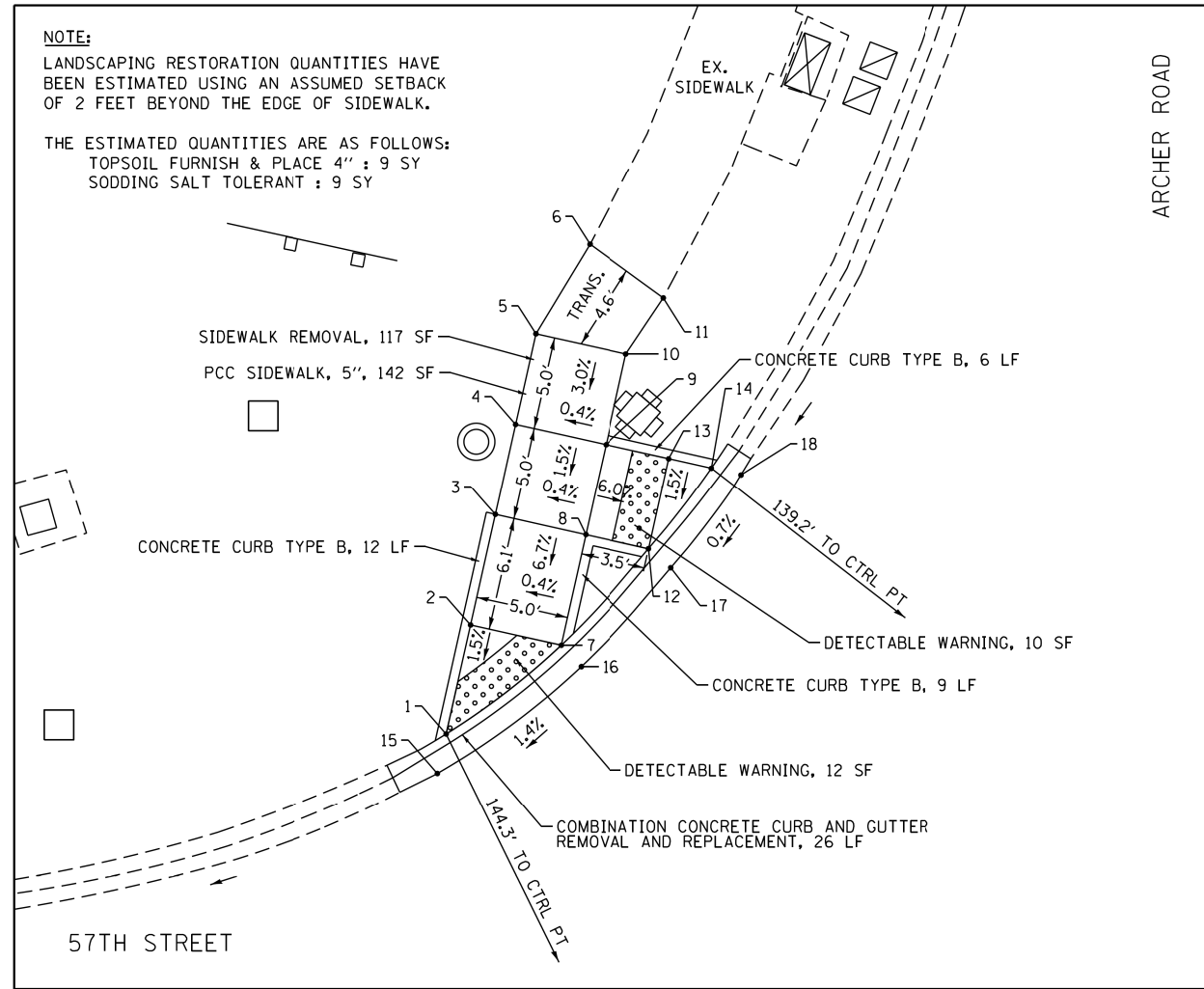
SCALE: 1" = 5' SHEET 18 OF 23 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3565	(46-2)RS	COOK	49	32
CONTRACT NO. 60Y87				
ILLINOIS FED. AID PROJECT				

NOTE:

LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.

THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 9 SY
 SODDING SALT TOLERANT : 9 SY



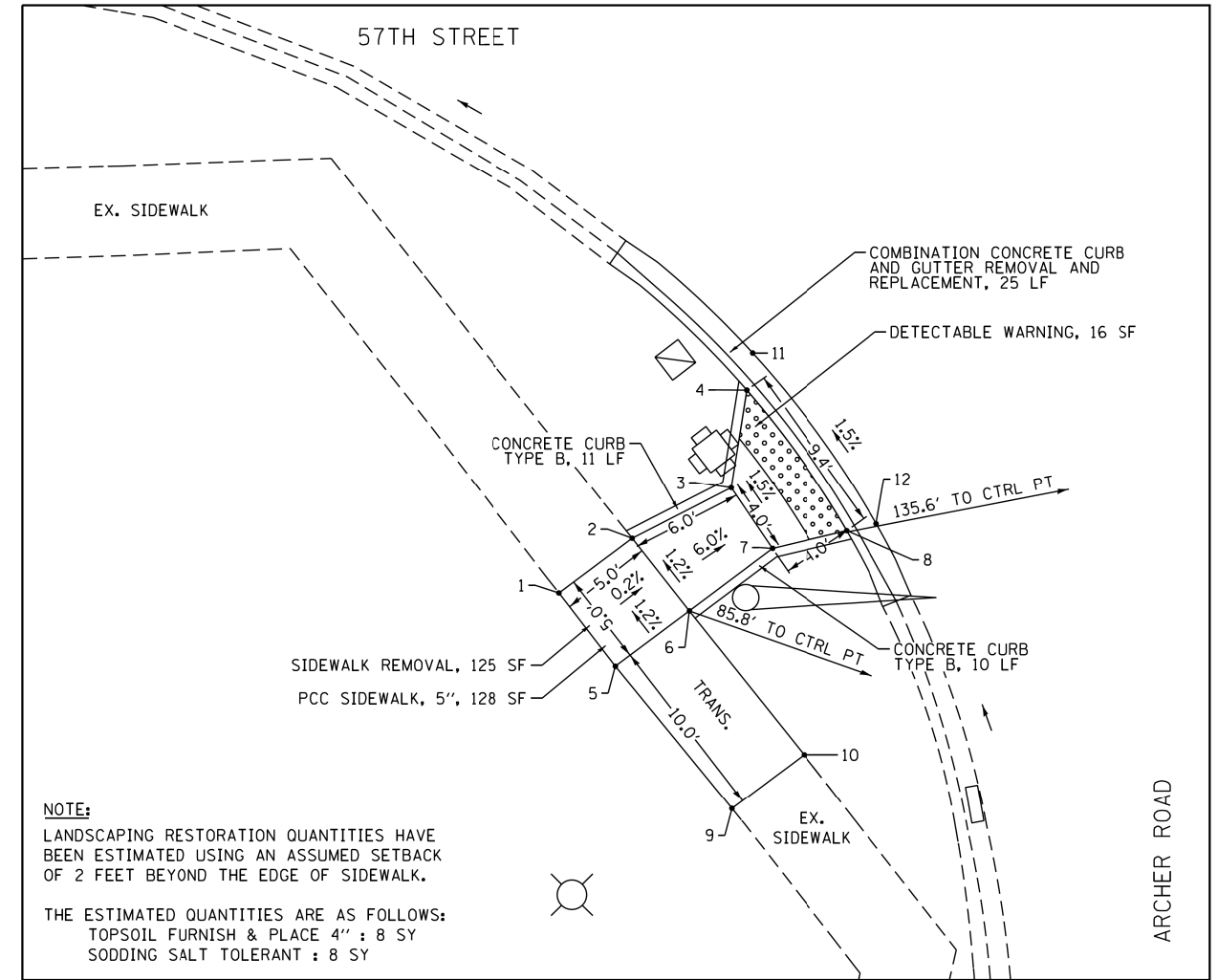
CONTROL POINTS:
 CUT CROSS ELEV: 614.89 STA: 50+53.51 OS: 34.32' RT N: 1865690.921 E: 1126888.704
 CUT CROSS ELEV: 614.507 STA: 51+24.74 OS: 79.87' RT N: 1865749.699 E: 1126949.487

**NORTHWEST CORNER
 ARCHER ROAD AND 57TH STREET**

POINT No.	STATION	OFFSET	ELEV.
1	51+64.79	57.61' LT	612.93
2	51+70.79	57.69' LT	613.02
3	51+76.90	57.77' LT	613.43
4	51+81.90	57.84' LT	613.50
5	51+86.90	57.90' LT	613.65
6	51+92.26	56.18' LT	MATCH EX.
7	51+70.86	52.69' LT	613.04
8	51+76.97	52.77' LT	613.45
9	51+81.97	52.84' LT	613.52
10	51+86.97	52.90' LT	613.67
11	51+90.38	51.62' LT	MATCH EX.
12	51+77.01	49.27' LT	613.24
13	51+82.01	49.34' LT	613.31
14	51+82.04	46.98' LT	613.28
15	51+62.61	57.58' LT	612.93
16	51+69.99	51.37' LT	613.05
17	51+76.29	47.87' LT	613.25
18	51+82.07	45.34' LT	613.29



57TH STREET



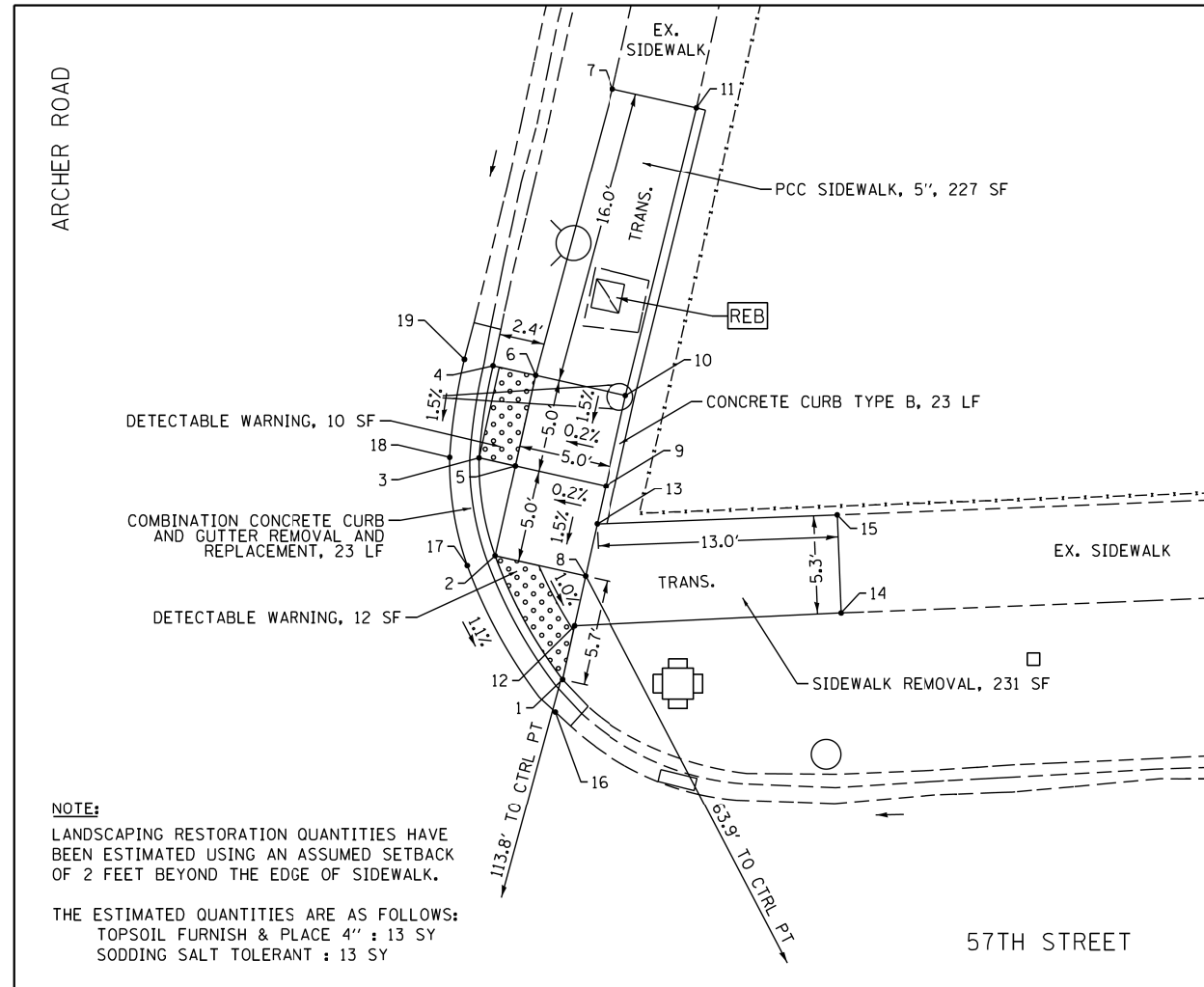
NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 8 SY
 SODDING SALT TOLERANT : 8 SY

CONTROL POINTS:
 CUT CROSS ELEV: 614.89 STA: 50+53.51 OS: 34.32' RT N: 1865690.921 E: 1126888.704
 CUT CROSS ELEV: 614.507 STA: 51+24.74 OS: 79.87' RT N: 1865749.699 E: 1126949.487

**SOUTHWEST CORNER
 ARCHER ROAD AND 57TH STREET**

POINT No.	STATION	OFFSET	ELEV.
1	50+61.98	58.13' LT	MATCH EX.
2	50+65.79	54.95' LT	MATCH EX.
3	50+69.72	50.35' LT	612.92
4	50+75.10	50.76' LT	612.86
5	50+58.82	54.22' LT	613.16
6	50+62.67	51.01' LT	613.15
7	50+67.02	47.40' LT	612.98
8	50+68.89	43.65' LT	613.00
9	50+52.73	46.28' LT	MATCH EX.
10	50+56.50	43.13' LT	MATCH EX.
11	50+77.14	50.92' LT	612.86
12	50+69.63	42.18' LT	613.01



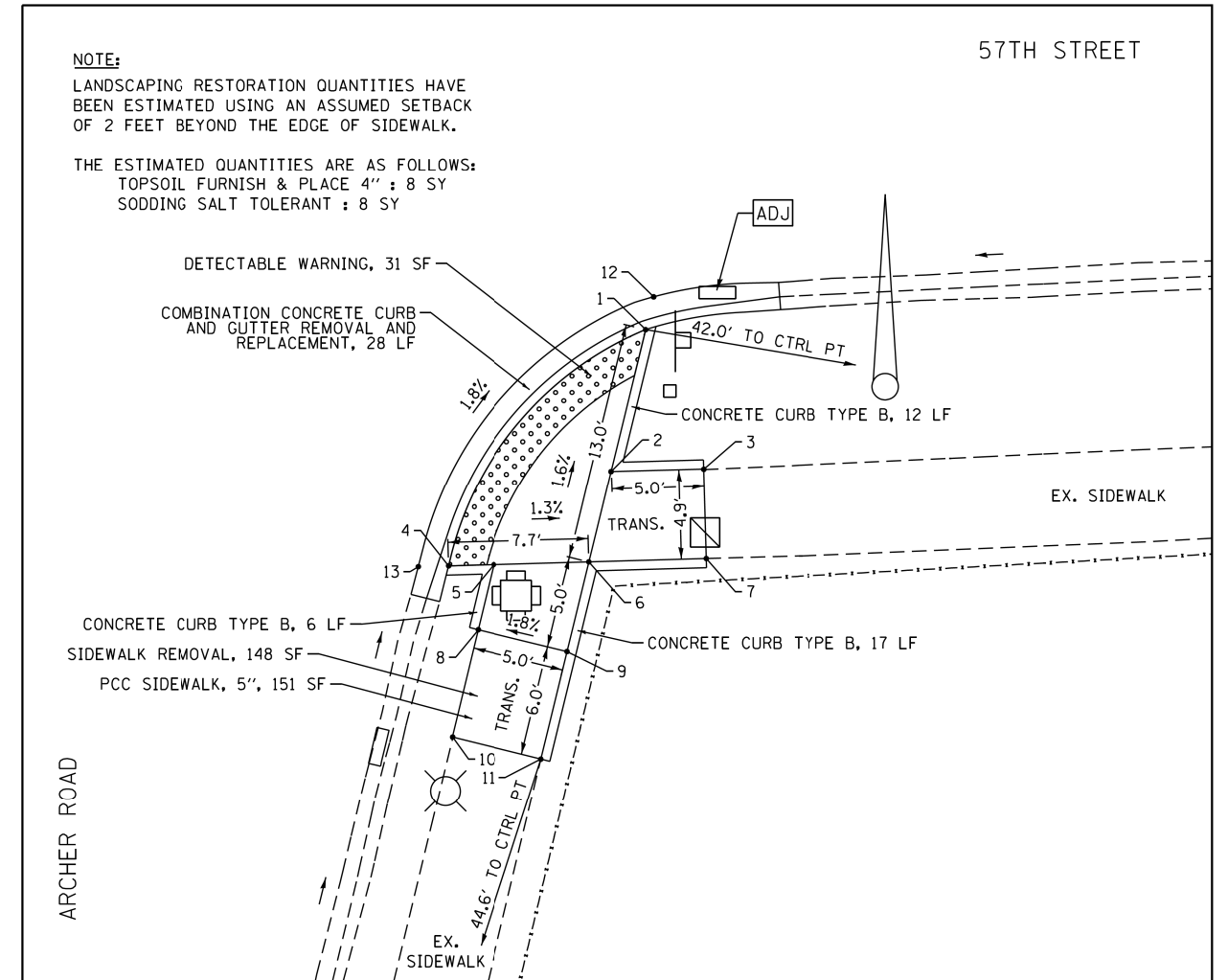


NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 13 SY
 SODDING SALT TOLERANT : 13 SY

CONTROL POINTS:
 CUT CROSS CUT CROSS
 ELEV: 614.89 ELEV: 614.507
 STA: 50+53.51 STA: 51+24.74
 OS: 34.32' RT OS: 79.87' RT
 N: 1865690.921 N: 1865749.699
 E: 1126888.704 E: 1126949.487

**NORTHEAST CORNER
 ARCHER ROAD AND 57TH STREET**

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	51+67.24	38.06' RT	613.29
2	51+72.87	32.98' RT	613.37
3	51+77.84	30.92' RT	613.40
4	51+82.84	30.51' RT	613.47
5	51+77.87	32.92' RT	613.43
6	51+82.87	32.85' RT	613.50
7	51+98.88	33.34' RT	MATCH EX.
8	51+72.94	37.98' RT	613.36
9	51+77.94	37.92' RT	613.44
10	51+82.94	37.85' RT	613.51
11	51+98.94	37.99' RT	MATCH EX.
12	51+70.19	38.02' RT	613.33
13	51+75.81	37.95' RT	613.41
14	51+74.20	51.90' RT	MATCH EX.
15	51+79.28	50.47' RT	MATCH EX.
16	51+65.44	38.08' RT	613.29
17	51+72.01	31.66' RT	613.38
18	51+77.51	29.37' RT	613.41
19	51+82.82	28.93' RT	613.48

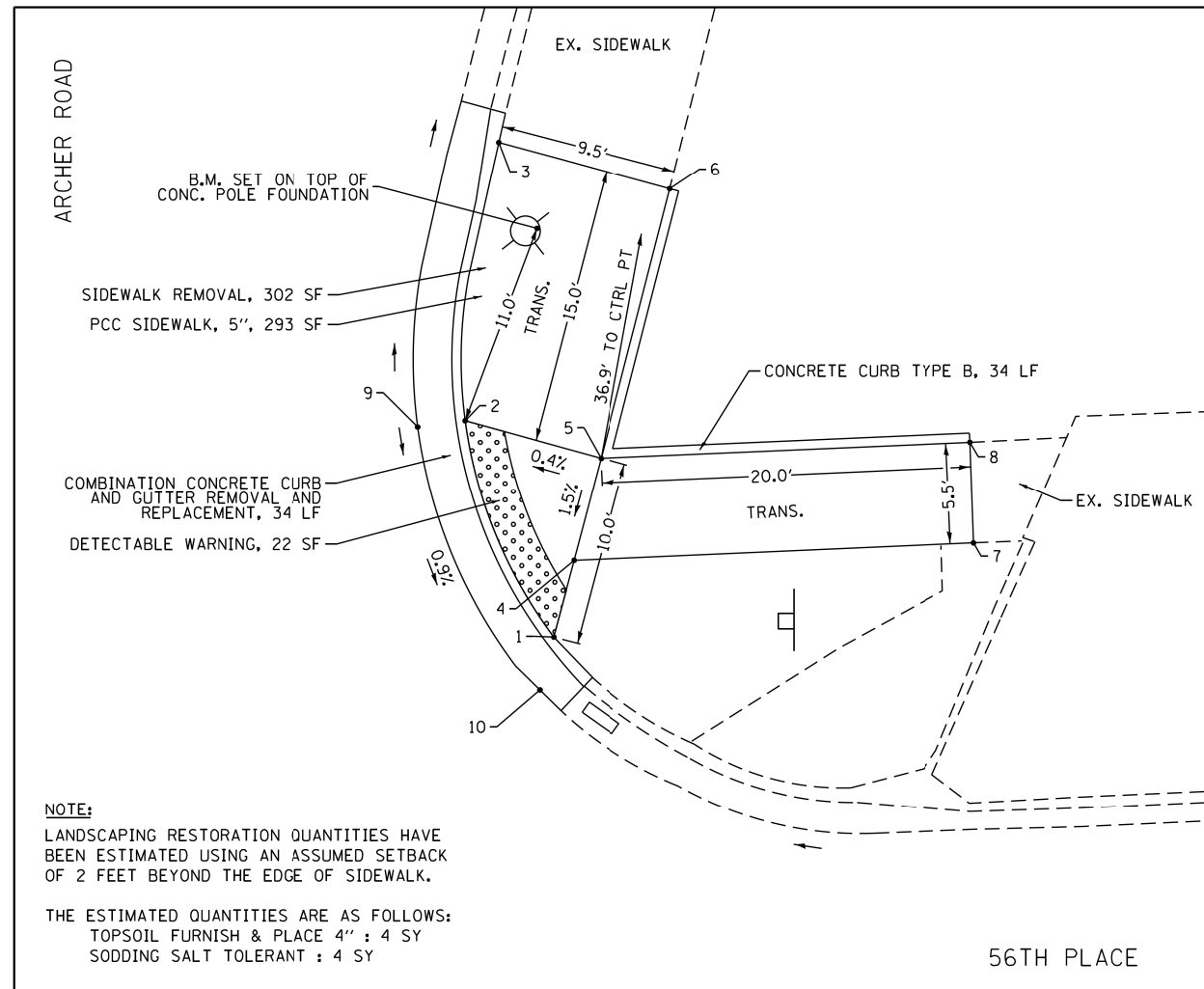


NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 8 SY
 SODDING SALT TOLERANT : 8 SY

CONTROL POINTS:
 CUT CROSS CUT CROSS
 ELEV: 614.89 ELEV: 614.507
 STA: 50+53.51 STA: 51+24.74
 OS: 34.32' RT OS: 79.87' RT
 N: 1865690.921 N: 1865749.699
 E: 1126888.704 E: 1126949.487

**SOUTHEAST CORNER
 ARCHER ROAD AND 57TH STREET**

ADA RAMP ELEVATION TABLE			
POINT No.:	STATION	OFFSET	ELEV.
1	51+21.94	37.96' RT	613.35
2	51+14.02	37.91' RT	613.48
3	51+15.32	42.77' RT	MATCH EX.
4	51+06.96	30.48' RT	613.66
5	51+07.60	32.88' RT	613.63
6	51+08.94	37.88' RT	613.56
7	51+10.58	44.04' RT	MATCH EX.
8	51+03.97	32.86' RT	613.56
9	51+03.94	37.86' RT	613.65
10	50+97.98	32.82' RT	MATCH EX.
11	50+97.93	37.82' RT	MATCH EX.
12	51+23.77	37.97' RT	613.36
13	51+06.53	28.89' RT	613.67

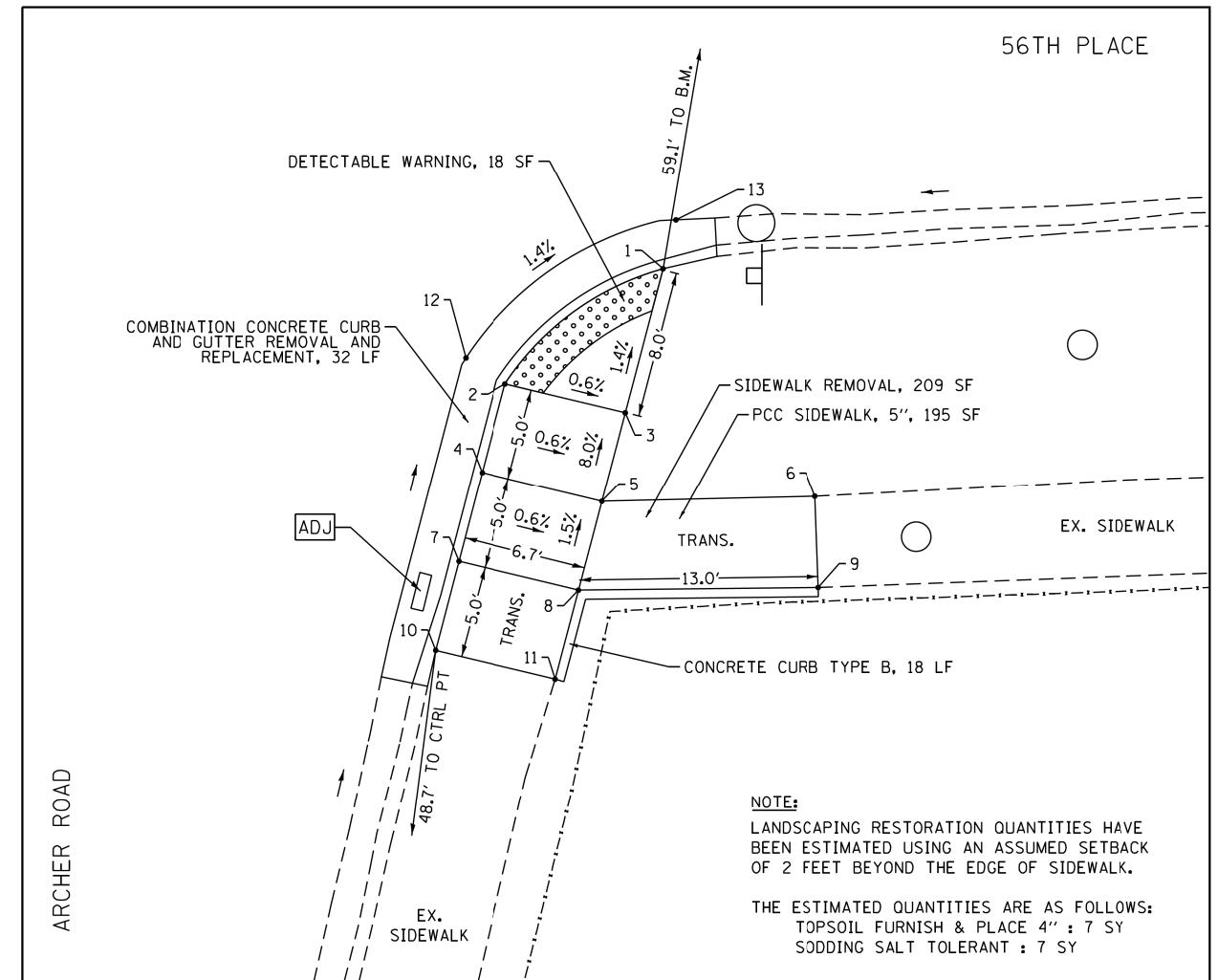


NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 4 SY
 SODDING SALT TOLERANT : 4 SY

CONTROL POINTS:
 CUT CROSS BENCHMARK
 ELEV: 612.88 ELEV: 613.13
 STA: 55+55.08 STA: 55+29.52
 OS: 38.29' RT OS: 34.00' RT
 N: 1866177.996 N: 1866154.122
 E: 1127008.486 E: 1126998.403

NORTHEAST CORNER
 ARCHER ROAD AND 56TH PLACE

POINT No.	STATION	OFFSET	ELEV.
1	55+08.29	39.97' RT	612.20
2	55+18.56	32.63' RT	612.32
3	55+33.59	30.92' RT	MATCH EX.
4	55+12.56	40.08' RT	612.26
5	55+18.27	40.22' RT	612.35
6	55+33.27	40.50' RT	MATCH EX.
7	55+18.48	60.91' RT	MATCH EX.
8	55+23.73	59.46' RT	MATCH EX.
9	55+17.64	30.22' RT	612.38
10	55+05.35	39.90' RT	612.25



NOTE:
 LANDSCAPING RESTORATION QUANTITIES HAVE BEEN ESTIMATED USING AN ASSUMED SETBACK OF 2 FEET BEYOND THE EDGE OF SIDEWALK.
 THE ESTIMATED QUANTITIES ARE AS FOLLOWS:
 TOPSOIL FURNISH & PLACE 4" : 7 SY
 SODDING SALT TOLERANT : 7 SY

CONTROL POINTS:
 BENCHMARK CUT CROSS
 ELEV: 613.13 ELEV: 613.69
 STA: 55+29.52 STA: 53+99.09
 OS: 34.00' RT OS: 35.73' RT
 N: 1866154.122 N: 186602.821
 E: 1126998.403 E: 1126969.949

SOUTHEAST CORNER
 ARCHER ROAD AND 56TH PLACE

POINT No.	STATION	OFFSET	ELEV.
1	54+70.55	37.93' RT	612.42
2	54+62.48	31.01' RT	612.57
3	54+62.46	37.74' RT	612.53
4	54+57.48	30.89' RT	612.97
5	54+57.46	37.62' RT	612.93
6	54+60.41	48.85' RT	MATCH EX.
7	54+52.48	30.77' RT	613.04
8	54+52.46	37.50' RT	613.00
9	54+55.62	50.18' RT	MATCH EX.
10	54+47.48	30.66' RT	MATCH EX.
11	54+47.46	37.38' RT	MATCH EX.
12	54+63.35	28.58' RT	612.63
13	54+73.29	37.99' RT	612.47

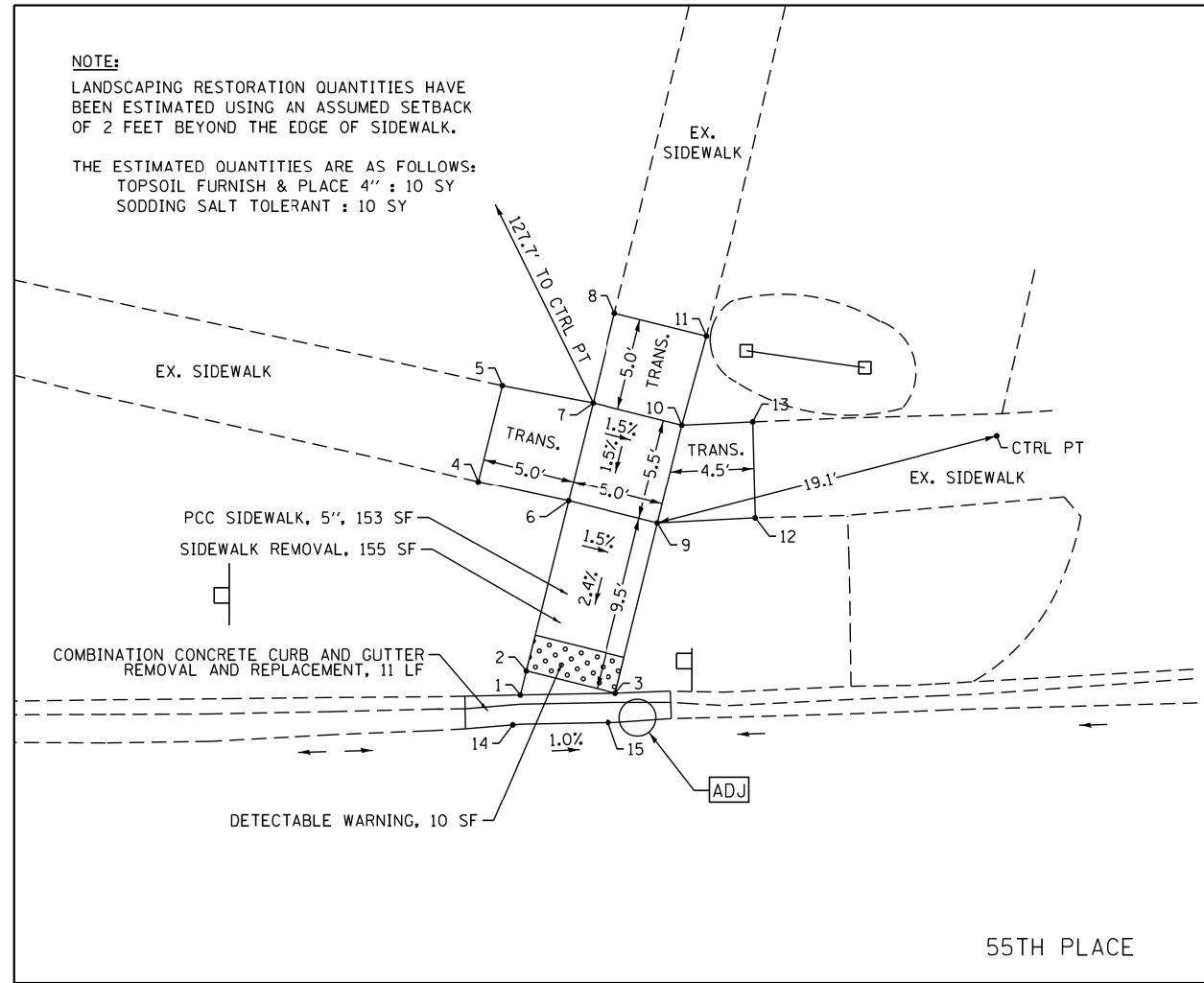


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PLOT SCALE = 10.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 10/17/2016	CHECKED - MTC	REVISED -
	DATE - 09/05/2016	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ARCHER ROAD ADA RAMP DETAILS			
SCALE: 1" = 5'	SHEET 21	OF 23 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3565	(46-2)RS	COOK	49	35
CONTRACT NO. 60Y87				
ILLINOIS FED. AID PROJECT				

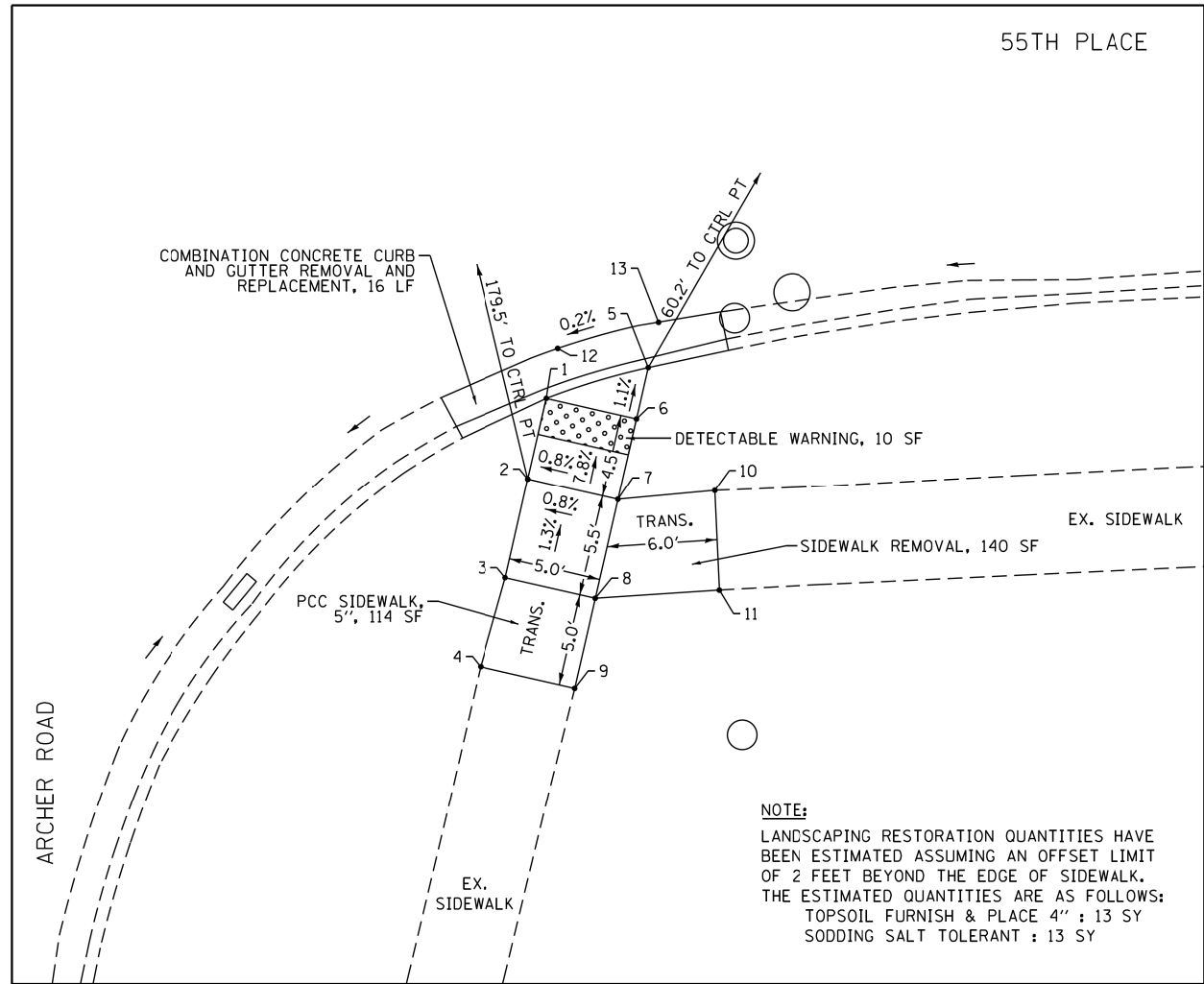


CONTROL POINTS:
 CUT CROSS ELEV: 614.40
 STA: 62+69.98
 OS: 95.31' RT
 N: 1866924.605
 E: 1127101.372

CUT CROSS ELEV: 615.776
 STA: 61+58.22
 OS: 133.54' RT
 N: 1866808.319
 E: 1127179.864

NORTHEAST CORNER
 ARCHER ROAD AND 55TH PLACE

POINT No.	STATION	OFFSET	ELEV.
1	61+51.37	105.24' RT	614.94
2	61+52.38	105.82' RT	614.96
3	61+50.57	110.33' RT	614.89
4	61+61.25	105.47' RT	MATCH EX.
5	61+65.26	107.90' RT	MATCH EX.
6	61+59.50	110.01' RT	615.72
7	61+63.57	112.48' RT	615.79
8	61+67.26	114.75' RT	MATCH EX.
9	61+57.65	114.49' RT	615.65
10	61+61.71	116.94' RT	615.72
11	61+65.32	119.33' RT	MATCH EX.
12	61+56.94	119.74' RT	MATCH EX.
13	61+61.16	120.74' RT	MATCH EX.
14	61+50.11	104.52' RT	614.95
15	61+49.35	109.63' RT	614.90



CONTROL POINTS:
 CUT CROSS ELEV: 614.40
 STA: 62+69.98
 OS: 95.31' RT
 N: 1866924.605
 E: 1127101.372

CUT CROSS ELEV: 615.776
 STA: 61+58.22
 OS: 133.54' RT
 N: 1866808.319
 E: 1127179.864

SOUTHEAST CORNER
 ARCHER ROAD AND 55TH PLACE

POINT No.	STATION	OFFSET	ELEV.
1	61+19.57	88.84' RT	614.95
2	61+15.95	87.22' RT	615.30
3	61+11.51	85.29' RT	615.37
4	61+07.52	83.33' RT	MATCH EX.
5	61+20.29	94.53' RT	614.96
6	61+18.02	93.50' RT	614.99
7	61+14.43	91.90' RT	615.34
8	61+10.01	89.98' RT	615.41
9	61+05.98	88.27' RT	MATCH EX.
10	61+14.24	97.16' RT	MATCH EX.
11	61+09.66	96.69' RT	MATCH EX.
12	61+21.78	89.85' RT	614.99
13	61+22.28	95.45' RT	615.00



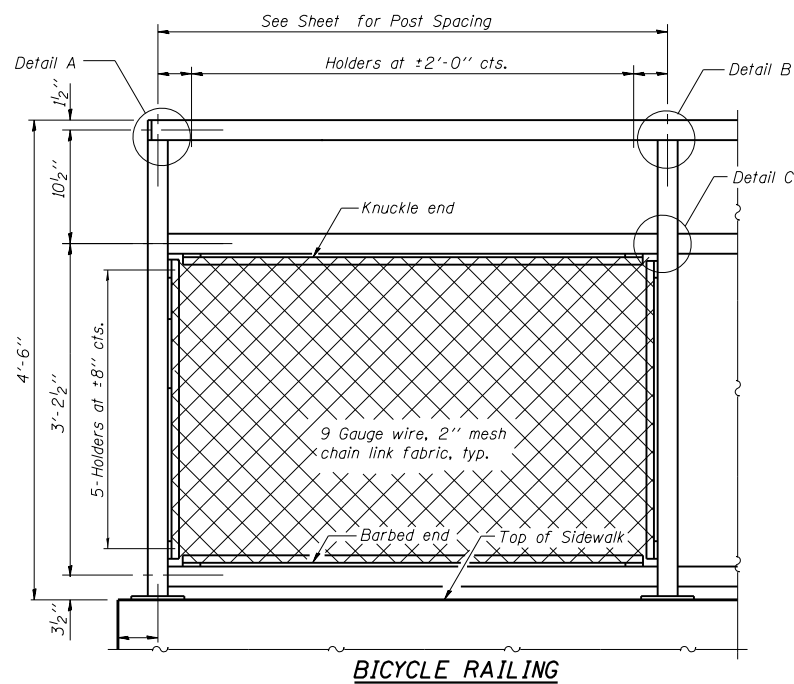
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PLOT SCALE = 10.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 10/17/2016	CHECKED - MTC	REVISED -
	DATE - 09/05/2016	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

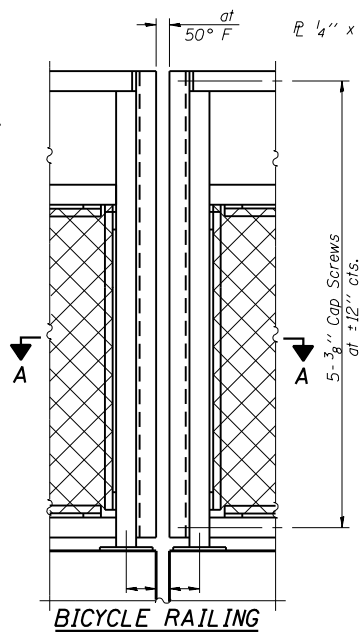
ARCHER ROAD
 ADA RAMP DETAILS

SCALE: 1" = 5'
 SHEET 23 OF 23 SHEETS
 STA. TO STA.

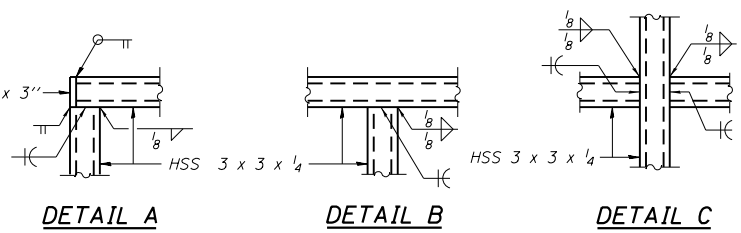
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3565	(46-2)RS	COOK	49	37
CONTRACT NO. 60Y87				
ILLINOIS FED. AID PROJECT				



BICYCLE RAILING

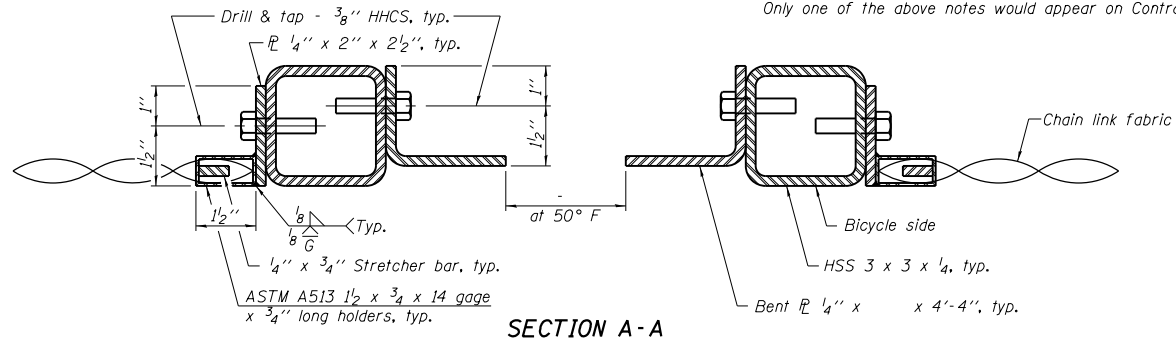


BICYCLE RAILING

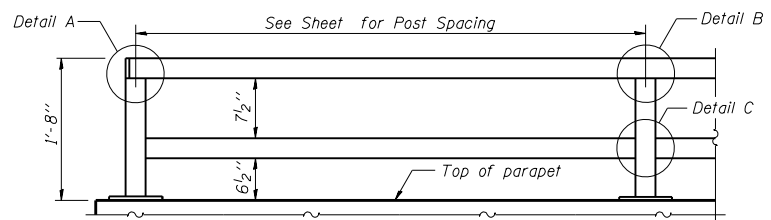


DETAIL A DETAIL B DETAIL C

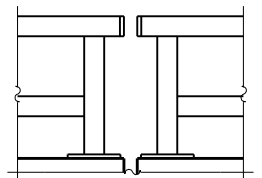
The designer should add the appropriate note as applicable.
 A. When railing is galvanized:
 All steel rail elements shall be galvanized according to Article 509.05 of the Standard Specifications.
 B. When railing is painted:
 All post, railing, splices, anchor devices, and bent plates shall be painted using the (List the appropriate paint system for Structural Steel).
 Only one of the above notes would appear on Contract Plans.



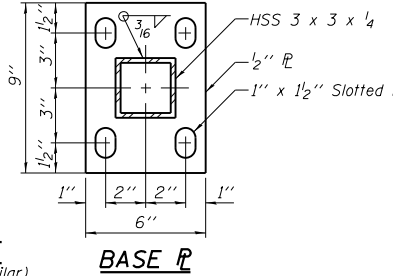
SECTION A-A



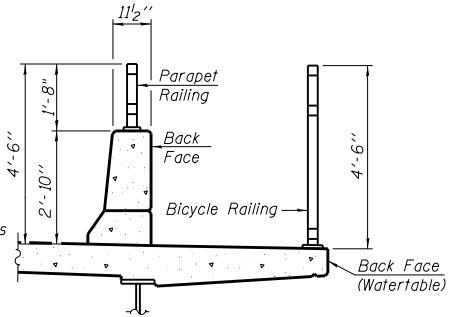
PARAPET RAILING ELEVATION
(Inside Face of Two Element Rail)



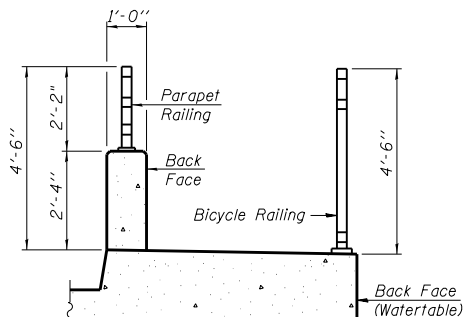
PARAPET RAILING ELEVATION AT EXPANSION JOINT
(Two Element Rail Shown - Three Element Rail Similar)



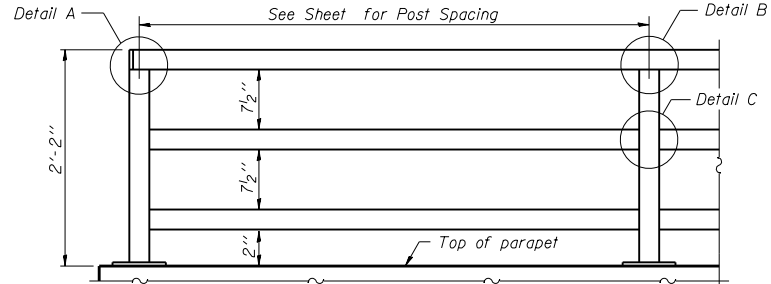
BASE PL



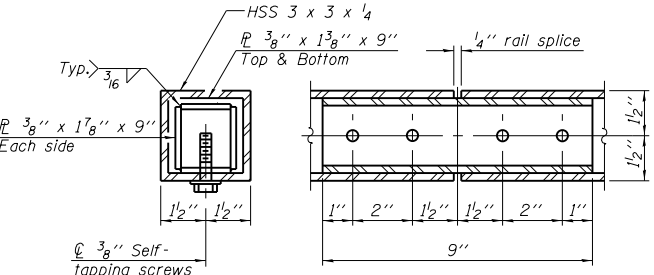
SECTION THRU DECK



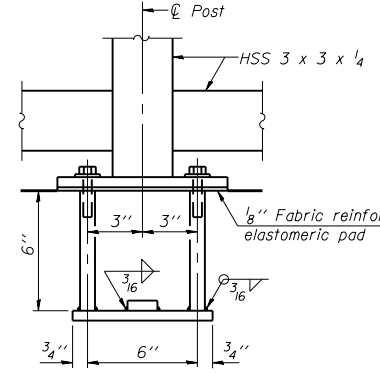
SECTION THRU SIDEWALK



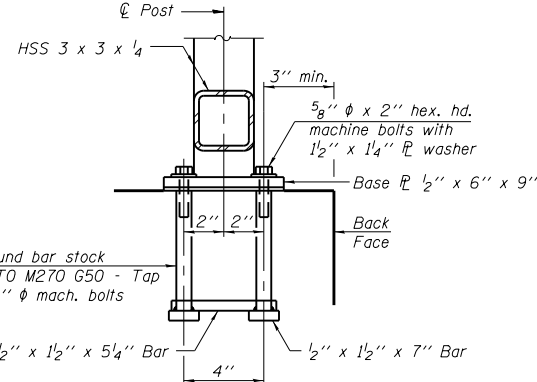
PARAPET RAILING ELEVATION
(Inside Face of Three Element Rail)



RAIL SPLICE



ANCHOR BOLT DETAILS



1" Round bar stock
 AASHTO M270 G50 - Tap
 for 5/8" φ mach. bolts

In lieu of the cast-in-place anchor device shown, the Contractor has the option of drilling and setting 3/8" φ anchor rods according to Article 509.06 of the Standard Specifications. Embedment shall be according to the manufacturer's specifications.

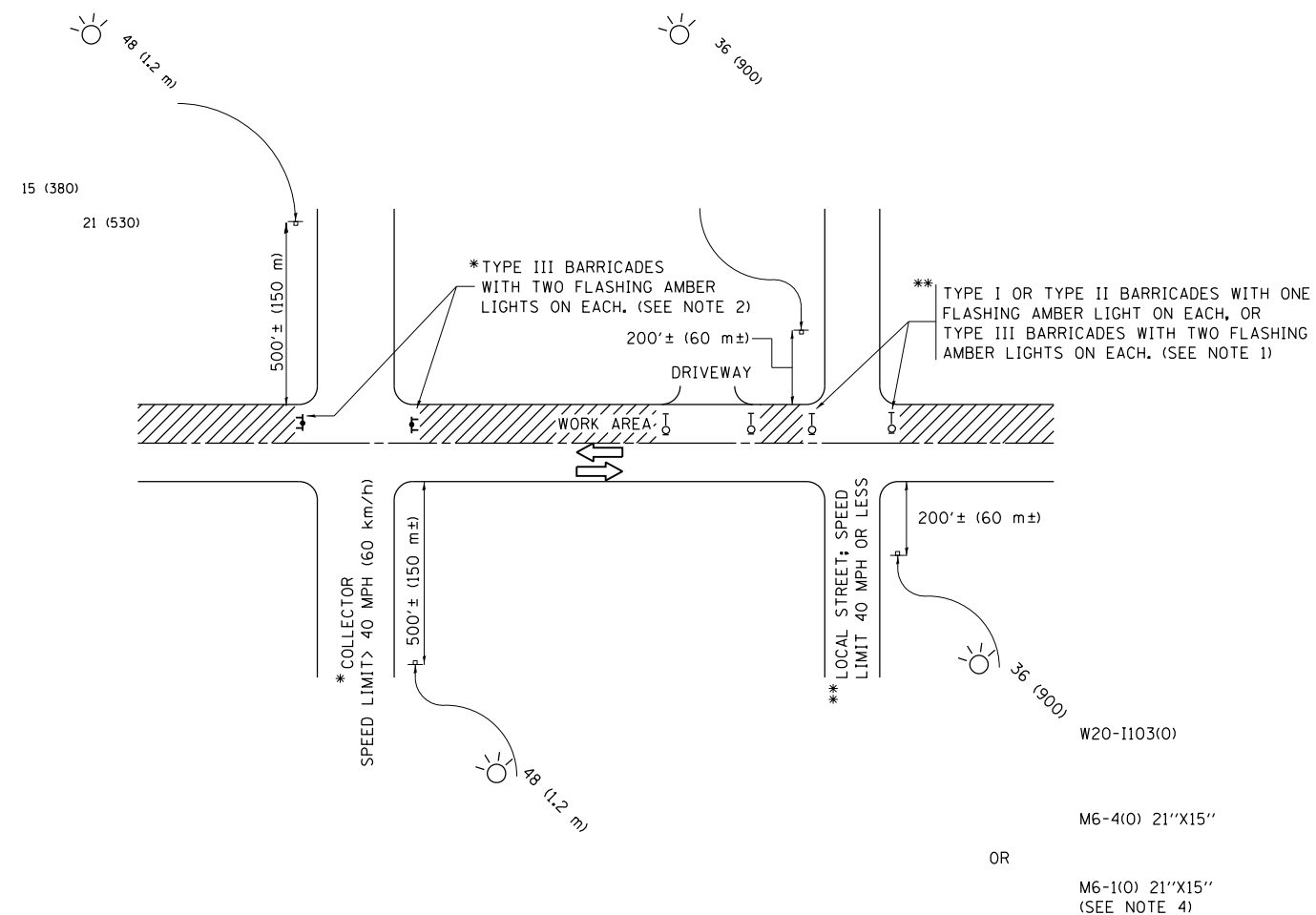
BILL OF MATERIAL

Item	Unit	Quantity
Bicycle Railing	Foot	
Parapet Railing	Foot	

R-29

1-12-15 (10'-0" Maximum Post Spacing)

BICYCLE RAILING STRUCTURE NO.



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

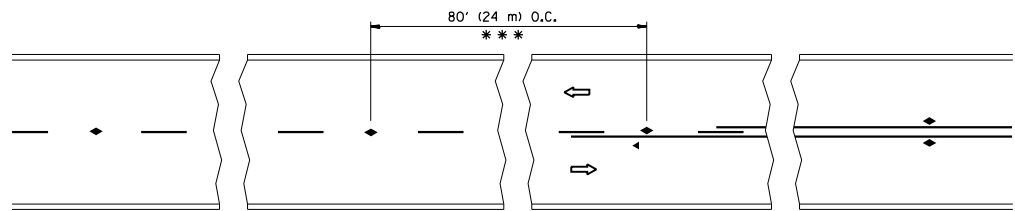
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	PLOT DATE = 10/31/2016	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

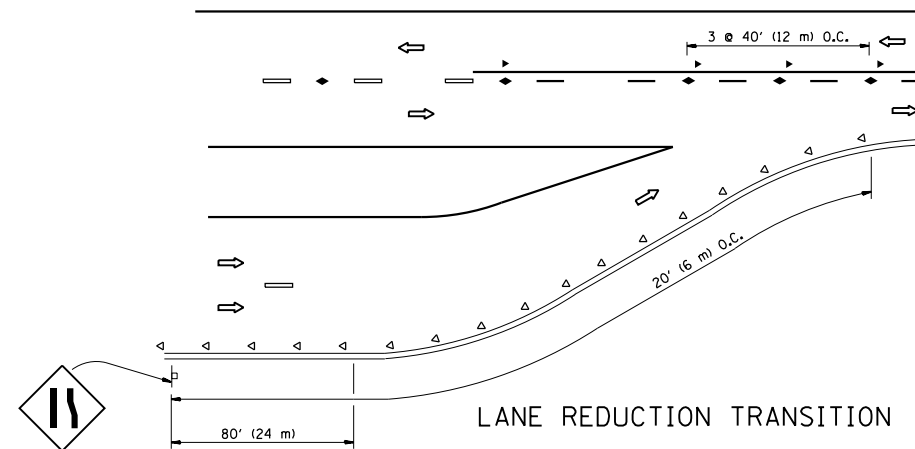
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO.	60Y87
ILLINOIS FED. AID PROJECT				

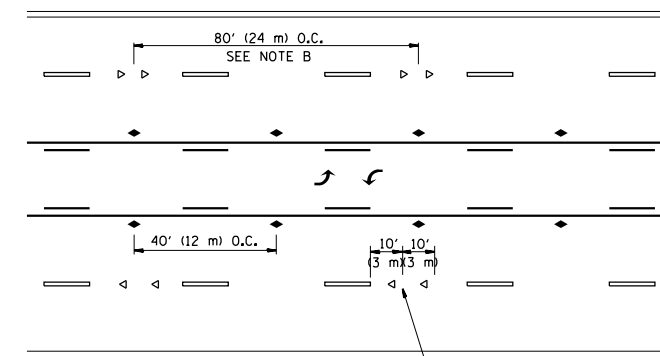


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

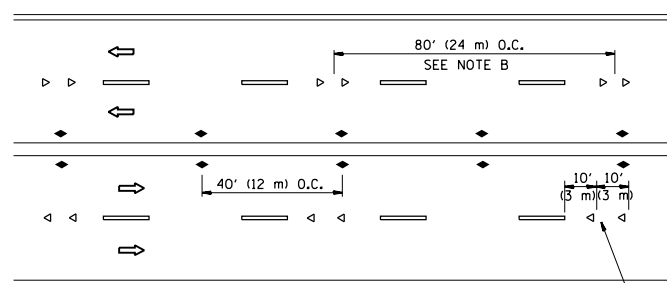
TWO-LANE/TWO-WAY



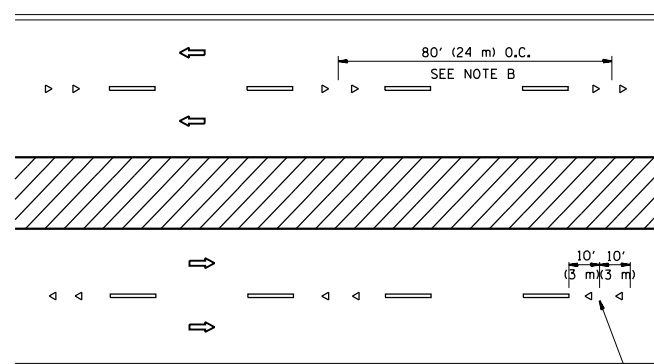
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

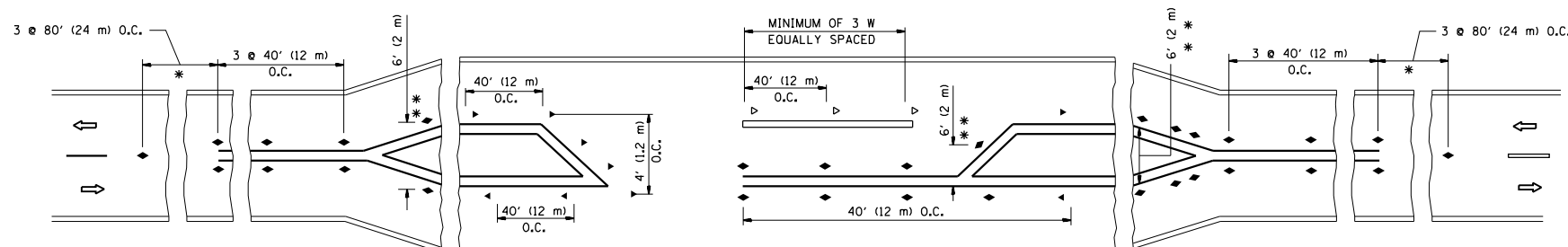
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

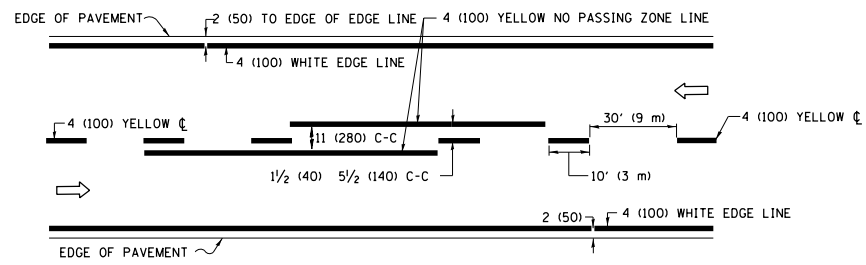
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

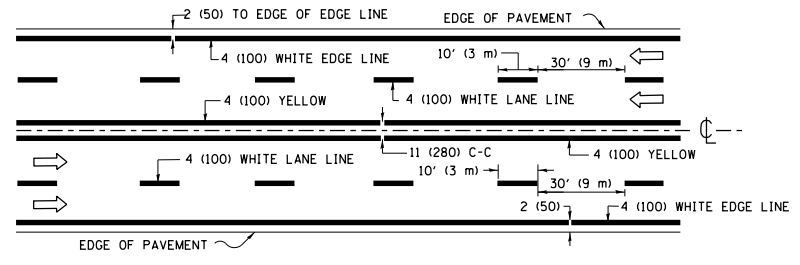
TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

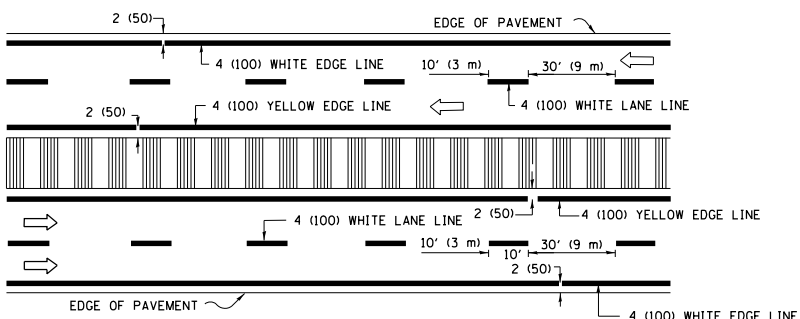
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3565	(46-2)RS	COOK	49	40
TC-11		CONTRACT NO.	60Y87	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

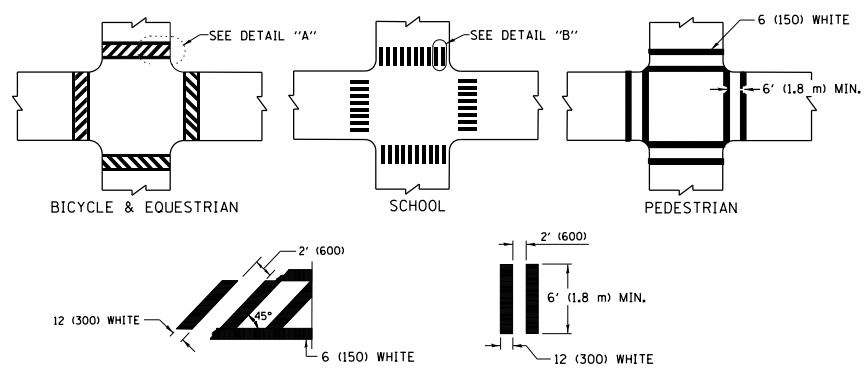


MULTI-LANE UNDIVIDED



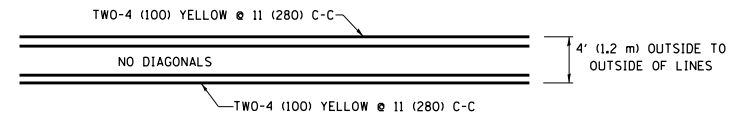
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

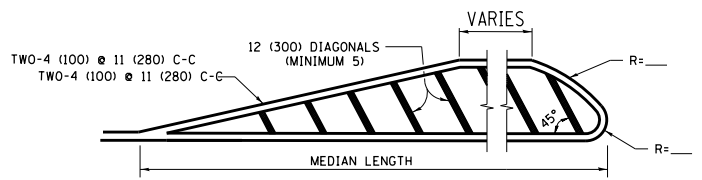


TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

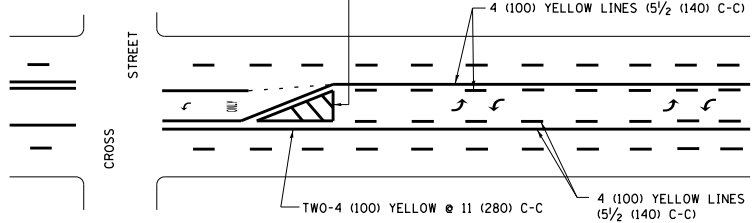


4' (1.2 m) WIDE MEDIANS ONLY

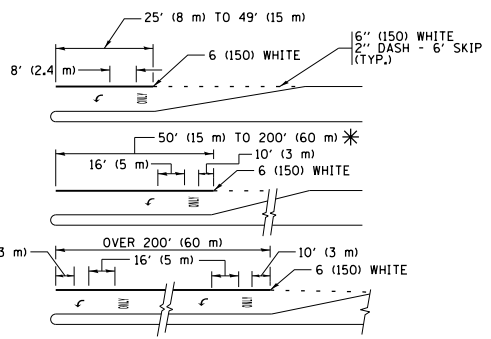


MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

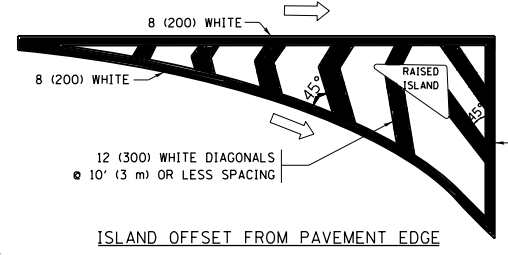


MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING

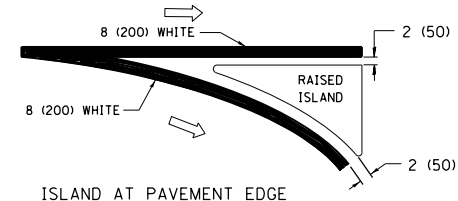


TYPICAL LEFT (OR RIGHT) TURN LANE TYPICAL TURN LANE MARKING

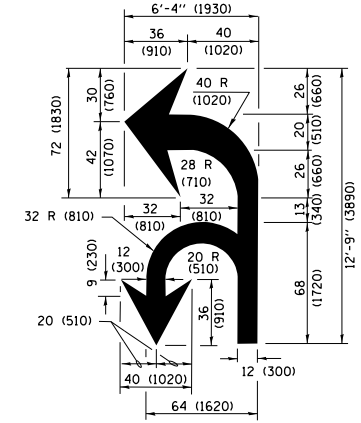
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".



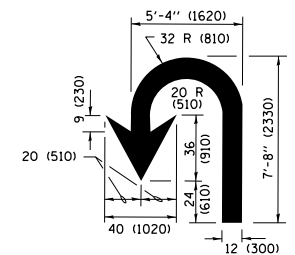
ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

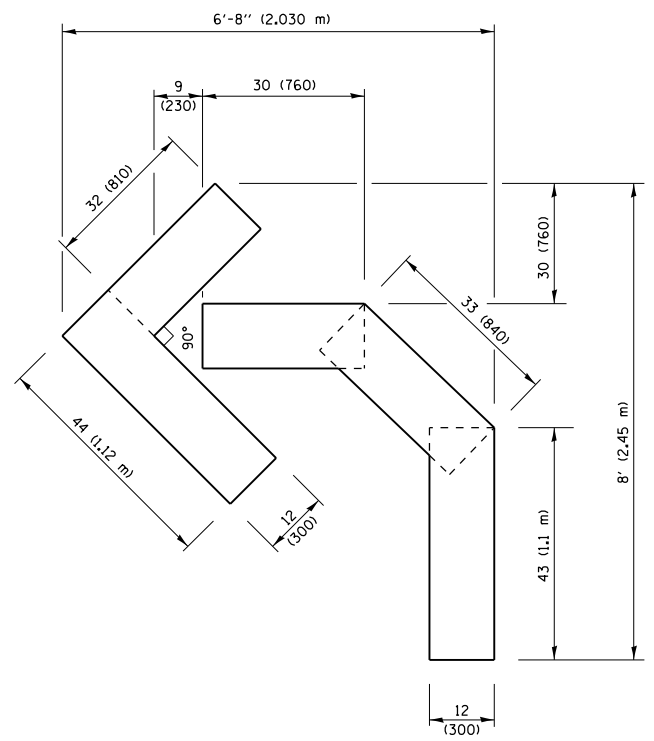
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Default	PLOT SCALE = 100.0000' / in.	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
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STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

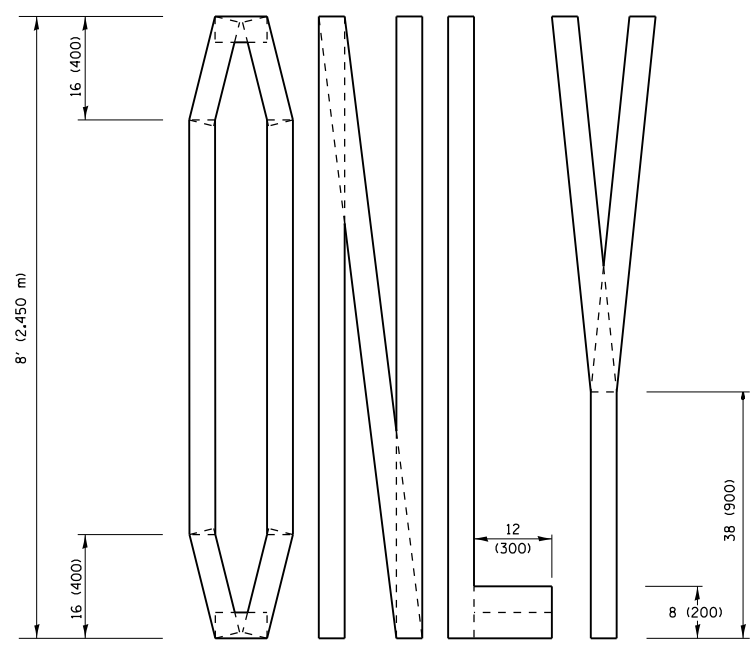
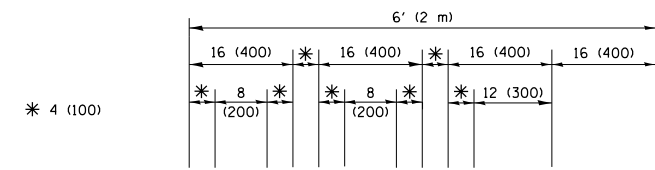
DISTRICT ONE TYPICAL PAVEMENT MARKINGS

SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.
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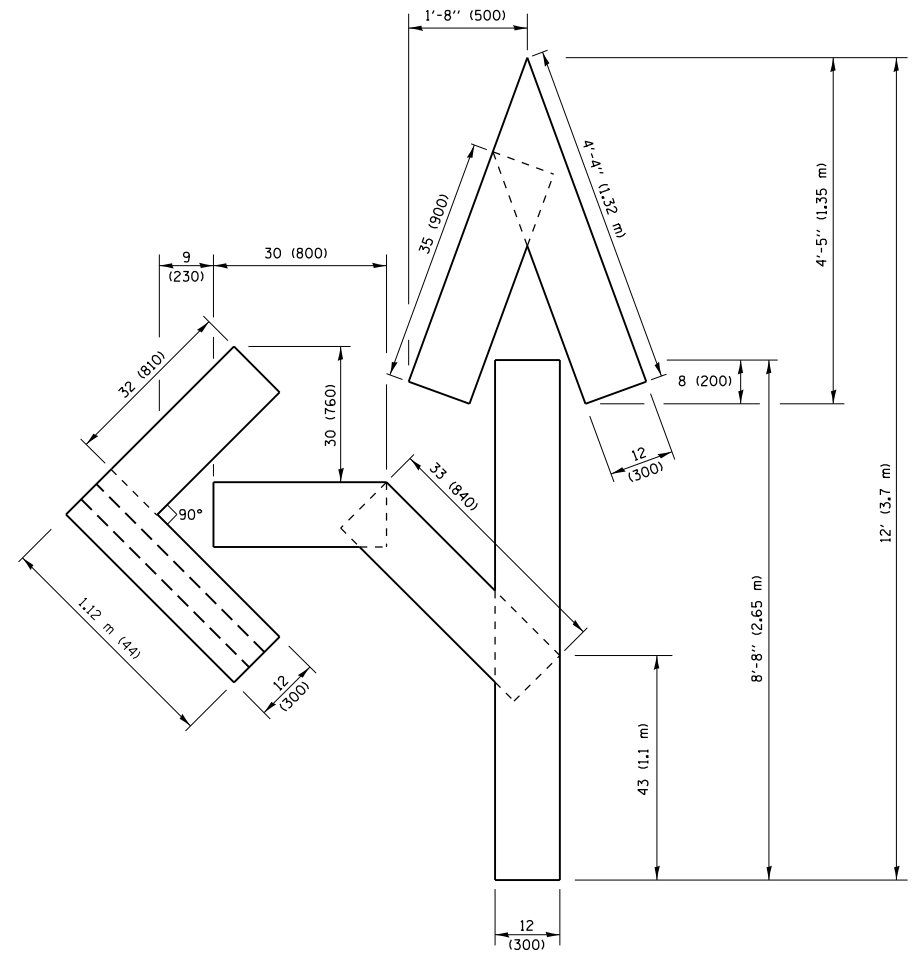
F.A.U R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3565	(46-2)RS	COOK	49	41
TC-13		CONTRACT NO.	60Y87	
ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

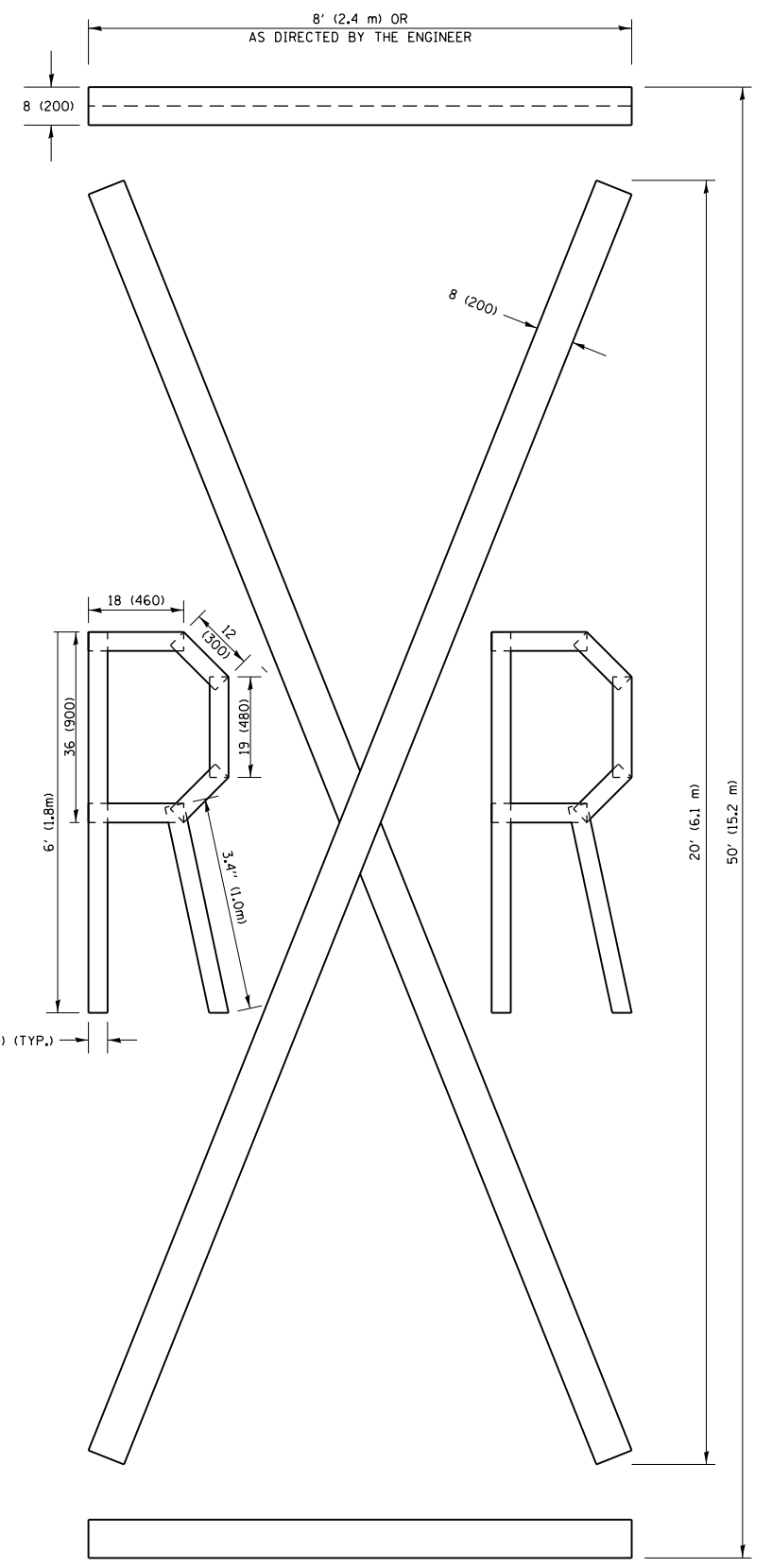


QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

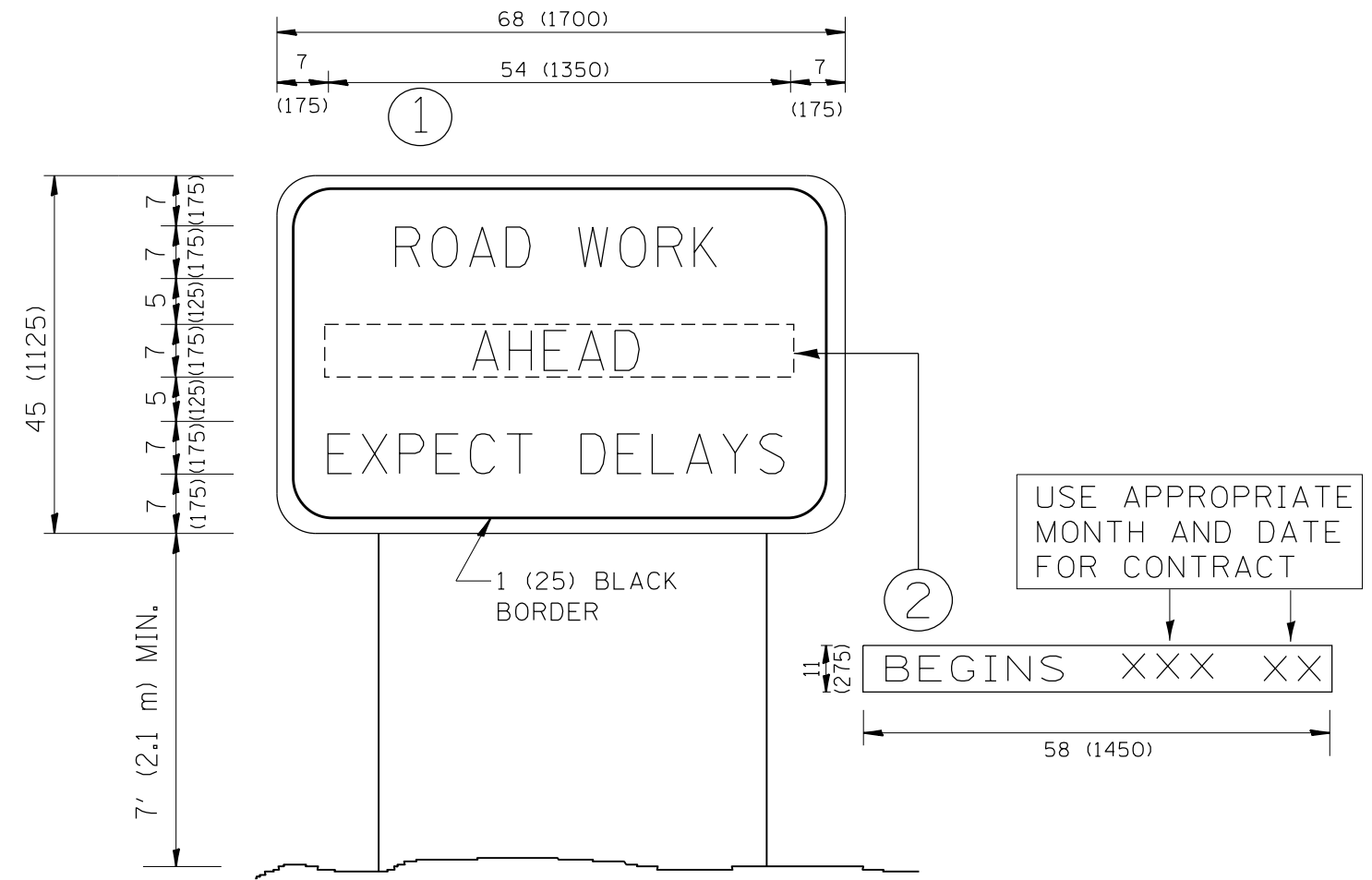
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = ledzmar	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
pw\11084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\018311\Drawings\Design\DistStd.dgn		CHECKED -	REVISED - E. GOMEZ 08-28-00
		DATE -	REVISED - E. GOMEZ 08-28-00
			REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3565	(46-2)RS	COOK	49	42
TC-16		CONTRACT NO.	60Y87	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

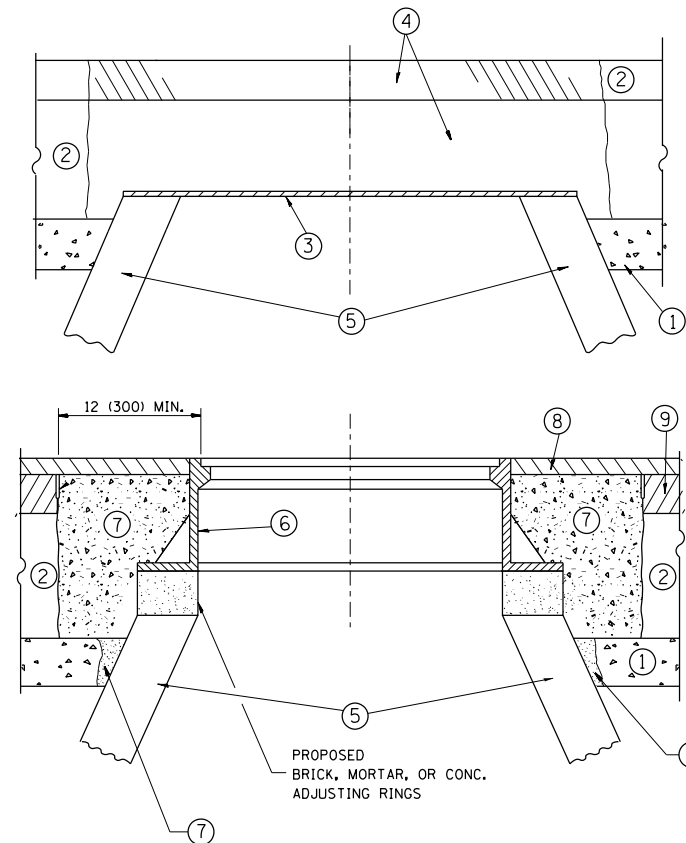
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	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 10/31/2016	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3565	(46-2)RS	COOK	49	43
TC-22		CONTRACT NO.	60Y87	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

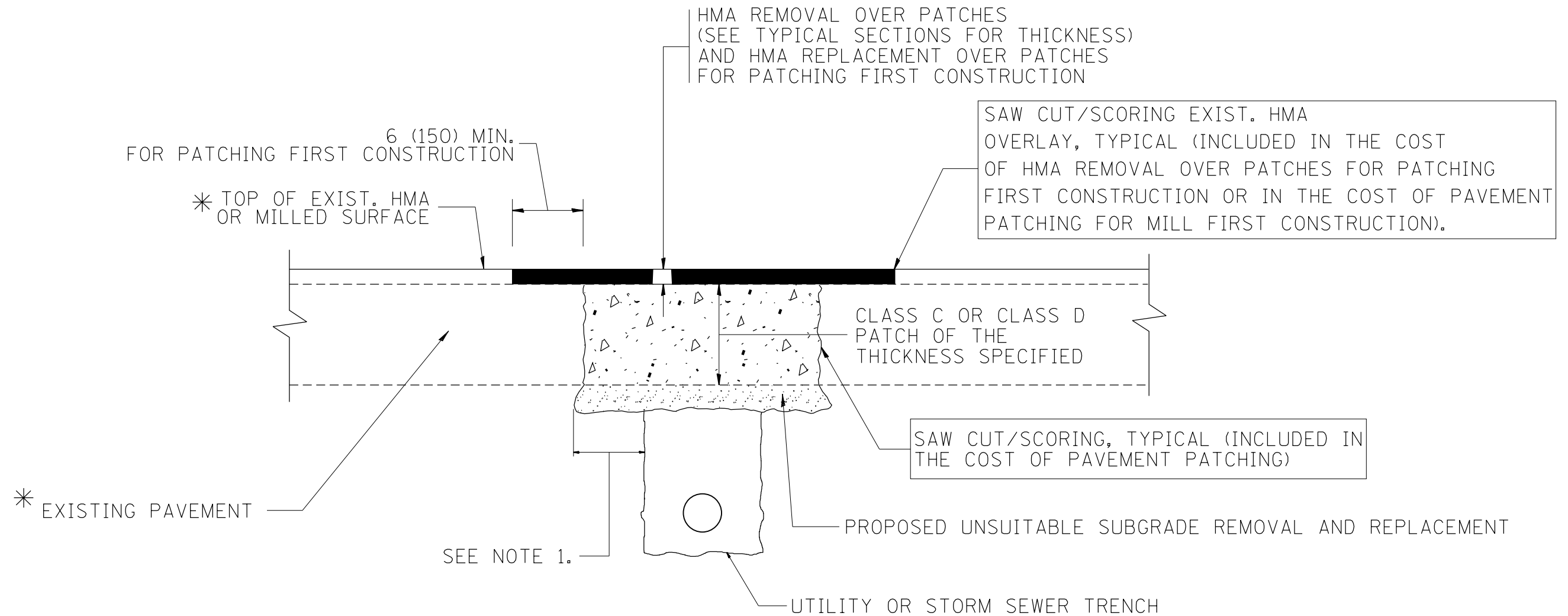
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = ldezma	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
pw\11084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI0311\Drawings\Design\DistStd.dgn		CHECKED -	REVISED - R. BORO 01-01-07
		DATE - 10-25-94	REVISED - R. BORO 03-09-11
			REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3565	(46-2)RS	COOK	49	44
BD600-03 (BD-8)		CONTRACT NO.	60Y87	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = ldezma	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI031\Drawings\Design\DistStd.dgn		CHECKED -	REVISED - R. BORO 01-01-07					3565	146-21RS	COOK	49	45
PLOT SCALE = 100.0000' / 1in.		DATE - 10-25-94	REVISED - R. BORO 09-04-07		BD400-04 (BD-22)			CONTRACT NO. 60Y87				
PLOT DATE = 10/31/2016			REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

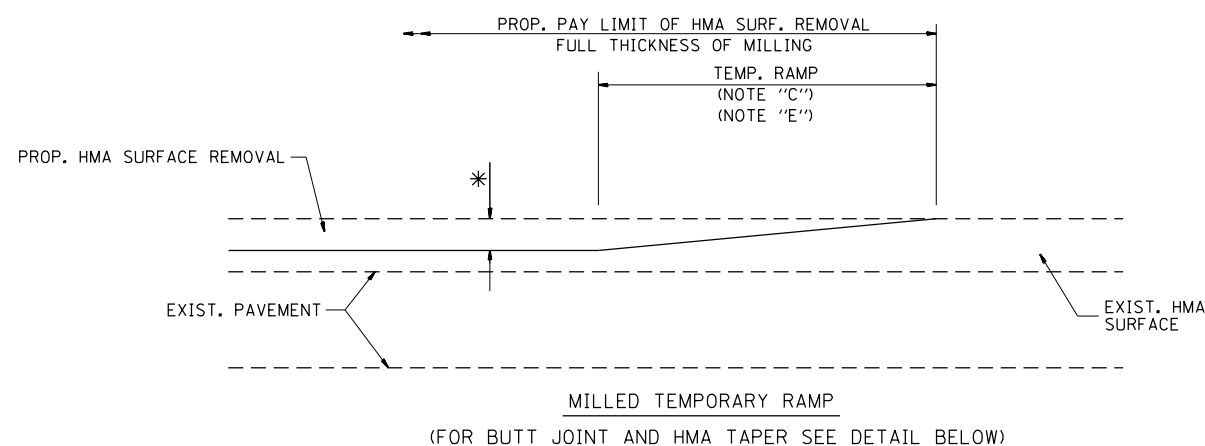
BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

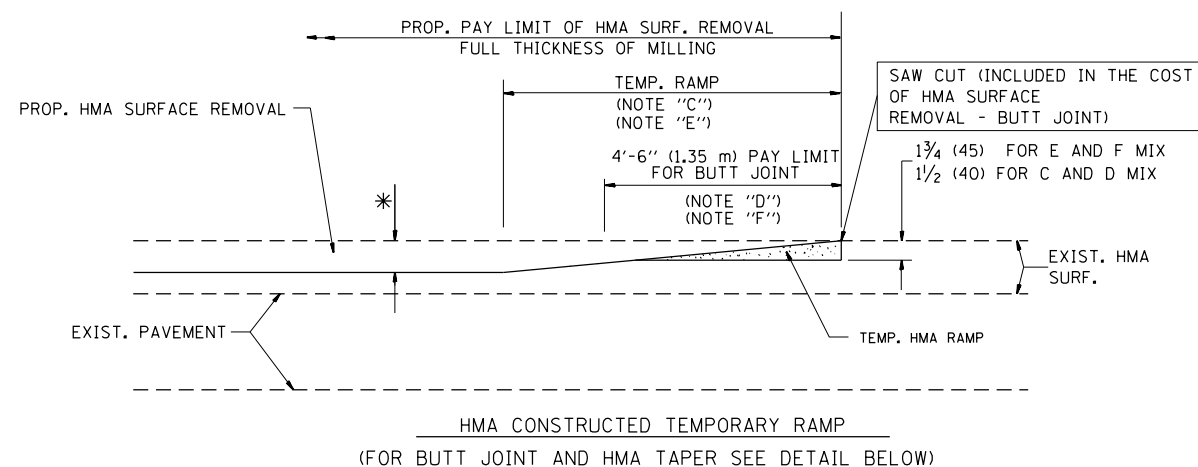
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = ldezma	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.U. RE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI0311\Drawings\Design\DistStd.dgn		REVISOR - A. ABBAS 03-21-97	REVISED - M. GOMEZ 01-22-01			3565	146-2)RS	COOK	49	46
PLOT SCALE = 100.0000' / 1".	CHECKED -	REVISOR - R. BORO 12-15-09				BD600-06 (BD-24)		CONTRACT NO. 60Y87		
PLOT DATE = 10/31/2016	DATE - 03-11-94					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

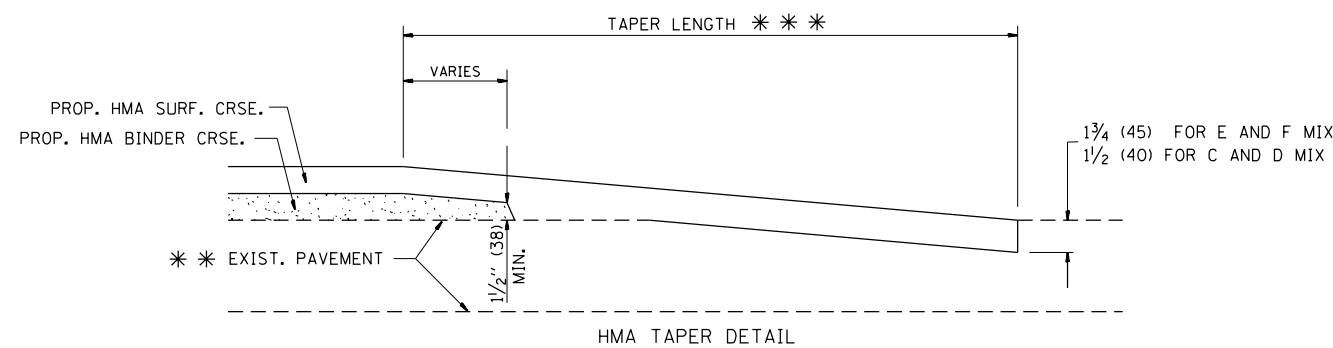
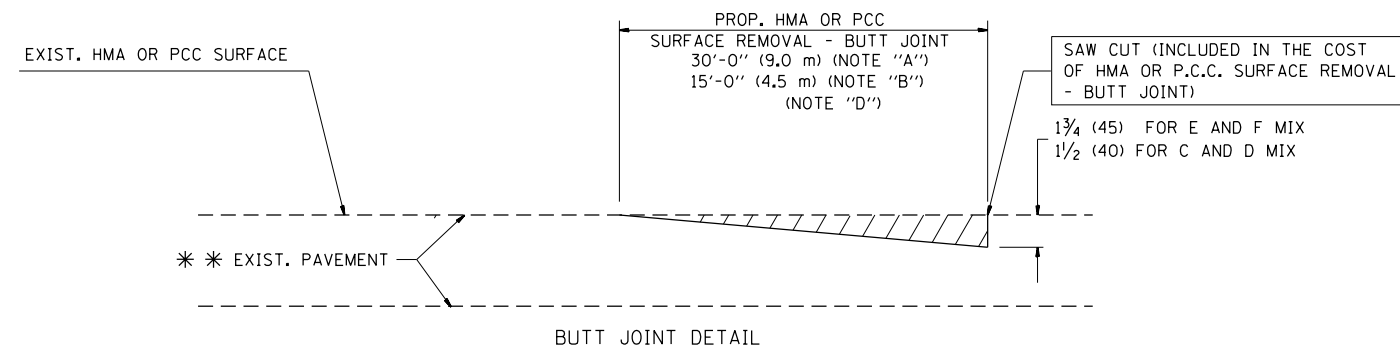


OPTION 1



OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

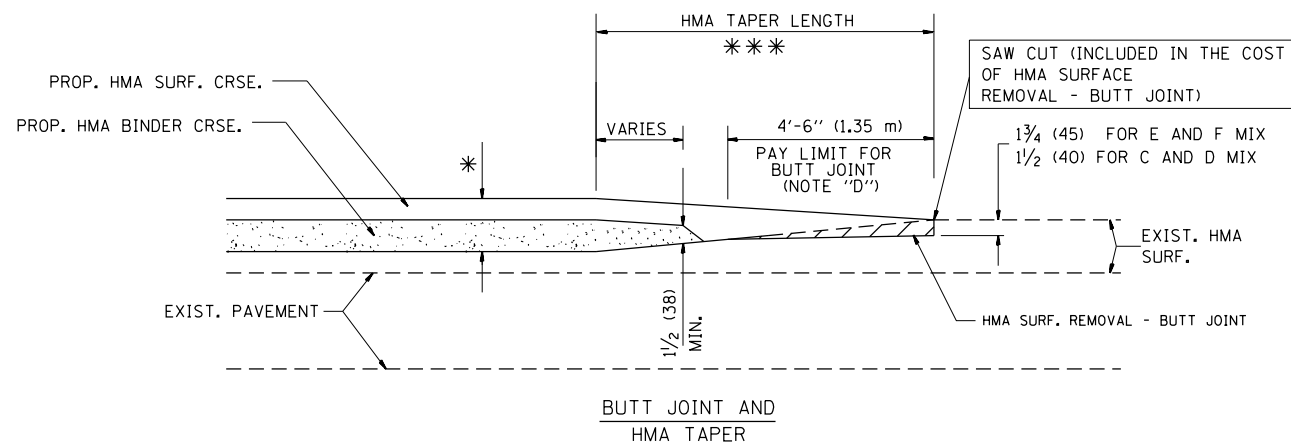
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- * * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

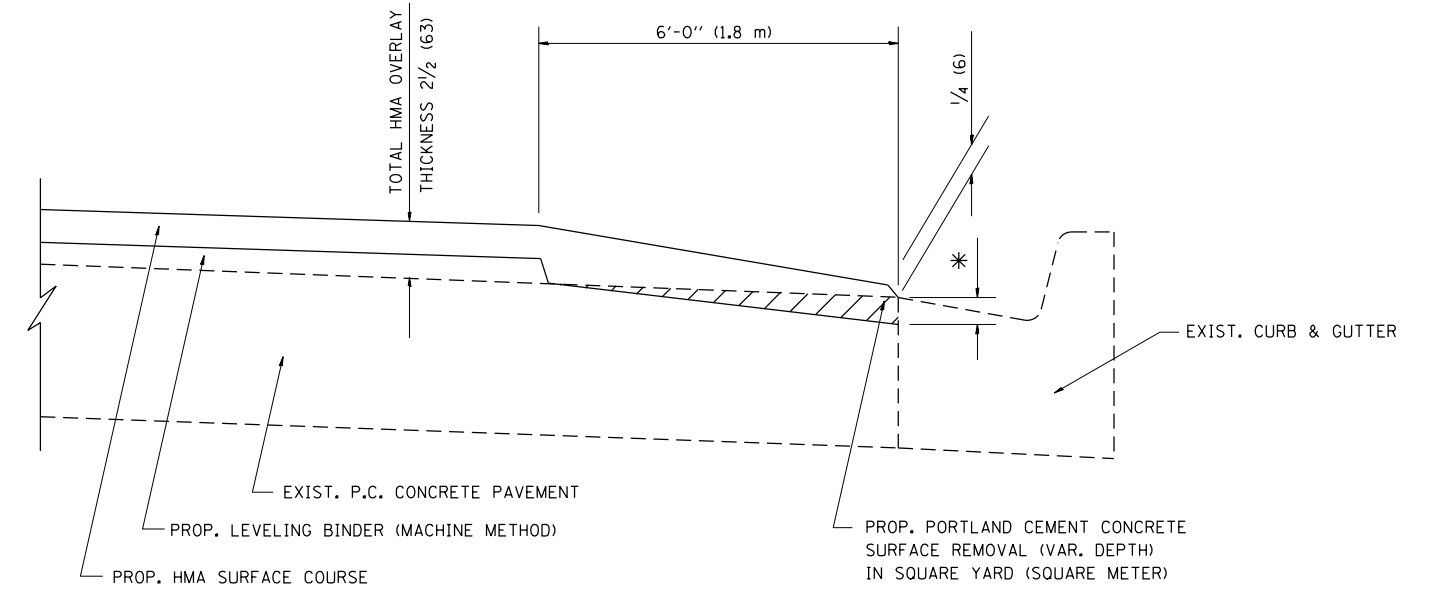
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	PLOT SCALE = 100.0000' / 1in.	DATE - 06-13-90	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 10/31/2016		REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3565	146-2/RS	COOK	49	47
BD400-05 BD32		CONTRACT NO. 60Y87		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



HMA TAPER AT
EDGE OF P.C.C PAVEMENT

HMA SURFACE	THICKNESS	LEVELING BINDER THICKNESS	* MILLING AT GUTTER FLAG
C OR D	1 1/2 (38)	1 (25)	1/4 (33)
E	1 3/4 (44)	3/4 (19)	1/2 (38)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = ledeznorm	DESIGNED - R. SHAH	REVISED - A. ABBAS 05-05-9
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Default	PLOT SCALE = 100.0000' / 1in.	DATE - 09-10-94	REVISED - R. BORO 01-01-07
	PLOT DATE = 10/31/2016		REVISED - JP CHANG 07-08-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

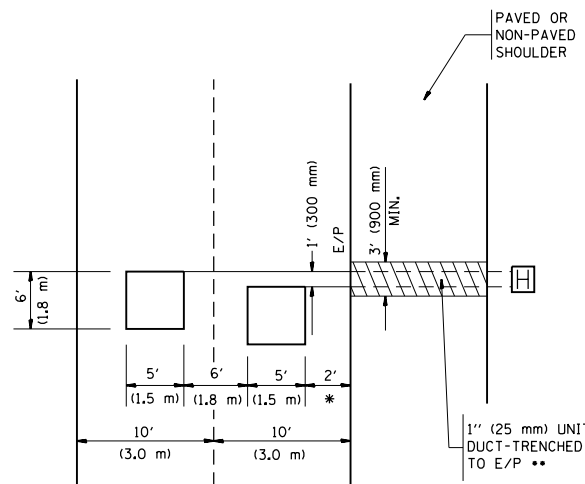
**HMA TAPER AT
EDGE OF P.C.C PAVEMENT**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3565	(46-2)RS	COOK	49	48
BD400-06 (BD33)		CONTRACT NO.	60Y87	
ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



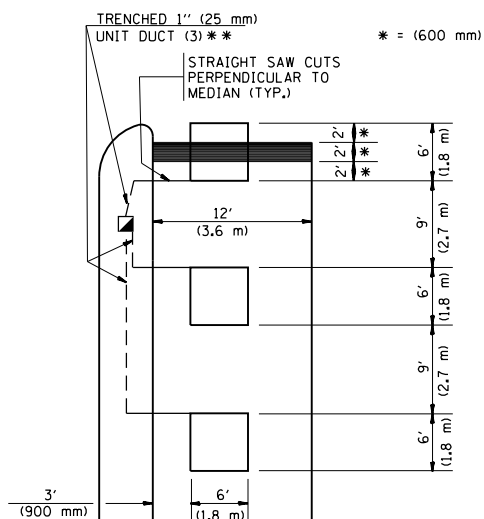
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

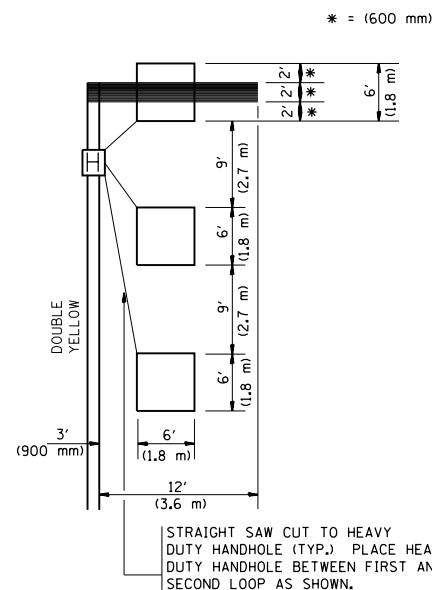


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

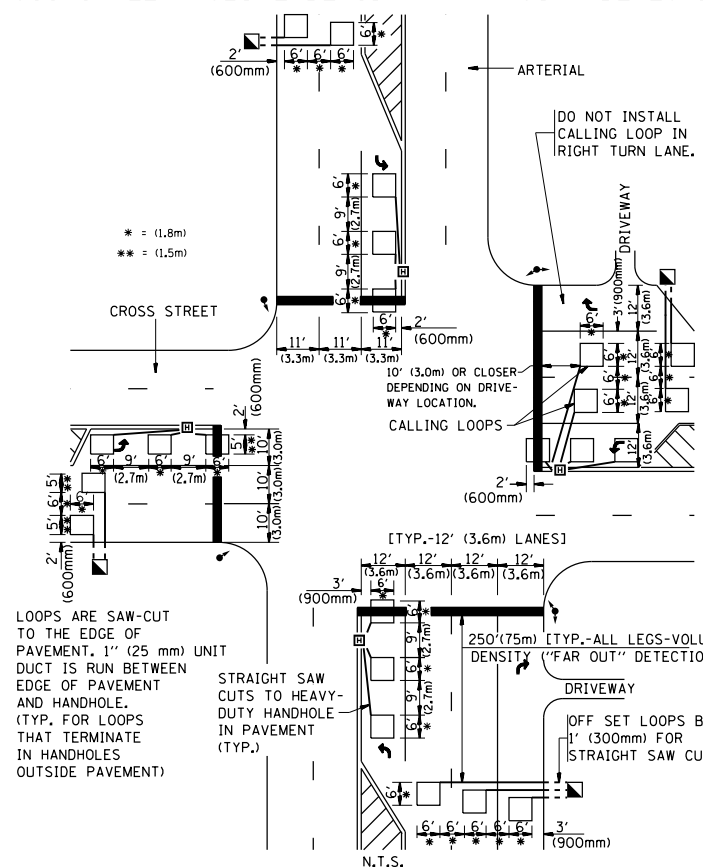
LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)



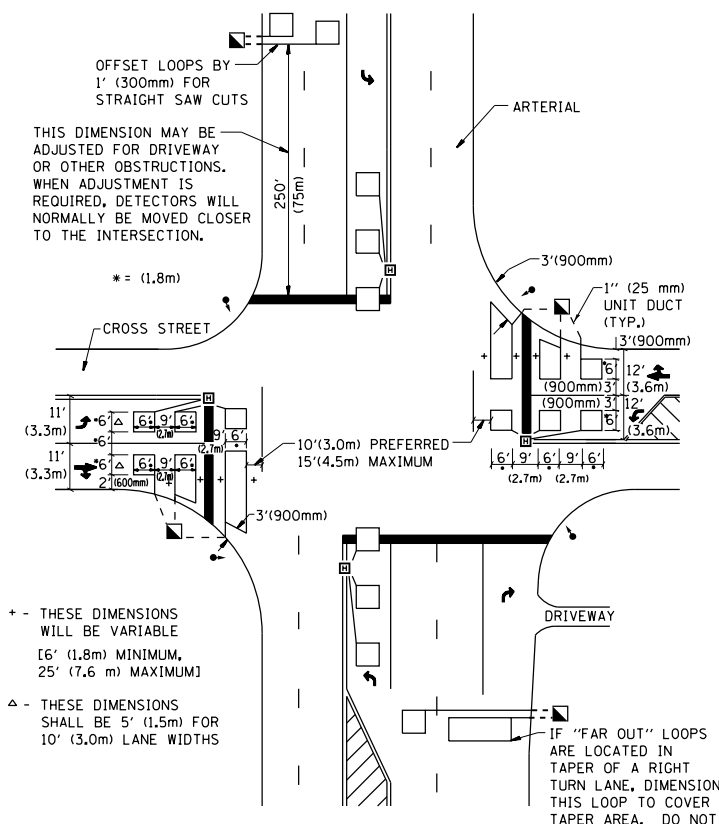
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = ldezmarm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING			F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBID\INTEG\11inois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\DI0311\Drawings\Design\DistStd.dgn		DRAWN -	REVISED -		3565	(46-2)RS	COOK	49	49			
PLOT SCALE = 100.0000' / 1"		CHECKED - R.K.F.	REVISED -		TS-07			CONTRACT NO.	60Y87			
PLOT DATE = 10/31/2016		DATE -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				