

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
 DIVISION OF HIGHWAYS

**PROPOSED  
 HIGHWAY PLANS**

FAI ROUTE 57 (NORTHBOUND ONLY) (I-57)  
 SECTION (13-1)R & (25-6-1)R  
 PROJECT: IM-057-3(159)142  
 RUBBLIZATION & OVERLAY  
 CLAY & EFFINGHAM COUNTIES

C-97-132-09

FOR INDEX OF SHEETS, SEE SHEET NO. 2

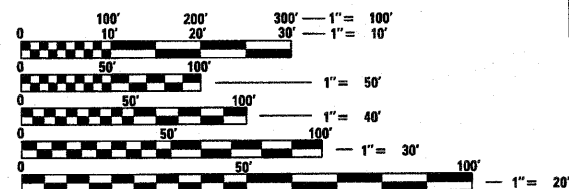
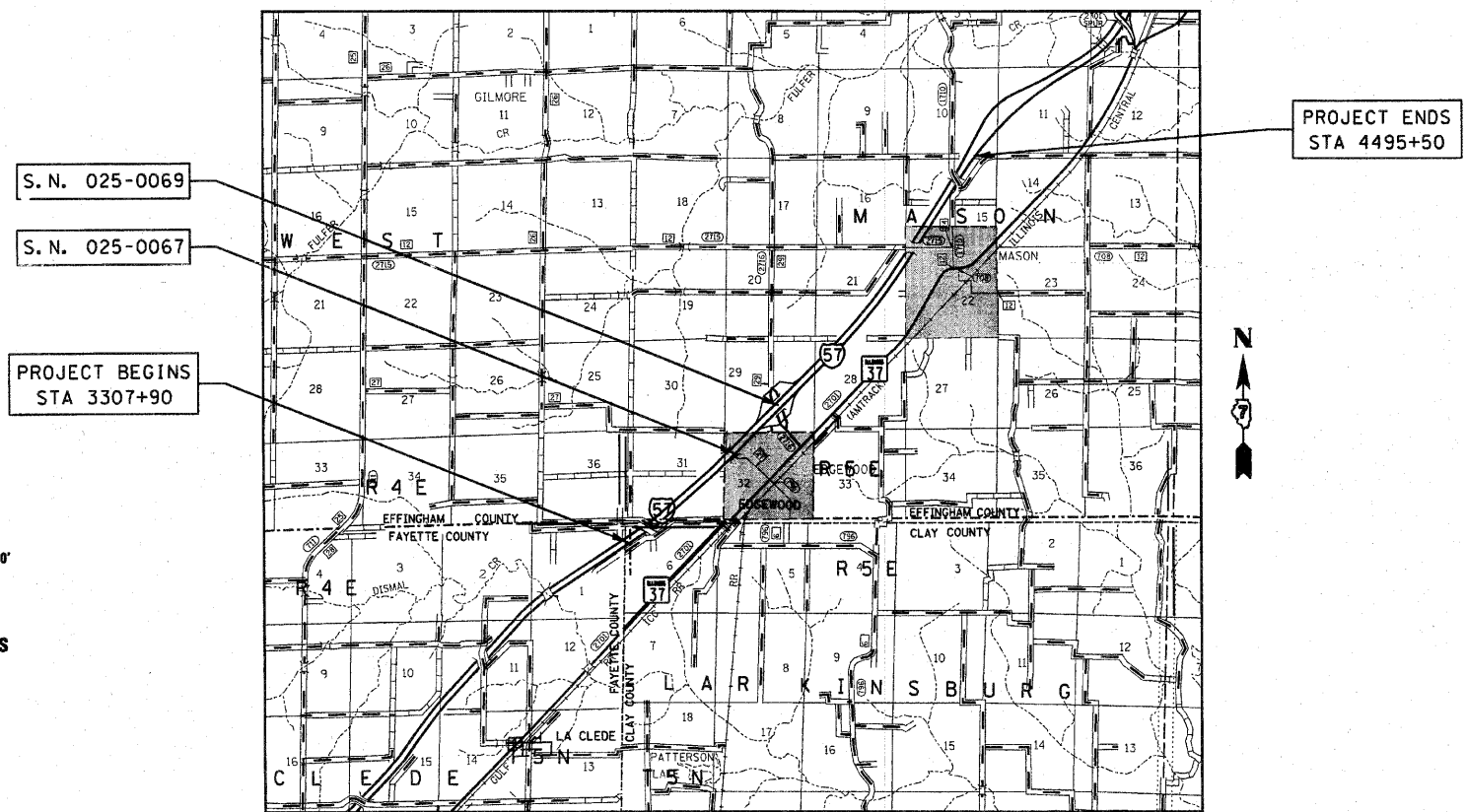
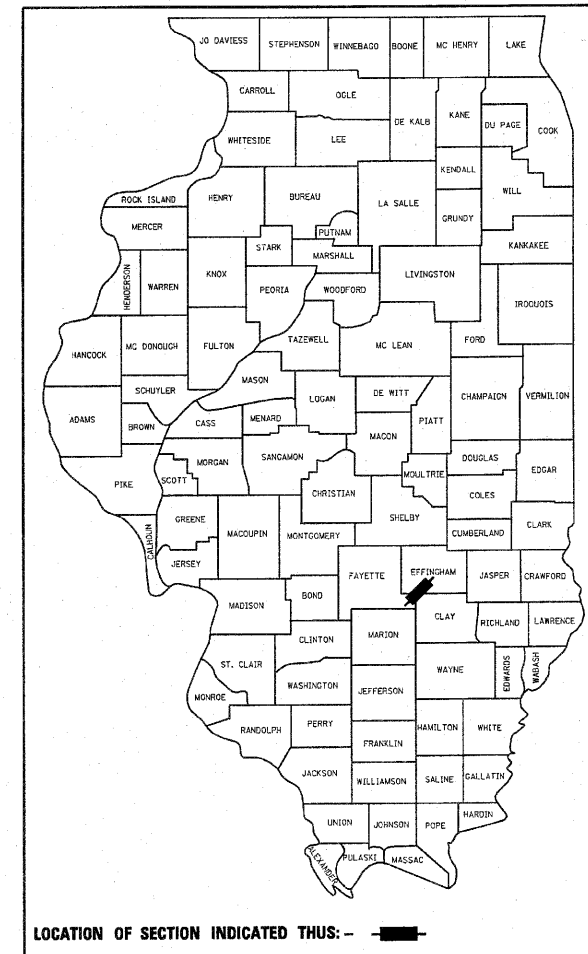
2009 ADT = 14,500

EQUATION:  
 STA 3319+28.74 BK = 4199+97.27 AH

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(13-1)R&(25-6-1)R	ILLINOIS	44	1
ILLINOIS CONTRACT NO. 74417				

• Clay, Effingham

D-97-075-09



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
 1-800-892-0123  
 OR 811

PROJECT ENGINEER: MARK DAUGHERTY  
 PROJECT MANAGER: BRIAN LEWIS

GROSS LENGTH = 30,469.47 FT. = 5.77 MILE  
 NET LENGTH = 30,469.47 FT. = 5.77 MILE

CONTRACT NO. 74417

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
 DIVISION OF HIGHWAYS

SUBMITTED October 21, 2010  
Ronald Orskell, Jr.  
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

December 10, 2010  
Scott E. Stitt, P.E.  
 acting ENGINEER OF DESIGN AND ENVIRONMENT

December 10, 2010  
Christine M. Reed, P.E.  
 DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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 OF THE STATE OF ILLINOIS**

**GENERAL NOTES**

THIS SECTION SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE PLANS, THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2007; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" INDICATED ON THE CHECK SHEET, AND "THE SPECIAL PROVISIONS" INCLUDED IN THE PROPOSAL.

THE WORK INCLUDED IN SECTION (13-1)R & (25-6-1)R CONSISTS OF HMA SURFACE REMOVAL, PCC PAVEMENT RUBBLIZATION, PAVEMENT PATCHING, RESURFACING WITH HMA, REPAIRS ON TWO BRIDGE STRUCTURES AND OTHER WORK NECESSARY TO COMPLETE THE SECTION.

PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING STRUCTURES HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. IT SHALL BE THE CONTRACTOR RESPONSIBILITY TO VERIFY SUCH DIMENSIONS AND DETAILS IN THE FIELD AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO THE CONSTRUCTION OR ORDERING OF MATERIAL. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION OR A CHANGE IN THE SCOPE OF THE WORK. THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.

SHORT TERM PAVEMENT MARKING ON THE MILLED SURFACE SHALL BE PAINT. ALL OTHER SHORT TERM PAVEMENT MARKINGS SHALL BE TAPE.

THE ACTUAL LOCATIONS OF PAVEMENT PATCHING SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.

ALL DISTURBED AREAS WITHIN THE CONSTRUCTION LIMITS SHALL BE SEEDED AND MULCHED. SEEDING SHALL BE CLASS 2 IN ACCORDANCE WITH THE APPLICABLE ARTICLES OF SECTION 250 OF THE STANDARD SPECIFICATIONS. COST SHALL BE INCLUDED IN THE BID PRICE FOR THE ITEMS CAUSING THE DISTURBANCE.

TREES SHALL BE PRESERVED THROUGHOUT THIS SECTION AS DIRECTED BY THE ENGINEER. THE INFIELDS OF THE EDGEWOOD INTERCHANGE AND THE MEDIAN BETWEEN STATIONS 4455+00 AND 4495+00 ARE PLANTED IN PRAIRIE GRASS AND ARE OFF-LIMITS FOR PARKING AND STORAGE OF MATERIALS AND MACHINERY.

THE LOCATIONS AND/OR DEPTHS OF UNDERGROUND UTILITIES SHOWN HAVE BEEN TAKEN FROM INFORMATION FURNISHED BY THE UTILITY OWNERS AND MUST BE CONSIDERED APPROXIMATE. FIELD MARKINGS OF ACILITIES IN CRITICAL AREAS MAY BE OBTAINED BY PROVIDING A MINIMUM OF 96 HOURS ADVANCE NOTICE THROUGH THE J.U.L.I.E. SYSTEM BY CALLING 800-892-0123.

THE CONTRACTOR SHALL PROVIDE INTERNET ACCESS TO THE BITUMINOUS PLANT QUALITY CONTROL LAB SO THAT BITUMINOUS PLANT REPORTS CAN BE E-MAILED TO THE DISTRICT HEADQUARTERS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICES FOR OTHER ITEMS IN THE CONTRACT.

THE RESIDENT ENGINEER SHALL BE THE SOLE JUDGE CONCERNING THE CURING TIME FOR THE BITUMINOUS SURFACE COURSE.

THE MATERIAL USED FOR BITUMINOUS MATERIALS (PRIME COAT) SHALL BE RC-70, SS-1H, OR SS-1HP APPLIED AT THE RATE DIRECTED BY THE ENGINEER. THE CONTRACTOR WILL BE REQUIRED TO FOG COAT ALL BINDER LIFTS WITH SS-1HP. THE TOP OF THE RUBBLIZED PAVEMENT WILL NOT BE PRIMED.

A MATERIAL TRANSFER DEVICE SHALL BE USED ON THE FINAL LIFT OF BINDER (2.25") AND ON THE SMA SURFACE LIFT (2") THROUGHOUT THE MAINLINE INTERSTATE PORTION OF THE PROJECT. USE OF A MATERIAL TRANSFER DEVICE ON THE RUBBLIZED PCC WILL BE PROHIBITED. A LOADED MATERIAL TRANSFER DEVICE MAY CROSS STRUCTURES 025-0067 AND 025-0069.

THE FACTOR USED TO COMPUTE THE QUANTITY OF THE STONE MATRIX ASPHALT SURFACE COURSE IS 130 POUNDS PER SQUARE YARD PER INCH AND 112 POUNDS PER SQUARE YARD PER INCH FOR ALL OTHER MIXES.

TOP LIFT OF HMA PAVEMENT (FULL-DEPTH) SHALL BE CONSTRUCTED USING POLYMERIZED HMA SURFACE COURSE, SMA. COST SHALL BE INCLUDED IN THE BID PRICE FOR HMA PAVEMENT (FULL-DEPTH).

AGGREGATE FOR AGGREGATE SHOULDERS SHALL BE CRUSHED STONE OR CRUSHED CONCRETE.

COST OF REMOVAL OF EXISTING GUARD POSTS SHALL BE INCLUDED IN THE UNIT PRICE FOR GUARD POST.

COST OF REMOVAL OF TEMPORARY RAMP SHALL BE INCLUDED IN THE UNIT PRICE FOR TEMPORARY RAMP.

THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE TO THIS PROJECT:

APPLICATION	AC/PG	DESIGN AIR VOIDS	MIXTURE COMPOSITION	FRICTION AGGREGATE
POLYMERIZED HMA SURFACE COURSE, SMA, N80	SBS PG 76-22	4.0% @ N=80	IL - 12.5	N/A
POLYMERIZED HMA SURFACE COURSE, MIX "D", N105	SBS PG 70-22	4.0% @ N=105	IL - 9.5	MIX D
HMA SURFACE COURSE, MIX "C", N70	PG 64-22	4.0% @ N=70	IL - 9.5	MIX C
POLYMERIZED HMA BINDER COURSE, IL-19.0, N105	SBS PG 70-22	4.0% @ N=105	IL - 19.0	N/A
HMA BINDER COURSE, IL-19.0, N90	PG 64-22	4.0% @ N=90	IL - 19.0	N/A
HMA BINDER COURSE, IL-19.0L, N30	PG 64-22	4.0% @ N=30	IL - 19.0L	N/A
HMA SHOULDERS	PG 64-22	4.0% @ N=30	IL - 9.5L	N/A
HMA LEVELING BINDER	PG 64-22	4.0% @ N=70	IL - 9.5	N/A
HMA SHOULDERS, 13"	PG 64-22	4.0% @ N=70	IL - 19.0	N/A
CLASS D PATCHING	PG 64-22	4.0% @ N=90	IL - 19.0	N/A

**INDEX OF SHEETS**

SHEET NO	TITLE
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7	PROPOSED TYPICAL SECTIONS - RAMPS & CH 29
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22	DETOUR SIGNING
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THE FOLLOWING STANDARDS ARE A PART OF THESE PLANS AND ARE INCLUDED FOLLOWING THE LAST NUMBERED SHEET OF THE PLANS.

000001-06	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
442201-03	CLASS C AND D PATCHES
515001-03	NAME PLATE FOR BRIDGES
542301-03	PRECAST REINFORCED CONCRETE FLARED END SECTION
601001-04	SUB-SURFACE DRAINS
601101-01	CONCRETE HEADWALL FOR PIPE DRAIN
602301-03	INLET, TYPE A
604066-02	FRAME AND LID, TYPE 15
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
630001-09	STEEL PLATE BEAM GUARDRAIL
631011-07	TRAFFIC BARRIER TERMINAL, TYPE 2
631031-09	TRAFFIC BARRIER TERMINAL, TYPE 6
631033-04	TRAFFIC BARRIER TERMINAL, TYPE 6B
635001-01	DELINEATORS
635006-03	REFLECTOR AND TERMINAL MARKER PLACEMENT
635011-02	REFLECTOR MARKER AND MOUNTING DETAILS
642001-01	SHOULDER RUMBLE STRIPS
701011-02	OFF-ROAD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701101-02	OFF-ROAD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE
701201-04	LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS >= 45 MPH
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701306-03	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS >= 45 MPH
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701336-06	LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES, FOR SPEEDS >= 45 MPH
701400-05	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701401-06	LANE CLOSURE, FREEWAY/EXPRESSWAY
701402-08	LANE CLOSURE, FREEWAY/EXPRESSWAY, WITH BARRIER
701406-06	LANE CLOSURE, FREEWAY/EXPRESSWAY, DAY OPERATIONS ONLY
701411-07	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS >= 45 MPH
701451-01	<del>RAMP CLOSURE, FREEWAY/EXPRESSWAY</del>
701456-01	PARTIAL EXIT RAMP CLOSURE, FREEWAY/EXPRESSWAY
701901-01	TRAFFIC CONTROL DEVICES
704001-06	TEMPORARY CONCRETE BARRIER
720011-01	METAL POSTS FOR SIGNS, MARKERS AND DELINEATORS
780001-02	TYPICAL PAVEMENT MARKINGS
781001-03	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
701426-04	

FILE NAME =	USER NAME = teasleyck	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>GENERAL NOTES &amp; INDEX OF SHEETS</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pw\work\p\dot\teasleyck\d0148697\074417-sht-gennote.dgn	DRAWN -	REVISED -	57			(13-1)R&(25-6-1)R		44	2	
PLOT SCALE = 1/80.0000 "/ IN.	CHECKED -	REVISED -	CONTRACT NO. 74417							
PLOT DATE = 10/22/2010	DATE -	REVISED -	SCALE:			SHEET NO. OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT		

SUMMARY OF QUANTITIES			90% FED./10% STATE	CONSTRUCTION TYPE CODE		
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	CLAY COUNTY	EFFINGHAM COUNTY	
				0004	0004	0014
20800150	TRENCH BACKFILL	CU YD	5.5		5.5	
31101010	SUB-BASE GRANULAR MATERIAL, TYPE B 12"	SQ YD	14032	1737	12295	
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	23886	1710	22176	
40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	671		671	
40600895	CONSTRUCTING TEST STRIP	EACH	2	0.1	1.9	
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	772	213	559	
40600990	TEMPORARY RAMP	SQ YD	265	31	234	
40603090	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90	TON	10847	1237	9610	
40603085	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON	3087	396	2691	
40603153	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80	TON	8684	431	8253	
40603245	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N105	TON	8449	420	8029	
40603315	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N70	TON	1343		1343	
40603550	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N105	TON	600		600	
40702006	HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 16 1/4"	SQ YD	14032	1737	12295	
44000100	PAVEMENT REMOVAL	SQ YD	14032	1737	12295	
44000151	HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"	SQ YD	75089		75089	
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	34869	356	34513	
44000173	HOT-MIX ASPHALT SURFACE REMOVAL, 6"	SQ YD	24544	2431	22113	
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	30		30	
44004250	PAVED SHOULDER REMOVAL	SQ YD	17636	3770	13866	
44201811	CLASS D PATCHES, TYPE I, 14 INCH	SQ YD	20		20	
44201815	CLASS D PATCHES, TYPE II, 14 INCH	SQ YD	400		400	
44201819	CLASS D PATCHES, TYPE III, 14 INCH	SQ YD	380		380	
44201821	CLASS D PATCHES, TYPE IV, 14 INCH	SQ YD	320		320	
48101200	AGGREGATE SHOULDERS, TYPE B	TON	2104	210	1894	
48203049	HOT-MIX ASPHALT SHOULDERS, 13"	SQ YD	17636	3770	13866	
48203100	HOT-MIX ASPHALT SHOULDERS	TON	8659	594	8065	
50102400	CONCRETE REMOVAL	CU YD	26.5		26.5	
50157300	PROTECTIVE SHIELD	SQ YD	476		476	
50300100	FLOOR DRAINS	EACH	40		40	
50300255	CONCRETE SUPERSTRUCTURE	CU YD	26.6		26.6	

SUMMARY OF QUANTITIES			90% FED./10% STATE	CONSTRUCTION TYPE CODE		
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	CLAY COUNTY	EFFINGHAM COUNTY	
				0004	0004	0014
50300260	BRIDGE DECK GROOVING	SQ YD	1587			1587
50300300	PROTECTIVE COAT	SQ YD	1689			1689
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	3640			3640
50800515	BAR SPLICERS	EACH	56			56
52000110	PREFORMED JOINT STRIP SEAL	FOOT	177			177
54213660	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 15"	EACH	1		1	
54390130	INSERTION CULVERT LINER 16"	FOOT	54	8	46	
550A0070	STORM SEWERS, CLASS A, TYPE 1 15"	FOOT	42		42	
60100072	SHOULDER REMOVAL AND REPLACEMENT 5"	FOOT	454	64	390	
60100073	SHOULDER REMOVAL AND REPLACEMENT 10"	FOOT	6.7		6.7	
60107700	PIPE UNDERDRAINS 6"	FOOT	27572	3000	24572	
60108200	PIPE UNDERDRAINS 6" (SPECIAL)	FOOT	1349	190	1159	
60237000	INLETS, TYPE A, TYPE 15 FRAME AND LID	EACH	1		1	
60600096	CLASS SI CONCRETE (OUTLET PROTECTOR)	CU YD	85.5	12	73.5	
60608600	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06	FOOT	30		30	
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SQ FT	240		240	
*63000001	STEEL PLATE BEAM GUARD RAIL, TYPE A, 6 FOOT POSTS	FOOT	2312.5	262.5	2050	
*63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	1		1	
*63100089	TRAFFIC BARRIER TERMINAL, TYPE 6B	EACH	3	1	2	
*63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	7	1	6	
*X6310218	TRAFFIC BARRIER TERMINAL, TYPE 6 (SPECIAL)	EACH	5		5	
63200310	GUARDRAIL REMOVAL	FOOT	2950	375	2575	
63400105	GUARD POSTS	EACH	72	6	66	
63500105	DELINEATORS	EACH	164	5	159	
X6350220	DELINEATOR REMOVAL	EACH	124	5	119	
64200105	SHOULDER RUMBLE STRIPS	FOOT	58836	3786	55050	
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	9	1	8	
67100100	MOBILIZATION	L SUM	1	0.05	0.95	
70100207	TRAFFIC CONTROL AND PROTECTION, STANDARD 701402	EACH	8	2	4	2
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	2		2	
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1		1	

\* Specialty Items

FILE NAME =	USER NAME = teasleyck	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
or \pwwork\pwwidot\teasleyck\d0148697\074417-sht-soq.dgn	4417-sht-soq.dgn	DRAWN -	REVISED -			57	(13-1)R&(25-6-1)R		44	3	
PLOT SCALE = 100.0000' / IN.		CHECKED -	REVISED -			CONTRACT NO. 74417					
PLOT DATE = 10/22/2010		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					

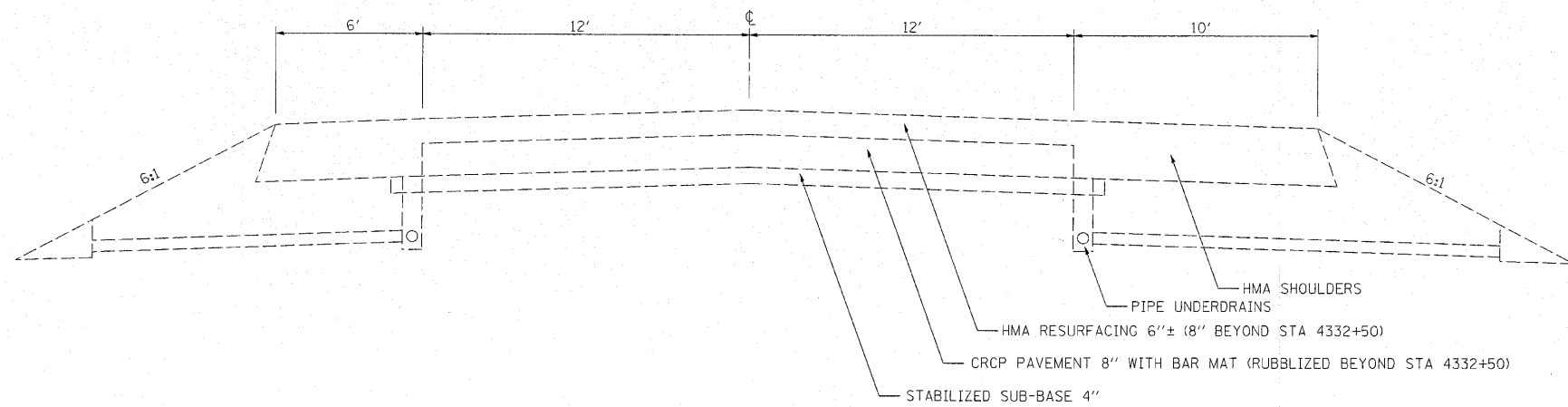
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CODE NO	ITEM	UNIT	TOTAL QUANTITIES	CLAY COUNTY	EFFINGHAM COUNTY	
				0004	0004	0014
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1		1	
70100600	TRAFFIC CONTROL AND PROTECTION, STANDARD 701336	L SUM	1		1	
70100700	TRAFFIC CONTROL AND PROTECTION, STANDARD 701406	L SUM	1		1	
70100800	TRAFFIC CONTROL AND PROTECTION, STANDARD 701401	L SUM	1		1	
<del>70100820</del>	<del>TRAFFIC CONTROL AND PROTECTION, STANDARD 701451</del>	<del>L SUM</del>	<del>1</del>		<del>1</del>	
70100825	TRAFFIC CONTROL AND PROTECTION, STANDARD 701456	L SUM	1		1	
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	10		10	
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	7595	306	7289	
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	84860	3726	81134	
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	2229	102	2127	
70400100	TEMPORARY CONCRETE BARRIER	FOOT	6600	875	5725	
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	6425	875	5550	
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	93.6		93.6	
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	22300		22300	
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	500		500	
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	80		80	
* 78004210	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 4"	FOOT	59164	3301	55863	
* 78004230	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 6"	FOOT	7690	425	7265	
* 78004240	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 8"	FOOT	1012		1012	
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	871	42	829	
* 78100105	RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE)	EACH	12		12	
* 78200410	GUARDRAIL MARKERS, TYPE A	EACH	44	5	39	
* 78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	7	1	6	
78300100	PAVEMENT MARKING REMOVAL	SQ FT	5168	1131	4037	
Δ X0320157	CLEANING UNDERDRAIN OUTLETS	EACH	68		68	
X0323583	SPEED INDICATOR SIGN	CAL DA	200	10	190	
X4060685	TEST STRIP (STONE MATRIX ASPHALT)	EACH	1	0.05	0.95	
X4063500	PRELIMINARY TEST STRIP	EACH	1	0.05	0.95	

\* Specialty Items  
Δ = NON-PARTICIPATING

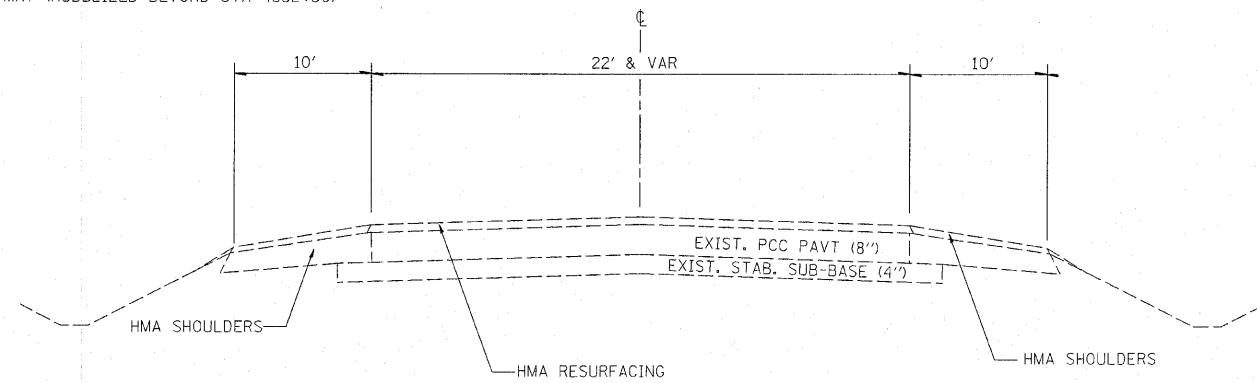
SUMMARY OF QUANTITIES			90% FED / 10% STATE	CONSTRUCTION TYPE CODE		
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	CLAY COUNTY	EFFINGHAM COUNTY	
				0004	0004	0014
X4401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	4176	1268	2908	
X4402020	CONCRETE MEDIAN SURFACE REMOVAL	SQ FT	240		240	
X7015005	CHANGEABLE MESSAGE SIGN	CAL DA	14	1	13	
X7830070	GROOVING FOR RECESSED PAVEMENT MARKING 5"	FOOT	766		766	
X7830074	GROOVING FOR RECESSED PAVEMENT MARKING 7"	FOOT	100		100	
XZ193500	BRIDGE DECK MICROSILICA CONCRETE OVERLAY 2 1/4"	SQ YD	1623			1623
Z0006220	BRIDGE DECK HYDRO-SCARIFICATION 2 1/4"	SQ YD	1623			1623
Z0012710	CONCRETE HEADWALL FOR PIPE UNDERDRAIN REMOVAL	EACH	57	8	49	
Z0015802	PLUG EXISTING DECK DRAINS	EACH	4			4
Z0016001	DECK SLAB REPAIR (FULL DEPTH, TYPE I)	SQ YD	1			1
Z0016002	DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	81			81
Z0016702	DETOUR SIGNING	L SUM	1		1	
Z0030250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	4	1		3
Z0030260	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	1		1	
Z0030330	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE), TEST LEVEL 3	EACH	1		1	
Z0030350	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3	EACH	4	1		3
Z0034105	MATERIAL TRANSFER DEVICE	TON	20692	1294	19398	
Z0049790	RELOCATING NAME PLATES	EACH	2			2
Z0055605	RUBBLIZING PORTLAND CEMENT CONCRETE PAVEMENT	SQ YD	22861	2431	20430	
⊙ Z0076600	TRAINEES	HOUR	500		500	

© 0042

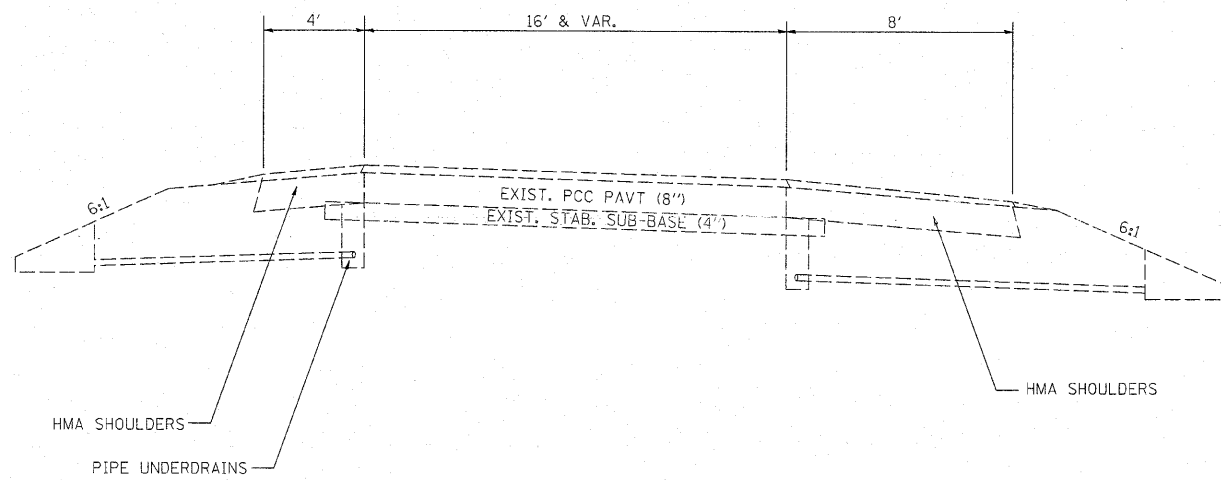
FILE NAME =	USER NAME = teasleyck	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cr:\pw\work\p\dot\teasleyck\102148697\0774417-sh1-soq.dgn	DRAWN -	REVISED -	57			(13-1)R&(25-6-1)R		44	4	
PLOT SCALE = 1/8"=1'-0" / IN.	CHECKED -	REVISED -	CONTRACT NO. 74417							
PLOT DATE = 10/22/2010	DATE -	REVISED -	ILLINOIS FED. AID PROJECT							



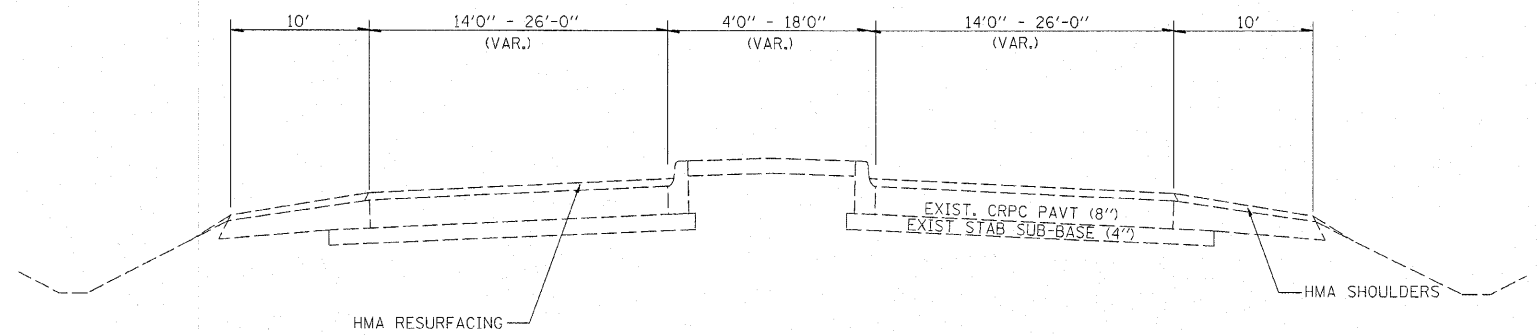
**EXISTING TYPICAL SECTION**  
NORTHBOUND ONLY



**EXISTING TYPICAL SECTION**  
C.H. 29

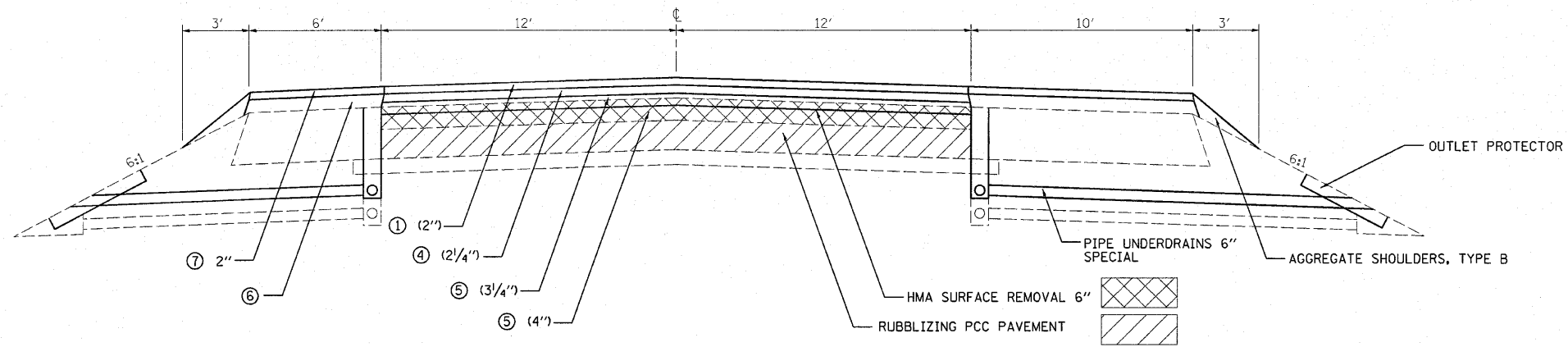


**EXISTING TYPICAL SECTION**  
RAMPS C & D

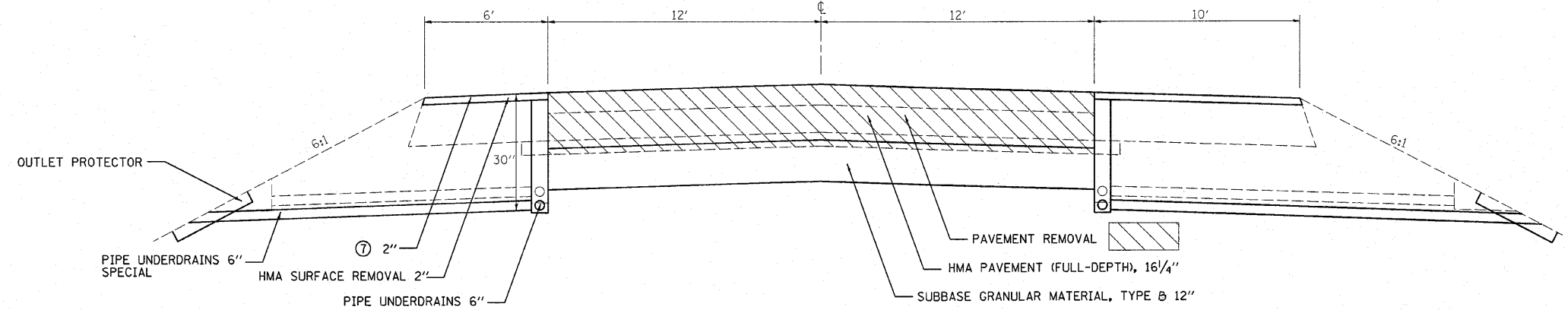


**EXISTING TYPICAL SECTION**  
C.H. 29

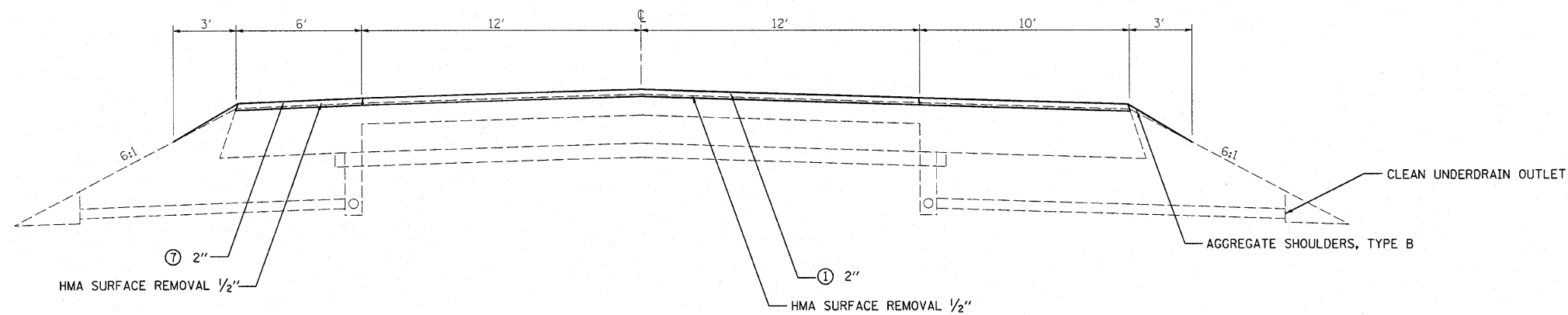
FILE NAME =	USER NAME = teasleyck	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EXISTING TYPICAL SECTIONS</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
et:\pwwork\pwwork\teasleyck\d0148697\07	4417-sht-typicals.dgn	DRAWN -	REVISED -			57	(13-1R&(25-6-1)R	•	44	5	
PLOT SCALE = 50,0000' / IN.		CHECKED -	REVISED -			CONTRACT NO. 74417		ILLINOIS FED. AID PROJECT			
PLOT DATE = 10/21/2010		DATE -	REVISED -			SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	• Clay, Effingham		



STA 3311+20 TO STA 4201+00  
 STA 4209+00 TO STA 4265+36  
 STA 4310+90 TO STA 4332+50



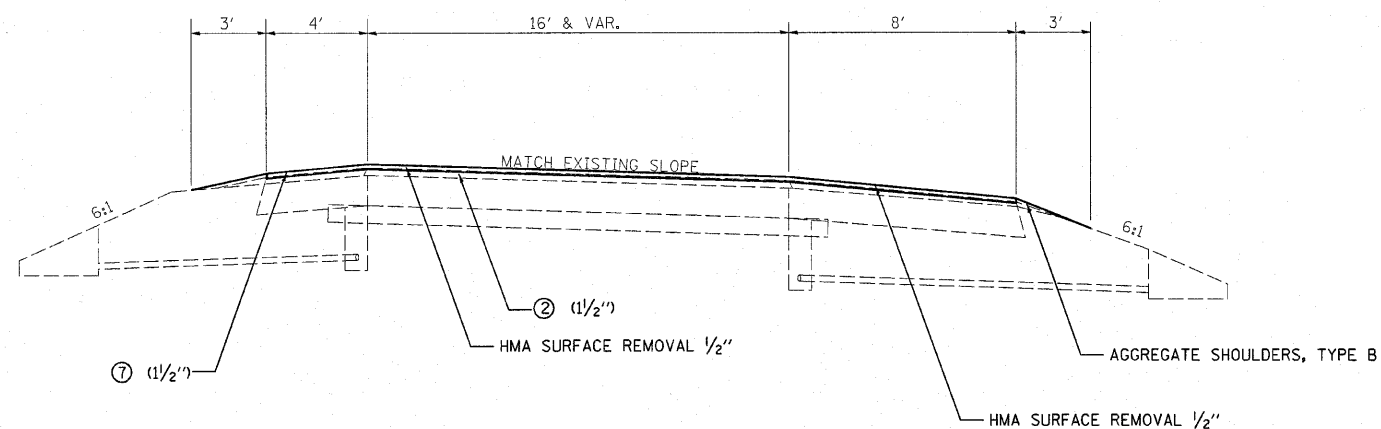
STA 4201+00 TO STA 4209+00  
 STA 4271+10 TO STA 4305+72  
 STA 4428+36 TO STA 4433+36  
 STA 4478+62 TO STA 4483+62



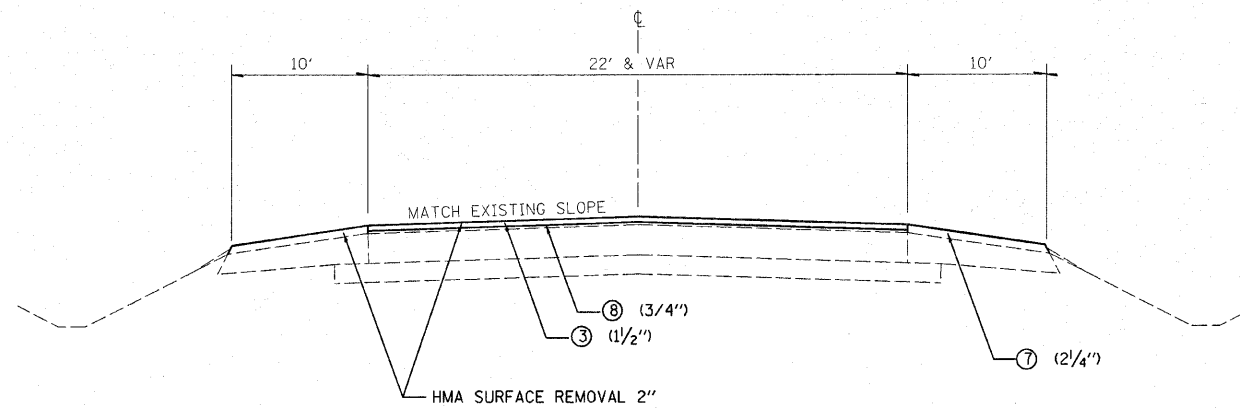
STA 4335+80 TO STA 4428+36  
 STA 4433+36 TO STA 4478+62  
 STA 4483+62 TO STA 4495+20

- ① POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80
- ② POLYMERIZED HMA SURFACE COURSE, MIX "D" N105
- ③ HMA SURFACE COURSE, MIX "C" N70
- ④ POLYMERIZED HMA BINDER COURSE, IL-19.0, N105
- ⑤ HMA BINDER COURSE, IL-19.0, N90
- ⑥ HMA BINDER COURSE, IL-19.0 N70
- ⑦ HMA SHOULDERS
- ⑧ LEVELING BINDER MACHINE METHOD, N70

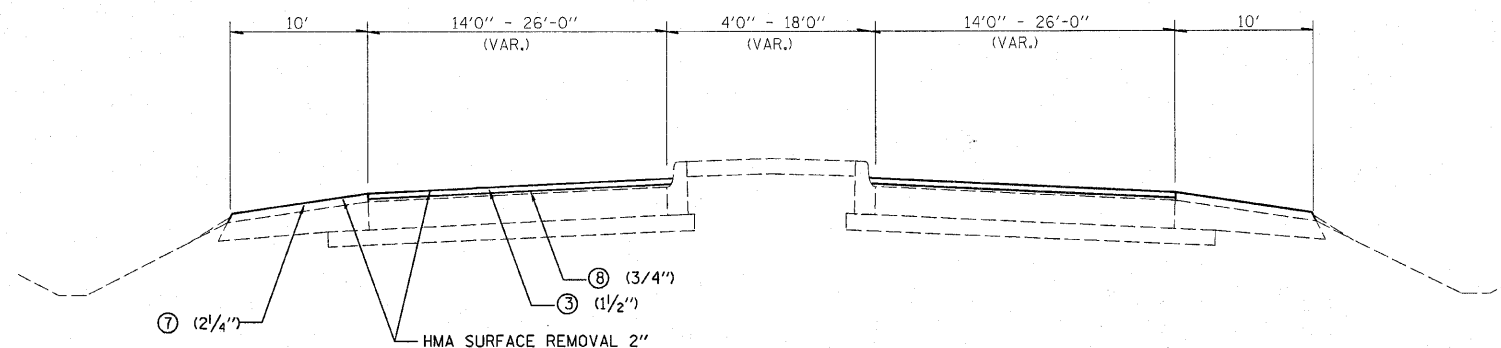
FILE NAME =	USER NAME = teasleyck	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PROPOSED TYPICAL SECTIONS MAINLINE</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwwork\pwwork\teasleyck\d0148597\07	4417-sht-typical.dgn	DRAWN -	REVISED -		57	(13-1)R&(25-6-1)R		44	6			
PLOT SCALE = 50,0000' / IN.		CHECKED -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.			CONTRACT NO. 74417				
PLOT DATE = 10/21/2010		DATE -	REVISED -					ILLINOIS FED. AID PROJECT				



PROPOSED TYPICAL SECTION  
RAMPS C & D



PROPOSED TYPICAL SECTION  
C.H. 29



PROPOSED TYPICAL SECTION  
C.H. 29

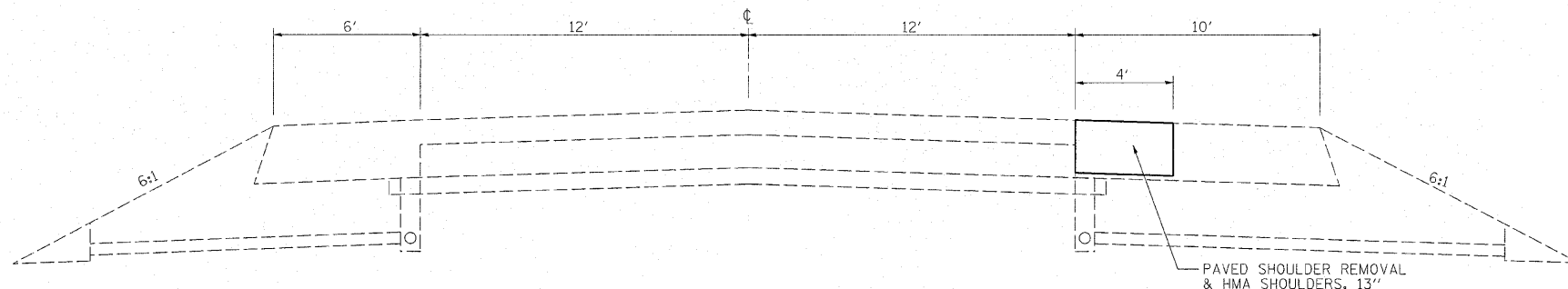
- ① POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80
- ② POLYMERIZED HMA SURFACE COURSE, MIX "D" N105
- ③ HMA SURFACE COURSE, MIX "C" N70
- ④ POLYMERIZED HMA BINDER COURSE, IL-19.0, N105
- ⑤ HMA BINDER COURSE, IL-19.0, N90
- ⑥ HMA BINDER COURSE, IL-19.0L N30
- ⑦ HMA SHOULDERS
- ⑧ LEVELING BINDER MACHINE METHOD, N70

FILE NAME =	USER NAME = teasleyck	DESIGNED -	REVISED -
c:\pwwork\pwwork\teasleyck\d0148697\07	4417-shr-typicals.dgn	DRAWN -	REVISED -
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	PLOT DATE = 10/21/2010	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

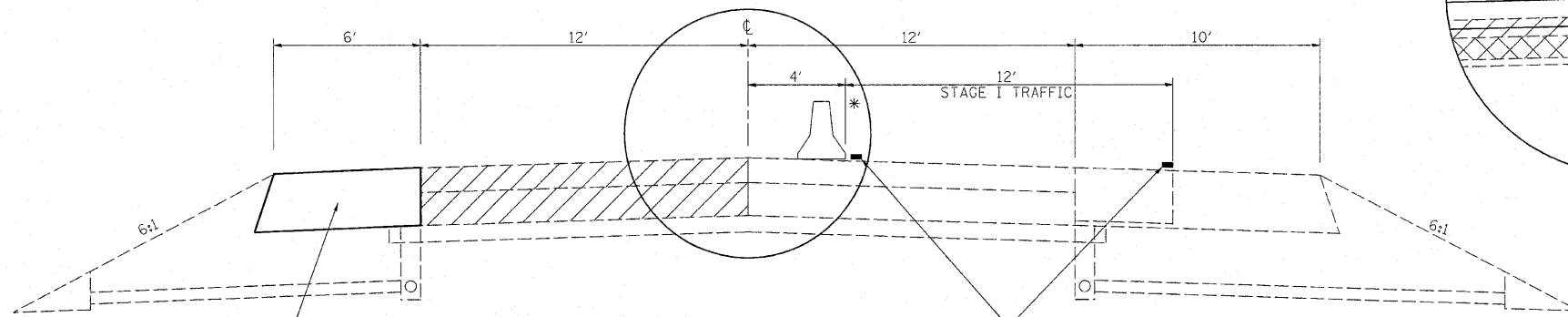
PROPOSED TYPICAL SECTIONS  
RAMPS & C.H. 29

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(13-1)R&(25-6-1)R	•	44	7
SCALE:			CONTRACT NO. 74417	
SHEET NO.	OF SHEETS	STA.	ILLINOIS FED. AID PROJECT	
		TO STA.		



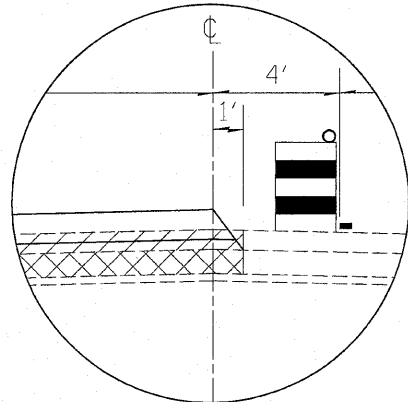
**PRE-STAGE 1**  
 STA 3292+90 TO STA 4337+50  
 STA 4426+11 TO STA 4434+36  
 STA 4476+37 TO STA 4484+62

NOTE: WHERE PCC WIDENING EXISTS NEAR BRIDGES, SWITCH TO 2" REMOVAL & REPLACEMENT

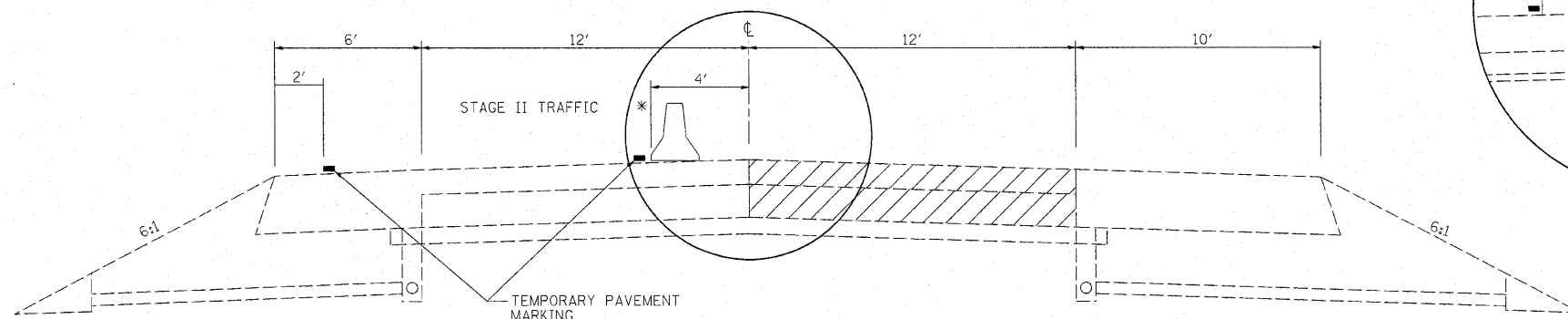
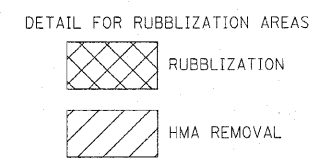


**STAGE 1**  
 STA 3311+20 TO STA 4332+50  
 STA 4426+11 TO STA 4434+36  
 STA 4476+37 TO STA 4484+62

NOTE: WHERE PCC WIDENING EXISTS NEAR BRIDGES, SWITCH TO 2" REMOVAL & REPLACEMENT

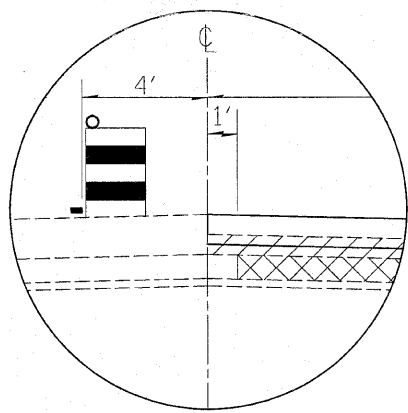
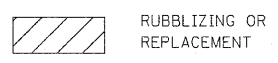


**STAGE 1**



**STAGE 2**  
 STA 3311+20 TO STA 4332+50  
 STA 4426+11 TO STA 4434+36  
 STA 4476+37 TO STA 4484+62

\* BARRIER WILL BE USED IN PAVEMENT REPLACEMENT AREAS USING STD 701402.



**STAGE 2**

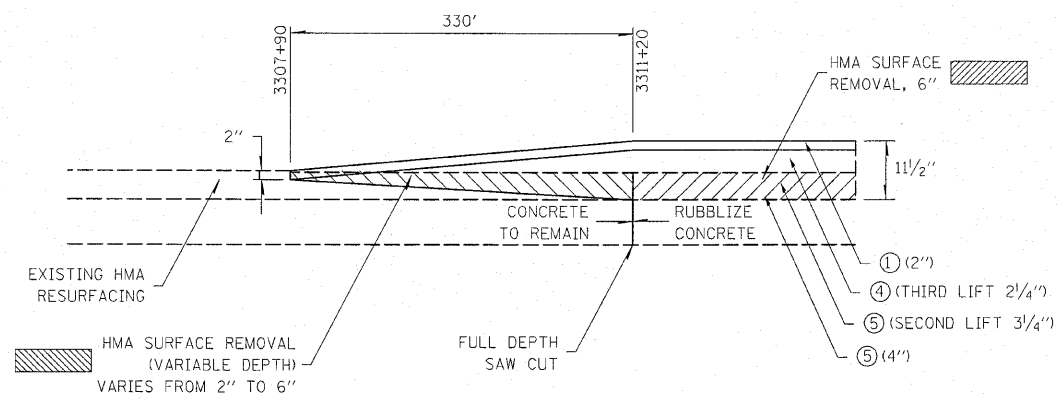
PRE-STAGE 1 QUANTITIES

STATION TO STATION	LENGTH FT	PAVEMENT MARKING REMOVAL SQ FT	PAVED SHOULDER REMOVAL SQ YD	HMA SHOULDERS, 13" SQ YD
3292+90 TO 3307+90	1500	500	667	667
3307+90 TO 3319+29	1139	380	506	506
4199+97 TO 4268+66	6869	2290	3053	3053
4268+66 TO 4270+90	224	75		
4270+90 TO 4287+85	1695	565	753	753
4287+85 TO 4294+64	679	297		
4294+64 TO 4305+85	1121	374	498	498
4305+85 TO 4307+50	165	55		
4307+50 TO 4315+45	795	265	353	353
4326+45 TO 4332+50	605	202	269	269
4332+50 TO 4337+50	500	167	222	222
4426+11 TO 4434+36	825	275	367	367
4476+37 TO 4484+62	825	275	367	367
<b>TOTALS</b>		<b>5718</b>	<b>7054</b>	<b>7054</b>

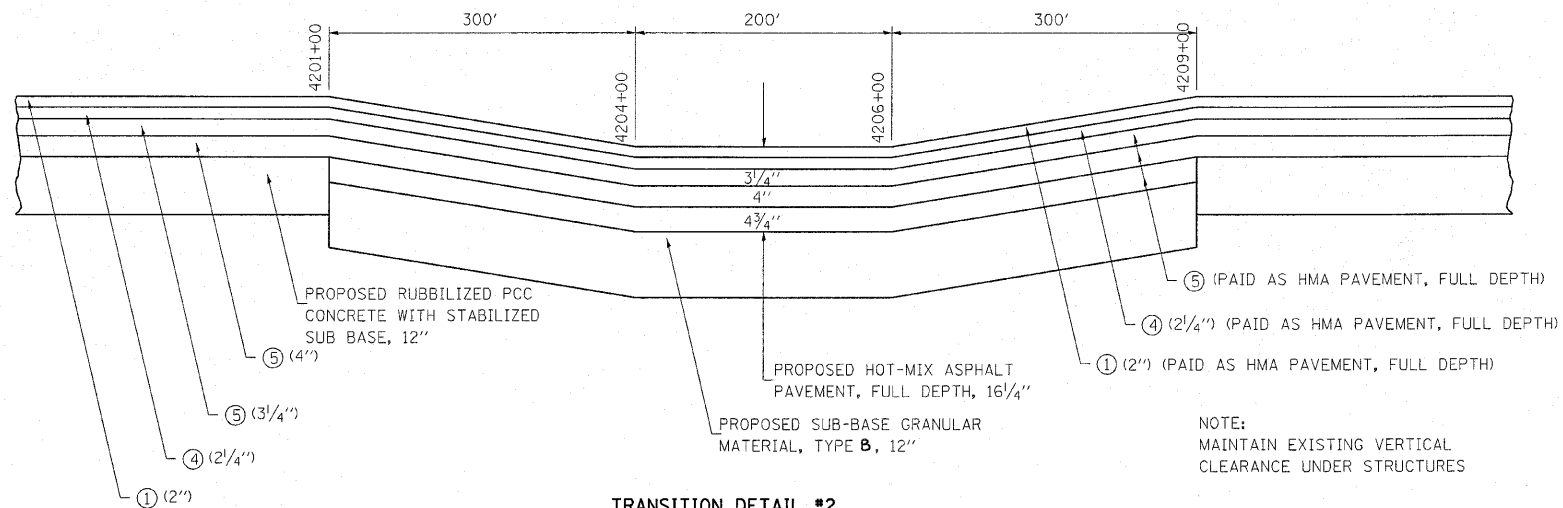
SHOULDER REPLACEMENT QUANTITIES - STAGE 1

STATION TO STATION	LENGTH FT	PAVED SHOULDER REMOVAL SQ YD	HMA SHOULDERS, 13" SQ YD
3292+90 TO 3307+90	1500	1000	1000
3307+90 TO 3319+29	1139	759	759
4199+97 TO 4268+66	6869	4579	4579
4268+66 TO 4270+90	224		
4270+90 TO 4287+85	1695	1130	1130
4287+85 TO 4294+64	679		
4294+64 TO 4305+85	1121	747	747
4305+85 TO 4307+50	165		
4307+50 TO 4315+45	795	530	530
4326+45 TO 4332+50	605	403	403
4332+50 TO 4337+50	500	333	333
4426+11 TO 4434+36	825	550	550
4476+37 TO 4484+62	825	550	550
<b>TOTALS</b>		<b>10581</b>	<b>10581</b>



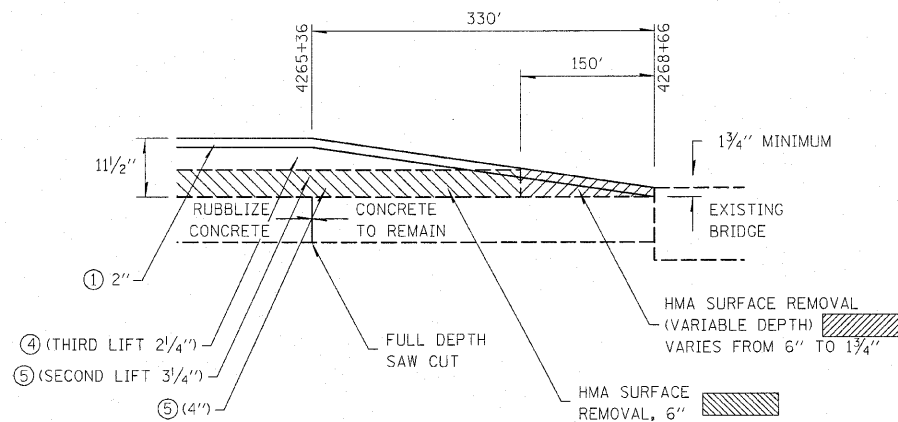


**BUTT JOINT DETAIL #1**

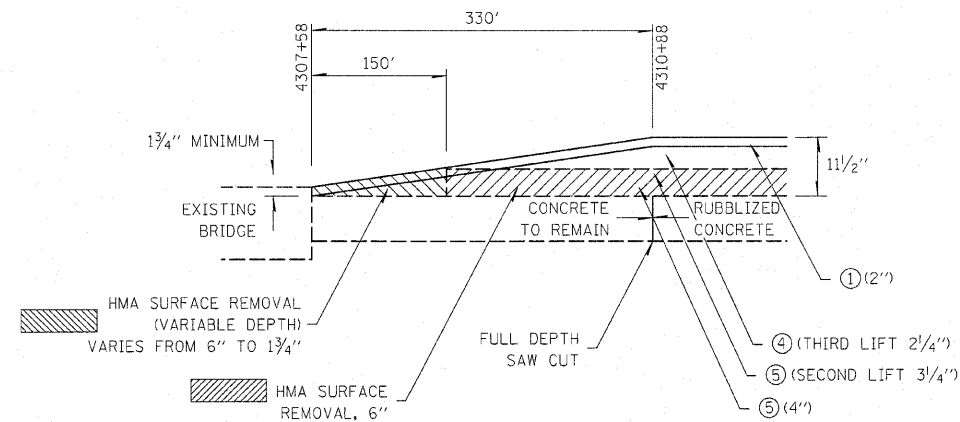


**TRANSITION DETAIL #2**

NOTE:  
MAINTAIN EXISTING VERTICAL  
CLEARANCE UNDER STRUCTURES

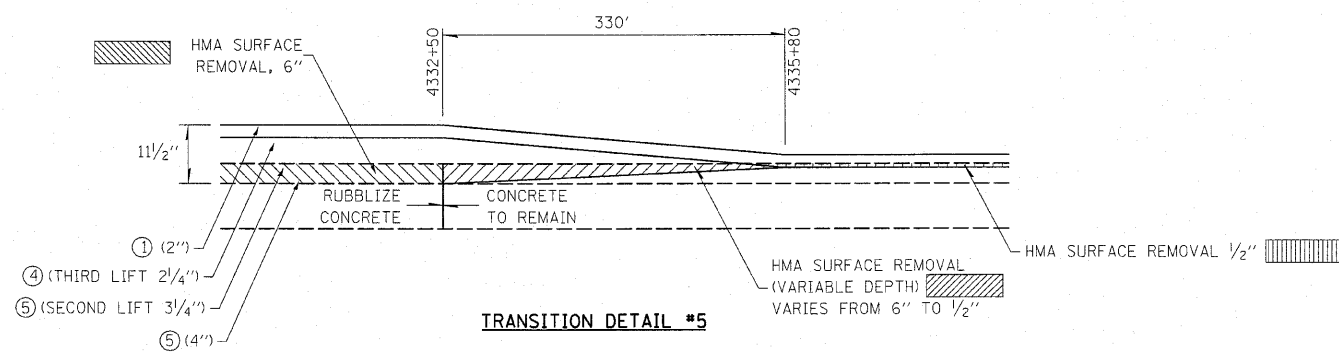


**BUTT JOINT DETAIL #3**

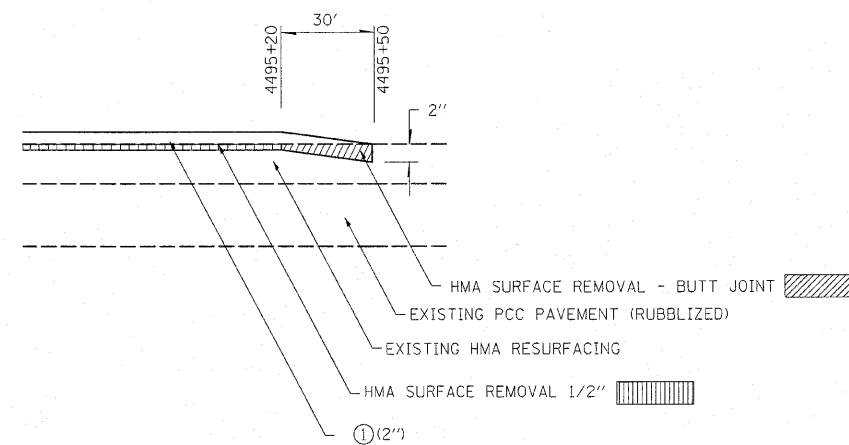


**BUTT JOINT DETAIL #4**

- ① POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80
- ④ POLYMERIZED HMA BINDER COURSE, IL-19.0, N105
- ⑤ HMA BINDER COURSE, IL-19.0, N90



**TRANSITION DETAIL #5**



**BUTT JOINT DETAIL #6**

FILE NAME =  
c:\pwwork\pwwork\teasleyck\d2148697\07

USER NAME = teasleyck  
4417-shr-detail.dgn  
PLOT SCALE = 50.0000' / IN.  
PLOT DATE = 10/21/2010

DESIGNED -  
DRAWN -  
CHECKED -  
DATE -

REVISED -  
REVISED -  
REVISED -  
REVISED -

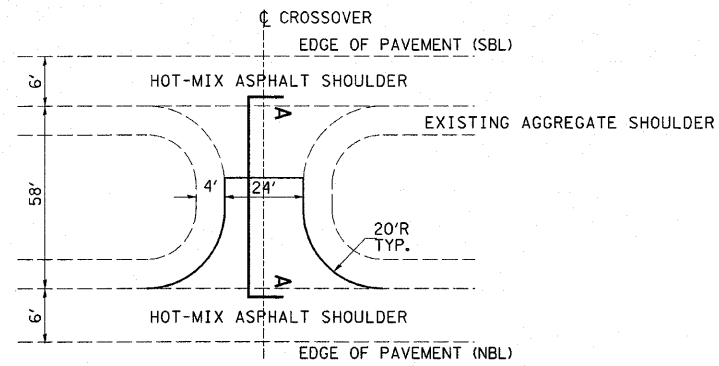
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT & TRANSITION  
DETAILS**

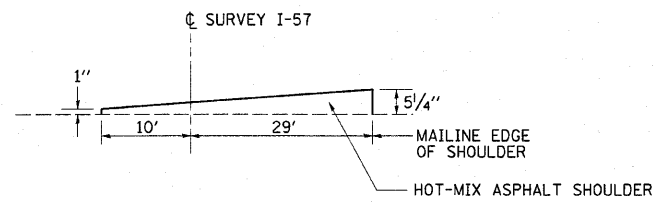
SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(13-1)R&(25-6-1)R		44	9
ILLINOIS FED. AID PROJECT			CONTRACT NO. 74417	

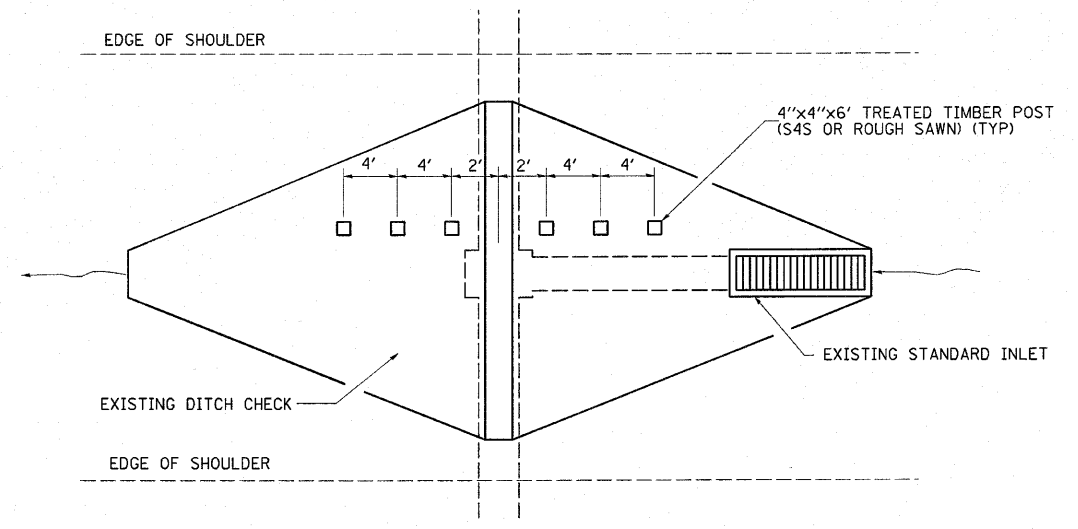
Clay, Effingham



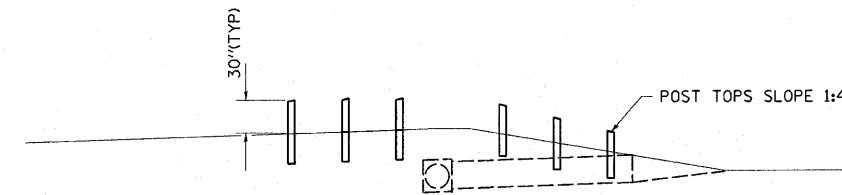
DETAIL OF MEDIAN CROSSOVER



SECTION A-A



PLAN VIEW



PROFILE VIEW

DETAIL OF GUARD POSTS

GUARD POSTS	
MEDIAN STATION	
3310+90	6 EACH
4225+00	6 EACH
4233+25	6 EACH
4249+35	6 EACH
4350+75	6 EACH
4361+40	6 EACH
4399+00	6 EACH
4425+85	6 EACH
4430+14	6 EACH
4451+67	6 EACH
4463+19	6 EACH
4479+40	6 EACH
<b>TOTAL</b>	<b>72 EACH</b>

FILE NAME =	USER NAME = teasleyck	DESIGNED -	REVISED - MKS 03-07	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>MEDIAN CROSS OVER AND WOOD POST BARRIER DETAIL</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cd:\pwork\pwidot\teasleyck\02148697\074417-shit-detail.dgn	DRAWN -	REVISED - MKS 04-08	57			(13-1)R&(25-6-1)R		44	10	
PLOT SCALE = 50.0000 "/ IN.	CHECKED -	REVISED -	CONTRACT NO. 74417							
PLOT DATE = 10/21/2010	DATE -	REVISED -	ILLINOIS FED. AID PROJECT							
SCALE: SHEET NO. OF SHEETS STA. TO STA.						Clay, Effingham				

RESURFACING SCHEDULE

STATION TO STATION	LENGTH FT	PAVEMENT WIDTH FT	AREA SQ YD	BITUMINOUS MATERIAL (PRIME COAT) GALLON	HMA SURFACE REMOVAL 1/2" * SQ YD	HMA SURFACE REMOVAL 2" ** SQ YD	HMA SURFACE REMOVAL 6" SQ YD	HMA SURFACE REMOVAL (VARIABLE DEPTH) * SQ YD	HMA SURFACE REMOVAL BUTT JOINT * SQ YD	RUBBLIZING PCC PAVEMENT SQ YD	PAVEMENT REMOVAL SQ YD	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 TON	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N105 TON	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N70 TON	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N105 TON	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 TON	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 TON	HOT-MIX ASPHALT SHOULDERS TON	LEVELING BINDER (MACHINE METHOD), N70 TON	HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 16 1/4" SQ YD	SUB-BASE GRANULAR MATERIAL, TYPE B 12" SQ YD	TEMPORARY RAMP SQ YD	AGGREGATE SHOULDERS, TYPE B TON	SHOULDER RUMBLE STRIP FOOT		
STA 330790 TO STA 331120.0	330	24	880	147				880				114.4			111.3	233	66.8	73.9				31	41	660		
STA 331120 TO STA 331170.0	50	24	133.3	22			133.3			133.3		17.3			16.9	55	16.2	11.2					6	100		
STA 331170 TO STA 331928.74	758.74	24	2023.3	337			2023.3			2023.3		263			255.9	838	245.5	170					94	1518		
STA 331928.74 TO STA 419997.27	EQUATION																									
STA 419997.27 TO STA 420100.0	102.73	24	273.9	46			273.9			273.9		35.6			34.7	114	33.2	23						13	206	
STA 420100 TO STA 420900.0	800	24	2133.3	1423							2133.3						68	179.2		2133.3	2133.3			74	1600	
STA 420900.00 TO STA 421200.0	300	24	800	133			800			800		104			101.2	331	97.1	67.2						37	600	
STA 421200 TO STA 421250.0	50	24	133.3	22			133.3			133.3		17.3			16.9	55	16.2	11.2						6	100	
STA 421250 TO STA 421350.0	100	24	266.7	44			266.7					35			33.7	111	32.4	22.4						12	200	
STA 421350 TO STA 421400.0	50	24	133.3	22			133.3			133.3		17.3			16.9	55	16.2	11.2						6	100	
STA 421400 TO STA 422400.0	1000	24	2666.7	444			2666.7			2666.7		347			337.3	1105	323.6	224						123	2000	
STA 422400 TO STA 422450.0	50	24	133.3	22			133.3			133.3		17.3			16.9	55	16.2	11.2						6	100	
STA 422450 TO STA 422550.0	100	24	266.7	44			266.7					34.7			33.7	111	32.4	22.4						12	200	
STA 422550 TO STA 422600.0	50	24	133.3	22			133.3			133.3		17.3			16.9	55	16.2	11.2						6	100	
STA 422600 TO STA 423272.0	672	24	1792	299			1792			1792		233			226.7	742	217.4	150.5						83	1344	
STA 423272 TO STA 423322.0	50	24	133.3	22			133.3			133.3		17.3			16.9	55	16.2	11.2						6	100	
STA 423322 TO STA 423422.0	100	24	266.7	44			266.7					34.7			33.7	111	32.4	22.4						12	200	
STA 423422 TO STA 423472.0	50	24	133.3	22			133.3			133.3		17.3			16.9	55	16.2	11.2						6	100	
STA 423472 TO STA 424882.0	1410	24	3760	627			3760			3760		489			475.6	1558	456.2	315.8						174	2820	
STA 424882 TO STA 424932.0	50	24	133.3	22			133.3			133.3		17.3			16.9	55	16.2	11.2						6	100	
STA 424932 TO STA 425032.0	100	24	266.7	44			266.7					34.7			33.7	111	32.4	22.4						12	200	
STA 425032 TO STA 425082.0	50	24	133.3	22			133.3			133.3		17.3			16.9	55	16.2	11.2						6	100	
STA 425082 TO STA 426536.0	1454	24	3877.3	646			3877.3			3877.3		504.1			490.5	1606	470.4	325.7						179	2908	
STA 426536 TO STA 426865.8	329.75	24	879.3	147				400				114.3			111.2	232	67.2	73.9					31	41	660	
STA 426865.8 TO STA 427090.1	224.33	BRIDGE																								
STA 427090.1 TO STA 427110.0	19.92	24	53.1	9								6.9			6.7			4					31		40	
STA 427110 TO STA 428785.0	1675	24	4466.7	3007							4466.7							333.5		4466.7	4466.7				3350	
STA 428785 TO STA 430572.0	1787	24	4765.3	3210							4765.3							290.8		4765.3	4765.3				2898	
STA 430572 TO STA 430599.4	27.41	24	73.1	12								9.5			9.2			5.5					31		55	
STA 430599.4 TO STA 430757.9	158.5	BRIDGE																								
STA 430757.9 TO STA 431090.0	332.09	24	885.6	148				400				115.1			112	225	67.9	74.4					31	41	664	
STA 431090 TO STA 433250.0	2160	24	5760	960			5760			5760		749			728.6	2386	698.9	483.8						267	3215	
STA 433250 TO STA 442065.0	8815	24	23506.7	3918	22627			880				3055.9			2973.4			1755.2						418	17630	
STA 442065 TO STA 442836.0	771	24	2056	343	2056							267.3			260.1			153.5						37	1542	
STA 442836 TO STA 443336.0	500	24	1333.3	889							1333.3							99.6		1333.3	1333.3				1000	
STA 443336 TO STA 447862.0	4526	24	12069.3	2012	12069							1569			1526.7			901.2						215	9052	
STA 447862 TO STA 448362.0	500	24	1333.3	889							1333.3							99.6		1333.3	1333.3				1000	
STA 448362 TO STA 449550.0	1188	24	3168	528	3088				80			412			400.7			236.5					31	56	2376	
MEDIAN CROSSOVERS																		40								
RAMP "D"													266					171								54
RAMP "C"										708			334					157								53
CH 29						26710								1343				1352	671							
TOTALS	30691.47			23887	44644	26710	24544	3576	80	22861	14032	8684	600	1343	8449	10847	3087	7951	671	14032	14032	265	2104	58836		

\* SEE SHOULDER MILLING SCHEDULE FOR ADDITIONAL QUANTITY \*\* SEE SHOULDER MILLING & PRE-STAGE 1 SCHEDULES FOR ADDITIONAL QUANTITIES

FILE NAME =	USER NAME = teasleyok	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>RESURFACING SCHEDULE</b>				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cr:\pwork\pwork\teasleyok\20148697\0774417-sht-schedule.dgn		DRAWN -	REVISED -		57	(13-1)R&(25-6-1)R		44	11				
PLOT SCALE = 50.0000 / / IN.		CHECKED -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.				CONTRACT NO. 74417				
PLOT DATE = 10/21/2010		DATE -	REVISED -						ILLINOIS FED. AID PROJECT				

PAYMENT MARKING SCHEDULE

STATION TO STATION	LENGTH FT	SHORT-TERM PAVEMENT MARKING FOOT	TEMPORARY PAINT PAVEMENT MARKING - LINE 4" FOOT	THERMOPLASTIC PAVEMENT MARKING - LTRS & SYMB SQ FT	THERMOPLASTIC PAVEMENT MARKING - LINE 4" FOOT	THERMOPLASTIC PAVEMENT MARKING - LINE 6" FOOT	THERMOPLASTIC PAVEMENT MARKING - LINE 24" FOOT	PERFORMED PLASTIC PAVEMENT MARKING TYPE B - LINE 4" (WHITE) SQ YD	PERFORMED PLASTIC PAVEMENT MARKING TYPE B - LINE 4" (YELLOW) FOOT	PERFORMED PLASTIC PAVEMENT MARKING TYPE B - LINE 6" (WHITE) FOOT	PERFORMED PLASTIC PAVEMENT MARKING TYPE B - LINE 8" (WHITE) FOOT	RAISED REFLECTIVE PAVEMENT MARKER (CRYSTAL) EACH	RAISED REFLECTIVE PAVEMENT MARKER (AMBER) EACH	RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE) EACH	GROOVING FOR RECESSED PAVEMENT MARKING 5" FOOT	GROOVING FOR RECESSED PAVEMENT MARKING 7" FOOT
STA 330982.00 TO STA 331928.74	946.74	174	2130					947	947	237		23.7				
STA 331928.74 TO STA 419997.27	EQUATION															
STA 419997.27 TO STA 426865.75	6868.48	1240	15455					6869	6869	1717		172				
STA 426865.75 TO STA 427090.08	224.33	44	505					224	224	56					448	60
STA 427090.08 TO STA 428788.00	1697.92	310	3820					1698	1698	425		42				
STA 428788.00 TO STA 429464.00	676	126	850						676			17				
STA 429464.00 TO STA 430599.41	1135.41	208	2555					1136	1136			28				
STA 430599.41 TO STA 430757.91	158.5	33	356					159	159	40				5	318	40
STA 430757.91 TO STA 431545.00	787.09	146	1775					787	787	197		20				
STA 431545.00 TO STA 432645.00	1100	202	1375						1100	275		28				
STA 432645.00 TO STA 449520.00	16875	3042	37969					16875	16875	4219		422				
RAMP "C"		198	3815		3745						140					
RAMP "D"		134	2660		2660		40				872		11			
CH 29		1738	11595	93.6	15895	500	40						44			
<b>TOTALS</b>	<b>30469.47</b>	<b>7595</b>	<b>84860</b>	<b>93.6</b>	<b>22300</b>	<b>500</b>	<b>80</b>	<b>28694</b>	<b>30470</b>	<b>7690</b>	<b>1012</b>	<b>816</b>	<b>55</b>	<b>12</b>	<b>766</b>	<b>100</b>

DELINEATOR REMOVAL

MAINLINE	76 EACH
RAMP "C"	18 EACH
RAMP "D"	30 EACH
<b>TOTAL</b>	<b>124 EACH</b>

DELINEATORS

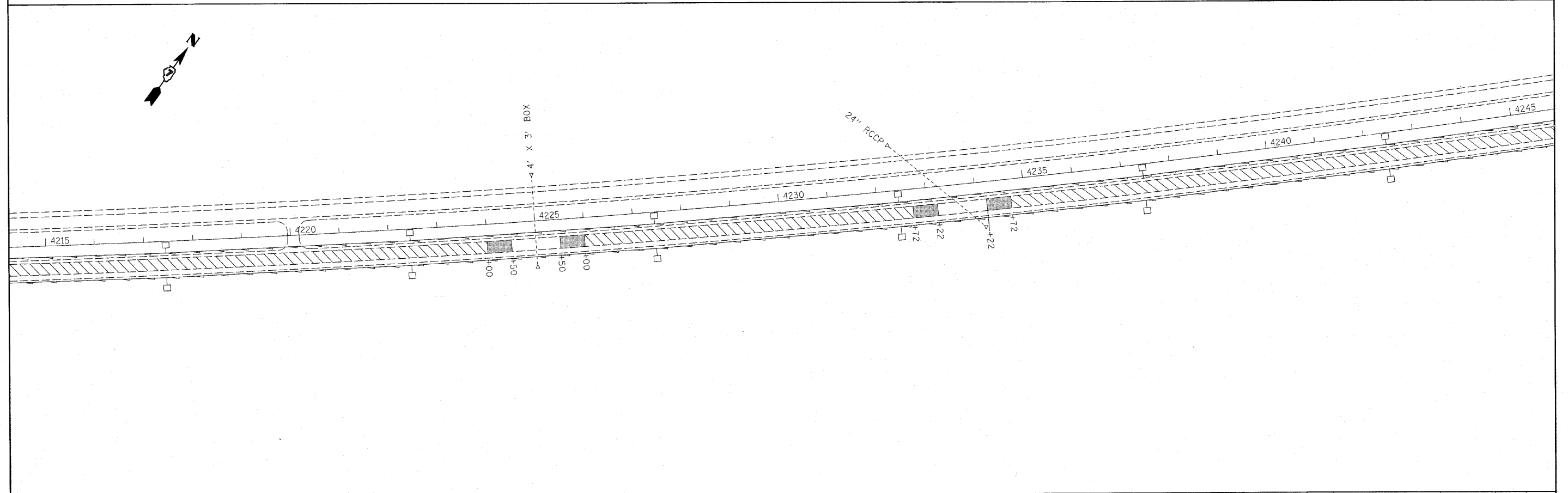
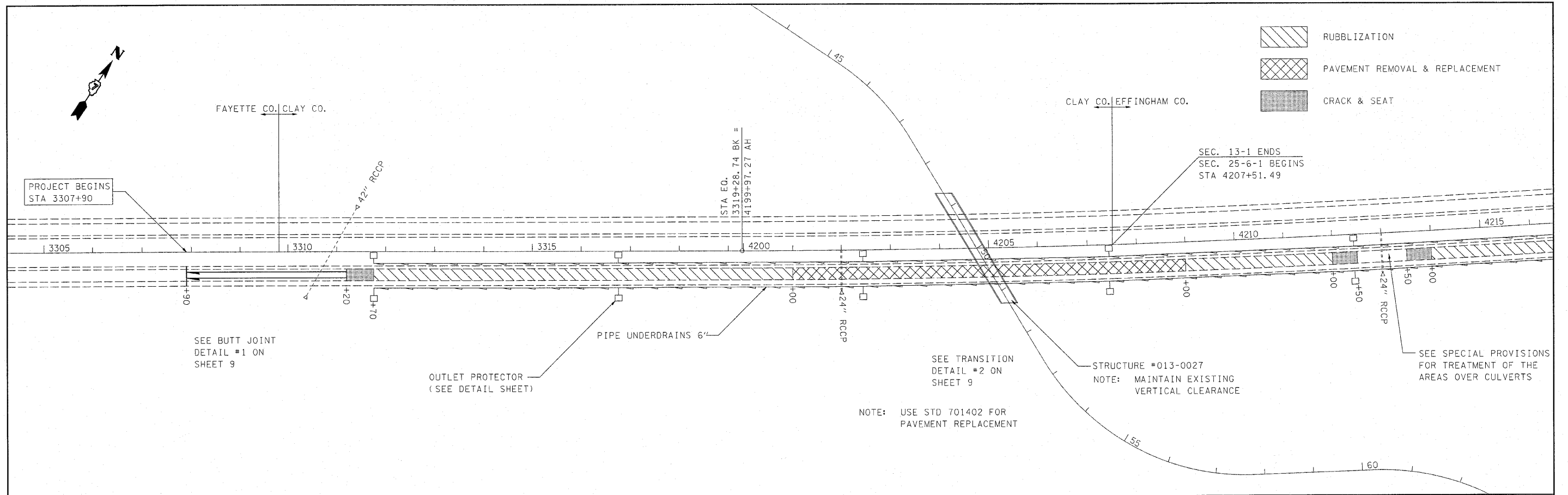
MAINLINE	76 EACH
RAMP "C"	36 EACH
RAMP "D"	52 EACH
<b>TOTAL</b>	<b>164 EACH</b>

GUARDRAIL REMOVAL			
DESCRIPTION	STATION TO STATION	SIDE	FOOT
OVERPASS 013-0027	4201+35 TO 4205+10	RT	375
BRIDGE 025-0067	4264+04 TO 4268+62	LT	458
	4263+29 TO 4268+62	RT	533
	4271+01 TO 4275+26	RT	425
BRIDGE 025-0069	4304+24 TO 4306+09	LT	185
	4301+52 TO 4305+86	RT	434
OVERPASS 025-0043	4428+33 TO 4431+03	RT	270
OVERPASS 025-0073	4478+14 TO 4480+84	RT	270
TOTAL			2950

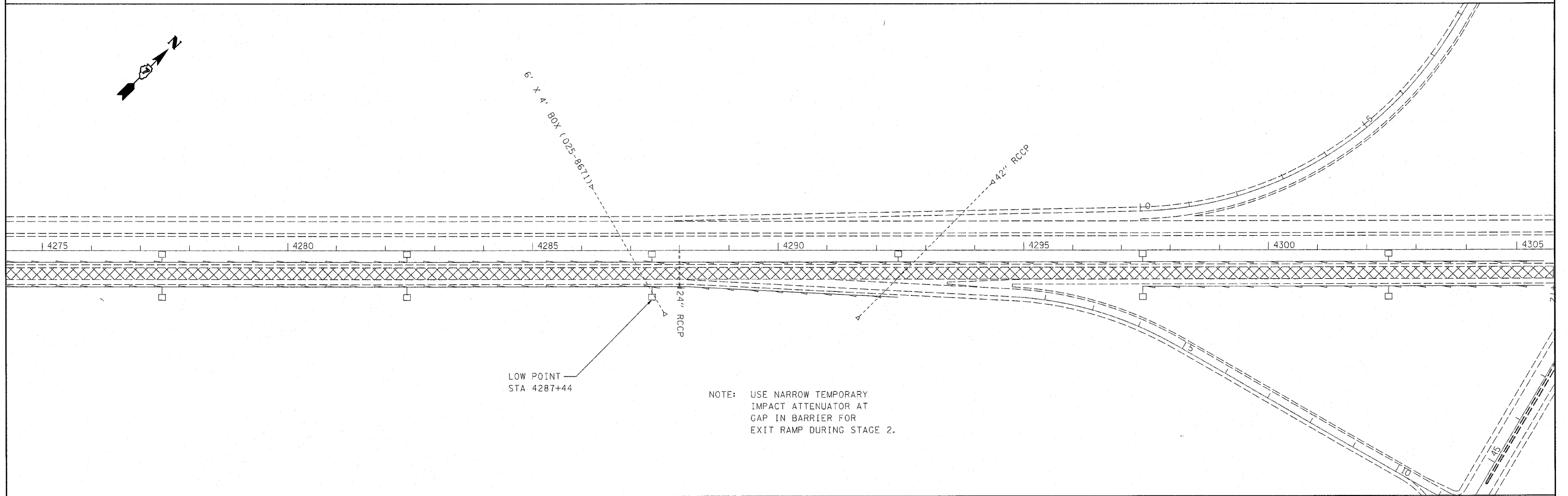
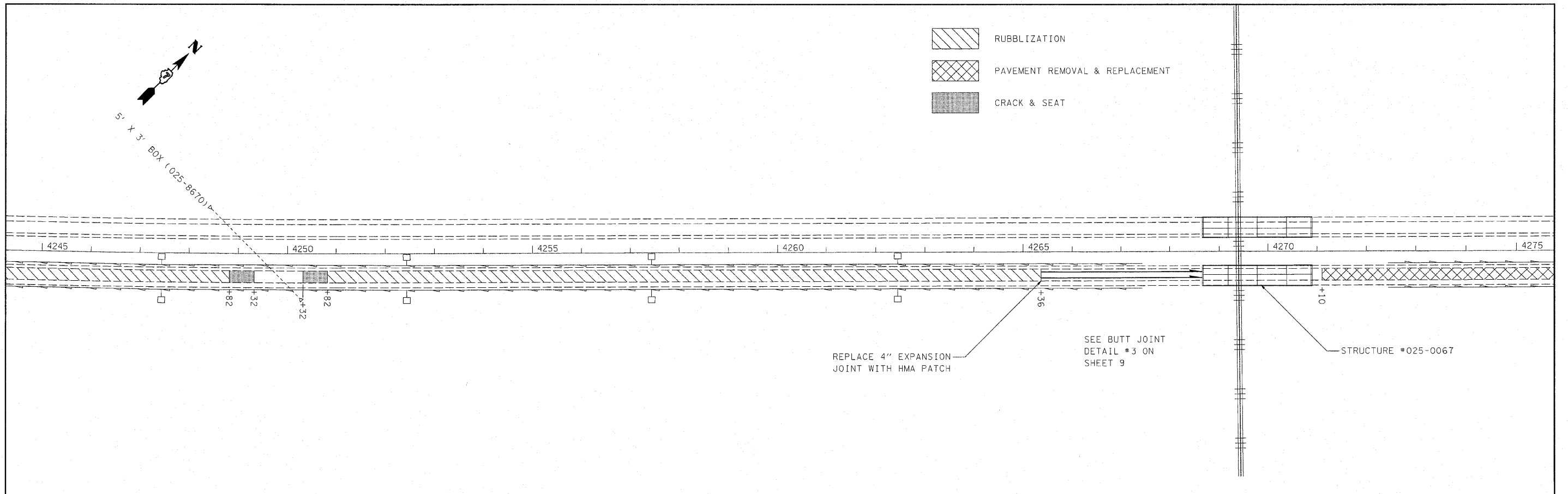
GUARDRAIL SCHEDULE									
DESCRIPTION	STATION TO STATION	SIDE	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	TRAFFIC BARRIER TERMINAL, TYPE 1 SPECIAL (TANGENT)	TRAFFIC BARRIER TERMINAL, TYPE 2	TRAFFIC BARRIER TERMINAL, TYPE 6 SPECIAL	TRAFFIC BARRIER TERMINAL, TYPE 6B	GUARDRAIL MARKER, TYPE A	TERMINAL MARKER, DIRECT APPLIED
			FOOT	EACH	EACH	EACH	EACH	EACH	EACH
OVERPASS 013-0027	4201+55 TO 4202+05	RT		1				4	1
	4202+05 TO 4204+68	RT	262.5					1	
	4204+68 TO 4205+25	RT				1		1	
BRIDGE 025-0067	4264+53 TO 4265+03	LT		1				5	1
	4265+03 TO 4268+16	LT	312.5					1	
	4268+16 TO 4268+62	LT						1	1
	4265+03 TO 4265+53	RT		1					1
	4265+53 TO 4268+16	RT	262.5					4	
	4268+16 TO 4268+62	RT						1	
	4271+01 TO 4271+47	RT						1	
	4271+47 TO 4275+22	RT	375					1	
4275+22 TO 4275+35	RT			1			6		
BRIDGE 025-0069	4302+00 TO 4302+50	LT		1				5	1
	4302+50 TO 4305+63	LT	312.5					1	
	4305+63 TO 4306+09	LT						1	1
	4302+27 TO 4302+77	RT		1					1
	4302+77 TO 4305+40	RT	262.5					4	
4305+40 TO 4305+86	RT						1	1	
OVERPASS 025-0043	4427+48 TO 4427+98	RT		1					1
	4427+98 TO 4430+61	RT	262.5					4	
	4430+61 TO 4431+18	RT					1	1	
OVERPASS 025-0073	4477+29 TO 4477+79	RT		1					1
	4477+79 TO 4480+42	RT	262.5					4	
	4480+42 TO 4480+99	RT					1	1	
TOTALS			2312.5	7	1	5	3	44	7

SHOULDER MILLING					
STATION TO STATION	LENGTH	HMA SURFACE REMOVAL - BUTT JOINT	HMA SURFACE REMOVAL 1/2"	HMA SURFACE REMOVAL 2"	HMA SURFACE REMOVAL, VARIABLE DEPTH
	FT	SQ YD	SQ YD	SQ YD	SQ YD
3307+90 TO 3309+10	120	213			
4202+91 TO 4204+00	109				194
4204+00 TO 4206+00	200			356	
4206+00 TO 4207+09	109				194
4267+46 TO 4268+66	120	213			
4270+90 TO 4305+99	3509			6239	
4307+58 TO 4308+78	120	213			
4335+80 TO 4428+36	9256		16455		
4428+36 TO 4428+66	30				53
4428+66 TO 4433+06	440			782	
4433+06 TO 4433+36	30				53
4433+36 TO 4478+62	4526		8046		
4478+62 TO 4478+92	30				53
4478+92 TO 4483+32	440			782	
4483+32 TO 4483+62	30				53
4483+62 TO 4495+20	1158		2059		
4495+20 TO 4495+50	30	53			
RAMP "D"	1710		2027		
RAMP "C"	1400		1858		
TOTALS		692	30445	8159	600

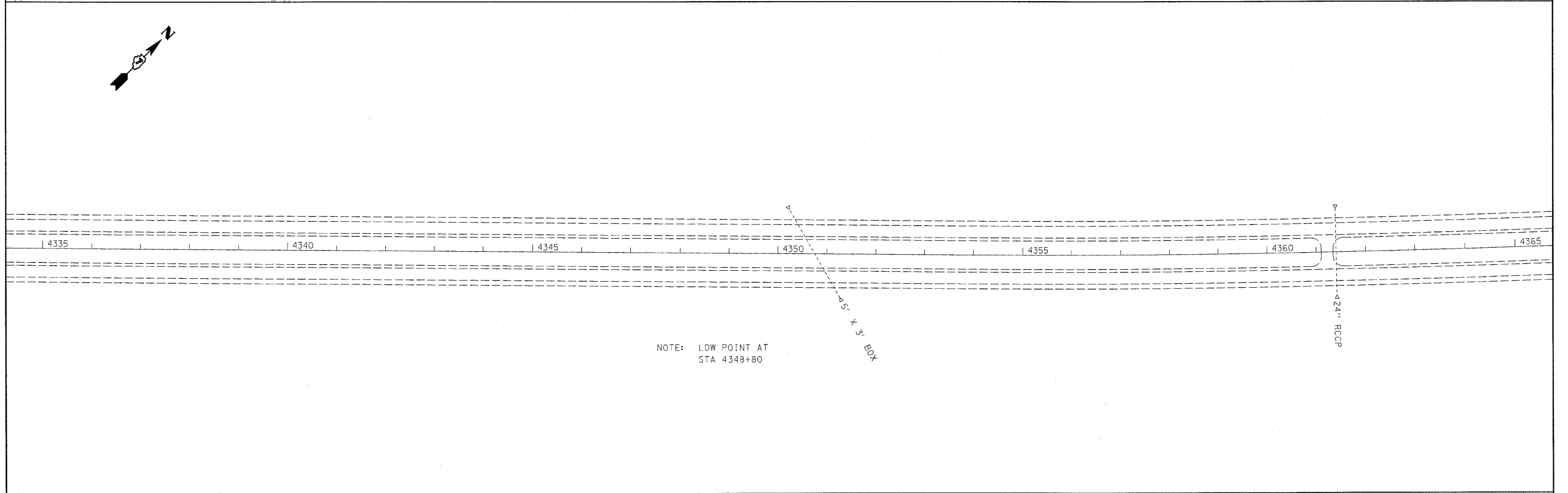
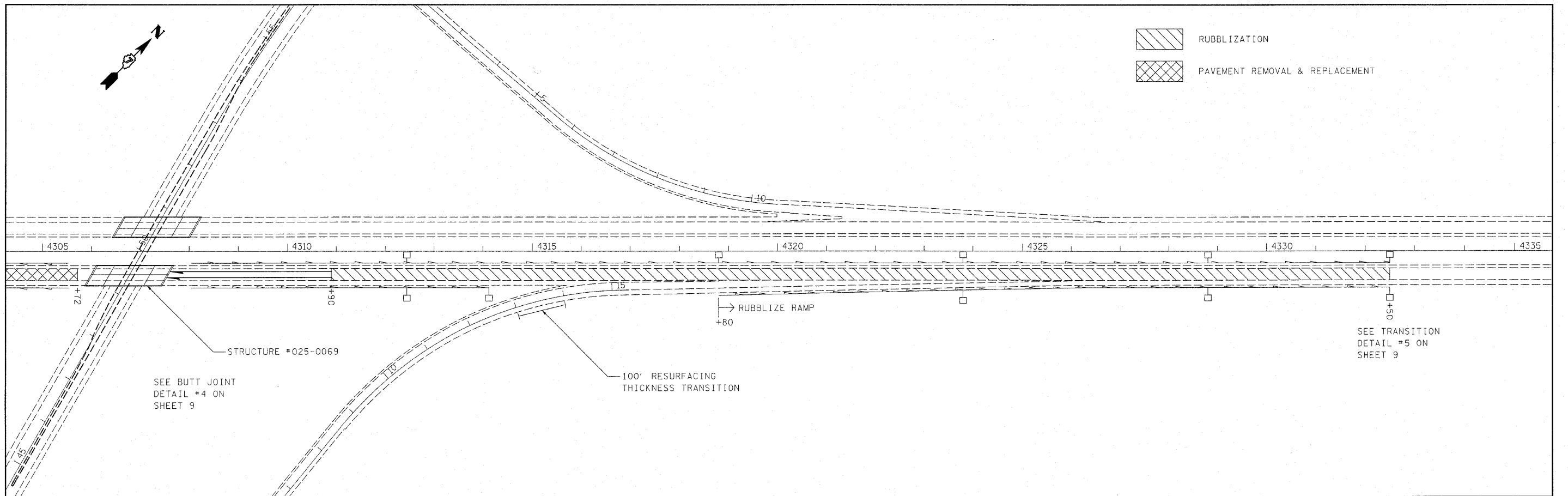
INSERTION CULVERT LINER 16"	
(LOCATED IN MEDIAN)	
STATION	LENGTH
3310+90	8 FEET
4225+00	4 FEET
4233+25	6 FEET
4249+35	6 FEET
4350+75	6 FEET
4399+00	4 FEET
4430+14	4 FEET
4451+67	6 FEET
4463+19	4 FEET
4479+40	6 FEET
TOTAL	54 FEET



FILE NAME =	USER NAME = teasleyck	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PLAN SHEET STA 3305+00 TO STA 4245+00</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwork\pwork\teasleyck\d0148697\0774417-sh1-plan.dgn		DRAWN -	REVISED -		57	(25-6-1)R&(13-1)R	•	44	14			
PLOT SCALE = 100.0000 "/ IN.		CHECKED -	REVISED -		CONTRACT NO. 74417							
PLOT DATE = 10/21/2010		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							
				SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	Effingham, Clay		



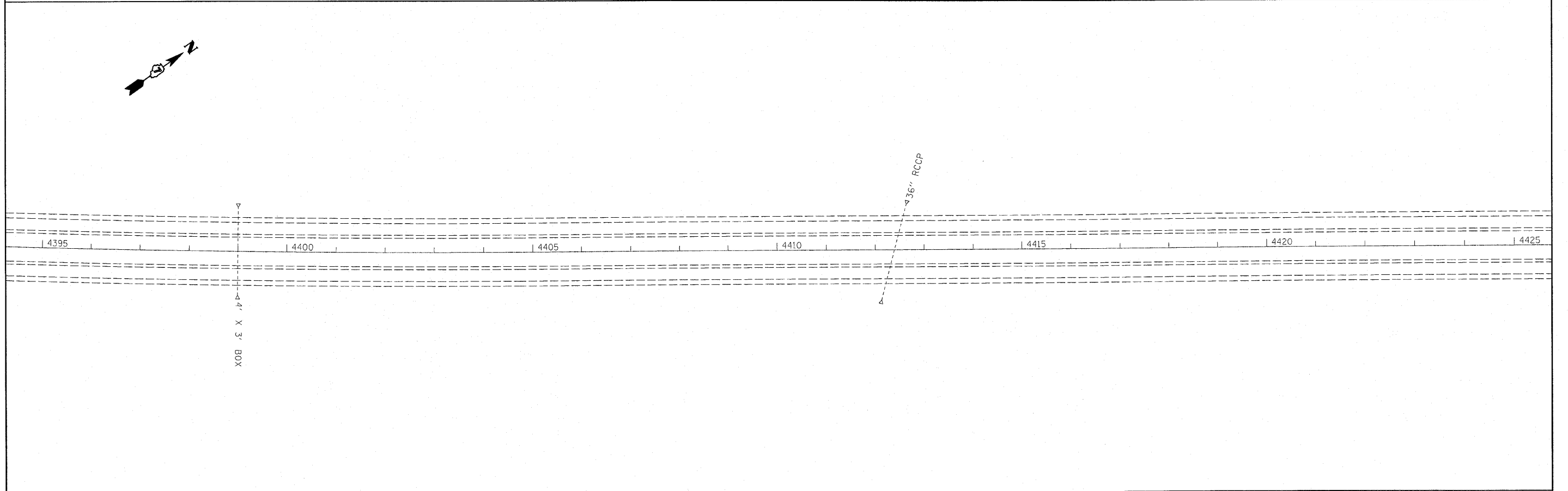
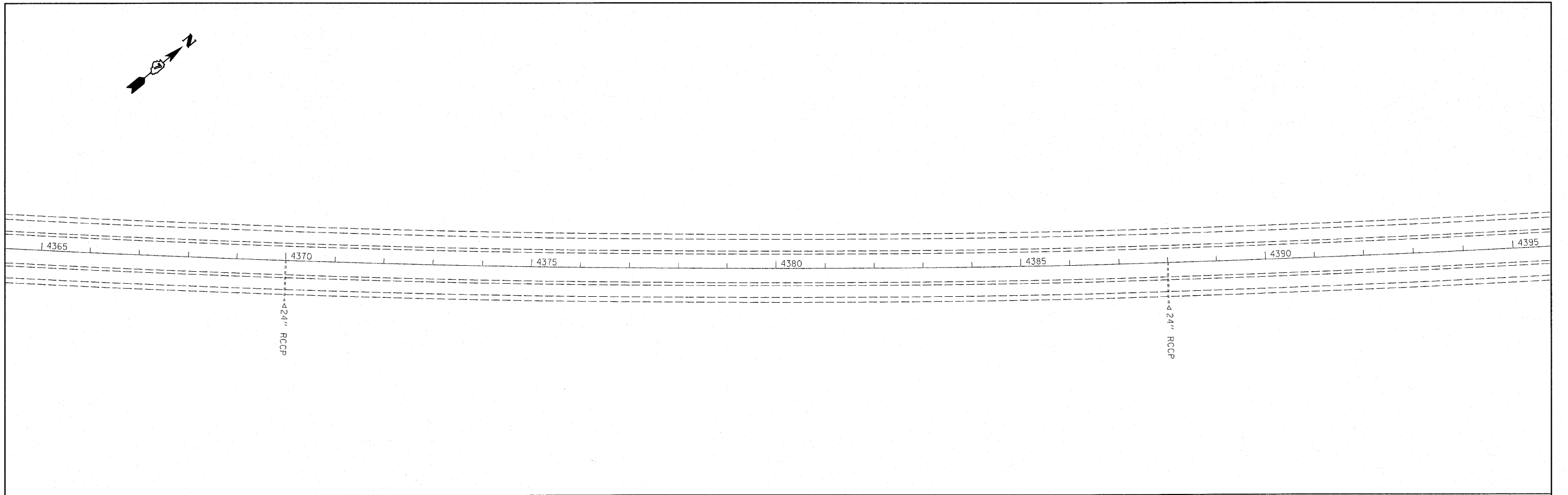
FILE NAME =	USER NAME = teasleyck	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PLAN SHEET STA 4245+00 TO STA 4305+00</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
o:\pwork\pwork\dot\teasleyck\d0148697\074417-sht-plan.dgn		DRAWN -	REVISED -		57	(25-6-1)R&(13-1)R		44	15			
PLOT SCALE = 100.0000 1/ IN.		CHECKED -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.			CONTRACT NO. 74417				
PLOT DATE = 10/21/2010		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							



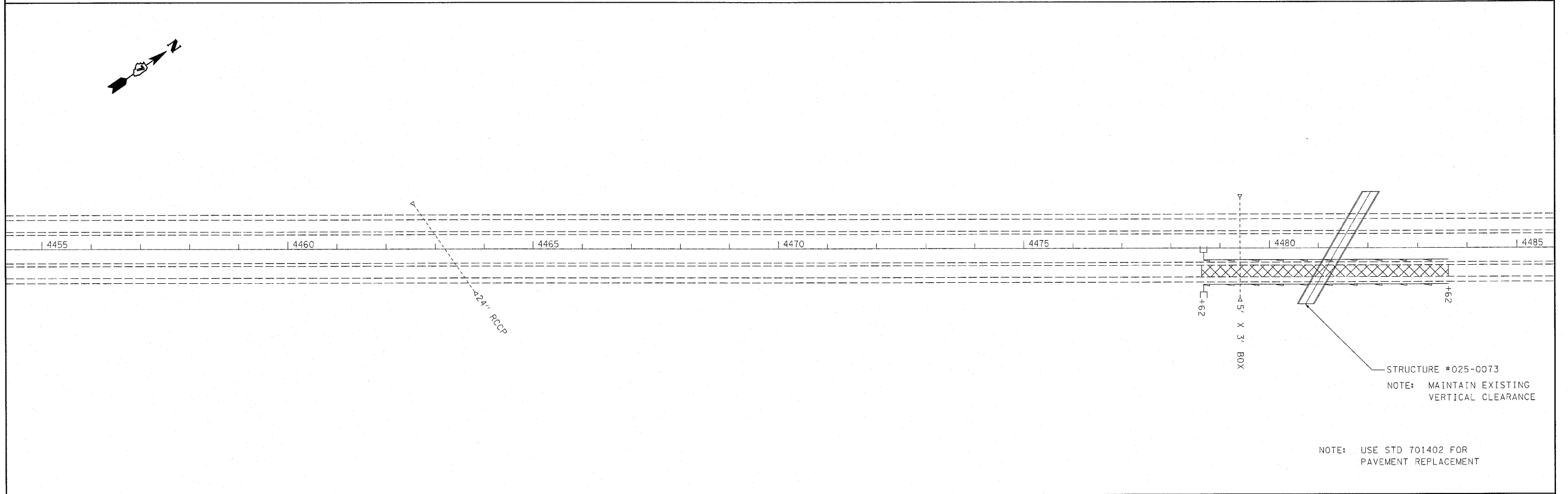
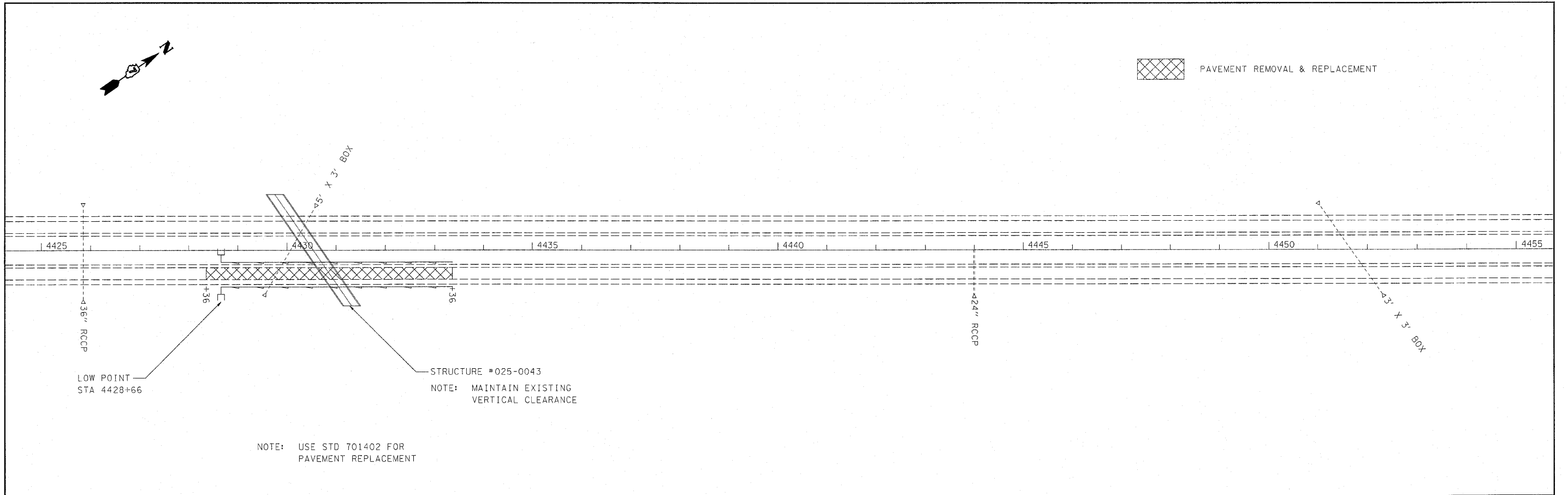
NOTE: LOW POINT AT STA 4348+80

FILE NAME =	USER NAME = teasleyck	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PLAN SHEET STA 4305+00 TO STA 4365+00</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
e:\pwork\pwork\teasleyck\d0148697\07	4417-sh-t-plan.dgn	DRAWN -	REVISED -		57	(25-6-1)R&(13-1)R	•	44	16			
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	PLOT DATE = 10/21/2010	DATE -	REVISED -		ILLINOIS FED. AID PROJECT			Effingham, Clay				

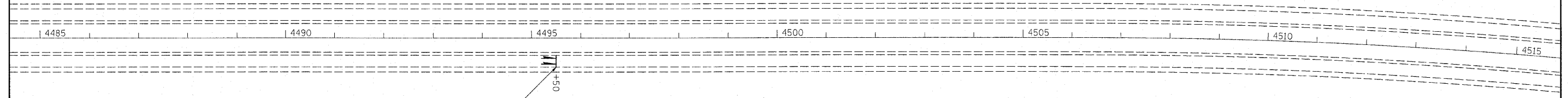
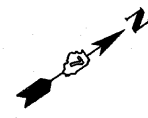




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	4417-shit-plan.dgn	DRAWN -	REVISED -		<b>STA 4365+00 TO STA 4425+00</b>			57	(25-6-1)R&(13-1)R	•	44	17	
	PLOT SCALE = 100.0000 ' / IN.	CHECKED -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		CONTRACT NO. 74417
	PLOT DATE = 10/21/2010	DATE -	REVISED -		• Effingham, Clay								



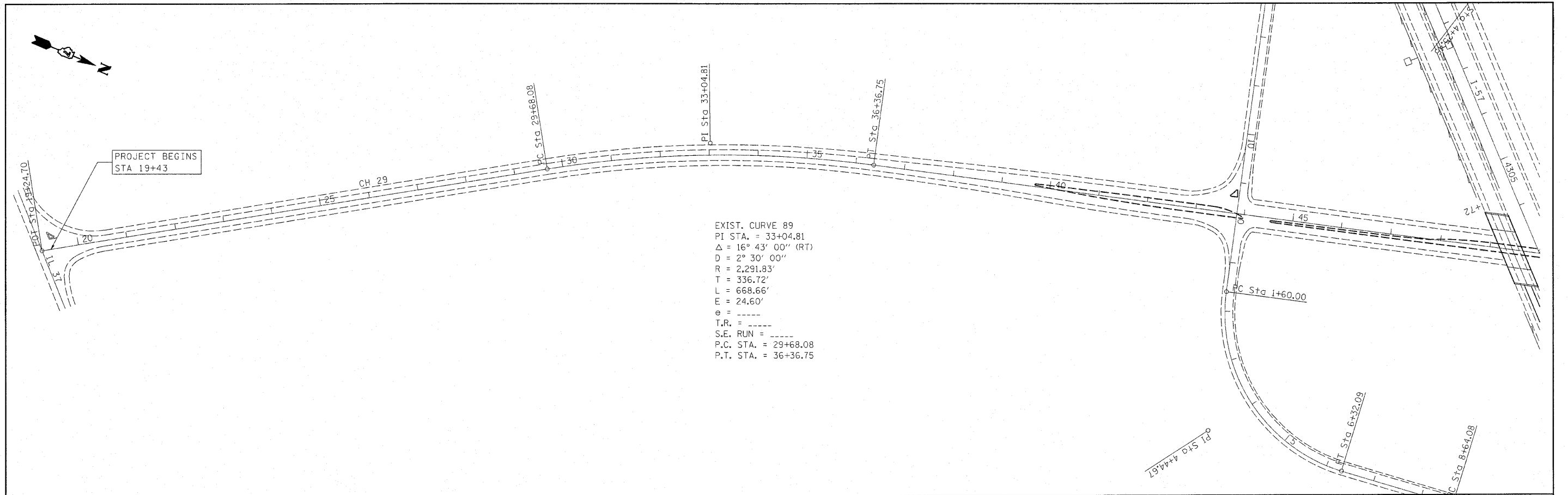
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ct:\p_w\work\p\dot\teasleyck\148697\07\4417-shr-plan.dgn		DRAWN -	REVISED -		57	(25-6-1)R&(13-1)R		44	18				
	PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.				CONTRACT NO. 74417				
	PLOT DATE = 10/21/2010	DATE -	REVISED -		ILLINOIS FED. AID PROJECT				Effingham, Clay				



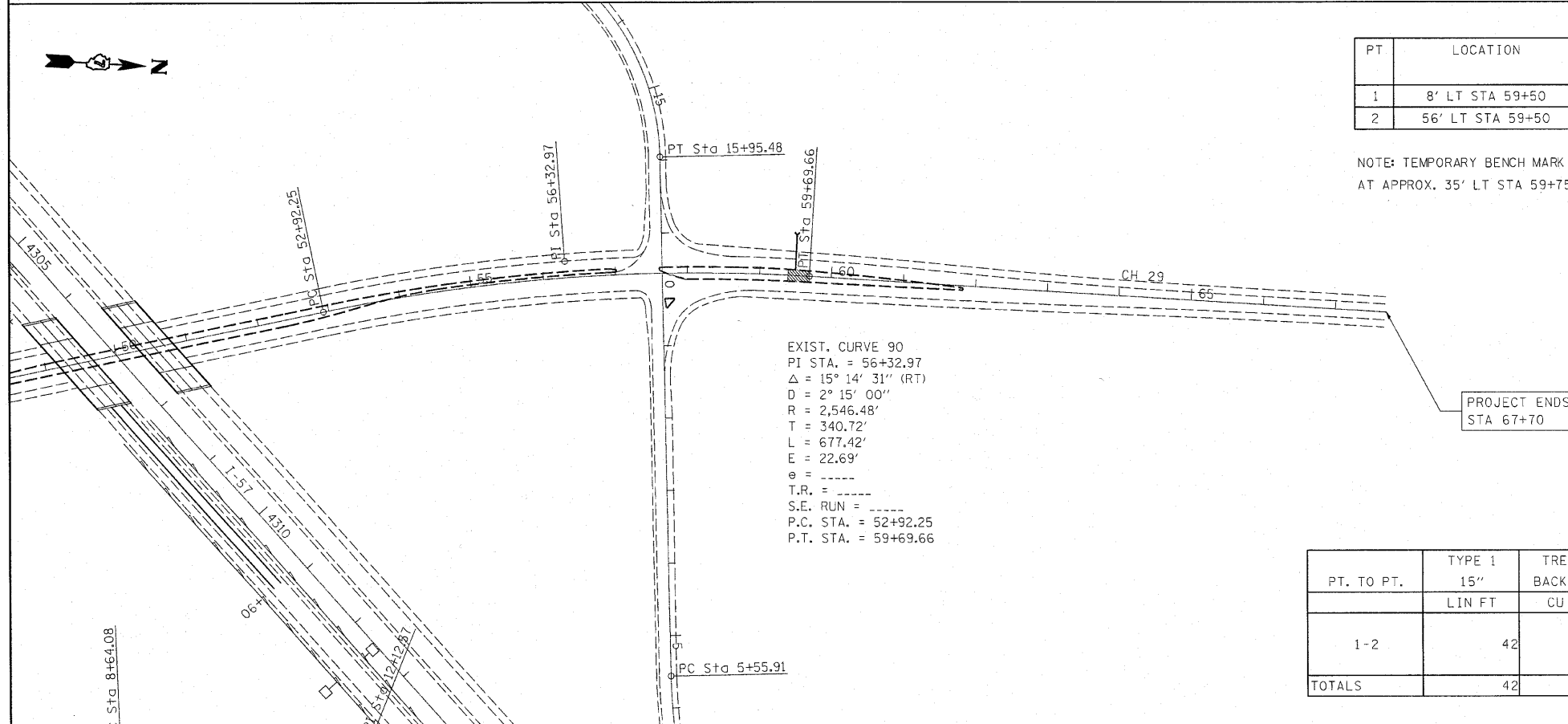
PROJECT ENDS  
STA 4495+50

SEE BUTT JOINT  
DETAIL #6 ON  
SHEET 9

FILE NAME = c:\pwork\pwork\teesleyok\d0148697\074417-shr-plan.dgn	USER NAME = teesleyok	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PLAN SHEET STA 4485+00 TO STA 4515+00</b>				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100.0000' / IN.	DRAWN -	REVISED -		57	(25-6-1)R&(13-1)R	•	44	19				
	PLOT DATE = 10/21/2010	CHECKED -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.				CONTRACT NO. 74417				
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT • Effingham, Clay								



EXIST. CURVE 89  
 PI STA. = 33+04.81  
 $\Delta = 16^\circ 43' 00''$  (RT)  
 $D = 2^\circ 30' 00''$   
 $R = 2,291.83'$   
 $T = 336.72'$   
 $L = 668.66'$   
 $E = 24.60'$   
 $\theta = \text{-----}$   
 T.R. = -----  
 S.E. RUN = -----  
 P.C. STA. = 29+68.08  
 P.T. STA. = 36+36.75



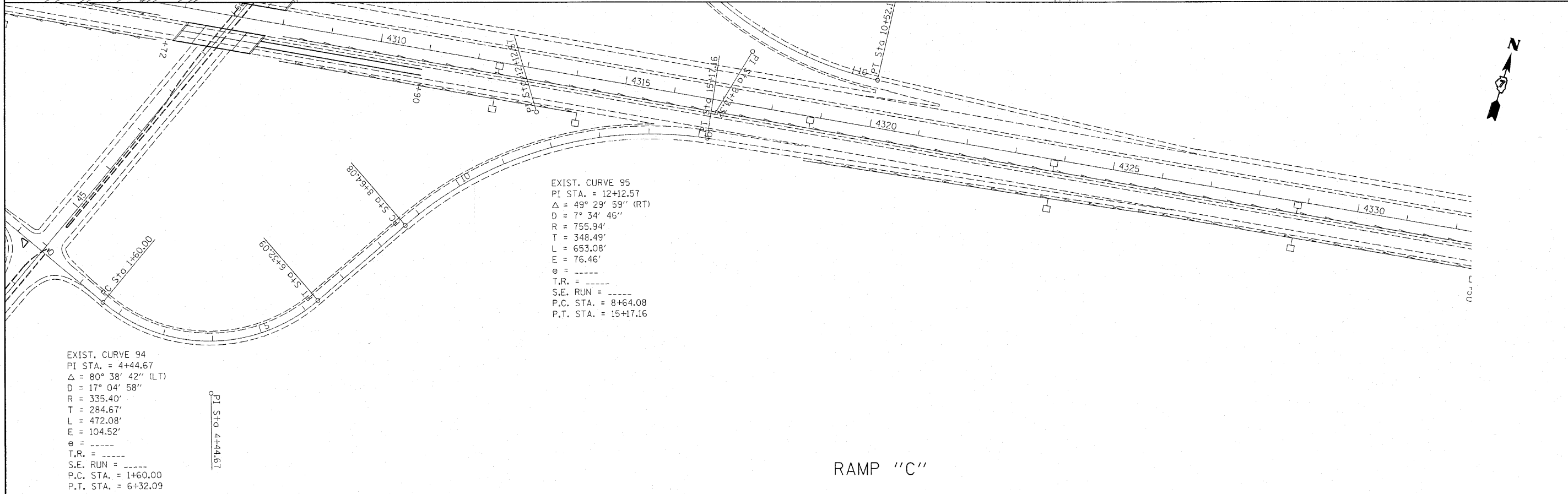
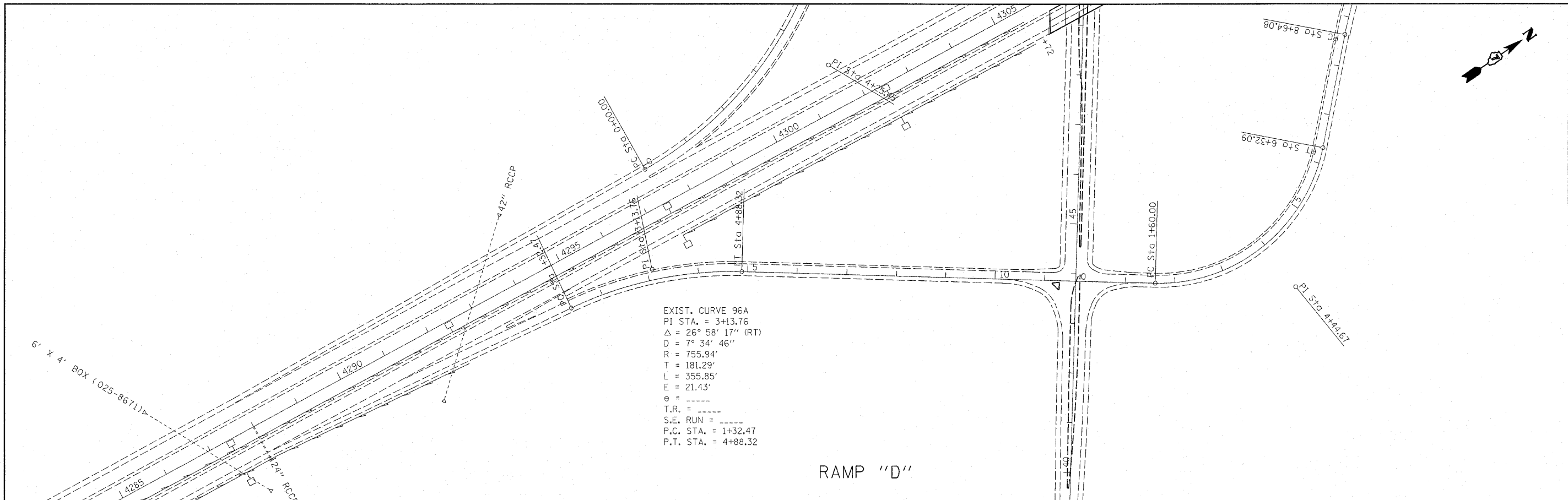
EXIST. CURVE 90  
 PI STA. = 56+32.97  
 $\Delta = 15^\circ 14' 31''$  (RT)  
 $D = 2^\circ 15' 00''$   
 $R = 2,546.48'$   
 $T = 340.72'$   
 $L = 677.42'$   
 $E = 22.69'$   
 $\theta = \text{-----}$   
 T.R. = -----  
 S.E. RUN = -----  
 P.C. STA. = 52+92.25  
 P.T. STA. = 59+69.66

PT	LOCATION	DESCRIPTION	LID OR EOP	INLET INVERT	OUTLET INVERT	SYM
1	8' LT STA 59+50	INLETS TA T15F&G	101.36	98.43		
2	56' LT STA 59+50	PRC END SECTION 15"			97.95	

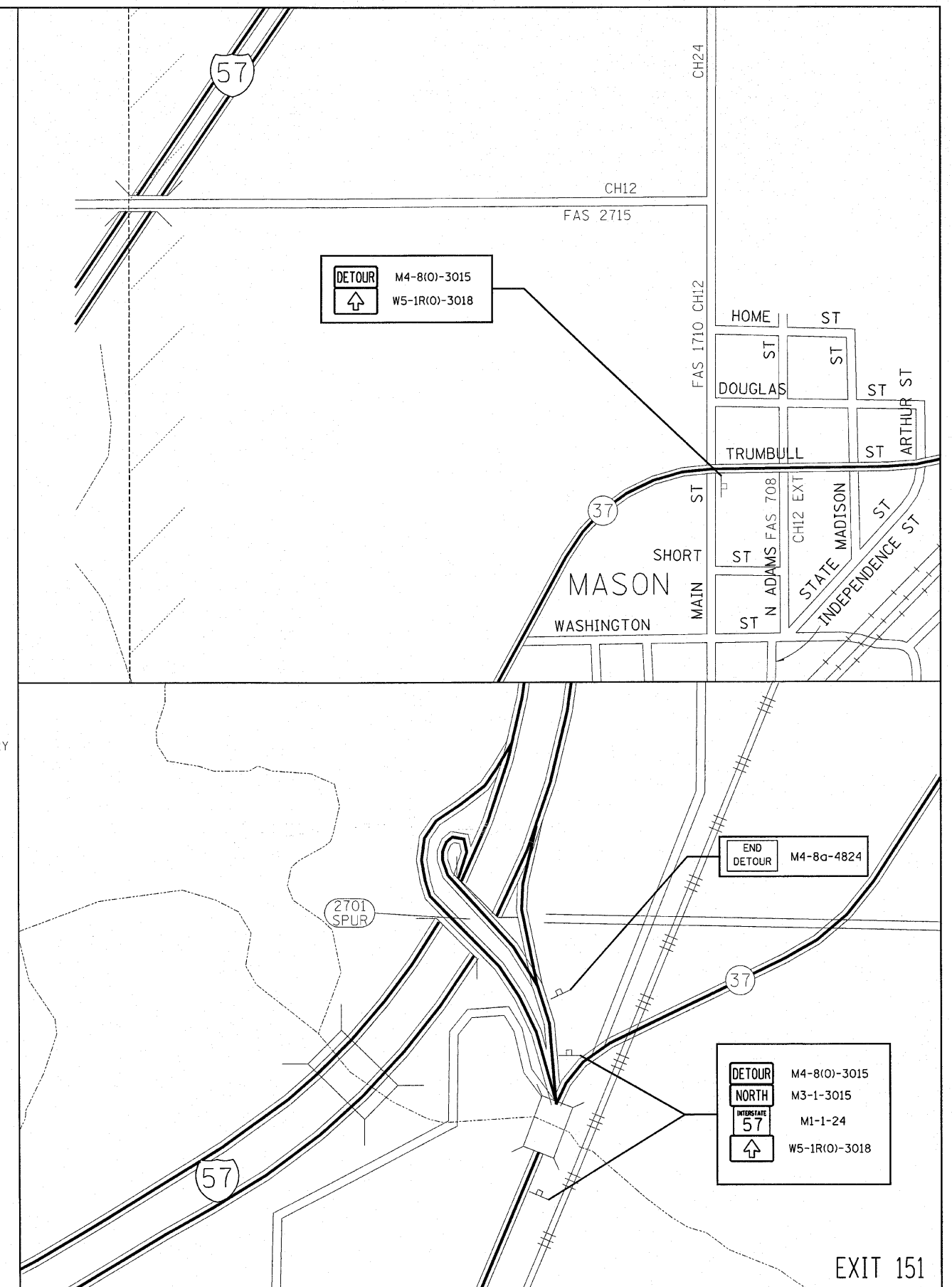
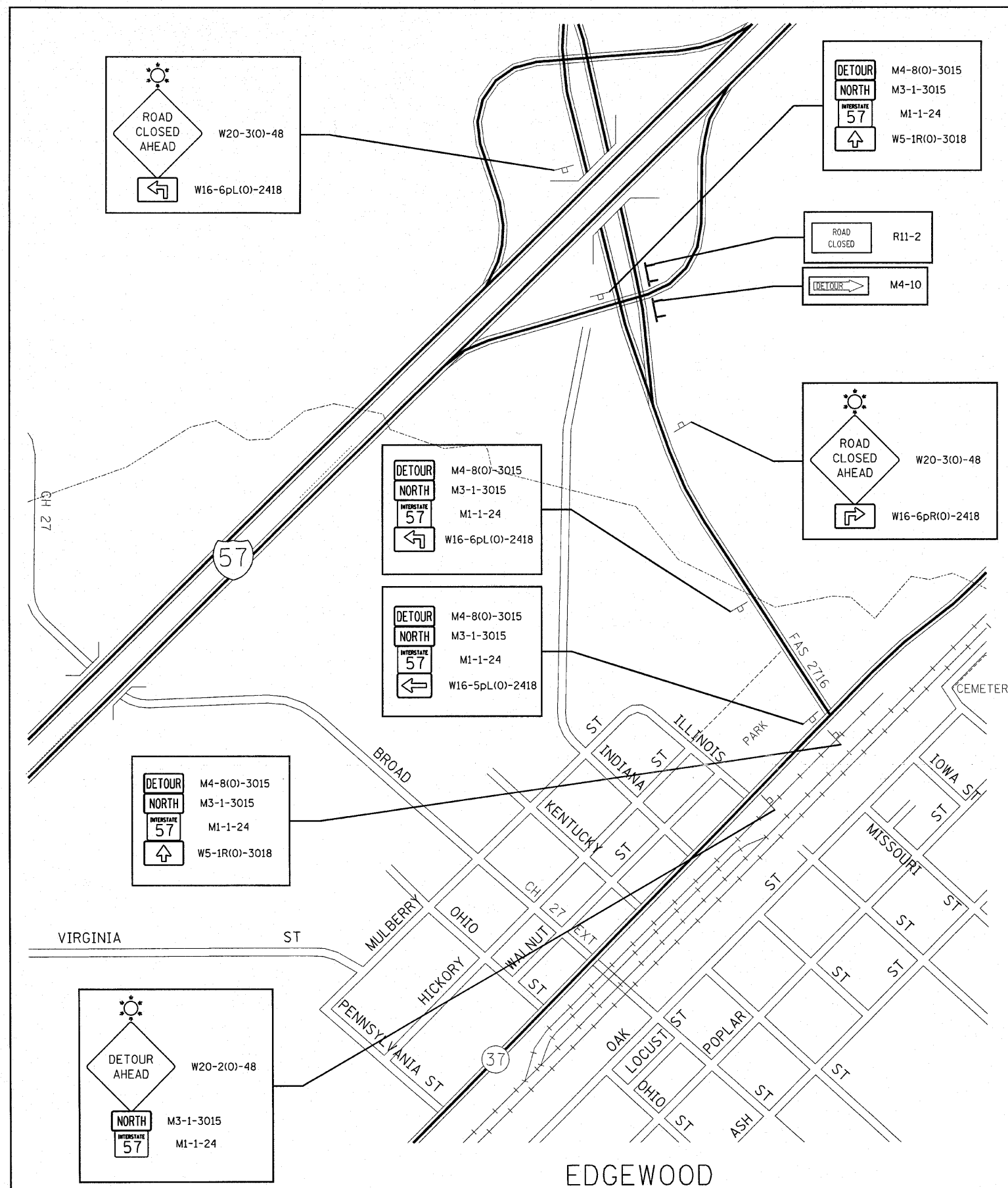
NOTE: TEMPORARY BENCH MARK WITH ASSUMED ELEVATION OF 100.00 IS ON SOUTHEAST BOLT OF SIGN FOUNDATION AT APPROX. 35' LT STA 59+75

COMBINATION CURB AND GUTTER REMOVAL LT STA 59+40 TO 59+70	30 FEET
CONCRETE MEDIAN SURFACE REMOVAL LT STA 59+40 TO 59+70	240 SQ FT
CLASS D PATCHES, TYPE II, 14 INCH LT STA 59+50	9.3 SQ YD
SHOULDER REMOVAL AND REPLACEMENT 10" LT STA 59+50	6.7 SQ YD
COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06 LT STA 59+40 TO 59+70	30 FEET
CONCRETE MEDIAN SURFACE, 4 INCH LT STA 59+40 TO 59+70	240 SQ FT

PT. TO PT.	TYPE 1 15" LIN FT	TRENCH BACKFILL CU YD
1-2	42	5.5
TOTALS	42	5.5



FILE NAME =	USER NAME = teasleyck	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PLAN SHEET RAMP C &amp; D</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwork\pwork\teasleyck\d0148697\074417-shs-plan.dgn		DRAWN -	REVISED -		57	(25-6-1)R&(13-1)R		44	21			
PLOT SCALE = 100.0000 / IN.		CHECKED -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.			CONTRACT NO. 74417				
PLOT DATE = 10/21/2010		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							



FILE NAME =	USER NAME = teasleyck	DESIGNED -	REVISED -
ca:\pw_work\p\dot\teasleyck\d0148697\07	4417-shc-staging.dgn	DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

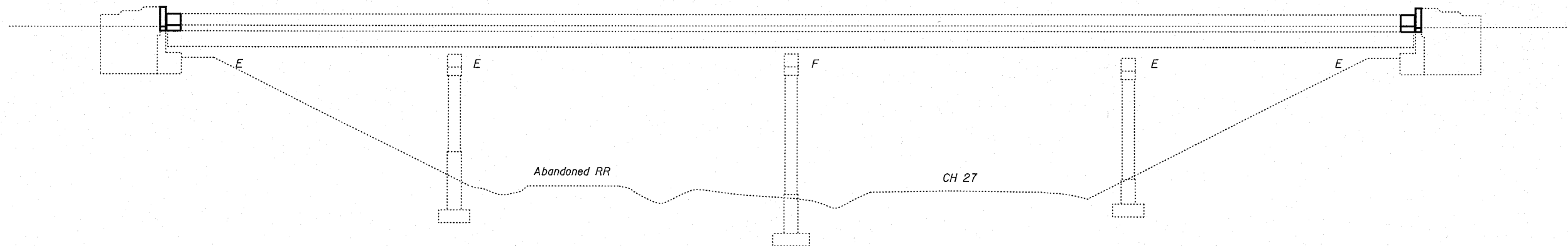
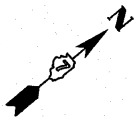
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DETOUR SIGNING</b>			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

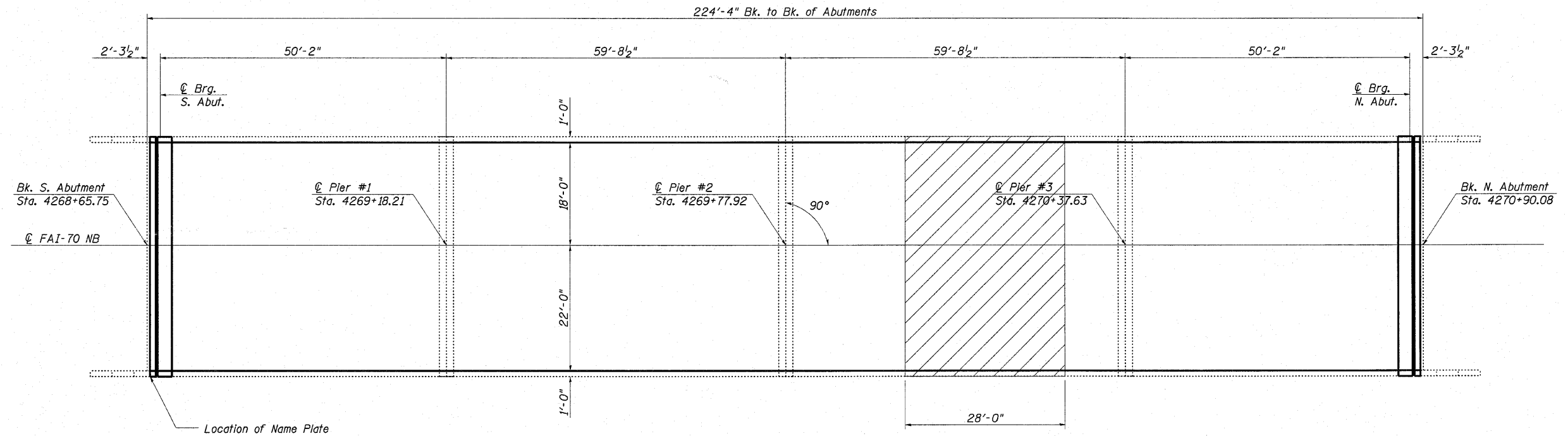
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(13-1)R&(25-6-1)R		44	22
CONTRACT NO. 74417			[ILLINOIS] FED. AID PROJECT	

Clay, Effingham

The existing four span continuous steel multi-beam structure was constructed in 1969 as FAI-57 section 25-6HVB-1 at Sta. 4269+39.34. SN. 025-0067 carries FAI-57 (Interstate 57). The proposed project consists of new expansion joints, full depth deck repair, new micorsillica wearing surface, and new deck drains.



**ELEVATION**



**PLAN**

LIMITS OF PROTECTIVE SHIELD



Expires 11/30/2012

FILE NAME =	USER NAME = biermarkl	DESIGNED - KLB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>GENERAL PLAN &amp; ELEVATION SN. 025-0067</b>			F.A.I. RTE. 57	SECTION (13-1)R&(25-6-UR	COUNTY	TOTAL SHEETS 44	SHEET NO. 23
es:\pwork\p\dot\biermarkl\d0148697\074417-shr-brp\brp.f.dgn	PLOT SCALE = 20.0000 1/1 IN.	DRAWN - KLB	REVISED -		SCALE: _____	SHEET NO. 1 OF 20 SHEETS	STA. _____	TO STA. _____	CONTRACT NO. 74417		ILLINOIS FED. AID PROJECT	
	PLOT DATE = 10/21/2010	CHECKED -	REVISED -									
		DATE -	REVISED -									

**GENERAL NOTES**

Plan Dimensions and details relative to the existing structure have been taken from existing plans and are subject to nominal construction variations. It shall be the contractor's responsibility to verify dimensions and details in the field, to make necessary approved adjustments prior to construction or material acquisition, such variations shall not be cause for additional compensation or change in the scope of work. The contractor will be paid for the quantity actually furnished at the unit bid price for the work.

Reinforcement bars shall conform to the requirements of ASTM A 706 GRADE 60. See Special Provisions.

Reinforcement Bars designated (E) shall be epoxy coated.

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. any reinforcement bars that are damaged during concrete removal operations shall be replaced using an approved bar splicer or anchorage system. Cost included in CONCRETE REMOVAL.

Joint openings shall be adjusted according to Article 520.04 of the Standard Specifications when the deck is poured at an ambient temperature other than 50°F.

Areas of deck repairs shown are estimated. The Engineer shall show actual locations of deck repairs on as-built plans.

Removal and replacement of handrail sections and support posts at both abutments will be necessary for construction of the expansion joints. The existing handrail sections and support posts shall be reused, new bolts, shim plates, and post support anchor assemblies as detailed in the plans are to be provided and installed for the replacement of the handrail and supports. This work and all materials shall be included in the contract unit price for CONCRETE SUPERSTRUCTURE.

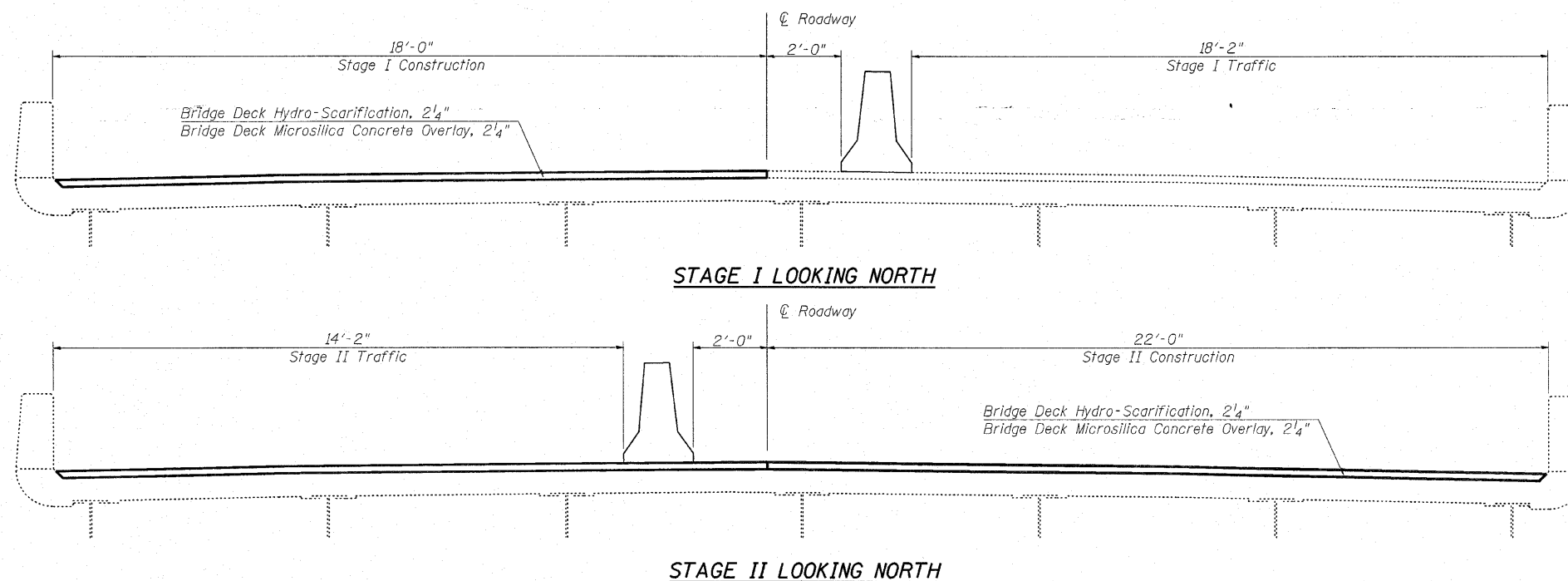
Prior to pouring the new concrete deck, all heavy and loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

Removal and installation of the existing name plate on the structure will be necessary for construction of the expansion joint. This work and all materials shall be included in the contract unit price for RELOCATING NAME PLATE.

All structural steel shall conform to AASHTO Classification M-270 Gr. 36 unless otherwise noted.

**TOTAL BILL OF MATERIALS**

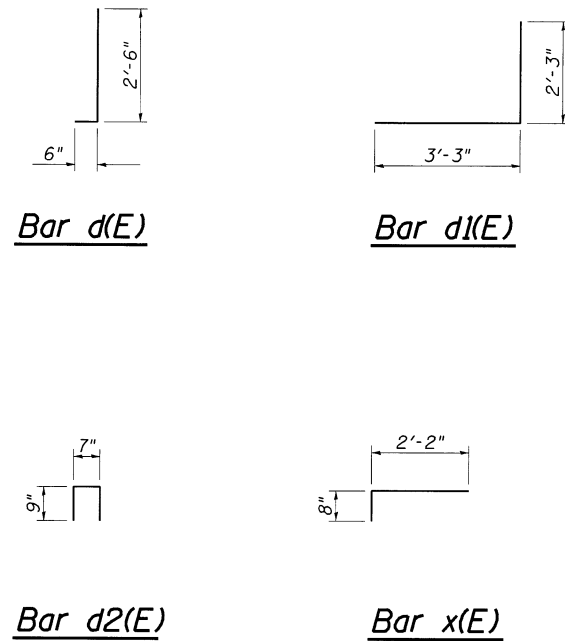
ITEM	UNIT	QUANTITY
Concrete Removal	Cu. Yd.	12.1
Concrete Superstructure	Cu. Yd.	12.2
Reinforcement Bars, Epoxy Coated	Pound	1700
Bar Splicers	Each	28
Preformed Joint Strip Seal	Foot	82
Bridge Deck Hydro-Scarification, 2 1/4"	Sq Yd	959
Bridge Deck Microsilica Concrete Overlay, 2 1/4"	Sq Yd	959
Bridge Deck Grooving	Sq Yd	932
Protective Coat	Sq Yd	995
Deck Slab Repair (Full Depth, Type I)	Sq Yd	1
Deck Slab Repair (Full Depth, Type II)	Sq Yd	62
Floor Drains	Each	24
Plug Existing Deck Drains	Each	4
Protective Shield	Sq Ft	131
Relocating Name Plates	Each	1



FILE NAME =	USER NAME = biermarkl	DESIGNED - KLB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>GENERAL NOTES &amp; BILL OF MATERIALS SN. 025-0067</b>	F.A.L. RTE. 57	SECTION (13-1)R&(25-6-1)R	COUNTY	TOTAL SHEETS 44	SHEET NO. 24	
ca:\pwork\pwork\bidot\biermarkl\d0148697\07	4417-sht-brgenote.dgn	DRAWN - KLB	REVISED -			SCALE: _____	SHEET NO. 2 OF 20 SHEETS	STA. _____	TO STA. _____	CONTRACT NO. 74417	
	PLOT SCALE = 20,000' / IN.	CHECKED -	REVISED -			ILLINOIS FED. AID PROJECT					
	PLOT DATE = 10/21/2010	DATE -	REVISED -			Clay, Effingham					



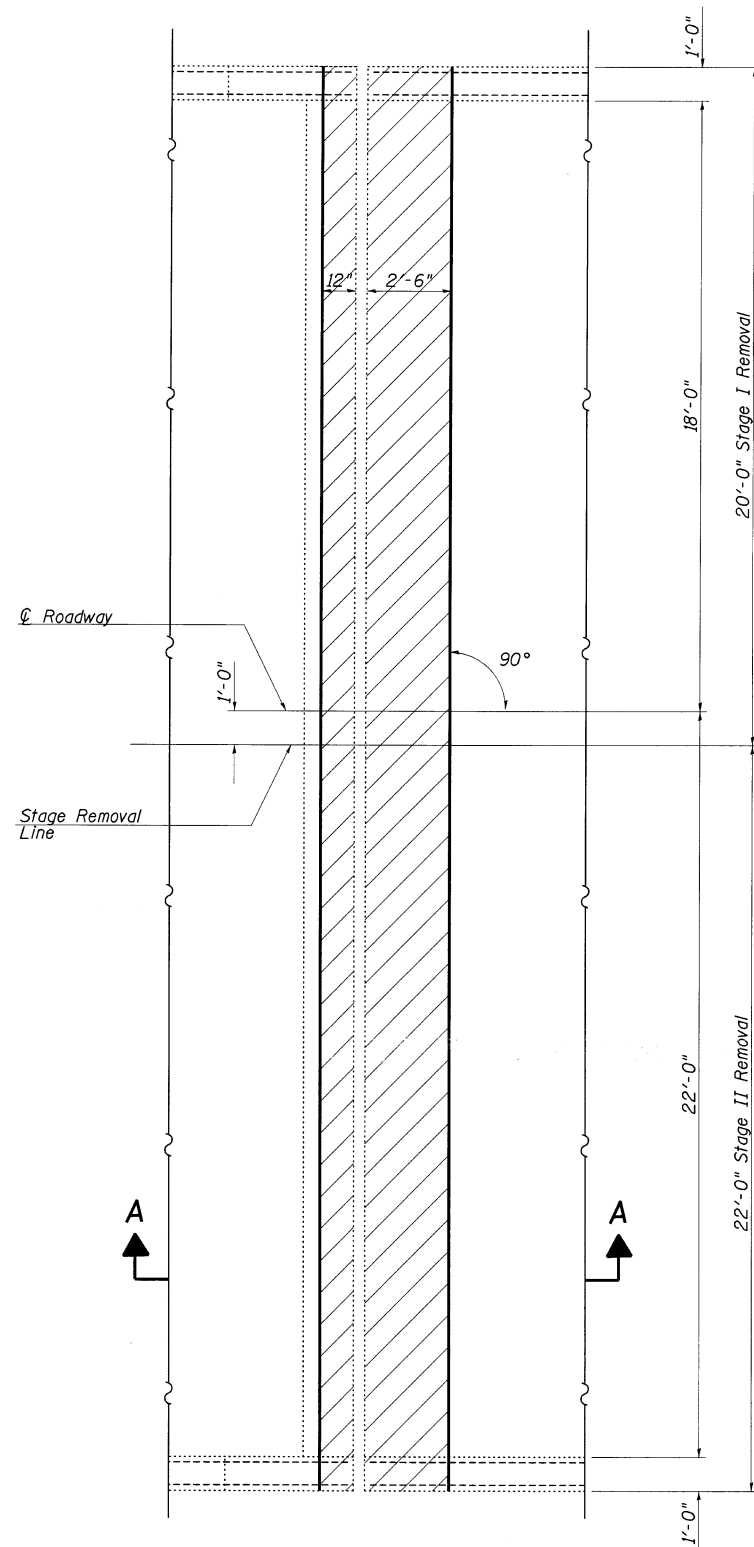
Hatched area indicates removal.



**BILL OF MATERIAL**

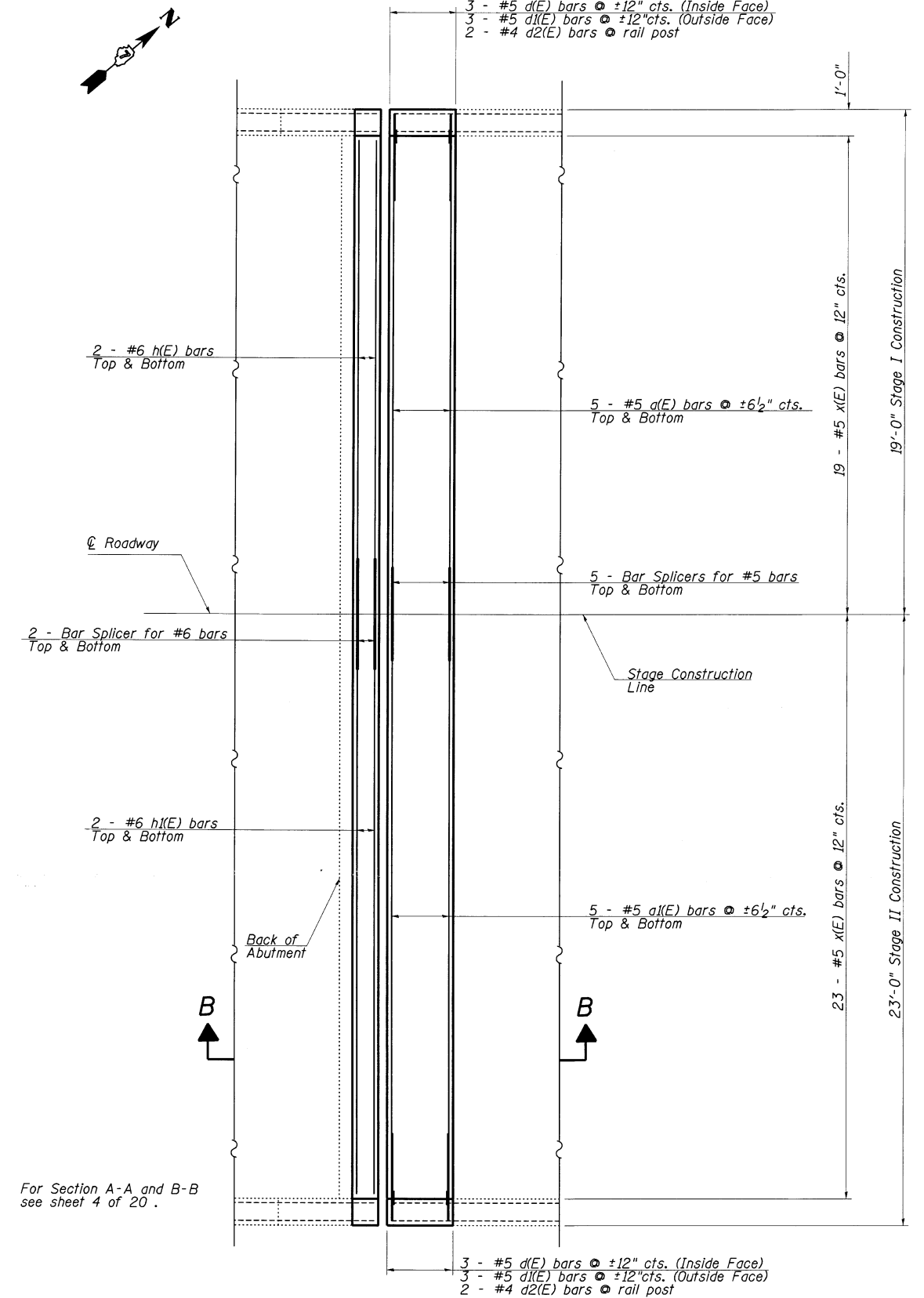
PER ABUTMENT

BAR	NUMBER OF BARS		TOTAL	SIZE	LENGTH	SHAPE
	STAGE I	STAGE II				
a (E)	10		10	#5	18'-8"	—
a1(E)		10	10	#5	22'-8"	—
d (E)	3	3	6	#5	3'-0"	J
d1(E)	3	3	6	#5	5'-6"	J
d2(E)	2	2	4	#4	2'-1"	□
h (E)	4		4	#6	17'-8"	—
h1(E)		4	4	#6	21'-8"	—
x (E)	19	23	42	#5	2'-10"	—
REINFORCEMENT BARS (EPOXY COATED)					POUND	850
CONCRETE REMOVAL					CU YD	6.05
CONCRETE SUPERSTRUCTURE					CU YD	6.1



**EXISTING PARTIAL PLAN**

(South Abutment shown; North Abutment similar)

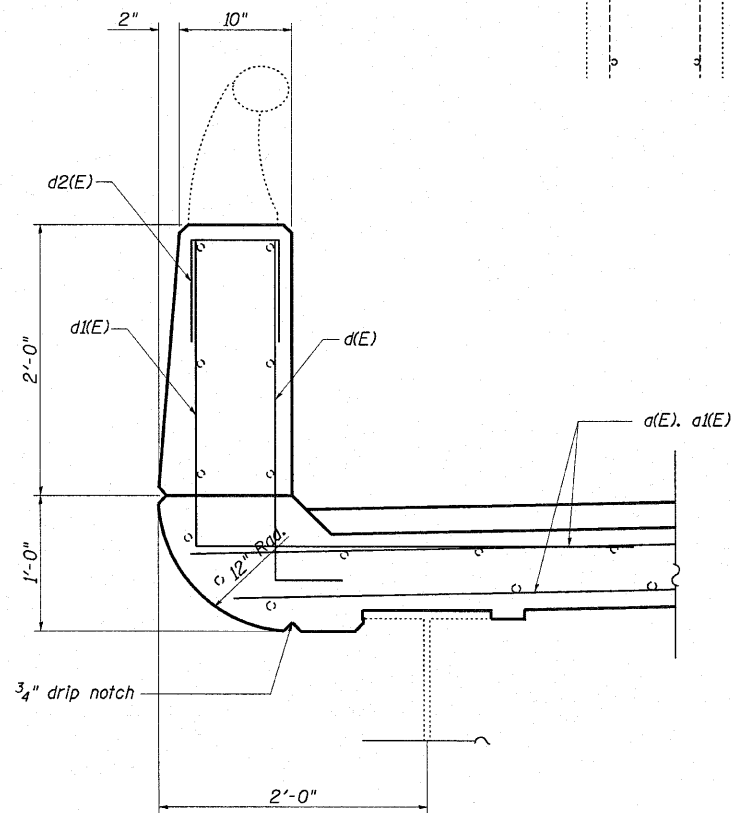
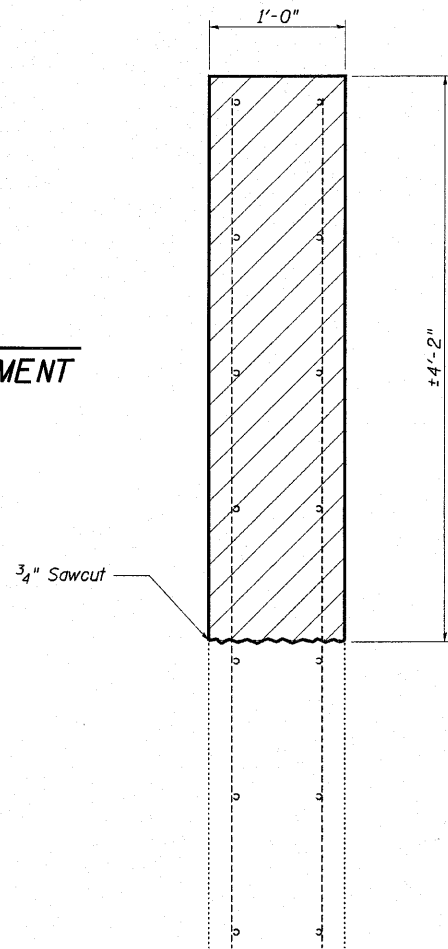


**PROPOSED PARTIAL PLAN**

(South Abutment shown; North Abutment similar)

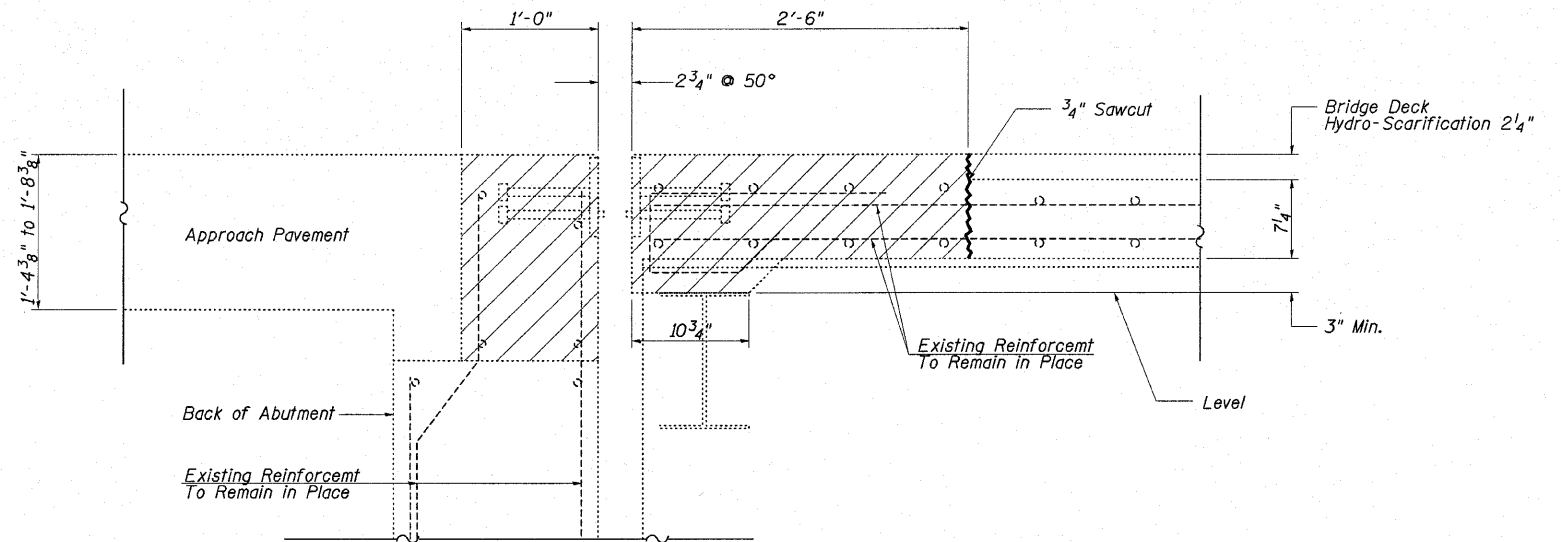
FILE NAME =	USER NAME = biermark1	DESIGNED - KLB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EXPANSION JOINT REPLACEMENT DETAILS SN. 025-0067</b>	F.A.I. RTE. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et\pwwork\pwwid\dot\biermark1\d0148697\07417-sh1-b\rdetails.dgn	DRAWN - KLB	REVISED -	57			(13-1)R&25-6-UB		44	25	
PLOT SCALE = 20.0000' / IN.	CHECKED -	REVISED -	CONTRACT NO. 74417							
PLOT DATE = 10/21/2010	DATE -	REVISED -	ILLINOIS FED. AID PROJECT							
SCALE: _____		SHEET NO. 3 OF 20 SHEETS		STA. _____ TO STA. _____		Clay, Effingham				

**TYPICAL WINGWALL  
REMOVAL AND REPLACEMENT**



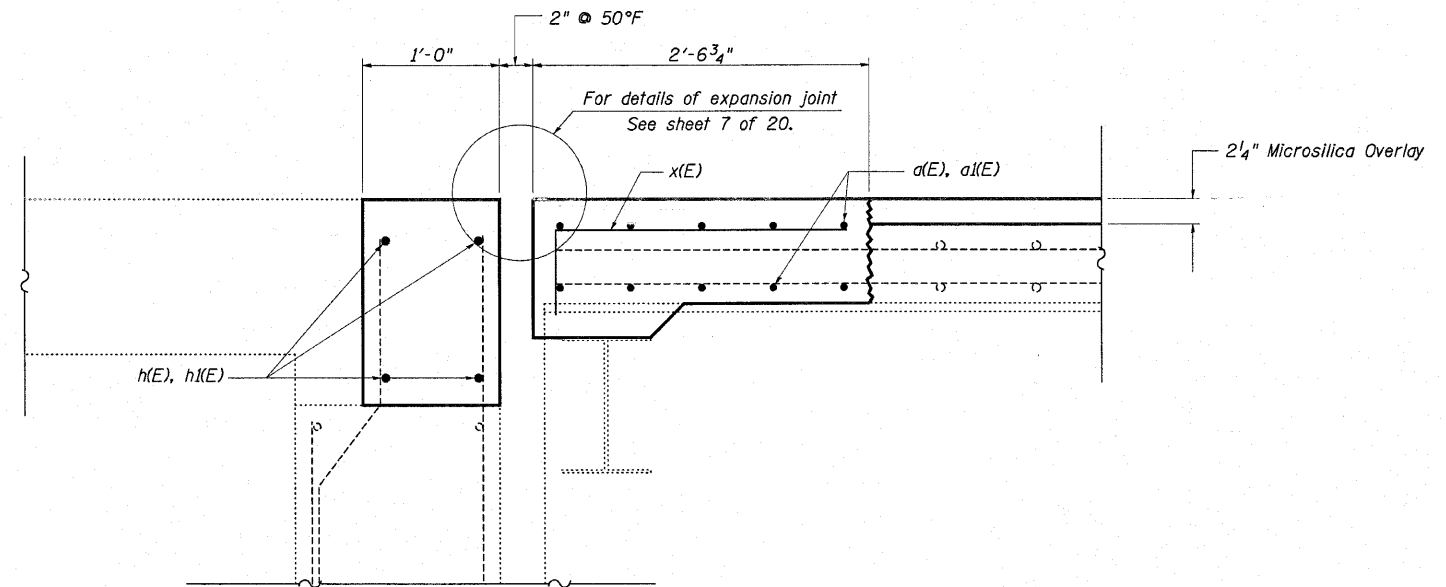
**SECTION THRU PARAPET**

Hatched area indicates removal.



**SECTION A-A**  
(Dimensions at Rt. L's to end of deck)

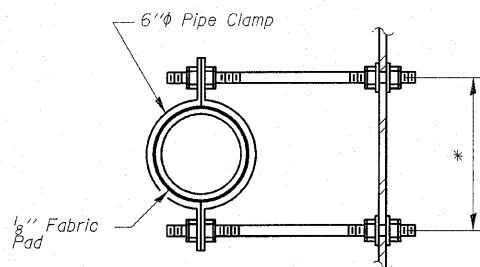
○ Existing Reinforcement  
● Proposed Reinforcement



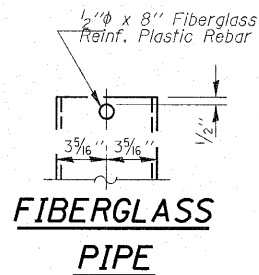
**SECTION B-B**  
(Dimensions at Rt. L's to end of deck)

FILE NAME =	USER NAME = bstermark1	DESIGNED - KLB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EXPANSION JOINT REPLACEMENT DETAILS SN. 025-0067</b>		F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pwork\pwork\stermark1\00148697\07	417-sht-brdeta11s.dgn	DRAWN - KLB	REVISED -		57	(13-1)R&(25-6-1)R		44	26		
	PLOT SCALE = 20.0000 / IN.	CHECKED -	REVISED -		SCALE: SHEET NO. 4 OF 20 SHEETS STA. TO STA.		CONTRACT NO. 74417		ILLINOIS FED. AID PROJECT		
	PLOT DATE = 10/21/2010	DATE -	REVISED -		ILLINOIS FED. AID PROJECT		CONTRACT NO. 74417		ILLINOIS FED. AID PROJECT		

\* Dimension as required by Pipe Clamp

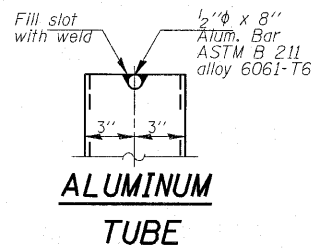


**SECTION A-A**

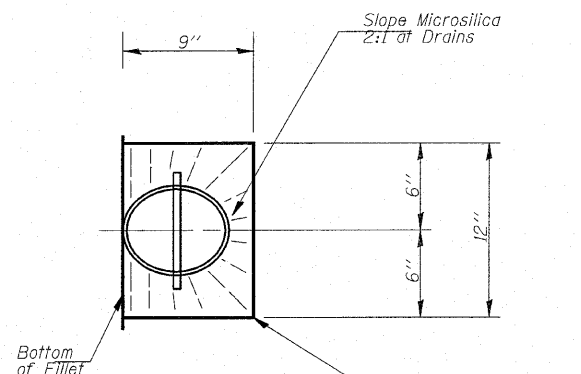


**FIBERGLASS PIPE**

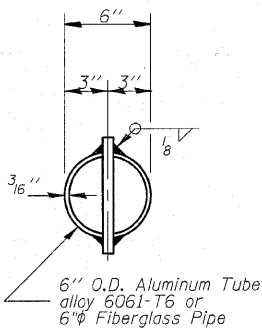
\*\* Concrete removal and replacement dimensions shall be determined by the Engineer based on the extent of deck deterioration at each drain.



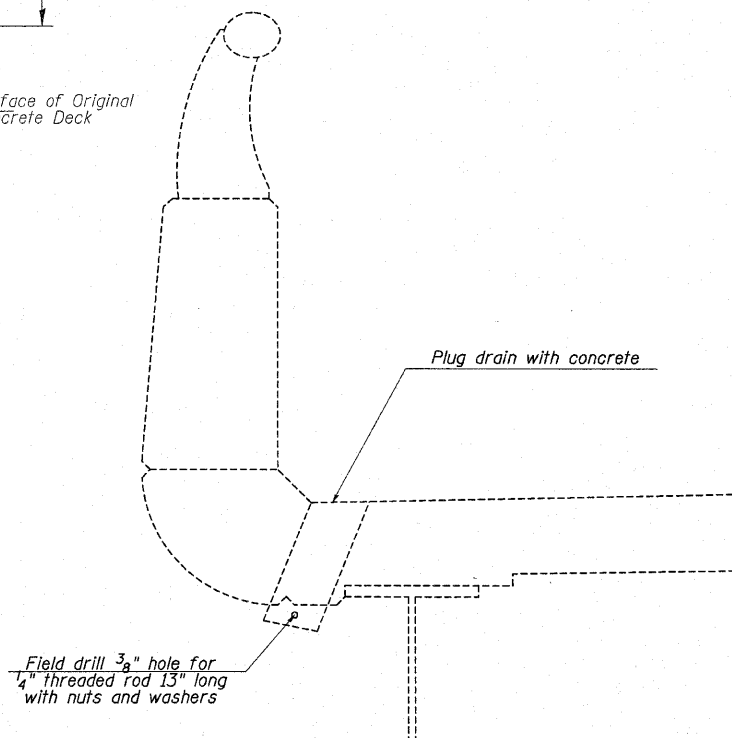
**ALUMINUM TUBE**



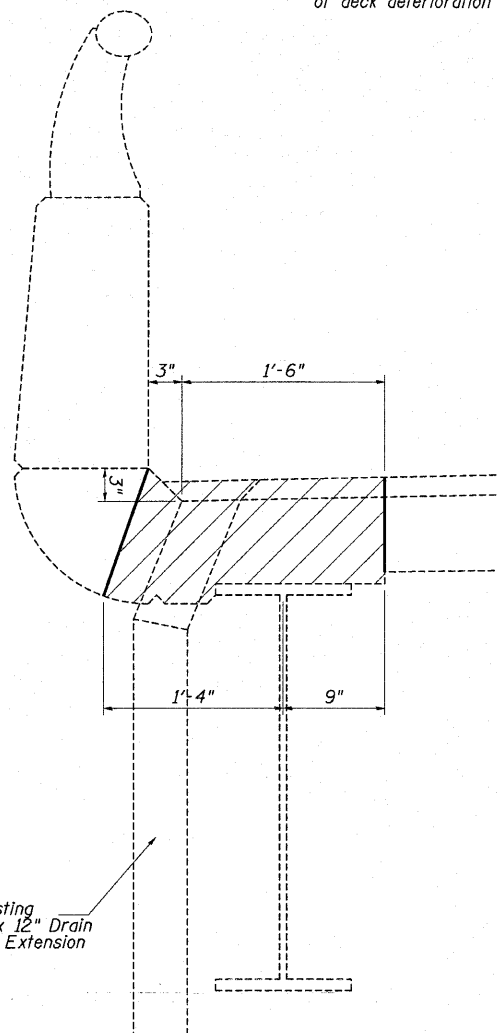
**TOP PLAN**



**TOP PLAN**  
(Showing Aluminum Tube)



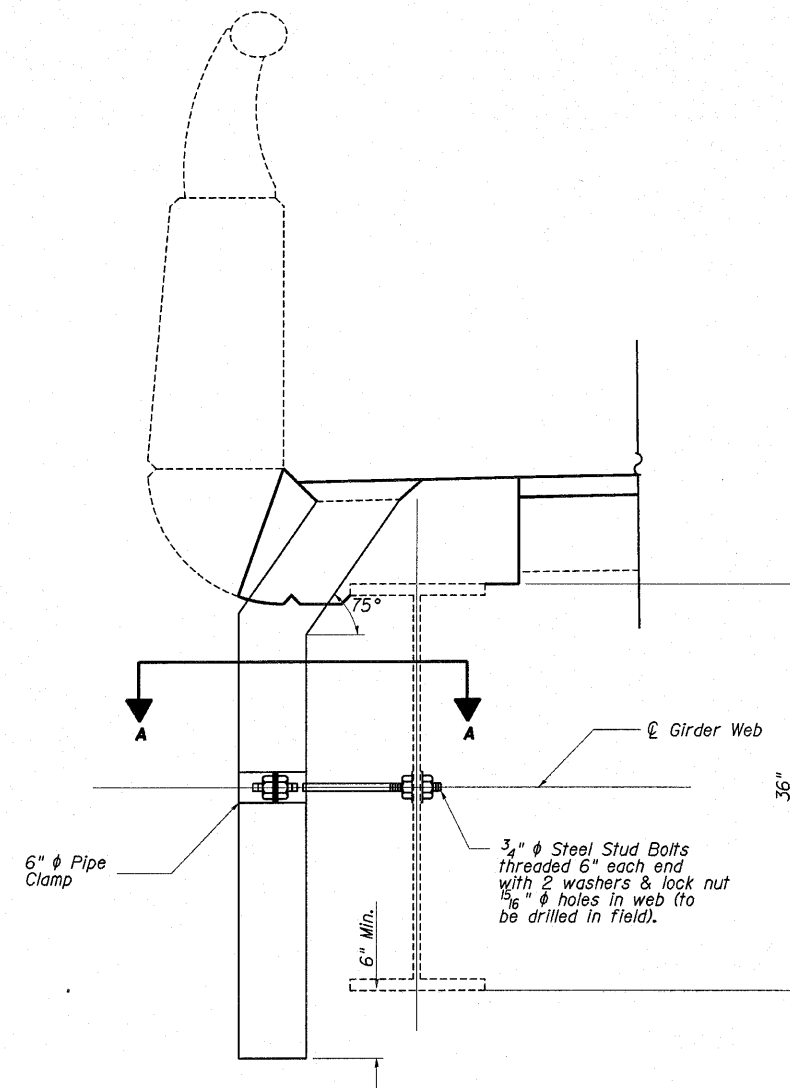
**DRAINS TO BE PLUGGED DETAIL**



**\*\* SECTION THRU EXISTING FLOOR DRAIN**

Hatched area indicates concrete removal at floor drain replacement.

Note: Concrete removal and replacement quantities for drains are included in Deck Slab Repair as shown on "Bridge Deck Patching" plan sheets.



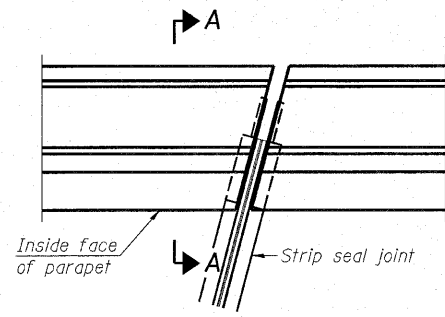
**DRAIN REPLACEMENT DETAIL**

Note: See "Bridge Deck Patching" plan sheets for Floor Drain replacement location and Plug Existing Deck Drain locations. See "Total Bill of Materials" for each structure for quantities.

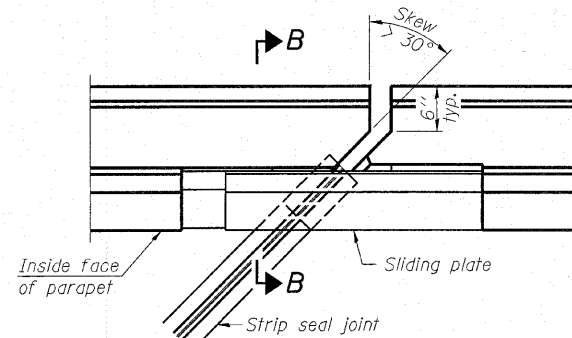
Notes: The floor drains shall be painted to match the outside of the fascia beams. Fiberglass pipe shall conform to ASTM D 2996, with short-time rupture strength hoop tensile stress of 30,000 p.s.i. minimum. Galvanize clamping device according to AASHTO M232. Cost of clamping device and galvanizing is included with Floor Drains.

FILE NAME =	USER NAME = biermarkl	DESIGNED - KLB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>FLOOR DRAIN DETAILS SN. 025-0067</b>	F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ea:\pwork\pwork\bid\0148697\07417-shl-brdetails.dgn		DRAWN - KLB	REVISED -			57	(13-1)R&(25-6-UR		44	27
PLOT SCALE = 20,0000' / IN.		CHECKED -	REVISED -			CONTRACT NO. 74417				
PLOT DATE = 10/21/2010		DATE -	REVISED -			ILLINOIS FED. AID PROJECT				
					SCALE: _____	SHEET NO. 5 OF 20 SHEETS	STA. _____	TO STA. _____		

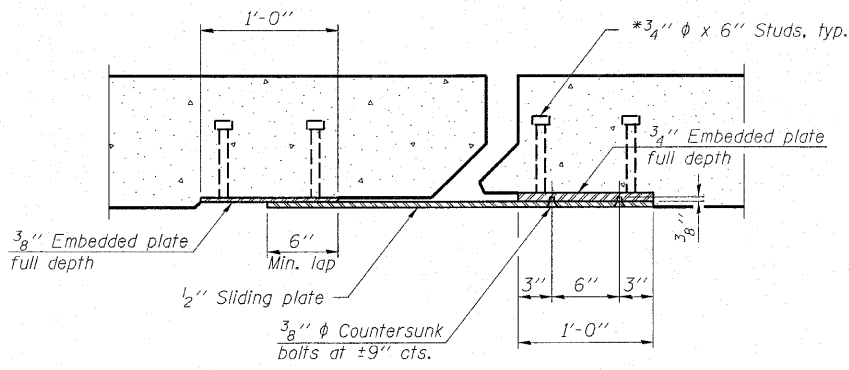




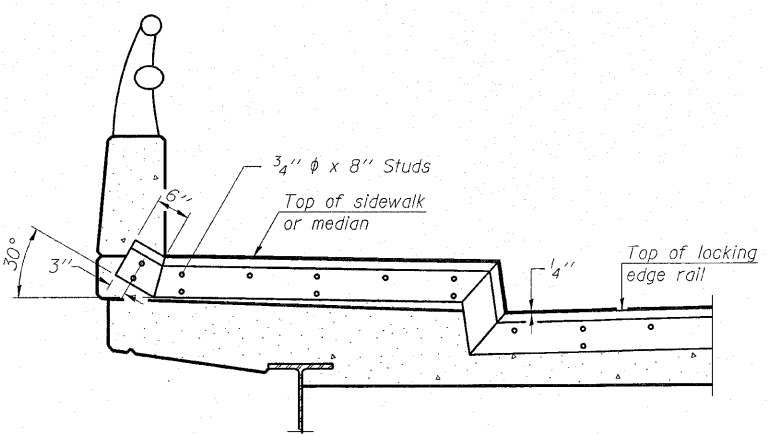
**PLAN**  
(For skews  $\le 30^\circ$ )



**PLAN**  
(For skews  $> 30^\circ$ )  
Showing point block

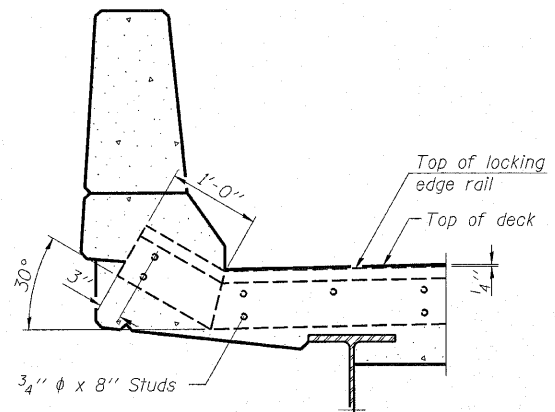


**SECTION C-C**

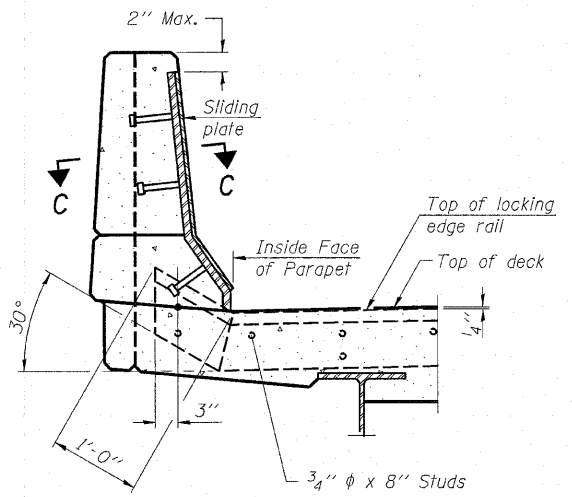


**TYPICAL END TREATMENT AT SIDEWALK OR MEDIAN**

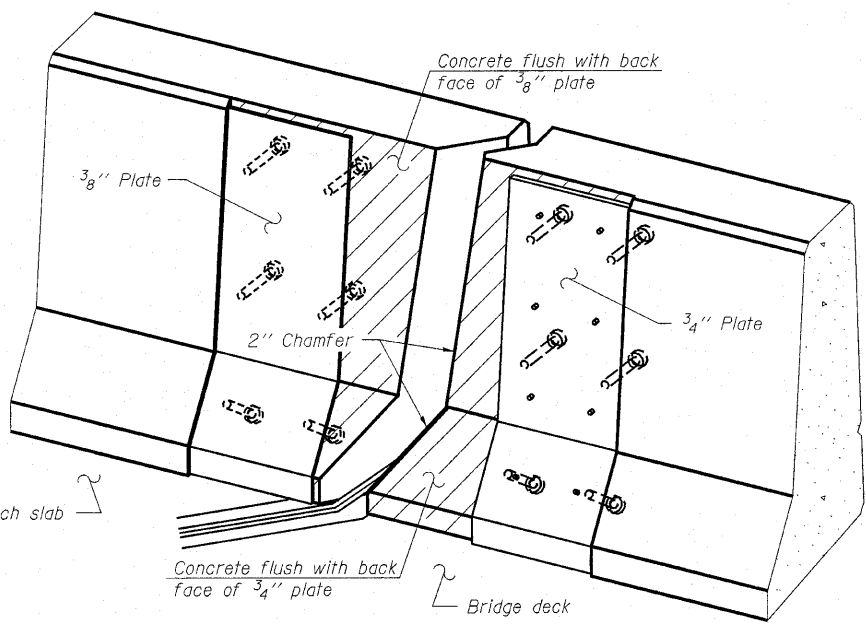
Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.



**SECTION A-A**



**SECTION B-B**



**TRIMETRIC VIEW**  
(Showing back plates only)

**Notes:**

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities.

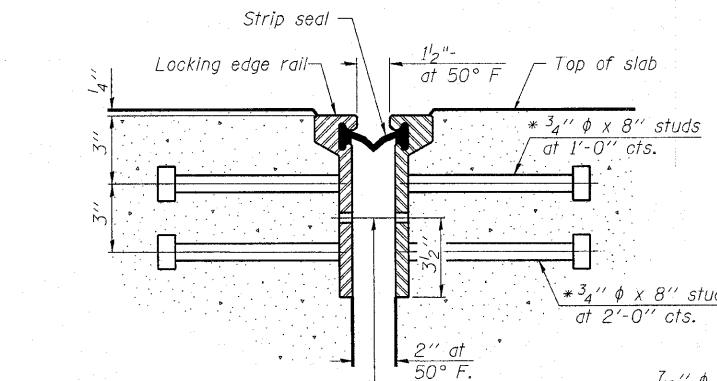
The manufacturer's recommended installation methods shall be followed.

The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.

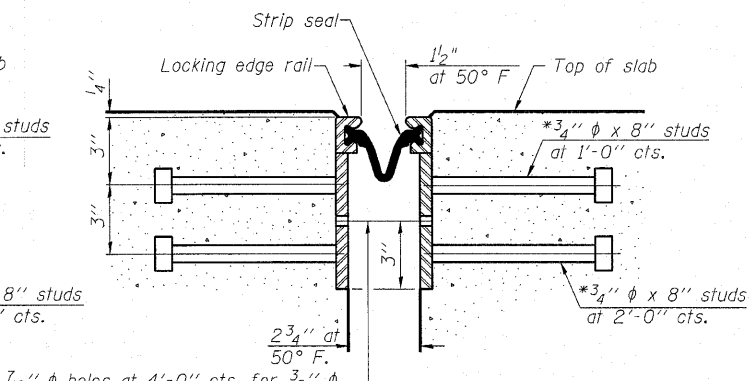
All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

Maximum space between rail segments at stage lines shall be 3/16", sealed with a suitable sealant.

Parapet plates and anchorage studs for skews  $> 30^\circ$  included in the cost of Preformed Joint Strip Seal.



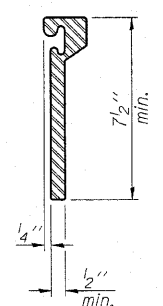
**SECTION THRU ROLLED RAIL JOINT**



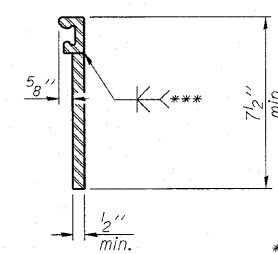
**SECTION THRU WELDED RAIL JOINT**

7/16"  $\phi$  holes at 4'-0" cts. for 3/8"  $\phi$  bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

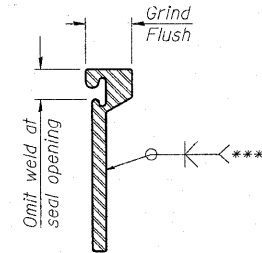
7/16"  $\phi$  holes at 4'-0" cts. for 3/8"  $\phi$  bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.



**ROLLED EXTRUDED RAIL**



**WELDED RAIL**



**LOCKING EDGE RAIL SPLICE**

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

\*\*\* Back gouge not required if complete joint penetration is verified by mock-up.

**LOCKING EDGE RAILS**

**BILL OF MATERIAL**

Item	Unit	Total
Preformed Joint Strip Seal	Foot	82

EJ-SSJ

7-1-10

FILE NAME =	USER NAME = biermarkl
DESIGNED -	REVISD -
DRAWN -	REVISD -
CHECKED -	REVISD -
DATE -	REVISD -

\* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

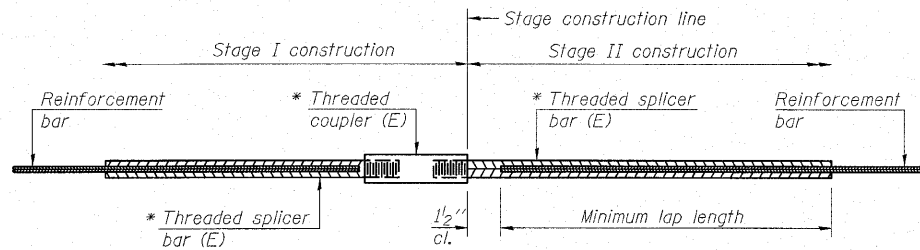
**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

**PREFORMED JOINT STRIP SEAL STRUCTURE NO. 025-0067**

F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(13-1)R&(25-6-1)R		44	29
CONTRACT NO. 74417			ILLINOIS FED. AID PROJECT	

SCALE: SHEET NO. 7 OF 20 SHEETS STA. TO STA.

Clay, Effingham



**STANDARD BAR SPLICER ASSEMBLY**

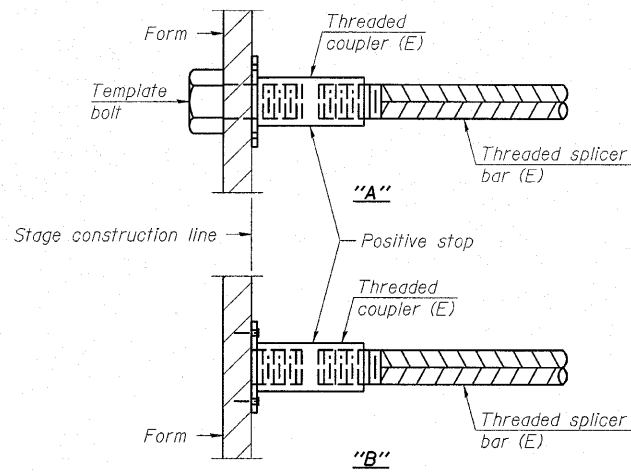
Minimum Lap Lengths					
Bar size to be spliced	Table 1	Table 2	Table 3	Table 4	Table 5
3, 4	1'-5"	1'-11"	2'-1"	2'-4"	2'-3"
5	1'-9"	2'-5"	2'-7"	2'-11"	2'-10"
6	2'-1"	2'-11"	3'-1"	3'-6"	3'-4"
7	2'-9"	3'-10"	4'-2"	4'-8"	4'-6"
8	3'-8"	5'-1"	5'-5"	6'-2"	5'-10"
9	4'-7"	6'-5"	6'-10"	7'-9"	7'-5"

- Table 1: Black bar, 0.8 Class C
- Table 2: Black bar, Top bar lap, 0.8 Class C
- Table 3: Epoxy bar, 0.8 Class C
- Table 4: Epoxy bar, Top bar lap, 0.8 Class C
- Table 5: Epoxy bar, Top bar lap, Class B

Threaded splicer bar length = min. lap length + 1/2" + thread length

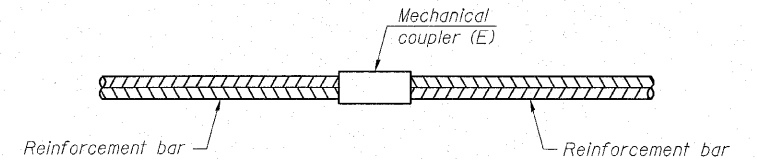
\* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length
025-0067	#5	20	Table 3
025-0067	#6	8	Table 3



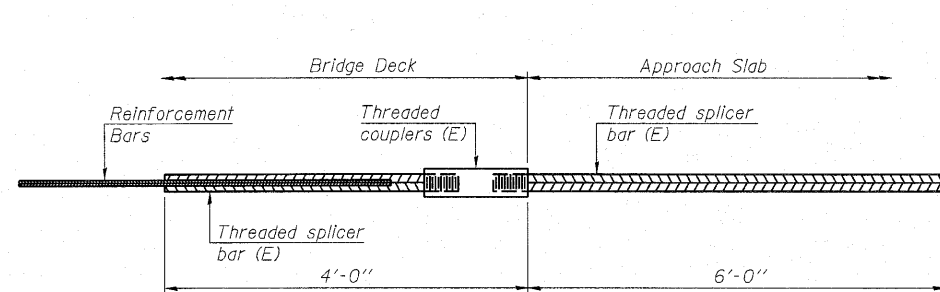
**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.  
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.  
 (E) : Indicates epoxy coating.



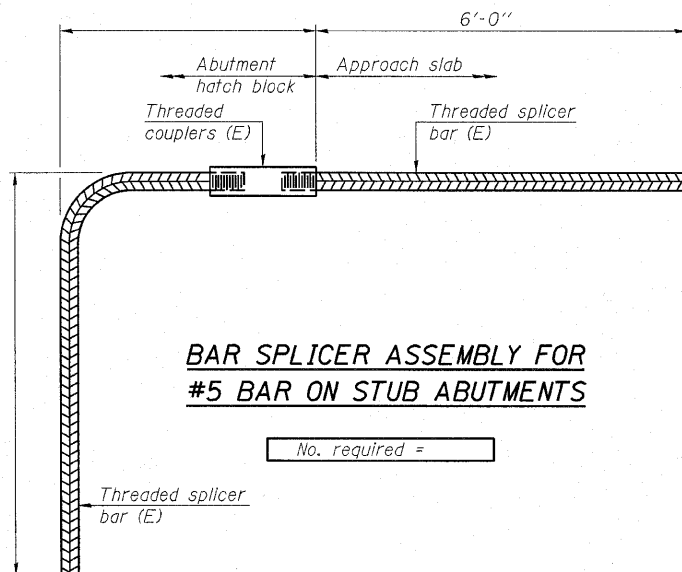
**STANDARD MECHANICAL SPLICER**

Location	Bar size	No. assemblies required



**BAR SPLICER ASSEMBLY FOR #5 BAR ON INTEGRAL OR SEMI-INTEGRAL ABUTMENTS**

No. required =



**BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS**

No. required =

**NOTES**

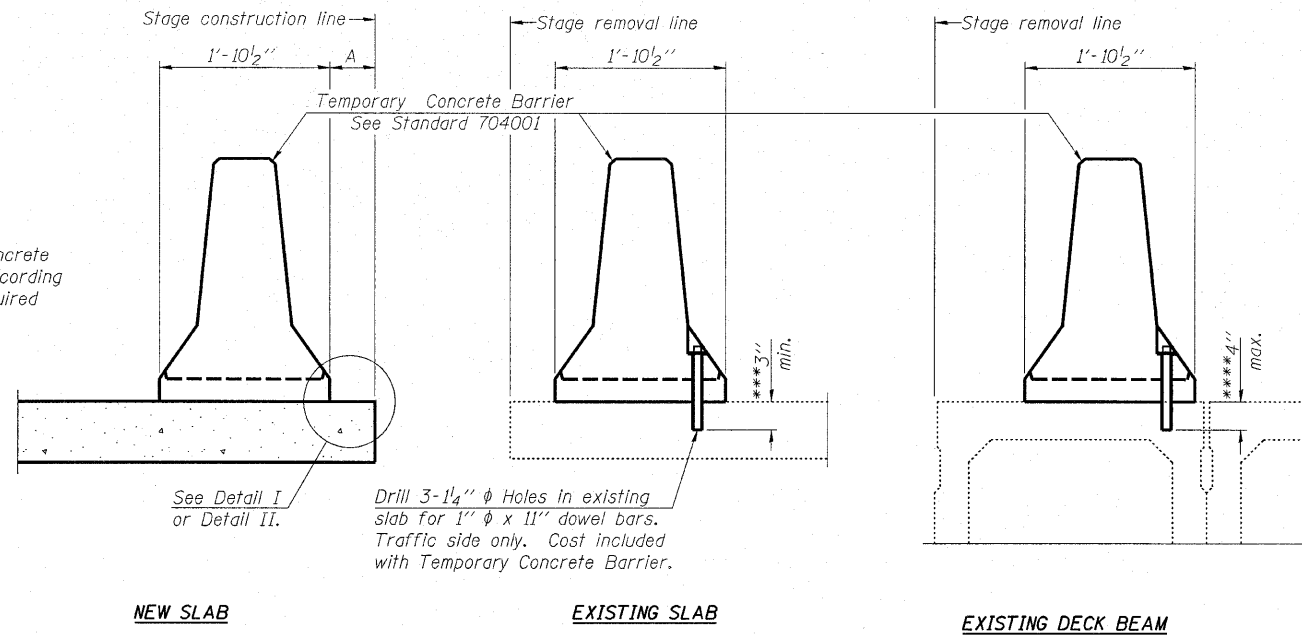
Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.  
 All reinforcement shall be lapped and tied to the splicer bars.  
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.  
 See special provision for Mechanical Splicers.  
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

7-1-10

FILE NAME =	USER NAME = b1armerk1	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS STRUCTURE NO. 025-0067</b>	F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pawork\pawidot\b1armerk1\d0148697\07-417-sht-brdtdetails.dgn	PLOT SCALE = 28.0000' / IN.	DRAWN -	REVISED -			57	(13-1)R&(25-6-UR		44	30
PLOT DATE = 10/21/2010	DATE -	CHECKED -	REVISED -			CONTRACT NO. 74417				
						ILLINOIS FED. AID PROJECT				

When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".



NEW SLAB

EXISTING SLAB

EXISTING DECK BEAM

SECTIONS THRU SLAB OR DECK BEAM

NOTES

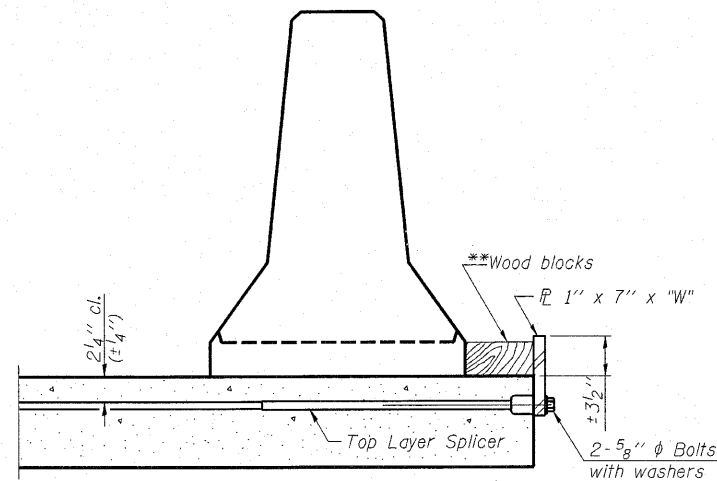
Detail I - With Bar Splicer or Couplers:  
Connect one (1) 1" x 7" x "W" steel  $\bar{P}$  to the top layer of couplers with 2- $\frac{5}{8}$ "  $\phi$  bolts screwed to coupler at approximate  $\bar{C}$  of each barrier panel.

Detail II - With Extended Reinforcement Bars:  
Connect one (1) 1" x 7" x "W" steel  $\bar{P}$  to the concrete slab or concrete wearing surface with 2- $\frac{5}{8}$ "  $\phi$  Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate  $\bar{C}$  of each barrier panel.

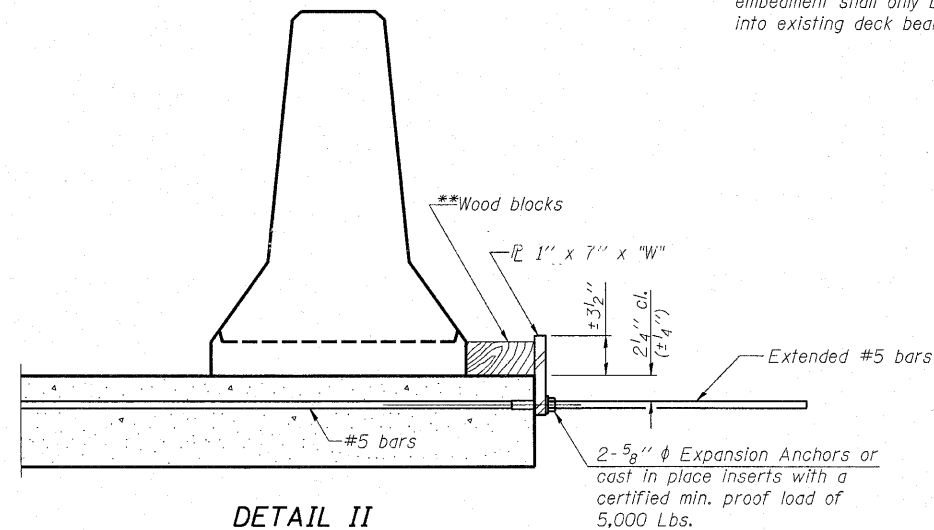
Cost of anchorage is included with Temporary Concrete Barrier. The 1" x 7" x "W" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

\*\*\* Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.

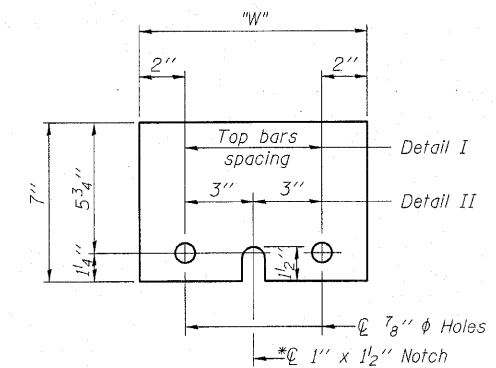
\*\*\*\* If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



DETAIL I



DETAIL II



STEEL RETAINER  $\bar{P}$  1" x 7" x "W"

\* Required only with Detail II

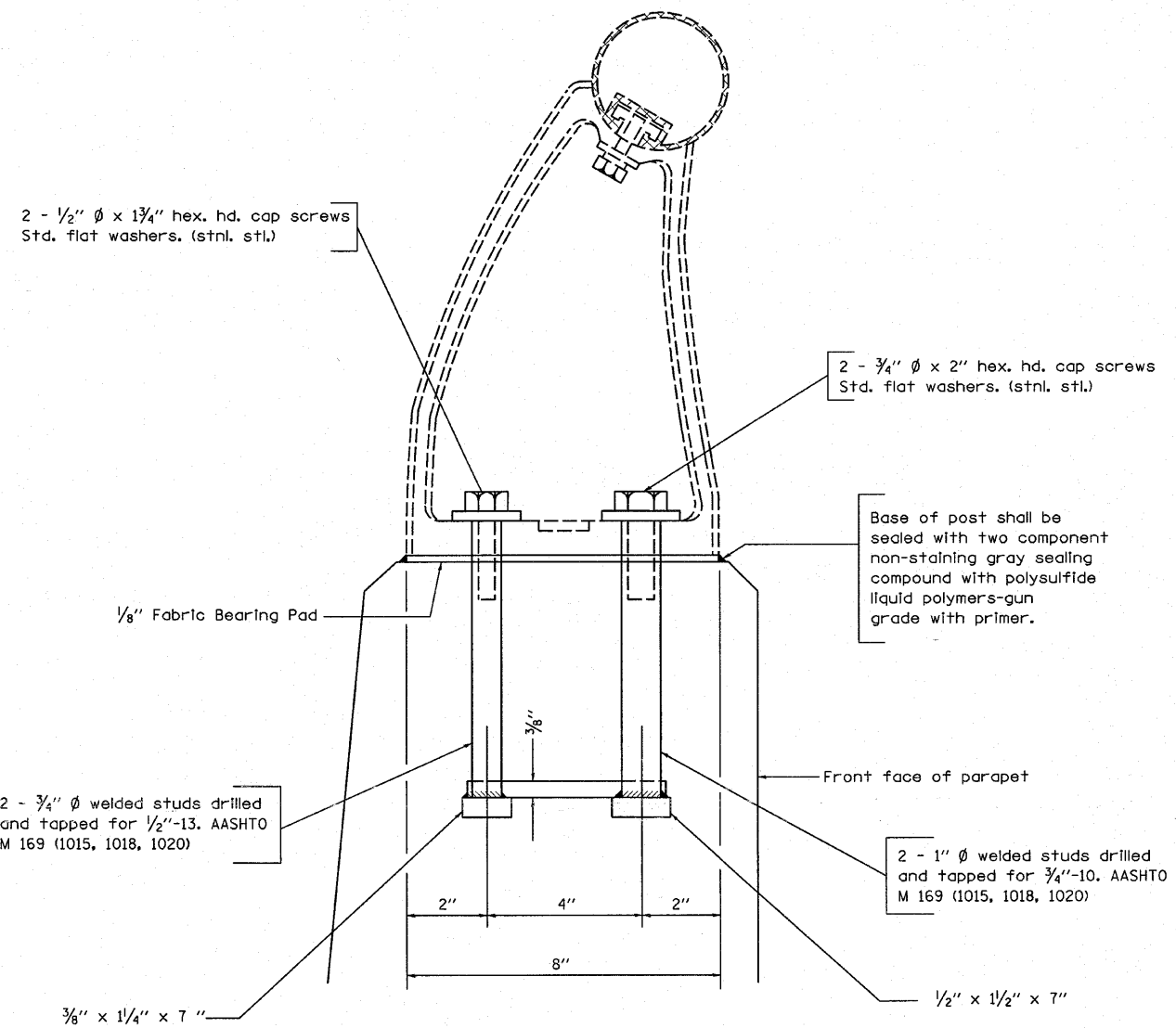
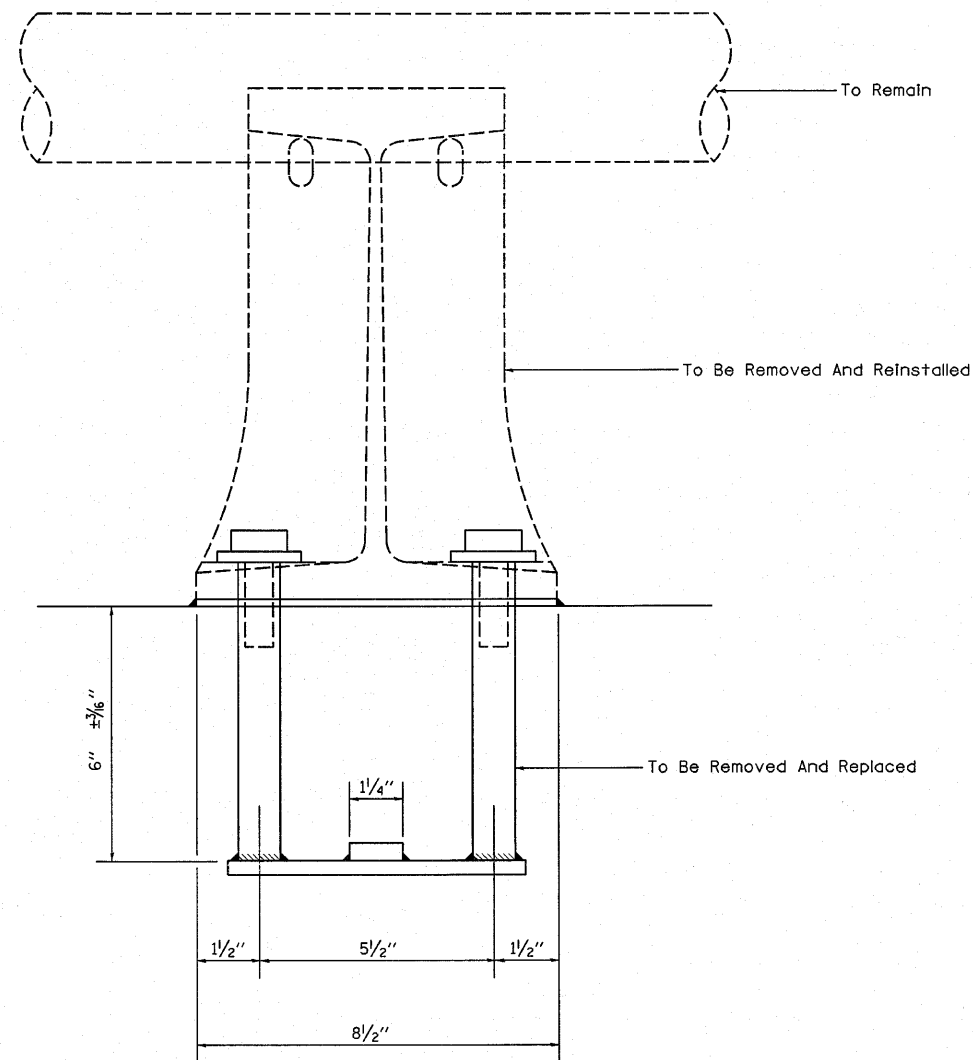
\*\* Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.

"W" = Top bars spacing + 4"

R-27

7-1-10

FILE NAME =	USER NAME = biermark1	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION STRUCTURE NO. 025-0067</b>		F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ce:\pwwork\pwwork\dot\biermark1\d0148697\07417-sht-brd\details.dgn	417-sht-brd\details.dgn	DRAWN -	REVISED -		-57-	(I3-1R&25-6-1R		44	31		
PLOT SCALE = 20,0000 "/ IN.	CHECKED -	REVISED -	REVISED -		CONTRACT NO. 74417						
PLOT DATE = 10/21/2010	DATE -	REVISED -	REVISED -		ILLINOIS FED. AID PROJECT						

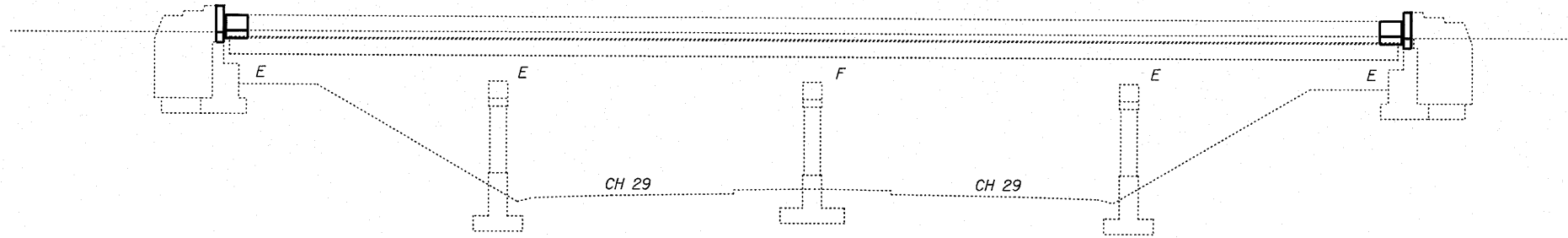
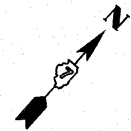


Note: New Rail Post anchorage devices will be required at each location where posts are connected to new construction. Cost shall be included with Concrete Superstructure.

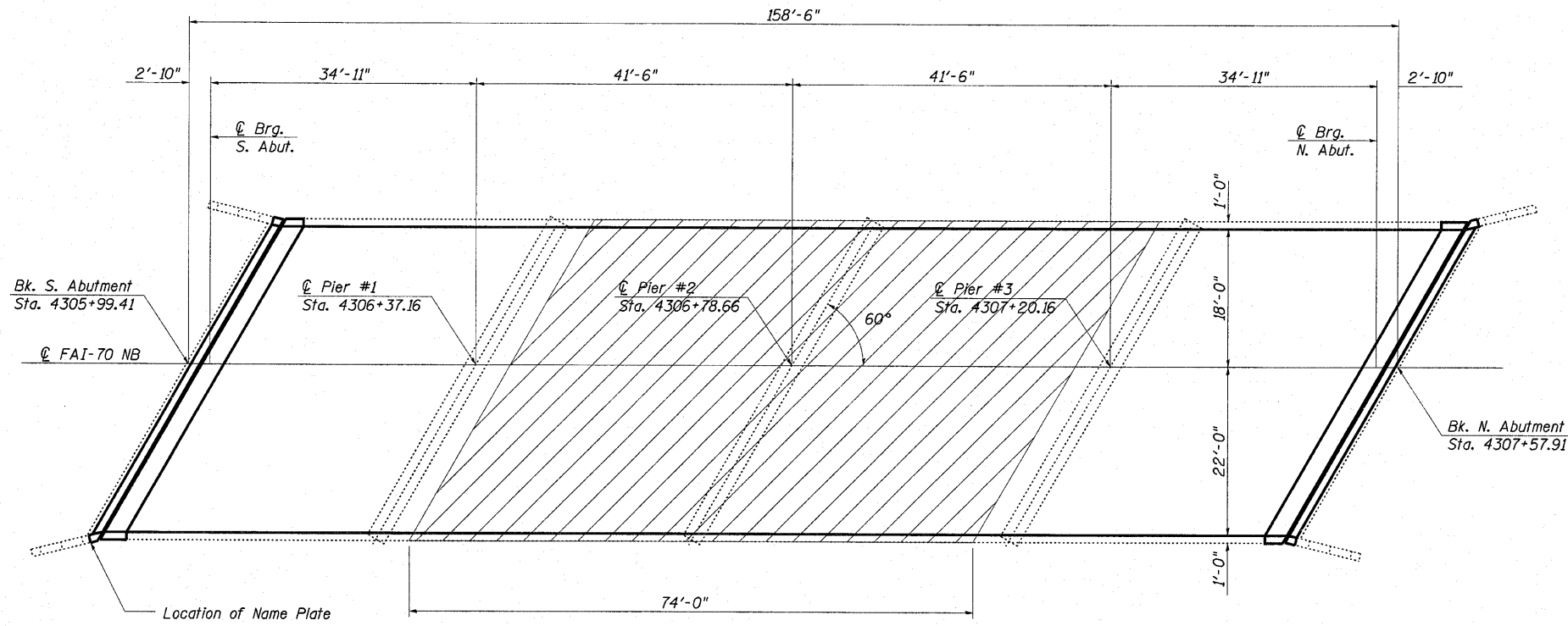
FILE NAME =	USER NAME = bstermark1	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>RAIL SUPPORT ANCHOR DETAILS SN. 025-0067</b>	F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ca:\proj\work\proj\dot\bstermark1\d0148697\07	417-sht-brdtdetails.dgn	DRAWN -	REVISED -			57	(13-1)R&(25-6-UR		44	32	
	PLOT SCALE = 20,0000' / IN.	CHECKED -	REVISED -			CONTRACT NO. 74417		ILLINOIS FED. AID PROJECT			
	PLOT DATE = 10/21/2010	DATE -	REVISED -			SCALE: _____	SHEET NO. 10 OF 20 SHEETS	STA. _____	TO STA. _____		




The existing four span continuous steel multi-beam structure was constructed in 1969 as FAI-57 section 25-6HB-2 at Sta. 4307+05.80. SN. 025-0069 carries FAI-57 (Interstate 57). The proposed project consists of new expansion joints, full depth deck repair, new micorsilica wearing surface, and new deck drains.



**ELEVATION**



**PLAN**

 LIMITS OF PROTECTIVE SHIELD



Expires 11/30/2012

FILE NAME =	USER NAME = biermerk1	DESIGNED - KLB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>GENERAL PLAN &amp; ELEVATION SN. 025-0069</b>		F.A.I. RTE. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pwork\pwork\dot\biermerk1\d0148697\07	4417-sht-brplnpr.f.dgn	DRAWN - KLB	REVISED -		SCALE: _____	SHEET NO. 11 OF 20 SHEETS	STA. _____ TO STA. _____	-57-	(I3-11R&25-6-UB)	.	44	33
	PLOT SCALE = 20.0000' / IN.	CHECKED -	REVISED -									
	PLOT DATE = 12/21/2010	DATE -	REVISED -									
							CONTRACT NO. 74417		ILLINOIS FED. AID PROJECT			

**GENERAL NOTES**

Plan Dimensions and details relative to the existing structure have been taken from existing plans and are subject to nominal construction variations. It shall be the contractor's responsibility to verify dimensions and details in the field, to make necessary approved adjustments prior to construction or material acquisition, such variations shall not be cause for additional compensation or change in the scope of work. The contractor will be paid for the quantity actually furnished at the unit bid price for the work.

Reinforcement bars shall conform to the requirements of ASTM A 706 GRADE 60. See Special Provisions.

Reinforcement Bars designated (E) shall be epoxy coated.

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction, any reinforcement bars that are damaged during concrete removal operations shall be replaced using an approved bar splicer or anchorage system. Cost included in CONCRETE REMOVAL.

Joint openings shall be adjusted according to Article 520.04 of the Standard Specifications when the deck is poured at an ambient temperature other than 50°F.

Areas of deck repairs shown are estimated. The Engineer shall show actual locations of deck repairs on as-built plans.

Removal and replacement of handrail sections and support posts at both abutments will be necessary for construction of the expansion joints. The existing handrail sections and support posts shall be reused, new bolts, shim plates, and post support anchor assemblies as detailed in the plans are to be provided and installed for the replacement of the handrail and supports. This work and all materials shall be included in the contract unit price for CONCRETE SUPERSTRUCTURE.

Prior to pouring the new concrete deck, all heavy and loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

The contractor shall take precaution so as to not damage the utility attached to the face of the south abutment backwall when working on SN. 025-0067.

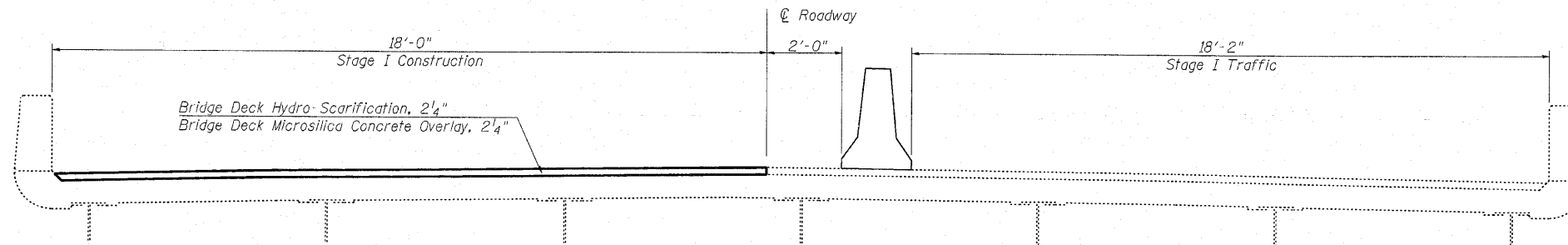
The removal and reinstallation of the first section of guardrail at each abutment to allow construction of the expansion joints shall be included in the contract unit price for CONCRETE SUPERSTRUCTURE.

Removal and installation of the existing name plate on the structure will be necessary for construction of the expansion joint. This work and all materials shall be included in the contract unit price for RELOCATING NAME PLATE.

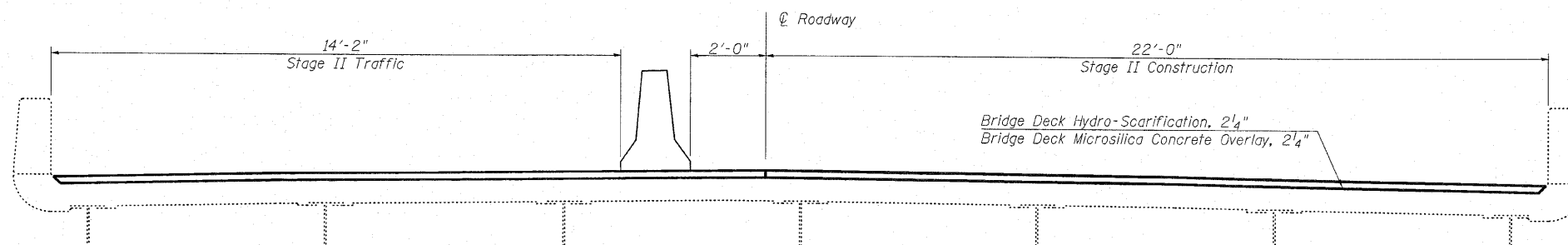
All structural steel shall conform to AASHTO Classification M-270 Gr. 36 unless otherwise noted.

**TOTAL BILL OF MATERIALS**

ITEM	UNIT	QUANTITY
Concrete Removal	Cu. Yd.	14.4
Concrete Superstructure	Cu. Yd.	14.4
Reinforcement Bars, Epoxy Coated	Pound	1940
Bar Splicers	Each	28
Preformed Joint Strip Seal	Foot	95
Bridge Deck Hydro-Scarification, 2 1/4"	Sq Yd	664
Bridge Deck Microsilica Concrete Overlay, 2 1/4"	Sq Yd	664
Bridge Deck Grooving	Sq Yd	655
Protective Coat	Sq Yd	694
Deck Slab Repair (Full Depth, Type II)	Sq Yd	19
Floor Drains	Each	16
Protective Shield	Sq Ft	345
Relocating Name Plates	Each	1



**STAGE I LOOKING NORTH**

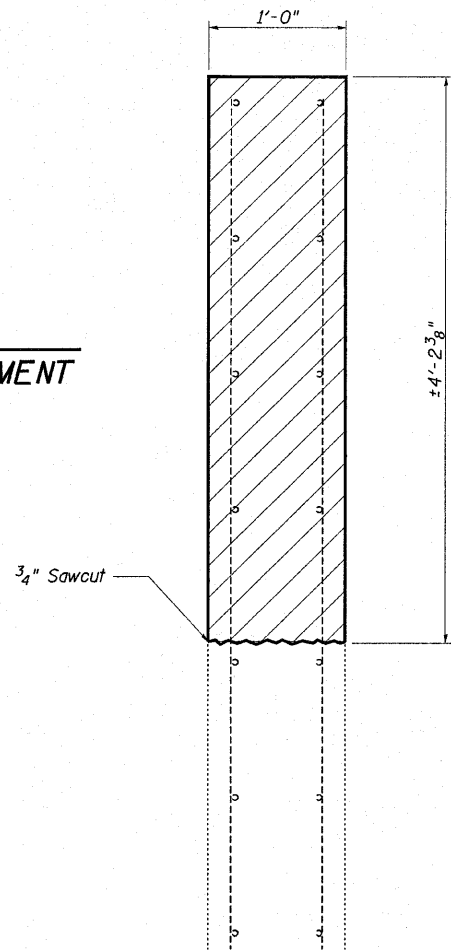


**STAGE II LOOKING NORTH**

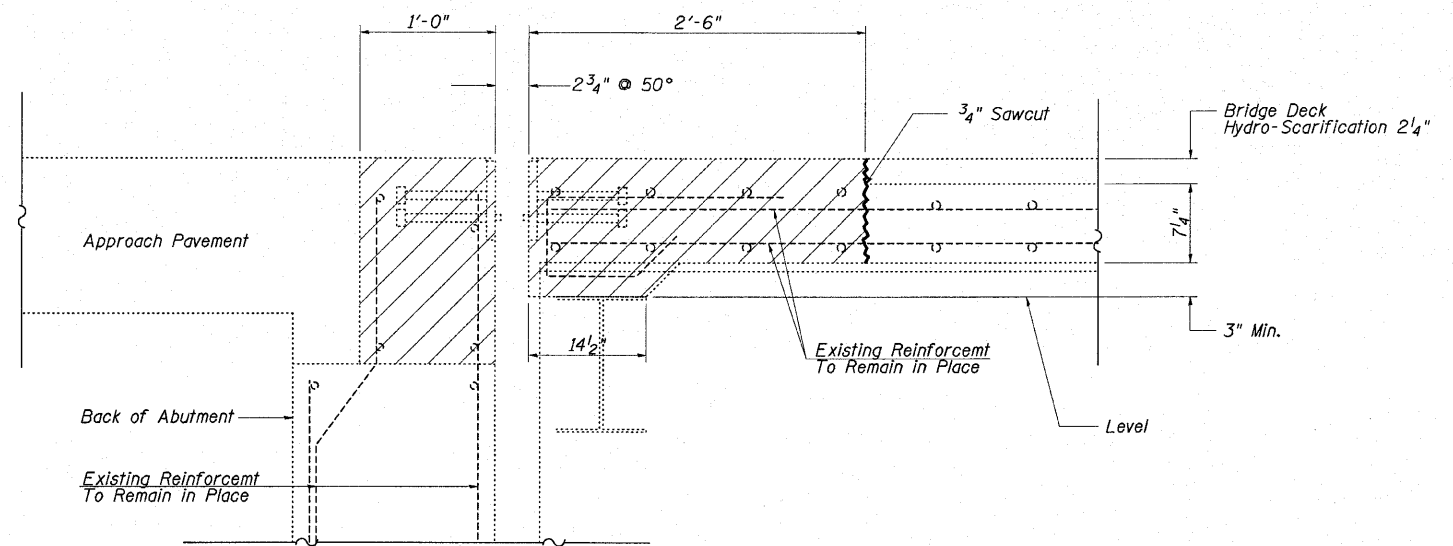
FILE NAME =	USER NAME = btermonk1	DESIGNED - KLB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>GENERAL NOTES &amp; BILL OF MATERIALS SN. 025-0069</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwork\pwork\dot\termonk1\d0148697\07	4417-ght-brgenote.dgn	DRAWN - KLB	REVISED -			57	(13-1)R&(25-6-1)R		44	34
PLOT SCALE = 20.0000' / IN.		CHECKED -	REVISED -			CONTRACT NO. 74417				
PLOT DATE = 10/21/2010		DATE -	REVISED -			ILLINOIS FED. AID PROJECT				
					SCALE:	SHEET NO. 12 OF 20 SHEETS		STA.	TO STA.	



**TYPICAL WINGWALL  
REMOVAL AND REPLACEMENT**

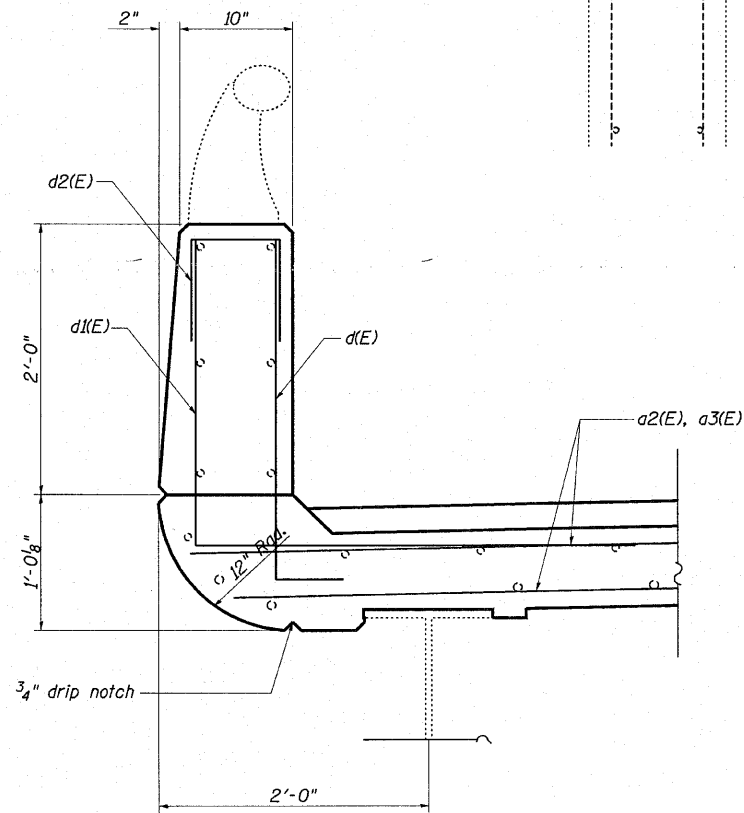


Hatched area indicates removal.

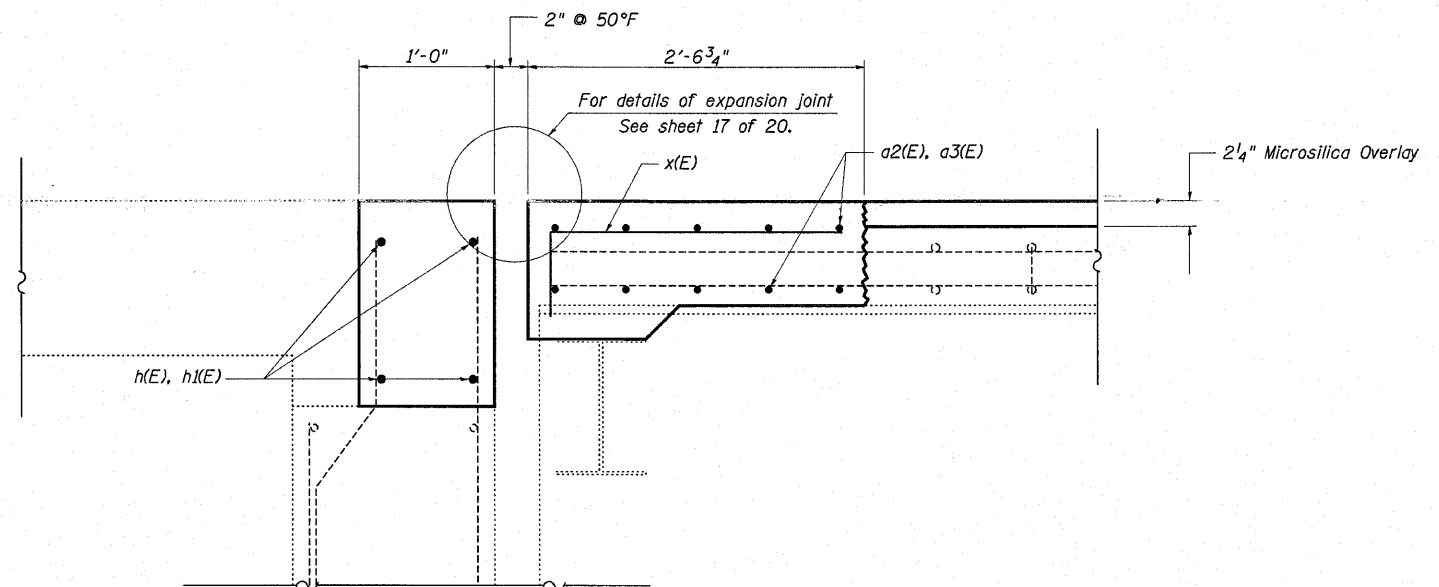


**SECTION A-A**  
(Dimensions at RT L's to end of deck)

○ Existing Reinforcement  
● Proposed Reinforcement



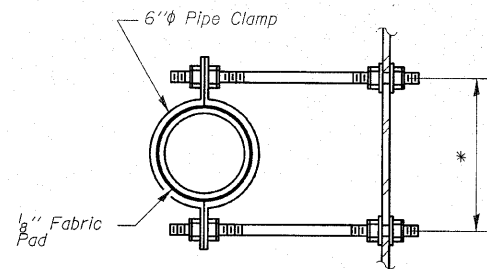
**SECTION THRU PARAPET**



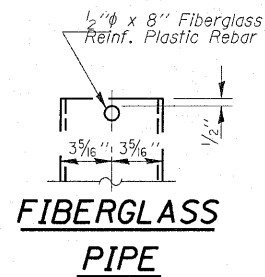
**SECTION B-B**  
(Dimensions at RT L's to end of deck)

FILE NAME =	USER NAME = biermark1	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EXPANSION JOINT REPLACEMENT DETAILS SN. 025-0069</b>	F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
es:\pw_work\pwwidot\biermark1\02148697\07	417-shit-brdetals.dgn	DRAWN -	REVISED -			57	(13-1)R&(25-6-1)R		44	36	
PLOT SCALE = 20.0000' / IN.		CHECKED -	REVISED -			CONTRACT NO. 14417					
PLOT DATE = 10/21/2010		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					
					SCALE: _____	SHEET NO. 14 OF 20 SHEETS		STA. _____	TO STA. _____		

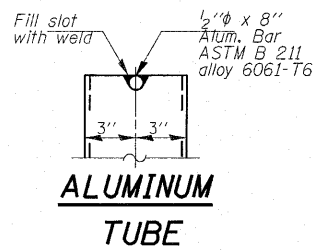
\* Dimension as required by Pipe Clamp



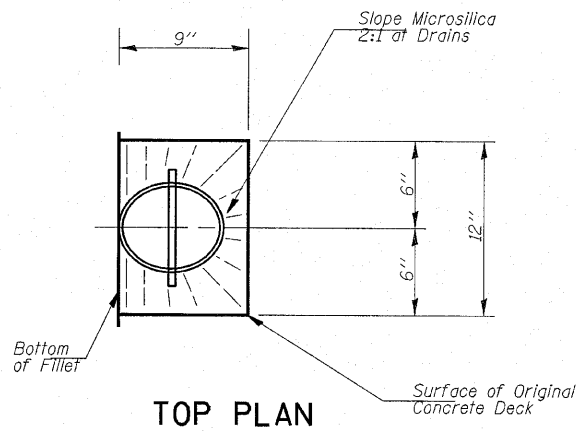
**SECTION A-A**



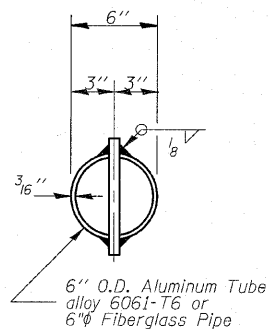
**FIBERGLASS PIPE**



**ALUMINUM TUBE**



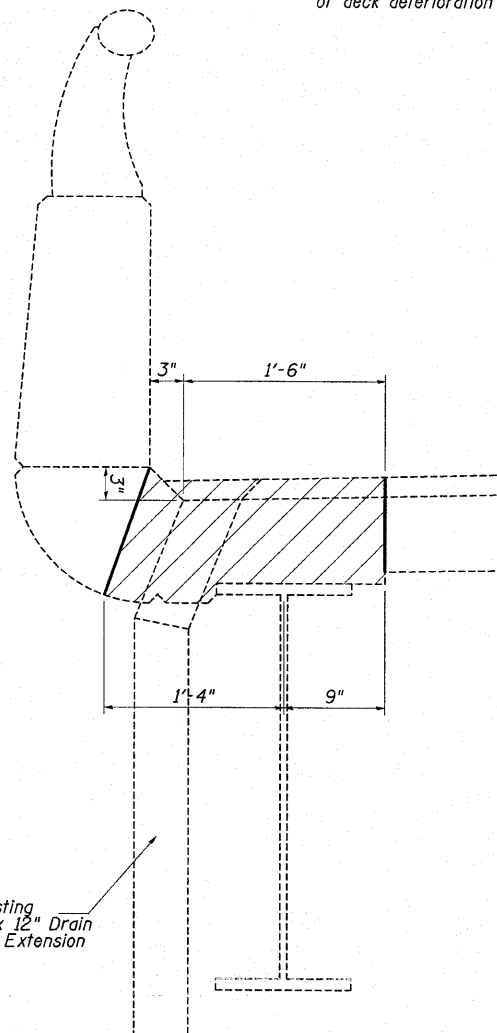
**TOP PLAN**



**TOP PLAN**

(Showing Aluminum Tube)

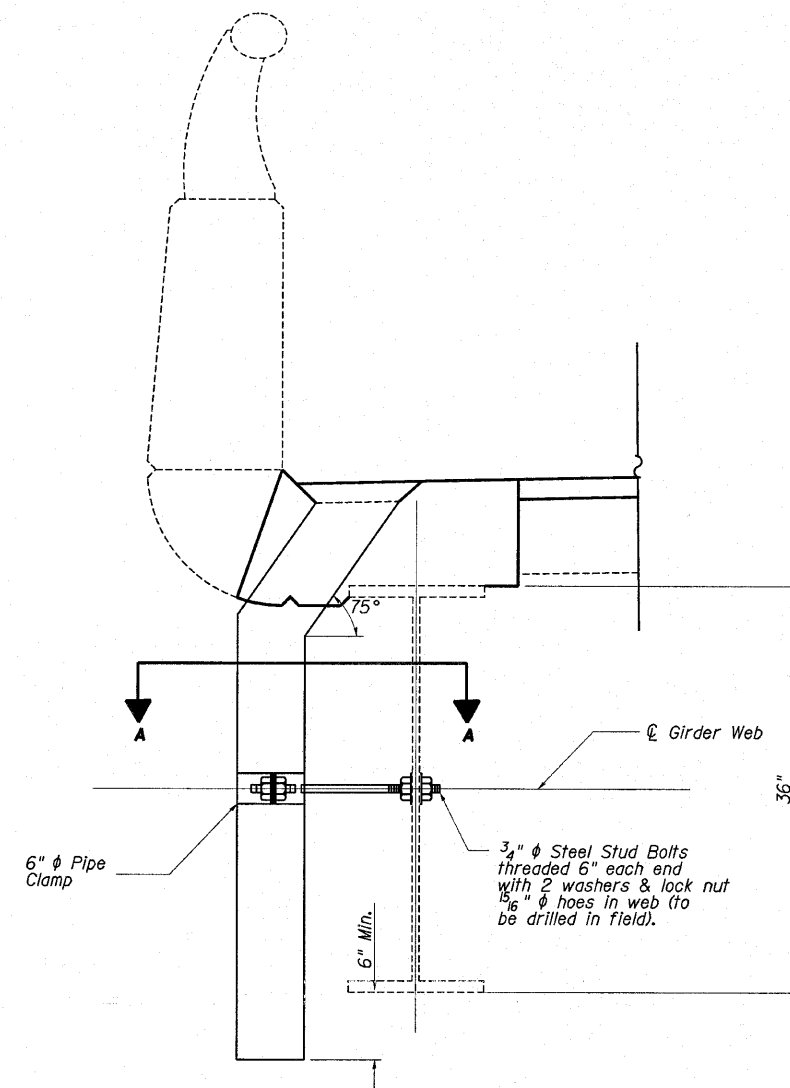
\*\* Concrete removal and replacement dimensions shall be determined by the Engineer based on the extent of deck deterioration at each drain.



**\*\* SECTION THRU EXISTING FLOOR DRAIN**

Hatched area indicates concrete removal at floor drain replacement.

Note: Concrete removal and replacement quantities for drains are included in Deck Slab Repair as shown on "Bridge Deck Patching" plan sheets.



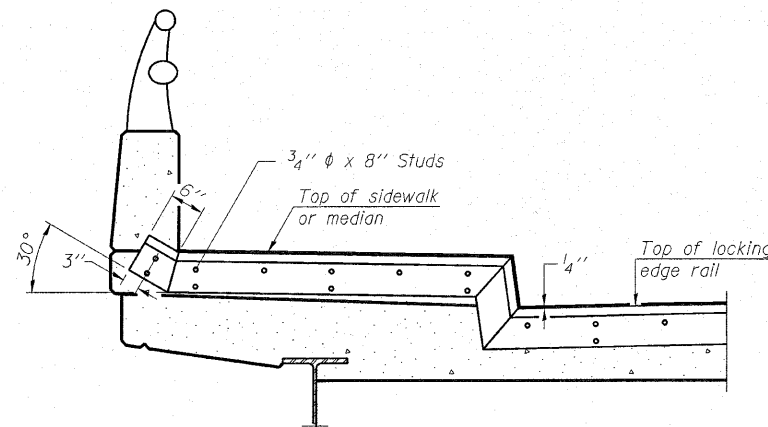
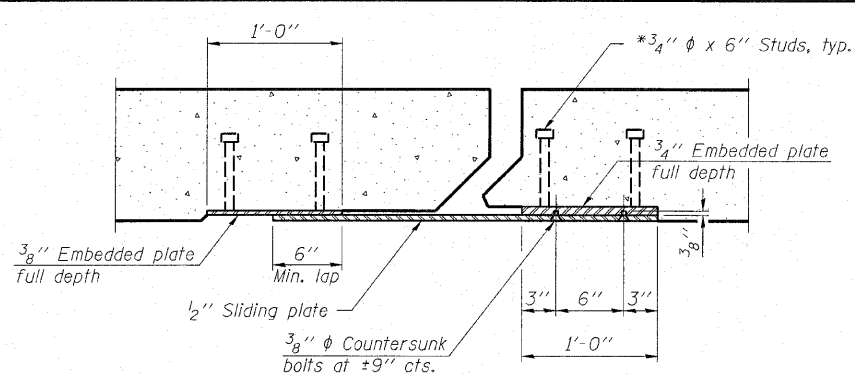
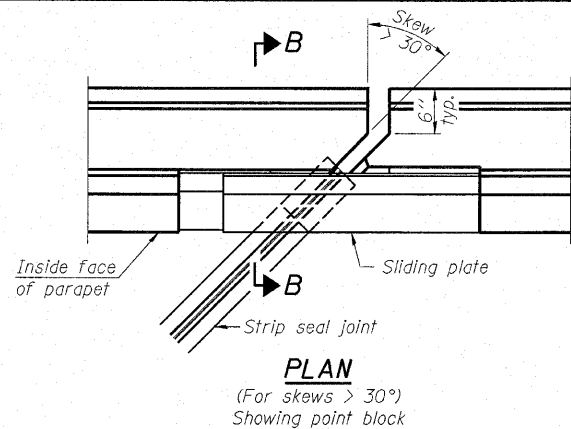
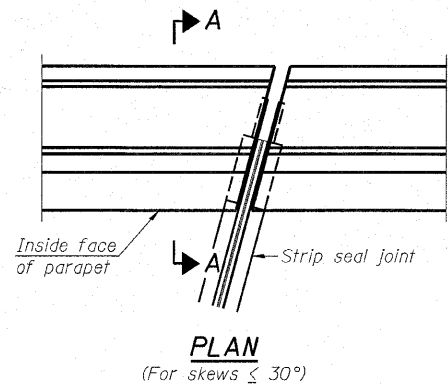
**DRAIN REPLACEMENT DETAIL**

Note: See "Bridge Deck Patching" plan sheets for Floor Drain replacement location and Plug Existing Deck Drain locations. See "Total Bill of Materials" for each structure for quantities.

Notes: The floor drains shall be painted to match the outside of the fascia beams. Fiberglass pipe shall conform to ASTM D 2996, with short-time rupture strength hoop tensile stress of 30,000 p.s.i. minimum. Galvanize clamping device according to AASHTO M232. Cost of clamping device and galvanizing is included with Floor Drains.

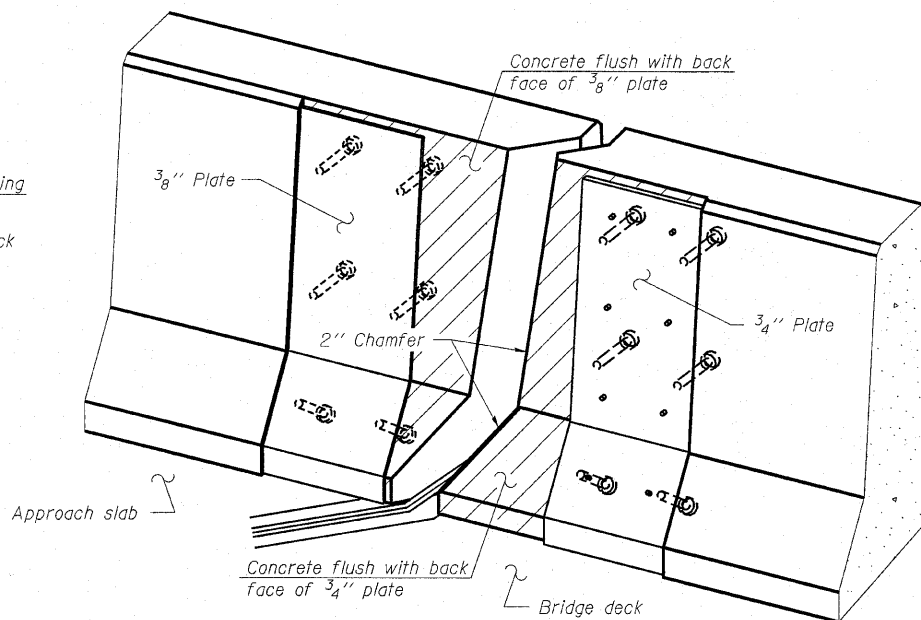
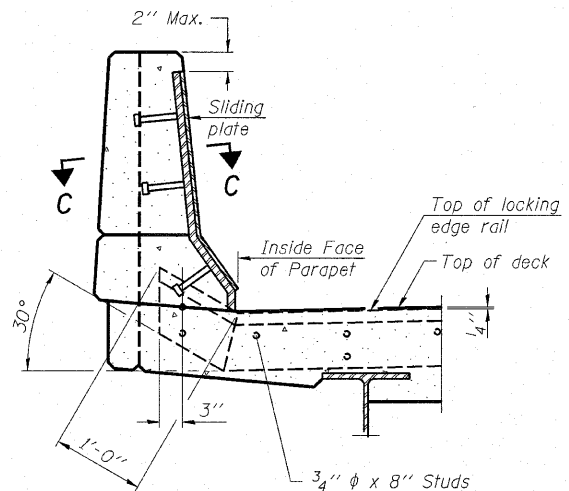
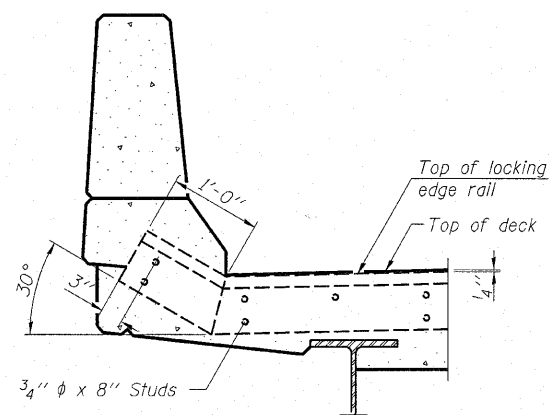
FILE NAME =	USER NAME = bsermarkl	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>FLOOR DRAIN DETAILS SN. 025-0069</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ar\pv_work\pwwdot\bsermarkl\d0148697\07	417-shit-brdetails.dgn	DRAWN -	REVISED -			57	(13-1R&125-6-1R)		44	37
PLOT SCALE = 20.0000' / IN.		CHECKED -	REVISED -			CONTRACT NO. <b>T4417</b>				
PLOT DATE = 10/21/2010		DATE -	REVISED -			ILLINOIS FED. AID PROJECT				
					SCALE: _____	SHEET NO. 15 OF 20 SHEETS		STA. _____ TO STA. _____		Clay, Effingham





**TYPICAL END TREATMENT AT SIDEWALK OR MEDIAN**

Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.



**Notes:**

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities.

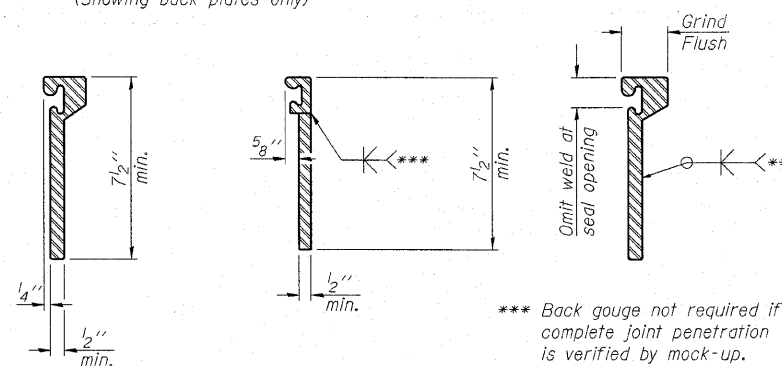
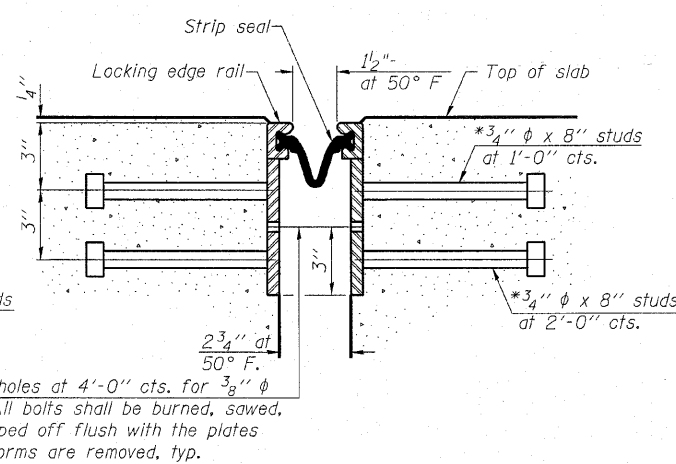
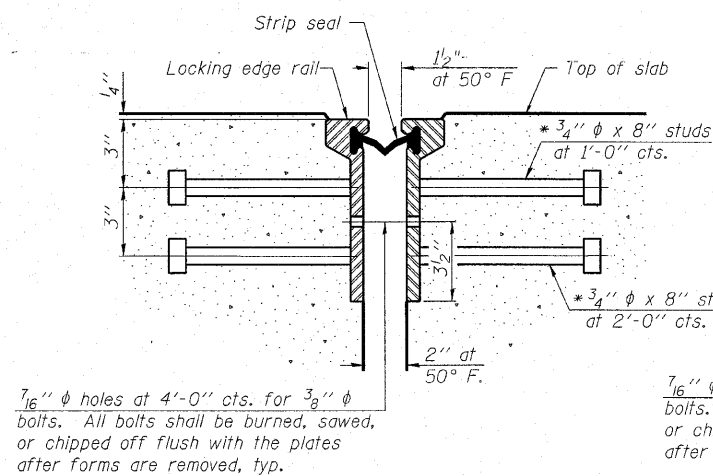
The manufacturer's recommended installation methods shall be followed.

The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

Maximum space between rail segments at stage lines shall be 3/16", sealed with a suitable sealant.

Parapet plates and anchorage studs for skews  $> 30^\circ$  included in the cost of Preformed Joint Strip Seal.



**LOCKING EDGE RAIL SPLICE**

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

**BILL OF MATERIAL**

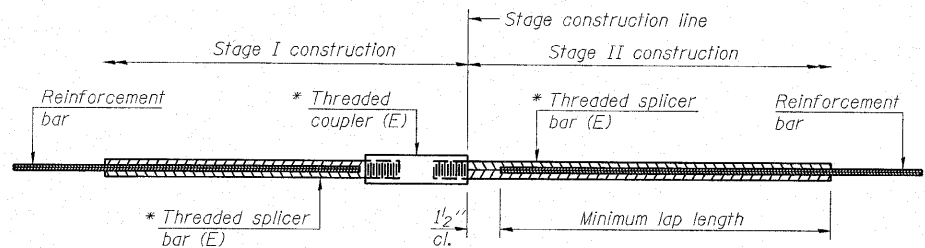
Item	Unit	Total
Preformed Joint Strip Seal	Foot	95

\* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

EJ-SSJ

7-1-10

FILE NAME =	USER NAME = biermankl	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PREFORMED JOINT STRIP SEAL STRUCTURE NO. 025-0069</b>	F.A.I. RTEL	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\paw_work\pawidot\biermankl\d0148697\07	417-sh-t-br-details.dgn	DRAWN -	REVISED -			57	(13-1)R&(25-6-1)R		44	39
PLOT SCALE = 20,0000' / IN.		CHECKED -	REVISED -			CONTRACT NO. 74417				
PLOT DATE = 10/21/2010		DATE -	REVISED -			ILLINOIS FED. AID PROJECT				
SCALE: _____ SHEET NO. 17 OF 20 SHEETS STA. _____ TO STA. _____						ILLINOIS FED. AID PROJECT Clay, Effingham				



**STANDARD BAR SPLICER ASSEMBLY**

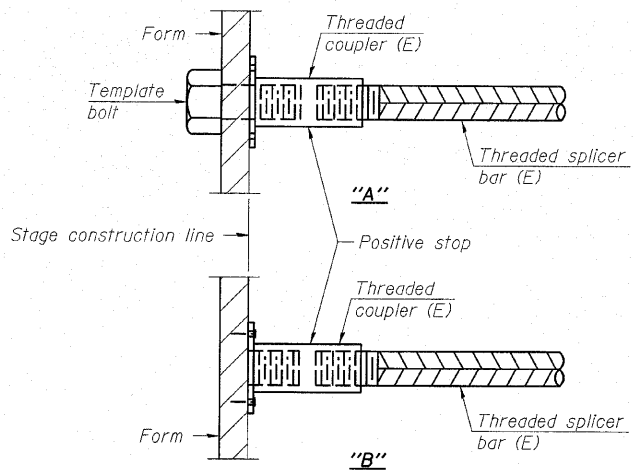
Bar size to be spliced	Minimum Lap Lengths				
	Table 1	Table 2	Table 3	Table 4	Table 5
3, 4	1'-5"	1'-11"	2'-1"	2'-4"	2'-3"
5	1'-9"	2'-5"	2'-7"	2'-11"	2'-10"
6	2'-1"	2'-11"	3'-1"	3'-6"	3'-4"
7	2'-9"	3'-10"	4'-2"	4'-8"	4'-6"
8	3'-8"	5'-1"	5'-5"	6'-2"	5'-10"
9	4'-7"	6'-5"	6'-10"	7'-9"	7'-5"

- Table 1: Black bar, 0.8 Class C
- Table 2: Black bar, Top bar lap, 0.8 Class C
- Table 3: Epoxy bar, 0.8 Class C
- Table 4: Epoxy bar, Top bar lap, 0.8 Class C
- Table 5: Epoxy bar, Top bar lap, Class B

Threaded splicer bar length = min. lap length + 1/2" + thread length

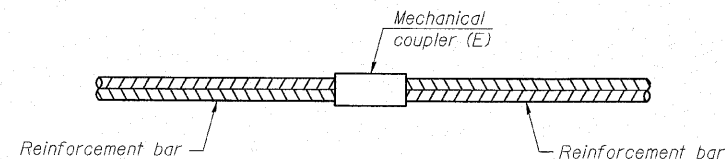
\* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length
025-0069	#5	20	Table 3
025-0069	#6	8	Table 3



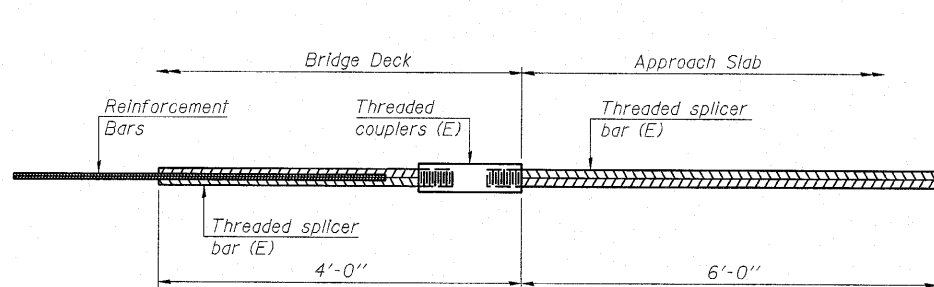
**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.  
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.  
 (E) : Indicates epoxy coating.



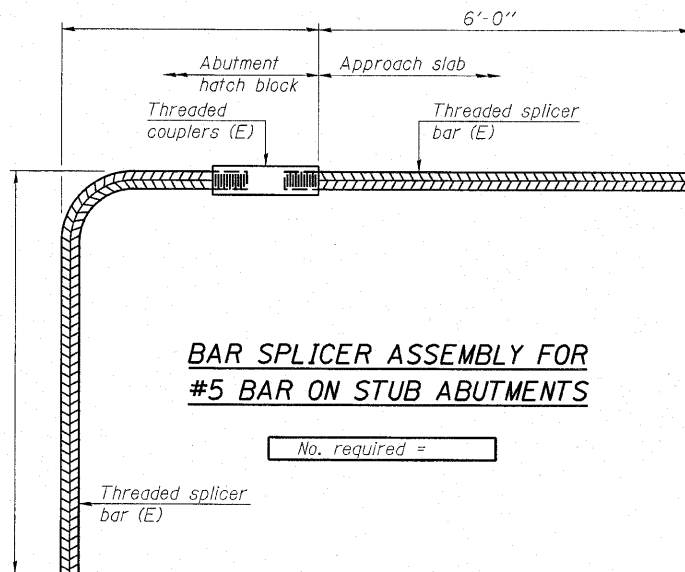
**STANDARD MECHANICAL SPLICER**

Location	Bar size	No. assemblies required



**BAR SPLICER ASSEMBLY FOR #5 BAR ON INTEGRAL OR SEMI-INTEGRAL ABUTMENTS**

No. required =



**BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS**

No. required =

**NOTES**

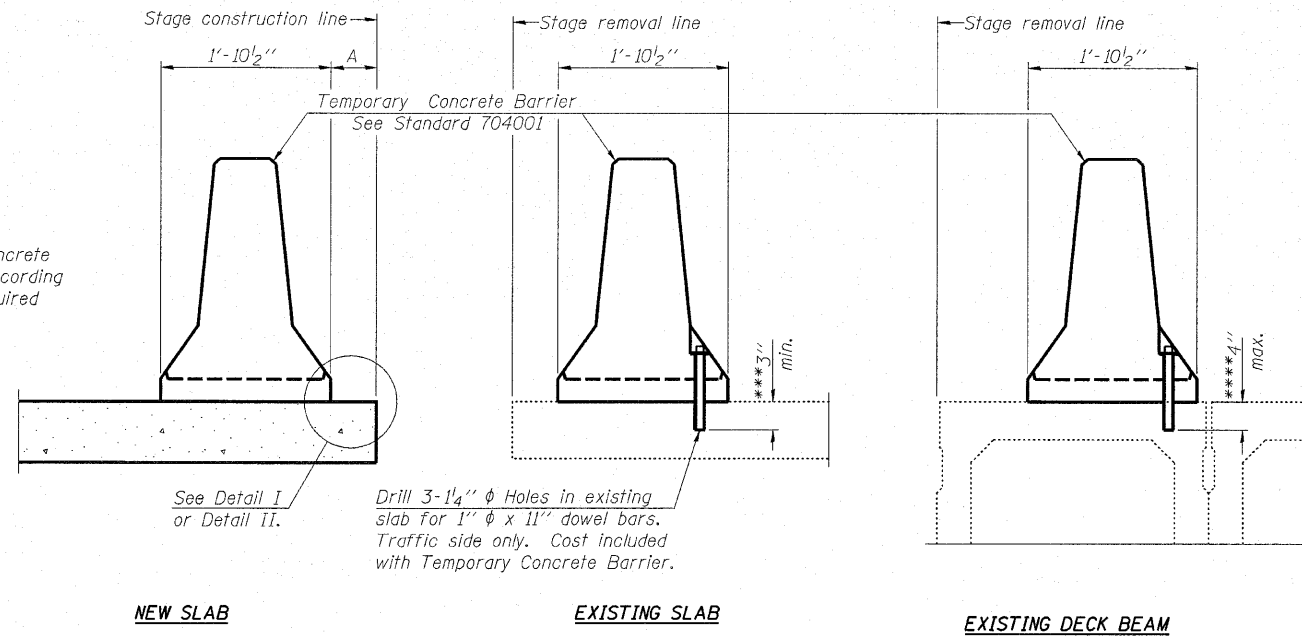
Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.  
 All reinforcement shall be lapped and tied to the splicer bars.  
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.  
 See special provision for Mechanical Splicers.  
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1 7-1-10

FILE NAME =	USER NAME = biermankl	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS STRUCTURE NO. 025-0069</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEET NO.		
ca:\pwwork\pwwork\biermankl\d0148697\07417-shr-brdetails.dgn	417-shr-brdetails.dgn	DRAWN -	REVISED -			57	(13-1)R&(25-6-1)R		44	40	
PLOT SCALE = 20,0000 "/>	PLOT DATE = 10/21/2010	CHECKED -	REVISED -			CONTRACT NO. 74417		ILLINOIS FED. AID PROJECT			
SCALE: _____	SHEET NO. 18 OF 20 SHEETS	DATE -	REVISED -			STA. _____ TO STA. _____					



When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".



Drill 3-1/4"  $\phi$  Holes in existing slab for 1"  $\phi$  x 11" dowel bars. Traffic side only. Cost included with Temporary Concrete Barrier.

**NOTES**

Detail I - With Bar Splicer or Couplers:  
Connect one (1) 1" x 7" x "W" steel  $P_L$  to the top layer of couplers with 2-5/8"  $\phi$  bolts screwed to coupler at approximate  $\phi$  of each barrier panel.

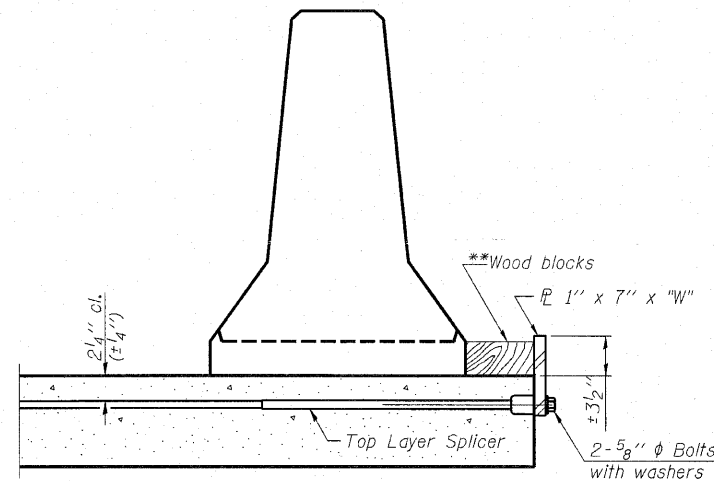
Detail II - With Extended Reinforcement Bars:  
Connect one (1) 1" x 7" x "W" steel  $P_L$  to the concrete slab or concrete wearing surface with 2-5/8"  $\phi$  Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate  $\phi$  of each barrier panel.

Cost of anchorage is included with Temporary Concrete Barrier. The 1" x 7" x "W" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

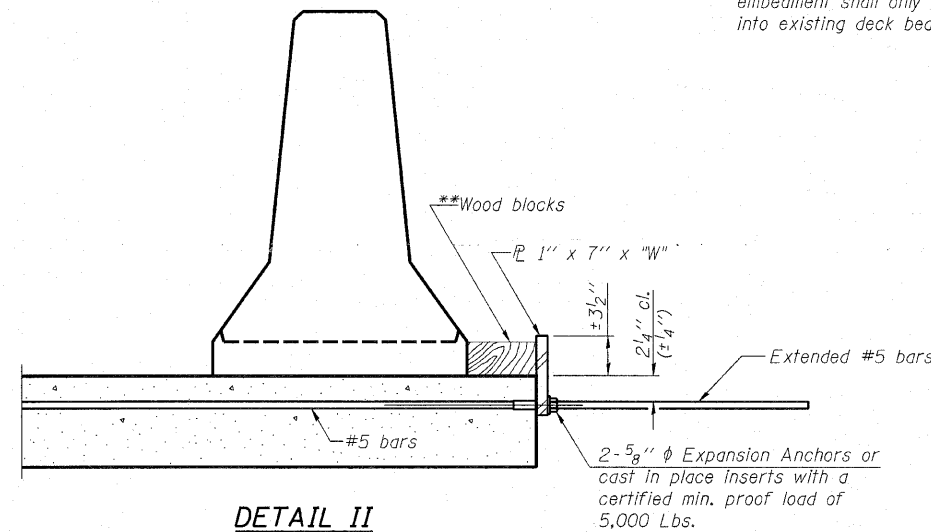
**SECTIONS THRU SLAB OR DECK BEAM**

\*\*\* Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.

\*\*\*\* If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



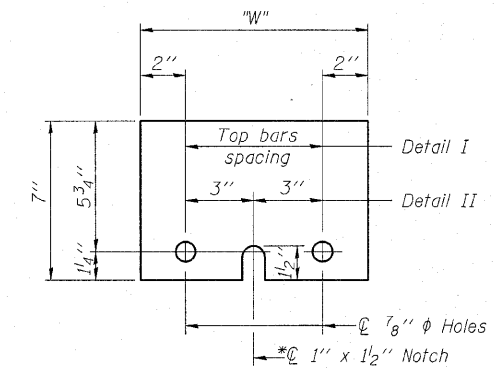
**DETAIL I**



**DETAIL II**

\*\* Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.

"W" = Top bars spacing + 4"



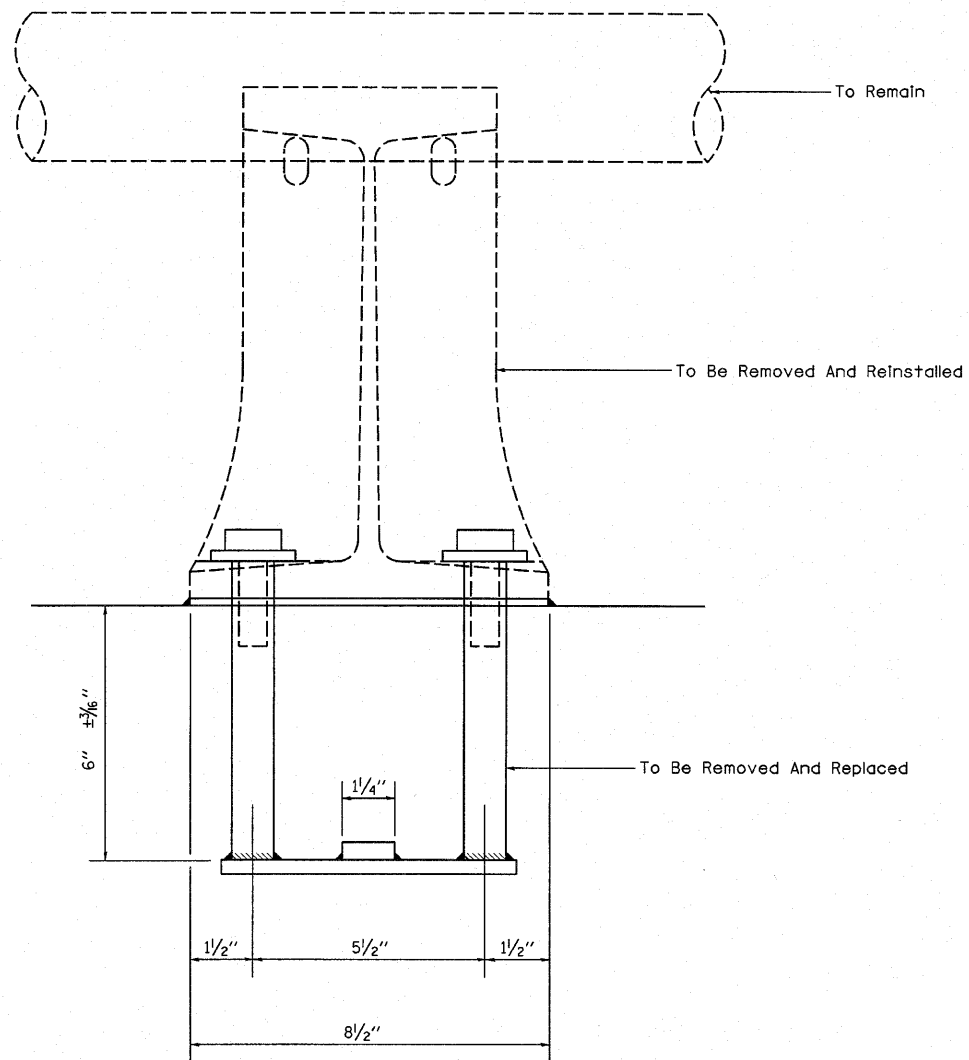
**STEEL RETAINER  $P_L$  1" x 7" x "W"**

\* Required only with Detail II

R-27

7-1-10

FILE NAME =	USER NAME = biermark1	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION STRUCTURE NO. 025-0069</b>	F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pwwork\pwwork\pwwork\pwwork\d0148697\07	417-sht-brdetails.dgn	DRAWN -	REVISED -			57	(13-1)R&(25-6-UR		44	41
PLOT SCALE = 20,0000' / IN.	CHECKED -	REVISED -	REVISED -			CONTRACT NO. 74417				
PLOT DATE = 10/21/2010	DATE -	REVISED -	REVISED -			ILLINOIS FED. AID PROJECT				
SCALE: _____		SHEET NO. 19 OF 20 SHEETS		STA. _____ TO STA. _____		ILLINOIS FED. AID PROJECT				

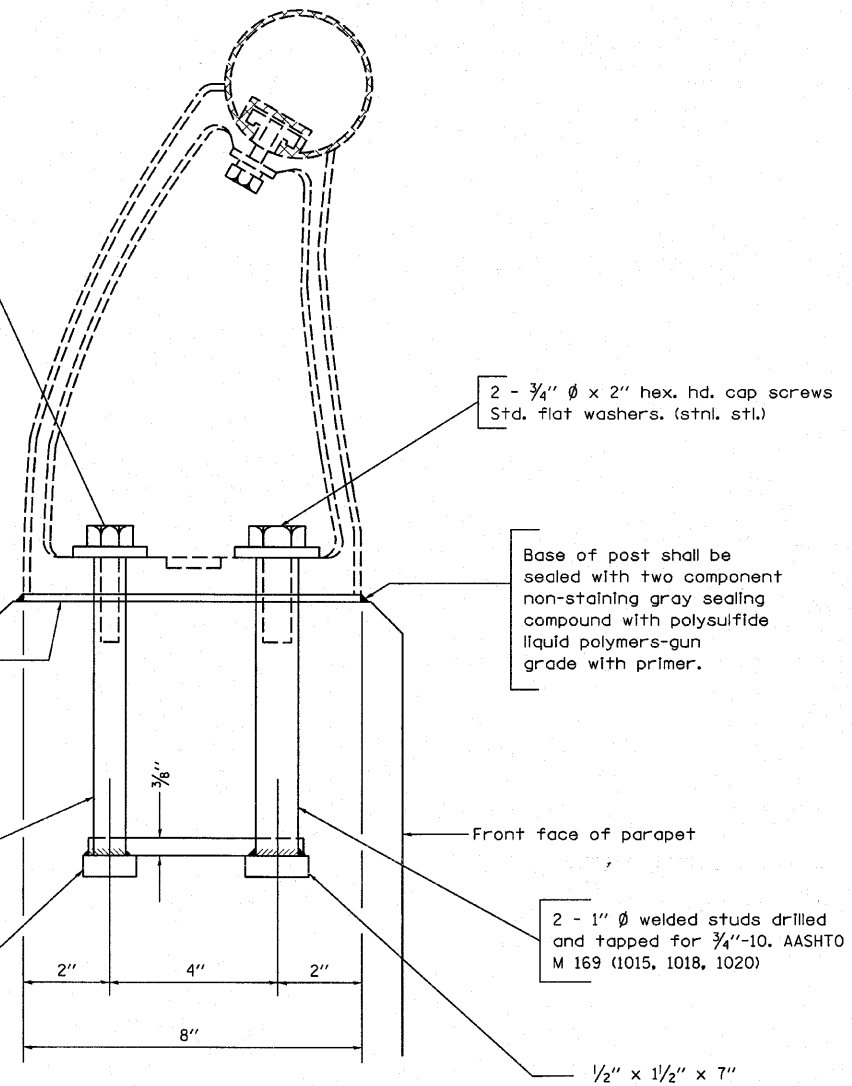


2 -  $\frac{1}{2}'' \text{ } \phi \times 1\frac{3}{4}''$  hex. hd. cap screws  
Std. flat washers. (stnl. stl.)

$\frac{1}{8}''$  Fabric Bearing Pad

2 -  $\frac{3}{4}'' \text{ } \phi$  welded studs drilled  
and tapped for  $\frac{1}{2}''$ -13. AASHTO  
M 169 (1015, 1018, 1020)

$\frac{3}{8}'' \times 1\frac{1}{4}'' \times 7''$



2 -  $\frac{3}{4}'' \text{ } \phi \times 2''$  hex. hd. cap screws  
Std. flat washers. (stnl. stl.)

Base of post shall be  
sealed with two component  
non-staining gray sealing  
compound with polysulfide  
liquid polymers-gun  
grade with primer.

Front face of parapet

2 -  $1'' \text{ } \phi$  welded studs drilled  
and tapped for  $\frac{3}{4}''$ -10. AASHTO  
M 169 (1015, 1018, 1020)

$\frac{1}{2}'' \times 1\frac{1}{2}'' \times 7''$

Note: New Rail Post anchorage devices will be required at each location where posts are connected to new construction. Cost shall be included with Concrete Superstructure.

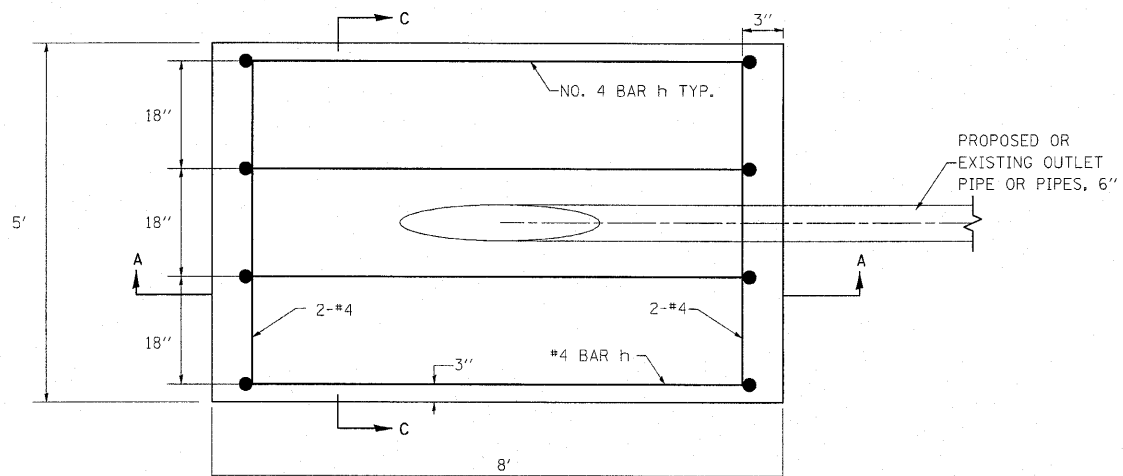
FILE NAME =	USER NAME = biermark1	DESIGNED -	REVISD -
c:\ps_work\pwt\dot\biermark1\08148697\07	417-sht-brdetail.s.dgn	DRAWN -	REVISD -
	PLOT SCALE = 20.0000 "/ IN.	CHECKED -	REVISD -
	PLOT DATE = 10/21/2010	DATE -	REVISD -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

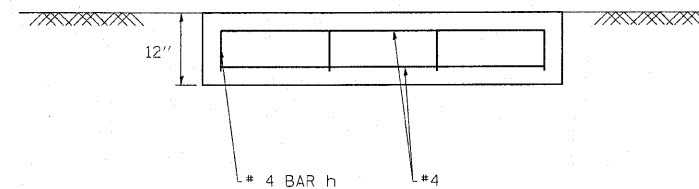
RAIL SUPPORT ANCHOR DETAILS  
SN. 025-0069

SCALE: SHEET NO. 20 OF 20 SHEETS STA. TO STA.

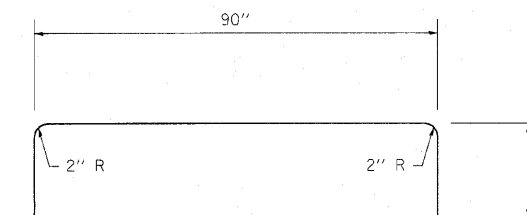
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(13-1)R&(25-6-UB		44	42
CONTRACT NO. 74417			ILLINOIS FED. AID PROJECT	



PLAN VIEW



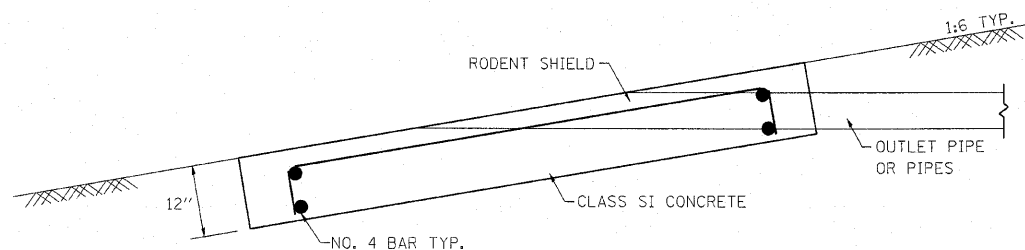
SECTION C-C



#4 h BAR

NOTES

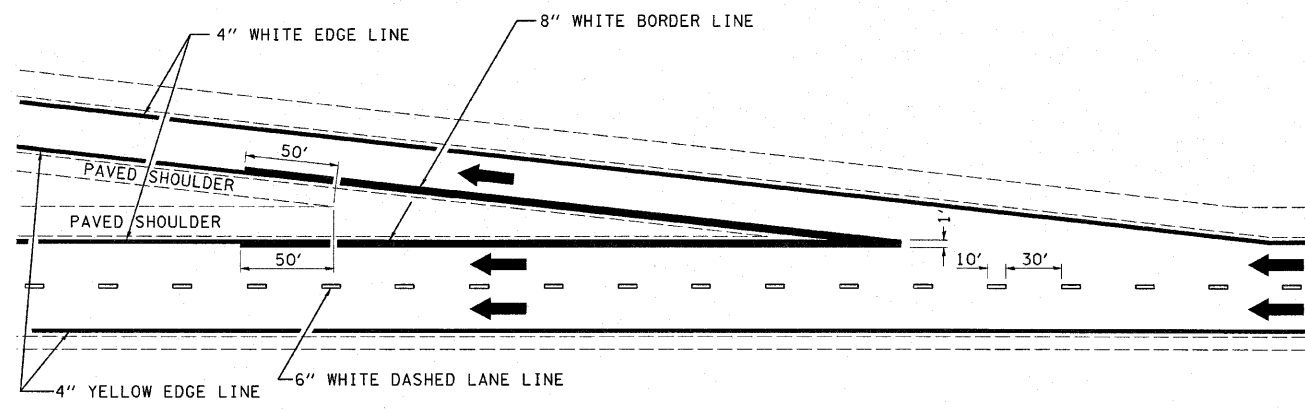
- See Standard 601101 for details of rodent shields.
- The outlet pipe or pipes shall be located as close as possible to the center of the outlet protector.
- The last 10' of outlet pipe shall be schedule 40 PVC.
- The rebars may be cut or relocated to accommodate pipe.
- Cut outlet pipe on a bevel to match finished surface of surrounding PCC.



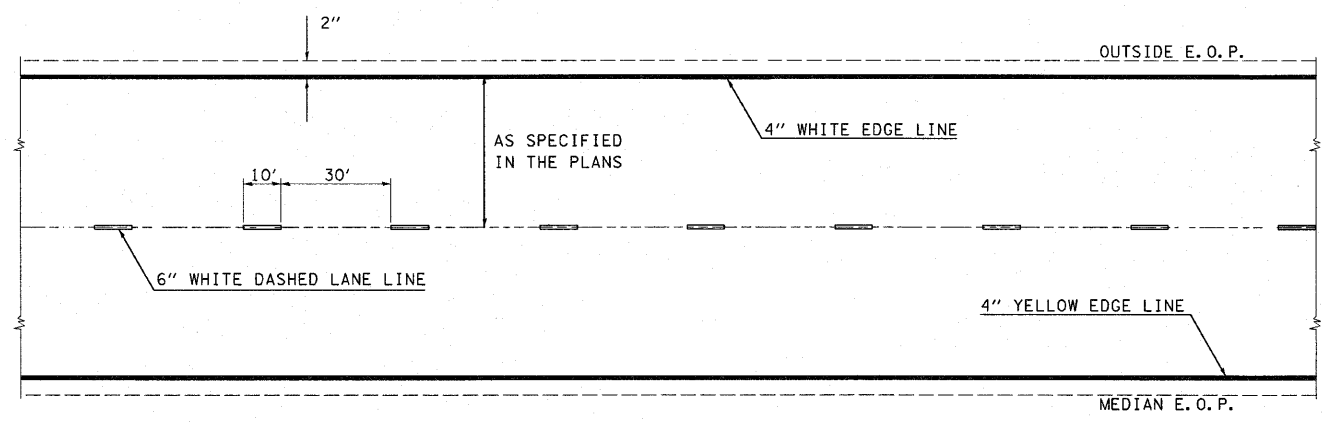
SECTION A-A

APPROXIMATE OUTLET PROTECTOR QUANTITIES FOR EACH OUTLET PROTECTOR	
CONCRETE, CLASS SI	REINFORCING STEEL
CU YD 1.5	LB 35.6

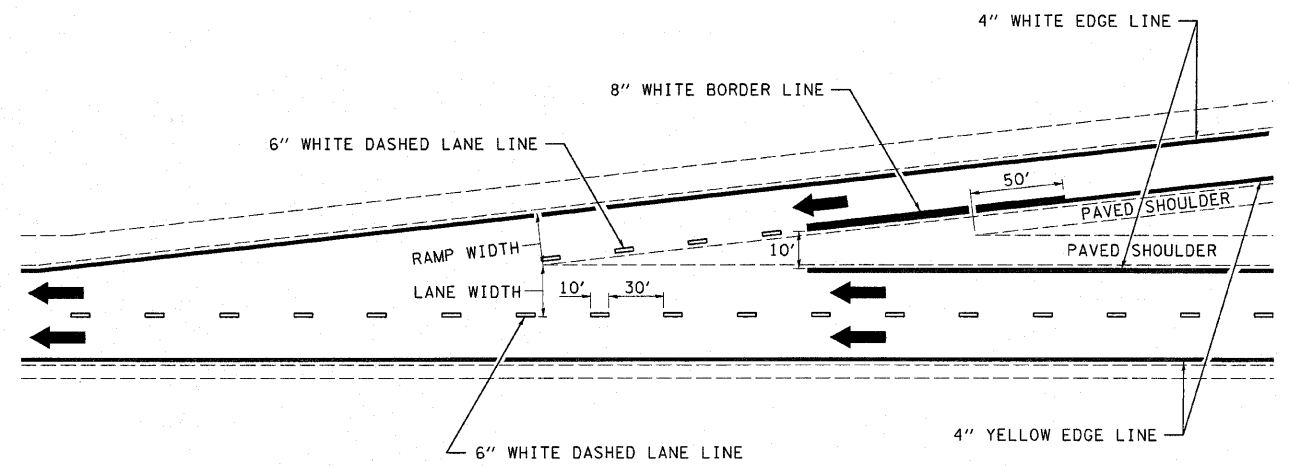
FILE NAME =	USER NAME = teasleyck	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>OUTLET PROTECTOR DETAILS</b>				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwork\pwork\teasleyck\08148697\07	4417-sht-detail.dgn	DRAWN -	REVISED -		57	(13-1)R&(25-6-1)R	•	44	43				
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -		CONTRACT NO. 74417								
	PLOT DATE = 10/21/2010	DATE -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		



TYPICAL EXIT RAMP MARKING



TYPICAL CENTERLINE & EDGELINE MARKINGS



TYPICAL ENTRANCE RAMP MARKING

NOT TO SCALE

FILE NAME =	USER NAME = teasleyck	DESIGNED -	REVISED - MMO 12-99
c:\pwork\pwork\teasleyck\d0148697\074417-shit-detail.dgn		DRAWN -	REVISED - DRM 08-04
		CHECKED -	REVISED - MKS 04-08
		DATE -	REVISED - DRM 01-09

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS OF INTERSTATE PAVEMENT MARKING

SCALE: SHEET NO. 1 OF 2 SHEETS STA. TO STA.

DISTRICT 7 DETAIL NO. 78000002				
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(13-1)R&(25-6-1)R		44	44
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 74417	

Clay, Effingham