

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

F.A.U. 2887 EAST END AVE.
26th ST. TO 30th ST.

SECTION 3155-N(13)

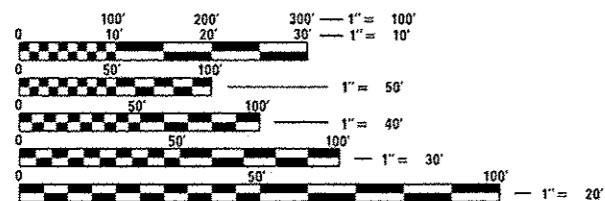
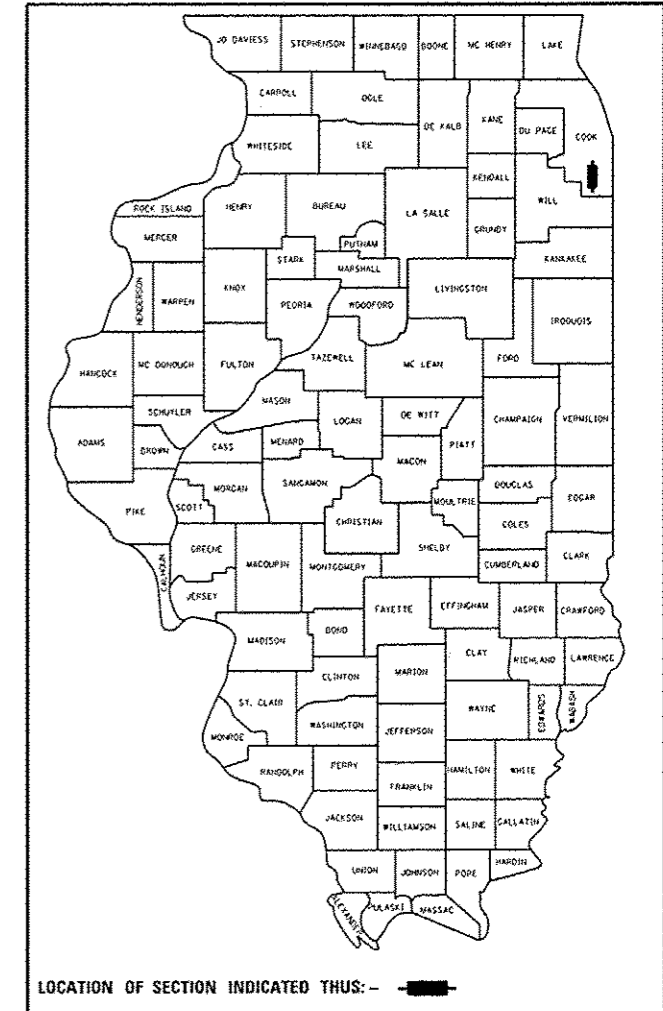
PROJECT: ACH5IP-2887(003)

RESURFACING, TRAFFIC SIGNAL MODERNIZATION
AND PAVEMENT MARKING
COOK COUNTY
C-91-375-13

| | | | | |
|---------------------|-----------------------|----------------------------|--------------------|----------------|
| F.A.U. RTE. 2887 | SECTION 3155-N(13) | COUNTY COOK ILLINOIS | TOTAL SHEETS 88 | SHEET NO. 1 |
| CONTRACT NO. 60W80 | | | | |

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT IS LOCATED IN THE CITY OF CHICAGO HEIGHTS AND THE VILLAGE OF SOUTH CHICAGO HEIGHTS

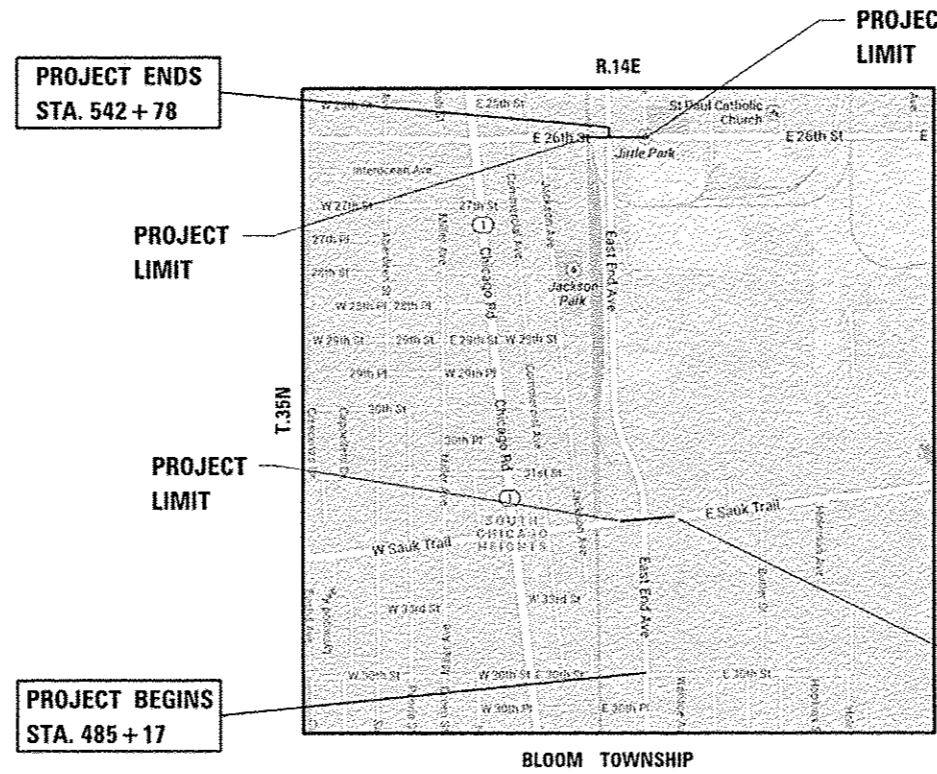


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER JENPAI CHANG 847-705-4432
PROJECT MANAGER KEN ENG

CONTRACT NO. 60W80



AVERAGE DAILY TRAFFIC
2010 = 4,000

POSTED SPEED LIMIT
45 MPH



GROSS LENGTH AND NET LENGTH = 5761 FT. = 1.09 MILE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED *DGIBBER 16, 2014*

[Signature]
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

Nov 14 2014
John D. Baranzoni PE
ENGINEER OF DESIGN AND ENVIRONMENT

Nov 14 2014
Omer Osman PE
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

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STANDARDS

| STD. NO. | DESCRIPTION |
|-------------|--------------------------------------------------------------------------|
| 000001-06 | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS |
| 424001 - 08 | PERPENDICULAR CURB RAMPS FOR SIDEWALKS |
| 442201 - 03 | CLASS C & D PATCHES |
| 606001 - 06 | CONCRETE CURB AND COMBINATION CONCRETE CURB AND GUTTER |
| 701301 - 04 | LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS. |
| 701306 - 03 | LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS, DAY ONLY FOR SPEED >45 MPH |
| 701311 - 03 | LANE CLOSURE, 2L, 2W, MOVING OPERATIONS, DAY ONLY |
| 701336 - 06 | LANE CLOSURE, 2L, 2W, WORK AREAS FOR SPEED >45 MPH |
| 701502 - 06 | URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT LANE |
| 701606 - 10 | URBAN LANE CLOSURE, MULTILANE 2W, WITH MOUNTABLE MEDIAN |
| 701701 - 09 | URBAN LANE CLOSURE, MULTILANE INTERSECTION |
| 701901 - 04 | TRAFFIC CONTROL DEVICES |
| 720001 - 01 | SIGN PANEL MOUNTING DETAIL |
| 814001 - 03 | HANDHOLES |
| 814006 - 02 | DOUBLE HANDHOLES |
| 857001 - 01 | STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES |
| 857006 - 01 | SUPERVISED RAILROAD INTERCONNECT CIRCUIT |
| 862001 - 01 | UNINTERRUPTIBLE POWER SUPPLY (UPS) |
| 873001 - 02 | TRAFFIC SIGNAL GROUNDING AND BONDING |
| 877001 - 05 | STEEL MAST ARM ASSEMBLY AND POLE 16' THROUGH 55' |
| 878001 - 10 | CONCRETE FOUNDATION DETAILS |
| 880001 - 01 | SPAN WIRE MOUNTED SIGNAL AND FLASHING BEACON INSTALLATION |
| 880006 - 01 | TRAFFIC SIGNAL MOUNTING DETAILS |
| 886001 - 01 | DETECTOR LOOP INSTALLATIONS |

PLAN NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. 48 HOUR NOTIFICATION IS REQUIRED.

TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND THE VILLAGE OF SOUTH CHICAGO HTS. AND THE CITY OF CHICAGO HTS.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

DRAINAGE STRUCTURE ADJUSTMENT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE ENGINEER SHALL CONTACT MS. PATRICE HARRIS, AREA TRAFFIC FIELD ENGINEER AT (708)-597-9800 A MINIMUM OF TWO WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM THE BUREAU OF CONSTRUCTION.

THE THICKNESS OF THE HMA MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA MIXTURE IS PLACED.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ADJUTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF PLATED STRUCTURES BY STATION AND OFFSET LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT.

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS- RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.

PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III AND ITS REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING.

WHEN THE MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 mm) WHERE THE SPEED LIMIT IS 40 MPH (60 Km/h) OR LESS AND 1 INCH (25 mm) WHERE THE SPEED LIMIT IS GREATER THAN 40 MPH (60 Km/h). WITH WRITTEN APPROVAL OF THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 mm) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

ACCESS TO ALL COMERCIAL & PRIVATE ENTRANCES WILL BE MAINTAINED AT ALL TIMES EXCEPT DURING THE SHORT DURATION WHEN CONSTRUCTION ACTIVITIES ARE UNDERWAY AT THIS ENTRANCE.

PLAN NOTES

THE CONTRACTOR WILL ASSUME RESPONSIBILITY FOR MAINTENANCE OF ALL SOIL EROSION CONTROL DURING CONSTRUCTION.

THE CONTRACTOR SHALL CHECK ALL ESC MEASURES WEEKLY AND AFTER EACH RAINFALL, 0.5 INCHES OR GREATER IN A 24 HOUR PERIOD, OR EQUIVALENT SNOWFALL. ADDITIONALLY DURING WINTER MONTHS, ALL MEASURES SHOULD BE CHECKED BY THE CONTRACTOR AFTER EACH SIGNIFICANT SNOWMELT.

ALL ESC MEASURES WILL BE MAINTAINED IN ACCORDANCE WITH THE IDOT EROSION AND SEDIMENT CONTROL FIELD GUIDE FOR CONSTRUCTION INSPECTION: ([HTTP://WWW.DOT.IL.GOV/DESENV/ENVIRONMENTAL/IDOTFIELDGUIDE.PDF](http://www.dot.il.gov/DESENV/ENVIRONMENTAL/IDOTFIELDGUIDE.PDF)) AND IDOT'S BEST MANAGEMENT PRACTICES - MAINTENANCE GUIDE: ([HTTP://WWW.DOT.STATE.IL.US/DESENV/ENVIRONMENTAL/BESTPRACTICES.HTML](http://www.dot.state.il.us/DESENV/ENVIRONMENTAL/BESTPRACTICES.HTML)).

THE CONTRACTOR SHOULD PROVIDE TO THE R.E. A PLAN TO ENSURE THAT A STABILIZED FLOW LINE WILL BE PROVIDED DURING STORM SEWER CONSTRUCTION. THIS IS IMPORTANT WHERE NEW STORM SEWER CONNECTS TO EXISTING CULVERTS. THE USE OF A STABILIZED FLOW LINE BETWEEN INSTALLED STORM SEWER AND OPEN DISTURBANCE WILL REDUCE THE POTENTIAL FOR THE OFFSITE DISCHARGE OF SEDIMENT-BEARING WATERS, ESPECIALLY WHEN RAIN IS FORECASTED, SO THAT FLOW WILL NOT ERODE. LACK OF APPROVED PLAN OR FAILURE TO COMPLY WILL RESULT IN AN ESC DEFICIENCY DEDUCTION.

ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER, SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED AS INCIDENTAL.

TEMPORARY OR PERMANENT STABILIZATION SHALL BE INITIATED IMMEDIATELY UPON COMPLETION OF DISTURBANCE OR IF THE WORK AREA IS TO BE LEFT UNDISTURBED FOR 14 DAYS OR MORE.

EROSION CONTROL ITEMS ARE CONSIDERED TO BE A HIGH PRIORITY ON THIS CONTRACT. THE CONTRACTOR IS RESPONSIBLE FOR INSTALLATION OF ANY ADDITIONAL EROSION CONTROL MEASURES NECESSARY TO PREVENT EROSION AND SEDIMENTATION AS DETERMINED BY THE R.E.

TREE REMOVAL PAY ITEMS SHALL BE USED TO REMOVE ANY DISEASED, DEAD, OR STRUCTURALLY UNSOUND TREES WITHIN THE RIGHT-OF-WAY AND TO CONTROL ANY EXISTING INVASIVE TREES THAT NEGATIVELY IMPACT THE NATIVE ECOSYSTEM ALONG THE ROADSIDE. THE ENGINEER WILL CONTACT THE ROADSIDE DEVELOPMENT UNIT AT 847-705-4471 AT LEAST 7 DAYS PRIOR TO TREE REMOVAL FOR MARKING ASSISTANCE.

ANY PROPOSED ACTIVITY IN THE VICINITY OF A HIGHWAY-RAIL GRADE CROSSING MUST ADHERE TO THE GUIDELINES SET FORTH IN THE MANUAL ON THE UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) UNDER SECTION 6G.18: WORK IN THE VICINITY A GRADE CROSSING WHICH STATES: "WHEN GRADE CROSSINGS EXIST EITHER WITHIN OR IN THE VICINITY OF A TTC ZONE, LANE RESTRICTIONS, FLAGGING, OR OTHER OPERATIONS SHALL NOT CREATE CONDITIONS WHERE VEHICLES CAN BE QUEUED ACROSS THE TRACKS. IF THE QUEUING OF VEHICLES ACROSS THE TRACKS CANNOT BE AVOIDED, A UNIFORMED LAW ENFORCEMENT OFFICER OR FLAGGER SHALL BE PROVIDED AT THE CROSSING TO PREVENT VEHICLES FROM STOPPING ON THE TRACKS, EVEN IF AUTOMATIC WARNING DEVICES ARE IN PLACE.

| | | | | | | | | | | | |
|-------------|------------------------------|------------|-----------|---------------------------------------------------|----------------------------------------------------------|-------------------|-----------------|--------------|---------------------------|-----------|--|
| FILE NAME = | USER NAME = driveaogn | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | EAST END AVE. (26TH ST. TO 30TH ST.) TYPICAL SECTIONS | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| Default | 1401-ahs-plandgn | DRAWN - | REVISED - | | | 2887 | 3155-N113 | COOK | 88 | 2 | |
| | PLOT SCALE = 100.0000' / IN. | CHECKED - | REVISED - | | | CONTRACT NO. 6W80 | | | | | |
| | PLOT DATE = 10/20/2014 | DATE - | REVISED - | | | SCALE: N/A | SHEET OF SHEETS | STA. TO STA. | ILLINOIS FED. AID PROJECT | | |

| SUMMARY OF QUANTITIES | | | URBAN 90% FED. 10% STATE TOTAL QUANTITIES | | CONSTRUCTION TYPE CODE | | | |
|-----------------------|-----------------------------------------------|--------|-------------------------------------------------------|--------------------------|------------------------|--|--|--|
| CODE NO | ITEM | UNIT | 0005 | TRAFFIC SIGNAL 0021 | | | | |
| | | | | EAST END AVE./SAUK TRAIL | EAST END AVE./26TH ST. | | | |
| 20100110 | TREE REMOVAL (6 TO 15 UNITS DIAMETER) | UNIT | 75 | 75 | | | | |
| 20100210 | TREE REMOVAL (OVER 15 UNITS DIAMETER) | UNIT | 120 | 120 | | | | |
| X0327900 | TREE REMOVAL (UNDER 6 INCH DIA., SAWED FLUSH) | IN DIA | 125 | 125 | | | | |
| 20101000 | TEMPORARY FENCE | FOOT | 330 | 330 | | | | |
| 20200500 | EARTH EXCAVATION (WIDENING) | CU YD | 354 | 354 | | | | |
| 20800150 | TRENCH BACKFILL | CU YD | 328 | 328 | | | | |
| 21101615 | TOPSOIL FURNISH AND PLACE, 4" | SO YD | 426 | 426 | | | | |
| 21400100 | GRADING AND SHAPING DITCHES | FOOT | 1670 | 1670 | | | | |
| 25000400 | NITROGEN FERTILIZER NUTRIENT | POUND | 5.4 | 5.4 | | | | |
| 25000500 | PHOSPHORUS FERTILIZER NUTRIENT | POUND | 5.4 | 5.4 | | | | |
| 25000600 | POTASSIUM FERTILIZER NUTRIENT | POUND | 5.4 | 5.4 | | | | |
| 25200110 | SODDING, SALT TOLERANT | SO YD | 426 | 426 | | | | |
| 28000400 | PERIMETER EROSION BARRIER | FOOT | 326 | 326 | | | | |
| 30300112 | AGGREGATE SUBGRADE IMPROVEMENT 12" | SO YD | 172 | 172 | | | | |
| 35600704 | HOT-MIX ASPHALT BASE COURSE WIDENING, 7" | SO YD | 293 | 293 | | | | |

| SUMMARY OF QUANTITIES | | | URBAN 90% FED. 10% STATE TOTAL QUANTITIES | | CONSTRUCTION TYPE CODE | | | |
|-----------------------|------------------------------------------------------------|-----------------|-------------------------------------------------------|--------------------------|------------------------|--|--|--|
| CODE NO | ITEM | UNIT | 0005 | TRAFFIC SIGNAL 0021 | | | | |
| | | | | EAST END AVE./SAUK TRAIL | EAST END AVE./26TH ST. | | | |
| 40600400 | MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS | TON | 42 | 42 | | | | |
| 40600827 | POLYMERIZED LEVELING BINDER (MACHINE METHOD), [L-4.75, N50 | TON | 1145 | 1145 | | | | |
| 40600895 | CONSTRUCTING TEST STRIP | EACH | 1 | 1 | | | | |
| 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT | SO YD | 211 | 211 | | | | |
| 40603335 | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 | TON | 21.5 | 21.5 | | | | |
| 40603340 | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 | TON | 2337 | 2337 | | | | |
| 42000500 | PORTLAND CEMENT CONCRETE PAVEMENT 10" | SO YD | 497 | 497 | | | | |
| 42001300 | PROTECTIVE COAT | SO YD | 903 | 903 | | | | |
| 44000100 | PAVEMENT REMOVAL | SO YD | 255 | 255 | | | | |
| 44000157 | HOT-MIX ASPHALT SURFACE REMOVAL, 2" | SO YD | 647 | 647 | | | | |
| 44000158 | HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4" | SO YD | 27821 | 27821 | | | | |
| 44000200 | DRIVEWAY PAVEMENT REMOVAL | SO YD | 191 | 191 | | | | |
| 44003100 | MEDIAN REMOVAL | SO FT | 2110 | 2110 | | | | |

13

Rev.

| | | | |
|-------------------------------------------------------|-----------------------|------------|-----------|
| FILE NAME = | USER NAME = drhvaasgn | DESIGNED - | REVISED - |
| C:\pe_work\101501\drhvaasgn\0271285\PI15431-4N-50.dgn | | DRAWN - | REVISED - |
| | | CHECKED - | REVISED - |
| | | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EAST END AVE. (26TH ST. - 30TH ST.)
SUMMARY OF QUANTITIES
SCALE: SHEET NO. OF SHEETS STA. TO STA.

| | | | | |
|-------------------------------------------------|-------------|--------|--------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2887 | 3155 N (13) | COOK | 88 | 3 |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 60W80 | |

| SUMMARY OF QUANTITIES | | | | CONSTRUCTION TYPE CODE | | | |
|-----------------------|-------------------------------------------------------------|-------|------------|--------------------------|------------------------|--|--|
| CODE NO | ITEM | UNIT | QUANTITIES | TRAFFIC SIGNAL 0021 | | | |
| | | | | EAST END AVE./SAUK TRAIL | EAST END AVE./26TH ST. | | |
| 44200970 | CLASS B PATCHES, TYPE II, 10 INCH | SQ YD | 12 | 12 | | | |
| 44201299 | DOWEL BARS 1 1/2" | EACH | 531 | 531 | | | |
| 44201777 | CLASS D PATCHES, TYPE II, 11 INCH | SQ YD | 1422 | 1422 | | | |
| 44201781 | CLASS D PATCHES, TYPE III, 11 INCH | SQ YD | 776 | 776 | | | |
| 44201783 | CLASS D PATCHES, TYPE IV, 11 INCH | SQ YD | 2490 | 2490 | | | |
| 48102100 | AGGREGATE WEDGE SHOULDER, TYPE B | TON | 4.5 | 4.5 | | | |
| 48203015 | HOT-MIX ASPHALT SHOULDERS, 4 1/2" | SQ YD | 581 | 581 | | | |
| 550A0340 | STORM SEWERS, CLASS A, TYPE 2 12" | FOOT | 98 | 98 | | | |
| 550A0360 | STORM SEWERS, CLASS A, TYPE 2 15" | FOOT | 261 | 261 | | | |
| 550A0380 | STORM SEWERS, CLASS A, TYPE 2 18" | FOOT | 62 | 62 | | | |
| 60201330 | CATCH BASINS, TYPE A, 4' -DIAMETER, TYPE 23 FRAME AND GRATE | EACH | 4 | 4 | | | |
| 60218400 | MANHOLES, TYPE A, 4' -DIAMETER, TYPE 1 FRAME, CLOSED LID | EACH | 1 | 1 | | | |
| 60221100 | MANHOLES, TYPE A, 5' -DIAMETER, TYPE 1 FRAME, CLOSED LID | EACH | 2 | 2 | | | |
| 60600605 | CONCRETE CURB, TYPE B | FOOT | 396 | 396 | | | |

| SUMMARY OF QUANTITIES | | | | CONSTRUCTION TYPE CODE | | | |
|-----------------------|---------------------------------------------------|--------|------------|--------------------------|------------------------|----|--|
| CODE NO | ITEM | UNIT | QUANTITIES | TRAFFIC SIGNAL 0021 | | | |
| | | | | EAST END AVE./SAUK TRAIL | EAST END AVE./26TH ST. | | |
| 60603800 | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 | FOOT | 210 | 210 | | | |
| 60605000 | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 | FOOT | 1058 | 1058 | | | |
| 60608582 | COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24 | FOOT | 1058 | 1058 | | | |
| 60618300 | CONCRETE MEDIAN SURFACE, 4 INCH | SQ FT | 1488 | 1488 | | | |
| 67000400 | ENGINEER'S FIELD OFFICE, TYPE A | CAL MO | 6 | 6 | | | |
| 67100100 | MOBILIZATION | L SUM | 1 | 1 | | | |
| 70300100 | SHORT TERM PAVEMENT MARKING | FOOT | 3556 | 3556 | | | |
| 70300210 | TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS | SQ FT | 758 | 758 | | | |
| 70300220 | TEMPORARY PAVEMENT MARKING - LINE 4" | FOOT | 30197 | 30197 | | | |
| 70300240 | TEMPORARY PAVEMENT MARKING - LINE 6" | FOOT | 852 | 852 | | | |
| 70300260 | TEMPORARY PAVEMENT MARKING - LINE 12" | FOOT | 213 | 213 | | | |
| 70300280 | TEMPORARY PAVEMENT MARKING - LINE 24" | FOOT | 237 | 237 | | | |
| 70301000 | WORK ZONE PAVEMENT MARKING REMOVAL | SQ FT | 187 | 187 | | | |
| * 72000100 | SIGN PANEL - TYPE 1 | SQ FT | 61 | 61 | 32 | 29 | |

*Specialty Items

REV. 12-11-14

90% FED.
7.5% STATE
2.5% CHICAGO
HEIGHTS

URBAN
90% FED.
10% STATE
TOTAL QUANTITIES

| SUMMARY OF QUANTITIES | | | URBAN | | CONSTRUCTION TYPE CODE | |
|-----------------------|------------------------------------------------------------|-------|----------------------------------------------|---------------------|------------------------|-----|
| CODE NO | ITEM | UNIT | 90% FED. 10% STATE TOTAL QUANTITIES | TRAFFIC SIGNAL 0021 | | |
| | | | | 0005 | | |
| * 72900200 | METAL POST - TYPE B | FOOT | 224 | 224 | | |
| * 78000100 | THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS | SQ FT | 758 | 758 | | |
| * 78000200 | THERMOPLASTIC PAVEMENT MARKING - LINE 4" | FOOT | 30197 | 30197 | | |
| * 78000400 | THERMOPLASTIC PAVEMENT MARKING - LINE 6" | FOOT | 852 | 852 | | |
| * 78000600 | THERMOPLASTIC PAVEMENT MARKING - LINE 12" | FOOT | 213 | 213 | | |
| * 78000650 | THERMOPLASTIC PAVEMENT MARKING - LINE 24" | FOOT | 237 | 237 | | |
| * 78008300 | POLYUREA PAVEMENT MARKING TYPE II - LETTERS AND SYMBOLS | SQ FT | 147 | 147 | | |
| * 78008310 | POLYUREA PAVEMENT MARKING TYPE II -LINE 4" | FOOT | 2737 | 2737 | | |
| * 78008330 | POLYUREA PAVEMENT MARKING TYPE II -LINE 6" | FOOT | 400 | 400 | | |
| * 78100100 | RAISED REFLECTIVE PAVEMENT MARKER | EACH | 216 | 216 | | |
| * 80500020 | SERVICE INSTALLATION - POLE MOUNTED | EACH | 2 | | 1 | 1 |
| * 81028200 | UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA. | FOOT | 1709 | | 725 | 984 |
| * 81028210 | UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA. | FOOT | 72 | | 59 | 13 |

| SUMMARY OF QUANTITIES | | | URBAN | | CONSTRUCTION TYPE CODE | |
|-----------------------|-----------------------------------------------------------|------|----------------------------------------------|---------------------|------------------------|------|
| CODE NO | ITEM | UNIT | 90% FED. 10% STATE TOTAL QUANTITIES | TRAFFIC SIGNAL 0021 | | |
| | | | | 0005 | | |
| * 81028220 | UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA. | FOOT | 181 | | 78 | 103 |
| * 81028240 | UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA. | FOOT | 682 | | 290 | 392 |
| * 81400100 | HANDHOLE | EACH | 6 | | 5 | 1 |
| * 81400200 | HEAVY-DUTY HANDHOLE | EACH | 13 | | 4 | 9 |
| * 81400300 | DOUBLE HANDHOLE | EACH | 3 | | 1 | 2 |
| * 85700200 | FULL-ACTUATED CONTROLLER AND TYPE IV CABINET | EACH | 1 | | 1 | |
| * 85700210 | RAILROAD, FULL-ACTUATED CONTROLLER AND TYPE IV CABINET | EACH | 1 | | | 1 |
| * 87301225 | ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C | FOOT | 1330 | | | 1330 |
| * 87301245 | ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C | FOOT | 2726 | | 1036 | 1690 |
| * 87301255 | ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C | FOOT | 2360 | | 1315 | 1045 |
| * 87301305 | ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR | FOOT | 3212 | | 1557 | 1655 |
| * 87301750 | ELECTRIC CABLE IN CONDUIT, RAILROAD, NO. 14 3C | FOOT | 539 | | | 539 |
| * 87301805 | ELECTRIC CABLE IN CONDUIT, SERVICE, NO.6 2 C | FOOT | 115 | | 39 | 76 |

| | | | |
|--------------------------|-----------------------|------------|-----------|
| FILE NAME = | USER NAME = drkotasgn | DESIGNED - | REVISED - |
| PLT SCALE = 1000000 / 1" | CHECKED - | DRAWN - | REVISED - |
| PLT DATE = 10/20/2014 | DATE - | | |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EAST END AVE. (26TH ST. - 30TH ST.)
SUMMARY OF QUANTITIES**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

| | | | | |
|---------------------------------------------------|---------------------|-------------|-----------------|-------------|
| F.A.U. RTE. 2887 | SECTION 3155 N (13) | COUNTY COOK | TOTAL SHEETS 88 | SHEET NO. 5 |
| FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT | | | | |

* SPECIALTY ITEM
* SUPER P CABINET
Rev.

90% FED.
7.5% STATE
2.5% CHICAGO
HEAVEN

URBAN
90% FED.
10% STATE

90% FED.
7.5% STATE
2.5% CHICAGO
HEAVEN

| SUMMARY OF QUANTITIES | | | URBAN 90% FED. 10% STATE TOTAL QUANTITIES | CONSTRUCTION TYPE CODE | |
|-----------------------|----------------------------------------------------------|------|-------------------------------------------------------|---------------------------------|-------------------------------|
| CODE NO | ITEM | UNIT | | EAST END AVE./ SAUK TRAIL | EAST END AVE./ 26TH ST. |
| 87301900 | ELECTRIC CABLE IN CONDUIT, EQUIPMENT | FOOT | 1556 | 599 | 957 |
| | GROUNDING CONDUCTOR, NO. 6 IC | | | | |
| 87502500 | TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT. | EACH | 5 | 4 | 1 |
| 87502520 | TRAFFIC SIGNAL POST, GALVANIZED STEEL 18 FT. | EACH | 1 | | 1 |
| 87700150 | STEEL MAST ARM ASSEMBLY AND POLE, 22 FT. | EACH | 1 | | 1 |
| 87700180 | STEEL MAST ARM ASSEMBLY AND POLE, 28 FT. | EACH | 1 | 1 | |
| 87700190 | STEEL MAST ARM ASSEMBLY AND POLE, 30 FT. | EACH | 2 | 1 | 1 |
| 87700200 | STEEL MAST ARM ASSEMBLY AND POLE, 32 FT. | EACH | 1 | 1 | |
| 87700240 | STEEL MAST ARM ASSEMBLY AND POLE, 40 FT. | EACH | 2 | 1 | 1 |
| 87700290 | STEEL MAST ARM ASSEMBLY AND POLE, 50 FT. | EACH | 1 | 1 | |
| 87800100 | CONCRETE FOUNDATION, TYPE A | FOOT | 24 | 16 | 8 |
| 87800150 | CONCRETE FOUNDATION, TYPE C | FOOT | 8 | 4 | 4 |
| 87800400 | CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER | FOOT | 61 | 37 | 24 |
| 87800415 | CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER | FOOT | 41 | 13 | 28 |
| 88030020 | SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED | EACH | 11 | 6 | 5 |

| SUMMARY OF QUANTITIES | | | URBAN 90% FED. 10% STATE TOTAL QUANTITIES | CONSTRUCTION TYPE CODE | |
|-----------------------|-----------------------------------------------------------------------------------|-----------------|-------------------------------------------------------|---------------------------------|-------------------------------|
| CODE NO | ITEM | UNIT | | EAST END AVE./ SAUK TRAIL | EAST END AVE./ 26TH ST. |
| 88030100 | SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED | EACH | 11 | 6 | 5 |
| 88030110 | SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED | EACH | 7 | 4 | 3 |
| 88200210 | TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM | EACH | 18 | 10 | 8 |
| 88500100 | INDUCTIVE LOOP DETECTOR | EACH | 15 | 8 | 7 |
| 88600100 | LIGHT DETECTOR | EACH | 2 | 2 | 2 |
| 88600100 | DETECTOR LOOP, TYPE I | FOOT | 1566 | 871 | 695 |
| 88700500 | LIGHT DETECTOR AMPLIFIER | EACH | 1 | 1 | 2 |
| 89000100 | TEMPORARY TRAFFIC SIGNAL INSTALLATION | EACH | 2 | 1 | 1 |
| 89100400 | ILLUMINATED SIGN, LED | EACH | 4 | | 4 |
| 89501400 | RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, DETECTOR UNIT | EACH | 2 | 2 | |
| 89501410 | RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, PHASING UNIT | EACH | 1 | 1 | |
| 89502375 | REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT | EACH | 2 | 1 | 1 |
| 89502380 | REMOVE EXISTING HANDHOLE | EACH | 21 | 11 | 10 |
| 89502382 | REMOVE EXISTING DOUBLE HANDHOLE | EACH | 1 | 1 | |
| 89502385 | REMOVE EXISTING CONCRETE FOUNDATION | EACH | 18 | 9 | 9 |

*

*

2

* 100% COST TO THE VILLAGE
OF CHICAGO HEIGHTS

* Specialty Items
Rev.

12 Rev. 12-11-14

| | | | |
|--------------|-------------|------------|-----------|
| FILE NAME : | USER NAME : | DESIGNED : | REVISED : |
| DRWN : | CHECKED : | DATE : | REVISED : |
| PLOT SCALE : | PLOT DATE : | | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EAST END AVE. (26TH ST. - 30TH ST.)
SUMMARY OF QUANTITIES

| | | | | | | | |
|--------|---------------------|--------------|-------------------------------------------------|-------------|--------|--------------|-----------|
| SCALE: | SHEET NO. OF SHEETS | STA. TO STA. | F.A.U. RATE: | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | | 2887 | 3155 N (13) | COOK | 88 | 6 |
| | | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

CONTRACT NO. 60W80

| SUMMARY OF QUANTITIES | | | | CONSTRUCTION TYPE CODE | | | |
|-----------------------|-------------------------------------------------------------------------------|-------|------------|--------------------------|------------------------|---|--|
| CODE NO | ITEM | UNIT | QUANTITIES | TRAFFIC SIGNAL 0021 | | | |
| | | | | EAST END AVE./SAUK TRAIL | EAST END AVE./26TH ST. | | |
| * X0324085 | EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3/C | FOOT | 448 | | 448 | | |
| X2020110 | GRADING AND SHAPING SHOULDERS | UNIT | 6.5 | 6.5 | | | |
| X4060110 | BITUMINOUS MATERIALS (PRIME COAT) | POUND | 18780 | 18780 | | | |
| X6020094 | MANHOLES, TYPE A, 6' -DIAMETER, TYPE 1 FRAME, CLOSED LID, RESTRICTOR PLATE | EACH | 1 | 1 | | | |
| X6030310 | FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) | EACH | 4 | 4 | | | |
| X6065701 | CONCRETE MEDIAN, TYPE SM-4.06 | SO FT | 1500 | 1500 | | | |
| X7010216 | TRAFFIC CONTROL AND PROTECTION, (SPECIAL) | L SUM | 1 | 1 | | | |
| X8100105 | CONDUIT SPLICE | EACH | 1 | 1 | | | |
| * X8571215 | RAILROAD, FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL | EACH | 1 | | 1 | | |
| * X8620200 | UNINTERRUPTABLE POWER SUPPLY, SPECIAL | EACH | 2 | | 1 | 1 | |
| Z0004538 | HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 10" | SO YD | 191 | 191 | | | |
| Z0004562 | COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT | FOOT | 850 | 850 | | | |
| Z0013798 | CONSTRUCTION LAYOUT | L SUM | 1 | 1 | | | |

| SUMMARY OF QUANTITIES | | | | CONSTRUCTION TYPE CODE | | | |
|-----------------------|-----------------------------------------|-------|------------|--------------------------|------------------------|---|--|
| CODE NO | ITEM | UNIT | QUANTITIES | TRAFFIC SIGNAL 0021 | | | |
| | | | | EAST END AVE./SAUK TRAIL | EAST END AVE./26TH ST. | | |
| Z0018500 | DRAINAGE STRUCTURES TO BE CLEANED | EACH | 41 | 41 | | | |
| Z0030850 | TEMPORARY INFORMATION SIGNING | 50 FT | 155 | 155 | | | |
| Z0048665 | RAILROAD PROTECTIVE LIABILITY INSURANCE | L SUM | 1 | 1 | | | |
| * Z0073510 | TEMPORARY TRAFFIC SIGNAL TIMING | EACH | 2 | | 1 | 1 | |

| | | | |
|---------------------------|-------------------------|------------|-----------|
| FILE NAME = | USER NAME = dr/rt/ta/gh | DESIGNED - | REVISED - |
| PLT SCALE = 1000000' / 1" | CHECKED - | REVISOR - | |
| PLT DATE = 10/20/2014 | DATE - | REVISION - | |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EAST END AVE. (26TH ST. - 30TH ST.)
SUMMARY OF QUANTITIES**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

| | | | | |
|--------------------|---------------------|-------------|-----------------|-------------|
| F.A.U. RTE. 2887 | SECTION 3155 N (13) | COUNTY COOK | TOTAL SHEETS 98 | SHEET NO. 7 |
| CONTRACT NO. 60W80 | | | | |

* Specialty Items
• NON PARTICIPATION ITEM
* 100% COST TO THE VILLAGE OF CHICAGO-HEIGHTS

REV. 12-11-14

URBAN
90% FED.
10% STATE
TOTAL

URBAN
90% FED.
10% STATE
TOTAL

90% FED.
7.5% STATE
2.5% CHICAGO
HEIGHTS

90% FED.
7.5% STATE
2.5% CHICAGO
HEIGHTS

NOTE: THE CONTRACTOR SHALL MILL FIRST, BEFORE PATCH.

| HOT MIX ASPHALT MIXTURE REQUIREMENTS | | |
|----------------------------------------------------------------------------------------------------------------------------|----------------------------|----------------|
| MIXTURE USES | DESIGN AIR VOIDS | OMP |
| SURFACE COURSE MIX "D", N70, IL-9.5mm, 1 1/2" | 4% @ 70 GYR. | QCP |
| POLYMERIZED LEVELING BINDER (MM), IL-4.75mm, N50, 3/4" | 3.5% @ 50 GYR. | QCP |
| HMA BASE COURSE WIDENING (7") (HMA BINDER IL-19.0) | 4% @ 70 GYR | QC/OA |
| CLASS "D" PATCHES (HMA BINDER IL-19mm) | 4% @ 70 GYR | QCP |
| HMA SHOULDER, 6" HMA SURF. CRSE MIX "D", N70, IL-9.5mm, 1 1/2" (HMA BINDER, 4 1/2") (*) | 4% @ 70 GYR 4% @ 70 GYR | QCP QC/OA |
| HMA DRIVEWAY PAVEMENT, 10" HMA BASE COURSE (HMA BINDER, IL-19mm, 8") HMA SURF. CRSE MIX "D", N50, IL-9.5mm, 2" | 4% @ 50 GYR 4% @ 50 GYR | QC/OA QC/OA |
| OMP DESIGNATION: QUALITY CONTROL / QUALITY ASSURANCE (QC/OA) | | |
| QUALITY CONTROL FOR PERFORMANCE (QCP) | | |

* (HMA BINDER IL-19.0)

NOTES:

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANT. IS 112 LBS/SQ YD/IN.

AC TYPE NOTE

"THE AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22", AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-28" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS."

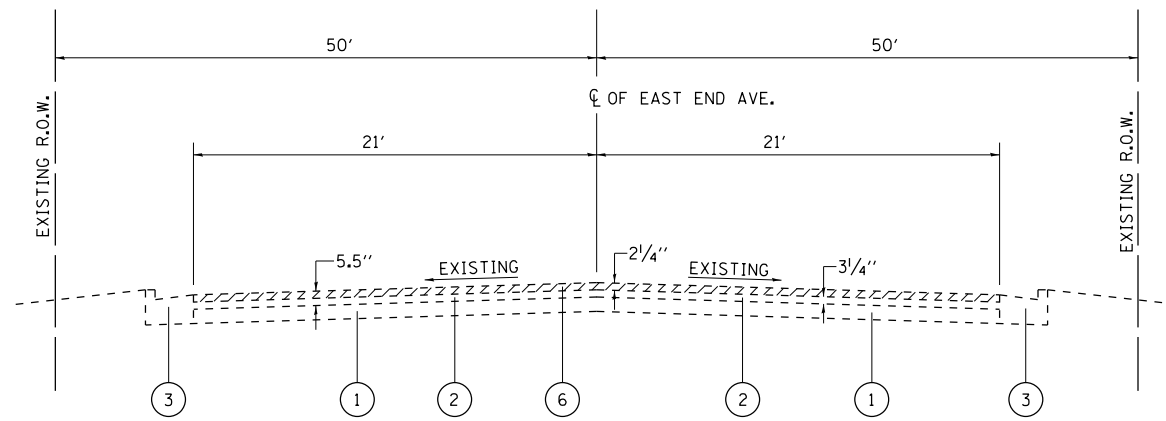
FOR USE OF RECYCLED MATERIALS, SEE DISTRICT ONE SPECIAL PROVISION.

OMP NOTE

"QUALITY MANAGEMENT PROGRAM (OMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE"

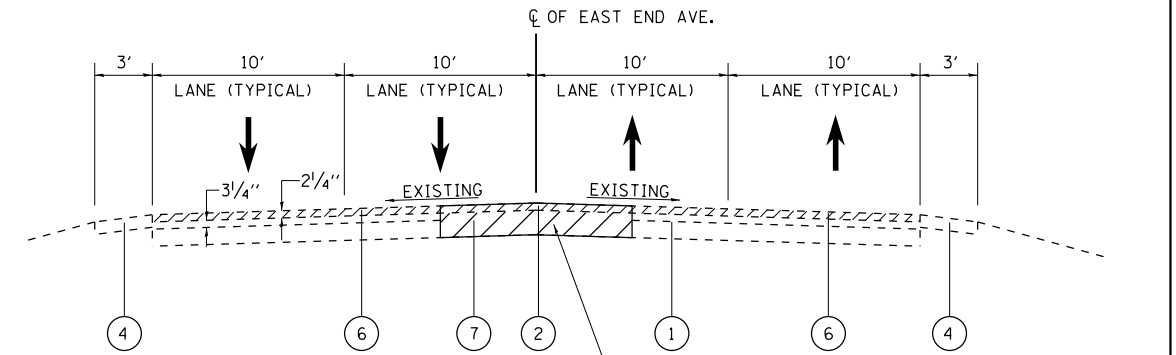
LEGEND

1. EXISTING P.C.C. PAVEMENT 7.5"±
- 1A. EXISTING P.C.C. PAVEMENT 10"±
2. EXISTING HMA SURFACE REMAIN AFTER MILLING, 3/4"
3. EXISTING COMBINATION CONC. CURB AND GUTTER B-6.12
- 3A. EXISTING MEDIAN REMOVABLE
4. EXISTING AGGREGATE SHOULDER
- 4A. PROPOSED GRADING AND SHAPING AGGREGATE SHOULDER
5. EXISTING CURB AND GUTTER REMOVAL
6. PROPOSED HMA SURFACE REMOVAL, 2 1/4"
- 6A. PROPOSED HMA SURFACE REMOVAL, 2"
7. PROPOSED P.C.C. REMOVAL, 7 1/2"
8. PROPOSED HMA SURFACE COURSE, MIX D, N70, 1 1/2"
- 8A. POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4"
9. PROPOSED HMA SHOULDER, 4 1/2"
10. PROPOSED P.C.C. PAVEMENT, 10"
11. DELETED
12. PROPOSED HOT MIX ASPHALT BASE COURSE WIDENING, N70, 7"
13. PROPOSED AGGREGATE SUBGRADE, IMPROVEMENT, 12"
14. PROPOSED CONCRETE CURB AND GUTTER TYPE M-2.12
15. DOWEL BAR
16. PROPOSED AGGREGATE WEDGE SHOULDER, TYPE "B"
17. PROPOSED CONCRETE MEDIAN



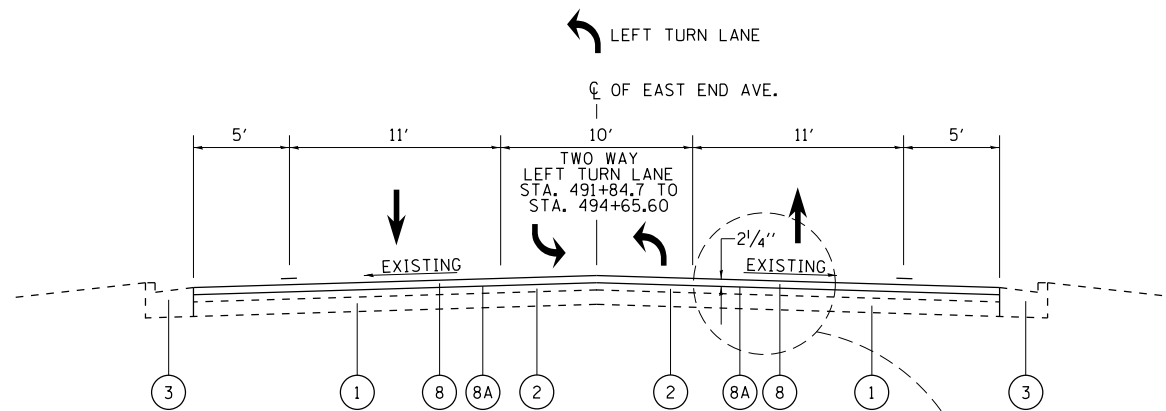
EXISTING TYPICAL SECTION

EAST END AVENUE
STA. 485+17 TO STA. 499+72



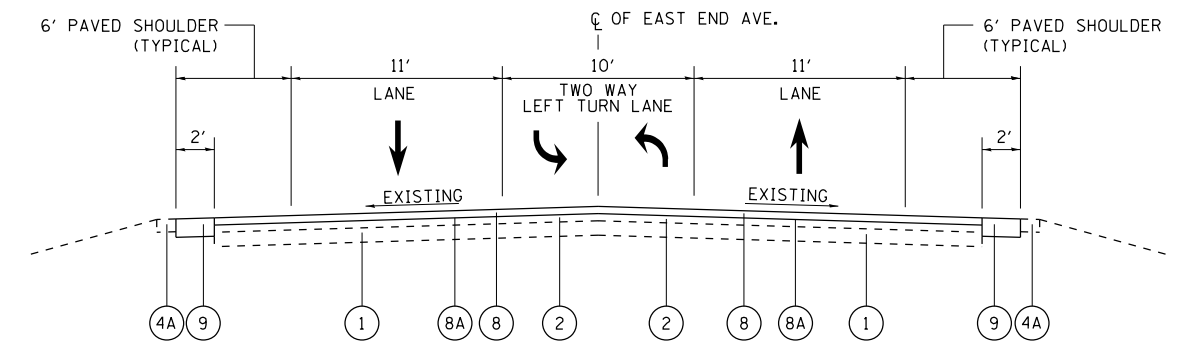
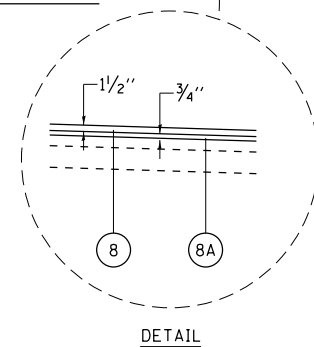
EXISTING TYPICAL SECTION

EAST END AVENUE
STA. 500+28 TO STA. 538+25



PROPOSED TYPICAL SECTION

EAST END AVENUE
STA. 485+17 TO STA. 499+72



PROPOSED TYPICAL SECTION

EAST END AVENUE
STA. 500+28 TO STA. 531+07.68

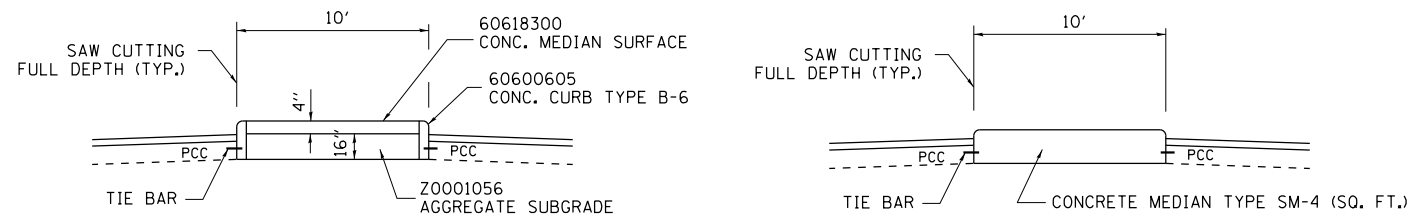
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| | PLOT DATE = 10/16/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EAST END AV. (26TH ST TO 30TH)
TYPICAL SECTIONS**

SCALE: SHEET OF SHEETS STA. TO STA.

| | | | | |
|--------------------|------------|--------|---------------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2887 | 3155-N(13) | COOK | 88 | 8 |
| CONTRACT NO. 60W80 | | | ILLINOIS FED. AID PROJECT | |

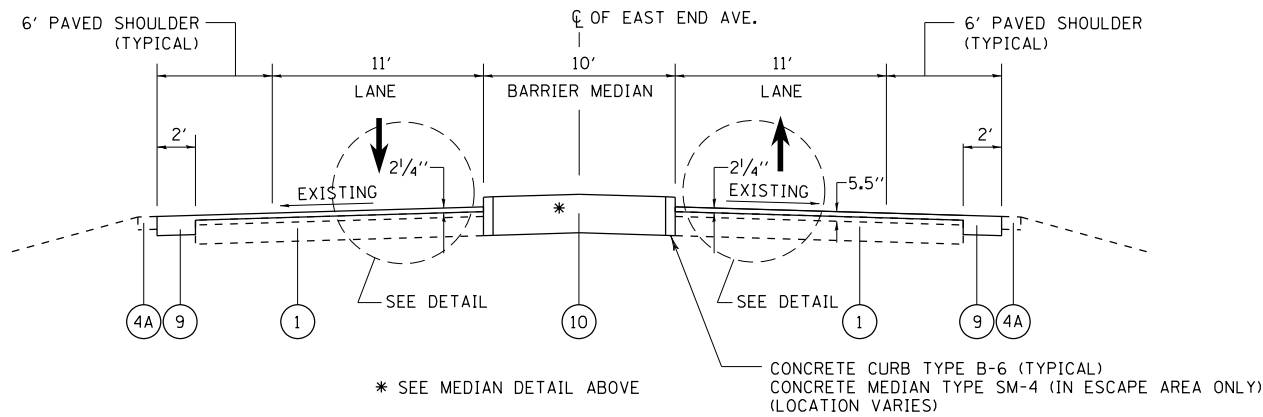


STA. 531+07 TO STA. 531+82
 STA. 533+66 TO STA. 534+89 (WIDTH VARIES 10' TO 2')

CURBED MEDIAN

STA. 531+82.7 TO STA. 532+57.7
 AND STA. 532+91.3 TO STA. 533+66

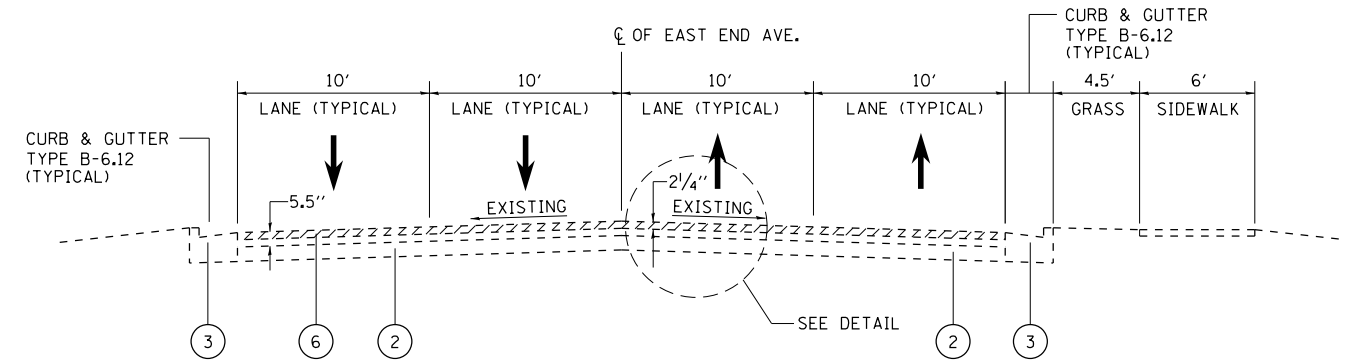
ESCAPE AREA



PROPOSED TYPICAL SECTION

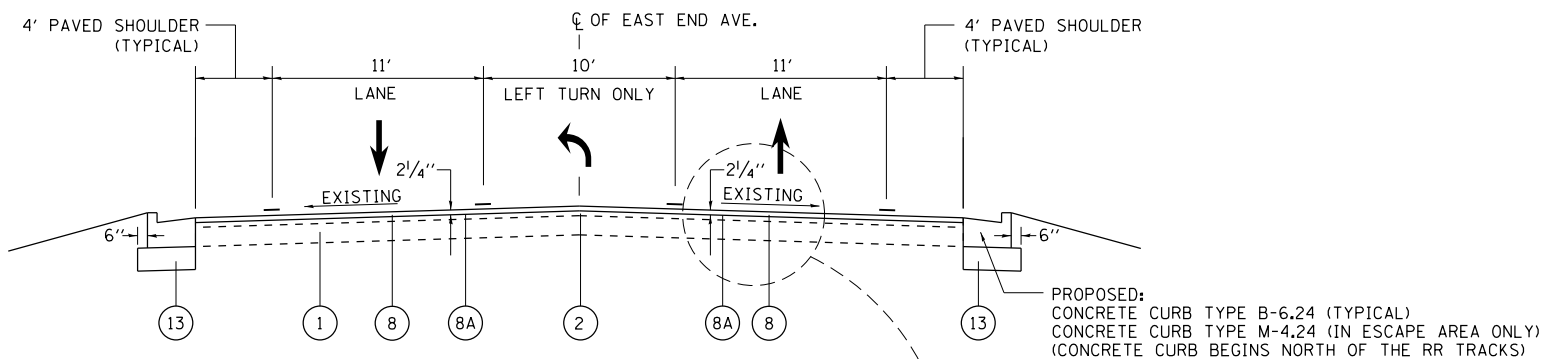
EAST END AVENUE
 STA. 531+07.68 TO STA. 534+89.53

*ESCAPE AREA STA. 531+82.7 TO STA. 532+57.7
 AND STA. 532+91.3 TO STA. 533+67.6



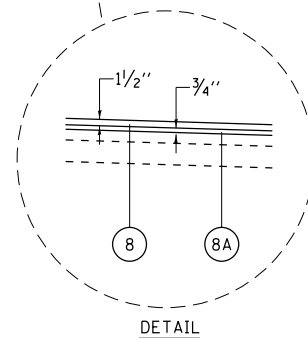
EXISTING TYPICAL SECTION

EAST END AVENUE
 STA. 538+25 TO STA. 542+78

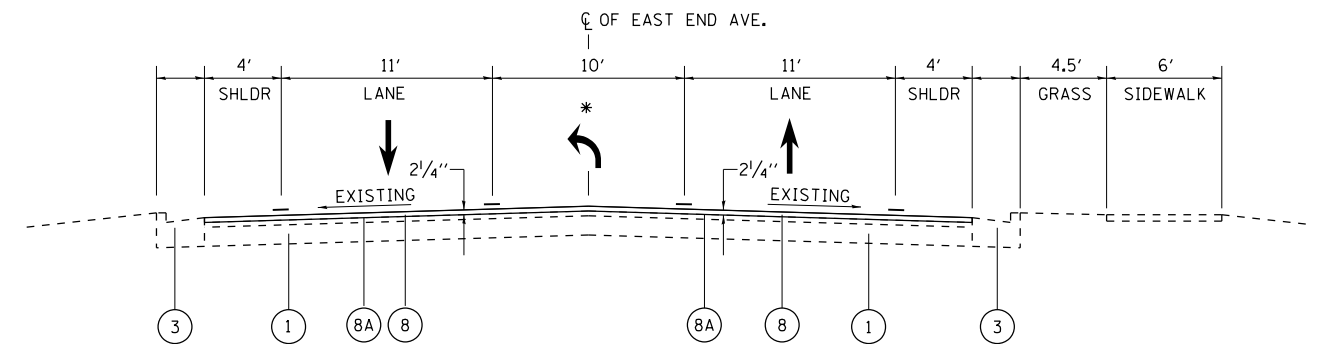


PROPOSED TYPICAL SECTION

EAST END AVENUE
 STA. 534+89.53 TO STA. 538+25



DETAIL



PROPOSED TYPICAL SECTION

EAST END AVENUE
 STA. 538+25 TO STA. 542+78
 (* STA. 542+30 TO STA. 542+78, TWO WAY LEFT TURN)

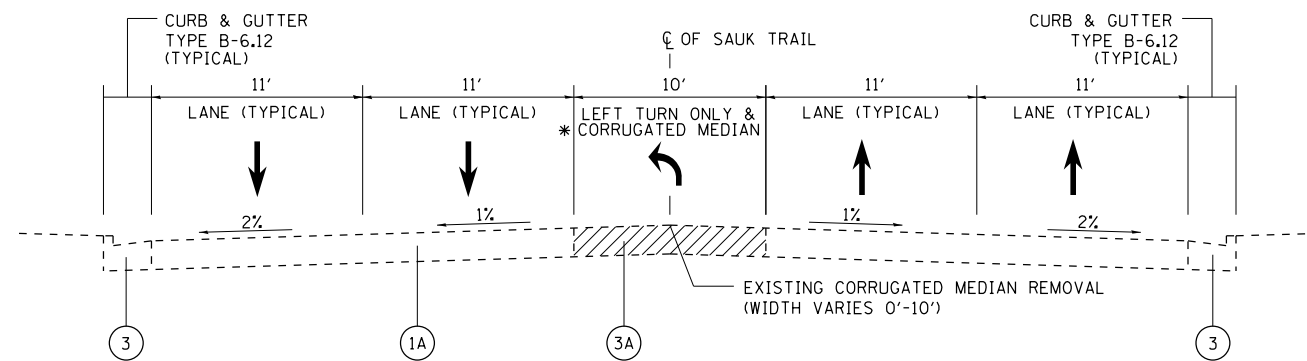
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| Default | PLOT SCALE = 100.0000' / 1in. | CHECKED - | REVISED - |
| | PLOT DATE = 10/16/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**EAST END AV. (26TH ST TO 30TH)
 TYPICAL SECTIONS**

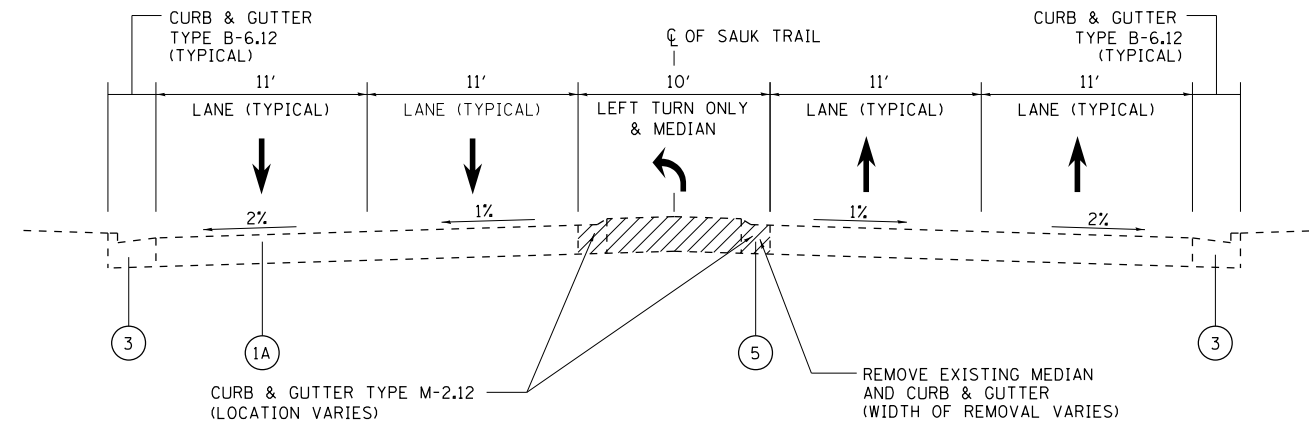
SCALE: SHEET OF SHEETS STA. TO STA.

| | | | | |
|--------------------|------------|--------|---------------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2887 | 3155-N(13) | COOK | 88 | 9 |
| CONTRACT NO. 60W80 | | | ILLINOIS FED. AID PROJECT | |



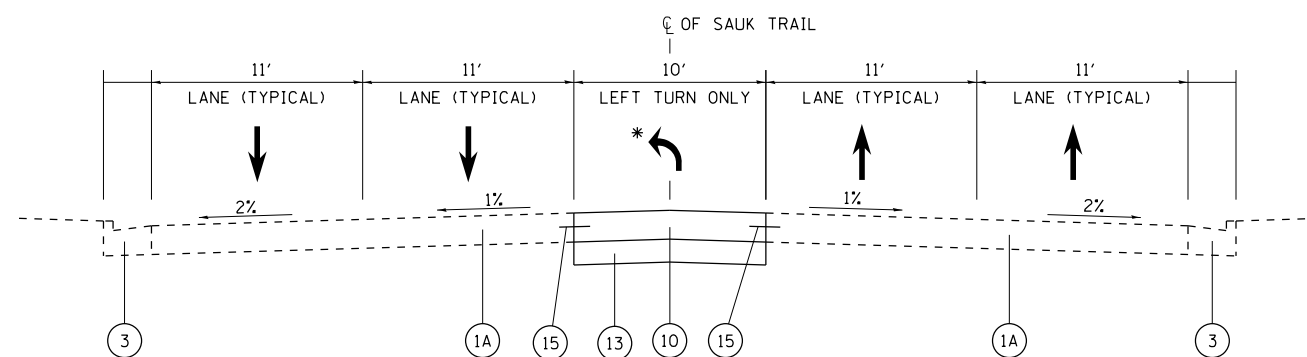
EXISTING TYPICAL SECTION

SAUK TRAIL
 STA. 195+71.73 TO STA. 199+46
 * EXISTING CORRUGATED MEDIAN
 STA. 195+71.73 TO STA. 197+94



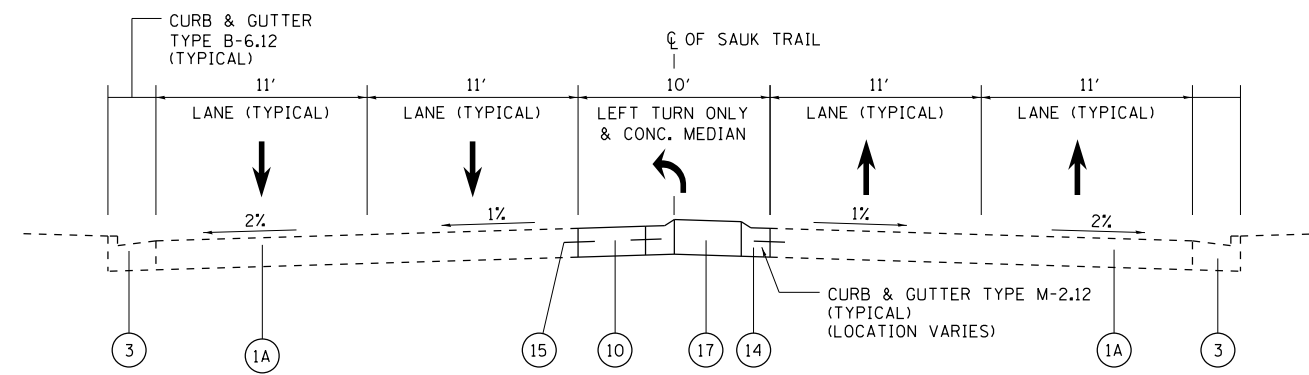
EXISTING TYPICAL SECTION

SAUK TRAIL
 STA. 200+53.9 TO STA. 205+43.28



PROPOSED TYPICAL SECTION

SAUK TRAIL
 STA. 195+71.73 TO STA. 199+46
 * EXISTING CORRUGATED MEDIAN
 STA. 195+71.73 TO STA. 197+94



PROPOSED TYPICAL SECTION

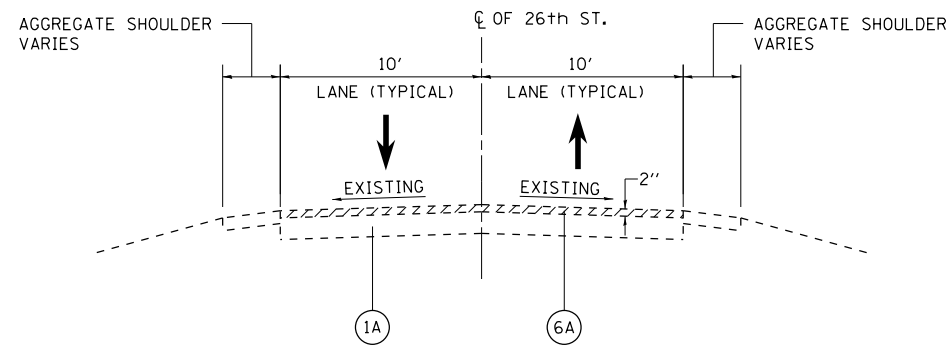
SAUK TRAIL
 STA. 200+53.9 TO STA. 205+43.28

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

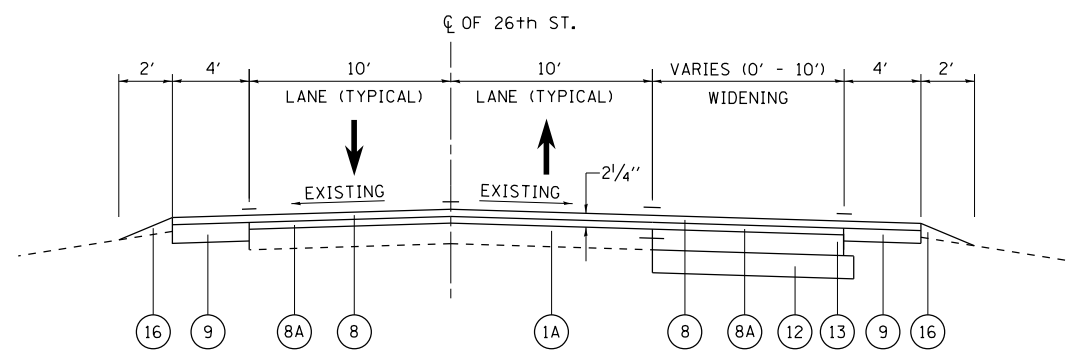
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|----------------------------------------|-------|----------------|---------|--------|-----------------|--------------|
| SAUK TRAIL TYPICAL SECTIONS | | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| SCALE: | SHEET | OF | SHEETS | STA. | TO | STA. |

| | | | | |
|---------------------------|------------|------|----|----|
| 2887 | 3155-N(13) | COOK | 88 | 10 |
| CONTRACT NO. 60W80 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



EXISTING TYPICAL SECTION

26TH STREET
EAST OF EAST END AVE
STA. 300+00 TO STA. 303+09.13



PROPOSED TYPICAL SECTION

26TH STREET
EAST OF EAST END AVE
STA. 300+00 TO STA. 303+09.13

| | | | |
|--------------------------------------------------------------|------------------------------|------------|-----------|
| FILE NAME = | USER NAME = drivakosgn | DESIGNED - | REVISED - |
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| Default | PLOT SCALE = 100.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 10/21/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| | | | |
|--------------------------------------|-------|-----------|--------------|
| 26TH ST. TYPICAL SECTIONS | | | |
| SCALE: | SHEET | OF SHEETS | STA. TO STA. |

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|------------|--------|--------------|-----------|
| 2887 | 3155-N(13) | COOK | 88 | 11 |
| CONTRACT NO. 60W80 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

SUGGESTED STAGING OF CONSTRUCTION AND TRAFFIC CONTROL

STAGE 1

STAGE 1 WILL INVOLVE SHOULDER WIDENING ALONG EAST END AVENUE, INTERSECTION RADII WIDENING AND CURB CONSTRUCTION, AND SAUK TRAIL MEDIAN TURN-LANE EXTENSIONS.

STAGE 1A - SHOULDER WIDENING ALONG EAST END AVENUE

- CONSTRUCT PRIOR TO RESURFACING OF TRAFFIC LANES.
- TWO LANES OF TRAFFIC IN EACH DIRECTION ARE MAINTAINED.
- GRADING AND BINDER COURSE CONSTRUCTED USING DAYTIME OPERATIONS WITH FLAGGERS CLOSING THE OUTER LANE ADJACENT TO CONSTRUCTION.
- CLOSE SHOULDER WITH BARRELS OR TYPE II BARRICADES DURING THE NIGHT.
- PAVE SURFACE COURSE LATER WITH ROADWAY PAVING OPERATION.

STAGE 1B - NE AND SE QUADRANT RADII WIDENING ON 26TH AT EAST END AVENUE AND WIDENING OF THE EAST LEG OF THE INTERSECTION.

- CONSTRUCT PRIOR TO RESURFACING.
- CONSTRUCT WITH DAYTIME OPERATIONS WITH FLAGGERS MAINTAINING ONE LANE OF TRAFFIC ON 26TH STREET.
- CLOSE WORK ZONE WITH BARRELS OR TYPE II BARRICADES DURING THE NIGHT MAINTAINING TWO-WAY TRAFFIC ON EXISTING PAVEMENT.
- PAVE SURFACE COURSE LATER WITH ROADWAY PAVING OPERATION.

STAGE 1C - NW AND SW QUADRANT RADII WIDENING ON SAUK TRAIL AT EAST END AVENUE

- CONSTRUCT PRIOR TO RESURFACING.
- CONSTRUCT WITH DAYTIME OPERATIONS WITH FLAGGERS CLOSING THE OUTER LANE ADJACENT TO CONSTRUCTION.
- CLOSE WORK ZONE WITH BARRELS OR TYPE II BARRICADES DURING THE NIGHT MAINTAINING TWO-WAY TRAFFIC ON EXISTING PAVEMENT.
- PAVE SURFACE COURSE LATER WITH ROADWAY PAVING OPERATION.

STAGE 1D - SAUK TRAIL MEDIAN TURN-LANE EXTENSIONS

- CONSTRUCT PRIOR TO RESURFACING.
- CONSTRUCT WITH DAYTIME OPERATIONS WITH FLAGGERS CLOSING THE INNER LANE ADJACENT TO CONSTRUCTION.
- CLOSE WORK ZONE WITH BARRELS OR TYPE II BARRICADES DURING THE NIGHT MAINTAINING TWO-WAY TRAFFIC ON EXISTING PAVEMENT.
- PAVE SURFACE COURSE LATER WITH ROADWAY PAVING OPERATION.

STAGE 2

STAGE 2 WILL INVOLVE CONSTRUCTION OF THE BARRIER MEDIAN AT THE SPUR RAILROAD CROSSING OF EAST END AVENUE.

- CONSTRUCT JUST PRIOR TO RESURFACING. CLOSE THE INNER TWO LANES FOR THE WORK ZONE MAINTAINING TRAFFIC ON THE OUTER TWO LANES.
- FLAGGERS AS NEEDED DURING CONSTRUCTION (1 LANE ROADWAY).

STAGE 3

STAGE 3 INVOLVES RESURFACING 26TH STREET FROM STA. 299+45.5 TO STA. 303+09.13 AND WIDENING THE EAST LEG OF THE EAST END AVENUE (AT 26TH STREET INTERSECTION).

STAGE 3A - ROADWAY RESURFACING OF EASTBOUND LANES AND WIDENING OF THE EAST LEG OF THE EAST END AVENUE (AT 26TH STREET INTERSECTION).

- COORDINATE WITH UNION PACIFIC RAILROAD.
- REPAVING TO OCCUR ON EXISTING EASTBOUND LANES.
- CONSTRUCT WITH DAYTIME MOVING OPERATIONS AND FLAGGERS.
- MAINTAIN TRAFFIC AT NIGHT WITH SHORT-TERM PAVEMENT MARKINGS.
- ONE 9-FOOT LANE OF TRAVEL MAINTAINED IN EACH DIRECTION ON THE EXISTING WESTBOUND LANES.

STAGE 3B - ROADWAY RESURFACING OF WESTBOUND LANES.

- COORDINATE WITH UNION PACIFIC RAILROAD.
- REPAVING TO OCCUR ON EXISTING WESTBOUND LANES.
- CONSTRUCT WITH DAYTIME MOVING OPERATIONS AND FLAGGERS.
- MAINTAIN TRAFFIC AT NIGHT WITH SHORT-TERM PAVEMENT MARKINGS.
- ONE 9-FOOT LANE OF TRAVEL MAINTAINED IN EACH DIRECTION ON THE EXISTING EASTBOUND LANES.

STAGE 4

STAGE 4 INVOLVES RESURFACING AND RESTRIPING THE EAST END AVENUE ROADWAY.

STAGE 4A - ROADWAY RESURFACING OF SOUTHBOUND LANE AND SHOULDER.

- CONSTRUCT WITH DAYTIME MOVING OPERATIONS AND FLAGGERS.
- MAINTAIN TRAFFIC AT NIGHT WITH SHORT-TERM PAVEMENT MARKINGS.
- ONE 11-FOOT LANE OF TRAVEL MAINTAINED IN EACH DIRECTION ON THE EXISTING NORTHBOUND LANES.

STAGE 4B - ROADWAY RESURFACING OF NORTHBOUND LANE AND SHOULDER.

- CONSTRUCT WITH DAYTIME MOVING OPERATIONS AND FLAGGERS.
- MAINTAIN TRAFFIC AT NIGHT WITH SHORT-TERM PAVEMENT MARKINGS.
- ONE 11-FOOT LANE OF TRAVEL MAINTAINED IN EACH DIRECTION ON THE EXISTING SOUTHBOUND LANES.

STAGE 4C - ROADWAY RESURFACING OF CENTER/MEDIAN TWO-WAY LEFT-TURN LANE.

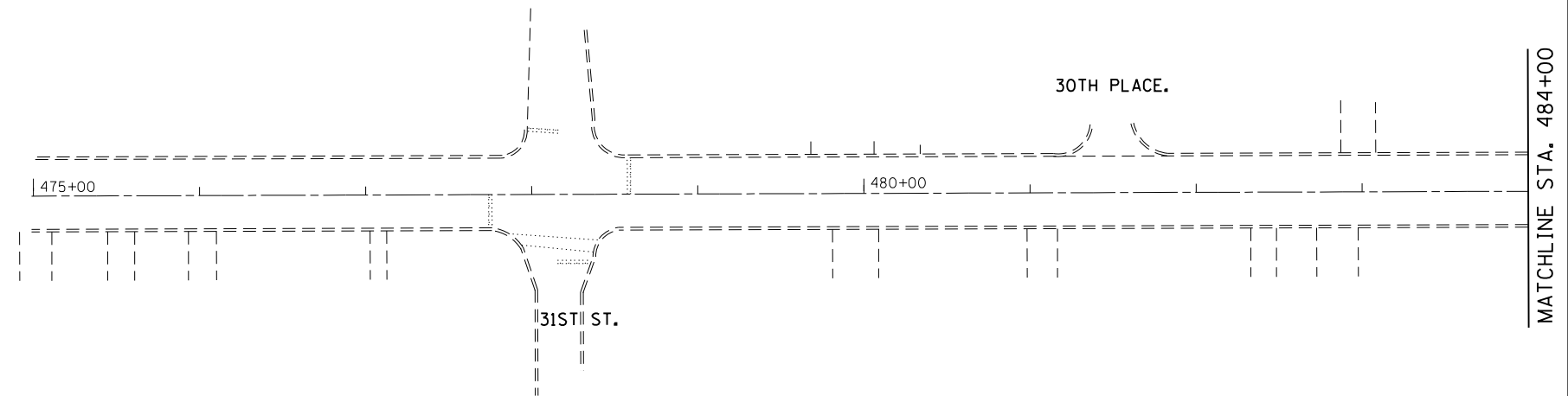
- CONSTRUCT WITH DAYTIME MOVING OPERATIONS AND FLAGGERS.
- MAINTAIN TRAFFIC AT NIGHT WITH SHORT-TERM PAVEMENT MARKINGS.
- ONE 11-FOOT LANE OF TRAVEL MAINTAINED IN EACH DIRECTION ON THE REPAVED OUTER LANES/SHOULDERS.

ANY PROPOSED ACTIVITY IN THE VICINITY OF A HIGHWAY-RAIL GRADE CROSSING MUST ADHERE TO THE GUIDELINES SET FORTH IN THE MANUAL ON THE UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) UNDER SECTION 6G.18: WORK IN THE VICINITY A GRADE CROSSING WHICH STATES: "WHEN GRADE CROSSINGS EXIST EITHER WITHIN OR IN THE VICINITY OF A TTC ZONE, LANE RESTRICTIONS, FLAGGING, OR OTHER OPERATIONS SHALL NOT CREATE CONDITIONS WHERE VEHICLES CAN BE QUEUED ACCROSS THE TRACKS. IF THE QUEUING OF VEHICLES ACROSS THE TRACKS CANNOT BE AVOIDED, A UNIFORMED LAW ENFORCEMENT OFFICER OR FLAGGER SHALL BE PROVIDED AT THE CROSSING TO PREVENT VEHICLES FROM STOPPING ON THE TRACKS, EVEN IF AUTOMATIC WARNING DEVICES ARE IN PLACE.

THE FOLLOWING LIST OF STANDARDS WILL BE USED AS APPLICABLE TO EACH STAGE OF CONSTRUCTION

| | |
|--------|-----------------------------------------------------------------------------|
| 701011 | OFF-ROAD MOVING OPERATIONS, 2L, 2W, DAYTIME ONLY FOR SPEEDS ≥ 45 MPH. |
| 701326 | LANE CLOSURE, 2L, 2W, PAVEMENT WIDENING FOR SPEEDS ≥ 45 MPH |
| 701426 | LANE CLOSURE MULTILANE INTERMITTENT OR MOVING OPERATION FOR SPEEDS ≥ 45 MPH |
| 701502 | URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT LANE |
| 701701 | URBAN LANE CLOSURE, MULTILANE INTERSECTION |

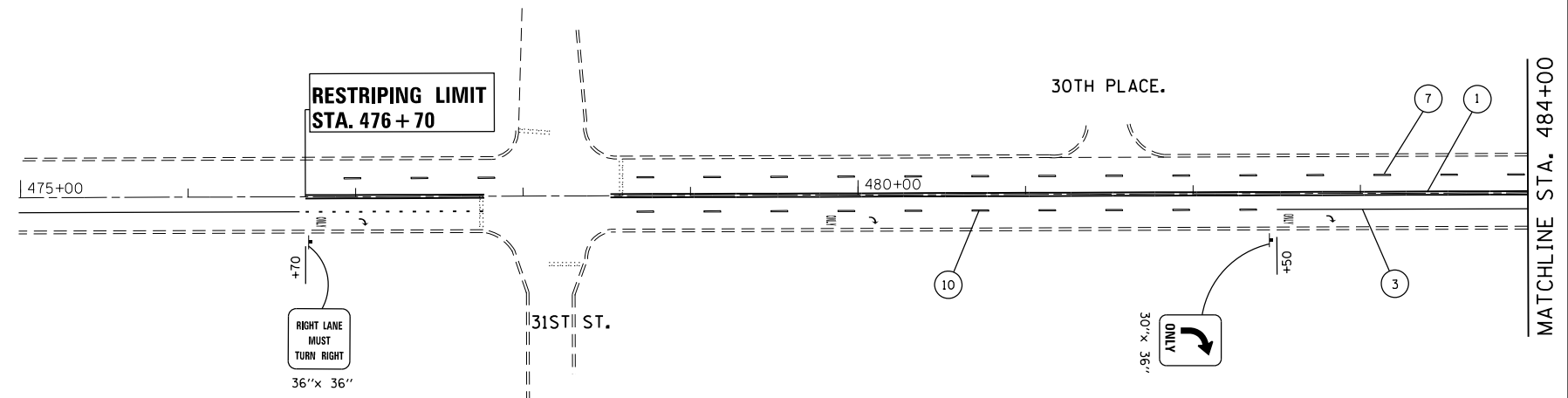
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| FILE NAME = | USER NAME = drivakosgn | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | EAST END AVE. (26TH ST. TO 30TH ST.) SUGGESTED STAGING OF CONSTRUCTION AND TRAFFIC CONTROL | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| ei:\pw\work\p1\dot\drivakosgn\d0271285\PI34811-sh-t-plan.dgn | DRAWN - | REVISED - | 2887 | | | 3155-N(13) | COOK | 88 | 12 | | |
| Default | PLOT SCALE = 100.0000' / in. | CHECKED - | REVISED - | | | CONTRACT NO. 60W80 | | | | | |
| | PLOT DATE = 10/21/2014 | DATE - | REVISED - | | | SCALE: N/A | SHEET OF SHEETS | STA. TO STA. | ILLINOIS FED. AID PROJECT | | |



EXISTING

MATCHLINE STA. 484+00

NOTE:
FOR RAISED REFLECTIVE PAVEMENT MARKERS, REFER TO TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) AND DISTRICT TYPICAL PAVEMENT MARKERS.



MATCHLINE STA. 484+00

THERMOPLASTIC PAVEMENT MARKING LEGEND

- | | |
|---------------------------------------------------------------|----------------------------------------------------|
| ① 4" DOUBLE SOLID YELLOW LINE, 11' C-C | ⑥ 12" SOLID WHITE DIAGONALS, AT 45 DEGREES |
| ② 4" SOLID WHITE LINE (EDGE LINE) | ⑦ 4" SKIP-DASH WHITE LINE, 10' LINE WITH 30' SPACE |
| ③ 6" SOLID WHITE LINE (TURN LANE LINE) | ⑧ SOLID WHITE LETTERS AND SYMBOLS |
| ④ 24" SOLID WHITE LINE (STOP BAR) | ⑨ 16" WHITE LINE AT RAILROAD CROSSING |
| ⑤ 12" SOLID YELLOW DIAGONALS, AT 45 DEGREES 20' C-C (MIN. 5') | ⑩ 6" SKIP-DASH WHITE LINE, 3' LINE WITH 9' SPACE |
| | ⑪ 6" SKIP-DASH WHITE LINE, 2' LINE WITH 6' SPACE |

PROPOSED

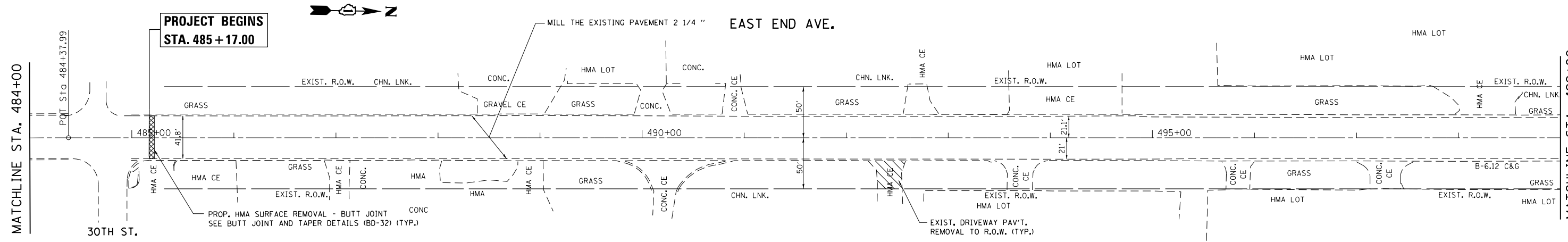
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| Default | PLOT SCALE = 100.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 10/17/2014 | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED ROADWAY AND PAVEMENT MARKING PLAN
EAST END AVENUE (26th ST. - 30th ST.)

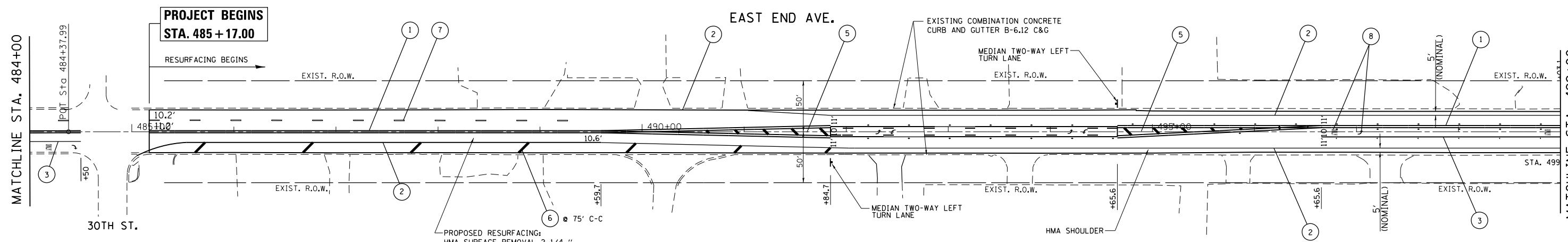
SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|------------|--------|--------------|-----------|
| 2887 | 3155-N(13) | COOK | 88 | 13 |
| CONTRACT NO. 60W80 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



EXISTING

NOTE:
FOR RAISED REFLECTIVE PAVEMENT MARKERS, REFER TO TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) AND DISTRICT TYPICAL PAVEMENT MARKERS.

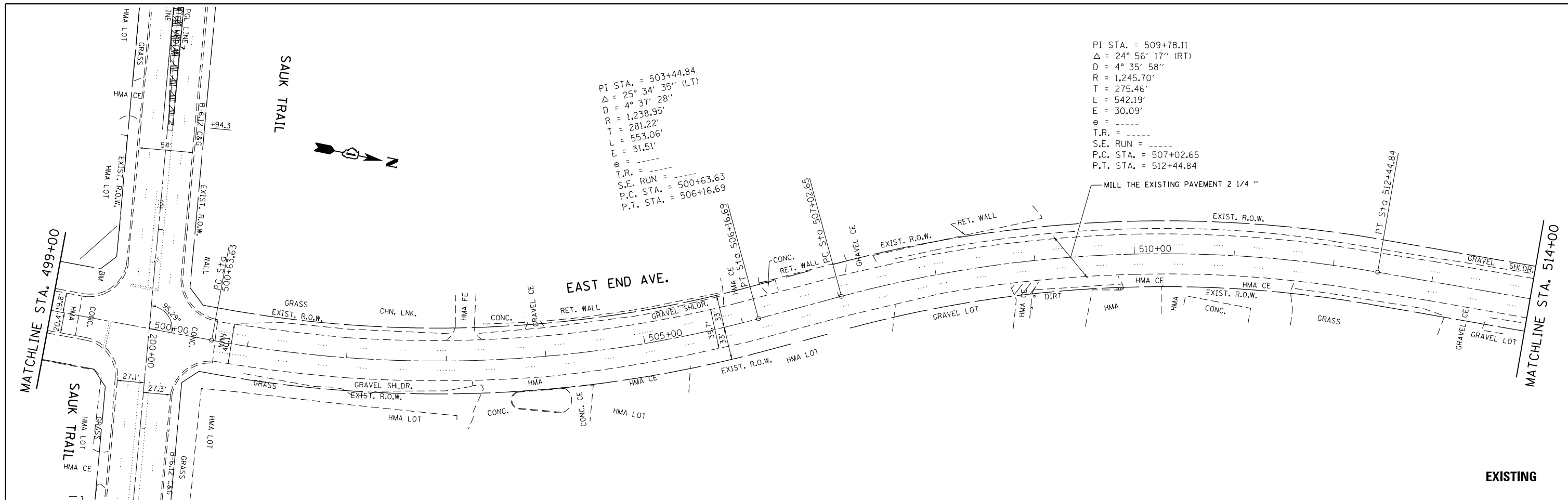


PROPOSED

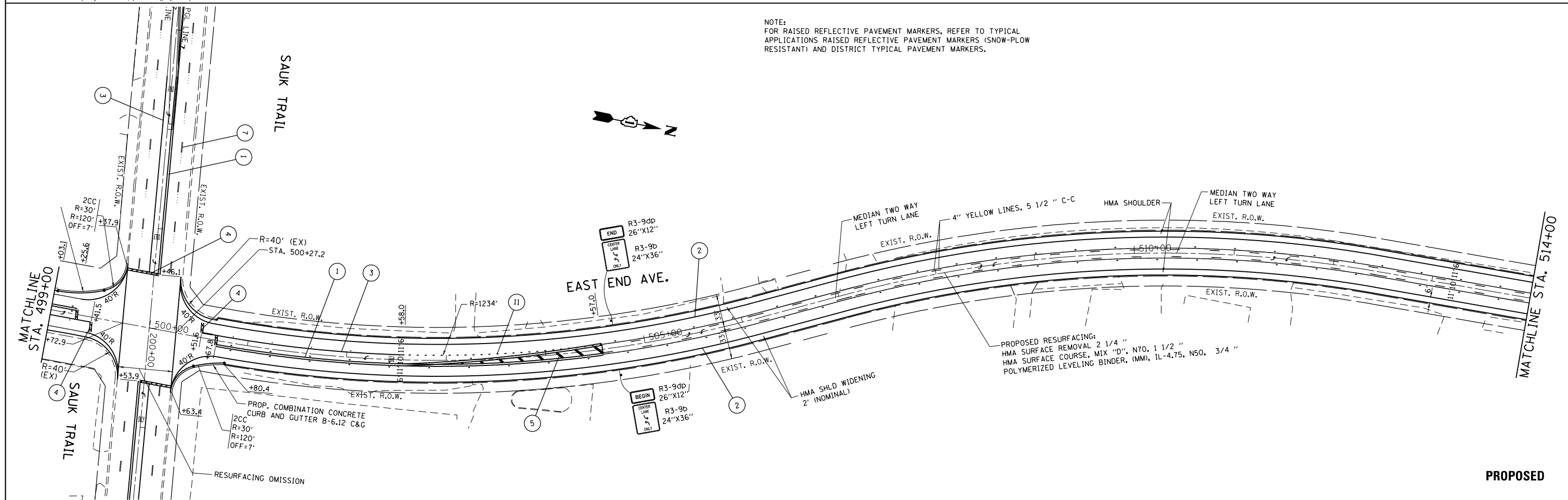
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED ROADWAY AND PAVEMENT MARKING PLAN
EAST END AVENUE (26th ST. - 30th ST.)

| | | | | | | | | | | |
|-------------------------------------------------------------|------------------------|------------|---------------------------|---------------|------------------------------|-------------|---------|--------|--------------|-----------|
| FILE NAME = | USER NAME = drivakosgn | DESIGNED - | REVISED - | SCALE: 1"=50' | SHEET OF SHEETS STA. TO STA. | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| ei:\pw\work\p\dot\drivakosgn\d0271285\PI34811-shit-plan.dgn | DRAWN - | REVISED - | 2887 | | | 3155-N(13) | COOK | 88 | 14 | |
| PLOT SCALE = 100.0000' / in. | CHECKED - | REVISED - | CONTRACT NO. 60W80 | | | | | | | |
| Default | DATE - | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | | |



EXISTING

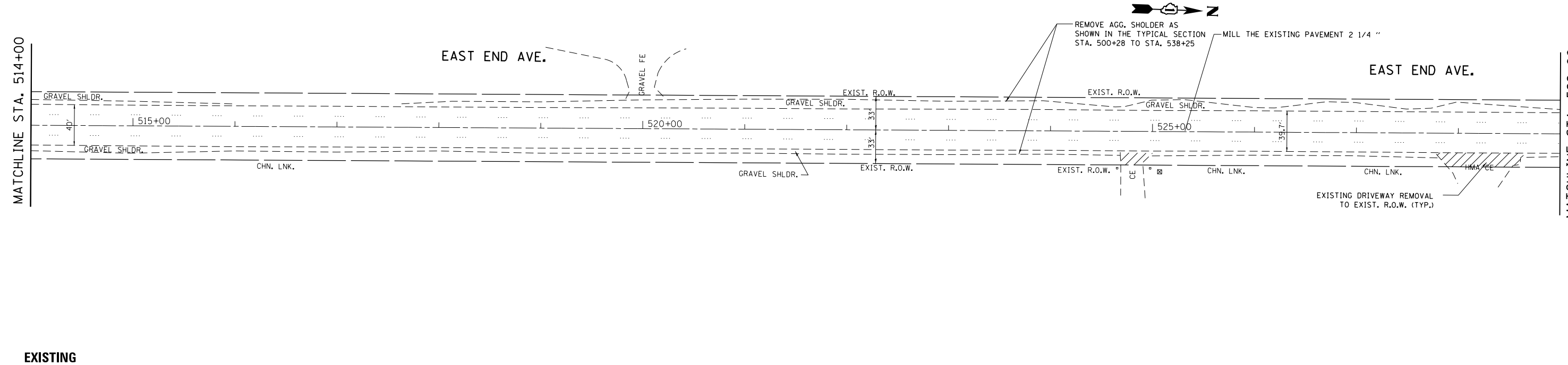


PROPOSED

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| Default | Plot Date = 10/17/2014 | DRAWN - | REVISED - | | | 2887 | 3155-N(13) | COOK | 88 | 15 | |
| | | CHECKED - | REVISED - | | | CONTRACT NO. 60W80 | | | | | |
| | | DATE - | REVISED - | | | ILLINOIS FED. AID PROJECT | | | | | |

MATCHLINE STA. 514+00

MATCHLINE STA. 529+00

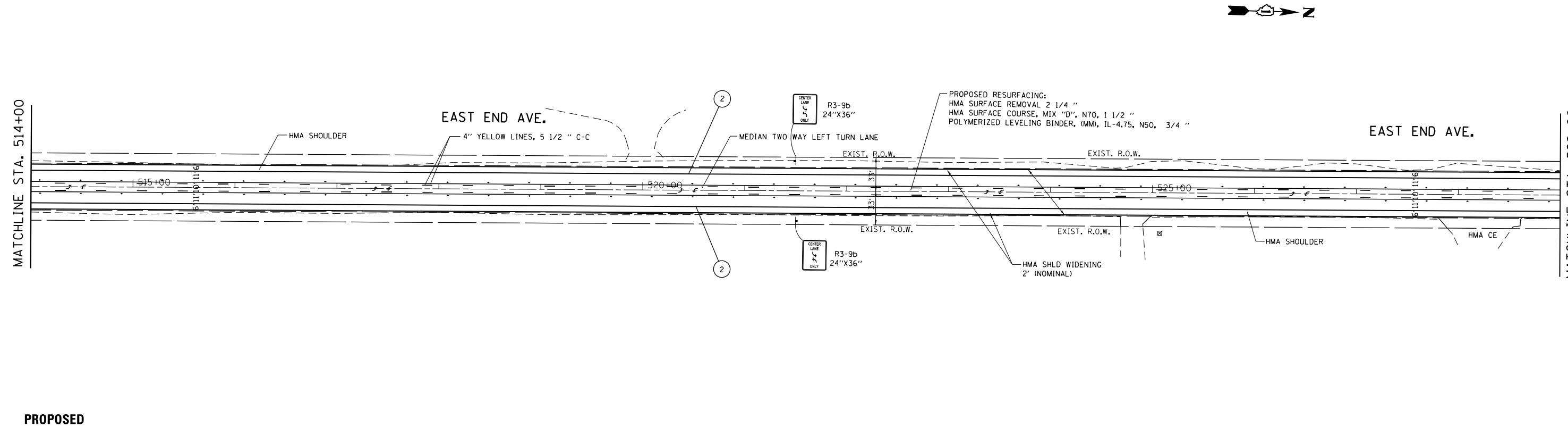


EXISTING

NOTE:
 FOR RAISED REFLECTIVE PAVEMENT MARKERS, REFER TO TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) AND DISTRICT TYPICAL PAVEMENT MARKERS.

MATCHLINE STA. 514+00

MATCHLINE STA. 529+00



PROPOSED

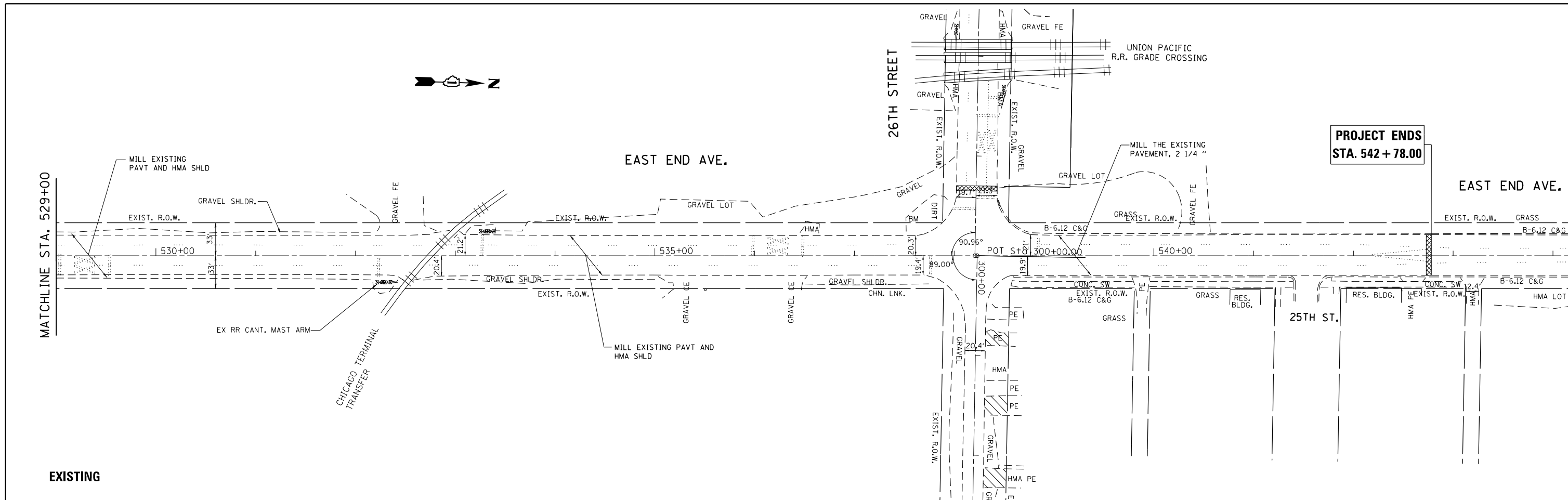
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| Default | PLOT SCALE = 100.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 10/17/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

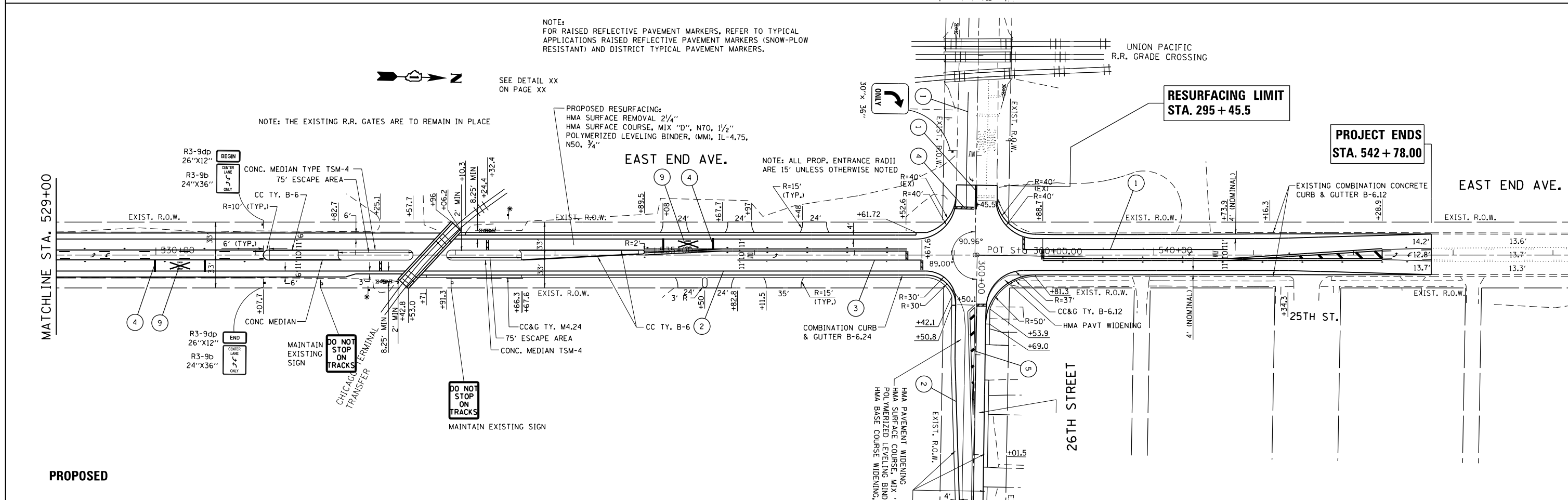
**EXISTING AND PROPOSED ROADWAY AND PAVEMENT MARKING PLAN
 EAST END AVENUE (26th ST. - 30th ST.)**

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|------------|--------|--------------|-----------|
| 2887 | 3155-N(13) | COOK | 88 | 16 |
| CONTRACT NO. 60W80 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



EXISTING



PROPOSED

NOTE: FOR RAISED REFLECTIVE PAVEMENT MARKERS, REFER TO TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) AND DISTRICT TYPICAL PAVEMENT MARKERS.

NOTE: THE EXISTING R.R. GATES ARE TO REMAIN IN PLACE

PROPOSED RESURFACING:
HMA SURFACE REMOVAL 2 1/4"
HMA SURFACE COURSE, MIX "D", N70, 1 1/2"
POLYMERIZED LEVELING BINDER, (MM), IL-4.75,
N50, 3/4"

NOTE: ALL PROP. ENTRANCE RADII ARE 15' UNLESS OTHERWISE NOTED

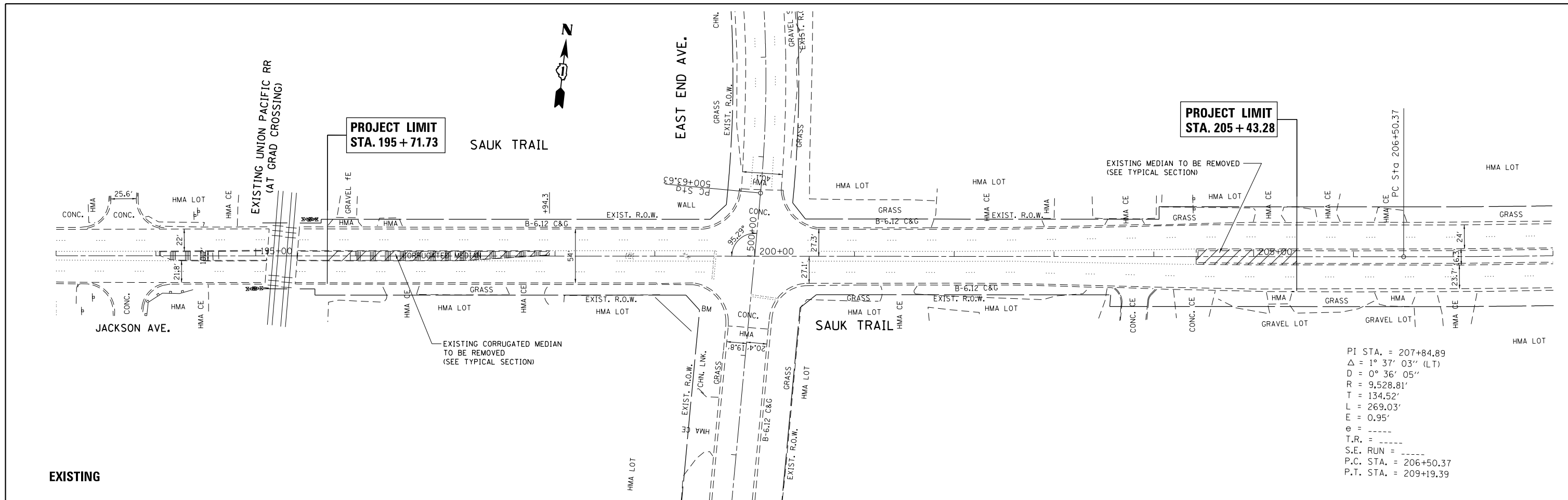
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| Default | PLOT SCALE = 100.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 10/17/2014 | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

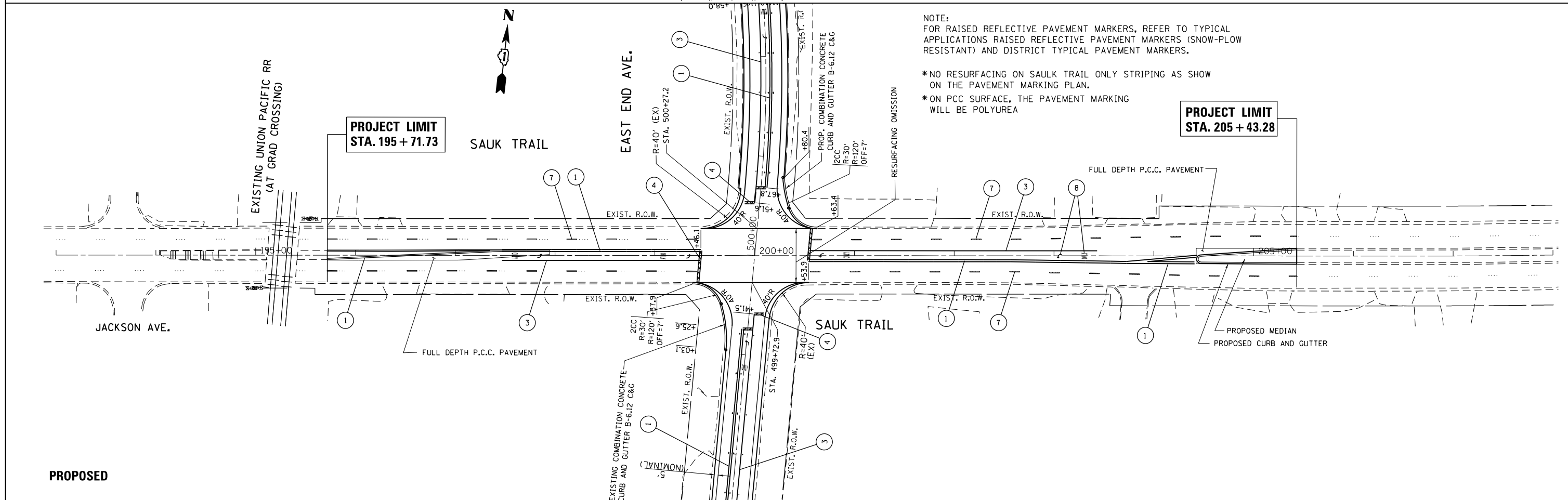
EXISTING AND PROPOSED ROADWAY AND PAVEMENT MARKING PLAN
EAST END AVENUE (26th ST. - 30th ST.)

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

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|--------------------|------------|--------|---------------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2887 | 3155-N(13) | COOK | 88 | 17 |
| CONTRACT NO. 60W80 | | | ILLINOIS FED. AID PROJECT | |



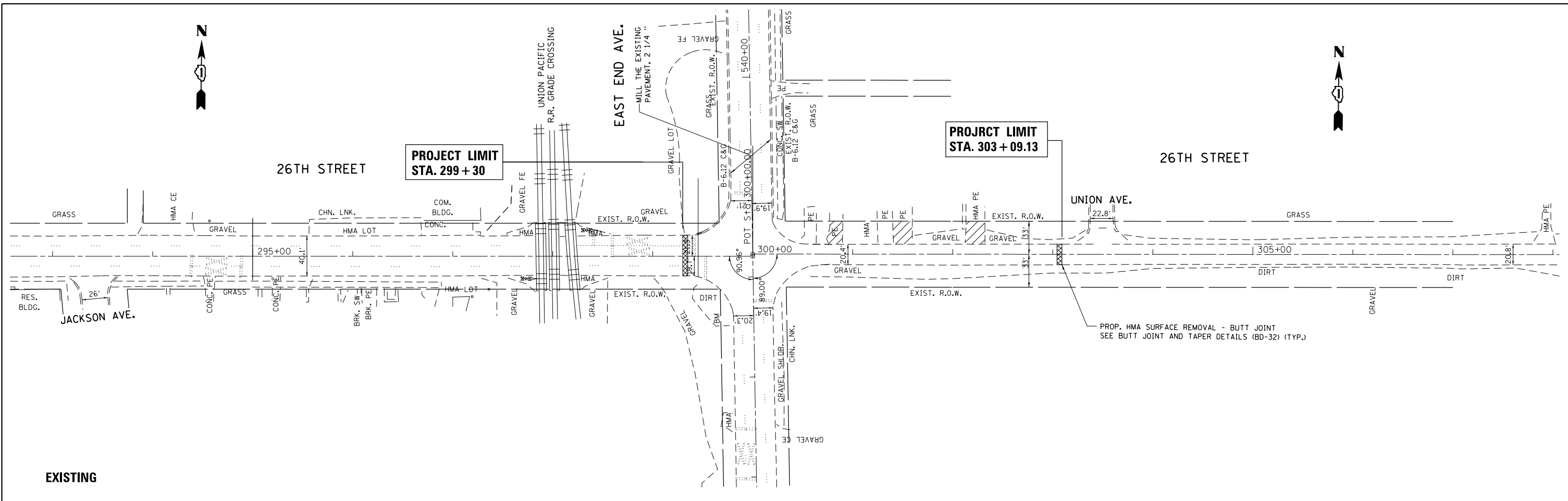
PI STA. = 207+84.89
 Δ = 1° 37' 03" (LT)
 D = 0° 36' 05"
 R = 9,528.81'
 T = 134.52'
 L = 269.03'
 E = 0.95'
 e = -----
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 206+50.37
 P.T. STA. = 209+19.39



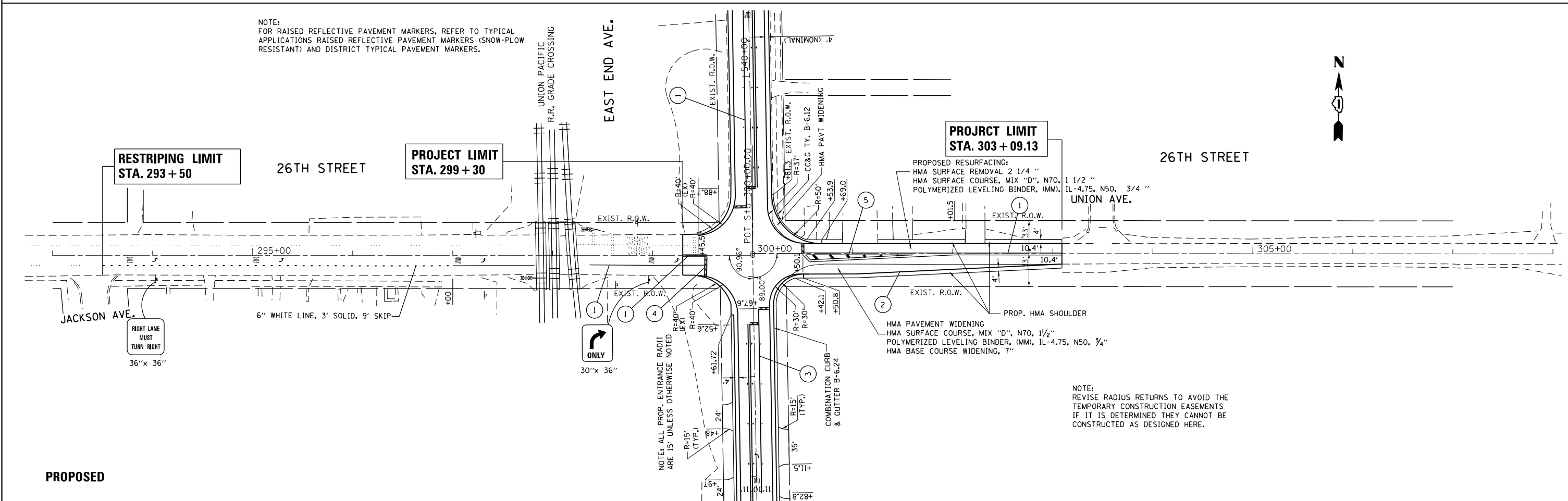
NOTE:
 FOR RAISED REFLECTIVE PAVEMENT MARKERS, REFER TO TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) AND DISTRICT TYPICAL PAVEMENT MARKERS.

- *NO RESURFACING ON SAULK TRAIL ONLY STRIPING AS SHOW ON THE PAVEMENT MARKING PLAN.
- *ON PCC SURFACE, THE PAVEMENT MARKING WILL BE POLYUREA

| | | | | | | | | | | | |
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| FILE NAME = | USER NAME = drivakosgn | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | EXISTING AND PROPOSED ROADWAY AND PAVEMENT MARKING PLAN SAULK TRAIL AT EAST END AVENUE | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
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| | PLOT SCALE = 100.0000' / in. | CHECKED - | REVISED - | | | CONTRACT NO. 60W80 | | | | | |
| | PLOT DATE = 10/17/2014 | DATE - | REVISED - | | | ILLINOIS FED. AID PROJECT | | | | | |



EXISTING



PROPOSED

NOTE:
FOR RAISED REFLECTIVE PAVEMENT MARKERS, REFER TO TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) AND DISTRICT TYPICAL PAVEMENT MARKERS.

NOTE: ALL PROP. ENTRANCE RADII ARE 15' UNLESS OTHERWISE NOTED

NOTE:
REVISE RADIUS RETURNS TO AVOID THE TEMPORARY CONSTRUCTION EASEMENTS IF IT IS DETERMINED THEY CANNOT BE CONSTRUCTED AS DESIGNED HERE.

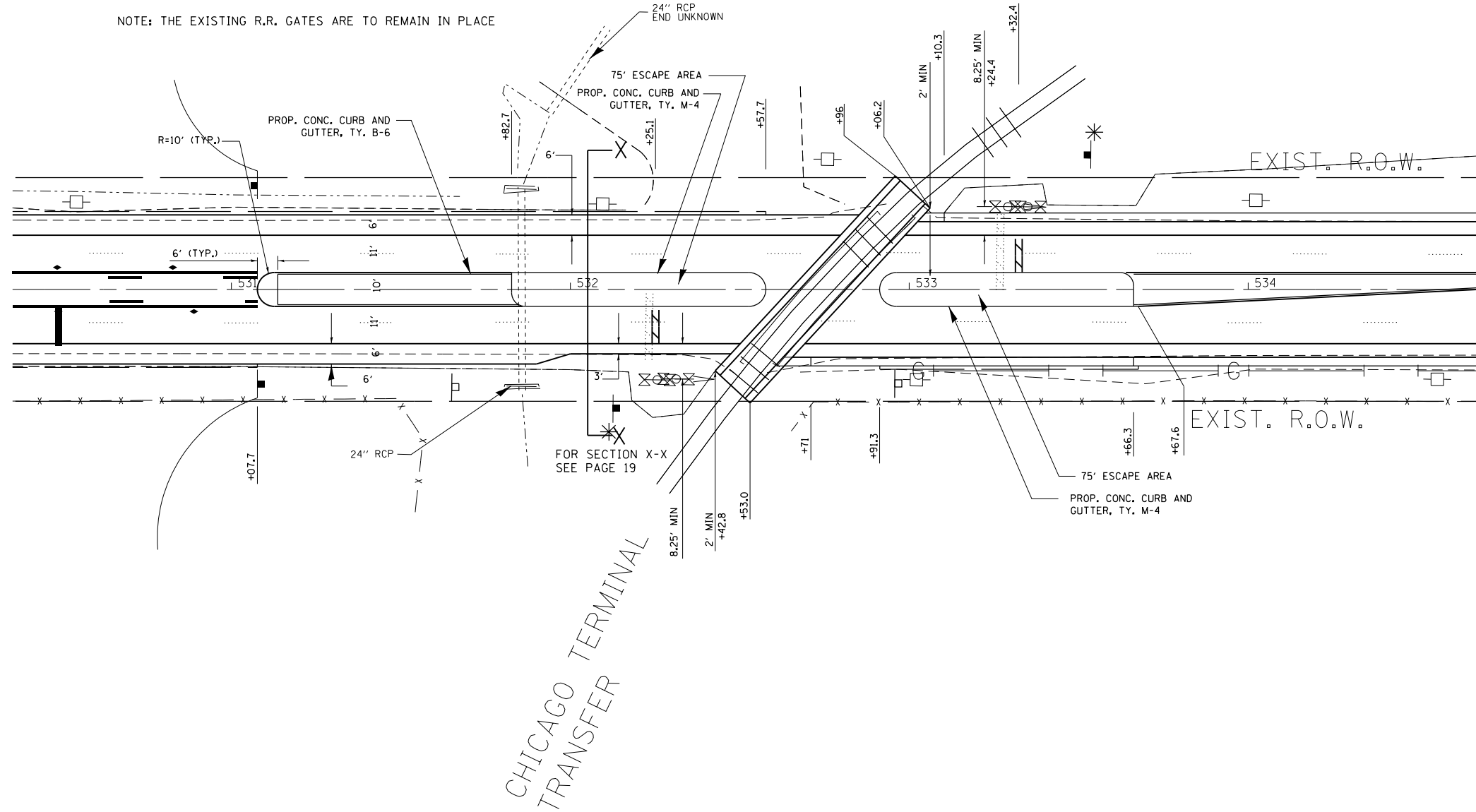
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| Default | PLOT SCALE = 100.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 10/17/2014 | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED ROADWAY AND PAVEMENT MARKING PLAN
26th ST. FROM STA. 299 + 30 TO STA. 303 + 09.13

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|------------|--------|--------------|-----------|
| 2887 | 3155-N(13) | COOK | 88 | 19 |
| CONTRACT NO. 60W80 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



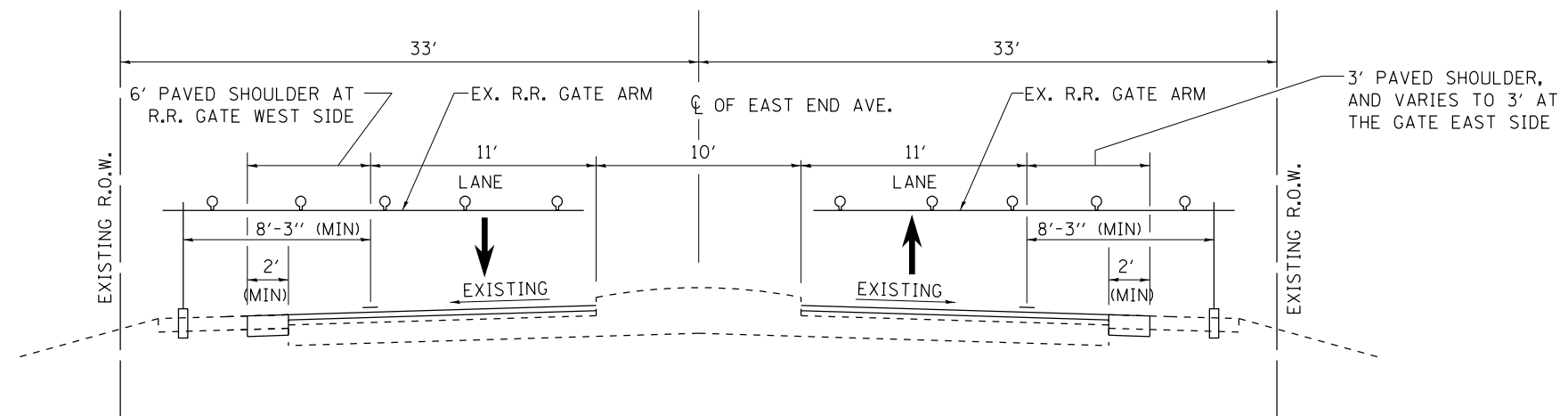
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| Default | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 10/17/2014 | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED DETAILS
AT CHICAGO TERMINAL TRANSFER AND EAST END AVE.

SCALE: 1"=20' SHEET OF SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|------------|--------|--------------|-----------|
| 2887 | 3155-N(13) | COOK | 88 | 20 |
| CONTRACT NO. 60W80 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



CROSS SECTION XX
 EAST END AVENUE
 AT CHICAGO TERMINAL RR CROSSING

NOTE
 PROPOSED HMA RESURFACING GOES UP TO THE EXISTING 2' WIDE CONCRETE PANEL

| | | | |
|----------------------------------------------------------------|------------------------------|------------|-----------|
| FILE NAME = | USER NAME = drivakosgn | DESIGNED - | REVISED - |
| ei:\pw\work\p\dot\drivakosgn\d0271285\p\34811-sh-t-details.dgn | | DRAWN - | REVISED - |
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| | PLOT DATE = 10/20/2014 | DATE - | REVISED - |

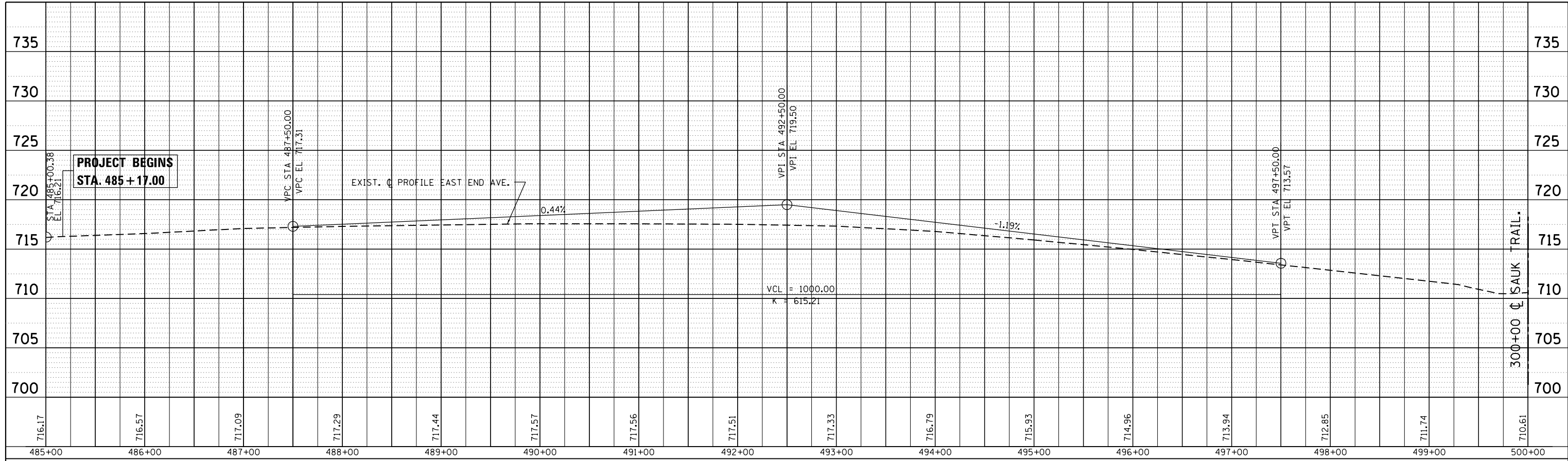
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PROPOSED SECTIONS
 EAST END AV. AT CHICAGO TERMINAL R.R. CROSSING**

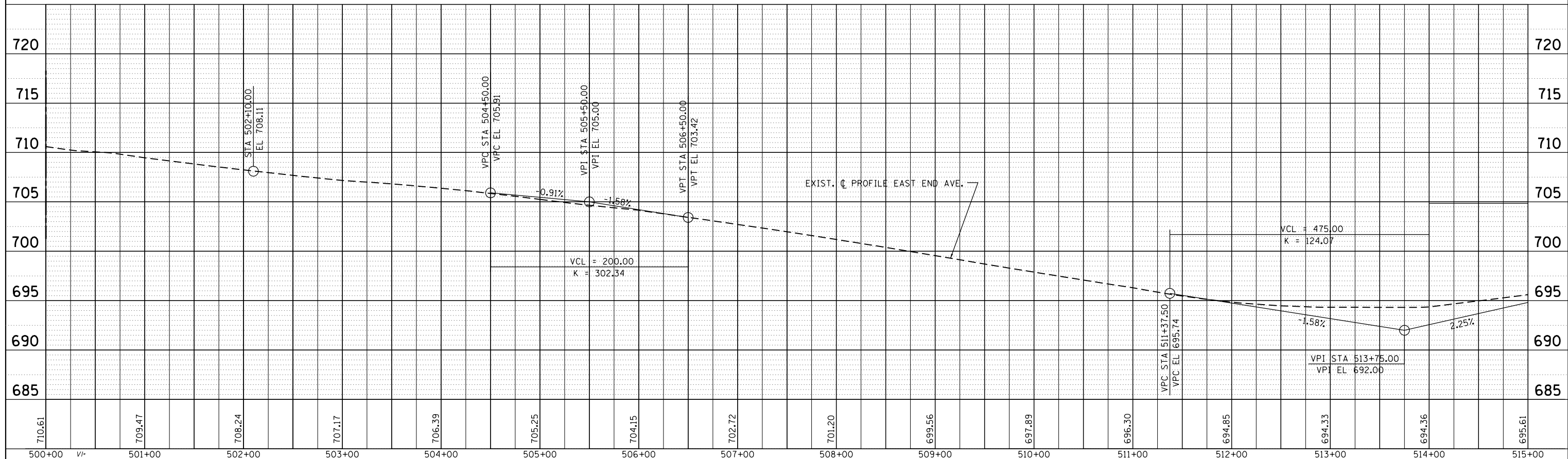
SCALE: SHEET OF SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|------------|--------|--------------|---------------------------|
| 2887 | 3155-N(13) | COOK | 88 | 21 |
| | | | | CONTRACT NO. 60W80 |
| ILLINOIS FED. AID PROJECT | | | | |

| | | | |
|------|----------------|----|------|
| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | NOTE BOOK | | |
| | NO. | | |
| | CARD FILE NAME | | |

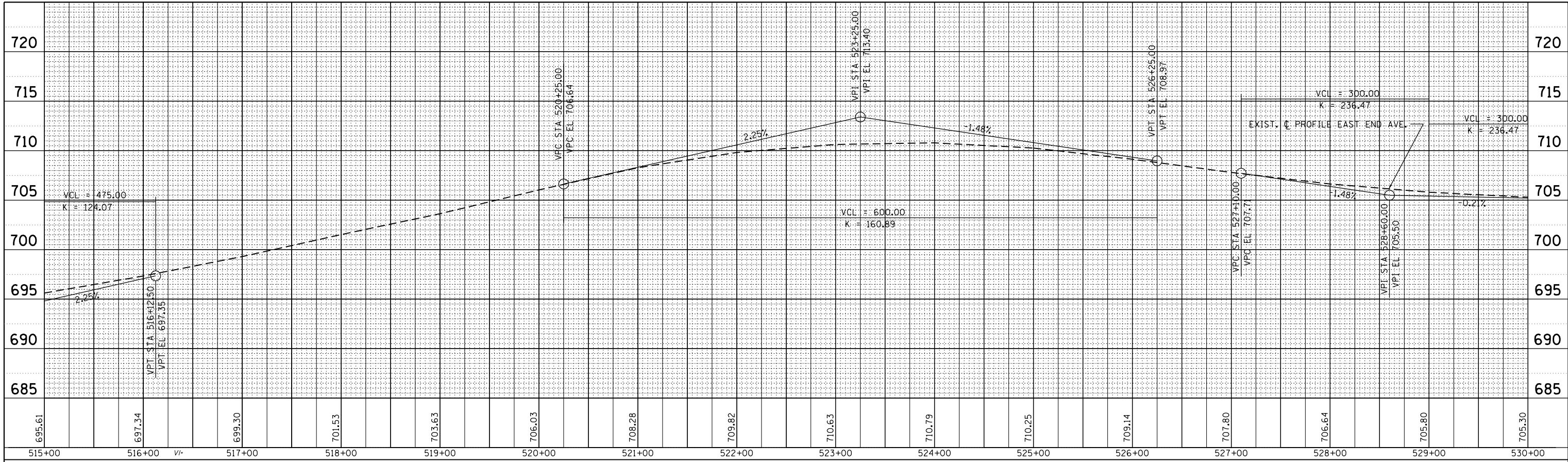


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|---------|----------------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
| | STRUCTURE | | |
| | NOTATIS CHKD | | |
| | NO. | | |

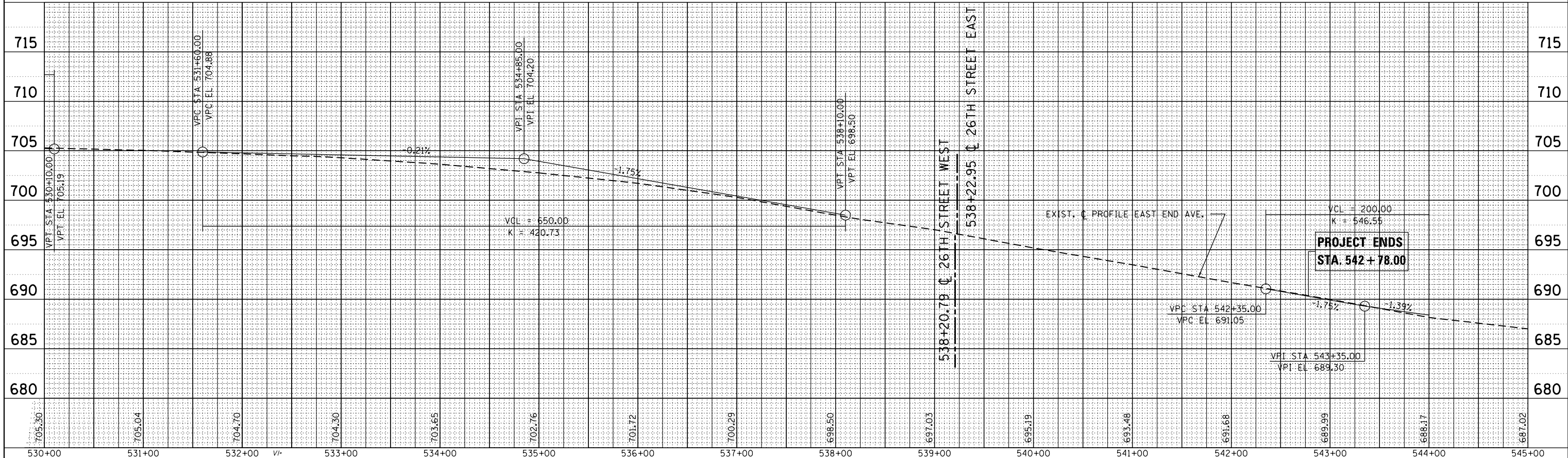


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| FILE NAME = | USER NAME = drivakosgn | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | EXISTING PROFILE EAST END AVENUE (26th ST. - 30th ST.) | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
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| Default | | DATE - | REVISED - | | | ILLINOIS FED. AID PROJECT | | | | |

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|------|-----------------------------|----|------|
| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
| | STRUCTURE NOTATIONS CHECKED | | |
| | NOTE BOOK NO. | | |
| | CARD FILE NAME | | |

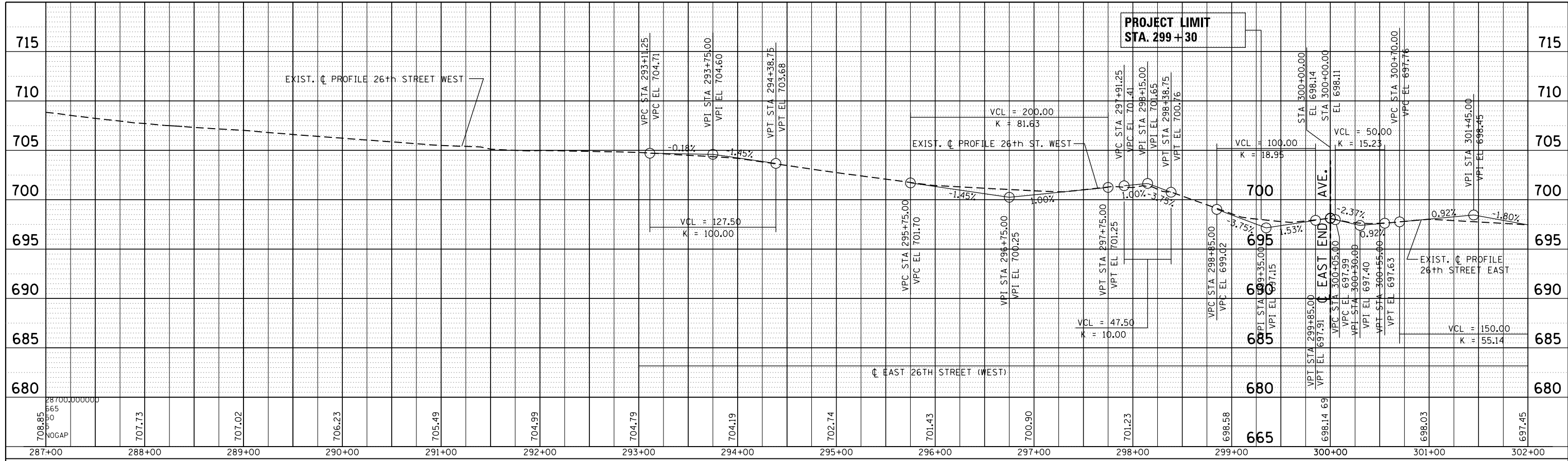


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| PROFILE | SURVEYED | BY | DATE |
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| | CARD FILE NAME | | |

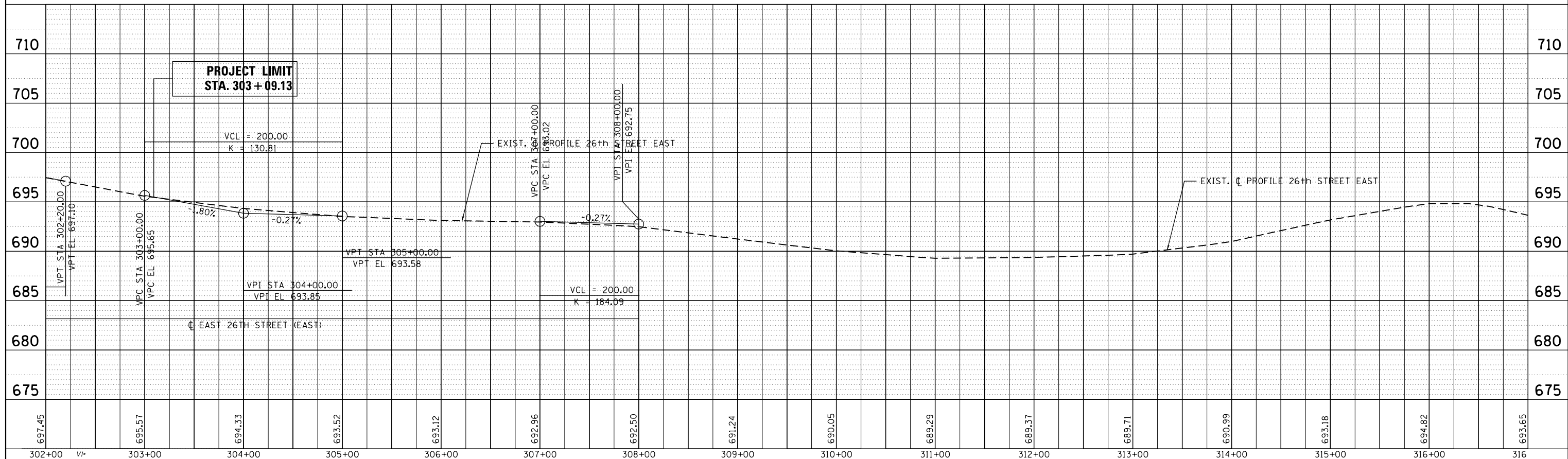


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| FILE NAME = | USER NAME = drivakosgn | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | EXISTING PROFILE EAST END AVENUE (26th ST. - 30th ST.) | F.A.U. RT. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
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| PLOT SCALE = 100.0000' / 1" | | CHECKED - | REVISED - | | | CONTRACT NO. 60W80 | | | | | |
| Default | | DATE - | REVISED - | | | ILLINOIS FED. AID PROJECT | | | | | |
| | PLOT DATE = 10/17/2014 | DATE - | REVISED - | SCALE: | SHEET OF SHEETS | STA. TO STA. | | | | | |

| | | | |
|------|-----------------------------|----|------|
| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
| | ALIGNMENT CHECKED | | |
| | STRUCTURE NOTATIONS CHECKED | | |
| | NOTE BOOK NO. | | |
| | CADD FILE NAME | | |

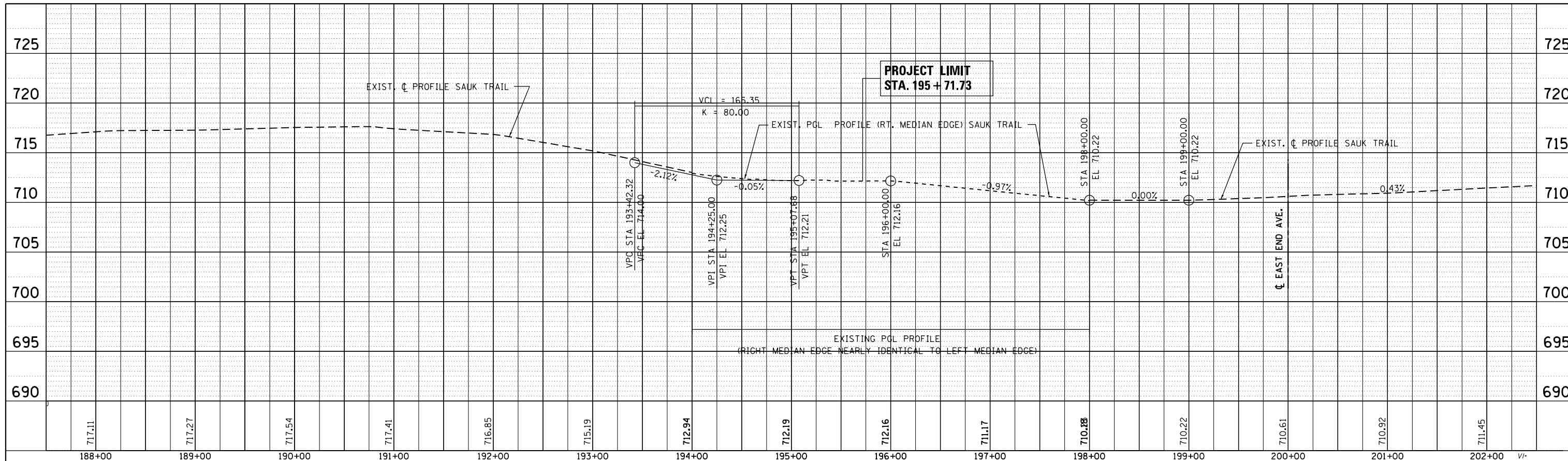


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|---------|-----------------------------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
| | ALIGNMENT CHECKED | | |
| | STRUCTURE NOTATIONS CHECKED | | |
| | NOTE BOOK NO. | | |
| | CADD FILE NAME | | |

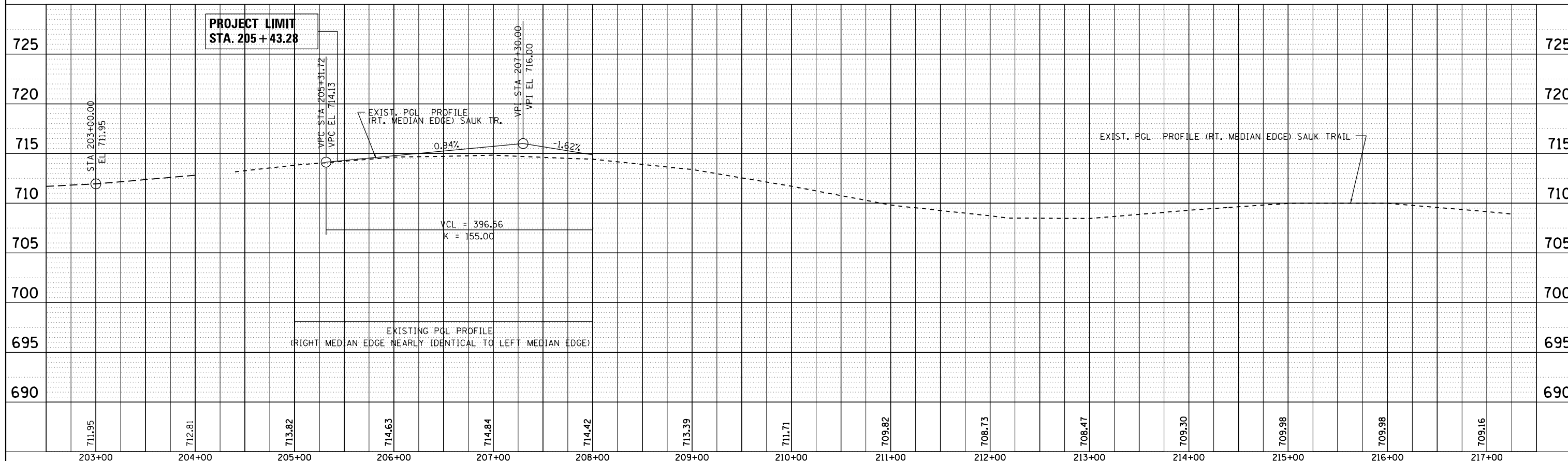


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| FILE NAME = | USER NAME = drivakosgn | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | EXISTING PROFILE 26TH ST. AT EAST END AVENUE | | | F.A.U. RT. = | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | | | | |
| c:\pwork\pwork\drivakosgn\0271285\PI34811-sht-profile.dgn | | DRAWN - | REVISED - | | SCALE: | SHEET | OF | SHEETS | STA. | TO | STA. | 2887 | 3155-N(13) | COOK | 88 | 24 |
| PLOT SCALE = 100.0000' / in. | | CHECKED - | REVISED - | | | | | | | | | CONTRACT NO. 60W80 | | | | |
| Default | | DATE - | REVISED - | | | | | | | | | ILLINOIS FED. AID PROJECT | | | | |

| | | | |
|------|-----------------------------|----|------|
| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
| | ALIGNMENT CHECKED | | |
| | STRUCTURE NOTATIONS CHECKED | | |
| | NOTE BOOK NO. | | |
| | CARD FILE NAME | | |



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|---------|-----------------------------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| | PLOTTED | | |
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| | NOTE BOOK NO. | | |
| | CARD FILE NAME | | |



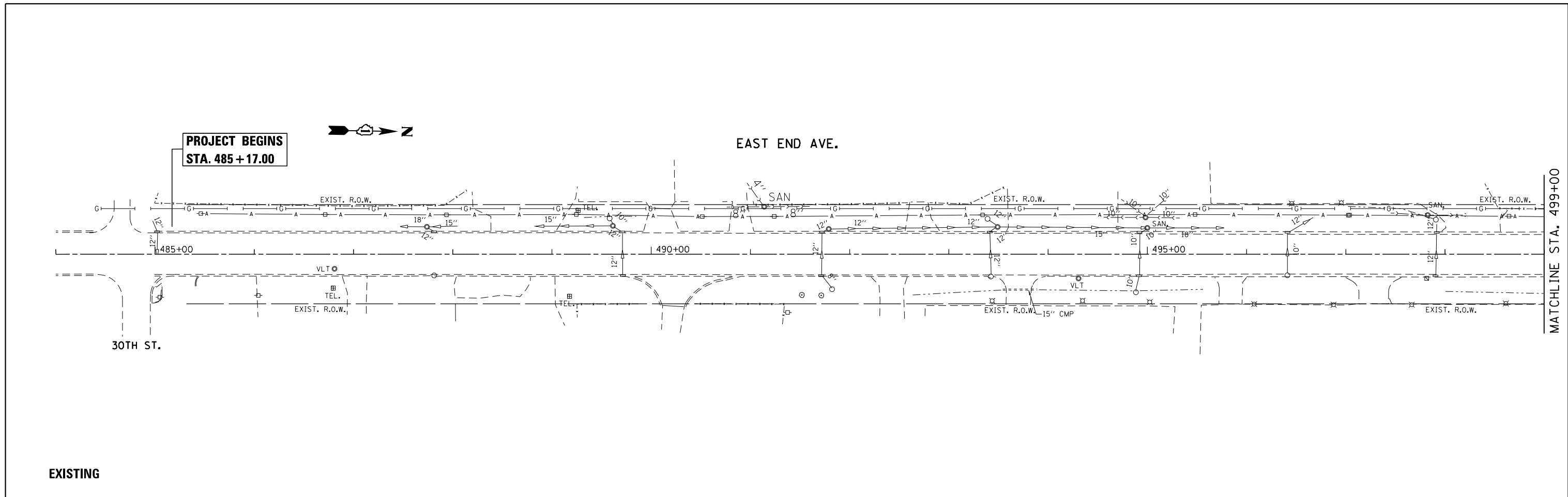
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| | | CHECKED - | REVISED - |
| | | DATE - | REVISED - |
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

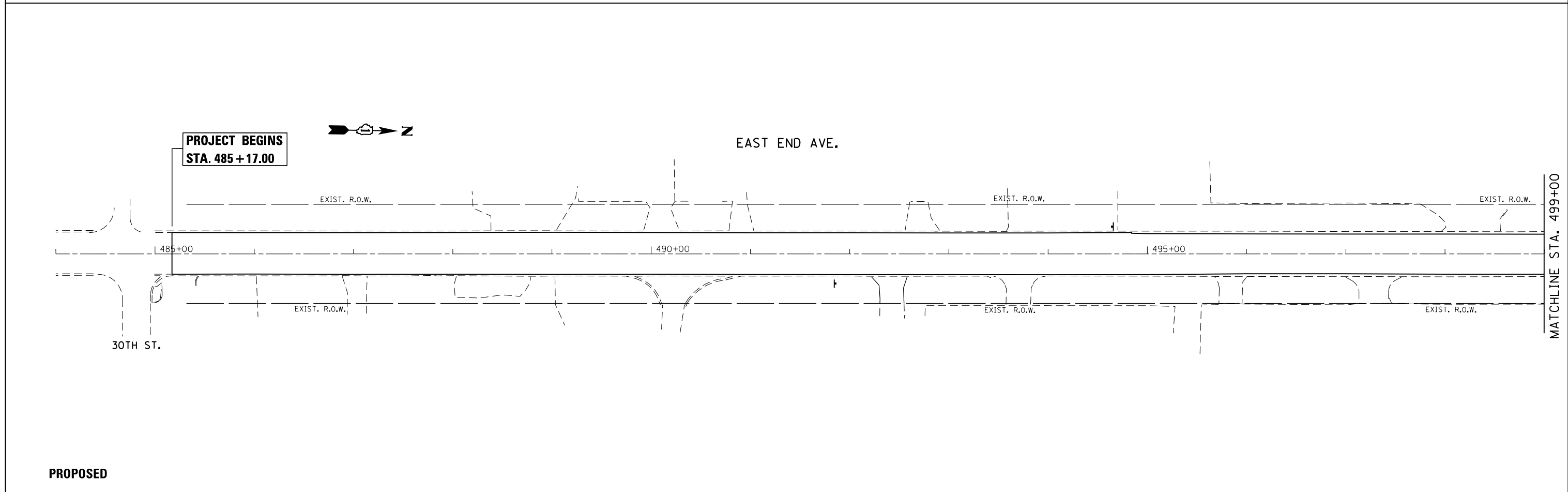
**EXISTING PROFILE
SAUK TRAIL AT EAST END AVENUE**

SCALE: SHEET OF SHEETS STA. TO STA.

| | | | | |
|---------------------------|------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2887 | 3155-N(13) | COOK | 88 | 25 |
| CONTRACT NO. 60W80 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



EXISTING



PROPOSED

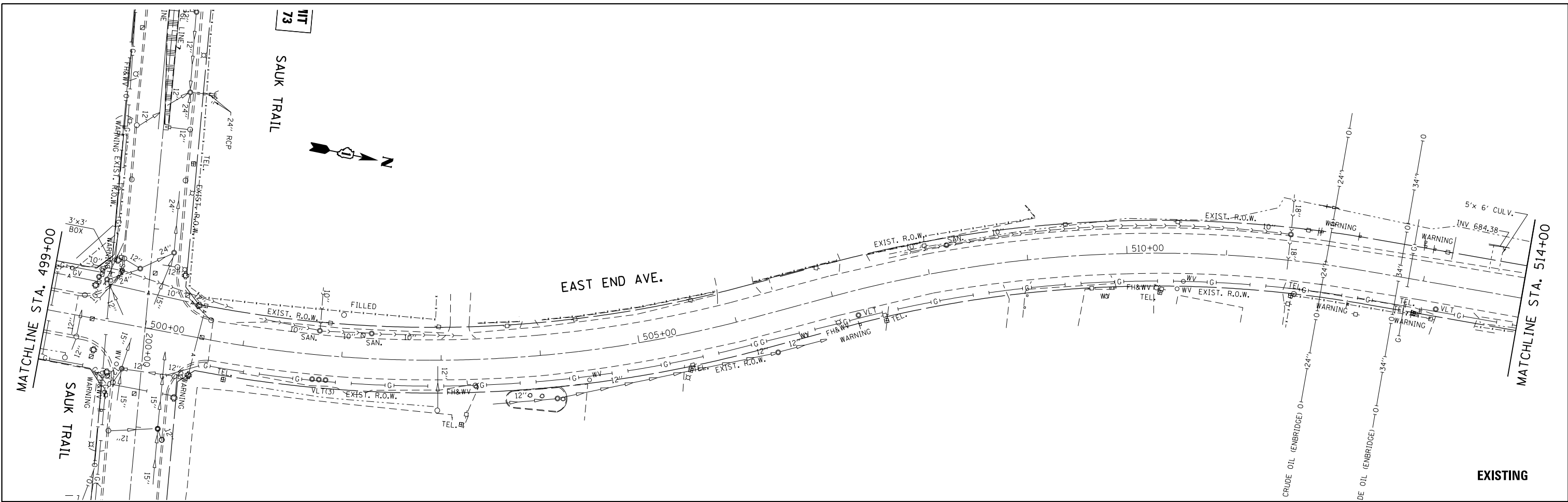
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| | PLOT SCALE = 100.0000' / in. | CHECKED - | REVISED - |
| Default | PLOT DATE = 10/17/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

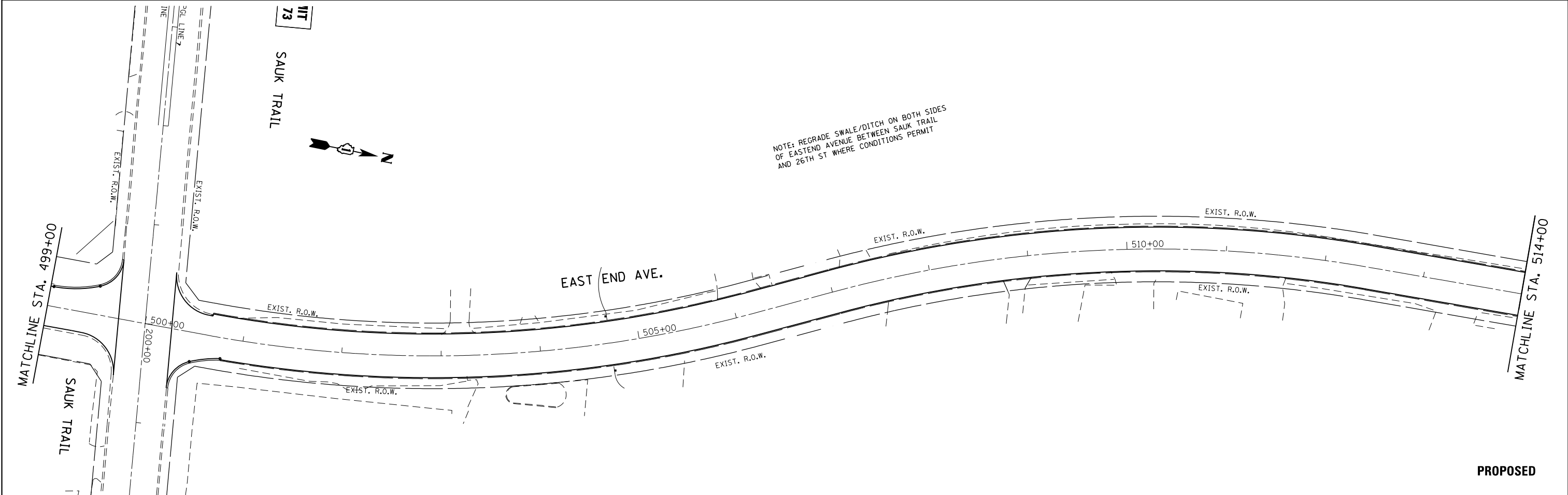
**EXISTING AND PROPOSED DRAINAGE AND UTILITIES PLAN
EAST END AVENUE (26th ST. - 30th ST.)**

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|------------|--------|--------------|-----------|
| 2887 | 3155-N(13) | COOK | 88 | 26 |
| CONTRACT NO. 60W80 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

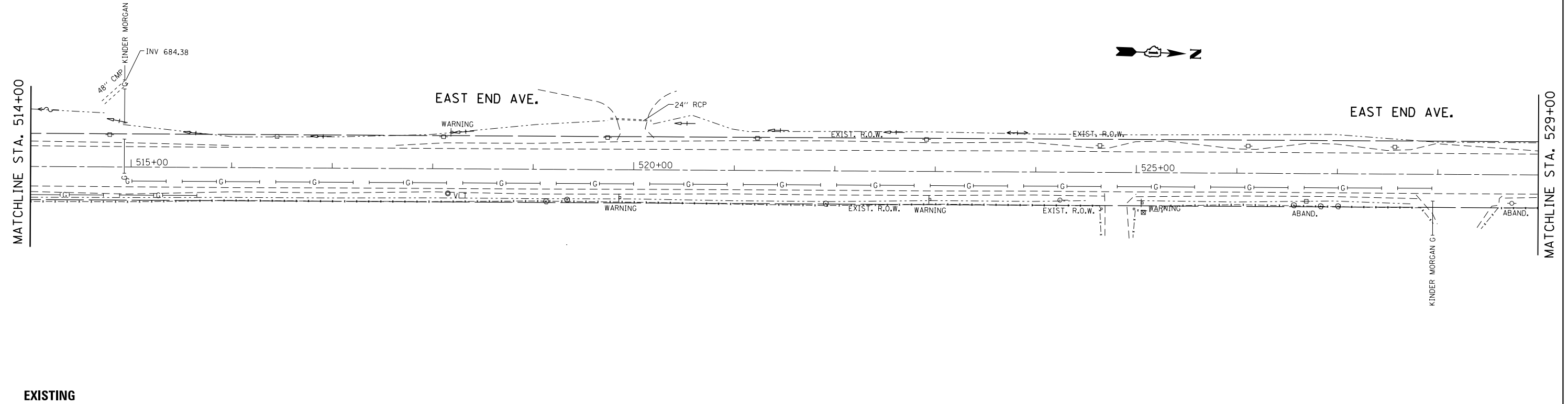


EXISTING

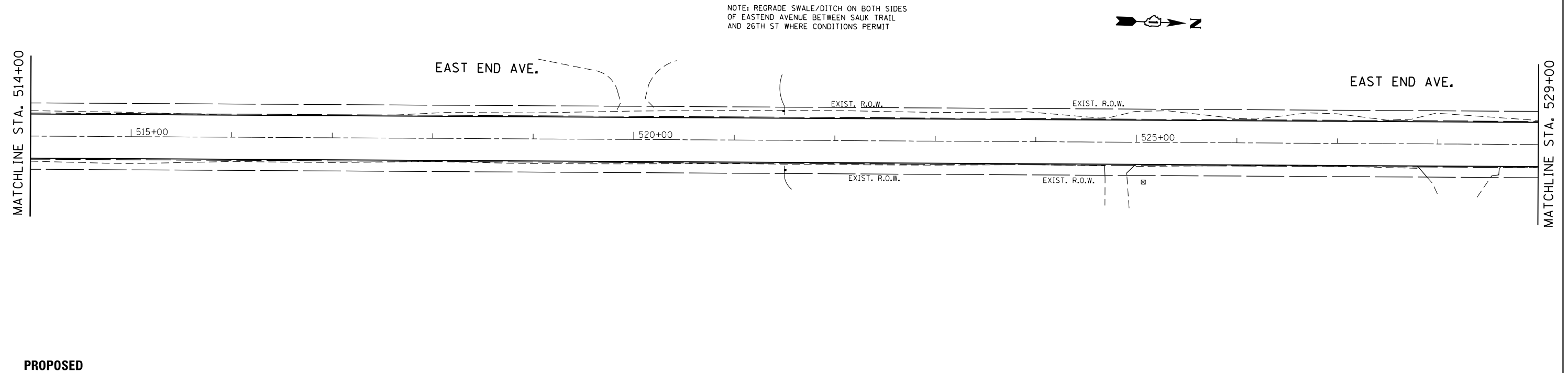


PROPOSED

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| FILE NAME = | USER NAME = drivakosgn | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | EXISTING AND PROPOSED DRAINAGE AND UTILITIES PLAN EAST END AVENUE (26th ST. - 30th ST.) | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| ei:\pw_work\p\dot\drivakosgn\d0271285\134811-sh1-drain.dgn | DRAWN - | REVISED - | 2887 | | | 3155-N(13) | COOK | 88 | 27 | |
| PLOT SCALE = 100.0000' / in. | CHECKED - | REVISED - | CONTRACT NO. 60W80 | | | | | | | |
| Default | DATE - | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | | |



EXISTING



PROPOSED

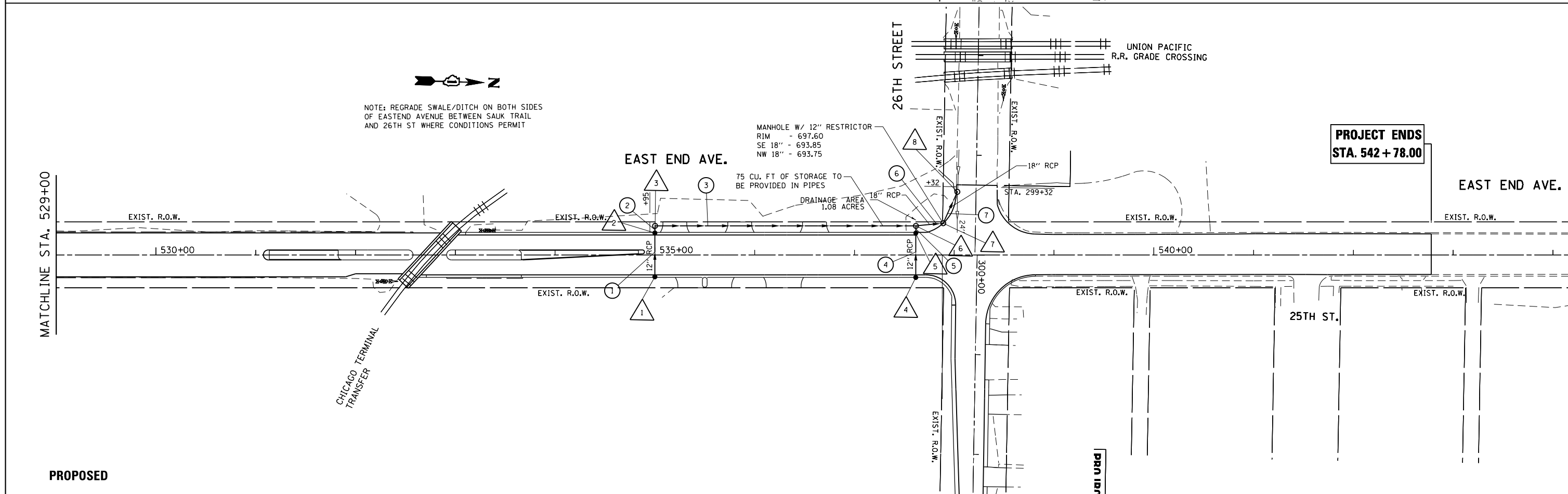
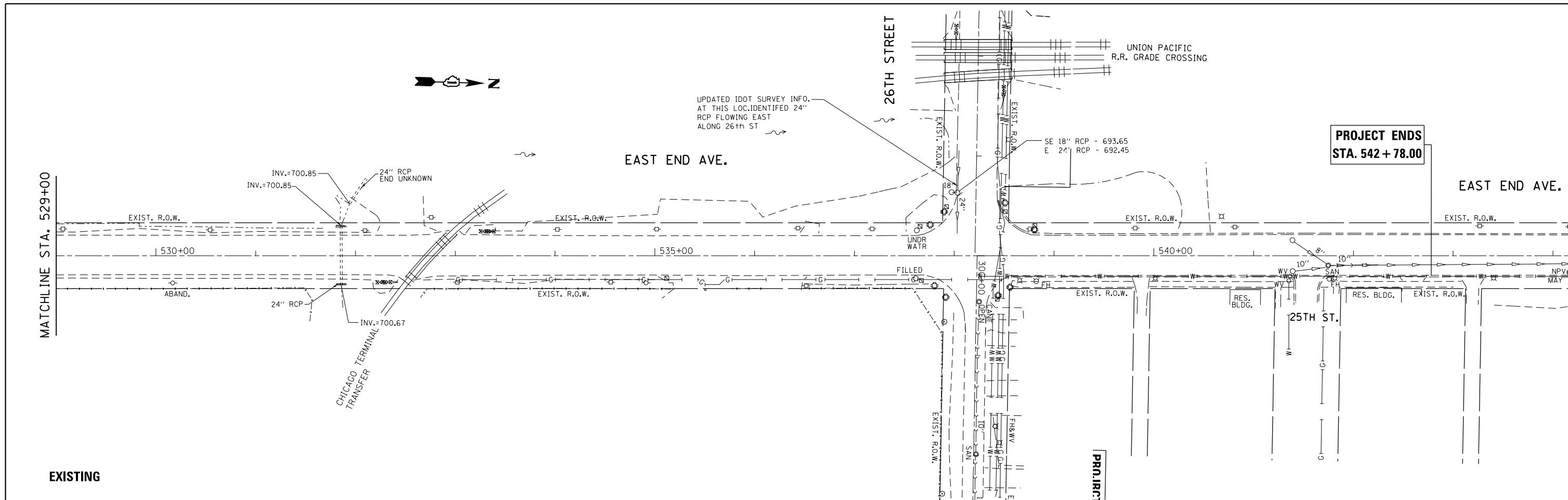
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| | PLOT SCALE = 100.0000' / in. | CHECKED - | REVISED - |
| Default | PLOT DATE = 10/17/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

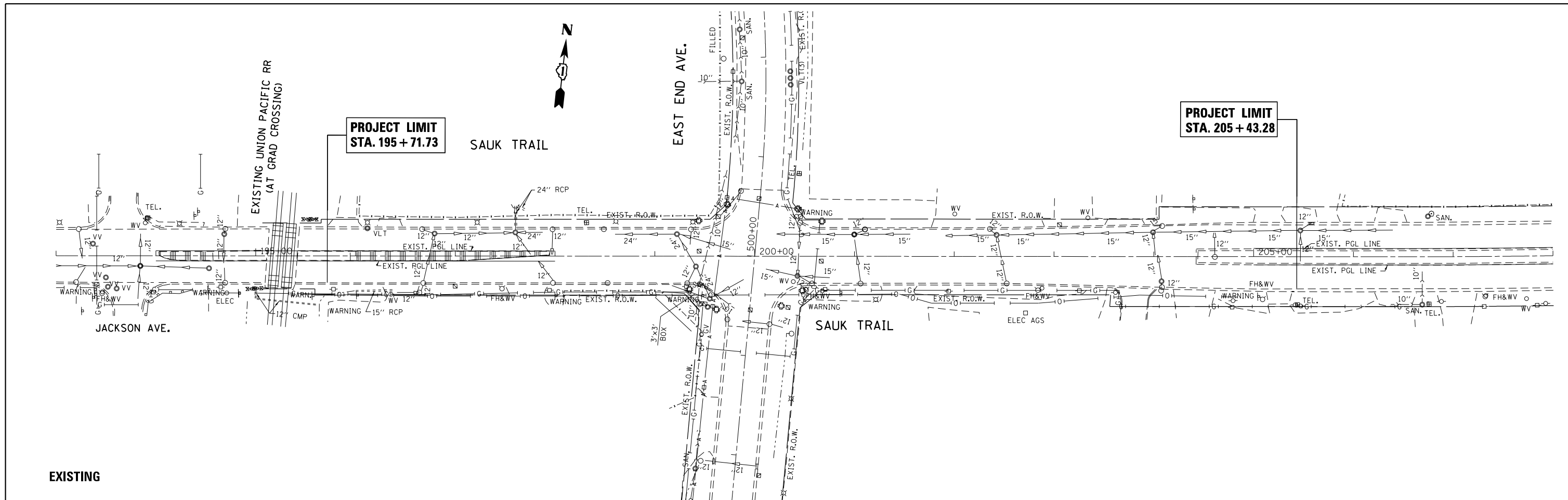
**EXISTING AND PROPOSED DRAINAGE AND UTILITIES PLAN
EAST END AVENUE (26th ST. - 30th ST.)**

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

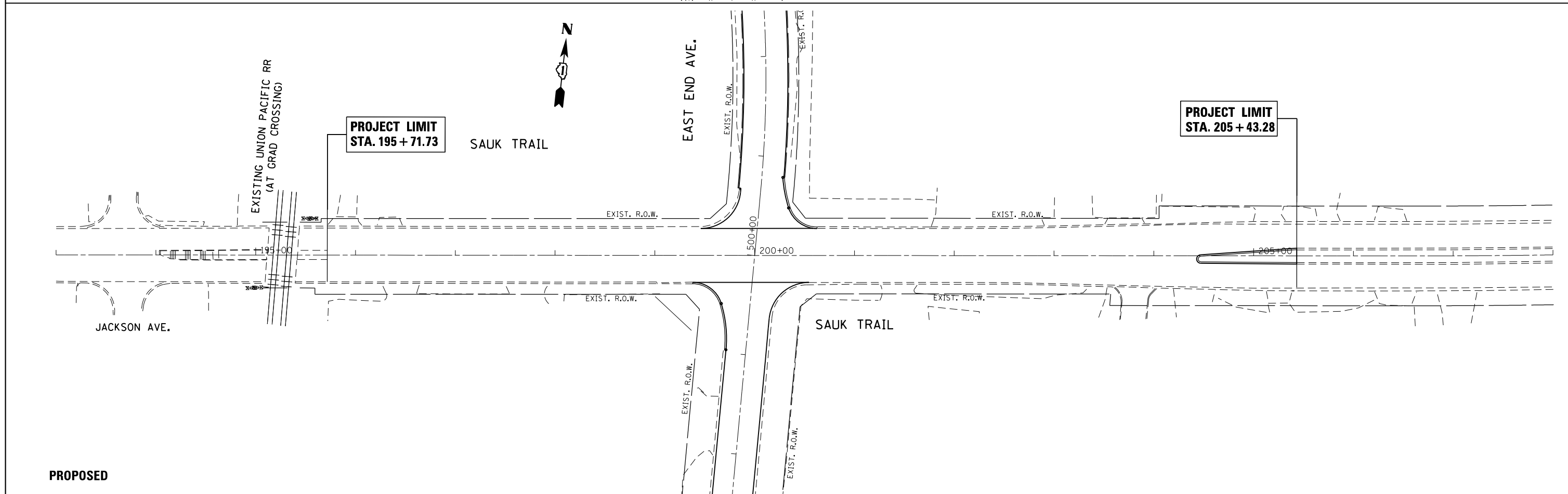
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|------------|--------|--------------|-----------|
| 2887 | 3155-N(13) | COOK | 88 | 28 |
| CONTRACT NO. 60W80 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



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| FILE NAME = | USER NAME = drivakosgn | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | EXISTING AND PROPOSED DRAINAGE AND UTILITIES PLAN EAST END AVENUE (26th ST. - 30th ST.) | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| ei:\pw\work\p\idot\drivakosgn\d0271285\PI34811-shit-drain.dgn | | DRAWN - | REVISED - | | | 2887 | 3155-N(13) | COOK | 88 | 29 | |
| PLOT SCALE = 100.0000' / in. | | CHECKED - | REVISED - | | | CONTRACT NO. 60W80 | | | | | |
| Default | | DATE - | REVISED - | | | ILLINOIS FED. AID PROJECT | | | | | |



EXISTING



PROPOSED

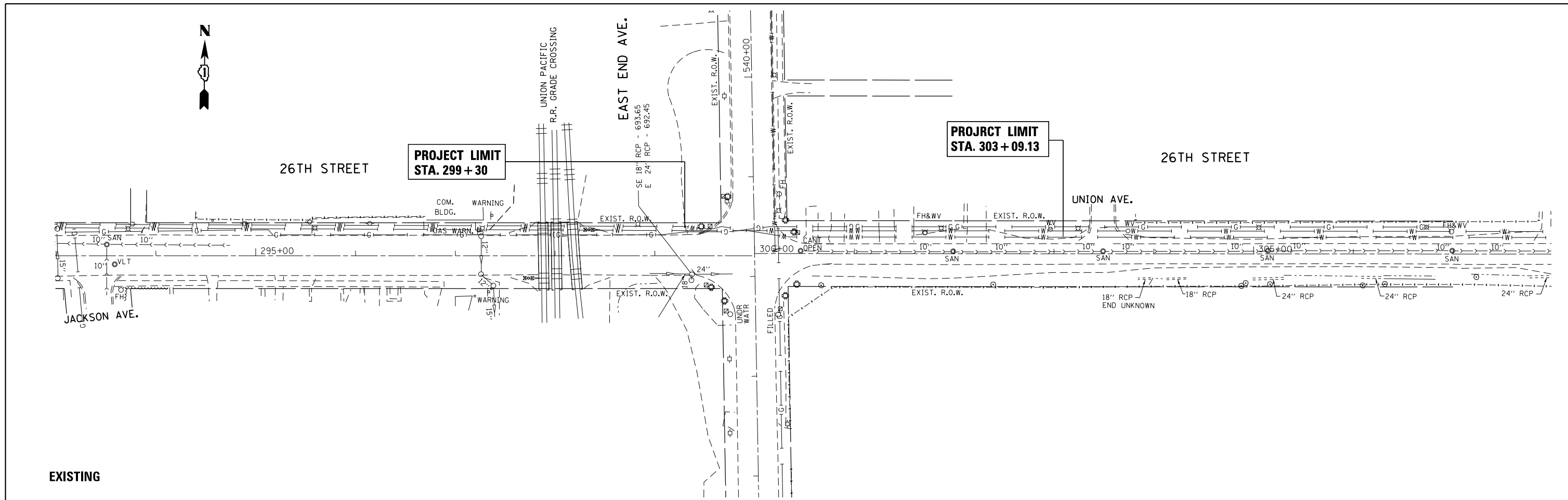
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| | PLOT SCALE = 100.0000' / in. | CHECKED - | REVISED - |
| Default | PLOT DATE = 10/17/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

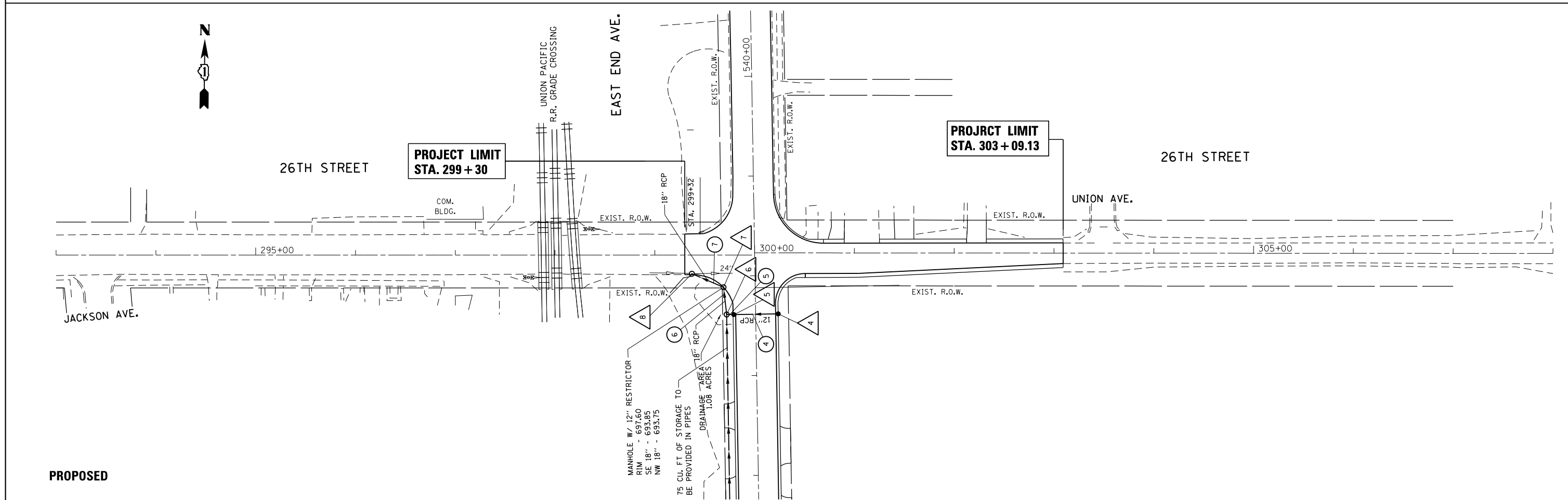
**EXISTING AND PROPOSED DRAINAGE AND UTILITIES PLAN
SAUK TRAIL AT EAST END AVENUE**

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

| | | | | |
|---------------------------|------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2887 | 3155-N(13) | COOK | 88 | 30 |
| CONTRACT NO. 60W80 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



EXISTING



PROPOSED

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|-------------|------------------------|------------|-----------|-----------------------------------------------------------|-----------------------------------------------------------------------------------------------------------------|---------------------------|------------|--------|--------------|-----------|--|
| FILE NAME = | USER NAME = drivakosgn | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | EXISTING AND PROPOSED DRAINAGE AND UTILITIES PLAN 26TH STREET FROM STA. 299 + 30 TO STA. 303 + 09.13 | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| Default | 10/17/2014 | DRAWN - | REVISED - | | | 2887 | 3155-N(13) | COOK | 88 | 31 | |
| | | CHECKED - | REVISED - | | | CONTRACT NO. 60W80 | | | | | |
| | | DATE - | REVISED - | | | ILLINOIS FED. AID PROJECT | | | | | |

1 CATCH BASIN TYPE "A", 4' DIA., TYPE 24 FRAME & GRATE, (EACH)
 STA. 535+00, (RT) TOP OF GRATE: 702.36
 T.G. 702.36
 INV. 698.36

2 CATCH BASIN TYPE "A", 4' DIA., TYPE 24 FRAME & GRATE, (EACH)
 STA. 535+00 (LT)
 T.G. 702.36
 INV. 698.16

3 M.H. TYPE "A", 4' DIA., TYPE 1 FRAME CLOSED LID, (EACH)
 STA. 535+00 (29' LT)
 T.G. 702.76
 INV. 695.05 (N), 698.16 (E)

4 CATCH BASIN TYPE "A", 6' DIA., TYPE 24 FRAME & GRATE, (EACH)
 STA. 537+61 (RT)
 T.G. 697.07
 INV. 695.7 (W)

5 CATCH BASIN TYPE "A", 4' DIA., TYPE 24 FRAME & GRATE, (EACH)
 STA. 537+62 (RT)
 T.G. 699.07
 INV. 694.87 (E)

6 M.H. TYPE "A", 5' DIA., TYPE 1 FRAME CLOSED LID, (EACH)
 STA. 537+61 (29' LT)
 T.G. 699.39
 INV. 693.95 (S)
 INV. 693.85 (N)
 INV. 693.95 (W)

7 M.H. TYPE "A", 6' DIA., TYPE 1 FRAME CLOSED LID, RESTRICTOR PLATE
 STA. 557+88 (32' LT)
 T.G. 697.60
 INV. 693.85 (SE)
 INV. 693.75 (NW)

8 M.H. TYPE "A", 5' DIA., TYPE 1 FRAME CLOSED LID, (EACH)
 STA. 578+02 (64' LT)
 T.G. 697.00
 INV. 693.65 (SE)
 INV. 692.45 (E)

1 STORM SEWER CLASS "A", TYPE 2, 12" DIA.
 STA. 535+00
 42' AT 0.5%
 T.B. 13 CU. YD.

2 STORM SEWER CLASS "A", TYPE 2, 12" DIA.
 STA. 535+00
 7' AT 0.5%
 T.B. 2.5 CU. YD.

3 STORM SEWER CLASS "A", TYPE 2, 15" DIA.
 STA. 535+00 TO STA. 537+61
 • "75' CU. FT. STORAGE AT PIPE"
 261' AT 0.33%
 T.B. 266 CU. YD.

4 STORM SEWER CLASS "A", TYPE 2, 12" DIA.
 STA. 535+61
 42' AT 0.5%
 T.B. 13 CU. YD.

5 STORM SEWER CLASS "A", TYPE 2, 12" DIA.
 STA. 535+61
 7' AT 0.5%
 T.B. 2.5 CU. YD.

6 STORM SEWER CLASS "A", TYPE 2, 18" DIA.
 STA. 535+61 TO STA. 535+88
 27' AT 0.33%
 T.B. 20.5 CU. YD.

7 STORM SEWER CLASS "A", TYPE 2, 18" DIA.
 STA. 535+88 TO STA. 536+00
 35' AT 0.29%
 T.B. 10.3 CU. YD.

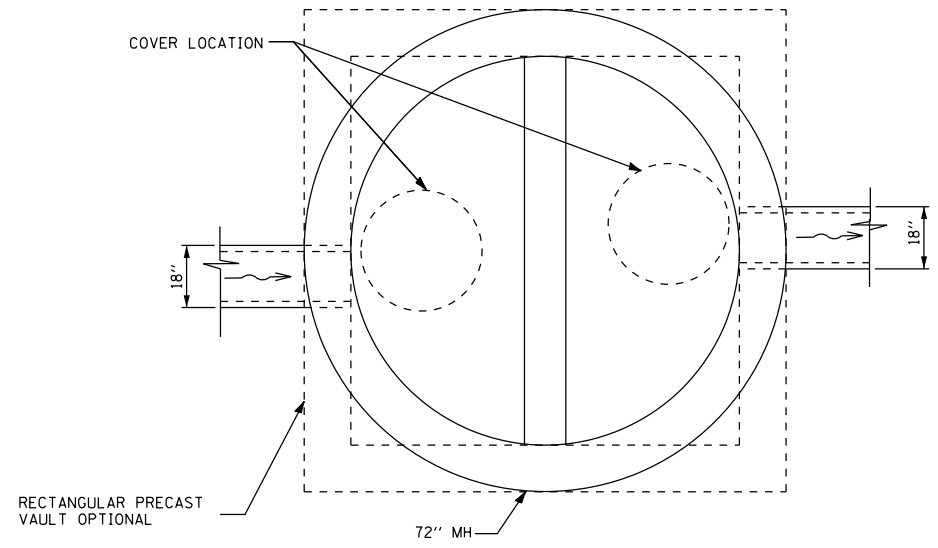
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| Default | PLOT SCALE = 100.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 10/17/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

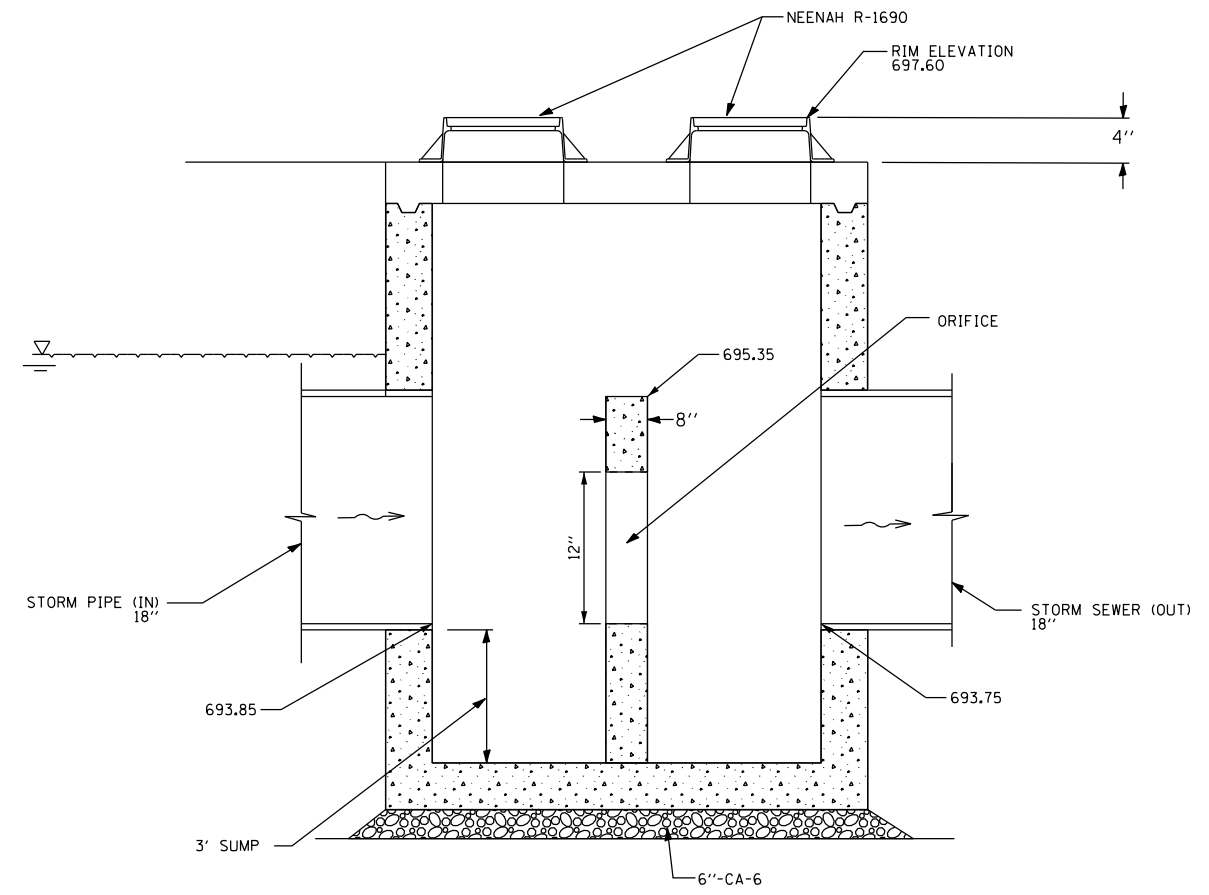
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|----------------------------------------------|-------|-----------|--------------|
| PIPES AND STRUCTURES TABLE | | | |
| EAST END AVENUE (26th ST. - 30th ST.) | | | |
| SCALE: | SHEET | OF SHEETS | STA. TO STA. |

| | | | | |
|---------------------------|------------|--------|---------------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2887 | 3155-N(13) | COOK | 88 | 32 |
| | | | CONTRACT NO. 60W80 | |
| ILLINOIS FED. AID PROJECT | | | | |

| STRUCTURE | RIM | INVERT IN | INVERT OUT | ORIFICE SIZE | ORIFICE INVERT | OVERFLOW WEIR ELEV. |
|-----------|--------|-----------|------------|--------------|----------------|---------------------|
| CS1 | 595.10 | 587.63 | 587.63 | 42" | 587.92 | |
| CS2 | 596.59 | 587.92 | 587.92 | 36" | 587.92 | |
| CS3 | 596.40 | 588.24 | 588.24 | 24" | 588.24 | |



PLAN VIEW



**SECTION A-A
TYPICAL CONTROL STRUCTURE**

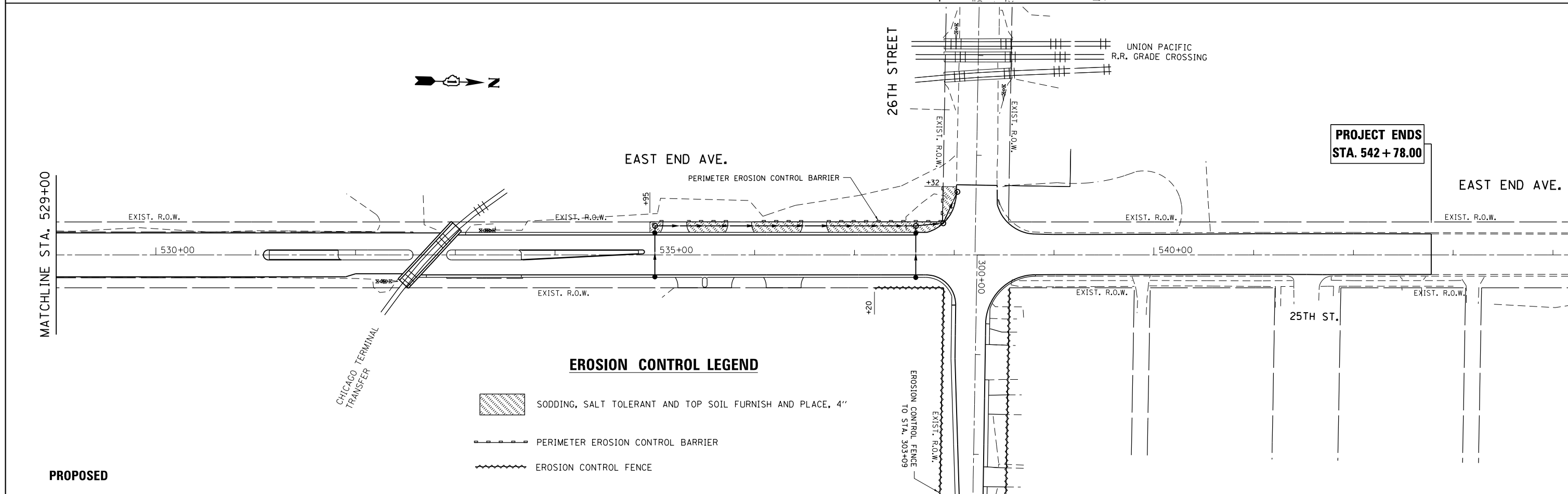
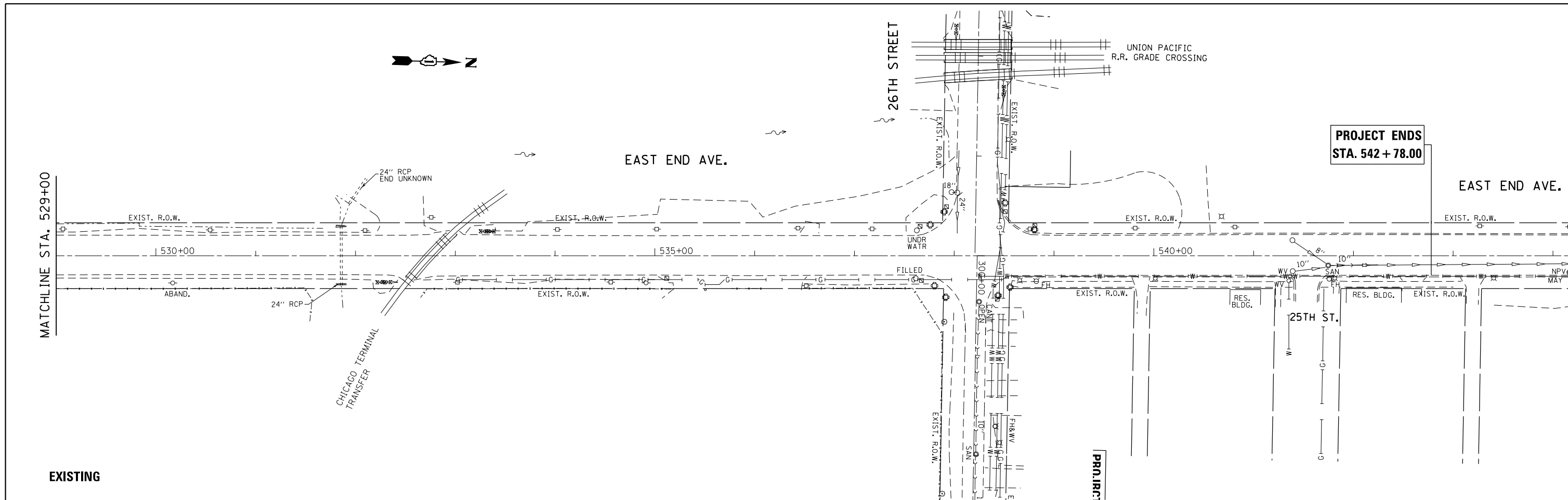
EAST END AVENUE
PROPOSED RESTRICTOR DETAIL

| | | | |
|---------------------------------------------------------------|------------------------------|------------|-----------|
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| c:\pwork\work\pwork\drivakosgn\d0271285\PI34811-shr-drain.dgn | | DRAWN - | REVISED - |
| Default | PLOT SCALE = 100.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 10/17/2014 | DATE - | REVISED - |




STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | |
|---------------------------------------|-------|-----------|--------------|
| PROPOSED RESTRICTOR DETAIL | | | |
| EAST END AVENUE (26th ST. - 30th ST.) | | | |
| SCALE: | SHEET | OF SHEETS | STA. TO STA. |

| | | | | |
|---------------------------|------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2887 | 3155-N(13) | COOK | 88 | 33 |
| CONTRACT NO. 60W80 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



EROSION CONTROL LEGEND

-  SODDING, SALT TOLERANT AND TOP SOIL FURNISH AND PLACE, 4"
-  PERIMETER EROSION CONTROL BARRIER
-  EROSION CONTROL FENCE

| | | | |
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| FILE NAME = | USER NAME = drivakosgn | DESIGNED - | REVISED - |
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| Default | PLOT SCALE = 100.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 10/17/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED EROSION CONTROL PLAN
EAST END AVENUE (26th ST. - 30th ST.)**

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

| | | | | |
|---------------------------|------------|--------|--------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2887 | 3155-N(13) | COOK | 88 | 34 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 60W80 | |

TRAFFIC SIGNAL LEGEND

| ITEM | REMOVAL | EXISTING | PROPOSED | ITEM | REMOVAL | EXISTING | PROPOSED | ITEM | REMOVAL | EXISTING | PROPOSED |
|---------------------------------------------------------------------------|---------|----------|----------|---------------------------------------------------------------------------------------------------------------------|---------|----------|----------|------------------------------------------------------------------------------------------|---------|----------|----------|
| CONTROLLER CABINET | | | | EMERGENCY VEHICLE LIGHT DETECTOR | | | | ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE | | | |
| RAILROAD CONTROL CABINET | | | | CONFIRMATION BEACON | | | | COAXIAL CABLE | | | |
| COMMUNICATIONS CABINET | | | | HANDHOLE | | | | VENDOR CABLE FOR CAMERA | | | |
| MASTER CONTROLLER | | | | HEAVY DUTY HANDHOLE | | | | COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED | | | |
| MASTER MASTER CONTROLLER | | | | DOUBLE HANDHOLE | | | | FIBER OPTIC CABLE NO. 62.5/125, MM12F | | | |
| UNINTERRUPTIBLE POWER SUPPLY | | | | JUNCTION BOX | | | | FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F | | | |
| SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT | | | | UNDERGROUND CONDUIT, GALVANIZED STEEL (UC) | | | | FIBER OPTIC CABLE NO. 62.5/125, MM12F SM24F | | | |
| TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT | | | | TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE | | | | FIBER OPTIC CABLE NO. 62.5/125, MM12F SM24F | | | |
| STEEL MAST ARM ASSEMBLY AND POLE | | | | COMMON TRENCH | | | | GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE | | | |
| ALUMINUM MAST ARM ASSEMBLY AND POLE | | | | COILABLE NONMETALLIC CONDUIT (EMPTY) | | | | CONTROLLER CABINET AND FOUNDATION TO BE REMOVED | | | |
| STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE | | | | SYSTEM ITEM | | | | STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED | | | |
| STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA | | | | INTERSECTION ITEM | | | | ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED | | | |
| SIGNAL POST | | | | REMOVE ITEM | | | | STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED | | | |
| TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM | | | | RELOCATE ITEM | | | | SIGNAL POST AND FOUNDATION TO BE REMOVED | | | |
| GUY WIRE | | | | ABANDON ITEM | | | | INTERSECTION & SAMPLING (SYSTEM) DETECTOR | | | |
| SIGNAL HEAD | | | | 12" (300mm) TRAFFIC SIGNAL SECTION | | | | SAMPLING (SYSTEM) DETECTOR | | | |
| SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE) | | | | 12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE | | | | QUEUE DETECTOR | | | |
| SIGNAL HEAD WITH BACKPLATE | | | | SIGNAL FACE | | | | PERFORMED QUEUE DETECTOR | | | |
| SIGNAL HEAD OPTICALLY PROGRAMMED | | | | SIGNAL FACE WITH BACKPLATE, "P" INDICATES PROGRAMMED HEAD | | | | PERFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR | | | |
| FLASHER INSTALLATION (S DENOTES SOLAR POWER) | | | | "RB" INDICATES REFLECTIVE BACKPLATE | | | | PERFORMED SAMPLING (SYSTEM) DETECTOR | | | |
| PEDESTRIAN SIGNAL HEAD | | | | 12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL | | | | | | | |
| PEDESTRIAN PUSHBUTTON DETECTOR | | | | 12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED | | | | | | | |
| ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR | | | | 12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID | | | | | | | |
| ILLUMINATED SIGN "NO LEFT TURN" | | | | PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER | | | | | | | |
| ILLUMINATED SIGN "NO RIGHT TURN" | | | | RADIO INTERCONNECT | | | | | | | |
| DETECTOR LOOP, TYPE I | | | | RADIO REPEATER | | | | | | | |
| PERFORMED DETECTOR LOOP | | | | DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED | | | | | | | |
| MICROWAVE VEHICLE SENSOR | | | | GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN) | | | | | | | |
| VIDEO DETECTION CAMERA | | | | | | | | | | | |
| VIDEO DETECTION ZONE | | | | | | | | | | | |
| PAN, TILT, ZOOM CAMERA | | | | | | | | | | | |
| WIRELESS DETECTOR SENSOR | | | | | | | | | | | |
| WIRELESS ACCESS POINT | | | | | | | | | | | |

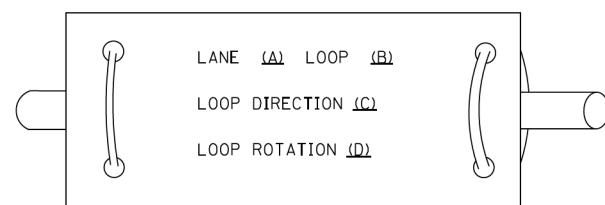
RAILROAD SYMBOLS

| | EXISTING | PROPOSED |
|------------------------------|----------|----------|
| RAILROAD CONTROL CABINET | | |
| RAILROAD CANTILEVER MAST ARM | | |
| FLASHING SIGNAL | | |
| CROSSING GATE | | |
| CROSSBUCK | | |

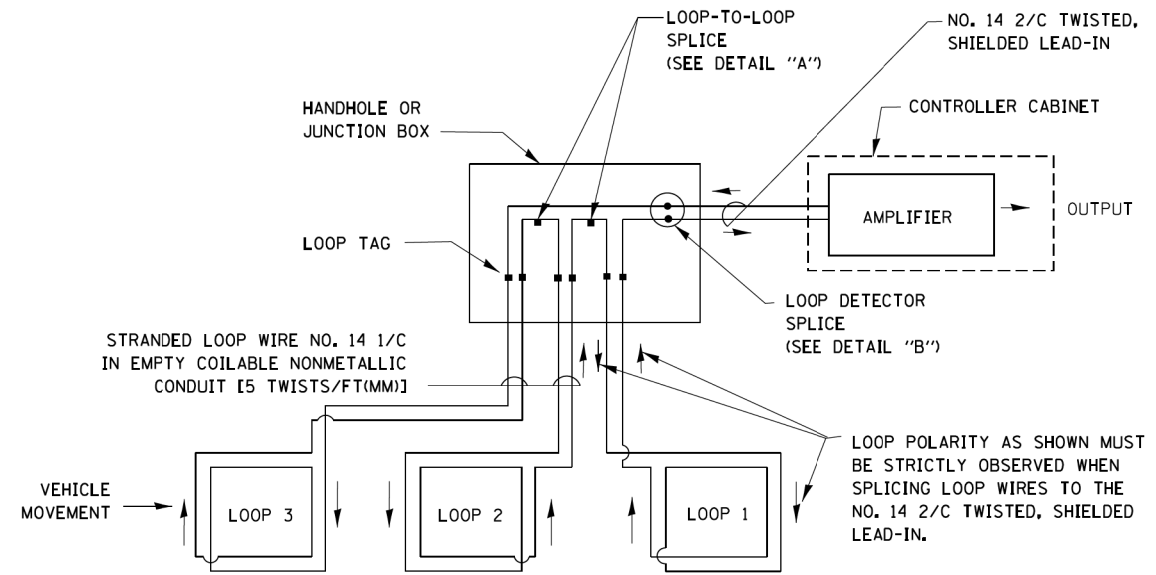
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

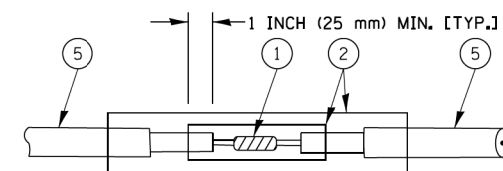


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

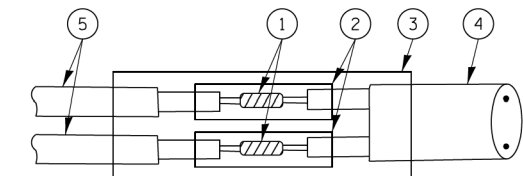


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

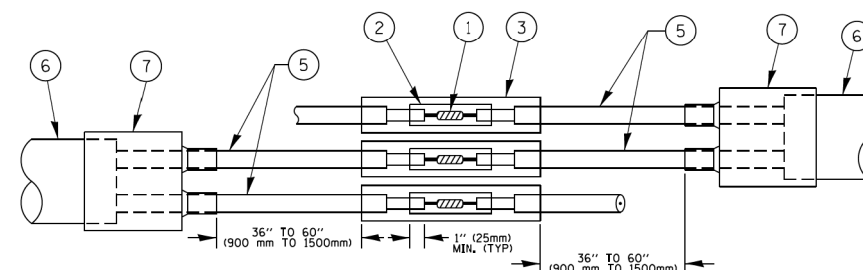


DETAIL "A"
LOOP-TO-LOOP SPLICE

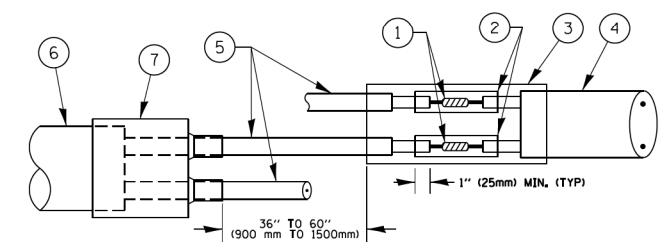


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

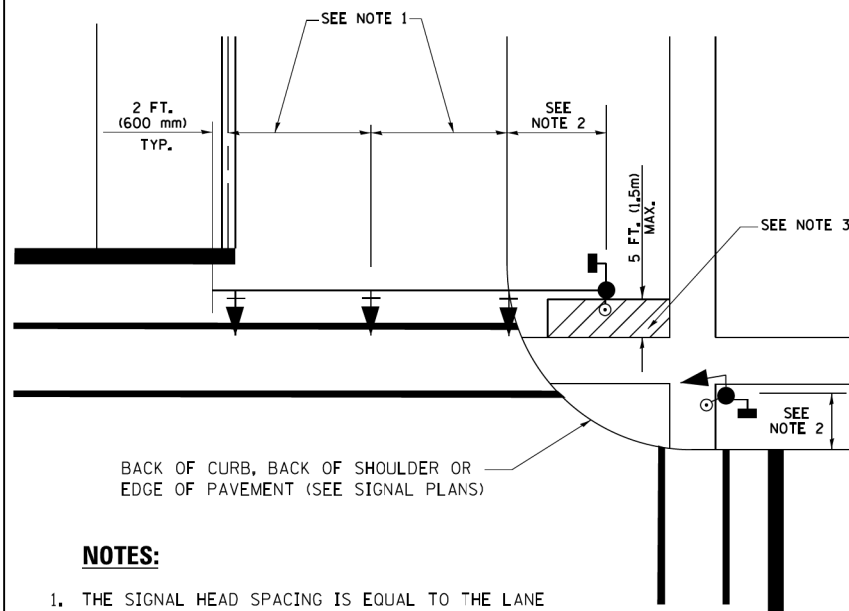
PREFORMED LOOP

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PREFORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

| | | | | | | | | | | | | |
|-------------------------------------------|---------------------|-----------------|----------------------|-----------------------------------------------------------|----------------------------------------------------------------|-------------------------|--------------|------------------|--------------------|--------------------|-----------------|--------------|
| FILE NAME = | USER NAME = footemj | DESIGNED - DAD | REVISED - DAG 1-1-14 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS | | | F.A.U. RTE. 2887 | SECTION 3155-N(13) | COUNTY COOK | TOTAL SHEETS 88 | SHEET NO. 36 |
| ca:\pwork\pwork\footemj\d0108315\ts05.dgn | | DRAWN - BCK | REVISED - | | SCALE: NONE | SHEET NO. 2 OF 7 SHEETS | STA. TO STA. | TS-05 | | CONTRACT NO. 60W80 | | |
| | | CHECKED - DAD | REVISED - | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | | | | |
| | | DATE - 10-28-09 | REVISED - | | | | | | | | | |

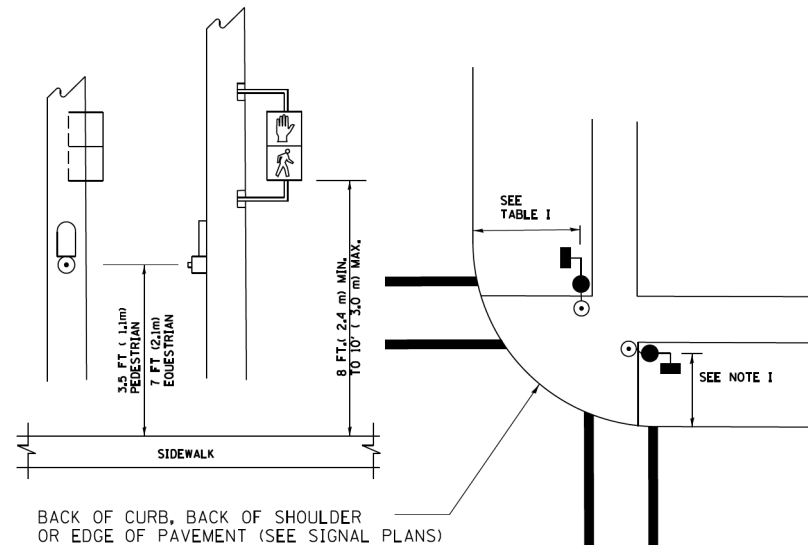
**TRAFFIC SIGNAL MAST ARM AND SIGNAL POST
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR
FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN
WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.**



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

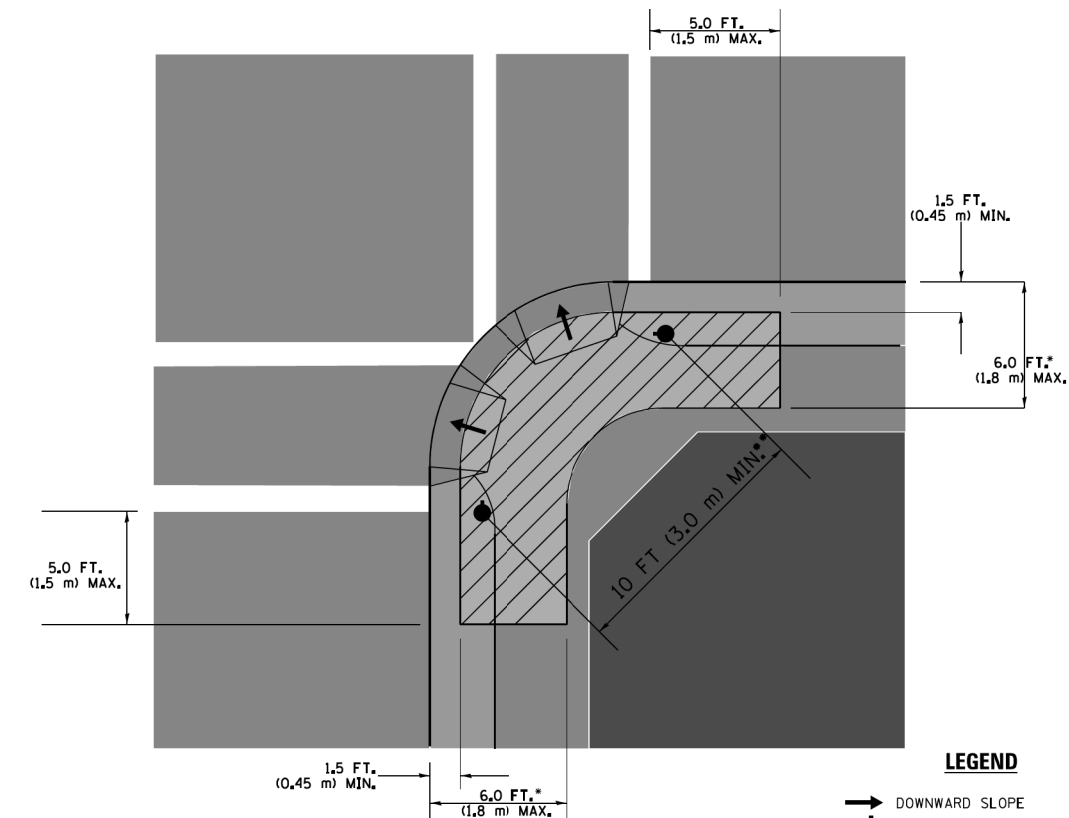
**PEDESTRIAN SIGNAL POST
AND
PEDESTRIAN PUSH BUTTON POST**



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



LEGEND

- DOWNWARD SLOPE
- PEDESTRIAN PUSHBUTTON
- ▨ RECOMMENDED PUSHBUTTON LOCATIONS

- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

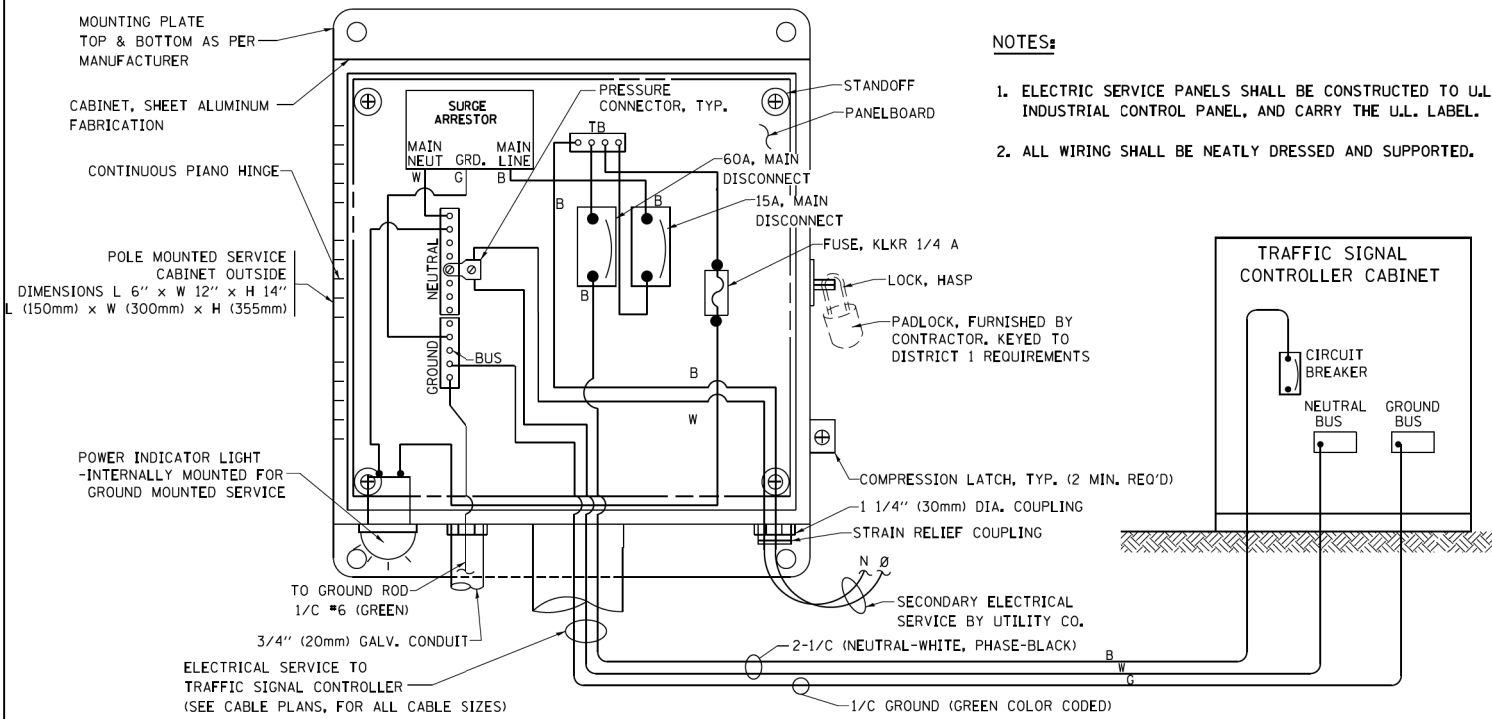
1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

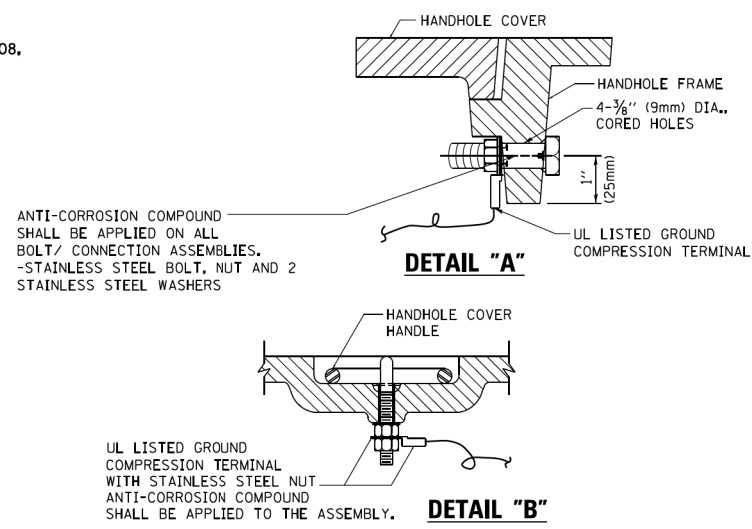
| TRAFFIC SIGNAL EQUIPMENT | COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION) | SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION) |
|------------------------------------|-------------------------------------------------------------------------------------------------------|-----------------------------------------------------------------------------------------------|
| TRAFFIC SIGNAL MAST ARM POLE | 6 FT (1.8m) | SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m) |
| TRAFFIC SIGNAL POST | 4 FT (1.2m) | SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m) |
| PEDESTRIAN SIGNAL POST | 4 FT (1.2m) | SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m) |
| PEDESTRIAN PUSHBUTTON POST | 4 FT (1.2m) | SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m) |
| TEMPORARY WOOD POLE | 6 FT (1.8m) | SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m) |
| CONTROLLER CABINET | 6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2 | SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3. |
| SERVICE INSTALLATION, GROUND MOUNT | 6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2 | SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3. |

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

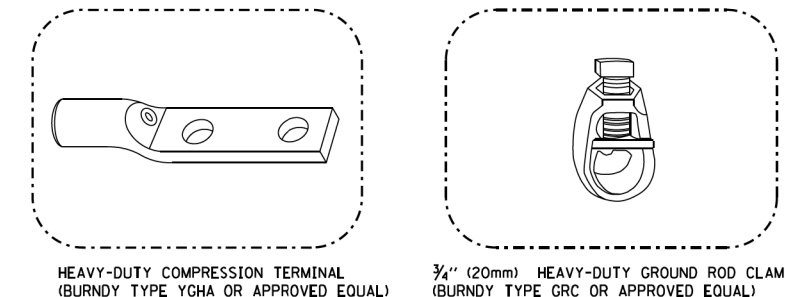
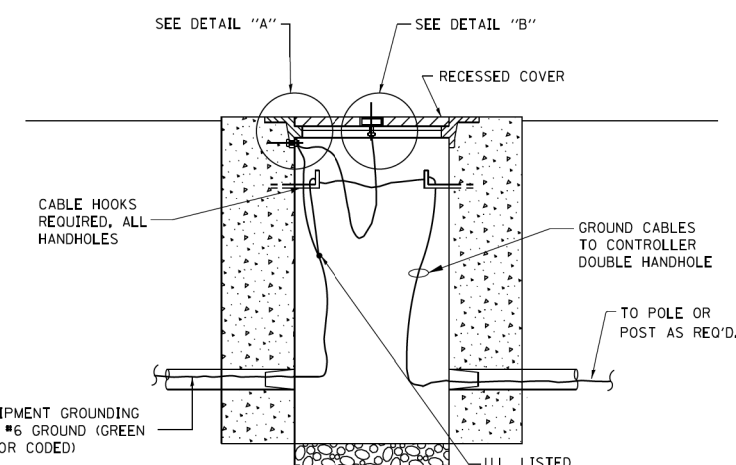


**ELECTRICAL SERVICE – PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
(NOT TO SCALE)**

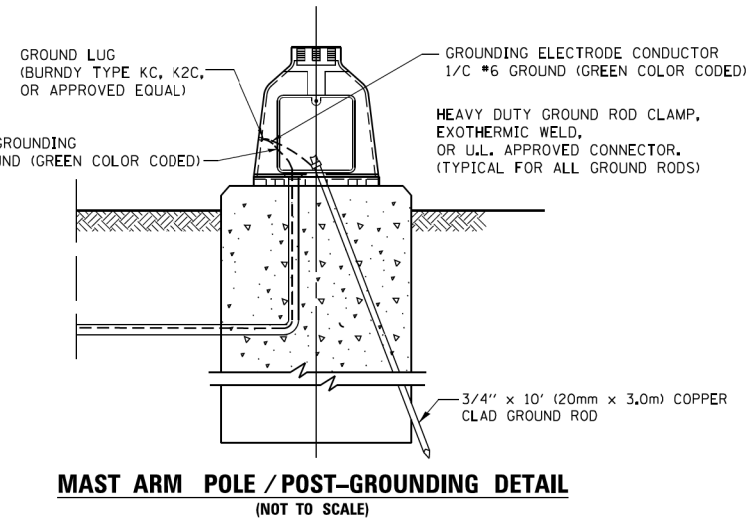
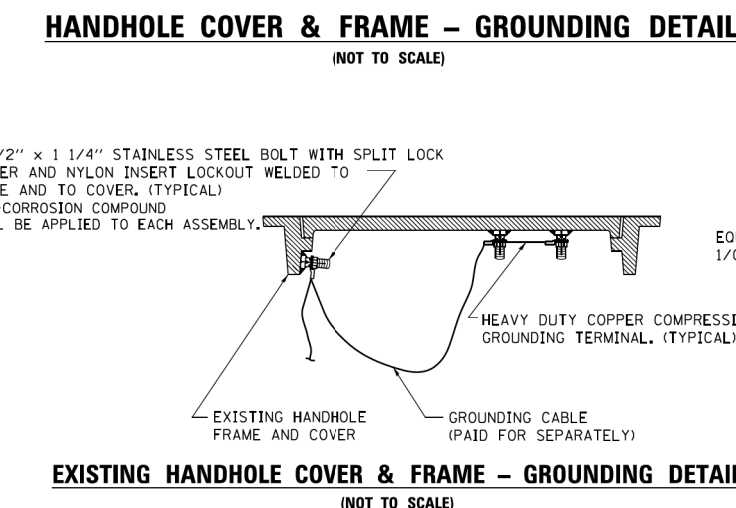
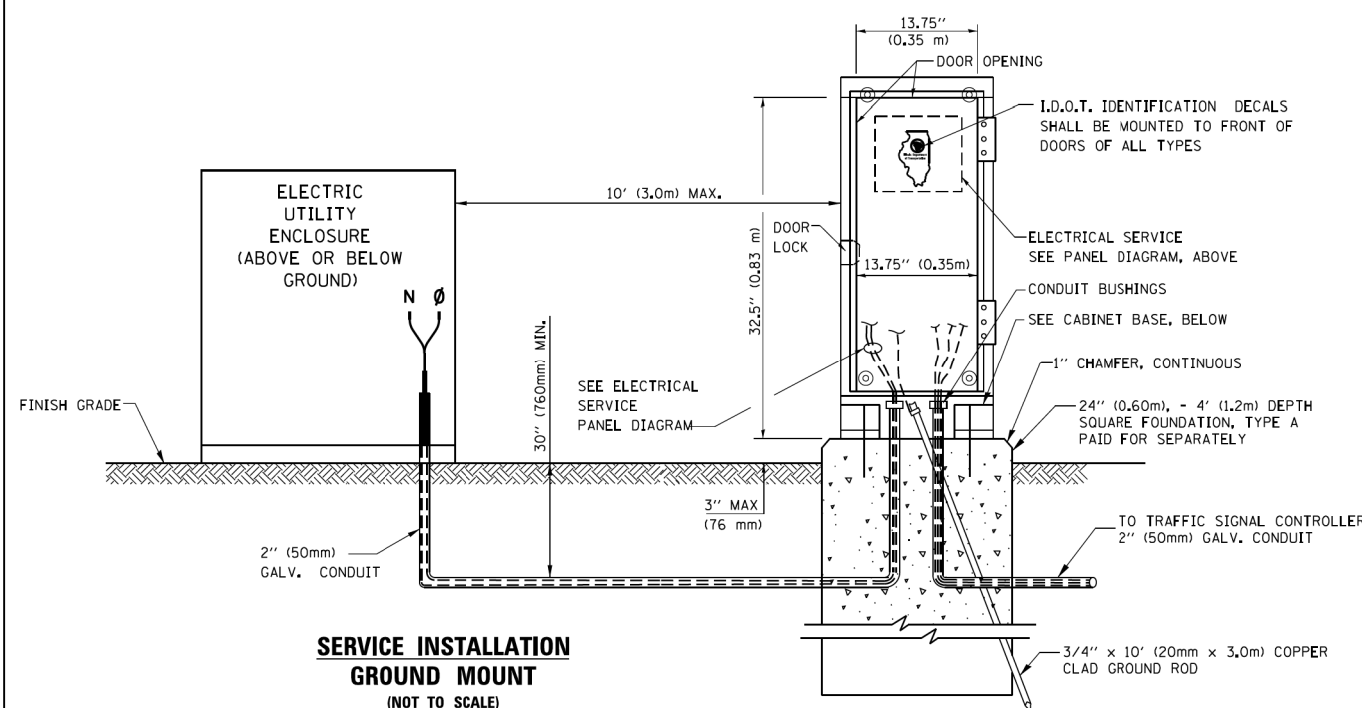


**NOTES:
GROUNDING SYSTEM**

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD, ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.

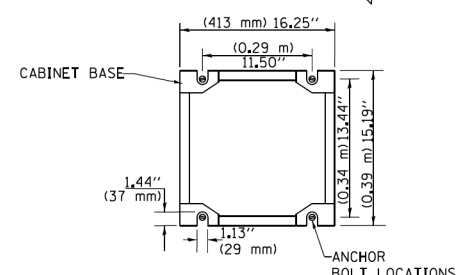


- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
 - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.

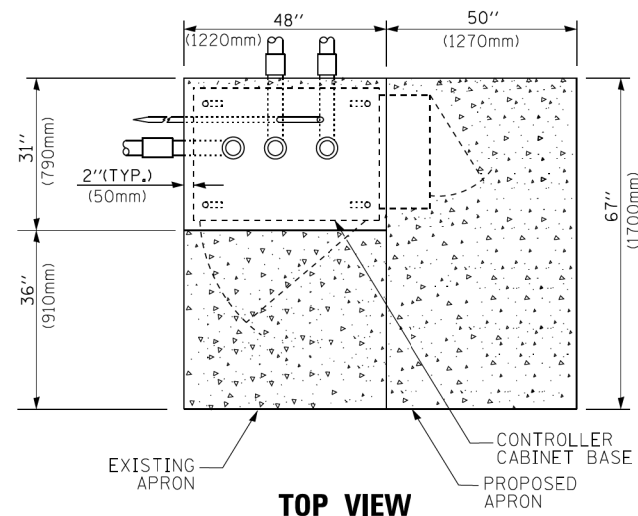


**SERVICE INSTALLATION GROUND MOUNT
(NOT TO SCALE)**

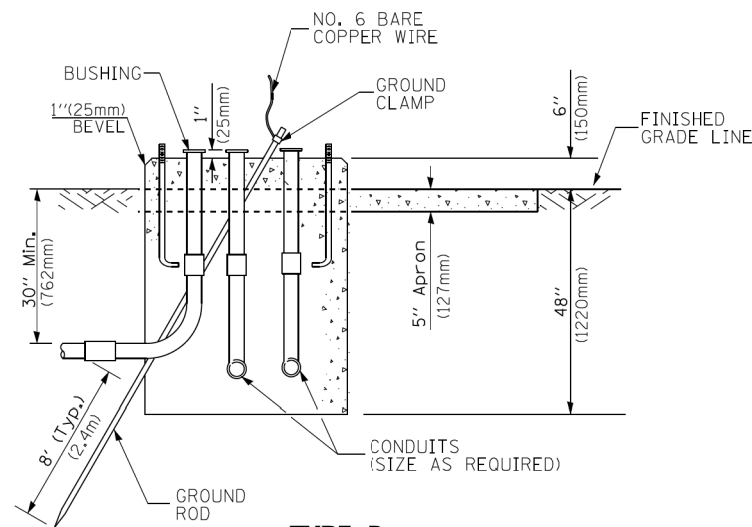
**CABINET – BASE BOLT PATTERN
(NOT TO SCALE)**



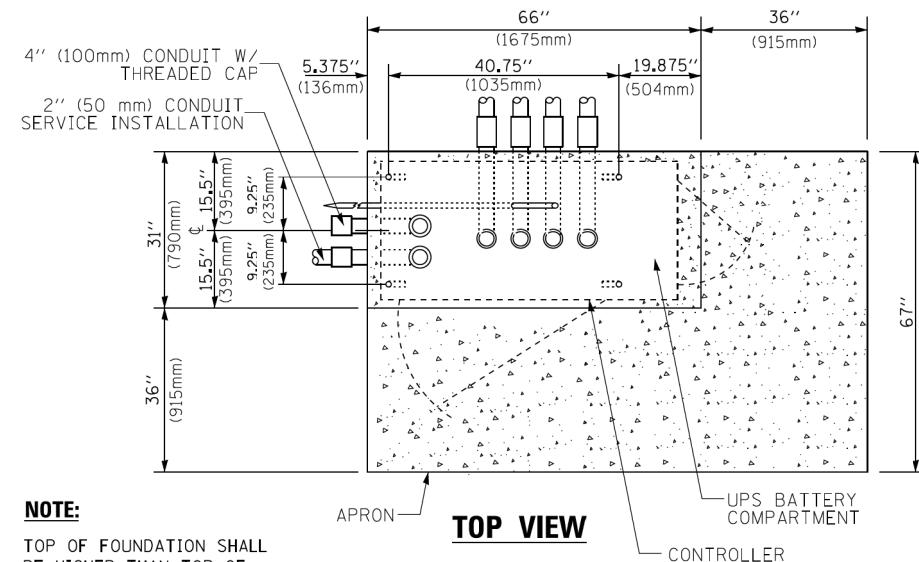
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| ca:\pwork\pwork\footemj\00108315\ts05.dgn | PLOT SCALE = 50.0000' / 1" | DRAWN - BCK | REVISED - | | SCALE: NONE | SHEET NO. 4 | OF 7 SHEETS | STA. | TO STA. | TS-05 | | | CONTRACT NO. 60W80 |
| | PLOT DATE = 1/13/2014 | CHECKED - DAD | REVISED - | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | | | | | |
| | | DATE - 10-28-09 | REVISED - | | | | | | | | | | |



TOP VIEW

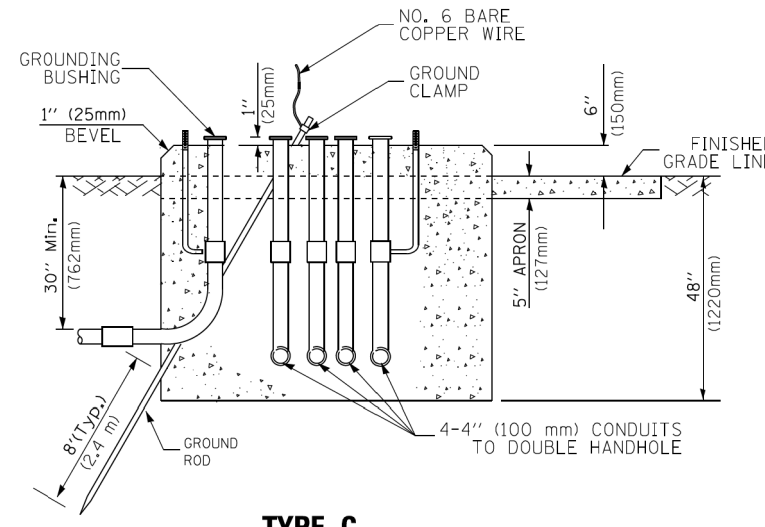


**TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET**

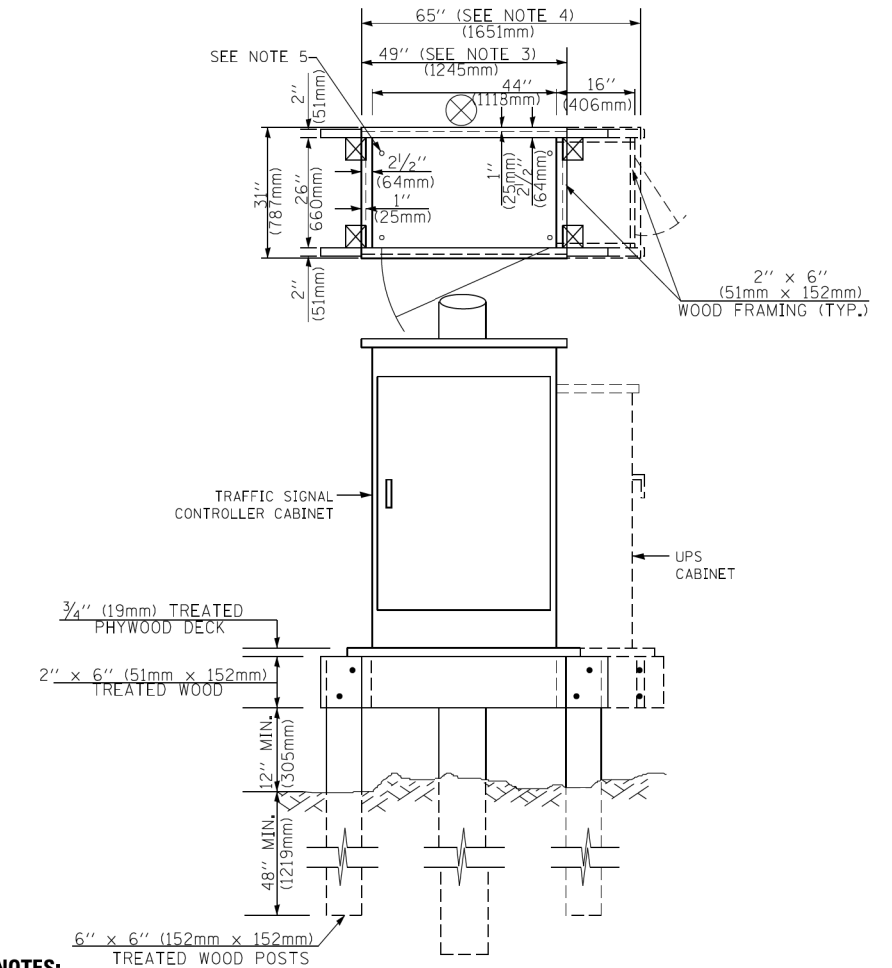


TOP VIEW

NOTE:
TOP OF FOUNDATION SHALL BE HIGHER THAN TOP OF DOUBLE HANDHOLE



**TYPE C
FOR GROUND MOUNTED
SUPER P (TYPE IV) AND SUPER R (TYPE V)
CONTROLLER CABINETS**



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

**TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM**

| CABLE SLACK LENGTH | FEET | METER |
|---------------------------------------------------|------|-------|
| HANDHOLE | 6.5 | 2.0 |
| DOUBLE HANDHOLE | 13.0 | 4.0 |
| SIGNAL POST | 2.0 | 0.6 |
| MAST ARM | 2.0 | 0.6 |
| CONTROLLER CABINET | 1.5 | 0.5 |
| FIBER OPTIC AT CABINET | 13.0 | 4.0 |
| ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION) | 1.5 | 0.5 |
| GROUND CABLE (SIGNAL POST, MAST ARM, CABINET) | 1.5 | 0.5 |
| GROUND CABLE (BETWEEN FRAME AND COVER) | 5.0 | 1.6 |

CABLE SLACK

| VERTICAL CABLE LENGTH | FEET | METER |
|-----------------------------------------------------------------------------------------------------------------|--------|-------|
| MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM) | 20.0+L | 6.0+L |
| BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE) | 13.0 | 4.0 |
| PEDESTRIAN PUSH BUTTON | 6.0 | 2.0 |
| SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP | 13.5 | 4.1 |
| SERVICE INSTALLATION POLE MOUNT TO GROUND | 13.5 | 4.1 |
| SERVICE INSTALLATION GROUND MOUNT | 6.0 | 2.0 |
| FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT) | 3.0 | 1.0 |

VERTICAL CABLE LENGTH

| FOUNDATION | DEPTH |
|-----------------------------------------------------|--------------|
| TYPE A - Signal Post | 4'-0" (1.2m) |
| TYPE C - CONTROLLER W/ UPS | 4'-0" (1.2m) |
| TYPE D - CONTROLLER | 4'-0" (1.2m) |
| SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE | 4'-0" (1.2m) |

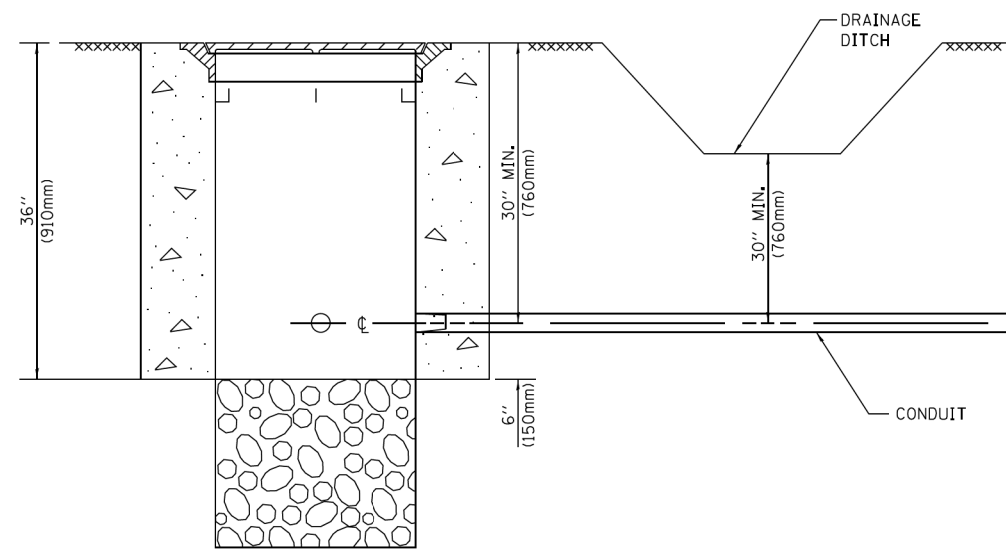
DEPTH OF FOUNDATION

| MAST ARM LENGTH | ① FOUNDATION DEPTH | FOUNDATION DIAMETER | SPIRAL DIAMETER | QUANTITY OF REBARS | SIZE OF REBARS |
|------------------------------------------------------------------|--------------------|---------------------|-----------------|--------------------|----------------|
| Less than 30' (9.1 m) | 10'-0" (3.0 m) | 30" (750mm) | 24" (600mm) | 8 | 6(19) |
| Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m) | 13'-6" (4.1 m) | 30" (750mm) | 24" (600mm) | 8 | 6(19) |
| Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m) | 11'-0" (3.4 m) | 36" (900mm) | 30" (750mm) | 12 | 7(22) |
| Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m) | 13'-0" (4.0 m) | 36" (900mm) | 30" (750mm) | 12 | 7(22) |
| Greater than or equal to 55' (16.8 m) and less than 65' (19.8 m) | 15'-0" (4.6 m) | 36" (900mm) | 30" (750mm) | 12 | 7(22) |
| Greater than or equal to 65' (19.8 m) and less than 75' (22.9 m) | 21'-0" (6.4 m) | 42" (1060mm) | 36" (900mm) | 16 | 8(25) |
| Greater than or equal to 75' (22.9 m) | 25'-0" (7.6 m) | 42" (1060mm) | 36" (900mm) | 16 | 8(25) |

NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
4. For mast arm assemblies with dual arms refer to state standard 878001..

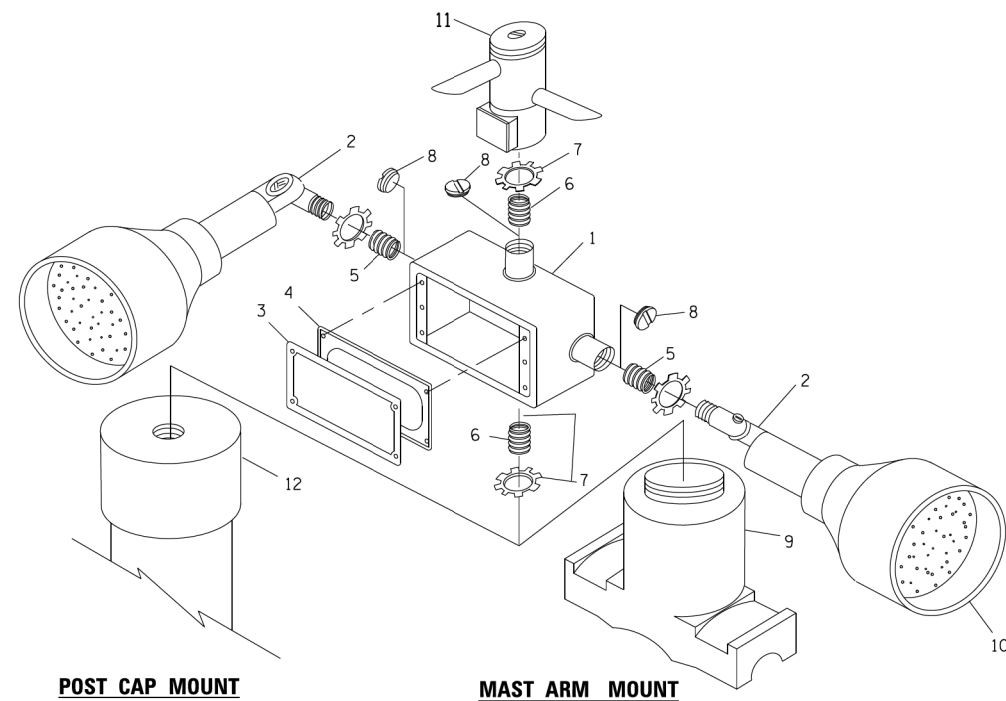
DEPTH OF MAST ARM FOUNDATIONS, TYPE E



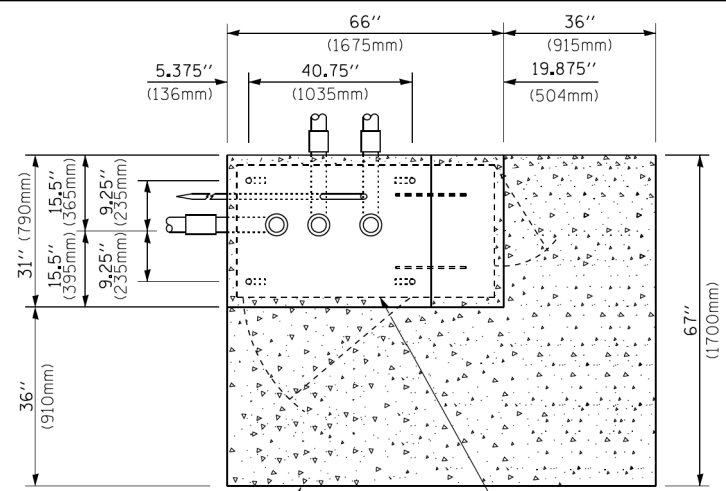
NOTES:

1. CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
2. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
3. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

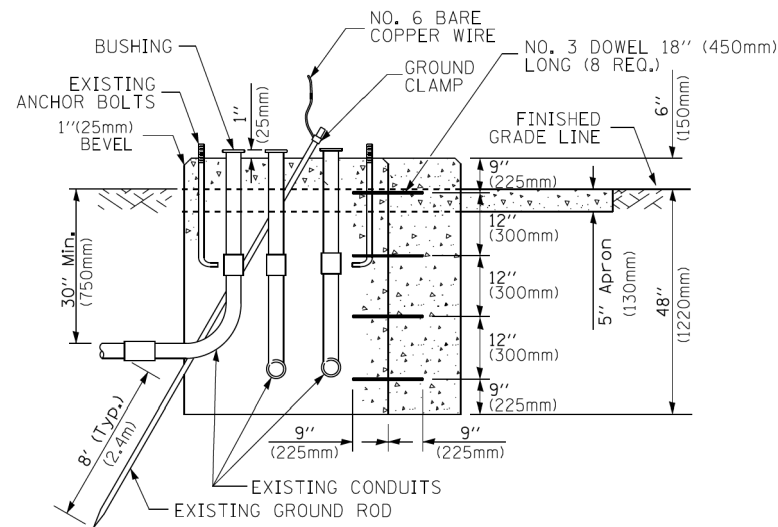
HANDHOLE WITH MINIMUM CONDUIT DEPTH
(NOT TO SCALE)



POST CAP MOUNT **MAST ARM MOUNT**
EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL



TOP VIEW
(NOT TO SCALE)

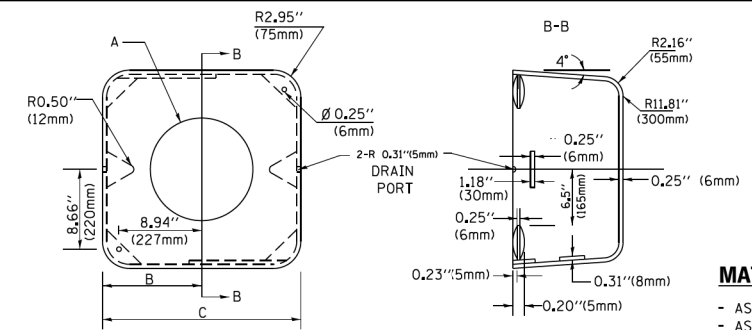


MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION
(NOT TO SCALE)

| ITEM NO. | IDENTIFICATION |
|----------|---------------------------------------------|
| 1 | OUTLET BOX- GALV. 21 CU.IN. (0,000344 CU-M) |
| 2 | LAMP HOLDER AND COVER |
| 3 | OUTLET BOX COVER |
| 4 | RUBBER COVER GASKET |
| 5 | REDUCING BUSHING |
| 6 | 3/4" (19 mm) CLOSE NIPPLE |
| 7 | 3/4" (19 mm) LOCKNUT |
| 8 | 3/4" (19 mm) HOLE PLUG |
| 9 | SADDLE BRACKET - GALV. |
| 10 | 6 WATT PAR 38 LED FLOOD LAMP |
| 11 | DETECTOR UNIT |
| 12 | POST CAP [18 FT. (5,4 m) POST MIN.] |

NOTES:

1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



MATERIAL:
- ASTM A36 STEEL
- ASTM A-123 HOT DIPPED GALVANIZED

| A | B | C | HEIGHT | WEIGHT |
|--------|----------------|---------------|--------------------------|-----------------|
| VARIES | 9.5" (241mm) | 19" (483mm) | 7" (178mm) - 12" (300mm) | 53 lbs (24kg) |
| VARIES | 10.75" (273mm) | 21.5" (546mm) | 7" (178mm) - 12" (300mm) | 68 lbs (31 kg) |
| VARIES | 13.0" (330mm) | 26" (660mm) | 7" (178mm) - 12" (300mm) | 81 lbs (37 kg) |
| VARIES | 18.5" (470mm) | 37" (940mm) | 7" (178mm) - 12" (300mm) | 126 lbs (57 kg) |

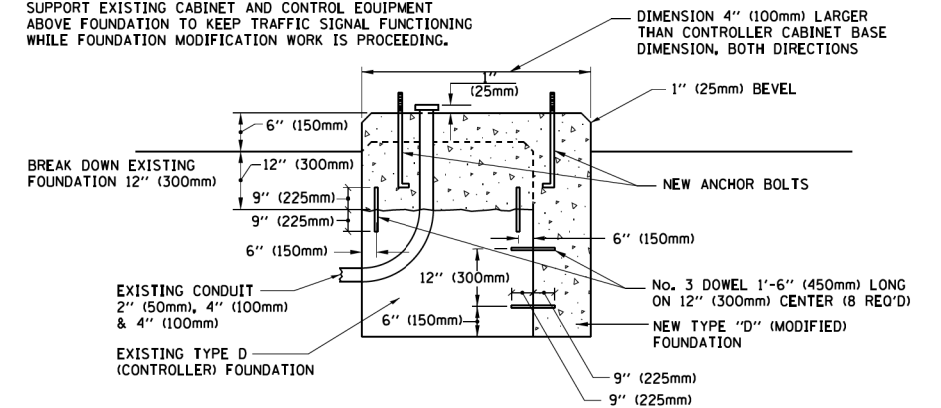
SHROUD

NOTES:

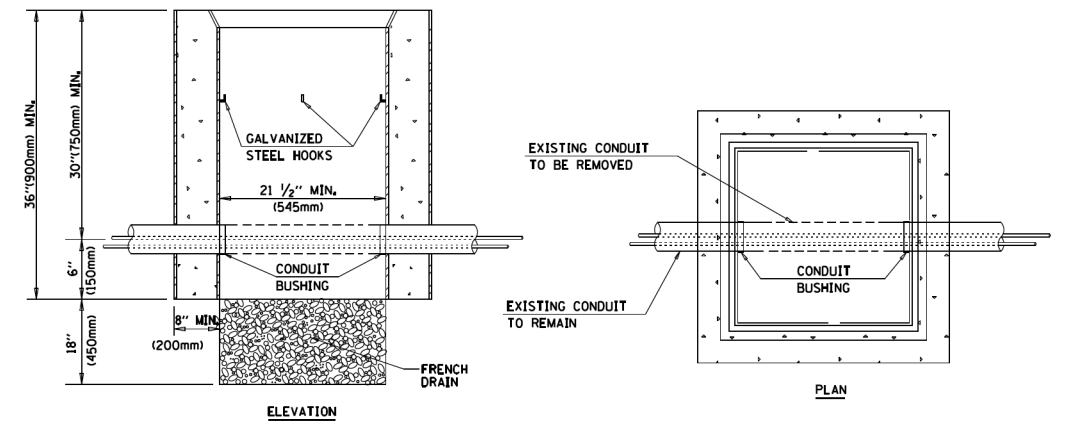
1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
2. THE SUPPLIER SHALL VERIFIED THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

NOTE:

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION



NOTES:

1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

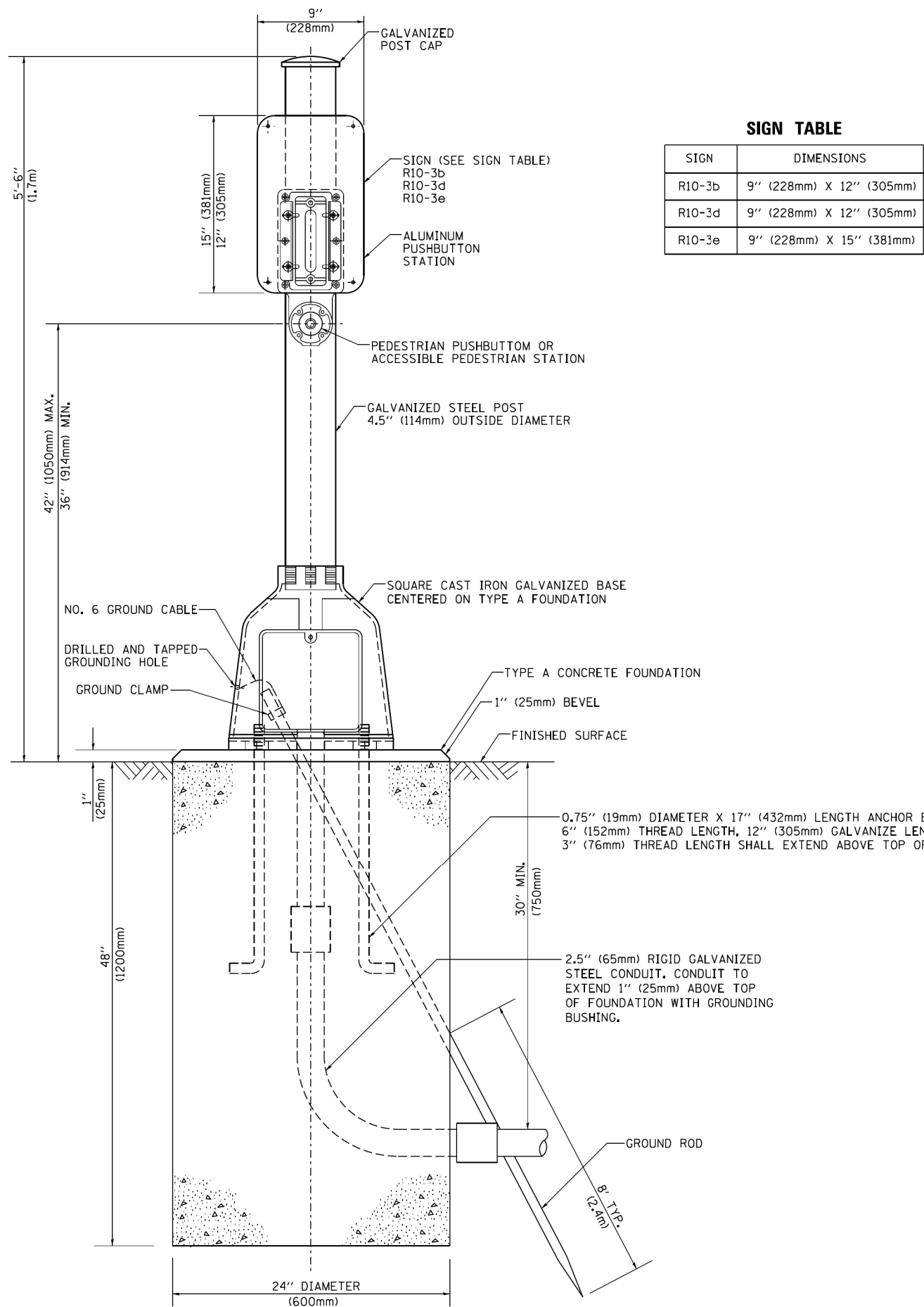
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| ca:\pex-work\pexdot\footemj\d0108315\ts05.dgn | | DRAWN - BCK | REVISED - |
| | PLOT SCALE = 50.0000' / 1" | CHECKED - DAD | REVISED - |
| | PLOT DATE = 1/13/2014 | DATE - 10-28-09 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

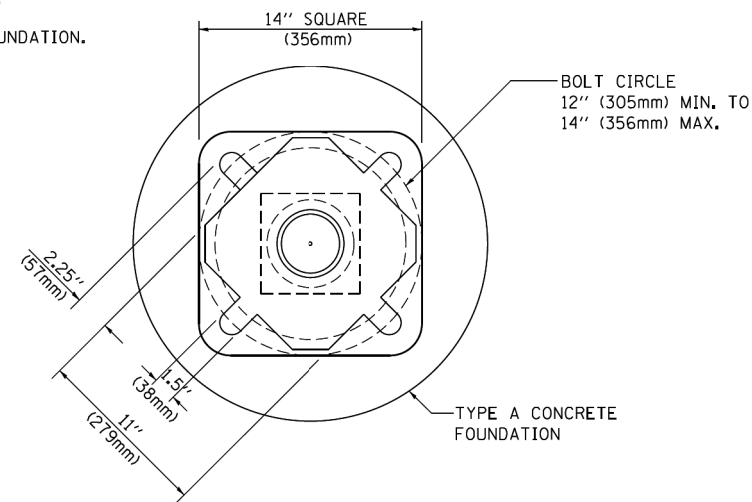
SCALE: NONE SHEET NO. 6 OF 7 SHEETS STA. TO STA.

| | | | | |
|-------------------------------------------------|--------------------|--------------------|-----------------|--------------|
| F.A.U. RTE. 2887 | SECTION 3155-N(13) | COUNTY COOK | TOTAL SHEETS 88 | SHEET NO. 40 |
| TS-05 | | CONTRACT NO. 60W80 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



SIGN TABLE

| SIGN | DIMENSIONS |
|--------|--------------------------|
| R10-3b | 9" (228mm) X 12" (305mm) |
| R10-3d | 9" (228mm) X 12" (305mm) |
| R10-3e | 9" (228mm) X 15" (381mm) |



BOLT PATTERN

PEDESTRIAN PUSH BUTTON POST, TYPE A

| | | | |
|-------------------------------------------|----------------------------|------------------|----------------------|
| FILE NAME = | USER NAME = footemj | DESIGNED - DAG | REVISED - DAG 1-1-14 |
| ca:\pwork\pwork\footemj\d0108315\ts05.dgn | | DRAWN - GND | REVISED - |
| | PLOT SCALE = 50.0000' / 1" | CHECKED - DAD | REVISED - |
| | PLOT DATE = 1/13/2014 | DATE - 10/1/2012 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET NO. 7 OF 7 SHEETS STA. TO STA.

| | | | | |
|-------------------------------------------------|-----------------------|--------------------|-----------------------|--------------------|
| F.A.U. RTE. 2887 | SECTION 3155-N(13) | COUNTY COOK | TOTAL SHEETS 88 | SHEET NO. 41 |
| TS-05 | | CONTRACT NO. 60W80 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

| ITEM NO. | ITEM | UNIT | TOTAL QUANTITY | 0021 | |
|----------|--------------------------------------------------------------------|-------|----------------|--------------------------------|---------------------------------|
| | | | | TRAFFIC SIGNALS | |
| | | | | EAST END AVENUE/ SAUK TRAIL | EAST END AVENUE/ 26TH STREET |
| 1 | SIGN PANEL - TYPE 1 | SQ FT | 61 | 32 | 29 |
| 2 | SERVICE INSTALLATION - POLE MOUNTED | EACH | 2 | 1 | 1 |
| 3 | UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA. | FOOT | 1709 | 725 | 984 |
| 4 | UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA. | FOOT | 72 | 59 | 13 |
| 5 | UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA. | FOOT | 181 | 78 | 103 |
| 6 | UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA. | FOOT | 682 | 290 | 392 |
| 7 | HANDHOLE | EACH | 6 | 5 | 1 |
| 8 | HEAVY-DUTY HANDHOLE | EACH | 13 | 4 | 9 |
| 9 | DOUBLE HANDHOLE | EACH | 3 | 1 | 2 |
| ** | 10 FULL-ACTUATED CONTROLLER AND TYPE IV CABINET | EACH | 1 | 1 | |
| ** | 11 RAILROAD, FULL-ACTUATED CONTROLLER AND TYPE IV CABINET | EACH | 1 | | 1 |
| 12 | ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C | FOOT | 882 | | 882 |
| 13 | ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C | FOOT | 2439 | 1036 | 1403 |
| 14 | ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C | FOOT | 2360 | 1315 | 1045 |
| 15 | ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR | FOOT | 3212 | 1557 | 1655 |
| 16 | ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C | FOOT | 115 | 39 | 76 |
| 17 | ELECTRIC CABLE IN CONDUIT, RAILROAD, NO. 14 3C | FOOT | 539 | | 539 |
| 18 | ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C | FOOT | 1556 | 599 | 957 |
| 19 | TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT. | EACH | 5 | 4 | 1 |
| 20 | TRAFFIC SIGNAL POST, GALVANIZED STEEL 18 FT. | EACH | 1 | | 1 |
| 21 | STEEL MAST ARM ASSEMBLY AND POLE, 22 FT. | EACH | 1 | | 1 |
| 22 | STEEL MAST ARM ASSEMBLY AND POLE, 28 FT. | EACH | 1 | 1 | |
| 23 | STEEL MAST ARM ASSEMBLY AND POLE, 30 FT. | EACH | 2 | 1 | 1 |
| 24 | STEEL MAST ARM ASSEMBLY AND POLE, 32 FT. | EACH | 1 | 1 | |
| 25 | STEEL MAST ARM ASSEMBLY AND POLE, 40 FT. | EACH | 2 | 1 | 1 |

| ITEM NO. | ITEM | UNIT | TOTAL QUANTITY | 0021 | |
|----------|-------------------------------------------------------|------|----------------|--------------------------------|---------------------------------|
| | | | | TRAFFIC SIGNALS | |
| | | | | EAST END AVENUE/ SAUK TRAIL | EAST END AVENUE/ 26TH STREET |
| 26 | STEEL MAST ARM ASSEMBLY AND POLE, 50 FT. | EACH | 1 | | 1 |
| 27 | CONCRETE FOUNDATION, TYPE A | FOOT | 24 | 16 | 8 |
| 28 | CONCRETE FOUNDATION, TYPE C | FOOT | 8 | 4 | 4 |
| 29 | CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER | FOOT | 61 | 37 | 24 |
| 30 | CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER | FOOT | 41 | 13 | 28 |
| 31 | SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED | EACH | 11 | 6 | 5 |
| 32 | SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED | EACH | 1 | | 1 |
| 33 | SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED | EACH | 7 | 4 | 3 |
| 34 | SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED | EACH | 7 | 4 | 3 |
| 35 | TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM | EACH | 18 | 10 | 8 |
| 36 | INDUCTIVE LOOP DETECTOR | EACH | 15 | 8 | 7 |
| 37 | DETECTOR LOOP, TYPE 1 | FOOT | 1296 | 749 | 547 |
| 38 | TEMPORARY TRAFFIC SIGNAL INSTALLATION | EACH | 2 | 1 | 1 |
| 39 | ILLUMINATED SIGN, LED | EACH | 4 | | 4 |
| 40 | REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT | EACH | 2 | 1 | 1 |
| 41 | REMOVE EXISTING HANDHOLE | EACH | 21 | 11 | 10 |
| 42 | REMOVE EXISTING DOUBLE HANDHOLE | EACH | 1 | 1 | |
| 43 | REMOVE EXISTING CONCRETE FOUNDATION | EACH | 18 | 9 | 9 |
| 44 | CONDUIT SPLICE | EACH | 1 | | 1 |
| 45 | UNINTERRUPTABLE POWER SUPPLY SPECIAL | EACH | 2 | 1 | 1 |
| 46 | TEMPORARY TRAFFIC SIGNAL TIMING | EACH | 2 | 1 | 1 |

** SUPER P CABINET



| | | |
|------------------------------|-------------------|-----------|
| USER NAME = brd | DESIGNED - BRD | REVISED - |
| PLOT SCALE = 48,0000' / 1" = | DRAWN - JRT | REVISED - |
| PLOT DATE = 12/18/2014 | CHECKED - JJE | REVISED - |
| | DATE - 12/18/2014 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC SIGNAL SUMMARY OF QUANTITIES
EAST END AVENUE**

| | | | | |
|---------------------------|-----------------------|----------------|--------------------|-----------------|
| F.A.U. RTE. 2887 | SECTION 3155-N(13) | COUNTY COOK | TOTAL SHEETS 88 | SHEET NO. 42 |
| CONTRACT NO. 60W80 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

NO SCALE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

NOTES FOR TEMPORARY TRAFFIC SIGNALS

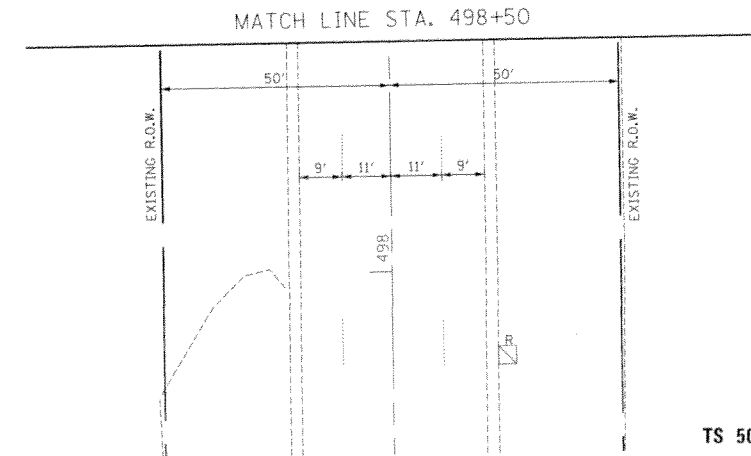
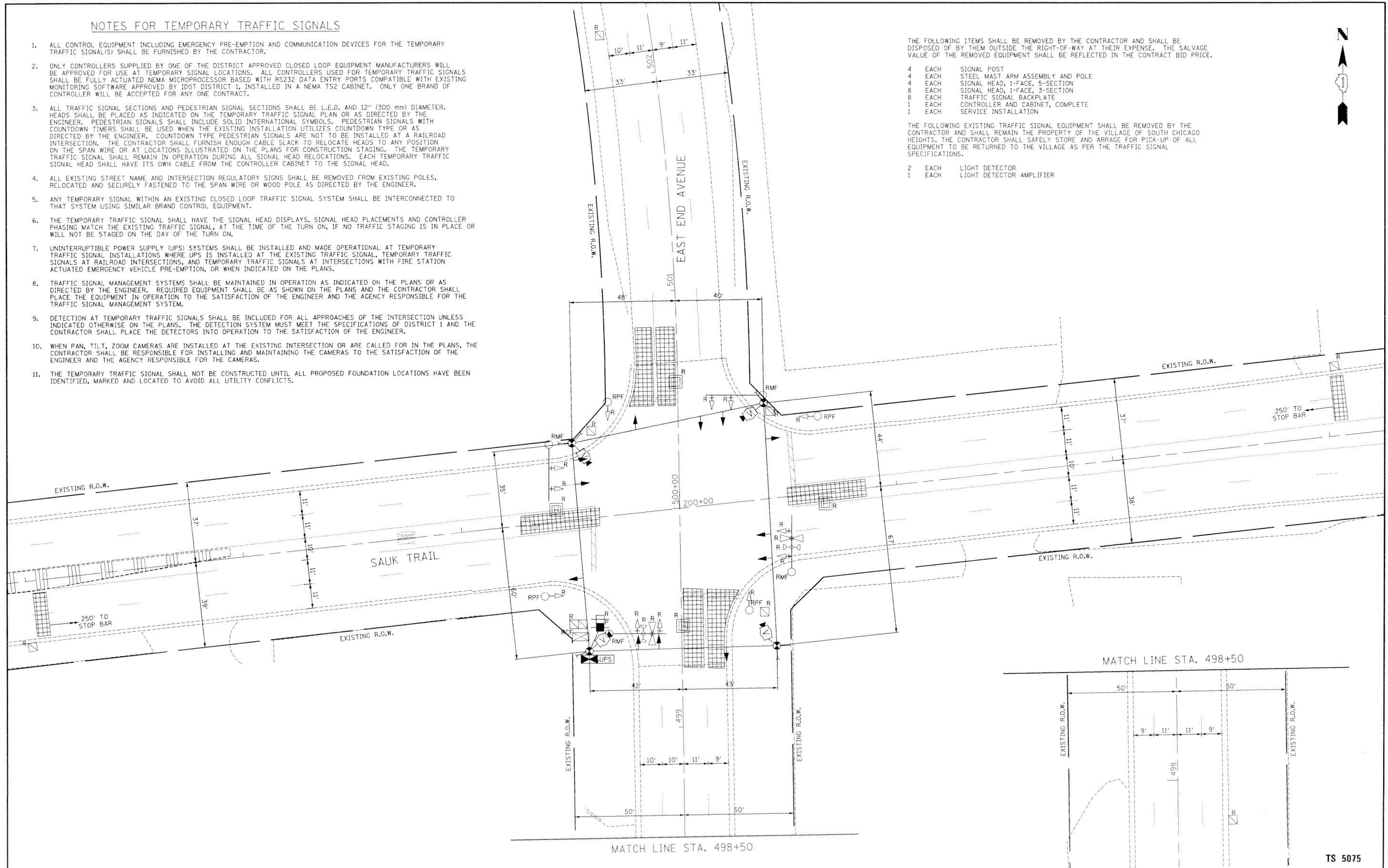
1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE L.E.D. AND 12" (300 mm) DIAMETER. HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. PEDESTRIAN SIGNALS SHALL INCLUDE SOLID INTERNATIONAL SYMBOLS. PEDESTRIAN SIGNALS WITH COUNTDOWN TIMERS SHALL BE USED WHEN THE EXISTING INSTALLATION UTILIZES COUNTDOWN TYPE OR AS DIRECTED BY THE ENGINEER. COUNTDOWN TYPE PEDESTRIAN SIGNALS ARE NOT TO BE INSTALLED AT A RAILROAD INTERSECTION. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
4. ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
6. THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL, AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
7. UNINTERRUPTIBLE POWER SUPPLY (UPS) SYSTEMS SHALL BE INSTALLED AND MADE OPERATIONAL AT TEMPORARY TRAFFIC SIGNAL INSTALLATIONS WHERE UPS IS INSTALLED AT THE EXISTING TRAFFIC SIGNAL, TEMPORARY TRAFFIC SIGNALS AT RAILROAD INTERSECTIONS, AND TEMPORARY TRAFFIC SIGNALS AT INTERSECTIONS WITH FIRE STATION ACTUATED EMERGENCY VEHICLE PRE-EMPTION, OR WHEN INDICATED ON THE PLANS.
8. TRAFFIC SIGNAL MANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
9. DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER.
10. WHEN PAN, TILT, ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.
11. THE TEMPORARY TRAFFIC SIGNAL SHALL NOT BE CONSTRUCTED UNTIL ALL PROPOSED FOUNDATION LOCATIONS HAVE BEEN IDENTIFIED, MARKED AND LOCATED TO AVOID ALL UTILITY CONFLICTS.

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 4 EACH SIGNAL POST
- 4 EACH STEEL MAST ARM ASSEMBLY AND POLE
- 4 EACH SIGNAL HEAD, 1-FACE, 5-SECTION
- 8 EACH SIGNAL HEAD, 1-FACE, 3-SECTION
- 8 EACH TRAFFIC SIGNAL BACKPLATE
- 1 EACH CONTROLLER AND CABINET, COMPLETE
- 1 EACH SERVICE INSTALLATION

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE VILLAGE OF SOUTH CHICAGO HEIGHTS. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR PICK-UP OF ALL EQUIPMENT TO BE RETURNED TO THE VILLAGE AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

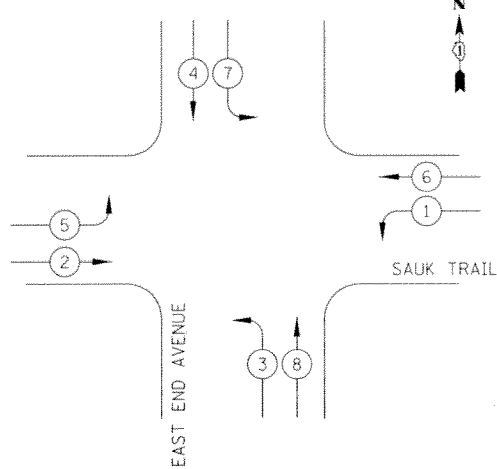
- 2 EACH LIGHT DETECTOR
- 1 EACH LIGHT DETECTOR AMPLIFIER



TS 5075

| | | | | | | | | | | | |
|--|-----------------------------|-------------------|-----------|----------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------------------------------------------------|-------------------------|------------------|---------------------------|-------------|--------------------|--------------|
| | USER NAME = brd | DESIGNED - BRD | REVISED - | <p align="center">STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</p> | <p align="center">TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVAL PLAN EAST END AVENUE AND SAUK TRAIL</p> | | F.A.U. RTE. 2887 | SECTION 3155-M131 | COUNTY COOK | TOTAL SHEETS 88 | SHEET NO. 43 |
| | PLOT SCALE = 48,0000' / in. | CHECKED - JJE | REVISED - | | SCALE: 1" = 20' | SHEET NO. 1 OF 1 SHEETS | STA. TO STA. | ILLINOIS FED. AID PROJECT | | CONTRACT NO. 60W80 | |
| | PLOT DATE = 12/18/2014 | DATE = 12/18/2014 | REVISED - | | | | | | | | |

PROPOSED CONTROLLER SEQUENCE



LEGEND

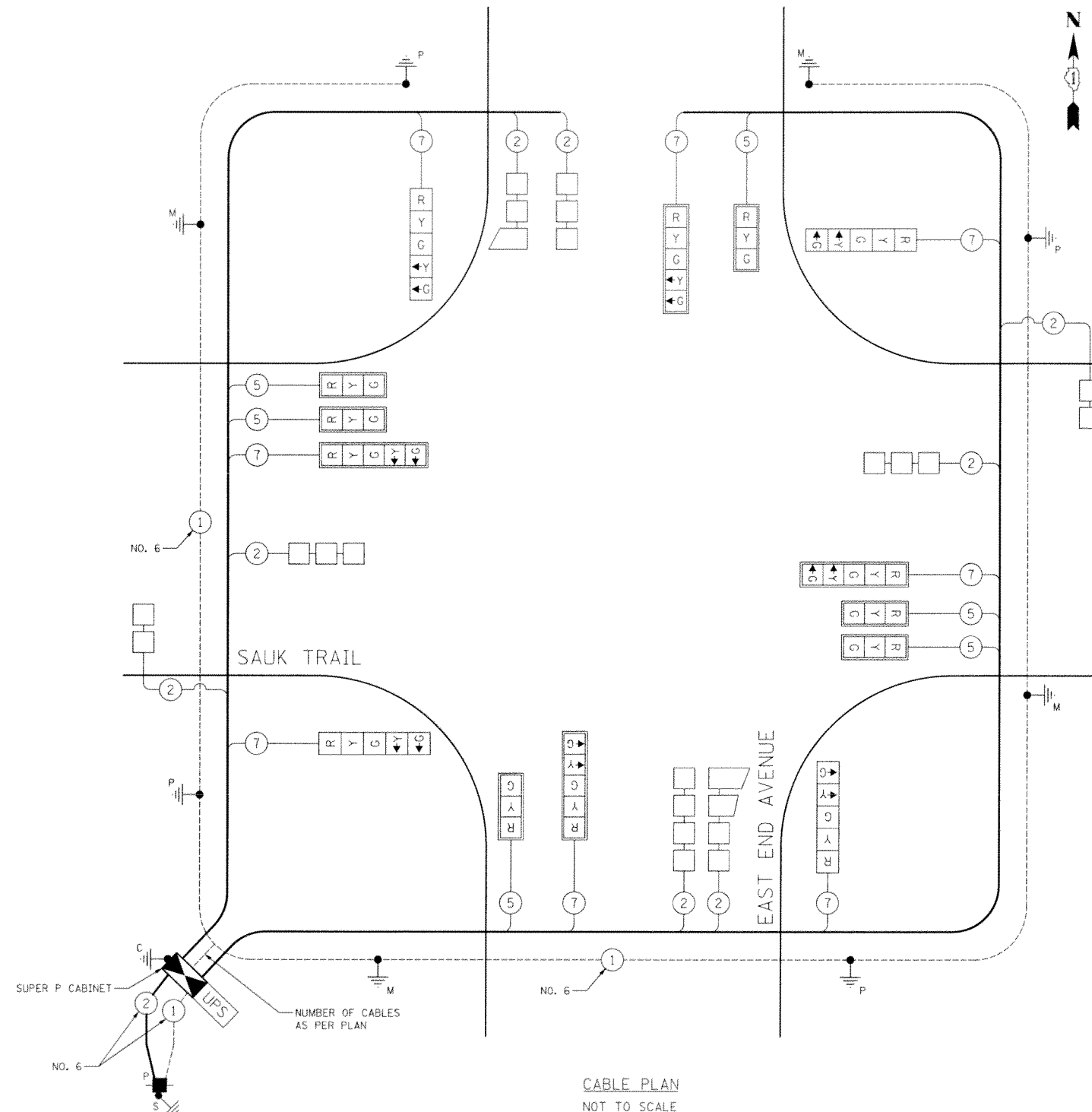
- DUAL ENTRY PHASE
- SINGLE ENTRY PHASE
- OVERLAP
- PEDESTRIAN PHASE
- NUMBER REFERS TO ASSOCIATED PHASE

PROPOSED PHASE DESIGNATION DIAGRAM

SCHEDULE OF QUANTITIES

| PAY ITEM | UNIT | QNTY. |
|--------------------------------------------------------------------|-------|-------|
| SIGN PANEL - TYPE 1 | SQ FT | 32 |
| SERVICE INSTALLATION - POLE MOUNTED | EACH | 1 |
| UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA. | FOOT | 725 |
| UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA. | FOOT | 59 |
| UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA. | FOOT | 78 |
| UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA. | FOOT | 290 |
| HANDHOLE | EACH | 5 |
| HEAVY-DUTY HANDHOLE | EACH | 4 |
| DOUBLE HANDHOLE | EACH | 1 |
| FULL-ACTUATED CONTROLLER AND TYPE IV CABINET | EACH | 1 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C | FOOT | 1036 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C | FOOT | 1315 |
| ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR | FOOT | 1557 |
| ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C | FOOT | 39 |
| ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C | FOOT | 599 |
| TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT. | EACH | 4 |
| STEEL MAST ARM ASSEMBLY AND POLE, 28 FT. | EACH | 1 |
| STEEL MAST ARM ASSEMBLY AND POLE, 30 FT. | EACH | 1 |
| STEEL MAST ARM ASSEMBLY AND POLE, 32 FT. | EACH | 1 |
| STEEL MAST ARM ASSEMBLY AND POLE, 40 FT. | EACH | 1 |
| CONCRETE FOUNDATION, TYPE A | FOOT | 16 |
| CONCRETE FOUNDATION, TYPE C | FOOT | 4 |
| CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER | FOOT | 37 |
| CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER | FOOT | 13 |
| SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED | EACH | 6 |
| SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED | EACH | 4 |
| SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED | EACH | 4 |
| TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM | EACH | 10 |
| INDUCTIVE LOOP DETECTOR | EACH | 8 |
| DETECTOR LOOP, TYPE 1 | FOOT | 749 |
| TEMPORARY TRAFFIC SIGNAL INSTALLATION | EACH | 1 |
| REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT | EACH | 1 |
| REMOVE EXISTING HANDHOLE | EACH | 11 |
| REMOVE EXISTING DOUBLE HANDHOLE | EACH | 1 |
| REMOVE EXISTING CONCRETE FOUNDATION | EACH | 9 |
| UNINTERRUPTABLE POWER SUPPLY SPECIAL | EACH | 1 |
| TEMPORARY TRAFFIC SIGNAL TIMING | EACH | 1 |

* SUPER P CABINET



| I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS | | | | | TOTAL WATTAGE |
|----------------------------------------------------------------------|-----------|---------|------|-------------|---------------|
| TYPE | NO. LAMPS | WATTAGE | | % OPERATION | |
| SIGNAL | (RED) | 14 | 17 | 0.50 | 119 |
| | (YELLOW) | 14 | 25 | 0.25 | 88 |
| | (GREEN) | 14 | 15 | 0.25 | 53 |
| ARROW | 16 | 12 | 0.10 | 19 | |
| PED. SIGNAL | | 25 | 1.00 | | |
| CONTROLLER | 1 | 100 | 1.00 | | 100 |
| FLASHER | | | | | 0.50 |
| TOTAL = | | | | | 379 |

ENERGY COSTS TO: ILLINOIS DEPARTMENT OF TRANSPORTATION
201 WEST CENTER COURT
SCHALMBURG, IL 60196-1096

ENERGY SUPPLY: CONTACT: DEB KOPESHKE
PHONE: (708) 235-2331
COMPANY: COM ED

| | | |
|---------------------------|------------------|------------|
| USER NAME: brd | DESIGNED: BRD | REVISED: - |
| PLOT SCALE: 48,0000" / 1" | DRAWN: JRT | REVISED: - |
| PLOT DATE: 12/18/2014 | CHECKED: JJE | REVISED: - |
| | DATE: 12/18/2014 | REVISED: - |

| | | |
|------------------------------|------------------|------------|
| STATE OF ILLINOIS | DESIGNED: BRD | REVISED: - |
| DEPARTMENT OF TRANSPORTATION | DRAWN: JRT | REVISED: - |
| | CHECKED: JJE | REVISED: - |
| | DATE: 12/18/2014 | REVISED: - |

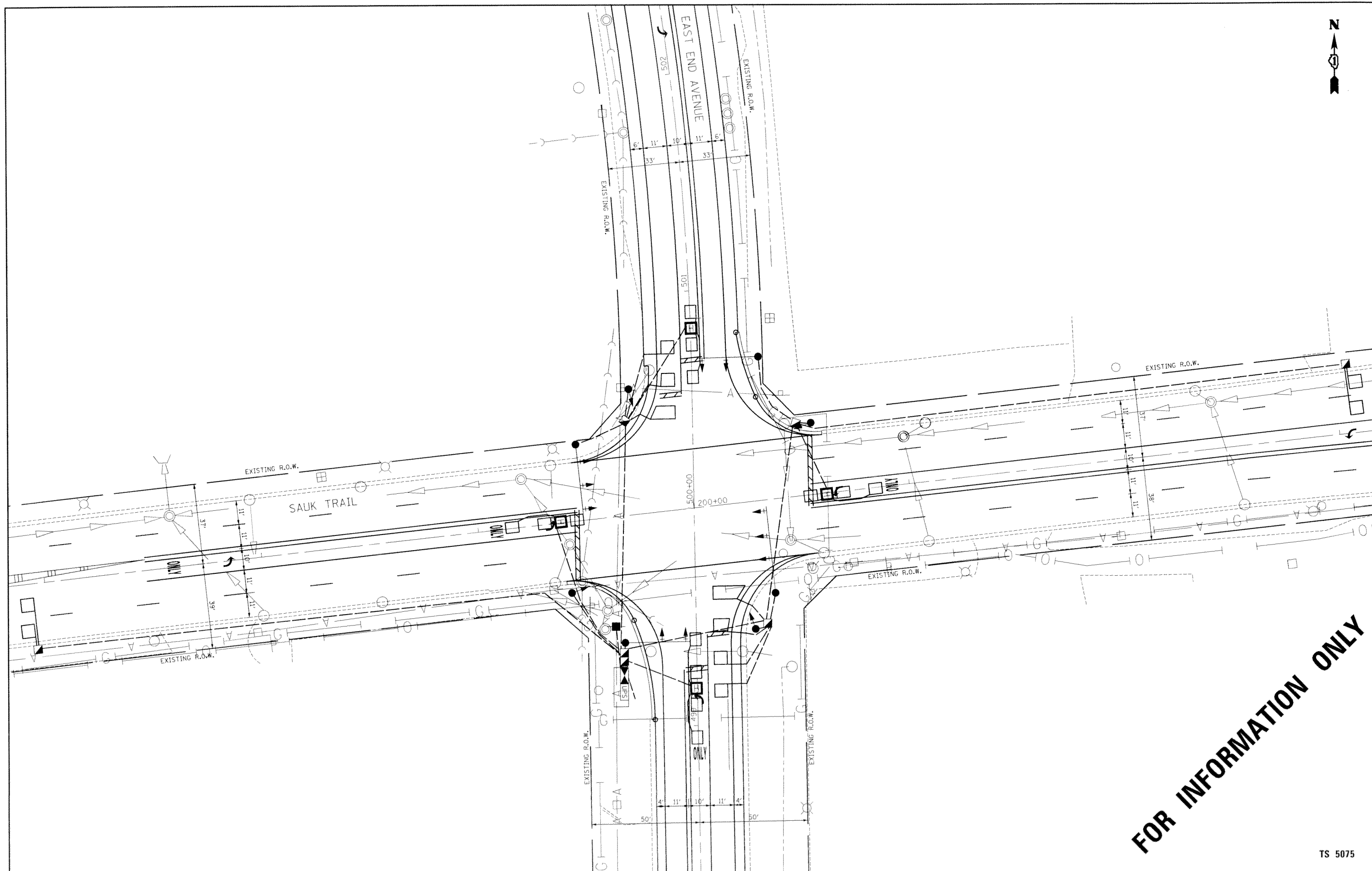
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CABLE PLAN, PHASE DESIGNATION DIAGRAM,
EMERGENCY VEHICLE PREEMPTION SEQUENCE & SCHEDULE OF QUANTITIES
EAST END AVENUE AND SAUK TRAIL

| | | | | |
|--------------------|--------------------|-------------|---------------------------|--------------|
| F.A.U. RTE. 2887 | SECTION 3155-N(13) | COUNTY COOK | TOTAL SHEETS 88 | SHEET NO. 44 |
| CONTRACT NO. 60W80 | | | ILLINOIS FED. AID PROJECT | |

TS 5075





FOR INFORMATION ONLY

TS 5075



| | | |
|--------------------------------|-------------------|-----------|
| USER NAME = brd | DESIGNED - BRD | REVISED - |
| | DRAWN - JRT | REVISED - |
| PLOT SCALE = 40,000.00' / 1" = | CHECKED - JJE | REVISED - |
| PLOT DATE = 12/18/2014 | DATE - 12/18/2014 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

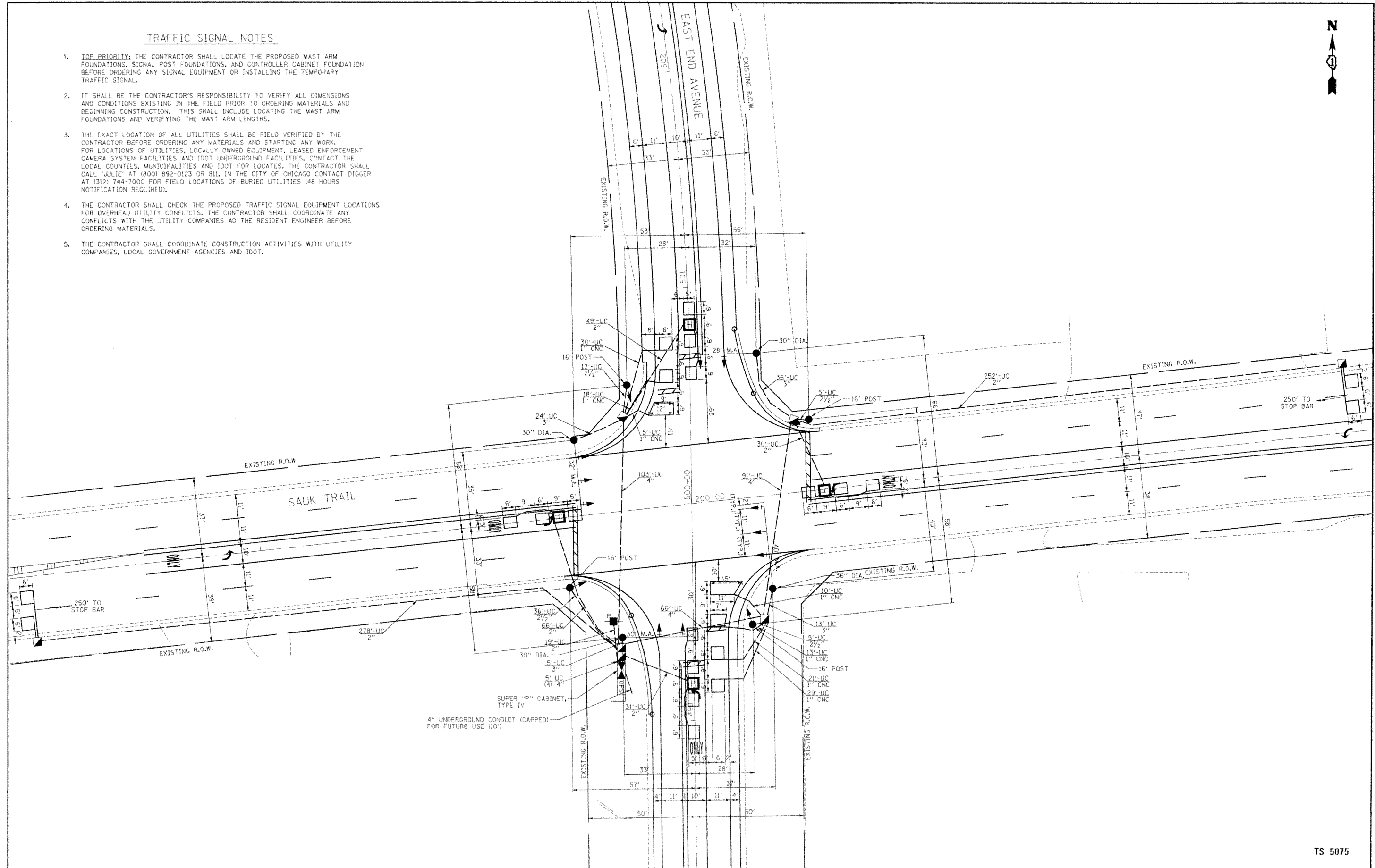
**TRAFFIC SIGNAL MODERNIZATION PLAN
EAST END AVENUE AND SAUK TRAIL**

SCALE: 1" = 20' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|------------|--------|--------------|--------------------|
| 2887 | 3155-N(13) | COOK | 88 | 45 |
| | | | | CONTRACT NO. 60W80 |
| ILLINOIS FED. AID PROJECT | | | | |

TRAFFIC SIGNAL NOTES

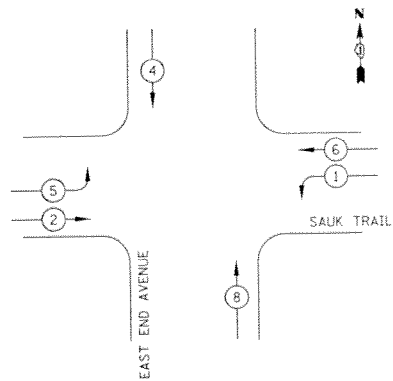
1. **TOP PRIORITY:** THE CONTRACTOR SHALL LOCATE THE PROPOSED MAST ARM FOUNDATIONS, SIGNAL POST FOUNDATIONS, AND CONTROLLER CABINET FOUNDATION BEFORE ORDERING ANY SIGNAL EQUIPMENT OR INSTALLING THE TEMPORARY TRAFFIC SIGNAL.
2. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARM LENGTHS.
3. THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES. THE CONTRACTOR SHALL CALL 'JULIE' AT (800) 892-0123 OR 811. IN THE CITY OF CHICAGO CONTACT DIGGER AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION REQUIRED).
4. THE CONTRACTOR SHALL CHECK THE PROPOSED TRAFFIC SIGNAL EQUIPMENT LOCATIONS FOR OVERHEAD UTILITY CONFLICTS. THE CONTRACTOR SHALL COORDINATE ANY CONFLICTS WITH THE UTILITY COMPANIES AND THE RESIDENT ENGINEER BEFORE ORDERING MATERIALS.
5. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, LOCAL GOVERNMENT AGENCIES AND IDOT.



TS 5075

| | | | | | | | | | | |
|--|------------------------------|-------------------|-----------|-----------------------------------------------------------------|-----------------------------------------------------------------------------------|--------------------|-------------------------|--------------|---------------------------|--------------|
| | USER NAME = brd | DESIGNED - BRD | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TRAFFIC SIGNAL MODERNIZATION PLAN EAST END AVENUE AND SAUK TRAIL | F.A.U. RTE. 2887 | SECTION 3155-N(13) | COUNTY COOK | TOTAL SHEETS 88 | SHEET NO. 46 |
| | PLOT SCALE = 48,0000 1/4 in. | CHECKED - JJE | REVISED - | | | SCALE: 1" = 20' | SHEET NO. 1 OF 1 SHEETS | STA. TO STA. | ILLINOIS FED. AID PROJECT | |
| | PLOT DATE = 12/18/2014 | DATE = 12/18/2014 | REVISED - | | | CONTRACT NO. 60W80 | | | | |

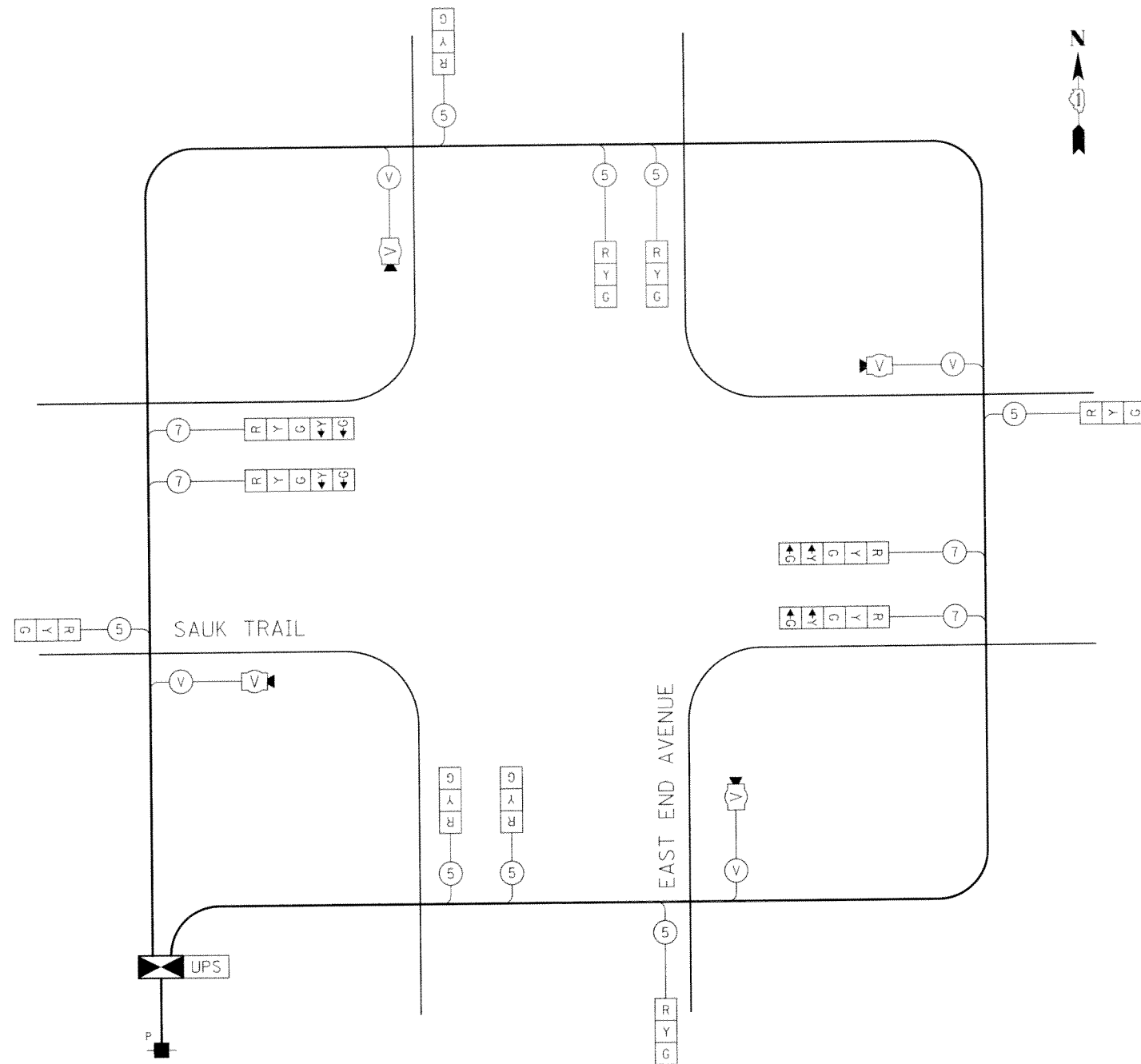
TEMPORARY CONTROLLER SEQUENCE



LEGEND

- DUAL ENTRY PHASE
- SINGLE ENTRY PHASE
- OVERLAP
- PEDESTRIAN PHASE
- NUMBER REFERS TO ASSOCIATED PHASE

TEMPORARY PHASE DESIGNATION DIAGRAM



TEMPORARY CABLE PLAN
NOT TO SCALE

| I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS | | | | | TOTAL WATTAGE |
|----------------------------------------------------------------------------|-----------|---------|-----|-------------|------------------|
| TYPE | NO. LAMPS | WATTAGE | | % OPERATION | |
| SIGNAL (RED) | 12 | INCAND. | 17 | 0.50 | 102 |
| (YELLOW) | 12 | | 25 | 0.25 | 75 |
| (GREEN) | 12 | | 15 | 0.25 | 45 |
| ARROW | 8 | | 12 | 0.10 | 10 |
| PED. SIGNAL | | | 25 | 1.00 | |
| CONTROLLER | 1 | | 100 | 1.00 | 100 |
| VIDEO SYSTEM | 1 | | 150 | 1.00 | 150 |
| FLASHER | | | | 0.50 | |
| TOTAL = | | | | | 482 |

ENERGY COSTS TO: ILLINOIS DEPARTMENT OF TRANSPORTATION
201 WEST CENTER COURT
SCHALMBURG, IL 60196-1096
ENERGY SUPPLY: CONTACT: DEB KOPESHKE
PHONE: (708) 235-2331
COMPANY: COM ED

TS 5075



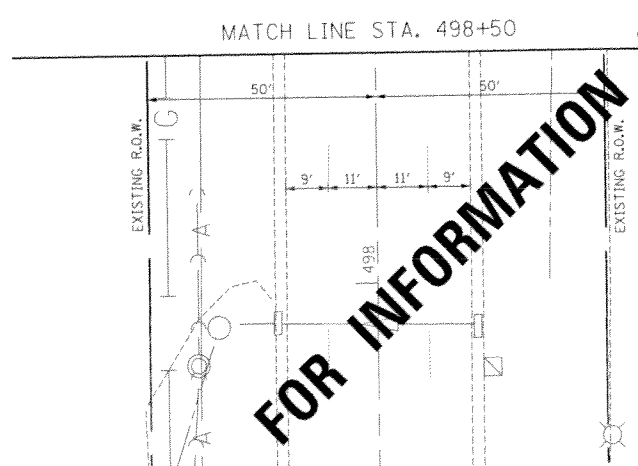
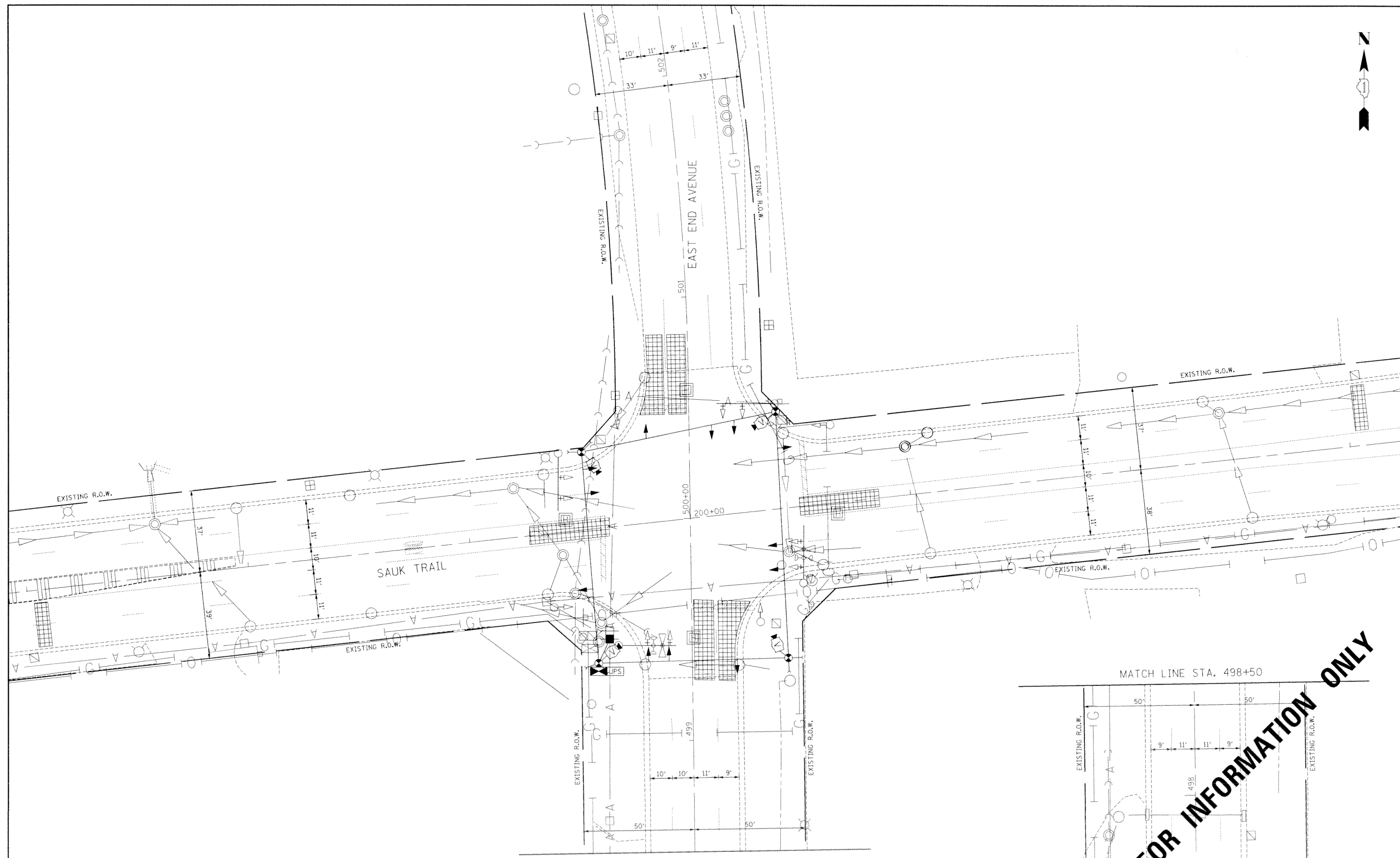
| | | |
|-----------------------------|-------------------|-----------|
| USER NAME = brd | DESIGNED - BRD | REVISED - |
| PLLOT SCALE = 48,0000 / in. | DRAWN - JRT | REVISED - |
| PLLOT DATE = 12/18/2014 | CHECKED - JJE | REVISED - |
| | DATE - 12/18/2014 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY CABLE PLAN, TEMPORARY PHASE DESIGNATION DIAGRAM,
AND TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE
EAST END AVENUE AND SAUK TRAIL

NOT TO SCALE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| | | | | |
|---------------------------|--------------------|-------------|--------------------|--------------|
| F.A.U. RTE. 2887 | SECTION 3155-N(13) | COUNTY COOK | TOTAL SHEETS 88 | SHEET NO. 47 |
| | | | CONTRACT NO. 60W80 | |
| ILLINOIS FED. AID PROJECT | | | | |



FOR INFORMATION ONLY

TS 5075



| | | |
|--------------------------------|-------------------|-----------|
| USER NAME = b-r-d | DESIGNED - BRD | REVISED - |
| PLOT SCALE = 48,0000 1/16" in. | DRAWN - JRT | REVISED - |
| PLOT DATE = 12/18/2014 | CHECKED - JJE | REVISED - |
| | DATE - 12/18/2014 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVAL PLAN
EAST END AVENUE AND SAUK TRAIL**

SCALE: 1" = 20' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| | | | | |
|---------------------------|--------------------|-------------|-----------------|--------------------|
| F.A.U. RTE. 2887 | SECTION 3155-N(13) | COUNTY COOK | TOTAL SHEETS 88 | SHEET NO. 48 |
| ILLINOIS FED. AID PROJECT | | | | CONTRACT NO. 60W80 |

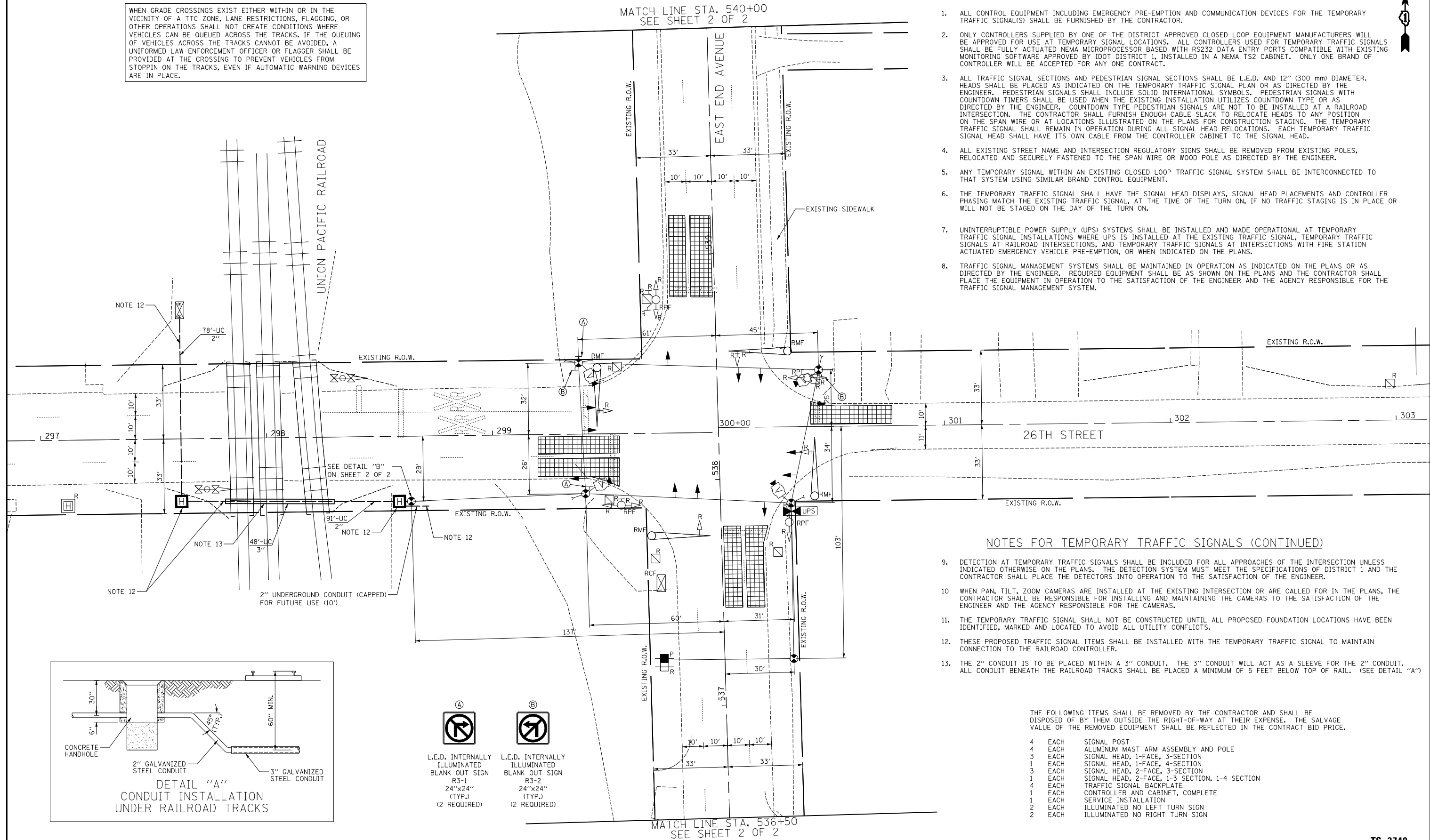


NOTES FOR TEMPORARY TRAFFIC SIGNALS

1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE L.E.D. AND 12" (300 mm) DIAMETER. HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. PEDESTRIAN SIGNALS SHALL INCLUDE SOLID INTERNATIONAL SYMBOLS. PEDESTRIAN SIGNALS WITH COUNTDOWN TIMERS SHALL BE USED WHEN THE EXISTING INSTALLATION UTILIZES COUNTDOWN TYPE OR AS DIRECTED BY THE ENGINEER. COUNTDOWN TYPE PEDESTRIAN SIGNALS ARE NOT TO BE INSTALLED AT A RAILROAD INTERSECTION. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
4. ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
6. THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL, AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
7. UNINTERRUPTIBLE POWER SUPPLY (UPS) SYSTEMS SHALL BE INSTALLED AND MADE OPERATIONAL AT TEMPORARY TRAFFIC SIGNAL INSTALLATIONS WHERE UPS IS INSTALLED AT THE EXISTING TRAFFIC SIGNAL, TEMPORARY TRAFFIC SIGNALS AT RAILROAD INTERSECTIONS, AND TEMPORARY TRAFFIC SIGNALS AT INTERSECTIONS WITH FIRE STATION ACTUATED EMERGENCY VEHICLE PRE-EMPTION, OR WHEN INDICATED ON THE PLANS.
8. TRAFFIC SIGNAL MANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.

WHEN GRADE CROSSINGS EXIST EITHER WITHIN OR IN THE VICINITY OF A TTC ZONE, LANE RESTRICTIONS, FLAGGING, OR OTHER OPERATIONS SHALL NOT CREATE CONDITIONS WHERE VEHICLES CAN BE QUEUED ACROSS THE TRACKS. IF THE QUEUING OF VEHICLES ACROSS THE TRACKS CANNOT BE AVOIDED, A UNIFORMED LAW ENFORCEMENT OFFICER OR FLAGGER SHALL BE PROVIDED AT THE CROSSING TO PREVENT VEHICLES FROM STOPPING ON THE TRACKS, EVEN IF AUTOMATIC WARNING DEVICES ARE IN PLACE.

MATCH LINE STA. 540+00
SEE SHEET 2 OF 2

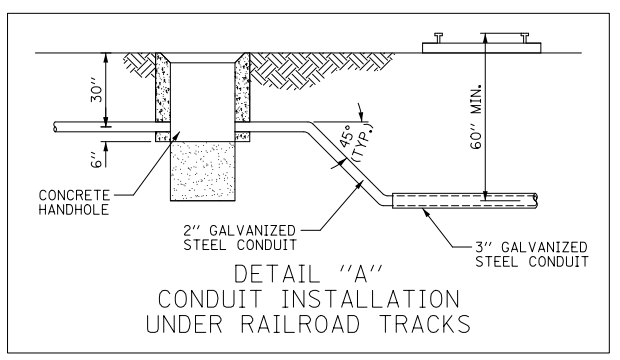


NOTES FOR TEMPORARY TRAFFIC SIGNALS (CONTINUED)

9. DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER.
10. WHEN PAN, TILT, ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.
11. THE TEMPORARY TRAFFIC SIGNAL SHALL NOT BE CONSTRUCTED UNTIL ALL PROPOSED FOUNDATION LOCATIONS HAVE BEEN IDENTIFIED, MARKED AND LOCATED TO AVOID ALL UTILITY CONFLICTS.
12. THESE PROPOSED TRAFFIC SIGNAL ITEMS SHALL BE INSTALLED WITH THE TEMPORARY TRAFFIC SIGNAL TO MAINTAIN CONNECTION TO THE RAILROAD CONTROLLER.
13. THE 2" CONDUIT IS TO BE PLACED WITHIN A 3" CONDUIT. THE 3" CONDUIT WILL ACT AS A SLEEVE FOR THE 2" CONDUIT. ALL CONDUIT BENEATH THE RAILROAD TRACKS SHALL BE PLACED A MINIMUM OF 5 FEET BELOW TOP OF RAIL. (SEE DETAIL "A")

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- | | | |
|---|------|-----------------------------------------------|
| 4 | EACH | SIGNAL POST |
| 4 | EACH | ALUMINUM MAST ARM ASSEMBLY AND POLE |
| 3 | EACH | SIGNAL HEAD, 1-FACE, 3-SECTION |
| 1 | EACH | SIGNAL HEAD, 1-FACE, 4-SECTION |
| 3 | EACH | SIGNAL HEAD, 2-FACE, 3-SECTION |
| 1 | EACH | SIGNAL HEAD, 2-FACE, 1-3 SECTION, 1-4 SECTION |
| 4 | EACH | TRAFFIC SIGNAL BACKPLATE |
| 1 | EACH | CONTROLLER AND CABINET, COMPLETE |
| 1 | EACH | SERVICE INSTALLATION |
| 2 | EACH | ILLUMINATED NO LEFT TURN SIGN |
| 2 | EACH | ILLUMINATED NO RIGHT TURN SIGN |



- | | |
|-------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------|
| (A) | (B) |
| | |
| L.E.D. INTERNALLY ILLUMINATED BLANK OUT SIGN R3-1 24"x24" (TYP.) (2 REQUIRED) | L.E.D. INTERNALLY ILLUMINATED BLANK OUT SIGN R3-2 24"x24" (TYP.) (2 REQUIRED) |

MATCH LINE STA. 536+50
SEE SHEET 2 OF 2



| | | |
|-----------------------------|-------------------|-----------|
| USER NAME = jrt | DESIGNED - BRD | REVISED - |
| PLOT SCALE = 40.0000' / in. | DRAWN - JRT | REVISED - |
| PLOT DATE = 10/10/2014 | CHECKED - JJE | REVISED - |
| | DATE - 10/10/2014 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

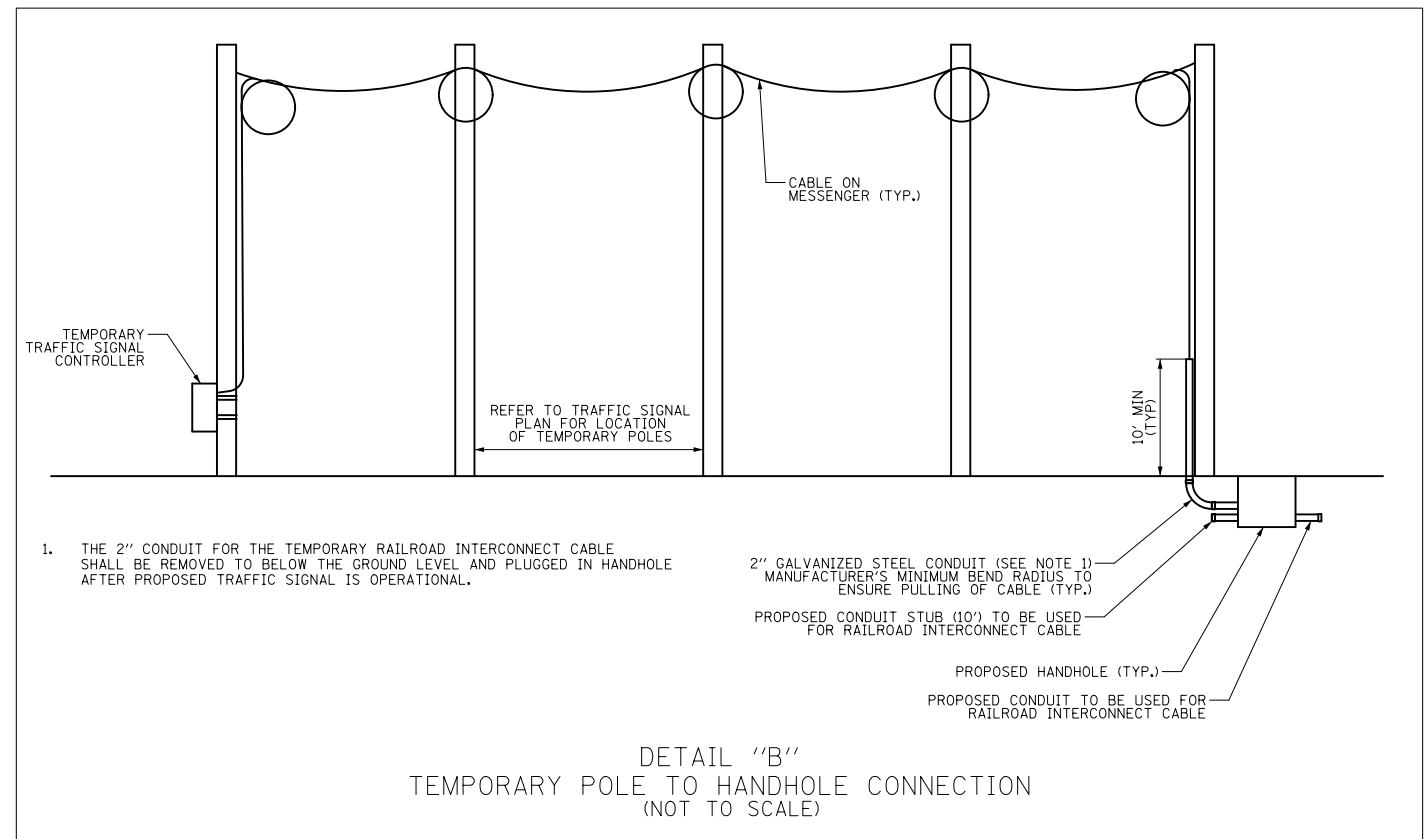
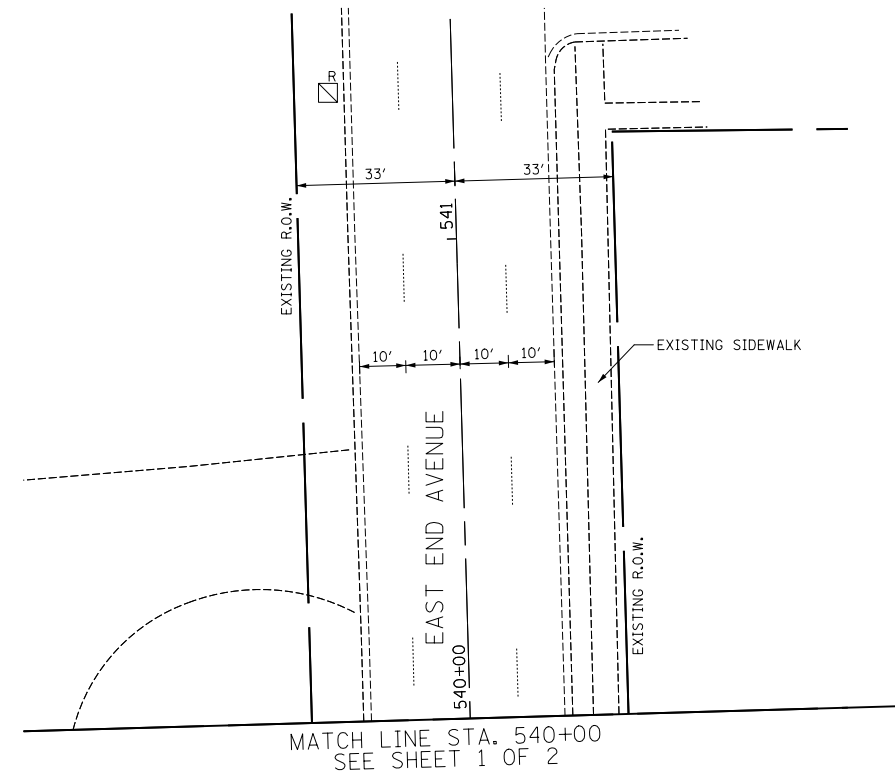
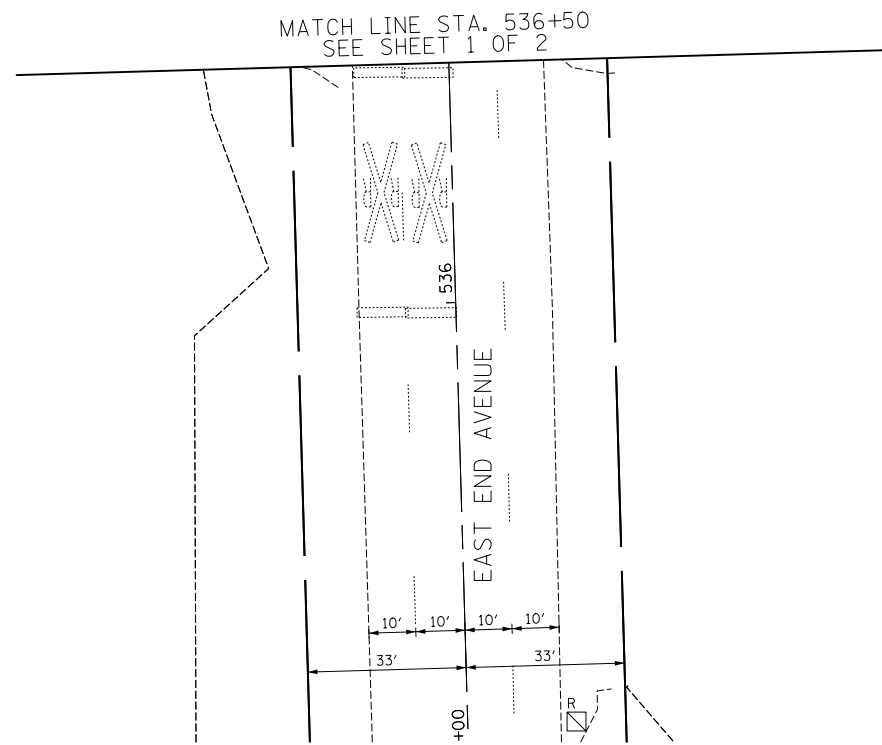
TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVAL PLAN
EAST END AVENUE AND 26TH STREET

SCALE: 1" = 20' SHEET NO. 1 OF 2 SHEETS STA. TO STA.

| | | | | |
|---------------------------|------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2887 | 3155-N(13) | COOK | 88 | 49 |
| CONTRACT NO. 60W80 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

TS 3740

WHEN GRADE CROSSINGS EXIST EITHER WITHIN OR IN THE VICINITY OF A TTC ZONE, LANE RESTRICTIONS, FLAGGING, OR OTHER OPERATIONS SHALL NOT CREATE CONDITIONS WHERE VEHICLES CAN BE QUEUED ACROSS THE TRACKS. IF THE QUEUING OF VEHICLES ACROSS THE TRACKS CANNOT BE AVOIDED, A UNIFORMED LAW ENFORCEMENT OFFICER OR FLAGGER SHALL BE PROVIDED AT THE CROSSING TO PREVENT VEHICLES FROM STOPPIN ON THE TRACKS, EVEN IF AUTOMATIC WARNING DEVICES ARE IN PLACE.



TS 3740

| | | | |
|--|-----------------------------|-------------------|-----------|
| | USER NAME = jrt | DESIGNED - BRD | REVISED - |
| | | DRAWN - JRT | REVISED - |
| | PLOT SCALE = 40.0000' / in. | CHECKED - JJE | REVISED - |
| | PLOT DATE = 10/10/2014 | DATE - 10/10/2014 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

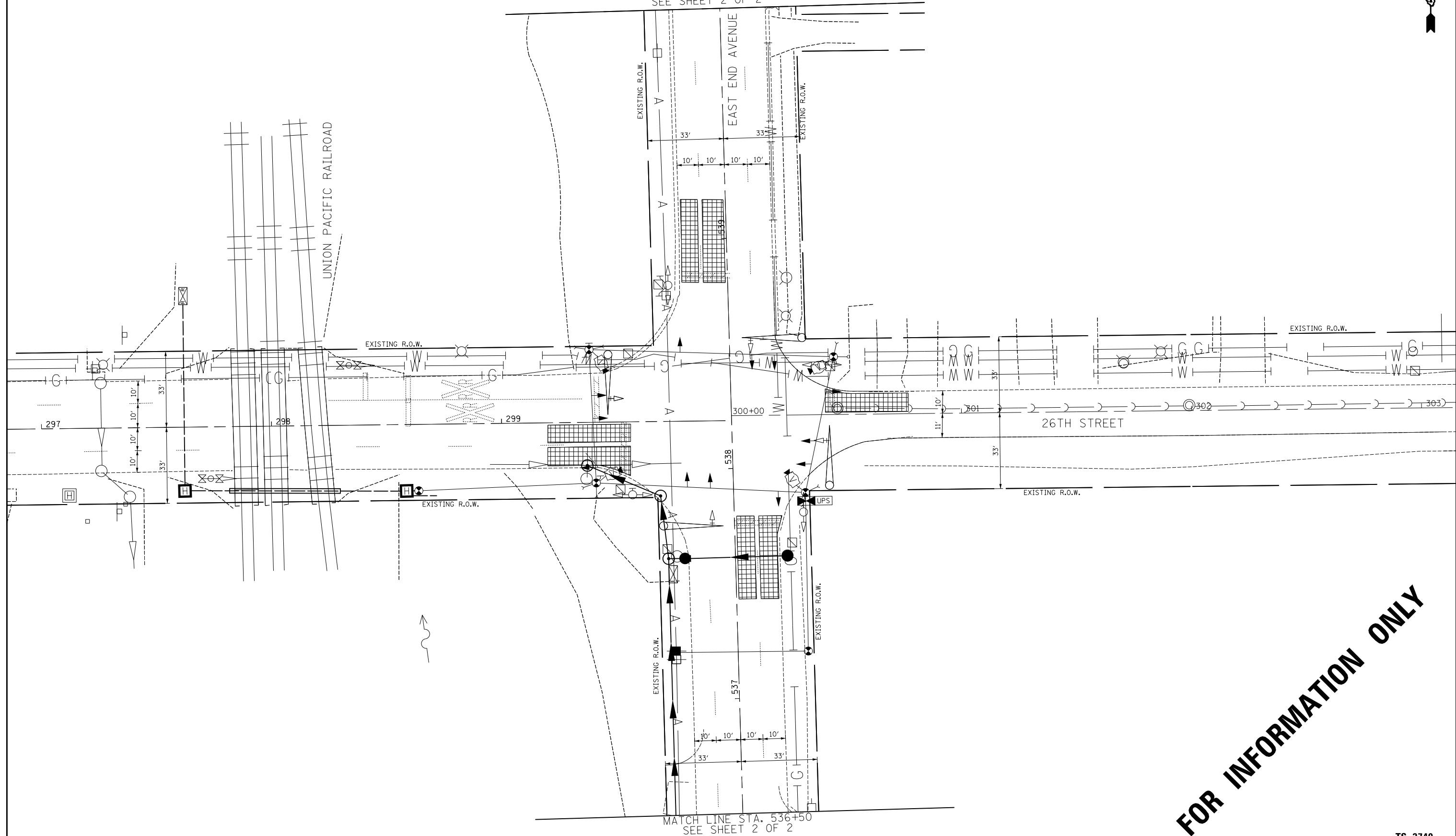
TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVAL PLAN
EAST END AVENUE AND 26TH STREET

SCALE: 1" = 20' SHEET NO. 2 OF 2 SHEETS STA. TO STA.

| | | | | |
|--------------------|------------|--------|---------------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2887 | 3155-N(13) | COOK | 88 | 50 |
| CONTRACT NO. 60W80 | | | ILLINOIS FED. AID PROJECT | |



MATCH LINE STA. 540+00
SEE SHEET 2 OF 2



MATCH LINE STA. 536+50
SEE SHEET 2 OF 2

FOR INFORMATION ONLY

TS 3740



| | | |
|-----------------------------|-------------------|-----------|
| USER NAME = jrt | DESIGNED - BRD | REVISED - |
| PLOT SCALE = 40.0000' / in. | DRAWN - JRT | REVISED - |
| PLOT DATE = 10/10/2014 | CHECKED - JJE | REVISED - |
| | DATE - 10/10/2014 | REVISED - |

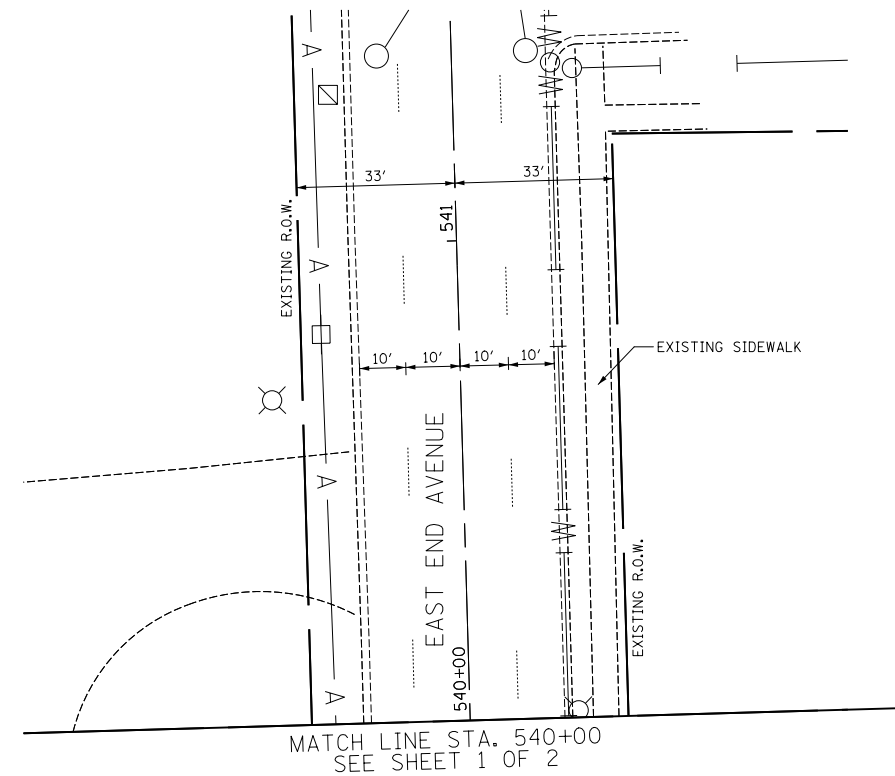
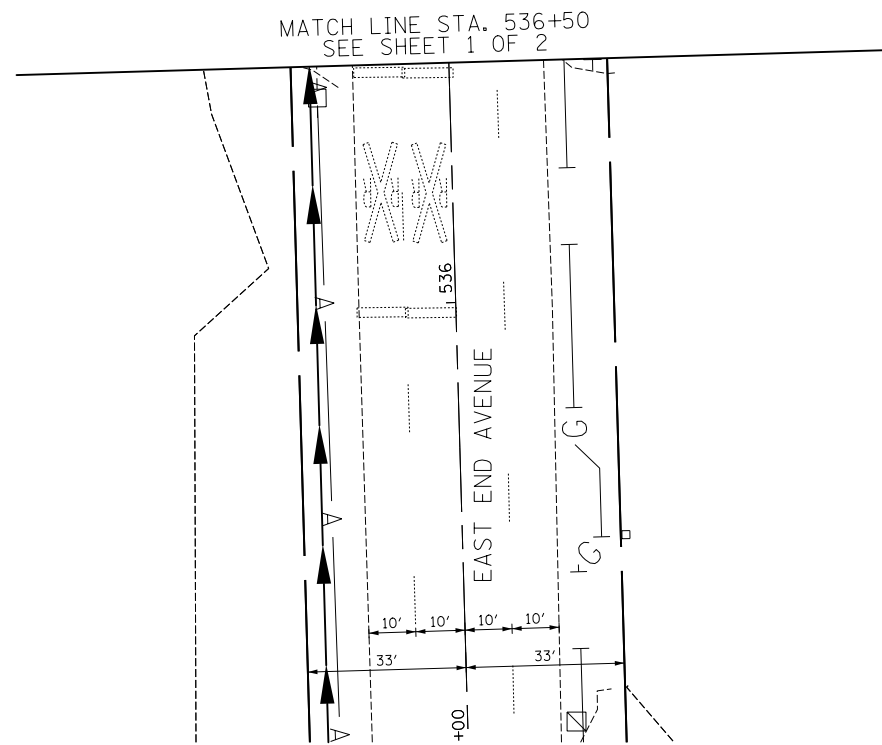
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVAL PLAN
EAST END AVENUE AND 26TH STREET**

SCALE: 1" = 20' SHEET NO. 1 OF 2 SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|------------|--------|--------------|-----------|
| 2887 | 3155-N(13) | COOK | 88 | 51 |
| CONTRACT NO. 60W80 | | | | |

ILLINOIS FED. AID PROJECT



FOR INFORMATION ONLY

TS 3740



| | | |
|-----------------------------|-------------------|-----------|
| USER NAME = jrt | DESIGNED - BRD | REVISED - |
| | DRAWN - JRT | REVISED - |
| PLOT SCALE = 40.0000' / in. | CHECKED - JJE | REVISED - |
| PLOT DATE = 10/10/2014 | DATE - 10/10/2014 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVAL PLAN
EAST END AVENUE AND 26TH STREET**

SCALE: 1" = 20' SHEET NO. 2 OF 2 SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|------------|--------|--------------|--------------------|
| 2887 | 3155-N(13) | COOK | 88 | 52 |
| | | | | CONTRACT NO. 60W80 |
| ILLINOIS FED. AID PROJECT | | | | |

TEMPORARY SEQUENCE OF OPERATION

| MOVEMENT | 2 + 6 | | 4 + 8 | | | | F | L | A | S | H |
|-----------------------------|-------|----|-------|---|----|----|---|---|---|---|---|
| PHASE | 1 | 2A | 2B | 3 | 4A | 4B | | | | | |
| EAST END AVENUE ALL SIGNALS | NB | G | Y | R | R | R | R | R | R | R | R |
| EAST END AVENUE ALL SIGNALS | SB | G | Y | R | R | R | R | R | R | R | R |
| 26TH STREET FAR SIGNALS | EB | R | R | R | G | Y | R | R | | | |
| 26TH STREET NEAR SIGNAL | EB | R | R | R | G | Y | R | R | | | |
| 26TH STREET ALL SIGNALS | WB | R | R | R | G | Y | R | R | | | |

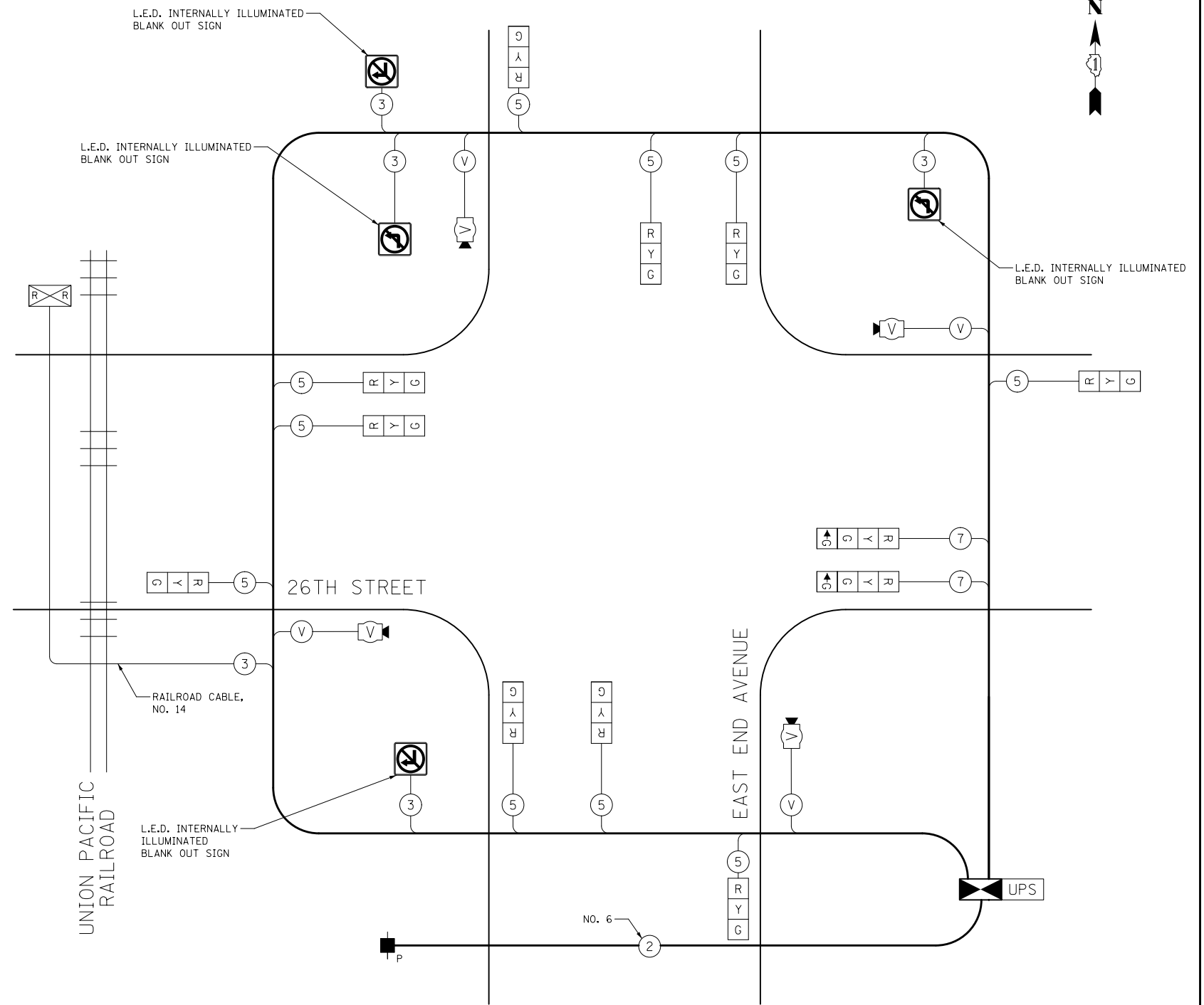
NOTE: PHASES 2 AND 6 SHALL BE PLACED ON RECALL.

TEMPORARY RAILROAD PREEMPTION SEQUENCE OF OPERATION

| CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER | 1 | | 3 | | PREEMPTOR NUMBER 2 | | | | | CLEAR TO NORMAL SEQUENCE |
|---------------------------------------------------------------------|-----|-----|-----|-----|--------------------|-----|-----|-----|-----|--------------------------|
| | 1A | 1B | 1C | 1D | 2 | 3 | 4 | 5 | | |
| RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER | 1A | 1B | 1C | 1D | 2 | 3 | 4 | 5 | | |
| CHANGE TO RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER | 1B | 2 | 1D | 2 | 3 | 4 | 5 | | | |
| EAST END AVENUE ALL SIGNALS | NB | Y | R | R | R | R | R | R | G | |
| EAST END AVENUE ALL SIGNALS | SB | Y | R | R | R | R | R | R | G | |
| 26TH STREET FAR SIGNALS | EB | R | R | G | G | G | Y | R | R | |
| 26TH STREET NEAR SIGNAL | EB | R | R | G | G | G | Y | R | R | |
| 26TH STREET ALL SIGNALS | WB | R | R | Y | R | R | R | R | R | |
| INTERNALLY ILLUMINATED NO RIGHT TURN SIGNS | NRT | NRT | NRT | NRT | NRT | NRT | NRT | NRT | NRT | |
| INTERNALLY ILLUMINATED NO LEFT TURN SIGNS | NLT | NLT | NLT | NLT | NLT | NLT | NLT | NLT | NLT | |

△ RAILROAD PREEMPTION SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL SEQUENCE OF OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY AN EMERGENCY VEHICLE PREEMPTION INTERVAL (IF APPLICABLE) AFTER RAILROAD PREEMPTION INTERVAL 5 IS TERMINATED.

NRT = "NO RIGHT TURN" OR NLT = "NO LEFT TURN" OR



TEMPORARY CABLE PLAN
NOT TO SCALE

| I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS | | | | | TOTAL WATTAGE |
|----------------------------------------------------------------------|-----------|-----------------|-----|-------------|---------------|
| TYPE | NO. LAMPS | WATTAGE INCAND. | LED | % OPERATION | |
| SIGNAL (RED) | 12 | | 17 | 0.50 | 102 |
| (YELLOW) | 12 | | 25 | 0.25 | 75 |
| (GREEN) | 12 | | 15 | 0.25 | 45 |
| ARROW | 2 | | 12 | 0.10 | 2 |
| PED. SIGNAL | 0 | | 25 | 1.00 | |
| CONTROLLER | 1 | | 100 | 1.00 | 100 |
| VIDEO SYSTEM | 1 | | 150 | 1.00 | 150 |
| BLANK OUT SIGN | 4 | | 25 | .05 | 5 |
| FLASHER | | | | 0.50 | |
| TOTAL = | | | | | 478 |

ENERGY COSTS TO: ILLINOIS DEPARTMENT OF TRANSPORTATION
201 WEST CENTER COURT
SCHAUMBURG, IL 60196-1096

ENERGY SUPPLY: CONTACT: DEB KOPESHKE
PHONE: (708) 235-2331
COMPANY: COM ED

| | | |
|-----------------------------|-------------------|-----------|
| USER NAME = jrt | DESIGNED - BRD | REVISED - |
| PLOT SCALE = 40.0000' / in. | DRAWN - JRT | REVISED - |
| PLOT DATE = 10/10/2014 | CHECKED - JJE | REVISED - |
| | DATE - 10/10/2014 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY CABLE PLAN, TEMPORARY SEQUENCE OF OPERATION,
AND TEMPORARY RAILROAD PREEMPTION SEQUENCE OF OPERATION
EAST END AVENUE AND 26TH STREET

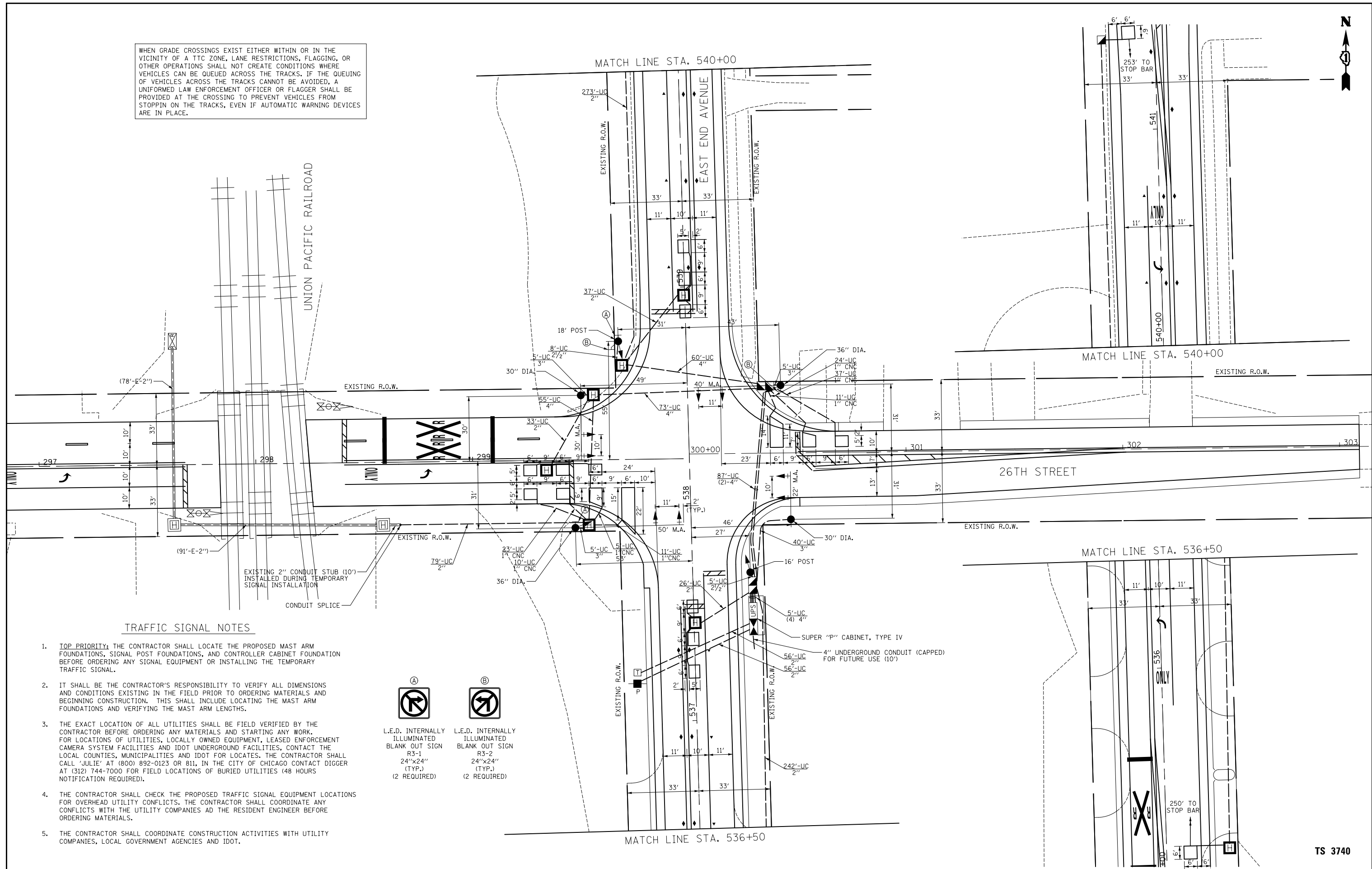
NOT TO SCALE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| | | | | |
|---------------------------|--------------------|-------------|-----------------|--------------|
| F.A.U. RTE. 2887 | SECTION 3155-N(13) | COUNTY COOK | TOTAL SHEETS 88 | SHEET NO. 53 |
| CONTRACT NO. 60W80 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

TS 3740

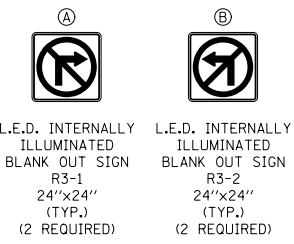


WHEN GRADE CROSSINGS EXIST EITHER WITHIN OR IN THE VICINITY OF A TTC ZONE, LANE RESTRICTIONS, FLAGGING, OR OTHER OPERATIONS SHALL NOT CREATE CONDITIONS WHERE VEHICLES CAN BE QUEUED ACROSS THE TRACKS. IF THE QUEUING OF VEHICLES ACROSS THE TRACKS CANNOT BE AVOIDED, A UNIFORMED LAW ENFORCEMENT OFFICER OR FLAGGER SHALL BE PROVIDED AT THE CROSSING TO PREVENT VEHICLES FROM STOPPING ON THE TRACKS, EVEN IF AUTOMATIC WARNING DEVICES ARE IN PLACE.

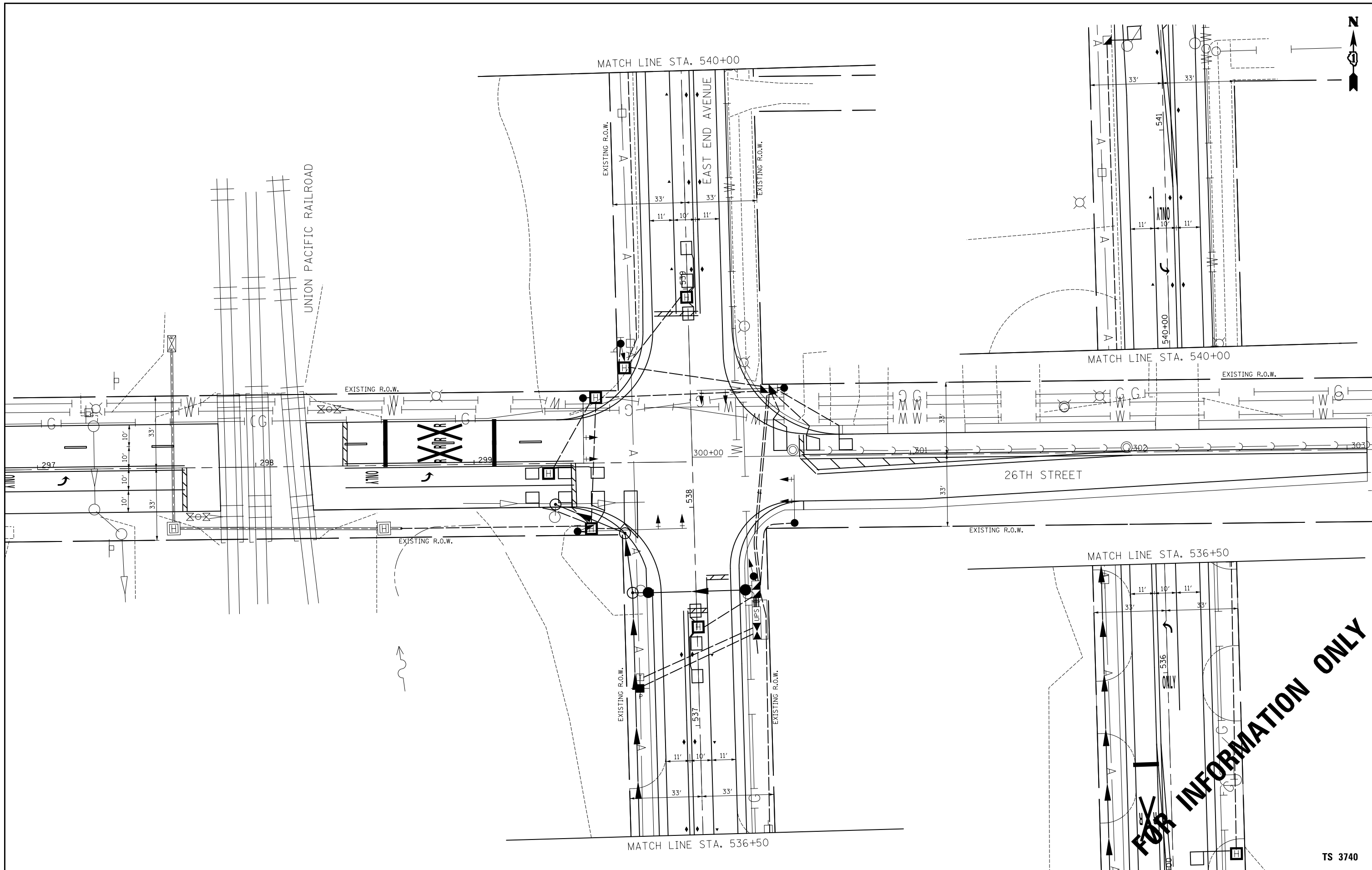


TRAFFIC SIGNAL NOTES

1. **IOP PRIORITY:** THE CONTRACTOR SHALL LOCATE THE PROPOSED MAST ARM FOUNDATIONS, SIGNAL POST FOUNDATIONS, AND CONTROLLER CABINET FOUNDATION BEFORE ORDERING ANY SIGNAL EQUIPMENT OR INSTALLING THE TEMPORARY TRAFFIC SIGNAL.
2. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARM LENGTHS.
3. THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES. THE CONTRACTOR SHALL CALL 'JULIE' AT (800) 892-0123 OR 811, IN THE CITY OF CHICAGO CONTACT DIGGER AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION REQUIRED).
4. THE CONTRACTOR SHALL CHECK THE PROPOSED TRAFFIC SIGNAL EQUIPMENT LOCATIONS FOR OVERHEAD UTILITY CONFLICTS. THE CONTRACTOR SHALL COORDINATE ANY CONFLICTS WITH THE UTILITY COMPANIES AND THE RESIDENT ENGINEER BEFORE ORDERING MATERIALS.
5. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, LOCAL GOVERNMENT AGENCIES AND IDOT.




TS 3740



FOR INFORMATION ONLY

TS 3740

| | | | | | | | | | | |
|-------------------------------------------------------------------------------------|-----------------------------|-------------------|-----------|-----------------------------------------------------------|------------------------------------------------------------------------------|---------------------------|--------------------|-------------------------|-----------------|--------------------|
|  | USER NAME = jrt | DESIGNED - BRD | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TRAFFIC SIGNAL MODERNIZATION PLAN EAST END AVENUE AND 26TH STREET | F.A.U. RTE. 2887 | SECTION 3155-N(13) | COUNTY COOK | TOTAL SHEETS 88 | SHEET NO. 55 |
| | PLOT SCALE = 40.0000' / in. | CHECKED - JJE | REVISED - | | | REVISED - | SCALE: 1" = 20' | SHEET NO. 1 OF 1 SHEETS | STA. TO STA. | CONTRACT NO. 60W80 |
| | PLOT DATE = 10/10/2014 | DATE - 10/10/2014 | REVISED - | | | ILLINOIS FED. AID PROJECT | | | | |

PROPOSED SEQUENCE OF OPERATION

| MOVEMENT | 1 | | | | 6 1 | | 5 2 | | 6 | | 7 | | 8 | | F L A S H | | | | | | |
|------------------------------------------------------|-------|---------|---------|---------|---------|-----|-------|-----|-------|-----|-------|----|-------|-----|-----------------------|-----|-----|-----|-----|-----|-----|
| PHASE | 1 + 5 | | | | 1 + 6 | | 2 + 5 | | 2 + 6 | | 4 + 7 | | 4 + 8 | | | | | | | | |
| INTERVAL | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | 10A | 10B | 11 | 12A | 12B | | 13 | 14 | 15A | 15B | | |
| CHANGE TO | | 1+6 | 2+5 | 2+6 | 2+6 | 2+6 | 2+6 | 2+6 | 4+7 | 4+8 | | | 1+5 | 1+6 | 2+5 | 2+6 | 4+8 | 1+5 | 1+6 | 2+5 | 2+6 |
| EAST END AVENUE END MAST ARM AND FAR LEFT SIGNALS | NB | R ←G | R ←Y | R ←G | R ←Y | R | R | ←G | ←Y | G | Y | R | R | R | R | R | R | R | R | R | R |
| EAST END AVENUE RIGHT MAST ARM MOUNTED SIGNAL | NB | R | R | R | R | R | R | G | G | G | Y | R | R | R | R | R | R | R | R | R | R |
| EAST END AVENUE END MAST ARM AND FAR LEFT SIGNALS | SB | R ←G | R ←G | R ←Y | R ←Y | ←G | ←Y | R | R | G | Y | R | R | R | R | R | R | R | R | R | R |
| EAST END AVENUE RIGHT MAST ARM MOUNTED SIGNAL | SB | R | R | R | R | G | G | R | R | G | Y | R | R | R | R | R | R | R | R | R | R |
| 26TH STREET END MAST ARM AND FAR LEFT SIGNALS | EB | R | R | R | R | R | R | R | R | R | R | R | ←G | Y | R | ←Y | G | Y | R | R | R |
| 26TH STREET RIGHT MAST ARM MOUNTED SIGNAL | EB | R | R | R | R | R | R | R | R | R | R | R | G | Y | R | G | G | Y | R | R | R |
| 26TH STREET ALL SIGNALS | WB | R | R | R | R | R | R | R | R | R | R | R | R | R | R | G | Y | R | R | R | R |

NOTE: PHASES 2 AND 6 SHALL BE PLACED ON RECALL.

PROPOSED RAILROAD PREEMPTION SEQUENCE OF OPERATION


| | | | | | | | | | | | | | | | PREEMPTOR NUMBER 2 | | | | | |
|------------------------------------------------------------------------|-----|---------|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|--------------------------------|---|--|--|--|--|
| CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER | 1 | 5 | 7 | 9 | 11 | 14 | | | | | | | | | | | | | | |
| CHANGE FROM EMERGENCY VEHICLE PREEMPTION SEQUENCE INTERVAL NUMBER | | | | | | | | | | | | | | | | | | | | |
| RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER | 1A | 1B | 1C | 1D | 1E | 1F | 1G | 1H | 1J | 1K | 2 | 3 | 4 | 5 | CLEAR TO NORMAL SEQUENCE | | | | | |
| CHANGE TO RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER | 2 | 1C | 2 | 1E | 2 | 1G | 2 | 2 | 1K | 2 | 3 | 4 | 5 | | | | | | | |
| EAST END AVENUE END MAST ARM AND FAR LEFT SIGNALS | NB | R ←Y | R | R | Y | R | Y | R | R | R | R | R | R | R | G | △ | | | | |
| EAST END AVENUE RIGHT MAST ARM MOUNTED SIGNAL | NB | R | R | R | Y | R | Y | R | R | R | R | R | R | R | G | △ | | | | |
| EAST END AVENUE END MAST ARM AND FAR LEFT SIGNALS | SB | R ←Y | Y | R | R | R | Y | R | R | R | R | R | R | R | G | △ | | | | |
| EAST END AVENUE RIGHT MAST ARM MOUNTED SIGNAL | SB | R | Y | R | R | R | Y | R | R | R | R | R | R | R | G | △ | | | | |
| 26TH STREET END MAST ARM AND FAR LEFT SIGNALS | EB | R | R | R | R | R | R | R | ←G | G | G | ←G | Y | R | R | △ | | | | |
| 26TH STREET RIGHT MAST ARM MOUNTED SIGNAL | EB | R | R | R | R | R | R | R | G | G | G | G | Y | R | R | △ | | | | |
| 26TH STREET ALL SIGNALS | WB | R | R | R | R | R | R | R | Y | R | R | R | R | R | R | △ | | | | |
| INTERNALLY ILLUMINATED NO RIGHT TURN SIGNS | NRT | NRT | NRT | NRT | NRT | NRT | NRT | NRT | NRT | NRT | NRT | NRT | NRT | NRT | NRT | △ | | | | |
| INTERNALLY ILLUMINATED NO LEFT TURN SIGNS | NLT | NLT | NLT | NLT | NLT | NLT | NLT | NLT | NLT | NLT | NLT | NLT | NLT | NLT | NLT | △ | | | | |

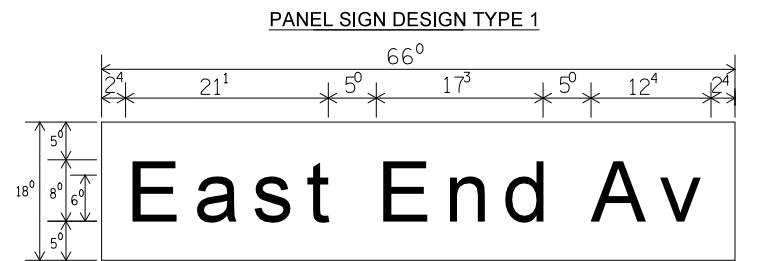
△ RAILROAD PREEMPTION SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL SEQUENCE OF OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY AN EMERGENCY VEHICLE PREEMPTION INTERVAL (IF APPLICABLE) AFTER RAILROAD PREEMPTION INTERVAL 5 IS TERMINATED.

NRT = "NO RIGHT TURN" OR  NLT = "NO LEFT TURN" OR 

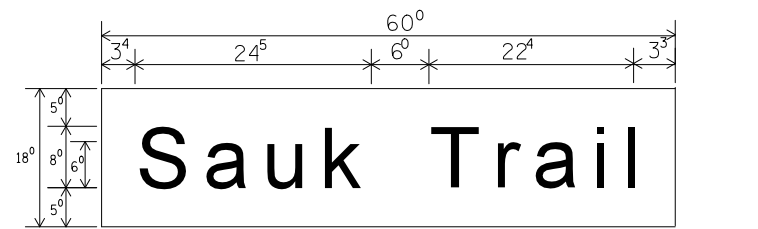
HOLD

TS 3740

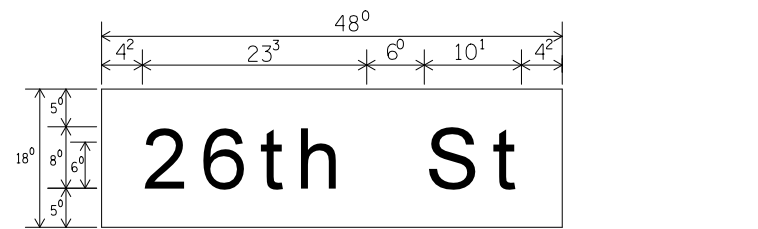
| | | | | | | | | | | | | |
|-------------------------------------------------------------------------------------|-----------------------------|-------------------|-----------|-----------------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------|------|---------|--------------------|---------------------------|--------|-----------------|--------------|
|  | USER NAME = jrt | DESIGNED - BRD | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SEQUENCE OF OPERATION AND RAILROAD PREEMPTION SEQUENCE OF OPERATION EAST END AVENUE AT 26TH STREET | | | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | PLOT SCALE = 48.0000' / in. | CHECKED - JJE | REVISED - | | | | | 2887 | 3155-N(13) | COOK | 88 | 57 |
| | PLOT DATE = 10/10/2014 | DATE - 10/10/2014 | REVISED - | | | | | CONTRACT NO. 60W80 | | | | |
| | | | | SCALE: NO SCALE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. | | ILLINOIS FED. AID PROJECT | | | |



___ Sq. M Each 8.25 Sq. Ft. Each 4 Required Design Series_D



___ Sq. M Each 7.5 Sq. Ft. Each 2 Required Design Series_D



___ Sq. M Each 6 Sq. Ft. Each 2 Required Design Series_D

NOTE: SIGN DIMENSIONS ARE IN ENGLISH UNITS.

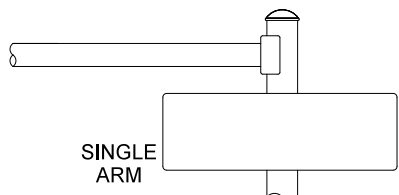
GENERAL NOTES

- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 877001, 877002, 877006, 877011, AND 877012, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 8'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
- ALL SIGNS SHALL HAVE A WHITE REFLECTORIZED LEGEND AND BORDER ON A GREEN REFLECTORIZED BACKGROUND, TYPE A SHEETING.
- THE SIGN LENGTH SHOULD BE INCREASED IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHOULD NOT EXCEED 8'-0".
- ALL BORDERS SHALL BE 3/4" WIDE AND CORNER RADIUS SHALL BE 2/4".
- SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS. LOCAL SUPPLIERS OF THE SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM ARE:

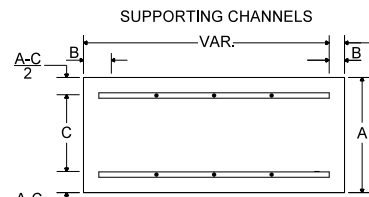
- J.O HERBERT CO. MIDLOTHIAN, VA.
- WESTERN REMAC INC. WOODRIDGE, IL.

PARTS LISTING:
SIGN CHANNEL PART #HPN053 (MED. CHANNEL)
SIGN SCREWS 1/4" x 14 x 1" H.W.H. #3
BRACKETS SELF TAPPING WITH NEOPRENE WASHER
PART #HPN034 (UNIVERSAL)
CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING

OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BACKET OF THE ABOVE PRODUCT.



SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM shall be used. See Note #5.



| | | |
|-----|----|-----|
| A | B | C |
| 18" | 2" | 12" |
| 30" | 2" | 22" |

UPPER TO LOWER CASE
SPACING CHART 8-6 INCH SERIES "C & D"

EXAMPLE, 2 3/8" DENOTES 3/8"

| SERIES | SECOND LETTER | | | | | | | | | | | | | | | |
|---------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|
| | a c d e | | b h i k | | f w | | j | | s t | | v y | | x | | z | |
| | g | o | q | l | m | n | p | r | u | | | | | | | |
| A W X | 1 ² | 1 ⁴ | 1 ⁴ | 1 ⁵ | 1 ² | 1 ⁴ | 0 ⁶ | 1 ⁰ | 1 ¹ | 1 ⁴ | 0 ⁶ | 1 ⁰ | 1 ¹ | 1 ² | 1 ² | 1 ⁴ |
| B | 1 ⁴ | 1 ⁵ | 2 ⁰ | 2 ¹ | 1 ⁴ | 1 ⁵ | 1 ¹ | 1 ² | 1 ⁴ | 1 ⁵ | 1 ² | 1 ⁴ | 1 ² | 1 ⁴ | 1 ⁶ | 1 ⁷ |
| C E G | 1 ⁴ | 1 ⁵ | 2 ⁰ | 2 ¹ | 1 ² | 1 ⁴ | 0 ⁶ | 1 ⁰ | 1 ² | 1 ⁴ | 1 ² | 1 ⁴ | 1 ⁴ | 1 ⁵ | 1 ⁴ | 1 ⁵ |
| D O Q R | 1 ⁴ | 1 ⁵ | 2 ⁰ | 2 ¹ | 1 ⁴ | 1 ⁵ | 0 ⁶ | 1 ⁰ | 1 ² | 1 ⁴ | 1 ² | 1 ⁴ | 1 ⁴ | 1 ⁵ | 1 ⁴ | 1 ⁵ |
| F | 0 ⁵ | 0 ⁶ | 1 ⁴ | 1 ⁵ | 0 ⁶ | 1 ⁰ | 0 ⁵ | 0 ⁶ | 0 ⁶ | 1 ⁰ | 0 ⁶ | 1 ⁰ | 0 ⁶ | 1 ⁰ | 1 ¹ | 1 ² |
| H I M N | 2 ⁰ | 2 ¹ | 2 ² | 2 ⁴ | 2 ⁰ | 2 ¹ | 1 ⁴ | 1 ⁵ | 1 ⁶ | 1 ⁷ | 1 ⁶ | 1 ⁷ | 2 ⁰ | 2 ¹ | 2 ⁰ | 2 ¹ |
| J U | 2 ⁰ | 2 ¹ | 2 ⁰ | 2 ¹ | 1 ⁶ | 1 ⁷ | 1 ⁴ | 1 ⁵ | 1 ⁶ | 1 ⁷ | 1 ⁶ | 1 ⁷ | 1 ⁶ | 1 ⁷ | 2 ⁰ | 2 ¹ |
| K L | 1 ¹ | 1 ² | 1 ⁶ | 1 ⁷ | 1 ¹ | 1 ² | 0 ⁵ | 0 ⁶ | 1 ¹ | 1 ² | 1 ¹ | 1 ² | 1 ¹ | 1 ² | 1 ² | 1 ⁴ |
| P | 1 ² | 1 ⁴ | 1 ⁴ | 1 ⁵ | 1 ² | 1 ⁴ | 0 ⁵ | 0 ⁶ | 1 ¹ | 1 ² | 1 ¹ | 1 ² | 1 ² | 1 ⁴ | 1 ² | 1 ⁴ |
| S | 1 ² | 1 ⁴ | 1 ⁶ | 1 ⁷ | 1 ² | 1 ⁴ | 0 ⁶ | 1 ⁰ | 1 ² | 1 ⁴ | 1 ² | 1 ⁴ | 1 ² | 1 ⁴ | 1 ² | 1 ⁴ |
| T | 1 ¹ | 1 ² | 1 ⁶ | 1 ⁷ | 0 ⁶ | 1 ⁰ | 0 ⁶ | 1 ⁰ | 1 ¹ | 1 ² | 1 ¹ | 1 ² | 1 ¹ | 1 ² | 1 ² | 1 ⁴ |
| V | 0 ⁶ | 1 ⁰ | 1 ⁴ | 1 ⁵ | 1 ¹ | 1 ² | 0 ⁶ | 1 ⁰ | 1 ² | 1 ⁴ | 1 ² | 1 ⁴ | 1 ² | 1 ⁴ | 1 ² | 1 ⁴ |
| Y | 0 ⁵ | 0 ⁶ | 1 ⁴ | 1 ⁵ | 0 ⁶ | 1 ⁰ | 0 ⁵ | 0 ⁶ | 0 ⁵ | 0 ⁷ | 0 ⁵ | 0 ⁶ | 0 ⁶ | 1 ⁰ | 1 ¹ | 1 ² |
| Z | 1 ⁶ | 1 ⁷ | 2 ² | 2 ⁴ | 1 ⁶ | 1 ⁷ | 1 ² | 1 ⁴ | 1 ⁶ | 1 ⁷ | 1 ⁶ | 1 ⁷ | 1 ⁶ | 1 ⁷ | 2 ⁰ | 2 ¹ |

LOWER CASE TO LOWER CASE
SPACING CHART 6 INCH SERIES "C" & "D"

| SERIES | SECOND LETTER | | | | | | | | | | | | | | | |
|-------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|
| | a c d e | | b h i k | | f w | | j | | s t | | v y | | x | | z | |
| | g | o | q | l | m | n | p | r | u | | | | | | | |
| a d g h | 1 ⁶ | 1 ⁷ | 2 ² | 2 ⁴ | 1 ⁶ | 1 ⁷ | 1 ² | 1 ⁴ | 1 ⁴ | 1 ⁵ | 1 ⁴ | 1 ⁵ | 1 ⁶ | 1 ⁷ | 1 ⁶ | 1 ⁷ |
| i j l m | | | | | | | | | | | | | | | | |
| n q u | | | | | | | | | | | | | | | | |
| b f k o p s | 1 ² | 1 ⁴ | 1 ⁶ | 1 ⁷ | 1 ¹ | 1 ² | 0 ⁵ | 0 ⁶ | 1 ¹ | 1 ² | 1 ¹ | 1 ² | 1 ² | 1 ⁴ | 1 ² | 1 ⁴ |
| c e | 1 ² | 1 ⁴ | 1 ⁶ | 1 ⁷ | 1 ² | 1 ⁴ | 0 ⁶ | 1 ⁰ | 1 ² | 1 ⁴ | 1 ² | 1 ⁴ | 1 ² | 1 ⁴ | 1 ² | 1 ⁴ |
| r | 0 ⁶ | 1 ⁰ | 1 ² | 1 ⁴ | 0 ⁶ | 1 ⁰ | 0 ³ | 0 ³ | 0 ⁵ | 0 ⁶ | 0 ⁵ | 0 ⁶ | 0 ⁶ | 1 ⁰ | 0 ⁶ | 1 ⁰ |
| t z | 1 ² | 1 ⁴ | 1 ⁶ | 1 ⁷ | 1 ² | 1 ⁴ | 0 ⁶ | 1 ⁰ | 1 ¹ | 1 ² | 1 ¹ | 1 ² | 1 ² | 1 ⁴ | 1 ² | 1 ⁴ |
| v y | 1 ¹ | 1 ² | 1 ⁴ | 1 ⁵ | 1 ¹ | 1 ² | 0 ⁵ | 0 ⁶ | 0 ⁶ | 1 ⁰ | 0 ⁶ | 1 ⁰ | 1 ¹ | 1 ² | 1 ¹ | 1 ² |
| w | 1 ¹ | 1 ² | 1 ⁴ | 1 ⁵ | 1 ¹ | 1 ² | 0 ⁵ | 0 ⁶ | 1 ¹ | 1 ² | 1 ¹ | 1 ² | 1 ¹ | 1 ² | 1 ² | 1 ⁴ |
| x | 1 ² | 1 ⁴ | 1 ⁶ | 1 ⁷ | 1 ¹ | 1 ² | 0 ⁵ | 0 ⁶ | 1 ¹ | 1 ² | 1 ¹ | 1 ² | 1 ¹ | 1 ² | 1 ² | 1 ⁴ |

NUMBER TO NUMBER
SPACING CHART 8 INCH SERIES "C" & "D"

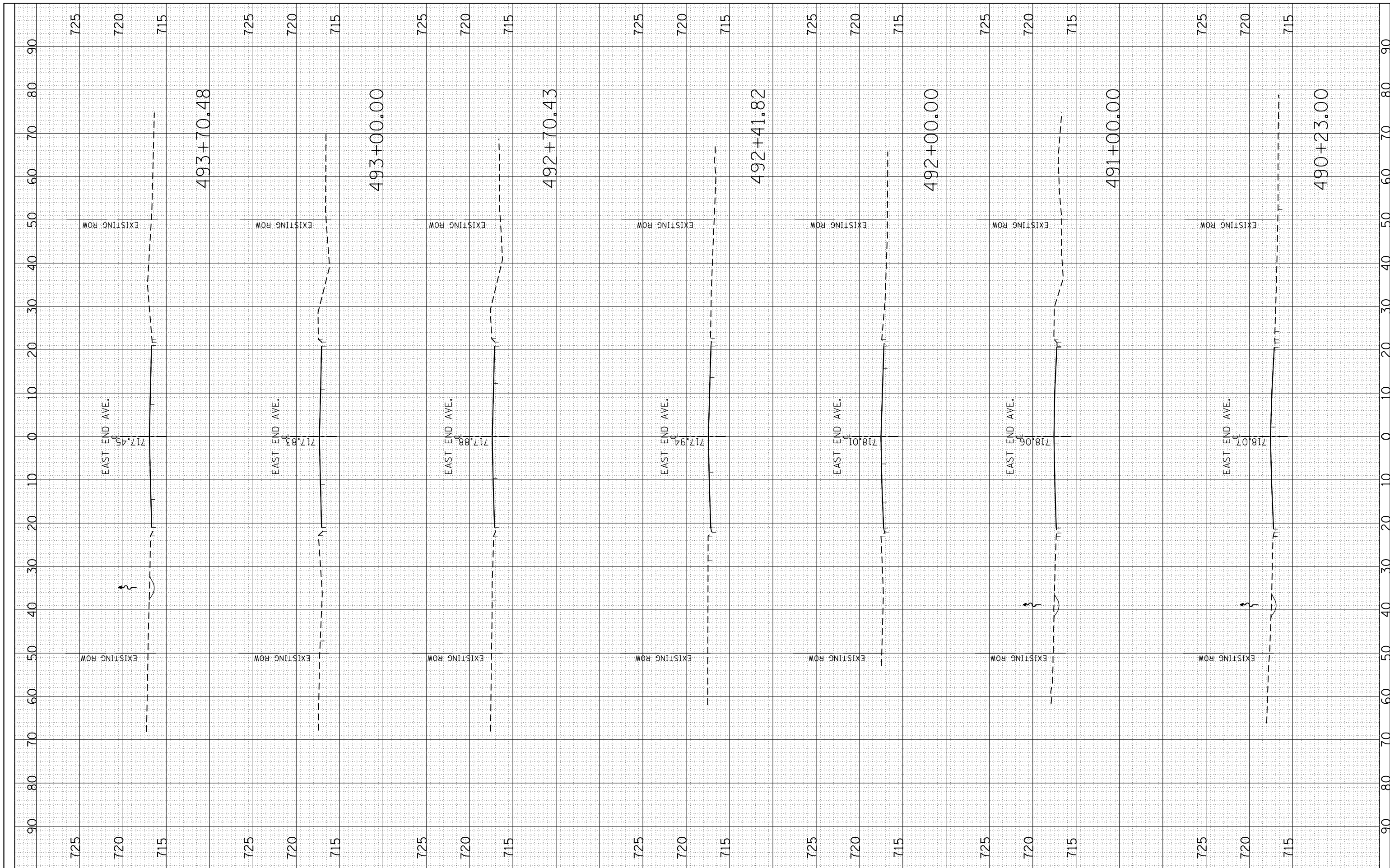
| SERIES | SECOND NUMBER | | | | | | | | | | | | | | | | | | | |
|--------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|
| | 0 | | 1 | | 2 | | 3 | | 4 | | 5 | | 6 | | 7 | | 8 | | 9 | |
| | | | | | | | | | | | | | | | | | | | | |
| 0 9 | 1 ⁶ | 1 ⁷ | 1 ⁶ | 1 ⁷ | 1 ⁴ | 1 ⁵ | 1 ² | 1 ⁴ | 1 ⁴ | 1 ⁵ | 1 ⁴ | 1 ⁵ | 1 ⁶ | 1 ⁷ | 1 ² | 1 ⁴ | 1 ⁶ | 1 ⁷ | 1 ⁶ | 1 ⁷ |
| 1 | 2 ⁰ | 2 ¹ | 2 ⁰ | 2 ¹ | 2 ⁰ | 2 ¹ | 1 ⁶ | 1 ⁷ | 1 ⁴ | 1 ⁵ | 2 ⁰ | 2 ¹ | 2 ⁰ | 2 ¹ | 1 ⁴ | 1 ⁵ | 2 ⁰ | 2 ¹ | 2 ⁰ | 2 ¹ |
| 2 3 4 | 1 ⁴ | 1 ⁵ | 1 ⁴ | 1 ⁵ | 1 ⁴ | 1 ⁵ | 1 ² | 1 ⁴ | 1 ² | 1 ⁴ | 1 ⁴ | 1 ⁵ | 1 ⁴ | 1 ⁵ | 1 ¹ | 1 ² | 1 ⁶ | 1 ⁷ | 1 ⁴ | 1 ⁵ |
| 5 | 1 ⁴ | 1 ⁵ | 1 ⁴ | 1 ⁵ | 1 ⁴ | 1 ⁵ | 1 ¹ | 1 ² | 1 ¹ | 1 ² | 1 ⁴ | 1 ⁵ | 1 ⁴ | 1 ⁵ | 1 ¹ | 1 ² | 1 ⁴ | 1 ⁵ | 1 ⁴ | 1 ⁵ |
| 6 | 1 ⁶ | 1 ⁷ | 1 ⁴ | 1 ⁵ | 1 ⁴ | 1 ⁵ | 1 ² | 1 ⁵ | 1 ² | 1 ⁴ | 1 ⁴ | 1 ⁵ | 1 ⁴ | 1 ⁵ | 1 ¹ | 1 ² | 1 ⁴ | 1 ⁵ | 1 ⁴ | 1 ⁵ |
| 7 | 1 ² | 1 ⁴ | 1 ² | 1 ⁴ | 1 ⁴ | 1 ⁵ | 1 ² | 1 ⁵ | 0 ⁵ | 0 ⁶ | 1 ² | 1 ⁴ | 1 ⁴ | 1 ⁵ | 1 ¹ | 1 ² | 1 ⁴ | 1 ⁵ | 1 ² | 1 ⁴ |
| 8 | 1 ⁶ | 1 ⁷ | 1 ⁶ | 1 ⁷ | 1 ⁴ | 1 ⁵ | 1 ² | 1 ⁵ | 1 ² | 1 ⁴ | 1 ⁴ | 1 ⁵ | 1 ⁶ | 1 ⁷ | 1 ² | 1 ⁴ | 1 ⁶ | 1 ⁷ | 1 ⁴ | 1 ⁵ |

| LETTERS | 6 INCH UPPER CASE LETTERS | | | | 8 INCH UPPER CASE LETTERS | | | | LETTERS | 6 INCH LOWER CASE LETTERS | |
|---------|---------------------------|----------------|----------------|----------------|---------------------------|----------------|----------------|---|---------|---------------------------|---|
| | SERIES | | SERIES | | SERIES | | SERIES | | | SERIES | |
| | C | D | C | D | C | D | C | D | | C | D |
| A | 3 ⁶ | 5 ⁰ | 5 ⁰ | 6 ⁵ | a | 3 ⁵ | 4 ² | | | | |
| B | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | b | 3 ⁵ | 4 ² | | | | |
| C | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | c | 3 ⁵ | 4 ¹ | | | | |
| D | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | d | 3 ⁵ | 4 ² | | | | |
| E | 3 ⁰ | 3 ⁵ | 4 ⁰ | 4 ⁷ | e | 3 ⁵ | 4 ² | | | | |
| F | 3 ⁰ | 3 ⁵ | 4 ⁰ | 4 ⁷ | f | 2 ³ | 2 ⁶ | | | | |
| G | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | g | 3 ⁵ | 4 ² | | | | |
| H | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | h | 3 ⁵ | 4 ² | | | | |
| I | 0 ⁷ | 0 ⁷ | 1 ¹ | 1 ² | i | 1 ¹ | 1 ¹ | | | | |
| J | 3 ⁰ | 3 ⁶ | 4 ⁰ | 5 ⁰ | j | 2 ⁰ | 2 ² | | | | |
| K | 3 ² | 4 ¹ | 4 ³ | 5 ⁴ | k | 3 ⁵ | 4 ² | | | | |
| L | 3 ⁰ | 3 ⁵ | 4 ⁰ | 4 ⁷ | l | 1 ¹ | 1 ¹ | | | | |
| M | 3 ⁷ | 4 ⁵ | 5 ¹ | 6 ¹ | m | 6 ⁰ | 7 ⁰ | | | | |
| N | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | n | 3 ⁵ | 4 ² | | | | |
| O | 3 ⁴ | 4 ² | 4 ⁵ | 5 ⁵ | o | 3 ⁶ | 4 ³ | | | | |
| P | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | p | 3 ⁵ | 4 ² | | | | |
| Q | 3 ⁴ | 4 ² | 4 ⁵ | 5 ⁵ | q | 3 ⁵ | 4 ² | | | | |
| R | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | r | 2 ⁶ | 3 ² | | | | |
| S | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | s | 3 ⁶ | 4 ² | | | | |
| T | 3 ⁰ | 3 ⁵ | 4 ⁰ | 4 ⁷ | t | 2 ⁷ | 3 ² | | | | |
| U | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | u | 3 ⁵ | 4 ² | | | | |
| V | 3 ⁵ | 4 ⁴ | 4 ⁷ | 6 ⁰ | v | 4 ² | 4 ⁷ | | | | |
| W | 4 ⁴ | 5 ² | 6 ⁰ | 7 ⁰ | w | 5 ⁵ | 6 ⁴ | | | | |
| X | 3 ⁴ | 4 ⁰ | 4 ⁵ | 5 ³ | x | 4 ⁴ | 5 ¹ | | | | |
| Y | 3 ⁶ | 5 ⁰ | 5 ⁰ | 6 ⁶ | y | 4 ⁶ | 5 ³ | | | | |
| Z | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | z | 3 ⁶ | 4 ³ | | | | |

| NUMBER | 6 INCH SERIES | | 8 INCH SERIES | |
|--------|----------------|----------------|----------------|----------------|
| | C | D | C | D |
| 1 | 1 ² | 1 ⁴ | 1 ⁵ | 2 ⁰ |
| 2 | 3 ² | 4 ⁰ | 4 ³ | 5 ³ |
| 3 | 3 ² | 4 ⁰ | 4 ³ | 5 ³ |
| 4 | 3 ⁵ | 4 ⁰ | 4 ⁷ | 5 ⁷ |
| 5 | 3 ² | 4 ⁰ | 4 ³ | 5 ³ |
| 6 | 3 ² | 4 ⁰ | 4 ³ | 5 ³ |
| 7 | 3 ² | 4< | | |

| | | | |
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| FINAL SURVEY NO. | SURVEYED PLOTTED AREAS CHECKED | BY | DATE |
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| ORIGINAL SURVEY NO. | SURVEYED PLOTTED AREAS CHECKED | BY | DATE |
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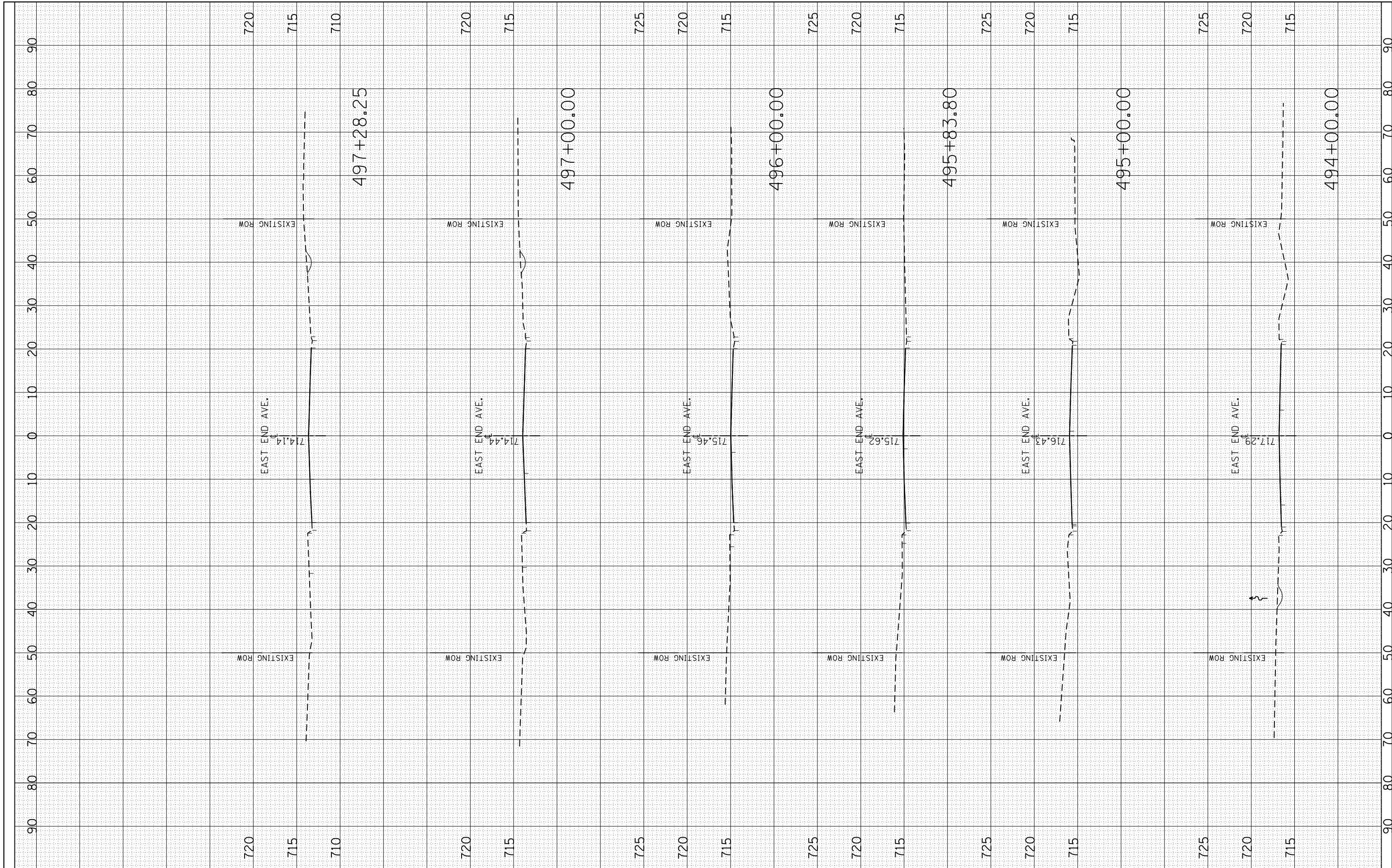
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**EAST END AVE.
 26th ST. to 30th ST.**
 SCALE: SHEET OF SHEETS STA. TO STA.

| | | | | |
|---------------------------|--------------------|-------------|-----------------|--------------|
| F.A.U. RTE. 2887 | SECTION 3155-N(13) | COUNTY COOK | TOTAL SHEETS 88 | SHEET NO. 60 |
| CONTRACT NO. 60W80 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

| | | | |
|--------------|----------|----|------|
| FINAL SURVEY | SURVEYED | BY | DATE |
| NOTE BOOK | PLOTTED | | |
| NO. | TEMPLATE | | |
| | AREAS | | |
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| ORIGINAL SURVEY | SURVEYED | BY | DATE |
| NOTE BOOK | PLOTTED | | |
| NO. | TEMPLATE | | |
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

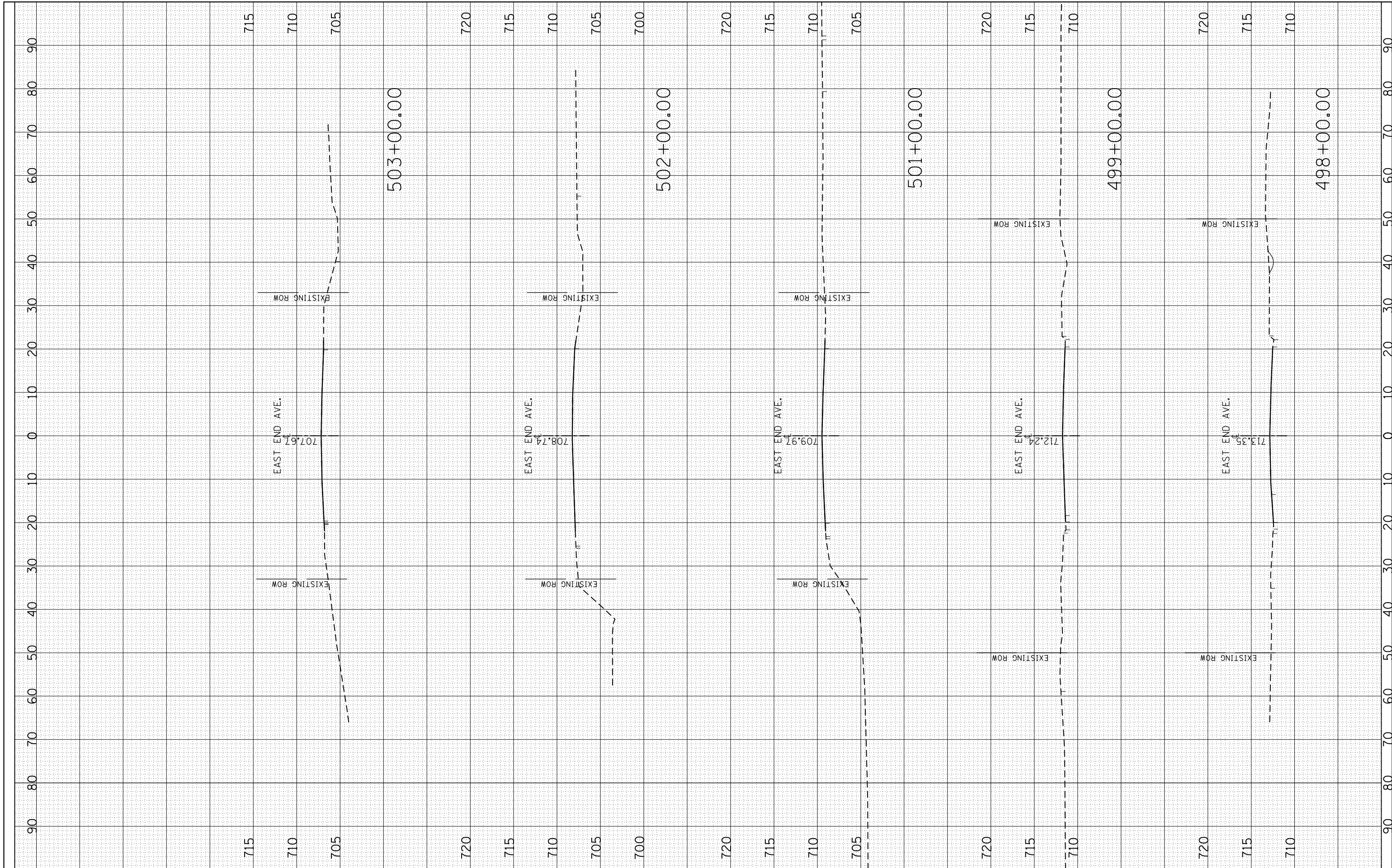
**EAST END AVE.
 26th ST. to 30th ST.**

SCALE: SHEET OF SHEETS STA. TO STA.

| | | | | |
|---------------------------|------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2887 | 3155-N(13) | COOK | 88 | 61 |
| CONTRACT NO. 60W80 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

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| FINAL SURVEY | SURVEYED | BY | DATE |
| NOTE BOOK | PLOTTED | | |
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| ORIGINAL SURVEY | SURVEYED | BY | DATE |
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

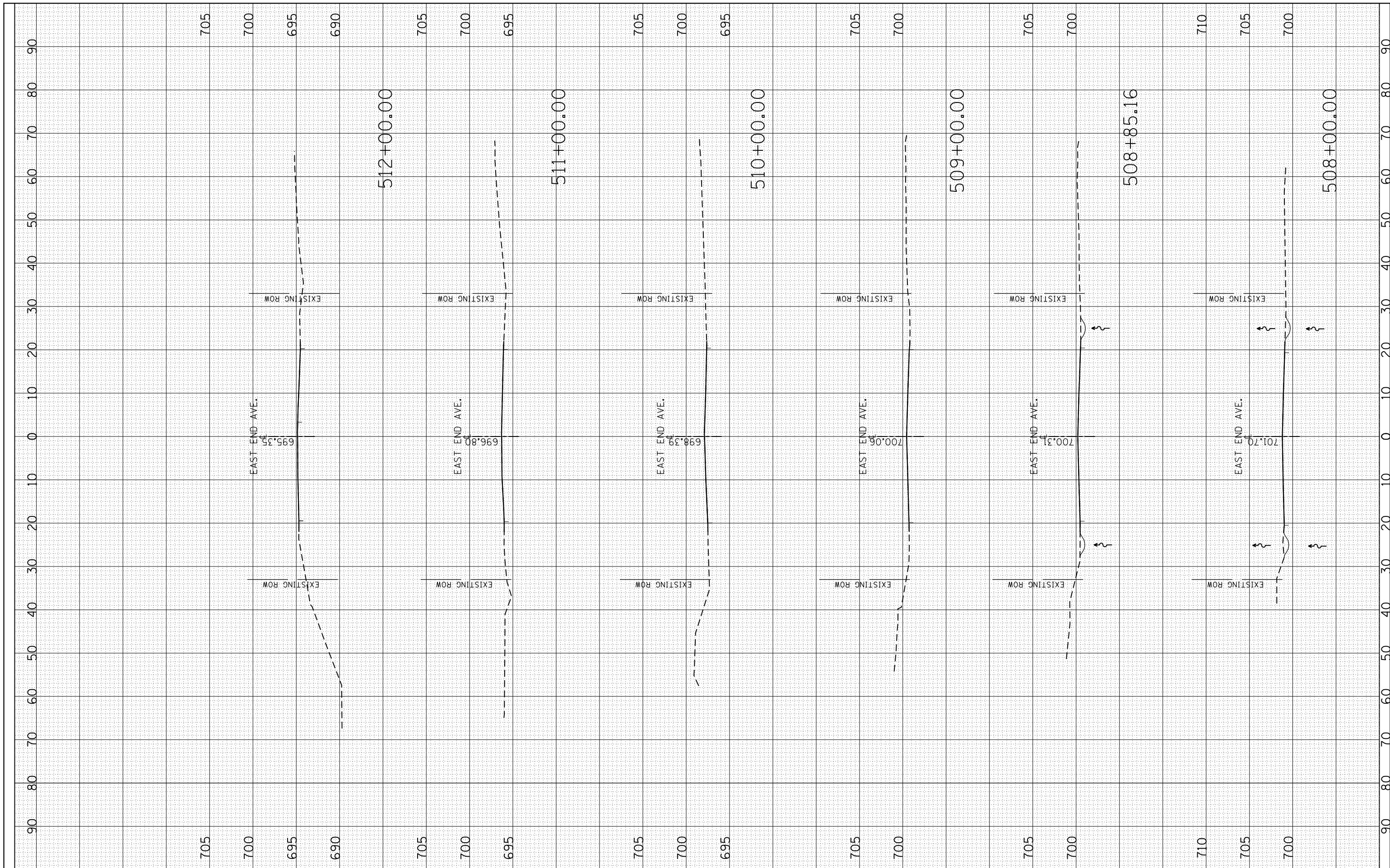
**EAST END AVE.
 26th ST. to 30th ST.**

SCALE: SHEET OF SHEETS STA. TO STA.

| | | | | |
|---------------------------|------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2887 | 3155-N(13) | COOK | 88 | 62 |
| CONTRACT NO. 60W80 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

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| FINAL SURVEY | SURVEYED | BY | DATE |
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

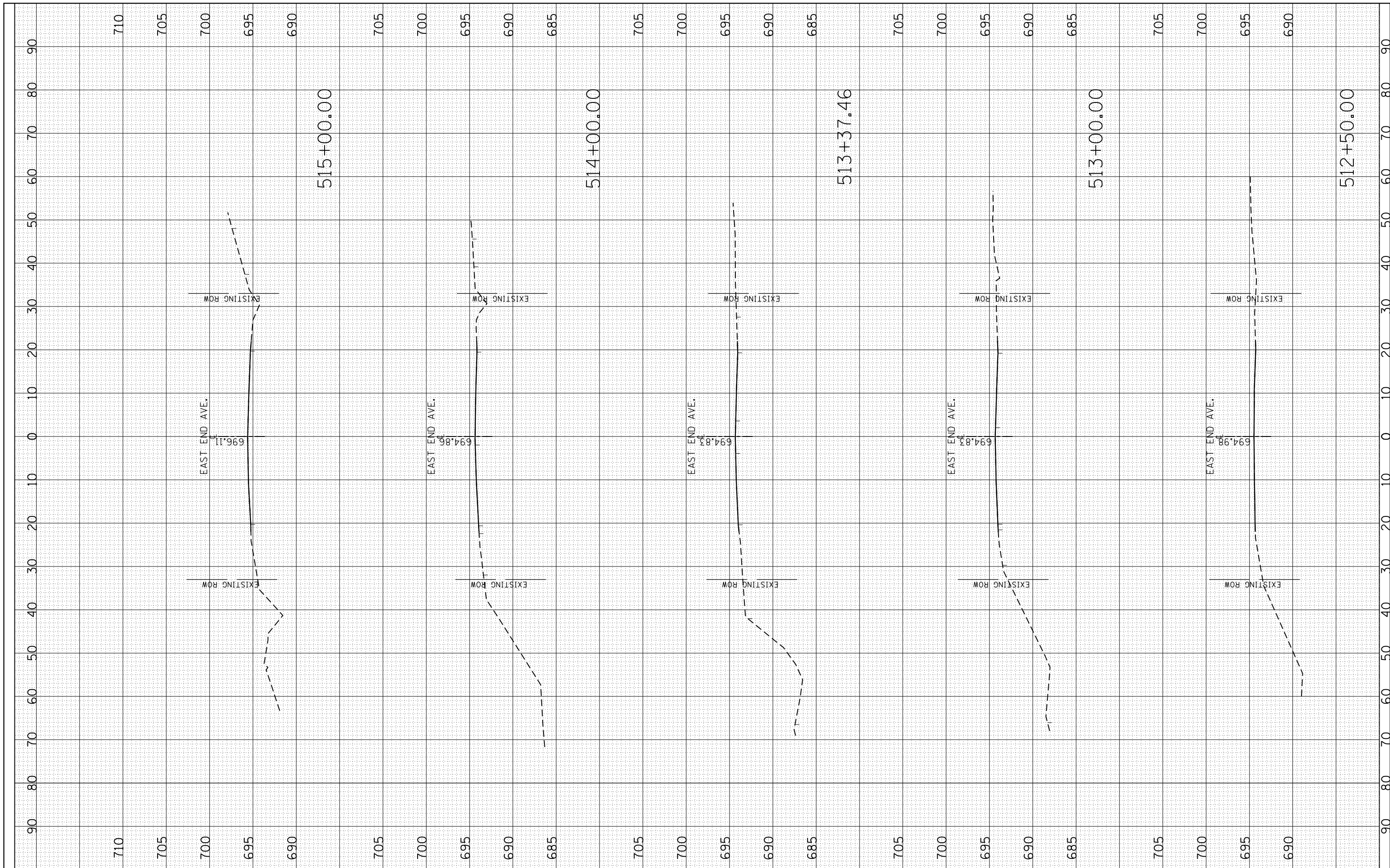
SCALE: SHEET OF SHEETS STA. TO STA.

**EAST END AVE.
 26th ST. to 30th ST.**

| | | | | |
|--------------------|------------|--------|---------------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2887 | 3155-N(13) | COOK | 88 | 64 |
| CONTRACT NO. 60W80 | | | ILLINOIS FED. AID PROJECT | |

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| FINAL SURVEY | SURVEYED | BY | DATE |
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| ORIGINAL SURVEY | SURVEYED | BY | DATE |
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

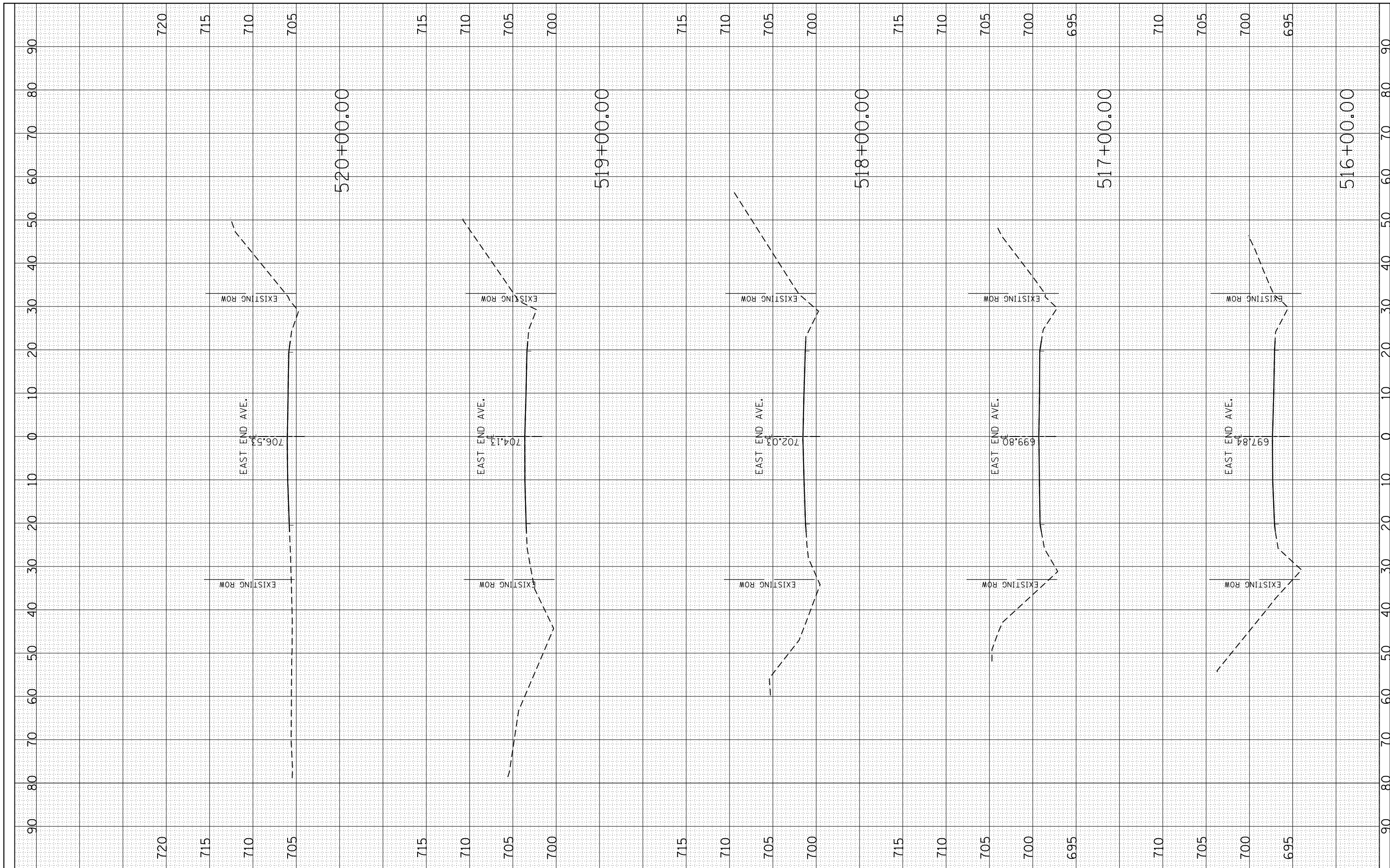
**EAST END AVE.
 26th ST. to 30th ST.**

SCALE: SHEET OF SHEETS STA. TO STA.

| | | | | |
|---------------------------|------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2887 | 3155-N(13) | COOK | 88 | 65 |
| CONTRACT NO. 60W80 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

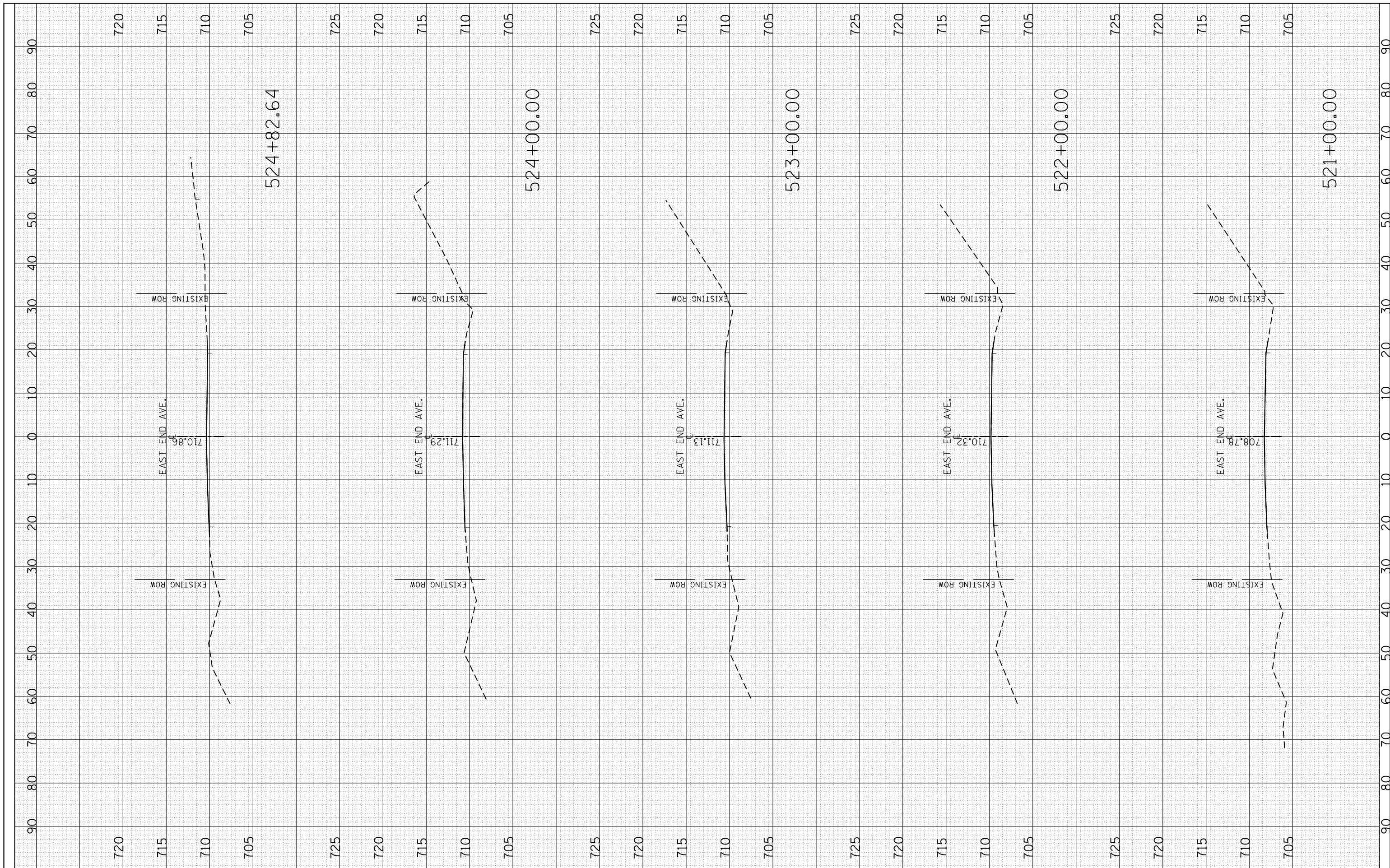
**EAST END AVE.
 26th ST. to 30th ST.**

SCALE: SHEET OF SHEETS STA. TO STA.

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|---------------------------|-----------------------|----------------|--------------------|-----------------|
| F.A.U. RTE. 2887 | SECTION 3155-N(13) | COUNTY COOK | TOTAL SHEETS 88 | SHEET NO. 66 |
| CONTRACT NO. 60W80 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

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| FINAL SURVEY | SURVEYED | BY | DATE |
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

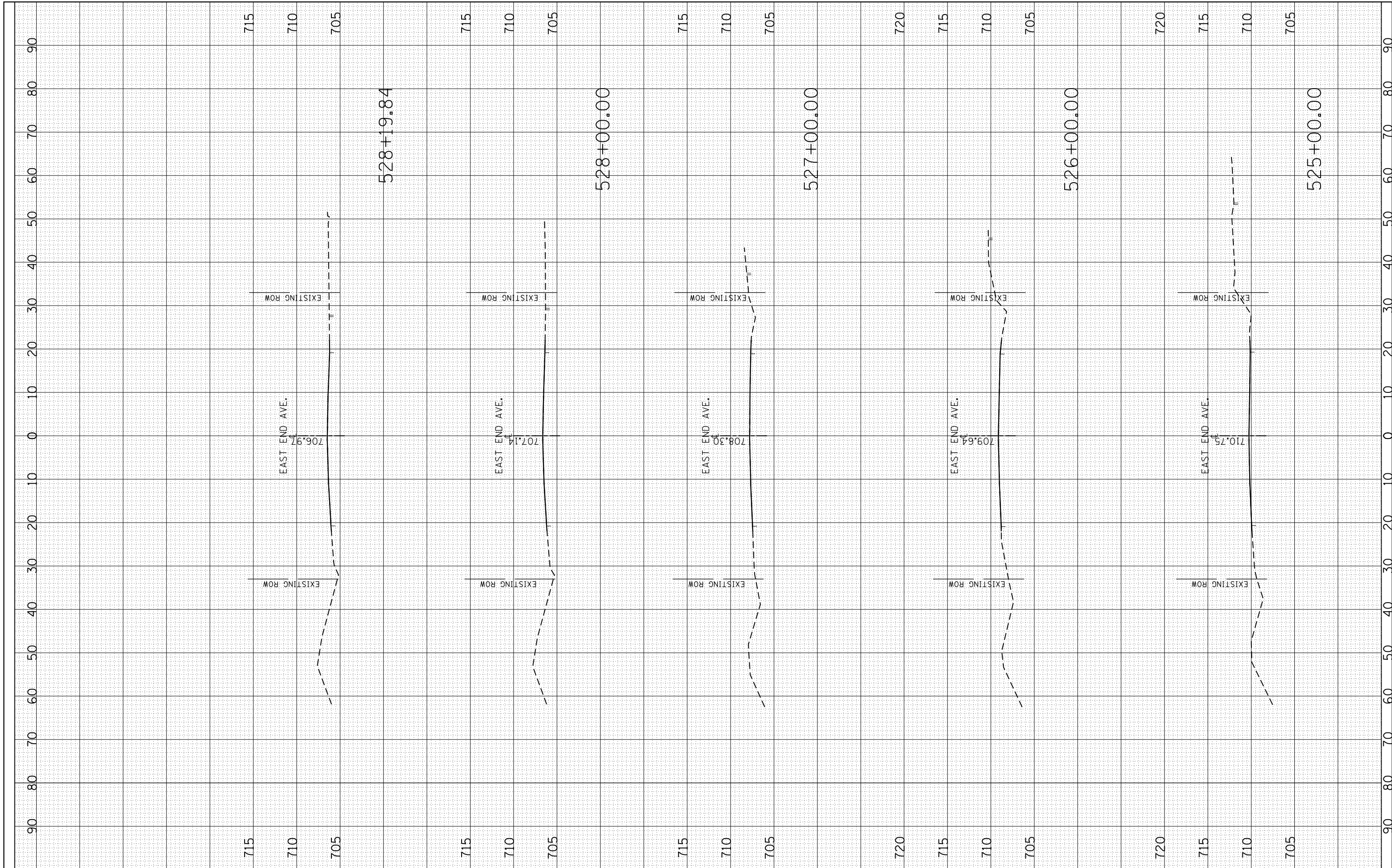
**EAST END AVE.
 26th ST. to 30th ST.**

SCALE: SHEET OF SHEETS STA. TO STA.

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|---------------------------|-----------------------|----------------|--------------------|-----------------|
| F.A.U. RTE. 2887 | SECTION 3155-N(13) | COUNTY COOK | TOTAL SHEETS 88 | SHEET NO. 67 |
| CONTRACT NO. 60W80 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

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| FINAL SURVEY | SURVEYED | BY | DATE |
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**EAST END AVE.
 26th ST. to 30th ST.**

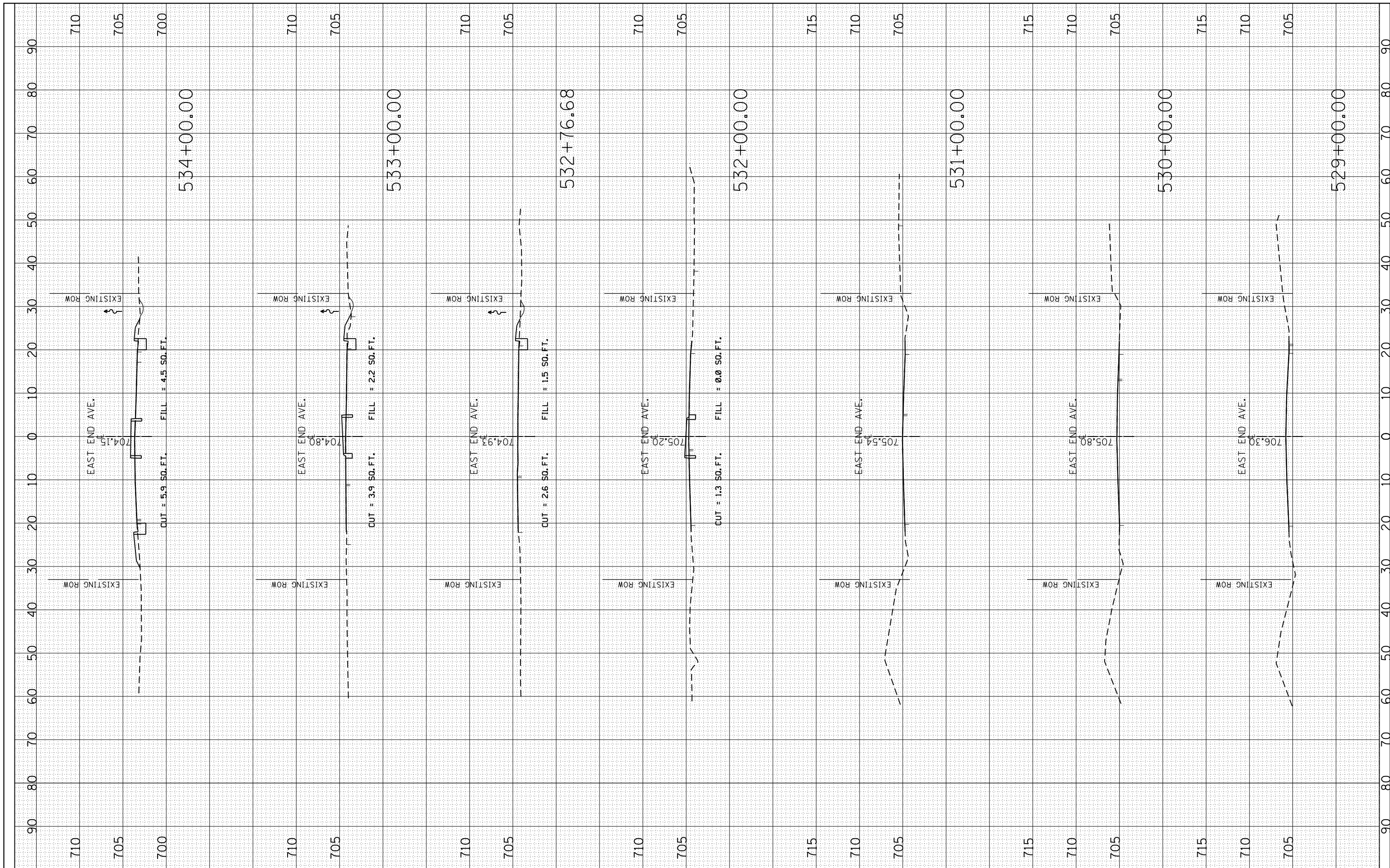
SCALE: SHEET OF SHEETS STA. TO STA.

| | | | | |
|--------------------|------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2887 | 3155-N(13) | COOK | 88 | 68 |
| CONTRACT NO. 60W80 | | | | |

ILLINOIS FED. AID PROJECT

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

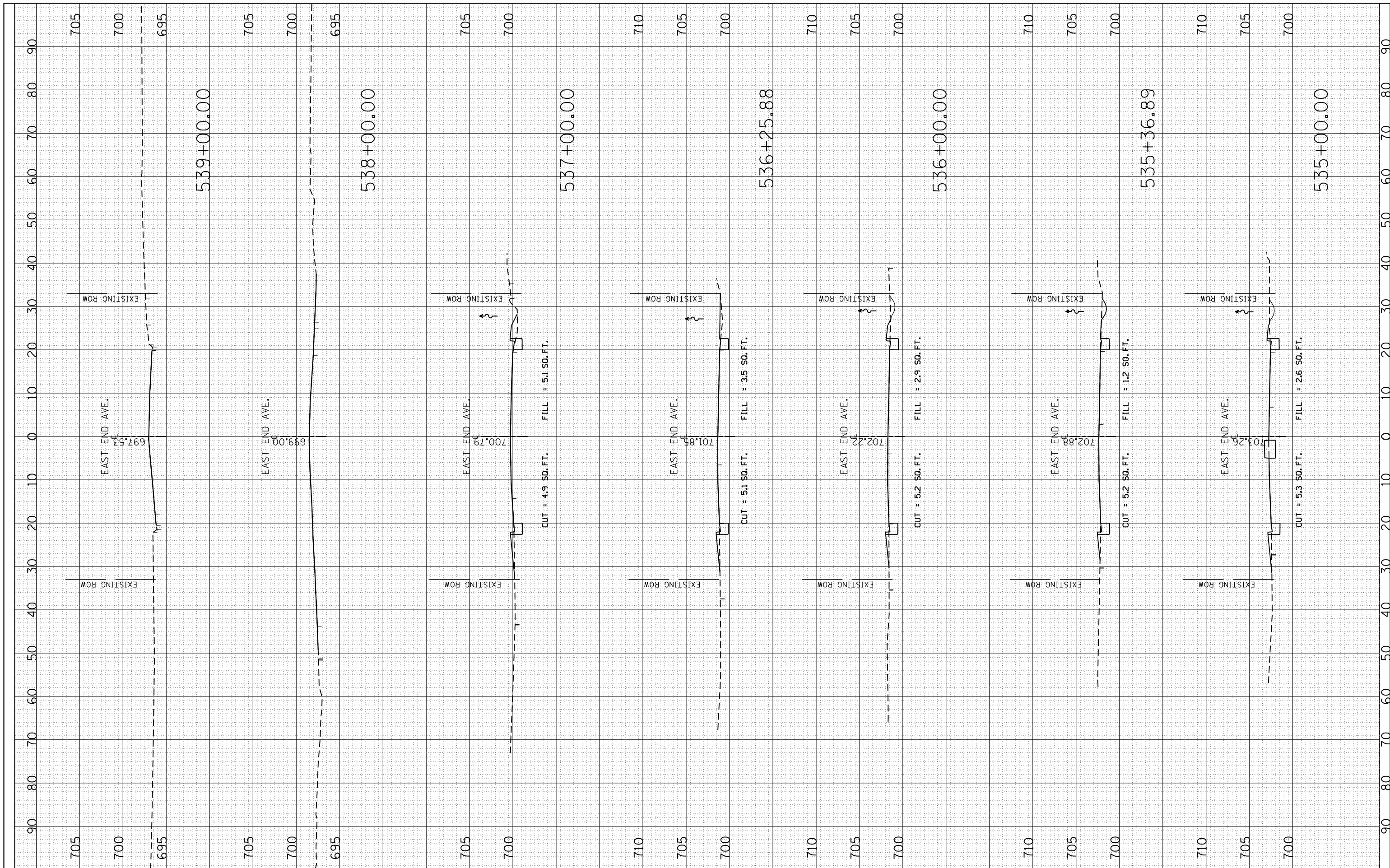
SCALE: SHEET OF SHEETS STA. TO STA.

**EAST END AVE.
 26th ST. to 30th ST.**

| | | | | |
|---------------------------|------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2887 | 3155-N(13) | COOK | 88 | 69 |
| CONTRACT NO. 60W80 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

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| FINAL SURVEY | SURVEYED | BY | DATE |
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

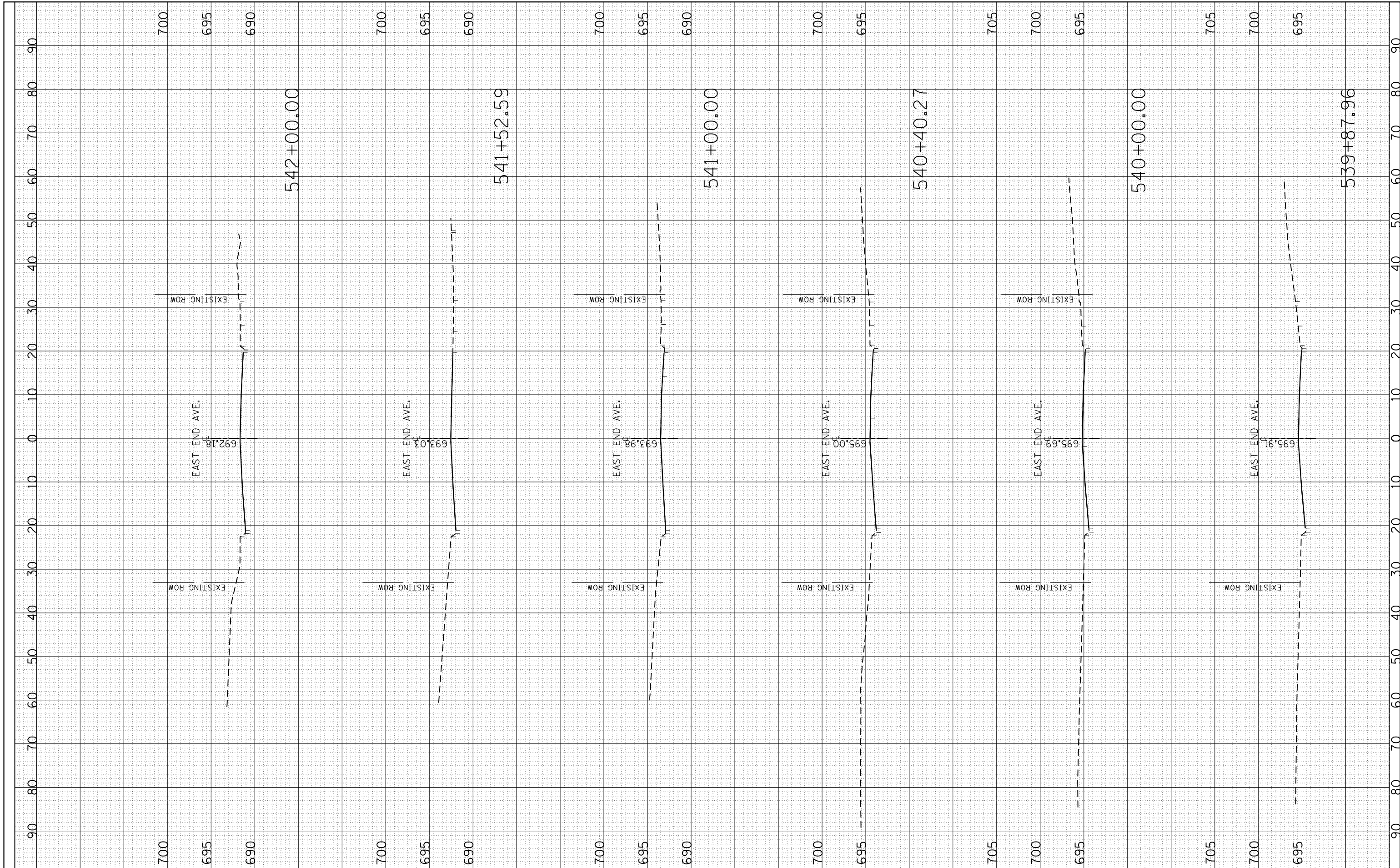
**EAST END AVE.
 26th ST. to 30th ST.**

SCALE: SHEET OF SHEETS STA. TO STA.

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|---------------------------|-----------------------|----------------|--------------------|-----------------|
| F.A.U. RTE. 2887 | SECTION 3155-N(13) | COUNTY COOK | TOTAL SHEETS 88 | SHEET NO. 70 |
| CONTRACT NO. 60W80 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

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|--------------|----------|----|------|
| FINAL SURVEY | SURVEYED | BY | DATE |
| NOTE BOOK | PLOTTED | | |
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| ORIGINAL SURVEY | SURVEYED | BY | DATE |
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

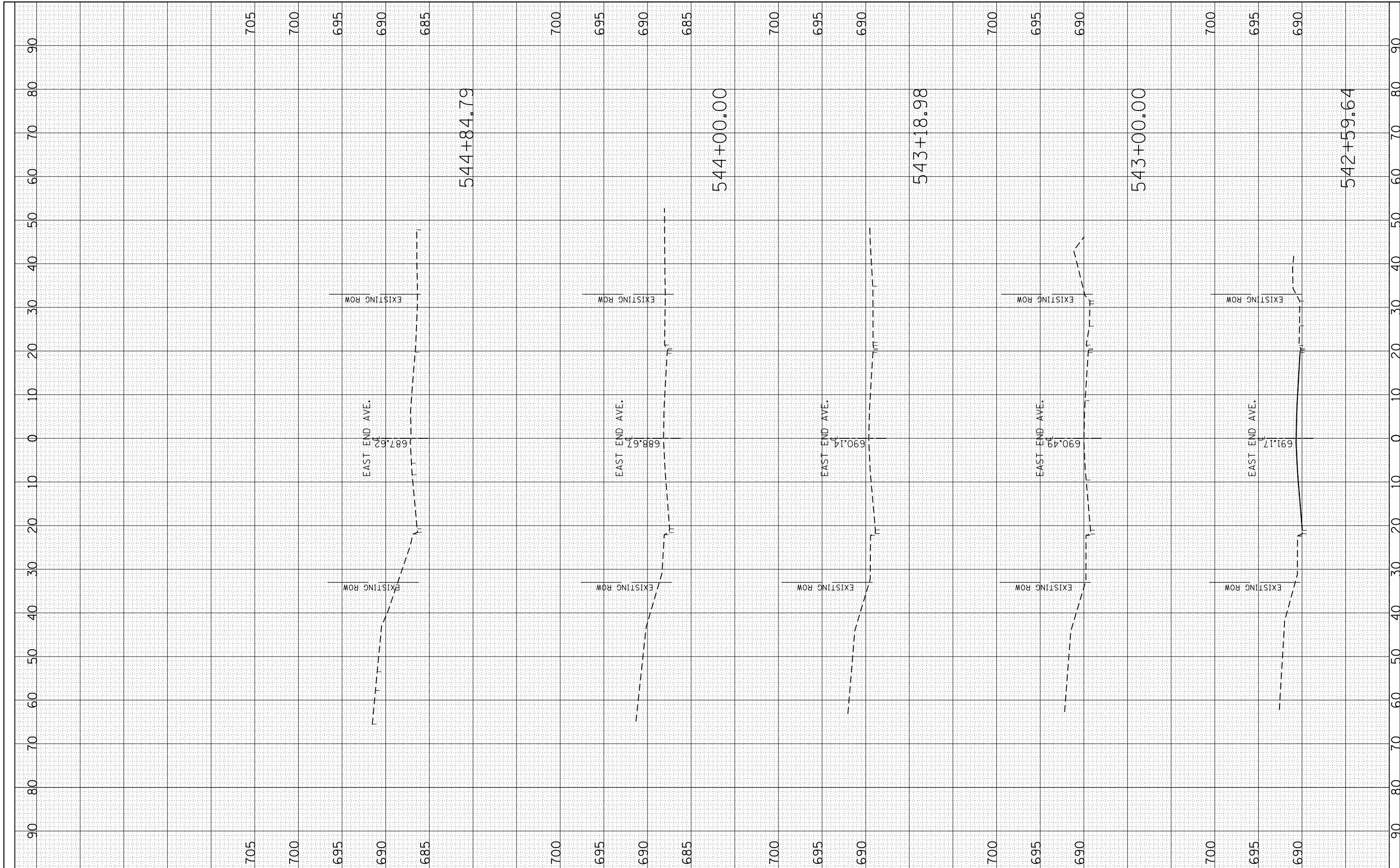
SCALE: SHEET OF SHEETS STA. TO STA.

**EAST END AVE.
 26th ST. to 30th ST.**

| | | | | |
|---------------------------|-----------------------|----------------|--------------------|-----------------|
| F.A.U. RTE. 2887 | SECTION 3155-N(13) | COUNTY COOK | TOTAL SHEETS 88 | SHEET NO. 71 |
| CONTRACT NO. 60W80 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

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|--------------|----------|----|------|
| FINAL SURVEY | SURVEYED | BY | DATE |
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

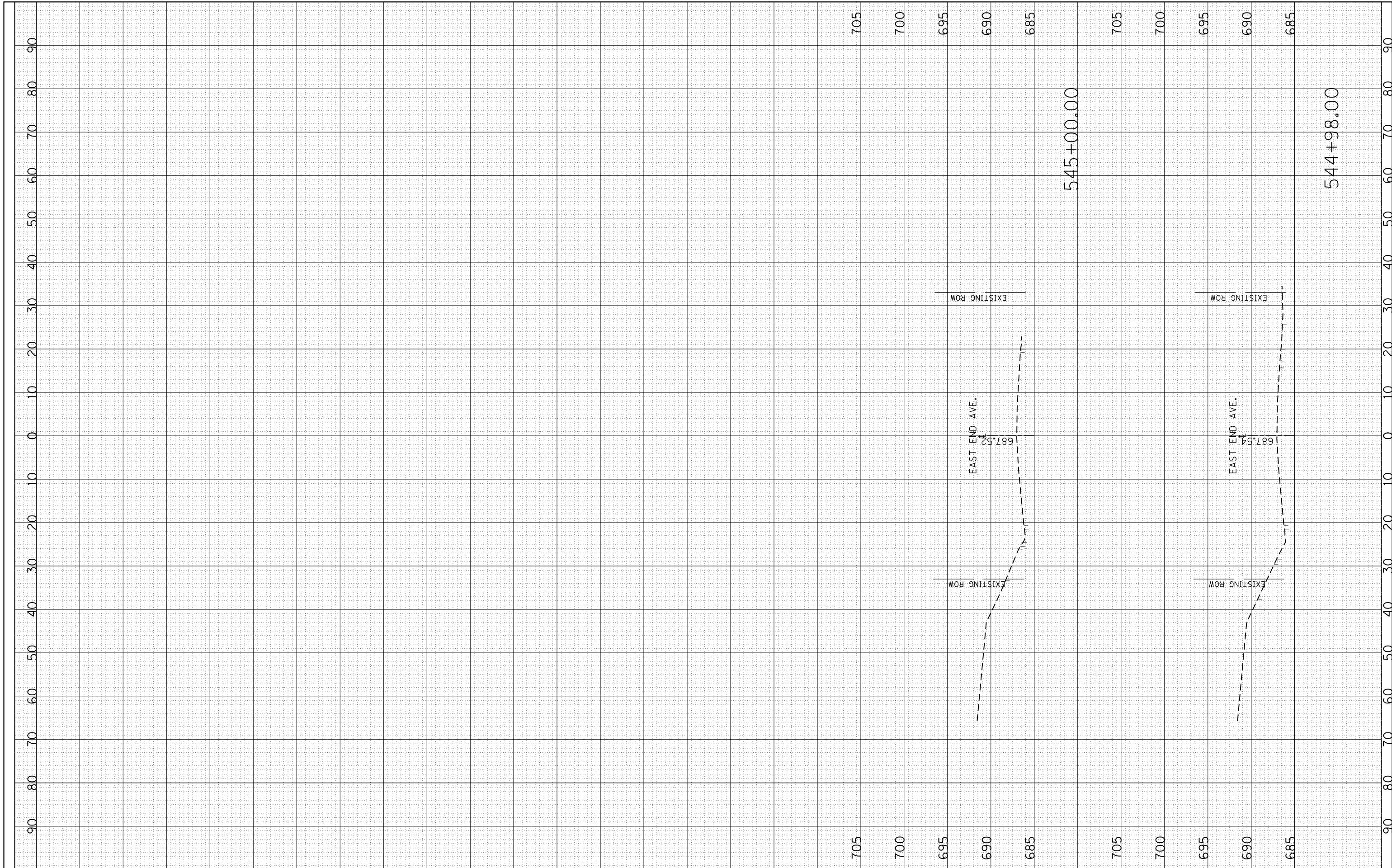
SCALE: SHEET OF SHEETS STA. TO STA.

**EAST END AVE.
 26th ST. to 30th ST.**

| | | | | |
|---------------------------|-----------------------|----------------|--------------------|-----------------|
| F.A.U. RTE. 2887 | SECTION 3155-N(13) | COUNTY COOK | TOTAL SHEETS 88 | SHEET NO. 72 |
| CONTRACT NO. 60W80 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

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|--------------|----------|----|------|
| FINAL SURVEY | SURVEYED | BY | DATE |
| NOTE BOOK | PLOTTED | | |
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| ORIGINAL SURVEY | SURVEYED | BY | DATE |
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

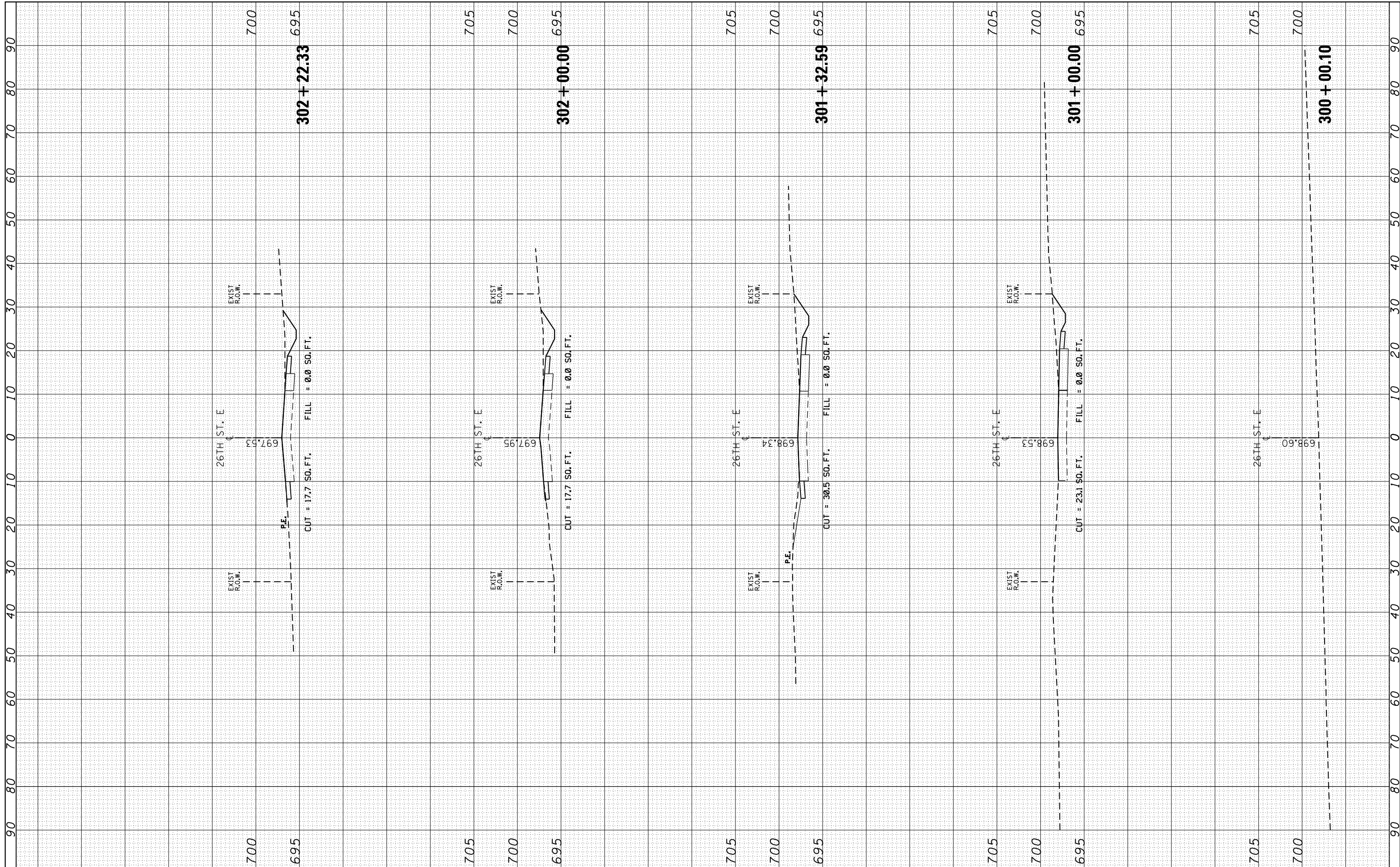
SCALE: SHEET OF SHEETS STA. TO STA.

**EAST END AVE.
 26th ST. to 30th ST.**

| | | | | |
|---------------------------|-----------------------|----------------|--------------------|-----------------|
| F.A.U. RTE. 2887 | SECTION 3155-N(13) | COUNTY COOK | TOTAL SHEETS 88 | SHEET NO. 73 |
| CONTRACT NO. 60W80 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

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|--------------|----------|----|------|
| FINAL SURVEY | SURVEYED | BY | DATE |
| NOTE BOOK | PLOTTED | | |
| NO. | TEMPLATE | | |
| | AREAS | | |
| | CHECKED | | |

| | | | |
|-----------------|----------|----|------|
| ORIGINAL SURVEY | SURVEYED | BY | DATE |
| NOTE BOOK | PLOTTED | | |
| NO. | TEMPLATE | | |
| | AREAS | | |
| | CHECKED | | |



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 PLOT SCALE = 20.000000 / in.
 PLOT DATE = 10/21/2014

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

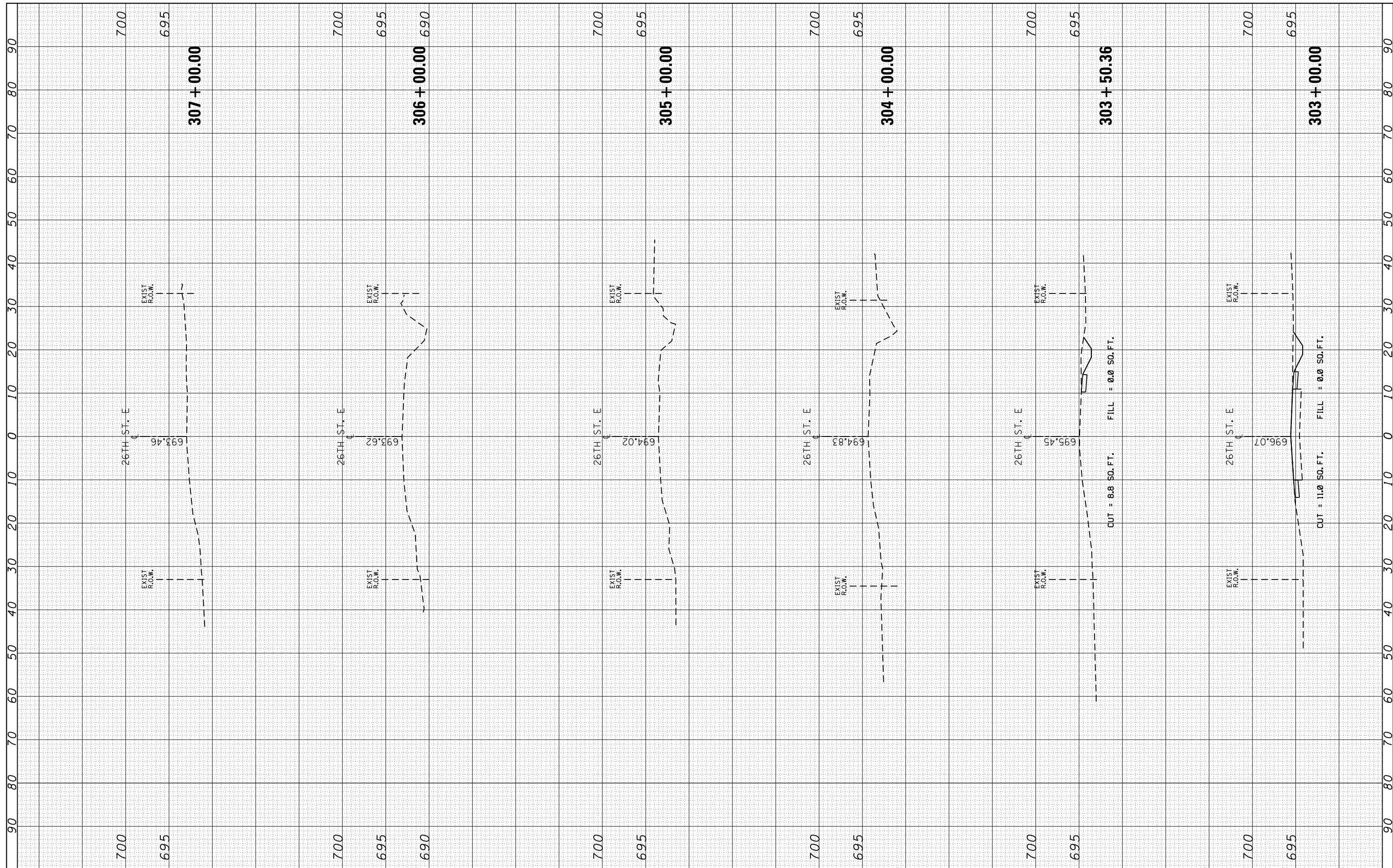
**CROSS SECTIONS
 26th STREET EAST**

SCALE: SHEET OF SHEETS STA. 300+00.10 TO STA. 302+22.33

| | | | | |
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| F.A.U. RTE. 2887 | SECTION 3155-N(13) | COUNTY COOK | TOTAL SHEETS 88 | SHEET NO. 74 |
| CONTRACT NO. 60W80 | | | ILLINOIS FED. AID PROJECT | |

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|------------------|--------------------------------|----|------|
| FINAL SURVEY NO. | SURVEYED PLOTTED AREAS CHECKED | BY | DATE |
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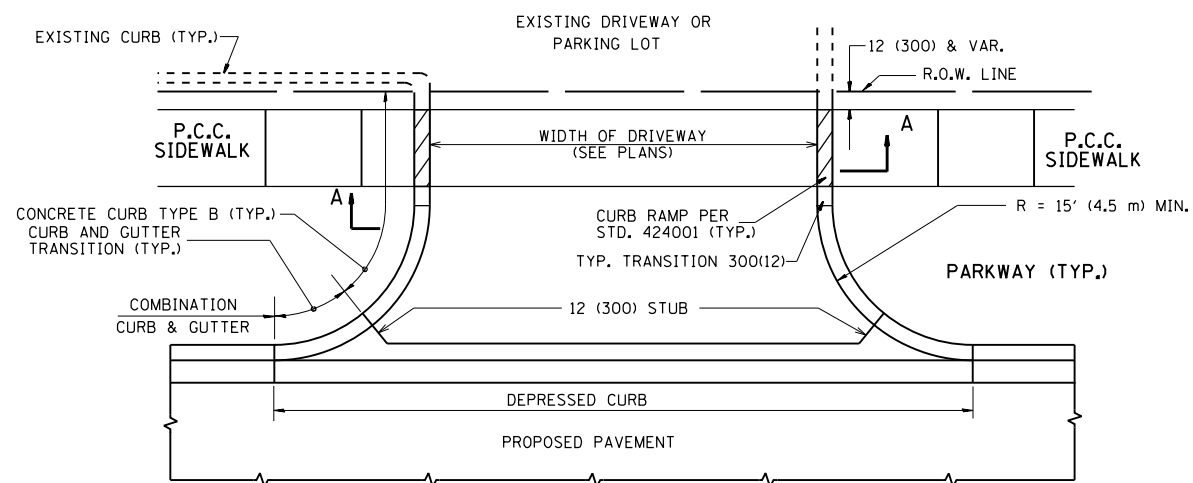


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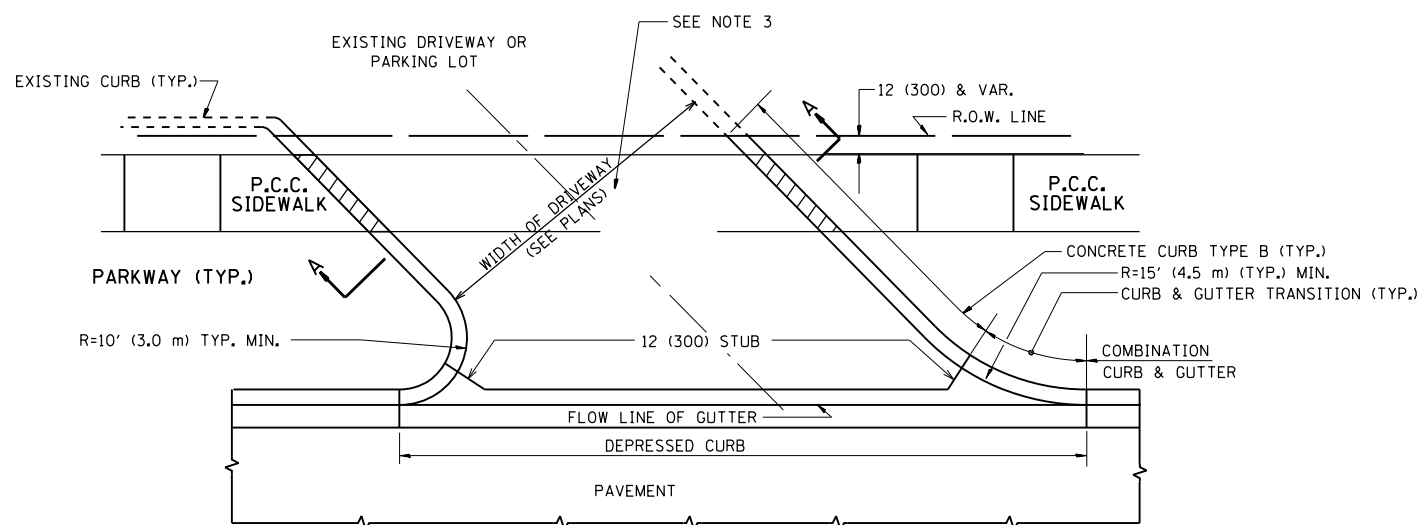
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| | |
|-------------------------|--------------------------------------------------|
| CROSS SECTIONS | |
| 26th STREET EAST | |
| SCALE: | SHEET OF SHEETS STA. 303+00.00 TO STA. 307+00.00 |

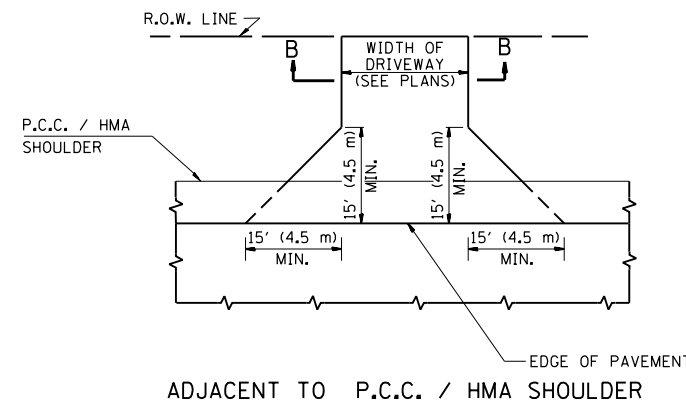
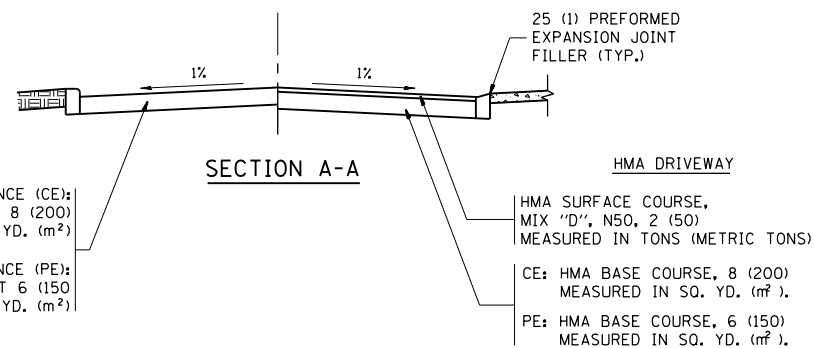
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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2887 | 3155-N(13) | COOK | 88 | 75 |
| CONTRACT NO. 60W80 | | | ILLINOIS FED. AID PROJECT | |



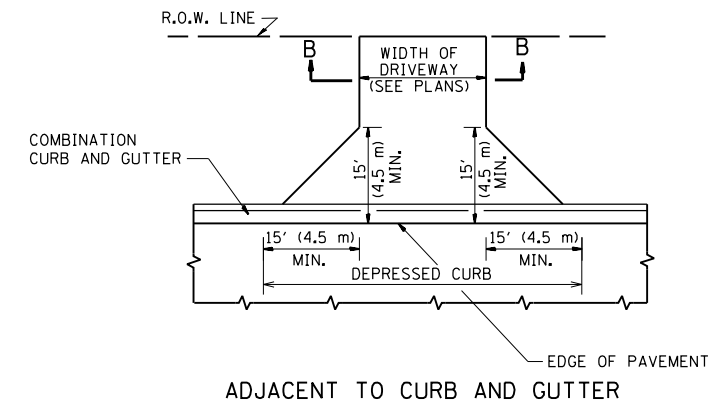
WITH CONCRETE CURB, TYPE B



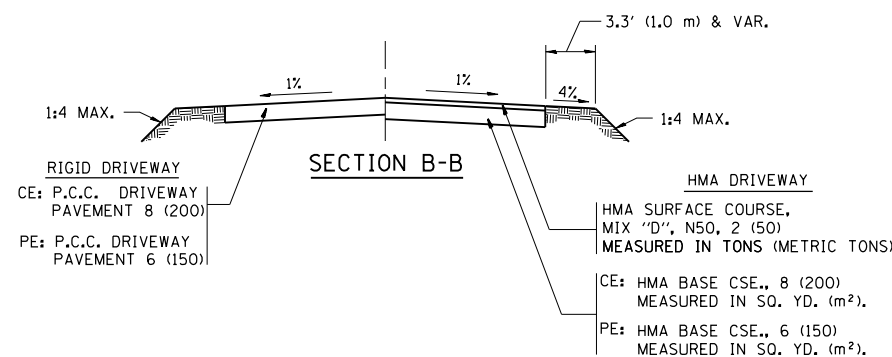
WITH CONCRETE CURB, TYPE B



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



RURAL FIELD ENTRANCE (FE)

HMA SURFACE COURSE, MIX "D", N50, 2 (50) MEASURED IN TONS (METRIC TONS)

AGGREGATE BASE CSE., TYPE B, 8 (200) MEASURED IN SQ. YD. (m²).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

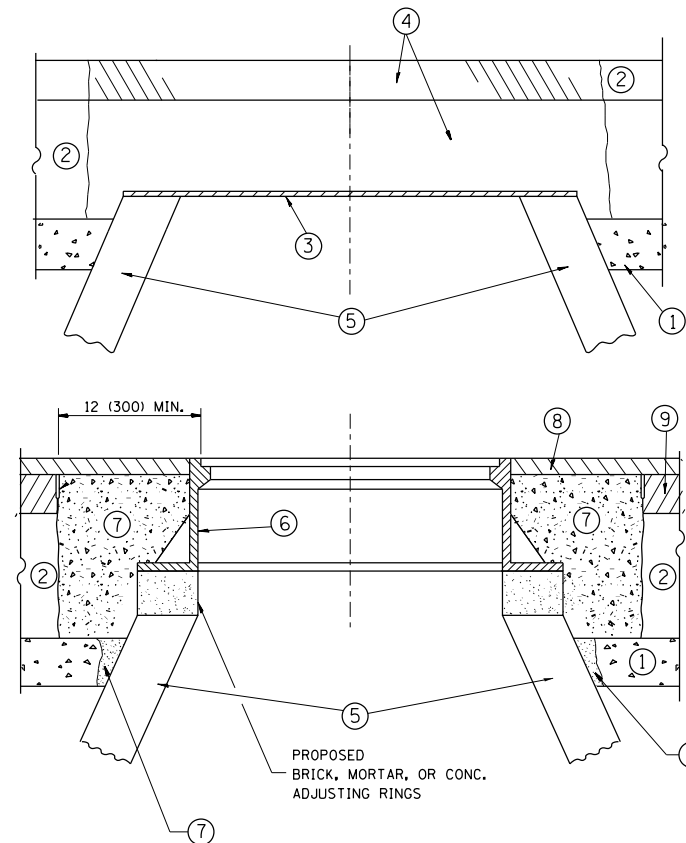
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| | PLOT SCALE = 100.0002' / in. | CHECKED - | REVISED - R. BORO 06-11-08 |
| | PLOT DATE = 10/16/2014 | DATE - 11-04-95 | REVISED - R. BORO 09-06-11 |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W.
AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| | | | | |
|-------------------------------------------------|------------|--------|--------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2887 | 3155-N(13) | COOK | 88 | 76 |
| BD0156-07 (BD-01) | | | CONTRACT NO. 60W80 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

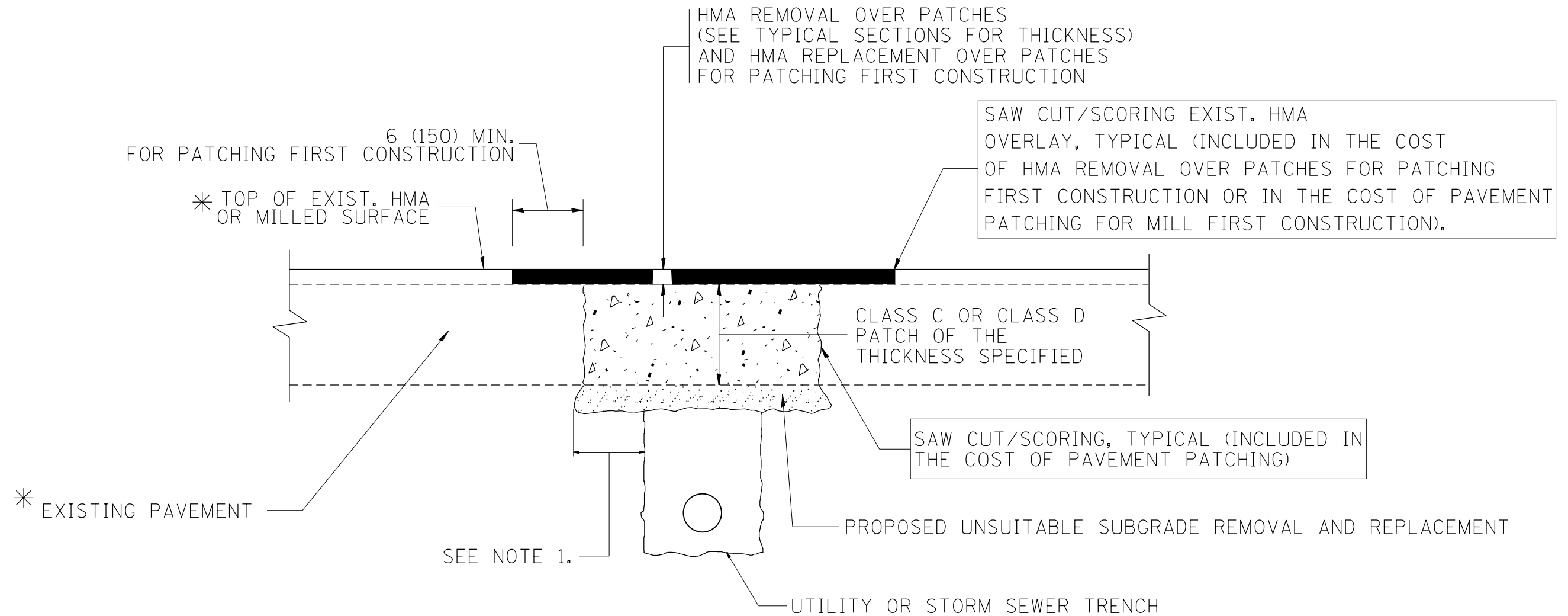
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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| | PLOT SCALE = 100.0000' / in. | CHECKED - | REVISED - R. BORO 03-09-11 |
| | PLOT DATE = 10/16/2014 | DATE - 10-25-94 | REVISED - R. BORO 12-06-11 |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| | | | |
|----------------------------------------------------------------|-------------------------|------|---------|
| DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING | | | |
| SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. |

| | | | | |
|-------------------------------------------------|------------|---------------------------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2887 | 3155-N(13) | COOK | 88 | 77 |
| BD600-03 (BD-8) | | CONTRACT NO. 60W80 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| | | | |
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| FILE NAME = | USER NAME = drivakosgn | DESIGNED - R. SHAH | REVISED - A. ABBAS 04-27-98 |
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| | PLOT SCALE = 100.0000' / 1in. | CHECKED - | REVISED - R. BORO 09-04-07 |
| | PLOT DATE = 10/16/2014 | DATE - 10-25-94 | REVISED - K. ENG 10-27-08 |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| | | | |
|--------------------------------------------------------|-------------------------|------|---------|
| PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT | | | |
| SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. |

| | | | | |
|-------------------------------------------------|------------|--------|---------------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2887 | 3155-N(13) | COOK | 88 | 78 |
| BD400-04 (BD-22) | | | CONTRACT NO. 60W80 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

1/4" (5) **

18" (450) MAX.

T/2 *

3" (75) MIN.

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

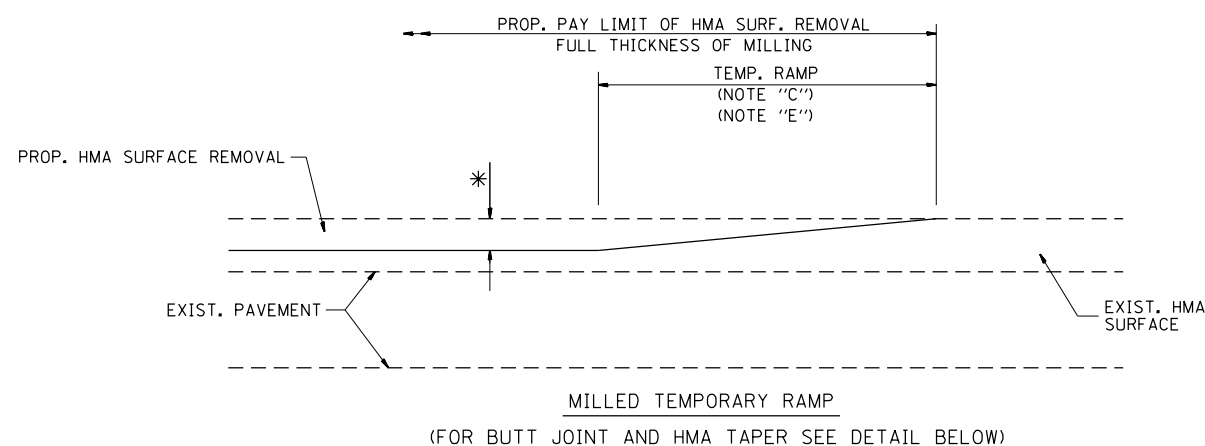
⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

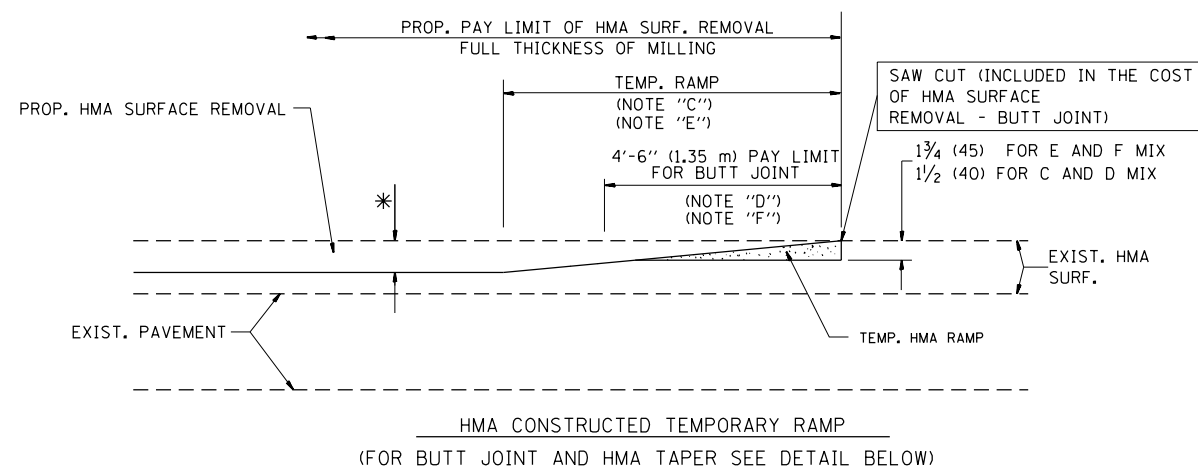
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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| PLOT SCALE = 100.0000' / in. | CHECKED - | REVISED - R. BORO 12-15-09 | | | | BD600-06 (BD-24) | | CONTRACT NO. 60W80 | | |
| PLOT DATE = 10/16/2014 | DATE - 03-11-94 | | | | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |

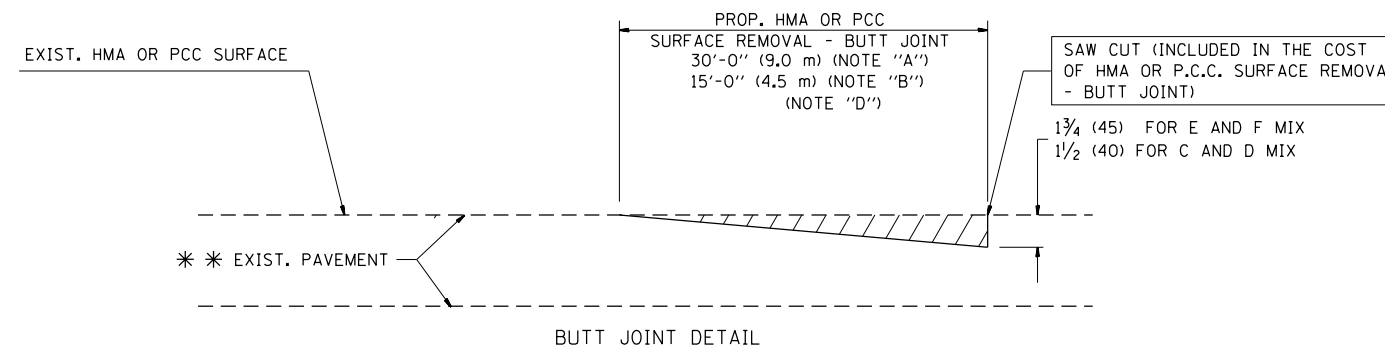


OPTION 1

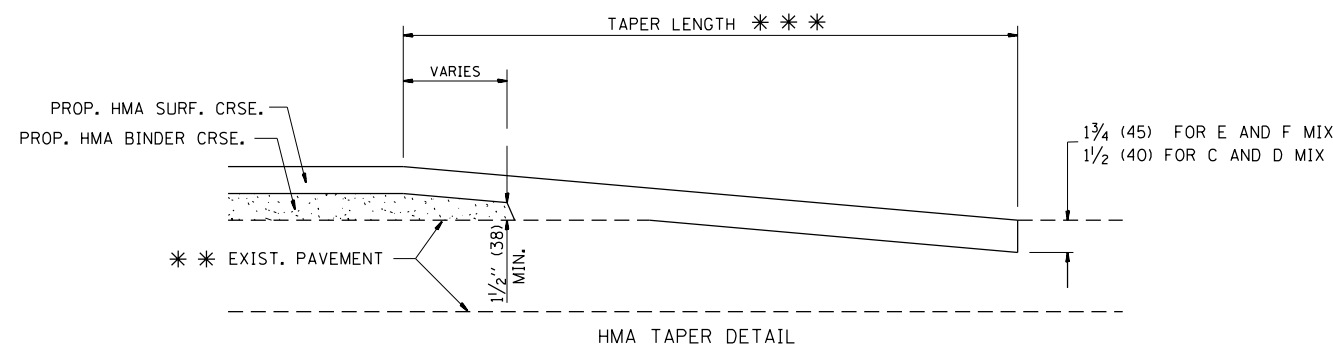


OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

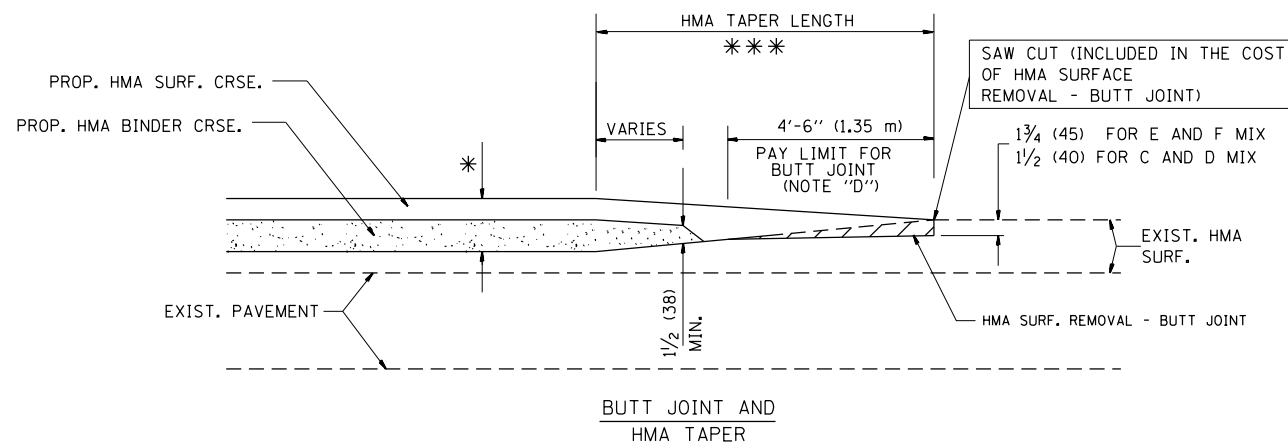
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

* * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

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| FILE NAME = | USER NAME = drivakosgn | DESIGNED - M. DE YONG | REVISED - R. SHAH 10-25-94 |
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| | PLOT SCALE = 100.0000' / in. | CHECKED - | REVISED - M. GOMEZ 04-06-01 |
| | PLOT DATE = 10/16/2014 | DATE - 06-13-90 | REVISED - R. BORO 01-01-07 |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| | | | | |
|-------------------------------------------------|------------|---------------------------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2887 | 3155-N(13) | COOK | 88 | 80 |
| BD400-05 BD32 | | CONTRACT NO. 60W80 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

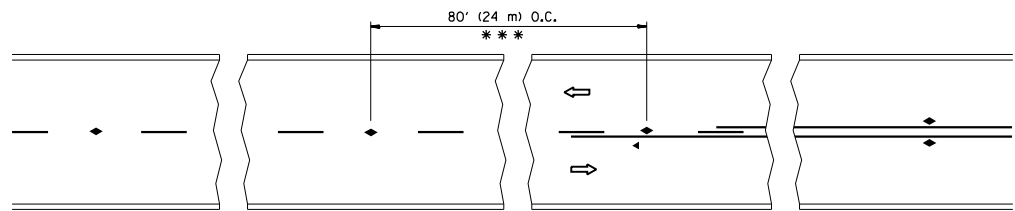
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| | PLOT SCALE = 100.0000' / in. | CHECKED - | REVISED - A. HOUSEH 10-15-96 |
| | PLOT DATE = 10/16/2014 | DATE - 06-89 | REVISED - T. RAMMACH 01-06-00 |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

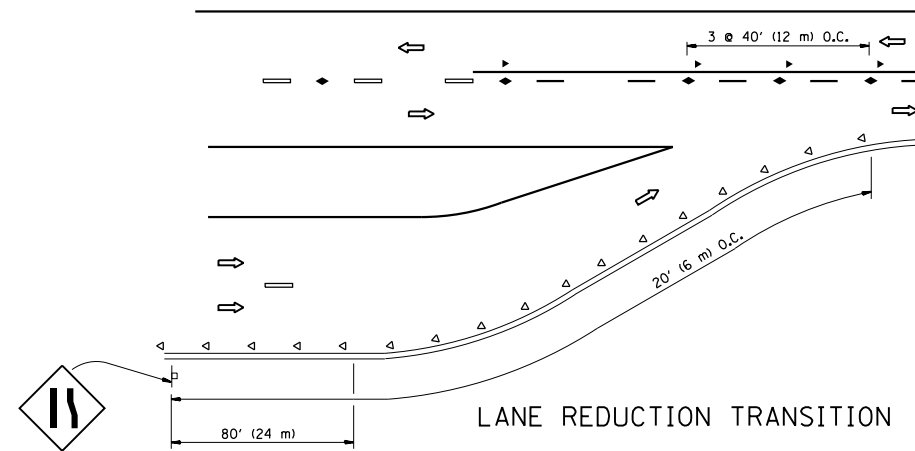
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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2887 | 3155-N(13) | COOK | 88 | 81 |
| TC-10 | | CONTRACT NO. 60W80 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

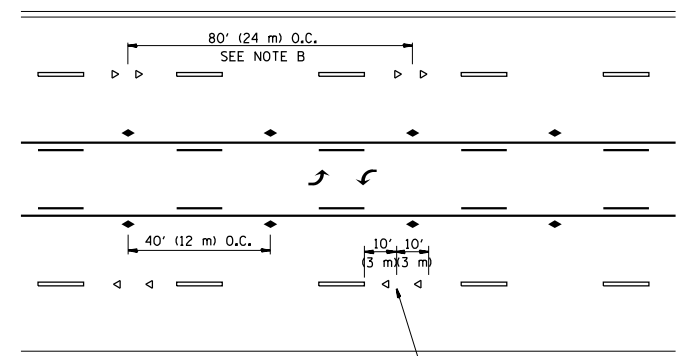


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

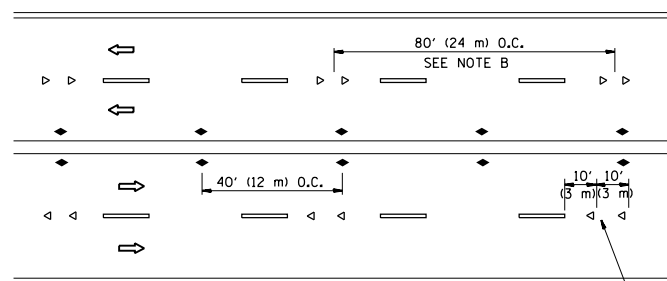
TWO-LANE/TWO-WAY



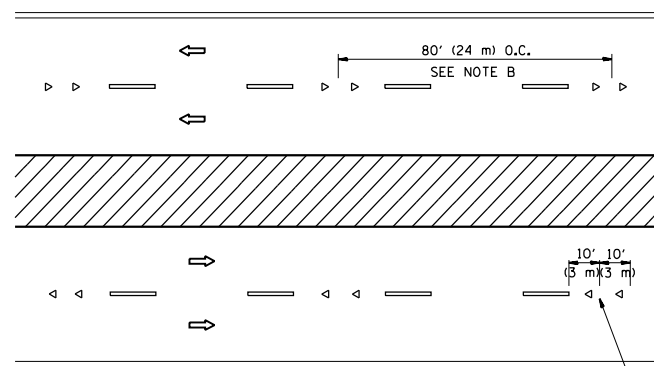
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

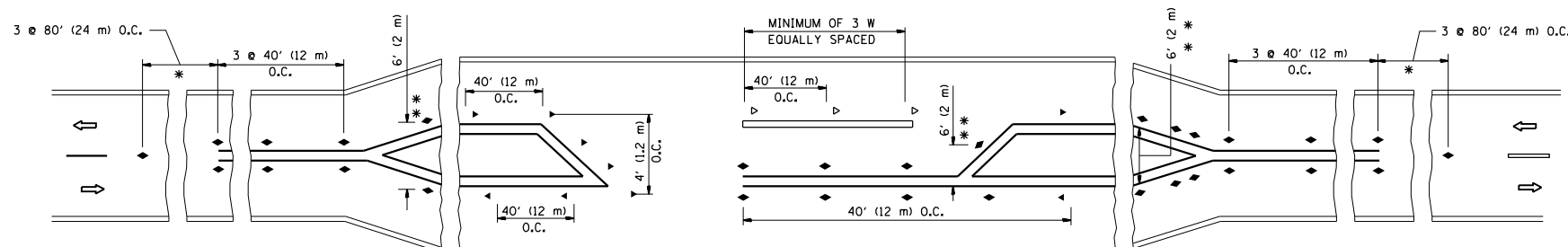
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

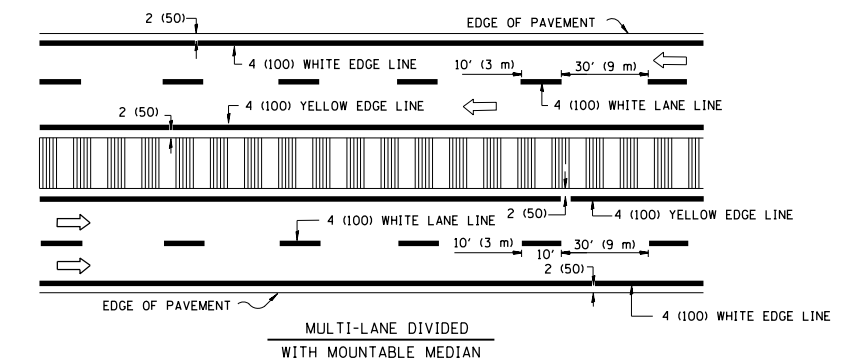
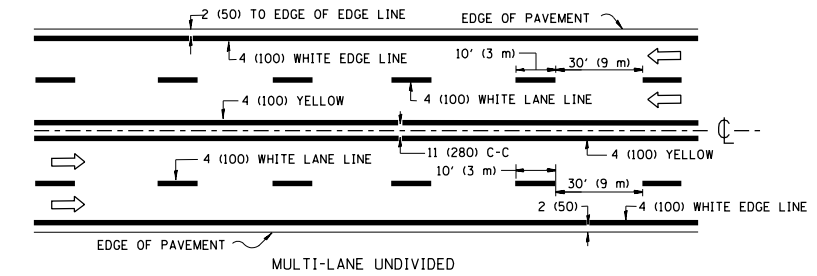
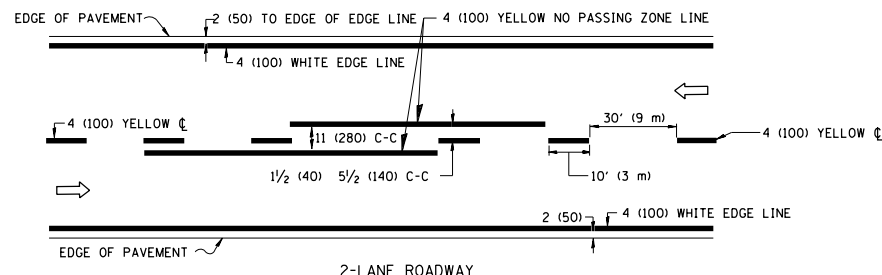
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| | PLOT SCALE = 100.0000' / 1" | CHECKED - | REVISED - T. RAMMACHER 01-06-00 |
| | PLOT DATE = 10/16/2014 | DATE - | REVISED - C. JUCIUS 09-09-09 |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

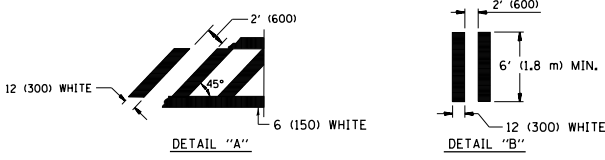
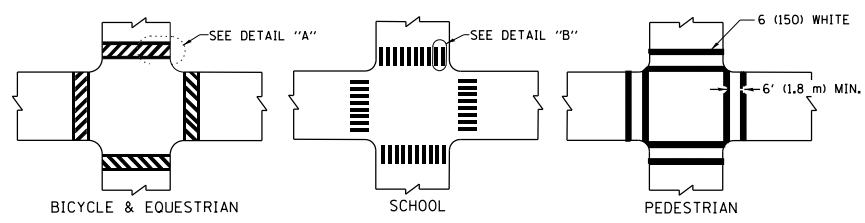
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|-------------------------------------------------|------------|--------------------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2887 | 3155-N(13) | COOK | 88 | 82 |
| TC-11 | | CONTRACT NO. 60W80 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

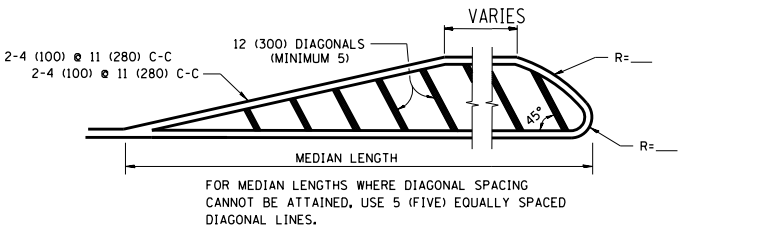
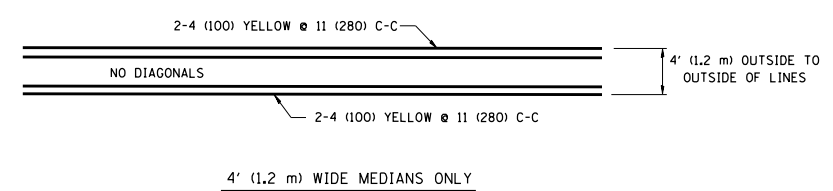


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

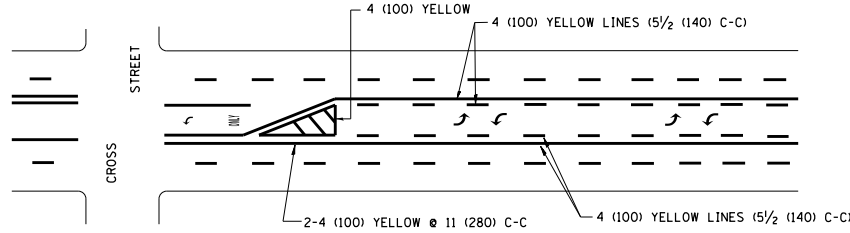


TYPICAL CROSSWALK MARKING

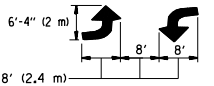


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

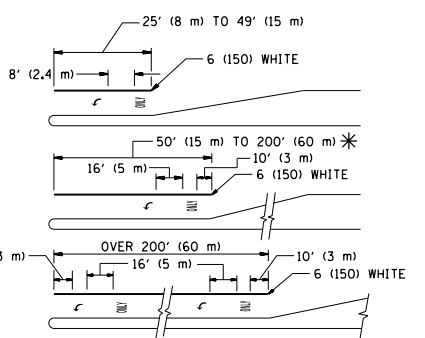


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

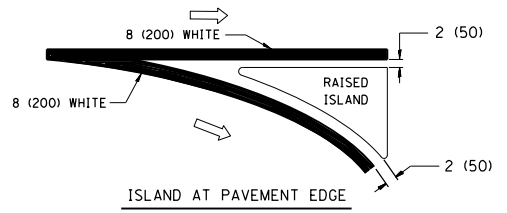
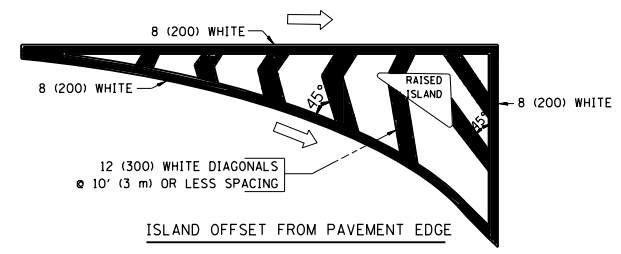


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

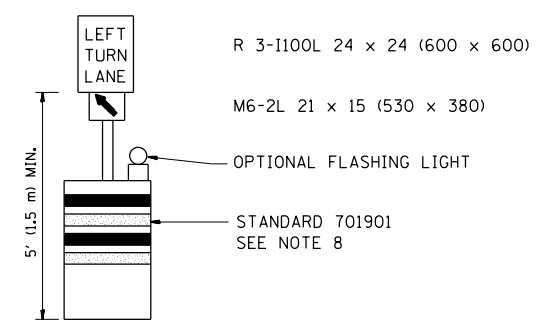
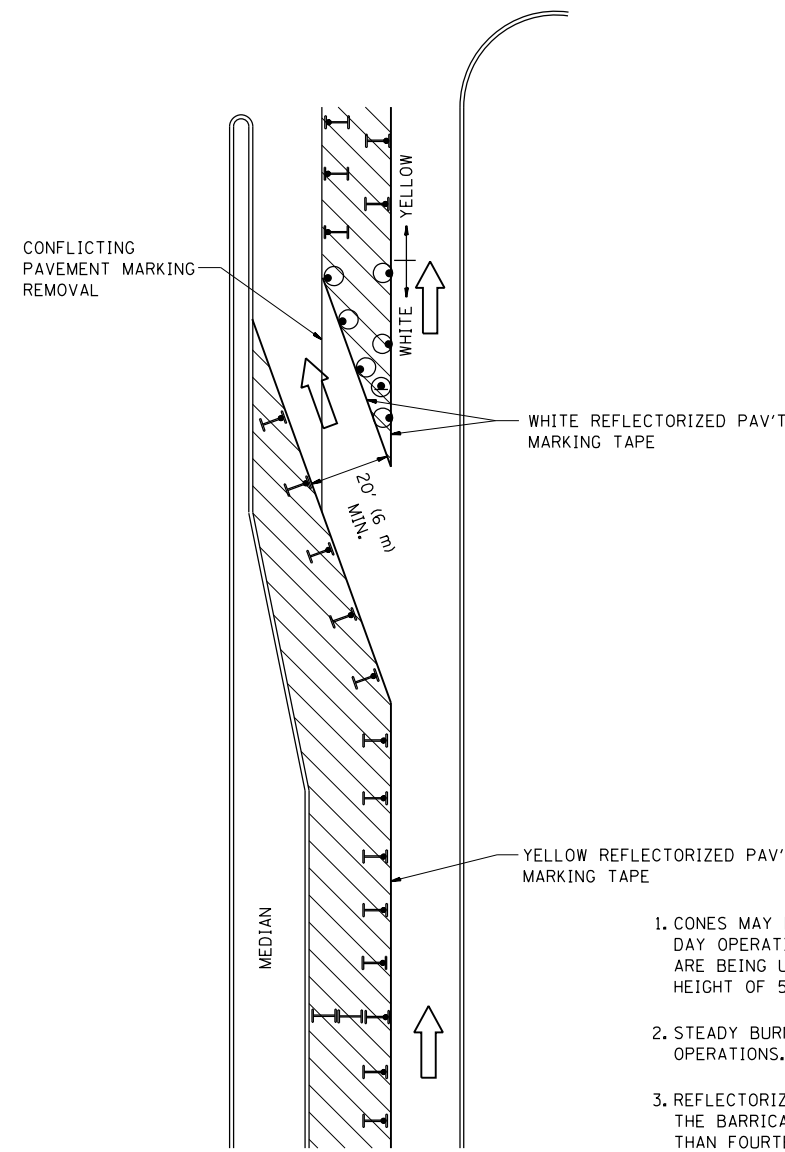


TYPICAL ISLAND MARKING

| TYPE OF MARKING | WIDTH OF LINE | PATTERN | COLOR | SPACING / REMARKS |
|---------------------------------------------------------------------------------------------------|--------------------------------------------------------------------------------------------|------------------------------|---------------------------------------------------------|--------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| CENTERLINE ON 2 LANE PAVEMENT | 4 (100) | SKIP-DASH | YELLOW | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT | 2 @ 4 (100) | SOLID | YELLOW | 11 (280) C-C |
| NO PASSING ZONE LINES: FOR ONE DIRECTION | 4 (100) | SOLID | YELLOW | 5/2 (140) C-C FROM SKIP-DASH CENTERLINE |
| NO PASSING ZONE LINES: FOR BOTH DIRECTIONS | 2 @ 4 (100) | SOLID | YELLOW | 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN |
| LANE LINES | 4 (100) 5 (125) ON FREEWAYS | SKIP-DASH SKIP-DASH | WHITE WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS) | SAME AS LINE BEING EXTENDED | SKIP-DASH | SAME AS LINE BEING EXTENDED | 2' (600) LINE WITH 6' (1.8 m) SPACE |
| EDGE LINES | 4 (100) | SOLID | YELLOW-LEFT WHITE-RIGHT | OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB |
| TURN LANE MARKINGS | 6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m)) | SOLID | WHITE | SEE TYPICAL TURN LANE MARKING DETAIL |
| TWO WAY LEFT TURN MARKING | 2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW | SKIP-DASH AND SOLID IN PAIRS | YELLOW WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE. SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL |
| CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL) | 2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90° | SOLID SOLID SOLID | WHITE WHITE WHITE | NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS. |
| STOP LINES | 24 (600) | SOLID | WHITE | PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE |
| PAINTED MEDIANS | 2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS | SOLID | YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC | 11 (280) C-C FOR THE DOUBLE LINE. SEE TYPICAL PAINTED MEDIAN MARKING. |
| GORE MARKING AND CHANNELIZING LINES | 8 (200) WITH 12 (300) DIAGONALS @ 45° | SOLID | WHITE | DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h)) |
| RAILROAD CROSSING | 24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X" | SOLID | WHITE | SEE STATE STANDARD 780001 AREA OF: "R": 3.6 SQ. FT. (0.33 m ²) EACH "X": 54.0 SQ. FT. (5.0 m ²) EACH |
| SHOULDER DIAGONALS | 12 (300) @ 45° | SOLID | WHITE - RIGHT YELLOW - LEFT | 50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h)) |

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.



GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

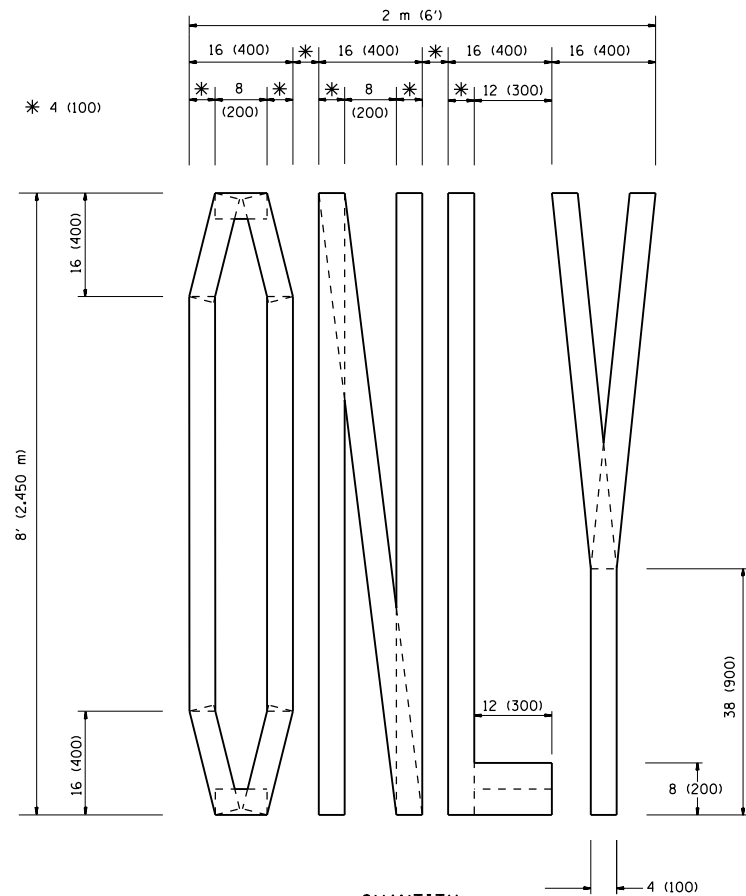
- WORK AREA
- LANE OPEN TO TRAFFIC
- TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

| | | | |
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| | PLOT DATE = 10/16/2014 | REVISED -T, RAMMACHER 01-06-00 | REVISED - |

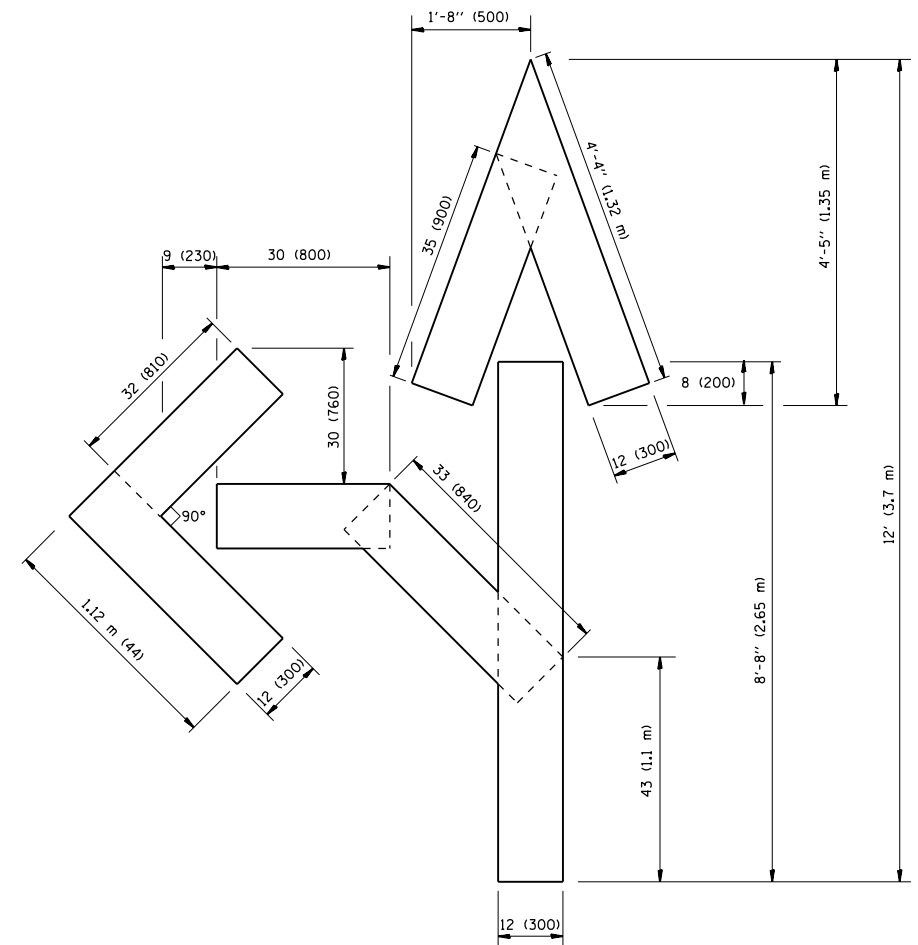
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) | | | |
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| SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. |

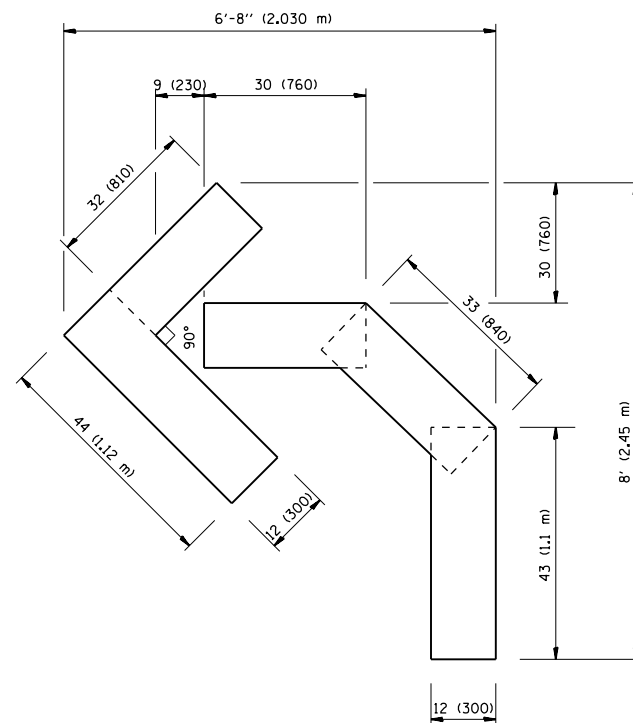
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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2887 | 3155-N(13) | COOK | 88 | 84 |
| TC-14 | | CONTRACT NO. 60W80 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

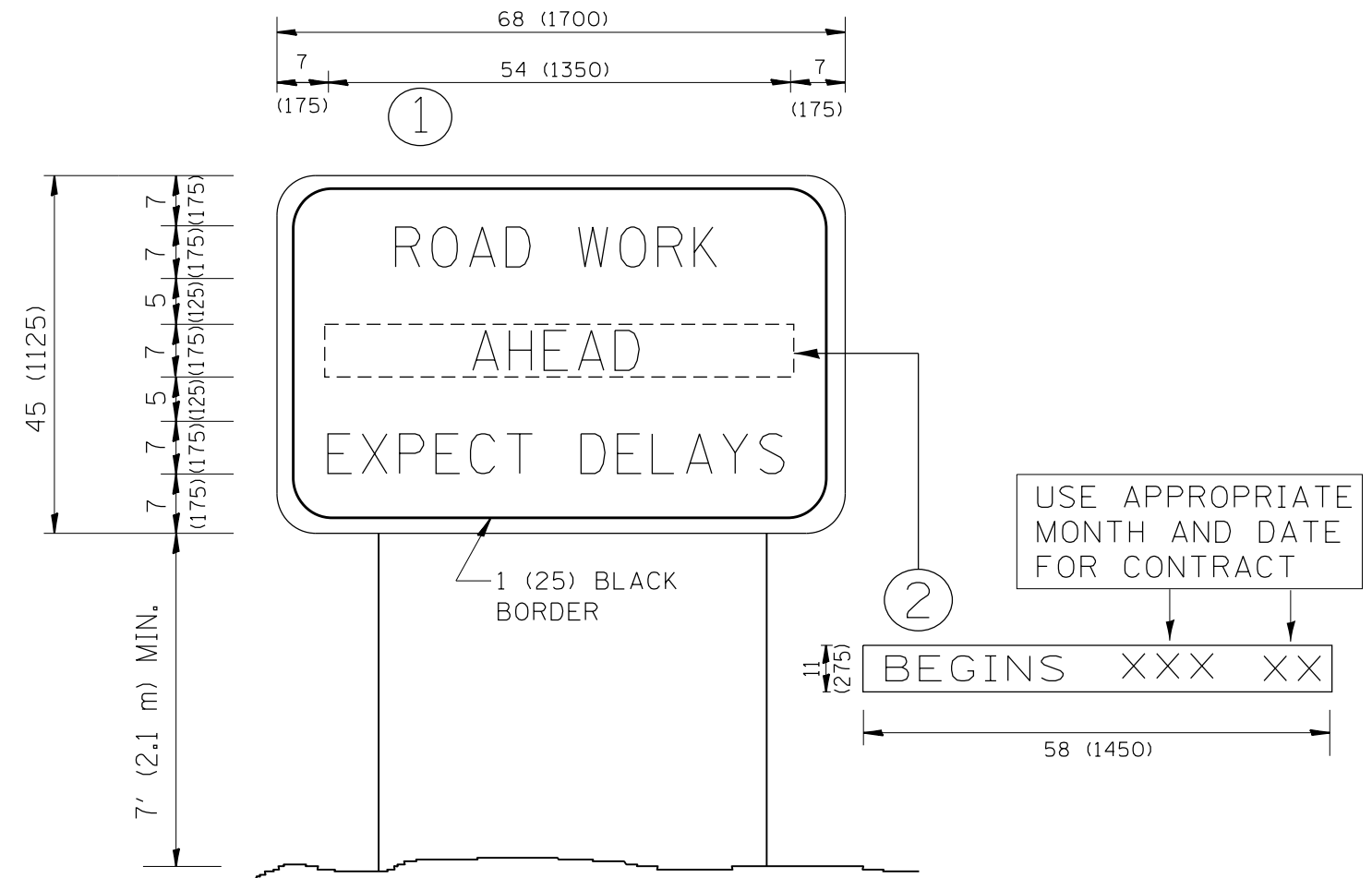
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| | PLOT DATE = 10/16/2014 | DATE - 09-18-94 | REVISED -E. GOMEZ 08-28-00 |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| | | | | |
|-------------------------------------------------|------------|--------|--------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2887 | 3155-N(13) | COOK | 88 | 85 |
| TC-16 | | | CONTRACT NO. 60W80 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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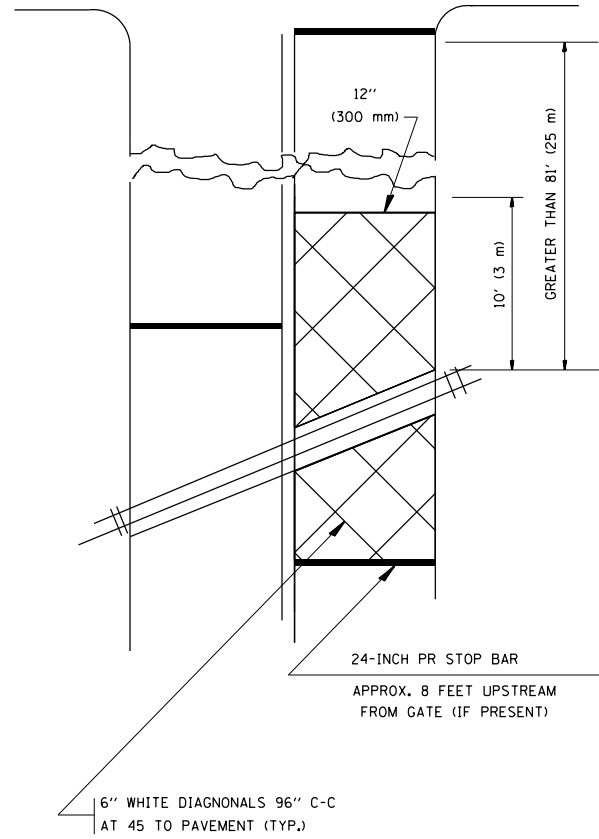
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

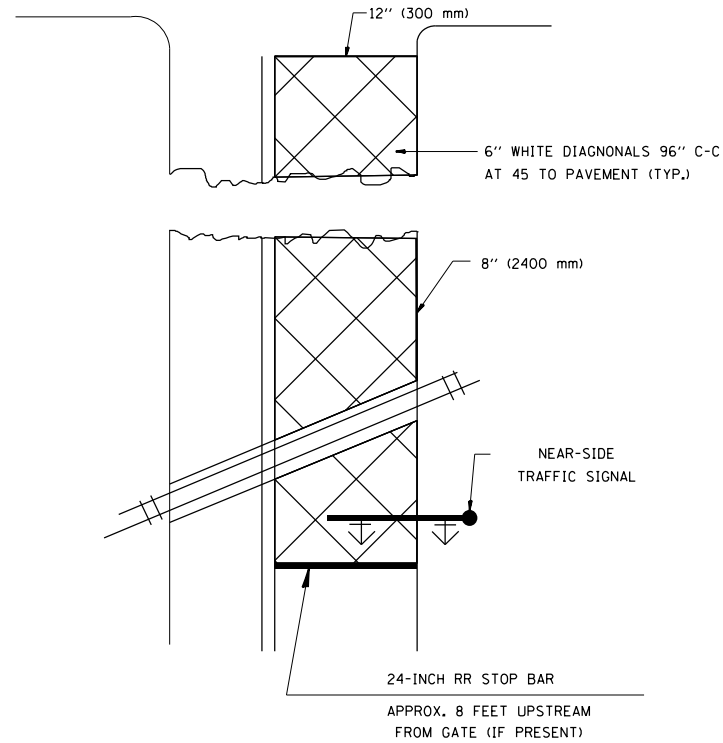
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| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

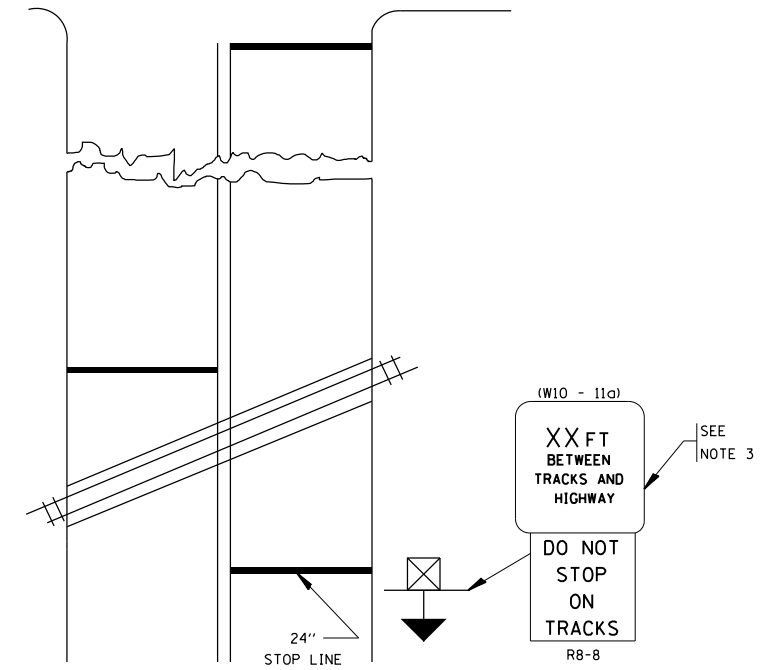
WITH INTERSECTION TRAFFIC SIGNALS
(SEE NOTE 1)



WITH NEAR-SIDE TRAFFIC SIGNALS
(SEE NOTE 1 & 2)



WITH NONSIGNALIZED INTERSECTION
81' (25 m) OR LESS TO CLOSEST RAIL



PLAN
N. T. S

PLAN
N. T. S

NOTES:

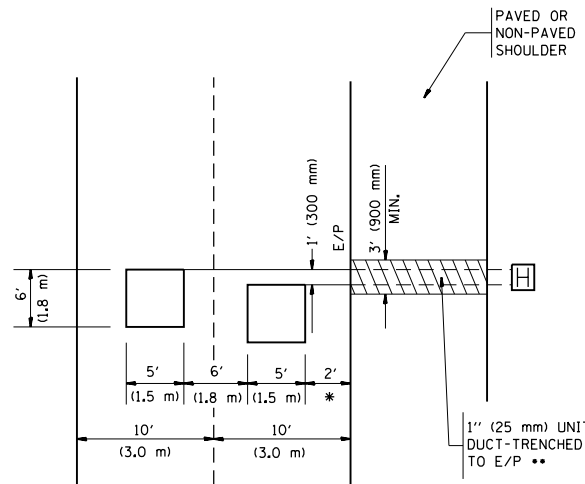
- PAVEMENT MARKINGS TO BE INSTALLED ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
- WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED, THE PAVEMENT MARKINGS EXTENDS TO THE INTERSECTION.
- DISTANCE TO BE SHOWN ON SIGN MEASURED FROM A POINT 6 FEET (1.8 m) FROM THE RAIL CLOSEST TO THE INTERSECTION TO THE STOP LINE OR CROSSWALK, WHICHEVER IS CLOSEST, ROUNDED DOWN TO THE NEAREST 5 FEET (1.5 m). WHERE THERE IS NO STOP LINE, MEASURE TO POINT WHERE THE DRIVER HAS A VIEW OF APPROACHING TRAFFIC.
THE CLEARANCE SIGN IS ALSO TO BE USED AS AN INTERIM MEASURE AT LOCATIONS WITH INTERCONNECTED INTERSECTION TRAFFIC SIGNALS WHERE IT IS PLANNED TO CHANGE THEM TO NEAR-SIDE SIGNALS AT A FUTURE TIME. IN THIS CASE, THE DISTANCE TO BE SHOWN ON THE SIGN IS MEASURED FROM THE EDGE OF THE STRIPED-OUT AREA INSTEAD OF 6-FEET FROM THE RAIL. THE SIGN IS TO BE REMOVED WHEN THE NEAR-SIDE SIGNALS ARE INSTALLED AND THE PAVEMENT MARKINGS EXTEND TO THE INTERSECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

| | | | | | | | | | | | |
|-------------|------------------------|------------|--------------------|-----------------------------------------------------------|-----------------------------------------------------------------------------------------------|-------------------------------------------------|------------|---------------------------|--------------|-----------|--|
| FILE NAME = | USER NAME = drivakosgn | DESIGNED - | REVISED - 02-25-11 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| | | DRAWN - | REVISED - 04-26-12 | | | 2887 | 3155-N(13) | COOK | 88 | 87 | |
| | | CHECKED - | REVISED - | | | TC-23 | | CONTRACT NO. 60W80 | | | |
| | | DATE - | REVISED - | | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | | |
| | | | | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. | | | |

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



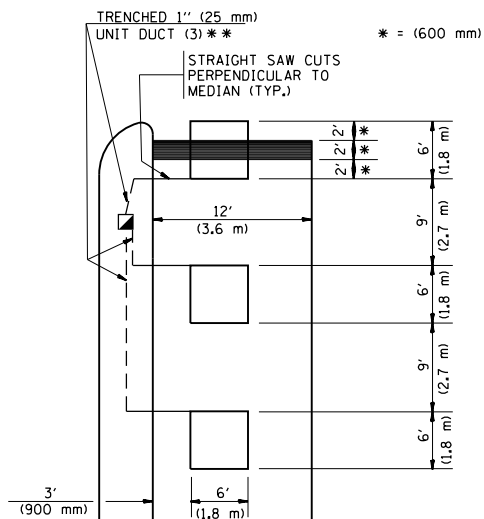
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



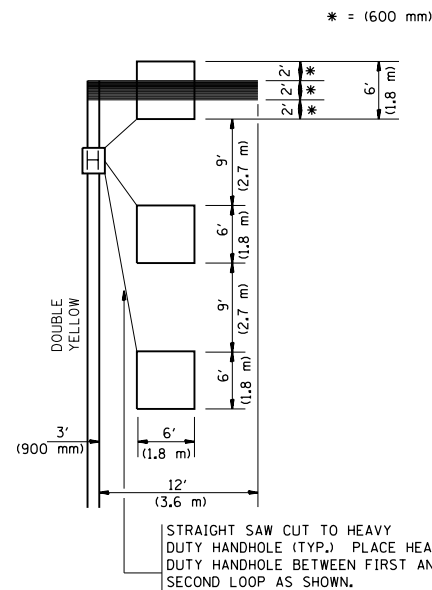
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH**

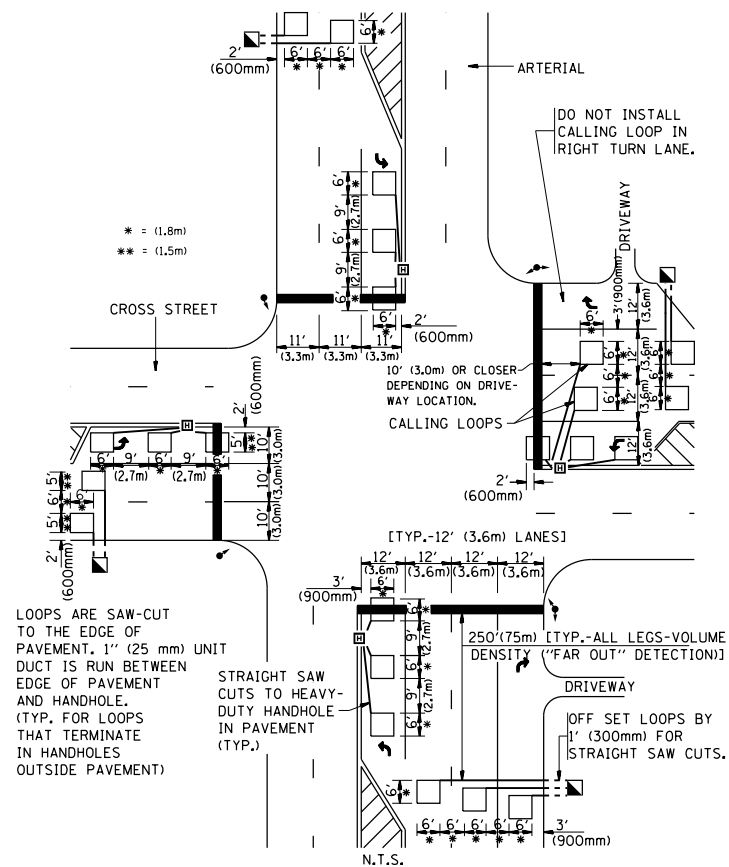
(PROTECTED / PERMITTED LEFT TURN PHASING)



* = (600 mm)

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**

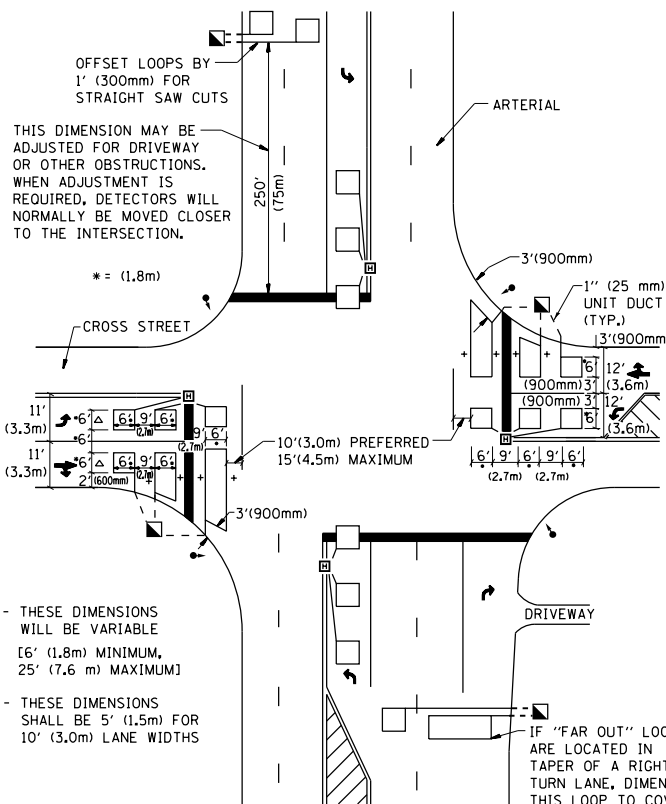


LOOPS ARE SAW-CUT TO THE EDGE OF PAVEMENT. 1" (25 mm) UNIT DUCT IS RUN BETWEEN EDGE OF PAVEMENT AND HANDHOLE. (TYP. FOR LOOPS THAT TERMINATE IN HANDHOLES OUTSIDE PAVEMENT)

STRAIGHT SAW CUTS TO HEAVY-DUTY HANDHOLE IN PAVEMENT (TYP.)

DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



+ - THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]

△ - THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

IF "FAR OUT" LOOPS ARE LOCATED IN TAPER OF A RIGHT TURN LANE, DIMENSION THIS LOOP TO COVER TAPER AREA. DO NOT COVER THE LEFT TURN LANE OR LEFT TURN LANE TAPER.

DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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|----------------------------------------------------|------------------------|------------------|-----------|-----------------------------------------------------------|------------------------------------------------------------------------------------|--------------|-------------------------|---------------------------|--------------|-----------|
| FILE NAME = | USER NAME = drivakosgn | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING | F.A.U. RTÉ. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| et:\pw\work\p1dot\drivakosgn\d0271276\Dist1Std.dgn | | DRAWN - | REVISED - | | | 2887 | 3155-N(13) | COOK | 88 | 88 |
| PLOT SCALE = 100.0000' / 1in. | | CHECKED - R.K.F. | REVISED - | | | TS-07 | | CONTRACT NO. 60W80 | | |
| PLOT DATE = 10/16/2014 | | DATE - | REVISED - | | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. | |