ITEM NO. 02A IDOT LETTING: JANUARY 21, 2022

CONSTRUCTION PLANS

RECONSTRUCT WEST PORTION OF RAMP

KEWANEE AIRPORT AUTHORITY KEWANEE MUNICIPAL AIRPORT (EZI) KEWANEE, HENRY COUNTY, ILLINOIS

SBG PROJECT NO. 3-17-SBGP-162/171 IDA PROJECT NO. EZI-4883 NOVEMBER 19, 2021

WORK ON THIS PROJECT SHALL BE COORDINATED AS TO NOT CONFLICT WITH AN EVENT TO BE HELD AT THE AIRPORT OVER LABOR DAY 2022. IT SHOULD BE ANTICIPATED THAT WORK WILL COMMENCE AFTER THE HOLIDAY ON SEPTEMBER 6, 2022.

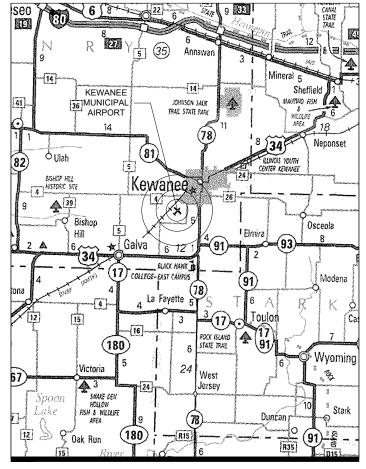
NOTICE TO CONTRACTORS AND BIDDERS

THESE CONSTRUCTION PLANS RELY UPON THE SPECIAL PROVISIONS AND THE SPECIFICATIONS TO PROVIDE FOR A COMPLETE DESCRIPTION OF THE WORK AND CONSTRUCTION REQUIREMENTS. THE PLANS SHALL ONLY BE USED IN COMBINATION WITH ALL CONTRACT DOCUMENTS.

No.	Issue/Description	Sheets Changed	Date	Ву

PLANS PREPARED BY:





VICINITY MAP



KE018 TOTAL SHEETS = 14



Rose Carbieron Rose Carbieron Alport Managor

19 November 2021 Date

	INDEX OF SHEETS			
SHEET NUMBER	SHEET TITLE			
1	COVER			
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4	SITE AND SAFETY PLAN NOTES			
5	PHASING PLAN			
6	ALIGNMENT DATA TABLE AND PAVEMENT LAYOUT			
7	STORMWATER POLLUTION PREVENTION PLAN (SWPP)			
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12	GRADING AND LANDSCAPING PLAN			
13	TIE DOWN AND MARKING DETAILS			
14	DRAINAGE DETAILS			

SUMMARY OF QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
AR150510	ENGINEER'S FIELD OFFICE	L SUM	1.00
AR150520	MOBILIZATION	L SUM	1.00
AR150530	TRAFFIC MAINTENANCE	L SUM	1.00
AR152410	UNCLASSIFIED EXCAVATION	CU YD	774.00
AR152540	SOIL STABILIZATION FABRIC	SQ YD	6,236.00
AR154606	GRANULAR DRAINAGE SUBBASE - 6"	SQ YD	6,236.00
AR156510	SILT FENCE	FOOT	421.00
AR156520	INLET PROTECTION	EACH	2.00
AR201661	CLEAN & SEAL BITUMINOUS CRACKS	FOOT	1,200.00
AR209604	CRUSHED AGG. BASE COURSE - 4"	SQ YD	6,236.00
AR401613	BIT. SURF. CSEMETHOD I, SUPERPAVE	TON	744.00
AR401660	SAW & SEAL BIT. JOINTS	FOOT	506.00
AR401900	REMOVE BITUMINOUS PAVEMENT	SQ YD	6,157.00
AR403613	BIT. BASE CSEMETHOD I, SUPERPAVE	TON	744.00
AR510510	TIE DOWN	EACH	16.00
AR510900	REMOVE TIE DOWN	EACH	16.00
AR602510	BITUMINOUS PRIME COAT	GALLON	1,852.00
AR603510	BITUMINOUS TACK COAT	GALLON	433.00
AR620520	PAVEMENT MARKING- WATERBORNE	SQ FT	1,695.00
AR620525	PAVEMENT MARKING- BLACK BORDER	SQ FT	1,695.00
AR705506	6" PERFORATED UNDERDRAIN	FOOT	400.00
AR705630	UNDERDRAIN INSPECTION HOLE	EACH	1.00
AR705640	UNDERDRAIN CLEANOUT	EACH	1.00
AR901510	SEEDING	ACRE	0.30
AR905520	TOPSOILING (FROM OFF-SITE)	CU YD	152.00
AR908510	MULCHING	ACRE	0.30

GENERAL NOTES

PROJECT DESCRIPTION

THIS PROJECT IS TO RECONSTRUCT THE WEST PORTION OF THE RAMP AT KEWANEE MUNICIPAL AIRPORT INCLUDING; PLACEMENT OF TEMPORARY SOIL EROSION CONTROL MEASURES, FULL-DEPTH REMOVAL OF EXISTING PAVEMENTS, CRACK REPAIR OF EXISTING PAVEMENTS TO REMAIN, INSTALLATION OF SEPARATION FABRIC, PLACEMENT OF AGGREGATE AND BITUMINOUS PAVEMENT, PLACEMENT OF PAVEMENT MARKINGS, TOPSOILING, SEEDING AND MULCHING IN ALL DISTURBED AREAS, INCLUDING ALONG NEW PAVEMENT EDGES.

PROTECTION OF EXISTING AIRPORT FACILITIES

THE CONTRACTOR IS TO BE RESPONSIBLE FOR THE PROTECTION OF EXISTING UNDERGROUND AND OVERHEAD UTILITIES AND LIGHTING EQUIPMENT; DRIVEWAY AND ROAD PAVEMENT AND SHOULDERS; RUNWAY, TAXIWAY AND APRON PAVEMENTS AND SHOULDERS; RUNWAY, TAXIWAY AND AIRPORT LIGHTING EQUIPMENT; AND SEEDED AND TURFED AREAS THAT ARE UTILIZED IN OR AFFECTED BY THE CONTRACTOR'S ACTIVITIES. ITEMS DAMAGED BY THE CONTRACTOR ARE TO BE REPAIRED AT CONTRACTOR'S EXPENSE AND TO THE SATISFACTION OF AIRPORT MANAGER AND THE OWNER'S REPRESENTATIVE

IN ADDITION, WHEN CONDITIONS DICTATE OR AS DETERMINED BY THE AIRPORT MANAGER OR THE OWNER'S REPRESENTATIVE, THE CONTRACTOR SHALL BE REQUIRED TO USE A PICK-UP TYPE SWEEPER IN ALL ACTIVE CONSTRUCTION AIRFIELD PAVEMENT AREAS. THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. THE COST OF SWEEPING SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

CONTRACTOR'S ACCESS AND TEMPORARY FACILITIES

CONTRACTOR'S ACCESS TO THE PROJECT WHEN ON AIRPORT PROPERTY IS SHOWN ON SHEET 3. CONTRACTOR'S ACCESS TO THE AIRPORT ITSELF IS TO BE PROVIDED BY PUBLIC RIGHTS-OF-WAY. THE CONTRACTOR IS TO SECURE ALL NECESSARY PERMITS FOR THE USE OF ANY PUBLIC RIGHTS-OF-WAY AND IS TO MAINTAIN TRAFFIC ON THESE PUBLIC ROADS AT ALL TIMES, WITH THE COSTS OF PERMITTING, CLEANING AND REPAIRING OF PAVEMENT DAMAGED BY CONTRACTOR'S ACTIVITIES INCIDENTAL TO THE CONTRACT. USE OF AND REPAIRS TO ANY PUBLIC FACILITIES ARE TO BE COMPLETED TO THE SATISFACTION OF THE FACILITY'S OWNER

THE CONTRACTOR IS TO PROVIDE TEMPORARY CONSTRUCTION ROADS WITHIN THE CONSTRUCTION LIMIT LINES AS MAY BE REQUIRED BY HIS ACTIVITIES. HEAVY VEHICLES SHALL NOT CROSS EXISTING PAVEMENT SURFACES EXCEPT AS APPROVED BY THE AIRPORT MANAGER AND THE OWNER'S REPRESENTATIVE. ANY DAMAGE TO PAVEMENTS THAT MAY OCCUR BY THE CONTRACTOR'S ACTIVITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE AND TO THE SATISFACTION OF THE AIRPORT MANAGER AND THE OWNER'S REPRESENTATIVE. FOR HAUL ROUTES MADE BY CONTRACTOR THROUGH GRASSED AREAS. CONTRACTOR SHALL GRADE, LEVEL, TOPSOIL, SEED AND MULCH AT THE END OF THE PROJECT, COST INCIDENTAL TO THE CONTRACT

THE CONTRACTOR IS TO PROVIDE AN EQUIPMENT STORAGE AND PARKING AREA AT THE LOCATIONS SHOWN ON SHEET 3. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE ACCESS ROADS AND THE STORAGE AREA DURING CONSTRUCTION AND TO RESTORE THE AREAS AT PROJECT COMPLETION TO CONDITIONS SUITABLE TO THE AIRPORT MANAGER AND THE OWNER'S REPRESENTATIVE. AT THE AIRPORT MANAGER'S DISCRETION, THE TEMPORARY FACILITIES MAY REMAIN, BUT THEY MUST BE LEFT IN CONDITIONS SUITABLE TO THE AIRPORT MANAGER. THE COST OF PROVIDING, MAINTAINING AND RESTORING THE TEMPORARY FACILITIES IS INCIDENTAL TO THE CONTRACT.

RESPONSIBILITY FOR EXISTING UTILITIES

THE LOCATION, SIZE AND/OR TYPE OF MATERIAL OF EXISTING UNDERGROUND OR OVERHEAD UTILITIES AS MAY BE INDICATED ON THESE CONSTRUCTION PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE PROJECT ENGINEER HAVE INDEPENDENTLY VERIFIED THIS INFORMATION AND NEITHER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY. SUFFICIENCY OR COMPLETENESS OF THE INFORMATION AND GIVE NO EXPRESSED OR IMPLIED GUARANTEE THAT ANY CONDITIONS INDICATED ARE REPRESENTATIVE OF ACTUAL CONDITIONS TO BE ENCOUNTERED

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL NOTIFY ALL UTILITY COMPANIES AND AGENCIES OF HIS CONSTRUCTION PLANS AND SHALL OBTAIN FROM EACH PARTY DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF ALL UTILITIES AND THE WORKING SCHEDULE OF ANY REMOVALS OR ADJUSTMENTS REQUIRED OF THE UTILITY. THE CONTRACTOR SHALL CONTACT J.U.L.I.E. (PHONE 800-892-0123) TO ASSIST IN THE ABOVE.

THE CONTRACTOR SHALL PROTECT ANY FACILITIES TO THE SATISFACTION OF THE UTILITY OR OWNING-AGENCY WITH THE COST OF ANY REQUIRED PROTECTION TO BE INCIDENTAL TO THE CONTRACT. IN THE EVENT A UTILITY LINE OR SERVICE IS UNEXPECTEDLY ENCOUNTERED DURING CONSTRUCTION. THE CONTRACTOR SHALL IMMEDIATELY NOTICY THE OWNER'S REPRESENTATIVE AND THE UTILITY COMPANY OR AGENCY OF JURISDICTION, ANY SUCH UTILITIES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED TO SERVICE AT ONCE.







PROJECT IS LOCATED IN NORTHWEST 1/4 OF SECTION 17, WETHERSFIELD TOWNSHIP, HENRY COUNTY



Offices Nationwide www.hanson-inc.com

Hanson Professional Services Inc. 1525 S. 6th Street Springfield, IL 62568 phone: 217-788-2450 fax: 217-788-2503

Illinois Licensed Professional Service Corporation #184-001084



KEWANEE MUNICIPAL AIRPORT 3769 Midland Road Kewanee. Illinois 61443 Telephone: 309.853.8222

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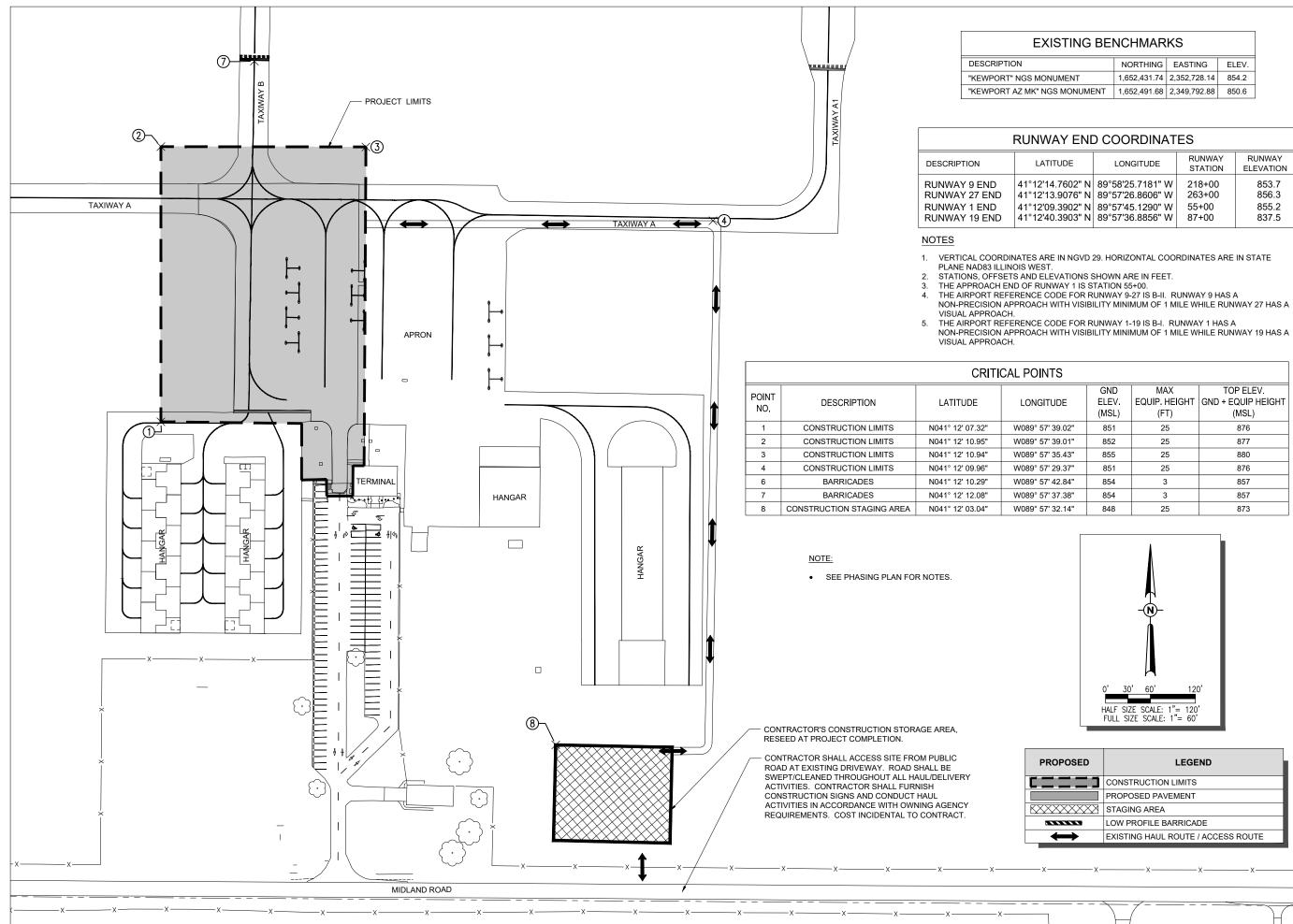
IDA No: EZI-4883

SBGP No: 3-17-SBGP-162/171

Contract No.: KE018

NO.	DATE	DESCRIPTI		ION
NO.	DATE	LAY	DWN	REV
ISSUE:	NOVEM	BER 1	9, 202	1
PROJEC	CT NO: 2	1A000	8	
CAD FIL	E: C-002-S	OQ.DWG		
LAYOUT	BY: LD	H 07/2	26/202	1
DRAWN	BY: HLE	E 07/2	6/202	1
REVIEW	ED BY:	LDH	11/17	/21
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SHEET INDEX AND SUMMARY OF QUANTITIES



02 NO 12

	NORTHING	EASTING	ELEV.	
1ENT	1,652,431.74	2,352,728.14	854.2	
MONUMENT	1,652,491.68	2,349,792.88	850.6	

DE	LONGITUDE	RUNWAY STATION	RUNWAY ELEVATION
076" N 902" N	89°58'25.7181" W 89°57'26.8606" W 89°57'45.1290" W 89°57'36.8856" W	218+00 263+00 55+00 87+00	853.7 856.3 855.2 837.5

	GND	MAX	TOP ELEV.
E	ELEV.	EQUIP. HEIGHT	GND + EQUIP HEIGHT
	(MSL)	(FT)	(MSL)
02"	851	25	876
01"	852	25	877
43"	855	25	880
37"	851	25	876
84"	854	3	857
38"	854	3	857
14"	848	25	873

OSED	LEGEND		
	CONSTRUCTION LIMITS		
	PROPOSED PAVEMENT		
	STAGING AREA		
	LOW PROFILE BARRICADE		
→	EXISTING HAUL ROUTE / ACCESS ROUTE		
x-	xxx		



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IDA No: EZI-4883

SBGP No: 3-17-SBGP-162/171

Contract No.: KE018



SITE AND SAFETY PLAN

CONSTRUCTION AND SAFETY NOTES

SAFETY IS REQUIRED

CONSTRUCTION OF THE PROJECT SHALL BE PERFORMED BY THE CONTRACTOR IN ACCORDANCE WITH THE GUIDELINES SPECIFIED IN FAA ADVISORY CIRCULAR 150/5370-2 (CURRENT ISSUE) AND THE AIRPORT RULES AND REGULATIONS. ANY CONTRACTOR ACTIVITIES REQUIRED FOR PROJECT SAFETY SHALL BE PROVIDED BY THE CONTRACTOR AND BE INCIDENTAL TO THE CONTRACT.

SEQUENCE OF CONSTRUCTION

TO MINIMIZE DISRUPTIONS OF AIRPORT OPERATIONS, CONSTRUCTION OPERATIONS MUST BE CONTROLLED THROUGHOUT THE PROJECT'S DURATION, AND WORK MUST BE COMPLETED EXPEDITIOUSLY. A CONSTRUCTION PHASING PLAN DETAILING THE SEQUENCING OF THE CONTRACTOR'S WORK THROUGHOUT THE PROJECT IS INCLUDED IN THE PLANS. THE CONTRACTOR SHALL PROVIDE HIS WRITTEN ACCEPTANCE OF THE PROJECT CONSTRUCTION PHASING PLAN AT THE PRE-CONSTRUCTION CONFERENCE. ANY AND ALL CHANGES TO THE CONSTRUCTION PHASING PLAN THAT MAY BE REQUESTED BY THE CONTRACTOR MUST BE APPROVED BY THE PROJECT ENGINEER AND THE AIRPORT OWNER. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE SUFFICIENT ADVANCE NOTICE OF ANY PROPOSED PHASING CHANGE TO PERMIT CONSIDERATION AND APPROVAL BY THE PROJECT ENGINEER AND THE AIRPORT OWNER. THE CONTRACTOR SHALL NOT BE ENTITLED TO ANY EXTRA COMPENSATION, NOR EXTENSION TO THE CONTRACT TIME, BECAUSE OF A PHASING CHANGE REQUEST NOR FOR ANY TIME NECESSARY IN RECEIVING THE REQUIRED APPROVALS. THE CONTRACTOR SHALL EXPEDITE WORK AT THOSE STAGES WHERE ACTIVE TAXIWAYS, HANGAR ACCESS, APRONS, ROADWAYS OR PARKING LOTS MUST BE CLOSED, TO MINIMIZE THE LENGTH OF TIME THAT AIRPORT OPERATIONS ARE RESTRICTED

AT THE PRE-CONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL PROVIDE A CONTRACTOR COORDINATION PLAN THAT COORDINATES HIS WORK WITH THE WORK OF HIS SUBCONTRACTORS AND THE WORK OF OTHER CONTRACTORS OF OTHER ON-GOING AIRPORT PROJECTS.

RUNWAY CLOSURE

NO RUNWAY CLOSURES WILL BE PERMITTED AS PART OF THIS PROJECT.

TEMPORARY BARRICADES

THE CONTRACTOR SHALL FURNISH BARRICADES FOR ANY AIRFIELD OR ROADWAY PAVEMENT TO BE CLOSED BY HIS WORK. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO FURNISH. PLACE AND MAINTAIN BARRICADES AS SHOWN IN DETAIL B. THIS SHEET, AND AS DIRECTED BY THE RESIDENT ENGINEER AND ARPORT DIRECTOR. THE COST OF THESE ITEMS, AND THEIR MAINTENANCE, IS TO BE PAID FOR UNDER AR150530 TRAFFIC MAINTENANCE. ANY WORK THAT REQUIRES PORTIONS OF AN ACTIVE RUNWAY, TAXIWAY OR APRON TO BE CLOSED MUST BE COMPLETED EXPEDITIOUSLY TO MINIMIZE DISRUPTION TO AIRCRAFT OPERATIONS.

VEHICULAR TRAFFIC CONTROL

THE CONTRACTOR SHALL ERECT AND MAINTAIN, AT NO COST TO THE CONTRACT, DIRECTIONAL AND INFORMATIONAL SIGNS FOR THE CONTRACTOR'S ACCESS ROUTES AT THE EXISTING CONSTRUCTION ENTRANCES AND FOR THE CONTRACTOR'S ROUTE WITHIN THE AIRPORT OPERATIONS AREA. AS NOTED ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. WHERE CONTRACTOR EQUIPMENT IS OPERATING WITHIN ACTIVE AIRCRAFT OPERATIONS AREAS, RADIO-EQUIPED FLAGGERS SHALL BE FURNISHED BY THE CONTRACTOR. CONTINUOUS PAVEMENT SWEEPING SHALL BE FURNISHED TO REMOVE DEBRIS FROM ACTIVE AIRCRAFT MOVEMENT PATHS. THE COST OF TRAFFIC CONTROL/FLAGGERS AND PAVEMENT SWEEPING SHALL BE PAID UNDER ITEM AR150530 TRAFFIC MAINTENANCE.

AIRFIELD OPERATIONAL SAFETY DURING CONSTRUCTION

THE CONTRACTOR SHALL NOT HAVE ACCESS TO ANY PART OF THE ACTIVE AIRFIELD (RUNWAYS, TAXIWAYS OR APRONS) FOR ANY EQUIPMENT OR PERSONNEL WITHOUT THE APPROVAL OF THE RESIDENT ENGINEER AND THE AIRPORT OWNER. ACTIVITIES WITHIN THE AIRPORT OPERATIONS AREA (AOA) ARE SUBJECT TO FEDERAL ACCESS CONTROL. BECAUSE OF THE HIGH REQUIREMENTS FOR AIRPORT SECURITY AND SAFETY, THE FOLLOWING REQUIREMENTS MUST BE ADHERED

- ALL EMPLOYEES OF THE CONTRACTOR SHALL PARK THEIR PERSONAL VEHICLES IN THE DESIGNATED FOUIPMENT PARKING AND STORAGE AREA. FACH . PERSON OR VEHICLE ENTERING THE CONTRACTOR AREA SHALL DO SO IN ACCORDANCE WITH THE POLICIES AND PROCEDURES OF THE AIRPORT OWNER. THE CONTRACTOR WILL TRANSPORT THE WORKERS FROM THE PARKING AREAS TO THE WORK AREA. ONLY CONTRACTOR VEHICLES WILL BE ALLOWED OUTSIDE OF THE PROPOSED EQUIPMENT STORAGE AND PARKING AREAS.
- SHOULD ANY CONTRACTOR PERSONNEL BE IDENTIFIED AS NONCOMPLIANT WITH ANY VEHICLE DRIVING SAFETY REQUIREMENTS IN THIS PROJECT SAFETY PLAN OR IN THE AIRPORT VEHICLE OPERATIONS REGULATIONS, SUCH DRIVERS SHALL BE PENALIZED BY RESCISSION OF THEIR ON-AIRPORT DRIVING PRIVILEGES, AND THEIR ACCESS TO THE CONSTRUCTION LIMIT AREA WHEN OPERATING VEHICLES SHALL BE REVOKED.
- THE CONTRACTOR WILL BE REQUIRED TO BE IN CONTACT WITH AIRPORT OPERATIONS. THIS WILL KEEP THE CONTRACTOR IN CONTACT WITH AIRPORT PERSONNEL AND ENABLE THE AIRPORT PERSONNEL TO IMMEDIATELY CONTACT THE CONTRACTOR IN CASE OF AN AERONAUTICAL EMERGENCY THAT WOULD REQUIRE ACTION BY THE CONTRACTOR AND/OR HIS PERSONNEL

THE CONTRACTOR SHALL REMAIN WITHIN THE CONSTRUCTION LIMITS LINE SHOWN IN THE PLANS. WHEN OUTSIDE THESE LIMITS, ALL CONTRACTOR ACTIVITIES SHALL REMAIN MORE THAN 250 FEET FROM THE CENTERLINE AND 300 FEET FROM THE END OF ACTIVE RUNWAY 9-27, AND 200 FEET FROM THE CENTERLINE AND 240 FEET FROM THE END OF ACTIVE RUNWAY 1-19. FOR WORK NEAR TAXIWAYS AND APRONS, THE CONTRACTOR'S PERSONNEL AND EQUIPMENT MUST REMAIN AT LEAST 44.5 FEET FROM CENTERLINE OF ACTIVE CATEGORY LTAXIWAYS 65.5 FEET FROM ACTIVE CATEGORY II TAXIWAYS, AND TEN (10) FEET FROM ACTIVE APRONS. WHEN CONSTRUCTION OPERATIONS MUST BE CONDUCTED WITHIN THESE SEPARATIONS, THE PAVEMENT MUST BE CLOSED TO AIRCRAFT ACTIVITY BY THE CONTRACTOR BY PROVIDING TEMPORARY BARRICADES AS SHOWN IN THE PLANS, AND IN THE CASE OF RUNWAY PAVEMENTS, CLOSED RUNWAY MARKERS. WHEN HAUL VEHICLES ARE PERMITTED TO CROSS ACTIVE AIRFIELD PAVEMENTS, THE CONTRACTOR WILL PROVIDE POSITIVE CONTROL OF CONSTRUCTION VEHICLES USING RADIO-EQUIPPED FLAGGERS. CONTRACTOR SHALL ESTABLISH AND MAINTAIN RADIO CONTACT WITH KEWANEE AIRPORT UNICOM (122.8 MHz) ALL CONTRACTOR'S EQUIPMENT USED IN ACTIVE AIRPORT OPERATIONS AREAS SHALL BE EQUIPPED WITH A FAA-STANDARD FLAG, AS REFERENCED IN FAA AC 150/5370-2. CURRENT ISSUE. AIRCRAFT SHALL HAVE THE RIGHT-OF-WAY

THE CONTRACTOR SHALL KEEP ALL OF HIS EQUIPMENT AND PERSONNEL AT LEAST 15 FEET FROM THE EDGE OF ANY ACTIVE ROADWAY OR AUTO PARKING PAVEMENT. WHEN HIS ACTIVITIES REQUIRE WORKING WITHIN 15 FEET OF THE ROAD/PAVEMENT EDGE, THE CONTRACTOR SHALL PROVIDE FOR TRAFFIC CONTROL IN ACCORDANCE WITH IDOT SPECIFICATIONS (HIGHWAY STANDARDS)

OPEN TRENCHES, EXCAVATIONS AND STOCKPILED MATERIAL AT THE CONSTRUCTION SITE SHALL BE DELINEATED WITH THE USE OF BARRICADES DURING HOURS OF RESTRICTED VISIBILITY AND/OR DARKNESS. NO OPEN TRENCHES SHALL BE ALLOWED WITHIN THE RUNWAY SAFETY AREA (RSA) OR THE TAXIWAY SAFETY AREA (TSA) WHEN THE RUNWAY OR TAXIWAY IS OPEN TO AIR TRAFFIC (INCLUDING OVERNIGHT). THE RSA IS DEFINED AS 75 FEET FROM THE CENTERLINE AND 300 FEET FROM THE END OF RUNWAY 9-27, AND 60 FEET FROM THE CENTERLINE AND 240 FEET FROM THE END OF RUNWAY 1-19. THE TSA IS MEASURED AT 24.5 FEET FROM THE CATEGORY I TAXIWAY CENTERLINE AND 39.5 FEET FROM THE CATEGORY II TAXIWAY CENTERLINE. NO VERTICAL DROP OF GREATER THAN 3-INCHES IN HEIGHT FROM PAVEMENT EDGE TO EARTH GRADE OR EARTH GRADE TO EARTH GRADE WITHIN THE RSA OR TSA WILL BE PERMITTED WHEN THE RUNWAY OR TAXIWAY IS OPEN TO AIR TRAFFIC. THE CONTRACTOR WILL HAVE STEEL PLATES ON-SITE TO ALLOW FOR THE RAPID COVERING OF TRENCHES OR EARTH DROPS IN THE EVENT OF UNEXPECTED WORK STOPPAGES FOR WEATHER OR AIRPORT EMERGENCIES.

WHEN NOT IN USE AND DURING NONWORKING HOURS. CONTRACTOR'S EQUIPMENT SHALL BE PARKED WITHIN THE CONTRACTOR'S EQUIPMENT STORAGE AND PARKING AREAS. THE EQUIPMENT STORAGE AND PARKING AREAS ARE TO BE LOCATED AS SHOWN ON THE PHASING PLAN. THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTAINING THE CONSTRUCTION ENTRANCES IN GOOD CONDITION. THE COST OF MAINTAINING THE CONSTRUCTION ENTRANCE AND CONTRACTOR AREAS IS TO BE INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL PROTECT ALL EXISTING PAVEMENT EDGES FROM DAMAGE FROM CONSTRUCTION EQUIPMENT AND HAUL VEHICLES.

AT NO TIME SHALL THE CONTRACTOR CONDUCT ANY ACTIVITIES OR OPERATE OR PARK EQUIPMENT SO AS TO OBSTRUCT ACTIVE PART 77 AIRPORT IMAGINARY SURFACES OR THE RUNWAY PROTECTION ZONES (RPZ) AS DELINEATED IN THE PLANS. CONTRACTOR'S EQUIPMENT SHALL EXTEND NO HIGHER THAN 25 FEET. CRANES SHALL NOT BE USED DURING INSTRUMENT WEATHER CONDITIONS OR AT NIGHT. CRANES SHALL BE LOWERED WHEN NOT IN USE.

BEFORE REOPENING TEMPORARILY CLOSED PAVEMENTS, THE CONTRACTOR SHALL INSPECT AND CLEAN, AS NECESSARY, THE PAVEMENT TO ASSURE THAT NO MATERIALS OR OBJECTS THAT MAY DAMAGE AIRCRAFT OR VEHICLES REMAIN. ANY REQUIRED CLEANING SHALL BE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT OWNER AND IS INCIDENTAL TO THE CONTRACT.

ALL WORK SHALL BE COMPLETED IN ACCORDANCE WITH THE APPROVED PROJECT SAFETY PLAN, ISSUED BY THE ILLINOIS DIVISION OF AERONAUTICS.

FAILURE TO USE THESE PRESCRIBED PROCEDURES OR ADHERE TO THE SAFETY REQUIREMENTS WILL RESULT IN THE SUSPENSION OF WORK

NOTIFICATIONS BY CONTRACTOR

THE CONTRACTOR MUST NOTIFY THE RESIDENT ENGINEER AND THE AIRPORT OWNER 3 DAYS IN ADVANCE OF ANY REQUIRED PARTIAL OR COMPLETE CLOSING OF ANY RUNWAY, TAXIWAY OR APRON. THE DATE, TIME AND SCHEDULED DURATION OF THE CLOSING MUST BE APPROVED BY THE RESIDENT ENGINEER AND THE AIRPORT OWNER. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT OWNER 3 DAYS IN ADVANCE OF THE CONTRACTOR'S CLOSING OF OTHER ACTIVE ROADWAYS, AIRFIELD OR ROADWAY LIGHTING CIRCUITS, OR OTHER AIRPORT FACILITIES.

CONTRACTOR'S USE OF SITE

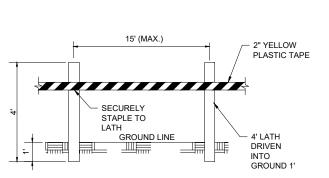
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THE CONTRACTOR IS TO PROVIDE TEMPORARY CONSTRUCTION ROADS WITHIN THE CONSTRUCTION LIMIT LINES AS MAY BE REQUIRED BY HIS ACTIVITIES. HEAVY VEHICLES SHALL NOT CROSS EXISTING PAVEMENT SURFACES EXCEPT AS APPROVED BY THE AIRPORT OWNER AND THE RESIDENT ENGINEER. ANY DAMAGE TO PAVEMENTS THAT MAY OCCUR BY THE CONTRACTOR'S ACTIVITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE AND TO THE SATISFACTION OF THE AIRPORT OWNER AND THE RESIDENT ENGINEER. FOR HAUL ROUTES MADE BY CONTRACTOR THROUGH GRASSED AREAS. CONTRACTOR SHALL GRADE. LEVEL, TOPSOIL, SEED AND MULCH AT THE END OF THE PROJECT, COST INCIDENTAL TO THE CONTRACT

THE CONTRACTOR IS TO PROVIDE AN EQUIPMENT STORAGE AND PARKING AREA AT THE LOCATIONS SHOWN IN THE PLANS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE ACCESS ROADS AND THE STORAGE AREA DURING CONSTRUCTION AND TO RESTORE THE AREAS AT PROJECT COMPLETION TO CONDITIONS SUITABLE TO THE AIRPORT OWNER AND THE RESIDENT ENGINEER. AT THE AIRPORT OWNER'S DISCRETION. THE TEMPORARY FACILITIES MAY REMAIN, BUT THEY MUST BE LEFT IN CONDITIONS SUITABLE TO THE AIRPORT OWNER. THE COST OF PROVIDING, MAINTAINING AND RESTORING THE TEMPORARY FACILITIES IS INCIDENTAL TO THE CONTRACT

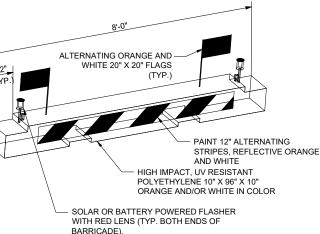
UTILITY OUTAGES AND SHUTDOWNS

THE CONTRACTOR SHALL PROVIDE 3 DAYS PRIOR NOTICE OF ANY OUTAGES OR SHUTDOWNS TO THE OWNER AND THE AGENCY OWNING THE AFFECTED UTILITY. THE CONTRACTOR SHALL PROVIDE ANY TEMPORARY CONNECTIONS OR OTHER MEASURES AS MAY BE REQUIRED TO MAINTAIN SERVICE AS MAY BE REQUIRED BY THE OWNING AGENCY AT NO COST TO THE OWNER.



MATERIALS ARE TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION. COST OF MATERIALS, INSTALLATION, RELOCATION AND MAINTENANCE OF LATHING AND WARNING TAPE IS TO BE INCIDENTAL TO THE CONTRACT.

> DETAIL A LATHING AND WARNING TAPE



- NOTES 1 INTENDED USE FOR THE FOLLOWING

- 20" FLAGS

 - 150/5370-2 (LATEST EDITION)

- MARKING/LIGHTING OF TEMPORARY HAZARDS WITHIN THE AOA. - LONGTERM CLOSURE OF AIRCRAFT ROUTES.

2. INSTALL AT 12' CENTER TO CENTER SPACING ALONG FULL WIDTH OF PAVEMENT

3. USE TYPE 2 AIRCRAFT BARRICADES IN AREAS SUBJECT TO JET BLAST.

4. BARRICADE SHALL BE EQUIPPED WITH ALTERNATING ORANGE AND WHITE 20" X

5. BARRICADES SHALL BE WATER-FILLED AND MODULAR TO ASSEMBLE/DISASSEMBLE AND NEST FOR COMPACT STORAGE

6. CONTRACTOR MAY SUBMIT ALTERNATIVE BARRICADE FOR APPROVAL BY ENGINEER. ALTERNATIVE MUST MEET MINIMUM REQUIREMENTS OF FAA AC

7 FURNISHING INSTALLING MAINTAINING AND REMOVING BARRICADES SHALL BE PAID UNDER ITEM AR150530. SEE SPECIAL PROVISIONS.

8. CONTRACTOR SHALL MAINTAIN THE BARRICADES. ANY DAMAGED BARRICADES SHALL BE REPLACED AND NEW BARRICADES PROVIDED

DETAIL B TYPE 2 - LOW PROFILE AIRCRAFT BARRICADE DETAIL



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KEWANEE MUNICIPAL AIRPORT 3769 Midland Road Kewanee, Illinois 61443 Telephone: 309.853.8222

RECONSTRUCT WEST PORTION OF RAMP

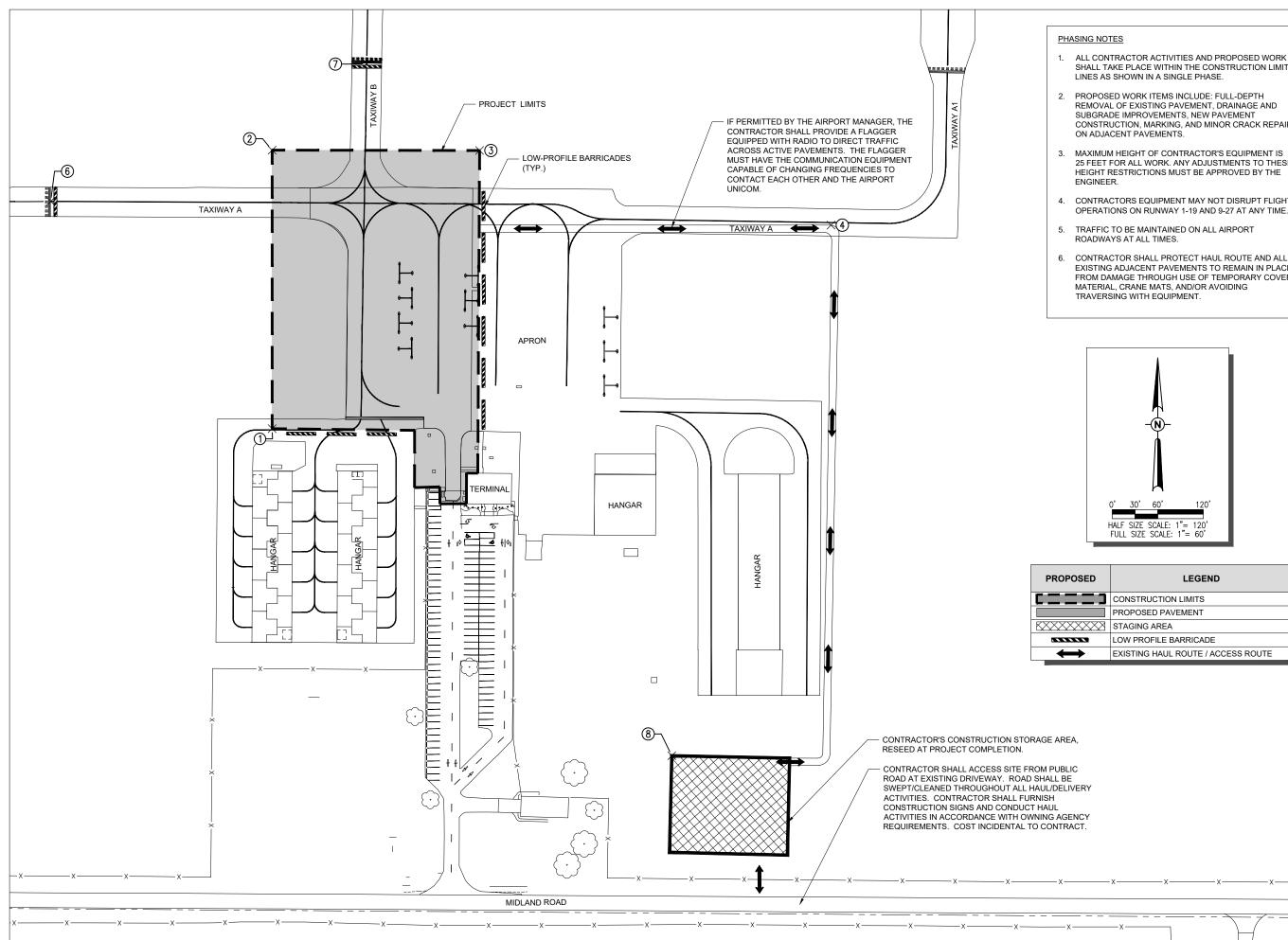
IDA No: EZI-4883

SBGP No: 3-17-SBGP-162/171

Contract No.: KE018

NO.	DATE	DES	CRIPT	ION
NO.	DATE	LAY	DWN	REV
ISSUE:	NOVEM	BER 1	9, 202	1
PROJEC	CT NO: 2	1A000	8	
CAD FIL	E: C-101-S	OW.DWG	i	
LAYOUT	BY: LD	H 08/2	24/202	1
DRAWN	BY: HLE	E 08/2	4/202	1
REVIEW	ED BY:	LDH	11/17	/21
Copyright Han	son Professional S	Services Inc. 2	2013	
SHEET	TITLE			

SITE AND SAFETY PLAN NOTES



NO 12

1. ALL CONTRACTOR ACTIVITIES AND PROPOSED WORK SHALL TAKE PLACE WITHIN THE CONSTRUCTION LIMIT

REMOVAL OF EXISTING PAVEMENT, DRAINAGE AND SUBGRADE IMPROVEMENTS, NEW PAVEMENT CONSTRUCTION, MARKING, AND MINOR CRACK REPAIR

25 FEET FOR ALL WORK. ANY ADJUSTMENTS TO THESE HEIGHT RESTRICTIONS MUST BE APPROVED BY THE

4. CONTRACTORS EQUIPMENT MAY NOT DISRUPT FLIGHT OPERATIONS ON RUNWAY 1-19 AND 9-27 AT ANY TIME.

6. CONTRACTOR SHALL PROTECT HAUL ROUTE AND ALL EXISTING ADJACENT PAVEMENTS TO REMAIN IN PLACE FROM DAMAGE THROUGH USE OF TEMPORARY COVER MATERIAL, CRANE MATS, AND/OR AVOIDING

SED	LEGEND
	CONSTRUCTION LIMITS
	PROPOSED PAVEMENT
\times	STAGING AREA
	LOW PROFILE BARRICADE
→	EXISTING HAUL ROUTE / ACCESS ROUTE

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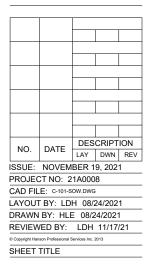
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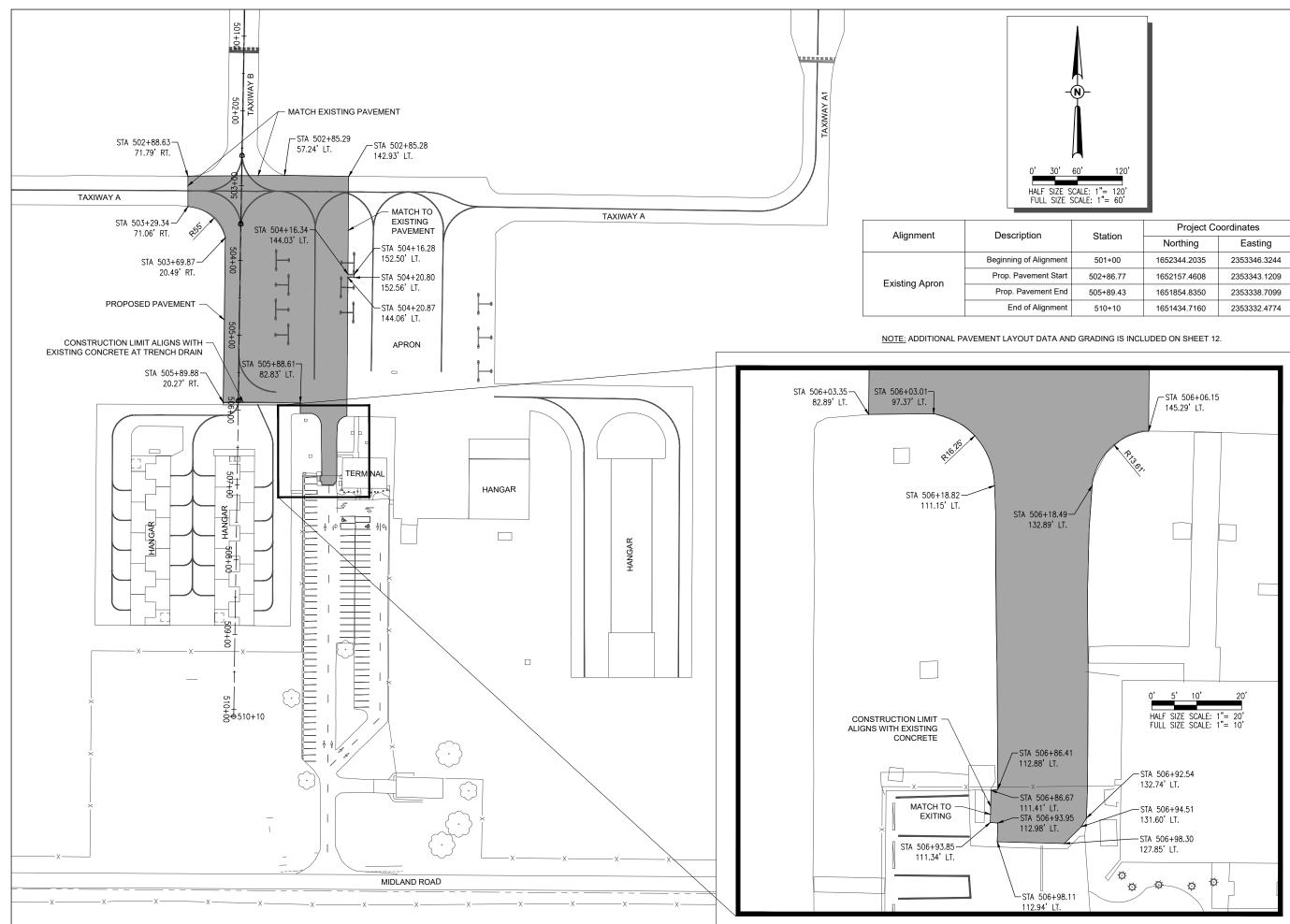
IDA No: EZI-4883

SBGP No: 3-17-SBGP-162/171

Contract No.: KE018



PHASING PLAN



JSM00682 AΜ 8 202 NOV 121.

	Station	Project Co	oordinates		
	Station	Northing	Easting		
nt	501+00	1652344.2035	2353346.3244		
rt	502+86.77	1652157.4608	2353343.1209		
d	505+89.43	1651854.8350	2353338.7099		
nt	510+10	1651434.7160	2353332.4774		



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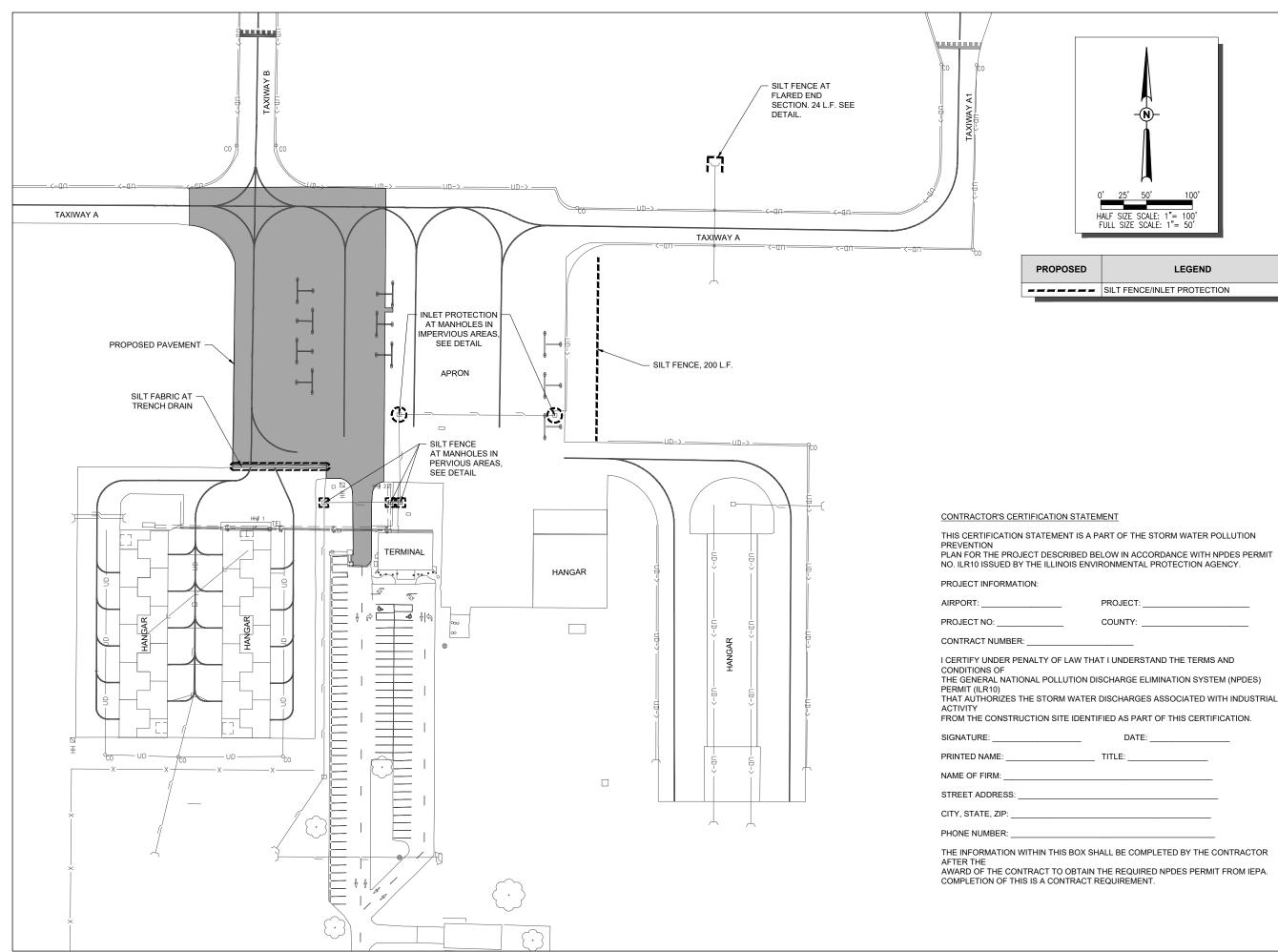
IDA No: EZI-4883

SBGP No: 3-17-SBGP-162/171

Contract No.: KE018



ALIGNMENT DATA TABLE AND PAVEMENT LAYOUT



N0 5

LEGEND
 SILT FENCE/INLET PROTECTION



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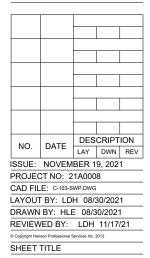
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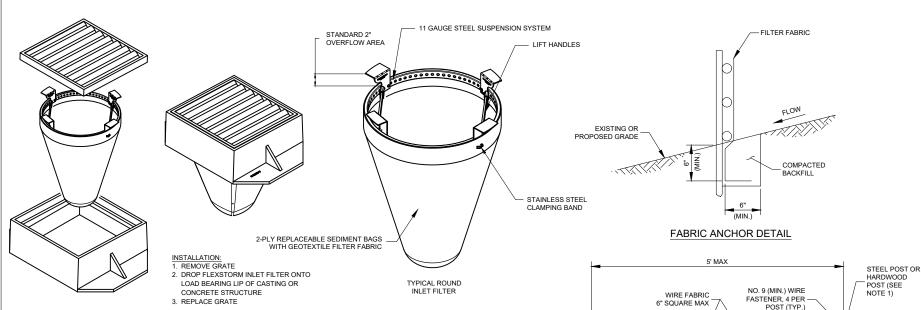
IDA No: EZI-4883

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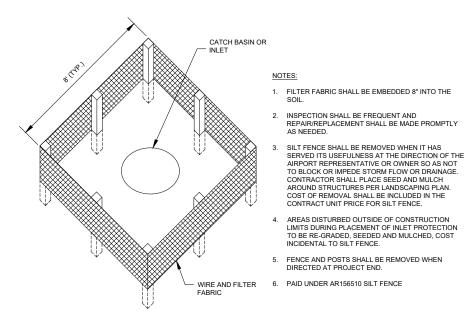
STORMWATER POLLUTION PREVENTION PLAN (SWPP)



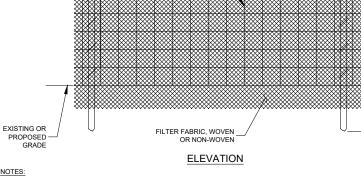
NOTES:

- 1. FILTER FABRIC INLET PROTECTION SHALL CONSIST OF INLET BASKET AND FABRIC INSERT. FLEXSTORM CATCH-IT BY ADVANCED DRAINAGE SYSTEMS, FLOGARD TEMPORARY INLET FILTER BY OLDCASTLE, OR APPROVED EQUAL
- 2. DEVICE SHALL BE EQUIPPED WITH AN OVERFLOW FEATURE SO DRAINAGE TO INLET IS NOT COMPLETELY BLOCKED IF DEVICE IS FULL OF SILT
- 3. INLET BASKET IS AVAILABLE TO FIT ROUND, RECTANGULAR, BEEHIVE OR CURB INLET CASTINGS.
- 4. FILTER FABRIC SHALL HAVE AN APPARENT OPENING SIZE (AOS) OF AT LEAST 70 SIEVE FOR NONWOVEN.
- 5. FILTER FABRIC SHALL HAVE A GRAB TENSILE STRENGTH OF A LEAST 100 LBS FOR NON WOVEN
- 6. POLYESTER OUTER REINFORCEMENT BAG SHALL HAVE FABRIC WITH A WEIGHT OF 4.55 OZ/SQYD +/- 15 PERCENT
- 7. FRAME CONSTRUCTION SHALL HAVE A TENSILE STRENGTH OF AT LEAST 58,000 PSI AND A YIELD STRENGTH OF AT LEAST 36,000 PSI
- 8. MAINTENANCE SHALL BE PERFORMED AS NEEDED. REMOVE SILT FROM FABRIC INSERT WHEN 50% OF CAPACITY IS REACHED REMOVE SILT FROM INTERIOR AND EXTERIOR OF INLET DAM WHEN 50% OF DAM HEIGHT IS REACHED.
- 9. PAYMENT FOR INLET PROTECTION MAINTENANCE SHALL BE INCIDENTAL TO INLET PROTECTION.

INLET PROTECTION AT MANHOLES IN IMPERVIOUS AREAS



SILT FENCE AT MANHOLES IN PERVIOUS AREAS



NOTES:

- FENCE POST SHALL BE EITHER STEEL "T" LINE POST OR HARDWOOD POST WITH A MINIMUM SECTIONAL AREA OF 2.0 SQUARE INCHES, A CARPENTER'S (NOMINAL) 2"x2" POST WILL MEET SPECIFICATIONS
- TOP AND BOTTOM WIRE OF WIRE FABRIC SHALL BE MINIMUM GAGE NO. 9. INTERMEDIATE 2. WIRES OF THE WIRE FABRIC SHALL BE MINIMUM GAGE NO. 11.
- WIRE FABRIC SHALL BE SECURELY FASTENED TO FENCE POSTS WITH NO. 9 GAGE WIRE MINIMUM. FOUR (4) FASTENERS PER POST REQUIRED.
- FILTER FABRIC SHALL BE SECURELY FASTENED TO WIRE FABRIC AND POSTS WITH TIES OR STAPLES SPACED AT 12" APART AT THE TOP, MIDDLE AND BOTTOM.
- 5. WHEN TWO SECTIONS OF FILTER FABRIC MEET, THEY SHALL BE OVERLAPPED BY 6" AND FOLDED AND ATTACHED TO THE WIRE FABRIC AT A POST.
- 6. FILTER FABRIC SHALL BE IN ACCORDANCE WITH SPECIAL PROVISIONS WITH APPARENT OPENING SIZE (AOS) OF AT LEAST 40 FOR NONWOVEN AND WOVEN. THE FABRIC MUST MEET THE APPLICABLE STANDARDS OF AASHTO 288-00 (Article IV, Section B.1,j.1,f.i, AS AMENDED), OR EQUIVALENT.
- A MAXIMUM OF 5 FEET IS USED FOR POST-TO-POST SPACING.
- SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR. SITE CONDITIONS AND THE USE OF TEMPORARY OR PERMANENT MEASURES
- ALL STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL 9 BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.
- 10. SILT FENCE SHALL BE INSTALLED PRIOR TO ANY GRADING WORK IN THE AREA TO BE PROTECTED. PERIODIC INSPECTION SHALL BE PERFORMED AND REQUIRED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN EVENT.
- 11. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED AND REPLACED WHEN BULGES DEVELOP IN THE SILT FENCE.
- 12. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION. DISCHARGES SHALL BE ROUTED THROUGH AN EFFECTIVE SEDIMENT CONTROL MEASURE (E.G. SEDIMENT TRAP, SEDIMENT BASIN, OR OTHER APPROPRIATE MEASURE).
- 13. FENCE POSTS SHALL BE REMOVED WHEN DIRECTED AT PROJECT END
- 14. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER OR GOVERNING AGENCY.

SEDIMENTATION AND EROSION CONTROL NOTES:

- UPLAND AREAS.
- DOCUMENTATION SHALL BE PERFORMED, AT A MINIMUM: UPON COMPLETION OF SEDIMENT AND RUNOFF CONTROL MEASURES (INCLUDING PERIMETER CONTROLS AND
- EQUIVALENT PRECIPITATION
- AND SEDIMENT CONTROL MEASURES.
- D. CONTROLLED SEDIMENT DISPOSAL AREA.
- Ε. DISTURBED AREAS TO AN APPROPRIATE SEDIMENT TRAP OR BASIN.
- FLOOD PRONE AREAS OR WETLANDS AND DESIGNATED BUFFERS
- OFFICER
- BETWEEN THE NORMAL WATER LEVEL AND HIGH WATER LEVEL
- J. SEDIMENT CONTROL MEASURE.
- SITE, ADDITIONAL MEASURES SUCH AS ANIONIC POLYMERS OR FILTRATION SYSTEMS MAY BE REQUIRED BY THE ENFORCEMENT OFFICER.
- ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED.

STORM WATER POLLUTION PREVENTION NOTES

THE CONTRACTOR SHALL IMPLEMENT ALL PROVISIONS OF THE CONTRACT DOCUMENTS TO ASSURE THAT STORM WATER POLLUTION PREVENTION ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY MANNER. SEDIMENTATION MUST NOT BE TRANSPORTED OFF THE CONSTRUCTION SITE. PERMANENT DRAINAGE FEATURES AND VEGETATIVE MEASURES SHALL BE PROVIDED AS SOON AS POSSIBLE.

THE MAINTENANCE OF ALL STORM WATER POLLUTION PREVENTION MEASURES IS INCIDENTAL TO THE ASSOCIATED ITEM.

POLLUTION PREVENTION MEASURES

THE CONTRACTOR SHALL BE REQUIRED TO IMPLEMENT AND MAINTAIN STORM WATER POLLUTION PREVENTION PRACTICES AND MEASURES PRIOR TO THE STRIPPING OF EXISTING VEGETATION WHEREVER POSSIBLE AND AS SOON AS CONSTRUCTION PERMITS IN OTHER AREAS. POLLUTION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE CONTRACT DOCUMENTS, INCLUDING THESE CONSTRUCTION PLANS, AND WITH STANDARDS AND SPECIFICATIONS FOR SOLIL EROSION AND SEDIMENT CONTROL, ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, CURRENT ISSUE. THE CONTRACTOR SHALL ADJUST HIS OPERATIONS AND IMPLEMENT POLITION CONTROL MEASURES SO THAT NO RUNOFF FROM STRIPPED AREAS WILL LEAVE THE CONSTRUCTION SITE OTHER THAN THROUGH SEDIMENT TRAPS OR OTHER SUITABLE CONTROL MEASURES.

POLLUTION CONTROL ITEMS SHALL BE PROVIDED AS NOTED ON THE STORM WATER POLLUTION PREVENTION PLAN AND IN THE STORM WATER POLLUTION PREVENTION DETAILS AND AS DIRECTED BY THE ENGINEER. THE LIMITS OF SUCH MEASURES SHALL BE STAKED BY THE CONTRACTOR PRIOR TO THE COMMENCEMENT OF CONSTRUCTION. SUCH LIMITS MAY BE ADJUSTED BY THE ENGINEER TO ACCOUNT FOR ACTUAL SITE CONDITIONS EXPERIENCED DURING CONSTRUCTION. ADDITIONAL COMPENSATION FOR MEASURES EXCEEDING THE PLAN QUANTITIES WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR

THE CONTRACTOR IS TO MAINTAIN AND ADJUST. REPAIR OR REPLACE ALL POLLUTION PREVENTION MEASURES AS REQUIRED OR AS DIRECTED BY THE ENGINEER UNTIL PERMANENT VEGETATION HAS BEEN ESTABLISHED. MAINTENANCE OF POLLUTION CONTROL MEASURES IS TO BE PROVIDED AT NO ADDITIONAL COST TO THE CONTRACT.

ADDITIONAL STORMWATER POLLUTION PREVENTION MEASURES ARE EXISTING ON SITE LOCATED AT DRAINAGE FACILITIES AND ALONG THE PROPERTY LINE.

A. SEDIMENT CONTROL MEASURES SHALL BE INSTALLED PRIOR TO THE COMMENCEMENT OF HYDROLOGIC DISTURBANCE OF

B. FOR THOSE DEVELOPMENTS THAT REQUIRE A DESIGNATED EROSION CONTROL INSPECTOR (DECI), INSPECTIONS AND

DIVERSIONS), PRIOR TO PROCEEDING WITH ANY OTHER EARTH DISTURBANCE OR GRADING. AFTER EVERY SEVEN (7) CALENDAR DAYS OR STORM EVENT WITH GREATER THAN 0.5 INCH OF RAINFALL OR LIQUID

C. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. IF STRIPPING, CLEARING, GRADING, OR LANDSCAPING ARE TO BE DONE IN PHASES, THE PERMITTEE SHALL PLAN FOR APPROPRIATE SOIL EROSION

A STABILIZED MAT OF CRUSHED STONE MEETING IDOT GRADATION CA-01 UNDERLAIN WITH FILTER FABRIC AND IN ACCORDANCE WITH THE ILLINOIS URBAN MANUAL, OR OTHER APPROPRIATE MEASURE(S) AS APPROVED BY THE ENFORCEMENT OFFICER, SHALL BE INSTALLED AT ANY POINT WHERE TRAFFIC WILL BE ENTERING OR LEAVING A CONSTRUCTION SITE. SEDIMENT OR SOIL REACHING AN IMPROVED PUBLIC RIGHT OF WAY, STREET, ALLEY OR PARKING AREA SHALL BE REMOVED BY SCRAPING OR STREET CLEANING AS ACCUMULATIONS WARRANT AND TRANSPORTED TO A

TEMPORARY DIVERSIONS SHALL BE CONSTRUCTED AS NECESSARY TO DIRECT ALL RUNOFF FROM HYDROLOGICALLY

DISTURBED AREAS SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES WITHIN SEVEN (7) CALENDAR DAYS FOLLOWING THE END OF ACTIVE HYDROLOGIC DISTURBANCE OR REDISTURBANCE.

G. ALL STOCKPILES SHALL HAVE APPROPRIATE MEASURES TO PREVENT EROSION. STOCKPILES SHALL NOT BE PLACED IN

SLOPES STEEPER THAN 3H:1V SHALL BE STABILIZED WITH APPROPRIATE MEASURES AS APPROVED BY THE ENFORCEMENT

APPROPRIATE EROSION CONTROL BLANKET SHALL BE INSTALLED ON ALL INTERIOR DETENTION BASIN SIDE SLOPES

STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE

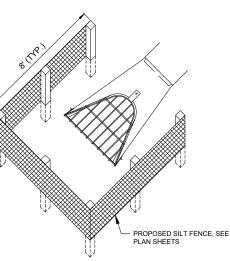
IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION. DISCHARGES SHALL BE ROUTED THROUGH AN APPROVED ANIONIC POLYMER DEWATERING SYSTEM OR A SIMILAR MEASURE AS APPROVED BY THE ENFORCEMENT OFFICER. DEWATERING SYSTEMS SHOULD BE INSPECTED DAILY DURING OPERATIONAL PERIODS. THE ENFORCEMENT OFFICER, OR APPROVED REPRESENTATIVE, MUST BE PRESENT AT THE COMMENCEMENT OF DEWATERING ACTIVITIES.

L. IF INSTALLED SOIL EROSION AND SEDIMENT CONTROL MEASURES DO NOT MINIMIZE SEDIMENT LEAVING THE DEVELOPMENT

M. ALL TEMPORARY AND PERMANENT EROSION CONTROL MEASURES MUST BE MAINTAINED AND REPAIRED AS NEEDED. THE PROPERTY OWNER SHALL BE ULTIMATELY RESPONSIBLE FOR MAINTENANCE AND REPAIR.

ALL TEMPORARY SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS

0. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER, ENFORCEMENT OFFICER, OR OTHER GOVERNING AGENCY.



SILT FENCE PLACEMENT AT FLARED END SECTIONS (FES)



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RECONSTRUCT WEST PORTION OF RAMP

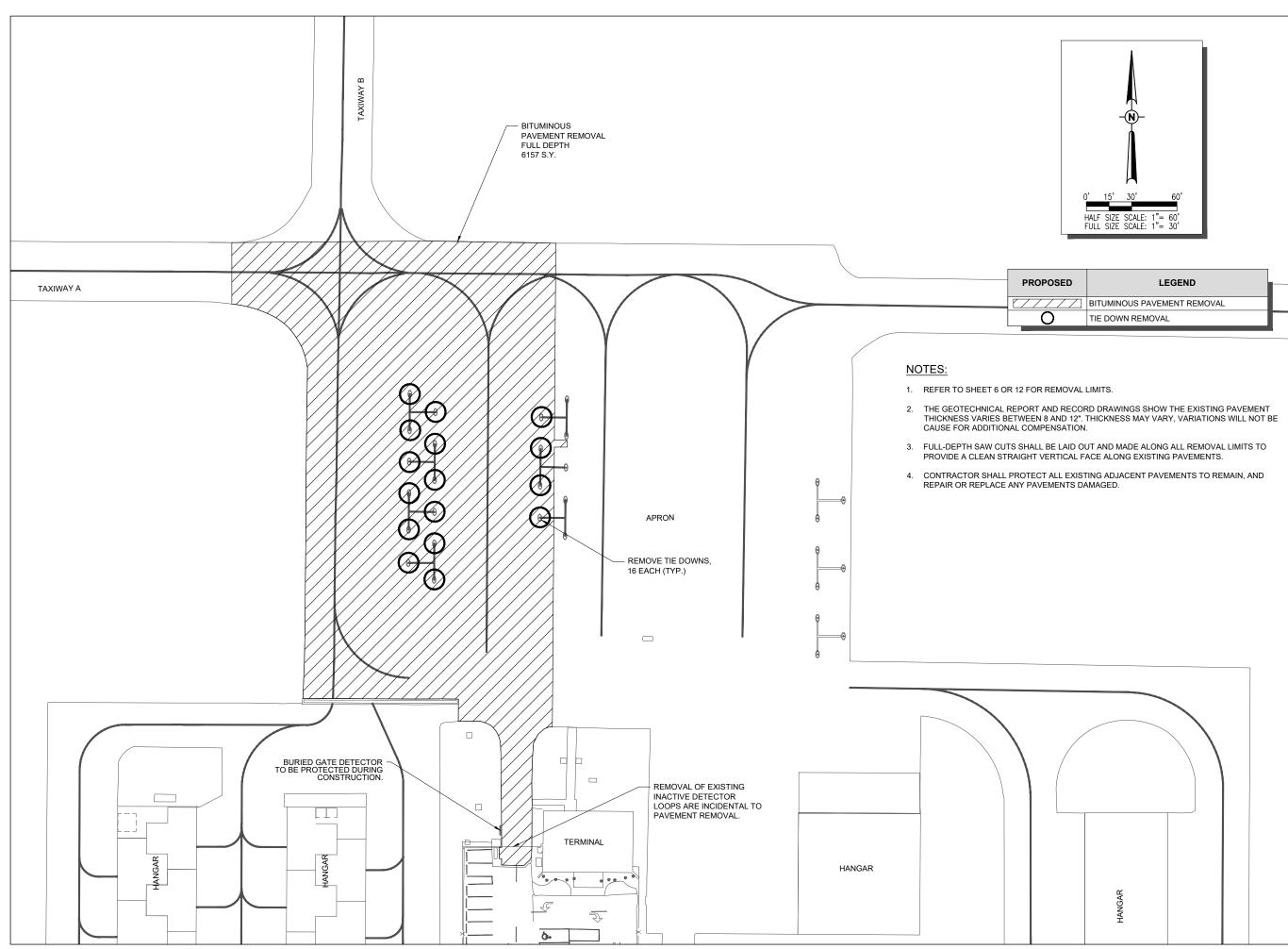
IDA No: EZI-4883

SBGP No: 3-17-SBGP-162/171

Contract No.: KE018

NO.	DATE	DES	CRIPT	ION
NO.	DATE	LAY	DWN	REV
ISSUE:	NOVEM	BER 1	9, 202	1
PROJEC	PROJECT NO: 21A0008			
CAD FILE: C-201-DET.DWG				
LAYOUT	BY: LD	H 08/3	30/202	1
DRAWN BY: HLE 08/30/2021				
REVIEWED BY: LDH 11/17/21				
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SHEET TITLE				

SWPP DETAILS



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	LEGEND	
\sim	BITUMINOUS PAVEMENT REMOVAL	
	TIE DOWN REMOVAL	



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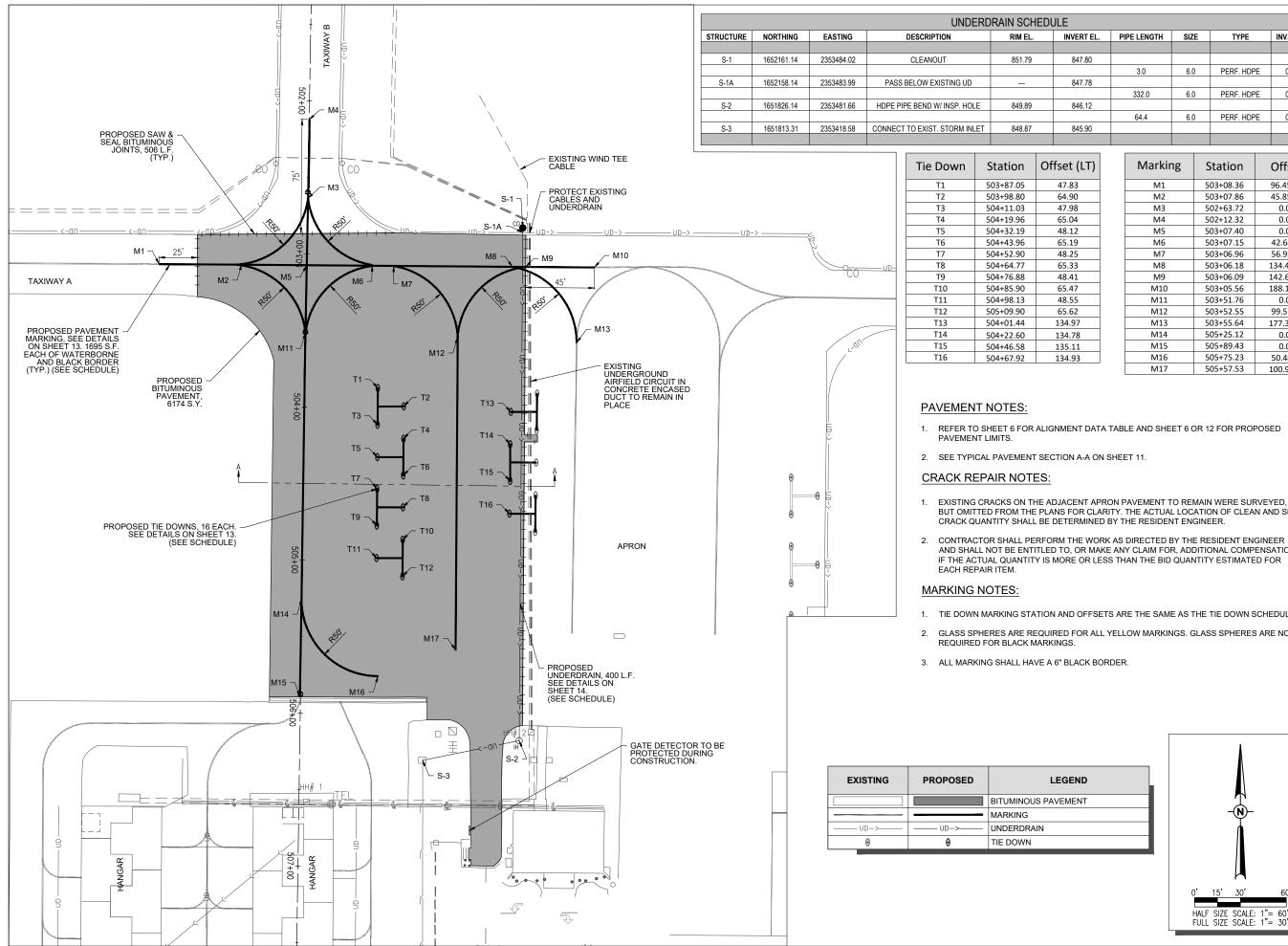
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Contract No.: KE018



REMOVAL PLAN



T EL.	PIPE LENGTH	SIZE	TYPE	INV. SLOPE
80				
	3.0	6.0	PERF. HDPE	0.50%
78				
	332.0	6.0	PERF. HDPE	0.50%
12				
	64.4	6.0	PERF. HDPE	0.34%
۵N				

Marking	Station	Offset
M1	503+08.36	96.45 RT
M2	503+07.86	45.85 RT
M3	502+63.72	0.00
M4	502+12.32	0.00
M5	503+07.40	0.00
M6	503+07.15	42.68 LT
M7	503+06.96	56.95 LT
M8	503+06.18	134.46 LT
M9	503+06.09	142.65 LT
M10	503+05.56	188.15 LT
M11	503+51.76	0.00
M12	503+52.55	99.57 LT
M13	503+55.64	177.32 LT
M14	505+25.12	0.00
M15	505+89.43	0.00
M16	505+75.23	50.48 LT
M17	505+57.53	100.95 LT

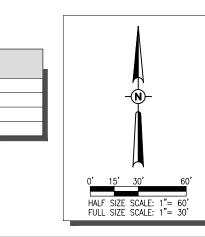
REFER TO SHEET 6 FOR ALIGNMENT DATA TABLE AND SHEET 6 OR 12 FOR PROPOSED

1. EXISTING CRACKS ON THE ADJACENT APRON PAVEMENT TO REMAIN WERE SURVEYED, BUT OMITTED FROM THE PLANS FOR CLARITY. THE ACTUAL LOCATION OF CLEAN AND SEAL

AND SHALL NOT BE ENTITLED TO, OR MAKE ANY CLAIM FOR, ADDITIONAL COMPENSATION IF THE ACTUAL QUANTITY IS MORE OR LESS THAN THE BID QUANTITY ESTIMATED FOR

1. TIE DOWN MARKING STATION AND OFFSETS ARE THE SAME AS THE TIE DOWN SCHEDULE.

2. GLASS SPHERES ARE REQUIRED FOR ALL YELLOW MARKINGS. GLASS SPHERES ARE NOT





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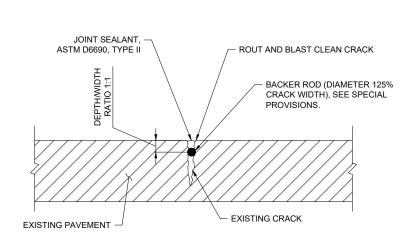
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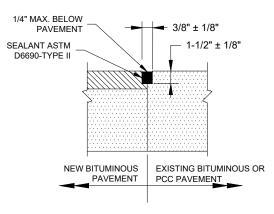
Contract No.: KE018

NO.	DATE	DES	CRIPT	ION
INU.	DATE	LAY	DWN	REV
ISSUE:	ISSUE: NOVEMBER 19, 2021			
PROJEC	CT NO: 2	1A000	8	
CAD FIL	E: C-121-C	ON.DWG		
LAYOUT BY: LDH 09/30/2021				
DRAWN BY: HLE 09/30/2021				
REVIEWED BY: LDH 11/17/21				
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PROPOSED PLAN

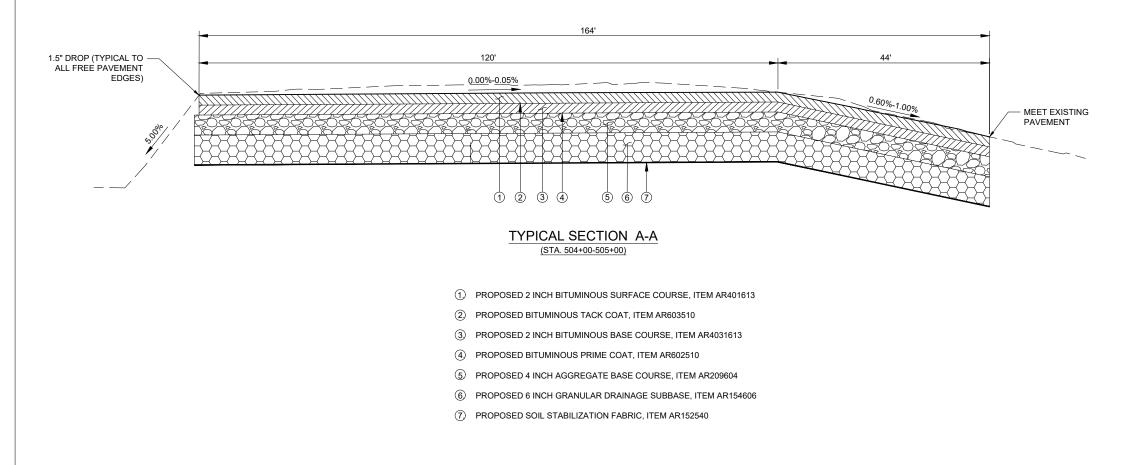


CLEAN AND SEAL CRACKS



NOTE: ALL BITUMINOUS/BITUMINOUS JOINT SEALING TO BE PAID UNDER SAW AND SEAL BITUMINOUS JOINTS, ITEM AR401660.

BITUMINOUS/BITUMINOUS SEAL





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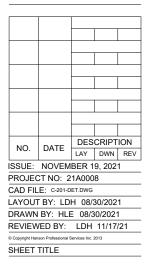
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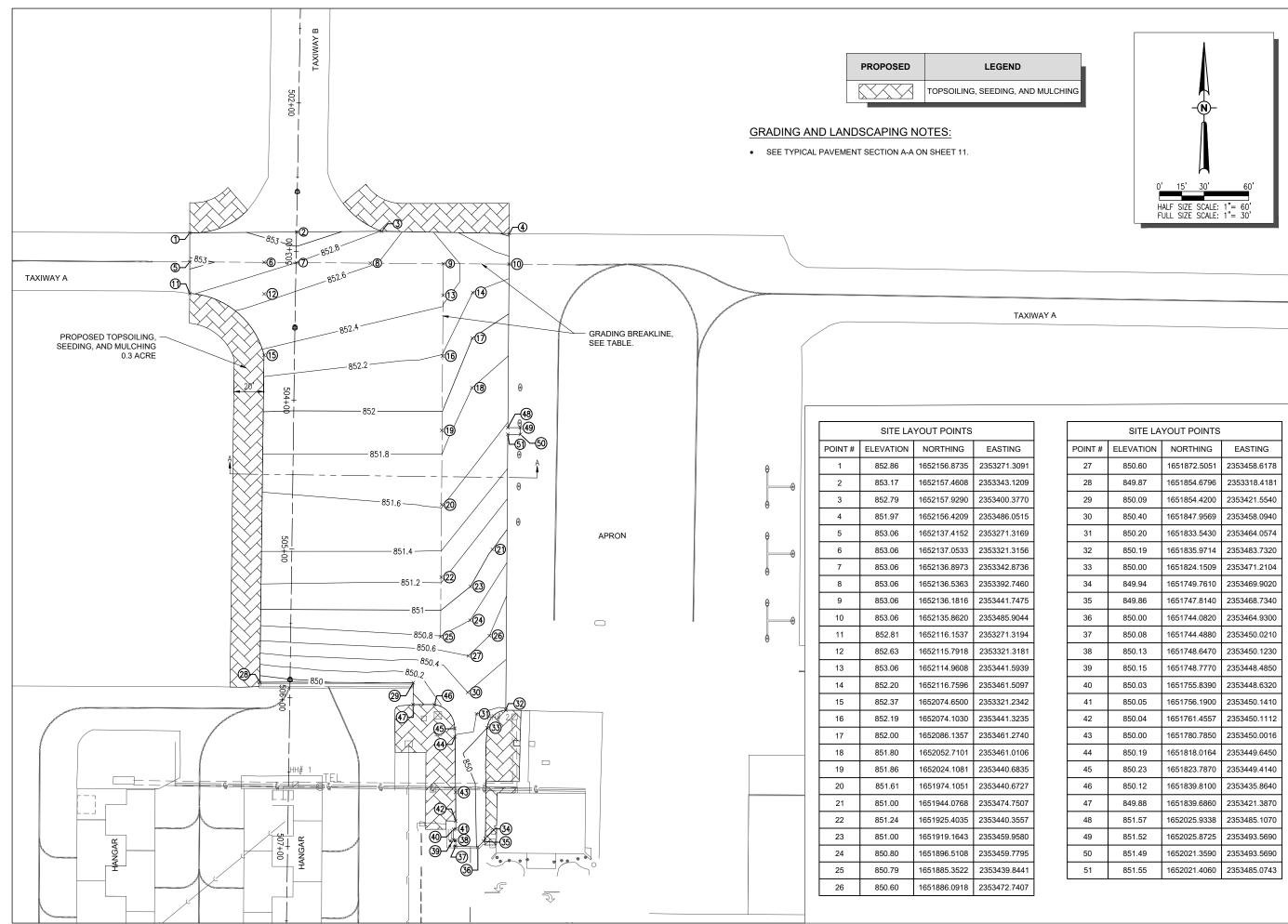
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TYPICAL SECTION AND PAVEMENT DETAILS





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SITE LAYOUT POINTS			
POINT #	ELEVATION	NORTHING	EASTING
27	850.60	1651872.5051	2353458.6178
28	849.87	1651854.6796	2353318.4181
29	850.09	1651854.4200	2353421.5540
30	850.40	1651847.9569	2353458.0940
31	850.20	1651833.5430	2353464.0574
32	850.19	1651835.9714	2353483.7320
33	850.00	1651824.1509	2353471.2104
34	849.94	1651749.7610	2353469.9020
35	849.86	1651747.8140	2353468.7340
36	850.00	1651744.0820	2353464.9300
37	850.08	1651744.4880	2353450.0210
38	850.13	1651748.6470	2353450.1230
39	850.15	1651748.7770	2353448.4850
40	850.03	1651755.8390	2353448.6320
41	850.05	1651756.1900	2353450.1410
42	850.04	1651761.4557	2353450.1112
43	850.00	1651780.7850	2353450.0016
44	850.19	1651818.0164	2353449.6450
45	850.23	1651823.7870	2353449.4140
46	850.12	1651839.8100	2353435.8640
47	849.88	1651839.6860	2353421.3870
48	851.57	1652025.9338	2353485.1070
49	851.52	1652025.8725	2353493.5690
50	851.49	1652021.3590	2353493.5690
51	851.55	1652021.4060	2353485.0743

RECONSTRUCT WEST PORTION OF RAMP

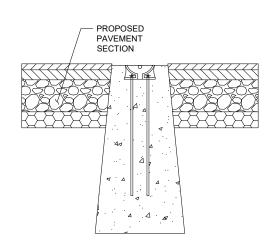
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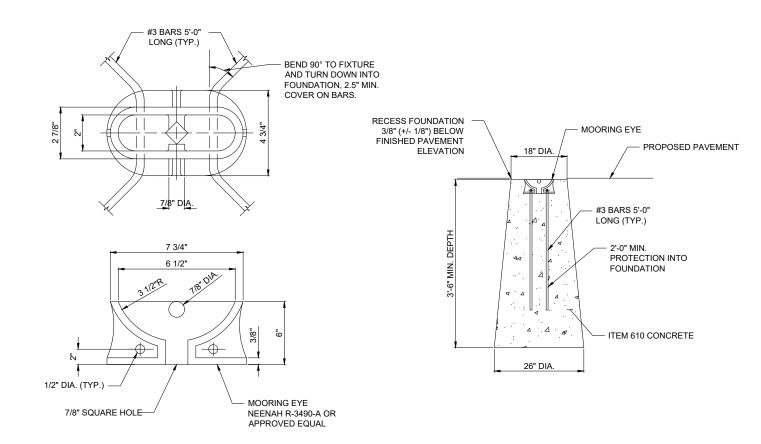
GRADING AND LANDSCAPING PLAN



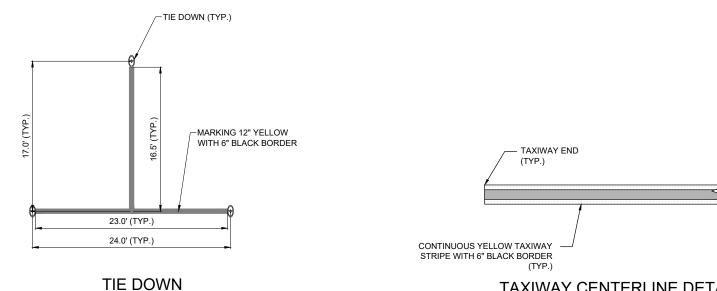
NOTES:

1. EXCAVATION, BACKFILL, CONCRETE FOUNDATION AND MOORING EYE SHALL BE INCIDENTAL TO TIE DOWN.

NEW TIE DOWN IN NEW PAVEMENT AREA



TIE DOWN (MOORING EYE)



MARKING DETAIL

TAXIWAY CENTERLINE DETAIL

-6" TYP.

-1' TYP.



Offices Nationwide www.hanson-inc.com

Hanson Professional Services Inc. 1525 S. 6th Street Springfield, IL 62568 phone: 217-788-2450 fax: 217-788-2503

Illinois Licensed Professional Service Corporation #184-001084



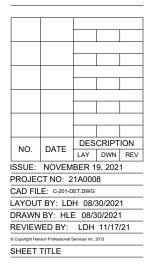
KEWANEE MUNICIPAL AIRPORT 3769 Midland Road Kewanee, Illinois 61443 Telephone: 309.853.8222

RECONSTRUCT WEST PORTION OF RAMP

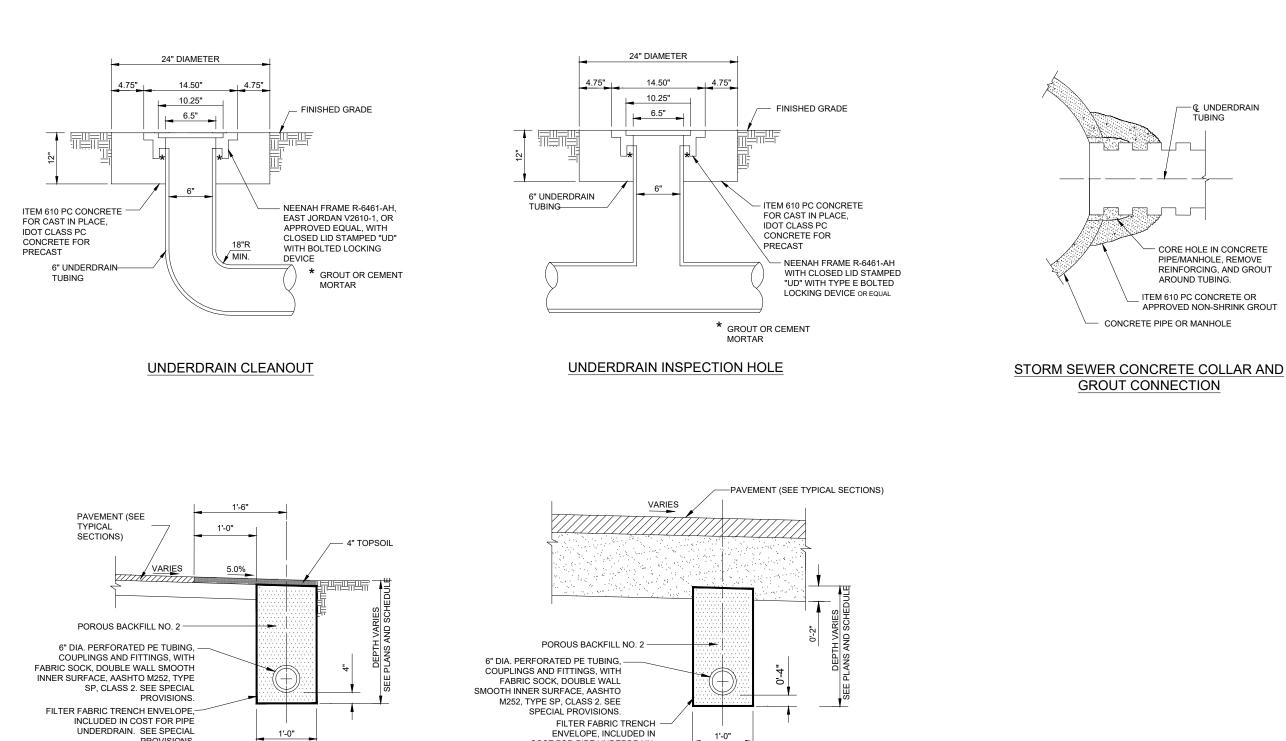
IDA No: EZI-4883

SBGP No: 3-17-SBGP-162/171

Contract No.: KE018



TIE DOWN AND MARKING DETAILS



COST FOR PIPE UNDERDRAIN.

SEE SPECIAL PROVISIONS.

UNDERDRAIN UNDER PAVEMENT

UNDERDRAIN ALONG PAVEMENT EDGE

PROVISIONS.

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DRAINAGE DETAILS