

03-03-2017 LETTING ITEM 016

FOR INDEX OF SHEETS, SEE SHEET NO.

HIGHWAY STANDARDS

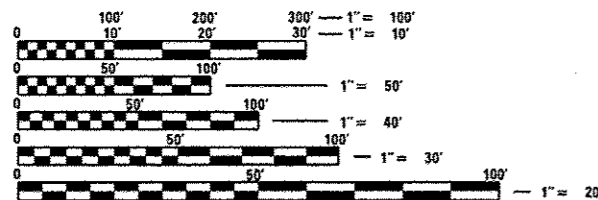
000001-06	701301-04	701001-02
406201-01	701306-03	701006-05
606001-06	701311-03	701011-04
606301-04	701326-04	
630001-11	701901-06	
642006	780001-05	
701201-04	781001-04	
	782006	

DISTRICT STANDARDS

- 406101-D4
- 606301-D4
- 630101-D4
- 780001-D4

DESIGN DESIGNATION:

MINOR ARTERIAL NON-URBAN
 ADT: 5600 (2014)
 MU = 22.47% (2014)
 SU = 12.92% (2014)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

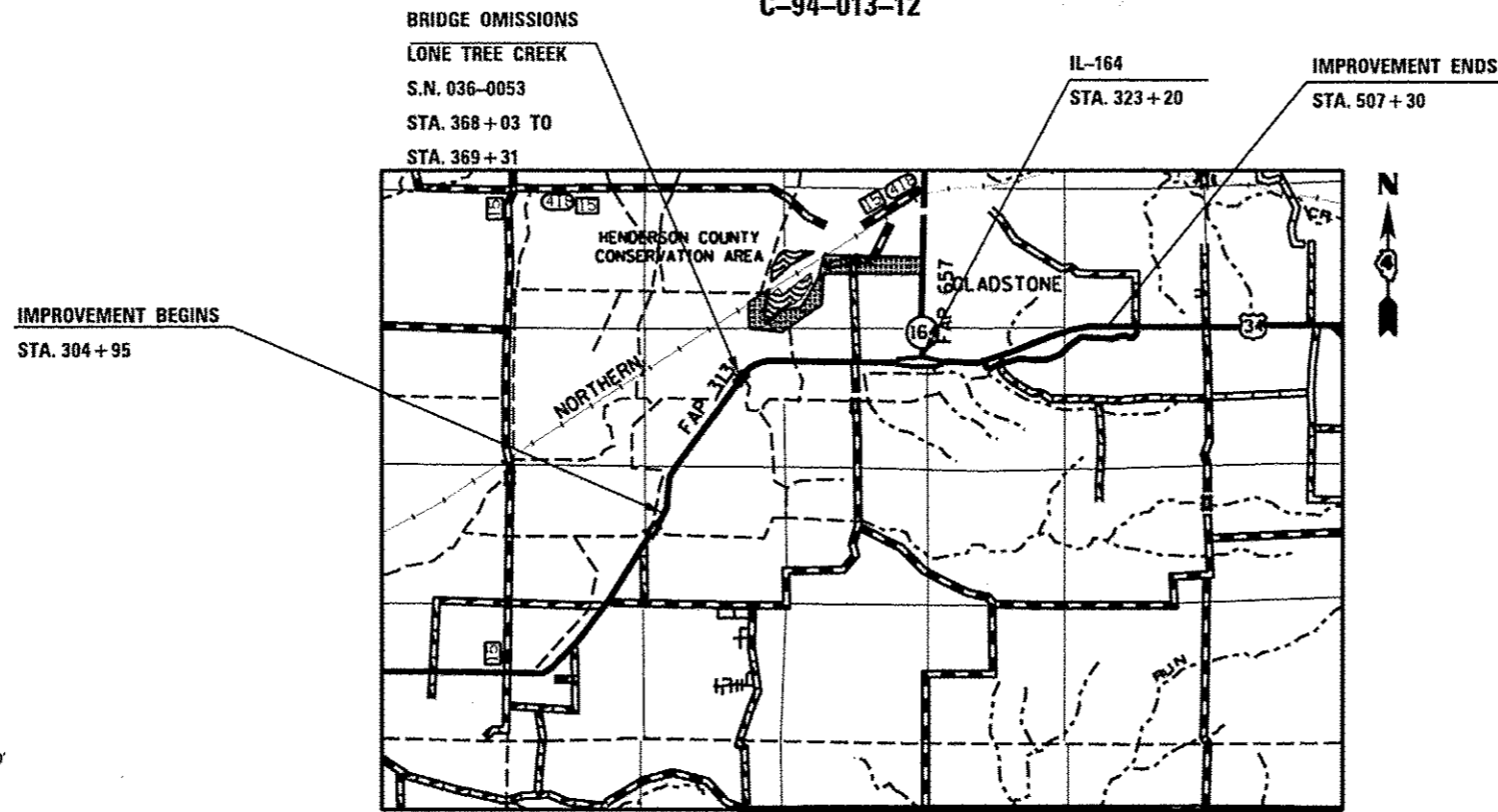
J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123
 OR 811

PROJECT ENGINEER: CHRISTOPHER MAUSHARD (309) 671-3453
 PROJECT MANAGER: MIKE MOHAMED (309) 671-3462
 CATALOG NO. 034721-00D
 CONTRACT NO. 68A67

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

**PROPOSED
 HIGHWAY PLANS**

F.A.P. ROUTE 313 (U.S. ROUTE 34)
 SECTION 6RS-4;7RS-5
 TYPE OF IMPROVEMENT: 3P
 HENDERSON COUNTY
 PROJECT ACNHPP-0313 (023)
 C-94-013-12



GROSS LENGTH = 20,733 FT. = 3.93 MILE
 NET LENGTH = 20,605.07 FT. = 3.90 MILE

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	6RS-4;7RS-5	HENDERSON	33	1
		ILLINOIS	CONTRACT NO. 68A67	

D-94-011-12



DESCRIPTION OF WORK

THIS PROJECT CONSIST OF PAVEMENT PATCHING, SHOULDER WIDENING, AND RESURFACING ON US 34 FROM EAST OF PD CREEK TO BIGGSVILLE BY-PASS.

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

SUBMITTED 12/9 20 16
Kend A. Gutter
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

Jan 27 20 17
Maureen M. Addis PE
 ENGINEER OF DESIGN AND ENVIRONMENT

Jan 27 20 17
David A. [Signature]
 DIRECTOR OF PROGRAM DEVELOPMENT

PRINTED BY THE AUTHORITY
 OF THE STATE OF ILLINOIS

REV

INDEX OF SHEETS

1.	COVER SHEET
2.	INDEX OF SHEET
2.-3.	GENERAL NOTES
4.	JOB SPECIFIC NOTES
4.	STATUS OF UTILITIES
5.-7.	SUMMARY OF QUANTITIES
8.-12.	TYPICAL SECTIONS
13.-20.	SCHEDULES OF QUANTITIES
21.	LINE DIAGRAM
22.	INTERSECTION DETAIL
23.	PAVEMENT MARKING DETAIL
24.	BUTT JOINT DETAIL
25.-33.	DISTRICT STANDARDS

GENERAL NOTES

AGGREGATE FOR DRIVEWAY REPLACEMENT

The material used for construction of permanent aggregate driveways shall be gravel or crushed stone as directed by the Engineer, to replace in kind the existing aggregate driveways.

No additional compensation shall be provided for this requirement but shall be considered as included in the cost of the pay item for the aggregate as specified on the plans.

BUTT JOINT CUTTING TIME RESTRICTION

Butt joints shall not be milled more than three (3) days prior to placement of the bituminous surface course.

PAVING SURFACE COURSE

Continuous paving operations on the main roadway shall be maintained at all times during the construction of the hot-mix asphalt surface. No interruptions for side roads, entrances, turn lanes, etc. will be allowed.

PAVEMENT STATIONING NUMBERS & PLACEMENT

The Contractor shall provide labor and materials required to imprint pavement station numbers in the finished surface of the pavement and/or overlay. The numbers shall be approximately 3/4 inch (20mm) wide, 5 inches (125 mm) high and 5/8 inch (15 mm) deep.

The pavement station numbers shall be installed as specified herein:

Interval – 200 feet (English stationing) or 100 meters (metric stationing)

Bottom of Numbers – 6 inches (150 mm) from the inside edge of the pavement marking

Location:

- * 2,3, & 5 Lane Pavements – right edge of pavement in direction of increasing stations
- * Multi-Lane Divided Roadways – outside edge of pavement in both directions
- * Ramps – along baseline edge of pavement

Position – stations shall be placed so they can be read from the adjacent shoulder

Format – English (Metric) pavement stations shall use this format "XXX (XX + X00)" where X represents the pavement station

This work will not be paid for separately, but will be considered included in the cost of the associated pavement and/or overlay pay items.

FILE NAME : Phase II Sheets.dgn	USER NAME : OTEROFEBUSA	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE : 100.0000' / 1"	DRAWN -	REVISED -						313	GRS-4:7RS-5	HENDERSON	33	2
	PLOT DATE : 12/9/2016	CHECKED -	REVISED -		REVISED -	CONTRACT NO. 68A67				ILLINOIS FED. AID PROJECT			
				NOT TO SCALE * SHEET NO. 1 OF 2 SHEETS STA. TO STA.									

GENERAL NOTES (CONTINUED)

PROPERTY OWNER ACCESS REQUIREMENTS

Access must be maintained to all existing properties during construction per Article 107.09 unless arrangements are made in writing by the Contractor with the property owners with a copy to the Engineer for short-term closures.

SIGN POST HOLES

Vertical holes shall be constructed in the island pavement and/or concrete median of the type specified or concrete median surface 4 inches (100mm). The holes shall be 24 inches (600mm) in diameter or 24 inches (600mm) square and they shall be free of any obstruction, except earth, for a depth of 5 feet (1.5m) at the locations shown on the plans or as directed by the Engineer. Any holes not used for the placement of signs shall be filled and compacted flush with the top of the island pavement, concrete median of the types specified, or concrete median surface 4 inches (100 mm). The top 3 inches (75 mm) of said compacted fill shall consist of a hot-mix asphalt mixture. All holes in which the sign posts are installed at the time of this contract shall be similarly filled.

This work, including any required pavement removal necessary to construct the sign post holes, will not be paid for separately but shall be included in the contract unit price per square foot (square meter) for ISLAND PAVEMENT and/or CONCRETE MEDIAN of the type specified, or CONCRETE MEDIAN SURFACE, 4 inches (100 mm).

NO PASSING ZONE VERIFICATION

Resident Engineer shall contact Bureau of Operations to verify the location of no passing zones prior to placement of centerline striping.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

Mixture Use(s):	Polymer Surface 1.5" & HMA Shoulders (Surface Lift)	Polymer Level Binder 1"	Partial Depth Patching (two 2" lifts)	HMA Shoulders (Lower Lifts)	Incidental HMA Surface	HMA Base Course
AC/PG:	SBS or SBR 76-28	SBS or SBR 76-28	SBS or SBR 76-28	PG 64-22	PG 64-22	PG 64-22
Design Air Voids:	4.0% @ N=70	4.0% @ N=50	4.0% @ N=70	4.0% @ N=50	4.0% @ N=50	4.0% @ N=50
Mixture Composition: (Mixture Gradation)	IL 9.5	IL 4.75	IL 9.5	IL 19.0	IL 9.5	IL 19.0
Friction Aggregate:	Mix E	N.A.	N.A.	N.A.	Mix C	N.A.
Quality Management Program:	PFP	QCP	QCQA	QCQA	QCQA	QCQA

WINTER SHUTDOWN RESTRICTIONS ON COLD MILLED PROJECTS

Prior to winter shutdown the following steps shall be taken:

- * All cold milled surfaces shall be overlaid.
- * All lanes shall be reopened to traffic.
- * Manholes, where applicable, shall be adjusted to the elevation of the binder course/leveling binder to ease in plowing snow, and re-adjusted to finished grade in the Spring. The initial manhole adjustment will be paid for at the contract unit price and any re-adjustment, as directed by the Engineer, will be paid for in accordance with Article 109.04.
- * Temporary or permanent pavement marking shall be placed as applicable.

STATUS OF UTILITIES

NO UTILITIES CONFLICTS

JOB SPECIFIC NOTES

EARTH EXCAVATION WIDENING

CONTRACTOR SHALL CORE 6" DEEP FOR THE HMA SHOULDER AND PLACE 6" BOTTOM LIFT, LEAVE 1/4" DROPPED OFF TO BE MILLED WITH THE 1/2" CROSS SLOPE TO MINIMIZE NEW SHOULDER MILLING.

EXISTING MAILBOXES

ALL WORK SHALL BE AS SPECIFIED IN THE STANDARD SPECIFICATIONS ARTICLE 107.20. CONTRACTOR SHALL CONTACT POST MASTER AT 306 S. MAIN ST, GLADSTONE, IL 61437, PHONE (309) 627-2766 PRIOR TO RELOCATING EXISTING MAILBOXES FOR MAILBOX TURNOUTS.

CROSSING EXISTING STRUCTURES WITH EQUIPMENT

The following structure SN 036-0053 may be crossed with unloaded MTD.

Any structures not listed above shall be verified by the resident prior to beginning work.

FILE NAME * Phase II Sheets.dgn	USER NAME * QTEROFEBUSA	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STATUS OF UTILITIES /JOB SPECIFIC NOTES	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE * 100.0000 ' / 1" =	DRAWN -	REVISED -			313	6RS-47RS-5	HENDERSON	33	4
	PLOT DATE * 12/9/2016	CHECKED -	REVISED -			CONTRACT NO. 68A67				
	DATE -	REVISED -		NOT TO SCALE*	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		

SUMMARY OF QUANTITIES					
PAY ITEM	DESCRIPTION	UNIT	TOTAL QUANTITY	HENDERSON ROADWAY 0005 80% FED; 20% STATE	HENDERSON MOWING 0005 100% STATE
20200500	EARTH EXCAVATION (WIDENING)	CU YD	2,839	2,839	
25000750	MOWING	ACRE	10		10
35300400	PORTLAND CEMENT CONCRETE BASE COURSE 9"	SQ YD	240	240	
35501306	HOT-MIX ASPHALT BASE COURSE, 5 1/2"	SQ YD	384	384	
40200800	AGGREGATE SURFACE COURSE, TYPE B	TON	53	53	
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	4,375	4,375	
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	706	706	
40600990	TEMPORARY RAMP	SQ YD	169	169	
40603565	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70	TON	6,656	6,656	
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	74	74	
44000151	HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"	SQ YD	77,349	77,349	
44003100	MEDIAN REMOVAL	SQ FT	3,306	3,306	
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	44,544	44,544	
48101200	AGGREGATE SHOULDERS, TYPE B	TON	1,285	1,285	
48203100	HOT-MIX ASPHALT SHOULDERS	TON	3,526	3,526	
60608600	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06	FOOT	662	662	
60618320	CONCRETE MEDIAN SURFACE, 6 INCH	SQFT	2,324	2,324	

17

NON-PARTICIPATING

FILE NAME : Phase II Sheets.dgn	USER NAME : DTEROFEBUSA	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES			F.A.P. RTE. 313	SECTION GRS-4;7RS-5	COUNTY HENDERSON	TOTAL SHEETS 33	SHEET NO. 5
		DRAWN -	REVISED -		NOT TO SCALE*	SHEET NO. 1	OF 3	SHEETS	STA.	TO STA.	CONTRACT NO. 68A67	
		CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT							
		DATE -	REVISED -									

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SUMMARY OF QUANTITIES

PAY ITEM	DESCRIPTION	UNIT	TOTAL QUANTITY	HENDERSON ROADWAY 0005 80% FED; 20% STATE	HENDERSON MOWING 0005 100% STATE
* 63000003	STEEL PLATE BEAM GUARDRAIL, TYPE A, 9 FOOT POSTS	FOOT	5,113	5,113	
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	26	26	
63200310	GUARDRAIL REMOVAL	FOOT	6,256	6,256	
64200108	SHOULDER RUMBLE STRIPS, 8 INCH	FOOT	40,214	40,214	
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6	
67100100	MOBILIZATION	LSUM	1	1	
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	LSUM	1	1	
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	LSUM	1	1	
70100500	TRAFFIC CONTROL AND PROTECTION, STANDARD 701326	LSUM	1	1	
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	10	10	
70300100	SHORT TERM PAVEMENT MARKING	FOOT	12,854	12,854	
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	1,067	1,067	
* 72501000	TERMINAL MARKER - DIRECT APPLIED	EACH	26	26	
* 78009000	MODIFIED URETHANE PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	62	62	
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	75,145	75,145	
* 78009008	MODIFIED URETHANE PAVEMENT MARKING - LINE 8"	FOOT	655	655	
* 78009012	MODIFIED URETHANE PAVEMENT MARKING - LINE 12"	FOOT	745	745	
* 78009024	MODIFIED URETHANE PAVEMENT MARKING - LINE 24"	FOOT	39	39	

* SPECIALTY ITEMS

FILE NAME * Phase II Sheets.dgn	USER NAME * OTEROFEBUSA	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES			F.A.P. RTE. 313	SECTION 6RS-4;7RS-5	COUNTY HENDERSON	TOTAL SHEETS 33	SHEET NO. 6
	PLOT SCALE * 1/8" = 1' / 1/4"	DRAWN -	REVISED -		NOT TO SCALE*	SHEET NO. 2	OF 3 SHEETS	STA.	TO STA.	CONTRACT NO. 68A67		
	PLOT DATE * 12/9/2016	CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT							
		DATE -	REVISED -									

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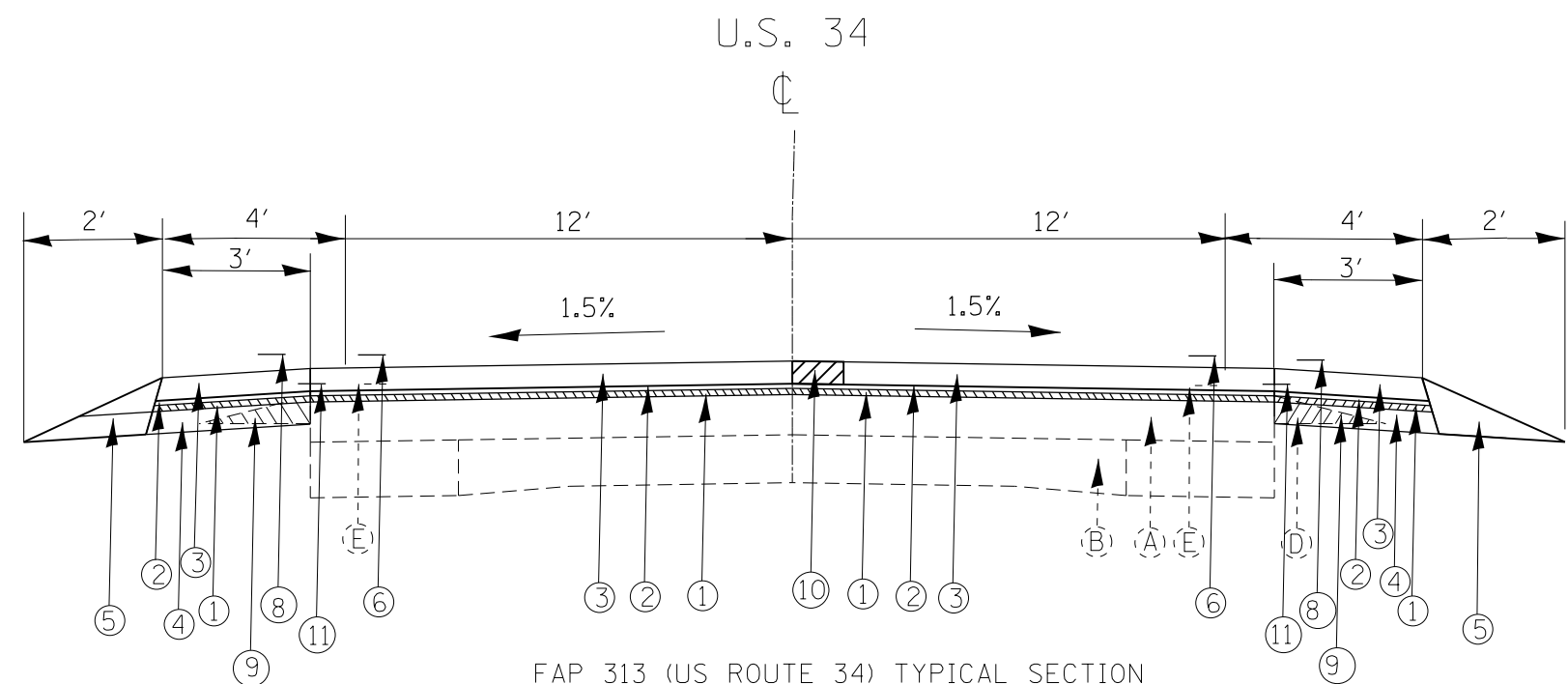
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SUMMARY OF QUANTITIES					
PAY ITEM	DESCRIPTION	UNIT	TOTAL QUANTITY	HENDERSON ROADWAY 0005 80% FED; 20% STATE	HENDERSON MOWING 0005 100% STATE
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	345	345	
* 78200005	GUARDRAIL REFLECTORS, TYPE A	EACH	90	90	
* 78200020	CURB REFLECTORS	EACH	28	28	
X0300003	TRACKLESS TACK	POUND	85,418	85,418	
X0556100	PARTIAL DEPTH PATCHING (SPECIAL)	SQ YD	2,688	2,688	
X4400196	HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL	SQ YD	1,117	1,117	
X4421000	PARTIAL DEPTH PATCHING	TON	602	602	
X7015005	CHANGEABLE MESSAGE SIGN	CAL DA	31	31	
XZ013798	CONSTRUCTION STATION LAYOUT	LSUM	1	1	
Z0001002	GUARDRAIL AGGREGATE EROSION CONTROL	TON	466	466	
Z0034105	MATERIAL TRANSFER DEVICE	TON	11,031	11,031	
∅ Z0076600	TRAINEES	hour	1000	1000	
∅ Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	hour	1000	1000	

13 * SPECIALTY ITEMS
∅ 0042

FILE NAME = Phase II Sheets.dgn	USER NAME = QTERDFEUSA	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY QUANTITIES			F.A.P. RTE. = 313	SECTION = 6RS-4;TRS-5	COUNTY = HENDERSON	TOTAL SHEETS = 33	SHEET NO. = 7
	PLOT SCALE = 100.0000' / 1"	DRAWN -	REVISED -		NOT TO SCALE*	SHEET NO. 3 OF 3 SHEETS	STA.	TO STA.	CONTRACT NO. 68A67			
	PLOT DATE = 12/9/2016	CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT							
		DATE -	REVISED -									

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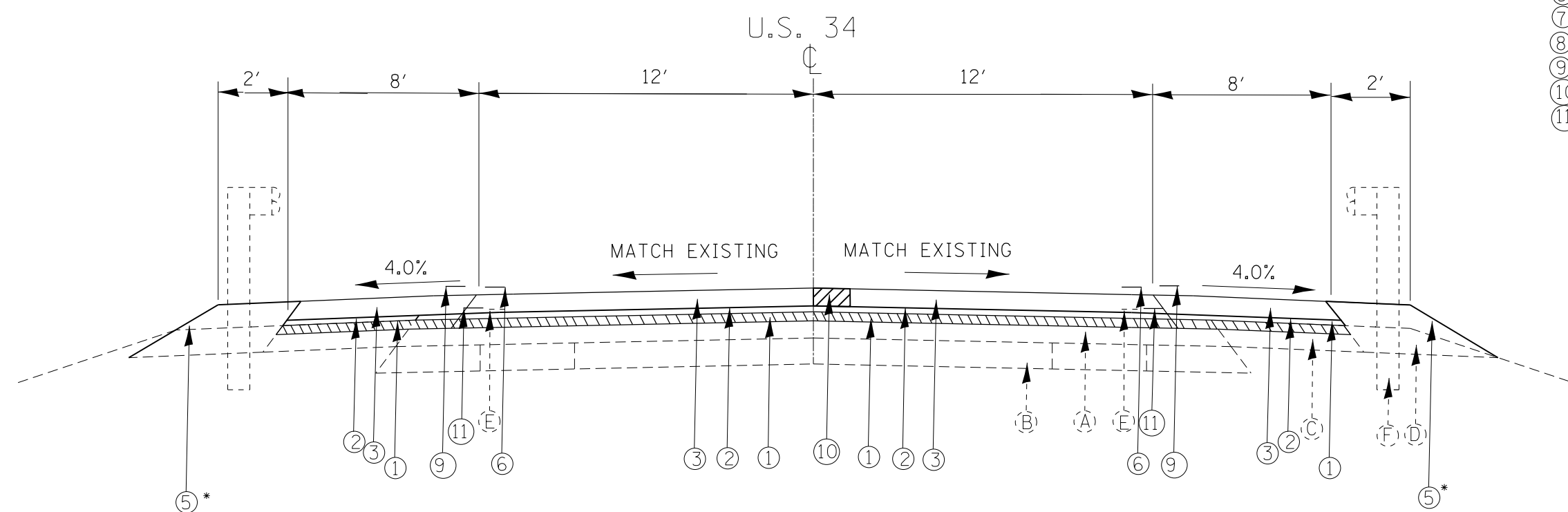
FAP 313 (US ROUTE 34) TYPICAL SECTION
 STA. 304+95 TO STA. 366+00
 STA. 381+00 TO STA. 413+00

LEGEND - EXISTING

- (A) EXIST. HMA OVERLAY, 1/2"
- (B) EXIST. PAVEMENT
- (C) EXIST. HMA SHOULDERS
- (D) EXIST. AGGREGATE SHOULDERS
- (E) EXIST. PAVEMENT MARKING LINE
- (F) EXIST. GUARDRAIL

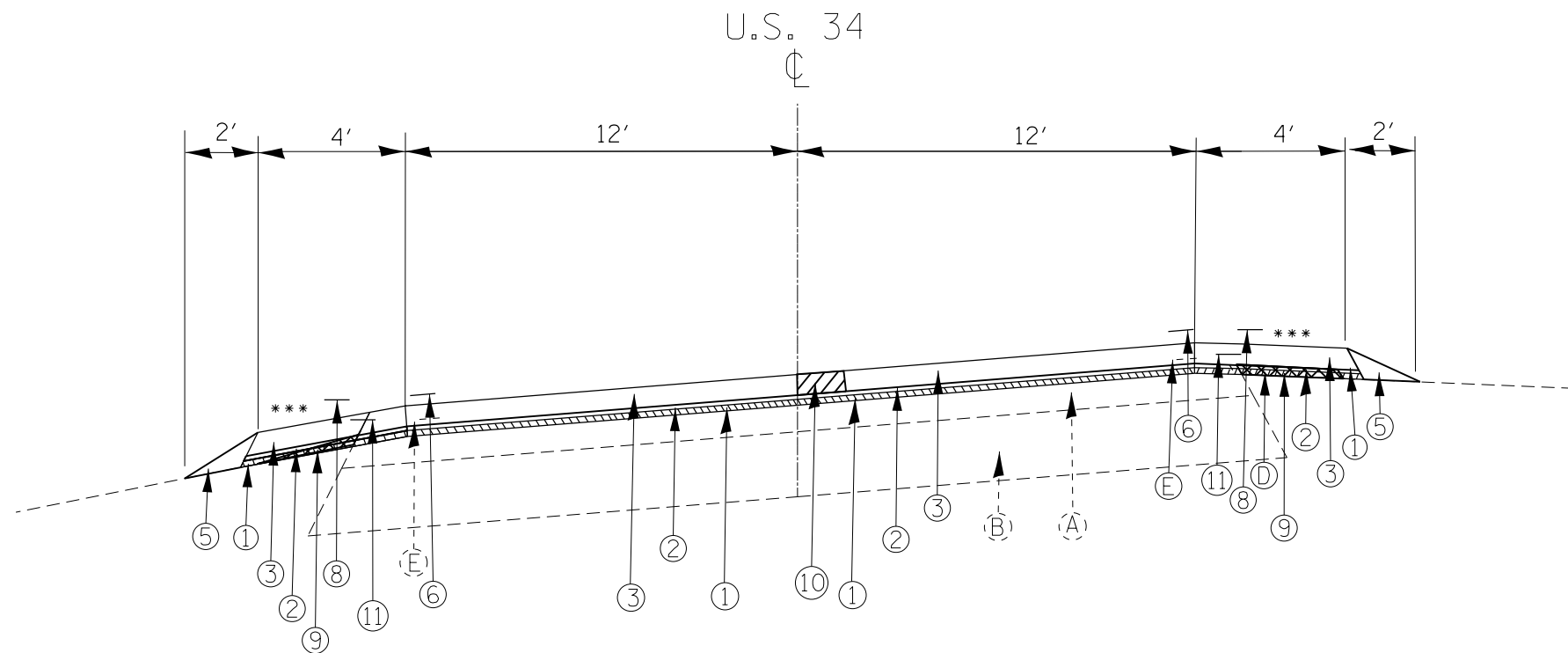
LEGEND - PROPOSED

- ① PROP. HMA SURFACE REMOVAL 1/2"
- ② PROP. LEVELING BINDER 1"
- ③ PROP. HMA SURFACE 1 1/2"
- ④ PROP. HMA SHOULDERS 6"
- ⑤ PROP. AGGREGATE SHOULDERS, TYPE B
- ⑥ PROP. PAINT PAVEMENT MARKING LINE
- ⑦ PROP. GUARDRAIL REPLACEMENT
- ⑧ PROP. SHOULDER RUMBLE STRIPS, 8 IN
- ⑨ PROP. EARTH EXCAVATION (WIDENING)
- ⑩ PROP. HMA REMOVAL SPECIAL TREATMENT
- ⑪ PROP. STRIP REFLECTIVE CRACK CONTROL TREATMENT



FAP 313 (US ROUTE 34) TYPICAL SECTION
 STA. 366+00 TO STA. 371+00
 BRIDGE OMISSION STA. 368+03.41 TO STA. 369+31.34
 *NOTE: GUARDRAIL AGGREGATE EROSION CONTROL AT LOCATIONS OF GUARDRAIL

FILE NAME = D468A67_sht-typical.dgn	USER NAME = OTEROFEBUSA	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED TYPICAL SECTION			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 5.0000' / in.	DRAWN -	REVISED -					313	6RS-4;7RS-5	HENDERSON	33	8
Default	PLOT DATE = 10/14/2016	CHECKED -	REVISED -	SCALE: SHEET 1 OF 5 SHEETS STA. TO STA.			ILLINOIS FED. AID PROJECT CONTRACT NO. 68A67					



*** THE ALGEBRAIC DIFFERENCE BETWEEN S.E. AND HIGH SIDE SHOULDER SLOPES SHALL NOT EXCEED 0.08 FT./FT. LOW SIDE SHOULDER SLOPE IS S.E. SLOPE OR 4% WHICHEVER IS GREATER.

FAP 313 (US ROUTE 34) TYPICAL SUPERELEVATION SECTION

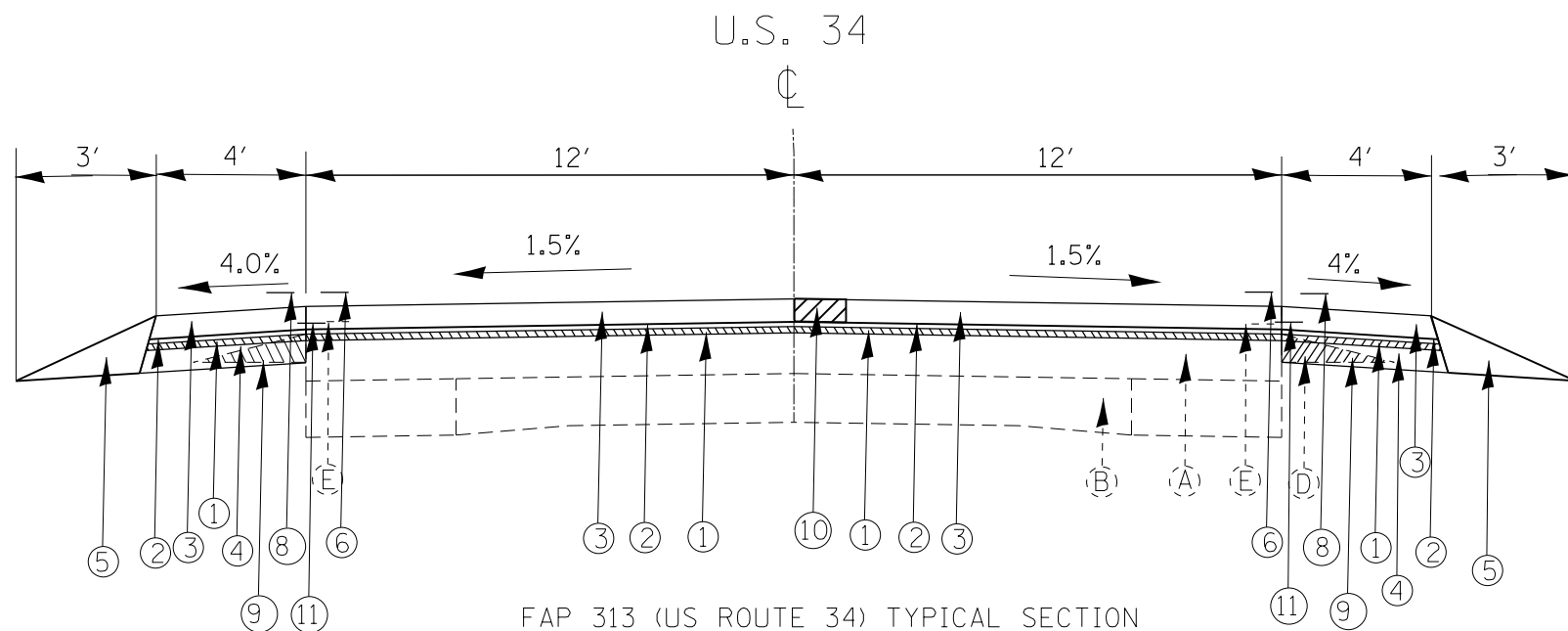
STA. 368+50 TO STA. 371+00.00
 STA. 371+00 TO STA. 381+00.00
 STA. 381+00 TO STA. 383+50.00

LEGEND - EXISTING

- (A) EXIST. HMA OVERLAY, 1/2"
- (B) EXIST. PAVEMENT
- (C) EXIST. HMA SHOULDERS
- (D) EXIST. AGGREGATE SHOULDERS
- (E) EXIST. PAVEMENT MARKING LINE
- (F) EXIST. GUARDRAIL

LEGEND - PROPOSED

- ① PROP. HMA SURFACE REMOVAL 1/2"
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- ⑨ PROP. EARTH EXCAVATION (WIDENING)
- ⑩ PROP. HMA REMOVAL SPECIAL
- ⑪ PROP. STRIP REFLECTIVE CRACK CONTROL TREATMENT



FAP 313 (US ROUTE 34) TYPICAL SECTION
 STA 413+00 TO STA 421+76

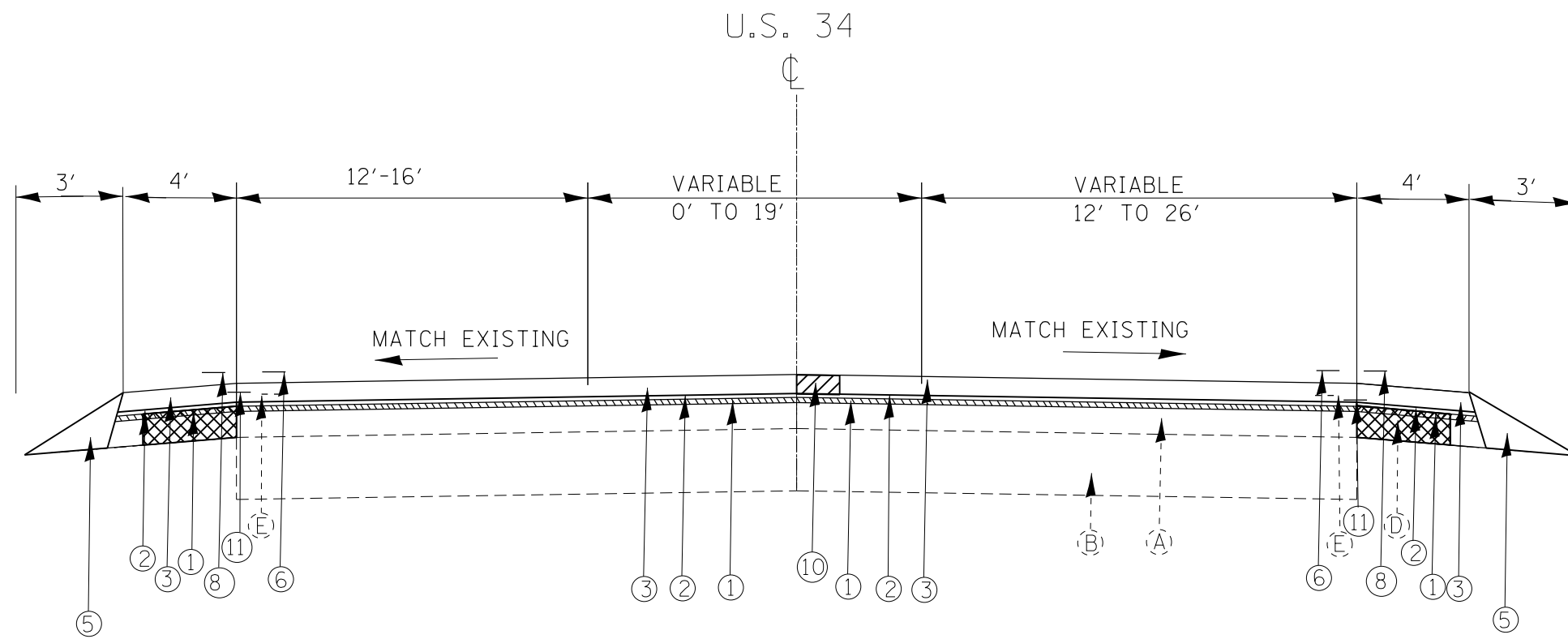
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	PLOT DATE = 10/14/2016	CHECKED -	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PROPOSED TYPICAL SECTION

SCALE: SHEET 2 OF 5 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	6RS-4;7RS-5	HENDERSON	33	9
CONTRACT NO. 68A67				
ILLINOIS FED. AID PROJECT				



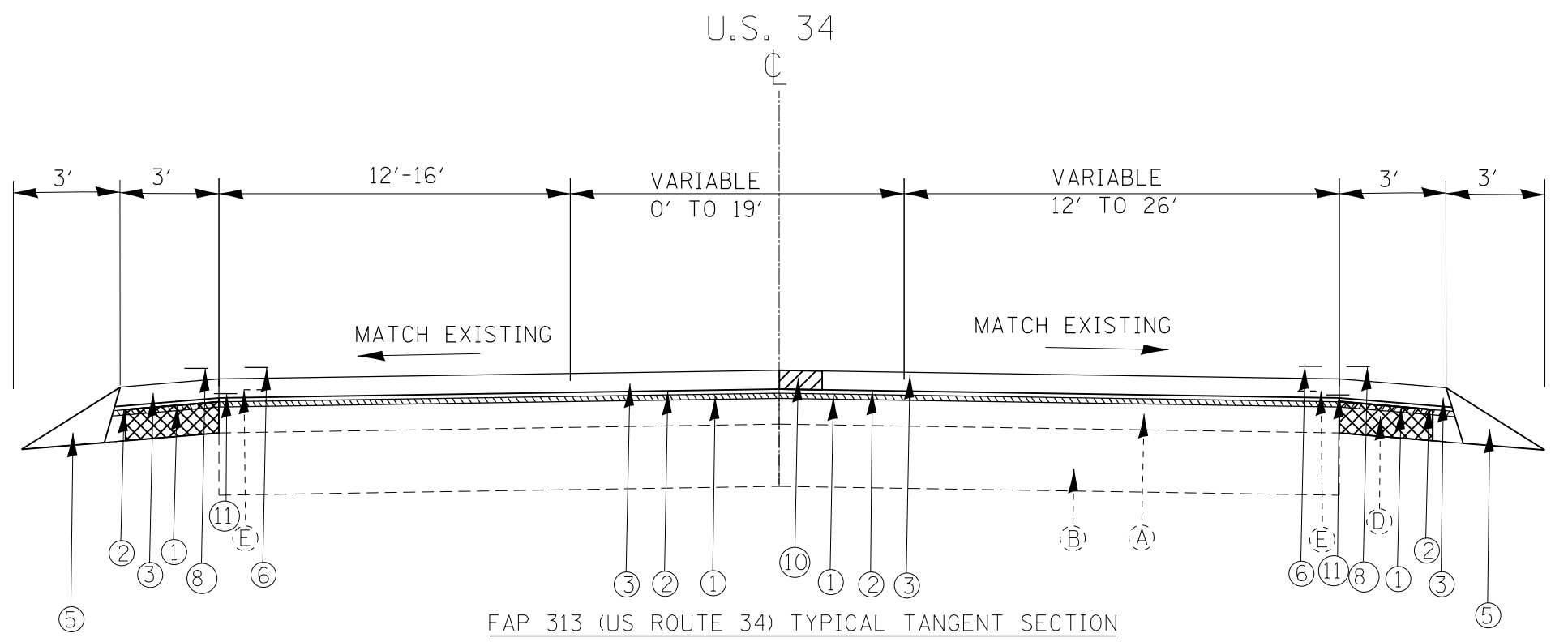
FAP 313 (US ROUTE 34) TYPICAL TANGENT SECTION
STA. 421+76 TO STA. 438+17

LEGEND - EXISTING

- (A) EXIST. HMA OVERLAY, 1/2"
- (B) EXIST. PAVEMENT
- (C) EXIST. HMA SHOULDERS
- (D) EXIST. AGGREGATE SHOULDERS
- (E) EXIST. PAVEMENT MARKING LINE
- (F) EXIST. GUARDRAIL

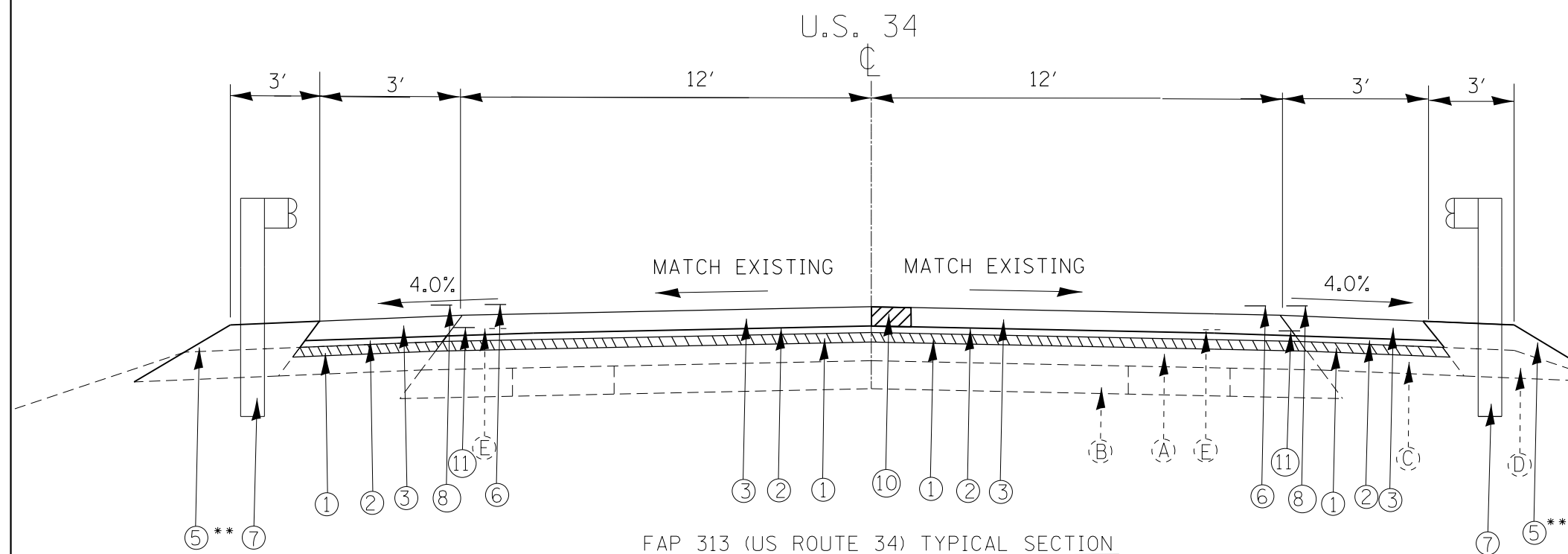
LEGEND - PROPOSED

- ① PROP. HMA SURFACE REMOVAL 1/2"
- ② PROP. LEVELING BINDER 1"
- ③ PROP. HMA SURFACE 1 1/2"
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- ⑨ PROP. EARTH EXCAVATION (WIDENING)
- ⑩ PROP. HMA REMOVAL SPECIAL
- ⑪ PROP. STRIP REFLECTIVE CRACK CONTROL TREATMENT



FAP 313 (US ROUTE 34) TYPICAL TANGENT SECTION
STA. 438+17 TO STA. 446+54

FILE NAME = D468A67_sht-typical.dgn	USER NAME = OTEROFEBUSA	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED TYPICAL SECTION			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 5.0000' / in.	CHECKED -	REVISED -					313	6RS-4;7RS-5	HENDERSON	33	10
Default	PLOT DATE = 10/14/2016	DATE -	REVISED -	SCALE: SHEET 3 OF 5 SHEETS STA. TO STA.			CONTRACT NO. 68A67					
ILLINOIS FED. AID PROJECT												



LEGEND - EXISTING

- (A) EXIST. HMA OVERLAY, 1/2"
- (B) EXIST. PAVEMENT
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LEGEND - PROPOSED

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- ⑨ PROP. EARTH EXCAVATION (WIDENING)
- ⑩ PROP. HMA REMOVAL SPECIAL
- ⑪ PROP. STRIP REFLECTIVE CRACK CONTROL TREATMENT

FAP 313 (US ROUTE 34) TYPICAL SECTION

STA 446+54 TO STA 507+30

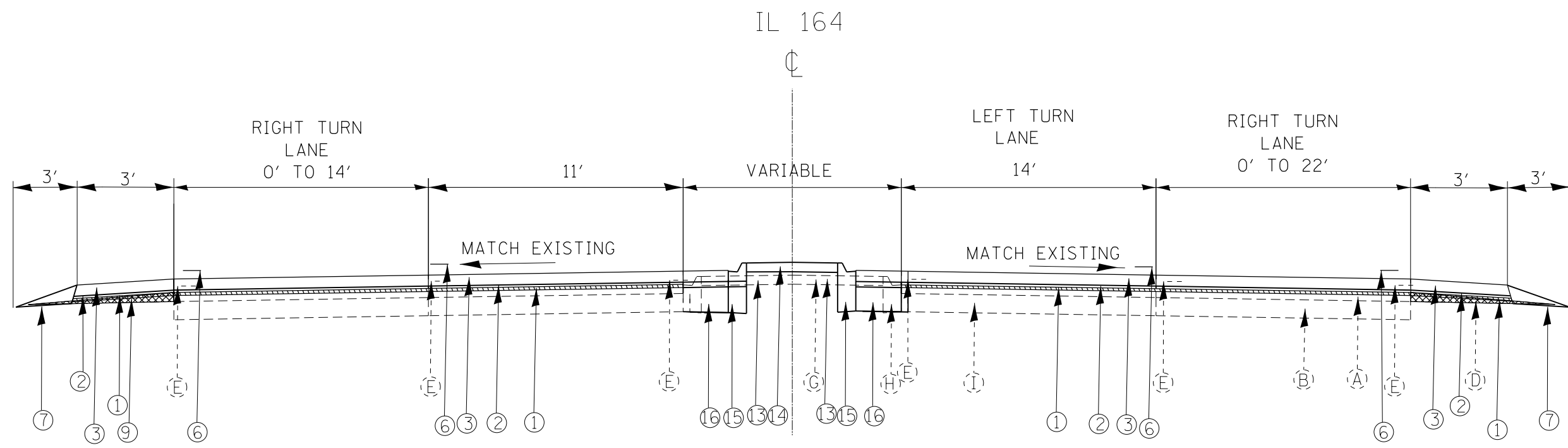
**NOTE: GUARDRIAL AGGREGATE EROSION CONTROL

FILE NAME = D468A67_sht-typical.dgn	USER NAME = OTEROFEBUSA	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 5.0000' / in.	CHECKED -	REVISED -
Default	PLOT DATE = 10/14/2016	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PROPOSED TYPICAL SECTION			
SCALE:	SHEET 4	OF 5 SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	6RS-4;7RS-5	HENDERSON	33	11
CONTRACT NO. 68A67				
ILLINOIS FED. AID PROJECT				



IL - 164 TYPICAL TANGENT SECTION
 STA. 318+80 TO STA. 323+20

LEGEND - EXISTING

- (A) EXIST. HMA OVERLAY, 1/2"
- (B) EXIST. PAVEMENT
- (C) EXIST. HMA SHOULDERS
- (D) EXIST. AGGREGATE SHOULDERS
- (E) EXIST. PAVEMENT MARKING LINE
- (F) EXIST. GUARDRAIL
- (G) EXIST. MEDIAN
- (H) EXIST. CURB & GUTTER
- (I) EXIST. PCC BASE COURSE 9"

LEGEND - PROPOSED

- ① PROP. HMA SURFACE REMOVAL 1/2"
- ② PROP. LEVELING BINDER 1"
- ③ PROP. HMA SURFACE 1 1/2"
- ④ PROP. HMA SHOULDERS 6"
- ⑤ PROP. AGGREGATE SHOULDERS, TYPE B
- ⑥ PROP. PAINT PAVEMENT MARKING LINE
- ⑦ PROP. GUARDRAIL REPLACEMENT
- ⑧ PROP. SHOULDER RUMBLE STRIPS, 8 IN
- ⑨ PROP. EARTH EXCAVATION (WIDENING)
- ⑩ PROP. HMA REMOVAL SPECIAL
- ⑪ PROP. STRIP REFLECTIVE CRACK CONTROL TREATMENT

FILE NAME = D468A67_sht-typical.dgn	USER NAME = OTEROFEBUSA	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED TYPICAL SECTION			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 5.0000' / in.	CHECKED -	REVISED -					313	6RS-4;7RS-5	HENDERSON	33	12
Default	PLOT DATE = 10/14/2016	DATE -	REVISED -		SCALE:	SHEET 5	OF 5 SHEETS	STA.	TO STA.	CONTRACT NO. 68A67		
										ILLINOIS FED. AID PROJECT		

TABULATION OF RESURFACING QUANTITIES - MAINLINE												
LOCATION	LENGTH	WIDTH	AREA	HMA SURFACE REMOVAL BUTT JOINT	TEMPORARY RAMP	HMA SURFACE REMOVAL, 1/2 IN	HMA SURFACE REMOVAL, SPECIAL	TRACKLESS TACK	POLY LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1 IN	POLY HMA SURFACE COURSE, MIX E, N70, 1.5 IN	MATERIAL TRANSFER DEVICE	
	FT	FT	SQ YD	SQ YD	SQ YD	SQ YD	SQ YD	POUND	TON	TON	TON	
US-34												
STA. 304+95.00 TO STA. 305+25.00	30	24.0	80	80.0	16.0			86	4.5	6.7	11.2	
STA. 305+25.00 TO STA. 305+45.00	20	24.0	53	53.3	16.0			58	3.0	4.5	7.5	
STA. 305+45.00 TO STA. 367+54.00	6,209	24.0	16557			16,557		17,882	927.2	1390.8	2318.0	
STA. 367+54.00 TO STA. 367+84.00	30	24.0	80	80.0	16.0			86	4.5	6.7	11.2	
STA. 367+84.00 TO STA. 368+04.00	20	24.0	53	53.3	16.0			58	3.0	4.5	7.5	
STA. 368+04.00 TO STA. 369+31.00 SN # 036-0053	OMISSION											
STA. 369+31.00 TO STA. 369+61.00	30	24.0	80	80.0	16.0			86	4.5	6.7	11.2	
STA. 369+61.00 TO STA. 369+81.00	20	24.0	53	53.3	16.0			58	3.0	4.5	7.5	
STA. 369+81.00 TO STA. 421+76.00	5,195	24.0	13853			13,853		14,962	775.8	1163.7	1939.5	
STA. 421+76.00 TO STA. 446+54.00	2,478	43.0	11839			11,839		12,786	663.0	994.5	1657.5	
STA. 446+54.00 TO STA. 506+80.00	6,026	24.0	16069			16,069		17,355	899.9	1349.8	2249.7	
STA. 506+80.00 TO STA. 507+10.00	30	24.0	80	80.0	16.0			86	4.5	6.7	11.2	
STA. 507+10.00 TO STA. 507+30.00	20	24.0	53	53.3	16.0			58	3.0	4.5	7.5	
SIDEROAD IL-164												
STA. 318+80.00 TO STA. 321+98.00	318	123.0	1,860			1,795		2,009	104.2	156.3	260.5	
STA. 321+98.00 TO STA. 322+70.00	72	35.0	280			280		302	15.7	23.5	39.2	
STA. 322+70.00 TO STA. 323+00.00	30	33.0	110	110	22.0			119	6.2	9.2	15.4	
STA. 323+00.00 TO STA. 323+20.00	20	28.0	62	62	18.7			67	3.5	5.2	8.7	
SIDEROAD 1150 E												
STA. 413+22.00 SOUTH	35	24.5	95			95		103	5.3	8.0	13.3	
STA. 413+22.00 NORTH	30	24.5	82			82		88	4.6	6.9	11.4	
PAVE WIDE MILL WIDE QUANTITIES	20,108	0.5	1,117				1,117	804		93.8	93.8	
SUB-TOTAL:			62,459	706	169	60,571	1,117	67,054	3,435	5,247	8,682	
QUANTITIES FROM RESURFACING SHOULDER TABLE			16,778			16,778		18,120	940	1,409	2,349	
QUANTITIES FROM RESURFACING ENTRANCES & MAILBOX TURNOUTS TABLE								244				
GRAND-TOTAL:			79,237	706	169	77,349	1,117	85,418	4,375	6,656	11,031	

SCHEDULE OF RESURFACING QUANTITIES - SHOULDER

LOCATION	LENGTH FT	EXISTING HMA SHOULDER WIDTH		PROPOSED HMA SHOULDER		AREA SQ YD	HMA SURFACE REMOVAL 0.5 IN		POLY LEVELING BINDER (MACHINE METHOD) IL-4.75,N50, 1 IN		TRACKLESS TACK POUND	POLY HMA SURFACE COURSE, MIX E, N70, 1.5 IN		HMA SHOULDERS		AGGREGATE SHOULDER TYPE B TON	
		LT	RT	LT	RT		LT	RT	LT	RT		LT	RT	LT	RT		
		FT		FT			SQ YD		TON			TON		TON			
US-34																	
1' EXISTING SAFETY SHOULDER	STA. 304+95.00 TO STA. 305+70.00	75	1	1		17	8	8	0.5	0.5	18	0.7	0.7			3.8	
	STA. 305+70.00 TO STA. 306+30.00	60	1	1		13	7	7	0.4	0.4	14	0.6	0.6			3.0	
	STA. 306+30.00 TO STA. 310+40.00	410	1	0		46	46	0	2.6	0	49	3.8	0			20.8	
	STA. 310+40.00 TO STA. 364+20.00	5,380	1	1		1,196	598	598	33.5	33.5	1,291	50.2	50.2			272.6	
	STA. 364+20.00 TO STA. 366+00.00	180	0	1		20	0	20	0.0	1.1	22	0.0	1.7			9.1	
3' EXISTING HMA SHOULDER	STA. 369+81.00 TO STA. 412+90.00	4,309	1	1		958	479	479	26.8	26.8	1,034	40.2	40.2			218.3	
	STA. 306+30.00 TO STA. 310+40.00	410	0	3		137	0	137	0.0	7.7	148	0.0	11.5				
	STA. 364+20.00 TO STA. 366+00.00	180	3	0		60	60	0	3.4	0.0	65	5.0	0.0				
6' EXISTING HMA SHOULDER	STA. 446+54.00 TO STA. 507+30.00	6,076	3	3		4,051	2,025	2,025	113.4	113.4	4,375	170.1	170.1			461.8	
	STA. 306+31.00 TO STA. 307+63.00	132	0	6		88	0	88	0.0	4.9	95	0.0	7.4				
8' EXISTING HMA SHOULDER	STA. 366+00.00 TO STA. 368+04.00	204	8	8		363	181	181	10.2	10.2	392	15.2	15.2			10.3	
	STA. 369+31.00 TO STA. 369+81.00	50	8	8		89	44	44	2.5	2.5	96	3.7	3.7			2.5	
3' PROPOSED HMA SHOULDER	STA. 307+63.00 TO STA. 310+40.00	277			3	0	92	92	0	5.2	0.0	100	7.8	0.0	33.6	0.0	
	STA. 310+40.00 TO STA. 364+20.00	5,380			3	3	3,587	1,793	1,793	100.4	100.4	3,874	150.6	150.6	652.8	652.8	
	STA. 364+20.00 TO STA. 366+00.00	180			0	3	60	0	60	0.0	3.4	65	0.0	5.0	0.0	21.8	
	STA. 369+81.00 TO STA. 412+90.00	4,309			3	3	2,873	1,436	1,436	80.4	80.4	3,102	120.7	120.7	522.8	522.8	
4' PROPOSED HMA SHOULDER	STA. 438+17.00 TO STA. 446+54.00	837			3	3	558	279	279	15.6	15.6	603	23.4	23.4	101.6	101.6	
	STA. 413+49.00 TO STA. 436+47.00	2,298			4	4	2,043	1,021	1,021	57.2	57.2	2,206	85.8	85.8	364.6	364.6	
6' PROPOSED HMA SHOULDER	STA. 436+47.00 TO STA. 438+17.00	170			0	4	76	0	76	0.0	4.2	82	0.0	6.3	0.0	27.0	
	STA. 305+70.00 TO STA. 306+31.00	61			6	6	81	41	41	2.3	2.3	88	3.4	3.4	14.2	14.2	
8' PROPOSED HMA SHOULDER	STA. 306+30.00 TO STA. 307+63.00	133			6	0	89	89	0	5.0	0.0	96	7.4	0.0	31.0	0.0	
	STA. 304+95.00 TO STA. 305+70.00	75			8	8	133	67	67	3.7	3.7	144	5.6	5.6	23.1	23.1	
SIDEROAD IL-164																	
PROPOSED HMA SHOULDER	STA. 318+80.00 TO STA. 320+00.00	120			4	3	93	53	40	3.0	2.2	101	4.5	3.4	19.0	14.6	
PROPOSED AGG SHOULDER	STA. 320+00.00 TO STA. 323+20.00	320														24.3	
SIDEROAD 1150 E																	
PROPOSED HMA SHOULDER	STA. 413+22.00 SOUTH	35			4	4	31	16	16	0.9	0.9	34	1.3	1.3	5.6	5.6	
	STA. 413+22.00 NORTH	30			4	4	27	13	13	0.7	0.7	29	1.1	1.1	4.8	4.8	
SUB-TOTAL:																	
*TOTAL:																	
							16,778	8,349	8,429	468	472	18,120	701	708	1,773	1,753	1,285
							16,778	16,778		940	18,120		1,409		3,526	1,285	

* SEE HMA SURFACE REMOVAL 0.5", POLY LEVELING BINDER AND POLY HMA SURFACE COURSE GRAND TOTAL ON TABLE "TABULATION OF RESURFACING QUANTITIES - MAINLINE".

ENTRANCES & MAILBOX TURNOUTS								
LOCATION		TYPE	LENGTH	PROPOSED HMA AREA	HMA BASE COURSE 5 1/2"	AGG. SURFACE COURSE TYPE B	INCIDENTAL HMA SURFACING 2 1/2"	TRACKLESS TACK
			FT	SQ YD	SQ YD	TON	TON	POUND
US 34								
RT.	STA. 305+59.00	PE	35	15.56			2.18	11.20
RT.	STA. 307+81.00	PE	64	28.44			3.98	20.48
LT.	STA. 307+93.00	MB	72	64.00	64.00		8.96	23.04
LT.	STA. 311+77.00	MB	72	64.00	64.00		8.96	23.04
RT.	STA. 311+92.00	PE	45			2.32		0.00
RT.	STA. 314+53.00	PE	48			2.48		0.00
LT.	STA. 318+50.00	PE	95			4.90		0.00
LT.	STA. 319+31.00	MB	72	64.00	64.00		8.96	23.04
RT.	STA. 320+30.00	PE	43	19.11			2.68	13.76
RT.	STA. 323+68.00	PE	50			2.58		0.00
LT.	STA. 325+22.00	MB	72	64.00	64.00		8.96	23.04
LT.	STA. 325+51.00	PE	25			1.29		0.00
RT.	STA. 325+94.00	PE	34			1.76		0.00
RT.	STA. 338+88.00	PE	80			4.13		0.00
LT.	STA. 341+82.00	PE	38			1.96		0.00
RT.	STA. 342+34.00	PE	120			6.20		0.00
LT.	STA. 342+87.00	MB	72	64.00	64.00		8.96	23.04
LT.	STA. 357+56.00	PE	35			1.81		0.00
LT.	STA. 364+52.00	MB	72	64.00	64.00		8.96	23.04
RT.	STA. 364+62.00	PE	79			4.08		0.00
LT.	STA. 372+77.00	PE	58			2.99		0.00
LT.	STA. 374+40.00	PE	77			3.98		0.00
RT.	STA. 387+22.00	PE	48			2.48		0.00
RT.	STA. 437+68.00	CE	97			5.01		0.00
LT.	STA. 456+02.00	PE	72			3.72		0.00
RT.	STA. 463+26.00	SR	146	64.89			9.08	46.72
RT.	STA. 489+74.00	PE	42	18.67			2.61	13.44
LT.	STA. 490+05.00	PE	28			1.45		0.00
TOTAL:			1791	531	384	53	74	244

* SEE HMA TRACKLESS TRACK GRAND TOTAL ON TABLE "TABULATION OF RESURFACING QUANTITIES - MAINLINE".

SCHEDULE OF GUARDRAIL QUANTITIES						
LOCATION	GUARDRAIL REMOVAL	TR BAR TRM T1 SPL TAN	SPBGR, TY A 9 FT POSTS	TERMINAL MARKER-DIRECT APPLIED	GUARDRAIL REFLECTORS, TYPE A	GUARDRAIL AGGREGATE EROSION CONTROL
	FOOT	EACH	FOOT	EACH	EACH	TON
LT. STA. 325+57 TO STA. 326+82	125.00	2	25	2	4	9.3
LT. STA. 328+43 TO STA. 329+42	99.00	2	25	2	4	7.4
LT. STA. 357+70 TO STA. 358+46	76.00	2	25	2	4	5.7
RT. STA. 420+42 TO STA. 426+96	654.00	2	563	2	8	48.7
RT. STA. 450+96 TO STA. 455+96	500.00	2	400	2	6	37.2
RT. STA. 466+46 TO STA. 476+97	1051.00	2	963	2	13	78.2
LT. STA. 467+41 TO STA. 476+04	863.00	2	775	2	11	64.2
RT. STA. 481+48 TO STA. 483+50	202.00	2	113	2	4	15.0
LT. STA. 482+20 TO STA. 483+97	177.00	2	88	2	4	13.2
LT. STA. 490+19 TO STA. 492+96	277.00	2	188	2	4	20.6
RT. STA. 490+18 TO STA. 495+48	530.00	2	438	2	7	39.5
RT. STA. 496+22 TO STA. 504+46	824.00	2	725	2	10	61.3
RT. STA. 496+17 TO STA. 504+95	878.00	2	788	2	11	65.4
TOTAL:	6,256	26	5,113	26	90	466

ENGINEERS FIELD OFFICE, TYPE A	
LOCATION	CAL MO
US-34	5.88
IL-164	0.12
1150 E	0.02
TOTAL:	6

TRAFFIC CONTROL SURVEILLANCE	
LOCATION	CAL DAY.
US-34	10
TOTAL:	10

MOWING	
LOCATION	ACRE
US-34	10
TOTAL:	10

EARTHWORK SCHEDULE	
LOCATION	EARTH EXCAVATION (WIDENING)
	CU YD
US 34	LT/RT
STA. 304+95 TO STA. 413+00	2,001
STA. 413+00 TO STA. 438+17	653
STA. 438+17 TO STA. 446+54	186
TOTAL:	2,839

MOBILIZATION	
LOCATION	LSUM
US-34	1
TOTAL:	1

CHANGEABLE MESSAGE SIGN	
LOCATION	CAL DA
IL 164	7
US-34	24
TOTAL:	31

CONSTRUCTION STATION LAYOUT	
LOCATION	LSUM
US-34	1
TOTAL:	1

SHOULDER RUMBLE STRIP, 8 INCH	
LOCATION	FOOT
US-34 LT/RT	
STA 304+95 TO STA 368+03	12,616
STA 369+31 TO STA 507+30	27,598
TOTAL:	40,214

TO BE PAID FOR AS HMA SHOULDERS (LOWER LIFT)

TRAFFIC CONTROL			
LOCATION	STD 701201-04	STD 701306-03	STD 701326-04
	L. SUM	L.SUM	L. SUM
US-34	0.98	0.98	0.98
IL-164	0.02	0.02	0.02
1150E	0.00	0.00	0.00
TOTAL:	1	1	1

STRIP REFLECTIVE CRACK CONTROL TREATMENT	
LOCATION	FOOT
US-34 LT/RT	
SHOULDER AND MAINLANE JOINT STA 304+95 TO STA 507+30	40,214
BIDIRECTIONAL WIDENING JOINT STA 425+20 TO STA 446+85	4,330
TOTAL:	44,544

PAVEMENT MARKING																							
LOCATION	MODIFIED URETHANE PAVEMENT MARKING LINE 4"						MODIFIED URETHANE PAVEMENT MARKING LINE 8"		MODIFIED URETHANE PAVEMENT MARKING LINE 12"			MODIFIED URETHANE PAVEMENT MARKING LINE 24"	MODIFIED URETHANE PAVEMENT MARKING LETTERS AND SYMBOLS	RAISED REFLECTIVE PAVEMENT MARKERS									
	SOLID - WHITE		SOLID - YELLOW		SKIP DASH - YELLOW		SOLID WHITE		SOLID - WHITE		DIAGONAL YELLOW	WHITE	WHITE	ONE-WAY AMBER	ONE-WAY CRYSTAL	TWO-WAY AMBER							
	EB	WB	EB	WB	EB	WB	EB	WB	EB	WB	FT	FT	SQFT	EACH	EACH	EACH							
	FT		FT		FT		FT		FT		FT	FT	FT	EACH	EACH	EACH							
US 34																							
STA.	304+95.00	TO STA.	323+84.00	1,889	1,889	3,778																	
STA.	323+84.00	TO STA.	334+00.00	1,016	1,016		1,016	254															
STA.	334+00.00	TO STA.	362+53.00	2,853	2,853			713															
STA.	362+53.00	TO STA.	374+00.00	1,019	1,019	1,147			255														
STA.	374+00.00	TO STA.	381+08.00	708	708	1,416																	
STA.	381+08.00	TO STA.	384+64.00	356	356	356			89														
STA.	384+64.00	TO STA.	408+02.18	2,338	2,338	4,676																	
STA.	408+02.18	TO STA.	418+02.00	941	941	2,000																	
STA.	418+02.00	TO STA.	419+33.00	131	131	262																	
STA.	419+33.00	TO STA.	426+78.00	745	745	745			186														
STA.	426+78.00	TO STA.	437+11.00	1,033	936	3,910			208		393	11	47	50	6	6							
STA.	437+11.00	TO STA.	438+11.00	100	0																		
STA.	438+11.00	TO STA.	449+24.00	1,113	951	3,982			107		296		16	48	3	7							
STA.	449+24.00	TO STA.	461+10.00	1,186	1,186	1,186			297														
STA.	461+10.00	TO STA.	464+00.00	290	290	290	290																
STA.	464+00.00	TO STA.	473+80.00	980	980		980	245															
STA.	473+80.00	TO STA.	478+10.00	430	430			108															
STA.	478+10.00	TO STA.	488+72.00	1,062	1,062	1,062			266														
STA.	488+72.00	TO STA.	507+30.00	1,858	1,858	3,716																	
IL-164																							
STA.	318+82.00	TO STA.	321+06.00	321	350	418			189	151	56		28										
STA.	321+06.00	TO STA.	323+20.00	214	214	338	329																
SUB-TOTAL:				40,836		31,897		2,412		655		56	689	39	62	98	9	238					
TOTAL:						75,145				655		745	39	62	345								

SHORT-TERM PAVEMENT MARKINGS					
LOCATION	LENGTH	COLD MILLED	LEVELING BINDER	PRIME COAT	SURFACE COURSE
	FT	FT	FT	FT	FT
US 34					
STA. 304+95 TO STA. 323+84	1,889	378	378	378	378
STA. 323+85 TO STA. 374+00	4,887	489	489	489	489
STA. 374+00 TO STA. 381+08	708	142	142	142	142
STA. 381+08 TO STA. 408+02	2,694	269	269	269	269
STA. 408+02 TO STA. 418+02	1,000	200	200	200	200
STA. 418+02 TO STA. 427+91	989	99	99	99	99
STA. 427+91 TO STA. 437+11	920	576	576	576	576
STA. 438+11 TO STA. 446+85	874	350	350	350	350
STA. 446+85 TO STA. 507+30	6,045	605	605	605	605
US-34 SUB-TOTAL:	12,426				
IL- 164					
STA. 318+82 TO STA. 321+98	0	0	0	0	0
STA. 321+98 TO STA. 323+20	238	48	48	48	48
IL-164 SUB-TOTAL:	428				
TOTAL:	12,854				

NOTE: STA. 368+03 TO 368+03 BRIDGE OMISSION

SHORT TERM PAVEMENT MARKING REMOVAL		
LOCATION	LENGTH APPLIED	AREA REMOVED
	FT	SQ FT
US 34		
STA. 304+95.00 TO STA. 507+30.00	3,154	1,051
US-34 SUB-TOTAL:	1,051	
IL- 164		
STA. 318+82.00 TO STA. 323+20.00	48	16
IL-164 SUB-TOTAL:	16	
TOTAL:	1,067	

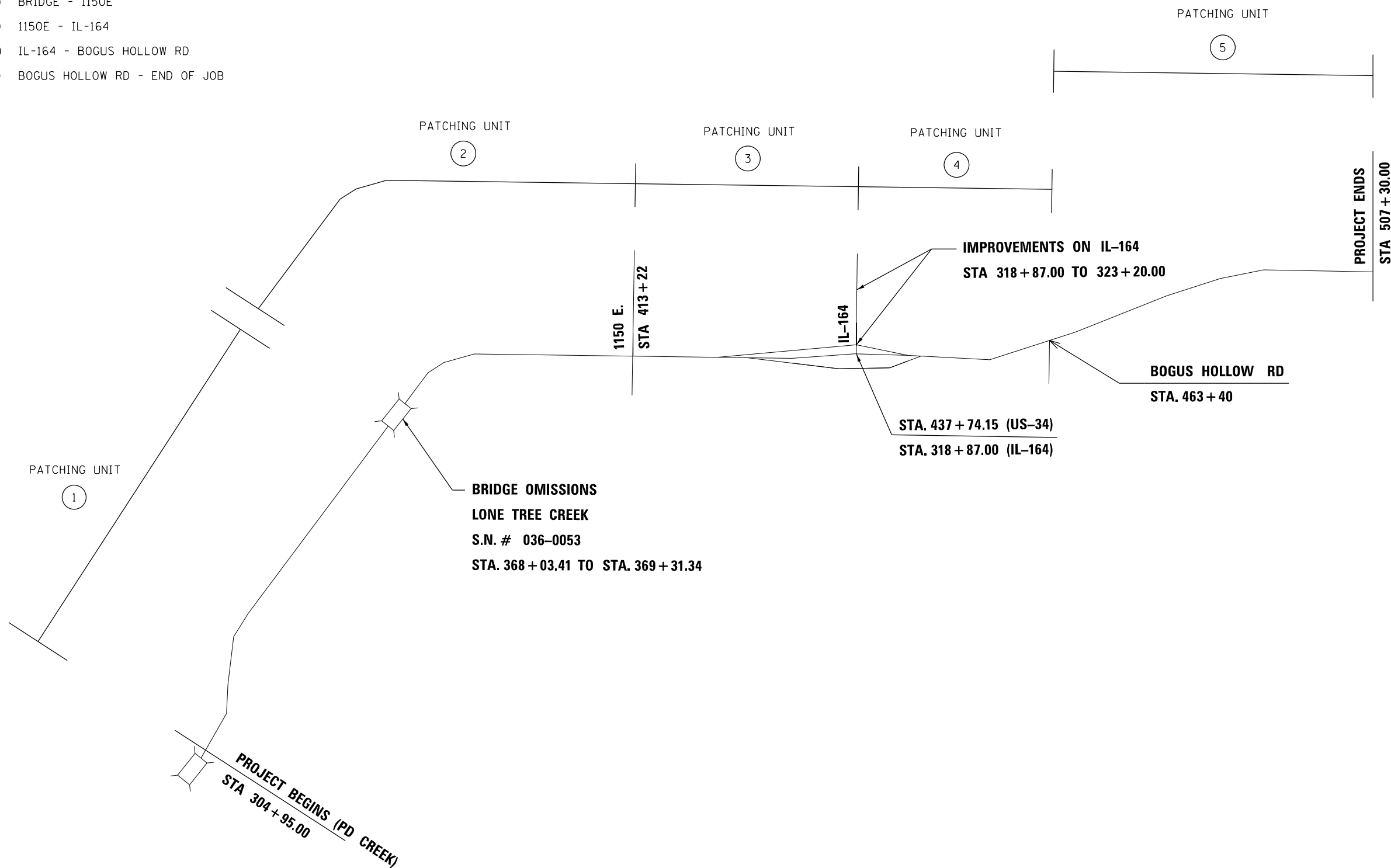
PARTIAL DEPTH PATCHING (SPECIAL)															
UNIT	STA.	TO	STA.	EASTBOUND LANE				CENTER LANE				WESTBOUND LANE			
				L	W	#	AREA	L	W	#	AREA	L	W	#	AREA
				FT	FT		SQ YD	FT	FT		SQ YD	FT	FT		SQ YD
1	STA. 507+30	TO	STA. 463+40	8	12	23	245.3	25	4	1	11.1	8	12	23	245.3
	START OF JOB		BOGUS HOLLOW RD	10	12	8	106.7	150	4	1	66.7	10	12	4	53.3
				20	12	1	26.7					20	12	1	26.7
				40	12	1	53.3					300	4	1	133.3
				50	4	2	44.4								
2	STA. 463+40	TO	STA. 437+74	8	12	2	21.3					8	12	3	32.0
	BOGUS HOLLOW RD		IL-164												
3	STA. 437+74	TO	STA. 413+22	8	12	3	32.0					8	12	2	21.3
	IL-164		1150E	20	12	2	53.3					20	4	1	8.9
				70	12	1	93.3					30	12	1	40.0
				100	4	1	44.4					40	4	1	17.8
				400	4	1	177.8					50	4	1	22.2
												400	4	1	177.8
4	STA. 413+22	TO	STA. 369+31	8	12	2	21.3	200	4	1	88.9	8	12	1	10.7
	1150E		LONE TREE CREEK	10	12	1	13.3					10	12	1	13.3
				20	12	1	26.7					20	12	1	26.7
				50	12	1	66.7					30	12	1	40.0
				200	4	1	88.9					50	4	1	22.2
												50	12	1	66.7
												100	12	1	133.3
												300	4	1	133.3
5	STA. 368+03	TO	STA. 304+95	8	12	8	85.3					8	12	6	64.0
	LONE TREE CREEK		PD CREEK									12	12	2	32.0
SUBTOTAL:				1,030	172	59	1,201	375	12	3	167	1,482	196	55	1,321
TOTAL:				LENGTH (FT)			WIDTH (FT)			# OF PATCHES			AREA (SQ YD)		
				2,887			380			117			2,688		

PARTIAL DEPTH PATCHING															
UNIT	STA.	TO	STA.	EASTBOUND LANE				CENTER LANE				WESTBOUND LANE			
				L	W	#	QUANTITY	L	W	#	QUANTITY	L	W	#	QUANTITY
				FT	FT		TON	FT	FT		TON	FT	FT		TON
1	STA. 507+30	TO	STA. 463+40	8	12	23	55.0	25	4	1	2.5	8	12	23	55.0
	START OF JOB		BOGUS HOLLOW RD	10	12	8	23.9	150	4	1	14.9	10	12	4	11.9
				20	12	1	6.0					20	12	1	6.0
				40	12	1	11.9					300	4	1	29.9
				50	4	2	10.0								
2	STA. 463+40	TO	STA. 437+74	8	12	2	4.8					8	12	3	7.2
	BOGUS HOLLOW RD		IL-164												
3	STA. 437+74	TO	STA. 413+22	8	12	3	7.2					8	12	2	4.8
	IL-164		1150E	20	12	2	11.9					20	4	1	2.0
				70	12	1	20.9					30	12	1	9.0
				100	4	1	10.0					40	4	1	4.0
				400	4	1	39.8					50	4	1	5.0
												400	4	1	39.8
4	STA. 413+22	TO	STA. 369+31	8	12	2	4.8	200	4	1	19.9	8	12	1	2.4
	1150E		LONE TREE CREEK	10	12	1	3.0					10	12	1	3.0
				20	12	1	6.0					20	12	1	6.0
				50	12	1	14.9					30	12	1	9.0
				200	4	1	19.9					50	4	1	5.0
												50	12	1	14.9
												100	12	1	29.9
												300	4	1	29.9
5	STA. 368+03	TO	STA. 304+95	8	12	8	19.1					8	12	6	14.3
	LONE TREE CREEK		PD CREEK									12	12	2	7.2
SUBTOTAL:				1,030	172	59	269	375	12	3	37	1,482	196	55	296
TOTAL:				LENGTH (FT)			WIDTH (FT)			# OF PATCHES			QUANTITY (TON)		
				2,887			380			117			602		

ISLAND PAVEMENT							
LOCATION	LENGTH	AREA	MEDIAN REMOVAL	PCC BASE COURSE 9 IN	COMBINATION CONCRETE CURB & GUTTER TYPE M-6.06	CONCRETE MEDIAN SURFACE 6 IN	CURB REFLECTOR
	FT	SQ YD	SQ FT	SQ YD	FOOT	SQ FT	EACH
SIDEROAD IL-164							
STA. 318+70.90 TO STA. 322+12.00	341	336.6	3,030				
STA. 319+00.00 TO STA. 321+98.00	298	244		93	596	2,194	20
STA. 318+68.00 TO STA. 318+89.00	21	31	276				
STA. 318+85.90 TO STA. 319+02.40	17	25		147	66	130	8
TOTAL:		636	3,306	240	662	2,324	28

PATCHING UNITS

- ① START OF JOB (PD CREEK) - BRIDGE
- ② BRIDGE - 1150E
- ③ 1150E - IL-164
- ④ IL-164 - BOGUS HOLLOW RD
- ⑤ BOGUS HOLLOW RD - END OF JOB



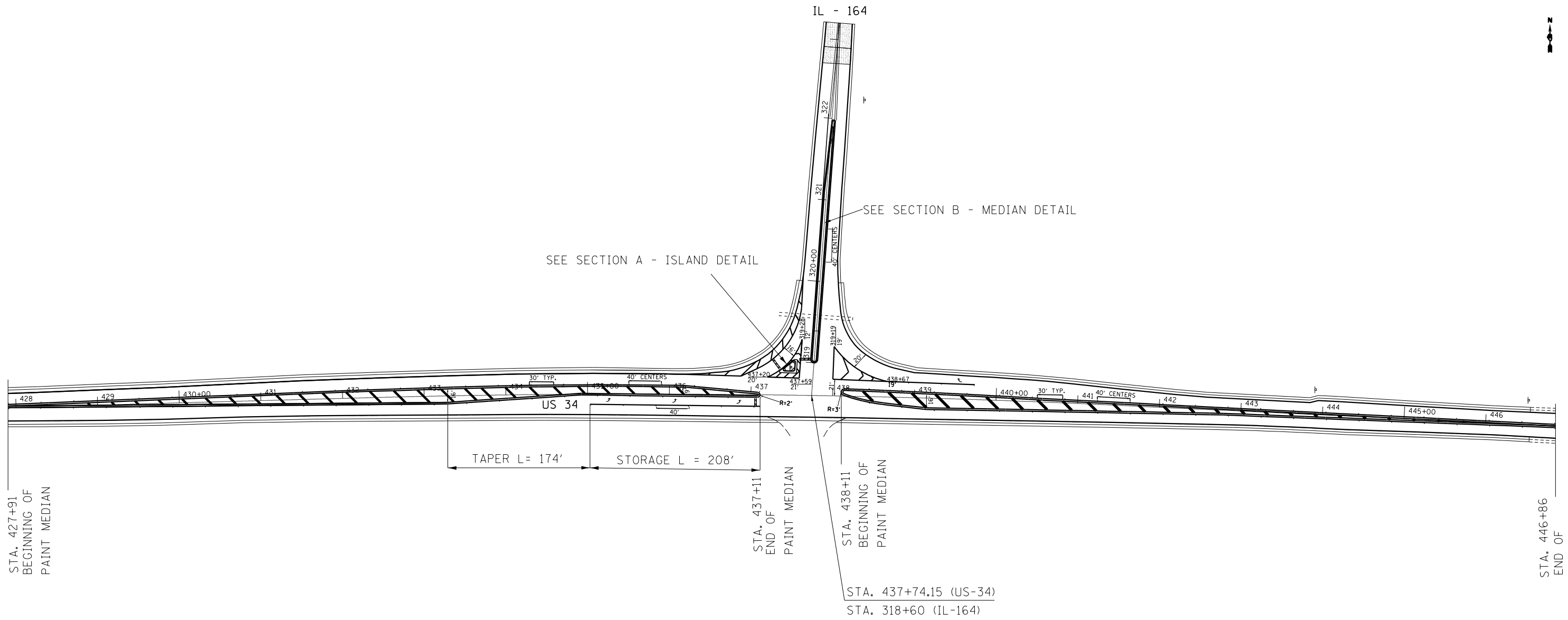
FILE NAME = Phase II Sheets.dgn	USER NAME = OTEROFEBUSA	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/9/2016	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

LINE DIAGRAM

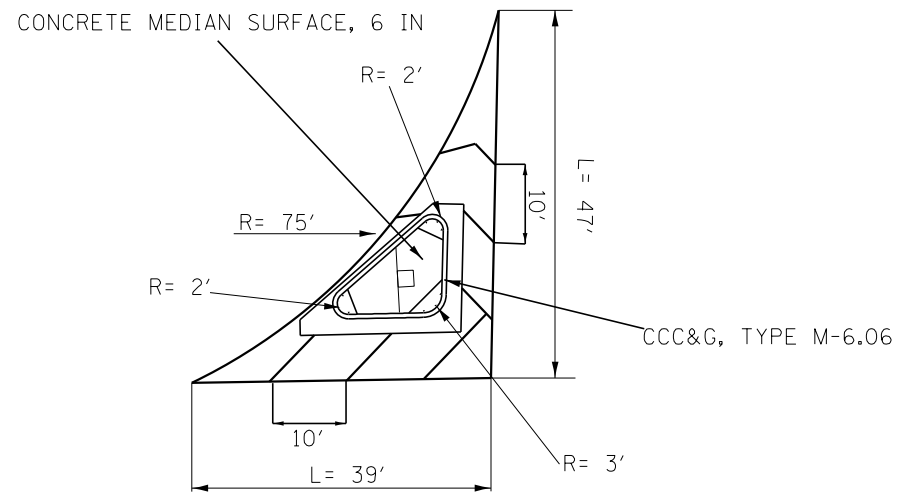
NOT TO SCALE • SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	6R-47RS-5	HENDERSON	33	21
ILLINOIS FED. AID PROJECT			CONTRACT NO. 68A67	



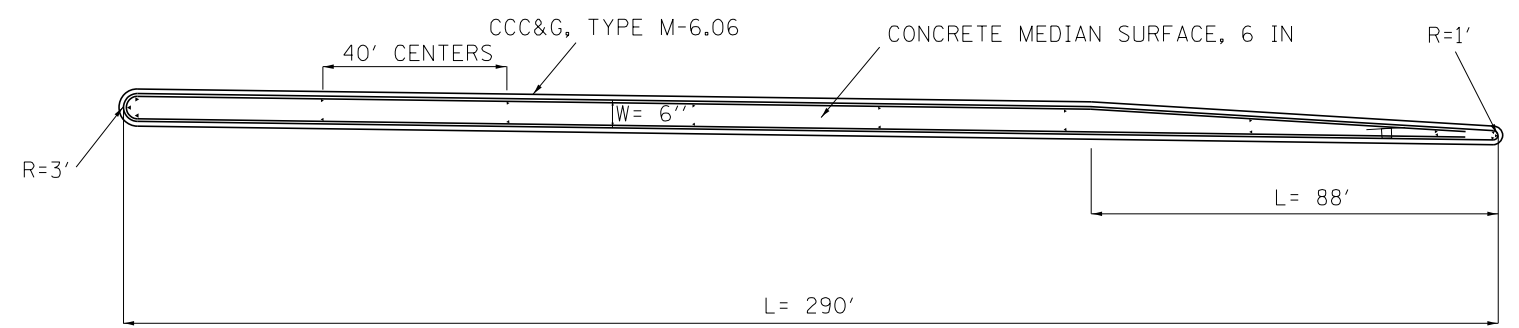
SEE SECTION A - ISLAND DETAIL

SEE SECTION B - MEDIAN DETAIL



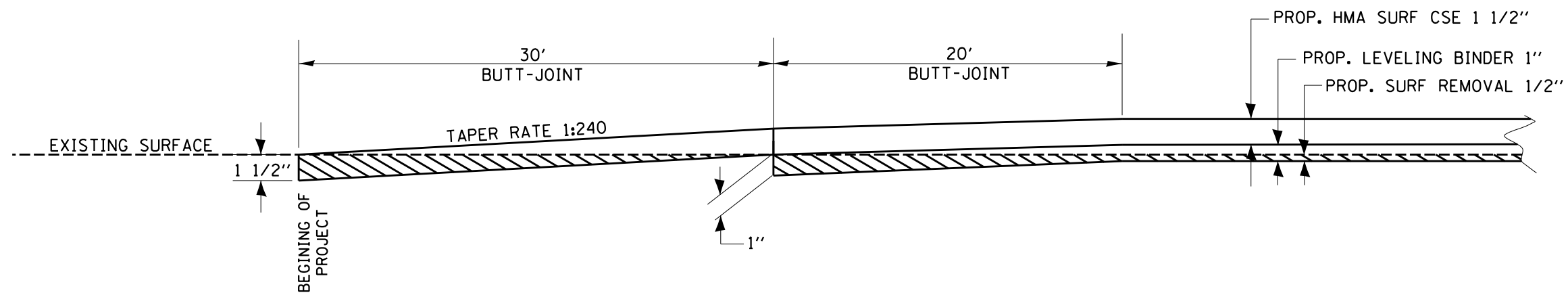
SECTION A - ISLAND DETAIL

INTERSECTION DETAIL



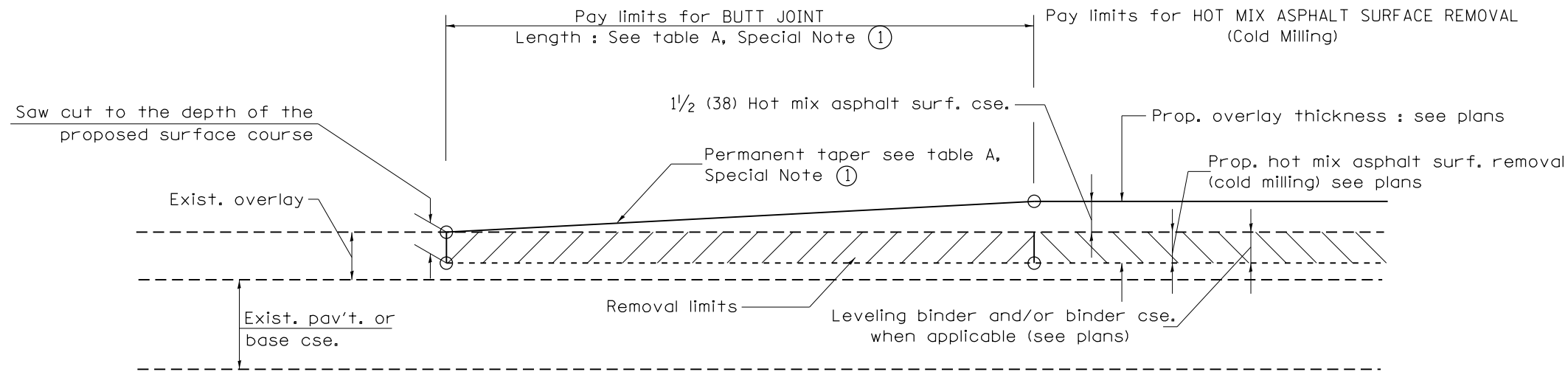
SECTION B - MEDIAN DETAIL

FILE NAME = Phase II Sheets.dgn	USER NAME = oterofebuse	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US-34 AND IL-164 INTERSECTION DETAIL			F.A. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
Default	PLOT SCALE = 124.0000' / in.	DRAWN -	REVISED -		SCALE:	SHEET 1	OF 1	SHEETS	STA.	TO STA.	313	6RS-4;7RS-5	HENDERSON	33	22
	PLOT DATE = 12/9/2016	CHECKED -	REVISED -		CONTRACT NO. 68A67										
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT										



US-34 BUTT-JOINT DETAIL

FILE NAME = Phase II Sheets.dgn	USER NAME = OTEROFEBUSA	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINT DETAIL			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100.0000' / 1"	DRAWN -	REVISED -		313	6RS-4;7RS-5	HENDERSON	33	24			
PLOT DATE = 12/9/2016	CHECKED -	REVISED -	NOT TO SCALE • SHEET NO. 1 OF 1 SHEETS STA. TO STA.			CONTRACT NO. 68A67						
	DATE -	REVISED -	ILLINOIS FED. AID PROJECT									



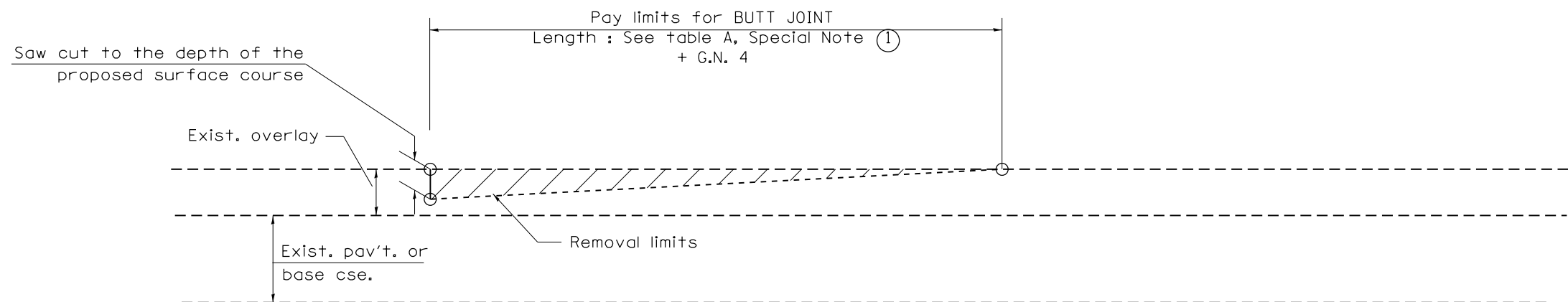
CASE 1 : WITH HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

**TABLE A
TAPER RATES**

SPECIAL NOTE NUMBER	ELEMENT	MAINLINE INTERSTATES & 4-LANE EXPRESSWAYS	ALL OTHERS
①	BUTT JOINT TAPER RATE	1:480	1:240
②	TEMPORARY RAMP TAPER RATE	1:80	1:40

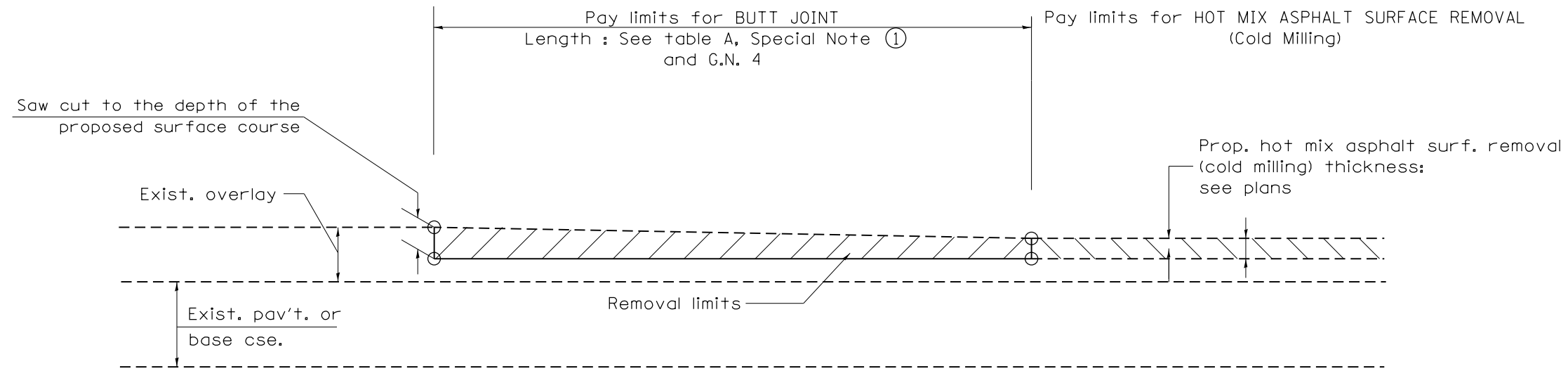
GENERAL NOTES

1. The work shall be done in accordance with Article 406.08 and the Special Provision for Butt Joints.
2. The pavement surface to be removed may be either bituminous or P.C. concrete. The work shall be performed in accordance with Article 440.04 and the Special Provisions for Butt Joints.
3. The saw cut joints shall be primed just prior to the placing of bituminous material. The work will be in accordance with the applicable portions of Article 406.05.
4. The length of butt joint is based on the taper rate times change in cold milling depth within the butt joint pay limits, unless otherwise indicated.

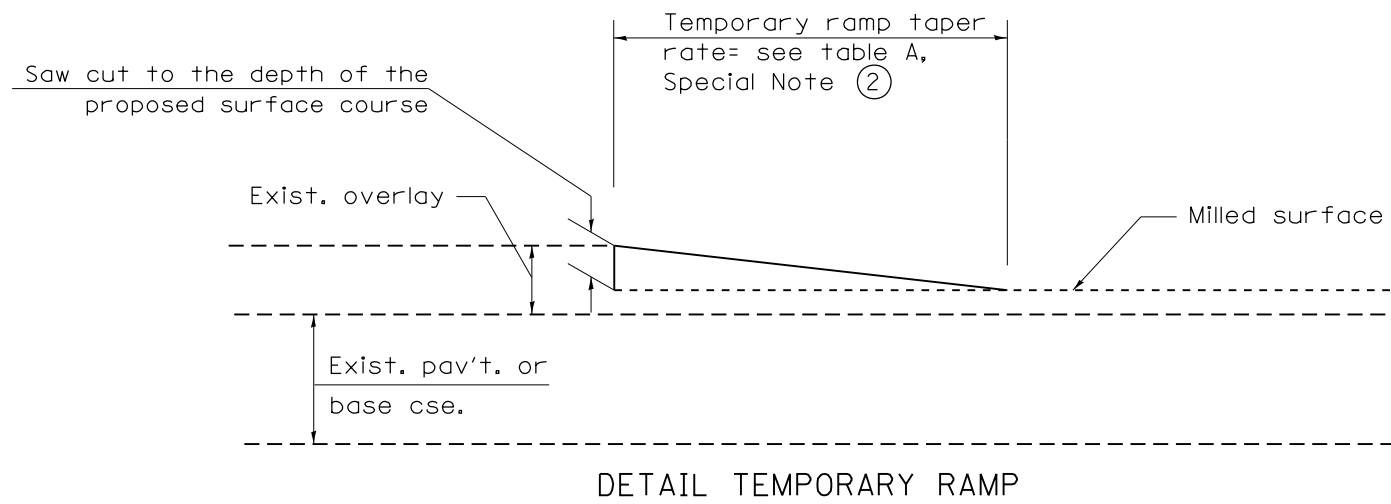


CASE 2 : NO HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

All dimensions are in inches (millimeters) unless otherwise noted.



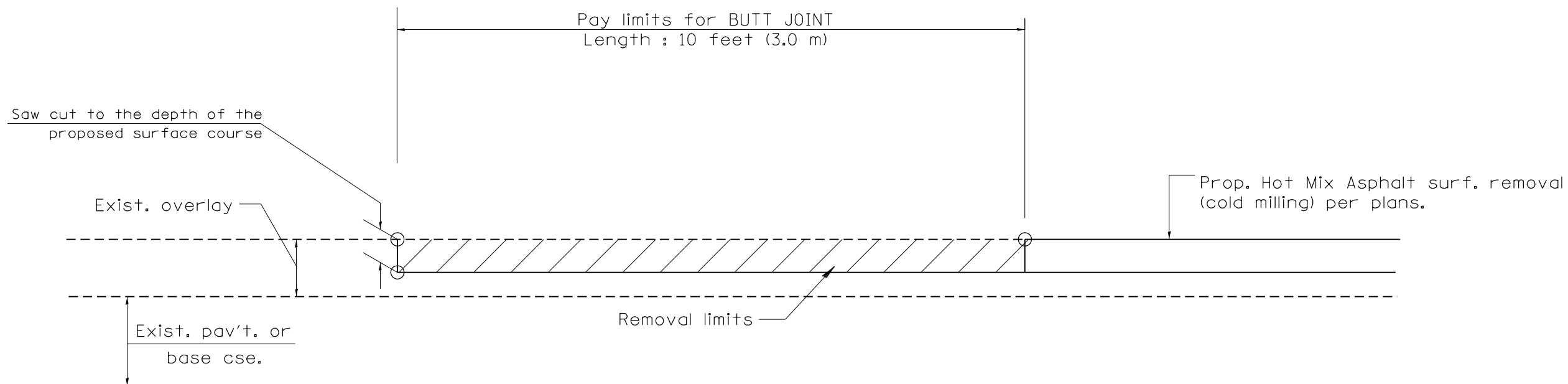
CASE 3 : HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)
TIE-IN TO EXISTING BITUMINOUS TAPER



All dimensions are in inches (millimeters) unless otherwise noted.

				STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		BUTT JOINTS		SHT. 2 OF 3 CADD STD. 406101-D4	
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.					
313	6RS-4;7RS-5	HENDERSON	33	26	CONTRACT NO. 68A67				
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT							

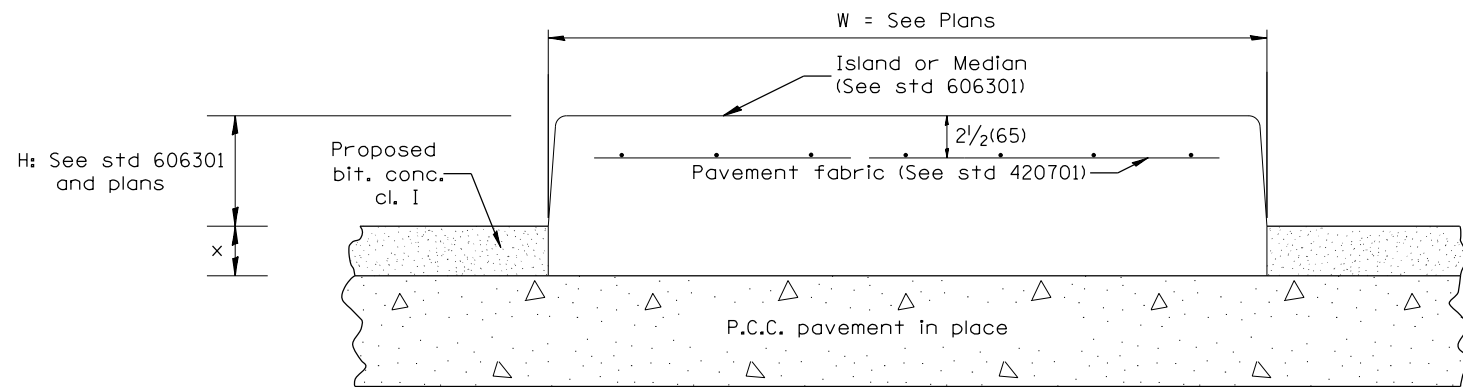
NOT TO SCALE



CASE 4 : SINGLE LIFT OVERLAY WITH EQUIVALENT DEPTH
HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)
TIE-IN TO EXISTING BITUMINOUS TAPER

All dimensions are in inches (millimeters) unless otherwise noted.

				STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		BUTT JOINTS		SHT. 3 OF 3 CADD STD. 406101-D4		<table border="1"> <tr> <th>F.A. RTE.</th> <th>SECTION</th> <th>COUNTY</th> <th>TOTAL SHEETS</th> <th>SHEET NO.</th> </tr> <tr> <td>313</td> <td>6RS-4;7RS-5</td> <td>HENDERSON</td> <td>33</td> <td>27</td> </tr> </table>		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	313	6RS-4;7RS-5	HENDERSON	33	27
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.																	
313	6RS-4;7RS-5	HENDERSON	33	27																	
						NOT TO SCALE				<table border="1"> <tr> <td>FED. ROAD DIST. NO.</td> <td>ILLINOIS FED. AID PROJECT</td> </tr> <tr> <td></td> <td></td> </tr> </table>		FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT								
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT																				



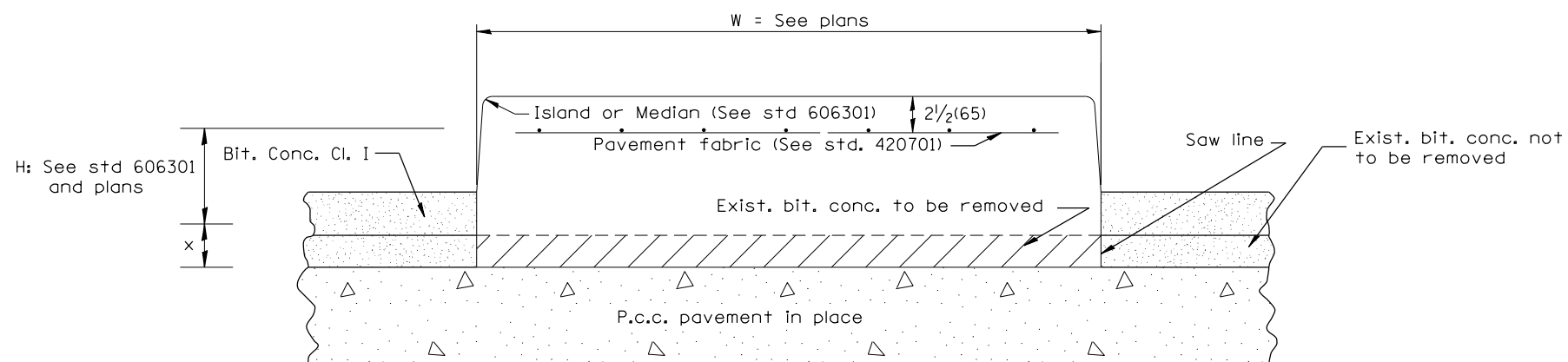
P.C.C. PAVEMENT

LEGEND

- X = thickness of proposed overlay (see plans)
- H = height of island or median (see std 606301 and plans)
- W = width of island or median (see plans)

GENERAL NOTES:

1. This work shall consist of constructing P.C. concrete Islands and medians of variable width as shown on the plans and in accordance to the applicable portions of section 606 of the Standard Specifications and Standard 606301.



PREVIOUSLY RESURFACED P.C.C. PAVEMENT

DESIGNER NOTES: 1. INCLUDE STD 420701 AND 606301
2. INCLUDE DISTRICT SPECIAL PROVISION

All dimensions are in inches (millimeters) unless otherwise noted.

01-01-97	RENUM. A-7.05, & A-7.07, NEW REVISION BOX, NEW FORMAT, REVISED TITLE BOX, REVISED NOTES	T.P.
10-16-06	REVISED TO 2007 SPEC.	M.A.

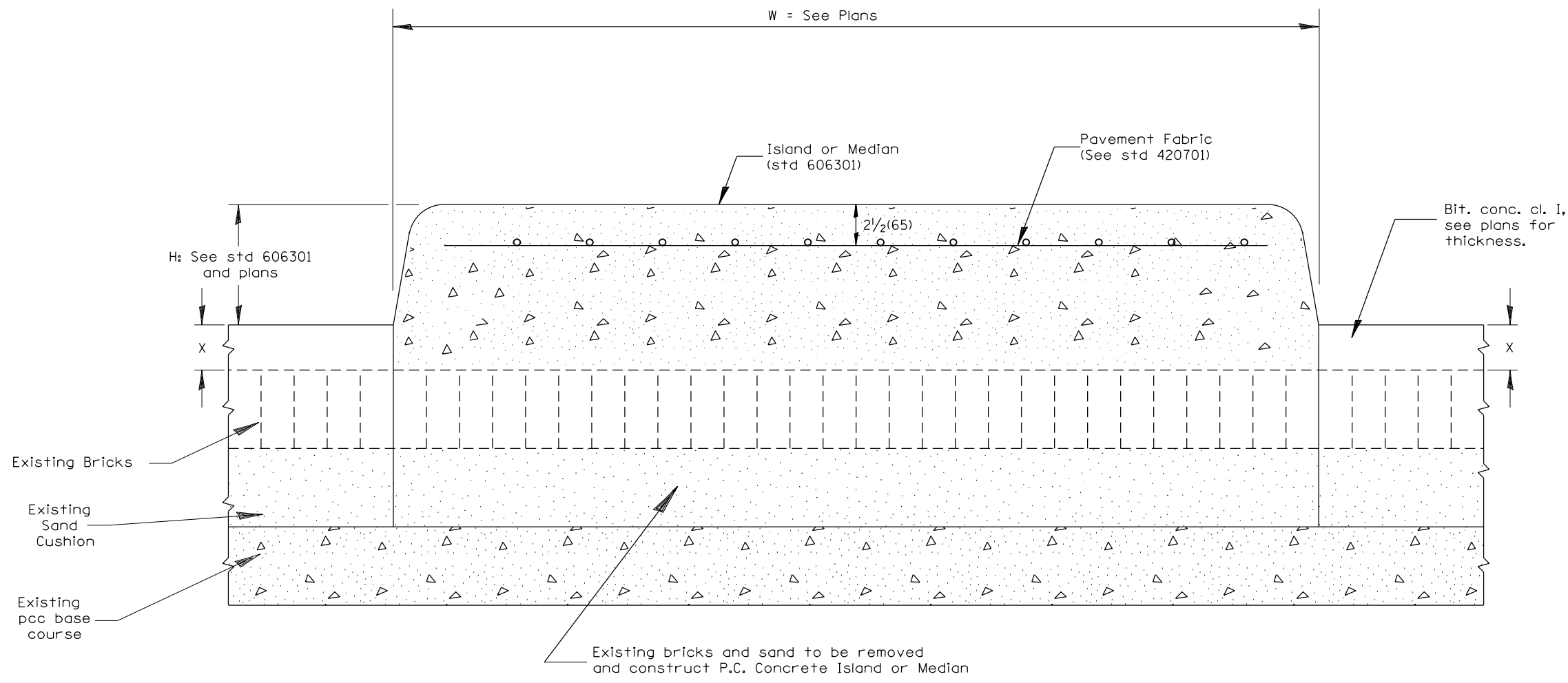
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**P.C. CONCRETE ISLANDS AND MEDIANS
(CONSTRUCTED ON TOP OF EXISTING PAVEMENT)**

NOT TO SCALE

SHT. 1 OF 2
CADD STD. 606301-D4

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	6RS-4;7RS-5	HENDERSON	33	28
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 68A67	



BRICK PAVEMENT

LEGEND

- X = thickness of proposed overlay (see plans)
- H = height of island or median (see std 606301 and plans)
- W = width of island or median (see plans)

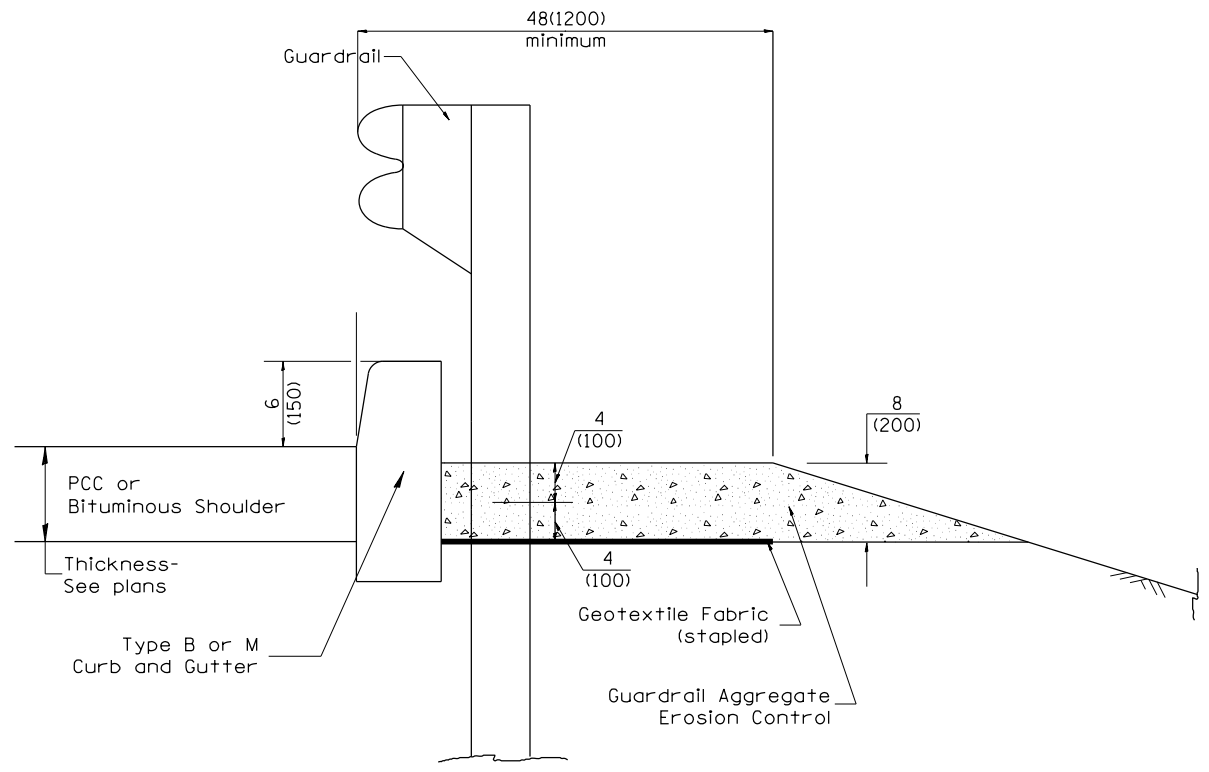
All dimensions are in inches (millimeters) unless otherwise noted.

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				P.C. CONCRETE ISLANDS AND MEDIANS (CONSTRUCTED ON TOP OF EXISTING PAVEMENT)				SHT. 2 OF 2 CADD STD. 606301-D4		CONTRACT NO. 68A67	
F.A. RTE.		SECTION		COUNTY		TOTAL SHEETS		SHEET NO.			
313		6RS-4;7RS-5		HENDERSON		33		29			
FED. ROAD DIST. NO.				ILLINOIS FED. AID PROJECT							

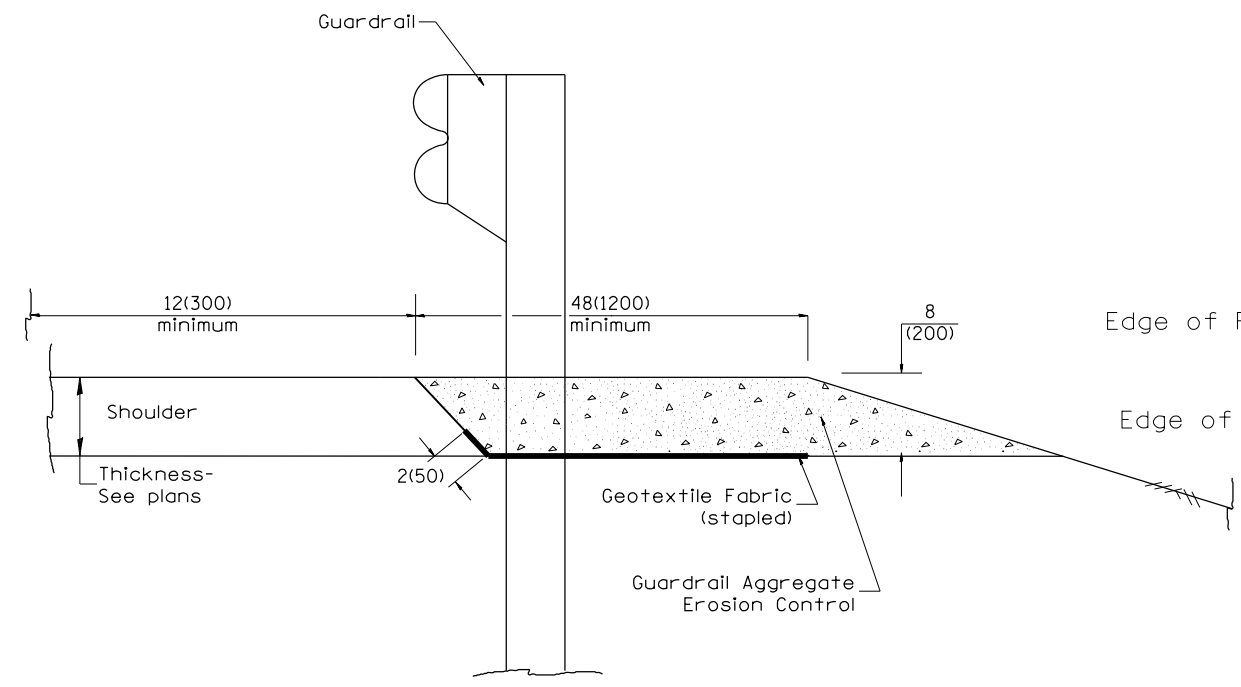
NOT TO SCALE

DESIGNER NOTES:

1. CONSIDER USING A "B" CURB PAY ITEM AT GUARDRAIL INSTALLATIONS WHERE GRADES ARE EQUAL TO OR GREATER THAN 1/2" AND AT INLETS. (INCLUDE DISTRICT SPECIAL PROVISION)
2. USE "GUARDRAIL AGGREGATE EROSION CONTROL" AT GUARDRAIL INSTALLATIONS WHERE GRADES ARE LESS THAN 1/2" (INCLUDE DISTRICT SPECIAL PROVISION).
3. INCLUDE STATE STANDARD 610001, IF APPLICABLE.
4. INCLUDE THE FOLLOWING DISTRICT CADD STANDARDS AS NEEDED: SLOPE DRAINS FOR EXPOSED PIPES; SLOPE DRAINS FOR BURIED PIPES; SEEPAGE COLLARS FOR BURIED PIPES
5. SLOPE DRAINS FOR EXPOSED PIPES; CONCRETE THRUST BLOCKS AND PIPE ELBOW.
6. INCLUDE DISTRICT SPECIAL PROVISION - "AGGREGATE QUALITY" FOR PROJECTS LOCATED IN THE WESTERN AREA OF THE DISTRICT - APPROX. DIVIDING LINE IS IL 97.
7. DELETE DESIGNER NOTES WHEN INSERTING INTO PLAN FILES.
8. OPERATIONS PREFERS USE OF PIPE OUTLETTING ONTO FORESLOPE WITH RIPRAP. USE NON-METALLIC PIPE WHEN POSSIBLE BECAUSE OF FUTURE CORROSION ISSUES.



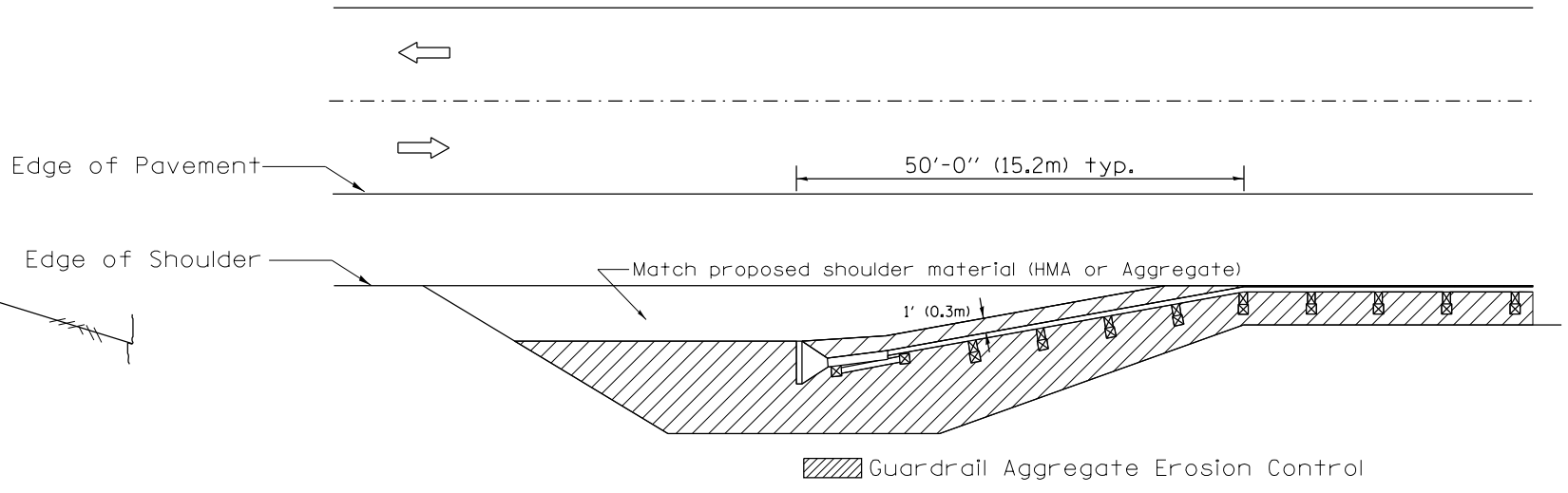
TYPICAL SECTION WITH EROSION CONTROL CURB



TYPICAL SECTION WITHOUT EROSION CONTROL CURB

GENERAL NOTES: GUARDRAIL AGGREGATE EROSION CONTROL

1. This work shall consist of grading as needed, furnishing and installing geotextile fabric and staples, and furnishing, placing and shaping crushed aggregate around and behind Steel Plate Beam Guardrail posts in accordance with Plan Details.
2. Before placing the aggregate and the Geotextile Fabric, weeds and grass shall be removed from the area to be covered.
3. After the area has been prepared, and in a dry condition, the Geotextile fabric shall be placed with a 12(300) minimum overlap. A knife cut for guardrail post installation is necessary.
4. The aggregate shall be deposited, compacted and shaped by either mechanical or hand methods, in a manner reasonably true to line and grade.
5. The Contractor shall have the option of placing the guardrail before or after the Geotextile Fabric and Aggregate are in place. If the guardrail is placed after the Geotextile Fabric and Aggregate, then any voids must be filled and the aggregate returned to line and grade.
6. Materials shall meet the following requirements:
 - A. The crushed aggregate shall be CA1 gradation in accordance with Article 1004.01(c) of the Standard Specifications.
 - B. The Geotextile Fabric shall be nonwoven fabric in accordance with Article 1080.02 of the Standard Specifications.



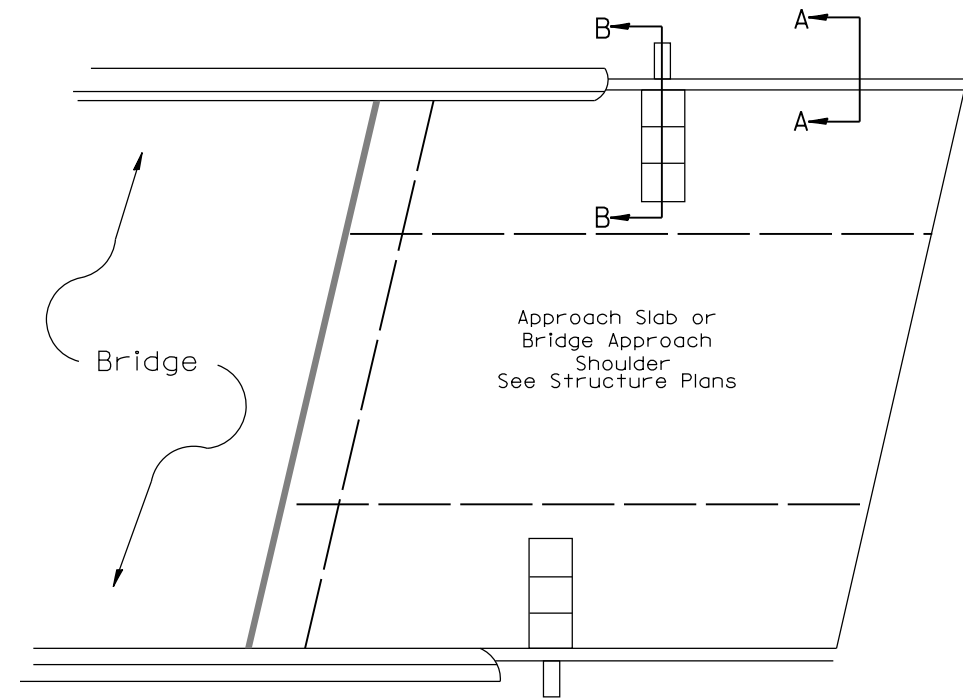
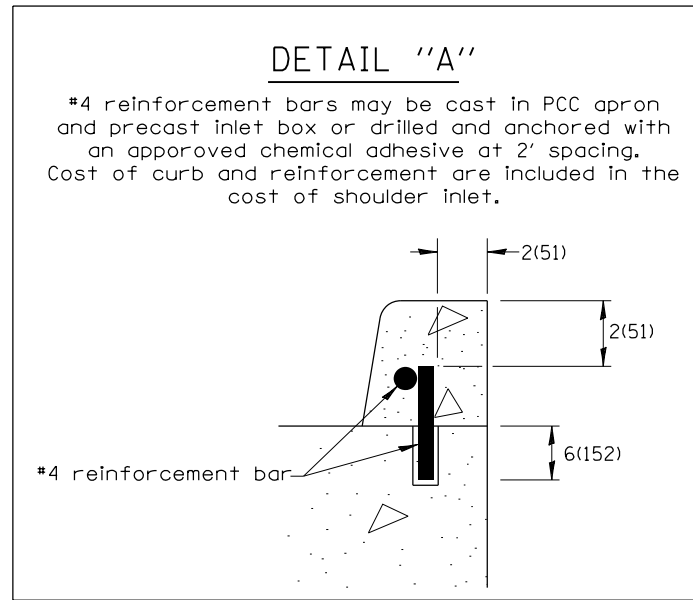
All dimensions are in inches (millimeters) unless otherwise noted.

01-01-97	RENUM. C-22.01, NEW REVISION BOX	T.P.	3-7-11	Added Detail showing plan view	R.D.
03-01-97	CORRECT STD. NUMBERS IN NOTES PG. 2	J.A.	8-10-12	Revised curb "B" and aggregate	R.D.
11-03-00	CORRECTION TO NOTES	M.A.	7-15-15	Addressed shoulder inlet curb	R.D.
10-16-06	REVISED TO 2007 SPEC.	M.A.			

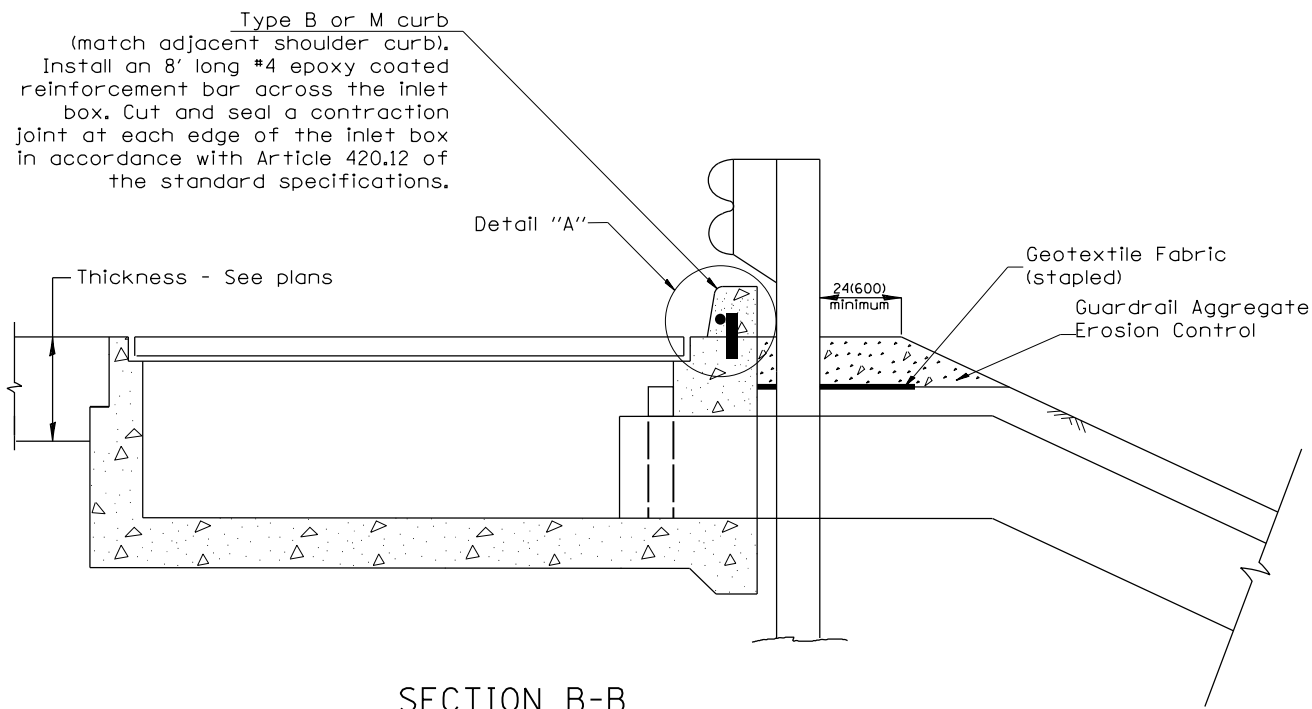
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GUARDRAIL EROSION CONTROL TREATMENTS
NOT TO SCALE
SHT. 1 OF 2
CADD STD. 630101-D4

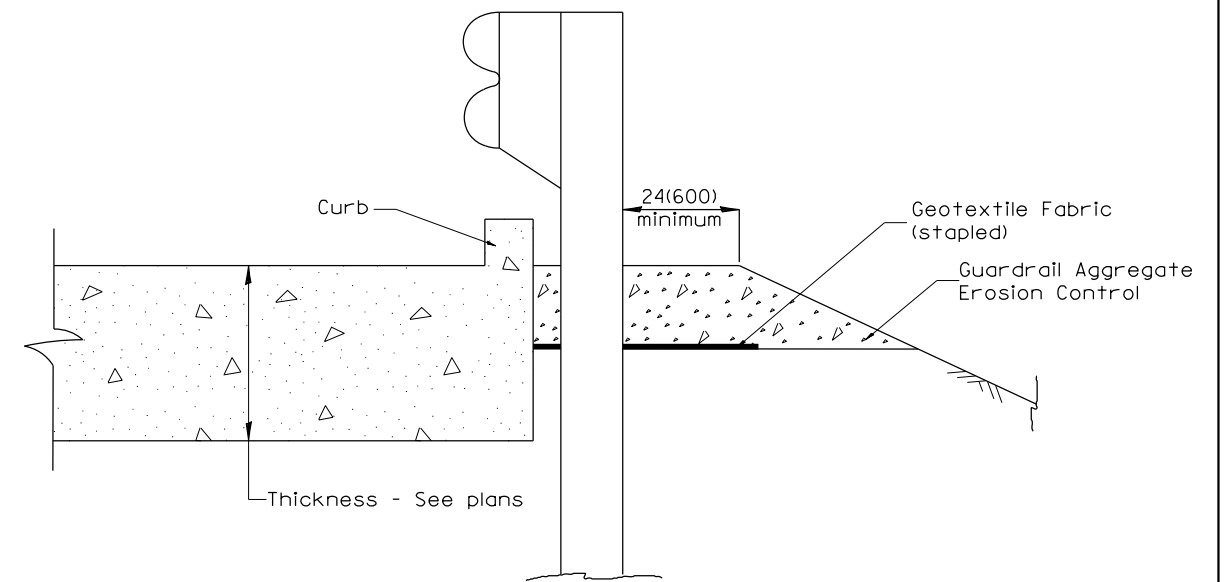
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	6RS-4;7RS-5	HENDERSON	33	30
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 68A67	



PLAN VIEW
APPROACH SLAB OR SHOULDER PLACEMENT



SECTION B-B
TYPICAL SECTION AT INLETS
TYPE E, F & G (HIGHWAY STANDARD 610001)

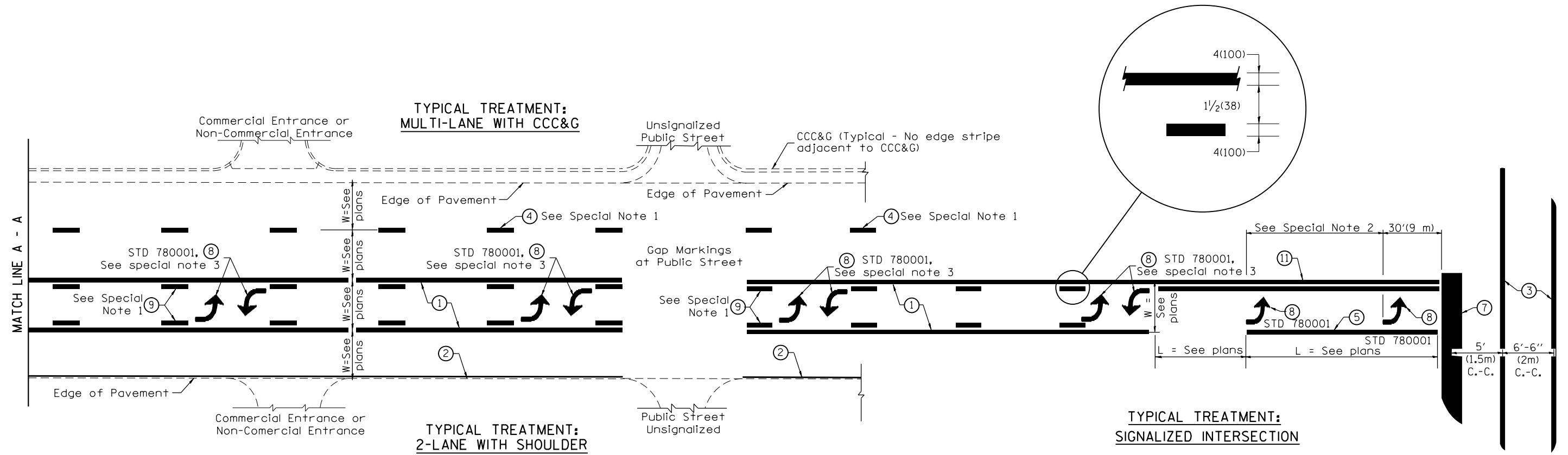


SECTION A-A
TYPICAL SECTION WITH BRIDGE APPROACH CURB

All dimensions are in inches (millimeters) unless otherwise noted.

FILE NAME =	USER NAME = oterofebuse	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GUARDRAIL EROSION CONTROL TREATMENTS	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Phase II Sheets.dgn		DRAWN -	REVISED -			313	6RS-4;7RS-5	HENDERSON	33	31	
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			SHT. 2 OF 2 CONTRACT NO. 68A67					
Default	PLOT DATE = 12/9/2016	DATE -	REVISED -			NOT TO SCALE	SHEET OF SHEETS	STA.	CADD STD. 630101-D4	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT

DESIGNER NOTES:
1. Include State Standard 780001 (Typical Pavement Markings)



FLUSH PAVED MEDIAN: TWO-WAY LEFT TURN LANE WITH ONE-WAY LEFT TURN LANE AT SIGNALIZED INTERSECTION

TYPICAL PAVEMENT MARKING LEGEND
(Note: This is a District Standard Legend. Some elements may not apply to specific project.)

- ① 4(100) Solid (Yellow)
- ② 4(100) Solid (White)
- ③ 2-6(150) Crosswalk @ 6'-6" (2m)min C.-C. (White)
2-8(200) Crosswalk @ 6'-6" (2m)min C.-C. (White) (When traffic signals are present.)
- ④ 6(150) Skip-Dash (White) (See Special Note 1)
- ⑤ 8(200) Solid (White)
- ⑥ 12(300) Diagonal (White) (Item ⑥ is shown on Std. 780001)
- ⑦ 24(600) Stop Bar (White)
- ⑧ Letters & Arrows (See Std. 780001 and Special Notes 2 & 3)
- ⑨ 4(100) Skip-Dash (Yellow) (See Special Note 1)
- ⑩ 12(300) Diagonal (Yellow) (See Table A) ⑩
- ⑪ 4(100) Double Solid (Yellow) ⑪

SPECIAL NOTES

1. Skip-Dash markings will be centered between both ends of city blocks and shall be placed in alignment transversely across the pavement.
2. The following shall apply to arrows located in one-way left turn lanes:
 - A. A minimum of two (2) arrows is required.
 - B. The maximum spacing between arrows is 80' (24 m).
 - C. Arrows shall be evenly spaced if three (3) or more are required.
3. The following shall apply to arrow pairs located in two-way left turn lanes:
 - A. A minimum of two (2) arrow pairs is required.
 - B. The maximum spacing between arrow pairs is 200' (61 m).
 - C. Arrow pairs shall be evenly spaced if three (3) or more are required.
 - D. The spacing between Bi Directional Left Turn Arrows is 33' (10 m).

GENERAL NOTES

1. Refer to State Standard 780001 for additional Pavement Markings including letters & arrows.
2. See Plans for Pavement Markings adjacent to curbed islands and medians, and through lane reductions.
3. Refer to Article 780.13 for letter, number and symbol areas (sq. ft.)
4. Areas are grooved 1" beyond each edge for the following symbols:
Through Arrow= 14.8 sq. ft.
Large Left or Right Arrow= 21.9 sq. ft.
2 Arrow Combination Left (or Right) and Through= 34.9 sq. ft.
Wrong Way Arrow= 29.5 sq. ft.
Railroad Crossing Symbol= 69.8 sq. ft.
(For further information, refer to BDE Special Provision: Grooving for Recessed Pavement Markings)

01-01-97	RENUM. F-8.03, NEW REVISION BOX	T.P.	10-16-06	REVISED TO 2007 SPEC.	
02-07-97	ADD BI DIRECTIONAL DIMENSION	J.A.	2/29/16	ADDED GROOVING AREAS	R.D.
10-97	CORRECT BI DIRECTIONAL DIMENSION	J.A.			
08-02	ADD CROSSWALK DMNS. WITH T.S.	M.A.			

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL PAVEMENT MARKINGS

SHT. 1 OF 2
CADD STD. 780001-D4

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	6RS-5;7RS-5	HENDERSON	33	32
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	

