01-20-2017 LETTING ITEM 141

#48+1 = 49 TOTAL SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	1
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 60X38		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED
HIGHWAY PLANS

F.A.U. 1632 (SAUK TRAIL)
SECTION 49R–RS
1) AT BURNHAM AVE.
2) TORRENCE AVE TO US 30 (LINCOLN HIGHWAY)
LEFT TURN LANES, RESURFACING, GUARDRAIL, ADA
PROJECT: ACHSIP-1632(004)
COOK COUNTY
C–91–086–14

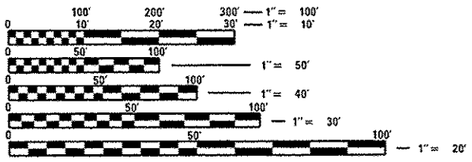
FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT IS LOCATED
WITHIN SAUK VILLAGE AND
UNINCORPORATED BLOOM TOWNSHIP

TRAFFIC DATA

SAUK TRAIL
2014 ADT – 10,900
POSTED SPEED LIMIT – 45 MPH
DESIGN SPEED LIMIT – 50 MPH

BURNHAM AVE.
2014 ADT – 1,800

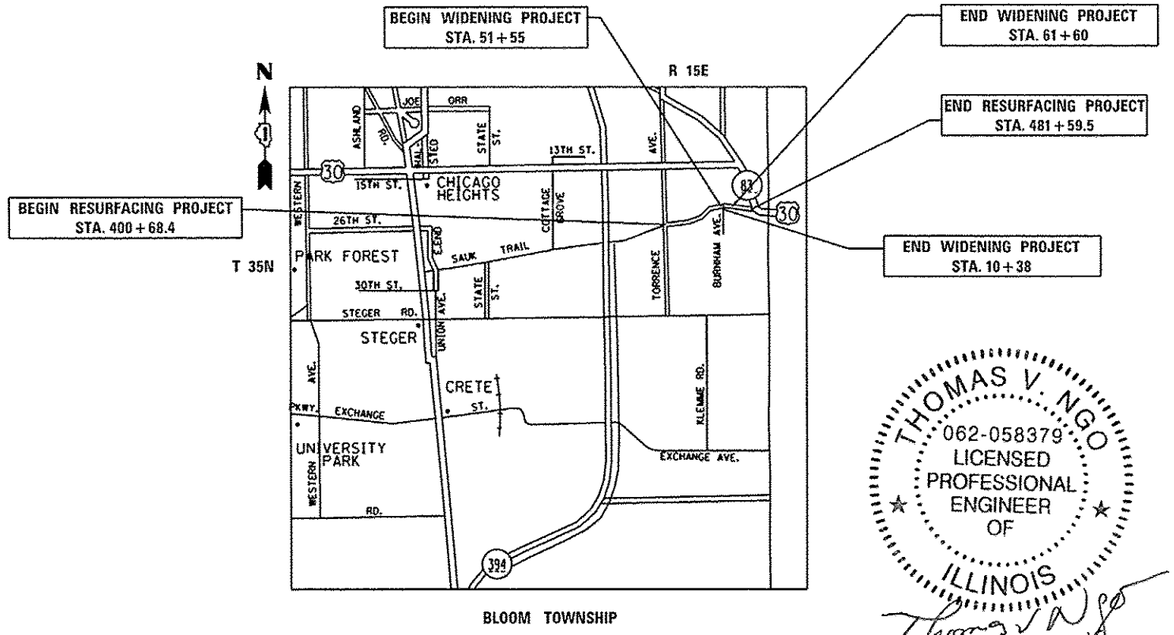


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1–800–892–0123
OR 811

PROJECT ENGINEER: JENPAI CHANG (847) 705–4432
PROJECT MANAGER: FAWAD AQUEEL (847) 702–4247

CONTRACT NO. 60X38

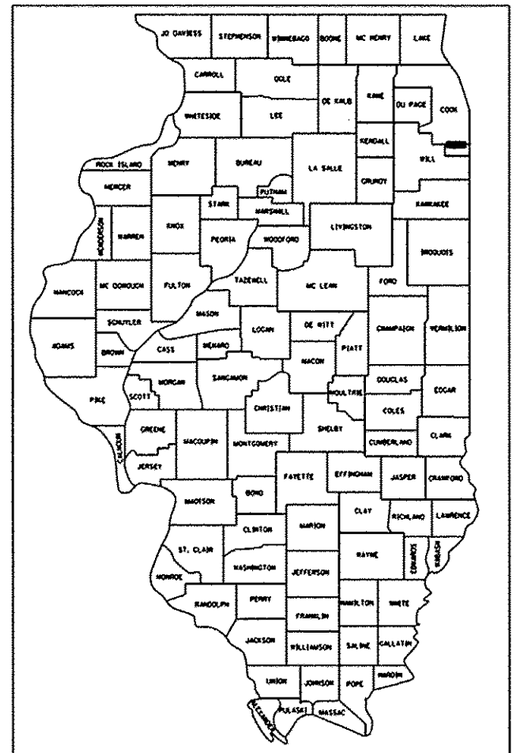


GROSS AND NET LENGTH OF PROJECT = 8,091 FT = 1.53 MI.

THOMAS V. NGO
062-058379
LICENSED
PROFESSIONAL
ENGINEER
OF
ILLINOIS
Thomas V. Ngo
10-14-16

SHEET NO. 9,10,15-19

D–91–086–14



LOCATION OF SECTION INDICATED THUS: –

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED October 25, 20 16
John Fortmann for
REGIONAL ENGINEER
Dec 9 20 16
Maureen M. Addis, PE for
ENGINEER OF DESIGN AND ENVIRONMENT
Dec 9 20 16
Omer Osman, PE for
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

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701006-05	OFF ROAD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701201-04	LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS > 45 MPH
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701306-03	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS > 45 MPH
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725001-01	OBJECT AND TERMINAL MARKERS
782006	GUARDRAIL BARRIER WALL REFLECTOR MOUNTING DETAILS

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		DRAWN -	REVISED -
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	PLOT DATE = 12/5/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS

SCALE: | SHEET NO. 1 OF 2 SHEETS | STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	2
		CONTRACT NO. 60X38		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

GENERAL NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED)

10 FEET (3 METER) TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS & GUTTERS AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND SAUK VILLAGE.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTERWISE SPECIFIED.

UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.

PRIOR TO EMBANKMENT PLACEMENT, ALL VEGETATION, LOOSE MATERIAL, AND UNSTABLE MATERIAL SHOULD BE REMOVED TO DEPTH ENCOUNTERED AND REPLACED WITH SUITABLE EMBANKMENT MATERIAL. ANY EMBANKMENT WIDENING ON EXISTING SLOPES SHOULD BE BENCHED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.

THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS FOR THE PROTECTION OF THE EXISTING PLANT MATERIAL FOR WHICH THE CONTRACT DOES NOT PROVIDE REMOVAL. THE PROTECTION OF EXISTING PLANT MATERIAL AND THE REPAIR OR REPLACEMENT OF THE EXISTING PLANT MATERIAL DAMAGED BY THE CONTRACTOR SHALL BE DONE IN ACCORDANCE WITH THE REQUIREMENT OF SECTION 201 OF THE STANDARD SPECIFICATIONS AT THE CONTRACTORS OWN EXPENSE.

PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH THE "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL. (TC-13)

RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL APPLICATION RAISED REFLECTIVE PAVEMENT MARKERS DETAIL."

THE RESIDENT ENGINEER SHALL VERIFY THE LOCATIONS OFF ALL EXISTING PAVEMENT MARKINGS PRIOR TO MILLING OR RESURFACING.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE A FIELD LABORATORY FOR USE FOR ANY ON SITE TESTING BY THE ENVIRONMENTAL FIRM. NO TESTING OF ANY KIND, CONTAMINATED OR NON-CONTAMINATED FLUID OR SOLID SHALL BE PERMITTED IN THE ENGINEER'S FIELD OFFICE.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER

THE RESIDENT ENGINEER SHALL CONTACT THE AREA TRAFFIC FIELD TECHNICIAN, MS. PATRICE HARRIS, AT PATRICE.HARRIS@ILLINOIS.GOV TWO WEEKS PRIOR TO PLACING PERMANENT PAVEMENT MARKINGS.

THE CONTRACTOR SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE DEPARTMENT HAS NOT OBTAINED ANY PERMITS FOR OFFSITE BORROW, WASTE, USE (BWU) AREAS. PRIOR TO WORKING IN BWU AREAS, IF THE CONTRACTOR CHOOSES TO USE ACTIVITIES REQUIRING PERMITS IT IS THE CONTRACTOR'S RESPONSIBILITY TO SECURE THE PROPER PERMITS. IN ADDITION TO THE BORROW REVIEW (BDE 2289) AND USE/WASTE REVIEW(BDE 2290) SUBMITTALS, THE CONTRACTOR SHALL SUBMIT AN EROSION AND SEDIMENT CONTROL (ESC) PLAN FOR EVERY BWU SITE TO THE DEPARTMENT FOR ACCEPTANCE. GUIDELINES FOR ACCEPTABLE BWU PRACTICES CAN BE FOUND IN SECTION II.G.1 AND 2 OF THE SWPPP. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT ESC PLANS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE CONTRACTOR'S ATTENTION IS CALLED TO THE FACT THAT THE PRESERVATION OF EXISTING TREES IS OF UTMOST IMPORTANCE TO THE VILLAGE OF SAUK. IT WILL BE THE RESPONSIBILITY OF THE CONTRACT TO ARRANGE TREE PROTECTION WITH THE ROADSIDE DEVELOPMENT UNIT (847.705.4171) PRIOR TO SCHEDULING TREE REMOVAL. ALL TREE PROTECTION, TREE REMOVAL, PRUNING AND ROOT PRUNING SHALL BE COMPLETED BEFORE CONSTRUCTION OPERATIONS COMMENCE IN ANY AREA. AT NO TIME SHALL THE CONTRACTOR PRUNE OR REMOVE ANY TREES UNLESS SPECIFICALLY DIRECTED BY THE ROADSIDE DEVELOPMENT UNIT.

SUPPLEMENTAL WATERING IS SPECIFIED FOR TREES AND SHRUBS THAT WILL BE DISTURBED BY CONSTRUCTION BUT WILL REMAIN. NOTE THAT WATERING SHOULD BEGIN IMMEDIATELY AFTER ROOT PRUNING, TOP PRUNING OR OTHER CONSTRUCTION DISTURBANCE.

AGGREGATE SUBGRADE IMPROVEMENT (CU YD) HAS BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR THE SOILS THAT TEND TO BE UNSTABLE AND/OR UNSUITABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH ASI WILL BE DETERMINED IN THE FIELD AT THE TIMEOF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH STATIC OR DYMANIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.04 OF THE SSRBC AND IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE SOILS ARE NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.

PIPE UNDERDRAINS TYPE 2 SHALL BE INSTALLED ACCORDING TO SECTION 601 OF THE SSRBC AND STANDARD 601001-05. TOP OF PIPE UNDERDRAINS SHALL BE PLACED 6" BELOW THE SUBGRADE OR UNDERCUT. THE COST OF MAKING PIPE UNDERDRAINS CONNECTIONS TO DRAINAGE STRUCTURES SHALL BE INCLUDED IN THE COST OF THE PIPE UNDERDRAINS.

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEMS SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC. AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

EXISTING VEGETATED AREAS (TREES, SHRUBS, VEGETATIVE BUFFERS, TURF AREAS, ETC.) WHERE DISTURBANCE IS NOT OCCURRING (INCLUDING AREAS OUTSIDE THE PROJECT LIMITS) SHALL NOT BE DISTURBED TO ENSURE THAT EXISTING VEGETATION IS PRESERVED TO MINIMIZE SOIL EROSION AND TO ELIMINATE SOIL COMPACTION. NO MATERIAL ARE TO BE STORED OR VEHICLES DRIVEN OR PARKED WITHIN THESE UNDISTURBED AREAS AT ANY TIME.

PRUNE TREE LIMBS THAT MIGHT BE DAMAGED BY EQUIPMENT OPERATIONS AT LEAST ONE WEEK PRIOR TO THE START OF CONSTRUCTION BY A CERTIFIED ARBORIST. ANY TREE LIMBS THAT ARE BROKEN BY CONSTRUCTION EQUIPMENT AFTER THE INITIAL PRUNING MUST BE PRUNED CORRECTLY WITHIN 72 HOURS.

THE CONTRACTOR SHALL TAKE EXTRA CARE IN GRADING AND EXCAVATING NEAR TREES WHICH ARE NOT MARKED FOR REMOVAL SO AS NOT TO CAUSE INJURY TO THE ROOT SYSTEM OR TRUNKS. ROOTS OF A TREE THAT ARE TO REMAIN IN PLACE EXTENDING INTO THE EXCAVATION AREAS AT AN ELEVATION THAT WOULD INTERFERE WITH ANY PORTION OF THE PLANNED CONSTRUCTION SHALL BE SEVERED AT A POINT IMMEDIATELY OUTSIDE OF THE EXCAVATION AREA IN A MANNER THAT WILL CAUSE THE LEAST AMOUNT OF SYSTEMIC DAMAGE TO THE REMAINING TREE STRUCTURE. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE.

THE CONTRACTOR SHALL ERECT A TEMPORARY FENCE AROUND ALL TREES WITHIN THE CONSTRUCTION AREA TO ESTABLISH A "TREE PROTECTION ZONE" AND AROUND EXISTING WETLANDS TO ESTABLISH A "WETLAND PROTECTION ZONE" BEOFRE ANY WORK BEGINS OR ANY MATERIAL IS DELIVERED TO THE JOBSITE. NO WORK IS TO BE PERFORMED (OTHER THAN ROOT PRUNING), MATERIALS STORED OR VEHICLES DRIVEN OR PARKED WITHIN THE "TREE PROTECTION ZONE" AND "WETLAND PROTECTION ZONE". REMOVE TEMPORARY FENCE ONLY AFTER ALL CONSTRUCTION WORK HAS BEEN COMPLETED.

THE CONTRACTOR SHALL ATTACH AN ALUMINUM SIGN WITH THE FOLLOWING TEXT: "PROTECTED WETLAND - NO INTRUSION". THE SIGN(S) SHALL BE ATTACHED TO THE STAKES BY A METHOD APPROVED BY THE ENGINEER. THE SIGN(S) WILL BE PROVIDED BY THE DEPARTMENT AND SHALL BE PICKED UP BY THE CONTRACTOR FROM THE DISTRICT ONE ROADSIDE DEVELOPMENT ARCHITECT IN SCHAUMBURG, ILLINOIS. SCHEDULING THE PICKUP OF THE SIGNS CAN BE ARRANGED BY CONTACTING THE DISTRICT ONE ROADSIDE DEVELOPMENT UNIT AT 847.705.4171. WHEN WORK HAS BEEN COMPLETED, THE SIGN(S) SHALL BE RETURNED TO THE DISTRICT ONE ROADSIDE DEVELOPMENT UNIT. THE COST OF PICKING UP, ATTACHING THE SIGN(S) TO THE TEMPORARY FENCE STAKES AND RETURNING THE SIGN(S) WILL NOT BE PAID FOR SEPERATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR TEMPORARY FENCE.

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	PLOT SCALE = 100.0000 ' / in.	DRAWN - ___	REVISED - ___
	PLOT DATE = 11/22/2016	CHECKED - ___	REVISED - ___
		DATE - _____	REVISED - ___

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

SCALE: _____ | SHEET NO. 2 OF 2 SHEETS | STA. _____ TO STA. _____

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	3
		CONTRACT NO. 60X38		
FED. ROAD DIST. NO. _	ILLINOIS	FED. AID PROJECT		

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URBAN — 90% FED / 10% STATE

SUMMARY OF QUANTITIES

CODE NO	ITEM	UNIT	TOTAL QUANTITIES	CONSTRUCTION TYPE CODE: ROADWAY 0004
20200100	EARTH EXCAVATION	CU YD	1376	1376
20101000	TEMPORARY FENCE	FOOT	250	250
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	767	767
20101300	TREE PRUNING (1-10 INCH DIAMETER)	EACH	10	10
20400800	FURNISHED EXCAVATION	CU YD	45	45
20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	10	10
20800150	TRENCH BACKFILL	CU YD	225	225
20100110	TREE REMOVAL (6 TO 15 INCH DIAMETER)	UNIT	45	45
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	200	200
20100210	TREE REMOVAL (OVER 15 INCH DIAMETER)	UNIT	40	40
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	200	200
21101625	TOPSOIL FURNISH AND PLACE, 6"	SQ YD	400	400
21400100	GRADING AND SHAPING DITCHES	FOOT	2150	2150
X2501800	SEEDING, CLASS 4 (MODIFIED)	ACRE	0.25	0.25
~~25000400~~	~~NITROGEN FERTILIZER NUTRIENT~~	~~POUND~~	~~30~~	~~30~~
~~25000600~~	~~POTASSIUM FERTILIZER NUTRIENT~~	~~POUND~~	~~30~~	~~30~~
25000750	MOWING	ACRE	2	2
25100630	EROSION CONTROL BLANKET	SQ YD	1652	1652
25200110	SODDING, SALT TOLERANT	SQ YD	400	400
25200200	SUPPLEMENTAL WATERING	UNIT	30	30
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	300	300

* SPECIALITY ITEMS
~~NP NON-PARTICIPATING ITEMS~~

URBAN — 90% FED / 10% STATE

SUMMARY OF QUANTITIES

CODE NO	ITEM	UNIT	TOTAL QUANTITIES	CONSTRUCTION TYPE CODE: ROADWAY 0004
28000400	PERIMETER EROSION BARRIER	FOOT	850	850
28000510	INLET FILTERS	EACH	6	6
30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	550	550
30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	2925	2925
35101582	AGGREGATE BASE COURSE, TYPE B 2"	SQ YD	501	501
35501308	HOT-MIX ASPHALT BASE COURSE, 6"	SQ YD	236	236
35501310	HOT-MIX ASPHALT BASE COURSE 6 1/2"	SQ YD	2925	2925
35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SQ YD	20	20
35501324	HOT-MIX ASPHALT BASE COURSE, 10"	SQ YD	501	501
35600702	HOT-MIX ASPHALT BASE COURSE WIDENING, 6 1/2"	SQ YD	30	30
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	23950	23950
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	46	46
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	1550	1550
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	140	140

SUMMARY OF QUANTITIES

URBAN — 90% FED 10% STATE

CODE NO	ITEM	UNIT	TOTAL QUANTITIES	CONSTRUCTION TYPE CODE: ROADWAY 0004					
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SQ YD	316	316					
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	250	250					
40603085	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON	51	51					
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	85	85					
40603565	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70	TON	3134	3134					
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	428	428					
42400800	DETECTABLE WARNINGS	SQ FT	26	26					
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	400	400					
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	24470	24470					
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	1450	1450					
44000600	SIDEWALK REMOVAL	SQ FT	337	337					

* SPECIALITY ITEMS
~~NP NON-PARTICIPATING ITEMS~~

SUMMARY OF QUANTITIES

URBAN — 90% FED 10% STATE

CODE NO	ITEM	UNIT	TOTAL QUANTITIES	CONSTRUCTION TYPE CODE: ROADWAY 0004					
44002210	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 2 1/2"	SQ YD	1850	1850					
44003100	MEDIAN REMOVAL	SQ FT	960	960					
44003510	MEDIAN REMOVAL PARTIAL DEPTH	SQ FT	910	910					
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SQ YD	650	650					
44201757	CLASS D PATCHES, TYPE III, 9 INCH	SQ YD	525	525					
44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SQ YD	435	435					
44213204	TIE BARS 3/4"	EACH	587	587					
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	500	500					
54213657	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12"	EACH	1	1					
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	925	925					
55100500	STORM SEWER REMOVAL 12"	FOOT	900	900					
60100060	CONCRETE HEADWALLS FOR PIPE DRAINS	EACH	7	7					
60108100	PIPE UNDERDRAINS 4" (SPECIAL)	FOOT	250	250					
60108204	PIPE UNDERDRAINS, TYPE 2, 4"	FOOT	1700	1700					

14

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DESIGNED - | DRAWN - | CHECKED - | DATE -
REVISED - | REVISED - | REVISED - | REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	5

CONTRACT NO. 60X38

FED. ROAD DIST. NO. 1 | ILLINOIS FED. AID PROJECT

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SUMMARY OF QUANTITIES			URBAN	90% FED 10% STATE	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	ROADWAY 0004						
60200805	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE	EACH	5	5						
	8 GRATE									
60219000	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 8	EACH	4	4						
	GRATE									
60262700	INLETS TO BE RECONSTRUCTED	EACH	1	1						
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	7	7						
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	3	3						
60605000	COMBINATION CONCRETE CURB AND GUTTER,	FOOT	140	140						
	TYPE B-6.24									
60608300	COMBINATION CONCRETE CURB AND GUTTER,	FOOT	1083	1083						
	TYPE M-2.12									
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1	EACH	3	3						
	(SPECIAL) TANGENT									
63200310	GUARDRAIL REMOVAL	FOOT	150	150						
64200108	SHOULDER RUMBLE STRIPS, 8 INCH	FOOT	1598	1598						
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	19	19						
* 66900450	SPECIAL WASTE PLANS AND REPORTS	LSUM	1	1						
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	2	2						
	# SPECIALTY ITEM									

* SPECIALITY ITEMS
NP NON-PARTICIPATING ITEMS

SUMMARY OF QUANTITIES			URBAN	90% FED 10% STATE	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	ROADWAY 0004						
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6						
67100100	MOBILIZATION	LSUM	1	1						
70100450	TRAFFIC CONTROL AND PROTECTION,	LSUM	1	1						
	STANDARD 701201									
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1						
70100500	TRAFFIC CONTROL AND PROTECTION,	LSUM	1	1						
	STANDARD 701326									
70100600	TRAFFIC CONTROL AND PROTECTION, STANDARD 701336	L SUM	1	1						
70102620	TRAFFIC CONTROL AND PROTECTION,	LSUM	1	1						
	STANDARD 701501									
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1						
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	6	6						
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1						
70300100	SHORT TERM PAVEMENT MARKING	FOOT	6450	6450						
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	2150	2150						
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND	SQ FT	200	200						
	SYMBOLS									
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	35500	35500						
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	250	250						
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	120	120						
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	60	60						
~~70301000~~	~~WORK ZONE PAVEMENT MARKING REMOVAL~~	~~SQ FT~~	~~200~~	~~200~~						
72000100	SIGN PANEL - TYPE 1	SQ FT	16	16						

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	498-RS	COOK	48	6
		CONTRACT NO. 60X38		
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			

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SUMMARY OF QUANTITIES

CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	90% FED 10% STATE CONSTRUCTION TYPE CODE ROADWAY 0004					
*78000100	THERMOPLASTIC PAVEMENT MARKING -	SQ FT	200	200					
	LETTERS AND SYMBOLS								
72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	30	30					
*78000200	THERMOPLASTIC PAVEMENT MARKING - LINE	FOOT	35500	35500					
	4"								
*78000400	THERMOPLASTIC PAVEMENT MARKING - LINE	FOOT	250	250					
	6"								
*78000600	THERMOPLASTIC PAVEMENT MARKING - LINE	FOOT	120	120					
	12"								
*78000650	THERMOPLASTIC PAVEMENT MARKING - LINE	FOOT	60	60					
	24"								
*78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	392	392					
78300200	RAISED REFLECTIVE PAVEMENT MARKER	EACH	350	350					
	REMOVAL								
* 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL	EACH	1	1					
	INSTALLATION								
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	260	260					
* 89502376	REBUILD EXISTING HANDHOLE	EACH	1	1					
* 89502378	REBUILD EXISTING HANDHOLE TO HEAVY-DUTY	EACH	1	1					
	HANDHOLE								
K1005863	TREE ROOT PRUNING	EACH	5	5					
* X0322936	REMOVE EXISTING FLARED END SECTION	EACH	2	2					

SUMMARY OF QUANTITIES

CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	90% FED 10% STATE CONSTRUCTION TYPE CODE ROADWAY 0004					
X2020110	GRADING AND SHAPING SHOULDERS	UNIT	125	125					
X0327980	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	600	600					
X4400100	PORTLAND CEMENT CONCRETE SURFACE	SQ YD	1137	1137					
	REMOVAL (VARIABLE DEPTH)								
□ X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	1328	1328					
X7010216	TRAFFIC CONTROL AND PROTECTION,	LSUM	1	1					
	(SPECIAL)								
X2043900	PREFORMED JOINT FILLER REMOVAL	FOOT	2346	2346					
Z0013798	CONSTRUCTION LAYOUT	LSUM	1	1					
□ Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	22	22					
Z0018700	DRAINAGE STRUCTURE TO BE REMOVED	EACH	7	7					
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	104	104					
Z0064800	SELECTIVE CLEARING	UNIT	7	7					

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	DATE	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	7

CONTRACT NO. 60X38

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT

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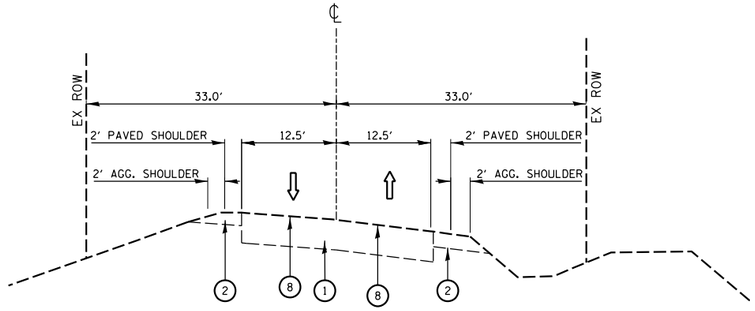
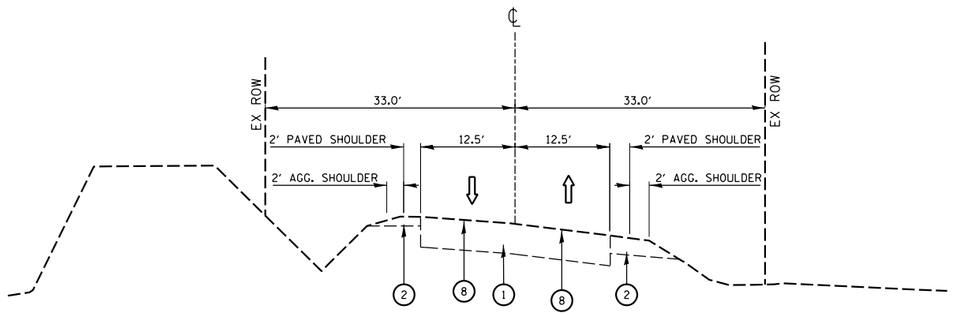
* SPECIALTY ITEM □ NON-PART. (100 % STATE)



EXISTING TYPICAL SECTION
SAUK TRAIL
STA. 51+85 TO STA. 55+00



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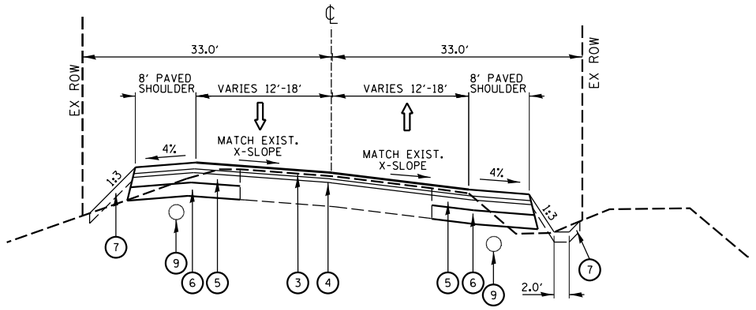
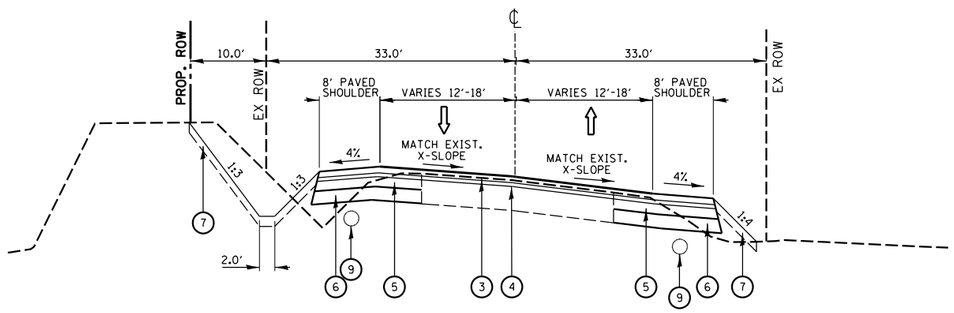


PROPOSED TYPICAL SECTION
SAUK TRAIL
STA. 51+85 TO STA. 55+00



PROPOSED TYPICAL SECTION
SAUK TRAIL
STA. 55+00 TO STA. 61+60

1. EXISTING HMA SURFACE
2. EXISTING SHOULDER
3. PROPOSED POLY. HMA SURFACE COURSE, MIX "E", N70, 1 3/4"
4. PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4"
5. PROPOSED HMA BASE COURSE, 6 1/2"
6. PROPOSED AGGREGATE SUBGRADE IMPROVEMENT, 12"
7. PROPOSED TOPSOIL, 6"
8. PROPOSED HMA SURFACE REMOVAL, 2 1/2"
9. PROPOSED PIPE UNDERDRAINS

NOTE:

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT QUANTITIES IS 112 LBS./SQ. YD./ IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFED BY DISTRICT ONE SPECIAL PROVISIONS

FOR THE USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS

QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE

CONTRACTOR SHALL PATCH FIRST BEFORE MILLING

MIXTURE REQUIREMENTS

MIXTURE PURPOSE	MIXTURE USE	AIR VOIDS @NDES	QMP
PATCHING	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES, (HMA BINDER IL-19MM)	4% @ 70 GYR	QC/QA
PATCHING	CLASS "D" PATCHES, (HMA BINDER IL-19MM)	4% @ 70 GYR	QC/QA
RESURFACING/WIDENING/ SHOULDER	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, IL-9.5MM	4% @ 70 GYR	QCP
RESURFACING/WIDENING/ SHOULDER	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75MM, N50	3.5% @ 50 GYR	QCP
WIDENING/SHOULDER	HMA BASE COURSE, 6 1/2" (HMA BINDER IL-19.0)	4% @ 70 GYR	QCP
DRIVEWAYS	HOT-MIX ASPHALT BASE COURSE, PE - 6" & CE - 8", (HMA BINDER IL-19MM)	4% @ 50 GYR	QC/QA
DRIVEWAYS	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, IL-9.5MM	4% @ 50 GYR	QC/QA
MEDIAN	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5 MM) - 2"	4% @ 50 GYR	QC/QA
MEDIAN	HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19 MM) - 10"	4% @ 50 GYR	QC/QA
SHOULDER REPAIR	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	4% @ 70 GYR	QC/QA

QMP DESIGNATIONS: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PFP)

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		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SAUK TRAIL at BURNHAM AVE.
TYPICAL SECTIONS

SCALE: _____ SHEET ___ OF ___ SHEETS STA. _____ TO STA. _____

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	8
		CONTRACT NO. 60X38		
		ILLINOIS FED. AID PROJECT		

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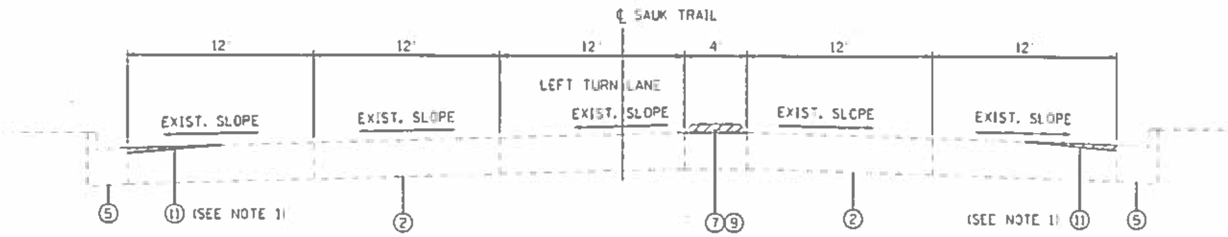

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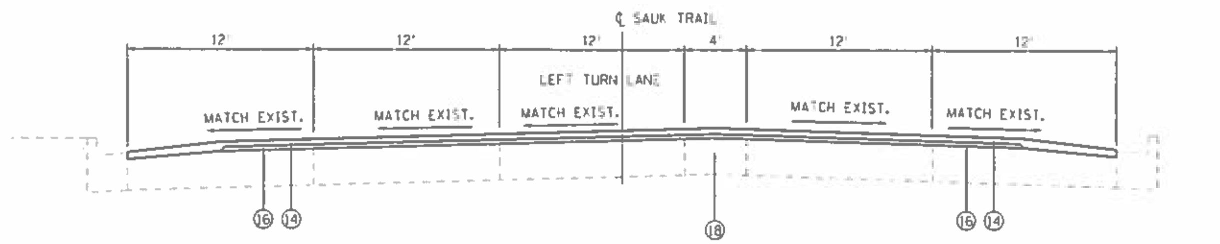
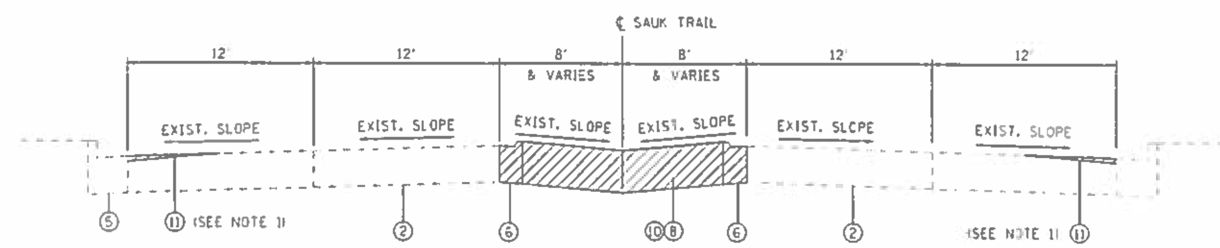
SAUK TRAIL
STA. 400+74 TO STA. 403+01



PROPOSED TYPICAL SECTION
SAUK TRAIL
STA. 400+74 TO STA. 403+01



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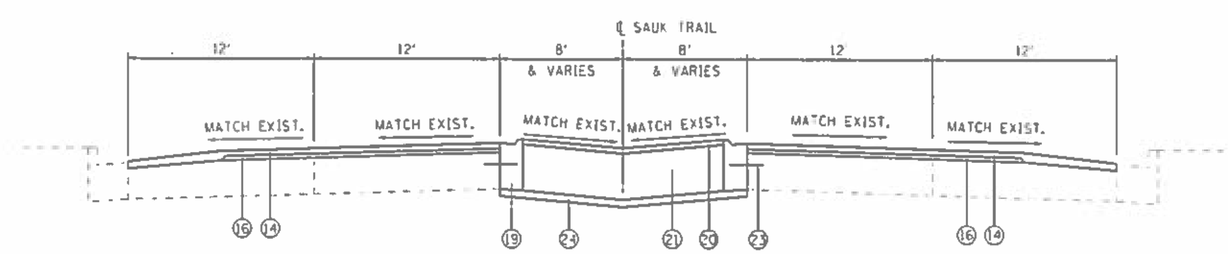
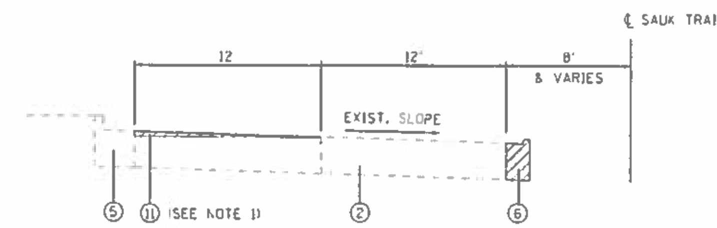
SAUK TRAIL
STA. 403+01 TO STA. 408+43



PROPOSED TYPICAL SECTION
SAUK TRAIL
STA. 403+01 TO STA. 408+43



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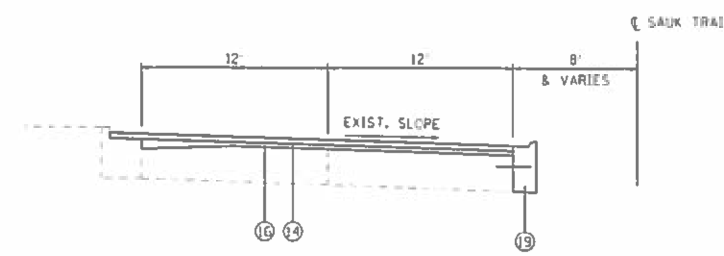
STA. 405+25 TO STA. 408+43



PROPOSED TYPICAL SECTION
STA. 405+25 TO STA. 408+43

SUPERELEVATION SECTION (WB)

LEGEND

1. EXISTING BITUMINOUS SURFACE +/-6"
2. EXISTING P.C.C. PAVEMENT +/-9"
3. EXISTING BITUMINOUS SHOULDER
4. EXISTING AGGREGATE SHOULDER
5. EXISTING B-6.24 CURB AND GUTTER
6. EXISTING M-2.12 CURB AND GUTTER
7. EXISTING CORRUGATED MEDIAN
8. EXISTING HMA 12"
9. MEDIAN REMOVAL PARTIAL DEPTH
10. PAVEMENT REMOVAL 12"
11. PORTLAND CEMENT CONCRETE SURFACE REMOVAL VARIABLE DEPTH, 2¼"
12. HMA SURFACE REMOVAL - 2½" (SEE NOTE 2)
13. HMA SURFACE REMOVAL - 2 1/4" (SEE NOTE 2)
14. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70-1¾"
15. POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50-3/4"
16. POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50-1"
17. HMA BINDER COURSE, IL-19.0, N70 - 2 1/4"
18. PROPOSED MEDIAN
19. PROPOSED M-2.12 CURB AND GUTTER
20. PROPOSED HOT MIX ASPHALT SURFACE COURSE, MIX "D", N50-2"
21. PROPOSED HOT MIX ASPHALT BASE COURSE, 10"
22. GRADING AND SHAPING SHOULDERS & AGGREGATE WEDGE SHOULDER, TYPE B
23. TIE BARS ¾" @ 24" CENTERS
23. AGGREGATE BASE COURSE, TYPE B 2"

NOTES

1. SEE DISTRICT ONE DETAIL (BD33) HMA TAPER AT EDGE OF P.C.C. PAVEMENT
2. THE CONTRACTOR SHALL PERFORM THE PAVEMENT PATCHING OPERATIONS PRIOR TO THE HMA SURFACE REMOVAL OPERATION. SEE IDOT DISTRICT 1 DETAIL PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT BD-400-04 (BD-22) FOR ADDITIONAL INFORMATION.

TYP-01

2600 Warrenville Road, Suite 203, Downers Grove, IL 60515 630.785.0110 voice, 630.839.2566 fax www.mps-il.com MILLENNIA PROFESSIONAL SERVICES	DESIGNED - TVN DRAWN - JP CHECKED - TVN DATE - 9/12/2016	REVISED - REVISED - REVISED - REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINLINE TYPICAL SECTIONS
SAUK TRAIL

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	9
		CONTRACT NO. 60X38		

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT

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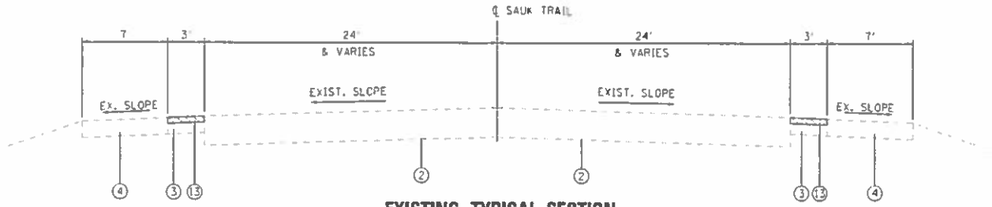
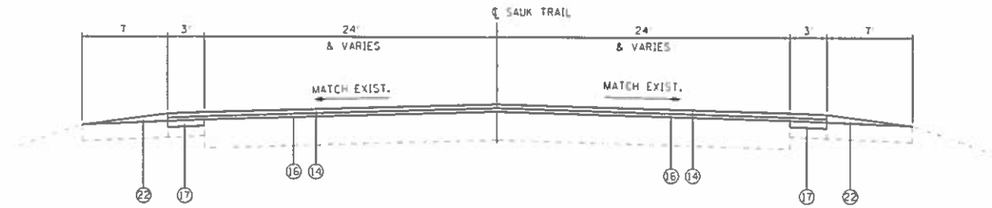


EXISTING TYPICAL SECTION
SAUK TRAIL
STA. 408+43 TO STA. 415+16



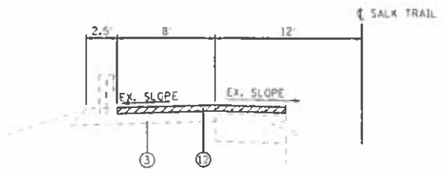
PROPOSED TYPICAL SECTION
SAUK TRAIL
STA. 408+43 TO STA. 415+16

LEGEND

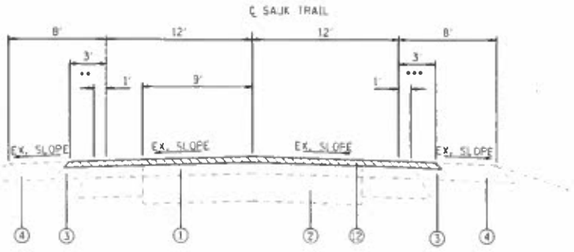
1. EXISTING BITUMINOUS SURFACE +/-6"
2. EXISTING P.C.C. PAVEMENT +/-9"
3. EXISTING BITUMINOUS SHOULDER
4. EXISTING AGGREGATE SHOULDER
5. EXISTING B-6.24 CURB AND GUTTER
6. EXISTING M-2.12 CURB AND GUTTER
7. EXISTING CORRUGATED MEDIAN
8. EXISTING HMA 12"
9. MEDIAN REMOVAL PARTIAL DEPTH
10. PAVEMENT REMOVAL 12"
11. PORTLAND CEMENT CONCRETE SURFACE REMOVAL VARIABLE DEPTH
12. HMA SURFACE REMOVAL - 2½" (SEE NOTE 2)
13. HMA SURFACE REMOVAL - 2 1/4" (SEE NOTE 2)
14. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70-1¾"
15. POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50-3/4"
16. POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50-1"
17. HMA BINDER COURSE, IL-19.0,N70 - 2 1/4"
18. PROPOSED MEDIAN
19. PROPOSED M-2.12 CURB AND GUTTER
20. PROPOSED HOT MIX ASPHALT SURFACE COURSE, MIX "D", N50-2"
21. PROPOSED HOT MIX ASPHALT BASE COURSE, 10"
22. GRADING AND SHAPING SHOULDERS & AGGREGATE WEDGE SHOULDER, TYPE B
23. TIE BARS ¾" ø 24" CENTERS

NOTES

1. SEE DISTRICT ONE DETAIL (BD33) HMA TAPER AT EDGE OF P.C.C. PAVEMENT
2. THE CONTRACTOR SHALL PERFORM THE PAVEMENT PATCHING OPERATIONS PRIOR TO THE HMA SURFACE REMOVAL OPERATION. SEE IDOT DISTRICT 1 DETAIL PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT BD-400-04 (BD-22) FOR ADDITIONAL INFORMATION.



EXISTING 8' SHOULDER SECTION **
STA. 436+32 TO STA. 440+98



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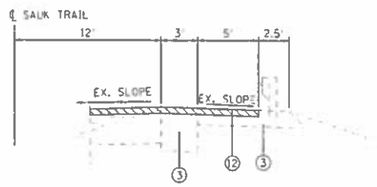
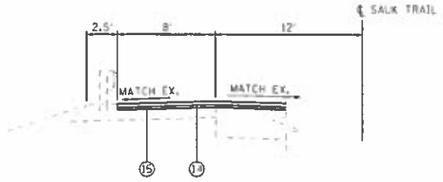
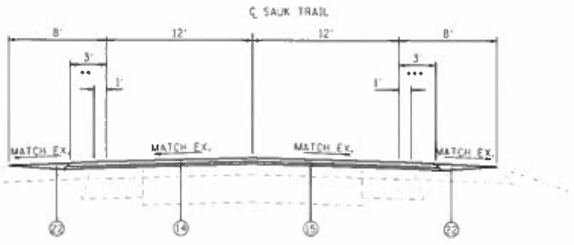
SAUK TRAIL
STA. 415+16 TO STA. 481+59



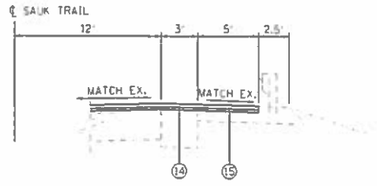
EXISTING 8' SHOULDER SECTION ***
STA. 435+79 TO STA. 440+28



PROPOSED 8' SHOULDER SECTION **
STA. 436+32 TO STA. 440+98



PROPOSED TYPCIAL SECTION
SAUK TRAIL
STA. 415+16 TO STA. 481+59



PROPOSED 8' SHOULDER SECTION ***
STA. 435+79 TO STA. 440+28

TYP-02

2600 Warrenville Road, Suite 303, Downers Grove, IL 60515
630.705.8330 voice, 630.839.2566 fax
www.mps-il.com
MILLENNIA PROFESSIONAL SERVICES

DESIGNED - TVN	REVISED -	
DRAWN - JP	REVISED -	
CHECKED - TVN	REVISED -	
DATE - 9/12/2016	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINLINE TYPICAL SECTIONS
SAUK TRAIL

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	10
		CONTRACT NO. 60X38		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

P:\2014\ME14027_VarVar_Phil\VOI6_Sauk Trail_Updates\CADD\Shts\04-DI6BP22-sht-typical.dgn

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USER NAME = jp
PLOT SCALE = 1.0000 '/ In.
PLOT DATE = 11/3/2016
Millennia Professional Services

SCHEDULE OF QUANTITIES (EARTHWORK)

1	2	3	4	5	6	7
THORNTON-LANSING RD. AT STONY ISLAND AVE.	EARTH EXCAVATION (CU YD)	UNSUITABLE MATERIAL (CU YD)	EMBANKMENT (CU YD)	ADJUSTMENT FOR SHRINKAGE (CU YD)	FURNISHED EXCAVATION (CU YD)	TOP SOIL FURNISH AND PLACE (SQ YD)
SAUK TRAIL	1376	767	178	207	29	400
TOTAL	**1376**	**767**	**178**	**207**	**29**	**400**

COLUMN 1: LOCATION FROM PLANS
COLUMN 2: CUT QUANTITIES AFTER UNSUITABLE MATERIAL IS REMOVED
COLUMN 3: MATERIAL THAT IS DETERMINED TO BE EITHER UNSTABLE OR UNSUITABLE FOR USE IN EMBANKMENT (TOP SOIL EXCAVATED AT 12'' (150 MM) AVERAGE DEPTH)
COLUMN 4: FILL QUANTITIES AFTER UNSUITABLE MATERIAL IS REMOVED
COLUMN 5: EARTH EXCAVATION THAT IS TO BE USED AS FILL MATERIAL IN THE EMBANKMENT, SHRINKAGE FACTOR WAS DETERMINED TO BE 15%
COLUMN 6: COLUMN 5 - COLUMN 4, POSITIVE QUANTITY= FURNISHED EXCAVATION, NEGATIVE QUANTITY= BORROW EXCAVATION
COLUMN 7: TOPSOIL FURNISH AND PLACE= AREA OF SODDING

NOTE: THE TOP 6'' OF TOPSOIL IS TO BE REMOVED AND PAID FOR AS REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SAUK TRAIL AT BURNHAM AVE.
SCHEDULE OF QUANTITIES

SCALE: _____ | SHEET NO. __ OF ___ SHEETS | STA. _____ TO STA. _____

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	11
				CONTRACT NO. 60X38
		ILLINOIS	FED. AID PROJECT	

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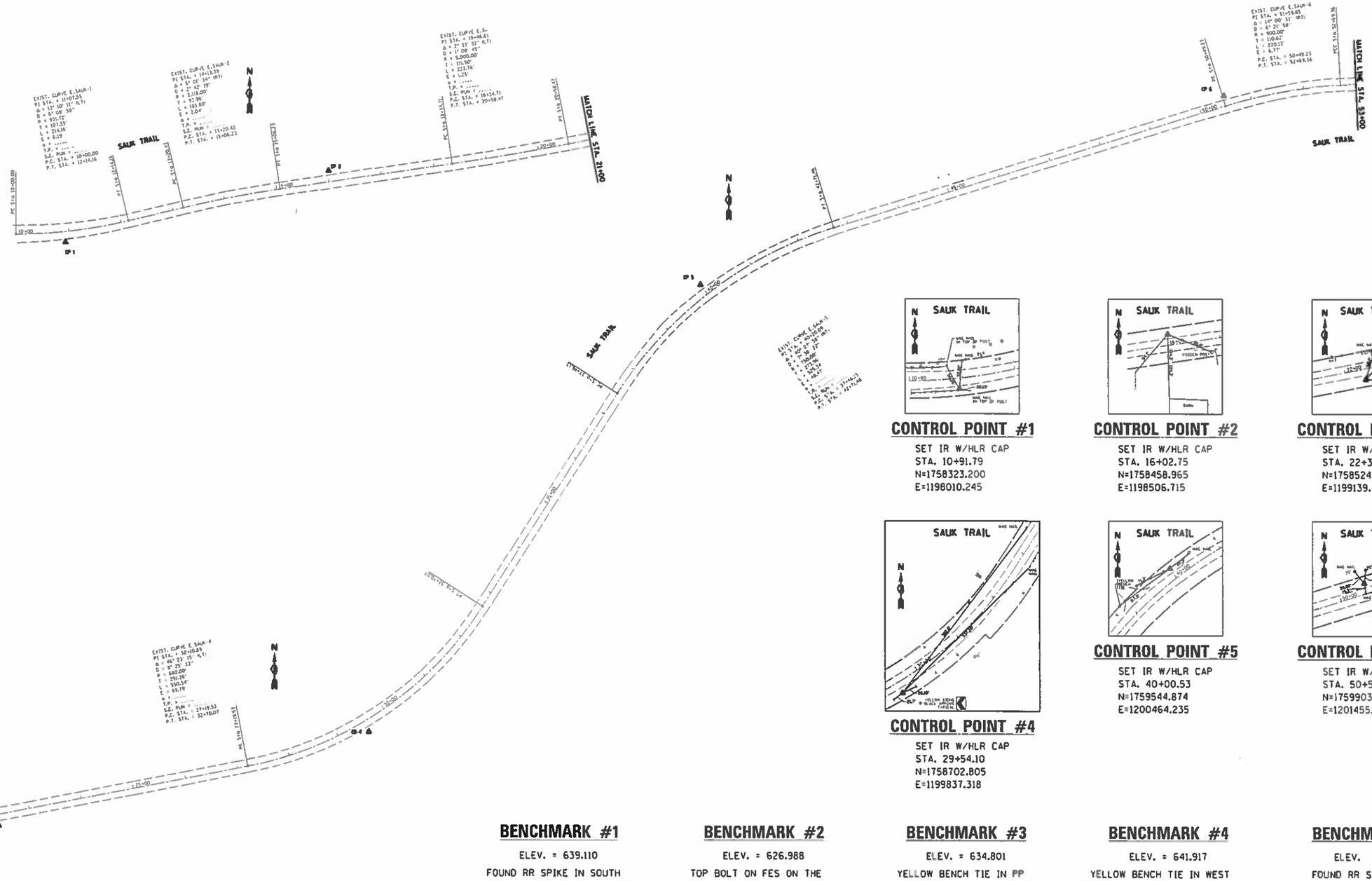

CONTROL POINT #1

SET IR W/HLR CAP
STA. 10+91.79
N=1758323.200
E=1198010.245

CONTROL POINT #2

SET IR W/HLR CAP
STA. 16+02.75
N=1758458.965
E=1198506.715

CONTROL POINT #3

SET IR W/HLR CAP
STA. 22+37.57
N=1758524.891
E=1199139.407

CONTROL POINT #4

SET IR W/HLR CAP
STA. 29+54.10
N=1758702.805
E=1199837.318

CONTROL POINT #5

SET IR W/HLR CAP
STA. 40+00.53
N=1759544.874
E=1200464.235

CONTROL POINT #6

SET IR W/HLR CAP
STA. 50+51.97
N=1759903.452
E=1201455.824

BENCHMARK #1

ELEV. = 639.110

FOUND RR SPIKE IN SOUTH FACE OF PP. NORTH SIDE OF SAUK TRAIL. EAST OF BIT. FIELD ENTRANCE. ±400' WEST OF ENTTRANCE TO #2730 SAUK TRAIL

BENCHMARK #2

ELEV. = 626.988

TOP BOLT ON FES ON THE SOUTH OF SAUK TRAIL ±50' WEST OF THE GUARDRAIL END AND ±800' WEST OF THE MOOSE LODGE ENTRANCE.

BENCHMARK #3

ELEV. = 634.801

YELLOW BENCH TIE IN PP W/LIGHT S.W. SIDE OF ENTRANCE TO MOOSE LODGE

BENCHMARK #4

ELEV. = 641.917

YELLOW BENCH TIE IN WEST FACE OF PP S.E. CORNER OF SAUK TRAIL AND BURNHAM AVE.

BENCHMARK #5

ELEV. 635.235

FOUND RR SPIKE IN PP AT THE S.W. CORNER OF SUNSET LANE AND SAUK TRAIL.

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Default	PLOT DATE = 10/26/2015	DATE =	REVISED =

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ALIGNMENTS, TIES AND BENCHMARKS
SAUK TRAIL AT BURNHAM AVENUE

SCALE: 1"= 100' | SHEET ___ OF ___ SHEETS | STA. _____ TO STA. _____

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49-N	COOK	45	12
		CONTRACT NO. 60X38		
		ILLINOIS FED. AID PROJECT		

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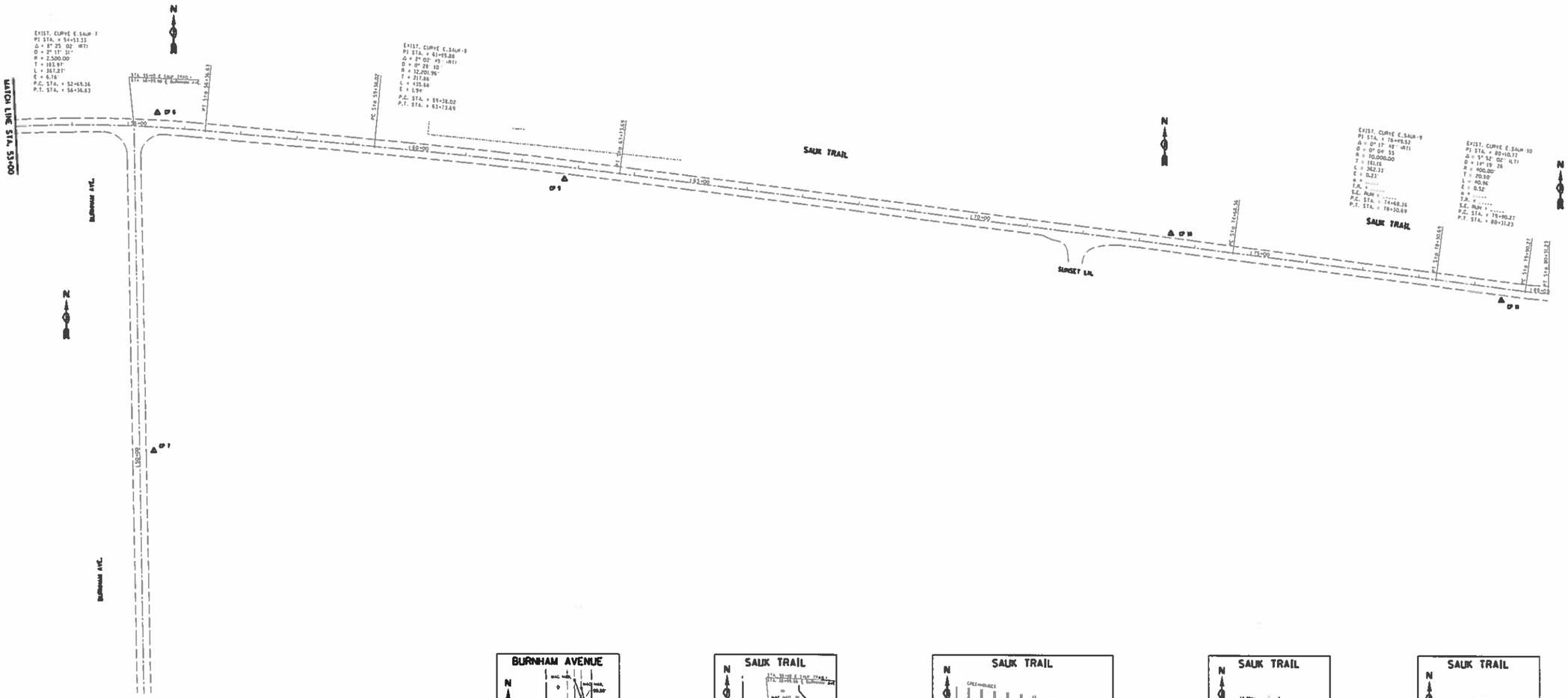
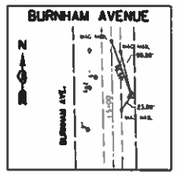
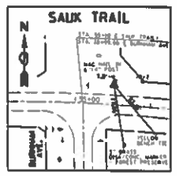




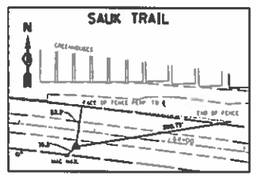
CONTROL POINT #7

SET IR W/HLR CAP
STA. 5+27.75
N=1759354.099
E=1201950.766



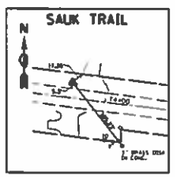
CONTROL POINT #8

SET IR W/HLR CAP
STA. 55+50.57
N=1759949.871
E=1201957.456



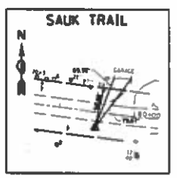
CONTROL POINT #9

SET IR W/HLR CAP
STA. 62+78.20
N=1759834.824
E=1202677.915



CONTROL POINT #10

SET IR W/HLR CAP
STA. 73+54.43
N=1759742.174
E=1203750.820



CONTROL POINT #11

SET IR W/HLR CAP
STA. 79+50.67
N=1759625.414
E=1204336.945

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Default	PLOT DATE = 10/26/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ALIGNMENTS, TIES AND BENCHMARKS
SAUK TRAIL AT BURNHAM AVENUE

SCALE: NONE | SHEET ___ OF ___ SHEETS | STA. _____ TO STA. _____

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49-N	COOK	45	13
		CONTRACT NO. 60X38		
		ILLINOIS FED. AID PROJECT		

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EXIST. CURVE E_SAUK-6
PI STA. = 51+59.85
Δ = 14° 00' 51" (RT)
D = 6° 21' 58"
R = 900.00'
T = 110.62'
L = 220.13'
E = 6.77'
P.C. STA. = 50+49.23
P.T. STA. = 52+69.36

EXIST. CURVE E_SAUK-7
PI STA. = 54+53.33
Δ = 8° 25' 02" (RT)
D = 2° 17' 31"
R = 2,500.00'
T = 183.97'
L = 367.27'
E = 6.76'
P.C. STA. = 52+69.36
P.T. STA. = 56+36.63

EXIST. CURVE E_SAUK-8
PI STA. = 61+55.88
Δ = 2° 02' 45" (RT)
D = 0° 28' 10"
R = 12,201.96'
T = 217.86
L = 435.66
E = 1.94'
P.C. STA. = 59+38.02
P.T. STA. = 63+73.69

SMITS FARM SIGN W/BOULDERS (TO BE REMOVED AND PUT BACK)
WETLAND (PROTECT)
PCC Sta 50+49.23
PCC Sta 52+69.36
PC Sta 59+38.02
PT Sta 56+36.63
PT Sta 63+73.69
STA. 55+10 ℄ SAUK TRAIL = STA. 10+99.90 ℄ BURNHAM AVE.
STA. 55+79 43' LT PROP. R.O.W.
STA. 57+43 48' LT
STA. 57+43 43' LT PROP. T.E.
STA. 58+76 48' LT
STA. 59+68 43' LT
PRIV. BLDG.
RET. WALL
GRAVEL
WHITE PICKET FENCE (PROTECT)
GREENHOUSES
ELEC
CM
C.E.
HMA SHLDR.
HMA P.E.
EXISTING 18" OAK TREE (PROT W/ TEMP. FENCE)
PP TO BE RELOCATED BY OTHERS
8" HACKBERRY (REMOVE)
EXISTING TREE W/BOULDERS AROUND (PROTECT W/TEMP. FENCE)
SAUK TRAIL
BURNHAM AVE.
WETLAND (PROTECT)
N

8' HMA SHOULDER (TYP.)
10' TAPER TO MEET EXISTING SHOULDER
2524' R
910' R
883' R
2041' R
40' R
36' R
8' HMA SHOULDERS
300' TAPER 50it
+55.1 HMA PE
+55.7 HMA PE
+38.0 HMA EE
BEGIN WIDENING PROJECT STA. 51+55
LIMIT OF IMPROVEMENT STA. 10+38.3
END WIDENING PROJECT STA. 61+60

① RESURFACING & WIDENING
POLYMERIZED LEVELING BINDER (MM) IL-4.75, N50, ¾"
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, IL 9.5 mm 1¾"
HMA BASE COURSE, 6½"

EXIST. ℄ PROFILE FOR SAUK TRAIL
PROP. ℄ PROFILE FOR SAUK TRAIL
VPI STA. 53+20.00 ELEVATION 640.45
VPI STA. 55+10.00 ELEVATION 640.70
VPI STA. 58+50.00 ELEVATION 641.65
℄ BURNHAM AVE. STA. 55+10
+0.94% +0.43% +0.13% +0.27% +0.27% -1.48%
638.90 639.28 640.61 641.15 639.03 637.02
+95.00 EL. +45.00 EL. +75.00 EL. +25.00 EL.
SSD = 1464'
ex = -0.25'
K = 310
250.00' V.C.
SSD = 789'
ex = -0.77'
K = 199
350.00' V.C.

645 640 635 630 625

637.02	637.32	637.63	638.04	638.47	638.86	639.26 639.32	639.65 639.74	640.04 640.08	640.31 640.34	640.54 640.52	640.60 640.62	640.63 640.69	640.73 640.81	640.95 640.95	641.04 641.08	641.07 641.20	641.01 641.21	640.90 641.10	640.76 640.86	640.47 640.49	640.00 640.00	639.38 639.38	638.66 638.66	637.84 637.91	637.16 637.17	636.47	636.07	635.66	635.71	635.76
49+00	50+00	51+00	52+00	53+00	54+00	55+00	56+00	57+00	58+00	59+00	60+00	61+00	62+00	63+00	64+00														

PLAN: SURVEYED, PLOTTED, DRAWN, CHECKED, QUANTITIES CHECKED, STRUCTURE NOTATIONS CHKD, NOTE BOOK, NO., FLOOD FILE NAME, BY, DATE
PROFILE: SURVEYED, PLOTTED, DRAWN, CHECKED, QUANTITIES CHECKED, STRUCTURE NOTATIONS CHKD, NOTE BOOK, NO., BY, DATE

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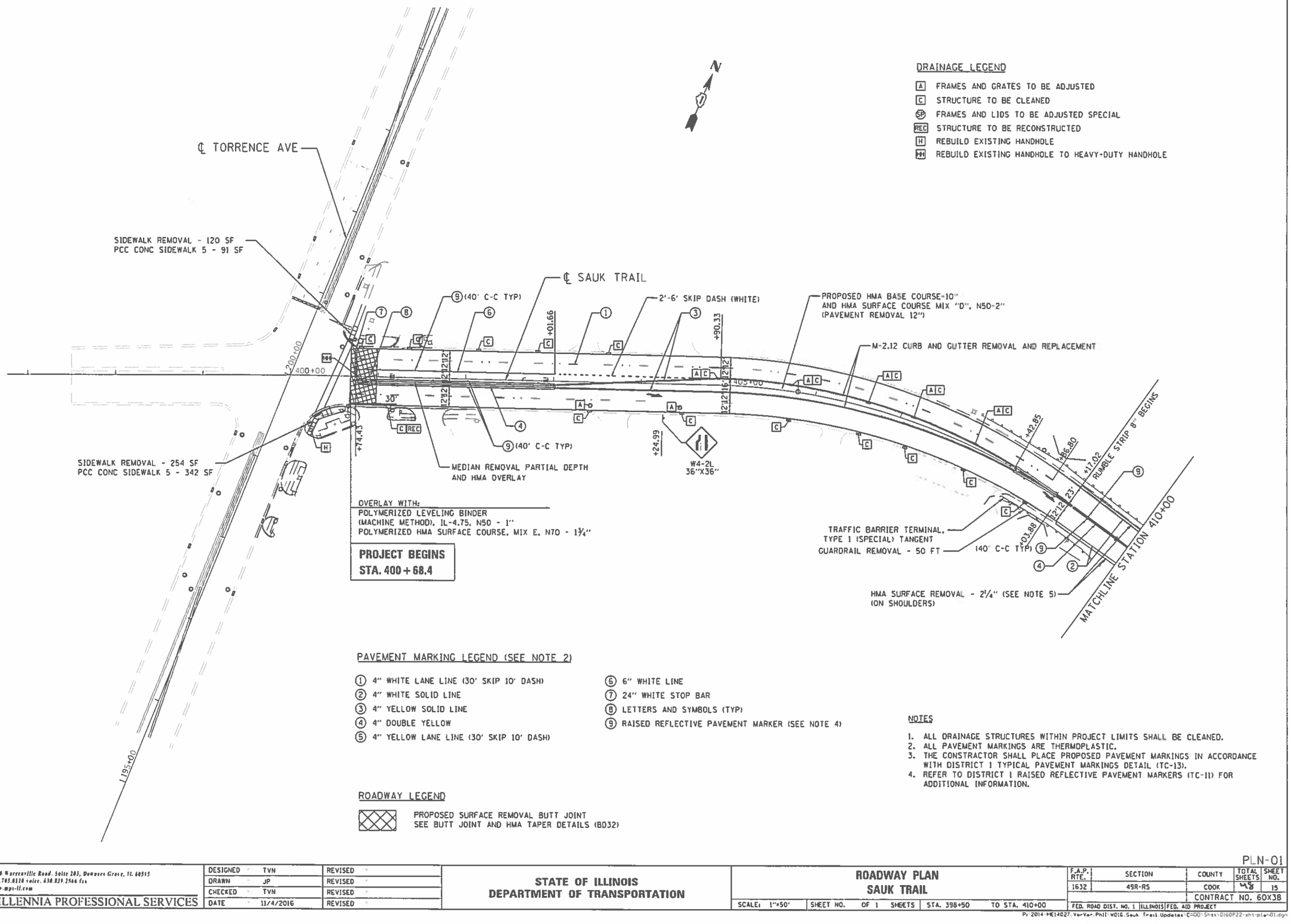
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -
	PLOT DATE = 11/22/2016	DATE	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN & PROFILE
SAUK TRAIL AT BURNHAM AVE.
SCALE: 1"=50' | SHEET OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-R5	COOK	48	14
		CONTRACT NO. 60X38		
	ILLINOIS FED. AID PROJECT			



DRAINAGE LEGEND

- A FRAMES AND GRATES TO BE ADJUSTED
- C STRUCTURE TO BE CLEANED
- SP FRAMES AND LIDS TO BE ADJUSTED SPECIAL
- REC STRUCTURE TO BE RECONSTRUCTED
- H REBUILD EXISTING HANDHOLE
- HH REBUILD EXISTING HANDHOLE TO HEAVY-DUTY HANDHOLE

PAVEMENT MARKING LEGEND (SEE NOTE 2)

1. 4" WHITE LANE LINE (30' SKIP 10' DASH)
2. 4" WHITE SOLID LINE
3. 4" YELLOW SOLID LINE
4. 4" DOUBLE YELLOW
5. 4" YELLOW LANE LINE (30' SKIP 10' DASH)
6. 6" WHITE LINE
7. 24" WHITE STOP BAR
8. LETTERS AND SYMBOLS (TYP)
9. RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 4)

ROADWAY LEGEND

PROPOSED SURFACE REMOVAL BUTT JOINT
SEE BUTT JOINT AND HMA TAPER DETAILS (BD32)

NOTES

1. ALL DRAINAGE STRUCTURES WITHIN PROJECT LIMITS SHALL BE CLEANED.
2. ALL PAVEMENT MARKINGS ARE THERMOPLASTIC.
3. THE CONSTRACTOR SHALL PLACE PROPOSED PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT 1 TYPICAL PAVEMENT MARKINGS DETAIL (TC-13).
4. REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKERS (TC-11) FOR ADDITIONAL INFORMATION.

PLN-01

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USER NAME = Millennia Professional Services

2600 Warrenville Road, Suite 203, Downers Grove, IL 60515
630.705.8118 voice, 630.829.2566 fax
www.mps-il.com
MILLENNIA PROFESSIONAL SERVICES

DESIGNED	TVN	REVISED
DRAWN	JP	REVISED
CHECKED	TVN	REVISED
DATE	11/4/2016	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN
SAUK TRAIL

SCALE: 1"=50' | SHEET NO. OF 1 SHEETS | STA. 398+50 TO STA. 410+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	43	15

CONTRACT NO. 60X38

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT

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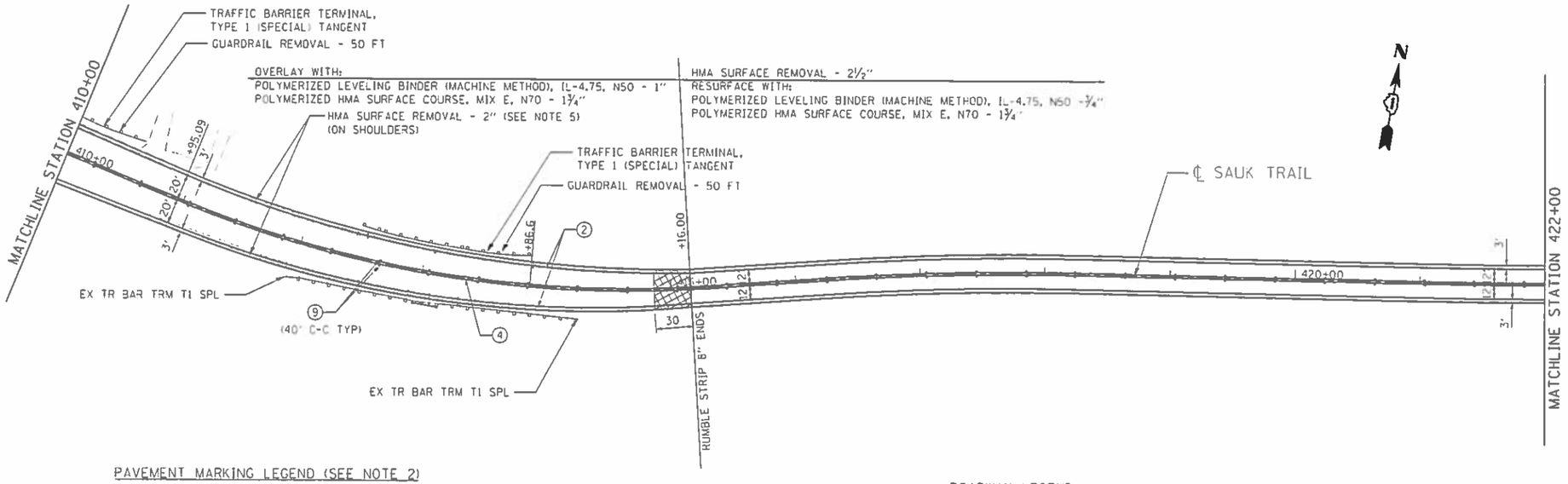
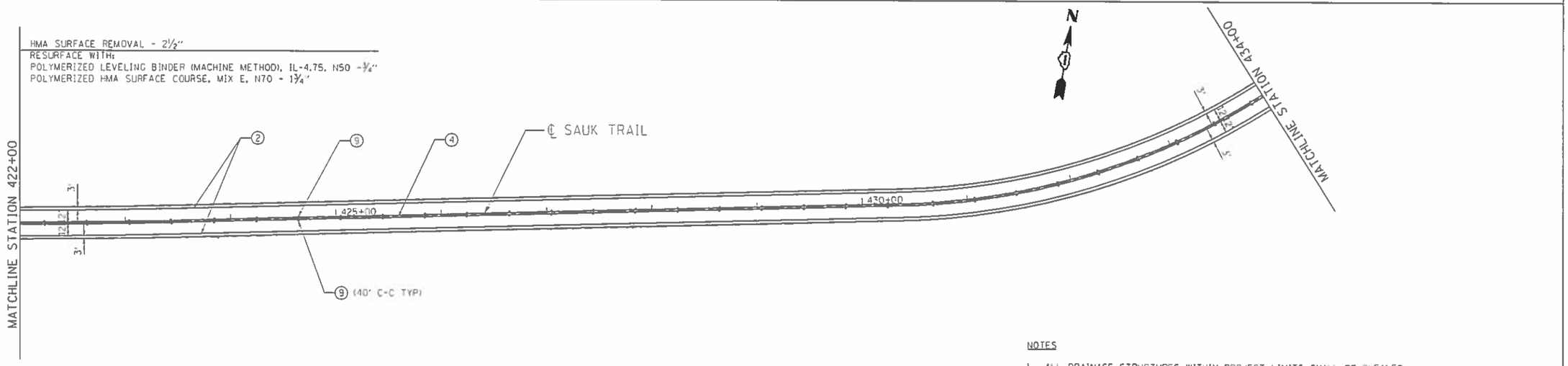


PAVEMENT MARKING LEGEND (SEE NOTE 2)

1. 4" WHITE LANE LINE (30' SKIP 10' DASH)
2. 4" WHITE SOLID LINE
3. 4" YELLOW SOLID LINE
4. 4" DOUBLE YELLOW
5. 4" YELLOW LANE LINE (30' SKIP 10' DASH)
6. 6" WHITE LINE
7. 24" WHITE STOP BAR
8. LETTERS AND SYMBOLS (TYP)
9. RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 4)

ROADWAY LEGEND

PROPOSED SURFACE REMOVAL BUTT JOINT
SEE BUTT JOINT AND HMA TAPER DETAILS (BD32)



NOTES

1. ALL DRAINAGE STRUCTURES WITHIN PROJECT LIMITS SHALL BE CLEANED.
2. ALL PAVEMENT MARKINGS ARE THERMOPLASTIC.
3. THE CONSTRACTOR SHALL PLACE PROPOSED PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT 1 TYPICAL PAVEMENT MARKINGS DETAIL (TC-13).
4. REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKERS (TC-11) FOR ADDITIONAL INFORMATION.
5. REPLACE WITH HMA BINDER COURSE, IL-19, N70 -2¼",
POLYMERIZED LEVELING BINDER (MM), N50 - 1",
AND HOT MIX ASPHALT SURFACE COURSE, MIX E, N70 - 1¾"

PLN-02

2660 Warrenville Road, Suite 203, Downers Grove, IL 60515
630.705.0110 voice, 630.829.2566 fax
www.mps-il.com
MILLENNIA PROFESSIONAL SERVICES

DESIGNED - TVN	REVISED -	
DRAWN - JP	REVISED -	
CHECKED - TVN	REVISED -	
DATE - 9/12/2016	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED PLAN
SAUK TRAIL

SCALE: 1"=50' | SHEET NO. OF 2 SHEETS | STA. 410+00 TO STA. 434+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	16
		CONTRACT NO. 60X38		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

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Millennia Professional Services, Inc.

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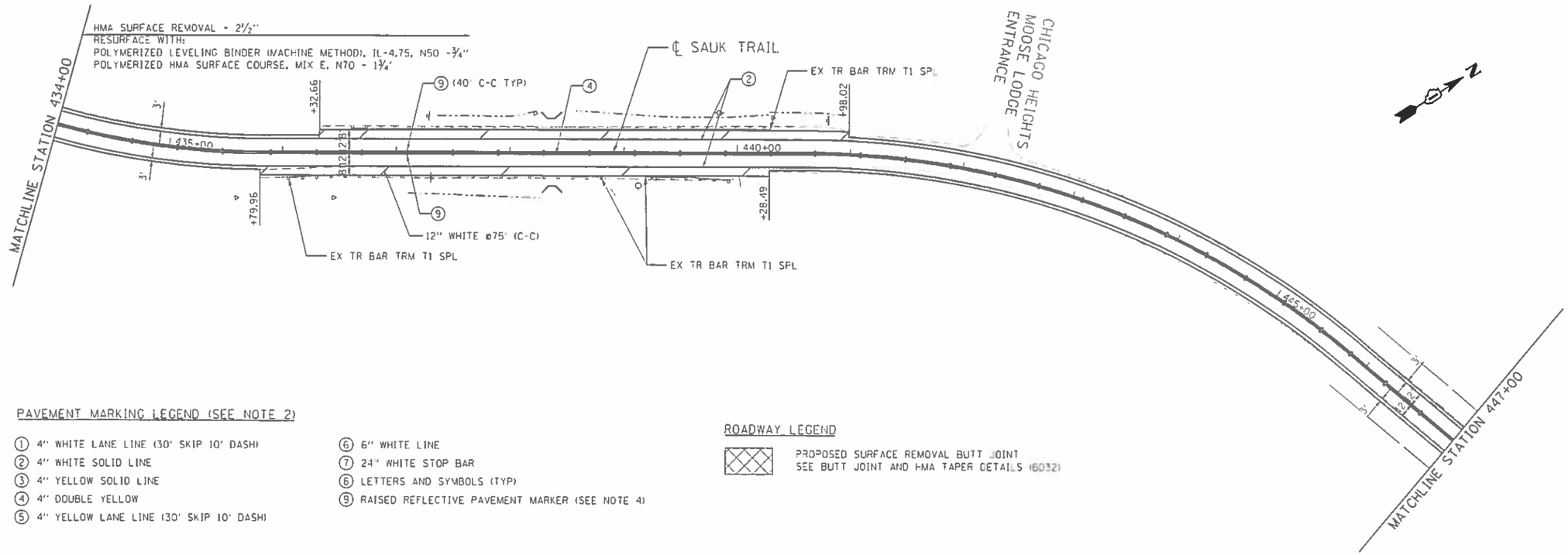
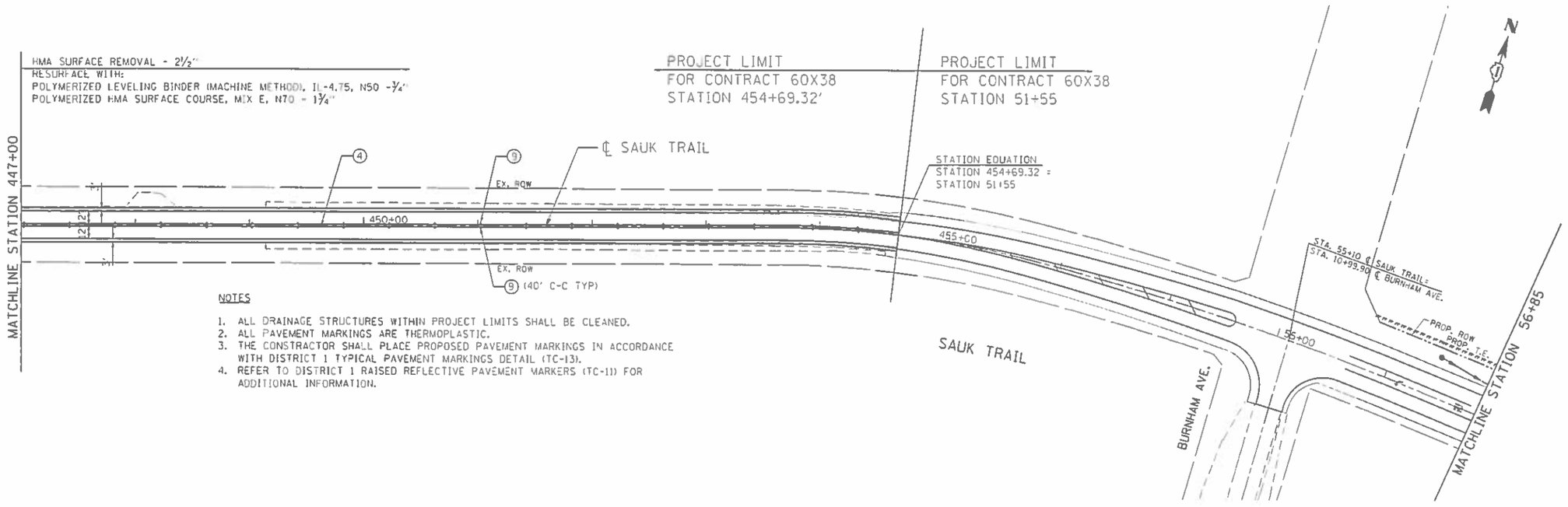


PAVEMENT MARKING LEGEND (SEE NOTE 2)

1. 4" WHITE LANE LINE (30' SKIP 10' DASH)
2. 4" WHITE SOLID LINE
3. 4" YELLOW SOLID LINE
4. 4" DOUBLE YELLOW
5. 4" YELLOW LANE LINE (30' SKIP 10' DASH)
6. 6" WHITE LINE
7. 24" WHITE STOP BAR
8. LETTERS AND SYMBOLS (TYP)
9. RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 4)

ROADWAY LEGEND

PROPOSED SURFACE REMOVAL BUTT JOINT
SEE BUTT JOINT AND HMA TAPER DETAILS (6032)



NOTES

1. ALL DRAINAGE STRUCTURES WITHIN PROJECT LIMITS SHALL BE CLEANED.
2. ALL PAVEMENT MARKINGS ARE THERMOPLASTIC.
3. THE CONSTRACTOR SHALL PLACE PROPOSED PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT 1 TYPICAL PAVEMENT MARKINGS DETAIL (TC-13).
4. REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKERS (TC-11) FOR ADDITIONAL INFORMATION.

PLN-03

2690 Warrenville Road, Suite 203, Downers Grove, IL 60515
630.705.0130 voice, 630.839.2566 fax
www.mps-il.com
MILLENNIA PROFESSIONAL SERVICES

DESIGNED	- TVN	REVISED	-
DRAWN	- JP	REVISED	-
CHECKED	- TVN	REVISED	-
DATE	- 9/12/2016	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN
SAUK TRAIL

SCALE: 1"=50' | SHEET NO. 3 OF 4 SHEETS | STA. 434+00 TO STA. 460+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	17
		CONTRACT NO. 60X38		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

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PLOT MODE = Millennia Professional Services

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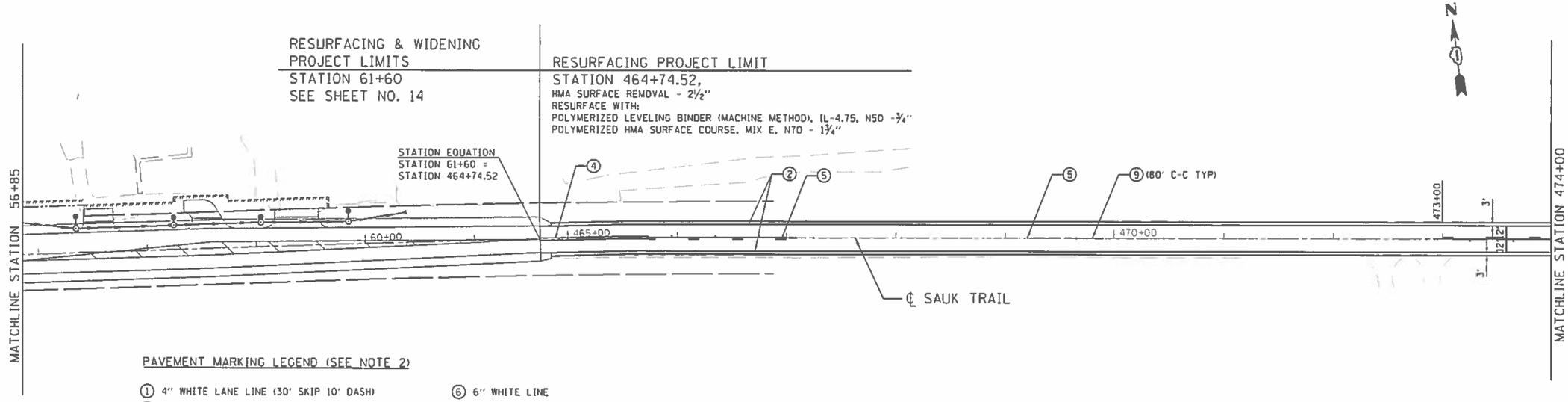
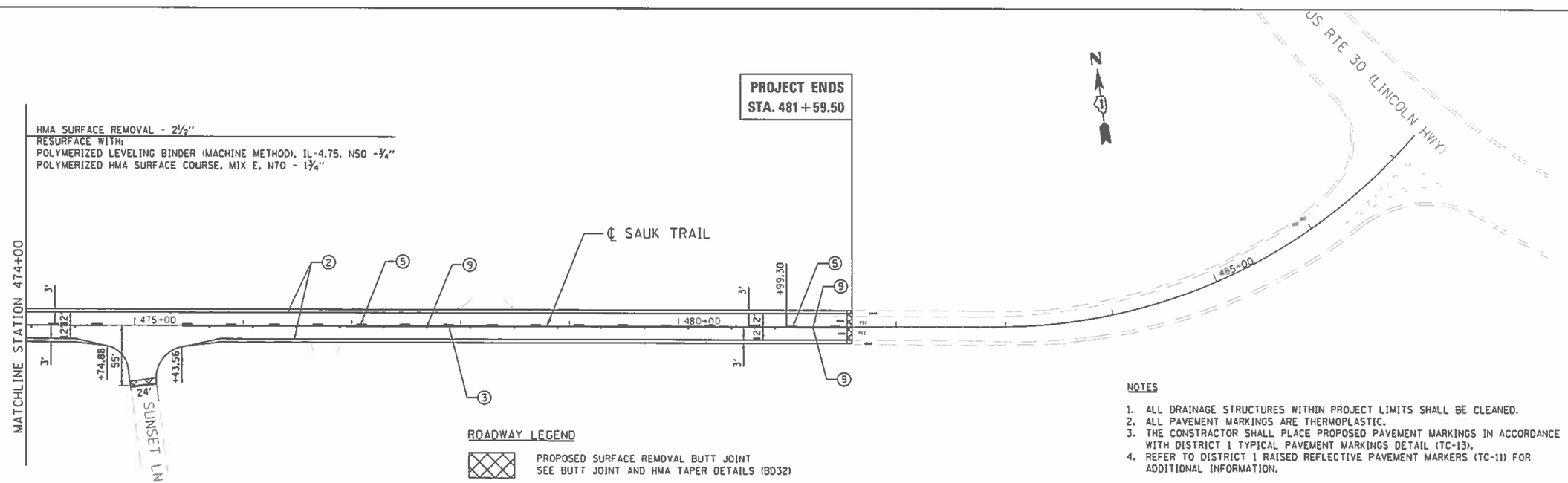


PAVEMENT MARKING LEGEND (SEE NOTE 2)

1. 4" WHITE LANE LINE (30' SKIP 10' DASH)
2. 4" WHITE SOLID LINE
3. 4" YELLOW SOLID LINE
4. 4" DOUBLE YELLOW
5. 4" YELLOW LANE LINE (30' SKIP 10' DASH)
6. 6" WHITE LINE
7. 24" WHITE STOP BAR
8. LETTERS AND SYMBOLS (TYP)
9. RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 4)



ROADWAY LEGEND

PROPOSED SURFACE REMOVAL BUTT JOINT
SEE BUTT JOINT AND HMA TAPER DETAILS (BD32)

NOTES

1. ALL DRAINAGE STRUCTURES WITHIN PROJECT LIMITS SHALL BE CLEANED.
2. ALL PAVEMENT MARKINGS ARE THERMOPLASTIC.
3. THE CONSTRACTOR SHALL PLACE PROPOSED PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT 1 TYPICAL PAVEMENT MARKINGS DETAIL (TC-13).
4. REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKERS (TC-11) FOR ADDITIONAL INFORMATION.

PLN-04

FILE NAME = P:\2014\ME14027_VarVar_Phil\WD16_Sauk Trail_Updates\CADD\Shts\D16DP22-sht-plan04.dgn
PLOT SCALE = 100.0000 ' / in.
USER NAME = Millennia Professional Services

2680 Warrenville Road, Suite 203, Downers Grove, IL 60515
630.705.8118 voice, 630.839.1566 fax
www.mps-il.com
MILLENNIA PROFESSIONAL SERVICES

DESIGNED - TVN		REVISED -
DRAWN - JP		REVISED -
CHECKED - TVN		REVISED -
DATE - 11/4/2016		REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN
SAUK TRAIL

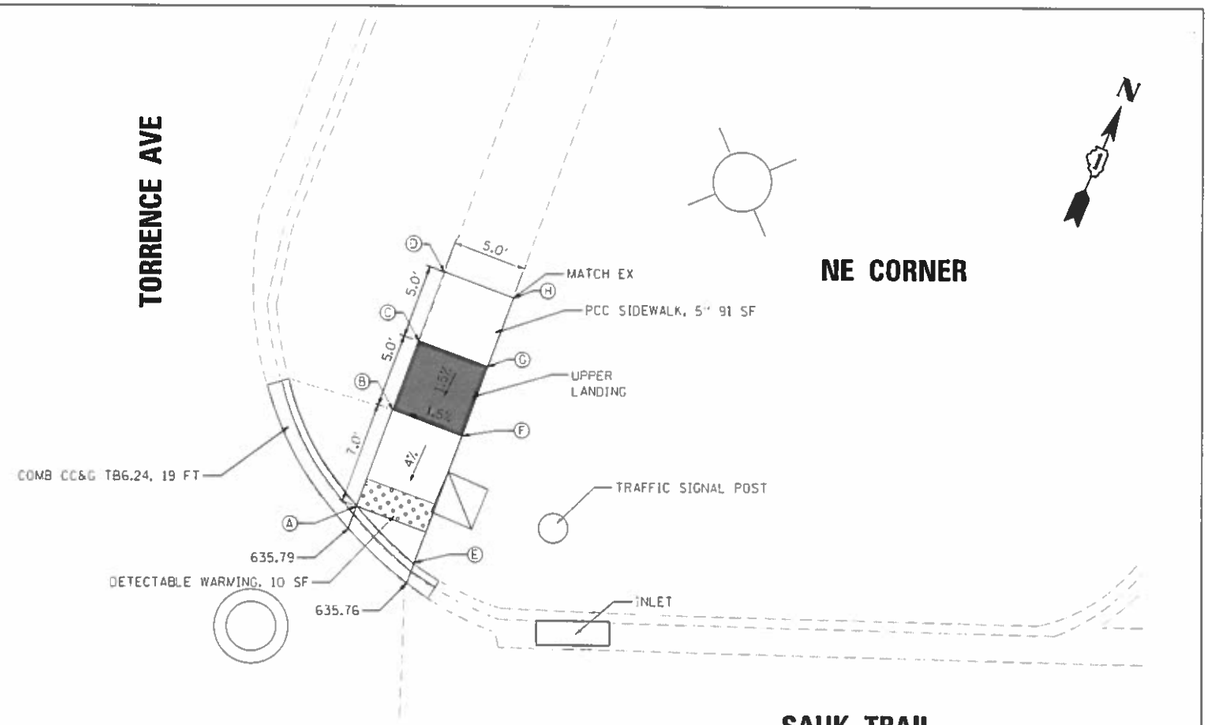
SCALE: 1"=50' | SHEET NO. 4 OF 4 SHEETS | STA. 460+00 TO STA. 481+58.61

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	18
		CONTRACT NO. 60X38		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

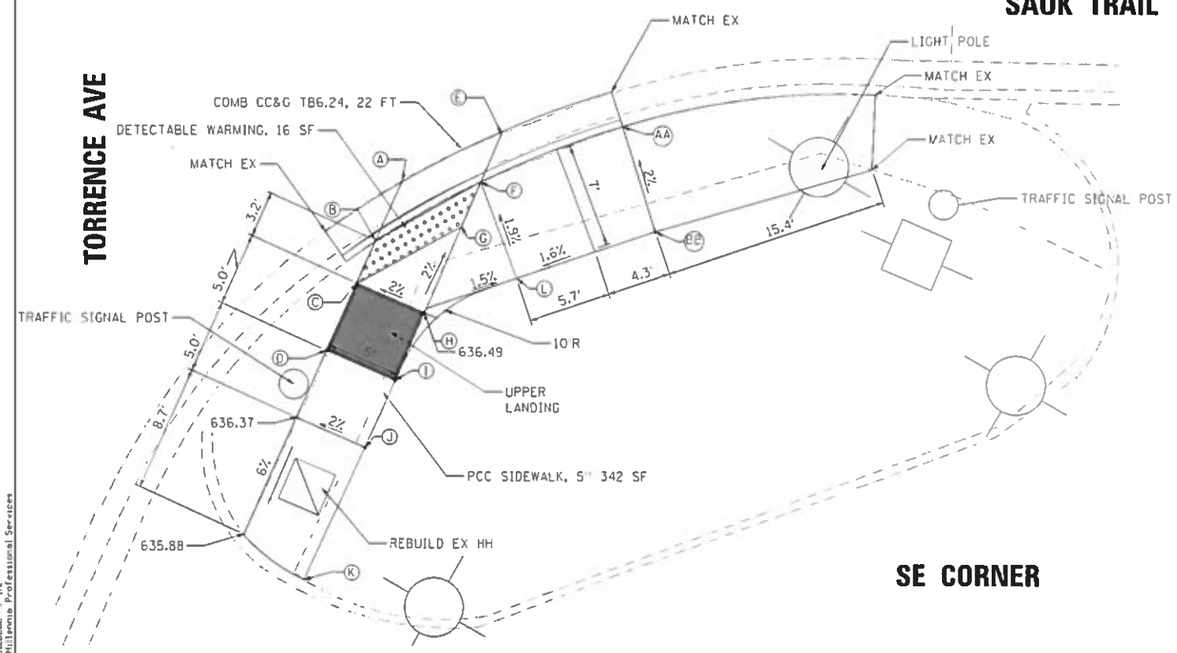
P:\2014\ME14027_VarVar_Phil\WD16_Sauk Trail_Updates\CADD\Shts\D16DP22-sht-plan04.dgn

NE CORNER

POINT	STA	OFFSET	ELEV
A			635.89
B	400+67.78	48.06 LT	636.17
C			636.22
D			ME
E			635.86
F	400+72.50	46.42 LT	636.23
G	400+74.15	51.14 LT	636.30
H			ME



SAUK TRAIL



POINT	STA	OFFSET	ELEV
A			636.13
B			636.30
C			636.36
D	400+25.84	52.78 RT	636.46
E			636.10
F			636.27
G			636.34
H	400+32.47	50.08 RT	636.46
I	400+30.58	54.72 RT	636.46
J	400+28.62	59.32 RT	636.47
K			635.88
L	400+38.72	47.60 RT	636.40
AA			636.42
BB	400+48.13	44.21 RT	636.56

1600 Warrenville Road, Suite 283, Downers Grove, IL 60515
630.705.0150 voice, 630.839.2566 fax
www.mps-il.com
MILLENNIA PROFESSIONAL SERVICES

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DRAWN - JP		REVISED -
CHECKED - TVN		REVISED -
DATE - 9/12/2016		REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA SIDEWALK DETAILS
SAUK TRAIL

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	[illegible]	[illegible]
		CONTRACT NO. 60X38		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

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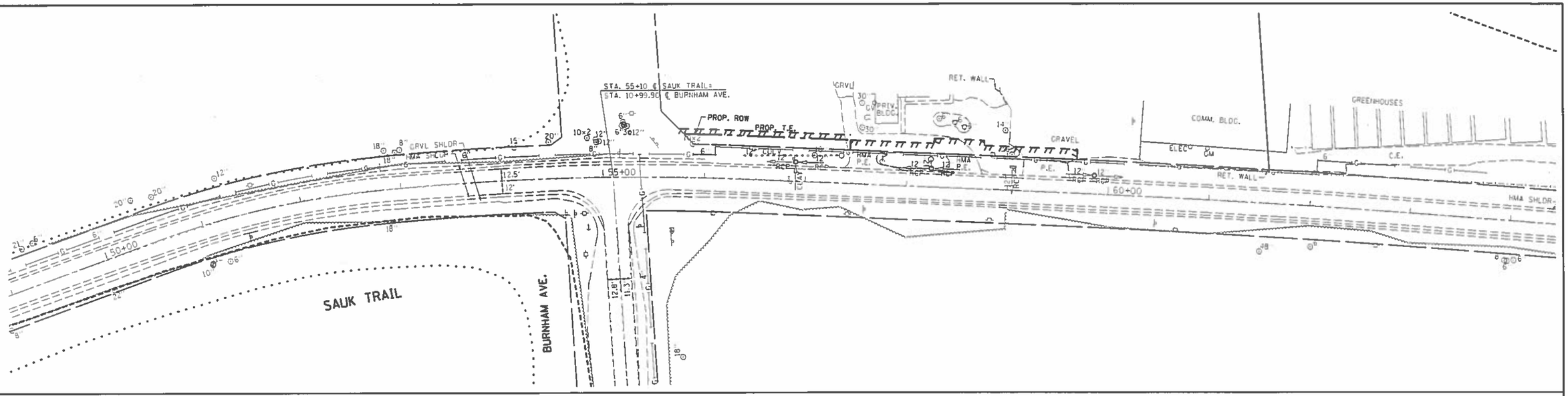
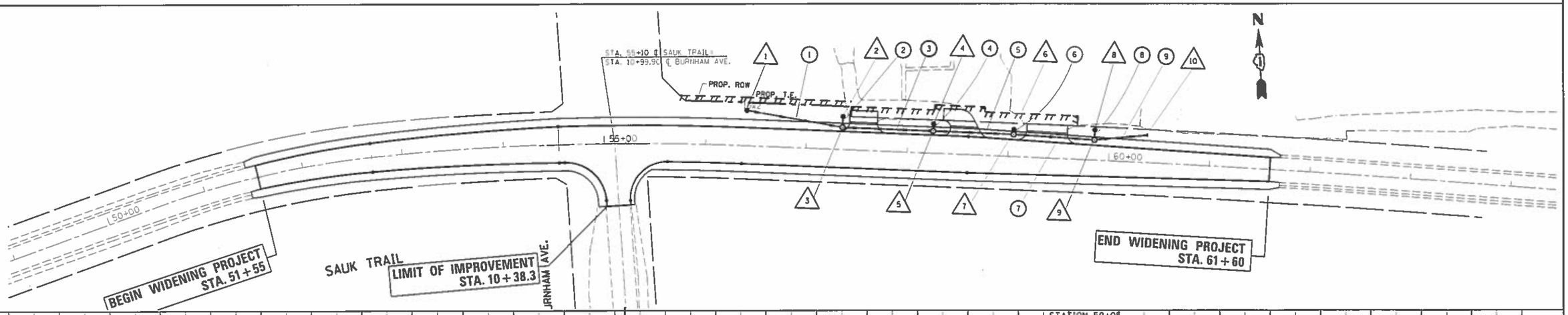
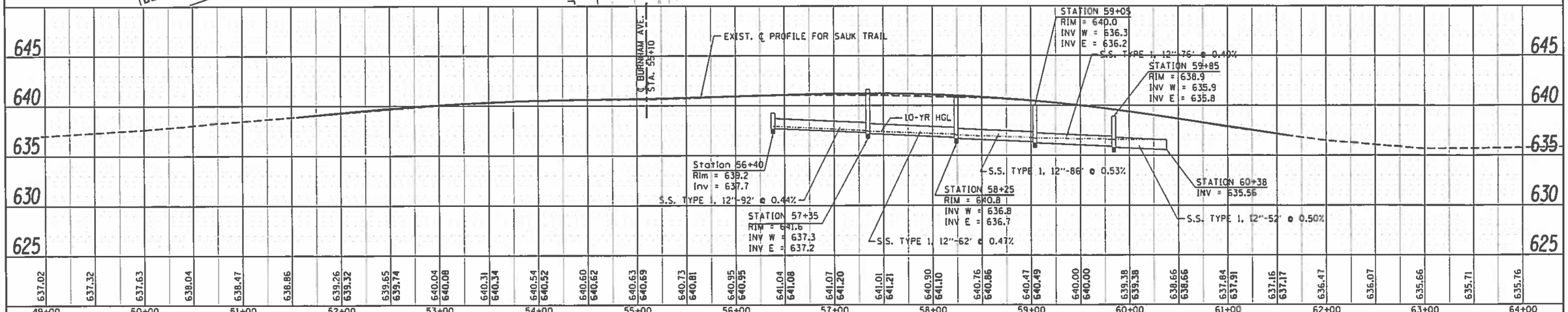
USER NAME = tvn
PLOT SCALE = 1.0000 '/ In.
PLOT DATE = 9/12/2016
Millennia Professional Services

PLAN
SURVEYED
PLOTTED
ALIGNMENT CHECKED
GRADES CHECKED
NOTE BOOK
NO.
DATE
BY



PROFILE
SURVEYED
PLOTTED
GRADES CHECKED
NOTE BOOK
NO.
DATE
BY





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pw:\\IL084EBIDINTEC.illinois.gov:PWIDOT\Documents\IDOT Offices-District 1-Projects-P110012\sht\Drain.dgn		DRAWN - CADsheets\P110012\sht\Drain.dgn	REVISED -
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -
	PLOT DATE = 10/27/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRAINAGE PLAN & PROFILE
SAUK TRAIL AT BURNHAM AVE.

SCALE: 1"=50' | SHEET OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	20
		CONTRACT NO. 60X38		
		ILLINOIS FED. AID PROJECT		

DRAINAGE STRUCTURES TABLE

NO.	STATION	OFFSET	STRUCTURE TYPE MH	STRUCTURE TYPE CB	STRUCTURE TYPE INL	DIA.	FRAME	TOP OF FRAME	INVERT (W)	INVERT (E)	INVERT (S)	INVERT (N)
1	56+40	35' LT.	_____	TA	_____	4'	T8	639.2		637.2		
2	57+35	33' LT.	_____	TA	__	4'	T8	639.7			637.36	
3	57+35	22' LT.	IA		_____	4'	T8	641.6	637.3	637.2		637.3
4	58+25	29' LT.	_____	TA	_____	4'	T8	639.9			636.84	
5	58+25	22' LT.	IA		_____	4'	T8	640.8	636.8	636.7	_____	636.8
6	59+05	27' LT.	_____	TA	_____	4'	T8	639.5			636.33	__
7	59+05	22' LT.	IA		_____	4'	T8	640.0	636.3	636.2	___	636.3
8	59+85	30' LT.	_____	TA	__	4'	T8	636.6	__	_____	635.95	
9	59+85	20' LT.	IA		_____	4'	T8	638.9	635.9	635.8	_____	635.9
10	60+38	27' LT.	FLARED END SECTION			12"			635.56			_

PIPE TABLE

NO.	STATION - STATION	TYPE	DIA.	LIN. FT.	SLOPE	
1	56+40, 35' LT. - 57+35, 22' LT.	1	12"	92	0.44%	___
2	57+35 -	1	12"	11	0.50%	___
3	57+35, 22' LT. - 58+25, 22' LT.	1	12"	86	0.47%	
4	58+25 -	1	12"	7	0.50%	___
5	58+25, 22' LT - 59+05, 22' LT	1	12"	76	0.53%	___
6	59+05 -	1	12"	5	0.50%	___
7	59+05, 22' LT - 59+85, 20' LT	1	12"	76	0.40%	
8	58+85 -	1	12"	10	0.50%	
9	59+85, 20' LT. - 60+38, 27' LT.	1	12"	52	0.50%	
	-					___
	-					___
	-					___
___	_____ - _____	_____	_____	_____	_____	___
___	_____ - _____	_____	_____	_____	_____	___
___	_____ - _____	_____	_____	_____	_____	___
___	_____ - _____	_____	_____	_____	_____	___
___	_____ - _____	_____	_____	_____	_____	___
___	_____ - _____	_____	_____	_____	_____	___
___	_____ - _____	_____	_____	_____	_____	___
___	_____ - _____	_____	_____	_____	_____	___

WM=WATER MAIN REQUIREMENTS

NOTES

1. STATION AND OFFSETS ARE BASED UPON EXISTING CENTERLINE

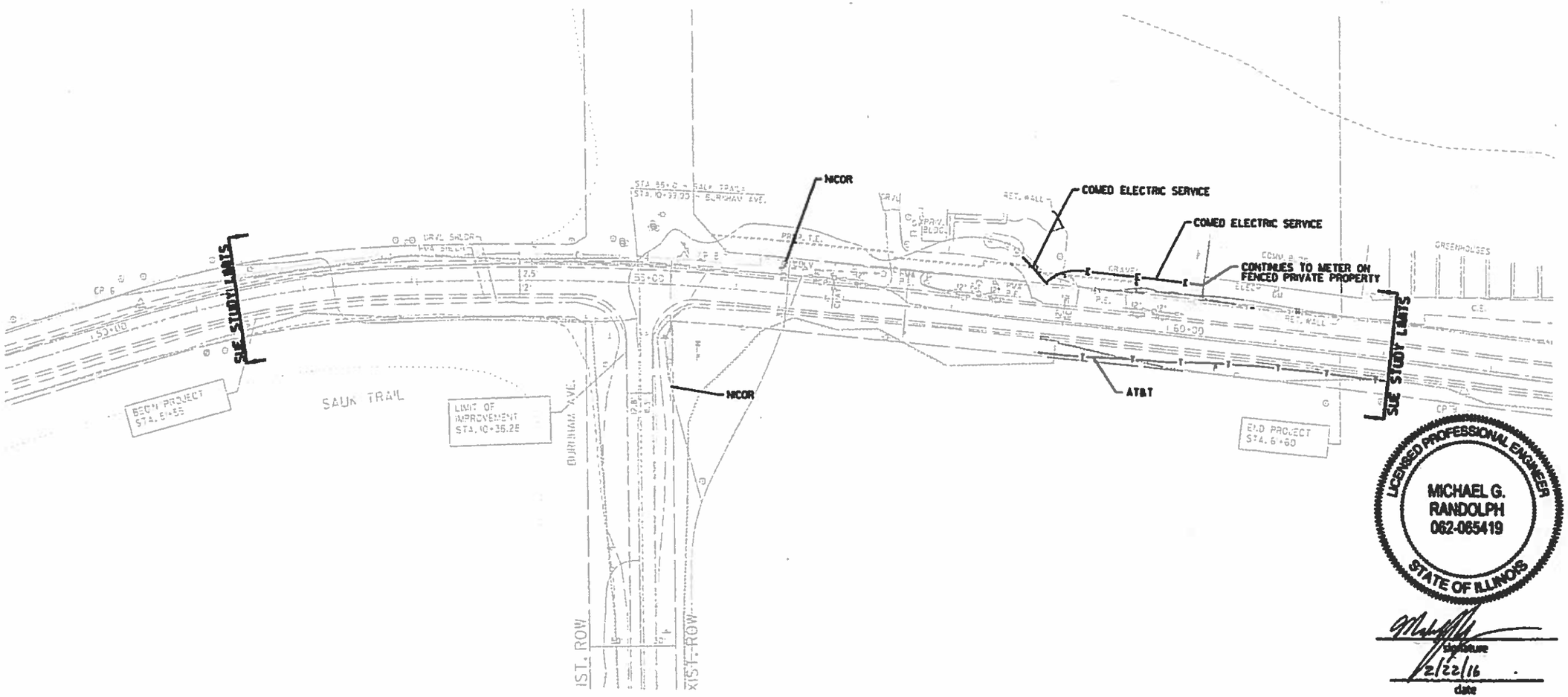
FILE NAME = c:\pw_work\pwidot\qureshiya\d0283041\P1100112-sht-typical.dgn	USER NAME = qureshiya	DESIGNED - ___	REVISED - ___ _____
		DRAWN - ___	REVISED - ___ _____
	PLOT SCALE = 100.0000 ' / in.	CHECKED - ___	REVISED - ___ _____
	PLOT DATE = 10/26/2016	DATE - _____	REVISED - ___ _____

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED DRAINAGE TABLE
SAUK TRAIL AT BURNHAM AVE.

SCALE: _____ | SHEET NO. __ OF ___ SHEETS | STA. _____ TO STA. _____

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	21
		CONTRACT NO. 60X38		
	ILLINOIS	FED. AID PROJECT		



LICENSED PROFESSIONAL ENGINEER
MICHAEL G. RANDOLPH
062-065419
STATE OF ILLINOIS

signature
2/22/16
date

license expires 11-30-2017

AERIAL
UNKNOWN
OIL
CTV — CTV — CABLE TV
T — T — TELEPHONE
GAS
E — E — ELECTRIC
TRAFFIC SIGNAL/LIGHTING
W — WATER
FORCE MAIN
FO — FO — FIBER OPTIC
TBE TEST HOLE
EOI END OF INFORMATION

UTILITY OWNERS

NICOR - GAS
AT&T - TELEPHONE
COMED - ELECTRIC

Utilities shown on these plans as depicted in the legend have been investigated by Cardno in accordance with SUE Industry Standards. All other information shown has been provided to Cardno by others. Cardno's SUE field investigation was performed 2/1/16 through 2/12/16. Changes to utilities after 2/12/16 may have been made and therefore may result in variances from this plan. Consideration should be given to updating this plan if deemed advisable prior to final design and construction.

ALL UTILITIES SHOWN QUALITY LEVEL "B" UNLESS NOTED OTHERWISE.

20 0 METERS 20 40
50 0 FEET 50 100

Cardno

CWA SURVEY

CLAASSEN, WHITE & ASSOCIATES, P.C.
LAND SURVEYORS
[illegible]

TBE Job No. L0950109
SUE Plan Pages 1 of 1

Utility Quality Level 'A': Visually Verified Test Hole
Utility Quality Level 'B': Designating/non Visually Verified Test Hole
Utility Quality Level 'C': Research with Survey
Utility Quality Level 'D': Records Research

DESIGNED	EG	REVISED
DRAWN	KLC	REVISED
CHECKED	MGR	REVISED
DATE	2/19/16	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

Sauk Trail & Burnham Ave.
Chicago Heights, Illinois

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	60X38	Cook	148	22
		Contract No. 49N		
FED. ROAD DIST. NO.	ILLINOIS	IDOT Project No.		

STATE OF ILLINOIS

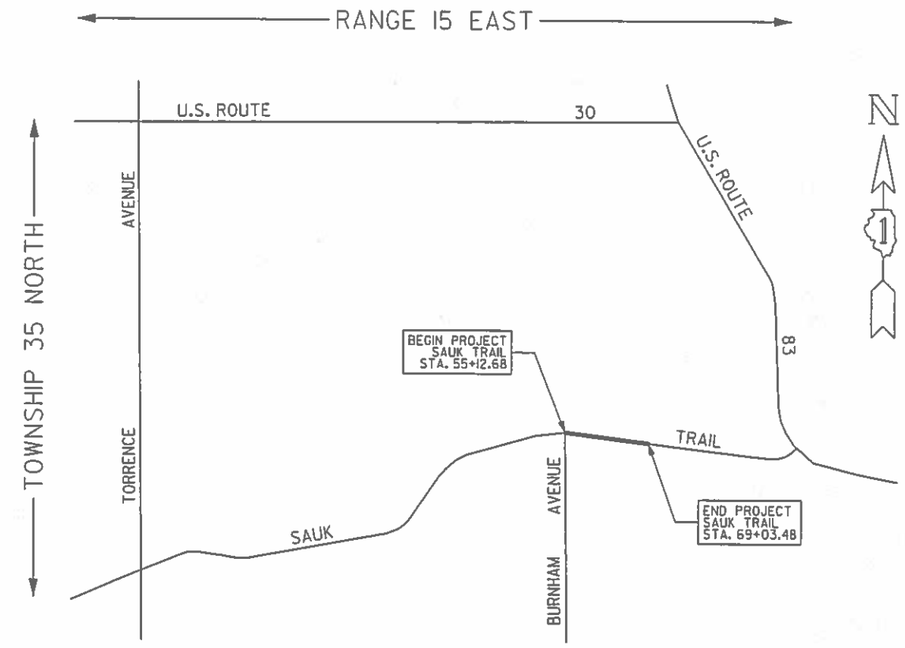
DEPARTMENT OF TRANSPORTATION

DIVISION OF HIGHWAYS

PLAT OF HIGHWAYS

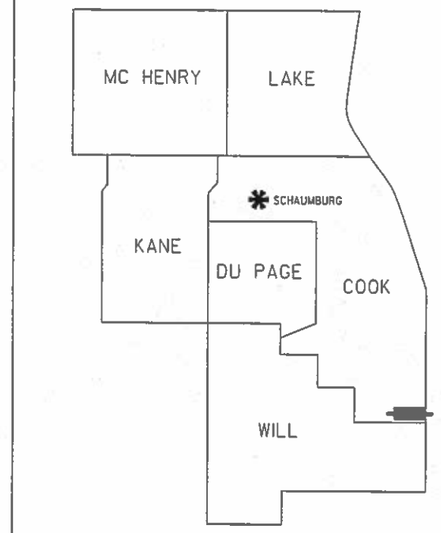
ROUTE: SAUK TRAIL
SECTION:
COUNTY: COOK
LIMITS: AT BURNHAM AVENUE
JOB NO.: R-90-025-13

PARCEL NUMBER	OWNER	SHEET NUMBER	PROPERTY ACQUIRED BY
OKE0001 OKE001TE-A OKE001TE-B	Carl A. Smits, a bachelor	2	_____



LOCATION MAP

PROJECT LENGTH = 1391.2 FT. = 0.263 MILES, SAUK TRAIL



DISTRICT HEADQUARTERS: -
LOCATION OF SECTION INDICATED THUS: -

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

Robinson ENGINEERING
17000 SOUTH PARK AVENUE
SOUTH HOLLAND, IL 60473
(708) 331-6700

PART OF SECTION 29, TWP. 35 N., R. 15 E. OF THE 3RD. P.M., IN COOK COUNTY, ILLINOIS.

PARCEL NUMBER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	PART TAKEN SQUARE FEET	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	EASEMENT AREA SQUARE FEET	PARCEL INDEX NUMBER
OKE0001 OKE0011E-A OKE0011E-B	29.712	0.485		0.476	29.227	TE-A=0.019 TE-B=0.026		33-29-100-023 33-29-100-014

PROJECT COORDINATES FOR PROPOSED CENTERLINE				
POINT NUMBER	STATION	OFFSET	NORTHING	EASTING
2002	STA.=52+69.36	0.00' RIGHT	1759923.4786'	1201674.9229'
2003	STA.=56+36.63	0.00' RIGHT	1759917.6455'	1202041.8167'
2004	STA.=59+38.01	0.00' RIGHT	1759890.7519'	1202342.0033'
2005	STA.=63+73.68	0.00' RIGHT	1759844.1391'	1202775.1424'
2006	STA.=74+68.35	0.00' RIGHT	1759707.6012'	1203861.2611'

PROJECT COORDINATES FOR PROPOSED R.O.W.				
POINT NUMBER	STATION	OFFSET	NORTHING	EASTING
3000	STA.=55+83.96	43.00' LEFT	1759964.6918'	1201992.2518'
3001	STA.=56+36.63	43.00' LEFT	1759960.4740'	1202045.6525'
3002	STA.=57+43.00	43.00' LEFT	1759950.9820'	1202151.6023'
3003	STA.=57+43.00	41.69' LEFT	1759949.8804'	1202151.4857'
3014	STA.=61+36.27	31.32' LEFT	1759902.6024'	1202542.6169'

CURVE "A"
Δ=08°25'02"
L=367.27'
R=2500.00'
CH.=366.94'
CH. BRG.=S89°05'21"E

CURVE "B"
Δ=02°02'45"
L=435.67'
R=12201.98'
CH.=435.64'
CH. BRG.=S83°51'28"E

CURVE "1"
Δ=01°12'25"
L=53.57'
R=2543.00'
CH.=53.57'
CH. BRG.=S85°29'02"E

CURVE "2"
Δ=03°48'15"
L=154.36'
R=2324.84'
CH.=154.33'
CH. BRG.=N86°22'00"W

CURVE "3"
Δ=01°20'07"
L=59.38'
R=2548.00'
CH.=59.36'
CH. BRG.=S85°32'53"E

CURVE "4"
Δ=00°06'27"
L=30.09'
R=12244.21'
CH.=30.09'
CH. BRG.=S84°48'37"E

CURVE "5"
Δ=01°26'01"
L=58.17'
R=2324.84'
CH.=58.17'
CH. BRG.=S83°44'52"E

CURVE "6"
Δ=09°40'28"
L=386.98'
R=2291.84'
CH.=386.52'
CH. BRG.=S87°52'05"E

CURVE "7"
Δ=07°11'50"
L=287.89'
R=2291.84'
CH.=287.70'
CH. BRG.=N86°37'46"W

CURVE "8"
Δ=01°58'19"
L=80.02'
R=2324.84'
CH.=80.01'
CH. BRG.=S89°15'17"E

LEGEND

- SECTION CORNER
- QUARTER SECTION CORNER
- SECTION / QUARTER SECTION LINE
- PLATTED LOT LINES
- PROPERTY (DEED) LINE
- APPARENT PROPERTY LINE
- EXISTING CENTERLINE
- PROPOSED CENTERLINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- EXISTING EASEMENT
- PROPOSED EASEMENT
- EXISTING ACCESS CONTROL LINE
- PROPOSED ACCESS CONTROL LINE
- 129.32' MEASURED DIMENSION
- 129.32' (COMP) COMPUTED DIMENSION
- (129.32') RECORDED DIMENSION
- EXISTING BUILDING

GRAPHIC SCALE FEET 0 100
SCALE: 1" = 100'

BEARINGS ARE REFERENCED TO THE ILLINOIS STATE PLANE COORDINATE SYSTEM, (NAD'83 (2007 ADJUSTMENT)), EAST ZONE.

- IRON PIPE OR ROD FOUND
- "MAG" NAIL SET
- CUT CROSS FOUND OR SET
- 5/8" REBAR SET
- T1, T2, T3 **THESE STAKES REFERENCE FOUND OR SET MONUMENTATION.** SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- BT1, BT2, BT3 **THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION.** BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- **STAKING OF PROPOSED RIGHT OF WAY.** SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- M **STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS.** BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- PERMANENT SURVEY MARKER, I.D.O.T. STANDARD 2135 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET

STATE OF ILLINOIS)
)SS
COUNTY OF COOK)

THIS IS TO CERTIFY THAT I, RANDELL E. GANN, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, (WE, ROBINSON ENGINEERING, LTD., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184001128,) HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 29, TOWNSHIP 35 NORTH, RANGE 15 EAST OF THE THIRD PRINCIPAL MERIDIAN, COOK COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT SOUTH HOLLAND, ILLINOIS THIS 6TH DAY OF NOVEMBER, 2013 A.D.

ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 035-003241
LICENSE EXPIRATION DATE: NOVEMBER 30, 2014

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

RANDELL E. GANN 035-003241 PROFESSIONAL LAND SURVEYOR STATE OF ILLINOIS SOUTH HOLLAND, ILLINOIS

RECEIVED
APR 25 2014
PLATS & LEGALS

Robinson ENGINEERING
17000 SOUTH PARK AVENUE
SOUTH HOLLAND, IL 60473
(708) 331-6700

NOTES:

1. EXISTING CENTERLINE OF RIGHT OF WAY AS DEPICTED IN PLAT PREPARED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, C.H. ROUTE 203, SECTION 203-0815-R, SHEET 32

COORDINATE SYSTEM DEFINITION:
North American Datum Of 1983 adjustment of 2007 (N.A.D. '83 (2007)) Illinois State Plane Eastern Zone estimated GROUND coordinates based upon an average location for the length of the project having the following parameters:

WGS '84 North Latitude 41°29'08.25505"N
WGS '84 West Longitude 87°32'14.35128"W
WGS '84 Height 555.010 U.S. Feet
Orthometric Height 664.760 U.S. Feet (estimated utilizing Geoid '03)

Utilization of these parameters should yield a ground scale factor of 0.9999971188

EXISTING R.O.W. PER COOK COUNTY PUBLIC HIGHWAY RIGHT OF WAY PROPERTY RECORD PAGE 17 MAP INDEX 26 DATED AUGUST 28, 1851 AND RE-ESTABLISHED AS DEPICTED IN DRAWING PREPARED BY THE ILLINOIS BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, C.H. ROUTE 203, SECTION 203-0815-R, SHEET 32

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
SAUK TRAIL AT BURNHAM AVENUE

LIMITS:
SECTION: BURNHAM AVENUE
STATION 55+12.68
SCALE: 1"=100'

COUNTY: COOK
JOB NO.: R-90-025-13
TO STATION 69+03.84
SHEET 02 OF 03

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

REVISION DATE: REVISION MADE BY

SAVED: 11/11/2013 9:04:17 AM DWG NAME: N:\PROJECTS\2013\510-01-915_DOT_COOK_SAUK_VILLAGE_SAUK_TRAIL_BURNHAM_AVE\DGN\510-01-915_02_Plan.dgn

BLOOM TWP.

BURNHAM AVE.

E LINE SE¼ SEC. 30-35-15
W LINE SW¼ SEC. 29-35-15

N½ NE¼ SEC. 31-35-15
E½ SE¼ SEC. 30-35-15
N½ NW¼ SEC. 32-35-15
E½ SW¼ SEC. 29-35-15
N½ SE¼ SEC. 30-35-15
E½ NE¼ SEC. 30-35-15
N½ SW¼ SEC. 29-35-15
E½ NW¼ SEC. 29-35-15

SAUK TRAIL 66

BURNHAM AVE.

E LINE NE¼ SEC. 30-35-15
W LINE NW¼ SEC. 29-35-15

ELGIN JOLIET & EASTERN R.R.

JOLIET & NORTHERN INDIANA R.R.

BLOOM TWP.

NOTE:
SAUK TRAIL HIGHWAY
66' BY TWP DEDICATION 8-28-1861
COTTAGE GROVE AVE. TO INDIANA LINE
NO WIDTH GIVEN ILLINOIS STATUE'S GIVE 66'

NOTE:
BURNHAM AVE.
60' BY CONDEMNATION 6-13-1838
STEGER ROAD TO SAUL TRAIL

N

100 0 100 200 300 400

NO.	DATE	BY	DESCRIPTION	APP.

REVISIONS

R/W DEPARTMENT OF HIGHWAYS
RIGHT OF WAY ENGINEERING AND SURVEY DIVISION
COOK COUNTY, ILLINOIS

PRESIDENT BOARD OF COMMISSIONERS — SUPERINTENDENT OF HIGHWAYS

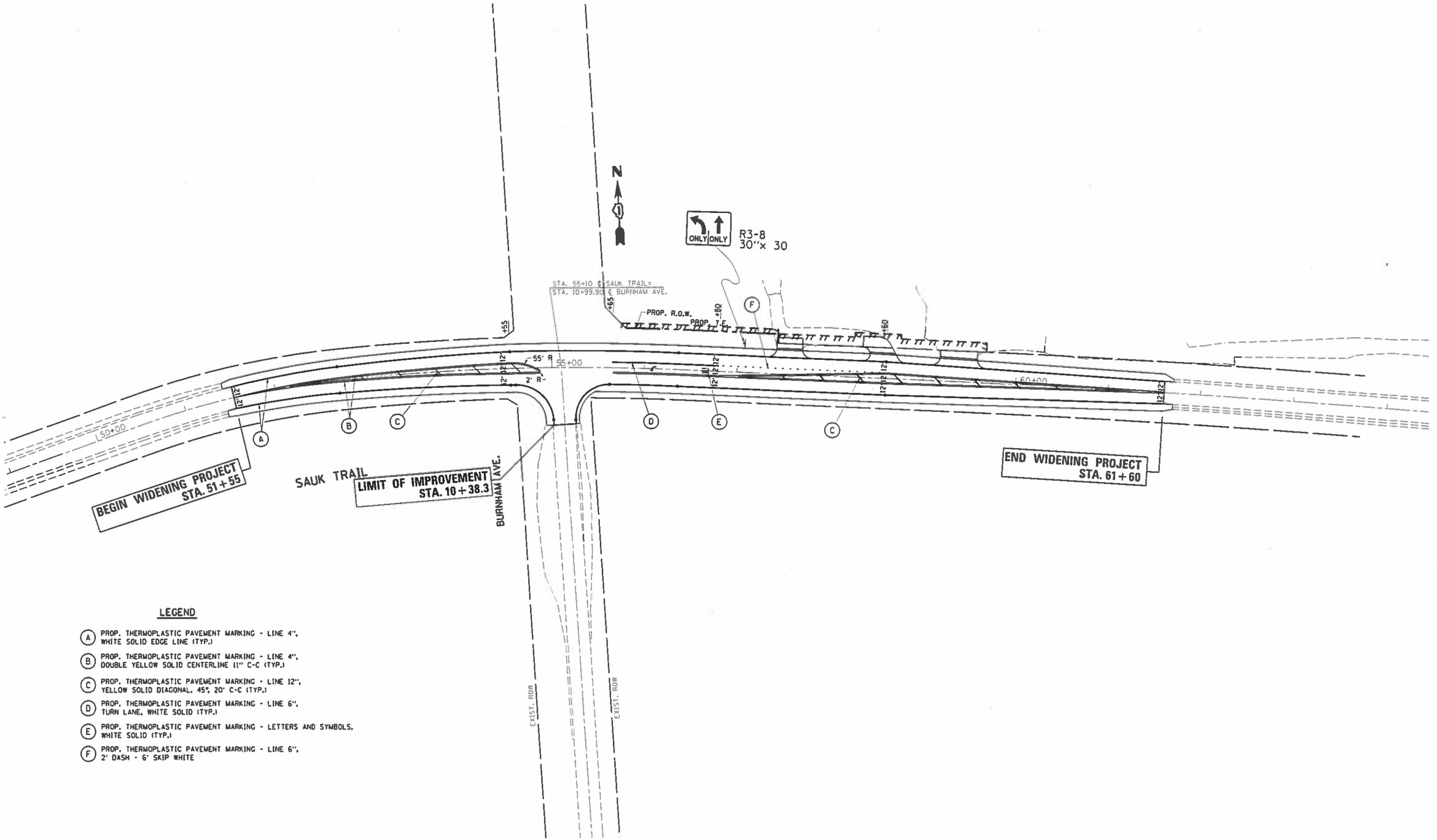
STRIP MAP
BURNHAM AVE.
183RD ST. TO STEGER RD.

DRAWN J.K. DATE 10-15-1973
SCALE 1" = 100 CHECKED
SURVEY NO. APPROVED

APPROVED
CHIEF ENGINEER OF DESIGN

COUNTY HIGHWAY SECTION NO.	SHEET NO.	TOTAL SHEETS	PLAT NO.
039-0106	3	6	SM# 282

W & E CO. 10-6139



LEGEND

- (A) PROP. THERMOPLASTIC PAVEMENT MARKING - LINE 4", WHITE SOLID EDGE LINE (TYP.)
- (B) PROP. THERMOPLASTIC PAVEMENT MARKING - LINE 4", DOUBLE YELLOW SOLID CENTERLINE 11" C-C (TYP.)
- (C) PROP. THERMOPLASTIC PAVEMENT MARKING - LINE 12", YELLOW SOLID DIAGONAL, 45°, 20' C-C (TYP.)
- (D) PROP. THERMOPLASTIC PAVEMENT MARKING - LINE 6", TURN LANE, WHITE SOLID (TYP.)
- (E) PROP. THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS, WHITE SOLID (TYP.)
- (F) PROP. THERMOPLASTIC PAVEMENT MARKING - LINE 6", 2' DASH - 6' SKIP WHITE

FILE NAME = p:\IL084EBIDINTEG.illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\PI00\Data\CADsheets\PI00I2-sht-pmk.dgn	USER NAME = liszek-f	DESIGNED -	REVISED -
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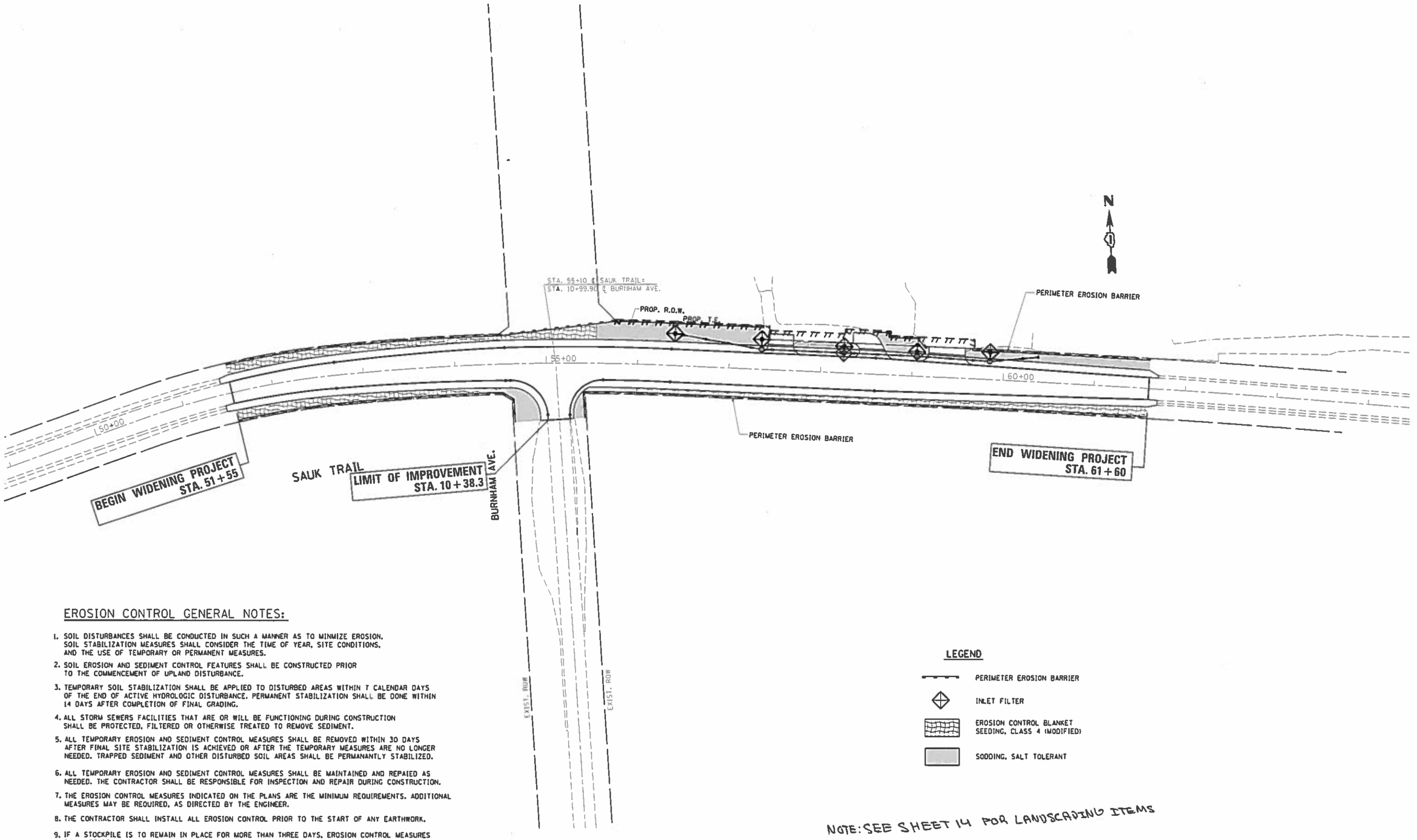
	PLOT DATE = 11/4/2016	CHECKED -	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING AND SIGNAGE PLAN
SAUK TRAIL AT BURNHAM RD.

SCALE: 1"=50' | SHEET OF SHEETS | STA. 51+55 TO STA. 61+60

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	26
		CONTRACT NO. 60X38		
	ILLINOIS FED. AID PROJECT			



EROSION CONTROL GENERAL NOTES:

1. SOIL DISTURBANCES SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, SITE CONDITIONS, AND THE USE OF TEMPORARY OR PERMANENT MEASURES.
2. SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE CONSTRUCTED PRIOR TO THE COMMENCEMENT OF UPLAND DISTURBANCE.
3. TEMPORARY SOIL STABILIZATION SHALL BE APPLIED TO DISTURBED AREAS WITHIN 7 CALENDAR DAYS OF THE END OF ACTIVE HYDROLOGIC DISTURBANCE. PERMANENT STABILIZATION SHALL BE DONE WITHIN 14 DAYS AFTER COMPLETION OF FINAL GRADING.
4. ALL STORM SEWERS FACILITIES THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED, FILTERED OR OTHERWISE TREATED TO REMOVE SEDIMENT.
5. ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED. TRAPPED SEDIMENT AND OTHER DISTURBED SOIL AREAS SHALL BE PERMANANTLY STABILIZED.
6. ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED AND REPAIED AS NEEDED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSPECTION AND REPAIR DURING CONSTRUCTION.
7. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER.
8. THE CONTRACTOR SHALL INSTALL ALL EROSION CONTROL PRIOR TO THE START OF ANY EARTHWORK.
9. IF A STOCKPILE IS TO REMAIN IN PLACE FOR MORE THAN THREE DAYS, EROSION CONTROL MEASURES SHALL BE PROVIDED BY THE CONTRACTOR.

LEGEND

- PERIMETER EROSION BARRIER
- INLET FILTER
- EROSION CONTROL BLANKET SEEDING, CLASS 4 (MODIFIED)
- SODDING, SALT TOLERANT

NOTE: SEE SHEET 14 FOR LANDSCAPING ITEMS

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Default	PLOT SCALE = 100.0000 ' / in.	DRAWN -	REVISED -
	PLOT DATE = 11-22-2016	CHECKED -	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

LANDSCAPING, EROSION & SEDIMENT CONTROL PLAN
SAUK TRAIL AT BURNHAM RD.

SCALE: 1"=50' | SHEET OF SHEETS | STA. 51+55 TO STA. 61+60

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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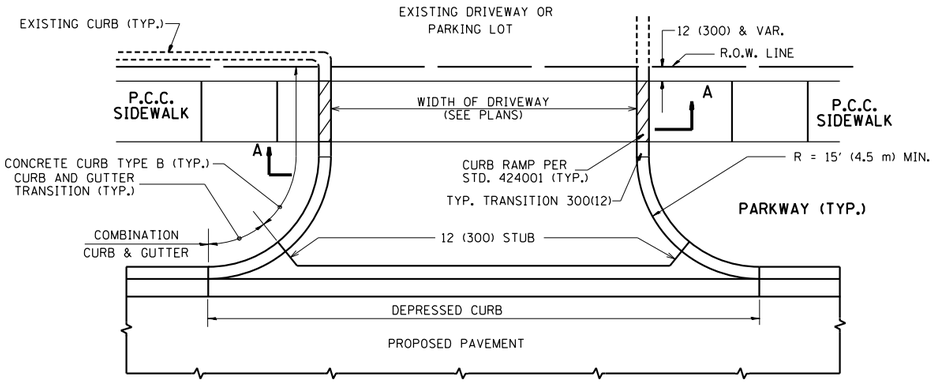
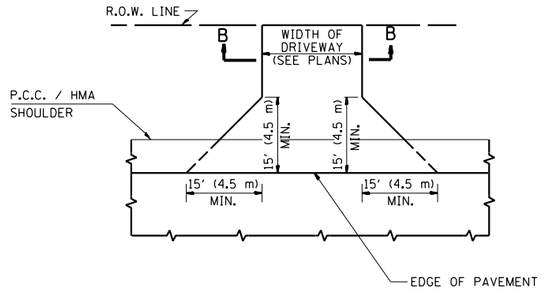
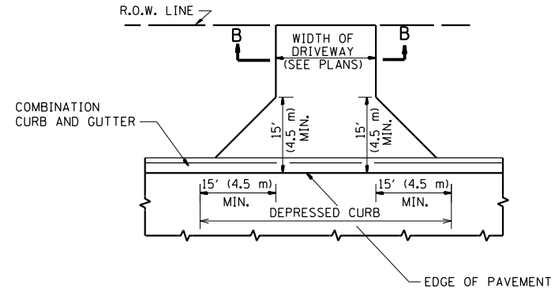
		CONTRACT NO. 60X38		
		ILLINOIS FED. AID PROJECT		



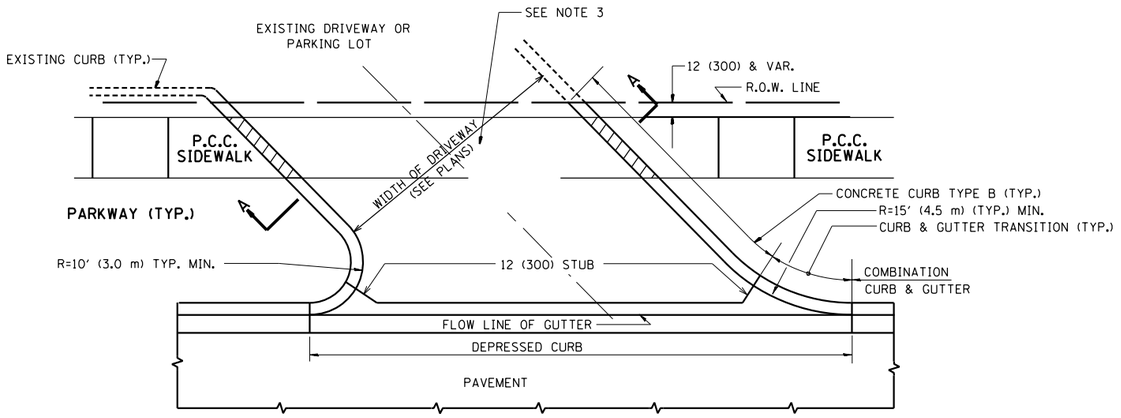
WITH CONCRETE CURB, TYPE B



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



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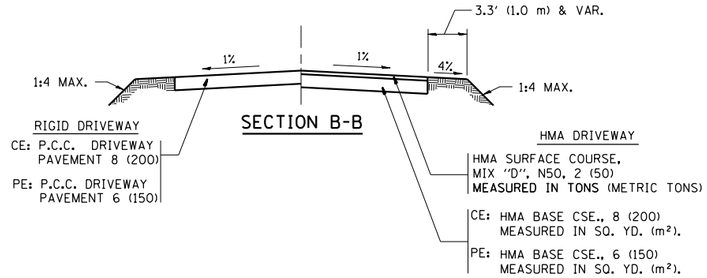
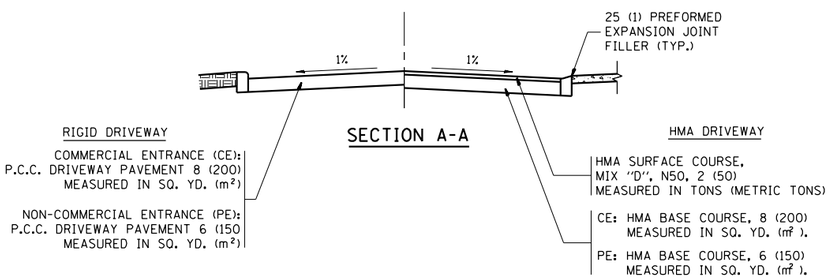


SECTION B-B

RURAL FIELD ENTRANCE (FE)

HMA SURFACE COURSE, MIX "D", N50, 2 (50) MEASURED IN TONS (METRIC TONS)

AGGREGATE BASE CSE., TYPE B, 8 (200) MEASURED IN SQ. YD. (m²).



SECTION A-A

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

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		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0002 ' / in.	CHECKED -	REVISED - R. BORO 06-11-08
	PLOT DATE = 10/26/2016	DATE - 11-04-95	REVISED - R. BORO 09-06-11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS – DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER > = 15' (4.5 m)

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	27
	BD0156-07 (BD-01)		CONTRACT NO. 60X38	
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

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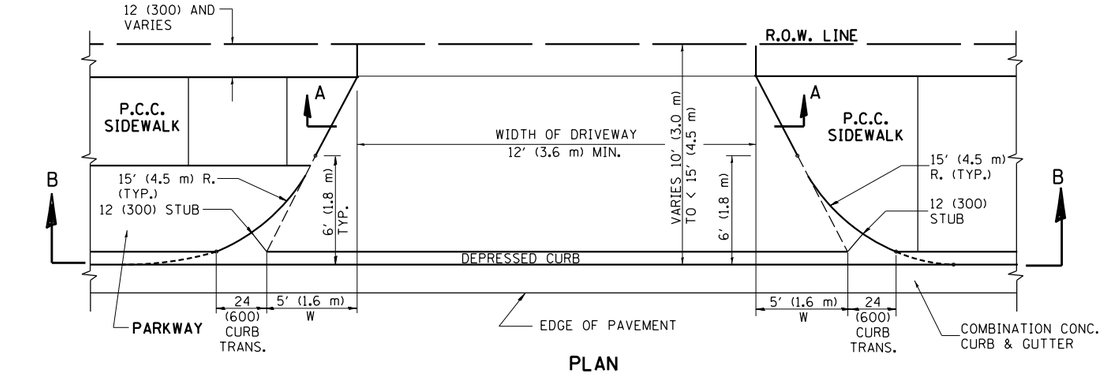
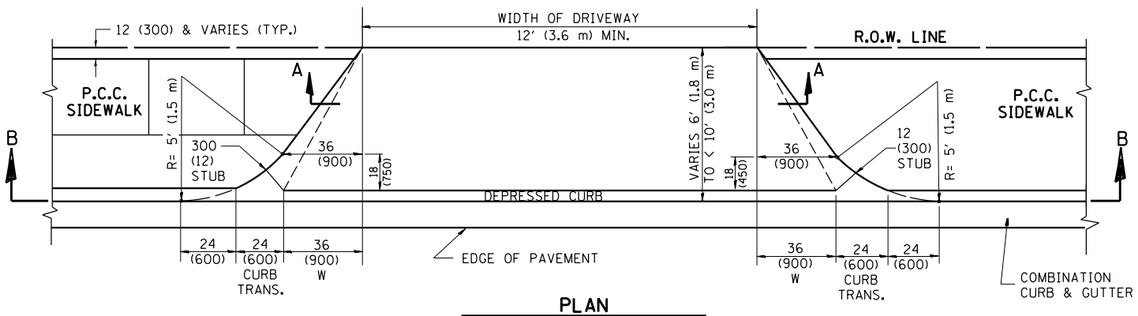
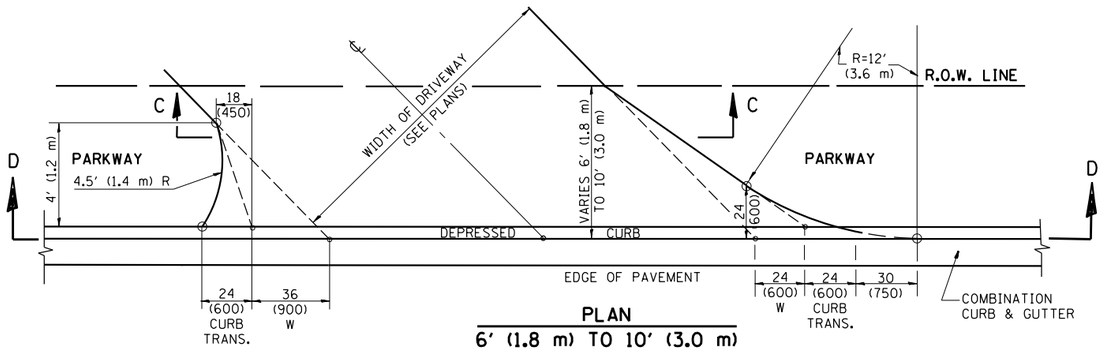


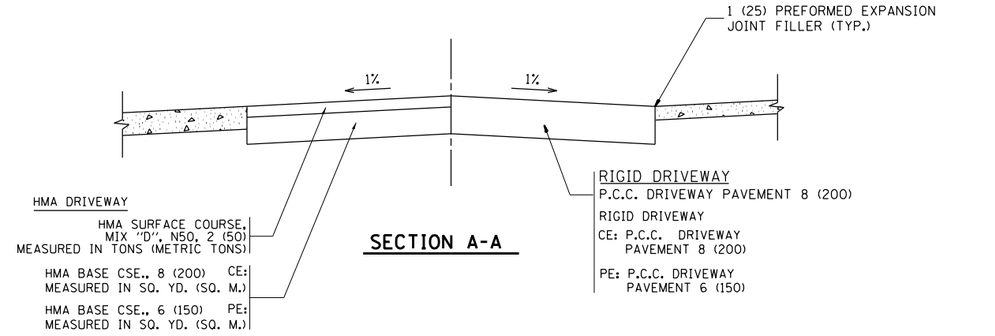
PLAN
10' (3.0 m) TO < 15' (4.5 m)



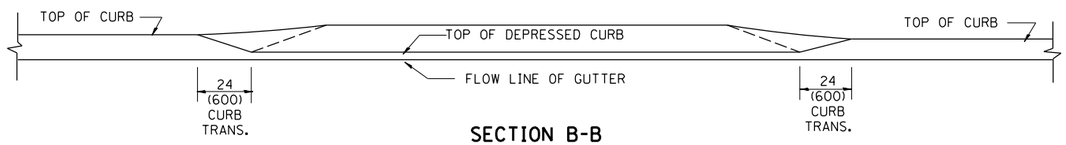
PLAN
6' (1.8 m) TO < 10' (3.0 m)



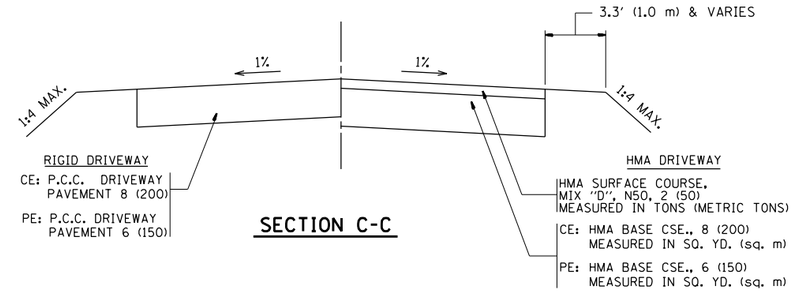
PLAN
6' (1.8 m) TO 10' (3.0 m)



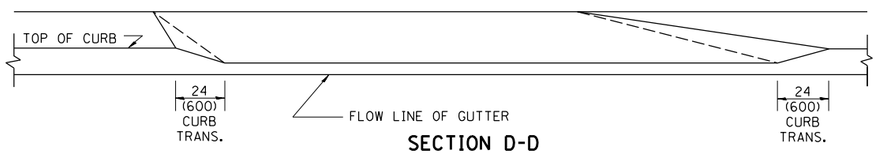
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

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		DRAWN -	REVISED - P. LaFLEUR 04-15-03			1632	49R-RS	COOK	48	28
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	PLOT DATE = 10/26/2016	DATE - 11-06-95	REVISED - R. BORO 01-01-07		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

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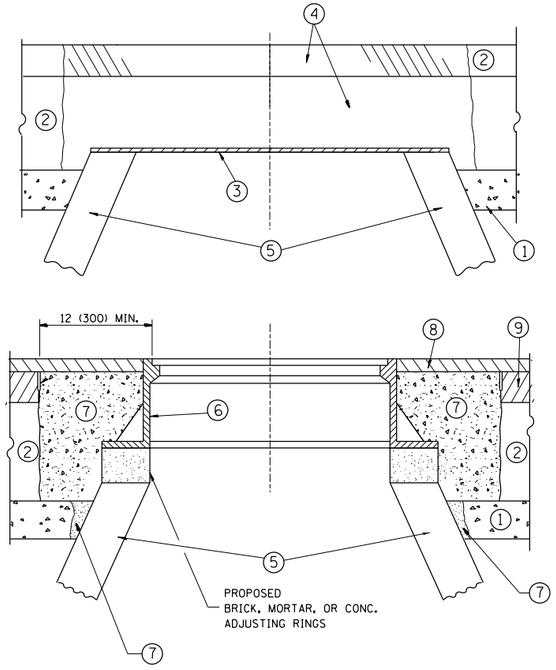

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

1. SUB-BASE GRANULAR MATERIAL
2. EXISTING PAVEMENT
3. 36 (900) DIAMETER METAL PLATE
4. PROPOSED CRUSHED STONE AND HMA SURFACE MIX
5. EXISTING STRUCTURE
6. FRAME AND LID (SEE NOTES)
7. CLASS PP-1* CONCRETE
8. PROPOSED HMA SURFACE COURSE
9. PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0002 ' / in.	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 10/26/2016	DATE - 10-25-94	REVISED - R. BORO 12-06-11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	29
	BD600-03 (BD-8)	CONTRACT NO. 60X38		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

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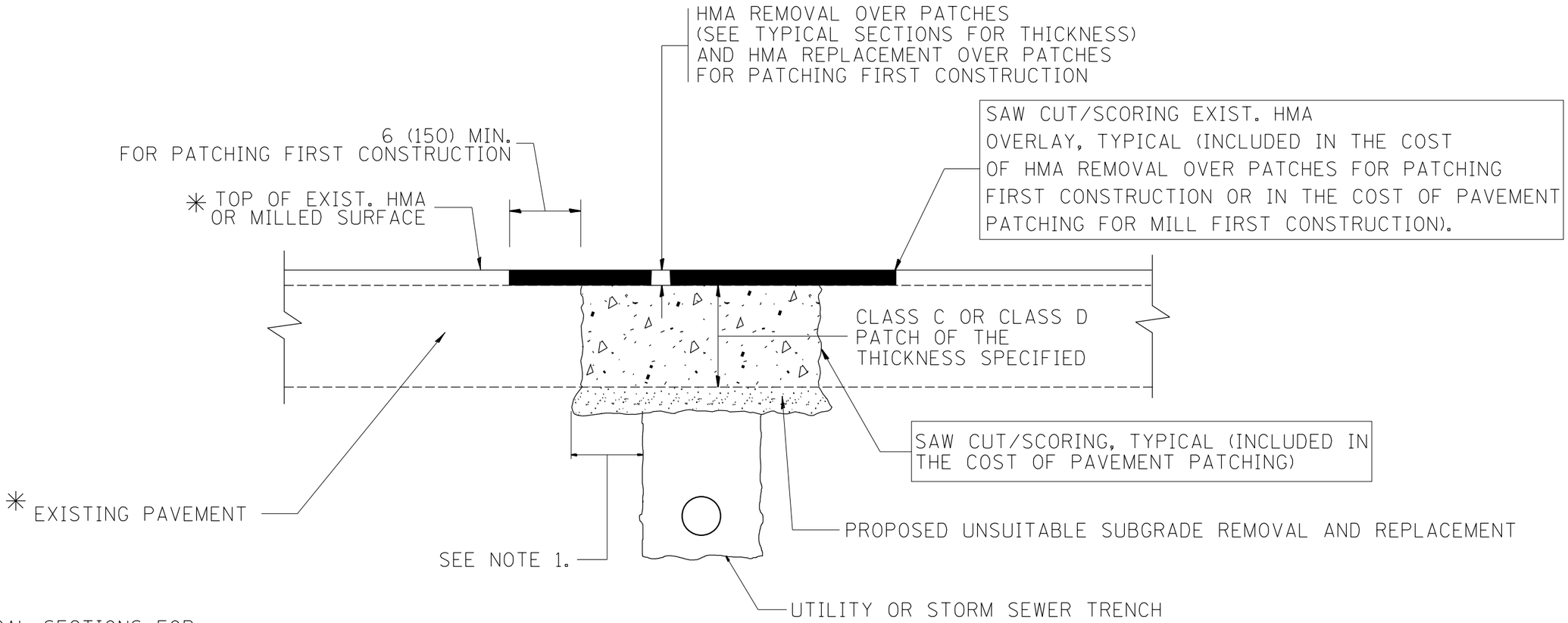

✳ SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST $4\frac{1}{2}$ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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	PLOT SCALE = 100.0002 ' / in.	DRAWN -	REVISED - R. BORO 01-01-07		
	PLOT DATE = 10/26/2016	CHECKED -	REVISED - R. BORO 09-04-07		
		DATE - 10-25-94	REVISED - K. ENG 10-27-08		SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	30
	BD400-04 (BD-22)	CONTRACT NO. 60X38		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

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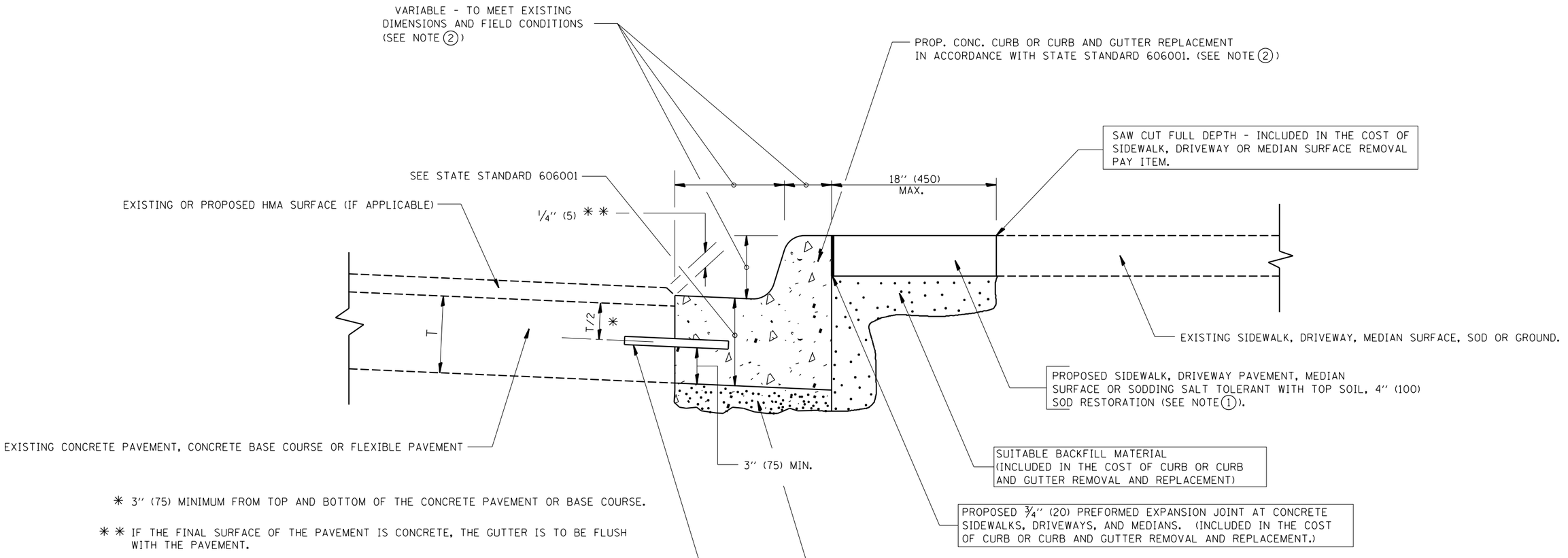

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

* * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: (1) SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

(2) FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

(3) CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

(4) FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

(5) LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

(6) THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

(7) THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

(8) THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 100.0002 ' / in.	CHECKED -	REVISED - M. GOMEZ 01-22-01
	PLOT DATE = 10/26/2016	DATE - 03-11-94	REVISED - R. BORO 12-15-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

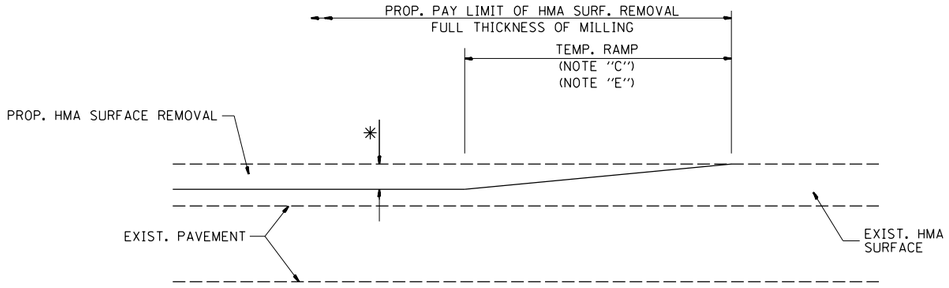
SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	31

BD600-06 (BD-24) | **CONTRACT NO. 60X38**

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT

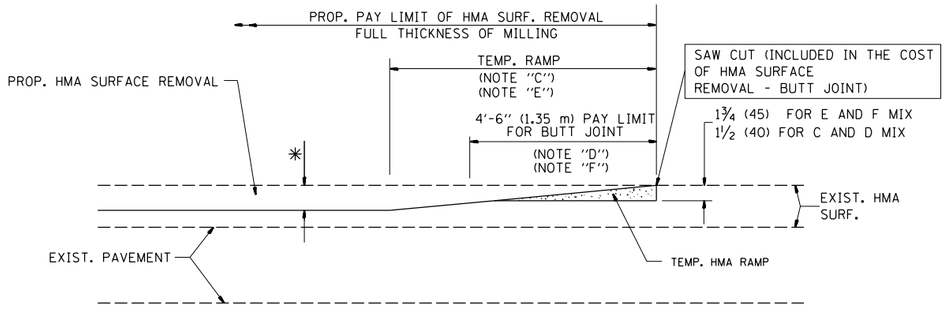
DistStd.dgn 10/26/2016 8:14:32 AM User=qureshiya



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

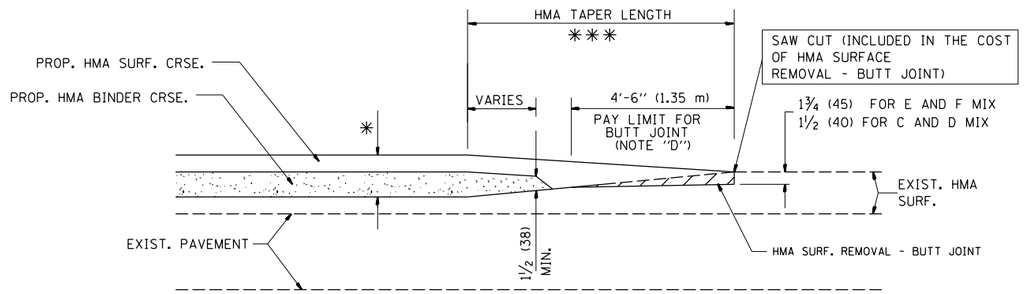


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

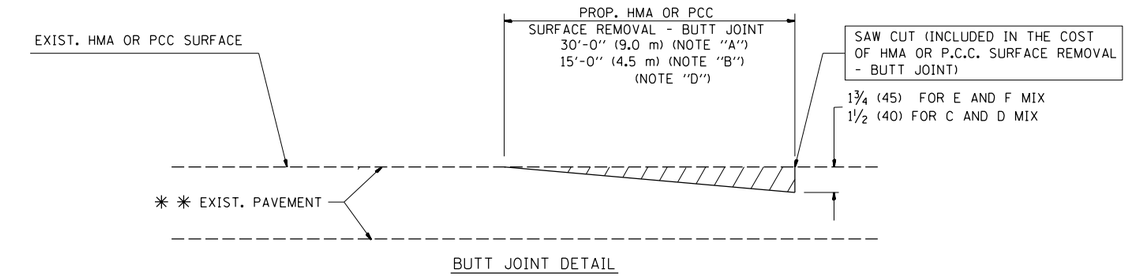
OPTION 2

TYPICAL TEMPORARY RAMP

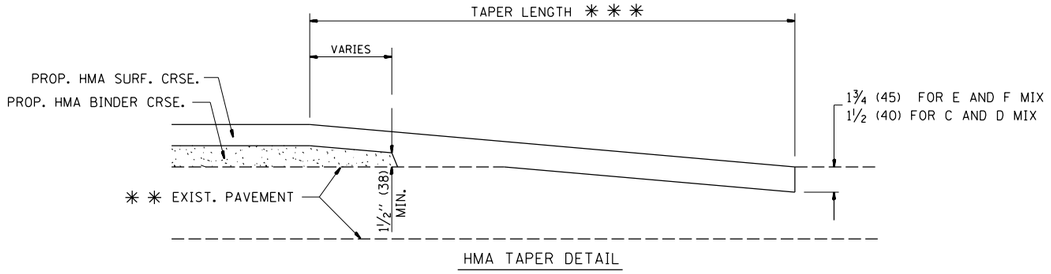


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.

B: MINOR SIDE ROADS.

C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.

F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 100.0002 ' / in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 10/26/2016	DATE - 06-13-90	REVISED - R. BORO 01-01-07

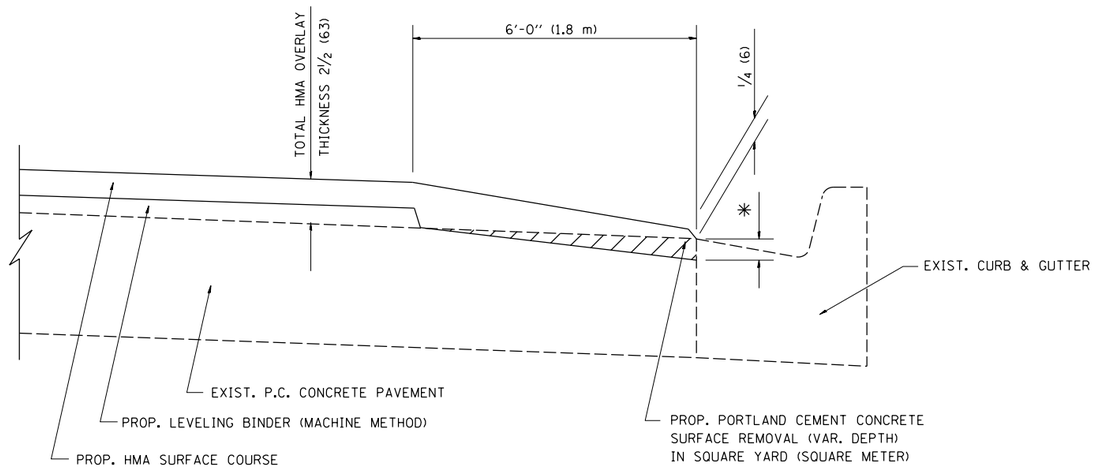
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	32
	BD400-05 BD32	CONTRACT NO. 60X38		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

DistStd.dgn 10/26/2016 8:14:45 AM User=qureshiya



HMA TAPER AT
EDGE OF P.C.C PAVEMENT

HMA SURFACE		LEVELING BINDER	
MIX	THICKNESS	THICKNESS	* MILLING AT GUTTER FLAG
C OR D	1½ (38)	1 (25)	1¼ (33)
E	1¾ (44)	¾ (19)	1½ (38)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		DRAWN - JIS	REVISED - E. GOMEZ 12-21-00
	PLOT SCALE = 100.0002 ' / in.	CHECKED - A. ABBAS	REVISED - R. BORO 01-01-07
Default	PLOT DATE = 10/26/2016	DATE - 09-10-94	REVISED - JP CHANG 07-08-16

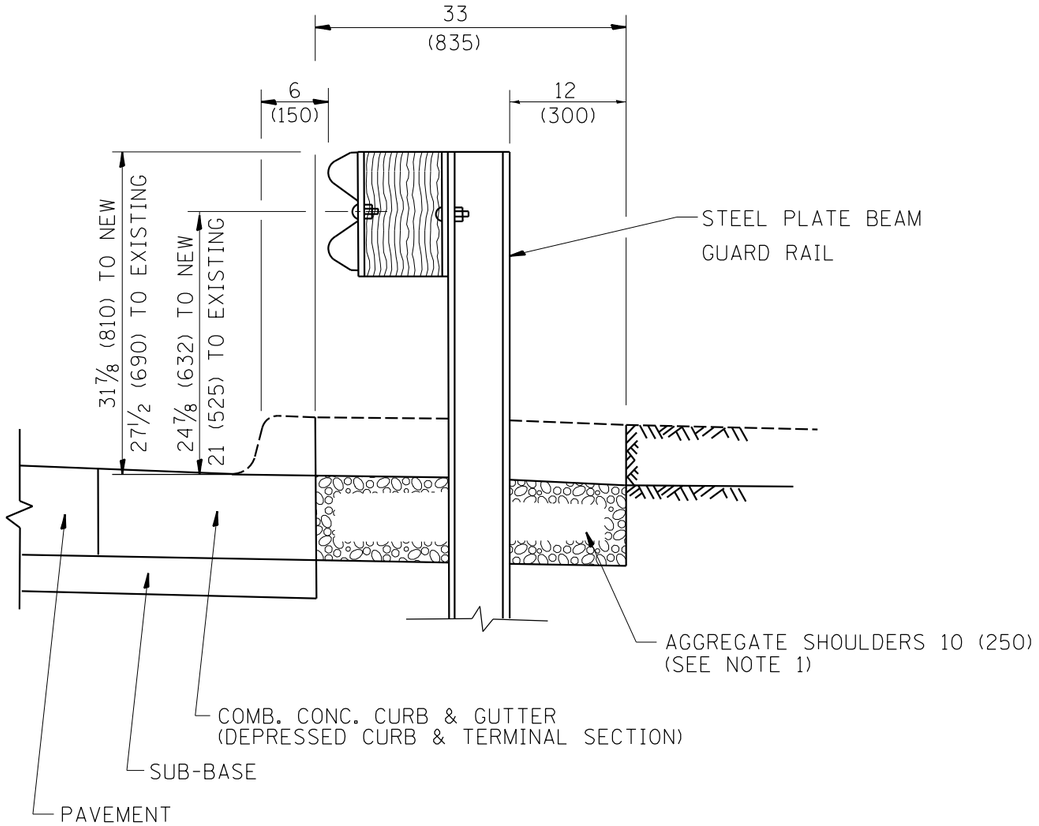
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

HMA TAPER AT
EDGE OF P.C.C. PAVEMENT

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	33
	BD400-06 (BD33)	CONTRACT NO. 60X38		
	ILLINOIS FED. AID PROJECT			

DistStd.dgn 10/26/2016 8:15:01 AM User=qureshiya

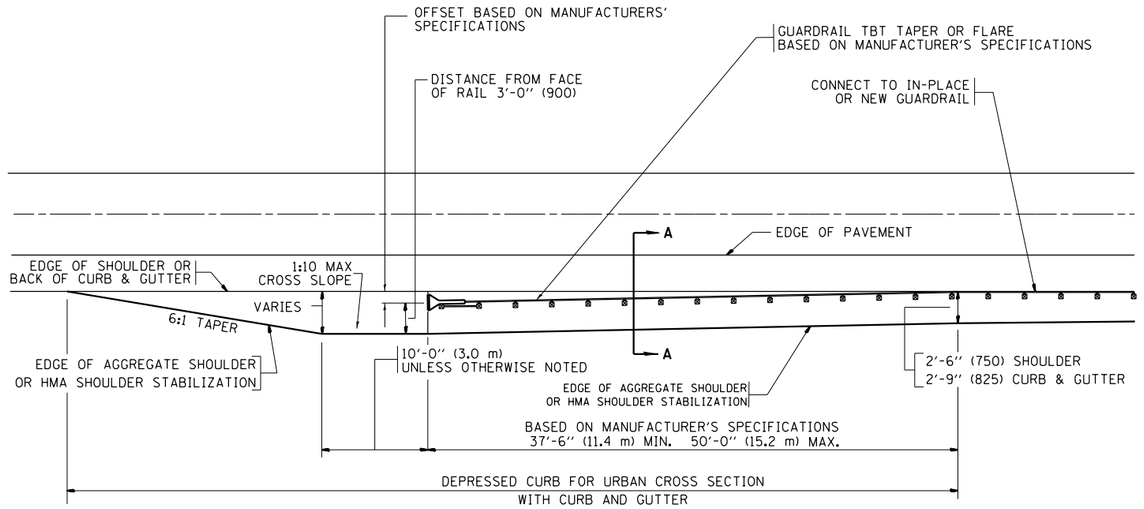


SECTION A-A

NOTES:
1. THE AGGREGATE SHOULDER, 10'' OR HMA SHOULDER, 6'' (IF REQUIRED) SHALL EXTEND UNDER THE TRAFFIC BARRIER TERMINAL.
2. ''EXISTING'' GUARDRAIL REFERS TO CONNECTING TERMINAL SECTION TO GUARD RAILING PRIOR TO THE MIDWEST GUARDRAIL SYSTEM.
3. THE CONTRACTOR SHALL VERIFY THE TYPE/HEIGHT OF GUARDRAIL IN-PLACE BEFORE ORDERING THE NEW TERMINAL SECTION. COST INCLUDED WITH THE COST OF THE TERMINAL. THE TERMINAL SECTION HEIGHT TO BE PLACED MUST MATCH THE HEIGHT OF THE IN-PLACE GUARDRAIL.

DETAILS FOR STEEL PLATE BEAM GUARD RAIL ADJACENT TO CURB AND GUTTER

[FOR ROADWAY SPEED 35 MPH (60 kmh) TO 45 MPH (70 kmh)]



DEPRESSED CURB AND GUTTER AND SHOULDER TREATMENT AT TBT TY. 1 SPL.

BASIS OF PAYMENT: HMA SHOULDERS 6 (150) (IF REQUIRED) WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR ''HOT-MIX ASPHALT SHOULDERS 6'' (150 mm)''.

STEEL PLATE BEAM GUARD RAIL AND TRAFFIC BARRIER TERMINAL, OF THE TYPE SPECIFIED WILL BE PAID FOR SEPARATELY.

TBT = TRAFFIC BARRIER TERMINAL

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		DRAWN -	REVISED - R. BORO 01-01-07		
	PLOT SCALE = 100.0002 '/ in.	CHECKED -	REVISED - R. BORO 12-08-2008		
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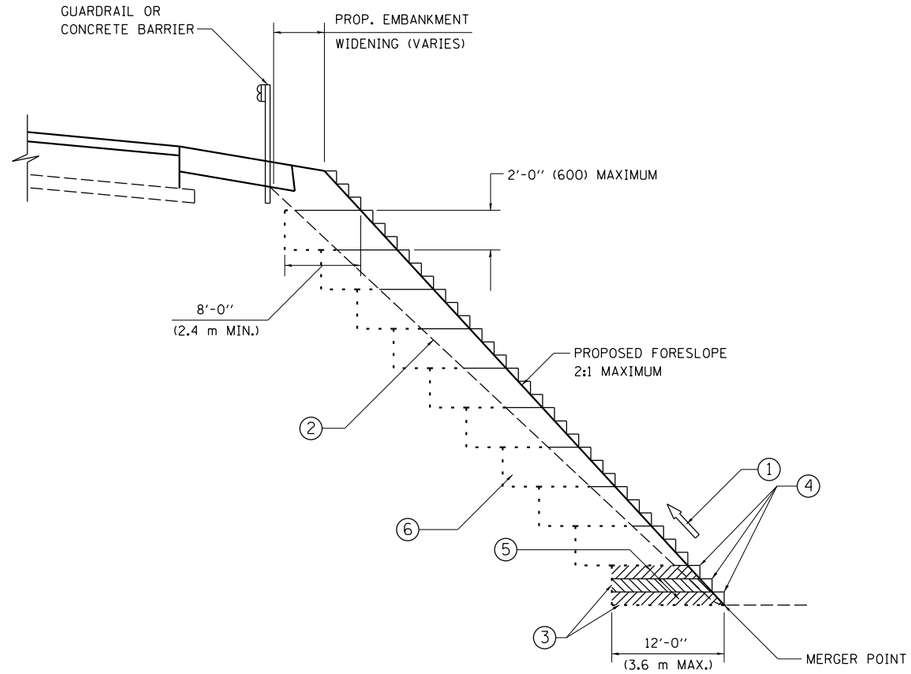
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	34
	BD600-10 (BD 34)	CONTRACT NO. 60X38		
FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT				

DistStd.dgn 10/26/2016 8:15:15 AM User=qureshiya



TYPICAL BENCHING DETAIL FOR EMBANKMENT

NOTES:

1. CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
2. EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS.
3. BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
4. TRIM TO FINAL SLOPE.
5. EQUAL 8-INCH (200) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.
6. EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR "EARTH EXCAVATION". THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL, NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
7. SLOPES SHALL BE BENCHED ACCORDING TO THIS DETAIL WHEN THE SLOPE IS STEEPER THAN 4:1 AND THE HEIGHT IS GREATER THAN 5' (1.5 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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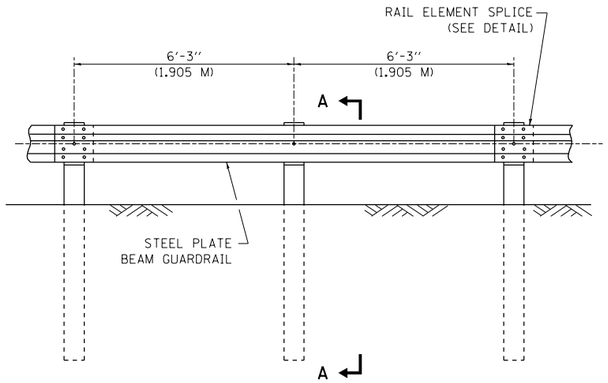
Default	PLOT SCALE = 100.0002 ' / in.	CHECKED - S.E.B.	REVISED -
	PLOT DATE = 10/26/2016	DATE - 06-16-04	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BENCHING DETAIL
FOR EMBANKMENT WIDENING

SCALE: | SHEET OF SHEETS | STA. | TO STA.

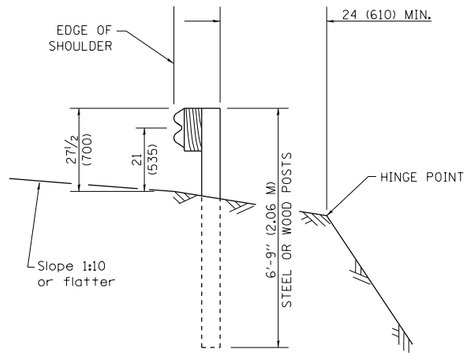
F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	35
	BD-51	CONTRACT NO. 60X38		
	ILLINOIS FED. AID PROJECT			



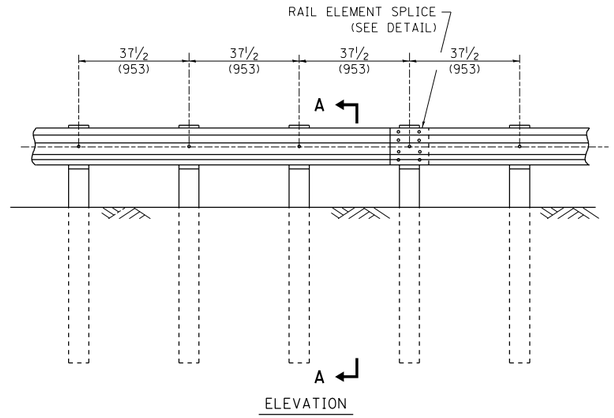
ELEVATION

TYPE A

6'-3" (1.905 M) TYPICAL POST SPACING



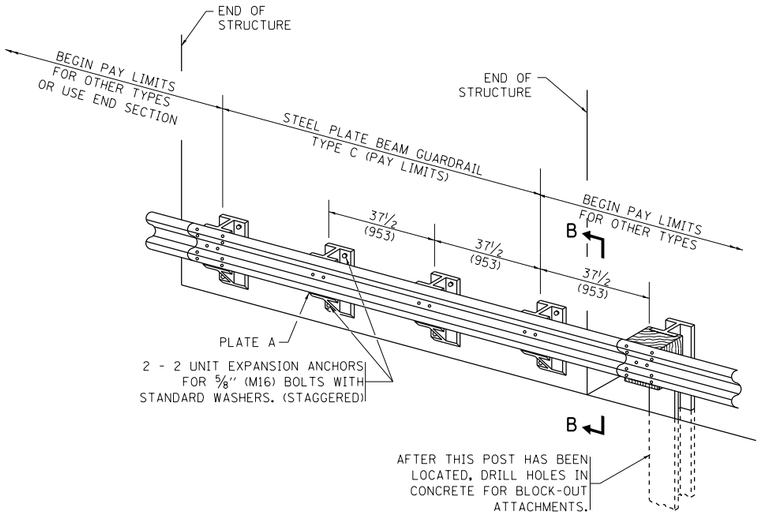
SECTION A-A



ELEVATION

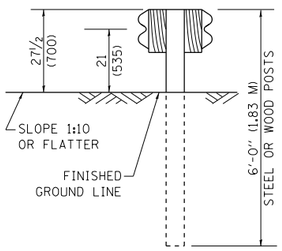
TYPE A

37½ (953) CLOSED POST SPACING

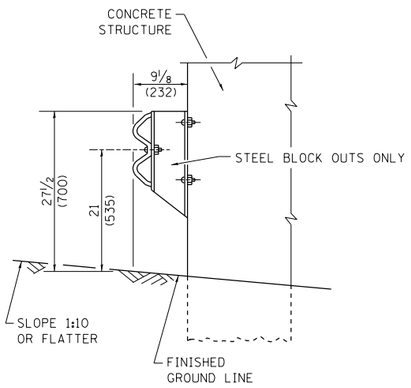


TYPE C

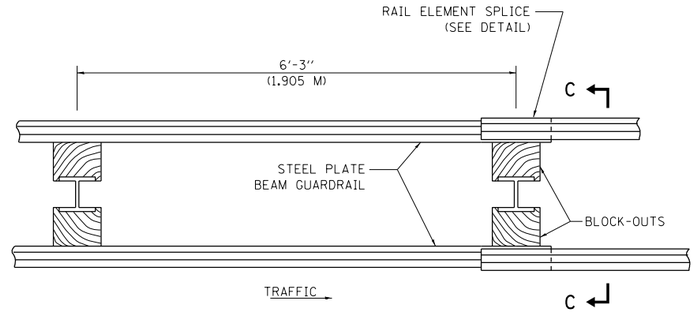
37½ (953) BLOCK-OUT SPACING



SECTION C-C



SECTION B-B



PLAN

TYPE D

DOUBLE STEEL PLATE BEAM GUARDRAIL
6'-3" (1.905 M) TYPICAL POST SPACING

GENERAL NOTES

ALL SLOPE RATIOS ARE EXPRESSED AS UNITS OF VERTICAL DISPLACEMENT TO UNITS OF HORIZONTAL DISPLACEMENT (V:H).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

THE EXISTING STEEL POSTS MAY BE DRILLED TO MATCH THE BOLT PATTERN SHOWN HEREIN FOR THE WOOD BLOCK-OUT, OR A NEW STEEL POST SHALL BE PROVIDED.

THIS DETAIL IS APPLICABLE TO THE GUARDRAIL SYSTEM USED PRIOR TO JANUARY 1, 2007. FOR DETAILS ON THE MIDWEST GUARDRAIL SYSTEM, SEE STANDARD 630001.

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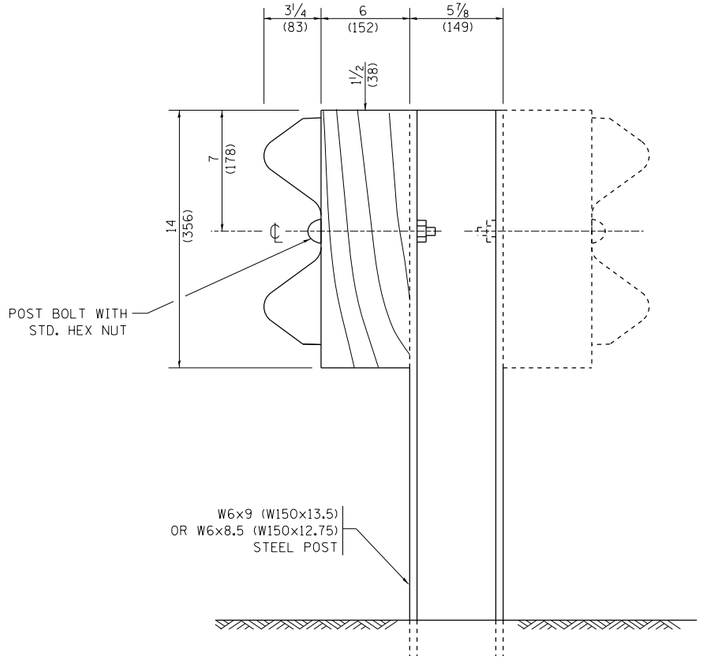
		DRAWN -	REVISED -
	PLOT SCALE = 100.0002 ' / in.	CHECKED -	REVISED -
	PLOT DATE = 10/26/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

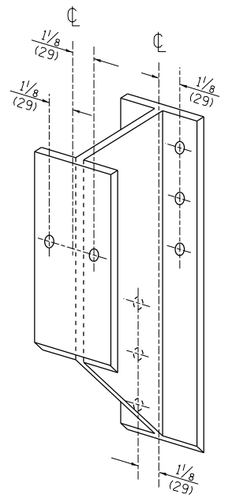
REMOVE AND REERECT
STEEL PLATE BEAM GUARDRAIL

SCALE: NONE | SHEET NO. 1 OF 4 SHEETS | STA. | TO STA.

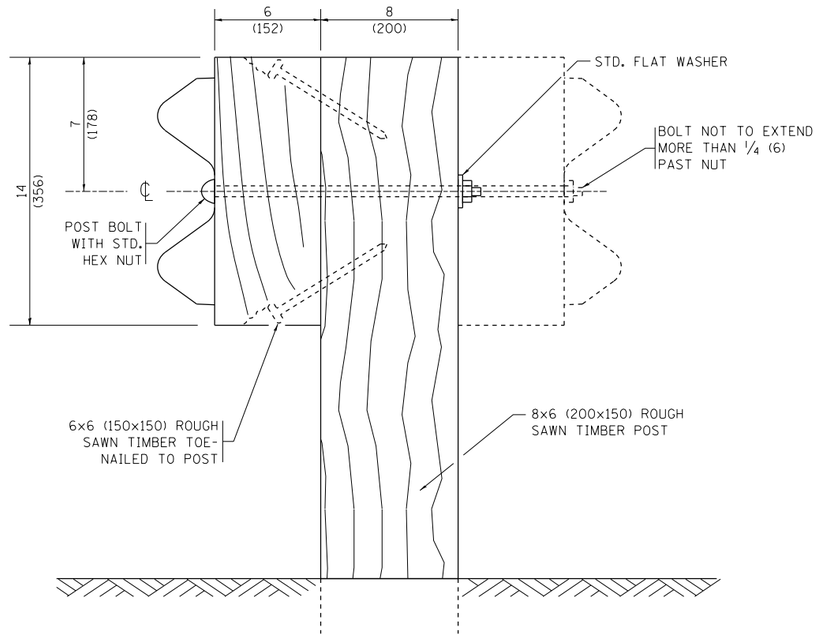
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	36
	BM-21	CONTRACT NO. 60X38		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



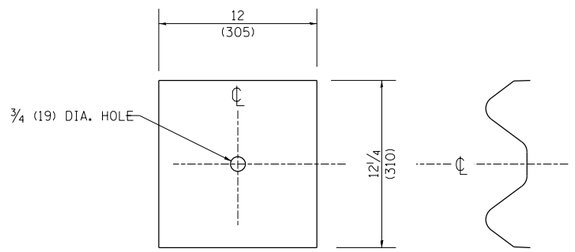
STEEL POST CONSTRUCTION



STEEL BLOCK-OUT DETAIL



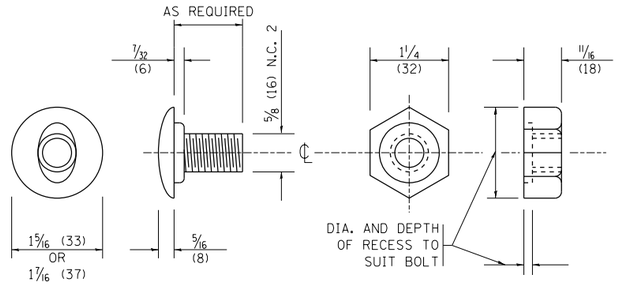
WOOD POST CONSTRUCTION



NOTE:

PLATE A SHALL BE PLACED BETWEEN RAIL ELEMENT AND BLOCK-OUT AT NON-SPLICE MOUNTING POINTS ONLY WHEN STEEL BLOCK-OUTS ARE USED.

PLATE A



POST OR SPLICE BOLT & NUT

FILE NAME = c:\pw_work\pwidot\qureshiya\d0283041\DistStd.dgn	USER NAME = qureshiya	DESIGNED -	REVISED - 10-31-06	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -					1632	49R-RS	COOK	48	37
	PLOT SCALE = 100.0002 ' / in.	CHECKED -	REVISED -						BM-21		CONTRACT NO. 60X38	
	PLOT DATE = 10/26/2016	DATE -	REVISED -		SCALE: NONE	SHEET NO. 2 OF 4 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

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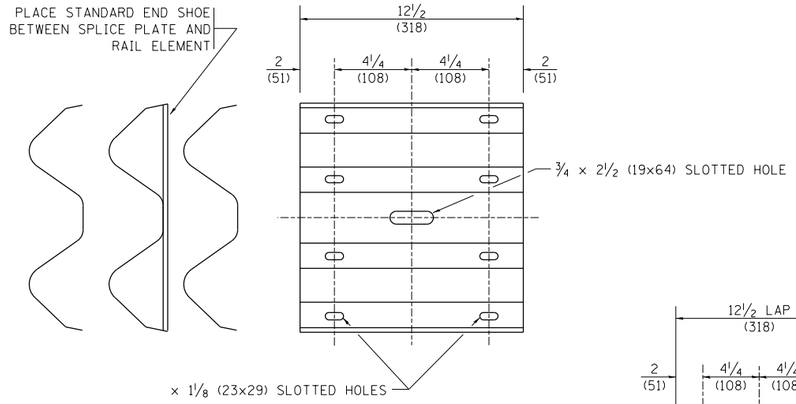
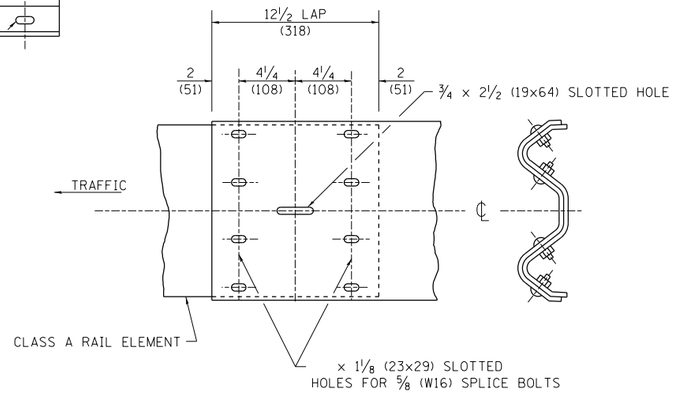
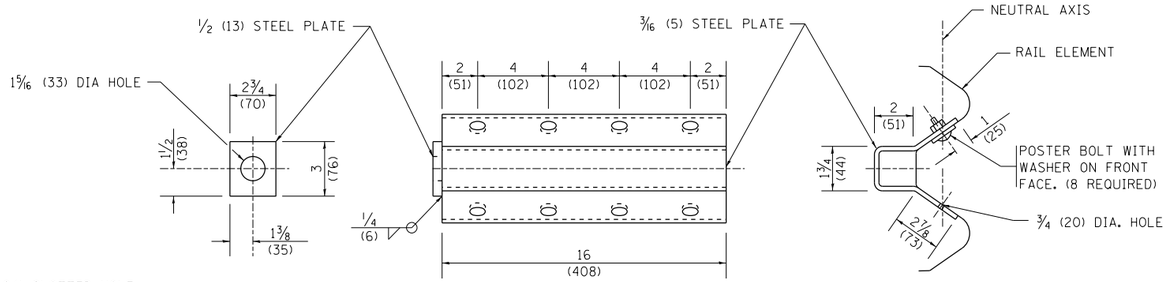


SPLICE PLATE



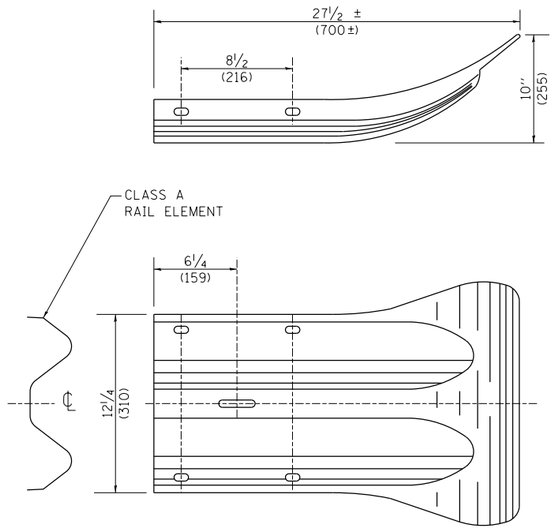
RAIL ELEMENT SPLICE



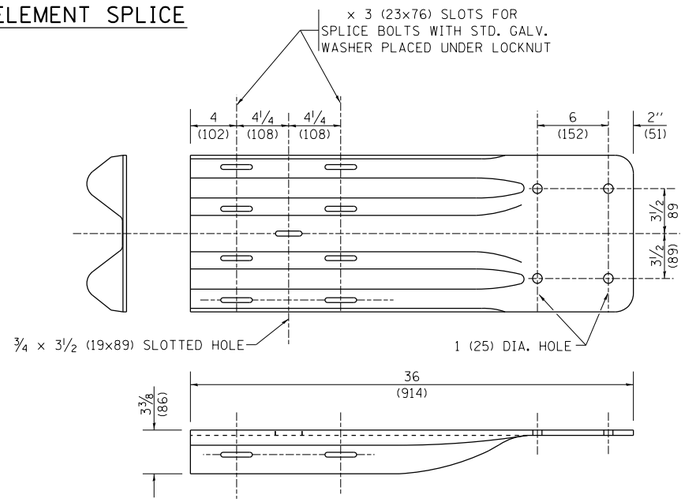
NOTE:

ANCHOR PLATE T SHALL BE USED TO ATTACH CABLE ASSEMBLY TO GUARDRAIL WHEN REQUIRED ON TRAFFIC BARRIER TERMINALS.

ANCHORE PLATE T DETAILS



END SECTION



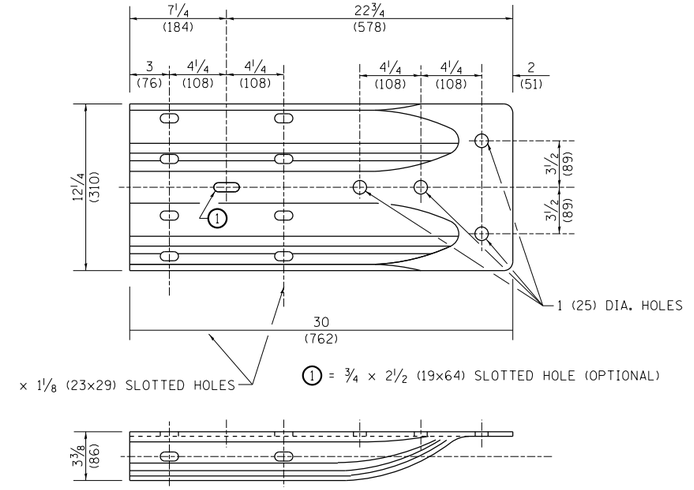
END SHOE

NOTE:

WHEN END SHOE IS ATTACHED TO A BRIDGE PARAPET WHICH HAS AN EXPANSION JOINT, THE BOLTS SHALL BE PROVIDED WITH A LOCKNUT OR DOUBLE NUT AND SHALL BE TIGHTENED ONLY TO A POINT THAT WILL ALLOW GUARDRAIL MOVEMENT.

THE STANDARD END SHOE SHALL BE ATTACHED TO THE CONCRETE WITH PRE-DRILLED OR SELF-DRILLING ANCHOR BOLTS. THE ANCHOR CONE SHALL BE SET FLUSH WITH THE SURFACE OF THE CONCRETE.

EXTERNALLY THREADED STUDS PROTRUDING FROM THE SURFACE OF THE CONCRETE WILL NOT BE PERMITTED.

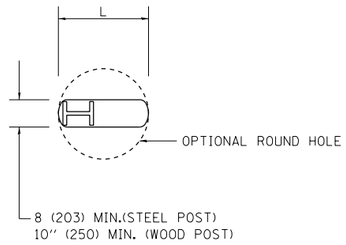


ALTERNATE END SHOE

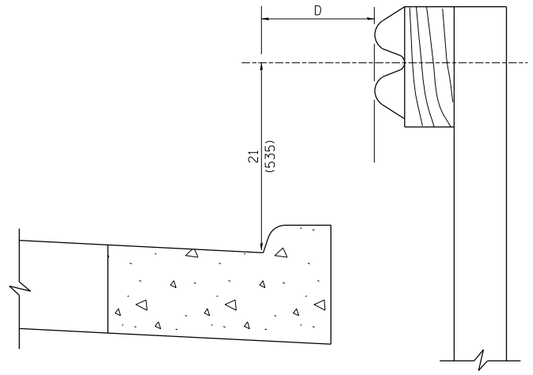
FILE NAME = c:\pw_work\pwidot\qureshiya\d0283041\DistStd.dgn	USER NAME = qureshiya	DESIGNED -	REVISED - 10-31-06	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL
	PLOT SCALE = 100.0002 ' / in.	DRAWN -	REVISED -		SCALE: NONE | SHEET NO. 3 OF 4 SHEETS | STA. | TO STA.
	PLOT DATE = 10/26/2016	CHECKED -	REVISED -		
		DATE -	REVISED -		

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	38
	BM-21	CONTRACT NO. 60X38		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

DistStd.dgn 10/26/2016 8:16:26 AM User=qureshiya



PLAN

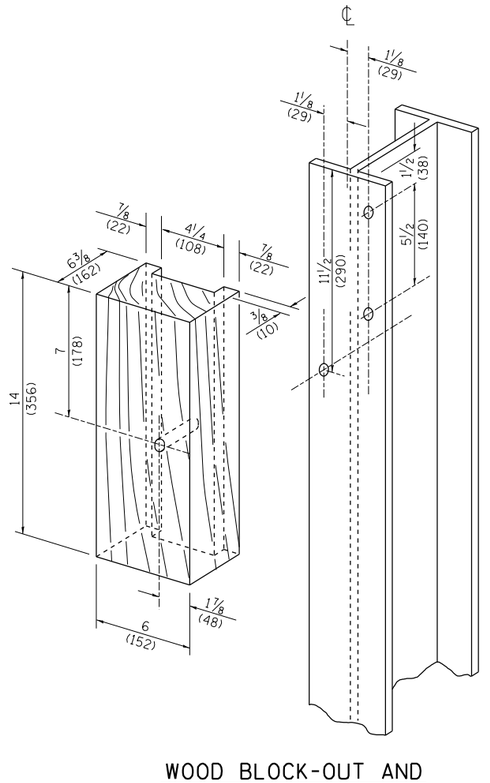


NOTE:

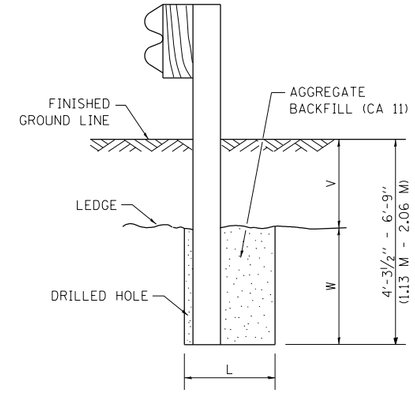
IF IT IS NECESSARY FOR D TO BE MORE THAN 12 (300) AND LESS THAN 10'-0'' (3.0 M) TYPE M-2 (M-5) CURB AND GUTTER (STD. 606001) SHALL BE USED IN FRONT OF AND IN ADVANCE OF THE GUARDRAIL.

GUARDRAIL PLACED BEHIND CURB

(D = 0 DESIRABLE TO 12 (300) MAXIMUM)



WOOD BLOCK-OUT AND STEEL POST DETAILS



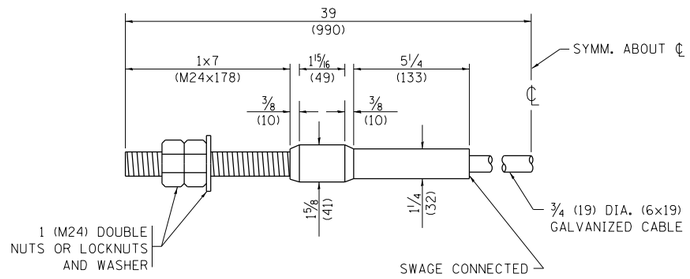
NOTE:

LEDGE LINE IS TOP OF ROCK LEDGE OR HARD SLAG FILL.

ELEVATION

FOOTING FOR POST WHEN IMPERVIOUS MATERIAL IS ENCOUNTERED

V	W	L	
		STEEL POST	WOOD POST
0 - 18 (0 - 460)	24 (610)	21 (530)	23 (580)
>18 - 41.5 (> 460 - 825)	12 (305)	8 (203)	10 (250)
>41.5 - 53.5 (> 825 - 1.13 M)	12 - 0 (350 - 0)	8 (203)	10 (250)



CABLE ASSEMBLY

(40,000 LBS (18,100 KG) MIN. BREAKING STRENGTH)
TIGHTEN TO TAUT TENSION

FILE NAME = c:\pw_work\pwidot\qureshiya\d0283041\DistStd.dgn	USER NAME = qureshiya	DESIGNED -	REVISED - 10-31-06
		DRAWN -	REVISED -
	PLOT SCALE = 100.0002 ' / in.	CHECKED -	REVISED -
	PLOT DATE = 10/26/2016	DATE -	REVISED -

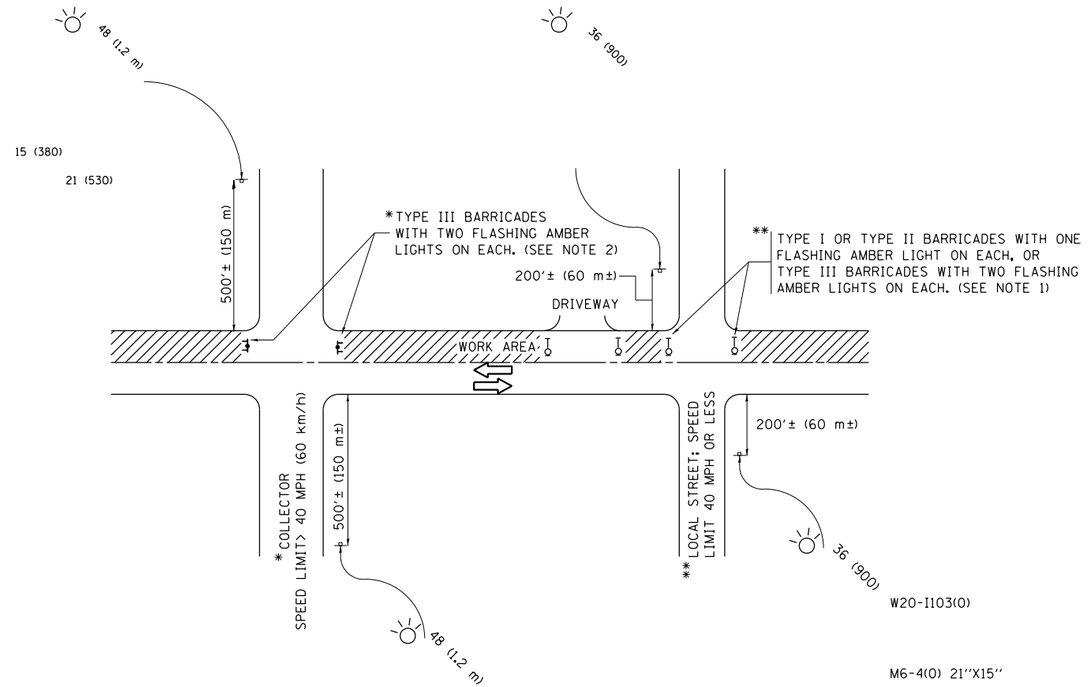
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REMOVE AND REERECT
STEEL PLATE BEAM GUARDRAIL

SCALE: NONE | SHEET NO. 4 OF 4 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	39
	BM-21	CONTRACT NO. 60X38		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

DistStd.dgn 10/26/2016 8:16:38 AM User=qureshiya



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = c:\pw_work\pwidot\qureshiya\d0283041\DistStd.dgn	USER NAME = qureshiya	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
	PLOT SCALE = 100.0002 ' / in.	DRAWN -	REVISED -T. RAMMACHER 01-06-00
Default	PLOT DATE = 10/26/2016	CHECKED -	REVISED - A. SCHUETZE 07-01-13
		DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

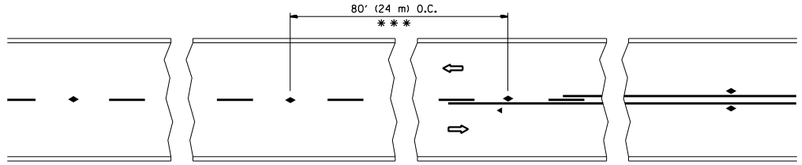
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

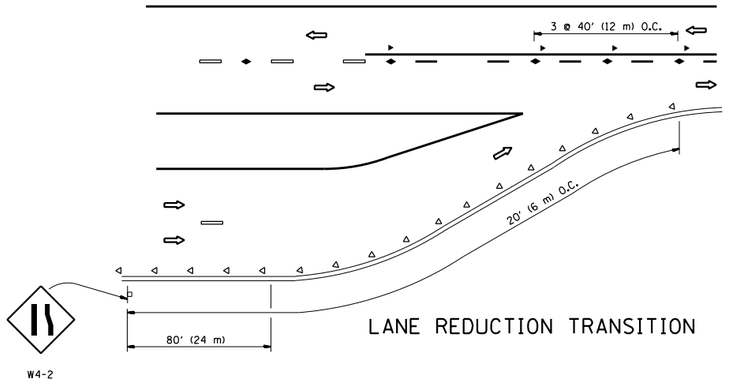
F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	40
	TC-10	CONTRACT NO. 60X38		
	ILLINOIS FED. AID PROJECT			

DistStd.dgn 10/26/2016 8:16:57 AM User=qureshiya

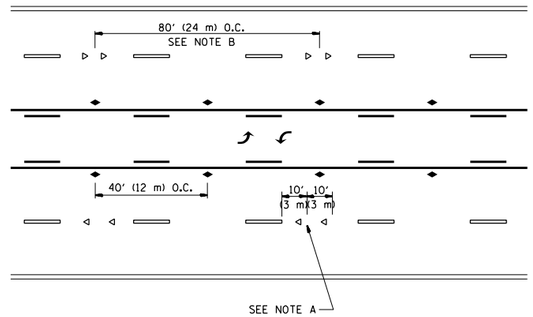


* * * REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

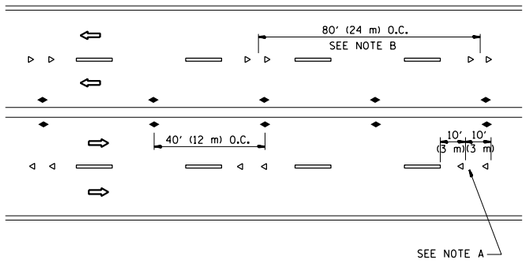
TWO-LANE/TWO-WAY



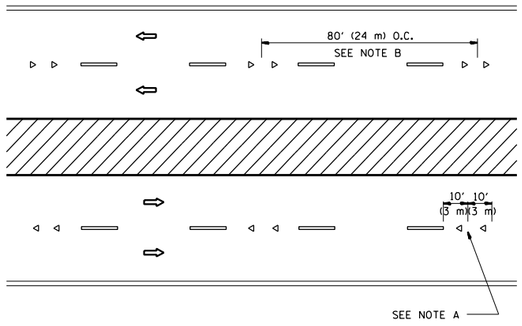
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



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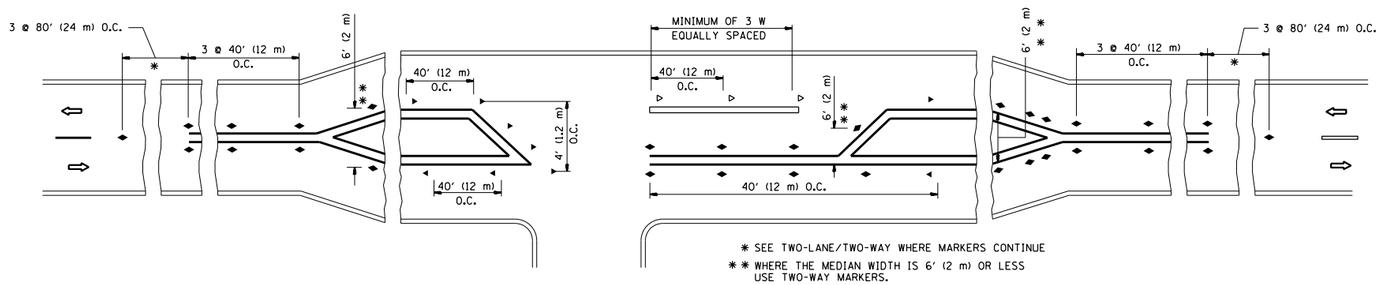


* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE

** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

LEFT TURN

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

LANE MARKER NOTES

A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

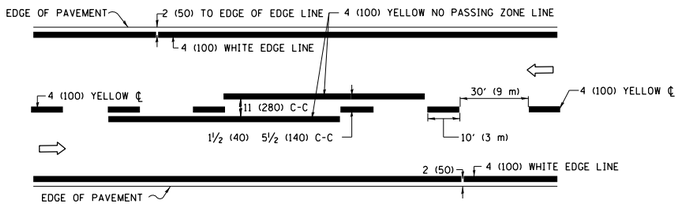
DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

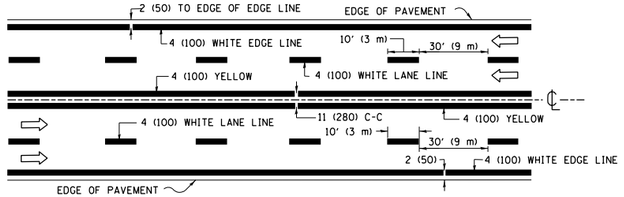
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = c:\pw_work\pwidot\qureshiya\d0283041\DistStd.dgn	USER NAME = qureshiya	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
	PLOT SCALE = 100.0002 ' / in.	DRAWN -	REVISED - T. RAMMACHER 03-12-99		
	PLOT DATE = 10/26/2016	CHECKED -	REVISED - T. RAMMACHER 01-06-00		
		DATE -	REVISED - C. JUCIUS 09-09-09		SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

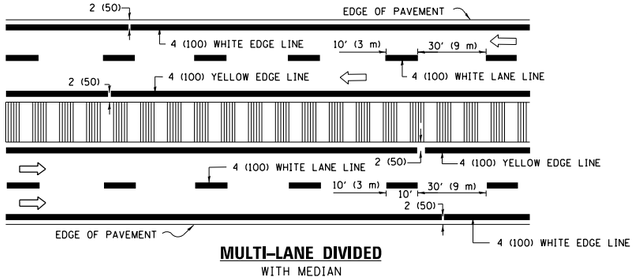
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	41
	TC-11	CONTRACT NO. 60X38		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



2–LANE ROADWAY

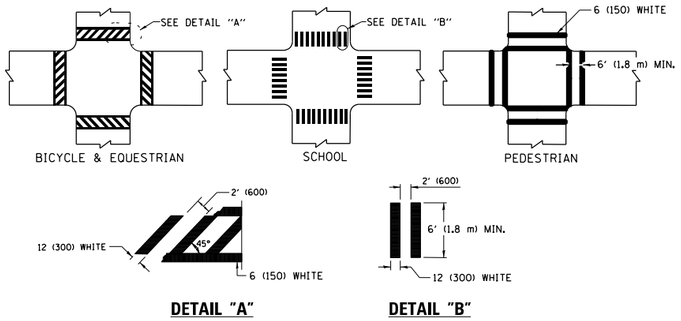


MULTI–LANE UNDIVIDED



MULTI–LANE DIVIDED
WITH MEDIAN

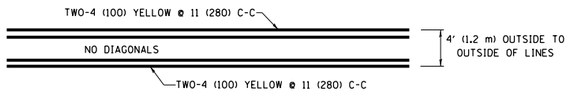
TYPICAL LANE AND EDGE LINE MARKING



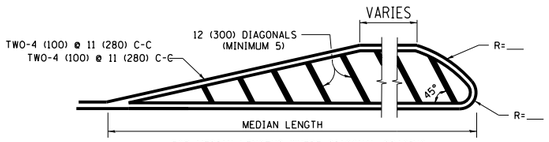
DETAIL "A" **DETAIL "B"**

TYPICAL CROSSWALK MARKING

✳ MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



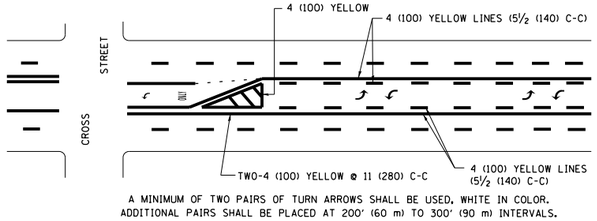
4' (1.2 m) WIDE MEDIANS ONLY



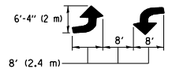
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

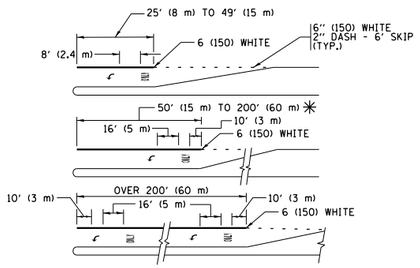


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO–WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

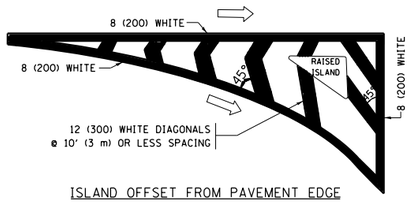


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
↰ AREA = 15.6 SQ. FT. (1.5 m^2) **ONLY** AREA = 20.8 SQ. FT. (1.9 m^2)

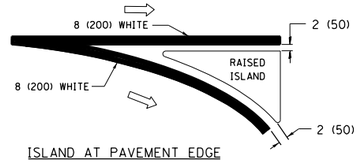
✳ TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

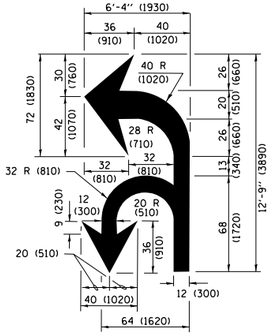


ISLAND OFFSET FROM PAVEMENT EDGE

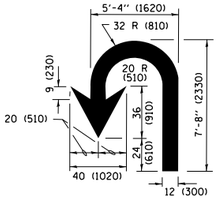


ISLAND AT PAVEMENT EDGE

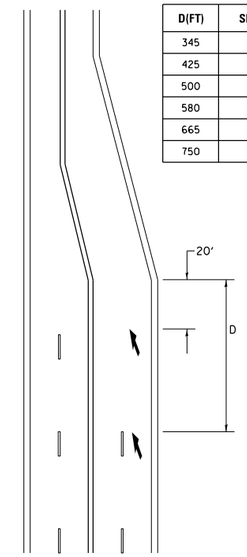
TYPICAL ISLAND MARKING



COMBINATION LEFT AND U–TURN



U–TURN



LANE REDUCTION TRANSITION

✳ LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 ⌀ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 ⌀ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 ⌀ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 ⌀ 6 (150) 12 (300) ⌀ 45° 12 (300) ⌀ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 ⌀ 4 (100) WITH 12 (300) DIAGONALS ⌀ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS ⌀ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m^2) EACH "X"=54.0 SQ. FT. (5.0 m^2)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) ⌀ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = c:\pw_work\pwidot\qureshiya\d0283041\Dist\Std.dgn Default	USER NAME = qureshiya	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
		DRAWN -	REVISED - C. JUCIUS 07-01-13
	PLOT SCALE = 100.0002 ' / in.	CHECKED -	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 10/26/2016	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	42
	TC-13	CONTRACT NO. 60X38		
	ILLINOIS FED. AID PROJECT			

DistStd.dgn 10/26/2016 8:17:37 AM User=qureshiya

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

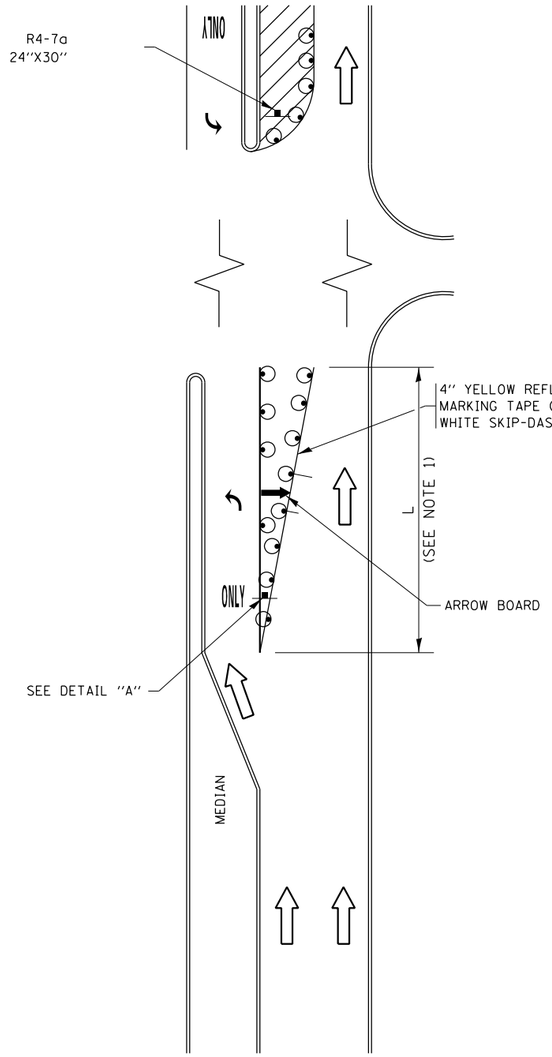


FIGURE 1

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

1. A) WHEN ''L'' IS ≤ THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.

   B) WHEN ''L'' IS > THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.

2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.

3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.

4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.

5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, ''RIGHT TURN LANE'' R3-I10OR 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.

6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.

7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.

8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

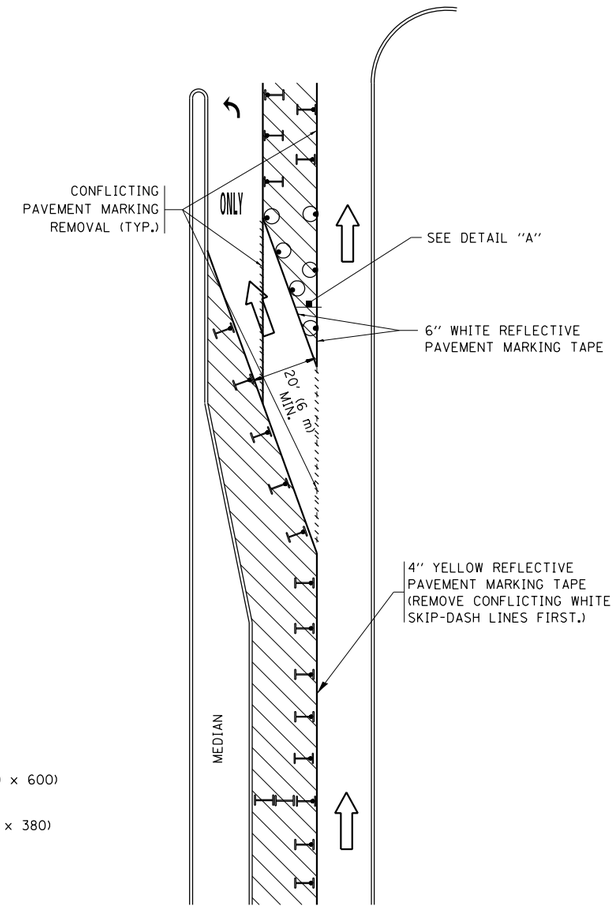
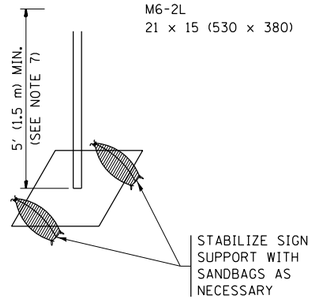


FIGURE 2



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

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	PLOT SCALE = 100.0002 ' / in.	REVISED - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13				1632	49R-RS	COOK	48	43
Default	PLOT DATE = 10/26/2016	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16					TC-14	CONTRACT NO. 60X38		
		REVISED - T. RAMMACHER 01-06-00	REVISED -		SCALE: NONE	SHEET 1 OF 1 SHEETS STA. TO STA.		ILLINOIS	FED. AID PROJECT		

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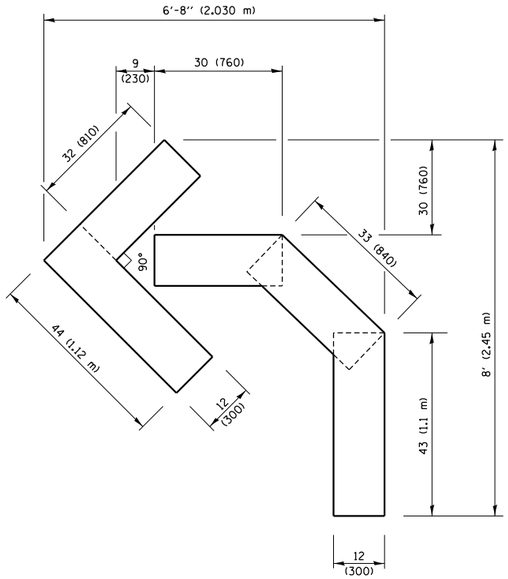
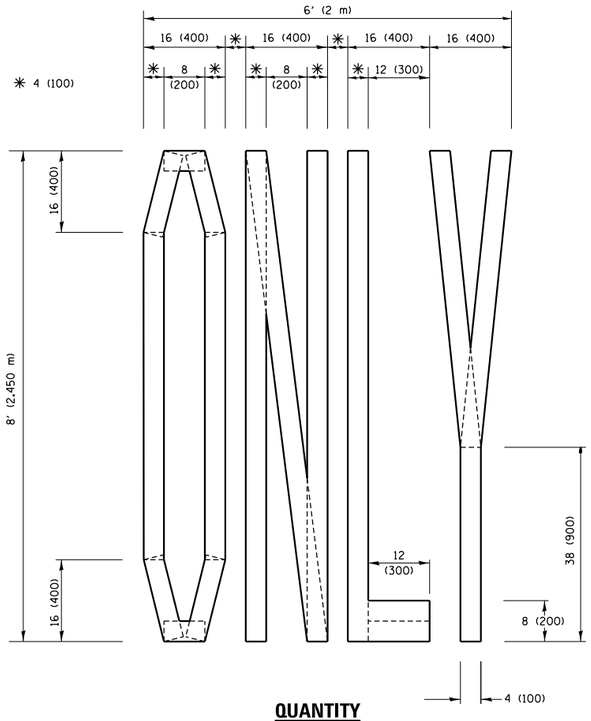


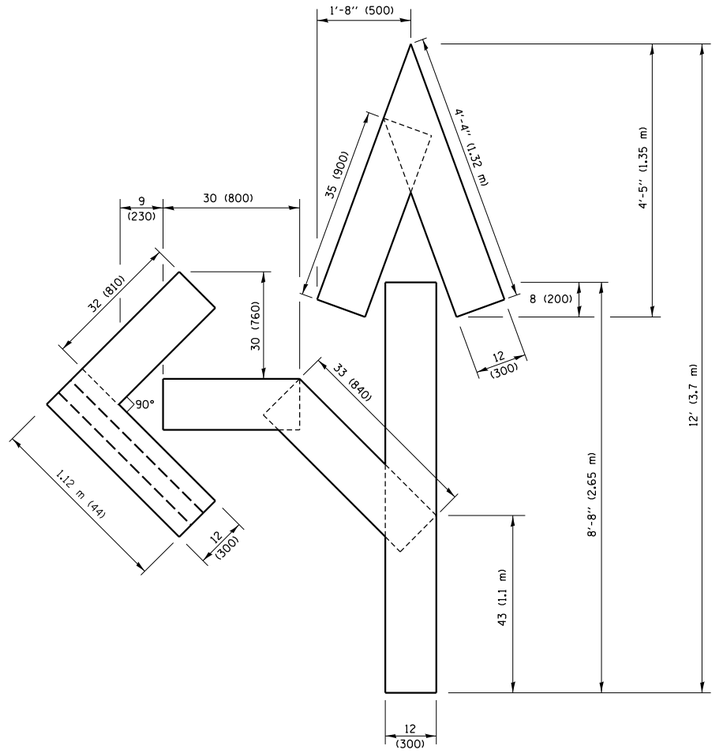
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

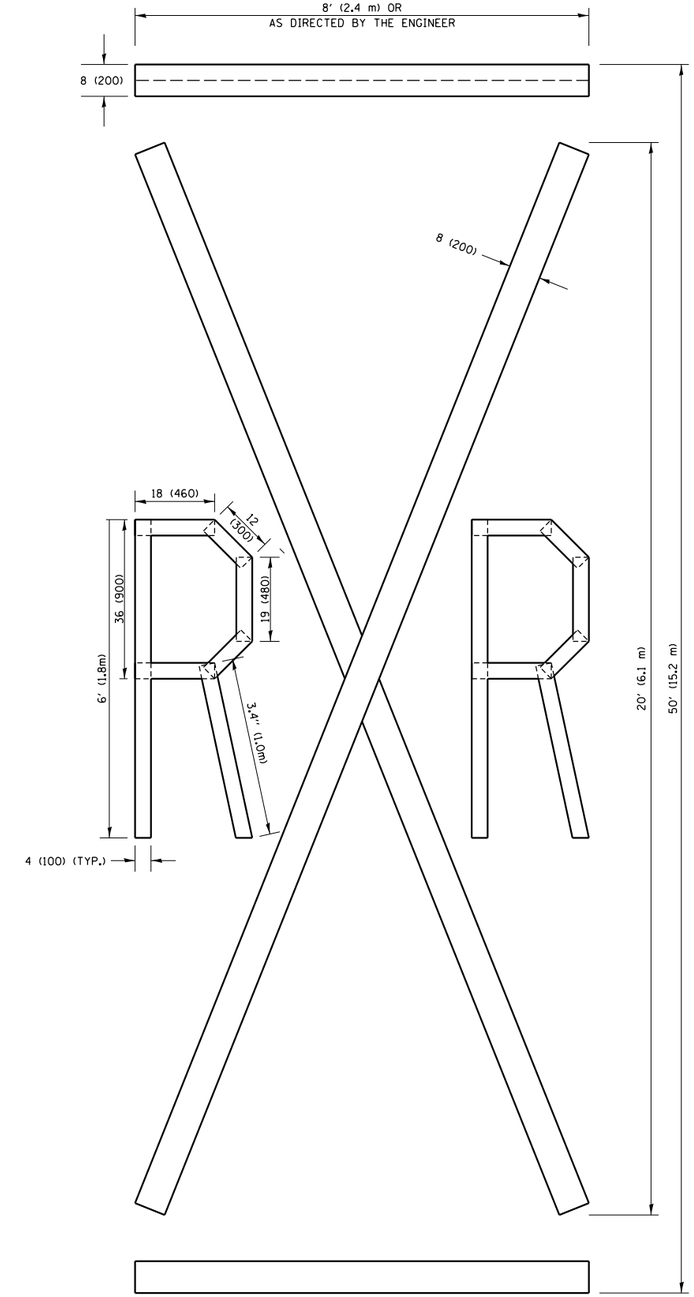


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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		DRAWN -	REVISED -E. GOMEZ 08-28-00
	PLOT SCALE = 100.0010 ' / in.	CHECKED -	REVISED -E. GOMEZ 08-28-00
	PLOT DATE = 10/26/2016	DATE - 09-18-94	REVISED -A. SCHUETZE 09-15-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	44
	TC-16	CONTRACT NO. 60X38		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

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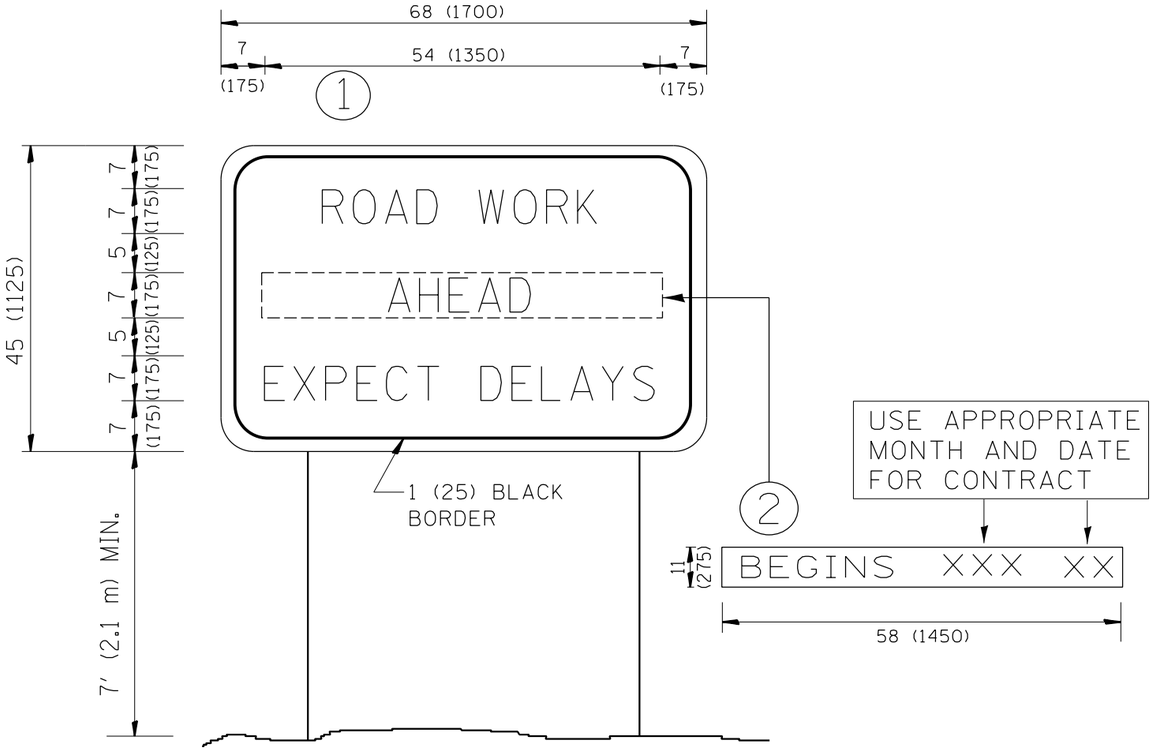

NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE ''ROAD CONSTRUCTION AHEAD'' SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR ''TEMPORARY INFORMATION SIGNING'' FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 10/26/2016	DATE -	REVISED - C. JUCIUS 01-31-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD INFORMATION SIGN

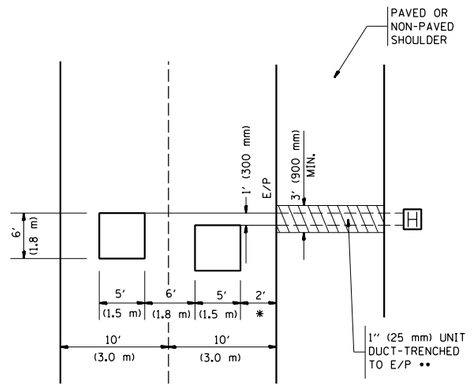
SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	45
	TC-22	CONTRACT NO. 60X38		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

DistStd.dgn 10/26/2016 8:20:11 AM User=qureshiya

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

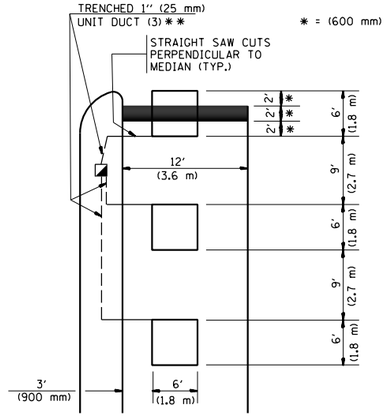


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

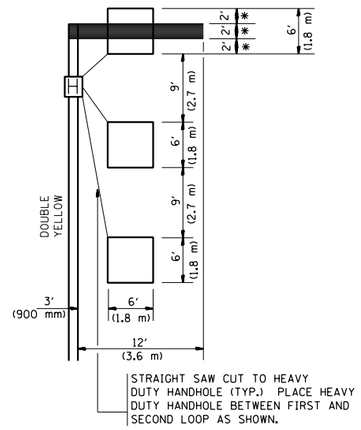


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

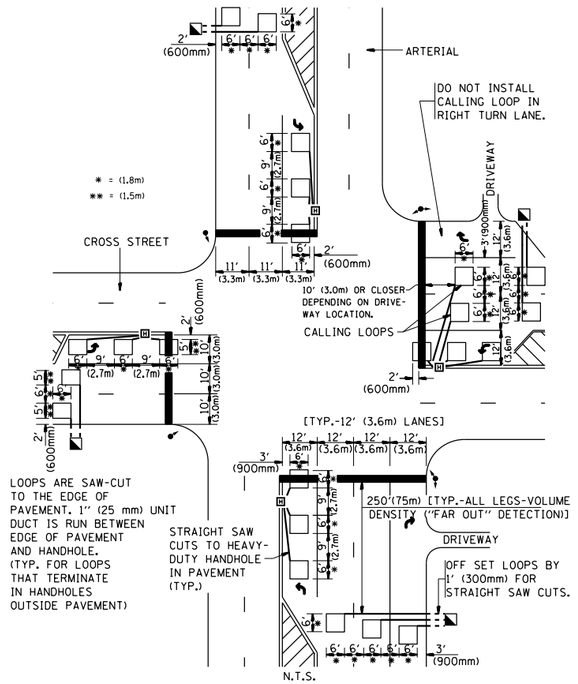
LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

* = (600 mm)



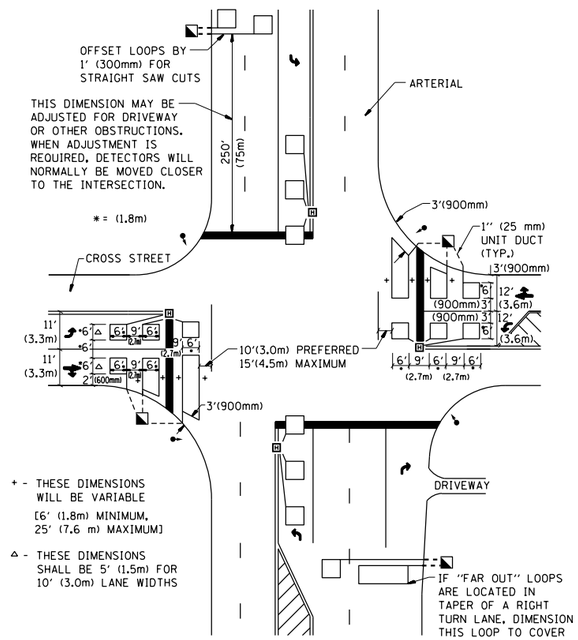
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

~~ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)~~ ~~CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)~~



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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		DRAWN -	REVISED -
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	PLOT DATE = 10/26/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 – DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

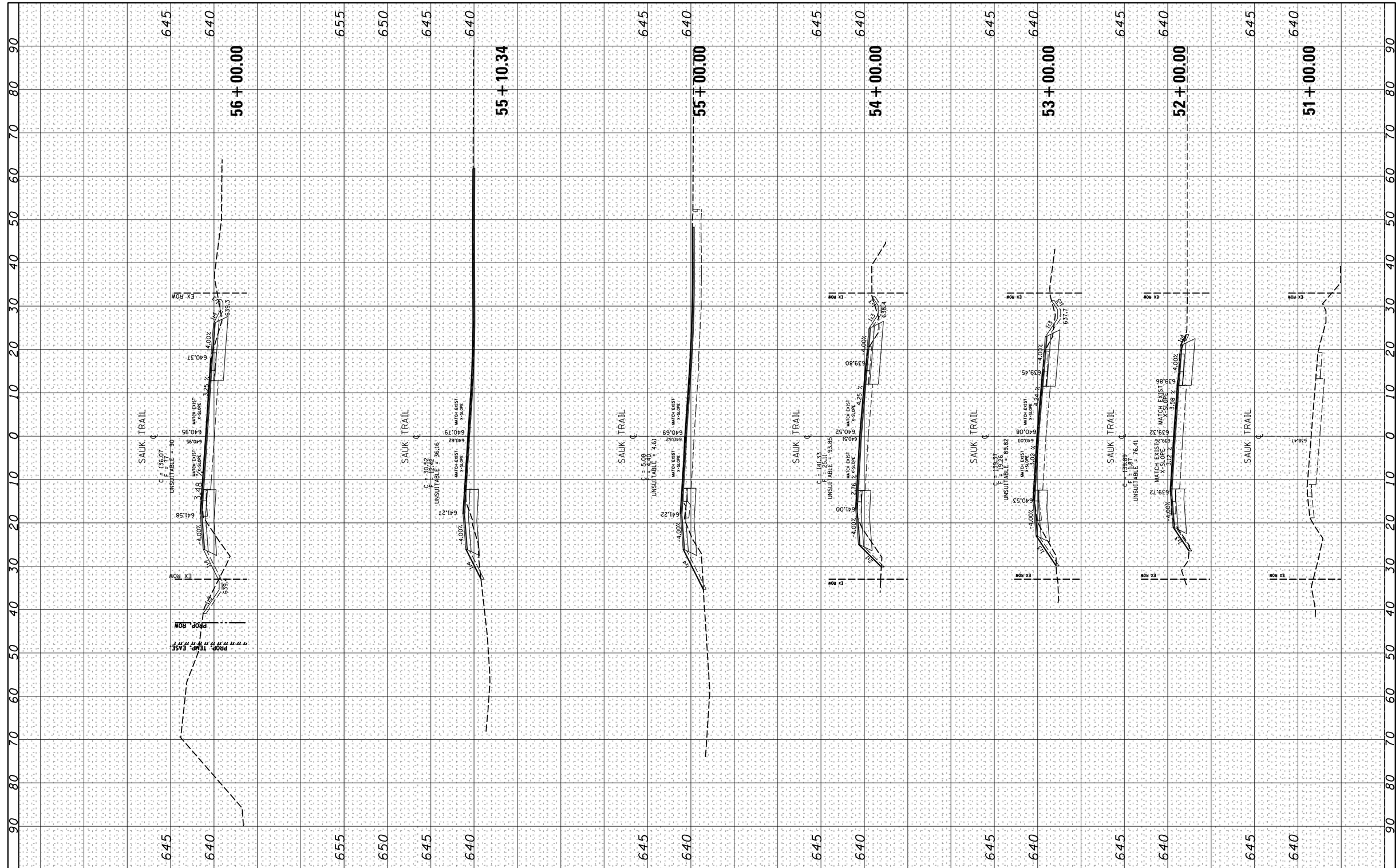
SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	46
	TS-07	CONTRACT NO. 60X38		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

DistStd.dgn 10/26/2016 8:20:25 AM User=qureshiya

FINAL SURVEY		BY	DATE
	SURVEYED		
	PLOTTED		
NOTE BOOK	TEMPLATE		
	AREAS		
NO.	AREAS CHECKED		

ORIGINAL SURVEY		BY	DATE
	SURVEYED		
	PLOTTED		
NOTE BOOK	TEMPLATE		
	AREAS		
NO.	AREAS CHECKED		



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		DRAWN -	REVISED -
	PLOT SCALE = 20.0000 ' / in.	CHECKED -	REVISED -
	PLOT DATE = 10/31/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
SAUK TRAIL AT BURNHAM AVE.

SCALE: | SHEET OF SHEETS | STA. 48+00.00 | TO STA. 55+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	47
		CONTRACT NO. 60X38		
	ILLINOIS FED. AID PROJECT			

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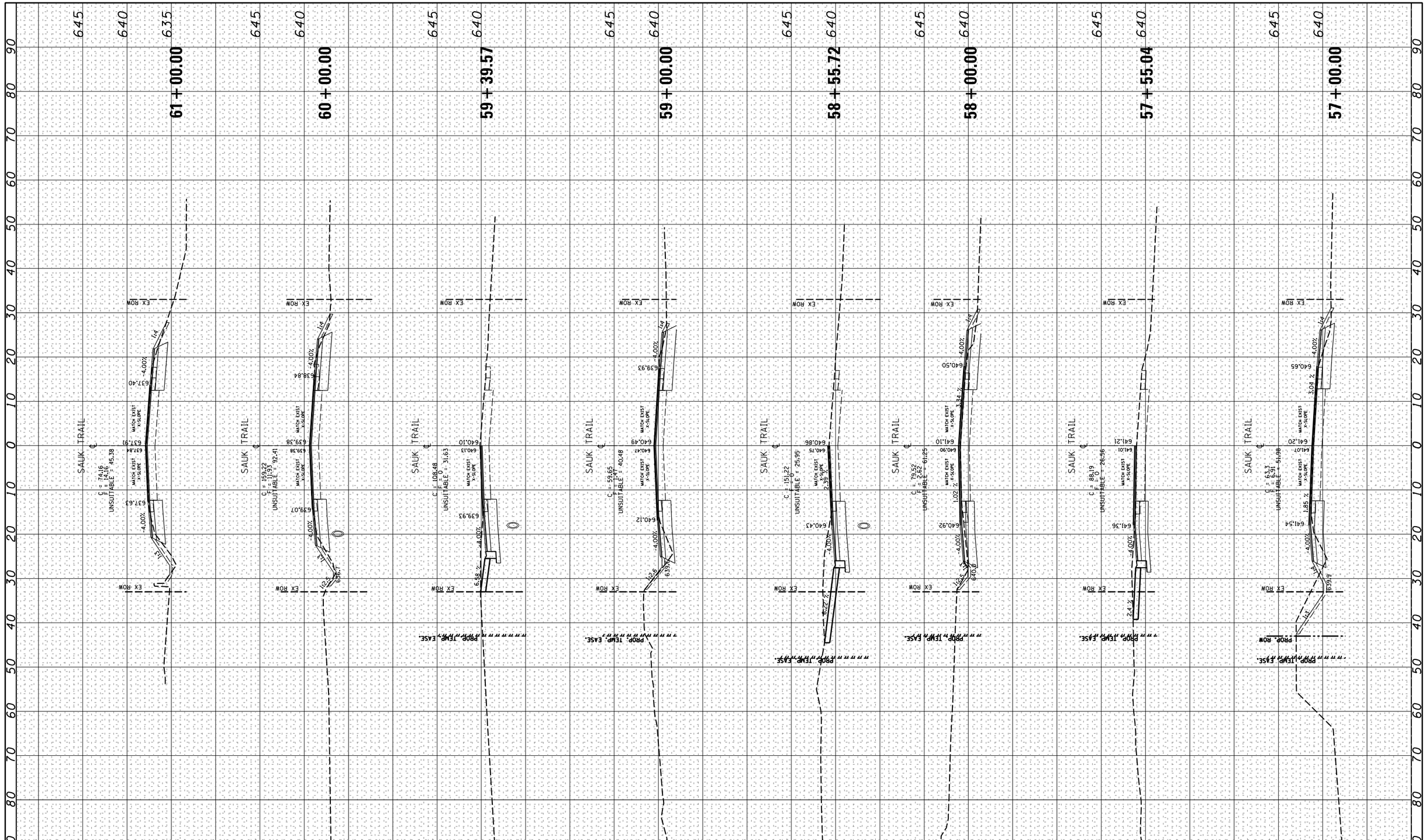
FINAL SURVEY		BY	DATE
	SURVEYED		
	PLOTTED		
NOTE BOOK	TEMPLATE		
	AREAS		
NO.	AREAS CHECKED		

ORIGINAL SURVEY		BY	DATE
	SURVEYED		
	PLOTTED		
NOTE BOOK	TEMPLATE		
	AREAS		
NO.	AREAS CHECKED		



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	PLOT SCALE = 20.0000 ' / in.	CHECKED -	REVISED -
	PLOT DATE = 10/31/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
SAUK TRAIL AT BURNHAM AVE.

SCALE: | SHEET OF SHEETS | STA. 55+10.34 | TO STA. 59+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1632	49R-RS	COOK	48	48
		CONTRACT NO. 60X38		
	ILLINOIS FED. AID PROJECT			

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