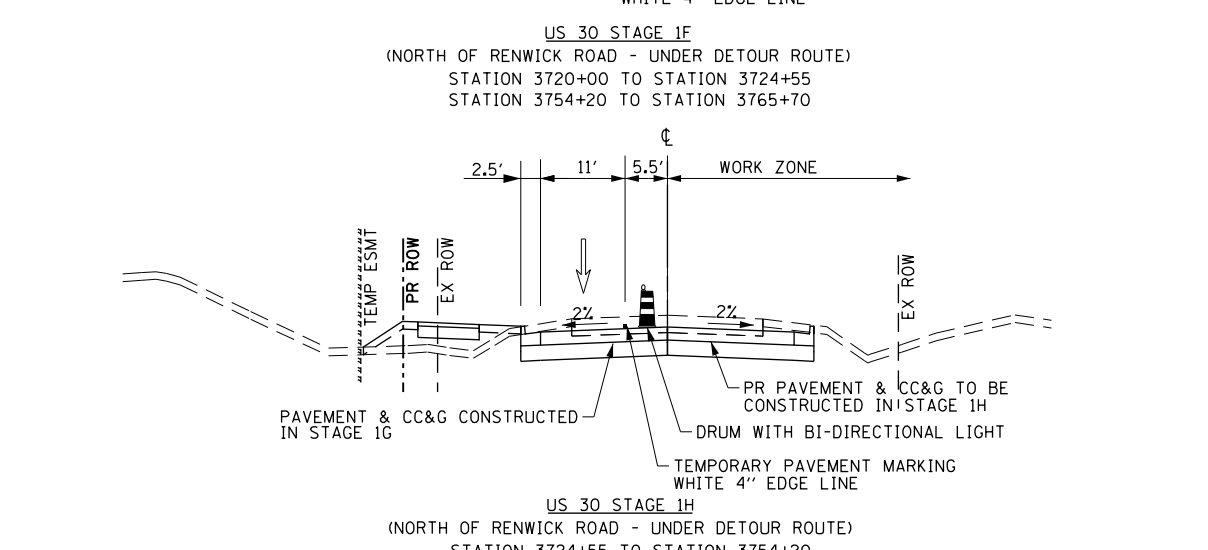
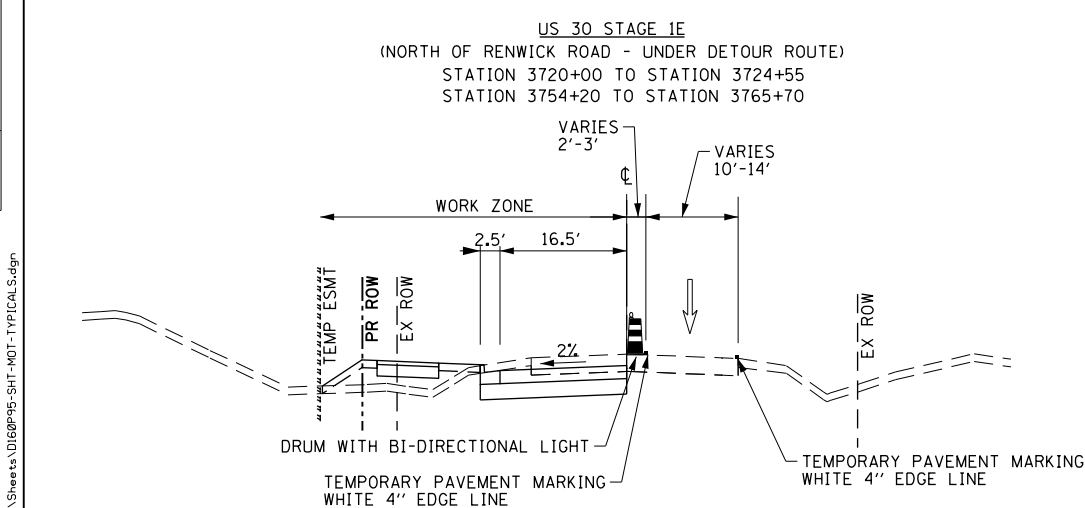
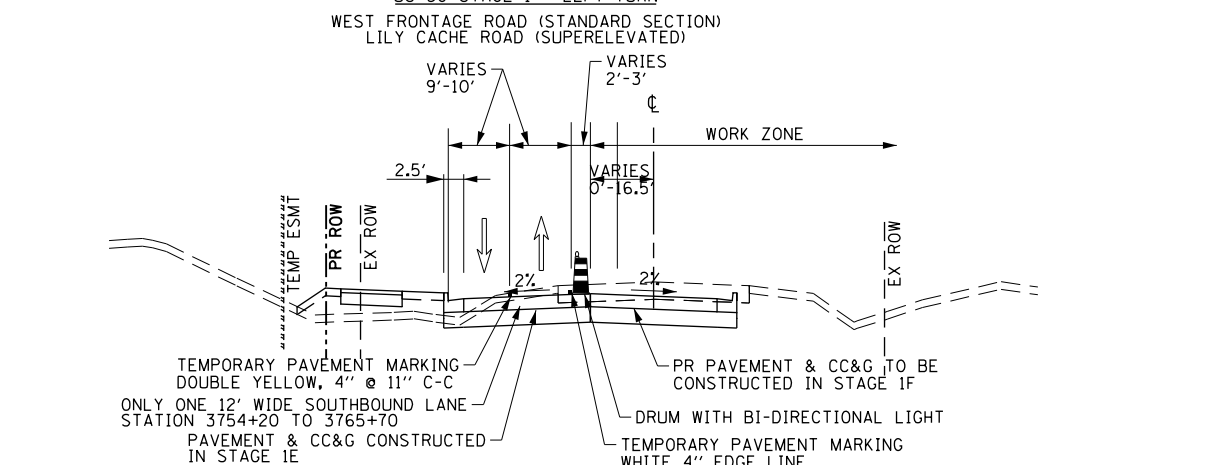
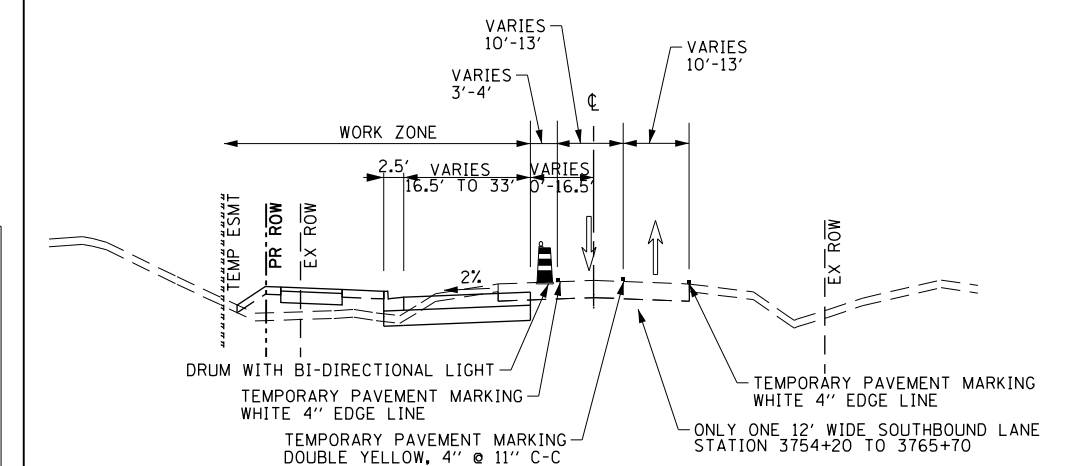
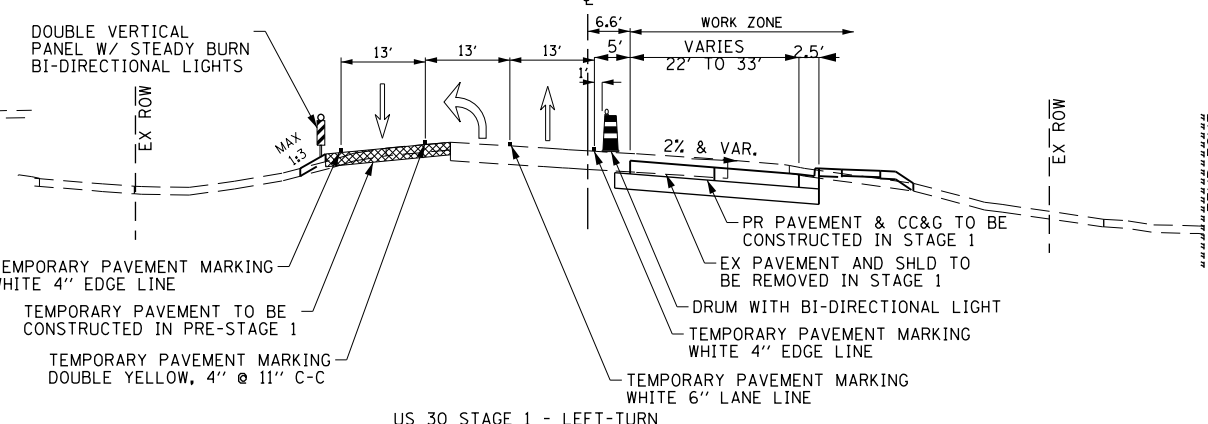
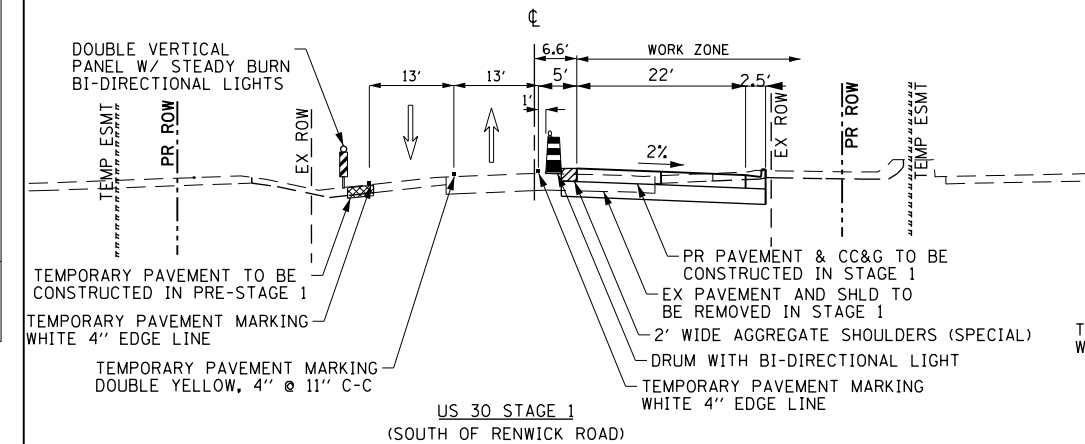
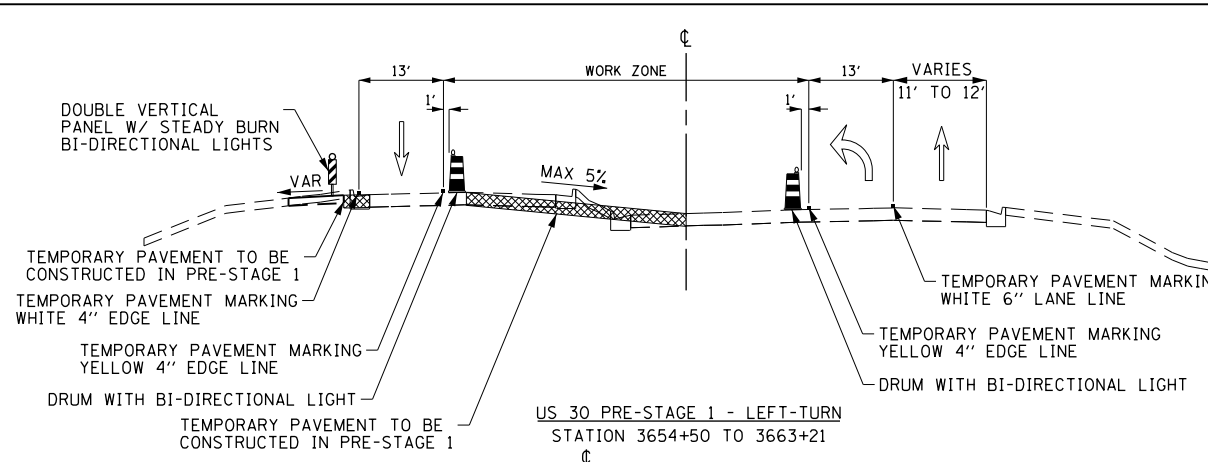
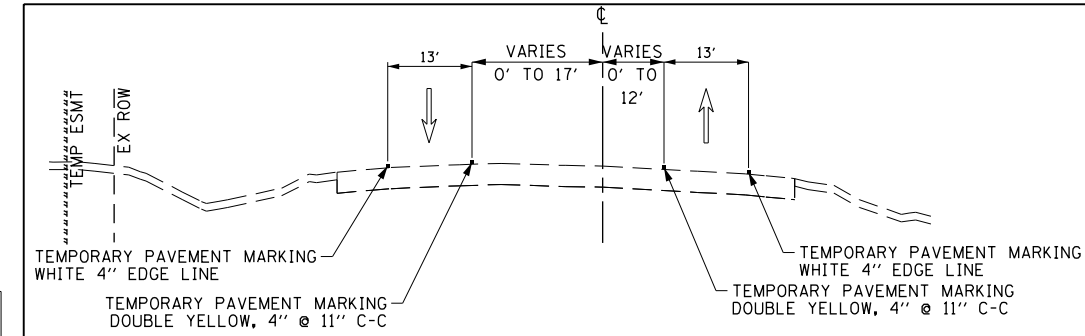




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**PRE-STAGE 1**  
MAINTAIN TWO WAY TRAFFIC ON TWO LANES OF EXISTING PAVEMENT USING DRUMS WITH BI-DIRECTIONAL LIGHTS AT THE SPACING SHOWN IN THE PLANS. REDUCE TO ONE LANE WITH DAYTIME CLOSURES AND FLAGGERS AS NEEDED.

THE EXISTING MEDIAN BETWEEN THE I-55 RAMPS AND WEST FRONTAGE ROAD SHALL BE REPLACED WITH TEMPORARY PAVEMENT. THE TEMPORARY PAVEMENT SECTION WILL BE WIDER THAN THE EXISTING MEDIAN TO LIMIT THE CROSS-SLOPE. REFER TO THE TEMPORARY PAVEMENT DETAILS AND PLANS FOR ADDITIONAL GUIDANCE. A 6" BARRIER CURB SHALL BE INSTALLED TO PROVIDE PROTECTION FOR THE SIGNAL EQUIPMENT AT THE I-55 RAMPS, AS SHOWN IN THE PLANS.

THE CONTRACTOR SHALL INSTALL THE PROPOSED STORM SEWER TRUNKLINE FROM WEST FRONTAGE ROAD TO STRUCTURE 3-03, INCLUDING 3-05, AS SHOWN IN THE PROPOSED DRAINAGE PLANS. INSTALLATION WILL ALLOW FOR THE OUTLET OF STRUCTURES INSTALLED ALONG WESTBOUND US 30 IN STAGE 1 THAT TIE-INTO THE TRUNKLINE. THE CONTRACTOR SHALL EXTEND THE LATERAL PIPES IN PRE-STAGE 1 TO A POINT WHERE THEY WILL BE ABLE TO TIE-IN WESTBOUND STRUCTURES DURING STAGE 1 OF SUGGESTED TRAFFIC CONTROL PLANS. CONSTRUCTION OF THE LATERALS IN PRE-STAGE 1 MAY REQUIRE DAYTIME LANE CLOSURES AND ROADWAY PATCHING. ALL TRAFFIC CONTROL DEVICES, LABOR, AND MAINTENANCE FOR THESE OPERATIONS SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION, (SPECIAL). ROADWAY PATCHING SHALL BE ACCORDING TO DISTRICT DETAIL BD-22 AND SHALL BE PAID FOR SEPARATELY. AT RENWICK ROAD, STRUCTURES 16-09 AND 16-10 SHALL BE INSTALLED TO ALLOW FOR OUTLET OF THE TRUNKLINE ALONG THE NORTH SIDE OF RENWICK THAT WILL BE INSTALLED IN STAGE 1.

THE CONTRACTOR SHALL INSTALL THE TEMPORARY STORM SEWER FOR DIVERSION OF WATER AS SHOWN ON THE EROSION CONTROL PLANS EC1-2 AND EC1-7. THE STORM SEWER WILL BEGIN AT THE SOUTH PROJECT LIMITS OF THE WEST FRONTAGE ROAD AND EAST PROJECT LIMITS OF U.S. ROUTE 30 AND END AT PROPOSED DRAINAGE STRUCTURE 2-44. THE FLARED END SECTIONS AND CATCH BASIN ON THIS TEMPORARY SYSTEM SHALL BE REMOVED AND THE PIPES PLUGGED WHEN STAGES 2, 2A AND 2B ARE COMPLETED AND LANDSCAPED.

CONSTRUCT TEMPORARY PAVEMENT ALONG THE LEFT (WEST) SIDE OF US 30 AND SELECT LOCATIONS ALONG RENWICK ROAD. INSTALLATION OF TEMPORARY PAVEMENT ALONG EDGE OF PAVEMENT WILL REQUIRE TEMPORARY DRAINAGE SOLUTIONS AT SELECT LOCATIONS. CONTRACTOR SHALL ADJUST, RECONSTRUCT, AND INSTALL TEMPORARY DRAINAGE FEATURES AS SHOWN IN THE PRE-STAGE 1 STAGING PLANS. THE COST FOR TEMPORARY DRAINAGE FEATURES SHOWN IN THE PLANS WILL BE PAID FOR SEPARATELY ACCORDING TO THE APPLICABLE PAY ITEMS PROVIDED. ANY ADDITIONAL TEMPORARY DRAINAGE NECESSARY DUE TO THE CONTRACTOR ALTERING THE STAGING PLANS SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION, (SPECIAL).

**STAGE 1**  
SET UP TRAFFIC CONTROL STRIPING, SIGNS AND DRUMS FOR US 30 WITH TWO LANES OF TRAFFIC (ONE THRU LANE IN EACH DIRECTION). LANES TO BE SHIFTED WEST ON EXISTING AND TEMPORARY PAVEMENT.

CONSTRUCT 2 LANES OF PROPOSED PCC PAVEMENT, CURB AND GUTTER, DRIVEWAYS, AND SIDEWALK. INSTALL DRAINAGE STRUCTURE AND PIPES ALONG WESTBOUND US 30. A 2' WIDE AGGREGATE SHOULDER SHALL BE BUILT ALONG THE SOUTH LEG OF US 30 TO PROVIDE A PLATFORM FOR TRAFFIC CONTROL DEVICES, AND TO LIMIT THE DROP-OFF FOR TRAFFIC.

THE CONTRACTOR SHALL RELOCATE THE EXISTING TEMPORARY CONCRETE BARRIER AT THE ENTRANCE OF ABANDONED TRUCK STOP, APPROXIMATE STATION 3663+50, SO THAT IS NOT IN CONFLICT WITH PROPOSED CONSTRUCTION. SHALL BE PAID FOR AS RELOCATE TEMPORARY CONCRETE BARRIER. THE CONTRACTOR SHALL NOT MOVE THE CONCRETE BARRIER BACK TO ITS ORIGINAL LOCATION ONCE CONSTRUCTION OF THE PAVEMENT IS COMPLETE. THE ENTRANCE TO THE ABANDONED TRUCK STOP SHALL STILL BE BLOCKED FROM TRAFFIC ENTERING.

THE CONTRACTOR SHALL CONSTRUCT MCCLELLAN STREET AND COLLINS DRIVE AT RENWICK ROAD UNDER FULL CLOSURES TO EXPEDITE THE CONSTRUCTION, AS DESCRIBED IN THE CONTRACT SPECIFICATIONS. THE CLOSURES SHALL NOT EXCEED ONE WEEK IN DURATION.

THE INTERSECTION OF US 30 AND RENWICK ROAD SHALL BE CONSTRUCTED OVER SEVERAL SUB-STAGES DURING THE PROJECT. TRAFFIC SHALL REMAIN IN THE SAME CONFIGURATION ALONG THE SOUTH LEG OF US 30, WHILE LANES ARE SHIFTED ALONG RENWICK ROAD DURING STAGE 1 SUB-STAGES, TO ACCOMMODATE WORK ZONES. REFER TO RENWICK ROAD PLAN SHEETS AND STAGING TYPICAL SECTIONS FOR DETAILS.

AT THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY PLACE FINAL PAVEMENT MARKINGS AT LOCATIONS WHERE TRAFFIC WILL BE IN ITS FINAL CONFIGURATION IN STAGES 2 AND 3. OTHERWISE, THE CONTRACTOR SHALL PLACE TEMPORARY PAVEMENT MARKINGS AS SHOWN IN THE PLANS.

**STAGE 1E, 1F, 1G, & 1H (U.S. ROUTE 30 NORTH OF RENWICK ROAD)**  
US 30, FROM RENWICK ROAD TO IL 59, SHALL BE CONSTRUCTED UNDER AN ALTERNATE ROUTE OR DETOUR IN FOUR (4) STAGES AS SHOWN IN THE PLANS. CONSTRUCTION OF US 30, FROM COLLINS DRIVE TO COPLEY PARK, SHALL BE LIMITED DURING THE HERON MATING SEASON, MARCH 1 TO AUGUST 1. THE CONTRACTOR SHALL SET UP THE ALTERNATE ROUTE BEFORE BEGINNING ANY WORK ON U.S. ROUTE 30 NORTH OF RENWICK ROAD. ALLOWABLE WORK FROM COLLINS DRIVE TO COPLEY PARK BETWEEN MARCH 1 AND AUGUST 1 INCLUDES CONSTRUCTION OF EROSION CONTROL FEATURES, STORM SEWER SYSTEMS, PRIVATE UTILITY RELOCATION, TREE REMOVAL, SHARED-USE PATH, CHAIN LINK FENCE, TOPSOIL, SODDING, SEEDING, AND TREE PLANTINGS. OTHER WORK ITEMS MAY BE REQUESTED BY THE CONTRACTOR, BUT ARE AT THE DISCRETION OF THE ENGINEER. PAVEMENT REMOVAL AND PCC PAVEMENT CONSTRUCTION ARE PROHIBITED BETWEEN COLLINS DRIVE AND COPLEY PARK BETWEEN MARCH 1 AND AUGUST 1. THE CONTRACTOR SHALL SET UP THE DETOUR ROUTE BEFORE BEGINNING PAVEMENT REMOVAL, AND THE DETOUR ROUTE SHALL BE IN PLACE NO EARLIER THAN JULY 8, 2015. THE DETOUR ROUTE SHALL BE FINISHED AND THE ROADWAY OPENED TO TRAFFIC BY NOVEMBER 20, 2015. NO WORK ITEMS ARE PROHIBITED PRIOR TO MARCH 1 AND AFTER AUGUST 1.

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>U.S. ROUTE 30</b>	
<b>SUGGESTED TRAFFIC CONTROL TYPICAL SECTIONS</b>	
SCALE: NTS	SHEET NAME: MOT-TYP-01
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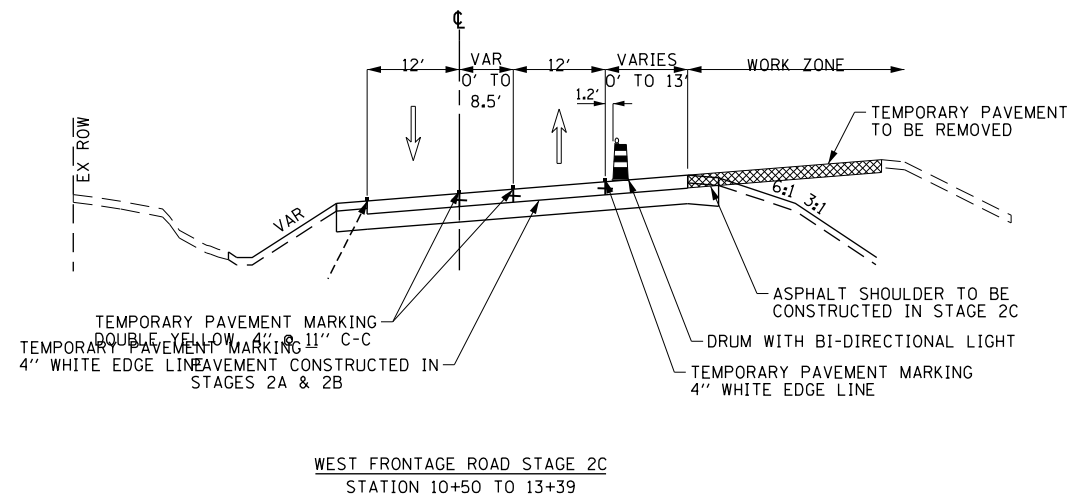
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CONTRACT NO. 60P95				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				



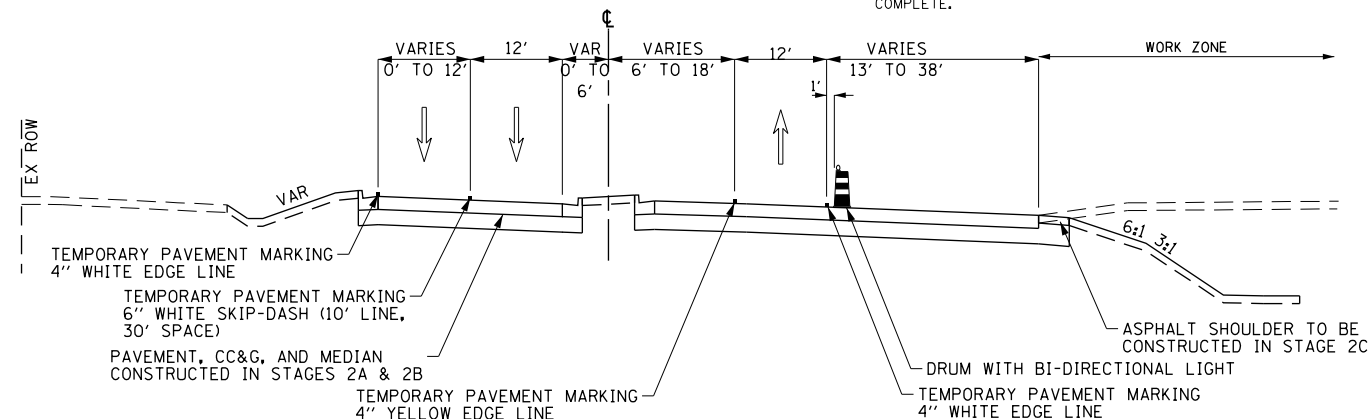


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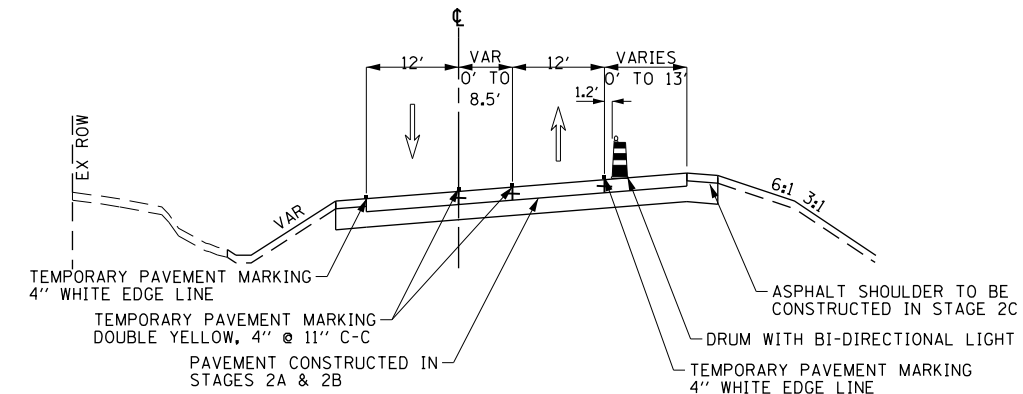
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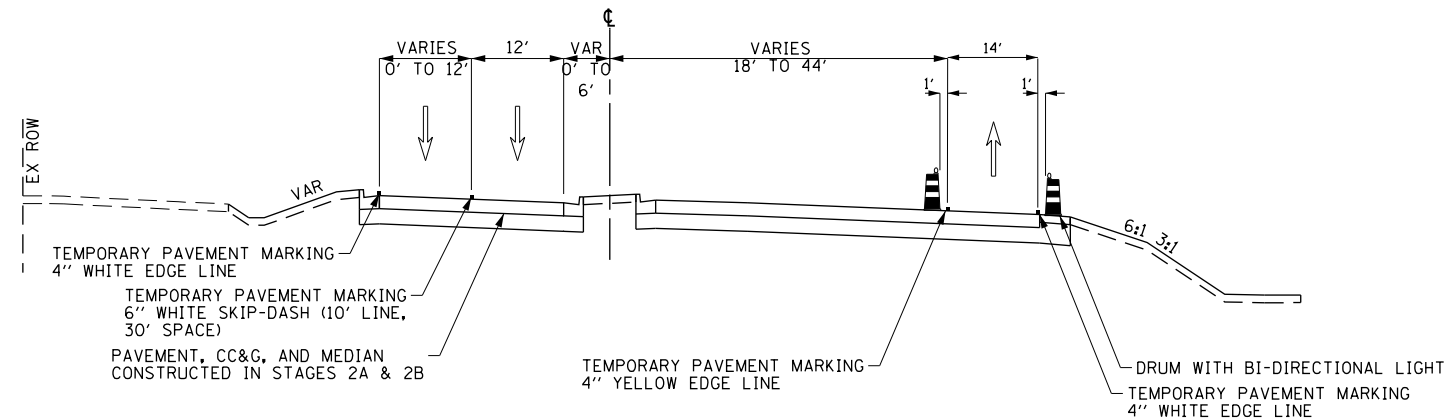
WEST FRONTAGE ROAD STAGE 2C  
STATION 10+50 TO 13+39



WEST FRONTAGE ROAD STAGE 2C  
STATION 13+39 TO 19+70



WEST FRONTAGE ROAD STAGE 2D  
STATION 10+50 TO 13+39



WEST FRONTAGE ROAD STAGE 2D  
STATION 13+39 TO 19+70

STAGE 2C  
SET UP TRAFFIC CONTROL STRIPING, SIGNS AND DRUMS FOR WEST FRONTAGE ROAD WITH TWO LANES OF TRAFFIC (ONE THRU LANE IN EACH DIRECTION). LANES TO BE SHIFTED TO NEWLY CONSTRUCTED PAVEMENT.

REMOVE TEMPORARY PAVEMENT AND COMPLETE EARTHWORK TO FINAL GRADE.

CONSTRUCT SHOULDER AND DRAINAGE ON NORTHBOUND WEST FRONTAGE ROAD AND PROPOSED PCC IN THE INTERSECTION ACCORDING TO THE PLANS.

CONSTRUCT TEMPORARY RAMP TO TIE-INTO US 30 AT INTERSECTION.

STAGE 2D  
SET UP TRAFFIC CONTROL STRIPING, SIGNS, AND DRUMS FOR WEST FRONTAGE ROAD WITH TWO LANES OF TRAFFIC (ONE THRU LANE IN EACH DIRECTION).

CONSTRUCT PROPOSED PAVEMENT ACCORDING TO THE PLANS.

STAGE 2 FINAL CONFIGURATION & STAGE 3  
SET UP TRAFFIC CONTROL STRIPING, SIGNS AND DRUMS FOR WEST FRONTAGE ROAD WITH TWO LANES OF TRAFFIC (ONE THRU LANE IN EACH DIRECTION) ONCE STAGE 2D IS COMPLETED FOR WEST FRONTAGE ROAD.

TRAFFIC SHALL REMAIN IN THIS CONFIGURATION THROUGH STAGE 3. THE CONTRACTOR CAN PLACE FINAL PAVEMENT MARKINGS ALONG WEST FRONTAGE ROAD ONCE STAGE 3 IS COMPLETE.

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

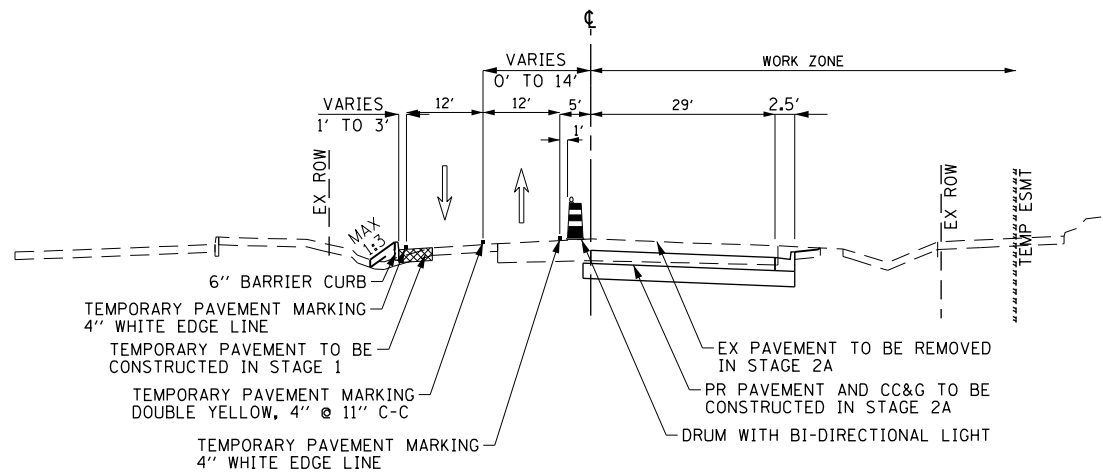
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SUGGESTED TRAFFIC CONTROL TYPICAL SECTIONS

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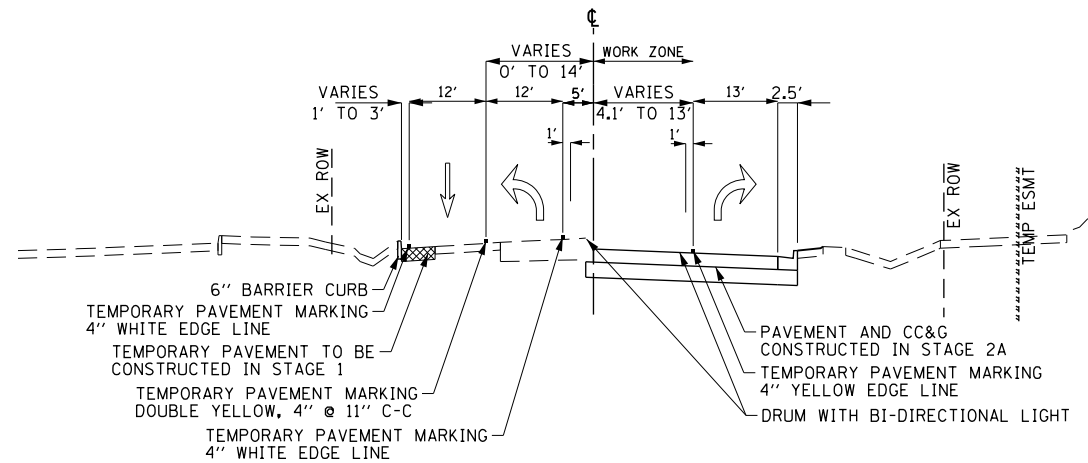
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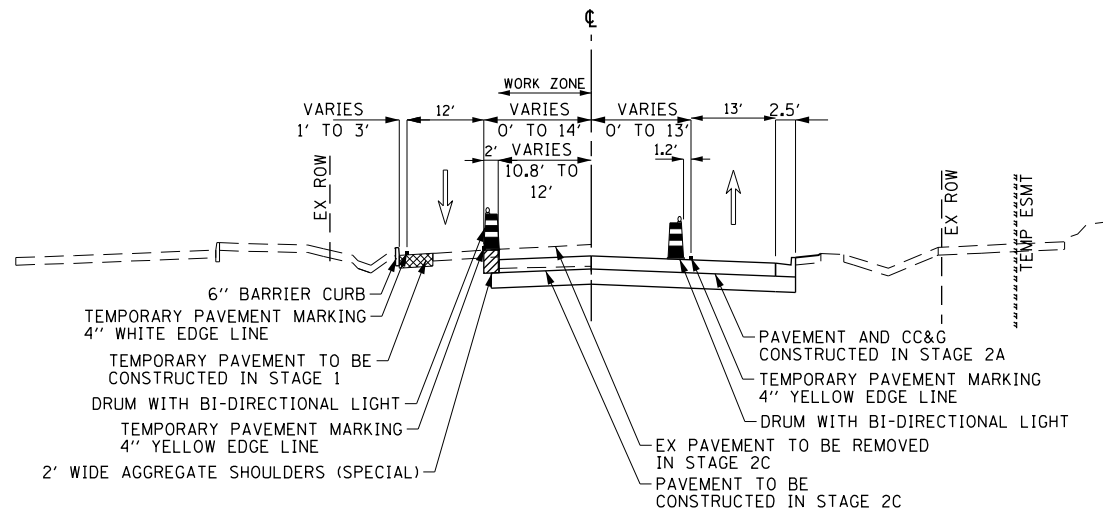
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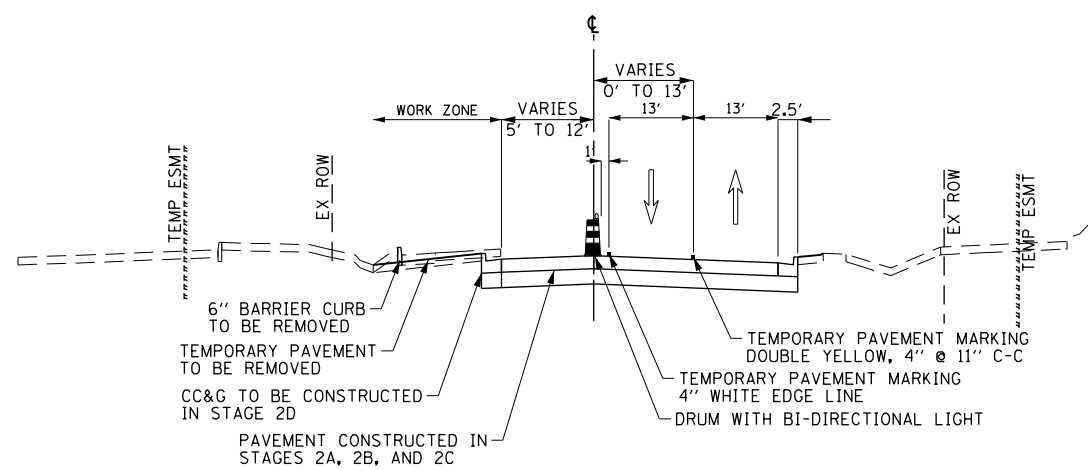
LILY CACHE ROAD STAGE 2A  
STATION 5+00 TO 9+10



LILY CACHE ROAD STAGE 2B  
STATION 5+00 TO 9+09



LILY CACHE ROAD STAGE 2C  
STATION 5+00 TO 9+13



LILY CACHE ROAD STAGE 2D  
STATION 5+00 TO 9+30

PRE-STAGE & STAGE 1  
CONSTRUCT TEMPORARY PAVEMENT AND 6" BARRIER CURB ALONG THE LEFT (WEST) SIDE OF LILY CACHE ROAD AND MAINTAIN TWO LANE TRAFFIC ON EXISTING PAVEMENT.

ADJUST THE EXISTING STORM SEWER STRUCTURES FLUSH WITH THE TEMPORARY PAVEMENT TO ACCOMMODATE TEMPORARY DRAINAGE.

STAY IN EXISTING CONFIGURATION THROUGH PRE-STAGE AND STAGE 1.

STAGE 2A  
SET UP TRAFFIC CONTROL STRIPING, SIGNS AND DRUMS FOR LILY CACHE ROAD WITH TWO LANES OF TRAFFIC (ONE LANE FOR SOUTHBOUND TRAFFIC AND ONE LANE FOR NORTHBOUND RIGHT TURN ONLY).

VARIABLE DEPTH TEMPORARY PAVEMENT SHALL BE CONSTRUCTED ALONG US 30 DURING PRE-STAGE 2, ALLOWING FOR LEFT TURN LANE.

CONSTRUCT TWO LANES OF PROPOSED PCC PAVEMENT, CURB AND GUTTER, DRIVEWAYS, DRAINAGE STRUCTURES, AND PIPES ON NORTHBOUND LILY CACHE ROAD.

STAGE 2B  
SET UP TRAFFIC CONTROL STRIPING, SIGNS AND DRUMS FOR LILY CACHE ROAD WITH THREE LANES OF TRAFFIC (ONE LANE FOR SOUTHBOUND TRAFFIC, ONE LANE FOR NORTHBOUND LEFT TURN ONLY, AND ONE LANE FOR NORTHBOUND RIGHT TURN ONLY).

CONSTRUCT PROPOSED PCC PAVEMENT ACCORDING TO THE PLANS. THE LENGTH OF THE STAGE SHALL BE LIMITED TO ONE (1) WEEK OR LESS IN DURATION.

SUGGESTED STAGE 2B AT LILY CACHE SHALL NOT ALLOW TRUCK TRAFFIC TO MAKE A LEFT TURN FROM THE WB TURN LANE DURING THE STAGE. THE CONTRACTOR SHALL CONTACT THE BUSINESSES AT THE NORTHWEST CORNER OF LILY CACHE ROAD AND US ROUTE 30 TWO (2) WEEKS IN ADVANCE OF THE STAGE TO COORDINATE TRUCK TRAFFIC AND DELIVERIES. STAGE 2B AT LILLY CACHE ROAD SHALL NOT EXCEED ONE (1) WEEK IN DURATION.

STAGE 2C  
SET UP TRAFFIC CONTROL STRIPING, SIGNS AND DRUMS FOR LILY CACHE ROAD WITH TWO LANES OF TRAFFIC (ONE THRU LANE IN EACH DIRECTION). NORTHBOUND LANE TO BE SHIFTED EAST ON NEWLY CONSTRUCTED PAVEMENT. SOUTHBOUND LANE TO STAY ON EXISTING AND TEMPORARY PAVEMENT.

CONSTRUCT ONE (1) LANE OF PCC PAVEMENT ON SOUTHBOUND LILY CACHE ROAD.

SUGGESTED STAGE 2C AT LILY CACHE SHALL NOT ALLOW TRUCK TRAFFIC TO MAKE A LEFT TURN FROM THE WB TURN LANE DURING THE STAGE. THE CONTRACTOR SHALL CONTACT THE BUSINESSES AT THE NORTHWEST CORNER OF LILY CACHE ROAD AND US ROUTE 30 TWO (2) WEEKS IN ADVANCE OF THE STAGE TO COORDINATE TRUCK TRAFFIC AND DELIVERIES. STAGE 2C AT LILLY CACHE ROAD SHALL NOT EXCEED ONE (1) WEEK IN DURATION.

STAGE 2D  
SET UP TRAFFIC CONTROL STRIPING, SIGNS, AND DRUMS FOR LILY CACHE ROAD WITH TWO LANES OF TRAFFIC (ONE THRU LANE IN EACH DIRECTION). BOTH LANES TO BE SHIFTED EAST ON NEWLY CONSTRUCTED PAVEMENT.

REMOVE TEMPORARY PAVEMENT ALONG WEST SIDE OF LILY CACHE ROAD.

CONSTRUCT PROPOSED PAVEMENT, CURB AND GUTTER, DRIVEWAYS, AND BIKE PATH ON SOUTHBOUND SIDE.

ADJUST THE EXISTING DRAINAGE STRUCTURES ON WEST SIDE OF LILY CACHE ROAD IN THE DITCH LINE TO FINAL GRADE.

STAGE 2 FINAL CONFIGURATION & STAGE 3  
SET UP TRAFFIC CONTROL STRIPING, SIGNS AND DRUMS FOR LILY CACHE ROAD WITH THREE (3) LANES OF TRAFFIC (ONE SOUTHBOUND THRU LANE, ONE DESIGNATED RIGHT AND LEFT TURNS LANES FOR NORTHBOUND LILY CACHE) ONCE STAGE 2D IS COMPLETED FOR LILY CACHE ROAD.

TRAFFIC SHALL REMAIN IN THIS CONFIGURATION THROUGH STAGE 3. THE CONTRACTOR CAN PLACE FINAL PAVEMENT MARKINGS ALONG LILY CACHE ROAD ONCE STAGE 3 IS COMPLETE.

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DEPARTMENT OF TRANSPORTATION

LILY CACHE ROAD  
SUGGESTED TRAFFIC CONTROL TYPICAL SECTIONS

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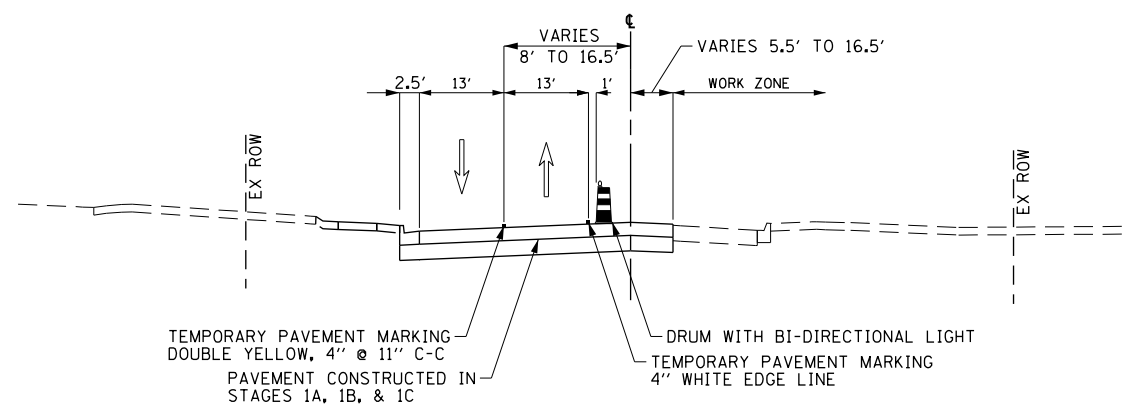
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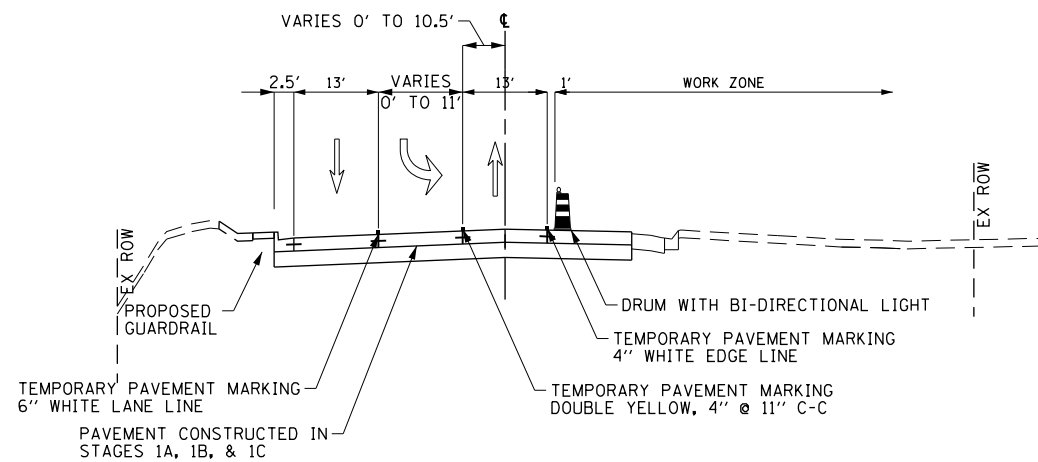
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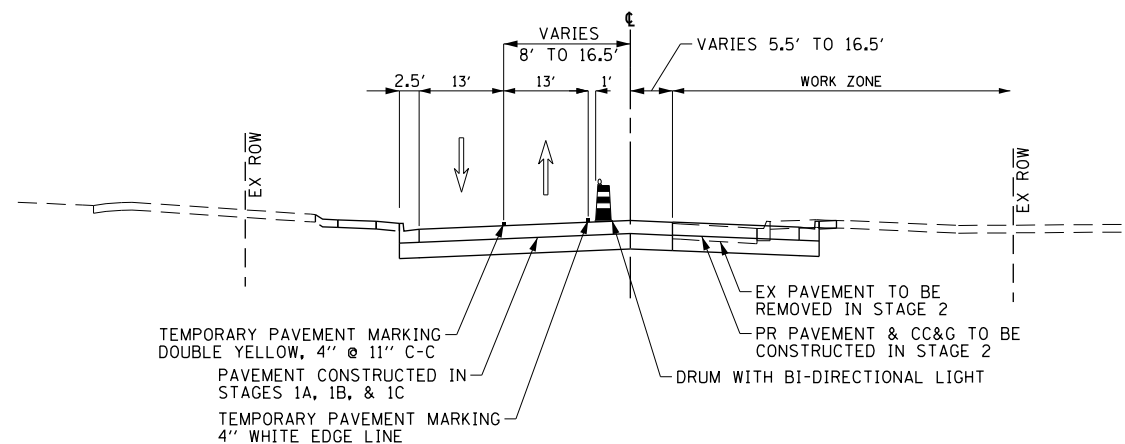
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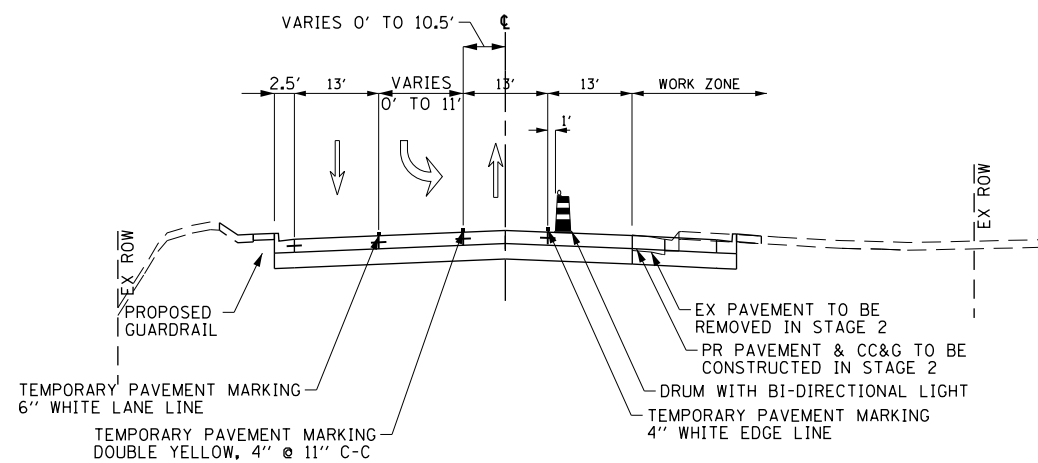
RENWICK ROAD PRE-STAGE 2  
STATION 9+11 TO 21+44



RENWICK ROAD PRE-STAGE 2  
STATION 21+44 TO 30+03



RENWICK ROAD STAGE 2  
STATION 9+11 TO 21+44



RENWICK ROAD STAGE 2  
STATION 21+44 TO 30+03

PRE-STAGE 2  
SET UP TRAFFIC CONTROL STRIPING, SIGNS AND DRUMS FOR RENWICK ROAD WITH TWO LANES OF TRAFFIC (ONE THRU LANE IN EACH DIRECTION). WESTBOUND RENWICK ROAD SHALL HAVE A DESIGNATED LEFT TURN LANE AT THE INTERSECTION.

SHIFT US 30 TRAFFIC ALONG SOUTH LEG ONTO NEWLY BUILT PAVEMENT TO AVOID WORK ZONE.

OPEN NORTH LEG OF US 30 TO TRAFFIC. SHIFT THE TRAFFIC TO THE EAST TO AVOID WORK ZONE.

CONSTRUCT TEMPORARY PAVEMENT AS SHOWN IN THE PLANS AND DETAILS, INCLUDING VARIABLE DEPTH TEMPORARY PAVEMENT ALONG US 30 THAT WILL BE UTILIZED AS A LEFT TURN LANE IN STAGE 2.

STAGE 2  
SET UP TRAFFIC CONTROL STRIPING, SIGNS AND DRUMS FOR RENWICK ROAD WITH TWO LANES OF TRAFFIC (ONE THRU LANE IN EACH DIRECTION). WESTBOUND RENWICK ROAD SHALL HAVE A DESIGNATED LEFT TURN LANE AT THE INTERSECTION.

CONSTRUCT PAVEMENT, CC&G, SIDEWALK, AND DRAINAGE ON EASTBOUND RENWICK ROAD.

REMOVE THE TEMPORARY PAVEMENT ALONG THE SOUTH SIDE OF RENWICK ROAD THAT WAS USED IN PREVIOUS STAGES.



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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

RENWICK ROAD  
SUGGESTED TRAFFIC CONTROL TYPICAL SECTIONS

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CONTRACT NO. 60P95				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				



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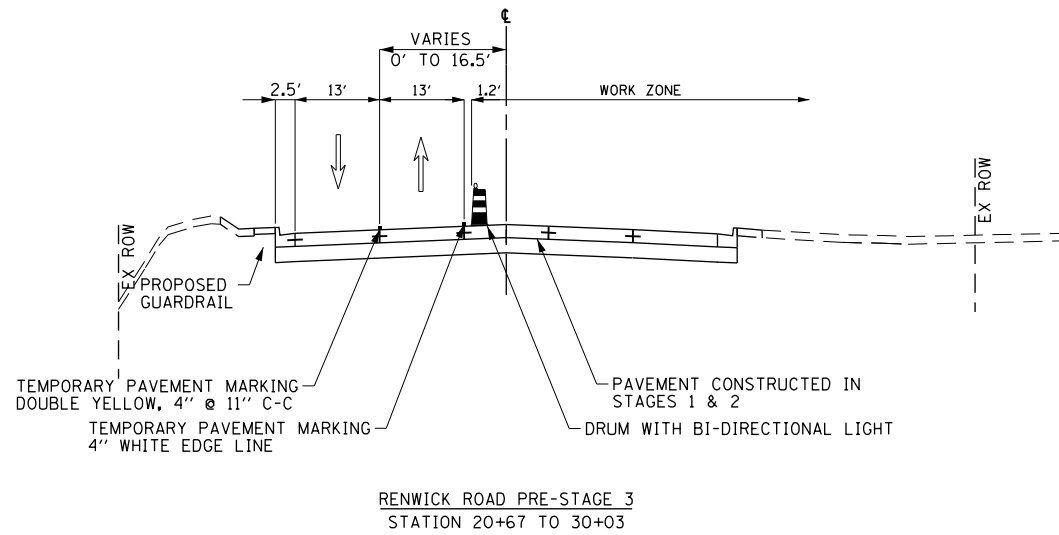
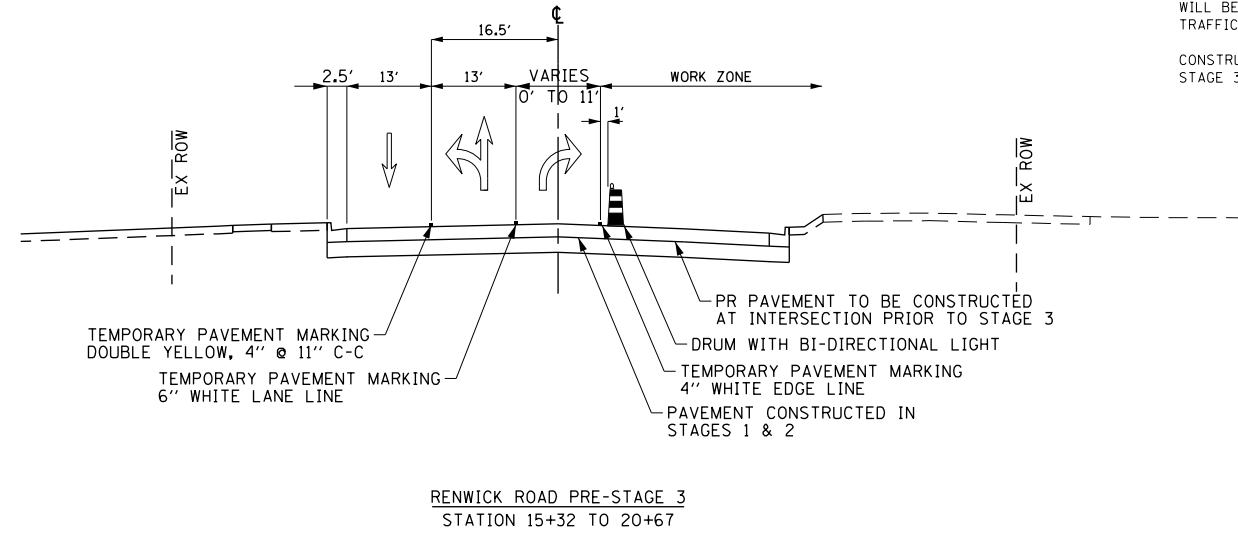
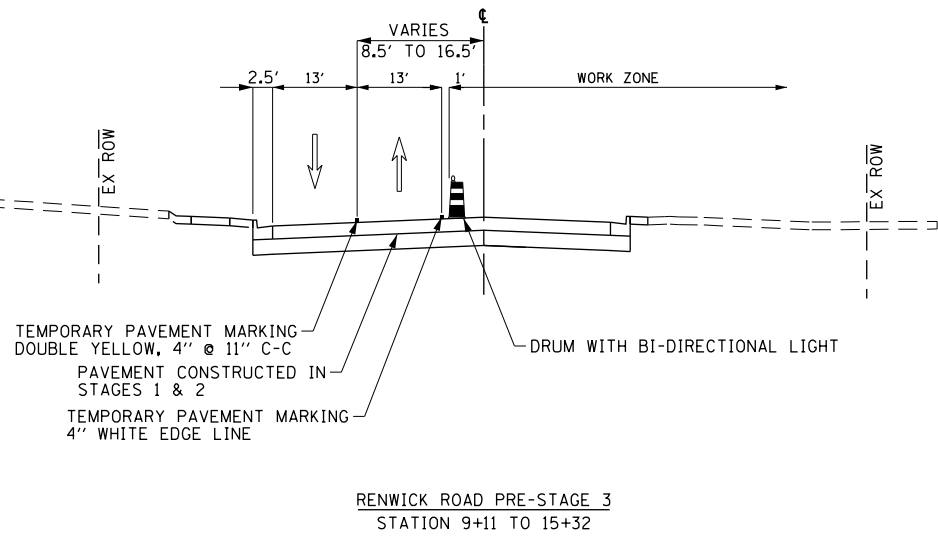
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**PRE-STAGE 3**  
 SET UP TRAFFIC CONTROL STRIPING, SIGNS AND DRUMS FOR RENWICK ROAD WITH TWO LANES OF TRAFFIC (ONE THRU LANE IN EACH DIRECTION). EASTBOUND RENWICK ROAD SHALL HAVE A DESIGNATED RIGHT TURN LANE AT THE INTERSECTION.

SPLIT US 30 TRAFFIC TO AVOID WORK ZONE. A DESIGNATED RIGHT TURN LANE WILL BE PROVIDED ON THE WESTBOUND SIDE OF US 30. ONE THROUGH LANE IN EACH DIRECTION WILL BE PROVIDED ON THE EASTBOUND SIDE OF US 30. ON THE NORTH LEG OF US 30 THE TRAFFIC WILL BE SHIFTED TO THE EASTBOUND SIDE OF THE ROADWAY.

CONSTRUCT THE LEFT TURN LANE ALONG THE SOUTH LEG OF US 30 PRIOR TO STARTING STAGE 3.



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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

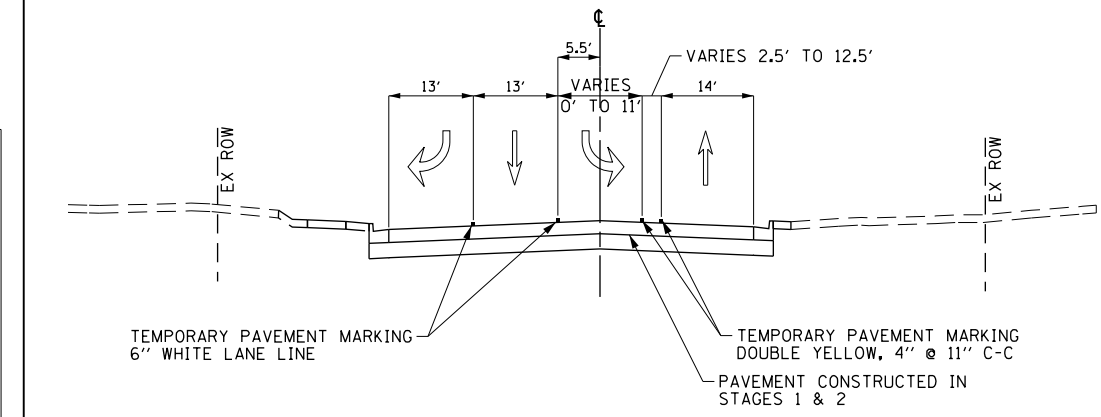
**RENWICK ROAD  
 SUGGESTED TRAFFIC CONTROL TYPICAL SECTIONS**

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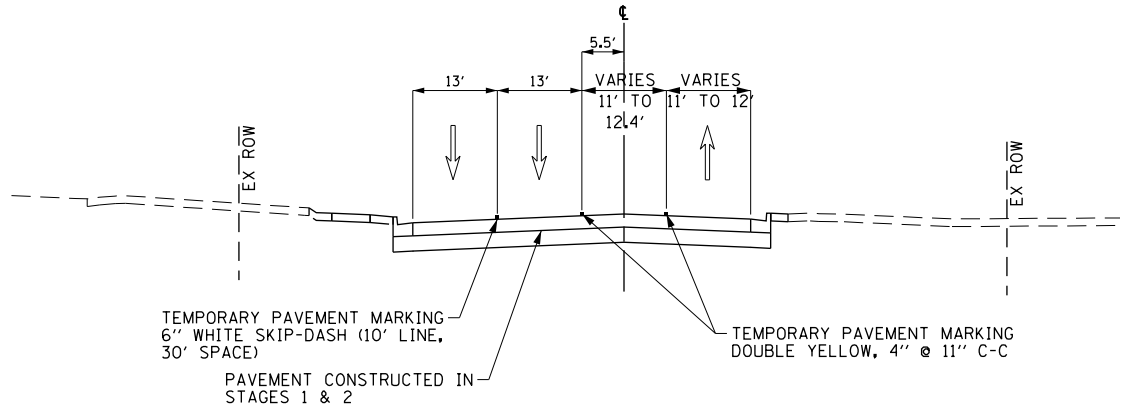
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			CONTRACT NO. 60P95	
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

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GRADES CHECKED	
STRUCTURE NOTATIONS CHECKED	
NOTE BOOK NO.	
CADD FILE NAME	

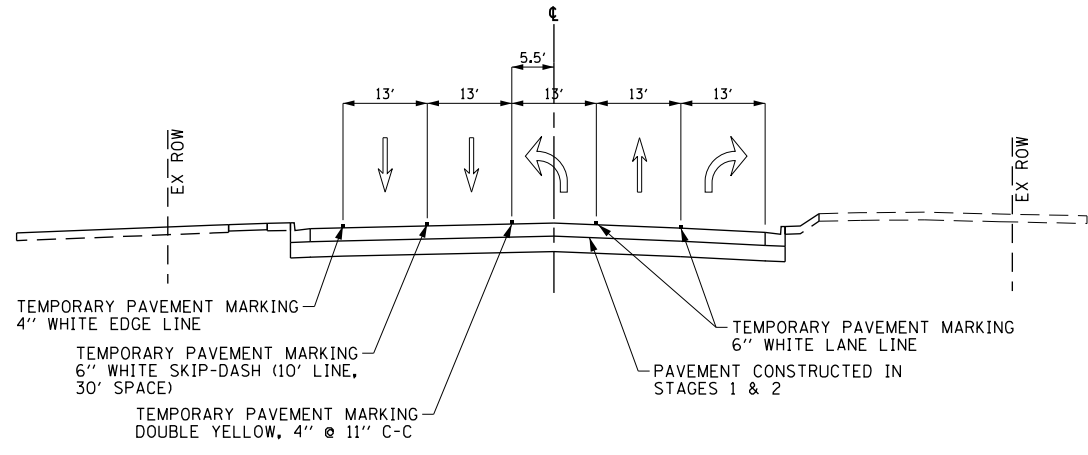
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NOTE BOOK NO.	
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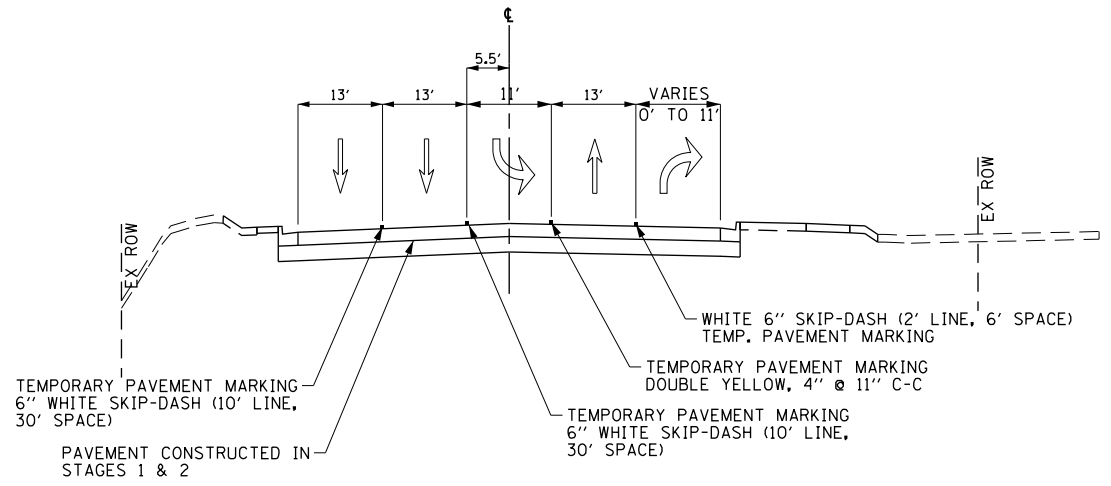
RENWICK ROAD STAGE 3  
STATION 9+11 TO 12+34



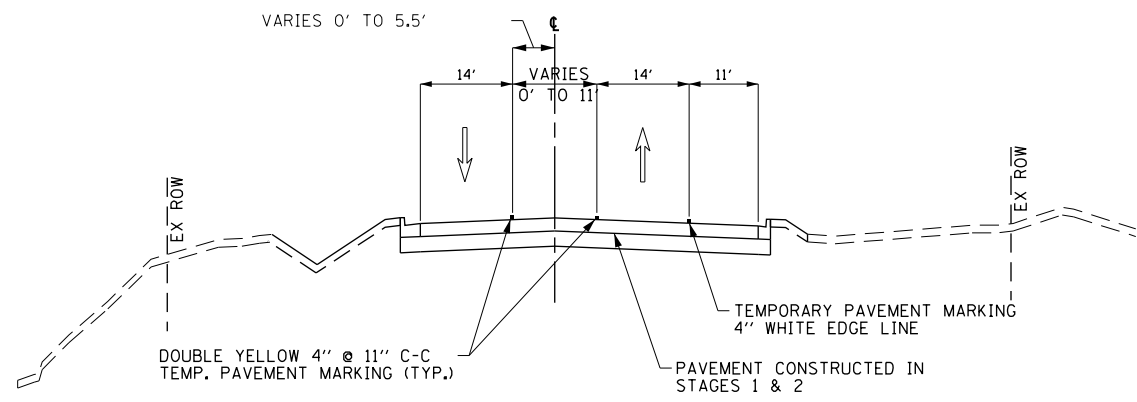
RENWICK ROAD STAGE 3  
STATION 12+34 TO 15+43



RENWICK ROAD STAGE 3  
STATION 15+43 TO 20+00



RENWICK ROAD STAGE 3  
STATION 20+00 TO 23+85



RENWICK ROAD STAGE 3  
STATION 23+85 TO 28+60

STAGE 3  
SET UP TRAFFIC CONTROL STRIPING, SIGNS AND DRUMS FOR RENWICK ROAD WITH TWO (2) LANES OF THRU TRAFFIC IN THE WESTBOUND DIRECTION AND ONE (1) LANE OF THROUGH BOUND TRAFFIC IN THE EASTBOUND DIRECTION. EASTBOUND LANES WILL HAVE A DESIGNATED RIGHT AND LEFT TURN LANES.

SPLIT US 30 TRAFFIC TO AVOID WORK ZONE. A DESIGNATED RIGHT TURN LANE WILL BE PROVIDED ON THE WESTBOUND SIDE OF US 30. ONE THROUGH LANE IN EACH DIRECTION WILL BE PROVIDED ON THE EASTBOUND SIDE OF US 30. ON THE NORTH LEG OF US 30 THE TRAFFIC WILL BE SHIFTED TO THE EASTBOUND SIDE OF THE ROADWAY.

CONSTRUCT THE MEDIANS ALONG US ROUTE 30 AT THE INTERSECTION.

AT THE DIRECTION OF THE ENGINEER, THE CONTRACTOR CAN PLACE FINAL PAVEMENT MARKINGS ALONG RENWICK ROAD PRIOR TO STAGE 3.

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

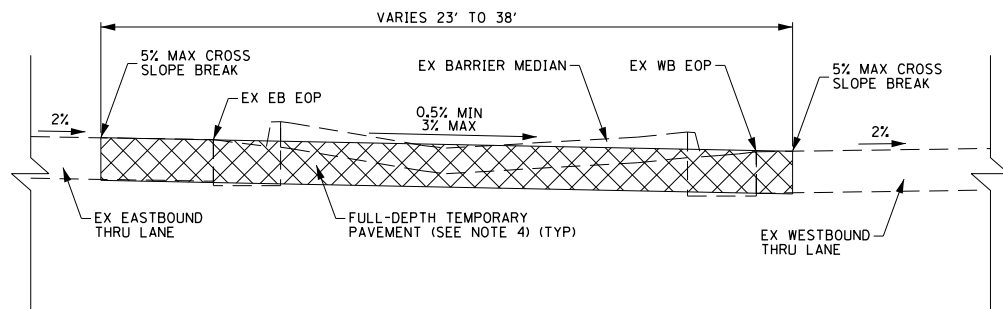
RENWICK ROAD			
SUGGESTED TRAFFIC CONTROL TYPICAL SECTIONS			
SCALE: NTS	SHEET NAME: MOT-TYP-09	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	110
CONTRACT NO. 60P95				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

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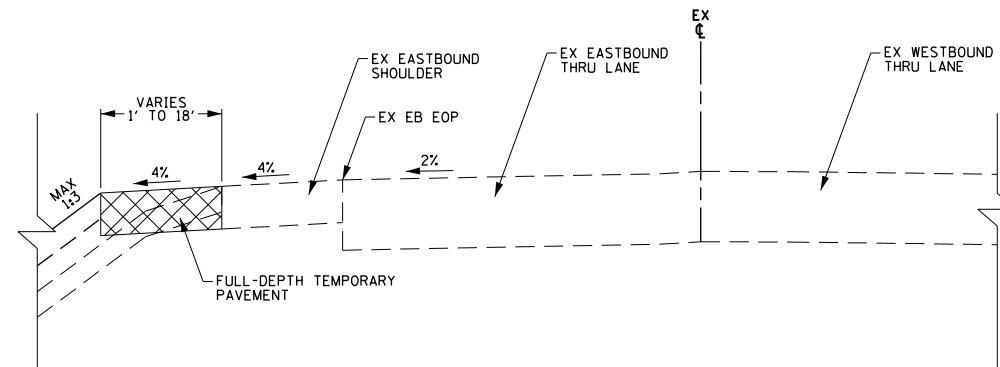
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PROFILE	
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PRE-STAGE 1 TEMPORARY PAVEMENT TYPICAL SECTION  
U.S. ROUTE 30  
STATION 3654+20 TO STATION 3663+20

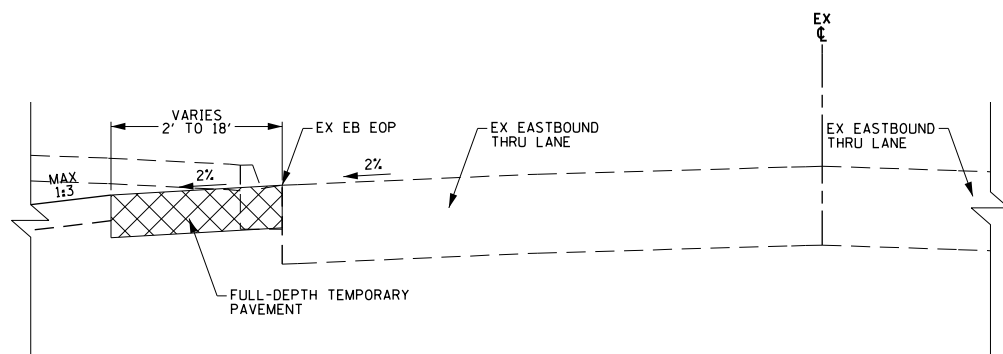
**NOTE:**  
THIS CONDITION REPRESENTS THE AREA WHERE THE MEDIAN MUST BE REPLACED WITH TEMPORARY PAVEMENT IN PRE-STAGE 1 TO ACCOMMODATE CROSS-OVER LANES. IN STAGE 1, THE TEMPORARY PAVEMENT SHALL BE SLOPED AT A MAXIMUM OF 5% AND A MINIMUM OF 0.5%.



PRE-STAGE 1 TEMPORARY PAVEMENT TYPICAL SECTION  
U.S. ROUTE 30  
STATION 3667+57 TO STATION 3674+72  
STATION 3680+53 TO STATION 3685+20  
STATION 3686+15 TO STATION 3687+61  
STATION 3694+15 TO STATION 3696+49

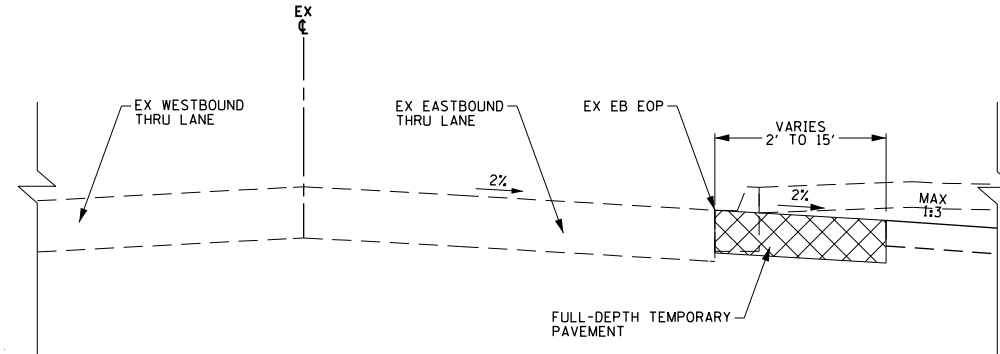
**NOTE:**  
THIS CONDITION REPRESENTS THE AREAS WHERE THE PRE-STAGE 1 TEMPORARY PAVEMENT CAN BE TIED INTO THE EXISTING EB SHOULDER. THE TEMPORARY PAVEMENT SHOULD HAVE A SLOPE OF 4% TO MATCH THE EXISTING EB SHOULDER SLOPE.

- TEMPORARY PAVEMENT GENERAL NOTES**
1. THE DROP OFF POLICY (SAFETY 4-08, UPDATED 8/27/10) MUST BE FOLLOWED FOR ALL STAGES.
  2. IN ALL CONDITIONS, THE MAXIMUM ROLL-OVER IS 5% WITHIN THE TRAVELEDWAY.
  3. TEMP. PAVEMENT SHALL BE CONSTRUCTED 2' BEYOND EDGE OF TRAVELED LANE.
  4. FULL-DEPTH TEMP. PAVEMENT SHALL HAVE A THICKNESS OF 8 INCHES OF PCC OR 10 INCHES OF HMA (2" SURFACE OVER 8" BINDER), WITH A 4 INCH BASE OF AGGREGATE SUBGRADE IMPROVEMENT, WHICH IS INCLUDED IN THE COST OF TEMPORARY PAVEMENT.
  5. THE PLACEMENT AND REMOVAL OF ALL TEMPORARY RAMPS SHALL BE INCLUDED IN THE COST OF PAY ITEM TEMPORARY PAVEMENT.
  6. ANY AGGREGATE FILL REQUIRED FOR THE CONSTRUCTION OF TEMPORARY PAVEMENT (VARIABLE DEPTH) OR TEMPORARY RAMPS SHALL BE INCLUDED IN THE UNIT COST OF THE RESPECTIVE PAY ITEM, AND SHALL NOT BE PAID FOR SEPARATELY.



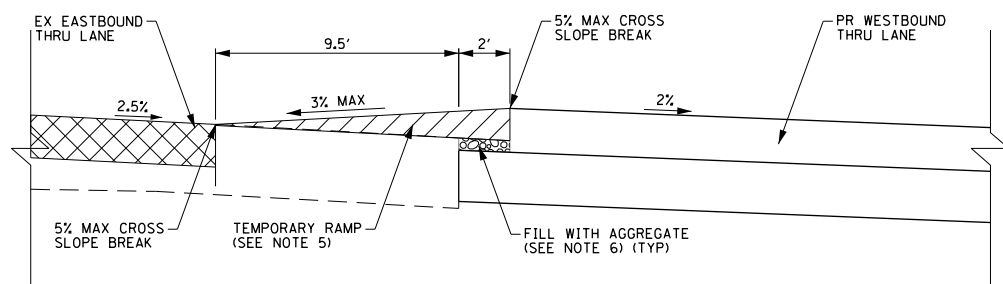
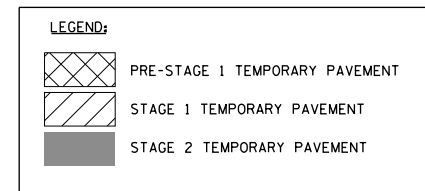
PRE-STAGE 1 TEMPORARY PAVEMENT TYPICAL SECTION  
U.S. ROUTE 30  
STATION 3658+82 TO STATION 3663+54  
STATION 3709+88 TO STATION 3714+16

**NOTE:**  
THIS CONDITION REPRESENTS THE AREAS WHERE THE PRE-STAGE 1 TEMPORARY PAVEMENT CAN BE TIED INTO THE EXISTING EB EOP. THE TEMPORARY PAVEMENT SHOULD HAVE A SLOPE OF 2% TO MATCH THE EXISTING EB PAVEMENT.



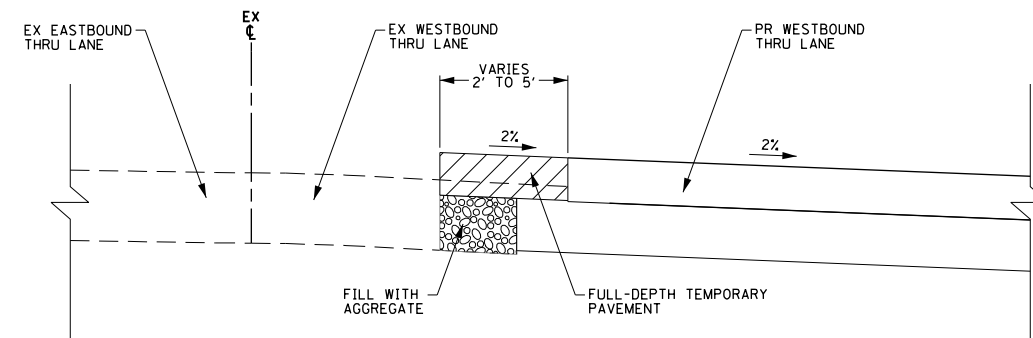
PRE-STAGE 1 TEMPORARY PAVEMENT TYPICAL SECTION  
RENWICK ROAD  
STATION 9+45 TO STATION 16+24  
STATION 16+68 TO STATION 19+96  
STATION 20+88 TO STATION 28+59

**NOTE:**  
THIS CONDITION REPRESENTS THE AREAS WHERE THE PRE-STAGE 1 TEMPORARY PAVEMENT CAN BE TIED INTO THE EXISTING EB EOP. THE TEMPORARY PAVEMENT SHOULD HAVE A SLOPE OF 2% TO MATCH THE EXISTING EB EOP.



STAGE 1 TEMPORARY PAVEMENT TYPICAL SECTION  
U.S. ROUTE 30  
STATION 3658+12 TO STATION 3658+27

**NOTE:**  
THIS CONDITION REPRESENTS THE AREAS WHERE A TEMPORARY RAMP WILL BE USED TO CONNECT THE NEWLY CONSTRUCTED WB PAVEMENT WITH THE PRE-STAGE 1 TEMPORARY PAVEMENT TO ALLOW THE CROSS-OVER MOVEMENT IN STAGE 2. A MAX SLOPE OF 3% WILL BE USED TO ENSURE A MAX CROSS SLOPE BREAK OF 5%.



STAGE 1 TEMPORARY PAVEMENT TYPICAL SECTION  
U.S. ROUTE 30  
STATION 3664+41 TO STATION 3665+22

**NOTE:**  
THIS CONDITION REPRESENTS THE AREAS WHERE FULL-DEPTH TEMPORARY PAVEMENT WILL BE USED TO CONNECT THE NEWLY CONSTRUCTED WB PAVEMENT WITH THE EXISTING EB PAVEMENT TO ALLOW TURNING MOVEMENTS ONTO WEST FRONTAGE ROAD. THE SLOPE OF THE TEMPORARY PAVEMENT WILL BE 2% TO MATCH THE NEWLY CONSTRUCTED PAVEMENT.



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DEPARTMENT OF TRANSPORTATION

U.S. ROUTE 30  
TEMPORARY PAVEMENT DETAILS

SCALE: NTS SHEET NAME: MOT-TEMP-01 STA. TO STA.

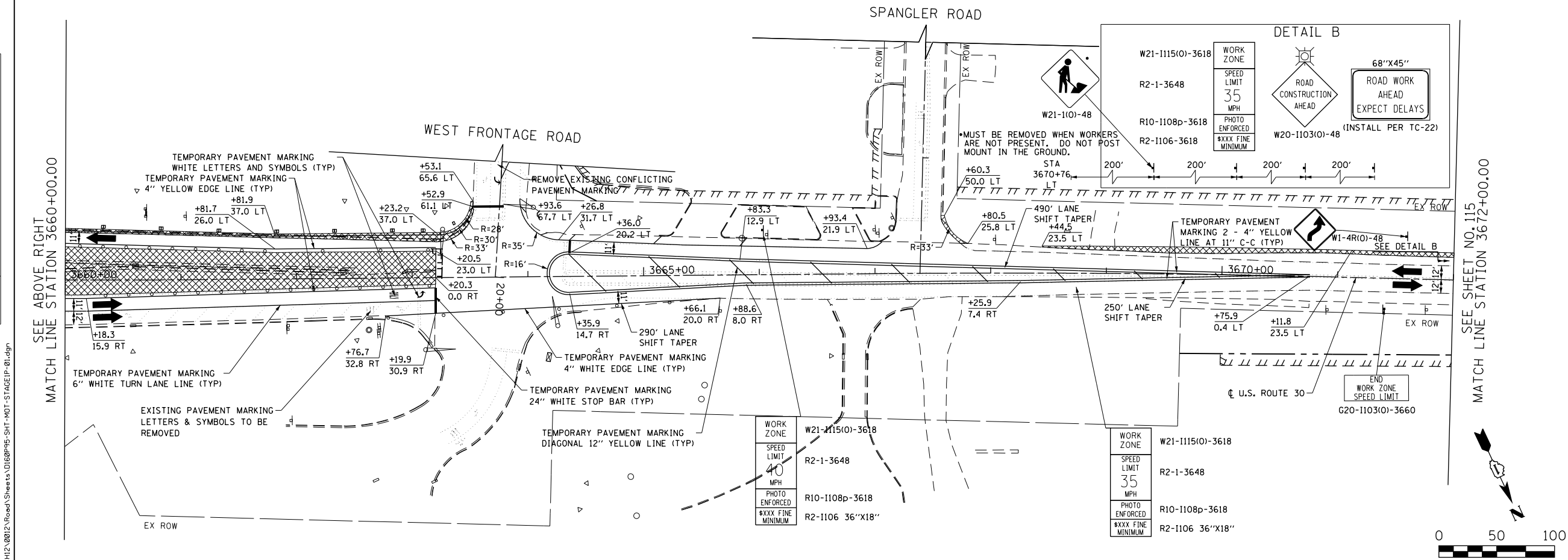
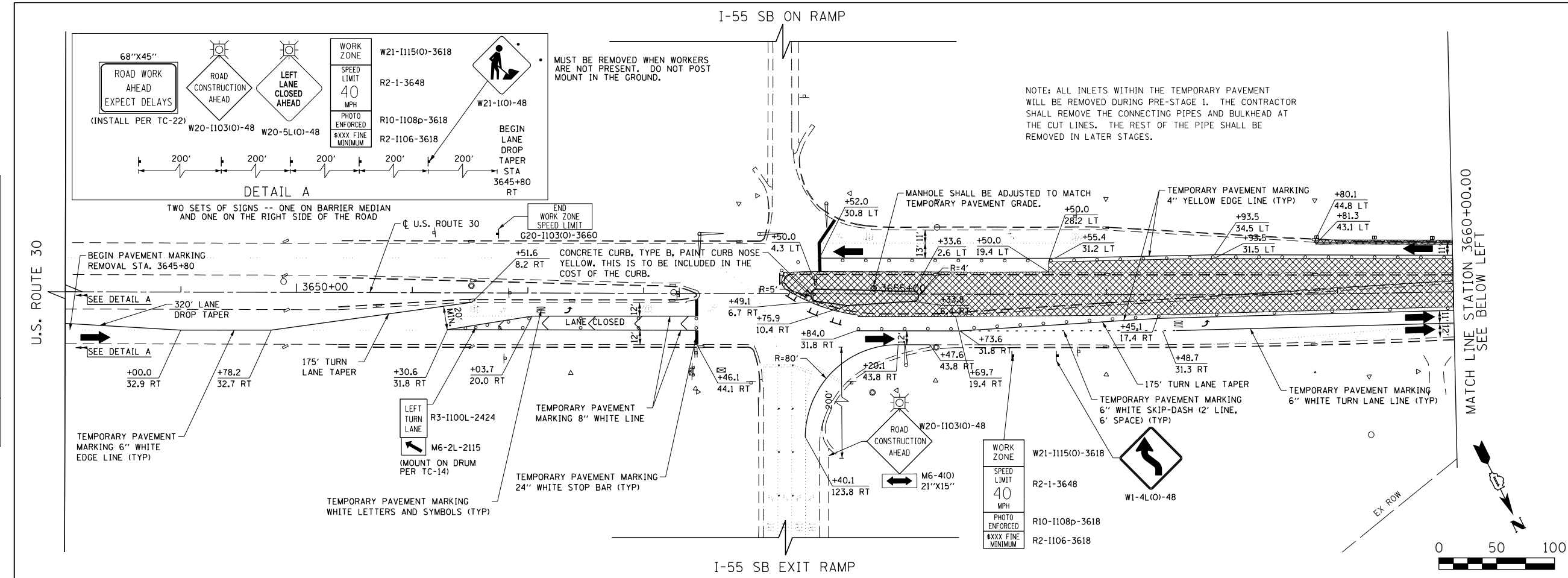
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575	14W - R	WILL	681	111
				CONTRACT NO. 60P95
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				





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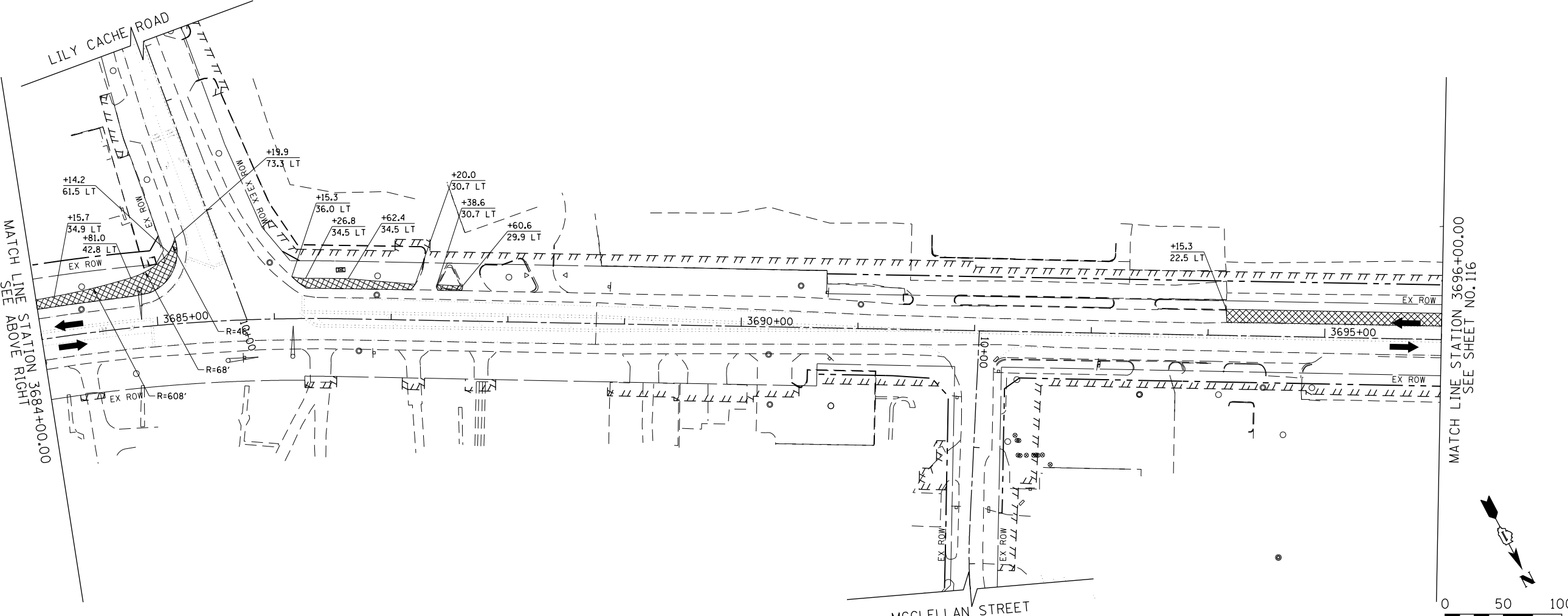
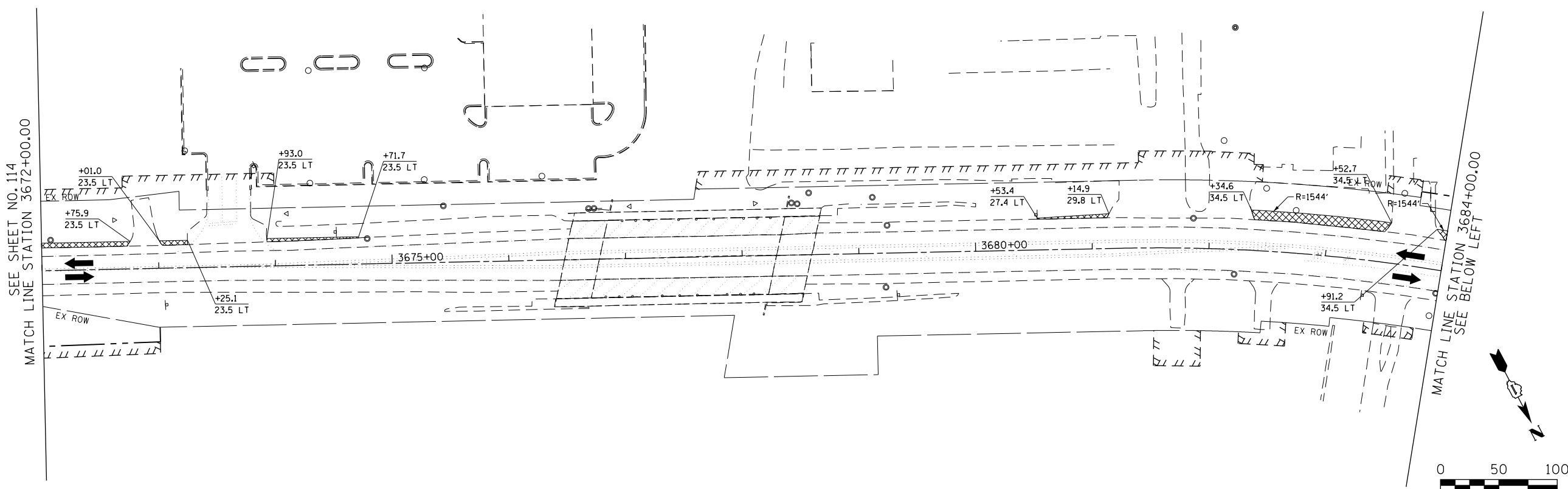
- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID

- DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- R3-2L 24" X 24"
- R3-2R 24" X 24"
- STOP R1-1
- ONE WAY R6-1R 36" X 12"
- ONE WAY R6-1L 36" X 12"

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**LEGEND**

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
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- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID

- 1 DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- 2 DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- 3 R3-2L 24" X 24"
- 4 R3-2R 24" X 24"
- 5 STOP R1-1
- 6 ONE WAY R6-1R 36" X 12"
- 7 ONE WAY R6-1L 36" X 12"



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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

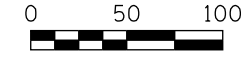
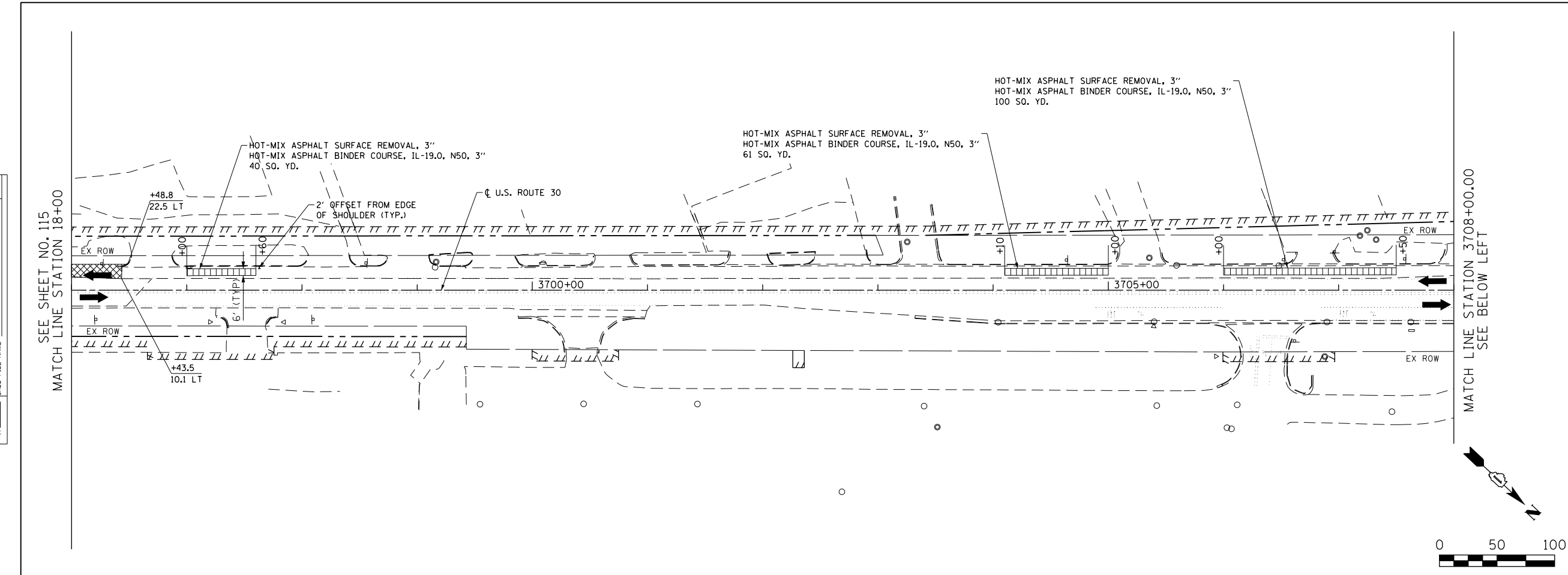
**U.S. ROUTE 30  
SUGGESTED TRAFFIC CONTROL PLANS (PRE-STAGE 1)**

SCALE: 1"=50' SHEET NAME: MOT-1-02 STA. 3672+00 TO STA. 3696+00

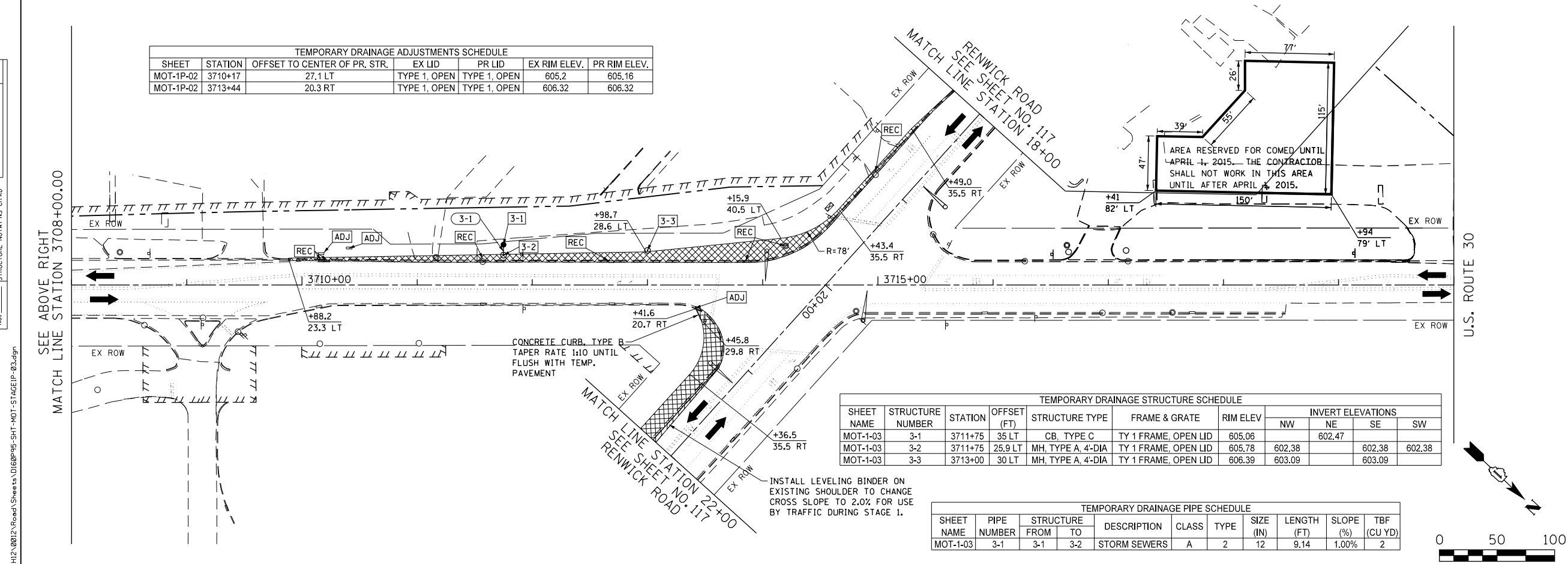
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CONTRACT NO. 60P95				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

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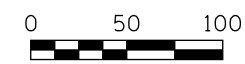


TEMPORARY DRAINAGE ADJUSTMENTS SCHEDULE						
SHEET	STATION	OFFSET TO CENTER OF PR. STR.	EX LID	PR LID	EX RIM ELEV.	PR RIM ELEV.
MOT-1P-02	3710+17	27.1 LT	TYPE 1, OPEN	TYPE 1, OPEN	605.2	605.16
MOT-1P-02	3713+44	20.3 RT	TYPE 1, OPEN	TYPE 1, OPEN	606.32	606.32



TEMPORARY DRAINAGE STRUCTURE SCHEDULE										
SHEET NAME	STRUCTURE NUMBER	STATION	OFFSET (FT)	STRUCTURE TYPE	FRAME & GRATE	RIM ELEV	INVERT ELEVATIONS			
							NW	NE	SE	SW
MOT-1-03	3-1	3711+75	35 LT	CB, TYPE C	TY 1 FRAME, OPEN LID	605.06				
MOT-1-03	3-2	3711+75	25.9 LT	MH, TYPE A, 4'-DIA	TY 1 FRAME, OPEN LID	605.78	602.38	602.47	602.38	602.38
MOT-1-03	3-3	3713+00	30 LT	MH, TYPE A, 4'-DIA	TY 1 FRAME, OPEN LID	606.39	603.09		603.09	

TEMPORARY DRAINAGE PIPE SCHEDULE										
SHEET NAME	PIPE NUMBER	STRUCTURE		DESCRIPTION	CLASS	TYPE	SIZE (IN)	LENGTH (FT)	SLOPE (%)	TBF (CU YD)
		FROM	TO							
MOT-1-03	3-1	3-1	3-2	STORM SEWERS	A	2	12	9.14	1.00%	2



LEGEND

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
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- DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- R3-2L 24" X 24"
- R3-2R 24" X 24"
- STOP R1-1
- ONE WAY R6-1R 36" X 12"
- ONE WAY R6-1L 36" X 12"

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U.S. ROUTE 30  
SUGGESTED TRAFFIC CONTROL PLANS (PRE-STAGE 1)

SCALE: 1"=50' SHEET NAME: MOT-1P-03 STA. 3696+00 TO STA. 3720+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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				CONTRACT NO. 60P95
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

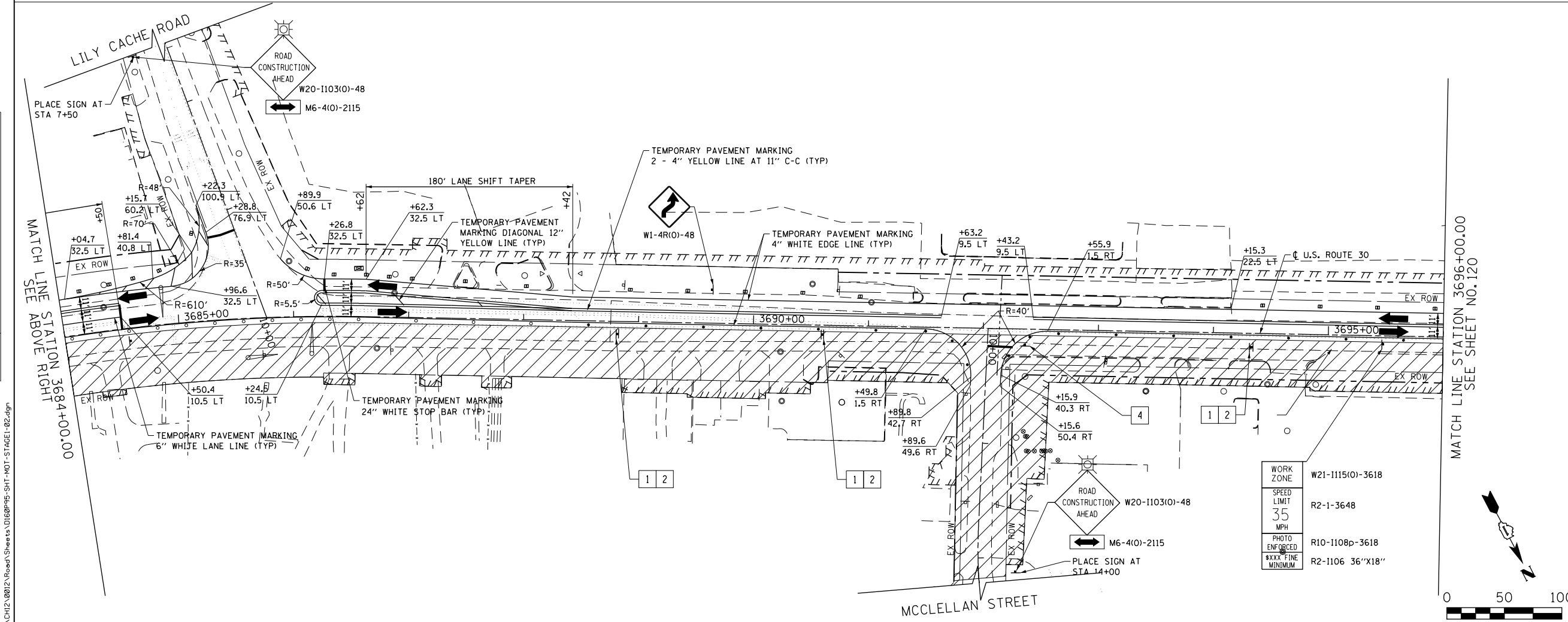
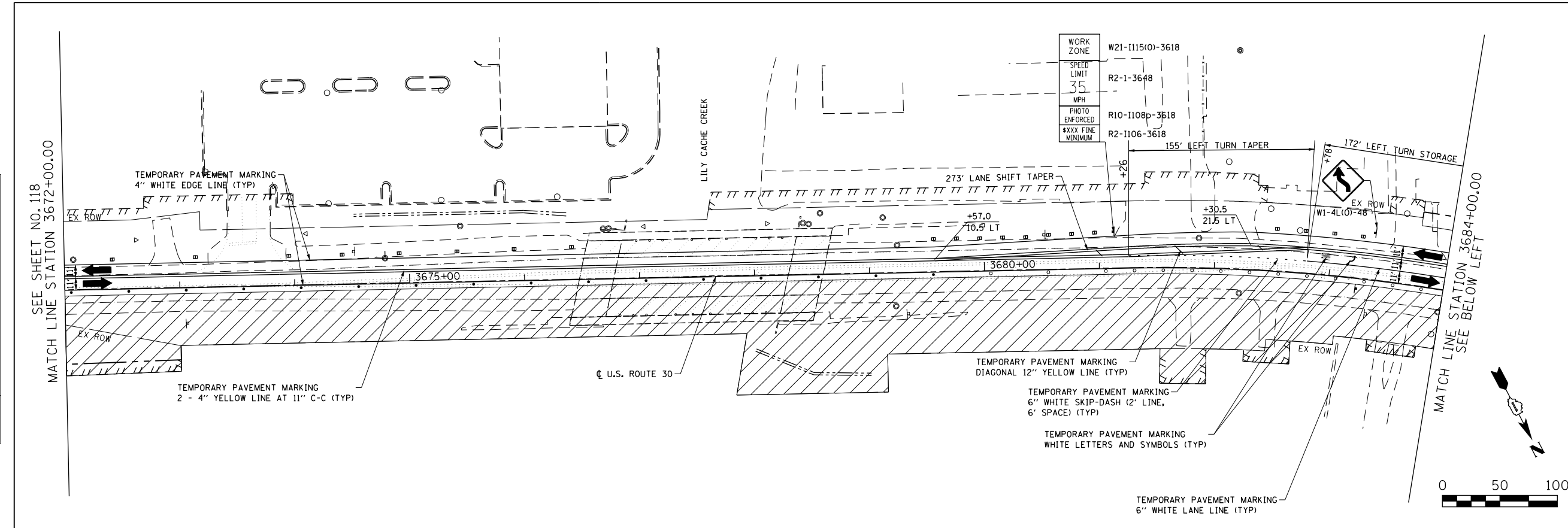






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**LEGEND**

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
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- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID

1	DRIVEWAY ENTRANCE	30" X 30" SEE TC-26
2	DRIVEWAY ENTRANCE	30" X 30" SEE TC-26
3		R3-2L 24" X 24"
4		R3-2R 24" X 24"
5		R1-1
6		R6-1R 36" X 12"
7		R6-1L 36" X 12"



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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

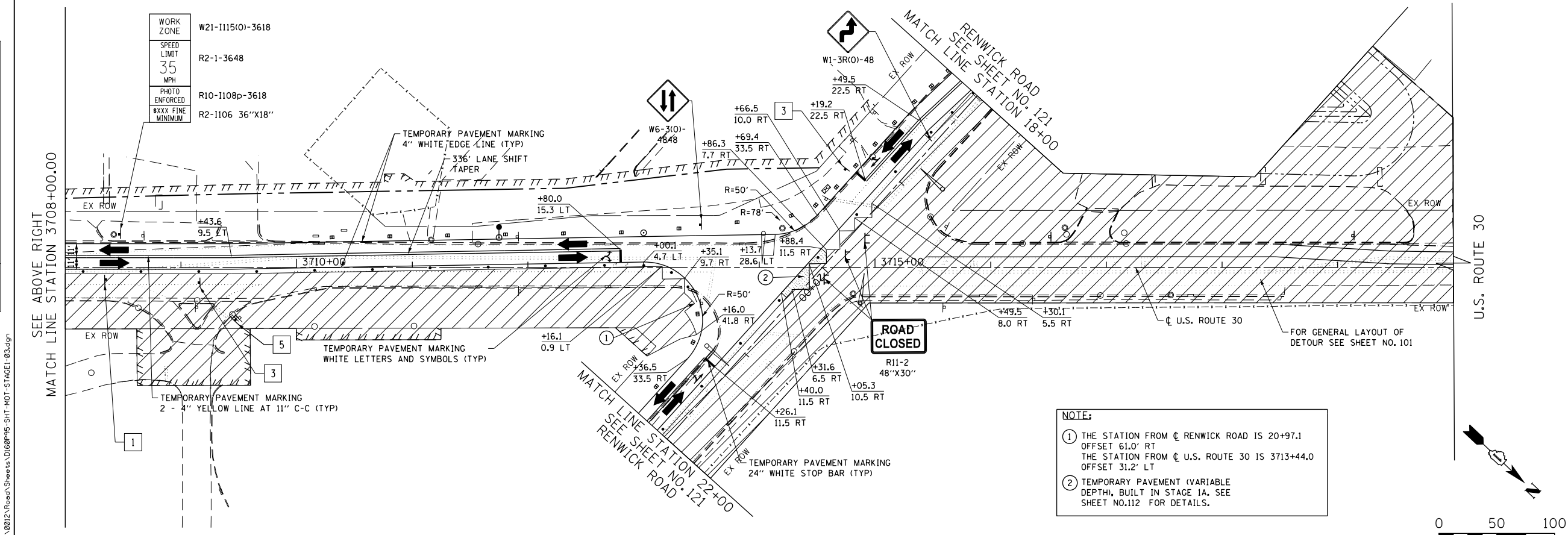
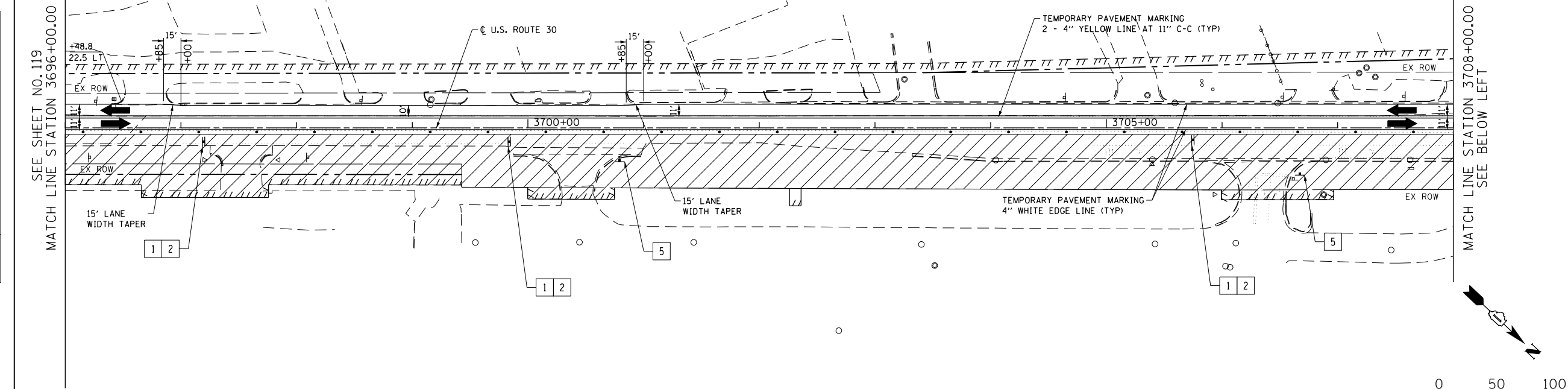
**U.S. ROUTE 30  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 1)**

SCALE: 1"=50'    SHEET NAME: MOT-1-02    STA. 3672+00 TO STA. 3696+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	119
				CONTRACT NO. 60P95
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	BY	
	DATE	

PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	BY	
	DATE	



**NOTE:**

① THE STATION FROM  $\phi$  RENWICK ROAD IS 20+97.1  
 OFFSET 61.0' RT  
 THE STATION FROM  $\phi$  U.S. ROUTE 30 IS 3713+44.0  
 OFFSET 31.2' LT

② TEMPORARY PAVEMENT (VARIABLE DEPTH), BUILT IN STAGE 1A. SEE SHEET NO.112 FOR DETAILS.

**LEGEND**

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT  $\phi$  50" C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT  $\phi$  50" C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT  $\phi$  50" C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT  $\phi$  50" C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID

1 DRIVEWAY ENTRANCE 30" X 30" SEE TC-26

2 DRIVEWAY ENTRANCE 30" X 30" SEE TC-26

3 R3-2L 24" X 24"

4 R3-2R 24" X 24"

5 STOP R1-1

6 ONE WAY R6-1R 36" X 12"

7 ONE WAY R6-1L 36" X 12"



USER NAME = bshoeffler	DESIGNED - MNB	REVISED -
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PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

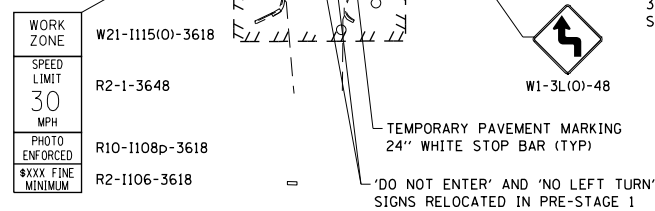
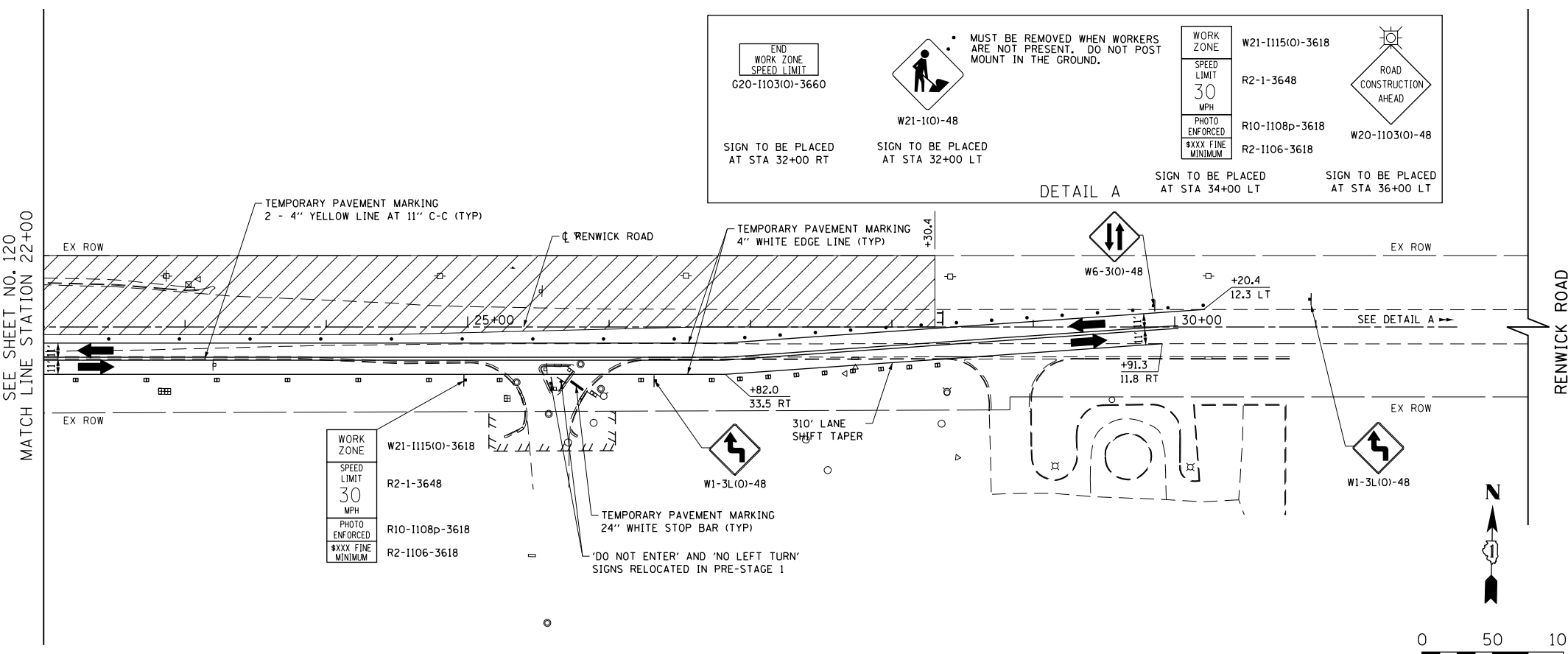
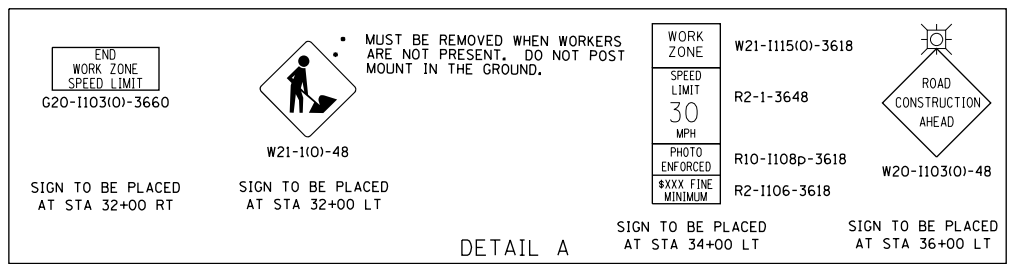
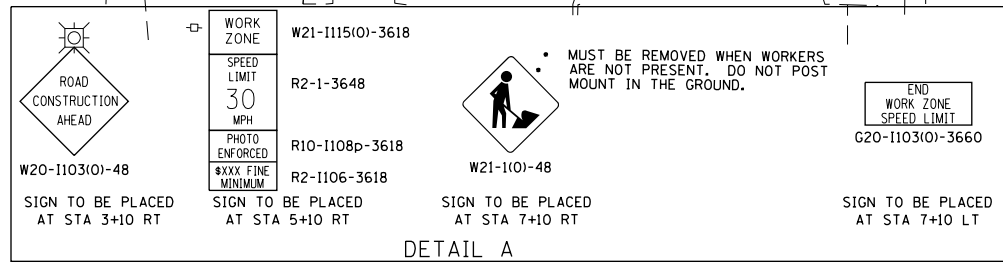
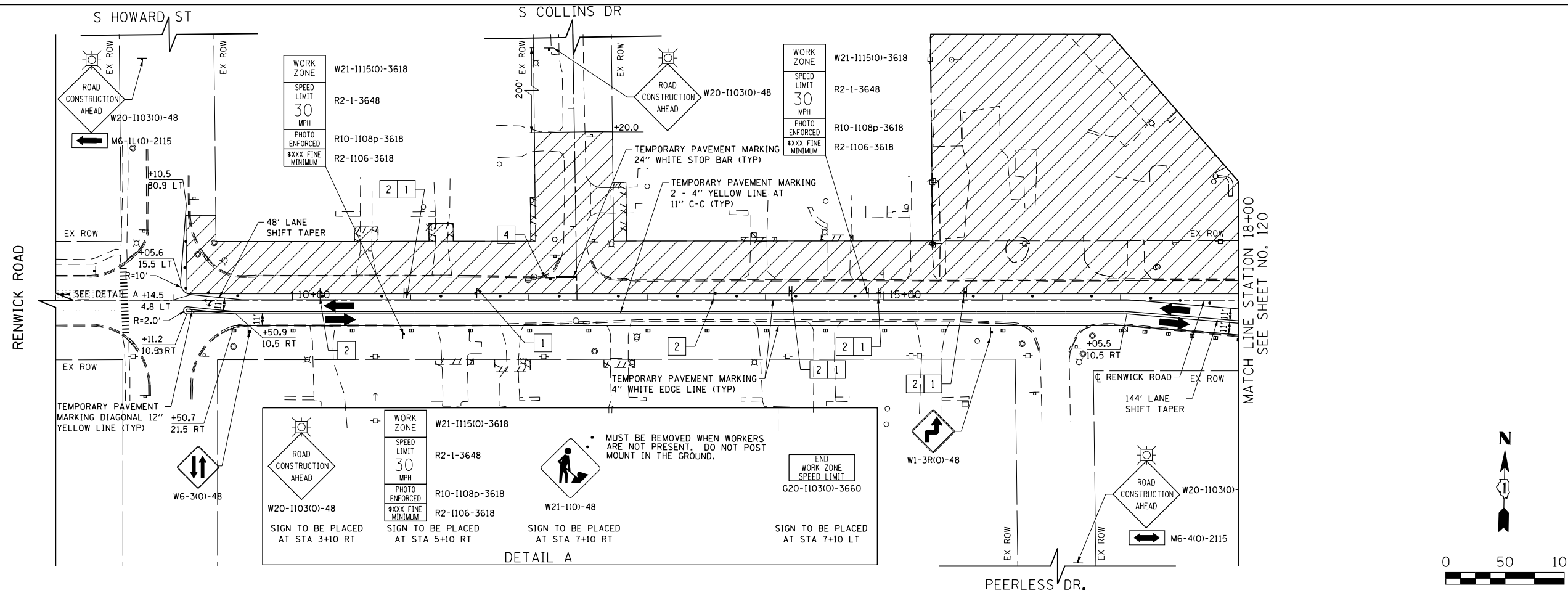
<b>U.S. ROUTE 30 SUGGESTED TRAFFIC CONTROL PLANS (STAGE 1)</b>	
SCALE: 1"=50'	SHEET NAME: MOT-1-03
STA. 3696+00	TO STA. 3720+00

F.A.P. RTE. 575	SECTION 14W - R	COUNTY WILL	TOTAL SHEETS 681	SHEET NO. 120
CONTRACT NO. 60P95				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	BY
	NOTES CHECKED	
	AT OR MAY BE CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NO.	

PROFILE	SURVEYED	DATE
	PLOTTED	BY
	NOTES CHECKED	
	AT OR MAY BE CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NO.	

FILE PATH = G:\CH2\0012\Road\Sheet\1\160P95-SHT-MOT-STAGE1-04.dgn



LEGEND	
	CONSTRUCTION WORK ZONE
	COMPLETED PAVEMENT
	TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
	TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
	TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
	SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
	MONODIRECTIONAL FLASHING LIGHT
	DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
	DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
	TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
	VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
	DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
	DIRECTION OF TRAFFIC FLOW
	DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
	INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
	NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID
	DRIVEWAY ENTRANCE 30' X 30" SEE TC-26
	DRIVEWAY ENTRANCE 30' X 30" SEE TC-26
	R3-2L 24" X 24"
	R3-2R 24" X 24"
	STOP R1-1
	ONE WAY R6-1R 36" X 12"
	ONE WAY R6-1L 36" X 12"



USER NAME = bshoeffler	DESIGNED - MNB	REVISED -
FILE = D:\60P95-SHT-MOT-STAGE1-04.dgn	DRAWN - MNB	REVISED -
PLOT SCALE = 100.0000' / 1\"/>		

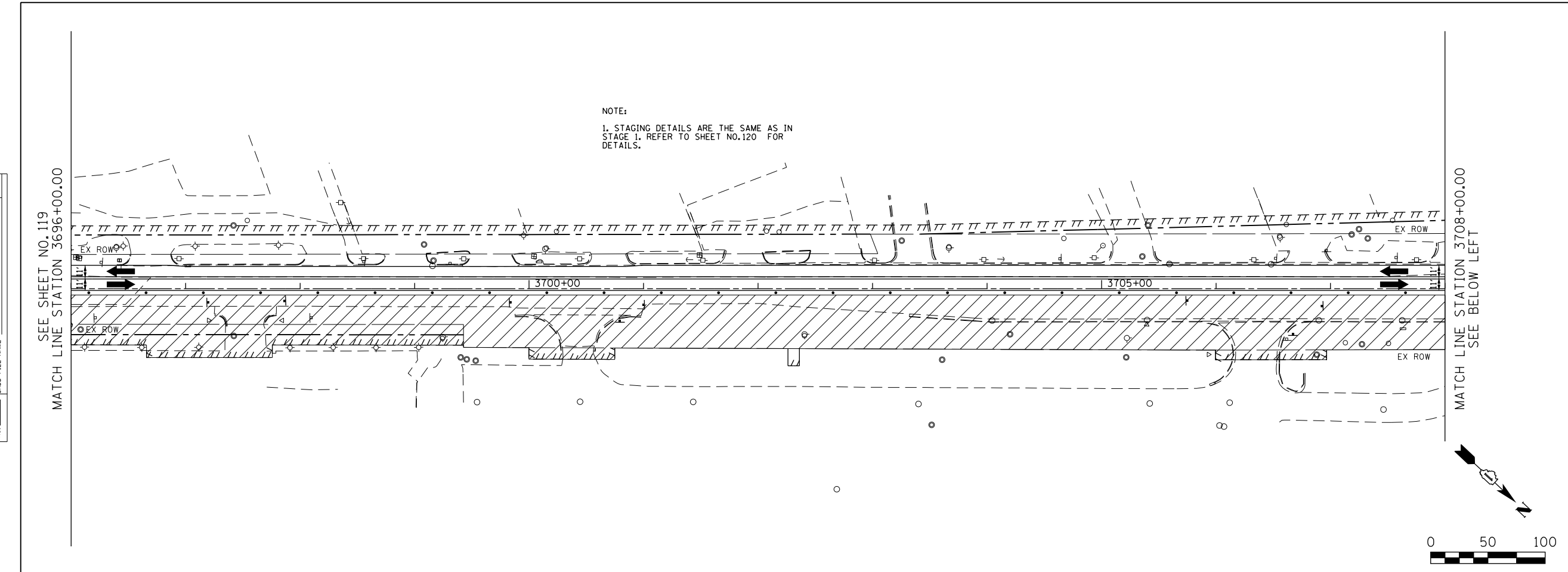
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

RENWICK ROAD SUGGESTED TRAFFIC CONTROL PLANS (STAGE 1)			
SCALE: 1"=50'	SHEET NAME: MOT-1-04	STA. 8+00 TO STA. 32+00	

F.A.P. RTE. 575	SECTION 14W - R	COUNTY WILL	TOTAL SHEETS 681	SHEET NO. 121
CONTRACT NO. 60P95				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	BY
	GRADES CHECKED	
	NOTES CHECKED	
	STRUCTURE NOTATIONS OK'D	
	NO. _____	
	DATE _____	

PROFILE	SURVEYED	DATE
	PLOTTED	BY
	GRADES CHECKED	
	NOTES CHECKED	
	STRUCTURE NOTATIONS OK'D	
	NO. _____	
	DATE _____	



NOTE:  
1. STAGING DETAILS ARE THE SAME AS IN STAGE 1. REFER TO SHEET NO.120 FOR DETAILS.

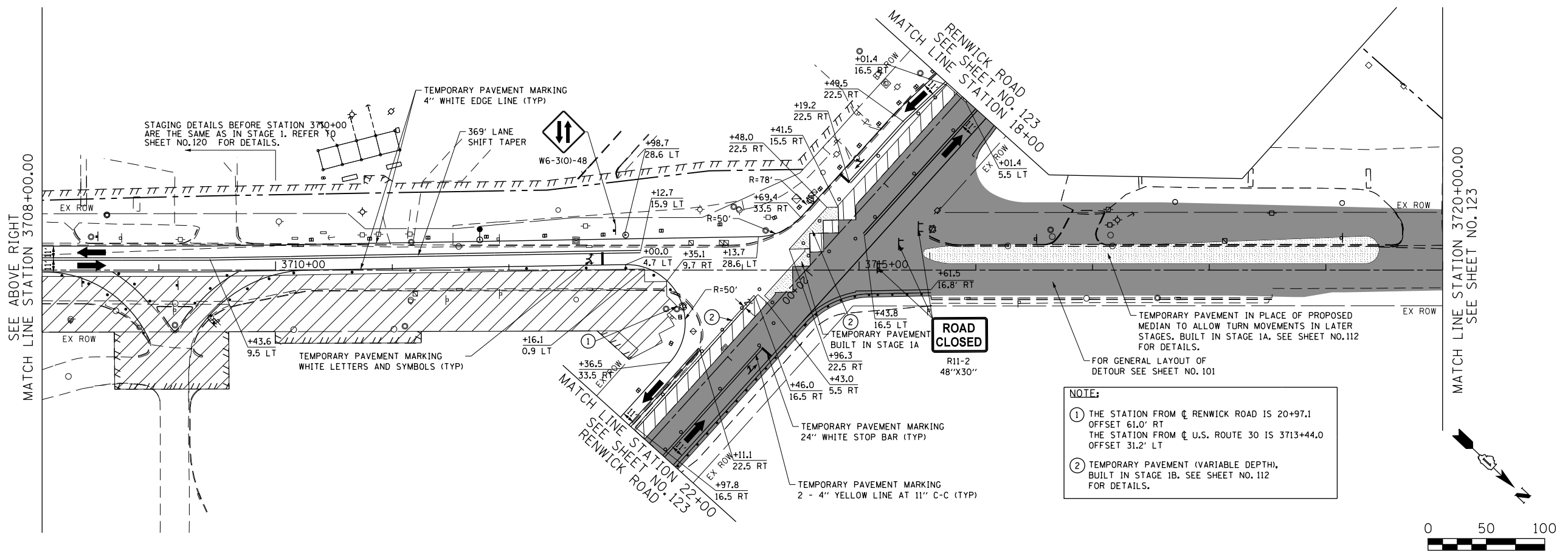
SEE SHEET NO.119  
MATCH LINE STATION 3696+00.00

MATCH LINE STATION 3708+00.00  
SEE BELOW LEFT

- LEGEND**
- CONSTRUCTION WORK ZONE
  - COMPLETED PAVEMENT
  - TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
  - TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
  - TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
  - SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
  - MONODIRECTIONAL FLASHING LIGHT
  - DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
  - DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
  - TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
  - VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
  - DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
  - DIRECTION OF TRAFFIC FLOW
  - DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
  - INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
  - NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID

- DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- R3-2L 24" X 24"
- R3-2R 24" X 24"
- STOP R1-1
- ONE WAY R6-1R 36" X 12"
- ONE WAY R6-1L 36" X 12"

FILE PATH = G:\CH2\0012\Road\Sheet\0160P95-SHT-MOT-STAGE1B-01.dgn



STAGING DETAILS BEFORE STATION 3710+00 ARE THE SAME AS IN STAGE 1. REFER TO SHEET NO.120 FOR DETAILS.

NOTE:  
① THE STATION FROM C RENWICK ROAD IS 20+97.1 OFFSET 61.0' RT. THE STATION FROM C U.S. ROUTE 30 IS 3713+44.0 OFFSET 31.2' LT  
② TEMPORARY PAVEMENT (VARIABLE DEPTH), BUILT IN STAGE 1B. SEE SHEET NO. 112 FOR DETAILS.

SEE ABOVE RIGHT  
MATCH LINE STATION 3708+00.00

MATCH LINE STATION 3720+00.00  
SEE SHEET NO.123



USER NAME = bshoeffler	DESIGNED - MNB	REVISED -
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PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

U.S. ROUTE 30  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 1B)

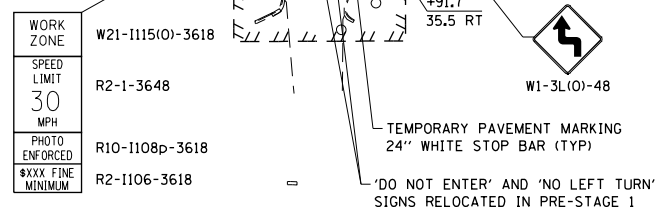
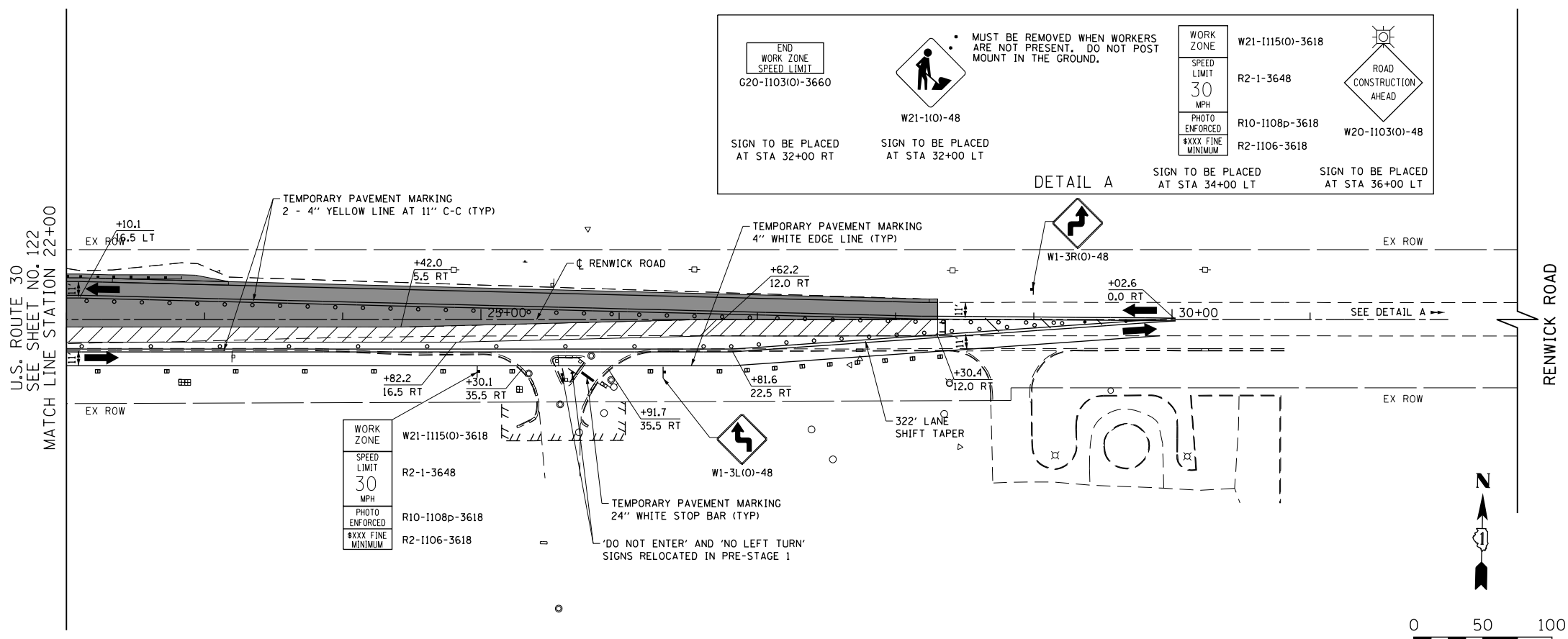
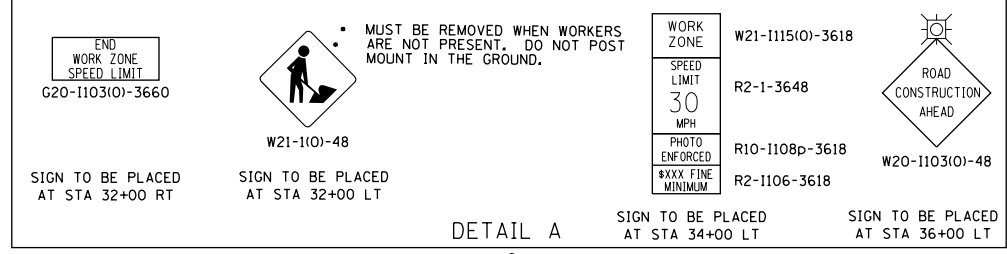
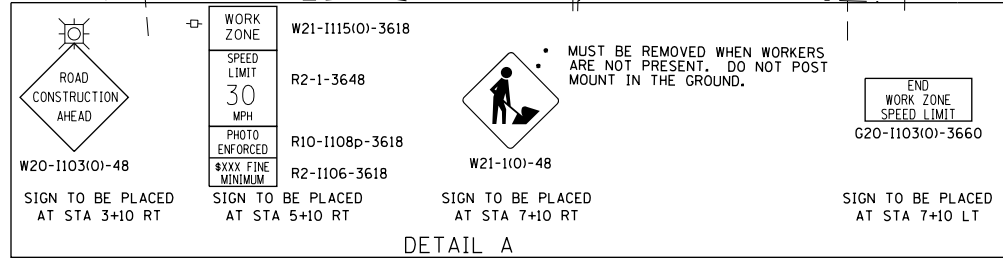
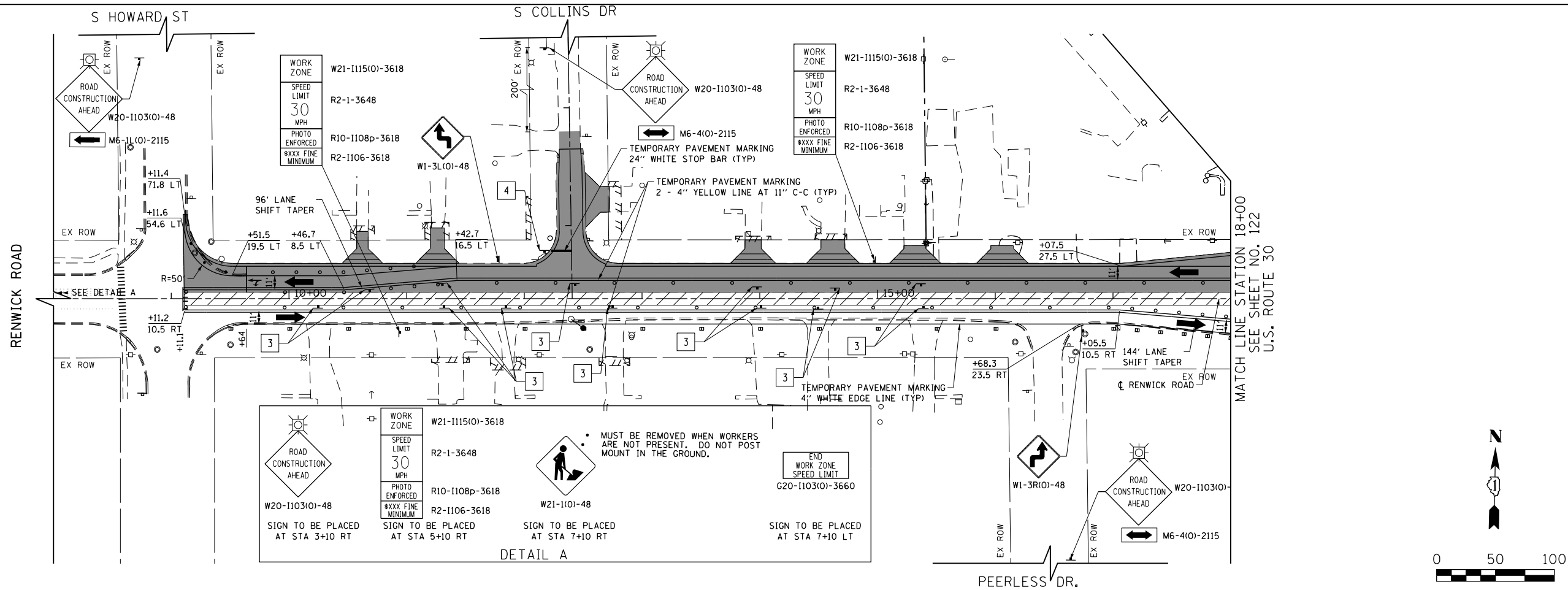
SCALE: 1"=50' SHEET NAME: MOT-1B-01 STA. 3696+00 TO STA. 3720+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	122
				CONTRACT NO. 60P95
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	
	NOTED	
	BY	
	NO.	
	FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	
	NOTED	
	BY	
	NO.	
	FILE NAME	

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**LEGEND**

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID

- DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- R3-2L 24" X 24"
- R3-2R 24" X 24"
- STOP R1-1
- ONE WAY R6-1R 36" X 12"
- ONE WAY R6-1L 36" X 12"



USER NAME = bshoeffler	DESIGNED - MNB	REVISED -
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PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE = 08/18/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**RENWICK ROAD  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 1B)**

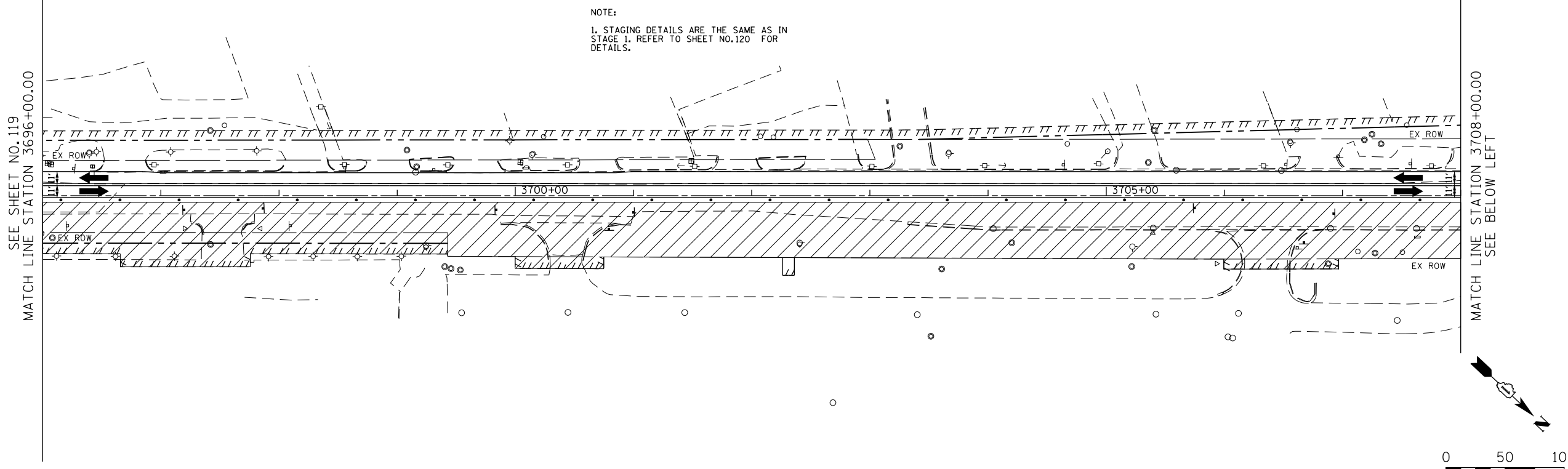
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	123
				CONTRACT NO. 60P95
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

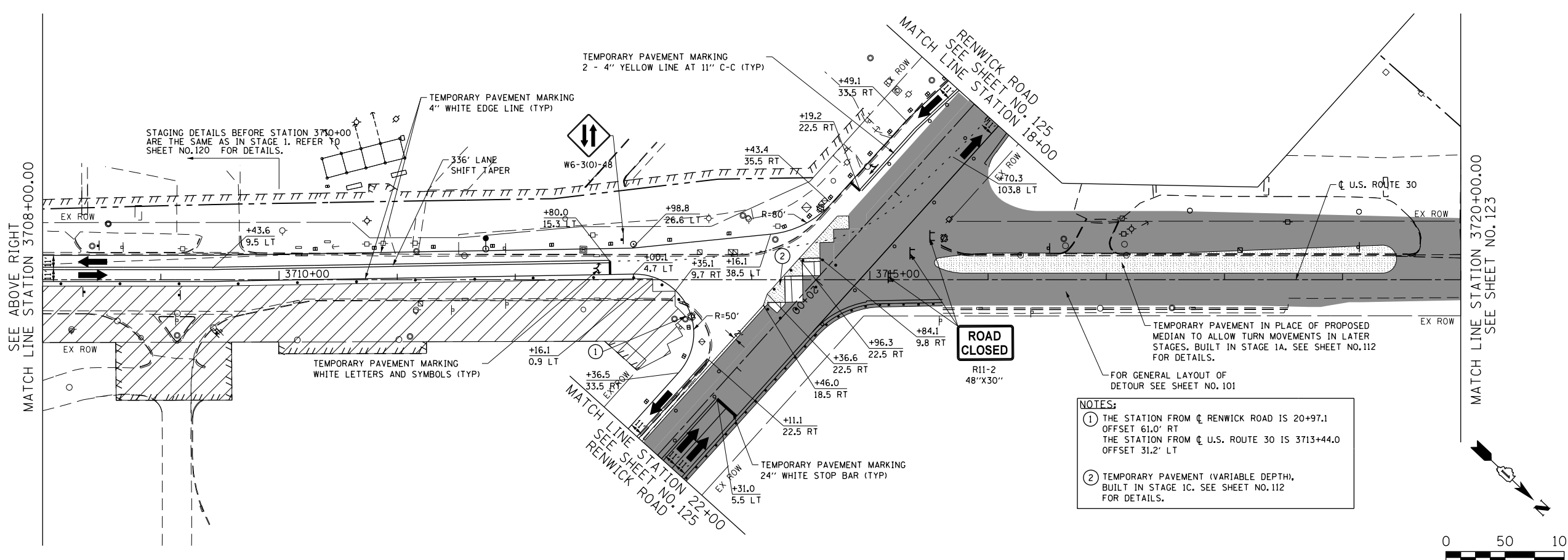
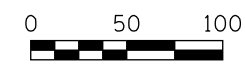
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	PLOTTED	BY
	GRADES CHECKED	
	NOTES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NO. _____	
	DATE _____	

PROFILE	SURVEYED	DATE
	PLOTTED	BY
	GRADES CHECKED	
	NOTES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NO. _____	
	DATE _____	

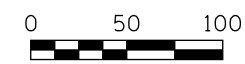
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NOTE:  
1. STAGING DETAILS ARE THE SAME AS IN STAGE 1. REFER TO SHEET NO.120 FOR DETAILS.



NOTES:  
① THE STATION FROM  $\phi$  RENWICK ROAD IS 20+97.1 OFFSET 61.0' RT  
THE STATION FROM  $\phi$  U.S. ROUTE 30 IS 3713+44.0 OFFSET 31.2' LT  
② TEMPORARY PAVEMENT (VARIABLE DEPTH), BUILT IN STAGE 1C. SEE SHEET NO.112 FOR DETAILS.



LEGEND

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT  $\phi$  50" C-C (TYP), 25" C-C IN CURVES AND RADII, AND 20" C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT  $\phi$  50" C-C (TYP), 25" C-C IN CURVES AND RADII, AND 20" C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT  $\phi$  50" C-C (TYP), 25" C-C IN CURVES AND RADII, AND 20" C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT  $\phi$  50" C-C (TYP), 25" C-C IN CURVES AND RADII, AND 20" C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID

- 1 DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- 2 DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- 3 R3-2L 24" X 24"
- 4 R3-2R 24" X 24"
- 5 STOP R1-1
- 6 ONE WAY R6-1R 36" X 12"
- 7 ONE WAY R6-1L 36" X 12"



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PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

U.S. ROUTE 30  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 1C)

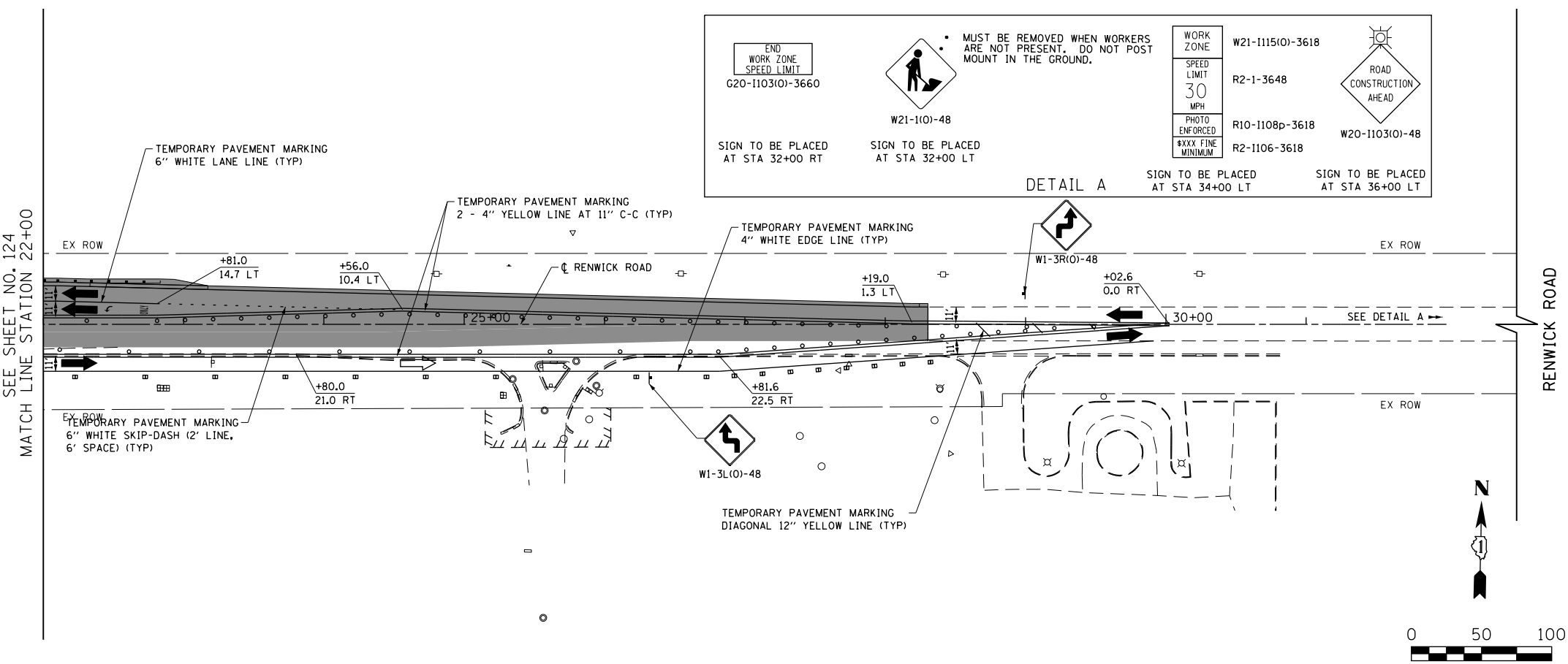
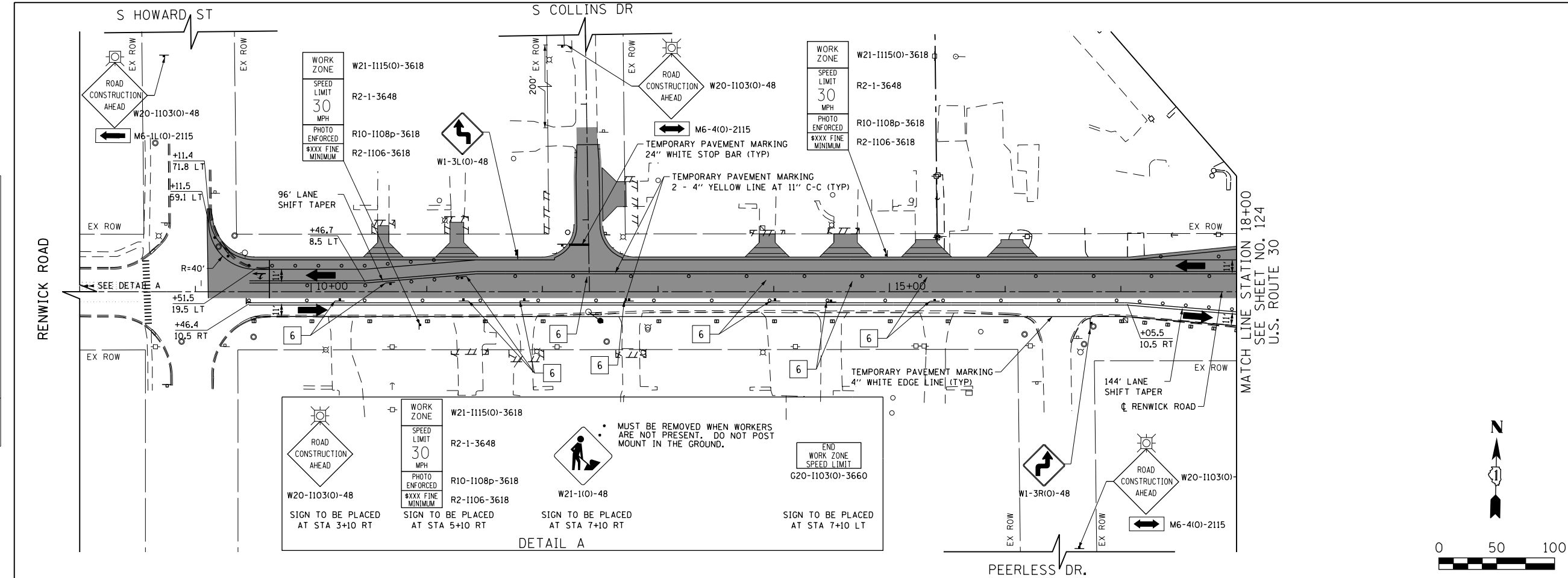
SCALE: 1"=50' SHEET NAME: MOT-1C-01 STA. 3696+00 TO STA. 3720+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	124
				CONTRACT NO. 60P95
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				



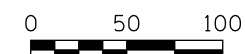
PLAN	SURVEYED	DATE
	PLOTTED	
	NOTED	
	BY	
	NO.	

PROFILE	SURVEYED	DATE
	PLOTTED	
	NOTED	
	BY	
	NO.	



**LEGEND**

- CONSTRUCTION WORK ZONE
  - COMPLETED PAVEMENT
  - TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
  - TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
  - TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
  - SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
  - MONODIRECTIONAL FLASHING LIGHT
  - DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
  - DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
  - TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
  - VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
  - DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
  - DIRECTION OF TRAFFIC FLOW
  - DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
  - INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
  - NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID
- 
- 1 DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
  - 2 DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
  - 3 R3-2L 24" X 24"
  - 4 R3-2R 24" X 24"
  - 5 STOP R1-1
  - 6 ONE WAY R6-1R 36" X 12"
  - 7 ONE WAY R6-1L 36" X 12"



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USER NAME = bshoeffler	DESIGNED - MNB	REVISED -
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

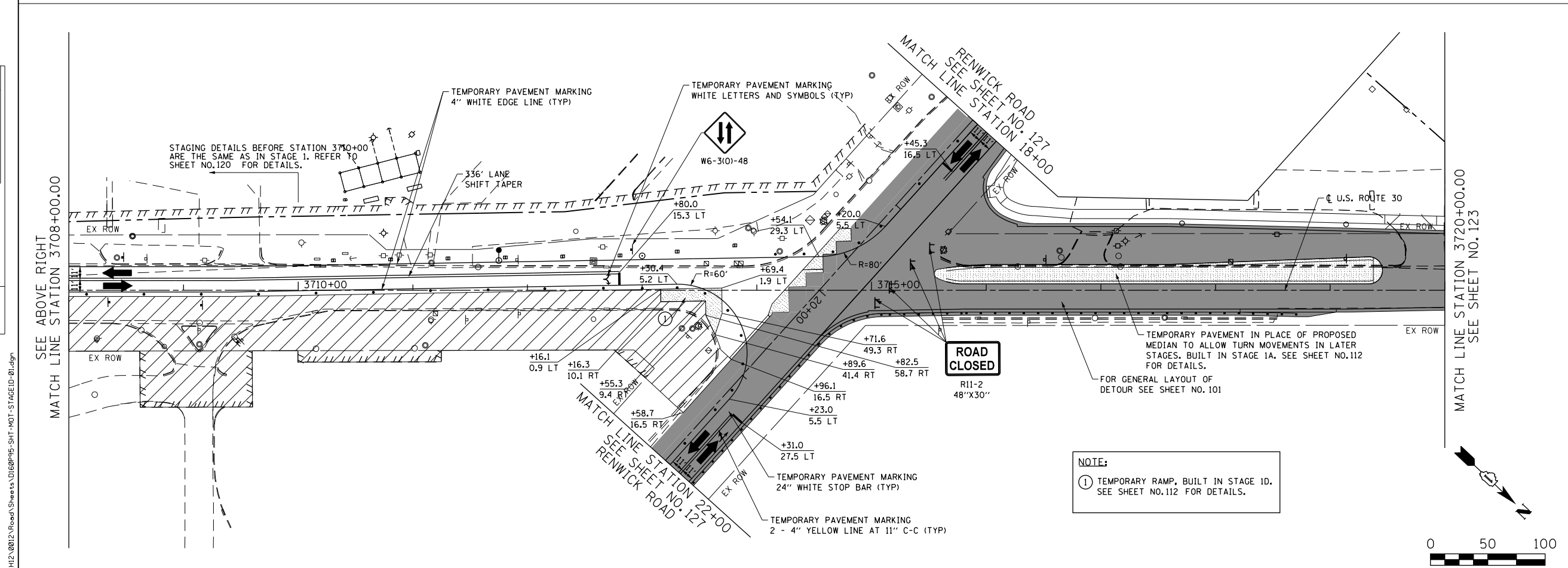
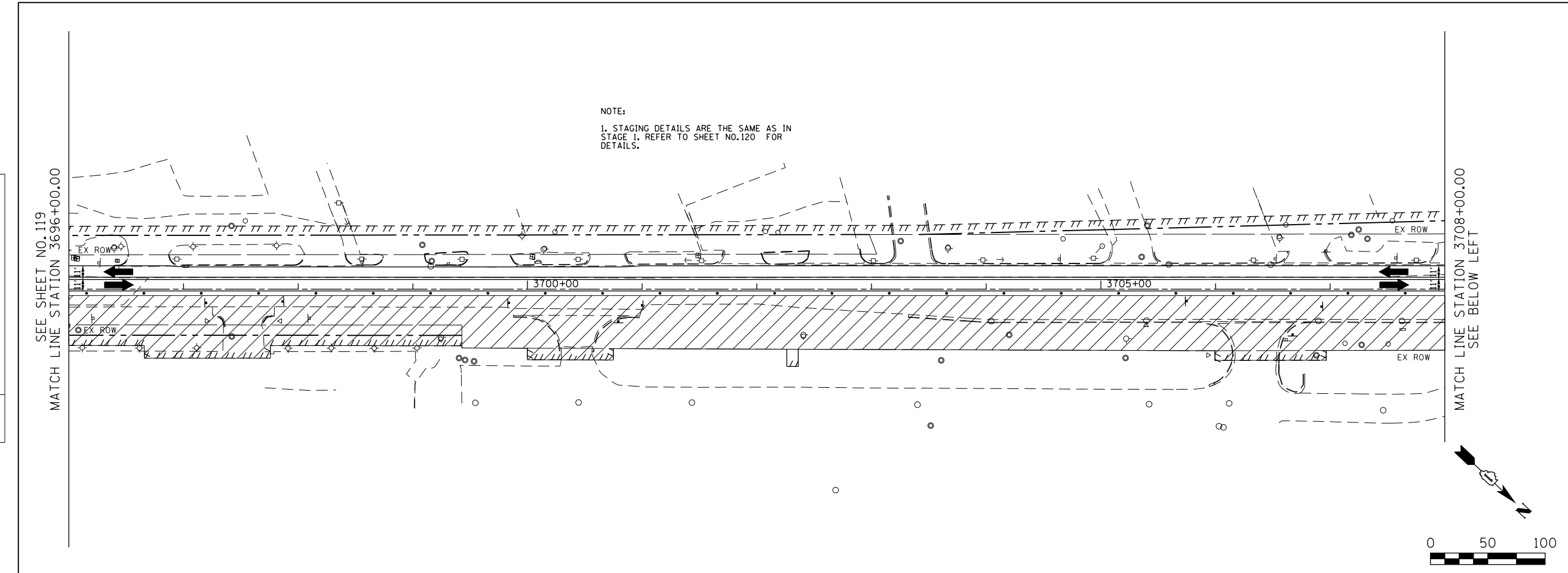
**RENWICK ROAD  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 1C)**

SCALE: 1"=50' SHEET NAME: MOT-1C-02 STA. 8+00 TO STA. 32+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	125
				CONTRACT NO. 60P95
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	
	NOTES CHECKED	
	BY	
	NO. OF	
	FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	
	NOTES CHECKED	
	BY	
	NO. OF	
	FILE NAME	



- LEGEND**
- CONSTRUCTION WORK ZONE
  - COMPLETED PAVEMENT
  - TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
  - TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
  - TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
  - SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
  - MONODIRECTIONAL FLASHING LIGHT
  - DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
  - DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
  - TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
  - VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
  - DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
  - DIRECTION OF TRAFFIC FLOW
  - DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
  - INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
  - NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID

- DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- R3-2L 24" X 24"
- R3-2R 24" X 24"
- STOP R1-1
- ONE WAY R6-1R 36" X 12"
- ONE WAY R6-1L 36" X 12"



USER NAME = bshaeffiger	DESIGNED - MNB	REVISED -
FILE = D:\60P95-SHT-MOT-STAGE1D-01.dgn	DRAWN - MNB	REVISED -
PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**U.S. ROUTE 30  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 1D)**

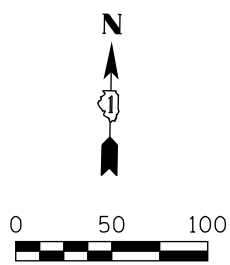
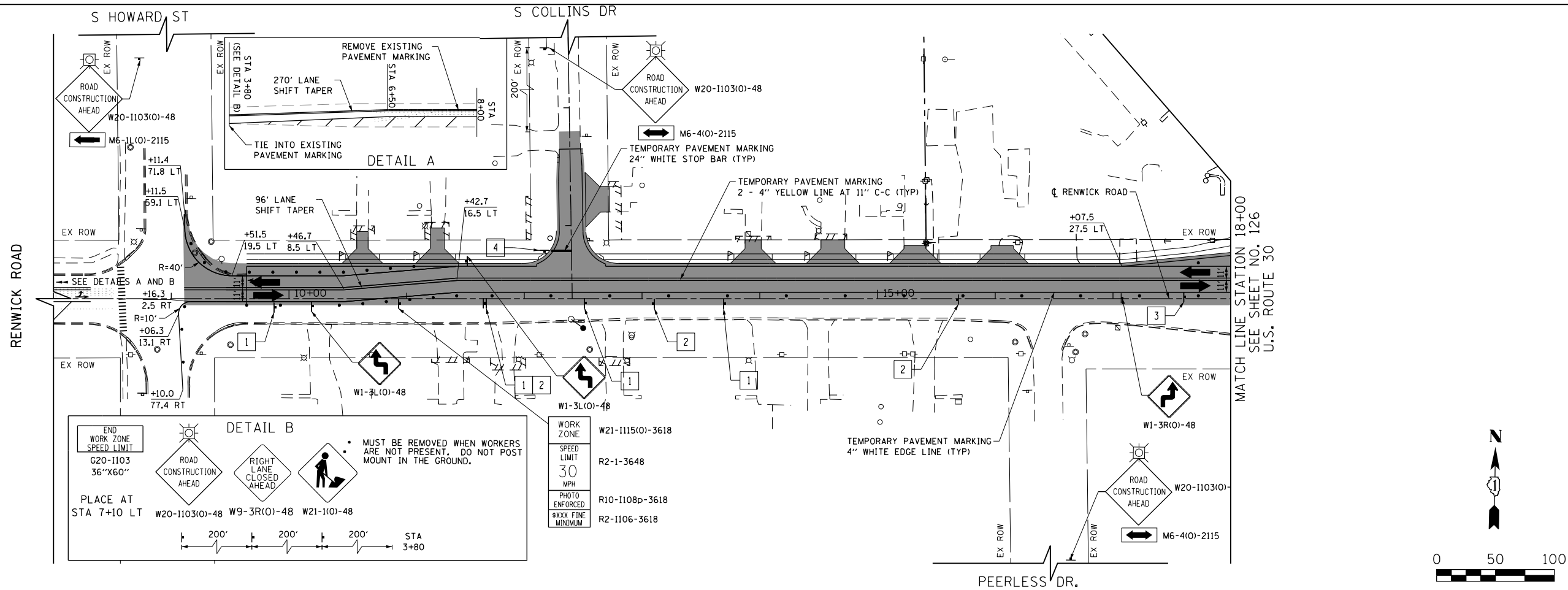
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	126
				CONTRACT NO. 60P95
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

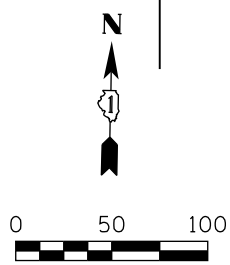
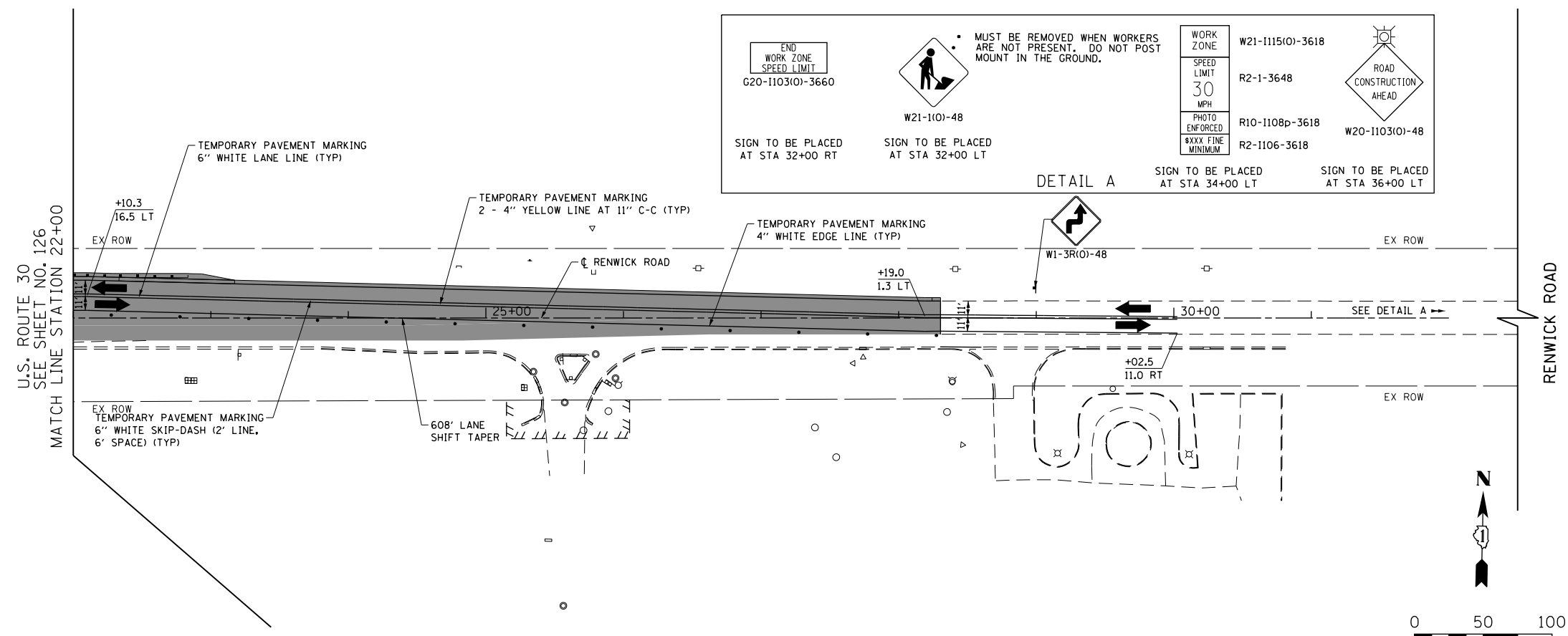
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PROFILE	SURVEYED	DATE
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	AT OR MAY BE CHECKED	
	BY	
	DATE	
	FILE NAME	

FILE PATH = G:\CH2\0012\Road\Sheet\0160P95-SHT-STAGEID-02.dgn



- LEGEND**
- CONSTRUCTION WORK ZONE
  - COMPLETED PAVEMENT
  - TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
  - TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
  - TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
  - SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
  - MONODIRECTIONAL FLASHING LIGHT
  - DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
  - DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
  - TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
  - VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
  - DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
  - DIRECTION OF TRAFFIC FLOW
  - DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
  - INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
  - NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID



- DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- R3-2L 24" X 24"
- R3-2R 24" X 24"
- STOP R1-1
- ONE WAY R6-1R 36" X 12"
- ONE WAY R6-1L 36" X 12"



USER NAME = bshoeffler	DESIGNED - MNB	REVISED -
FILE = D:\60P95-SHT-MOT-STAGEID-02.dgn	DRAWN - MNB	REVISED -
PLOT SCALE = 1/8" = 1' @ 11/23/2014	CHECKED -	REVISED -
	DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

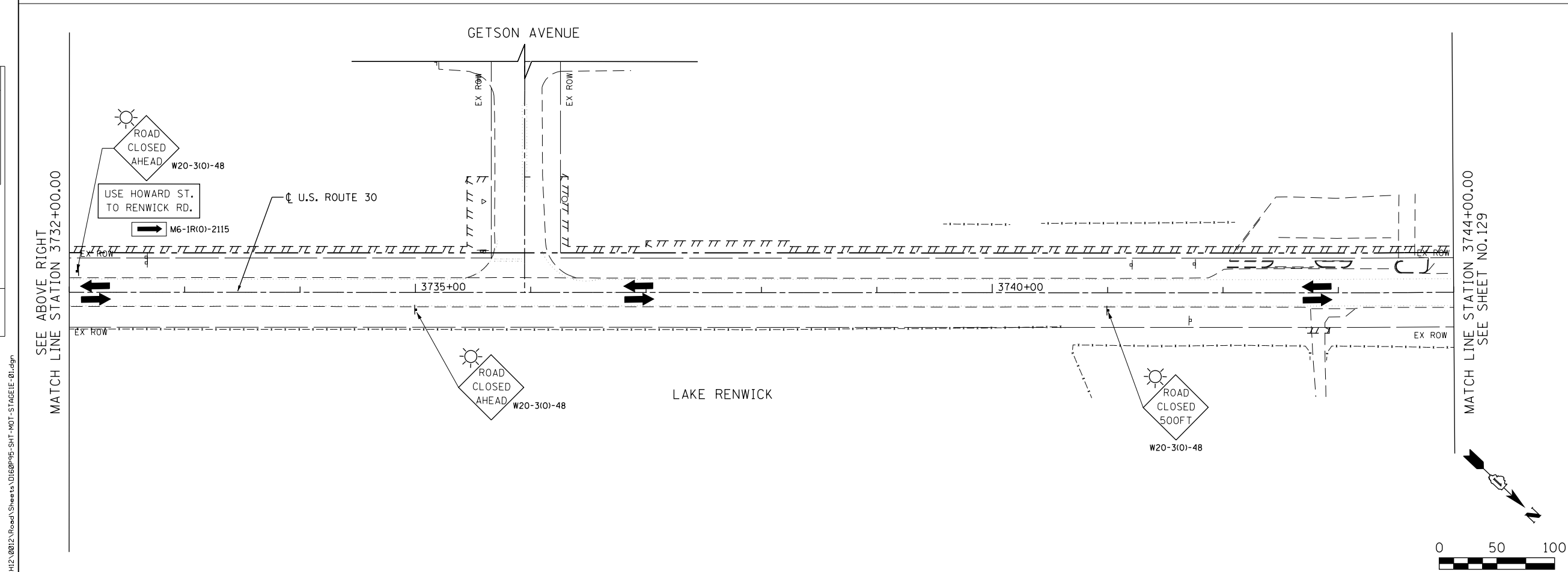
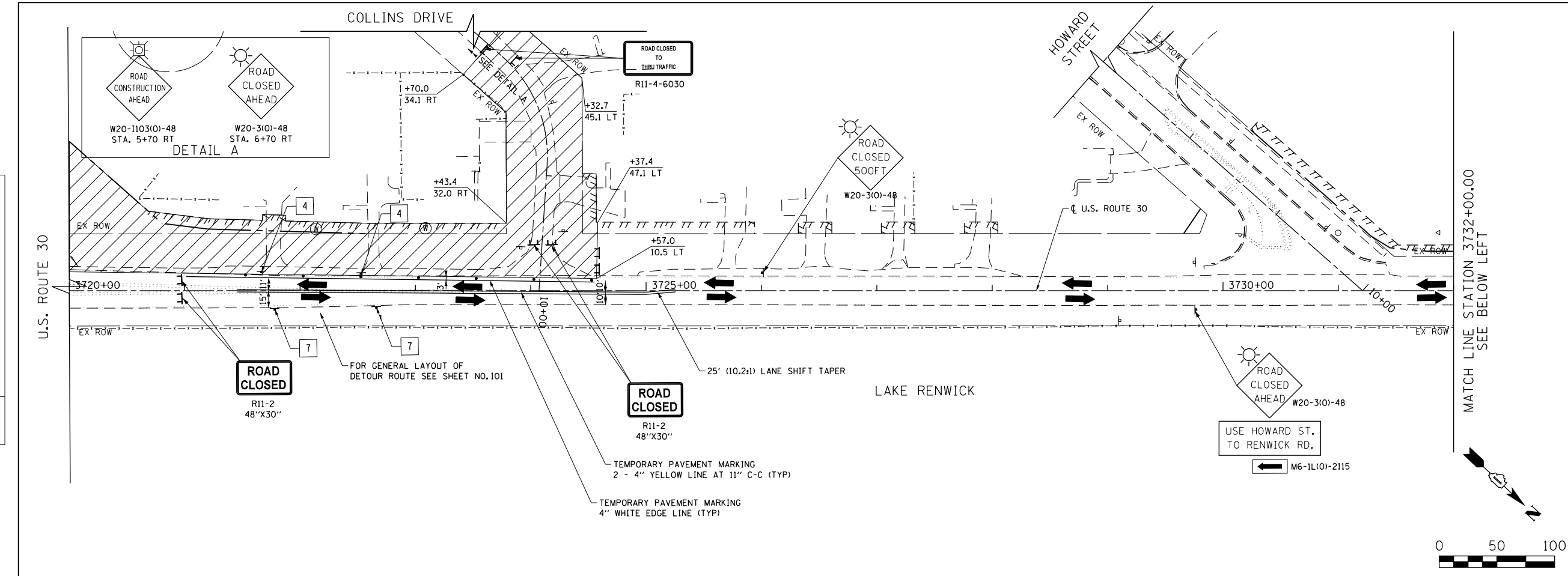
**RENWICK ROAD  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 1D)**

SCALE: 1"=50' SHEET NAME: MOT-1D-02 STA. 8+00 TO STA. 32+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	127
				CONTRACT NO. 60P95
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	CADD FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	CADD FILE NAME	



**LEGEND**

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONODIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID

1 DRIVEWAY ENTRANCE 30" X 30" SEE TC-26

2 DRIVEWAY ENTRANCE 30" X 30" SEE TC-26

3 R3-2L 24" X 24"

4 R3-2R 24" X 24"

5 STOP R1-1

6 ONE WAY R6-1R 36" X 12"

7 ONE WAY R6-1L 36" X 12"

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USER NAME = bshoefliger	DESIGNED - MNB	REVISED -
FILE = D:\60P95-SHT-MOT-STAGE1E-01.dgn	DRAWN - MNB	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

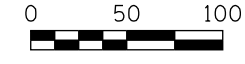
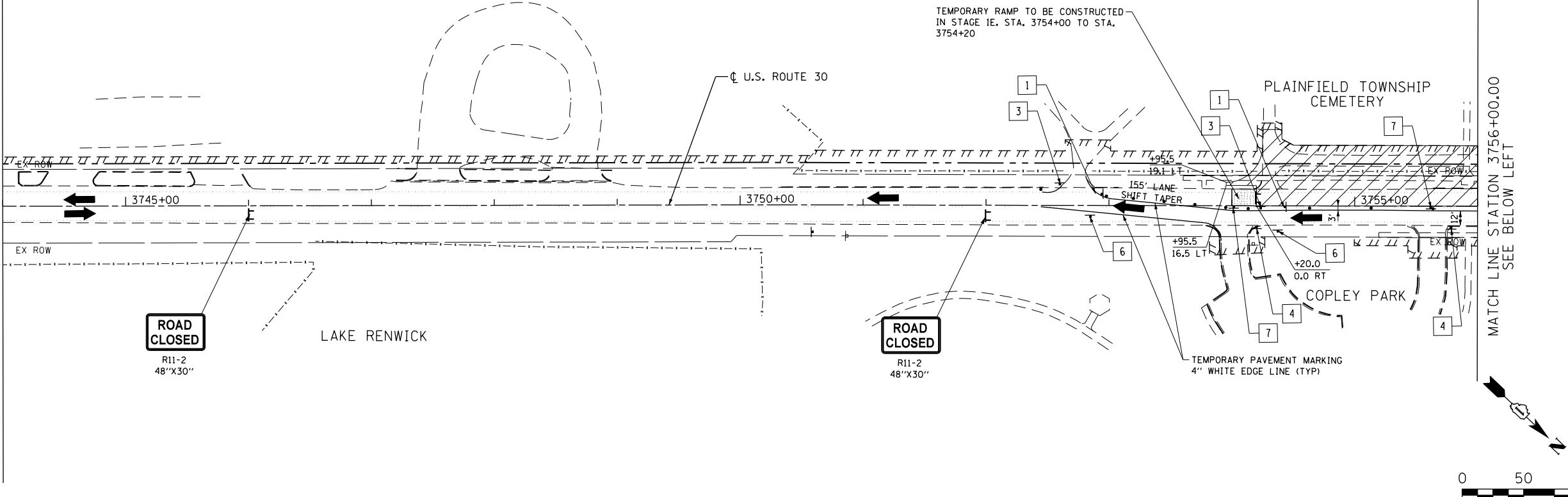
**U.S. ROUTE 30 - RENWICK ROAD TO IL ROUTE 59  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 1E)**

SCALE: 1"=50' SHEET NAME: MOT-1E-01 STA. 3720+00 TO STA. 3744+00

F.A.P. RTE. 575	SECTION 14W - R	COUNTY WILL	TOTAL SHEETS 681	SHEET NO. 128
CONTRACT NO. 60P95				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

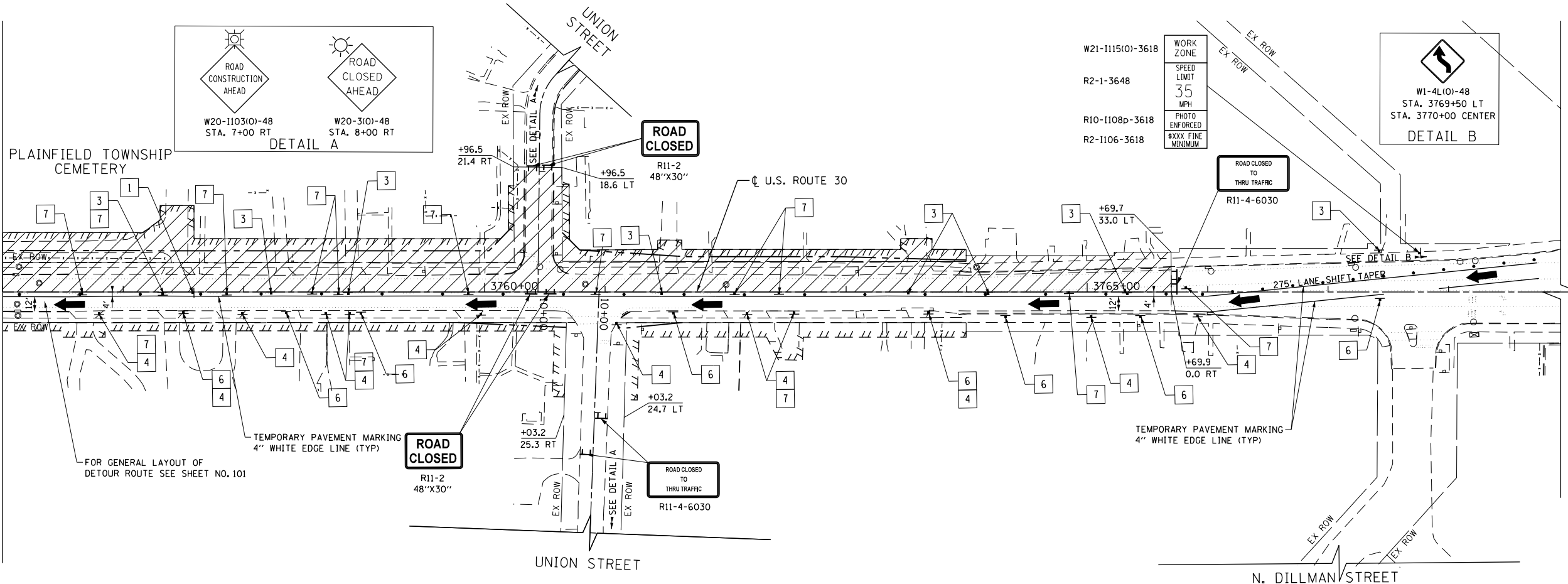
PLAN	SURVEYED	DATE
	PLOTTED	BY
	NOTED	
	FILED	
	NO. _____	
	FILE NAME	

SEE SHEET NO. 128  
MATCH LINE STATION 3744+00.00



PROFILE	SURVEYED	DATE
	PLOTTED	BY
	NOTED	
	FILED	
	NO. _____	
	FILE NAME	

SEE ABOVE RIGHT  
MATCH LINE STATION 3756+00.00



LEGEND

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONODIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID
- DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- R3-2L 24" X 24"
- R3-2R 24" X 24"
- R1-1
- R6-1R 36" X 12"
- R6-1L 36" X 12"

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USER NAME = bshoefliger	DESIGNED - MNB	REVISED -
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PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

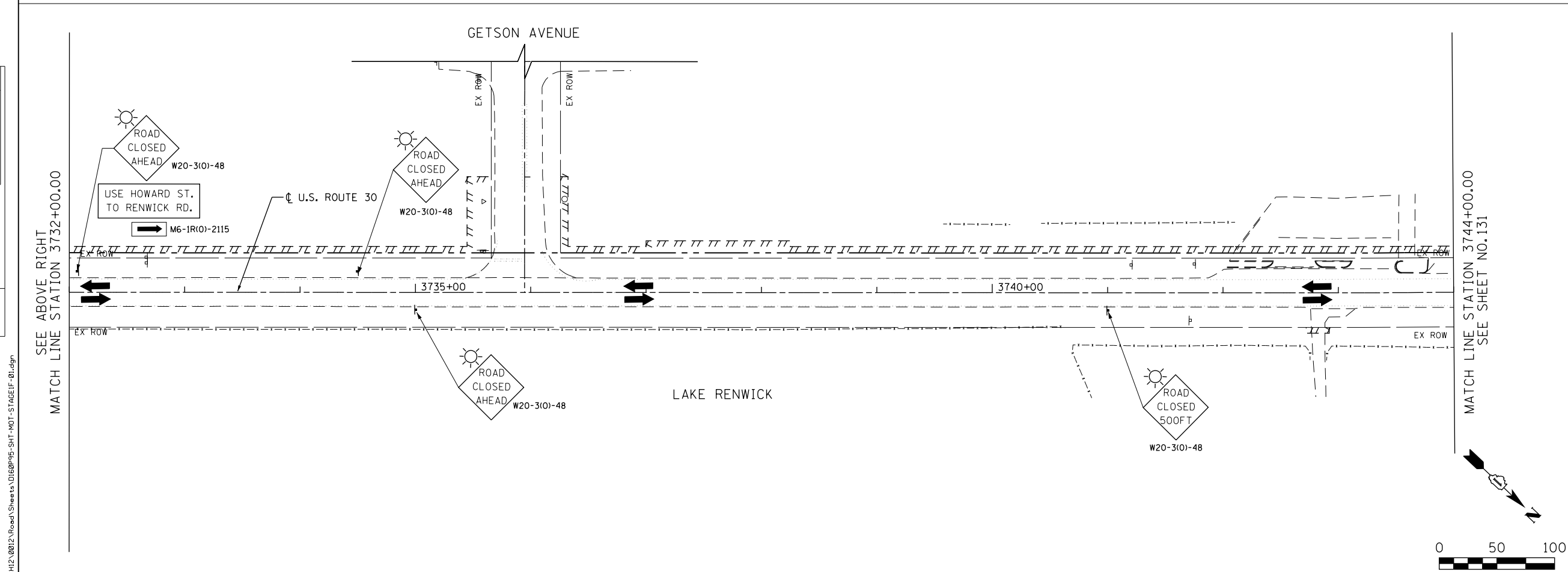
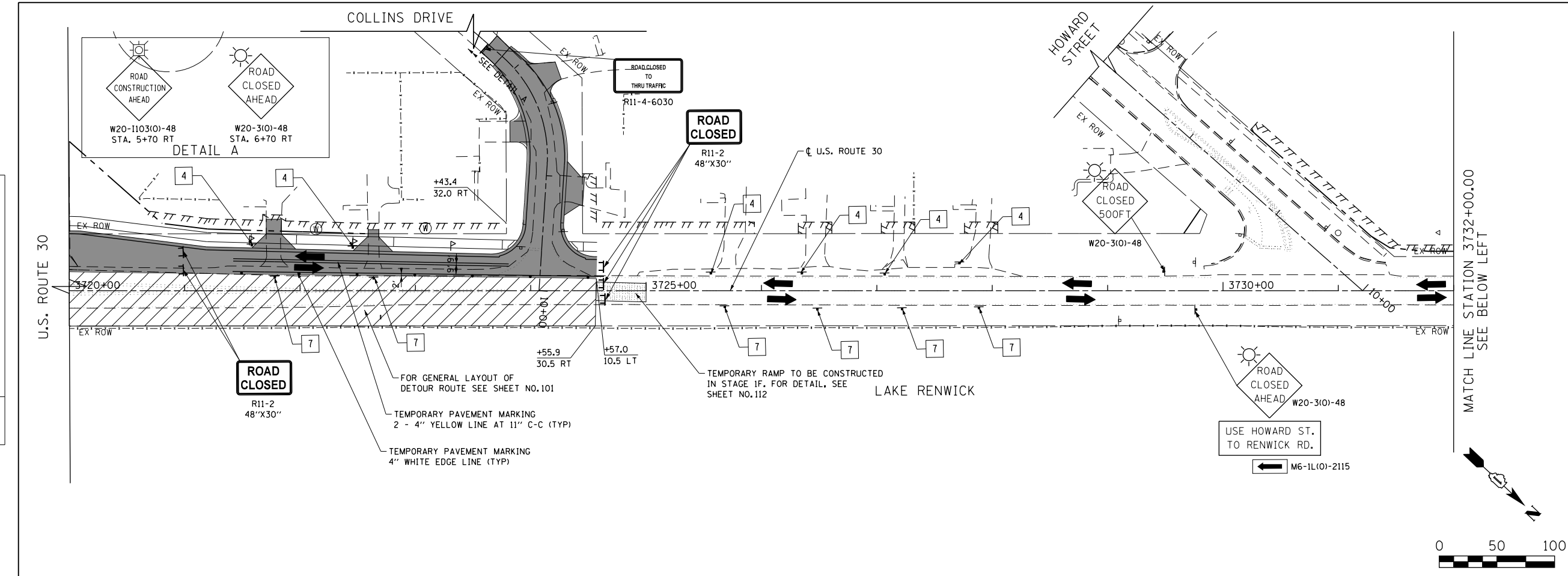
U.S. ROUTE 30 - RENWICK ROAD TO IL ROUTE 59  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 1E)

SCALE: 1"=50' SHEET NAME: MOT-1E-02 STA. 3744+00 TO STA. 3763+00

F.A.P. RTE. 575	SECTION 14W - R	COUNTY WILL	TOTAL SHEETS 681	SHEET NO. 129
CONTRACT NO. 60P95				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	CADD FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	CADD FILE NAME	



**LEGEND**

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID

1	DRIVEWAY ENTRANCE	30" X 30" SEE TC-26
2	DRIVEWAY ENTRANCE	30" X 30" SEE TC-26
3		R3-2L 24" X 24"
4		R3-2R 24" X 24"
5		R1-1
6		R6-1R 36" X 12"
7		R6-1L 36" X 12"

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PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

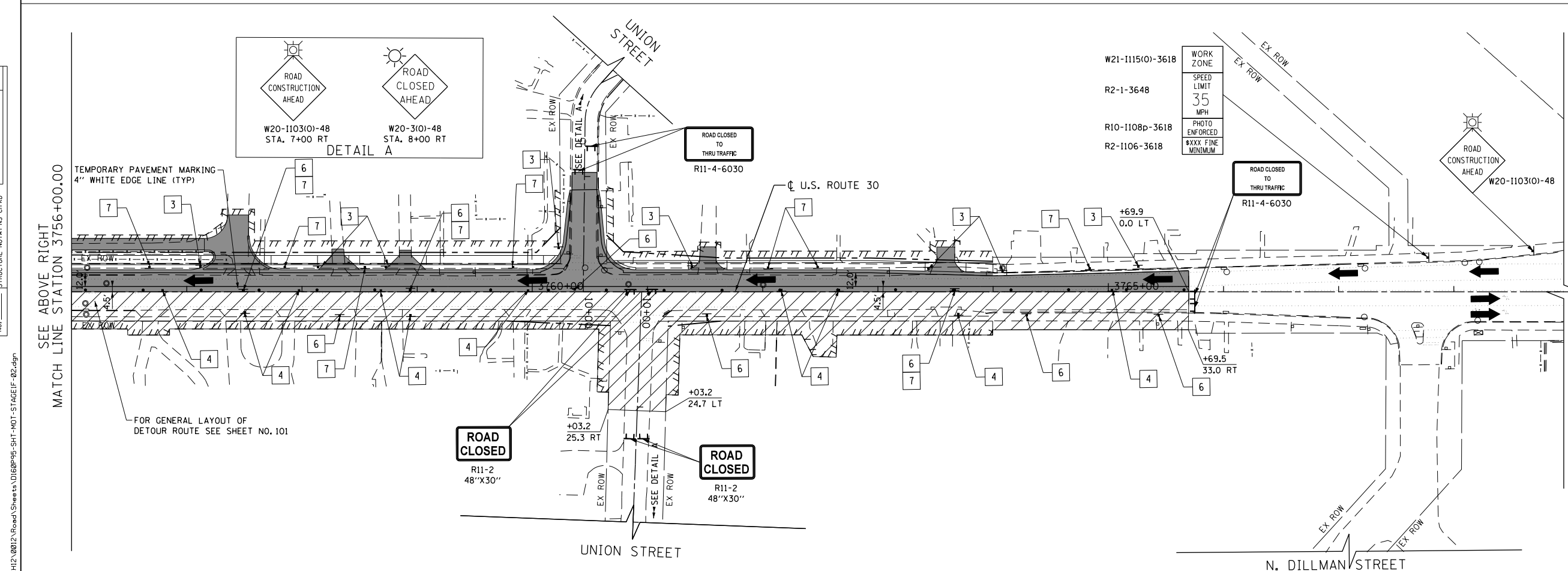
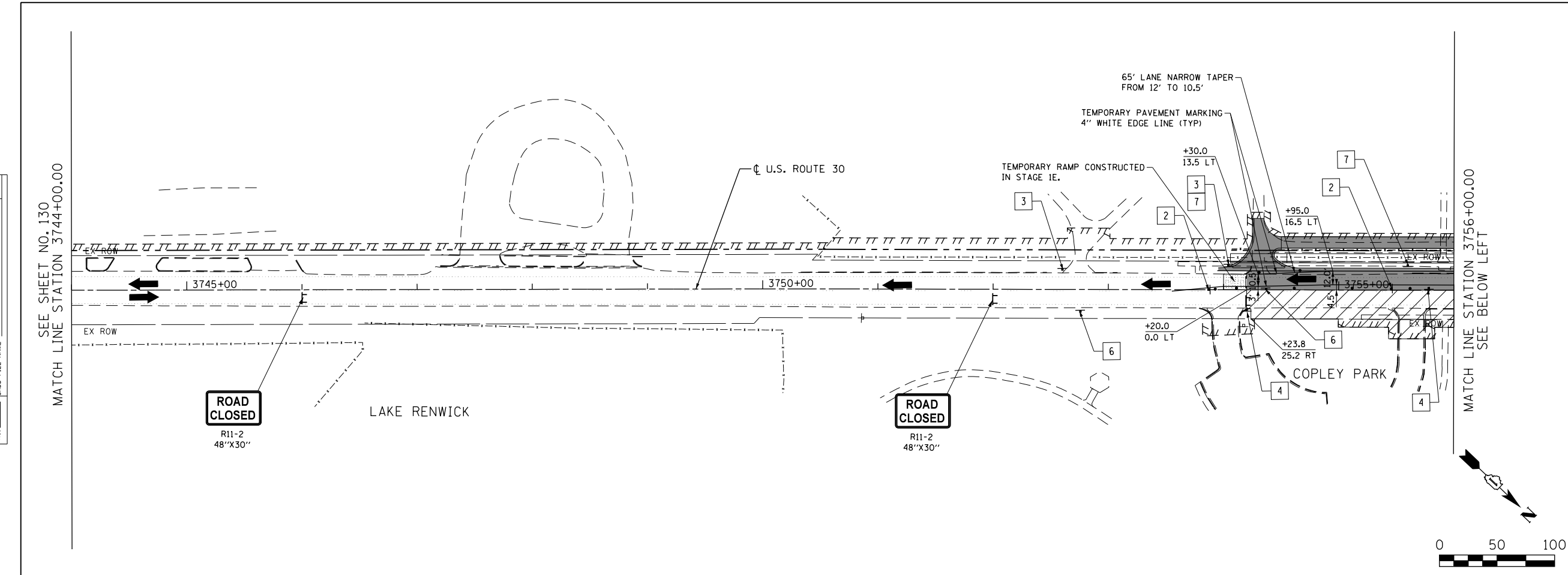
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>U.S. ROUTE 30 - RENWICK ROAD TO IL ROUTE 59 SUGGESTED TRAFFIC CONTROL PLANS (STAGE 1F)</b>	
SCALE: 1"=50'	SHEET NAME: MOT-1F-01
STA. 3720+00	TO STA. 3744+00

F.A.P. RTE. 575	SECTION 14W - R	COUNTY WILL	TOTAL SHEETS 681	SHEET NO. 130
CONTRACT NO. 60P95				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
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	NOTED	
	BY	
	NO.	
	FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	
	NOTED	
	BY	
	NO.	
	FILE NAME	



**LEGEND**

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
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- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID

1 DRIVEWAY ENTRANCE 30" X 30" SEE TC-26

2 DRIVEWAY ENTRANCE 30" X 30" SEE TC-26

3 R3-2L 24" X 24"

4 R3-2R 24" X 24"

5 STOP R1-1

6 ONE WAY R6-1R 36" X 12"

7 ONE WAY R6-1L 36" X 12"

0 50 100



USER NAME = bshoeffler	DESIGNED - MNB	REVISED -
FILE = D:\60P95-SHT-MOT-STAGE1F-02.dgn	DRAWN - MNB	REVISED -
PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

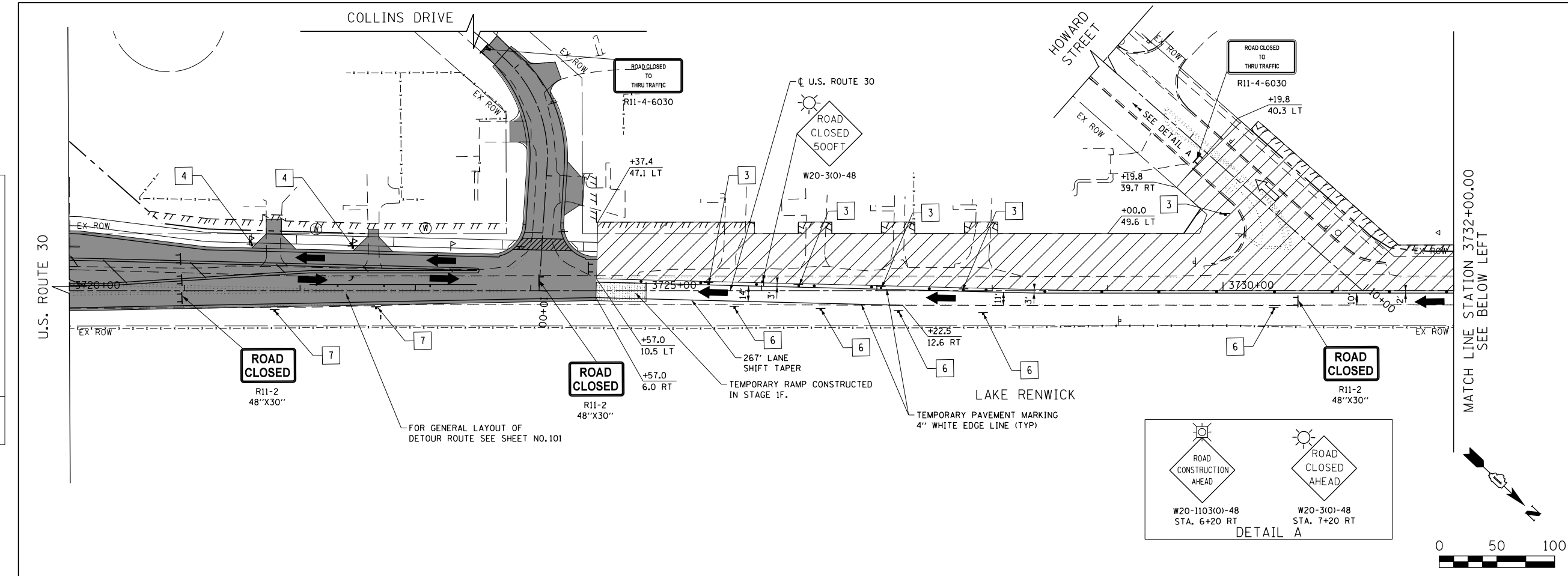
**U.S. ROUTE 30 - RENWICK ROAD TO IL ROUTE 59  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 1F)**

SCALE: 1"=50' SHEET NAME: MOT-1F-02 STA. 3744+00 TO STA. 3763+00

F.A.P. RTE. 575	SECTION 14W - R	COUNTY WILL	TOTAL SHEETS 681	SHEET NO. 131
CONTRACT NO. 60P95				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

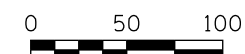
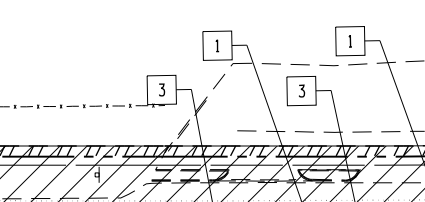
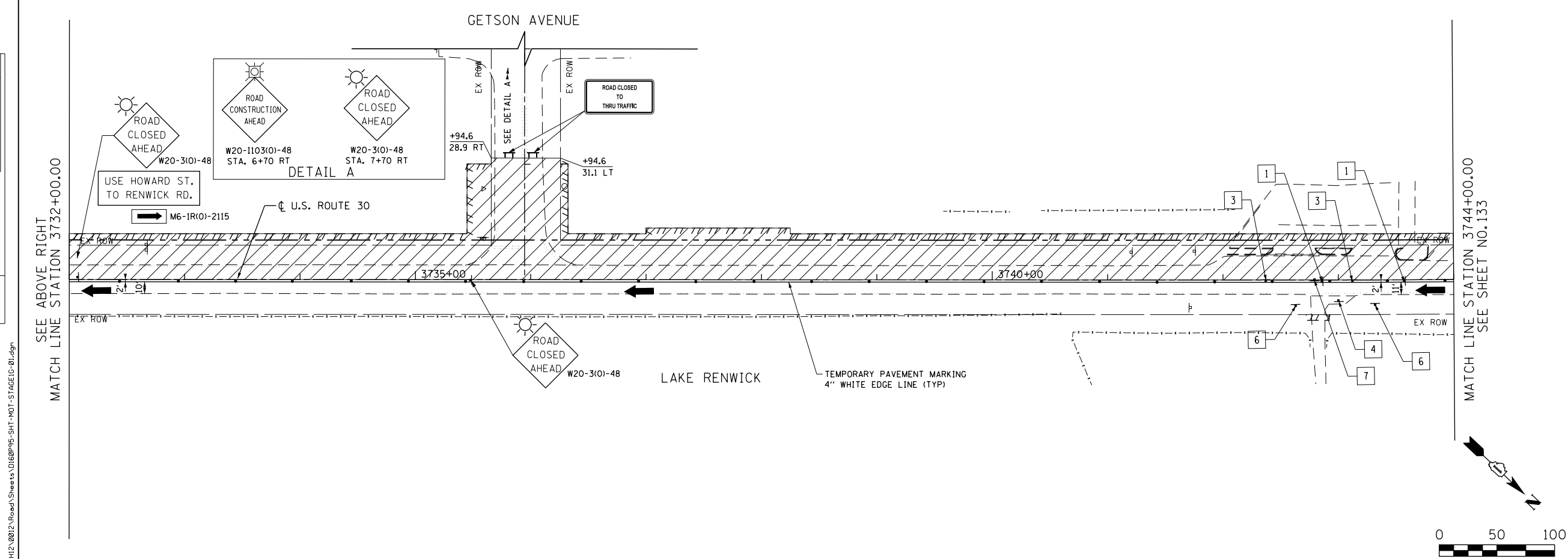
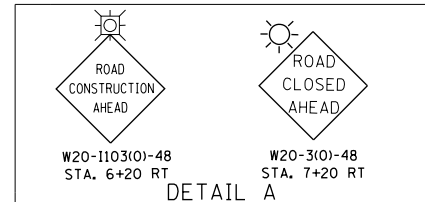
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	NOTED	
	BY	
	NO.	
	FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	
	NOTED	
	BY	
	NO.	
	FILE NAME	



**LEGEND**

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONODIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID



- DRIVEWAY ENTRANCE 30' X 30" SEE TC-26
- DRIVEWAY ENTRANCE 30' X 30" SEE TC-26
- R3-2L 24" X 24"
- R3-2R 24" X 24"
- STOP R1-1
- ONE WAY R6-1R 36" X 12"
- ONE WAY R6-1L 36" X 12"



USER NAME = bshoefliger	DESIGNED - MNB	REVISED -
FILE = D:\60P95-SHT-MOT-STAGE1G-01.dgn	DRAWN - MNB	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**U.S. ROUTE 30 - RENWICK ROAD TO IL ROUTE 59  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 1G)**

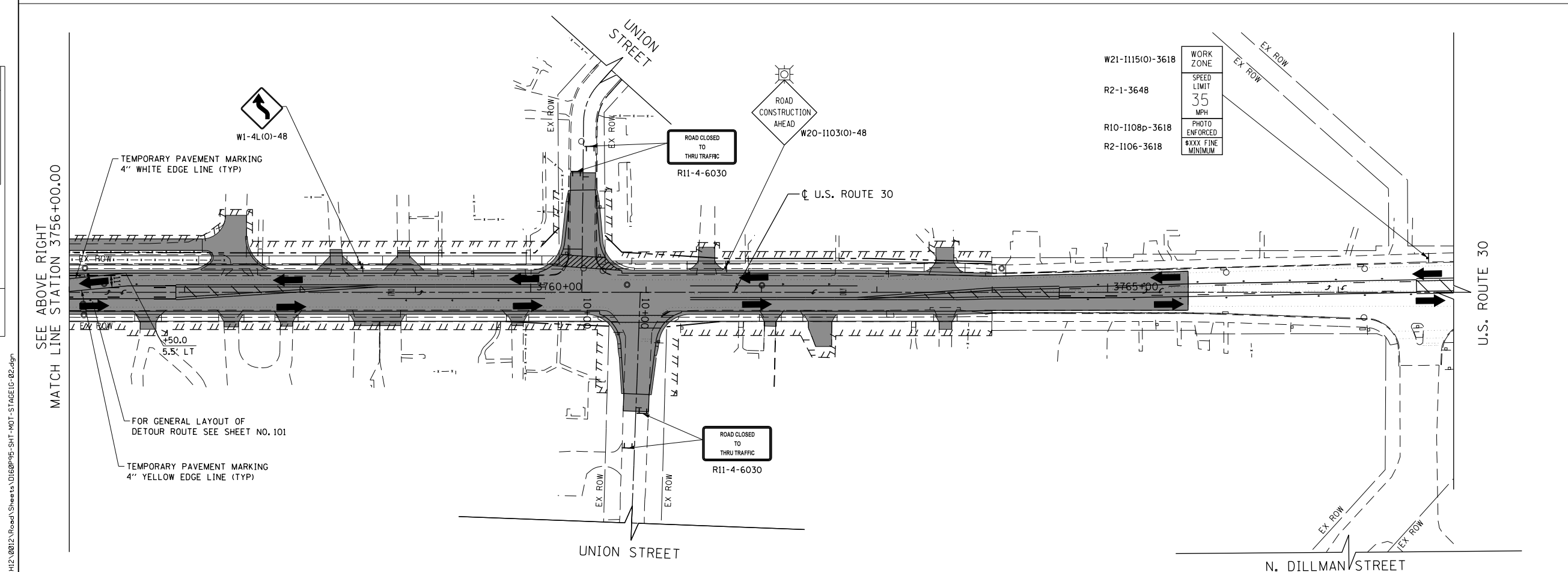
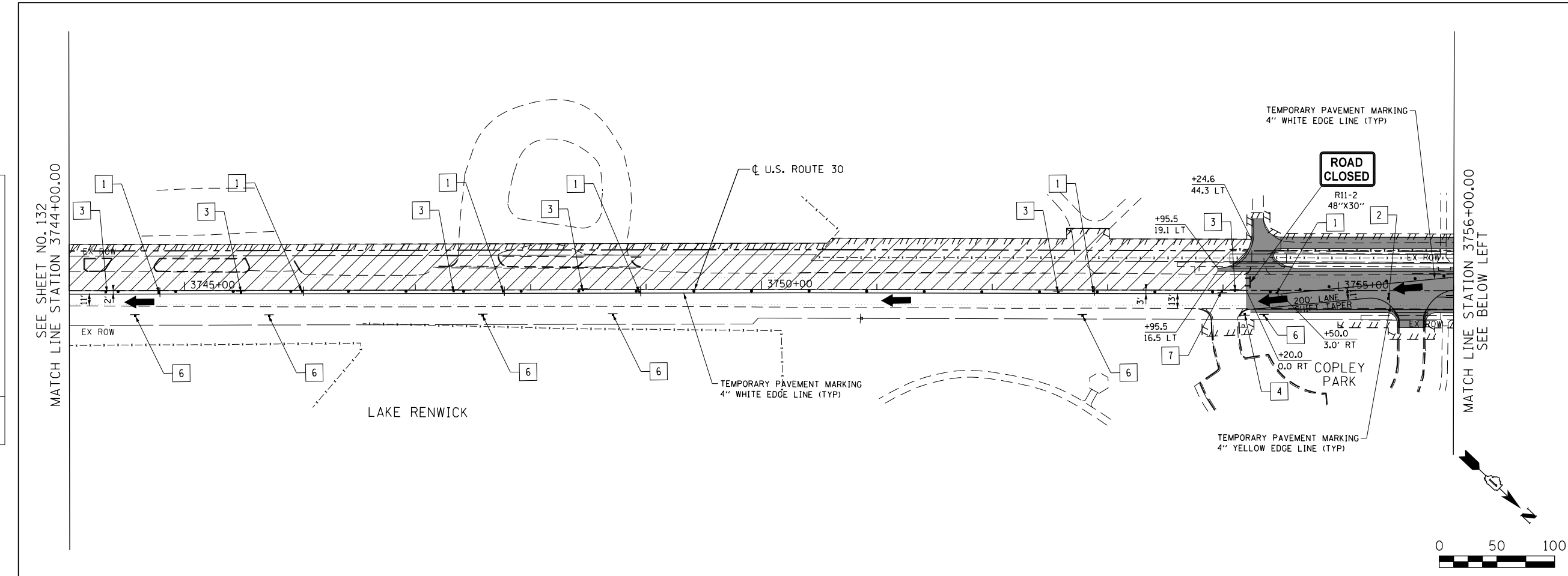
SCALE: 1"=50' SHEET NAME: MOT-1G-01 STA. 3720+00 TO STA. 3744+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	132
				CONTRACT NO. 60P95
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				



PLAN	SURVEYED	DATE
	PLOTTED	
	NOTED	
	BY	
	NO.	

PROFILE	SURVEYED	DATE
	PLOTTED	
	NOTED	
	BY	
	NO.	



**LEGEND**

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID

1 DRIVEWAY ENTRANCE 30" X 30" SEE TC-26

2 DRIVEWAY ENTRANCE 30" X 30" SEE TC-26

3 R3-2L 24" X 24"

4 R3-2R 24" X 24"

5 R1-1

6 R6-1R 36" X 12"

7 R6-1L 36" X 12"

0 50 100

FILE PATH = G:\CH21\0012\Road\Sheet\10160P95-SHT-MOT-STAGE1G-02.dgn



USER NAME = bshoeffler	DESIGNED - MNB	REVISED -
FILE = D160P95-SHT-MOT-STAGE1G-02.dgn	DRAWN - MNB	REVISED -
PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

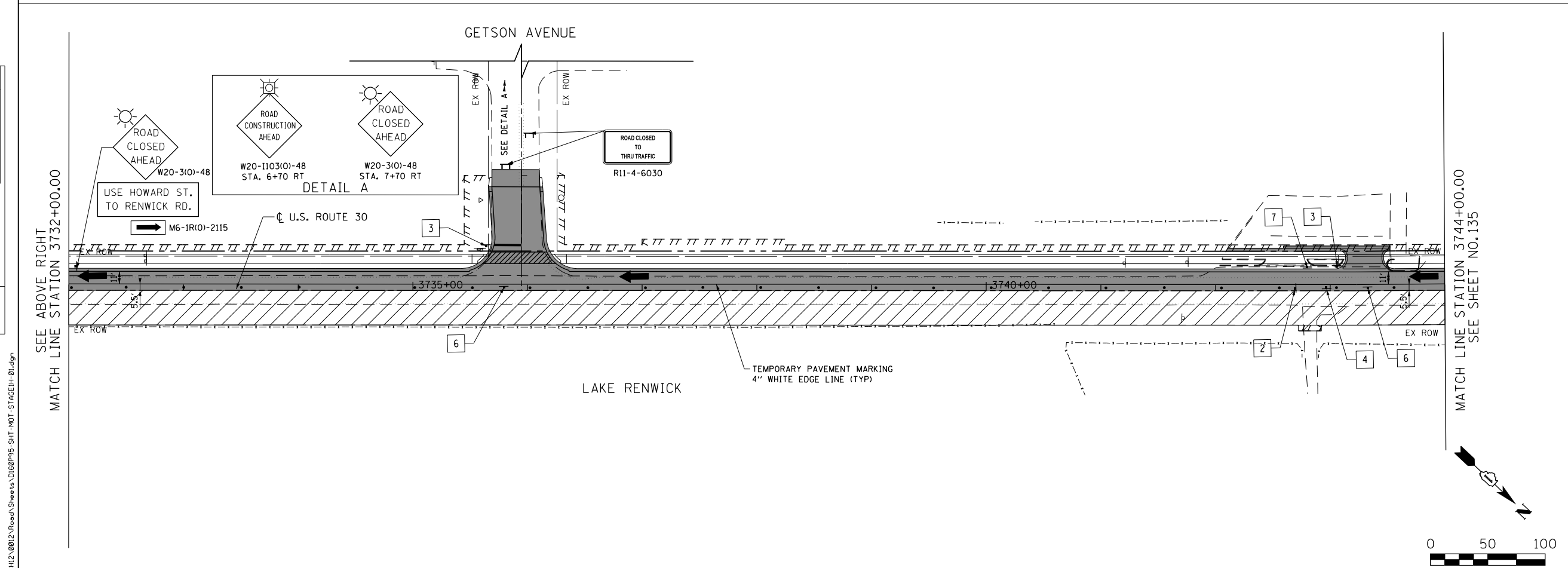
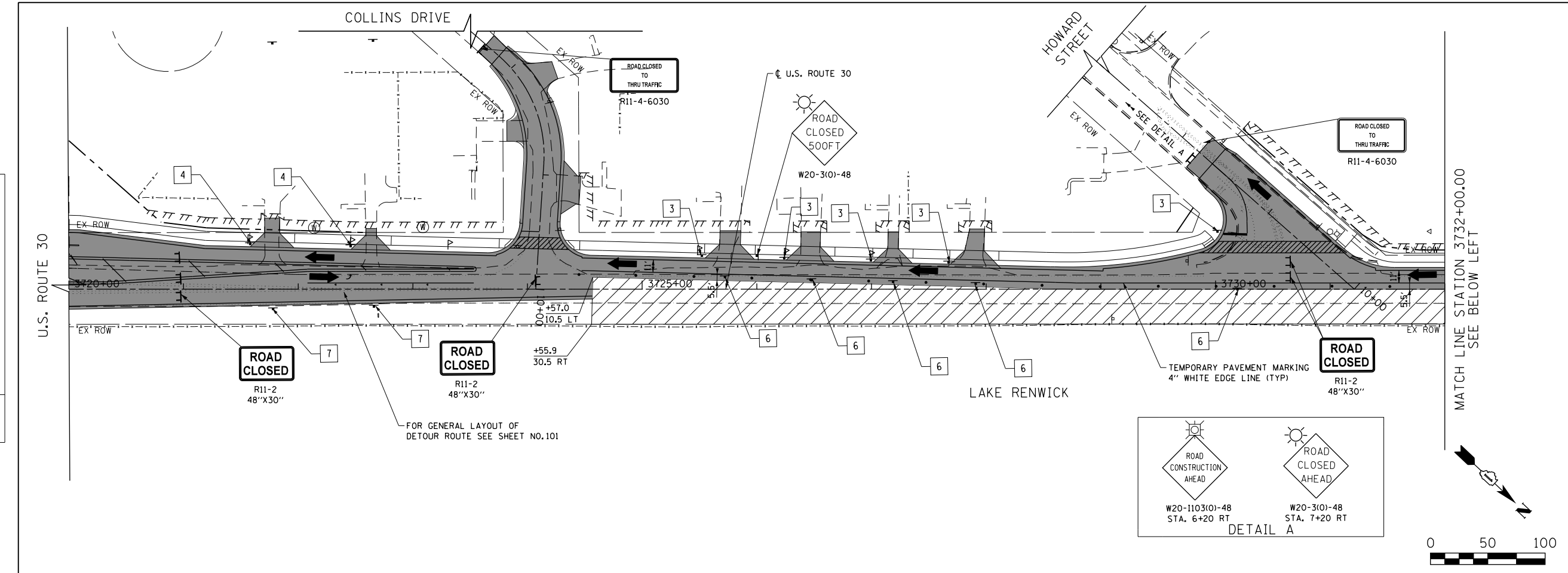
**U.S. ROUTE 30 - RENWICK ROAD TO IL ROUTE 59  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 1G)**

SCALE: 1"=50' SHEET NAME: MOT-1G-02 STA. 3744+00 TO STA. 3763+00

F.A.P. RTE. 575	SECTION 14W - R	COUNTY WILL	TOTAL SHEETS 681	SHEET NO. 133
CONTRACT NO. 60P95				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	BY	
	DATE	

PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	BY	
	DATE	



**LEGEND**

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID

1	DRIVEWAY ENTRANCE	30" X 30" SEE TC-26
2	DRIVEWAY ENTRANCE	30" X 30" SEE TC-26
3		R3-2L 24" X 24"
4		R3-2R 24" X 24"
5	STOP	R1-1
6	ONE WAY	R6-1R 36" X 12"
7	ONE WAY	R6-1L 36" X 12"

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USER NAME = bshoefliger	DESIGNED - MNB	REVISED -
FILE = D160P95-SHT-MOT-STAGE1H-01.dgn	DRAWN - MNB	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**U.S. ROUTE 30 - RENWICK ROAD TO IL ROUTE 59  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 1H)**

SCALE: 1"=50' SHEET NAME: MOT-1H-01 STA. 3720+00 TO STA. 3744+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	134
				CONTRACT NO. 60P95
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	NOTES CHECKED	
	BY	
	NO. _____	
	DATE	

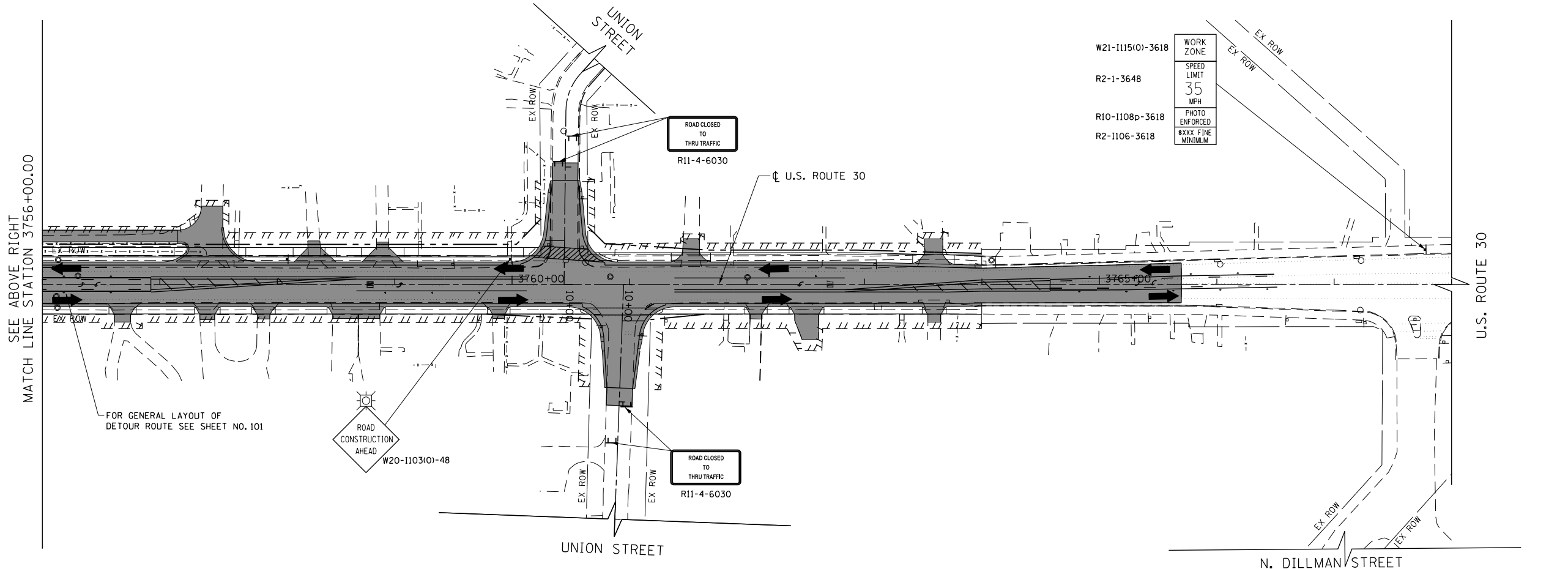
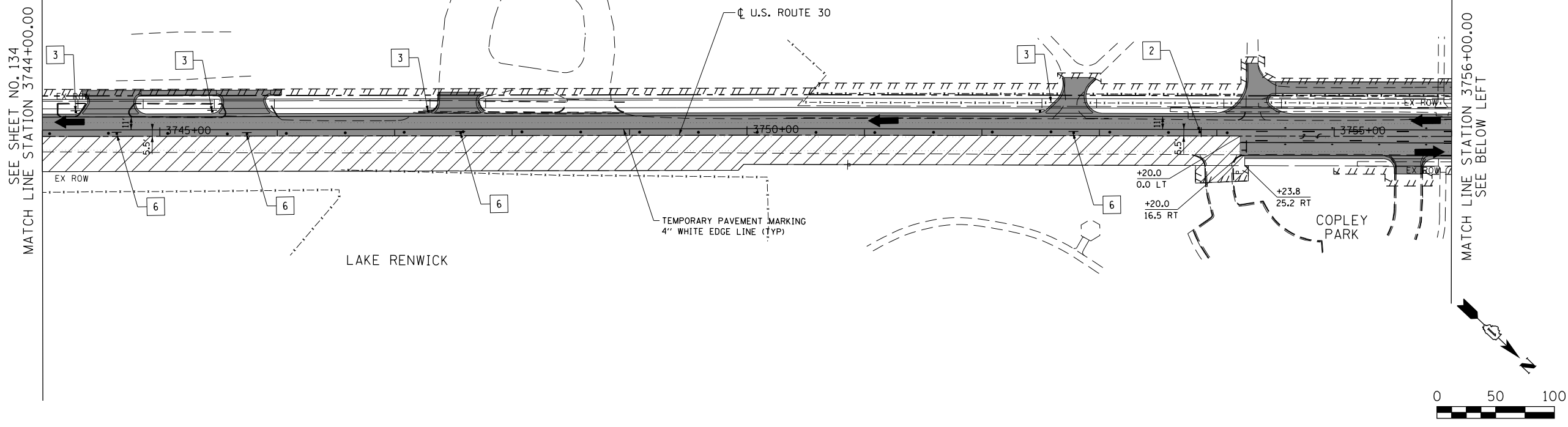
PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	NOTES CHECKED	
	BY	
	NO. _____	
	DATE	

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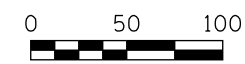
SEE SHEET NO. 134  
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MATCH LINE STATION 3756+00.00  
SEE BELOW LEFT

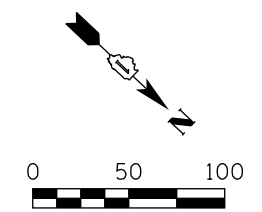
SEE ABOVE RIGHT  
MATCH LINE STATION 3756+00.00



- LEGEND**
- CONSTRUCTION WORK ZONE
  - COMPLETED PAVEMENT
  - TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
  - TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
  - TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
  - SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
  - MONODIRECTIONAL FLASHING LIGHT
  - DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
  - DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
  - TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
  - VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
  - DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
  - DIRECTION OF TRAFFIC FLOW
  - DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
  - INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
  - NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID



- 1. DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- 2. DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- 3. R3-2L 24" X 24"
- 4. R3-2R 24" X 24"
- 5. STOP R1-1
- 6. ONE WAY R6-1R 36" X 12"
- 7. ONE WAY R6-1L 36" X 12"



USER NAME = bshoeffliger	DESIGNED - MNB	REVISED -
FILE = D160P95-SHT-MOT-STAGE1H-02.dgn	DRAWN - MNB	REVISED -
PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

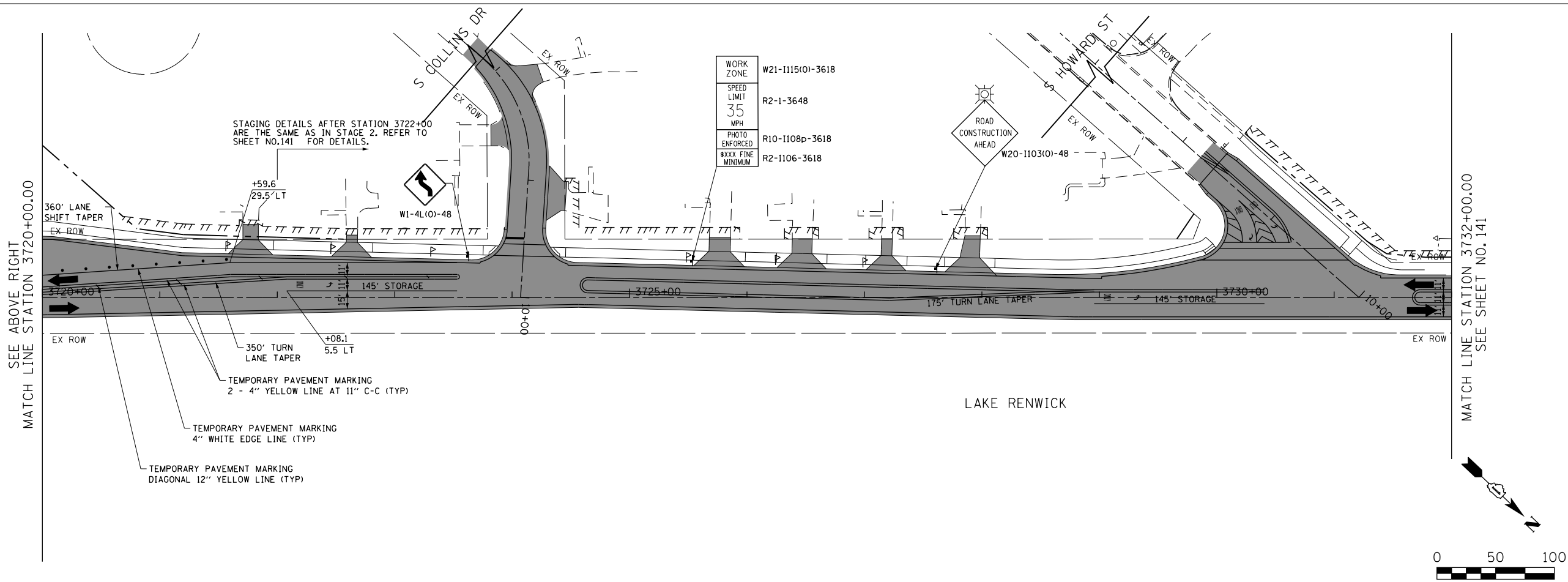
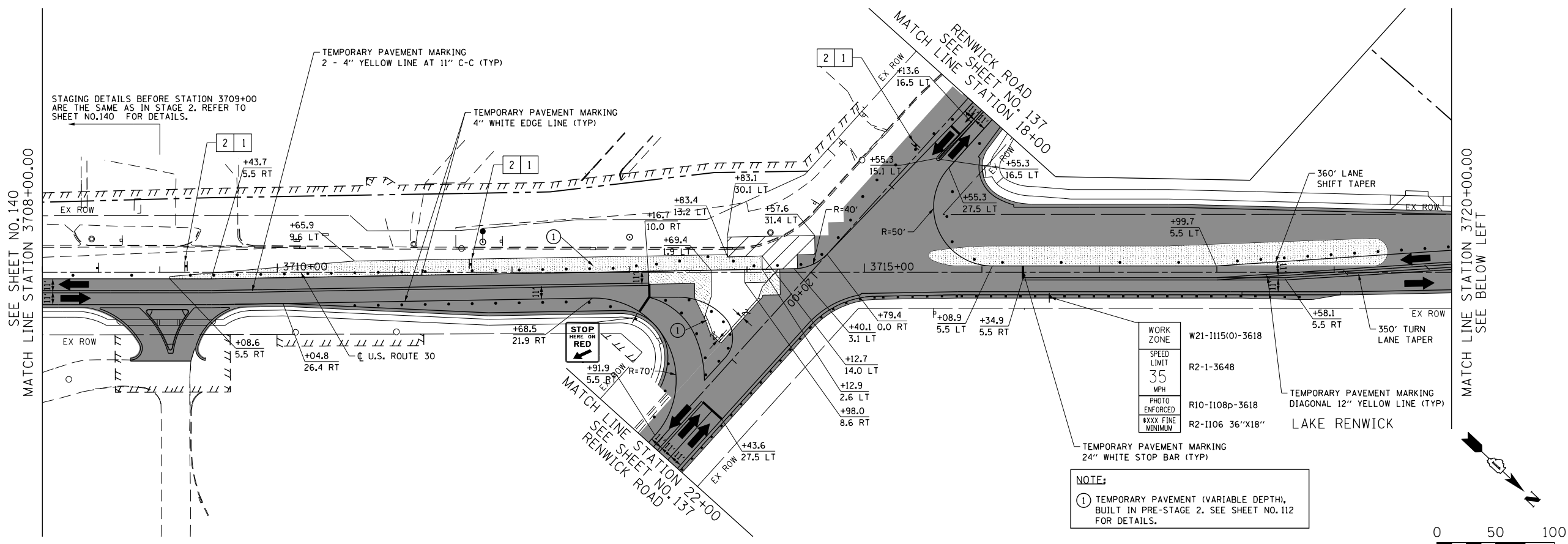
**U.S. ROUTE 30 - RENWICK ROAD TO IL ROUTE 59  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 1H)**

SCALE: 1"=50' SHEET NAME: MOT-1H-02 STA. 3744+00 TO STA. 3763+00

F.A.P. RTE. 575	SECTION 14W - R	COUNTY WILL	TOTAL SHEETS 681	SHEET NO. 135
CONTRACT NO. 60P95				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	BY
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK	
	NO. _____	
	CADD FILE NAME	
	NO. _____	

PROFILE	SURVEYED	DATE
	PLOTTED	BY
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK	
	NO. _____	
	CADD FILE NAME	
	NO. _____	



**LEGEND**

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONODIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID

1	DRIVEWAY ENTRANCE	30" X 30" SEE TC-26
2	DRIVEWAY ENTRANCE	30" X 30" SEE TC-26
3		R3-2L 24" X 24"
4		R3-2R 24" X 24"
5		R1-1
6		R6-1R 36" X 12"
7		R6-1L 36" X 12"

**WORK ZONE**

W21-1115(O)-3618
SPEED LIMIT 35 MPH
PHOTO ENFORCED
\$XXX FINE MINIMUM

**NOTE:**

① TEMPORARY PAVEMENT (VARIABLE DEPTH), BUILT IN PRE-STAGE 2. SEE SHEET NO. 112 FOR DETAILS.

**WORK ZONE**

W21-1115(O)-3618
SPEED LIMIT 35 MPH
PHOTO ENFORCED
\$XXX FINE MINIMUM



USER NAME = bshaeffiger	DESIGNED - MNB	REVISED -
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PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**U.S. ROUTE 30  
SUGGESTED TRAFFIC CONTROL PLANS (PRE-STAGE 2)**

SCALE: 1"=50' SHEET NAME: MOT-2P-01 STA. 3708+00 TO STA. 3732+00

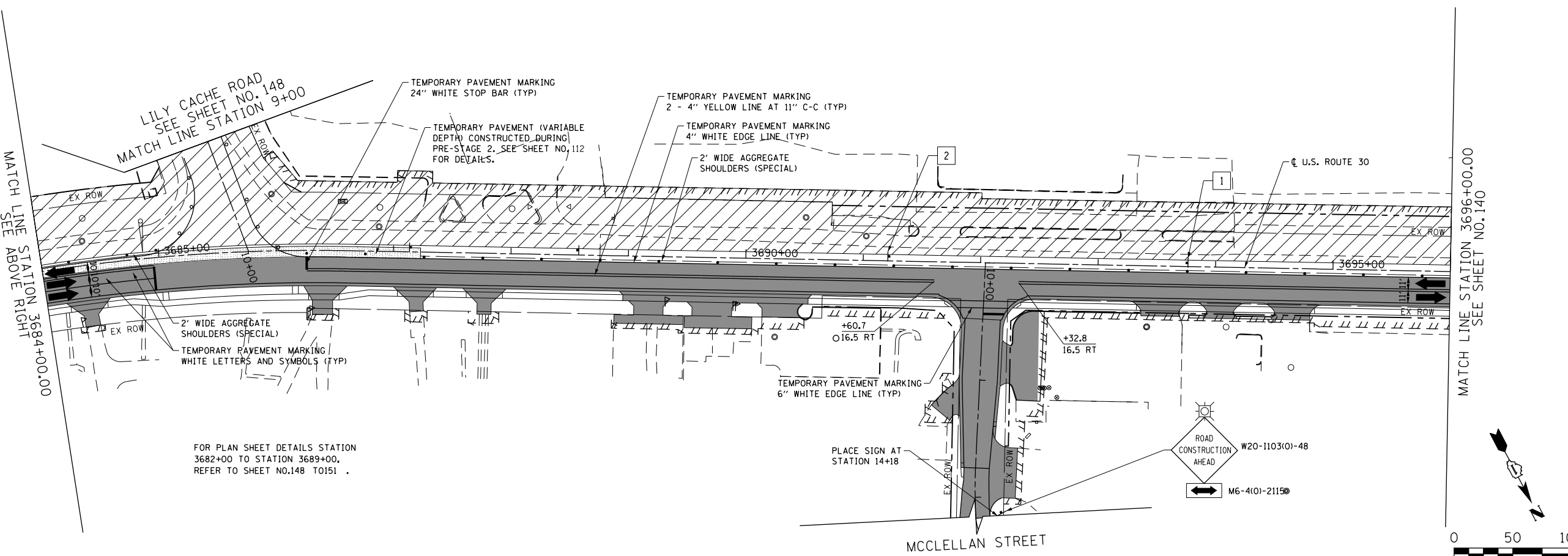
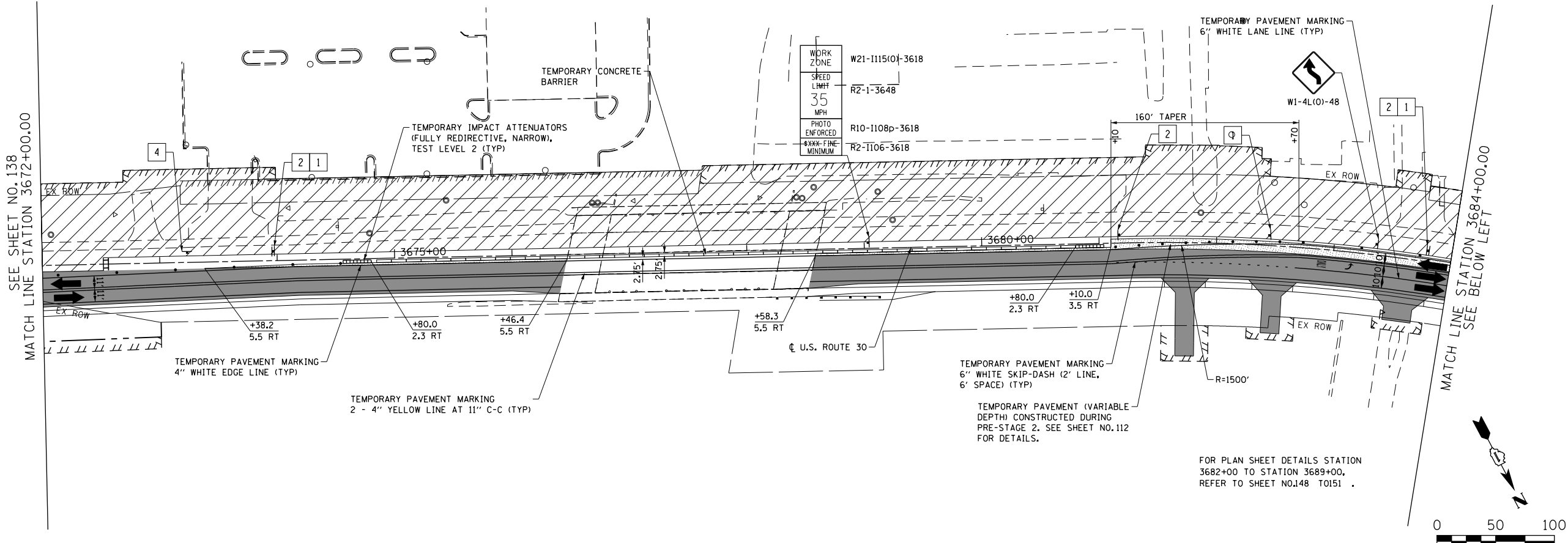
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	136
				CONTRACT NO. 60P95
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

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	PLOTTED	BY
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	CADD FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	BY
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	CADD FILE NAME	

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**LEGEND**

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID

1	DRIVEWAY ENTRANCE	30" X 30" SEE TC-26
2	DRIVEWAY ENTRANCE	30" X 30" SEE TC-26
3		R3-2L 24" X 24"
4		R3-2R 24" X 24"
5		R1-1
6		R6-1R 36" X 12"
7		R6-1L 36" X 12"



USER NAME = bshoeffler	DESIGNED - MNB	REVISED -
FILE = D160P95-SHT-MOT-STAGE2-02.dgn	DRAWN - MNB	REVISED -
PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -
PLOT DATE = 11/20/2014	DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

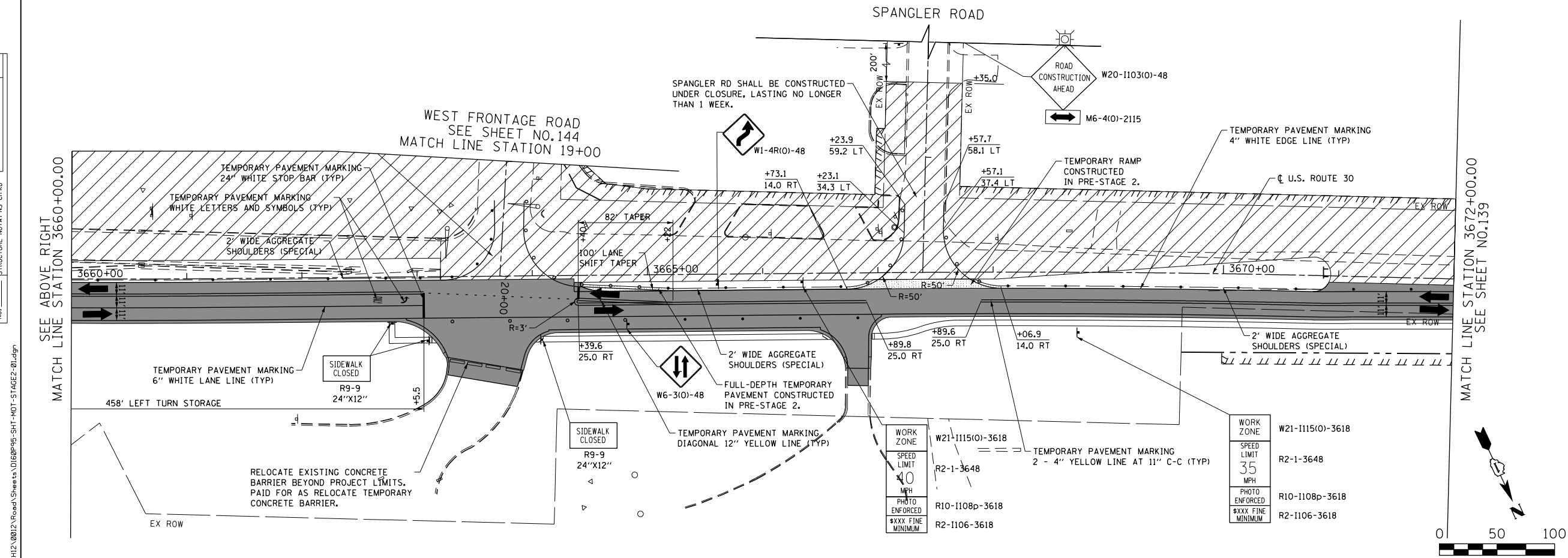
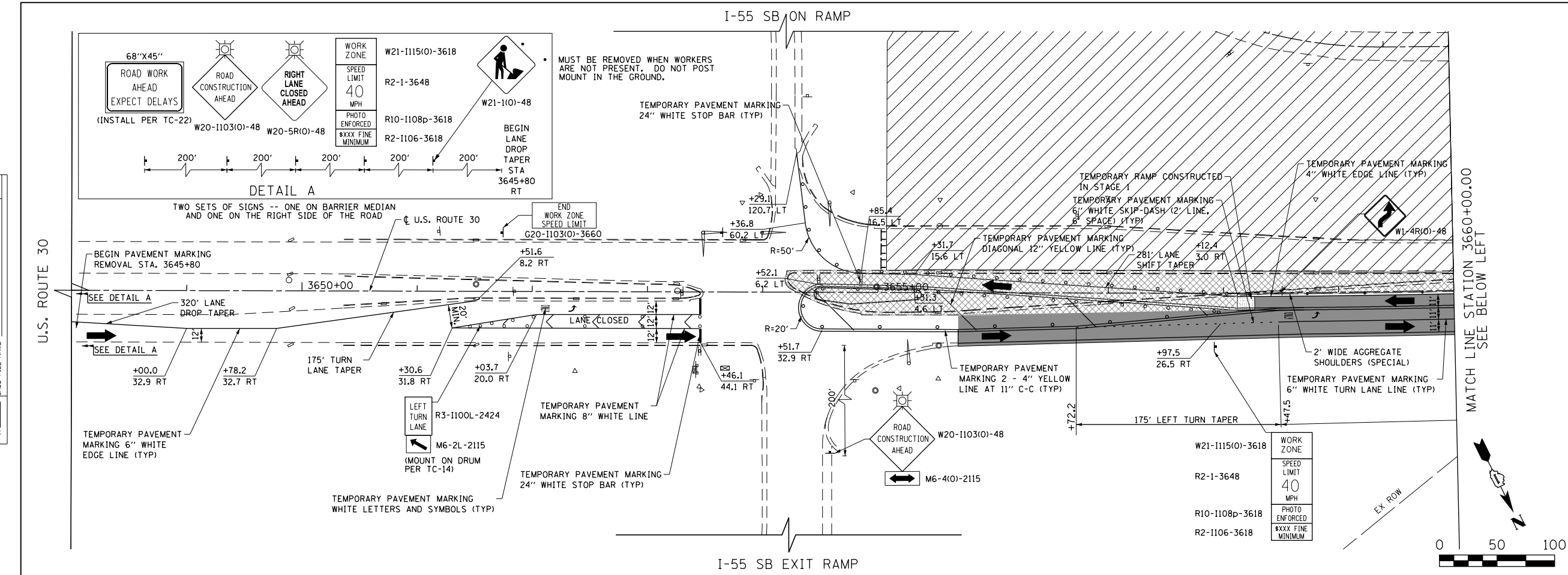
**U.S. ROUTE 30  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 2)**

SCALE: 1"=50' SHEET NAME: MOT-2-02 STA. 3672+00 TO STA. 3696+00

F.A.P. RTE. 575	SECTION 14W - R	COUNTY WILL	TOTAL SHEETS 681	SHEET NO. 139
CONTRACT NO. 60P95				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

DATE	
BY	
PLAN	SURVEYED
	PLOTTED
	NOTED
	FILED
	NO. _____
	DATE _____

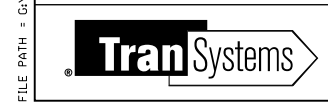
DATE	
BY	
PROFILE	SURVEYED
	PLOTTED
	NOTED
	FILED
	NO. _____
	DATE _____



**LEGEND**

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
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- DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID

- DRIVEWAY ENTRANCE 30' X 30" SEE TC-26
- DRIVEWAY ENTRANCE 30' X 30" SEE TC-26
- R3-2L 24" X 24"
- R3-2R 24" X 24"
- STOP R1-1
- ONE WAY R6-1R 36" X 12"
- ONE WAY R6-1L 36" X 12"



USER NAME = bshae1f1ger	DESIGNED - MNB	REVISED -
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PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**U.S. ROUTE 30  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 2)**

SCALE: 1"=50' SHEET NAME: MOT-2-01 STA. 3648+00 TO STA. 3672+00

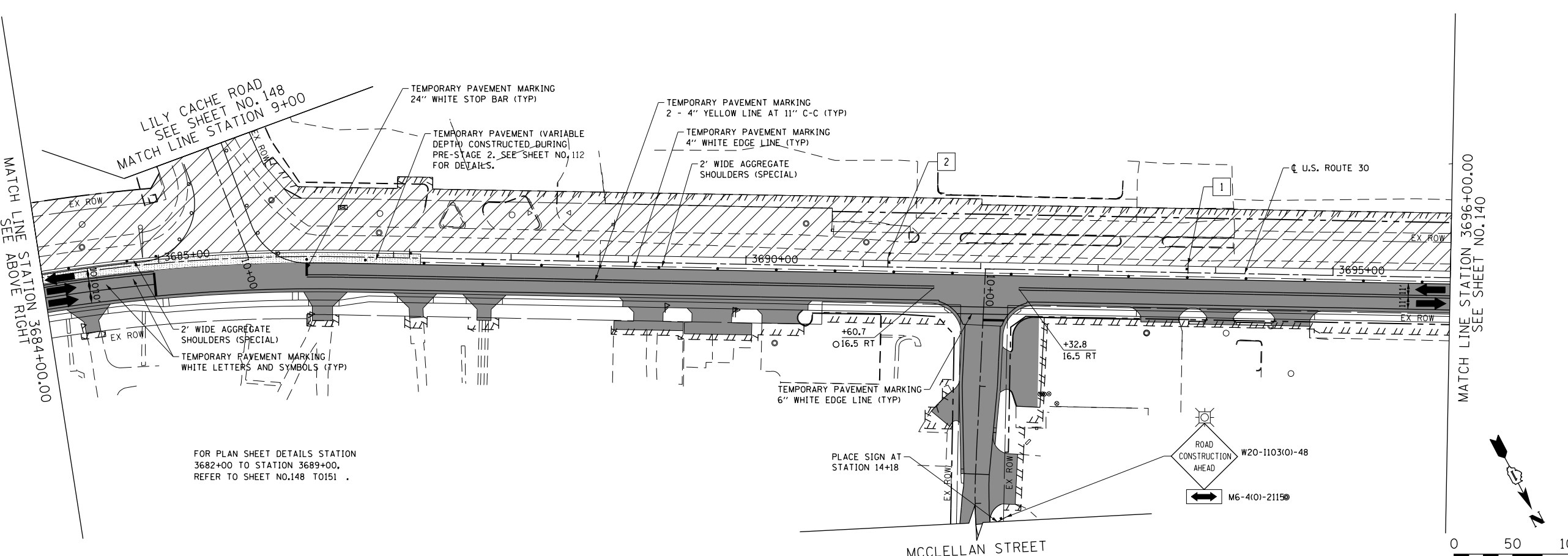
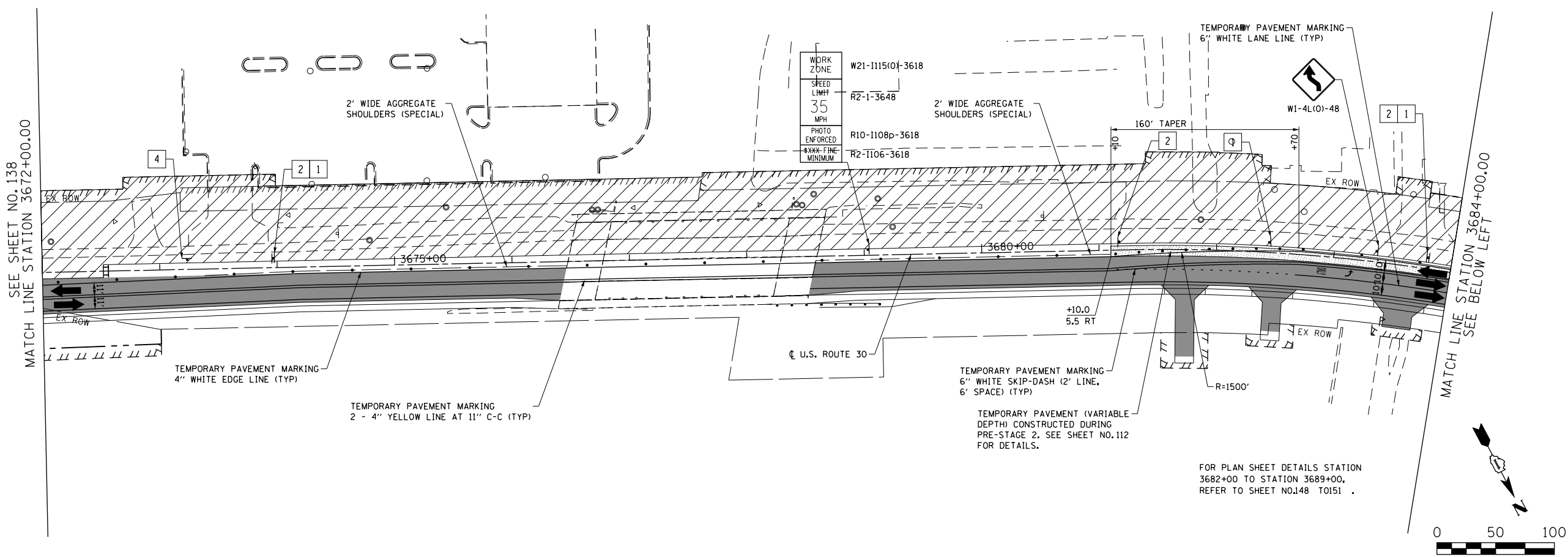
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	138
				CONTRACT NO. 60P95
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

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	PLOTTED	
	NOTED	
	BY	
	NO.	
	FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	
	NOTED	
	BY	
	NO.	
	FILE NAME	

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**LEGEND**

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID

1	DRIVEWAY ENTRANCE	30" X 30" SEE TC-26
2	DRIVEWAY ENTRANCE	30" X 30" SEE TC-26
3		R3-2L 24" X 24"
4		R3-2R 24" X 24"
5	STOP	R1-1
6	ONE WAY	R6-1R 36" X 12"
7	ONE WAY	R6-1L 36" X 12"



USER NAME = bshoeffler	DESIGNED - MNB	REVISED -
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PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

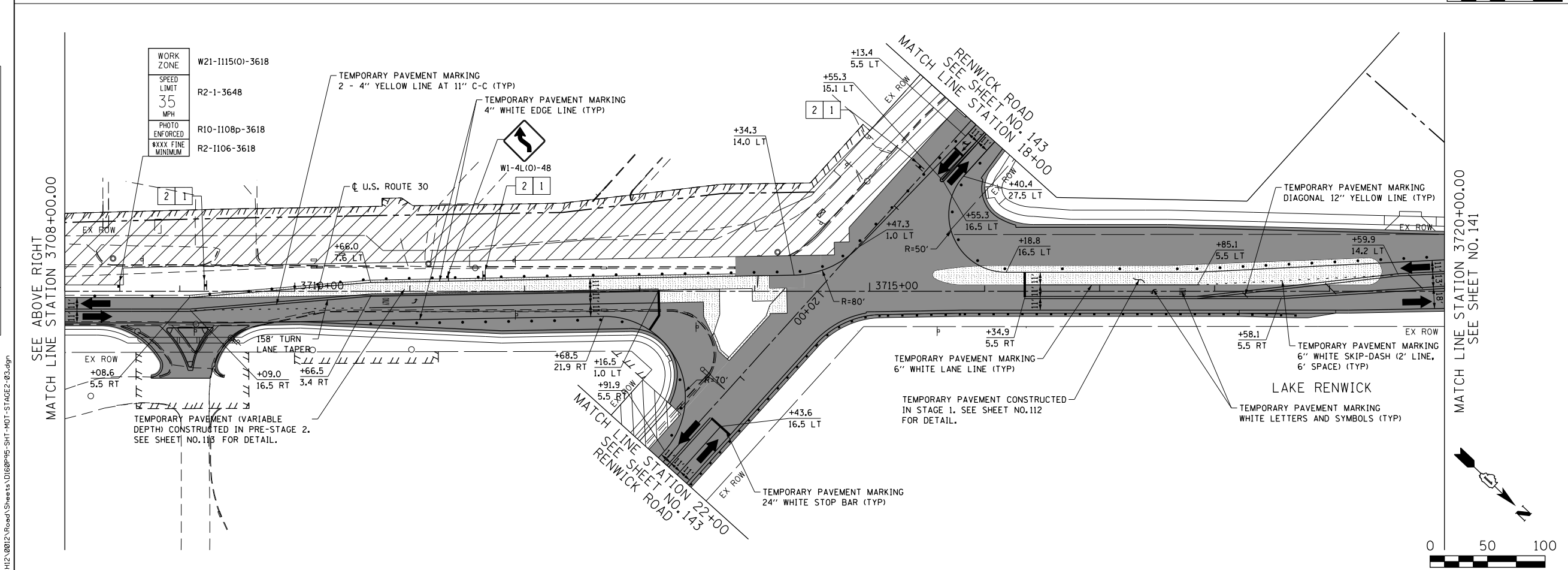
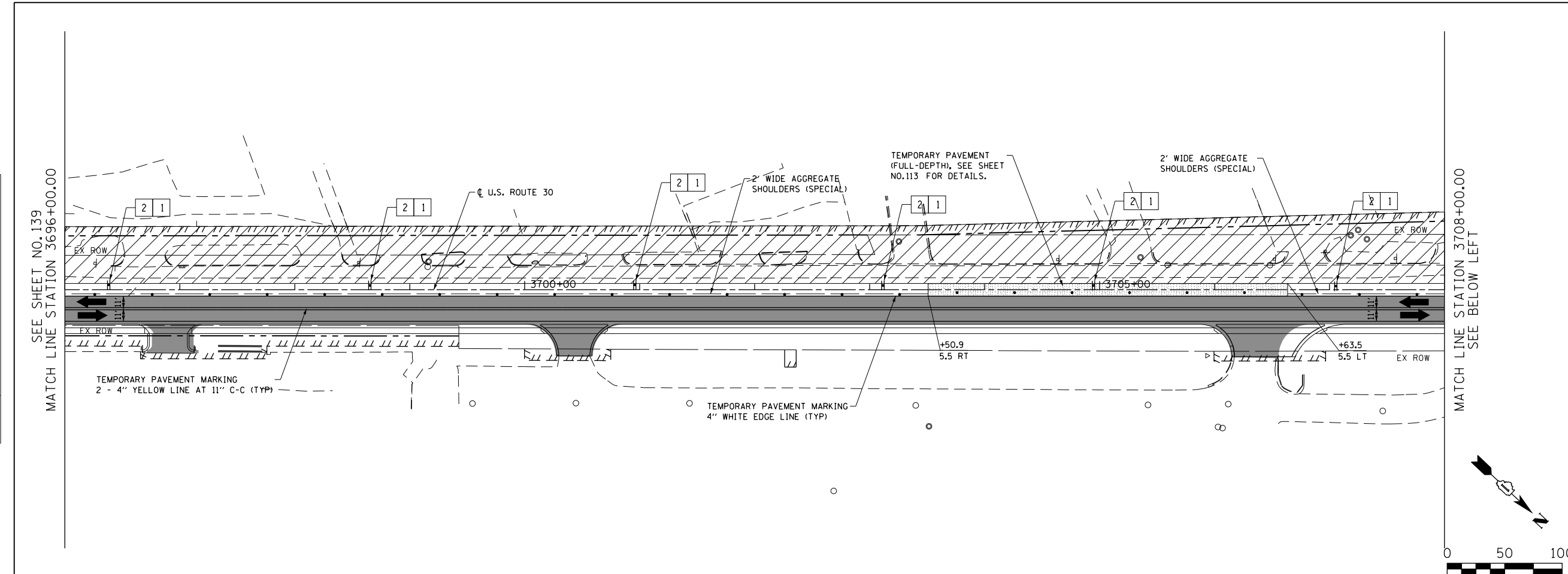
**U.S. ROUTE 30  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 2)**

SCALE: 1"=50' SHEET NAME: MOT-2-02 STA. 3672+00 TO STA. 3696+00

F.A.P. RTE. 575	SECTION 14W - R	COUNTY WILL	TOTAL SHEETS 681	SHEET NO. 139
CONTRACT NO. 60P95				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	BY
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	DATE FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	BY
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	DATE FILE NAME	



**LEGEND**

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID
- DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- R3-2L 24" X 24"
- R3-2R 24" X 24"
- R1-1
- R6-1R 36" X 12"
- R6-1L 36" X 12"



USER NAME = bshoeffler	DESIGNED - MNB	REVISED -
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PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**U.S. ROUTE 30  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 2)**

SCALE: 1"=50' SHEET NAME: MOT-2-03 STA. 3696+00 TO STA. 3720+00

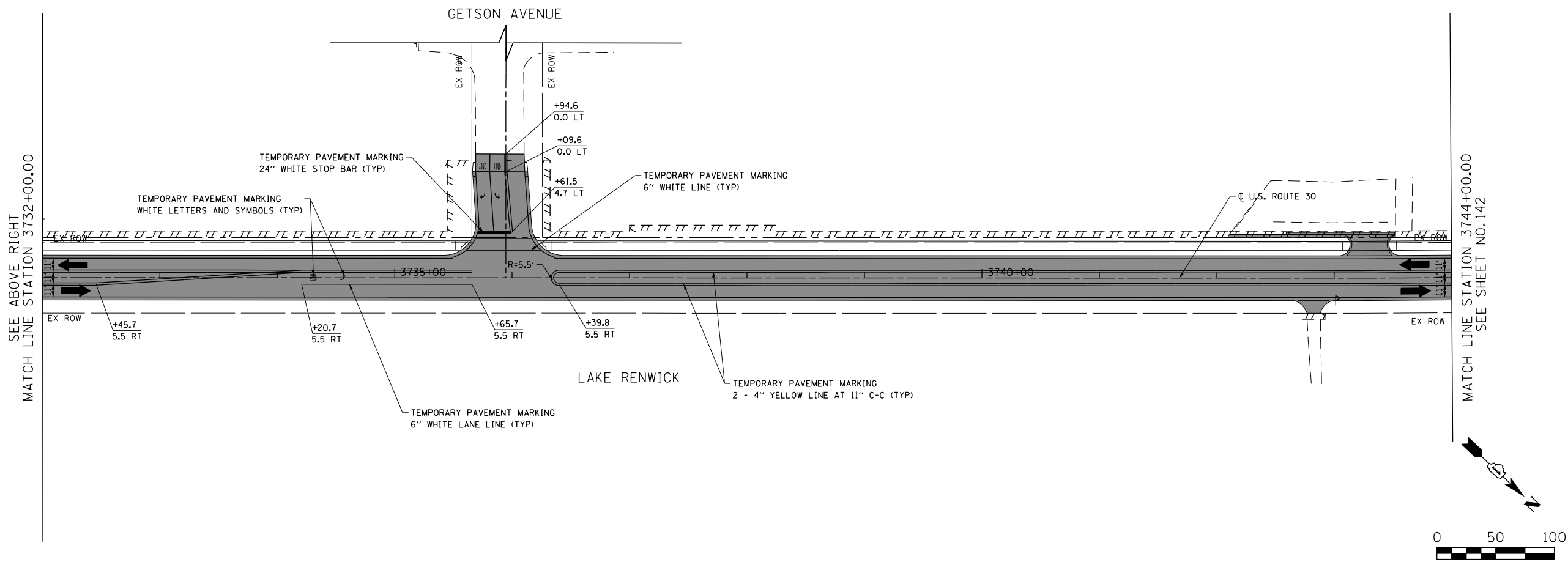
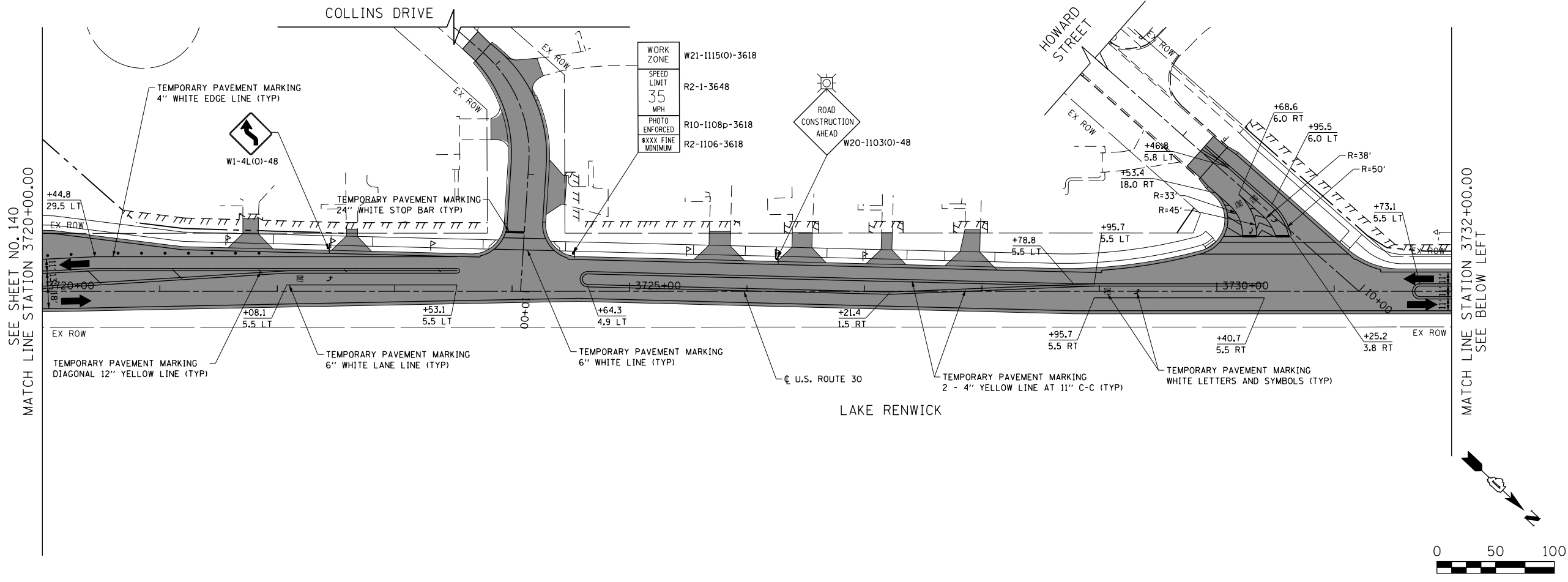
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	140
				CONTRACT NO. 60P95
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				



PLAN	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	BY	
	DATE	

PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	BY	
	DATE	

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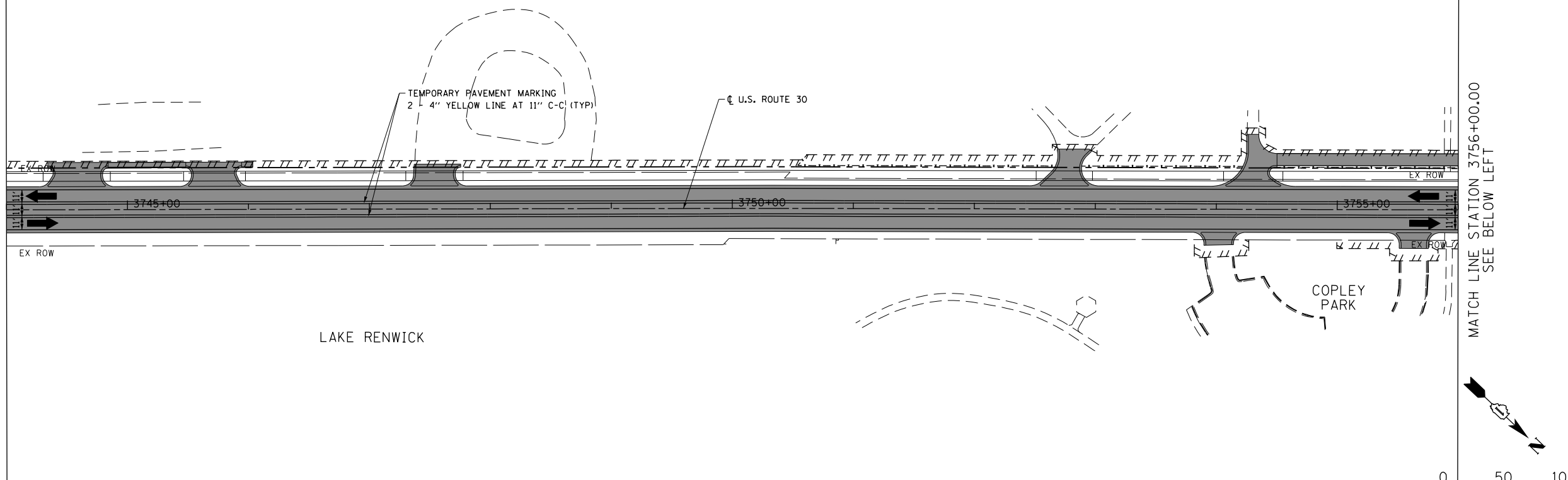
**LEGEND**

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONODIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID

- 1 DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- 2 DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- 3 R3-2L 24" X 24"
- 4 R3-2R 24" X 24"
- 5 STOP R1-1
- 6 ONE WAY R6-1R 36" X 12"
- 7 ONE WAY R6-1L 36" X 12"

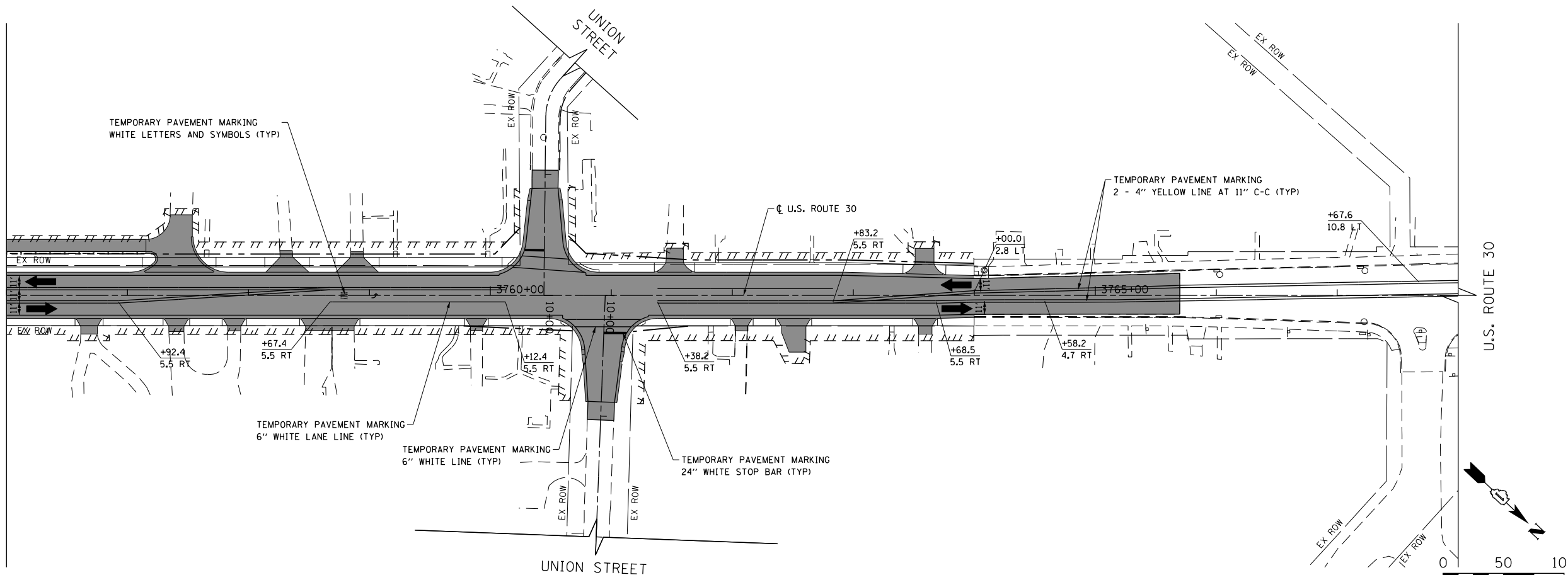
PLAN	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	BY	
	DATE	

SEE SHEET NO. 141  
MATCH LINE STATION 3744+00.00



PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	BY	
	DATE	

SEE ABOVE RIGHT  
MATCH LINE STATION 3756+00.00



LEGEND

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID
- DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- R3-2L 24" X 24"
- R3-2R 24" X 24"
- STOP R1-1
- ONE WAY R6-1R 36" X 12"
- ONE WAY R6-1L 36" X 12"

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FILE = D:\60P95-SHT-MOT-STAGE2-05.dgn	DRAWN - MNB	REVISED -
PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

U.S. ROUTE 30  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 2)

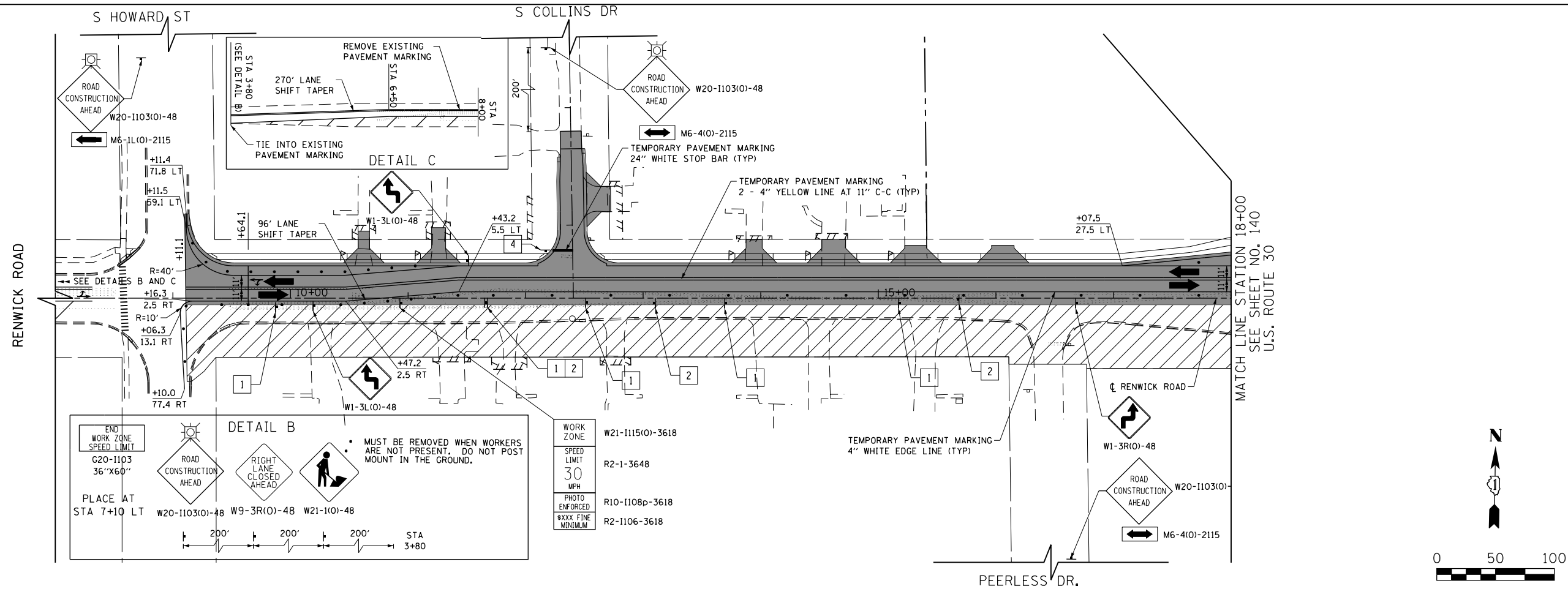
SCALE: 1"=50' SHEET NAME: MOT-2-05 STA. 3744+00 TO STA. 3763+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	142
			CONTRACT NO. 60P95	
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	
	NOTED	
	BY	
	NO. OF REVISIONS	
	AT OR MAY BE CHECKED	
	CADD FILE NAME	

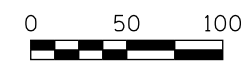
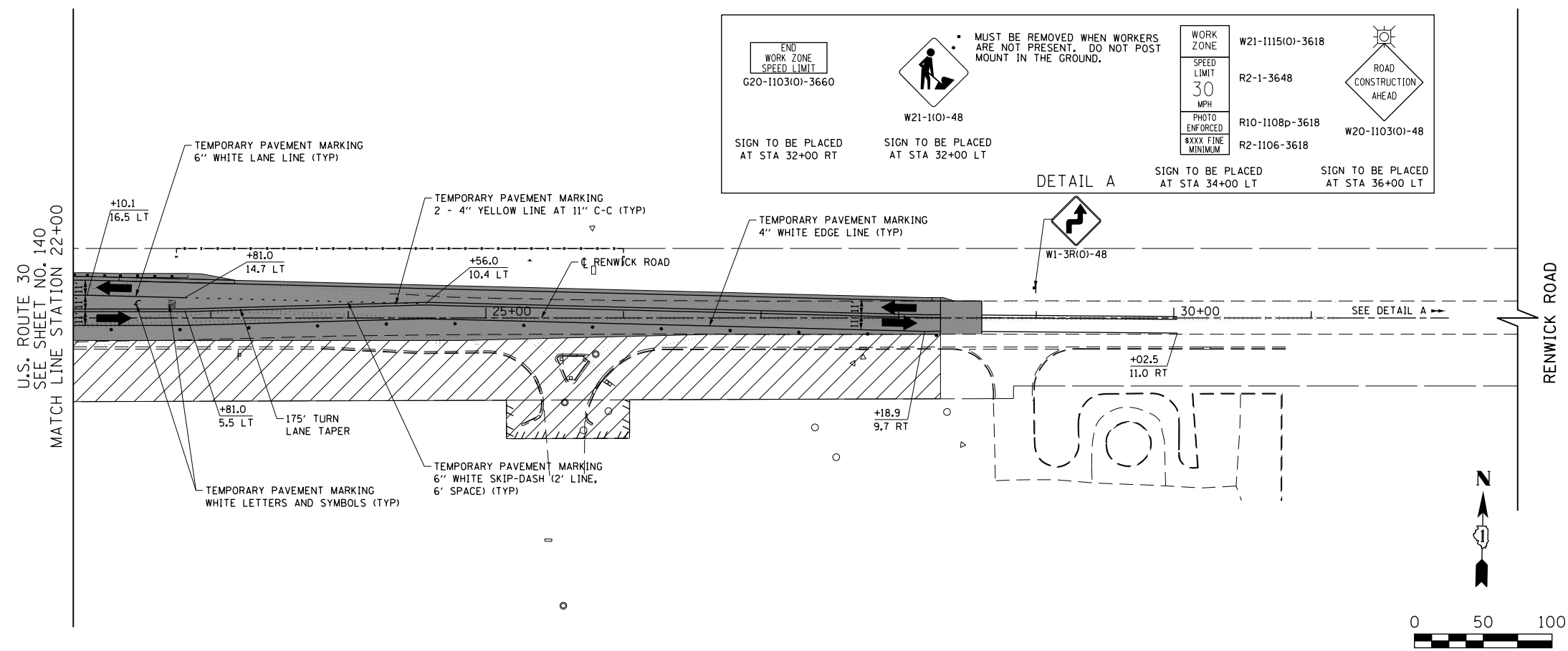
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	NO. OF REVISIONS	
	AT OR MAY BE CHECKED	
	CADD FILE NAME	

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**LEGEND**

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID



1	DRIVEWAY ENTRANCE	30" X 30" SEE TC-26
2	DRIVEWAY ENTRANCE	30" X 30" SEE TC-26
3		R3-2L 24" X 24"
4		R3-2R 24" X 24"
5	STOP	R1-1
6	ONE WAY	R6-1R 36" X 12"
7	ONE WAY	R6-1L 36" X 12"



USER NAME = bshoeffler	DESIGNED - MNB	REVISED -
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PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**RENWICK ROAD  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 2)**

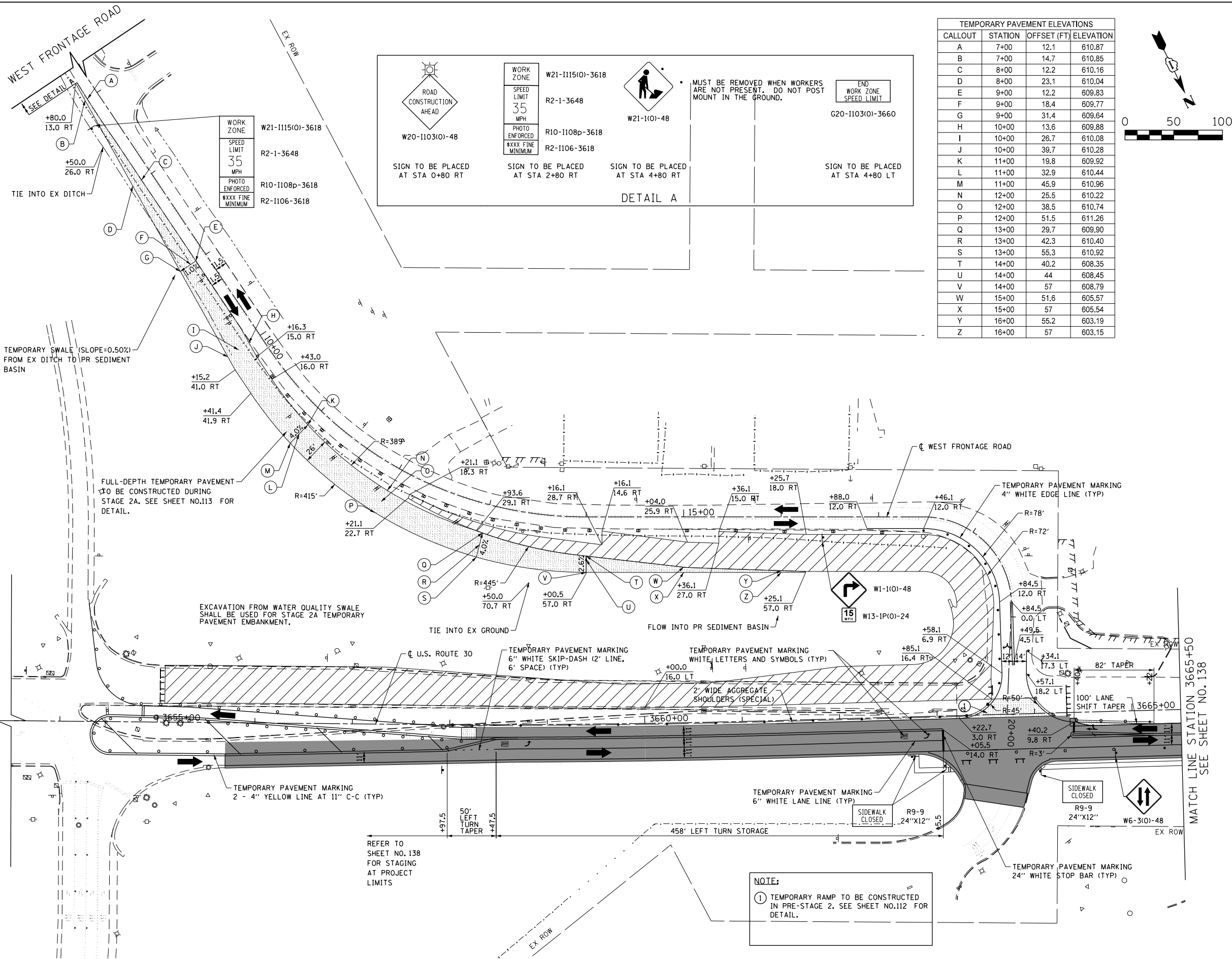
SCALE: 1"=50' SHEET NAME: MOT-2-06 STA. 8+00 TO STA. 32+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	143
				CONTRACT NO. 60P95
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	
	NOTED	
	BY	
	NO.	

PROFILE	SURVEYED	DATE
	PLOTTED	
	NOTED	
	BY	
	NO.	

FILE PATH = G:\CH2\0012\Road\Sheet\10160P95-SHT-MOT-STAGE2-07.dgn



**DETAIL A**

**ROAD CONSTRUCTION AHEAD**  
W20-1103(O)-48  
SIGN TO BE PLACED AT STA 0+80 RT

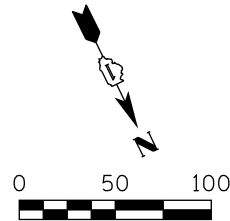
**WORK ZONE**  
W21-1115(O)-3618  
R2-1-3648  
R10-1108p-3618  
R2-1106-3618  
SPEED LIMIT 35 MPH  
PHOTO ENFORCED  
\$XXX FINE MINIMUM  
SIGN TO BE PLACED AT STA 2+80 RT

**W21-11(O)-48**  
SIGN TO BE PLACED AT STA 4+80 RT

**MUST BE REMOVED WHEN WORKERS ARE NOT PRESENT. DO NOT POST MOUNT IN THE GROUND.**

**END WORK ZONE SPEED LIMIT**  
G20-1103(O)-3660  
SIGN TO BE PLACED AT STA 4+80 LT

TEMPORARY PAVEMENT ELEVATIONS			
CALLOUT	STATION	OFFSET (FT)	ELEVATION
A	7+00	12.1	610.87
B	7+00	14.7	610.85
C	8+00	12.2	610.16
D	8+00	23.1	610.04
E	9+00	12.2	609.83
F	9+00	18.4	609.77
G	9+00	31.4	609.64
H	10+00	13.6	609.88
I	10+00	26.7	610.08
J	10+00	39.7	610.28
K	11+00	19.8	609.92
L	11+00	32.9	610.44
M	11+00	45.9	610.96
N	12+00	25.5	610.22
O	12+00	38.5	610.74
P	12+00	51.5	611.26
Q	13+00	29.7	609.90
R	13+00	42.3	610.40
S	13+00	55.3	610.92
T	14+00	40.2	608.35
U	14+00	44	608.45
V	14+00	57	608.79
W	15+00	51.6	605.57
X	15+00	57	605.54
Y	16+00	55.2	603.19
Z	16+00	57	603.15



**LEGEND**

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID
- DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- R3-2L 24" X 24"
- R3-2R 24" X 24"
- STOP R1-1
- ONE WAY R6-1R 36" X 12"
- ONE WAY R6-1L 36" X 12"



USER NAME = bshoeffler	DESIGNED - MNB	REVISED -
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PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**WEST FRONTAGE ROAD**  
**SUGGESTED TRAFFIC CONTROL PLANS (STAGE 2A)**

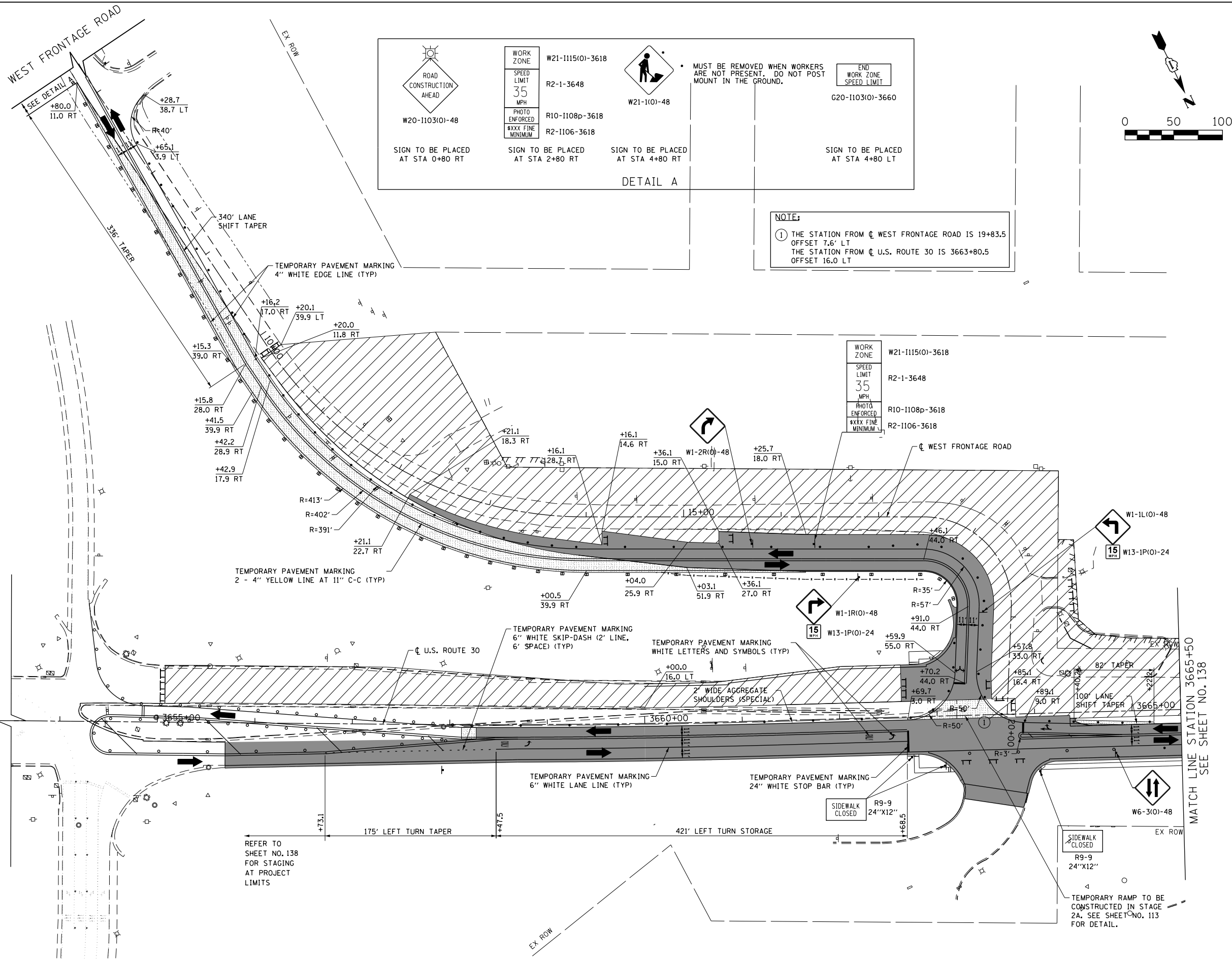
SCALE: 1"=50' SHEET NAME: MOT-2-07 STA. 6+80 TO STA. 20+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	144
CONTRACT NO. 60P95				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	BY
	NOTED	
	FILED	
	NO. _____	
	DATE _____	

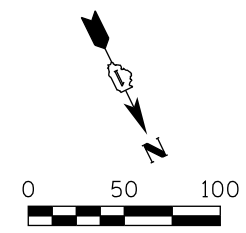
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	PLOTTED	BY
	NOTED	
	FILED	
	NO. _____	
	DATE _____	

FILE PATH = G:\CH2\0012\Road\Sheet\14\160P95-SHT-MOT-STAGE2-08.dgn



 W20-1103(O)-48 SIGN TO BE PLACED AT STA 0+80 RT	WORK ZONE W21-1115(O)-3618 SPEED LIMIT 35 MPH PHOTO ENFORCED \$XXX FINE MINIMUM	 W21-1(O)-48 SIGN TO BE PLACED AT STA 4+80 RT	END WORK ZONE SPEED LIMIT G20-1103(O)-3660 SIGN TO BE PLACED AT STA 4+80 LT
	R2-1-3648 R10-1108p-3618 R2-1106-3618 SIGN TO BE PLACED AT STA 2+80 RT		

**NOTE:**  
 ① THE STATION FROM  $\text{C}$  WEST FRONTAGE ROAD IS 19+83.5 OFFSET 7.6' LT  
 THE STATION FROM  $\text{C}$  U.S. ROUTE 30 IS 3663+80.5 OFFSET 16.0 LT



**LEGEND**

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID
- DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- R3-2L 24" X 24"
- R3-2R 24" X 24"
- R1-1
- R6-1R 36" X 12"
- R6-1L 36" X 12"



USER NAME = bshoeffler	DESIGNED - MNB	REVISED -
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PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**WEST FRONTAGE ROAD  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 2B)**

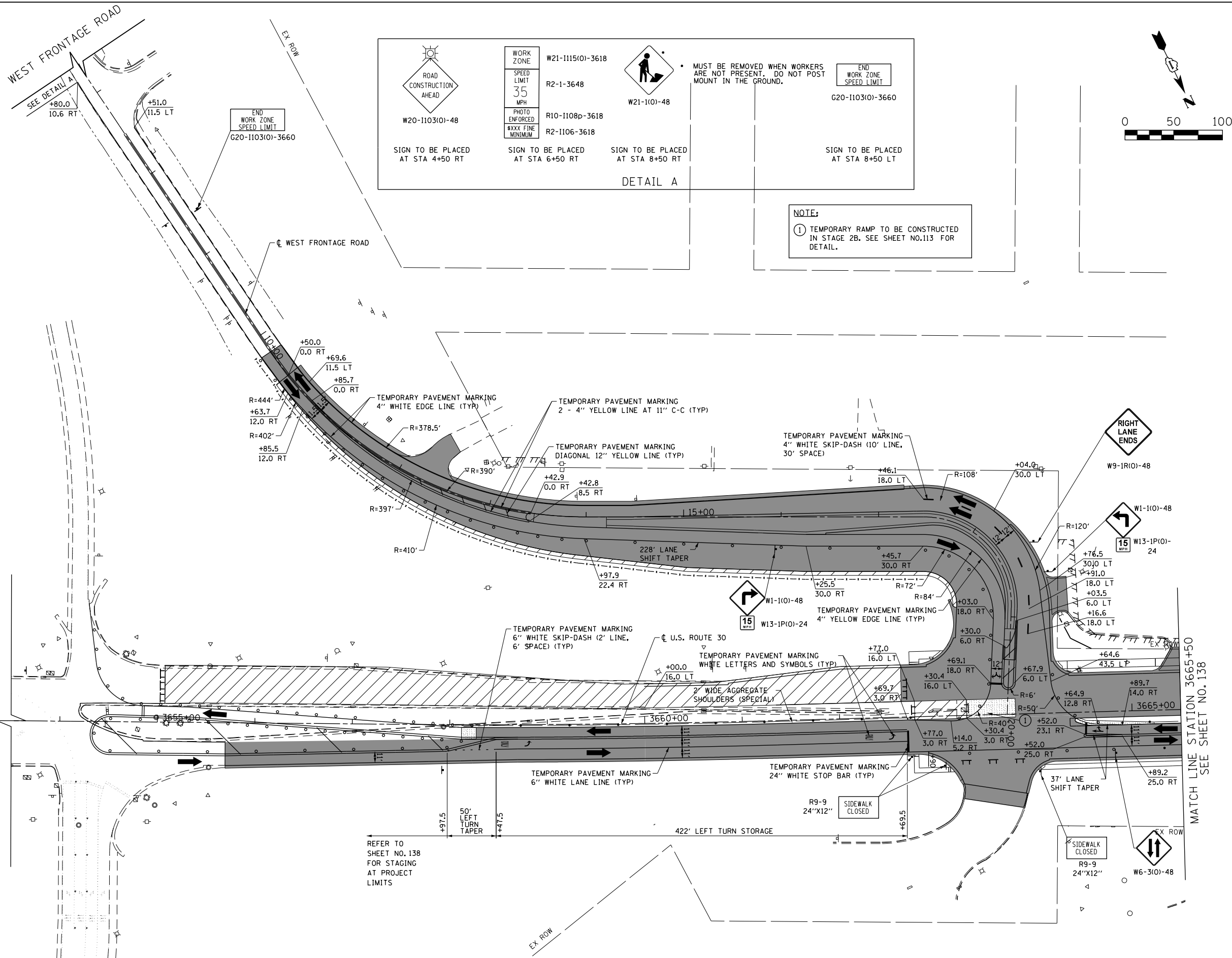
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	145
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60P95	

PLAN	SURVEYED	DATE
	PLOTTED	BY
	NOTED	
	FILED	
	NO. OF REVISIONS	
	NO. OF SHEETS	
	DATE	
	BY	
	FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	BY
	NOTED	
	FILED	
	NO. OF REVISIONS	
	NO. OF SHEETS	
	DATE	
	BY	
	FILE NAME	

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**DETAIL A**

WORK ZONE	W21-1115(O)-3618
SPEED LIMIT	R2-1-3648
PHOTO ENFORCED	R10-1108p-3618
MINIMUM	R2-1106-3618

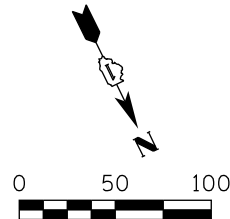
ROAD CONSTRUCTION AHEAD  
W20-1103(O)-48  
SIGN TO BE PLACED AT STA 4+50 RT

W21-1(O)-48  
SIGN TO BE PLACED AT STA 8+50 RT

MUST BE REMOVED WHEN WORKERS ARE NOT PRESENT. DO NOT POST MOUNT IN THE GROUND.

END WORK ZONE SPEED LIMIT  
G20-1103(O)-3660  
SIGN TO BE PLACED AT STA 8+50 LT

**NOTE:**  
① TEMPORARY RAMP TO BE CONSTRUCTED IN STAGE 2B. SEE SHEET NO.113 FOR DETAIL.



**LEGEND**

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID
- DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- R3-2L 24" X 24"
- R3-2R 24" X 24"
- R1-1
- R6-1R 36" X 12"
- R6-1L 36" X 12"



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PLOT SCALE = 100.0000' / 1" =	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**WEST FRONTAGE ROAD  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 2C)**

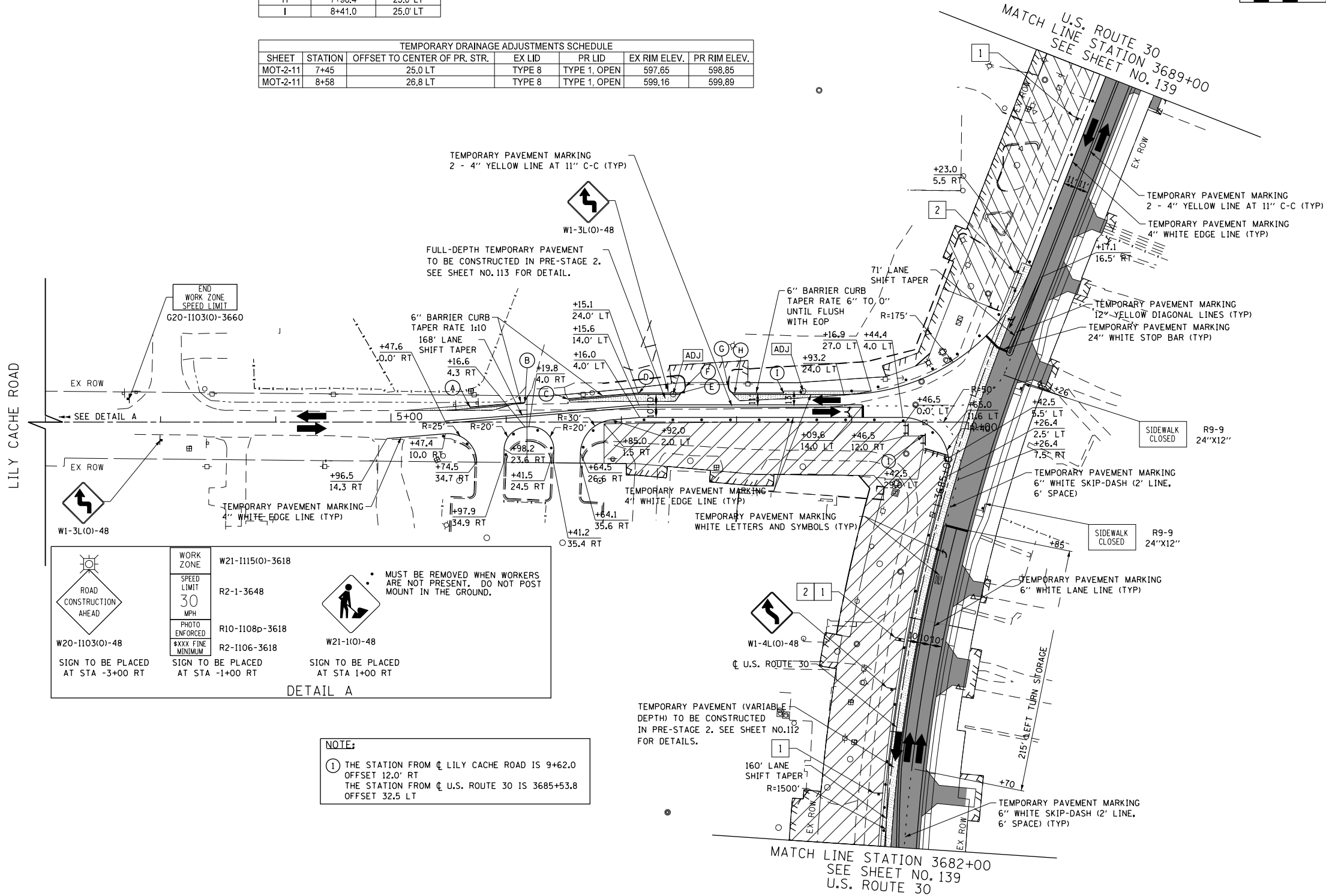
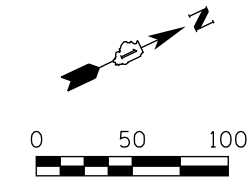
SCALE: 1"=50' SHEET NAME: MOT-2-09 STA. 6+80 TO STA. 20+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	146
				CONTRACT NO. 60P95
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				



FACE OF BARRIER CURB		
POINT	STATION	OFFSET
A	5+68.6	12.8' LT
B	6+18.2	16.8' LT
C	6+53.9	19.9' LT
D	7+15.1	25.0' LT
E	7+50.2	25.0' LT
F	7+55.7	30.7' LT
G	7+92.9	30.6' LT
H	7+98.4	25.0' LT
I	8+41.0	25.0' LT

TEMPORARY DRAINAGE ADJUSTMENTS SCHEDULE						
SHEET	STATION	OFFSET TO CENTER OF PR. STR.	EX LID	PR LID	EX RIM ELEV.	PR RIM ELEV.
MOT-2-11	7+45	25.0 LT	TYPE 8	TYPE 1, OPEN	597.65	598.85
MOT-2-11	8+58	26.8 LT	TYPE 8	TYPE 1, OPEN	599.16	599.89



**DETAIL A**

	WORK ZONE	W21-1115(O)-3618
	SPEED LIMIT	R2-1-3648
	PHOTO ENFORCED	R10-1108p-3618
	XXXX FINE MINIMUM	R2-1106-3618

W20-1103(O)-48  
SIGN TO BE PLACED AT STA -3+00 RT

W21-1101-48  
SIGN TO BE PLACED AT STA +1+00 RT

MUST BE REMOVED WHEN WORKERS ARE NOT PRESENT. DO NOT POST MOUNT IN THE GROUND.

**NOTE:**

① THE STATION FROM  $\phi$  LILY CACHE ROAD IS 9+62.0 OFFSET 12.0' RT  
 THE STATION FROM  $\phi$  U.S. ROUTE 30 IS 3685+53.8 OFFSET 32.5 LT

**LEGEND**

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT  $\phi$  50" C-C (TYP), 25" C-C IN CURVES AND RADII, AND 20" C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT  $\phi$  50" C-C (TYP), 25" C-C IN CURVES AND RADII, AND 20" C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT  $\phi$  50" C-C (TYP), 25" C-C IN CURVES AND RADII, AND 20" C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT  $\phi$  50" C-C (TYP), 25" C-C IN CURVES AND RADII, AND 20" C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID

1	DRIVEWAY ENTRANCE	30" X 30" SEE TC-26
2	DRIVEWAY ENTRANCE	30" X 30" SEE TC-26
3		R3-2L 24" X 24"
4		R3-2R 24" X 24"
5	STOP	R1-1
6	ONE WAY	R6-1R 36" X 12"
7	ONE WAY	R6-1L 36" X 12"

DATE	
BY	
SURVEYED	
PLANNED	
NOTED	
FILED	

DATE	
BY	
SURVEYED	
PLANNED	
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FILED	

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PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

<b>LILY CACHE ROAD</b>	
<b>SUGGESTED TRAFFIC CONTROL PLANS (STAGE 2A)</b>	
SCALE: 1"=50'	SHEET NAME: MOT-2-11
STA. 3+00	TO STA. 10+00

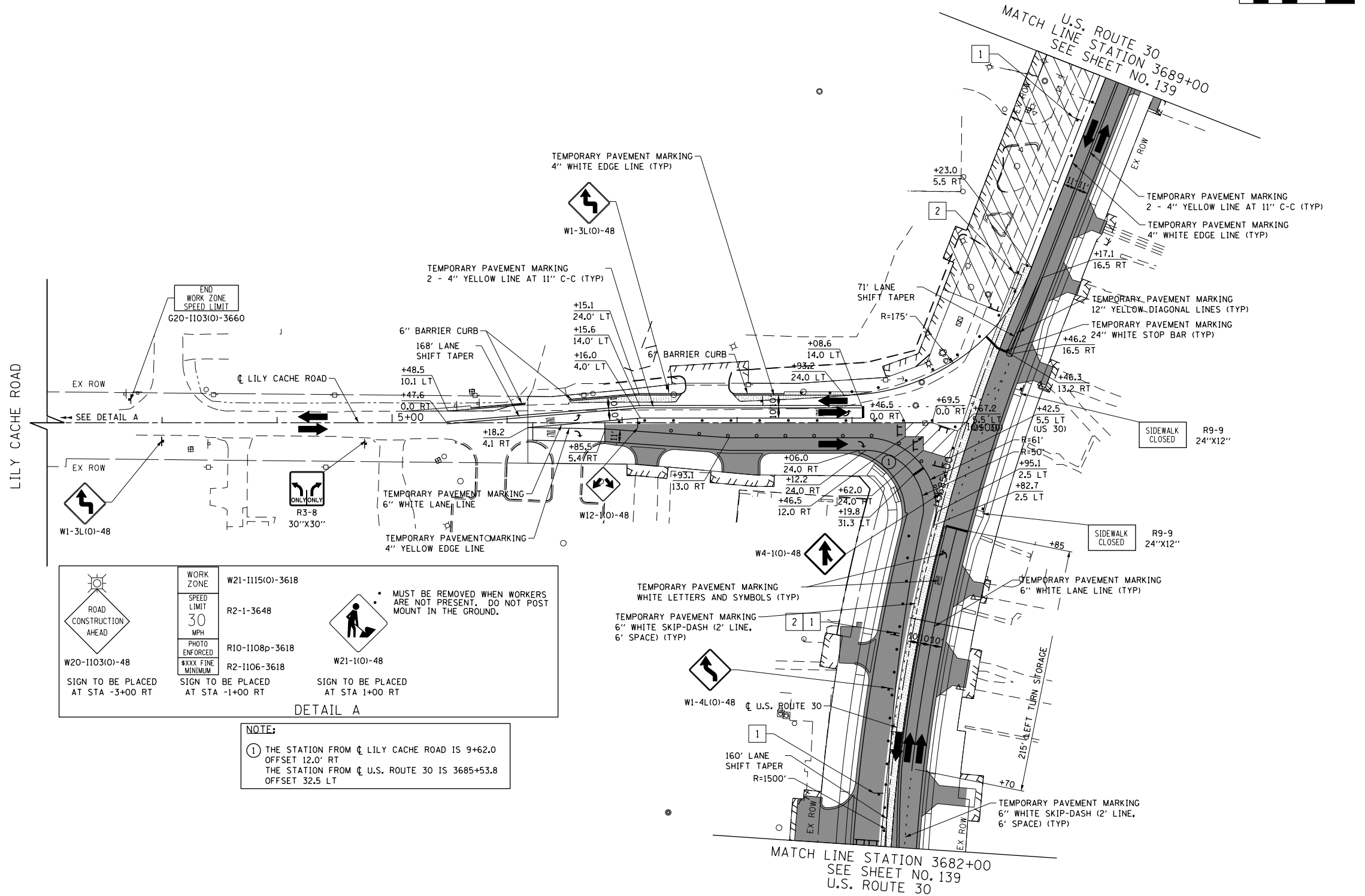
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	148
CONTRACT NO. 60P95				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				



PLAN	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	BY	
	DATE	

PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	BY	
	DATE	

FILE PATH = G:\CH2\0012\Road\Sheet\1160P95-SHT-MOT-STAGE2-12.dgn



	WORK ZONE W21-1115(O)-3618	MUST BE REMOVED WHEN WORKERS ARE NOT PRESENT. DO NOT POST MOUNT IN THE GROUND.
	R2-1-3648	
	R10-1108p-3618	
	R2-1106-3618	
W20-1103(O)-48 SIGN TO BE PLACED AT STA -3+00 RT	W21-1(O)-48 SIGN TO BE PLACED AT STA 1+00 RT	

**NOTE:**  
 ① THE STATION FROM CL LILY CACHE ROAD IS 9+62.0 OFFSET 12.0' RT  
 THE STATION FROM CL U.S. ROUTE 30 IS 3685+53.8 OFFSET 32.5' LT

**LEGEND**

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID

1 DRIVEWAY ENTRANCE 30" X 30" SEE TC-26

2 DRIVEWAY ENTRANCE 30" X 30" SEE TC-26

3 R3-2L 24" X 24"

4 R3-2R 24" X 24"

5 STOP R1-1

6 ONE WAY R6-1R 36" X 12"

7 ONE WAY R6-1L 36" X 12"



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FILE = D:\60P95-SHT-MOT-STAGE2-12.dgn	DRAWN - MNB	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

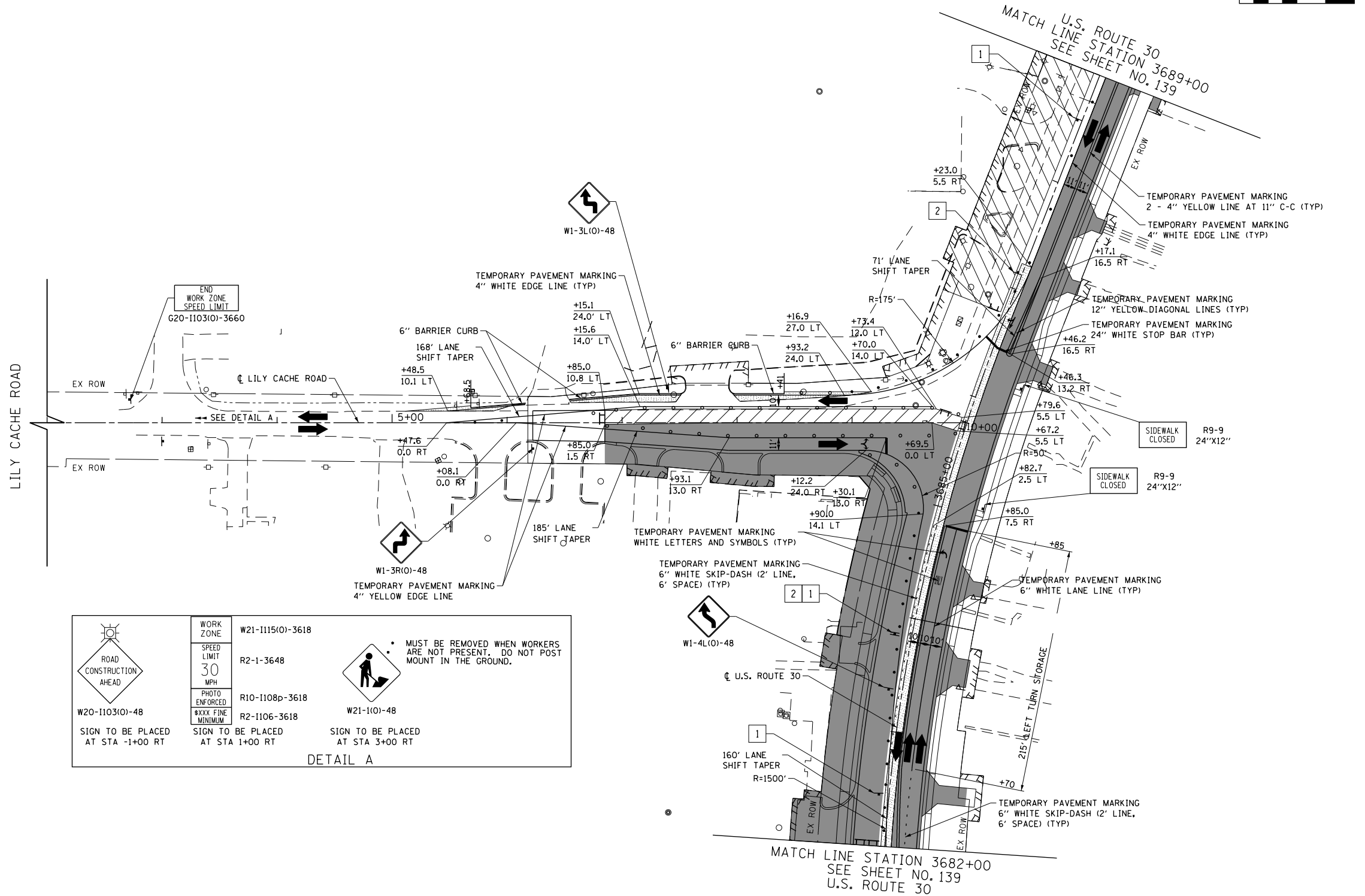
<b>LILY CACHE ROAD</b>	
<b>SUGGESTED TRAFFIC CONTROL PLANS (STAGE 2B)</b>	
SCALE: 1"=50'	SHEET NAME: MOT-2-12
STA. 3+00	TO STA. 10+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	149
CONTRACT NO. 60P95				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	STRUCTURE NOTATIONS CHECKED		
	NOTE BOOK NO.		
	CADD FILE NAME		

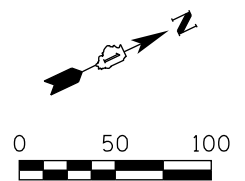
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	PLOTTED		
	GRADES CHECKED		
	STRUCTURE NOTATIONS CHECKED		
	NOTE BOOK NO.		
	CADD FILE NAME		

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**DETAIL A**

	<b>WORK ZONE</b> W21-115(O)-3618	<ul style="list-style-type: none"> <li>MUST BE REMOVED WHEN WORKERS ARE NOT PRESENT. DO NOT POST MOUNT IN THE GROUND.</li> </ul>
	R2-1-3648	
	R10-1108p-3618	
	R2-1106-3618	
	W20-1103(O)-48	
	W21-110-48	



**LEGEND**

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID

1		DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
2		DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
3		R3-2L 24" X 24"
4		R3-2R 24" X 24"
5		R1-1
6		R6-1R 36" X 12"
7		R6-1L 36" X 12"



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PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**LILY CACHE ROAD  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 2C)**

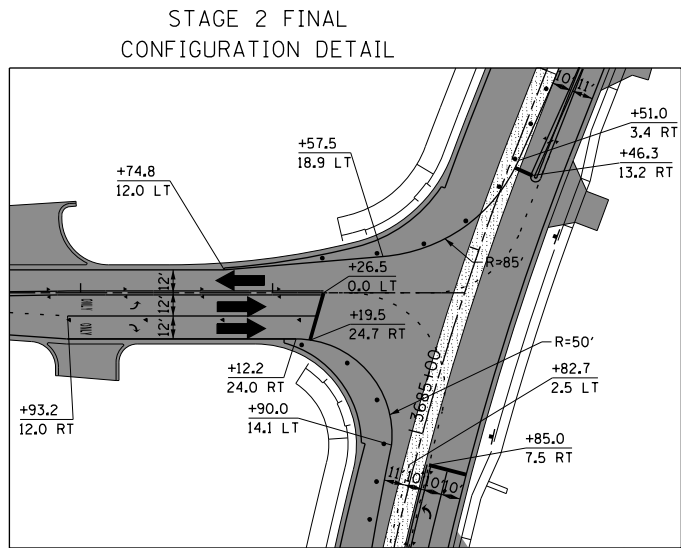
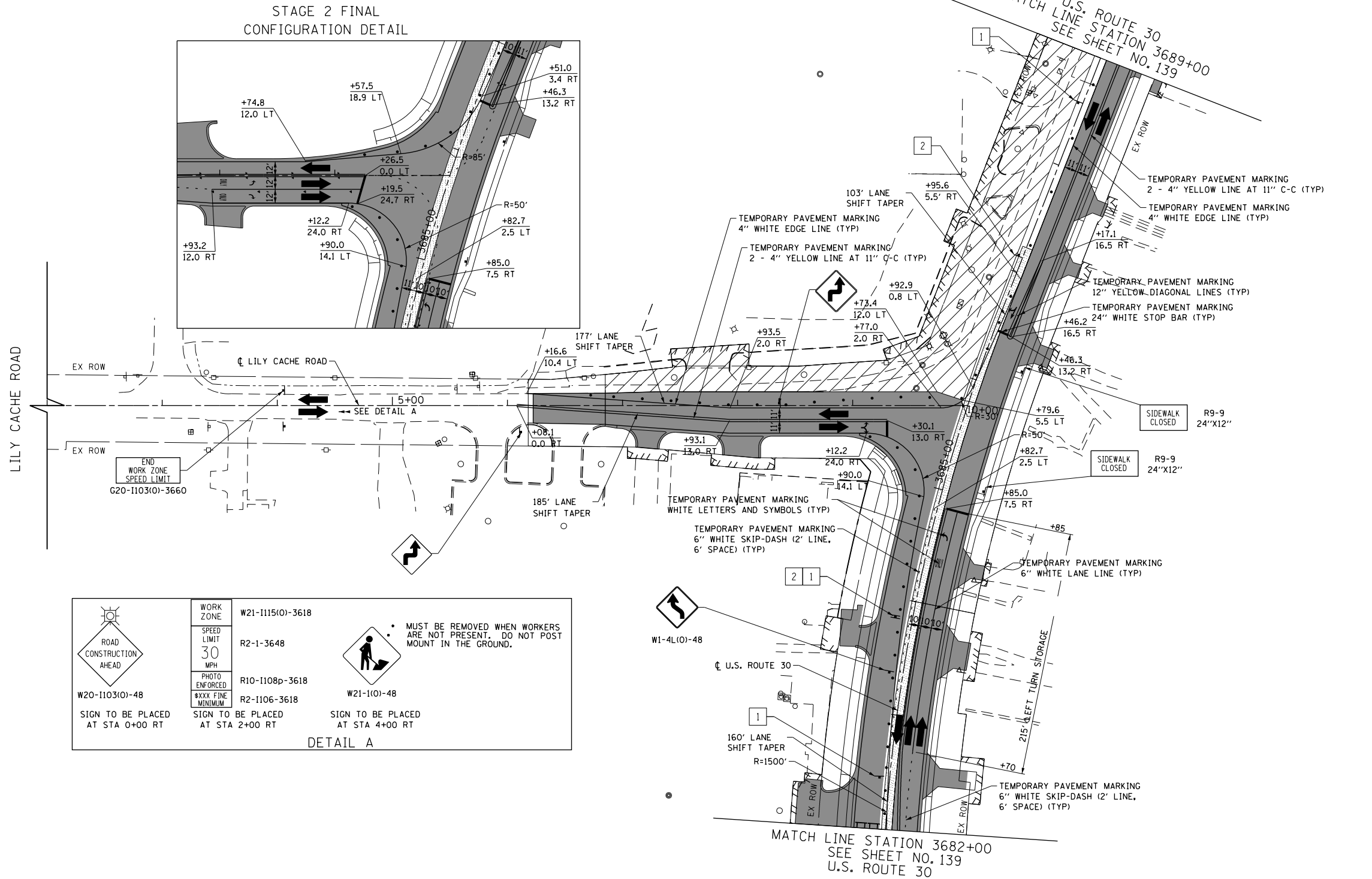
SCALE: 1"=50' SHEET NAME: MOT-2-13 STA. 3+00 TO STA. 10+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	150
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60P95	

PLAN	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	BY	
	DATE	

PROFILE	SURVEYED	DATE
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	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	BY	
	DATE	

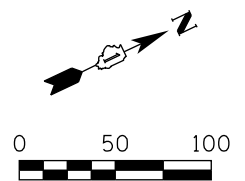
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 W20-1103(O)-48 SIGN TO BE PLACED AT STA 0+00 RT	WORK ZONE W21-1115(O)-3618	 W21-1101(O)-48 SIGN TO BE PLACED AT STA 4+00 RT
	SPEED LIMIT 30 MPH R2-1-3648	
	PHOTO ENFORCED \$XXX FINE MINIMUM R10-1108p-3618 R2-1106-3618	
	SIGN TO BE PLACED AT STA 2+00 RT	

MUST BE REMOVED WHEN WORKERS ARE NOT PRESENT. DO NOT POST MOUNT IN THE GROUND.

DETAIL A



**LEGEND**

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID

1	DRIVEWAY ENTRANCE	30" X 30" SEE TC-26
2	DRIVEWAY ENTRANCE	30" X 30" SEE TC-26
3		R3-2L 24" X 24"
4		R3-2R 24" X 24"
5		R1-1
6		R6-1R 36" X 12"
7		R6-1L 36" X 12"



USER NAME = bshoeffliger	DESIGNED - MNB	REVISED -
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PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**LILY CACHE ROAD  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 2D)**

SCALE: 1"=50' SHEET NAME: MOT-2-14 STA. 3+00 TO STA. 10+00

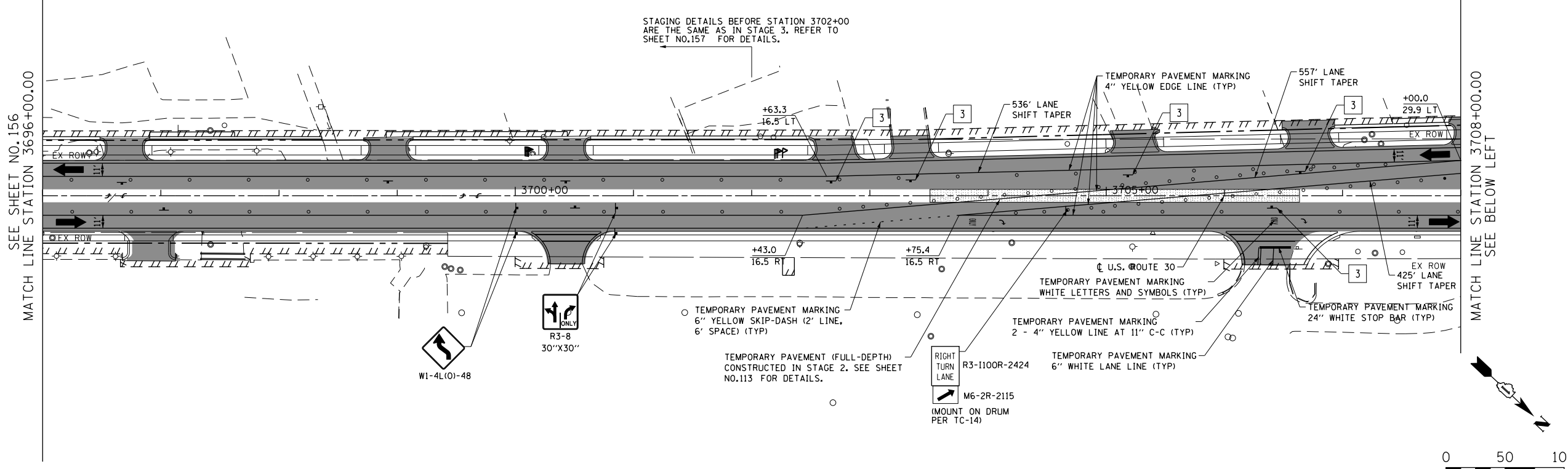
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	151
CONTRACT NO. 60P95				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	
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	NO.	
	FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	
	NOTED	
	BY	
	NO.	
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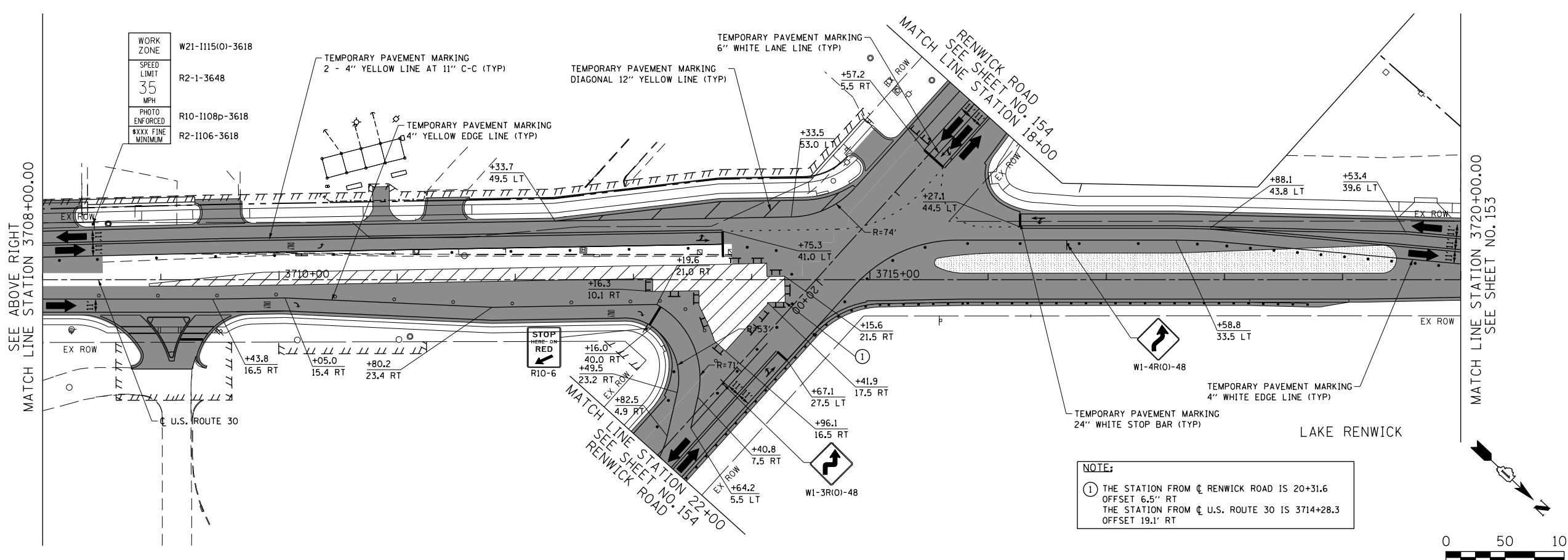
SEE SHEET NO. 156  
MATCH LINE STATION 3696+00.00

MATCH LINE STATION 3708+00.00  
SEE BELOW LEFT



SEE ABOVE RIGHT  
MATCH LINE STATION 3708+00.00

MATCH LINE STATION 3720+00.00  
SEE SHEET NO. 153



NOTE:  
① THE STATION FROM  $\phi$  RENWICK ROAD IS 20+31.6  
OFFSET 6.5' RT  
THE STATION FROM  $\phi$  U.S. ROUTE 30 IS 3714+28.3  
OFFSET 19.1' RT

LEGEND

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT  $\phi$  50" C-C (TYP), 25" C-C IN CURVES AND RADII, AND 20" C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT  $\phi$  50" C-C (TYP), 25" C-C IN CURVES AND RADII, AND 20" C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT  $\phi$  50" C-C (TYP), 25" C-C IN CURVES AND RADII, AND 20" C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT  $\phi$  50" C-C (TYP), 25" C-C IN CURVES AND RADII, AND 20" C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID
- DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- R3-2L 24" X 24"
- R3-2R 24" X 24"
- STOP R1-1
- ONE WAY R6-1R 36" X 12"
- ONE WAY R6-1L 36" X 12"



USER NAME = bshoeffler	DESIGNED - MNB	REVISED -
FILE = D:\60P95-SHT-MOT-STAGE3P-01.dgn	DRAWN - MNB	REVISED -
PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

U.S. ROUTE 30  
SUGGESTED TRAFFIC CONTROL PLANS (PRE-STAGE 3)

SCALE: 1"=50' SHEET NAME: MOT-3P-01 STA. 3696+00 TO STA. 3720+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	152
				CONTRACT NO. 60P95
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

FILE PATH = G:\CH2\0012\Road\Sheet\15160P95-SHT-MOT-STAGE3P-01.dgn

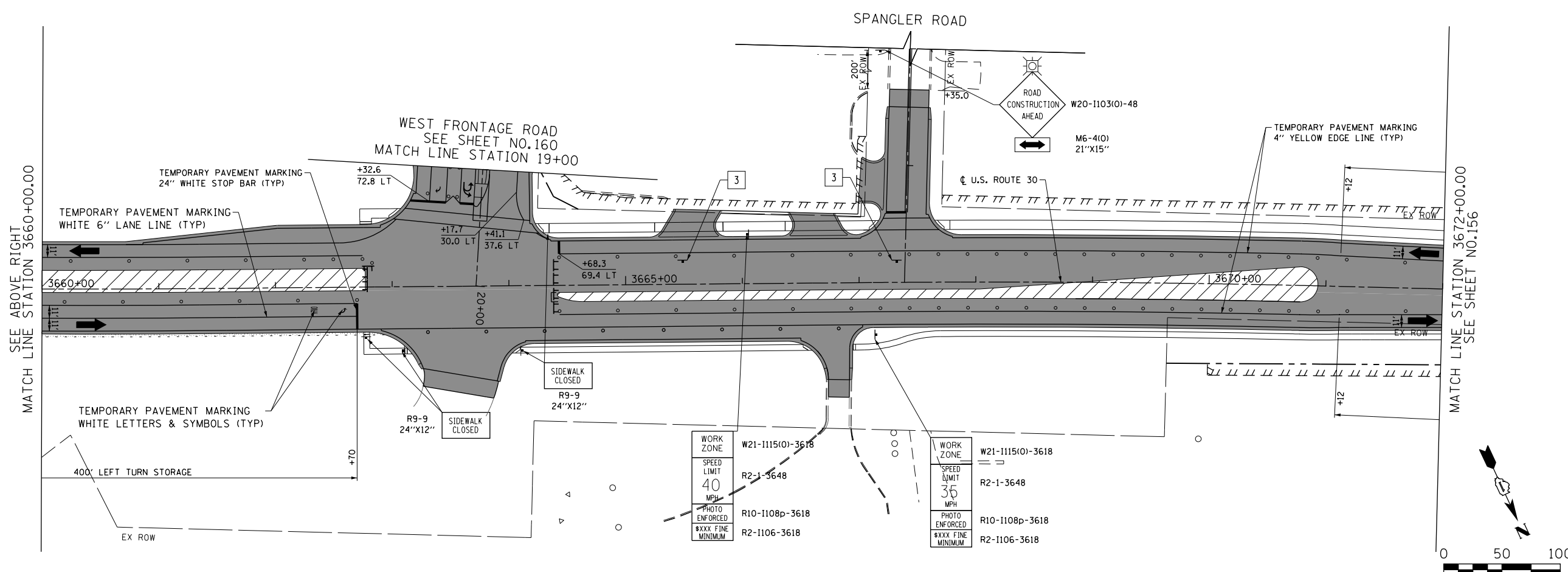
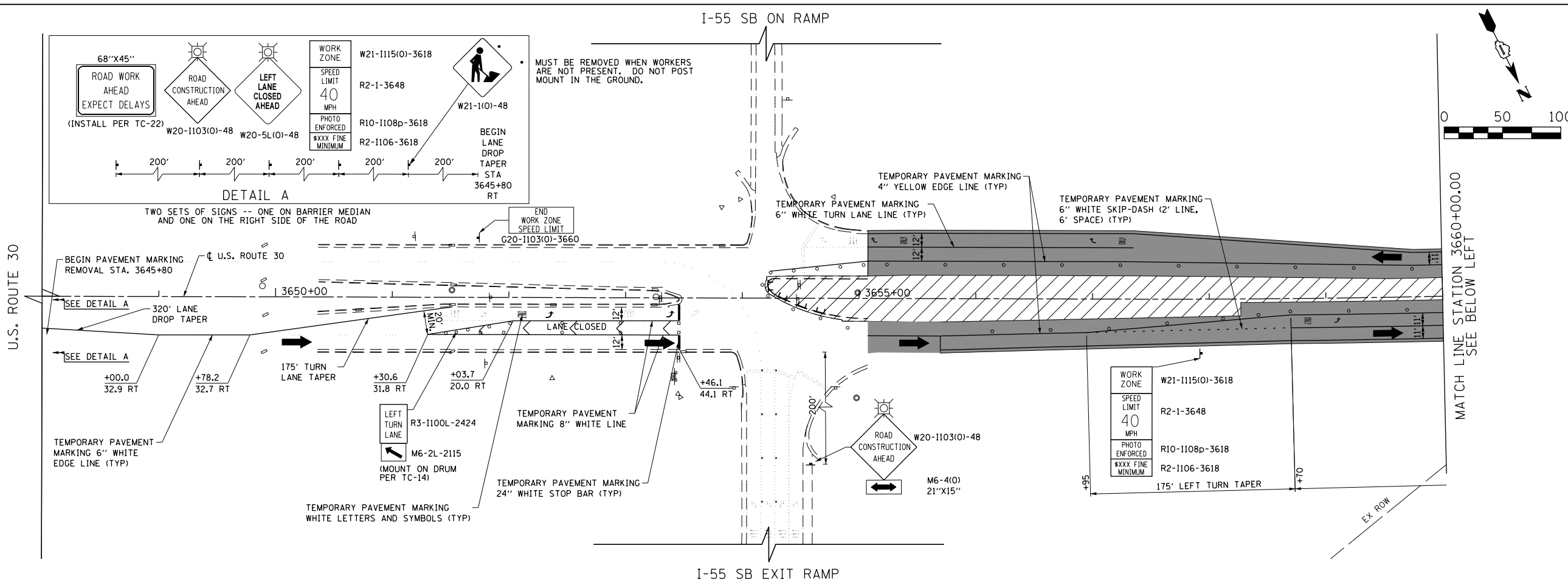




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PLOTTED	
GRADES CHECKED	
NOTES CHECKED	
STRUCTURE NOTATIONS CHECKED	
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PROFILE	
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GRADES CHECKED	
NOTES CHECKED	
STRUCTURE NOTATIONS CHECKED	
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GRADES CHECKED	
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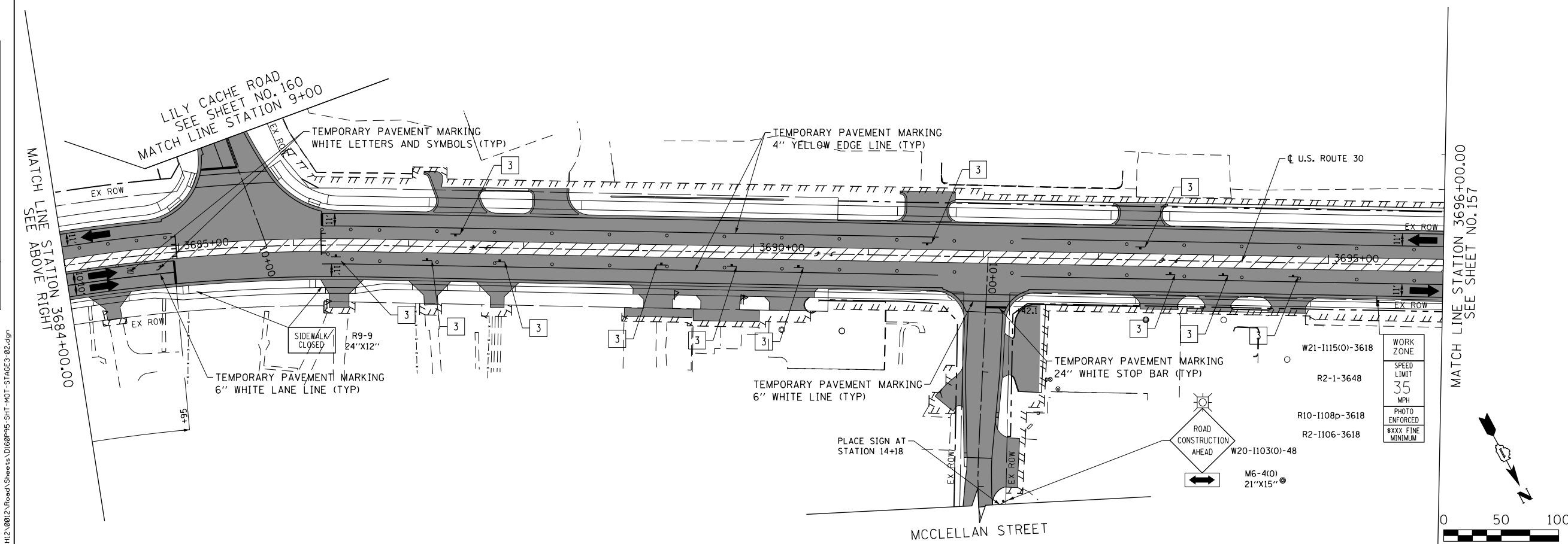
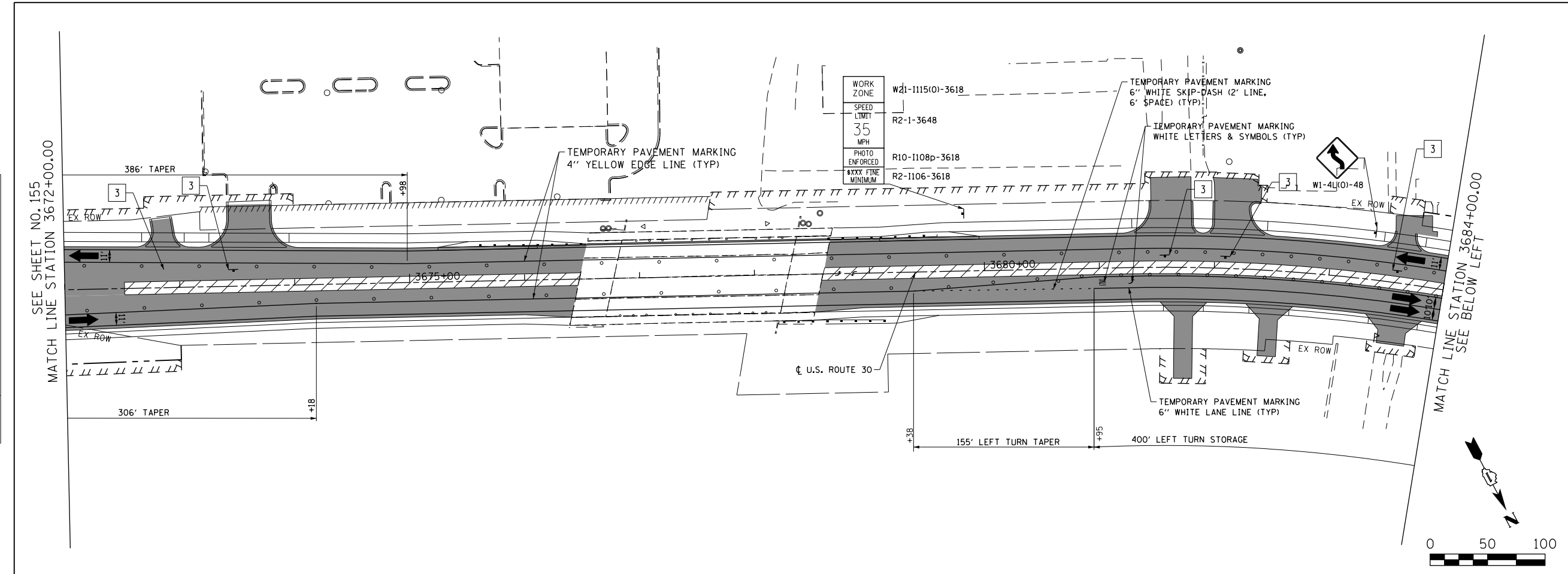
**LEGEND**

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID

**DRIVEWAY ENTRANCE**  
1 30" X 30" SEE TC-26  
2 30" X 30" SEE TC-26  
3 R3-2L 24" X 24"  
4 R3-2R 24" X 24"  
5 STOP R1-1  
6 ONE WAY R6-1R 36" X 12"  
7 ONE WAY R6-1L 36" X 12"

PLAN	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	BY	
	DATE	

PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	BY	
	DATE	



**LEGEND**

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID

1	DRIVEWAY ENTRANCE	30" X 30" SEE TC-26
2	DRIVEWAY ENTRANCE	30" X 30" SEE TC-26
3		R3-2L 24" X 24"
4		R3-2R 24" X 24"
5	STOP	R1-1
6	ONE WAY	R6-1R 36" X 12"
7	ONE WAY	R6-1L 36" X 12"



USER NAME = bshoeffler	DESIGNED - MNB	REVISED -
FILE = D:\60P95-SHT-MOT-STAGE3-02.dgn	DRAWN - MNB	REVISED -
PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**U.S. ROUTE 30  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 3)**

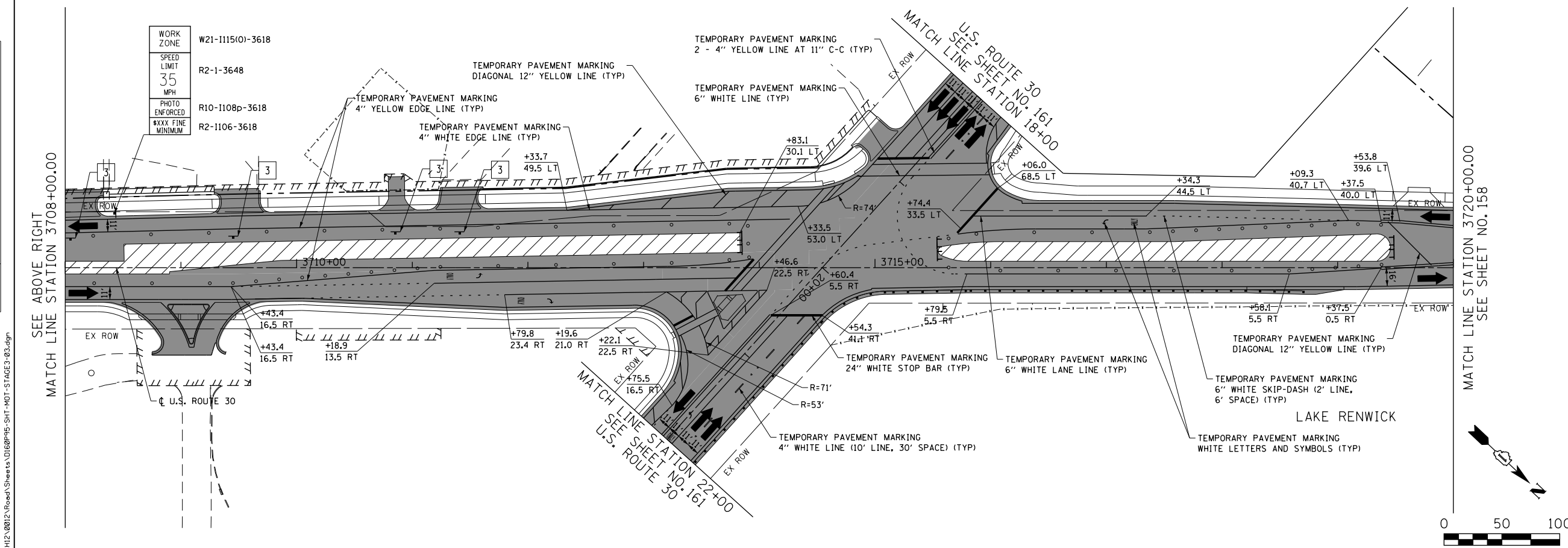
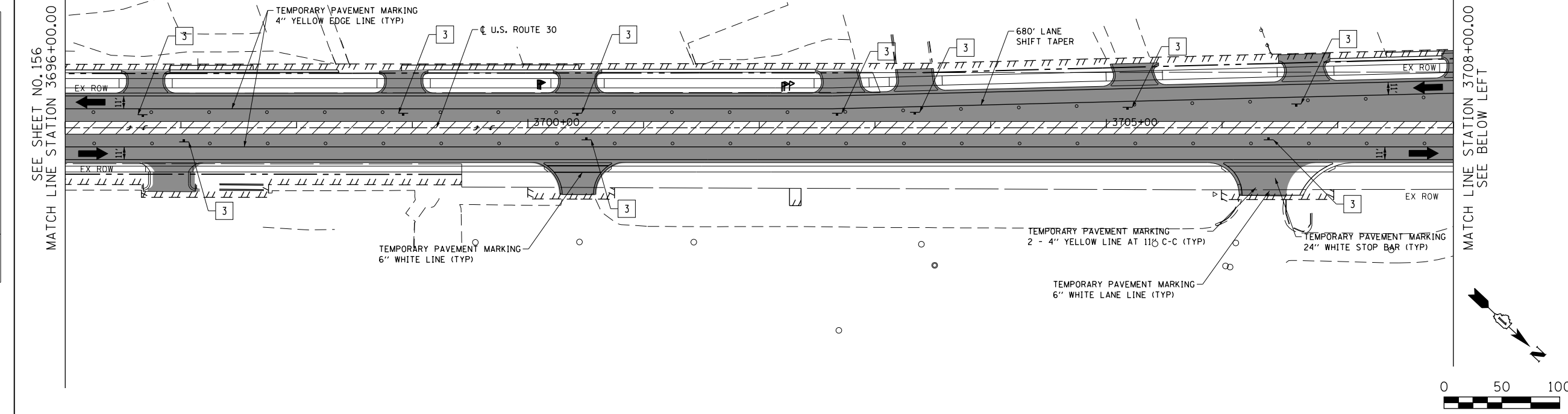
SCALE: 1"=50' SHEET NAME: MOT-3-02 STA. 3672+00 TO STA. 3696+00

F.A.P. RTE. 575	SECTION 14W - R	COUNTY WILL	TOTAL SHEETS 681	SHEET NO. 156
CONTRACT NO. 60P95				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				



PLAN	SURVEYED	DATE
	PLOTTED	
	NOTED	
	BY	
	NO.	
	FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	
	NOTED	
	BY	
	NO.	
	FILE NAME	



**LEGEND**

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID

1	DRIVEWAY ENTRANCE	30" X 30" SEE TC-26
2	DRIVEWAY ENTRANCE	30" X 30" SEE TC-26
3		R3-2L 24" X 24"
4		R3-2R 24" X 24"
5	STOP	R1-1
6	ONE WAY	R6-1R 36" X 12"
7	ONE WAY	R6-1L 36" X 12"



USER NAME = bshoeffler	DESIGNED - MNB	REVISED -
FILE = D:\60P95-SHT-MOT-STAGE3-03.dgn	DRAWN - MNB	REVISED -
PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**U.S. ROUTE 30  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 3)**

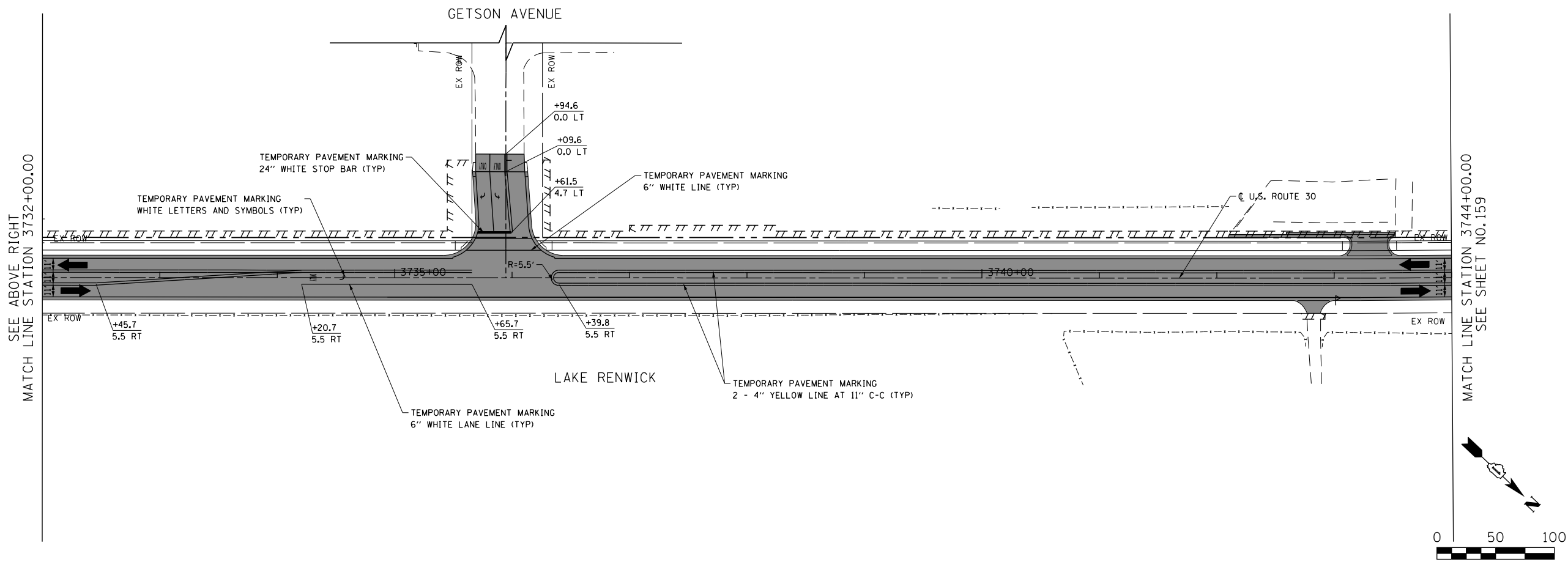
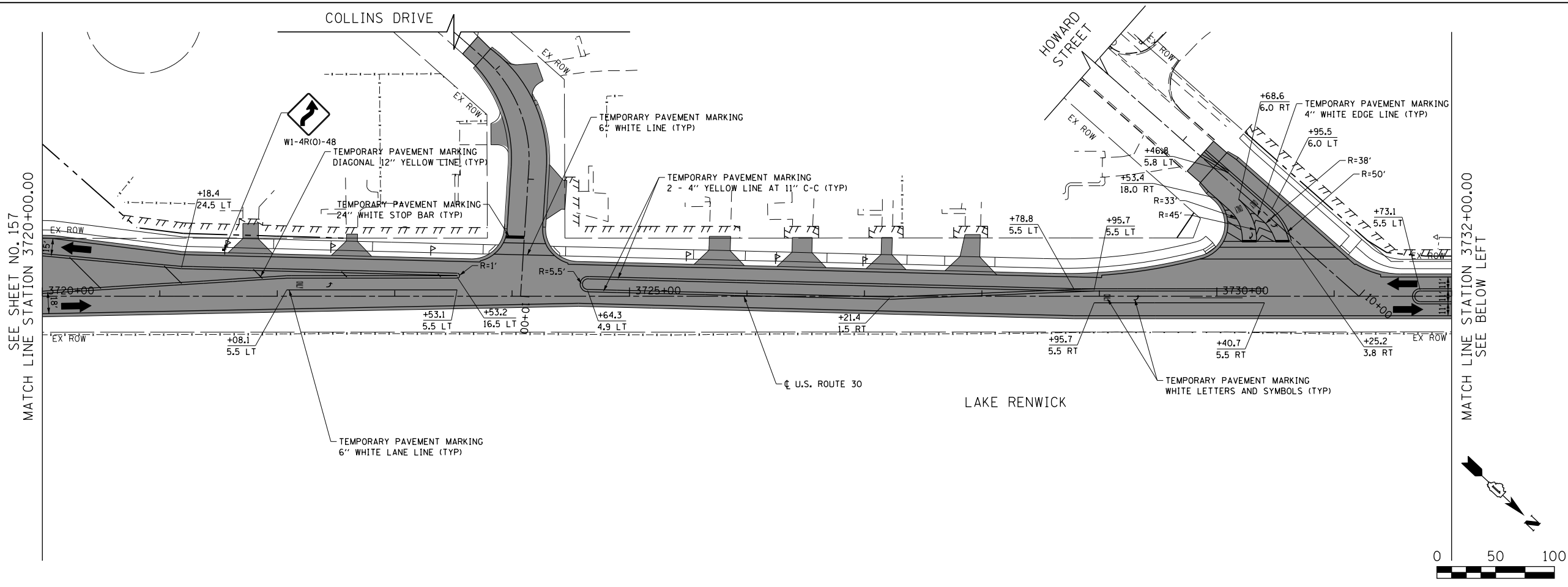
SCALE: 1"=50' SHEET NAME: MOT-3-03 STA. 3696+00 TO STA. 3720+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	157
				CONTRACT NO. 60P95
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	BY	
	DATE	

PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	BY	
	DATE	

FILE PATH = G:\CH2\0012\Road\Sheet\160P95-SHT-MOT-STAGE3-04.dgn



**LEGEND**

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID

1	DRIVEWAY ENTRANCE	30" X 30" SEE TC-26
2	DRIVEWAY ENTRANCE	30" X 30" SEE TC-26
3		R3-2L 24" X 24"
4		R3-2R 24" X 24"
5	STOP	R1-1
6	ONE WAY	R6-1R 36" X 12"
7	ONE WAY	R6-1L 36" X 12"



USER NAME = bshoeffliger	DESIGNED - MNB	REVISED -
FILE = D160P95-SHT-MOT-STAGE3-04.dgn	DRAWN - MNB	REVISED -
PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**U.S. ROUTE 30  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 3)**

SCALE: 1"=50' SHEET NAME: MOT-3-04 STA. 3720+00 TO STA. 3744+00

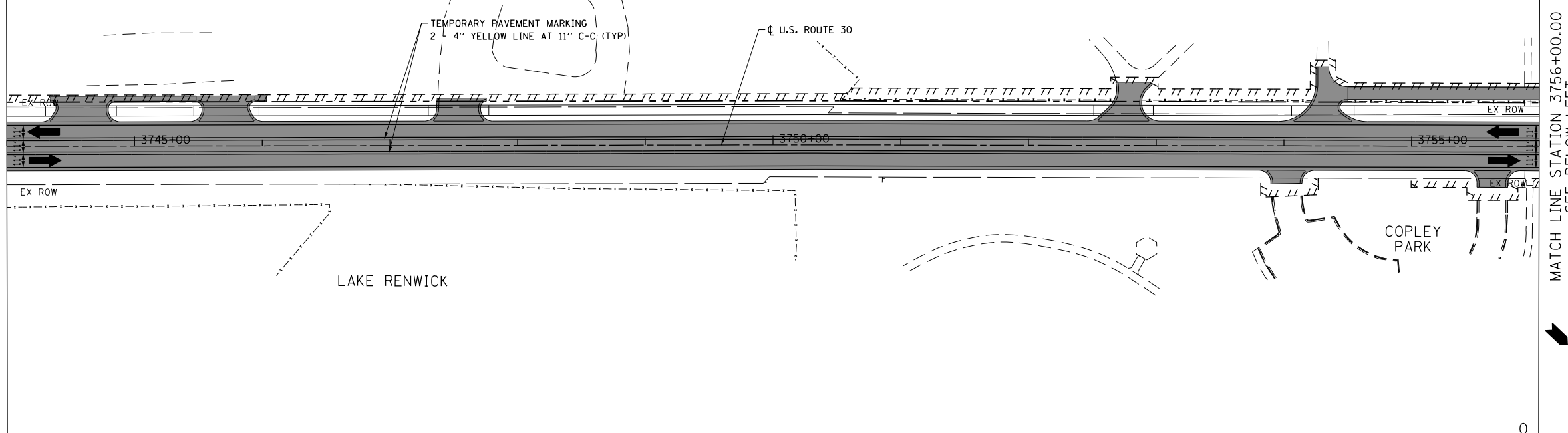
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575	14W - R	WILL	681	158
				CONTRACT NO. 60P95
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	BY	
	DATE	

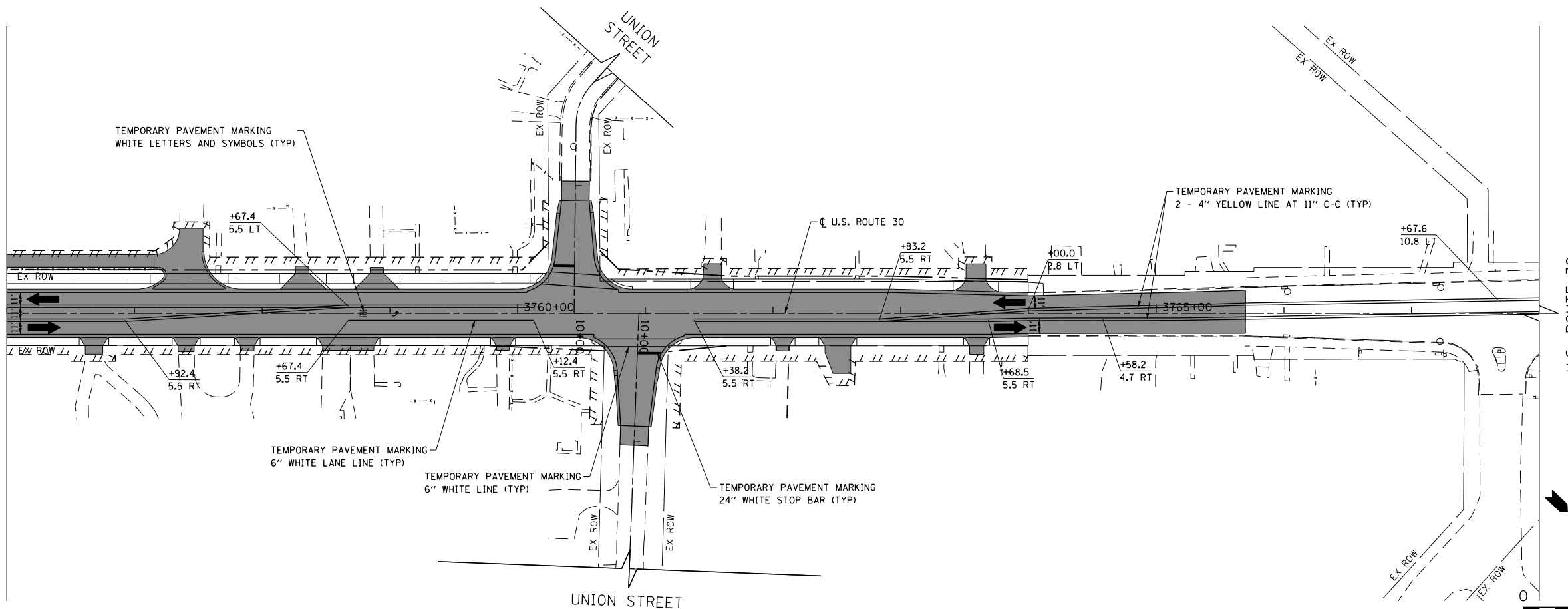
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	GRADES CHECKED	
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	NOTE BOOK NO.	
	BY	
	DATE	

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SEE SHEET NO. 158  
MATCH LINE STATION 3744+00.00

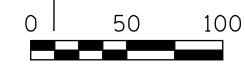


SEE ABOVE RIGHT  
MATCH LINE STATION 3756+00.00



LEGEND

- CONSTRUCTION WORK ZONE
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
- TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
- SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
- MONODIRECTIONAL FLASHING LIGHT
- DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
- DIRECTION OF TRAFFIC FLOW
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
- INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
- NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID
- DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
- R3-2L 24" X 24"
- R3-2R 24" X 24"
- R1-1
- R6-1R 36" X 12"
- R6-1L 36" X 12"



USER NAME = bshoeffler	DESIGNED - MNB	REVISED -
FILE = D160P95-SHT-MOT-STAGE3-05.dgn	DRAWN - MNB	REVISED -
PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

U.S. ROUTE 30  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 3)

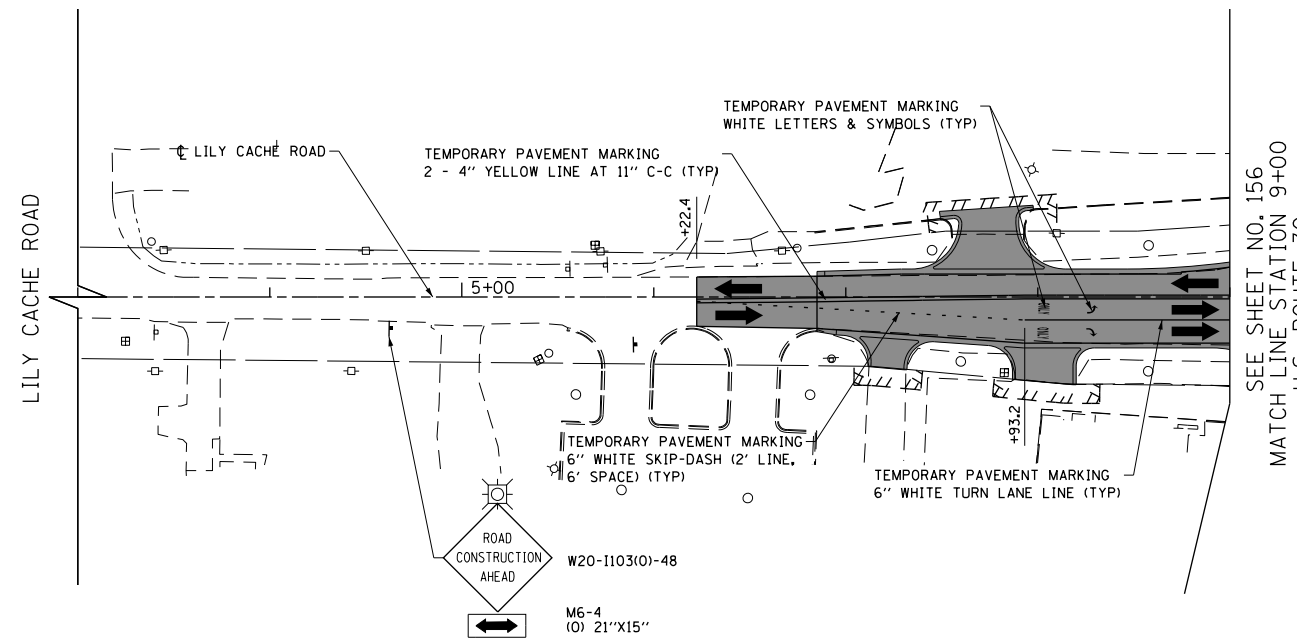
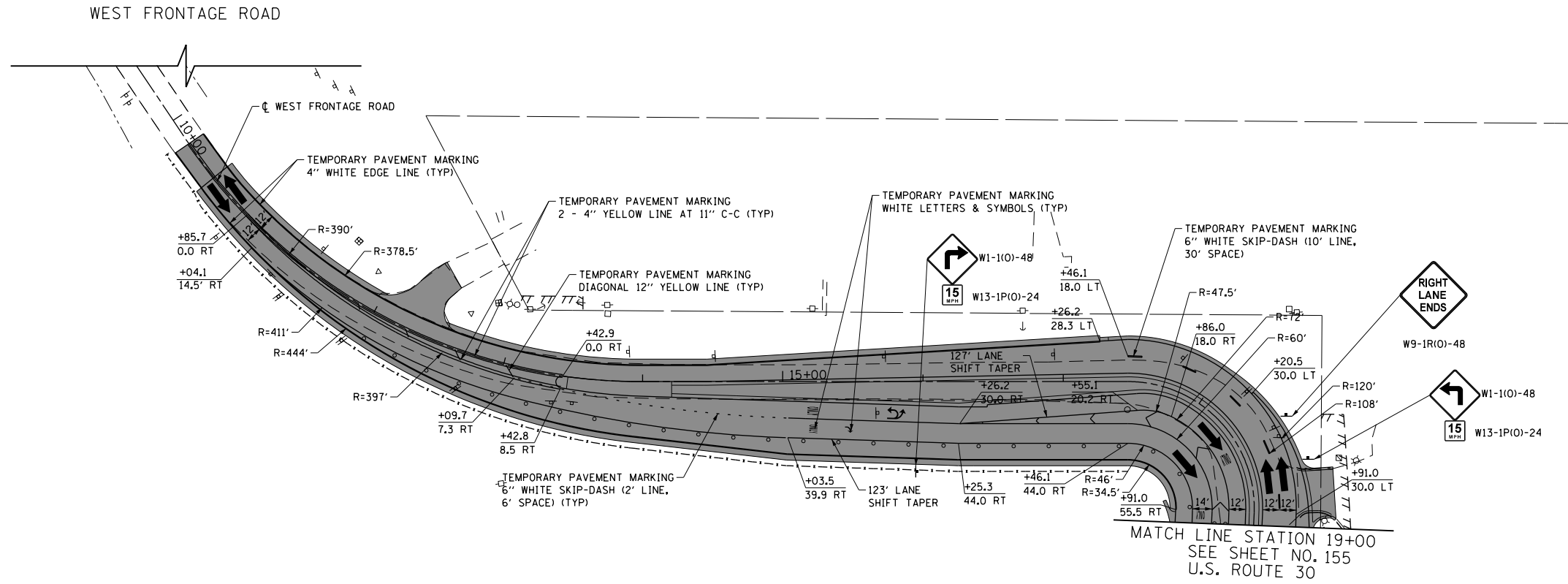
SCALE: 1"=50' SHEET NAME: MOT-3-05 STA. 3744+00 TO STA. 3763+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	159
				CONTRACT NO. 60P95
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	CADD FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	CADD FILE NAME	

FILE PATH = G:\CH2\0012\Road\Sheet\1\160P95-SHT-MOT-STAGE3-06.dgn



LEGEND	
	CONSTRUCTION WORK ZONE
	COMPLETED PAVEMENT
	TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
	TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
	TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
	SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
	MONODIRECTIONAL FLASHING LIGHT
	DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
	DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
	TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
	VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
	DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
	DIRECTION OF TRAFFIC FLOW
	DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
	INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
	NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID
	1 DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
	2 DRIVEWAY ENTRANCE 30" X 30" SEE TC-26
	3 R3-2L 24" X 24"
	4 R3-2R 24" X 24"
	5 STOP R1-1
	6 ONE WAY R6-1R 36" X 12"
	7 ONE WAY R6-1L 36" X 12"



USER NAME = bshoeffler	DESIGNED - MNB	REVISED -
FILE = D:\60P95-SHT-MOT-STAGE3-06.dgn	DRAWN - MNB	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

WEST FRONTAGE ROAD AND LILY CACHE ROAD  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 3)

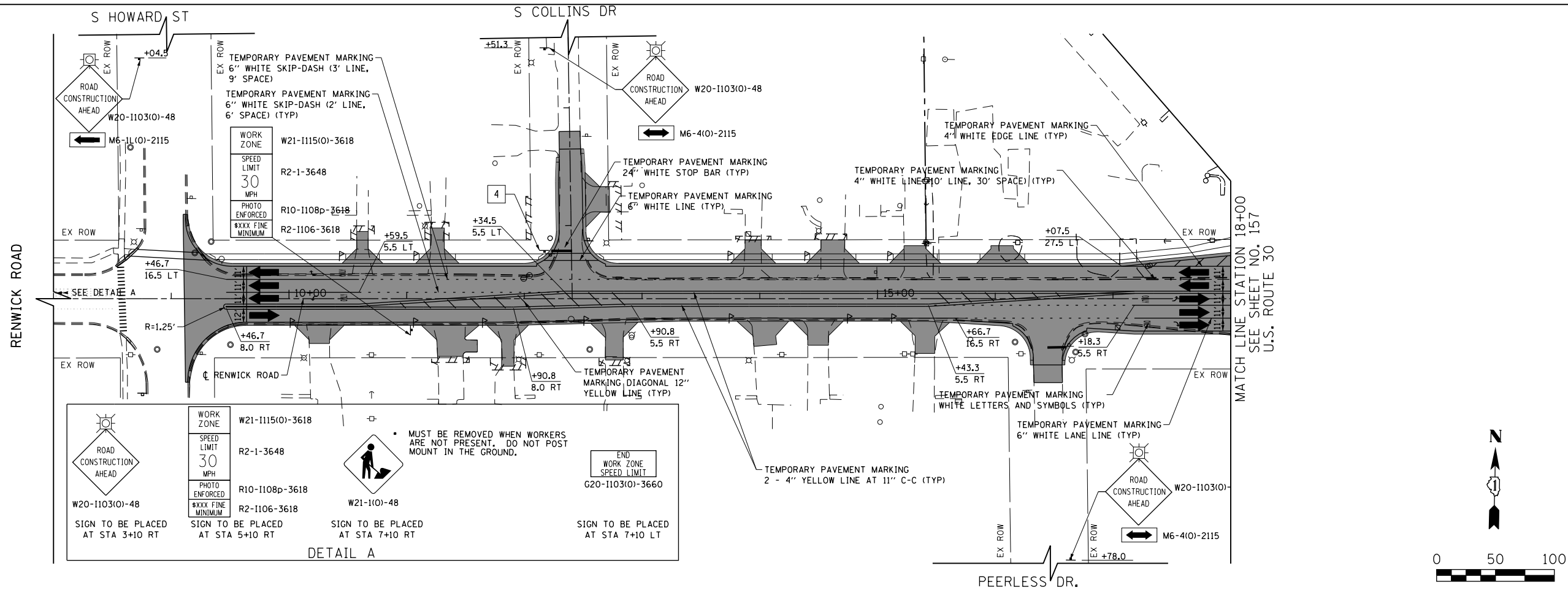
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	160
				CONTRACT NO. 60P95
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	BY	
	DATE	

PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	BY	
	DATE	

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**DETAIL A**

WORK ZONE W21-1115(O)-3618  
SPEED LIMIT 30 MPH  
PHOTO ENFORCED  
\$XXX FINE MINIMUM

R2-1-3648  
R10-1108p-3618  
R2-1106-3618

ROAD CONSTRUCTION AHEAD W20-1103(O)-48  
SIGN TO BE PLACED AT STA 3+10 RT

ROAD CONSTRUCTION AHEAD W20-1103(O)-48  
SIGN TO BE PLACED AT STA 5+10 RT

W21-11(O)-48  
SIGN TO BE PLACED AT STA 7+10 RT

MUST BE REMOVED WHEN WORKERS ARE NOT PRESENT. DO NOT POST MOUNT IN THE GROUND.

END WORK ZONE SPEED LIMIT G20-1103(O)-3660  
SIGN TO BE PLACED AT STA 7+10 LT

**DETAIL A**

END WORK ZONE SPEED LIMIT G20-1103(O)-3660  
SIGN TO BE PLACED AT STA 32+00 RT

W21-11(O)-48  
SIGN TO BE PLACED AT STA 32+00 LT

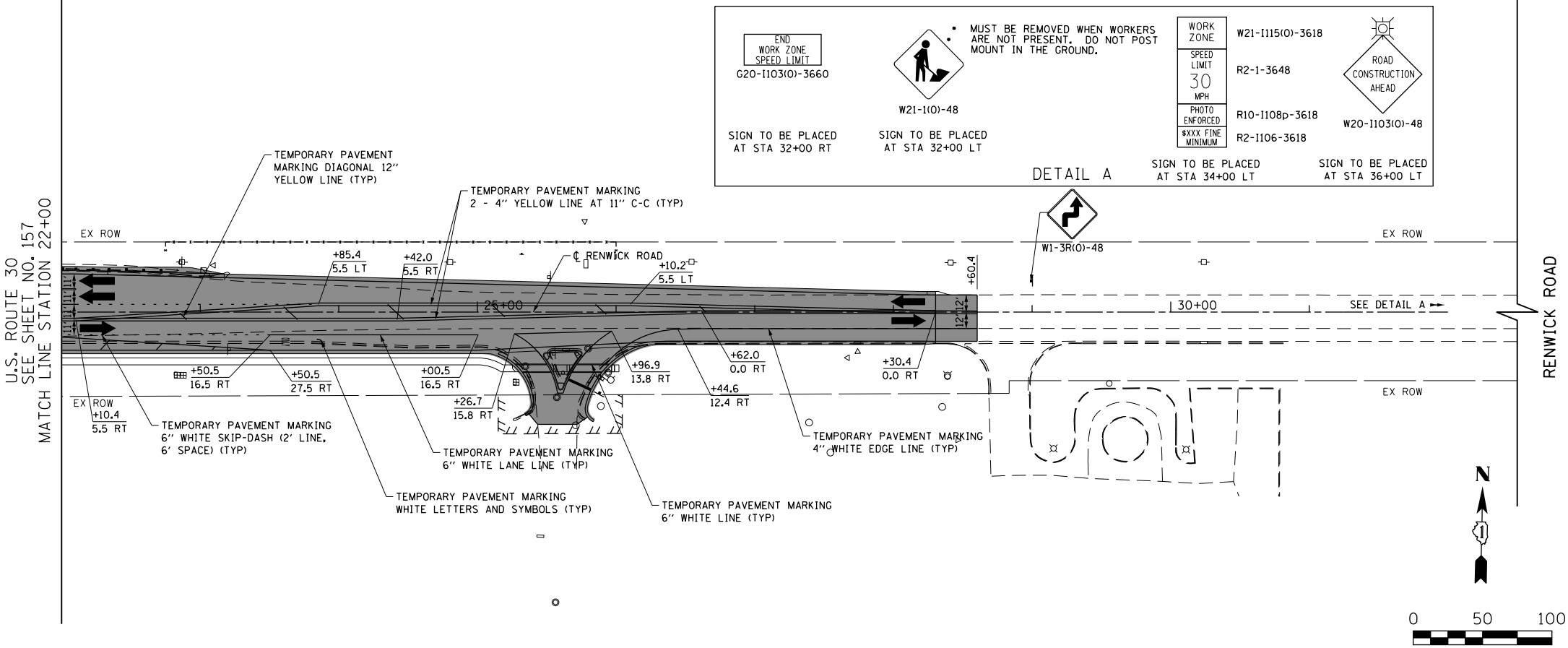
MUST BE REMOVED WHEN WORKERS ARE NOT PRESENT. DO NOT POST MOUNT IN THE GROUND.

WORK ZONE W21-1115(O)-3618  
SPEED LIMIT 30 MPH  
PHOTO ENFORCED  
\$XXX FINE MINIMUM

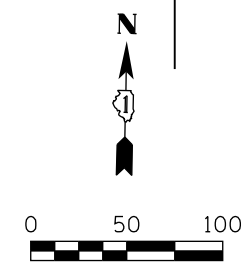
R2-1-3648  
R10-1108p-3618  
R2-1106-3618

ROAD CONSTRUCTION AHEAD W20-1103(O)-48  
SIGN TO BE PLACED AT STA 34+00 LT

ROAD CONSTRUCTION AHEAD W20-1103(O)-48  
SIGN TO BE PLACED AT STA 36+00 LT



- LEGEND**
- CONSTRUCTION WORK ZONE
  - COMPLETED PAVEMENT
  - TEMPORARY PAVEMENT TO BE CONSTRUCTED PRE-STAGE
  - TEMPORARY RESURFACE TO BE CONSTRUCTED PRE-STAGE
  - TEMPORARY PAVEMENT TO BE CONSTRUCTED (SEE PLANS FOR MORE DETAILS)
  - SIGN TO BE PLACED ON PERMANENT OR PORTABLE SUPPORT
  - MONODIRECTIONAL FLASHING LIGHT
  - DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
  - DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
  - TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
  - VERTICAL PANELS WITH STEADY BURN MONO-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
  - DOUBLE VERTICAL PANELS WITH STEADY BURN BI-DIRECTIONAL LIGHT @ 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS
  - DIRECTION OF TRAFFIC FLOW
  - DRAINAGE STRUCTURES TO BE RECONSTRUCTED (SPECIAL)
  - INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID
  - NEW MANHOLE, TYPE A, 4'-DIAMETER, WITH TYPE 1 FRAME, OPEN LID



USER NAME = bshae1iger	DESIGNED - MNB	REVISED -
FILE = D:\60P95-SHT-MOT-STAGE3-07.dgn	DRAWN - MNB	REVISED -
PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**RENWICK ROAD  
SUGGESTED TRAFFIC CONTROL PLANS (STAGE 3)**

SCALE: 1"=50' SHEET NAME: MOT-3-07 STA. 8+00 TO STA. 32+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	161
				CONTRACT NO. 60P95

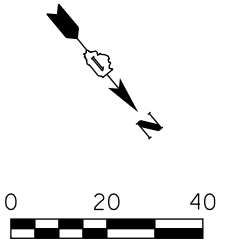
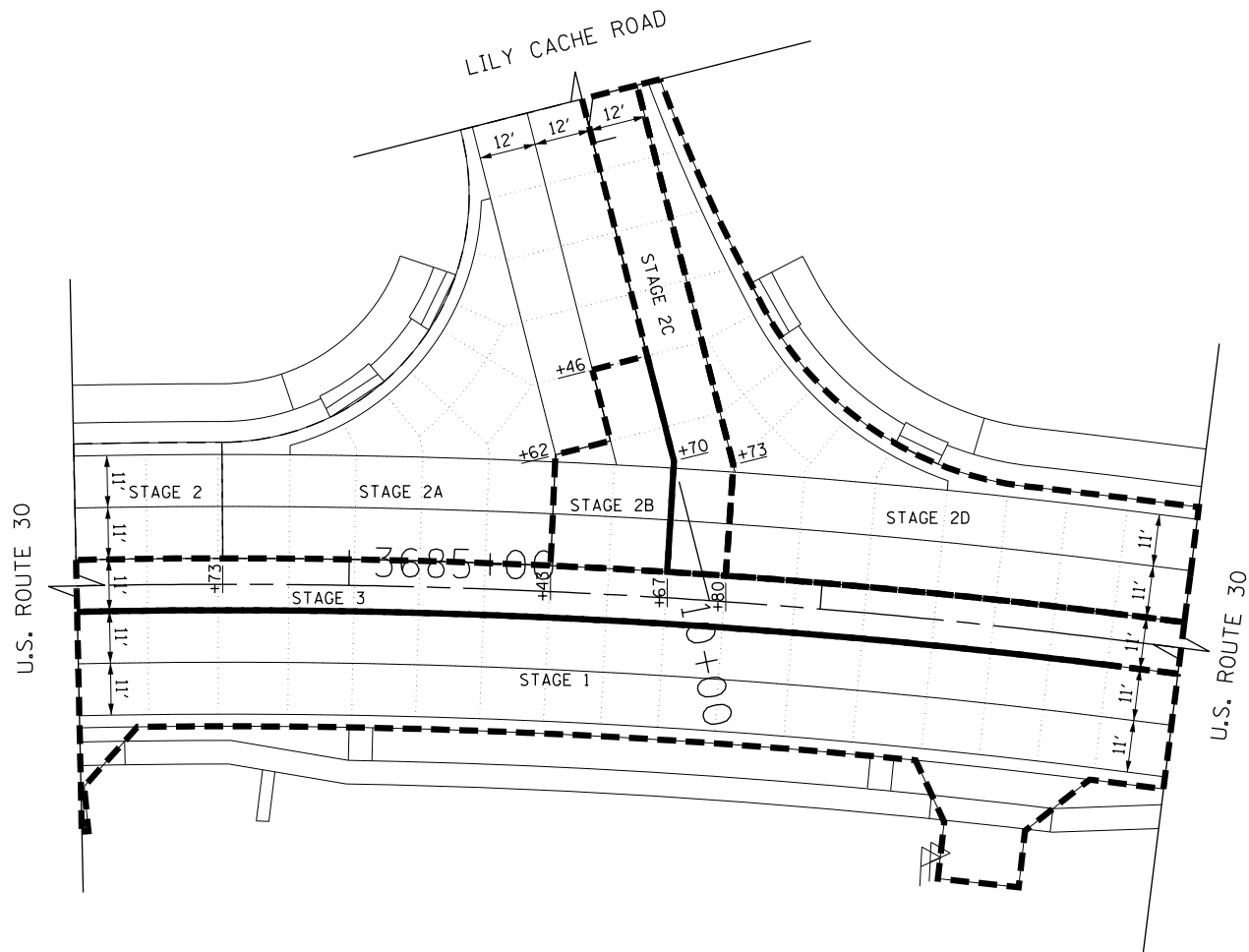
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT



PLAN	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	GRADES CHECKED		
	STRUCTURE NOTATIONS CHECKED		
	CADD FILE NAME		

PROFILE	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	GRADES CHECKED		
	STRUCTURE NOTATIONS CHECKED		

FILE PATH = G:\CH2\0012\Road\Sheet\160P95-SHT-15B-02.dgn



———— LANE LINES  
 - - - - - PAVEMENT CONSTRUCTION STAGE BOUNDARIES



USER NAME = bshoeffliger	DESIGNED -	REVISED -
FILE = D:\60P95-SHT-15B-02.dgn	DRAWN -	REVISED -
PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE = 08/18/2014	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**LILY CACHE ROAD  
 INTERSECTION STAGING BOUNDARIES**

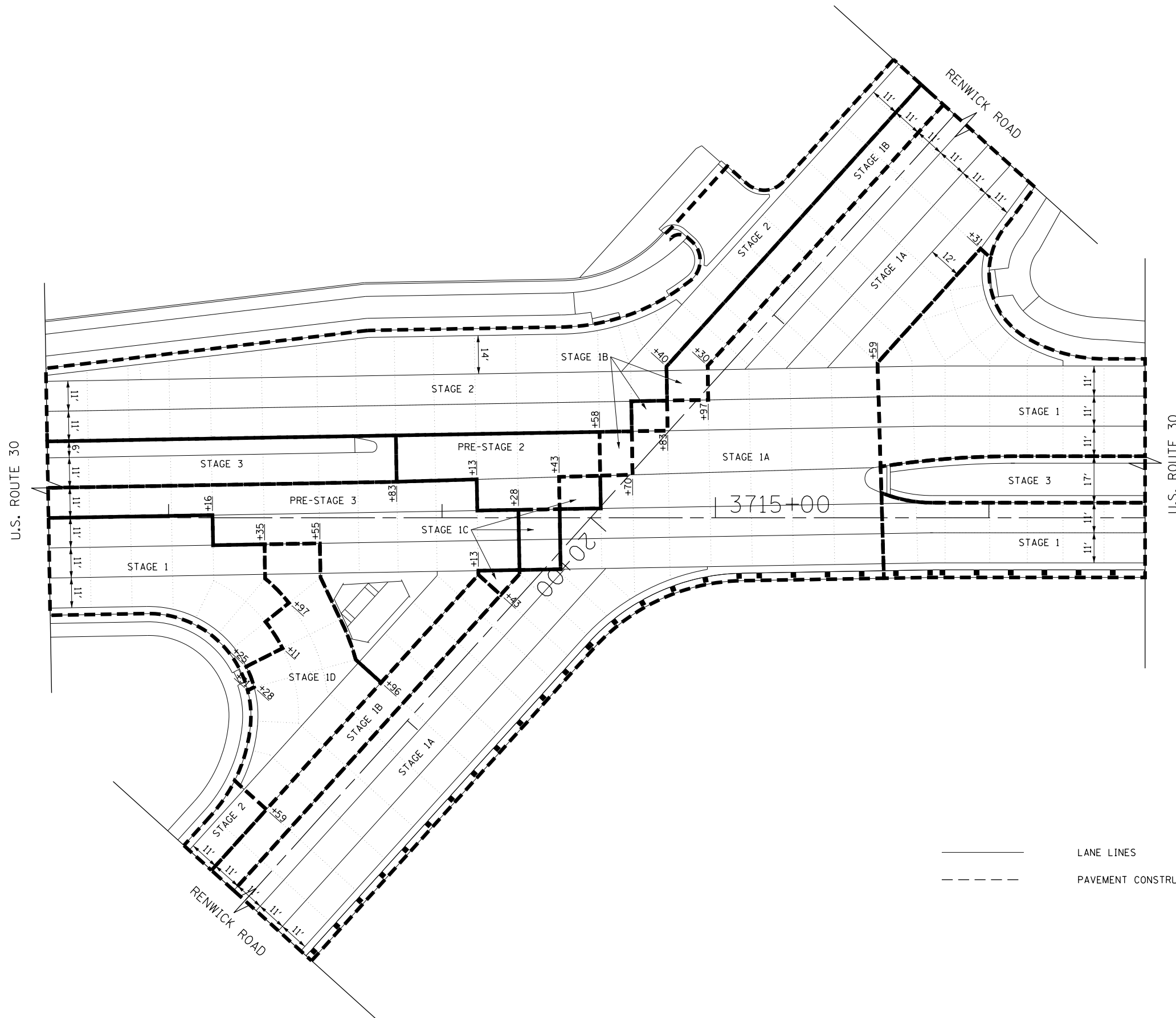
SCALE: 1"=20'    SHEET NAME: ISB-02    STA. 8+00 TO STA. 32+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	163
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60P95	

PLAN	SURVEYED	BY	DATE
NO.	PLOTTED		
	GRADES CHECKED		
	STRUCTURE NOTATIONS CHECKED		
	BY		
	DATE		

PROFILE	SURVEYED	BY	DATE
NO.	PLOTTED		
	GRADES CHECKED		
	STRUCTURE NOTATIONS CHECKED		
	BY		
	DATE		

FILE PATH = G:\CH2\0012\Road\Sheet\0160P95-SHT-15B-03.dgn



———— LANE LINES  
 - - - - - PAVEMENT CONSTRUCTION STAGE BOUNDARIES



USER NAME = bshoeffliger	DESIGNED -	REVISED -
FILE = D160P95-SHT-15B-03.dgn	DRAWN -	REVISED -
PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE = 08/18/2014	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**RENWICK ROAD  
 INTERSECTION STAGING BOUNDARIES**

SCALE: 1"=20' SHEET NAME: ISB-03 STA. TO STA.

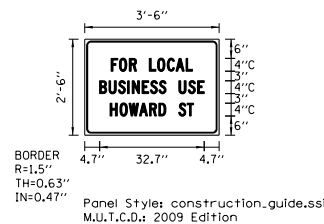
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	164
CONTRACT NO. 60P95			FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT	



PLAN	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	GRADES CHECKED	
	STRUCTURE NOTATION OK'D	
	CADD FILE NAME	

**SIGN DETAIL**

1:50



Panel Style: construction\_guide.ssi  
M.U.T.C.D.: 2009 Edition

Panel Style: construction\_guide.ssi

Dimensions are in inches.tenths

Letter locations are panel edge to lower left corner

SIGN NUMBER	TEMP INFO SIGN 1
WIDTH x HGHT.	3'-6" x 2'-6"
BORDER WIDTH	0.63"
CORNER RADIUS	1.5"
MOUNTING	Ground
BACKGROUND	TYPE: Reflective
	COLOR: Orange
LEGEND/BORDER	TYPE: Reflective
	COLOR: Black

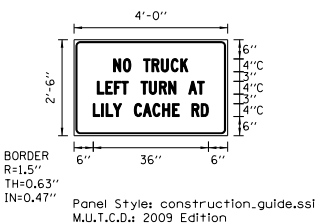
SYMBOL	ROT	X	Y	WID	HT

LETTER POSITIONS (X)												LENGTH	SERIESSIZE	
F	O	R		L	O	C	A	L					C	2000
8.3	10.8	14	16.2	20.2	22.7	25.8	28.5	31.7					25.4	4
B	U	S	I	N	E	S	S		U	S	E		C	2000
4.7	7.6	10.5	13.4	14.9	18	20.5	23.2	25.5	29.5	32.4	35.3		32.7	4
H	O	W	A	R	D		S	T					C	2000
8	11	13.9	17.1	20.3	23.2	25.4	29.4	32					26.1	4

PROFILE	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	GRADES CHECKED	
	STRUCTURE NOTATION OK'D	
	CADD FILE NAME	

**SIGN DETAIL**

1:50



Panel Style: construction\_guide.ssi  
M.U.T.C.D.: 2009 Edition

Panel Style: construction\_guide.ssi

Dimensions are in inches.tenths

Letter locations are panel edge to lower left corner

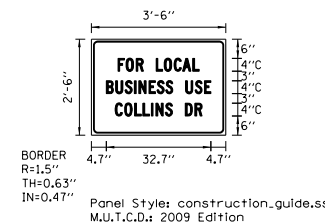
SIGN NUMBER	TEMP INFO SIGN 3
WIDTH x HGHT.	4'-0" x 2'-6"
BORDER WIDTH	0.63"
CORNER RADIUS	1.5"
MOUNTING	Ground
BACKGROUND	TYPE: Reflective
	COLOR: Orange
LEGEND/BORDER	TYPE: Reflective
	COLOR: Black

SYMBOL	ROT	X	Y	WID	HT

LETTER POSITIONS (X)												LENGTH	SERIESSIZE	
N	O		T	R	U	C	K						C	2000
12.4	15.4	17.8	21.8	24.4	27.3	30.3	33.4						23.3	4
L	E	F	T		T	U	R	N		A	T		C	2000
7.3	9.9	12.6	14.9	17	21	23.6	26.7	29.6	31.8	35.8	38.6		33.4	4
L	I	L	Y		C	A	C	H	E		R	D	C	2000
6	8.6	10	12.3	14.9	18.9	21.6	24.6	27.7	30.8	32.8	36.8	39.8	36	4

**SIGN DETAIL**

1:50



Panel Style: construction\_guide.ssi  
M.U.T.C.D.: 2009 Edition

Panel Style: construction\_guide.ssi

Dimensions are in inches.tenths

Letter locations are panel edge to lower left corner

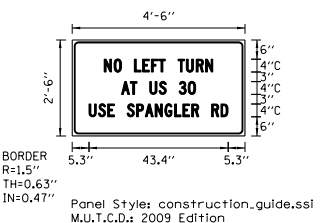
SIGN NUMBER	TEMP INFO SIGN 2
WIDTH x HGHT.	3'-6" x 2'-6"
BORDER WIDTH	0.63"
CORNER RADIUS	1.5"
MOUNTING	Ground
BACKGROUND	TYPE: Reflective
	COLOR: Orange
LEGEND/BORDER	TYPE: Reflective
	COLOR: Black

SYMBOL	ROT	X	Y	WID	HT

LETTER POSITIONS (X)												LENGTH	SERIESSIZE	
F	O	R		L	O	C	A	L					C	2000
8.3	10.8	14	16.2	20.2	22.7	25.8	28.5	31.7					25.4	4
B	U	S	I	N	E	S	S		U	S	E		C	2000
4.7	7.6	10.5	13.4	14.9	18	20.5	23.2	25.5	29.5	32.4	35.3		32.7	4
C	O	L	L	I	N	S		D	R				C	2000
7.4	10.4	13.5	16.1	18.7	20.2	23.1	25.3	29.3	32.4				27.2	4

**SIGN DETAIL**

1:50



Panel Style: construction\_guide.ssi  
M.U.T.C.D.: 2009 Edition

Panel Style: construction\_guide.ssi

Dimensions are in inches.tenths

Letter locations are panel edge to lower left corner

SIGN NUMBER	TEMP INFO SIGN 4
WIDTH x HGHT.	4'-6" x 2'-6"
BORDER WIDTH	0.63"
CORNER RADIUS	1.5"
MOUNTING	Ground
BACKGROUND	TYPE: Reflective
	COLOR: Orange
LEGEND/BORDER	TYPE: Reflective
	COLOR: Black

SYMBOL	ROT	X	Y	WID	HT

LETTER POSITIONS (X)												LENGTH	SERIESSIZE			
N	O		L	E	F	T		T	U	R	N		C	2000		
10	13.1	15.4	19.4	22	24.8	27	29.1	33.1	35.7	38.8	41.7		33.9	4		
A	T		U	S		3	0						C	2000		
15.4	18.2	20.2	24.2	27.2	29.4	33.4	36.2						23.2	4		
U	S	E		S	P	A	N	G	L	E	R		R	D	C	2000
5.3	8.2	11.1	13.2	17.2	20.1	22.8	25.9	29	32	34.6	37.3	39.6	43.6	46.5	43.4	4

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USER NAME = bshoeffliger	DESIGNED -	REVISED -
FILE = D:\60P95-SHT-MOT-TIS-01.dgn	DRAWN -	REVISED -
PLOT SCALE = 5.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>U.S. ROUTE 30 TEMPORARY INFORMATION SIGNING</b>				F.A.P. RTE. 575	SECTION 14W - R	COUNTY WILL	TOTAL SHEETS 681	SHEET NO. 165	
SCALE: NTS	SHEET NAME: MOT-TIS-01	STA. N/A	TO STA. N/A	CONTRACT NO. 60P95				FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT	

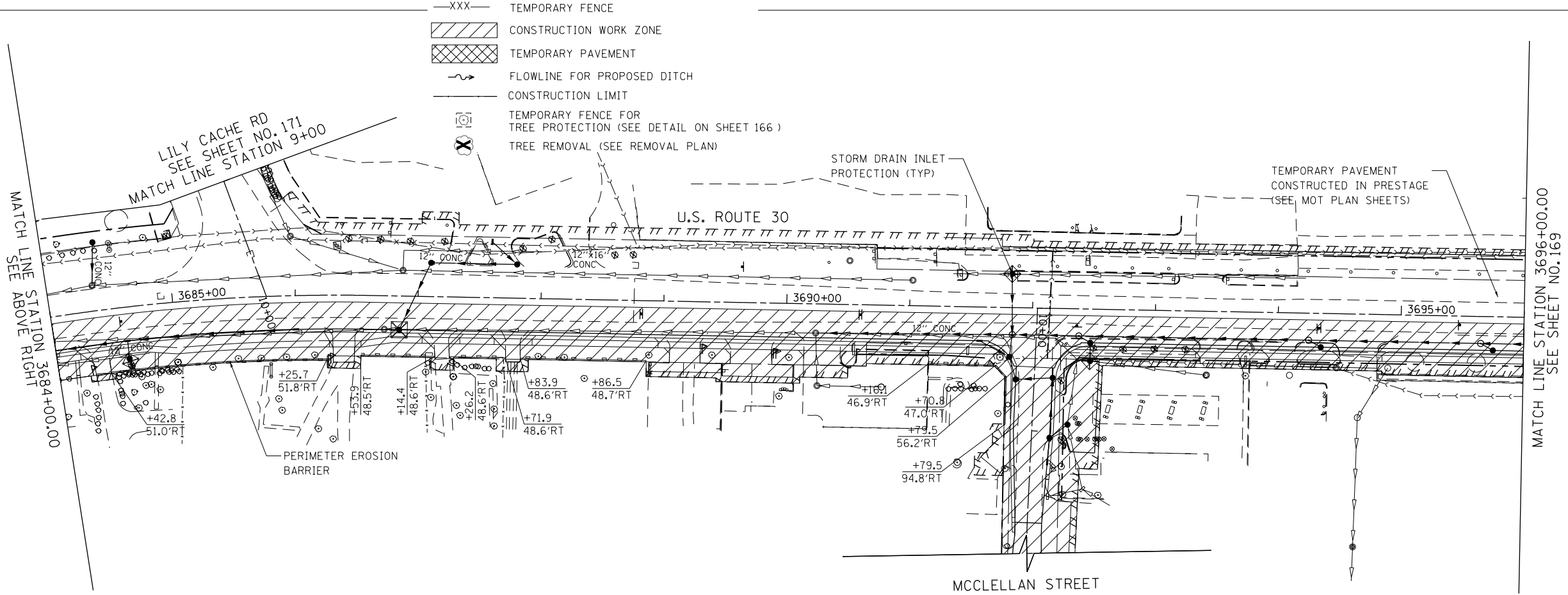
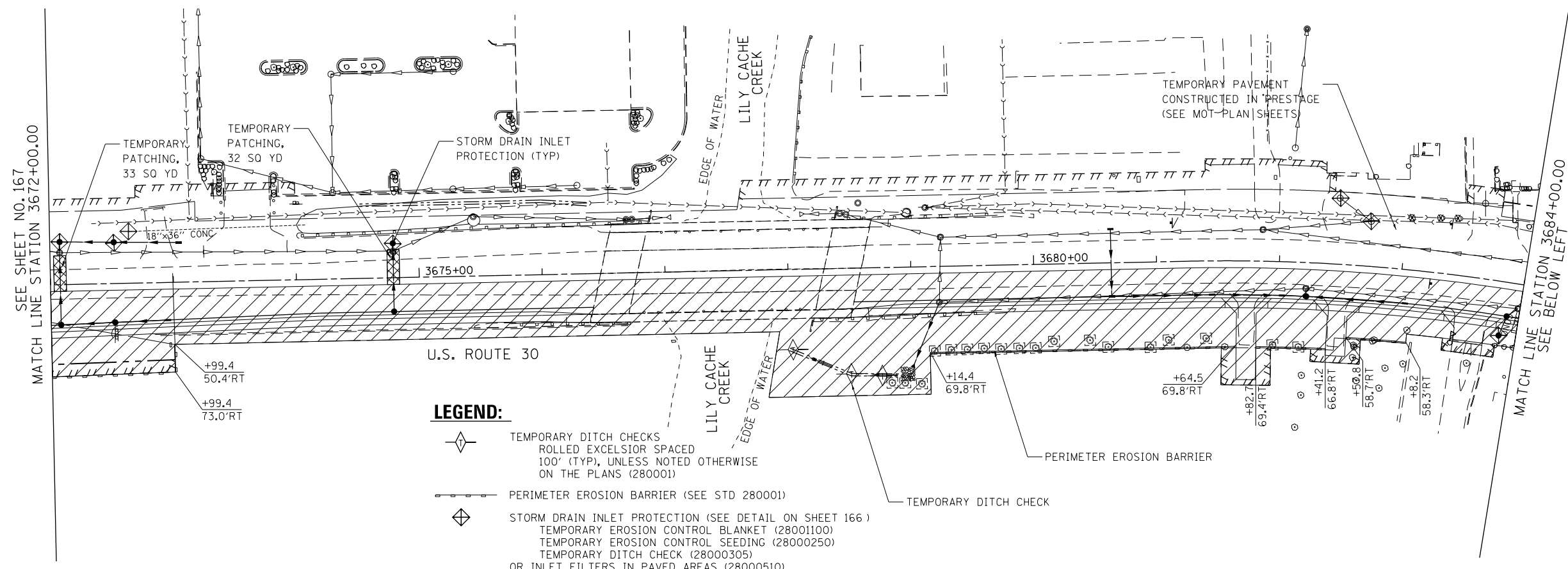




PLAN	SURVEYED	DATE
	ALIGNED	
	NOTED	
	RT. OF WAY CHECKED	
	NO. _____	
	ADD FILE NAME	

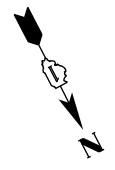
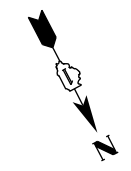
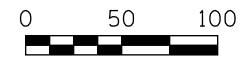
PROFILE	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	B.M. NOTED	
	STRUCTURE NOTATIONS CHKD	
	NO. _____	

PLOT DATE = 10/23/2014  
 FILE NAME = G:\CH12\0012\Road\Shreve\EROSION CONTROL - MILLENNIA\0160P95-SHT-EROSI-3.dgn  
 USER = 10600000 / in.  
 USER NAME = 03/09/13/2014



**LEGEND:**

- TEMPORARY DITCH CHECKS  
ROLLED EXCELSIOR SPACED  
100' (TYP), UNLESS NOTED OTHERWISE  
ON THE PLANS (280001)
- PERIMETER EROSION BARRIER (SEE STD 280001)
- STORM DRAIN INLET PROTECTION (SEE DETAIL ON SHEET 166)  
TEMPORARY EROSION CONTROL BLANKET (28001100)  
TEMPORARY EROSION CONTROL SEEDING (28000250)  
TEMPORARY DITCH CHECK (28000305)  
OR INLET FILTERS IN PAVED AREAS (28000510)
- TEMPORARY FENCE
- CONSTRUCTION WORK ZONE
- TEMPORARY PAVEMENT
- FLOWLINE FOR PROPOSED DITCH
- CONSTRUCTION LIMIT
- TEMPORARY FENCE FOR  
TREE PROTECTION (SEE DETAIL ON SHEET 166)
- TREE REMOVAL (SEE REMOVAL PLAN)



**MILLENNIA PROFESSIONAL SERVICES**  
 2600 Warrenville Rd, Ste 203, Downers Grove, IL 60515  
 630.705.0110 voice, 630.839.2566 fax  
 www.mps-il.com

DESIGNED - TVN	REVISED -
DRAWN - JP	REVISED -
CHECKED - EG	REVISED -
DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**US ROUTE 30  
 STAGE 1 EROSION & SEDIMENT CONTROL PLAN**

SCALE: 1"=50' SHEET NAME: EC1-3 STA. 3672+00.00 TO STA. 3696+00.00

F.A.P. RTE. 575	SECTION 14W - R	COUNTY WILL	TOTAL SHEETS 681	SHEET NO. 168
CONTRACT NO. 60P95				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

EC1-3

PLAN	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NO. OF	
	FILE	
	NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NO. OF	
	FILE	
	NAME	

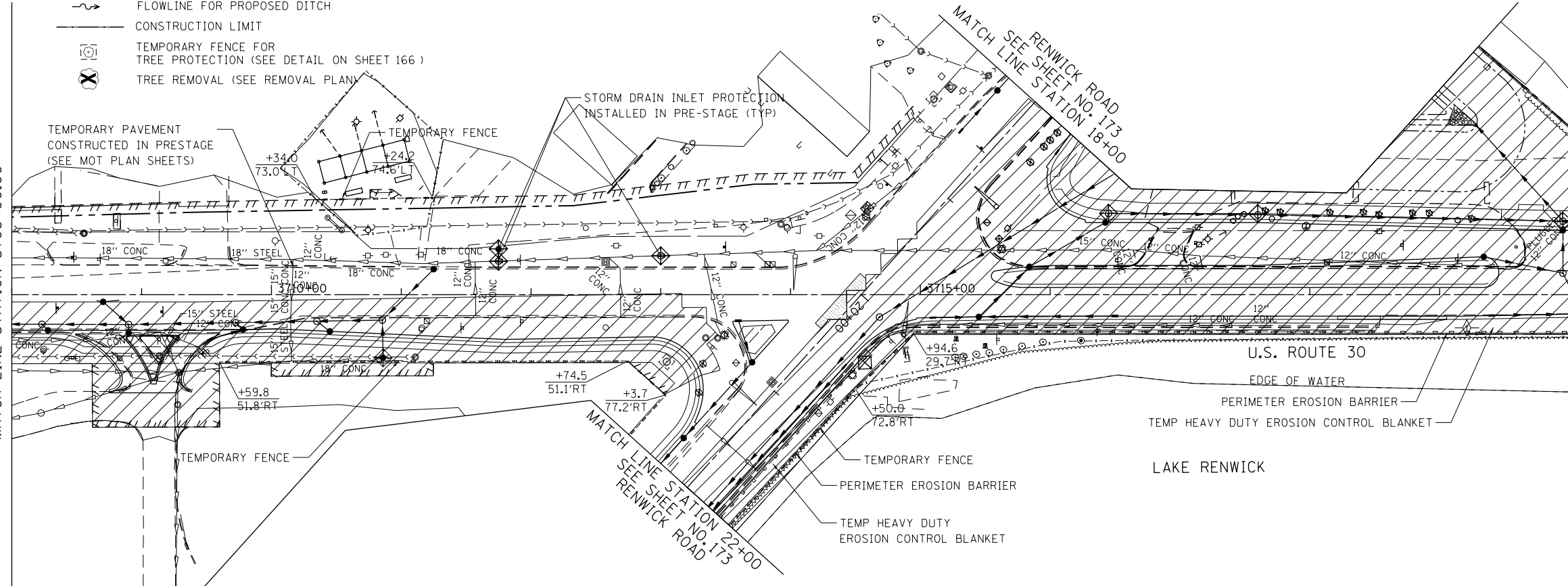
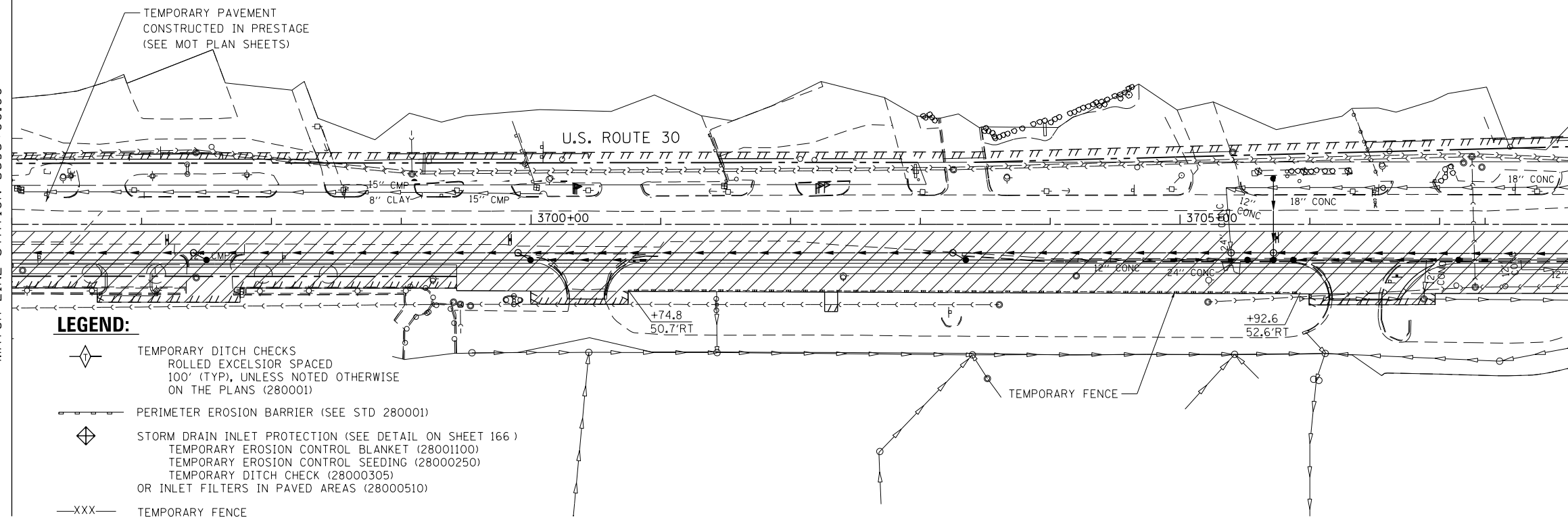
10/23/2014  
 G:\CH12\0012\Road\Shive\EROSION CONTROL - MILLENNIA\DIGP95-SHT-EROSI-4.dgn  
 180.0000 1/ in.  
 User: mps

SEE SHEET NO. 168  
 MATCH LINE STATION 3696+00.00

SEE ABOVE RIGHT  
 MATCH LINE STATION 3708+00.00

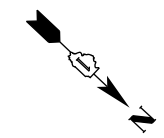
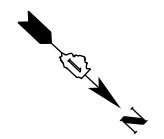
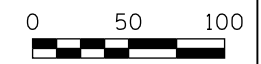
**LEGEND:**

- TEMPORARY DITCH CHECKS  
ROLLED EXCELSIOR SPACED  
100' (TYP), UNLESS NOTED OTHERWISE  
ON THE PLANS (280001)
- PERIMETER EROSION BARRIER (SEE STD 280001)
- STORM DRAIN INLET PROTECTION (SEE DETAIL ON SHEET 166 )  
TEMPORARY EROSION CONTROL BLANKET (28001100)  
TEMPORARY EROSION CONTROL SEEDING (28000250)  
TEMPORARY DITCH CHECK (28000305)  
OR INLET FILTERS IN PAVED AREAS (28000510)
- XXX- TEMPORARY FENCE
- CONSTRUCTION WORK ZONE
- TEMPORARY PAVEMENT
- FLOWLINE FOR PROPOSED DITCH
- CONSTRUCTION LIMIT
- TEMPORARY FENCE FOR  
TREE PROTECTION (SEE DETAIL ON SHEET 166 )
- TREE REMOVAL (SEE REMOVAL PLAN)



MATCH LINE STATION 3708+00.00  
 SEE BELOW LEFT

MATCH LINE STATION 3720+00.00  
 SEE SHEET NO. 170



2600 Warrenville Rd, Ste 203, Downers Grove, IL 60515  
 630.705.0110 voice, 630.839.2566 fax  
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DESIGNED	- TVN	REVISED	-
DRAWN	- JP	REVISED	-
CHECKED	- EG	REVISED	-
DATE	- 08/18/2014	REVISED	-

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>US ROUTE 30</b>	
<b>STAGE 1 EROSION &amp; SEDIMENT CONTROL PLAN</b>	
SCALE: 1"=50'	SHEET NAME: EC1-4
STA. 3696+00	STA. 3720+00

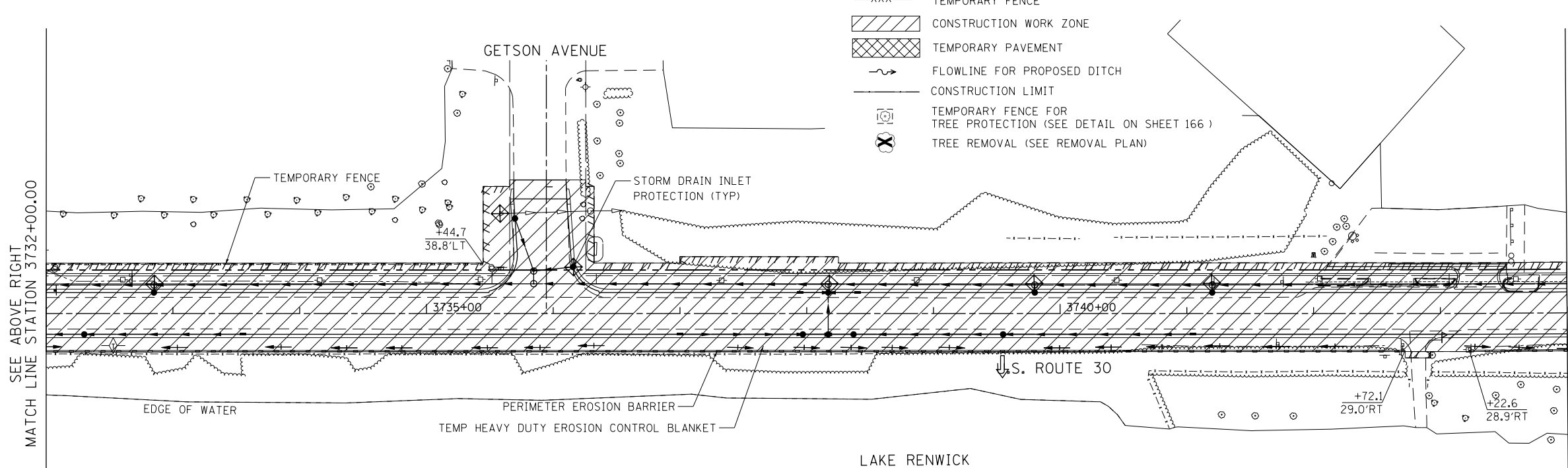
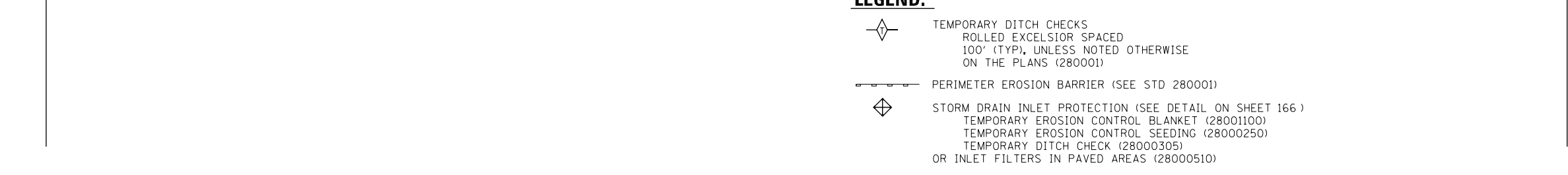
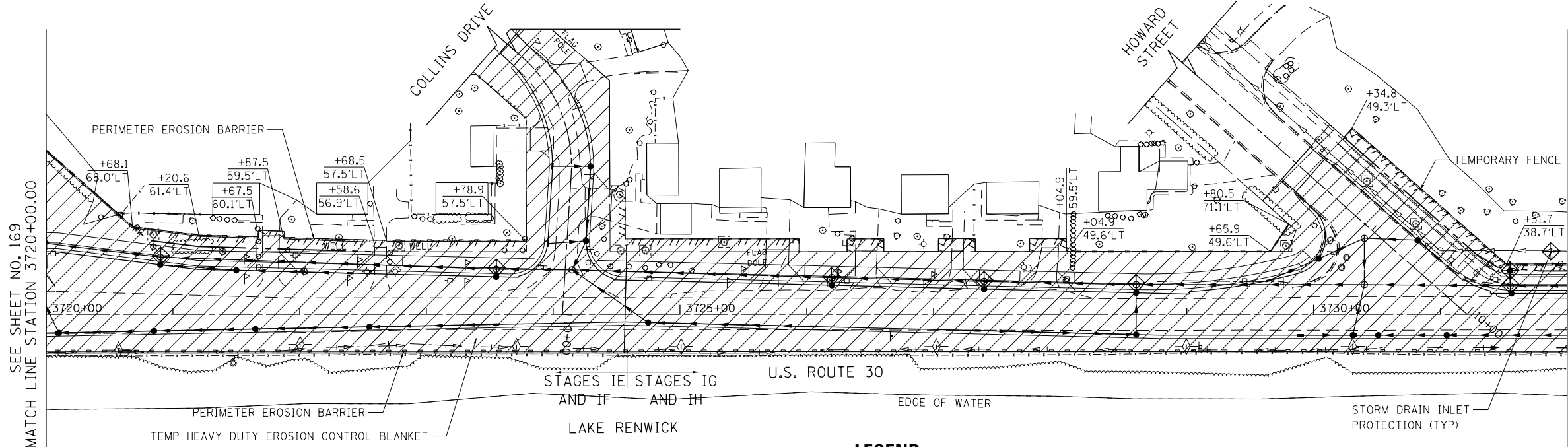
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	169
			CONTRACT NO. 60P95	

EC1-4

PLAN	SURVEYED	DATE
	ALIGNED	
	NOTED	
	CHECKED	
	RT. OF WAY	
	CHECKED	
	NO.	
	ADD FILE NAME	

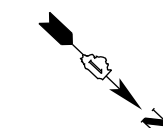
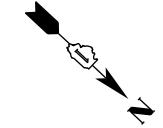
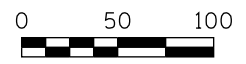
PROFILE	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	B.M. NOTED	
	STRUCTURE NOTATIONS	
	CHKD	
	NO.	
	ADD FILE NAME	

PLOT DATE = 10/23/2014  
 FILE NAME = G:\CH2\0012\Road\Shreve\EROSION CONTROL - MILLENNIA\0160P95-SHT-EROSI-5.dgn  
 USER NAME = 10600000 / in.  
 USER = 03/09/13/2014



**LEGEND:**

- TEMPORARY DITCH CHECKS  
ROLLED EXCELSIOR SPACED  
100' (TYP), UNLESS NOTED OTHERWISE  
ON THE PLANS (280001)
- PERIMETER EROSION BARRIER (SEE STD 280001)
- STORM DRAIN INLET PROTECTION (SEE DETAIL ON SHEET 166 )  
TEMPORARY EROSION CONTROL BLANKET (28001100)  
TEMPORARY EROSION CONTROL SEEDING (28000250)  
TEMPORARY DITCH CHECK (28000305)  
OR INLET FILTERS IN PAVED AREAS (28000510)
- XXX— TEMPORARY FENCE
- CONSTRUCTION WORK ZONE
- TEMPORARY PAVEMENT
- FLOWLINE FOR PROPOSED DITCH
- CONSTRUCTION LIMIT
- TEMPORARY FENCE FOR  
TREE PROTECTION (SEE DETAIL ON SHEET 166 )
- TREE REMOVAL (SEE REMOVAL PLAN)



SEE SHEET NO. 169  
MATCH LINE STATION 3720+00.00

MATCH LINE STATION 3732+00.00  
SEE BELOW LEFT

SEE ABOVE RIGHT  
MATCH LINE STATION 3732+00.00

MATCH LINE STATION 3744+00.00  
SEE SHEET NO. 171

EC1-5

2600 Warrenville Rd, Ste 203, Downers Grove, IL 60515  
 630.705.0110 voice, 630.839.2566 fax  
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**MILLENNIA PROFESSIONAL SERVICES**

DESIGNED - TVN	REVISED -
DRAWN - JP	REVISED -
CHECKED - EG	REVISED -
DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

<b>US ROUTE 30</b>			
<b>STAGE 1 EROSION &amp; SEDIMENT CONTROL PLAN</b>			
SCALE: 1"=50'	SHEET NAME: EC1-5	STA. 3720+00.00 TO STA. 3744+00.00	

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	170
				CONTRACT NO. 60P95

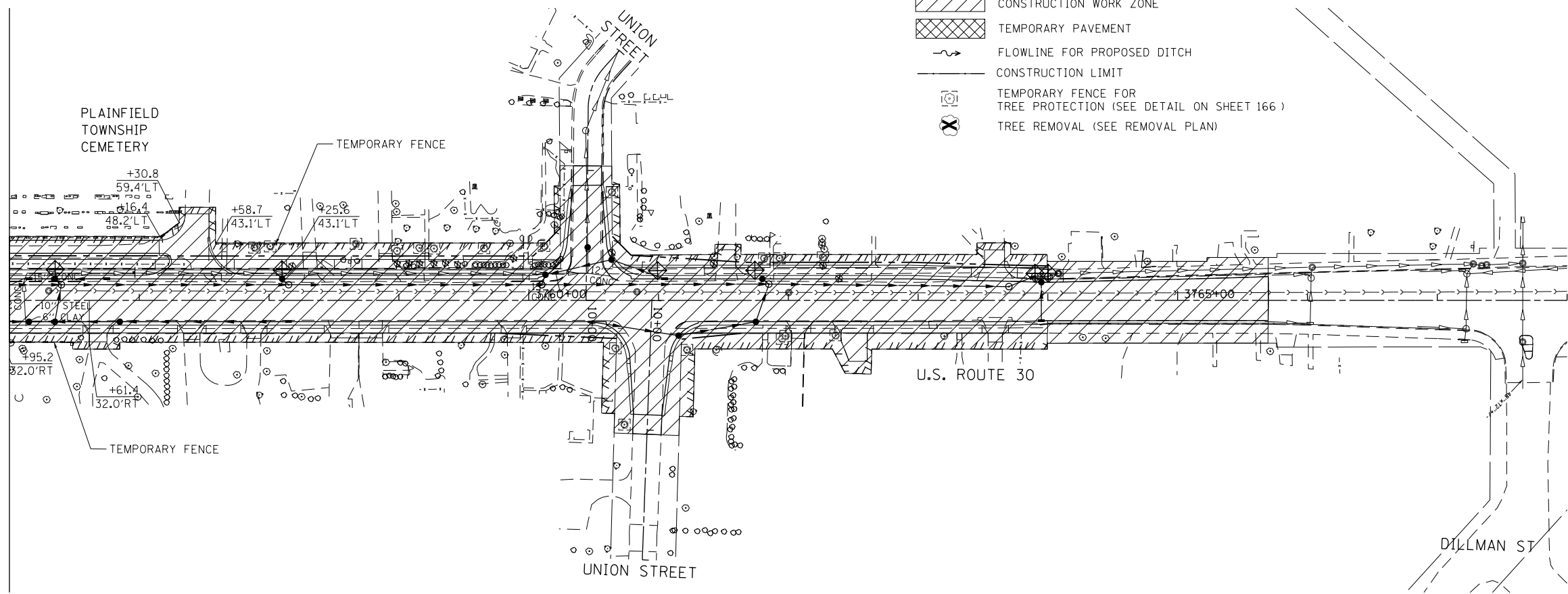
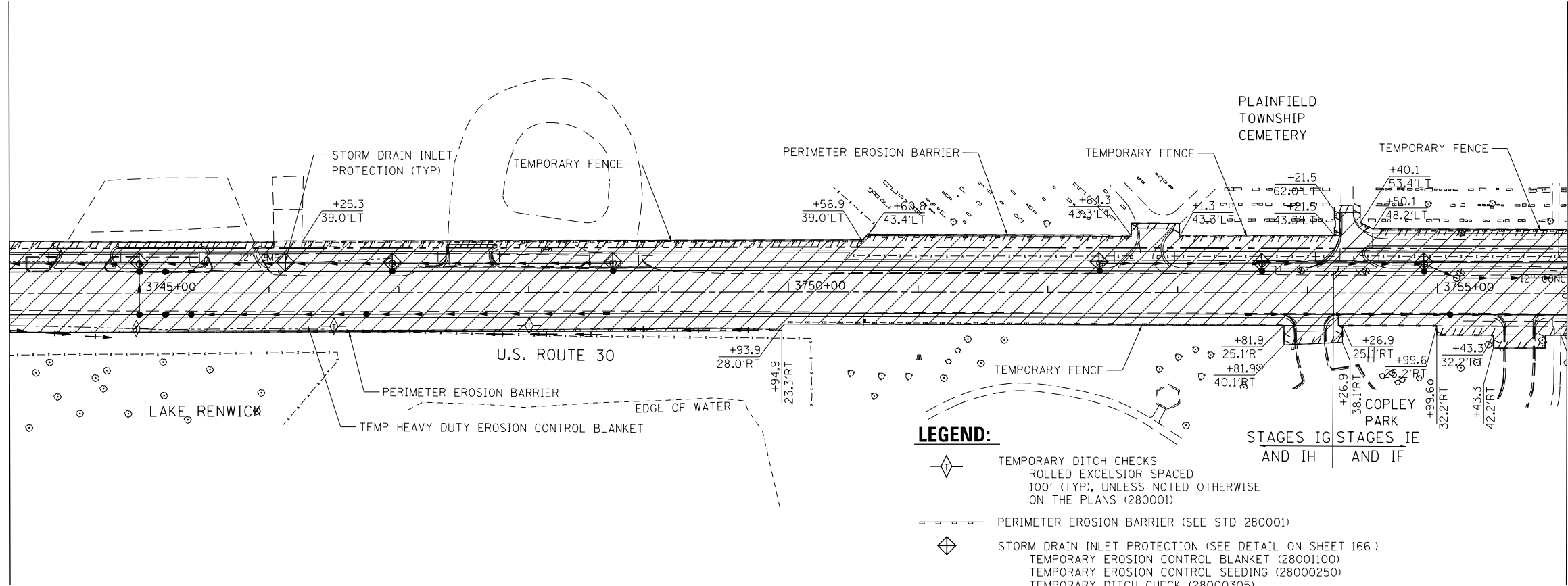
PLAN	SURVEYED	DATE
	ALIGNED	
	NOTED	
	RT. OF WAY CHECKED	
	NO. _____	
	ADD FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	B.M. NOTED	
	STRUCTURE NOTATIONS CHKD	
	NO. _____	

10/23/2014  
 G:\CH2\0012\Road\Shreve\EROSION CONTROL - MILLENNIA\1060P95-SHT-EROSI-6.dgn  
 FILE NAME = 1060P95  
 USER NAME = jg

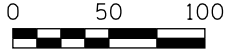
SEE SHEET NO. 170  
 MATCH LINE STATION 3744+00.00

SEE ABOVE RIGHT  
 MATCH LINE STATION 3756+00.00



**LEGEND:**

- TEMPORARY DITCH CHECKS  
ROLLED EXCELSIOR SPACED  
100' (TYP), UNLESS NOTED OTHERWISE  
ON THE PLANS (280001)
- PERIMETER EROSION BARRIER (SEE STD 280001)
- STORM DRAIN INLET PROTECTION (SEE DETAIL ON SHEET 166)  
TEMPORARY EROSION CONTROL BLANKET (28001100)  
TEMPORARY EROSION CONTROL SEEDING (28000250)  
TEMPORARY DITCH CHECK (28000305)  
OR INLET FILTERS IN PAVED AREAS (28000510)
- TEMPORARY FENCE
- CONSTRUCTION WORK ZONE
- TEMPORARY PAVEMENT
- FLOWLINE FOR PROPOSED DITCH
- CONSTRUCTION LIMIT
- TEMPORARY FENCE FOR  
TREE PROTECTION (SEE DETAIL ON SHEET 166)
- TREE REMOVAL (SEE REMOVAL PLAN)



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DESIGNED	- TVN	REVISED	-
DRAWN	- JP	REVISED	-
CHECKED	- EG	REVISED	-
DATE	- 08/18/2014	REVISED	-

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**US ROUTE 30  
 STAGE 1 EROSION & SEDIMENT CONTROL PLAN**

SCALE: 1"=50' SHEET NAME: EC1-6 STA. 3744+00.00 TO STA. 3768+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	171
			CONTRACT NO. 60P95	

EC1-6

D:\60P95-SHT-EROSI-6.dgn

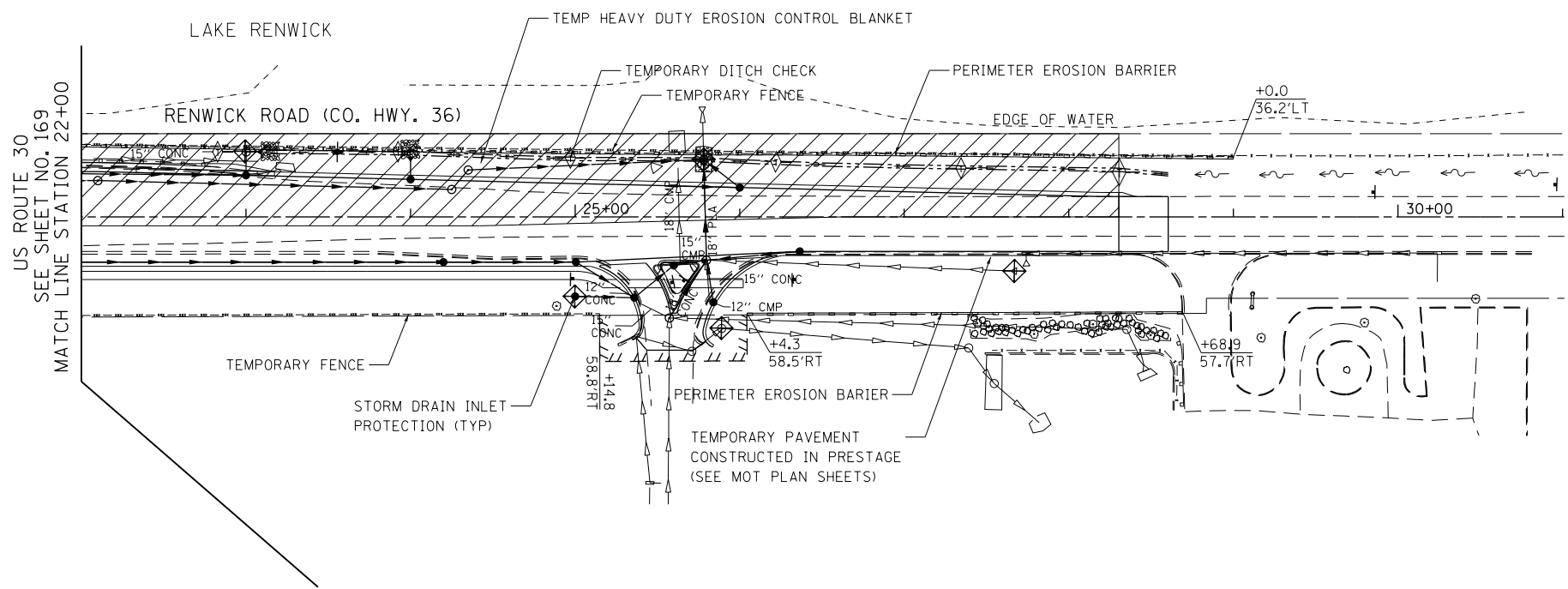
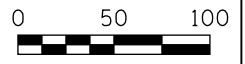
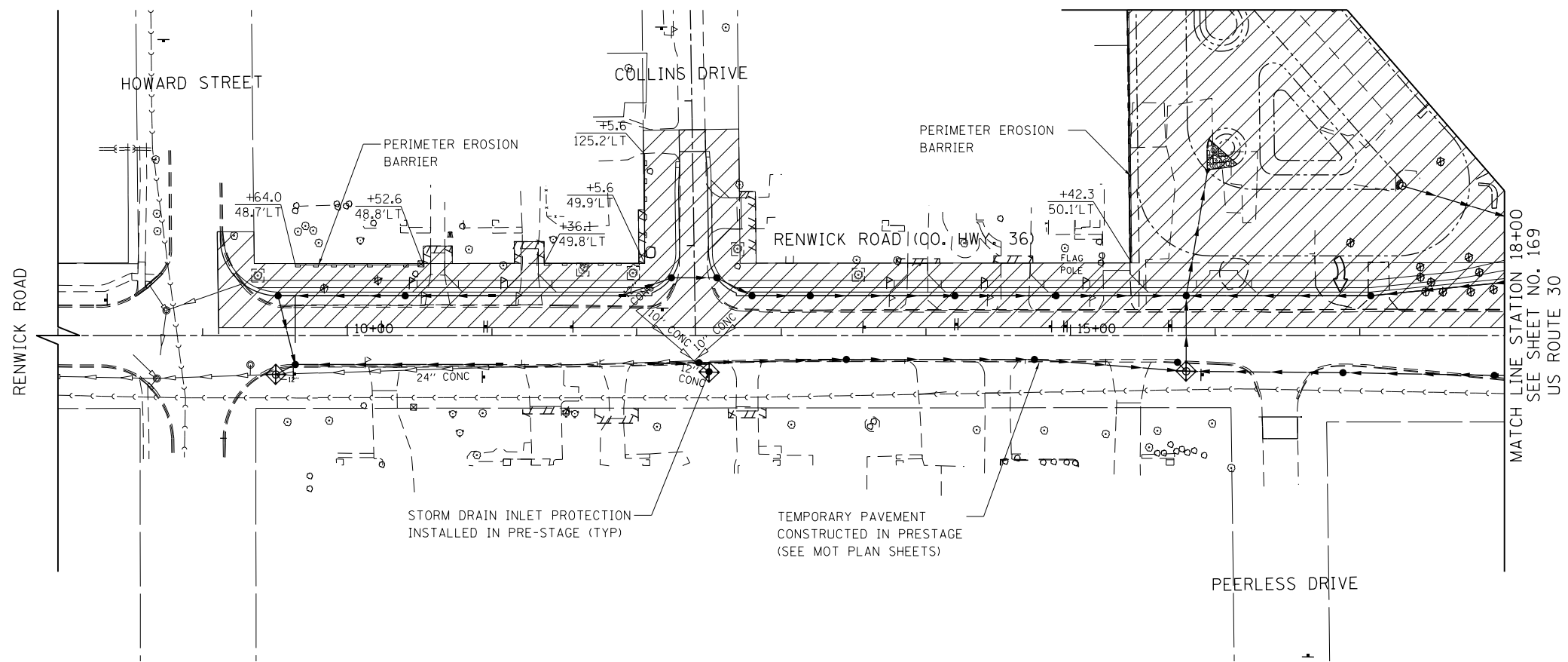




PLAN	SURVEYED	DATE
	ALIGNED	
	NOTED	
	RT. OF WAY CHECKED	
	NO. _____	
	ADD FILE NAME	


PROFILE	SURVEYED	DATE
	PLOTTED	
	NOTED	
	STRUCTURE NOTATIONS CHKD	
	NO. _____	

10/23/2014  
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 USER: NAME = 18600000 / in.  
 USER: NAME = 18600000 / in.



**LEGEND:**

- TEMPORARY DITCH CHECKS  
ROLLED EXCELSIOR SPACED  
100' (TYP), UNLESS NOTED OTHERWISE  
ON THE PLANS (280001)
- PERIMETER EROSION BARRIER (SEE STD 280001)
- STORM DRAIN INLET PROTECTION (SEE DETAIL ON SHEET 166)  
TEMPORARY EROSION CONTROL BLANKET (28001100)  
TEMPORARY EROSION CONTROL SEEDING (28000250)  
TEMPORARY DITCH CHECK (28000305)  
OR INLET FILTERS IN PAVED AREAS (28000510)
- XXX TEMPORARY FENCE
- CONSTRUCTION WORK ZONE
- TEMPORARY PAVEMENT
- FLOWLINE FOR PROPOSED DITCH
- CONSTRUCTION LIMIT
- TEMPORARY FENCE FOR TREE PROTECTION (SEE DETAIL ON SHEET 166)
- TREE REMOVAL (SEE REMOVAL PLAN)



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 630.705.0110 voice, 630.839.2566 fax  
 www.mps-il.com

**MILLENNIA PROFESSIONAL SERVICES**

DESIGNED	- TVN	REVISED	-
DRAWN	- JP	REVISED	-
CHECKED	- EG	REVISED	-
DATE	- 08/18/2014	REVISED	-

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**RENWICK ROAD  
 STAGE 1 EROSION & SEDIMENT CONTROL PLAN**

SCALE: 1"=50'    SHEET NAME: EC1-8    STA. 8+00    TO STA. 32+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	173
			CONTRACT NO. 60P95	
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

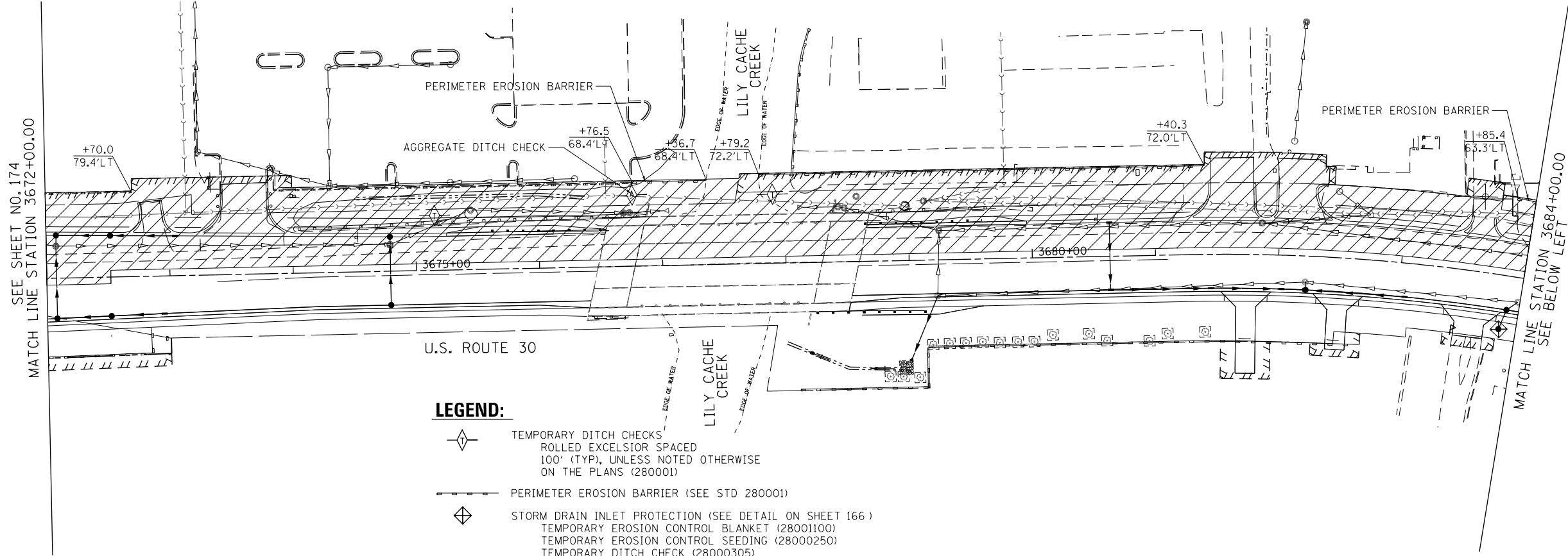
EC1-8



PLAN	SURVEYED	DATE
	ALIGNED	
	NOTED	
	RT. OF WAY CHECKED	
	NO. _____	
	ADD FILE NAME	

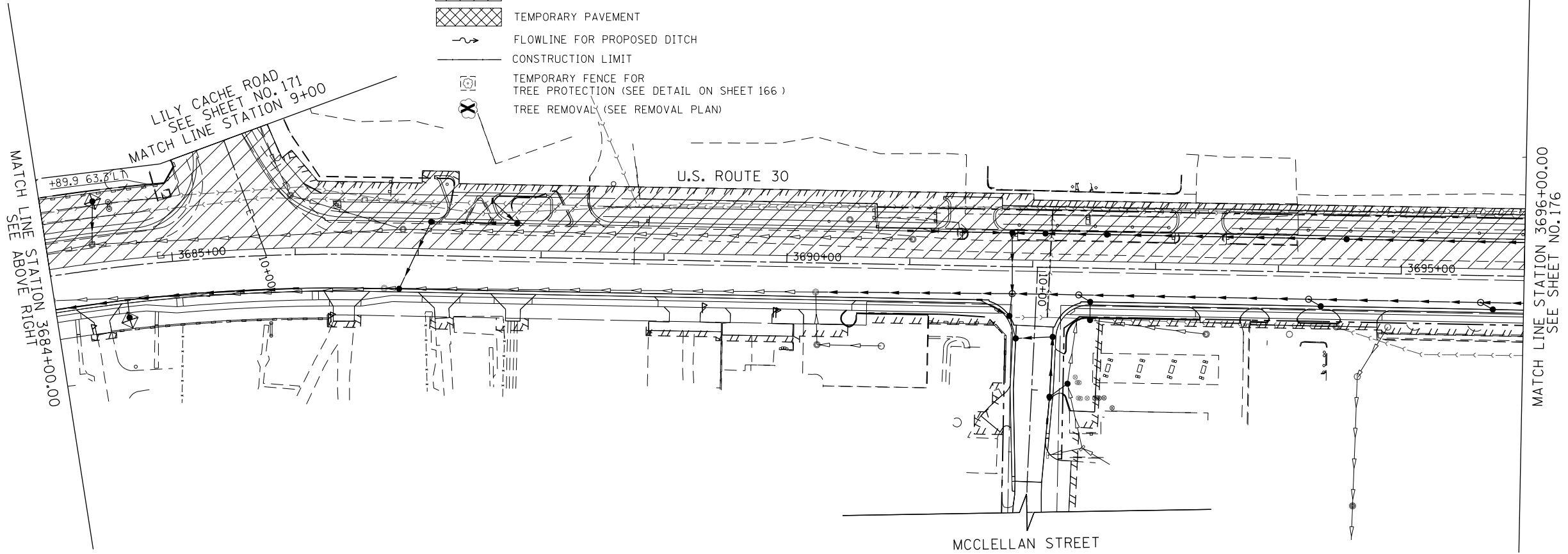
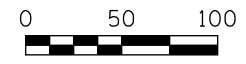
PROFILE	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	B.M. NOTED	
	STRUCTURE NOTATIONS CHK'D	
	NO. _____	

PLOT DATE = 10/23/2014  
 FILE NAME = G:\CH12\0012\Road\Shreve\EROSION CONTROL - MILLENNIA\1060P95-SHT-EROS2-2.dgn  
 USER NAME = 10600900 / in.  
 USER = 03/09/13/2014



**LEGEND:**

- TEMPORARY DITCH CHECKS  
ROLLED EXCELSIOR SPACED  
100' (TYP), UNLESS NOTED OTHERWISE  
ON THE PLANS (280001)
- PERIMETER EROSION BARRIER (SEE STD 280001)
- STORM DRAIN INLET PROTECTION (SEE DETAIL ON SHEET 166 )  
TEMPORARY EROSION CONTROL BLANKET (28001100)  
TEMPORARY EROSION CONTROL SEEDING (28000250)  
TEMPORARY DITCH CHECK (28000305)  
OR INLET FILTERS IN PAVED AREAS (28000510)
- XXX- TEMPORARY FENCE
- CONSTRUCTION WORK ZONE
- TEMPORARY PAVEMENT
- FLOWLINE FOR PROPOSED DITCH
- CONSTRUCTION LIMIT
- TEMPORARY FENCE FOR  
TREE PROTECTION (SEE DETAIL ON SHEET 166 )  
TREE REMOVAL (SEE REMOVAL PLAN)



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**MILLENNIA PROFESSIONAL SERVICES**

DESIGNED - TVN	REVISED -
DRAWN - JP	REVISED -
CHECKED - EG	REVISED -
DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>US ROUTE 30    STAGE 2 EROSION &amp; SEDIMENT CONTROL PLAN</b>			
SCALE: 1"=50'	SHEET NAME: EC-02	STA. 3672+00.00	TO STA. 3696+00.00

F.A.P. RTE. 575	SECTION 14W - R	COUNTY WILL	TOTAL SHEETS 681	SHEET NO. 175
CONTRACT NO. 60P95				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

EC2-2

D:\60P95-SHT-EROS2-2.dgn

PLAN	SURVEYED	DATE
	ALIGNED	
	NOTED	
	RT. OF WAY CHECKED	
	NO. _____	
	ADD FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	
	NOTED	
	B.M. NOTED	
	STRUCTURE NOTATIONS CHK'D	
	NO. _____	
	ADD FILE NAME	

PLOT DATE = 10/23/2014  
FILE NAME = G:\CH12\0012\Road\Shwa\Shwa\EROSION CONTROL - MILLENNIA\DIGP95-SHT-EROS2-3.dgn  
USER NAME = 100.0000 / in.  
DATE = 08/18/2014











SEE SHEET NO. 175  
MATCH LINE STATION 3696+00.00

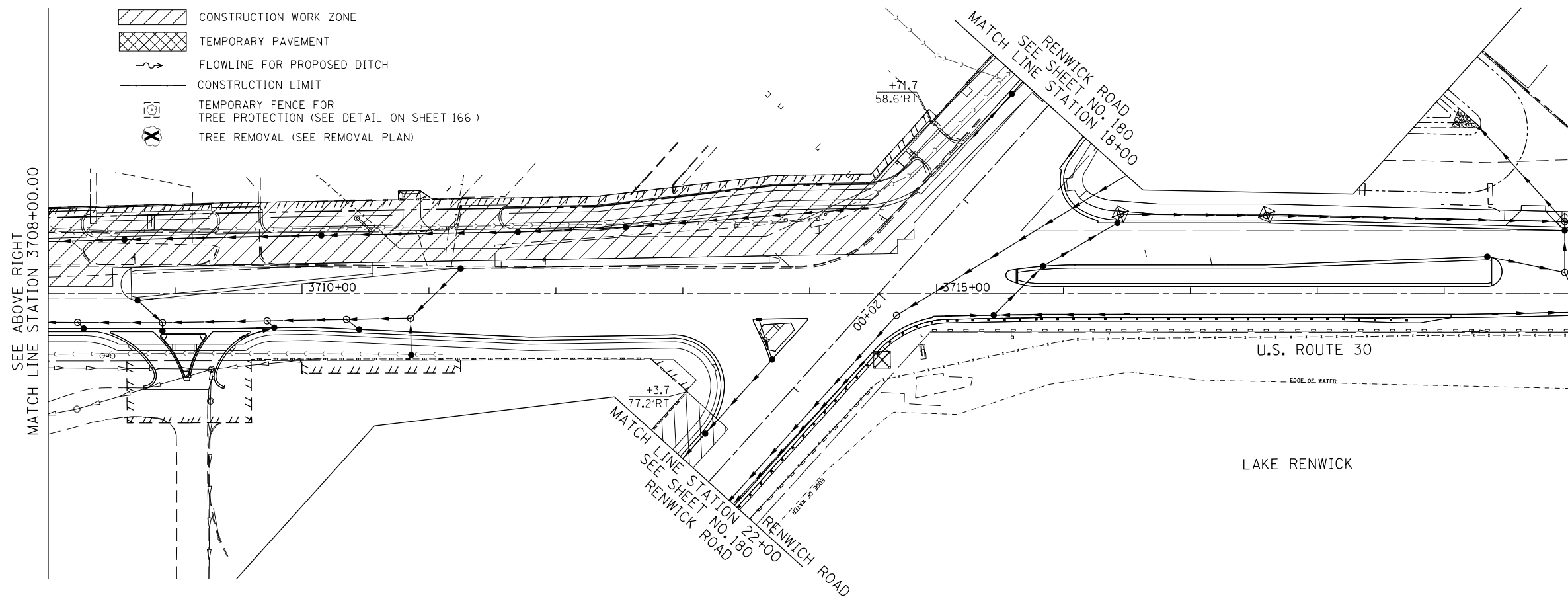
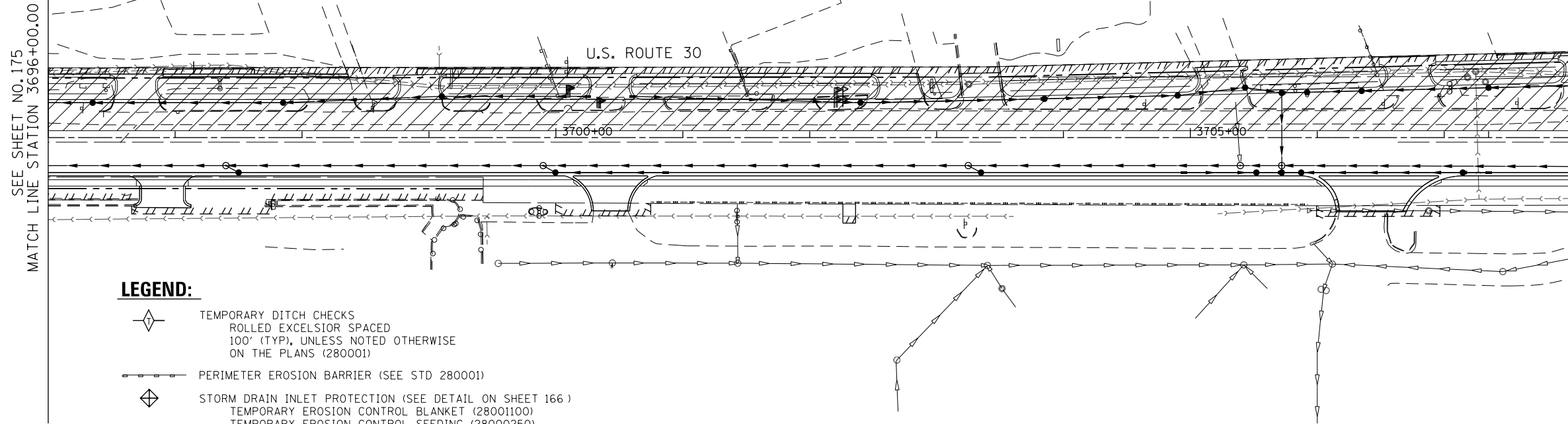
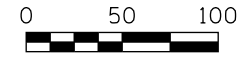
SEE ABOVE RIGHT  
MATCH LINE STATION 3708+00.00

MATCH LINE STATION 3708+00.00  
SEE BELOW LEFT

MATCH LINE STATION 3720+00.00  
SEE SHEET NO. 177

**LEGEND:**

-  TEMPORARY DITCH CHECKS  
ROLLED EXCELSIOR SPACED  
100' (TYP), UNLESS NOTED OTHERWISE  
ON THE PLANS (280001)
-  PERIMETER EROSION BARRIER (SEE STD 280001)
-  STORM DRAIN INLET PROTECTION (SEE DETAIL ON SHEET 166 )  
TEMPORARY EROSION CONTROL BLANKET (28001100)  
TEMPORARY EROSION CONTROL SEEDING (28000250)  
TEMPORARY DITCH CHECK (28000305)  
OR INLET FILTERS IN PAVED AREAS (28000510)
-  -XXX- TEMPORARY FENCE
-  CONSTRUCTION WORK ZONE
-  TEMPORARY PAVEMENT
-  FLOWLINE FOR PROPOSED DITCH
-  CONSTRUCTION LIMIT
-  TEMPORARY FENCE FOR  
TREE PROTECTION (SEE DETAIL ON SHEET 166 )
-  TREE REMOVAL (SEE REMOVAL PLAN)



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MILLENNIA PROFESSIONAL SERVICES

DESIGNED	- TVN	REVISED	-
DRAWN	- JP	REVISED	-
CHECKED	- EG	REVISED	-
DATE	- 08/18/2014	REVISED	-

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

US ROUTE 30  
STAGE 2 EROSION & SEDIMENT CONTROL PLAN

SCALE: 1"=50' SHEET NAME: EC-03 STA. 3696+00 TO STA. 3720+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	176
			CONTRACT NO. 60P95	
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

EC2-3

DIGP95-SHT-EROS2-3.dgn

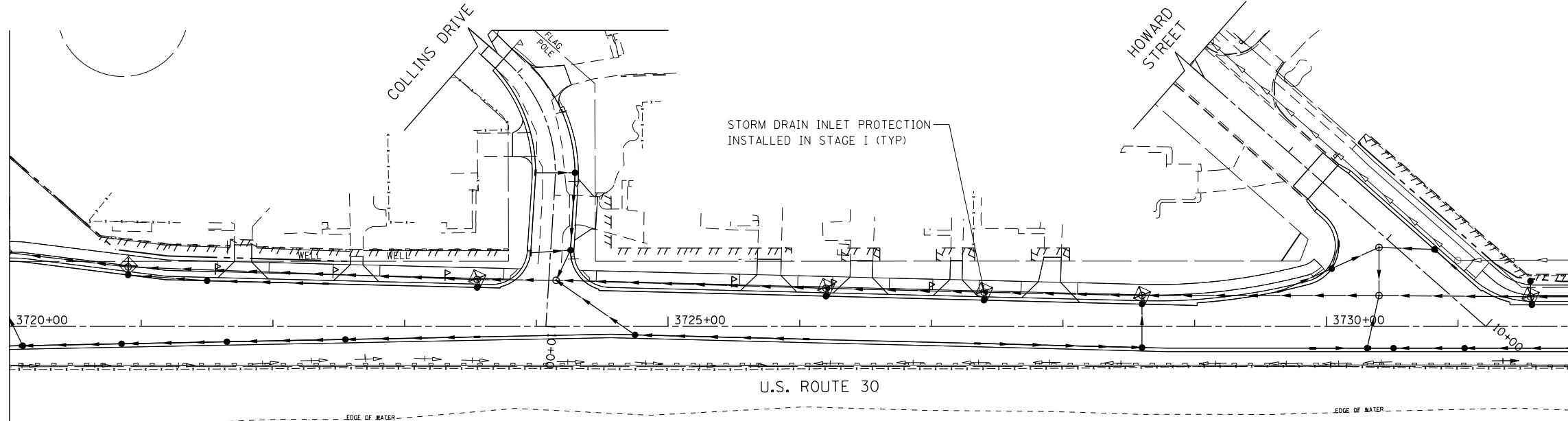
PLAN	SURVEYED	BY	DATE
	ALIGNED		
	NOTED		
	CHECKED		
	RT. OF WAY CHECKED		
	NO. _____		
	ADD FILE NAME		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	B.M. NOTED		
	STRUCTURE NOTATIONS CHK'D		
	NO. _____		

PLOT DATE = 10/23/2014  
 FILE NAME = G:\CH2\0012\Road\Shreve\EROSION CONTROL - MILLENNIA\DIGP95-SHT-EROS2-4.dgn  
 USER NAME = 180.0000 / in.  
 USER NAME = 0.0000 1/32"

SEE SHEET NO. 176  
 MATCH LINE STATION 3720+00.00

MATCH LINE STATION 3732+00.00  
 SEE BELOW LEFT



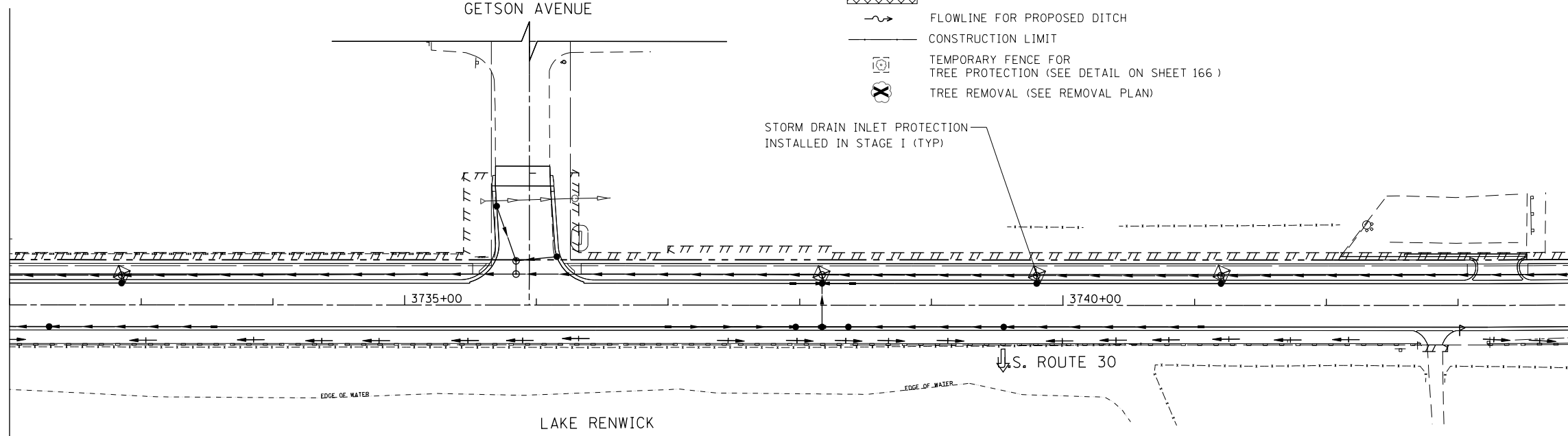
**LEGEND:**

- TEMPORARY DITCH CHECKS  
ROLLED EXCELSIOR SPACED  
100' (TYP), UNLESS NOTED OTHERWISE  
ON THE PLANS (280001)
- PERIMETER EROSION BARRIER (SEE STD 280001)
- STORM DRAIN INLET PROTECTION (SEE DETAIL ON SHEET 166 )  
TEMPORARY EROSION CONTROL BLANKET (28001100)  
TEMPORARY EROSION CONTROL SEEDING (28000250)  
TEMPORARY DITCH CHECK (28000305)  
OR INLET FILTERS IN PAVED AREAS (28000510)
- XXX— TEMPORARY FENCE
- CONSTRUCTION WORK ZONE
- TEMPORARY PAVEMENT
- FLOWLINE FOR PROPOSED DITCH
- CONSTRUCTION LIMIT
- TEMPORARY FENCE FOR  
TREE PROTECTION (SEE DETAIL ON SHEET 166 )
- TREE REMOVAL (SEE REMOVAL PLAN)



SEE ABOVE RIGHT  
 MATCH LINE STATION 3732+00.00

MATCH LINE STATION 3744+00.00  
 SEE SHEET NO. 178



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**MILLENNIA PROFESSIONAL SERVICES**

DESIGNED	- TVN	REVISED	-
DRAWN	- JP	REVISED	-
CHECKED	- EG	REVISED	-
DATE	- 08/18/2014	REVISED	-

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**US ROUTE 30  
 STAGE 2 EROSION & SEDIMENT CONTROL PLAN**

SCALE: 1"=50' SHEET NAME: EC1-4 STA. 3720+00.00 TO STA. 3744+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	177
			CONTRACT NO. 60P95	

EC2-4

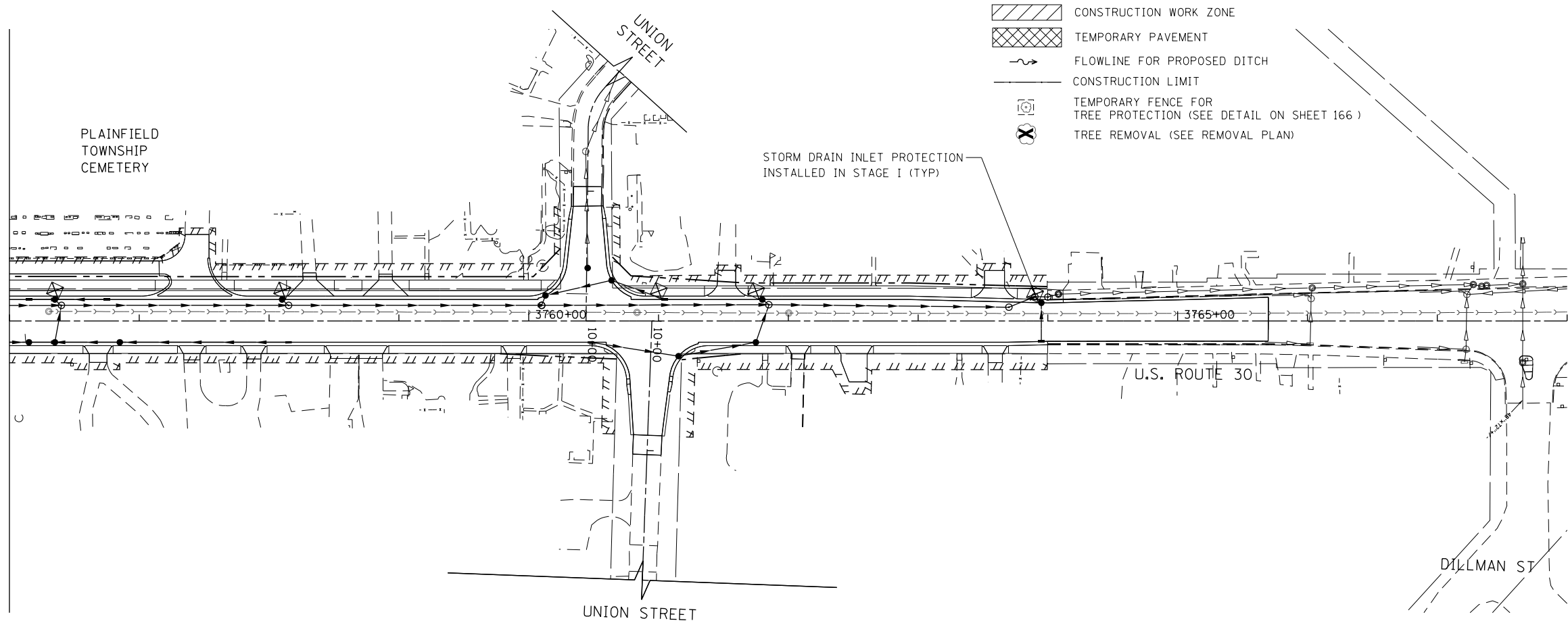
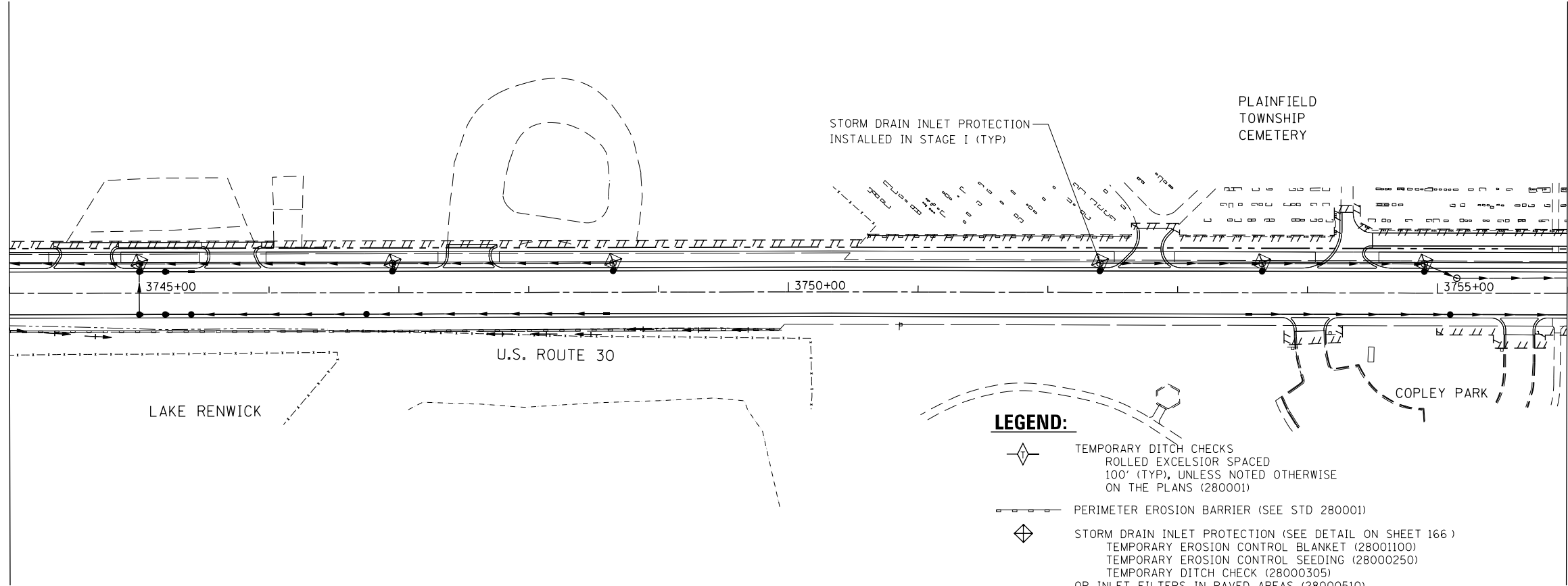
PLAN	SURVEYED	BY	DATE
	ALIGNED		
	NOTED		
	RT. OF WAY CHECKED		
	NO.		
	ADD FILE NAME		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	NO.		
	STRUCTURE NOTATIONS CHKD		

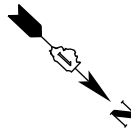
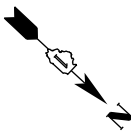
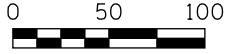
10/23/2014  
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 10600000 / in.  
 USER NAME =

SEE SHEET NO. 177  
 MATCH LINE STATION 3744+00.00

SEE ABOVE RIGHT  
 MATCH LINE STATION 3756+00.00



- LEGEND:**
- TEMPORARY DITCH CHECKS  
ROLLED EXCELSIOR SPACED  
100' (TYP), UNLESS NOTED OTHERWISE  
ON THE PLANS (280001)
  - PERIMETER EROSION BARRIER (SEE STD 280001)
  - STORM DRAIN INLET PROTECTION (SEE DETAIL ON SHEET 166 )  
TEMPORARY EROSION CONTROL BLANKET (28001100)  
TEMPORARY EROSION CONTROL SEEDING (28000250)  
TEMPORARY DITCH CHECK (28000305)  
OR INLET FILTERS IN PAVED AREAS (28000510)
  - XXX- TEMPORARY FENCE
  - CONSTRUCTION WORK ZONE
  - TEMPORARY PAVEMENT
  - FLOWLINE FOR PROPOSED DITCH
  - CONSTRUCTION LIMIT
  - TEMPORARY FENCE FOR  
TREE PROTECTION (SEE DETAIL ON SHEET 166 )
  - TREE REMOVAL (SEE REMOVAL PLAN)



MATCH LINE STATION 3756+00.00  
 SEE BELOW LEFT

EC2-5

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**MILLENNIA PROFESSIONAL SERVICES**

DESIGNED - TVN	REVISED -
DRAWN - JP	REVISED -
CHECKED - EG	REVISED -
DATE - 08/18/2014	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**US ROUTE 30  
 STAGE 2 EROSION & SEDIMENT CONTROL PLAN**

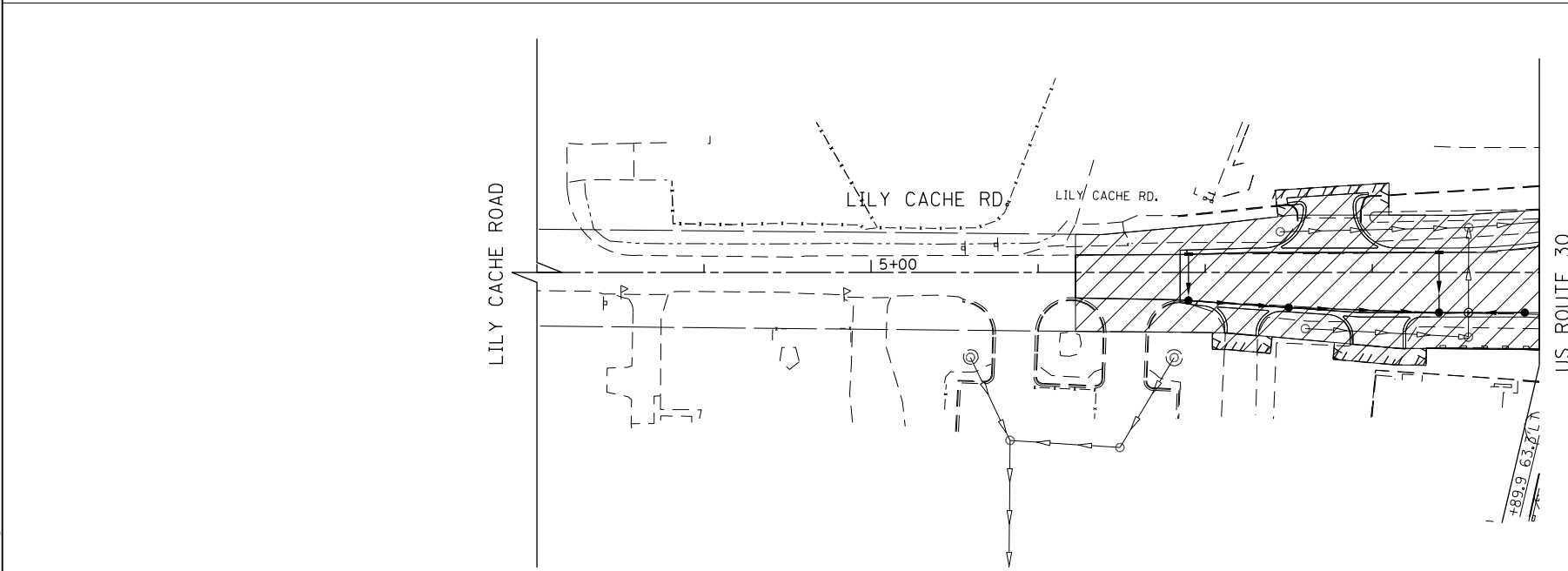
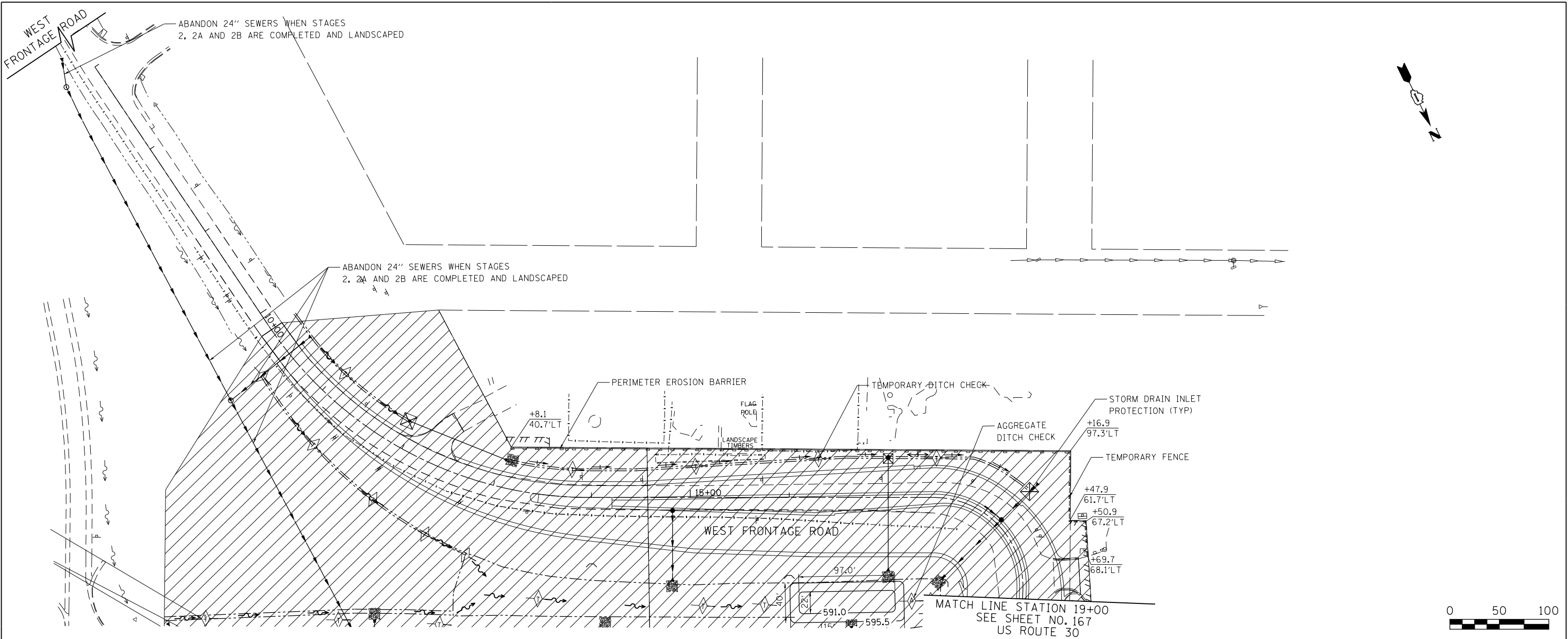
SCALE: 1"=50' SHEET NAME: EC2-5 STA. 3744+00.00 TO STA. 3768+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	178
			CONTRACT NO. 60P95	

PLAN	SURVEYED	DATE
NOTE BOOK	ALIGNED	BY
NO.	RT. OF WAY CHECKED	
	ADD FILE NAME	

PROFILE	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	B.M. NOTED	
	STRUCTURE NOTATIONS CHKD	

10/23/2014  
 G:\CH2\0017\Road\Shive\EROSION CONTROL - MILLENNIA\10160P95-SHT-EROS2-6.dgn  
 FILE NAME = 10160P95 / in.  
 USER NAME =



- LEGEND:**
- TEMPORARY DITCH CHECKS  
ROLLED EXCELSIOR SPACED  
100' (TYP), UNLESS NOTED OTHERWISE  
ON THE PLANS (280001)
  - PERIMETER EROSION BARRIER (SEE STD 280001)
  - STORM DRAIN INLET PROTECTION (SEE DETAIL ON SHEET 166 )  
TEMPORARY EROSION CONTROL BLANKET (28001100)  
TEMPORARY EROSION CONTROL SEEDING (28000250)  
TEMPORARY DITCH CHECK (28000305)  
OR INLET FILTERS IN PAVED AREAS (28000510)
  - TEMPORARY FENCE
  - CONSTRUCTION WORK ZONE
  - TEMPORARY PAVEMENT
  - FLOWLINE FOR PROPOSED DITCH
  - CONSTRUCTION LIMIT
  - TEMPORARY FENCE FOR  
TREE PROTECTION (SEE DETAIL ON SHEET 166 )
  - TREE REMOVAL (SEE REMOVAL PLAN)



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**MILLENNIA PROFESSIONAL SERVICES**

DESIGNED	- TVN	REVISED	-
DRAWN	- JP	REVISED	-
CHECKED	- EG	REVISED	-
DATE	- 08/18/2014	REVISED	-

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**WEST FRONTAGE ROAD AND LILY CACHE ROAD  
 STAGE 2 EROSION & SEDIMENT CONTROL PLAN**

SCALE: 1"=50'    SHEET NAME: EC1-7    STA.    TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	179
			CONTRACT NO. 60P95	
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

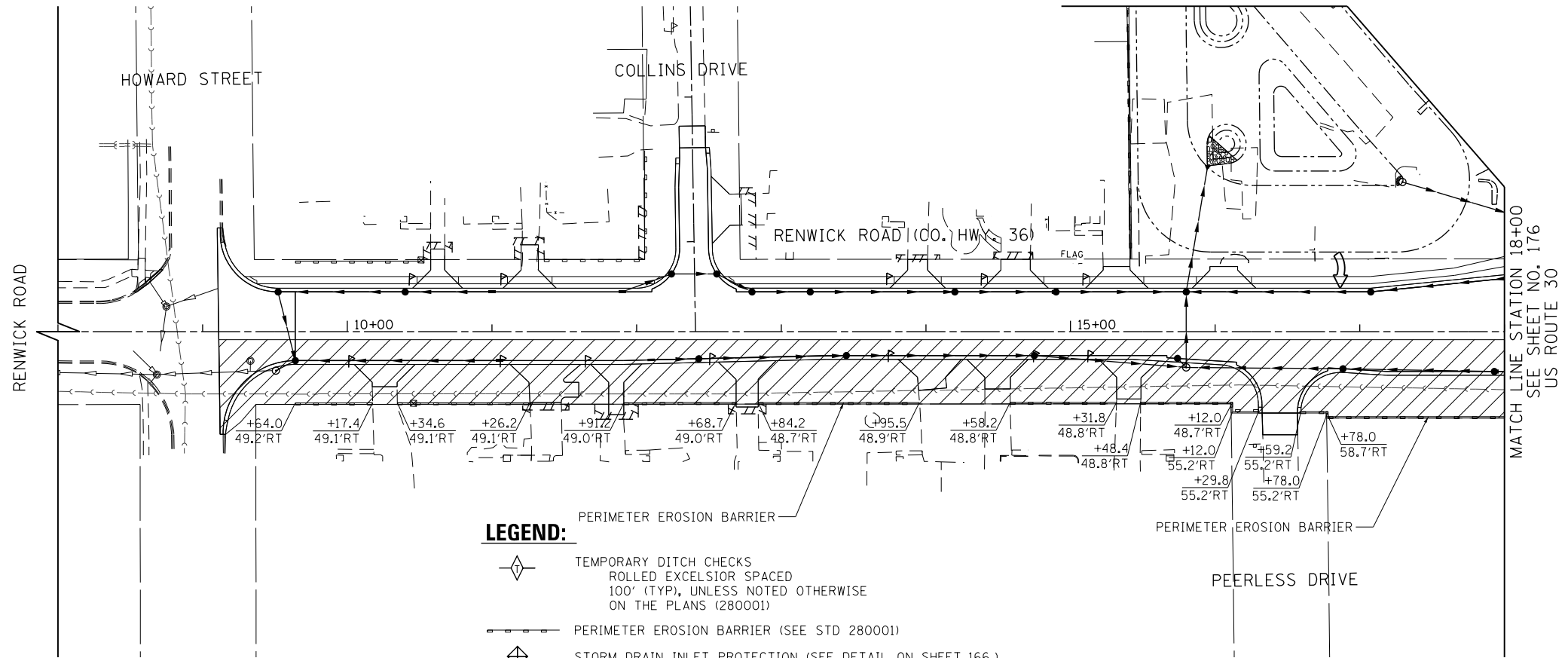
EC2-6

D:\60P95-SHT-EROS2-6.dgn

PLAN	SURVEYED	DATE
	ALIGNED	
	NOTED	
	RT. OF WAY CHECKED	
	NO. _____	
	ADD FILE NAME	

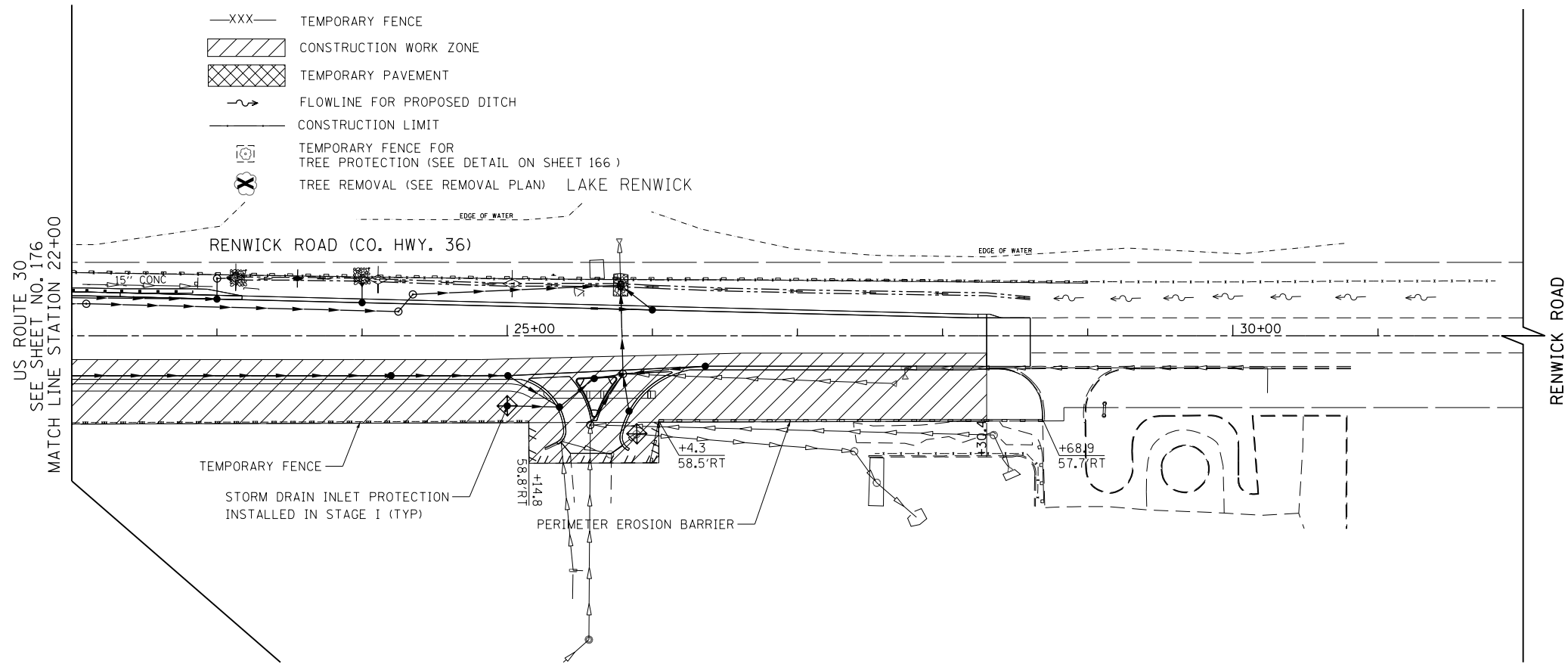
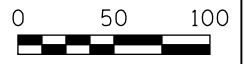
PROFILE	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	B.M. NOTED	
	STRUCTURE NOTATIONS CHK'D	
	NO. _____	

PLOT DATE = 10/23/2014  
 FILE NAME = G:\CH2\0012\Road\Shreve\EROSION CONTROL - MILLENNIA\DIGP95-SHT-EROS2-7.dgn  
 USER NAME = 180.0000 / in.  
 USER NAME = 0.0000 1/32"



**LEGEND:**

- TEMPORARY DITCH CHECKS  
ROLLED EXCELSIOR SPACED  
100' (TYP), UNLESS NOTED OTHERWISE  
ON THE PLANS (280001)
- PERIMETER EROSION BARRIER (SEE STD 280001)
- STORM DRAIN INLET PROTECTION (SEE DETAIL ON SHEET 166)  
TEMPORARY EROSION CONTROL BLANKET (28001100)  
TEMPORARY EROSION CONTROL SEEDING (28000250)  
TEMPORARY DITCH CHECK (28000305)  
OR INLET FILTERS IN PAVED AREAS (28000510)
- XXX- TEMPORARY FENCE
- CONSTRUCTION WORK ZONE
- TEMPORARY PAVEMENT
- FLOWLINE FOR PROPOSED DITCH
- CONSTRUCTION LIMIT
- TEMPORARY FENCE FOR  
TREE PROTECTION (SEE DETAIL ON SHEET 166)
- TREE REMOVAL (SEE REMOVAL PLAN) LAKE RENWICK



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**MILLENNIA PROFESSIONAL SERVICES**

DESIGNED	- TVN	REVISED	-
DRAWN	- JP	REVISED	-
CHECKED	- EG	REVISED	-
DATE	- 08/18/2014	REVISED	-

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**RENWICK ROAD  
STAGE 2 EROSION & SEDIMENT CONTROL PLAN**

SCALE: 1"=50'    SHEET NAME: EC-07    STA. 8+00    TO STA. 32+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	180
			CONTRACT NO. 60P95	
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

EC2-7



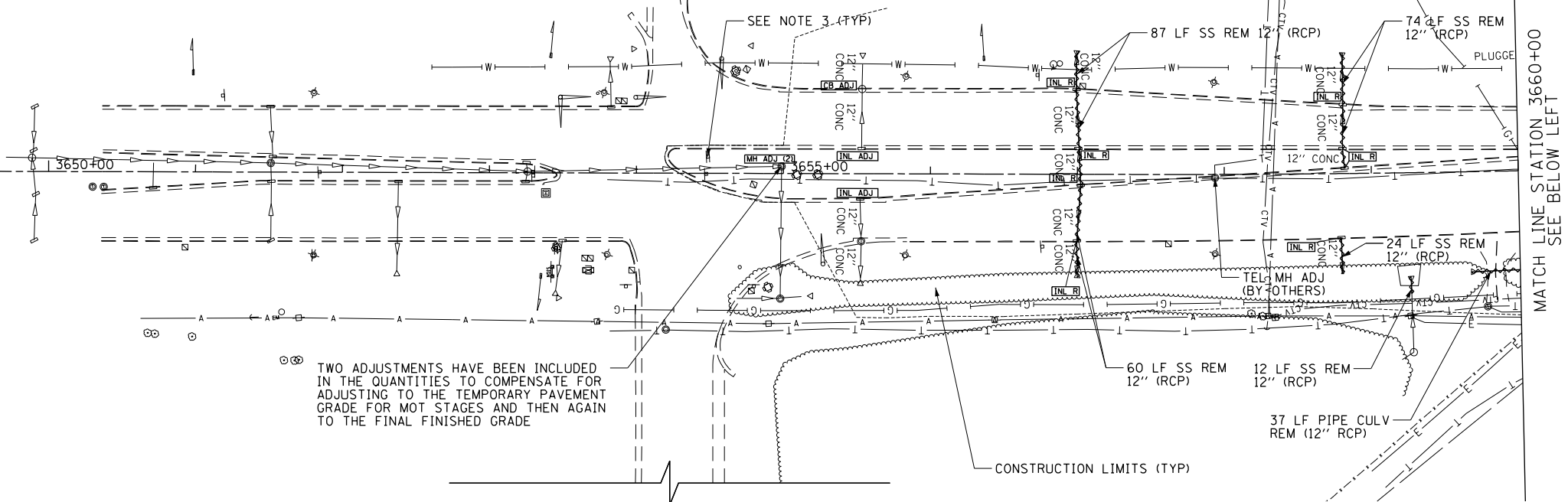


PLAN	SURVEYED	DATE
	PLOTTED	
	NOTED	
	BY	
	NO.	
	FILE NAME	

U.S. ROUTE 30

I-55 SB ON RAMP

LEGEND	
	STORM SEWER REMOVAL/CULVERT REMOVAL
	STRUCTURE TO BE ADJUSTED
	STRUCTURE TO BE REMOVED
	STRUCTURE TO BE RECONSTRUCTED
	ITEM TO BE RELOCATED



TWO ADJUSTMENTS HAVE BEEN INCLUDED IN THE QUANTITIES TO COMPENSATE FOR ADJUSTING TO THE TEMPORARY PAVEMENT GRADE FOR MOT STAGES AND THEN AGAIN TO THE FINAL FINISHED GRADE

CONSTRUCTION LIMITS (TYP)

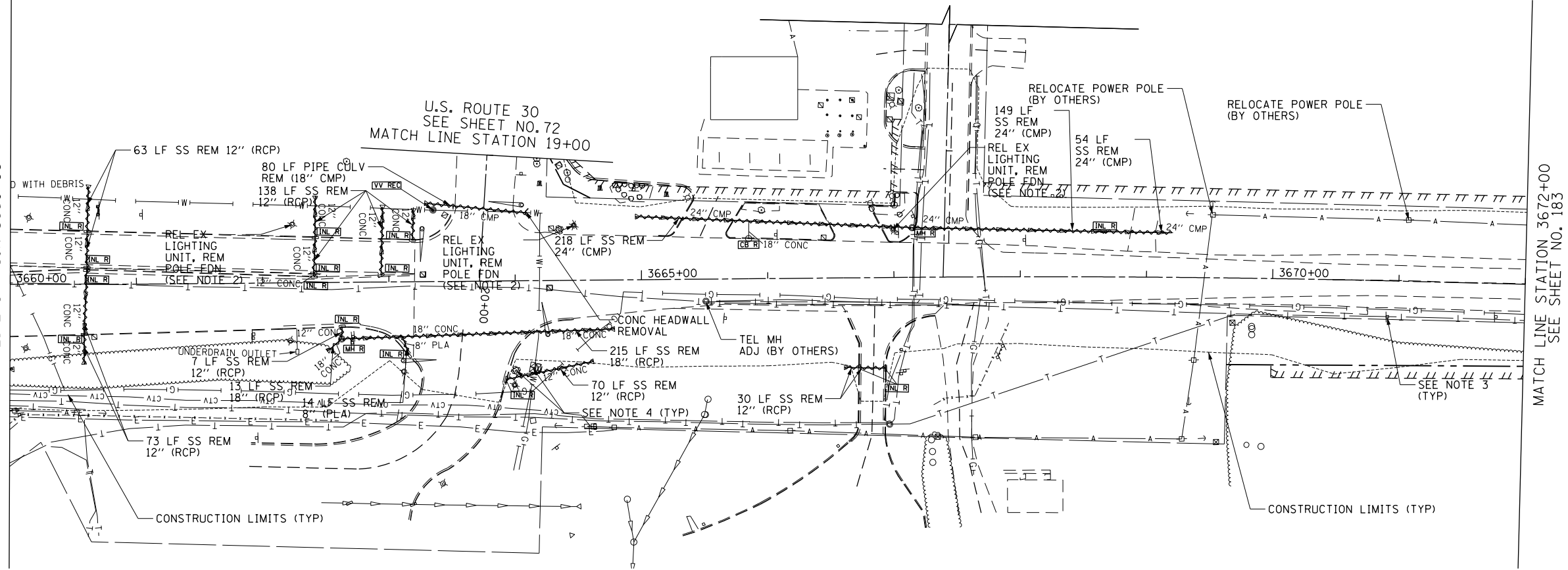
I-55 SB EXIT RAMP



PROFILE	SURVEYED	DATE
	PLOTTED	
	NOTED	
	BY	
	NO.	
	FILE NAME	

SEE ABOVE RIGHT  
MATCH LINE STATION 3660+00

SPANGLER ROAD



U.S. ROUTE 30  
SEE SHEET NO. 72  
MATCH LINE STATION 19+00

- NOTES:
- FOR WATER MAIN/SANITARY SEWER ADJUSTMENT/REMOVAL ITEMS SEE SHEETS 226 TO 233.
  - SEE PLAN AND PROFILE SHEET 75 FOR RELOCATED LIGHT POLE LOCATIONS.
  - FOR SIGNING REMOVALS AND RELOCATIONS SEE SHEETS 302 TO 322
  - FOR TRAFFIC SIGNAL ITEMS REMOVALS SEE SHEETS 340 TO 345.



USER NAME = bshoeffliger	DESIGNED -	REVISED -
FILE = D:\60P95-SHT-DRAIN-UTIL-REMOVAL-01.dgn	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE = 08/18/2014	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

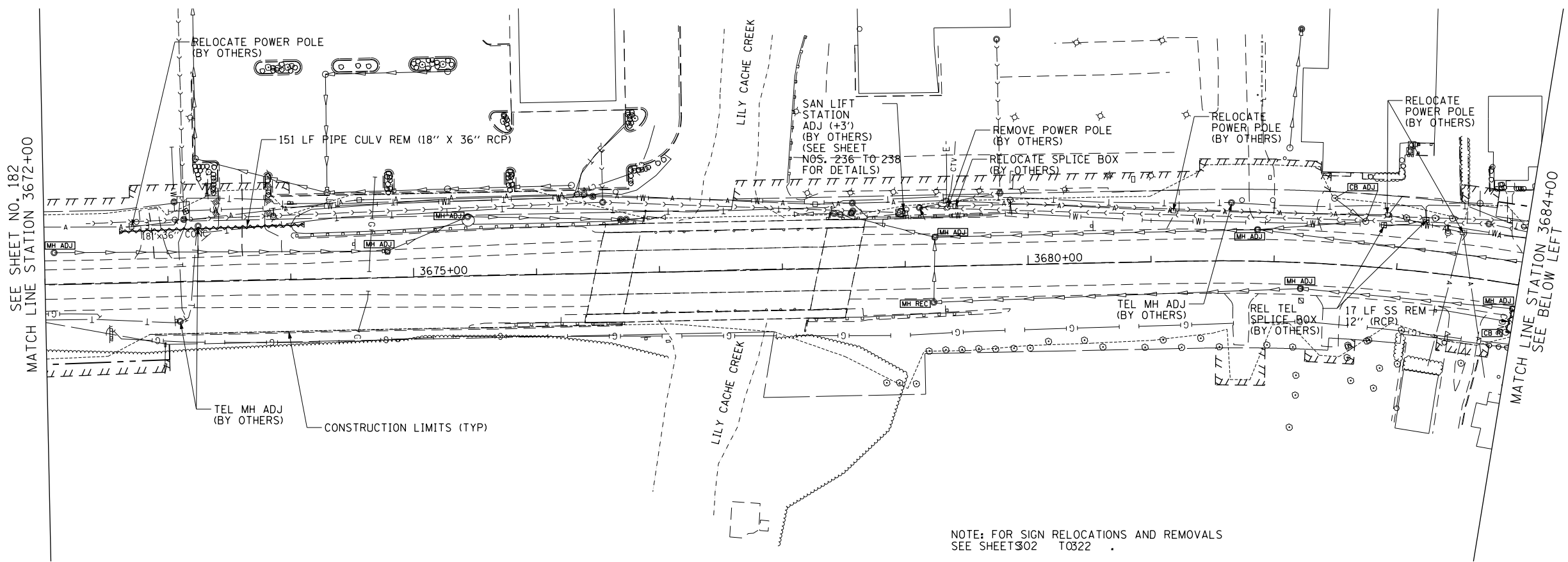
U.S. ROUTE 30 DRAINAGE AND UTILITIES REMOVAL PLAN

SCALE: 1" = 50' SHEET NAME: DUREM-01 STA. 3648+00 TO STA. 3672+00

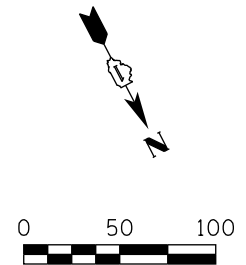
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	182
				CONTRACT NO. 60P95
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

FILE PATH = G:\CH2\0012\Road\Sheet\60P95-SHT-DRAIN-UTIL-REMOVAL-01.dgn

PLAN	SURVEYED	DATE
	PLOTTED	
	NOTED	
	BY	
	NO.	
	FILE NAME	

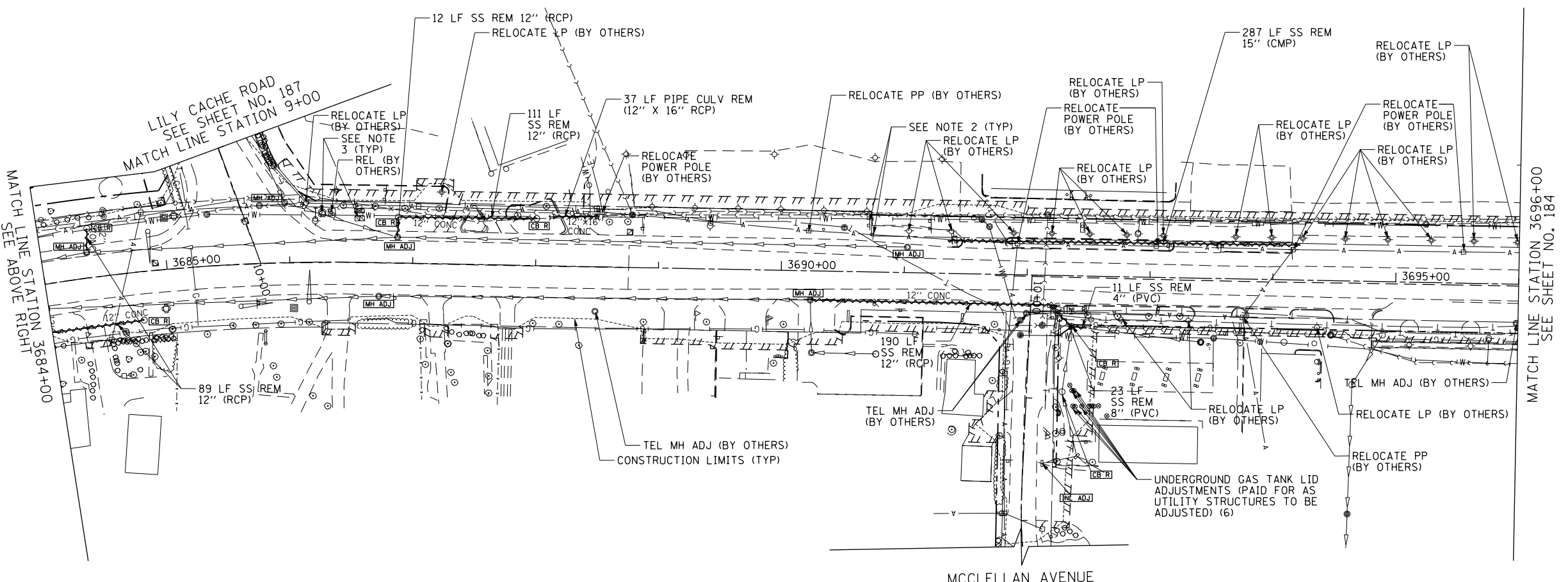


LEGEND	
	STORM SEWER REMOVAL/CULVERT REMOVAL
	STRUCTURE TO BE ADJUSTED
	STRUCTURE TO BE REMOVED
	STRUCTURE TO BE RECONSTRUCTED
	ITEM TO BE RELOCATED

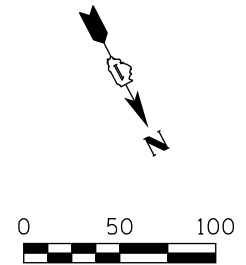


NOTE: FOR SIGN RELOCATIONS AND REMOVALS SEE SHEETS 02 TO 322

PROFILE	SURVEYED	DATE
	PLOTTED	
	NOTED	
	BY	
	NO.	
	FILE NAME	



- NOTES
1. FOR WATER MAIN/SANITARY SEWER ADJUSTMENT/REMOVAL ITEMS SEE SHEETS 226 TO 233.
  2. FOR SIGNING REMOVALS AND RELOCATIONS SEE SHEETS 302 TO 322
  3. FOR TRAFFIC SIGNAL ITEMS REMOVALS SEE SHEETS 340 TO 345.



MCCLELLAN AVENUE



USER NAME = bshoefliger	DESIGNED -	REVISED -
FILE = D:\60P95-SHT-DRAIN-UTIL-REMOVAL-02.dwg	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE = 08/18/2014	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

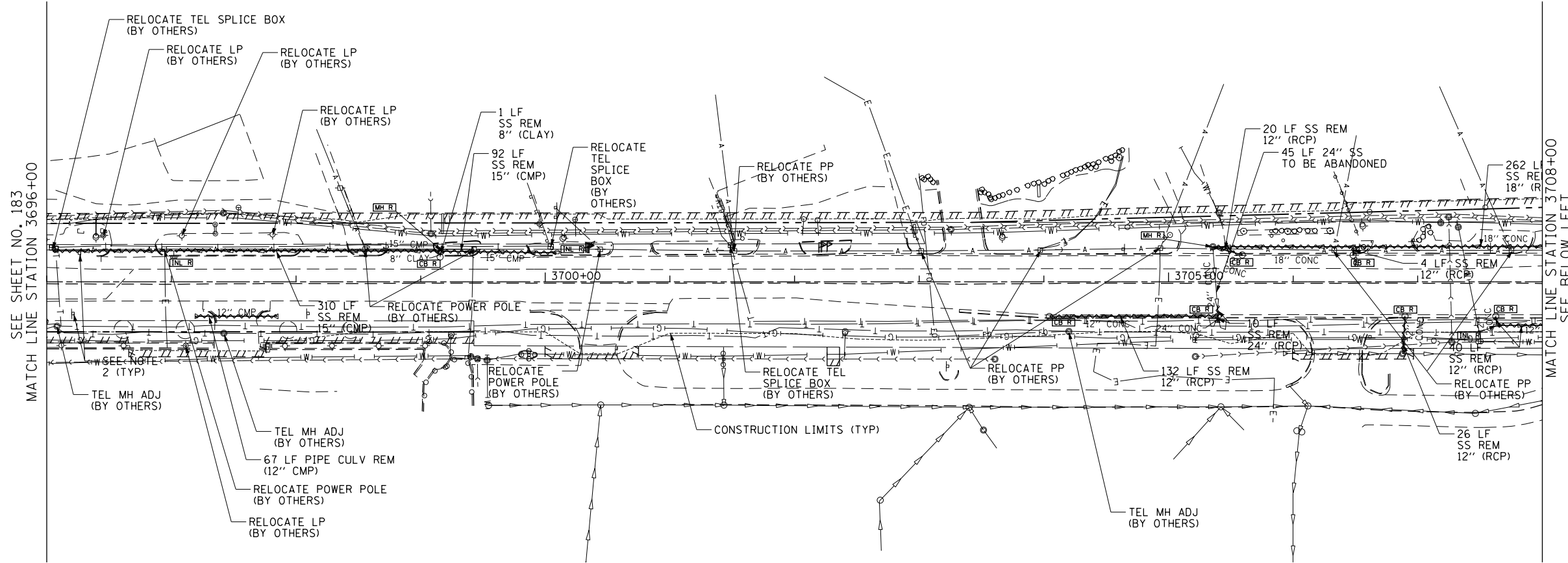
U.S. ROUTE 30 DRAINAGE AND UTILITIES REMOVAL PLAN

SCALE: 1" = 50' SHEET NAME: DUREM-02 STA. 3672+00 TO STA. 3696+00

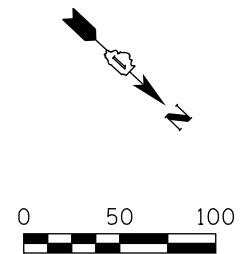
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	183
				CONTRACT NO. 60P95
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

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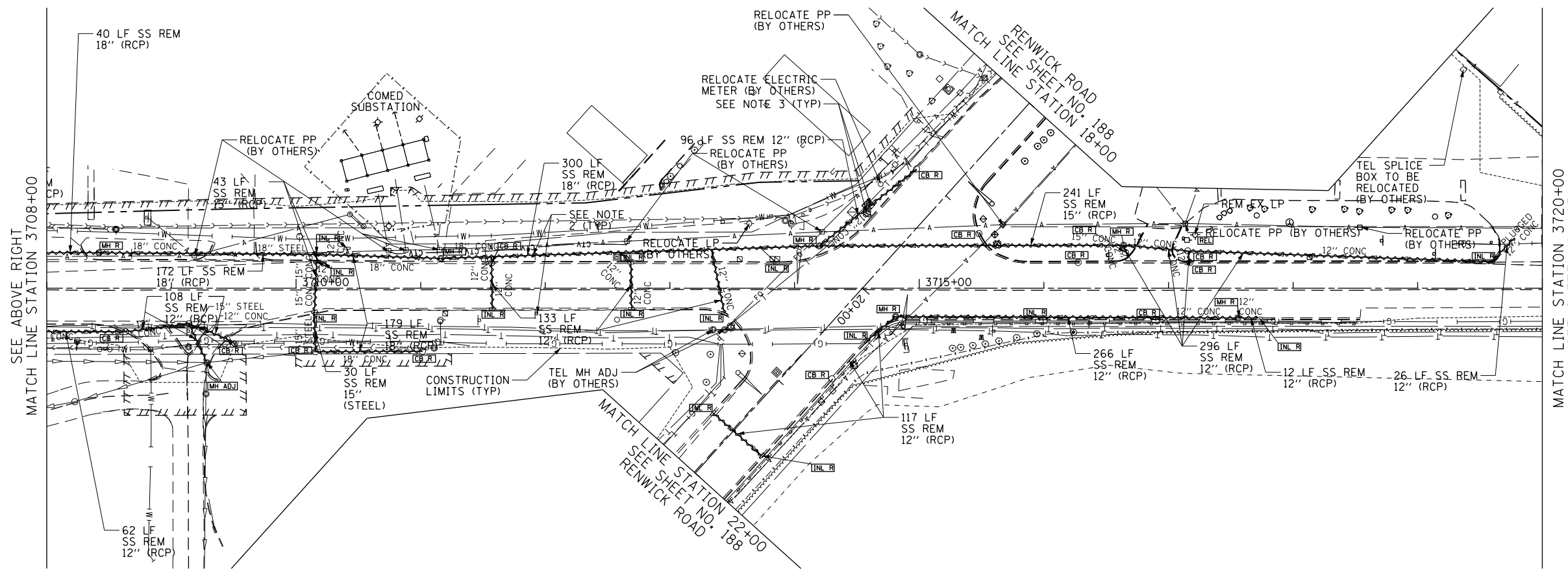
PLAN	SURVEYED	DATE
	PLOTTED	BY
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK	
	NO. OF MAY CHECKED	
	CADD FILE NAME	



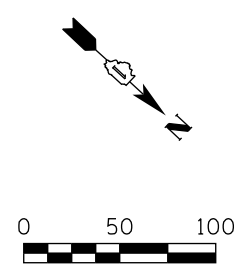
LEGEND	
	STORM SEWER REMOVAL/CULVERT REMOVAL
	STRUCTURE TO BE ADJUSTED
	STRUCTURE TO BE REMOVED
	STRUCTURE TO BE RECONSTRUCTED
	ITEM TO BE RELOCATED



PROFILE	SURVEYED	DATE
	PLOTTED	BY
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK	
	NO. OF MAY CHECKED	
	CADD FILE NAME	



- NOTES
1. FOR WATER MAIN/SANITARY SEWER ADJUSTMENT/REMOVAL ITEMS SEE SHEETS 226 TO 233.
  2. FOR SIGNING REMOVALS AND RELOCATIONS SEE SHEETS 302 TO 322
  3. FOR TRAFFIC SIGNAL ITEMS REMOVALS SEE SHEETS 340 TO 345.



USER NAME = bshae1iger	DESIGNED -	REVISED -
FILE = D:\60P95-SHT-DRAIN-UTIL-REMOVAL-03.dwg	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE = 08/18/2014	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

U.S. ROUTE 30 DRAINAGE AND UTILITIES REMOVAL PLAN

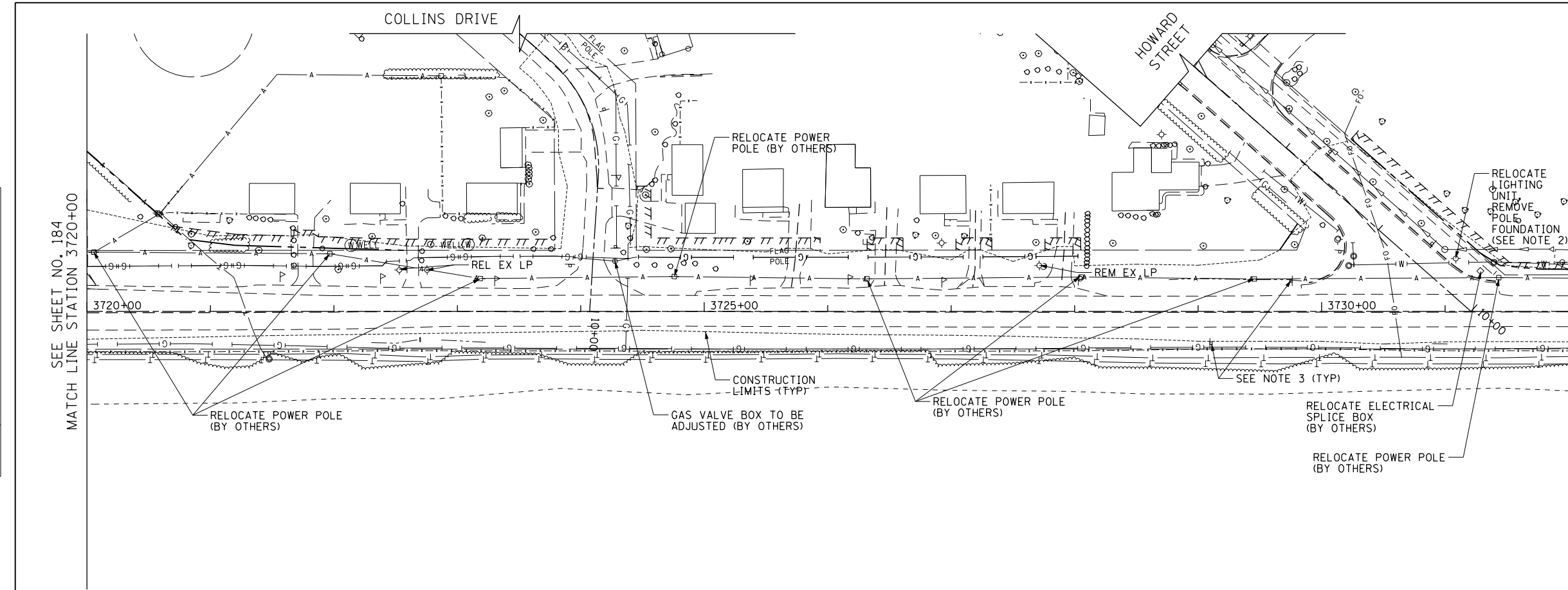
SCALE: 1" = 50' SHEET NAME: DUREM-03 STA. 3696+00 TO STA. 3720+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	184
CONTRACT NO. 60P95				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

FILE PATH = G:\CH2\0012\Road\Sheet\60P95-SHT-DRAIN-UTIL-REMOVAL-03.dwg

PLAN	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	BY	
	DATE	

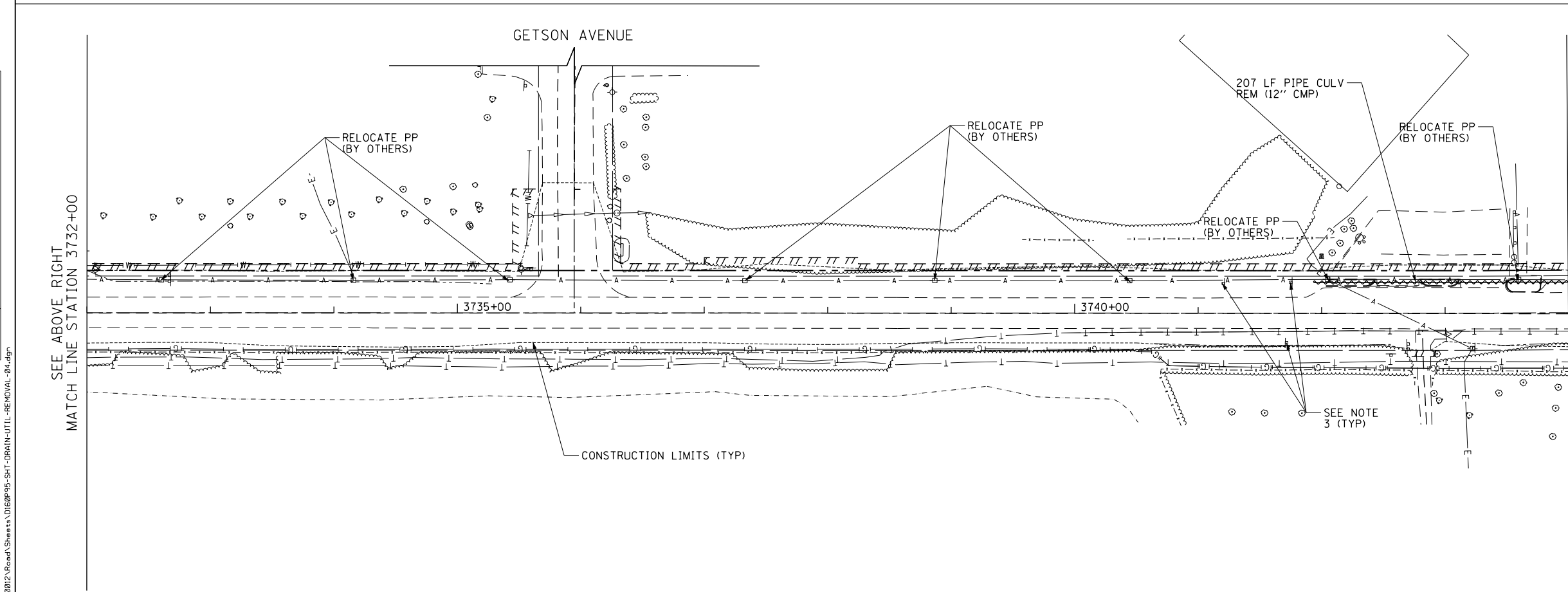
PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	BY	
	DATE	



LEGEND	
	STORM SEWER REMOVAL/CULVERT REMOVAL
	STRUCTURE TO BE ADJUSTED
	STRUCTURE TO BE REMOVED
	STRUCTURE TO BE RECONSTRUCTED
	ITEM TO BE RELOCATED

MATCH LINE STATION 3732+00  
SEE BELOW LEFT

SEE SHEET NO. 184  
MATCH LINE STATION 3720+00



- NOTES
1. FOR WATER MAIN/SANITARY SEWER ADJUSTMENT/REMOVAL ITEMS SEE SHEETS 226 TO 233 .
  2. SEE PLAN AND PROFILE SHEET 80 FOR RELOCATED LIGHT POLE LOCATION.
  3. FOR SIGNING REMOVALS AND RELOCATIONS SEE SHEETS 302 TO 322.

MATCH LINE STATION 3744+00  
SEE SHEET NO. 186

SEE ABOVE RIGHT  
MATCH LINE STATION 3732+00



USER NAME = bshaefliger	DESIGNED -	REVISED -
FILE = D:\60P95-SHT-DRAIN-UTIL-REMOVAL-04.dwg	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

U.S. ROUTE 30 DRAINAGE AND UTILITIES REMOVAL PLAN

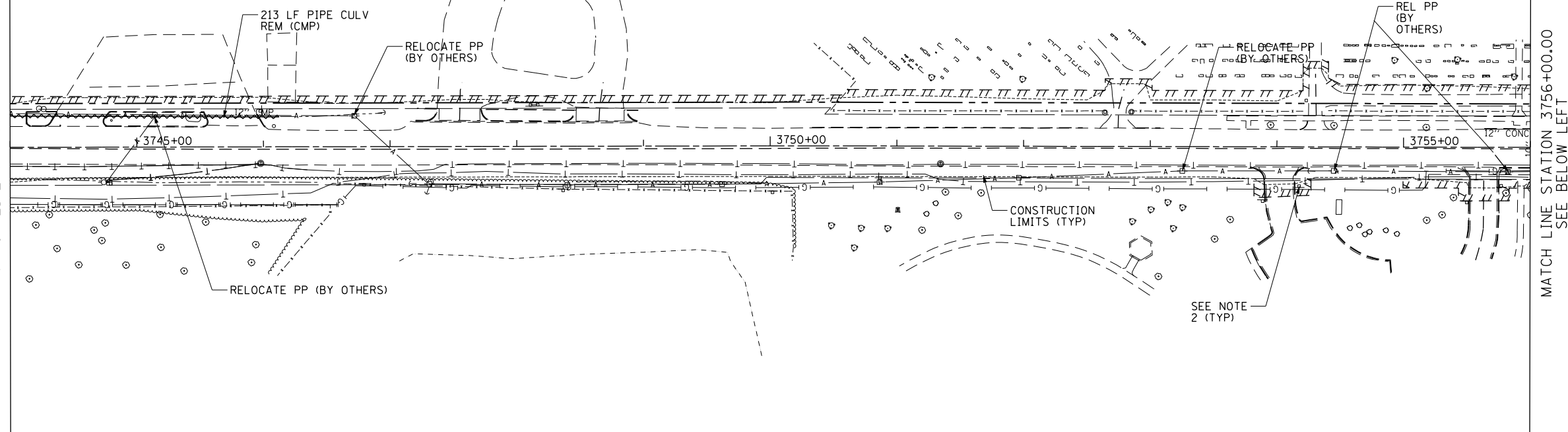
SCALE: 1" = 50' SHEET NAME: DUREM-04 STA. 3720+00 TO STA. 3744+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	185
				CONTRACT NO. 60P95
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

FILE PATH = G:\CH2\0012\Road\Sheet\60P95-SHT-DRAIN-UTIL-REMOVAL-04.dwg

PLAN	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	DATE FILE NAME	

SEE SHEET NO. 185  
MATCH LINE STATION 3744+00.00

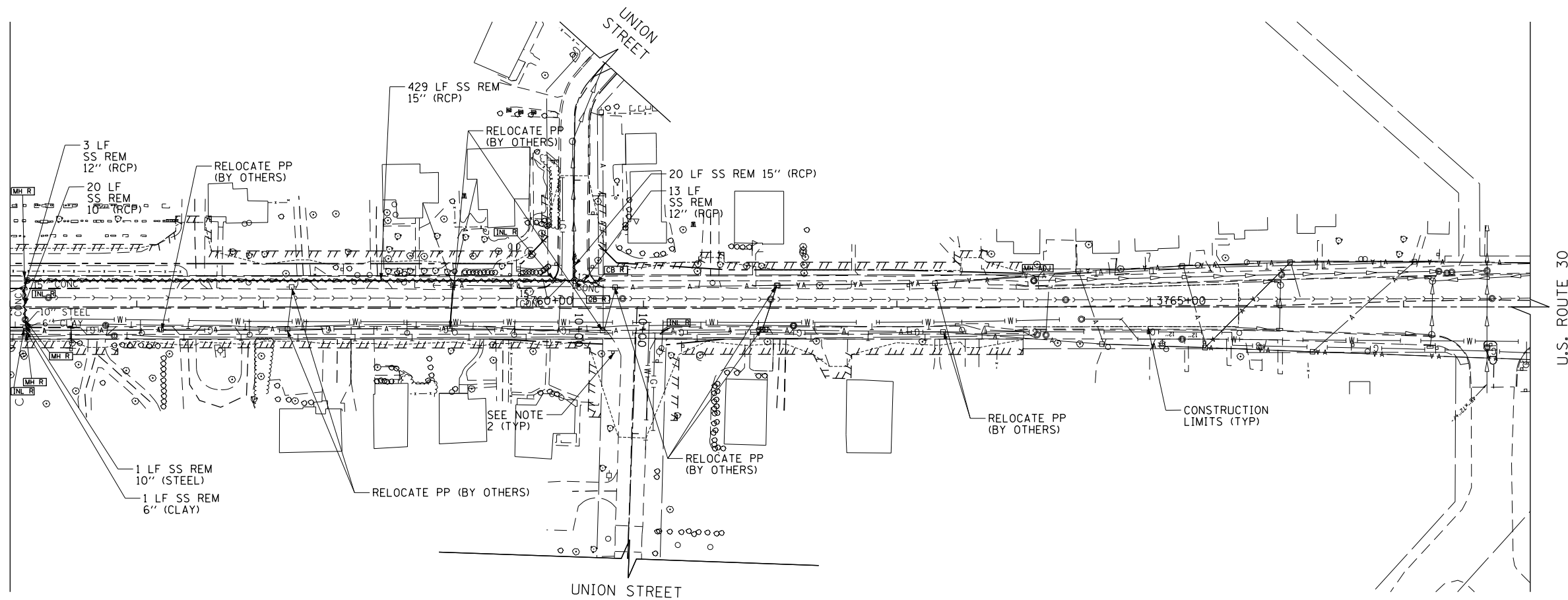


LEGEND	
	STORM SEWER REMOVAL/CULVERT REMOVAL
	STRUCTURE TO BE ADJUSTED
	STRUCTURE TO BE REMOVED
	STRUCTURE TO BE RECONSTRUCTED
	ITEM TO BE RELOCATED

MATCH LINE STATION 3756+00.00  
SEE BELOW LEFT

PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	DATE FILE NAME	

SEE ABOVE RIGHT  
MATCH LINE STATION 3756+00.00



- NOTES
1. FOR WATER MAIN/SANITARY SEWER ADJUSTMENT/REMOVAL ITEMS SEE SHEETS 226 TO 233.
  2. FOR SIGNING REMOVALS AND RELOCATIONS SEE SHEETS 302 TO 322.



USER NAME = bshoeffliger	DESIGNED -	REVISED -
FILE = D:\60P95-SHT-DRAIN-UTIL-REMOVAL-05.dwg	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/23/2014	DATE = 08/18/2014	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

U.S. ROUTE 30 DRAINAGE AND UTILITIES REMOVAL PLAN

SCALE: 1" = 50' SHEET NAME: DUREM-05 STA. 3744+00 TO STA. 3763+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	186
				CONTRACT NO. 60P95
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

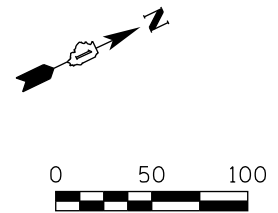
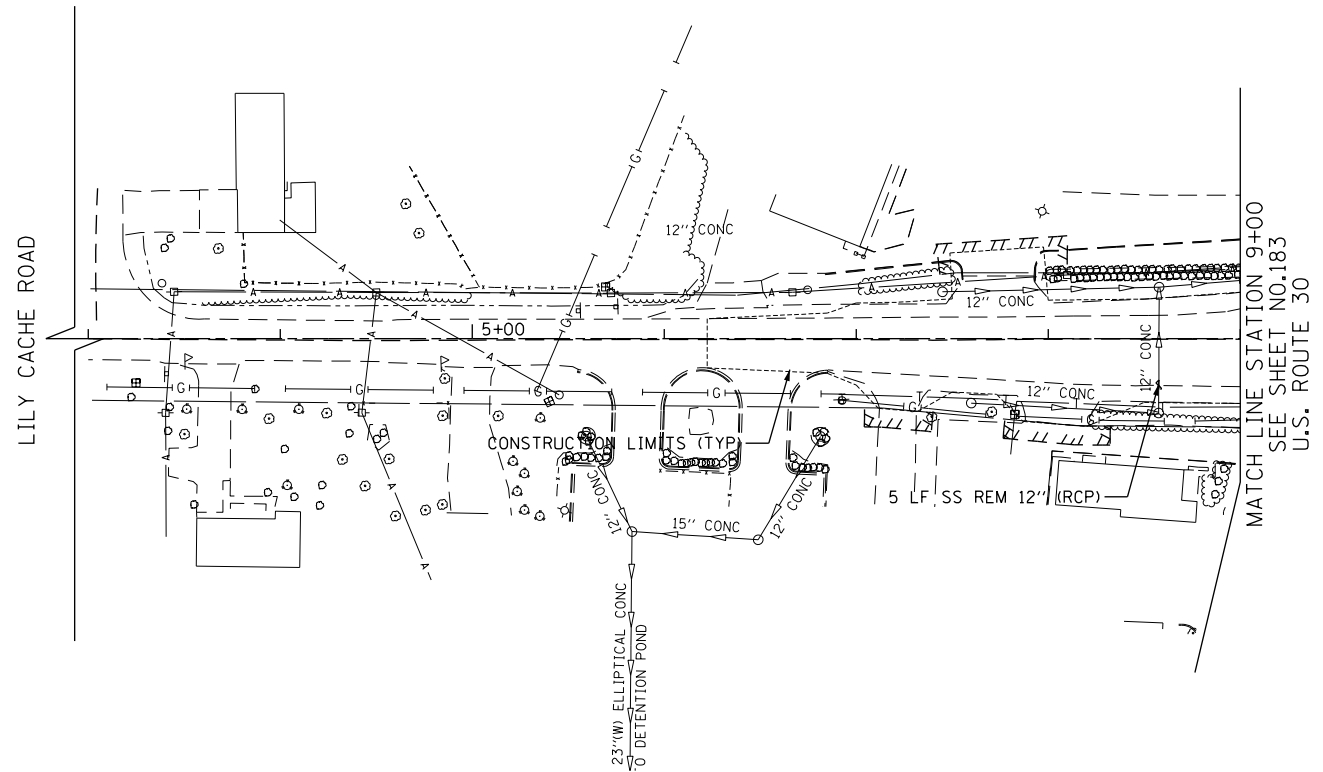
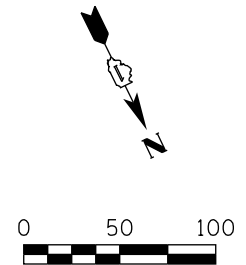
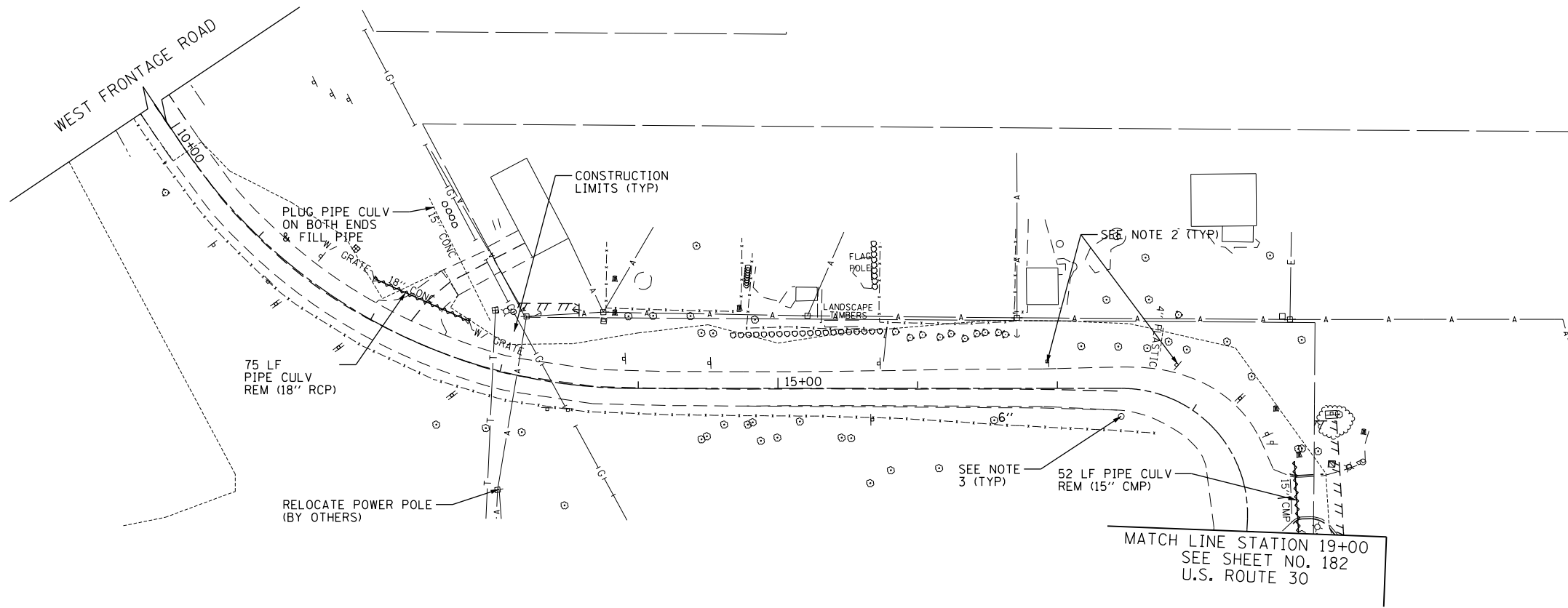
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	PLOTTED	
	GRADES CHECKED	
	NOTES CHECKED	
	BY	
	NO. _____	
	FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	NOTES CHECKED	
	BY	
	NO. _____	
	FILE NAME	

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LEGEND	
	STORM SEWER REMOVAL/CULVERT REMOVAL
	STRUCTURE TO BE ADJUSTED
	STRUCTURE TO BE REMOVED
	STRUCTURE TO BE RECONSTRUCTED
	ITEM TO BE RELOCATED



- NOTES:
1. FOR WATER MAIN/SANITARY SEWER ADJUSTMENT/REMOVAL ITEMS SEE SHEETS 226 TO 233
  2. FOR SIGNING REMOVALS AND RELOCATIONS SEE SHEETS 302 TO 322.
  3. FOR TRAFFIC SIGNAL ITEMS REMOVALS SEE SHEETS 340 TO 346.



USER NAME = bshoeffler	DESIGNED -	REVISED -
FILE = D160995-SHT-DRAIN-UTIL-REMOVAL-06.dgn	DRAWN -	REVISED -
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

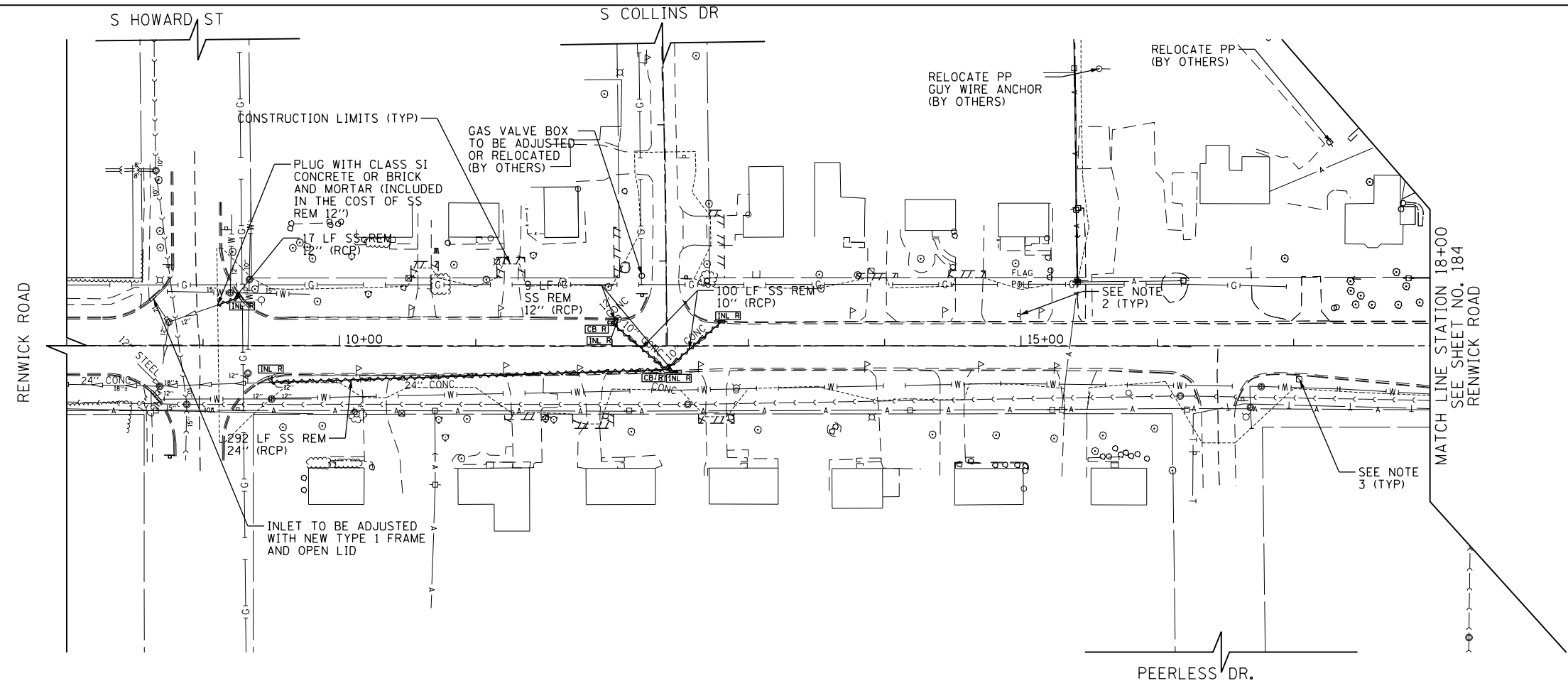
WEST FRONTAGE ROAD AND LILY CACHE ROAD DRAINAGE AND UTILITIES REMOVAL PLAN			
SCALE: 1" = 50'	SHEET NAME: DUREM-06	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	187
				CONTRACT NO. 60P95
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

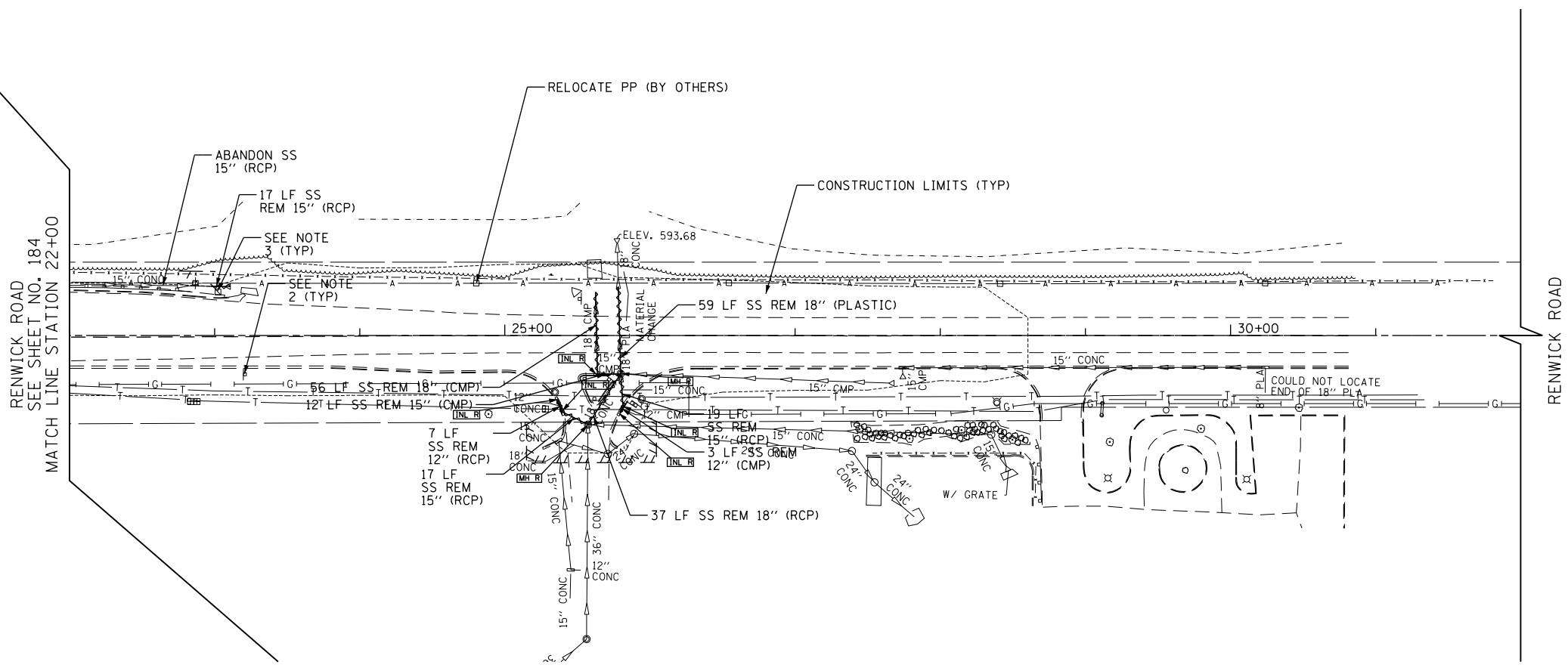
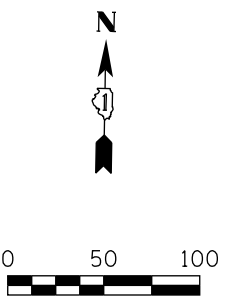
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	PLOTTED	
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	NOTE BOOK NO.	
	CADD FILE NAME	

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	PLOTTED	
	GRADES CHECKED	
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	NOTE BOOK NO.	
	CADD FILE NAME	

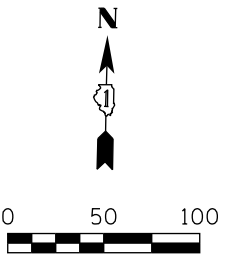
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LEGEND	
	STORM SEWER REMOVAL/CULVERT REMOVAL
	STRUCTURE TO BE ADJUSTED
	STRUCTURE TO BE REMOVED
	STRUCTURE TO BE RECONSTRUCTED
	ITEM TO BE RELOCATED



- NOTES:
- FOR WATER MAIN/SANITARY SEWER ADJUSTMENT/REMOVAL ITEMS SEE SHEETS 226 TO 233.
  - FOR SIGNING REMOVALS AND RELOCATIONS SEE SHEETS 302 TO 322.
  - FOR TRAFFIC SIGNAL ITEMS REMOVALS SEE SHEETS 340 TO 345.



USER NAME = bshoeffler	DESIGNED -	REVISED -
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PLOT DATE = 10/23/2014	DATE = 08/18/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**RENWICK ROAD DRAINAGE AND UTILITIES REMOVAL PLAN**

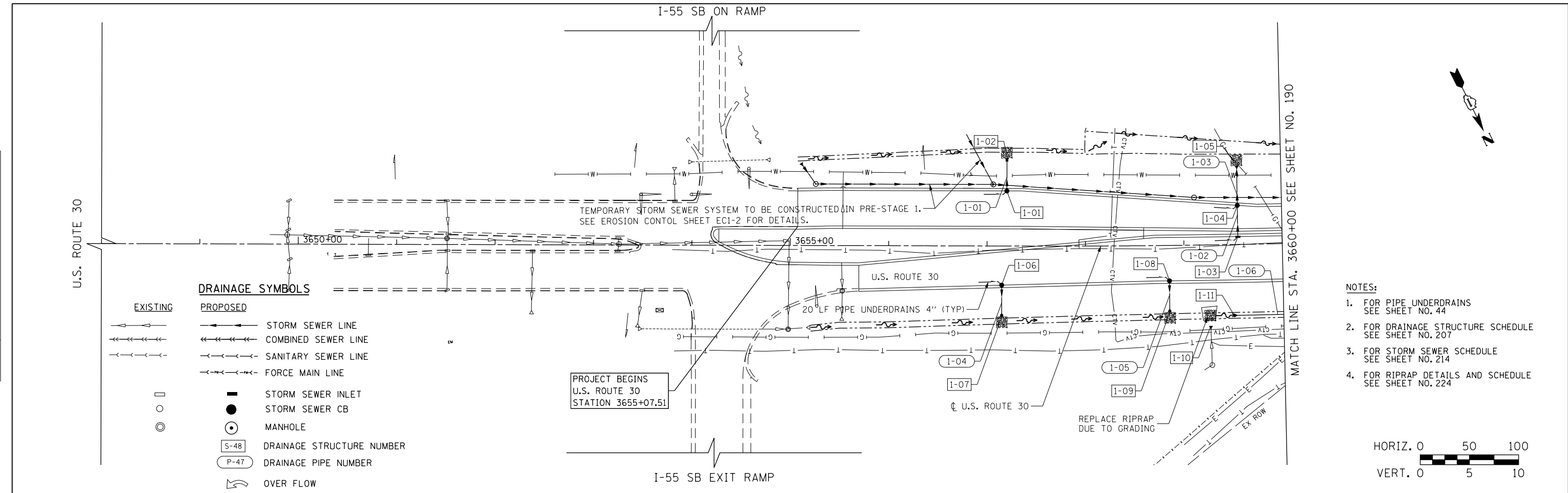
SCALE: 1" = 50' SHEET NAME: DUREM-07 STA. 8+00 TO STA. 32+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	188
				CONTRACT NO. 60P95
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

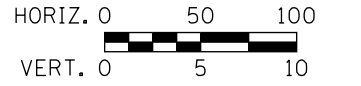


PLAN	SURVEYED	BY	DATE
	PLOTTED		
	NOTED		
	NO. OF REVISIONS		
	NO. OF CHECKS		
	NO. OF APPROVALS		
	NO. OF REVISIONS		
	NO. OF CHECKS		
	NO. OF APPROVALS		
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	NO. OF CHECKS		
	NO. OF APPROVALS		

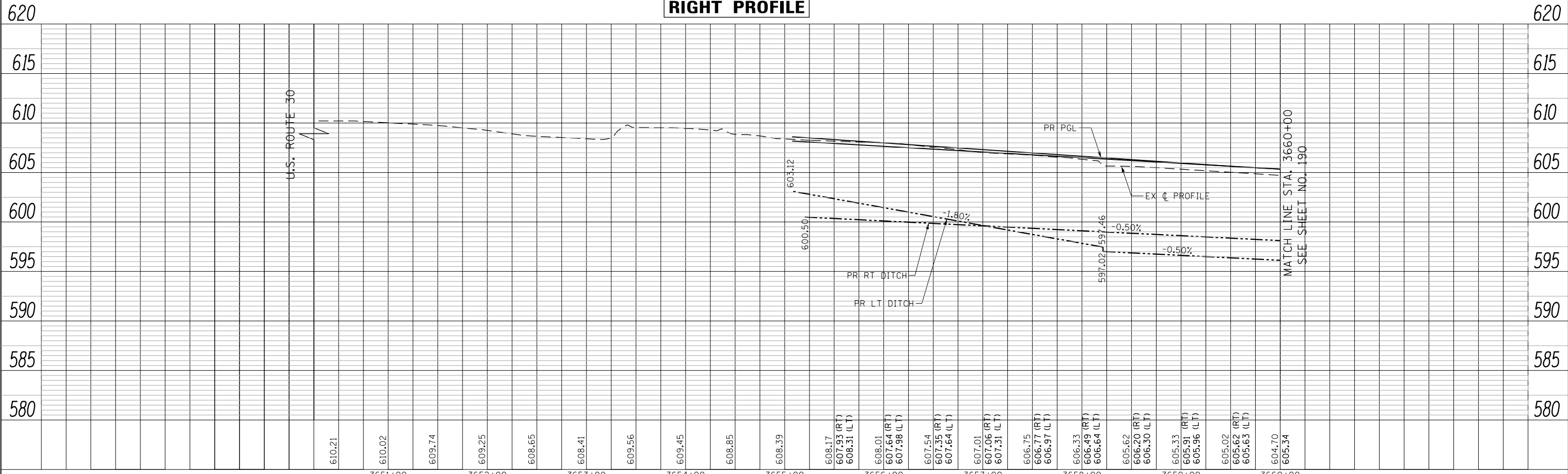
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	NO. OF REVISIONS		
	NO. OF CHECKS		
	NO. OF APPROVALS		
	NO. OF REVISIONS		
	NO. OF CHECKS		
	NO. OF APPROVALS		



- NOTES:**
1. FOR PIPE UNDERDRAINS SEE SHEET NO. 44
  2. FOR DRAINAGE STRUCTURE SCHEDULE SEE SHEET NO. 207
  3. FOR STORM SEWER SCHEDULE SEE SHEET NO. 214
  4. FOR RIPRAP DETAILS AND SCHEDULE SEE SHEET NO. 224



**RIGHT PROFILE**



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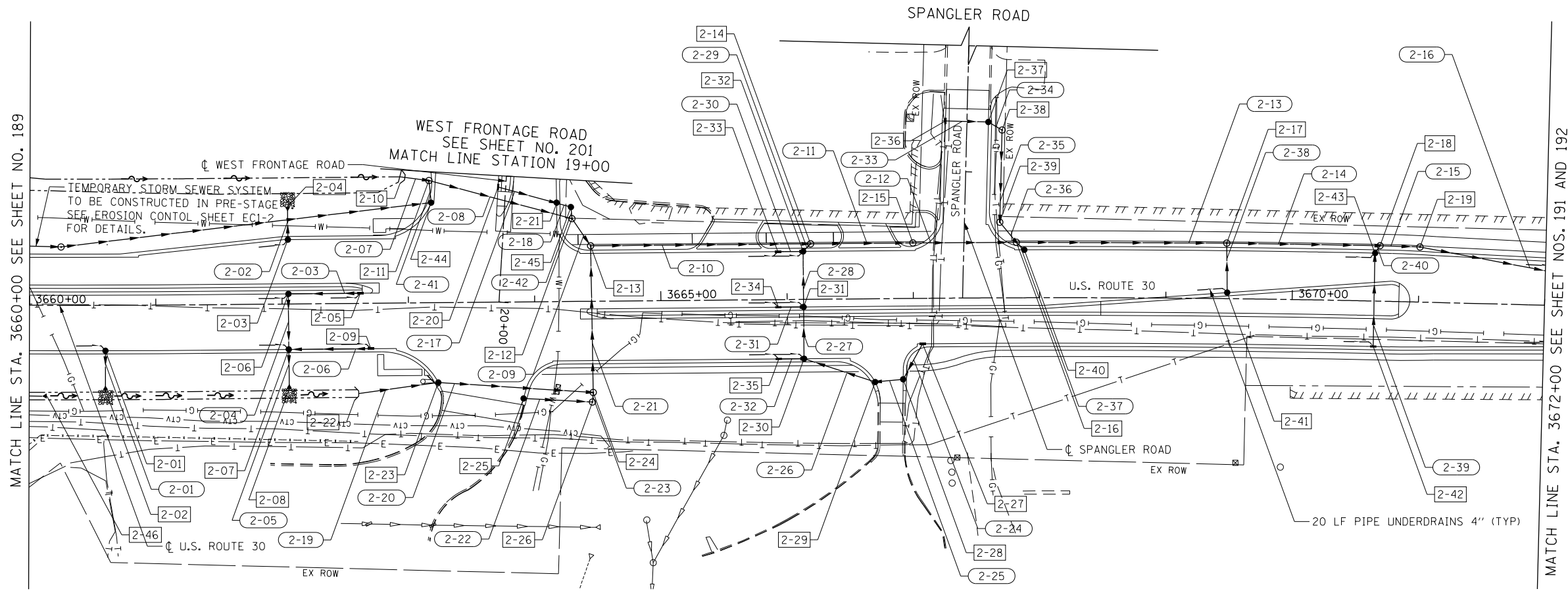
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PLOT DATE = 10/23/2014	DATE = 08/18/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

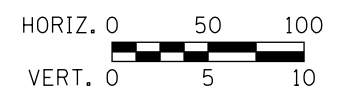
<b>DRAINAGE AND UTILITIES U.S. ROUTE 30</b>	
SCALE: HORIZ. 1"=50'	VERT. 1"=5'
SHEET NAME: DU-01	STA. 3648+00 TO STA. 3660+00

F.A.P. RTE. 575	SECTION 14W - R	COUNTY WILL	TOTAL SHEETS 681	SHEET NO. 189
CONTRACT NO. 60P95				FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT

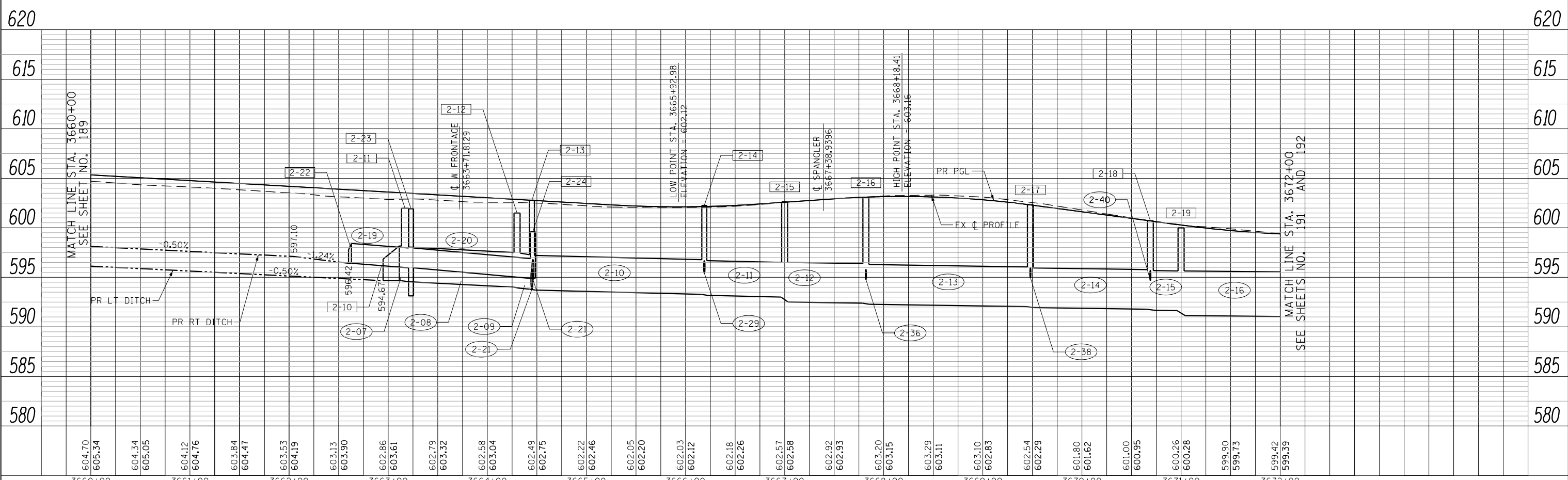
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	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	BY	
	DATE	



- NOTES:
- FOR PIPE UNDERDRAINS SEE SHEET NO. 44
  - FOR DRAINAGE STRUCTURE SCHEDULE SEE SHEET NO. 207
  - FOR STORM SEWER SCHEDULE SEE SHEET NO. 214
  - FOR RIPRAP DETAILS AND SCHEDULE SEE SHEET NO. 224
  - FOR LEGEND OF SYMBOLS SEE SHEET NO. 189



PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	BY	
	DATE	



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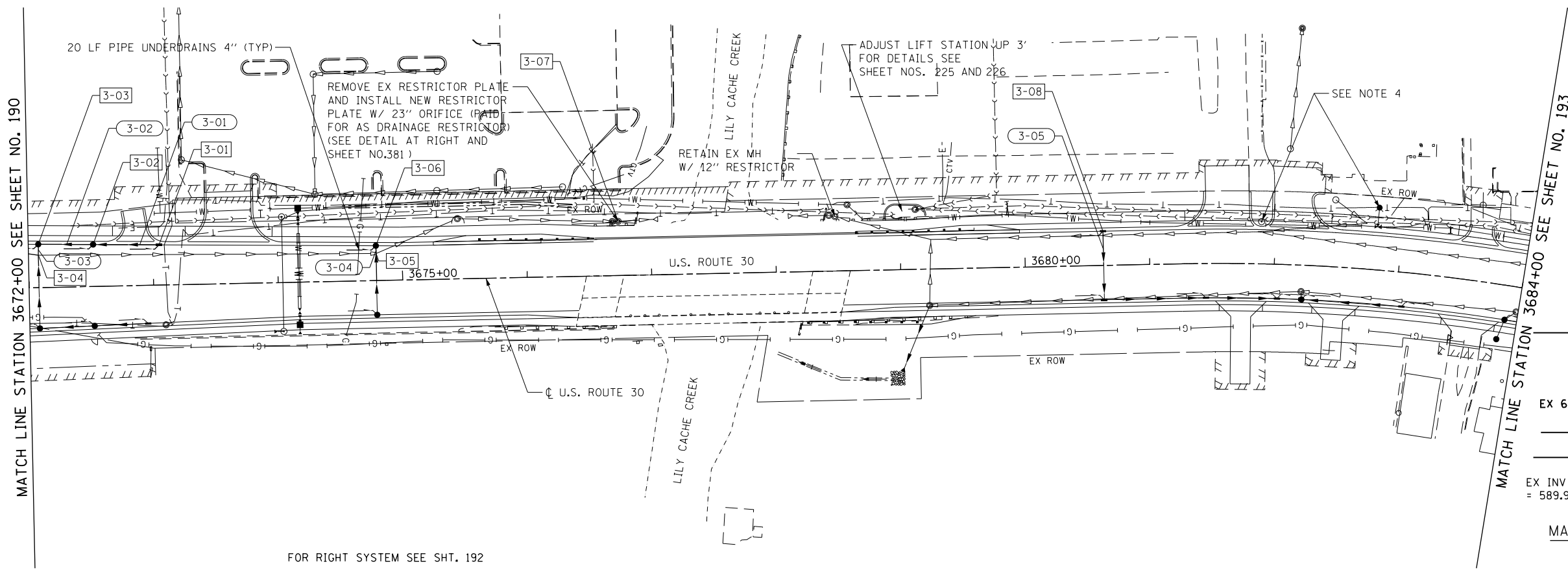
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PLOT DATE = 10/23/2014	DATE = 08/18/2014	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

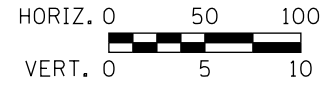
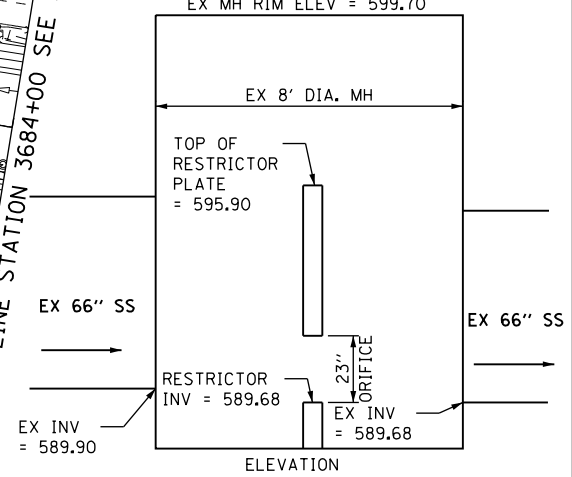
DRAINAGE AND UTILITIES U.S. ROUTE 30  
SCALE: HORIZ. 1"=50'  
VERT. 1"=5'  
SHEET NAME: DU-02  
STA. 3660+00 TO STA. 3672+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	190
				CONTRACT NO. 60P95
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	
	NOTED	
	BY	
	NO. OF REVISIONS	
	FILE NAME	

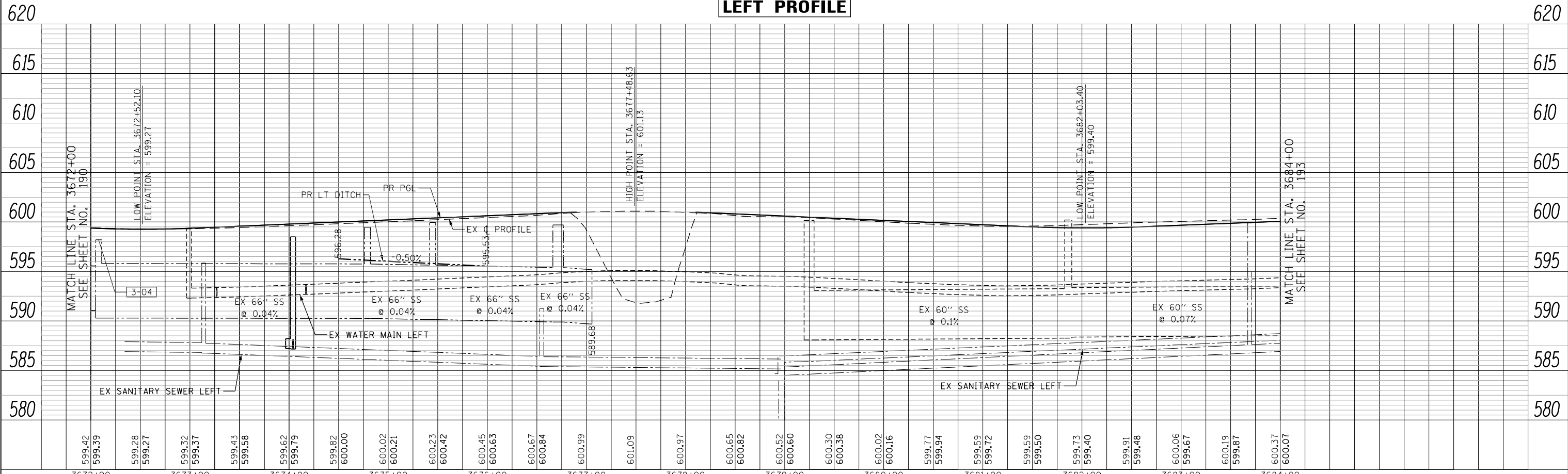


- NOTES:
- FOR PIPE UNDERDRAINS SEE SHEET NO. 44
  - FOR DRAINAGE STRUCTURE SCHEDULE SEE SHEET NO. 207
  - FOR STORM SEWER SCHEDULE SEE SHEET NO. 214
  - FOR WATER MAIN AND SANITARY SEWER/FORCE MAIN PLANS SEE SHEET NOS. 226 TO 233
  - FOR SANITARY LIFT STATION PLANS SEE SHEET NOS. 234 AND 235
  - FOR LEGEND OF SYMBOLS SEE SHEET NO. 189



FOR RIGHT SYSTEM SEE SHT. 192

**LEFT PROFILE**

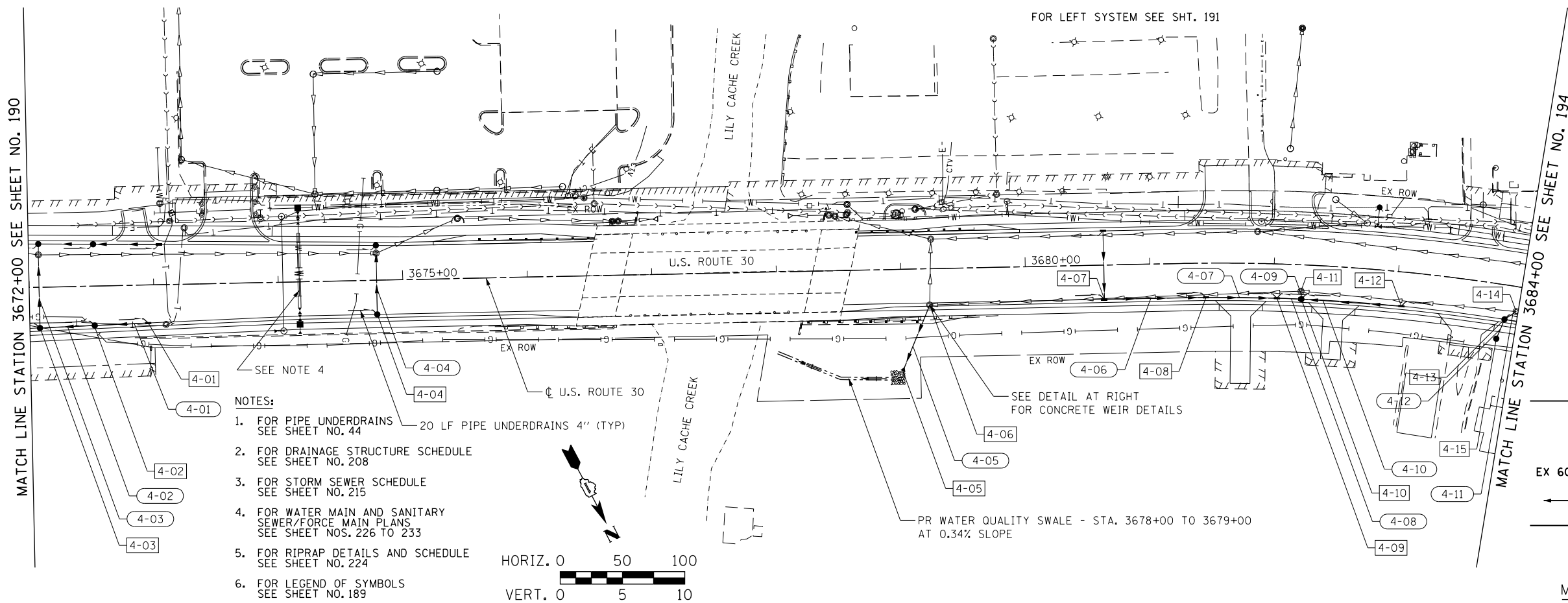


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	PLOTTED	
	NOTED	
	BY	
	NO. OF REVISIONS	
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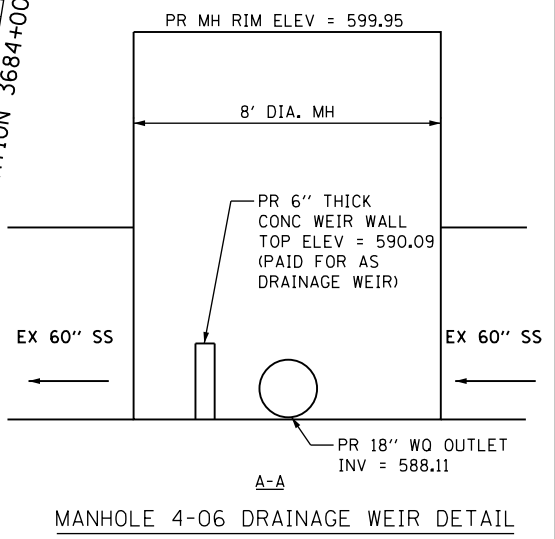
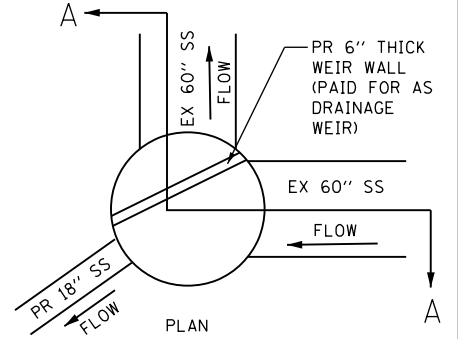
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	USER NAME = bshae1iger FILE = D:\60P95-SHT-DRAIN-UTIL-PP-03.dgn PLOT SCALE = 100.0000' / 1"	DESIGNED - DRAWN - CHECKED - DATE - 08/18/2014	REVISED - REVISED - REVISED - REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>DRAINAGE AND UTILITIES U.S. ROUTE 30 (LEFT SIDE)</b> SCALE: HORIZ. 1"=50' VERT. 1"=5' SHEET NAME: DU-03 STA. 3672+00 TO STA. 3684+00	F.A.P. RTE. 575 SECTION 14W - R COUNTY WILL TOTAL SHEETS 681 SHEET NO. 191 CONTRACT NO. 60P95 FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT
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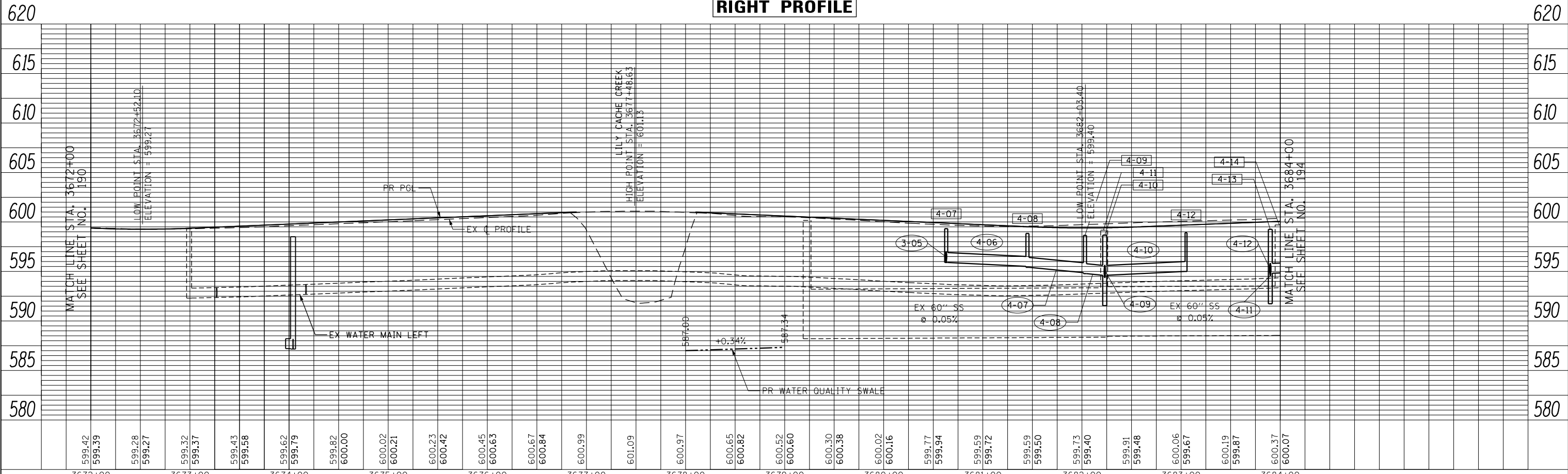
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	PLOTTED		
	GRADES CHECKED		
	STRUCTURE NOTATIONS CHECKED		
	NOTE BOOK NO.		
	CADD FILE NAME		



- NOTES:
- FOR PIPE UNDERDRAINS SEE SHEET NO. 44
  - FOR DRAINAGE STRUCTURE SCHEDULE SEE SHEET NO. 208
  - FOR STORM SEWER SCHEDULE SEE SHEET NO. 215
  - FOR WATER MAIN AND SANITARY SEWER/FORCE MAIN PLANS SEE SHEET NOS. 226 TO 233
  - FOR RIPRAP DETAILS AND SCHEDULE SEE SHEET NO. 224
  - FOR LEGEND OF SYMBOLS SEE SHEET NO. 189



**RIGHT PROFILE**



PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	STRUCTURE NOTATIONS CHECKED		
	NOTE BOOK NO.		
	CADD FILE NAME		



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PLOT DATE = 11/13/2014	DATE = 08/18/2014	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

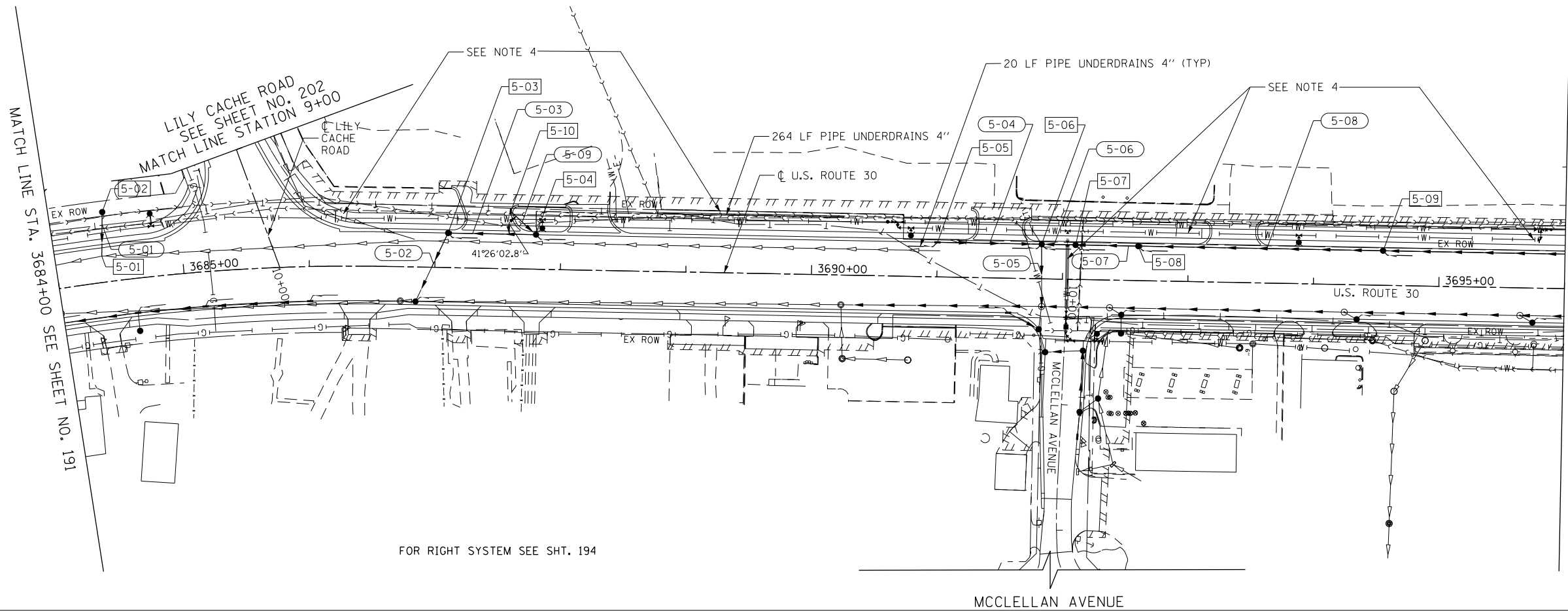
DRAINAGE AND UTILITIES U.S. ROUTE 30 (RIGHT SIDE)

SCALE: HORIZ. 1"=50' VERT. 1"=5'  
SHEET NAME: DU-04 STA. 3672+00 TO STA. 3684+00

F.A.P. RTE. 575	SECTION 14W - R	COUNTY WILL	TOTAL SHEETS 681	SHEET NO. 192
CONTRACT NO. 60P95				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

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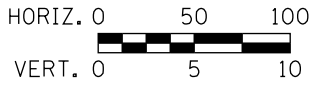
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	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	CADD FILE NAME	



MATCH LINE STA. 3684+00 SEE SHEET NO. 191

MATCH LINE STA. 3696+00 SEE SHEET NO. 195

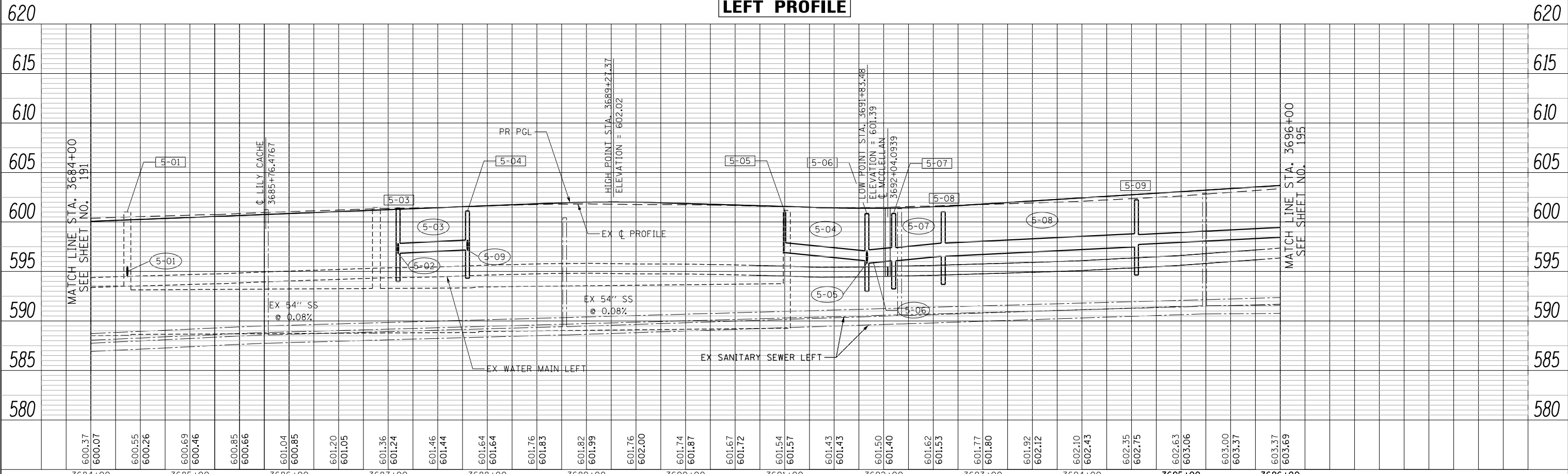
- NOTES:
1. FOR PIPE UNDERDRAINS SEE SHEET NO. 44
  2. FOR DRAINAGE STRUCTURE SCHEDULE SEE SHEET NO. 208
  3. FOR STORM SEWER SCHEDULE SEE SHEET NO. 215
  4. FOR WATER MAIN AND SANITARY SEWER/FORCE MAIN PLANS SEE SHEET NOS. 226 TO 233
  5. FOR LEGEND OF SYMBOLS SEE SHEET NO. 189



FOR RIGHT SYSTEM SEE SHT. 194

MCCLELLAN AVENUE

**LEFT PROFILE**



PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	CADD FILE NAME	

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PLOT DATE = 10/23/2014	DATE = 08/18/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DRAINAGE AND UTILITIES U.S. ROUTE 30 (LEFT SIDE)**

SCALE: HORIZ. 1"=50'  
VERT. 1"=5'

SHEET NAME: DU-05

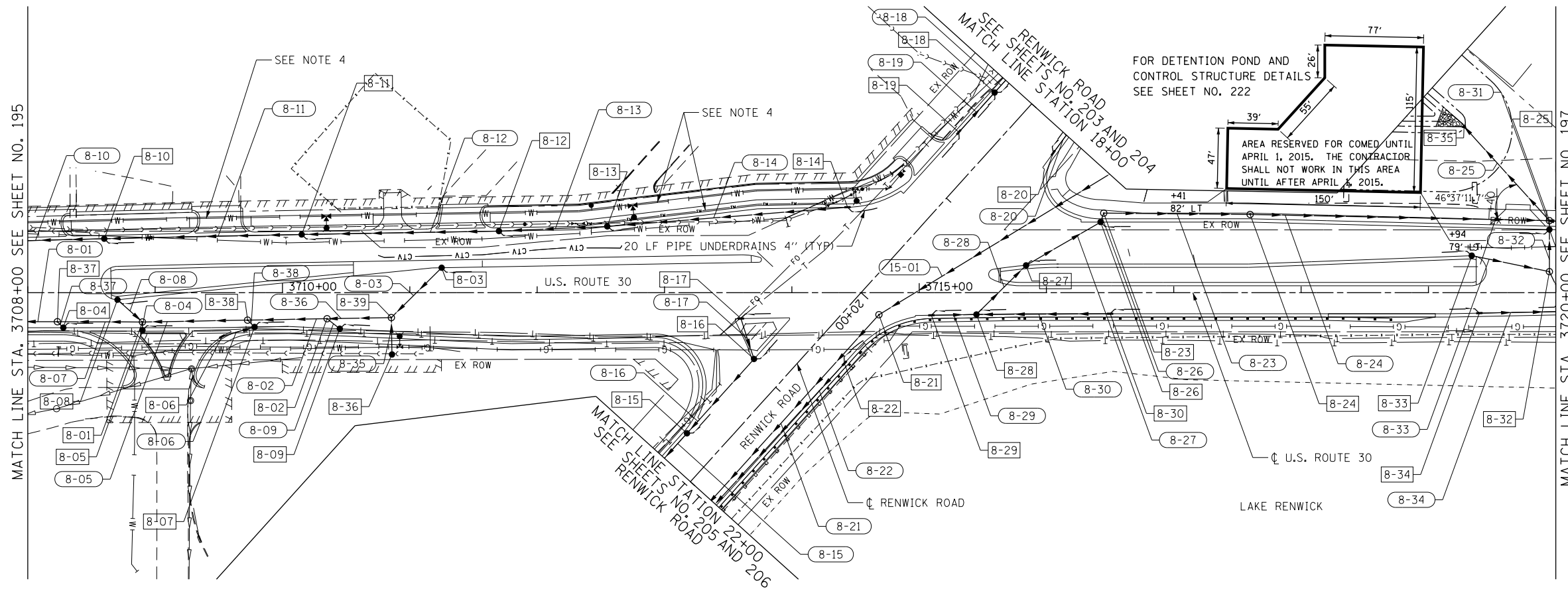
STA. 3684+00 TO STA. 3696+00

F.A.P. RTE. 575	SECTION 14W - R	COUNTY WILL	TOTAL SHEETS 681	SHEET NO. 193
CONTRACT NO. 60P95			FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT	



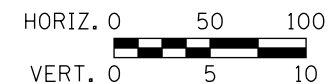


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	PLOTTED	
	NOTED	
	BY	
	NO. OF	
	FILE NAME	

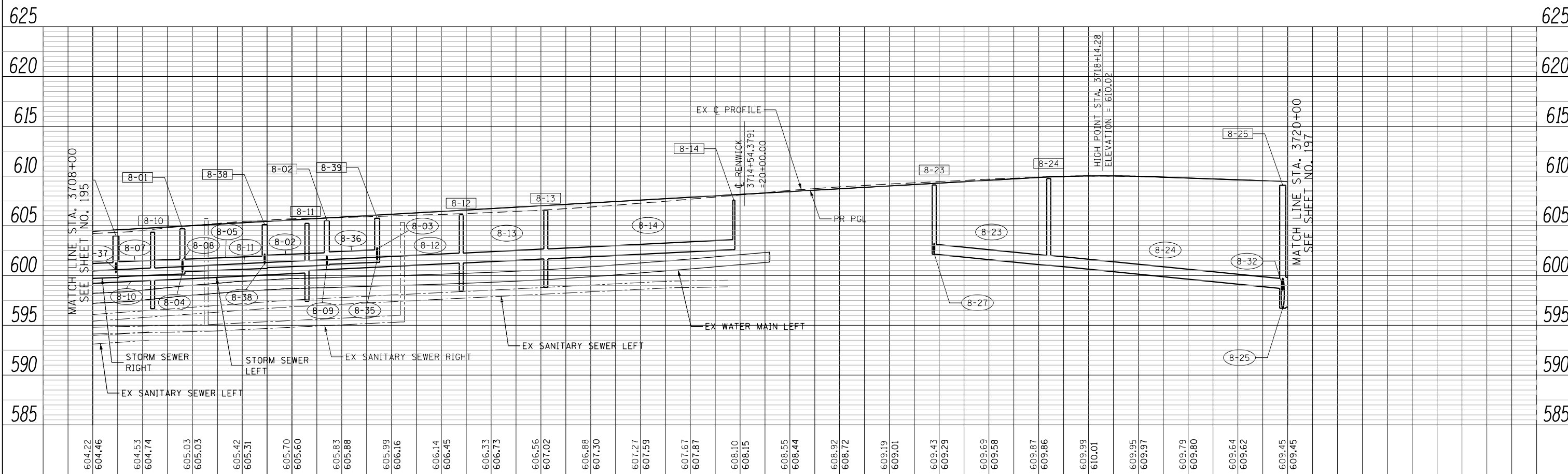


**NOTES:**

1. FOR PIPE UNDERDRAINS SEE SHEET NO.44
2. FOR DRAINAGE STRUCTURE SCHEDULE SEE SHEET NO. 209
3. FOR STORM SEWER SCHEDULE SEE SHEET NO. 216
4. FOR WATER MAIN AND SANITARY SEWER/FORCE MAIN PLANS SEE SHEET NOS. 226 TO 233
5. FOR RIPRAP DETAILS AND SCHEDULE SEE SHEET NO. 224
6. FOR LEGEND OF SYMBOLS SEE SHEET NO. 189



PROFILE	SURVEYED	DATE
	PLOTTED	
	NOTED	
	BY	
	NO. OF	
	FILE NAME	



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PLOT DATE = 10/23/2014	DATE = 08/18/2014	REVISED -

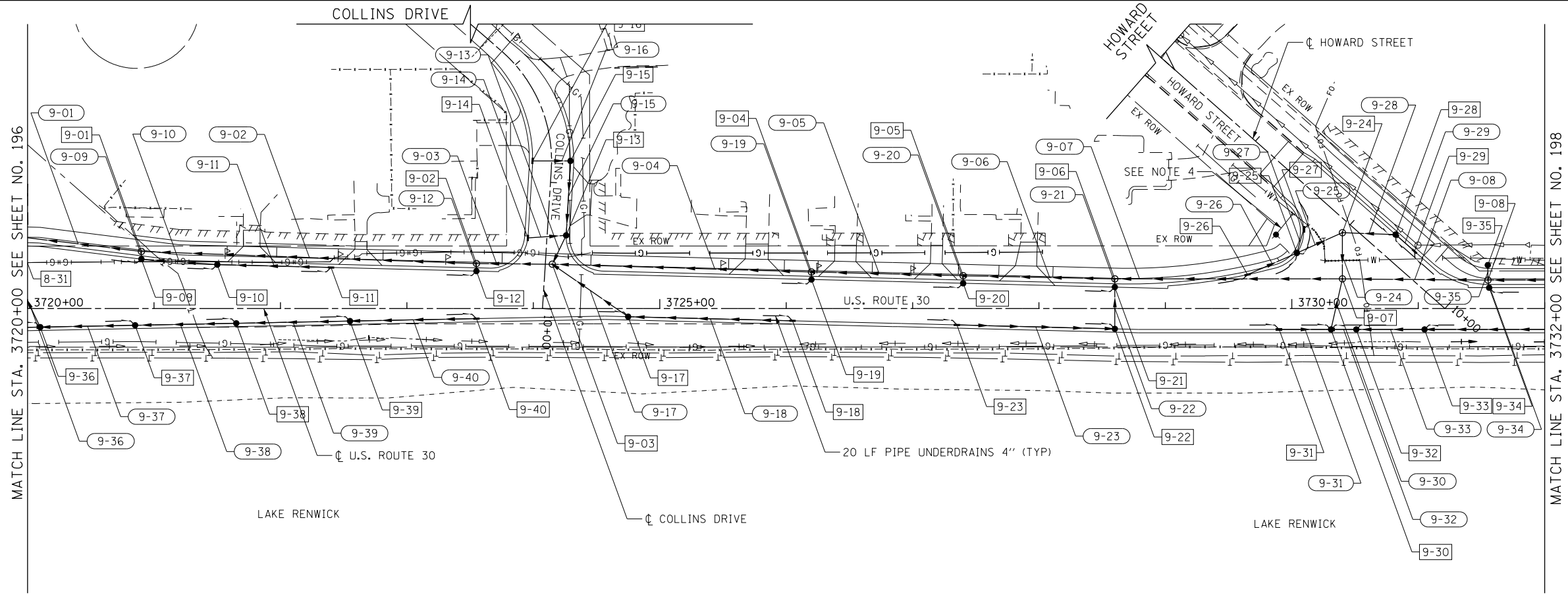
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DRAINAGE AND UTILITIES U.S. ROUTE 30</b>	
SCALE: HORIZ. 1"=50'	VERT. 1"=5'
SHEET NAME: DU-08	STA. 3708+00 TO STA. 3720+00

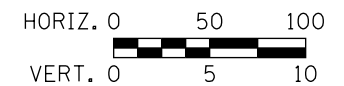
F.A.P. RTE. 575	SECTION 14W - R	COUNTY WILL	TOTAL SHEETS 681	SHEET NO. 196
CONTRACT NO. 60P95			FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT	



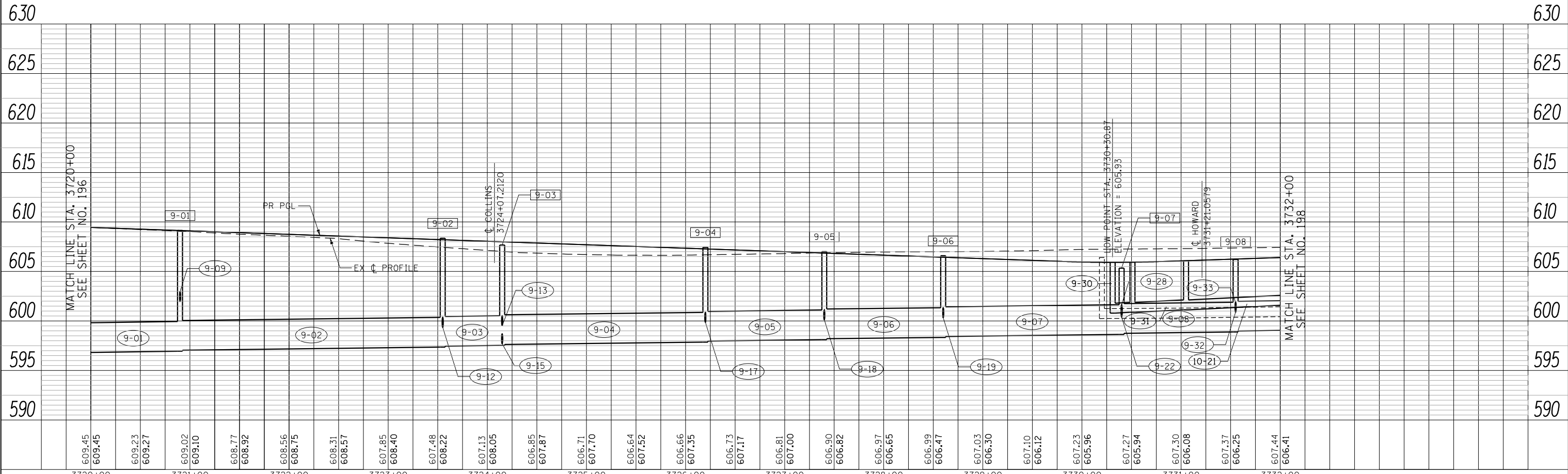
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	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	BY	
	DATE	



- NOTES:
1. FOR PIPE UNDERDRAINS SEE SHEET NO. 44
  2. FOR DRAINAGE STRUCTURE SCHEDULE SEE SHEET NO. 210
  3. FOR STORM SEWER SCHEDULE SEE SHEET NO. 217
  4. FOR WATER MAIN AND SANITARY SEWER/FORCE MAIN PLANS SEE SHEET NOS. 226 TO 233
  5. FOR LEGEND OF SYMBOLS SEE SHEET NO. 189



PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	BY	
	DATE	



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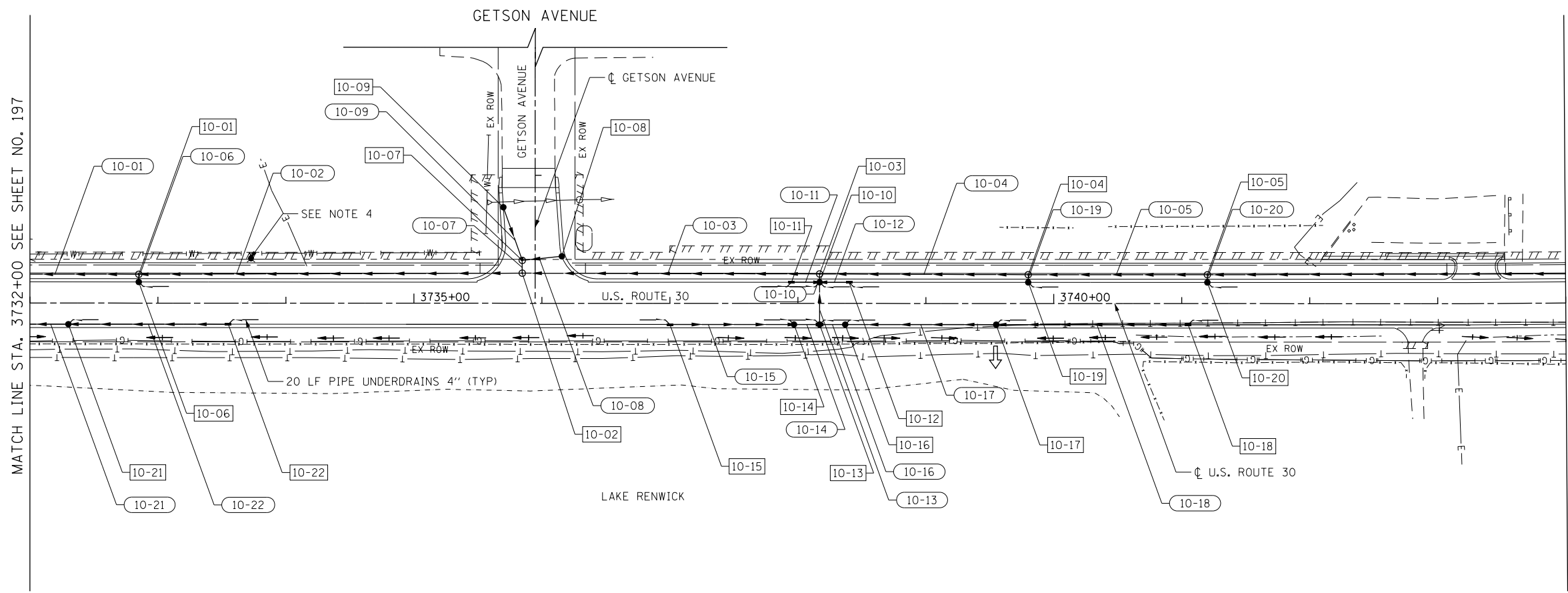
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PLOT DATE = 10/23/2014	DATE = 08/18/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

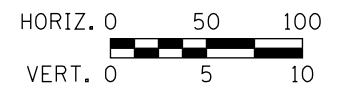
<b>DRAINAGE AND UTILITIES U.S. ROUTE 30</b>	
SCALE: HORIZ. 1"=50' VERT. 1"=5'	SHEET NAME: DU-09
STA. 3720+00	TO STA. 3732+00

F.A.P. RTE. 575	SECTION 14W - R	COUNTY WILL	TOTAL SHEETS 681	SHEET NO. 197
CONTRACT NO. 60P95				FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT

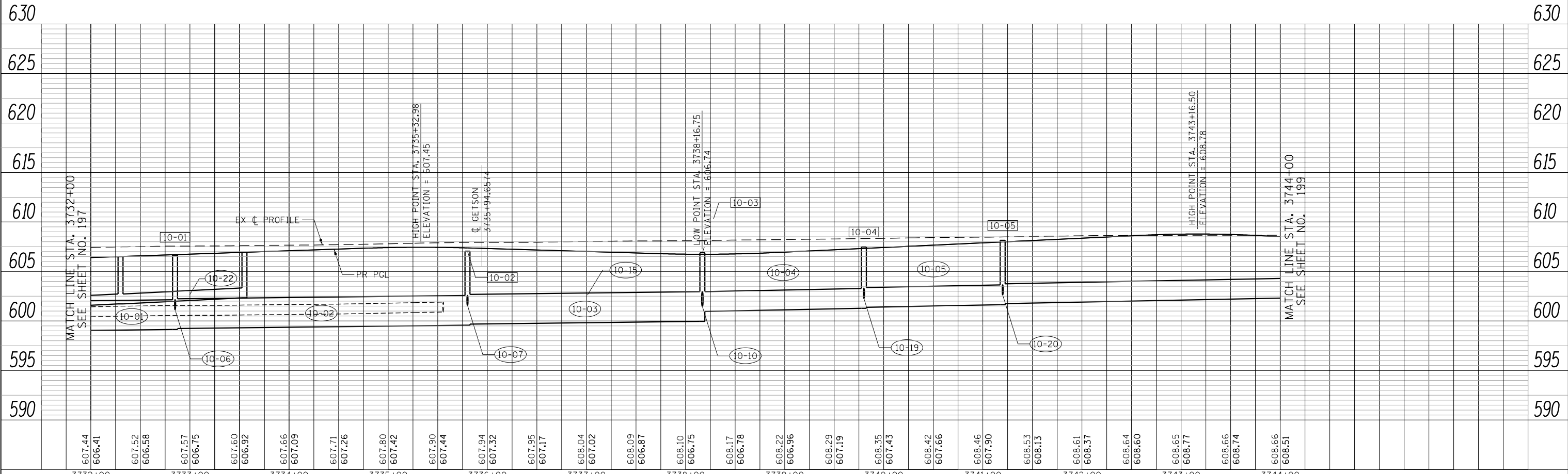
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	PLOTTED	
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	BY	
	NOTE BOOK	
	NO.	
	STRUCTURE	
	NOTATIONS	
	DATE	



- NOTES:
- FOR PIPE UNDERDRAINS SEE SHEET NO. 44
  - FOR DRAINAGE STRUCTURE SCHEDULE SEE SHEET NO. 210
  - FOR STORM SEWER SCHEDULE SEE SHEET NO. 217
  - FOR WATER MAIN AND SANITARY SEWER/FORCE MAIN PLANS SEE SHEET NOS. 226 TO 233
  - FOR LEGEND OF SYMBOLS SEE SHEET NO. 189



PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	BY	
	NOTE BOOK	
	NO.	
	STRUCTURE	
	NOTATIONS	
	DATE	



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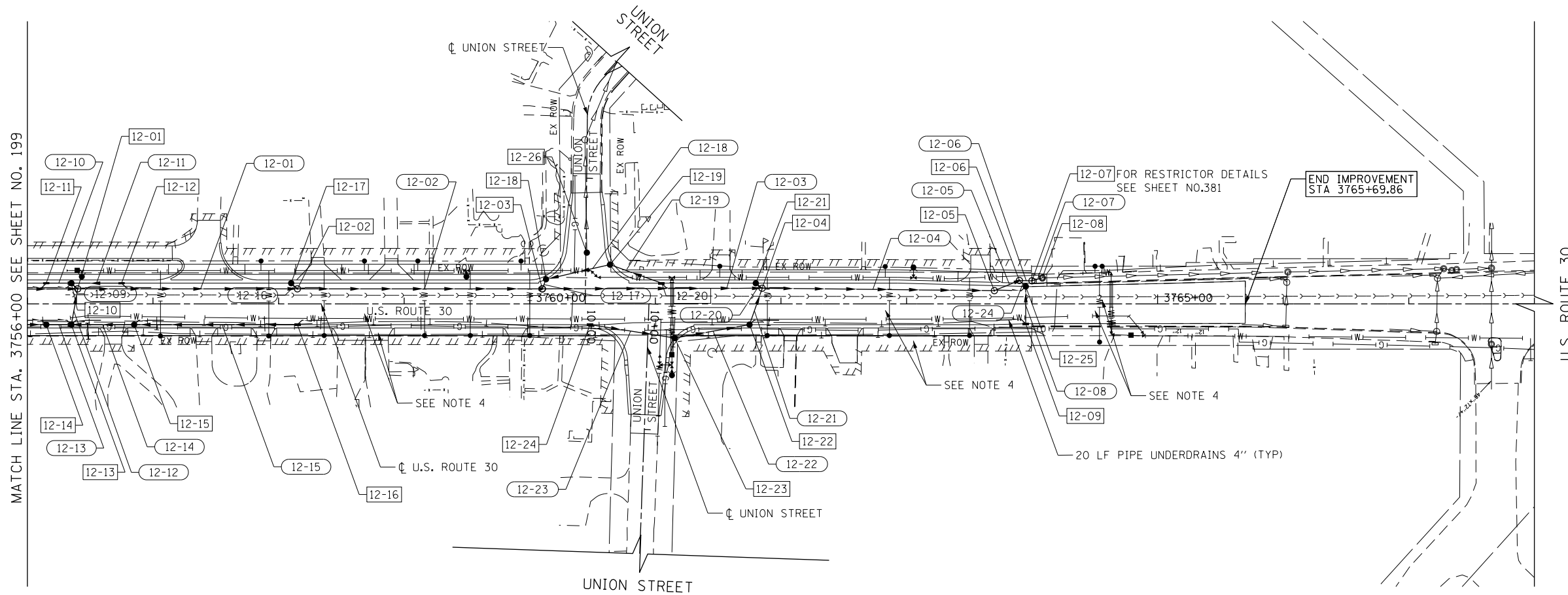
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DRAINAGE AND UTILITIES U.S. ROUTE 30	
SCALE: HORIZ. 1"=50'	VERT. 1"=5'
SHEET NAME: DU-10	STA. 3732+00 TO STA. 3744+00

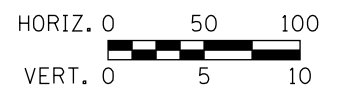
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CONTRACT NO. 60P95				FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT



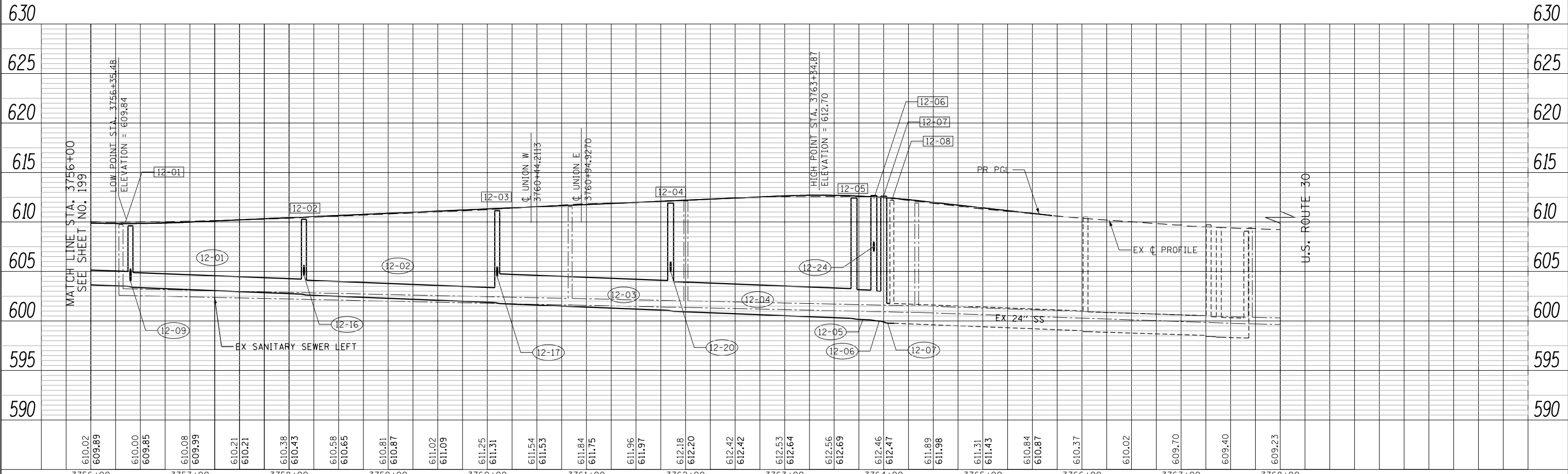
PLAN	SURVEYED	DATE
	PLOTTED	
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	BY	
	NO. OF	
	FILE NAME	



- NOTES:
1. FOR PIPE UNDERDRAINS SEE SHEET NO. 44
  2. FOR DRAINAGE STRUCTURE SCHEDULE SEE SHEET NO. 211
  3. FOR STORM SEWER SCHEDULE SEE SHEET NO. 218
  4. FOR WATER MAIN AND SANITARY SEWER/FORCE MAIN PLANS SEE SHEET NOS. 226 TO 233
  5. FOR LEGEND OF SYMBOLS SEE SHEET NO. 189



PROFILE	SURVEYED	DATE
	PLOTTED	
	NOTED	
	BY	
	NO. OF	
	FILE NAME	



FILE PATH = G:\CH2\0012\Road\Sheet\160995-SHT-DRAIN-UTIL-PP-12.dgn



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PLOT DATE = 10/23/2014	DATE - 08/18/2014	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DRAINAGE AND UTILITIES U.S. ROUTE 30  
SCALE: HORIZ. 1"=50'  
VERT. 1"=5'  
SHEET NAME: DU-12  
STA. 3756+00 TO STA. 3768+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
575	14W - R	WILL	681	200
CONTRACT NO. 60P95				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				