

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2626	05-00193-04-BR	LAKE	36	1
FED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT NO. 63531	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PLANS FOR PROPOSED
FEDERAL AID HIGHWAY**

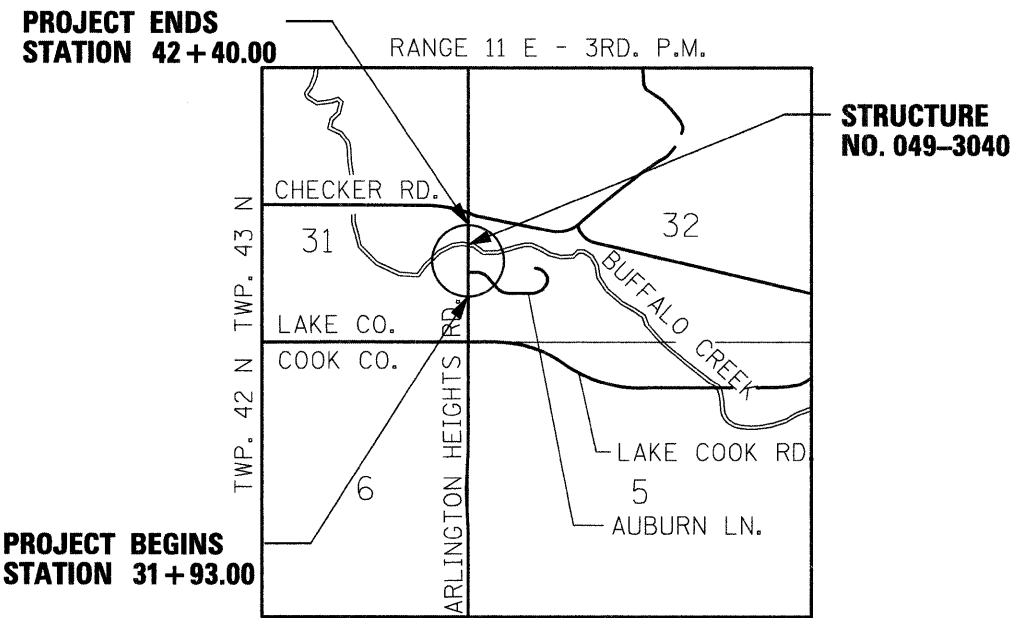
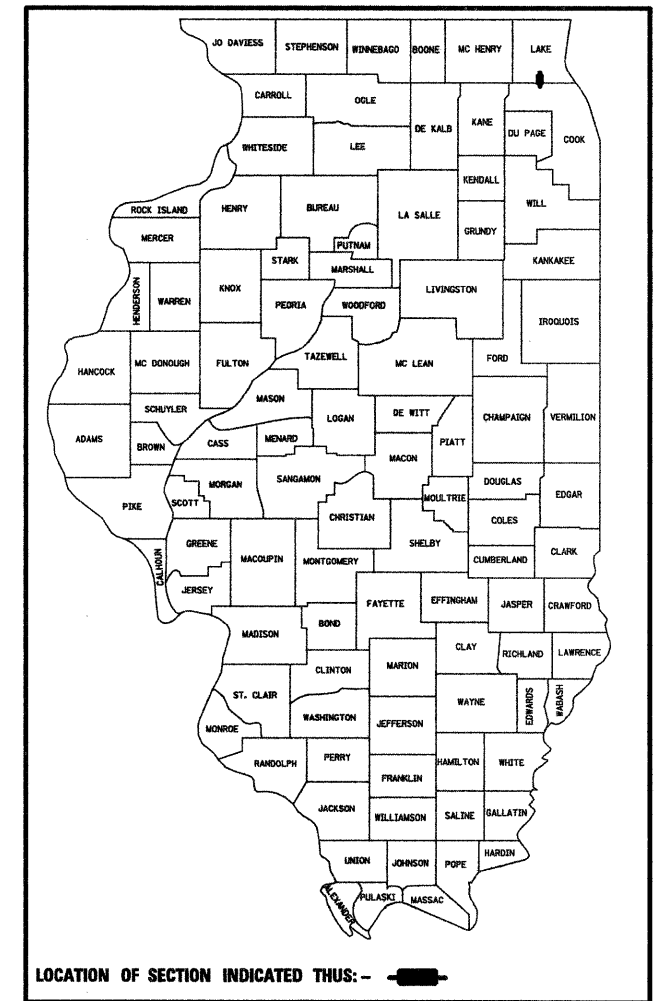
FOR INDEX OF SHEETS, SEE SHEET NO. 2
FOR LIST OF STANDARDS, SEE SHEET NO. 2

DESIGN DESIGNATION: MINOR ARTERIAL

TRAFFIC DATA
2006 ADT: 32,050
POSTED SPEED LIMIT: 45 MPH

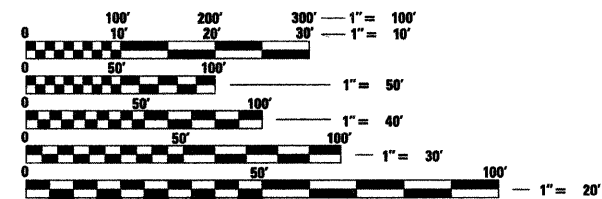
**F.A.U. ROUTE 2626 (ARLINGTON HEIGHTS ROAD)
NB BRIDGE OVER BUFFALO CREEK
SECTION 05-00193-04-BR
PROJECT BRM-8003(792)
BRIDGE DECK REPLACEMENT
LAKE COUNTY**

C-91-180-07



VERNON TOWNSHIP
MAP SCALE: NTS

NET AND GROSS LENGTH = 1,047 FT. = 0.20 MILE



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

CONTRACT NO. 63531

TOWNSHIP: VERNON
MUNICIPALITY: BUFFALO GROVE

MACTEC
8745 W. Higgins Road
Suite 300
Chicago, Illinois 60631
Phone (773) 693-6030
Fax (773) 693-6039
IL License 184-001138
Expires 04/30/2009

Michael R. Kurtz
Michael R. Kurtz
EXPIRATION DATE: 11/30/2011
DATE: October 25, 2010
DRAWING NO. ALL EXCEPT 12 - 26

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED October 21 2010
M. M. Buehler
LAKE COUNTY DIVISION OF TRANSPORTATION, COUNTY ENGINEER

PASSED NOVEMBER 9 2010
C. H. Christensen
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID
BASED ON LIMITED
REVIEW Nov 4 2010
Don Oll
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. 847-705-4406 SCHAUMBURG, IL

INDEX OF SHEETS

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15	17" x 36" PCC DECK BEAM
16	17" x 36" PCC BEAM DETAILS
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18 - 19	BIKEWAY RAILING DETAILS
20	TEMPORARY BARRIER DETAILS
21	SOUTH ABUTMENT DETAILS
22	NORTH ABUTMENT DETAILS
23	ABUTMENT DETAILS
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LIST OF STANDARDS

<u>NUMBER</u>	<u>STATE STANDARD</u>
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
280001-05	TEMPORARY EROSION CONTROL SYSTEMS
420401-08	BRIDGE APPROACH PAVEMENT CONNECTOR
515001-03	NAME PLATES FOR BRIDGES
606001-04	CONCRETE CURB TYPE B AND COMBINATION CURB AND GUTTER
630001-09	STEEL PLATE BEAM GUARDRAIL
631011-07	TRAFFIC BARRIER TERMINAL, TYPE 2
631026-05	TRAFFIC BARRIER TERMINAL, TYPE 5
631031-09	TRAFFIC BARRIER TERMINAL, TYPE 6
635001-01	DELINEATORS
635006-03	REFLECTOR AND TERMINAL MARKER PLACEMENT
635011-02	REFLECTOR MARKER AND MOUNTING DETAILS
701101-02	OFF-ROAD OPERATIONS, MULTILANE, 15' (4.5M) TO 24' (600MM) FROM PAVEMENT EDGE
701801-04	LANE CLOSURE, MULTILANE 1W OR 2W, CROSSWALK OR SIDEWALK CLOSURE
701901-01	TRAFFIC CONTROL DEVICES
704001-06	TEMPORARY CONCRETE BARRIER
780001-02	TYPICAL PAVEMENT MARKINGS
781001-03	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
 <u>DISTRICT 1 STANDARDS</u>	
BD-32	BUTT JOINTS AND HMA TAPER
TC-11	RAISED REFLECTOR PAVEMENT MARKERS (SNOW PLOW RESISTANT)
TC-22	ARTERIAL ROAD INFORMATION SIGN

GENERAL NOTES

- ALL CONSTRUCTION SHALL BE IN CONFORMANCE WITH THE APPLICABLE REQUIREMENT SET FORTH IN "THE CONSTRUCTION SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED JANUARY 1, 2007 THEREINAFTER REFERRED TO AS STANDARD SPECIFICATIONS. THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM MANUAL TRAFFIC CONTROL DEVICES FOR STREETS & HIGHWAYS" IN EFFECT ON THE DATE OF INVITATION FOR BIDS; THE "SUPPLEMENTAL SPECIFICATIONS & RECURRING SPECIAL PROVISIONS" ADOPTED JANUARY 1, 2011; SPECIAL PROVISIONS AS INCLUDED IN THE CONTRACT DOCUMENTS; AND THE DETAILS AND STANDARDS CONTAIN IN THESE PLANS.
- BEFORE STARTING ANY EXCAVATIONS, THE CONTRACTOR SHALL CALL "JULIE" AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE & GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)
- THE LOCATIONS OF THE EXISTING UTILITIES, AS SHOWN ON THE DRAWINGS, REPRESENT DATA RECEIVED FROM VARIOUS SOURCES; IT IS NOT GUARANTEED TO BE CORRECT OR ALL INCLUSIVE. THE CONTRACTOR SHALL CONDUCT HIS OWN INVESTIGATIONS INTO THE LOCATION, SIZE, DEPTH, AND NATURE OF ANY AND ALL EXISTING UTILITIES WHICH MAY INTERFERE WITH THE WORK UNDER THIS CONTRACT. ANY EXISTING UTILITIES WHICH ARE TO REMAIN IN SERVICE SHALL BE FULLY PROTECTED BY THE CONTRACTOR AND ANY DAMAGE CAUSED BY THE CONSTRUCTION SHALL BE IMMEDIATELY REPAIRED AT NO ADDITIONAL COST TO THE COUNTY.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, LAKE COUNTY DIVISION OF TRANSPORTATION, AND THE VILLAGE OF BUFFALO GROVE.
- ALL WORK SHALL BE COMPLETED WITHIN THE LIMITS OF THE PROJECT SHOWN. NO EQUIPMENT, MATERIALS OR A YARD OR FIELD OFFICE SHALL BE SET UP OR STORED ON COUNTY OR PRIVATE PROPERTY WITHOUT WRITTEN PERMISSION OF THE COUNTY OR THE PROPERTY OWNER.
- BARRICADES: THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE II BARRICADE USED - ONE (1) WEIGHTED SANDBAG ACROSS EACH BOTTOM RAIL.
- ALL EXCAVATION AND EMBANKMENT LOCATIONS REQUIRING SEEDING OR SODDING SHALL BE CONSTRUCTED TO 6 IN BELOW FINISHED GRADE LINE TO ALLOW TOPSOIL PLACEMENT.
- PAVEMENT ELEVATIONS: THE ELEVATIONS SHOWN ON THE PLANS ARE FINISHED GRADES FOR THE PAVEMENT OR SURFACE COURSE, UNLESS OTHERWISE INDICATED.
- MAINTENANCE OF TRAFFIC-GENERAL: TRAFFIC CONDITIONS, ACCIDENTS AND OTHER UNFORESEEN EMERGENCY CONDITIONS MAY REQUIRE THE ENGINEER TO RESTRICT, MODIFY OR REMOVE LANE CLOSURES OR CHANNELIZATION SHOWN ON THE PLANS. THE CONTRACTOR SHALL RESPOND WITHIN 30 MINUTES OF THE TIME OF THE NOTIFICATION BY THE ENGINEER FOR THE MAINTENANCE OF TRAFFIC CONTROL DEVICES.
- PROTECTION OF AND RESTORATION OF TRAFFIC SIGNS: PRIOR TO THE BEGINNING OF CONSTRUCTION OPERATIONS, THE CONTRACTOR WILL PROVIDE A SIGN LOG OF ALL EXISTING SIGNS WITHIN THE LIMITS OF THE CONSTRUCTION ZONE. THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING THE ACCURACY OF THE SIGN LOG THROUGHOUT THE DURATION OF THIS PROJECT. THIS WORK IS CONSIDERED INCLUDED IN THE COST OF THE CONTRACT. ALL EXISTING TRAFFIC SIGNS SHALL MAINTAIN, FURNISH, AND REPLACE AT HIS EXPENSE, ANY TRAFFIC SIGN OR POST WHICH HAS BEEN DAMAGED OR LOST BY THE CONTRACTOR.
- TRAFFIC CONTROL DEVICES: ALL TRAFFIC CONTROL DEVICES USED FOR THE MAINTENANCE OF TRAFFIC AS DETAILED ON THE PLANS SHALL BE REFLECTORIZED PRIOR TO INSTALLATION AND CLEANED AS NECESSARY THROUGHOUT THE DURATION OF THE CONTRACT.
- DURING CONSTRUCTION OPERATIONS ANY CONSTRUCTION DEBRIS DEPOSITED IN THE FLOW LINE DRAINAGE STRUCTURE SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY AT THE CONCLUSION OF THE CONSTRUCTION OPERATIONS. ALL DRAINAGE STRUCTURES SHALL BE CLEANED AS NECESSARY TO INSURE THAT THEY ARE FREE FROM ALL DIRT AND CONSTRUCTION DEBRIS PRIOR TO THE FINAL INSPECTION OF THE PROJECT. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF EARTH EXCAVATION.
- OFFSETS TO DRAINAGE STRUCTURES ARE MEASURED TO THE FLOWLINE OF THE GUTTER.
- IF THE CONTRACTOR CHOOSES TO DISPOSE OF UNCONTAMINATED SOIL OR UNCONTAMINATED SOIL MIXED WITH CLEAN CONSTRUCTION AND DEMOLITION DEBRIS (CCDD) AT A CCDD FILL OPERATION, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PERFORM ALL NECESSARY FIELD AND LABORATORY ANALYSIS AND TO OBTAIN THE LICENSED PROFESSIONAL ENGINEER'S CERTIFICATION REQUIRED AS PER PUBLIC ACT 96-1416 TO USE THE SITE. NO ADDITIONAL COMPENSATION WILL BE PROVIDED.

15. UTILITIES CONTACTS:
- | | | |
|--|---|---|
| J.U.L.I.E.
JOINT UTILITY LOCATION FOR EXCAVATORS
(800) 892-0123 OR 811 | LAKE COUNTY DEPT OF PUBLIC WORKS
650 W. WINCHESTER ROAD
LIBERTYVILLE, IL 60048
GORDON WHITE
(847) 377-7135 | COMCAST
688 INDUSTRIAL DRIVE
ELMHURST, IL 60126
MR. ROBERT SCHULTER, JR.
(630) 600-6347 |
| ILLINOIS DEPARTMENT OF TRANSPORTATION
GEORGE GUDERLEY
AREA PERMIT COORDINATOR
201 WEST CENTER COURT
SCHAUMBURG, IL 60196
(847) 705-4131 | LAKE COUNTY DIVISION OF TRANSPORTATION
JON NELSON
ENGINEER OF TRAFFIC
(847) 377-7400 | VILLAGE OF BUFFALO GROVE
51 RAUPP BOULEVARD
BUFFALO GROVE, IL 60089
(847)459-2545 |
| NICOR GAS
90 N. FINLEY ROAD
GLEN ELLYN, IL 60137
TIM HENEHAN | COMMONWEALTH EDISON ELECTRIC COMPANY
1500 FRANKLIN BLVD.
LIBERTYVILLE, IL 60048
TIMOTHY TAMASON
(847)816-5521 | AT&T
255 E CHICAGO STREET
ELGIN, IL 60120
DON VIRMOND
(847)888-6621 |

NOTE: "THE ILLINOIS DEPARTMENT OF TRANSPORTATION IS NOT THE OWNER OF RECORD FOR THIS BRIDGE. THOSE SEEKING HISTORIC, AS-BUILT OR OTHER EXISTING DOCUMENTS AND OWNER OF RECORD PLANS MUST CONTACT THE TO MAKE ARRANGEMENTS FOR ACCESS TO THIS INFORMATION."

FILE NAME = LC8987-sht-gennote.dgn	USER NAME = 3202080907	DESIGNED - TC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, IDOT STANDARDS, AND GENERAL NOTES		F.A.J. RTE. 2626	SECTION 05-00193-04-BR	COUNTY LAKE	TOTAL SHEETS 36	SHEET NO. 2	
	PLOT SCALE = 2.0000 "/ IN.	DRAWN - PWL	REVISED -		SCALE: NTS	SHEET NO. 1 OF 1 SHEETS	STA. N/A	TO STA. N/A	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			
	PLOT DATE = 11/2/2010	CHECKED - MRH	REVISED -		CONTRACT NO. 63531							
		DATE - 11/2/2010	REVISED -									

SUMMARY OF QUANTITIES

SPECIALITY ITEMS	SPECIAL PROVISIONS	CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE 0014
		20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	55	55
	•	20200100	EARTH EXCAVATION	CU YD	117	117
	•	20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	138	138
		20400800	FURNISHED EXCAVATION	CU YD	36	36
	•	21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	802	802
		25000210	SEEDING, CLASS 2A	ACRE	0.17	0.17
		25000400	NITROGEN FERTILIZER NUTRIENT	POUND	15	15
		25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	15	15
		28000500	INLET AND PIPE PROTECTION	EACH	2	2
		31101200	SUBBASE GRANULAR MATERIAL, TYPE B 4"	SQ YD	239	239
		35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	242	242
		40300100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	39	39
		40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SQ YD	24	24
		40603595	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	TON	92	92
		42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	1,998	1,998
		44000100	PAVEMENT REMOVAL	SQ YD	296	296
		44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	564	564
		44000600	SIDEWALK REMOVAL	SQ FT	2,091	2,091
		50101500	REMOVAL OF EXISTING SUPERSTRUCTURES	EACH	1	1
		50102400	CONCRETE REMOVAL	CU YD	9.7	9.7
		50300225	CONCRETE STRUCTURES	CU YD	19.5	19.5
		50300255	CONCRETE SUPERSTRUCTURE	CU YD	102.1	102.1
	•	50300300	PROTECTIVE COAT	SQ YD	206	206
		50400305	PRECAST PRESTRESSED CONCRETE DECK BEAMS (17" DEPTH)	SQ FT	1,853	1,853
		50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	27,140	27,140
S		50901720	BICYCLE RAILING	FOOT	59	59

SUMMARY OF QUANTITIES

SPECIALITY ITEMS	SPECIAL PROVISIONS	CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE 0014
S		50901750	PARAPET RAILING	FOOT	102	102
		59000200	EPOXY CRACK INJECTION	FOOT	50	50
		60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	1	1
		60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	334	334
		60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	170	170
S		63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	1	1
S		63100070	TRAFFIC BARRIER TERMINAL, TYPE 5	EACH	1	1
S		63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	1	1
S	•	63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	1	1
		63200310	GUARDRAIL REMOVAL	FOOT	238	238
		67100100	MOBILIZATION	L SUM	1	1
	•	X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1
		70300320	PAVEMENT MARKING TAPE, TYPE I 4"	FOOT	1,336	1,336
		70300330	PAVEMENT MARKING TAPE, TYPE I 5"	FOOT	989	989
		70400100	TEMPORARY CONCRETE BARRIER	FOOT	504	504
		72400710	RELOCATE SIGN PANEL - TYPE 1	SQ FT	6	6
S		78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	225	225
S		78000300	THERMOPLASTIC PAVEMENT MARKING - LINE 5"	FOOT	180	180
S		78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	930	930
S		78008220	POLYUREA PAVEMENT MARKING TYPE I - LINE 5"	FOOT	777	777
S		78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	20	20
S	•	78200450	MONODIRECTIONAL GUARD RAIL REFLECTORS	EACH	4	4
S		78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	1	1
		78300100	PAVEMENT MARKING REMOVAL	SQ FT	63	63
S		78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	20	20
	•	Z0004552	APPROACH SLAB REMOVAL	SQ YD	130	130

FILE NAME = LC0907-sh1-S00-1.dgn

USER NAME = 320200907

DESIGNED - TC

REVISED -

DRAWN - PWL

REVISED -

PLOT SCALE = 100.0000' / IN.

CHECKED - MRH

REVISED -

PLOT DATE = 11/2/2010

DATE - 11/2/2010

REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

F.A.U. RTE. 2626

SECTION 05-00193-04-BR

COUNTY LAKE

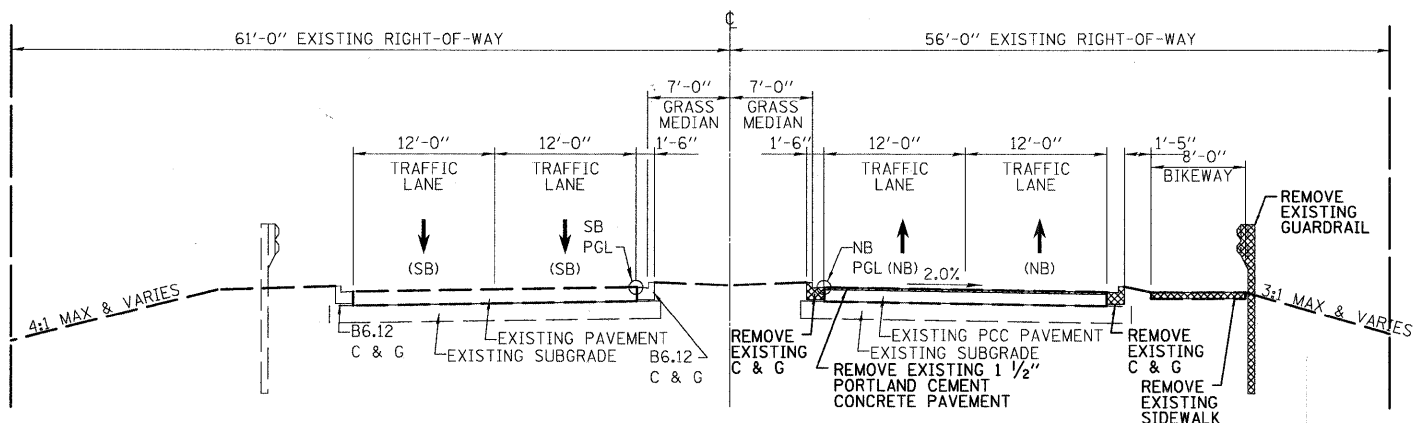
TOTAL SHEETS 36

SHEET NO. 3

CONTRACT NO. 63531

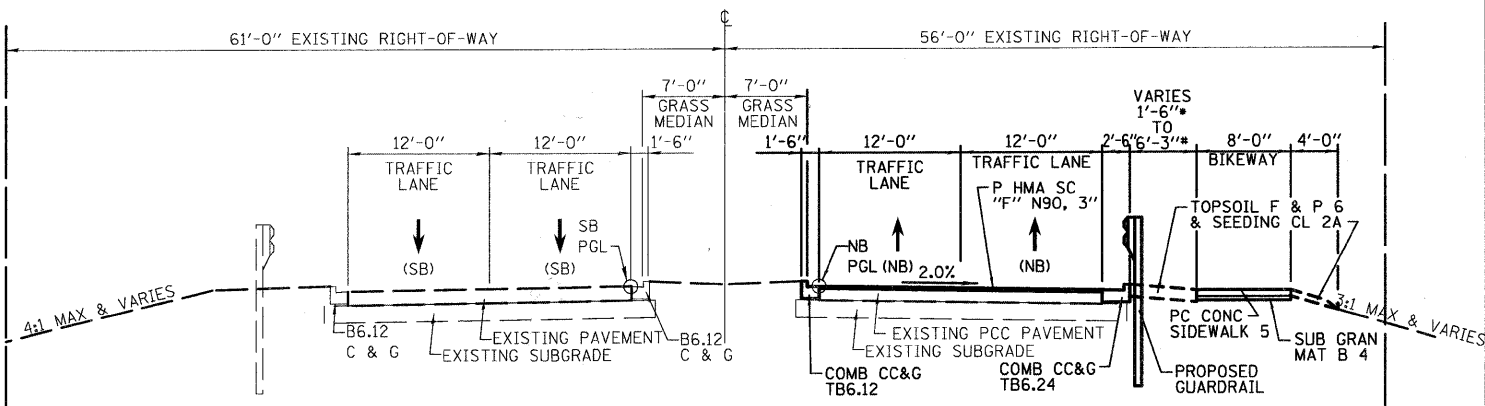
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FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



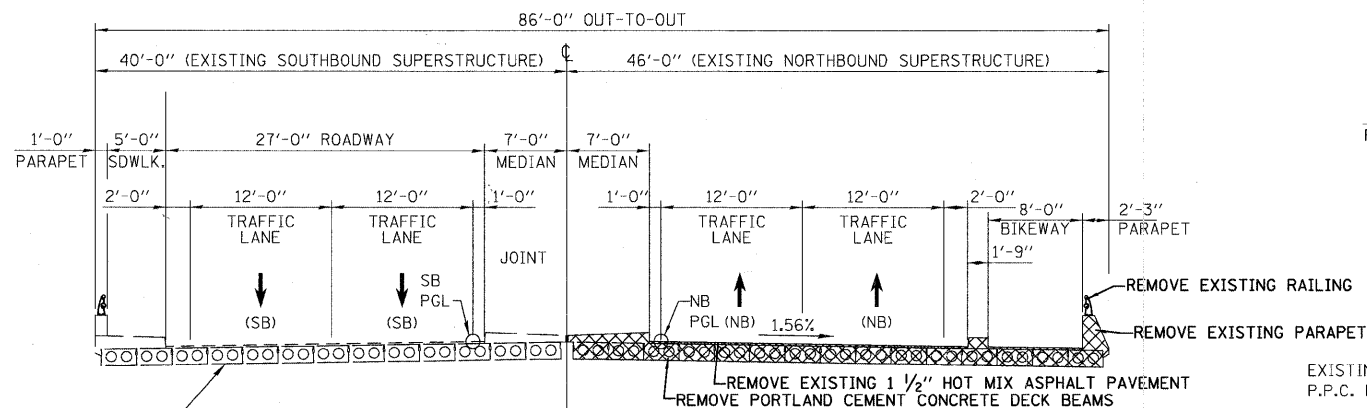
ARLINGTON HEIGHTS ROAD
EXISTING TYPICAL ROADWAY SECTION

STA. 38+93.00 TO STA. 39+81.33
STA. 40+82.49 TO STA. 42+40.00



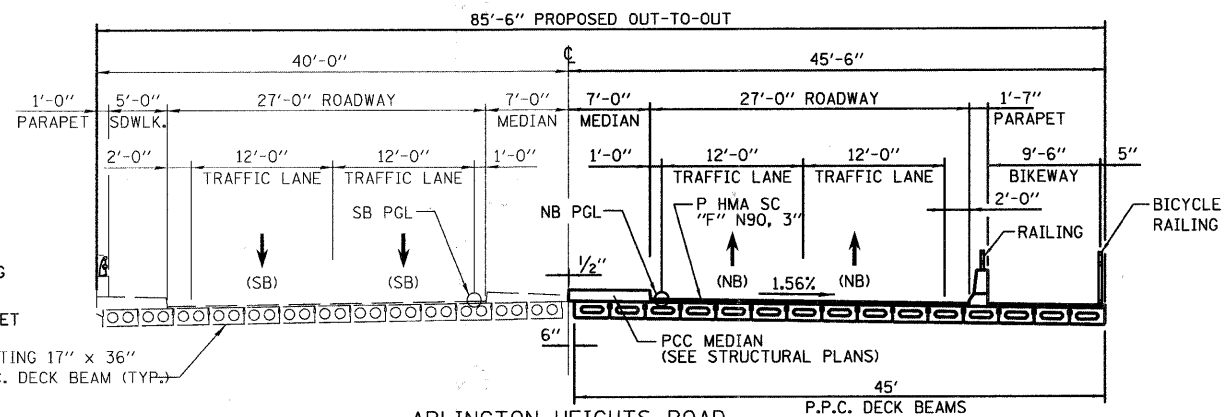
ARLINGTON HEIGHTS ROAD OVER BUFFALO CREEK
PROPOSED TYPICAL ROADWAY SECTION

STA. 31+93.00 TO STA. 39+81.33
STA. 40+82.49 TO STA. 42+40.00
* STA. 31+93.00 TO STA. 38+87.88
* STA. 38+87.88 TO STA. 39+81.33 AND
STA. 40+82.49 TO STA. 42+40.00



ARLINGTON HEIGHTS ROAD
EXISTING TYPICAL BRIDGE SECTION

STA. 39+81.33 TO STA. 40+82.49



ARLINGTON HEIGHTS ROAD
PROPOSED TYPICAL BRIDGE SECTION

STA. 39+81.33 TO STA. 40+82.49

STRUCTURAL PAVEMENT DESIGN

STRUCTURAL DESIGN TRAFFIC: YEAR 2030
PV = 30,127 SU = 962 MU = 961
ROAD/STREET CLASSIFICATION: CLASS 1
PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:
P = 94 S = 3 M = 3
TRAFFIC FACTOR: ACTUAL TF = 5.47 AC TYPE = N/A
MINIMUM TF = N/A SURFACE = N/A
PG GRADE: BINDER = N/A
SUBGRADE SUPPORT RATING:
SSR = POOR

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

ITEM	VOIDS
ARLINGTON HEIGHTS ROAD	
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL 9.5MM), 3"	4% @ 90 GYR.
TEMPORARY ROAD	
TEMPORARY PAVEMENT (HOT-MIX ASPHALT BINDER COURSE, IL-19 mm), 6"	4% @ 50 GYR.
TEMPORARY OVERLAY	
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL 9.5MM), 1 1/2"	4% @ 90 GYR.

NOTE: THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN.
THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA, THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP", SEE DISTRICT ONE SPECIAL PROVISIONS.

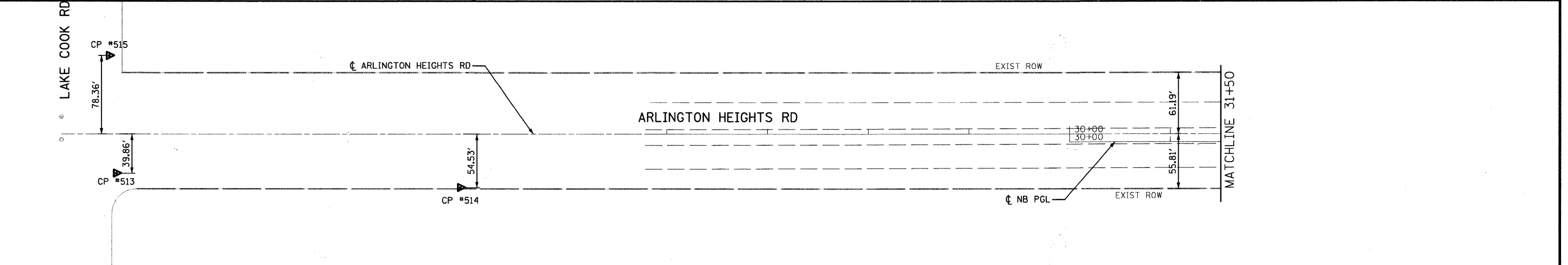
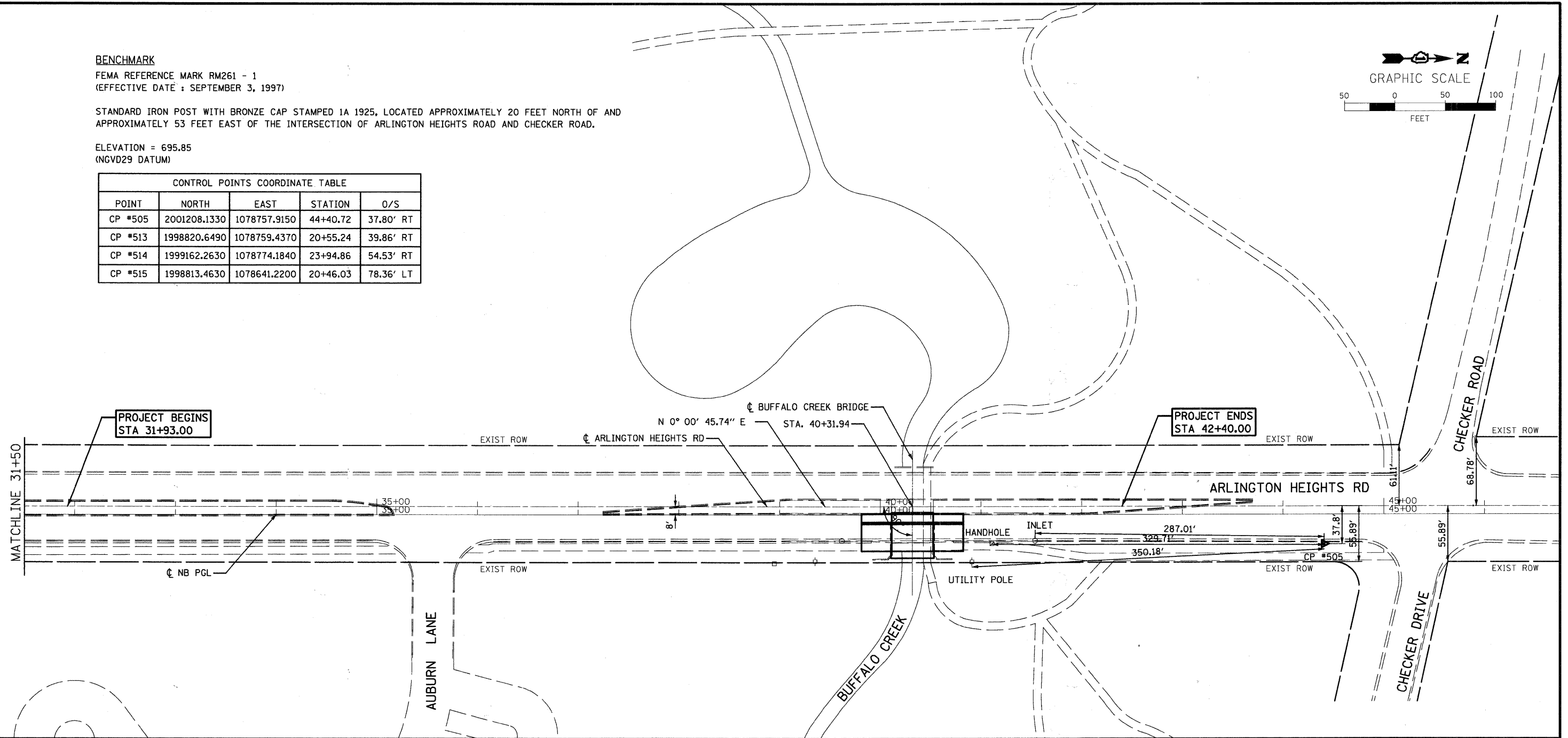
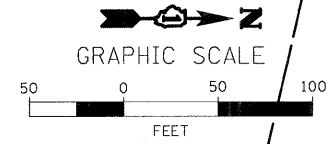
BENCHMARK

FEMA REFERENCE MARK RM261 - 1
(EFFECTIVE DATE : SEPTEMBER 3, 1997)

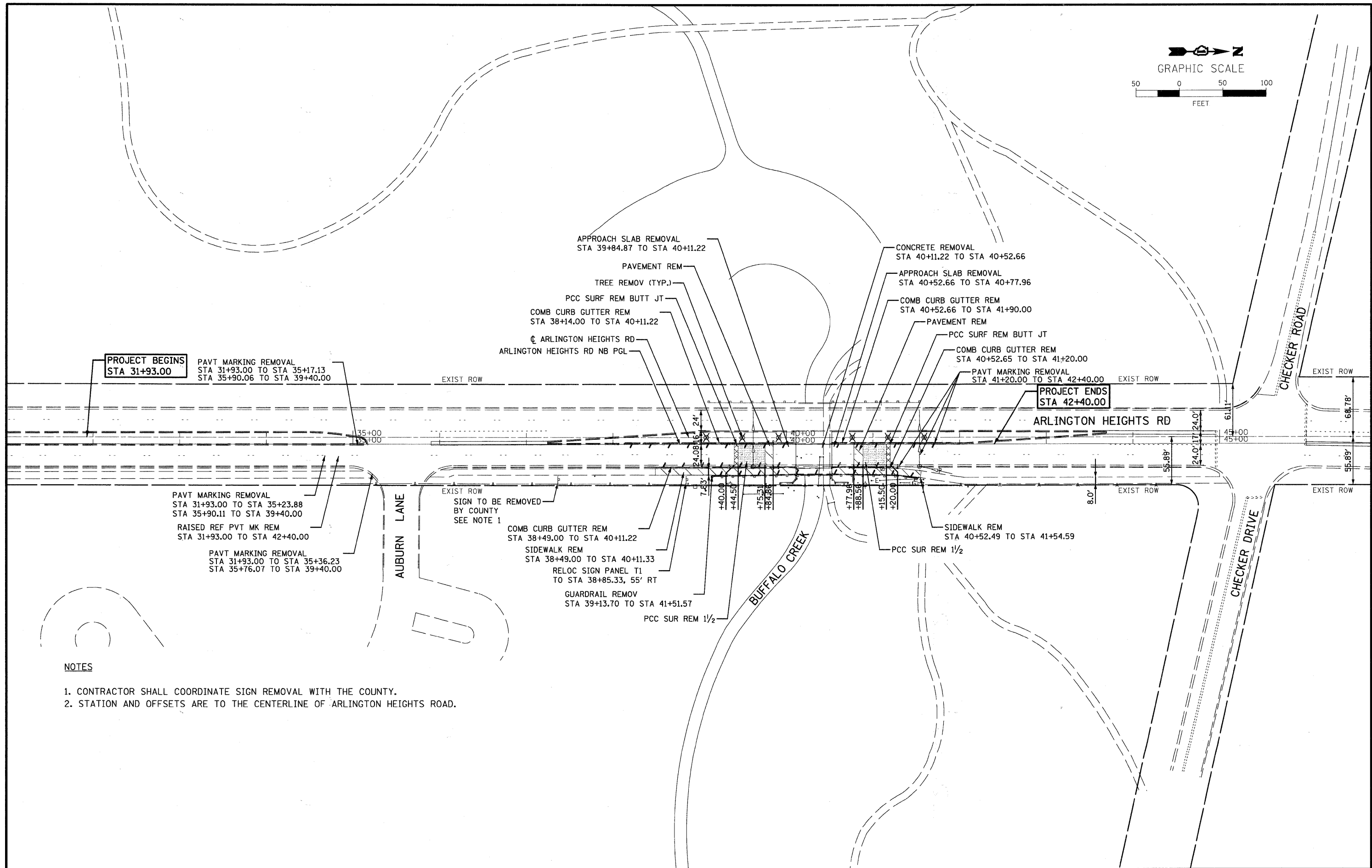
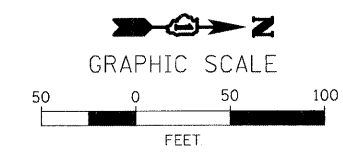
STANDARD IRON POST WITH BRONZE CAP STAMPED 1A 1925, LOCATED APPROXIMATELY 20 FEET NORTH OF AND APPROXIMATELY 53 FEET EAST OF THE INTERSECTION OF ARLINGTON HEIGHTS ROAD AND CHECKER ROAD.

ELEVATION = 695.85
(NGVD29 DATUM)

CONTROL POINTS COORDINATE TABLE				
POINT	NORTH	EAST	STATION	O/S
CP #505	2001208.1330	1078757.9150	44+40.72	37.80' RT
CP #513	1998820.6490	1078759.4370	20+55.24	39.86' RT
CP #514	1999162.2630	1078774.1840	23+94.86	54.53' RT
CP #515	1998813.4630	1078641.2200	20+46.03	78.36' LT



FILE NAME = LC0987-sht-ATB.dgn	USER NAME = 320200907	DESIGNED - TC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ALIGNMENT, TIES, AND BENCHMARKS STA. 31 + 93.00 TO STA 42 + 40.00	F.A.U. RTE. 2626	SECTION 05-00193-04-BR	COUNTY LAKE	TOTAL SHEETS 36	SHEET NO. 6		
PLOT SCALE = 1/80.0000" / IN.	CHECKED - MRH	REVISED -	SCALE: 1" = 50'			SHEET NO. 1 OF 1 SHEETS	STA. 31+93.00 TO STA. 42+40.00	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
PLOT DATE = 10/25/2010	DATE - 10/25/2010	REVISED -										
CONTRACT NO. 63531												



PROJECT BEGINS
STA 31+93.00

PROJECT ENDS
STA 42+40.00

PAVT MARKING REMOVAL
STA 31+93.00 TO STA 35+23.88
STA 35+90.11 TO STA 39+40.00
RAISED REF PVT MK REM
STA 31+93.00 TO STA 42+40.00
PAVT MARKING REMOVAL
STA 31+93.00 TO STA 35+36.23
STA 35+76.07 TO STA 39+40.00

EXIST ROW
SIGN TO BE REMOVED
BY COUNTY
SEE NOTE 1

COMB CURB GUTTER REM
STA 38+49.00 TO STA 40+11.22
SIDEWALK REM
STA 38+49.00 TO STA 40+11.33
RELOC SIGN PANEL T1
TO STA 38+85.33, 55' RT
GUARDRAIL REMOV
STA 39+13.70 TO STA 41+51.57
PCC SUR REM 1/2

CONCRETE REMOVAL
STA 40+11.22 TO STA 40+52.66
APPROACH SLAB REMOVAL
STA 40+52.66 TO STA 40+77.96
COMB CURB GUTTER REM
STA 40+52.66 TO STA 41+90.00
PAVEMENT REM
PCC SURF REM BUTT JT
COMB CURB GUTTER REM
STA 40+52.65 TO STA 41+20.00
PAVT MARKING REMOVAL
STA 41+20.00 TO STA 42+40.00

SIDEWALK REM
STA 40+52.49 TO STA 41+54.59
PCC SUR REM 1/2

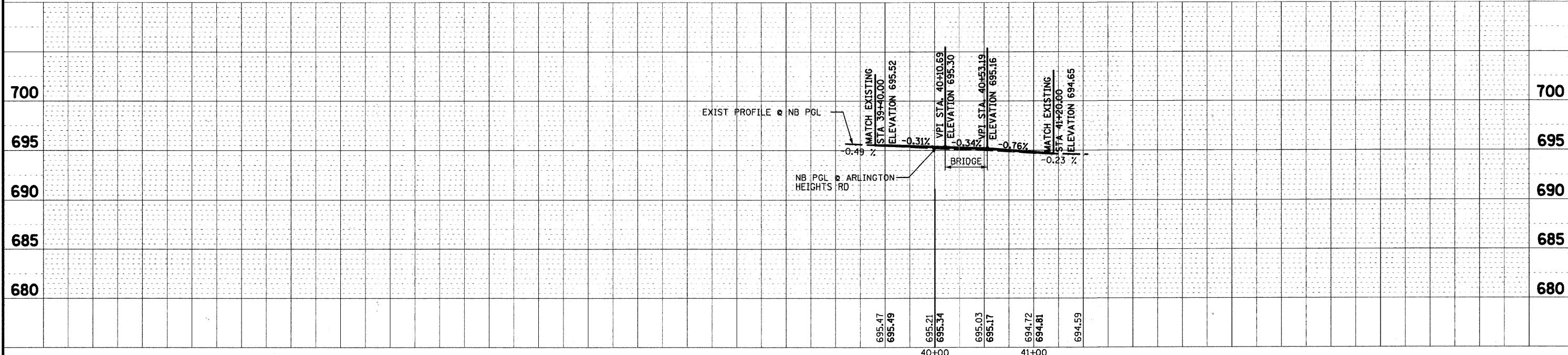
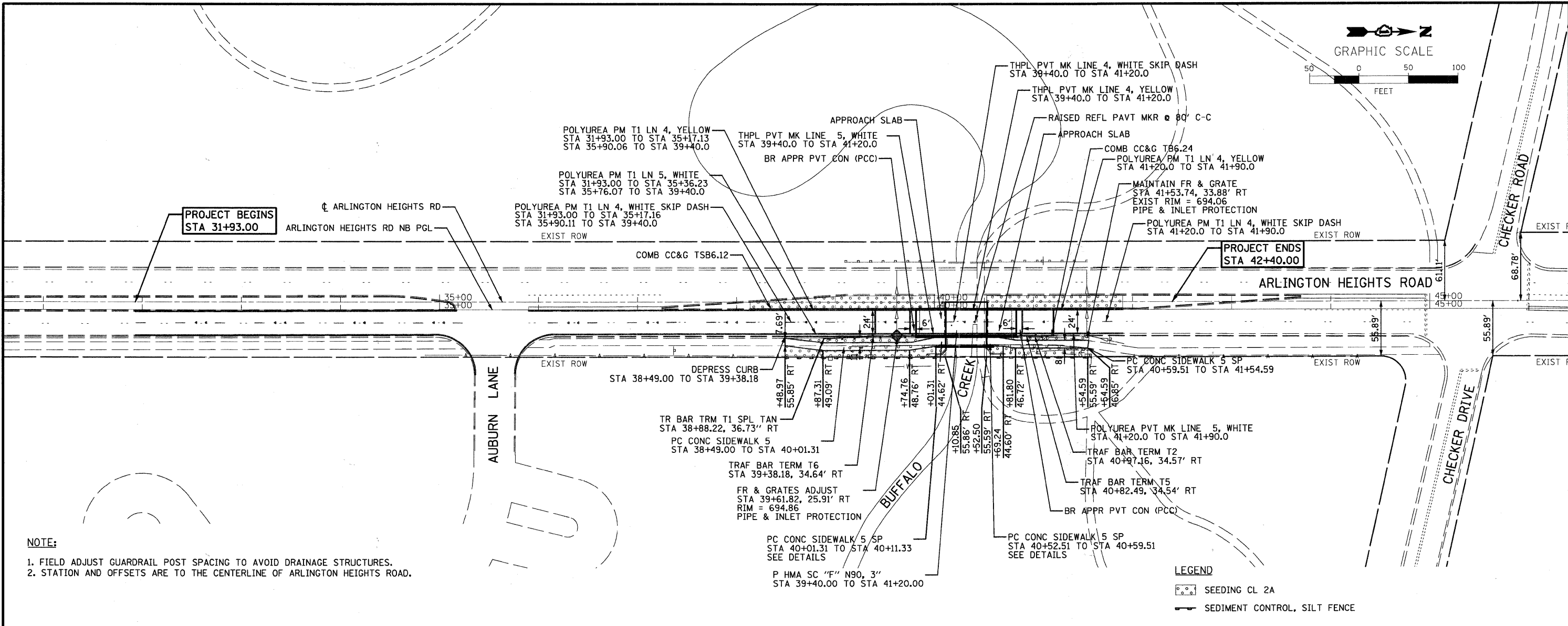
NOTES

1. CONTRACTOR SHALL COORDINATE SIGN REMOVAL WITH THE COUNTY.
2. STATION AND OFFSETS ARE TO THE CENTERLINE OF ARLINGTON HEIGHTS ROAD.

FILE NAME = LC0907-ah-removal.dgn	USER NAME = 3202980907	DESIGNED - TC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING AND REMOVAL PLAN STA. 31+93.00 TO STA. 42+40.00	F.A.U. RTE. 2626	SECTION 05-00193-04-BR	COUNTY LAKE	TOTAL SHEETS 36	SHEET NO. 7		
PLOT SCALE = 100.0000' / IN.	CHECKED - MRH	DATE - 11/2/2010	REVISED -			SCALE: 1" = 50'	SHEET NO. 1 OF 1 SHEETS	STA. 31+93.00 TO STA. 42+40.00	FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT			
PLOT DATE = 11/2/2010	DATE - 11/2/2010	REVISED -	REVISED -			CONTRACT NO. 63531						

DATE: _____
 BY: _____
 REVIEWED: _____
 PLOTTED: _____
 ALIGNMENT CHECKED: _____
 GRADES CHECKED: _____
 E.M. NOTED: _____
 STRUCTURE NOTATIONS CHECKED: _____
 NO. _____
 FILE NAME: _____

DATE: _____
 BY: _____
 REVIEWED: _____
 GRADES CHECKED: _____
 E.M. NOTED: _____
 STRUCTURE NOTATIONS CHECKED: _____
 NO. _____
 FILE NAME: _____



FILE NAME = LC0907-shr-plnprf.dgn

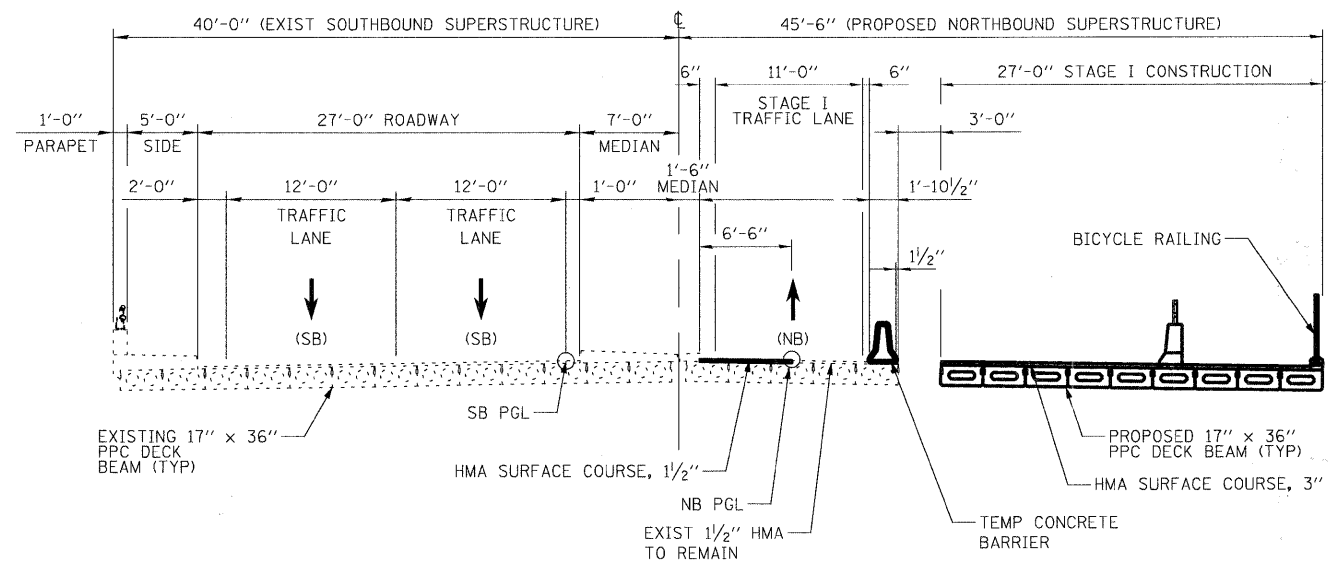
USER NAME = 32020007	DESIGNED - TC	REVISED -
PLOT SCALE = 100.0000' / IN.	DRAWN - PWL	REVISED -
PLOT DATE = 10/25/2010	CHECKED - MRH	REVISED -
	DATE - 10/25/2010	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

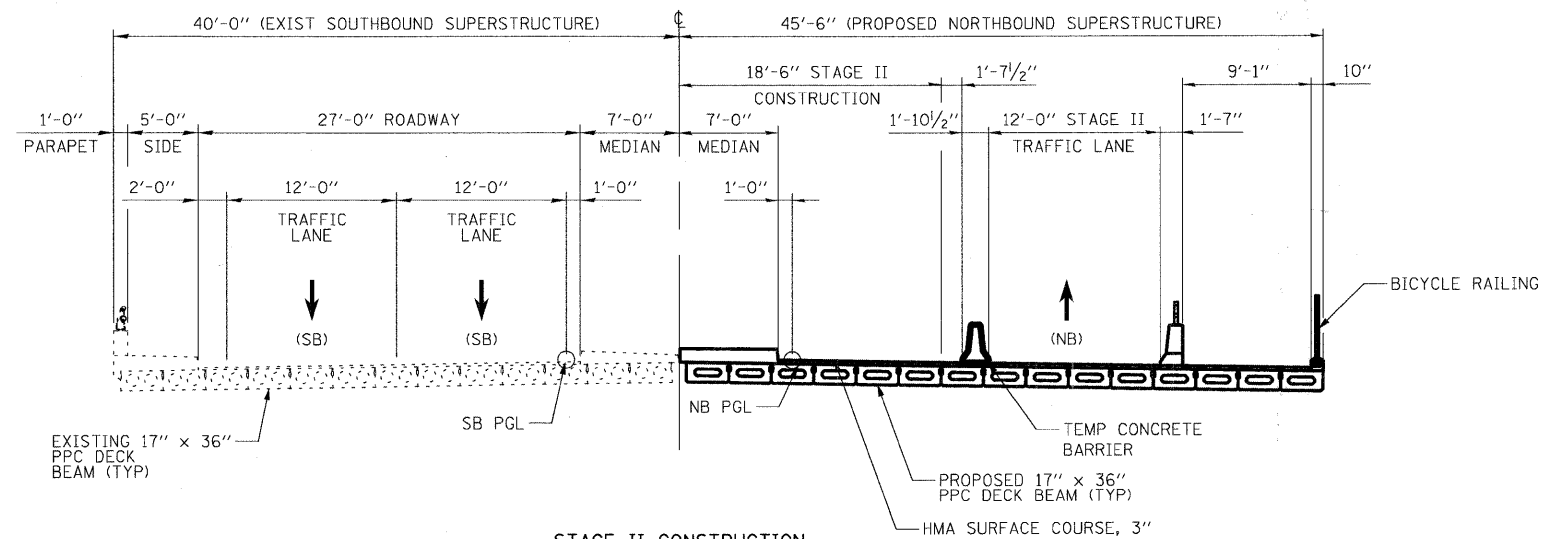
**PLAN AND PROFILE
 STA. 31 + 93.00 TO STA 42 + 40.00**

SCALE: 1" = 50' SHEET NO. 1 OF 1 SHEETS STA. 31+93.00 TO STA. 42+40.00

F.A.U. RTE. 2626	SECTION 05-00193-04-BR	COUNTY LAKE	TOTAL SHEETS 36	SHEET NO. 8
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT	CONTRACT NO. 63531	



STAGE I CONSTRUCTION
STA 38+14.00 TO STA 41+90.00



STAGE II CONSTRUCTION
STA 38+14.00 TO STA 41+90.00

FILE NAME =
LC0907-staging-typical.dgn

USER NAME = 320200907

DESIGNED - TC

REVISED -

DRAWN - PWL

REVISED -

PLOT SCALE = 20.0000' / IN.

CHECKED - MRH

REVISED -

PLOT DATE = 10/25/2010

DATE = 10/25/2010

REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC TYPICALS
STA. 31+93.00 TO STA 42+40.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2626	05-00193-04-BR	LAKE	36	9

SCALE: NTS | SHEET NO. 1 OF 1 SHEETS | STA. 31+93.00 TO STA. 42+40.00

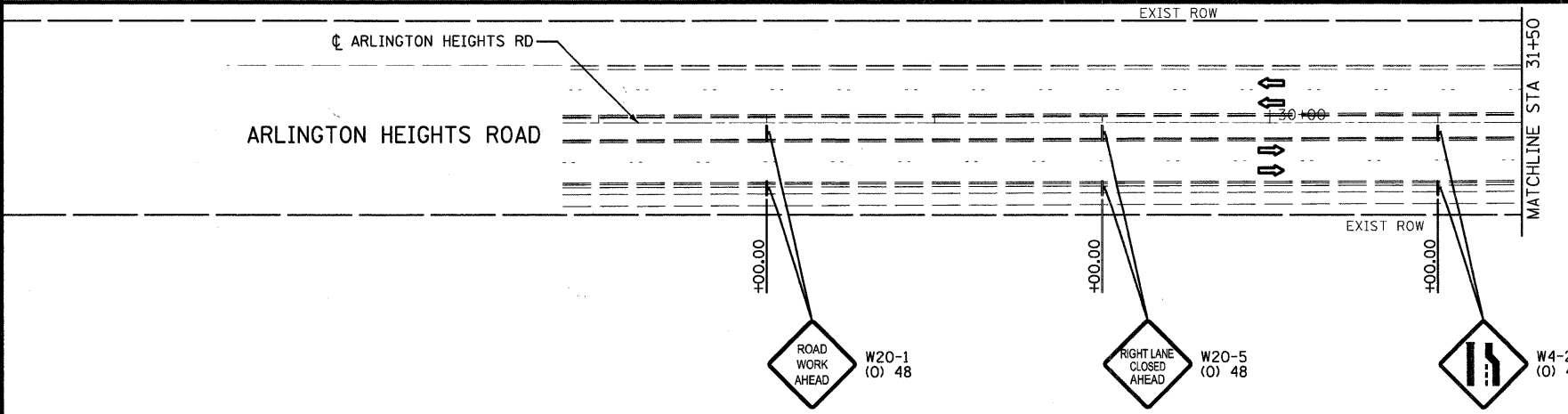
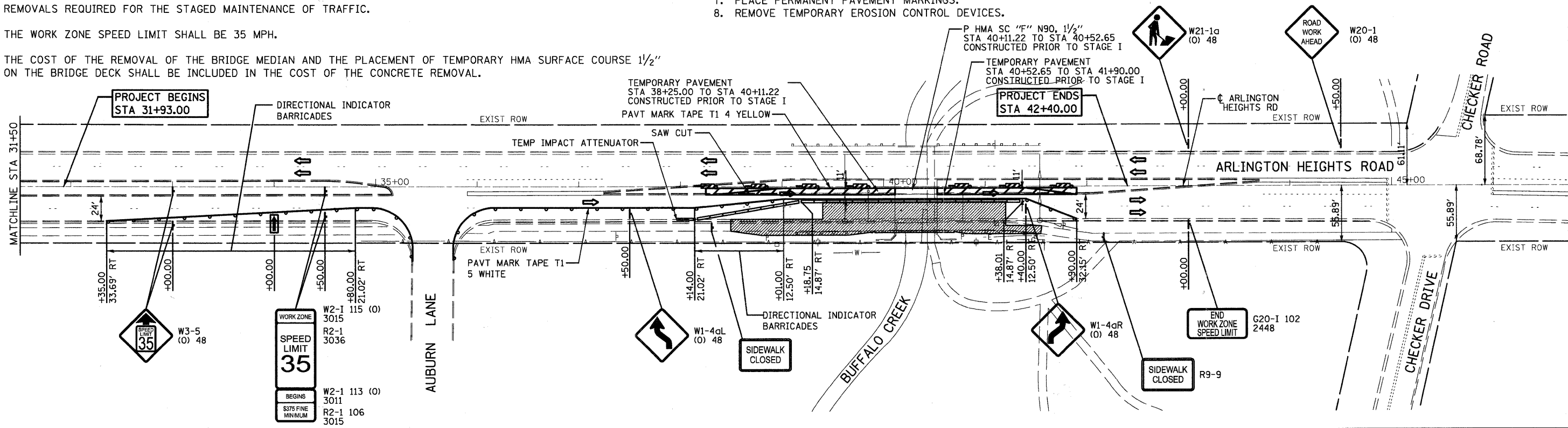
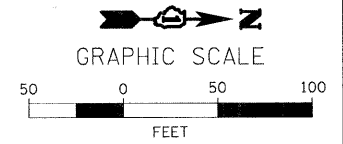
FED. ROAD DIST. NO. 1 | ILLINOIS FED. AID PROJECT | CONTRACT NO. 63531

MAINTENANCE OF TRAFFIC GENERAL NOTES

1. TRAFFIC CONTROL FOR STAGES I & II WILL BE PAID FOR AS A LUMP SUM, "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".
2. TRAFFIC CONDITIONS, ACCIDENTS, AND OTHER UNFORESEEN EMERGENCY CONDITIONS MAY REQUIRE THE ENGINEER TO RESTRICT, MODIFY OR REMOVE LANE CLOSURES OR CHANNELIZATION SHOWN IN THE PLANS. THE CONTRACTOR SHALL RESPOND PROMPTLY TO THE TIME OF NOTIFICATION BY THE ENGINEER FOR THE MAINTENANCE OF TRAFFIC CONTROL DEVICES.
3. SIDE STREETS SHALL REMAIN OPEN AT ALL TIMES. SPECIAL CONSIDERATION MAY BE GIVEN TO A SHORT TERM CLOSURE ON AS-NEEDED BASIS. THESE CLOSURES WILL BE COORDINATED WITH THE RESIDENT ENGINEER.
4. ALL COUNTY OWNED UTILITY STRUCTURES SHALL BE KEPT ACCESSIBLE AT ALL TIMES DURING CONSTRUCTION. STRUCTURES SHALL BE ADJUSTED TO MATCH THE TEMPORARY PAVEMENT AND GRADING ELEVATION. ALL TEMPORARY ADJUSTMENTS TO STRUCTURES REQUIRED AS A RESULT OF THE STAGED MAINTENANCE OF TRAFFIC WILL NOT BE PAID FOR, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE FINAL ADJUSTMENT ITEM.
5. EXCAVATION REQUIRED TO CONSTRUCT THE TEMPORARY PAVEMENT IN STAGE I SHALL BE MEASURED FOR PAYMENT AND SHALL BE PAID FOR AS "EARTH EXCAVATION". QUANTITIES FOR THIS WORK HAVE BEEN INCLUDED IN THE EARTHWORK ITEM FOR THE STAGE.
6. ALL OBSTRUCTIONS SHALL BE REMOVED PRIOR TO CONSTRUCTING THE TEMPORARY PAVEMENT AND SHIFTING TRAFFIC TO IT. REMOVALS SHALL BE DETAILED ON THE REMOVAL SHEET. REMOVAL SHALL BE PAID FOR ONCE AND SHALL BE PAID FOR AS INDICATED ON THE REMOVAL SHEET. THERE WILL BE NO ADDITIONAL COMPENSATION FOR PARTIAL REMOVALS REQUIRED FOR THE STAGED MAINTENANCE OF TRAFFIC.
7. THE WORK ZONE SPEED LIMIT SHALL BE 35 MPH.
8. THE COST OF THE REMOVAL OF THE BRIDGE MEDIAN AND THE PLACEMENT OF TEMPORARY HMA SURFACE COURSE 1 1/2" ON THE BRIDGE DECK SHALL BE INCLUDED IN THE COST OF THE CONCRETE REMOVAL.

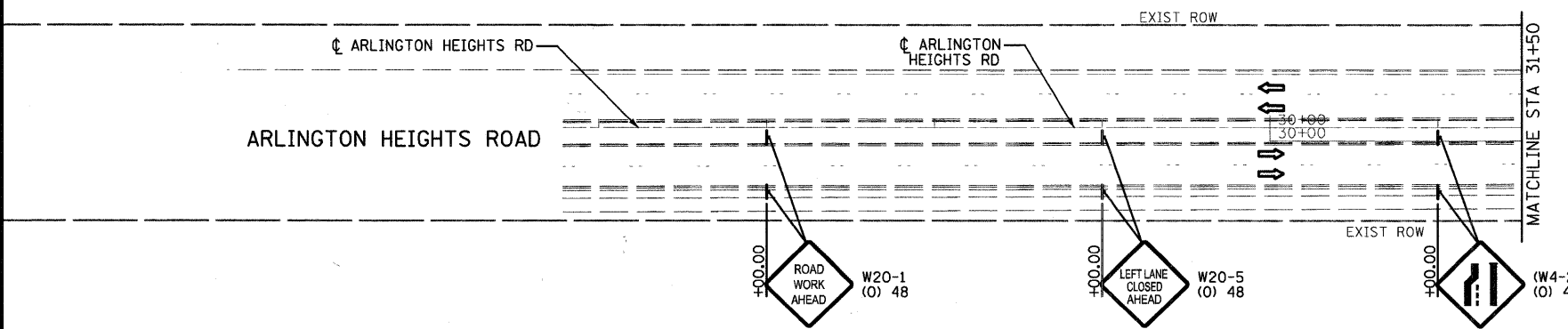
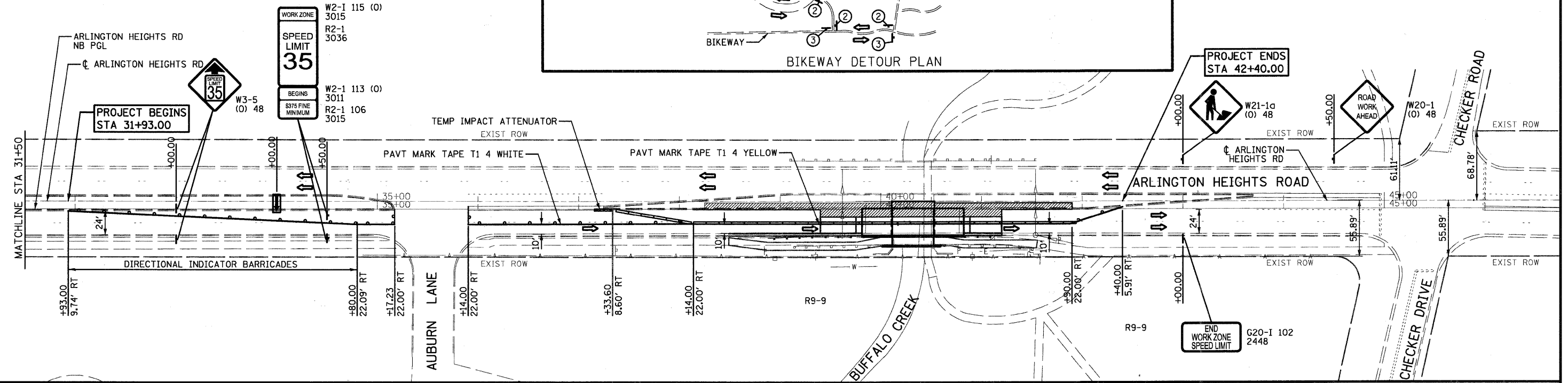
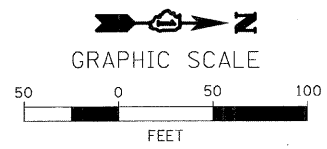
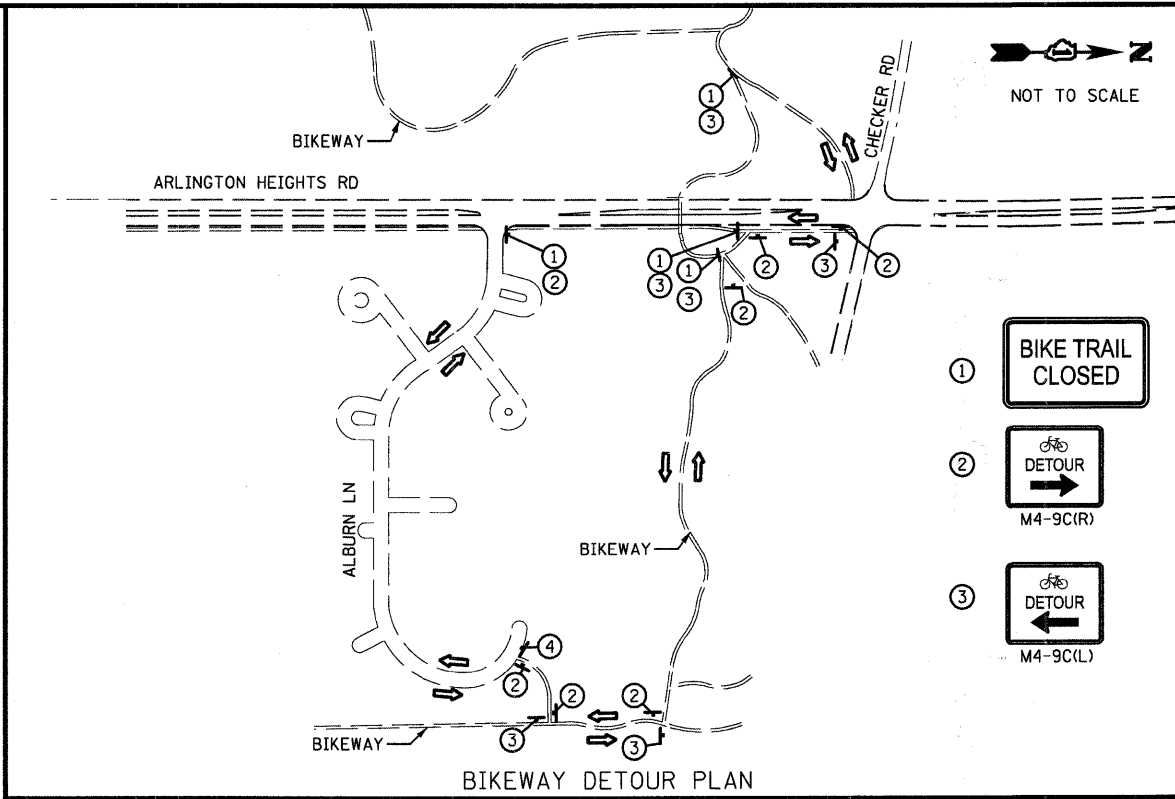
SEQUENCE OF CONSTRUCTION

- STAGE I**
1. INSTALL EROSION CONTROL DEVICES USING STANDARD 701101.
 2. INSTALL TRAFFIC CONTROL DEVICES ALONG THE WEST SIDE OF THE ROADWAY. REMOVE ALL CONFLICTING EXISTING PAVEMENT MARKINGS AND SIGN USING COUNTY STANDARD LC7002.
 3. REMOVE CURB AND GUTTER AND BRIDGE MEDIAN USING COUNTY STANDARD LC7002.
 4. CONSTRUCT TEMPORARY PAVEMENT FROM STA 38+25.00 TO STA. 41+90.00 USING COUNTY STANDARD LC7002.
 5. INSTALL BIKEWAY DETOUR PLAN AS SHOWN ON SHEET 11.
 6. INSTALL STAGE I TRAFFIC CONTROL.
 7. SHIFT TRAFFIC TO WEST SIDE OF ROADWAY TO UTILIZE THE EXISTING AND TEMPORARY PAVEMENT.
 8. REMOVE THE EXISTING CURB AND GUTTER AND THE BIKEWAY ON THE EAST SIDE.
 9. CONSTRUCT THE APPROACH SLAB, BRIDGE DECK BEAMS, AND PARAPET.
- STAGE II**
1. MAINTAIN BIKEWAY DETOUR FOR TRAIL UNDERNEATH BRIDGE AS SHOWN ON SHEET 11. REMOVE ALL CONFLICTING EXISTING PAVEMENT MARKINGS AND SIGNS. INSTALL STAGE II TRAFFIC CONTROL PLAN.
 2. REMOVE TEMPORARY PAVEMENT ON THE WEST SIDE OF THE ROAD CONSTRUCTED IN STAGE I.
 3. SHIFT TRAFFIC TO THE EAST SIDE TO UTILIZE THE EXISTING PAVEMENT AND BRIDGE DECK BEAMS CONSTRUCTED IN STAGE I.
 4. PRIOR TO REMOVAL OF THE EXISTING BRIDGE DECK BEAMS ON THE WEST SIDE INSTALL TEMPORARY CONCRETE BARRIER ALONG THE SOUTHBOUND BRIDGE DECK.
 5. CONSTRUCT THE APPROACH SLAB, BRIDGE DECK BEAMS, AND CURB AND GUTTER AND RESTORE THE MEDIAN ON THE WEST SIDE.
 6. REMOVE BIKEWAY DETOUR PLAN.
 7. PLACE PERMANENT PAVEMENT MARKINGS.
 8. REMOVE TEMPORARY EROSION CONTROL DEVICES.



- LEGEND**
- CONSTRUCTION ZONE
 - TEMPORARY HMA PAVEMENT
TEMPORARY PAVEMENT (HMA BINDER IL-19MM), 6"
TEMPORARY AGGREGATE BASE COURSE, TYPE B, 4"
 - TEMPORARY CONCRETE BARRIER
 - TEMPORARY IMPACT ATTENUATOR
 - VERTICAL SIGN PANELS (GROUND MOUNTED)
 - BARRICADE TYPE II WITH MONO DIRECTIONAL STEADY BURNING LIGHT
 - FLASHING ARROW BOARD

FILE NAME = LC0907-sht-staging-1.dgn	USER NAME = 3202000907	DESIGNED - TC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	MAINTENANCE OF TRAFFIC PLAN STAGE I STA. 38+14.00 TO STA 41+90.00	F.A.U. RTE. 2626	SECTION 05-00193-04-BR	COUNTY LAKE	TOTAL SHEETS 36	SHEET NO. 10		
PLOT SCALE = 100.0000' / IN.	CHECKED - MRH	DATE - 10/25/2010	REVISED -			SCALE: 1" = 50'	SHEET NO. 1 OF 1 SHEETS	STA. 38+14.00	TO STA. 41+90.00	CONTRACT NO. 63531		
PLOT DATE = 10/25/2010	DATE - 10/25/2010	REVISED -	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						

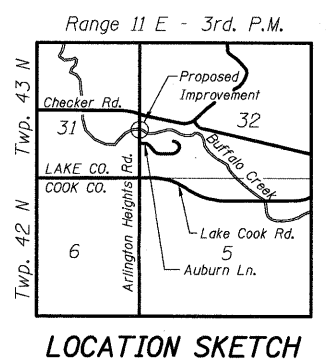


- LEGEND**
- CONSTRUCTION ZONE
 - TEMPORARY HMA PAVEMENT
 - TEMPORARY CONCRETE BARRIER
 - TEMPORARY IMPACT ATTENUATOR
 - VERTICAL SIGN PANELS (GROUND MOUNTED)
 - DRUM WITH MONO DIRECTIONAL STEADY BURNING LIGHT

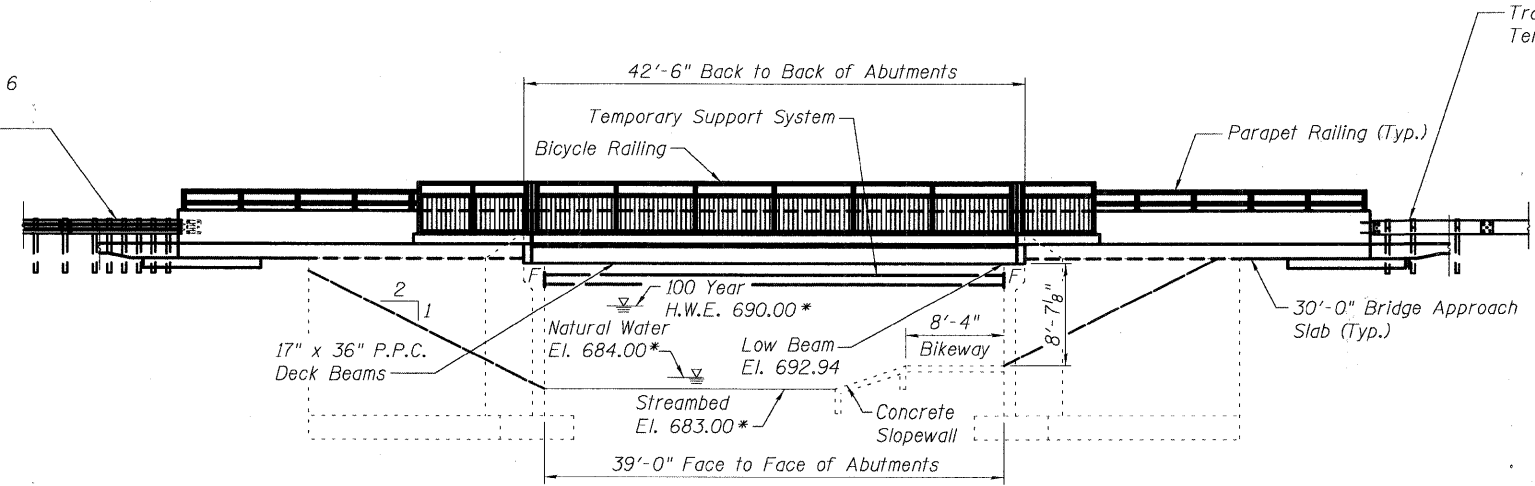
NOTES

1. CONTRACTOR SHALL COORDINATE WITH THE LAKE COUNTY FOREST PRESERVE PRIOR TO INSTALLING BIKEWAY DETOUR PLAN

FILE NAME = LC0907-sht-staging-11.dgn	USER NAME = 3202000907	DESIGNED - TC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	MAINTENANCE OF TRAFFIC PLAN STAGE II STA. 31 + 93.00 TO STA. 42 + 40.00			F.A.U. RTE. 2626	SECTION 05-00193-04-BR	COUNTY LAKE	TOTAL SHEETS 36	SHEET NO. 11
	PLOT SCALE = 1/8" = 1' IN.	DRAWN - PWL	REVISED -		SCALE: 1" = 50'	SHEET NO. 1 OF 1 SHEETS	STA. 31+93.00 TO STA. 42+40.00	CONTRACT NO. 63531				
	PLOT DATE = 10/25/2010	CHECKED - MRH	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
		DATE - 10/25/2010	REVISED -									



Traffic Barrier Terminal Type 6
(Attached to parapet in front
of bicycle railing)



Traffic Barrier
Terminal Type 5

Bench Mark:
FEMA Reference Mark RM261 - 1
(Effective Date: September 3, 1997)

Standard iron post with bronze cap stamped 1A 1925, located approximately 20 feet north of and approximately 53 feet east of the intersection of Arlington Heights Road and Checker Road.

Existing Structure:
S.N. 049-3040 (NB) & 049-3055 (SB) originally constructed in 1969 and widened in 1980. The single span superstructure consists of 17" precast, prestressed concrete (P.P.C.) deck beams. The substructure consists of reinforced concrete closed abutments on spread footings. The northbound traffic lane side of the bridge is the original structure, while the southbound side is from the 1980 widening. The structure measures 42'-6" from back to back of abutment and 86'-0" out to out of deck. The existing northbound superstructure is to be removed and replaced. One lane of northbound traffic will be maintained by using staged construction.

Salvage: No salvage

EXISTING WATERWAY INFORMATION

Drainage Area =	16.25 mi
Design Discharge (100 yr) =	1,000 c.f.s.
High Water Elevation (100 yr) =	690.0 *
Existing Opening (below H.W.E.) =	273 ft ²
Proposed opening (below H.W.E.) =	250 ft ²
Created Head =	Negligible

* These elevations were taken off the existing bridge plans and may not coincide with the current profile. They were included for background information only.

PROPOSED SCOPE OF WORK

1. Remove existing northbound superstructure, including bearing pads, and replace in-kind with new standard 17" x 36" P.P.C. deck beams, a 3" HMA wearing surface and waterproof membrane system.
2. Replace raised concrete curb with F-shaped concrete parapet with railing and replace outside concrete parapet with bicycle railing.
3. Remove existing 20'-0" approach slabs and replace with new IDOT standard 30'-0" bridge approach slabs.
4. Repair cracks in abutments with epoxy crack injection where necessary.
5. Repair deteriorated concrete with formed concrete repair where necessary.
6. Transition pavement back to existing grade by tapering asphalt.
7. Construction will be staged to maintain one lane of traffic in the northbound direction.

DESIGN SPECIFICATIONS

2007 AASHTO LRFD Bridge Design Specifications with 2008 and 2009 Interims

LOADING HL-93

No future wearing surface allowed

DESIGN STRESSES

FIELD UNITS (EXISTING)

$f'_c = 3,500$ psi (Substructure)
 $f_s = 20,000$ psi (Substructure)

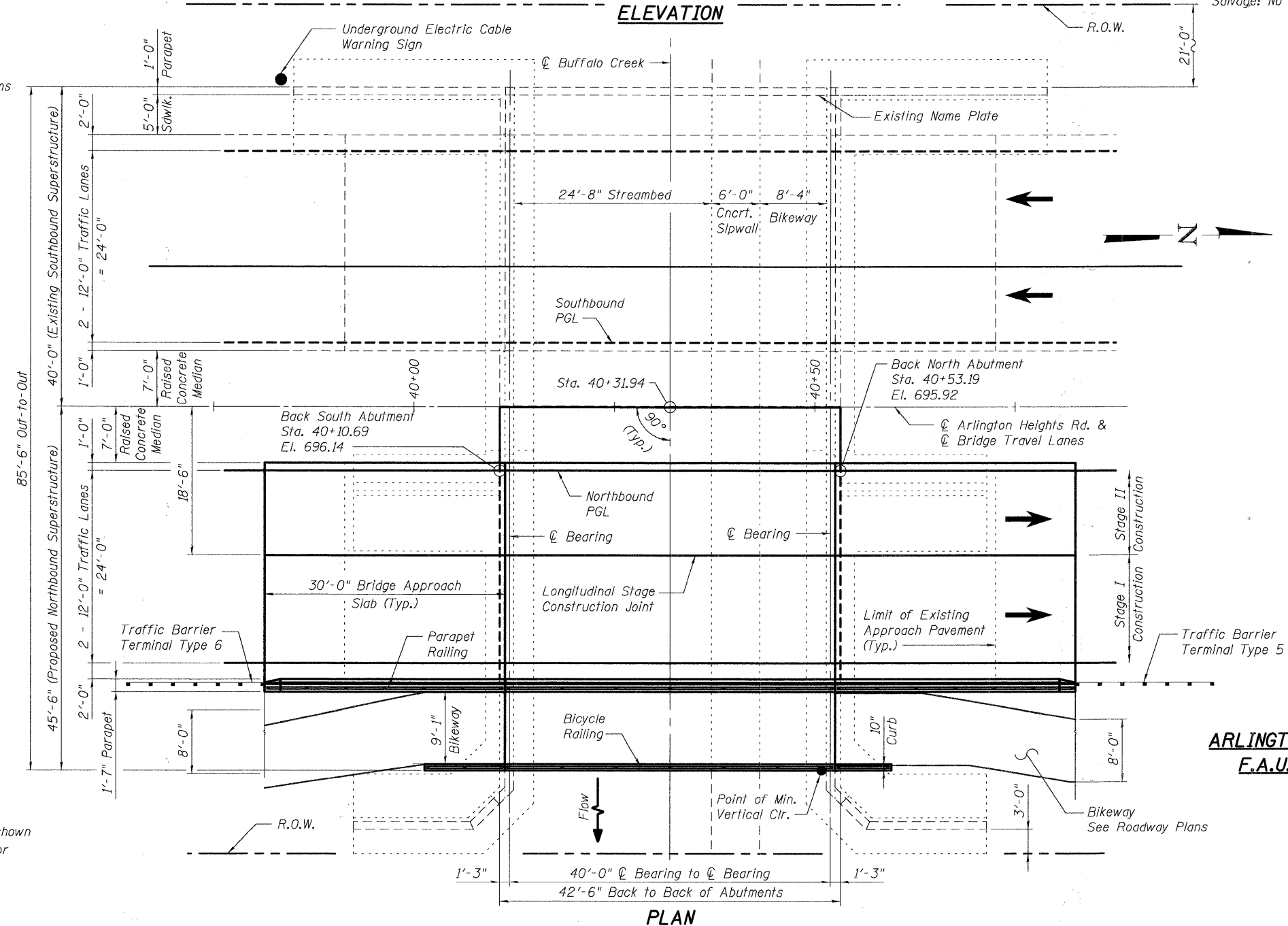
FIELD UNITS (PROPOSED)

$f'_c = 3,500$ psi
 $f_y = 60,000$ psi (Reinforcement)

PRECAST PRESTRESSED UNITS

$f'_c = 6,000$ psi
 $f'_{ci} = 5,000$ psi
 $f_{pu} = 270,000$ psi ($\frac{1}{2}$ " ϕ low lax. strands)
 $f_{pbl} = 201,960$ psi ($\frac{1}{2}$ " ϕ low lax. strands)

ELEVATION



PLAN

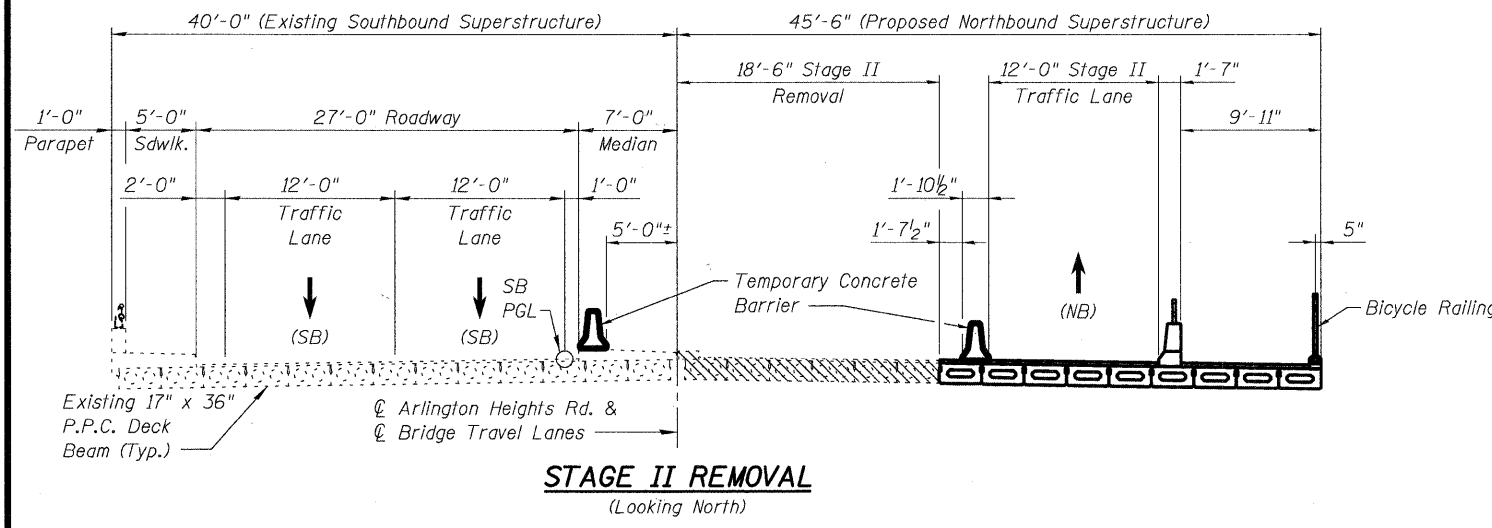
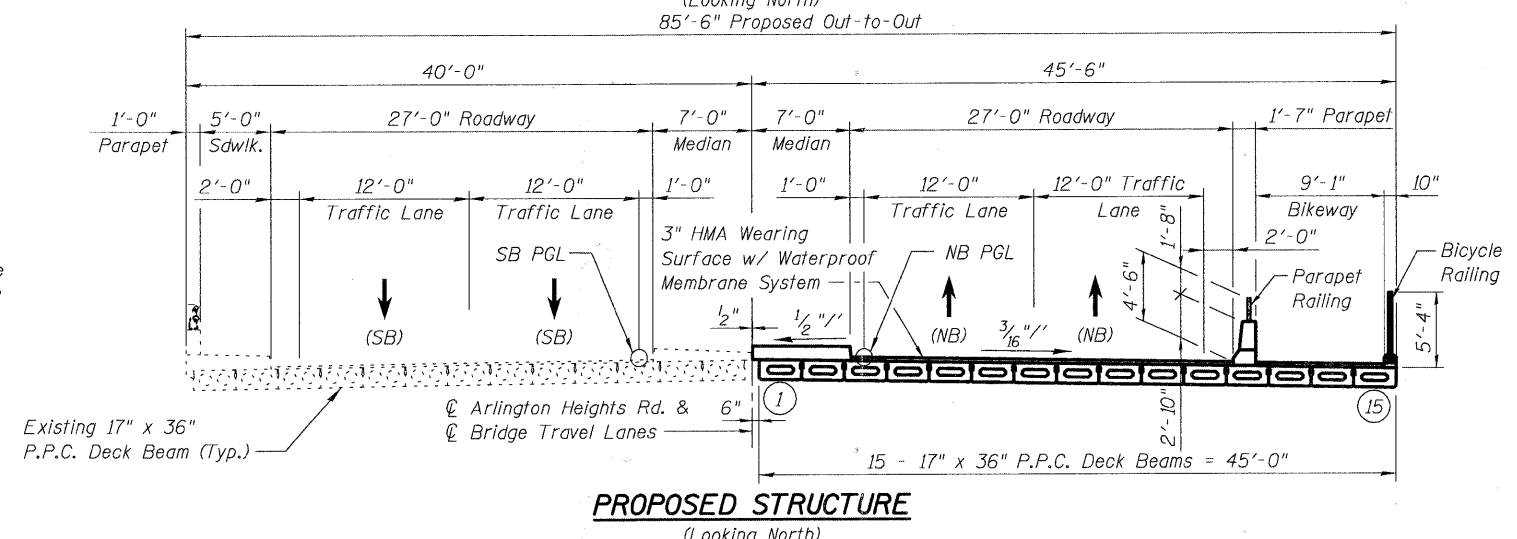
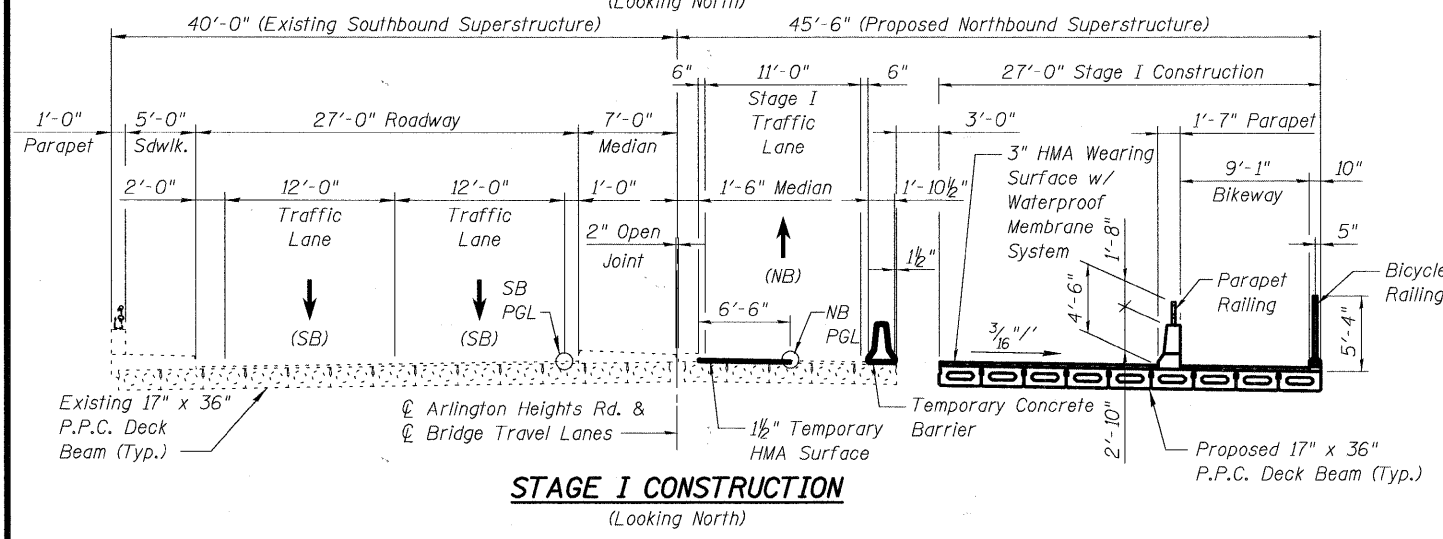
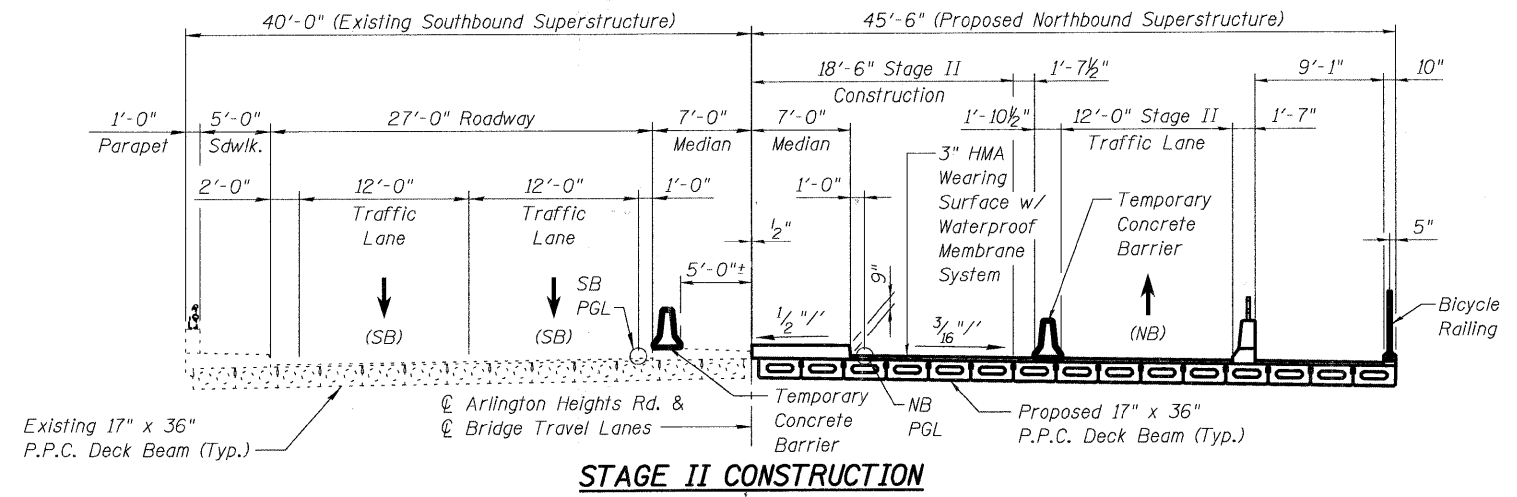
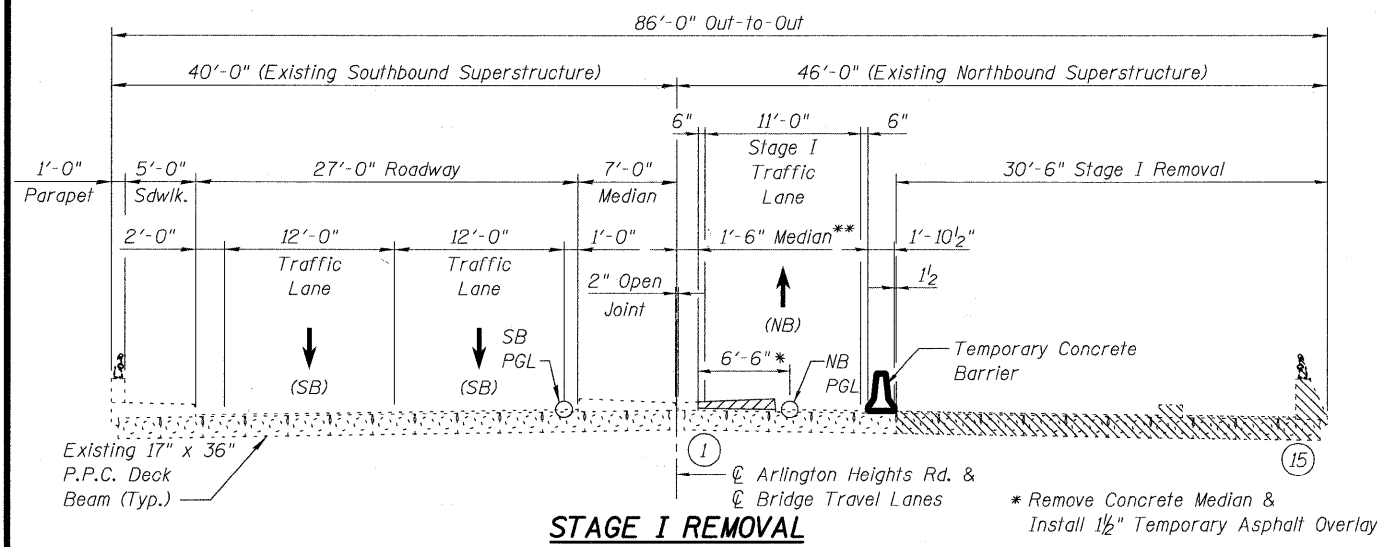
William Malinowski
EXPIRATION DATE: 11/30/2010
DATE: 10/26/2010
DRAWING NO.: 12 - 26

I certify that to the best of my knowledge, information and belief, this bridge design is structurally adequate for the design loading shown on plans. The design is an economical one for the style of structure and complies with requirements of the current AASHTO LRFD Bridge Design Specifications.

GENERAL PLAN & ELEVATION
ARLINGTON HEIGHTS ROAD OVER BUFFALO CREEK
F.A.U. R.T.E. 2626 - SEC. 05-00193-04-BR
LAKE COUNTY
STATION 40+34.94
STRUCTURE NO. 049-3040



FILE NAME = 0493040-63531-001-GPE.dgn	USER NAME = 320200907	DESIGNED - KO	REVISD -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL PLAN AND ELEVATION STRUCTURE NO. 049-3040 (NB)	F.A.U. R.T.E. = 2626	SECTION = 05-00193-04-BR	COUNTY = LAKE	TOTAL SHEETS = 36	SHEET NO. = 12
PLOT SCALE = 20.0000' / IN.	DRAWN - KO	CHECKED - WPM	REVISD -			CONTRACT NO. 63531				
PLOT DATE = 11/2/2010	DRAWN - KO	CHECKED - WPM	REVISD -			ILLINOIS FED. AID PROJECT				
	CHECKED - WPM	CHECKED - WPM	REVISD -							



Legend

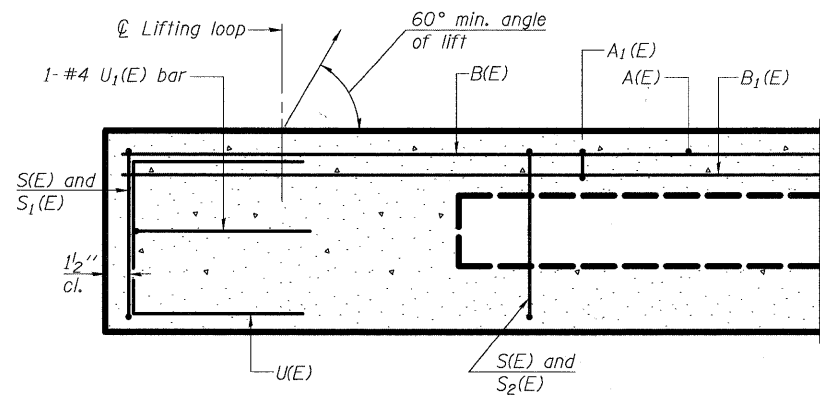
- Limits of Removal of Existing Superstructure
- Limits of Concrete Removal

** Prior to Stage I Removal, a pre-construction traffic stage is necessary for the removal of 5'-5" of the northbound median. Type II barricades shall be used to maintain one lane of northbound traffic during this removal. Barricades may be adjusted in the field to facilitate median removal, as long as minimum stage construction traffic lane widths are maintained. The removal of the median and placement of Temporary Hot-Mix Asphalt shall be included in the cost of Concrete Removal.

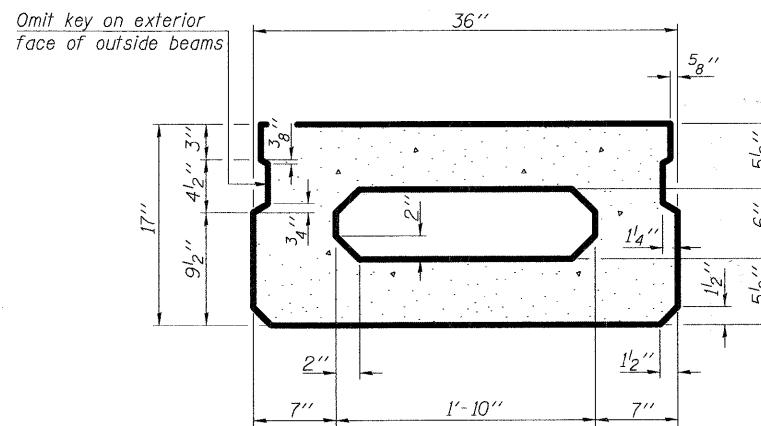
TOTAL BILL OF MATERIAL

ITEM	UNIT	Quantity
Removal of Existing Superstructure	Each	1
Concrete Removal	Cu. Yd.	7.6

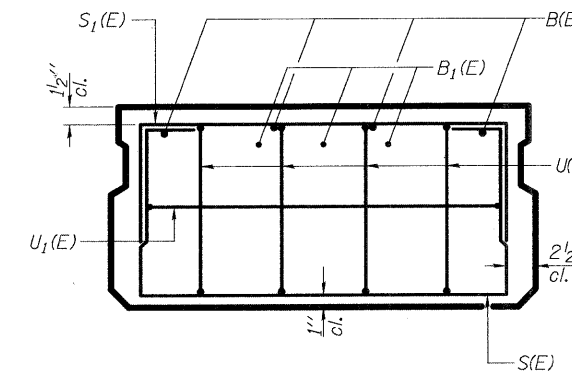




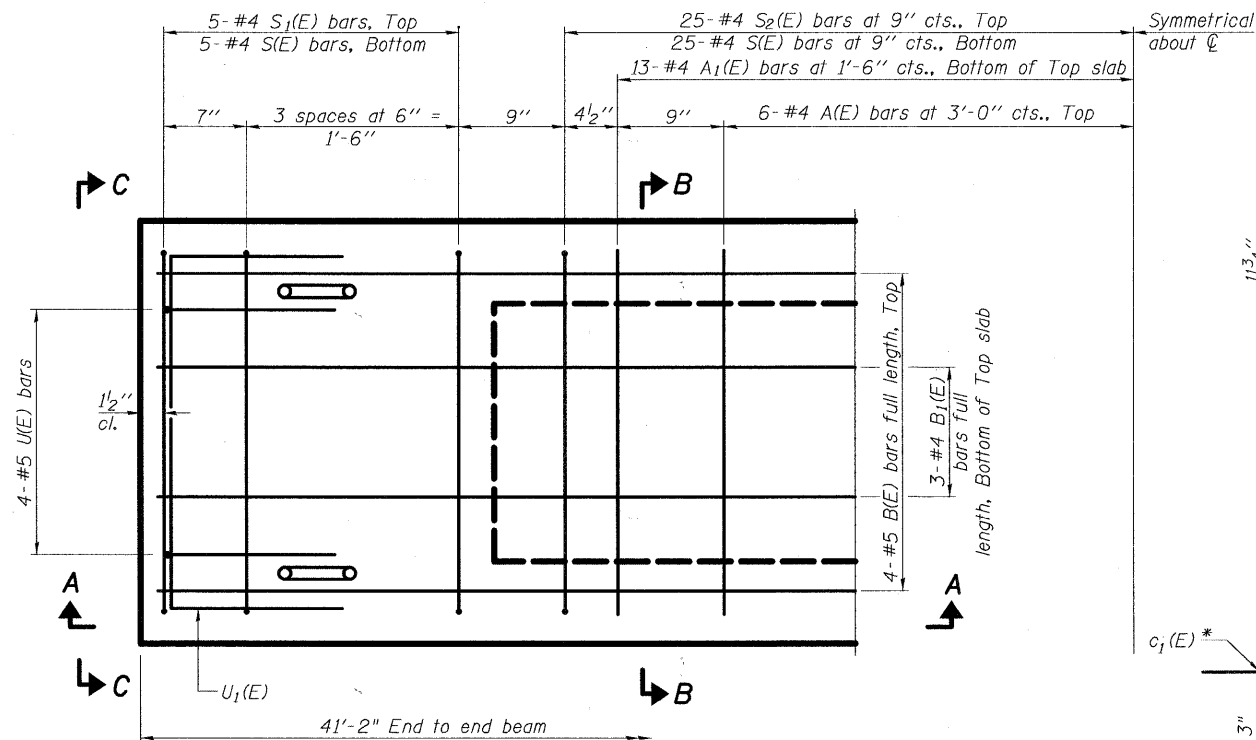
SECTION A-A



SECTION B-B
(Showing dimensions)



VIEW C-C



PLAN VIEW

BAR LIST
ONE BEAM ONLY
(For information only)

Bar	No.	Size	Length	Shape
A(E)	12	#4	2'-7"	
A1(E)	26	#4	2'-10"	
B(E)	4	#5	40'-11"	
B1(E)	3	#4	40'-11"	
S(E)	60	#4	5'-9"	
S1(E)	10	#4	4'-3"	
S2(E)	50	#4	4'-6"	
U(E)	8	#5	3'-8"	
U1(E)	2	#4	5'-0"	

Note: See sheet 5 of 15 for additional details and Bill of Material.

Note: Spacing of S(E) and S2(E) bars may be adjusted up to 4" in the immediate area of the transverse tie diaphragms to miss the block outs for the transverse ties.

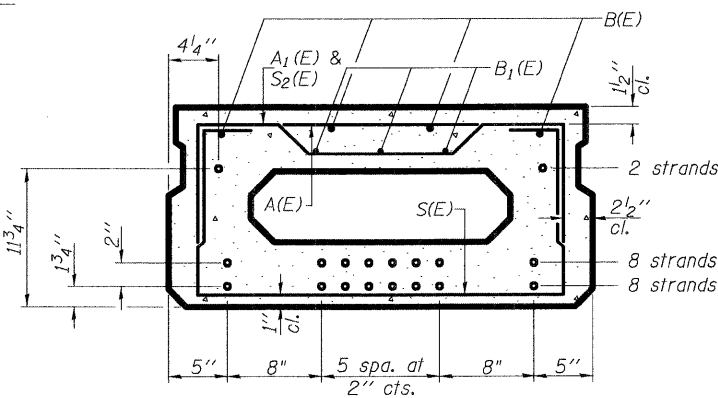
MINIMUM BAR LAP

#4 bar = 2'-0"
#5 bar = 2'-6"

ADDITIONAL BARS *
(For information only)

Beam	Bar	No.	Size	Length	Shape
Beam 1	c2(E)	42	#5	2'-9"	L
Beam 3	c1(E)	42	#5	2'-4"	L
Beam 12	d1(E)	46	#5	5'-10"	L
Beam 15	d2(E)	42	#5	4'-7"	L

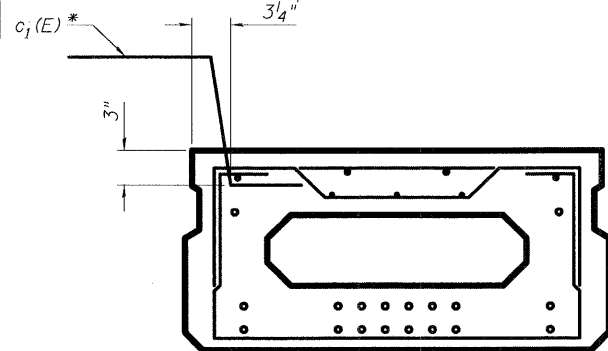
Symmetrical about C



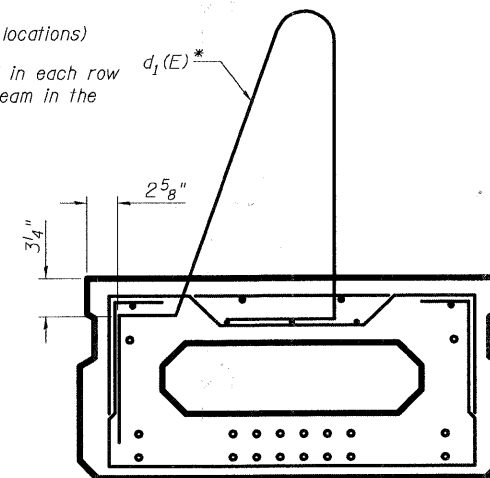
SECTION B-B

(Showing reinforcement and permissible strand locations)

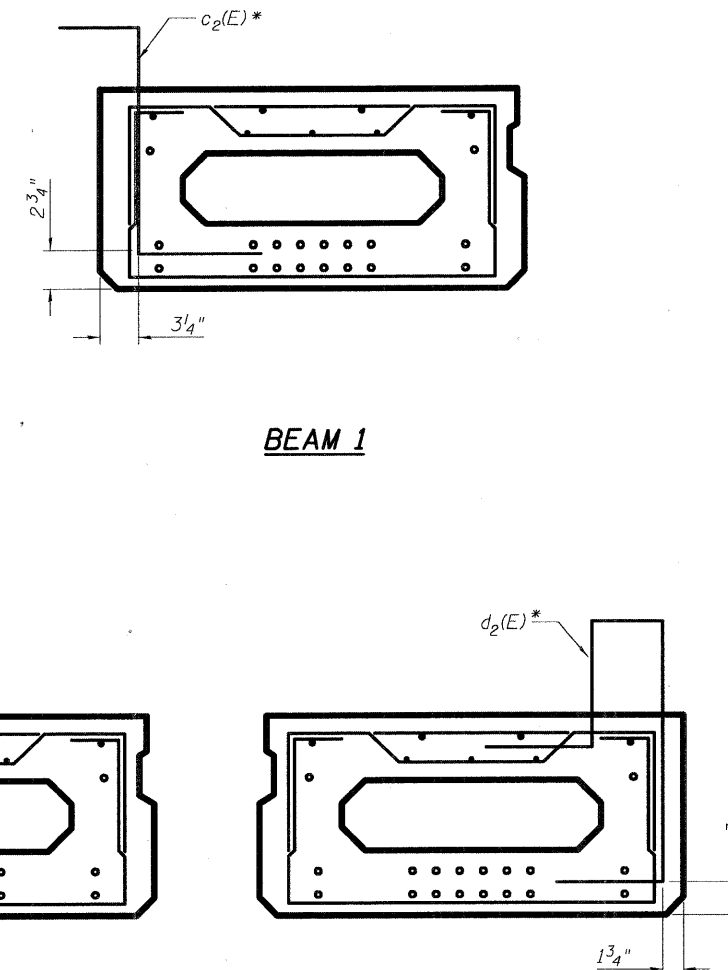
Note: Place the number of strands specified in each row symmetrically about the centerline of beam in the permissible strand locations shown.



BEAM 3



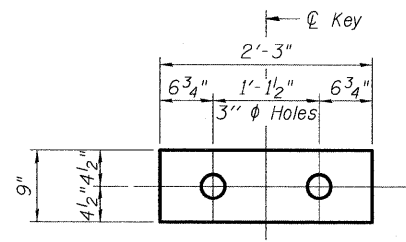
BEAM 12



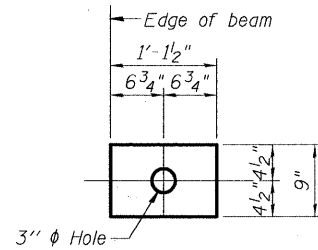
BEAM 15

* These bars are additional bars required for Beams 1, 3, 12, and 15 as shown above, which is why they are not included in the "Bar List One Beam Only" table Above. The bars are to be included in the cost of "Precast Prestressed Concrete Deck Beams (17" depth)".





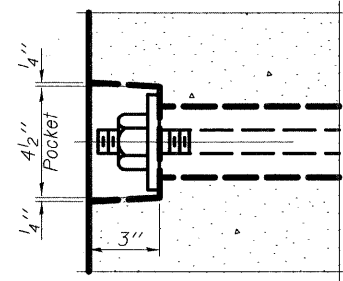
FABRIC BEARING PAD
(Interior)



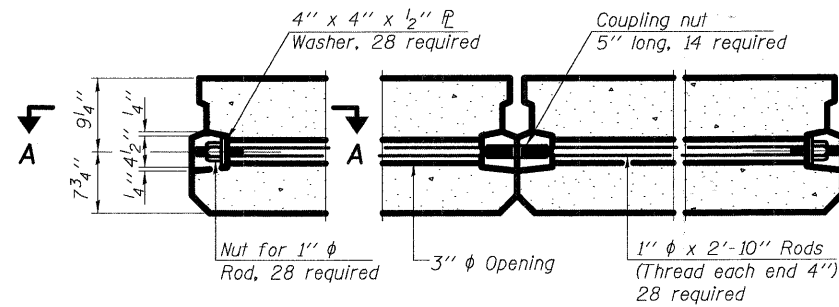
FABRIC BEARING PAD
(Exterior)

FIXED

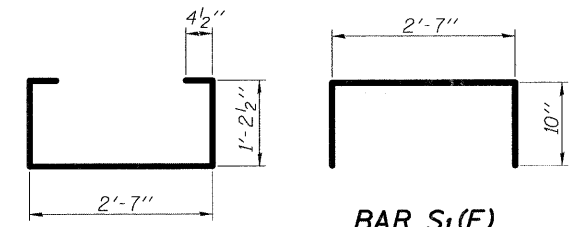
Notes:
All bearing pads shall be 1" thick.



SECTION A-A

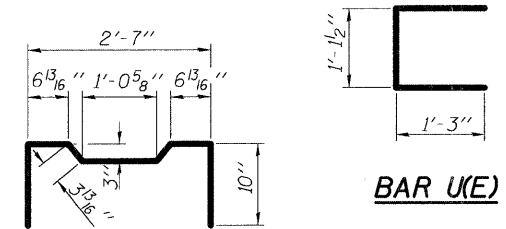


TYPICAL TRANSVERSE TIE ASSEMBLY

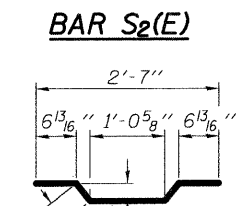


BAR S(E)

BAR S₁(E)



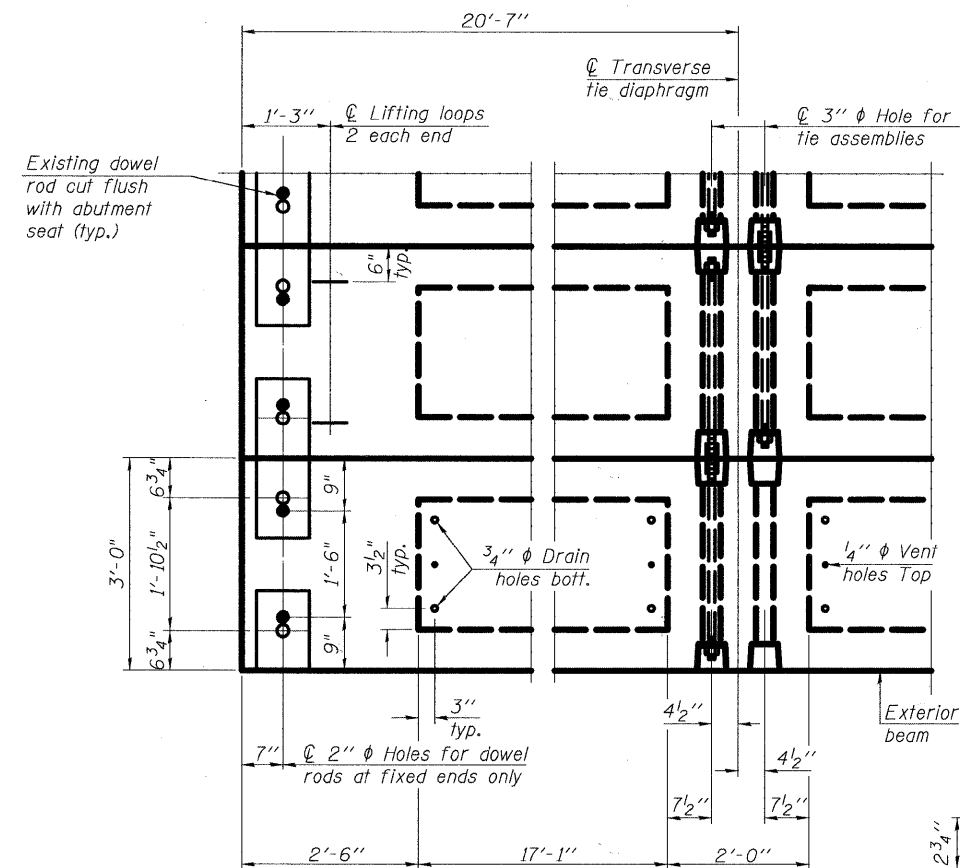
BAR U(E)



BAR S₂(E)

BAR U₁(E)

BAR A₁(E)



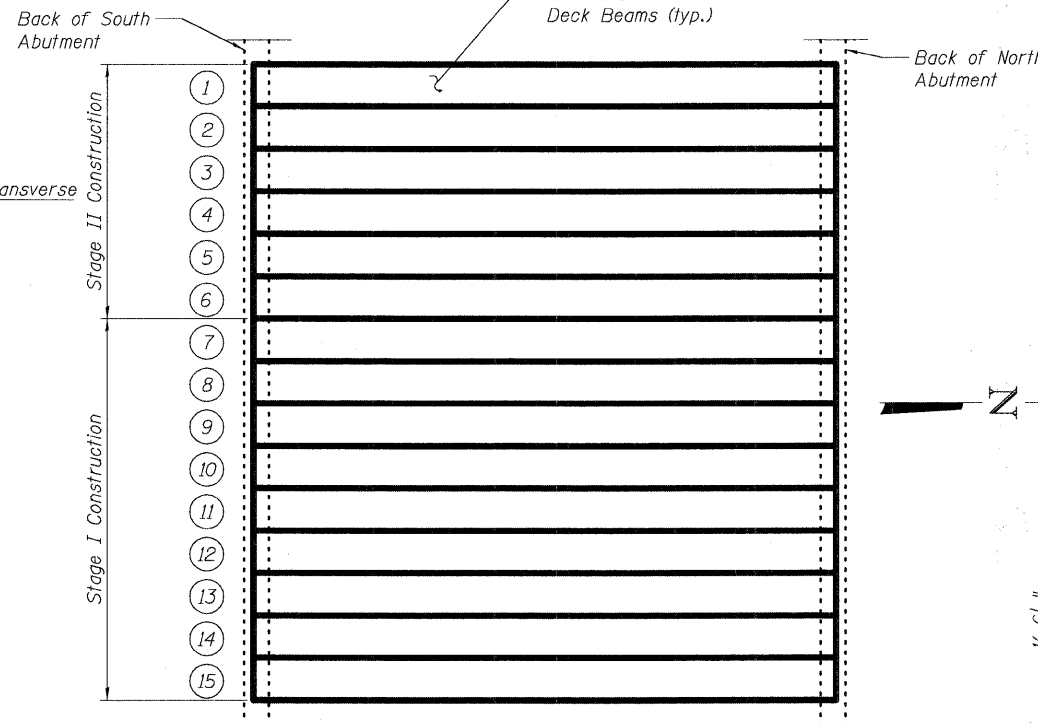
PLAN VIEW

Note: Verify existing 1" diameter dowel spacing and existing dimensions in field prior to ordering 17" x 36" PPC deck beams.

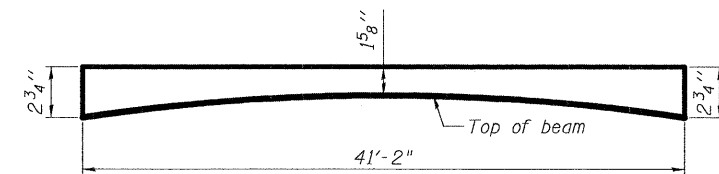
Connect beams in pairs with the transverse tie configuration shown.

BILL OF MATERIAL

Precast Prestressed Conc. Deck Bms. (17" depth)	Sq. Ft.	1853
---	---------	------



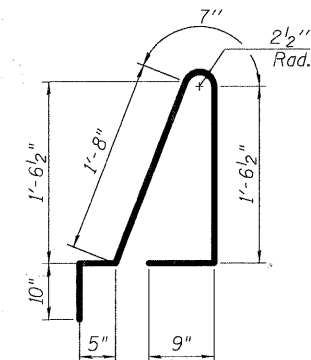
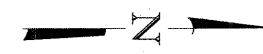
FRAMING PLAN



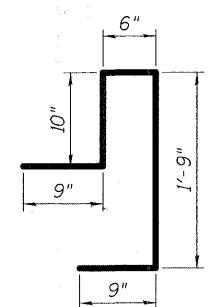
ANTICIPATED CONCRETE WEARING SURFACE PROFILE
(For information only)

NOTES

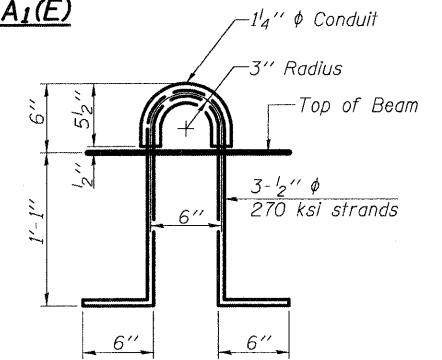
Prestressing steel shall be uncoated high strength, low relaxation 7-wire strand, Grade 270. The nominal diameter shall be 1/2" and the nominal cross-sectional area shall be 0.153 sq. in. The 1" diameter rods in the transverse tie assembly shall be tightened to a snug fit and the threads set. Pockets on exterior faces of bridge shall be filled with grout after transverse tie assembly is in place. Reinforcement bars shall conform to ASTM A 706, Grade 60. (See Special Provisions). Two 1/8" fabric adjusting shims of the dimensions of the exterior bearing pad shall be provided for each bearing pad location. A minimum 2 1/2" diameter lifting pin shall be used to engage the lifting loops during handling. Corrosion Inhibitor, per Article 1020.05(b)(12) and 1021.06 of the Standard Specifications, shall be used in the concrete for precast prestressed concrete deck beams. Compressive strength of prestressed concrete, f'c, shall be 6000 psi. Compressive strength of prestressed concrete at release, f'cl, shall be 5000 psi.



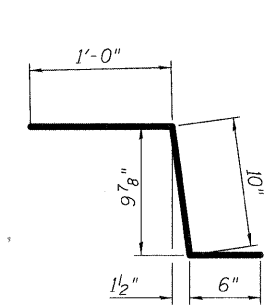
BAR d₁(E)*



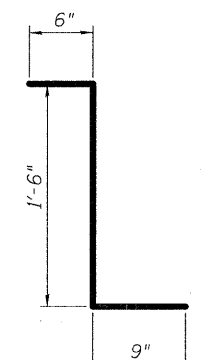
BAR d₂(E)*



LIFTING LOOP DETAIL



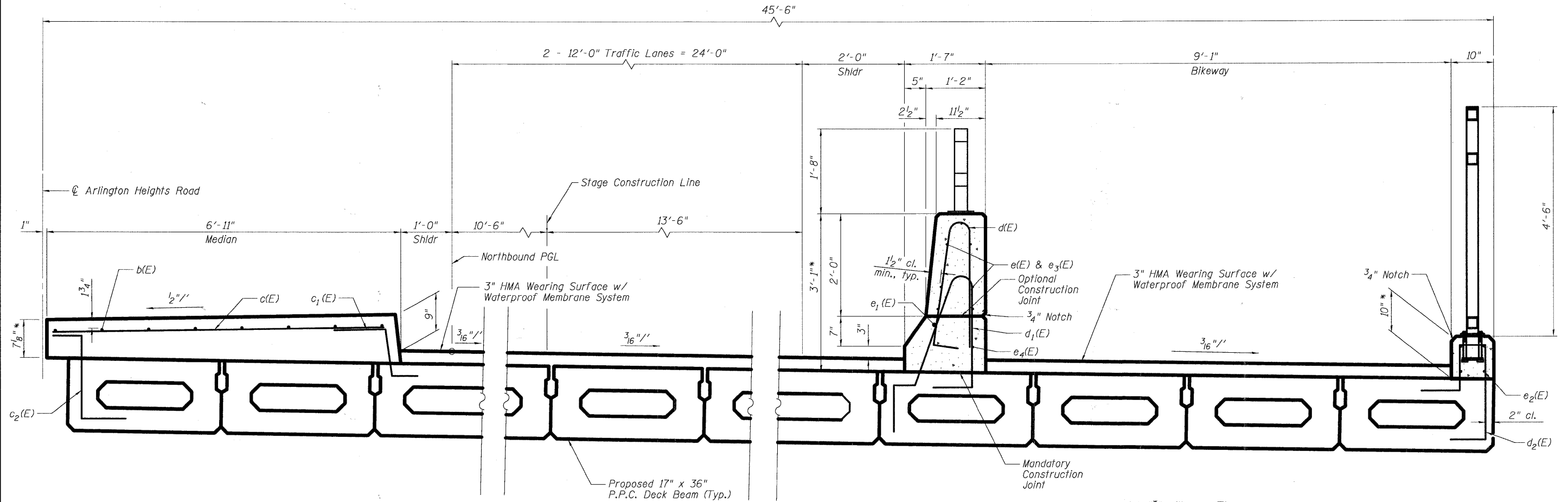
BAR c₁(E)*



BAR c₂(E)*



FILE NAME = 0493042-63531-005-BeamDetails2.dgn	USER NAME = 328208987	DESIGNED = KO	REVISED =	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	17" x 36" PPC DECK BEAM DETAILS 2 STRUCTURE NO. 049-3040 (NB)	F.A.U. RTE. = 2626	SECTION = 05-00193-04-BR	COUNTY = LAKE	TOTAL SHEETS = 36	SHEET NO. = 16	
PLOT SCALE = 2.0000' / IN.	DRAWN = KO	CHECKED = WPM	REVISED =			SHEET NO. 5 OF 15 SHEETS					
PLOT DATE = 10/25/2010	CHECKED = WPM	REVISED =	REVISED =			CONTRACT NO. 63531					
						ILLINOIS FED. AID PROJECT					

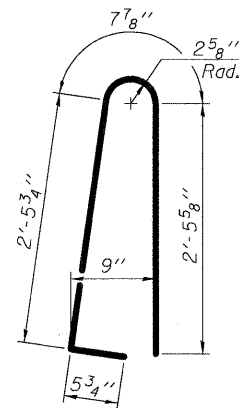


* Vertical dimensions marked with an asterisk "*" will vary. The dimensions provided are the maximum dimensions located at the abutments. Due to the camber of beams, these dimensions will be reduced by approximately 1/8" at midspan. Adjust reinforcement bars in field accordingly.

DECK CROSS SECTION

**PARAPET & BICYCLE RAILING
BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
b(E)	16	#4	21'-6"	—
c(E)	42	#5	6'-6"	—
d(E)	46	#5	6'-1"	∧
e(E)	8	#4	14'-8"	—
e ₁ (E)	1	#8	40'-11"	—
e ₂ (E)	8	#4	21'-6"	—
e ₃ (E)	16	#4	12'-10"	—
e ₄ (E)	1	#4	40'-11"	—
Concrete Superstructure			Cu. Yd.	15.2
Protective Coat			Sq. Yd.	99
Reinforcement Bars, Epoxy Coated			Pound	1280

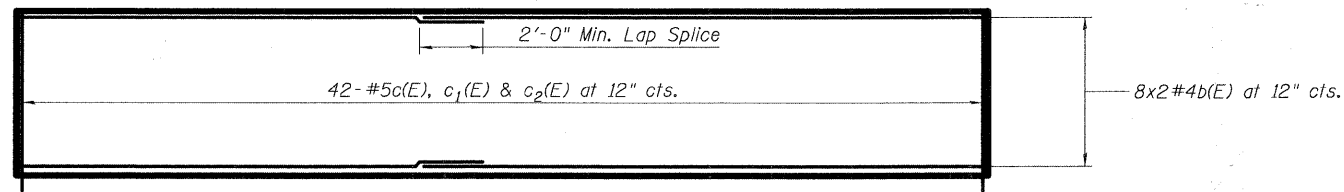


BAR d(E)

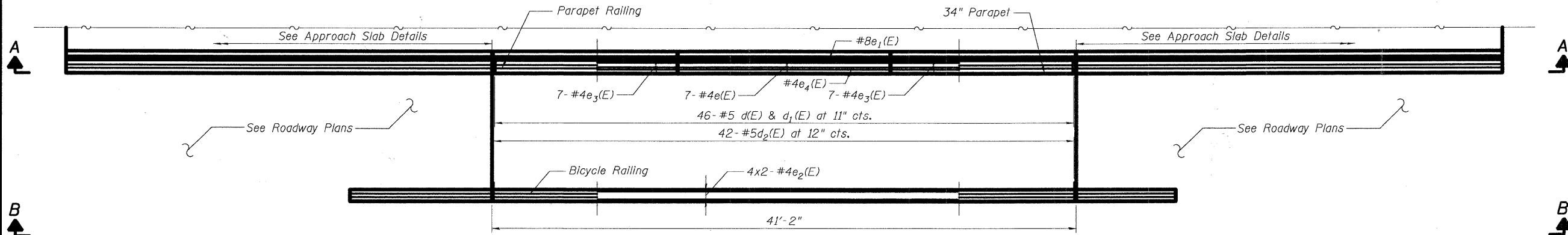
Notes:

1. For quantities and payment of 3" HMA wearing surface see civil plan.
2. Apply protective coat to the vertical projection of the traffic face and the top of the median and the parapet.

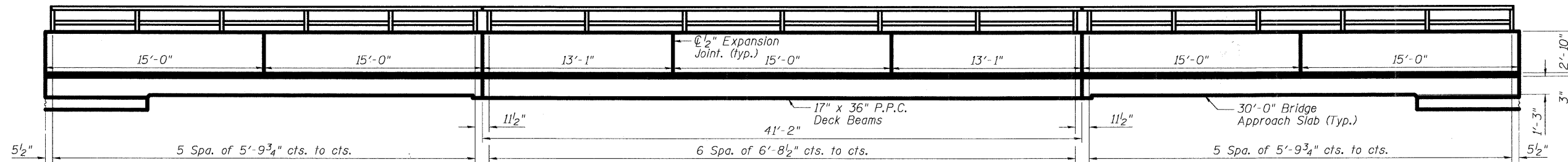




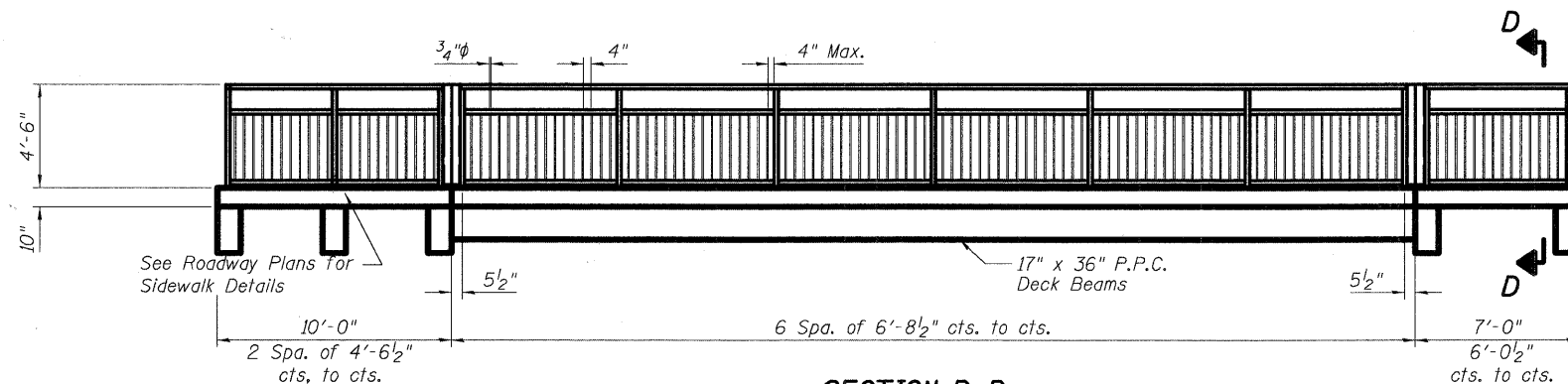
MEDIAN PLAN



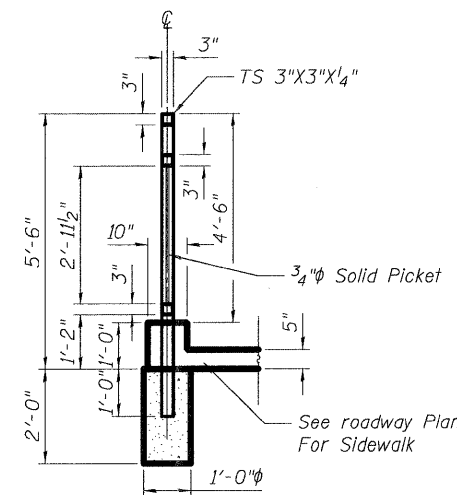
BIKEWAY PLAN



SECTION A-A



SECTION B-B

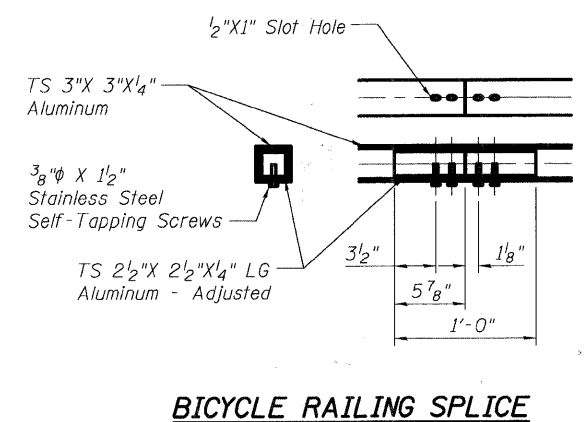
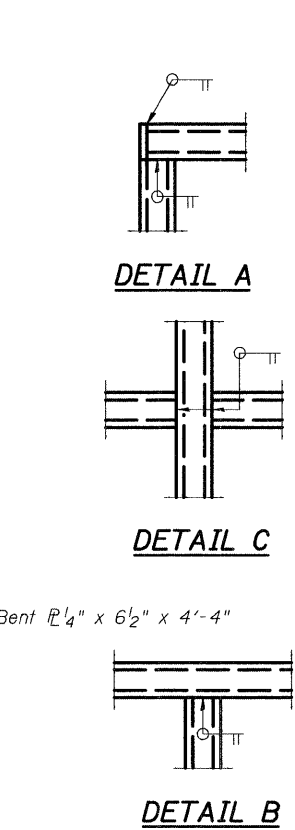
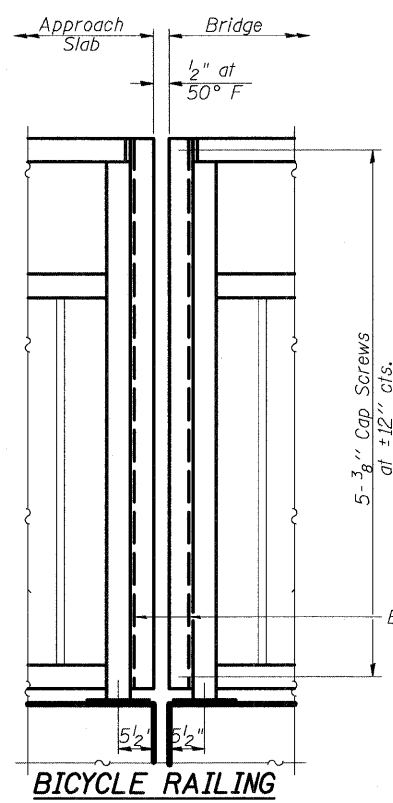
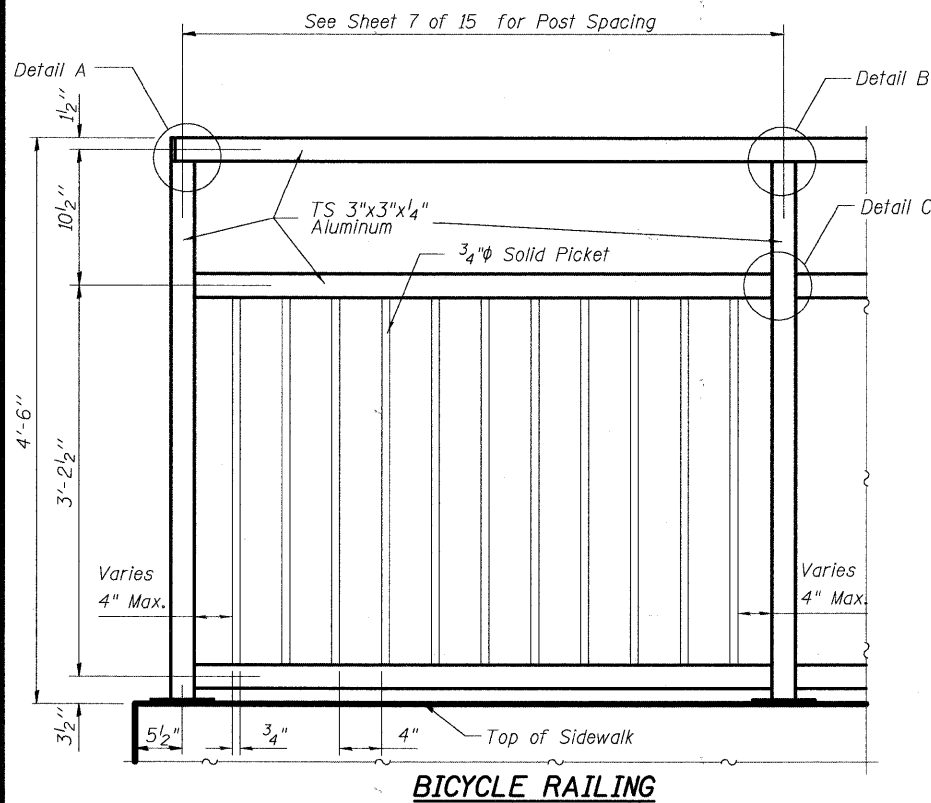


SECTION D-D

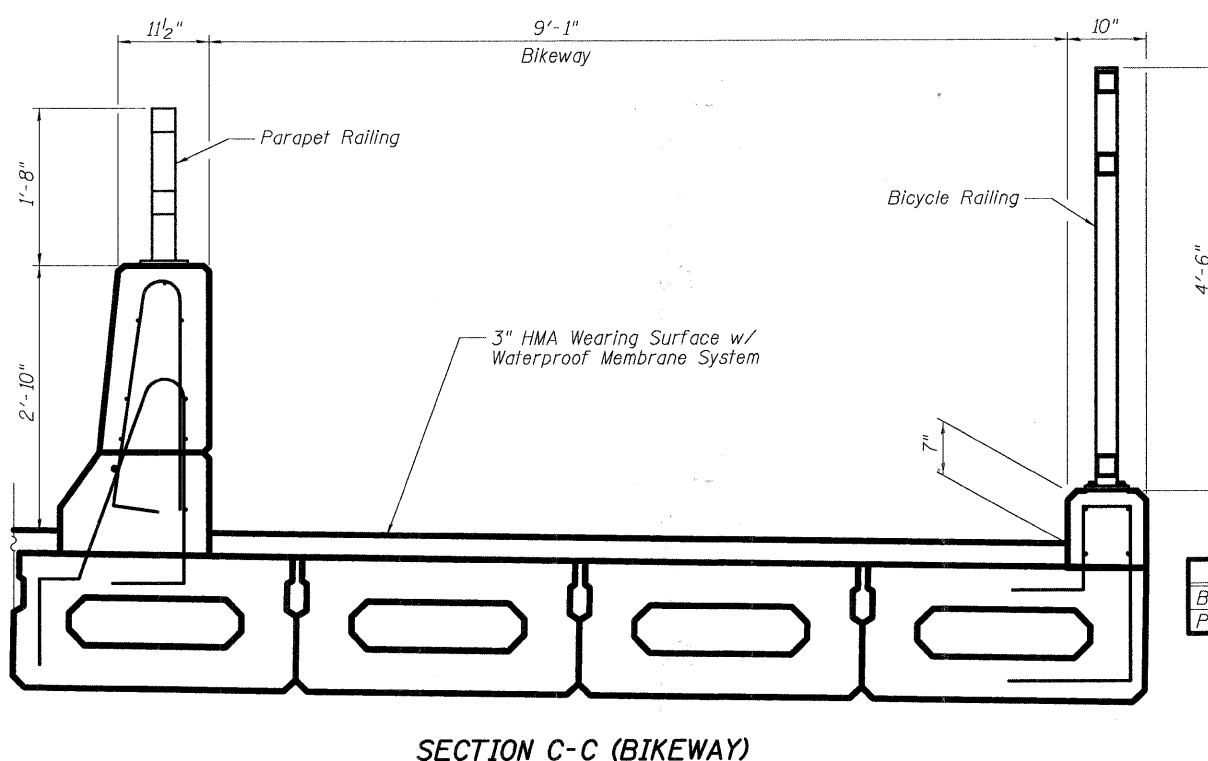
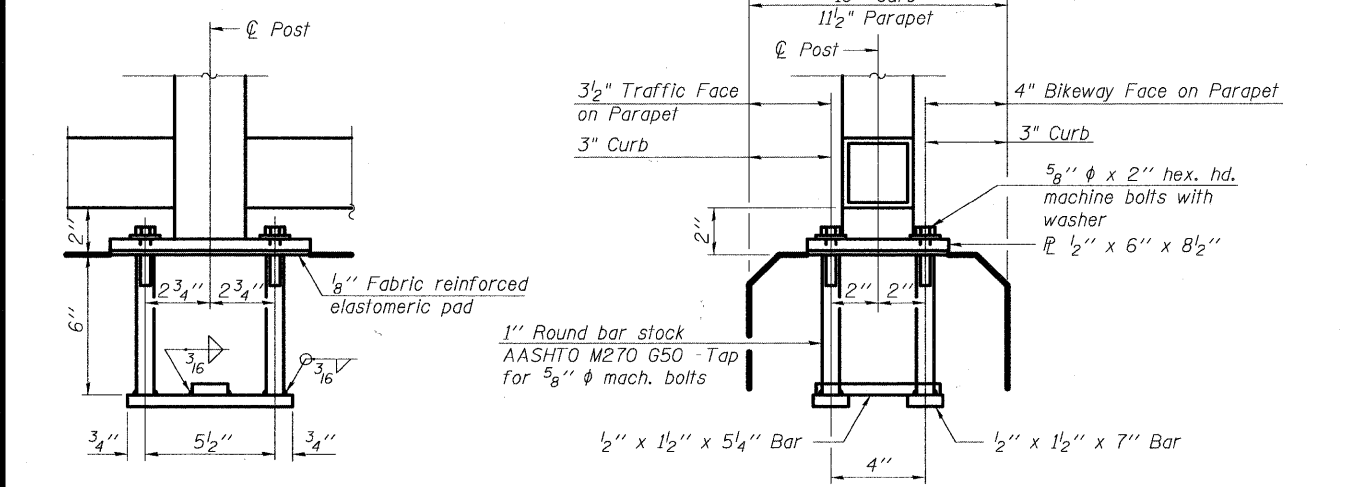
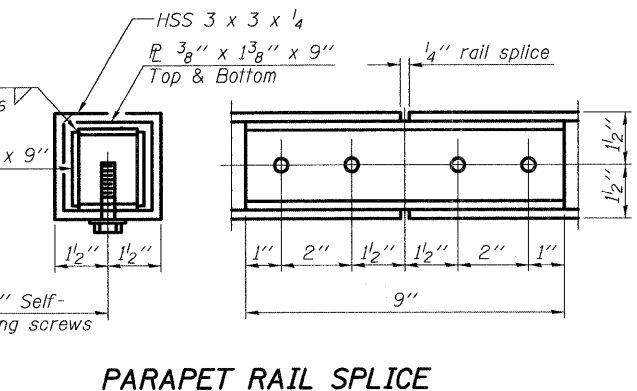
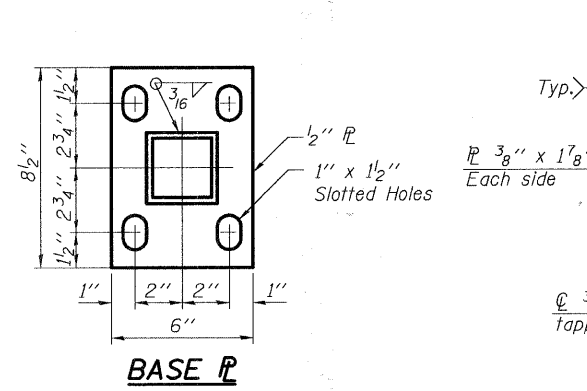
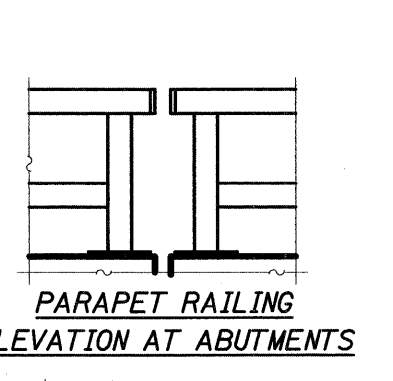
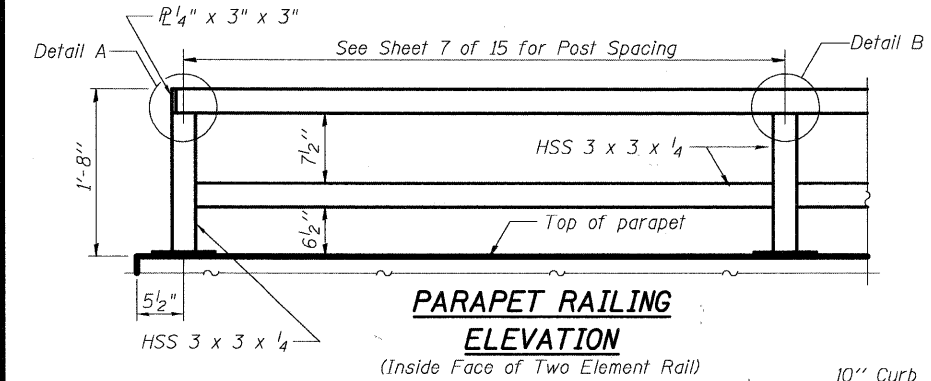
Note: See Sheet No. 13 of 15 for Parapet Details



FILE NAME = 0493040-63531-007-Railing1.dgn	USER NAME = 328288787	DESIGNED - KO	REVISD -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BIKEWAY RAILING DETAILS 1 STRUCTURE NO. 049-3040 (NB)	F.A.J. RTE. 2626	SECTION 05-00193-04-BR	COUNTY LAKE	TOTAL SHEETS 36	SHEET NO. 18
PLOT SCALE = 2.0000' / IN.	DRAWN - KO	CHECKED - WPM	REVISD -			CONTRACT NO. 63531				
PLOT DATE = 10/25/2010	DRAWN - KO	CHECKED - WPM	REVISD -			ILLINOIS FED. AID PROJECT				
SHEET NO. 7 OF 15 SHEETS										



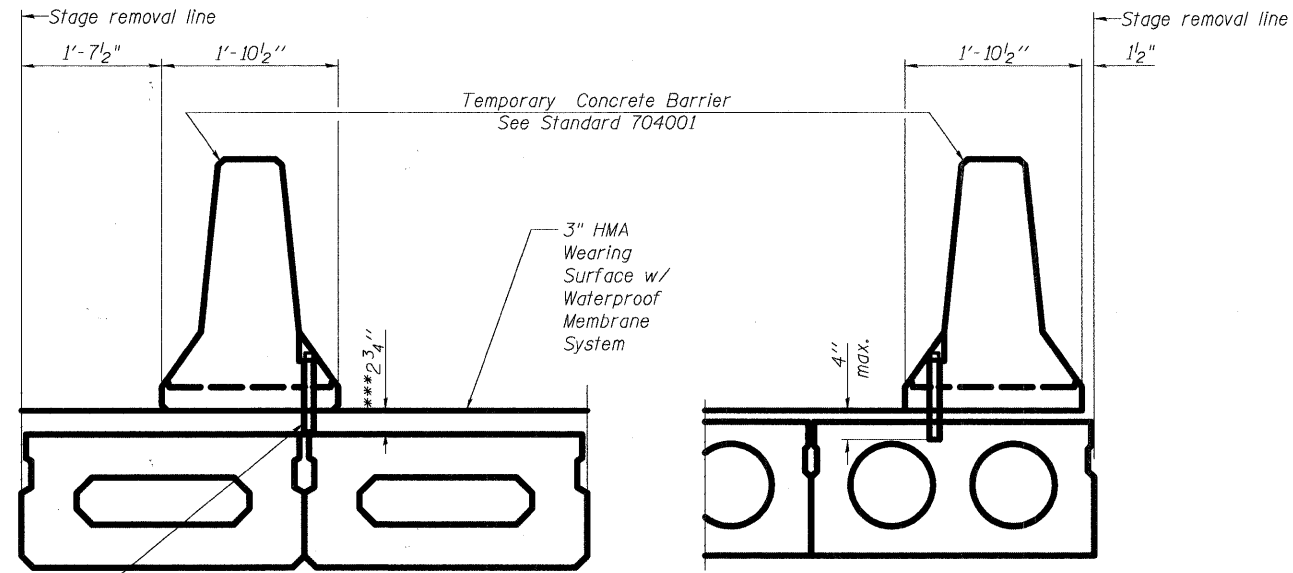
- Parapet Railing Notes:**
- All steel rail elements shall be galvanized according to Article 509.05 of the Standard Specifications.
- Bicycle Railing Notes:**
- Bicycle railings shall be according to section 509 of the IDOT Standard Specifications, except as noted.
 - All tubing will be aluminum alloy 6063-T52.
 - All dimensions are in feet and inches except as noted.
 - All posts, railing shall be painted in accordance with section 506 of the IDOT Standard Specifications.
 - All rail will be sandblasted per SSPC-SP6 followed by shop applied primer and top coat as follows:
3.00 MILS DFT TNEMEC 66-Hi build epoxyline epoxy interim coat, then
2.00 MILS DFT TNEMEC 73-Color aliphatic acrylic polyurethane topcoat - black.
 - All bolts, nuts and washers shall be stainless steel.
 - Shop drawings based on individual project plan view locations must be submitted to the resident engineer for approval.



BILL OF MATERIAL

Item	Unit	Quantity
Bicycle Railing	Foot	59
Parapet Railing	Foot	102





Drill 3-1/4" ϕ Holes in new wearing surface only for 1" ϕ x 11" dowel bars. Traffic side only. Cost included with Temporary Concrete Barrier.

NEW DECK BEAM

EXISTING DECK BEAM

*** Drilling into PPC deck beam is prohibited.
Embedment of 2 3/4" Dowel Bar into HMA wearing surface only.

NOTES

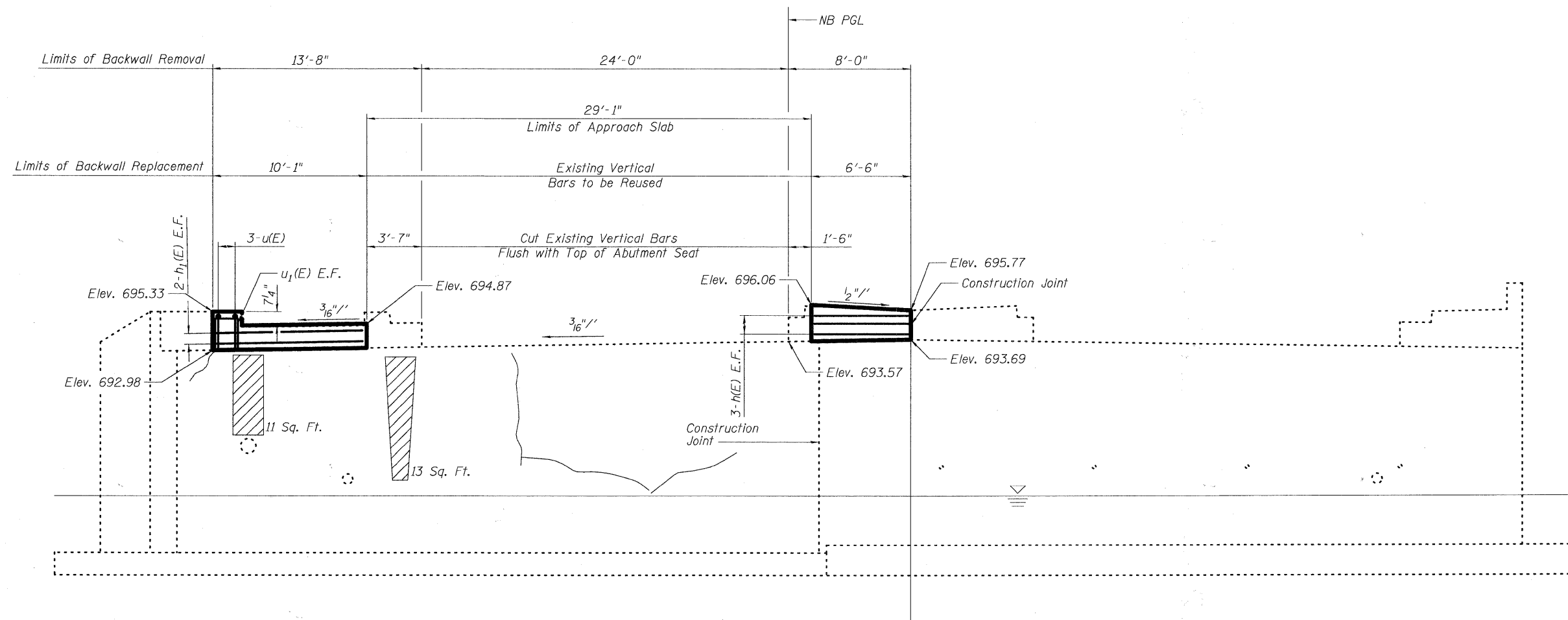
Any hole drilled into the wearing surface shall be repaired upon removal of the Temporary Concrete Barrier. Cost shall be included in the pay item Temporary Concrete Barrier.

For quantities and payment of Temporary Concrete Barrier see Roadway Plans.

SECTIONS THRU OR DECK BEAM





FILE NAME = 0493040-63531-009-TempBarrier.dgn	USER NAME = 320200907	DESIGNED - KO	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION STRUCTURE NO. 049-3040 (NB)	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLLOT SCALE = 2.0000' / IN.	CHECKED - WPM	REVISED -			2626	05-00193-04-BR	LAKE	36	20
	PLLOT DATE = 10/25/2010	DRAWN - KO	REVISED -			CONTRACT NO. 63531				
		CHECKED - WPM	REVISED -			ILLINOIS FED. AID PROJECT				
SHEET NO. 9 OF 15 SHEETS										



South Abutment
Looking South

Legend

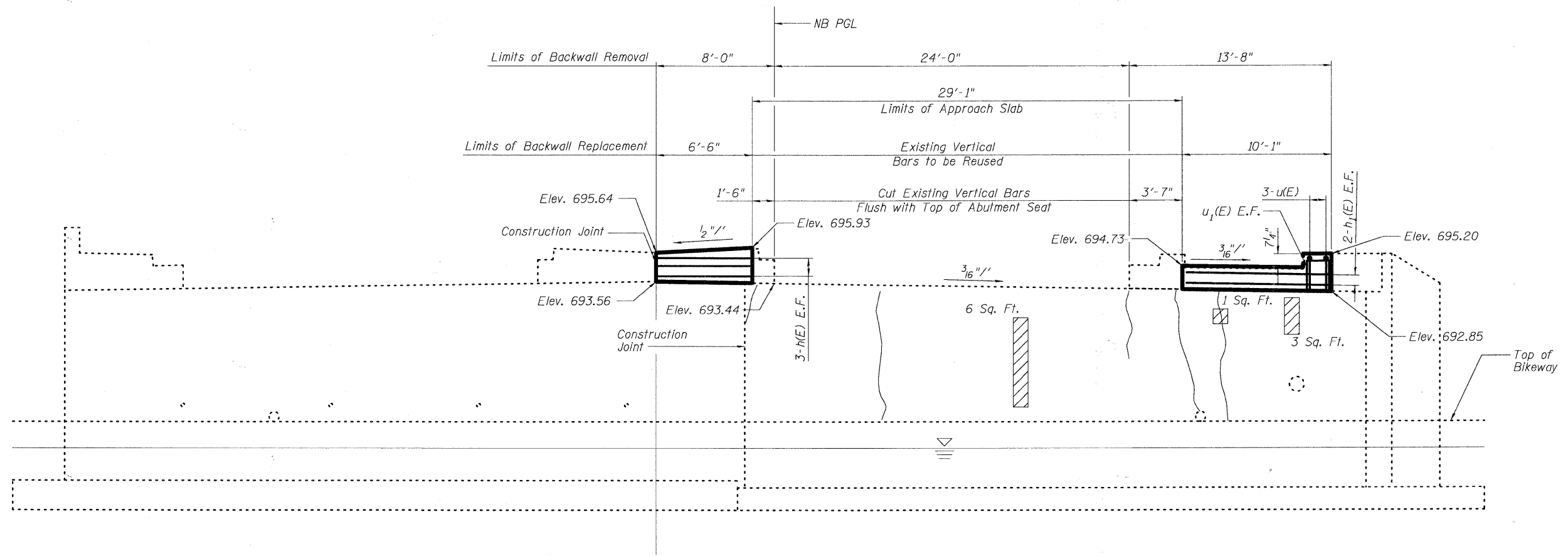
-  - Epoxy Crack Injection
-  - Structural Repair of Concrete (Depth Equal to or Less Than 5 inches)

Notes:

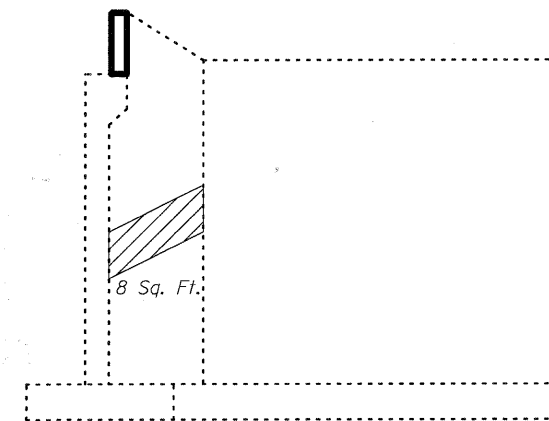
See sheet No. 12 of 15 for Reinforcement Bars details.



FILE NAME = 0493040-63531-010-S AbutLdgn	USER NAME = 32020097	DESIGNED - KO	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SOUTH ABUTMENT DETAILS STRUCTURE NO. 049-3040 (NB)	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 2.0000' / IN.	CHECKED - WPM	REVISED -			2626	05-00193-04-BR	LAKE	36	21
PLOT DATE = 10/25/2010	CHECKED - WPM	REVISED -		SHEET NO. 10 OF 15 SHEETS		CONTRACT NO. 63531 ILLINOIS FED. AID PROJECT				



North Abutment
Looking North



Northeast Wingwall Elevation

Legend

- Epoxy Crack Injection
- Structural Repair of Concrete (Depth Equal to or Less Than 5 inches)

Notes:

See sheet No. 12 of 15 for Reinforcement Bars details.

FILE NAME =	0493040-63531-011-N Abut.dgn
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USER NAME =	320200907
PLOT SCALE =	2.0000' / IN.
PLOT DATE =	10/25/2010

DESIGNED -	KO
CHECKED -	WPM
DRAWN -	KO
CHECKED -	WPM

REVISED -	-
REVISED -	-
REVISED -	-
REVISED -	-

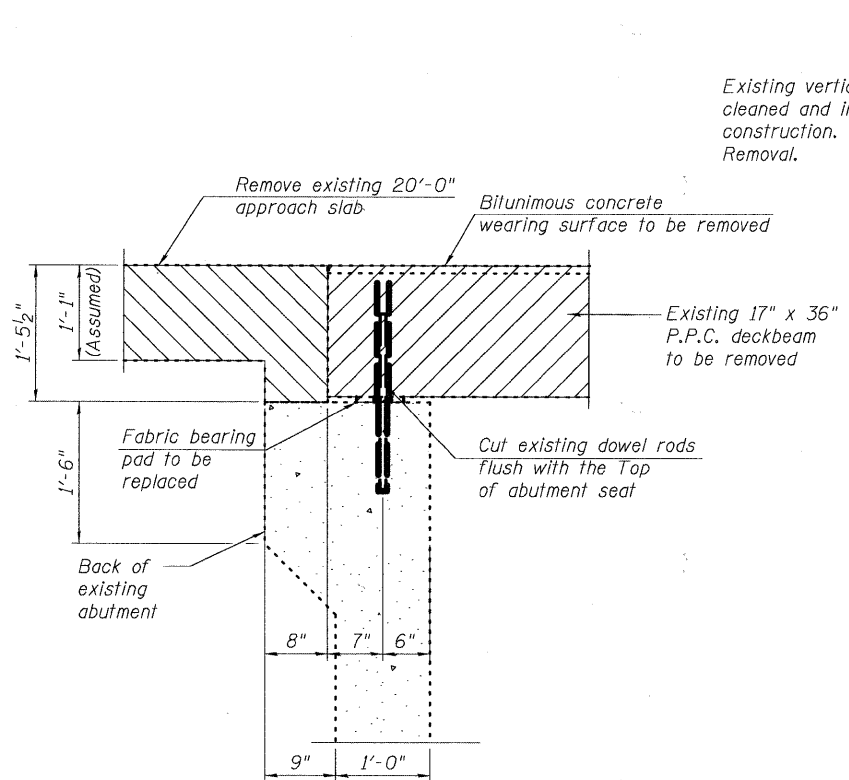
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**NORTH ABUTMENT DETAILS
STRUCTURE NO. 049-3040 (NB)**

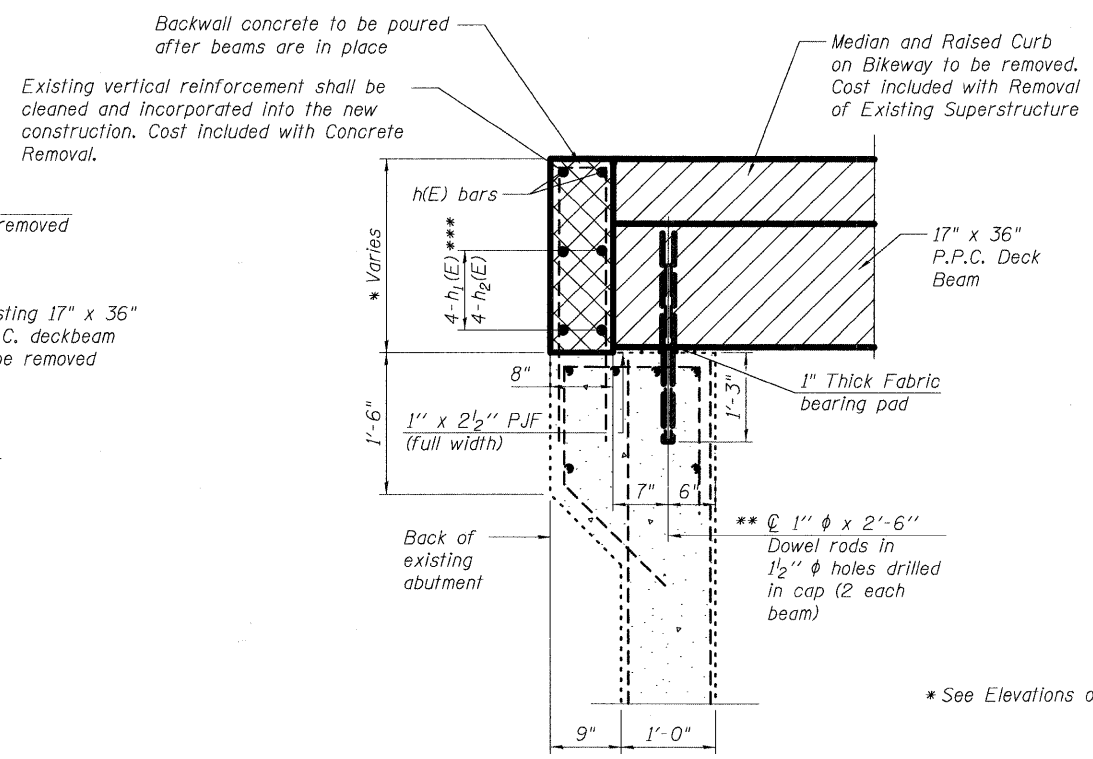
SHEET NO. 11 OF 15 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2626	05-00193-04-BR	LAKE	36	22
CONTRACT NO. 63531			ILLINOIS FED. AID PROJECT	

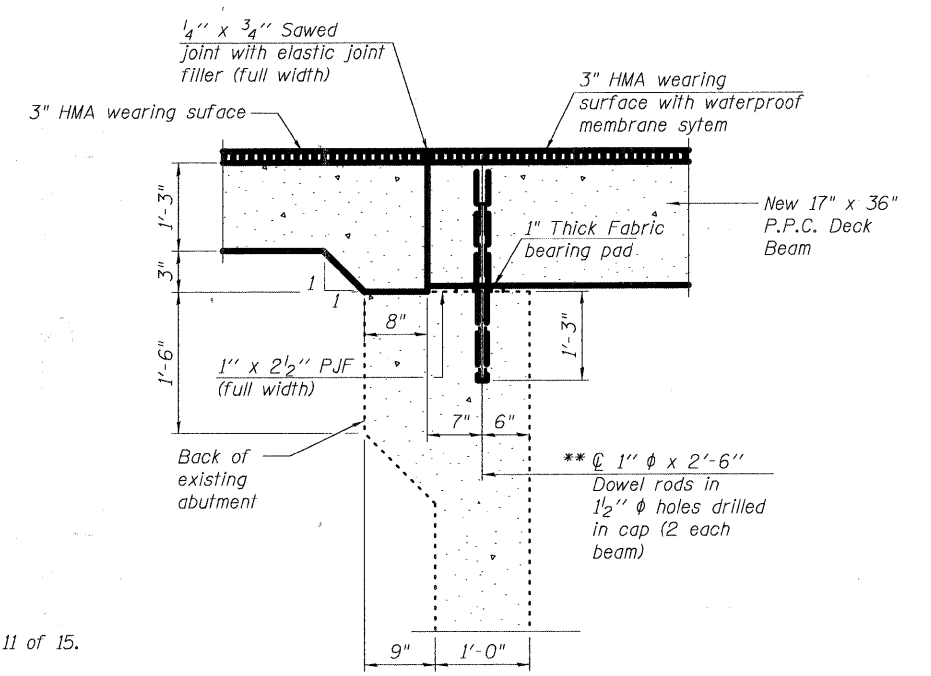




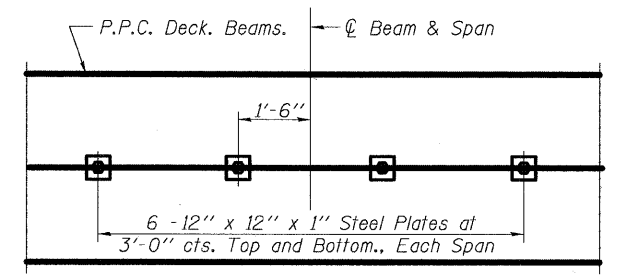
SECTION THRU ABUTMENT
 (Typical at Approach Slab)



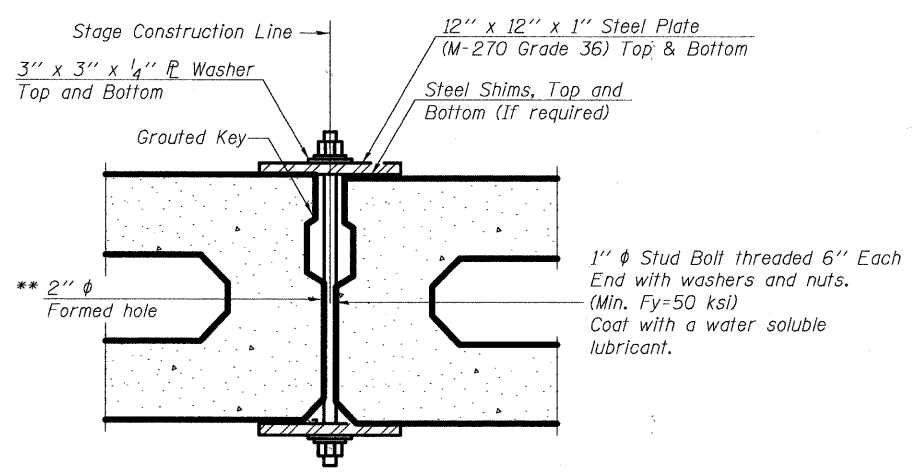
SECTION THRU ABUTMENT
 (Typical at Median & Raised Curb Along Bikeway)*



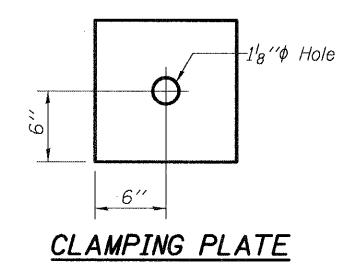
PROPOSED SECTION THRU ABUTMENT
 (Typical at Approach Slab)



PLAN



SECTION

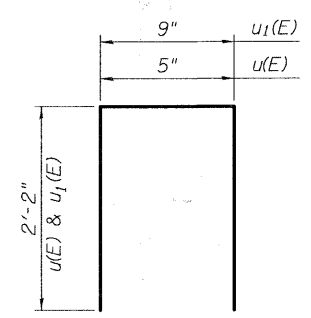


CLAMPING PLATE

SHEAR KEY CLAMPING DETAILS AT STAGE CONST. JT.

Cost included with Precast Prestressed Concrete Deck Beams.
 See Stage Construction Details for traffic lanes.

** Cast semicircular recesses in the sides of each beam adjacent to the stage construction line. These recesses should align to form a hole at the appropriate locations for the clamping device bolts.



BAR u(E) and u₁(E)

** Existing Dowel Rods shall be cut flush with the top of abutment seat. After beams have been erected, holes shall be drilled into substructure and anchor dowels placed.
 *** h(E) bars in backwall at Concrete Median
 h₁(E) bars in backwall at Sidewalk

* See Elevations on Sheets 10 and 11 of 15.

TWO BACKWALL BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h(E)	12	#5	6'-2"	—
h ₁ (E)	8	#5	9'-9"	—
u(E)	6	#5	4'-9"	□
u ₁ (E)	4	#5	5'-1"	□
Approach Slab Removal		Sq. Yd.	130	
Concrete Removal		Cu. Yd.	2.1	
Concrete Structures		Cu. Yd.	1.8	
Reinforcement Bars, Epoxy Coated		Pound	420	

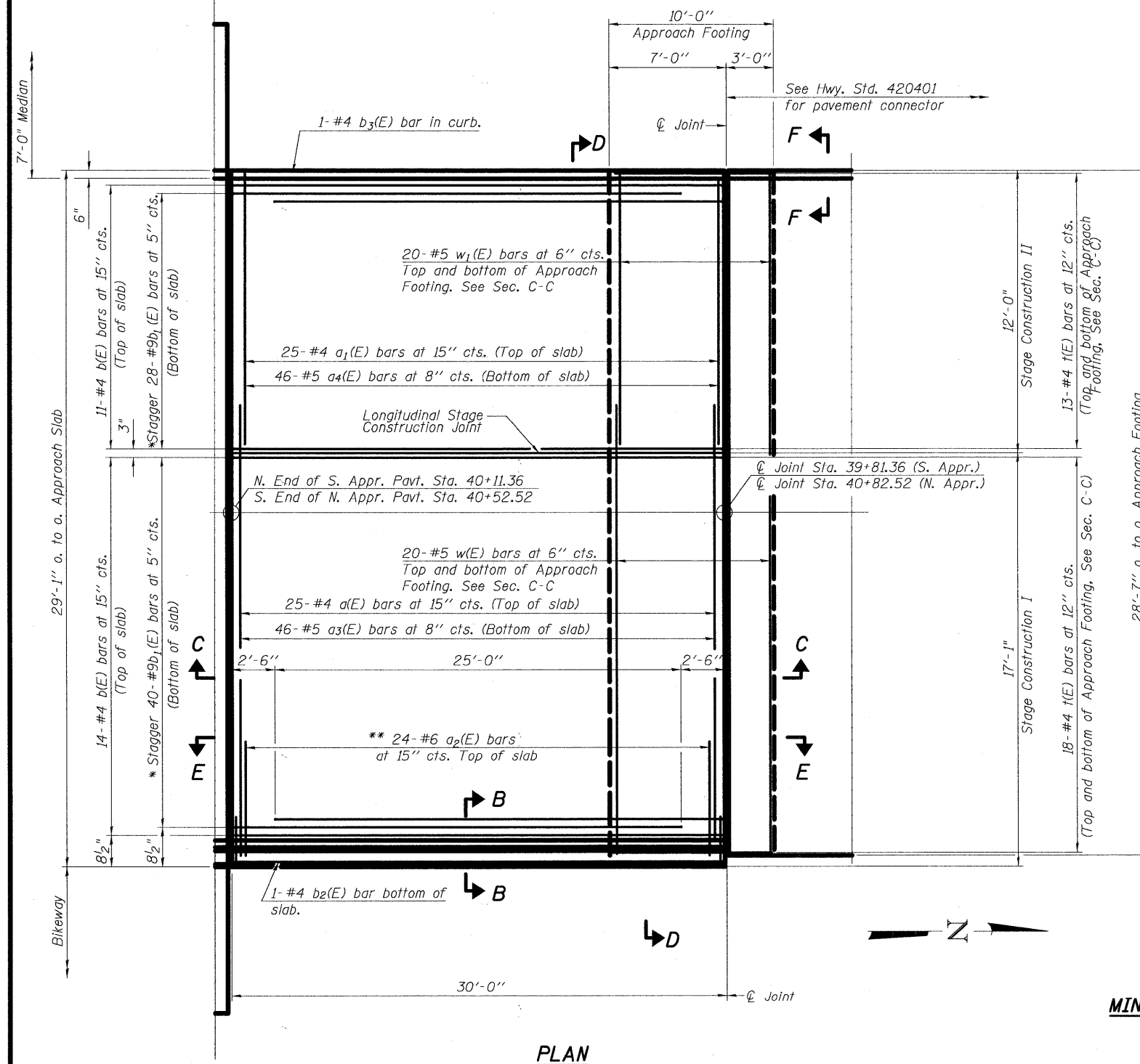
LEGEND:

- Limits of Approach Slab Removal
- Limits of Concrete removal
- Limits of Removal of Existing Superstructure



Notes:
See sheet No. 14 of 15 for Sections C-C & D-D and View E-E.
a(E) and a₁(E) bar spacings measured along ϕ Rdwy.

*** Cost included with Concrete Superstructure.

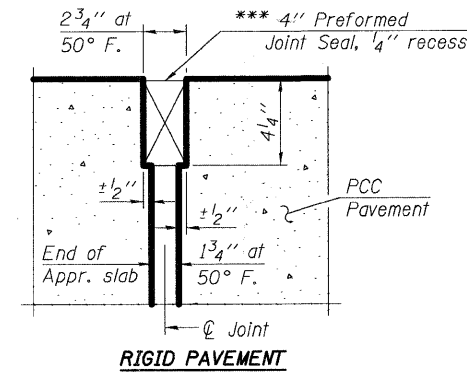


PLAN

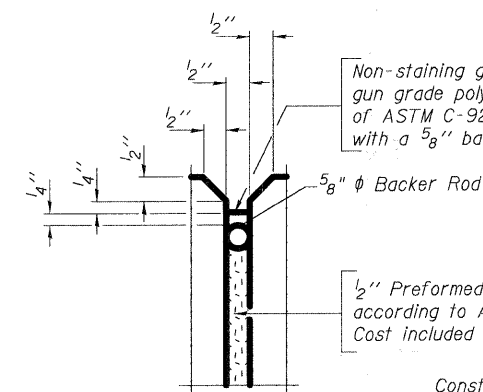
* Tilt #9 b₁(E) bars as required to maintain clearance.
** Space between a(E) bars, typ. ea. parapet.

MINIMUM BAR LAP

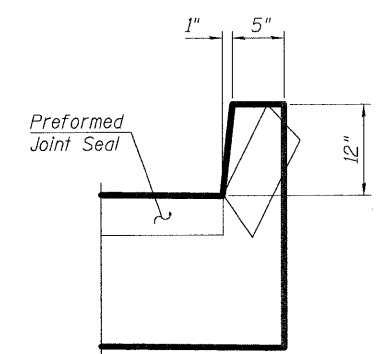
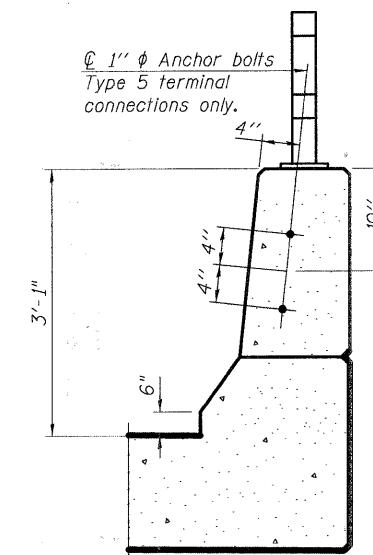
#4 bar = 2'-0"
#5 bar = 2'-6"
#6 bar = 3'-0"



DETAIL A



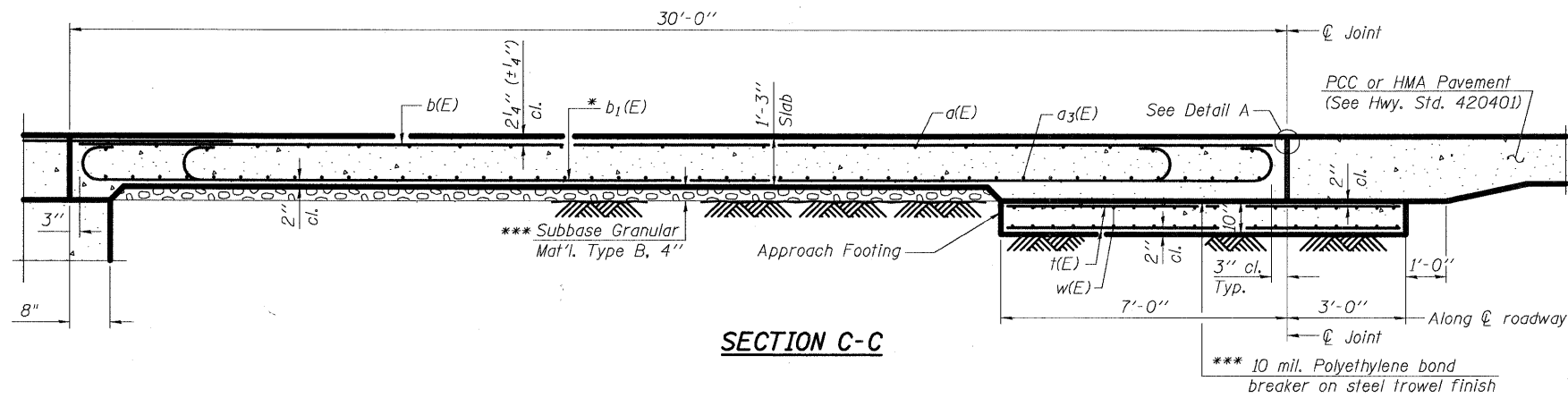
PARAPET JOINT DETAILS



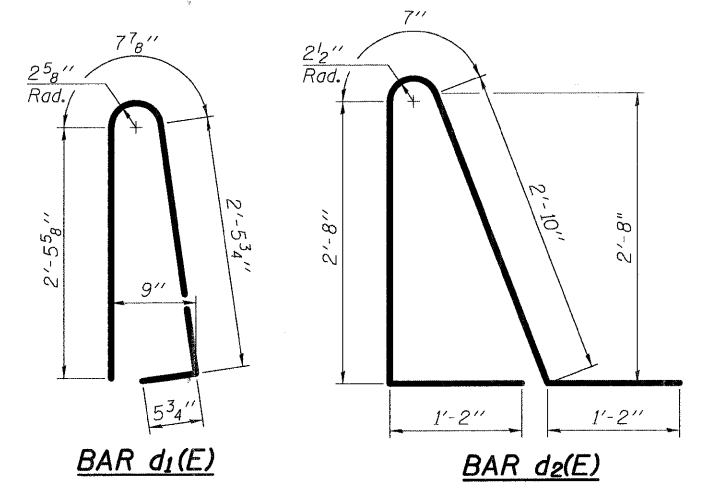
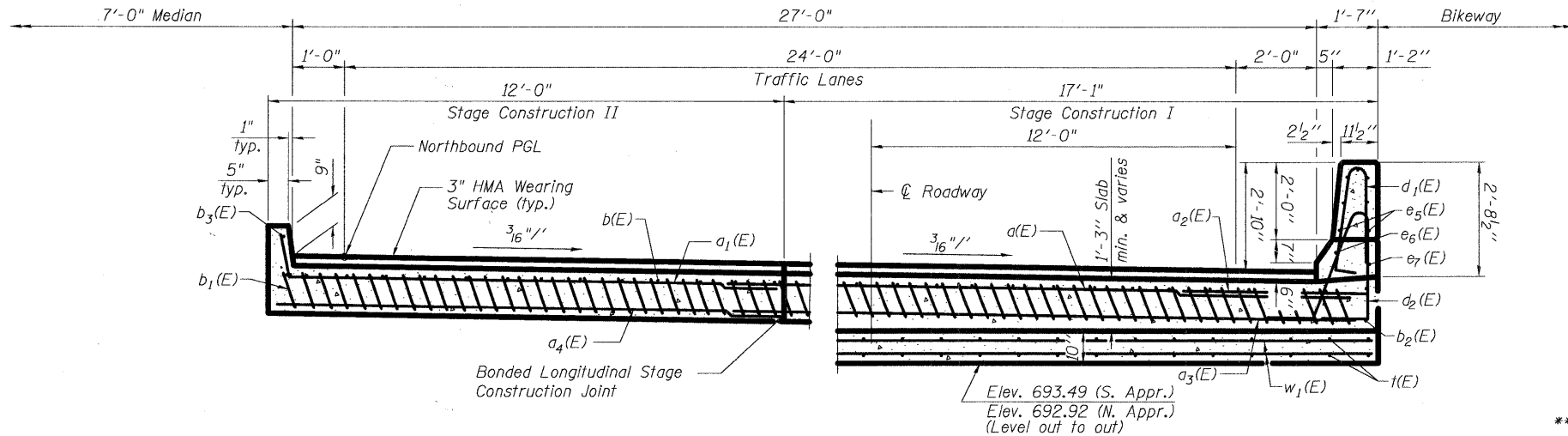
Angle Preformed Joint Seal at 45° at curbs when req'd for drainage.



FILE NAME = 0493040-63531-013-ApprDetails1.dgn	USER NAME = 3202080907	DESIGNED - KO	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	APPROACH SLAB DETAILS 1 STRUCTURE NO. 049-3040 (NB)	F.A.U. RTE. 2626	SECTION 05-00193-04-BR	COUNTY LAKE	TOTAL SHEETS 36	SHEET NO. 24	
PLOT SCALE = 2.0000' / IN.	DRAWN - KO	CHECKED - WPM	REVISED -			SHEET NO. 13 OF 15 SHEETS					
PLOT DATE = 10/25/2010	DRAWN - KO	CHECKED - WPM	REVISED -			ILLINOIS FED. AID PROJECT					
						CONTRACT NO. 63531					



Notes:
 See sheet No. 13 of 15 for Detail A and View B-B.
 Approach slab and parapet concrete shall be paid for as Concrete Superstructure.
 Approach footing concrete shall be paid for as Concrete Structures.
 Reinforcement shall be paid for as Reinforcement Bars, Epoxy Coated.
 The approach footing maximum applied service bearing pressure (Qmax) = 2.0 ksf.
 Cost of excavation for approach footing included with Concrete Structures.
 For additional parapet details, see sheet No. 13 of 15.



* Till #9 b1(E) bars as required to maintain clearance.
 *** Cost included with Concrete Superstructure.

NEAR ABUTMENT

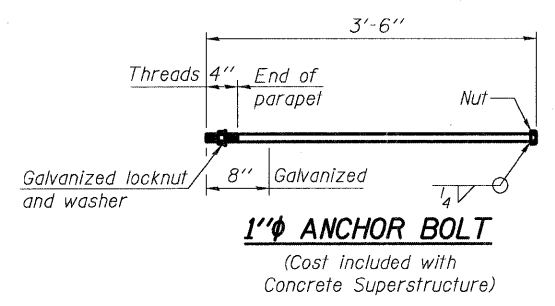
AT APPROACH FOOTING

SECTION D-D

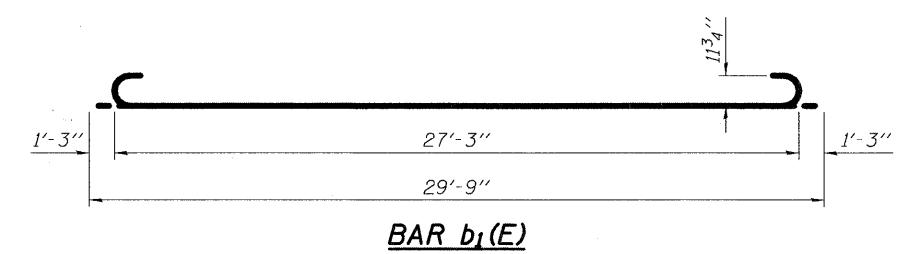
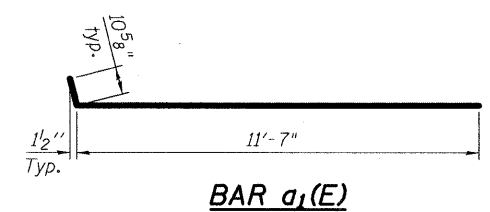
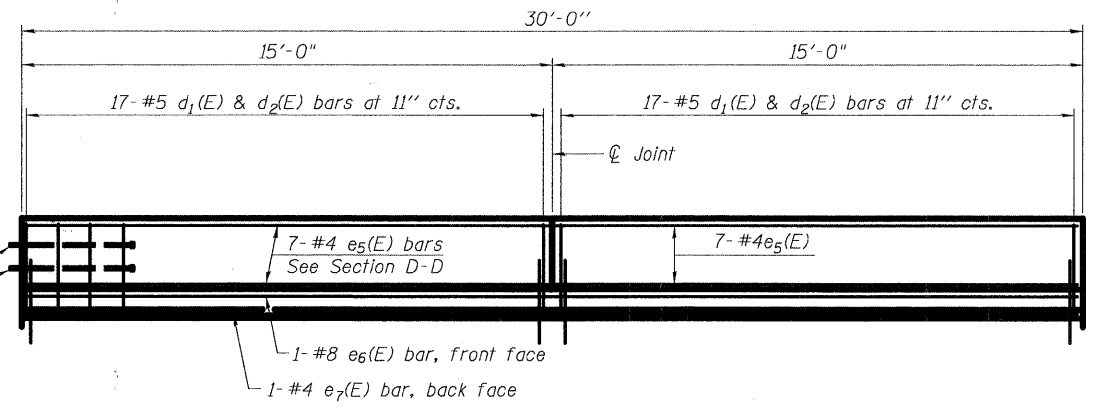
(See Plan for dimensions not shown)

**TWO APPROACHES
 BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a(E)	50	#4	19'-5"	—
a1(E)	50	#4	12'-3"	—
a2(E)	48	#6	6'-6"	—
a3(E)	92	#5	19'-5"	—
a4(E)	92	#5	11'-10"	—
b(E)	50	#4	29'-8"	—
b1(E)	136	#9	29'-9"	—
b2(E)	2	#4	29'-8"	—
b3(E)	2	#4	29'-8"	—
d1(E)	68	#5	6'-1"	U
d2(E)	68	#5	8'-5"	T
e5(E)	28	#4	14'-8"	—
e6(E)	2	#8	29'-8"	—
e7(E)	2	#4	29'-8"	—
t(E)	124	#4	9'-8"	—
w(E)	80	#5	19'-5"	—
w1(E)	80	#5	11'-9"	—
Concrete Superstructure			Cu. Yd.	86.9
Concrete Structures			Cu. Yd.	17.7
Reinforcement Bars, Epoxy Coated			Pound	25440



1' ϕ Anchor bolts at Type 5 terminal connections only See View B-B



STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

APPROACH SLAB DETAILS 2
 STRUCTURE NO. 049-3040 (NB)

SHEET NO. 14 OF 15 SHEETS



FILE NAME = 0493040-63531-014-ApprDetails2.dgn	USER NAME = 320200707	DESIGNED - KO	REVISIONS -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	APPROACH SLAB DETAILS 2 STRUCTURE NO. 049-3040 (NB)	SHEET NO. 14 OF 15 SHEETS	F.A.U. RTE. 2626	SECTION 05-00193-04-BR	COUNTY LAKE	TOTAL SHEETS 36	SHEET NO. 25
PLOT SCALE = 2.0000' / IN.	DRAWN - KO	REVISIONS -	CONTRACT NO. 63531								
PLOT DATE = 10/25/2010	CHECKED - WPM	REVISIONS -	ILLINOIS FED. AID PROJECT								

WEST CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
S. End of S. Appr. Pav't	39+81.36	-1	695.41
A1	39+91.36	-1	695.38
A2	40+01.36	-1	695.34
N. End of S. Appr. Pav't	40+11.36	-1	695.31
S. End of N. Appr. Pav't	40+52.52	-1	695.18
A3	40+62.52	-1	695.10
A4	40+72.52	-1	695.03
N. End of N. Appr. Pav't	40+82.52	-1	694.95

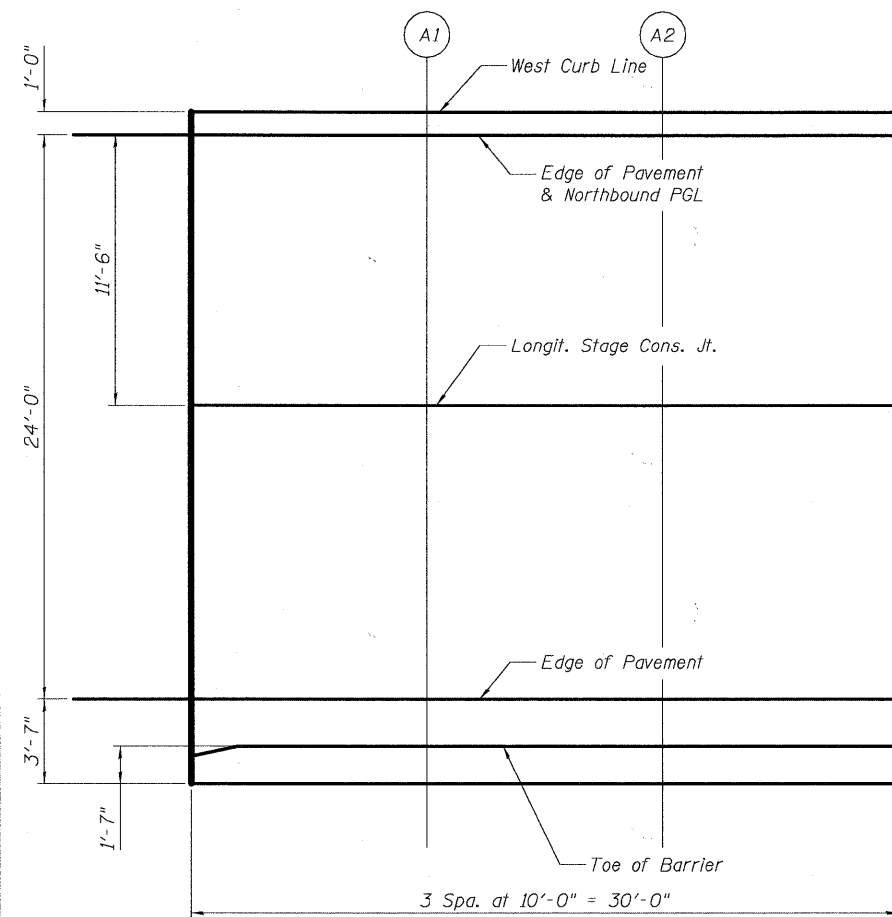
EDGE OF PAVEMENT & NORTHBOUND PGL

Location	Station	Offset	Theoretical Grade Elevations
S. End of S. Appr. Pav't	39+81.36	0	695.39
A1	39+91.36	0	695.36
A2	40+01.36	0	695.33
N. End of S. Appr. Pav't	40+11.36	0	695.30
S. End of N. Appr. Pav't	40+52.52	0	695.16
A3	40+62.52	0	695.09
A4	40+72.52	0	695.01
N. End of N. Appr. Pav't	40+82.52	0	694.94

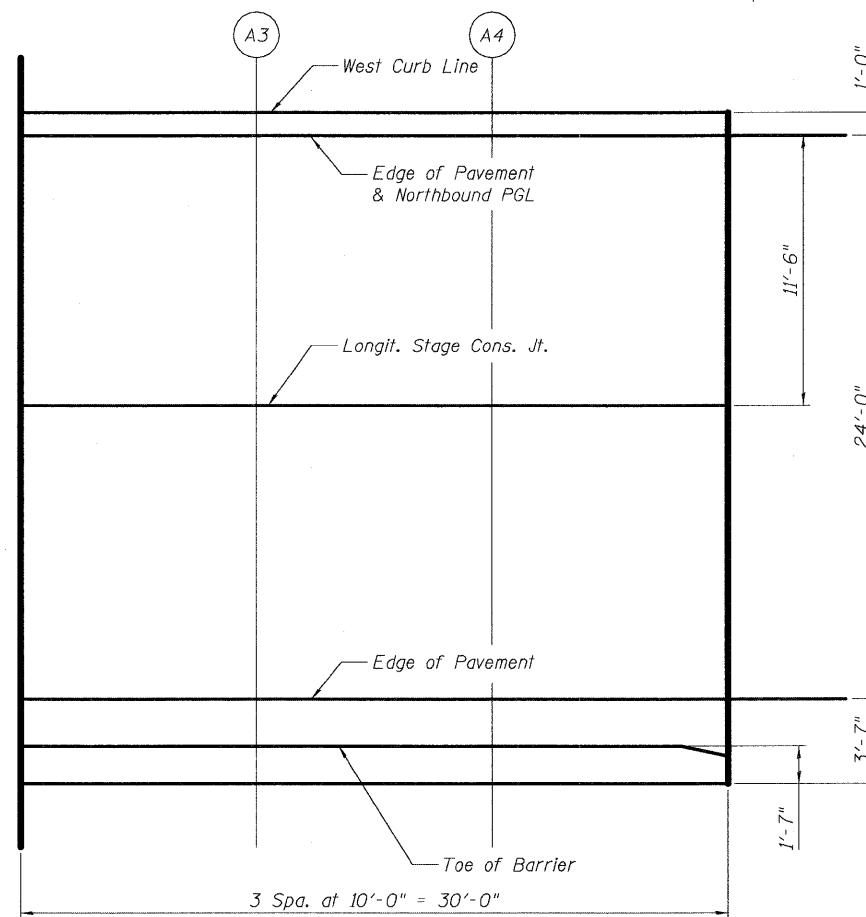
LONGIT. STG. CONST. JT.

Location	Station	Offset	Theoretical Grade Elevations
S. End of S. Appr. Pav't	39+81.36	10.5	695.23
A1	39+91.36	10.5	695.20
A2	40+01.36	10.5	695.16
N. End of S. Appr. Pav't	40+11.36	10.5	695.13
S. End of N. Appr. Pav't	40+52.52	10.5	695.00
A3	40+62.52	10.5	694.92
A4	40+72.52	10.5	694.85
N. End of N. Appr. Pav't	40+82.52	10.5	694.77

Note:
Theoretical Grade Elevations provided are measured from the top of the 3" HMA Wearing Surface.



PLAN
South Approach Slab



PLAN
North Approach Slab

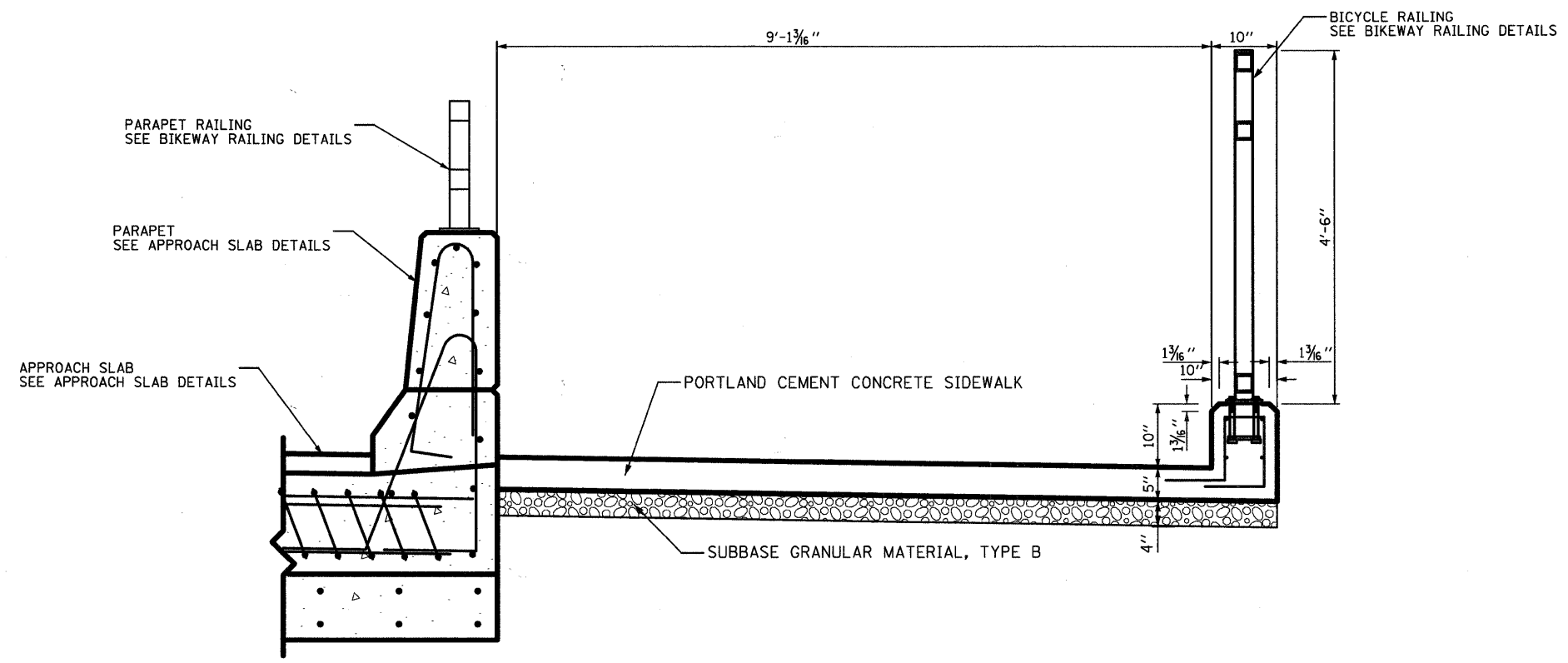
EDGE OF PAVEMENT

Location	Station	Offset	Theoretical Grade Elevations
S. End of S. Appr. Pav't	39+81.36	24	695.02
A1	39+91.36	24	694.99
A2	40+01.36	24	694.95
N. End of S. Appr. Pav't	40+11.36	24	694.92
S. End of N. Appr. Pav't	40+52.52	24	694.79
A3	40+62.52	24	694.71
A4	40+72.52	24	694.64
N. End of N. Appr. Pav't	40+82.52	24	694.56

TOE OF BARRIER

Location	Station	Offset	Theoretical Grade Elevations
S. End of S. Appr. Pav't	39+81.36	26	694.99
A1	39+91.36	26	694.95
A2	40+01.36	26	694.92
N. End of S. Appr. Pav't	40+11.36	26	694.89
S. End of N. Appr. Pav't	40+52.52	26	694.76
A3	40+62.52	26	694.68
A4	40+72.52	26	694.61
N. End of N. Appr. Pav't	40+82.52	26	694.53





PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL

FILE NAME = LC0907-shd-details.dgn	USER NAME = 3202000907	DESIGNED - TC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS		F.A.U. RTE. 2626	SECTION 05-00193-04-BR	COUNTY LAKE	TOTAL SHEETS 36	SHEET NO. 27
	PLOT SCALE = 2.0000' / IN.	CHECKED - MRH	REVISED -		SCALE: N/A	SHEET NO. 1 OF 1 SHEETS	STA. N/A	TO STA. N/A	CONTRACT NO. 63531		
	PLOT DATE = 10/25/2010	DATE - 10/25/2010	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						

TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES
for HIGHWAY CONSTRUCTION, CONTRACT MAINTENANCE
and UTILITY OPERATIONS

GENERAL NOTE:

This Standard is used where at anytime, day or night, any vehicle, equipment, workers or their activities encroach on the pavement requiring the closure of one or more traffic lanes in an urban area.

DESIGN NOTES:

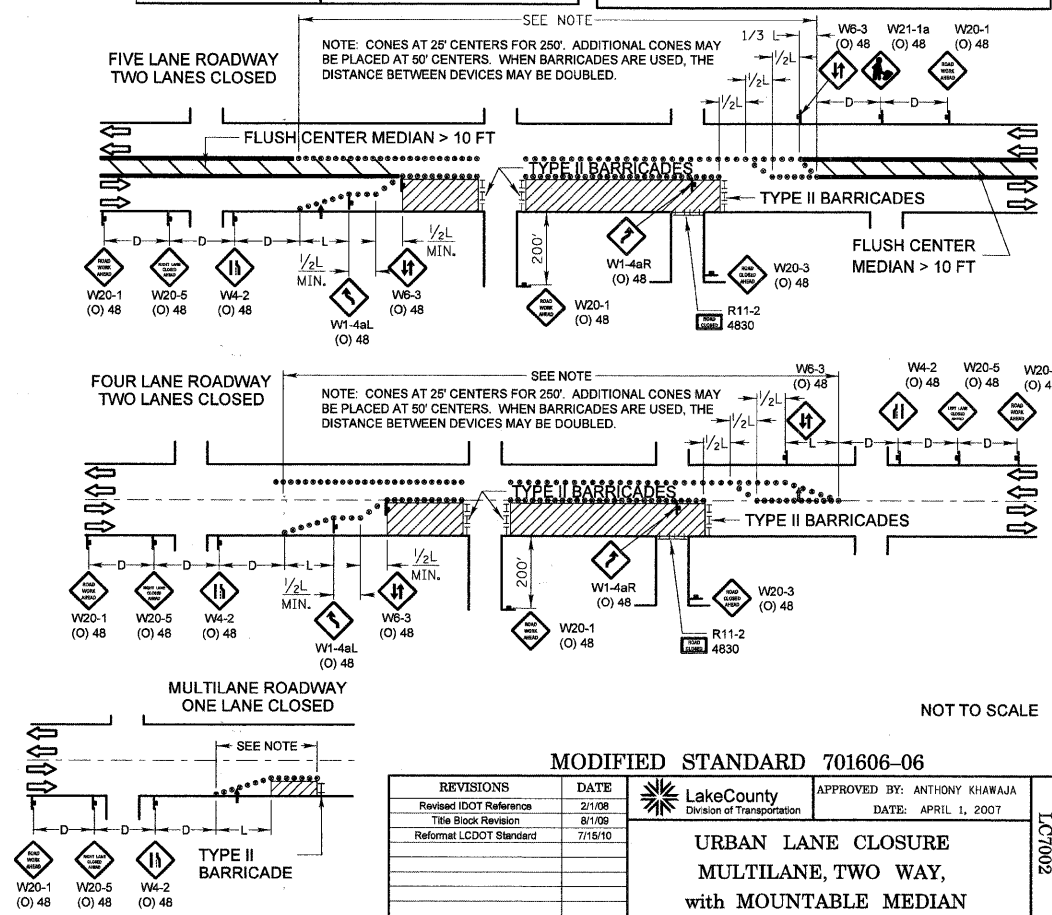
- All warning signs shall have minimum dimensions of 48" x 48".
- All signs not on the traveled way shall be post mounted if the closure time exceeds four calendar days. All signs shall be posted with the bottom of the sign not less than 7' above the edge of pavement. "NO PARKING" signs shall be installed throughout the work area at the discretion of the Engineer.
- The distance "L" shall be defined as:

SPEED	FORMULA
≤ 40 MPH	$L = (WS^2) / 60$
≥ 45 MPH	$L = LW \times S$

W = Width of Closure in FEET
S = Normal Posted Speed Limit in MPH
LW = Lane Width in FEET
- Type II barricades with Type C steady burning lights shall be used in lieu of cones for night operations. All cones and barricades shall be in accordance with IDOT Standard 701901.
- Type A flashing lights shall be used on each approach in advance of the work area during hours of darkness and installed above the first two signs in each series and the high level warning devices.
- If the work operation is performed between 9:00 am and 3:00 pm and the work does not exceed 15 minutes, the traffic protection shall be as shown for IDOT Standard 701301. Signs, when required, shall be at the spacing specified in the Advance Warning Sign Spacing Table.
- If the work area is in the parking lane and the parking exists during work hours, a "ROAD WORK AHEAD" sign shall be installed in advance of work area at the spacing specified in the Advance Warning Sign Spacing Table and the area protected with cones or barricades.
- Longitudinal dimensions may be adjusted to fit field conditions.
- Form BT 725 is required.

POSTED SPEED LIMIT	DISTANCE BETWEEN SIGNS "D"
40 MPH or less	200 FEET
45-50 MPH	350 FEET
55 MPH	500 FEET

	WORK AREA
	• CONE OR BARRICADE
	▬ SIGN ON PORTABLE OR PERMANENT SUPPORT
	← ARROW BOARD
	⊥ TYPE II BARRICADE
	⊥⊥ TYPE III BARRICADE



REVISIONS	DATE
Revised IDOT Reference	2/1/08
Title Block Revision	8/1/09
Reformat LCDOT Standard	7/15/10

LakeCounty
Division of Transportation

APPROVED BY: ANTHONY KHAWAJA
DATE: APRIL 1, 2007

URBAN LANE CLOSURE
MULTILANE, TWO WAY,
with MOUNTABLE MEDIAN

LC7002

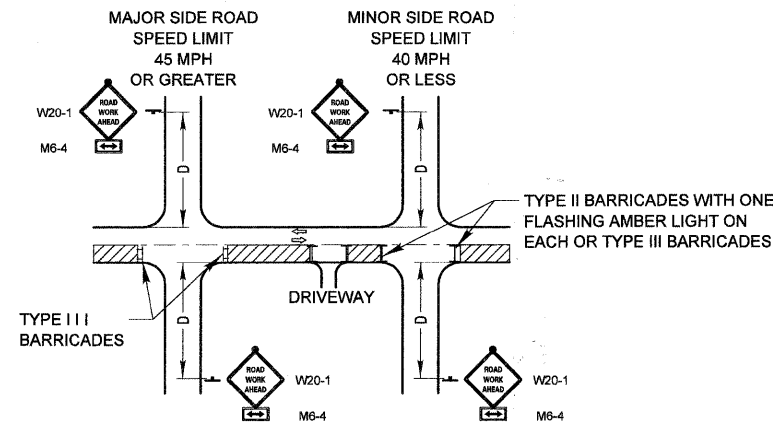
TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES
for HIGHWAY CONSTRUCTION, CONTRACT MAINTENANCE
and UTILITY OPERATIONS

GENERAL NOTE:

This Standard is used where at anytime, day or night, any vehicle, equipment, workers or their activities encroach on the pavement or where construction requires lane closures.

DESIGN NOTES:

- For a side road with a speed limit of 40 mph or less, the closed portion of the main route shall be protected by blocking with Type II or Type III barricades, 1/2 of the cross section of the closed portion of the roadway.
- For a side road with a speed limit of 45 mph or greater, the closed portion of the main route shall be protected by blocking with Type III barricades, 1/2 of the cross section of the closed portion of the roadway.
- All W20-1 "ROADWORK AHEAD" signs shall be 48" x 48" with fluorescent orange reflective sheeting with an amber Type A flashing light mounted on the sign.
- When the side road lies between the beginning of the mainline signing and the work zone, an M6-1 Single Headed Arrow shall be used in lieu of the M6-4 Double Headed Arrow.
- For a lane closure on a side road or driveway, use the applicable portions of the appropriate Highway Standard or Traffic Control Detail. The spacing of the signs and barricades shall be adjusted for field conditions as directed by the engineer. The directional arrow shall be covered or removed when no longer consistent with the side road lane closure.
- Advance warning signs shall be omitted on driveways unless otherwise noted.
- The traffic control and protection for side roads and intersections shall be included in the contract unit lump sum price for "TRAFFIC CONTROL AND PROTECTION."



	WORK AREA
	▬ SIGN ON PORTABLE OR PERMANENT SUPPORT
	⊥ TYPE II BARRICADE W/TYPE A FLASHING LIGHT
	⊥⊥ TYPE III BARRICADE

POSTED SPEED LIMIT	DISTANCE BETWEEN SIGNS "D"
40 MPH or less	200 FEET
45-50 MPH	350 FEET
55 MPH	500 FEET

MODIFIED IDOT DISTRICT ONE
SIDE ROAD DETAIL

REVISIONS	DATE
Title Block Revision	8/1/09
Reformat LCDOT Standard	7/15/10

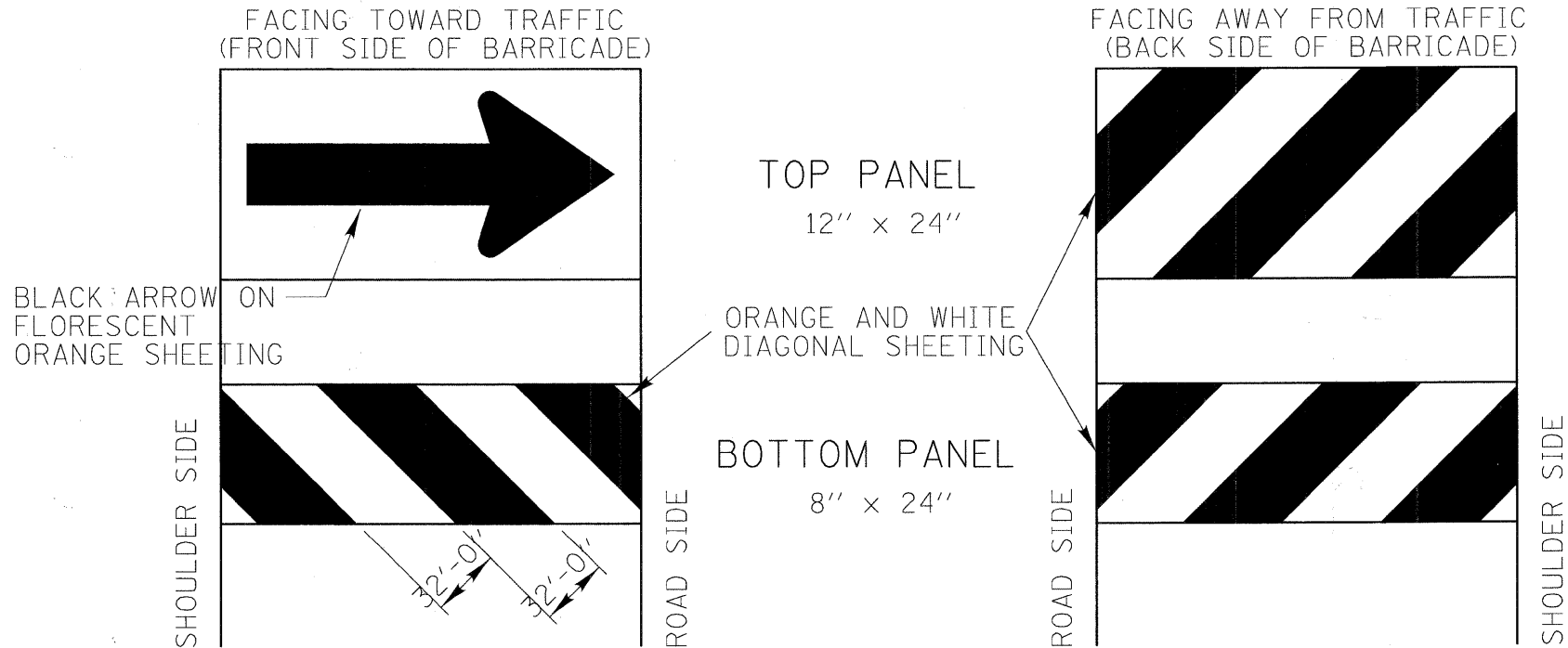
LakeCounty
Division of Transportation

APPROVED BY: ANTHONY KHAWAJA
DATE: APRIL 1, 2007

TRAFFIC CONTROL and PROTECTION
for SIDEROADS, INTERSECTIONS
and DRIVEWAYS

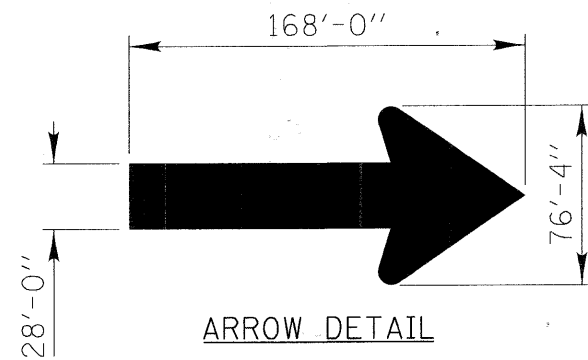
LC7004

DIRECTION INDICATOR BARRICADES



GENERAL NOTES

- 1) Direction Indicator Barricades shall be constructed from non-metallic Type II barricades meeting the requirements of Article 1106.02 of the Standard Specifications, except where modified by this detail.
- 2) The Direction Indicator Barricades shall be equipped with Type C steady burning lights if used to channelize traffic during the hours of darkness.
- 3) The reflective sheeting for the top panel shall be Type AZ fluorescent orange. The diagonal panels shall have orange and white Type A or better reflective sheeting.



REVISIONS	DATE		

LakeCounty
Division of Transportation

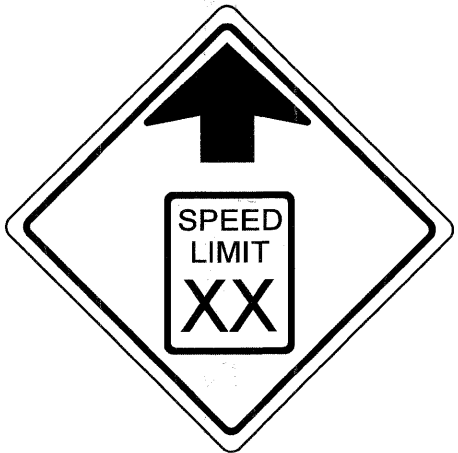
APPROVED BY: ANTHONY KHAWAJA
DATE: APRIL 1, 2007

LC7200

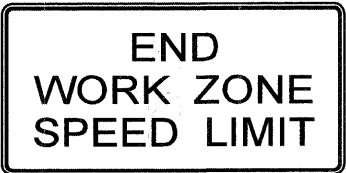
TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES
HIGHWAY CONSTRUCTION, CONTRACT MAINTENANCE

DIRECTION INDICATOR BARRICADES

WORK ZONE SPEED LIMIT SIGNS



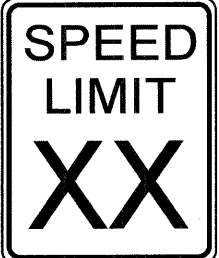
W3-5 (O) - 48



G20-I 102, 24"X48"



W2-I 115 (O) 3015



R2-1 3036



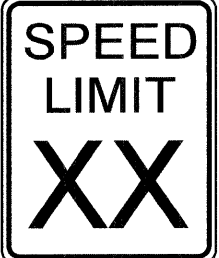
W2-I 113 (O) 3011



R2-I 106 3015



W2-I 115 (O) 3015

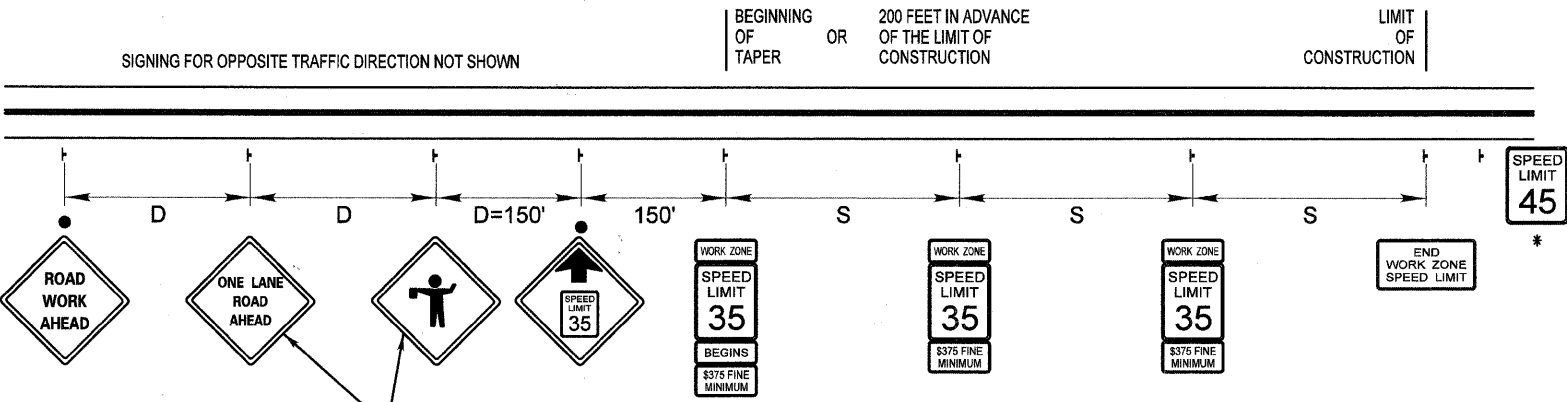


R2-1 3036



R2-I 106 3015

R2-1 SPECIAL



THESE SIGNS WILL VARY DEPENDING ON CONSTRUCTION ACTIVITY

NOTE: SPEED LIMIT VALUES SHOWN FOR EXAMPLE ONLY

ADVANCE WARNING SIGN SPACING TABLE	
ROAD TYPE (EXISTING SPEED LIMIT)	DISTANCE BETWEEN SIGNS "D"
URBAN-LOW SPEED (40 MPH or less)	200 FEET
URBAN-HIGH SPEED (45-50 MPH)	350 FEET
RURAL (55 MPH or greater)	500 FEET

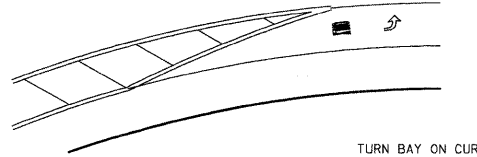
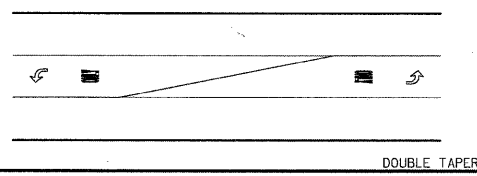
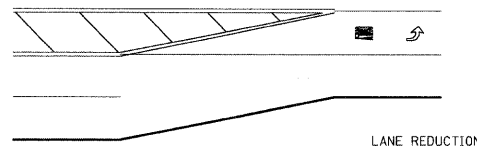
WORK ZONE SPEED LIMIT SIGN SPACING	
WORK ZONE SPEED LIMIT	DISTANCE BETWEEN SIGNS "S"
35 TO 40 MPH	990 FEET to 1,980 FEET
45 TO 50 MPH	1,320 FEET to 2,640 FEET

- NOTES:**
- * = EXISTING SPEED LIMIT BEFORE CONSTRUCTION.
 - THE SIGN SPACING BETWEEN WORK ZONE SPEED LIMIT SIGNS SHALL BE THE SAME AS COMPARABLE REGULAR SPEED LIMIT SIGNING AS STATED IN THE L.C.D.O.T. POLICY FOR THE ESTABLISHMENT AND POSTING OF SPEED LIMITS ON COUNTY AND TOWNSHIP HIGHWAYS WITHIN LAKE COUNTY, ILLINOIS. A MINIMUM OF 2 WORK ZONE SPEED LIMIT SIGNS SHALL BE REQUIRED PER DIRECTION OF TRAFFIC.
 - EXISTING SPEED LIMIT SIGNS AND POST SHALL BE REMOVED AND RETURNED TO THE L.C.D.O.T.'S SIGN SHOP WITHIN 24 HOURS OF REMOVAL.
 - ON MULTILANE HIGHWAYS, WORK ZONE SPEED LIMIT AND ADVANCE CONSTRUCTION SIGNING SHALL BE ON THE RIGHT SIDE RIGHT-OF-WAY AND THE LEFT MEDIAN, WHEN THE MEDIAN IS NOT PAVED AND THE WIDTH IS 10 FEET OR GREATER.
 - ALL SIGNS SHALL BE PERMANENTLY POST MOUNTED IF THE SIGNS WILL BE DISPLAYED FOR MORE THAN 48 HOURS.

<table border="1"> <thead> <tr> <th>REVISIONS</th> <th>DATE</th> </tr> </thead> <tbody> <tr> <td>Reformat LCDOT Standard</td> <td>7/15/10</td> </tr> </tbody> </table>	REVISIONS	DATE	Reformat LCDOT Standard	7/15/10		APPROVED BY: A. KHAWAJA DATE: 4/14/09	LC7203
REVISIONS	DATE						
Reformat LCDOT Standard	7/15/10						
WORK ZONE SPEED LIMIT SIGNING DIAGRAM (SHEET 1 OF 2)							

<table border="1"> <thead> <tr> <th>REVISIONS</th> <th>DATE</th> </tr> </thead> <tbody> <tr> <td>Reformat LCDOT Standard</td> <td>7/15/10</td> </tr> </tbody> </table>	REVISIONS	DATE	Reformat LCDOT Standard	7/15/10		APPROVED BY: A. KHAWAJA DATE: 4/14/09	LC7203
REVISIONS	DATE						
Reformat LCDOT Standard	7/15/10						
WORK ZONE SPEED LIMIT SIGNING DIAGRAM (SHEET 2 OF 2)							

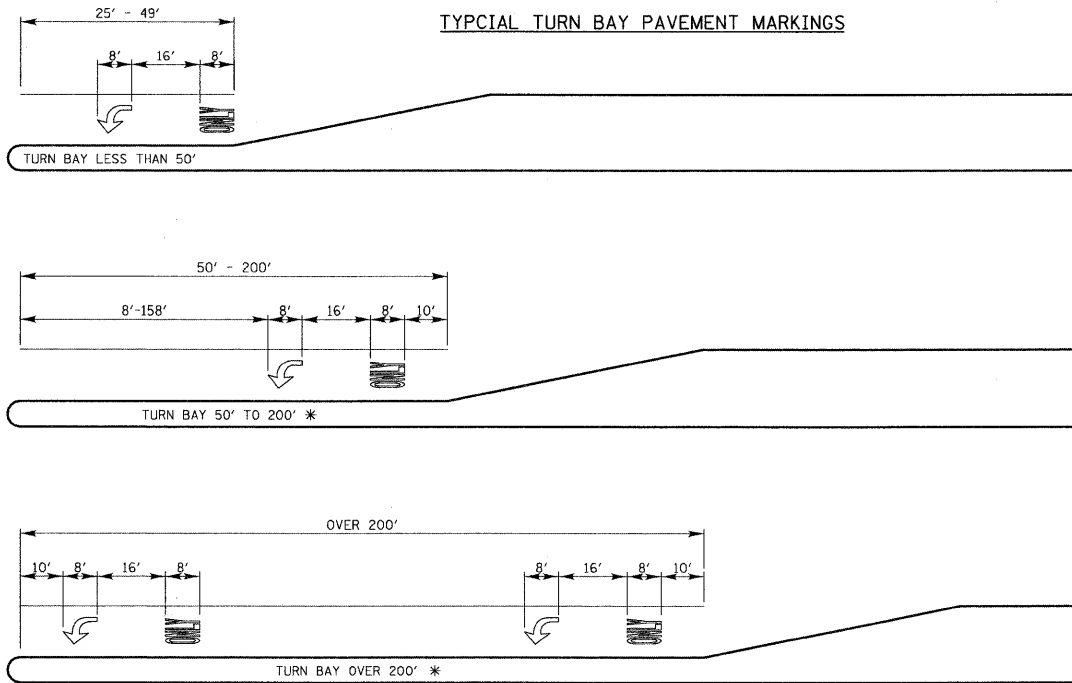
TYPICAL MINI-SKIP PAVEMENT MARKINGS



MINI-SKIPS ARE 2 FEET WHITE LINE WITH 6 FEET SPACING. THE MINI-SKIP IS THE SAME WIDTH AS THE PAVEMENT MARKING LINE, IT EXTENDS.

TYPICAL PAVEMENT MARKINGS

TYPICAL TURN BAY PAVEMENT MARKINGS



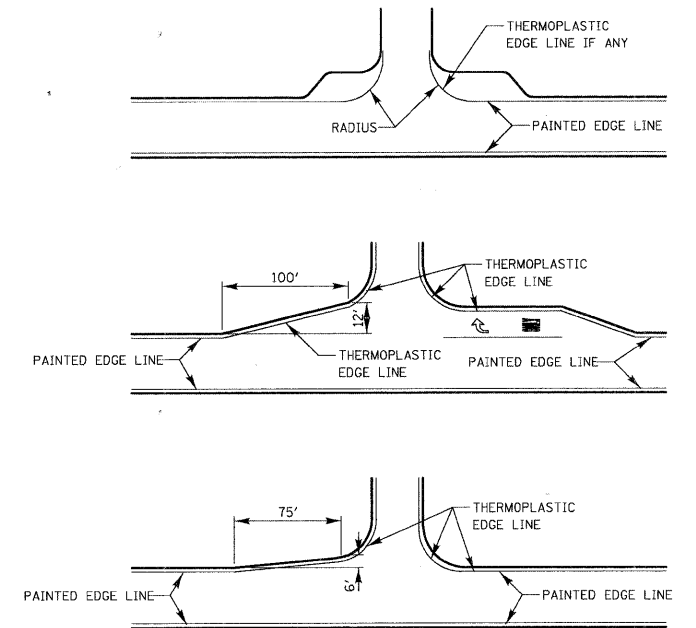
* AT INTERSECTIONS WITH VIDEO DETECTION, THE ARROW AND ONLY PAVEMENT MARKINGS SHALL BE A MINIMUM OF 30' BEHIND THE STOP BAR.

AREA = 15.6 SQ. FT.

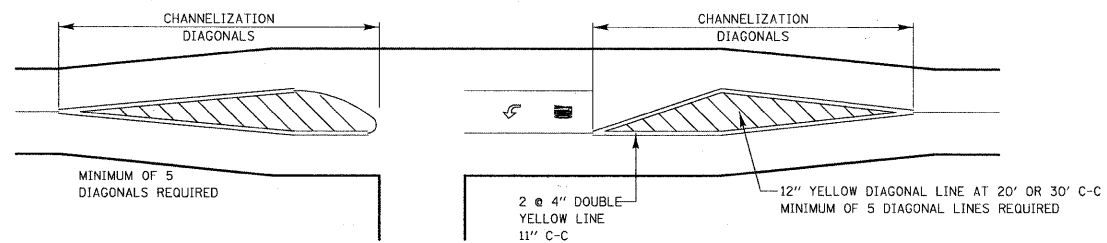
AREA = 20.8 SQ. FT.

FULL SIZE LETTERS (8") AND ARROWS SHALL BE USED. TURN LANES IN EXCESS OF 400' IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW W/ "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW W/ "ONLY".

EDGE LINE RADII AT SIDE STREETS

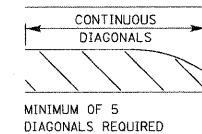


TWO LANE ROAD

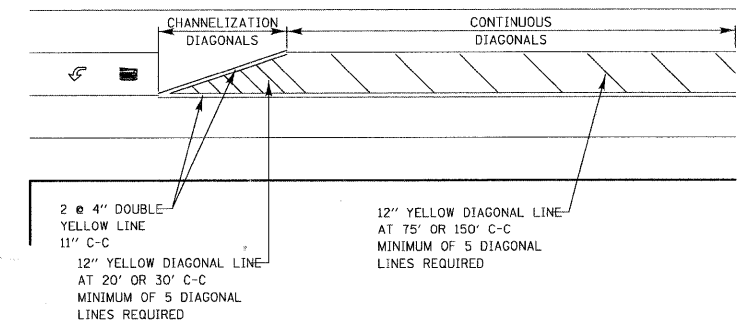


TYPICAL DIAGONAL SPACING

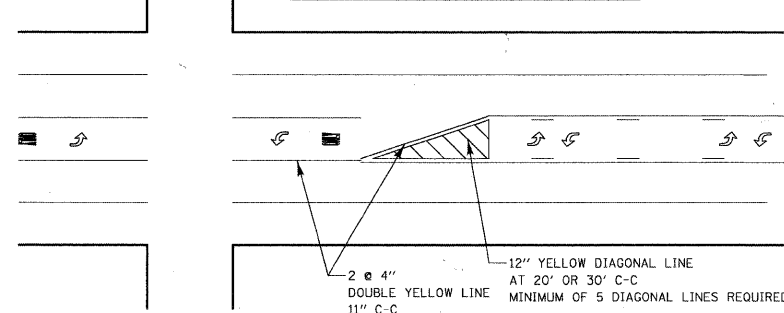
SPEED LIMIT RANGE	DIAGONAL SPACING	
	CONTINUOUS	INTERSECTION CHANNELIZATION
30-45 MPH	75 FT.	20 FT.
OVER 45 MPH	150 FT.	30 FT.



3 TO 5 LANE ROAD



TWO-WAY LEFT TO LEFT TURN BAY



DUAL LEFT TURN ARROWS



31.2 SQ. FT. MINIMUM OF 2 SETS REQUIRED

A MINIMUM OF TWO PAIRS OF DUAL LEFT TURN ARROWS SHALL BE USED. THE DUAL LEFT TURN ARROWS SHALL BE WHITE IN COLOR. THE INTERVAL BETWEEN SETS OF DUAL LEFT TURN ARROWS SHOULD BE 200' AND 300'.

REVISIONS	DATE	APPROVED BY
Separated Railroad Sheet	6/2/2008	A. KHAWAJA

Lake County
Division of Transportation

DATE: APRIL 1, 2007

TYPICAL PAVEMENT MARKINGS FOR COUNTY HIGHWAYS
SHEET 1 OF 2

NO.	DESCRIPTION	DATE	BY	SURVEYOR

DESIGNER: TC
DRAWN: PWL
CHECKED: MRH
DATE: 10/25/2010



ROUTE	SECTION	SECTION NUMBER	SHEET	SHEETS
CHXX	XXX	XX-XXXX-XX-XX	XXX	XXX

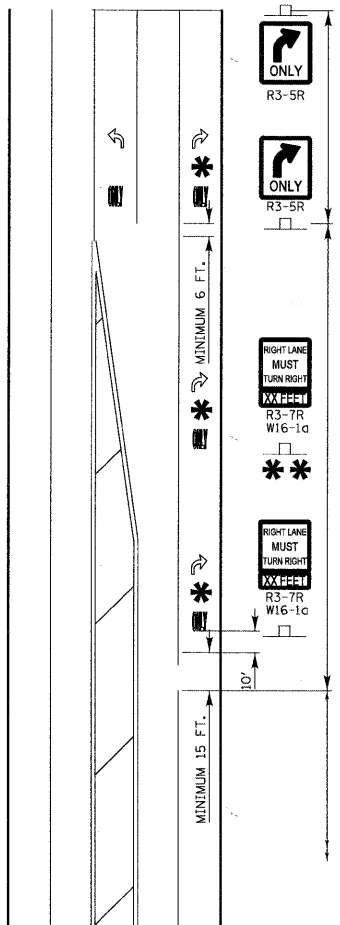
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FILE NAME	USER NAME	DESIGNED	REVISIONS	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
LC0907-sh1-deta1a5.dgn	320200707	TC				2626	05-00193-04-BR	LAKE	36	31

SCALE: N/A SHEET NO. 1 OF 1 SHEETS STA. N/A TO STA. N/A

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT CONTRACT NO. 63531

THRU LANE TO
TURN LANE CONVERSION



TYPICAL PAVEMENT MARKINGS AND RAISED PAVEMENT MARKERS

TURN BAY
6" WHITE LINE
(ADDITIONAL PAV'T MARKINGS AS SHOWN
ON SHEET ONE OF THE L.C.D.O.T. PAV'T
MARKINGS DETAIL SHEETS)

MINIMUM TRANSITION ZONE LENGTH

TRANSITION ZONE
6" WHITE 3/12" SKIP DASH
LANE LINE

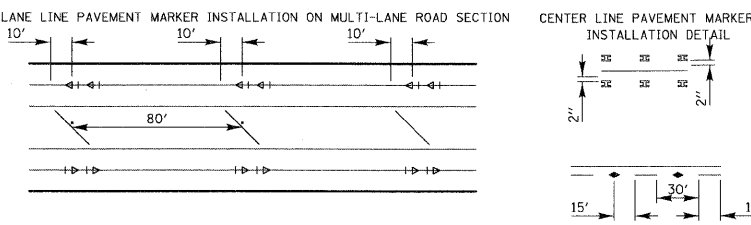
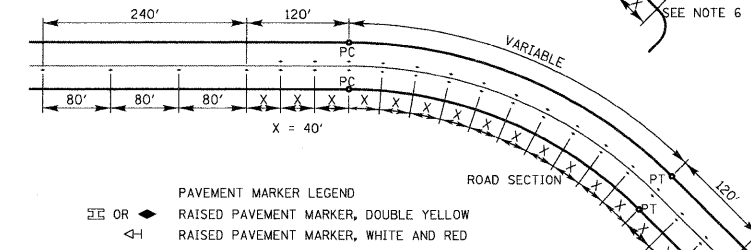
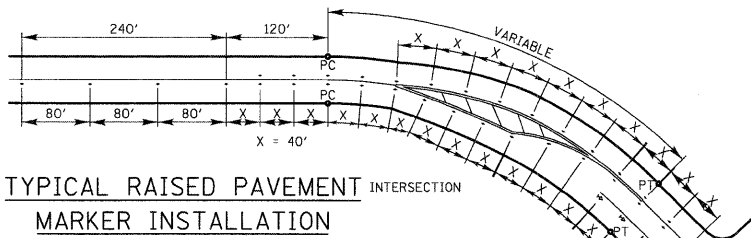
POSTED SPEED	LENGTH
25 M.P.H.	255 FT.
30 M.P.H.	330 FT.
35 M.P.H.	405 FT.
40 M.P.H.	480 FT.
45 M.P.H.	555 FT.
50 M.P.H.	630 FT.
55 M.P.H.	705 FT.

* LOCATION OF PAV'T MARKINGS

(MEASURED FROM BEGINNING OF TRANSITION ZONE)

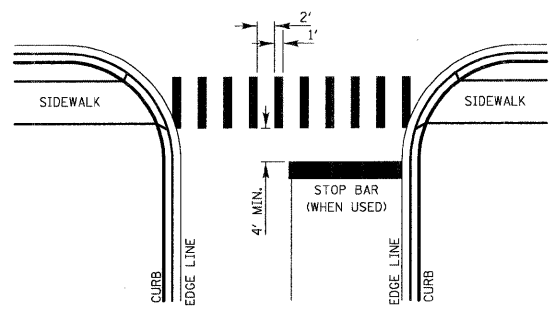
POSTED SPEED	LOCATION OF PAV'T MARKINGS
25 M.P.H.	10 FT., 260 FT.
30 M.P.H.	10 FT., 170 FT., 340 FT.
35 M.P.H.	10 FT., 210 FT., 410 FT.
40 M.P.H.	10 FT., 170 FT., 330 FT., 490 FT.
45 M.P.H.	10 FT., 190 FT., 370 FT., 560 FT.
50 M.P.H.	10 FT., 170 FT., 330 FT., 490 FT., 640 FT.
55 M.P.H.	10 FT., 180 FT., 350 FT., 520 FT., 710 FT.

FOR POSTED SPEEDS 40 M.P.H. OR GREATER
A SECOND R3-7/W16-1c SIGN INSTALLATION
SHALL BE LOCATED HALFWAY BETWEEN THE
BEGINNING OF THE TRANSITION ZONE AND THE
BEGINNING OF THE TURN LANE



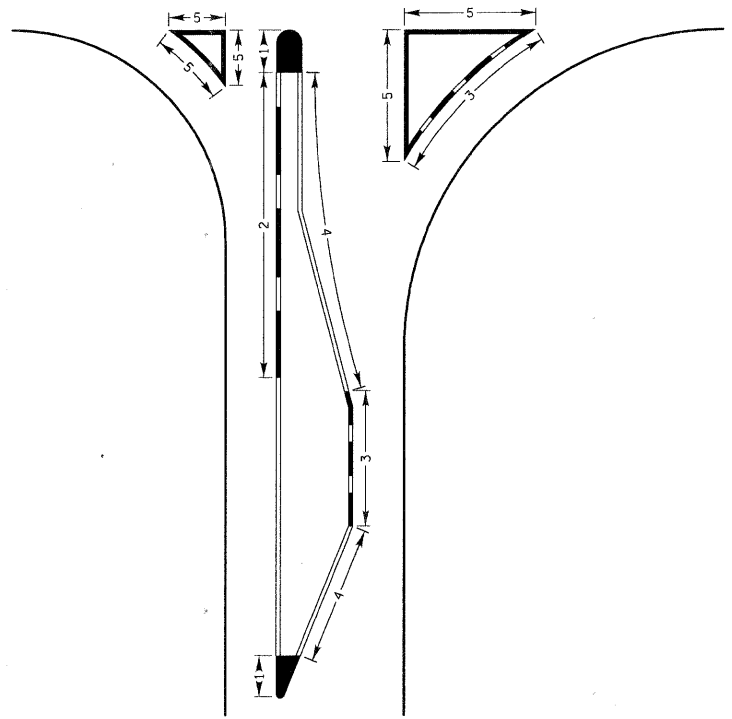
- NOTES:
- CENTERLINE RAISED PAVEMENT MARKERS (RPM'S) SHALL BE PLACED ON ALL CURVES OVER 3 1/2 DEGREES ON ALL TWO AND THREE LANE HIGHWAYS, UNLESS DIRECTED OTHERWISE BY THE ENGINEER..
 - SPACING = 40' FOR CENTERLINE MARKERS.
 - ALL RPM'S ON CENTERLINE ARE 2-WAY YELLOW. LANE LINE MARKERS ARE WHITE/RED.
 - MARKERS SHALL BE INSTALLED IN ACCORDANCE WITH F.H.W.A. MEMORANDUM H10-21.
 - MARKERS SHALL BE FIELD ADJUSTED TO BE LOCATED IN CENTER OF THE 30' GAP OF A 30'/10' SKIP-DASH CENTERLINE.
 - RPM'S WHICH ARE TO BE LOCATED WITHIN THE INTERSECTION OF A CROSS STREET, SHALL NOT BE INSTALLED.
 - A MINIMUM OF 4 WHITE/RED MARKERS SHALL BE INSTALLED ALONG THE TURN LANE LINE.
 - RPM'S INSTALLED ON MULTI-LANE ROAD SECTIONS SHALL BE INSTALLED ON THE WHITE SKIP-DASH LANE LINE ONLY. THESE RPM'S SHALL BE INSTALLED IN PAIRS AND SHALL BE 80' CENTER TO CENTER. SPACING WITHIN EACH PAIR SHALL BE 10', CENTERED WITHIN THE 30' SKIP.

CROSSWALKS



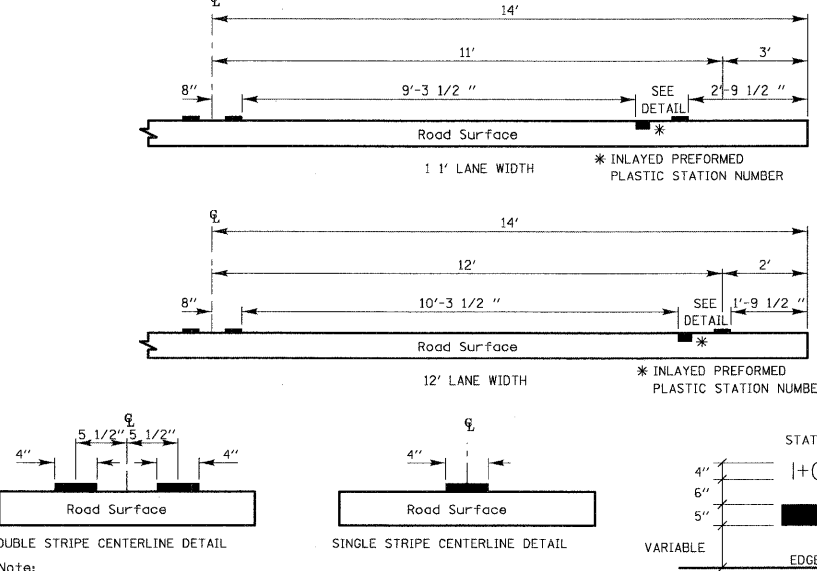
- WIDTH OF THE CROSSWALK IS GENERALLY 6' EXCEPT AT SCHOOL CROSSINGS AND BICYCLE CROSSINGS, WHICH CAN BE 8'.
- THE STOP BAR SHOULD BE INSTALLED A MINIMUM OF 4' IN ADVANCE OF THE CROSSWALK.

CURB MARKING



- NOTES:
- PAINT CURB AND NOSE SOLID FOR 10' OR RADIUS OF NOSE, WHICHEVER IS GREATER.
 - PAINT MINIMUM OF 3 STRIPES IN DIRECTION OF TRAFFIC.
 - REDUCED SPACING USED TO OBTAIN 3 STRIPE MINIMUM.
 - STRIPING RECOMMENDED ONLY WHERE OPERATIONAL PROBLEMS DICTATE.
 - PAINT SOLID WHERE A MINIMUM OF 3 STRIPES CANNOT BE PLACED.

PAVEMENT CROSS SECTION SHOWING TYPICAL PAVEMENT MARKINGS (2-LANE ROADWAY)



PAVEMENT MARKING GUIDELINES				
TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE OF 2 LANE PAVEMENT	4 IN.	SKIP-DASH	YELLOW	10 FT. LINE WITH 30 FT. SPACE
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 IN.	SOLID	YELLOW	5 1/2 IN. C-C FROM SKIP-DASH CENTERLINE
CENTERLINE ON MULTI-LANE UNDIVIDED LANE LINES	2 @ 4 IN.	SOLID	YELLOW	11 IN. C.C
DOTTED LINES (EXTENSIONS OF CENTERLINE OR TURN LANE MARKINGS)	4 IN.	SKIP-DASH	WHITE	10 FT. LINE WITH 30 FT. SPACE
EDGE LINES	5 IN. WHITE 4 IN. YELLOW	SOLID	WHITE - RIGHT YELLOW - LEFT	OUTLINE RAISED MEDIANS IN YELLOW
TURN LANE MARKINGS	6 IN. LINE FULL SIZE LETTERS AND SYMBOLS (8 FT.)	SOLID	WHITE	TURN ARROW 156 SQ. FT. STRAIGHT ARROW 115 SQ. FT. ONLY 200 SQ. FT. COMB. ARROW 260 SQ. FT.
TWO WAY LEFT TURN MARKING	2 @ 4 IN. EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10 FT. LINE WITH 30 FT. SPACE FOR SKIP-DASH 5 1/2 IN. C-C BETWEEN SKIP-DASH LINE AND SOLID LINE.
CROSSWALK	8 FT. LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
STOP BARS	12 IN. @ 90'	SOLID	WHITE	12 IN. LONGITUDINAL BAR WITH 24 IN. SPACE 6 FT. TO 12 FT. WIDE SEE TYPICAL CROSSWALK MARKING DETAIL
PAINTED MEDIANS	2 @ 4 IN. WITH 11 IN. DIAGONALS @ 45'	SOLID	YELLOW - 2-WAY WHITE - 1-WAY TRAFFIC	11 IN. C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING DETAIL MINIMUM OF 5 DIAGONALS
GORE MARKING AND CHANNELIZING LINES	8 IN. WITH 12 IN. DIAGONALS @ 45'	SOLID	WHITE	DIAGONALS 15 FT. C.C. (LESS THAN 30 M.P.H.) 20 FT. C.C. (30 TO 45 M.P.H.) 30 FT. C.C. (OVER 45 M.P.H.) MINIMUM OF 5 DIAGONALS
R.R. CROSSING	24 IN. TRANSVERSE LINES RR IS 6 FT. LETTER 16 IN. LINE FOR 'X'	SOLID	WHITE	SEE I.D.O.T. STD. 780001 50 FT. AREA OF "R" - 36 SQ. FT. / "R" "X" - 540 SQ. FT.
SHOULDER DIAGONALS	12 IN. @ 45'	SOLID	WHITE - RIGHT YELLOW - LEFT	50 FT. C.C. (LESS THAN 30 M.P.H.) 75 FT. C.C. (30 TO 45 M.P.H.) 150 FT. C.C. (OVER 45 M.P.H.) MINIMUM OF 5 DIAGONALS

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO PART III "MARKINGS" IN THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES".
THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND I.D.O.T. HIGHWAY STANDARD 780001 EFFECTIVE JAN. 3, 1998.

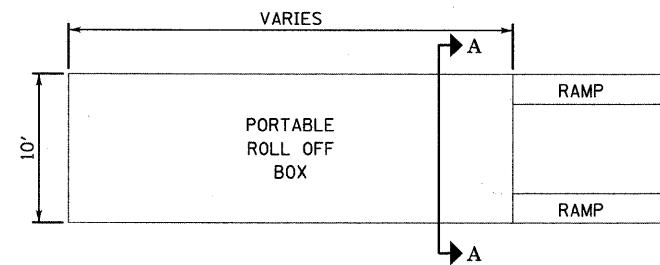
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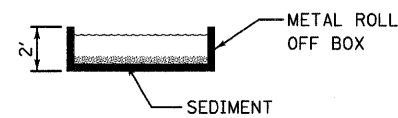
REVISIONS	DATE	APPROVED BY
Superseded Railroad Sheet	6/2/2008	A. KHAWAJA

Lake County
Division of Transportation

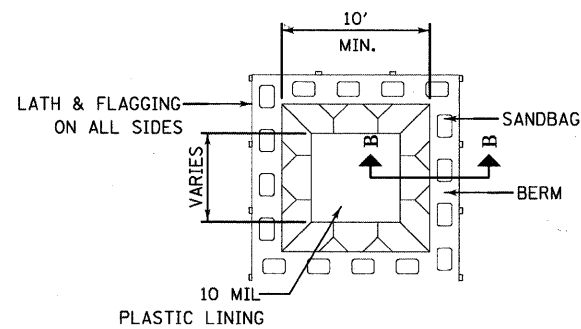
TYPICAL PAVEMENT MARKINGS
FOR COUNTY HIGHWAYS
SHEET 2 OF 2



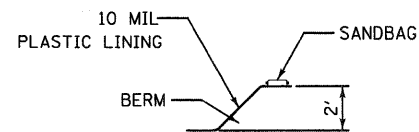
PLAN VIEW PREFAB PORTABLE WASHOUT



SECTION A-A



PLAN VIEW BELOW GRADE



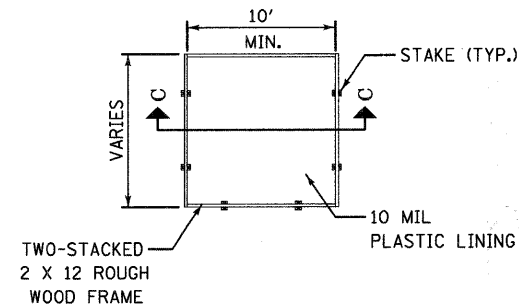
SECTION B-B

NOTES:

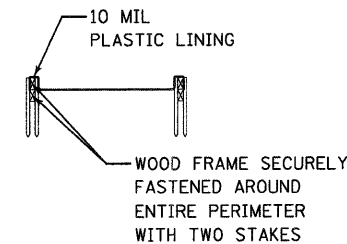
1. ACTUAL LAYOUT DETERMINED IN FIELD.
2. OTHER WASHOUT DESIGNS MAY BE USED IF APPROVED BY THE ENGINEER.
3. THE CONCRETE WASHOUT SIGN SHALL BE INSTALLED WITHIN 30 FEET OF THE TEMPORARY CONCRETE WASHOUT FACILITY.

NOT TO SCALE

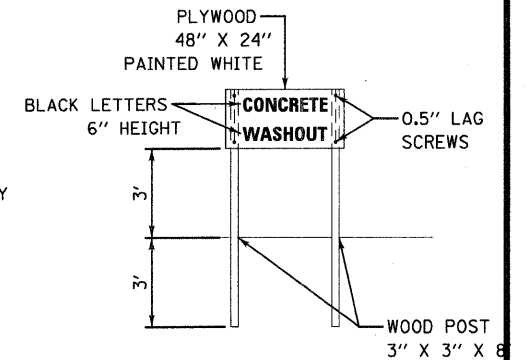
REVISIONS	DATE	APPROVED BY: MGZ
		DATE: March 17, 2008
		CONCRETE WASHOUT FACILITIES
		SHEET 1 OF 2



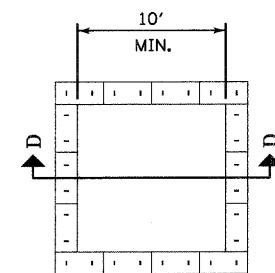
PLAN VIEW ABOVE GRADE



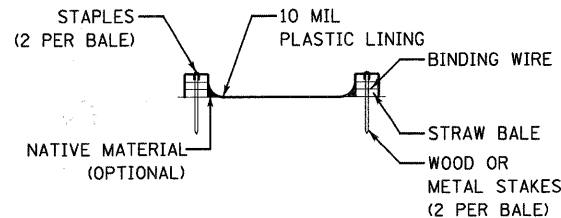
SECTION C-C



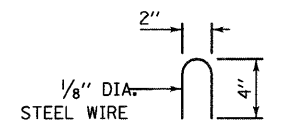
CONCRETE WASHOUT SIGN DETAIL (OR EQUIVALENT)



PLAN VIEW ABOVE GRADE WITH STRAW BALES



SECTION D-D



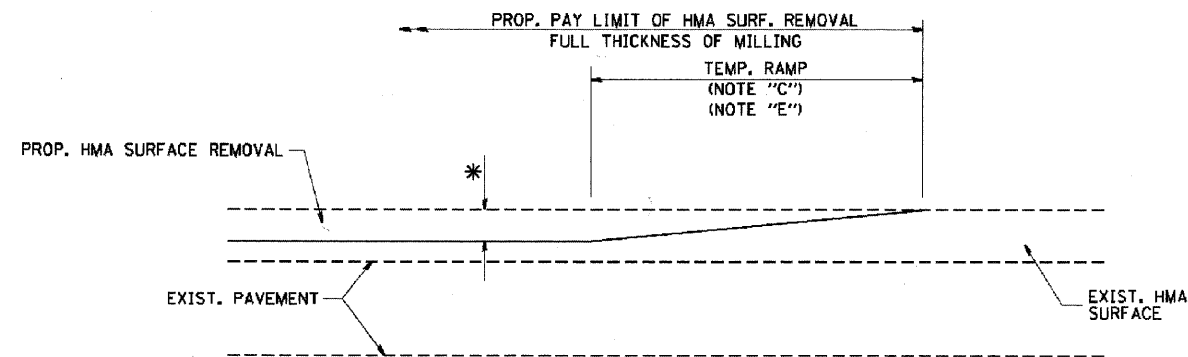
STAPLE DETAIL

NOTES:

1. ACTUAL LAYOUT DETERMINED IN FIELD.
2. OTHER WASHOUT DESIGNS MAY BE USED IF APPROVED BY THE ENGINEER.
3. THE CONCRETE WASHOUT SIGN SHALL BE INSTALLED WITHIN 30 FEET OF THE TEMPORARY CONCRETE WASHOUT FACILITY.

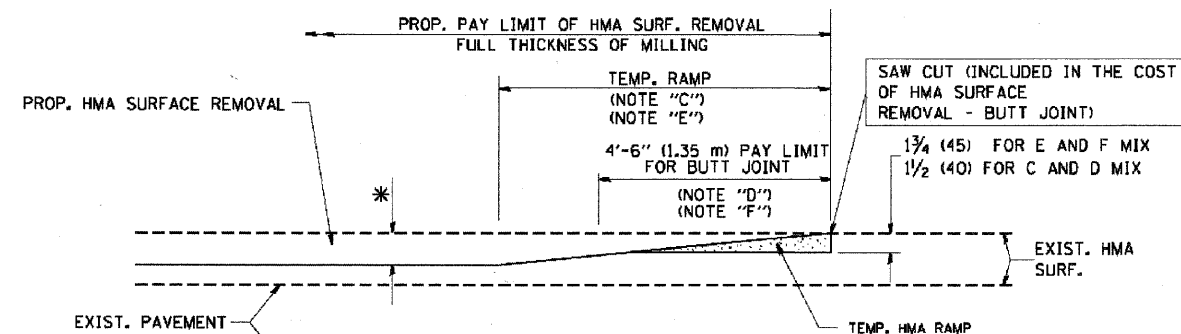
NOT TO SCALE

REVISIONS	DATE	APPROVED BY: MGZ
		DATE: March 17, 2008
		CONCRETE WASHOUT FACILITIES
		SHEET 2 OF 2



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

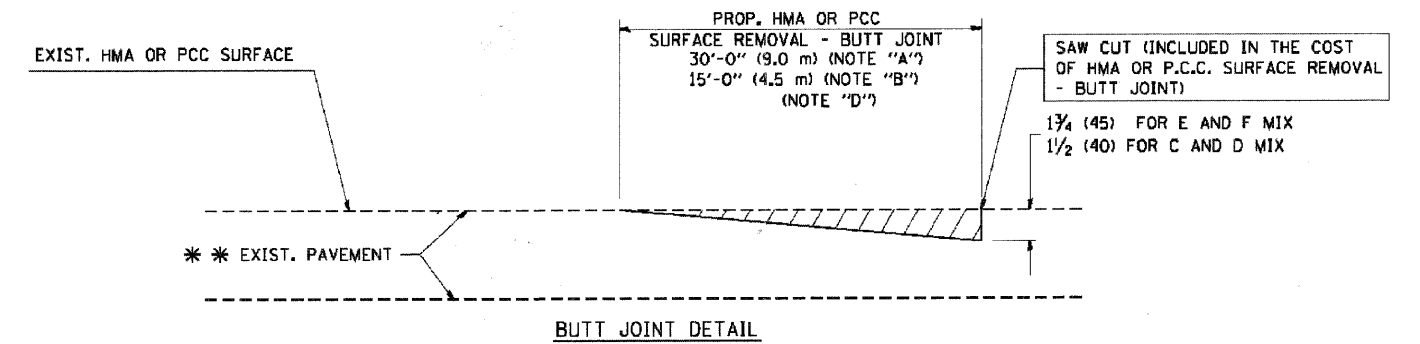
OPTION 1



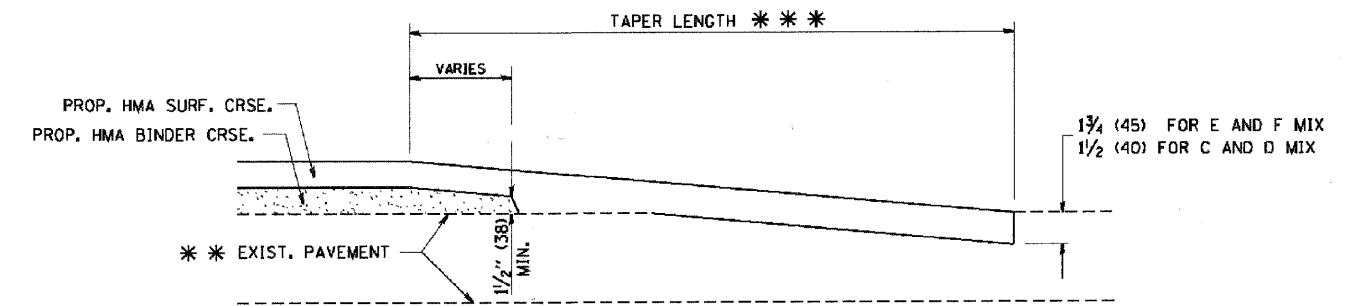
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

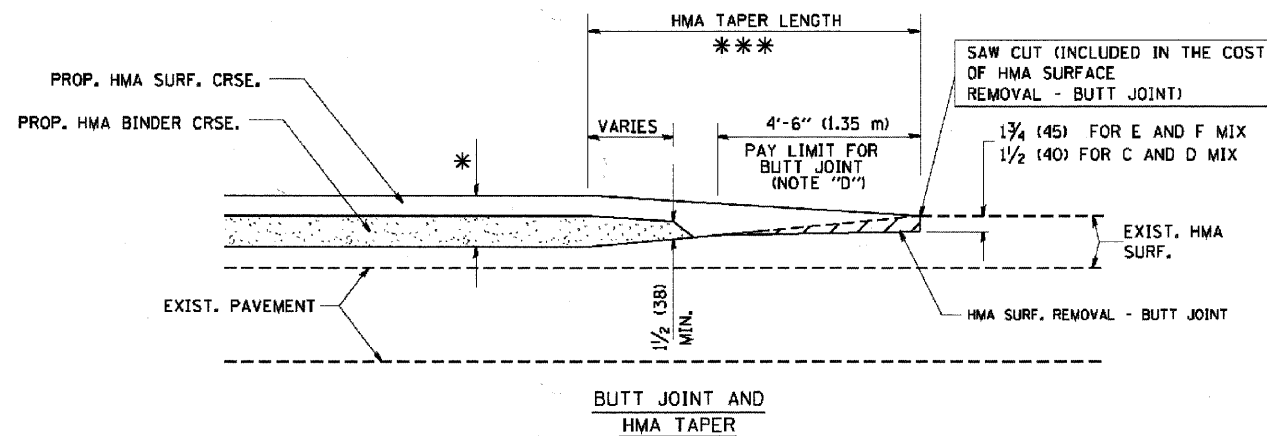
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

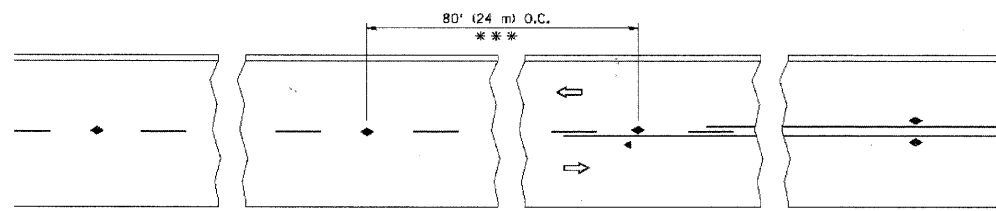
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

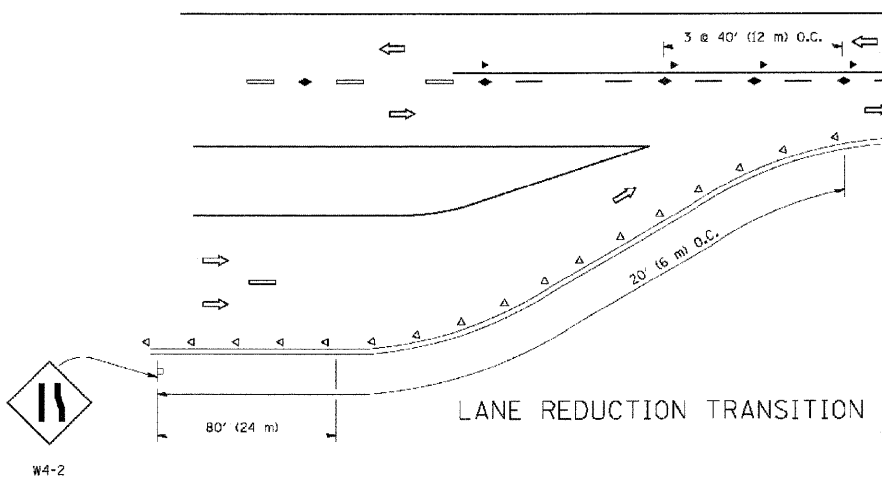
TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

FILE NAME = W:\diststd\22x34\bd32.dgn	USER NAME = gagliemobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINT AND HMA TAPER DETAILS		F.A. RTE. 2626	SECTION 05-00193-04-BR	COUNTY LAKE	TOTAL SHEETS 36	SHEET NO. 34	
PLOT SCALE = 50.00000' / IN.	DRAWN -	CHECKED -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. N/A	TO STA. N/A	BD400-05 BD32 CONTRACT NO. 63531			
PLOT DATE = 1/4/2020	DATE - 06-13-90	CHECKED -	REVISED - M. GOMEZ 04-06-01		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
		DATE -	REVISED - R. BORO 01-01-07									

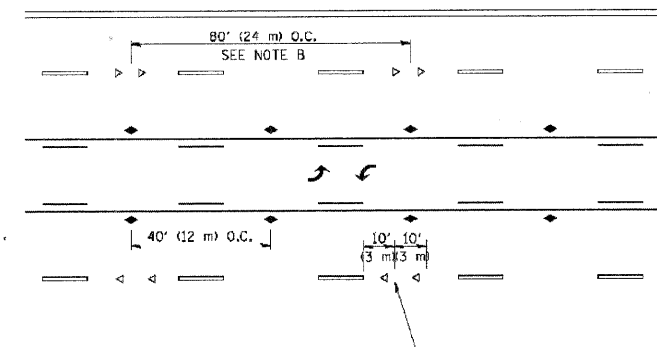


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

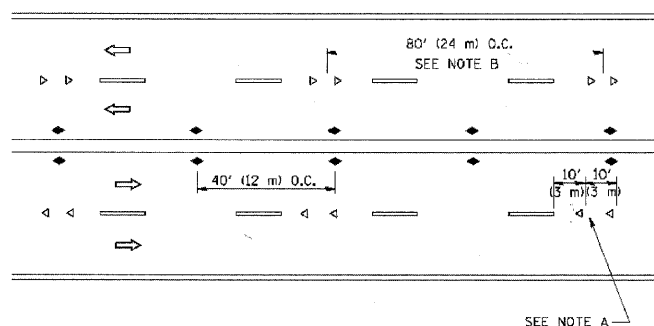
TWO-LANE/TWO-WAY



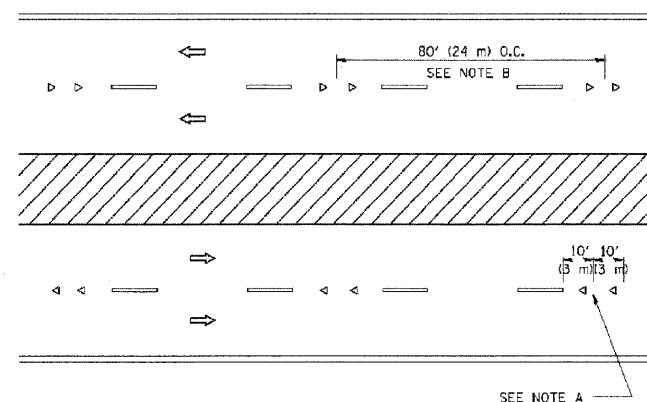
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

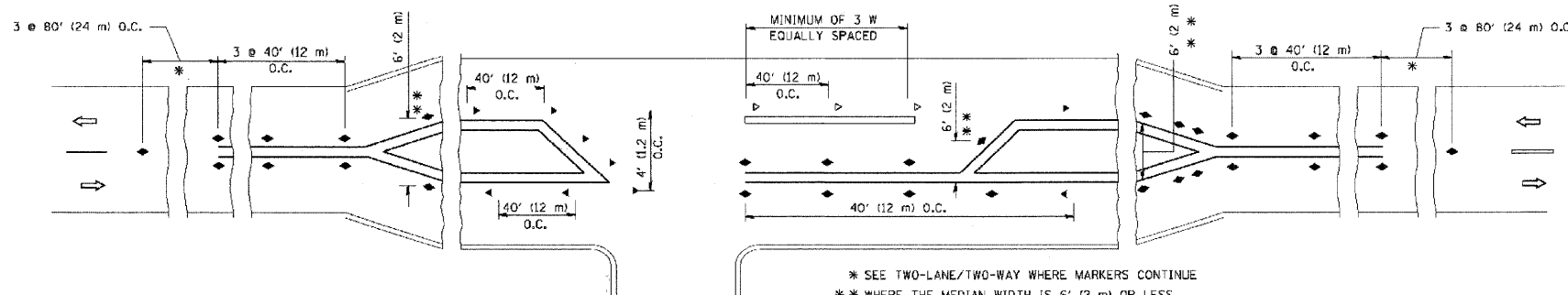
- YELLOW STRIPE
- WHITE STRIPE
- ◄ ONE-WAY AMBER MARKER
- < ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME -	USER NAME - dr.vakosgn	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PROJECT -	PI 01 SCALE -	DRAWN -	REVISED - T. RAMMACHER 03-12-99		2626	05-00193-04-BR	LAKE	36	35			
PI 01 DATE -	PI 01 DATE -	CHECKED -	REVISED - T. RAMMACHER 01-06-00		TC-11			CONTRACT NO. 63531				
PI 01 DATE -	PI 01 DATE -	DATE -	REVISED - C. JUCIUS 09-09-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. N/A	TO STA. N/A	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

