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GENERAL NOTES

SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED APRIL 1, 2016; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", THE LATEST REVISION; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", (IMUTCD); "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" JUNE 2014 SEVENTH EDITION, THE "DETAILS" IN THE PLANS AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS.

ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST IDOT HIGHWAY STANDARD. CODES OF THE IEPA TITLE 35, AND O.S.H.A. SHALL BE ADHERED TO FOR THE CONSTRUCTION OF THIS PROJECT.

ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH SECTION 700 OF THE STANDARD SPECIFICATIONS.

ALL REQUIRED PERMITS FROM THE PROPER GOVERNING AGENCY SHALL BE OBTAINED FOR CONSTRUCTION ALONG OR ACROSS EXISTING STREETS OR HIGHWAYS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR THE PROPER BRACING, SHEETING, SHORING AND OTHER REQUIRED PROTECTION OF ALL ROADWAYS BEFORE CONSTRUCTION BEGINS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO THE STREETS OR ROADWAYS AND ASSOCIATED STRUCTURES AND SHALL MAKE REPAIRS AS NECESSARY TO THE SATISFACTION OF THE AGENCY, AT THE CONTRACTOR'S OWN EXPENSE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND MAINTENANCE OF ADEQUATE SIGNS AND WARNING DEVICES TO INFORM AND PROTECT THE PUBLIC.

UTILITIES

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING UTILITY FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS.

THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM AND SANITARY SEWERS, WATER SERVICE LINES AND OTHER UTILITY LINES ARE APPROXIMATE, AND THE CITY DOES NOT GUARANTEE THEIR ACCURACY. THEIR EXACT HORIZONTAL AND VERTICAL LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER.

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 8-1-1 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS AND CABLE.

THE CONTRACTOR SHALL CONTACT IDOT'S BUREAU OF MATERIALS (PHONE 847-705-4337) AT LEAST 24 HOURS BEFORE PLACING HOT-MIX ASPHALT OR PORTLAND CEMENT CONCRETE.

STAKING

THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE ENGINEER, ITS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

WATER, STORM SEWER AND SANITARY SEWER

WHENEVER DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS.

THE CONTRACTOR SHALL NOT OPEN OR SHUT ANY WATER VALVES OR FIRE HYDRANTS. CONTACT THE CITY OF ZION WATER DEPARTMENT (TEL. 847-746-4060) FOR THEM TO TURN VALVES OR OPERATE HYDRANTS. UNAUTHORIZED USE SHALL SUBJECT THE OFFENDER TO ARREST AND PROSECUTION.

MISCELLANEOUS

ACCESS: THE CONTRACTOR SHALL PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT, EXCEPT FOR PERIODS OF SHORT DURATION. THE COST TO PROVIDE ACCESS SHALL BE PAID FOR AND INCLUDED IN THE ITEM TEMPORARY ACCESS (ROAD) OR TEMPORARY ACCESS (PRIVATE ENTRANCE).

DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.

ALL SAWCUTTING SHALL BE PERFORMED PRIOR TO BEGINNING REMOVAL. ANY ITEMS OF WORK REMOVED PRIOR TO SAWCUTTING WILL NOT BE MEASURED FOR PAYMENT.

COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, SIDEWALK REMOVAL AND REPLACEMENT, DRIVEWAY REMOVAL AND REPLACEMENT, AND STRUCTURES TO BE ADJUSTED WILL BE DETERMINED BY THE ENGINEER IN THE FIELD AND WILL NOT EXCEED THE PLANNED QUANTITY.

THE THICKNESSES OF HOT-MIX ASPHALT MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASIS ON WHICH THEY ARE TO BE PLACED. PLAN THICKNESSES SHOULD BE CONSIDERED THE MINIMUM THICKNESS PERMITTED.

DETECTABLE WARNINGS FOR THE HANDICAPPED SHALL BE INSTALLED AT INTERSECTING STREETS, DRIVEWAYS, AND ALLEYS AT THE LOCATIONS SHOWN ON THE PLANS. SEE DETAIL ON SHEET 22 FOR ADA RAMP AT BIKE PATH LIMITS.

PAVEMENT GRADES: THE ELEVATIONS INDICATED ON THE PLANS ARE FINISHED GRADES OF PROPOSED PAVEMENT OR SURFACE COURSE, UNLESS OTHERWISE INDICATED.

RELOCATING EXISTING SIGNS: EXISTING SIGNS WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED AND REINSTALLED UPON COMPLETION OF CONFLICTING IMPROVEMENTS IN ACCORDANCE WITH ARTICLE 107.25 OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION". STOP SIGNS, SPEED LIMIT SIGNS, AND STREET NAME SIGNS SHALL BE UP AND VISIBLE AT ALL TIMES.

FRESH OIL SIGNS SHALL BE POSTED AT BOTH ENDS OF THE ROADWAY AND ALL SIDE STREETS AS DIRECTED BY THE ENGINEER. CONSTRUCTION AHEAD SIGNS SHALL BE PLACED AT ALL SIDE STREETS AND BOTH ENDS OF THE ROADWAY WHILE CONSTRUCTION IS IN PROGRESS.

PROPOSED CONCRETE CURB AND GUTTER SHALL BE TRANSITIONED TO EXISTING CURB AND GUTTER OVER A LENGTH OF 5 FEET. THIS WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, SPECIAL.

CONTRACTOR SHALL NOT PLACE SOD UNTIL THE TEMPERATURE IS 80° OR LESS AND THE FORECAST FOR THE NEXT 7 DAYS SHOWS TEMPERATURES OF 80° OR LESS. IF ALL OTHER PAY ITEMS ARE COMPLETED, THE CONTRACTOR WILL NOT BE CHARGED WORKING DAYS FOR DELAYS IN PARKWAY RESTORATION DUE TO TEMPERATURE.

NO CONSTRUCTION SHALL BEGIN UNTIL ALL PROPER TEMPORARY SIGNS AND BARRICADES HAVE BEEN INSTALLED.

AT NO TIME SHALL LESS THAN HALF OF THE STREET BE AVAILABLE FOR PARKING.

ALL ROADS MUST HAVE ONLY ONE LONGITUDINAL JOINT WHILE PAVING.

VANDALISM - SPECIAL ATTENTION IS CALLED TO THE SPECIAL PROVISION FOR "INSPECTION" AS WELL AS ARTICLE 107.30 OF THE "STANDARD SPECIFICATIONS." ANY DEFACED WORK AS DETERMINED AND DIRECTED BY THE CITY SHALL BE CORRECTED OR REPLACED TO THE SATISFACTION OF THE ENGINEER BY THE CONTRACTOR AT HIS SOLE EXPENSE PRIOR TO FINAL PAYMENT. THE CITY OF ZION WILL COOPERATE WITH THE CONTRACTOR TO MINIMIZE VANDALISM, BUT THE CONTRACTOR SHALL BE ULTIMATELY RESPONSIBLE TO CORRECT ANY DAMAGE. THE CITY WILL NOT BE RESPONSIBLE FOR THE SECURITY OF THE WORK SITE IN THIS REGARD, OTHER THAN NORMAL PATROLLING AND RESPONSE TO EMERGENCIES. THE COST OF ADDITIONAL SECURITY REQUIRED TO MEET THIS SPECIAL PROVISION SHALL BE SOLELY THE CONTRACTOR'S RESPONSIBILITY.

HIGHWAY STANDARDS AND DISTRICT ONE DETAILS

| | |
|-----------|---|
| 000001-06 | STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS |
| 424001-08 | PERPENDICULAR CURB RAMPS FOR SIDEWALKS |
| 604001-04 | FRAME AND LIDS TYPE 1 |
| 606001-06 | CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER |
| 701006-05 | OFF ROAD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE |
| 701301-04 | LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS |
| 701311-03 | LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY |
| 701501-06 | URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED |
| 701701-10 | URBAN LANE CLOSURE, MULTILANE INTERSECTION |
| 701801-06 | SIDEWALK, CORNER, OR CROSSWALK CLOSURE |
| 701901-05 | TRAFFIC CONTROL DEVICES |
| 720001-01 | SIGN PANEL MOUNTING DETAILS |
| 720006-04 | SIGN PANEL ERECTION DETAILS |
| 720011-01 | METAL POSTS FOR SIGNS, MARKERS, & DELINEATORS |
| 729001-01 | APPLICATIONS OF TYPE A&B METAL POSTS (FOR SIGNS & MARKERS) |
| 780001-05 | TYPICAL PAVEMENT MARKINGS |
| BD-08 | FRAMES AND LIDS ADJUSTMENT WITH MILLING |
| BD-22 | PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT |
| BD-24 | CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT |
| BD-32 | BUTT JOINT AND HMA TAPER DETAILS |
| TC-10 | TRAFFIC CONTROL FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS |

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| PLOT SCALE = NOT TO SCALE | CHECKED - LMF | REVISED - | 1002 | | | 16-00087-00-RS | LAKE | 21 | 2 | |
| PLOT DATE = 12/2/2016 | DATE - 12/02/2016 | REVISED - | CONTRACT NO. | | | 61D41 | | | | |
| | | | | | | ILLINOIS FED. AID PROJECT | | | | |
| SCALE: N.T.S. | | | | | | SHEET NO. OF 21 SHEETS | | STA. TO STA. | | |

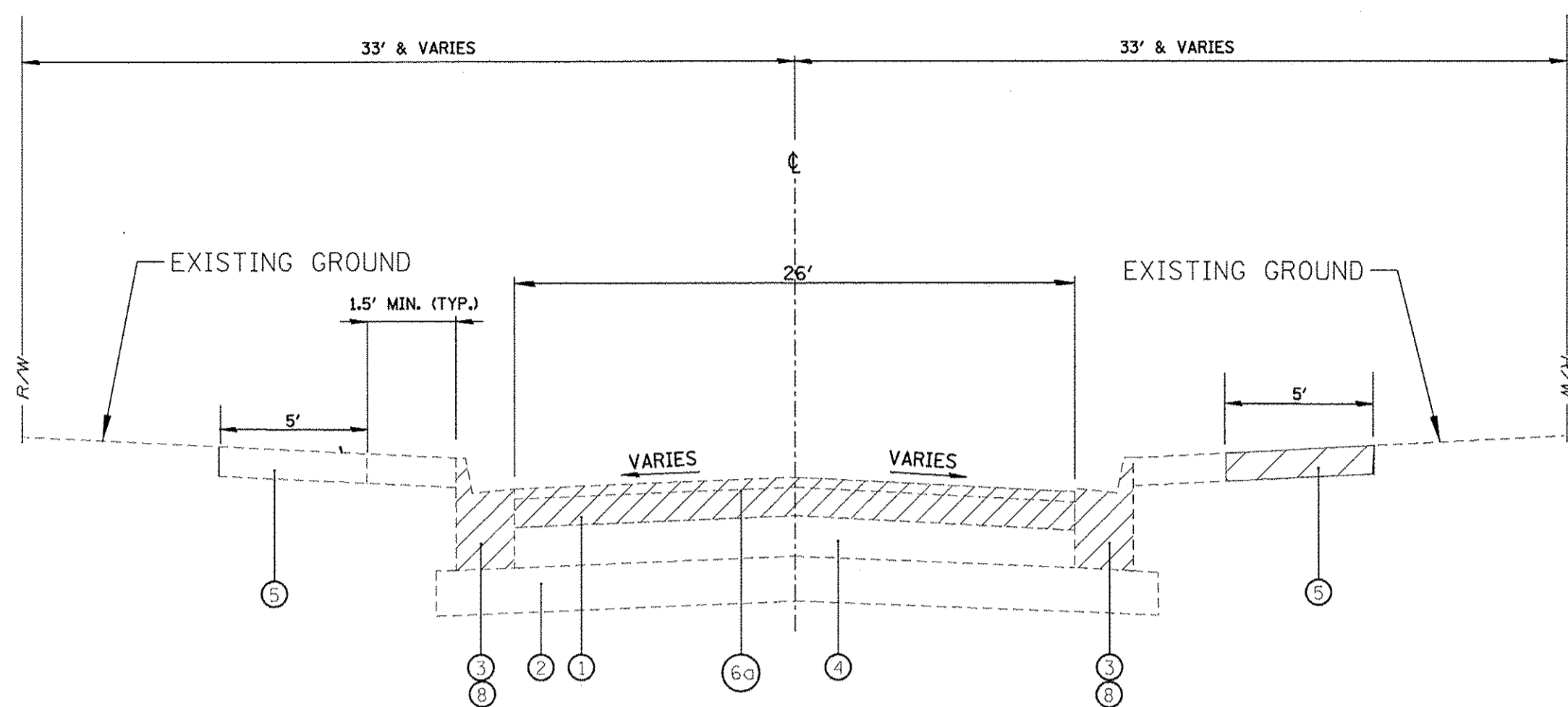
SUMMARY OF QUANTITIES

| CODE NO. | ITEM | UNIT | CONSTRUCTION CODE 0005 TOTAL QUANTITY |
|------------|--|-------|--|
| 20201200 | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL | CU YD | 625 |
| 20800150 | TRENCH BACKFILL | CU YD | 5 |
| 21001000 | GEOTECHNICAL FABRIC FOR GROUND STABILIZATION | SQ YD | 1,800 |
| 21101625 | TOPSOIL FURNISH AND PLACE, 6" | SQ YD | 30 |
| 25200110 | SODDING, SALT TOLERANT | SQ YD | 30 |
| 28000510 | INLET FILTERS | EACH | 26 |
| 30300001 | AGGREGATE SUBGRADE IMPROVEMENT | CU YD | 625 |
| 35800100 | PREPARATION OF BASE | SQ YD | 8300 |
| 35800200 | AGGREGATE BASE REPAIR | TON | 440 |
| 40600290 | BITUMINOUS MATERIALS (TACK COAT) | POUND | 14,000 |
| 40600400 | MIXTURE FOR CRACKS, JOINTS AND FLANGEWAYS | TON | 25 |
| 40600625 | LEVELING BINDER (MACHINE METHOD), N50 | TON | 575 |
| 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT | SQ YD | 295 |
| 40603080 | HOT MIX ASPHALT BINDER COURSE, IL - 19.0, N50 | TON | 1,275 |
| 40603335 | HOT MIX ASPHALT SURFACE COURSE, "MIX D", N50 | TON | 2,100 |
| 42400800 | DETECTABLE WARNINGS | SQ FT | 300 |
| 44000166 | HOT MIX ASPHALT SURFACE REMOVAL, 4 1/4 " | SQ YD | 8263 |
| 44000160 | HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4" | SQ YD | 9400 |
| 44000600 | SIDEWALK REMOVAL | SQ FT | 1930 |
| 44201737 | CLASS "D" PATCHES, TYPE I, 8" | SQ YD | 165 |
| 44201741 | CLASS "D" PATCHES, TYPE II, 8" | SQ YD | 1,140 |
| 44201745 | CLASS "D" PATCHES, TYPE III, 8" | SQ YD | 165 |
| 44201747 | CLASS "D" PATCHES, TYPE IV, 8" | SQ YD | 165 |
| 60236200 | INLETS, TYPE A, TYPE 8 GRATE | EACH | 2 |
| 67100100 | MOBILIZATION | L SUM | 1 |
| 70102635 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701701 | L SUM | 1 |
| 70102620 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701501 | L SUM | 1 |
| 70102640 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701801 | L SUM | 1 |
| 70300100 | SHORT TERM PAVEMENT MARKING | FOOT | 2,700 |
| 70300150 | SHORT TERM PAVEMENT MARKING REMOVAL | SQ FT | 2,700 |
| 70300210 | TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS | SQ FT | 150 |
| 72000100 | SIGN PANEL - TYPE 1 | SQ FT | 12 |
| 72900100 | METAL POST - TYPE A | FOOT | 16 |
| * 78000400 | THERMOPLASTIC PAVEMENT MARKING - LINE 6" | FOOT | 1,550 |
| * 78000600 | THERMOPLASTIC PAVEMENT MARKING - LINE 12" | FOOT | 950 |
| * 78000650 | THERMOPLASTIC PAVEMENT MARKING - LINE 24" | FOOT | 300 |
| X0326862 | STRUCTURES TO BE ADJUSTED | EACH | 48 |
| X0327890 | DRIVEWAY REMOVAL AND REPLACEMENT | SQ YD | 50 |
| X4021000 | TEMPORARY ACCESS (PRIVATE ENTRANCE) | EACH | 63 |
| X4022000 | TEMPORARY ACCESS COMMERCIAL ENTRANCE) | EACH | 23 |
| X4240430 | PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL | SQ FT | 1930 |
| X6030310 | FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) | EACH | 59 |
| Z0013798 | CONSTRUCTION LAYOUT | L SUM | 1 |
| XX008348 | MORTAR EXISTING STRUCTURE | EACH | 2 |
| Z0004542 | COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT | FOOT | 920 |

* INDICATES SPECIALITY ITEM
~ INDICATES SPECIAL PROVISION

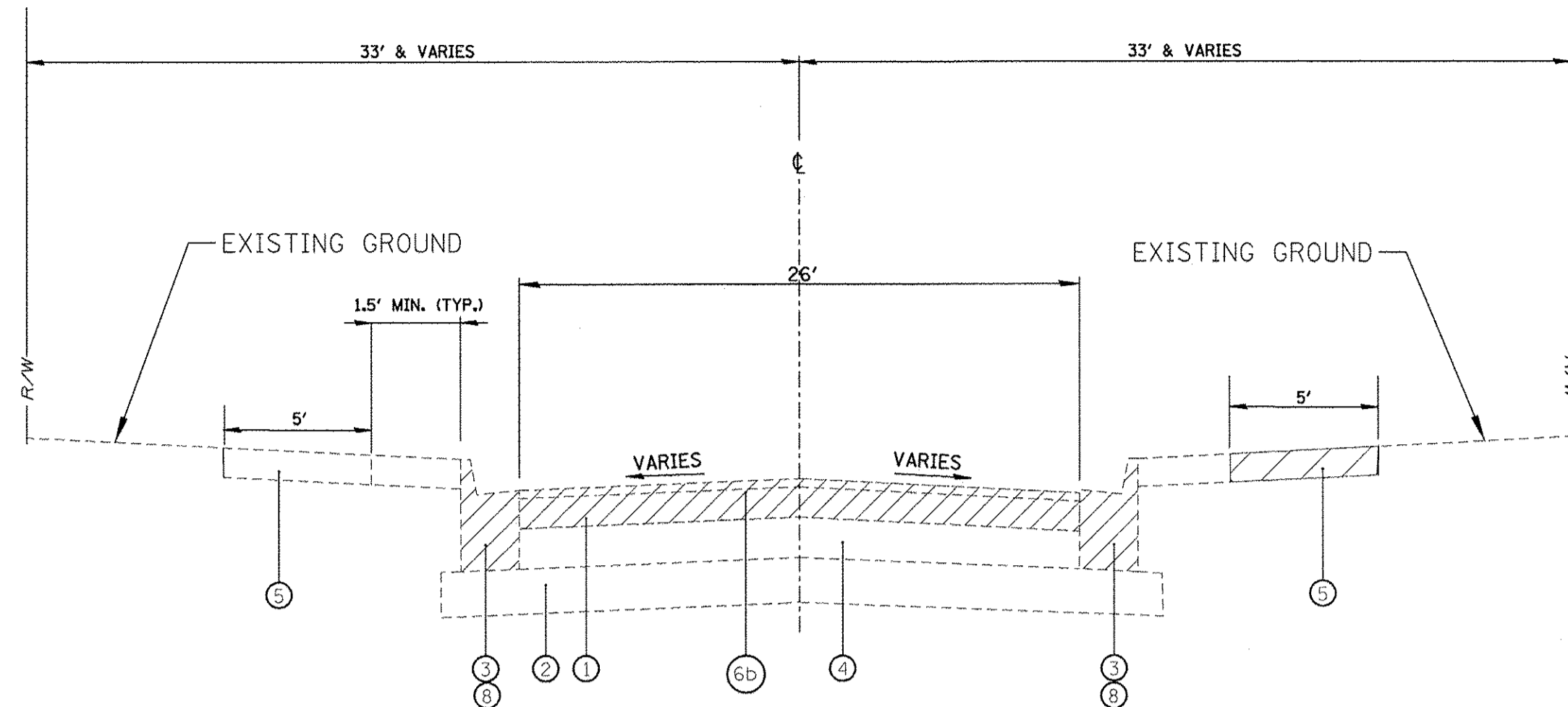
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| PLLOT SCALE = NOT TO SCALE | CHECKED - LMF | REVISIED - | REVISIED - | SCALE: N.T.S. | SHEET NO. 3 OF 21 SHEETS | STA. | TO STA. | CONTRACT NO. | 61041 | ILLINOIS FED. AID PROJECT |
| PLLOT DATE = 10/31/2016 | DATE - 10/31/2016 | REVISIED - | REVISIED - | | | | | | | |

RT. 173 TO HIGHLAND EXISTING TYPICAL SECTION

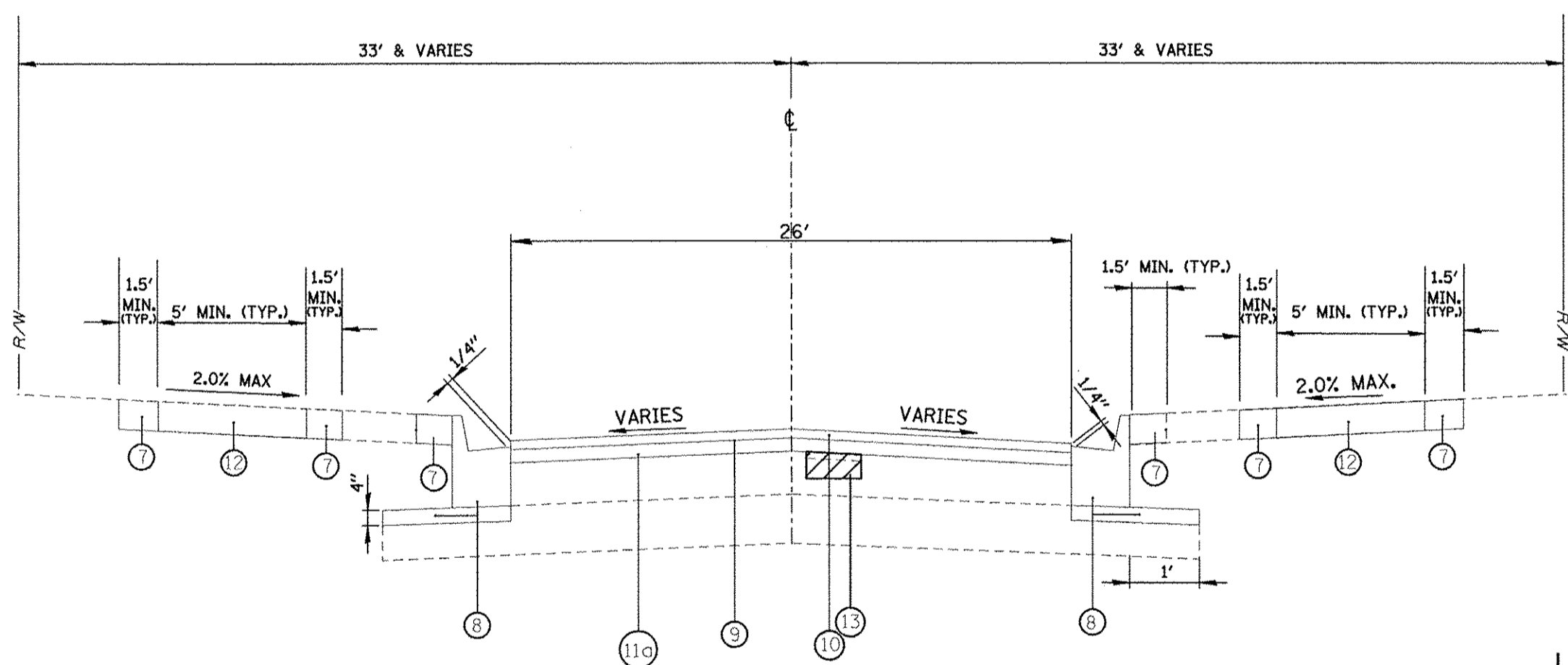


EXISTING TYPICAL SECTION
STATION 10+46 TO 36+50, LORELEI DRIVE

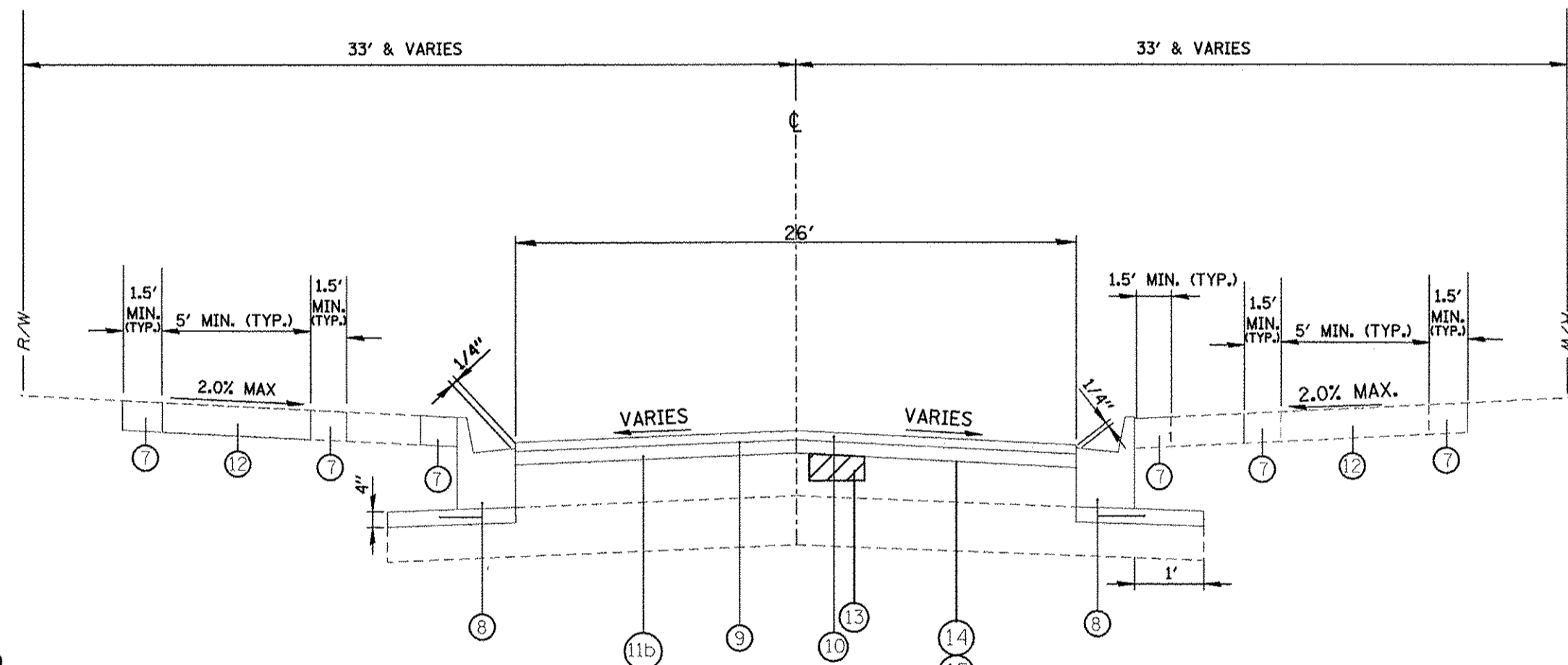
HIGHLAND TO 9TH EXISTING TYPICAL SECTION



EXISTING TYPICAL SECTION
STATION 36+50 TO 64+80.42, LORELEI DRIVE



PROPOSED TYPICAL SECTION
STATION 10+46 TO 36+50, LORELEI DRIVE



PROPOSED TYPICAL SECTION
STATION 36+50 TO 64+80.42, LORELEI DRIVE

LEGEND

- ① EXISTING HOT-MIX ASPHALT PAVEMENT
- ② EXISTING AGGREGATE SUBBASE
- ③ EXISTING CURB AND GUTTER
- ④ EXISTING AGGREGATE BASE
- ⑤ EXISTING PCC SIDEWALK
- ⑥a HOT-MIX ASPHALT SURFACE REMOVAL - 2.75"
- ⑥b HOT MIX ASPHALT SURFACE REMOVAL - 4.25"
- ⑦ SODDING, SALT TOLERANT (INCIDENTAL TO COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT AND SIDEWALK REMOVAL AND REPLACEMENT)
- ⑧ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER). INCLUDES 4" SUBBASE GRANULAR MATERIAL TYPE B. PROPOSED BITUMINOUS MATERIAL (TACK COAT)
- ⑩ HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50 - 2"
- ⑩a POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL 4.75, N50 - 1"
- ⑩b HMA BINDER COURSE, IL - 19.0, N50 - 2.5"
- ⑫ PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL AS DIRECTED BY THE ENGINEER (SIDEWALKS THROUGH DRIVEWAYS SHALL BE 6 INCHES THICK - THIS WORK WILL BE INCLUDED IN THE PAY ITEM FOR PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH, SPECIAL)
- ⑬ CLASS "D" PATCHES, 8"
- ⑭ PREPARATION OF BASE
- ⑮ AGGREGATE BASE REPAIR

NOTE:
PAVING OF THE FULL ROADWAY WIDTH SHALL BE COMPLETED AT THE END OF EACH DAY OF PAVING TO PREVENT A LONGITUDINAL COLD JOINT FROM APPEARING WHEN OPPOSITE SIDES OF THE ROAD ARE PAVED ON DIFFERENT DAYS. THE CONTRACTOR SHALL ALSO ENSURE THAT AT THE END OF EACH DAY EACH PASS ENDS AT APPROXIMATELY THE SAME STATION TO PREVENT A COLD JOINT.

- NOTES:
- THE HIGH SIDE OF THE ROADWAY SHALL BE PAVED FIRST.
 - AGGREGATE BASE REPAIR - ANY NEW MATERIAL NECESSARY TO BRING THE EXISTING SUBBASE TO THE GRADE MIN. DEPTH 9" CROSS SLOPE OR WIDTH SHOWN SHALL BE PAID FOR UNDER THIS ITEM.
 - ANY AGGREGATE BASE REMOVAL DUE TO PROPOSED ASPHALT SHALL BE CONSIDERED INCLUDED IN THE COST OF HOT-MIX ASPHALT SURFACE REMOVAL, 2.75" AND 4.25".
 - ANY EXCAVATION OF DIRT/CLAY NECESSARY TO OBTAIN THE NECESSARY DEPTH FOR THE PROPOSED PAVEMENT SHALL BE PAID FOR AS REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL.

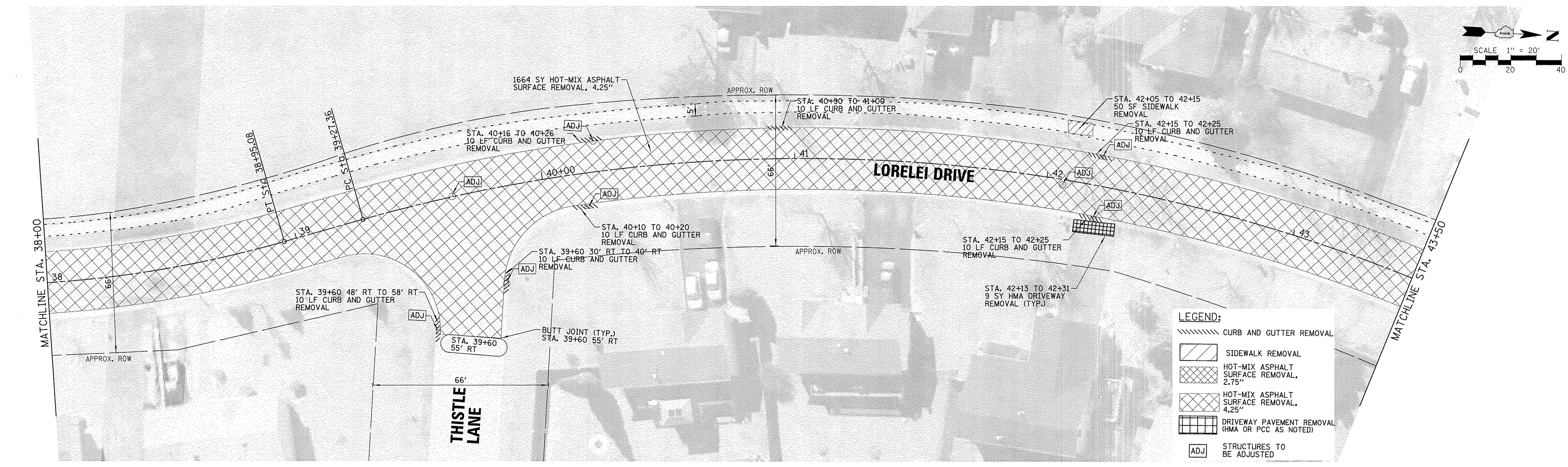
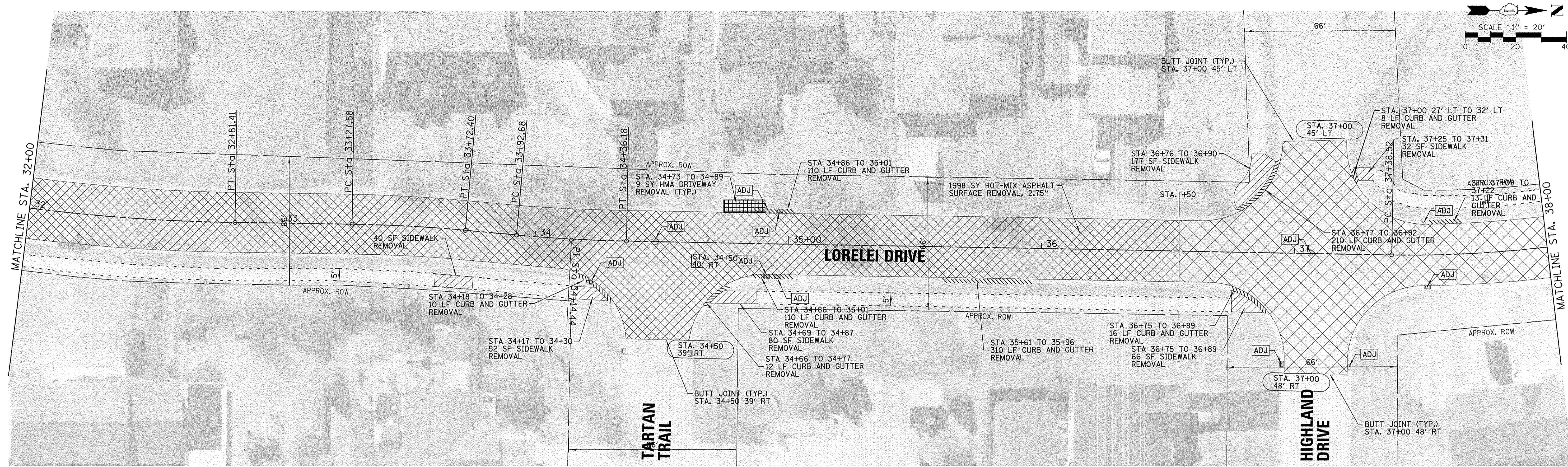
| HOT-MIX ASPHALT MIXTURE REQUIREMENTS ITEM | VOIDS |
|--|----------------|
| HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm), 2" | 4% @ 50 GYR. |
| POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL 4.75, N50, 1" | 3.5% @ 50 GYR. |
| HMA BINDER COURSE, IL-19.0, N50, 2.5" | 4% @ 50 GYR. |
| CLASS "D" PATCHES, 8" | 4% @ 70 GYR. |

- MIXTURE REQUIREMENT NOTES:
- THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE IS 112 LBS/SY/IN. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
 - FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

CORE DETAILS

| CORE NUMBER (AS SHOWN IN REPORT) | ASPHALT TOTAL THICKNESS | SUBBASE THICKNESS | CORE STATION |
|----------------------------------|-------------------------|-------------------|--------------|
| 1 | 5.3" | 12.0" | N/A |
| 2 | 8.8" | NONE | N/A |
| 3 | 8.1" | 1.0" | N/A |
| 4 | 8.5" | NONE | N/A |
| 5 | 8.8" | 2.0" | N/A |
| 6 | 9.6" | NONE | 64+99.73 |
| 7 | 8.0" | NONE | 56+87.28 |
| 8 | 3.5" | 16.5" | 49+60.16 |
| 9 | 4.0" | 17.0" | 44+25.69 |
| 10 | 4.0" | 13.0" | 37+46.55 |

AVG. 7.0" 6.0"



LEGEND:

| | |
|--|---|
| | CURB AND GUTTER REMOVAL |
| | SIDEWALK REMOVAL |
| | HOT-MIX ASPHALT SURFACE REMOVAL, 2.75" |
| | HOT-MIX ASPHALT SURFACE REMOVAL, 4.25" |
| | DRIVEWAY PAVEMENT REMOVAL (HMA OR PCC AS NOTED) |
| | STRUCTURES TO BE ADJUSTED |

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 PLOT DATE = 11/17/2016

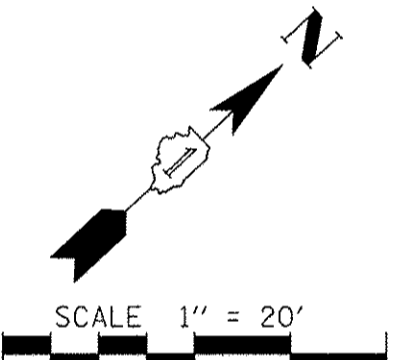
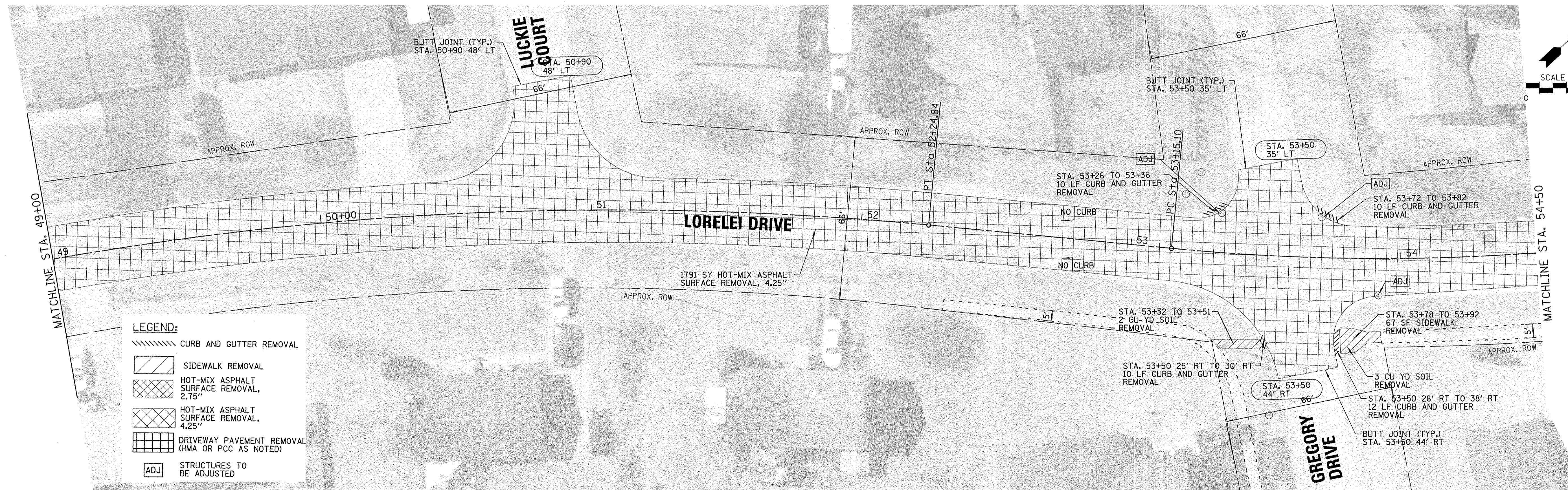
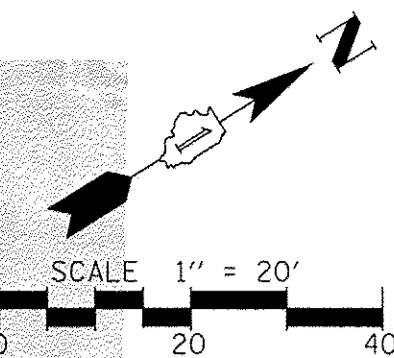
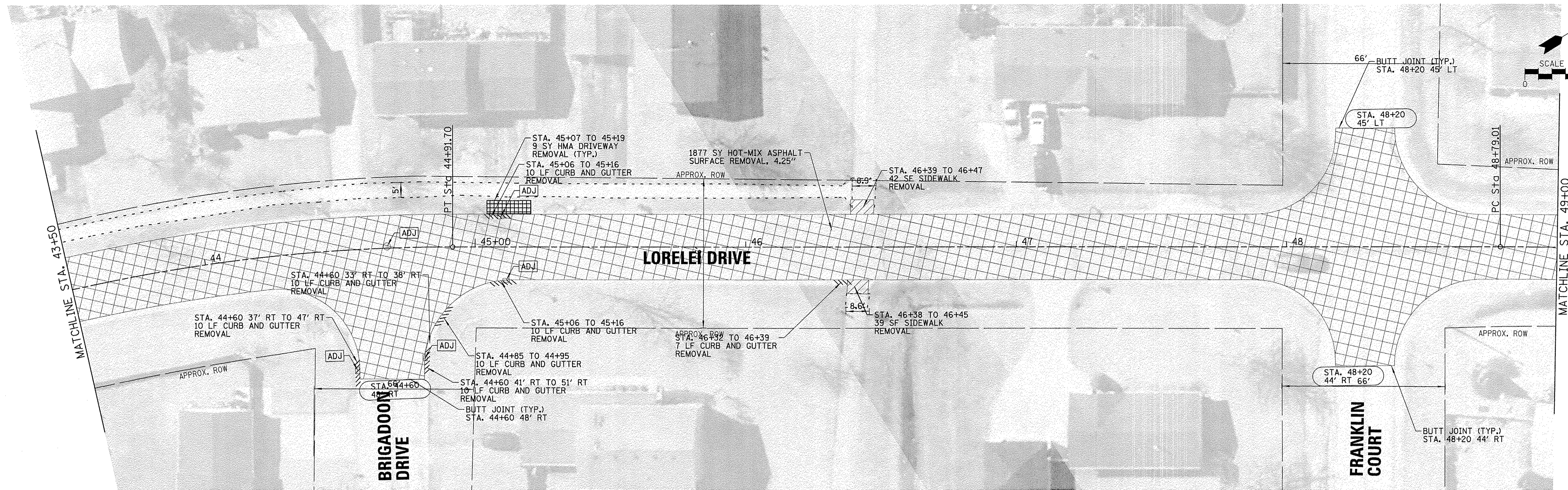
DESIGNED - JEH
 DRAWN - EDT
 CHECKED - LMF
 DATE - 10/31/2016

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**LORELEI DRIVE
 EXISTING CONDITIONS AND REMOVAL PLAN**
 SCALE: 20' SHEET NO. 7 OF 21 SHEETS STA. TO STA.

| | | | | |
|---------------------------|----------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1002 | 16-00087-00-RS | LAKE | 21 | 7 |
| CONTRACT NO. 61D41 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



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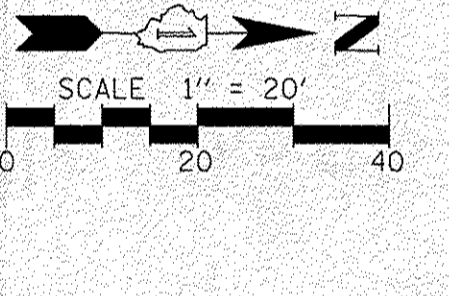
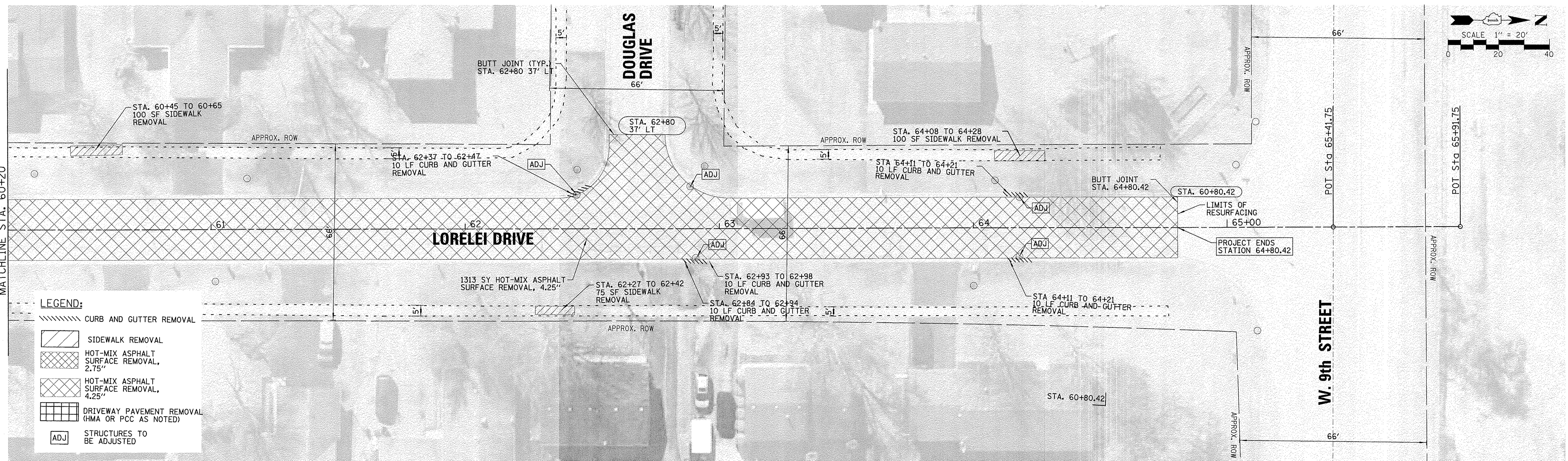
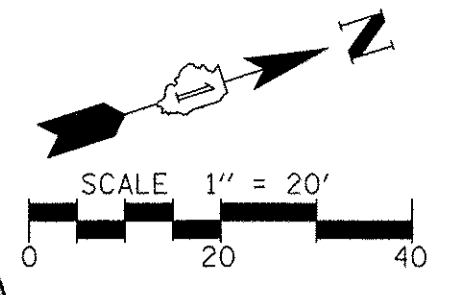
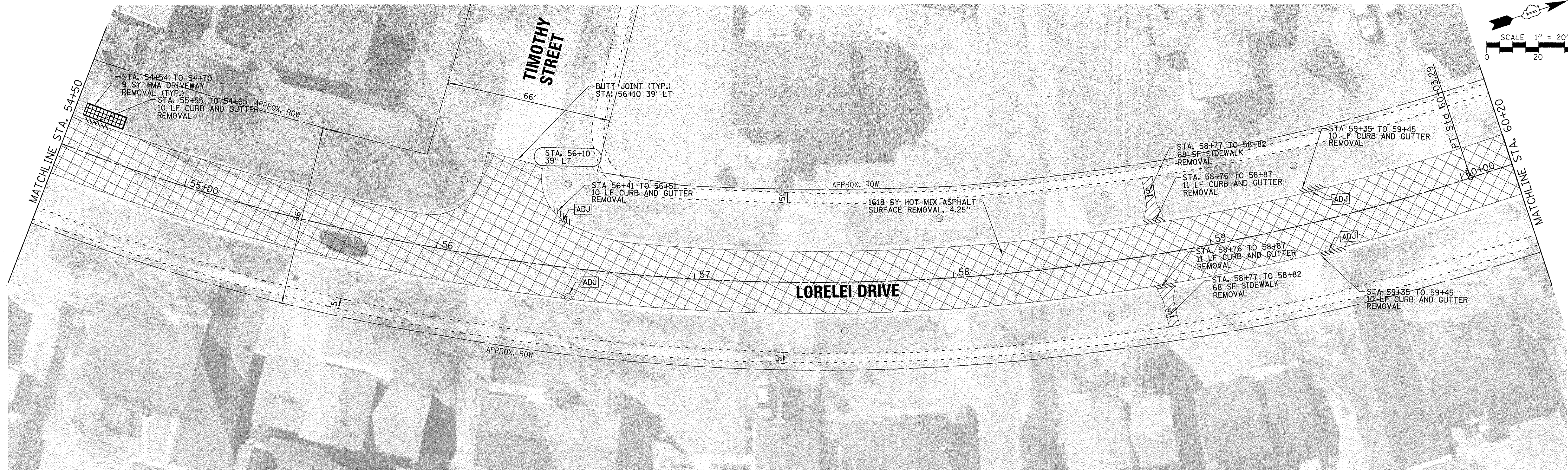
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|--|---|
| | CURB AND GUTTER REMOVAL |
| | SIDEWALK REMOVAL |
| | HOT-MIX ASPHALT SURFACE REMOVAL, 2.75" |
| | HOT-MIX ASPHALT SURFACE REMOVAL, 4.25" |
| | DRIVEWAY PAVEMENT REMOVAL (HMA OR PCC AS NOTED) |
| | STRUCTURES TO BE ADJUSTED |

| | | | |
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| FILE NAME = N:\Z10N\160172\Civil\rem.160172-04.sht | USER NAME = jhouseh | DESIGNED - JEH | REVISED - |
| PLOT SCALE = 20' | CHECKED - LMF | DRAWN - EDT | REVISED - |
| PLOT DATE = 11/1/2016 | DATE - 10/31/2016 | REVISED - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| | |
|---|--------------------------|
| LORELEI DRIVE EXISTING CONDITIONS AND REMOVAL PLAN | |
| SCALE: 20' | SHEET NO. 8 OF 21 SHEETS |
| STA. TO STA. | |

| | | | | |
|---------------------------|------------------------|-------------|--------------------|-------------|
| F.A.U. RTE. 1002 | SECTION 16-00087-00-RS | COUNTY LAKE | TOTAL SHEETS 21 | SHEET NO. 8 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 61D41 | |



LEGEND:

| | |
|--|---|
| | CURB AND GUTTER REMOVAL |
| | SIDEWALK REMOVAL |
| | HOT-MIX ASPHALT SURFACE REMOVAL, 2.75" |
| | HOT-MIX ASPHALT SURFACE REMOVAL, 4.25" |
| | DRIVEWAY PAVEMENT REMOVAL (HMA OR PCC AS NOTED) |
| | STRUCTURES TO BE ADJUSTED |

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 USER NAME = jhouseh
 PLOT SCALE = 20'
 PLOT DATE = 11/1/2016

DESIGNED - JEH
 DRAWN - EDT
 CHECKED - LMF
 DATE - 10/31/2016

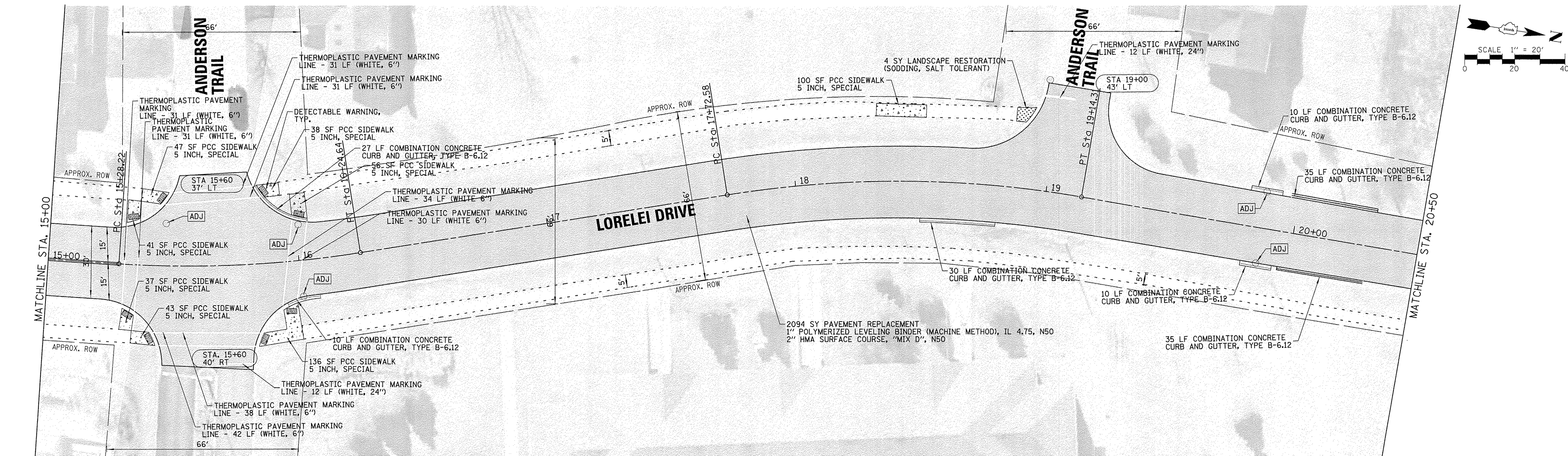
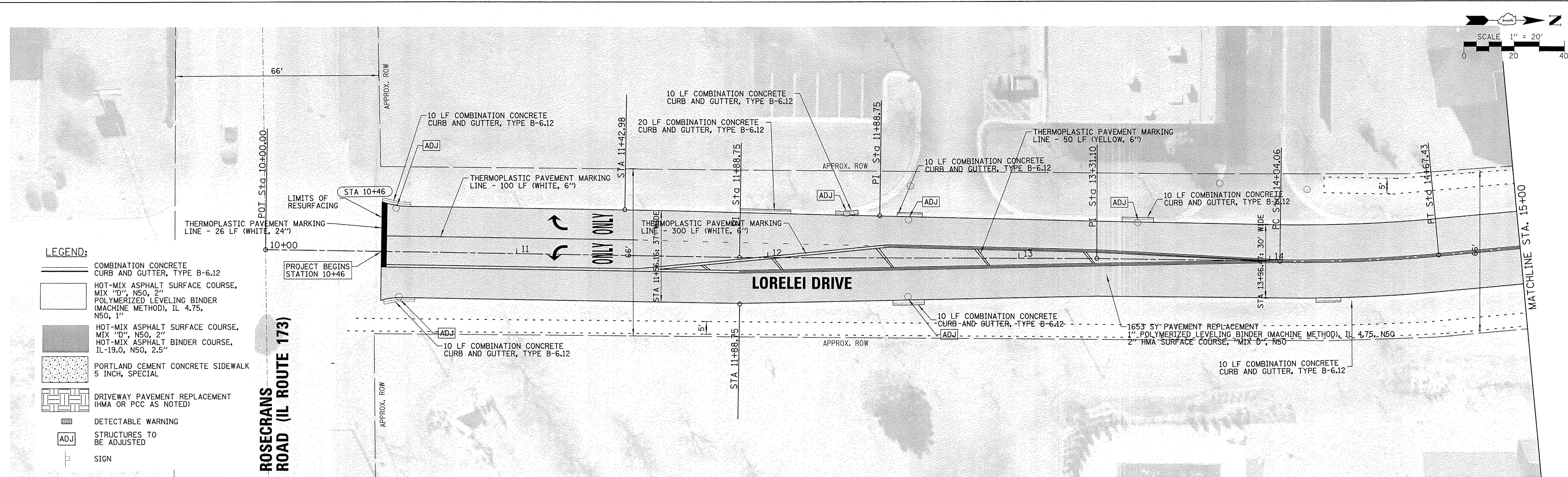
REVISED -
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**LORELEI DRIVE
 EXISTING CONDITIONS AND REMOVAL PLAN**

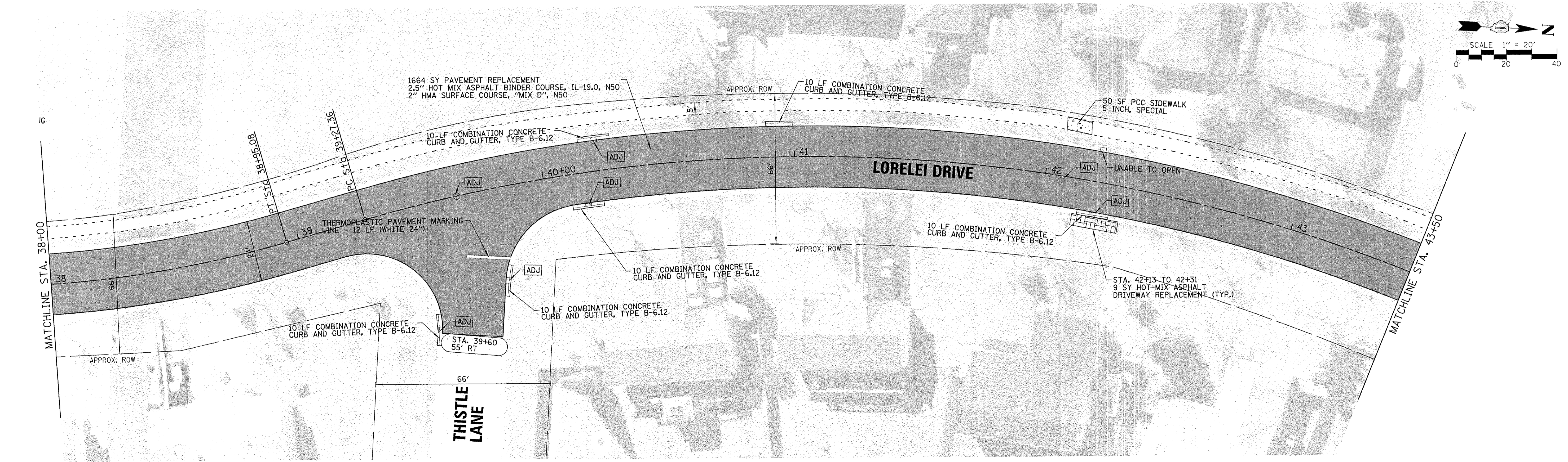
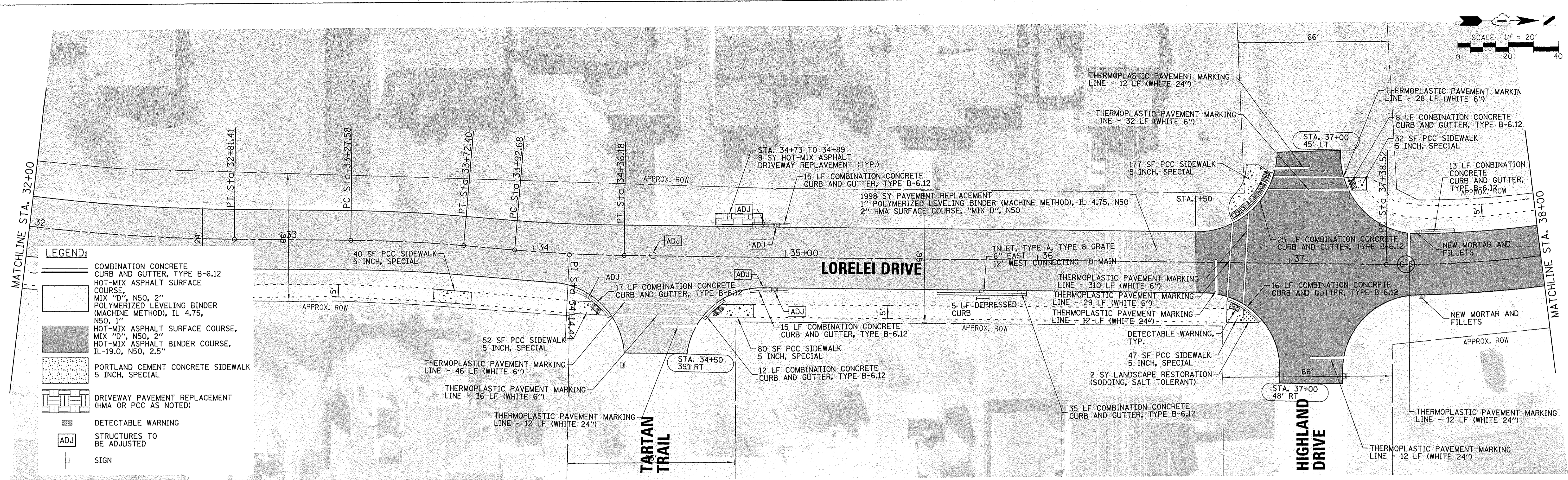
SCALE: 20' SHEET NO. 9 OF 21 SHEETS STA. TO STA.

| | | | | |
|---------------------------|----------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1002 | 16-00087-00-RS | LAKE | 21 | 9 |
| CONTRACT NO. | | | 61D41 | |
| ILLINOIS FED. AID PROJECT | | | | |

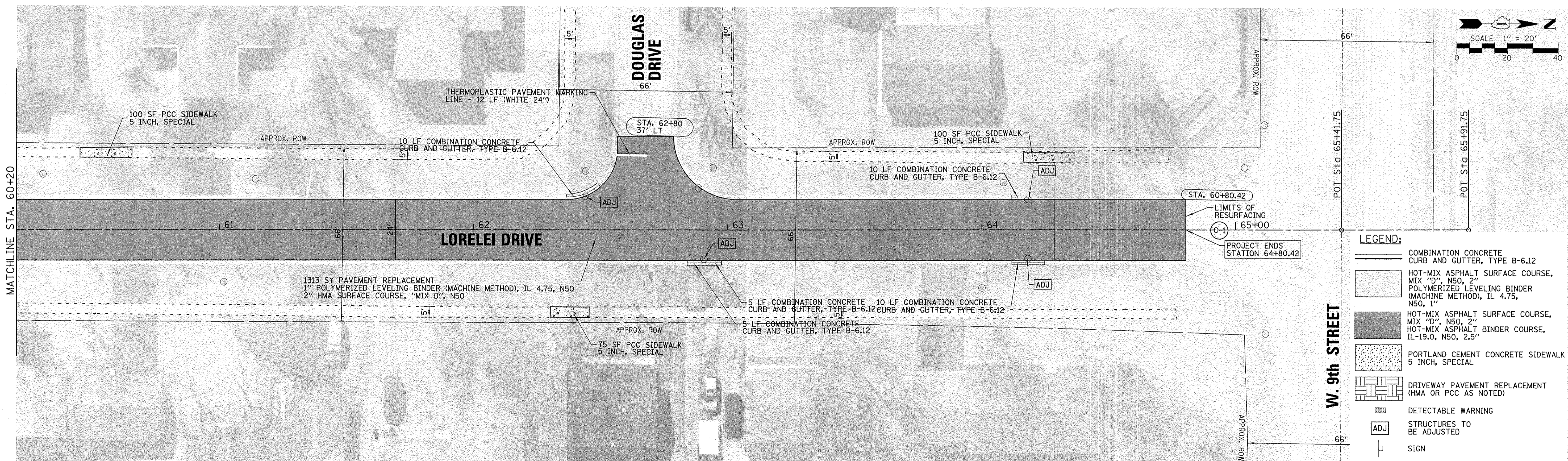
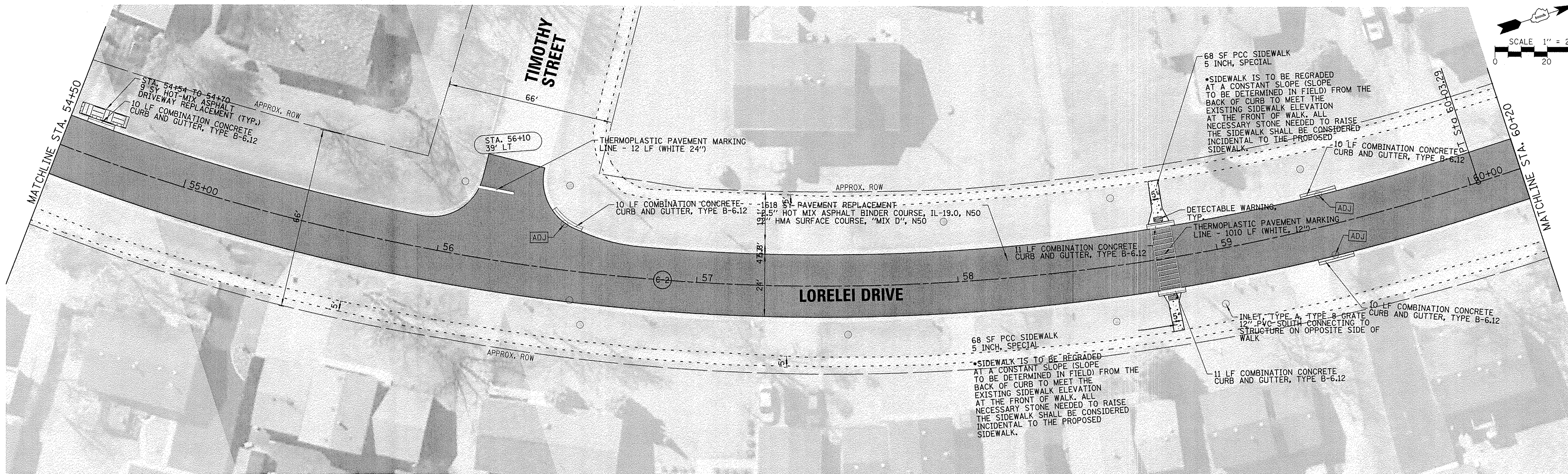


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| PLOT DATE = 11/1/2016 | DATE = 10/31/2016 | CHECKED - LMF | REVISED - | | | CONTRACT NO. 61D41 | | | | | |
| | | | REVISED - | | | ILLINOIS FED. AID PROJECT | | | | | |

SCALE: 28' SHEET NO. 10 OF 21 SHEETS STA. TO STA.



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| PLOT DATE = 11/17/2016 | DATE - 10/31/2016 | REVISED - | | | | | | | | | |
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| FILE NAME = | USER NAME = jhoush | DESIGNED - JEH | REVISED - |
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| | PLOT SCALE = 20' | CHECKED - LMF | REVISED - |
| | PLOT DATE = 11/1/2016 | DATE - 10/31/2016 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**LORELEI DRIVE
PROPOSED PLAN**

SCALE: 20' SHEET NO. 14 OF 21 SHEETS STA. TO STA.

| | | | | |
|---------------------------|----------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1002 | 16-00087-00-RS | LAKE | 21 | 14 |
| CONTRACT NO. | | | 61D41 | |
| ILLINOIS FED. AID PROJECT | | | | |

CONSTRUCTION PROCEDURES

STAGE I (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12" OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36" DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1" THICK HOT-MIX MATERIAL APPROVED BY THE ENGINEER.

STAGE II (AFTER PAVEMENT MILLING)

- A) REMOVE THE HOT-MIX MATERIAL AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602 AND 603 OF THE STANDARD SPECIFICATIONS.

LOCATION OF STRUCTURES

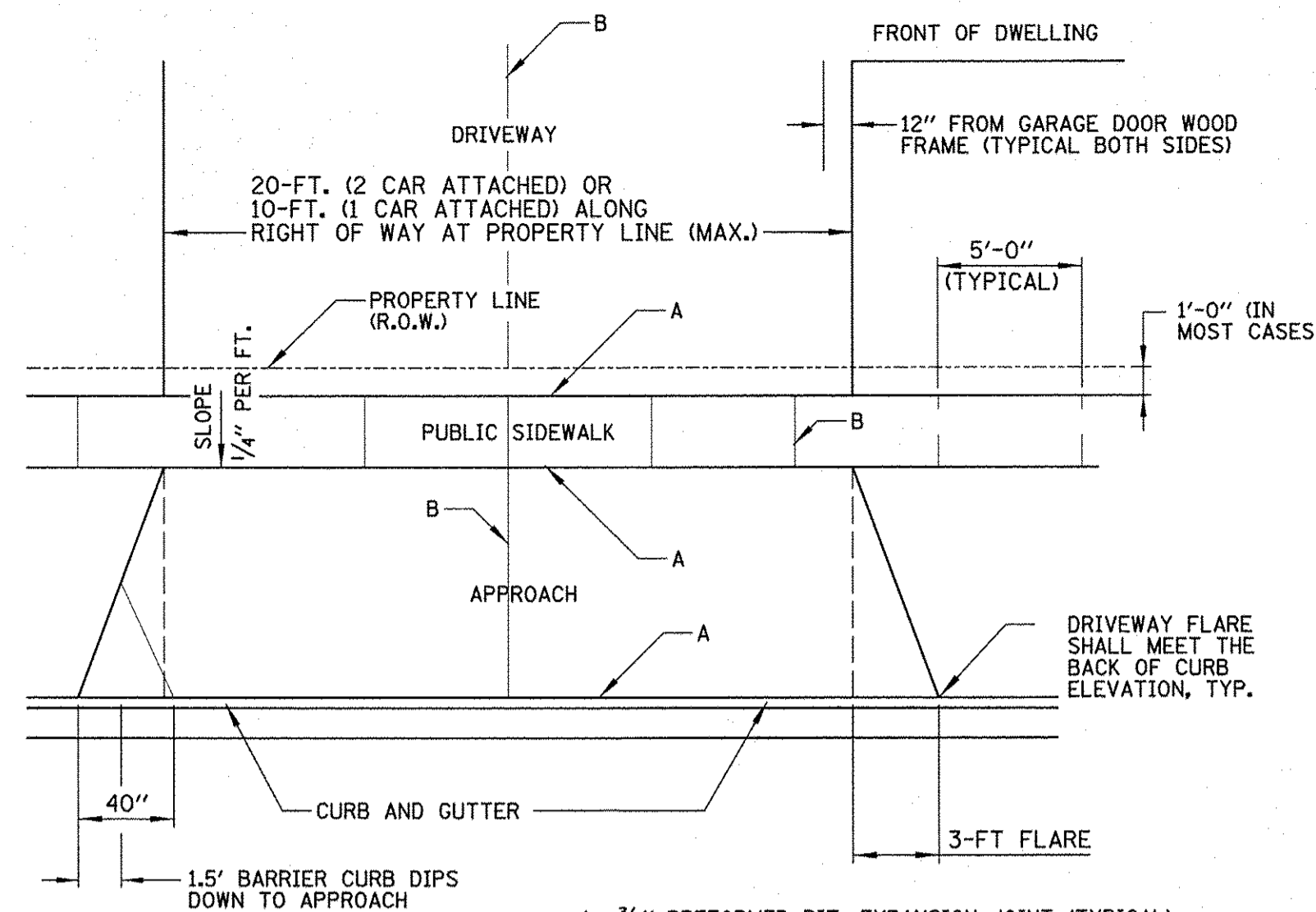
THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

STRUCTURES TO BE ADJUSTED, SPECIAL

NOTES

- 1. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS AS FRAMES AND LIDS, TYPE 1, OF THE APPROPRIATE LID ACCORDING TO THE ENGINEER.
- 2. IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
- 3. THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
- 4. EXTERNAL MANHOLE CHIMNEY SEAL SHALL BE PROVIDED AND SHALL CONSIST OF A RUBBER SLEEVE, COMPRESSION BAND AND EXTENSION SKIRT. RUBBER SLEEVE SHALL BE HIGH GRADE RUBBER COMPOUND CONFORMING TO ASTM C293 WITH A HARDNESS OF 45 PLUS OR MINUS 5. COMPRESSION BANDS SHALL BE 16 GAUGE TYPE 304 STAINLESS STEEL WITH A MINIMUM WIDTH OF 1 INCH. EXTENSION WEIGHT OF 12 OUNCES PER SQUARE YARD. EXTERNAL MANHOLE CHIMNEY SEAL SHALL BE MANUFACTURED BY CANUSA, INFISHIELD OR APPROVED EQUAL.



DRIVEWAY WITH A CURB AND GUTTER

GENERAL NOTES:

- 1. DRIVEWAY SHALL HAVE A MIN. SLOPE OF 2% AND MAX. SLOPE OF 6%.
- 2. APPROACH SHALL HAVE A MIN. SLOPE OF 2% AND MAX. OF 6%.
- 3. ALL AGGREGATE SUB-BASE SHALL BE MECHANICALLY COMPACTED. (95% PROCTOR)
- 4. PUBLIC SIDEWALK SHALL BE 6" AT RESIDENTIAL DRIVEWAYS AND 8" AT COMMERCIAL/INDUSTRIAL DRIVEWAYS. (NO WIRE MESH)
- 5. MINIMUM THICKNESS FOR APPROACH. (NO WIRE MESH). THIS WILL BE PAID FOR BY THE FOLLOWING ITEMS:
 - A. 7" THK. P.C. CONCRETE ON 2" AGGREGATE BASE COURSE TYPE B OR
 - B. 3" THK. HOT-MIX ASPHALT SURFACE, MIX "D" N50 ON 6" AGGREGATE BASE COURSE TYPE B
- 6. SEEDING AND TOPSOIL, 4" (100) RESTORATION WILL BE PAID FOR SEPARATELY AS THEIR RESPECTIVE PAY ITEMS.

FRAME AND LID:
ALL LIDS SHALL HAVE A 1" CONCEALED PICK HOLE AND THE WORD "STORM" AND CITY OF ZION LOGO CAST IN LID.

ALL FLAT TOP STRUCTURES SHALL HAVE A 4" CAST RING (TO BE INCLUDED IN THE COST OF "STRUCTURES TO BE ADJUSTED IF THE STRUCTURE IS EXISTING, OR IN THE COST OF THE PROPOSED STRUCTURE IF THE STRUCTURE IS NEW).

CHIMNEY SEAL TO BE CANUSA RAPID SEAL TYPE ANS OR APPROVED EQUAL TO CAPTURE 4" OF FRAME, ALL RINGS AND 4" OF BARREL SECTION. CHIMNEY SEAL TO BE USED ON ALL SANITARY MANHOLES, AND ANY CATCH BASINS OR INLETS IN THE ROADWAY.

WALLS:
PRECAST REINFORCED CONCRETE RISER UNITS
MINIMUM THICKNESSES:
5" FOR 4'-0" INSIDE DIAMETER
6" FOR 5'-0" INSIDE DIAMETER

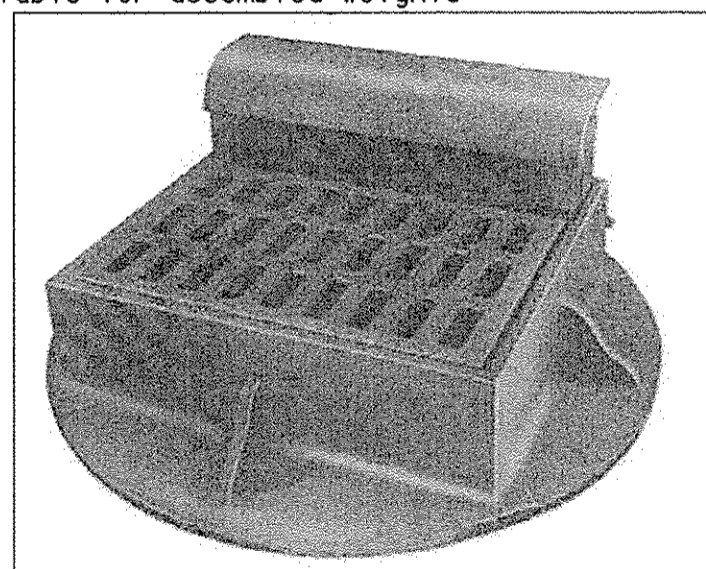
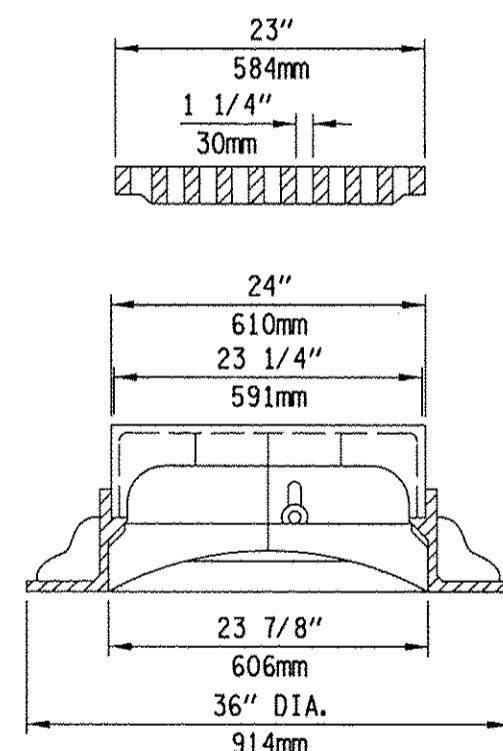
BOTTOM:
PRECAST REINFORCED AND CAST INTEGRAL WITH FIRST VERTICAL SECTION.

INSIDE DIAMETERS:
4'-0" FOR 18" MAIN SEWER AND UNDER
FOR MANHOLES 20' DEEP OR LESS
5'-0" FOR 21" TO 42" MAIN SEWER
FOR MANHOLES 20' DEEP OR DEEPER
BOTH INCLUSIVE

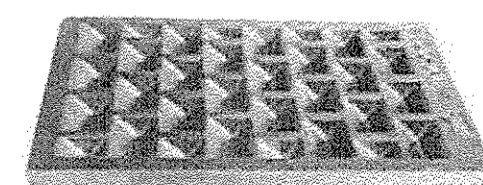
ADJUSTMENT RINGS:
TOP ADJUSTMENT RING OR RISER RING
TO BE "INFRA-RISER" TYPE
GNR RECYCLED RUBBER OR APPROVED EQUAL
TOTAL OF 3 RINGS MAY BE USED
FOR A 8" MAX ADJUSTMENT

RISER JOINTS AND FRAME AND COVER:
ALL JOINTS BETWEEN PRE-CAST ELEMENTS, ADJUSTING RINGS AND MANHOLE FRAMES ON ALL UNDERGROUND STRUCTURES- ALL MANHOLES IN THE ROADWAY AND PARKWAY, SHALL BE SET IN PLACE WITH ONE OF THE FOLLOWING BUTYL RUBBER JOINT SEALANTS; CONCRETE PRODUCTS SUPPLY CO. - EZ STIK 8, HAMILTON-KENT GASKET CO. - KENT SEAL, AS APPROVED BY DISTRICT ENGINEER AND ALL JOINTS TO BE TUCKPOINTED WITH HYDRAULIC CEMENT.

Heavy Duty
With Type M1 Grate and Type T1 Back
Approx. 130 sq. in. of opening
Curb Adjustable from 3 1/2" to 8" (89mm to 203mm)
See Table for assembled weights

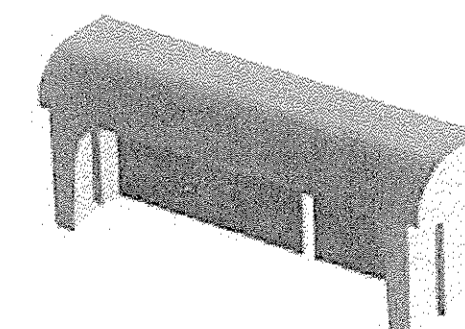


CATCH BASIN CURB INLETS

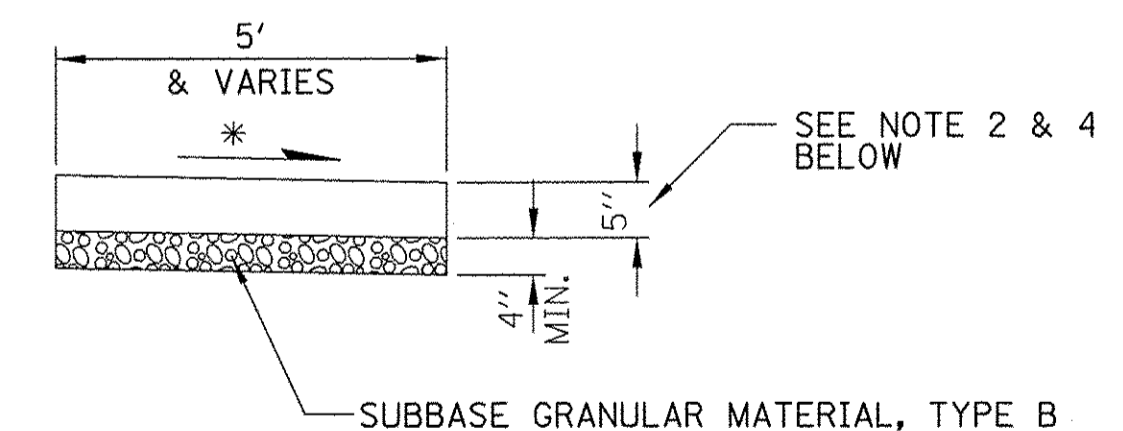
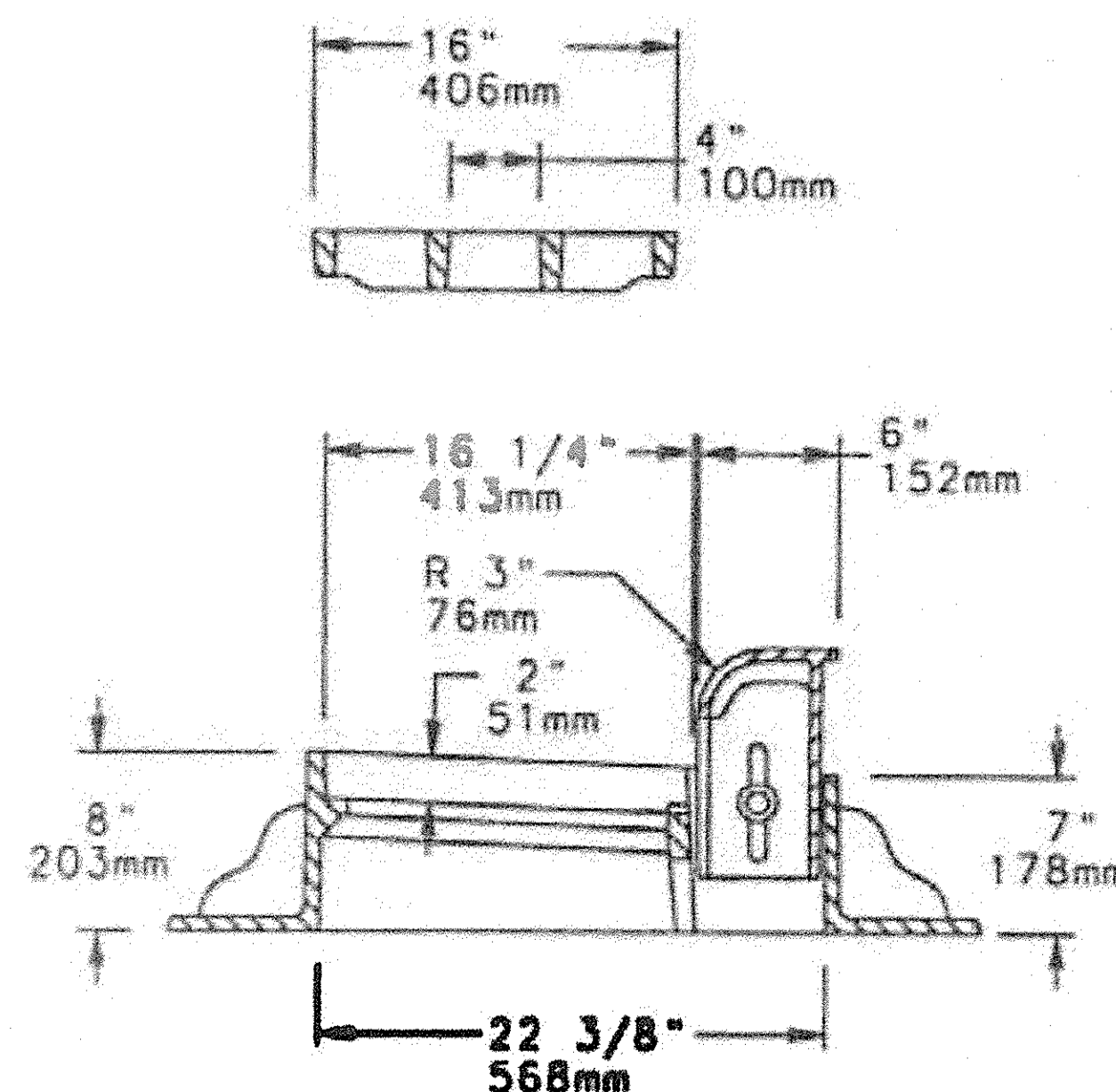


TYPE M4 Vane Grate
Approx. 125 sq. in. of opening

| CATALOG NUMBER | GRATE TYPE | | | |
|----------------|----------------|----------------|----------------|----------------|
| | M1 | M3 | M4 | M5 |
| Type T1 Back | 390lbs (177kg) | 390lbs (177kg) | 390lbs (177kg) | 380lbs (172kg) |
| Type T2 Back | 375lbs (170kg) | 375lbs (170kg) | 375lbs (170kg) | 365lbs (166kg) |



TYPE T1 Back
Curb Adjustable from 3 1/2" to 8" (89mm to 203mm)



* CROSS SLOPE 2% OR AS SHOWN ON CROSS SECTIONS

- 1. ALL REQUIRED EARTH EXCAVATION TO CONSTRUCT P.C.C. SIDEWALK SHALL BE INCLUDED IN THE COST OF P.C.C. SIDEWALK 5 INCH, REMOVE AND REPLACE
- 2. WHEN FORMS ARE REMOVED FROM THE SIDEWALK EITHER THE SIDEWALK SHALL BE BARRICADED OR BACKFIELD WITHIN 24 HOURS.
- 3. SEEDING AND TOPSOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF SIDEWALK REMOVAL AND REPLACEMENT.
- 4. PUBLIC SIDEWALK SHALL BE 6" AT RESIDENTIAL DRIVEWAYS AND 8" AT COMMERCIAL/INDUSTRIAL DRIVEWAYS.

P.C.C. SIDEWALK DETAIL

FILE NAME = N:\ZION\160172\Civil\det_160172-01.sht

USER NAME = jhouseh

DESIGNED - JEJ

REVISED -

DRAWN - EDT

REVISED -

PLOT SCALE = NOT TO SCALE

CHECKED - LMF

REVISED -

PLOT DATE = 11/11/2016

DATE - 10/31/2016

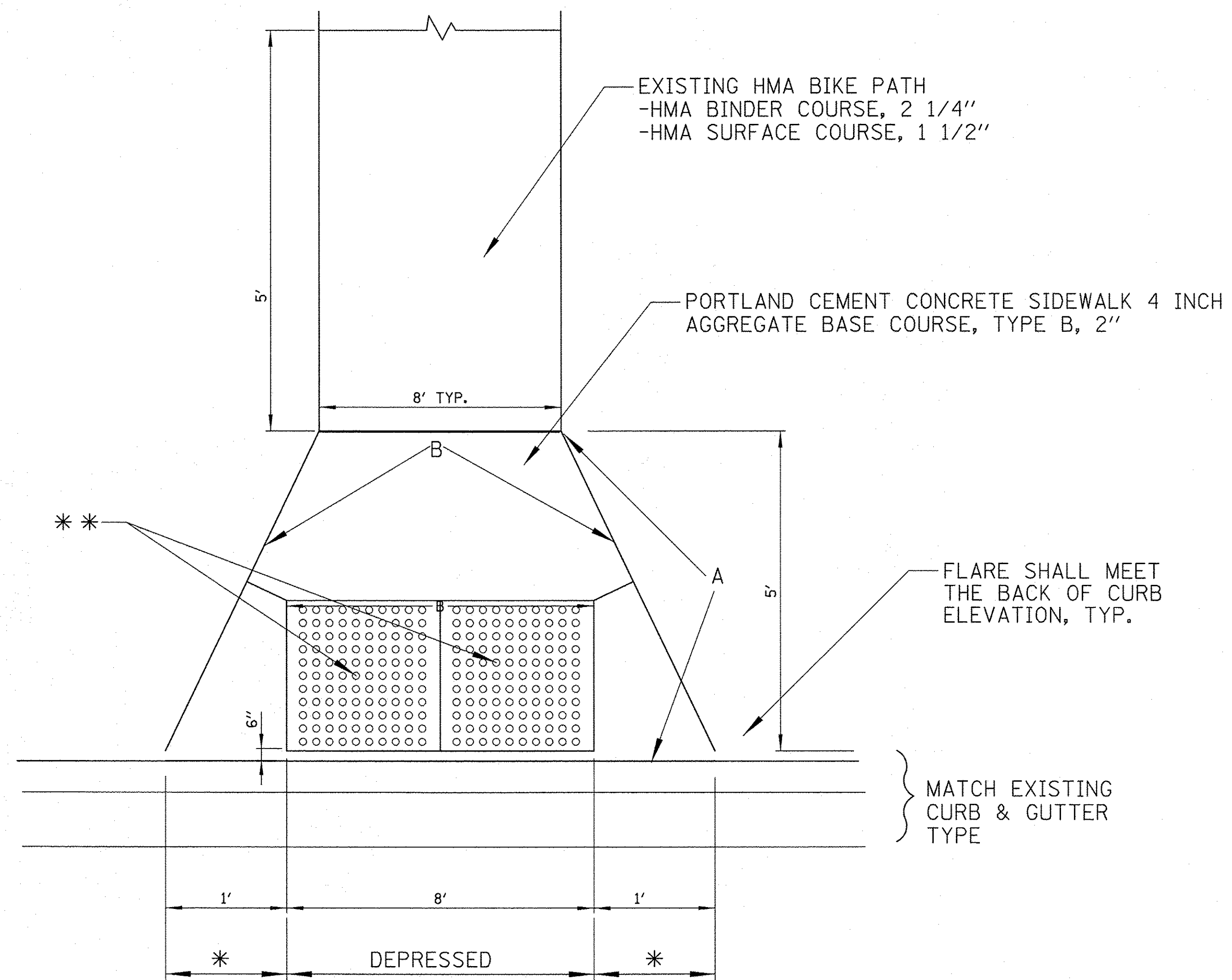
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

LORELEI DRIVE
CONSTRUCTION DETAILS

SCALE: N.T.S. SHEET NO. 15 OF 21 SHEETS STA. TO STA.

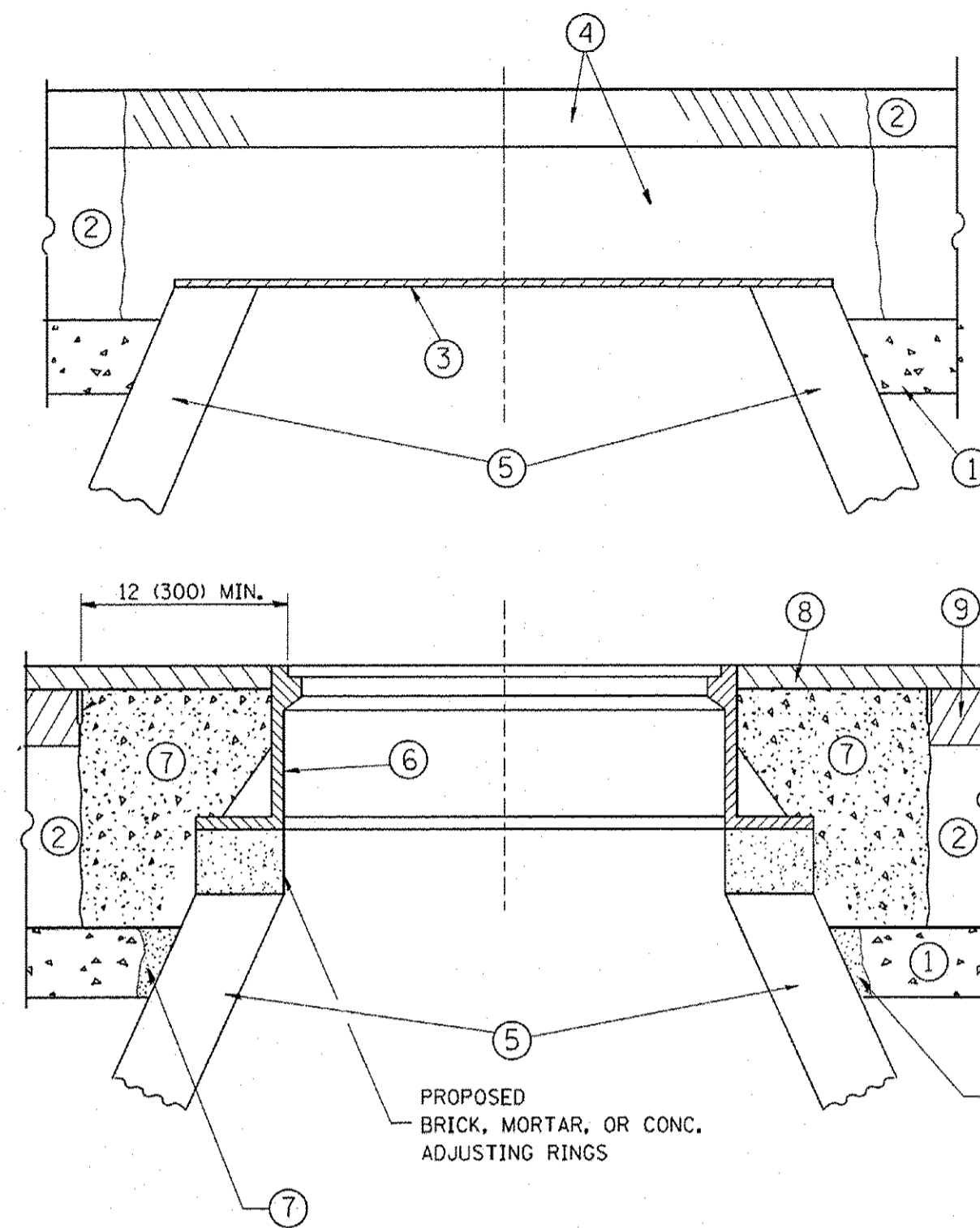
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------|--------|--------------|-----------|
| 1002 | 16-00087-00-RS | LAKE | 21 | 15 |
| CONTRACT NO. | | | 61041 | |
| ILLINOIS FED. AID PROJECT | | | | |



- * 1' TRANSITION FROM DEPRESSED CURB AND GUTTER TO FULL HEIGHT CURB AND GUTTER
- ** DETECTABLE WARNING PLATES
- A = 1/2" PREFORMED EXPANSION JOINT (TYP.)
- B = TOOLED OR SAWED CONTRACTION JOINT

ADA RAMP AT BIKE PATH
NOT TO SCALE

| | | | | | | | | | | | | |
|---|---------------------------|----------------|-----------|---|--|--|--|--------------------|----------------|---------------------------|--------------|-----------|
| FILE NAME = N:\ZION\160172\Civil\std.160172-04.sht | USER NAME = jhouseh | DESIGNED - JEH | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | LORELEI DRIVE ADA RAMP AT BIKE PATH | | | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | PLOT SCALE = NOT TO SCALE | DRAWN - EDT | REVISED - | | | | | 1002 | 16-00087-00-RS | LAKE | 21 | 16 |
| | PLOT DATE = 11/11/2016 | CHECKED - LMF | REVISED - | | SCALE: N.T.S. SHEET NO. 16 OF 21 SHEETS STA. TO STA. | | | CONTRACT NO. 61041 | | ILLINOIS FED. AID PROJECT | | |
| | DATE - 10/31/2016 | REVISOR - | REVISED - | | | | | | | | | |



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

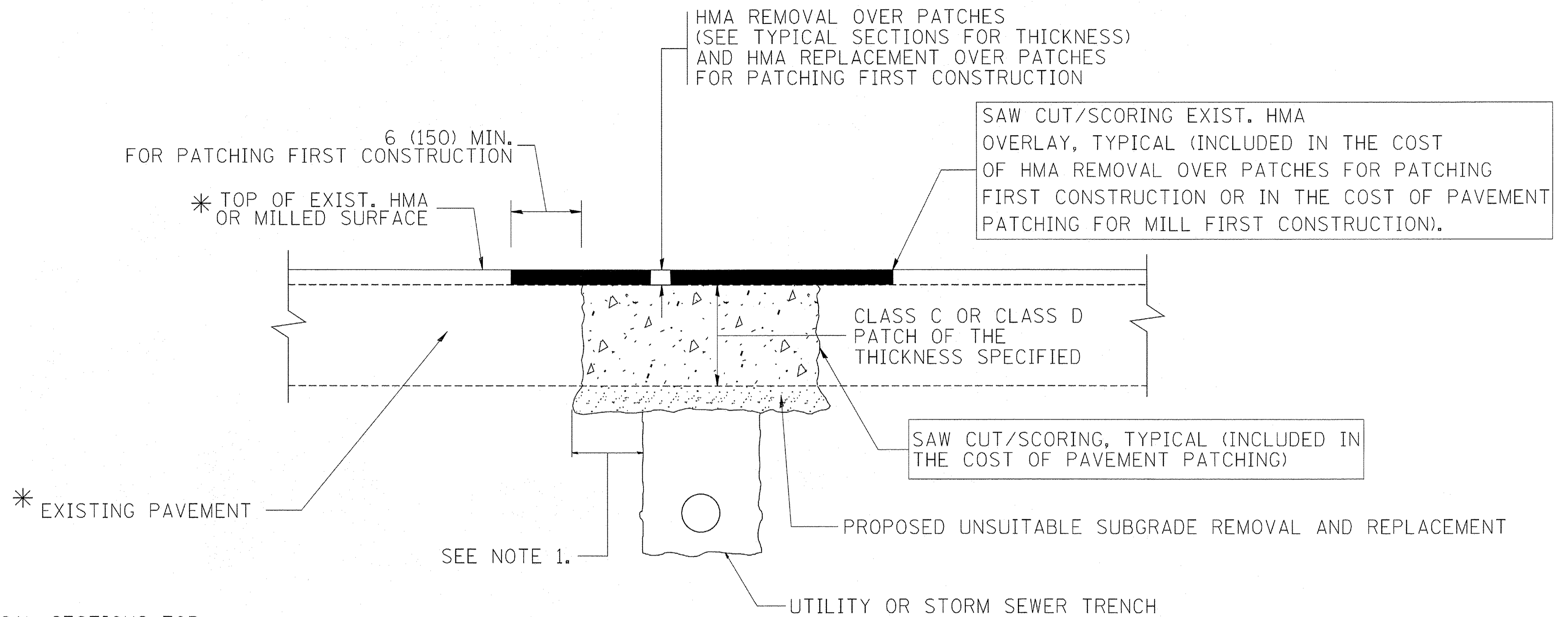
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| PLOT SCALE = 1/8" = 1'-0" | | CHECKED - | REVISED - R. BORO 03-09-11 |
| PLOT DATE = 12/6/2011 | | DATE - 10-25-94 | REVISED - R. BORO 12-06-11 |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|----------------|--------|--------------|-----------|
| 1002 | 16-00087-00-RS | LAKE | 21 | 17 |
| BD600-03 (BD-8) | | | CONTRACT NO. | 61041 |
| FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT | | | | |



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

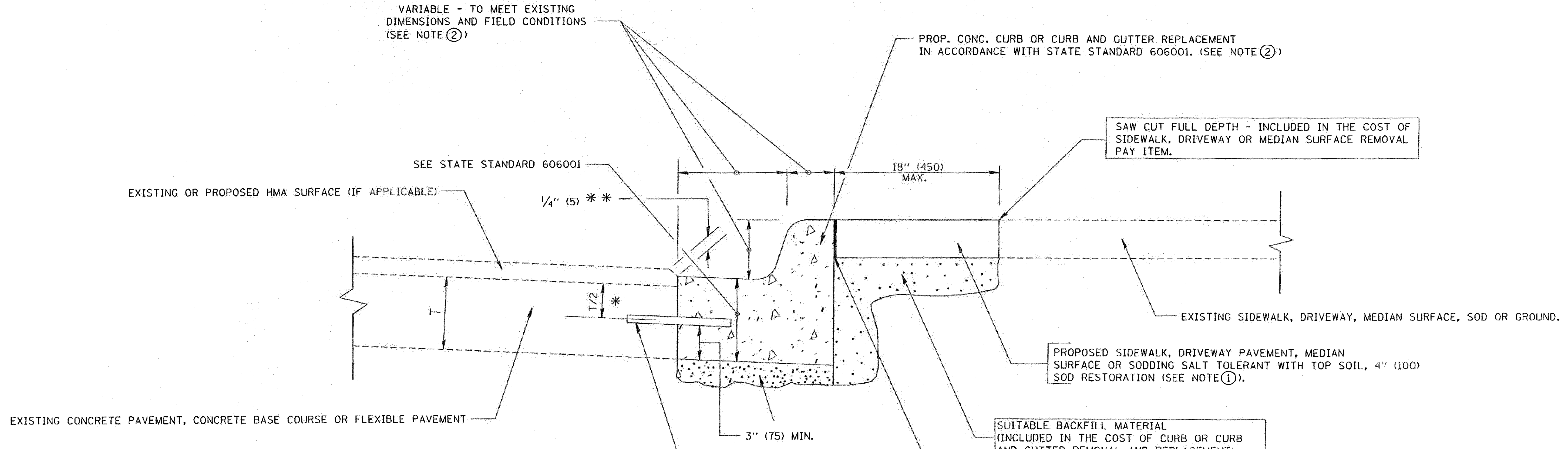
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| | | | | | | | | | | | | |
|--|----------------------------|--------------------|-----------------------------|---|--|-------------------------|------|-------------------|---------------------------|----------------|--------------------|-----------------|
| FILE NAME = c:\projects\diststd22x34\bd22.dgn | USER NAME = bouerd1 | DESIGNED - R. SHAH | REVISED - A. ABBAS 04-27-98 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT | | | F.A. RTE. 1002 | SECTION 16-00087-00-RS | COUNTY LAKE | TOTAL SHEETS 21 | SHEET NO. 18 |
| | PLOT SCALE = 50.000' / IN. | CHECKED - | REVISED - R. BORO 09-04-07 | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. | BD400-04 (BD-22) | | CONTRACT NO. | 61041 |
| | PLOT DATE = 10/27/2008 | DATE - 10-25-94 | REVISED - K. ENG 10-27-08 | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | | | | |
| | | | | | | | | | | | | |



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
 - ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

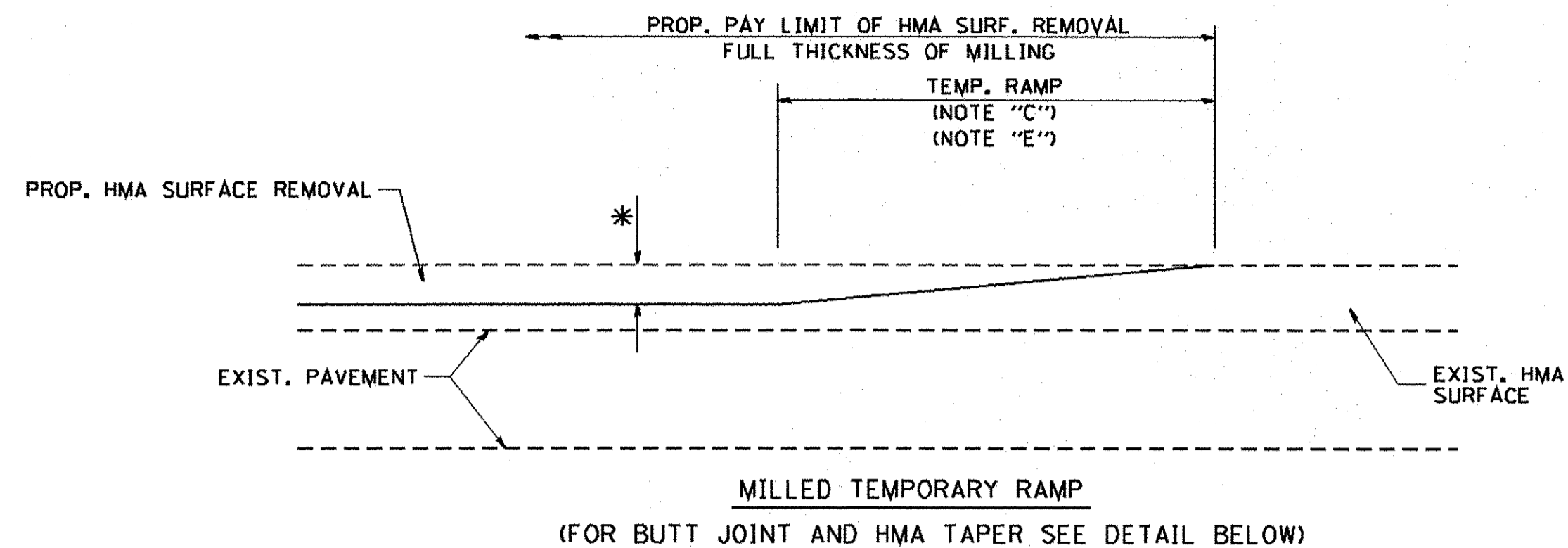
- PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)
- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

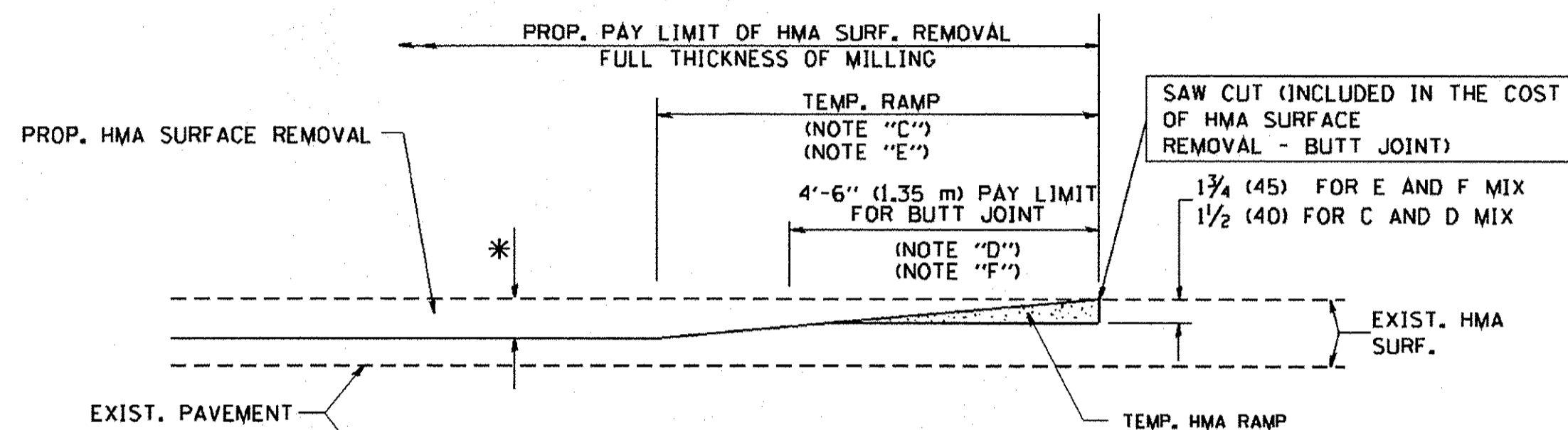
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| | | | | | | | | | | |
|-------------|------------------------|----------------------|-----------------------------|---|--|-------------------------|----------------|--------------------|---|-----------|
| FILE NAME = | USER NAME = drivakosgn | DESIGNED - A. HOUSEH | REVISED - R. SHAH 10-03-96 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT | F.A. RTE. = | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | DRAWN - | REVISED - A. ABBAS 03-21-97 | | | 1002 | 16-00087-00-RS | LAKE | 21 | 19 |
| | | CHECKED - | REVISED - M. GOMEZ 01-22-01 | | | BD600-06 (BD-24) | | CONTRACT NO. 61D41 | | |
| | | DATE - 03-11-94 | REVISED - R. BORO 12-15-09 | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | |



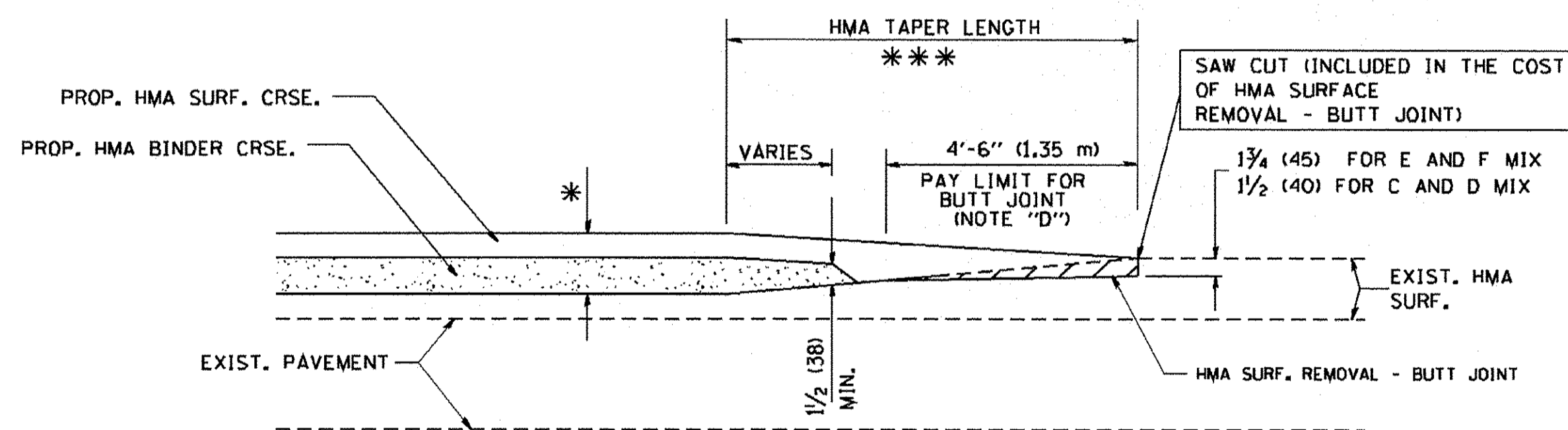
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

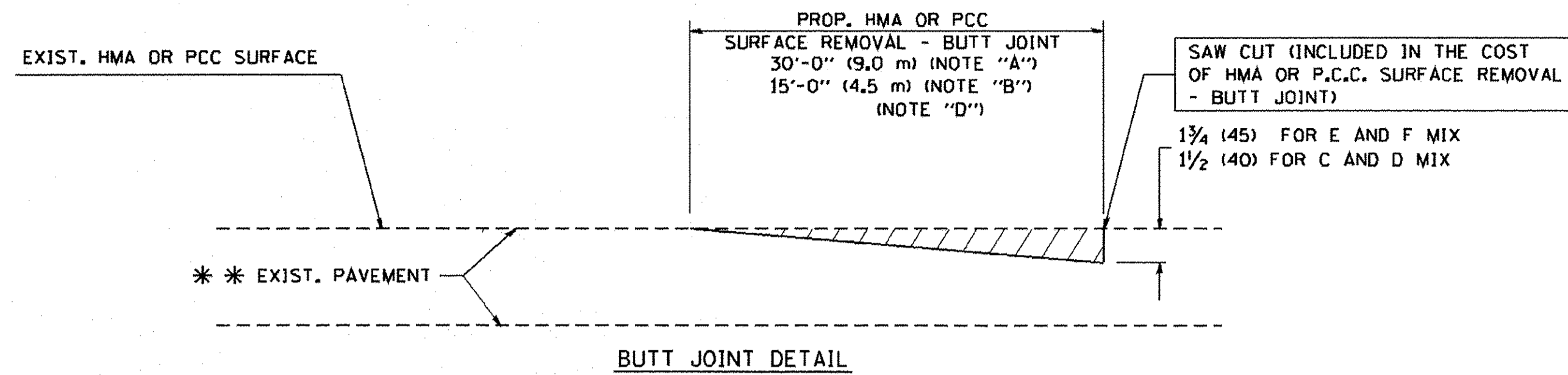
OPTION 2

TYPICAL TEMPORARY RAMP

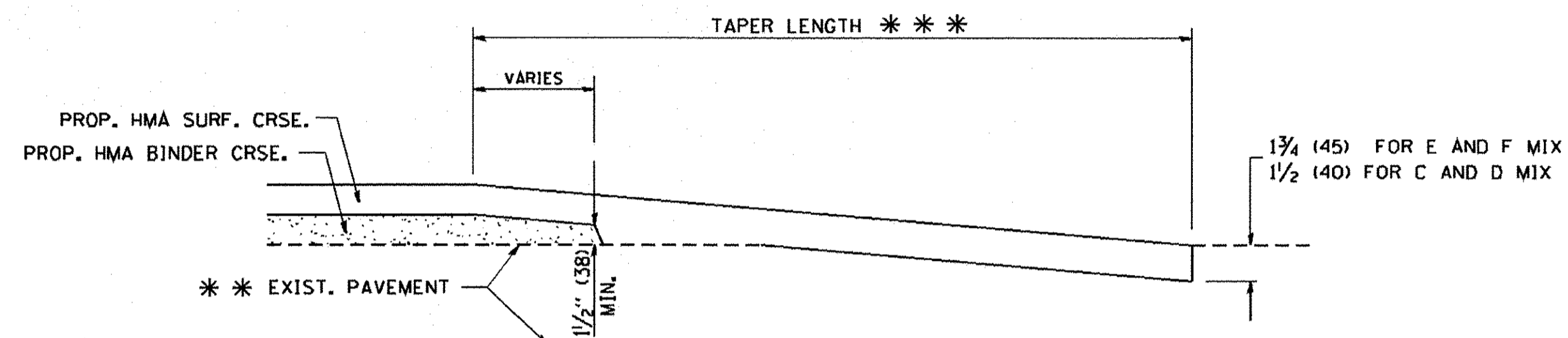


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

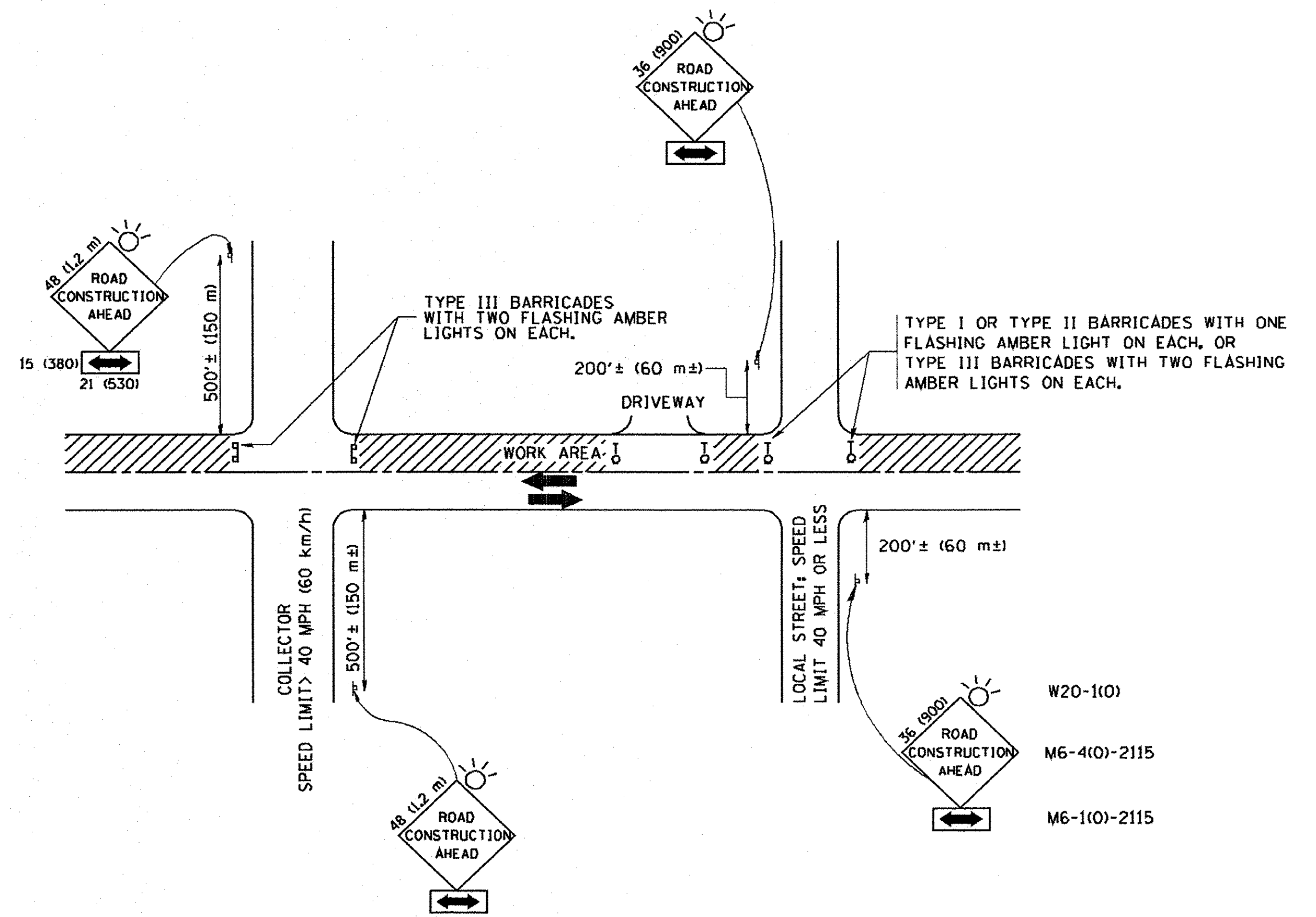
| | | | |
|--|---------------------------|--------------------------|--------------------------------|
| FILE NAME = W:\distatd\22x34\bd32.dgn | USER NAME = gaglionebt | DESIGNED - M. DE YONG | REVISED - R. SHAH 10-25-94 |
| | | DRAWN - | REVISED - A. ABBAS 03-21-97 |
| PLOT SCALE = 50.0000' / IN. | | CHECKED - | REVISED - M. GOMEZ 04-06-01 |
| PLOT DATE = 1/4/2008 | | DATE - 06-13-90 | REVISED - R. BORO 01-01-07 |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|----------------|--------------|--------------|-----------|
| 1002 | 16-00087-00-RS | LAKE | 21 | 20 |
| BD400-05 BD32 | | CONTRACT NO. | 61D41 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAYS

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

| | | | |
|---|----------------------|----------------|---------------------------------|
| FILE NAME = W:\dststd\22x34\tcl0.dgn | USER NAME = geglnebt | DESIGNED - LHA | REVISED - J. OBERLE 10-18-95 |
| | | DRAWN - | REVISED - A. HOUSEH 03-06-96 |
| | | CHECKED - | REVISED - A. HOUSEH 10-15-96 |
| | | DATE - 06-89 | REVISED - T. RAMMACHER 01-06-00 |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| | | | | |
|---|----------------|--------|--------------------|--------------|
| F.A. - RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1002 | 16-00087-00-RS | LAKE | 21 | 21 |
| TC-10 | | | CONTRACT NO. 61D41 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |