

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	09-00057-00-CH	COOK	103	1
FED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT NO. 63523	

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FOR INDEX OF SHEETS, SEE SHEET NO. 2

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PLANS FOR PROPOSED
FEDERAL AID HIGHWAY**

FAP ROUTE 344 (IL ROUTE 83 - BUSSE ROAD)
AT PRATT BOULEVARD
SECTION 09-00057-00-CH
PROJECT M-9003(660)
ROADWAY RECONSTRUCTION AND
TRAFFIC SIGNAL MODERNIZATION
VILLAGE OF ELK GROVE VILLAGE
COOK COUNTY
C-91-617-10



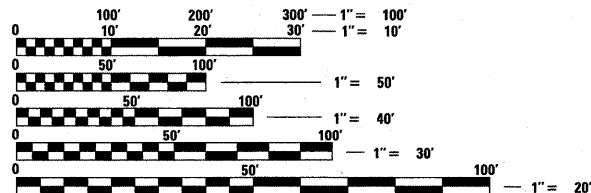
LOCATION OF SECTION INDICATED THIS: - [Black Rectangle] -

DESIGN SPEED:
IL ROUTE 83 - 50 MPH
PRATT BOULEVARD - 35 MPH

POSTED SPEED:
IL ROUTE 83 - 45 MPH
PRATT BOULEVARD - 30 MPH

DESIGN DESIGNATIONS:

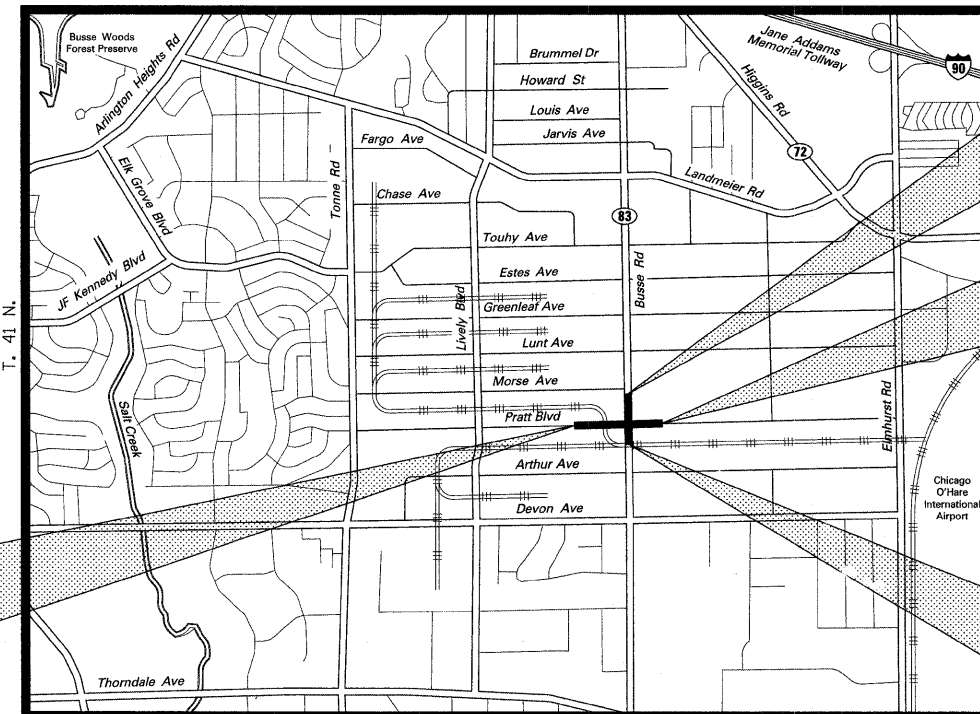
IL ROUTE 83 - 53,000 (2030) ARTERIAL 10.11 (HMA OVERLAY-15)
PRATT BOULEVARD - 9,000 (2030) COLLECTOR 5.08 (PCC-20)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123 OR 811

CONTRACT NO. 63523



IMPROVEMENT BEGINS
STA. 401+27.62
PRATT BOULEVARD

IMPROVEMENT ENDS
STA. 82+50.53
ILLINOIS ROUTE 83

IMPROVEMENT ENDS
STA. 415+68.82
PRATT BOULEVARD

IMPROVEMENT BEGINS
STA. 70+95.10
ILLINOIS ROUTE 83

PROJECT LENGTH:
IL ROUTE 83 - 1,155.43 FT. (0.219 MILE) (NET AND GROSS)
PRATT BOULEVARD - 1,441.20 FT. (0.273 MILE) (NET AND GROSS)
TOTAL - 2,596.63 FT (0.492 MILE) (NET AND GROSS)

LOCATION MAP
NOT TO SCALE

3RD P.M., ELK GROVE TOWNSHIP



SIGNED [Signature] DATE 10-21-10 EXPIRES 11-30-2011

FOR DRAWINGS 1-51, 70-103



SIGNED [Signature] DATE 10/21/10 EXPIRES 11-30-2011

FOR DRAWINGS 52-64

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED OCTOBER 21 2010
Mary Jo Pyle
ACTING DIRECTOR OF ENGINEERING AND COMMUNITY DEVELOPMENT

PASSED NOVEMBER 4 2010
Chet Christoffel
DISTRICT ONE ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID
BASED ON LIMITED
REVIEW November 4 2010
D. Mall
DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**



SIGNED [Signature] EXPIRES 11-30-2011

DATE 10/21/10 FOR DRAWINGS 65-69



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Tel: 630.773.3900 - Fax: 630.773.3975
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PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. (847) 705-4406
CONSULTANT ENGINEER: DAVID KREEGER, P.E. CIVILTECH ENGINEERING, INC.

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SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS

- ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ("STANDARD SPECIFICATIONS"), ADOPTED JANUARY 1, 2007; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2011; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", (IMUTCD); "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" JULY 2009 SIXTH EDITION, THE DETAILS IN THE PLANS, AND THE SPECIAL PROVISIONS AND IDOT STANDARD DRAWINGS INCLUDED IN THE CONTRACT DOCUMENTS.
- NO WORK SHALL COMMENCE UNTIL TRAFFIC CONTROL REQUIREMENTS ARE MET AND APPROPRIATE PERMITS HAVE BEEN OBTAINED.
- THE ENGINEER AND ALL UTILITY COMPANIES, SCHOOL DISTRICTS, AND LOCAL POLICE AND FIRE DEPARTMENTS SHALL BE NOTIFIED BY THE CONTRACTOR AT LEAST 72 HOURS PRIOR TO THE START OF CONSTRUCTION.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.
- WHEN REMOVING CURB AND GUTTER, PAVEMENT OR ANY OTHER STRUCTURE, THE CONTRACTOR SHALL TAKE PRECAUTIONS NECESSARY TO AVOID DAMAGE TO UNDERGROUND PUBLIC OR PRIVATE UTILITIES IN ACCORDANCE WITH ARTICLES 105.07, 107.20, AND 107.31. UNDER NO CIRCUMSTANCES WILL THE USE OF A FROST BALL CONCRETE BREAKER BE ALLOWED.

6. THE CONTRACTOR IS PROHIBITED FROM BURNING ANY MATERIAL WITHIN OR ADJACENT TO THE PROJECT LIMITS. ALL EXCESS OR WASTE MATERIAL SHALL BE EITHER HAULED AWAY FROM THE PROJECT SITE BY THE CONTRACTOR AND DEPOSITED AT LOCATIONS PROVIDED BY HIM, OR DISPOSED OF WITHIN THE RIGHT-OF-WAY IN A MANNER OTHER THAN BURNING, SUBJECT TO THE APPROVAL OF THE ENGINEER. NO EXTRA COMPENSATION WILL BE ALLOWED THE CONTRACTOR FOR ANY EXPENSE INCURRED BY COMPLYING WITH THE REQUIREMENTS OF THIS NOTE.

PAVING, SHOULDERS, CURB & GUTTER AND SIDEWALK

1. THE CONTRACTOR SHALL SAW CUT PAVEMENT, CURB & GUTTER, MEDIAN, SHOULDER, AND SIDEWALK AS INDICATED ON THE PLANS TO SEPARATE THE EXISTING MATERIAL TO BE REMOVED BY MEANS OF AN APPROVED CONCRETE SAW TO A DEPTH AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED.

THE CONTRACTOR SHALL BE REQUIRED TO SAW VERTICAL CUTS SO AS TO FORM CLEAN VERTICAL JOINTS. SHOULD THE CONTRACTOR DEFACE ANY EDGE, A NEW SAWED JOINT SHALL BE PROVIDED AND ANY ADDITIONAL WORK, INCLUDING REMOVAL AND REPLACEMENT, SHALL BE DONE AT THE CONTRACTOR'S EXPENSE.

2. REMOVAL OF ALL REINFORCEMENT WITHIN THE EXISTING PAVEMENT SHALL BE INCLUDED IN THE COST OF "PAVEMENT REMOVAL".

3. HOT-MIX ASPHALT BINDER COURSE SHALL NOT BE PLACED ADJACENT TO CURB AND GUTTER UNTIL THE CURB AND GUTTER HAS BEEN PROPERLY CURED AND BACKFILLED TO THE SATISFACTION OF THE ENGINEER.

4. HOT-MIX ASPHALT SURFACE COURSE SHALL NOT BE PLACED UNTIL ALL EARTH EXCAVATION, TOPSOIL PLACEMENT, AND HOT-MIX ASPHALT BINDER COURSE HAVE BEEN COMPLETED TO THE SATISFACTION OF THE ENGINEER.

5. THE THICKNESSES OF HOT-MIX ASPHALT MIXTURES SHOWN ON THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACE, BINDER, OR BASE UPON WHICH THE HOT-MIX ASPHALT MATERIALS ARE PLACED.

6. ALL RAISED REFLECTIVE PAVEMENT MARKERS IN THE RESURFACING AREA SHALL BE REMOVED PRIOR TO MILLING AND REPLACED WHEN PAVING IS COMPLETE. THE REMOVAL OF ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PAID FOR AS "RAISED REFLECTIVE PAVEMENT MARKER REMOVAL".

7. ALL SIDEWALK CONSTRUCTED OVER A UTILITY TRENCH SHALL BE REINFORCED WITH THREE #4 REBARS WHICH EXTEND 5 FEET BEYOND THE TRENCH WALLS. AT LOCATIONS WHERE THE SIDEWALK IS ADJACENT TO THE BACK OF CURB, A 1" PREFORMED EXPANSION JOINT FILLER SHALL BE INSTALLED BETWEEN THE CURB AND SIDEWALK. THIS WORK SHALL BE INCLUDED IN THE COST OF "PCC SIDEWALK 5 INCH".

8. ALL FORMS USED FOR SIDEWALK SHALL BE 2" X 6" LUMBER, 2" X 8" LUMBER, OR APPROVED METAL FORMS, AND ALL FORMS USED FOR DRIVEWAY PAVEMENT SHALL BE 2" X 8" LUMBER OR APPROVED METAL FORMS, EXCEPT WITHIN AREAS WITH RADII WHEN 1" X 6" FORMS SHALL BE UTILIZED. THE ENGINEER MUST INSPECT AND APPROVE THE BASE AND FORMWORK BEFORE ANY CONCRETE IS POURED. A MINIMUM 24 HOUR NOTICE SHALL BE PROVIDED FOR FORM WORK INSPECTION. THIS WORK SHALL BE INCLUDED IN THE COST OF THE SIDEWALK OR DRIVEWAY PAVEMENT BEING CONSTRUCTED.

9. THE EXISTING CURB AND GUTTER SHALL BE SAW CUT AT EACH LIMIT OF REMOVAL AND THE NEW CURB AND GUTTER SHALL BE TIED IN TO THE EXISTING WITH TWO 1/4 INCH STEEL DOWEL BARS (18" LONG) DRILLED INTO THE EXISTING CURB AND GUTTER END. THE ENGINEER MUST INSPECT AND APPROVE THE BASE AND FORMWORK BEFORE ANY CONCRETE IS POURED. A MINIMUM 24 HOUR NOTICE SHALL BE PROVIDED FOR FORM WORK INSPECTION. THIS WORK SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER BEING CONSTRUCTED.

TREE REMOVAL, CLEARING AND HEDGE REMOVAL

- THE CONTRACTOR'S ATTENTION IS CALLED TO THE FACT THAT THE PRESERVATION OF EXISTING TREES IS OF THE UTMOST IMPORTANCE TO THE VILLAGE. ALL TREE PROTECTION, TREE REMOVAL, TREE PRUNING AND ROOT PRUNING SHALL BE COMPLETED BEFORE CONSTRUCTION OPERATIONS COMMENCE IN ANY AREA. AT NO TIME SHALL THE CONTRACTOR PRUNE OR REMOVE ANY TREES UNLESS SPECIFICALLY DIRECTED BY THE ENGINEER.
- TEMPORARY FENCE SHALL BE ERECTED ALONG THE DRIP LINE OF EXISTING TREES TO REMAIN WHEN DIRECTED BY THE ENGINEER. AFTER TREES ARE SAFELY FENCED NOTHING IS TO BE STORED, DRIVEN, OR DISTURBED INSIDE THE FENCE. REMOVE PROTECTIVE TEMPORARY FENCE ONLY AFTER ALL CONSTRUCTION WORK HAS BEEN COMPLETED.

ROADWAY EXCAVATION

1. POROUS GRANULAR EMBANKMENT, SUBGRADE (PGES) HAS BEEN INCLUDED IN THE CONTRACT TO REPLACE SOILS WHICH TEND TO BE UNSTABLE WHEN WET. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH PGES WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE ENGINEER. IF UNSUITABLE SOILS ARE ENCOUNTERED THE SOILS SHALL BE REMOVED AND REPLACED WITH PGES. THESE LIMITS MAY BE ALTERED BY THE ENGINEER IF FIELD CONDITIONS SO WARRANT. REMOVAL OF THESE UNSUITABLE SOILS SHALL BE PAID FOR AS "REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL."

2. THE QUANTITIES OF FURNISHED EXCAVATION HAVE BEEN CALCULATED ASSUMING THAT ALL MATERIAL EXCAVATED UNDER THE PAY ITEM EARTH EXCAVATION WILL BE REMOVED FROM THE JOB SITE. IF THE CONTRACTOR EXCAVATES SUITABLE MATERIAL AND PLACES IT IN AREAS OF THE PROJECT REQUIRING EMBANKMENT UNDER THE PAY ITEM EARTH EXCAVATION, AS DESCRIBED IN SECTION 202 OF THE STANDARD SPECIFICATIONS AND AS APPROVED BY THE ENGINEER, THE APPLICABLE DEDUCTION TO THE FURNISHED EXCAVATION QUANTITY SHALL BE MADE AS DEFINED BY ARTICLE 204.07(B), EXCEPT THAT A SHRINKAGE FACTOR OF 15% SHALL BE USED. THE CONTRACTOR SHALL NOT BE ALLOWED A CHANGE IN THE UNIT PRICES FOR EARTH EXCAVATION OR FURNISHED EXCAVATION BASED ON THESE CHANGES TO THE QUANTITIES. THE VOLUMES OF FURNISHED EXCAVATION SHOWN ON THE PLANS ARE THE COMPACTED VOLUMES. THE VOLUMES SHOWN ON THE PLANS HAVE NOT BEEN ADJUSTED TO ACCOUNT FOR SHRINKAGE DUE TO COMPACTION.

3. USE OF CCDD FILL OPERATIONS - IF THE CONTRACTOR CHOOSES TO DISPOSE OF UNCONTAMINATED SOIL OR UNCONTAMINATED SOIL MIXED WITH CLEAN CONSTRUCTION AND DEMOLITION DEBRIS (CCDD) AT A CCDD FILL OPERATION, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PERFORM ALL NECESSARY FIELD AND LABORATORY ANALYSIS AND TO OBTAIN THE LICENSED PROFESSIONAL ENGINEER'S CERTIFICATION REQUIRED AS PER PUBLIC ACT 96-1416 TO USE THE SITE. NO ADDITIONAL COMPENSATION WILL BE PROVIDED.

UTILITIES

- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES. THE LOCATION OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE ENGINEER DOES NOT GUARANTEE THEIR ACCURACY.
- COORDINATION OF ANY UTILITY WORK INVOLVED IN THE CONSTRUCTION AREA WILL BE DISCUSSED AT THE PRECONSTRUCTION CONFERENCE.
- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS, WATER, SEWER AND CABLE TELEVISION FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED.)

4. WHENEVER DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS. THE COST OF ALL MATERIALS REQUIRED AND ALL LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE COST OF THE STORM SEWERS AND DRAINAGE STRUCTURES INSTALLED AS PART OF THIS PROJECT.

- ANY EXISTING OR PROPOSED SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AT NO COST TO THE DEPARTMENT, IN ACCORDANCE WITH ARTICLES 105.07, 107.20, AND 107.31.
- THE CONTRACTOR SHALL RECEIVE NO ADDITIONAL COMPENSATION FOR CONSTRUCTION STAGING NECESSARY TO ACCOMMODATE UTILITY RELOCATION OR ADJUSTMENT CAUSED BY UTILITY RELOCATION OR ADJUSTMENT.

7. THE CONTRACTOR SHALL FURNISH ALL LABOR, EQUIPMENT AND MATERIAL NECESSARY FOR DEWATERING TRENCH EXCAVATIONS AS WELL AS SHORING TRENCH WALLS DURING UTILITY OPERATIONS. THE COST TO COMPLY WITH THE ABOVE SHALL BE INCLUDED IN THE COST OF THE STORM SEWERS AND DRAINAGE STRUCTURES INSTALLED AS PART OF THIS PROJECT.

8. SANITARY SEWER REQUIRED FOR THE ITEM "ADJUSTING SANITARY SEWERS, 8-INCH DIAMETER OR LESS" SHALL MEET THE FOLLOWING MATERIAL REQUIREMENTS: PIPE SHALL BE PVC PLASTIC PIPE. ALL PIPE AND FITTINGS SHALL CONFORM TO ASTM D3034 SDR 26 PS=115. THE SDR SHALL BE 26 AS A MINIMUM. ALL PIPE SHALL BE MADE FROM QUALITY PVC RESIN, COMPOUNDED TO PROVIDE PHYSICAL AND MECHANICAL PROPERTIES THAT EQUAL OR EXCEED CELL CLASS 12454 AS DEFINED IN ASTM D1784.

FILE NAME = ...\\2363\cad\sheet\2363_notes.dgn	USER NAME = jat	DESIGNED - JAT	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES	F.A.P. RTE. 334	SECTION 09-00057-00-CH	COUNTY COOK	TOTAL SHEETS 103	SHEET NO. 2	CONTRACT NO. 63523	
PLOT SCALE = 50.0000' / IN.	CHECKED - DJK	REVISED -										
PLOT DATE = 10/21/2010	DATE - 10-22-10	REVISED -										
						SHEET NO. 1 OF 2 SHEETS						
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003660												

STORM & SANITARY SEWER

1. THE COST OF MAKING SEWER CONNECTIONS TO EXISTING OR PROPOSED SEWER OR DRAINAGE STRUCTURES SHALL BE INCLUDED IN THE COST OF THE SEWER OR STRUCTURE BEING CONSTRUCTED.
2. UNLESS OTHERWISE NOTED ON THE PLANS, THE EXISTING DRAINAGE FACILITIES SHALL REMAIN IN USE DURING THE PERIOD OF CONSTRUCTION. LOCATIONS OF EXISTING DRAINAGE STRUCTURES AND SEWERS AS SHOWN ON THE PLANS ARE APPROXIMATE. PRIOR TO COMMENCING WORK THE CONTRACTOR, AT HIS OWN EXPENSE, SHALL DETERMINE THE EXACT LOCATIONS OF EXISTING STRUCTURES WHICH ARE WITHIN THE PROPOSED CONSTRUCTION LIMITS.

DURING CONSTRUCTION, IF THE CONTRACTOR ENCOUNTERS OR OTHERWISE BECOMES AWARE OF ANY SEWERS, UNDERDRAINS OR FIELD DRAINS WITHIN THE RIGHT-OF-WAY OTHER THAN THOSE SHOWN ON THE PLANS, HE SHALL SO INFORM THE ENGINEER, WHO SHALL DIRECT THE WORK NECESSARY TO MAINTAIN OR REPLACE THE FACILITIES IN SERVICE AND TO PROTECT THEM FROM DAMAGE DURING CONSTRUCTION IF MAINTAINED. EXISTING FACILITIES TO BE MAINTAINED THAT ARE DAMAGED BECAUSE OF THE NON-COMPLIANCE WITH THIS PROVISION SHALL BE REPLACED AT THE CONTRACTOR'S OWN EXPENSE. SHOULD THE ENGINEER HAVE DIRECTED THE REPLACEMENT OF A FACILITY, THE NECESSARY WORK AND PAYMENT SHALL BE IN ACCORDANCE WITH SECTIONS 550 AND 601, AND ARTICLE 104.02 OF THE STANDARD SPECIFICATIONS.
3. WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE OR PUBLIC DRAINS, SEWERS OR CATCH BASINS. THE CONTRACTOR SHALL PROVIDE FACILITIES TO TAKE IN ALL STORM WATER WHICH WILL BE RECEIVED BY THESE DRAINS AND SEWERS AND DISCHARGE THE SAME. HE SHALL PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT, IF NECESSARY, AND A TEMPORARY OUTLET. HE SHALL BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER RECEIVED FROM TEMPORARY CONNECTIONS UNTIL SUCH TIME AS THE PERMANENT CONNECTIONS WITH SEWER ARE BUILT AND IN SERVICE. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF THE STORM SEWERS AND DRAINAGE STRUCTURES INSTALLED AS PART OF THIS PROJECT.
4. TOP OF FRAME ("RIM") ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF EACH STRUCTURE. FRAMES ON ALL NEW STRUCTURES SHALL BE ADJUSTED TO THE FINAL ELEVATIONS OF THE AREAS IN WHICH THEY ARE LOCATED, AS PART OF THE STRUCTURE COST.
5. DRAINAGE STRUCTURE FLAT-TOPS AND CONES SHALL BE TURNED SO THAT THE FRAMES ARE CLOSEST TO THE CENTERLINE OF THE LANE. ALL FLAT-TOPS AND CONES ARE ASSUMED TO BE ECCENTRIC.
6. ALL SEWER AND WATER SERVICES CROSSED BY NEW STORM SEWERS SHALL BE PROPERLY LOCATED AND PROTECTED DURING CONSTRUCTION. ANY DAMAGE TO SAID SERVICES NOT CONSIDERED TO BE IN CONFLICT WITH THE PROPOSED STORM SEWER SHALL BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE.
7. THE REMOVAL OF END SECTIONS SHALL BE PAID FOR PER FOOT AS "STORM SEWER REMOVAL" OF THE DIAMETER INDICATED.
8. ONLY METHOD 1 UNDER SECTION 550.07 OF THE STANDARD SPECIFICATIONS SHALL BE ALLOWED FOR THE PLACEMENT OF TRENCH BACKFILL.
9. THE CONTRACTOR SHALL BE AWARE THAT AT TIMES THE ENGINEER MAY REQUIRE A CHANGE IN STORM SEWER ELEVATION DUE TO A UTILITY LINE OR OTHER OBSTRUCTION. IF SUCH A GRADE CHANGE DOES NOT ALTER THE PIPE CLASSIFICATION, THE ADDITIONAL EXCAVATION OR SHEETING REQUIRED SHALL BE INCLUDED IN THE COST OF THE STORM SEWER BEING INSTALLED. IF THE REVISED GRADE RESULTS IN A CHANGE IN PIPE CLASSIFICATION, PAYMENT WILL BE MADE FOR THE REVISED TYPE OF STORM SEWER.
10. ALL ABANDONED PIPE AND STRUCTURE INVERTS SHALL BE PLUGGED WITH CONCRETE BRICKS AND NON-SHRINK MORTAR FOR A LENGTH OF 2' MEASURED FROM THE END OF THE PIPE. THIS WORK SHALL BE INCLUDED IN THE COST OF THE STORM OR SANITARY SEWER ITEMS BEING REMOVED.

LANDSCAPING

1. WHEN DIRECTED BY THE ENGINEER, SUPPLEMENTAL WATERING SHALL BE APPLIED TO ALL SODDED AREAS PRIOR TO FINAL ACCEPTANCE AT A RATE SPECIFIED BY THE ENGINEER.
2. THE CONTRACTOR SHALL ADHERE TO LIMITS OF RESTORATION SHOWN. AREAS OUTSIDE THESE LIMITS THAT ARE DAMAGED OR DISTURBED BY THE CONTRACTOR SHALL BE RESTORED BY THE CONTRACTOR AT HIS EXPENSE, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

EROSION CONTROL

1. ALL VEGETATIVE AND STRUCTURAL EROSION CONTROL PRACTICES SHALL BE CONSTRUCTED AND MAINTAINED IN ACCORDANCE WITH THE MINIMUM STANDARDS AND SPECIFICATIONS OF THE "ILLINOIS PROCEDURES AND STANDARDS FOR URBAN SOIL EROSION AND SEDIMENTATION CONTROL" AND THE "STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL" OF THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.
2. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, SITE CONDITIONS AND THE USE OF TEMPORARY OR PERMANENT MEASURES.
3. THE CONTRACTOR SHALL INSPECT ALL EROSION CONTROL MEASURES PERIODICALLY AND AFTER EACH RUNOFF-PRODUCING RAINFALL EVENT. ANY NECESSARY REPAIRS OR CLEANUP TO MAINTAIN THE EFFECTIVENESS OF SAID MEASURES SHALL BE MADE IMMEDIATELY.
4. ALL STORM SEWER FACILITIES THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED, FILTERED OR OTHERWISE TREATED TO REMOVE SEDIMENT. MUD AND SEDIMENT DEPOSITS SHALL BE REMOVED FROM THE ROADWAY AT THE END OF EACH WORK DAY BY SHOVELING AND/OR SWEEPING.
5. ALL SLOPES SHALL BE COVERED WITH SOD AS GRADING AND PLACEMENT OF TOPSOIL HAS BEEN COMPLETED. THE LIMITS OF THE SOD SHALL BE THE LIMITS OF GRADING.
6. INLET FILTERS SHALL BE PLACED ON ALL CATCH BASINS, INLETS, AND MANHOLES WITH OPEN GRATES IN THE CURB AND GUTTER AND SHOULDERS.
7. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER.
8. SEE STANDARD 280001-05 FOR ADDITIONAL SOIL EROSION AND SEDIMENT CONTROL DETAILS AND REQUIREMENTS.
9. WHEN A TOPSOIL STOCKPILE IS TO REMAIN IN PLACE FOR MORE THAN THREE DAYS, EROSION CONTROL MEASURES MEETING THE APPROVAL OF THE ENGINEER SHALL BE PROVIDED. THIS WORK SHALL BE PAID FOR AT THE UNIT PRICE FOR THE INDIVIDUAL ITEMS USED.
10. THE SURFACE OF ALL STRIPPED AREAS SHALL BE PERMANENTLY OR TEMPORARILY PROTECTED FROM SOIL EROSION WITHIN 14 DAYS AFTER FINAL GRADE IS REACHED. STRIPPED AREAS THAT WILL REMAIN UNDISTURBED FOR MORE THAN 14 DAYS AFTER INITIAL DISTURBANCE SHALL BE PROTECTED FROM EROSION WITH THE USE OF TEMPORARY EROSION CONTROL SEEDING. TEMPORARY SEDIMENT AND EROSION CONTROL MEASURES SHALL BE MAINTAINED CONTINUOUSLY UNTIL PERMANENT COVER IS ESTABLISHED.

MISCELLANEOUS COMMITMENTS

1. SITE OBJECTS: REMOVAL OF MISCELLANEOUS PARKWAY IMPROVEMENTS INCLUDING, BUT NOT LIMITED TO, BLOCK RETAINING WALLS, CONCRETE RETAINING WALLS, LANDSCAPE TIMBERS, LANDSCAPE ROCKS, FENCES, FENCE POSTS, PLANTERS, VEGETATION, BRICK OR BRICK PAVER WALKWAYS WITHIN R.O.W. LIMITS SHALL BE INCLUDED IN THE COST OF "EARTH EXCAVATION." THE CONTRACTOR SHALL CONTACT THE ADJACENT PROPERTY OWNER TO DETERMINE IF SUCH ITEMS SHALL BE RETURNED TO THE PROPERTY OWNER OR BE DISPOSED OF PROPERLY.
2. UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER, ALL EXISTING ACCESS POINTS SHALL BE MAINTAINED AT ALL TIMES BY THE CONTRACTOR.
3. THE CONTRACTOR SHALL NOT CROSS COMPLETED BINDER COURSE, OR EXISTING PAVEMENT NOT SCHEDULED TO BE REMOVED, WITH CONSTRUCTION EQUIPMENT WHICH MAY DAMAGE THE PAVEMENT.
4. THE RESIDENT ENGINEER SHALL CONTACT WALTER CZARNY, IDOT AREA TRAFFIC FIELD ENGINEER AT 847-705-8419 AT LEAST TWO (2) WEEKS PRIOR TO PLACING PERMANENT PAVEMENT MARKINGS.
5. THE RESIDENT ENGINEER SHALL CONTACT THE ENGINEER AND TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TRAFFIC CONTROL DEVICES.

STAKING

1. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE ENGINEER, HIS/HER AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
2. ALL RADII FOR PROPOSED CURB AND GUTTER ARE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
3. THE STATION/OFFSET/ELEVATIONS NOTED FOR ALL DRAINAGE STRUCTURES LOCATED IN THE CURB LINE REFER TO THE POSITION OF THE ADJACENT PROPOSED EDGE OF PAVEMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE OFFSET NECESSARY FOR EACH STRUCTURE TO SET THE FRAME AND GRATE IN THE PROPER LOCATION. ALL OTHER STRUCTURES ARE DIMENSIONED TO THE CENTER OF STRUCTURE.
4. PAVEMENT GRADES: THE ELEVATIONS INDICATED ON THE PLANS ARE FINISHED GRADES OF PROPOSED PAVEMENT, UNLESS OTHERWISE INDICATED.
5. ESTIMATED LOCATIONS OF SIDEWALK REMOVAL AND REPLACEMENT HAVE BEEN SHOWN ON THE PLANS. THE ENGINEER WILL DETERMINE THE EXACT LIMITS IN THE FIELD DURING CONSTRUCTION.
6. THE CONSTRUCTION BASELINE HAS BEEN ESTABLISHED FOR STAKING PURPOSES ONLY AND IS NOT INTENDED TO BE A CENTERLINE OF RIGHT-OF-WAY.

UTILITY CONTACTS

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AT&T
MICHAEL CARNEY
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OAKBROOK, IL 60523
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AT&T LOCAL NETWORK SERVICES
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LISLE, IL 60532
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THOMAS MUNAR
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MCI/VERIZON
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SUMMIT, IL 60501
708-458-6410

NICOR
CONSTANCE LANE
1844 FERRY ROAD
NAPERVILLE, IL 60563
PH. 630-983-8676

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	PLOT SCALE = 50.0000' / IN.	CHECKED - DJK	REVISED -			SHEET NO. 2 OF 2 SHEETS			CONTRACT NO. 63523		
	PLOT DATE = 10/21/2010	DATE - 10-22-10	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003660					

CODED PAY ITEM NO.	ITEM	UNIT	TOTAL QUANTITY	0004	0005	0021	0021	0021	0031	0043
				RECONSTRUCTION, NO CAPACITY ADDED	SYSTEM PRESERVATION, RESURFACING	SAFETY (TRAFFIC SIGNALS)	SAFETY (LIGHTING)	SAFETY (SIDEWALKS)	LANDSCAPING AND OTHER SCENIC BEAUTIFICATION	NON-PARTICIPATING
20101000	TEMPORARY FENCE	FOOT	400	400						
20101100	TREE TRUNK PROTECTION	EACH	10	10						
20101200	TREE ROOT PRUNING	EACH	10	10						
20101300	TREE PRUNING (1 TO 10 INCH DIAMETER)	EACH	9	9						
20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	1	1						
20200100	EARTH EXCAVATION	CU YD	2548	1643	905					
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	1200	1200						
20400800	FURNISHED EXCAVATION	CU YD	1108	1039	69					
20800150	TRENCH BACKFILL	CU YD	1223	128	1					1094
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	3933						3933	
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	49						49	
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	49						49	
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	49						49	
25200100	SODDING	SQ YD	3933						3933	
25200200	SUPPLEMENTAL WATERING	UNIT	59						59	
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	81	81						
28000305	TEMPORARY DITCH CHECKS	FOOT	36	12	24					
28000400	PERIMETER EROSION BARRIER	FOOT	313	313						
28000510	INLET FILTERS	EACH	37	33	4					
X2800 520	ABOVE GRADE INLET FILTERS	EACH	6	3	3					
31101100	SUBBASE GRANULAR MATERIAL, TYPE B	CU YD	487		487					
31101180	SUBBASE GRANULAR MATERIAL, TYPE B 2"	SQ YD	1212					1212		
31101400	SUBBASE GRANULAR MATERIAL, TYPE B 6"	SQ YD	11511	9725	1786					
35102000	AGGREGATE BASE COURSE, TYPE B 8"	SQ YD	1387	1387						
35300500	PORTLAND CEMENT CONCRETE BASE COURSE 10"	SQ YD	241		241					
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	2344		2344					
40600300	AGGREGATE (PRIME COAT)	TON	23		23					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGWAYS	TON	40		40					
40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	1591		1591					
40600895	CONSTRUCTING TEST STRIP	EACH	1		1					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	208		208					
40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	178		178					
40603595	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	TON	1149		1149					
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	36	33	3					
42000501	PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)	SQ YD	6192	6192						

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SHEET NO. 1 OF 7 SHEETS

F.A.P. RTE. 334	SECTION 09-0057-00-CH	COUNTY COOK	TOTAL SHEETS 103	SHEET NO. 4
CONTRACT NO. 63523				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003660				

CODED PAY ITEM NO.	ITEM	UNIT	TOTAL QUANTITY	0004	0005	0021	0021	0021	0031	0043
				RECONSTRUCTION, NO CAPACITY ADDED	SYSTEM PRESERVATION, RESURFACING	SAFETY (TRAFFIC SIGNALS)	SAFETY (LIGHTING)	SAFETY (SIDEWALKS)	LANDSCAPING AND OTHER SCENIC BEAUTIFICATION	NON-PARTICIPATING
42001300	PROTECTIVE COAT	SQ YD	12770	11073	485			1212		
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	10909					10909		
42400800	DETECTABLE WARNINGS	SQ FT	94					94		
44000100	PAVEMENT REMOVAL	SQ YD	10546	10532	14					
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	12729		12729					
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	1240	1240						
44000300	CURB REMOVAL	FOOT	244	244						
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	6047	4076	1971					
44000600	SIDEWALK REMOVAL	SQ FT	8226	8183	43					
44003100	MEDIAN REMOVAL	SQ FT	13869	3439	10430					
44004250	PAVED SHOULDER REMOVAL	SQ YD	982		982					
44201761	CLASS D PATCHES, TYPE I, 10 INCH	SQ YD	115	49	66					
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	460	142	316					
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SQ YD	406	24	382					
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	382		382					
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	17000		17000					
45200100	JOINT OR CRACK ROUTING (PC CONCRETE PAVEMENT AND SHOULDER)	FOOT	18040		18040					
45200300	JOINT OR CRACK FILLING	POUND	4228		4228					
48203037	HOT-MIX ASPHALT SHOULDERS, 10"	SQ YD	940		940					
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	259	259						
550A0120	STORM SEWERS, CLASS A, TYPE 1 24"	FOOT	52	52						
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	4		4					
55100400	STORM SEWER REMOVAL 10"	FOOT	48	45	3					
55100500	STORM SEWER REMOVAL 12"	FOOT	420	420						
55101200	STORM SEWER REMOVAL 24"	FOOT	62	62						
56100600	WATER MAIN 6"	FOOT	220							220
56100700	WATER MAIN 8"	FOOT	75							75
56100900	WATER MAIN 12"	FOOT	1411							1411
56200700	WATER SERVICE LINE 2"	FOOT	34							34
56300100	ADJUSTING SANITARY SEWERS, 8-INCH DIAMETER OR LESS	FOOT	150							150
56400810	FIRE HYDRANT EXTENSION	FOOT	3							3
56400820	FIRE HYDRANT WITH AUXILIARY VALVE AND VALVE BOX	EACH	3							3
56500800	DOMESTIC WATER SERVICE BOXES	EACH	1							1
60109510	PIPE UNDERDRAINS, FABRIC LINED TRENCH 4"	FOOT	250	250						
60201105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 11 FRAME AND GRATE	EACH	1		1					

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● SPECIALTY ITEM

CODED PAY ITEM NO.	ITEM	UNIT	TOTAL QUANTITY	0004	0005	0021	0021	0021	0031	0043
				RECONSTRUCTION, NO CAPACITY ADDED	SYSTEM PRESERVATION, RESURFACING	SAFETY (TRAFFIC SIGNALS)	SAFETY (LIGHTING)	SAFETY (SIDEWALKS)	LANDSCAPING AND OTHER SCENIC BEAUTIFICATION	NON-PARTICIPATING
60201340	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 24 FRAME AND GRATE	EACH	6	6						
60219540	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 24 FRAME AND GRATE	EACH	2	2						
60221100	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	3	3						
60237470	INLETS, TYPE A, TYPE 24 FRAME AND GRATE	EACH	9	9						
60240210	INLETS, TYPE B, TYPE 1 FRAME, OPEN LID	EACH	1	1						
60250500	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	1	1						
60251740	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 24 FRAME AND GRATE	EACH	1	1						
60260300	INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	1		1					
60261540	INLETS TO BE ADJUSTED WITH NEW TYPE 24 FRAME AND GRATE	EACH	1	1						
X6026622	VALVE VAULTS TO BE REMOVED	EACH	1							1
60266600	VALVE BOXES TO BE ADJUSTED	EACH	1							1
60500050	REMOVING CATCH BASINS	EACH	1	1						
60500060	REMOVING INLETS	EACH	14	13	1					
60600095	CLASS SI CONCRETE (OUTLET)	CU YD	4	4						
60600605	CONCRETE CURB, TYPE B	FOOT	279	279						
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	298	288	10					
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	1743	1743						
60605400	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (SPECIAL)	FOOT	1023	1023						
60605900	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-9.12	FOOT	1803		1803					
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SQ FT	1397		1397					
X6061900	CONCRETE MEDIAN, TYPE SB-6.12 (SPECIAL)	SQ FT	1783	1783						
60620800	CONCRETE MEDIAN, TYPE SB-9.12	SQ FT	146		146					
60624600	CORRUGATED MEDIAN	SQ FT	119	119						
66900205	SPECIAL WASTE DISPOSAL	CU YD	322	322						
66900450	SPECIAL WASTE PLANS AND REPORT	L SUM	1	1						
66900530	SOIL DISPOSAL ANALYSIS	EACH	2	2						
66900565	PNAS SOIL ANALYSIS	EACH	2	2						
66900590	RCRA METALS TCLP SOIL ANALYSIS	EACH	1	1						
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	9	9						
67100100	MOBILIZATION	L SUM	1	1						
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1						
XX008438	TRAFFIC CONTROL AND PROTECTION FOR TEMPORARY DETOUR	EACH	1	1						
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	36	36						
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	8177	2623	5554					
70300210	TEMPORARY PAVEMENT MARKING- LETTERS AND SYMBOLS	SQ FT	290	73	217					

● SPECIALTY ITEM

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PLOT DATE = 11/3/2018

DATE - 10-22-10

REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 3 OF 7 SHEETS

F.A.P. RTE. 334	SECTION 09-00057-00-CH	COUNTY COOK	TOTAL SHEETS 103	SHEET NO. 6
CONTRACT NO. 63523				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003660				

CODED PAY ITEM NO.	ITEM	UNIT	TOTAL QUANTITY	0004	0005	0021	0021	0021	0031	0043
				RECONSTRUCTION, NO CAPACITY ADDED	SYSTEM PRESERVATION, RESURFACING	SAFETY (TRAFFIC SIGNALS)	SAFETY (LIGHTING)	SAFETY (SIDEWALKS)	LANDSCAPING AND OTHER SCENIC BEAUTIFICATION	NON-PARTICIPATING
70300220	TEMPORARY PAVEMENT MARKING- LINE 4"	FOOT	11328	7525	3803					
70300240	TEMPORARY PAVEMENT MARKING- LINE 6"	FOOT	1150	450	700					
70300280	TEMPORARY PAVEMENT MARKING- LINE 24"	FOOT	178	44	134					
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	11446	9595	1851					
72000100	SIGN PANEL - TYPE 1	SQ FT	289	101	159		29			
72000200	SIGN PANEL - TYPE 2	SQ FT	25				25			
72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	27	13	14					
72400200	REMOVE SIGN PANEL ASSEMBLY - TYPE B	EACH	3	1	2					
72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	555	264	291					
78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	439		439					
78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	2746		2746					
78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1355		1355					
78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	243		243					
78008200	POLYUREA PAVEMENT MARKING TYPE I - LETTERS AND SYMBOLS	SQ FT	110	110						
78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	3741	3741						
78008230	POLYUREA PAVEMENT MARKING TYPE I - LINE 6"	FOOT	1488	1488						
78008250	POLYUREA PAVEMENT MARKING TYPE I - LINE 12"	FOOT	144	144						
78008270	POLYUREA PAVEMENT MARKING TYPE I - LINE 24"	FOOT	82	82						
78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	114		114					
78300100	PAVEMENT MARKING REMOVAL	SQ FT	2425	438	1987					
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	114		114					
81000600	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL	FOOT	693			693				
81000700	CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL	FOOT	262			262				
81000800	CONDUIT IN TRENCH, 3" DIA., GALVANIZED STEEL	FOOT	120			120				
81001000	CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL	FOOT	60			60				
81018500	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL	FOOT	531			531				
81018600	CONDUIT PUSHED, 2 1/2" DIA., GALVANIZED STEEL	FOOT	30			30				
81018900	CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL	FOOT	512			512				
81400100	HANDHOLE	EACH	3			3				
81400200	HEAVY-DUTY HANDHOLE	EACH	5			5				
81400300	DOUBLE HANDHOLE	EACH	2			2				
81603085	UNIT DUCT, 600V, 3-1C NO.4, 1/C NO.4 GROUND, (XLP-TYPE USE), 1 1/4" DIA. POLYETHYLENE	FOOT	406				406			
81900200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	1180			1120	60			
81900302	TRENCH AND BACKFILL WITH SCREENINGS OR SAND	FOOT	310				310			
83600200	LIGHT POLE FOUNDATION, 24" DIAMETER	FOOT	20				20			

● SPECIALTY ITEM

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 4 OF 7 SHEETS

F.A.P. RTE. 334	SECTION 09-00057-00-CH	COUNTY COOK	TOTAL SHEETS 103	SHEET NO. 7
CONTRACT NO. 63523				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(660)				

CODED PAY ITEM NO.	ITEM	UNIT	TOTAL QUANTITY	0004	0005	0021	0021	0021	0031	0043
				RECONSTRUCTION, NO CAPACITY ADDED	SYSTEM PRESERVATION, RESURFACING	SAFETY (TRAFFIC SIGNALS)	SAFETY (LIGHTING)	SAFETY (SIDEWALKS)	LANDSCAPING AND OTHER SCENIC BEAUTIFICATION	NON-PARTICIPATING
● 84400105	RELOCATE EXISTING LIGHTING UNIT	EACH	2				2			
● 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1			1				
● 85700205	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL	EACH	1			1				
● 86400100	TRANSCIEVER - FIBER OPTIC	EACH	1			1				
● 87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	1554			1554				
● 87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	2275			2275				
● 87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	2385			2385				
● 87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	2842			2842				
● 87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	4253			4253				
● 87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C	FOOT	112			112				
● 87502500	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	2			2				
● 87700170	STEEL MAST ARM ASSEMBLY AND POLE, 26 FT.	EACH	1			1				
● 87700200	STEEL MAST ARM ASSEMBLY AND POLE, 32 FT.	EACH	1			1				
● 87700250	STEEL MAST ARM ASSEMBLY AND POLE, 42 FT.	EACH	1			1				
● 87700290	STEEL MAST ARM ASSEMBLY AND POLE, 50 FT.	EACH	1			1				
● 87700300	STEEL MAST ARM ASSEMBLY AND POLE, 52 FT.	EACH	1			1				
● 87700400	STEEL MAST ARM ASSEMBLY AND POLE, 60 FT.	EACH	1			1				
● 87800100	CONCRETE FOUNDATION, TYPE A	FOOT	8			8				
● 87800150	CONCRETE FOUNDATION, TYPE C	FOOT	4			4				
● 87800400	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	30			30				
● 87800415	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	45			45				
● 87800420	CONCRETE FOUNDATION, TYPE E 42-INCH DIAMETER	FOOT	21			21				
● 87900200	DRILL EXISTING HANDHOLE	EACH	2			2				
● 88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	8			8				
● 88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	10			10				
● 88030240	SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED	EACH	2			2				
● 88102747	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	4			4				
● 88200210	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	18			18				
● 88500100	INDUCTIVE LOOP DETECTOR	EACH	15			15				
● 88600100	DETECTOR LOOP, TYPE I	FOOT	652			652				
● 88700200	LIGHT DETECTOR	EACH	3							3
● 88700300	LIGHT DETECTOR AMPLIFIER	EACH	1							1
● 88800100	PEDESTRIAN PUSH-BUTTON	EACH	8			8				
● 89000100	TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1			1				
● 89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	3513			1923	1590			

● SPECIALTY ITEM

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DESIGNED - BLG

REVISED -

DRAWN - BLG

REVISED -

PLOT SCALE = 50.0000 / IN.

CHECKED - DJK

REVISED -

PLOT DATE = 11/3/2010

DATE - 10-22-10

REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 5 OF 7 SHEETS

F.A.P. RTE. 334

SECTION 09-00057-00-CH

COUNTY COOK

TOTAL SHEETS 103

SHEET NO. 8

CONTRACT NO. 63523

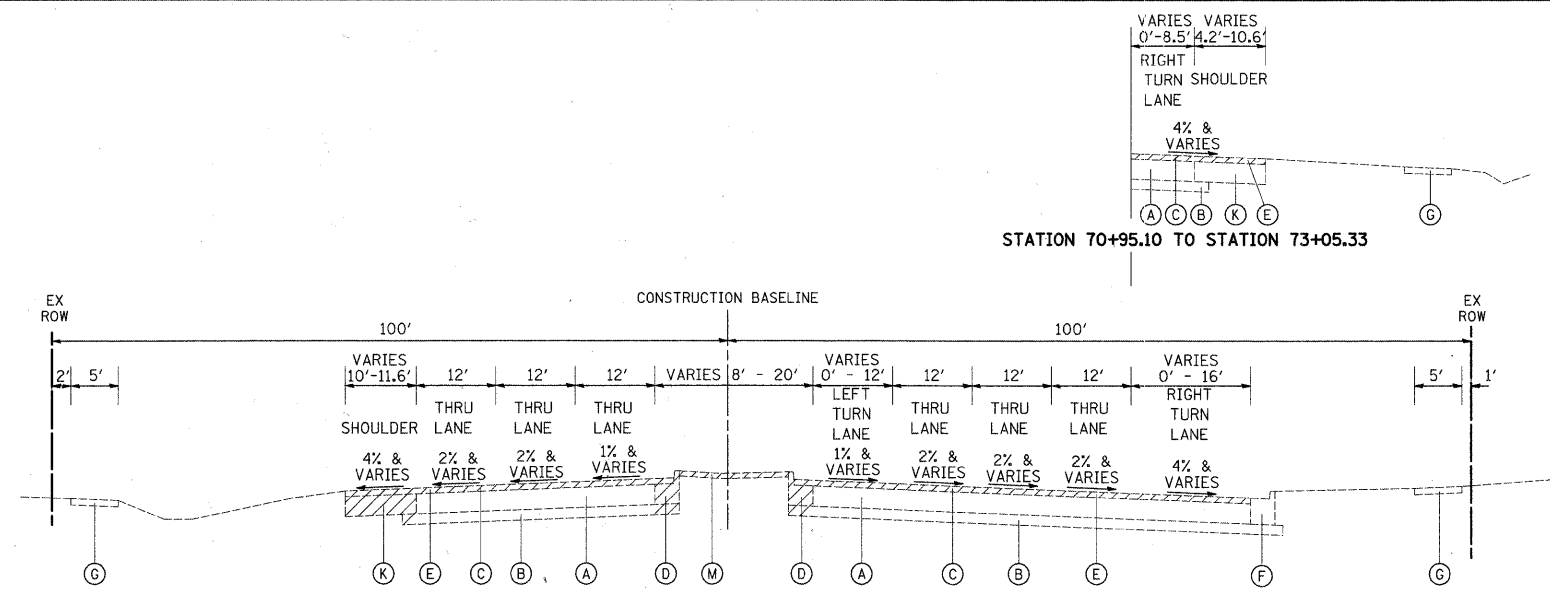
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003660

CODED PAY ITEM NO.	ITEM	UNIT	TOTAL QUANTITY	0004	0005	0021	0021	0021	0031	0043
				RECONSTRUCTION, NO CAPACITY ADDED	SYSTEM PRESERVATION, RESURFACING	SAFETY (TRAFFIC SIGNALS)	SAFETY (LIGHTING)	SAFETY (SIDEWALKS)	LANDSCAPING AND OTHER SCENIC BEAUTIFICATION	NON-PARTICIPATING
● XT030025	WET REFLECTIVE TEMPORARY TAPE, TYPE III, LETTERS AND SYMBOLS	SQ FT	109	109						
● XT030055	WET REFLECTIVE TEMPORARY TAPE, TYPE III, 24 INCH	FOOT	77	77						
● X8050015	SERVICE INSTALLATION - POLE MOUNTED	EACH	1			1				
● X8440116	RELOCATE EXISTING LIGHTING UNIT, SPECIAL	EACH	2				2			
● X8620020	UNINTERRUPTIBLE POWER SUPPLY	EACH	1			1				
● X8710020	FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM12F	FOOT	2390			2390				
● X8730027	ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C	FOOT	983			983				
● X8730250	ELECTRIC CABLE IN CONDUIT NO. 20 3/C, TWISTED, SHIELDED	FOOT	665							665
● XX004907	GATE VALVE 12" WITH VAULT, 5' DIAMETER	EACH	4							4
● XX004913	REMOVE FIBER OPTIC CABLE FROM CONDUIT	FOOT	1935			1935				
● XX006077	GATE VALVE AND BOX 6"	EACH	3							3
● XX006241	GATE VALVE AND BOX 8"	EACH	2							2
● XX006633	DUCTILE IRON PIPE INSTALLED IN STEEL CASING, 12"	FOOT	45							45
● XX006826	REMOVE AND RELOCATE LAWN SPRINKLER SYSTEM	FOOT	50	50						
● XX006827	SIDEWALK RAILROAD CROSSING	EACH	1	1						
● XX007090	VALVE VAULT, 5' DIA., WITH 12" VALVE	EACH	3							3
● XX008361	WATER MAIN TO BE ABANDONED	L SUM	1							1
● XX011700	WATER MAIN FITTINGS	POUND	4236							4236
● XX008439	HOT SYNTHETIC PATTERNED TEXTURED PAVING, 3/4"	SQ FT	7794							7794
● 0200824	TREE, ACER PLATANOIDES CRIMSON KING (CRIMSON KING NORWAY MAPLE), 4" CALIPER, BALLED AND BURLAPPED	EACH	3						3	

● SPECIALTY ITEM

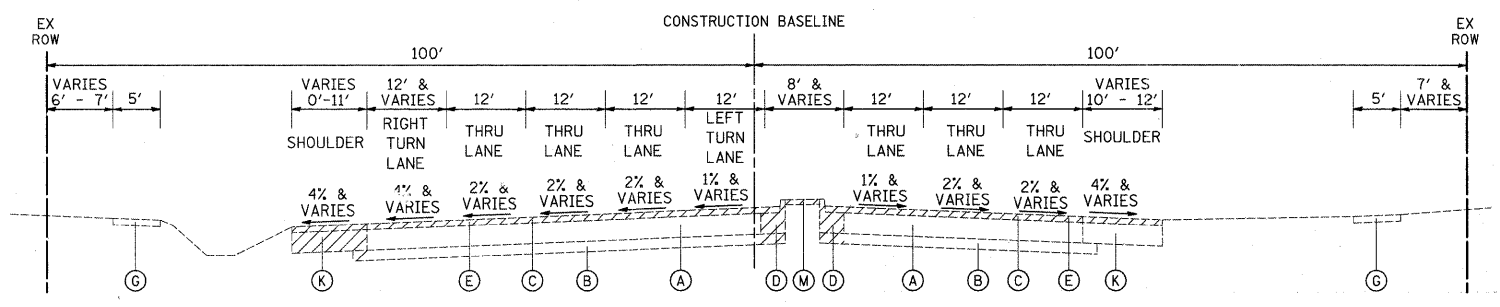
LEGEND

- (A) PORTLAND CEMENT CONCRETE PAVEMENT, 10"
- (B) HOT-MIX ASPHALT BASE COURSE, 4"
- (C) HOT-MIX ASPHALT SURFACE COURSE - VARIES 2 1/2" TO 3 1/2"
- (D) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-9.12 (GUTTER OVERLAID)
- (E) HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- (F) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (G) PORTLAND CEMENT CONCRETE SIDEWALK
- (H) CONCRETE MEDIAN
- (I) HOT-MIX ASPHALT SURFACE COURSE, 1 1/2"
- (J) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-9.12
- (K) HOT-MIX ASPHALT SHOULDER, 10"
- (L) ASPHALT MEDIAN
- (M) CONCRETE MEDIAN SURFACE, 4"



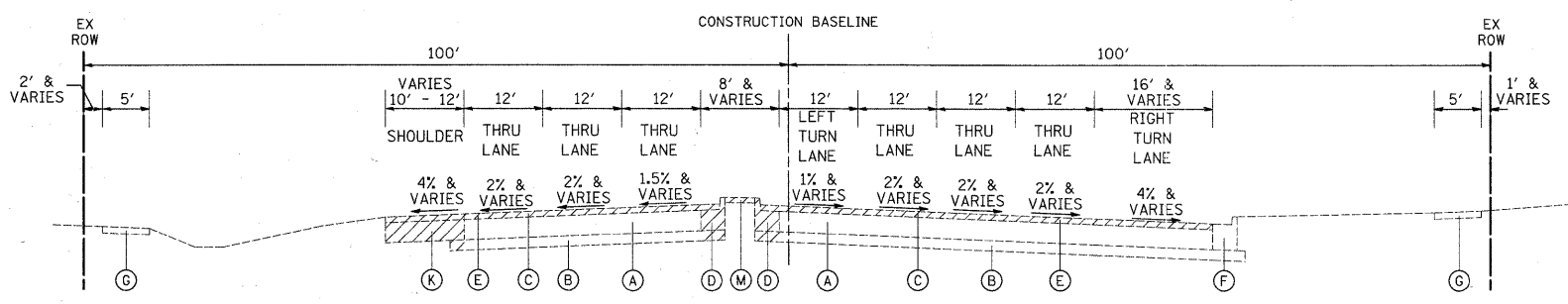
EXISTING TYPICAL SECTION

BUSSE ROAD
STATION 70+95.10 TO STATION 74+70.53



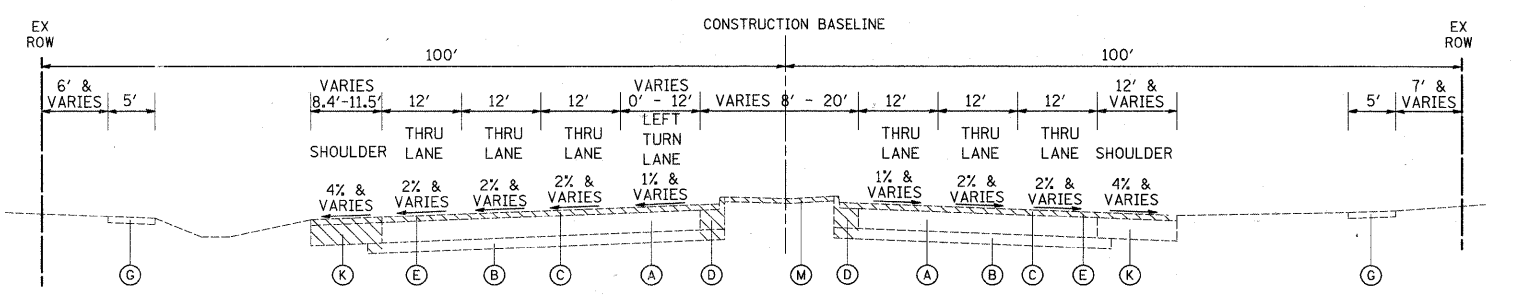
EXISTING TYPICAL SECTION

BUSSE ROAD
STATION 76+77.38 TO STATION 79+15.53



EXISTING TYPICAL SECTION

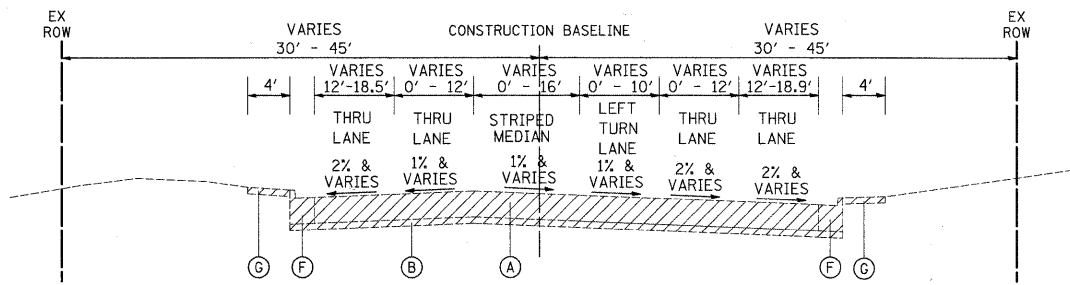
BUSSE ROAD
STATION 74+70.53 TO STATION 76+77.38



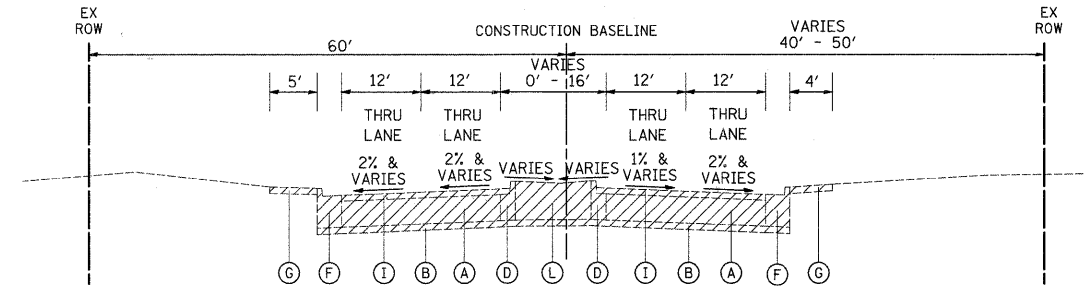
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BUSSE ROAD
STATION 79+15.53 TO STATION 82+50.53

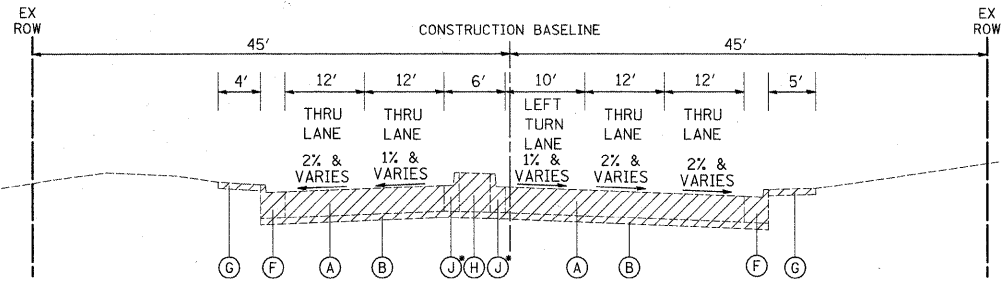
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PLOT SCALE = 50.0000' / IN.	CHECKED - DJK	REVISED -	SCALE: N.T.S.			SHEET NO. 1 OF 2 SHEETS	CONTRACT NO. 63523					
PLOT DATE = 10/21/2010	DATE - 10-22-10	REVISED -	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-900316501									



EXISTING TYPICAL SECTION
 PRATT BOULEVARD
 STATION 401+27.62 TO STATION 407+65.42



EXISTING TYPICAL SECTION
 PRATT BOULEVARD
 STATION 412+99.78 TO STATION 415+68.82



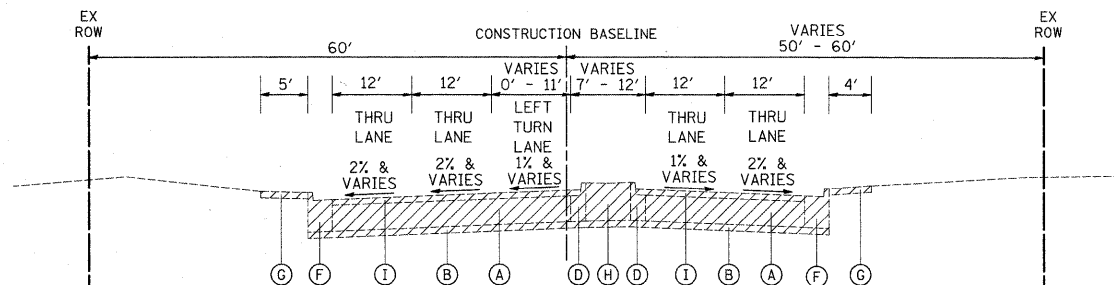
*GUTTER OVERLAID STATION 408+21.95 TO STATION 408+72.84
EXISTING TYPICAL SECTION
 PRATT BOULEVARD
 STATION 407+65.42 TO STATION 409+05.58

LEGEND

- (A) PORTLAND CEMENT CONCRETE PAVEMENT, 10"
- (B) HOT-MIX ASPHALT BASE COURSE, 4"
- (C) HOT-MIX ASPHALT SURFACE COURSE - VARIES 2 1/2" TO 3 1/2"
- (D) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-9.12 (GUTTER OVERLAID)
- (E) HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- (F) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (G) PORTLAND CEMENT CONCRETE SIDEWALK
- (H) CONCRETE MEDIAN
- (I) HOT-MIX ASPHALT SURFACE COURSE, 1 1/2"
- (J) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-9.12
- (K) HOT-MIX ASPHALT SHOULDER, 10"
- (L) ASPHALT MEDIAN
- (M) CONCRETE MEDIAN SURFACE, 4"



REMOVAL ITEMS



EXISTING TYPICAL SECTION
 PRATT BOULEVARD
 STATION 410+33.74 TO STATION 412+99.78

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 DATE - 10-22-10

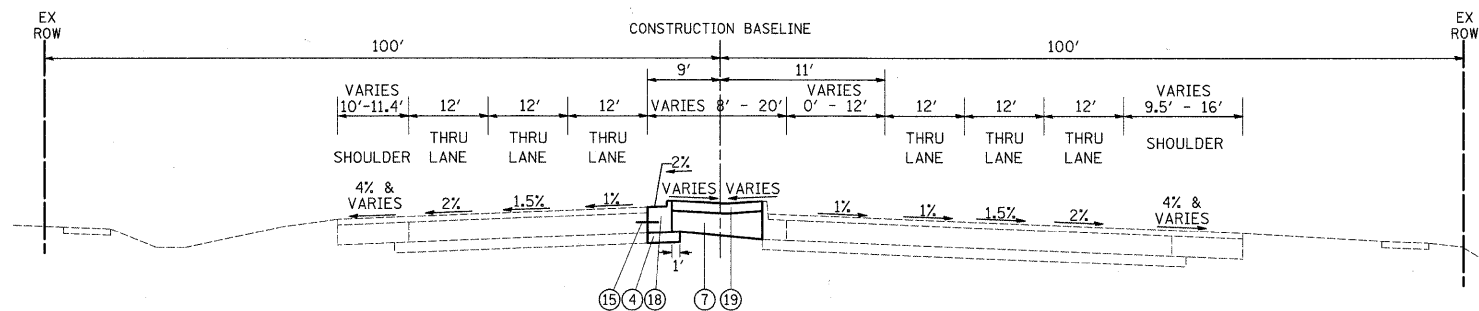
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PRATT BOULEVARD
EXISTING TYPICAL SECTIONS

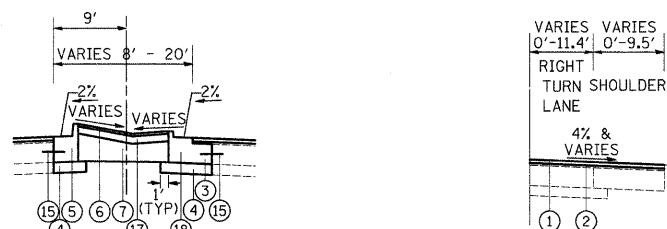
SCALE: N.T.S. SHEET NO. 2 OF 2 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	09-00057-00-CH	COOK	103	12
CONTRACT NO. 63523				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(660)				



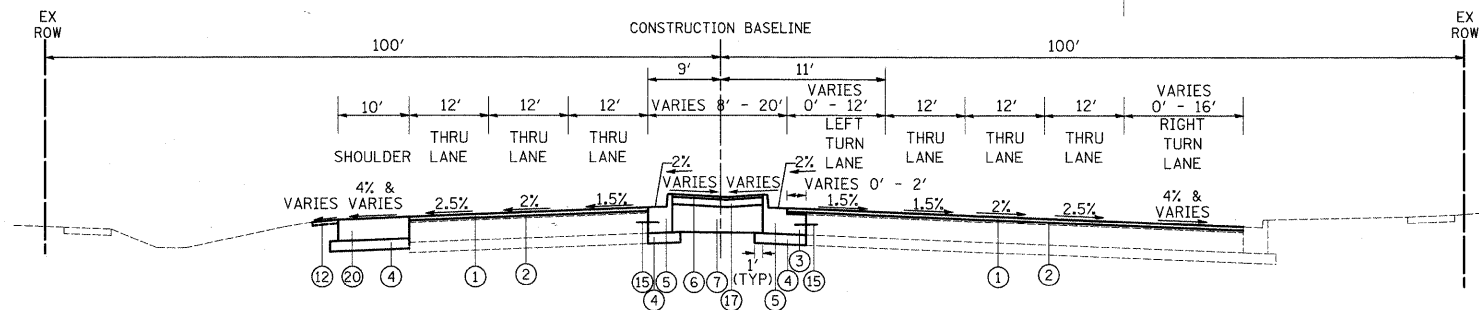
PROPOSED TYPICAL SECTION

BUSSE ROAD
STATION 70+95.10 TO STATION 71+94.48



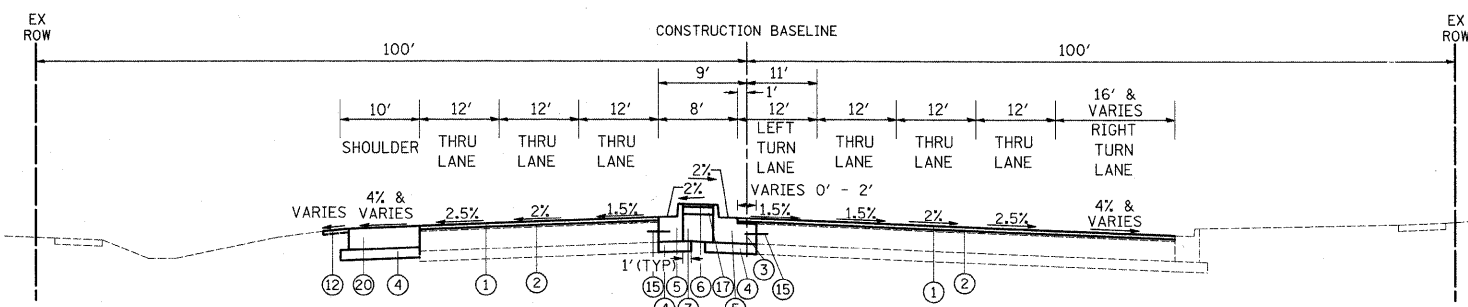
STATION 71+94.48 TO STATION 72+72.00

STATION 71+94.48 TO STATION 73+05.33



PROPOSED TYPICAL SECTION

BUSSE ROAD
STATION 71+94.48 TO STATION 74+15.20



PROPOSED TYPICAL SECTION

BUSSE ROAD
STATION 74+15.20 TO STATION 76+77.38

LEGEND

- ① POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX F, N90, 1 3/4"
- ② LEVELING BINDER (MACHINE METHOD), N70, 3/4" (MIN. & VARIES)
- ③ PORTLAND CEMENT CONCRETE BASE COURSE WIDENING, 10"
- ④ SUBBASE GRANULAR MATERIAL, TYPE B, 6"
- ⑤ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-9.12
- ⑥ HOT SYNTHETIC PATTERNED TEXTURED PAVING, 3/4"
- ⑦ SUBBASE GRANULAR MATERIAL, TYPE B
- ⑧ PORTLAND CEMENT CONCRETE PAVEMENT, 10" (JOINTED)
- ⑨ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- ⑩ PORTLAND CEMENT CONCRETE SIDEWALK, 5"
- ⑪ SUBBASE GRANULAR MATERIAL, TYPE B 2"
- ⑫ TOPSOIL FURNISH AND PLACE, 4" SODDING
- ⑬ CONCRETE MEDIAN, TYPE SB-6.12 (SPECIAL)
- ⑭ NO. 6 EPOXY COATED, DEFORMED TIE BAR @ 24" C-C (PER STANDARD 606001) (INCLUDED IN THE COST OF COMBINATION CONCRETE CURB AND GUTTER, TYPE B-9.12 OR COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24)
- ⑮ NO. 6 EPOXY COATED, DEFORMED TIE BAR, 24" LONG @ 24" C-C (DRILLED AND GROUTED) (EMBED 8" MINIMUM)
- ⑯ LONGITUDINAL JOINT (SEE JOINTING PLAN FOR REINFORCEMENT DETAILS)
- ⑰ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 4"
- ⑱ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.12
- ⑲ CONCRETE MEDIAN SURFACE, 4 INCH
- ⑳ HOT-MIX ASPHALT SHOULDERS, 10"

CONTRACTOR SHALL MILL BEFORE PATCHING

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

PAY ITEM	AIR VOIDS @ Ndes
PAVEMENT RESURFACING	
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL-9.5mm); 1 3/4"	4% @ 90 GYR.
LEVELING BINDER (MACHINE METHOD), N70; 3/4" (MIN. & VARIES)	4% @ 70 GYR.
TEMPORARY PAVEMENT	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5mm); 1 1/2"	4% @ 50 GYR.
TEMPORARY PAVEMENT (HMA BINDER IL-19mm); 8 1/2"	4% @ 50 GYR.
PATCHING	
CLASS D PATCHES (HMA BINDER IL-19mm); 10"	4% @ 70 GYR.
MEDIAN	
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50; 4"	4% @ 50 GYR.
INCIDENTAL HOT-MIX ASPHALT SURFACING	
HOT-MIX ASPHALT SURFACE, COURSE, IL-9.5, MIX "C", N50 (IL-9.5mm)	4% @ 50 GYR.
SHOULDER	
HMA SHOULDER (HMA BINDER IL-19mm); 10"	2% @ 30 GYR.

NOTES:

1. THE UNIT WEIGHT TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LB/SY-IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

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PLOT SCALE = 50.0000' / IN.
PLOT DATE = 10/21/2010

CHECKED - DJK
DATE - 10-22-10

REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL ROUTE 83 (BUSSE ROAD)
PROPOSED TYPICAL SECTIONS**

SCALE: N.T.S. SHEET NO. 1 OF 2 SHEETS

F.A.P. RTE. 344	SECTION 09-00057-00-CH	COUNTY COOK	TOTAL SHEETS 103	SHEET NO. 13
CONTRACT NO. 63523				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(660)				

EARTHWORK SCHEDULE - GREENLEAF AVENUE				
ITEM	UNIT	STAGE 1	STAGE 2	STAGE 3
EARTH EXCAVATION	C.Y.	457	540	646
EARTH EXCAVATION ADJUSTED FOR SHRINKAGE	C.Y.	389	459	549
EMBANKMENT REQUIRED	C.Y.	246	785	9
EARTHWORK BALANCE				
WASTE (+) OR SHORTAGE (-)	C.Y.	+143	-326	+540

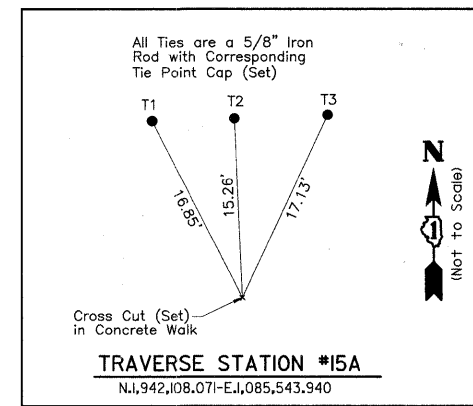
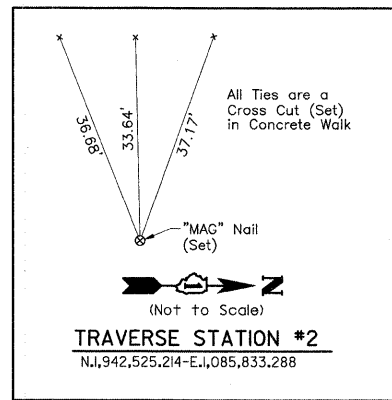
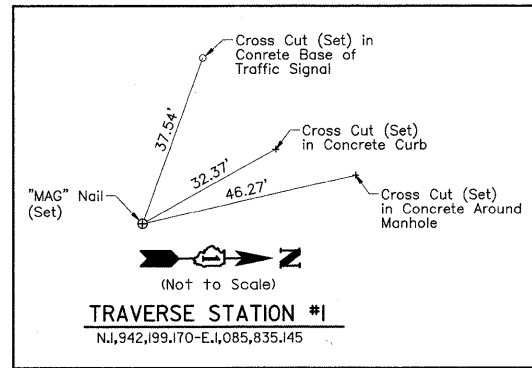
EARTHWORK SCHEDULE - IL ROUTE 83 (BUSSE ROAD)				
ITEM	UNIT	STAGE 1	STAGE 2	STAGE 3
EARTH EXCAVATION	C.Y.	-	-	905
EARTH EXCAVATION ADJUSTED FOR SHRINKAGE	C.Y.	-	-	769
EMBANKMENT REQUIRED	C.Y.	-	-	69
EARTHWORK BALANCE				
WASTE (+) OR SHORTAGE (-)	C.Y.	-	-	+700

SHRINKAGE CALCULATED USING 15% SHRINKAGE FACTOR

A PAY ITEM FOR "FURNISHED EXCAVATION" HAS BEEN INCLUDED ON THE ASSUMPTION THAT, DUE TO LIMITED WORKING SPACE, ALL EMBANKMENT MAY HAVE TO BE BROUGHT IN FROM OUTSIDE THE PROJECT LIMITS.

SCHEDULE OF DRIVEWAY PAVEMENT REMOVAL & REPLACEMENT		
STATION	DRIVEWAY PAVEMENT REMOVAL	PCC DW, 8", SPECIAL
402+12, LT	103 SY	106 SY
402+19, RT	112 SY	176 SY
402+87, RT*	38 SY	0 SY
403+50, RT	62 SY	64 SY
404+66, LT	75 SY	75 SY
405+08, RT	112 SY	113 SY
406+70, RT	109 SY	120 SY
407+65, RT	31 SY	34 SY
410+95, LT	127 SY	109 SY
412+15, RT	52 SY	61 SY
413+25, LT	117 SY	113 SY
413+38, RT	58 SY	65 SY
415+05, RT	103 SY	115 SY
415+34, LT	141 SY	128 SY

* PROPOSED DRIVEWAY AREA COMBINED WITH DRIVEWAY AT 402+19, RT

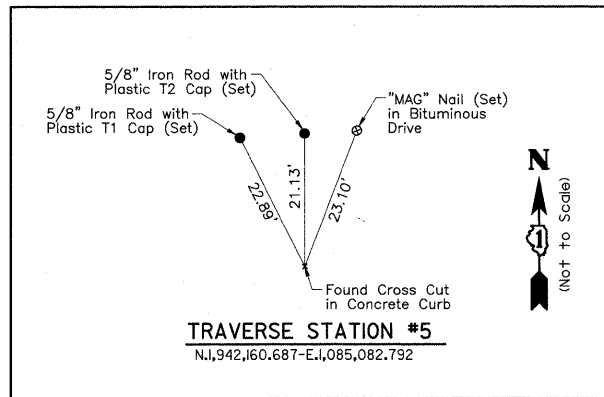
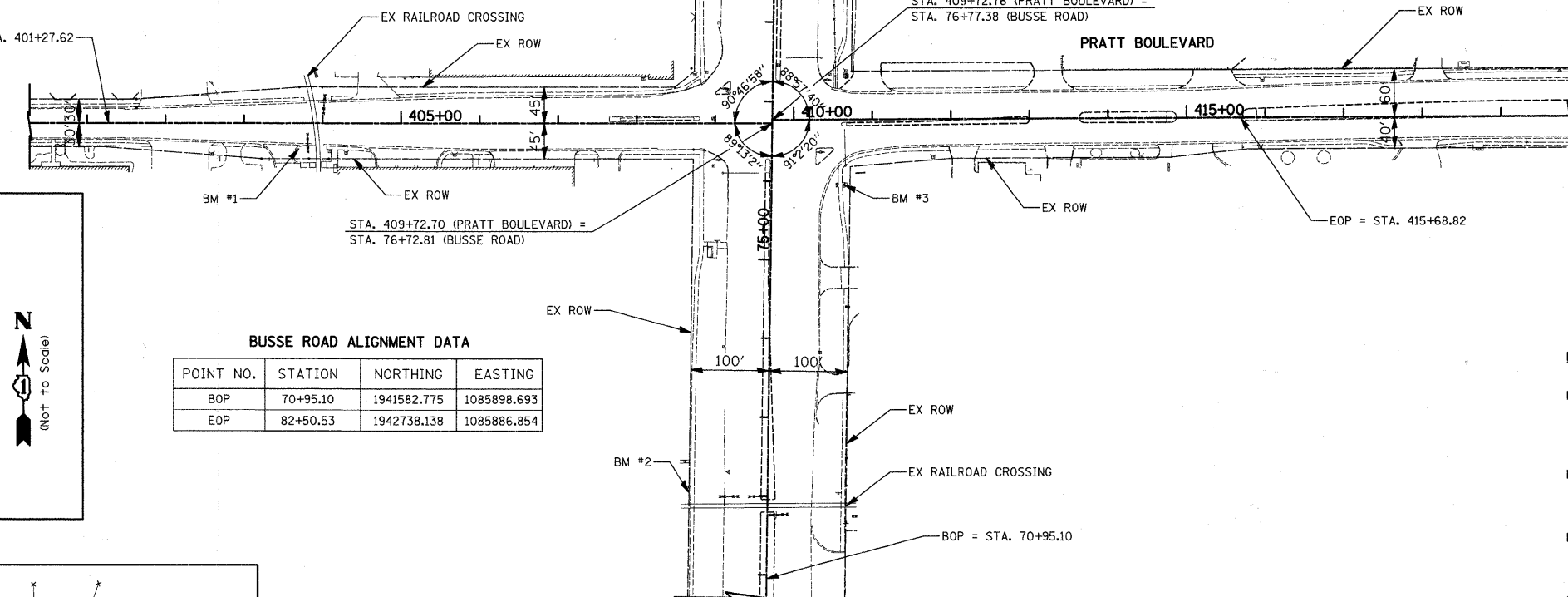


PRATT BOULEVARD (WEST) ALIGNMENT DATA

POINT NO.	STATION	NORTHING	EASTING
BOP	401+27.62	1942140.248	1085047.935
EOP	409+72.70	1942160.451	1085892.774

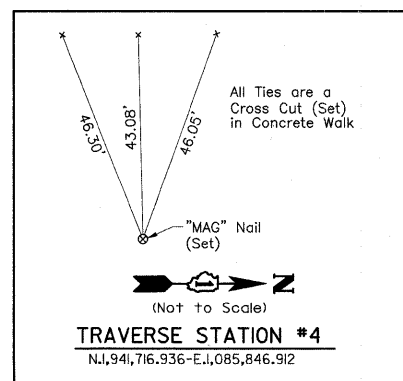
PRATT BOULEVARD (EAST) ALIGNMENT DATA

POINT NO.	STATION	NORTHING	EASTING
BOP	409+72.76	1942165.018	1085892.727
EOP	415+68.82	1942181.930	1086488.548



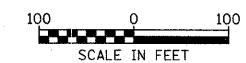
BUSSE ROAD ALIGNMENT DATA

POINT NO.	STATION	NORTHING	EASTING
BOP	70+95.10	1941582.775	1085898.693
EOP	82+50.53	1942738.138	1085886.854



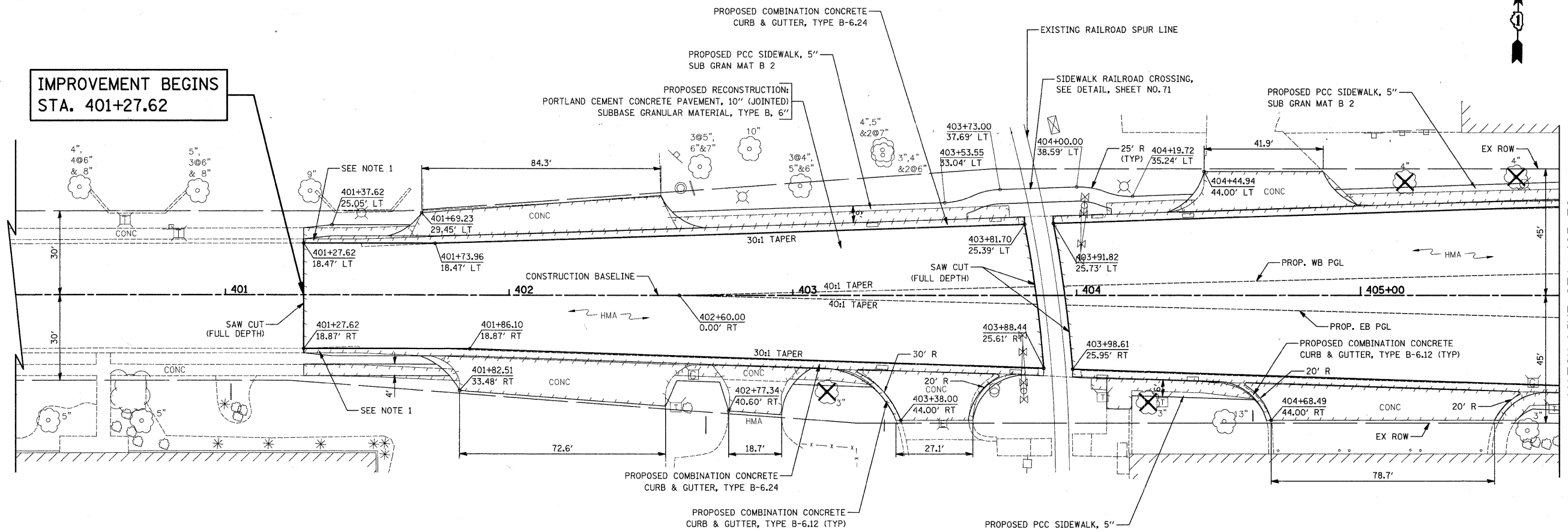
NOTES:
 BOP = BEGINNING OF PROJECT
 EOP = END OF PROJECT
 P.I. = POINT OF INTERSECTION
 P.C. = POINT OF CURVATURE
 P.T. = POINT OF TANGENT
 BM = BENCHMARK

- BENCHMARKS**
- REF BM ELEVATIONS ARE BASED ON G.P.S. RTK OBSERVATIONS MEASURED AT TRAVERSE STATION #2, AS SHOWN HEREON. NGVD '88 ELEV. = 678.54
 - BM #1 CROSS CUT (SET) IN FIRST FIRE HYDRANT WEST OF SPUR TRACK ON SOUTH SIDE OF PRATT BOULEVARD. ELEV. = 676.88
 - BM #2 RAILROAD SPIKE (SET) IN FIRST POWER POLE NORTH OF SPUR TRACK ON WEST SIDE OF ILLINOIS ROUTE 83. ELEV. = 675.71
 - BM #3 BOX CUT (SET) IN THE SOUTHWEST CORNER OF THE CONCRETE BASE OF TRAFFIC SIGNAL CONTROLLER IN THE SOUTHEAST QUADRANT OF THE INTERSECTION OF ILLINOIS ROUTE 83 AND PRATT BOULEVARD. ELEV. = 675.68
 - BM #4 CROSS CUT (SET) ON NORTHEAST FLANGE BOLT OF SECOND FIRE HYDRANT ON NORTH SIDE OF PRATT BOULEVARD ON THE EAST SIDE OF ILLINOIS ROUTE 83. ELEV. = 676.23
 - BM #5 CROSS CUT (SET) ON SOUTHEAST FLANGE BOLT OF THIRD FIRE HYDRANT EAST OF ILLINOIS ROUTE 83 ON THE SOUTH SIDE OF PRATT BOULEVARD. ELEV. = 676.05

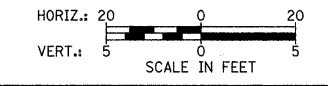


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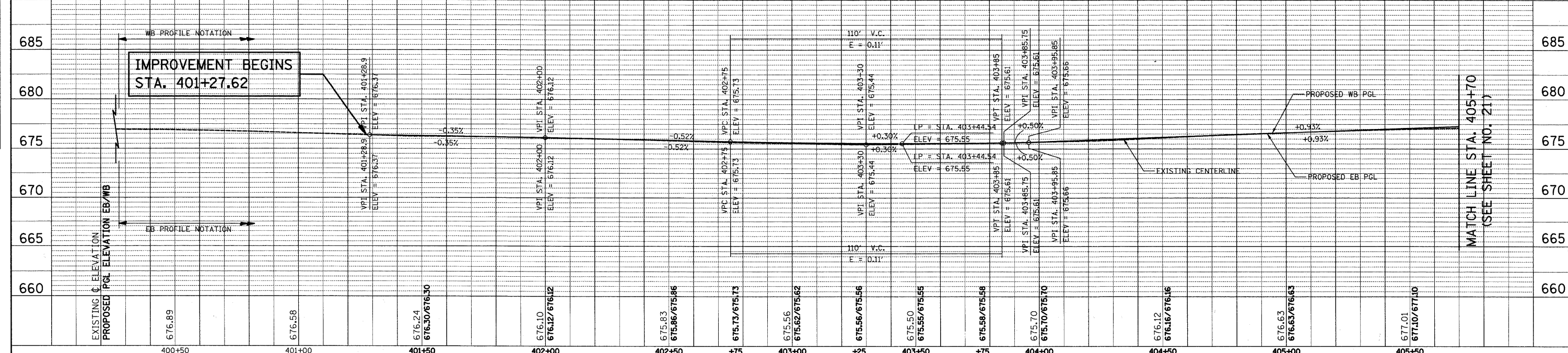
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- NOTES:
1. TRANSITION CURB FROM B-6.24 TO MATCH EXISTING OVER 10'. PAID FOR AS "COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24".
 2. ALL DRIVEWAYS SHALL BE REPLACED WITH PCC DRIVEWAY, 8 INCH, SPECIAL.
 3. SEE MAINTENANCE OF TRAFFIC AND WATERMAIN PLANS FOR REPLACEMENT REQUIRED WEST OF RECONSTRUCTION LIMITS.



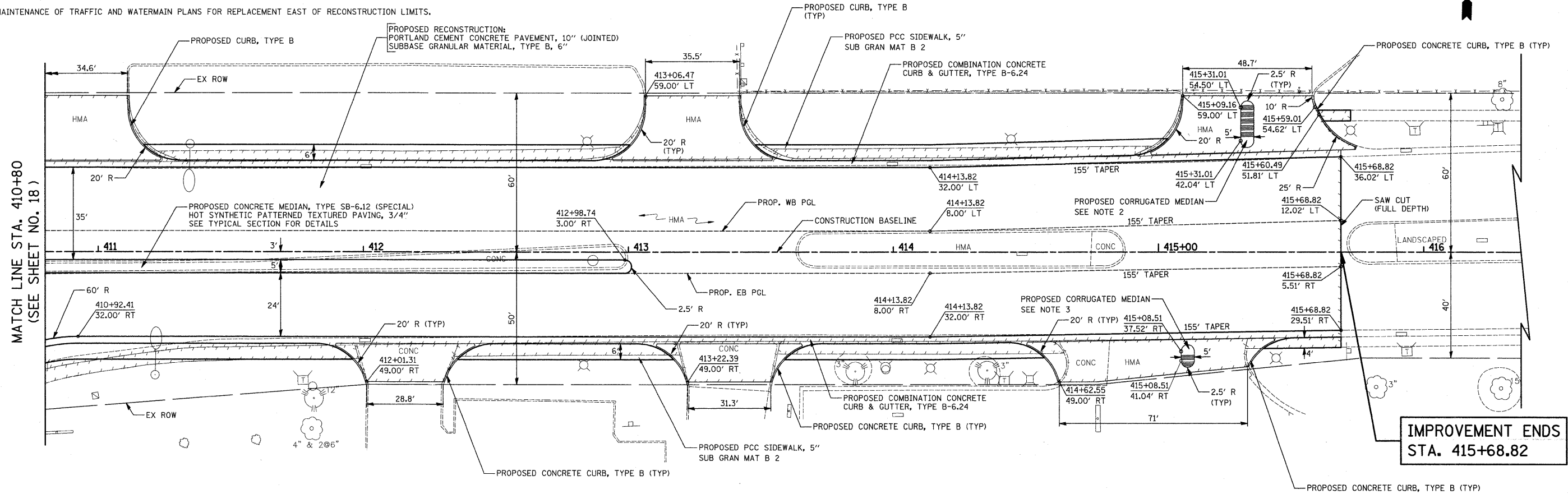
PRATT BOULEVARD



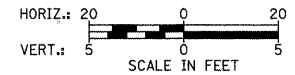
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PLOT SCALE = 20.0000' / IN.	CHECKED - DJK	DATE - 10-22-10	REVISIONS	DEPARTMENT OF TRANSPORTATION				CONTRACT NO. 63523		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003660		
PLOT DATE = 10/22/2010	DATE - 10-22-10	REVISIONS	SCALE: 1" = 20'				SHEET NO. 4 OF 6 SHEETS		STA. 401+27.62 TO STA. 405+70.00			

NOTES:

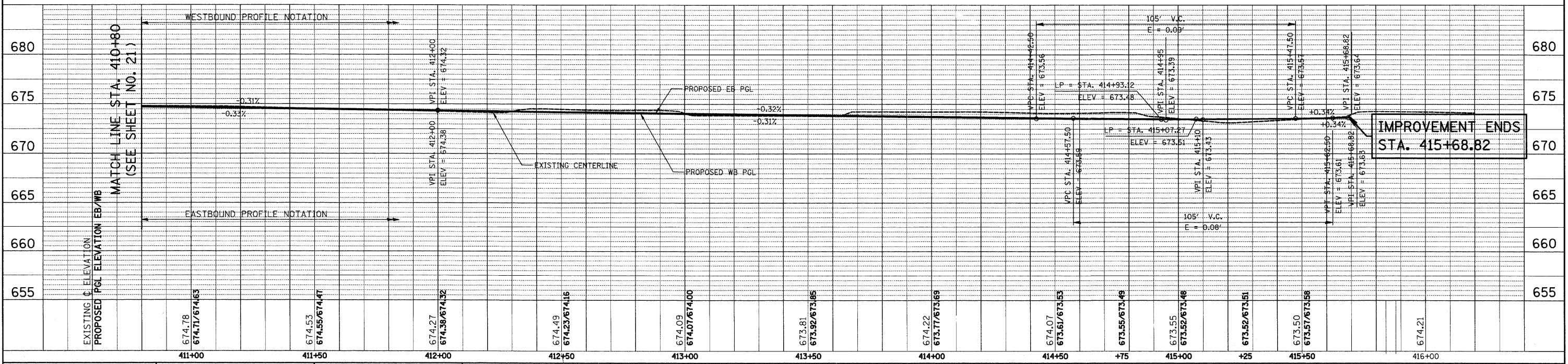
1. ALL DRIVEWAYS SHALL BE REPLACED WITH PCC DRIVEWAY, 8 INCH, SPECIAL.
2. SOUTHERN 4' OF MEDIAN SHALL BE SMOOTH AND WITHOUT CORRUGATION TO ACCOMMODATE PEDESTRIANS.
3. NORTHERN 4' OF MEDIAN SHALL BE SMOOTH AND WITHOUT CORRUGATION TO ACCOMMODATE PEDESTRIANS.
4. SEE MAINTENANCE OF TRAFFIC AND WATERMAIN PLANS FOR REPLACEMENT EAST OF RECONSTRUCTION LIMITS.



**IMPROVEMENT ENDS
STA. 415+68.82**



PRATT BOULEVARD



**IMPROVEMENT ENDS
STA. 415+68.82**

PLAN	REVIEWED	DATE
	BY	
	DATE	
	NO.	
	FILE NAME	

PROFILE	REVIEWED	DATE
	BY	
	DATE	
	NO.	
	FILE NAME	

ACCESS MAINTENANCE NOTES

MAINTAINING ACCESS TO PRIVATE DRIVEWAYS IS OF THE UTMOST IMPORTANCE. ACCESS TO ALL PROPERTIES SHALL BE MAINTAINED AT ALL TIMES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUB-STAGING THE CONSTRUCTION OF DRIVEWAYS. THE CONTRACTOR SHALL FOLLOW THESE PROCEDURES TO ENSURE PROPER DRIVEWAY ACCESS:

- THE CONTRACTOR SHALL WORK WITH ADJACENT BUSINESS OWNERS TO DETERMINE DRIVEWAY RECONSTRUCTION SCHEDULING. ALL DRIVEWAY CLOSURES SHALL BE APPROVED BY THE ENGINEER.
- TEMPORARY DRIVES SHALL BE CONSTRUCTED USING 100% RECYCLED ASPHALT PAVEMENT. THE WIDTH OF THE DRIVE SHALL BE DETERMINED BY THE ENGINEER. THE CONTRACTOR SHALL NOTE THAT THE TEMPORARY DRIVEWAY WIDTH MAY EXCEED THE WIDTH OF THE EXISTING DRIVEWAY IN ORDER TO ALLOW FOR SEMI-TRAILERS TO BACK INTO THE LOADING DOCKS.

THE COST OF PLACING, MAINTAINING AND REMOVING TEMPORARY DRIVES SHALL BE INCLUDED IN THE COST OF "TEMPORARY ACCESS (COMMERCIAL ENTRANCE)".

IT MAY BE NECESSARY TO CONSTRUCT TEMPORARY PADS IN THE CONSTRUCTION ZONE FOR DRIVEWAYS ON THE OPPOSITE SIDE OF THE ROAD. THESE PADS MAY BE NECESSARY FOR SEMI-TRAILERS TO BACK INTO THE LOADING DOCKS. THE ENGINEER SHALL DIRECT THE CONTRACTOR AS TO THE LOCATIONS AND SIZES OF THESE PADS. THE COST OF PLACING THESE TEMPORARY PADS SHALL NOT BE PAID FOR SEPARATELY BUT INCLUDED IN THE COST OF THE TEMPORARY ACCESS FOR THE LOADING DOCK OR DRIVEWAY.
- A 4" PVC DRAIN SHALL BE PLACED UNDER TEMPORARY DRIVES TO PROVIDE POSITIVE DRAINAGE WHEN THE ROADWAY IS EXCAVATED. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF "DRIVEWAY PAVEMENT REMOVAL".
- QUANTITIES FOR HIGH-EARLY-STRENGTH PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED) AND COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (SPECIAL) HAVE BEEN INCLUDED FOR USE IN FRONT OF THE DRIVEWAYS IN ORDER TO EXPEDITE THE COMPLETION OF THIS WORK. THESE ITEMS SHALL ONLY BE USED WHEN APPROVED BY THE ENGINEER.
- ALL BARRICADES REQUIRED TO SUB-STAGE DRIVEWAY CONSTRUCTION AND MAINTAIN ACCESS TO DRIVEWAYS SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".

KEEPING ROADS OPEN TO TRAFFIC

EXCEPT FOR APPROVED CLOSURES AS DEPICTED ON THE MAINTENANCE OF TRAFFIC PLANS, ALL ROADS SHALL BE KEPT OPEN TO TRAFFIC DURING THE ENTIRE CONSTRUCTION PERIOD. THE CONTRACTOR MAY CLOSE ONE LANE OF TRAFFIC (BECAUSE OF CONSTRUCTION) ONLY BETWEEN THE HOURS OF 9:00 A.M. AND 3:00 P.M.

WHEN NECESSARY TO CLOSE ONE LANE OF THE ROADWAY ON TWO-LANE ROADS, THE CONTRACTOR SHALL MAINTAIN TWO-WAY TRAFFIC DURING THE RESTRICTED HOURS WITH THE USE OF SIGNS AND FLAGGERS AS SHOWN ON THE TRAFFIC CONTROL STANDARDS. WHEN NECESSARY TO CLOSE ONE LANE OF THE ROADWAY ON FOUR-LANE ROADS, THE CONTRACTOR SHALL MAINTAIN TWO-WAY TRAFFIC DURING THE RESTRICTED HOURS WITH THE USE OF SIGNS AND BARRICADES AS SHOWN ON THE TRAFFIC CONTROL STANDARDS. ALL EXISTING LANES OF TRAFFIC IN EACH DIRECTION WILL BE MAINTAINED BETWEEN 3:00 P.M. AND 9:00 A.M. AND WHEN NO CONSTRUCTION ACTIVITIES ARE BEING CARRIED ON, THE ENGINEER MAY WAIVE THE LANE CLOSURE TIME RESTRICTION AT HIS DISCRETION.

THE CONTRACTOR SHALL LIMIT ANY DROP-OFF BETWEEN LANES TO 1-1/2" DURING ANY OVERNIGHT PERIOD.

TEMPORARY PAVEMENT FOR ALL STAGES SHALL CONSIST OF THE FOLLOWING:

- 1.5" HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5 mm)
- 8.5" TEMP PAVEMENT (HMA BINDER IL-19 mm)

OR

- 8" PORTLAND CEMENT CONCRETE BASE COURSE

PRE-STAGE 1 (NOT SHOWN)

- INSTALL AND ACTIVATE TEMPORARY TRAFFIC SIGNALS AT THE INTERSECTION OF IL ROUTE 83 AND PRATT BOULEVARD. (NOTE: THE EXISTING SIGNALS SHALL BE TURNED OFF AT THE SAME TIME THE TEMPORARY SIGNALS ARE ACTIVATED.)
- REMOVE EXISTING SIGNAL POLES AND EQUIPMENT.
- REMOVE EXISTING BARRIER MEDIANS ON PRATT BOULEVARD, AND CURB AND GUTTER, SIDEWALKS, AND DRIVEWAYS IN AREAS REQUIRING TEMPORARY PAVEMENT. PLACE TEMPORARY PAVEMENT. THIS WORK SHALL BE PERFORMED USING DAILY LANE CLOSURES IN ACCORDANCE WITH IDOT HIGHWAY STANDARDS 701501 AND 701601.

STAGE 1

- PLACE ALL CONSTRUCTION SIGNS, TEMPORARY PAVEMENT MARKINGS, AND BARRICADES. SHIFT TRAFFIC AS INDICATED.
- REMOVE EXISTING PAVEMENT, CURB AND GUTTER AND DRIVEWAYS ON THE SOUTH SIDE OF PRATT BOULEVARD.
- CONSTRUCT WATERMAIN AND STORM SEWER LATERALS ACROSS PRATT BOULEVARD. DAILY LANE CLOSURES IN ACCORDANCE WITH IDOT STANDARD 701501 SHALL BE ALLOWED BETWEEN THE HOURS OF 9:00 AM AND 3:00 PM. TRENCHES SHALL NOT BE LEFT IN STONE OVERNIGHT. THE CONTRACTOR SHALL CONSTRUCT, REPAIR AND MAINTAIN THE TRENCHES WITH CLASS D PATCHES, 10 INCH. THE CONTRACTOR SHALL ENSURE POSITIVE DRAINAGE IS PROVIDED TO THE NEW STORM SEWER STRUCTURES ON BOTH SIDES OF PRATT BOULEVARD.
- CONSTRUCT CURB AND GUTTER, CONCRETE PAVEMENT, SIDEWALK AND ENTRANCES AS SHOWN.
- REDUCE RADIUS AT THE SOUTHEAST CORNER TO COMPLETE CONSTRUCTION AT THE INTERSECTION. THE RADIUS SHALL ONLY BE REDUCED FOR A MAXIMUM OF 7 CALENDAR DAYS.
- CONSTRUCT TEMPORARY PAVEMENT REQUIRED FOR STAGE 2 CONSTRUCTION USING DAILY LANE CLOSURES IN ACCORDANCE WITH IDOT HIGHWAY STANDARDS 701501.

STAGE 1A

- PLACE ALL CONSTRUCTION SIGNS, TEMPORARY PAVEMENT MARKINGS, AND BARRICADES AND SHIFT EASTBOUND TRAFFIC AS INDICATED.
- CONSTRUCT CONCRETE PAVEMENT AS SHOWN.
- THE MAXIMUM DURATION FOR THIS STAGE SHALL BE 7 CALENDAR DAYS.

STAGE 2

- PLACE ALL CONSTRUCTION SIGNS, TEMPORARY PAVEMENT MARKINGS, AND BARRICADES. ADJUST TEMPORARY TRAFFIC SIGNALS AND SHIFT TRAFFIC AS INDICATED.
- REMOVE EXISTING PAVEMENT, CURB AND GUTTER AND DRIVEWAYS ON THE NORTH SIDE OF PRATT BOULEVARD.
- CONSTRUCT CURB AND GUTTER, CONCRETE PAVEMENT, SIDEWALK AND ENTRANCES AS SHOWN.
- REDUCE RADIUS AT THE NORTHWEST CORNER TO COMPLETE CONSTRUCTION AT THE INTERSECTION. THE RADIUS SHALL ONLY BE REDUCED FOR A MAXIMUM OF 7 CALENDAR DAYS.

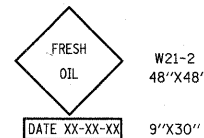
STAGE 3

- PLACE ALL CONSTRUCTION SIGNS, TEMPORARY PAVEMENT MARKINGS, AND BARRICADES. ADJUST TEMPORARY TRAFFIC SIGNALS AND SHIFT TRAFFIC AS INDICATED.
- IMPLEMENT DETOUR.
- CONSTRUCT TURN LANE AND MEDIAN ON PRATT BOULEVARD. RESTORE AREAS BEYOND RECONSTRUCTION LIMIT AFFECTED BY THE TEMPORARY PAVEMENT.
- REMOVE EXISTING MEDIAN AND CURB AND GUTTER ON IL ROUTE 83.
- CONSTRUCT CURB AND GUTTER AND BASE COURSE WIDENING ON IL ROUTE 83. DAILY LANE CLOSURES WILL BE ALLOWED BETWEEN THE HOURS OF 9:00 AM AND 3:00 PM IN ACCORDANCE WITH IDOT HIGHWAY STANDARDS 701421.
- REMOVE AND REPLACE SHOULDERS AT LOCATIONS SHOWN ON THE PLAN AND PROFILE SHEETS ON IL ROUTE 83. DAILY LANE CLOSURES WILL BE ALLOWED BETWEEN THE HOURS OF 9:00 AM AND 3:00 PM IN ACCORDANCE WITH IDOT HIGHWAY STANDARDS 701421.
- COMPLETE ALL LANDSCAPING.
- MILL EXISTING PAVEMENT ON IL ROUTE 83.
- PLACE LEVEL BINDER AND SURFACE COURSE ON IL ROUTE 83 MAINTAINING TRAFFIC PER STANDARD 701421.
- PLACE PERMANENT PAVEMENT MARKINGS, RAISED REFLECTIVE PAVEMENT MARKERS AND SIGNS.
- INSTALL AND ACTIVATE TRAFFIC SIGNALS AT THE INTERSECTION OF IL ROUTE 83 AND PRATT BOULEVARD. (NOTE: THE TEMPORARY SIGNALS SHALL BE TURNED OFF AT THE SAME TIME THE PERMANENT SIGNALS ARE ACTIVATED.)
- REMOVE CONSTRUCTION SIGNS AND OPEN ALL LANES TO TRAFFIC.

MAINTENANCE OF TRAFFIC GENERAL NOTES

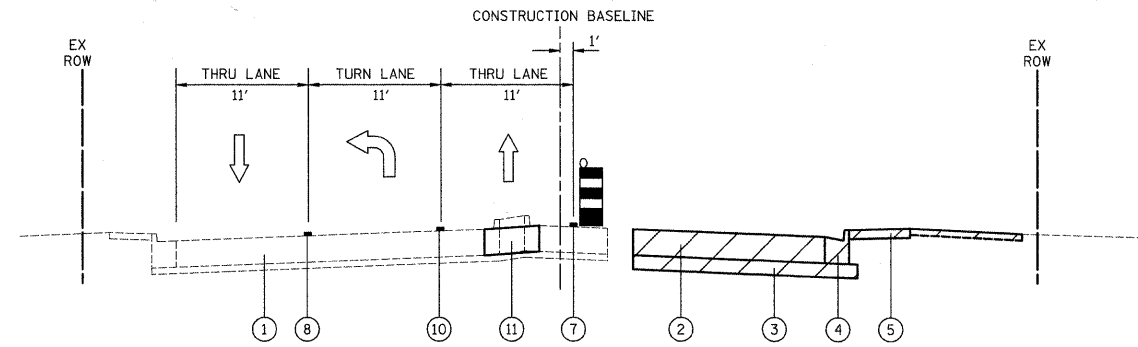
- TRAFFIC CONTROL DEPICTED IN THESE PLANS AND THE APPLICABLE IDOT DETAILS AND STANDARDS ARE THE MINIMUM REQUIREMENTS. OTHER WORK OR SIGNING MAY BE REQUIRED BY THE ENGINEER. TRAFFIC CONTROL AND PROTECTION SHALL BE PERFORMED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS, DIVISION 700; APPLICABLE GUIDELINES IN THE ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS; AND APPLICABLE HIGHWAY STANDARDS FOR TRAFFIC CONTROL, UNLESS HEREIN REVISED.
- THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND TRAFFIC CONTROL DEVICES SHALL FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.
- ALL CONSTRUCTION SIGNS SHALL HAVE FLUORESCENT ORANGE BACKGROUNDS.
- ALL SIGNS SHALL BE MOUNTED ON METAL POSTS, 7 FEET ABOVE THE EXISTING GROUND AND DRIVEN A MINIMUM OF 3 FEET INTO THE GROUND. A JUL.I.E. LOCATE SHALL BE PERFORMED PRIOR TO THE INSTALLATION OF THE POSTS.
- BARRICADES WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS WILL BE REQUIRED ADJACENT TO PAVEMENT EDGES WHERE WIDENING, CURB AND GUTTER OR OVERLAYING WORK IS BEING DONE, AS SPECIFIED IN SECTION 701 OF THE STANDARD SPECIFICATIONS. SPACING SHALL BE AS SHOWN ON THE CONSTRUCTION STAGING PLANS UNLESS OTHERWISE DIRECTED BY THE ENGINEER. BARRICADES THAT MUST BE PLACED IN EXCAVATED AREAS SHALL HAVE LEG EXTENSIONS INSTALLED SUCH THAT THE TOPS OF THE BARRICADES ARE IN COMPLIANCE WITH THE HEIGHT REQUIREMENTS OF STANDARD 701901.
- ALL BARRICADES OR DRUMS AT LANE DIVERSTIONS WITHIN TAPER SECTIONS SHALL HAVE DIRECTION INDICATOR PANELS.
- BARRICADES OR DRUMS EQUIPPED WITH ONE-WAY FLASHING LIGHTS WILL BE REQUIRED AT ALL OPEN TRENCHES, EXCAVATIONS, OPEN OR EXPOSED SEWER STRUCTURES, AND AT ANY OTHER LOCATIONS DESIGNATED BY THE ENGINEER OR LAW ENFORCEMENT AGENCIES. BARRICADES SHALL BE PLACED AT 50' CENTERS ALONG TANGENTS, 25' ALONG TAPERS AND 10' AROUND RADII.
- DRUMS SHALL HAVE ALTERNATING REFLECTORIZED TYPE AA OR TYPE AP FLUORESCENT ORANGE AND REFLECTORIZED WHITE HORIZONTAL, CIRCUMFERENTIAL STRIPES.
- DRUMS AND BARRICADES SHALL MEET THE REQUIREMENTS OF THE NATIONAL COOPERATIVE HIGHWAY RESEARCH PROGRAM (NCHRP) REPORT 350 AND THE SUPPLEMENTAL SPECIAL PROVISION "WORK ZONE TRAFFIC CONTROL DEVICES".
- TYPE III BARRICADES ARE TO BE PLACED IN ACCORDANCE WITH STANDARD 701901 UNLESS AUTHORIZED BY THE ENGINEER TO USE AN ALTERNATE ARRANGEMENT.
- THE CONTRACTOR SHALL INFORM THE ENGINEER OF ANY STAGE CHANGE AT LEAST TWO WEEKS IN ADVANCE OF THE CHANGE.
- EXISTING TRAFFIC CONTROL SIGNS AND DEVICES SHALL BE REMOVED OR RELOCATED BY THE CONTRACTOR AFTER THE TRAFFIC CONTROL REQUIREMENTS ARE MET OR AS AUTHORIZED BY THE ENGINEER. ANY SIGNS OR DEVICES LEFT IN PLACE ARE TO BE PROTECTED FROM DAMAGE AND MAINTAINED. ANY DAMAGE CAUSED BY HIS WORK SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER AT THE EXPENSE OF THE CONTRACTOR.
- THE FIRST TWO WARNING SIGNS IN EACH DIRECTION OF TRAVEL SHALL BE EQUIPPED WITH MONO-DIRECTIONAL AMBER FLASHING LIGHTS DURING HOURS OF DARKNESS. FLAGS ARE OPTIONAL.
- EXCEPT FOR APPROVED CLOSURES AS DEPICTED ON THE MAINTENANCE OF TRAFFIC PLANS, ALL ROADS SHALL BE KEPT OPEN TO TRAFFIC DURING THE ENTIRE CONSTRUCTION PERIOD. THE CONTRACTOR MAY CLOSE ONE LANE OF TRAFFIC (BECAUSE OF CONSTRUCTION) ONLY BETWEEN THE HOURS OF 9:00 AM AND 3:00 PM.

WHEN NECESSARY TO CLOSE ONE LANE OF THE ROADWAY ON TWO-LANE ROADS, THE CONTRACTOR SHALL MAINTAIN TWO-WAY TRAFFIC DURING THE RESTRICTED HOURS WITH THE USE OF SIGNS AND FLAGGERS AS SHOWN ON THE TRAFFIC CONTROL STANDARDS. WHEN NECESSARY TO CLOSE ONE LANE OF THE ROADWAY ON FOUR-LANE ROADS, THE CONTRACTOR SHALL MAINTAIN TWO-WAY TRAFFIC DURING THE RESTRICTED HOURS WITH THE USE OF SIGNS AND BARRICADES AS SHOWN ON THE TRAFFIC CONTROL STANDARDS. ALL EXISTING LANES OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED BETWEEN 3:00 PM AND 9:00 AM WHEN NO CONSTRUCTION ACTIVITIES ARE BEING CARRIED ON. THE ENGINEER MAY WAIVE THE LANE CLOSURE TIME RESTRICTION AT HIS DISCRETION.
- "WORKERS" SIGNS SHALL ONLY BE ERECTED WHEN WORKERS ARE PRESENT. SIGN MUST BE COVERED OR REMOVED WHEN NO WORKERS ARE PRESENT.
- "FRESH OIL" SIGNS (W21-2-4848) WITH DATE SIGNS SHALL BE ERECTED 48 HOURS PRIOR TO PRIMING ALONG IL ROUTE 83. THE COST OF THESE SIGNS SHALL BE INCLUDED IN THE PAY ITEM "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".
- THE CONTRACTOR SHALL ERECT TEMPORARY STREET NAME SIGNS ON METAL POSTS THROUGHOUT CONSTRUCTION TO THE SATISFACTION OF THE ENGINEER. THE COST OF THESE SIGNS SHALL BE INCLUDED IN THE PAY ITEM "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".
- TEMPORARY PAVEMENT MARKING TAPE SHALL BE USED ON ALL SURFACES OUTSIDE OF THE PROJECT LIMITS AND ON THE FINAL PAVEMENT SURFACE. THIS WORK SHALL BE PAID FOR AS "WET TEMPORARY PAVEMENT MARKING TAPE, TYPE III" OF THE SIZE SPECIFIED.
- ARROW BOARDS WILL BE REQUIRED WHEN IMPLEMENTING ALL LANE CLOSURES, AND SHALL BE INCLUDED IN THE PAY ITEM "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".
- THE COST OF SUPPLYING, ERECTING, AND MAINTAINING BARRICADES, DRUMS, WARNING LIGHTS, AND SIGNS SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)". QUANTITIES FOR SHORT-TERM PAVEMENT MARKINGS, TEMPORARY PAVEMENT MARKINGS, AND WORK ZONE PAVEMENT MARKING REMOVAL ARE NOT INCLUDED IN THE ITEM "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)" AND SHALL BE MEASURED SEPARATELY FOR PAYMENT.
- A TOTAL QUANTITY OF FOUR EACH FOR "TEMPORARY ACCESS (ROAD)" HAS BEEN INCLUDED FOR USE IN PROVIDING PEDESTRIAN ACCESS ACROSS THE INTERSECTION OF IL ROUTE 83 AND PRATT BOULEVARD.
- ALL TYPE III BARRICADES SHALL HAVE 2 AMBER TYPE A-LOW INTENSITY FLASHING LIGHTS SPACED NEAR THE CENTERLINES OF THE SUPPORTS.
- DROP-OFFS ADJACENT TO THE TRAVEL LANES SHALL BE KEPT TO A MINIMUM. DROP-OFFS GREATER THAN 18" WILL NOT BE ALLOWED WHEN TRAFFIC IS PRESENT IN THE ADJACENT LANE. THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE EXCAVATION REQUIRED FOR THE CONSTRUCTION OF THE WIDENING DURING THE HOURS THAT THE ADJACENT LANE IS CLOSED, AS NOTED ABOVE. PRIOR TO RE-OPENING THE LANE TO TRAFFIC THE CONTRACTOR SHALL PLACE SUFFICIENT MATERIAL TO REDUCE THE DROP-OFF TO LESS THAN 18". THE CONTRACTOR SHALL BE RESPONSIBLE TO DETERMINE THE AMOUNT OF WORK THAT CAN BE COMPLETED WITHIN THE TIME LIMIT OF THE DAILY LANE CLOSURE. NO ADDITIONAL COMPENSATION SHALL BE ALLOWED TO COMPLY WITH THIS REQUIREMENT.

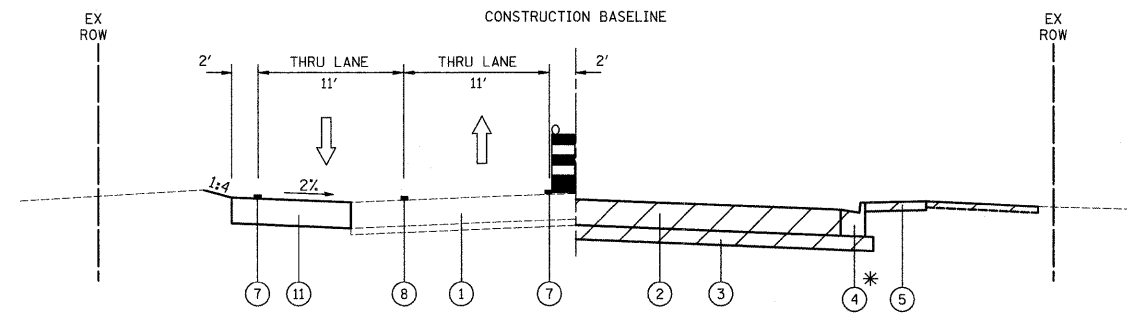


TO BE PLACED AS DIRECTED BY THE ENGINEER

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PLOT DATE = 10/22/2010	DATE = 10-22-10	REVISIONS =	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(660)							
			SHEET NO. 1 OF 2 SHEETS							

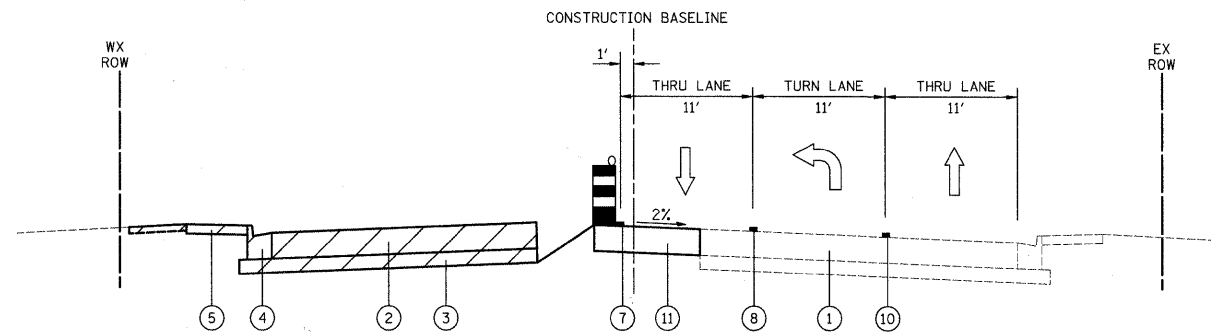


**STAGE 1 TYPICAL SECTION
PRATT BOULEVARD
SECTION A-A**

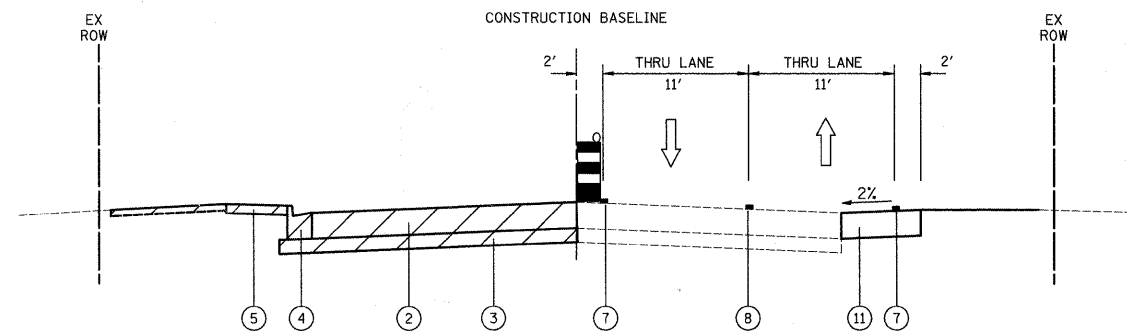


**STAGE 1 TYPICAL SECTION
PRATT BOULEVARD
SECTION B-B**

* SEE MOT PLAN VIEW FOR LIMITS OF CURB AND GUTTER TO BE CONSTRUCTED DURING STAGE 1



**STAGE 2 TYPICAL SECTION
PRATT BOULEVARD
SECTION A-A**



**STAGE 2 TYPICAL SECTION
PRATT BOULEVARD
SECTION B-B**

LEGEND

- ① EXISTING PAVEMENT
- ② PROPOSED PCC PAVEMENT
- ③ PROPOSED SUB-BASE GRANULAR MATERIAL
- ④ PROPOSED COMB. CONC. CURB AND GUTTER
- ⑤ PROPOSED PCC SIDEWALK
- ⑥ PROPOSED CONCRETE MEDIAN
- ⑦ LINE 4" (WHITE EDGE LINE)
- ⑧ LINE 4" (DOUBLE YELLOW)
- ⑨ LINE 4" (WHITE- 10' DASH - 30' SKIP)
- ⑩ LINE 6" (WHITE LANE LINE, SOLID OR DOTTED)
- ⑪ TEMPORARY PAVEMENT
- ⑫ LINE 4" (YELLOW EDGE LINE)

CONSTRUCTION ZONE

TRAFFIC FLOW

DRUMS WITH MONO DIRECTIONAL STEADY BURN LIGHT

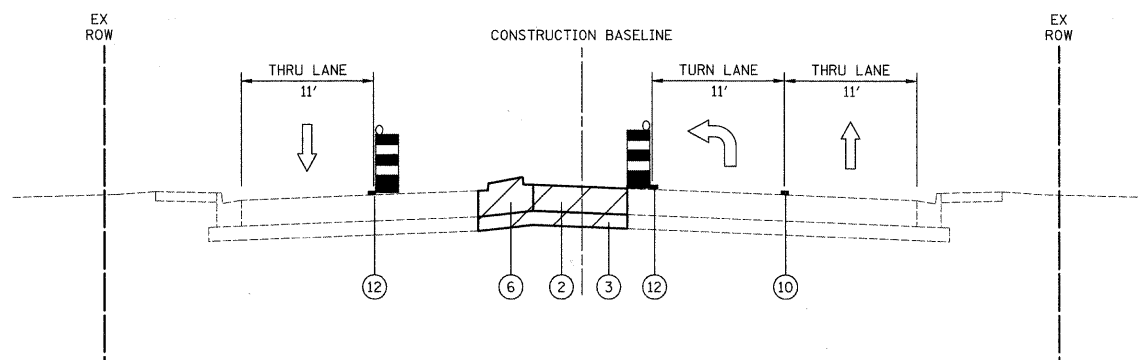
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		DATE - 10-22-10	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

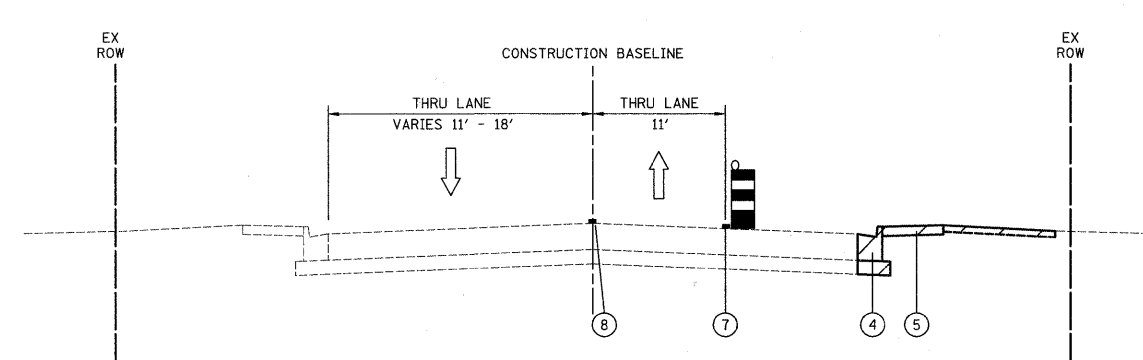
**IL ROUTE 83 (BUSSE ROAD) AND PRATT BOULEVARD
MAINTENANCE OF TRAFFIC TYPICAL SECTIONS**

SCALE: N.T.S. SHEET NO. 1 OF 2 SHEETS

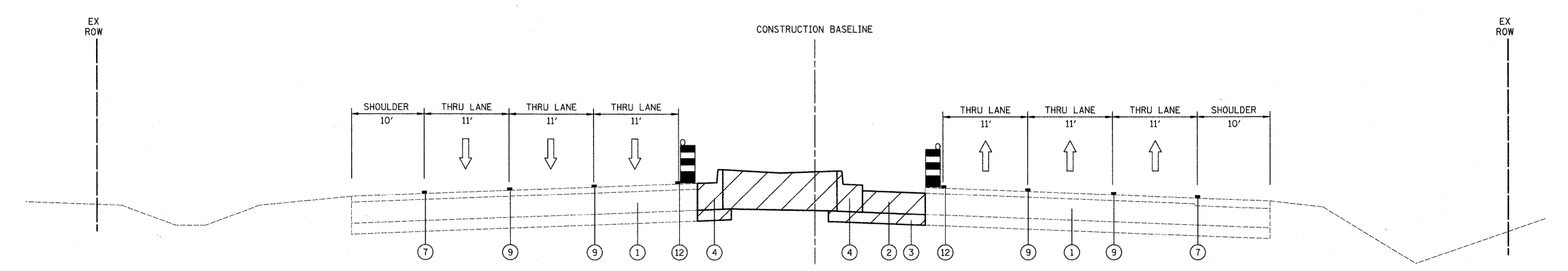
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CONTRACT NO. 63523				
<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003660</small>				



**STAGE 3 TYPICAL SECTION
PRATT BOULEVARD**
SECTION A-A



**STAGE 3 TYPICAL SECTION
PRATT BOULEVARD**
SECTION B-B



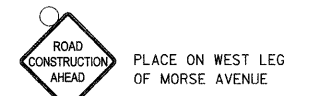
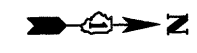
**STAGE 3 TYPICAL SECTION
IL ROUTE 83 (BUSSE ROAD)**
SECTION C-C

- LEGEND**
- ① EXISTING PAVEMENT
 - ② PROPOSED PCC PAVEMENT
 - ③ PROPOSED SUB-BASE GRANULAR MATERIAL
 - ④ PROPOSED COMB. CONC. CURB AND GUTTER
 - ⑤ PROPOSED PCC SIDEWALK
 - ⑥ PROPOSED CONCRETE MEDIAN
 - ⑦ LINE 4" (WHITE EDGE LINE)
 - ⑧ LINE 4" (DOUBLE YELLOW)
 - ⑨ LINE 4" (WHITE - 10' DASH - 30' SKIP)
 - ⑩ LINE 6" (WHITE LANE LINE, SOLID OR DOTTED)
 - ⑪ TEMPORARY PAVEMENT
 - ⑫ LINE 4" (YELLOW EDGE LINE)
- ▨ CONSTRUCTION ZONE
- ↑ TRAFFIC FLOW
- ⊞ DRUMS WITH MONO DIRECTIONAL STEADY BURN LIGHT

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	PLOT SCALE = 50.0000' / IN.	DRAWN - BLG	REVISED -			344	09-00057-00-CH	COOK	103	25
PLOT DATE = 10/21/2010	CHECKED - DJK	DATE - 10-22-10	REVISED -	SCALE: N.T.S.	SHEET NO. 2 OF 2 SHEETS	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-90036601				
						CONTRACT NO. 63523				

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W20-1(0)-3636
M6-1L(0)-2115



W20-1(0)-4848

W20-1(0)-4848

TEMPORARY INFORMATION SIGN-
LOCATE AS DIRECTED
BY THE ENGINEER
(SEE SHEET 79 FOR DETAILS)

MATCH LINE STA. 407+50
(SEE SHEET NO. 27)

407+60.50
10.00' LT
4" WHITE EDGE LINE

W10-1
36" DIA.

6" WHITE
4" DOUBLE YELLOW

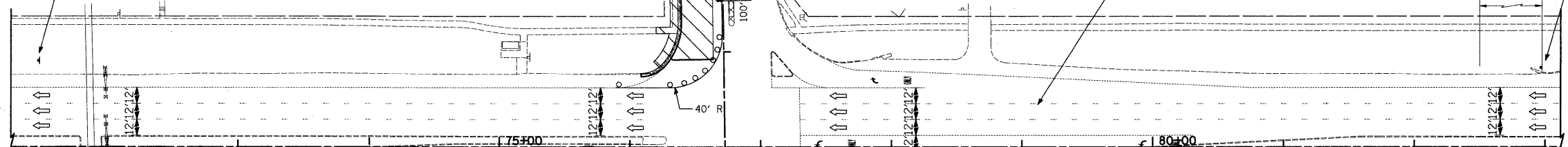
24" WHITE STOP BAR

408+60.50
21.00' LT

EXISTING PAVEMENT MARKINGS TO REMAIN

END CONSTRUCTION
G20-2-2460

IL ROUTE 83
BUSSE ROAD



TEMPORARY INFORMATION SIGN-
LOCATE AS DIRECTED BY THE ENGINEER
(SEE SHEET 79 FOR DETAILS)



W20-1(0)-4848

W20-1(0)-4848



RIGHT TURN LANE TO BE CLOSED



PLACE ON EAST LEG OF ARTHUR AVENUE

W20-1(0)-3636
M6-1L(0)-2115



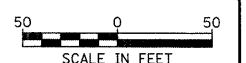
MATCH LINE STA. 415+00
(SEE SHEET NO. 27)

LEGEND

- TYPE II DRUM
- ⇨ DIRECTION OF TRAFFIC
- ⊥ TEMPORARY TRAFFIC SIGN ON PERMANENT SUPPORT
- ⊥ TYPE III BARRICADE
- ⇨ ARROW BOARD
- ▨ WORK ZONE
- ▩ TEMPORARY PAVEMENT

NOTE:

1. ALL TAPE PAVEMENT MARKINGS SHALL BE PAID FOR AS "WET TEMPORARY PAVEMENT MARKING TAPE, TYPE III".
 2. ADDITIONAL SIGNS WILL BE REQUIRED FOR LANE CLOSURES WHICH ARE NOT SHOWN ON THE PLANS BUT WILL BE REQUIRED AS PER I.D.O.T. STANDARD 70101.
- * SEE SHEET NO. 80 FOR SIGN DETAILS.



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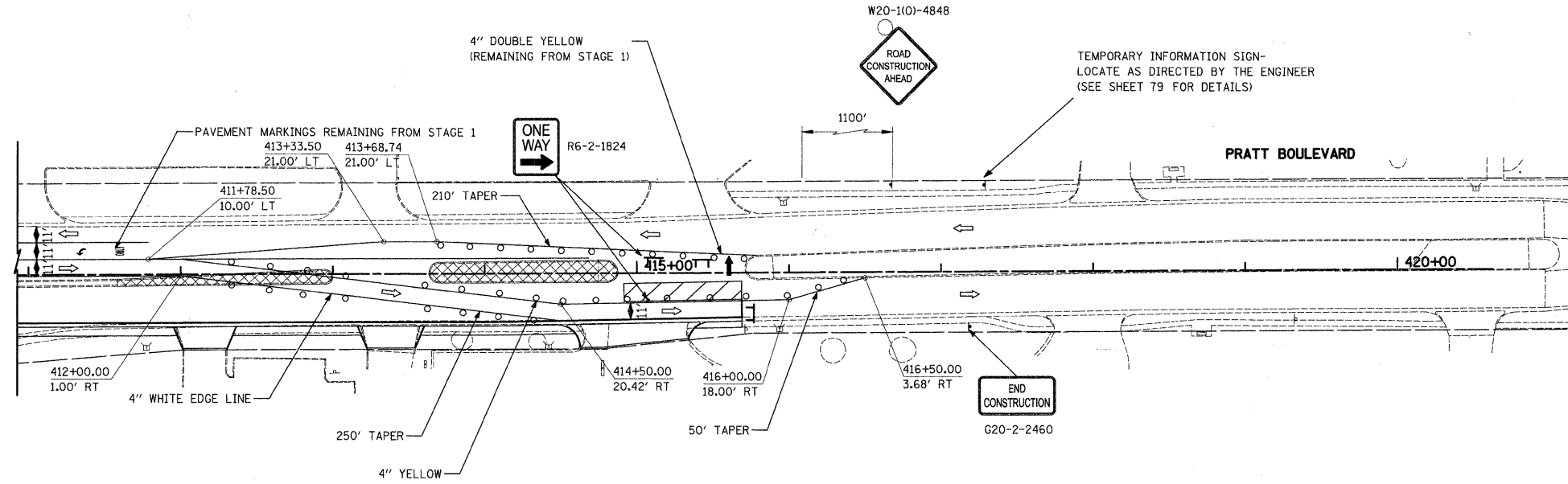
**IL ROUTE 83 (BUSSE ROAD) AND PRATT BOULEVARD
MAINTENANCE OF TRAFFIC - STAGE 1**

SCALE: 1" = 50' SHEET NO. 1 OF 2 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	09-00057-00-CH	COOK	103	26
CONTRACT NO. 63523				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003660				

PLAN	DESIGNED	DATE
	PLOTTED	
	ALIGNED	
	CHECKED	
	ROAD FILE NAME	
	NO.	

PROFILE	DESIGNED	DATE
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	GRADES CHECKED	
	BLM. NOTED	
	STRUCTURE NOTATIONS CHKD	
	NO.	

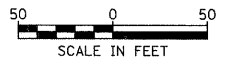


LEGEND

- TYPE II DRUM
- ⇨ DIRECTION OF TRAFFIC
- ⊕ TEMPORARY TRAFFIC SIGN ON PERMANENT SUPPORT
- ⊥ TYPE III BARRICADE
- ➔ ARROW BOARD
- ▨ WORK ZONE
- ▩ TEMPORARY PAVEMENT

NOTE:

1. ALL TAPE PAVEMENT MARKINGS SHALL BE PAID FOR AS "WET TEMPORARY PAVEMENT MARKING TAPE, TYPE III".
 2. ADDITIONAL SIGNS WILL BE REQUIRED FOR LANE CLOSURES WHICH ARE NOT SHOWN ON THE PLANS BUT WILL BE REQUIRED AS PER I.D.O.T. STANDARD 701701.
- * SEE SHEET NO. 80 FOR SIGN DETAILS.



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PLOT DATE = 10/22/2010	DATE - 10-22-10	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

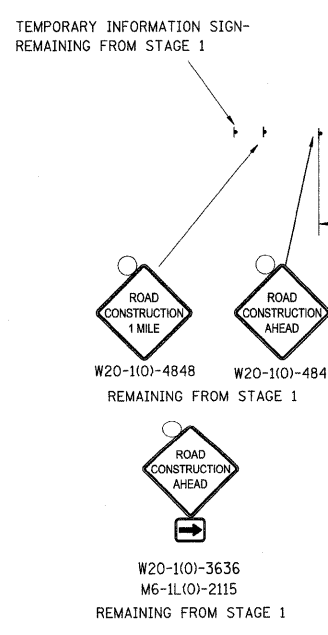
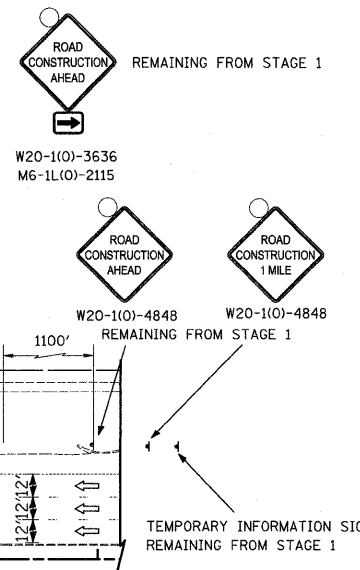
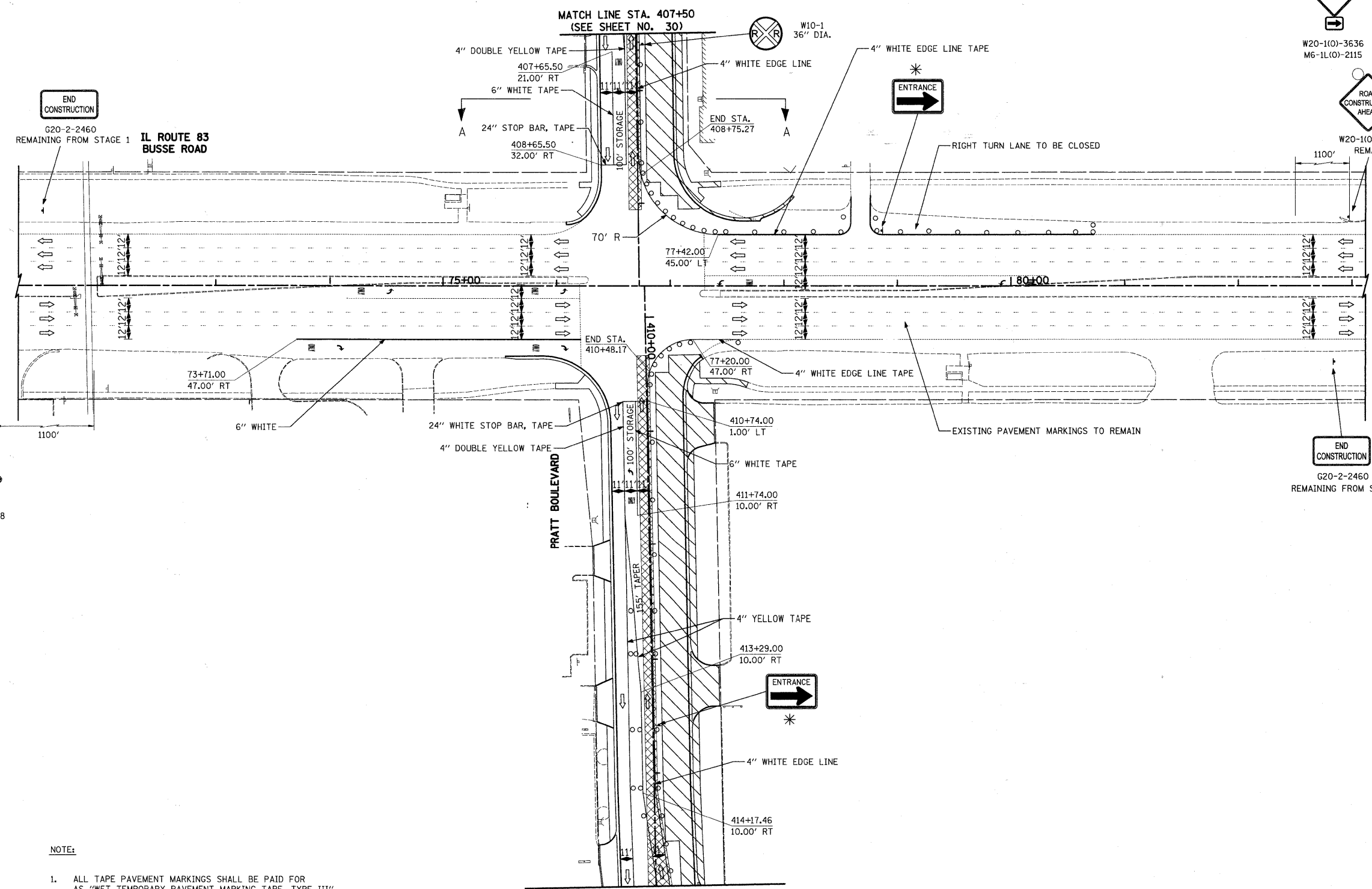
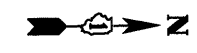
**IL ROUTE 83 (BUSSE ROAD) AND PRATT BOULEVARD
MAINTENANCE OF TRAFFIC - STAGE 1A**

SCALE: 1" = 50' SHEET NO. 1 OF 1 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	09-00057-00-CH	COOK	103	28
				CONTRACT NO. 63523
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003660				

DATE	
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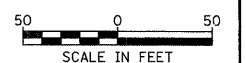


LEGEND

- TYPE II DRUM
- ⇨ DIRECTION OF TRAFFIC
- ⊥ TEMPORARY TRAFFIC SIGN ON PERMANENT SUPPORT
- ⊥ TYPE III BARRICADE
- ➔ ARROW BOARD
- ▨ WORK ZONE
- ▩ TEMPORARY PAVEMENT

NOTE:

1. ALL TAPE PAVEMENT MARKINGS SHALL BE PAID FOR AS "WET TEMPORARY PAVEMENT MARKING TAPE, TYPE III".
 2. ADDITIONAL SIGNS WILL BE REQUIRED FOR LANE CLOSURES WHICH ARE NOT SHOWN ON THE PLANS BUT WILL BE REQUIRED AS PER I.D.O.T. STANDARD 701701.
- * SEE SHEET NO. 80 FOR SIGN DETAILS.



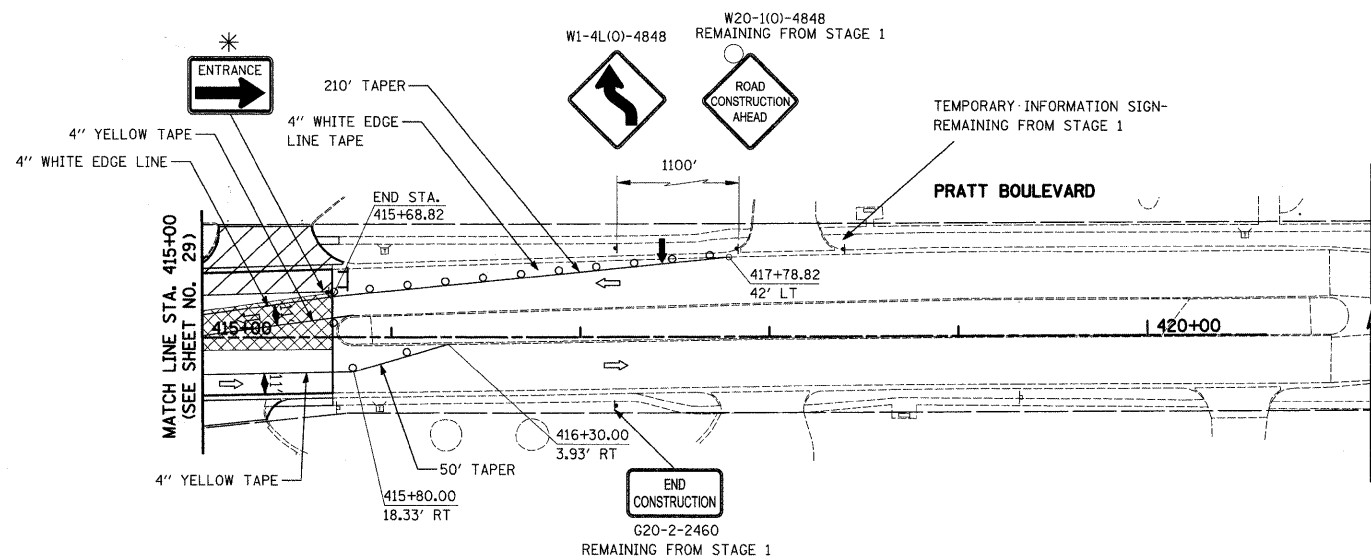
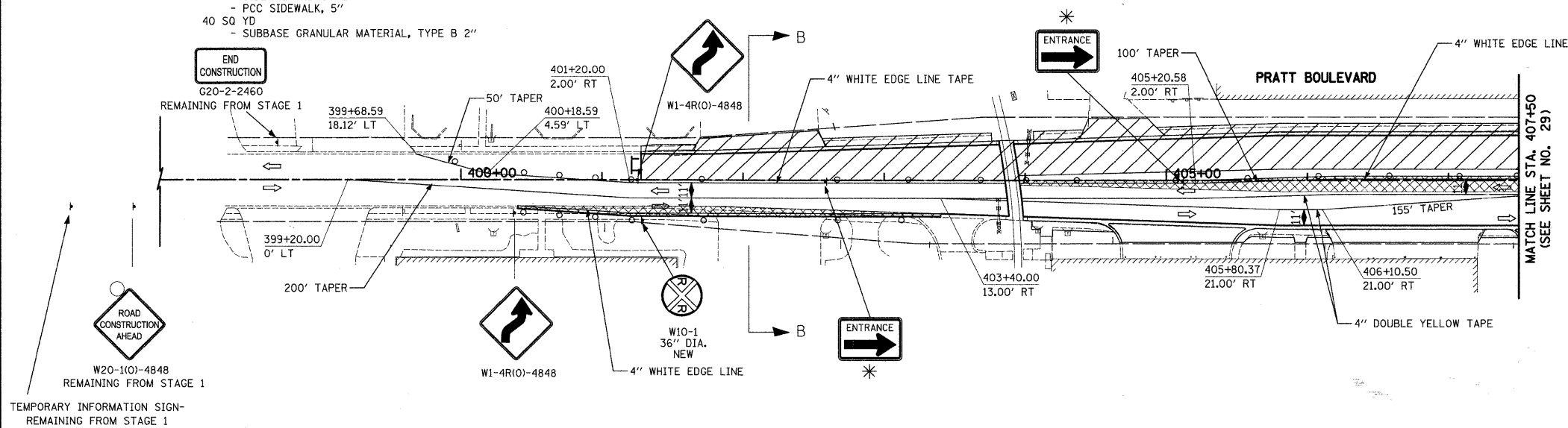
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PLOT SCALE = 50.0000' / IN.	CHECKED - DJK	REVISED -	SCALE: 1" = 50'			SHEET NO. 1 OF 2 SHEETS	CONTRACT NO. 63523				
PLOT DATE = 10/22/2010	DATE - 10-22-10	REVISED -	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-90036601								

REMOVAL AND REPLACEMENT INCLUDED WEST OF RECONSTRUCTION LIMIT:
 87 FT
 - CURB AND GUTTER REMOVAL
 - COMBINATION CONCRETE CURB AND GUTTER TYPE B-6.12
 25 SQ YD
 - SUBBASE GRANULAR MATERIAL, TYPE B 6"
 364 SQ FT
 - SIDEWALK REMOVAL
 - PCC SIDEWALK, 5"
 40 SQ YD
 - SUBBASE GRANULAR MATERIAL, TYPE B 2"



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REGISTERED	
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ALIGNMENT CHECKED	
GRADES CHECKED	
NOTE BOOK	
NO.	
ADD FILE NAME	

DATE	
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REGISTERED	
PLOTTED	
GRADES CHECKED	
PLAN NOTED	
STRUCTURE NOTATIONS CHECKED	



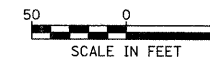
LEGEND

- TYPE II DRUM
- ⇒ DIRECTION OF TRAFFIC
- ⊥ TEMPORARY TRAFFIC SIGN ON PERMANENT SUPPORT
- ⊥ TYPE III BARRICADE
- ARROW BOARD
- ▨ WORK ZONE
- ▩ TEMPORARY PAVEMENT

NOTE:

1. ALL TAPE PAVEMENT MARKINGS SHALL BE PAID FOR AS "WET TEMPORARY PAVEMENT MARKING TAPE, TYPE III".
2. ADDITIONAL SIGNS WILL BE REQUIRED FOR LANE CLOSURES WHICH ARE NOT SHOWN ON THE PLANS BUT WILL BE REQUIRED AS PER I.D.O.T. STANDARD 701701.

* SEE SHEET NO. 80 FOR SIGN DETAILS.



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 PLOT DATE = 10/22/2010

CHECKED - DJK
 DATE - 10-22-10

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**IL ROUTE 83 (BUSSE ROAD) AND PRATT BOULEVARD
 MAINTENANCE OF TRAFFIC - STAGE 2**

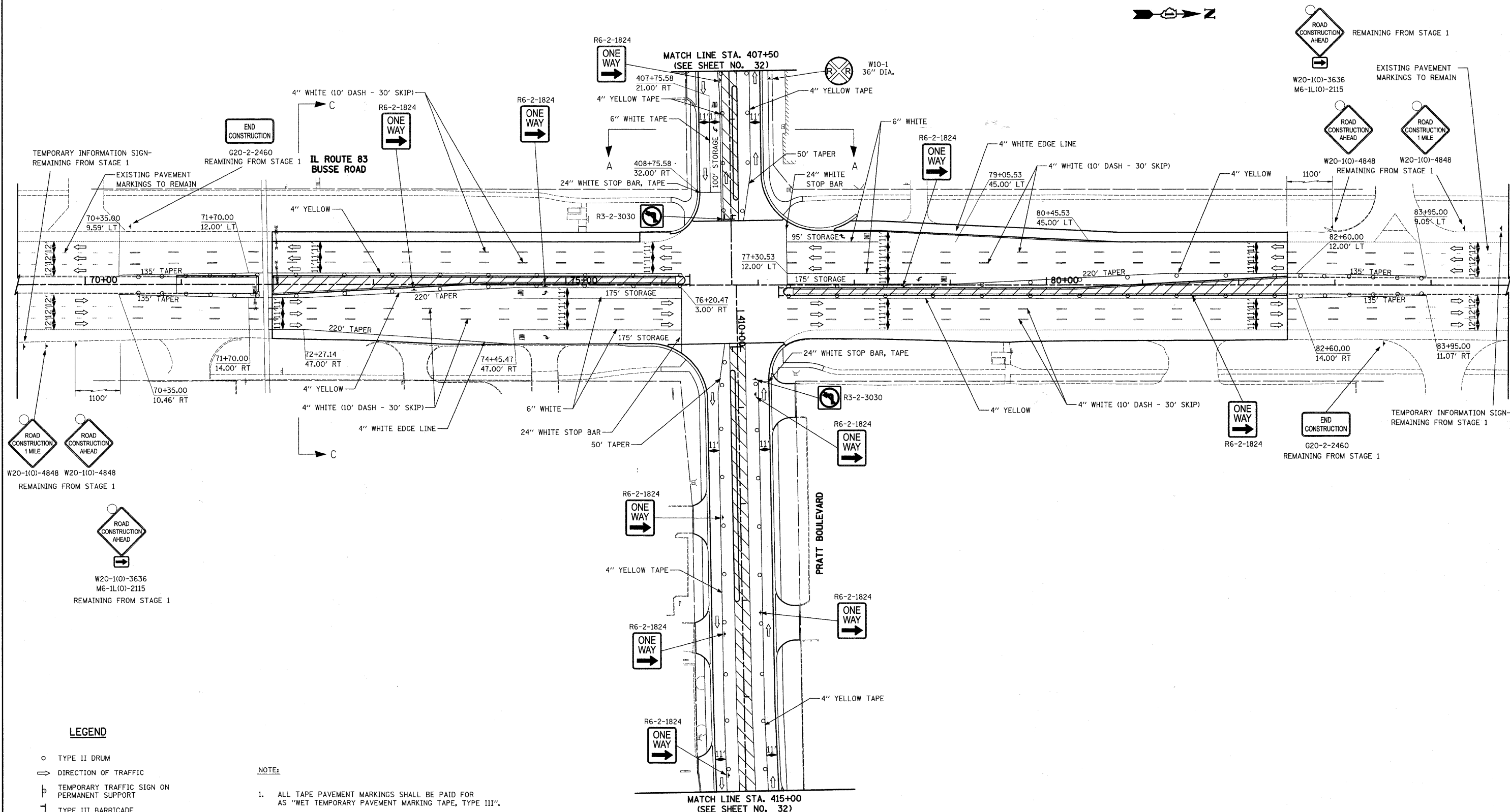
SCALE: 1" = 50' SHEET NO. 2 OF 2 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	09-00057-00-CH	COOK	103	30
CONTRACT NO. 63523				

FED. ROAD DIST. NO. 1 | ILLINOIS FED. AID PROJECT M-9003(660)

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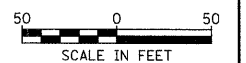
LEGEND

- TYPE II DRUM
- ⇨ DIRECTION OF TRAFFIC
- ⊥ TEMPORARY TRAFFIC SIGN ON PERMANENT SUPPORT
- ⊥ TYPE III BARRICADE
- ⇨ ARROW BOARD
- ▨ WORK ZONE
- ▩ TEMPORARY PAVEMENT

NOTE:

- ALL TAPE PAVEMENT MARKINGS SHALL BE PAID FOR AS "WET TEMPORARY PAVEMENT MARKING TAPE, TYPE III".
- ADDITIONAL SIGNS WILL BE REQUIRED FOR LANE CLOSURES WHICH ARE NOT SHOWN ON THE PLANS BUT WILL BE REQUIRED AS PER I.D.O.T. STANDARD 701701.

* SEE SHEET NO. 80 FOR SIGN DETAILS.



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	DATE - 10-22-10	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

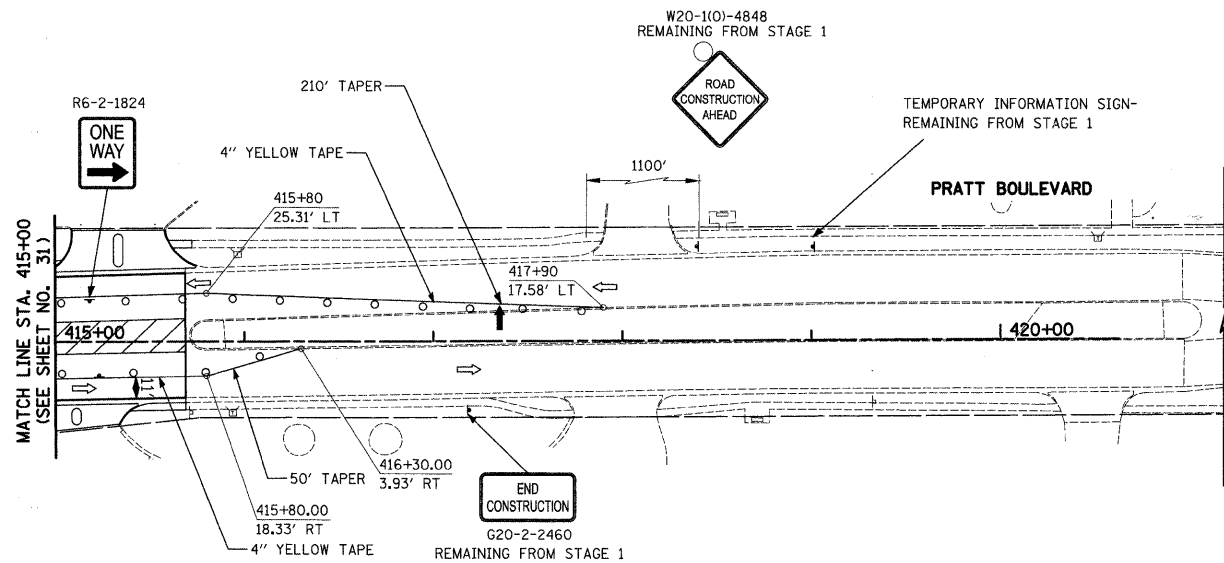
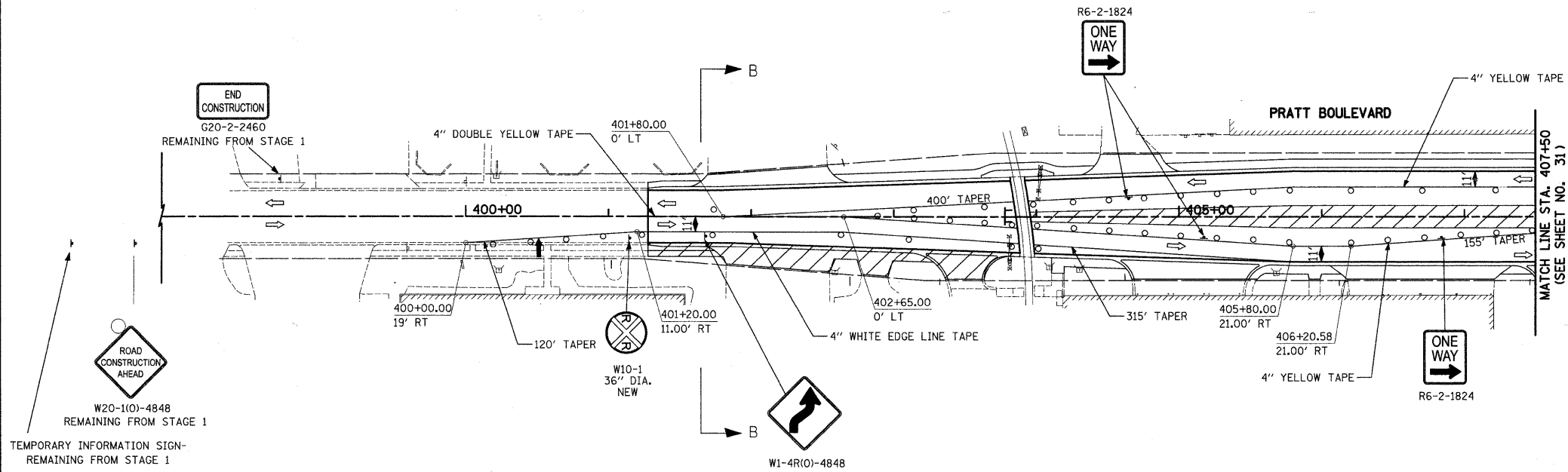
**IL ROUTE 83 (BUSSE ROAD) AND PRATT BOULEVARD
MAINTENANCE OF TRAFFIC - STAGE 3**

SCALE: 1" = 50' SHEET NO. 1 OF 2 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	09-00057-00-CH	COOK	103	31
CONTRACT NO. 63523				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-90036601				

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DESCRIPTION	
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PROFILE	
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DESCRIPTION	
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LEGEND

- TYPE II DRUM
- ⇨ DIRECTION OF TRAFFIC
- ⊞ TEMPORARY TRAFFIC SIGN ON PERMANENT SUPPORT
- ⊥ TYPE III BARRICADE
- ➔ ARROW BOARD
- ▨ WORK ZONE
- ▩ TEMPORARY PAVEMENT

NOTES

1. ALL TAPE PAVEMENT MARKINGS SHALL BE PAID FOR AS "WET TEMPORARY PAVEMENT MARKING TAPE, TYPE III".
 2. ADDITIONAL SIGNS WILL BE REQUIRED FOR LANE CLOSURES WHICH ARE NOT SHOWN ON THE PLANS BUT WILL BE REQUIRED AS PER I.D.O.T. STANDARD 701701.
- * SEE SHEET NO. 80 FOR SIGN DETAILS.

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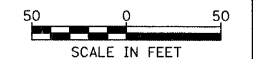
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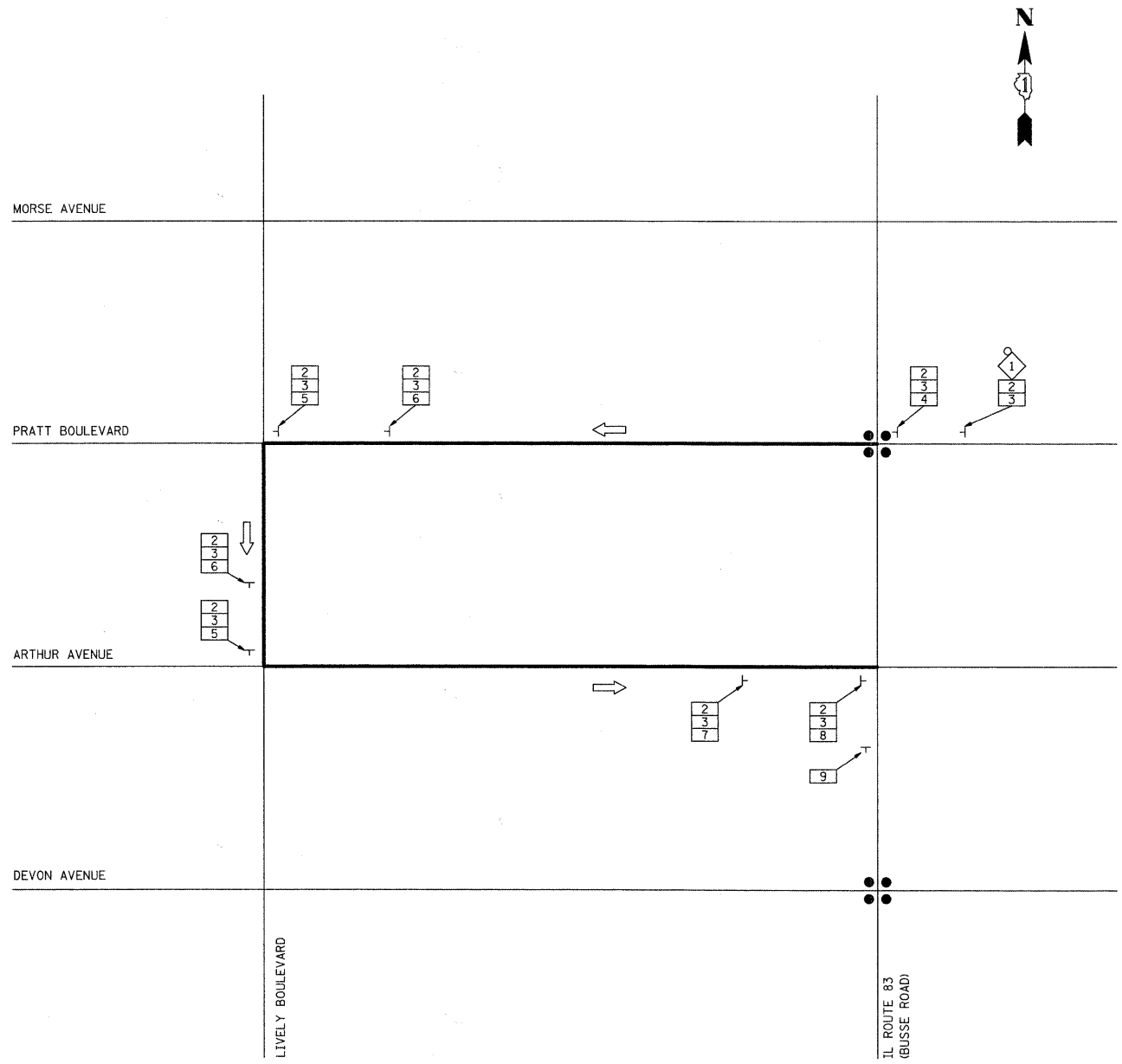
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**IL ROUTE 83 (BUSSE ROAD) AND PRATT BOULEVARD
 MAINTENANCE OF TRAFFIC - STAGE 3**

SCALE: 1" = 50' SHEET NO. 2 OF 2 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	09-00057-00-CH	COOK	103	32
CONTRACT NO. 63523				
<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-90036601</small>				





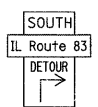
LEGEND

- SIGNALIZED INTERSECTION
- 48" X 48" CONSTRUCTION WARNING SIGN WITH AMBER FLASHING LIGHT (NUMBER DENOTES TYPE)
- M4-9 SERIES DETOUR SIGN WITH ROAD NAME & DIRECTION PLATES (NUMBER DENOTES TYPE)
- OTHER DETOUR SIGN (NUMBER DENOTES TYPE)
- DETOUR ROUTE
- DETOUR ROUTE DIRECTION
- SIGN POST

SCHEDULE OF SIGNS

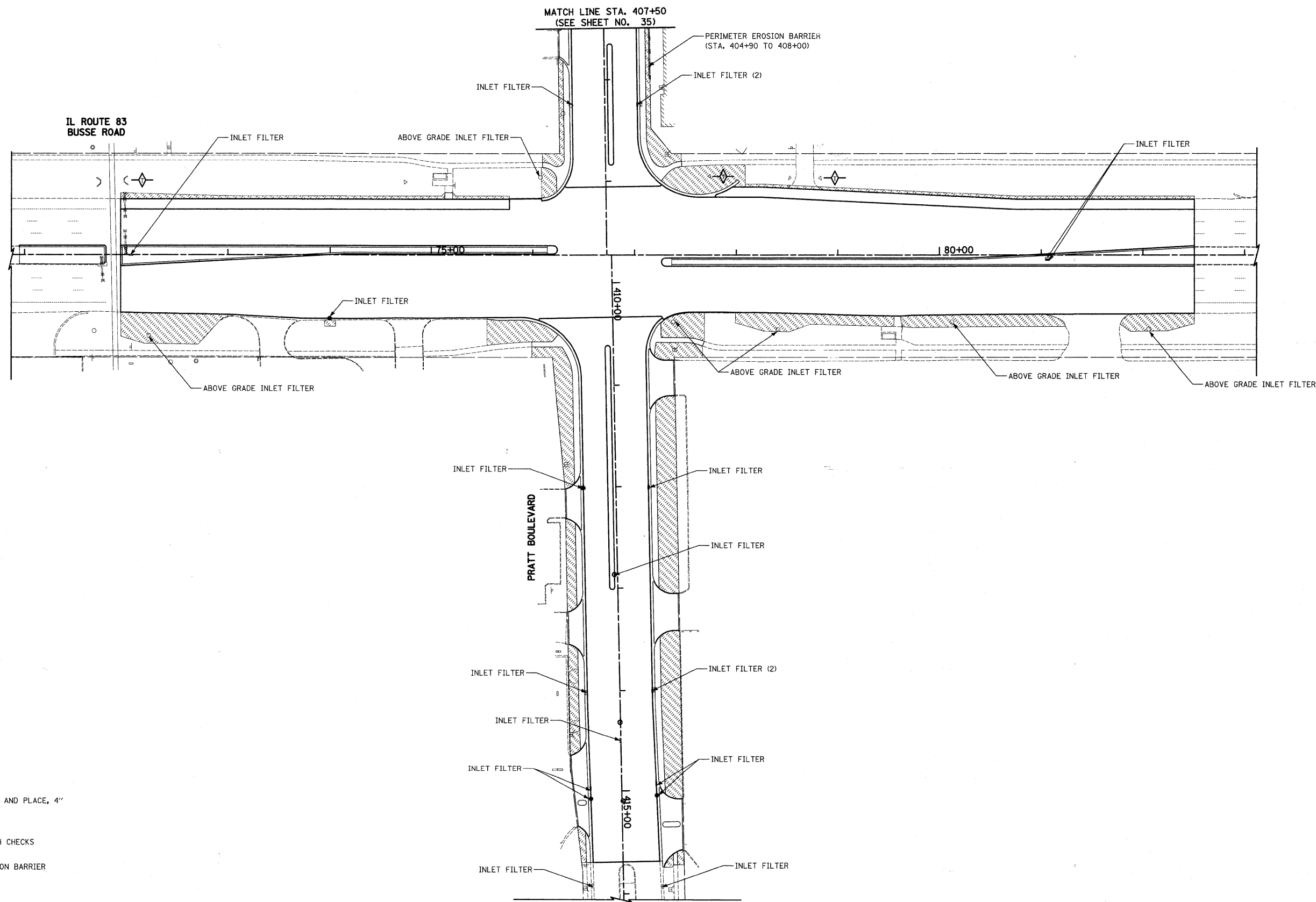
SIGN NO.	SIGN TYPE	TYPE
1		W20-210-4848
2		M3-210-2412
3		W17-1100-2412
4		M4-910-3030
5		M4-9L10-3024
6		M4-9L10-3030
7		M4-9R10-3030
8		M4-9R10-3024
9		M4-8a10-2418

TYPICAL SIGN ORIENTATION







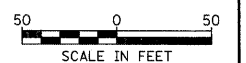
DETOUR GENERAL NOTES

1. THE ENGINEER SHALL BE NOTIFIED IN WRITING AT LEAST THREE WEEKS PRIOR TO THE DAY THE DETOUR IS TO BE IN EFFECT. THE CONTRACTOR SHALL ALSO CONTACT THE ELK GROVE VILLAGE POLICE DEPARTMENT (847-357-4100) AND ELK GROVE VILLAGE FIRE DEPARTMENT (847-734-8000).
2. ALL SIGNING SHALL BE IN ACCORDANCE WITH THE APPLICABLE PROVISIONS OF THE STANDARD SPECIFICATIONS, THE DETAILS IN THESE PLANS, THE LATEST EDITION OF THE STATE OF ILLINOIS "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES," AND AS DIRECTED BY THE ENGINEER.
3. THE SIZES OF ALL SIGNS NOT SPECIFIED IN THESE PLANS SHALL BE AS REQUIRED BY THE ILLINOIS "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES."
4. ADDITIONAL SIGNING AND/OR BARRICADES DEEMED NECESSARY BY THE ENGINEER SHALL BE PROVIDED AND INSTALLED AT NO ADDITIONAL COST.
5. THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH THE NAMES AND PHONE NUMBERS OF HIS REPRESENTATIVES ON THE CONSTRUCTION SITE, AND HIS REPRESENTATIVE RESPONSIBLE FOR THE DETOUR SIGNING, PRIOR TO THE START OF WORK.
6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE FIELD LOCATION OF ALL DETOUR AND CONSTRUCTION SIGNING. THE CONTRACTOR MAY REQUEST THE ENGINEER TO FIELD VERIFY THE POSITIONS OF ANY SIGNS.
7. ACTUAL LOCATIONS FOR SIGNING SHOWN ON THE DETOUR PLANS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
8. ALL EXISTING SIGNING THAT IS NOT APPLICABLE WHILE THE DETOUR IS IN EFFECT SHALL BE COMPLETELY COVERED BY THE CONTRACTOR IN A MANNER MEETING THE APPROVAL OF THE ENGINEER.
9. ALL DETOUR SIGNING SHALL BE POST MOUNTED.
10. ALL DETOUR SIGNING EXCEPT REGULATORY SIGNS SHALL HAVE BLACK LEGENDS ON FLUORESCENT ORANGE SHEETING AND STANDARD BLACK BORDERS. THE FLUORESCENT ORANGE REFLECTIVE SHEETING SHALL MEET THE REQUIREMENTS OF ARTICLE 1106.01 OF THE STANDARD SPECIFICATIONS. ALL DETOUR SIGNING SHALL BE NEW OR IN LIKE-NEW CONDITION. THE ENGINEER SHALL BE THE SOLE JUDGE OF THE CONDITION OF THE SIGNS.
11. THE ROAD NAME SIGN SHALL BE A BLACK LEGEND ON ORANGE REFLECTIVE SHEETING. THE SIGN BLANK SHALL BE VARIABLE WITH DESIGN SERIES C LETTERS. THE CAPITAL LETTERS SHALL BE 6".
12. AT A MINIMUM, ALL AMBER FLASHING LIGHTS THAT ARE REQUIRED FOR THE DETOUR SIGNING SHALL MEET THE REQUIREMENTS FOR TYPE A-LOW INTENSITY FLASHING LIGHTS IN ARTICLE 1106.02 OF THE STANDARD SPECIFICATIONS. ALL LIGHTS SHALL OPERATE DURING HOURS OF DARKNESS. ONLY LIGHTS THAT HAVE BEEN APPROVED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION SHALL BE USED.
13. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THAT ALL BARRICADES, SIGNS, LIGHTS AND OTHER DEVICES INSTALLED BY HIM ARE IN PLACE AND OPERATING 24 HOURS EACH DAY, INCLUDING SUNDAYS AND HOLIDAYS.
14. THE TYPE III BARRICADES USED AT POINTS OF CLOSURE TO THRU TRAFFIC ONLY SHALL NOT EXCEED 8 FEET IN WIDTH EACH FOR A SINGLE APPROACH LANE. ALL BARRICADES AT THESE LOCATIONS SHALL HAVE REFLECTORIZED STRIPING ON THE BACK SIDES OF THE BARRICADES.
15. CONSTRUCTION EQUIPMENT SHALL NOT BE PARKED IMMEDIATELY BEHIND THE TYPE III BARRICADES DURING NON-WORKING HOURS. IN ANY EVENT, ARTICLE 701.11 OF THE STANDARD SPECIFICATIONS SHALL APPLY.
16. DURING NON-WORKING HOURS THE CONTRACTOR SHALL PROVIDE A MEANS TO RESTRAIN THE TYPE III BARRICADES FROM EASY MOVEMENT BY VANDALS. THE CHOSEN METHOD SHALL BE APPROVED BY THE ENGINEER.
17. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE VISIBILITY OF ALL DETOUR AND CONSTRUCTION SIGNS, INCLUDING BRUSHING BACK VEGETATION IF DEEMED NECESSARY BY THE ENGINEER.
18. THE ENGINEER SHALL BE NOTIFIED AT LEAST 24 HOURS BEFORE THE ROAD IS TO BE REOPENED TO TRAFFIC. THE CONTRACTOR WILL CONTACT THE APPROPRIATE LOCAL AGENCIES AND INTERESTED PARTIES.
19. THE COST OF THIS WORK FOR THE DETOUR SHALL BE INCLUDED IN THE UNIT PRICE FOR "TRAFFIC CONTROL AND PROTECTION FOR TEMPORARY DETOUR".



LEGEND

-  TOPSOIL FURNISH AND PLACE, 4" SODDING
-  TEMPORARY DITCH CHECKS
-  PERIMETER EROSION BARRIER
-  PROPOSED TREE



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	ALIGNMENT CHECKED	
	NOTE BOOK NO.	
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	NOTE BOOK NO.	
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

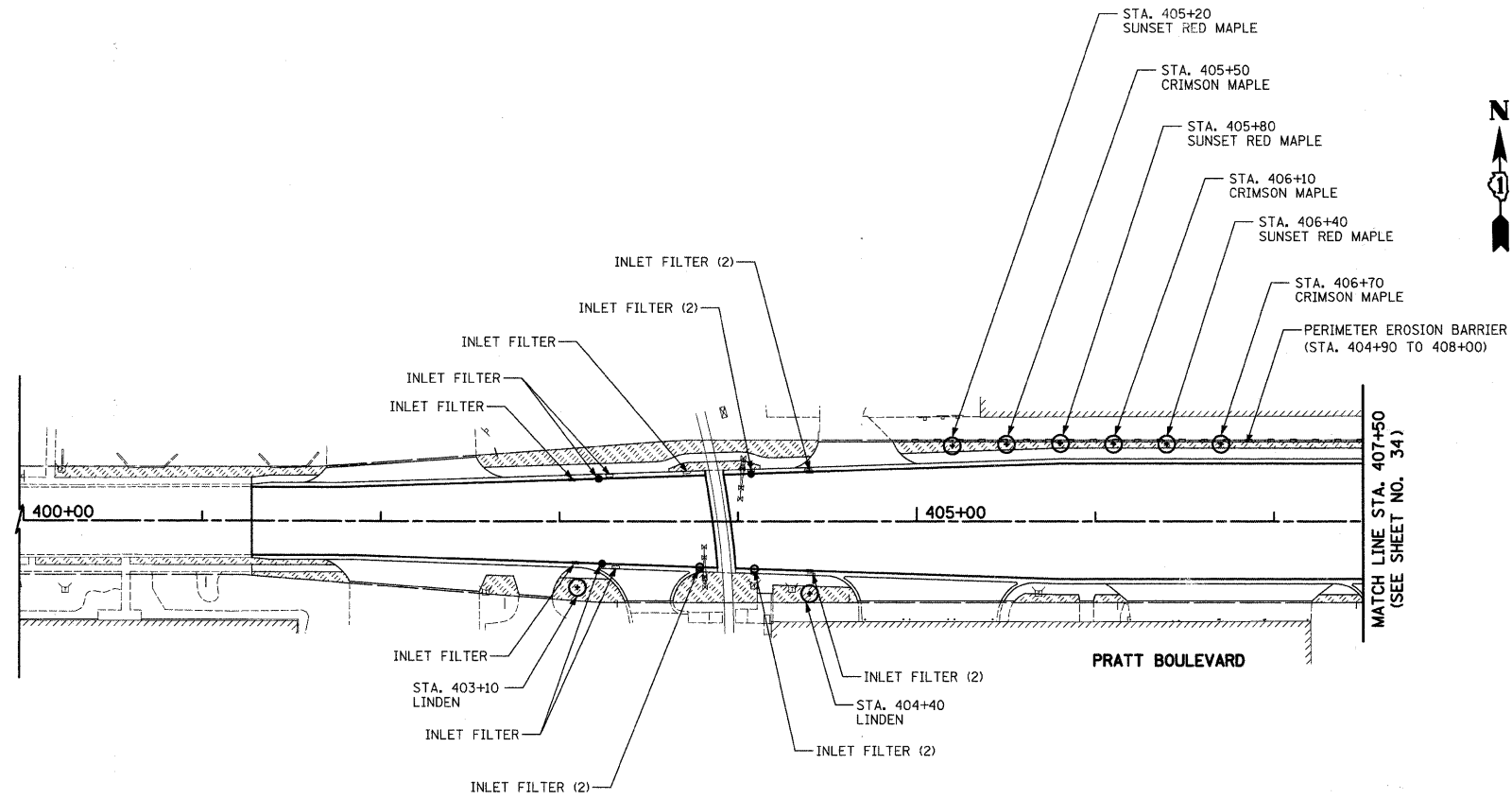
**IL ROUTE 83 (BUSSE ROAD) AND PRATT BOULEVARD
 EROSION CONTROL AND LANDSCAPING PLAN**

SCALE: 1" = 50' SHEET NO. 1 OF 2 SHEETS





F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	09-00057-00-CH	COOK	103	34
CONTRACT NO. 63523				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(660)				

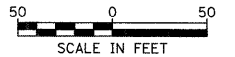
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PROFILE	DESIGNED	DATE
NOTE BOOK NO.	CHECKED	
	PLOTTED	
	GRADES CHECKED	
	BY, NOTED	
	STRUCTURE NOTATIONS CHK'D	



LEGEND

-  TOPSOIL FURNISH AND PLACE, 4" SODDING
-  TEMPORARY DITCH CHECKS
-  PERIMETER EROSION BARRIER
-  PROPOSED TREE



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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL ROUTE 83 (BUSSE ROAD) AND PRATT BOULEVARD
EROSION CONTROL AND LANDSCAPING**

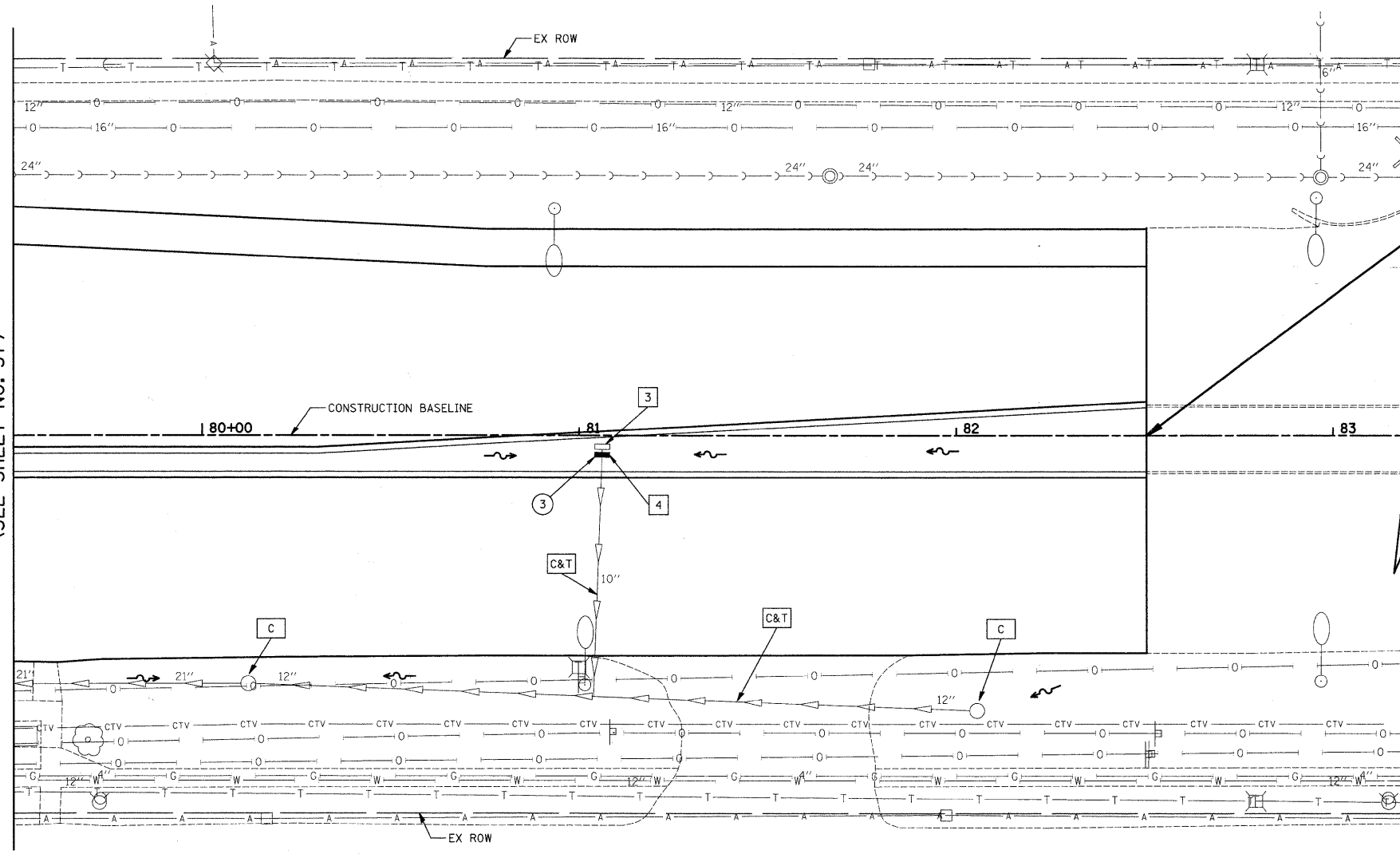
SCALE: 1" = 50' SHEET NO. 2 OF 2 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 63523				
<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(660)</small>				

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NOTE BOOK	
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ALIGNED CHECKED	
PLANNED FILE NAME	

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PROFILE	
NOTE BOOK	
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PLANNED FILE NAME	

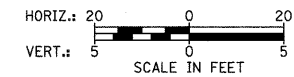
MATCH LINE STA. 79+50
(SEE SHEET NO. 37)



IMPROVEMENT ENDS
STA. 82+50.53

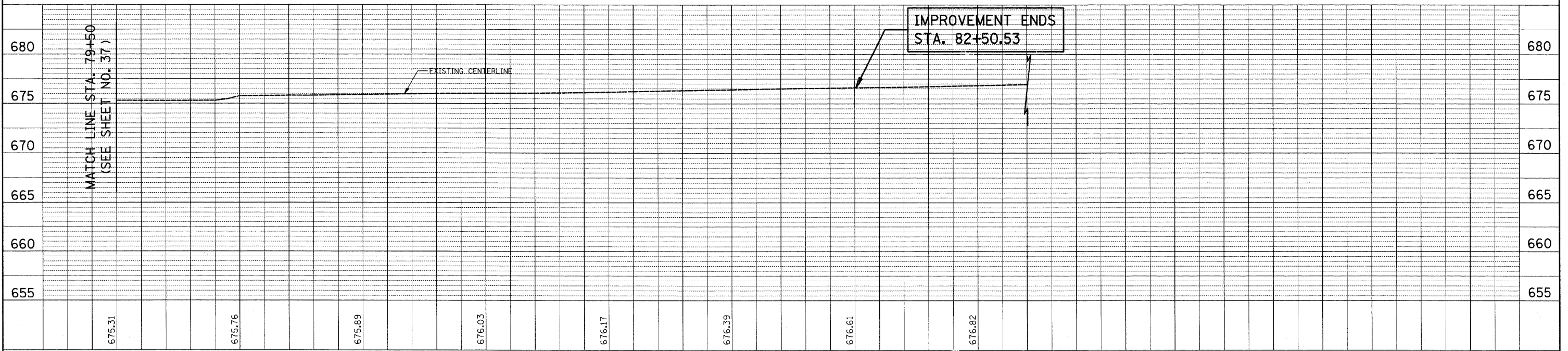
- 3 STA. 81+06, 2.9' RT
INLET TO BE REMOVED
- 4 STA. 81+06, 4.9' RT
INLET T-B
TYPE 1 FRAME, O.L.
RIM = 676.34
INV = 671.37 (EX 10" E)

3 3' - 10" S.S. REMOVAL
T.B.F. = 0.9 CU. YD.



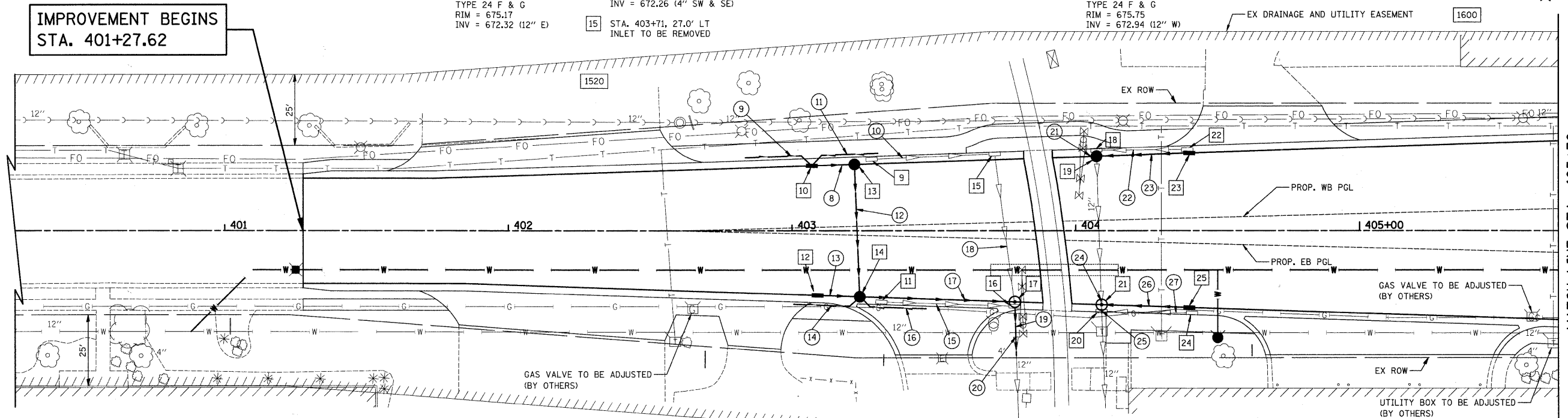
ILLINOIS ROUTE 83

IMPROVEMENT ENDS
STA. 82+50.53

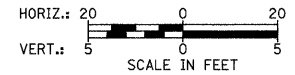


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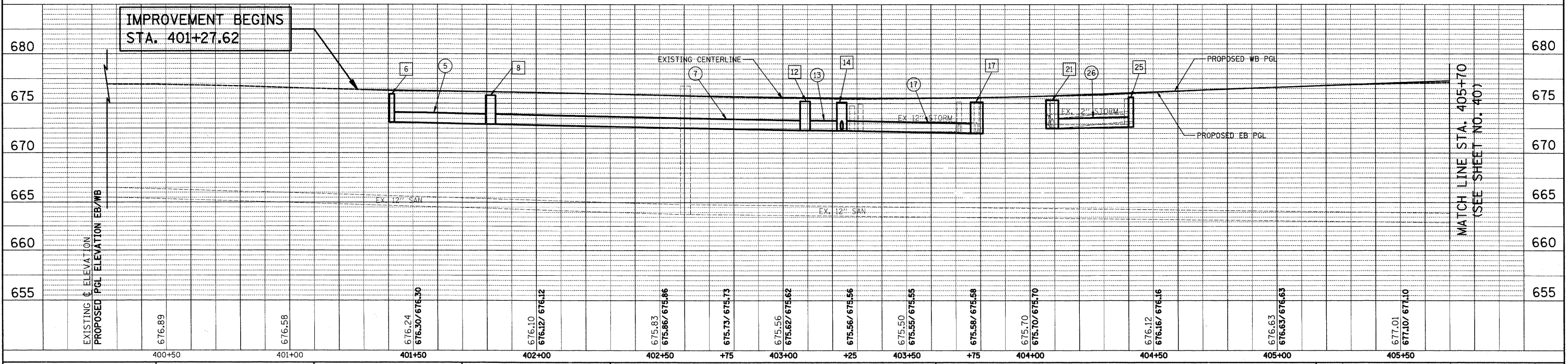
- 5 NOT USED
- 6 NOT USED
- 7 NOT USED
- 8 NOT USED
- 9 STA. 403+28, 25.2' LT
INLET TO BE REMOVED
- 10 STA. 403+07, 22.9' LT
INLET T-A
TYPE 24 F & G
RIM = 675.17
INV = 672.56 (12" E)
INV = 672.56 (4" NW & NE)
- 11 STA. 403+32, 25.3' RT
INLET TO BE REMOVED
- 12 STA. 403+09, 23.0' RT
INLET T-A
TYPE 24 F & G
RIM = 675.17
INV = 672.32 (12" E)
- 13 STA. 403+22.0, 23.4' LT
CB T-A, 4' DIA
TYPE 24 F & G
RIM = 675.22
INV = 672.50 (12" S & W)
- 14 STA. 403+23.83, 23.5' RT
T-A, 4' DIA
TYPE 24 F & G
RIM = 675.12
INV = 672.26 (12" E & W)
INV = 672.26 (12" N)
INV = 672.26 (4" SW & SE)
- 15 STA. 403+71, 27.0' LT
INLET TO BE REMOVED
- 16 STA. 403+79, 26.9' RT
INLET TO BE REMOVED
- 17 STA. 403+78.52, 25.3' RT
MH T-A, 4' DIA
TYPE 24 F & G
RIM = 675.46
INV = 672.00 (12" S)
INV = 672.00 (12" W)
- 18 STA. 404+07, 28.3' LT
INLET TO BE REMOVED
- 19 STA. 404+07.32, 26.2' LT
CB T-A, 4' DIA
TYPE 24 F & G
RIM = 675.64
INV = 672.79 (12" E)
INV = 672.79 (EX 12" S)
- 20 STA. 404+09, 28.6' RT
INLET TO BE REMOVED
- 21 STA. 404+09.09, 26.3' RT
MH T-A, 4' DIA
TYPE 24 F & G
RIM = 675.41
INV = 672.50 (EX 12" N)
INV = 672.50 (EX 12" S)
INV = 672.50 (12" E)
- 22 STA. 404+39, 28.5' LT
INLET TO BE REMOVED
- 23 STA. 404+40, 27.3' LT
INLET T-A
TYPE 24 F & G
RIM = 675.75
INV = 672.94 (12" W)
- 24 STA. 404+41, 29.3' RT
INLET TO BE REMOVED
- 25 STA. 404+40, 27.3' RT
INLET T-A
TYPE 24 F & G
RIM = 675.62
INV = 672.64 (12" W)



- 4 NOT USED
- 5 NOT USED
- 6 NOT USED
- 7 NOT USED
- 8 12' - 12" S.S., CL. A, T-1 @ 0.5%
T.B.F. = 1.6 CU. YD.
- 9 25' - PIPE UNDERDRAIN, FABRIC
LINED TRENCH 4"
- 10 41' - 12" S.S. REMOVAL
T.B.F. = 6.1 CU. YD.
- 11 25' - PIPE UNDERDRAIN, FABRIC
LINED TRENCH 4"
- 12 47' - 12" S.S., W.M.R. @ 0.5%
T.B.F. = 6.3 CU. YD.
- 13 11' - 12" S.S., W.M.R. @ 0.5%
T.B.F. = 1.5 CU. YD.
- 14 25' - PIPE UNDERDRAIN, FABRIC
LINED TRENCH 4"
- 15 45' - 12" S.S. REMOVAL
T.B.F. = 7.1 CU. YD.
- 16 25' - PIPE UNDERDRAIN, FABRIC
LINED TRENCH 4"
- 17 51' - 12" S.S., CL. A, T-1 @ 0.5%
T.B.F. = 6.8 CU. YD.
- 18 54' - 12" S.S. REMOVAL
T.B.F. = 10.0 CU. YD.
- 19 15' - 12" S.S., W.M.R. @ 0.5%
T.B.F. = 0 CU. YD.
- 20 15' - 12" S.S. REMOVAL
T.B.F. = 0 CU. YD.
- 21 2' - 12" S.S. REMOVAL
T.B.F. = 0.2 CU. YD.
- 22 30' - 12" S.S. REMOVAL
T.B.F. = 3.8 CU. YD.
- 23 30' - 12" S.S., CL. A, T-1 @ 0.5%
T.B.F. = 4.0 CU. YD.
- 24 2' - 12" S.S. REMOVAL
T.B.F. = 0.3 CU. YD.
- 25 1' - 12" S.S., CL. A, T-1 @ 1.7%
T.B.F. = 0.2 CU. YD.
- 26 28' - 12" S.S., CL. A, T-1 @ 0.5%
T.B.F. = 3.7 CU. YD.
- 27 30' - 12" S.S. REMOVAL
T.B.F. = 3.5 CU. YD.



IMPROVEMENT BEGINS
STA. 401+27.62



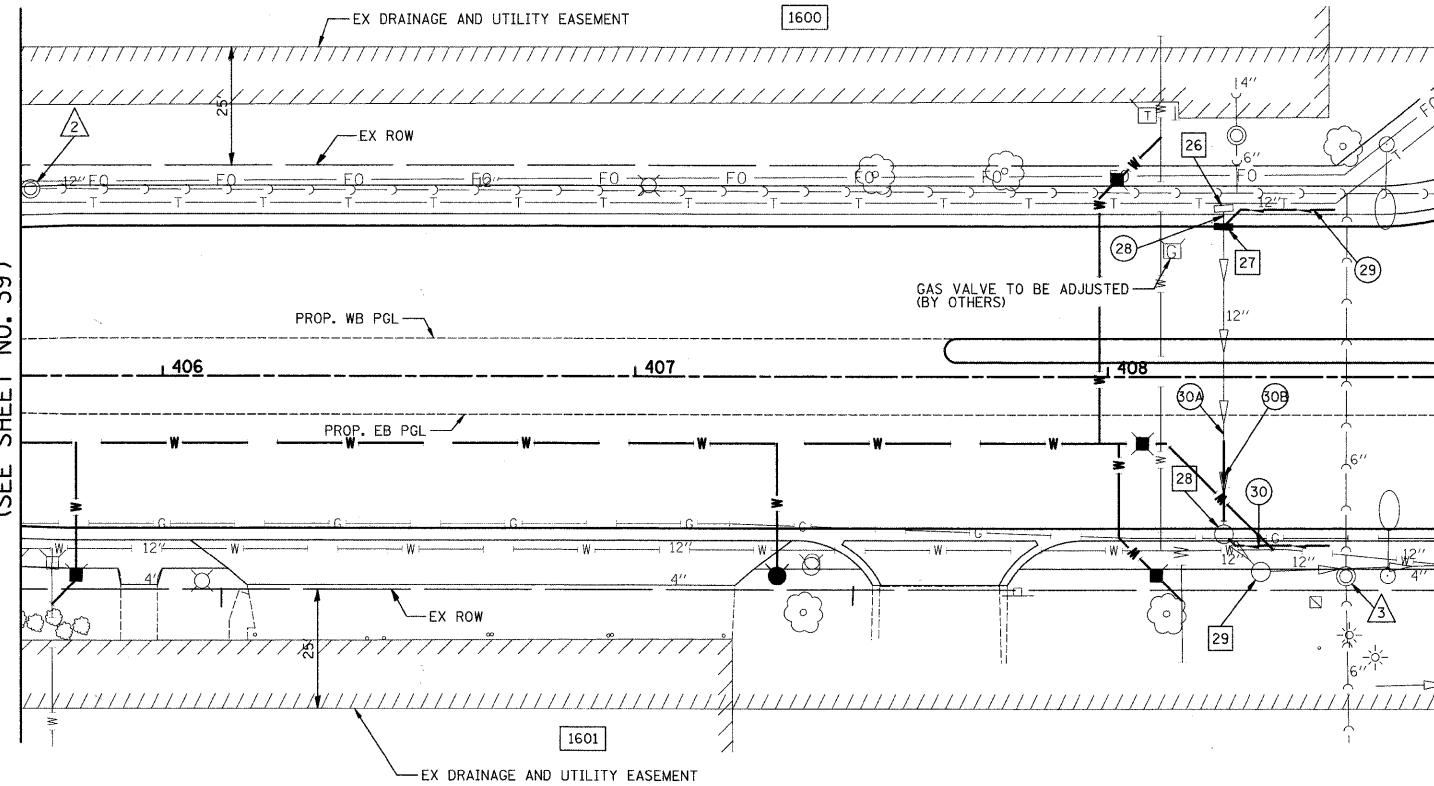
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REVIEWED	PLANNED
ALIGNED	CHECKED
NOTE BOOK NO.	FILE NAME

DATE	BY
REVIEWED	PLANNED
GRADES CHECKED	NOTED
NOTE BOOK NO.	STRUCTURE NOTATIONS CHKD

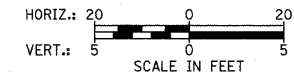
MATCH LINE STA. 405+70
(SEE SHEET NO. 39)



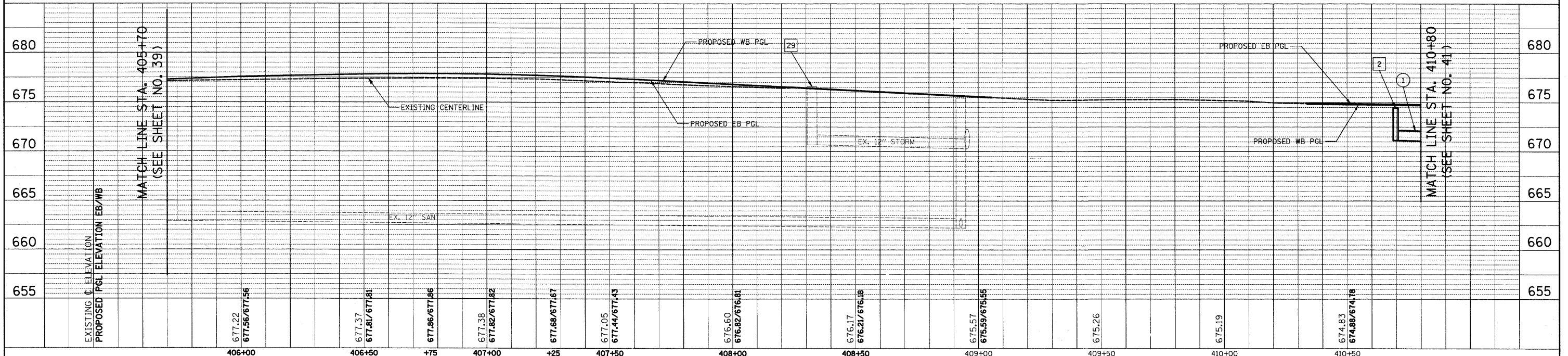
MATCH LINE STA. 408+70
(SEE SHEET NO. 37)

- 26 STA. 408+25, 35.9' LT
INLET TO BE REMOVED
- 27 STA. 408+24.60, 32.0' LT
INLET T-A,
TYPE 24 F & G
RIM = 676.01
INV = 672.71 (EX 12" S)
INV = 672.71 (4" NE)
- 28 STA. 408+25, 33.2' RT
CB TO BE ADJUSTED
W/ NEW TYPE 24 F & G
EX RIM = 676.04
RIM = 676.04
INV = 672.54 (4" E)
- 29 STA. 408+32, 41.1' RT
CB TO BE ADJUSTED
W/ NEW TYPE 1 FR., C.L.
EX RIM = 676.59
RIM = 676.45
- 28 4' - 12" S.S. REMOVAL
T.B.F. = 0.8 CU. YD.
- 29 25' - PIPE UNDERDRAIN, FABRIC
LINED TRENCH 4"
- 30 25' - PIPE UNDERDRAIN, FABRIC
LINED TRENCH 4"
- 50A 17' - 12" S.S. REMOVAL
T.B.F. = 0.0 CU. YD.
- 50B 17' - 12" S.S., W.M.R. @ 2.3%
T.B.F. = 4.8 CU. YD.

- 2 STA. 405+72, 39.9' LT
SAN. MH TO BE ADJ
EX RIM = 677.12
PR RIM = 677.31
- 3 STA. 408+50, 42.0' RT
SAN. MH TO BE ADJ
EX RIM = 675.80
PR RIM = 676.25



PRATT BOULEVARD



MATCH LINE STA. 410+80
(SEE SHEET NO. 41)

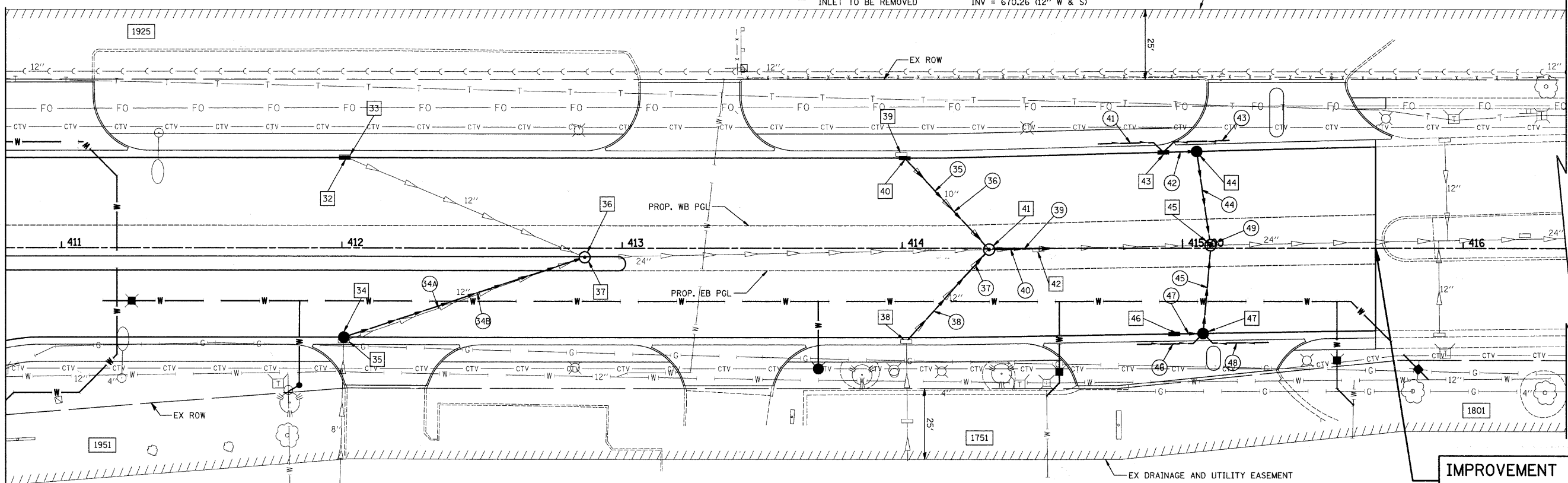
FILE NAME = ...\\2363\cod\sheet\2363.DU_05.dgn	USER NAME = k+k	DESIGNED - BLG	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL ROUTE 83 (BUSSE ROAD) AND PRATT BOULEVARD DRAINAGE AND UTILITIES	F.A.P. RTE. 334	SECTION 09-00057-00-CH	COUNTY COOK	TOTAL SHEETS 103	SHEET NO. 40		
PLOT SCALE = 20.0000' / IN.	CHECKED - DJK	REVISED -	SCALE: 1" = 20'			SHEET NO. 5 OF 6 SHEETS	STA. 405+70.00 TO STA. 408+70.00	CONTRACT NO. 63523				
PLOT DATE = 11/3/2010	DATE - 10-22-10	REVISED -	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(660)									

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FILE NAME	

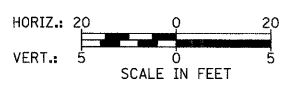
DATE	
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REVISIONS	
PLANNED	
NOTED	
CHECKED	
NO.	
FILE NAME	

- 30 NOT USED
- 31 NOT USED
- 32 STA. 412+01.08, 32.0' LT INLET TYPE A TYPE 24 F & G RIM = 673.84 INV = 670.50 (EX 12" SE)
- 33 STA. 412+01. 32.4' LT INLET TO BE REMOVED
- 34 STA. 412+00.68, 32.0' RT CB T-A, 4' DIA TYPE 24 F & G RIM = 673.90 INV = 670.51 (12" W) INV = 670.14 (12" NE) INV = 670.60 (EX 8" S)
- 35 STA. 412+01. 33.4' RT INLET TO BE REMOVED
- 36 STA. 412+87, 3.2' RT CB TO BE REMOVED
- 37 STA. 412+87, 3.2' RT MH T-A, 5' DIA T-1 FR., C.L. RIM = 674.27 INV = 669.70 (EX 12" NW) INV = 669.70 (12" SW) INV = 669.70 (EX 24" E)
- 38 STA. 414+01, 33.3' RT INLET TO BE ADJUSTED W/ NEW TYPE 24 F & G EX RIM = 672.92 RIM = 673.28 INV = 669.80 (12" NE) INV = 669.90 (EX 6" S)
- 39 STA. 414+00, 33.3' LT INLET TO BE REMOVED
- 40 STA. 414+00.89, 32.0' LT INLET T-A TYPE 24 F & G RIM = 673.21 INV = 670.55 (12" SE)
- 41 STA. 414+31, 0.4' RT MH T-A, 5' DIA T-1 FR., C.L. RIM = 673.77 INV = 669.21 (EX 24" W) INV = 669.21 (24" E) INV = 669.59 (12" SW) INV = 670.34 (12" NW)
- 42 STA. 414+49, 0.6' RT INLET TO BE REMOVED
- 43 STA. 414+93.12, 34.1' LT INLET T-A TYPE 24 F & G RIM = 673.00 INV = 670.30 (12" E, 4" NW & NE)
- 44 STA. 415+05, 34.4' LT CB T-A, 4' DIA TYPE 24 F & G RIM = 673.00 INV = 670.26 (12" W & S)
- 45 STA. 415+10, 1.1' LT MH T-A, 5' DIA T-1 FR., C.L. RIM = 673.67 INV = 670.11 (12" N) INV = 670.16 (12" S) INV = 668.95 (EX 24" E) INV = 668.95 (EX 24" W)
- 46 STA. 414+93, 30.7' RT INLET T-A TYPE 24 F & G RIM = 673.04 INV = 670.34 (12" E)
- 47 STA. 415+07.27, 30.5' RT CB T-A, 4' DIA TYPE 24 F & G RIM = 673.03 INV = 670.31 (12" W) INV = 670.31 (12" N) INV = 670.31 (4" SW & SE)

MATCH LINE STA. 410+80 (SEE SHEET NO. 37)

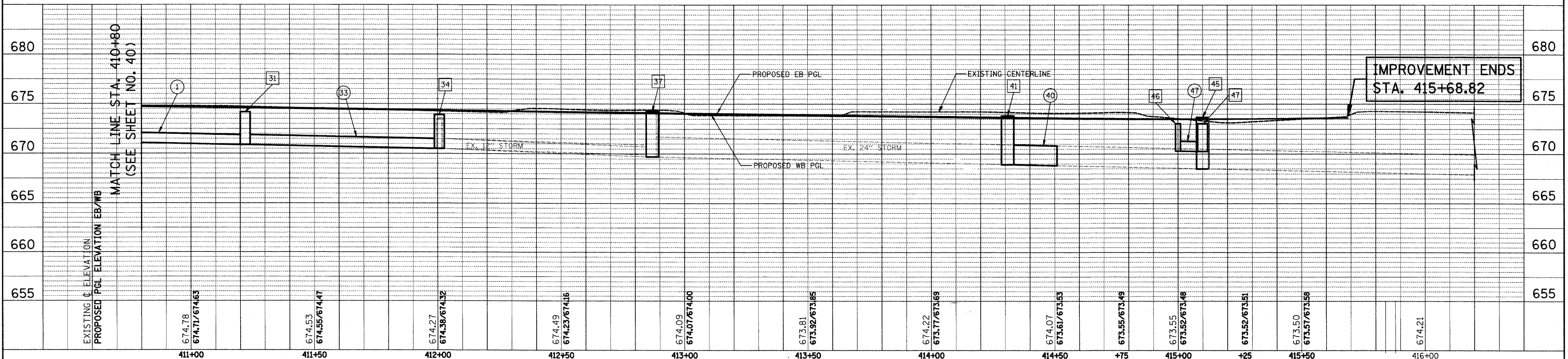


- 31 NOT USED
- 32 NOT USED
- 33 NOT USED
- 34 NOT USED
- 35 45' - 10" S.S. REMOVAL T.B.F. = 8.9 CU. YD.
- 36 42' - 12" S.S., CL. A, T-1 @ 0.5% T.B.F. = 0 CU. YD.
- 37 43' - 12" S.S. REMOVAL T.B.F. = 10.7 CU. YD.
- 38 42' - 12" S.S., W.M.R. @ 0.5% T.B.F. = 0 CU. YD.
- 39 25' - 24" S.S. REMOVAL T.B.F. = 11.0 CU. YD.
- 40 20' - 24" S.S., CL. A, T-1 @ 0.35% T.B.F. = 0 CU. YD.
- 41 25' - PIPE UNDERDRAIN, FABRIC LINED TRENCH 4"
- 42 9' - 12" S.S., CL. A, T-1 @ 0.5% T.B.F. = 1.2 CU. YD.
- 43 25' - PIPE UNDERDRAIN, FABRIC LINED TRENCH 4"
- 44 31' - 12" S.S., CL. A, T-1 @ 0.5% T.B.F. = 4.1 CU. YD.
- 45 30' - 12" S.S., W.M.R. @ 0.5% T.B.F. = 3.9 CU. YD.
- 46 25' - PIPE UNDERDRAIN, FABRIC LINED TRENCH 4"
- 47 7' - 12" S.S., CL. A, T-1 @ 0.5% T.B.F. = 1.0 CU. YD.
- 48 25' - PIPE UNDERDRAIN, FABRIC LINED TRENCH 4"
- 49 5' - 24" S.S. REMOVAL T.B.F. = 2.3 CU. YD.



SCALE IN FEET

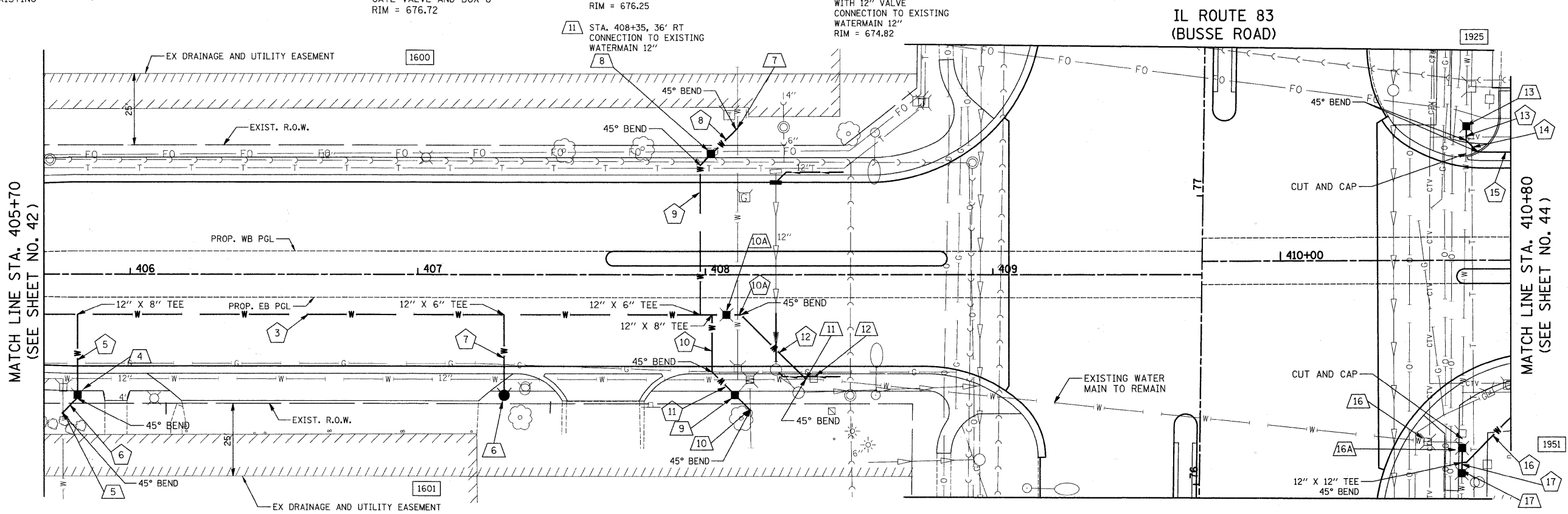
PRATT BOULEVARD



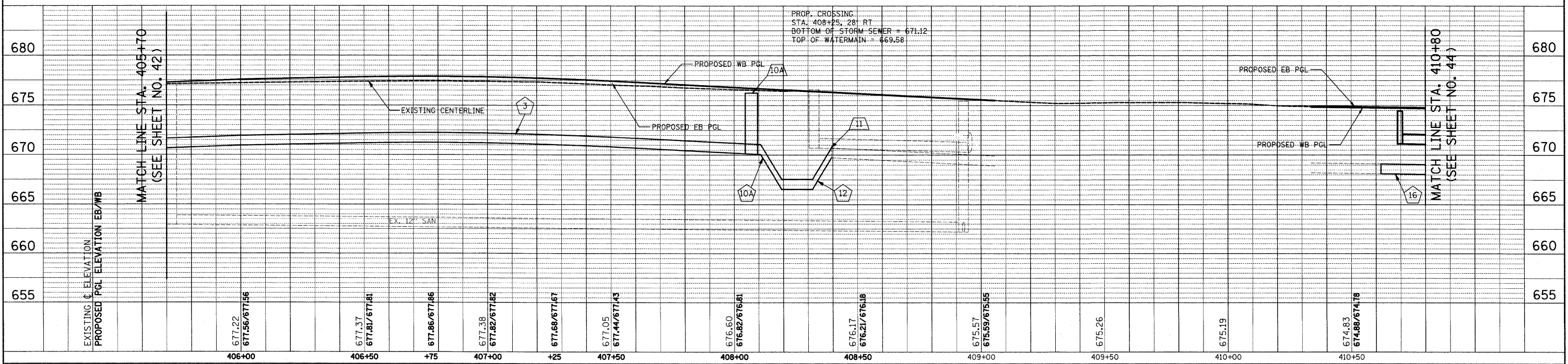
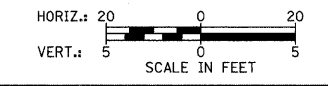
IMPROVEMENT ENDS STA. 415+68.82

FILE NAME =	USER NAME = krk	DESIGNED - BLG	REVISED -	STATE OF ILLINOIS	IL ROUTE 83 (BUSSE ROAD) AND PRATT BOULEVARD	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
...\\2363\oad\sheet\2363.DU.06.dgn		DRAWN - BLG	REVISED -	DEPARTMENT OF TRANSPORTATION	DRAINAGE AND UTILITIES	334	09-00057-00-CH	COOK	103	41
PLOT SCALE = 20.0000' / IN.		CHECKED - DJK	REVISED -			CONTRACT NO. 63523				
PLOT DATE = 11/3/2010		DATE = 10-22-10	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-90036601				

- 4 STA. 405+82, 42.0' RT GATE VALVE AND BOX 8" RIM = 677.41
- 5 STA. 405+77, 48' RT CONNECTION TO EXISTING WATERMAIN 8"
- 6 STA. 407+30, 42.0' RT FH W/ AUX VALVE & VALVE BOX
- 7 STA. 408+11, 51' LT CONNECTION TO EXISTING WATERMAIN 6"
- 8 STA. 408+02, 44.5' LT GATE VALVE AND BOX 6" RIM = 676.80
- 9 STA. 408+10, 39.5' RT GATE VALVE AND BOX 8" RIM = 676.72
- 10 STA. 408+16, 47' RT CONNECTION TO EXISTING WATERMAIN 8"
- 10A STA. 408+07, 14.0' RT GATE VALVE 12" WITH VAULT, 5'-DIAMETER RIM = 676.25
- 11 STA. 408+35, 36' RT CONNECTION TO EXISTING WATERMAIN 12"
- 12 STA. 408+38, 36' RT VALVE BOX TO BE ADJUSTED EX RIM = 676.22 PR RIM = 676.30
- 13 STA. 410+65, 47' LT VALVE VAULT, 5'-DIA., WITH 12" VALVE CONNECTION TO EXISTING WATERMAIN 12" RIM = 674.82
- 14 NOT USED
- 15 NOT USED
- 16 STA. 410+51, 63' RT VALVE VAULT TO BE REMOVED (VALVE TO REMAIN IN OPEN POSITION)
- 16A STA. 410+62, 59' RT VALVE VAULT, 5'-DIA., WITH 12" VALVE CONNECTION TO EXISTING WATERMAIN 12" RIM = 674.90
- 17 STA. 410+63, 74' RT VALVE VAULT, 5'-DIA., WITH 12" VALVE CONNECTION TO EXISTING WATERMAIN 12" RIM = 675.00



- 3 SEE SHEET NO. 42
- 5 29' - WATER MAIN 8" T.B.F. = 17.9 CU. YD.
- 6 7' - WATER MAIN 8" T.B.F. = 0.0 CU. YD.
- 7 28' - WATER MAIN 6" T.B.F. = 16.2 CU. YD.
- 8 18' - WATER MAIN 6" T.B.F. = 0.0 CU. YD.
- 9 52' - WATER MAIN 6" T.B.F. = 35.6 CU. YD.
- 10 20' - WATER MAIN 8" T.B.F. = 15.7 CU. YD.
- 10A 5' - WATER MAIN 12" T.B.F. = 3.5 CU. YD.
- 11 19' - WATER MAIN 8" T.B.F. = CU. YD.
- 12 32' - WATER MAIN 12" T.B.F. = 28.2 CU. YD.
- 13 5' - WATER MAIN 12" T.B.F. = 1.4 CU. YD.
- 14 6' - WATER MAIN 12" T.B.F. = 4.1 CU. YD.
- 15 39' - WATER MAIN 12" T.B.F. = 26.8 CU. YD.
- 16 26' - WATER MAIN 12" T.B.F. = 0.0 CU. YD.
- 17 8' - WATER MAIN 12" T.B.F. = 5.5 CU. YD.

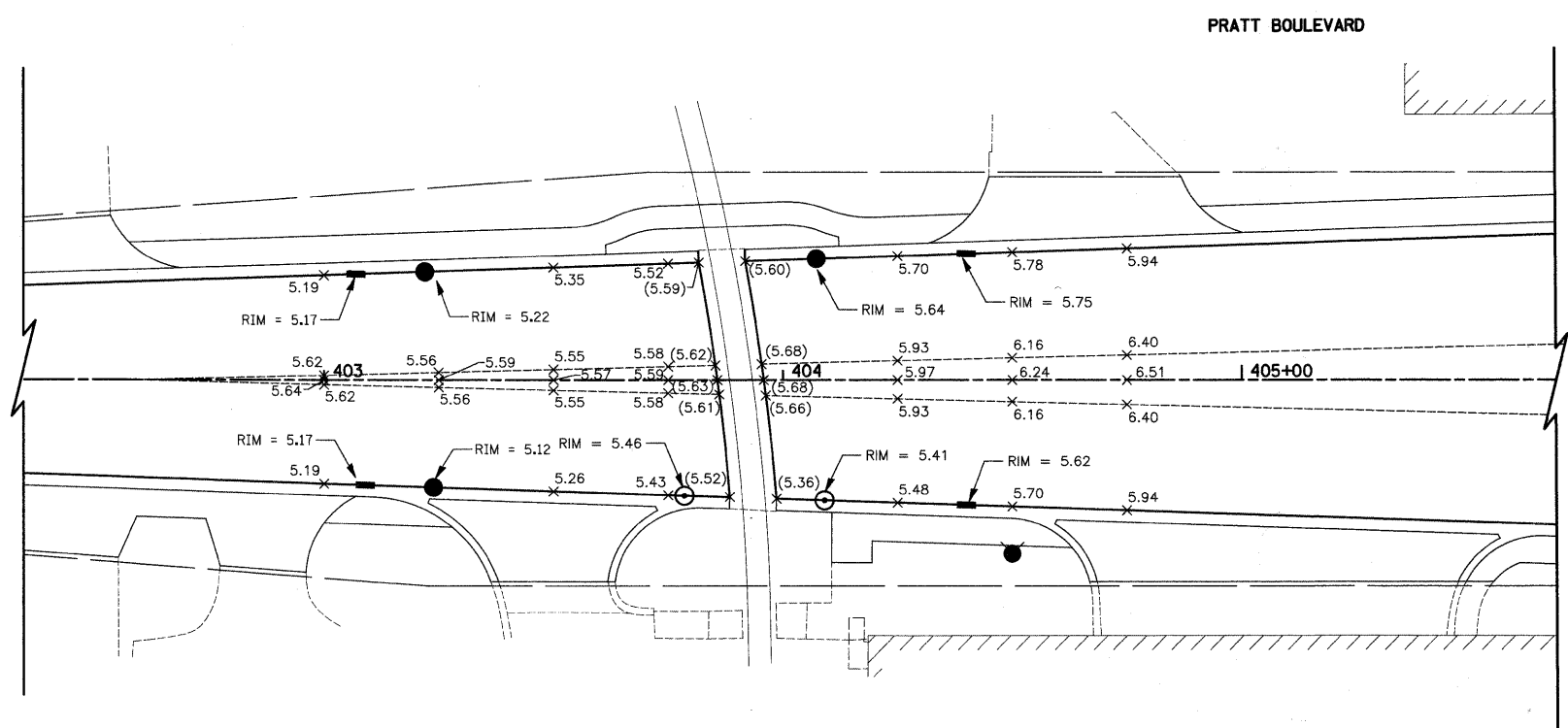


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PLAN	DESIGNED	DATE
NOTE BOOK	PLOTTED	BY
NO.	ALIGNMENT CHECKED	
	FILE NAME	
	ADD. FILE NAME	

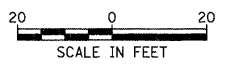
PROFILE	DESIGNED	DATE
NOTE BOOK	GRADES CHECKED	BY
NO.	BM. NOTED	
	STRUCTURE NOTATIONS CHKD	



LEGEND

- (X.XX) EXISTING ELEVATION
- X.XX PROPOSED ELEVATION

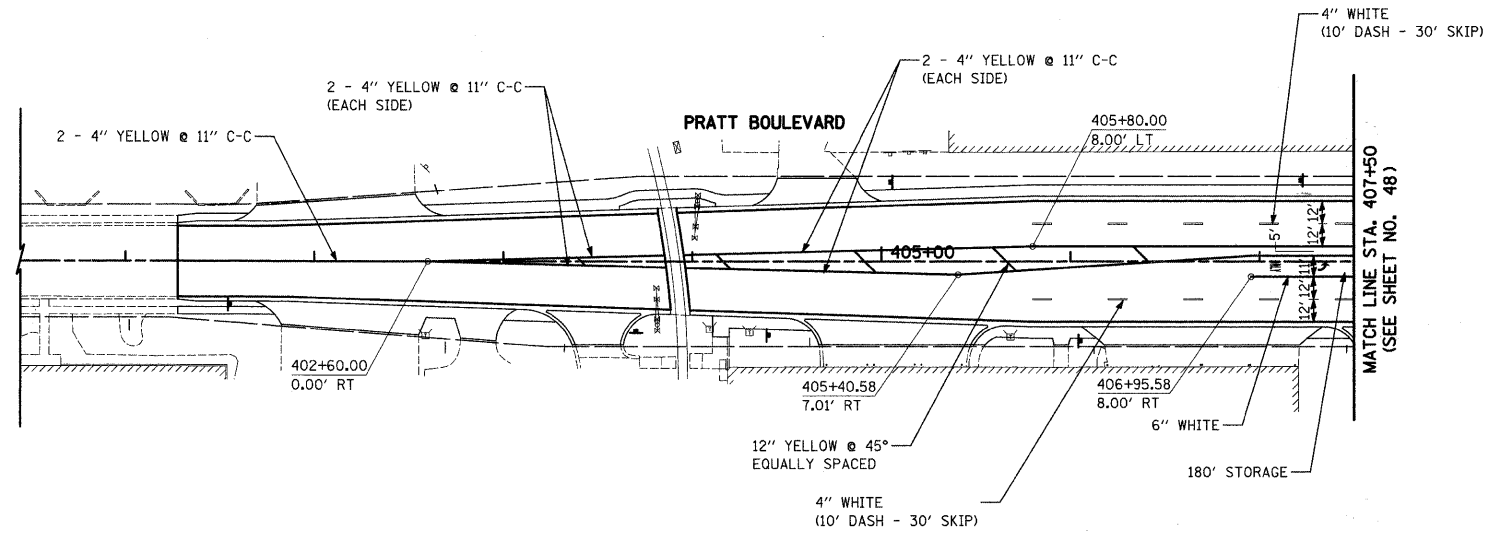
*ADD 670' TO ALL ELEVATIONS



FILE NAME = ...2363\oad\sheet\2363_IOP_02.dgn	USER NAME = jat	DESIGNED - BLG	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL ROUTE 83 (BUSSE ROAD) AND PRATT BOULEVARD RAILROAD CROSSING GRADING PLAN	F.A.P. RTE. 334	SECTION 09-00057-00-CH	COUNTY COOK	TOTAL SHEETS 103	SHEET NO. 47
PLOT SCALE = 20.0000' / IN.	CHECKED - DJK	DRAWN - BLG	REVISED -			CONTRACT NO. 63523				
PLOT DATE = 12/21/2010	DATE - 10-22-10	REVISOR -	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003660				

PLAN	DESIGNED	BY	DATE
NOTE BOOK NO.	ALIGNED		
	CHECKED		
	CADD FILE NAME		

PROFILE	DESIGNED	BY	DATE
NOTE BOOK NO.	GRADES CHECKED		
	STATIONED		
	NOTATIONS CHK'D		

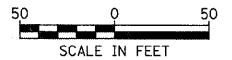


GENERAL NOTES

1. DIMENSIONS TO PAVEMENT MARKINGS ARE TO THE CENTER OF A SINGLE LINE OR THE CENTER OF GAP FOR A DOUBLE LINE.
2. SEE I.D.O.T. STANDARD TC-13 FOR TURN LANE MARKING DETAILS.
3. ALL PERMANENT PAVEMENT MARKINGS ON CONCRETE SURFACES SHALL BE POLYUREA. ALL PERMANENT PAVEMENT MARKINGS ON ASPHALT SURFACES SHALL BE THERMOPLASTIC.

LEGEND

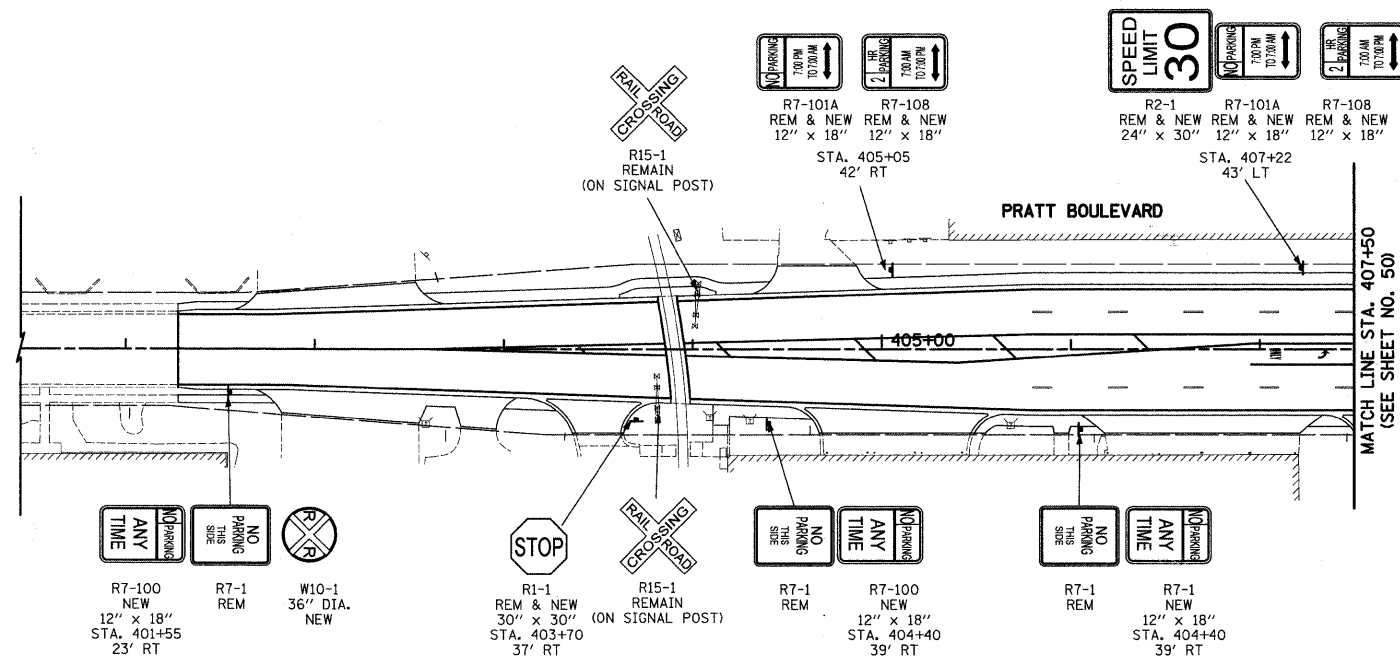
- ▬ PROPOSED WHITE LETTERS AND SYMBOLS
- ◁ RAISED REFLECTIVE PAVEMENT MARKERS



FILE NAME = ...ced\sheet\2363_Striping_02.dgn	USER NAME = kkk	DESIGNED - BLG	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL ROUTE 83 (BUSSE ROAD) AND PRATT BOULEVARD PAVEMENT MARKING PLAN	F.A.P. RTE. 334	SECTION 09-00057-00-CH	COUNTY COOK	TOTAL SHEETS 103	SHEET NO. 49		
PLOT SCALE = 50.0000 ' / IN.	CHECKED - DJK	REVISED -	SCALE: 1" = 50'			SHEET NO. 2 OF 2 SHEETS	CONTRACT NO. 63523					
PLOT DATE = 10/22/2010	DATE - 10-22-10	REVISED -										
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-90036600												

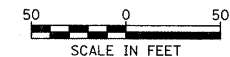
PLAN	DESIGNED	DATE
NOTE BOOK NO.	ALIGNED CHECKED	
	PLOTTED	
	FILE NAME	

PROFILE	DESIGNED	DATE
NOTE BOOK NO.	GRADES CHECKED	
	PLOTTED	
	FILE NAME	



LEGEND

- ⊕ EXISTING SIGN
- ⊕ PROPOSED SIGN
- NEW SIGN PANEL OF THE TYPE REQUIRED
- REMAIN SIGN PANEL TO REMAIN IN SAME LOCATION
- REM REMOVE SIGN PANEL ASSEMBLY, TYPE A OR B



FILE NAME = ...\\oad\sheet\2363_Signing_02.dgn

USER NAME = krk
 PLOT SCALE = 50.0000' / IN.
 PLOT DATE = 10/22/2010

DESIGNED - BLG
 DRAWN - BLG
 CHECKED - DJK
 DATE - 10-22-10

REVISED -
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**IL ROUTE 83 (BUSSE ROAD) AND PRATT BOULEVARD
 SIGNING PLAN**

SCALE: 1" = 50' SHEET NO. 2 OF 2 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	09-00057-00-CH	COOK	103	51
CONTRACT NO. 63523				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-90036601				

NOTES FOR TEMPORARY TRAFFIC SIGNALS

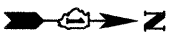
1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE L.E.D. AND 12" (300 mm) DIAMETER. HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. PEDESTRIAN SIGNALS SHALL INCLUDE SOLID INTERNATIONAL SYMBOLS. PEDESTRIAN SIGNALS WITH COUNTDOWN TIMERS SHALL BE USED WHEN THE EXISTING INSTALLATION UTILIZES COUNTDOWN TYPE OR AS DIRECTED BY THE ENGINEER. COUNTDOWN TYPE PEDESTRIAN SIGNALS ARE NOT TO BE INSTALLED AT A RAILROAD INTERSECTION. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
4. ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE VILLAGE OF ELK GROVE VILLAGE. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR PICK-UP OF ALL EQUIPMENT TO BE RETURNED TO THE VILLAGE AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

- 3 EACH LIGHT DETECTOR
- 1 EACH LIGHT DETECTOR AMPLIFIER

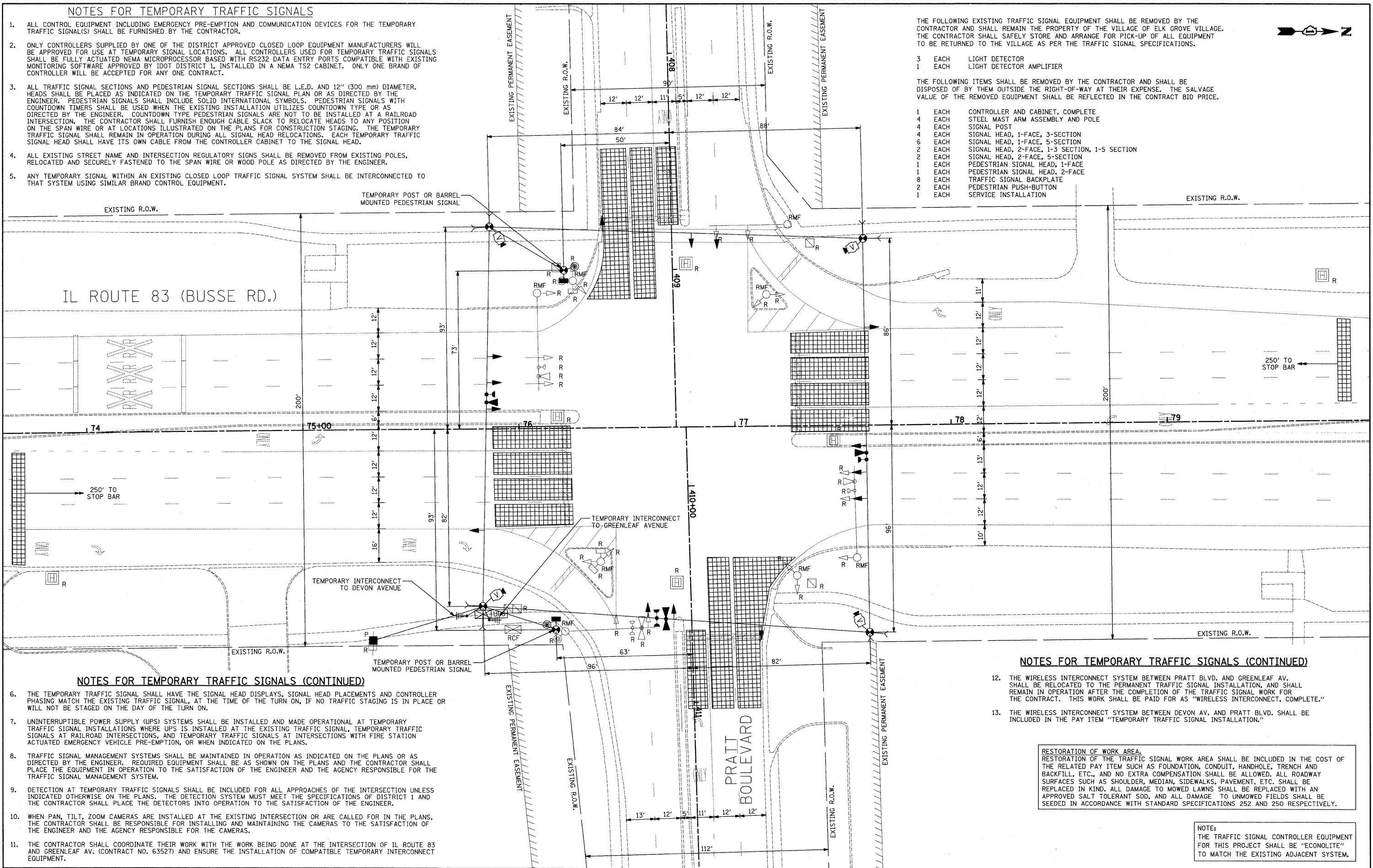
THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 1 EACH CONTROLLER AND CABINET, COMPLETE
- 4 EACH STEEL MAST ARM ASSEMBLY AND POLE
- 4 EACH SIGNAL POST
- 4 EACH SIGNAL HEAD, 1-FACE, 3-SECTION
- 6 EACH SIGNAL HEAD, 1-FACE, 5-SECTION
- 2 EACH SIGNAL HEAD, 2-FACE, 1-3 SECTION, 1-5 SECTION
- 2 EACH SIGNAL HEAD, 2-FACE, 5-SECTION
- 1 EACH PEDESTRIAN SIGNAL HEAD, 1-FACE
- 1 EACH PEDESTRIAN SIGNAL HEAD, 2-FACE
- 8 EACH TRAFFIC SIGNAL BACKPLATE
- 2 EACH PEDESTRIAN PUSH-BUTTON
- 1 EACH SERVICE INSTALLATION



PLAN	BY	DATE
REVISION		
NOTES		
NO.		

PROFILE	BY	DATE
REVISION		
NOTES		
NO.		



NOTES FOR TEMPORARY TRAFFIC SIGNALS (CONTINUED)

6. THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL, AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
7. UNINTERRUPTIBLE POWER SUPPLY (UPS) SYSTEMS SHALL BE INSTALLED AND MADE OPERATIONAL AT TEMPORARY TRAFFIC SIGNAL INSTALLATIONS WHERE UPS IS INSTALLED AT THE EXISTING TRAFFIC SIGNAL, TEMPORARY TRAFFIC SIGNALS AT RAILROAD INTERSECTIONS, AND TEMPORARY TRAFFIC SIGNALS AT INTERSECTIONS WITH FIRE STATION ACTUATED EMERGENCY VEHICLE PRE-EMPTION, OR WHEN INDICATED ON THE PLANS.
8. TRAFFIC SIGNAL MANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
9. DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER.
10. WHEN PAN, TILT, ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.
11. THE CONTRACTOR SHALL COORDINATE THEIR WORK WITH THE WORK BEING DONE AT THE INTERSECTION OF IL ROUTE 83 AND GREENLEAF AV. (CONTRACT NO. 63527) AND ENSURE THE INSTALLATION OF COMPATIBLE TEMPORARY INTERCONNECT EQUIPMENT.

NOTES FOR TEMPORARY TRAFFIC SIGNALS (CONTINUED)

12. THE WIRELESS INTERCONNECT SYSTEM BETWEEN PRATT BLVD. AND GREENLEAF AV. SHALL BE RELOCATED TO THE PERMANENT TRAFFIC SIGNAL INSTALLATION, AND SHALL REMAIN IN OPERATION AFTER THE COMPLETION OF THE TRAFFIC SIGNAL WORK FOR THE CONTRACT. THIS WORK SHALL BE PAID FOR AS "WIRELESS INTERCONNECT, COMPLETE."
13. THE WIRELESS INTERCONNECT SYSTEM BETWEEN DEVON AV. AND PRATT BLVD. SHALL BE INCLUDED IN THE PAY ITEM "TEMPORARY TRAFFIC SIGNAL INSTALLATION."

RESTORATION OF WORK AREA.
 RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE COST OF THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDER, MEDIAN, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SALT TOLERANT SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

NOTE:
 THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

FILE NAME =	USER NAME = jst
...\\01.Temp Signal Plan.dgn	

DESIGNED - BRD	REVISIONS -
DRAWN - JRT	REVISIONS -
CHECKED - JJE	REVISIONS -
DATE - 10-22-10	REVISIONS -

DESIGNED - BRD	REVISIONS -
DRAWN - JRT	REVISIONS -
CHECKED - JJE	REVISIONS -
DATE - 10-22-10	REVISIONS -

DESIGNED - BRD	REVISIONS -
DRAWN - JRT	REVISIONS -
CHECKED - JJE	REVISIONS -
DATE - 10-22-10	REVISIONS -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVAL PLAN
 IL ROUTE 83 (BUSSE RD.) AND PRATT BOULEVARD**

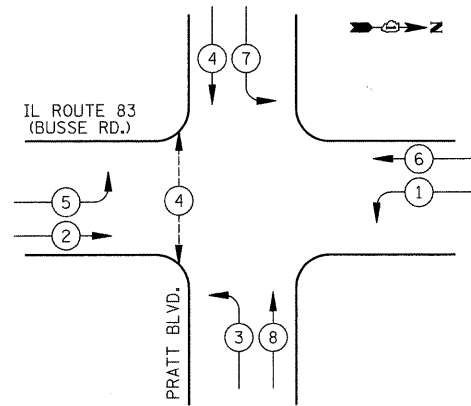
SCALE: 1"=20' SHEET NO. 1 OF 1 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	09-00057-00-CH	COOK	103	52
				CONTRACT NO. 63523
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT M-90036601				

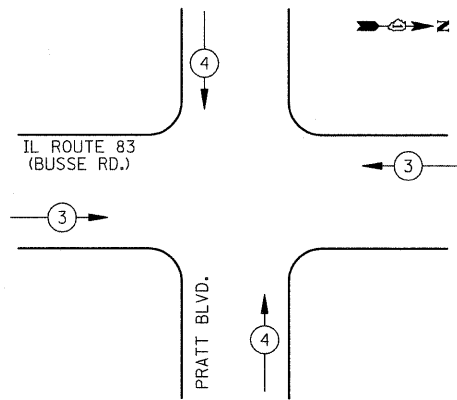
PLAN	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHKD	
	NOTE BOOK NO.	
	BY	
	DATE	

PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHKD	
	NOTE BOOK NO.	
	BY	
	DATE	

TEMPORARY CONTROLLER SEQUENCE



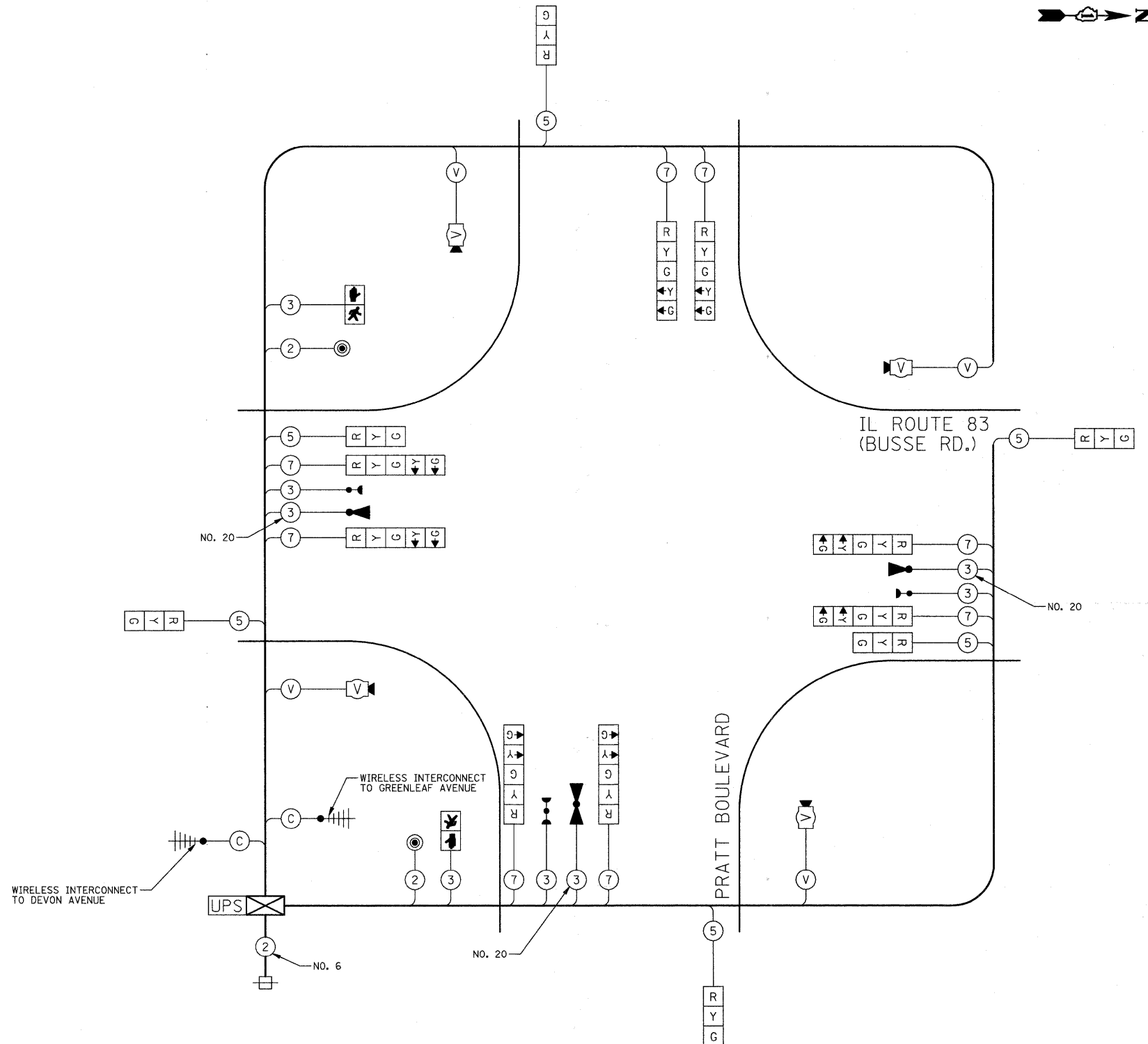
TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE



TEMPORARY EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	← →	↑ ↓

- LEGEND
- ◉ DUAL ENTRY PHASE
 - ◻ SINGLE ENTRY PHASE
 - ◊ OL OVERLAP
 - ◉ PEDESTRIAN PHASE
 - NUMBER REFERS TO ASSOCIATED PHASE

TEMPORARY PHASE DESIGNATION DIAGRAM



TEMPORARY CABLE PLAN
NOT TO SCALE

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE INCAND.	LED	% OPERATION	
SIGNAL (RED)	14	17		0.50	119
(YELLOW)	14		25	0.25	88
(GREEN)	14		15	0.25	53
ARROW	16		12	0.10	19
PED. SIGNAL	2		25	1.00	50
CONTROLLER	1		100	1.00	100
VIDEO SYSTEM	1		150	1.00	150
FLASHER				0.50	
TOTAL =					579

ENERGY COSTS TO: ILLINOIS DEPARTMENT OF TRANSPORTATION
201 WEST CENTER COURT
SCHAUMBURG, IL 60196-1096
CONTACT: ELEANOR SARALLO
PHONE: (630) 424-5124
COMPANY: COM ED

NOTE:
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT
FOR THIS PROJECT SHALL BE "ECONOLITE"
TO MATCH THE EXISTING ADJACENT SYSTEM.

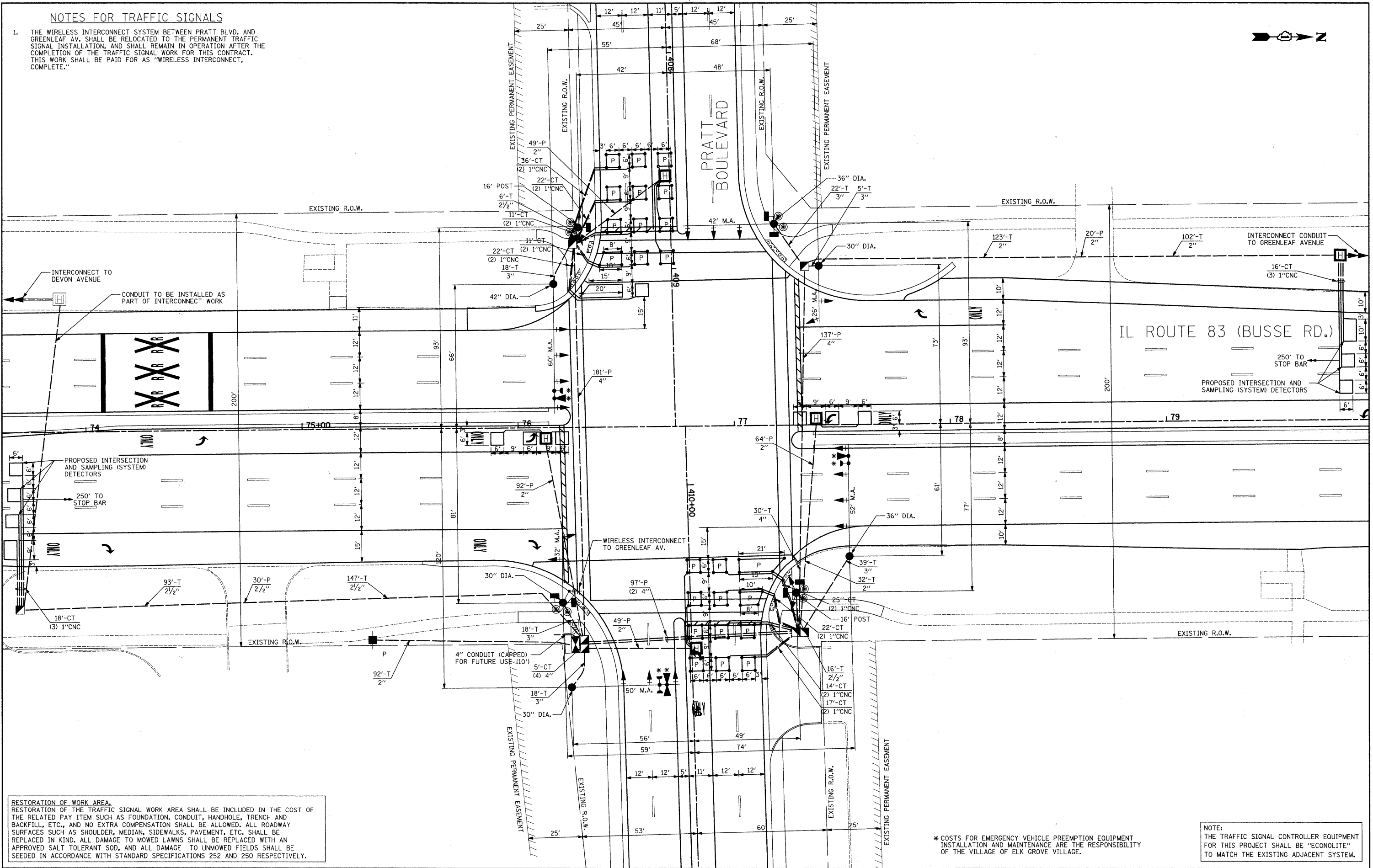
NOTES FOR TRAFFIC SIGNALS

1. THE WIRELESS INTERCONNECT SYSTEM BETWEEN PRATT BLVD. AND GREENLEAF AV. SHALL BE RELOCATED TO THE PERMANENT TRAFFIC SIGNAL INSTALLATION, AND SHALL REMAIN IN OPERATION AFTER THE COMPLETION OF THE TRAFFIC SIGNAL WORK FOR THIS CONTRACT. THIS WORK SHALL BE PAID FOR AS "WIRELESS INTERCONNECT, COMPLETE."



DATE	BY
DATE	BY
DATE	BY
DATE	BY

DATE	BY
DATE	BY
DATE	BY
DATE	BY



RESTORATION OF WORK AREA.
 RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE COST OF THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDER, MEDIAN, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SALT TOLERANT SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

* COSTS FOR EMERGENCY VEHICLE PREEMPTION EQUIPMENT INSTALLATION AND MAINTENANCE ARE THE RESPONSIBILITY OF THE VILLAGE OF ELK GROVE VILLAGE.

NOTE:
 THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

FILE NAME = ...\\03.Signal Plan.dgn

USER NAME = jat
 DESIGNED - BRD
 DRAWN - JRT
 CHECKED - JJE
 DATE - 10-22-10

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

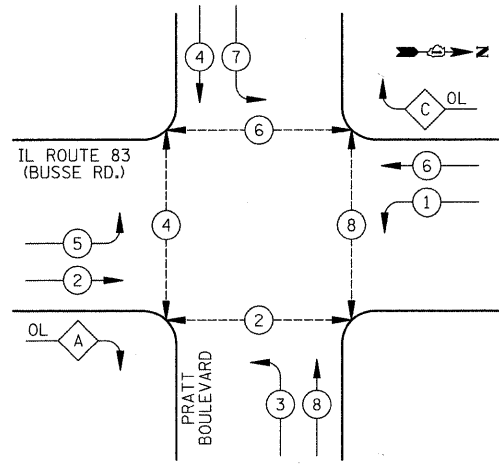
**TRAFFIC SIGNAL MODERNIZATION PLAN
 IL ROUTE 83 (BUSSE RD.) AND PRATT BOULEVARD**
 SCALE: 1"=20' SHEET NO. 1 OF 1 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	09-00057-00-CH	COOK	103	54
CONTRACT NO. 63523				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003/6601				

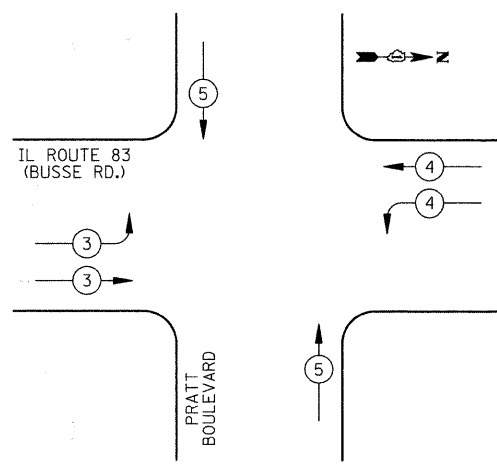
DATE: _____
 BY: _____
 PLAN NO.: _____
 REVISIONS: _____

DATE: _____
 BY: _____
 PROFILE NO.: _____
 REVISIONS: _____

PROPOSED CONTROLLER SEQUENCE



PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE



LEGEND

- DUAL ENTRY PHASE
- SINGLE ENTRY PHASE
- OVERLAP
- PEDESTRIAN PHASE
- NUMBER REFERS TO ASSOCIATED PHASE

PHASE DESIGNATION DIAGRAM

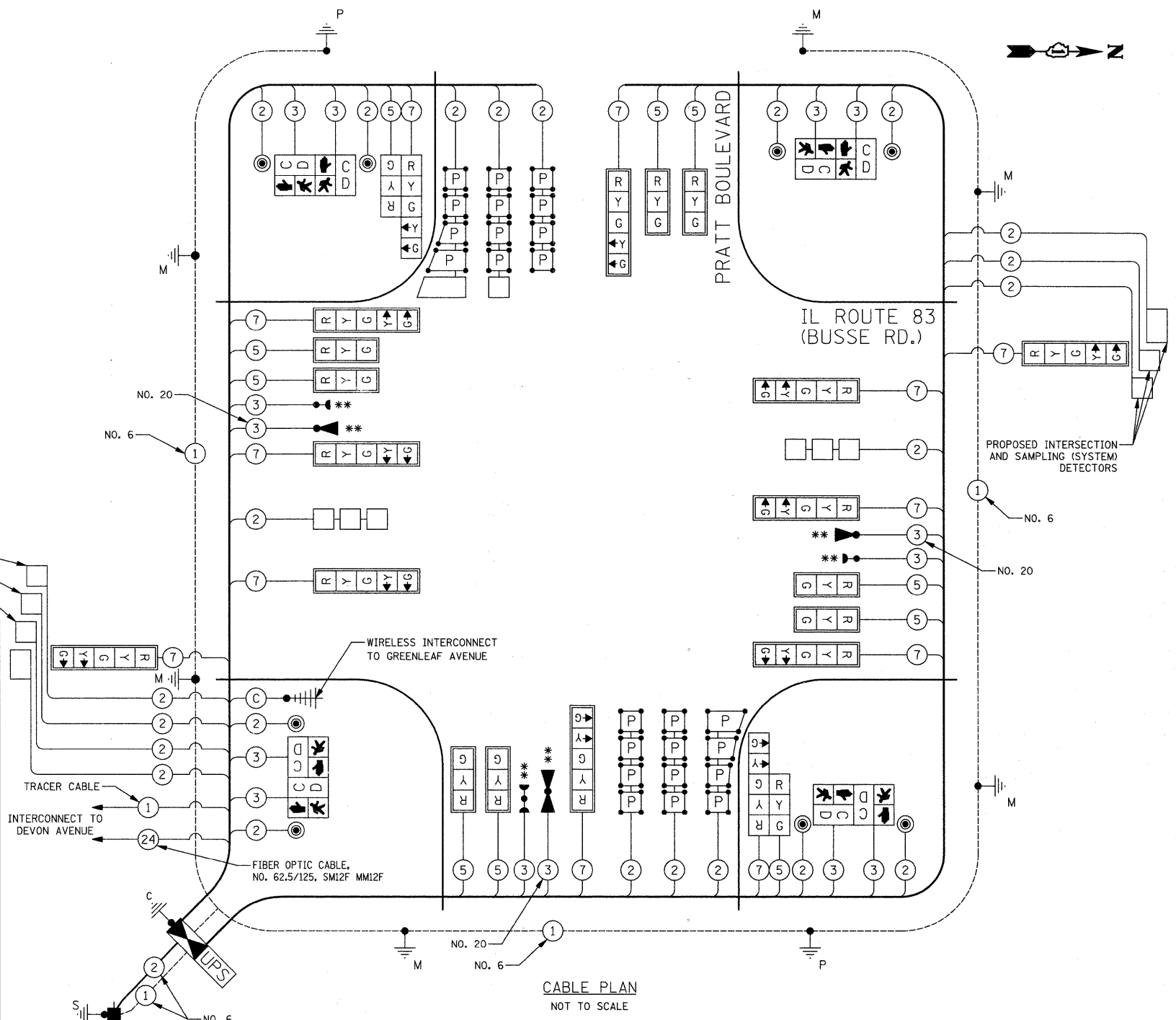
RIGHT TURN OVERLAP PHASE DESIGNATION

OVERLAP LETTER	PERMISSIVE PHASE	PROTECTED PHASE
A	= 2	+ 3
C	= 6	+ 7

PROPOSED EMERGENCY VEHICLE PREEMPTORS			
EMERGENCY VEHICLE PREEMPTOR	3	4	5
MOVEMENT			

SCHEDULE OF QUANTITIES

PAY ITEM	UNIT	QNTY.
SIGN PANEL - TYPE 1	SQ. FT.	29
SIGN PANEL - TYPE 2	SQ. FT.	25
CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL	FOOT	349
CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL	FOOT	262
CONDUIT IN TRENCH, 3" DIA., GALVANIZED STEEL	FOOT	120
CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL	FOOT	60
CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL	FOOT	274
CONDUIT PUSHED, 2 1/2" DIA., GALVANIZED STEEL	FOOT	30
CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL	FOOT	512
HANDHOLE	EACH	3
HEAVY-DUTY HANDHOLE	EACH	5
DOUBLE HANDHOLE	EACH	2
TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	776
FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL	EACH	1
TRANSCIVER - FIBER OPTIC	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	1554
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	2275
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	2385
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	2842
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	4253
ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C	FOOT	112
TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	2
STEEL MAST ARM ASSEMBLY AND POLE, 26 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 32 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 42 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 50 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 52 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 60 FT.	EACH	1
CONCRETE FOUNDATION, TYPE A	FOOT	8
CONCRETE FOUNDATION, TYPE C	FOOT	4
CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	30
CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	45
CONCRETE FOUNDATION, TYPE E 42-INCH DIAMETER	FOOT	21
SIGNAL HEAD, L.E.D., 1-FACE, 3-SECTION, MAST ARM MOUNTED	EACH	8
SIGNAL HEAD, L.E.D., 1-FACE, 5-SECTION, MAST ARM MOUNTED	EACH	10
SIGNAL HEAD, L.E.D., 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED	EACH	2
PEDESTRIAN SIGNAL HEAD, L.E.D., 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	4
TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	18
INDUCTIVE LOOP DETECTOR	EACH	15
DETECTOR LOOP, TYPE I	FOOT	652
* LIGHT DETECTOR	EACH	3
* LIGHT DETECTOR AMPLIFIER	EACH	1
PEDESTRIAN PUSH-BUTTON	EACH	8
TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REMOVE EXISTING HANDHOLE	EACH	10
REMOVE EXISTING CONCRETE FOUNDATION	EACH	9
PREFORMED DETECTOR LOOP	FOOT	891
TEMPORARY TRAFFIC SIGNAL TIMINGS	EACH	1
SERVICE INSTALLATION, POLE MOUNTED	EACH	1
UNINTERRUPTIBLE POWER SUPPLY	EACH	1
ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C	FOOT	983
* ELECTRIC CABLE IN CONDUIT, NO. 20 3C, TWISTED, SHIELDED	FOOT	665
* 100% COST TO VILLAGE OF ELK GROVE VILLAGE		



I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE INCAND.	LED	% OPERATION	
SIGNAL (RED)	22		17	0.50	187
(YELLOW)	22		25	0.25	138
(GREEN)	22		15	0.25	83
ARROW	24		12	0.10	29
PED. SIGNAL	8		25	1.00	200
CONTROLLER	1		100	1.00	100
VIDEO SYSTEM	1		150	1.00	150
FLASHER				0.50	
TOTAL =					742

ENERGY COSTS TO: ILLINOIS DEPARTMENT OF TRANSPORTATION
 201 WEST CENTER COURT
 SCHAUMBURG, IL 60196-1096
 CONTACT: ELEANOR SARALLO
 PHONE: (630) 424-5124
 COMPANY: COM ED

FILE NAME =	USER NAME = jst	DESIGNED - BRD	REVISED -
...\\04.Cable Plan.dgn		DRAWN - JRT	REVISED -
		CHECKED - JJE	REVISED -
		DATE - 10-22-10	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CABLE PLAN, SEQUENCE OF OPERATION, EMERGENCY VEHICLE PREEMPTION SEQUENCE & SCHEDULE OF QUANTITIES
 IL ROUTE 83 (BUSSE RD.) AND PRATT BOULEVARD**

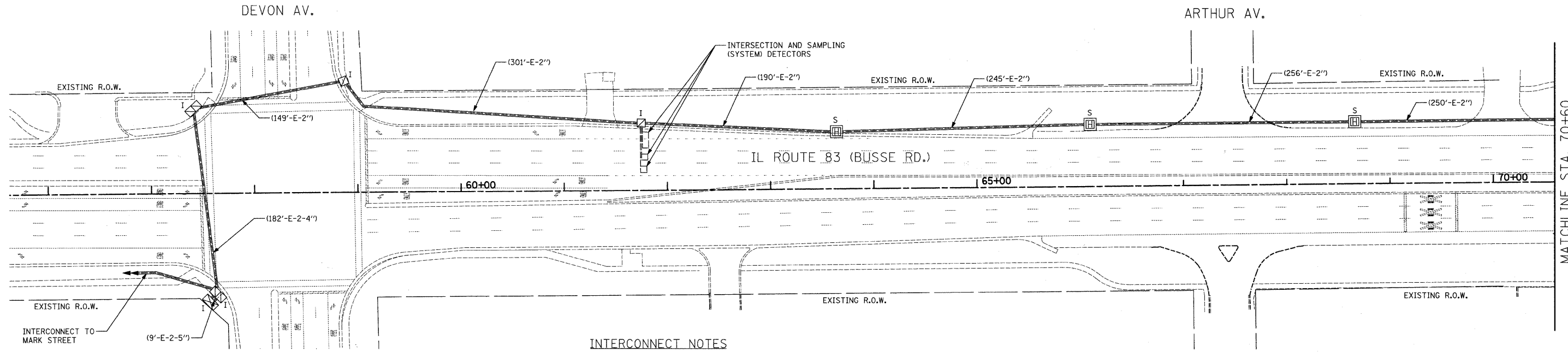
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	09-00057-00-CH	COOK	103	55
CONTRACT NO. 63523				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-90036601				

** COSTS FOR EMERGENCY VEHICLE PREEMPTION EQUIPMENT INSTALLATION AND MAINTENANCE ARE THE RESPONSIBILITY OF THE VILLAGE OF ELK GROVE VILLAGE.

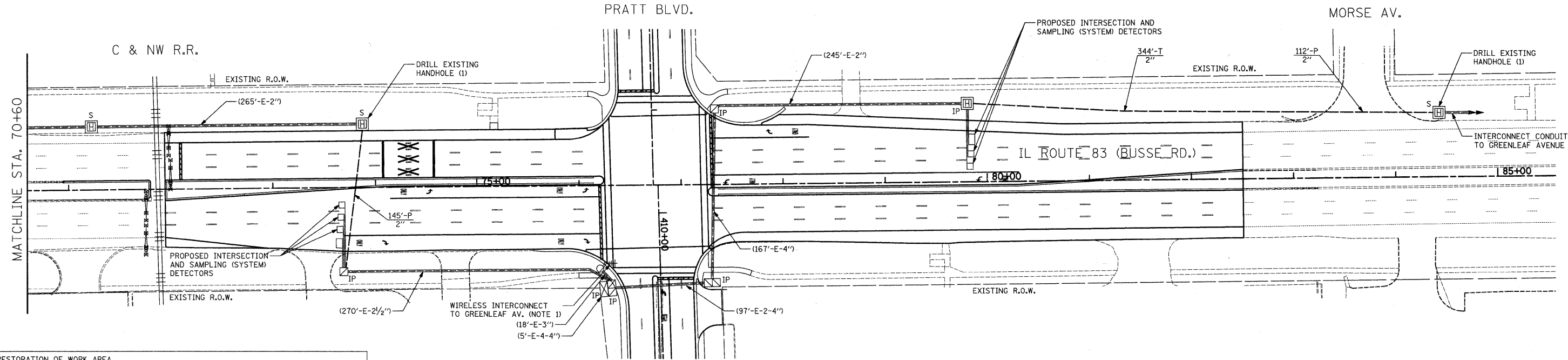
NOTE:
 THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "EAGLE" TO MATCH THE EXISTING ADJACENT SYSTEM.

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ALIGNED	
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NOTE BOOK	
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FILE NAME	

DATE	
BY	
REVIEWED	
PLANNED	
GRADES CHECKED	
NOTE BOOK	
NO.	
FILE NAME	



- INTERCONNECT NOTES**
1. THE WIRELESS INTERCONNECT SYSTEM BETWEEN PRATT BLVD. AND GREENLEAF AV. SHALL REMAIN FOR USE WITH THE PERMANENT SIGNAL INSTALLATION. ALL RADIO EQUIPMENT AND CABLING SHALL BE RELOCATED TO THE PERMANENT SIGNAL INSTALLATION. THIS WORK SHALL BE PAID FOR AS "WIRELESS INTERCONNECT (COMPLETED)." PERMANENT FIBER OPTIC INTERCONNECT SHALL BE INSTALLED BY OTHERS AS A PART OF THE GREENLEAF AV. PROJECT (CONTRACT NO. 63527).
 2. THE WORK AT THE INTERSECTION OF IL ROUTE 83 AND GREENLEAF AV. IS TO BE COMPLETED BY OTHERS UNDER CONTRACT NO. 63527. THE CONTRACTOR SHALL CLOSELY COORDINATE THEIR TRAFFIC SIGNAL WORK WITH THE TRAFFIC SIGNAL WORK BEING PERFORMED UNDER THE GREENLEAF AV. CONTRACT.



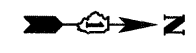
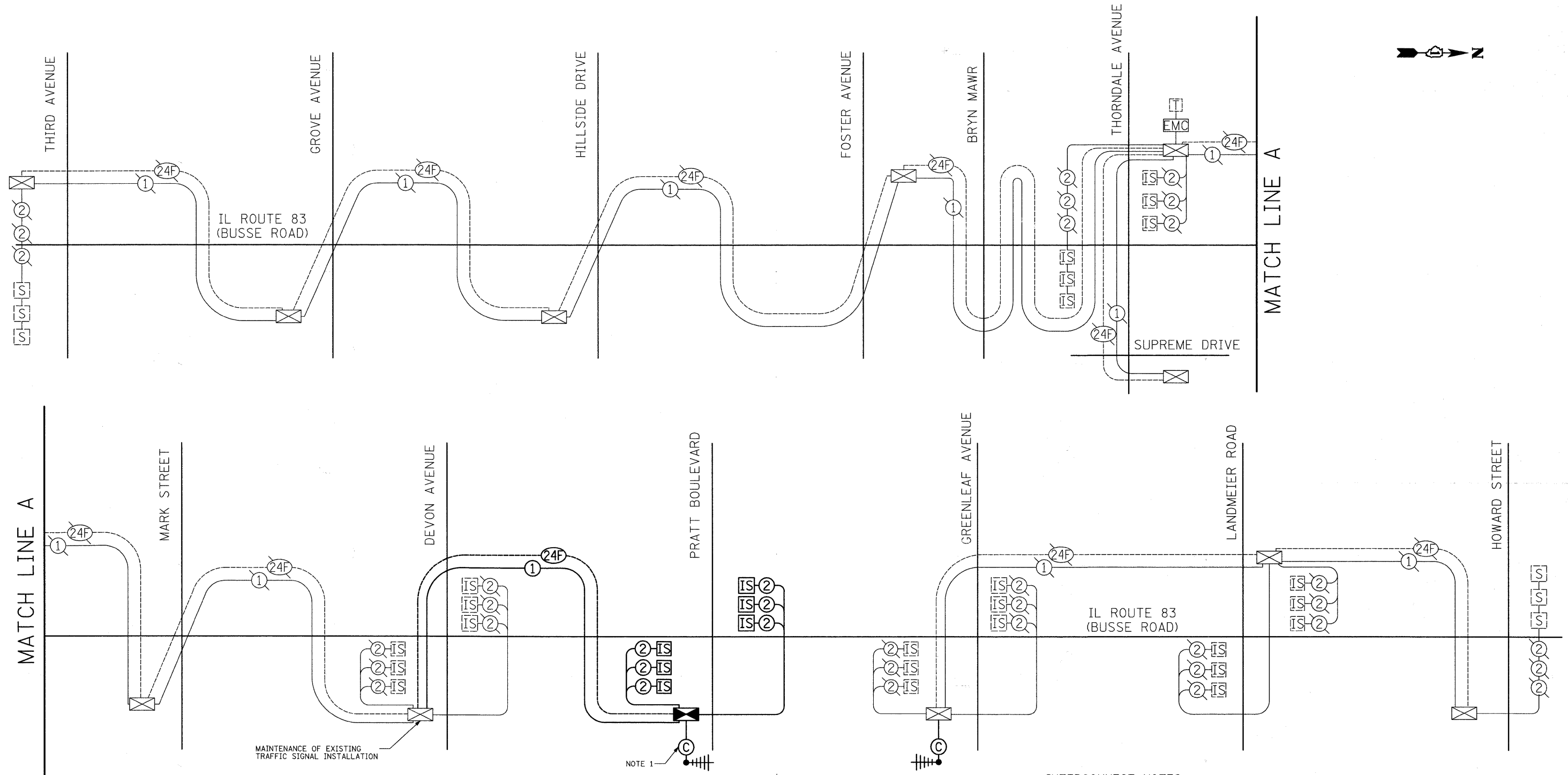
RESTORATION OF WORK AREA.
 RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE COST OF THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDER, MEDIAN, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SALT TOLERANT SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

NOTE:
 THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

FILE NAME = ...\\05.Interconnect Plan.dgn	USER NAME = jet	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL ROUTE 83 (BUSSE ROAD) TRAFFIC SIGNAL INTERCONNECT PLAN	F.A.P. RTE. 334	SECTION 09-00057-00-CH	COUNTY COOK	TOTAL SHEETS 103	SHEET NO. 56		
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -	NOT TO SCALE			SHEET NO. 1 OF 1 SHEETS	CONTRACT NO. 63523					
PLOT DATE = 10/21/2010	DATE = 10-22-10	REVISED -										
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003660												

PLAN	DESIGNED	DATE
NOTE BOOK	ALIGNED	BY
NO.	CHECKED	
	FILE NAME	

PROFILE	DESIGNED	DATE
NOTE BOOK	GRADES	BY
NO.	NOTED	
	NOTATIONS	



MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION

NOTE 1

INTERCONNECT NOTES

1. THE WIRELESS INTERCONNECT SYSTEM BETWEEN PRATT BLVD. AND GREENLEAF AV. SHALL REMAIN FOR USE WITH THE PERMANENT SIGNAL INSTALLATION. ALL RADIO EQUIPMENT AND CABLING SHALL BE RELOCATED TO THE PERMANENT SIGNAL INSTALLATION. THIS WORK SHALL BE PAID FOR AS "WIRELESS INTERCONNECT (COMPLETE)." PERMANENT FIBER OPTIC INTERCONNECT SHALL BE INSTALLED BY OTHERS AS A PART OF THE GREENLEAF AV. PROJECT (CONTRACT NO. 63527).
2. THE WORK AT THE INTERSECTION OF IL ROUTE 83 AND GREENLEAF AV. IS TO BE COMPLETED BY OTHERS UNDER CONTRACT NO. 63527. THE CONTRACTOR SHALL CLOSELY COORDINATE THEIR TRAFFIC SIGNAL WORK WITH THE TRAFFIC SIGNAL WORK BEING PERFORMED UNDER THE GREENLEAF AV. CONTRACT.

NOTE:
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

INTERCONNECT SCHEDULE OF QUANTITIES		
PAY ITEM	UNIT	QNTY.
CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL	FOOT	344
CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL	FOOT	257
TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	344
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
DRILL EXISTING HANDHOLE	EACH	2
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	1923
WIRELESS INTERCONNECT (COMPLETE)	EACH	1
FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM12F	FOOT	2390
REMOVE FIBER OPTIC CABLE FROM CONDUIT	FOOT	1935
ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	2367

FILE NAME = ...\\06.Interconnect.Schematic.dgn

USER NAME = jet
PLOT SCALE = 50,0000 FT / IN.
PLOT DATE = 10/21/2010

DESIGNED - BLG
DRAWN - BLG
CHECKED - DJK
DATE - 10-22-10

REVISED -
REVISED -
REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL ROUTE 83 (BUSSE ROAD)
INTERCONNECT SCHEMATIC**

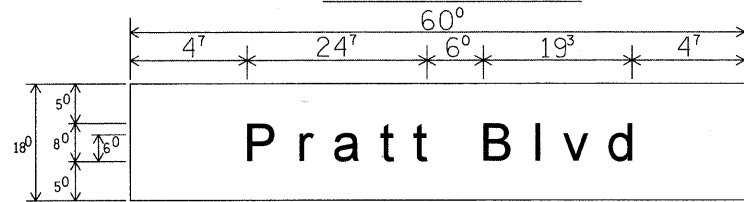
NOT TO SCALE SHEET NO. 1 OF 1 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	09-00057-00-CH	COOK	103	57
CONTRACT NO. 63523				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-90036601				

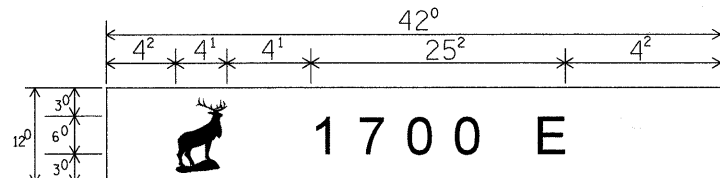
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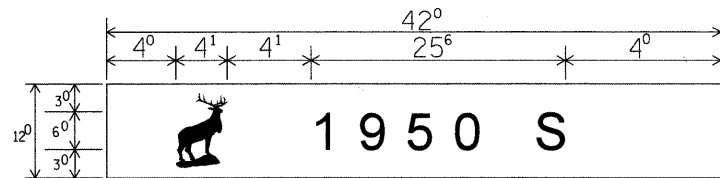
PANEL SIGN DESIGN TYPE 1



Sq. M Each 7.50 Sq. Ft. Each 2 Required Design Series D

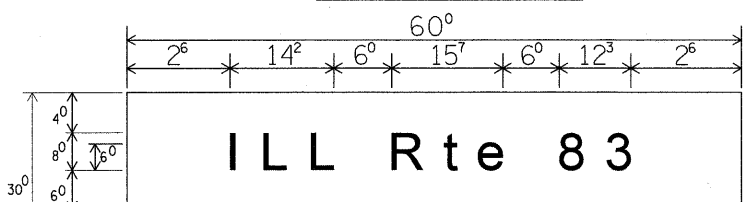


Sq. M Each 3.50 Sq. Ft. Each 2 Required Design Series D



Sq. M Each 3.50 Sq. Ft. Each 2 Required Design Series D

PANEL SIGN DESIGN TYPE 2



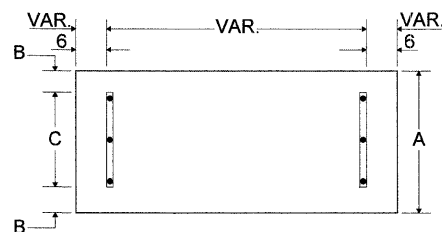
Sq. M Each 12.50 Sq. Ft. Each 2 Required Design Series D

NOTE: SIGN DIMENSIONS ARE IN ENGLISH UNITS.

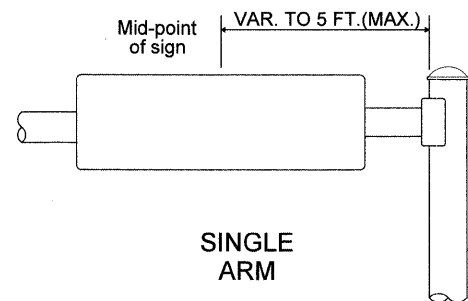
GENERAL NOTES

- WHERE MAST ARM MOUNTED STREET SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 8877001, 877002, 877006, 877011, 877012, AS APPLICABLE. PLUS TWO (2) SIGN PANELS 2'-6" X 8'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR THE STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITIES.
 - ALL SIGNS SHALL HAVE A WHITE REFLECTORIZED LEGEND AND BORDER ON A GREEN REFLECTORIZED BACKGROUND, TYPE A SHEETING.
 - THE SIGN LENGTH SHOULD BE INCREASED IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHALL NOT EXCEED 8'0".
 - ALL BORDERS SHALL BE 3/4" WIDE AND CORNER RADIUS SHALL BE 2-1/4".
 - SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS. LOCAL SUPPLIERS OF THE SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM ARE:
 * J.O. HERBERT CO. MIDLOTHIAN, VA
 * WESTERN REMAC INC. WOODRIDGE, IL
- PARTS LISTING:**
 SIGN CHANNEL PART #HPN053 (MED. CHANNEL)
 SIGN SCREWS 1/4" x 14 x 1" H.W.H #3
 BRACKETS PART #HPN034 (UNIVERSAL)
 CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING
 OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BACKET OF THE ABOVE PRODUCT.

SUPPORTING CHANNELS



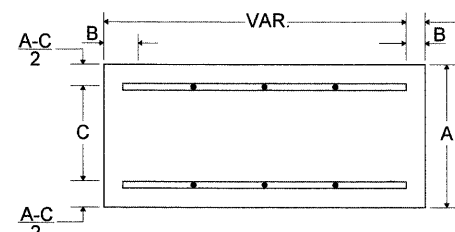
A	B	C
18"	2"	14"



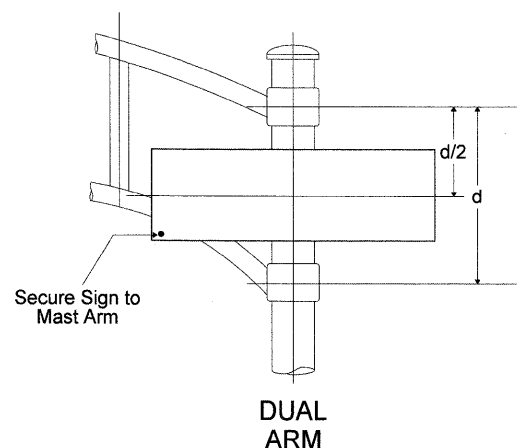
SINGLE ARM

SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM shall be used. See Note #5.

SUPPORTING CHANNELS



A	B	C
18"	2"	12"
30"	2"	22"



DUAL ARM

UPPER TO LOWER CASE SPACING CHART 8-6 INCH SERIES "C" & "D"

EXAMPLE, 2³ DENOTES 3/8"

SERIES	SECOND LETTER															
	a c c d e		b h i k		f w		j		s t		v y		x		z	
	g o q	l m n p r u														
A W X	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ²	1 ⁴	0 ⁶	1 ⁰	1 ¹	1 ⁴	0 ⁶	1 ⁰	1 ¹	1 ²	1 ²	1 ⁴
B	1 ⁴	1 ⁵	2 ⁰	2 ¹	1 ⁴	1 ⁵	1 ¹	1 ²	1 ⁴	1 ⁵	1 ²	1 ⁴	1 ²	1 ⁴	1 ⁶	1 ⁷
C E G	1 ⁴	1 ⁵	2 ⁰	2 ¹	1 ²	1 ⁴	0 ⁶	1 ⁰	1 ²	1 ⁴	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ⁴	1 ⁵
D O Q R	1 ⁴	1 ⁵	2 ⁰	2 ¹	1 ⁴	1 ⁵	0 ⁶	1 ⁰	1 ²	1 ⁴	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ⁴	1 ⁵
F	0 ⁵	0 ⁶	1 ⁴	1 ⁵	0 ⁶	1 ⁰	0 ⁵	0 ⁶	0 ⁶	1 ⁰	0 ⁶	1 ⁰	0 ⁶	1 ⁰	1 ¹	1 ²
H I M N	2 ⁰	2 ¹	2 ²	2 ⁴	2 ⁰	2 ¹	1 ⁴	1 ⁵	1 ⁶	1 ⁷	1 ⁶	1 ⁷	2 ⁰	2 ¹	2 ⁰	2 ¹
J U	2 ⁰	2 ¹	2 ⁰	2 ¹	1 ⁶	1 ⁷	1 ⁴	1 ⁵	1 ⁶	1 ⁷	1 ⁶	1 ⁷	1 ⁶	1 ⁷	2 ⁰	2 ¹
K L	1 ¹	1 ²	1 ⁶	1 ⁷	1 ¹	1 ²	0 ⁵	0 ⁶	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ²	1 ⁴
P	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ²	1 ⁴	0 ⁵	0 ⁶	1 ¹	1 ²	1 ¹	1 ²	1 ²	1 ⁴	1 ²	1 ⁴
S	1 ²	1 ⁴	1 ⁶	1 ⁷	1 ²	1 ⁴	0 ⁶	1 ⁰	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴
T	1 ¹	1 ²	1 ⁶	1 ⁷	0 ⁶	1 ⁰	0 ⁶	1 ⁰	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ²	1 ⁴
V	0 ⁶	1 ⁰	1 ⁴	1 ⁵	1 ¹	1 ²	0 ⁶	1 ⁰	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴
Y	0 ⁵	0 ⁶	1 ⁴	1 ⁵	0 ⁶	1 ⁰	0 ⁵	0 ⁶	0 ⁵	0 ⁷	0 ⁵	0 ⁶	0 ⁶	1 ⁰	1 ¹	1 ²
Z	1 ⁶	1 ⁷	2 ²	2 ⁴	1 ⁶	1 ⁷	1 ²	1 ⁴	1 ⁶	1 ⁷	1 ⁶	1 ⁷	1 ⁶	1 ⁷	2 ⁰	2 ¹

LOWER CASE TO LOWER CASE SPACING CHART 6 INCH SERIES "C" & "D"

SERIES	SECOND LETTER															
	a c c d e		b h i k		f w		j		s t		v y		x		z	
	g o q	l m n p r u														
F	1 ⁶	1 ⁷	2 ²	2 ⁴	1 ⁶	1 ⁷	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ⁶	1 ⁷	1 ⁶	1 ⁷
I	1 ²	1 ⁴	1 ⁶	1 ⁷	1 ¹	1 ²	0 ⁵	0 ⁶	1 ¹	1 ²	1 ¹	1 ²	1 ²	1 ⁴	1 ²	1 ⁴
R	1 ²	1 ⁴	1 ⁶	1 ⁷	1 ²	1 ⁴	0 ⁶	1 ⁰	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴
S	1 ²	1 ⁴	1 ⁶	1 ⁷	1 ²	1 ⁴	0 ⁶	1 ⁰	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴
T	1 ²	1 ⁴	1 ⁶	1 ⁷	1 ²	1 ⁴	0 ⁶	1 ⁰	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴
L	0 ⁶	1 ⁰	1 ²	1 ⁴	0 ⁶	1 ⁰	0 ³	0 ³	0 ⁵	0 ⁶	0 ⁵	0 ⁶	0 ⁶	1 ⁰	0 ⁶	1 ⁰
E	1 ²	1 ⁴	1 ⁶	1 ⁷	1 ²	1 ⁴	0 ⁶	1 ⁰	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²
T	1 ¹	1 ²	1 ⁴	1 ⁵	1 ¹	1 ²	0 ⁵	0 ⁶	0 ⁶	1 ⁰	0 ⁶	1 ⁰	1 ¹	1 ²	1 ¹	1 ²
E	1 ¹	1 ²	1 ⁴	1 ⁵	1 ¹	1 ²	0 ⁵	0 ⁶	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²
R	1 ²	1 ⁴	1 ⁶	1 ⁷	1 ¹	1 ²	0 ⁵	0 ⁶	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²

NUMBER TO NUMBER

SPACING CHART 8 INCH SERIES "C" & "D"

SERIES	SECOND NUMBER																			
	0		1		2		3		4		5		6		7		8		9	
	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
F	1 ⁶	1 ⁷	1 ⁶	1 ⁷	1 ⁴	1 ⁵	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ⁶	1 ⁷	1 ²	1 ⁴	1 ⁶	1 ⁷	1 ⁶	1 ⁷
I	2 ⁰	2 ¹	2 ⁰	2 ¹	2 ⁰	2 ¹	1 ⁶	1 ⁷	1 ⁴	1 ⁵	2 ⁰	2 ¹	2 ⁰	2 ¹	1 ⁴	1 ⁵	2 ⁰	2 ¹	2 ⁰	2 ¹
R	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ²	1 ⁴	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ¹	1 ²	1 ⁶	1 ⁷	1 ⁴	1 ⁵
S	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ¹	1 ²	1 ¹	1 ²	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ¹	1 ²	1 ⁴	1 ⁵	1 ⁴	1 ⁵
L	1 ⁶	1 ⁷	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ²	1 ⁴	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ¹	1 ²	1 ⁴	1 ⁵	1 ⁴	1 ⁵
E	1 ²	1 ⁴	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ²	1 ⁵	0 ⁵	0 ⁶	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ¹	1 ²	1 ⁴	1 ⁵	1 ²	1 ⁴
T	1 ⁶	1 ⁷	1 ⁶	1 ⁷	1 ⁴	1 ⁵	1 ²	1 ⁵	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ⁶	1 ⁷	1 ²	1 ⁴	1 ⁶	1 ⁷	1 ⁴	1 ⁵

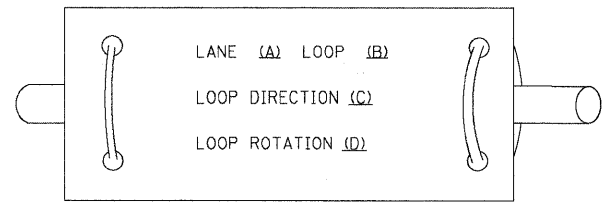
UPPER AND LOWER CASE LETTER WIDTHS

LETTERS	6 INCH UPPER CASE LETTERS		8 INCH UPPER CASE LETTERS		LETTERS	6 INCH LOWER CASE LETTERS	
	SERIES		SERIES			SERIES	
	C	D	C	D		C	D
A	3 ⁶	5 ⁰	5 ⁰	6 ⁵	a	3 ⁵	4 ²
B	3 ²	4 ⁰	4 ³	5 ³	b	3 ⁵	4 ²
C	3 ²	4 ⁰	4 ³	5 ³	c	3 ⁵	4 ¹
D	3 ²	4 ⁰	4 ³	5 ³	d	3 ⁵	4 ²
E	3 ⁰	3 ⁵	4 ⁰	4 ⁷	e	3 ⁵	4 ²
F	3 ⁰	3 ⁵	4 ⁰	4 ⁷	f	2 ³	2 ⁶
G	3 ²	4 ⁰	4 ³	5 ³	g	3 ⁵	4 ²
H	3 ²	4 ⁰	4 ³	5 ³	h	3 ⁵	4 ²
I	0 ⁷	0 ⁷	1 ¹	1 ²	i	1 ¹	1 ¹
J	3 ⁰	3 ⁶	4 ⁰	5 ⁰	j	2 ⁰	2 ²
K	3 ²	4 ¹	4 ³	5 ⁴	k	3 ⁵	4 ²
L	3 ⁰	3 ⁵	4 ⁰	4 ⁷	l	1 ¹	1 ¹
M	3 ⁷	4 ⁵	5 ¹	6 ¹	m	6 ⁰	7 ⁰
N	3 ²	4 ⁰	4 ³	5 ³	n	3 ⁵	4 ²
O	3 ⁴	4 ²	4 ⁵	5 ⁵	o	3 ⁶	4 ³
P	3 ²	4 ⁰	4 ³	5 ³	p	3 ⁵	4 ²
Q	3 ⁴	4 ²	4 ⁵	5 ⁵	q	3 ⁵	4 ²
R	3 ²	4 ⁰	4 ³	5 ³	r	2 ⁶	3 ²
S	3 ²	4 ⁰	4 ³	5 ³	s	3 ⁶	4 ²
T	3 ⁰	3 ⁵	4 ⁰	4 ⁷	t	2 ⁷	3 ²
U	3 ²	4 ⁰					

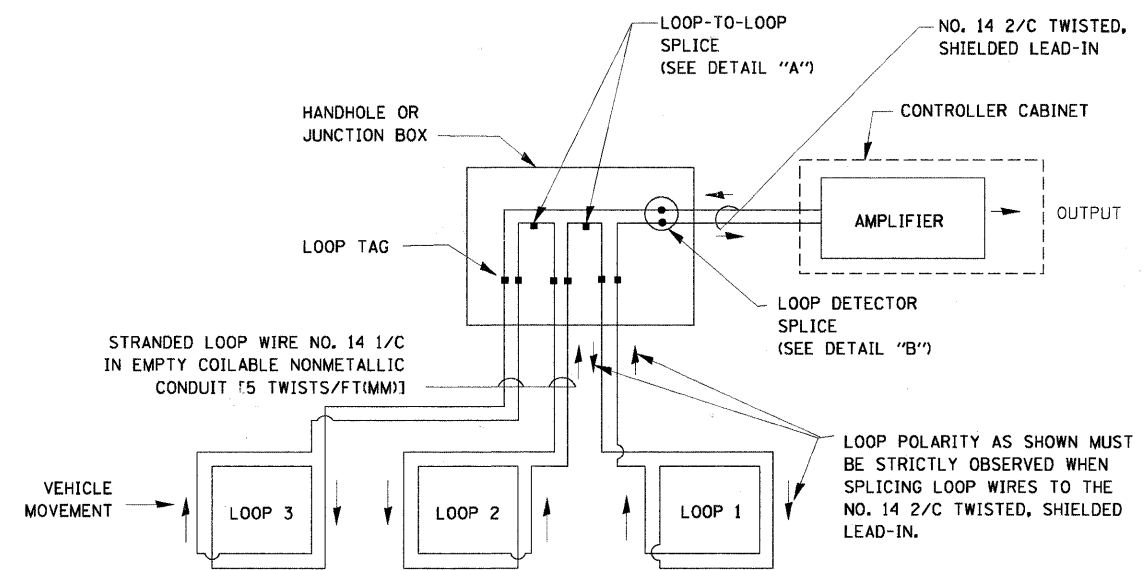
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

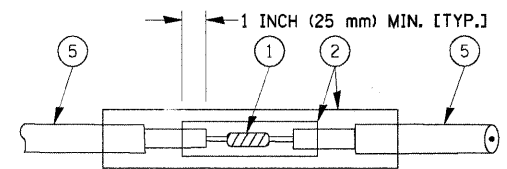


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

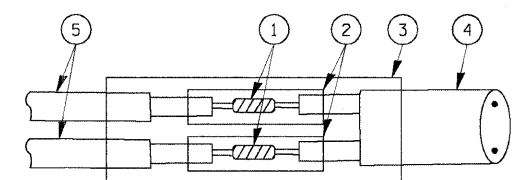


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

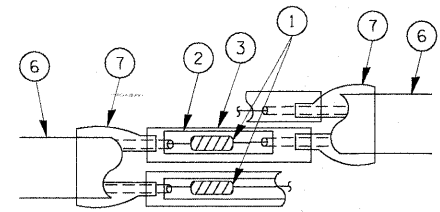


**DETAIL "A"
LOOP-TO-LOOP SPLICE**

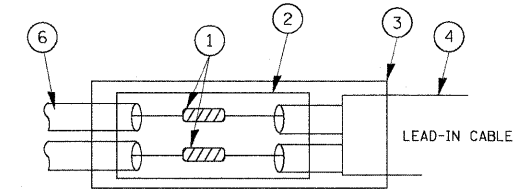


**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

TYPE I LOOP



**DETAIL "A"
LOOP-TO-LOOP SPLICE**



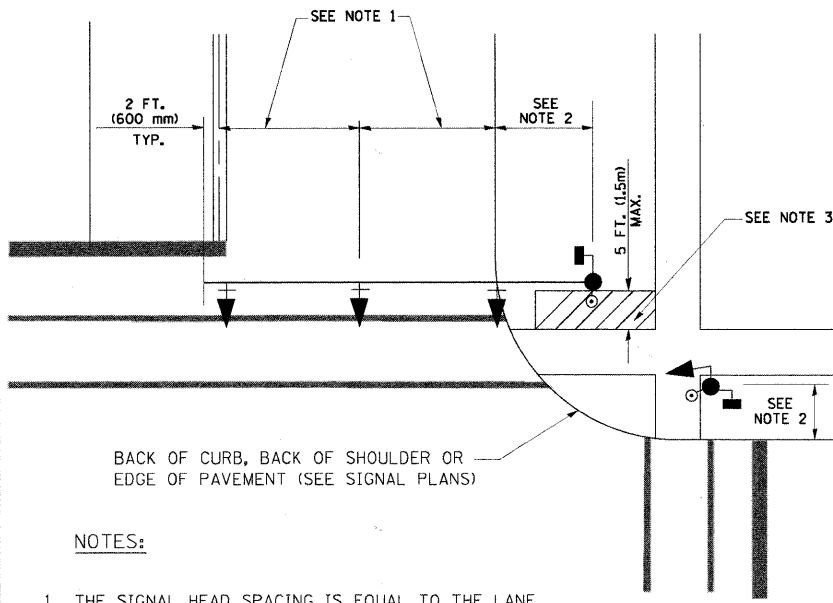
**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

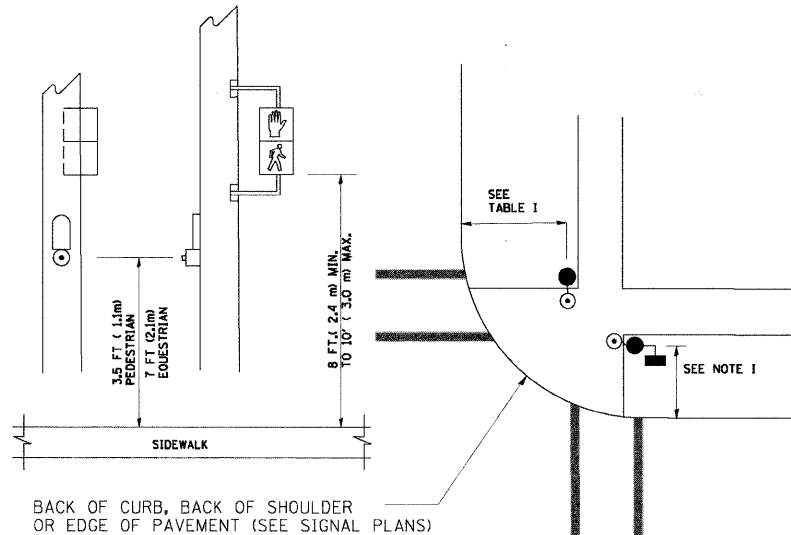
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

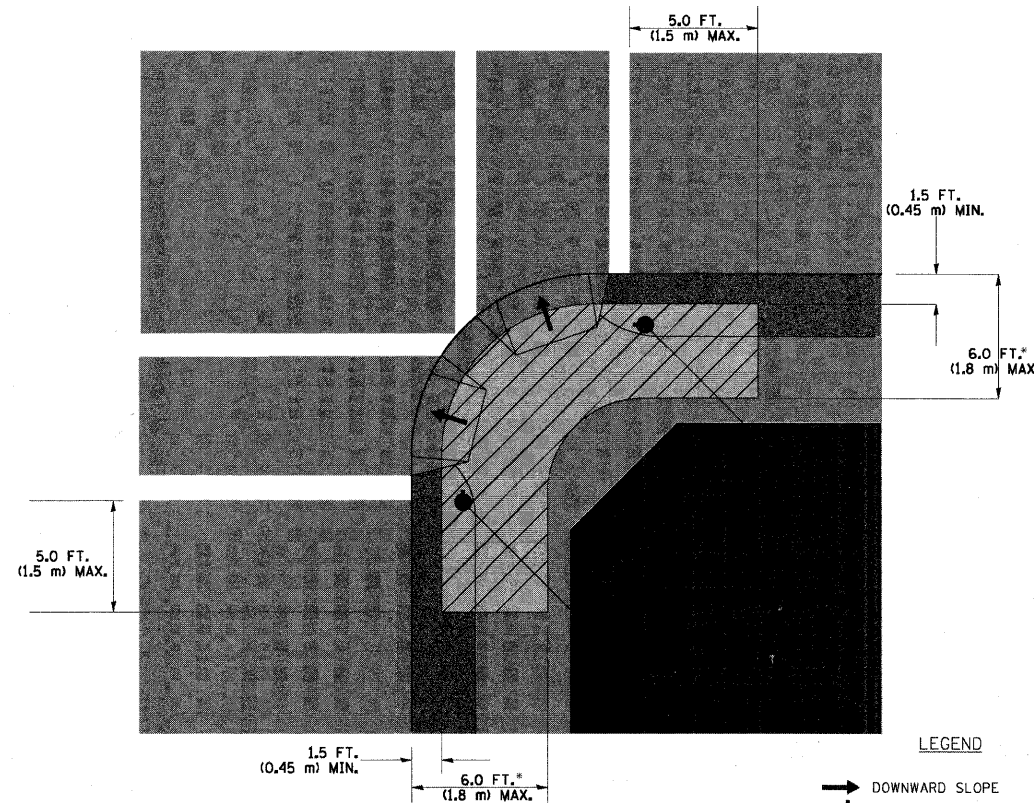
PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



LEGEND

- DOWNWARD SLOPE
- PEDESTRIAN PUSHBUTTON
- ▨ RECOMMENDED PUSHBUTTON LOCATIONS

- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

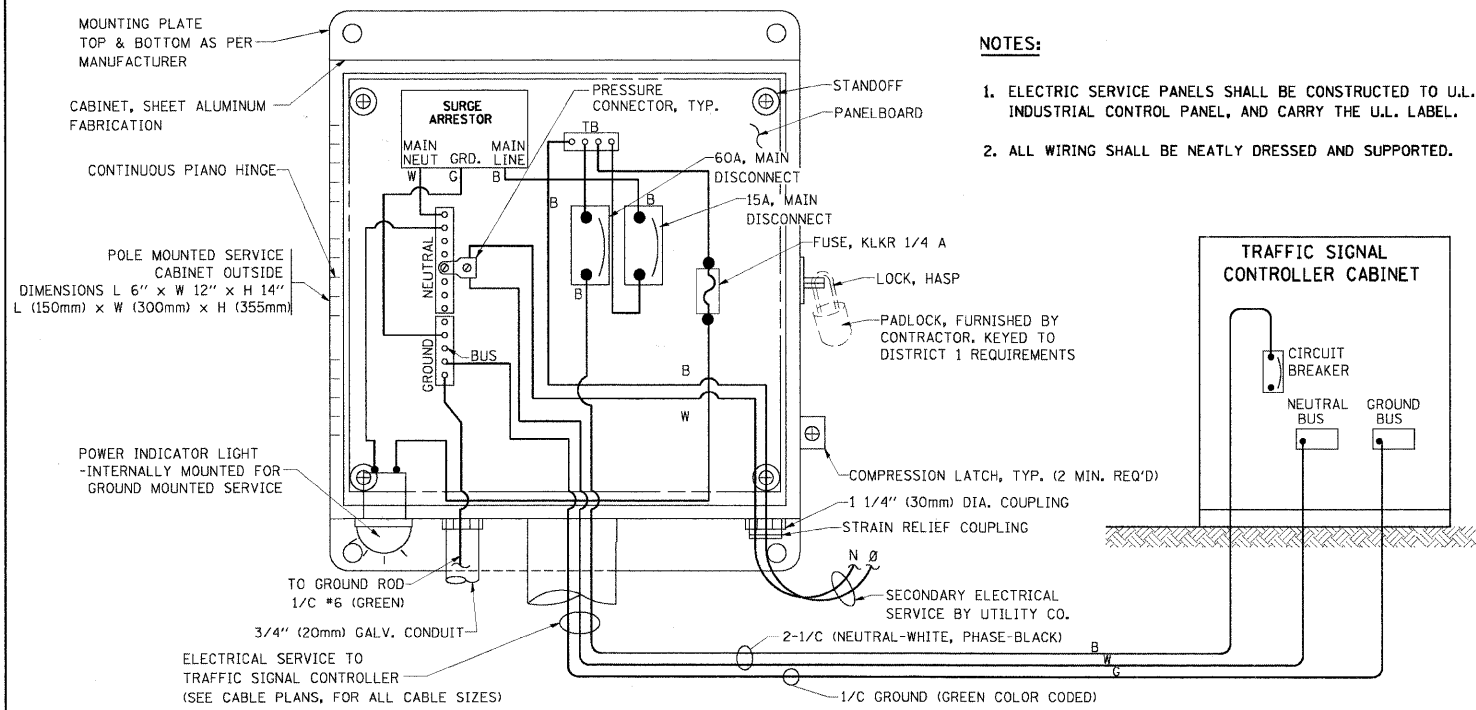
1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

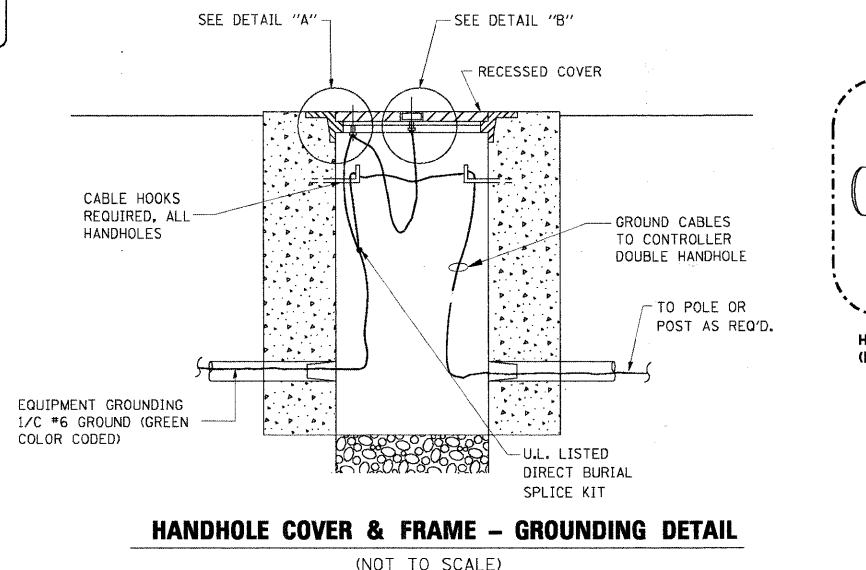
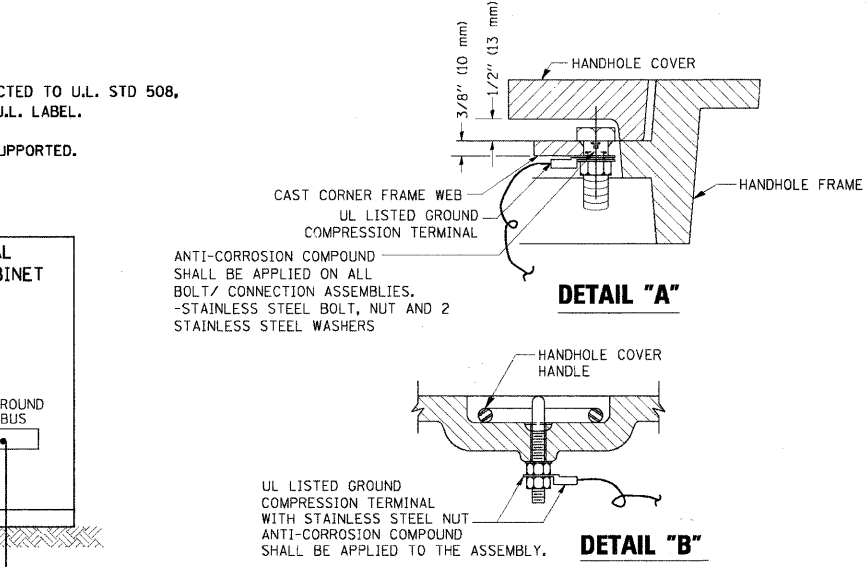
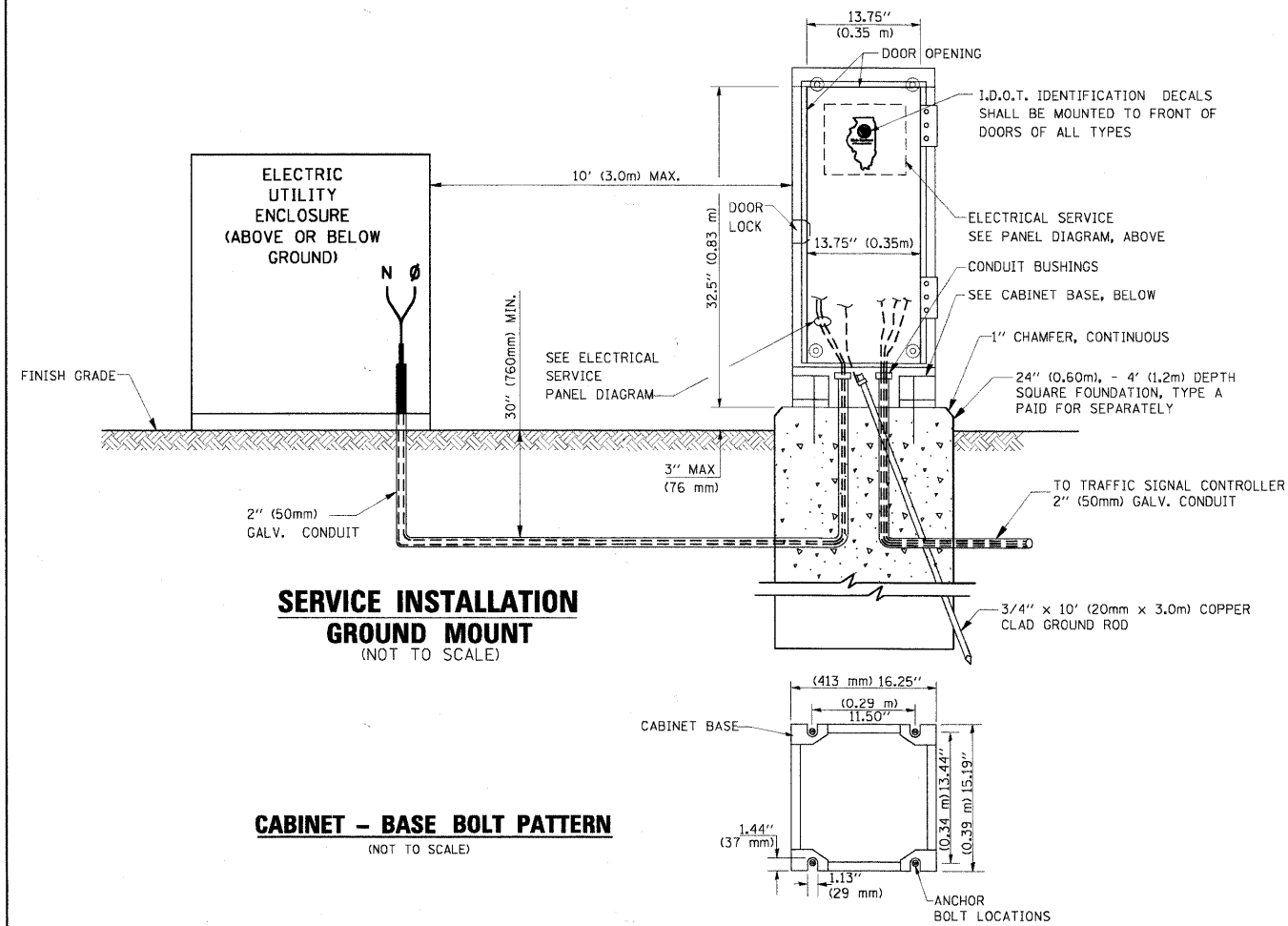
TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.



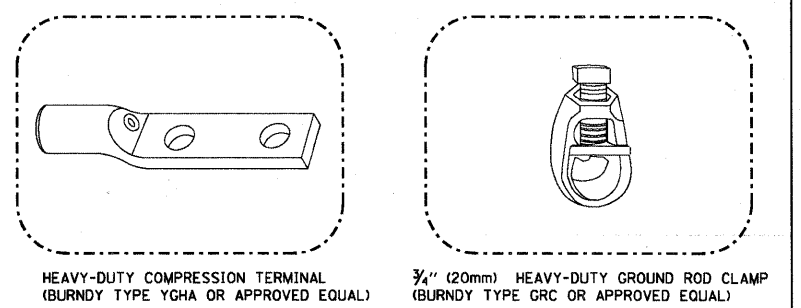
ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
 (NOT TO SCALE)



NOTES:

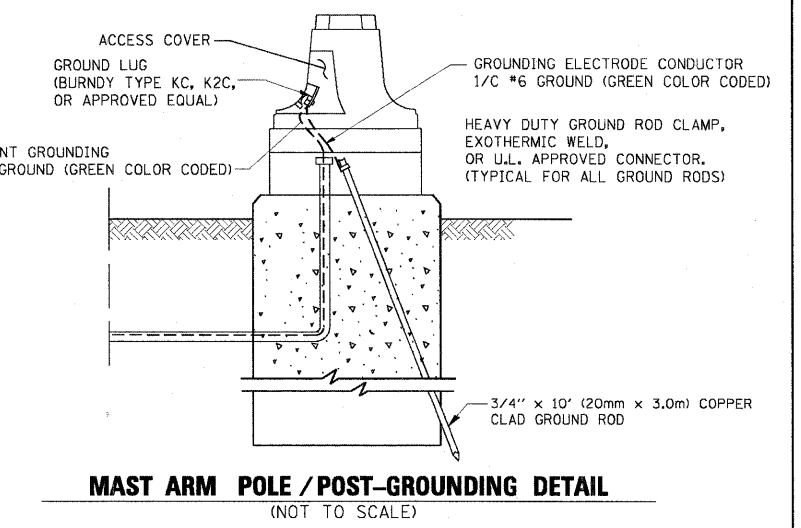
GROUNDING SYSTEM

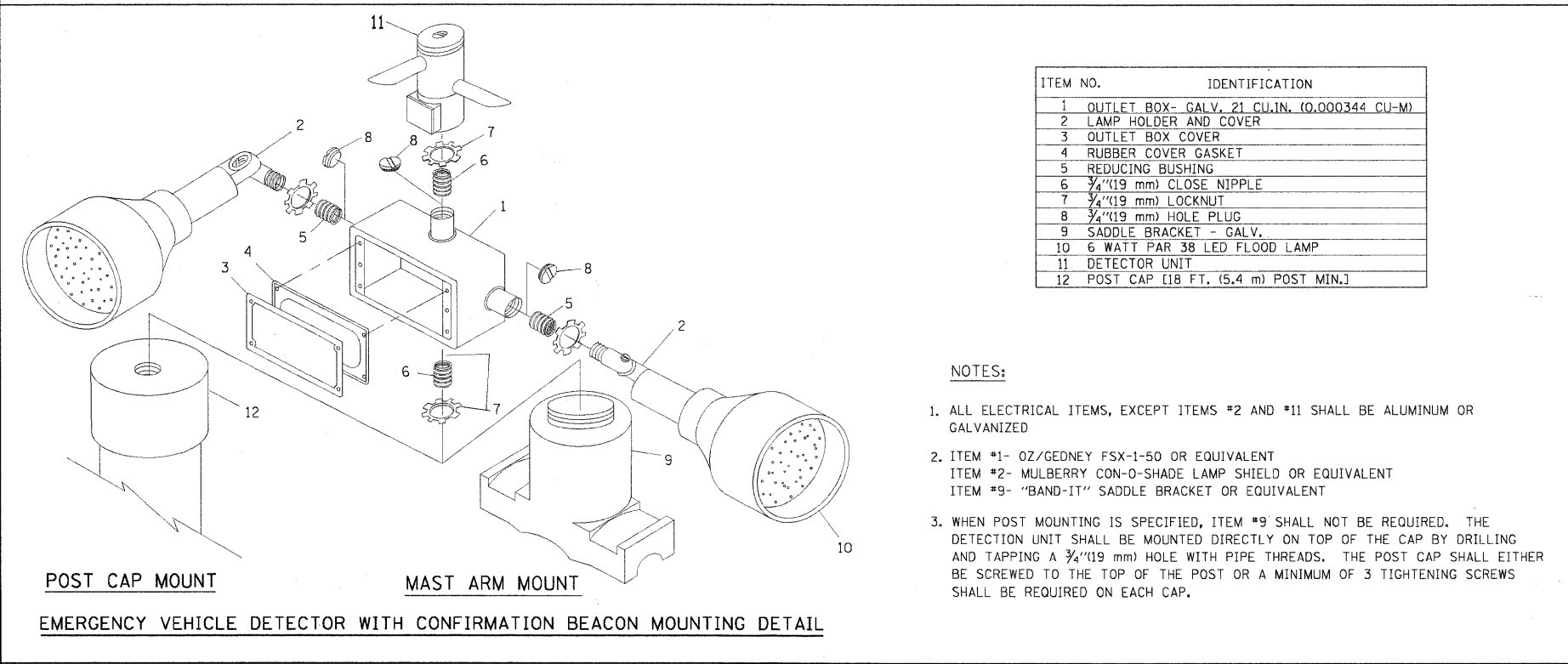
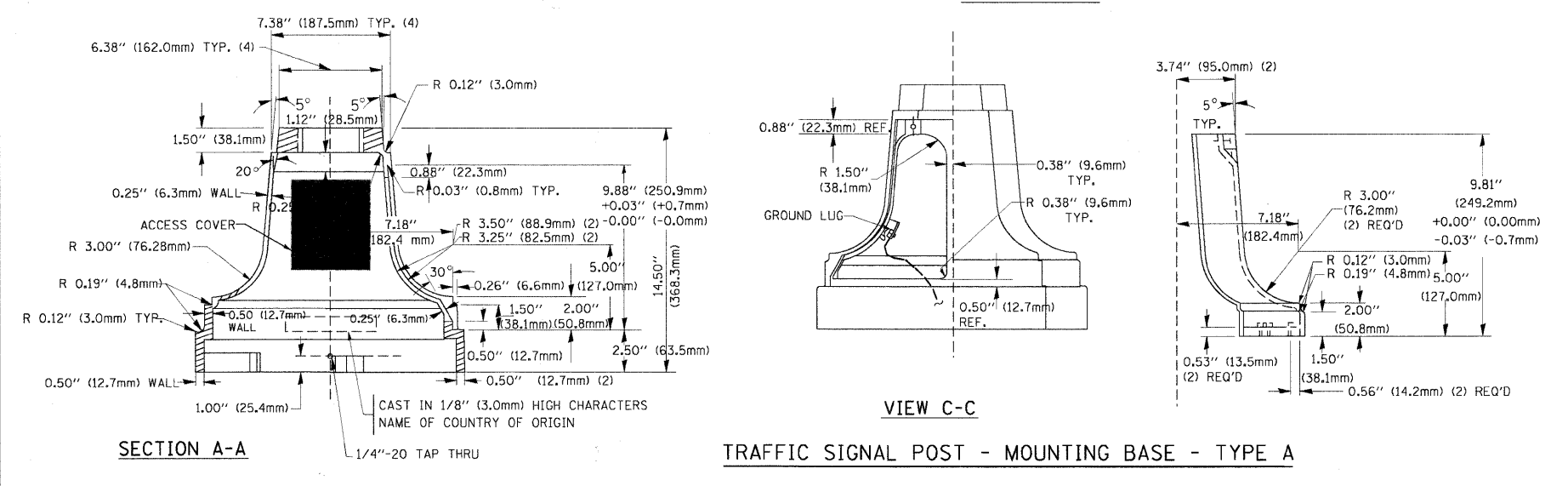
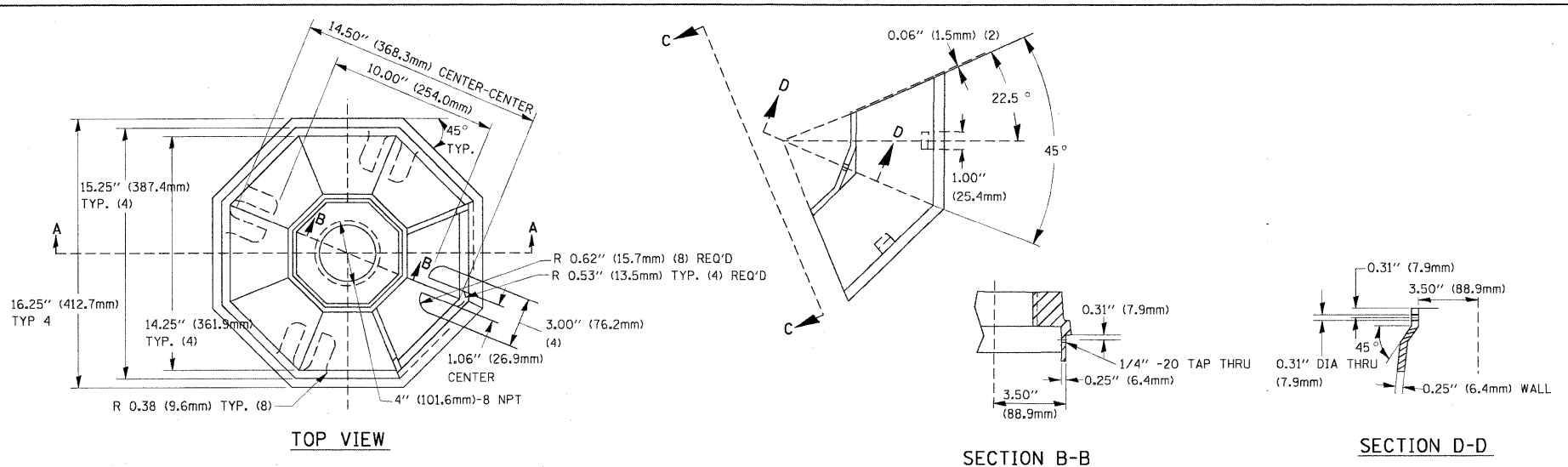
1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



NOTES:

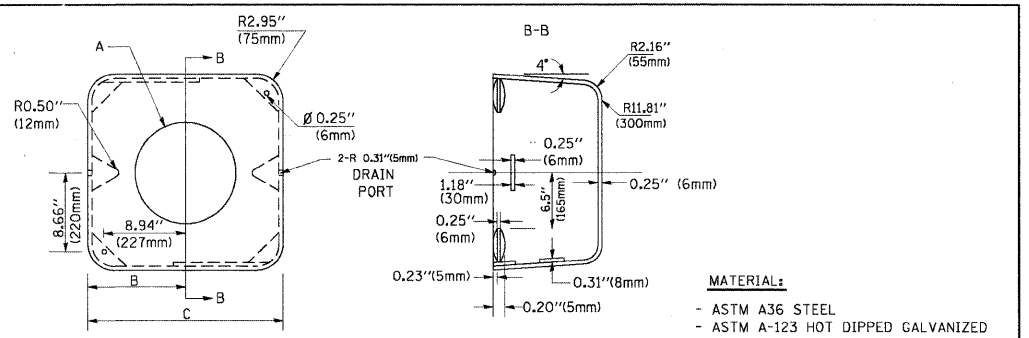
- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.





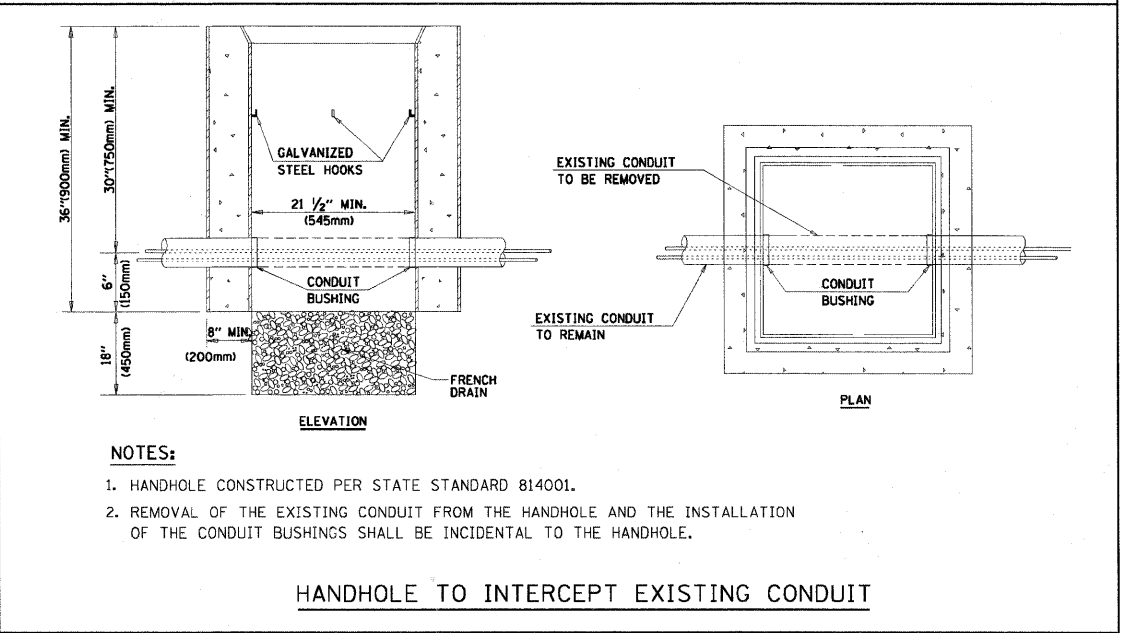
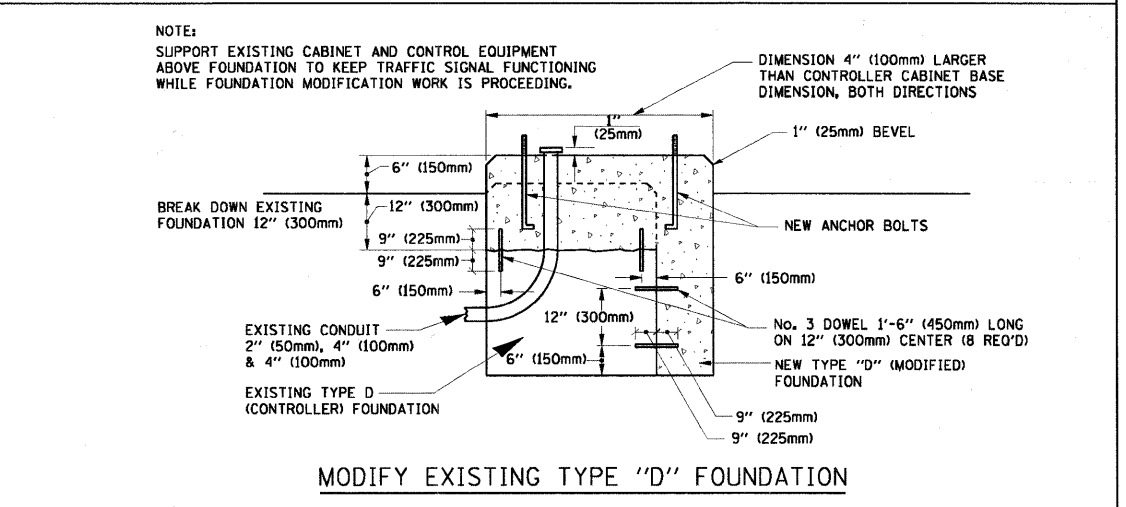
ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4\" (19 mm) CLOSE NIPPLE
7	3/4\" (19 mm) LOCKNUT
8	3/4\" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

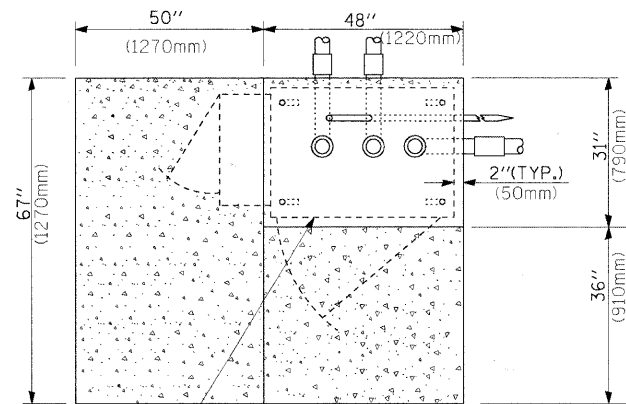
- NOTES:
- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
 - ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
 - WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4\" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



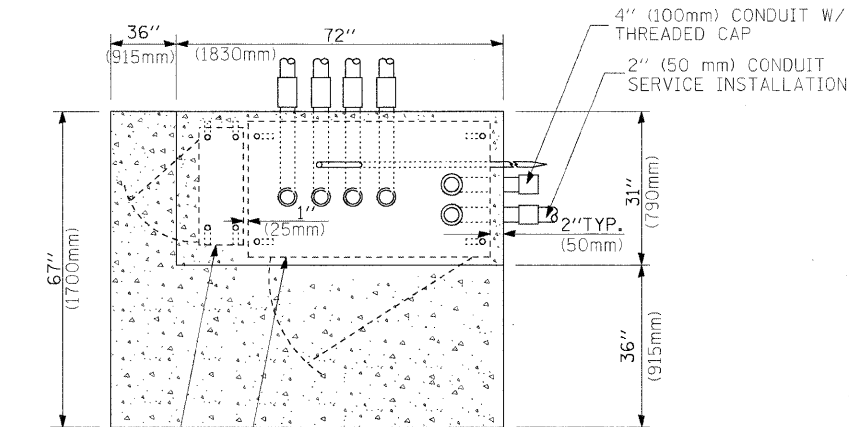
A	B	C	HEIGHT	WEIGHT
VARIES	9.5\" (241mm)	19\" (483mm)	7\" (178mm) - 12\" (300mm)	53 lbs (24kg)
VARIES	10.75\" (273mm)	21.5\" (546mm)	7\" (178mm) - 12\" (300mm)	68 lbs (31 kg)
VARIES	13.0\" (330mm)	26\" (660mm)	7\" (178mm) - 12\" (300mm)	81 lbs (37 kg)
VARIES	18.5\" (470mm)	37\" (940mm)	7\" (178mm) - 12\" (300mm)	126 lbs (57 kg)

- NOTES:
- DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
 - THE SUPPLIER SHALL VERIFIED THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
 - THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

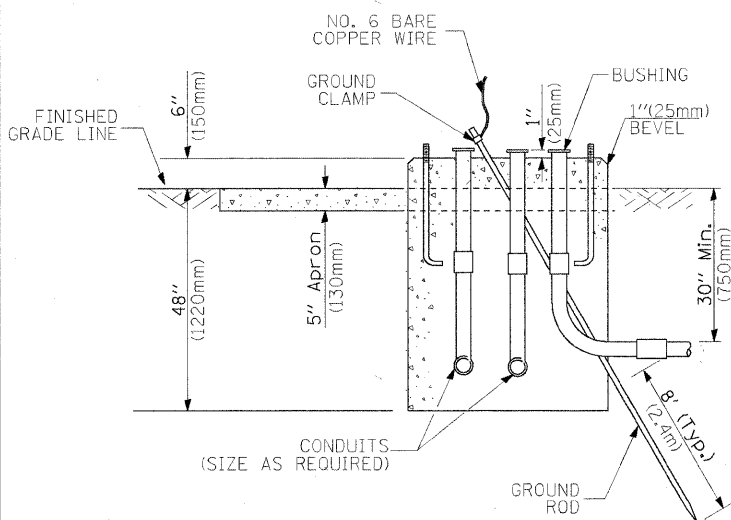




CONTROLLER CABINET BASE
EXISTING APRON
PROPOSED APRON
TOP VIEW

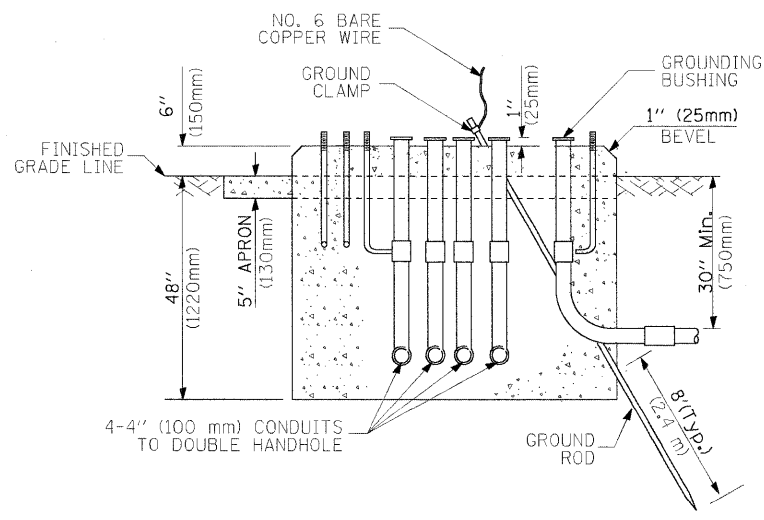


UPS CABINET BASE
CONTROLLER CABINET BASE
APRON
TOP VIEW



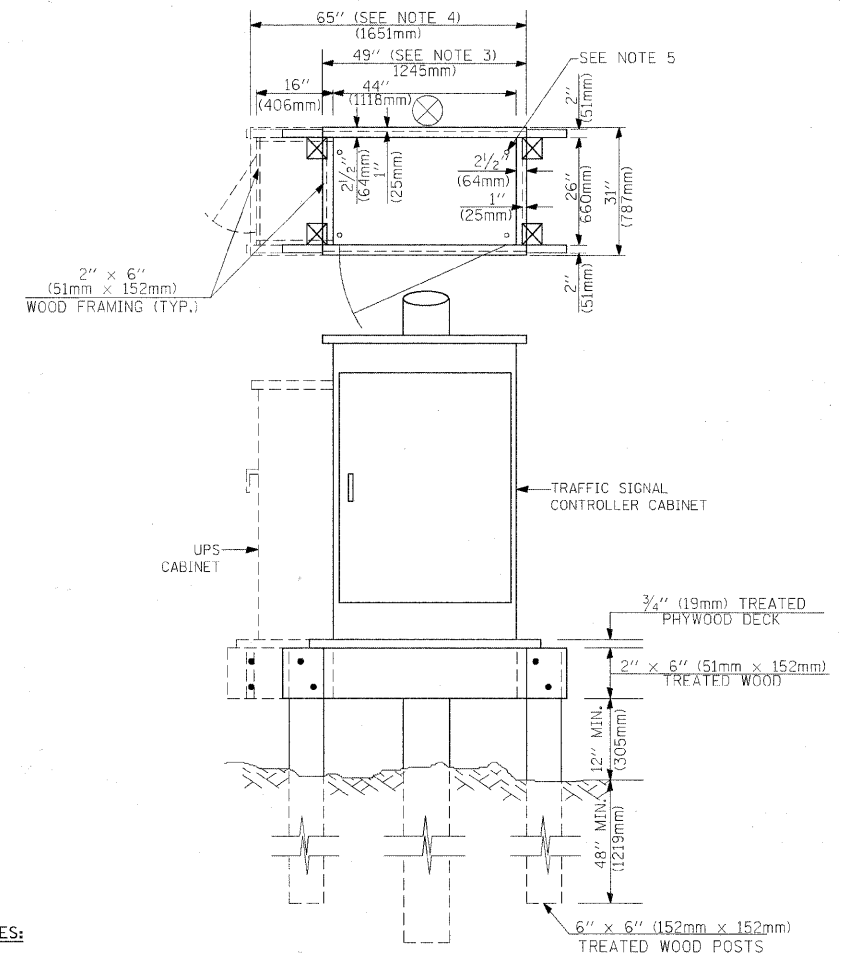
NO. 6 BARE COPPER WIRE
GROUND CLAMP
BUSHING
1" (25mm) BEVEL
30" MIN. (750mm)
5" APRON (130mm)
6" (150mm)
FINISHED GRADE LINE
CONDUITS (SIZE AS REQUIRED)
8" (TYP.)
GROUND ROD

**TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET**



NO. 6 BARE COPPER WIRE
GROUND CLAMP
BUSHING
1" (25mm) BEVEL
30" MIN. (750mm)
5" APRON (130mm)
6" (150mm)
FINISHED GRADE LINE
4-4" (100 mm) CONDUITS TO DOUBLE HANDHOLE
8" (TYP.)
GROUND ROD

**TYPE C
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET**



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

**TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION





MAST ARM LENGTH	① FOUNDATION DEPTH	FOUNDATION DIAMETER	SPIRAL DIAMETER	QUANTITY OF REBARS	SIZE OF REBARS
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
4. For mast arm assemblies with dual arms refer to state standard 878001.

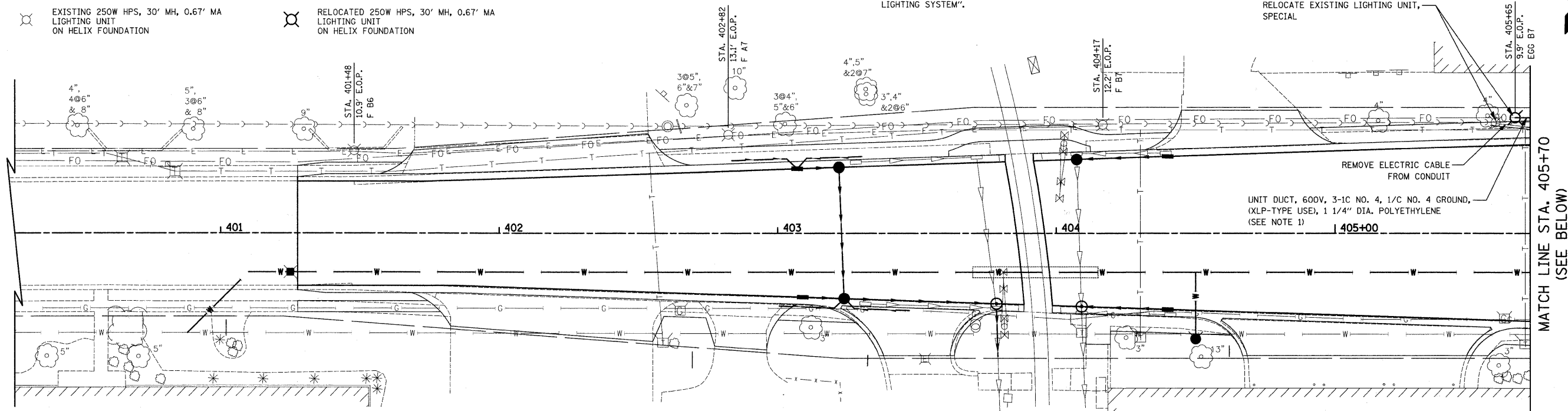
DEPTH OF MAST ARM FOUNDATIONS, TYPE E

LEGEND

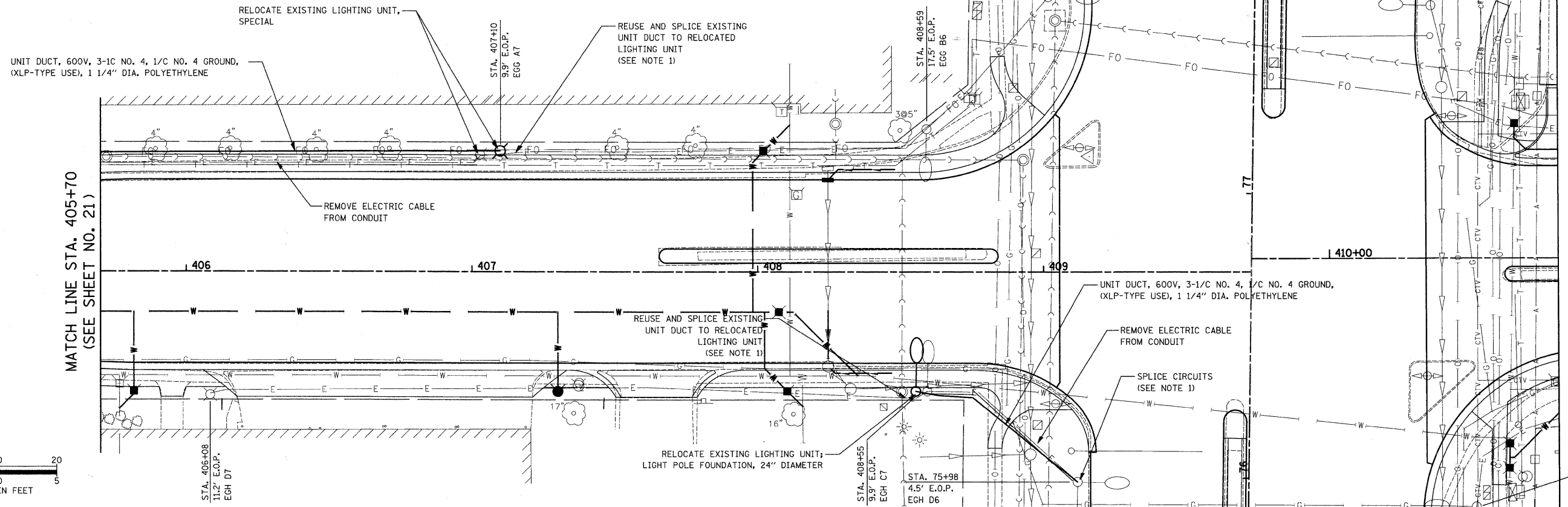
-  EXISTING 400W HPS, 45' MH, 10' MA LIGHTING UNIT ON CONCRETE FOUNDATION
-  RELOCATED 400W HPS, 45' MH, 10' MA LIGHTING UNIT ON CONCRETE FOUNDATION
-  EXISTING 250W HPS, 30' MH, 0.67' MA LIGHTING UNIT ON HELIX FOUNDATION
-  RELOCATED 250W HPS, 30' MH, 0.67' MA LIGHTING UNIT ON HELIX FOUNDATION

NOTES:

1. SEE EXISTING WIRING DIAGRAM ON SHEET 67 FOR CIRCUITRY.
2. SEE LIGHT POLE FOUNDATION ADJUSTMENT DETAIL ON SHEET 68.
3. THERE SHALL BE NO UNDERGROUND SPLICING OF UNIT DUCT CONDUCTORS EXCEPT AT THE POLE BASE.
4. CONTINUITY OF EQUIPMENT GROUNDING CONDUCTOR FROM CONTROLLER TO THE LAST POLE OF EACH IMPACTED CIRCUIT SHALL BE CONFIRMED. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN "MAINTENANCE OF LIGHTING SYSTEM".



MATCH LINE STA. 405+70
(SEE BELOW)



MATCH LINE STA. 410+80
(SEE SHEET NO. 66)



DATE	
BY	
REVIEWED	
PLANNED	
NOTE BOOK	
NO.	
FILE NAME	

DATE	
BY	
REVIEWED	
PLANNED	
NOTE BOOK	
NO.	
FILE NAME	

FILE NAME = ...\\cad\sheet\2363_Light_01.dgn

USER NAME = krk
 PLOT SCALE = 20.0000 "/>

DESIGNED - SJC
 DRAWN - SJC
 CHECKED - DNM
 DATE - 10-22-10

REVISED -
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**IL ROUTE 83 (BUSSE ROAD) AND PRATT BOULEVARD
 LIGHTING PLAN**

SCALE: 1" = 20' SHEET NO. 1 OF 2 SHEETS STA. 401+27.62 TO STA. 410+80.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	09-00057-00-CH	COOK	103	65
CONTRACT NO. 63523				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-900316601				

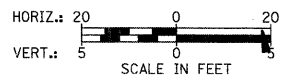
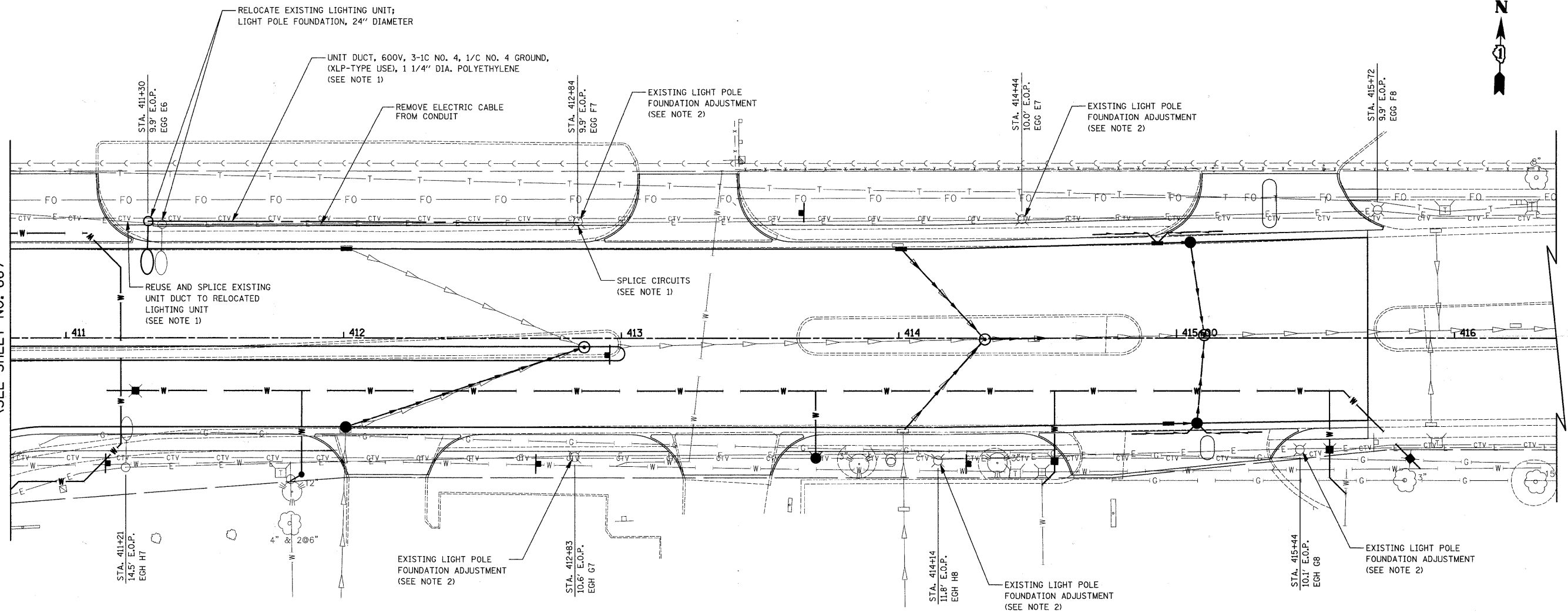
NOTES:

- SEE EXISTING WIRING DIAGRAM ON SHEET 67 FOR CIRCUITRY.
- SEE LIGHT POLE FOUNDATION ADJUSTMENT DETAIL ON SHEET 68.
- THERE SHALL BE NO UNDERGROUND SPLICING OF UNIT DUCT CONDUCTORS EXCEPT AT THE POLE BASE.
- CONTINUITY OF EQUIPMENT GROUNDING CONDUCTOR FROM CONTROLLER TO THE LAST POLE OF EACH IMPACTED CIRCUIT SHALL BE CONFIRMED. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN "MAINTENANCE OF LIGHTING SYSTEM".

PLAN	REVISIONS	DATE
NOTE BOOK NO.	ALIGNED	
	CHECKED	
	PLOTTED	
	FILE NAME	

PROFILE	REVISIONS	DATE
NOTE BOOK NO.	GRADES CHECKED	
	PLANNING	
	NOTATION CHKD	

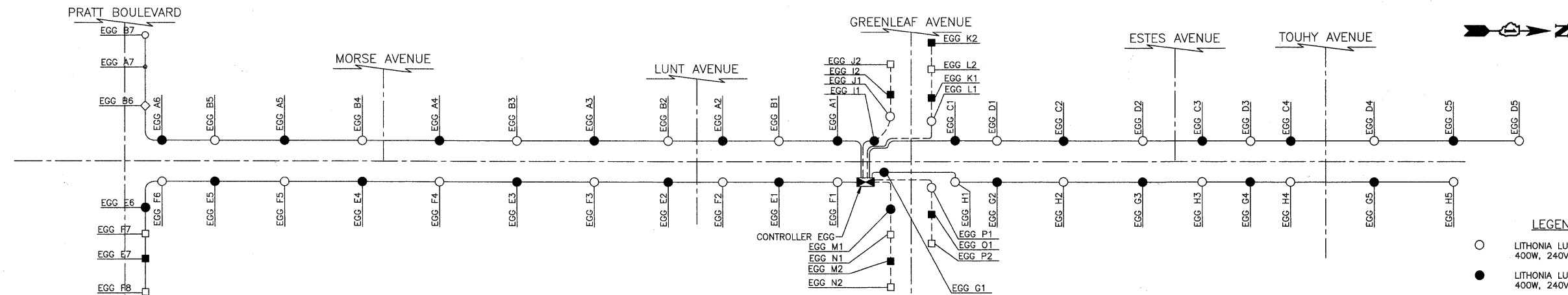
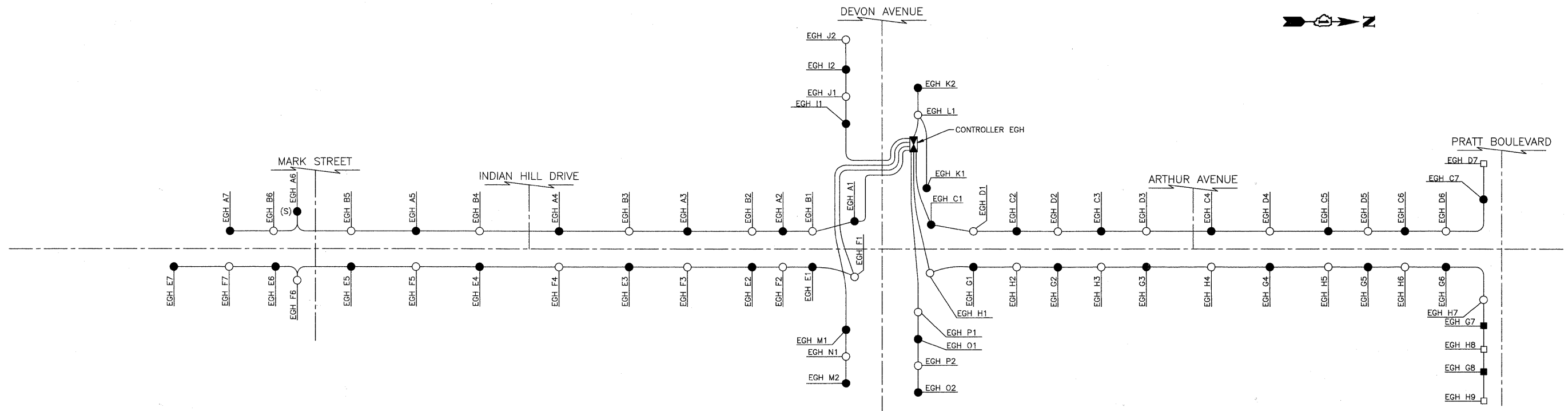
MATCH LINE STA. 410+80
(SEE SHEET NO. 66)



FILE NAME = ...oad\sheet\2363.Light_02.dgn	USER NAME = krk	DESIGNED - SJC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL ROUTE 83 (BUSSE ROAD) AND PRATT BOULEVARD LIGHTING PLAN	F.A.P. RTE. 334	SECTION 09-00057-00-CH	COUNTY COOK	TOTAL SHEETS 103	SHEET NO. 66		
PLOT SCALE = 20,0000 "/ IN.	DRAWN - SJC	CHECKED - DNM	REVISED -			SCALE: 1" = 20'	SHEET NO. 2 OF 2 SHEETS	STA. 410+80.00 TO STA. 415+68.82	CONTRACT NO. 63523			
PLOT DATE = 11/3/2010	DATE - 10-22-10	REVISED -	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003660						

PLAN	REVISIONS	DATE
NOTE BOOK NO.	PLOTTED	
	ALIGNMENT CHECKED	
	FIELD FILE NAME	

PROFILE	REVISIONS	DATE
NOTE BOOK NO.	GRADES CHECKED	
	PLANTING NOTATIONS CHKD	



CIRCUIT NO.	TOTAL FIXTURE AMPS	KW LOAD
A	13.0	3.1
B	11.7	2.8
C	13.65	2.8
D	11.05	2.7
E	13.65	3.3
F	13.65	3.3
G	14.95	3.6
H	16.25	3.9
I	3.90	0.9
J	3.90	0.9
K	3.90	0.9
L	1.95	0.9
M	3.90	0.9
N	1.95	0.5
O	3.90	0.9
P	3.90	0.9
TOTAL	139.1	33.2

CIRCUIT NO.	TOTAL FIXTURE AMPS	KW LOAD
A	13.00	3.1
B	13.00	3.1
C	9.75	2.3
D	9.75	2.3
E	14.30	3.4
F	14.30	3.4
G	9.75	2.3
H	9.75	2.3
I	3.25	0.8
J	3.25	0.8
K	2.60	0.6
L	3.25	0.8
M	3.25	0.8
N	2.60	0.6
O	1.30	0.3
P	3.25	0.8
TOTAL	116.4	27.7

400W LUMINAIRE = 1.95A
250W LUMINAIRE = 1.30A

400W LUMINAIRE = 1.95A
250W LUMINAIRE = 1.30A

- LEGEND**
- LITHONIA LUMINAIRE, SODIUM VAPOR 400W, 240V ON RED WIRE
 - LITHONIA LUMINAIRE, SODIUM VAPOR 400W, 240V ON BLACK WIRE
 - (S)○ LITHONIA LUMINAIRE, SODIUM VAPOR 250W, 240V ON RED WIRE
 - KIM LUMINAIRE, SODIUM VAPOR 250W, 240V ON RED WIRE
 - KIM LUMINAIRE, SODIUM VAPOR 250W, 240V ON BLACK WIRE
 - ⊠ PROPOSED LIGHTING CONTROLLER 240/480V, 1 φ, 3-WIRE
 - UNIT DUCT, WITH 3-1/8" NO. 4 AND 1-1/8" NO. 4 GROUND, 600V (XLP-TYPE USE) 1 1/4" DIA. POLYETHYLENE
 - - - UNIT DUCT, WITH 3-1/8" NO. 8 AND 1-1/8" NO. 8 GROUND, 600V (XLP-TYPE USE) 1" DIA. POLYETHYLENE

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DRAWN - SJC
CHECKED - DNM
DATE - 10-22-10

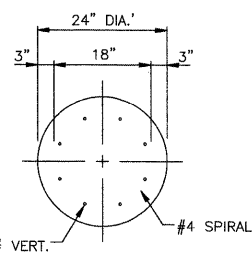
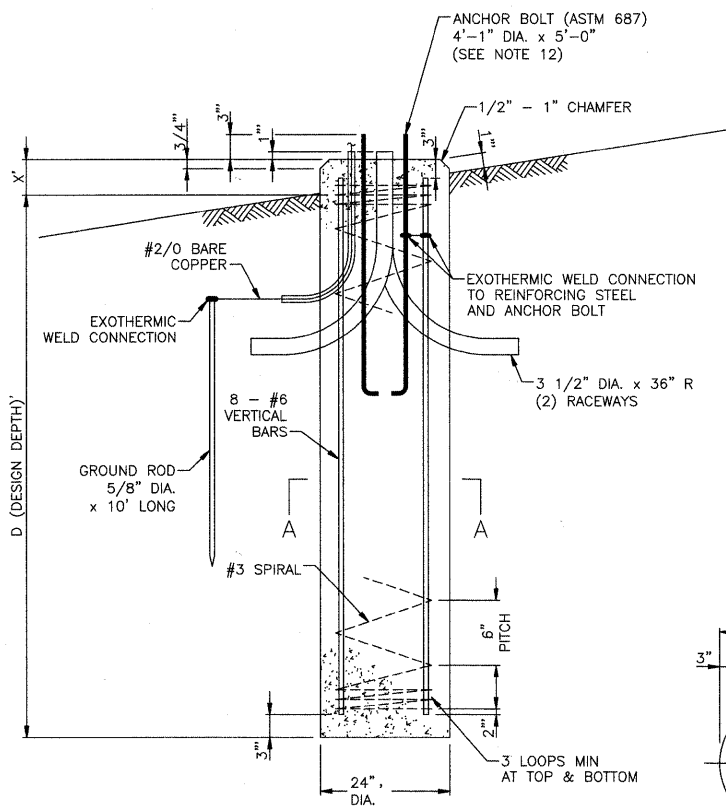
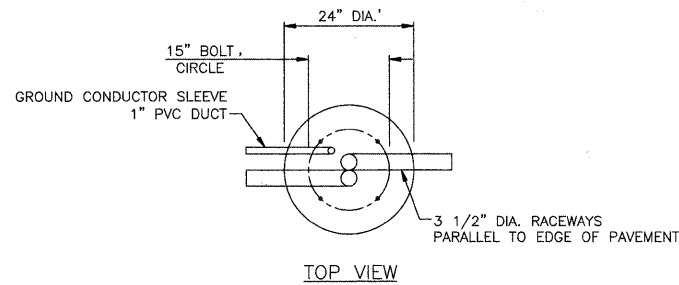
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

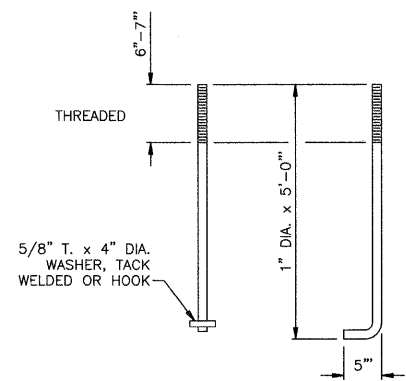
**IL ROUTE 83 (BUSSE ROAD) AND PRATT BOULEVARD
EXISTING CABLE PLAN**

SCALE: 1" = 40' SHEET NO. 1 OF 1 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	09-00057-00-CH	COOK	103	67
CONTRACT NO. 63523				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(660)				



SECTION A-A



ANCHOR BOLT DETAIL

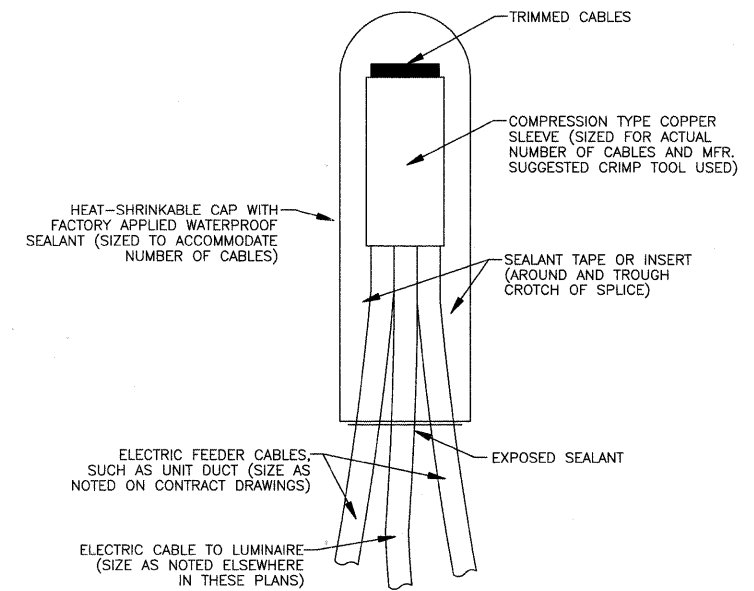
NOTES:

1. THE ENGINEER SHALL DETERMINE THE CLASS OF SOIL DURING EXCAVATION AND SELECT THE DESIGN DEPTH OF FOUNDATION FROM THE DESIGN TABLE. THE CONTRACTOR SHALL NOT ORDER REINFORCEMENT BARS UNTIL THE OFFSET AND DIMENSION D ARE DETERMINED.
2. EXCAVATION OF THE POLE FOUNDATION SHALL BE MADE WITH AN AUGER, 24" OR 30" IN DIAMETER.
3. THE CONTRACTOR SHALL USE #3 SPIRAL AT 6" PITCH OR AT HIS OPTION MAY SUBSTITUTE #3 TIES AT 12" CENTER.
4. THE ANCHOR SHALL BE A TACK WELDED TYPE BOLT OR HOOK TYPE BOLT. COLD BENDING OF THE HOOK BOLT WILL NOT BE ALLOWED.
5. THE ANCHOR BOLTS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED IN THE FORM.
6. THE ENTIRE LENGTH OF THE ANCHOR BOLTS AS WELL AS THE NUTS AND WASHERS SHALL BE HOT DIP GALVANIZED IN ACCORDANCE WITH THE REQUIREMENTS OF ASTM DESIGNATION A 153.
7. CONCRETE SHALL BE CLASS "DS". CONCRETE FOUNDATION MUST BE CURED FOR (10) TEN DAYS BEFORE THE LIGHT STANDARD IS ERECTED.
8. THE CABLE TRENCH SHALL BE BACKFILLED AND FIRMLY COMPACTED BEFORE THE LIGHT IS ERECTED.
9. ANCHOR BOLTS SHALL PROJECT 3" ABOVE THE TOP OF THE FOUNDATION.
10. RACEWAYS SHALL PROJECT 1" ABOVE THE TOP OF THE FOUNDATION.
11. THE CONTRACTOR SHALL COORDINATE THE EXTENSION OF ANCHOR BOLTS ABOVE TOP OF FOUNDATION WITH THE BREAKAWAY DEVICE MANUFACTURER'S REQUIREMENTS.
12. A MINIMUM OF 3" OF THE THREADING ON THE ANCHOR BOLTS SHALL REMAIN BELOW THE TOP OF THE FOUNDATION.

DESIGN TABLE - LIGHT POLE FOUNDATION, 24" DIAMETER

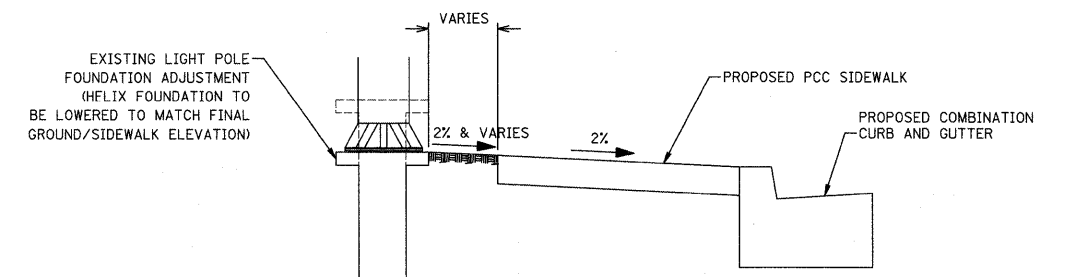
TYPE OF SOIL	DESIGN DEPTH OF FOUNDATION		REINFORCEMENT IN FOUNDATION			
	SINGLE ARM D	TWIN ARM D	SINGLE ARM		TWIN ARM	
			VERT. BARS	SPIRAL	VERT. BARS	SPIRAL
SOFT CLAY	13'-0"	15'-0"	8-#6 x 12'-6"	#3 x 122'	8-#6 x 14'-3"	#3 x 141'
MEDIUM CLAY	9'-6"	10'-9"	8-#6 x 9'-0"	#3 x 90'	8-#6 x 10'-0"	#3 x 100'
STIFF CLAY	7'-0"	8'-0"	8-#6 x 6'-6"	#3 x 66'	8-#6 x 7'-6"	#3 x 76'
LOOSE SAND	10'-0"	11'-0"	8-#6 x 9'-6"	#3 x 94'	8-#6 x 10'-6"	#3 x 103'
MEDIUM SAND	8'-3"	9'-0"	8-#6 x 8'-0"	#3 x 78'	8-#6 x 8'-6"	#3 x 85'
DENSE SAND	7'-9"	9'-0"	8-#6 x 7'-6"	#3 x 73'	8-#6 x 8'-6"	#3 x 85'
ROCK OR SOLIDIFIED SLAG	5'-0"	5'-0"	NONE	NONE	NONE	NONE

LIGHT POLE FOUNDATION, 24" DIAMETER

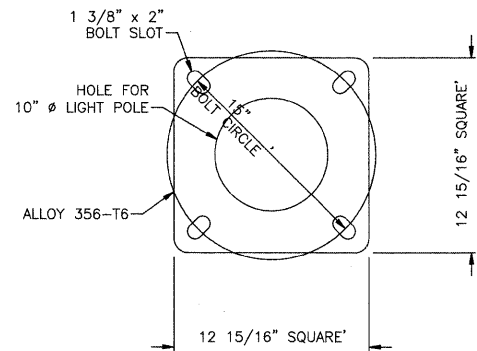
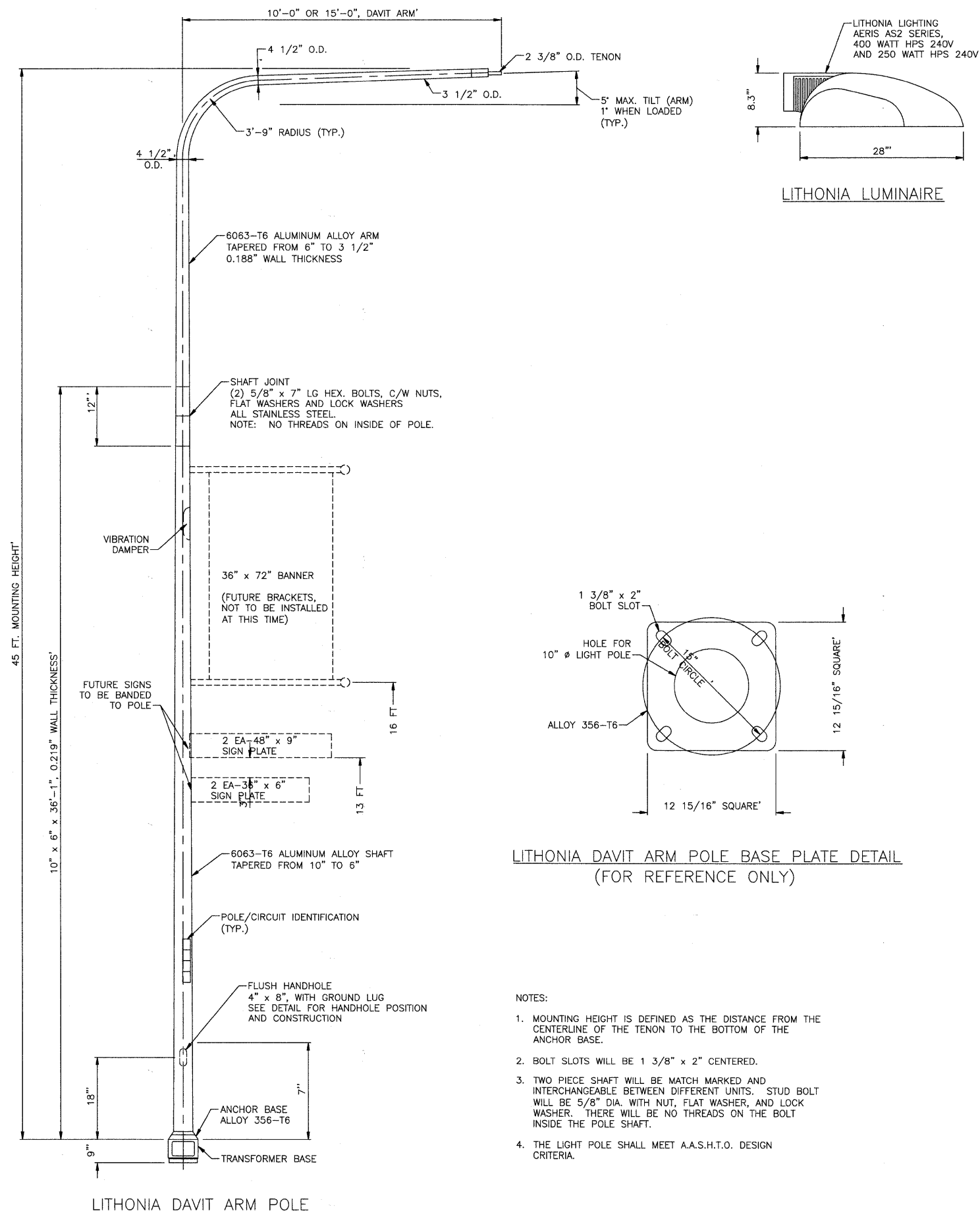


NOTE: NUMBER OF CABLES IN SPLICE MAY VARY

SPLICING ELECTRIC CABLES
BASIC MATERIALS AND METHODS



EXISTING LIGHT POLE FOUNDATION
ADJUSTMENT

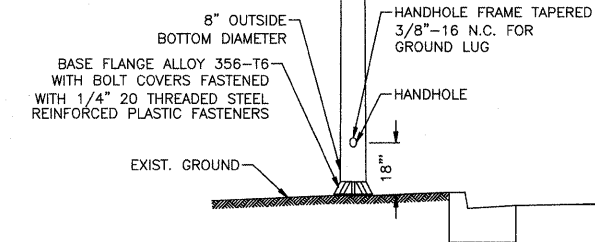


- NOTES:
1. MOUNTING HEIGHT IS DEFINED AS THE DISTANCE FROM THE CENTERLINE OF THE TENON TO THE BOTTOM OF THE ANCHOR BASE.
 2. BOLT SLOTS WILL BE 1 3/8" x 2" CENTERED.
 3. TWO PIECE SHAFT WILL BE MATCH MARKED AND INTERCHANGEABLE BETWEEN DIFFERENT UNITS. STUD BOLT WILL BE 5/8" DIA. WITH NUT, FLAT WASHER, AND LOCK WASHER. THERE WILL BE NO THREADS ON THE BOLT INSIDE THE POLE SHAFT.
 4. THE LIGHT POLE SHALL MEET A.A.S.H.T.O. DESIGN CRITERIA.

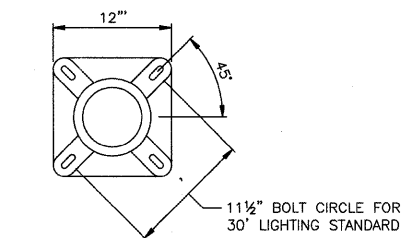
30 FT. LIGHT STANDARD

LUMINAIRE:
KIM LIGHTING - AR3 MODEL
250 WATT, H.P.S., M-C-II, 240V,
30 FT. M.H., 8 IN. SUPPORT ARM,
DARK BRONZE-POWDER COAT FINISH

POLE:
ROUND TAPERED ALUMINUM,
KIM-LIGHTING-LTRA30-8188
DARK BRONZE-POWDER COAT FINISH,
11-1/2" BOLT CIRCLE,
OR APPROVED EQUAL



KIM LIGHTING UNIT DETAIL
(FOR REFERENCE ONLY)



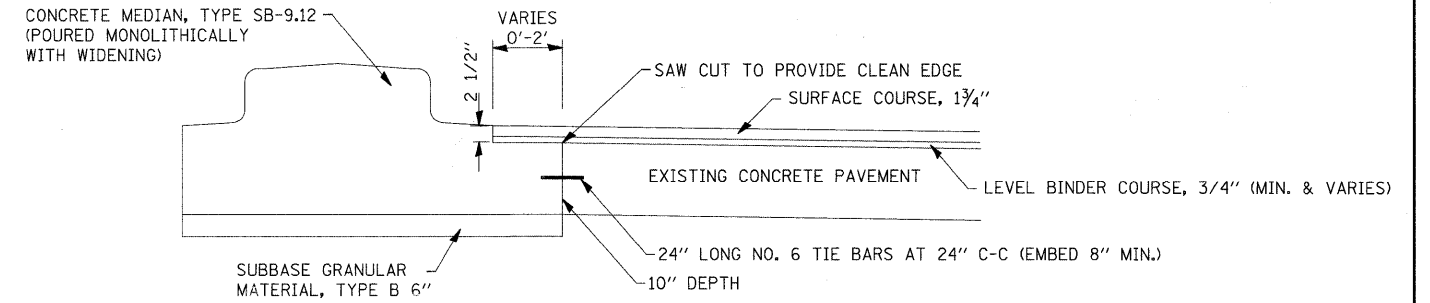
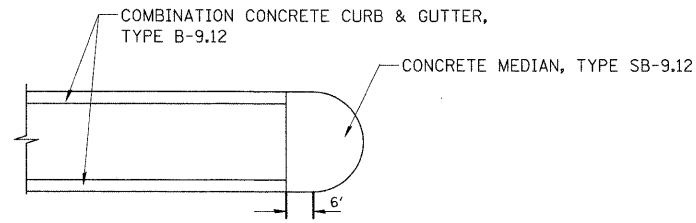
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ANCHOR BASE DETAIL
(FOR REFERENCE ONLY)

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		DRAWN - SJC	REVISED -			344	09-00057-00-CH	COOK	103	69	
		CHECKED - DNM	REVISED -			CONTRACT NO. 63523					
		DATE - 10-22-10	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003660					

CONCRETE MEDIAN, TYPE SB-9.12

AT MEDIAN NOSES

NOTE: CONCRETE MEDIAN, TYPE SB-9.12 SHALL BE CONSTRUCTED IN ACCORDANCE WITH APPLICABLE PORTIONS OF STANDARD 606301



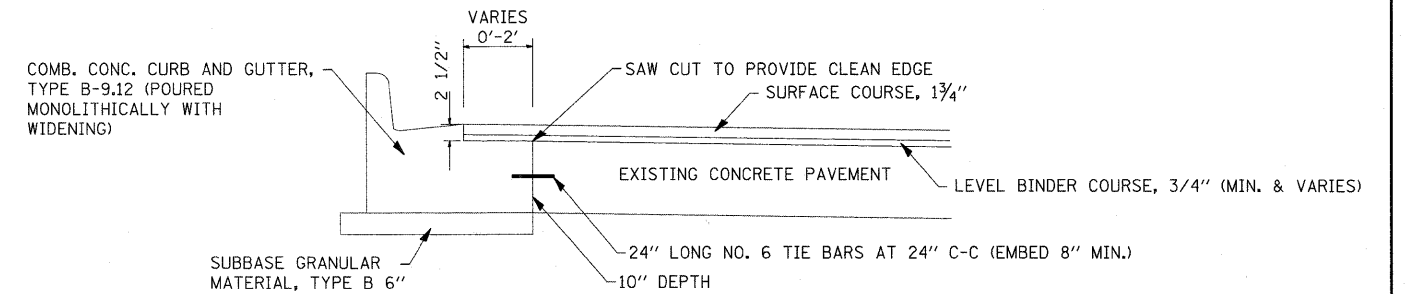
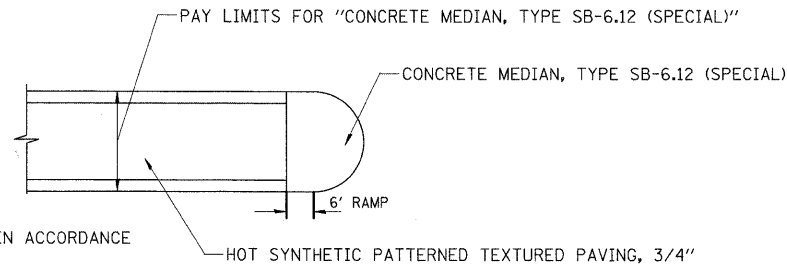
CONCRETE MEDIAN, TYPE SB-9.12

AT LOCATIONS WHERE WIDENING IS 2' OR LESS

CONCRETE MEDIAN, TYPE SB-6.12 (SPECIAL)

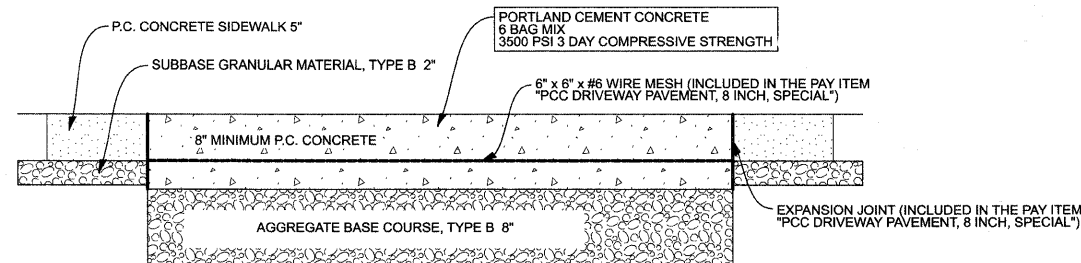
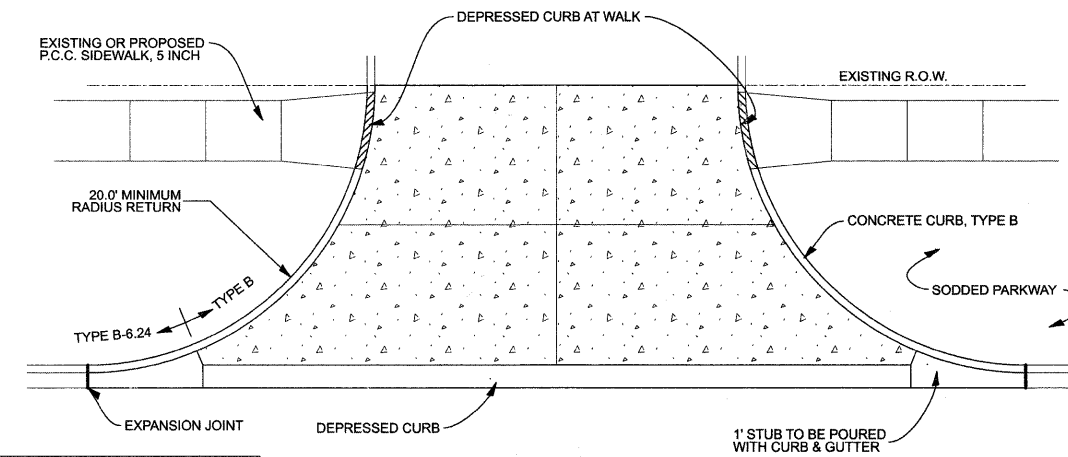
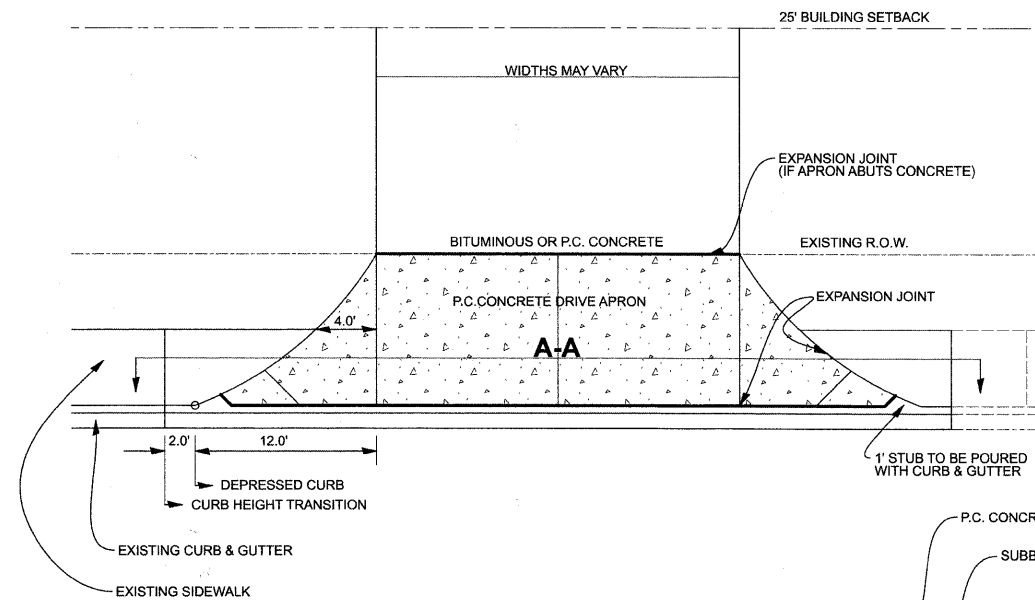
AT MEDIAN NOSES

NOTE: CONCRETE MEDIAN, TYPE SB-6.12 SHALL BE CONSTRUCTED IN ACCORDANCE WITH APPLICABLE PORTIONS OF STANDARD 606301



COMBINATION CONCRETE CURB AND GUTTER, TYPE B-9.12

AT LOCATIONS WHERE WIDENING IS 2' OR LESS



SECTION A-A

DRIVEWAY DETAIL

NOT TO SCALE

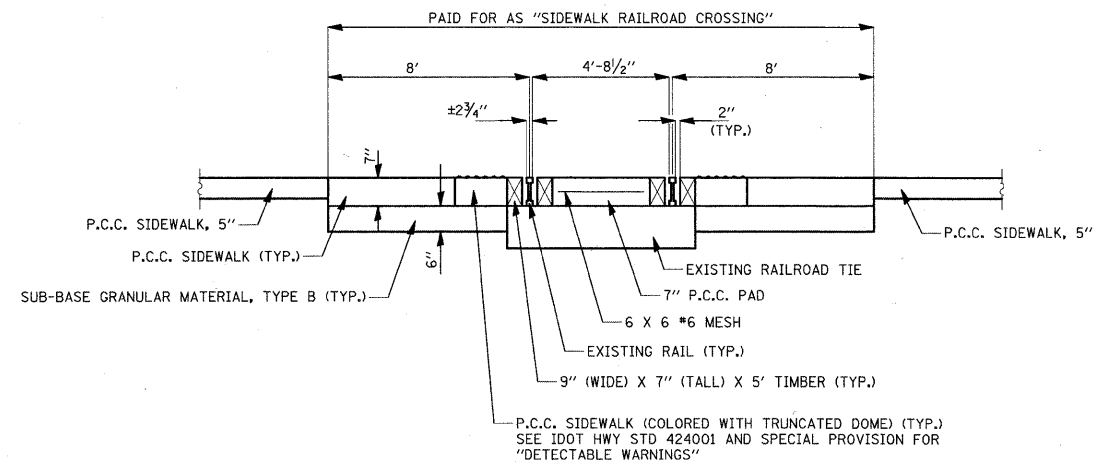
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY DETAILS

SHEET NO. 1 OF 4 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	09-00057-00-CH	COOK	103	70
CONTRACT NO. 63523				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(660)				



NOTES:

1. ALL SURFACES SHALL BE FLUSH WITH EXISTING RAILS.
2. THE PAY ITEM "SIDEWALK RAILROAD CROSSING" SHALL INCLUDE THE CONSTRUCTION OF THE 7" P.C.C. PAD BETWEEN THE RAILS AND THE CONSTRUCTION OF THE 7" P.C.C. SIDEWALK, DETECTABLE WARNINGS, AND 6" SUB-BASE GRANULAR MATERIAL AS SHOWN ABOVE.
3. ANY BALLAST REMOVAL REQUIRED TO CONSTRUCT THE RAILROAD CROSSING SHALL BE INCLUDED IN THE COST OF THE PAY ITEM "SIDEWALK RAILROAD CROSSING".
4. ALL MATERIAL SHALL BE APPROVED BY THE ENGINEER PRIOR TO THEIR INSTALLATION.
5. THE CONTRACTOR SHALL COORDINATE ALL CONSTRUCTION ACTIVITIES RELATED TO THE CONSTRUCTION OF THE RAILROAD CROSSING WITH THE RAILROAD COMPANY.

SIDEWALK RAILROAD CROSSING DETAIL

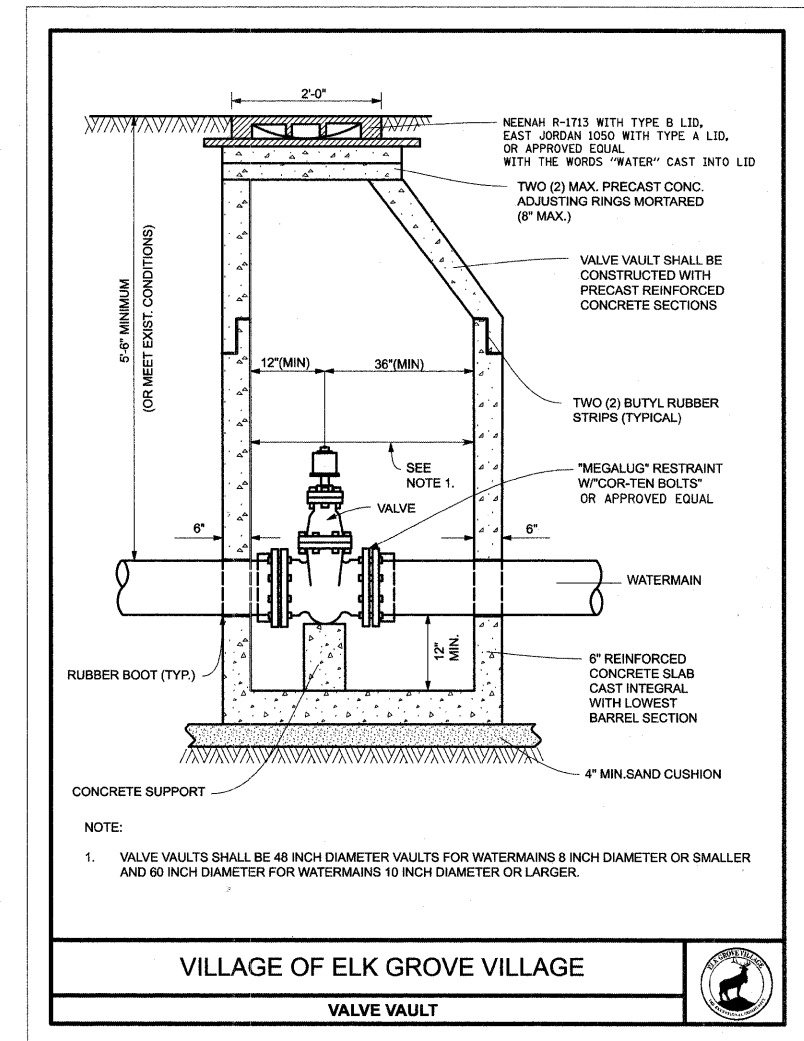
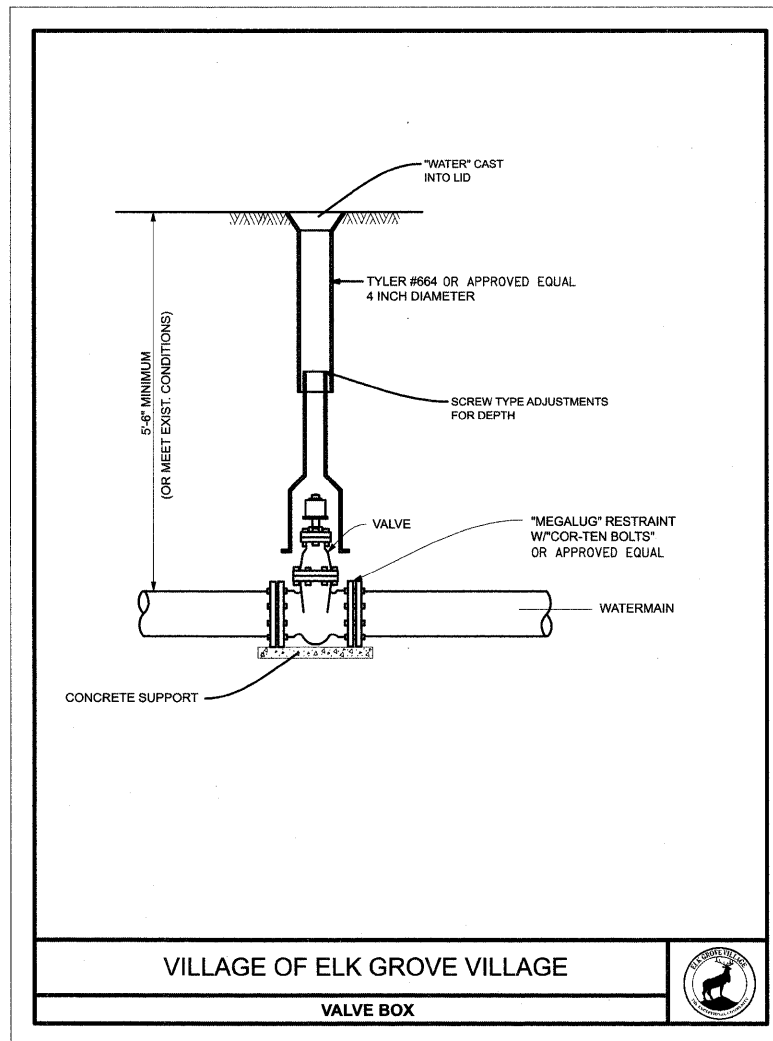
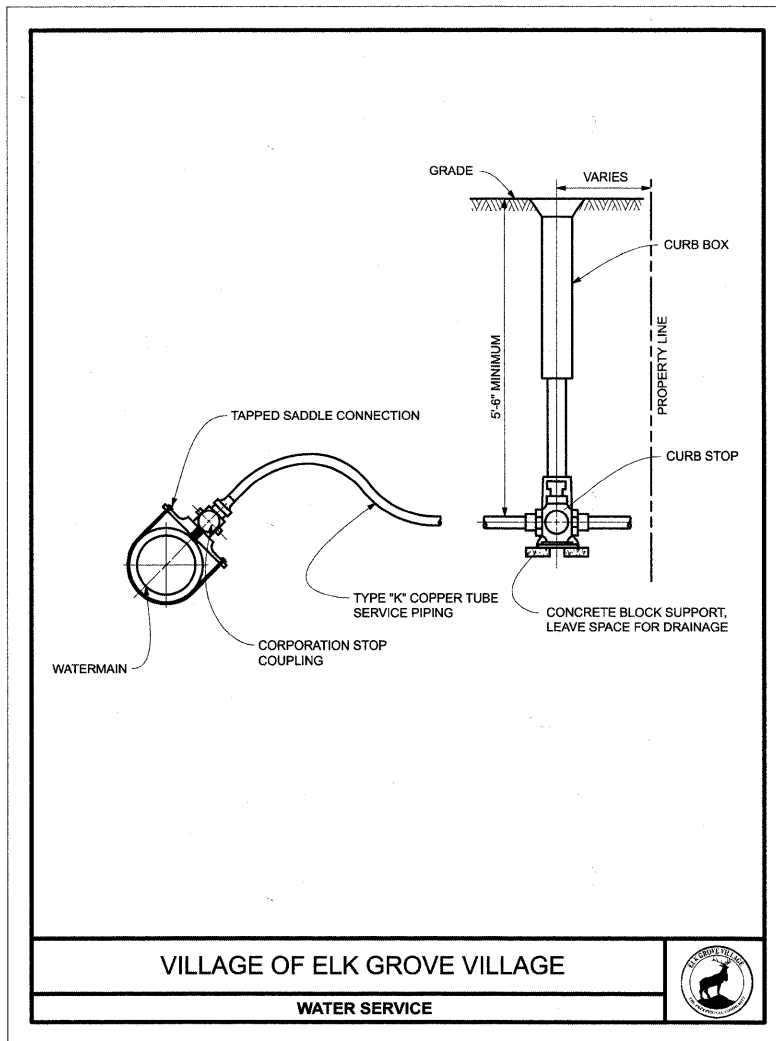
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY DETAILS

SHEET NO. 2 OF 3 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 63523				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(660)				



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DATE - 10-22-10

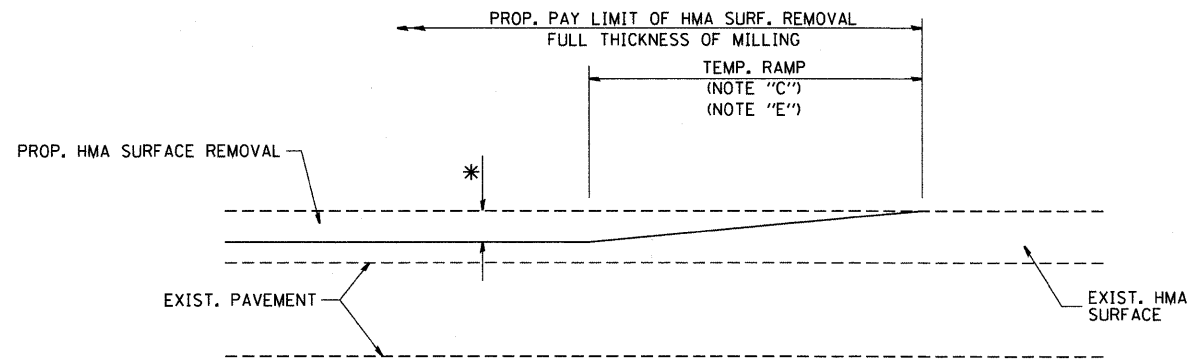
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY DETAILS

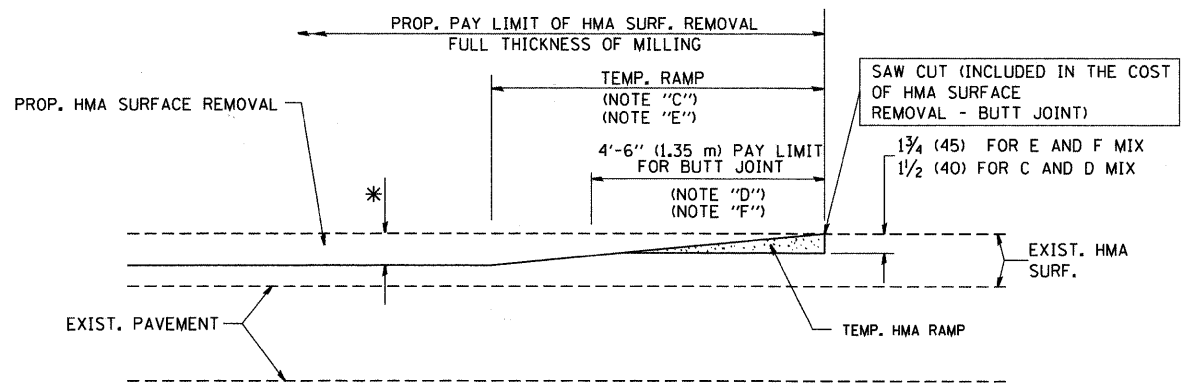
SHEET NO. 3 OF 3 SHEETS

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334	09-00057-00-CH	COOK	103	72
CONTRACT NO. 63523				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(660)				



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

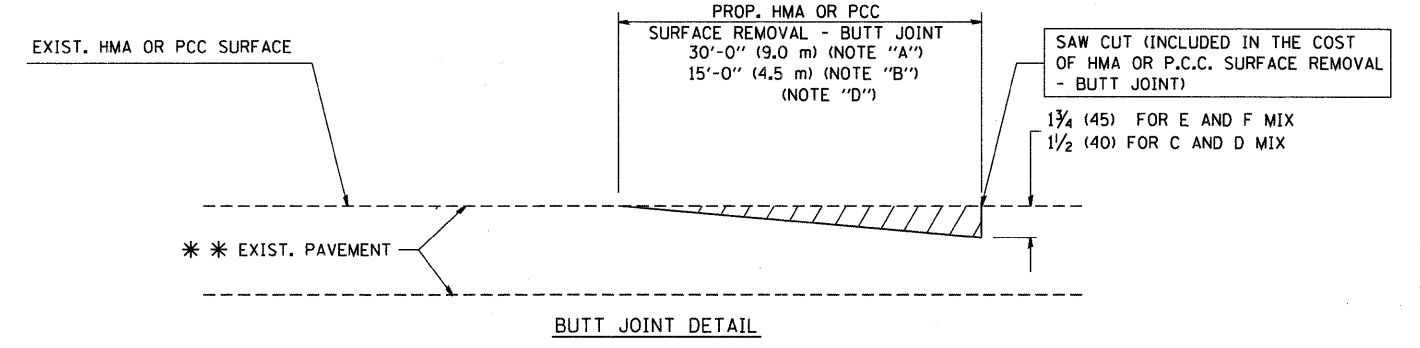
OPTION 1



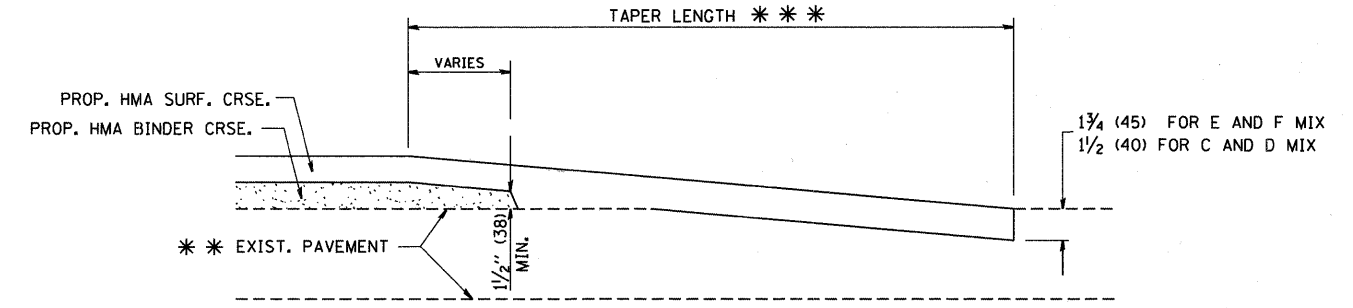
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

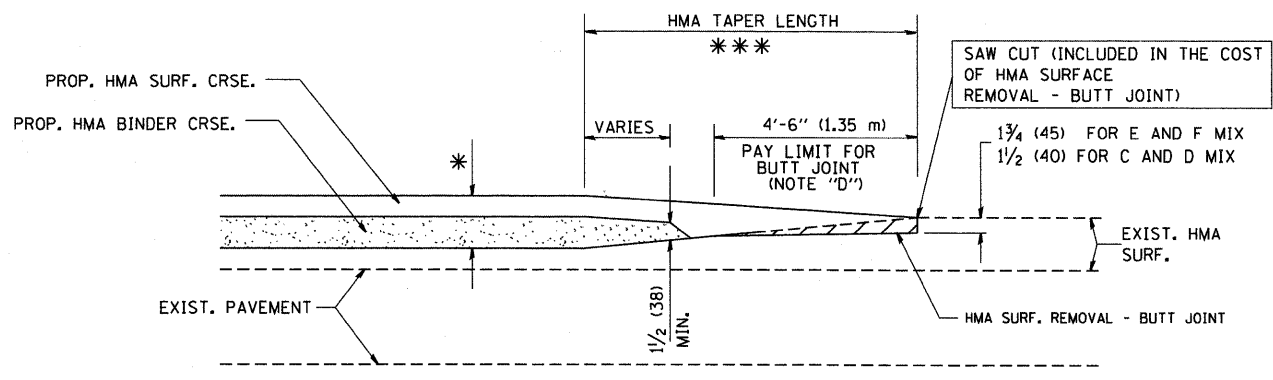
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

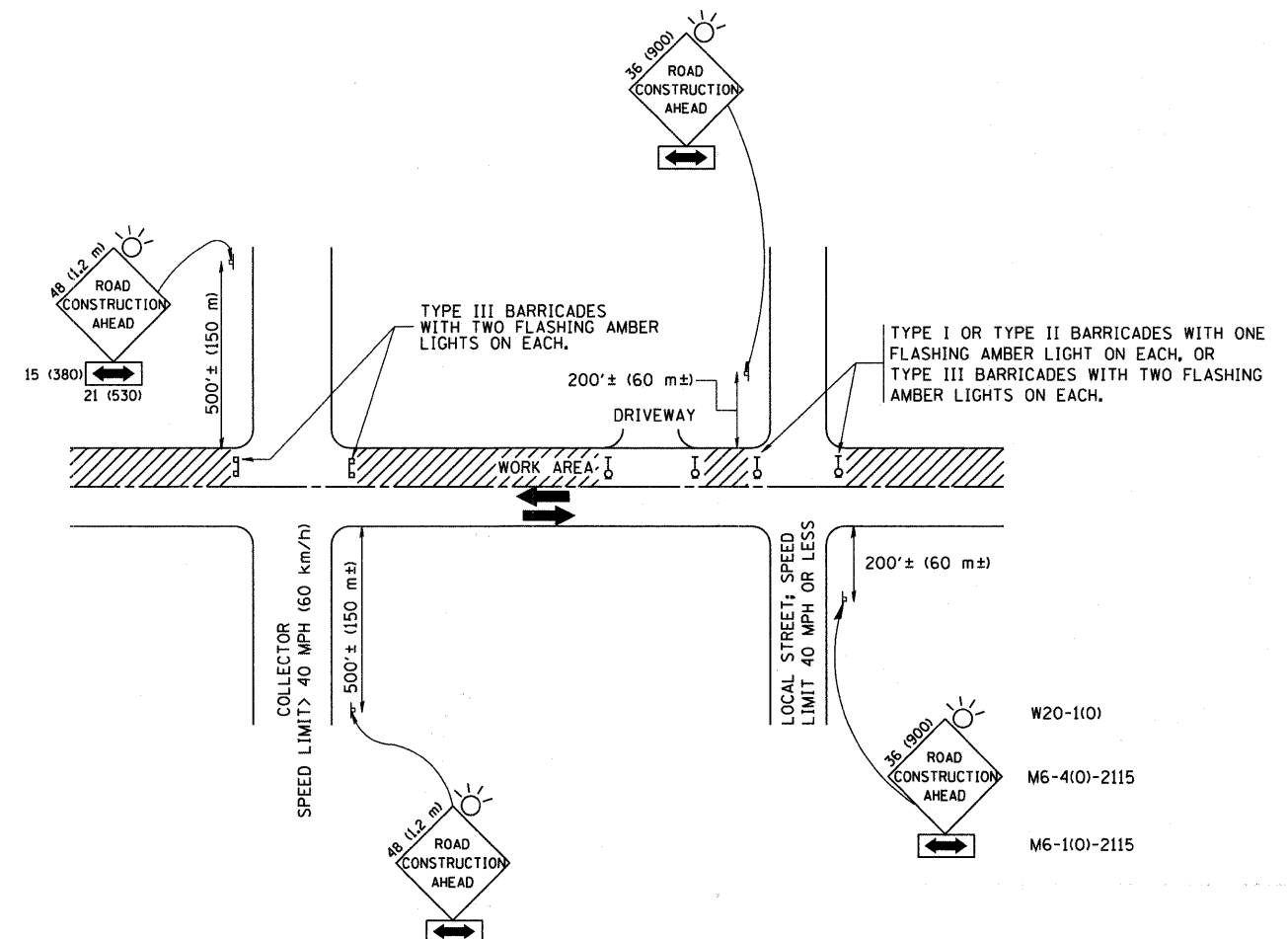
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DRAWN -	REVISED - A. ABBAS 03-21-97
CHECKED -	REVISED - M. GOMEZ 04-06-01
DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT AND HMA TAPER DETAILS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	09-00057-00-CH	COOK	103	73
BD400-05 BD32		CONTRACT NO. 63523		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(660)				



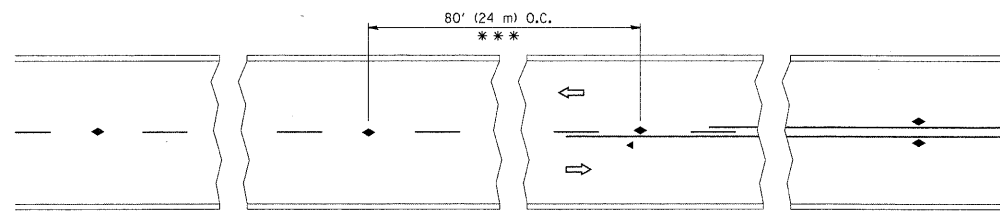
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

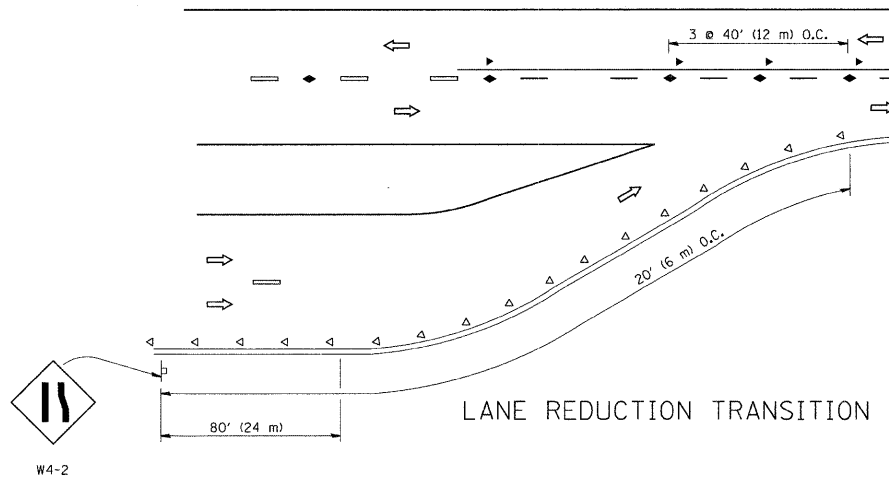
All dimensions are in millimeters (inches) unless otherwise shown.

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	PLOT SCALE = 50,000 / IN.	CHECKED -	REVISED - A. HOUSEH 03-06-96				TC-10		CONTRACT NO. 63523					
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00				SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(660)	

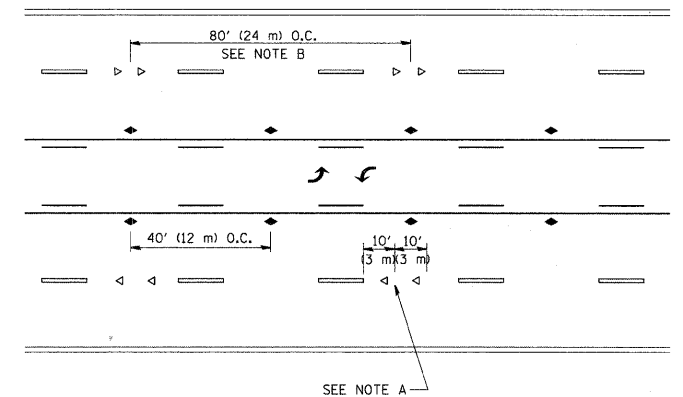


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

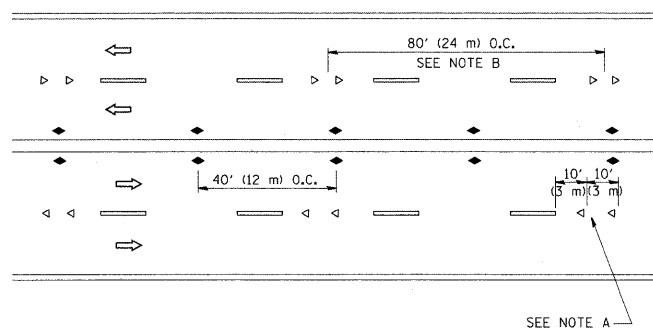
TWO-LANE/TWO-WAY



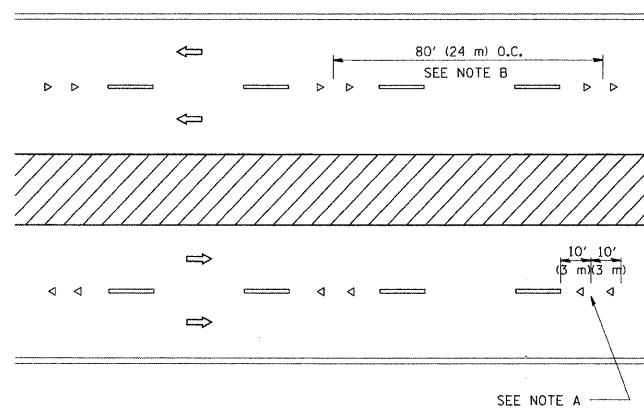
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

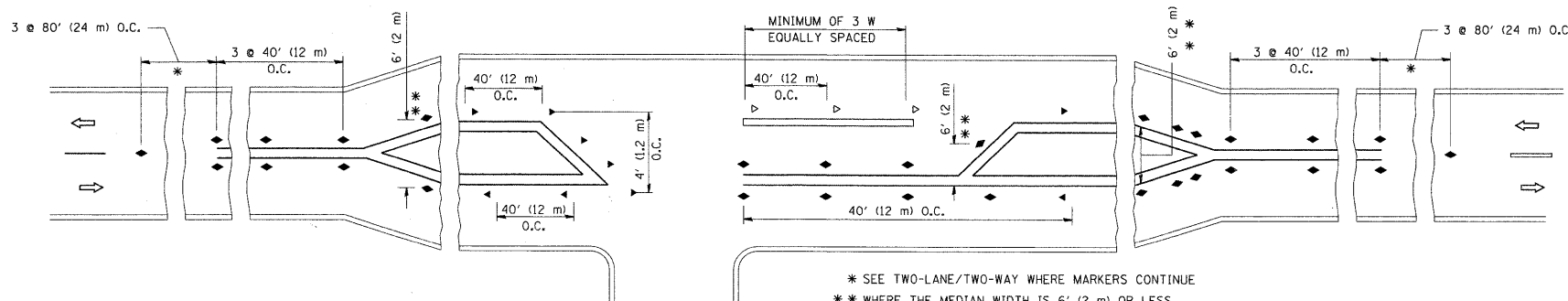
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

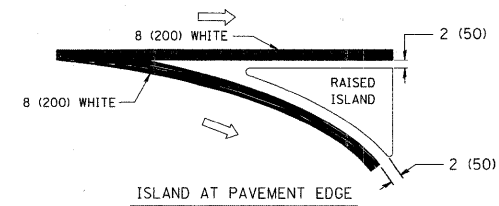
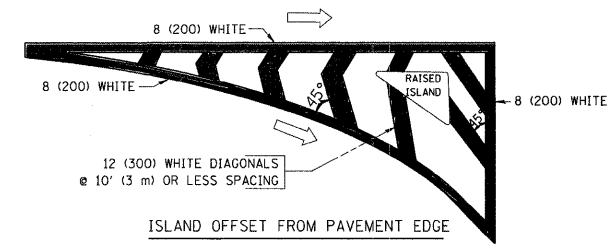
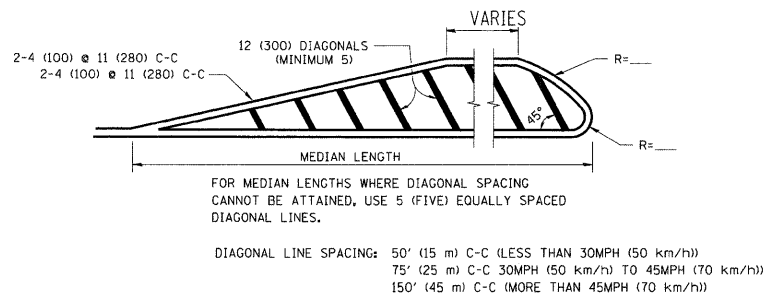
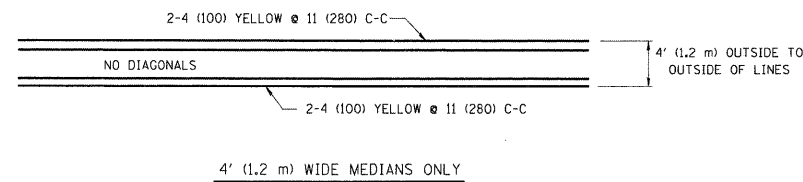
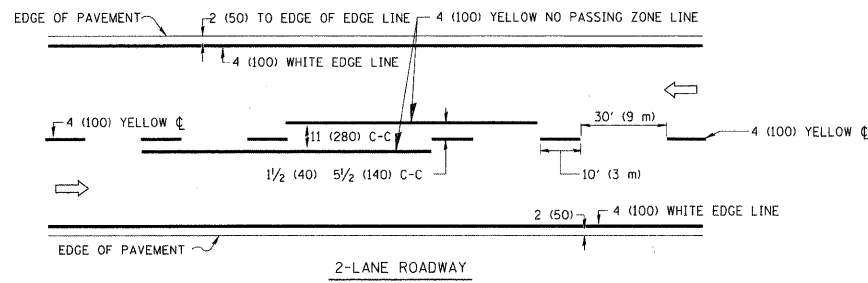


LEFT TURN

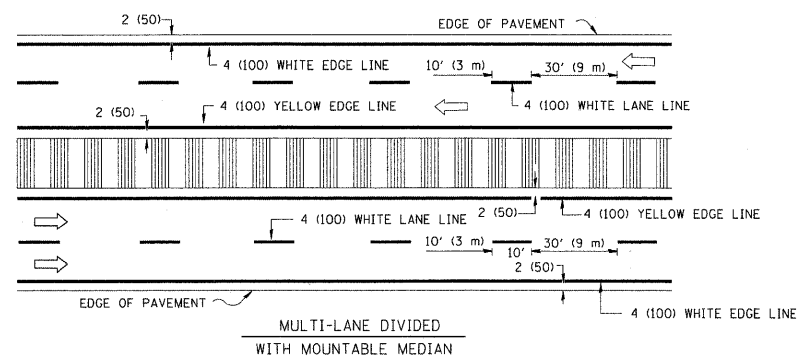
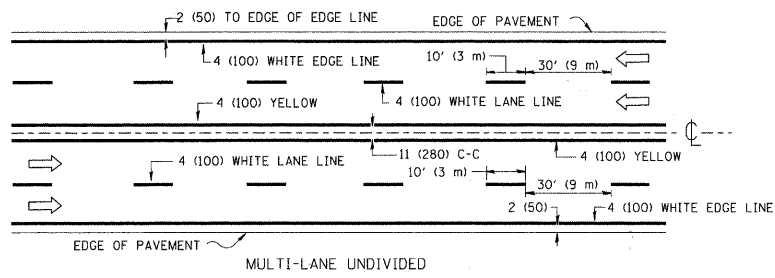
* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = drivakosgn	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pw\work\pwi\dot\drivakosgn\d0108315\td\l.dgn		DRAWN -	REVISED - T. RAMMACHER 03-12-99		344	09-00057-00-CH	COOK	103	75			
PLOT SCALE = 50.000' / IN.		CHECKED -	REVISED - T. RAMMACHER 01-06-00		TC-11			CONTRACT NO. 63523				
PLOT DATE = 9/9/2009		DATE -	REVISED - C. JUCIUS 09-09-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(660)				

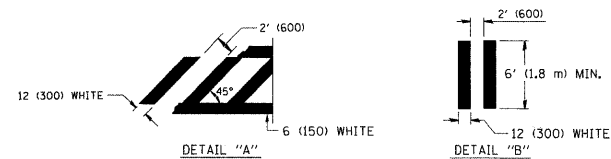
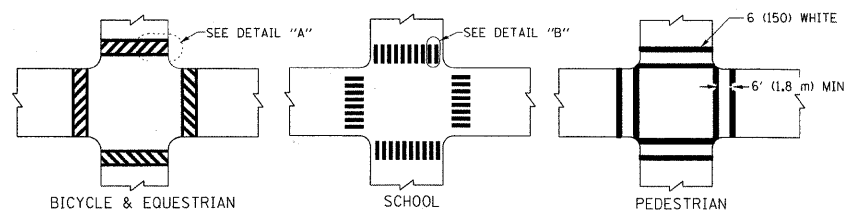


TYPICAL ISLAND MARKING

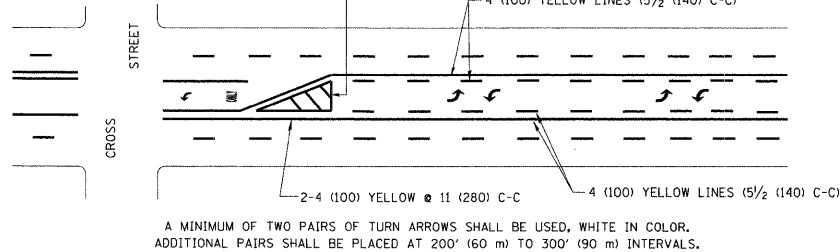


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

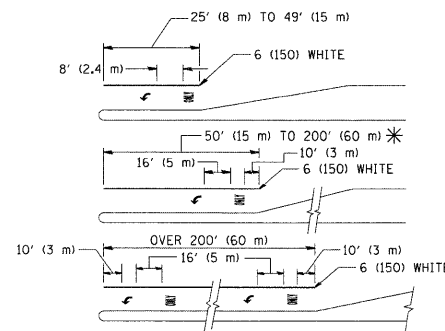


TYPICAL CROSSWALK MARKING



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES: FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN)	2 @ 6 (150)	SOLID	WHITE	NOT LESS THAN 6' (1.8 m) APART
A. DIAGONALS (BIKE & EQUESTRIAN)	12 (300) @ 45°	SOLID	WHITE	2' (600) APART
B. LONGITUDINAL BARS (SCHOOL)	12 (300) @ 90°	SOLID	WHITE	2' (600) APART
				SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE
	NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS			SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

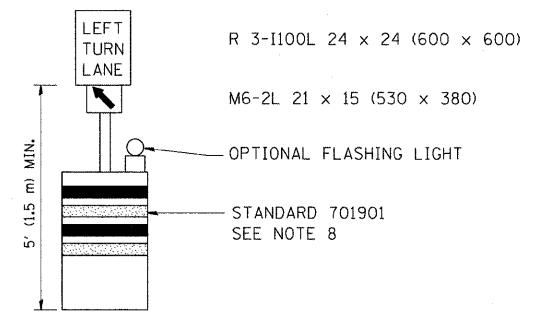
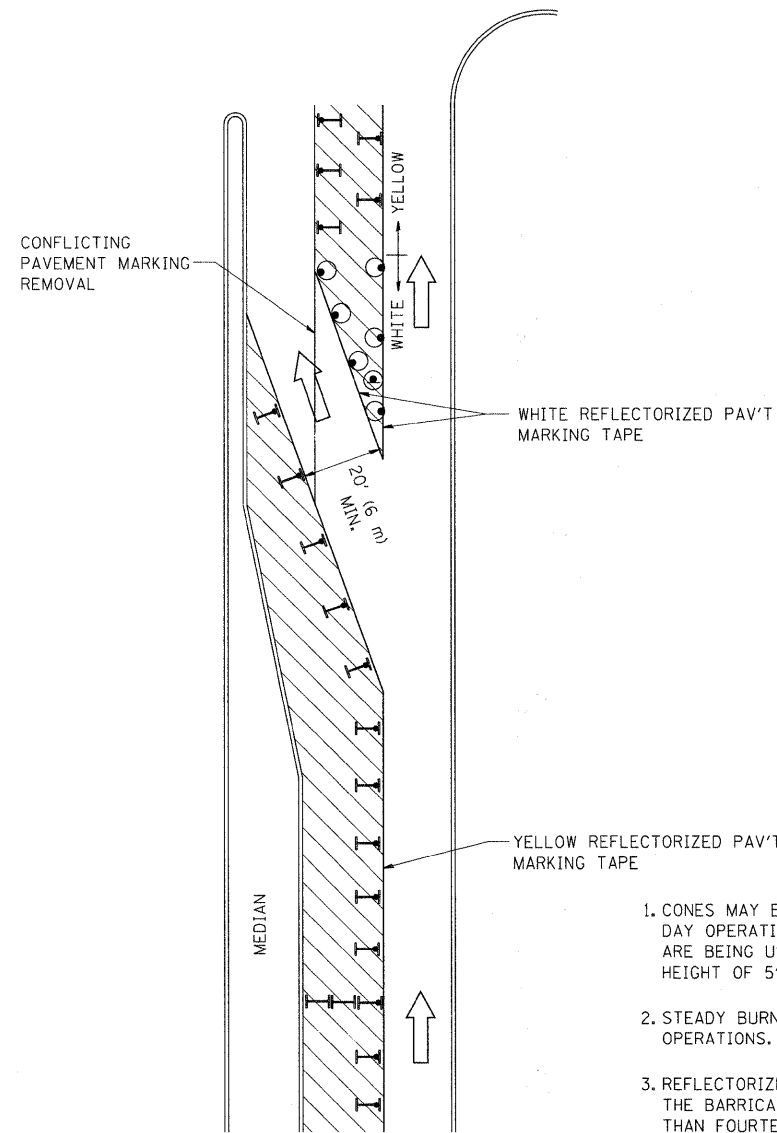
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = drivakosgn	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
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	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TYPICAL PAVEMENT MARKINGS		344	09-00057-00-CH	COOK	103	76
SCALE: NONE		TC-13		CONTRACT NO. 63523		
SHEET NO. 1 OF 1 SHEETS		ILLINOIS FED. AID PROJECT M-9003(660)		FED. ROAD DIST. NO. 1		

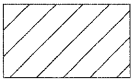







GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

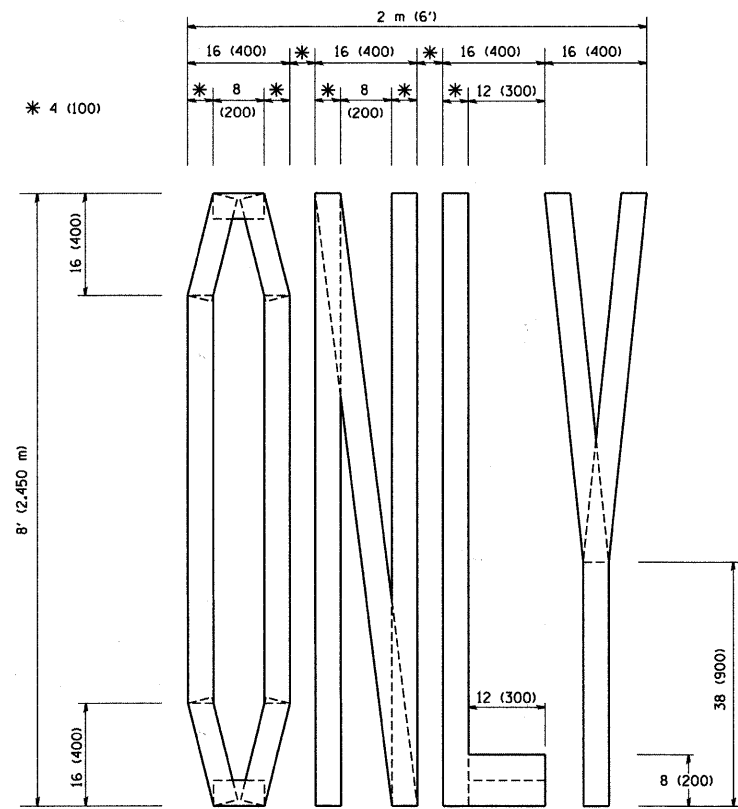
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c:\pwork\p\WIDOT\DRIVAKOSGN\d0108315\14.dgn		REVISED - A. HOUSEH 11-07-95	REVISED -
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

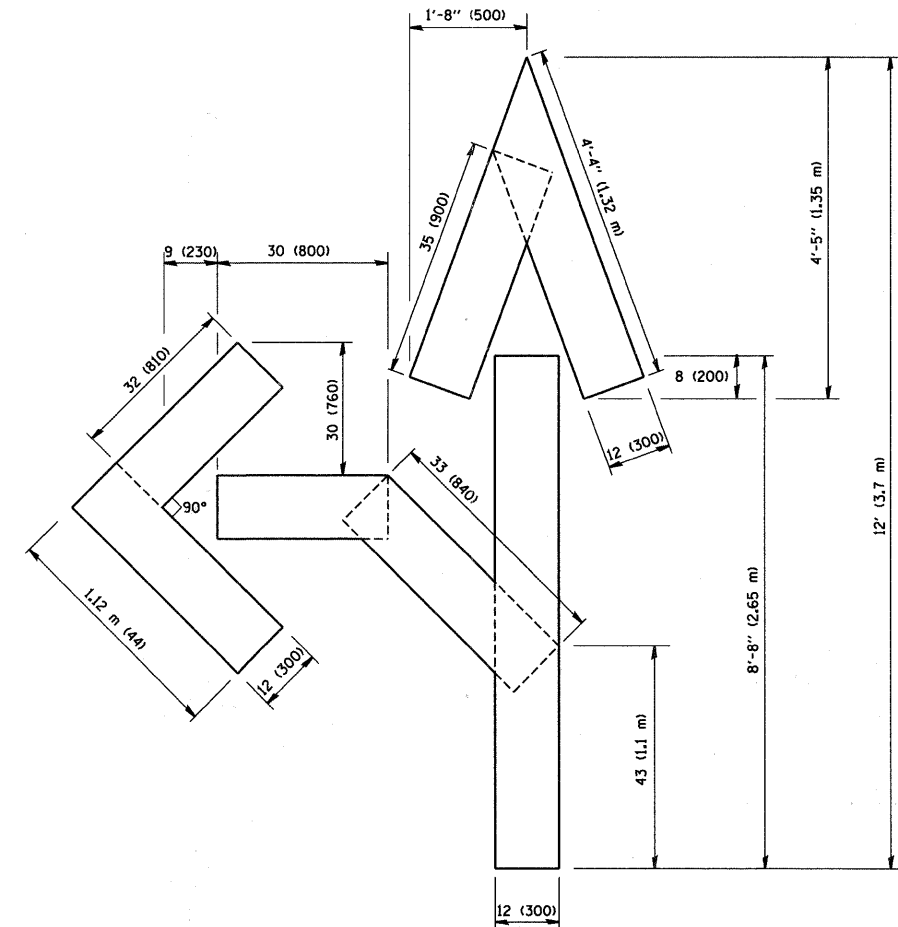
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

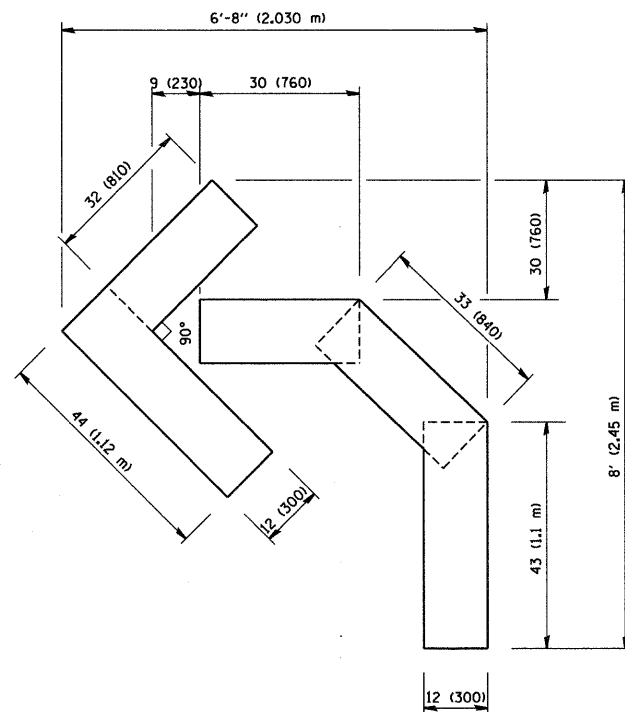
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	09-00057-00-CH	COOK	103	77
TC-14		CONTRACT NO. 63523		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(660)				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



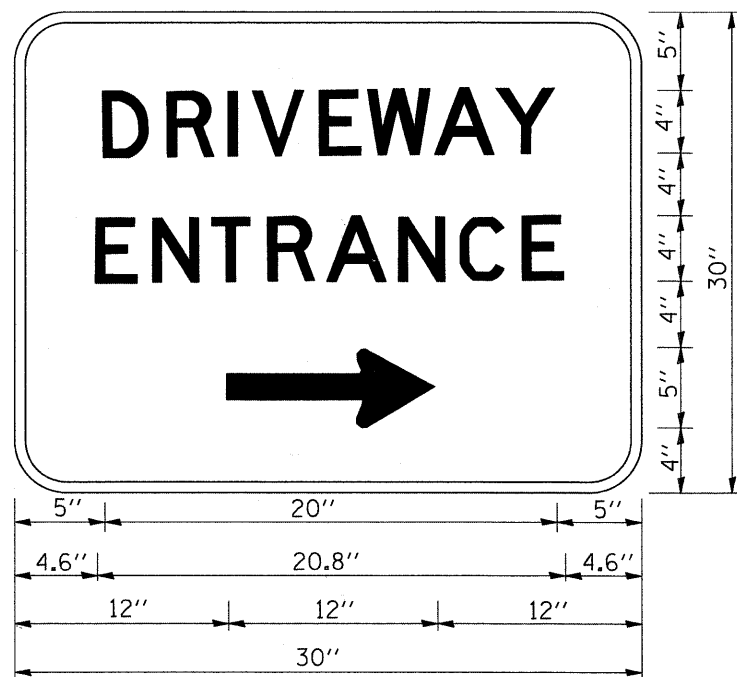
QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\diststd\22x34\tcl6.dgn	USER NAME = gegienobt	DESIGNED -	REVISED -T. RAMMACHER 06-05-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING		F.A.P. RTE. 344	SECTION 09-00057-00-CH	COUNTY COOK	TOTAL SHEETS 103	SHEET NO. 78	
	PLOT SCALE = 50,0000 / IN.	DRAWN -	REVISED -T. RAMMACHER 11-04-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	TC-16			
	PLOT DATE = 1/4/2008	CHECKED -	REVISED -T. RAMMACHER 03-02-98		CONTRACT NO. 63523							
		DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(660)							



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE
 PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN)
 SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY
 AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE
 FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME = W:\diststd\22x34\tc26.dgn	USER NAME = gegljanobt	DESIGNED -	REVISED - C. JUCIUS 02-15-07
		DRAWN -	REVISED -
	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 1/4/2008	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

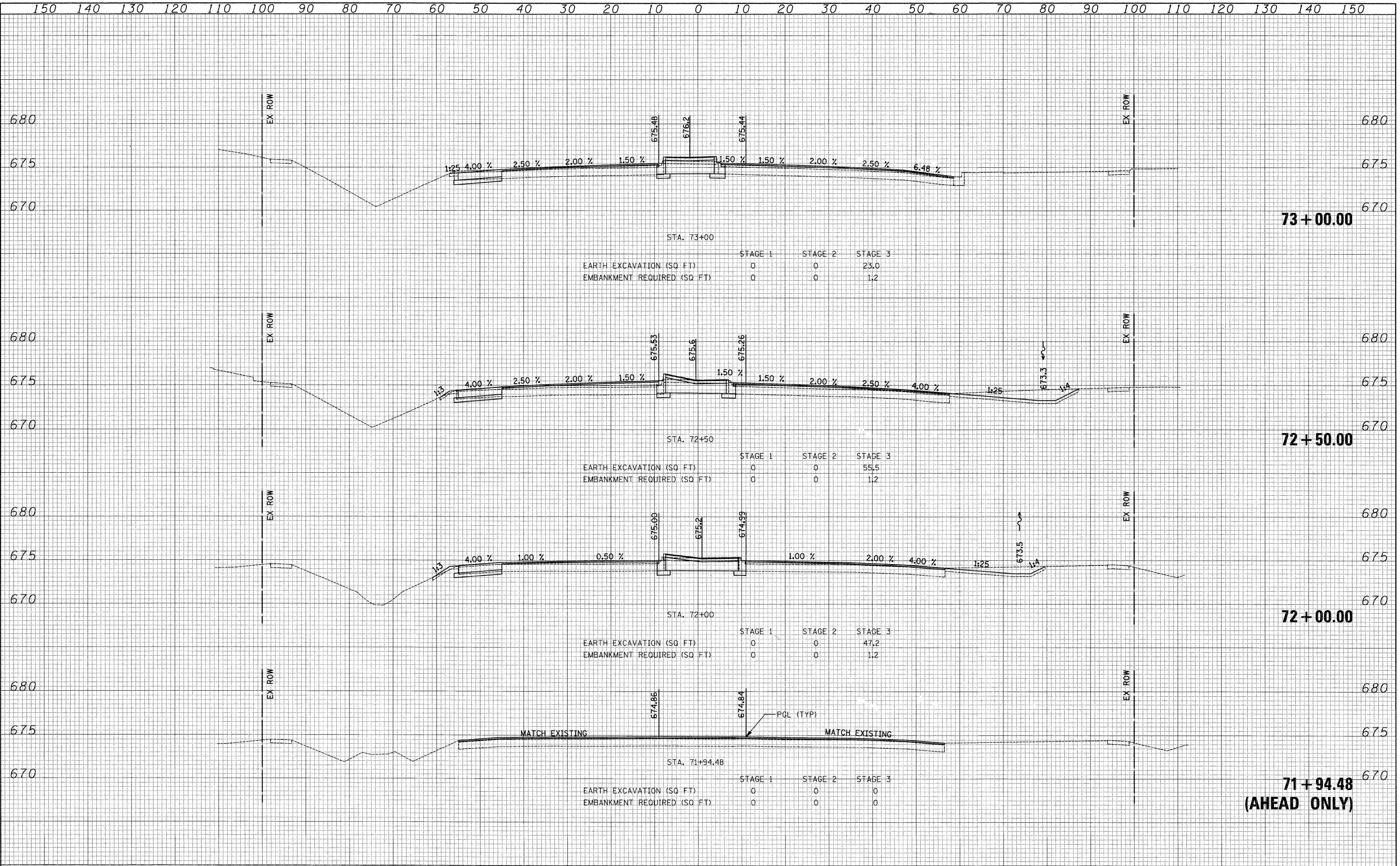
DRIVEWAY ENTRANCE SIGNING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE. 344	SECTION 09-00057-00-CH	COUNTY COOK	TOTAL SHEETS 103	SHEET NO. 80
TC-26			CONTRACT NO. 63523	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(660)				

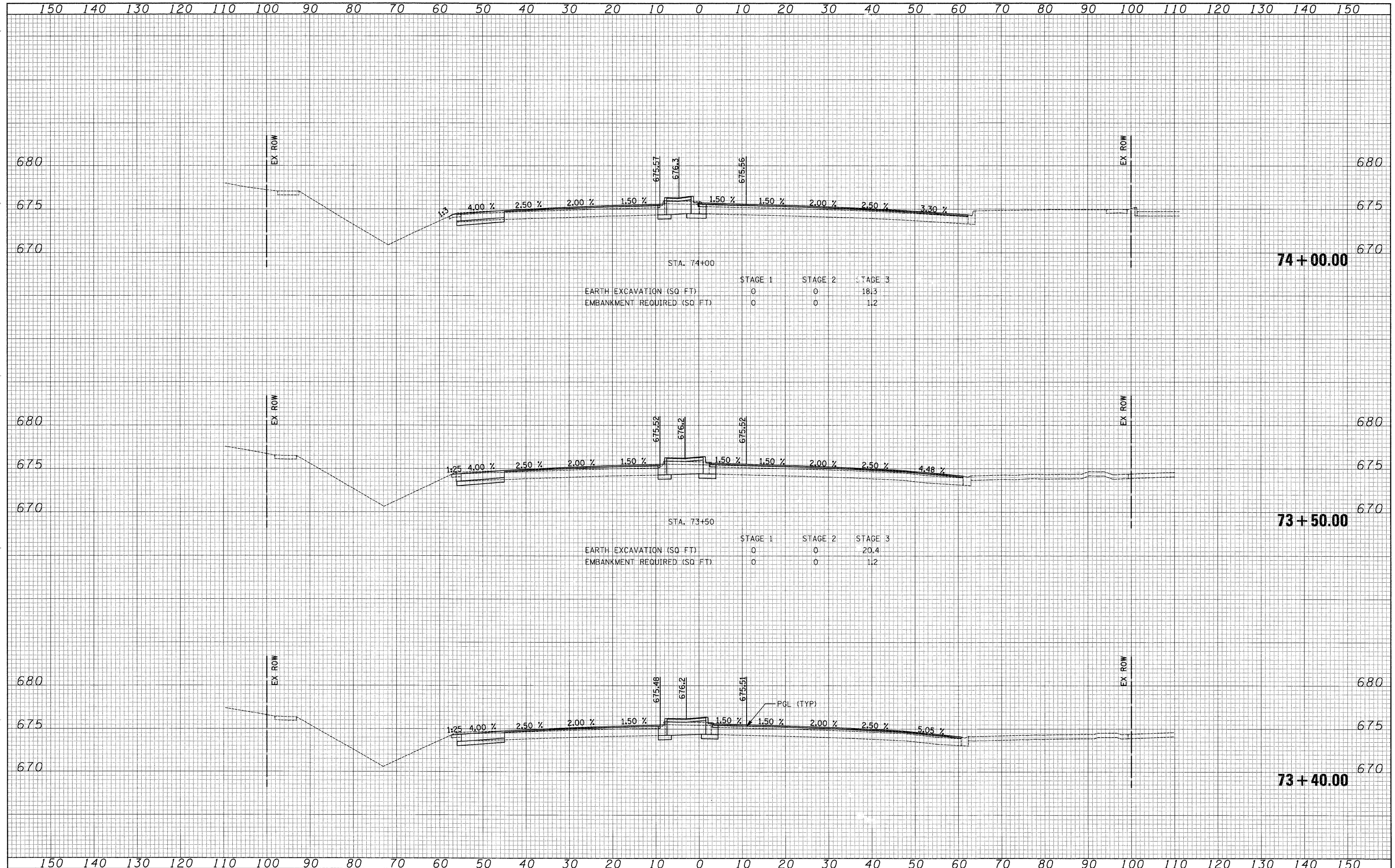
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 BY _____
 SURVEY PLOTTED _____
 TEMPLATE AREAS CHECKED _____
 ORIGINAL SURVEY NOTE BOOK NO. _____

DATE _____
 BY _____
 SURVEY PLOTTED _____
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 ORIGINAL SURVEY NOTE BOOK NO. _____



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SURVEY	
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FINAL SURVEY NOTE BOOK NO.	

DATE	
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PLOTTED	
TEMPLATE	
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ORIGINAL SURVEY NOTE BOOK NO.	



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 PLOT DATE = 10/22/2010

DESIGNED - BLG
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 CHECKED - DJK
 DATE - 10-22-10

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

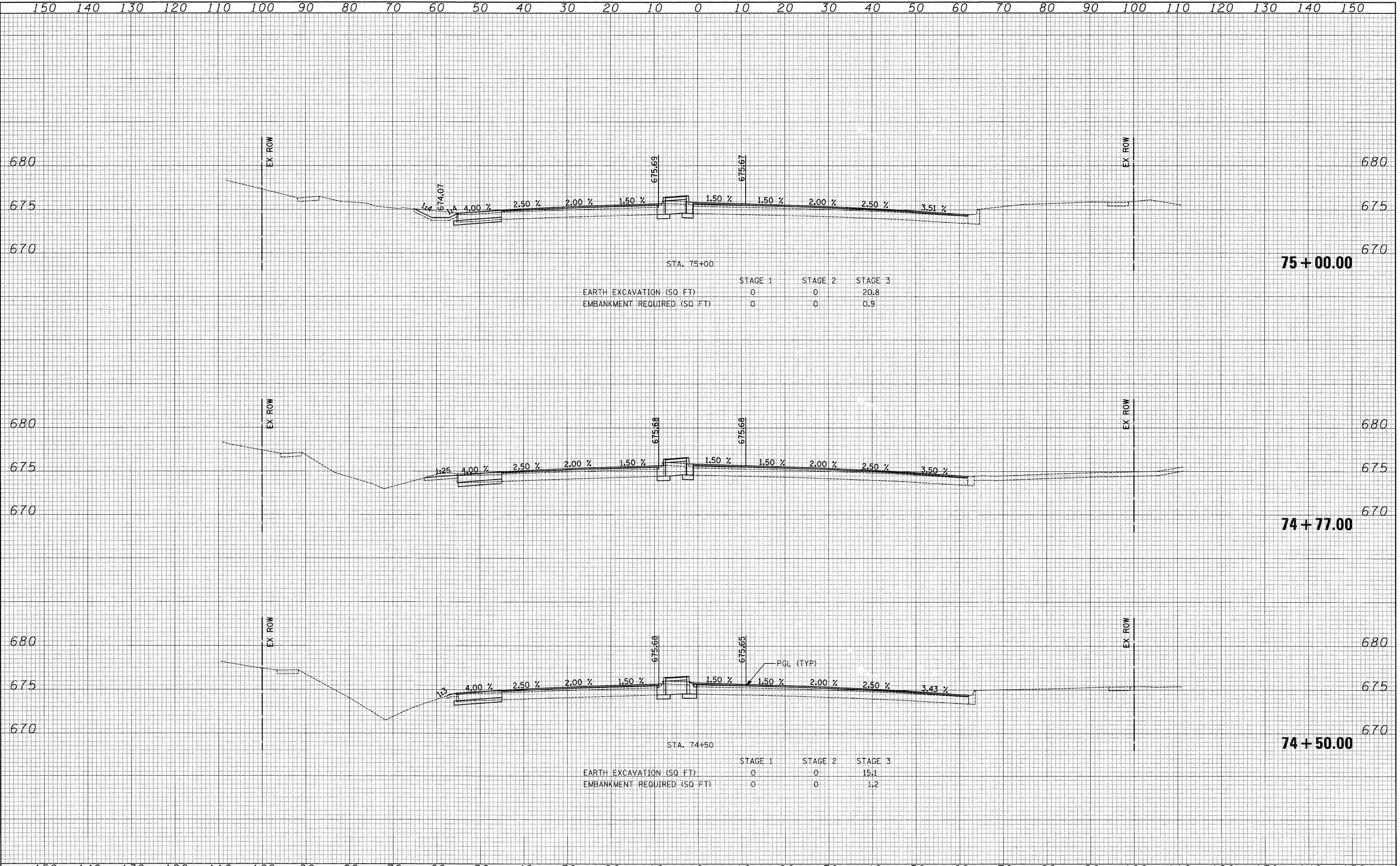
**BUSSE ROAD
 CROSS SECTIONS**

SCALE: $\frac{H}{V}$ 1"=10'
 $\frac{H}{V}$ 1"=5'

SHEET NO. 2 OF 9 SHEETS STA. 73+40.00 TO STA. 74+00.00

F.A.P. RTE. 344	SECTION 09-00057-00-CH	COUNTY COOK	TOTAL SHEETS 103	SHEET NO. 82
CONTRACT NO. 63523				
FED. ROAD DIST. NO. 1 ILLINOIS' FED. AID PROJECT M-90036601				

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BY	
SPRINKLED	
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FINAL SURVEY	
NOTE BOOK	
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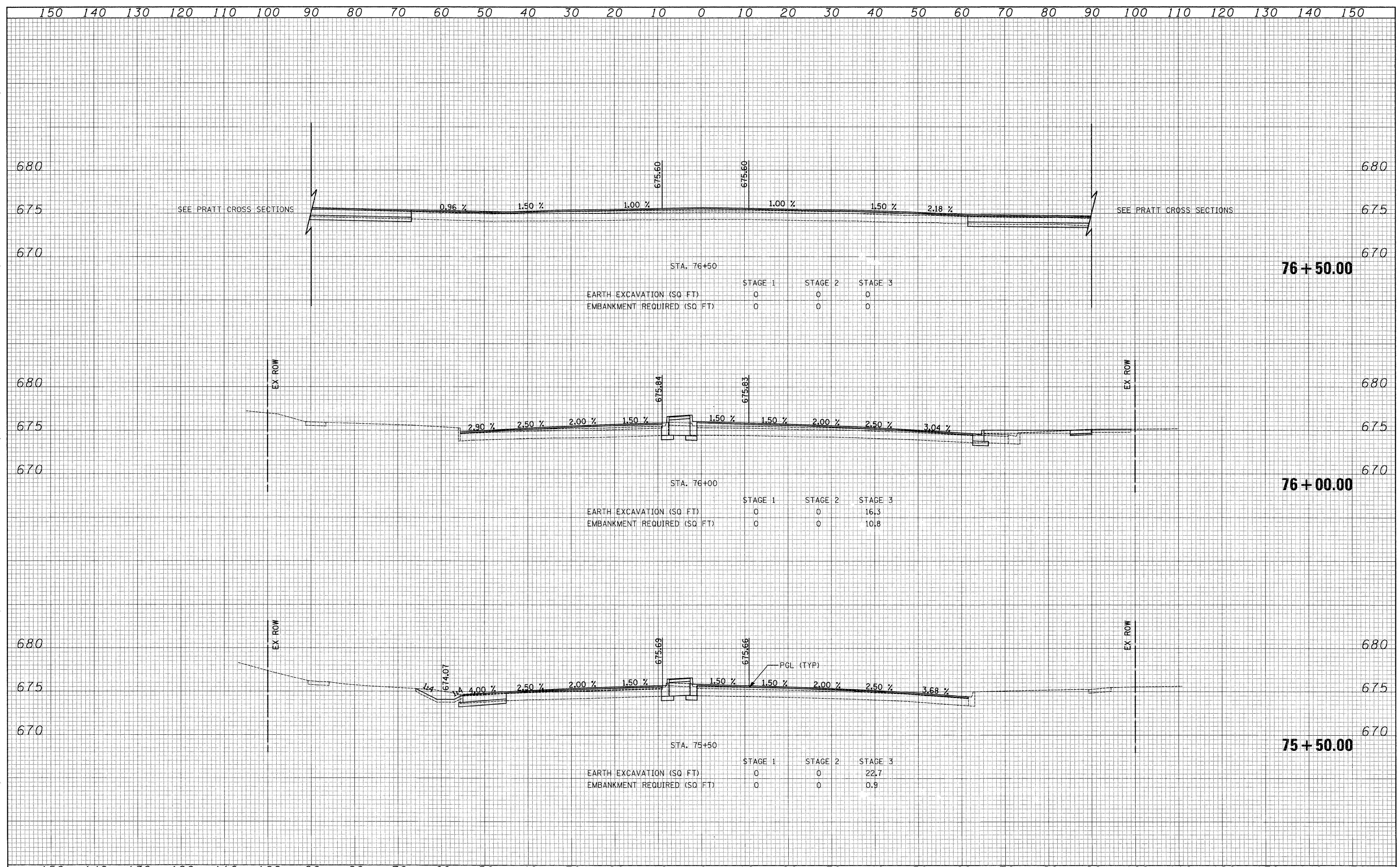


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NOTE BOOK	
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PLOT SCALE = 1/8" = 10' / IN.	DRAWN - BLG	REVISED -	SCALE: H _v 1"=10' V ₁ 1"=5'			SHEET NO. 3 OF 9 SHEETS		STA. 74+50.00 TO STA. 75+00.00		CONTRACT NO. 63523	
PLOT DATE = 10/22/2010	CHECKED - DJK	REVISED -	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-90031660								
DATE - 10-22-10	DATE - 10-22-10	REVISED -									

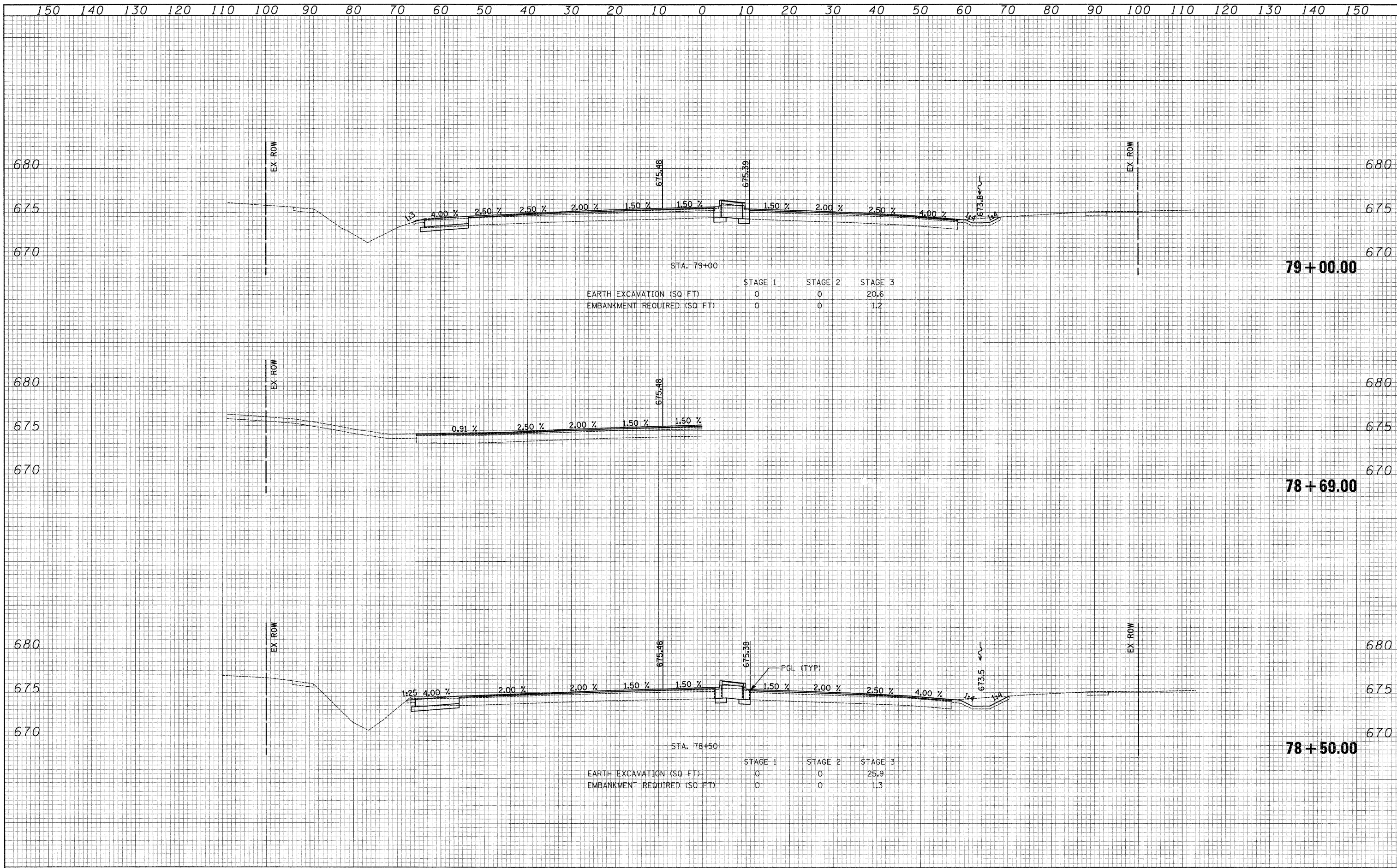
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 BY _____
 SKETCHED _____
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 NOTE BOOK _____
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DATE _____
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 TEMPLATE _____
 NOTE BOOK _____
 AREAS CHECKED _____
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DATE _____
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 SURVEY NO. _____
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 BY _____
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STA. 79+00

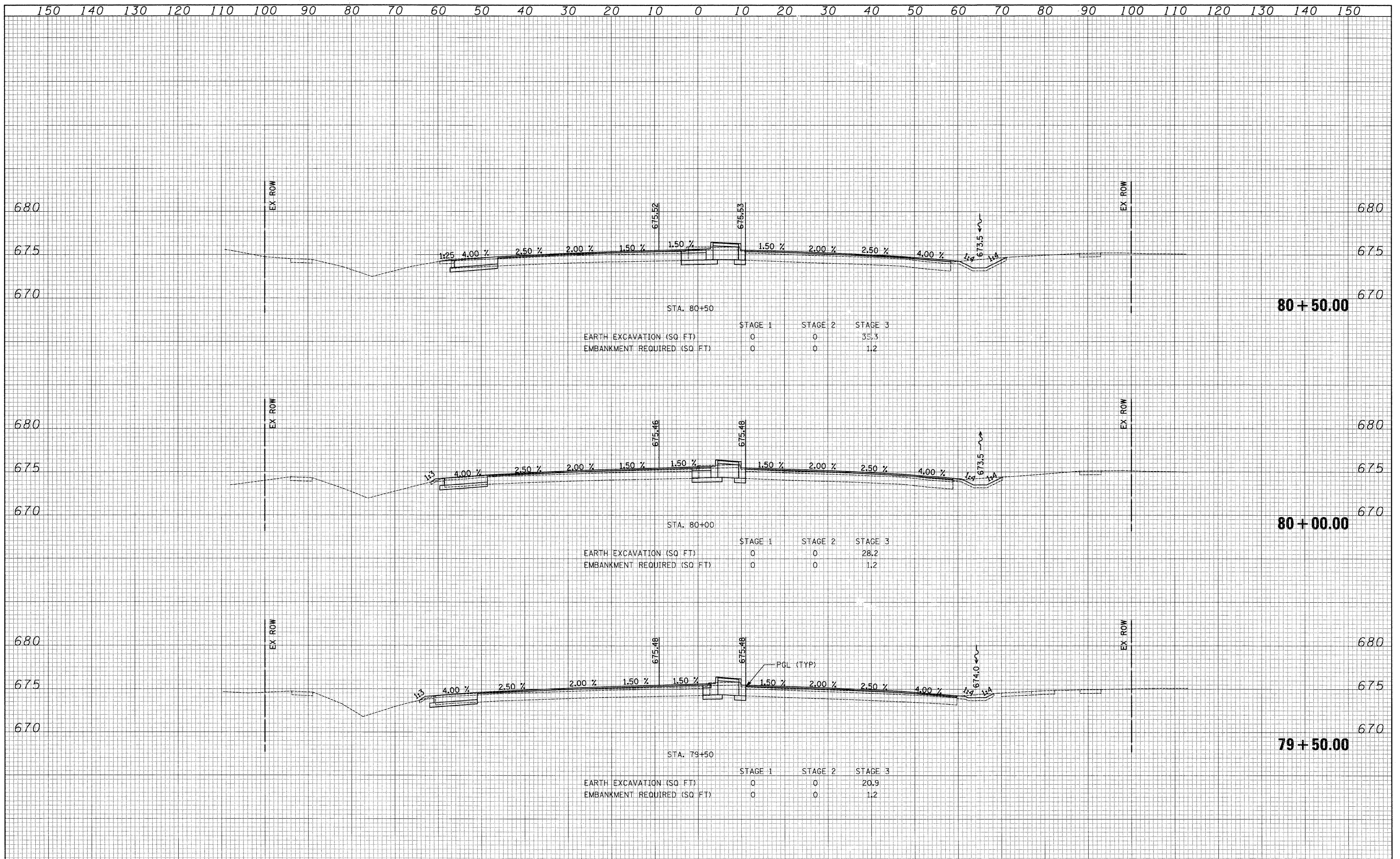
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EMBANKMENT REQUIRED (SQ FT)	0	0	1.2

STA. 78+50

	STAGE 1	STAGE 2	STAGE 3
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EMBANKMENT REQUIRED (SQ FT)	0	0	1.3

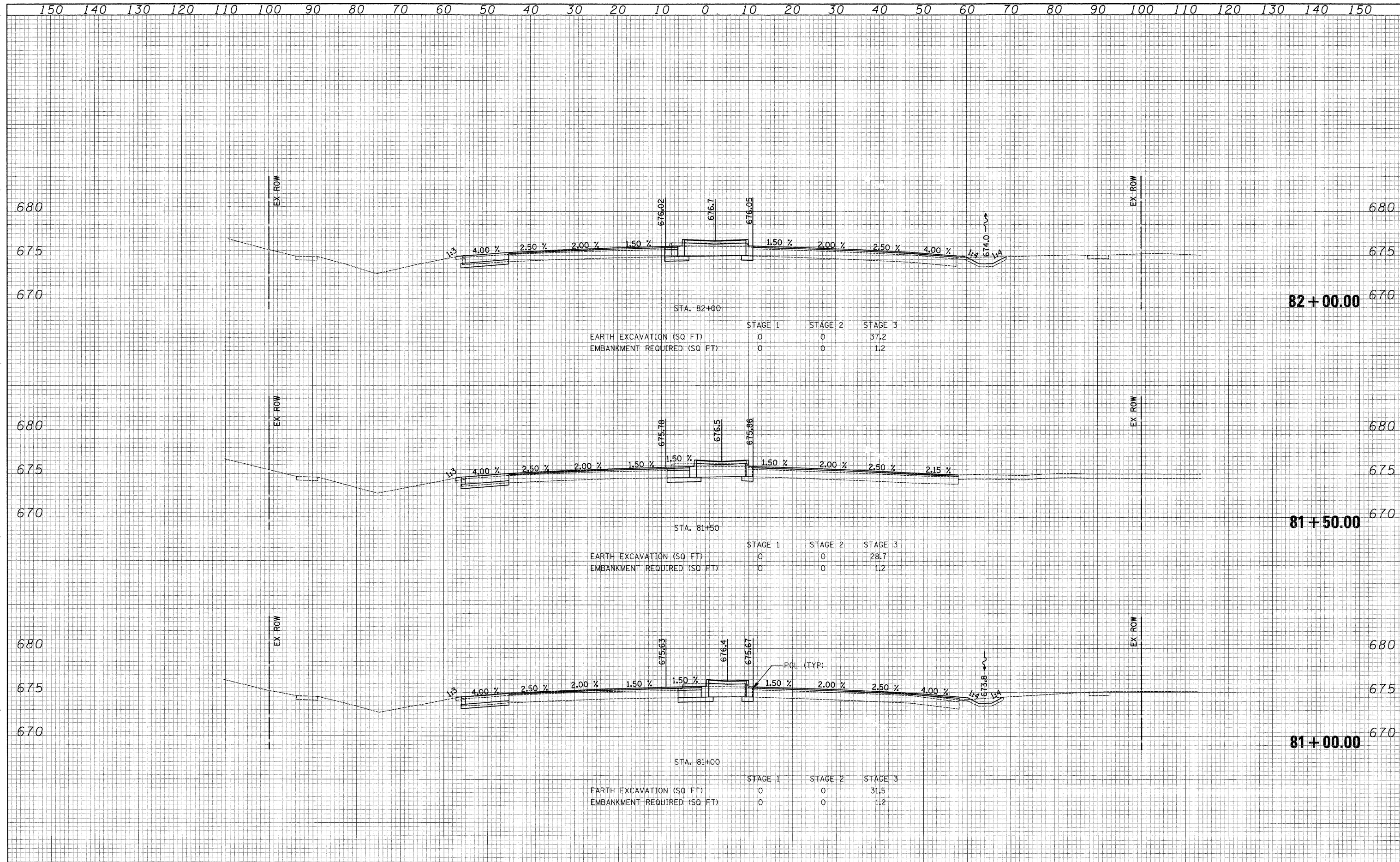
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BY	
FINAL SURVEY	
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NOTE BOOK	
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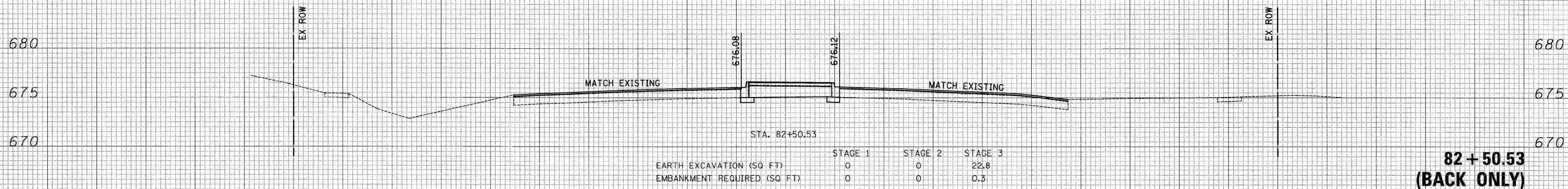
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NOTE BOOK	
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DATE	BY	DESIGNED	PLANNED	DATE
NO.	NO.	NO.	NO.	NO.

DATE	BY	DESIGNED	PLANNED	DATE
NO.	NO.	NO.	NO.	NO.



**82 + 50.53
(BACK ONLY)**

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DESIGNED - BLG
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 DATE - 10-22-10

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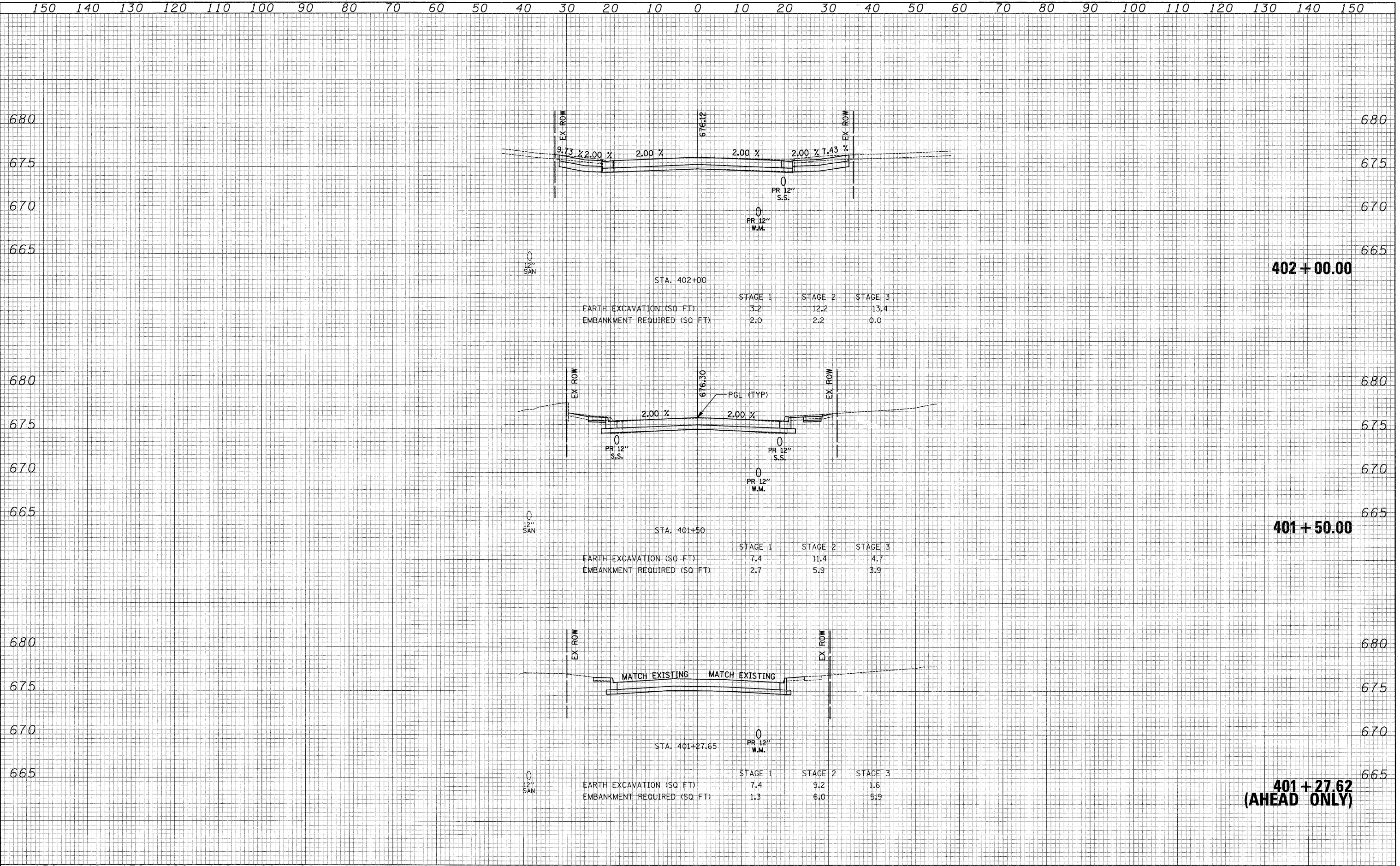
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**BUSSE ROAD
 CROSS SECTIONS**
 SCALE: H_v 1"=10'
 V_v 1"=5'
 SHEET NO. 9 OF 9 SHEETS
 STA. 82+50.53 TO STA. 82+50.53

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	09-00057-00-CH	COOK	103	89
CONTRACT NO. 63523				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003660				

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STA. 402+00

	STAGE 1	STAGE 2	STAGE 3
EARTH EXCAVATION (SQ FT)	3.2	12.2	13.4
EMBANKMENT REQUIRED (SQ FT)	2.0	2.2	0.0

STA. 401+50

	STAGE 1	STAGE 2	STAGE 3
EARTH EXCAVATION (SQ FT)	7.4	11.4	4.7
EMBANKMENT REQUIRED (SQ FT)	2.7	5.9	3.9

STA. 401+27.62

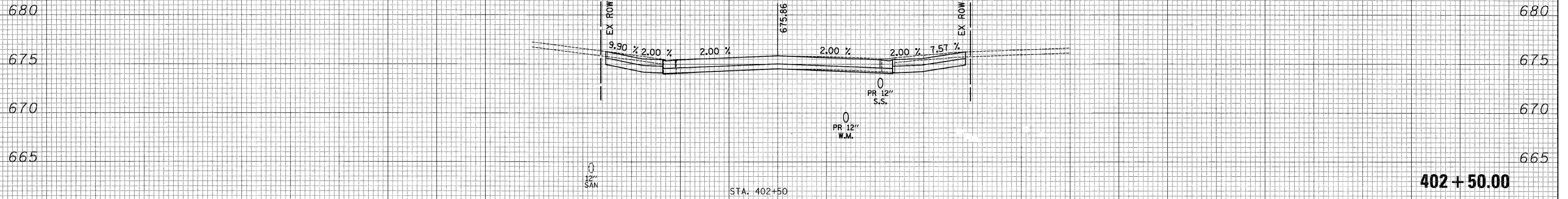
	STAGE 1	STAGE 2	STAGE 3
EARTH EXCAVATION (SQ FT)	7.4	9.2	1.6
EMBANKMENT REQUIRED (SQ FT)	1.3	6.0	5.9

402 + 00.00

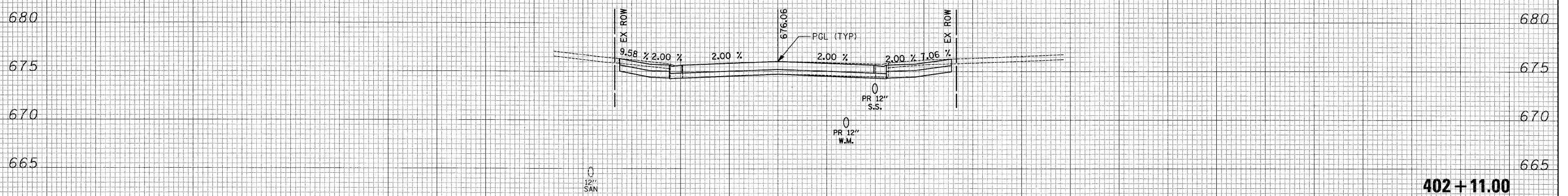
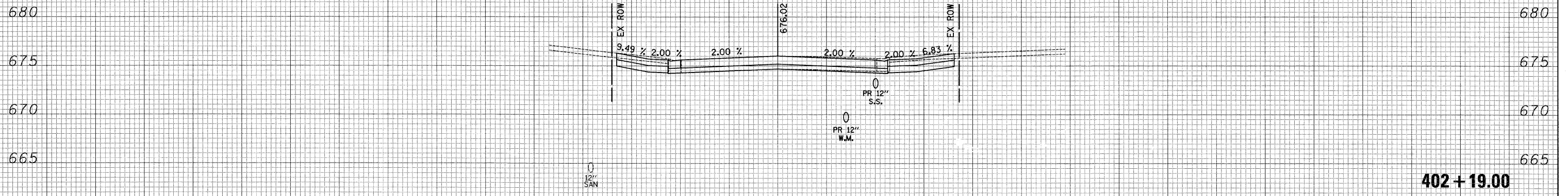
401 + 50.00

**401 + 27.62
(AHEAD ONLY)**

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150



	STAGE 1	STAGE 2	STAGE 3
EARTH EXCAVATION (SQ FT)	3.1	13.4	16.3
EMBANKMENT REQUIRED (SQ FT)	2.1	2.3	0.0



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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PRATT BOULEVARD
 CROSS SECTIONS**

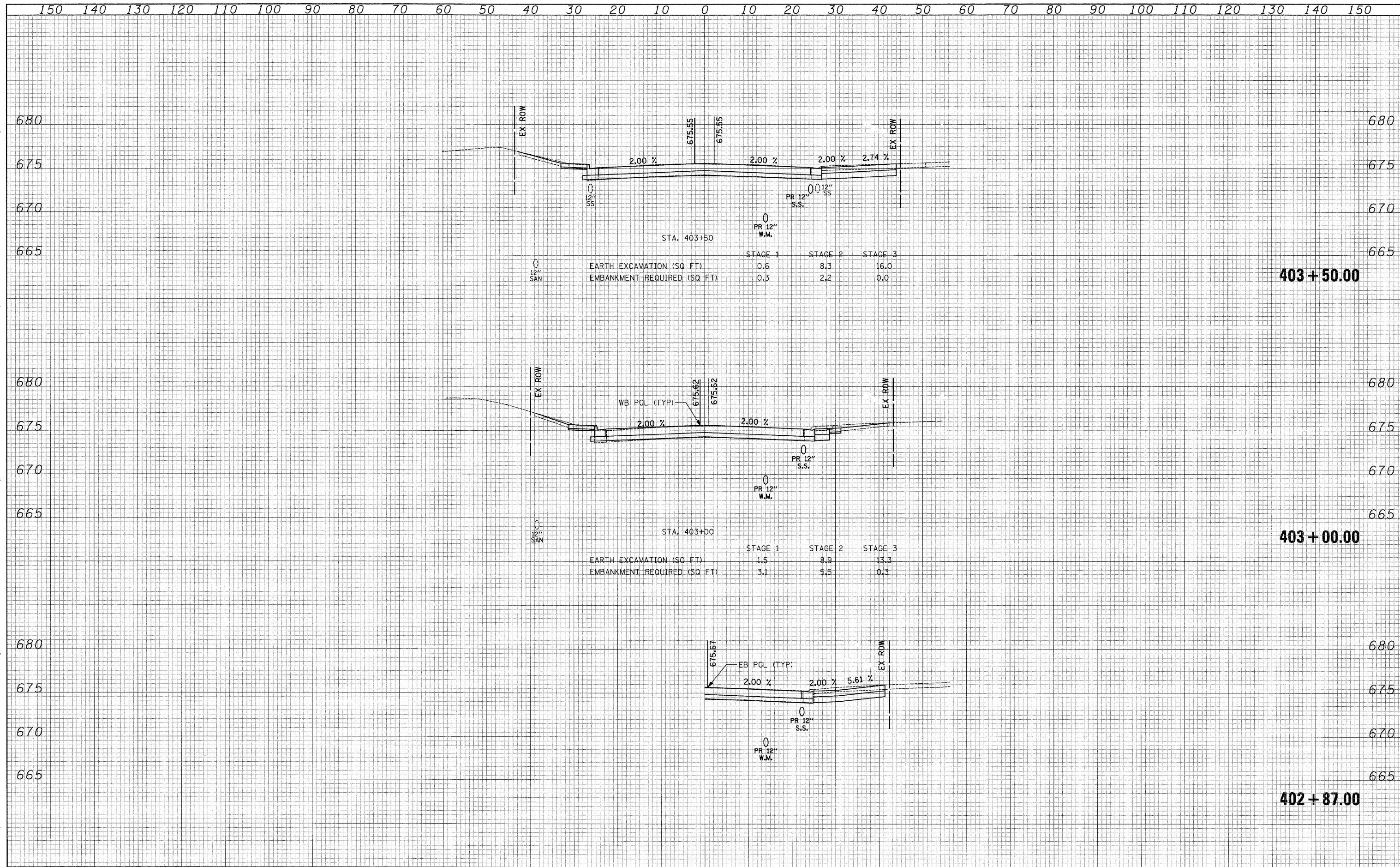
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SHEET NO. 2 OF 14 SHEETS STA. 402+11.00 TO STA. 402+50.00

F.A.P. RTE. 344	SECTION 09-000057-00-CH	COUNTY COOK	TOTAL SHEETS 103	SHEET NO. 91
CONTRACT NO. 63523				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003660				

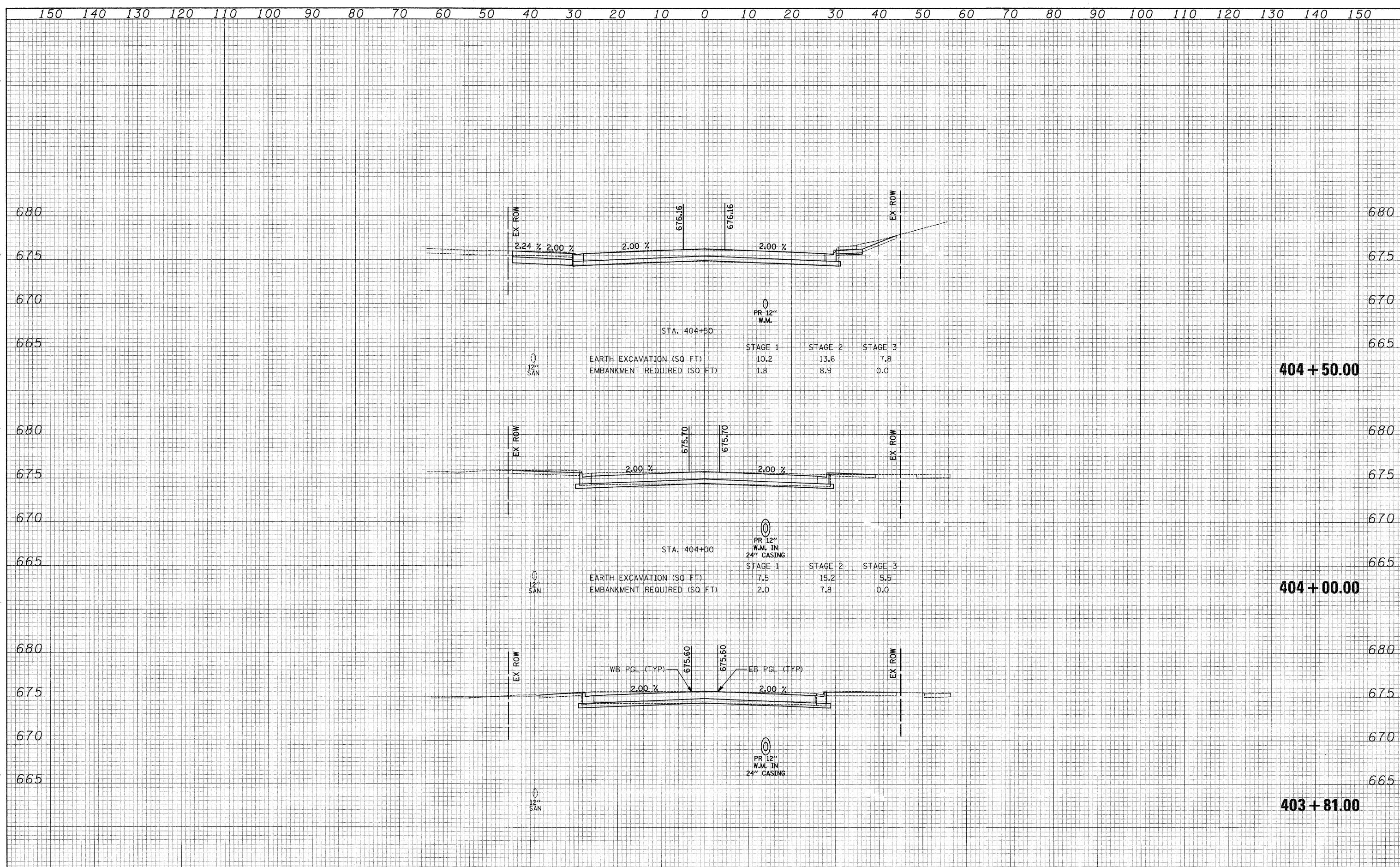
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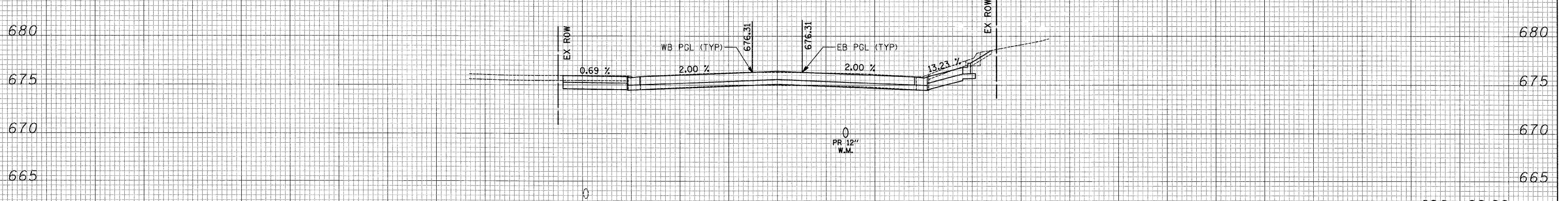
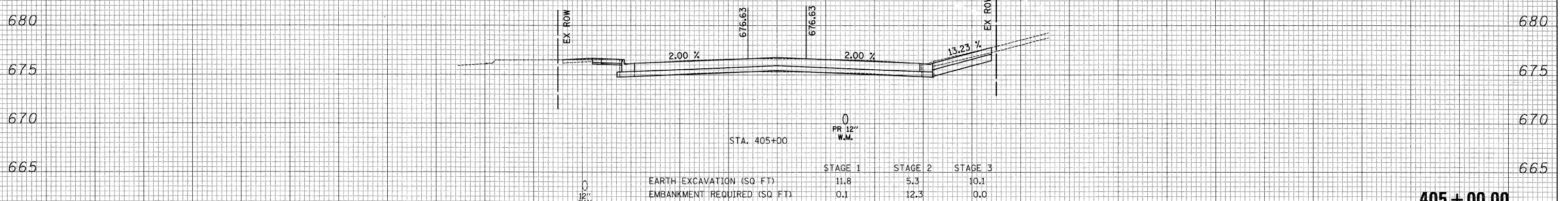
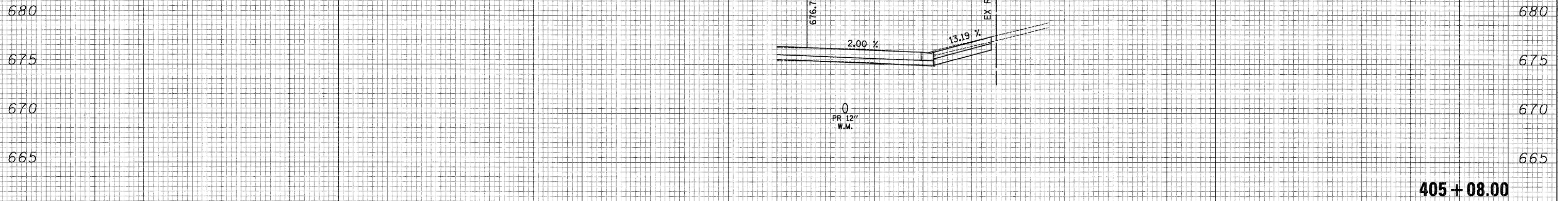
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PLOT SCALE = 10.0000' / IN.	CHECKED - DJK	REVISIED -	REVISIED -		SCALE: H: 1"=10' V: 1"=5'	SHEET NO. 4 OF 14 SHEETS	STA. 403+81.00 TO STA. 404+50.00	CONTRACT NO. 63523				
PLOT DATE = 10/22/2010	DATE - 10-22-10	REVISIED -	REVISIED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003660							

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

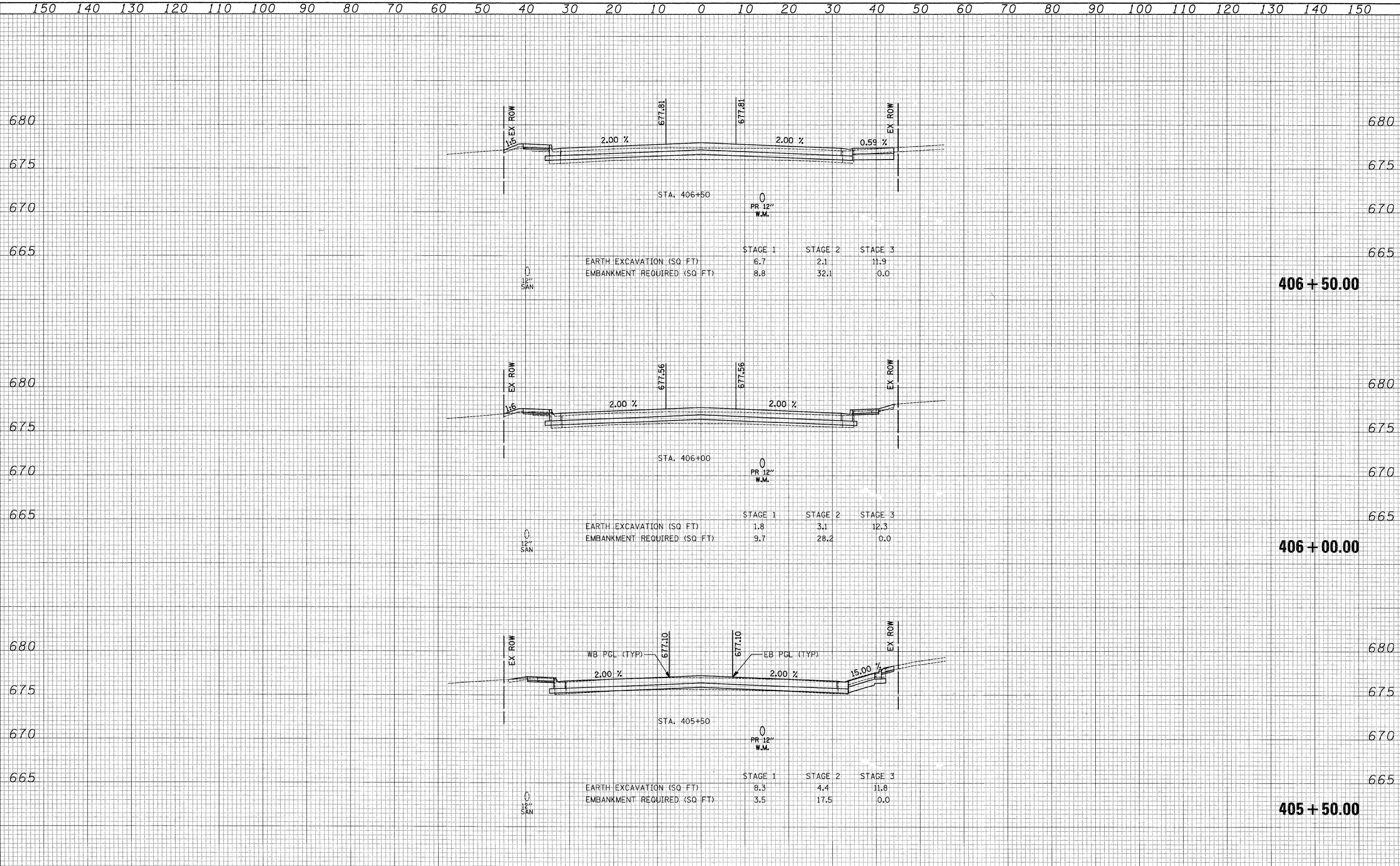
**PRATT BOULEVARD
 CROSS SECTIONS**

SCALE: $\frac{1}{4}$ " = 10' $\frac{1}{8}$ " = 5' SHEET NO. 5 OF 14 SHEETS STA. 404+66.00 TO STA. 405+08.00

F.A.P. RTE. 344	SECTION 09-00057-00-CH	COUNTY COOK	TOTAL SHEETS 103	SHEET NO. 94
CONTRACT NO. 63523				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-90031660				

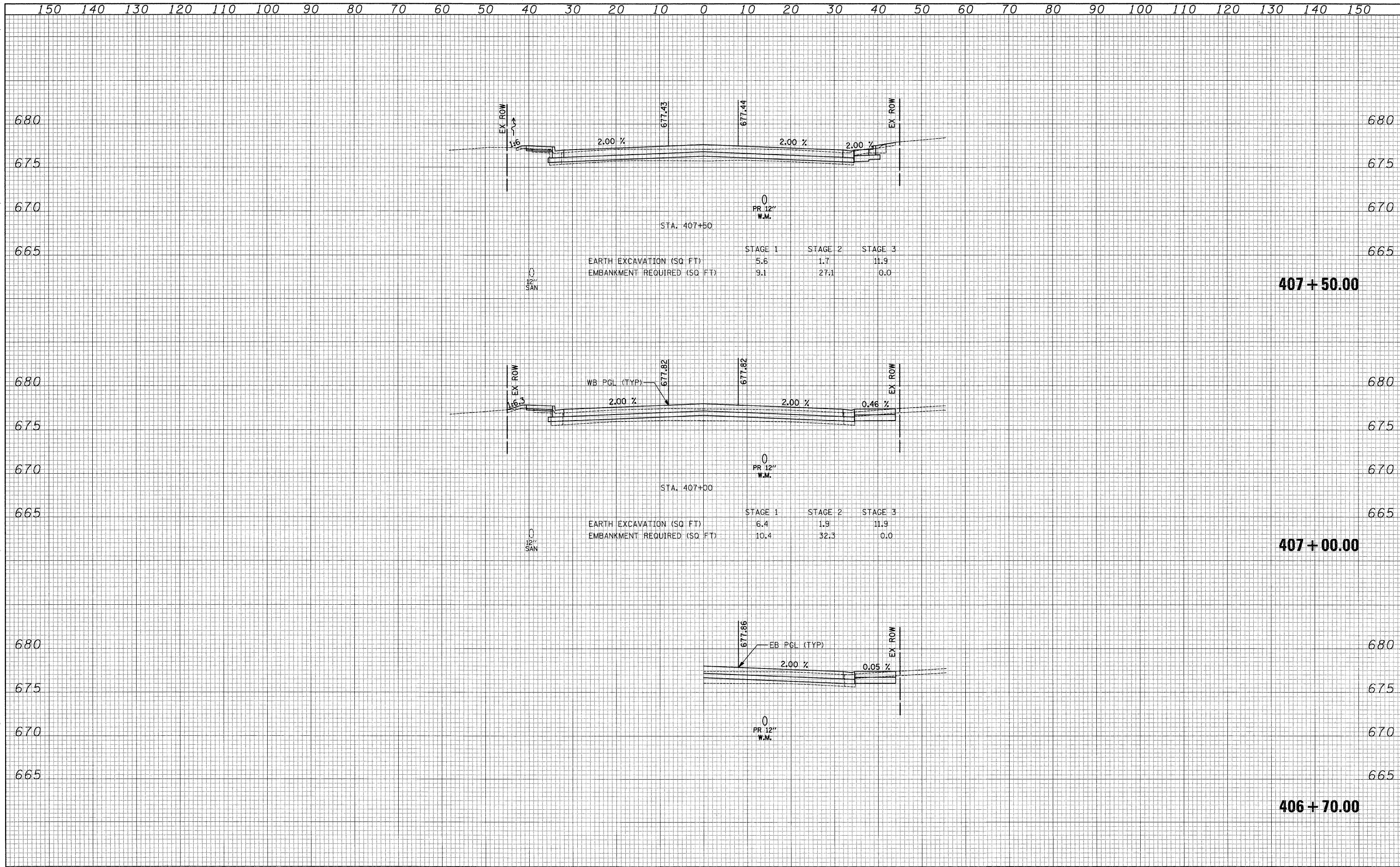
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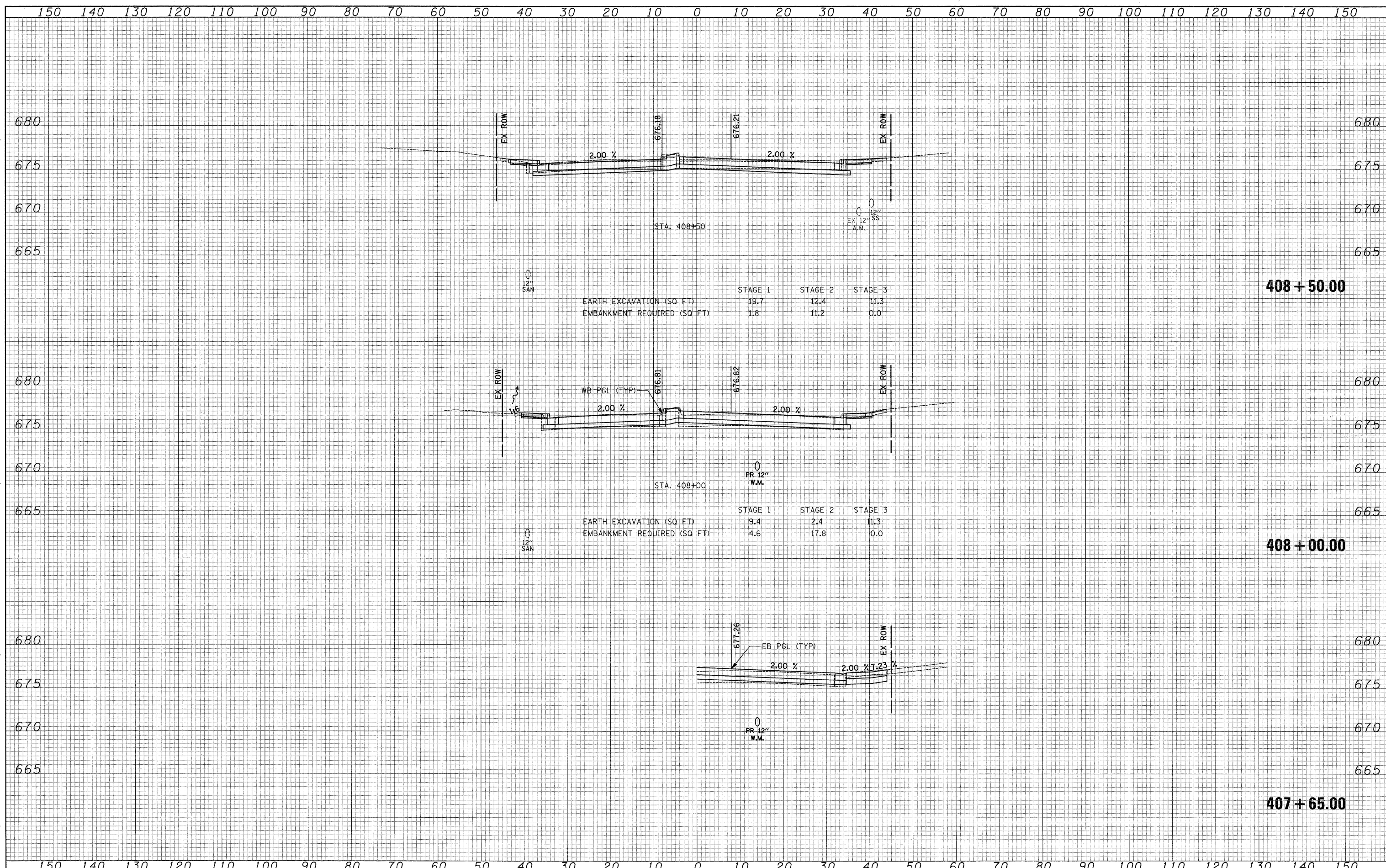
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PLOT DATE = 10/22/2010	DATE - 10-22-10	REVISED -	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-90031660									

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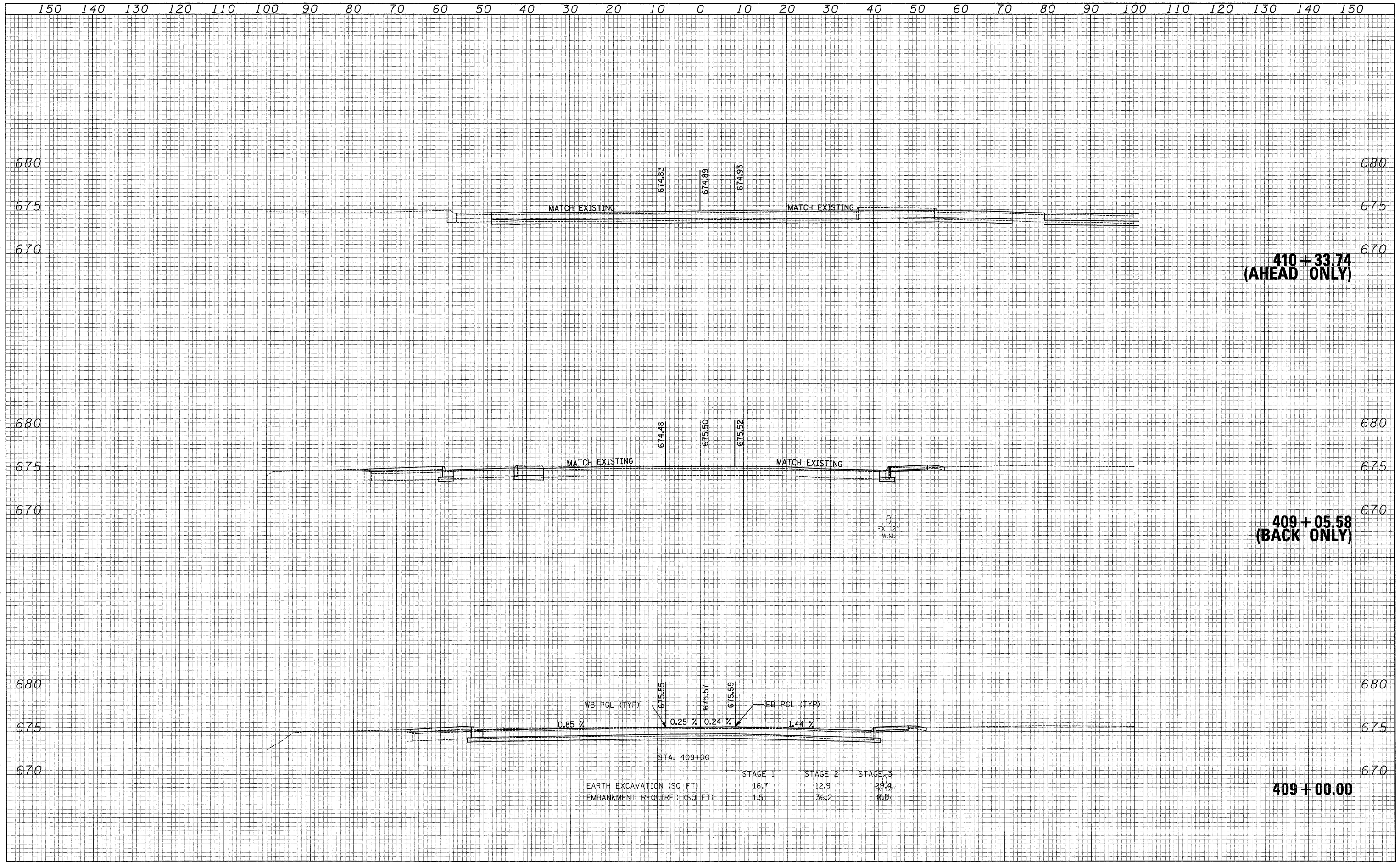


	STAGE 1	STAGE 2	STAGE 3
EARTH EXCAVATION (SQ FT)	19.7	12.4	11.3
EMBANKMENT REQUIRED (SQ FT)	1.8	11.2	0.0

	STAGE 1	STAGE 2	STAGE 3
EARTH EXCAVATION (SQ FT)	9.4	2.4	11.3
EMBANKMENT REQUIRED (SQ FT)	4.6	17.8	0.0

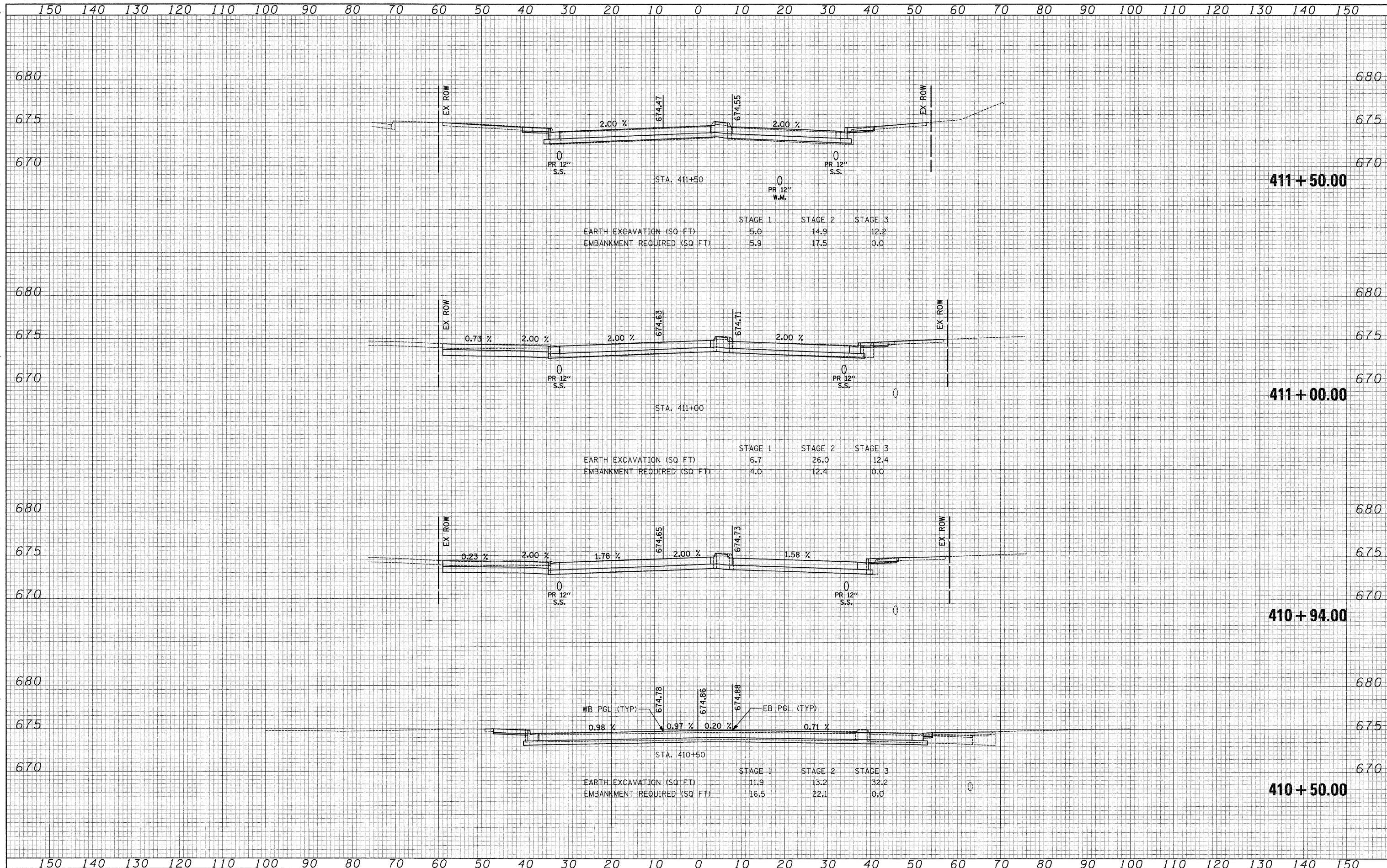
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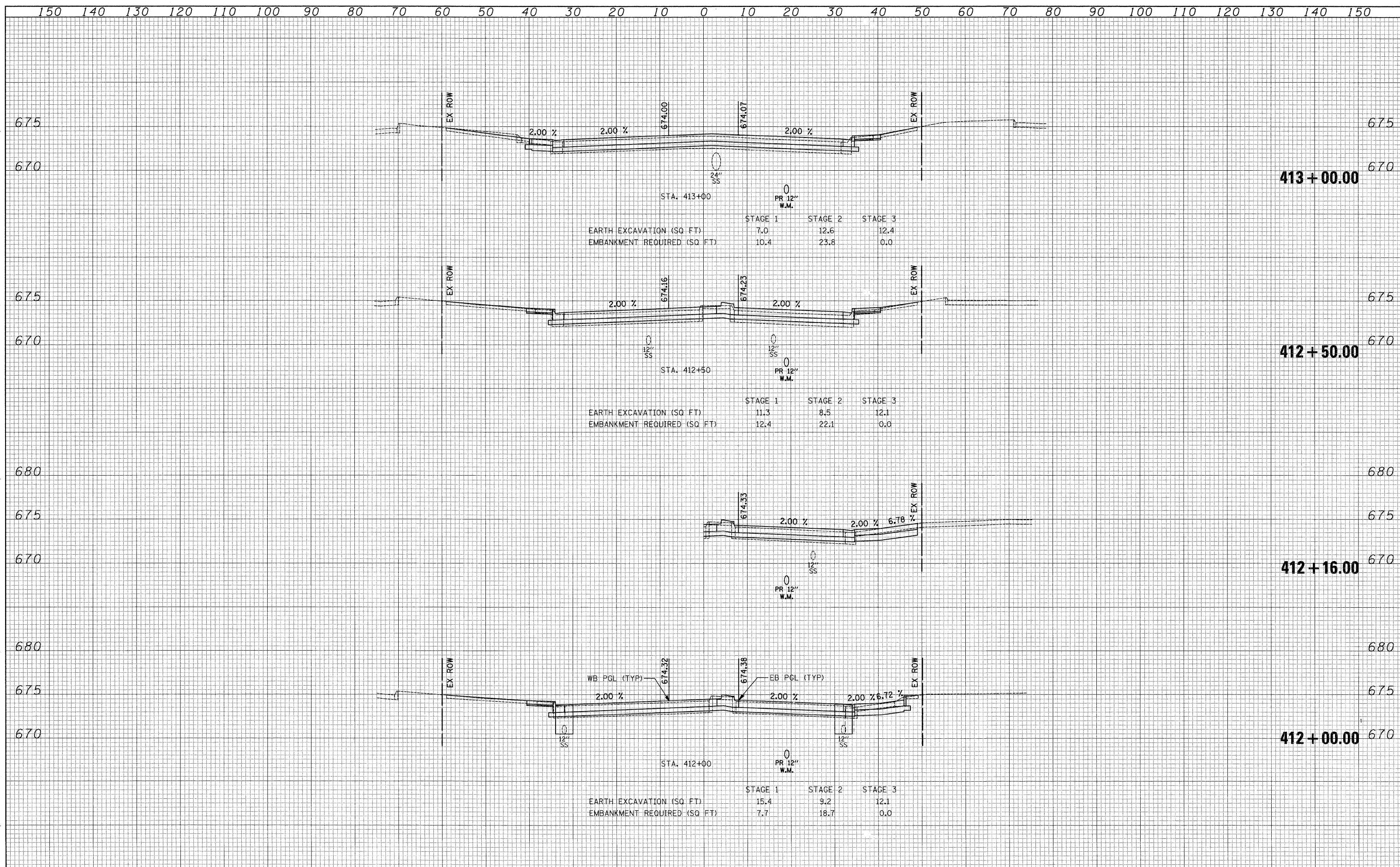
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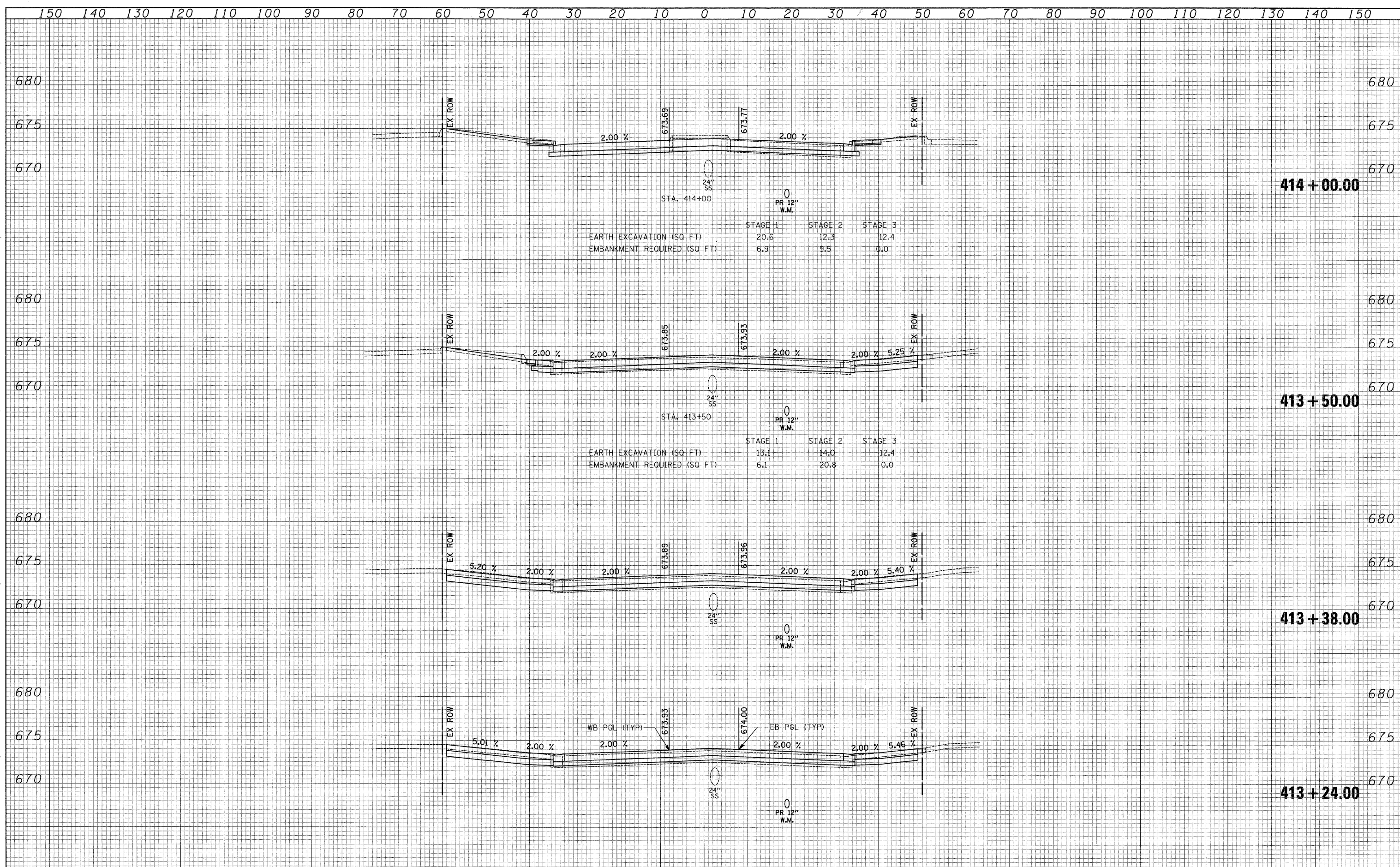
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PLOT SCALE = 10.0000' / IN.	CHECKED - DJK	DRAWN - BLG	REVISED -			SCALE: H _v 1"=10' V _v 1"=5'	SHEET NO. 11 OF 14 SHEETS	STA. 412+00.00 TO STA. 413+00.00	CONTRACT NO. 63523			
PLOT DATE = 10/22/2010	DATE = 10-22-10	CHECKED - DJK	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-90031660						
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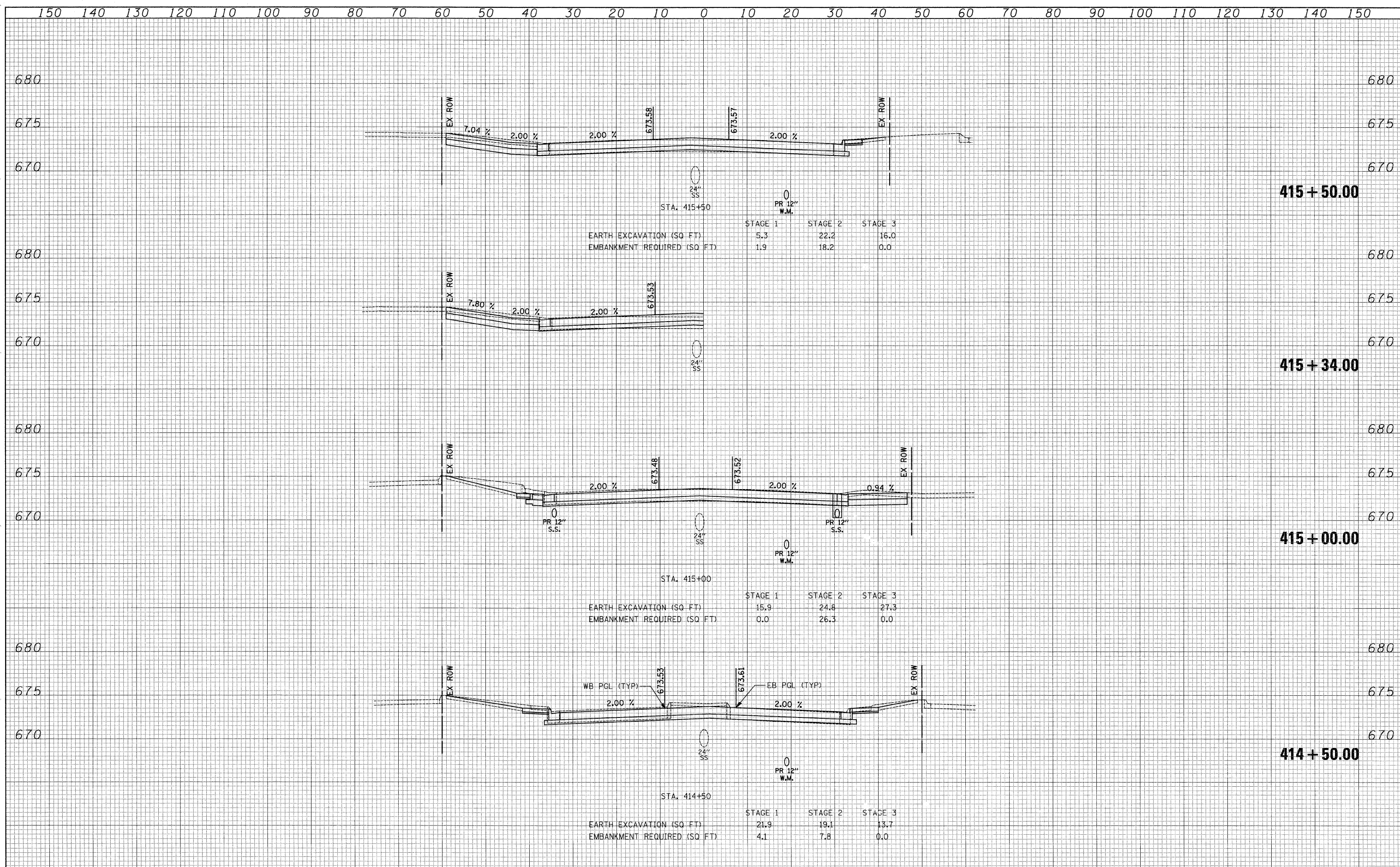
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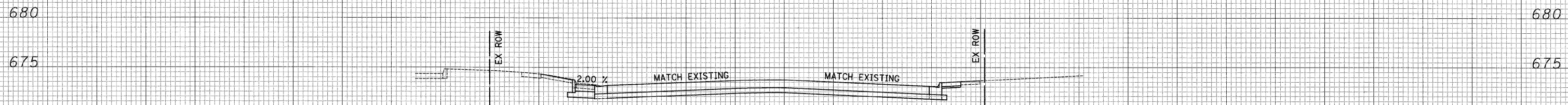
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STA. 415+68.82
 24" SS
 0 PR 12" W.M.

	STAGE 1	STAGE 2	STAGE 3
EARTH EXCAVATION (SQ FT)	4.0	9.6	13.6
EMBANKMENT REQUIRED (SQ FT)	1.5	15.9	0.0

**415 + 68.82
 (BACK ONLY)**

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PRATT BOULEVARD
 CROSS SECTIONS**
 SCALE: H: 1"=10'
 V: 1"=5'
 SHEET NO. 14 OF 14 SHEETS
 STA. 415+68.82 TO STA. 415+68.82

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	09-00057-00-CH	COOK	103	103
CONTRACT NO. 63523				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-90031660				