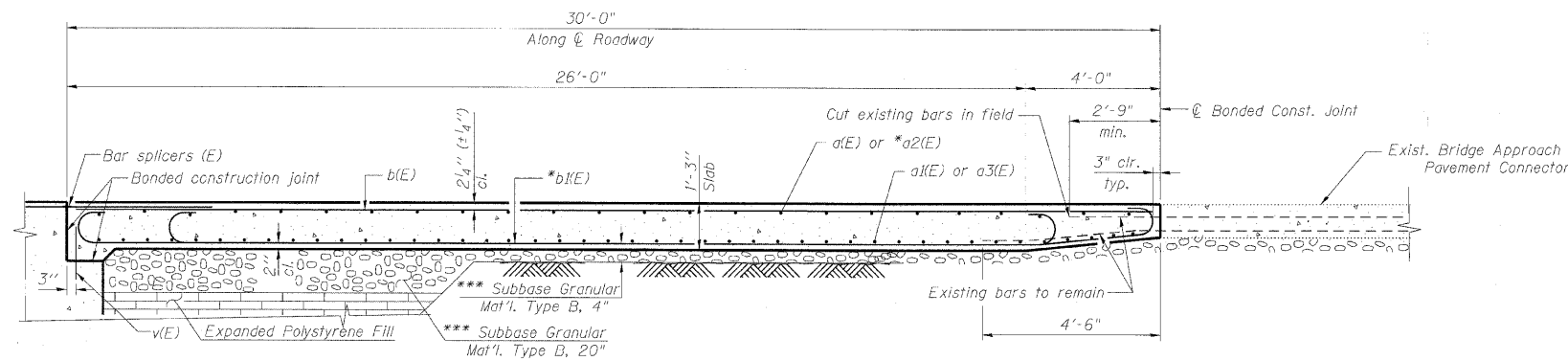


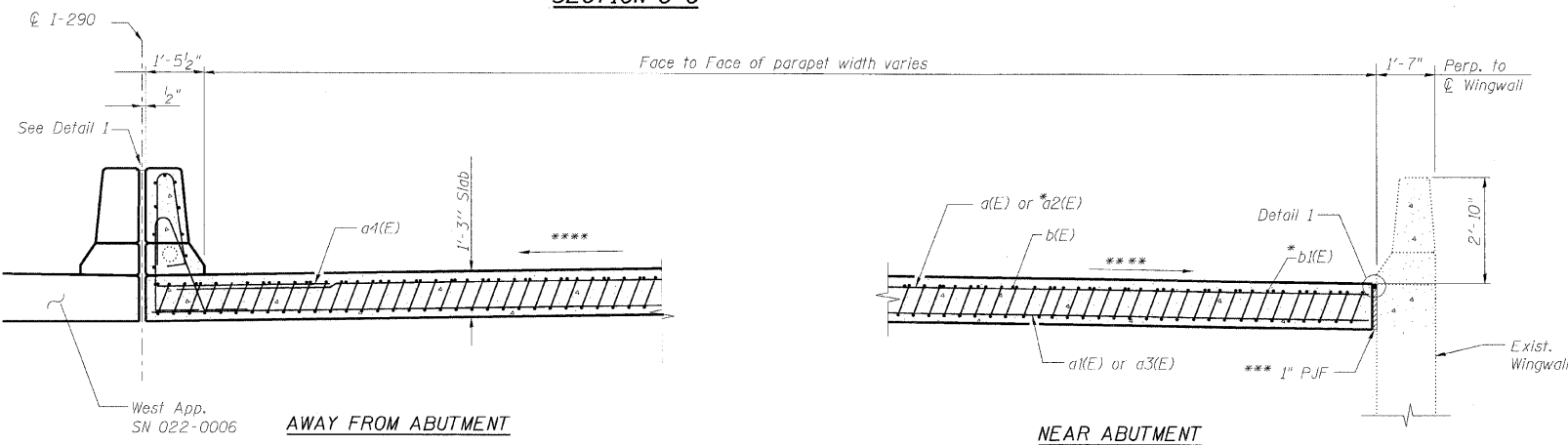
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BILL OF MATERIAL

BAR	NO.	SIZE	LENGTH	SHAPE
a(E)	75	#4	25'-6"	—
a1(E)	92	#5	25'-9"	—
a2(E)	25	#4	23'-5"	—
a3(E)	92	#5	24'-6"	—
a4(E)	21	#6	6'-0"	—
a5(E)	2	#5	4'-0"	—
b(E)	83	#4	29'-8"	—
b1(E)	198	#9	29'-9"	—
b2(E)	1	#4	25'-3"	—
b3(E)	7	#4	17'-5"	—
d(E)	28	#5	5'-7"	—
d1(E)	28	#5	7'-11"	—
e(E)	8	#4	26'-9"	—
e1(E)	1	#8	26'-9"	—
ITEM				
UNIT				
TOTAL				
Approach Slab Removal	Sq. Yd.		292	
Concrete Barrier Removal	Fool		28	
Concrete Superstructure	Cu. Yd.		118.2	
Bridge Deck Grooving	Sq. Yd.		269	
Protective Coat	Sq. Yd.		295	
Reinforcement Bars, Epoxy Coated	Pound		29,070	



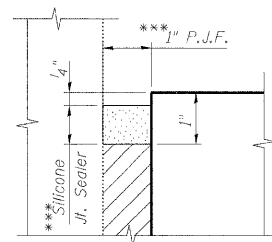
SECTION C-C



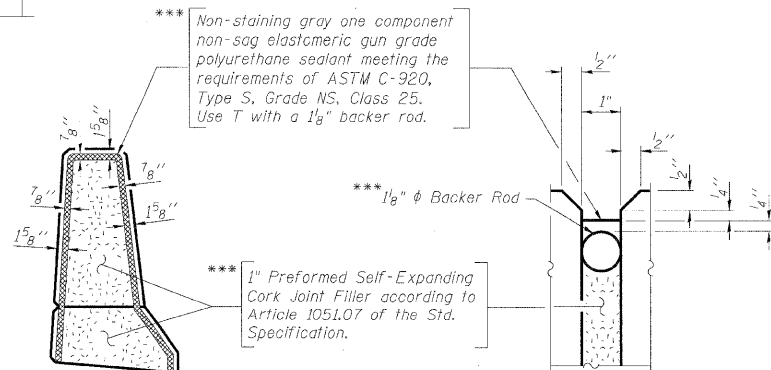
SECTION D-D

(See Plan For dimensions not shown)

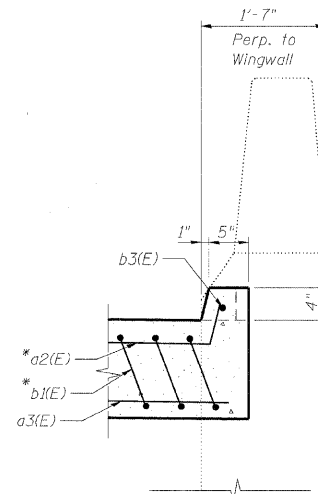
- *Tilt bars as required to maintain clearance.
- ***Cost included with Concrete Superstructure.
- ****Match existing grades and cross slopes.



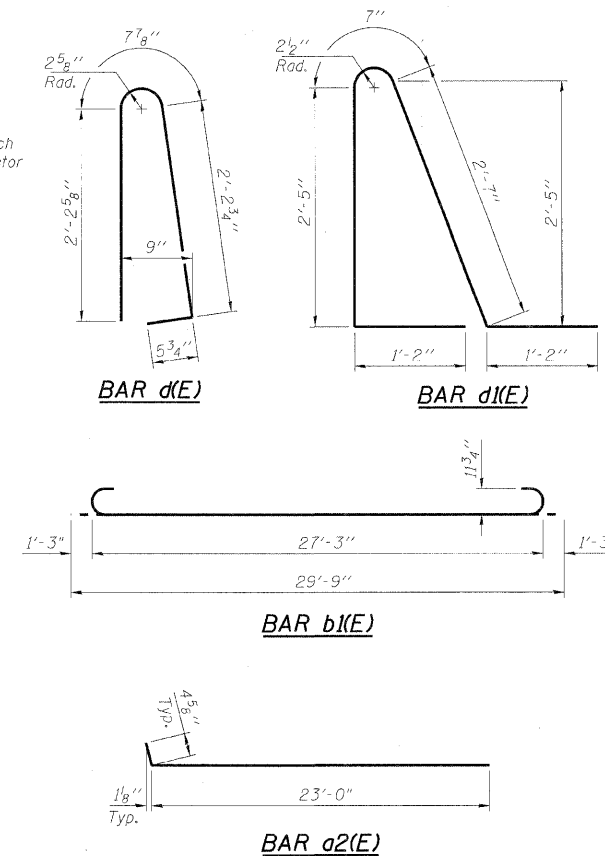
DETAIL 1



DETAIL 2



SECTION A-A



BAR d(E)

BAR d1(E)

BAR b1(E)

BAR a2(E)

Notes:

- a(E), a1(E), a2(E) and a3(E) bar spacings measured parallel to \varnothing Roadway. b(E) and b1(E) bars spacings measured perpendicular to \varnothing Roadway.
- For existing approach slab and shoulder pavement details, see existing plans.
- Existing reinforcement bars extending into the concrete removal area shall be blast-cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during approach slab removal shall be repaired or replaced with an approved bar splicer or anchorage system. Cost included with Approach Slab Removal.
- Approach slab and parapet concrete shall be paid for as Concrete Superstructure.
- Reinforcement shall be paid for as Reinforcement Bars, Epoxy Coated.
- For bar splicer details, see Bar Splicers Assembly Details sheet.
- For Expanded Polystyrene Fill and drainage treatment details, see sheet 16.
- The Contractor shall exercise extreme care with the existing conduits in sections of the parapet to be removed and to protect and support the conduit. The Contractor will be required to repair any damage done to the conduit to the satisfaction of the Engineer. No splicing will be allowed to any cable damage resulting from this work, instead the Contractor will be required to repair the entire span of any damaged cable at no additional cost to the Department.
- Bars indicated thus 8x2-#4 etc. indicates 8 lines of bars with 2 lengths per line.
- Minimum bar lap length: #4 = 1'-8" #5 = 2'-2"
- The steel plate beam guardrail is scheduled to be removed and reinstalled. The guard rail posts located within the proposed approach slab shall be installed prior to placement of the approach slab concrete. The approach slab reinforcement shall not be cut but should be spaced around the reinstalled guard rail posts.
- Work this sheet with West Bridge Approach Slab Details (1 of 2) sheet.

WEST BRIDGE APPROACH SLAB DETAILS

(2 OF 2)

STRUCTURE NO. 022-0005

DESIGNED	MFB
CHECKED	KWS
DRAWN	RMG
CHECKED	KWS

benesch

alfred benesch & company
Engineers • Surveyors • Planners
205 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450 Job No. 10050

SHEET NO. 7 25 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	290	2009-099 BR	COOK/DUPAGE	309	251
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT		
			CONTRACT NO. 60157		